

705-UNDERDRAIN NOTES:

THE PROPOSED UNDERDRAIN PIPE WILL BE CONSTRUCTED IN ACCORDANCE WITH ITEM 705 "PIPE UNDERDRAINS FOR AIRPORTS" AS STATED ON PAGE 310 OF THE ILLINOIS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THIS ITEM OF WORK SHALL CONSIST OF FURNISHING AND INSTALLING 6" P.E. TUBING (WRAPPED) AND UNDERDRAIN INSPECTION HOLES AT THE LOCATIONS AND TO THE GRADES SHOWN ON THE CONSTRUCTION PLANS.

705-3.3 LAYING AND INSTALLING PIPE. REVISE THIS SECTION AS FOLLOWS:

"PIPE DRAINS SHALL BE INSTALLED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. THE PIPE SHALL BE BEDDED IN THE UNDERLYING MATERIAL TO A DEPTH NOT LESS THAN 10 PERCENT OF THE EXTERNAL DIAMETER OF THE PIPE, AND WHERE TRENCHING IS REQUIRED, THE TRENCH SHALL HAVE A WIDTH OF NOT LESS 8 IN. THE BOTTOM OF THE TRENCH SHALL BE COMPACTED IN A MANNER MEETING THE APPROVAL OF THE ENGINEER.

JOINTS AND FITTINGS MAY BE ASSEMBLED WITHOUT GASKETS OR SOLVENT CEMENT IF THE JOINT IS SAND TIGHT AND THE SPIGOT ENTERS THE SOCKET NOT LESS THAN 1/3 OF THE SOCKET DEPTH FOR SOLVENT CEMENT JOINTS AND FULL-DEPTH FOR ELASTOMERIC GASKET JOINTS.

NO PIPE SHALL BE PLACED IN THE TRENCH UNTIL IT AND THE PREPARED FOUNDATION HAVE BEEN APPROVED BY THE RESIDENT ENGINEER. THE PIPE SHALL BE LAID SO THAT THE FLOWLINE WILL BE AT THE GRADE SHOWN ON THE PLANS OR ESTABLISHED BY THE RESIDENT ENGINEER. THE PERMISSIBLE MINIMUM COVER OVER A PIPE SHALL BE 12 IN.

LAYING OF PIPES SHALL COMMENCE AT THE OUTLET END AND PROCEED TOWARD THE INLET END WITH THE PIPES TRUE TO LINE AND GRADE.

THE ENDS OF THE PIPE SHALL BE CAREFULLY CLEANED BEFORE THEY ARE PLACED, AND SHALL BE PLACED TO AVOID UNNECESSARY HANDLING ON THE FOUNDATION. AS EACH LENGTH OF PIPE IS LAID, THE ENDS OF THE PIPE SHALL BE PROTECTED TO PREVENT THE ENTRANCE OF ANY MATERIAL.

LONGITUDINAL LAPS SHALL BE PLACED AT THE SIDES AND SEPARATE SECTIONS OF PIPE SHALL BE JOINED WITH TIGHTLY-DRAWN, APPROVED CONNECTING BANDS.

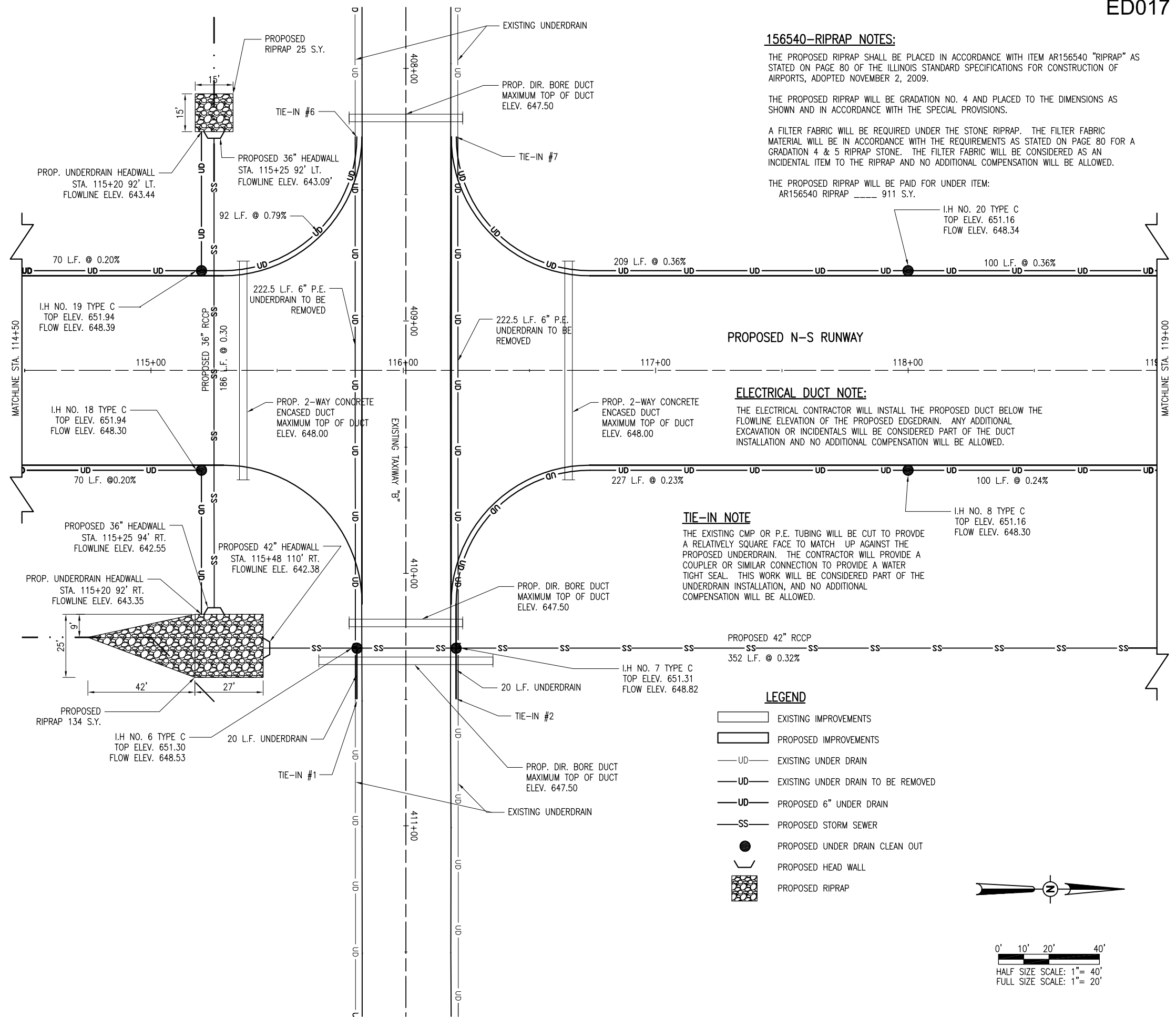
705-3.6 BACKFILLING; ADD THE FOLLOWING TO THIS SECTION:

"THE EDGE DRAIN TRENCH WILL BE BACKFILLED WITH CONSOLIDATED POROUS BACKFILL NO. 1 IN ACCORDANCE WITH THE DETAIL ON THE CONSTRUCTION PLANS. THE POROUS BACKFILL WILL BE COMPACTED IN ACCORDANCE WITH THE SPECIFICATION SET FORTH FOR GRANULAR MATERIAL BACKFILL."

THE PROPOSED UNDERDRAIN PIPE WILL BE PAID FOR UNDER ITEMS: AR70541? POROUS BACKFILL NO. 1 370 C.Y. AR705526 6" PERFORATED UNDERDRAIN W/SOCK 7,198 L.F. AR705530 UNDERDRAIN INSPECTION HOLE 24 EACH AR705900 REMOVE UNDERDRAIN 1,009 L.F.

UNDERDRAIN HEADWALL NOTE

THE PROPOSED EDGEDRAIN LATERAL RUNS AT STA 115+20 WILL REQUIRE SMALL CONCRETE HEADWALL STRUCTURES. THESE STRUCTURES WILL BE IN ACCORDANCE TO ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD 601101. THESE TWO STRUCTURES WILL BE CONSIDERED PART OF THE "6" PERFORATED UNDERDRAIN W/ SOCK" PAY ITEM AND NO OTHER COMPENSATION WILL BE ALLOWED.



156540-RIPRAP NOTES:

THE PROPOSED RIPRAP SHALL BE PLACED IN ACCORDANCE WITH ITEM AR156540 "RIPRAP" AS STATED ON PAGE 80 OF THE ILLINOIS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THE PROPOSED RIPRAP WILL BE GRADATION NO. 4 AND PLACED TO THE DIMENSIONS AS SHOWN AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

A FILTER FABRIC WILL BE REQUIRED UNDER THE STONE RIPRAP. THE FILTER FABRIC MATERIAL WILL BE IN ACCORDANCE WITH THE REQUIREMENTS AS STATED ON PAGE 80 FOR A GRADATION 4 & 5 RIPRAP STONE. THE FILTER FABRIC WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE RIPRAP AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE PROPOSED RIPRAP WILL BE PAID FOR UNDER ITEM: AR156540 RIPRAP 911 S.Y.

ELECTRICAL DUCT NOTE:

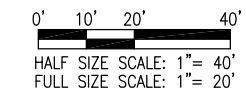
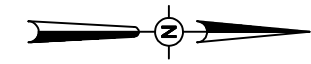
THE ELECTRICAL CONTRACTOR WILL INSTALL THE PROPOSED DUCT BELOW THE FLOWLINE ELEVATION OF THE PROPOSED EDGEDRAIN. ANY ADDITIONAL EXCAVATION OR INCIDENTALS WILL BE CONSIDERED PART OF THE DUCT INSTALLATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

TIE-IN NOTE

THE EXISTING CMP OR P.E. TUBING WILL BE CUT TO PROVIDE A RELATIVELY SQUARE FACE TO MATCH UP AGAINST THE PROPOSED UNDERDRAIN. THE CONTRACTOR WILL PROVIDE A COUPLER OR SIMILAR CONNECTION TO PROVIDE A WATER TIGHT SEAL. THIS WORK WILL BE CONSIDERED PART OF THE UNDERDRAIN INSTALLATION, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

LEGEND

- EXISTING IMPROVEMENTS
PROPOSED IMPROVEMENTS
EXISTING UNDER DRAIN
EXISTING UNDER DRAIN TO BE REMOVED
PROPOSED 6" UNDER DRAIN
PROPOSED STORM SEWER
PROPOSED UNDER DRAIN CLEAN OUT
PROPOSED HEAD WALL
PROPOSED RIPRAP



Revision table with columns for REVISION, DATE, and description. Project information: EDGAR COUNTY AIRPORT, PARIS, ILLINOIS. Project No.: 3-17-0077-B13. ILL. PROJ.: PRG-4018. Hanson Professional Services Inc. logo and contact info. Drawing title: CONSTRUCT CROSSWIND RUNWAY 18-36. Proposed Drainage Plan STA. 115+00 TO STA. 119+00. Sheet number 29 of 137 sheets.

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