

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	J
ILLINOIS			CONTRACT NO. 62P10	

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT LOCATED IN UNINCORPORATED BREMEN TOWNSHIP OF COOK COUNTY

PROPOSED HIGHWAY PLANS

FAI ROUTE 57 (I-57) AT 175TH STREET SECTION 2021-073-BDR PROJECT NHPP-9KPB(049) BRIDGE REHABILITATION COOK COUNTY

C-91-196-21

TRAFFIC DATA

I-57
ADT: 123,100 (2018)
POSTED SPEED: 55 MPH
DESIGN SPEED: 60 MPH
DESIGN DESIGNATION: INTERSTATE
175TH ST.
ADT: 4450 (2018)
POSTED SPEED: 45 MPH
DESIGN SPEED: 45 MPH
DESIGN DESIGNATION: MAJOR COLLECTOR

APPLY TO SHEETS: 1-27, 63-89

APPLY TO SHEETS: 28-33

APPLY TO SHEETS: 34-62



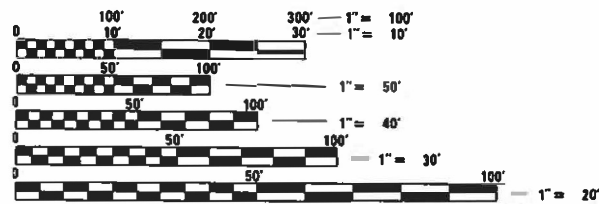
Matthew A. Miller
MATTHEW A. MILLER
HNTB
NO.: 062060879
DATE SIGNED: 1/26/2022
EXP. DATE: 11/30/2023



Matthew C. Durning
MATTHEW C. DURNING
HNTB
NO.: 062055699
DATE SIGNED: 1/26/2022
EXP. DATE: 2/28/2022



Luke C. Martin
LUKE C. MARTIN
GKE, LLC
NO.: 081007429
DATE SIGNED: 1/26/2022
EXP. DATE: 11/30/2022



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811

MEADE ELECTRIC CO. DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES 773-287-7672

PROJECT ENGINEER: PRAVEEN KAINI (847) 705-4237
PROJECT MANAGER: JEAN ALAIN MIDY (847) 221-3056

CONTRACT NO. 62P10

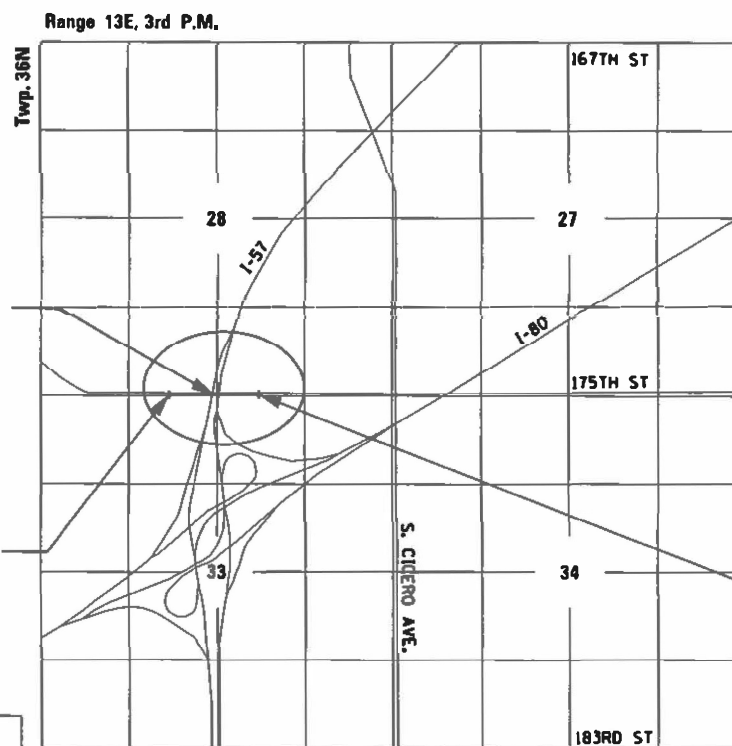


LOCATION OF SECTION INDICATED THIS: - [black rectangle]



BRIDGE REHABILITATION
EX. S.N. 016-1095

BEGIN IMPROVEMENT
STA. 100 + 30



END IMPROVEMENT
STA. 114 + 75

PROJECT LENGTH

GROSS LENGTH = 1445 FT. = 0.274 MILE
NET LENGTH = 1445 FT. = 0.274 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED FEBRUARY 1, 2022
[Signature] REGIONAL ENGINEER

March 25, 2022
[Signature] ENGINEER OF DESIGN AND ENVIRONMENT

March 2, 2022
[Signature] DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL JULIE AT (800) 892-0123 OR 811 TO DETERMINE WHICH UTILITIES ARE WITHIN THE AREA. 48 HOUR NOTIFICATION IS REQUIRED.
2. EXCEPT AS NOTED ON THE PLANS, PAVEMENT GRADES SHOWN ARE AT THE TOP OF PAVEMENT.
3. 10' TRANSITIONS SHALL BE USED FOR PROPOSED CURB AND GUTTER EXISTING CURB AND GUTTERS IN THE FIELD UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5. WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJACENT RESIDENTIAL AREAS.
6. THE CONTRACTOR SHALL CONTACT THE IDOT ARTERIAL TRAFFIC SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV AND THE IDOT EXPRESSWAY TRAFFIC CONTROL SUPERVISOR AT 847-705-4155 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
7. SEEDING SHALL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION.
8. ONLY THOSE TREES DESIGNATED BY THE ENGINEER OR LISTED IN THE TREE REMOVAL SCHEDULE SHALL BE REMOVED. THE CONTRACTOR SHALL PROTECT ALL REMAINING TREES AND ROOT SYSTEMS FROM DAMAGE DUE TO CONSTRUCTION OPERATIONS.
9. THE REMOVAL OF EXISTING GUARDRAIL AND TERMINAL SECTIONS SHALL BE INCLUDED IN THE UNIT PRICE PER FOOT FOR GUARDRAIL REMOVAL.
10. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
11. THE COST OF SAW CUTTING, (FULL DEPTH) SHALL BE INCLUDED IN THE UNIT PRICES FOR THE VARIOUS REMOVAL PAY ITEMS.
12. THE CONTRACTOR SHALL PROTECT EXISTING AND NEW UTILITIES AND SHALL BRACE AND SUPPORT THE UTILITIES PROPERLY IN ORDER TO PREVENT SETTLEMENT, DISPLACEMENT, OR DAMAGE TO THE UTILITIES. THE PROTECTION OF THE UTILITIES AS SPECIFIED HEREIN WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT.
13. THE CONTRACTOR SHALL TAKE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
14. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS, AS REQUIRED, PRIOR TO COMMENCING WITH CONSTRUCTION. ANY COST ASSOCIATED WITH OBTAINING THESE PERMITS SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE FOR THE ITEMS BEING INSTALLED.
15. POLLUTION CONTROL: THE CONTRACTOR SHALL BE REQUIRED TO COMPLY WITH STATE REGULATIONS REGARDING AIR, WATER, AND NOISE POLLUTION. THIS WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.
16. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING DRAINAGE THROUGHOUT THE CONSTRUCTION OF THIS PROJECT. THIS WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.
17. NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET.
18. GEOTECHNICAL FABRIC FOR GROUND STABILIZATION AND/OR AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAVE BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ABOVE ITEM WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
19. ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTOR'S VEHICLES AND/OR EQUIPMENTS IS TO BE REMOVED AND REPLACED AS DIRECT BY THE ENGINEER AT CONTRACTOR EXPENSE.
20. THE SUBGRADE STABILITY SHALL BE VERIFIED BY PROOF ROLLING WITH A FULLY LOADED TANDEM-AXLE TRUCK.
21. THE AGGREGATE GRADATION FOR THE AGGREGATE SUBGRADE IMPROVEMENT 12" LOWER LIFT SHALL BE CS 1 OR RR1.
22. THE CONTRACTOR SHALL OBSERVE AND COMPLY WITH ALL SECTIONS OF THE ILLINOIS SPRAY LAW, INCLUDING, LICENSING. CONTRACTOR PERSONNEL APPLYING HERBICIDES SHALL HAVE A VALID PESTICIDE APPLICATOR LICENSE ISSUED BY THE ILLINOIS DEPARTMENT OF AGRICULTURE. THE LICENSED PESTICIDE APPLICATOR SHALL SUBMIT THEIR CURRENT LICENSE TO THE ENGINEER. THE LICENSED PESTICIDE APPLICATOR SHALL BE QUALIFIED AT A MINIMUM IN RIGHT-OF-WAY AND AQUATICS. THE LICENSED APPLICATOR SHALL WORK ON-SITE.
23. THE SEEDING DATES FOR BARE EARTH SEEDING MIXTURE CLASS 4A (MODIFIED) SHALL BE FROM NOVEMBER TO MAY 31. ALL SEEDING NOT SOWN ACCORDING TO THE SPECIFIED SEASONAL DATE SHALL REQUIRE PRIOR WRITTEN APPROVAL FROM THE ENGINEER. FAILURE TO SECURE SUCH APPROVAL SHALL RESULT IN THE REJECTION OF THE SEEDING AND REPLACEMENT BY THE CONTRACTOR AT HIS/HER EXPENSE.
24. THE ENGINEER WILL CONTACT FABIOLA QUIROZ OF THE ROADSIDE DEVELOPMENT UNIT (847) 705-4596, AT LEAST 7 DAYS PRIOR TO PLANTING FOR LAYOUT OF THE SEEDING.
25. ALL PERMANENT SEEDING RESTORATION SHALL ALSO TAKE PLACE IN AREAS WHERE LIGHTING AND CABLE WORK IS PROPOSED.
26. THE SEEDING DATES FOR BARE EARTH SEEDING MIXTURE CLASS 2A (MODIFIED) SHALL BE FROM APRIL 1 TO JUNE 1 AND FROM AUGUST 15 TO SEPTEMBER 30. ALL SEEDING NOT SOWN ACCORDING TO THE SPECIFIED SEASONAL DATE SHALL REQUIRE PRIOR WRITTEN APPROVAL FROM THE ENGINEER. FAILURE TO SECURE SUCH APPROVAL SHALL RESULT IN THE REJECTION OF THE SEEDING AND REPLACEMENT BY THE CONTRACTOR AT HIS/HER EXPENSE.
27. THE FINAL LOCATIONS OF THE LANDSCAPE ITEMS WILL BE DETERMINED IN THE FIELD OFFICE. CONTRACTOR SHALL NOTIFY THE ROADSIDE DEVELOPMENT UNIT, FABIOLA QUIROZ, AT 847-705-4596 7 DAYS PRIOR TO STARTING INSTALLATION.
28. THE CONTRACTOR SHALL ERECT A TEMPORARY FENCE AROUND ALL TREES WITHIN THE CONSTRUCTION AREA TO ESTABLISH A "TREE PROTECTION ZONE" BEFORE ANY WORK BEGINS OR ANY MATERIAL IS DELIVERED TO THE JOBSITE. NO WORK IS TO BE PERFORMED (OTHER THAN ROOT PRUNING), MATERIALS STORED, OR VEHICLES DRIVEN OR PARKED WITHIN THE "TREE PROTECTION ZONE". REMOVE PROTECTIVE TEMPORARY FENCE ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED.
29. THE CONTRACTOR SHALL TAKE EXTRA CARE IN GRADING AND EXCAVATING NEAR TREES WHICH ARE NOT MARKED FOR REMOVAL SO AS NOT TO CAUSE INJURY TO THE ROOT SYSTEMS FOR TRUNKS. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
30. THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS WHICH WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
31. THE CONTRACTOR SHALL NOTIFY THE ROADSIDE DEVELOPMENT UNIT, FABIOLA QUIROZ, AT 847-705-4596, 2 WEEKS PRIOR TO TREE REMOVAL OPERATIONS.
32. THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) AND USE/ REVIEW (BDE 2290) SUBMITTALS, THE CONTRACTOR SHALL SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION II.G.1 AND 2 OF THE SWPPP. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

INDEX OF SHEETS

1	COVER SHEET
2	GENERAL NOTES, INDEX OF SHEETS, STANDARD DRAWINGS
3 - 7	SUMMARY OF QUANTITIES
8 - 10	SCHEDULE OF QUANTITIES
11 - 12	TYPICAL SECTIONS
13	ALIGNMENT, TIES, AND BENCHMARKS
14	REMOVAL AND PROPOSED PLAN
15	PROPOSED PROFILE
16	MAINTENANCE OF TRAFFIC GENERAL NOTES
17	MAINTENANCE OF TRAFFIC TYPICAL SECTIONS
18	MAINTENANCE OF TRAFFIC TEMPORARY PAVEMENT DETAIL
19 - 20	MAINTENANCE OF TRAFFIC - STAGING
21 - 22	SIGNING PLANS
23 - 27	EROSION CONTROL PLANS
28 - 33	PROPOSED LIGHTING PLANS
34 - 62	STRUCTURAL PLANS
63 - 78	CROSS SECTIONS
79 - 89	DISTRICT 1 HIGHWAY STANDARDS

IDOT STANDARD DRAWINGS

STANDARD NUMBERS	DESCRIPTION
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
420401-13	PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB
482001-02	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
482011-03	HMA SHLD. STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
483001-06	PCC SHOULDER
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
610001-09	SHOULDER INLET WITH CURB
630001-12	STEEL PLATE BEAM GUARDRAIL
630201-07	PCC/HMA STABILIZATION AT STEEL BEAM GUARDRAIL
630301-09	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
631026-06	TRAFFIC BARRIER TERMINAL, TYPE 5
631031-17	TRAFFIC BARRIER TERMINAL, TYPE 6
664001-02	CHAIN LINK FENCE
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701011-04	OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701201-05	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS >=45 MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-04	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >=45 MPH
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
701326-04	LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS LESS THAN OR EQUAL TO 45 MPH
701400-11	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-13	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS >=45 MPH
701428-01	TRAFFIC CONTROL, SETUP AND REMOVAL, FREEWAY/EXPRESSWAY
701446-11	TWO LANE CLOSURE FREEWAY/EXPRESSWAY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701901-08	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
725001-01	OBJECT AND TERMINAL MARKERS
728001-01	TELESCOPING STEEL SIGN SUPPORT
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

DISTRICT 1 STANDARD DRAWINGS

STANDARD NUMBERS	DESCRIPTION
TC-08	ENTRANCE AND EXIT RAMP CLOSURE DETAILS
TC-09	SINGLE LANE WEAVE AND MULTI-LANE WEAVE
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-11	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
TC-13	TYPICAL PAVEMENT MARKINGS
TC-17	PARTIAL RAMP AND SHOULDER CLOSURE DETAILS
TC-22	ARTERIAL ROAD INFORMATION SIGN
BD-32	BUTT JOINT AND HMA TAPER DETAILS
BD-34	DETAILS FOR DEPRESSED CURB & GUTTER AND SHOULDER TREATMENT AT TBT TY. 1 SPL.
BD-51	BENCHING DETAIL FOR EMBANKMENT WIDENING
BE-903	PIER/ABUTMENT MOUNTED LED UNDERPASS LUMINAIRE INSTALLATION DETAILS

MODEL: Default
 FILE NAME: p:\updown\int-bhb\comp\PIUG\cast_1\skel\Documents\Chicago Projects\80677 IDOT Phase II Various Tack_Curbs\TOD-I & TD-045 - 175th Street at 1577\CAD\Drawings\Sheet\02918-int-notes01.dgn



USER NAME = amalla.baymundo	DESIGNED - AMB	REVISED -
PLOT SCALE = 40,0000 */ in.	DRAWN - AMB	REVISED -
PLOT DATE = 3/17/2022	CHECKED - MM	REVISED -
	DATE - 01/31/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES
INDEX OF SHEETS AND STANDARD DRAWINGS**

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	2
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

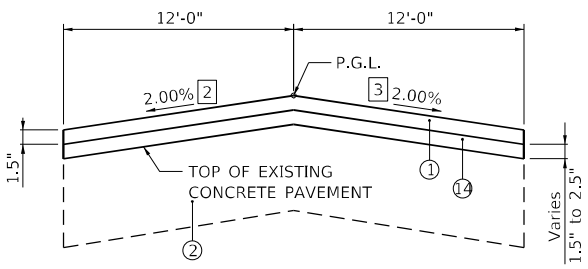
MODEL: Default
 FILE NAME: p:\work\hntb\hntb\p01\Grest_Lakes\Documents\Chicgo_Proj\2021-073-BDR_175th_Street_157\CAD\DWG\Sheet\021018-Int-ched03.dwg
 PROJECT: 2021-073-BDR

STATION	INTERVAL	PR TOPSOIL (SF)	PR FILL (SF)	PR EXCAVATION (SF)	PR TOPSOIL REM (SF)	TOPSOIL EXCAVATION (CU YD)	TOPSOIL FURNISH (CU YD)	EARTH EXCAVATION (CU YD)	EXCAVATION AVAILABLE FOR EMBANKMENT (ADJ. FOR SHRINKAGE - 25%) (CU YD)	EMBANKMENT (CU YD)	EARTHWORK WASTE (+) OR SHORTAGE (-) (CU YD)
PRE-STAGE											
100+00.00	0.00	1.3	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
100+50.00	50.00	1.3	0.2	2.7	2.4	2.3	2.3	2.5	1.8	0.2	1.6
101+00.00	50.00	0.0	0.0	8.4	0.0	2.3	1.2	10.2	7.6	0.2	7.4
101+50.00	50.00	3.5	0.0	11.2	2.5	2.3	3.2	18.1	13.5	0.0	13.5
102+00.00	50.00	1.3	0.0	14.7	2.0	4.1	4.4	24.0	18.0	0.0	18.0
102+50.00	50.00	0.0	0.4	20.2	2.9	4.4	1.2	32.3	24.2	0.4	23.8
103+00.00	50.00	0.0	0.4	24.0	4.8	7.1	0.0	40.9	30.6	0.7	29.9
103+50.00	50.00	0.0	0.4	22.3	5.4	9.5	0.0	42.9	32.1	0.7	31.4
104+00.00	50.00	0.0	0.5	12.7	3.2	8.0	0.0	32.4	24.3	0.8	23.5
104+50.00	50.00	0.0	0.5	10.5	3.3	6.0	0.0	21.5	16.1	0.8	15.3
105+00.00	50.00	0.0	0.1	12.8	3.2	6.0	0.0	21.6	16.2	0.5	15.7
105+50.00	50.00	0.0	0.6	9.9	3.3	6.0	0.0	21.0	15.7	0.7	15.0
106+00.00	50.00	0.0	0.1	12.9	3.4	6.2	0.0	21.1	15.8	0.7	15.1
106+50.00	50.00	0.0	0.0	0.0	0.0	3.2	0.0	12.0	9.0	0.1	8.9
107+00.00	50.00										
107+50.00	50.00										
108+00.00	50.00										
108+50.00	50.00										
109+00.00	50.00										
109+50.00	50.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
110+00.00	50.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
110+50.00	50.00	0.0	0.0	0.8	0.0	0.0	0.0	0.7	0.5	0.0	0.5
111+00.00	50.00	0.0	0.3	1.5	0.4	0.4	0.0	2.1	1.5	0.3	1.2
111+50.00	50.00	0.0	0.4	2.6	0.9	1.1	0.0	3.8	2.8	0.6	2.2
112+00.00	50.00	0.0	0.4	6.6	2.0	2.6	0.0	8.5	6.3	0.7	5.6
112+50.00	50.00	0.0	0.4	4.9	1.4	3.1	0.0	10.6	7.9	0.7	7.2
113+00.00	50.00	0.0	0.2	2.2	0.4	1.6	0.0	6.5	4.8	0.5	4.3
113+50.00	50.00	0.0	0.1	1.4	0.1	0.5	0.0	3.3	2.4	0.3	2.1
114+00.00	50.00	0.0	0.0	0.0	0.0	0.1	0.0	1.3	0.9	0.1	0.8
SUBTOTAL						76.8	12.3	337.3	252.0	9.0	243.0
PREVIOUS PHASE						0.0	0.0	0.0			0.0
TOTAL						76.8	12.3	337.3			243.0

STATION	INTERVAL	PR TOPSOIL (SF)	PR FILL (SF)	PR EXCAVATION (SF)	PR TOPSOIL REM (SF)	TOPSOIL EXCAVATION (CU YD)	TOPSOIL FURNISH (CU YD)	EARTH EXCAVATION (CU YD)	EXCAVATION AVAILABLE FOR EMBANKMENT (ADJ. FOR SHRINKAGE - 25%) (CU YD)	EMBANKMENT (CU YD)	EARTHWORK WASTE (+) OR SHORTAGE (-) (CU YD)
STAGE 1											
100+00.00	0.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
100+50.00	50.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
101+00.00	50.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
101+50.00	50.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
102+00.00	50.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
102+50.00	50.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
103+00.00	50.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
103+50.00	50.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
104+00.00	50.00	0.0	2.8	4.4	3.2	3.0	0.0	4.1	3.0	2.6	0.4
104+50.00	50.00	0.0	2.5	6.0	3.2	5.9	0.0	9.6	7.2	4.9	2.3
105+00.00	50.00	0.0	1.1	13.5	2.7	5.5	0.0	18.0	13.5	3.2	10.3
105+50.00	50.00	0.0	8.4	11.4	5.0	7.1	0.0	23.0	17.2	8.8	8.4
106+00.00	50.00	0.0	0.9	14.2	2.6	7.0	0.0	23.6	17.7	8.7	9.0
106+50.00	50.00	0.0	0.0	0.0	0.0	2.4	0.0	13.2	9.9	0.9	9.0
107+00.00	50.00										
107+50.00	50.00										
108+00.00	50.00										
108+50.00	50.00										
109+00.00	50.00										
109+50.00	50.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
110+00.00	50.00	0.0	0.2	5.6	0.0	0.0	0.0	5.1	3.8	0.2	3.6
110+50.00	50.00	0.0	0.1	7.6	0.0	0.0	0.0	12.1	9.0	0.2	8.8
111+00.00	50.00	0.0	1.6	11.8	0.0	0.0	0.0	17.9	13.4	1.5	11.9
111+50.00	50.00	0.0	0.1	3.8	0.3	0.3	0.0	14.3	10.7	1.6	9.1
112+00.00	50.00	0.0	0.0	5.5	0.8	1.0	0.0	8.5	6.3	0.1	6.2
112+50.00	50.00	0.0	0.0	5.3	0.7	1.4	0.0	9.9	7.4	0.0	7.4
113+00.00	50.00	0.0	0.0	0.0	0.0	0.7	0.0	4.9	3.6	0.0	3.6
113+50.00	50.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
113+60.00	10.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
SUBTOTAL						34.3	0.0	164.2	122.7	32.7	90.0
PREVIOUS PHASE						76.8	12.3	337.3			243.0
TOTAL						111.1	12.3	501.5			333.0

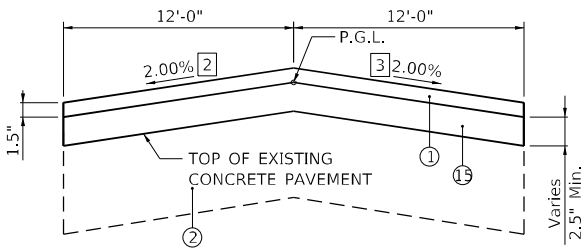
STATION	INTERVAL	PR TOPSOIL (SF)	PR FILL (SF)	PR EXCAVATION (SF)	PR TOPSOIL REM (SF)	TOPSOIL EXCAVATION (CU YD)	TOPSOIL FURNISH (CU YD)	EARTH EXCAVATION (CU YD)	EXCAVATION AVAILABLE FOR EMBANKMENT (ADJ. FOR SHRINKAGE - 25%) (CU YD)	EMBANKMENT (CU YD)	EARTHWORK WASTE (+) OR SHORTAGE (-) (CU YD)
STAGE 2											
100+00.00	0.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
100+50.00	50.00	2.3	0.2	2.0	0.0	0.0	2.1	1.9	1.4	0.2	1.2
101+00.00	50.00	1.5	0.0	1.6	0.0	0.0	3.4	3.3	2.4	0.2	2.2
101+50.00	50.00	2.1	0.0	1.3	0.0	0.0	3.2	2.7	2.0	0.0	2.0
102+00.00	50.00	2.2	2.1	0.1	0.3	0.3	3.9	1.2	0.9	2.0	-1.1
102+50.00	50.00	3.4	3.2	0.1	0.4	0.6	5.1	0.1	0.0	4.9	-4.9
103+00.00	50.00	4.8	4.2	0.0	0.6	0.9	7.5	0.1	0.0	6.9	-6.9
103+50.00	50.00	6.4	4.2	0.0	2.1	2.5	10.3	0.0	0.0	7.8	-7.8
104+00.00	50.00	6.3	7.1	0.0	2.6	4.3	11.7	0.0	0.0	10.5	-10.5
104+50.00	50.00	6.6	15.0	0.0	2.5	4.7	11.8	0.0	0.0	20.5	-20.5
105+00.00	50.00	6.9	14.8	2.8	3.5	5.5	12.4	2.6	1.9	27.5	-25.6
105+50.00	50.00	3.7	7.8	5.2	3.5	6.4	9.8	7.4	5.5	20.8	-15.3
106+00.00	50.00	4.0	5.8	5.0	3.6	6.5	7.1	9.4	7.0	12.5	-5.5
106+50.00	50.00	0.0	0.0	0.0	0.0	3.4	3.7	4.6	3.4	5.4	-2.0
107+00.00	50.00										
107+50.00	50.00										
108+00.00	50.00										
108+50.00	50.00										
109+00.00	50.00										
109+50.00	50.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
110+00.00	50.00	3.2	3.3	2.9	0.0	0.0	3.0	2.7	2.0	3.1	-1.1
110+50.00	50.00	6.5	3.9	5.0	3.9	3.6	8.9	7.3	5.4	6.7	-1.3
111+00.00	50.00	3.3	8.4	6.4	1.2	4.7	9.0	10.4	7.8	11.4	-3.6
111+50.00	50.00	3.9	9.0	1.4	1.5	2.4	6.6	7.1	5.3	16.1	-10.8
112+00.00	50.00	2.2	9.2	1.8	0.4	1.7	5.7	2.9	2.1	16.8	-14.7
112+50.00	50.00	3.1	6.5	0.2	1.6	1.8	4.9	1.8	1.3	14.6	-13.3
113+00.00	50.00	3.3	3.1	1.8	3.8	4.9	6.0	1.8	1.3	8.9	-7.6
113+50.00	50.00	3.5	0.5	1.8	3.4	6.6	6.3	3.2	2.4	3.3	-0.9
114+00.00	50.00	3.5	0.2	2.1	2.0	5.0	6.4	3.5	2.6	0.6	2.0
114+50.00	50.00	0.0	0.0	0.0	0.0	1.9	3.2	1.9	1.4	0.2	1.2
SUBTOTAL		67.7	152.0	75.9	56.1	200.9	144.8	33.0	111.1	12.3	333.0
PREVIOUS PHASE						111.1	12.3	501.5			333.0
TOTAL		178.8	164.3	577.4							188.2

STATION	INTERVAL	PR TOPSOIL (SF)	PR FILL (SF)	PR EXCAVATION (SF)	PR TOPSOIL REM (SF)	TOPSOIL EXCAVATION (CU YD)	TOPSOIL FURNISH (CU YD)	EARTH EXCAVATION (CU YD)	EXCAVATION AVAILABLE FOR EMBANKMENT (ADJ. FOR SHRINKAGE - 25%) (CU YD)	EMBANKMENT (CU YD)	EARTHWORK WASTE (+) OR SHORTAGE (-) (CU YD)
STAGE 3											
100+00.00	0.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
100+50.00	50.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
101+00.00	50.00	1.5	0.9	4.3	0.6	0.5	1.4	4.0	3.0	0.9	2.1
101+50.00	50.00	1.4	1.1	4.1	0.5	0.9	2.7	7.7	5.7	1.8	3.9
102+00.00	50.00	1.6	0.8	1.9	0.6	1.0	2.8	5.5	4.1	1.7	2.4
102+50.00	50.00	1.5	1.1	0.2	0.5	1.0	2.9	1.9	1.4	1.7	-0.3
103+00.00	50.00	2.9	2.2	0.0	0.9	1.3	4.1	0.2	0.1	3.0	-2.9
103+50.00	50.00	6.2	3.2	0.0	2.6	3.2	8.4	0.0	0.0	5.0	-5.0
104+00.00	50.00	6.7	9.1	0.0	3.0	5.2	12.0	0.0	0.0	11.4	-11.4
104+50.00	50.00	7.1	8.8	0.3	3.6	6.1	12.8	0.2	0.1	16.5	-16.4
105+00.00	50.00	7.4									



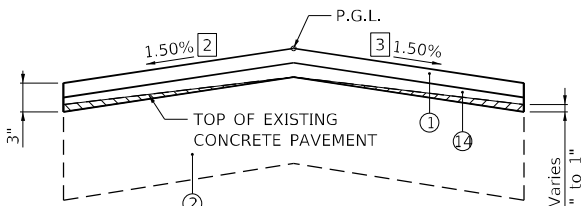
**175TH STREET- WEST APPROACH
VARIABLE DEPTH MILLING DETAIL**

STA. 103+70.00 TO 105+35.00
P.C.C. BUTT JOINT
STA 103+70.00 TO STA 104+30.00



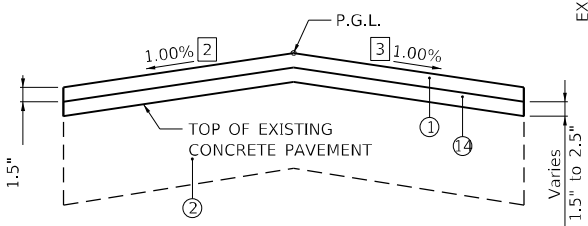
**175TH STREET- WEST APPROACH
VARIABLE DEPTH MILLING DETAIL**

STA. 105+35.00 TO 106+07.76



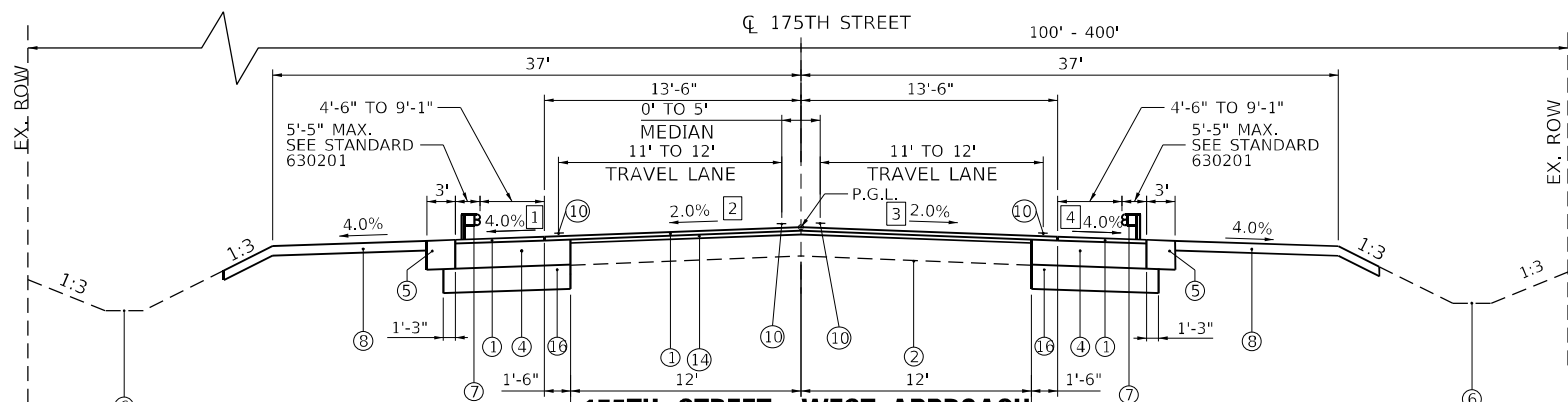
**175TH STREET- EAST APPROACH
VARIABLE DEPTH MILLING DETAIL**

STA. 109+75.49 TO 110+00.00



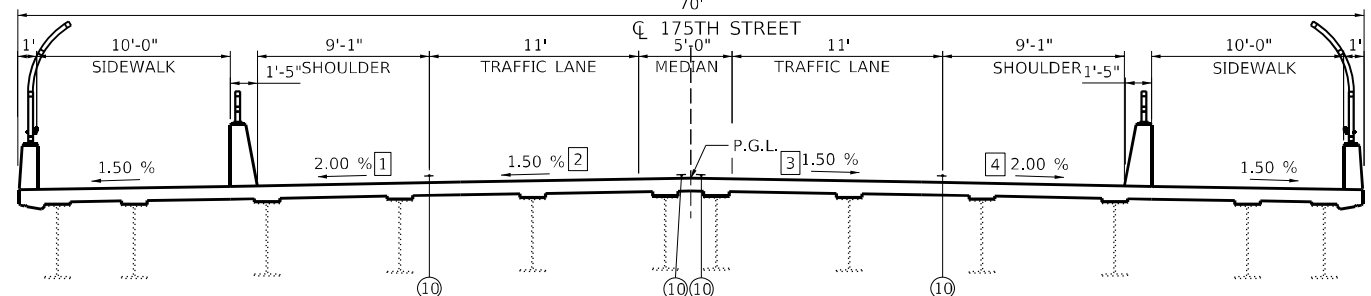
**175TH STREET- EAST APPROACH
VARIABLE DEPTH MILLING DETAIL**

STA. 110+00.00 TO 111+40.00
P.C.C. BUTT JOINT
STA 110+80.00 TO STA 111+40.00



**175TH STREET- WEST APPROACH
PROPOSED TYPICAL SECTION**

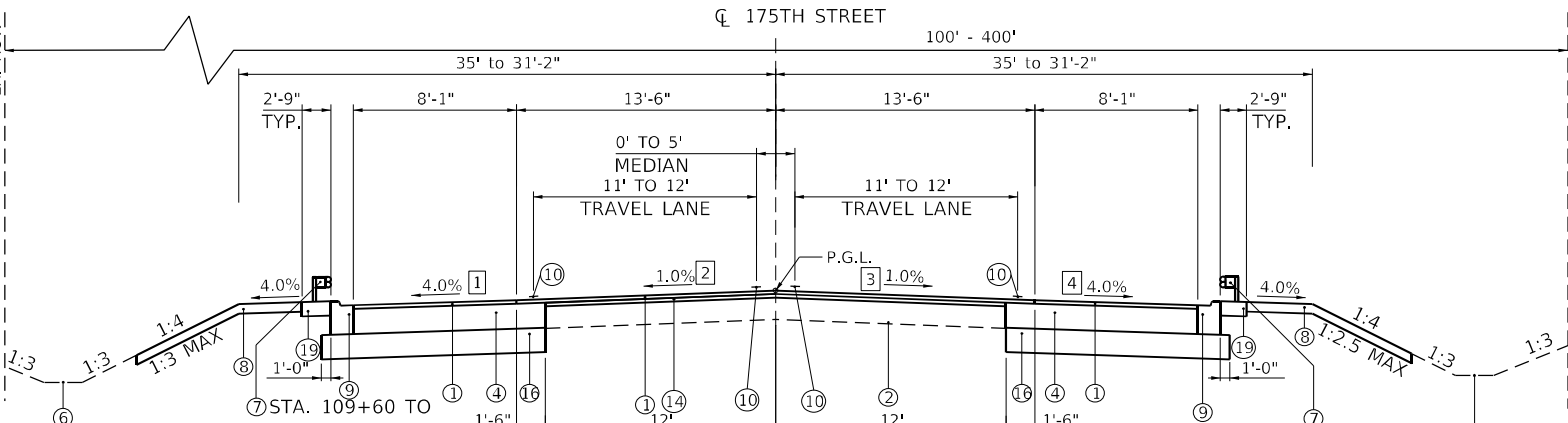
STA. 103+70.00 TO 106+28.02
PAVEMENT CONNECTOR
STA 106+07.76 TO STA 106+28.02



**175TH STREET BRIDGE
PROPOSED TYPICAL SECTION**

APPROACH SLABS
STA 106+28.02 TO STA 106+58.02 WEST
STA 109+25.23 TO STA 109+55.23 EAST

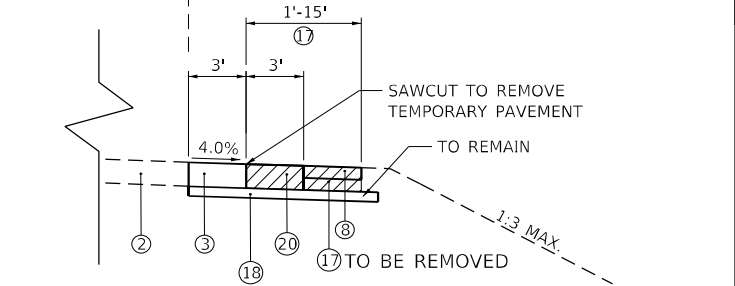
VAULTED ABUTMENTS
STA 106+58.02 TO STA 106+85.77 WEST
STA 108+91.40 TO STA 109+25.23 EAST



**175TH STREET- EAST APPROACH
PROPOSED TYPICAL SECTION**

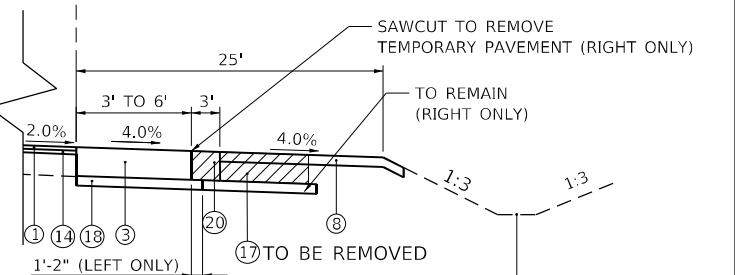
STA. 109+55.23 TO 111+40.00
PAVEMENT CONNECTOR
STA 109+55.23 TO STA 109+75.49

- LEGEND:
- ① HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 1.5"
 - ② EXISTING PCC PAVEMENT
 - ④ HMA SHOULDER, 8"
 - ⑤ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 7.5"
 - ⑥ AGGREGATE SHOULDER, TYPE B, 9"
 - ⑦ SWALE / DITCH
 - ⑧ STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
 - ⑩ EXISTING GUARDRAIL
 - ⑪ SEEDING, CLASS 2A OR SEEDING CLASS 3, TOPSOIL EXCAVATION AND PLACEMENT, 4"
 - ⑫ COMBINATION CURB AND GUTTER, TYPE B-6.12
 - ⑬ THERMOPLASTIC PAVEMENT MARKING
 - ⑭ EXISTING HMA SHOULDER
 - ⑮ EX CURB AND GUTTER
 - ⑯ EX AGGREGATE SHOULDER
 - ⑰ HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 1.5" AND VARIES
 - ⑱ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2.5" AND VARIES
 - ⑲ AGGREGATE SUBGRADE IMPROVEMENT, 12"
 - ⑳ TEMPORARY PAVEMENT
 - ㉑ SUBBASE, GRADNULAR MATERIAL, TYPE B, 4" (CA-6)
 - ㉒ HMA SHOULDER, 6"
 - ㉓ AGGREGATE SHOULDER, TYPE B, 8"
- REMOVALS (SEE REMOVAL PLAN FOR LIMITS OF REMOVALS)



**175TH STREET- WEST APPROACH
SHOULDER REPLACEMENT DETAIL**

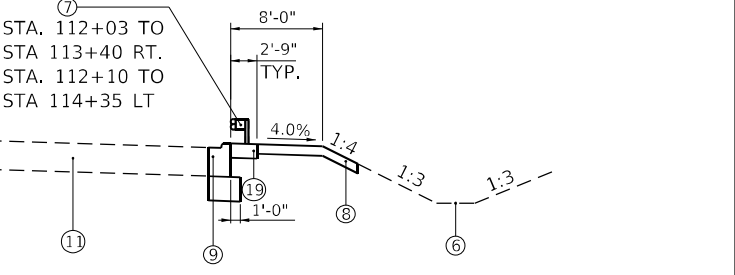
STA. 100+30.00 TO 103+70.00 LEFT (MIRROR)
STA. 100+95.00 TO 103+70.00 RIGHT



**175TH STREET- WEST APPROACH
SHOULDER RECONSTRUCTION DETAIL**

STA. 103+70.00 TO 104+90.00 (MIRROR LEFT)

STA. 112+03 TO STA 113+40 RT.
STA. 112+10 TO STA 114+35 LT



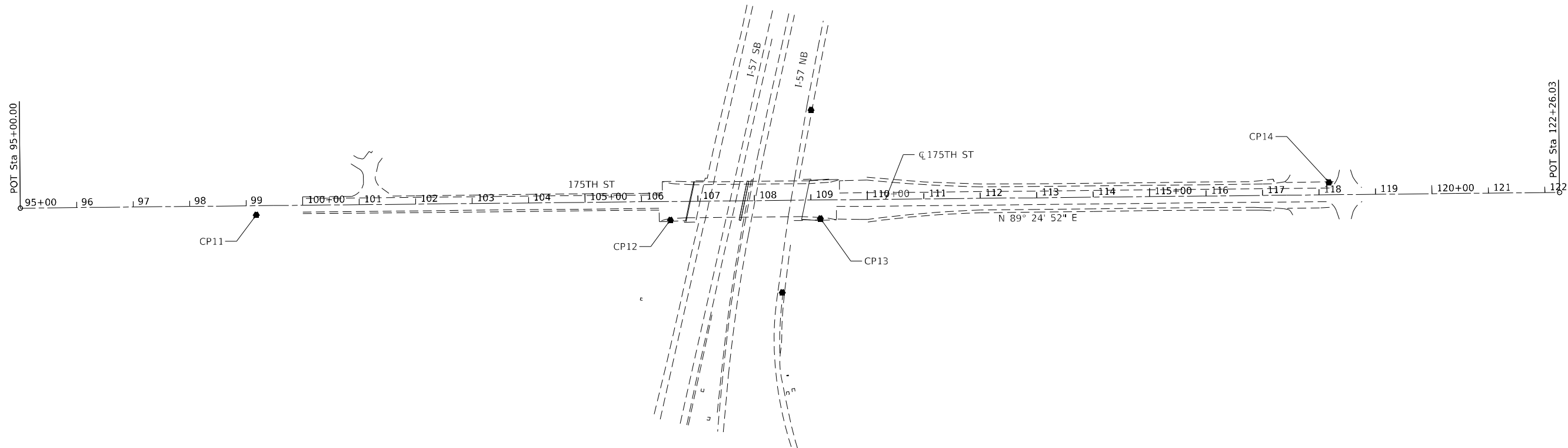
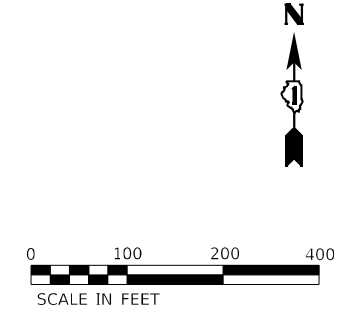
**175TH STREET- EAST APPROACH
CURB RECONSTRUCTION DETAIL**

STA. 111+40.00 TO 113+40.00 RIGHT
STA. 111+40.00 TO 114+35.00 LEFT (MIRROR)

LANE AND SHOULDER CROSS SLOPE TRANSITIONS					
STATION	LEFT SHOULDER ①	LEFT LANE ②	RIGHT LANE ③	RIGHT SHOULDER ④	NOTES
103+70.00	4.00%	0.50%	0.50%	4.00%	MATCH EXISTING
104+00.00		0.50%	0.50%		MATCH EXISTING
104+30.00		2.00%	2.00%		
105+75.00	4.00%			4.00%	
105+95.00		2.00%	2.00%		
106+05.00		1.50%	1.50%		
106+20.00	2.00%			2.00%	
109+65.00	2.00%			2.00%	
109+75.00		1.50%	1.50%		
109+85.00		1.00%	1.00%		MATCH EXISTING
110+10.00	4.00%			4.00%	MATCH EXISTING
111+40.00	4.00%	1.00%	1.00%	4.00%	MATCH EXISTING

BENCHMARK		
	ELEVATION	DESCRIPTION
CP11	719.99	MAG NAIL AT STA. 99+17.66, 16.07 RT
CP12	731.15	CUT CROSS AT STA. 106+51.03, 32.67 RT
CP13	731.81	CUT CROSS AT STA. 109+16.62, 32.77 RT
CP14	700.17	MAG NAIL AT STA. 118+17.89, 22.03 LT

ALIGNMENT COORDINATES 175TH STREET			
	STATION	N	E
POT	95+00.00	1787030.26	1144467.14
POT	122+26.03	1787058.11	1147193.03



MODEL: Default
 FILE NAME: p:\upstate\hntb\proj\Grest_Lakes\Documents\Chicago_Projects\80677_IDOT_Phase II_Various Ties_Curbs\175th Street at I-57\CAD\Drawings\102918-int-A100.dwg



USER NAME = amalla,baymundo
 PLOT SCALE = 200,0000 ' / in.
 PLOT DATE = 2/1/2022

DESIGNED - MAK
 DRAWN - MAK
 CHECKED - HER
 DATE - 1/31/2022

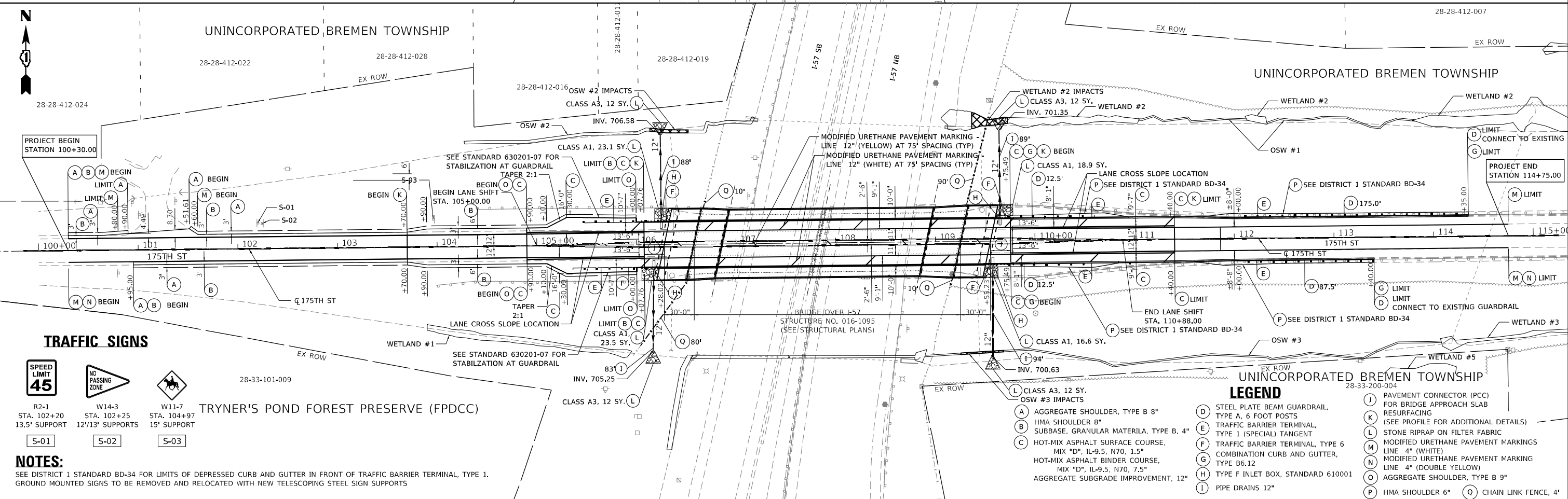
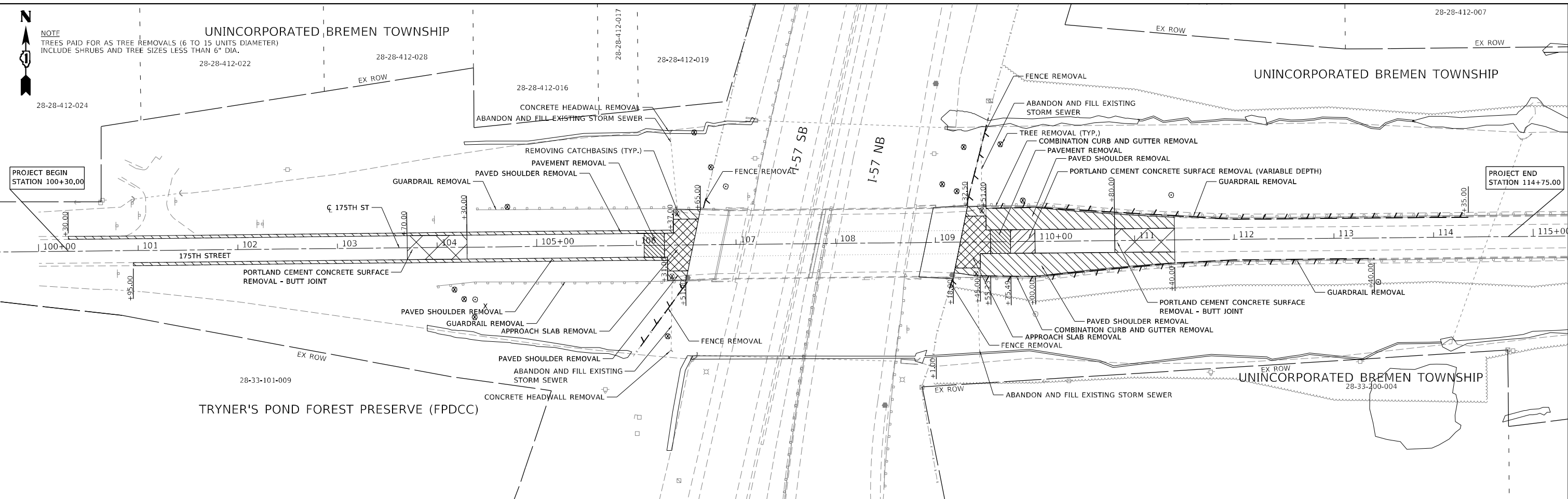
REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-57 AT 175TH STREET
ALIGNMENT, TIES, AND BENCHMARKS

SCALE: SHEET OF SHEETS STA. 95+00.00 TO STA. 122+26.03

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	13
CONTRACT NO. 62P10			ILLINOIS FED. AID PROJECT	



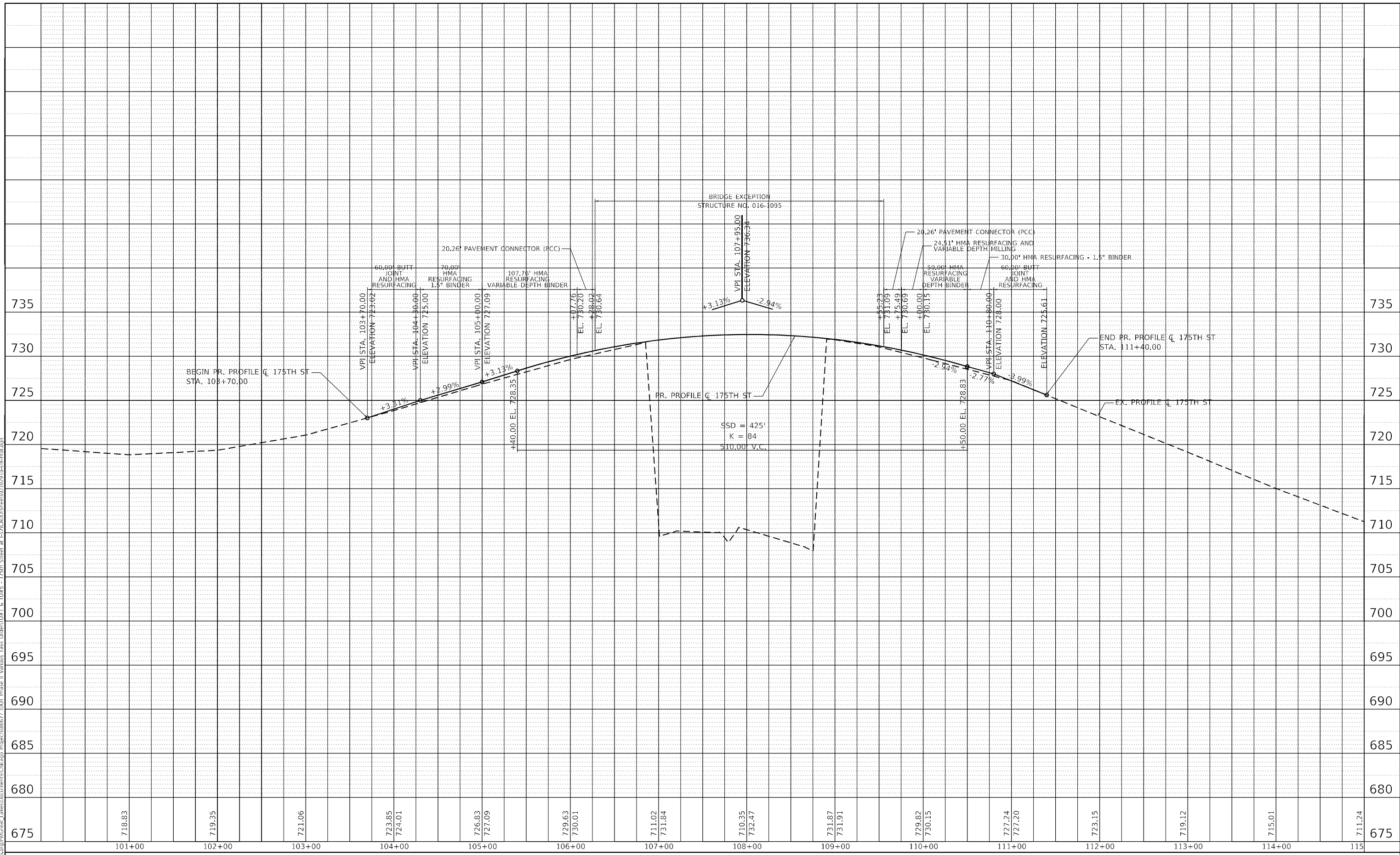
	USER NAME = amalla.baymundo DESIGNED - MAK DRAWN - MAK PLOT SCALE = 100,0000' / in. PLOT DATE = 3/15/2022	DESIGNED - MAK DRAWN - MAK CHECKED - HER DATE - 1/31/2022	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-57 AT 175TH STREET REMOVAL & PROPOSED PLANS	F.A.J. RTE. = 57 SECTION = 2021-073-BDR COUNTY = COOK ILLINOIS FED. AID PROJECT	TOTAL SHEETS = 89 SHEET NO. = 14 CONTRACT NO. 62P10
	SCALE: 50' = 1" SHEET 1 OF 1 SHEETS STA. TO STA.						
	PROJECT BEGIN STATION 100+30.00 PROJECT END STATION 114+75.00						

MODEL: Default
 FILE NAME: p:\work\project\hntb\p10\Grest_Lakes\Documents\Chicago Projects\806271.D01 - Phase II - Various Tasks - Corden\104-1 & 104-5 - 175th Street at I-57\CADD\Sheet\2021-073-BDR-14.dwg

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	ALIGNMENT CHECKED		
	NOTE BOOK		
	NO.		
	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	NOTE BOOK		
	NO.		
	STRUCTURE NOTATION		

MODEL: Default
 FILE NAME: p:\projects\hntb\proj\62p10\GreatLakes\Documents\Chicago\Project\80627\DOT Phase II\Various Task Order\T041 & T045 - 175th Street at I-57\CADD\57\Sheet\02915a-e1Prf.dwg



USER NAME = amalla.baymundo
 PLOT SCALE = 100,0000' / in.
 PLOT DATE = 2/1/2022

DESIGNED - MAK
 DRAWN - MAK
 CHECKED - HER
 DATE - 07/26/2021

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

I-57 AT 175TH STREET
 PROPOSED PROFILE

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	15
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

MAINTENANCE OF TRAFFIC GENERAL NOTES

- ALL LANES ON I-57 (11' MINIMUM) SHALL BE KEPT OPEN TO TRAFFIC DURING ALL HOURS EXCEPT HOURS AS SPECIFIED IN THE KEEPING THE EXPRESSWAYS OPEN TO THE TRAFFIC SPECIAL PROVISIONS.
- WHERE WORK AREAS ARE NOT SEPARATED FROM TRAFFIC BY TEMPORARY CONCRETE BARRIER WALL, WORK ON BOTH SIDES OF THE ROADWAY IN THE SAME DIRECTION, DURING THE SAME TIME PERIOD WILL NOT BE ALLOWED.
- "OFF PEAK" CLOSURES LISTED ON THE MOT PLAN SHEETS AND SUGGESTED STAGING SEQUENCE SHEET REFER TO ALLOWABLE HOURS LISTED IN THE SPECIAL PROVISION "KEEPING THE EXPRESSWAY OPEN TO TRAFFIC."
- ALL TRAFFIC CONTROL SIGNS AND DEVICES,WORK ZONE PAVEMENT MARKING, TEMPORARY CONCRETE BARRIERS AND ATTENUATORS NECESSARY FOR THE MAINTENANCE OF TRAFFIC, AS SHOWN ON THE PLANS,SHALL BE IN PLACE BEFORE THE START OF ANY CONSTRUCTION STAGE.
- ALL CONSTRUCTION WARNING SIGNS SHALL BE BLACK LEGEND ON ORANGE BACKGROUND. ALL CONSTRUCTION WARNING SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE SHOWN ON THE PLANS.
- ALL "ROAD CONSTRUCTION AHEAD" WARNING SIGNS SHALL BE EQUIPPED WITH TYPE A MONO-DIRECTIONAL FLASHING LIGHT.
- THE CONTRACTOR SHALL PLACE CHANGEABLE MESSAGE SIGN(S)TO INFORM MOTORISTS OF UPCOMING CONSTRUCTION ACTIVITIES. THE LOCATION SHALL BE AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER. THE MESSAGE SIGN SHALL BE IN PLACE TWO WEEKS PRIOR TO START OF CONSTRUCTION ACTIVITIES.THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CALENDAR DAYS FOR "CHANGEABLE MESSAGE SIGN".
- THE WORK REQUIRED TO COMPLY WITH THE MAINTENANCE OF TRAFFIC GENERAL NOTES SHALL BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR TRAFFIC CONTROL AND PROTECTION(EXPRESSWAYS),AND TRAFFIC CONTROL AND PROTECTION (SPECIAL),EXCEPT FOR PAY ITEMS WHICH HAVE BEEN ESTABLISHED IN THE CONTRACT AND ARE SHOWN ON THE PLANS.
- EXISTING PAVEMENT MARKINGS AND RAISED REFLECTIVE MARKERS THAT CONFLICT WITH TEMPORARY MARKINGS SHALL BE REMOVED.
- WORKERS SIGN SHALL BE USED ONLY WHEN WORKERS ARE PRESENT. FLAGGER SIGN SHALL BE USED ONLY WHEN FLAGGERS ARE PRESENT.
- THE APPROVAL OF THE ENGINEER SHALL BE REQUIRED FOR ALL TEMPORARY TRAFFIC BARRIER WALL OPENINGS NOT SHOWN ON THE PLANS. THE APPROACH END OF TEMPORARY TRAFFIC BARRIER SHALL BE PROTECTED WITH AN NCHRP 350 APPROVED END TREATMENT. SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS SHALL REQUIRE ADHERANCE TO DISTRICT 1 DETAIL TC-18.THE WORK REQUIRED TO PROVIDE ADDITIONAL TEMPORARY TRAFFIC BARRIER OPENINGS FOR CONTRACTOR'S ACCESS TO WORK AREAS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR TRAFFIC CONTROL AND PROTECTION (EXPRESSWAY), AND TRAFFIC CONTROL AND PROTECTION (SPECIAL).
- THE CONTRACTOR SHALL CONTACT THE IDOT ARTERIAL TRAFFIC SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- DROP OFF (BETWEEN TRAVEL LANES) ALONG ARTERIAL OR LOCAL ROADWAYS WITHIN PROJECT LIMITS, GREATER THAN 1" AND LESS THAN OR EQUAL TO 2 1/2" LIFT DIFFERENCE OR GREATER THAN 1 1/2" AND LESS THAN OR EQUAL TO 4" VERTICAL MILLED FACE SHALL REQUIRE UNEVEN LANE SIGNS PLACED ONE HALF MILE APART AS DIRECTED BY THE ENGINEER.
- BARRIER WALL MARKERS SHALL BE INSTALLED ON BOTH THE FACE OF THE TEMPORARY CONCRETE BARRIER WALL NEXT TO TRAFFIC,AND THE TOP OF SECTIONS OF THE TEMPORARY CONCRETE BARRIER WALL AS SHOWN IN STANDARD 704001. THE COLOR OF THESE REFLECTORS SHALL MATCH THE COLOR OF THE EDGELINES (YELLOW ON THE LEFT AND CRYSTAL OR WHITE ON THE RIGHT). IF THE BASE OF THE TEMPORARY CONCRETE BARRIER WALL IS 12 INCHES OR LESS FROM THE TRAVEL LANE,THEN THE LOWER SLOPE OF THE WALL SHALL ALSO HAVE A 6 INCH WIDE TEMPORARY PAVEMENT MARKING EDGELINE (YELLOW ON THE LEFT AND WHITE ON THE RIGHT). TEMPORARY PAVEMENT MARKING ON THE LOWER SLOPE OF THE TEMPORARY CONCRETE BARRIER WALL WILL BE MEASURED AND PAID FOR AS TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE III TAPE.
- ACCESS TO PRIVATE PROPERTIES SHALL BE PROVIDED AT ALL TIME DURING CONSTRUCTION.
- TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY LANE AND SHOULDER CLOSURES SHALL IN ACCORDANCE WITH THE APPLICABLE HIGHWAY STANDARD OR DISTRICT DETAIL FOR THE PROPOSED WORK.
- UNLESS SPECIFIED IN THE CONTRACT PLANS, ALL TRAFFIC LANES SHALL BE KEPT OPEN TO TRAFFIC DURING PEAK TRAFFIC VOLUME HOURS AS DEFINED IN THE CONTRACT SPECIAL PROVISION FOR KEEPING THE EXPRESSWAY OPEN TO TRAFFIC.

MAINTENANCE OF TRAFFIC GENERAL NOTES (CONT.)

- THE CONTRACTOR WILL BE ALLOWED TO IMPLEMENT TEMPORARY LANE CLOSURES, TEMPORARY SHOULDER CLOSURES,TEMPORARY RAMP CLOSURES (FULL OR PARTIAL)ACCORDING TO THE APPLICABLE IDOT HIGHWAY STANDARDS AND DISTRICT ONE DETAILS,AND PER THE ALLOWABLE HOURS IN THE CONTRACT SPECIAL PROVISION FOR KEEPING THE EXPRESSWAY OPEN TO TRAFFIC.
- THE OPEN TRAFFIC LANE ADJACENT TO TEMPORARY WIDENING OR PROPOSED ROADWAY SHALL BE CLOSED TO TRAFFIC DURING CONSTRUCTION WORK THAT RESULTS IN A DROP OFF OF 12" OR GREATER ADJACENT TO THE OPEN TRAFFIC LANE.
- DURING CONSTRUCTION OF PROPOSED IMPROVEMENTS ALONG COOK COUNTY HIGHWAYS, TWO-WAY TRAFFIC SHALL REMAIN OPEN FOR ALL TRAFFIC AT ALL TIMES. IF ANY ACTIVITY REQUIRES ENCROACHMENT INTO A LANE OPEN FOR TRAFFIC, THAT ACTIVITY SHALL BE RESTRICTED TO WITHIN THE HOURS OF 9:00AM TO 3:00PM FOLLOWING THE APPLICABLE IDOT AND IDOT-DISTRICT 1 TRAFFIC CONTROL STANDARDS INCLUDED IN THE PLANS FOR OFF-ROAD AND ON-ROAD APPLICATIONS.
- IF ANY EXISTING PAVEMENT MARKING AND/OR SIGNING ALONG COOK COUNTY ROADWAYS IS DAMAGED DUE TO CONSTRUCTION OF PROPOSED IMPROVEMENTS, THE CONTRACTOR SHALL REPLACE THE DAMAGED TRAFFIC CONTROL DEVICES TO THE SATISFACTION OF COOK COUNTY PER IDOT AND IDOT DISTRICT 1 STANDARDS FOR TRAFFIC CONTROL DEVICES, PAVEMENT MARKING, AND SIGNAGE INCLUDED IN THE PLANS.
- THE CONTRACTOR MAY ELECT TO PLACE THE FINAL SURFACE COURSE AFTER STAGE 3. IF THE SURFACE COURSE IS PLACED AFTER STAGE 3, THE CONTRACTOR SHALL ENSURE SMOOTH TRANSITIONS BETWEEN THE BINDER COURSE AND THE BRIDGE CONNECTOR PAVEMENT DURING CONSTRUCTION. THE COST OF PHASING THE WORK IN THIS MANNER SHALL BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL).

SUGGESTED STAGING AND TRAFFIC CONTROL

175TH STREET
PRE-STAGE
 CONSTRUCTION:
 RECONSTRUCT SHOULDER AND PLACE TEMPORARY WIDENING ALONG 175TH STREET UTILIZING HIGHWAY STANDARDS 701326-04 AND 701201-05.

TRAFFIC CONTROL:
 TRAFFIC IS TO REMAIN IN THE EXISTING CONFIGURATION EXCEPT DURING FLAGGING OPERATION.

STAGE 1
 CONSTRUCTION:
 THE SOUTH SIDE AND 3' OF THE NORTH SIDE OF THE BRIDGE AND ROADWAY WILL BE RECONSTRUCTED EXCEPT FOR THE AGGREGATE SHOULDERS AND THE CURB AND GUTTER ON THE WEST AND EAST SIDE OF THE BRIDGE, RESPECTIVELY. THE TEMPORARY PAVEMENT FOR STAGE 2 WILL BE CONSTRUCTED.

TRAFFIC CONTROL:
 ONE THROUGH LANE IN EACH DIRECTION WILL BE PROVIDED. TRAVEL LANES WILL BE SHIFTED TO THE NORTH SIDE OF 175TH STREET UTILIZING EXISTING SHOULDERS AND TEMPORARY PAVEMENT.

STAGE 2
 CONSTRUCTION:
 THE REMAINING NORTH SIDE OF THE BRIDGE AND ROADWAY WILL BE RECONSTRUCTED AND GRADED. THE TEMPORARY PAVEMENT FROM STAGE 1 WILL BE REMOVED. A 3-FOOT WIDE WIDTH OF SHOULDER WILL REMAIN FROM 100+30.00 TO 103+70.00.

TRAFFIC CONTROL:
 ONE THROUGH LANE IN EACH DIRECTION WILL BE PROVIDED. TRAVEL LANES WILL BE SHIFTED TO THE SOUTH SIDE OF 175TH STREET UTILIZING PROPOSED AND TEMPORARY PAVEMENT CONSTRUCTED IN STAGE 1.

STAGE 3
 CONSTRUCTION:
 THE REMAINING WORK ON THE SOUTH SIDE THE ROADWAY WILL BE CONSTRUCTED AND GRADED. THIS WORK SHALL INCLUDE THE PAVED SHOULDER, AGGREGATE SHOULDERS, CURB AND GUTTER, AND GUARDRAIL INSTALLATION.

TRAFFIC CONTROL:
 ONE THROUGH LANE IN EACH DIRECTION WILL BE PROVIDED. TRAVEL LANES WILL BE IN THEIR FINAL CONFIGURATION. TEMPORARY TRAFFIC BARRIER SHALL BE PLACED 2' FROM THE EASTBOUND EDGE OF TRAVEL. THE LENGTH TO RELOCATED FROM STAGE 2 IS 1137.5 FEET AND SHALL BEGIN AT STA. 102+90. TWO IMPACT ATTENUATOR, TEMPORARY, (NON-REDIRECTIVE, NARROW) TEST LEVEL 3 SHALL BE PLACED AT EACH END OF THE TEMPORARY TRAFFIC BARRIER. MAINTENANCE OF TRAFFIC SIGNS SHALL CONFORM TO HIGHWAY STANDARD 701101. WORK IN THIS PHASE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (SPECIAL).

STAGE 4
 CONSTRUCTION:
 REMAINING TEMPORARY PAVEMENT SHALL BE REMOVED UTILIZING HIGHWAY STANDARD 701326-04. A 3-FOOT WIDE WIDTH OF SHOULDER WILL REMAIN FROM 100+30.00 TO 103+70.00 TO REPLACE THE EXISTING SHOULDER REMOVED IN PREVIOUS PHASES.

TRAFFIC CONTROL:
 TRAFFIC IS TO REMAIN IN THE EXISTING CONFIGURATION EXCEPT DURING FLAGGING OPERATION.

SUGGESTED STAGING AND TRAFFIC CONTROL

INTERSTATE 57 STAGING

STAGE 1

CONSTRUCTION: INSTALLATION OF NEW PIER MOUNTED LIGHTING UNIT BY INSIDE LANES.

TRAFFIC CONTROL: UTILIZE HIGHWAY STANDARD 701400 AND 701401 FOR NIGHT TIME CLOSURE OF INSIDE SHOULDER AND LANE FOR NB AND SB I-57.

STAGE 2

CONSTRUCTION: INSTALLATION OF ABUTMENT LIGHTING UNIT ON OUTSIDE LANES.

TRAFFIC CONTROL: UTILIZE HIGHWAY STANDARD 701400 AND 701401 FOR NIGHT TIME CLOSURE OF OUTSIDE SHOULDER AND LANE ON NB AND SB I-57 AND HIGHWAY STANDARD 701411 SHOULD BE UTILIZED TO SHIFT RAMP TRAFFIC TO OPEN LANES FOR THE WB I-80 TO NB I-57 ON-RAMP.

STAGE 3

CONSTRUCTION: REMOVAL OF EXISTING LIGHTING.

TRAFFIC CONTROL: UTILIZE HIGHWAY STANDARD 701400 AND 701446 FOR NIGHT TIME CLOSURE OF INSIDE SHOULDER, LANE 1, LANE 2 FOR NB AND SB I-57.

STAGE 4

CONSTRUCTION: REMOVAL OF EXISTING LIGHTING.

TRAFFIC CONTROL: UTILIZE HIGHWAY STANDARD 701400 AND 701401 FOR NIGHT TIME CLOSURE OF OUTSIDE SHOULDER AND LANE ON NB AND SB I-57. HIGHWAY STANDARD 701411 SHOULD BE UTILIZED TO SHIFT RAMP TRAFFIC TO OPEN LANES FOR THE WB I-80 TO NB I-57 ON-RAMP.

MODEL: Default
 FILE NAME: P:\updown\hntb\comp\PIUG\cast_L\skel\Documents\Chicgo Projects\80677 IDOT Phase II Various Task_Curbs\TOD-1 & TOD-5 - 175th Street at I57\CAD\57\Sheets\022018-int-staging-gennotes.dwg



USER NAME = amalla.baymundo	DESIGNED - MAK	REVISED -
	DRAWN - MAK	REVISED -
PLOT SCALE = 2,0000 ' / in.	CHECKED - HER	REVISED -
PLOT DATE = 3/17/2022	DATE - 1/31/2022	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

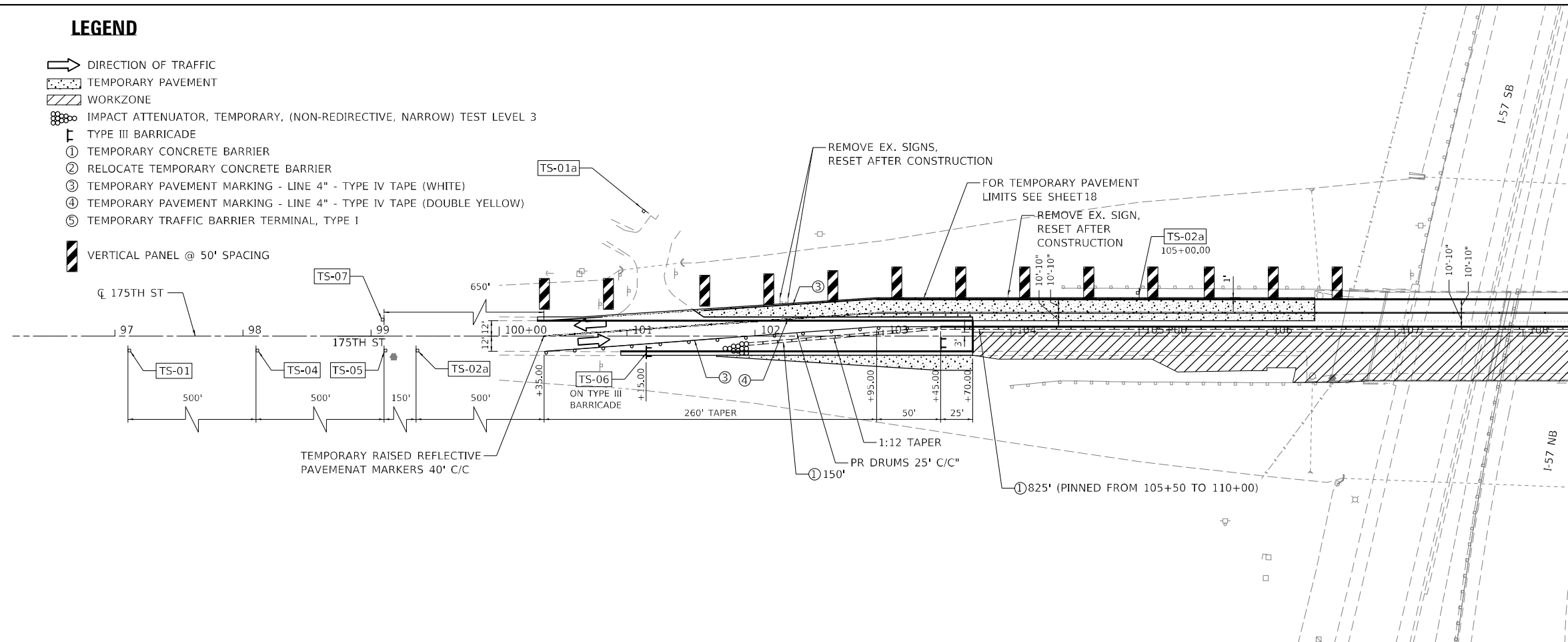
I-57 AT 175TH STREET
 MAINTENANCE OF TRAFFIC GENERAL NOTES

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	16
				CONTRACT NO. 62P10
		ILLINOIS	FED. AID PROJECT	

LEGEND

- ➔ DIRECTION OF TRAFFIC
- ▨ TEMPORARY PAVEMENT
- ▨ WORKZONE
- ⊗ IMPACT ATTENUATOR, TEMPORARY, (NON-REDIRECTIVE, NARROW) TEST LEVEL 3
- ⊥ TYPE III BARRICADE
- ① TEMPORARY CONCRETE BARRIER
- ② RELOCATE TEMPORARY CONCRETE BARRIER
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (WHITE)
- ④ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (DOUBLE YELLOW)
- ⑤ TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE I
- ▨ VERTICAL PANEL @ 50' SPACING



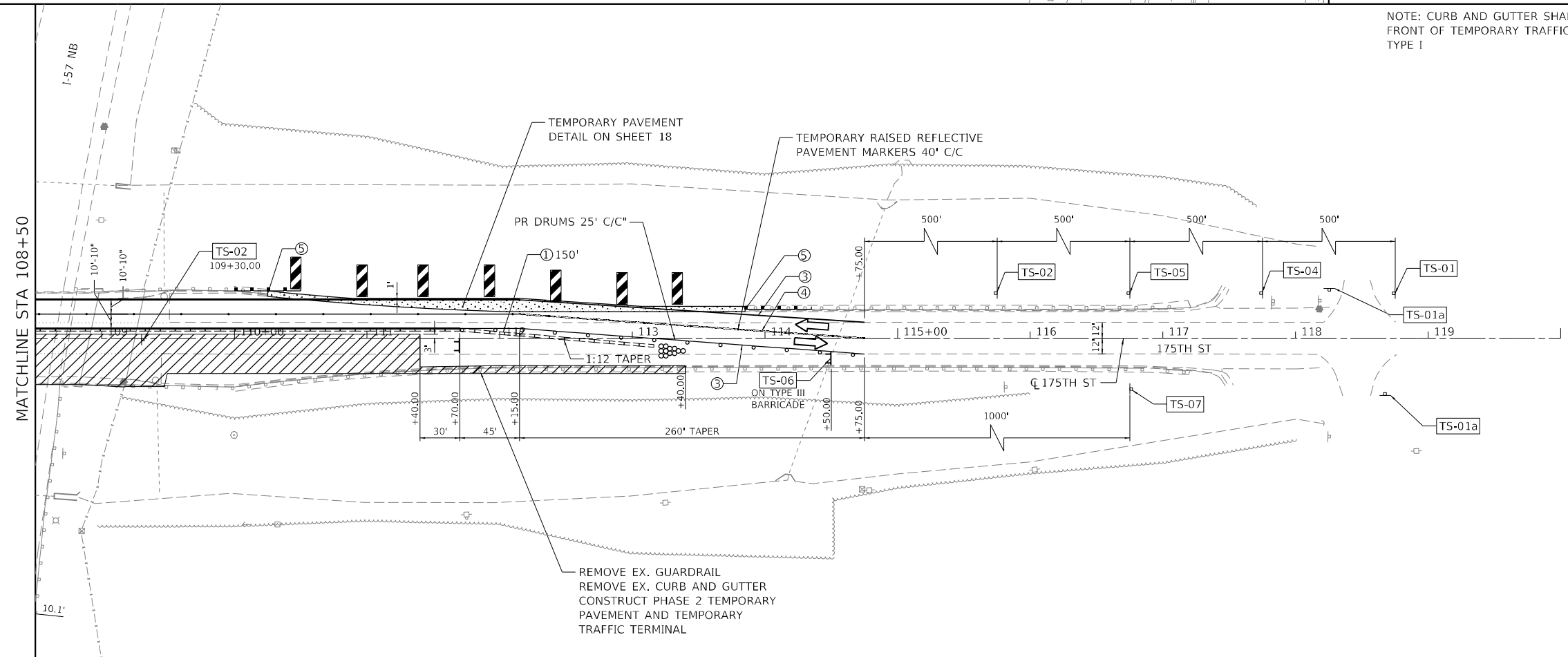
0 50 100 150
SCALE IN FEET

MAINTENANCE OF TRAFFIC SIGNS

TS-01		W20-1103(0)-48
TS-01a		W20-1103(0)-48
PLACE 200' PRIOR TO INTERSECTION		M6-1(0)-2115 (LEFT OR RIGHT)
TS-02		W1-4R(0)-48
		W13-1(0)-2424
TS-02a		W1-4L(0)-48
		W13-1(0)-2424
TS-03		W21-5aR(0)-48
TS-04		W12-1102(0)-48
TS-05		W21-1115(0)-3618 R2-1-3648 R2-1106p-3618
TS-06		W1-6(0)-6030 (LEFT OR RIGHT)
TS-07		G20-1103(0)-6036

MATCHLINE STA 108+50

NOTE: CURB AND GUTTER SHALL BE REMOVED IN FRONT OF TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE I



MODEL: Default
FILE: hntb\p\work\hntb\proj\G2011\DOT Phase II Varies Task_Curbs\DOT11 & TD045 - 175th Street at I-57\CD05\Sheet\102118-int-sta108+50-175th-phase1.dgn



USER NAME = amalla.baymundo	DESIGNED - MAK	REVISED -
	DRAWN - MAK	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - HER	REVISED -
PLOT DATE = 3/15/2022	DATE - 1/31/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-57 AT 175TH STREET
175TH STREET STAGE 1**

SCALE: 50' = 1" SHEET 1 OF 2 SHEETS STA. 96+00.00 TO STA. 120+00

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	19
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

EROSION AND SEDIMENT CONTROL GENERAL NOTES

1. THE WORK DESCRIBED ON THESE DRAWINGS IS AN INTEGRAL PART OF THE STORM WATER POLLUTION PREVENTION PLAN USED TO OBTAIN A NPDES PERMIT FROM IEPA FOR THE CONSTRUCTION OF THIS PROJECT.
2. THE PURPOSE OF THE EROSION AND SEDIMENT CONTROL MEASURES INCLUDED FOR THIS PROJECT IS TO LIMIT THE SEDIMENT POLLUTION IMPACT OF ANY STORM WATER DISCHARGE THAT ORIGINATE ON THIS SITE OR OFF-SITE FLOWS THAT FLOW OVER THE DISTURBED AREAS, ON DOWNSTREAM AREAS.
3. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO INSURE THAT SEDIMENT TRANSPORT OFF THE SITE IS REDUCED BY A COMBINATION OF MINIMIZATION OF EROSION AT THE SOURCE AND INSTALLATION OF SPECIFIC MEASURES TO CONTROL OR REDUCE THE TRANSPORT OF SEDIMENT. A COPY OF THE EROSION AND SEDIMENT CONTROL SCHEDULE BEING IMPLEMENTED BY THE CONTRACTOR WILL BE ON THE CONSTRUCTION SITE AT ALL TIMES.
4. ALL RUNOFF ORIGINATING ON DISTURBED AREAS ASSOCIATED WITH THIS PROJECT WILL PASS THROUGH ONE OR MORE MEASURES THAT WILL MINIMIZE THE OFF-SITE SEDIMENT IMPACTS OF THE CONSTRUCTION ACTIVITY.
5. ALL PERMANENT SEDIMENT BASINS, PERMANENT STORM WATER CONTROL MEASURES, AND RUNOFF CONTROL MEASURES REQUIRED TO KEEP OFF-SITE RUNOFF FROM FLOWING OVER THE CONSTRUCTION AREA WILL BE INSTALLED BEFORE CLEARING AND STRIPPING OF THE SITE PROCEEDS. PRIOR TO PROCEEDING WITH GENERAL EARTHWORK ON A PROJECT THE CONTRACTOR WILL OBTAIN APPROVAL OF HIS PROPOSED EARTHWORK AND STABILIZATION SCHEDULE.
6. EXCAVATION TO BE USED FOR EMBANKMENTS SHALL NOT BE STOCKPILED UNLESS PERIMETER CONTROLS ARE UTILIZED. WHEN THIS MATERIAL IS STOCKPILED FOR THE CONVENIENCE OF THE CONTRACTOR THE COST OF THE CONTROLS ARE BORNE BY THE CONTRACTOR. IF THE MATERIAL IS STOCKPILED AT THE DIRECTION OF THE ENGINEER THE DEPARTMENT WILL ASSUME THE COSTS OF THE CONTROLS.
 - (A) WHEN BALANCING EARTHWORK (BORROW FROM A CUT USED AS FILL AT A LOCATION DISTANT FROM THE CUT) THE ENGINEER WILL CONSIDER ALLOWING MORE THAN 10 ACRES OF GRADING AT A TIME. THE 10 ACRE LIMITATION DOES NOT INCLUDE HAUL ROADS, RIDGE CONSTRUCTION WORK AREAS AND STORAGE AREAS.
 - (B) VARIATIONS TO THE ABOVE MAY BE CONSIDERED BY THE ENGINEER UNDER ALL THE FOLLOWING CONDITIONS:
 - * IF THE CONTRACTOR FALLS BEHIND SCHEDULE THROUGH NO FAULT OF HIS OWN.
 - * THE CONTRACTOR MUST PRESENT A SCHEDULE DEMONSTRATING THE NEED FOR SUCH VARIATION IN ORDER TO COMPLETE THE WORK ON TIME.
 - * THE CONTRACTOR MUST COMPLY WITH ALL OTHER CONTRACT REQUIREMENTS.
7. DISTURBED AREAS ARE TO BE PROTECTED FROM EROSION IN A TIMELY MANNER. UPON COMPLETION OF GRADING OR CONSTRUCTION, THE AREA WILL BE STABILIZED (USING PERMANENT MEASURES WHEN POSSIBLE) WITHIN 7 CALENDAR DAYS. TEMPORARY STABILIZATION THROUGH USE OF GROUND COVER, MULCHING, OR OTHER APPROVED MEASURES WILL BE INSTALLED WHENEVER SITE DEVELOPMENT WORK, GRADING OR OTHER EARTH DISTURBING ACTIVITIES CEASE TO BE CONTINUOUS FOR A PERIOD EXCEEDING 14 CALENDAR DAYS. THE 7/14 DAY REQUIREMENT IS TAKEN TO MEAN THAT THE STABILIZATION OPERATION IS COMPLETE OR NEARING COMPLETION IN THE DEFINED TIME.
8. ALL EROSION AND SEDIMENT CONTROL MEASURES WILL BE MAINTAINED IN ACCORDANCE WITH THE IDOT EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION FOUND ON THE CONSTRUCTION TAB AT:
(<http://www.idot.illinois.gov/transportation-system/environment/erosion-and-sediment-control>).
9. THE CONTRACTOR WILL ASSUME RESPONSIBILITY FOR MAINTENANCE OF ALL SOIL EROSION CONTROL DURING CONSTRUCTION.
10. THE CONTRACTOR SHALL CHECK EROSION AND SEDIMENT CONTROL MEASURES WEEKLY AND AFTER EACH RAINFALL, 0.5 INCHES OR GREATER IN A 24 HOUR PERIOD, OR EQUIVALENT SNOWFALL. ADDITIONALLY, DURING THE WINTER MONTHS, ALL MEASURES SHOULD BE CHECKED BY THE CONTRACTOR AFTER EACH SIGNIFICANT SNOWMELT.
11. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER
12. UNDER NO CIRCUMSTANCE SHALL THE CONTRACTOR PROLONG GRADING AND SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDING AT ONE TIME.
13. EROSION CONTROL ITEMS ARE CONSIDERED TO BE A HIGH PRIORITY ON THIS CONTRACT. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE RE.
14. DITCHES, PERIMETER EROSION BARRIER, INLET FILTERS, ETC. SHALL BE MAINTAINED DURING THE CONSTRUCTION SEASON AS WELL AS THE WINTER MONTHS AND OTHER TIMES WHEN THE PROJECT IS CLOSED DOWN. PERIMETER EROSION BARRIER AND INLET FILTERS WILL BE CLEANED WHEN THEY ARE 50% FILLED, THESE SPOILS WILL BE REMOVED TO AN APPROVED SITE.
15. WHEN THE CONTRACTOR REQUESTS A CHANGE TO POSTPONE COMPLETION OF THE EXCAVATION OF A SPECIFIC AREA AS A CONTINUOUS OPERATION AND PLACING THE TOPSOIL AS DEFINED IN THE STANDARD SPECIFICATIONS, THE ENGINEER MAY ALLOW THE CONTRACTOR TO STABILIZE THE AREA USING TEMPORARY STABILIZATION WITH STRAW MULCH PROVIDING THE FOLLOWING CONDITIONS ARE MET:
 - (A) ALL AREAS BEING STABILIZED ARE 3:1 SLOPES OR FLATTER.
 - (B) THE CONTRACTOR BEARS THE COST OF PREPARING THE SEED BED AND STABILIZING THE AREA WITH TEMPORARY STABILIZATION WITH STRAW MULCH.
 - (C) ALL REQUIRED SEDIMENT CONTROL MEASURES FOR THE SECTION OF ROAD IN QUESTION HAVE BEEN INSTALLED AND ARE BEING MAINTAINED.
16. INLET FILTERS SHALL BE APPLIED AT EACH EXISTING AND PROPOSED INLET.
17. AVOID USING THE INLET AND PIPE PROTECTION SHOWN ON THE HIGHWAY STANDARD SHEETS 280001. STRAW BALES AND SILT FENCE SHOULD NOT BE USED AS INLET AND PIPE PROTECTION. INLET AND PIPE PROTECTION SHOULD BE COMPRISED OF DITCH CHECKS, TEMPORARY SEEDING AND TEMPORARY EROSION CONTROL BLANKET AND WILL BE INSTALLED AT ALL STORM SEWER AND CULVERTS. INLET FILTERS, AS SPECIFIED IN ARTICLE 1081.15(H) OF THE STANDARD SPECIFICATIONS (CURRENT EDITION) WILL BE INSTALLED AT ALL INLETS, CATCH BASINS, AND MANHOLES FOR THE DURATION OF CONSTRUCTION. INLET FILTERS WILL BE CLEANED ON A REGULAR BASIS. ENSURE PROPER QUANTITIES OF INLET FILTERS. DITCH CHECKS, TEMPORARY SEEDING AND TEMPORARY EROSION CONTROL BLANKET ARE INCLUDED IN THE CONTRACT
18. THE CONTRACTOR SHOULD PROVIDE TO THE RE A PLAN TO ENSURE THAT A STABILIZED FLOW LINE WILL BE PROVIDED DURING STORM SEWER CONSTRUCTION. THE USE OF A STABILIZED FLOW LINE BETWEEN INSTALLED STORM SEWER AND OPEN DISTURBANCE WILL REDUCE THE POTENTIAL FOR THE OFFSITE DISCHARGE OF SEDIMENT BEARING WATERS, PARTICULARLY WHEN RAIN IS FORECASTED SO THAT FLOW WILL NOT ERODE. LACK OF AN APPROVED PLAN OR FAILURE TO COMPLY WILL RESULT IN AN ESC DEFICIENCY DEDUCTION.
19. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL.
20. WETLANDS NO INTRUSION" SIGNAGE SHOULD ALSO BE PROVIDED AT THE BOUNDARY OF ALL UN-IMPACTED WETLANDS AND/OR WOUS. THE CONTRACTOR CAN BORROW THE SIGNS FROM THE BUREAU OF MAINTENANCE. INCLUDE TEMPORARY FENCING AND WETLAND SIGNAGE WITHIN EROSION AND SEDIMENT CONTROL STRATEGY.
21. THIS PROJECT REQUIRES A US ARMY CORPS OF ENGINEERS (USACE) 404 PERMIT THAT WILL BE SECURED BY THE DEPARTMENT. ALL CONDITIONS OF THE 404 PERMIT, FOUND IN THE SPECIAL PROVISIONS, MUST BE FOLLOWED. AS A CONDITION OF THIS PERMIT, THE CONTRACTOR WILL NEED TO SUBMIT AN IN-STREAM WORK PLAN (INCLUDING WORK WITHIN WETLANDS) TO THE DEPARTMENT FOR APPROVAL. GUIDELINES ON ACCEPTABLE IN-STREAM WORK TECHNIQUES (INCLUDING WORK WITHIN WETLANDS) CAN BE FOUND ON THE USACE WEBSITE. THE USACE DEFINES AND DETERMINES IN-STREAM WORK. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT AN IN-STREAM WORK PLAN (INCLUDING WORK WITHIN WETLANDS) WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. WITH THE EXCEPTION OF COFFERDAMS WHICH WILL BE PAID FOR AS COFFERDAM (TYPE 1) (IN-STREAM /WETLAND WORK) WITH A BASIS OF PAYMENT OF EACH.

MODEL: Default
 FILE: \\bmlf-pub\up\mnt\hnb\pmp\UG\Crat Projects\80677 IDOT Phase II Various Tech Corden\T04-I & T04-S - 175th Street at 157\CAD\DWG\Sheet\022918-ent-eros01.dwg



USER NAME = amalla.baymundo	DESIGNED - AMB	REVISED -
	DRAWN - AMB	REVISED -
PLOT SCALE = 40,0000 * / in.	CHECKED - MM	REVISED -
PLOT DATE = 2/4/2022	DATE - 01/31/2022	REVISED -

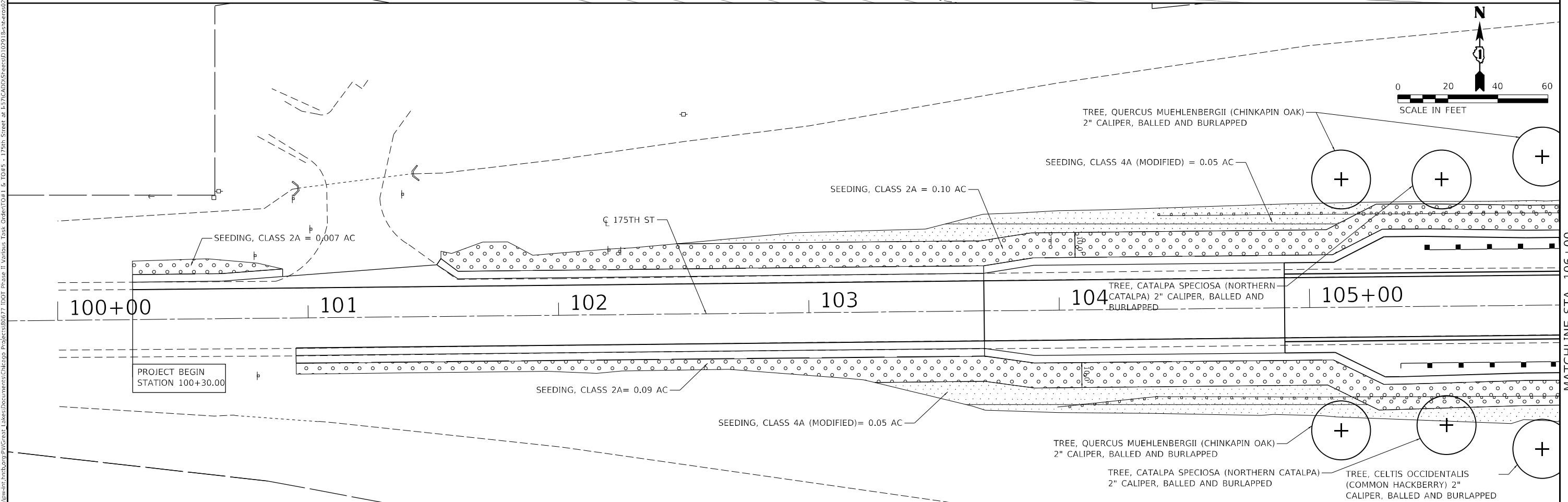
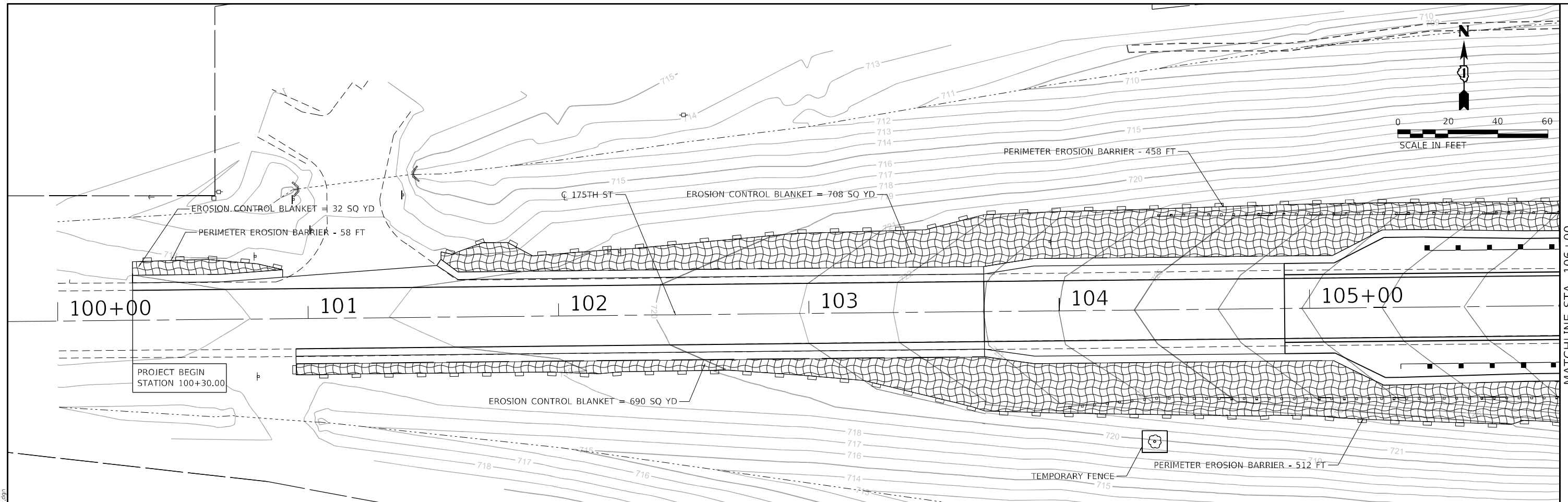
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES
EROSION AND SEDIMENT CONTROL**

SCALE: SHEET 1 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	23
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

MODEL: Default
 FILE NAME: P:\Document\hob\p\G\Garza_Karhoff\Projects\2021\DOT Phase II_Various Tasks_Curbs\1041 & 1045 - 175th Street at 157th\CAD\55\Sheet\102918-int-eroc02.dwg
 PLOT DATE = 3/15/2022



USER NAME = amalla,baymundo	DESIGNED - AMB	REVISED -
DRAWN - AMB	REVISIONS	
PLOT SCALE = 40,0000 * / in.	CHECKED - MM	REVISED -
PLOT DATE = 3/15/2022	DATE - 01/31/2022	REVISED -

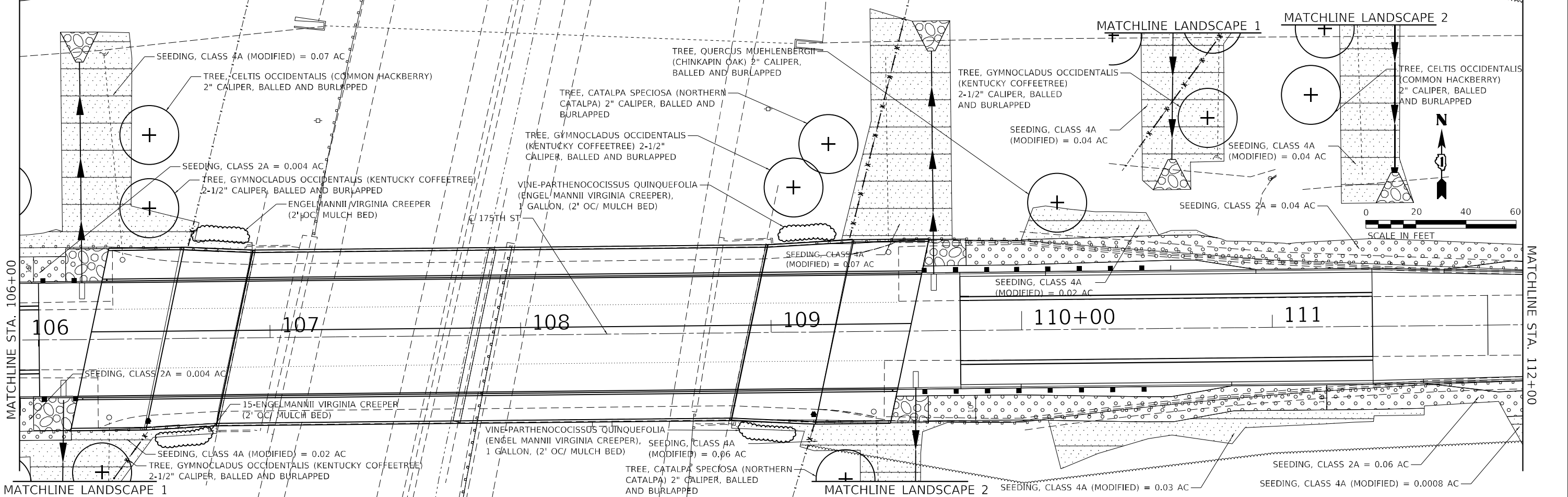
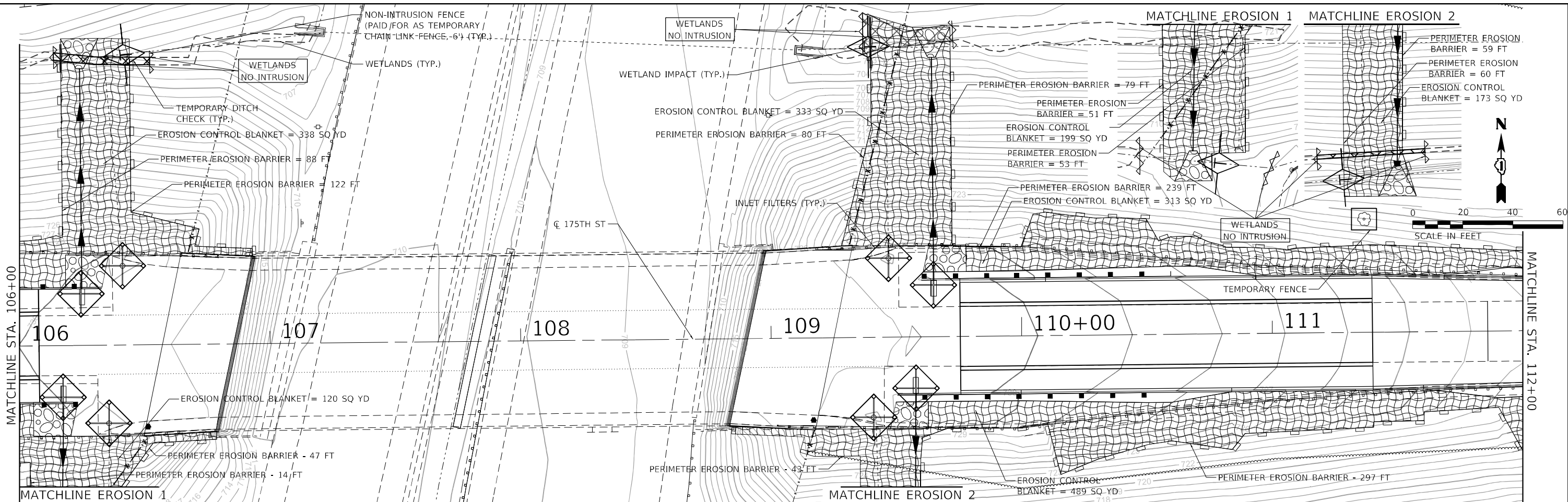
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION CONTROL & LANDSCAPING PLAN
 SCALE: 1" = 20' SHEET 2 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	24
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

MATCHLINE STA. 106+00

MATCHLINE STA. 106+00



MODEL: Default
 FILE NAME: p:\work\main\hwh\p\UG\creat...
 PROJECT: 2021-073-BDR
 SHEET: 3 OF 5
 DATE: 3/15/2022

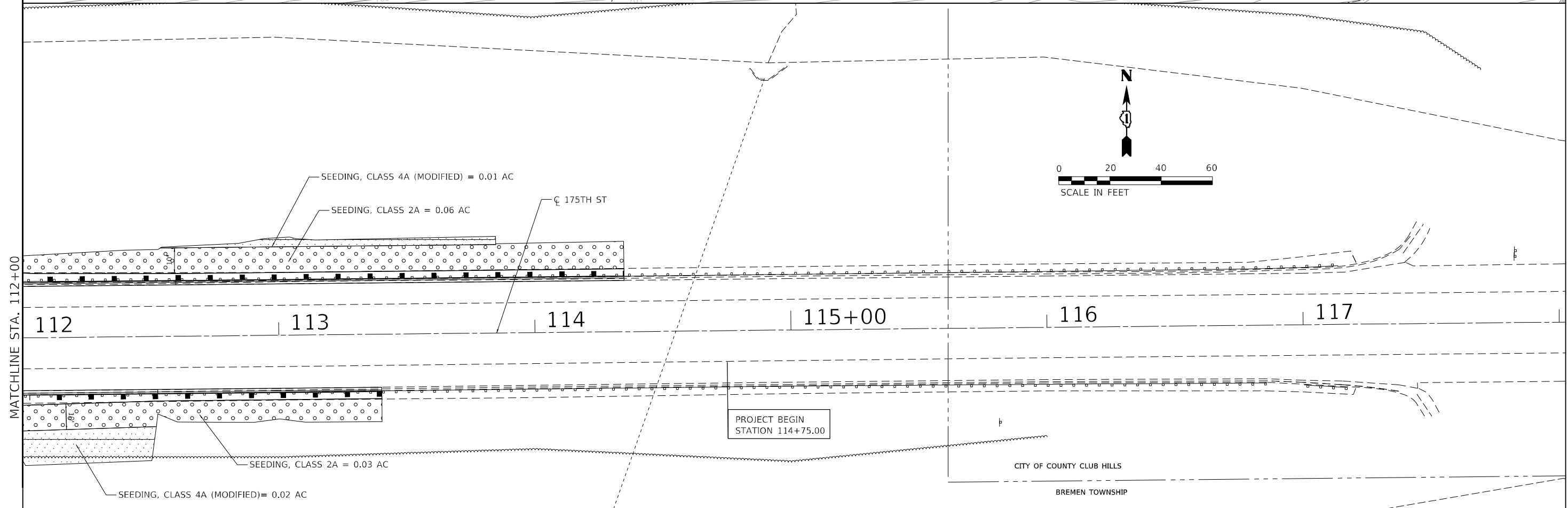
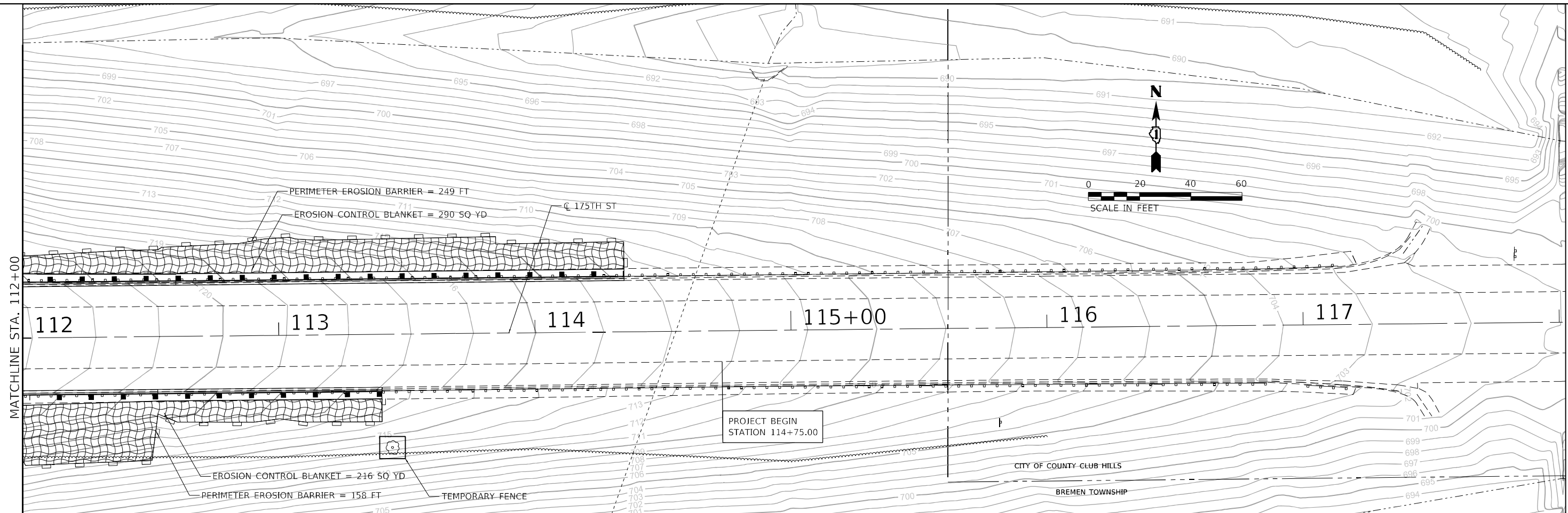
USER NAME = amalla.baymundo	DESIGNED - AMB	REVISED -
PLOT SCALE = 40,0000 * 1/ in.	DRAWN - AMB	REVISED -
PLOT DATE = 3/15/2022	CHECKED - MM	REVISED -
	DATE - 01/31/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION CONTROL & LANDSCAPING PLAN
 SCALE: 1" = 20' SHEET 3 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	25
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

MODEL: Default
 FILE NAME: p:\planning\hobbs\p\UGreat_Lakes\Documents\Chicago_Projects\80677_IDOT_Phase II_Various_Tasks_Curbs\1041 & 1045 - 175th_Street_at_157\CAD\5Sheets\102918-int-eroc04.dgn



USER NAME = amalla_baymundo	DESIGNED - AMB	REVISED -
PLOT SCALE = 40,0000 * / in.	DRAWN - AMB	REVISED -
PLOT DATE = 3/15/2022	CHECKED - MM	REVISED -
	DATE - 01/31/2022	REVISED -

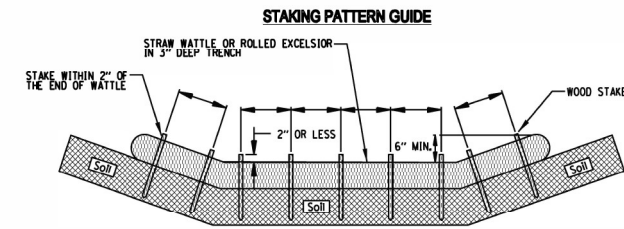
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EROSION CONTROL &
 LANDSCAPING PLAN**

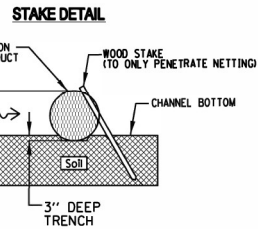
SCALE: 1" = 20' SHEET 4 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	26
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

ROLLED EROSION CONTROL PRODUCTS



- NOTES:
1. OVERLAP MINIMUM IS THE DIAMETER OF THE ROLL.
 2. 4" SPACING FOR WATTLES.
 3. 2" SPACING FOR ROLLED EXCELSIOR.
 4. OR SPACE ACCORDING TO MANUFACTURER'S SPECIFICATIONS.



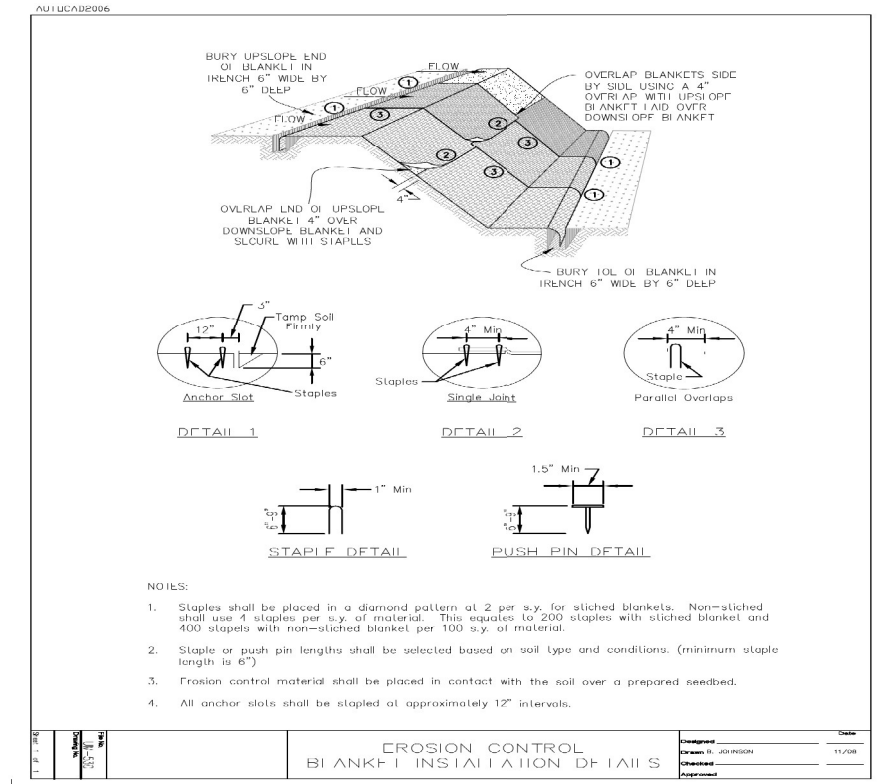
- NOTES:
1. DRAWINGS ARE NOT TO SCALE.
 2. ENDS OF WATTLES OR ROLLED EXCELSIOR SHALL BE TURNED AT LEAST 6" UPSLOPE.
 3. RECOMMENDED STAKES ARE 1 1/8" WIDE x 1 1/8" THICK x 30" LONG.
 4. STAKES SHALL NOT EXTEND ABOVE THE STRAW WATTLE MORE THAN 2".
 5. SPACING: THE TOE OF THE UPSTREAM DITCH CHECK SHALL CREATE A HORIZONTAL LINE WITH THE TOP OF THE DOWNSTREAM DITCH CHECK.

REFERENCE	
Project	
Designed	Date
Checked	Date
Approved	Date



STANDARD DWG. NO.
IUM-514
SHEET 1 OF 1
DATE 08-2-2019

DITCH CHECK DETAIL



- NOTES:
1. Staples shall be placed in a diamond pattern at 2 per s.y. for sliced blankets. Non-sliced shall use 4 staples per s.y. of material. This equates to 200 staples with sliced blanket and 400 staples with non-sliced blanket per 100 s.y. of material.
 2. Staple or push pin lengths shall be selected based on soil type and conditions. (minimum staple length is 6")
 3. Erosion control material shall be placed in contact with the soil over a prepared seedbed.
 4. All anchor slots shall be stapled at approximately 12" intervals.

EROSION CONTROL
BLANKET INSTALLATION DETAILS

Drawn	R. JOHNSON	Date	11/2008
Checked			
Approved			

EROSION CONTROL BLANKET DETAIL

MODEL: Default
 FILE: \\MAILSrv01\pub\updown\hnt\hnt\comp\PI\G\Cast_L\k\k\Documents\Chicgo_Proj\cts\80677_IDOT_Phase_II_Various_Tss_Curbs\T04-1 & T04-5 - 175th Street at 157\CAD\DWG\Sheets\102918-int-erc05.dwg



USER NAME = amalla,baymundo	DESIGNED - AMB	REVISED -
	DRAWN - AMB	REVISED -
PLOT SCALE = 40,0000 * / in.	CHECKED - MM	REVISED -
PLOT DATE = 2/1/2022	DATE - 01/31/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES			
EROSION AND SEDIMENT CONTROL			
SCALE:	SHEET 5	OF 5 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	27
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				














LIGHTING GENERAL NOTES

- PRIOR TO ANY WORK IN FIELD, THE CONTRACTOR MUST DETERMINE THE EXACT LOCATION OF EXISTING CONDUITS, CABLE AND UNDERGROUND UTILITIES. THE CONTRACTOR SHALL CALL J.U.L.I.E. (1-800-892-0123 OR 811) TO AID IN THIS TASK.
- THE CONTRACTOR MUST VERIFY ALL OF THE INFORMATION SHOWN ON THE CONTRACT PLANS WHICH WOULD EFFECT HIS WORK UNDER THIS CONTRACT FOR THE OPERATION OF THE EXISTING ROADWAY LIGHTING SYSTEM.
- NO MATERIAL OR EQUIPMENT SHALL BE DELIVERED TO THE JOB SITE WITHOUT PRIOR INSPECTION AND APPROVAL BY THE ENGINEER. ANY MATERIAL AND EQUIPMENT NOT APPROVED BY THE ENGINEER MUST BE REMOVED FROM THE JOB SITE AT THE CONTRACTOR'S EXPENSE.
- ALL ELECTRICAL SYSTEMS, EQUIPMENT AND APPURTENANCES SHALL BE PROPERLY GROUNDED IN STRICT CONFORMANCE WITH NATIONAL ELECTRICAL CODE EVEN THOUGH EVERY DETAIL OF REQUIREMENTS IS NOT SPECIFIED OR SHOWN.
- ALL UNDERGROUND WIRING SHALL BE 30 INCHES MINIMUM BELOW GRADE.
- THE NEW ELECTRICAL MATERIALS MUST MEET REQUIREMENTS OF STANDARDS BY THE FOLLOWING ORGANIZATIONS.
 - NATIONAL ELECTRICAL MANUFACTURER ASSOCIATION. (NEMA)
 - INSTITUTE OF ELECTRICAL AND ELECTRONICS ENGINEERS. (IEEE)
 - ILLUMINATING ENGINEERING SOCIETY OF NORTH AMERICA. (IES)
 - AMERICAN ASSOCIATION OF TRANSPORTATION OFFICIALS. (AATO)
 - U.S. DEPARTMENT OF TRANSPORTATION. (U.S.D.O.T)
 - UNDERWRITERS LABORATORIES. (UL)
 - AMERICAN STANDARD INSTITUTE. (ASI)
 - INSULATED POWER AND CABLE ENGINEERS ASSOCIATION. (IPCEA)
 - NATIONAL ELECTRICAL SAFETY CODE (NEC)
 - NATIONAL ELECTRICAL CODE 2011
 - AMERICAN NATIONAL STANDARD PRACTICE FOR ROADWAY LIGHTING (ANSI/IESNA RP-8)
- ALL SPLICING MUST BE IN POLE BASES OR JUNCTION BOXES ABOVE GRADE WITH WATERPROOF SEALANT AND HEAT SHRINKABLE PLASTIC CAPS.
- EACH LIGHTING UNIT SHALL BE CONTROLLED BY THE INDICATED LIGHTING CONTROLLER. NO LIGHT SHALL BE INDIVIDUALLY CONTROLLED BY PHOTOCELL.
- COMMENCEMENT OF WORK SHALL BE CONSTRUED AS EVIDENCE THAT A CAREFUL EXAMINATION OF THE PORTIONS OF THE EXISTING ROADWAY FOR THIS WORK, AND ACCESS TO WORK SPACES, HAS BEEN MADE AND THAT THE CONTRACTOR IS FAMILIAR WITH EXISTING CONDITIONS AND DIFFICULTIES THAT WILL AFFECT THE EXECUTION OF THE WORK. LATER CLAIMS SHALL NOT BE MADE FOR ADDITIONAL LABOR, EQUIPMENT OR MATERIALS REQUIRED BECAUSE OF DIFFICULTIES ENCOUNTERED WHICH COULD HAVE BEEN FORESEEN DURING SUCH AN EXAMINATION. CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING, REINSTALLING, REPAIRING, OR REPLACING EQUIPMENT.
- PRIOR TO SUBMITTING A BID, THE CONTRACTOR SHALL REVIEW THE CONTRACT DRAWINGS AND ASCERTAIN EXISTING SITE CONDITIONS TO VERIFY THE EXTENT OF DEMOLITION AND REMODELING WORK. FAILURE TO DO SO SHALL NOT RELIEVE THE CONTRACTOR FROM PERFORMING ALL RELOCATIONS AND REMOVALS REQUIRED IN THIS CONTRACT. CONTRACTOR SHALL VERIFY IN THE FIELD THE EXISTING CONDITIONS AND COORDINATE AS REQUIRED.
- BRANCH CIRCUIT WIRING FOR EXISTING EQUIPMENT TO BE REMOVED SHALL BE DISCONNECTED AND REMOVED AS INDICATED. REMOVAL OF EXISTING WIRING IS INCLUDED IN THE REMOVAL OF EXISTING EQUIPMENT.
- ALL EXISTING EQUIPMENT THAT ARE TO BE REMOVED SHALL BE DISPOSED OF AT THE CONTRACTOR'S EXPENSE.
- LOCATIONS SELECTED FOR COLLECTION OF DEBRIS AND/OR STORAGE OF EQUIPMENT SHALL BE SUBJECT TO THE ENGINEER'S APPROVAL.
- MEADE ELECTRIC CO. DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES: 773-287-7672.

ABBREVIATIONS

AMP	AMPERE
AWG	AMERICAN WIRE GAUGE
BC	BOLT CIRCLE
C	CONDUIT
DIA	DIAMETER
GRD	GROUND
HDPE	HIGH DENSITY POLYETHYLENE
HPS	HIGH PRESSURE SODIUM
ITS	INTELLIGENT TRANSPORTATION SYSTEMS
JB	JUNCTION BOX
MA	MAST ARM
MH	MOUNTING HEIGHT
N	NEUTRAL
PH	PHASE
PVC	POLYVINYL CHLORIDE
PVCC RGS	PVC COATED RIGID GALVANIZED STEEL
RGS	RIGID GALVANIZED STEEL
UNO	UNLESS NOTED OTHERWISE
USE	UNDERGROUND SERVICE ENTRANCE
W	WATTS
XLP	CROSS-LINKED POLYETHYLENE

LEGEND

	EXISTING CONDUIT AND WIRING
	EXISTING CONDUIT AND NEW WIRING
	PROPOSED CONDUIT AND WIRING
	EXISTING LIGHTING CONTROLLER "T"
	EXISTING UNDERPASS LPS LUMINAIRE ON RED PHASE WIRE, 55W TO BE REMOVED
	EXISTING UNDERPASS LPS LUMINAIRE ON BLACK PHASE WIRE, 55W TO BE REMOVED
	EXISTING HPS ROADWAY LUMINAIRE ON RED PHASE WIRE, 310W AND 400W
	EXISTING HPS ROADWAY LUMINAIRE ON BLACK PHASE WIRE, 310W AND 400W
	EXISTING HPS ROADWAY LUMINAIRE TO REMAIN IN SERVICE DURING CONSTRUCTION
	PROPOSED LED UNDERPASS LUMINAIRE ON RED PHASE WIRE
	PROPOSED LED UNDERPASS LUMINAIRE ON BLACK PHASE WIRE
	PROPOSED ELECTRICAL JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 12" X 10" X 6"
	PROPOSED ELECTRICAL JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 18" X 18" X 18"

MODEL: Default
 FILE: hntb\p\supplement\hntb\p\UGreat_Lakes\Documents\Chicago_Projects\80677_IDOT_Phase_II_Various_Task_Curbs\TOD-1_&_TOD-5_-_175th_Street_at_I57\CAD\55\SheetD102918-act-lighting-notes.dgn



USER NAME = amalla.baymundo	DESIGNED - MCD	REVISED -
	DRAWN - MF	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - MCD	REVISED -
PLOT DATE = 2/3/2022	DATE - 1/31/2022	REVISED -

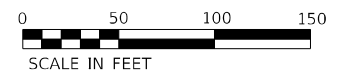
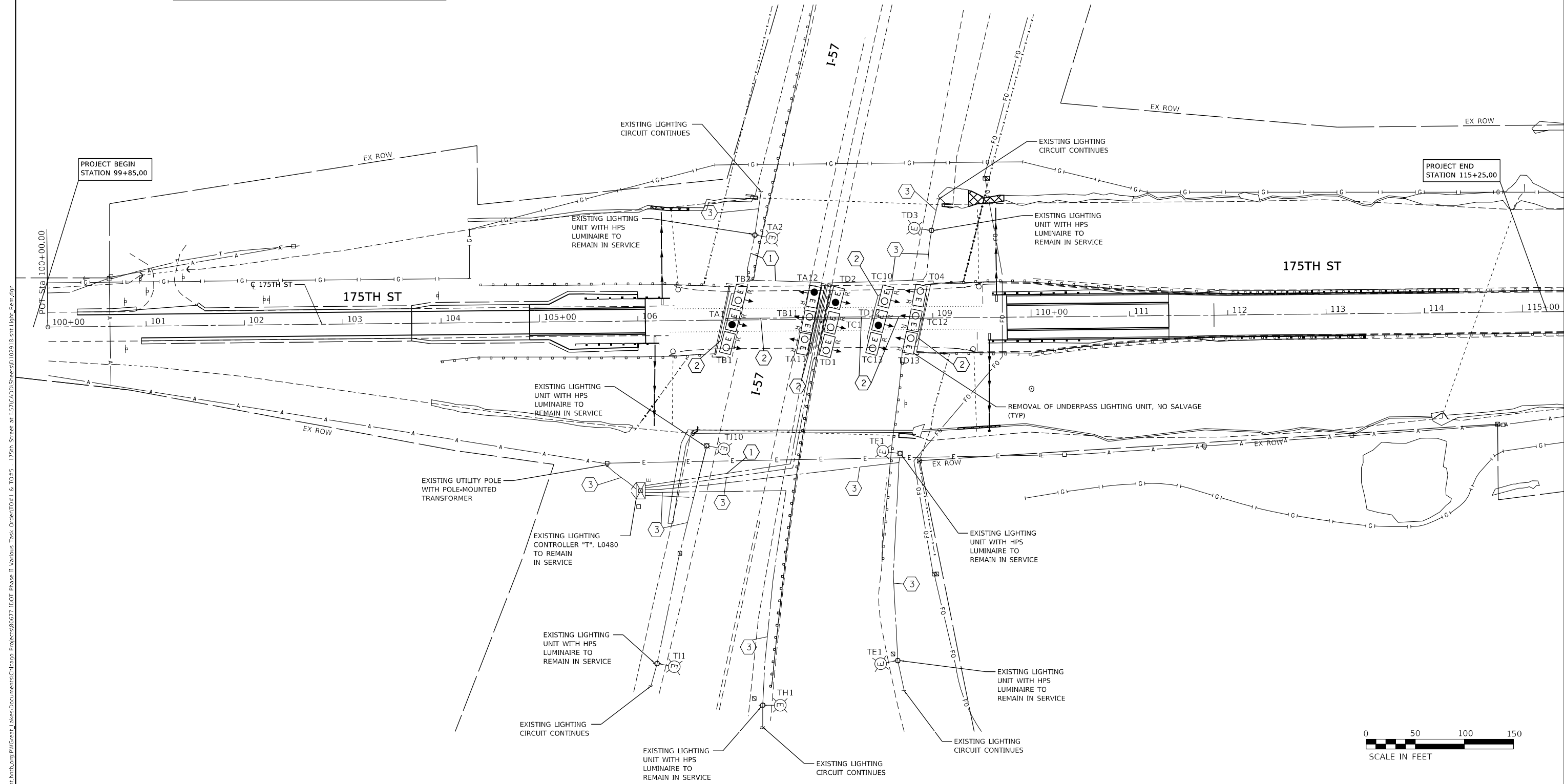
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-57 AT 175TH STREET LIGHTING GENERAL NOTES	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	28
CONTRACT NO. 62P10			ILLINOIS FED. AID PROJECT	

CONDUIT/CABLE SCHEDULE:

CALL-OUT	DESCRIPTION
①	ROD AND CLEAN EXISTING CONDUIT WHEN POSSIBLE, REMOVE EXISTING CONDUCTORS FROM EXISTING CONDUIT.
②	REMOVAL OF CABLE IN CONDUIT REMOVE EXISTING CONDUIT ATTACHED TO STRUCTURE
③	EXISTING CABLE AND CONDUIT, PROTECT IN PLACE DURING CONSTRUCTION



MODEL: Default
 FILE: hntb\p\supplement\hntb\p\UG\Crast_L\Jobs\Documents\Chicago_Projects\80677_IDOT_Phase_II_Various_Tasks_Curbs\1041 & 1045 - 175th Street at I-57\CAD\57\Sheets\102918-act\Lighting_Rem.dwg
 PLOT DATE = 2/1/2022



USER NAME = amalla,baymundo	DESIGNED - MCD	REVISED -
	DRAWN - MF	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - MCD	REVISED -
PLOT DATE = 2/1/2022	DATE - 1/31/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

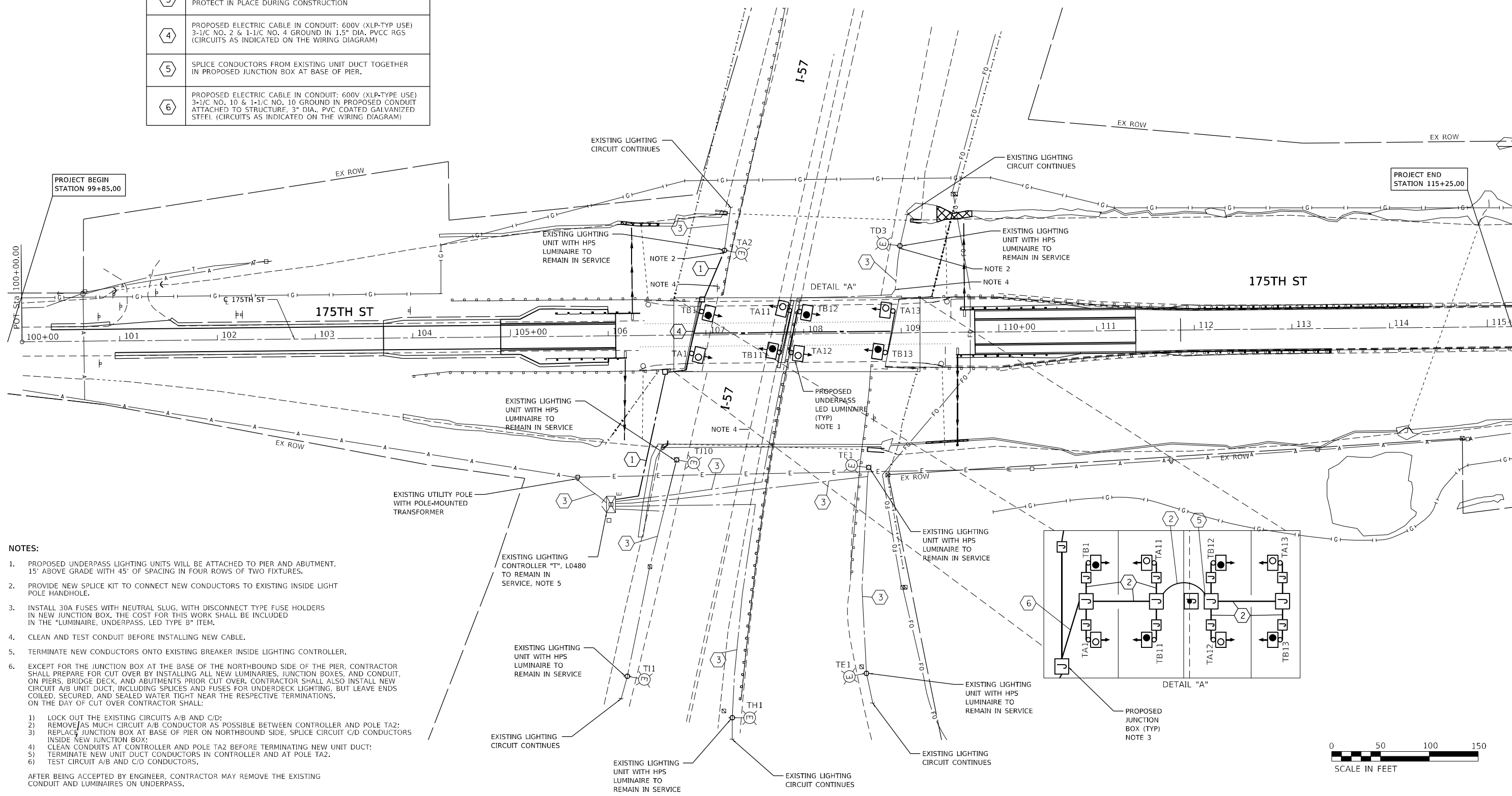
**I-57 AT 175TH STREET
LIGHTING REMOVAL PLAN**

SCALE: SHEET OF SHEETS STA. 99+85 TO STA. 115+25

F.A. RTE. 57	SECTION 2021-073-BDR	COUNTY COOK	TOTAL SHEETS 89	SHEET NO. 29
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

CONDUIT/CABLE SCHEDULE:

CALL-OUT	DESCRIPTION
1	PROPOSED UNIT DUCT, 600V, 3-1/2" NO. 2, 1-1/2" NO. 4 GROUND (XLP-TYPE USE), 1 1/2" DIA. POLYETHYLENE.
2	PROPOSED ELECTRIC CABLE IN CONDUIT; 600V (XLP-TYPE USE) 3-1/2" NO. 10 & 1-1/2" NO. 10 GROUND IN PROPOSED CONDUIT ATTACHED TO STRUCTURE, 1" DIA., PVC COATED GALVANIZED STEEL (CIRCUITS AS INDICATED ON THE WIRING DIAGRAM)
3	EXISTING CABLE AND CONDUIT, PROTECT IN PLACE DURING CONSTRUCTION
4	PROPOSED ELECTRIC CABLE IN CONDUIT; 600V (XLP-TYPE USE) 3-1/2" NO. 2 & 1-1/2" NO. 4 GROUND IN 1.5" DIA. PVCC RGS (CIRCUITS AS INDICATED ON THE WIRING DIAGRAM)
5	SPLICE CONDUCTORS FROM EXISTING UNIT DUCT TOGETHER IN PROPOSED JUNCTION BOX AT BASE OF PIER.
6	PROPOSED ELECTRIC CABLE IN CONDUIT; 600V (XLP-TYPE USE) 3-1/2" NO. 10 & 1-1/2" NO. 10 GROUND IN PROPOSED CONDUIT ATTACHED TO STRUCTURE, 3" DIA., PVC COATED GALVANIZED STEEL (CIRCUITS AS INDICATED ON THE WIRING DIAGRAM)



- NOTES:**
- PROPOSED UNDERPASS LIGHTING UNITS WILL BE ATTACHED TO PIER AND ABUTMENT, 15' ABOVE GRADE WITH 45' OF SPACING IN FOUR ROWS OF TWO FIXTURES.
 - PROVIDE NEW SPLICE KIT TO CONNECT NEW CONDUCTORS TO EXISTING INSIDE LIGHT POLE HANDHOLE.
 - INSTALL 30A FUSES WITH NEUTRAL SLUG, WITH DISCONNECT TYPE FUSE HOLDERS IN NEW JUNCTION BOX, THE COST FOR THIS WORK SHALL BE INCLUDED IN THE "LUMINAIRE, UNDERPASS, LED TYPE B" ITEM.
 - CLEAN AND TEST CONDUIT BEFORE INSTALLING NEW CABLE.
 - TERMINATE NEW CONDUCTORS ONTO EXISTING BREAKER INSIDE LIGHTING CONTROLLER.
 - EXCEPT FOR THE JUNCTION BOX AT THE BASE OF THE NORTHBOUND SIDE OF THE PIER, CONTRACTOR SHALL PREPARE FOR CUT OVER BY INSTALLING ALL NEW LUMINAIRES, JUNCTION BOXES, AND CONDUIT, ON PIERS, BRIDGE DECK, AND ABUTMENTS PRIOR CUT OVER. CONTRACTOR SHALL ALSO INSTALL NEW CIRCUIT A/B UNIT DUCT, INCLUDING SPLICES AND FUSES FOR UNDERDECK LIGHTING, BUT LEAVE ENDS COILED, SECURED, AND SEALED WATER TIGHT NEAR THE RESPECTIVE TERMINATIONS. ON THE DAY OF CUT OVER CONTRACTOR SHALL:
 - LOCK OUT THE EXISTING CIRCUITS A/B AND C/D;
 - REMOVE AS MUCH CIRCUIT A/B CONDUCTOR AS POSSIBLE BETWEEN CONTROLLER AND POLE TA2;
 - REPLACE JUNCTION BOX AT BASE OF PIER ON NORTHBOUND SIDE, SPLICE CIRCUIT C/D CONDUCTORS INSIDE NEW JUNCTION BOX;
 - CLEAN CONDUITS AT CONTROLLER AND POLE TA2 BEFORE TERMINATING NEW UNIT DUCT;
 - TERMINATE NEW UNIT DUCT CONDUCTORS IN CONTROLLER AND AT POLE TA2.
 - TEST CIRCUIT A/B AND C/D CONDUCTORS.
- AFTER BEING ACCEPTED BY ENGINEER, CONTRACTOR MAY REMOVE THE EXISTING CONDUIT AND LUMINAIRES ON UNDERPASS.



MODEL: Default
 FILE NAME: p:\work\hntb\p11\g1\cast_1\sketch\Documents\Chicago Projects\80677 IDOT Phase II Various Ties, Curb\1041 & 1045 - 175th Street at I-57\CADD\Sheet\022918-act-lighting-proposed.dgn



USER NAME = amalla,baymundo	DESIGNED - MCD	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN - MF	REVISED -
PLOT DATE = 3/15/2022	CHECKED - MCD	REVISED -
	DATE - 1/31/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-57 AT 175TH STREET
LIGHTING PROPOSED PLAN**

SCALE: SHEET OF SHEETS STA. 99+85 TO STA. 115+25

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	30
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

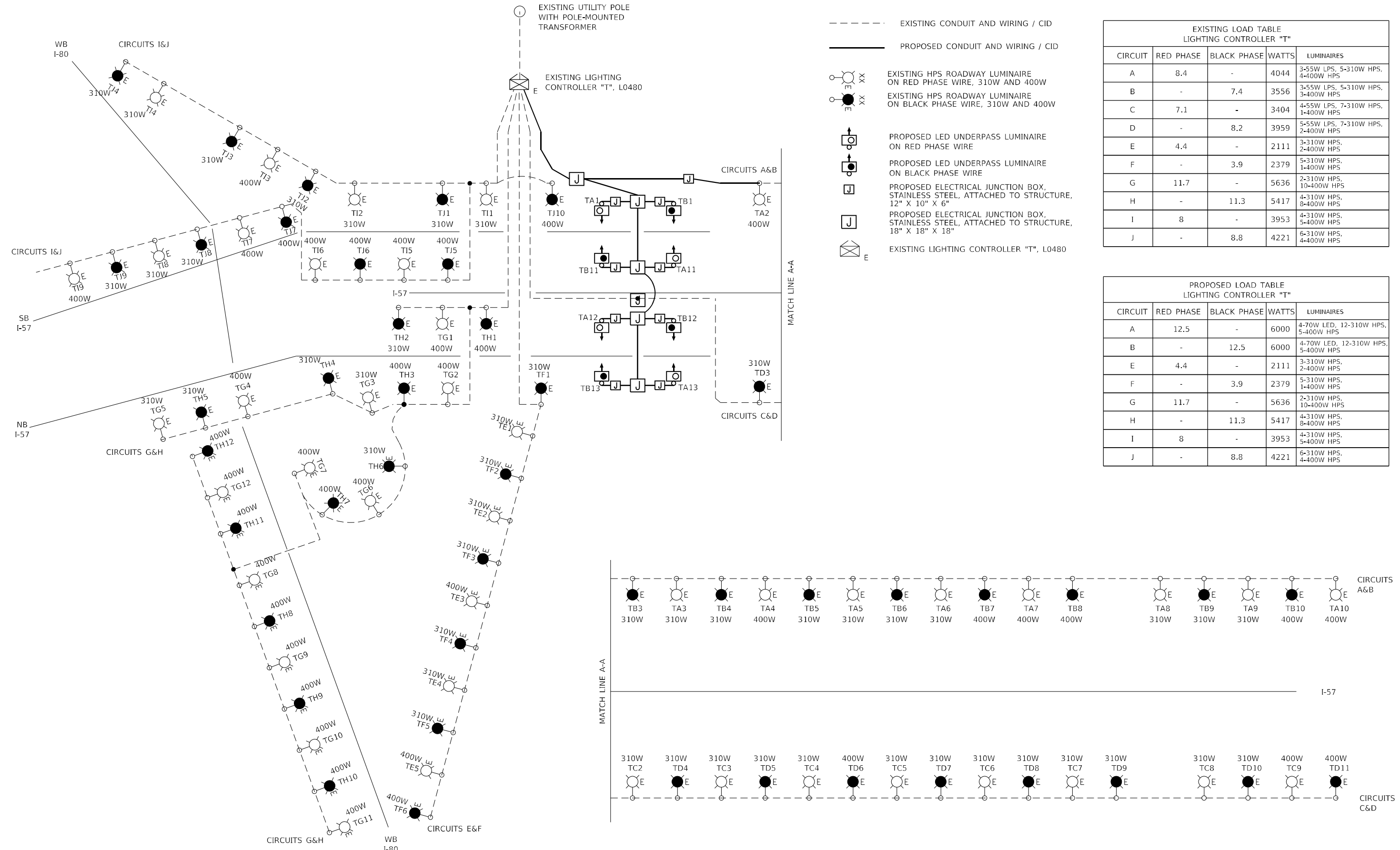


LEGEND

- EXISTING CONDUIT AND WIRING / CID
- PROPOSED CONDUIT AND WIRING / CID
- EXISTING HPS ROADWAY LUMINAIRE ON RED PHASE WIRE, 310W AND 400W
- EXISTING HPS ROADWAY LUMINAIRE ON BLACK PHASE WIRE, 310W AND 400W
- PROPOSED LED UNDERPASS LUMINAIRE ON RED PHASE WIRE
- PROPOSED LED UNDERPASS LUMINAIRE ON BLACK PHASE WIRE
-
-
- EXISTING LIGHTING CONTROLLER "T", L0480

EXISTING LOAD TABLE LIGHTING CONTROLLER "T"				
CIRCUIT	RED PHASE	BLACK PHASE	WATTS	LUMINAIRES
A	8.4	-	4044	3-55W LPS, 5-310W HPS, 4-400W HPS
B	-	7.4	3556	3-55W LPS, 5-310W HPS, 3-400W HPS
C	7.1	-	3404	4-55W LPS, 7-310W HPS, 1-400W HPS
D	-	8.2	3959	5-55W LPS, 7-310W HPS, 2-400W HPS
E	4.4	-	2111	3-310W HPS, 2-400W HPS
F	-	3.9	2379	5-310W HPS, 1-400W HPS
G	11.7	-	5636	2-310W HPS, 10-400W HPS
H	-	11.3	5417	4-310W HPS, 8-400W HPS
I	8	-	3953	4-310W HPS, 5-400W HPS
J	-	8.8	4221	6-310W HPS, 4-400W HPS

PROPOSED LOAD TABLE LIGHTING CONTROLLER "T"				
CIRCUIT	RED PHASE	BLACK PHASE	WATTS	LUMINAIRES
A	12.5	-	6000	4-70W LED, 12-310W HPS, 5-400W HPS
B	-	12.5	6000	4-70W LED, 12-310W HPS, 5-400W HPS
E	4.4	-	2111	3-310W HPS, 2-400W HPS
F	-	3.9	2379	5-310W HPS, 1-400W HPS
G	11.7	-	5636	2-310W HPS, 10-400W HPS
H	-	11.3	5417	4-310W HPS, 8-400W HPS
I	8	-	3953	4-310W HPS, 5-400W HPS
J	-	8.8	4221	6-310W HPS, 4-400W HPS



MODEL: Default
 FILE: hntb\p\supplement\hntb\p\UGreat_Lakes\Documents\Chicago Projects\80677_IDOT Phase II Varies Tss\Curden\T04-1 & T04-5 - 175th Street at I-57\CADD\Sheet\02918-act-lighting-diagram.dgn



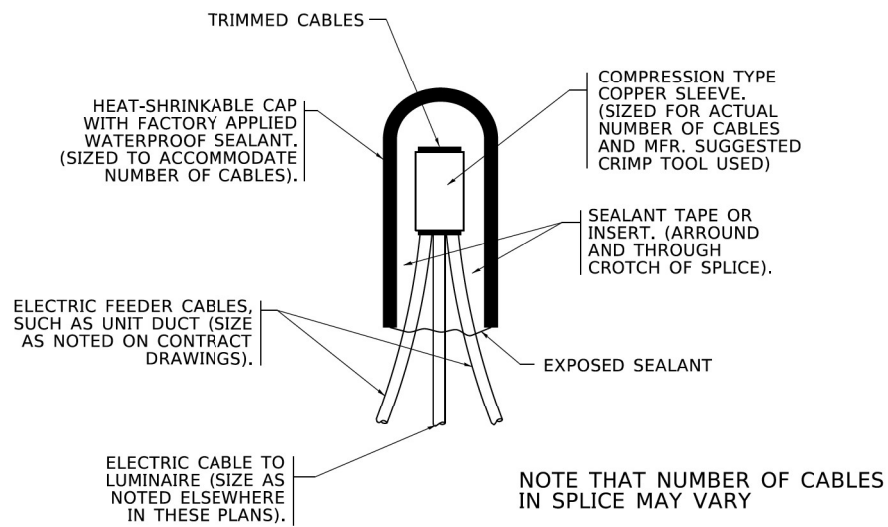
USER NAME = amalla.baymundo	DESIGNED - MCD	REVISED -
DRAWN - MF	REVISIONS -	
PLOT SCALE = 100,0000' / in.	CHECKED - MCD	REVISED -
PLOT DATE = 2/1/2022	DATE - 1/31/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

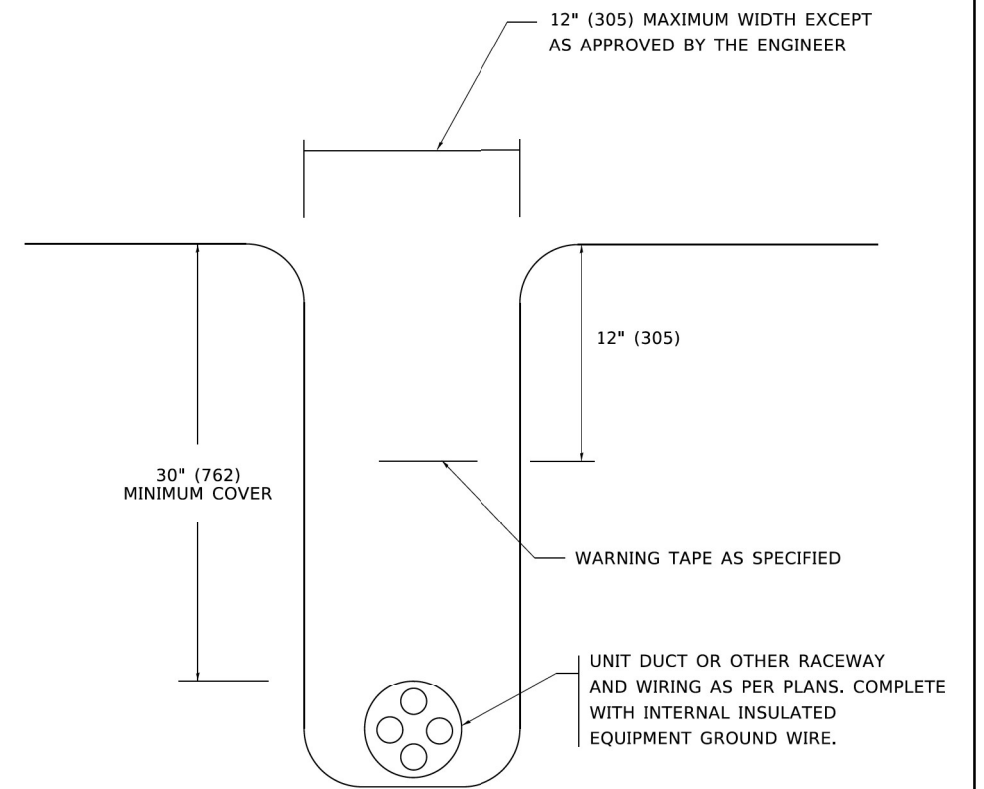
**I-57 AT 175TH STREET
LIGHTING WIRING DIAGRAM**

SCALE: SHEET OF SHEETS STA. TO STA.

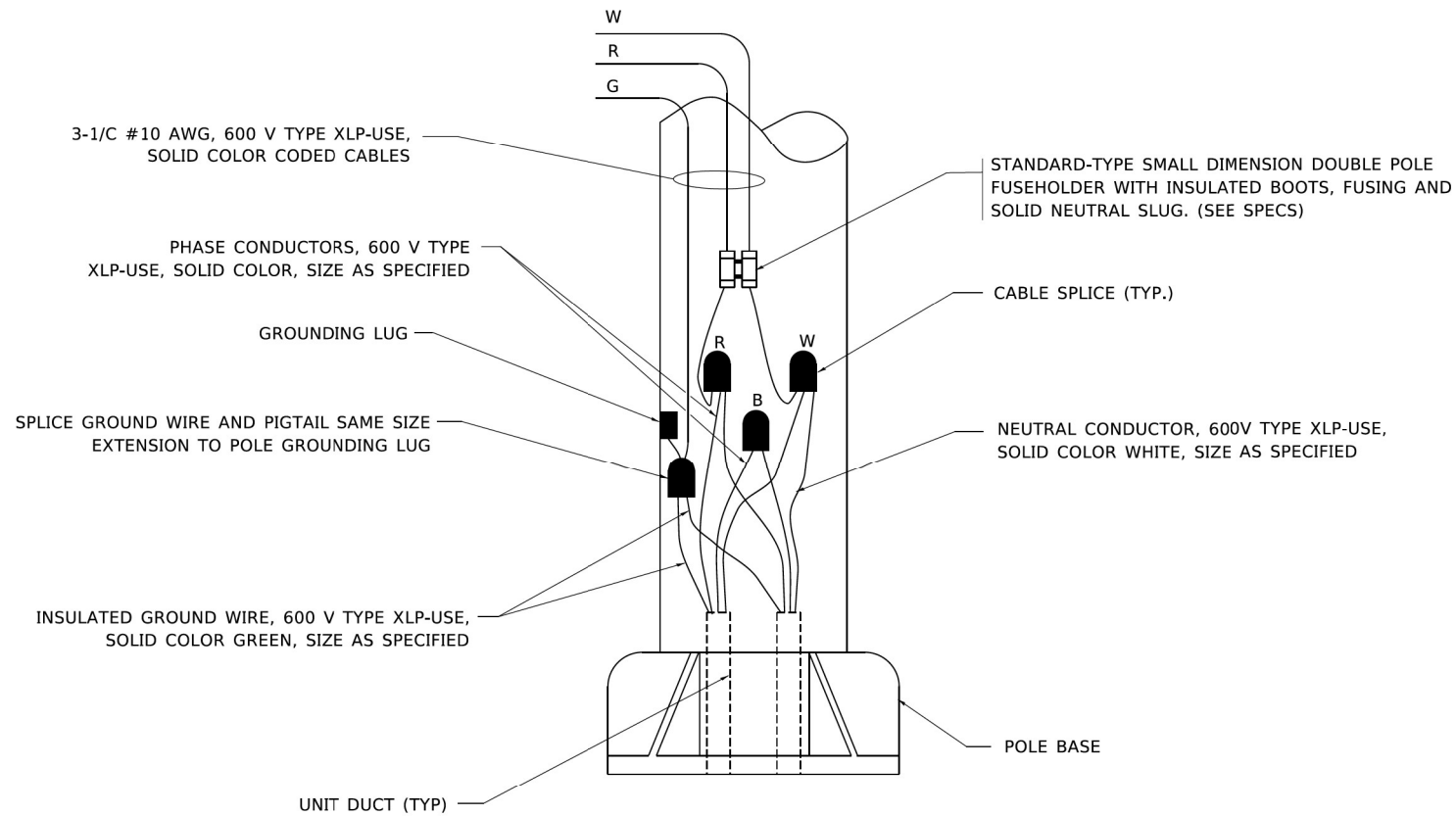
F.A. RTE. 57	SECTION 2021-073-BDR	COUNTY COOK	TOTAL SHEETS 89	SHEET NO. 31
CONTRACT NO. 62P10				
ILLINOIS		FED. AID PROJECT		



TYPICAL SPLICE DETAIL
N.T.S.



TYPICAL WIRING IN TRENCH DETAIL
N.T.S.



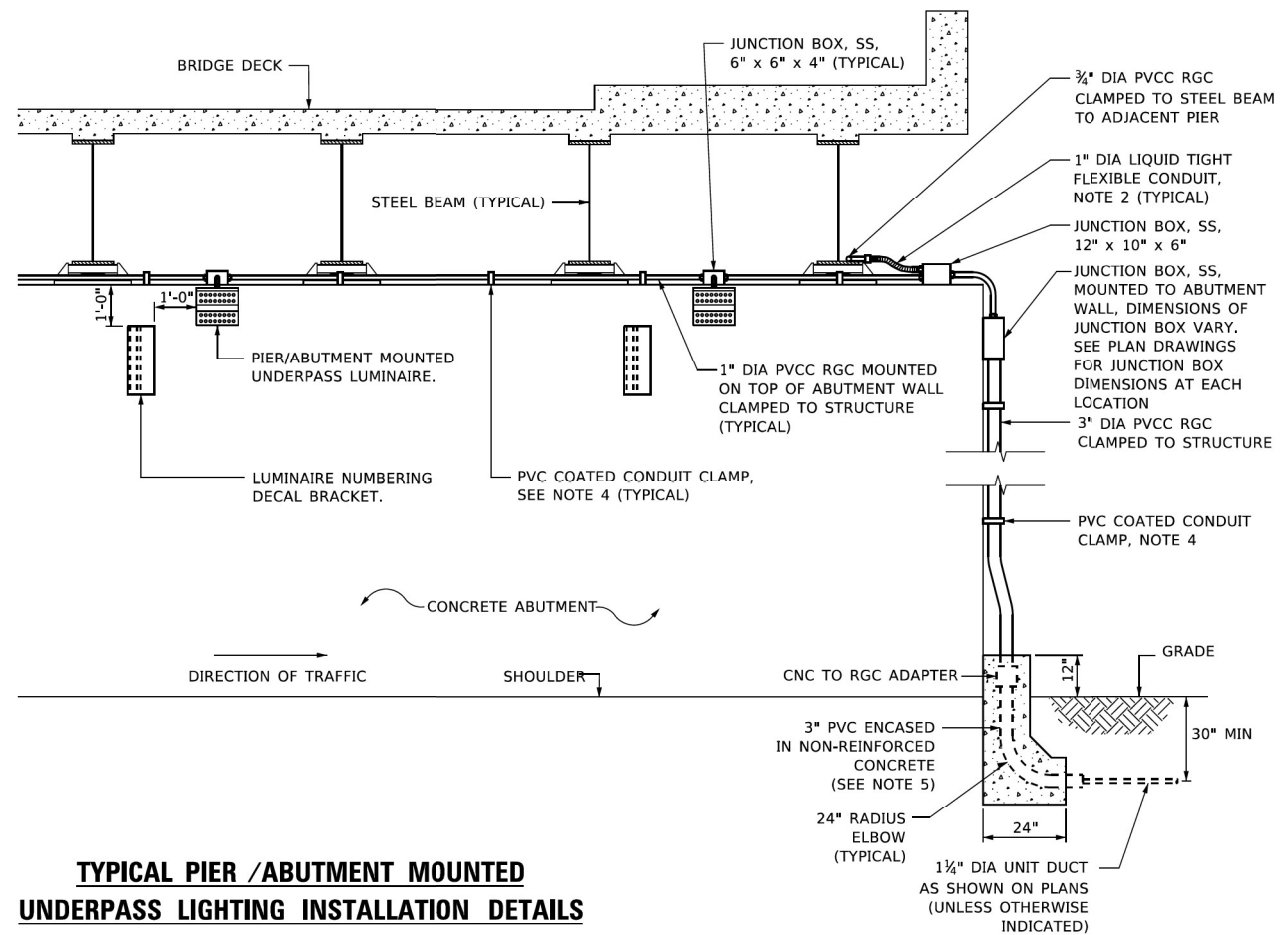
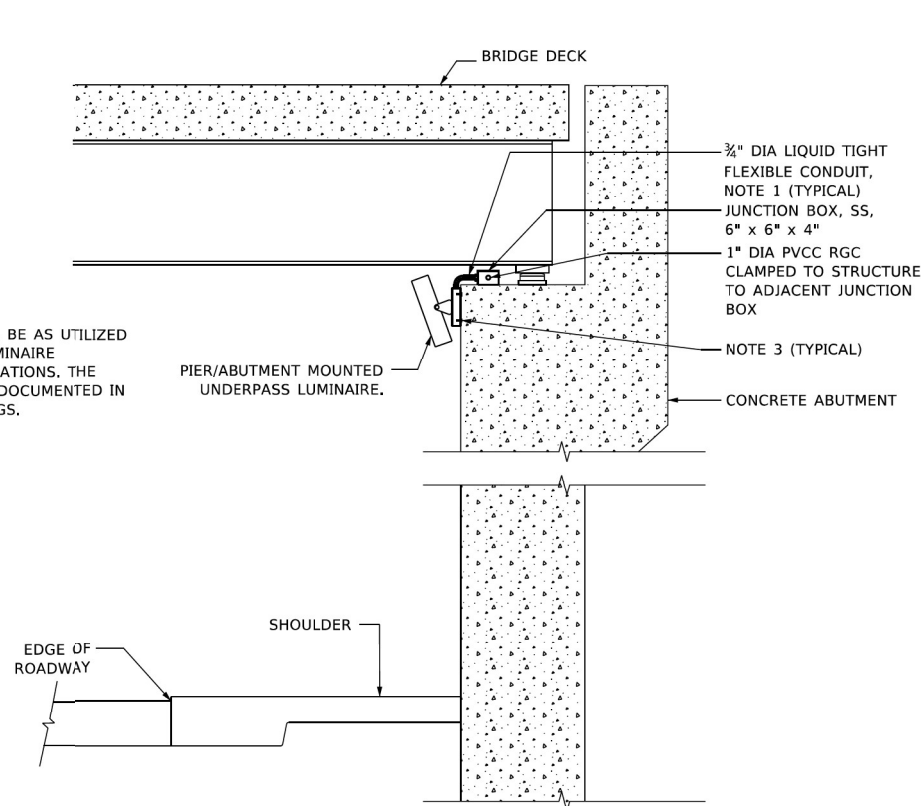
POLE WIRING DETAIL
N.T.S.

USER NAME = leysa	DESIGNED -	REVISED - 02/04/2020	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MISC. ELECTRICAL DETAILS SHEET A			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 50.0000' / in.	DRAWN -	REVISED -		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	CONTRACT NO.	ILLINOIS	FED. AID PROJECT
PLOT DATE = 3/2/2020	CHECKED -	REVISED -		BE-702							
	DATE - 08/08/2003	REVISED -									

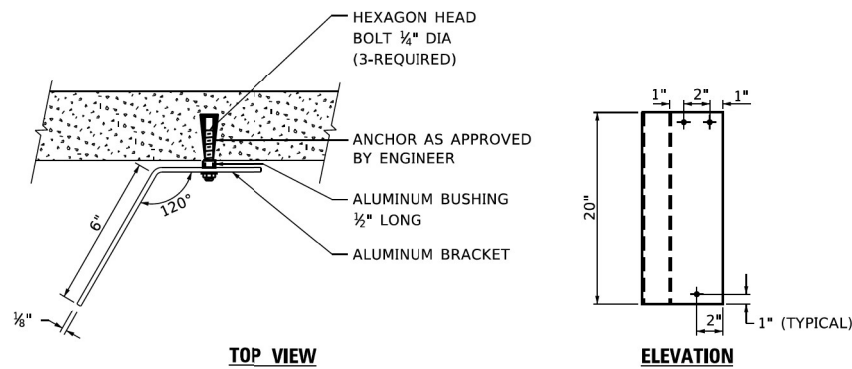
HNTB	USER NAME = amalla.baymundo	DESIGNED - MCD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-57 AT 175TH STREET LIGHTING ELECTRICAL DETAILS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 100.0000' / in.	DRAWN - MF	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	CONTRACT NO.	ILLINOIS	FED. AID PROJECT
	PLOT DATE = 2/1/2022	CHECKED - MCD	REVISED -		62P10								
		DATE - 1/31/2022	REVISED -										

MODEL: Default
 FILE NAME: p:\planningroom\c:\illinois\pwr\WIDOT\Documents\DOT Office\District 1\Projects\BES502\34\CADData\CADsheets\be702.dgn
 2020-09-22 09:27:37 AM User: jay

NOTE:
LUMINAIRE TILT SHALL BE AS UTILIZED IN THE APPROVED LUMINAIRE PHOTOMETRIC CALCULATIONS. THE TILT ANGLE MUST BE DOCUMENTED IN THE RECORD DRAWINGS.

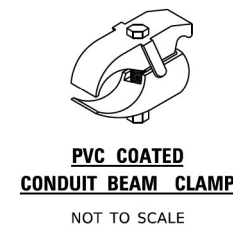


TYPICAL PIER /ABUTMENT MOUNTED UNDERPASS LIGHTING INSTALLATION DETAILS



LUMINAIRE NUMBERING DECAL BRACKET

NOT TO SCALE



NOTES:

- LIQUID TIGHT FLEXIBLE METAL CONDUIT, MAXIMUM LENGTH 6'-0". TYPICAL FOR EACH INSTANCE AS SHOWN. PROVIDE PVC COATED RIGID GALVANIZED STEEL CONDUIT AS REQUIRED NOT TO EXCEED 6'-0" OF FLEXIBLE LIQUID TIGHT METAL CONDUIT. LIQUID TIGHT FLEXIBLE METAL CONDUIT WILL BE INCLUDED IN THE COST OF THE CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED PAY ITEM EXCEPT THAT THE COST OF THE " DIA. RIGID STEEL CONDUIT AND " DIA. FLEXIBLE CONDUIT SHALL BE INCLUDED IN THE LUMINAIRE INSTALLATION.
- SEE UNDERPASS LIGHTING PLANS FOR INSTALLATION LOCATION OF UNDERPASS LIGHTING LUMINAIRES.
- UNDERPASS LUMINAIRE MOUNTED TO FACE OF PIER OR ABUTMENT WALL WITH 1/2" ALUMINUM SPACERS. MOUNTING HEIGHT OF 1" BELOW THE TOP OF PIER OR ABUTMENT WALL TYPICAL FOR ALL PIER/ABUTMENT MOUNTED UNDERPASS LUMINAIRES UNLESS OTHERWISE NOTED.
- EXPANSION ANCHOR, POWDER ACTUATED FASTENERS WILL NOT BE ALLOWED. EXPANSION ANCHOR MUST BE SIZED IN ACCORDANCE WITH MANUFACTURERS REQUIREMENTS.
- SECURE THE CONDUIT WITH PVC COATED CONDUIT CLAMPS OR CONDUIT BEAM CLAMPS AS SHOWN AT 5'-0" INTERVALS FOR LATERALS AND WITHIN 2'-0" MAXIMUM FROM ANY JUNCTION BOX, FLEXIBLE CONDUIT, OR CHANGE IN DIRECTION. ALL PVC COATED CONDUIT CLAMPS OR BEAM CLAMPS SHALL BE INCLUDED WITH THE COST OF THE "CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED" PAY ITEM.
- THE CONCRETE ENCASED CONDUIT TRANSITION SHALL BE INCLUDED IN THE COST OF THE GALVANIZED RIGID STEEL CONDUIT PAY ITEMS.
- ALL CONDUIT ATTACHED TO STRUCTURE SHALL BE PVC COATED RIGID STEEL CONDUIT (PVCC RGC) TYPICAL.

MODEL: Default
FILE NAME: p:\p\upstate-hrb\comp\p\UG\cast_1\books\Documents\Chicago Projects\80677 IDOT Phase II Various Tack_Curbs\104-1 & 104-5 - 175th Street at I-57\CADD\Sheets\102918-ehc-light-det-2.dgn

USER NAME = gagllanobt	DESIGNED -	REVISED - 01-25-05
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / In.	CHECKED -	REVISED -
PLOT DATE = 1/15/2020	DATE -	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	
PIER /ABUTMENT MOUNTED LED UNDERPASS LUMINAIRE INSTALLATION DETAILS	
SCALE: NONE	SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
BE-903			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				

USER NAME = amalla,baymundo	DESIGNED - MCD	REVISED -
	DRAWN - MF	REVISED -
PLOT SCALE = 100,0000' / In.	CHECKED - MCD	REVISED -
PLOT DATE = 2/1/2022	DATE - 1/31/2022	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	
I-57 AT 175TH STREET LIGHTING ELECTRICAL DETAILS	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	33
CONTRACT NO. 62P10			ILLINOIS FED. AID PROJECT	



Benchmark: Chiseled "□" in West end of South parapet of S.N. 016-1095, Elev. 732.61

Existing Structure: S.N. 016-1095, originally built in 1965 as F.A.I. Rt. 57 Sec. 0809-1014HB-1. In 1984 a concrete overlay was added and the longitudinal center joint was eliminated. In 2000 a 2.5 inch microsilica concrete deck overlay was completed. The existing structure consists of two-span structure with reinforced concrete slab and steel plate girders supported by a multi-column pier with a crashwall with creosoted piles and vaulted abutment spans with concrete piles. The bridge has a total span length of 269'-3" and an out-to-out width of 70'-0". Deck to be removed and replaced. Traffic to be maintained utilizing stage construction.

Salvage: Bridge Mounted Sign Panel

SCOPE OF WORK

1. Remove and replace existing concrete deck, vaulted abutment approach spans, and approach slabs.
2. Add shear studs to the existing girders to make the deck composite in positive and negative moment regions.
3. Replace existing expansion bearing side retainer and anchor bolt at Girder 4 on the West Abutment.
4. Remove, store, and re-erect existing bridge-mounted sign structure and panel.
5. Formed concrete repair and epoxy crack sealing to be performed on Substructure and Slope Walls.
6. All existing structural steel will be cleaned and painted under a separate contract.

LOADING HS20-44

No future wearing surface allowed.

DESIGN SPECIFICATIONS

2002 AASHTO Standard Specifications for Highway Bridges, 17th Edition

DESIGN STRESSES

FIELD UNITS (New Construction)

f'c = 4,000 psi (Superstructure)
f'c = 3,500 psi (Substructure)
fy = 60,000 psi (Reinforcement)
fy = 36,000 psi (M270 Grade 36)

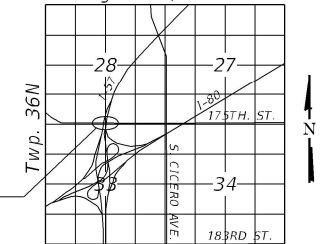
FIELD UNITS (Exist. Construction)

f'c = 3,500 psi (Super. & Sub.)
fy = 40,000 psi (Reinforcement)
fy = 36,000 psi (Structural Steel)

SEISMIC DATA

Seismic Performance Category (SPC) = A
Horizontal Bedrock Acceleration Coefficient (A) = 0.039g
Site Coefficient (S) = 1.5

Range 13E, 3rd P.M.



LOCATION SKETCH

STA. 107+82.25
RE-BUILT 20__ BY
STATE OF ILLINOIS
F.A.I. RTE. 57
SEC. 2021-073-BDR
LOADING HS20-44
STR. NO. 016-1095

NAME PLATE

See Std. 515001

Note: Existing Name Plate shall be cleaned and relocated next to new Name Plate. Cost included with Name Plates.

GENERAL PLAN & ELEVATION

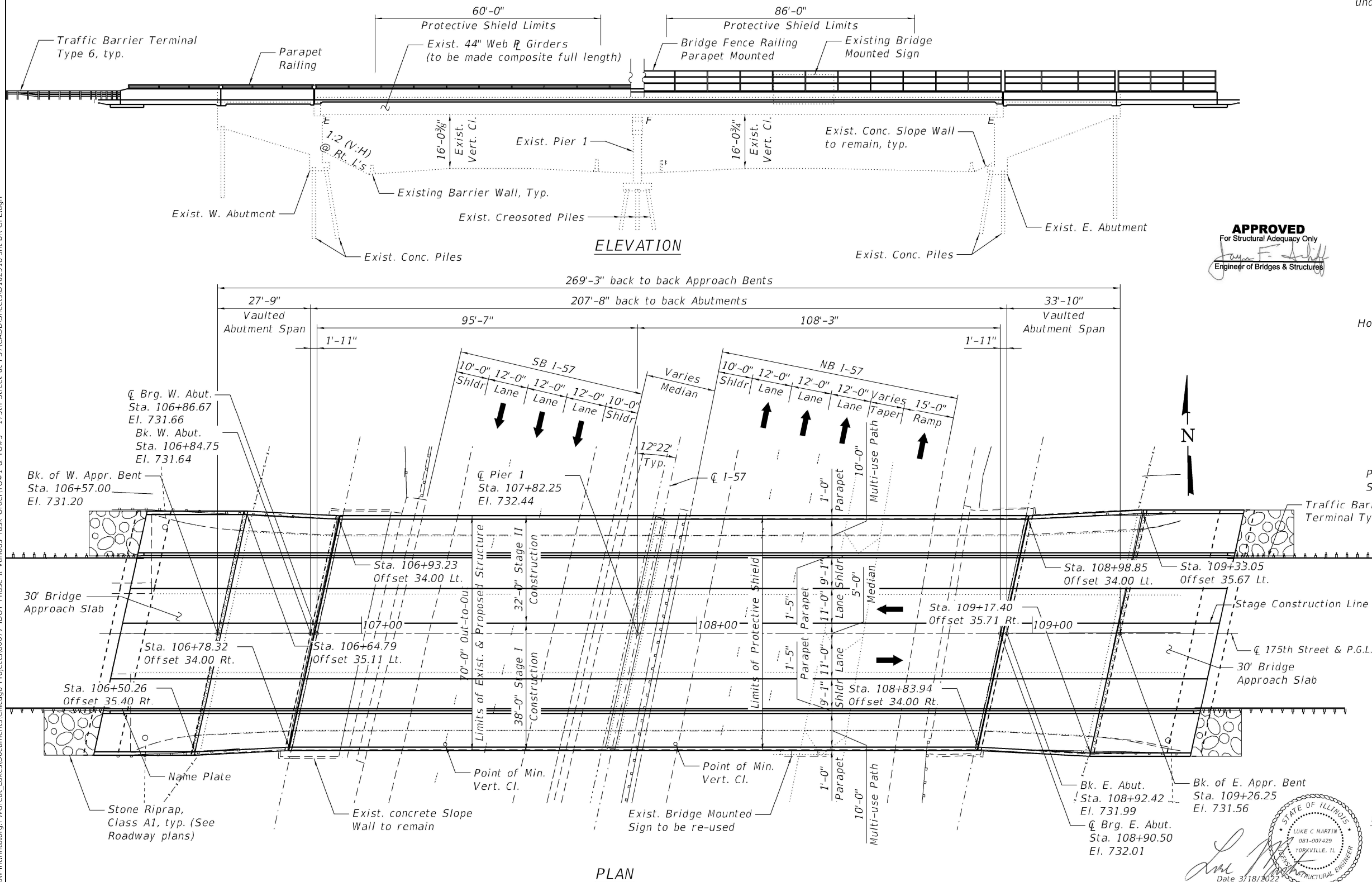
175TH ST OVER I-57

F.A.I. RTE. 57 - SEC. 2021-073-BDR

COOK COUNTY

STATION 107+82.25

STRUCTURE NO. 016-1095



APPROVED
For Structural Adequacy Only
[Signature]
Engineer of Bridges & Structures

[Signature]
Date 3/18/2022
Exp. 11/30/2022
STATE OF ILLINOIS
LUKE C. MARTIN
081-007429
YORKVILLE, IL
STRUCTURAL ENGINEER

MODEL: Default
FILE NAME: p:\v\p\w\m\h\h\h\p\w\g\real_lakes\Projects\80677 IDOT Phase II Various Task Order\TO#1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh-BR-GPE.dgn



USER NAME =	SUSERS\$	DESIGNED -	EG	REVISED -	
CHECKED -	LM	REVISOR -		REVISOR -	
PLOT SCALE =	\$SCALE\$	DRAWN -	EG	REVISOR -	
PLOT DATE =	\$DATES	CHECKED -	LM	REVISOR -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION
STRUCTURE NO. 016-1095

SHEET 1 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	34
CONTRACT NO. 62P10				

ILLINOIS FED. AID PROJECT

GENERAL NOTES:

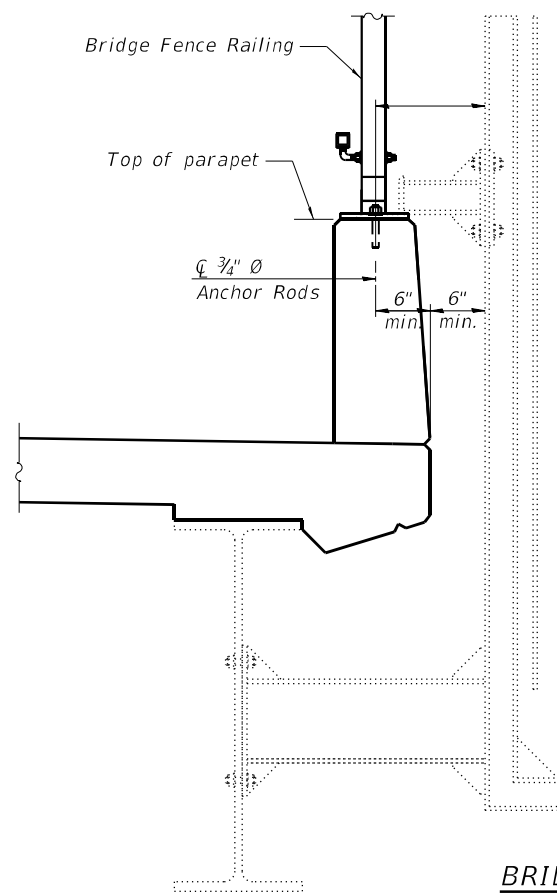
- Reinforcement bars designated (E) shall be epoxy coated.
- Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete. As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer. Any cracks that cannot be removed by grinding 1/4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.
- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Concrete Sealer shall be applied to the abutments and pier where structural concrete repair is performed.
- Cleaning and field painting of structural steel shall be done under a separate painting contract.
- The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
- The Contractor shall make allowance for the deflection of forms, shrinkage and settlement of falsework, in addition to allowance for dead load deflection. Forms for deck slab shall be removed prior to placement of bridge approach slab.
- All (embedded and separate) bearing plates, side retainers, anchor bolts, nuts, washers and pintles shall be galvanized according to AASHTO M111 or M232 as applicable.
- Existing Name Plate shall be cleaned and relocated next to new Name Plate. Cost included with Name Plates.
- Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with Removal of Existing Deck.
- Removal of the existing Vaulted Spans and Approach Slabs shall be included in the cost of Removal of Existing Concrete Deck.

INDEX OF SHEETS

- | | |
|-------|---|
| 1 | General Plan & Elevation |
| 2 | General Data |
| 3 | Construction Staging |
| 4 | Temporary Barrier for Stage Construction |
| 5-8 | Top of Slab Elevations |
| 9 | Top of West Approach Slab and Vaulted Span Elevations |
| 10 | Top of East Approach Slab and Vaulted Span Elevations |
| 11 | Superstructure |
| 12-13 | Superstructure Details |
| 14 | Vaulted Abutment Approach Spans |
| 15 | Vaulted Abutment Approach Span Details |
| 16 | Bridge Approach Slab Plan |
| 17 | Bridge Approach Slab Details |
| 18 | Bicycle Railing |
| 19 | Bridge Fence Railing, Parapet Mounted |
| 20-22 | Preformed Joint Strip Seal - Sidewalk |
| 23 | Structural Steel |
| 24 | Structural Steel Details |
| 25 | West Abutment Repair Details |
| 26 | East Abutment Repair Details |
| 27 | Pier Repair Details |
| 28 | Slope Walls Repair Details |
| 29 | Bar Splicer Assembly and Mechanical Splicer Details |

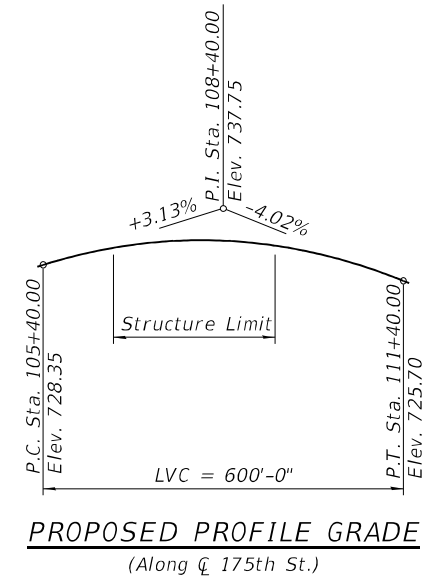
TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Removal of Existing Concrete Deck	Each	1		1
Protective Shield	Sq Yd	1,136		1,136
Concrete Structures	Cu Yd		46.2	46.2
Concrete Superstructure	Cu Yd	764.8		764.8
Bridge Deck Grooving	Sq Yd	1,642		1,642
Protective Coat	Sq Yd	3,163		3,163
Concrete Superstructure (Approach Slab)	Cu Yd	228.5		228.5
Furnishing and Erecting Structural Steel	Pound	22		22
Stud Shear Connectors	Each	5,472		5,472
Reinforcement Bars, Epoxy Coated	Pound	284,780		284,780
Bar Splicers	Each	1,111		1,111
Bridge Fence Railing	Foot	654		654
Parapet Railing	Foot	646		646
Name Plates	Each	1		1
Preformed Joint Strip Seal	Foot	144		144
Anchor Bolts, 1"	Each	1		1
Epoxy Crack Injection	Foot		126	126
Structural Repair of Concrete (depth equal to or less than 5 inches)	Sq Ft		244	244



**BRIDGE MOUNTED SIGN
STRUCTURE RE-ERECTION**

Notes:
The Sign Structure Upper Bracket and Anchor Rods shall be installed before the Bridge Fence Railing is installed.
The cost of Anchor Rods to be included with the cost of Bridge Mounted Sign Structure removal and replacement. See Roadway Signing Plan for payment of Sign Structure removal and replacement.



MODEL: Default
FILE NAME: p:\p\pww-int-hntb.org\pwwg\eat_Lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO# 1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh+BR-General\Dalad.dgn



USER NAME =	SUSERS\$	DESIGNED -	EG	REVISED -	
		CHECKED -	LM	REVISED -	
PLOT SCALE =	\$\$CALE\$	DRAWN -	EG	REVISED -	
PLOT DATE =	\$\$DATE\$	CHECKED -	LM	REVISED -	

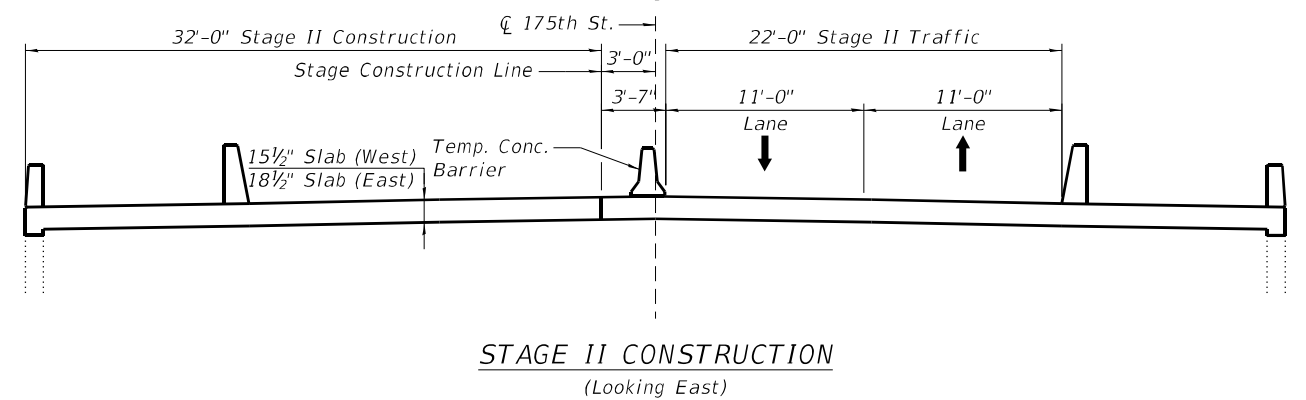
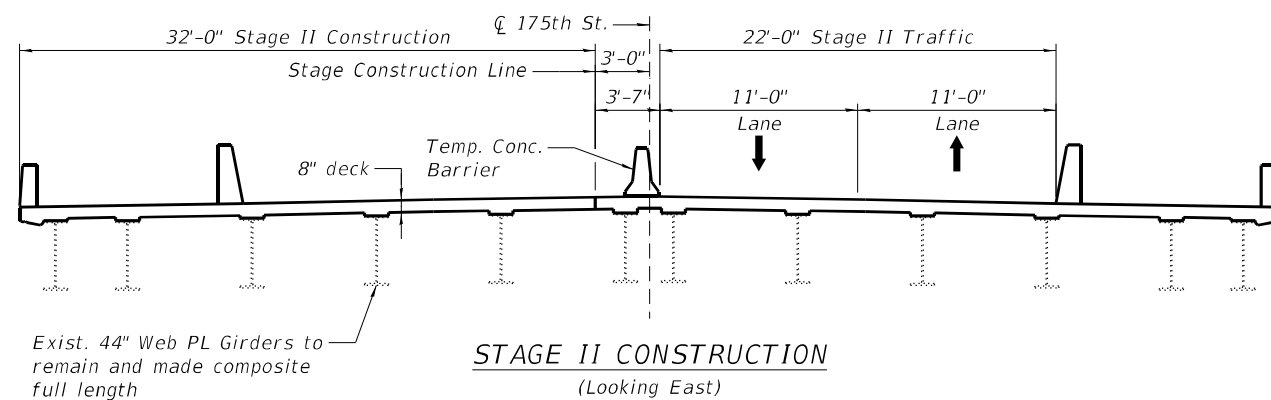
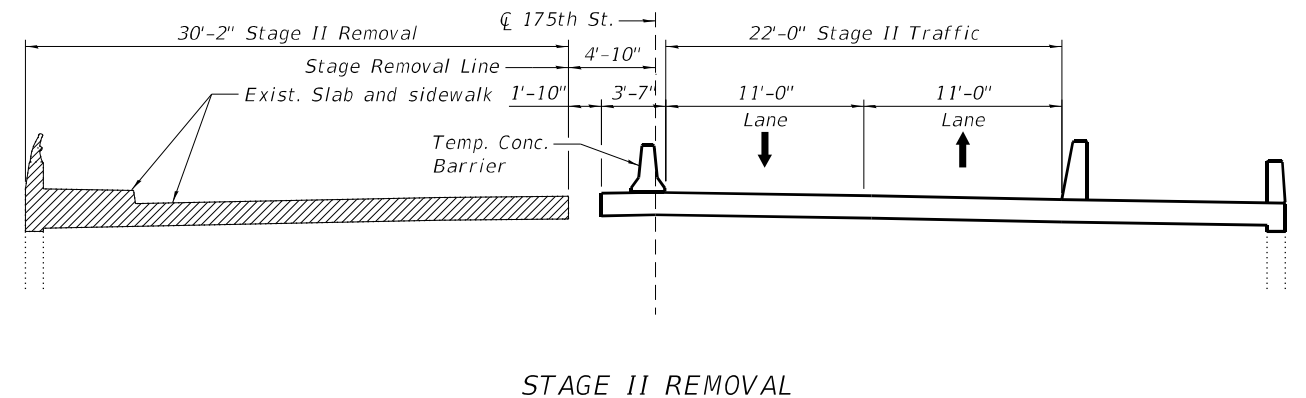
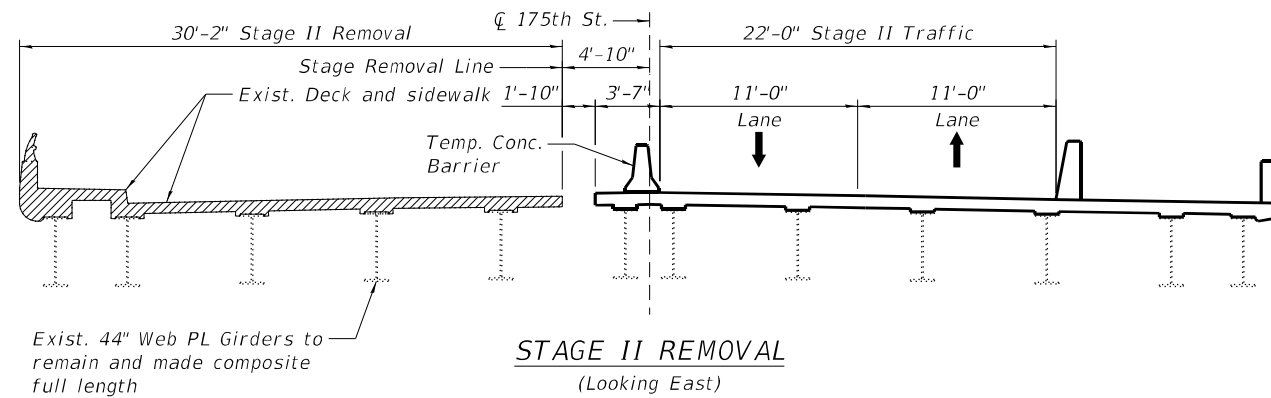
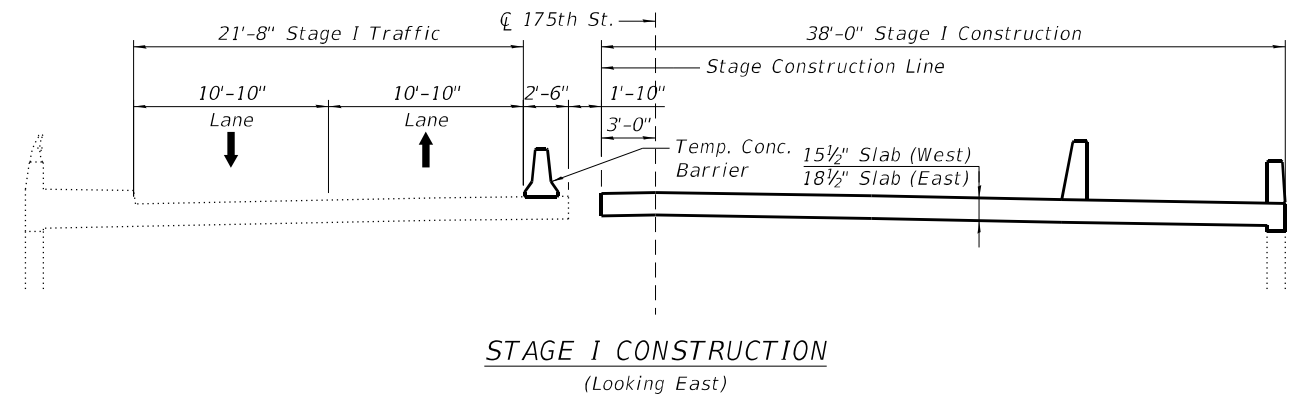
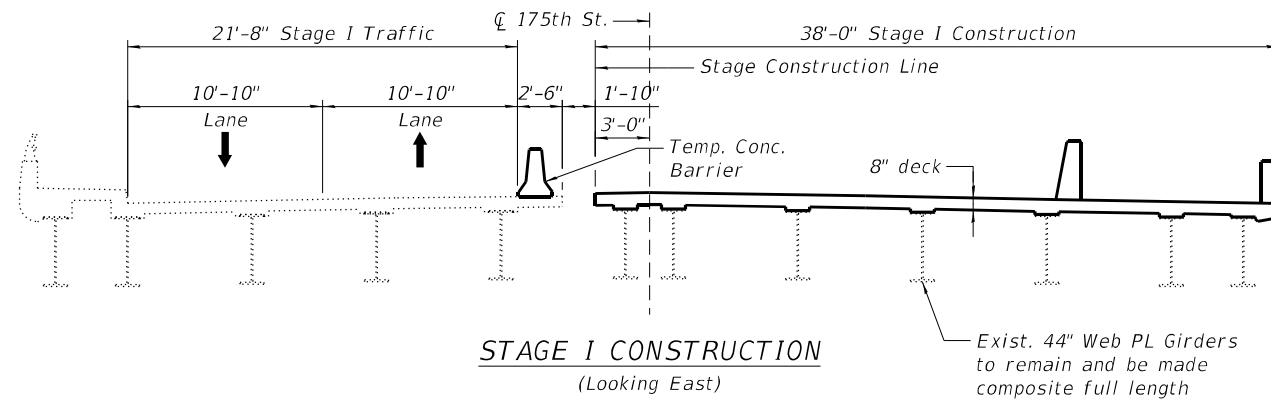
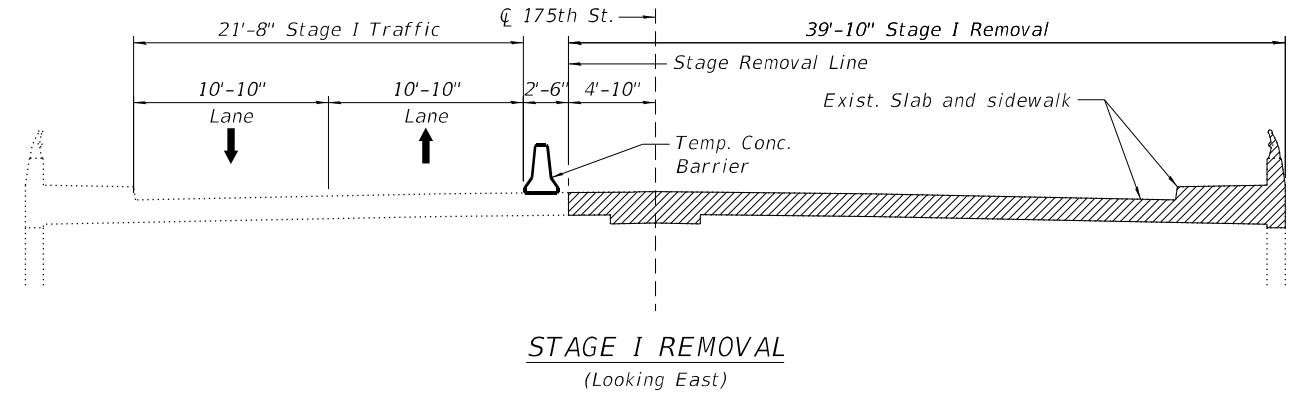
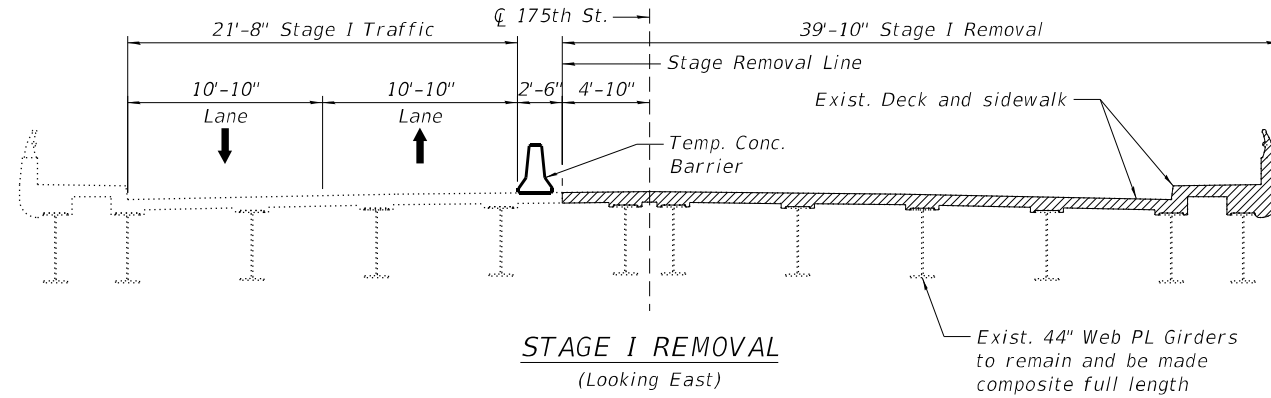
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL DATA
STRUCTURE NO. 016-1095**

SHEET 2 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	35
CONTRACT NO. 62P10				
ILLINOIS		FED. AID PROJECT		

MODEL: Default
FILE NAME: p:\v\p\w\m\h\hb\org\p\w\g\eat_Lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task_Order\TO# 1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh-BR-ConstructionStaging.dgn



STAGE CONSTRUCTION AT BRIDGE DECK

STAGE CONSTRUCTION AT VAULTED SLAB



USER NAME =	\$USER\$	DESIGNED -	EG	REVISED -	
CHECKED -		LM		REVISED -	
PLOT SCALE =	\$SCALE\$	DRAWN -	EG	REVISED -	
PLOT DATE =	\$DATES	CHECKED -	LM	REVISED -	

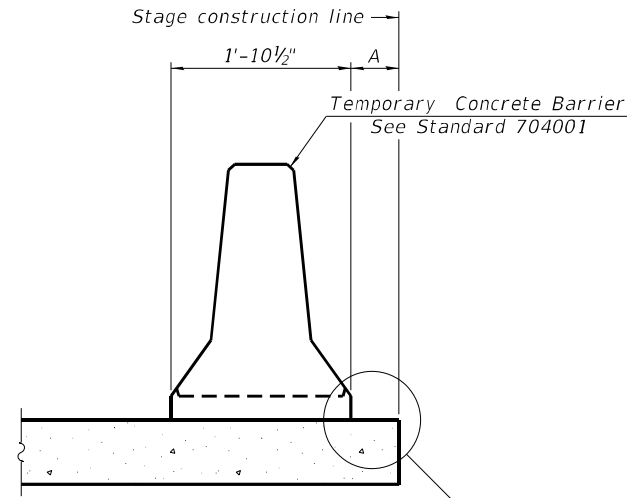
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION STAGING
STRUCTURE NO. 016-1095

SHEET 3 OF 29 SHEETS

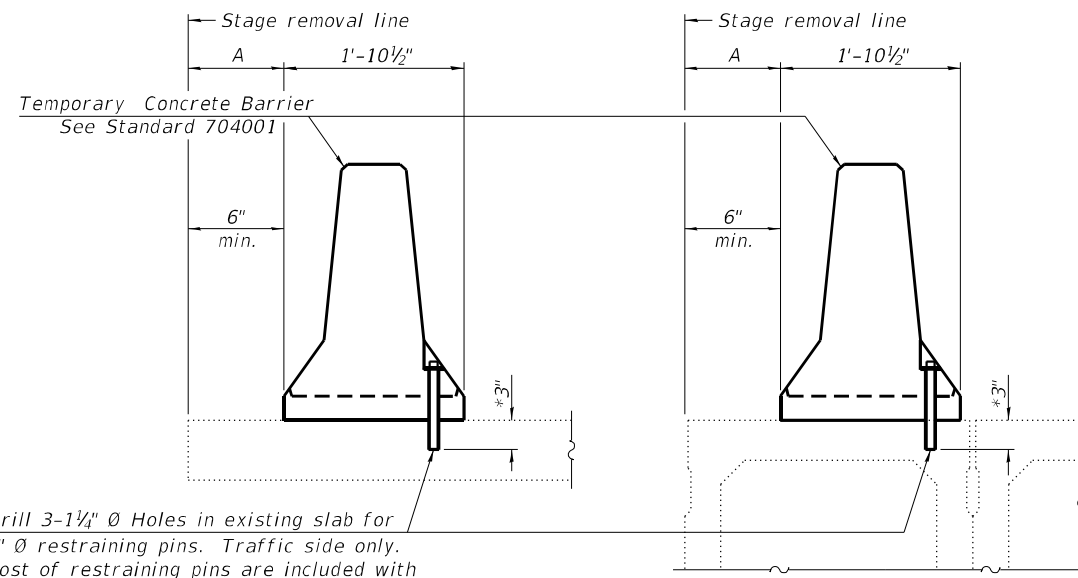
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	36
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

MODEL: Default
 FILE NAME: p:\w\pww-hl-hntb.org\pww\g\eat_Lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO#1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh-BR-TempConcBarrier.dgn



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



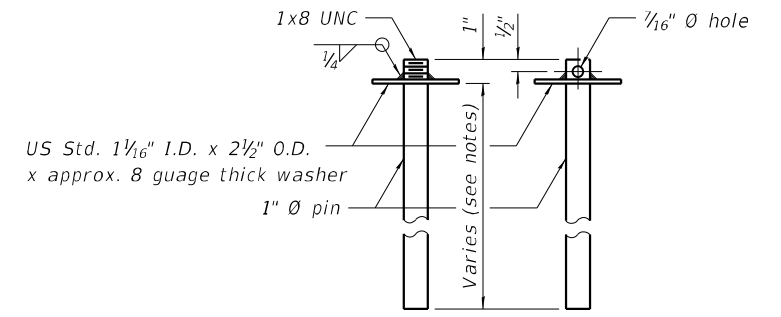
Drill 3-1 1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

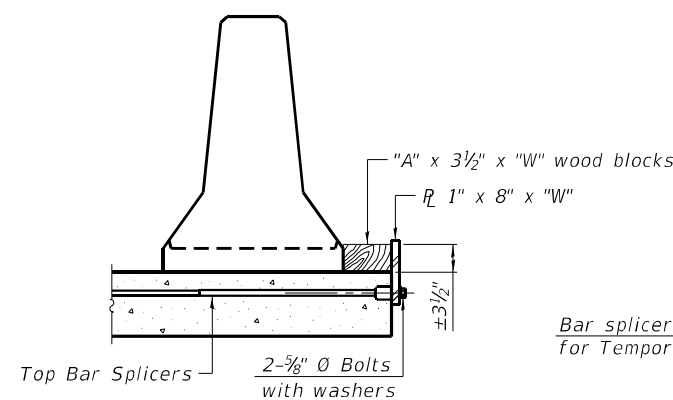
EXISTING DECK BEAM

* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

SECTIONS THRU SLAB OR DECK BEAM

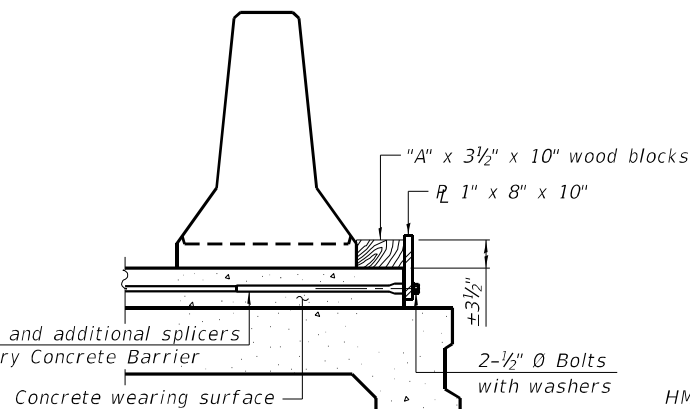


RESTRAINING PIN

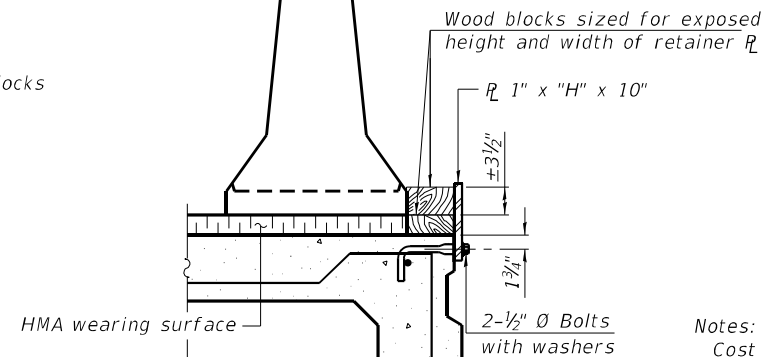


DETAIL I

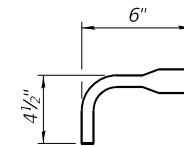
Bar splicers and additional splicers for Temporary Concrete Barrier



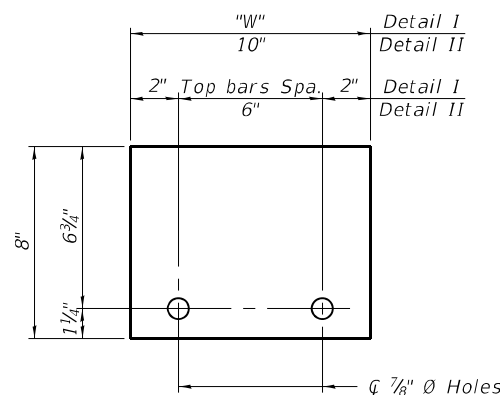
DETAIL II



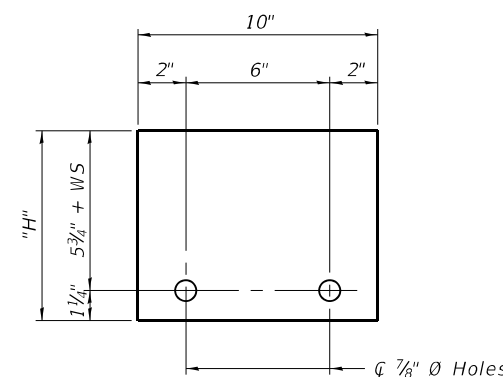
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate center of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

R-27 2-17-2017



USER NAME =	SUSERS\$	DESIGNED -	EG	REVISED -	
CHECKED -	LM	REVISED -			
PLOT SCALE =	\$\$CALE\$	DRAWN -	EG	REVISED -	
PLOT DATE =	\$\$DATE\$	CHECKED -	LM	REVISED -	

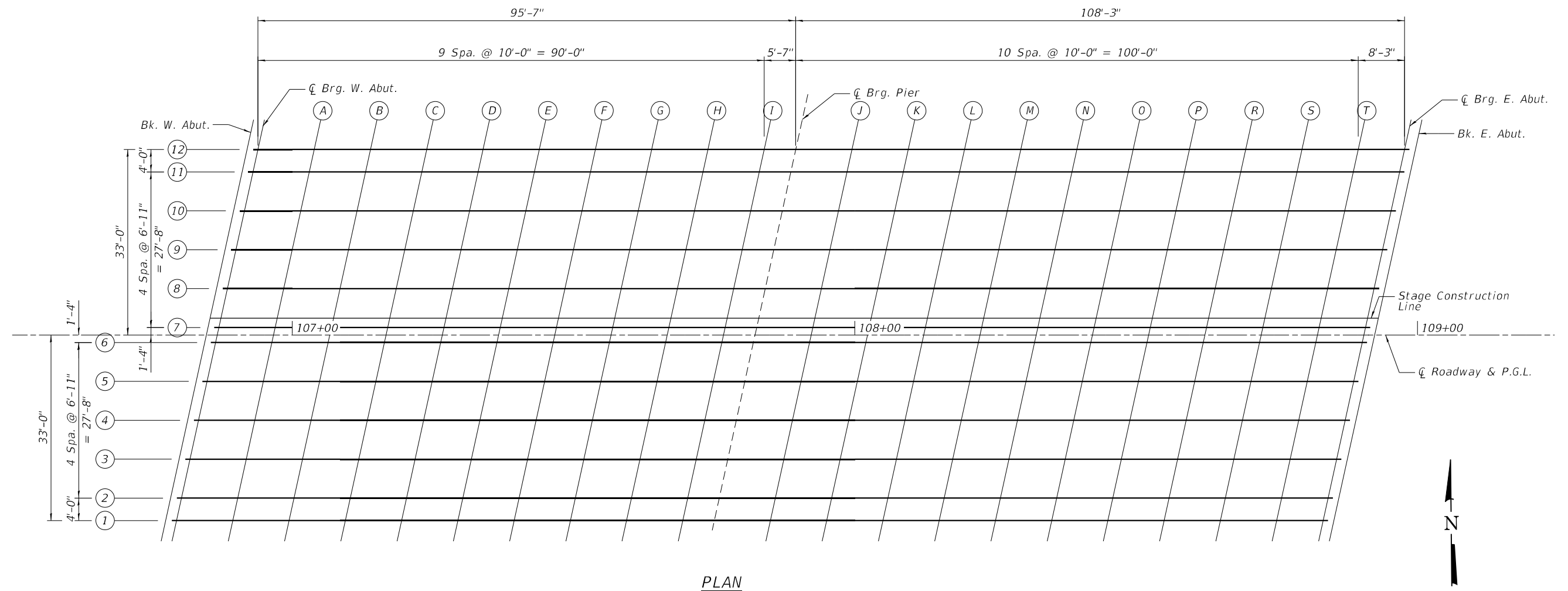
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
STRUCTURE NO. 016-1095**

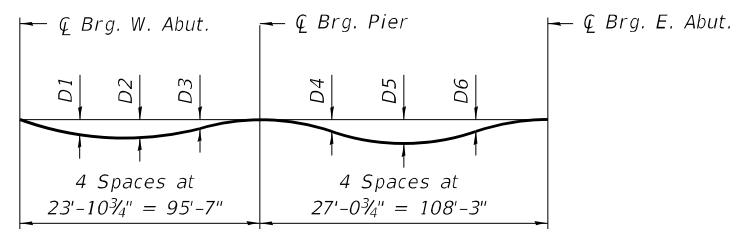
SHEET 4 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	37
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

MODEL: Default
 FILE NAME: p:\w\pww-hntb.org\pww\g\eat_Lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO# 1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh+BR-TopofSlabElev1.dgn



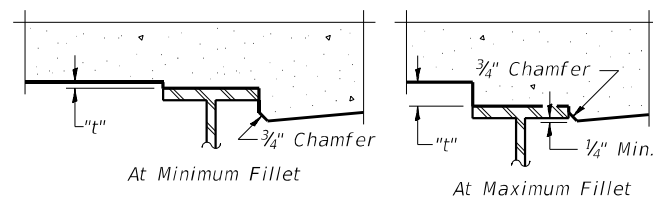
DL Deflections	D1	D2	D3	D4	D5	D6
Girders 1,12	1/4"	3/8"	1/8"	3/8"	7/8"	3/8"
Girders 2, 6, 7, 11	3/8"	3/8"	1/8"	1/2"	1 1/8"	7/8"
Girders 3-5, 8-10	1/2"	1/2"	1/8"	3/8"	1 3/8"	1 1/8"



DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)

Note:
 The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown below.



To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown above. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown above, minus slab thickness, equals the fillet heights "t" above top flange of beams.

FILLET HEIGHTS



USER NAME = \$USER\$	DESIGNED - EG	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED - LM	REVISED -
PLOT DATE = \$DATES\$	DRAWN - EG	REVISED -
	CHECKED - LM	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS
 STRUCTURE NO. 016-1095

SHEET 5 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	38
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

PGL

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	106+84.75	0.00	731.64	731.64
CL Brg. W. Abut.	106+86.67	0.00	731.66	731.66
A	106+96.67	0.00	731.80	731.81
B	107+06.67	0.00	731.92	731.95
C	107+16.67	0.00	732.03	732.06
D	107+26.67	0.00	732.12	732.16
E	107+36.67	0.00	732.21	732.24
F	107+46.67	0.00	732.28	732.30
G	107+56.67	0.00	732.34	732.35
H	107+66.67	0.00	732.39	732.39
I	107+76.67	0.00	732.43	732.43
CL Pier	107+82.25	0.00	732.44	732.44
J	107+92.25	0.00	732.46	732.47
K	108+02.25	0.00	732.47	732.50
L	108+12.25	0.00	732.46	732.52
M	108+22.25	0.00	732.45	732.52
N	108+32.25	0.00	732.42	732.50
O	108+42.25	0.00	732.38	732.47
P	108+52.25	0.00	732.32	732.41
R	108+62.25	0.00	732.26	732.34
S	108+72.25	0.00	732.18	732.24
T	108+82.25	0.00	732.09	732.12
CL Brg. E. Abut.	108+90.50	0.00	732.01	732.01
Bk. E. Abut.	108+92.42	0.00	731.99	731.99

BEAM 7

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	106+85.04	-1.33	731.62	731.62
CL Brg. W. Abut.	106+86.96	-1.33	731.65	731.65
A	106+96.96	-1.33	731.78	731.80
B	107+06.96	-1.33	731.90	731.93
C	107+16.96	-1.33	732.01	732.04
D	107+26.96	-1.33	732.11	732.14
E	107+36.96	-1.33	732.19	732.22
F	107+46.96	-1.33	732.26	732.29
G	107+56.96	-1.33	732.32	732.34
H	107+66.96	-1.33	732.37	732.37
I	107+76.96	-1.33	732.41	732.41
CL Pier	107+82.54	-1.33	732.42	732.42
J	107+92.54	-1.33	732.44	732.45
K	108+02.54	-1.33	732.45	732.48
L	108+12.54	-1.33	732.44	732.50
M	108+22.54	-1.33	732.43	732.50
N	108+32.54	-1.33	732.40	732.48
O	108+42.54	-1.33	732.36	732.45
P	108+52.54	-1.33	732.30	732.39
R	108+62.54	-1.33	732.24	732.31
S	108+72.54	-1.33	732.16	732.22
T	108+82.54	-1.33	732.07	732.10
CL Brg. E. Abut.	108+90.79	-1.33	731.99	731.99
Bk. E. Abut.	108+92.71	-1.33	731.97	731.97

STAGE CONSTRUCTION LINE

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	106+85.41	-3.00	731.60	731.60
CL Brg. W. Abut.	106+87.32	-3.00	731.63	731.63
A	106+97.32	-3.00	731.76	731.78
B	107+07.32	-3.00	731.88	731.91
C	107+17.32	-3.00	731.99	732.02
D	107+27.32	-3.00	732.08	732.12
E	107+37.32	-3.00	732.17	732.20
F	107+47.32	-3.00	732.24	732.26
G	107+57.32	-3.00	732.30	732.31
H	107+67.32	-3.00	732.35	732.35
I	107+77.32	-3.00	732.39	732.38
CL Pier	107+82.91	-3.00	732.40	732.40
J	107+92.91	-3.00	732.42	732.43
K	108+02.91	-3.00	732.42	732.45
L	108+12.91	-3.00	732.42	732.47
M	108+22.91	-3.00	732.40	732.47
N	108+32.91	-3.00	732.37	732.46
O	108+42.91	-3.00	732.33	732.42
P	108+52.91	-3.00	732.28	732.36
R	108+62.91	-3.00	732.21	732.29
S	108+72.91	-3.00	732.13	732.19
T	108+82.91	-3.00	732.04	732.07
CL Brg. E. Abut.	108+91.16	-3.00	731.96	731.96
Bk. E. Abut.	108+93.07	-3.00	731.94	731.94

BEAM 8

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	106+86.56	-8.25	731.54	731.54
CL Brg. W. Abut.	106+88.48	-8.25	731.56	731.56
A	106+98.48	-8.25	731.70	731.71
B	107+08.48	-8.25	731.81	731.85
C	107+18.48	-8.25	731.92	731.96
D	107+28.48	-8.25	732.02	732.06
E	107+38.48	-8.25	732.10	732.14
F	107+48.48	-8.25	732.17	732.20
G	107+58.48	-8.25	732.23	732.24
H	107+68.48	-8.25	732.27	732.28
I	107+78.48	-8.25	732.31	732.31
CL Pier	107+84.06	-8.25	732.32	732.32
J	107+94.06	-8.25	732.34	732.36
K	108+04.06	-8.25	732.35	732.38
L	108+14.06	-8.25	732.34	732.40
M	108+24.06	-8.25	732.32	732.41
N	108+34.06	-8.25	732.29	732.39
O	108+44.06	-8.25	732.24	732.36
P	108+54.06	-8.25	732.19	732.30
R	108+64.06	-8.25	732.12	732.22
S	108+74.06	-8.25	732.04	732.11
T	108+84.06	-8.25	731.95	731.99
CL Brg. E. Abut.	108+92.31	-8.25	731.87	731.87
Bk. E. Abut.	108+94.23	-8.25	731.85	731.85

BEAM 9

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	106+88.08	-15.17	731.45	731.45
CL Brg. W. Abut.	106+89.99	-15.17	731.47	731.47
A	106+99.99	-15.17	731.60	731.62
B	107+09.99	-15.17	731.72	731.75
C	107+19.99	-15.17	731.82	731.87
D	107+29.99	-15.17	731.92	731.96
E	107+39.99	-15.17	732.00	732.04
F	107+49.99	-15.17	732.07	732.09
G	107+59.99	-15.17	732.12	732.14
H	107+69.99	-15.17	732.17	732.17
I	107+79.99	-15.17	732.20	732.20
CL Pier	107+85.58	-15.17	732.22	732.22
J	107+95.58	-15.17	732.23	732.24
K	108+05.58	-15.17	732.23	732.27
L	108+15.58	-15.17	732.22	732.29
M	108+25.58	-15.17	732.20	732.29
N	108+35.58	-15.17	732.17	732.27
O	108+45.58	-15.17	732.13	732.24
P	108+55.58	-15.17	732.07	732.18
R	108+65.58	-15.17	732.00	732.09
S	108+75.58	-15.17	731.92	731.99
T	108+85.58	-15.17	731.83	731.86
CL Brg. E. Abut.	108+93.83	-15.17	731.74	731.74
Bk. E. Abut.	108+95.74	-15.17	731.72	731.72

BEAM 10

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	106+89.59	-22.08	731.33	731.33
CL Brg. W. Abut.	106+91.51	-22.08	731.36	731.36
A	107+01.51	-22.08	731.48	731.50
B	107+11.51	-22.08	731.60	731.63
C	107+21.51	-22.08	731.70	731.74
D	107+31.51	-22.08	731.79	731.84
E	107+41.51	-22.08	731.87	731.91
F	107+51.51	-22.08	731.94	731.97
G	107+61.51	-22.08	731.99	732.01
H	107+71.51	-22.08	732.04	732.04
I	107+81.51	-22.08	732.07	732.07
CL Pier	107+87.09	-22.08	732.08	732.08
J	107+97.09	-22.08	732.09	732.11
K	108+07.09	-22.08	732.09	732.13
L	108+17.09	-22.08	732.08	732.15
M	108+27.09	-22.08	732.06	732.15
N	108+37.09	-22.08	732.03	732.13
O	108+47.09	-22.08	731.98	732.09
P	108+57.09	-22.08	731.92	732.03
R	108+67.09	-22.08	731.85	731.94
S	108+77.09	-22.08	731.77	731.83
T	108+87.09	-22.08	731.67	731.71
CL Brg. E. Abut.	108+95.34	-22.08	731.59	731.59
Bk. E. Abut.	108+97.26	-22.08	731.57	731.57

MODEL: Default
 FILE NAME: p:\work\hntb\org\PWG\raat_Lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO# 1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh-bb-Topo\SlabElev.3.dgn



USER NAME = \$USERS\$	DESIGNED - EG	REVISED -
CHECKED - LM	REVISIONS -	
PLOT SCALE = \$SCALES\$	DRAWN - EG	REVISED -
PLOT DATE = \$DATES\$	CHECKED - LM	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS
 STRUCTURE NO. 016-1095

SHEET 7 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	40
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

BEAM 11

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	106+91.11	-29.00	731.44	731.44
CL Brg. W. Abut.	106+93.03	-29.00	731.46	731.46
A	107+03.03	-29.00	731.59	731.60
B	107+13.03	-29.00	731.70	731.73
C	107+23.03	-29.00	731.80	731.84
D	107+33.03	-29.00	731.89	731.93
E	107+43.03	-29.00	731.97	732.00
F	107+53.03	-29.00	732.03	732.06
G	107+63.03	-29.00	732.09	732.10
H	107+73.03	-29.00	732.13	732.13
I	107+83.03	-29.00	732.16	732.16
CL Pier	107+88.61	-29.00	732.17	732.17
J	107+98.61	-29.00	732.18	732.19
K	108+08.61	-29.00	732.18	732.21
L	108+18.61	-29.00	732.17	732.22
M	108+28.61	-29.00	732.14	732.21
N	108+38.61	-29.00	732.11	732.19
O	108+48.61	-29.00	732.06	732.15
P	108+58.61	-29.00	732.00	732.09
R	108+68.61	-29.00	731.92	732.00
S	108+78.61	-29.00	731.84	731.89
T	108+88.61	-29.00	731.74	731.77
CL Brg. E. Abut.	108+96.86	-29.00	731.66	731.66
Bk. E. Abut.	108+98.78	-29.00	731.63	731.63

BEAM 12

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	106+91.99	-33.00	731.51	731.51
CL Brg. W. Abut.	106+93.90	-33.00	731.53	731.53
A	107+03.90	-33.00	731.66	731.67
B	107+13.90	-33.00	731.77	731.79
C	107+23.90	-33.00	731.87	731.90
D	107+33.90	-33.00	731.96	731.99
E	107+43.90	-33.00	732.03	732.06
F	107+53.90	-33.00	732.10	732.12
G	107+63.90	-33.00	732.15	732.16
H	107+73.90	-33.00	732.19	732.19
I	107+83.90	-33.00	732.22	732.22
CL Pier	107+89.49	-33.00	732.23	732.23
J	107+99.49	-33.00	732.24	732.25
K	108+09.49	-33.00	732.24	732.26
L	108+19.49	-33.00	732.23	732.26
M	108+29.49	-33.00	732.20	732.25
N	108+39.49	-33.00	732.16	732.23
O	108+49.49	-33.00	732.11	732.18
P	108+59.49	-33.00	732.05	732.12
R	108+69.49	-33.00	731.98	732.04
S	108+79.49	-33.00	731.89	731.93
T	108+89.49	-33.00	731.80	731.82
CL Brg. E. Abut.	108+97.74	-33.00	731.71	731.71
Bk. E. Abut.	108+99.65	-33.00	731.68	731.68

MODEL: Default
 FILE NAME: p:\w\pww-int\hntb.org\pww\creat_Lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO# 1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh+BR-Topo\SlabElev.dgn



USER NAME =	\$USERS\$	DESIGNED -	EG	REVISED -	
		CHECKED -	LM	REVISED -	
PLOT SCALE =	\$SCALE\$	DRAWN -	EG	REVISED -	
PLOT DATE =	\$DATES\$	CHECKED -	LM	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS
STRUCTURE NO. 016-1095

SHEET 8 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	41
CONTRACT NO. 62P10				
ILLINOIS		FED. AID PROJECT		

NORTH EDGE OF MULTI-USE PATH

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Appr. Slab	106+35.78	-35.37	730.61
A1	106+45.78	-35.37	730.81
A2	106+55.78	-35.37	730.99
W. End of W. Vaulted Span	106+65.78	-35.37	731.16
V1	106+75.67	-34.88	731.30
V2	106+85.56	-34.38	731.44
E. End of W. Vaulted Span	106+93.23	-34.00	731.54

INSIDE FACE OF NORTH PARAPET

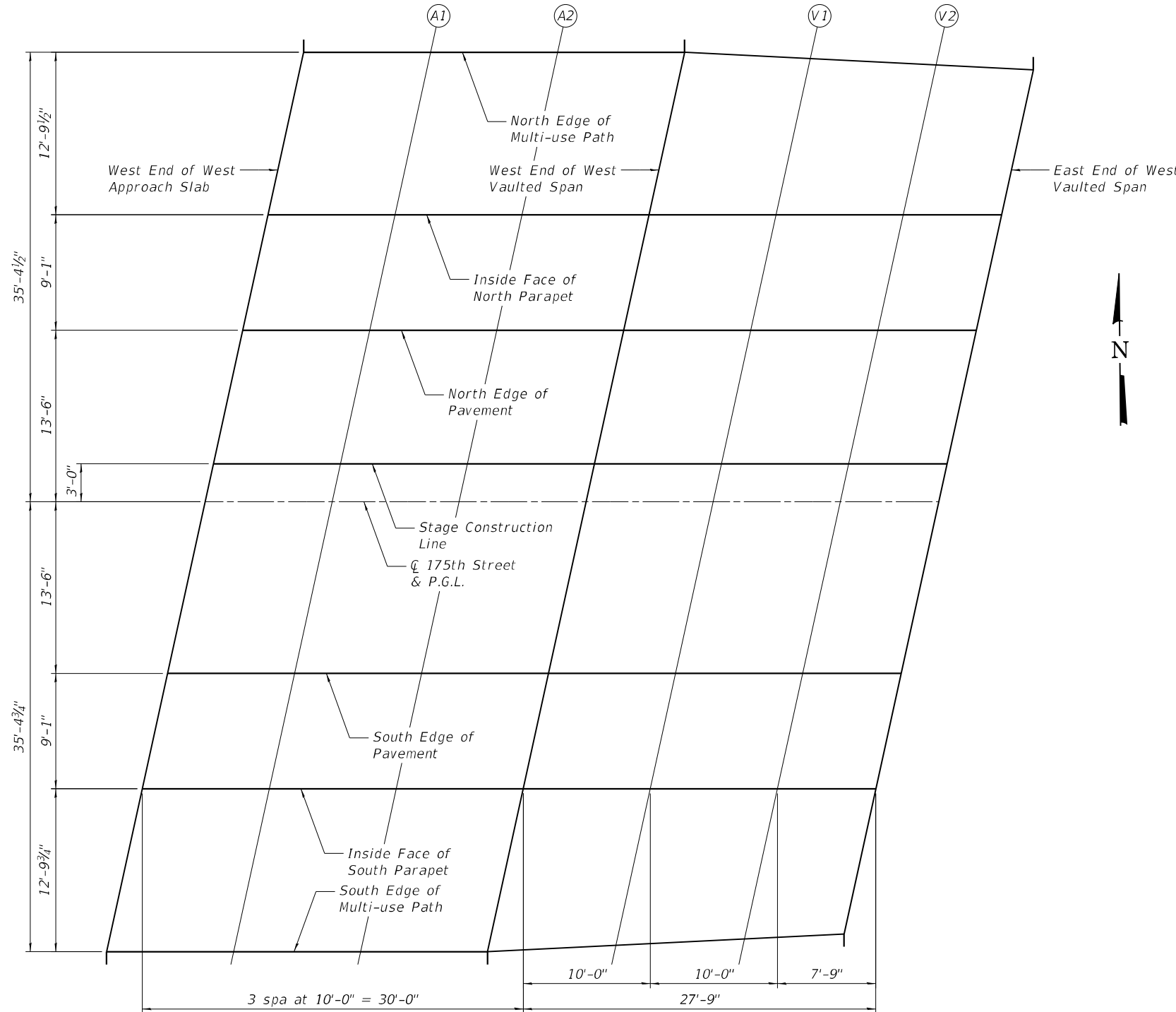
Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Appr. Slab	106+32.98	-22.58	730.36
A1	106+42.98	-22.58	730.56
A2	106+52.98	-22.58	730.75
W. End of W. Vaulted Span	106+62.98	-22.58	730.92
V1	106+72.98	-22.58	731.08
V2	106+82.98	-22.58	731.23
E. End of W. Vaulted Span	106+90.72	-22.58	731.33

NORTH EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Appr. Slab	106+30.98	-13.50	730.51
A1	106+40.98	-13.50	730.70
A2	106+50.98	-13.50	730.89
W. End of W. Vaulted Span	106+60.98	-13.50	731.07
V1	106+70.98	-13.50	731.23
V2	106+80.98	-13.50	731.38
E. End of W. Vaulted Span	106+88.73	-13.50	731.49

STAGE CONSTRUCTION LINE

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Appr. Slab	106+28.68	-3.00	730.62
A1	106+38.68	-3.00	730.82
A2	106+48.68	-3.00	731.01
W. End of W. Vaulted Span	106+58.68	-3.00	731.18
V1	106+68.68	-3.00	731.35
V2	106+78.68	-3.00	731.50
E. End of W. Vaulted Span	106+86.43	-3.00	731.62



PLAN

West Approach (W.B.)

175TH STREET & P.G.L.

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Appr. Slab	106+28.02	0.00	730.65
A1	106+38.02	0.00	730.85
A2	106+48.02	0.00	731.04
W. End of W. Vaulted Span	106+58.02	0.00	731.22
V1	106+68.02	0.00	731.38
V2	106+78.02	0.00	731.54
E. End of W. Vaulted Span	106+85.77	0.00	731.65

SOUTH EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Appr. Slab	106+25.06	13.50	730.38
A1	106+35.06	13.50	730.59
A2	106+45.06	13.50	730.78
W. End of W. Vaulted Span	106+55.06	13.50	730.96
V1	106+65.06	13.50	731.13
V2	106+75.06	13.50	731.29
E. End of W. Vaulted Span	106+82.81	13.50	731.41

INSIDE FACE OF SOUTH PARAPET

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Appr. Slab	106+23.07	-22.58	730.16
A1	106+33.07	-22.58	730.37
A2	106+43.07	-22.58	730.56
W. End of W. Vaulted Span	106+53.07	-22.58	730.75
V1	106+63.07	-22.58	730.92
V2	106+73.07	-22.58	731.08
E. End of W. Vaulted Span	106+80.82	-22.58	731.20

SOUTH EDGE OF MULTI-USE PATH

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Appr. Slab	106+20.26	-35.40	730.29
A1	106+30.26	-35.40	730.50
A2	106+40.26	-35.40	730.70
W. End of W. Vaulted Span	106+50.26	-35.40	730.89
V1	106+60.37	-34.90	731.06
V2	106+70.48	-34.39	731.22
E. End of W. Vaulted Span	106+78.32	-34.00	731.33

MODEL: Default
 FILE NAME: p:\w\pww-intl\hntb.org\pww\reg\Chicago Projects\80677 IDOT Phase II Various Task Order\TO# 1 & TO#5 - 175th Street at 157th CADD\Sheets\102918-sh-bdr-Topo\Approach\VaultSlabElev.dgn

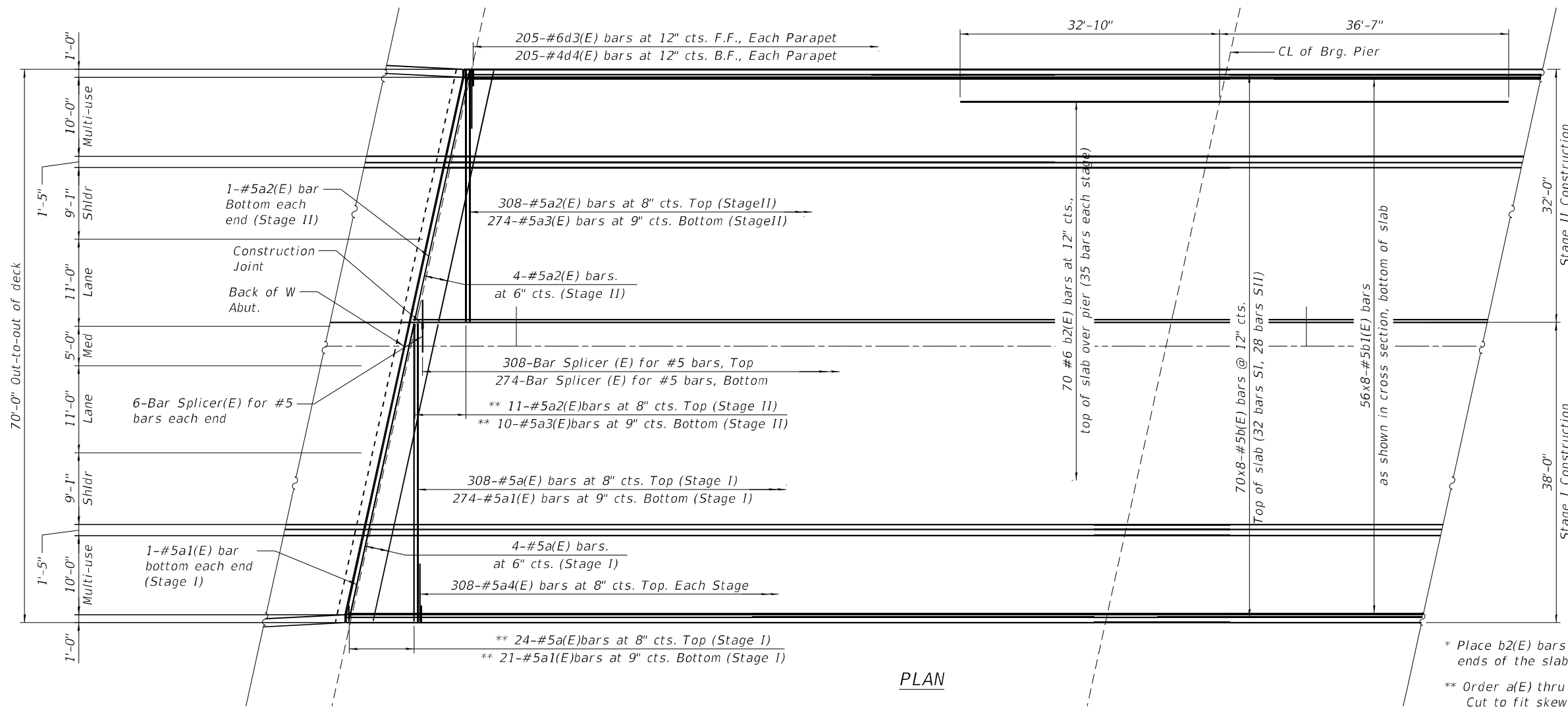


USER NAME = \$USERS\$	DESIGNED - EG	REVISED -
CHECKED - LM	REVISIONS -	
PLOT SCALE = \$SCALE\$	DRAWN - EG	REVISED -
PLOT DATE = \$DATES\$	CHECKED - LM	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

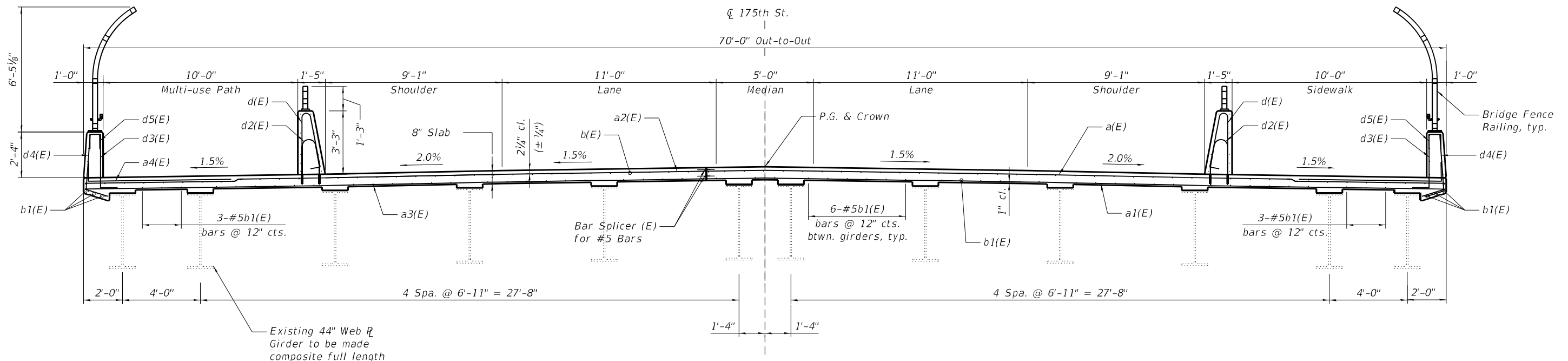
**TOP OF WEST APPROACH SLAB AND VAULTED SPAN ELEVATIONS
 STRUCTURE NO. 016-1095**

F.A.I. RTE. 57	SECTION 2021-073-BDR	COUNTY COOK	TOTAL SHEETS 89	SHEET NO. 42
CONTRACT NO. 62P10			ILLINOIS FED. AID PROJECT	



PLAN

* Place b2(E) bars in opposite ends of the slab
 ** Order a(E) thru a3(E) bars full length. Cut to fit skew and use remainder of bars in opposite end.



CROSS SECTION THRU BRIDGE DECK
 (Looking East)



USER NAME =	\$USERS\$	DESIGNED -	LM	REVISED -	
		CHECKED -	LM	REVISED -	
PLOT SCALE =	\$SCALE\$	DRAWN -	JM	REVISED -	
PLOT DATE =	\$DATES\$	CHECKED -	LM	REVISED -	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE
 STRUCTURE NO. 016-1095

SHEET 11 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	44
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

FILLET REINFORCEMENT

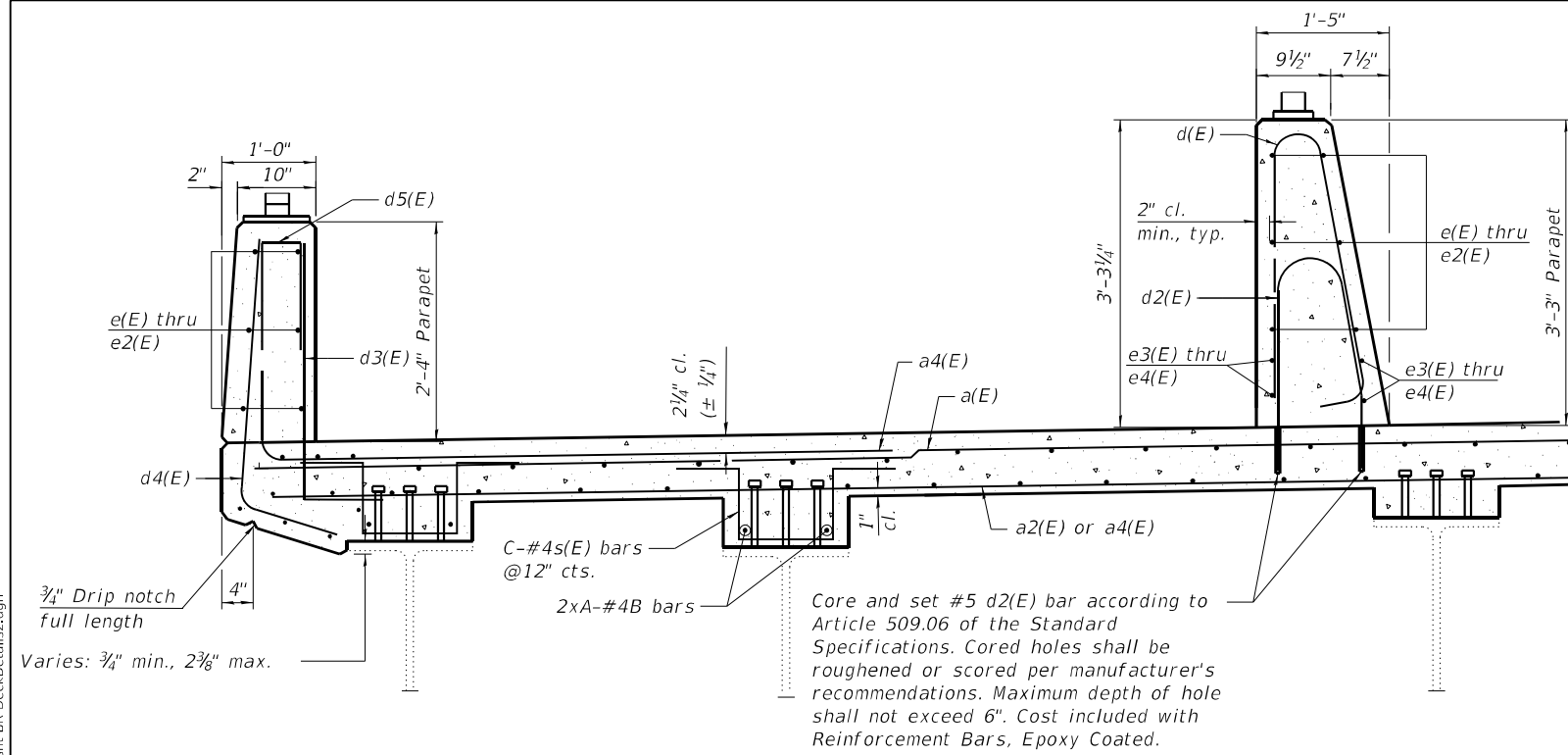
BEAM	C #s(E) bars	B (BAR DESIGNATION)	A (NUMBER OF LENGTHS)	LOCATION (ft)
1	72	b3(E)	3	15' to 87'
1	87	b4(E)	4	109' to 196'
2	51	b5(E)	2	34' to 85'
2	76	b6(E)	3	112' to 188'
7	15	b7(E)	1	141' to 156'
11	86	b8(E)	4	0' to 86'
11	70	b9(E)	3	107' to 177'
12	190	b10(E)	7	0' to 190'

FilletReinforcement to be provided where fillet height exceed 6". Start and End locations are measured from end of beams at West Abutment, to be verified in field by the Contractor.

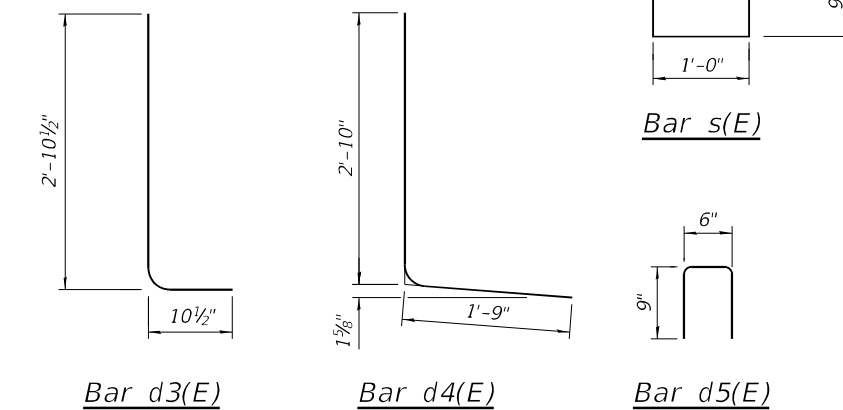
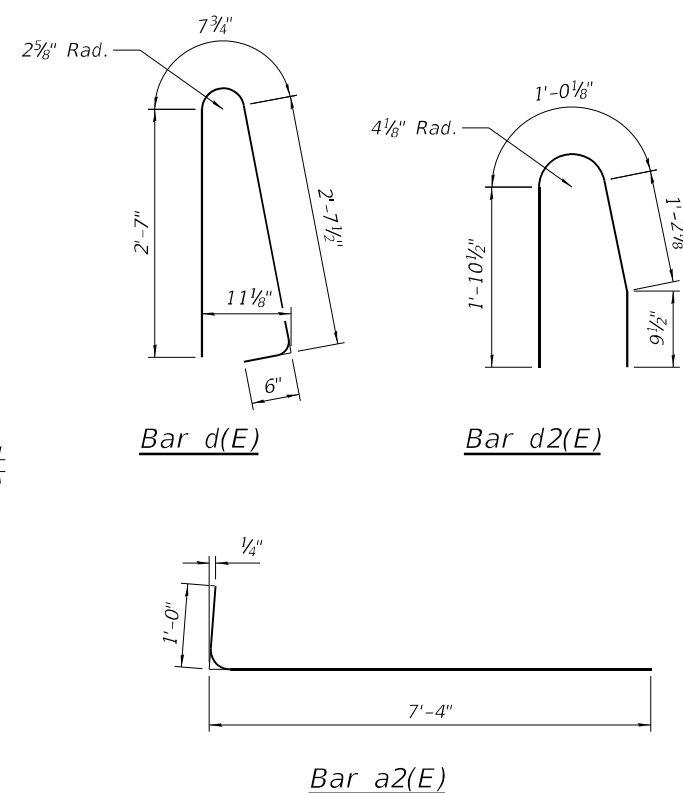
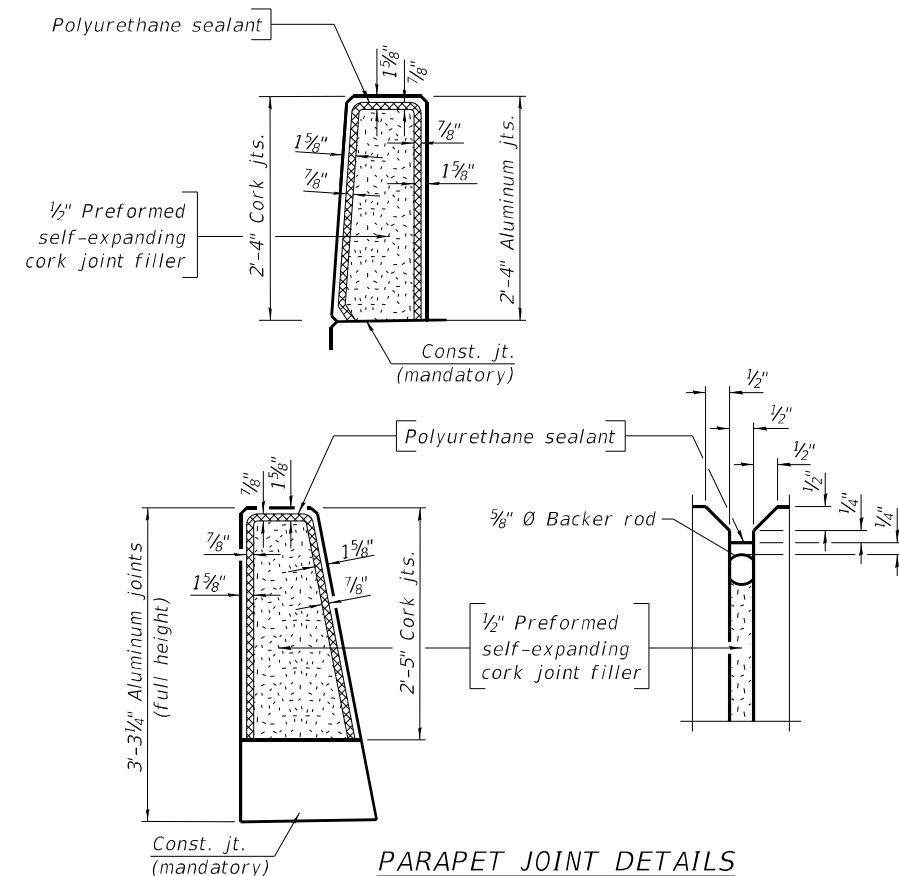
**SUPERSTRUCTURE
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	308	#5	37'-8"	—
a1(E)	308	#5	31'-8"	—
a2(E)	274	#5	36'-8"	—
a3(E)	274	#5	30'-8"	—
a4(E)	308	#6	8'-4"	└
b(E)	560	#5	28'-8"	—
b1(E)	448	#5	27'-8"	—
b2(E)	140	#6	26'-0"	—
b3(E)	216	#4	25'-6"	—
b4(E)	348	#4	24'-6"	—
b5(E)	102	#4	26'-7"	—
b6(E)	228	#4	26'-10"	—
b7(E)	15	#4	15'-0"	—
b8(E)	344	#4	23'-3"	—
b9(E)	210	#4	24'-10"	—
b10(E)	190	#4	29'-2"	—
d(E)	616	#5	6'-5"	∩
d2(E)	616	#5	4'-11"	∩
d3(E)	410	#6	3'-9"	L
d4(E)	410	#4	4'-8"	L
d5(E)	88	#4	2'-0"	n
e(E)	96	#4	19'-8"	—
e1(E)	64	#4	16'-3"	—
e2(E)	120	#4	18'-3"	—
e3(E)	24	#4	28'-2"	—
e4(E)	32	#4	24'-11"	—
s(E)	647	#4	3'-1"	└
Description	Unit	Quantity		
Concrete Superstructure	Cu Yd	505.3		
Bridge Deck Grooving	Sq Yd	1,030		
Protective Coat	Sq Yd	1,0965		
Reinforcement Bars, Epoxy Coated	Pound	125,180		

Bars indicated thus 1 x 2-#4 etc. indicates 1 line of bars with 2 lengths per line.



SECTION THRU PARAPET AND MULTI-USE PATH



Notes:
 The 1/8" aluminum sheet shall be ASTM B 209 alloy 3003-H14 and coated to minimize reaction with wet concrete. Cost included with Concrete Superstructure.
 The polyurethane sealant shall be according to Article 1050.04 of the Std. Spec. and the color shall be gray.
 Headed bars shall conform to ASTM A970 with threaded attachment; Class HA; and reinforcement bars conforming to ASTM A706. Cost included with Reinforcement Bars, Epoxy Coated.

MODEL: Default
 FILE NAME: p:\w\pww\m\hntb.org\pww\real_lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO#1 & TO#5 - 175th Street at 157th\CAD\Drawings\102918-sh-BR-DeckDetails2.dgn



USER NAME =	SUSERS\$	DESIGNED -	LM	REVISED -	
CHECKED -		CHECKED -	LM	REVISED -	
PLOT SCALE =	\$\$SCALE\$	DRAWN -	JM	REVISED -	
PLOT DATE =	\$\$DATE\$	CHECKED -	LM	REVISED -	

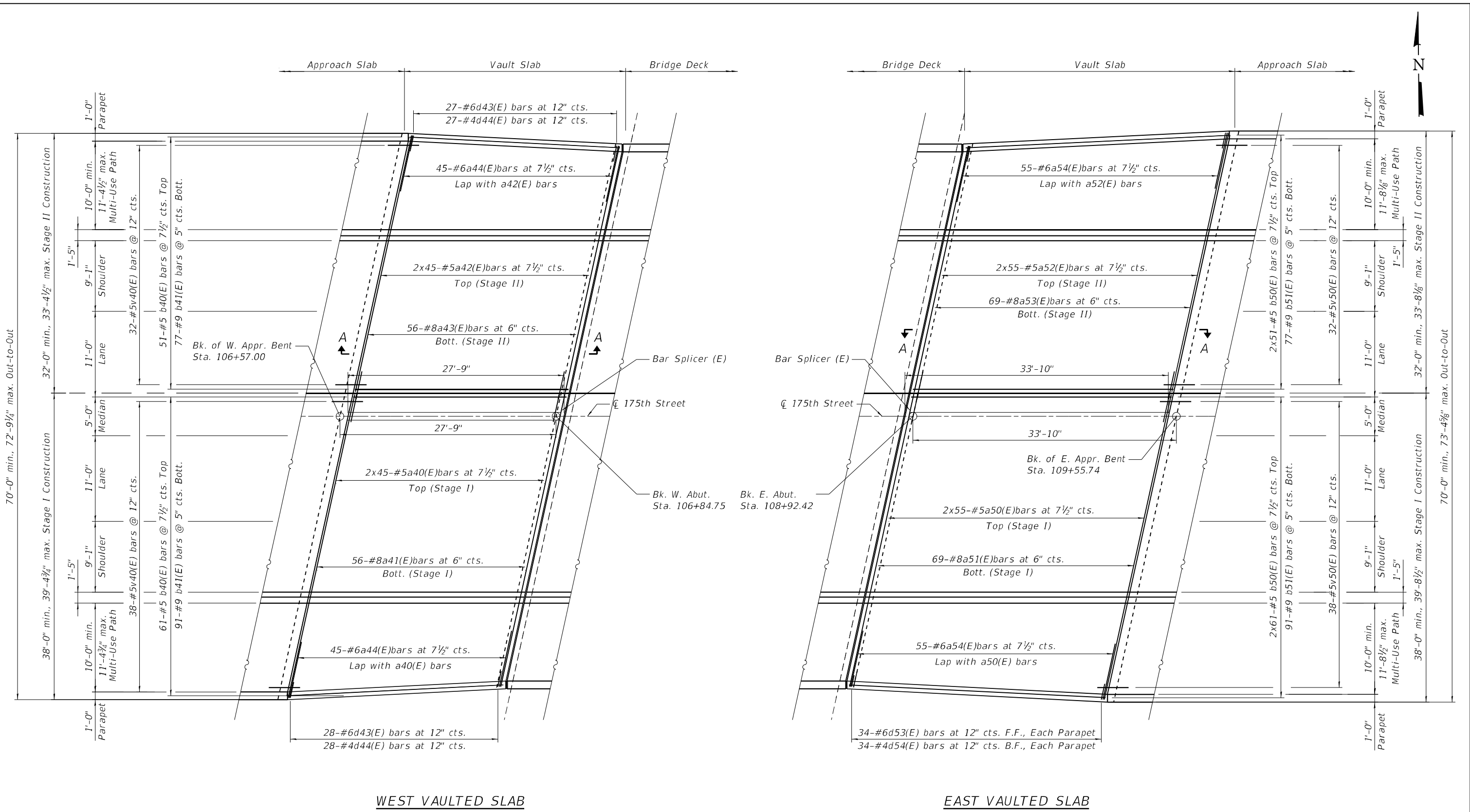
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUPERSTRUCTURE DETAILS
STRUCTURE NO. 016-1095**

SHEET 13 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	46
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

MODEL: Default
 FILE NAME: p:\w\pww\hntb\org\pwwg\eat_lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO#1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh-RR-Vaults\slab1.dgn
 2/4/2022 9:17:33 AM



WEST VAULTED SLAB

EAST VAULTED SLAB

PLAN



USER NAME =	SUSERS\$	DESIGNED -	EG	REVISED -	
		CHECKED -	LM	REVISED -	
PLOT SCALE =	\$\$SCALE\$	DRAWN -	EG	REVISED -	
PLOT DATE =	\$\$DATE\$	CHECKED -	LM	REVISED -	

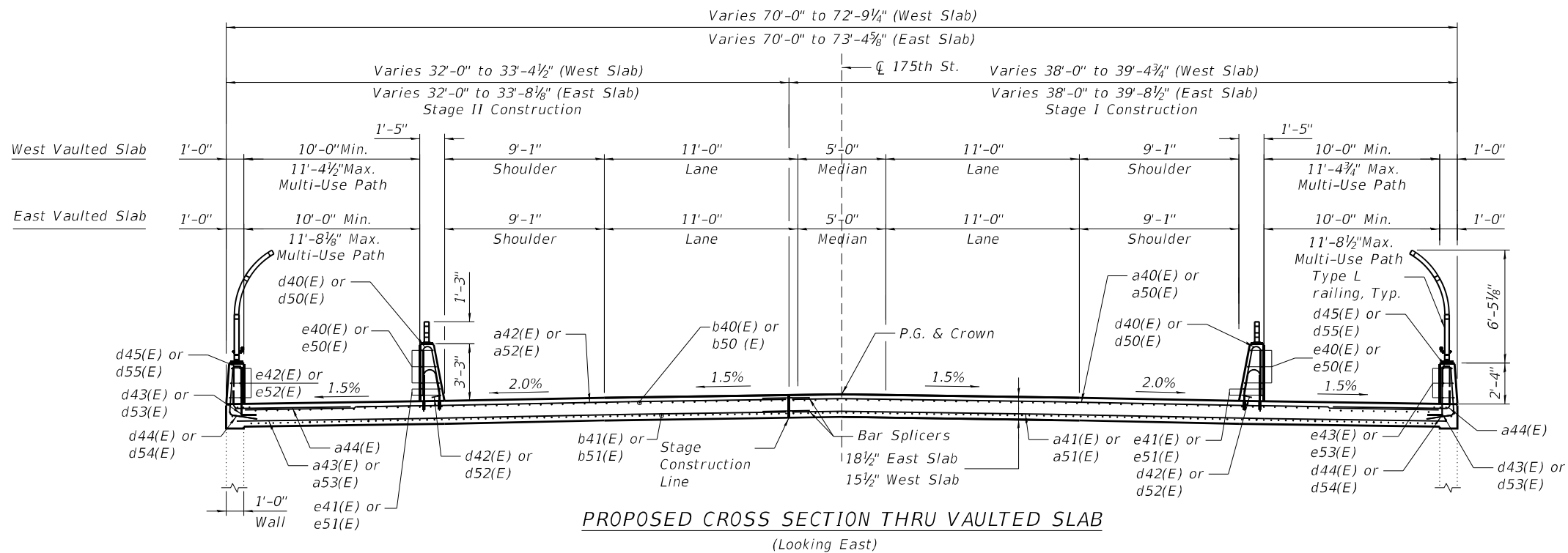
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**VAULTED ABUTMENT APPROACH SPANS
 STRUCTURE NO. 016-1095**

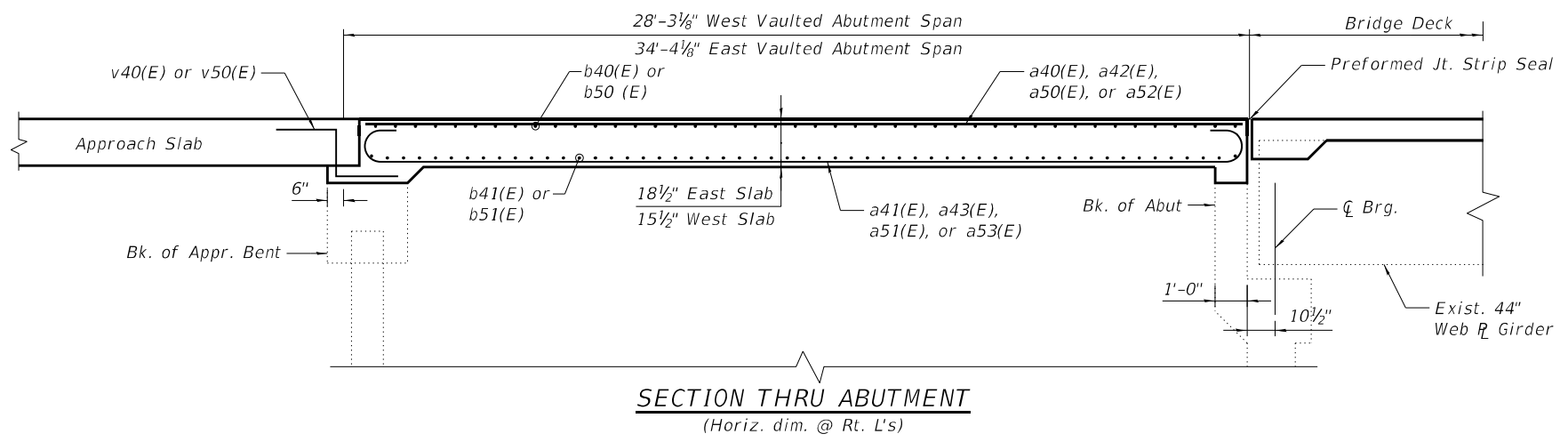
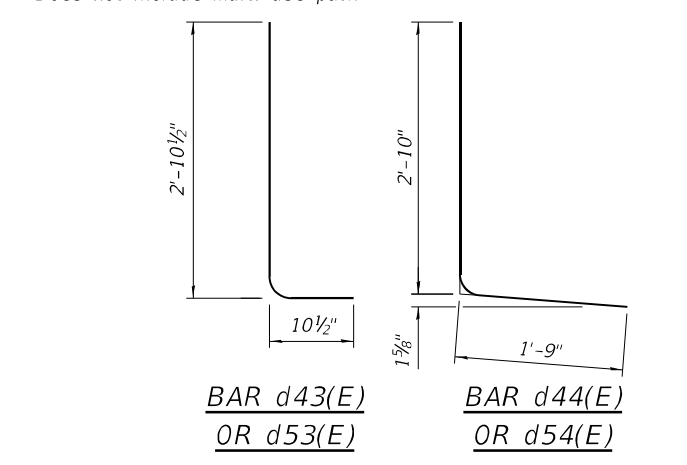
SHEET 14 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	47
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

MODEL: Default
 FILE NAME: p:\w\pww\m\h\h\h\pwwg\eat_Lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO# 1 & TO#5 - 175th Street at 157CADDSheets\102918-sh-BR-Vaults\157ab2.dgn



Notes:
 1. The u40(E) and u50(E) bars shall be placed parallel to \bar{C} Roadway.
 2. Any reinforcement bars to remain that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".
 * Does not include multi-use path



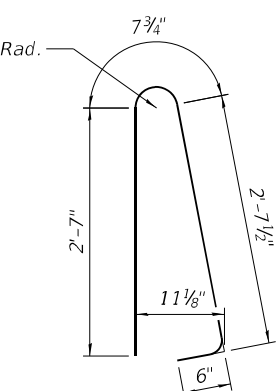
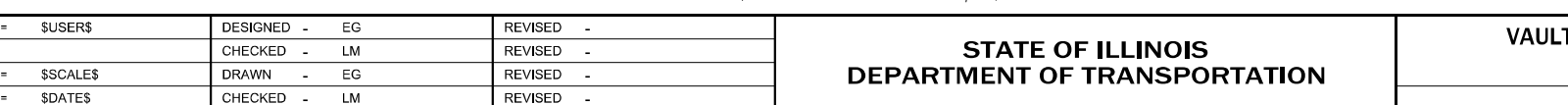
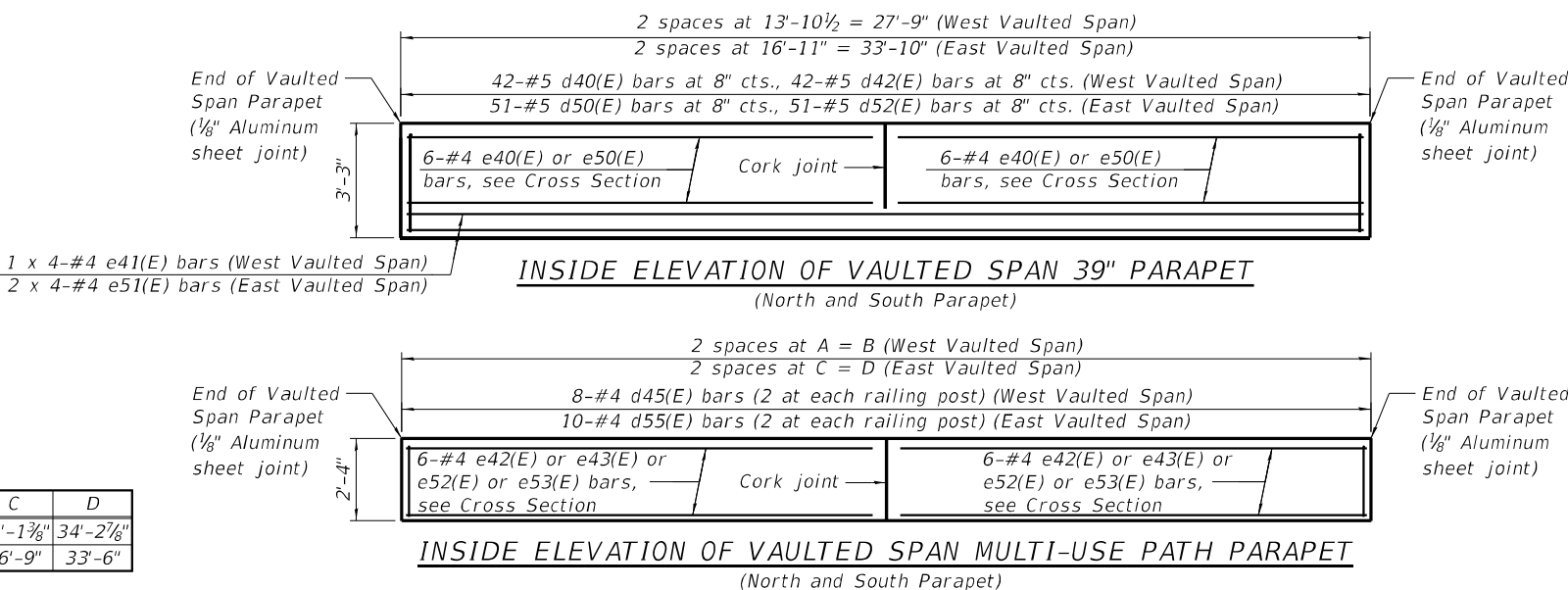
**WEST VAULTED SPAN
 BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a40(E)	90	#5	21'-4"	—
a41(E)	56	#8	24'-0"	—
a42(E)	90	#5	18'-4"	—
a43(E)	56	#8	21'-0"	—
a44(E)	45	#6	8'-4"	L
b40(E)	112	#5	27'-5"	—
b41(E)	168	#9	27'-5"	—
d40(E)	84	#5	6'-5"	∩
d42(E)	84	#5	4'-11"	∩
d43(E)	55	#6	3'-9"	L
d44(E)	55	#4	4'-7"	L
d45(E)	16	#4	2'-0"	∩
e40(E)	24	#4	13'-7"	—
e41(E)	8	#4	27'-6"	—
e42(E)	12	#4	13'-5"	—
e43(E)	12	#4	13'-9"	—
u40(E)	73	#5	9'-8"	—
v40(E)	70	#5	4'-0"	—
Description	Unit	Quantity		
Concrete Superstructure	Cu Yd	106.6		
Bridge Deck Grooving	Sq Yd	140		
Protective Coat	Sq Yd	272		
Reinforcement Bars, Epoxy Coated	Pound	32,420		

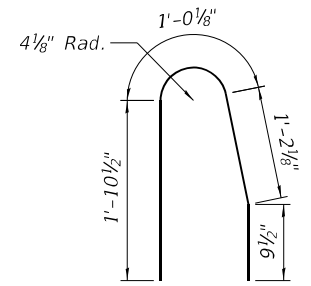
**EAST VAULTED SPAN
 BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a50(E)	110	#5	21'-6"	—
a51(E)	69	#8	24'-2"	—
a52(E)	110	#5	18'-5"	—
a53(E)	69	#8	21'-1"	—
a54(E)	55	#6	8'-4"	L
b50(E)	224	#5	18'-4"	—
b51(E)	168	#9	33'-6"	—
d50(E)	102	#5	6'-5"	∩
d52(E)	102	#5	4'-11"	∩
d53(E)	68	#6	3'-9"	L
d54(E)	68	#4	4'-7"	L
d55(E)	20	#4	2'-0"	∩
e50(E)	24	#4	16'-8"	—
e51(E)	16	#4	18'-0"	—
e52(E)	12	#4	16'-10"	—
e53(E)	12	#4	16'-6"	—
u50(E)	73	#5	9'-8"	—
v50(E)	70	#5	4'-0"	—
Description	Unit	Quantity		
Concrete Superstructure	Cu Yd	152.9		
Bridge Deck Grooving	Sq Yd	170		
Protective Coat	Sq Yd	332		
Reinforcement Bars, Epoxy Coated	Pound	39,910		

Bars indicated thus 1 x 2-#4 etc. indicates 1 line of bars with 2 lengths per line.



BAR d40(E)
OR d50(E)



BAR d42(E)
OR d52(E)

Parapet	A	B	C	D
North	13'-8 7/8"	27'-5 3/4"	17'-1 3/8"	34'-2 7/8"
South	14'-0 1/2"	28'-1 1/8"	16'-9"	33'-6"



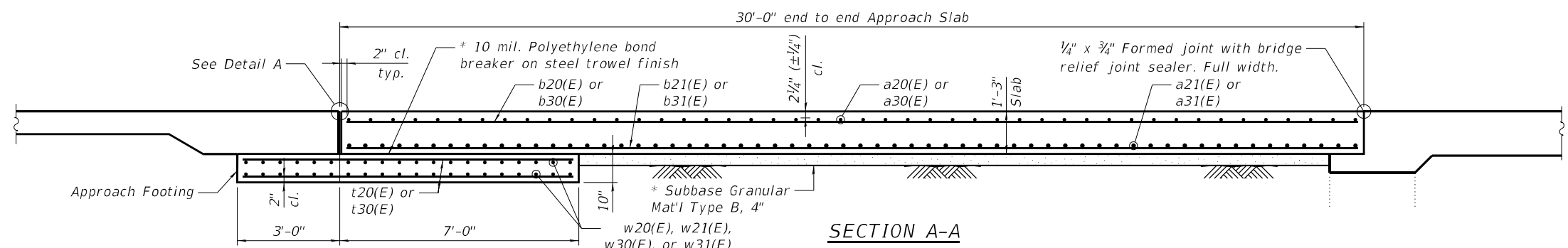
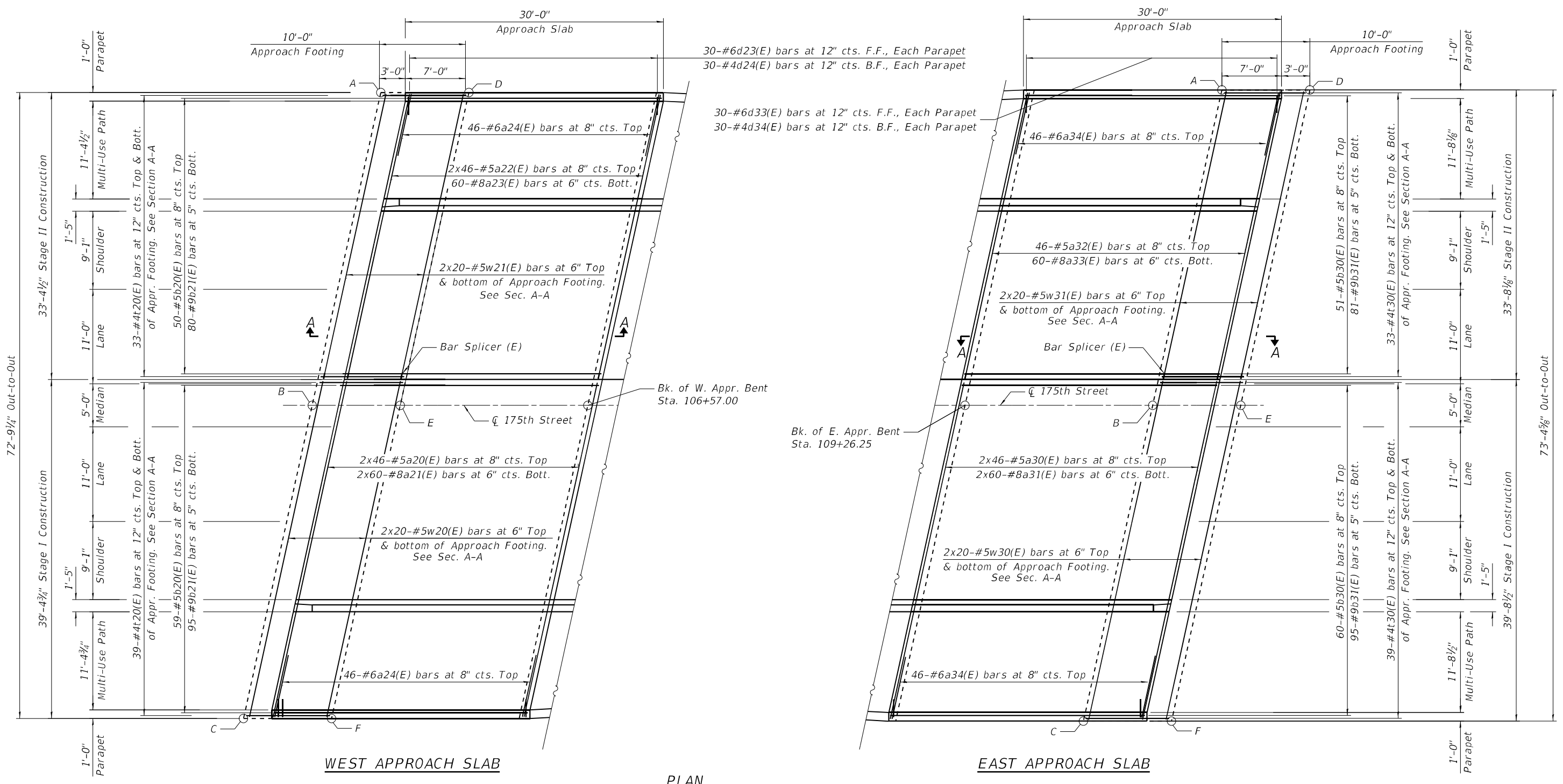
USER NAME =	SUSERS\$	DESIGNED -	EG	REVISED -	
CHECKED -	LM	CHECKED -	LM	REVISED -	
PLOT SCALE =	\$\$CALE\$	DRAWN -	EG	REVISED -	
PLOT DATE =	\$\$DATE\$	CHECKED -	LM	REVISED -	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

VAULTED ABUTMENT APPROACH SPAN DETAILS
 STRUCTURE NO. 016-1095

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	48
CONTRACT NO. 62P18				
ILLINOIS / FED. AID PROJECT				

MODEL: Default
 FILE NAME: p:\w\pww-hntb.org\pwwg\real_Lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO#1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh+BR-Appr\slab1.dgn



TOP AND BOTTOM ELEVATIONS FOR APPROACH FOOTING

Point	West Approach		East Approach	
	Top	Bottom	Top	Bottom
A	729.32	728.49	729.65	728.82
B	729.33	728.50	729.96	729.13
C	728.98	728.15	729.93	729.09
D	729.52	728.69	729.46	728.62
E	729.54	728.71	729.78	728.95
F	729.20	728.37	729.75	728.92



USER NAME =	SUSERS\$	DESIGNED -	EG	REVISED -	
CHECKED -		CHECKED -	LM	REVISED -	
PLOT SCALE =	\$\$SCALE\$	DRAWN -	EG	REVISED -	
PLOT DATE =	\$\$DATE\$	CHECKED -	LM	REVISED -	

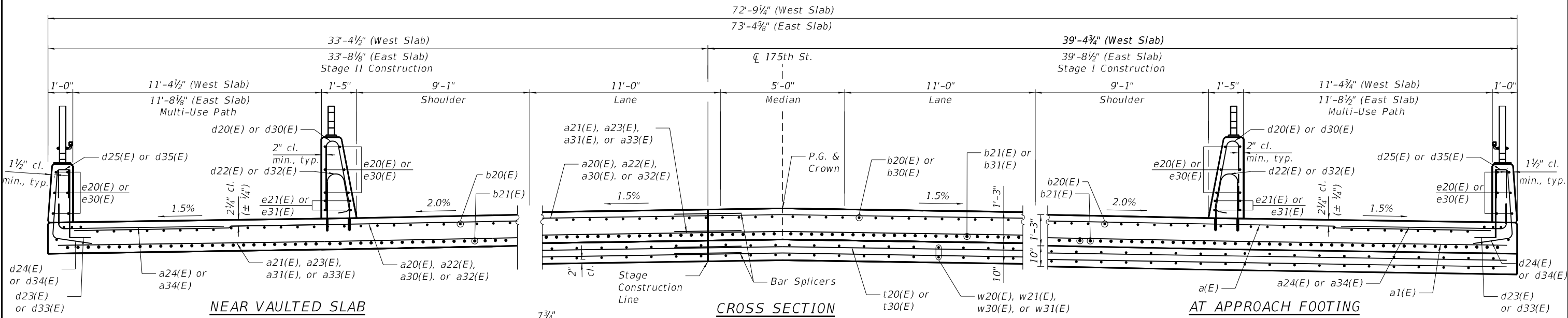
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BRIDGE APPROACH SLAB PLAN
STRUCTURE NO. 016-1095

SHEET 16 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	49
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

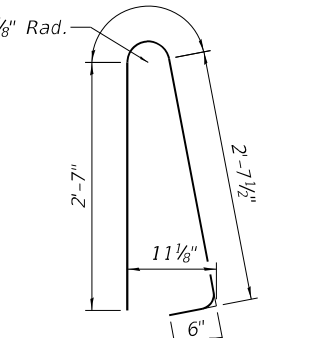
MODEL: Default
 FILE NAME: p:\work\m-h\lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO#1 & TO#5 - 175th Street at 157th\CAD\Drawings\BDR-Appr\slab2.dgn
 2/4/2022 9:17:46 AM



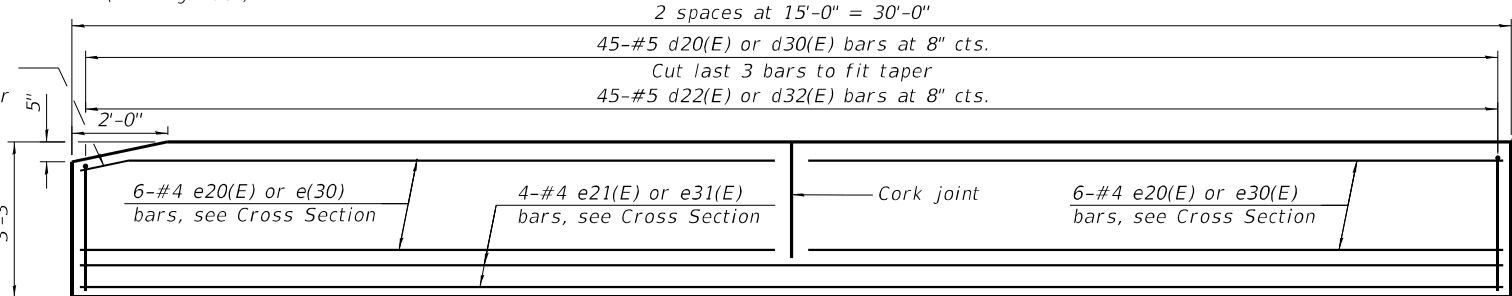
NEAR VAULTED SLAB

CROSS SECTION
(Looking East)

AT APPROACH FOOTING

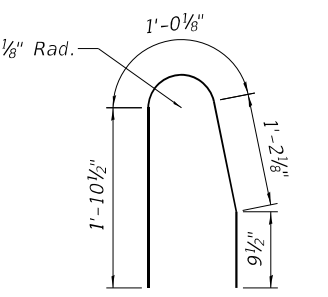


BAR d20(E) OR d30(E)

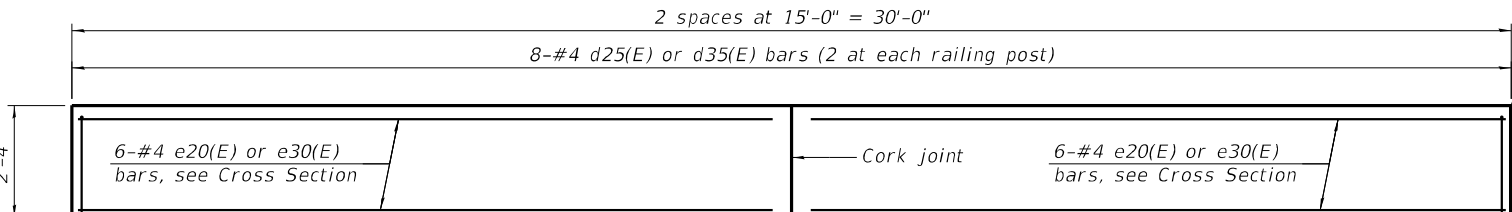


INSIDE ELEVATION OF APPROACH SLAB 39" PARAPET

(North and South Parapet, Opposite at each approach)

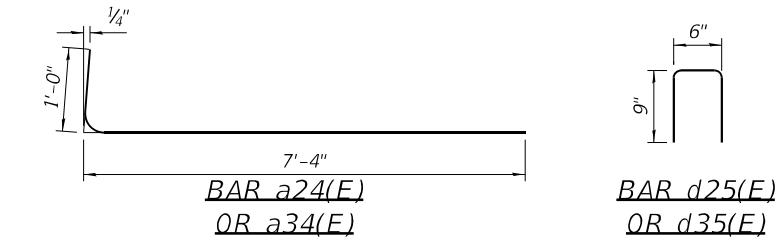
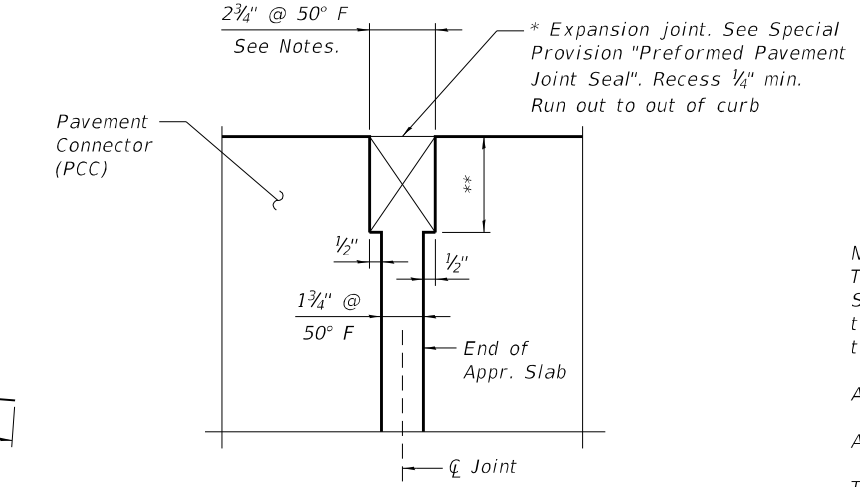


BAR d22(E) OR d32(E)



INSIDE ELEVATION OF APPROACH SLAB MULTI-USE PATH PARAPET

(North and South Parapet, Opposite at each approach)



Notes:
 The joint opening shall be adjusted for temperature per Article 520.04 of the Standard Specifications. However, since this detail is for jointless structures, the length of bridge used to calculate the adjustment shall be equal to half the total bridge length plus the length of the bridge approach slab.
 Approach slab shall be paid for as Concrete Superstructure (Approach Slab).
 Approach footing concrete shall be paid for as Concrete Structures.
 The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.
 Cost of excavation for approach footing included with Concrete Structures.

WEST APPROACH SLAB
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a20(E)	92	#5	21'-2"	—
a21(E)	120	#8	21'-9"	—
a22(E)	92	#5	18'-0"	—
a23(E)	60	#8	33'-0"	—
a24(E)	92	#6	8'-4"	—
b20(E)	109	#5	29'-8"	—
b21(E)	175	#9	29'-8"	—
d20(E)	90	#5	6'-5"	⏏
d22(E)	90	#5	4'-11"	⏏
d23(E)	60	#6	3'-9"	⏏
d24(E)	60	#4	4'-7"	⏏
d25(E)	16	#4	2'-0"	⏏
e20(E)	48	#4	14'-9"	—
e21(E)	8	#4	29'-9"	—
t20(E)	144	#4	9'-8"	—
w20(E)	80	#5	21'-2"	—
w21(E)	80	#5	18'-0"	—
Description	Unit	Quantity		
Concrete Structures	Cu Yd	23.0		
Bridge Deck Grooving	Sq Yd	151		
Protective Coat	Sq Yd	296		
Concrete Superstructure (Approach Slab)	Cu Yd	113.8		
Reinforcement Bars, Epoxy Coated	Pound	44,630		

EAST APPROACH SLAB
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a30(E)	92	#5	21'-3"	—
a31(E)	120	#8	21'-11"	—
a32(E)	92	#5	18'-3"	—
a33(E)	60	#8	33'-4"	—
a34(E)	92	#6	8'-4"	—
b30(E)	111	#5	29'-8"	—
b31(E)	176	#9	29'-8"	—
d30(E)	90	#5	6'-5"	⏏
d32(E)	90	#5	4'-11"	⏏
d33(E)	60	#6	3'-9"	⏏
d34(E)	60	#4	4'-7"	⏏
d35(E)	16	#4	2'-0"	⏏
e30(E)	48	#4	14'-9"	—
e31(E)	8	#4	29'-9"	—
t30(E)	144	#4	9'-8"	—
w30(E)	80	#5	21'-3"	—
w31(E)	80	#5	18'-3"	—
Description	Unit	Quantity		
Concrete Structures	Cu Yd	23.2		
Bridge Deck Grooving	Sq Yd	151		
Protective Coat	Cu Yd	298		
Concrete Superstructure (Approach Slab)	Cu Yd	114.7		
Reinforcement Bars, Epoxy Coated	Pound	44,960		

Bars indicated thus 1 x 2-#4 etc. indicates 1 line of bars with 2 lengths per line.

* Cost included with Concrete Superstructure (Approach Slab)
 ** Per manufacturer recommendations



USER NAME =	SUSERS\$	DESIGNED -	EG	REVISED -	
CHECKED -	LM	REVISIONS -			
PLOT SCALE =	SCALE\$	DRAWN -	EG	REVISED -	
PLOT DATE =	DATES	CHECKED -	LM	REVISED -	

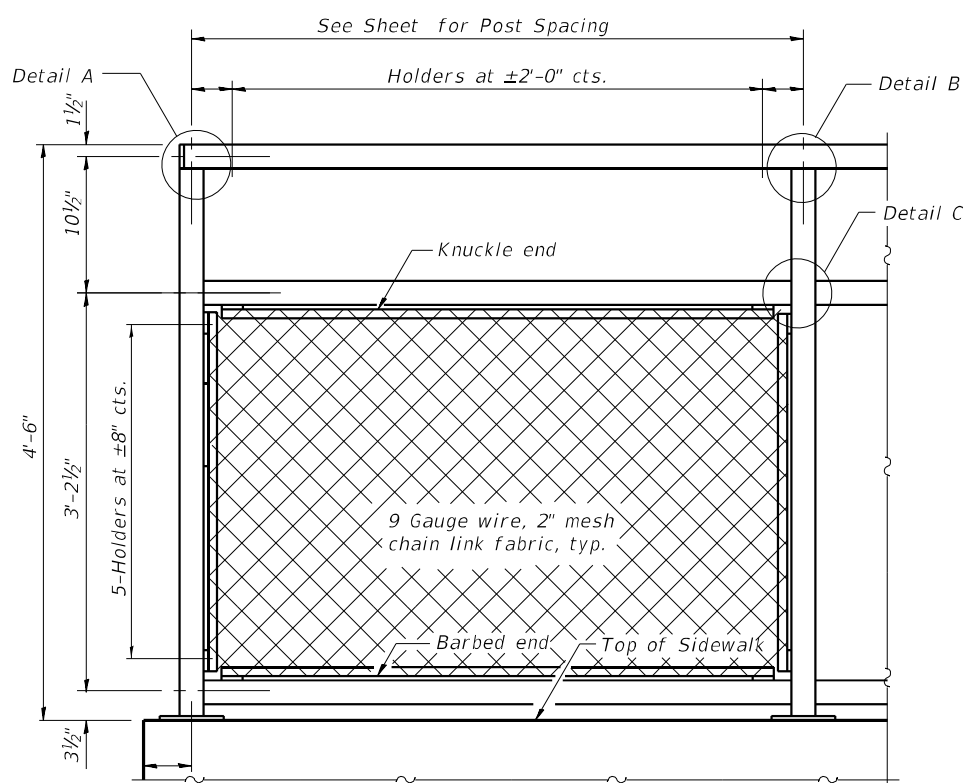
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BRIDGE APPROACH SLAB DETAILS
STRUCTURE NO. 016-1095

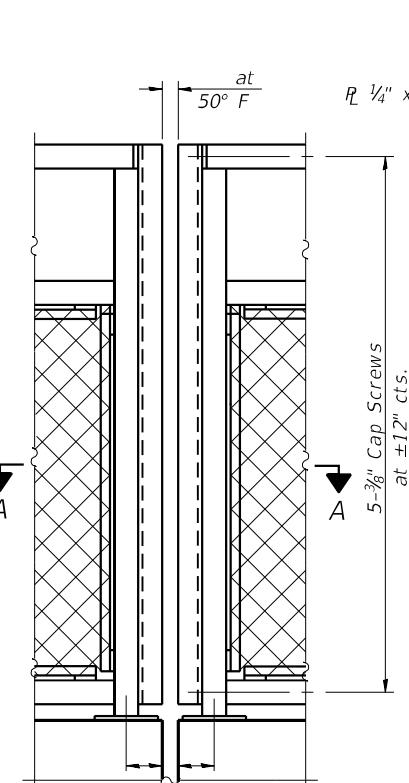
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	50
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

SHEET 17 OF 29 SHEETS

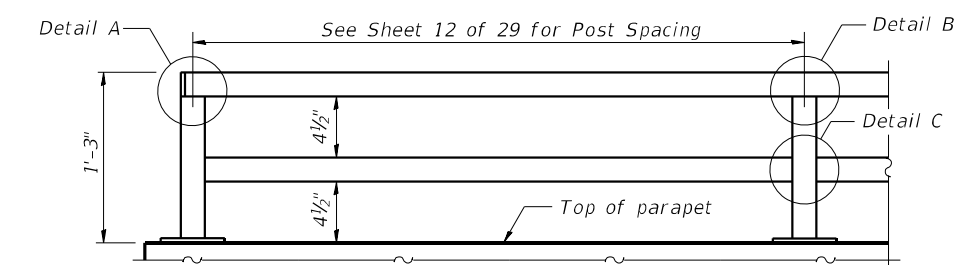
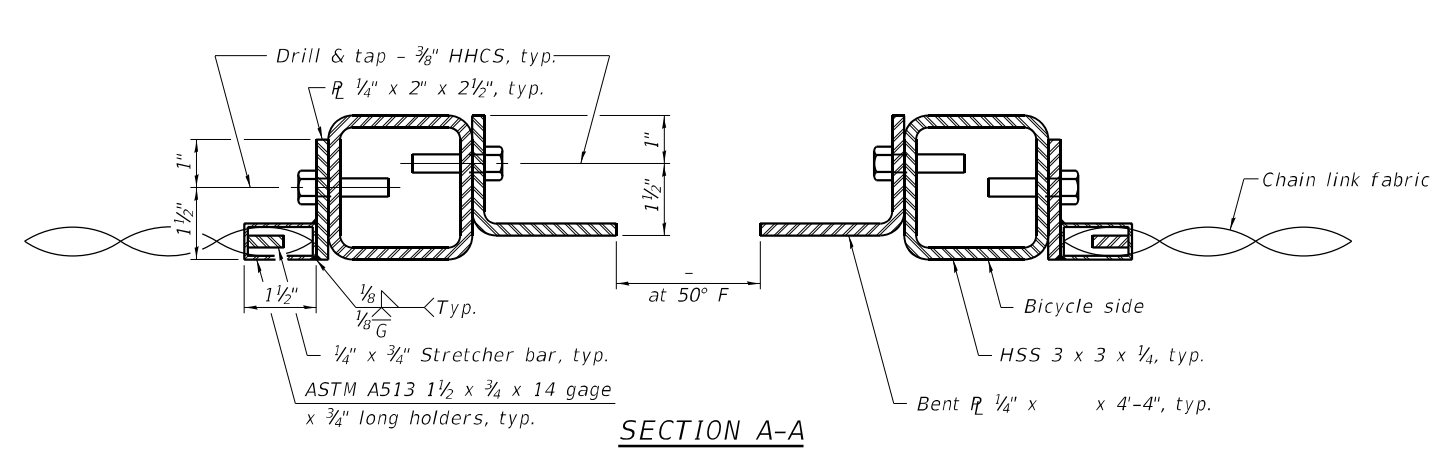
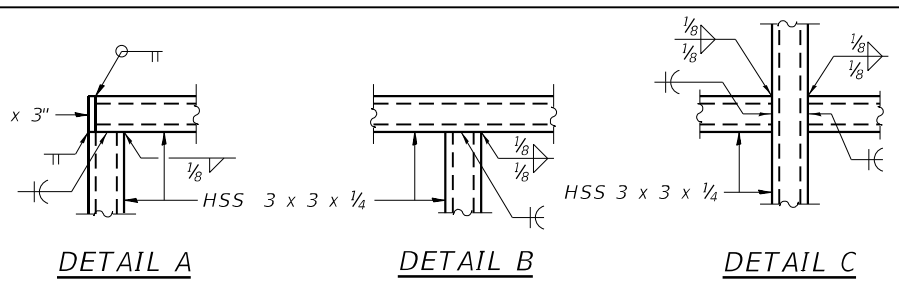
MODEL: Default
 FILE NAME: p:\w\p\w-hntb\org\p\w\g\eat_Lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task_Order\TO#1 & TO#5 - 175th Street at 157CAD\Drawings\102918-sh-BR-Railing\Details1.dgn
 2/4/2022 9:17:53 AM



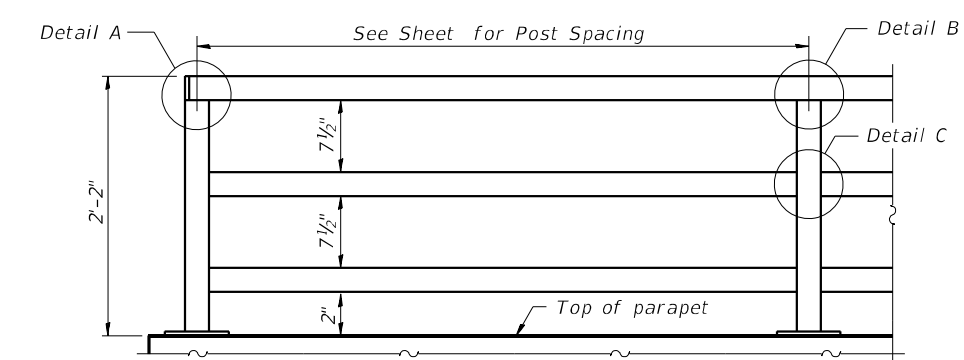
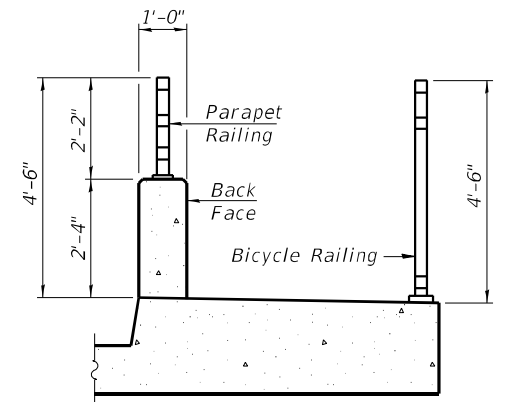
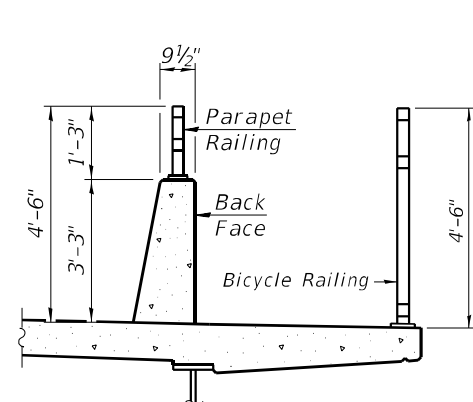
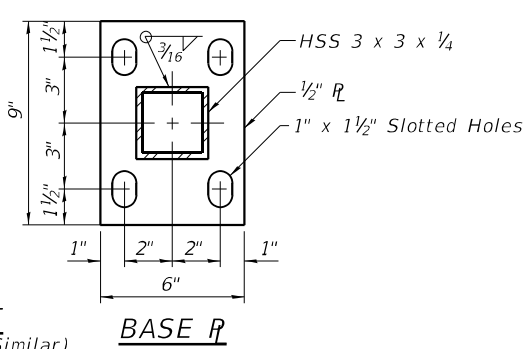
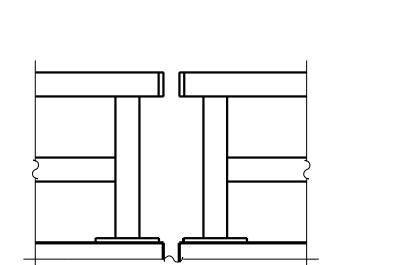
BICYCLE RAILING



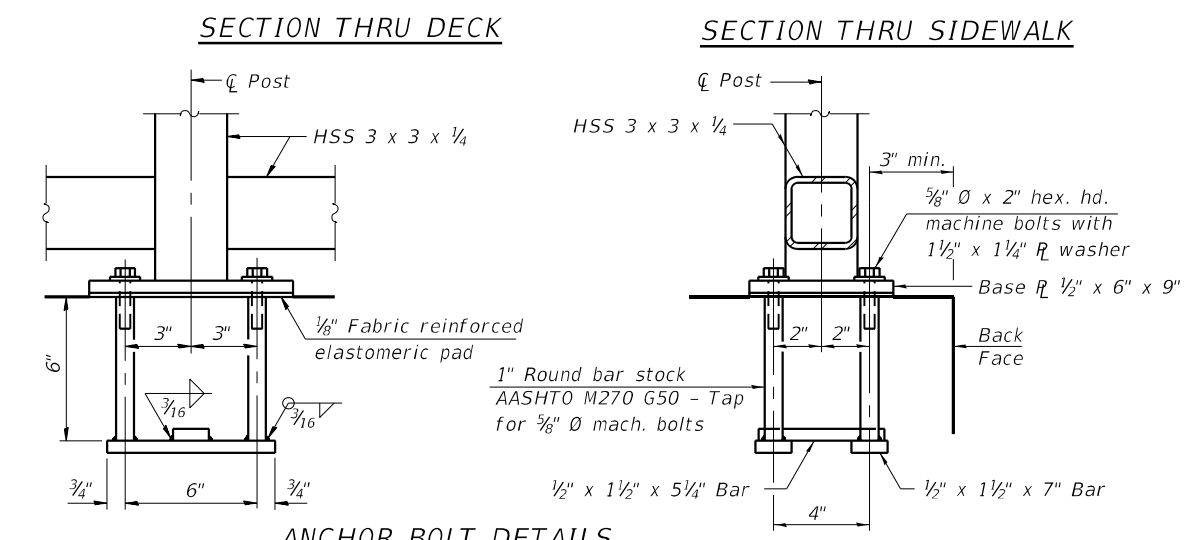
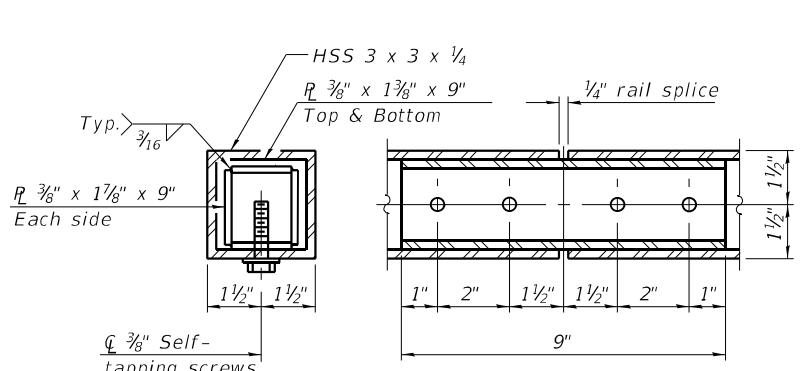
BICYCLE RAILING



PARAPET RAILING
ELEVATION
 (Inside Face of Two Element Rail)



PARAPET RAILING
ELEVATION
 (Inside Face of Three Element Rail)



Notes:
 All structural steel tubing, post and railing, for parapet railing shall be CVN tested according to 1006.34(b) of the Standard Specifications. CVN testing may be omitted for the Bicycle Railing. All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.

In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting 5/8" Ø anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.

BILL OF MATERIAL

Item	Unit	Quantity
Parapet Railing	Foot	646

R-29 1-14-2019 (10'-0" Maximum Post Spacing)

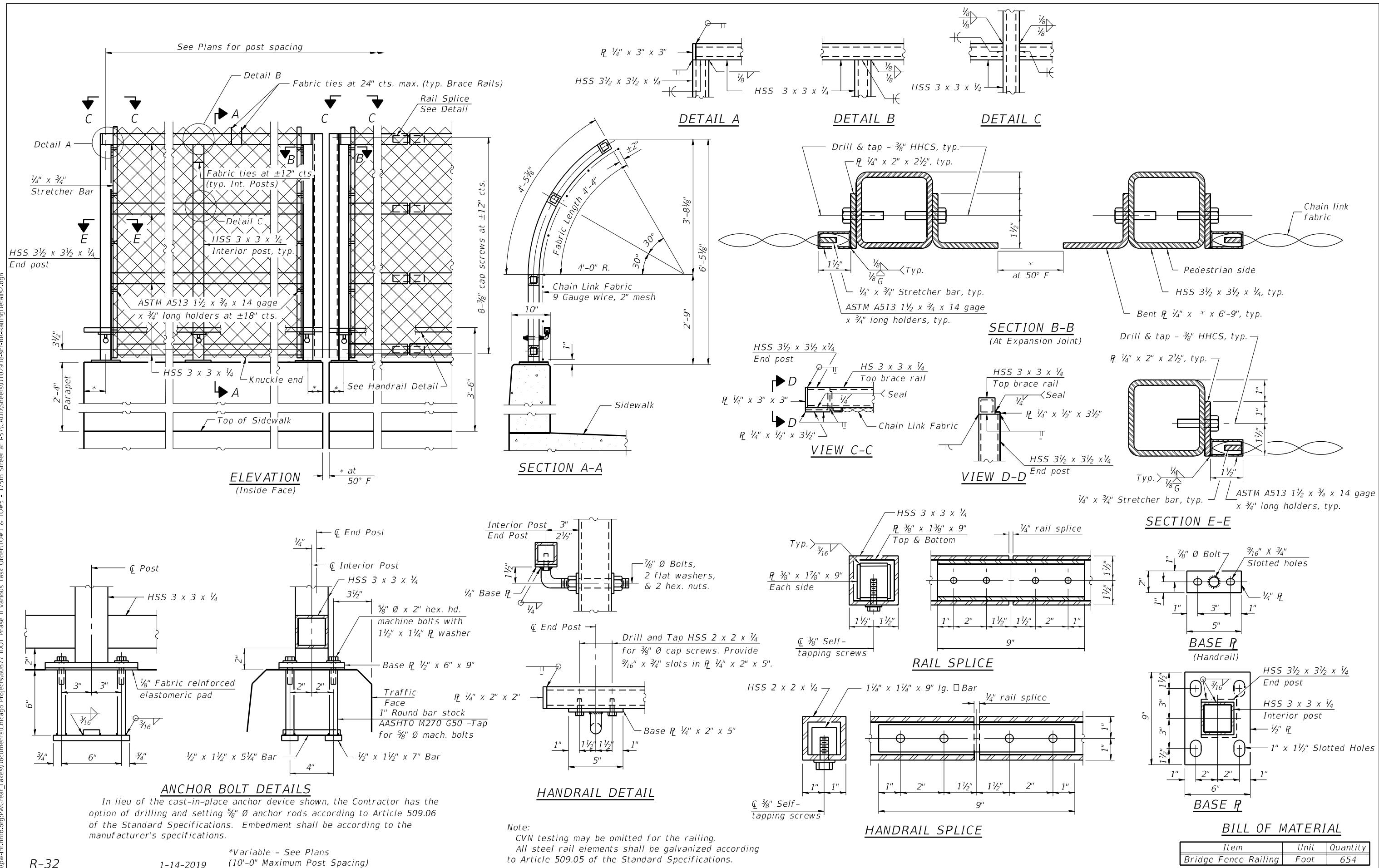
	USER NAME = \$USERS\$	DESIGNED - EG	REVISED -
	PLOT SCALE = \$SCALES\$	CHECKED - LM	REVISED -
	PLOT DATE = \$DATES\$	DRAWN - EG	REVISED -
		CHECKED - LM	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BICYCLE RAILING
STRUCTURE NO. 016-1095
 SHEET 18 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	51
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

MODEL: Default
FILE NAME: p:\w\m\h\hb\org\p\wg\eat_lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task_Order\TO#1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh-BR-Railing\Details2.dgn
2/4/2022 9:17:59 AM



R-32 1-14-2019 *Variable - See Plans (10'-0" Maximum Post Spacing)



USER NAME =	DESIGNED -	REVISIONS
SUSERS\$	EG	-
CHECKED -	LM	REVISIONS -
PLOT SCALE =	DRAWN -	REVISIONS -
SSCALE\$	EG	REVISIONS -
PLOT DATE =	CHECKED -	REVISIONS -
SDATES	LM	REVISIONS -

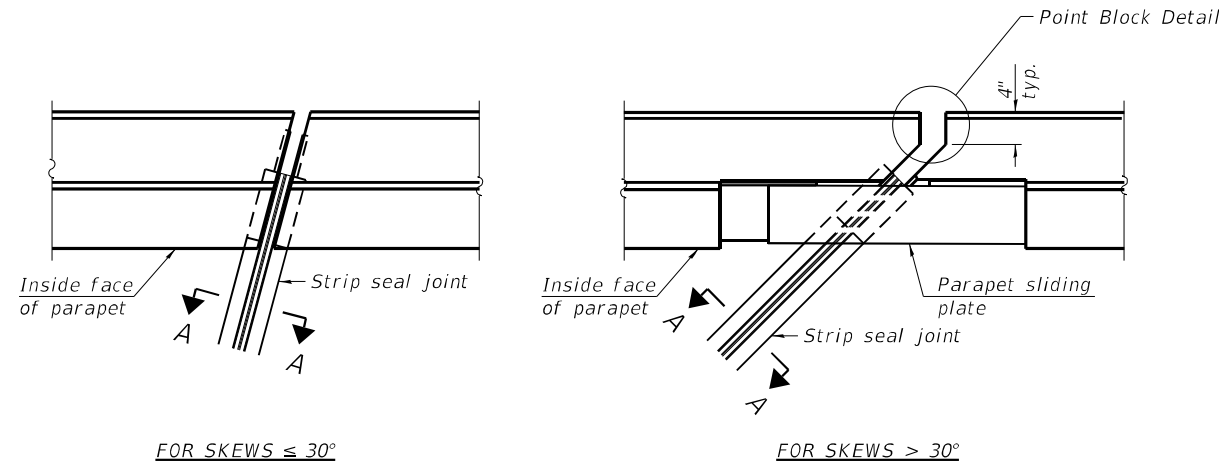
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BRIDGE FENCE RAILING, PARAPET MOUNTED
STRUCTURE NO. 016-1095

SHEET 19 OF 29 SHEETS

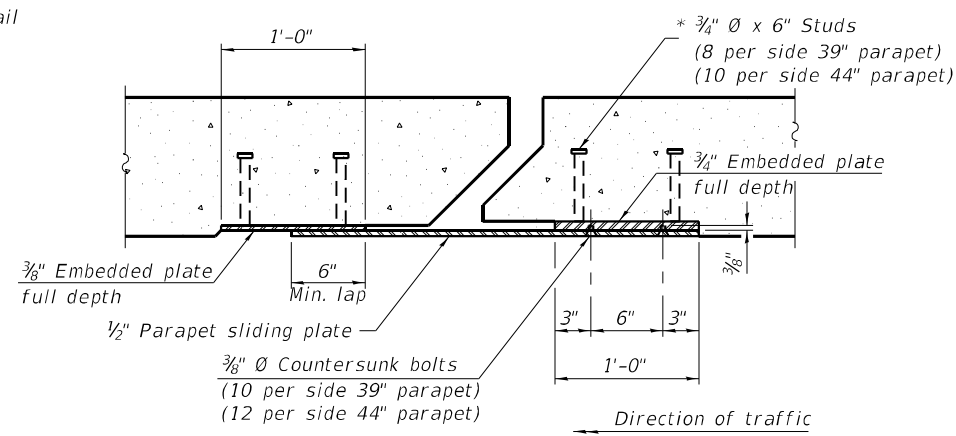
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	52
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

MODEL: Default
 FILE NAME: p:\w\pww-hntb.org\pwwg\eat_Lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO#1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh-BR-PreformedSeals.dgn
 2/4/2022 9:18:03 AM



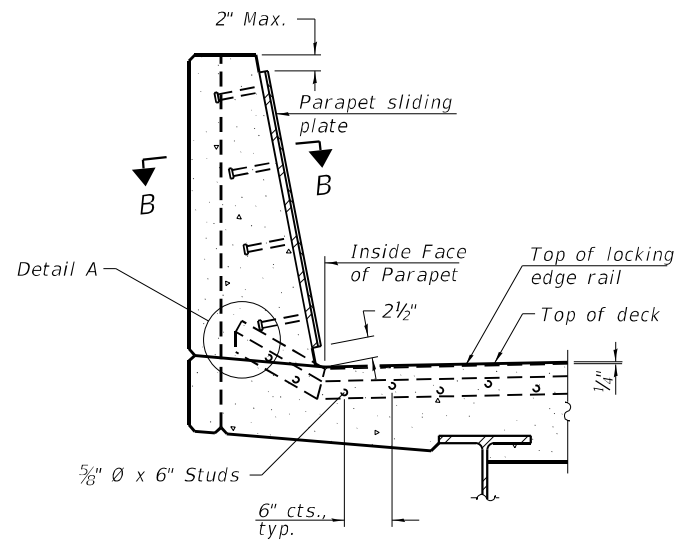
FOR SKEWS $\leq 30^\circ$

PLAN AT PARAPET



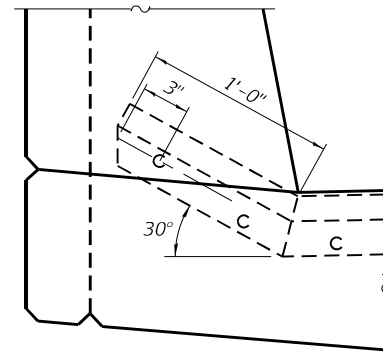
SECTION B-B

Notes:
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
 The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.
 The manufacturer's recommended installation methods shall be followed.
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
 The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.
 The top surface of sidewalk sliding plates shall have a raised pattern according to ASTM A786.
 Cost of parapet sliding plates, sidewalk sliding plates, embedded plates, anchorage studs, and expansion anchors included with Preformed Joint Strip Seal.
 39" constant slope barrier shown, 44" constant slope barrier similar as noted.
 The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

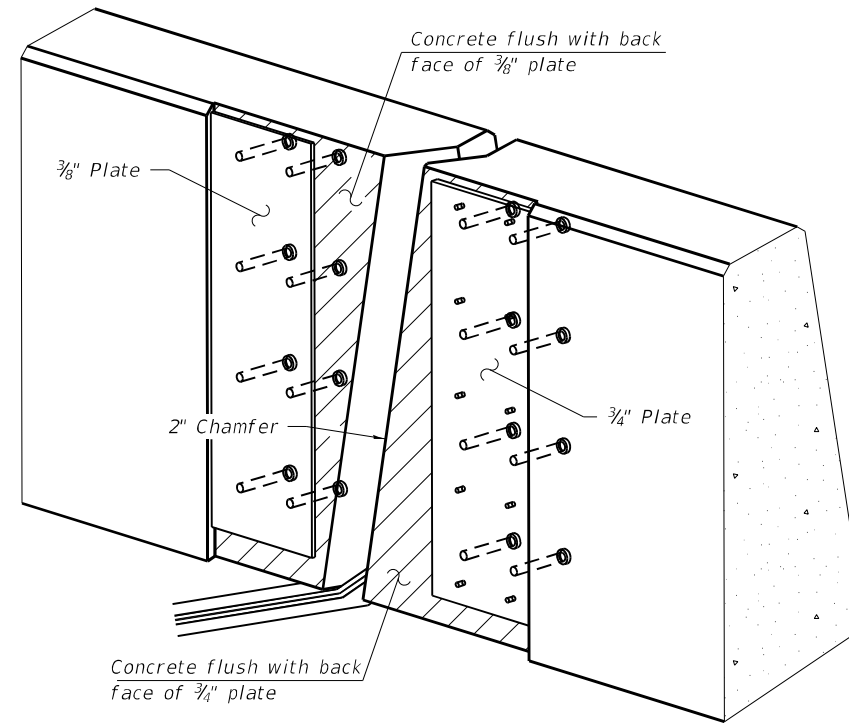


SECTION AT PARAPET

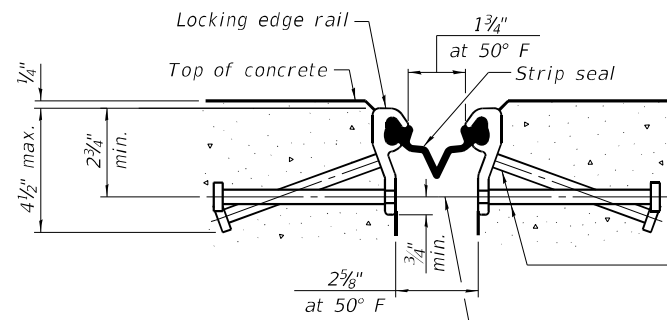
(Skews > 30° shown. Skews $\leq 30^\circ$ similar except as shown in plan view.)



DETAIL A



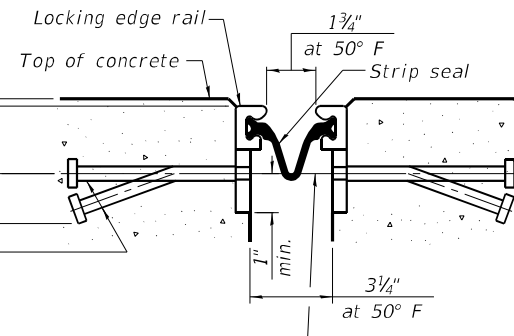
TRIMETRIC VIEW
(Showing embedded plates only)



SHOWING ROLLED RAIL JOINT

* 5/8" ϕ x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

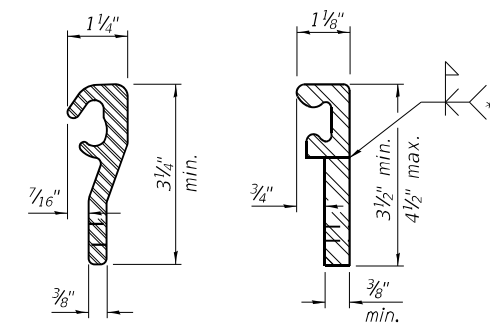
3/8" ϕ threaded rods in 1/16" ϕ holes at ± 4 -0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.



SHOWING WELDED RAIL JOINT

SECTION A-A

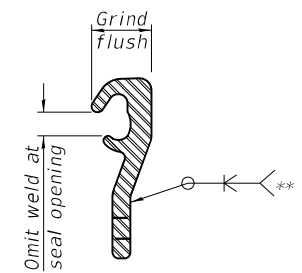
* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



LOCKING EDGE RAILS
ROLLED (EXTRUDED) RAIL WELDED RAIL

LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	144

EJ-SS-S

1-1-2020

(Sheet 1 of 3)



USER NAME =	SUSERS\$	DESIGNED -	EG	REVISED -	
CHECKED -	LM	REVISED -			
PLOT SCALE =	\$\$CALE\$	DRAWN -	EG	REVISED -	
PLOT DATE =	\$\$DATE\$	CHECKED -	LM	REVISED -	

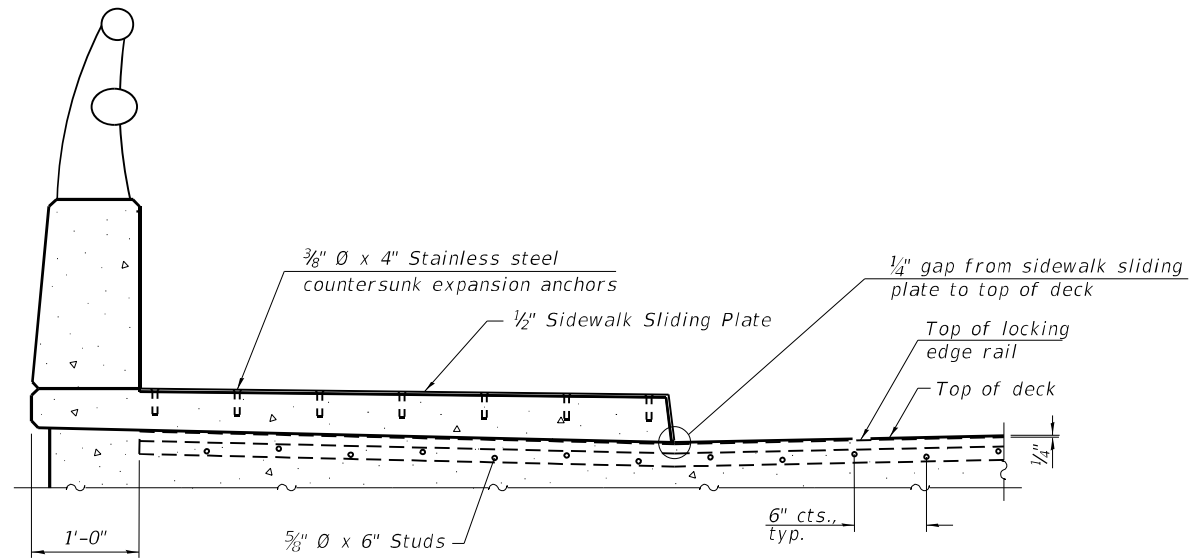
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL - SIDEWALK 1
STRUCTURE NO. 016-1095

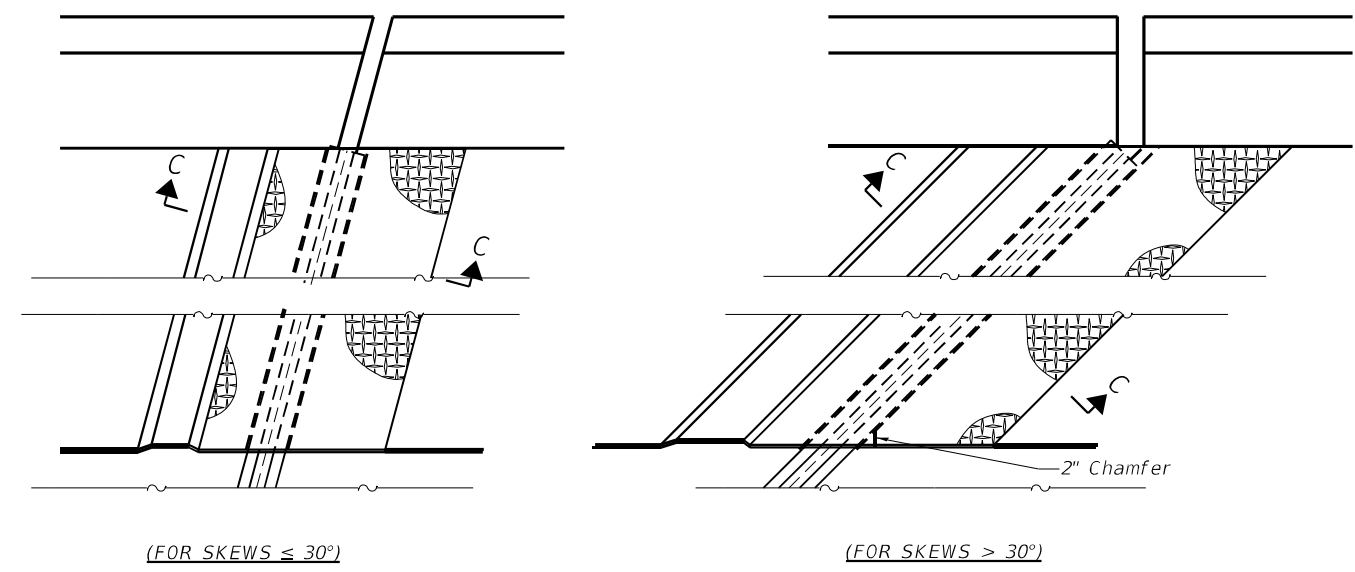
SHEET 20 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	53
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

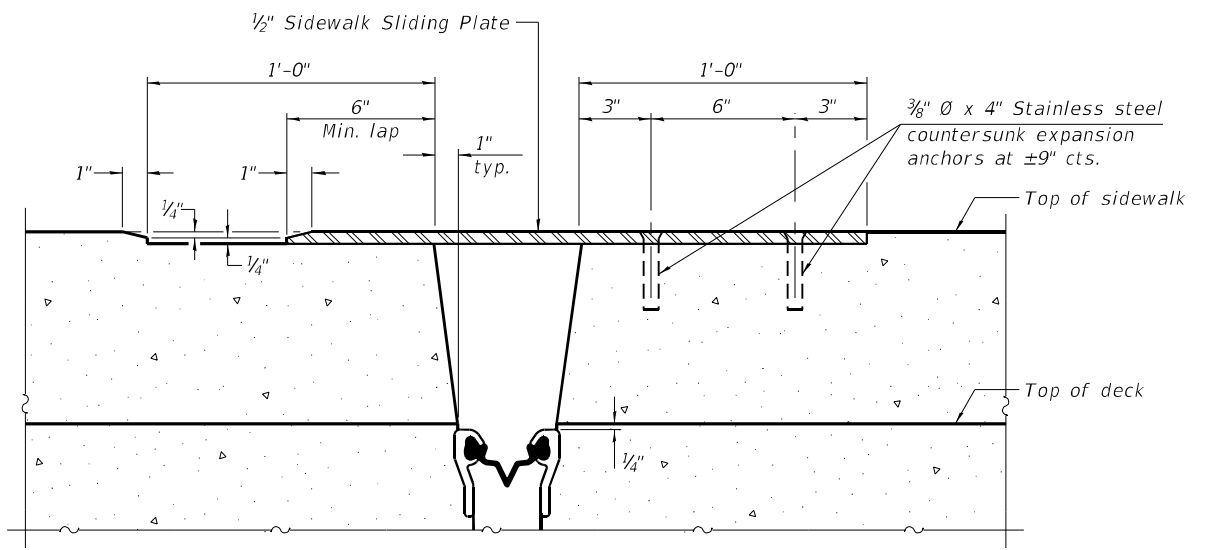
MODEL: Default
 FILE NAME: p:\w\pww-hntb.org\pww\g\at_Lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO# 1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh+BR-PreformedSeals2.dgn



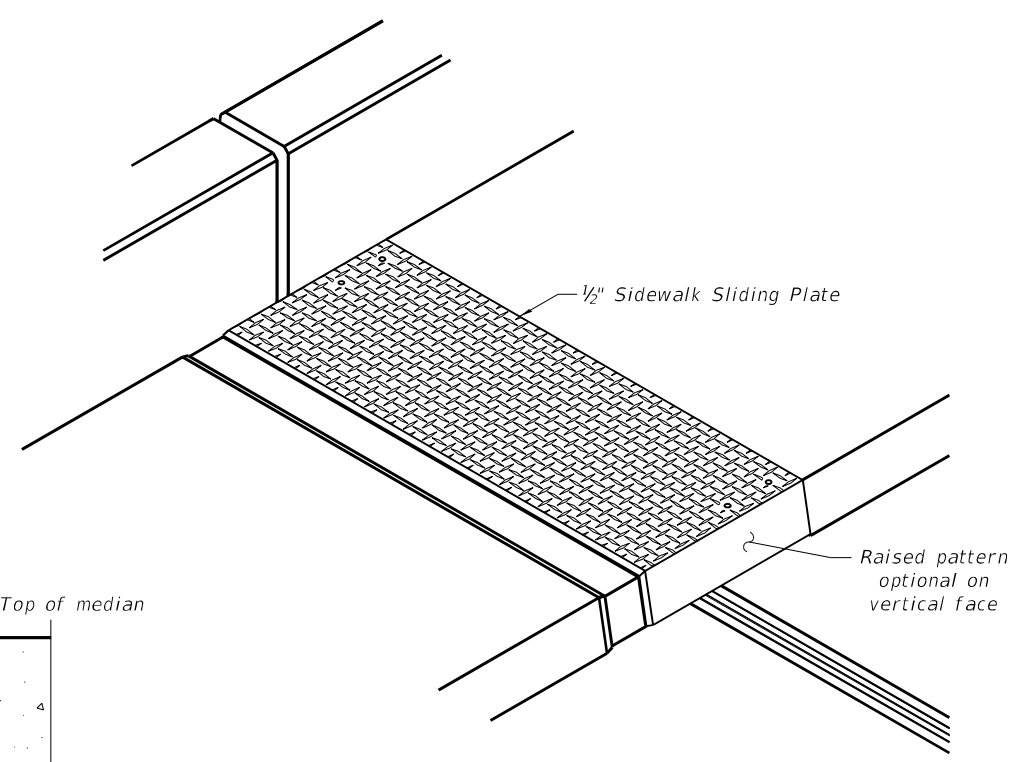
SECTION AT RAISED SIDEWALK



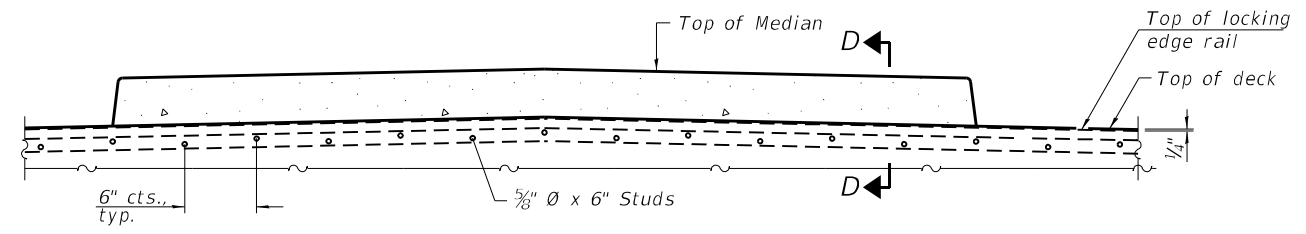
PLAN AT RAISED SIDEWALK



SECTION C-C

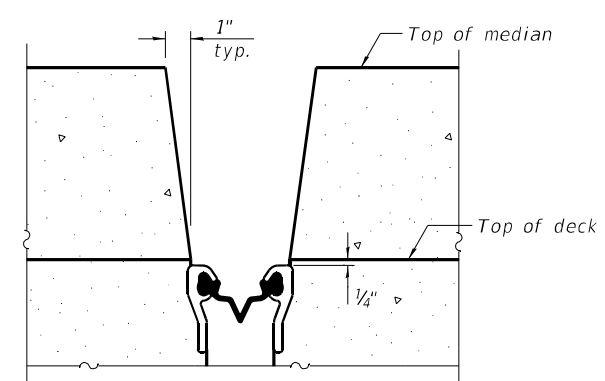


TRIMETRIC VIEW



SECTION AT MEDIAN

For skews > 30°, chamfer acute corners 2" similar to sidewalk.



**SECTION D-D
 (at Rt. L's)**

(Sheet 2 of 3)

EJ-SS-S

1-1-2020



USER NAME =	SUSERS\$	DESIGNED -	EG	REVISED -	
		CHECKED -	LM	REVISED -	
PLOT SCALE =	\$\$SCALE\$	DRAWN -	EG	REVISED -	
PLOT DATE =	\$\$DATE\$	CHECKED -	LM	REVISED -	

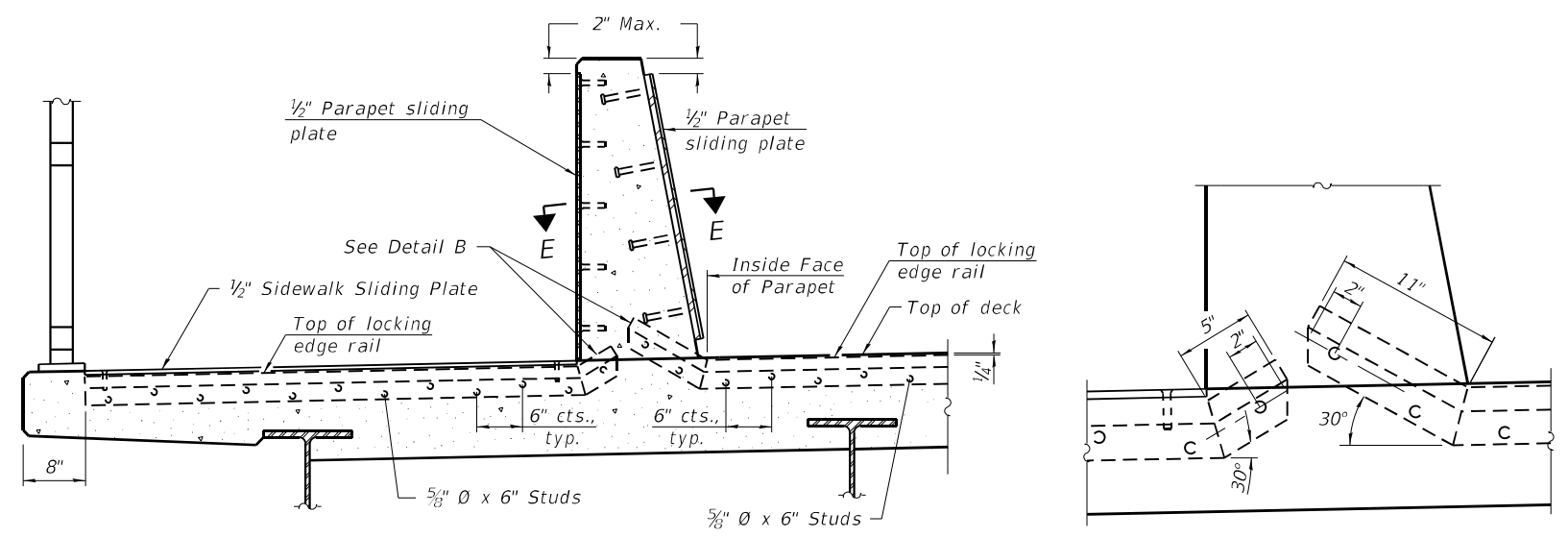
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PREFORMED JOINT STRIP SEAL - SIDEWALK 2
 STRUCTURE NO. 016-1095**

SHEET 21 OF 29 SHEETS

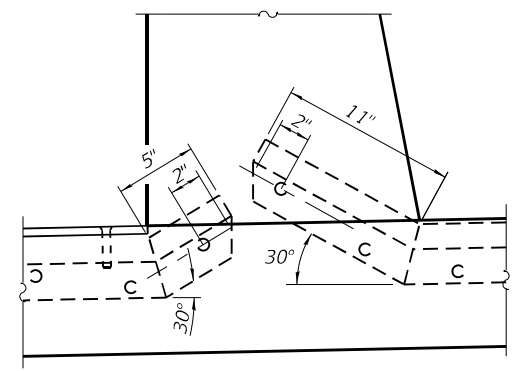
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	54
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

MODEL: Default
 FILE NAME: p:\w\pww-hntb.org\pww\g\at_Lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO# 1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh-BR-PreformedSeals.dgn
 2/4/2022 9:18:10 AM

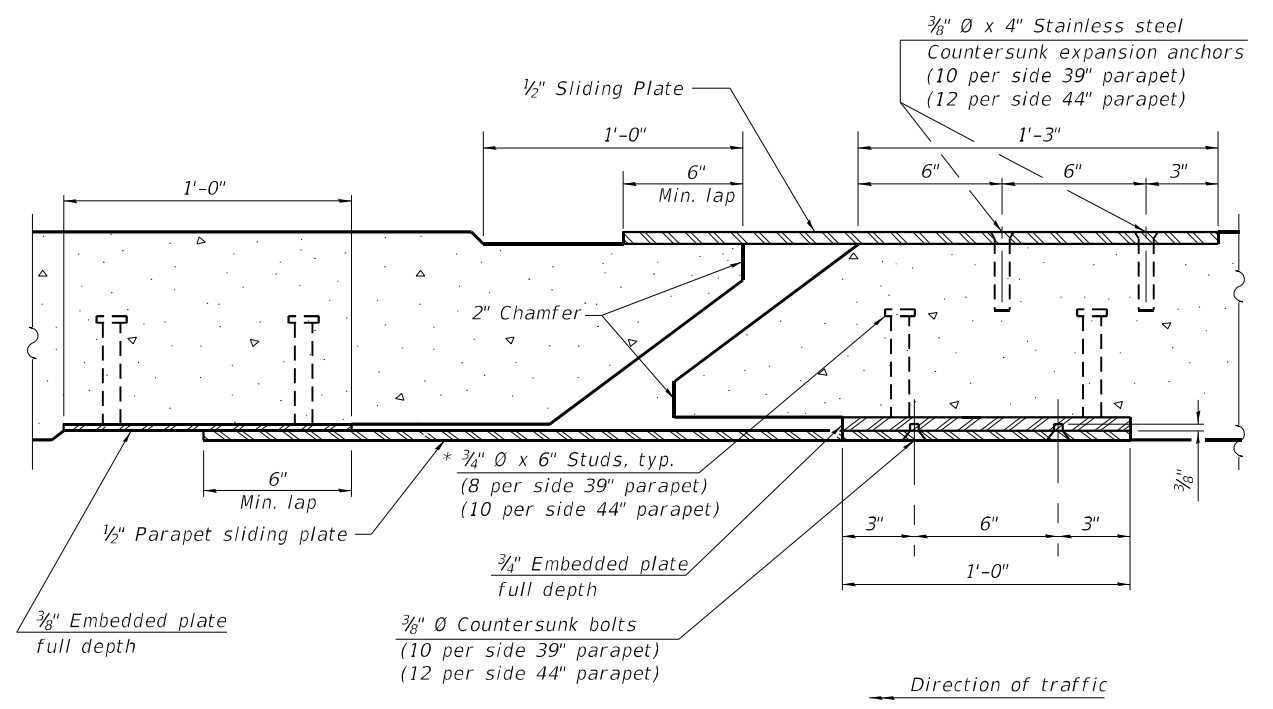


SECTION AT DECK LEVEL SIDEWALK

(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)

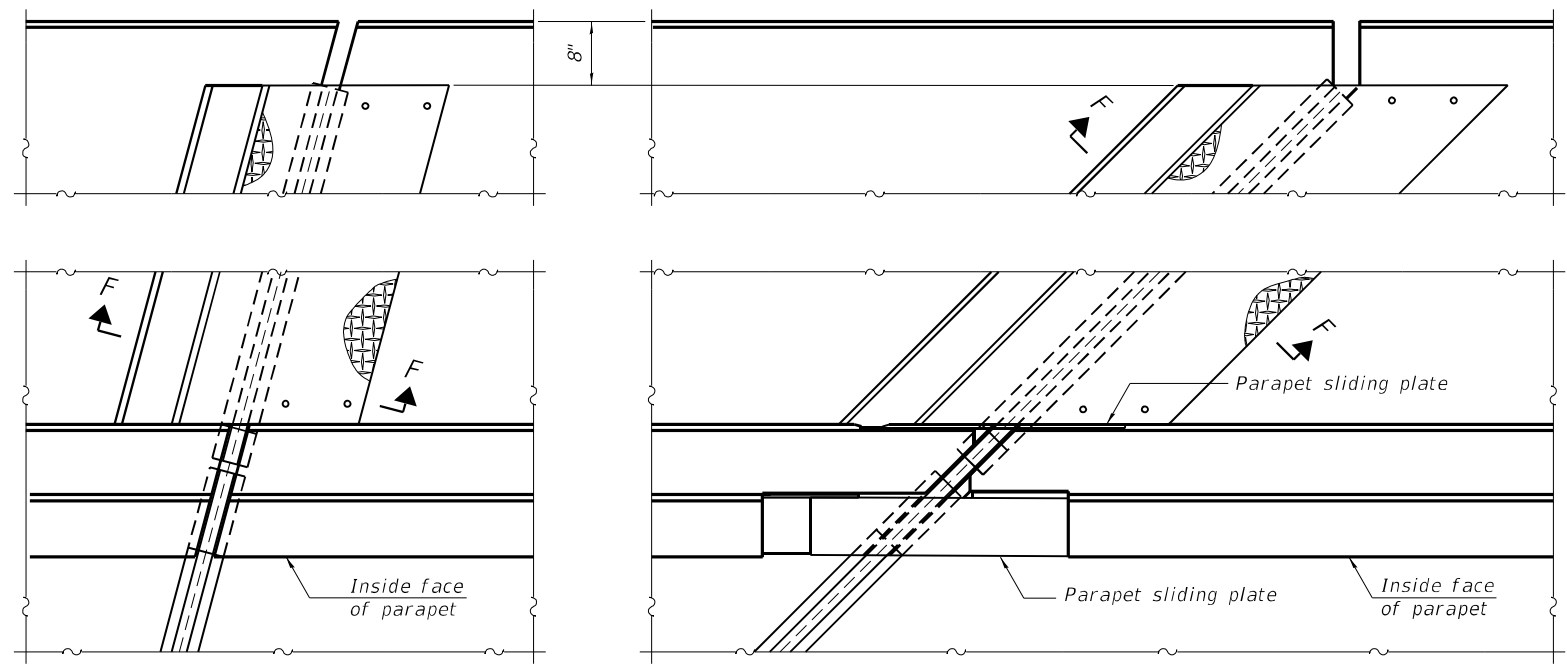


DETAIL B



SECTION E-E

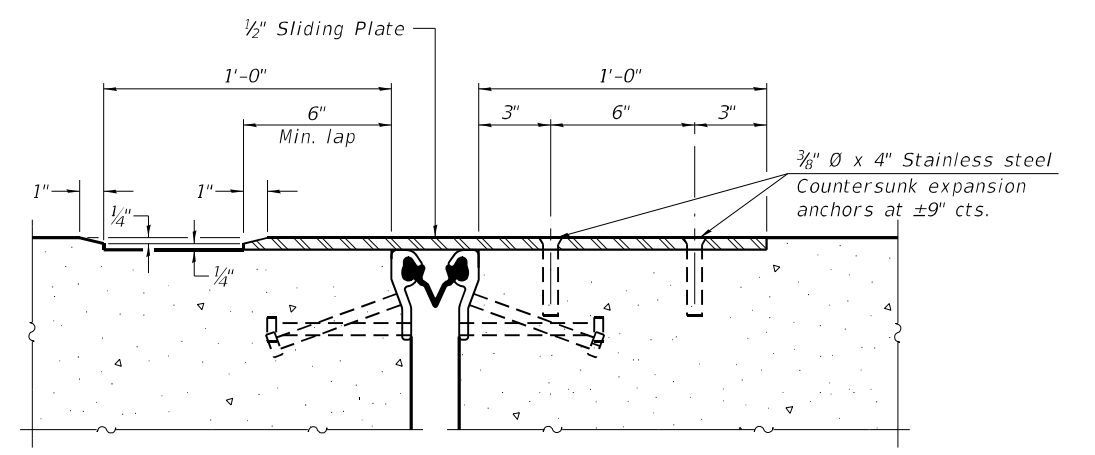
Direction of traffic



(FOR SKEWS ≤ 30°)

(FOR SKEWS > 30°)

PLAN AT DECK LEVEL SIDEWALK



SECTION F-F

EJ-SS-S

1-1-2020

(Sheet 3 of 3)



USER NAME =	\$USERS\$	DESIGNED -	EG	REVISED -	
CHECKED -	LM	REVISIONS -			
PLOT SCALE =	\$\$SCALE\$	DRAWN -	EG	REVISED -	
PLOT DATE =	\$\$DATE\$	CHECKED -	LM	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

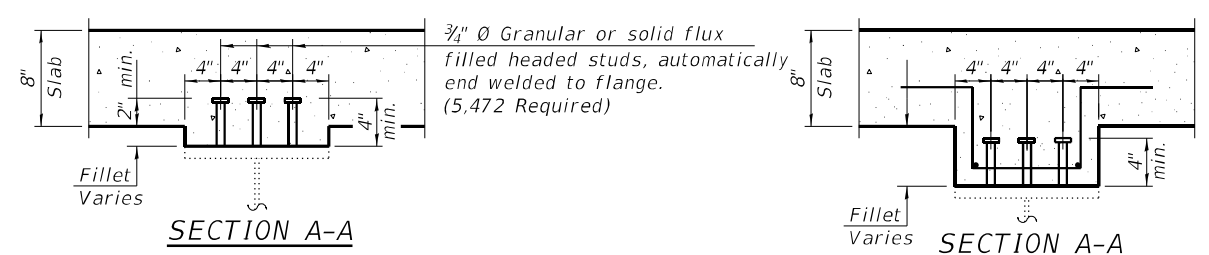
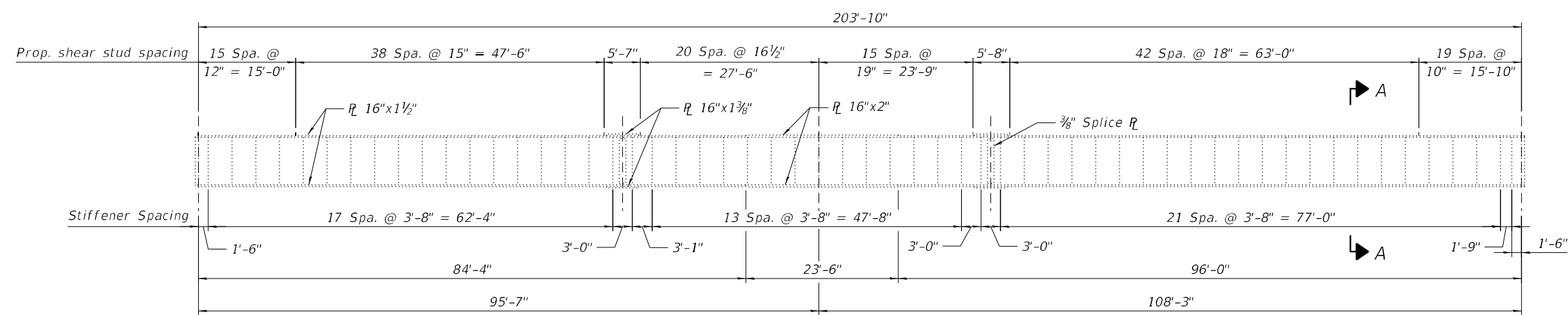
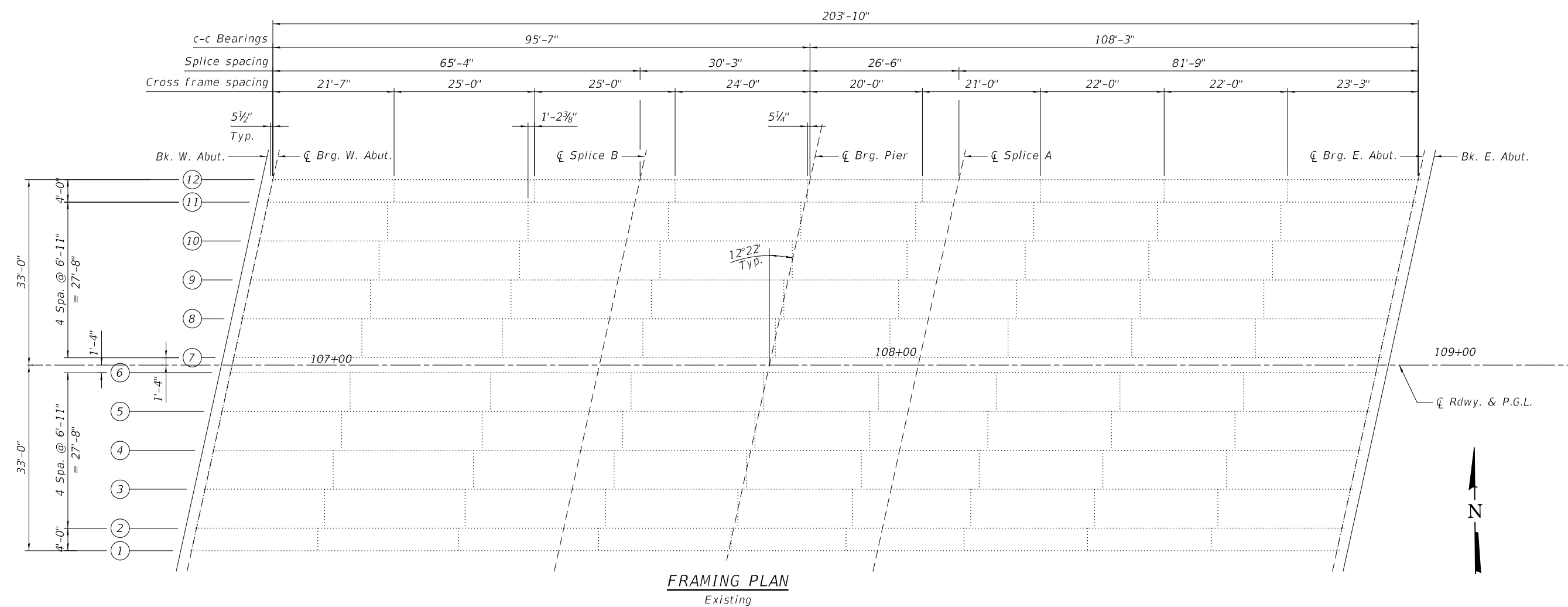
**PREFORMED JOINT STRIP SEAL - SIDEWALK 3
STRUCTURE NO. 016-1095**

SHEET 22 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	55
CONTRACT NO. 62P10				

ILLINOIS FED. AID PROJECT

MODEL: Default
 FILE NAME: p:\w\pww\hntb.org\pww\g\at\Chicago Projects\80677 IDOT Phase II Various Task Order\TO#1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh-BR-FramingPlan.dgn



GIRDER ELEVATION
Existing



USER NAME =	\$USERS\$	DESIGNED -	EG	REVISED -	
		CHECKED -	LM	REVISED -	
PLOT SCALE =	\$SCALE\$	DRAWN -	EG	REVISED -	
PLOT DATE =	\$DATES	CHECKED -	LM	REVISED -	

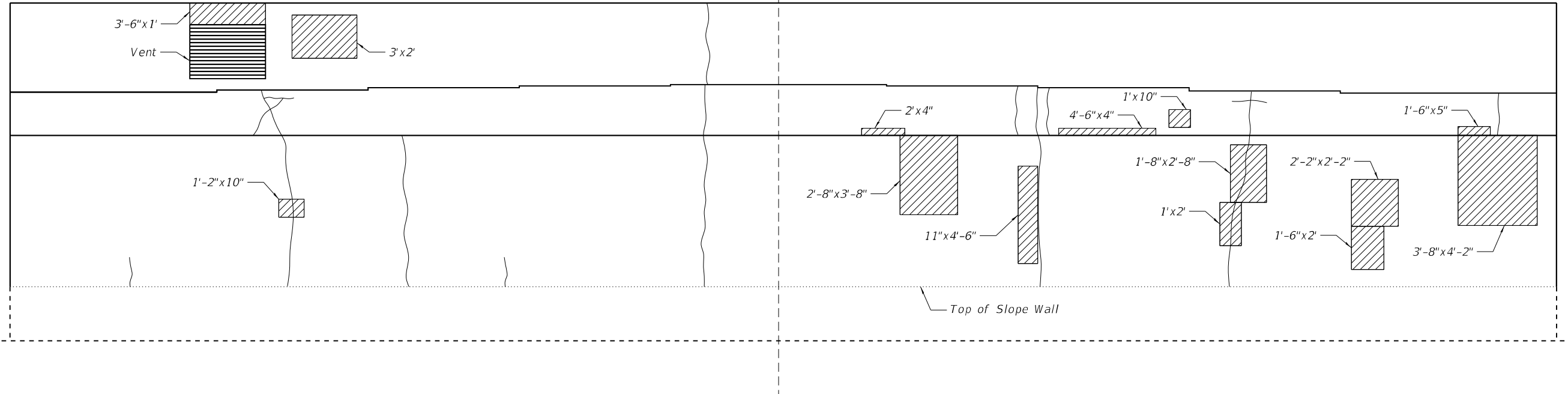
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STRUCTURAL STEEL
STRUCTURE NO. 016-1095

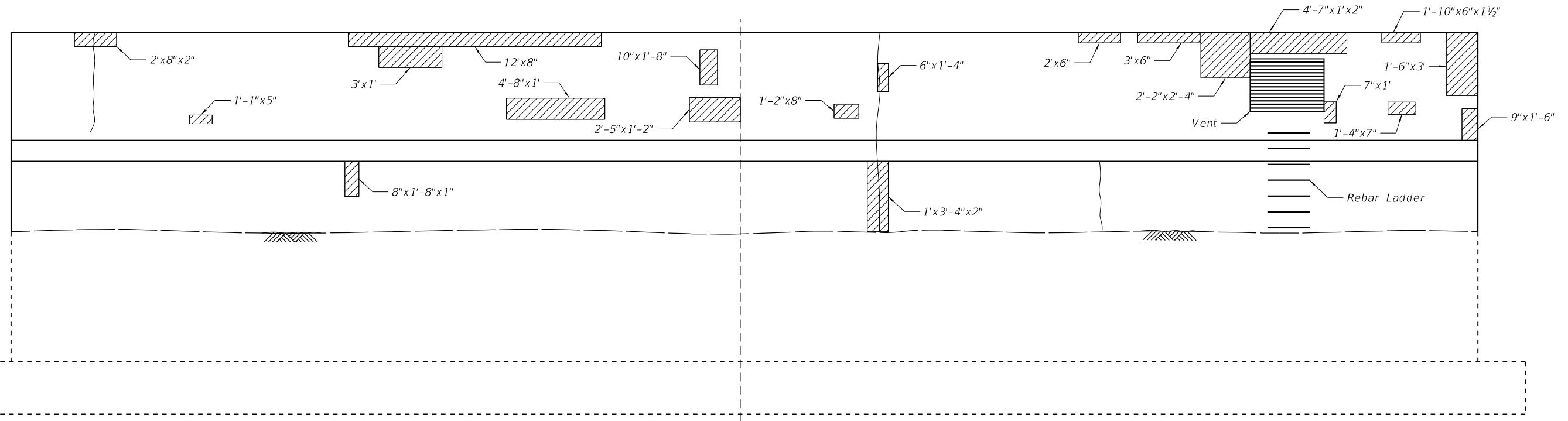
SHEET 23 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	56
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

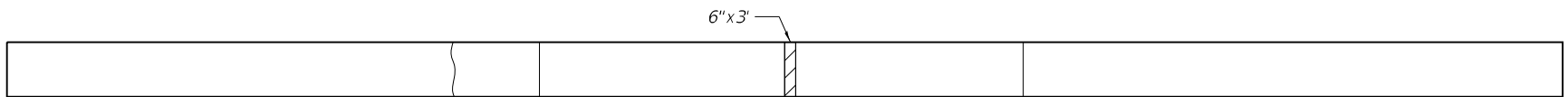
MODEL: Default
 FILE NAME: p:\w\pww\hntb.org\pww\g\eat_Lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO# 1 & TO#5 - 175th Street - 157\CADD\Sheets\102918-sh+BR-WestAbutRepairs.dgn



ELEVATION
 (Looking West)



ELEVATION
 (Looking East - Inside Vault)



WEST VAULTED APPROACH BENT
 (Inside Vault Looking West)

- LEGEND**
- Hairline Crack - Not to be sealed
 - Formed Concrete Repair Depth equal to or less than 5"

WEST ABUTMENT REPAIR BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq Ft	120



USER NAME = \$USER\$	DESIGNED - EG	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED - LM	REVISED -
PLOT DATE = \$DATES\$	DRAWN - EG	REVISED -
	CHECKED - LM	REVISED -

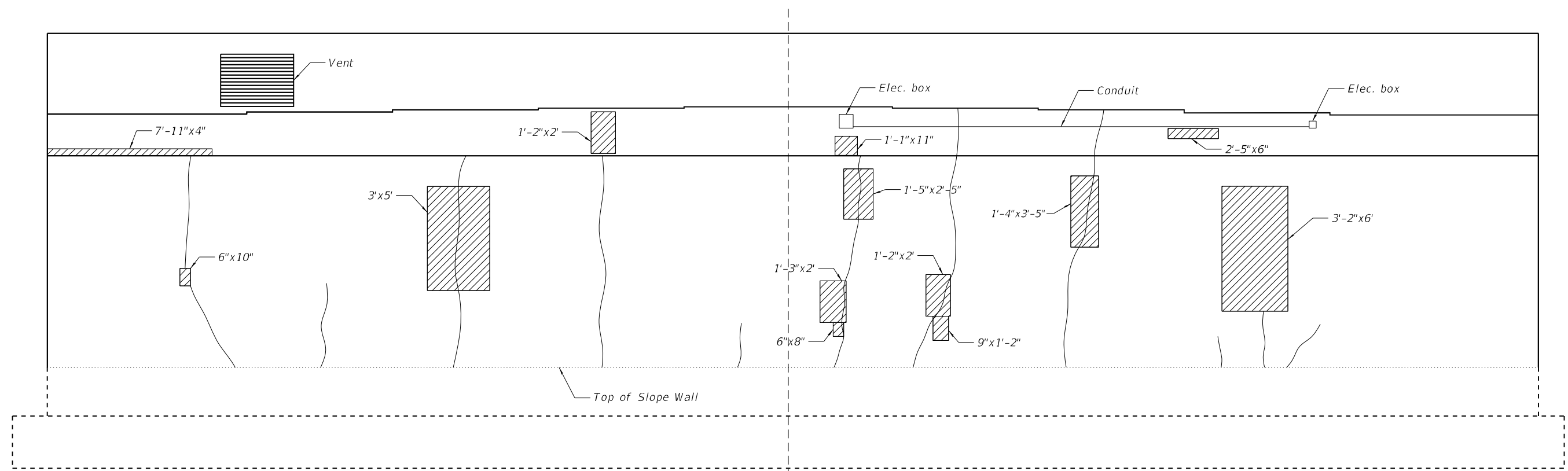
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WEST ABUTMENT REPAIR DETAILS
STRUCTURE NO. 016-1095

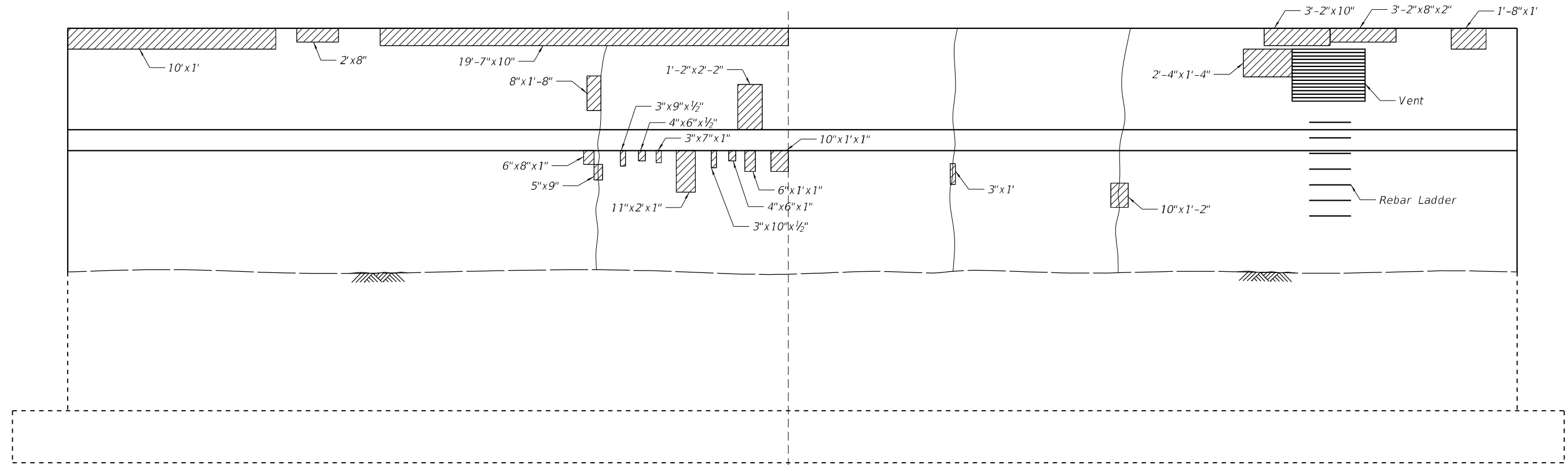
SHEET 25 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	58
			CONTRACT NO. 62P10	
		ILLINOIS FED. AID PROJECT		

MODEL: Default
 FILE NAME: p:\w\m\h\h\h\org\p\w\g\eat_Lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO# 1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh+BR-EastAbutRepairs.dgn
 2/4/2022 9:18:25 AM



ELEVATION
 (Looking East)



ELEVATION
 (Looking West - Inside Vault)

- LEGEND**
- Hairline Crack - Not to be sealed
 - Formed Concrete Repair Depth equal to or less than 5"

EAST ABUTMENT REPAIR BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq Ft	120

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EAST ABUTMENT REPAIR DETAILS
STRUCTURE NO. 016-1095

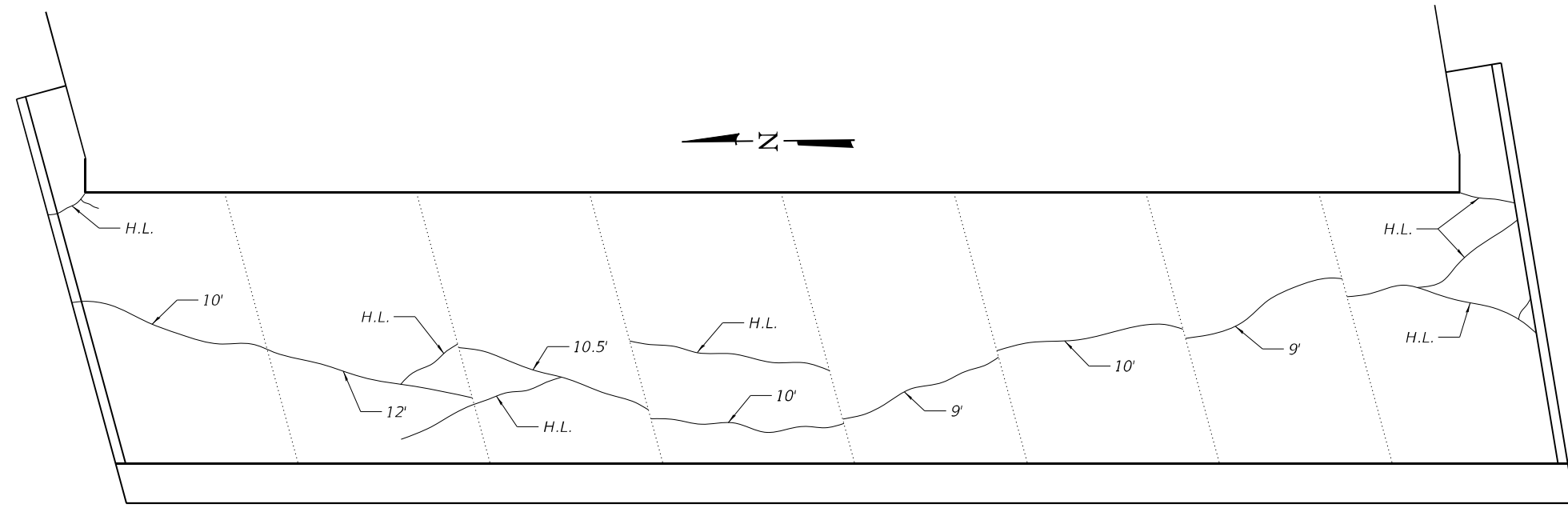
SHEET 26 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	59
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

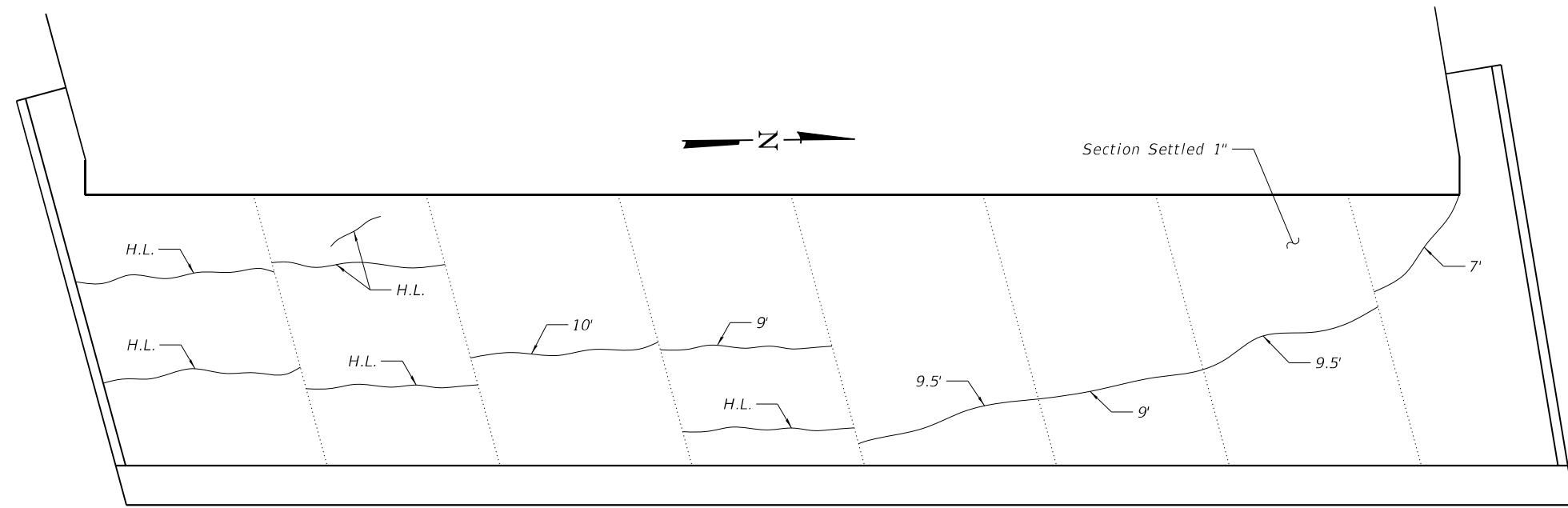


USER NAME =	\$USERS\$	DESIGNED -	EG	REVISED -	
		CHECKED -	LM	REVISED -	
PLOT SCALE =	\$SCALE\$	DRAWN -	EG	REVISED -	
PLOT DATE =	\$DATES\$	CHECKED -	LM	REVISED -	

MODEL: Default
 FILE NAME: p:\w\pww-hntb.org\pww\great_lakes\Documents\Chicago Projects\80677 IDOT Phase II Various Task Order\TO#1 & TO#5 - 175th Street at I-57\CADD\Sheets\102918-sh-BR-SlopeWallsRepairs.dgn
 2/4/2022 9:18:31 AM



PLAN
 (East Slope)



PLAN
 (West Slope)

SLOPED WALL REPAIRS BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Epoxy Crack Injection	FOOT	126

LEGEND

- 5' —| Epoxy Crack Sealing
- H.L. —| Hairline Crack - Not to be sealed



USER NAME = \$USERS\$	DESIGNED - EG	REVISED -
	CHECKED - LM	REVISED -
PLOT SCALE = \$SCALE\$	DRAWN - EG	REVISED -
PLOT DATE = \$DATES\$	CHECKED - LM	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SLOPE WALLS REPAIR DETAILS
 STRUCTURE NO. 016-1095**

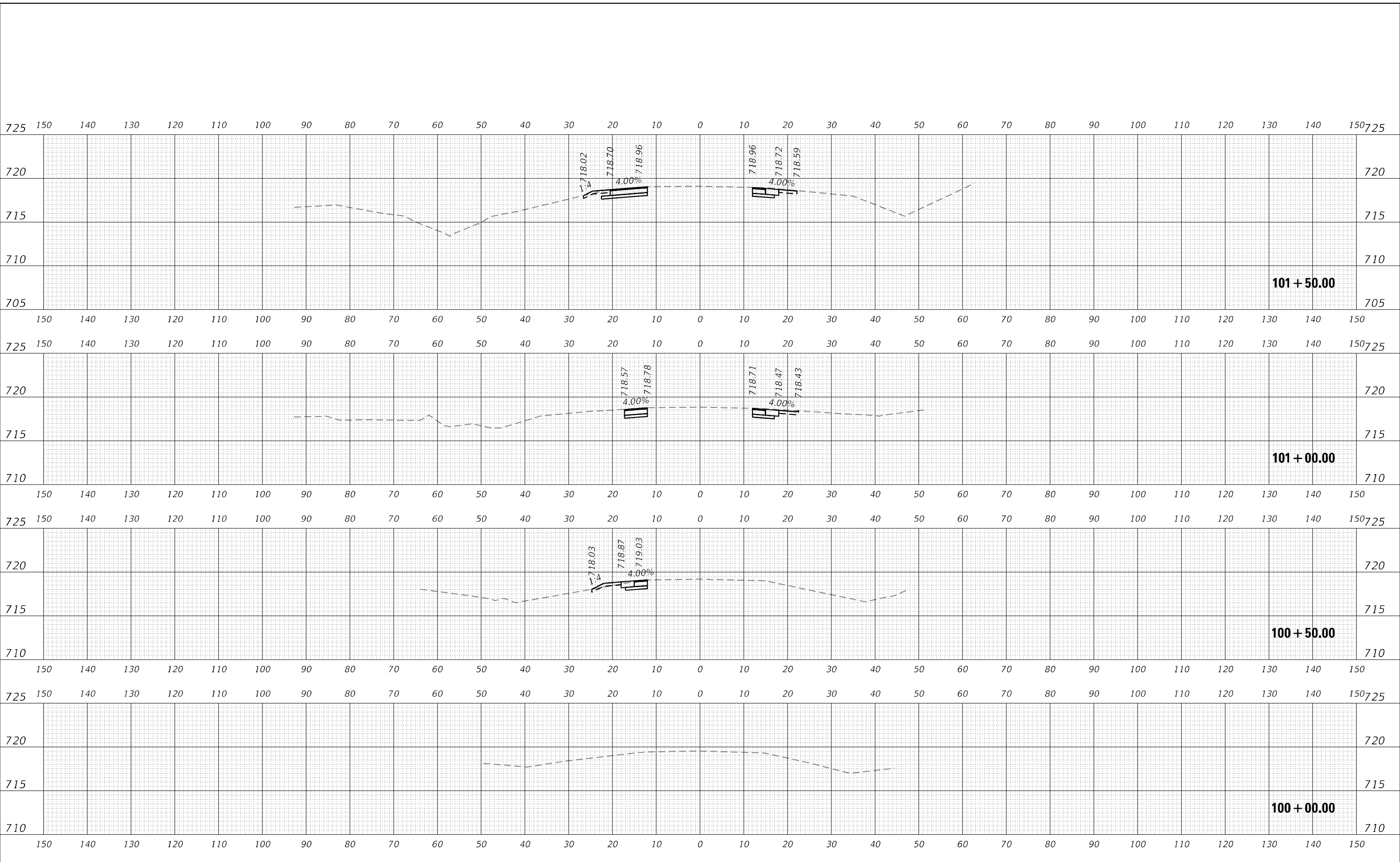
SHEET 28 OF 29 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	61
			CONTRACT NO. 62P10	
		ILLINOIS	FED. AID PROJECT	

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

MODEL: I75TH_E_01_19_2022
 FILE NAME: D:\work\in\comb\p\Grest_Lake\Documents\Chicago Project\80677\DOT Phase II\Various Task\Grden\T01 & T045 - 175th Street at I-57\CADD\ModelID 102913a-Cont\aster.dgn



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.



USER NAME = amalla.baymundo	DESIGNED - MAK	REVISED -
	DRAWN - MAK	REVISED -
PLOT SCALE = 20,0000 * / in.	CHECKED - HER	REVISED -
PLOT DATE = 2/1/2022	DATE - 1/31/2022	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

I-57 AT 175TH STREET
 PROPOSED CROSS SECTIONS

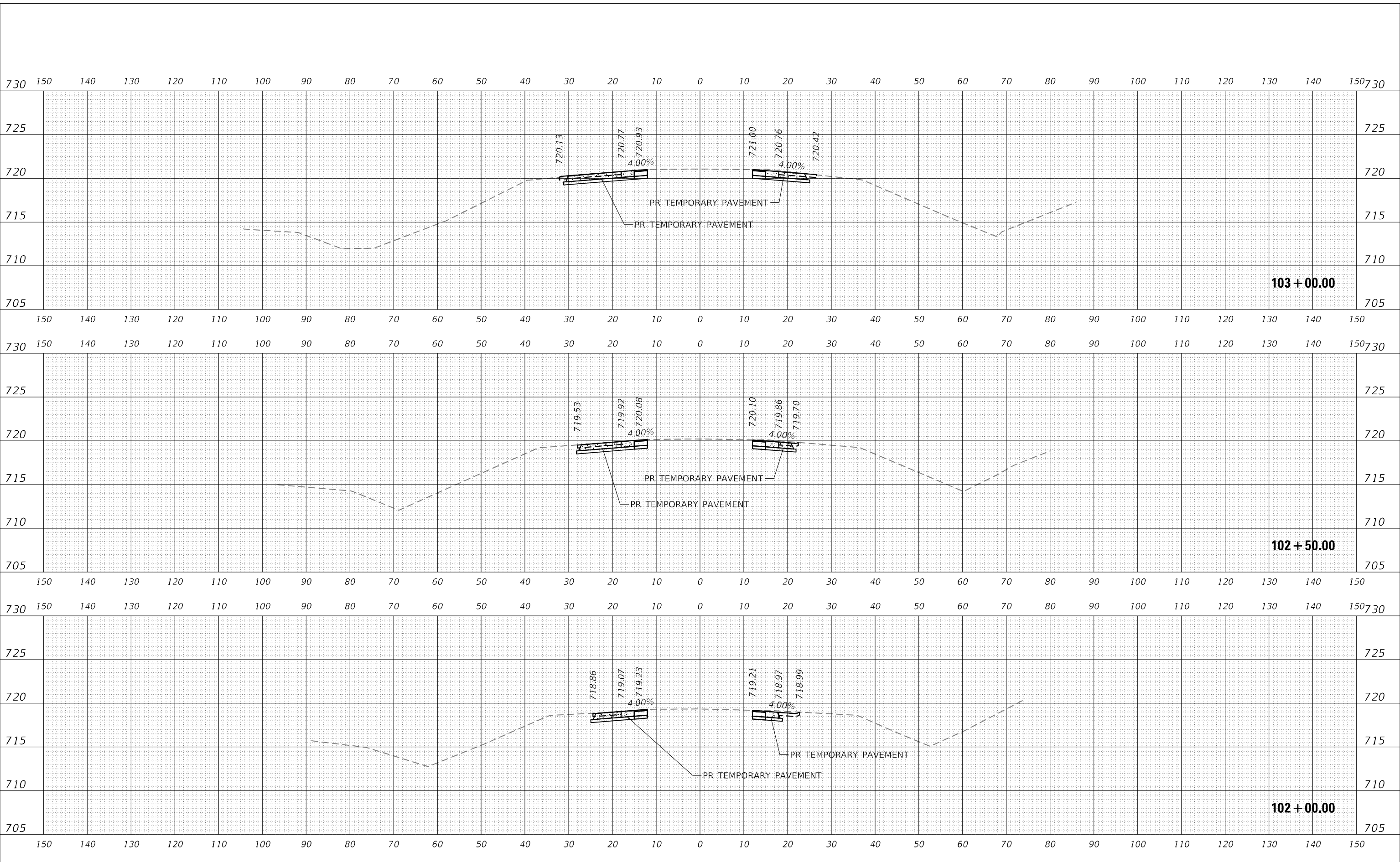
SCALE: 1:20 H 1:10 V SHEET 1 OF 16 SHEETS STA. 100+00.00 TO STA. 101+50.00

F.A.1 RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	63
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
	TEMPLATE	
	AREAS CHECKED	

ORIGINAL SURVEY NO.	SURVEYED	DATE
	PLOTTED	BY
	TEMPLATE	
	AREAS CHECKED	

MODEL: I75HL_E_01_19_2022
 FILE NAME: D:\work\in\cma\p\w\Great_Lakes\Documents\Chicago Projects\8027\DOT Phase II Various Task Order\01 & 045 - 175th Street at I-57\CADD\Model\102910a-Conf\aster.dgn



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.



USER NAME = amalla.baymundo	DESIGNED - MAK	REVISIONS
	DRAWN - MAK	REVISIONS
PLOT SCALE = 20,0000' / in.	CHECKED - HER	REVISIONS
PLOT DATE = 2/1/2022	DATE - 1/31/2022	REVISIONS

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

I-57 AT 175TH STREET
 PROPOSED CROSS SECTIONS

SCALE: 1:20 H 1:10 V SHEET 2 OF 16 SHEETS STA. 102+00.00 TO STA. 103+00.00

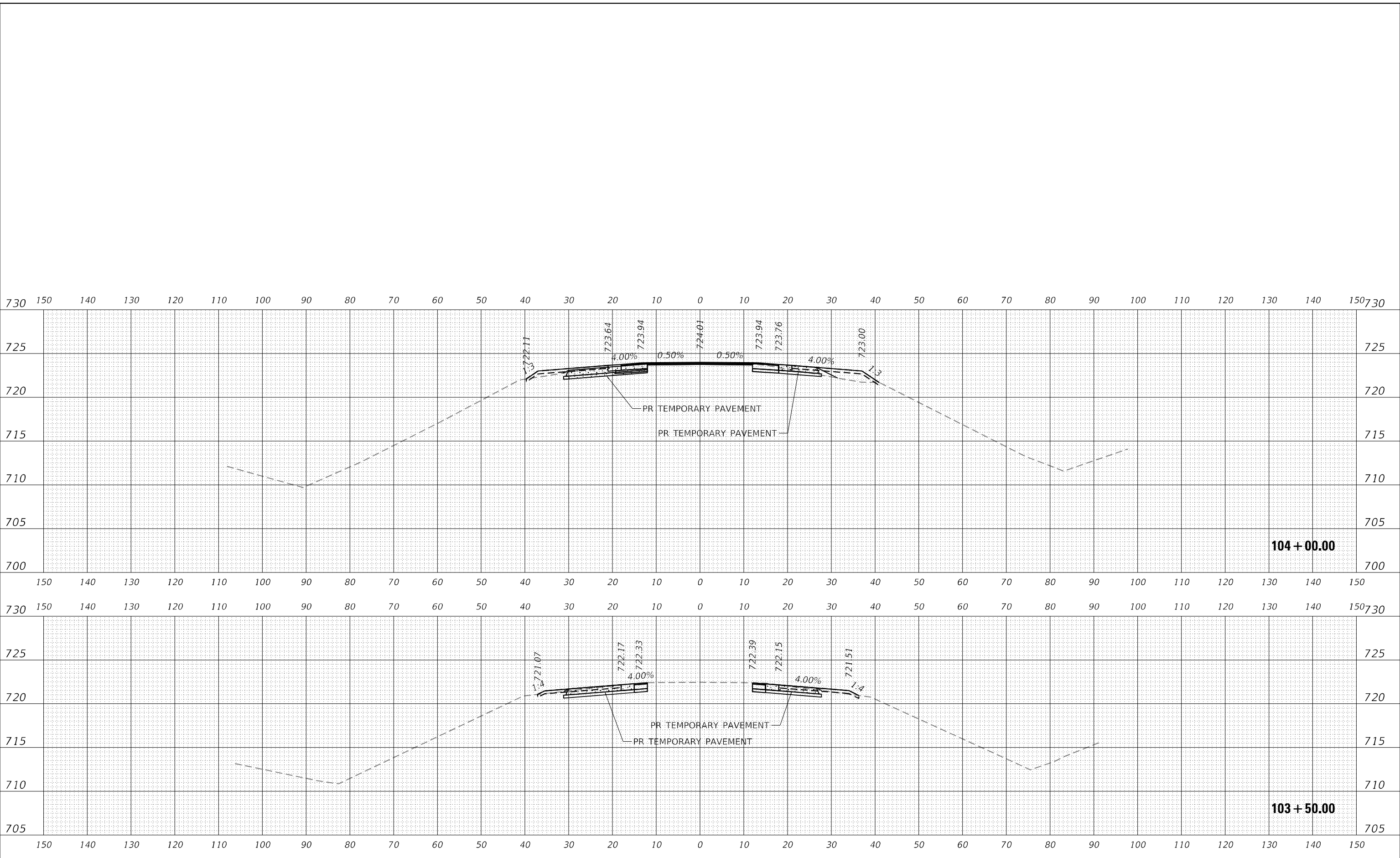
F.A.1 RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	64
CONTRACT NO. 62P10				

ILLINOIS FED. AID PROJECT

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

MODEL: I75TL_E_01_19_2022
 FILE NAME: D:\work\2021\Illinois\Projects\DOT Phase II Various Task Order\01 & 02\5 - 175th Street at I-57\CADD\Model\103910-Cont\aster.dgn



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.



USER NAME = amalla.baymundo	DESIGNED - MAK	REVISIED -
	DRAWN - MAK	REVISIED -
PLOT SCALE = 20,0000' / in.	CHECKED - HER	REVISIED -
PLOT DATE = 2/1/2022	DATE - 1/31/2022	REVISIED -

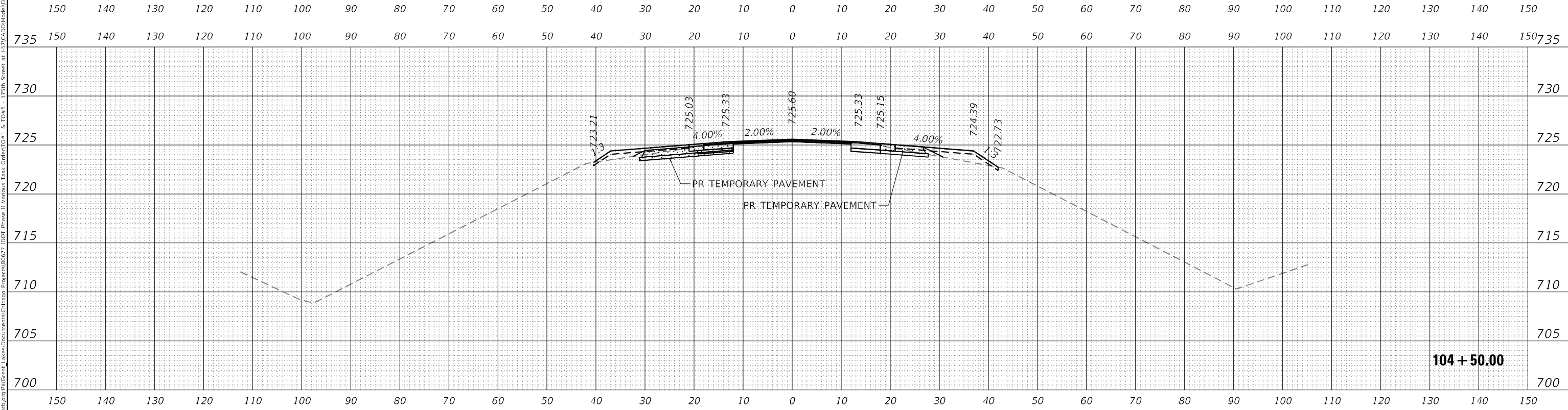
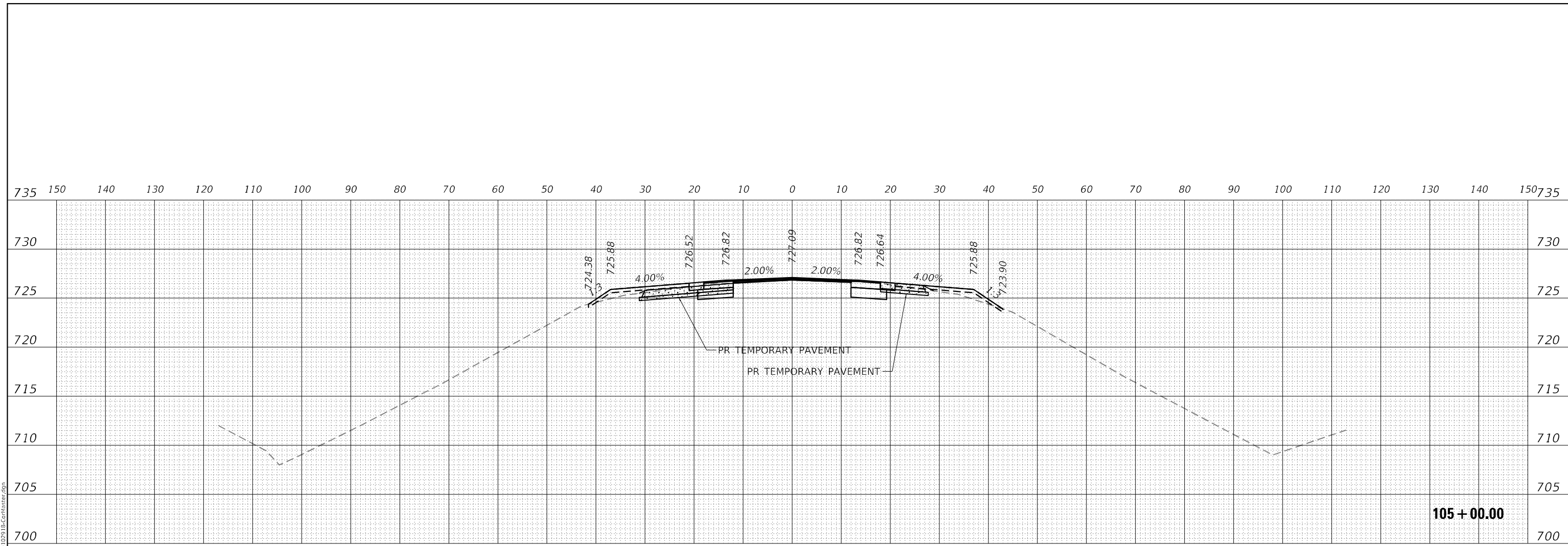
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

I-57 AT 175TH STREET
 PROPOSED CROSS SECTIONS

SCALE: 1:20 H 1:10 V SHEET 3 OF 16 SHEETS STA. 103+50.00 TO STA. 104+00.00

F.A.1 RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	65
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED



USER NAME = amalla,baymundo	DESIGNED - MAK	REVISED -
	DRAWN - MAK	REVISED -
PLOT SCALE = 20,0000' / in.	CHECKED - HER	REVISED -
PLOT DATE = 2/1/2022	DATE - 1/31/2022	REVISED -

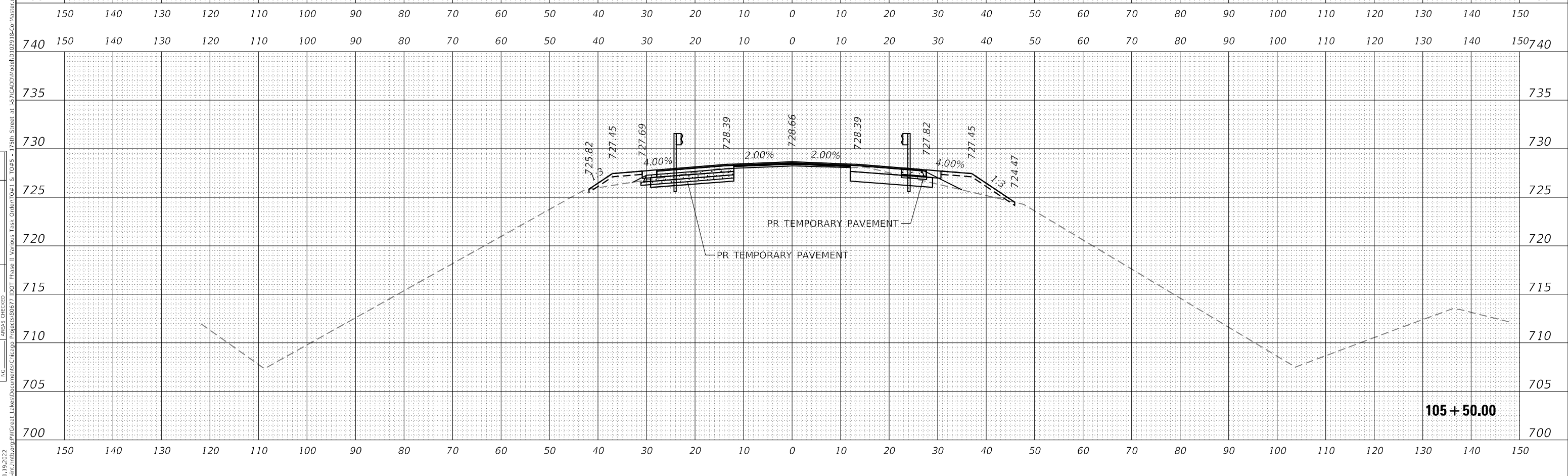
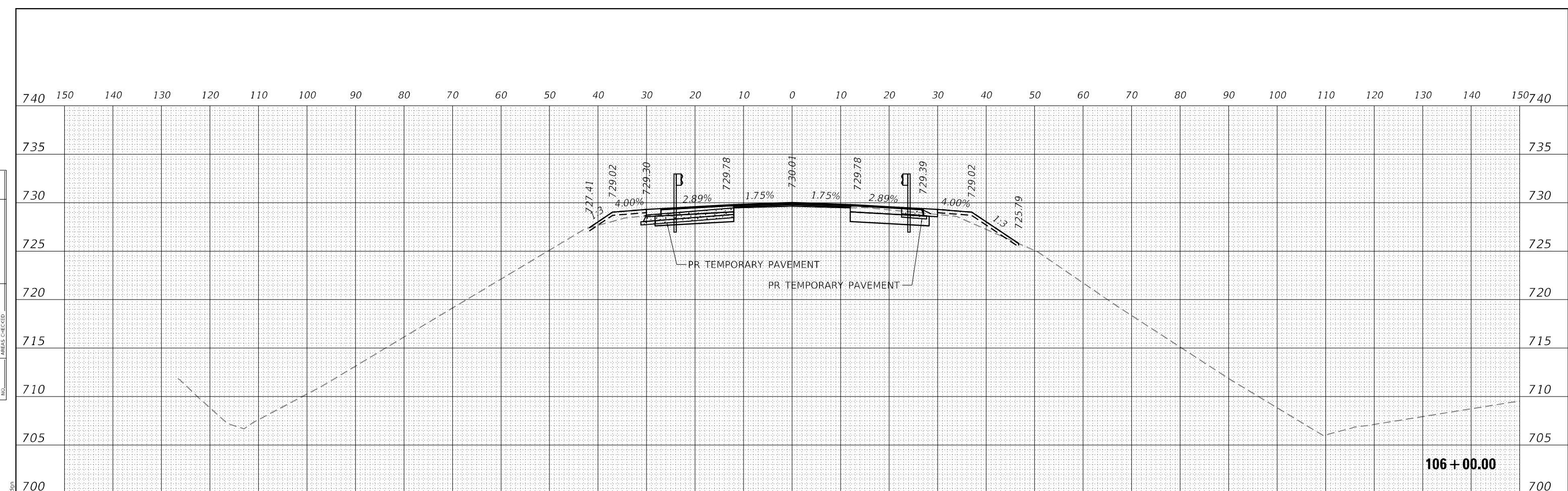
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-57 AT 175TH STREET
PROPOSED CROSS SECTIONS**

SCALE: 1:20 H 1:10 V SHEET 4 OF 16 SHEETS STA. 104+50.00 TO STA. 105+00.00

F.A.1 RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	66
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

MODEL: 175TH_E_01_19_2022
FILE NAME: D:\work\175th\175th\175th\175th Street at I-57\CADD\175th Street at I-57.dwg



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.

FINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	
AREAS CHECKED	TEMPLATE	
	AREAS CHECKED	

ORIGINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	
AREAS CHECKED	TEMPLATE	
	AREAS CHECKED	



USER NAME	= amalla.baymundo
PLOT SCALE	= 20,0000' * / in.
PLOT DATE	= 2/1/2022

DESIGNED	- MAK
DRAWN	- MAK
CHECKED	- HER
DATE	- 1/31/2022

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-57 AT 175TH STREET
PROPOSED CROSS SECTIONS

SCALE: 1:20 H 1:10 V SHEET 5 OF 16 SHEETS STA. 105+50.00 TO STA. 106+00.00

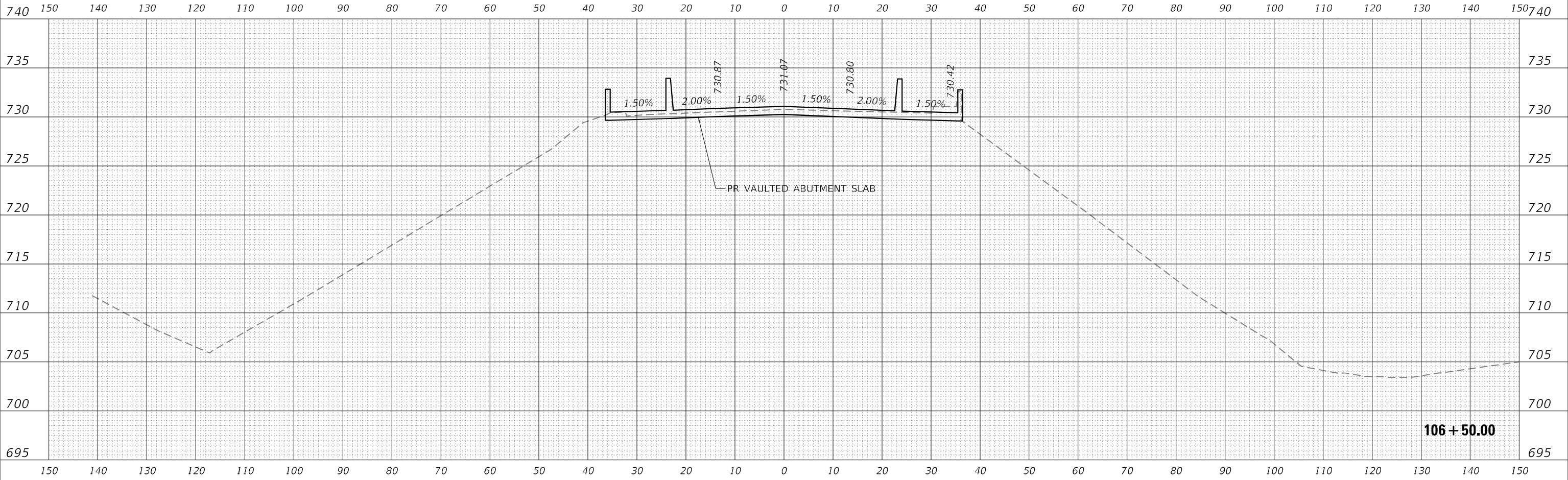
F.A.1 RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	67
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

MODEL: 175TH_E_01_19_2022
FILE NAME: D:\work\175th\175th\175th Street at I-57\CADD\ModelID 105910-01.dwg

FINAL SURVEY NO.	SURVEYED	BY	DATE
	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

MODEL: I75TH_E_01_19_2022
 FILE NAME: D:\work\2021\I75th\I75th\DOT Phase II Various Task Order\DOT1 & DOT5 - I75th Street at I57\CADD\Model\10291a-Cont\aster.dgn



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.



USER NAME = amalla.baymundo	DESIGNED - MAK	REVISED -
	DRAWN - MAK	REVISED -
PLOT SCALE = 20,0000 * / in.	CHECKED - HER	REVISED -
PLOT DATE = 3/15/2022	DATE - 1/31/2022	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

I-57 AT 175TH STREET
 PROPOSED CROSS SECTIONS

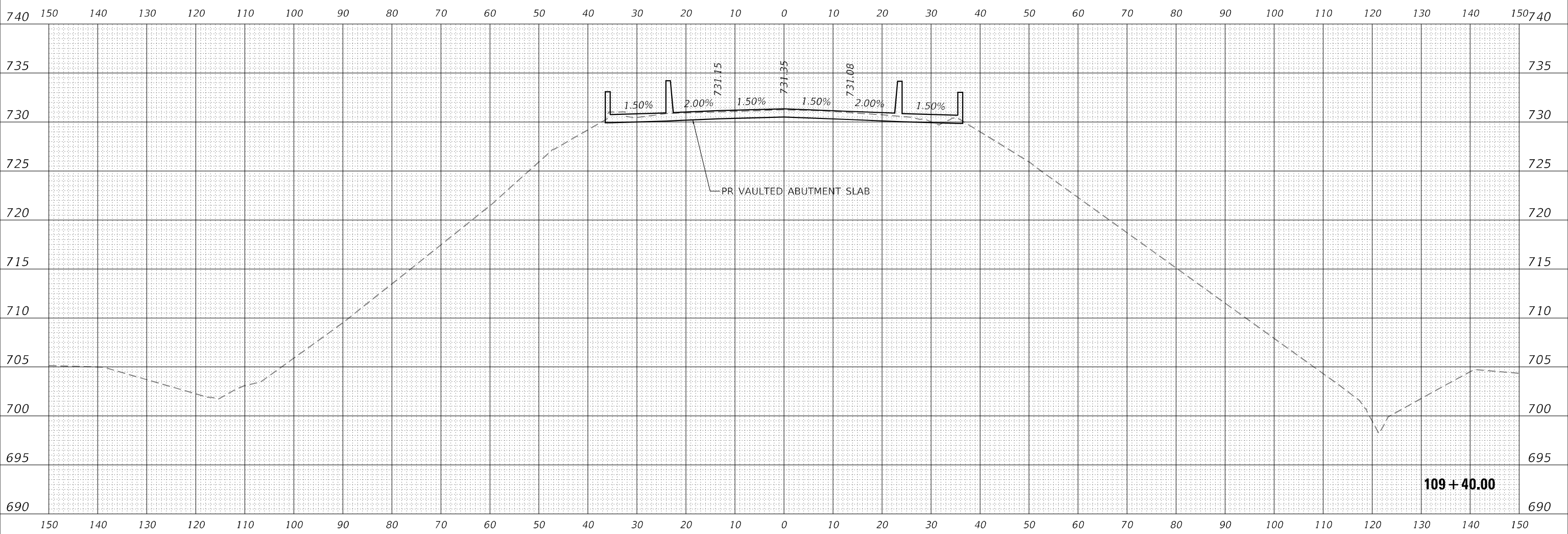
SCALE: 1:20 H 1:10 V SHEET 6 OF 16 SHEETS STA. 106+50.00 TO STA. 106+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	68
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

MODEL: I75TH_E_01_19_2022
 FILE NAME: D:\work\2021\2021-073-BDR\I75TH\DOT Phase II\Various Task\Grden\T01 & T045 - 175th Street at I-57\CAD\Model\102918-Contaster.dgn



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.



USER NAME = amalla.baymundo	DESIGNED - MAK	REVISIONS
PLOT SCALE = 20,0000 * / in.	DRAWN - MAK	REVISIONS
PLOT DATE = 3/15/2022	CHECKED - HER	REVISIONS
	DATE - 1/31/2022	REVISIONS

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**I-57 AT 175TH STREET
 PROPOSED CROSS SECTIONS**

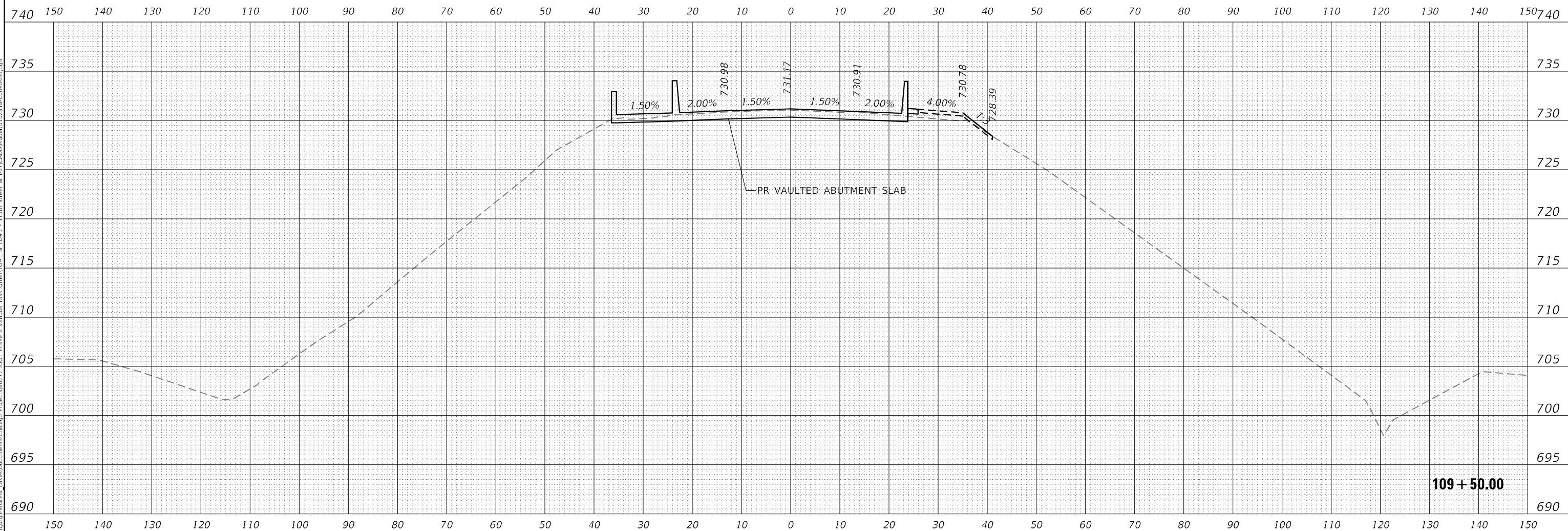
SCALE: 1:20 H 1:10 V SHEET 7 OF 16 SHEETS STA. 109+40.00 TO STA. 109+40.00

F.A.1 RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	69
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

MODEL: I75TH_E_01_19_2022
 FILE NAME: D:\work\2021\Illinois\I75TH\DOT Phase II\Various Task\Grden\T01 & T045 - I75th Street at I-57\CAD\Model\ID 10291\B-Contaster.dgn



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.



USER NAME = amalla.baymundo	DESIGNED - MAK	REVISIONS
PLOT SCALE = 20,0000 * / in.	DRAWN - MAK	REVISIONS
PLOT DATE = 3/15/2022	CHECKED - HER	REVISIONS
	DATE - 1/31/2022	REVISIONS

DESIGNED - MAK	REVISIONS
DRAWN - MAK	REVISIONS
CHECKED - HER	REVISIONS
DATE - 1/31/2022	REVISIONS

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

I-57 AT 175TH STREET
 PROPOSED CROSS SECTIONS

SCALE: 1:20 H 1:10 V SHEET 8 OF 16 SHEETS STA. 109+50.00 TO STA. 109+50.00

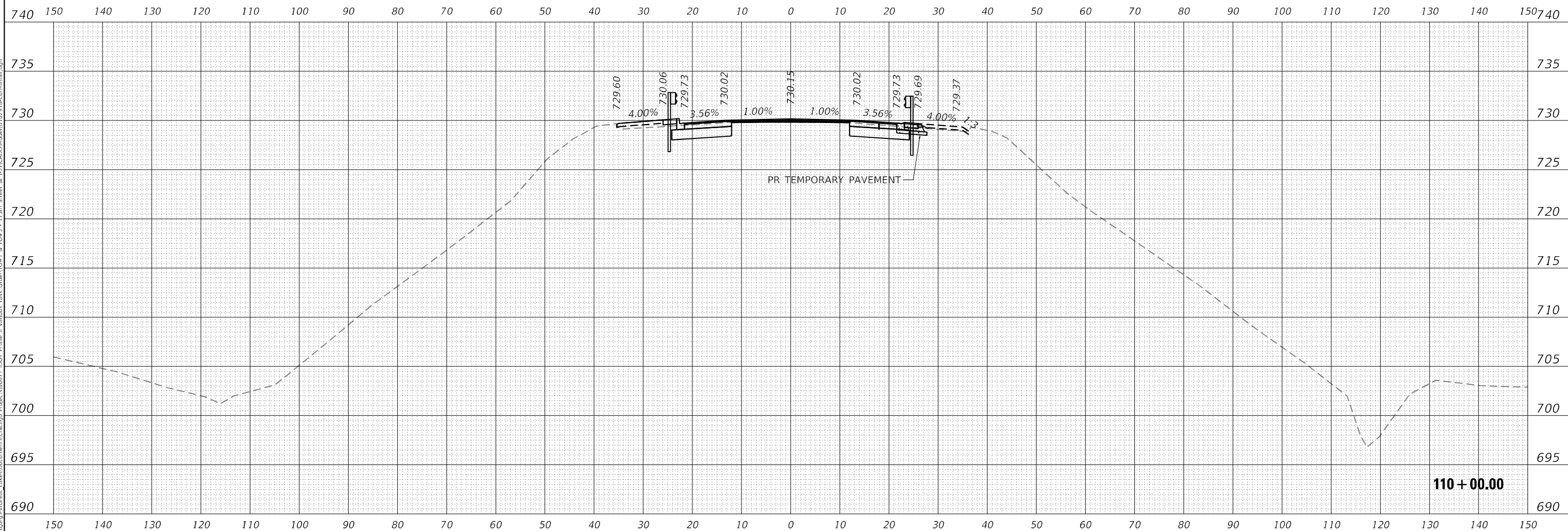
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	70
CONTRACT NO. 62P10				

ILLINOIS FED. AID PROJECT

FINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

MODEL: I75TH_E_01_19_2022
 FILE NAME: D:\work\in\camb\p\I75th\I75th Street at I-57\CAD\ModelID 102918\ConMaster.dgn
 PROJECT: CHICAGO PROJECTS\DOT Phase II Various Task Order\15 10415 - 175th Street at I-57\CAD\ModelID 102918\ConMaster.dgn



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.



USER NAME = amalla.baymundo	DESIGNED - MAK	REVISIED -
PLOT SCALE = 20,0000 * / in.	DRAWN - MAK	REVISIED -
PLOT DATE = 2/1/2022	CHECKED - HER	REVISIED -
	DATE - 1/31/2022	REVISIED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

I-57 AT 175TH STREET
 PROPOSED CROSS SECTIONS

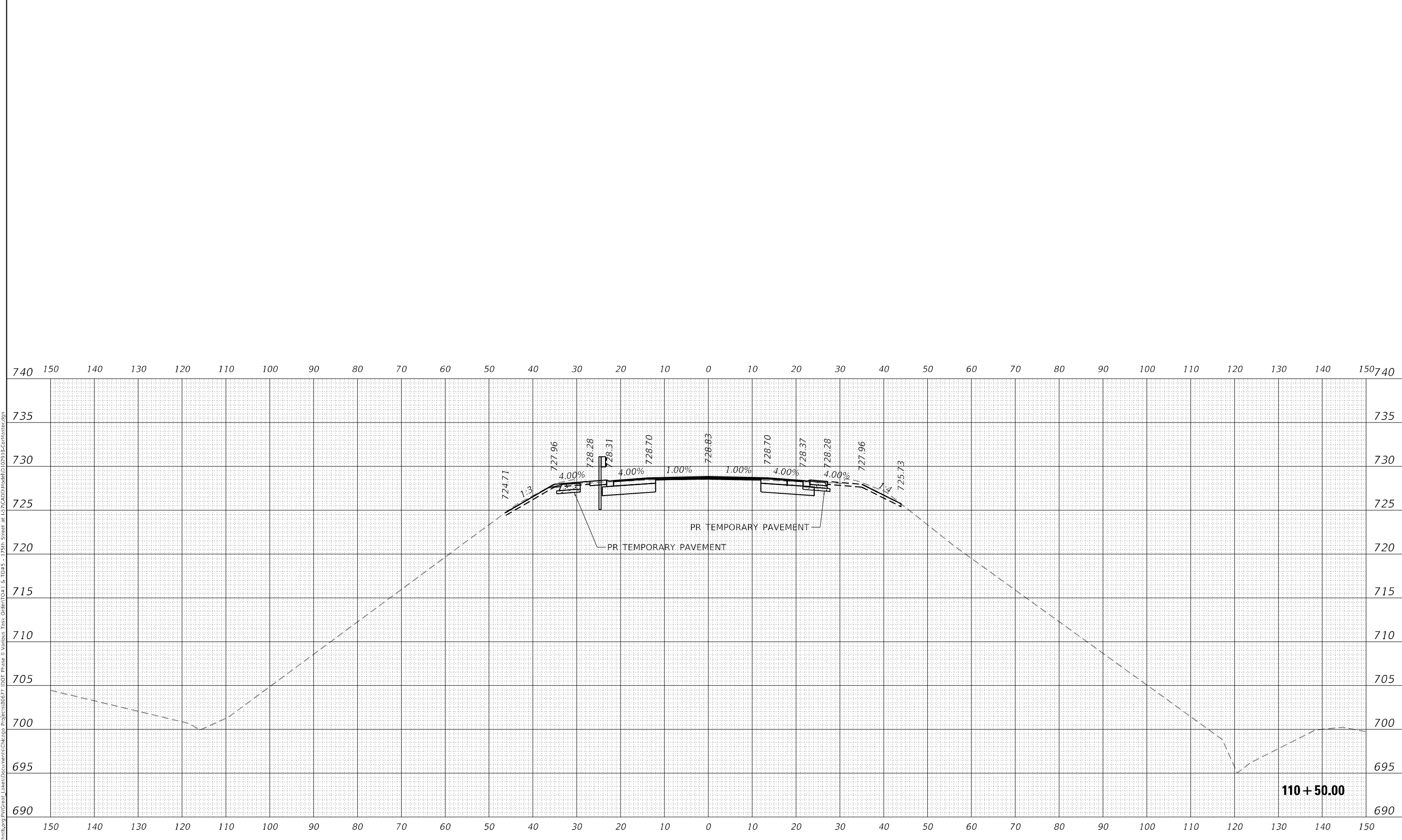
SCALE: 1:20 H 1:10 V SHEET 9 OF 16 SHEETS STA. 110+00.00 TO STA. 110+00.00

F.A.1 RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	71
			CONTRACT NO. 62P10	
			ILLINOIS FED. AID PROJECT	

FINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		

MODEL: I75TL_E_01_19_2022
 FILE NAME: D:\work\in\camb\p\w\grast_lake\Documents\Chicago Project\80677\DOT Phase II Various Task Order\01 & 02\15 - 175th Street at I-57\CADD\Model\102918-Contaster.dgn



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.



USER NAME = amalla.baymundo	DESIGNED - MAK	REVISED -
	DRAWN - MAK	REVISED -
PLOT SCALE = 20,0000 * / in.	CHECKED - HER	REVISED -
PLOT DATE = 2/1/2022	DATE - 1/31/2022	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**I-57 AT 175TH STREET
 PROPOSED CROSS SECTIONS**

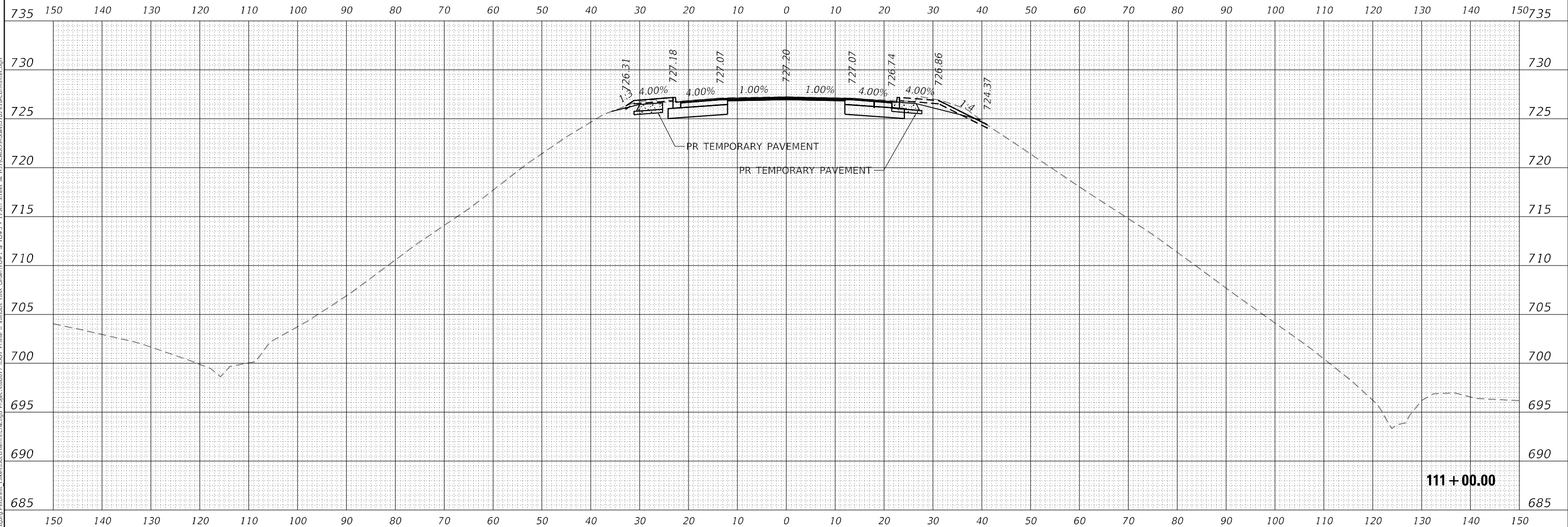
SCALE: 1:20 H 1:10 V SHEET 10 OF 16 SHEETS STA. 110+50.00 TO STA. 110+50.00

F.A.1 RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	72
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED	BY	DATE
	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

MODEL: I75TH_E_01_19_2022
 FILE NAME: D:\work\2021\2021-073-BDR\I75TH ST\I75TH ST - 175th Street at I-57\CAD\Model\102910-2021-073-BDR.dwg



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.



USER NAME = amalla.baymundo	DESIGNED - MAK	REVISIONS
PLOT SCALE = 20,0000 * / in.	DRAWN - MAK	REVISIONS
PLOT DATE = 2/1/2022	CHECKED - HER	REVISIONS
	DATE - 1/31/2022	REVISIONS

DESIGNED - MAK	REVISIONS
DRAWN - MAK	REVISIONS
CHECKED - HER	REVISIONS
DATE - 1/31/2022	REVISIONS

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**I-57 AT 175TH STREET
 PROPOSED CROSS SECTIONS**

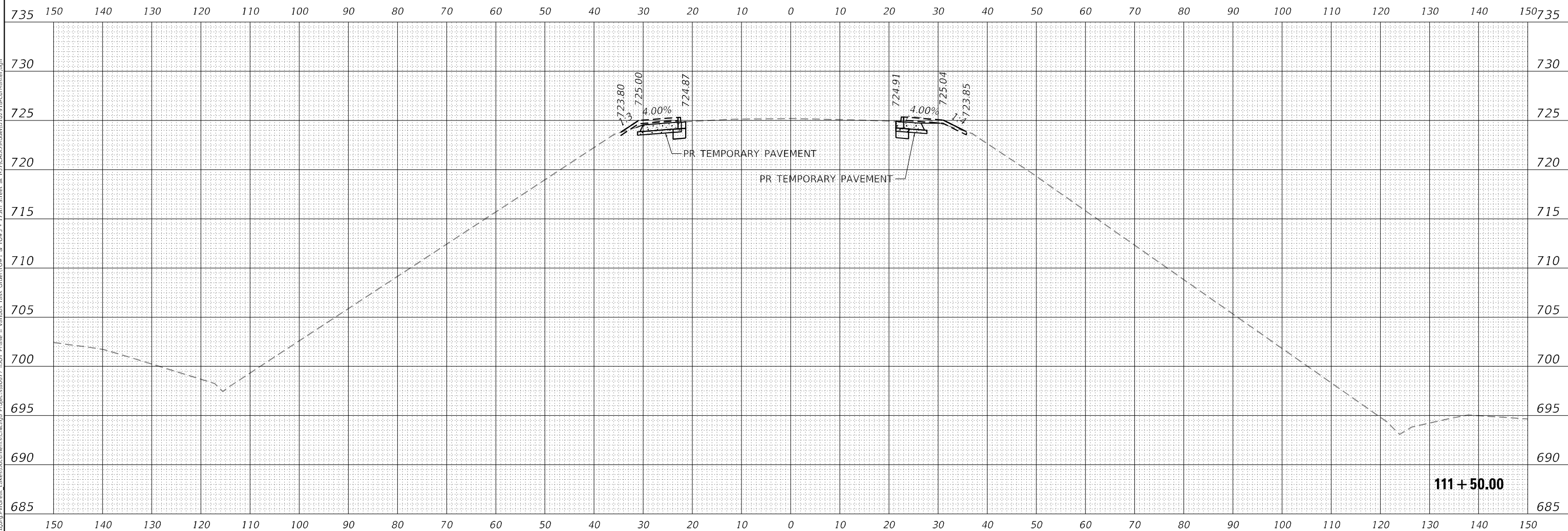
SCALE: 1:20 H 1:10 V SHEET 11 OF 16 SHEETS STA. 111+00.00 TO STA. 111+00.00

F.A.1 RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	73
CONTRACT NO. 62P10			ILLINOIS FED. AID PROJECT	

FINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

MODEL: I75TH_E_01_19_2022
 FILE NAME: D:\work\I75th\I75th_175th_Street_at_I57\CADD\ModelID 10291\I57-Conf\aster.dgn
 PROJECT: I75TH DOT Phase II Various Task Order 1 & 1045 - 175th Street at I57



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.



USER NAME = amalla.baymundo	DESIGNED - MAK	REVISIONS
PLOT SCALE = 20,0000' / in.	DRAWN - MAK	REVISIONS
PLOT DATE = 2/1/2022	CHECKED - HER	REVISIONS
	DATE - 1/31/2022	REVISIONS

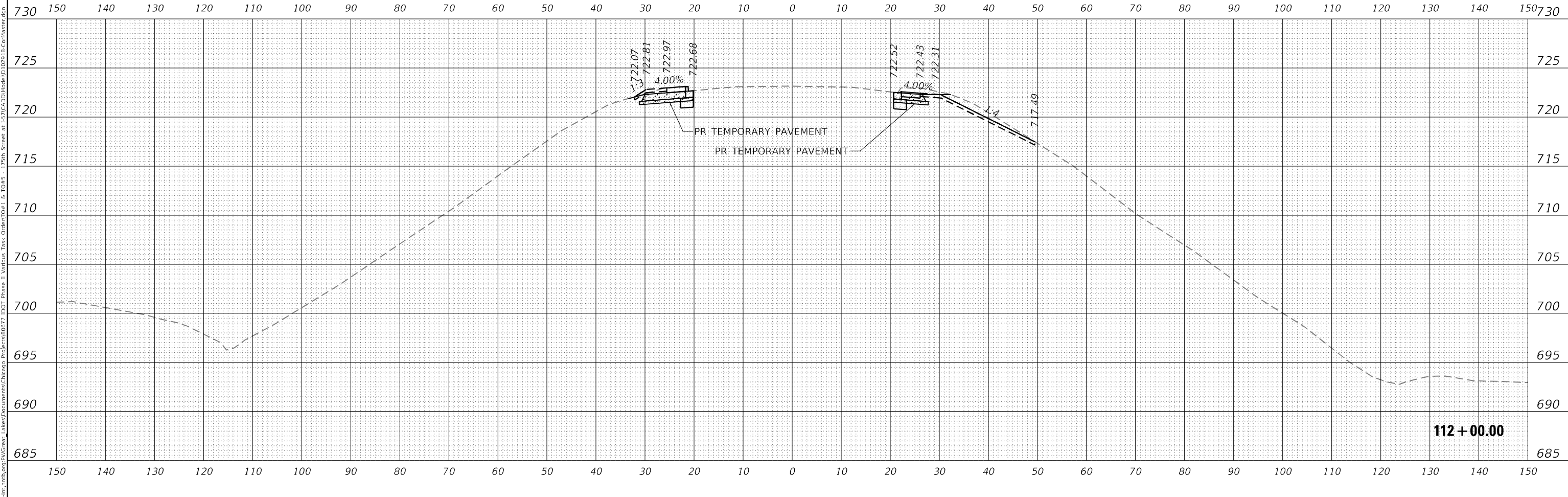
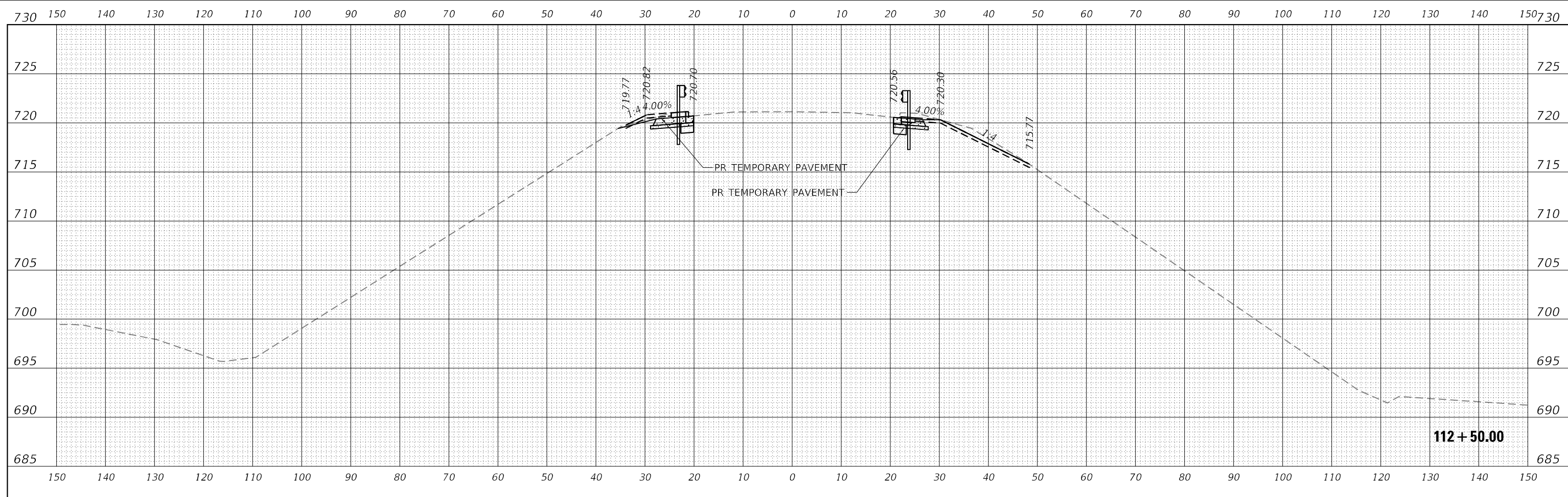
DESIGNED - MAK	REVISIONS
DRAWN - MAK	REVISIONS
CHECKED - HER	REVISIONS
DATE - 1/31/2022	REVISIONS

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

I-57 AT 175TH STREET
 PROPOSED CROSS SECTIONS

SCALE: 1:20 H 1:10 V SHEET 12 OF 16 SHEETS STA. 111+50.00 TO STA. 111+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	74
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.

FINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	
AREAS CHECKED	TEMPLATE	
	AREAS CHECKED	

ORIGINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	
AREAS CHECKED	TEMPLATE	
	AREAS CHECKED	

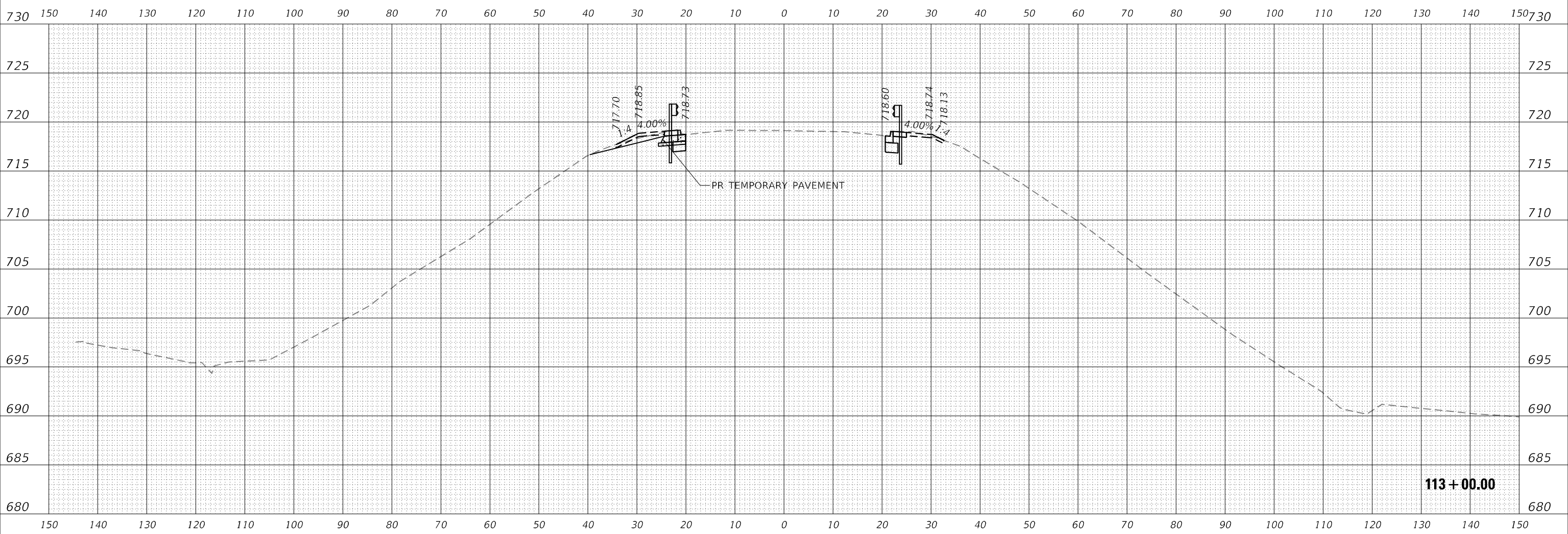
MODEL: 175TH_E_01_19_2022
 FILE NAME: D:\work\CAD\Drawings\175th Street at I-57\CADD\Model\102918-Const\Sheet.dgn

	USER NAME = amalla.baymundo	DESIGNED - MAK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION I-57 AT 175TH STREET PROPOSED CROSS SECTIONS	F.A.I. RTE. 57	SECTION 2021-073-BDR	COUNTY COOK	TOTAL SHEETS 89	SHEET NO. 75	
	PLOT SCALE = 20,0000 * / in.	CHECKED - HER	REVISED -		SCALE: 1:20 H 1:10 V SHEET 13 OF 16 SHEETS STA. 112+00.00 TO STA. 112+50.00					
	PLOT DATE = 2/1/2022	DATE - 1/31/2022	REVISED -		CONTRACT NO. 62P10					
	ILLINOIS FED. AID PROJECT									

FINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

MODEL: I75TH_E_01_19_2022
 FILE NAME: D:\work\2021\2021-073-BDR\175th Street at I-57\CAD\Model\102918-Conf\aster.dgn



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.



USER NAME = amalla.baymundo	DESIGNED - MAK	REVISED -
PLOT SCALE = 20,0000 * / in.	DRAWN - MAK	REVISED -
PLOT DATE = 2/1/2022	CHECKED - HER	REVISED -
	DATE - 1/31/2022	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

I-57 AT 175TH STREET
 PROPOSED CROSS SECTIONS

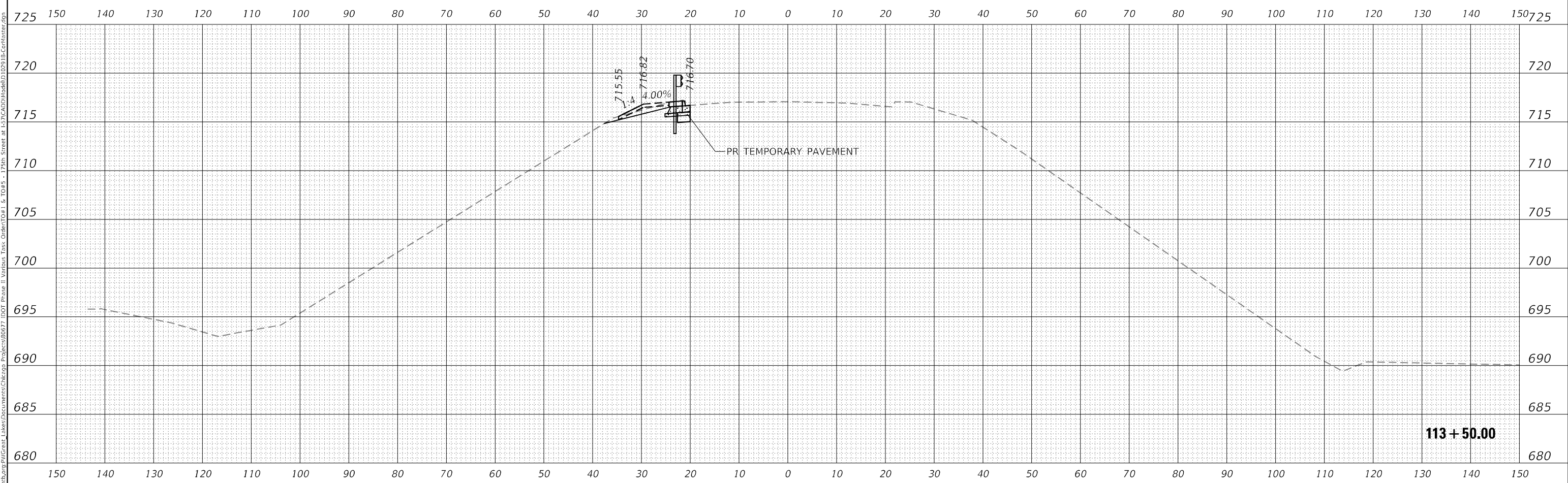
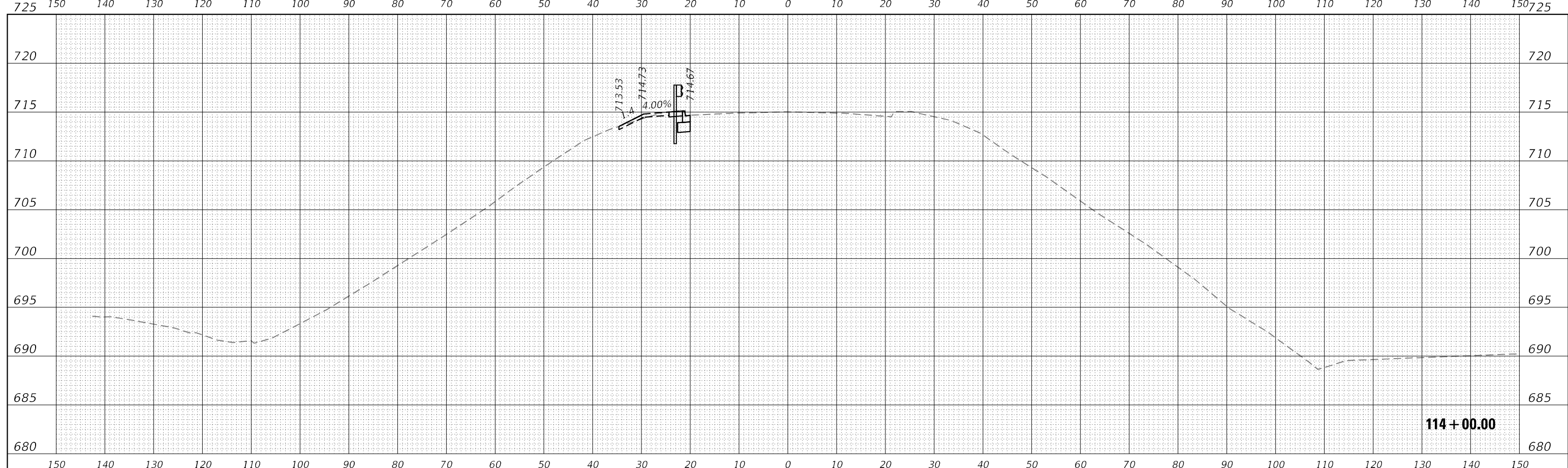
SCALE: 1:20 H 1:10 V SHEET 14 OF 16 SHEETS STA. 113+00.00 TO STA. 113+00.00

F.A.1 RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	76
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

FINAL				
SURVEY	SURVEYED	PLOTTED	TEMPLATE	DATE
NO.	NOTE BOOK	AREAS CHECKED	AREAS CHECKED	

ORIGINAL				
SURVEY	SURVEYED	PLOTTED	TEMPLATE	DATE
NO.	NOTE BOOK	AREAS CHECKED	AREAS CHECKED	

MODEL: 175TH_E_01.19.2022
 FILE NAME: \\pww\hntb\comp\wgreat_lakes\Documents\Chicago Projects\80271\DOT Phase II\Various Task\Grden\011 & 0115 - 175th Street at I-57\CADD\Model\0291a-Contaster.dgn



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.



USER NAME =	amalla.baymundo
DESIGNED -	MAK
DRAWN -	MAK
CHECKED -	HER
DATE -	1/31/2022

DESIGNED -	MAK	REVISED -	
DRAWN -	MAK	REVISED -	
CHECKED -	HER	REVISED -	
DATE -	1/31/2022	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

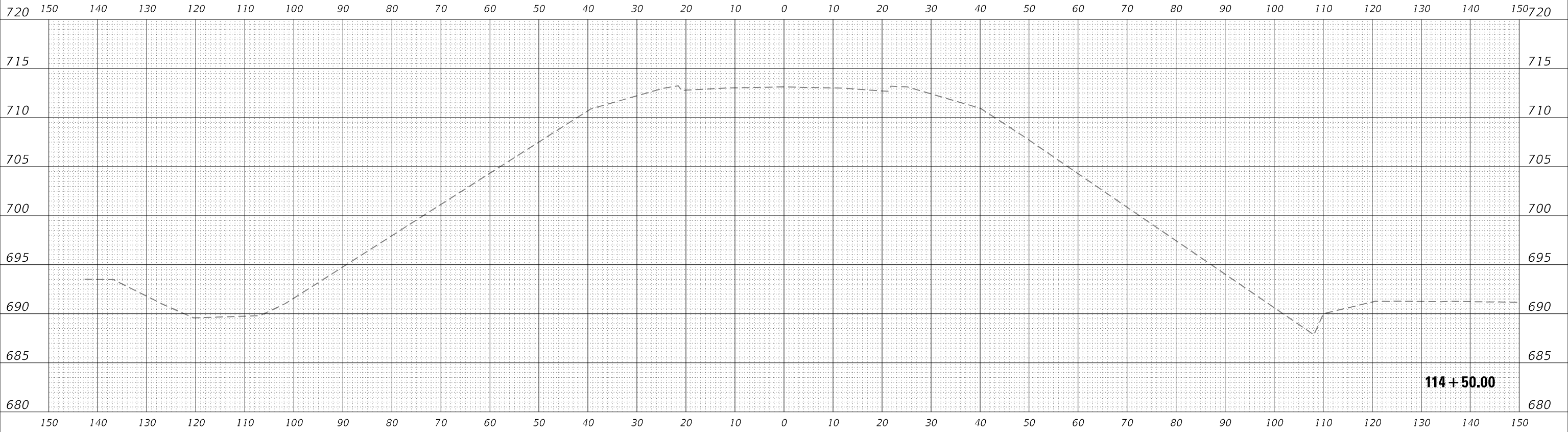
I-57 AT 175TH STREET				
PROPOSED CROSS SECTIONS				
SCALE:	1:20 H 1:10 V	SHEET 15	OF 16 SHEETS	STA. 113+50.00 TO STA. 114+00.00

F.A.1 RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	77
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED AREAS CHECKED	BY	DATE

MODEL: I75TH_E_01_19_2022
 FILE NAME: D:\work\I75th\I75th\DOT Phase II\Various Task\Grden\041 & 045 - 175th Street at I-57\CAD\ModelID 102916-ConfMaster.dgn



NOTES: SEE EARTHWORK SCHEDULES ON SHEET 8 FOR BREAKDOWN OF EARTHWORK QUANTITIES BY PHASE.



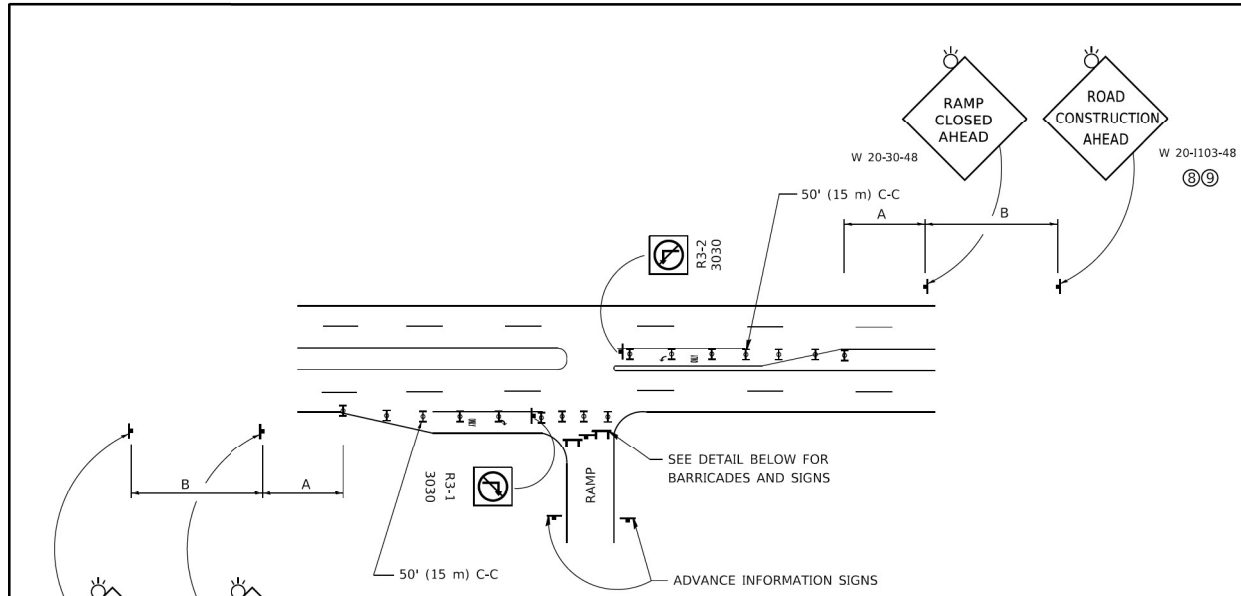
USER NAME = john.mccabe	DESIGNED - MAK	REVISIED -
	DRAWN - MAK	REVISIED -
PLOT SCALE = 20,0000 * / in.	CHECKED - HER	REVISIED -
PLOT DATE = 2/1/2022	DATE - 1/31/2022	REVISIED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**I-57 AT 175TH STREET
 PROPOSED CROSS SECTIONS**

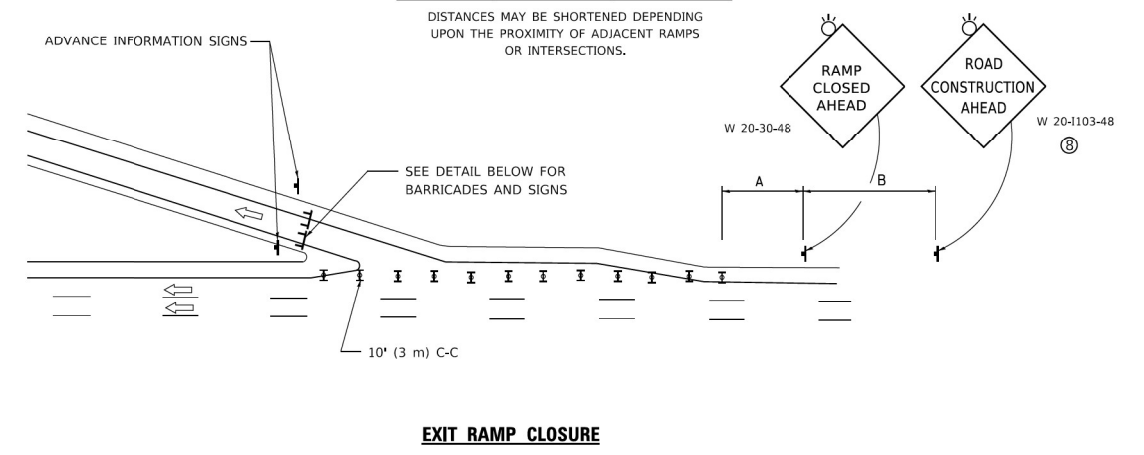
SCALE: 1:20 H 1:10 V SHEET 16 OF 16 SHEETS STA. 114+50.00 TO STA. 114+50.00

F.A.1 RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	78
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				



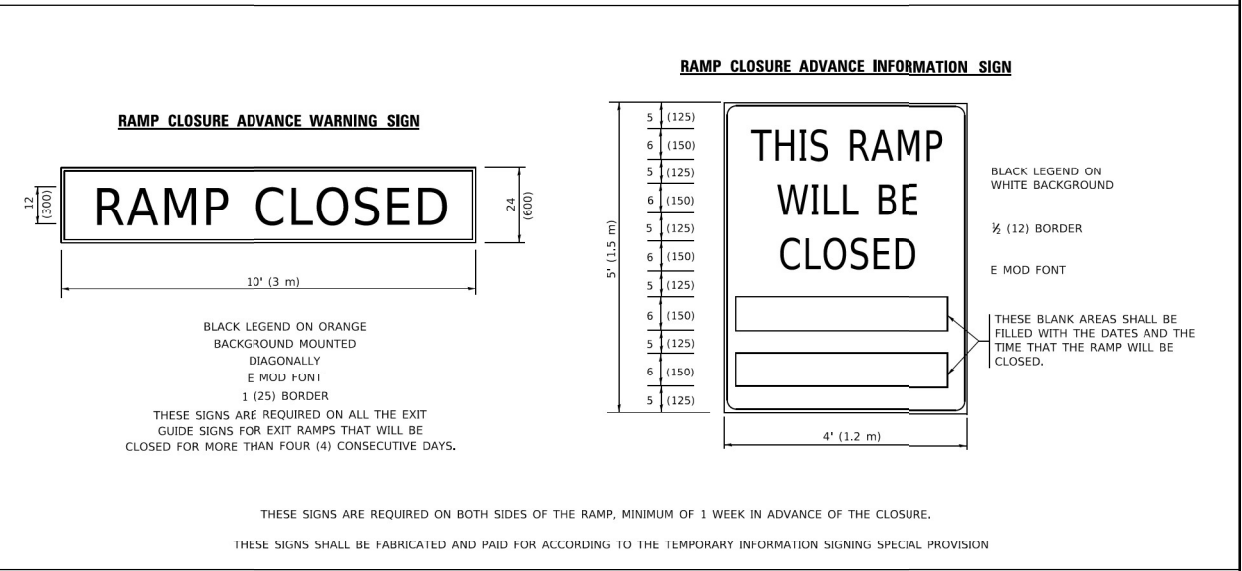
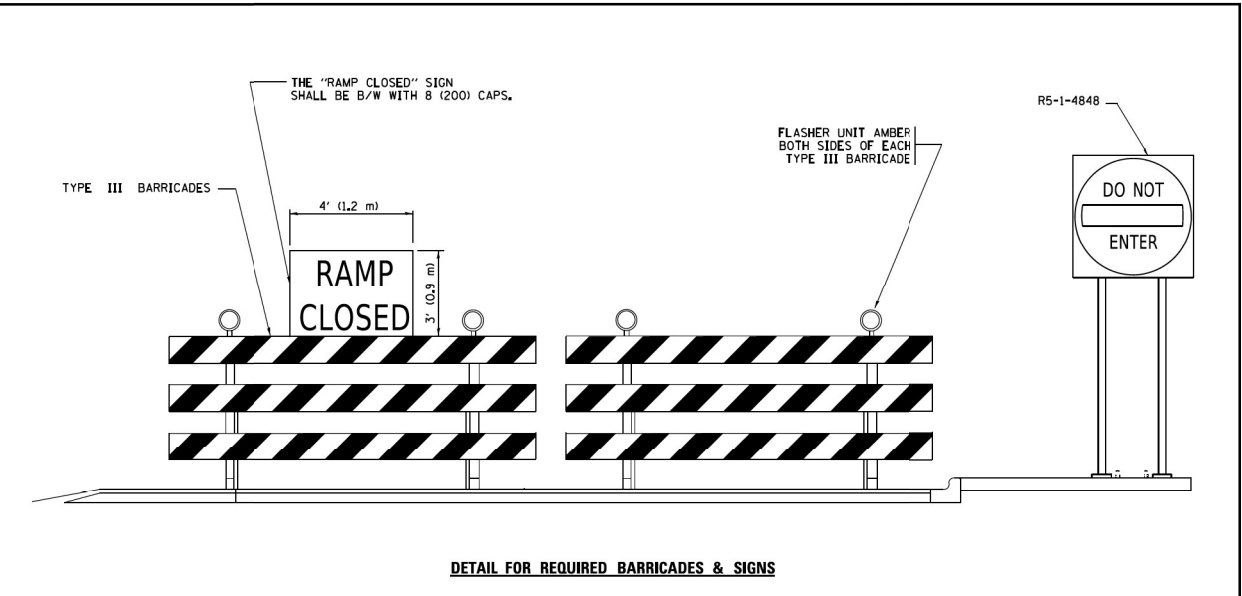
SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)



- SYMBOLS**
- I TYPE II BARRICADE OR DRUM
 - I TYPE III BARRICADE WITH 2 FLASHING LIGHTS

USER NAME = foortemj	DESIGNED - D.W.S.	REVISED - S.P.B._01-07
PLOT SCALE = 30,0000' / in.	CHECKED -	REVISED - S.P.B._12-09
PLOT DATE = 3/4/2019	DATE - 02-83	REVISED - M.D._06-13
		REVISED - M.D._01-18



GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS, CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS, ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = amalle_baymundo	DESIGNED - AMB	REVISED -
PLOT SCALE = 0.2000' / in.	CHECKED - MM	REVISED -
PLOT DATE = 2/1/2022	DATE - 01/31/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ENTRANCE AND EXIT RAMP CLOSURE DETAILS			
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS SHEET NO.
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	CONTRACT NO.

TC-08		ILLINOIS FED. AID PROJECT	
--------------	--	---------------------------	--

USER NAME = amalle_baymundo	DESIGNED - AMB	REVISED -
PLOT SCALE = 0.2000' / in.	CHECKED - MM	REVISED -
PLOT DATE = 2/1/2022	DATE - 01/31/2022	REVISED -

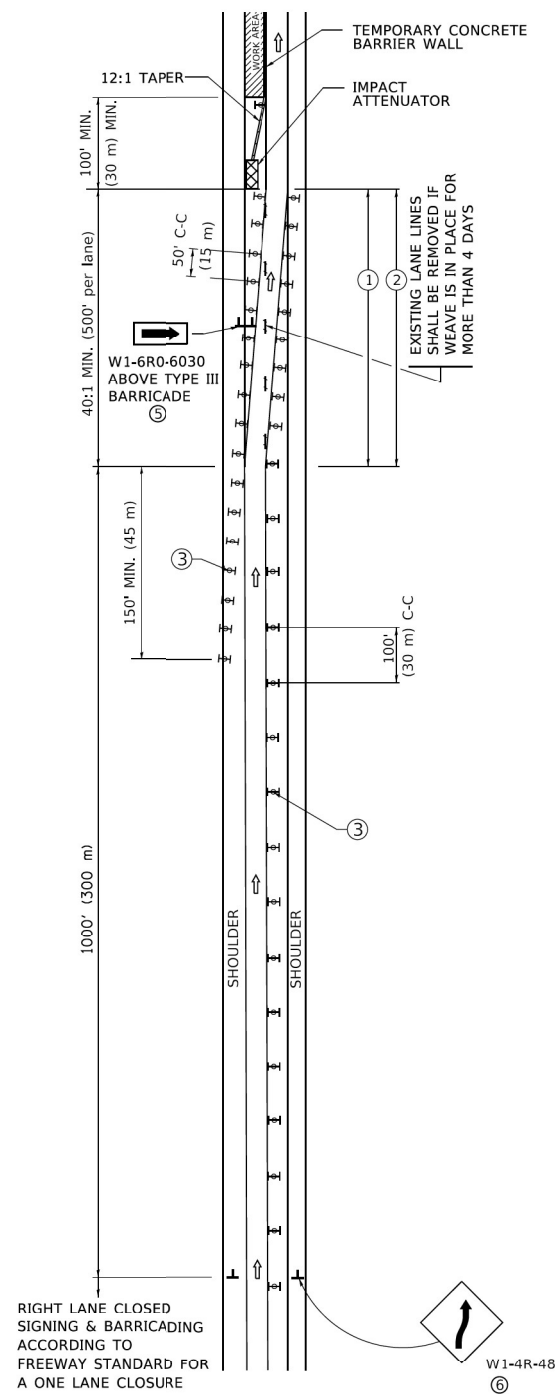
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ENTRANCE AND EXIT RAMP CLOSURE DETAILS			
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS SHEET NO.
SCALE:	SHEET 1 OF 11 SHEETS	STA. TO STA.	CONTRACT NO. 62P10

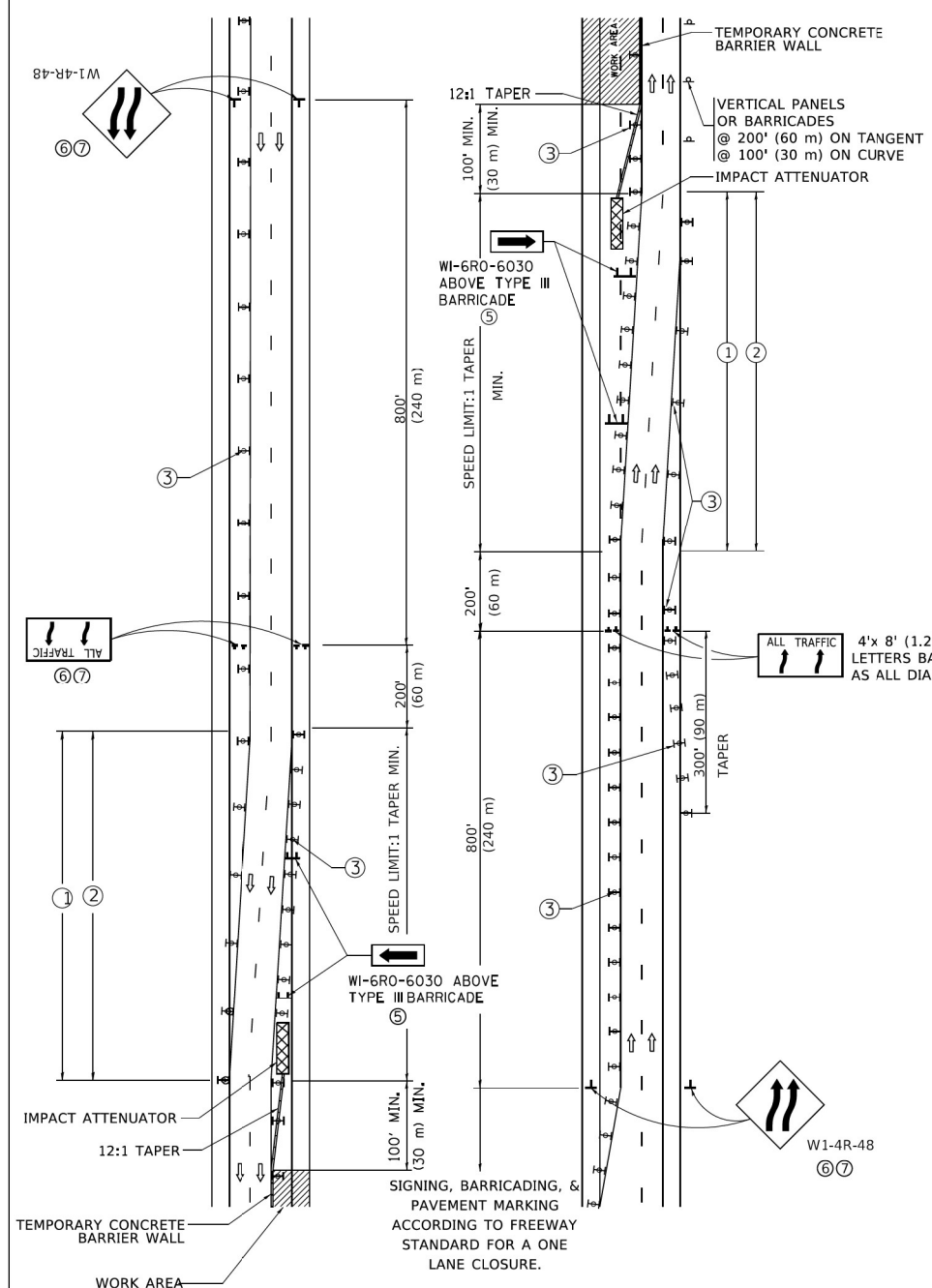
TC-08		ILLINOIS FED. AID PROJECT	
--------------	--	---------------------------	--

MODEL: Default
 FILE NAME: I:\Projects\1046166\BDR\Drawings\1046166_01.dwg
 MODEL: Default
 FILE NAME: I:\Projects\1046166\BDR\Drawings\1046166_01.dwg
 MODEL: Default
 FILE NAME: I:\Projects\1046166\BDR\Drawings\1046166_01.dwg
 MODEL: Default
 FILE NAME: I:\Projects\1046166\BDR\Drawings\1046166_01.dwg

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES:

- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W24-1-48
- W1-4R-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

USER NAME = footemj	DESIGNED - D.W.S.	REVISED - J.A.F. 02-06
DRAWN -	CHECKED -	REVISED - S.P.B. 01-07
PLOT SCALE = 30,0000' / in.	CHECKED -	REVISED - S.P.B. 12-09
PLOT DATE = 3/4/2019	DATE - 02-87	REVISED - M.D. 06-13

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAILS FOR
FREEWAY SINGLE & MULTI-LANE WEAVE**

SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			TC-09		CONTRACT NO.		
			ILLINOIS FED. AID PROJECT				

MODEL: Default
FILE NAME: p:\upward\htrb\comp\PIU\Grest_L\kels\Documents\Chicgo_Proj\cts\80677_IDOT_Phase_II_Various_Tss_Curbs\TOD-1_&_TOD-5_-_175th_Street_at_157th\CAD\55\Sheets\102918-ent-D1\trabandur_02.dgn
6/26/2019 10:24:46 AM User:folem



USER NAME = amalla.baymundo	DESIGNED - AMB	REVISED -
DRAWN - AMB	CHECKED - MM	REVISED -
PLOT SCALE = 0.2000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/1/2022	DATE - 01/31/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAILS FOR
FREEWAY SINGLE AND MULTI-LANE WEAVE**

SCALE:	SHEET 2 OF 11 SHEETS	STA. TO STA.	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			57		2021-073-BDR		
			COOK 89 80				
			CONTRACT NO. 62P10				
			ILLINOIS FED. AID PROJECT				

MODEL: Default
 FILE: \\nafe-01\p\updown\hnt-bhb\p\p\G\rest_L\k\k\Documents\Chicgo_Proj\cts\80677_IDOT_Phase_II_Various_Tss_Curbs\TD04-1 & TD05 - 175th_Street_at_157\CAD\DS\Sheets\102918-ent-D\Standard_03.dgn

MODEL: Default
 FILE: \\nafe-01\p\updown\hnt-bhb\p\p\G\rest_L\k\k\Documents\Chicgo_Proj\cts\80677_IDOT_Phase_II_Various_Tss_Curbs\TD04-1 & TD05 - 175th_Street_at_157\CAD\DS\Sheets\102918-ent-D\Standard_03.dgn

USER NAME = foatemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
DRAWN -	CHECKED -	REVISED - T. RAMMACHIER 01-06-00
PLOT SCALE = 30,0000 ' / in.	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
PLOT DATE = 3/4/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
 SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TC-10		CONTRACT NO.		
ILLINOIS		FED. AID PROJECT		

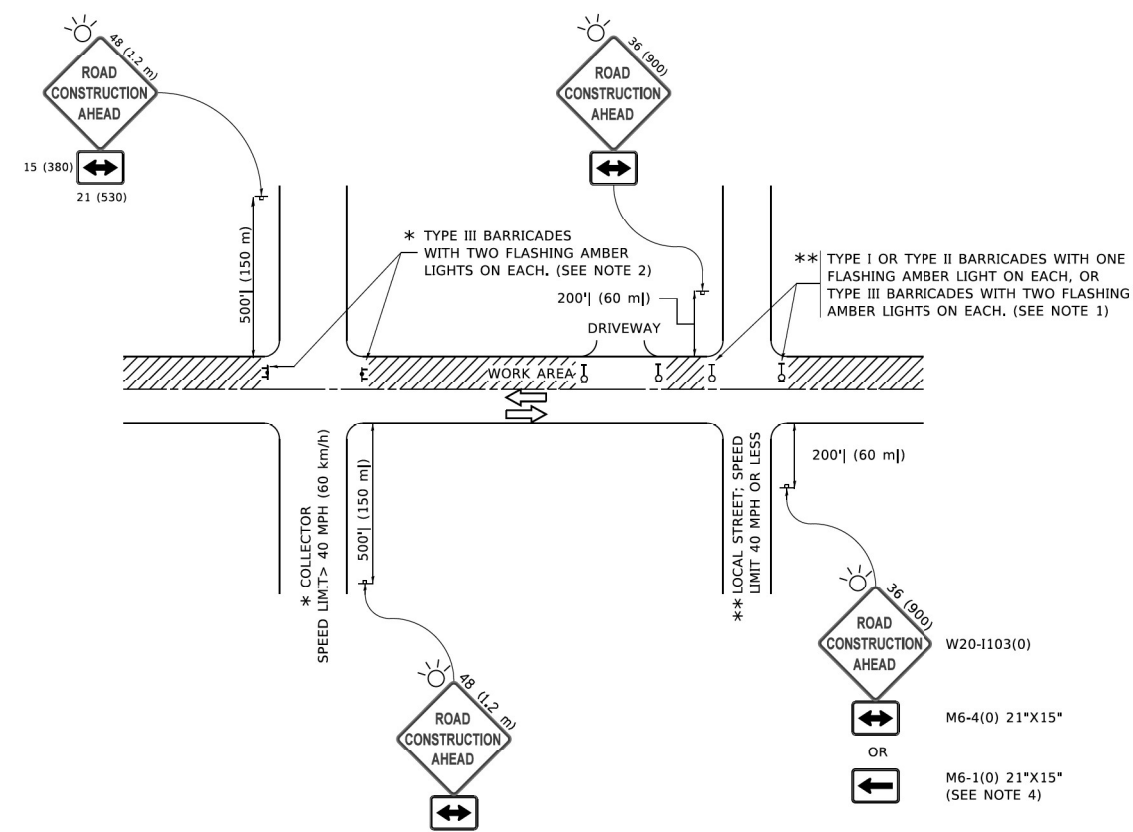
USER NAME = amalla,baymundo	DESIGNED - AMB	REVISED -
DRAWN - AMB	CHECKED - MM	REVISED -
PLOT SCALE = 0,2000 ' / in.	DATE - 01/31/2022	REVISED -
PLOT DATE = 2/1/2022	DATE - 01/31/2022	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
 SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: SHEET 3 OF 11 SHEETS STA. TO STA.

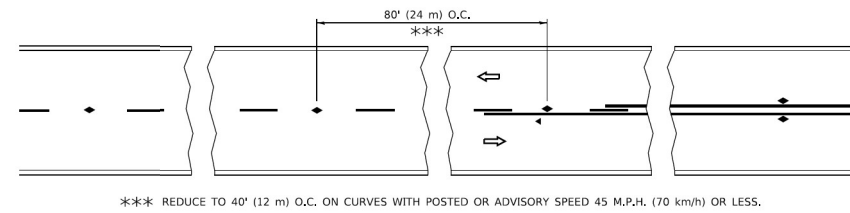
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	81
		CONTRACT NO. 62P10		
ILLINOIS		FED. AID PROJECT		



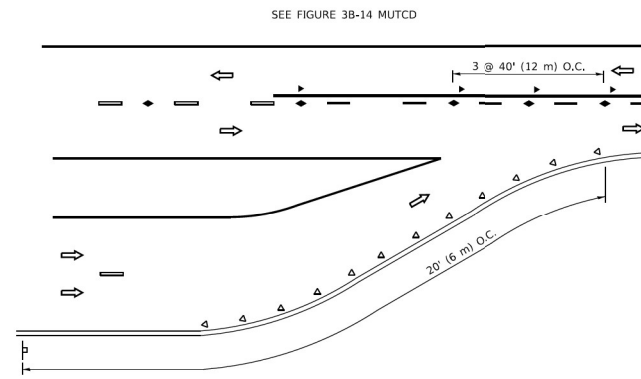
NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

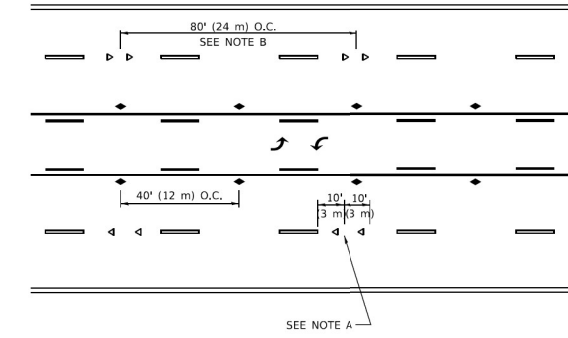
All dimensions are in inches (millimeters) unless otherwise shown.



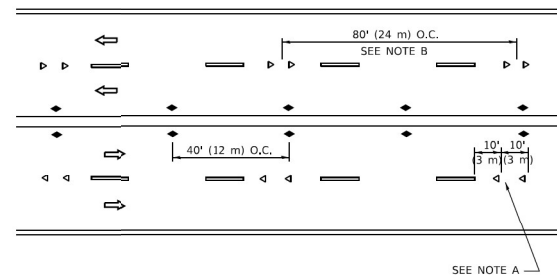
TWO-LANE/TWO-WAY



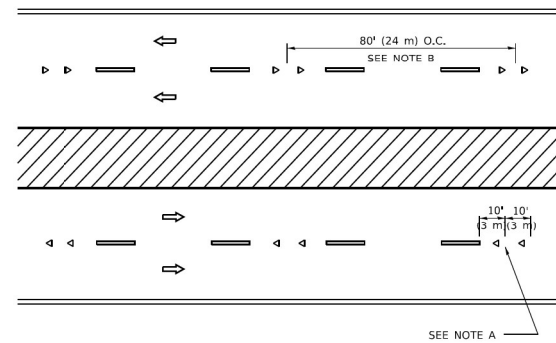
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

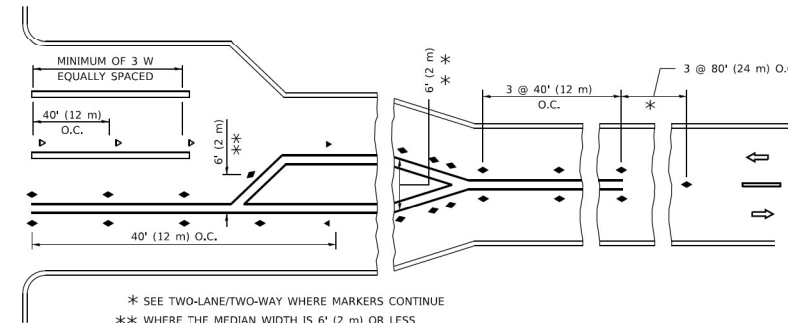
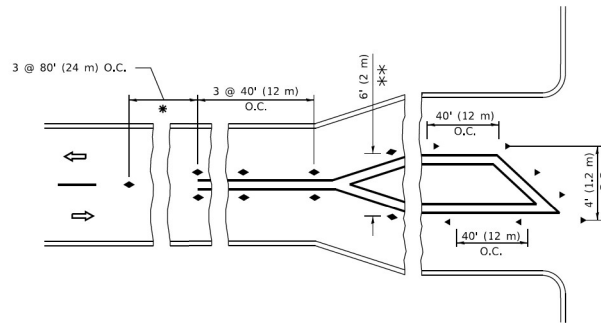
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



TURN LANES

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default FILE NAME: \\P:\work\p1\G\Garza\Projects\80677 IDOT Phase II Various Tack\Curbs\104-1_104-5_175th_Street_at_157\CAD\55\Sheets\102918-ent-D1\80677-04.dgn	USER NAME = amalla,baymundo	DESIGNED -	REVISED - T, RAMMACHER 03-12-99
	PLOT SCALE = 30,0000' / in.	DRAWN - AMB	REVISED - T, RAMMACHER 01-06-00
	PLOT DATE = 3/4/2019	CHECKED - MM	REVISED - C. JUCIUS 09-09-09
		DATE -	REVISED - C. JUCIUS 07-01-13

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.
-------------	---------------------	--------------

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TC-11				
ILLINOIS		FED. AID PROJECT		

USER NAME = amalla,baymundo	DESIGNED - AMB	REVISED -
PLOT SCALE = 0,2000' / in.	DRAWN - AMB	REVISED -
PLOT DATE = 2/1/2022	CHECKED - MM	REVISED -
	DATE - 01/31/2022	REVISED -

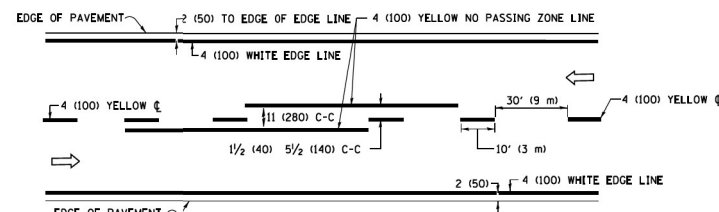
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

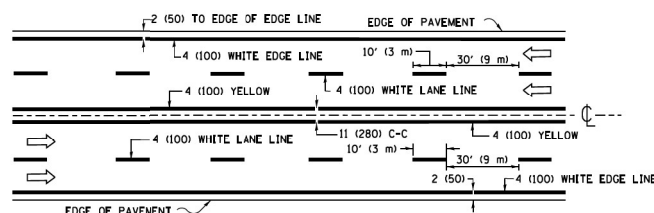
SCALE:	SHEET 4 OF 11 SHEETS	STA. TO STA.
--------	----------------------	--------------

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	82
ILLINOIS		FED. AID PROJECT		

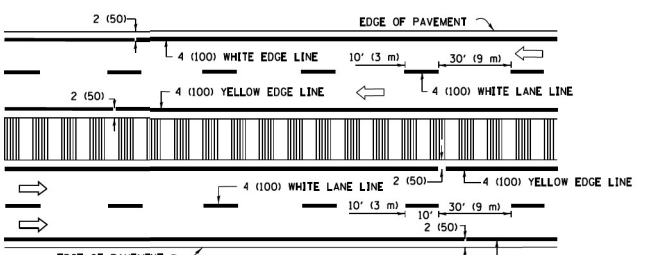




2-LANE ROADWAY

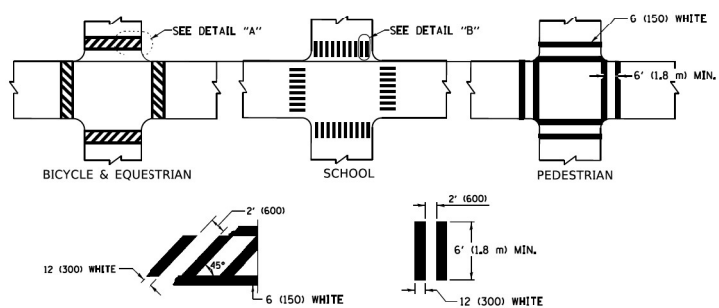


MULTI-LANE UNDIVIDED



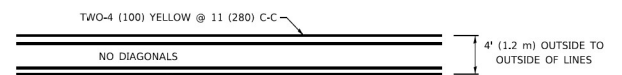
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

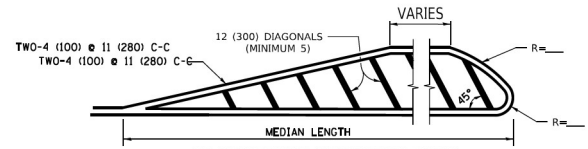


TYPICAL CROSSWALK MARKING

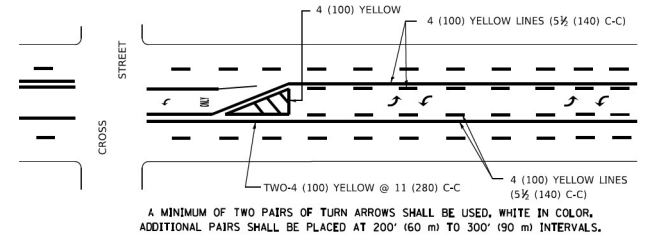
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



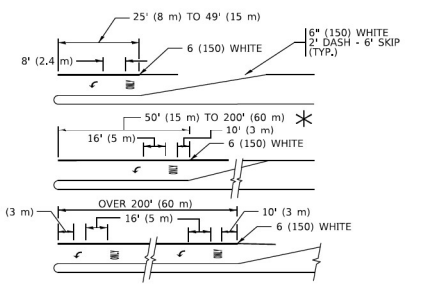
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

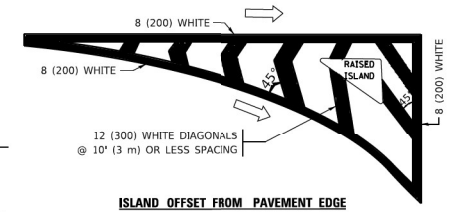


MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

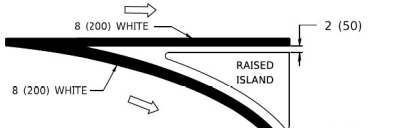


TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

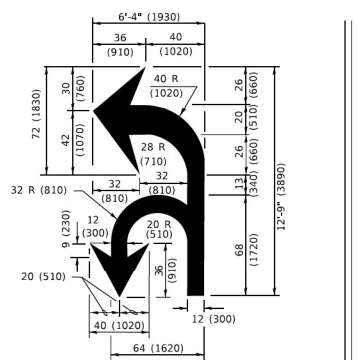


ISLAND OFFSET FROM PAVEMENT EDGE

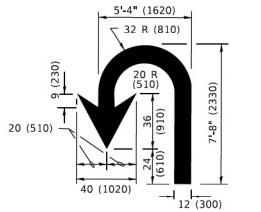


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30 MPH (50 km/h)) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m ²) EACH *X*=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h)) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footemj	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE TYPICAL PAVEMENT MARKINGS		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
DRAWN -	CHECKED -	REVISED - C. JUCIUS 07-01-13		SCALE: NONE	SHEET 1 OF 2 SHEETS	STA. TO STA.	TC-13 CONTRACT NO.				
PLOT SCALE = 30,0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15		ILLINOIS FED. AID PROJECT							
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16		ILLINOIS FED. AID PROJECT							

MODEL: Default
FILE NAME: p:\work\pavement\hob\pavement\Chicago Projects\80677 IDOT Phase II Various Task Order\04-1 & 10-45 - 175th Street at 157th\CAD\Drawings\23-36\CAD\Drawings\TC-13.dwg
1/1/2019 10:24:37 AM User:foote

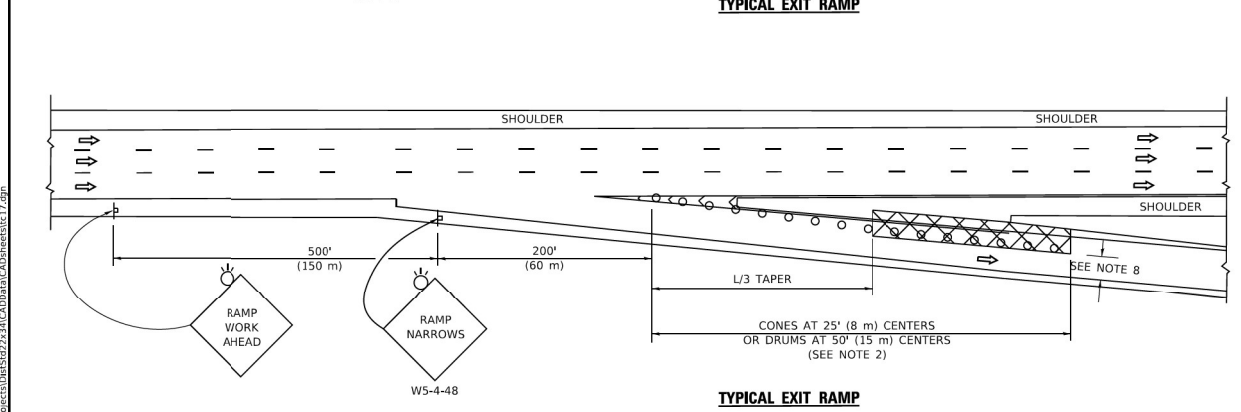
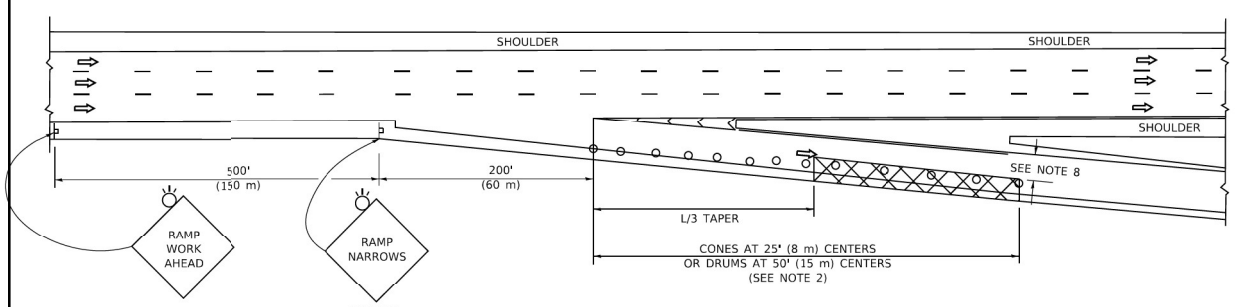
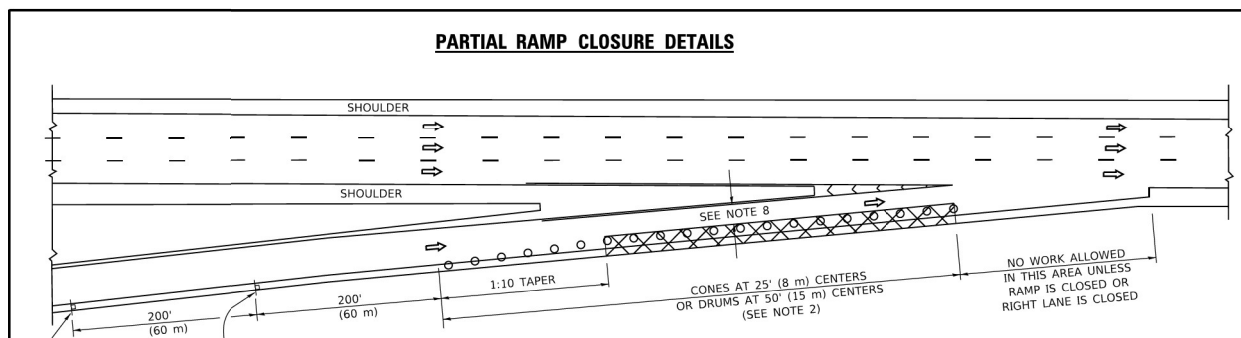


USER NAME = amalla.baymundo	DESIGNED - AMB	REVISED -
DRAWN - AMB	CHECKED - MM	REVISED -
PLOT SCALE = 0.2000' / in.	DATE - 01/31/2022	REVISED -
PLOT DATE = 2/1/2022	DATE - 01/31/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE TYPICAL PAVEMENT MARKINGS	
SCALE:	SHEET 5 OF 11 SHEETS STA. TO STA.

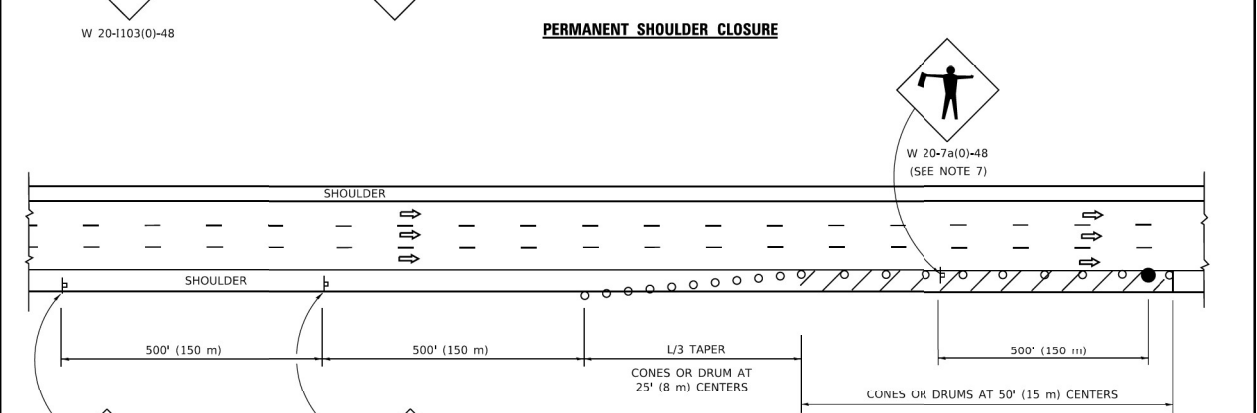
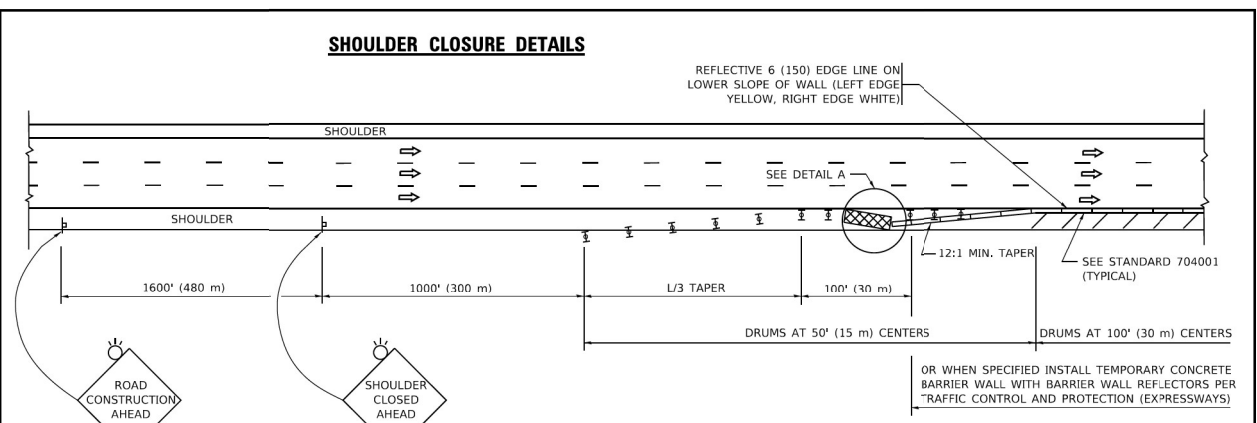
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	83
CONTRACT NO. 62P10				
ILLINOIS FED. AID PROJECT				



- SYMBOLS**
- ACTIVE WORK AREA
 - SIGN ON PORTABLE OR PERMANENT SUPPORT
 - FLAGGER WITH CONTROL SIGN
 - TYPE II BARRICADE OR DRUM
 - CONE, DRUM OR BARRICADE
 - IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

- GENERAL NOTES:**
- THE "L" DISTANCE EQUALS:

$$L = \frac{S^2}{300}$$
 WHERE:
 L = DISTANCE IN FEET (METERS)
 S = NORMAL POSTED SPEED MPH (KM/H)
 - TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30M) IN LENGTH AT NIGHT.
 - ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
 - FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.



- THIS DETAIL IS USED WHERE:
- VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCR OACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



USER NAME = footemj	DESIGNED -	REVISED -	S.P.B. 01-07
PLOT SCALE = 30,0000' / in.	DRAWN - D.W.S.	REVISED -	S.P.B. 12-09
PLOT DATE = 3/4/2019	CHECKED -	REVISED -	M.D. 06-13
	DATE - 11-96	REVISED -	M.D. 01-18

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAILS FOR FREEWAY
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-17			
ILLINOIS FED. AID PROJECT			CONTRACT NO.	

MODEL: Default
 FILE NAME: p:\projects\10464\10464.dwg
 PROJECT: 10464 - 175th Street at 157th Street
 DATE: 03/04/2019 10:41:41 AM
 USER: footemj

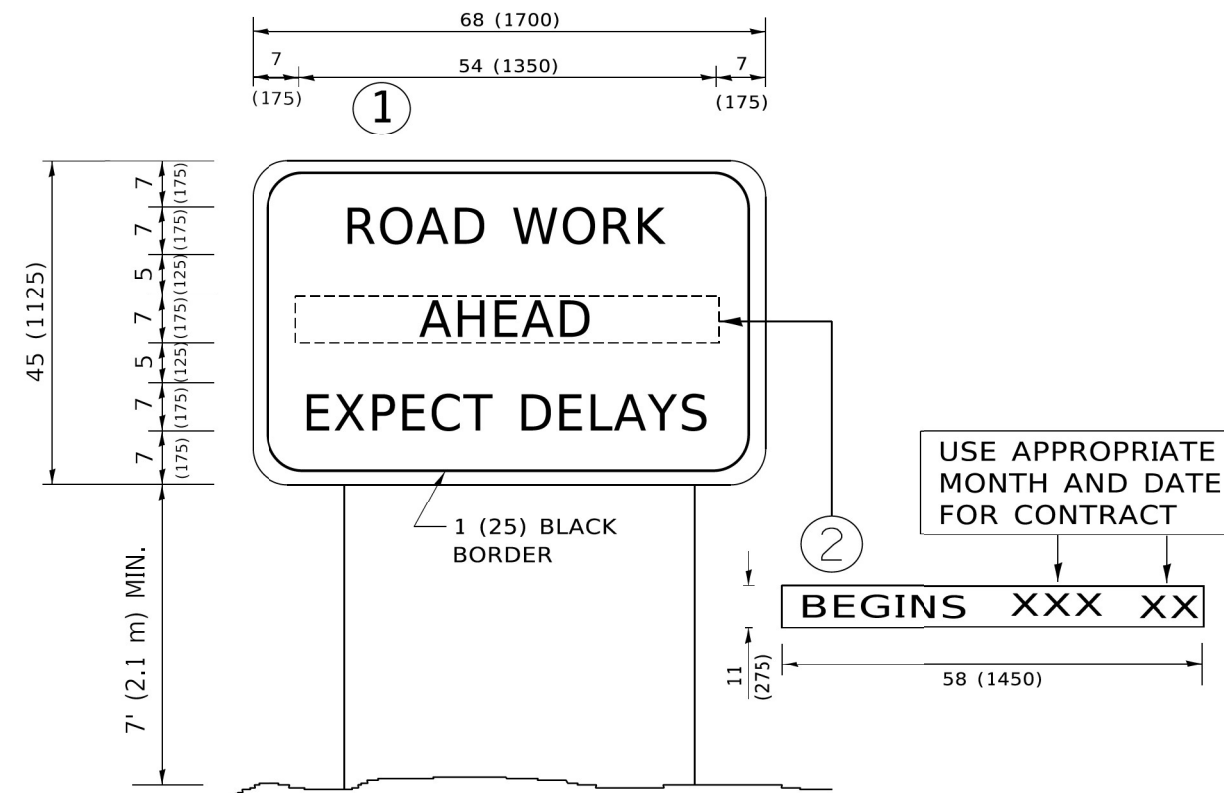
**garza karhoff
ENGINEERING, LLC**

USER NAME = amalla.baymundo	DESIGNED -	REVISED -	
PLOT SCALE = 0.2000' / in.	DRAWN - AMB	REVISED -	
PLOT DATE = 2/1/2022	CHECKED - MM	REVISED -	
	DATE - 01/31/2022	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAILS FOR FREEWAY
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	84
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P10	



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = footej	DESIGNED -	REVISED - R. MIRS 09-15-97
DRAWN -	CHECKED -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 30,0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07

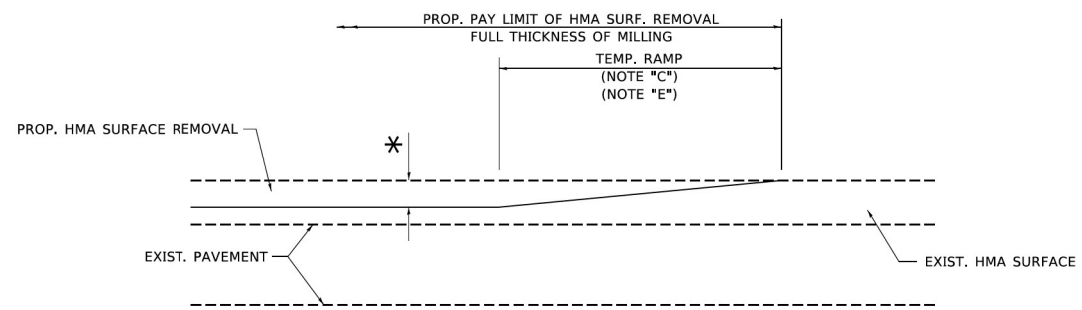
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		ARTERIAL ROAD INFORMATION SIGN				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.				TC-22 CONTRACT NO.				
						ILLINOIS FED. AID PROJECT				

USER NAME = amalla,baymundo	DESIGNED - AMB	REVISED -
DRAWN - AMB	CHECKED - MM	REVISED -
PLOT SCALE = 0,2000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/1/2022	DATE - 01/31/2022	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		ARTERIAL ROAD INFORMATION SIGN				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
						57	2021-073-BDR	COOK	89	85
		SCALE: SHEET 7 OF 11 SHEETS STA. TO STA.				CONTRACT NO. 62P10				
						ILLINOIS FED. AID PROJECT				

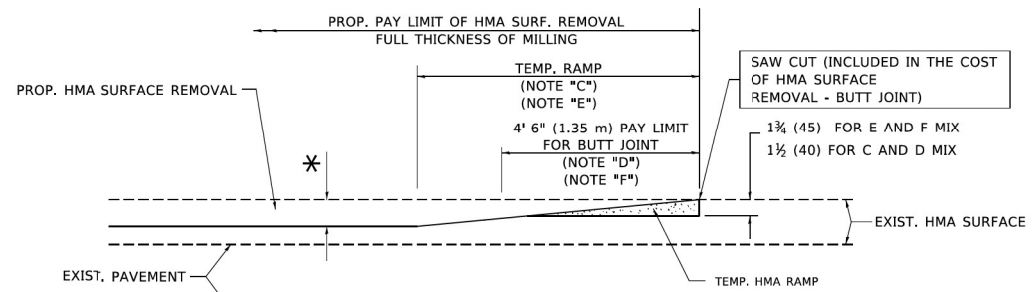
MODEL: Default
 FILE NAME: C:\Users\amallab\Documents\Chicago Projects\80677 IDOT Phase II Various Tack Orders\TOD-1 & TOD-5 - 175th Street at 157th\CAD\55\Sheets\022019-18-ent-D1\80677.dgn
 MODEL: Default
 FILE NAME: C:\Users\amallab\Documents\Chicago Projects\80677 IDOT Phase II Various Tack Orders\TOD-1 & TOD-5 - 175th Street at 157th\CAD\55\Sheets\022019-18-ent-D1\80677.dgn
 2/21/2022 10:46:15 AM User:footej





MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

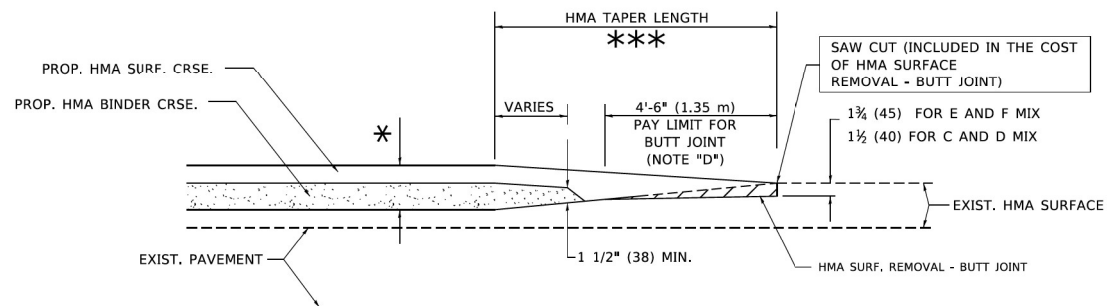
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

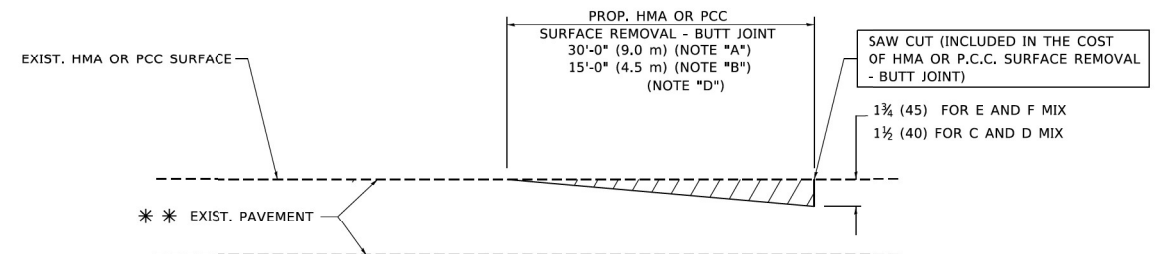
TYPICAL TEMPORARY RAMP



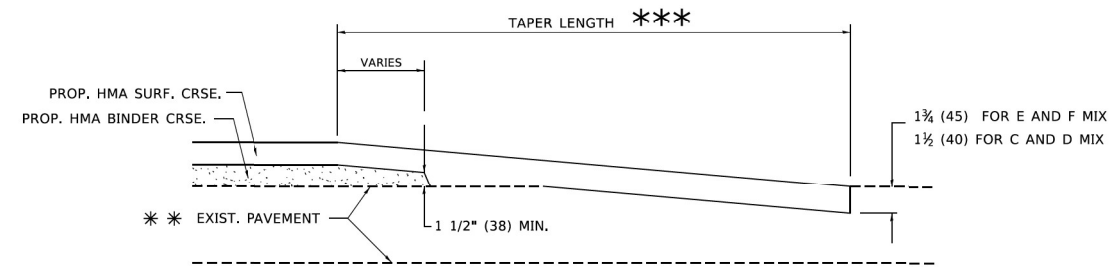
BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

MODEL: Default
FILE NAME: p:\work\p1\garza\1127\DOT\Documents\DOT_Offices\BDR\127\DOT\Garza\CAD\CAD\CAD\sheet\08.dgn
12/27/2019 8:46:47 AM User:sdm



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

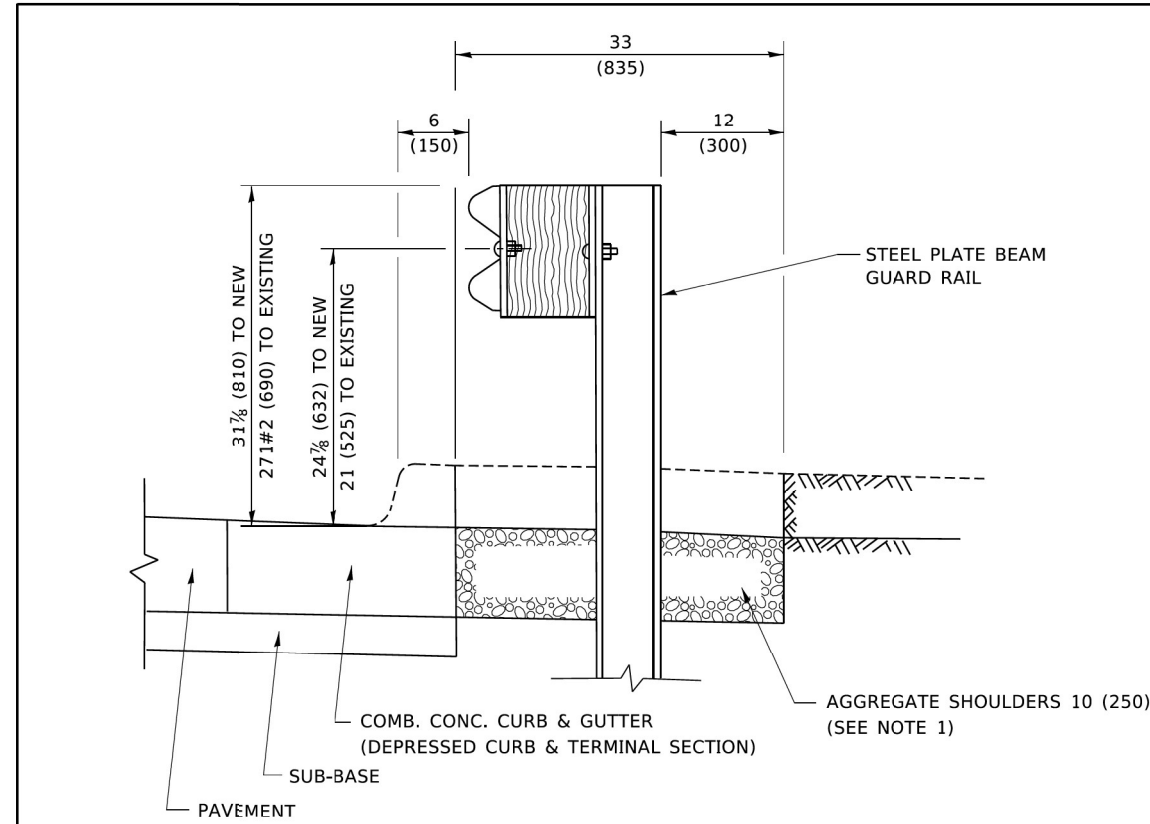
BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = footemj	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
	DRAWN -	REVISED - A. ADDAS 03-21-97
PLOT SCALE = 30,0000 ' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 3/27/2019	DATE - 06-13-90	REVISED - R.BORO 01-01-07

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		BUTT JOINT AND HMA TAPER DETAILS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
					SCALE: NONE	SHEET 1	OF 1	SHEETS	STA. TO STA.
					ILLINOIS		FED. AID PROJECT		

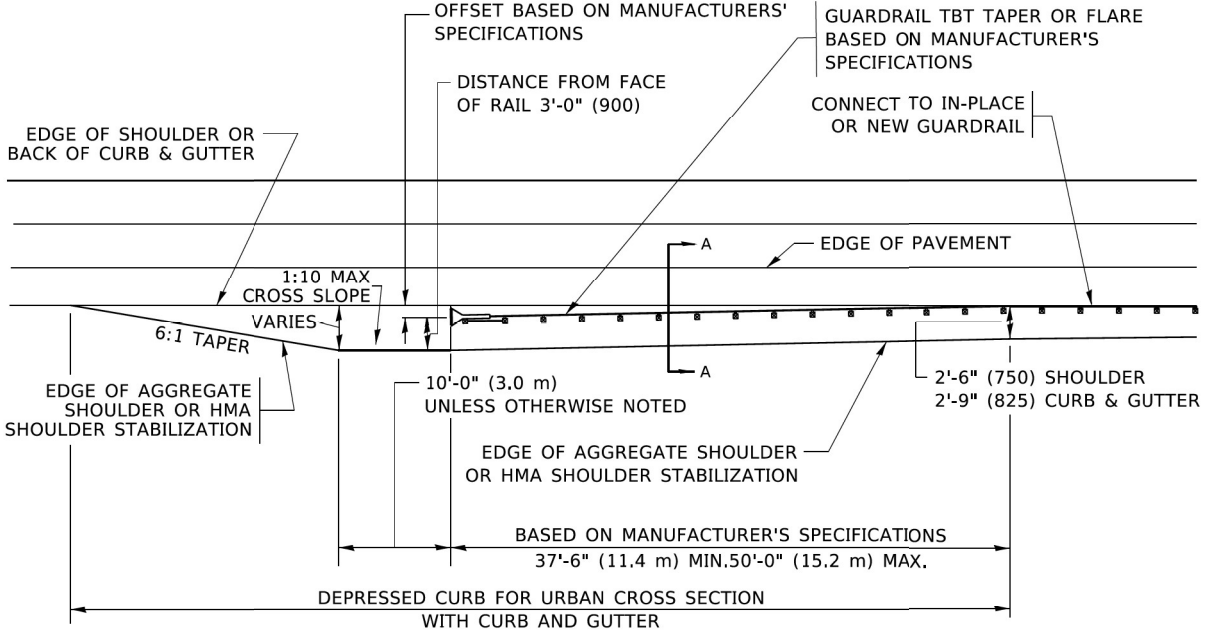


SECTION A-A

NOTES:

1. THE AGGREGATE SHOULDER, 10 (250) OR HMA SHOULDER, 6 (150) (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

**DETAILS FOR STEEL PLATE BEAM
GUARD RAIL ADJACENT TO CURB AND GUTTER
[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]**



**DEPRESSED CURB AND GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

AGGREGATE SHOULDER, 10 (250) WILL BE PAID ACCORDING TO SECTION 481.

HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID ACCORDING TO SECTION 482.

COMB. CONC. C&G, STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

USER NAME = footemj	DESIGNED - M. DE YONG	REVISED - R. BCRO 12-08-2008
DRAWN -	CHECKED -	REVISED - R. DCRO 09-14-2009
PLOT SCALE = 30,0000 ' / in.	DATE - 09-22-90	REVISED - R. BCRO 08-06-2012
PLOT DATE = 3/27/2019	DATE - 01-31-2022	REVISED - R. BCRO 05-08-2015

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR DEPRESSED CURB & GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
BD600-10 (BD 34)			89	87
CONTRACT NO.		ILLINOIS FED. AID PROJECT		

MODEL: Default
FILE NAME: p:\updown\htrb\comp\p1\Grest_L\kels\Documents\Chicgo_Proj\cts\80677_IDOT_Phase_II_Various_Task_Curbs\TOD11_1_TOD15_175th_Street_at_157th\CAD\Drawings\Sheet\102918-ent-D1\Bdradur_09.dgn



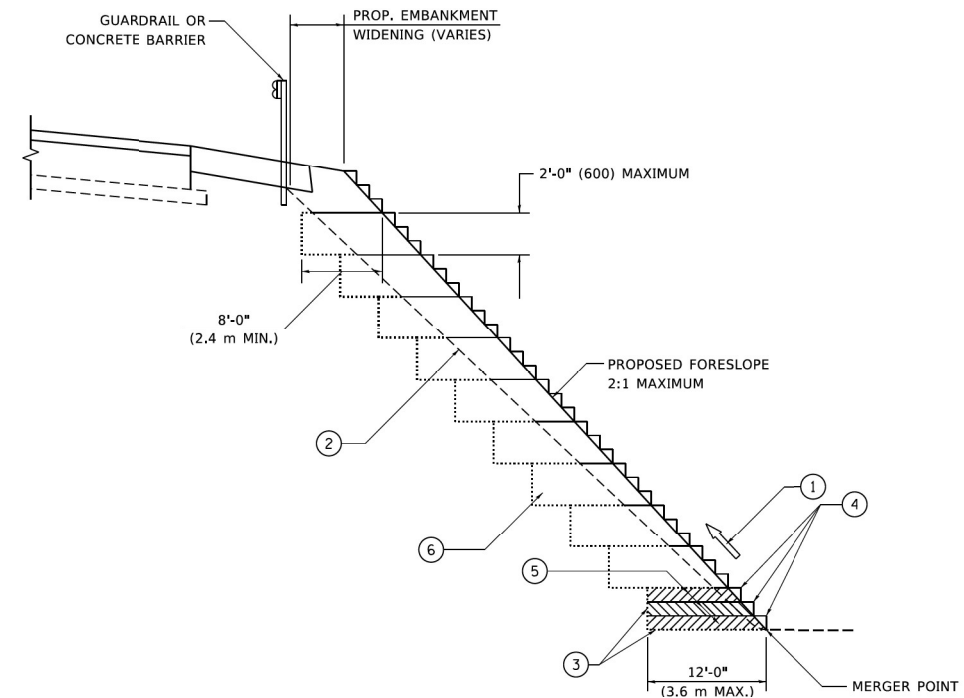
USER NAME = amalle.baymundo	DESIGNED - AMB	REVISED -
DRAWN - AMB	CHECKED - MM	REVISED -
PLOT SCALE = 0.2000 ' / in.	DATE - 01/31/2022	REVISED -
PLOT DATE = 2/1/2022		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR DEPRESSED CURB & GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

SCALE: SHEET 9 OF 11 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	87
CONTRACT NO.		ILLINOIS FED. AID PROJECT		



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABGR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

USER NAME = footeemj	DESIGNED -	REVISED -
PLOT SCALE = 30,0000 ' / in.	CHECKED - S.E.B.	REVISED -
PLOT DATE = 3/27/2019	DATE - 06-16-04	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BENCHING DETAIL
FOR EMBANKMENT WIDENING**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	BD-51			
ILLINOIS		FED. AID PROJECT		

MODEL: Default
FILE NAME: p:\updown\hntb\comp\p11\Grest_1\kels\Documents\Chicgo_Proj\cts\80677\DOT_Phase_II_Various_Tss_Curbs\TOD-1 & TD-05 - 175th_Street_at_157\CAD\55\Sheets\022918-ent-D1\Bambard_10.dgn
MODEL: Default
FILE NAME: p:\updown\hntb\comp\p11\Grest_1\kels\Documents\Chicgo_Proj\cts\80677\DOT_Phase_II_Various_Tss_Curbs\TOD-1 & TD-05 - 175th_Street_at_157\CAD\55\Sheets\022918-ent-D1\Bambard_10.dgn



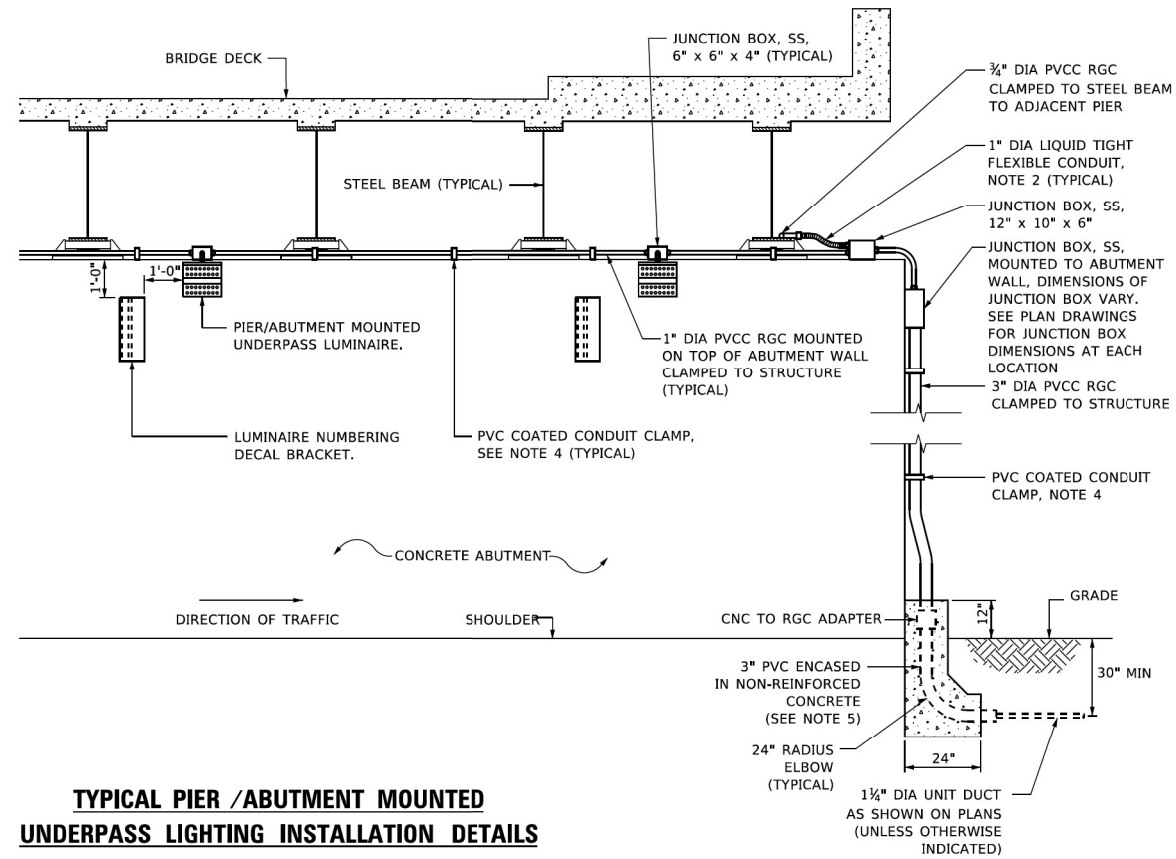
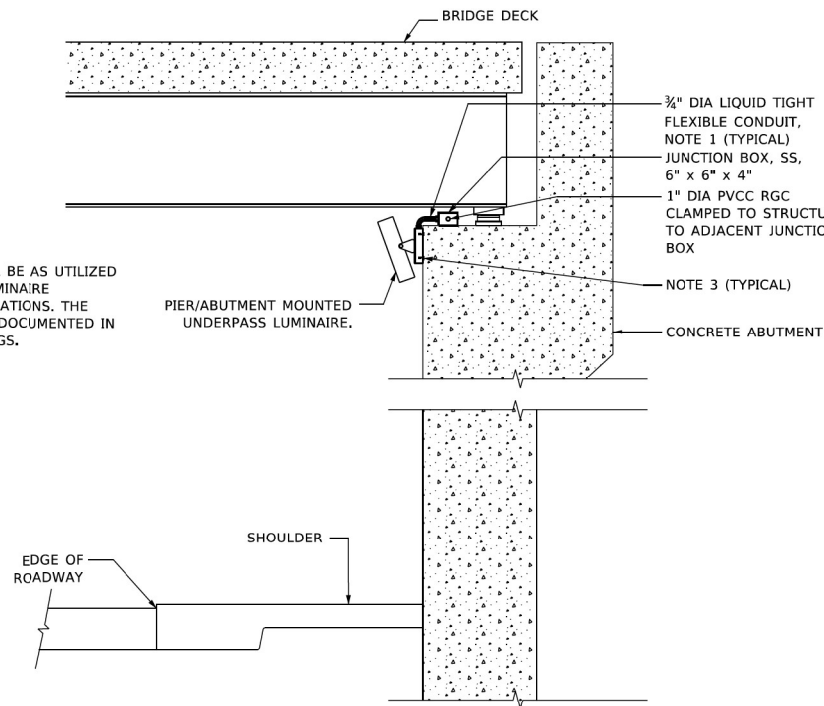
USER NAME = amalla,baymundo	DESIGNED - AMB	REVISED -
PLOT SCALE = 0,2000 ' / in.	CHECKED - MM	REVISED -
PLOT DATE = 2/1/2022	DATE - 01/31/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

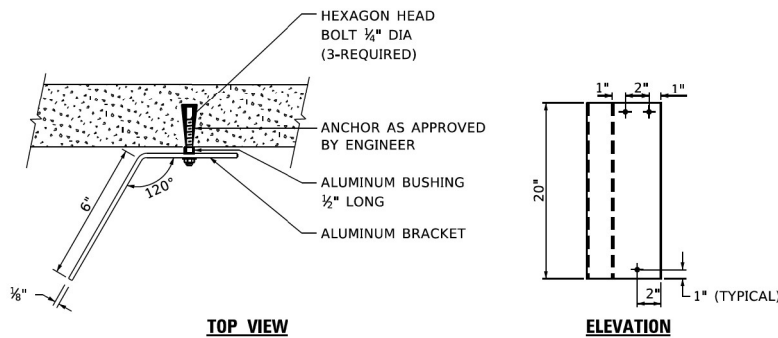
**BENCHING DETAIL
FOR EMBANKMENT WIDENING**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	88
ILLINOIS		FED. AID PROJECT		

NOTE:
LUMINAIRE TILT SHALL BE AS UTILIZED IN THE APPROVED LUMINAIRE PHOTOMETRIC CALCULATIONS. THE TILT ANGLE MUST BE DOCUMENTED IN THE RECORD DRAWINGS.

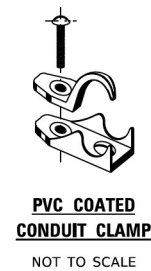


TYPICAL PIER /ABUTMENT MOUNTED UNDERPASS LIGHTING INSTALLATION DETAILS



LUMINAIRE NUMBERING DECAL BRACKET

NOT TO SCALE



NOTES:

- LIQUID TIGHT FLEXIBLE METAL CONDUIT, MAXIMUM LENGTH 6'-0", TYPICAL FOR EACH INSTANCE AS SHOWN. PROVIDE PVC COATED RIGID GALVANIZED STEEL CONDUIT AS REQUIRED NOT TO EXCEED 6'-0" OF FLEXIBLE LIQUID TIGHT METAL CONDUIT. LIQUID TIGHT FLEXIBLE METAL CONDUIT WILL BE INCLUDED IN THE COST OF THE CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED PAY ITEM EXCEPT THAT THE COST OF THE " DIA. RIGID STEEL CONDUIT AND " DIA. FLEXIBLE CONDUIT SHALL BE INCLUDED IN THE LUMINAIRE INSTALLATION.
- SEE UNDERPASS LIGHTING PLANS FOR INSTALLATION LOCATION OF UNDERPASS LIGHTING LUMINAIRES.
- UNDERPASS LUMINAIRE MOUNTED TO FACE OF PIER OR ABUTMENT WALL WITH 1/2" ALUMINUM SPACERS. MOUNTING HEIGHT OF 1" BELOW THE TOP OF PIER OR ABUTMENT WALL TYPICAL FOR ALL PIER/ABUTMENT MOUNTED UNDERPASS LUMINAIRES UNLESS OTHERWISE NOTED.
- EXPANSION ANCHOR, POWDER ACTUATED FASTENERS WILL NOT BE ALLOWED. EXPANSION ANCHOR MUST BE SIZED IN ACCORDANCE WITH MANUFACTURERS REQUIREMENTS.
- SECURE THE CONDUIT WITH PVC COATED CONDUIT CLAMPS OR CONDUIT BEAM CLAMPS AS SHOWN AT 5'-0" INTERVALS FOR LATERALS AND WITHIN 2'-3" MAXIMUM FROM ANY JUNCTION BOX, FLEXIBLE CONDUIT, OR CHANGE IN DIRECTION. ALL PVC COATED CONDUIT CLAMPS OR BEAM CLAMPS SHALL BE INCLUDED WITH THE COST OF THE "CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED" PAY ITEM.
- THE CONCRETE ENCASED CONDUIT TRANSITION SHALL BE INCLUDED IN THE COST OF THE GALVANIZED RIGID STEEL CONDUIT PAY ITEMS.
- ALL CONDUIT ATTACHED TO STRUCTURE SHALL BE PVC COATED RIGID STEEL CONDUIT (PVCC RGC) TYPICAL.

MODEL: D:\na\it... FILE NAME: ...

USER NAME = gajllanob	DESIGNED -	REVISED - 01-25-05
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 1/13/2020	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER /ABUTMENT MOUNTED LED UNDERPASS
LUMINAIRE INSTALLATION DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	BE-903			
ILLINOIS		FED. AID PROJECT		

MODEL: D:\na\it... FILE NAME: ...

**garza karhoff
ENGINEERING, LLC**

USER NAME = amalla,baymundo	DESIGNED - AMB	REVISED -
PLOT SCALE = 0,2000' / in.	DRAWN - AMB	REVISED -
PLOT DATE = 2/1/2022	CHECKED - MM	REVISED -
	DATE - 01/31/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER /ABUTMENT MOUNTED LED UNDERPASS
LUMINAIRE INSTALLATION DETAILS**

SCALE: SHEET 11 OF 11 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	2021-073-BDR	COOK	89	89
ILLINOIS		FED. AID PROJECT		