

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

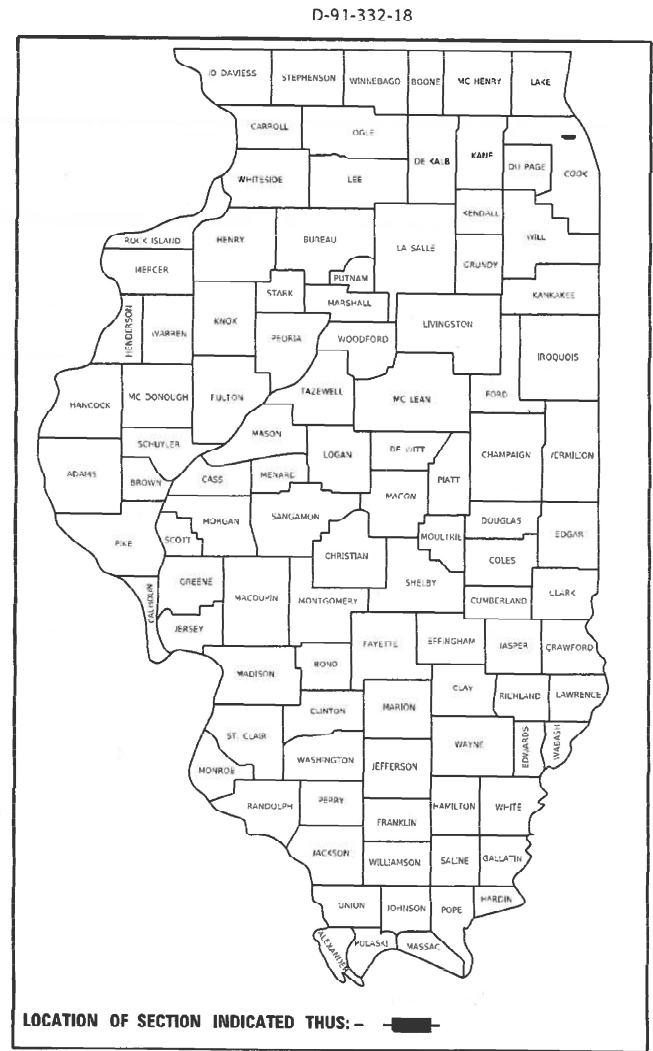
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------------|----------|--------------------|-----------|
| 1372 | 2018-019-RS-SW | COOK | 49 | 1 |
| | | ILLINOIS | CONTRACT NO. 62G71 | |

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE CITY OF CHICAGO

PROPOSED
HIGHWAY PLANS

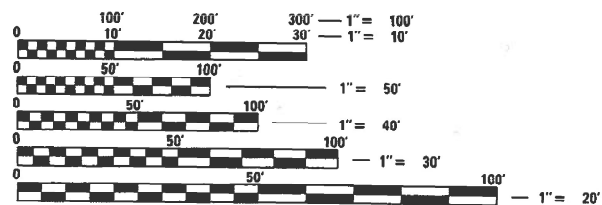
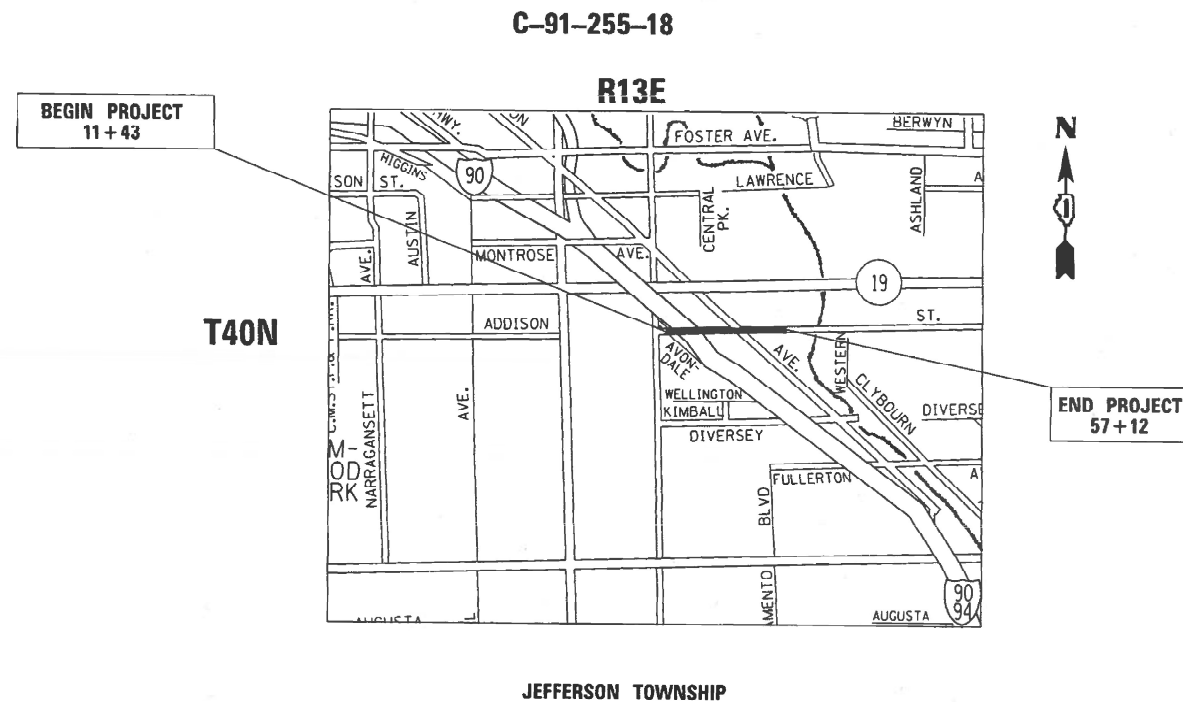
FAU ROUTE 1372: ADDISON ST
AVONDALE AVE TO SACRAMENTO AVE
SECTION 2018-019-RS-SW
PROJECT: STP-BBYL(553)
DESIGNED OVERLAY AND ADA IMPROVEMENTS
COOK COUNTY



TRAFFIC DATA

ADT: 26,100 (2018)

SPEED LIMIT: 30 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N.
CHICAGO UTILITY ALERT NETWORK
(312) 744-7000

PROJECT ENGINEER: LUKASZ POCIECHA (847) 705-4255
PROJECT MANAGER: FAWAD AQUEEL

CONTRACT NO. 62G71

GROSS LENGTH = 4569 FT. = 0.87 MILE
NET LENGTH = 4569 FT. = 0.87 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED January 28, 2022
Jose Rios REGIONAL ENGINEER

March 25, 2022
[Signature] ENGINEER OF DESIGN AND ENVIRONMENT

March 28, 2022
Stephen M. Smith DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

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| 48 | DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07) |
| 49 | P.C.C. CURB & GUTTER (CDOT) |

STATE HIGHWAY STANDARDS

| STANDARD NO. | DESCRIPTION |
|--------------|--|
| 000001-08 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 424001-11 | PERPENDICULAR CURB RAMPS FOR SIDEWALKS |
| 424011-04 | CORNER PARALLEL CURB RAMPS FOR SIDEWALKS |
| 424021-06 | DEPRESSED CORNER FOR SIDEWALKS |
| 424031-02 | MEDIAN PEDESTRIAN CROSSING |
| 442201-03 | CLASS C AND D PATCHES |
| 604001-05 | FRAME AND LIDS TYPE 1 |
| 604026-03 | FRAME AND GRATE TYPE 6 |
| 606001-08 | CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER |
| 701101-05 | OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE |
| 701427-05 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS =< 40 MPH |
| 701606-10 | URBAN SINGLE LANE CLOSURE MULTILANE 2W WITH MOUNTABLE MEDIAN |
| 701611-01 | URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN |
| 701701-10 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 701801-06 | SIDEWALK, CORNER OR CROSSWALK CLOSURE |
| 701901-08 | TRAFFIC CONTROL DEVICES |

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "C.U.A.N. "(CHICAGO UTILITY ALERT NETWORK) AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES. (48 HOUR NOTIFICATION REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- THE CONTRACTOR SHALL CONTACT THE IDOT ARTERIAL DISTRICT ONE TRAFFIC CONTROL SUPERVISOR KALPANA KANNAN-HOSADURGA AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
- UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAME AND LID UNLESS OTHERWISE SPECIFIED IN THE PLANS.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1V:3H.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE ENGINEER SHALL CONTACT ERIC CAMPOS, ARTERIAL TRAFFIC FIELD ENGINEER AT ERIC.CAMPOS@ILLINOIS.GOV A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.
- LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED IN THE PLANS)) WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- CONTACT THE IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST 2 WEEKS PRIOR TO BEGINNING WORK FOR LAYOUT.

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|-------------------------------|------------|-----------|---|--|---------|-------------|---------------------------|---------------|--------|-----------------|--------------|
| USER NAME = nullanecd | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES | | | F.A.U RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| DRAWN - | REVISED - | 1372 | | | | | 2018-019-RS-SW | COOK | 49 | 2 | |
| PLOT SCALE = 100,0000 ' / in. | CHECKED - | REVISED - | | CONTRACT NO. 62G71 | | | ILLINOIS FED. AID PROJECT | | | | |
| PLOT DATE = 2/7/2022 | DATE - | REVISED - | | SCALE: | SHEET 1 | OF 2 SHEETS | STA. 11+43 | TO STA. 57+12 | | | |

GENERAL NOTES (CONTINUED..)

- 22. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- 23. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- 24. SIDEWALK REMOVAL AND P.C.C. SIDEWALK 5" LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.
- 25. CURB RAMPS AND LANDING (KEystone) TO BE CONSTRUCTED WITH 8" THICK CONCRETE AT ALL TRAFFIC SIGNALIZED INTERSECTIONS AND INDUSTRIALIZED STREET INTERSECTIONS. AT ALL OTHER LOCATIONS, 5" THICK CONCRETE IS TO BE USED.
- 26. WHERE SECTION OR SUB-SECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED THEIR LOCATION.
- 27. THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.
- 28. ALL CATCH BASINS IN THE CITY OF CHICAGO MUST MEET THE DEPARTMENT OF SEWERS' SANDARDS.
- 29. PERMITS FROM THE DEPARTMENT OF SEWERS ARE REQUIRED FOR ALL UNDERGROUND STORM, SANITARY OR COMBINED SEWER SYSTEM CONSTRUCTION, AND FOR RESURFACING WORK INVOLVING ADJUSTMENT OF SEWER STRUCTURES. THE DEPARTMENT OF SEWERS' ERMIT MUST BE OBTAINED BY A LICENSED SEWER DRAIN LAYER PRIOR TO START OF ONSTRUCTION.
- 30. THE LICENSED SEWER CONTRACTOR/SUB-CONTRACTOR MUST SUBMIT TWO (2) SETS OF PLANS APPROVED BY THE DEPARTMENT OF SEWERS FOR THE ISSUE OF THE SEWER PERMIT IN SUITE 410 - 333 SOUTH STATE STREET, CHICAGO, IL 60604. INSPECTION WILL BE PROVIDED BY THE DEPARTMENT OF SEWERS
- 31. PERFORATED LIDS SHALL BE PLACED ON ALL MANHOLES AND CATCH BASINS
- 32. SIDEWALK HANDICAPPED RAMPS SHALL NOT BE CONSTRUCTED DIRECTLY OVER EXISTING OR PROPOSED DRAINAGE STRUCTURES.
- 33. ALL BROKEN, CRACKED, WORN OR OTHERWISE DAMAGED OR BICYCLE UNSAFE FRAMES AND LIDS ON SEWER STRUCTURES, SHALL BE REPLACED WITH NEW DEPARTMENT OF SEWERS' STANDARD FRAMES AND LIDS.
- 34. OPEN LID DRAINAGE STRUCTURES SHALL NOT BE CLOSED, COVERED OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION OF THIS ROADWAY WITHOUT THE WRITTEN PERMISSION FROM THE CITY OF CHICAGO.
- 35. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY OF CHICAGO AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- 36. THE AGGREGATE GRADATION FOR THE AGGREGATE SUBGRADE IMPROVEMENT 12" LOWER LIFT SHALL BE CS 1 OR RR 1.

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| | DRAWN - | REVISED - |
| PLOT SCALE = 100,0000 ' / in. | CHECKED - | REVISED - |
| PLOT DATE = 2/7/2022 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, STATE STANDARDS,
AND GENERAL NOTES**

SCALE: SHEET 2 OF 2 SHEETS STA. 11+43 TO STA. 57+12

| F.A.U RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------|----------------|----------|--------------------|--------------|
| 1372 | 2018-019-RS-SW | COOK | 49 | 3 |
| | | | CONTRACT NO. 62G71 | |
| | | ILLINOIS | FED. AID PROJECT | |

| SUMMARY OF QUANTITIES | | | CONSTRUCTION TYPE CODE | | | | | | |
|-----------------------|---|-------|------------------------|-------------------|------------|--|--|--|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES URBAN | 0005 | 0005 | | | | |
| | | | | 80% FED 20% STATE | 100% STATE | | | | |
| 20200100 | EARTH EXCAVATION | CU YD | 75 | 75 | | | | | |
| 21101615 | TOPSOIL FURNISH AND PLACE, 4" | SO YD | 107 | 107 | | | | | |
| 25200110 | SODDING, SALT TOLERANT | SO YD | 107 | 107 | | | | | |
| 25200200 | SUPPLEMENTAL WATERING | UNITS | 2 | 2 | | | | | |
| 30300112 | AGGREGATE SUBGRADE IMPROVEMENT 12" | SO YD | 120 | 120 | | | | | |
| 40600290 | BITUMINOUS MATERIALS (TACK COAT) | POUND | 15392 | 15392 | | | | | |
| 40600370 | LONGITUDINAL JOINT SEALANT | FOOT | 13503 | 13503 | | | | | |
| 40600400 | MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS | TON | 35 | 35 | | | | | |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SO YD | 626 | 626 | | | | | |
| 40602985 | HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 | TON | 2509 | 2509 | | | | | |
| 40604172 | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE IL-9.5, MIX "E", N70 | TON | 2235 | 2235 | | | | | |
| 42000501 | PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED) | SO YD | 89 | 89 | | | | | |

| SUMMARY OF QUANTITIES | | | CONSTRUCTION TYPE CODE | | | | | | |
|-----------------------|--|-------|------------------------|-------------------|------------|--|--|--|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES URBAN | 0005 | 0005 | | | | |
| | | | | 80% FED 20% STATE | 100% STATE | | | | |
| 42001300 | PROTECTIVE COAT | SO YD | 1383 | 1383 | | | | | |
| 42300200 | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH | SO YD | 25 | 25 | | | | | |
| 42300400 | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH | SO YD | 35 | 35 | | | | | |
| 42400200 | PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH | SO FT | 376 | 376 | | | | | |
| 42400410 | PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH | SO FT | 2689 | 2689 | | | | | |
| 44000100 | PAVEMENT REMOVAL | SO YD | 218 | 218 | | | | | |
| 44000164 | HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4" | SO YD | 22466 | 22466 | | | | | |
| 44000200 | DRIVEWAY PAVEMENT REMOVAL | SO YD | 60 | 60 | | | | | |
| 44000500 | COMBINATION CURB AND GUTTER REMOVAL | FOOT | 167 | 167 | | | | | |
| 44000600 | SIDEWALK REMOVAL | SO FT | 2471 | 2471 | | | | | |
| 44201773 | CLASS D PATCHES, TYPE I, 11 INCH | SO YD | 15 | 15 | | | | | |
| | * SPECIALTY ITEMS | | | | | | | | |

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADDISON ST (AVONDALE - SACRAMENTO AVE)
SUMMARY OF QUANTITIES**

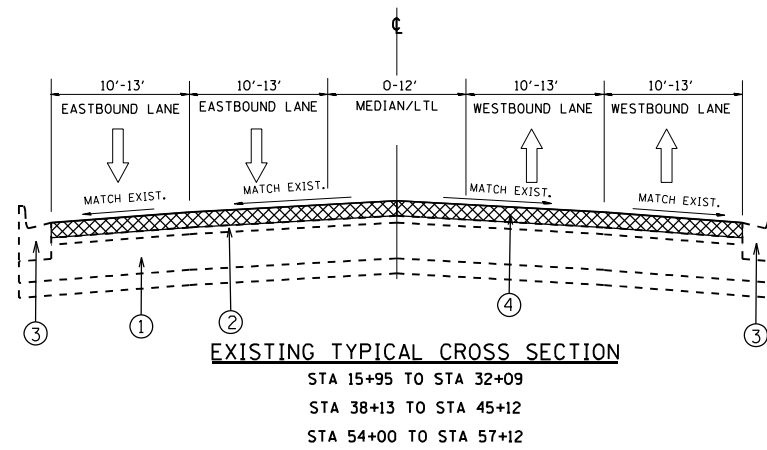
SCALE: SHEET NO. 1 OF 4 SHEETS STA. 11+43 TO STA. 57+12

| | | | | |
|---|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1372 | 2018-019-RS-SW | COOK | 49 | 4 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 62G71 | |

| SUMMARY OF QUANTITIES | | | CONSTRUCTION TYPE CODE | | | | | | |
|-----------------------|--|-------|------------------------|-------------------|------------|--|--|--|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES URBAN | 0005 | 0005 | | | | |
| | | | | 80% FED 20% STATE | 100% STATE | | | | |
| 44201777 | CLASS D PATCHES, TYPE II, 11 INCH | SO YD | 495 | 495 | | | | | |
| 44201781 | CLASS D PATCHES, TYPE III, 11 INCH | SO YD | 260 | 260 | | | | | |
| 44201783 | CLASS D PATCHES, TYPE IV, 11 INCH | SO YD | 120 | 120 | | | | | |
| 60252800 | CATCH BASINS TO BE RECONSTRUCTED | EACH | 5 | 5 | | | | | |
| 60300105 | FRAMES AND GRATES TO BE ADJUSTED | EACH | 4 | 4 | | | | | |
| 60300305 | FRAMES AND LIDS TO BE ADJUSTED | EACH | 22 | 22 | | | | | |
| 60404500 | FRAMES AND GRATES, TYPE 6 | EACH | 2 | 2 | | | | | |
| 60406000 | FRAMES AND LIDS, TYPE 1, OPEN LID | EACH | 7 | 7 | | | | | |
| 60406530 | FRAMES AND LIDS, CLOSED LID (CITY OF CHICAGO) | EACH | 19 | 19 | | | | | |
| 66900200 | NON-SPECIAL WASTE DISPOSAL | CU YD | 75 | 75 | | | | | |
| 66900530 | SOIL-DISPOSAL ANALYSIS | EACH | 3 | 3 | | | | | |
| 66901001 | REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN | L SUM | 1 | 1 | | | | | |
| 66901003 | REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT | L SUM | 1 | 1 | | | | | |

| SUMMARY OF QUANTITIES | | | CONSTRUCTION TYPE CODE | | | | | | |
|-----------------------|---|--------|------------------------|-------------------|------------|--|--|--|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES URBAN | 0005 | 0005 | | | | |
| | | | | 80% FED 20% STATE | 100% STATE | | | | |
| 66901006 | REGULATED SUBSTANCES MONITORING | CAL DA | 14 | 14 | | | | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | | | | | |
| 70102625 | TRAFFIC CONTROL AND PROTECTION STANDARD 701606 | L SUM | 1 | 1 | | | | | |
| 70102634 | TRAFFIC CONTROL AND PROTECTION STANDARD 701611 | L SUM | 1 | 1 | | | | | |
| 70102635 | TRAFFIC CONTROL AND PROTECTION STANDARD 701701 | L SUM | 1 | 1 | | | | | |
| 70102640 | TRAFFIC CONTROL AND PROTECTION STANDARD 701801 | L SUM | 1 | 1 | | | | | |
| 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 22191 | 22191 | | | | | |
| 70300150 | SHORT TERM PAVEMENT MARKING REMOVAL | SO FT | 9246 | 9246 | | | | | |
| 70300211 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS, PAINT | SO FT | 403.7 | 403.7 | | | | | |
| 70300221 | TEMPORARY PAVEMENT MARKING - LINE 4" PAINT | FOOT | 5332 | 5332 | | | | | |
| 70300241 | TEMPORARY PAVEMENT MARKING - LINE 6" PAINT | FOOT | 834 | 834 | | | | | |
| | SPECIALTY ITEMS | | | | | | | | |

ADDISON ST



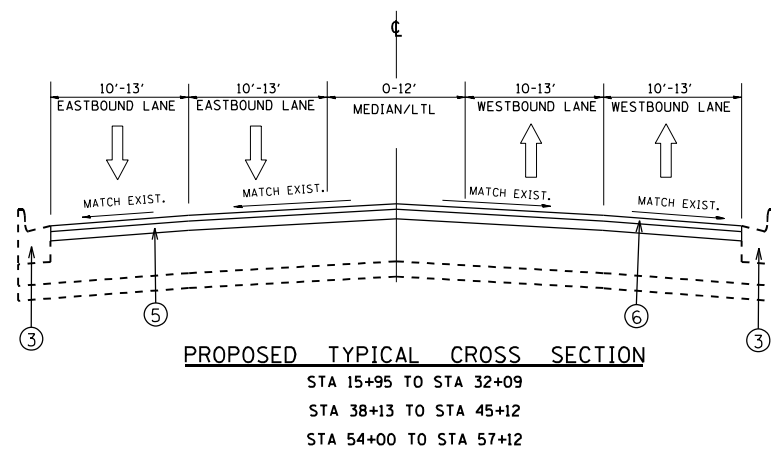
LEGEND

- ① EXISTING PCC BASE COURSE, ±8"
- ② EXISTING HMA PAVEMENT ±7"
- ③ EXISTING COMB. CONCRETE CURB AND GUTTER
- ④ PROPOSED HMA SURFACE REMOVAL, 3 3/4"
- ⑤ PROPOSED HMA BINDER COURSE, IL-9.5, N70; 2"
- ⑥ PROPOSED POLYMERIZED HMA SURFACE COURSE, IL-9.5, MIX E, N70 1 3/4"

NOTES

1. THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.
2. LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE HMA BINDER COURSE

ADDISON ST



| HOT-MIX ASPHALT MIXTURE REQUIREMENTS | | | |
|--------------------------------------|--|------------------|----------------------------------|
| MIXTURE USES | MIXTURE TYPE | AIR VOIDS @ Ndes | QUALITY MANAGEMENT PROGRAM (QMP) |
| PAVEMENT RESURFACING | HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70; 2" | 4.0% @ 70 GYR | OCP |
| | POLYMERIZED HMA SURFACE COURSE, IL-9.5, MIX E, N70; 1 3/4" | 4.0% @ 70 GYR | OCP |
| PATCHING | CLASS D PATCHES (HMA BINDER IL-19.0 mm) | 4.0% @ 70 GYR | QC/OA |
| TEMPORARY RAMP, SPECIAL | HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70; VARIABLE DEPTH | 4.0% @ 70 GYR | QC/OA |

QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (OCP)

NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

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 USER: mullanecd
 DATE: 2/7/2022

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| USER NAME = mullanecd | DESIGNED - | REVISED - |
| PLOT SCALE = 100,0000' / in. | DRAWN - | REVISED - |
| PLOT DATE = 2/7/2022 | CHECKED - | REVISED - |
| | DATE - | REVISED - |

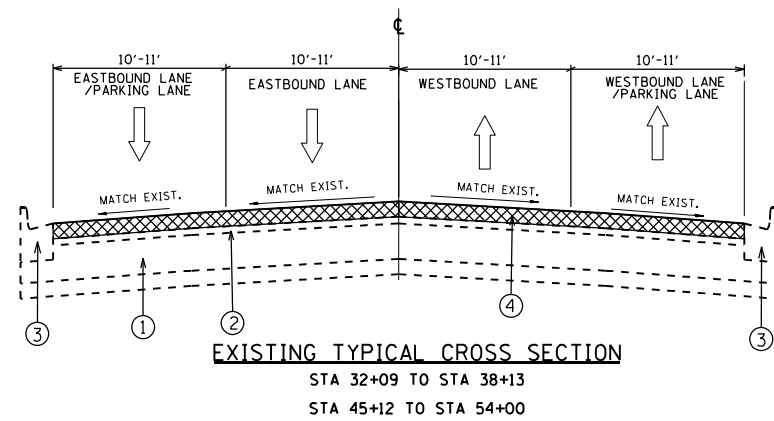
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED TYPICAL SECTIONS
 ADDISON ST- AVONDALE TO SACRAMENTO AVE**

SCALE: SHEET 1 OF 2 SHEETS STA. 11+43 TO STA. 57+12

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|---------------|--------|--------------------|-----------|
| 1372 | 2018-19-RS-SW | COOK | 49 | 8 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 62G71 | |

ADDISON ST



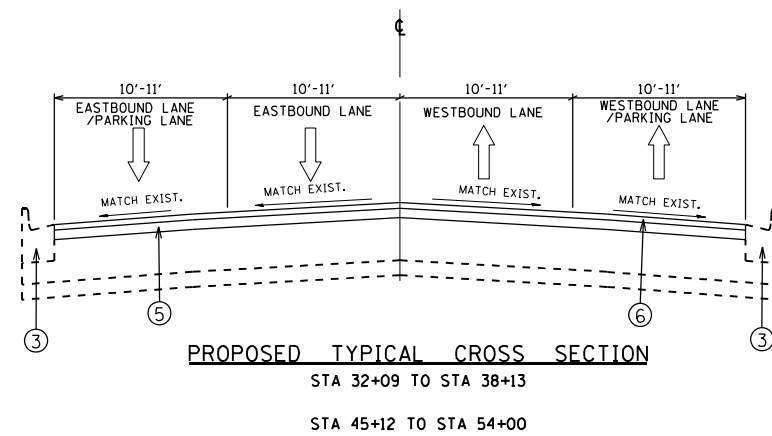
LEGEND

- ① EXISTING PCC BASE COURSE, ±8"
- ② EXISTING HMA PAVEMENT ±7"
- ③ EXISTING COMB. CONCRETE CURB AND GUTTER
- ④ PROPOSED HMA SURFACE REMOVAL, 3 3/4 "
- ⑤ PROPOSED HMA BINDER COURSE, IL-9.5, N70; 2"
- ⑥ PROPOSED POLYMERIZED HMA SURFACE COURSE, IL-9.5, MIX E, N70 1 3/4"

NOTES

1. THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.
2. LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE HMA BINDER COURSE

ADDISON ST



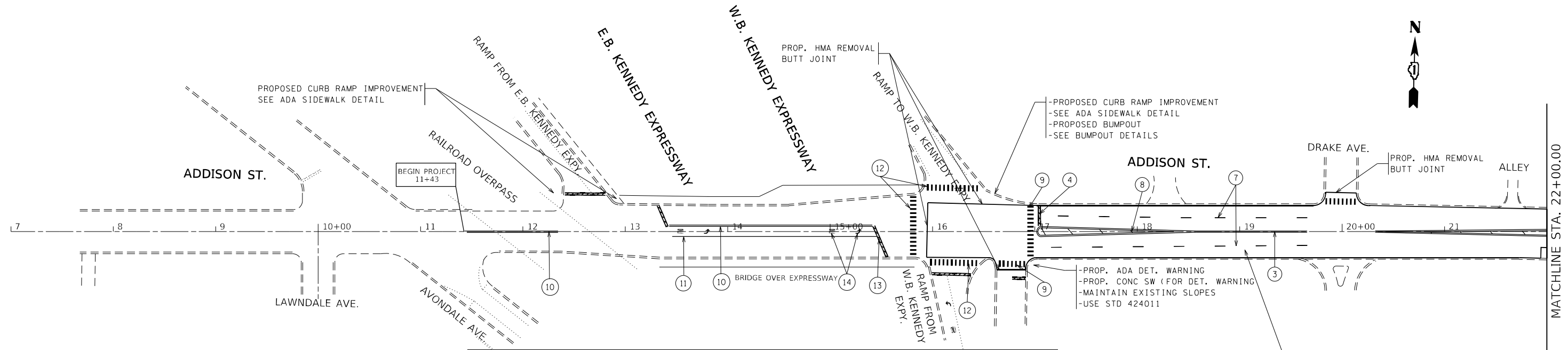
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED TYPICAL SECTIONS
 ADDISON ST- AVONDALE TO SACRAMENTO AVE

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------------|--------|------------------|-----------|
| 1372 | 2018-19-RS-SW | COOK | 49 | 9 |
| ILLINOIS | | | FED. AID PROJECT | |

SCALE: SHEET 2 OF 2 SHEETS STA. 11+43 TO STA. 57+12

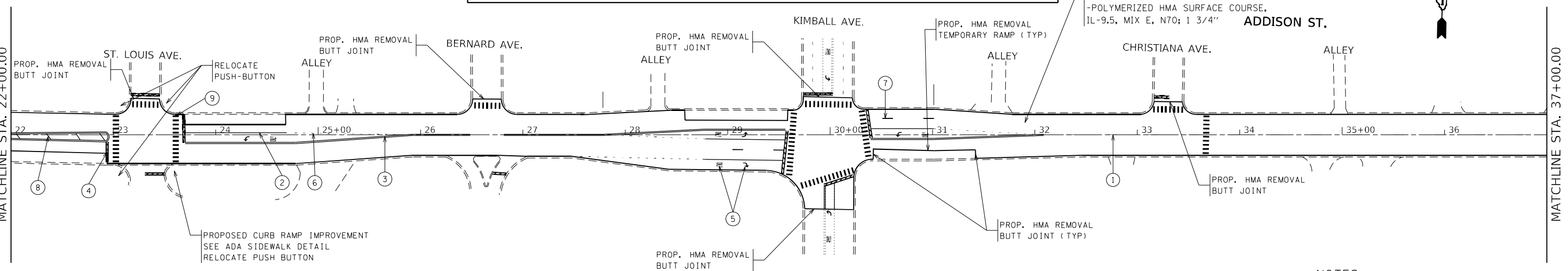
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|------------------------------|------------|-----------|
| USER NAME = nullanecd | DESIGNED - | REVISED - |
| PLOT SCALE = 100,0000' / in. | DRAWN - | REVISED - |
| PLOT DATE = 3/10/2022 | CHECKED - | REVISED - |
| | DATE - | REVISED - |



PAVEMENT MARKING LEGEND

| | |
|---|---|
| ① PROP. THERMOPLASTIC PVMT MARKING LINE 4", 6" DASH 18" SKIP, YELLOW (TYP.) | ⑧ PROP. THERMOPLASTIC PVMT MARKING LINE 12", DIAGONALS, YELLOW (TYP.) |
| ② PROP. THERMOPLASTIC PVMT MARKING LINE 6", TURN LANE, WHITE (TYP.) | ⑨ PROP. THERMOPLASTIC PVMT MARKING LINE 24", WHITE (TYP.) |
| ③ PROP. THERMOPLASTIC PVMT MARKING LINE 4", DOUBLE YELLOW @ 8" C-C (TYP.) | ⑩ PROP. URETHINE PVMT MARKING LINE 4", DOUBLE YELLOW @ 8" (TYP.) |
| ④ PROP. THERMOPLASTIC PVMT MARKING LINE 24", STOPBAR, WHITE (TYP.) | ⑪ PROP. URETHINE PVMT MARKING LINE 6", WHITE (TYP.) |
| ⑤ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS, WHITE (TYP.) | ⑫ PROP. URETHINE PVMT MARKING LINE 24", WHITE (TYP.) |
| ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE 6", 2" DASH 6" SKIP, WHITE (TYP.) | ⑬ PROP. URETHINE PVMT MARKING LINE 24", STOPBAR, WHITE (TYP.) |
| ⑦ PROP. THERMOPLASTIC PVMT MARKING LINE 4", 6" DASH 18" SKIP, WHITE (TYP.) | ⑭ PROP. URETHINE PVMT MARKING, LETTERS AND SYMBOLS, WHITE (TYP.) |

NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE DETAILS TC-24



NOTES:
 TEMPORARY RAMPS WILL BE CONSTRUCTED AT ALL DRIVEWAYS AND ENTRANCES WHERE DEPRESSED CURB EXISTS. THEY WILL BE 2 FEET WIDE.

MODEL: Default
 FILE: MainE_Plan_Schedule-sw_bentley.com/PIV/DOT/Documents/IDOT/Office/Dir/rdct_1/Projects/1337_8/CADD/1337_8/Drawings/1337_8-sw/plan.dgn

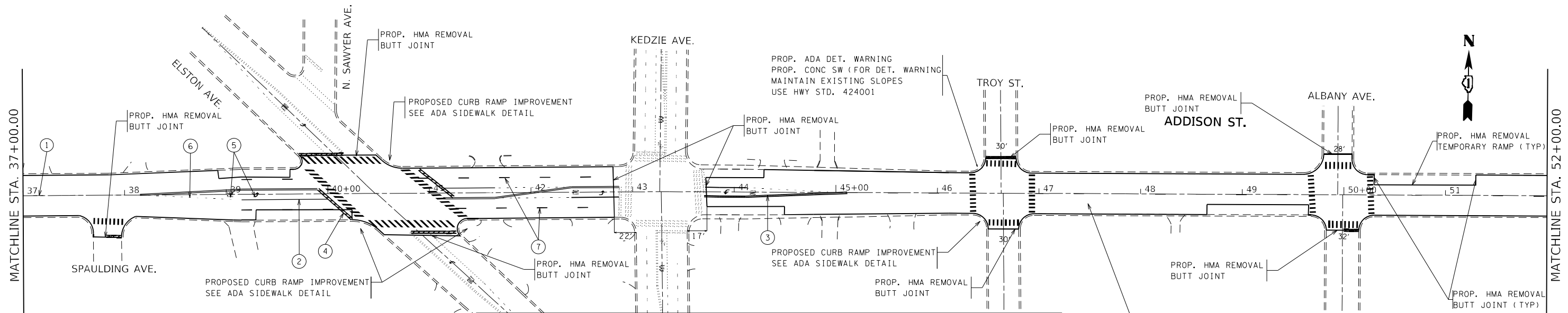
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|------------------------------|------------|-----------|
| USER NAME = nullanecd | DESIGNED - | REVISED - |
| | DRAWN - | REVISED - |
| PLOT SCALE = 100,0000' / in. | CHECKED - | REVISED - |
| PLOT DATE = 3/11/2022 | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ADDISON ST. (AVONDALE AVE. TO SACRAMENTO AVE.)
 PROPOSED ROADWAY PLAN**

SCALE: 1"=50' SHEET 1 OF 2 SHEETS STA. 11+43 TO STA. 57+12

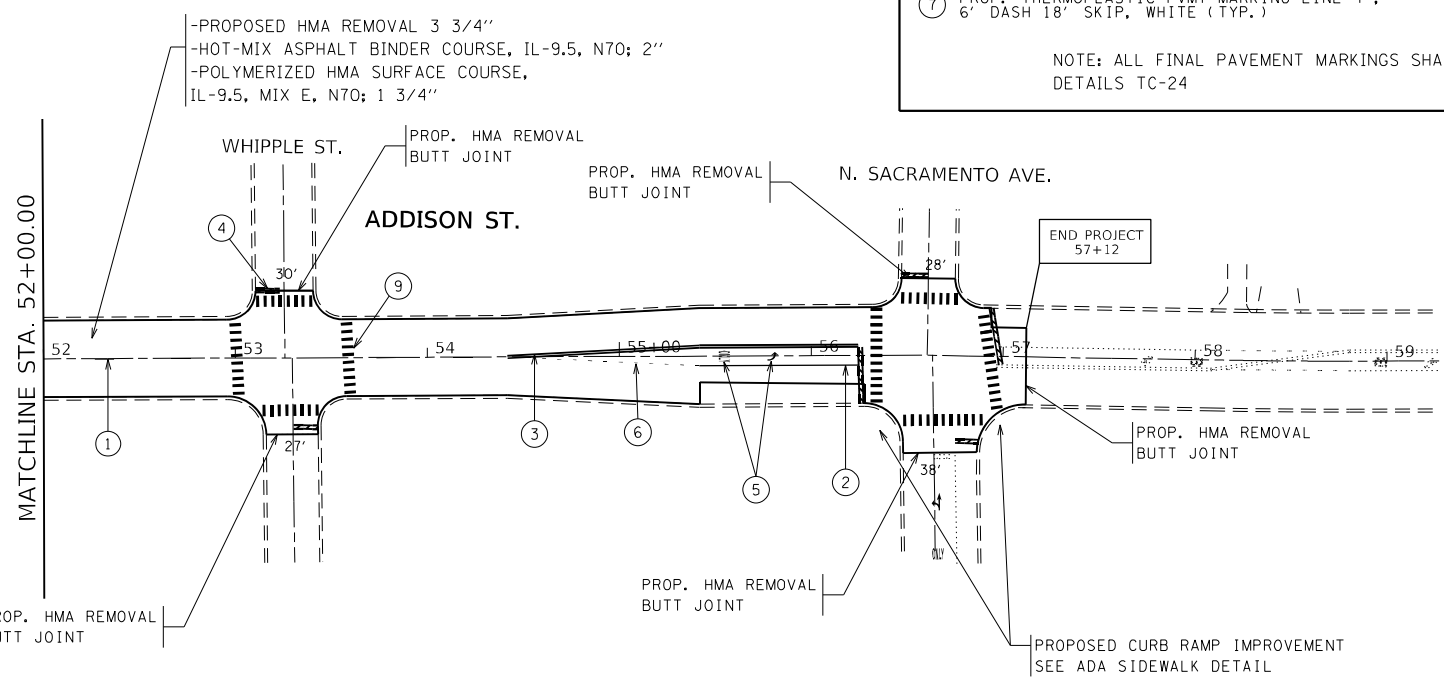
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------|-----------|
| 1372 | 2018-019-R5-SW | COOK | 49 | 10 |
| CONTRACT NO. 62G71 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



PAVEMENT MARKING LEGEND

| | |
|---|---|
| ① PROP. THERMOPLASTIC PVMT MARKING LINE 4", 6" DASH 18" SKIP, YELLOW (TYP.) | ⑧ PROP. THERMOPLASTIC PVMT MARKING LINE 12", DIAGONALS, YELLOW (TYP.) |
| ② PROP. THERMOPLASTIC PVMT MARKING LINE 6", TURN LANE, WHITE (TYP.) | ⑨ PROP. THERMOPLASTIC PVMT MARKING LINE 24", WHITE (TYP.) |
| ③ PROP. THERMOPLASTIC PVMT MARKING LINE 4", DOUBLE YELLOW @ 8" C-C (TYP.) | ⑩ PROP. URETHINE PVMT MARKING LINE 4", DOUBLE YELLOW @ 8" (TYP.) |
| ④ PROP. THERMOPLASTIC PVMT MARKING LINE 24", STOPBAR, WHITE (TYP.) | ⑪ PROP. URETHINE PVMT MARKING LINE 6", WHITE (TYP.) |
| ⑤ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS, WHITE (TYP.) | ⑫ PROP. URETHINE PVMT MARKING LINE 24", WHITE (TYP.) |
| ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE 6", 2" DASH 6" SKIP, WHITE (TYP.) | ⑬ PROP. URETHINE PVMT MARKING LINE 24", STOPBAR, WHITE (TYP.) |
| ⑦ PROP. THERMOPLASTIC PVMT MARKING LINE 4", 6" DASH 18" SKIP, WHITE (TYP.) | ⑭ PROP. URETHINE PVMT MARKING, LETTERS AND SYMBOLS, WHITE (TYP.) |

NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE DETAILS TC-24



NOTES:

TEMPORARY RAMPs WILL BE CONSTRUCTED AT ALL DRIVEWAYS AND ENTRANCES WHERE DEPRESSED CURB EXISTS. THEY WILL BE 2 FEET WIDE.

OMIT KEDZIE AVE INTERSECTION. IT IS TO BE RESURFACED AND RESTRIPEd BY CDOOT NORTH AREA CONTRACT B-7-173.

MODEL: Default
 FILE: Addisn_Plan_PlanView.dwg
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 PROJECT: 13728133218-Addisn_PlanView.dwg
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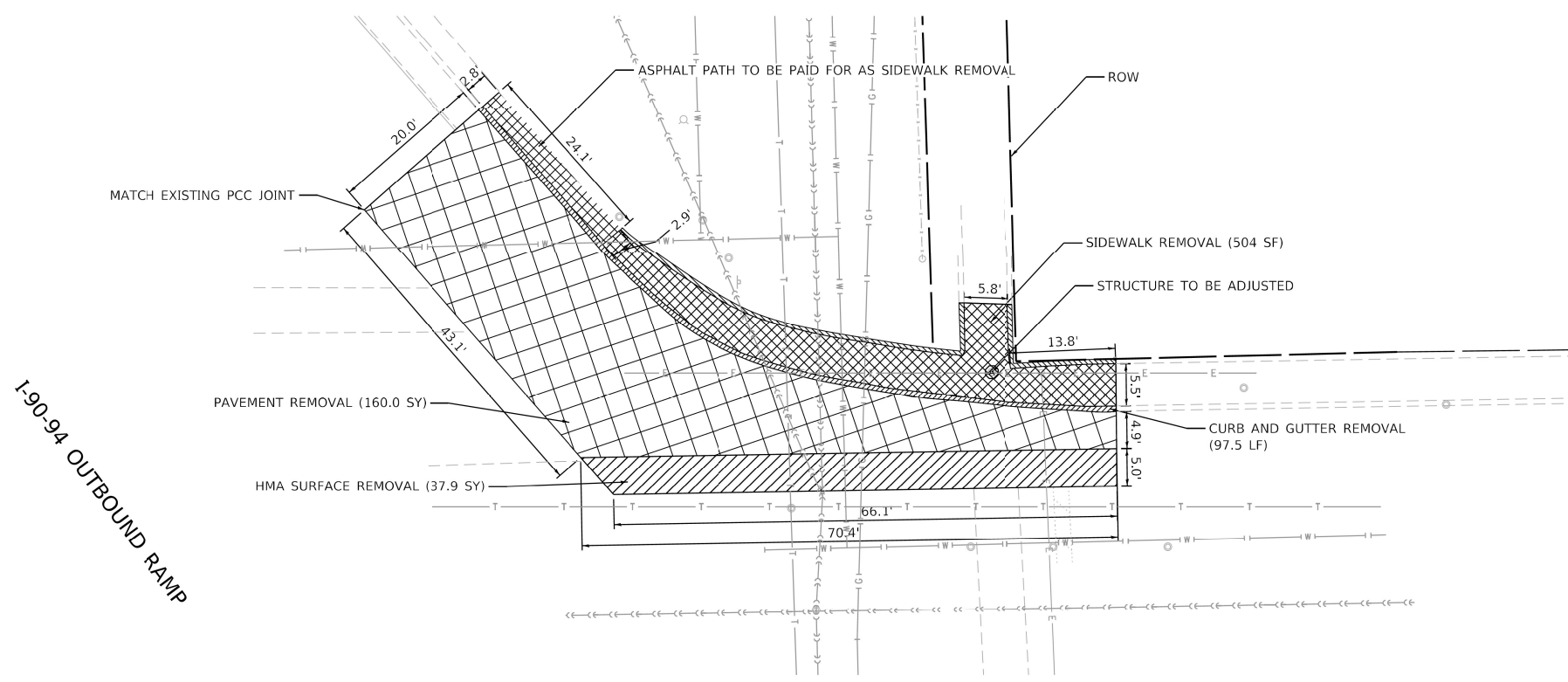
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| PLOT SCALE = 100,0000' / in. | DRAWN - | REVISED - |
| PLOT DATE = 3/11/2022 | CHECKED - | REVISED - |
| | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADDISON ST. (AVONDALE AVE. TO SACRAMENTO AVE.)
PROPOSED ROADWAY PLAN**

SCALE: 1"=50' SHEET 2 OF 2 SHEETS STA. 11+43 TO STA. 57+12

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|----------------|----------|------------------|-----------|
| 1372 | 2018-019-RS-SW | COOK | 49 | 11 |
| CONTRACT NO. 62G71 | | | | |
| | | ILLINOIS | FED. AID PROJECT | |



I-90-94 OUTBOUND RAMP

W. ADDISON ST.



LEGEND

- CURB & GUTTER REMOVAL
- CURB REMOVAL
- SIDEWALK REMOVAL
- PAVEMENT REMOVAL
- HMA SURFACE REMOVAL
- PARKWAY REMOVAL (TO BE PAID AS EARTH EXCAVATION - SOIL TO LANDFILL)
- DRAINAGE OR UTILITY STRUCTURE TO BE ADJUSTED



SURVEYED:
DESIGNED: DLEVIN 01-21-2022
CHECKED: SPOPOVICI 01-21-2022
CCE #:
PROJ #: D-1-312-25
SCALE: 1" = 10'

| NO. | BY | DATE | DESCRIPTION |
|-----------|----|------|-------------|
| REVISIONS | | | |

2022 CIC PROGRAM
WARD: 35
LOCATION: W. ADDISON ST.
FROM: I-94 OUTBOUND RAMP
TO: I-94 OUTBOUND RAMP

**W ADDISON ST AND
I-94 OUTBOUND RAMP
IMPROVEMENTS**

REMOVAL PLAN

DRAWING NO.
REM-1
SHEET NO.
4 OF 14

MODEL: Default
FILE NAME: p:\p\1\dot\documents\DOT Office\District 1\projects\133218\CADD\data\Design\133218-shr-ADA-design.dgn
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Time: 10:00 AM

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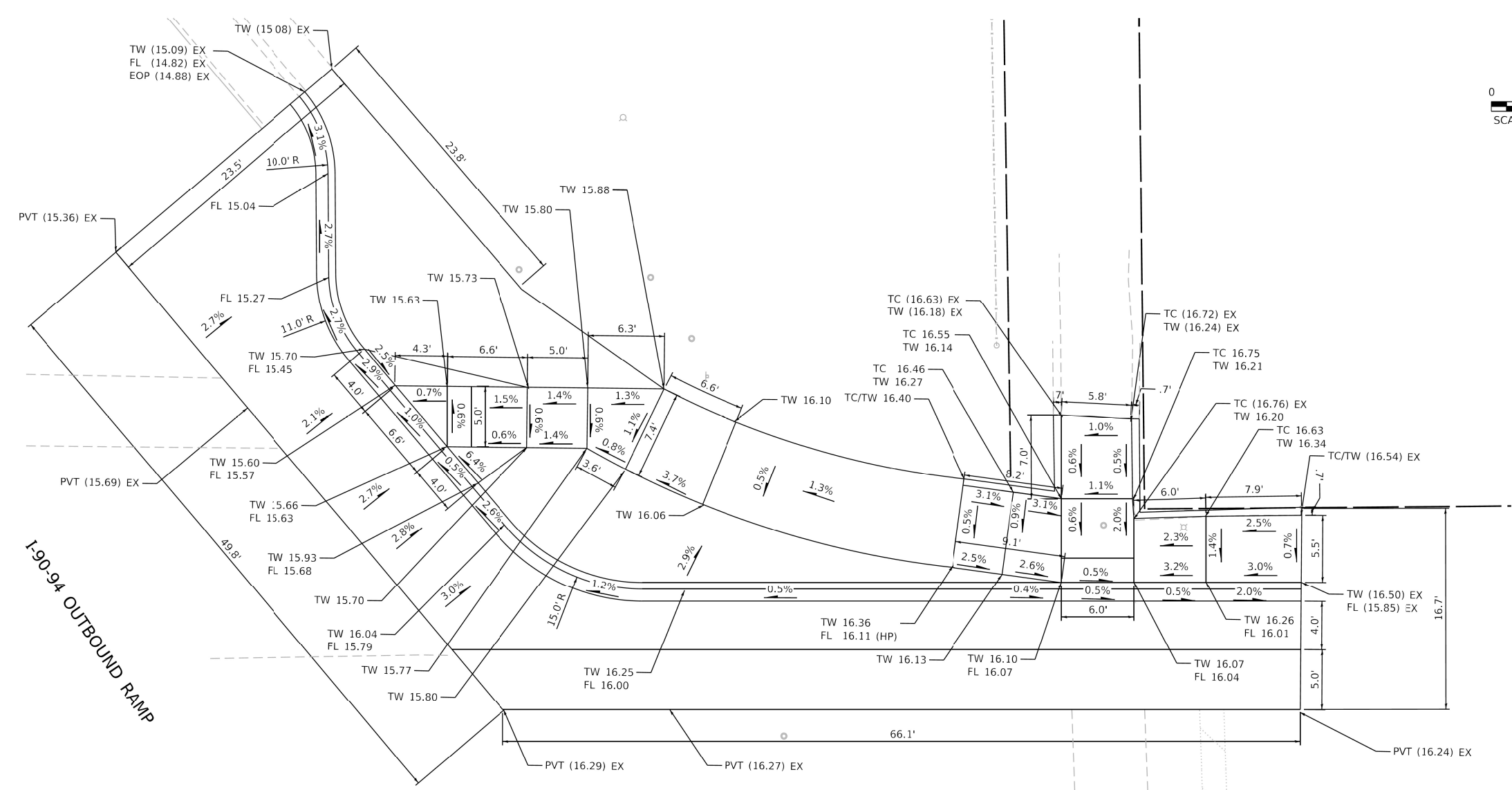
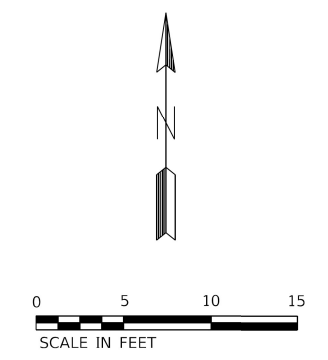
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| USER NAME = nullanecd | DESIGNED - | REVISED - |
| PLOT SCALE = 10,0000 * / in. | DRAWN - | REVISED - |
| PLOT DATE = 2/7/2022 | CHECKED - | REVISED - |
| | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**W ADDISON ST AND I-94
OUTBOUND RAMP IMPROVEMENTS- REMOVAL PLAN**

SCALE: SHEET OF SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------|-----------|
| 1372 | 2018-019-R5-SW | COOK | 49 | 14 |
| CONTRACT NO. 62G71 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



I-94 OUTBOUND RAMP

W. ADDISON ST.

| CDOT Chicago Department of Transportation | SURVEYED: DLEVIN DESIGNED: SPOPOVICI 01-21-2022 CHECKED: 01-21-2022 CCE #: PROJ #: D-1-312-25 SCALE: 1" = 5' | <table border="1" style="width:100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 10%;">NO.</th> <th style="width: 10%;">BY</th> <th style="width: 10%;">DATE</th> <th style="width: 70%;">DESCRIPTION</th> </tr> </thead> <tbody> <tr> <td colspan="4" style="text-align: center;">REVISIONS</td> </tr> </tbody> </table> | NO. | BY | DATE | DESCRIPTION | REVISIONS | | | | 2022 CIC PROGRAM WARD: 35 LOCATION: W. ADDISON ST. FROM: I-94 OUTBOUND RAMP TO: I-94 OUTBOUND RAMP | W ADDISON ST AND I-94 OUTBOUND RAMP IMPROVEMENTS | GRADING PLAN | DRAWING NO. GR-1 SHEET NO. 6 OF 14 |
|---|---|---|------|-------------|------|-------------|-----------|--|--|--|---|---|---------------------|---|
| | NO. | BY | DATE | DESCRIPTION | | | | | | | | | | |
| REVISIONS | | | | | | | | | | | | | | |
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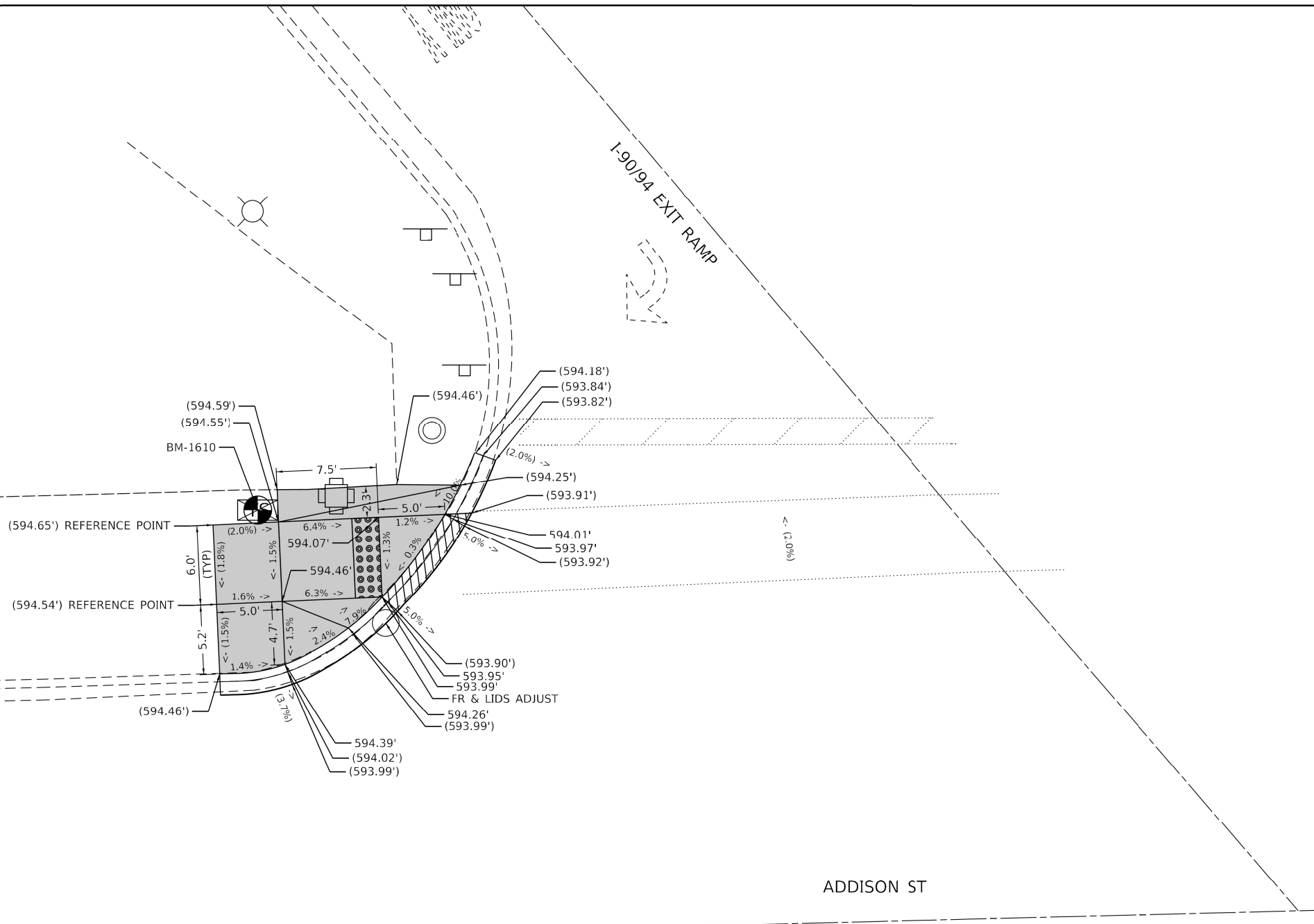
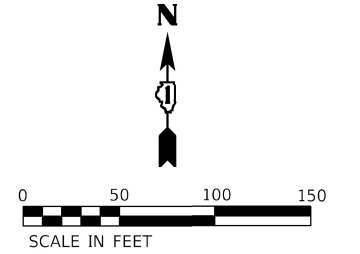
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 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**W ADDISON ST AND I-94
 OUTBOUND RAMP IMPROVEMENTS- GRADING PLAN**

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------|-----------|
| 1372 | 2018-019-RS-SW | COOK | 49 | 16 |
| CONTRACT NO. 62G71 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

SCALE: SHEET OF SHEETS STA. TO STA.



- NOTES:**
1. AT DEPRESSED CURBS, ADJACENT TO CURB RAMPS ACCESSIBLE TO THE DISABLED, THE GUTTER FLAG SLOPE SHALL BE -5% UNLESS NOTED OTHERWISE.

REFERENCE BENCHMARK 1610 EL 596.25
 BENCHMARK: SE SQUARE TCB
 LOCATION: NW CORNER OF ADDISON ST AND EXIT RAMP

LEGEND

| | |
|--------|--|
| XX.XX' | EXISTING LENGTH |
| () | EXISTING ELEVATION / SLOPE |
| | PROPOSED SIDEWALK |
| | DETECTABLE WARNINGS |
| | SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD |
| | DEPRESSED CURB AND GUTTER |



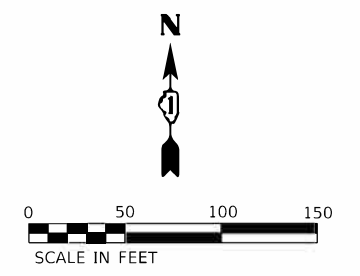
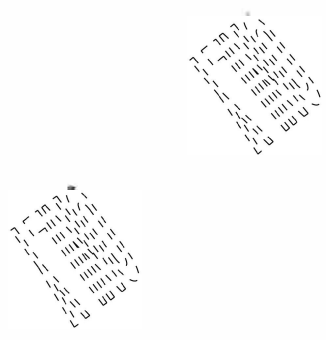
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| | DRAWN - SK | REVISED - |
| PLOT SCALE = 10.0000' / 1" | CHECKED - RG | REVISED - |
| PLOT DATE = 2/2/2022 | DATE - 02/01/2022 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

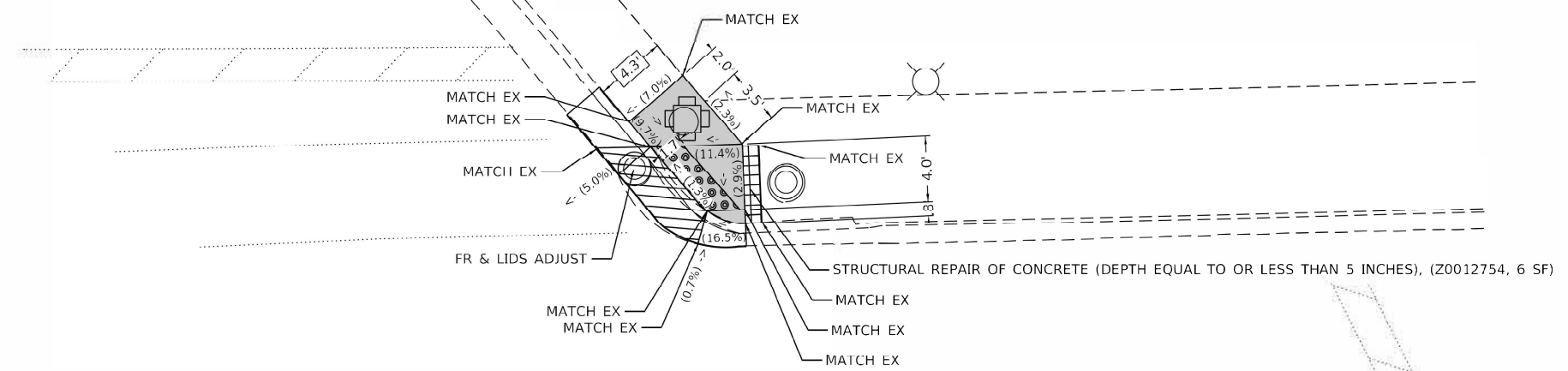
**ADA RAMP DETAILS
NW CORNER ADDISON ST AND I-90/94 EXIT RAMP**

| | | | | |
|---------------------------|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1372 | 2018-019-RS-SW | COOK | 49 | 18 |
| CONTRACT NO. 62G71 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

MODEL: Default
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I-90/94 EXIT RAMP



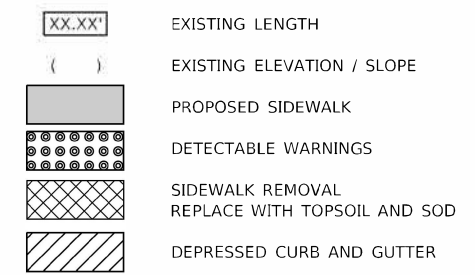
ADDISON ST

NOTES:

1. AT DEPRESSED CURBS, ADJACENT TO CURB RAMPS ACCESSIBLE TO THE DISABLED, THE GUTTER FLAG SLOPE SHALL BE -5% UNLESS NOTED OTHERWISE.

REFERENCE BENCHMARK 1610 EL 596.25 (NOT SHOWN)
 BENCHMARK: SE SQUARE TCB
 LOCATION: NW CORNER OF ADDISON ST AND EXIT RAMP

LEGEND



MODEL: P624.dwg
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| | | |
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| USER NAME = sklerpiec | DESIGNED - SK | REVISED - |
| | DRAWN - SK | REVISED - |
| PLOT SCALE = 10.0000' / in. | CHECKED - RG | REVISED - |
| PLOT DATE = 3/3/2022 | DATE - 02/01/2022 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS
 NE CORNER ADDISON ST AND I-90/94 EXIT RAMP**

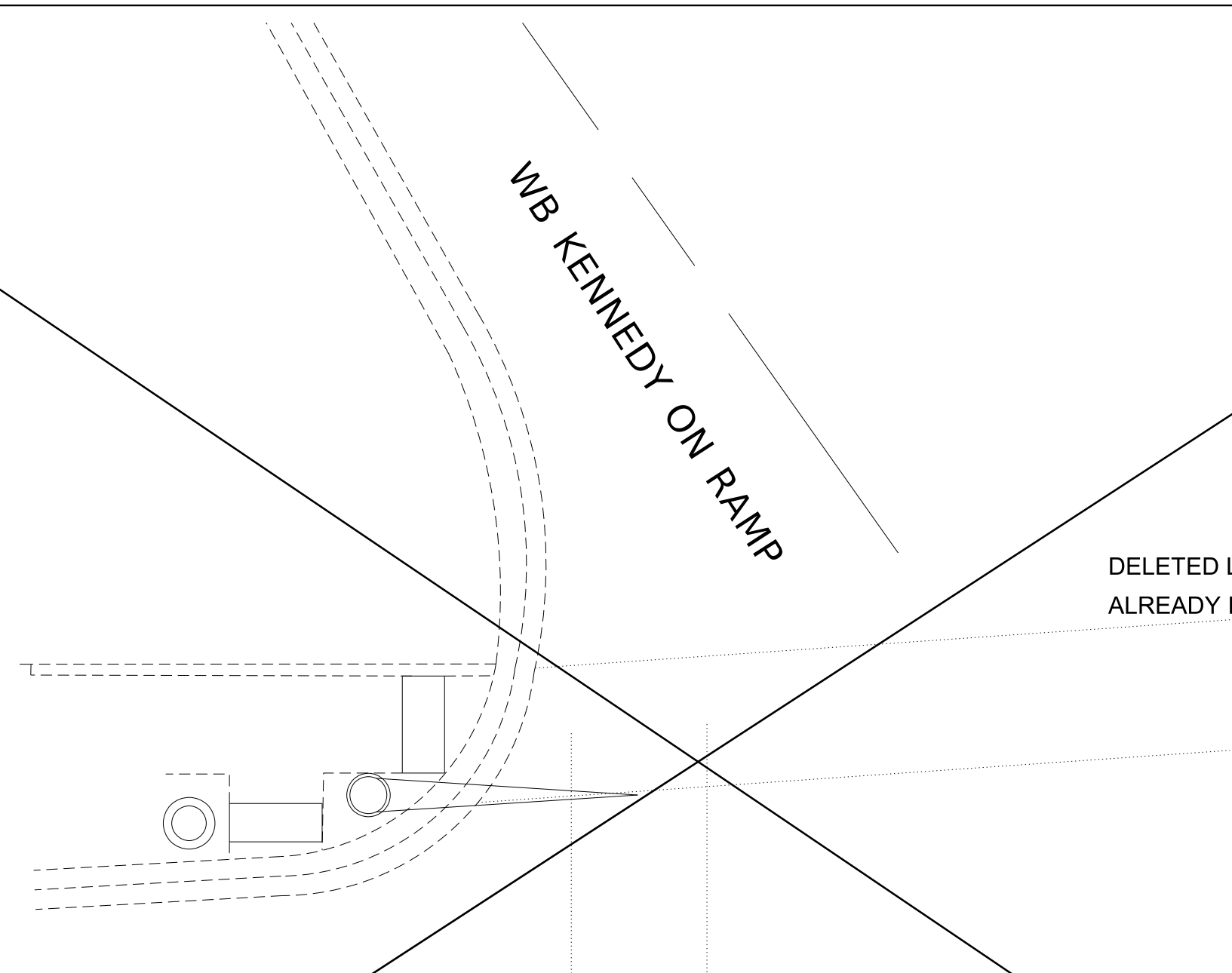
SCALE: SHEET OF SHEETS STA. TO STA.

| | | | | |
|---------------------------|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1372 | 2018-019-R5-SW | COOK | 49 | 19 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 62G71 | |



WB KENNEDY ON RAMP

DELETED LOCATION
ALREADY IMPROVED



NOTES

AT DEPRESSED CURBS, ADJACENT TO CURB RAMPS ACCESSABLE TO THE DISABLED, THE GUTTER FLAG SLOPE SHALL BE -5% UNLESS NOTED OTHERWISE

LEGEND

- XX.XX' EXISTING LENGTH
- DETECTABLE WARNING
- PROPOSED SIDEWALK
- SIDEWALK REMOVAL WITH TOPSOIL AND SOD
- DEPRESSED CURB AND GUTTER

M:\EPL_Default\...
 FILE NAME: ...
 PROJECT: ...
 USER: ...

| | | |
|------------------------------------|------------|-----------|
| USER NAME = nullanecd | DESIGNED - | REVISED - |
| | DRAWN - | REVISED - |
| PLOT SCALE = 1/4" = 14.0000' / in. | CHECKED - | REVISED - |
| PLOT DATE = 2/7/2022 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS
NE CORNER OF DDISON ST AND KENNEDY WB ENTRANCE**

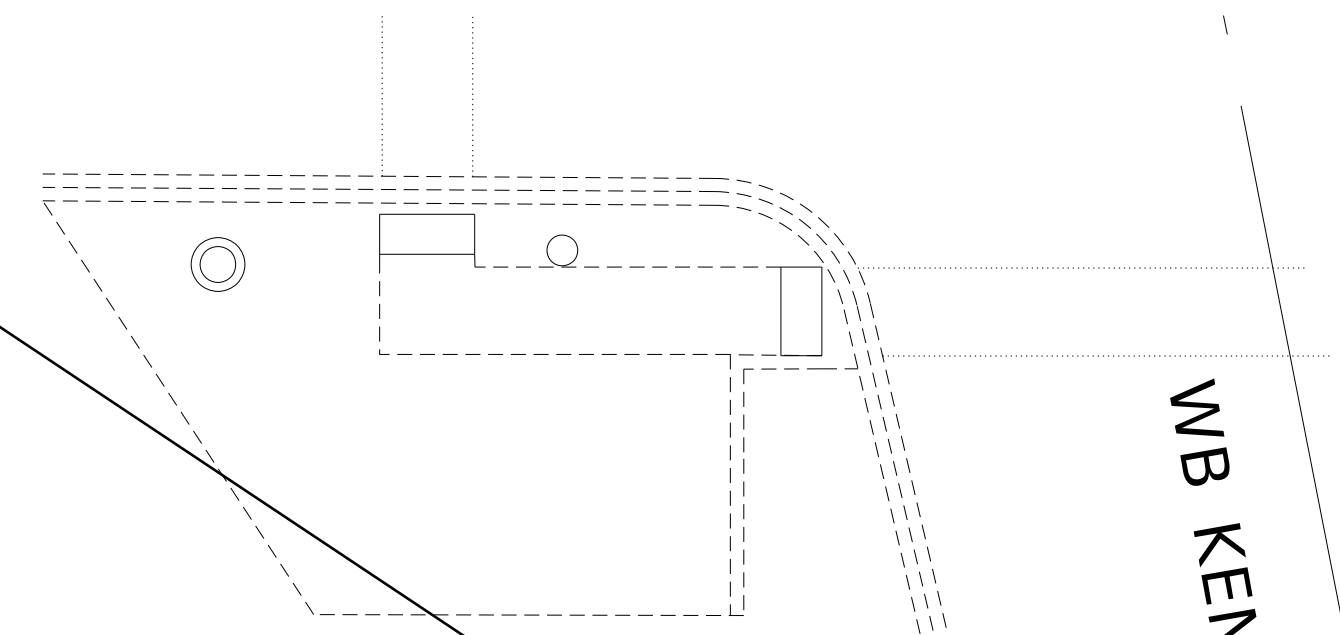
SCALE: SHEET OF SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------------|-----------|
| 1372 | 2018-019-R5-SW | COOK | 49 | 20 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 62G71 | |



WB KENNEDY OFF RAMP

DELETED LOCATION
ALREADY IMPROVED



NOTES

AT DEPRESSED CURBS, ADJACENT TO CURB RAMPS ACCESSABLE TO THE DISABLED, THE GUTTER FLAG SLOPE SHALL BE -5% UNLESS NOTED OTHERWISE

LEGEND

- XX.XX' EXISTING LENGTH
- DETECTABLE WARNING
- PROPOSED SIDEWALK
- SIDEWALK REMOVAL WITH TOPSOIL AND SOD
- DEPRESSED CURB AND GUTTER

MODEL: Default
FILE: \\mllc-pw-bentley.com\PW\DOT\Documents\DOT Office\District_1\Projects\133218\133218-shr-ADA-design.dgn

| | | |
|-------------------------------|------------|-----------|
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| PLOT SCALE = 1/4,0000 * / in. | DRAWN - | REVISED - |
| PLOT DATE = 2/7/2022 | CHECKED - | REVISED - |
| | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

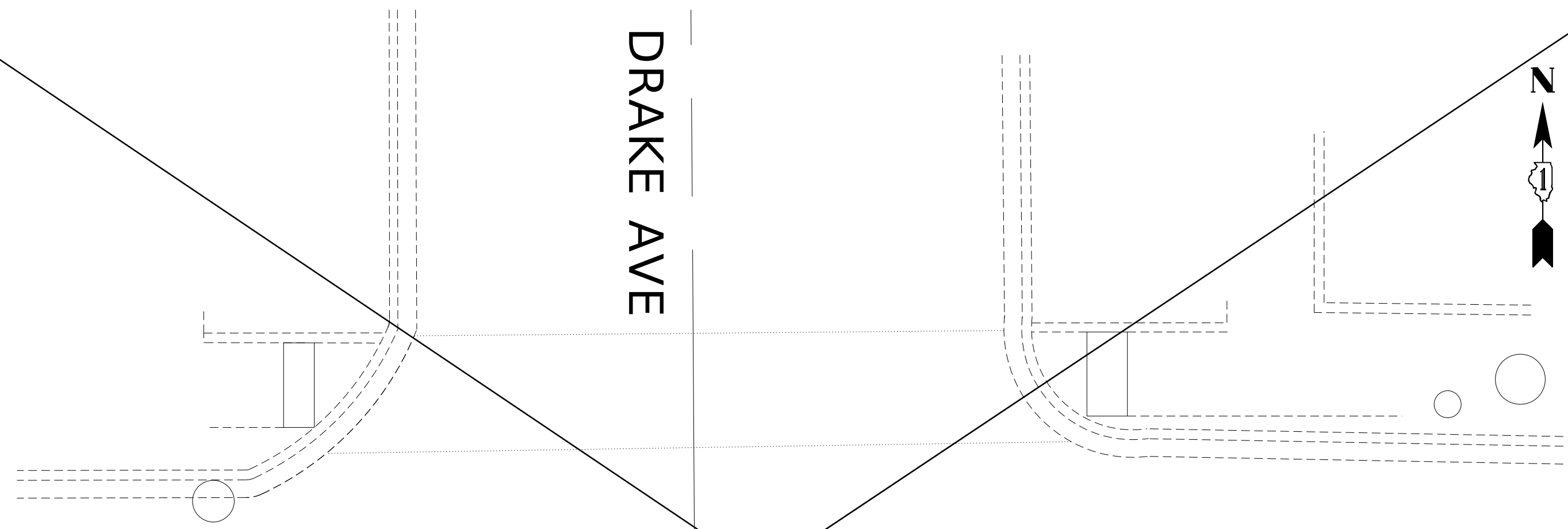
**ADA RAMP DETAILS
NE CORNER OF DDISON ST AND KENNEDY WB ENTRANCE**

SCALE: SHEET OF SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------------|-----------|
| 1372 | 2018-019-RS-SW | COOK | 19 | 21 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 05G71 | |



DRAKE AVE



DELETED LOCATION
ALREADY IMPROVED

NOTES

AT DEPRESSED CURBS, ADJACENT TO CURB RAMPS ACCESSABLE TO THE DISABLED, THE GUTTER FLAG SLOPE SHALL BE -5% UNLESS NOTED OTHERWISE

LEGEND

- XX.XX' EXISTING LENGTH
- DETECTABLE WARNING
- PROPOSED SIDEWALK
- SIDEWALK REMOVAL WITH TOPSOIL AND SOD
- DEPRESSED CURB AND GUTTER

MODEL: Default
FILE: \\blmfr-pw-bentley.com\P\W\DOT\Documents\DOT Office\District_1\Projects\133218\133218.dwg - ADA-design.dgn

| | | |
|------------------------------------|------------|-----------|
| USER NAME = nullanecd | DESIGNED - | REVISED - |
| | DRAWN - | REVISED - |
| PLOT SCALE = 1/4" = 14.0000' / in. | CHECKED - | REVISED - |
| PLOT DATE = 2/7/2022 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS
NE CORNER OF DDISON ST AND KENNEDY WB ENTRANCE**

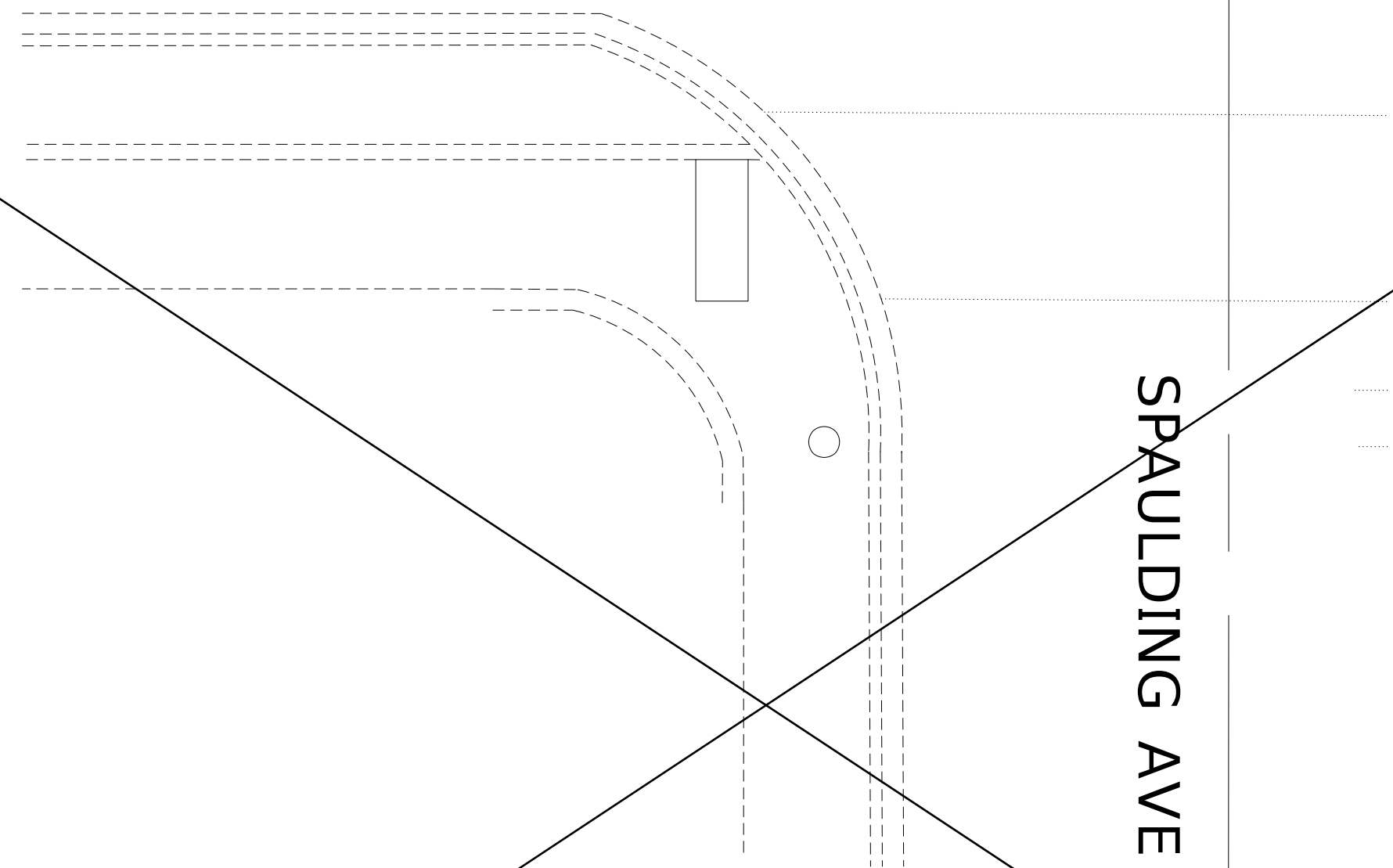
SCALE: SHEET OF SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------|-----------|
| 1372 | 2018-019-RS-SW | COOK | 49 | 22 |
| CONTRACT NO. 62G71 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



SPAULDING AVE

DELETED LOCATION
ALREADY IMPROVED



NOTES
AT DEPRESSED CURBS, ADJACENT TO CURB RAMPS ACCESSABLE TO THE DISABLED, THE GUTTER FLAG SLOPE SHALL BE -5% UNLESS NOTED OTHERWISE

LEGEND

- XX.XX' EXISTING LENGTH
- DETECTABLE WARNING
- PROPOSED SIDEWALK
- SIDEWALK REMOVAL WITH TOPSOIL AND SOD
- DEPRESSED CURB AND GUTTER

MODEL: Default
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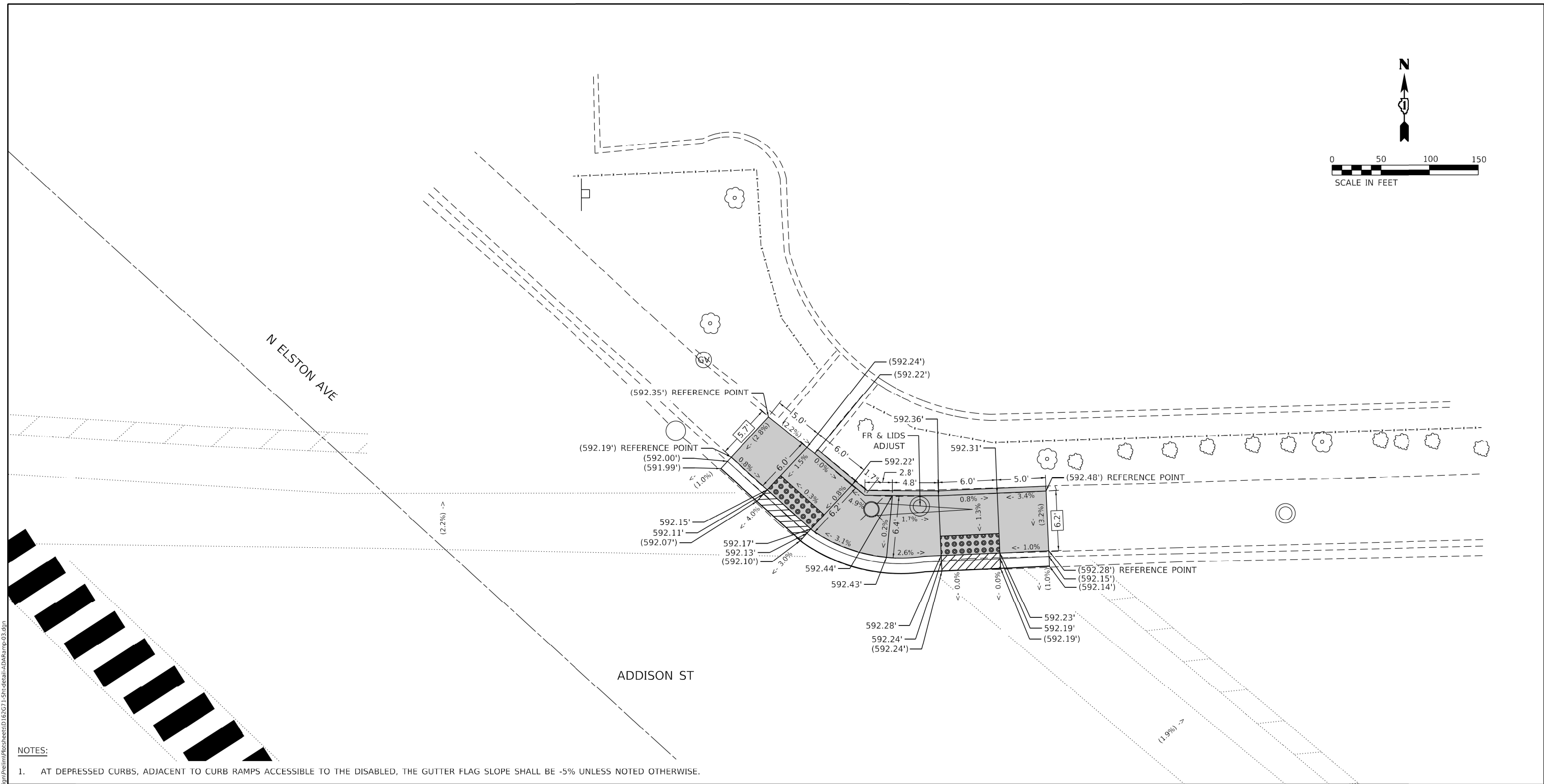
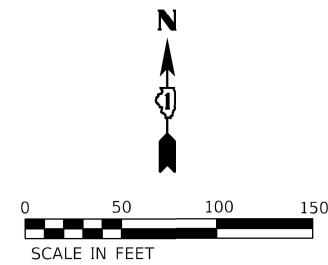
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|---------------------------|------------|-----------|
| USER NAME = nullanecd | DESIGNED - | REVISED - |
| PLOT SCALE = 1/4" = 1'-0" | DRAWN - | REVISED - |
| PLOT DATE = 2/7/2022 | CHECKED - | REVISED - |
| | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS
NE CORNER OF DDISON ST AND KENNEDY WB ENTRANCE**

SCALE: SHEET OF SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------------|-----------|
| 1372 | 2018-019-RS-SW | COOK | 49 | 25 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 62671 | |



NOTES:

1. AT DEPRESSED CURBS, ADJACENT TO CURB RAMPS ACCESSIBLE TO THE DISABLED, THE GUTTER FLAG SLOPE SHALL BE -5% UNLESS NOTED OTHERWISE.

REFERENCE BENCHMARK 3256 EL 592.599 (NOT SHOWN)
 BENCHMARK: CUT SQUARE N CORNER TCB
 LOCATION: SE CORNER OF ADDISON ST AND ELSTON AVE

LEGEND

- XX.XX' EXISTING LENGTH
- () EXISTING ELEVATION / SLOPE
- [Solid Grey] PROPOSED SIDEWALK
- [Dotted Pattern] DETECTABLE WARNINGS
- [Cross-hatch Pattern] SIDEWALK REMOVAL
REPLACE WITH TOPSOIL AND SOD
- [Diagonal Line Pattern] DEPRESSED CURB AND GUTTER

MODEL: Default
 FILE NAME: 20220303.06 DOT D1 IPEL 199.01.2 WO B Addison ADA Ramps DGN Design Prelim Sheets D162G71-Sht-detail-ADA Ramp-03.dgn



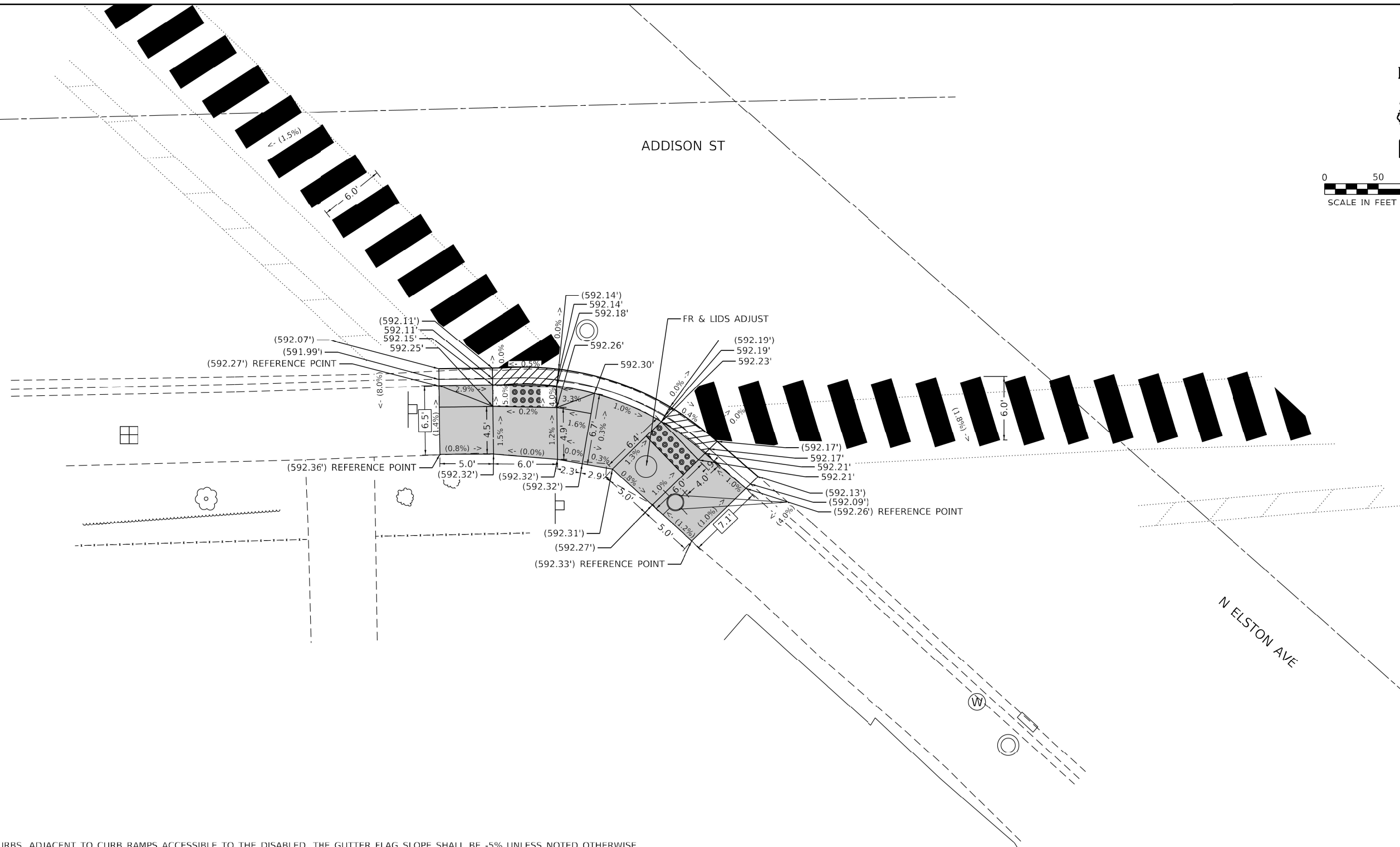
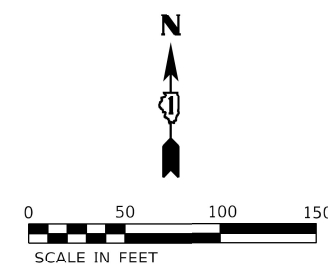
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|----------------------------|-------------------|-----------|
| USER NAME = skierpiec | DESIGNED - SK | REVISED - |
| | DRAWN - SK | REVISED - |
| PLOT SCALE = 10.0000' / 1" | CHECKED - RG | REVISED - |
| PLOT DATE = 2/2/2022 | DATE - 02/01/2022 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS
NE CORNER ADDISON ST AND N ELSTON AVE**

SCALE: SHEET OF SHEETS STA. TO STA.

| | | | | |
|---------------------------|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1372 | 2018-019-RS-SW | COOK | 49 | 26 |
| CONTRACT NO. 62G71 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



- NOTES:**
1. AT DEPRESSED CURBS, ADJACENT TO CURB RAMP ACCESSIBLE TO THE DISABLED, THE GUTTER FLAG SLOPE SHALL BE -5% UNLESS NOTED OTHERWISE.

REFERENCE BENCHMARK 3256 EL 592.599 (NOT SHOWN)
 BENCHMARK: CUT SQUARE N CORNER TCB
 LOCATION: SE CORNER OF ADDISON ST AND ELSTON AVE

LEGEND

| | |
|--------|--|
| XX.XX' | EXISTING LENGTH |
| () | EXISTING ELEVATION / SLOPE |
| | PROPOSED SIDEWALK |
| | DETECTABLE WARNINGS |
| | SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD |
| | DEPRESSED CURB AND GUTTER |



| | | |
|----------------------------|-------------------|-----------|
| USER NAME = skierpiec | DESIGNED - SK | REVISED - |
| | DRAWN - SK | REVISED - |
| PLOT SCALE = 10.0000' / 1" | CHECKED - RG | REVISED - |
| PLOT DATE = 2/2/2022 | DATE - 02/01/2022 | REVISED - |

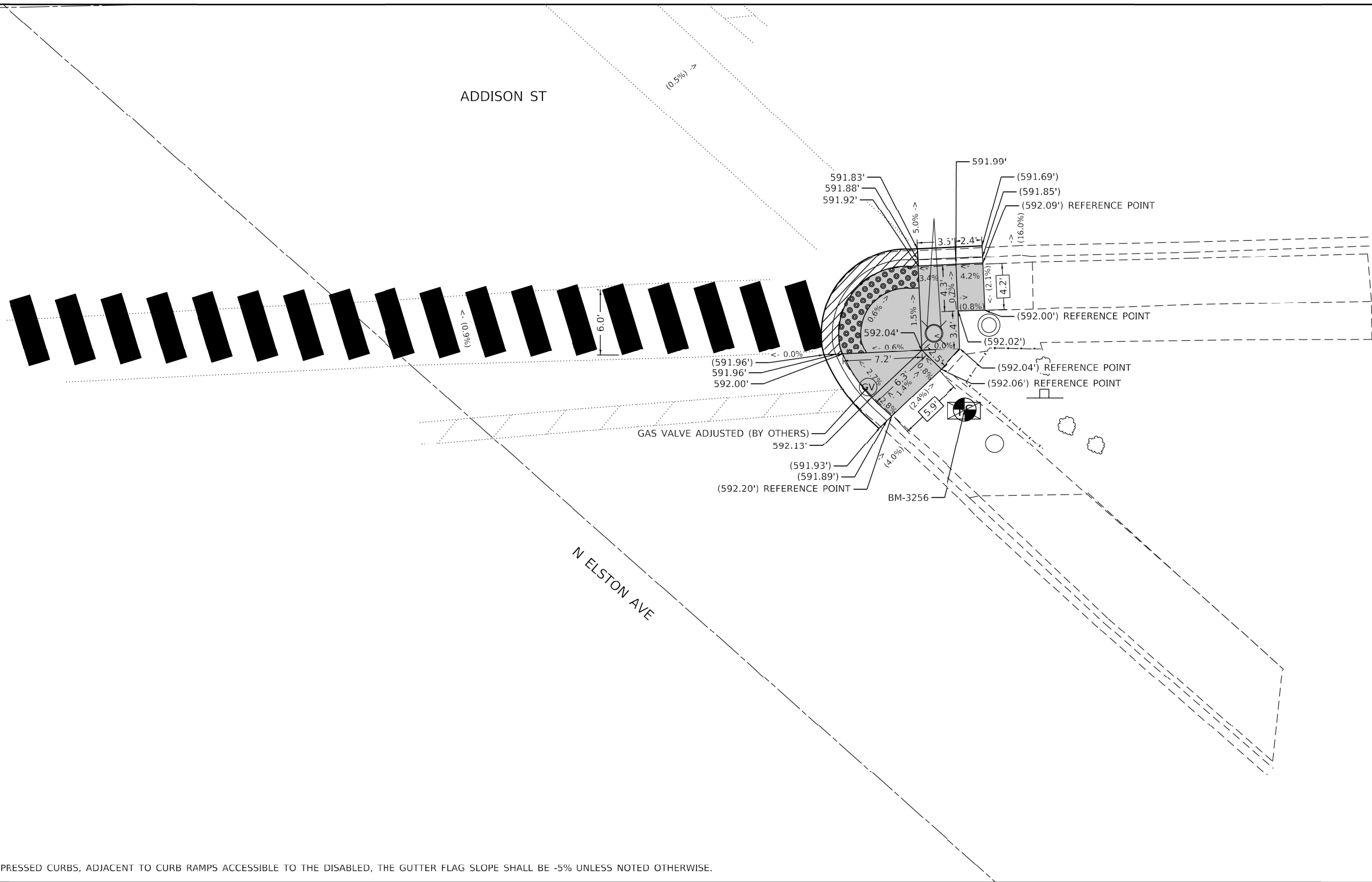
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS
 SW CORNER ADDISON ST AND N ELSTON AVE**

| | | | | |
|---------------------------|------------------------|-------------|--------------------|--------------|
| F.A.U. RTE. 1372 | SECTION 2018-019-RS-SW | COUNTY COOK | TOTAL SHEETS 49 | SHEET NO. 27 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 62G71 | |

MODEL: Default
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NOTES:

1. AT DEPRESSED CURBS, ADJACENT TO CURB RAMPS ACCESSIBLE TO THE DISABLED, THE GUTTER FLAG SLOPE SHALL BE -5% UNLESS NOTED OTHERWISE.

LEGEND

- XX.XX' EXISTING LENGTH
- () EXISTING ELEVATION / SLOPE
- [Solid Grey Box] PROPOSED SIDEWALK
- [Dotted Box] DETECTABLE WARNINGS
- [Cross-hatched Box] SIDEWALK REMOVAL
REPLACE WITH TOPSOIL AND SOD
- [Diagonal Lines Box] DEPRESSED CURB AND GUTTER

REFERENCE BENCHMARK 3256 EL 592.599
BENCHMARK: CUT SQUARE N CORNER TCB
LOCATION: SE CORNER OF ADDISON ST AND ELSTON AVE



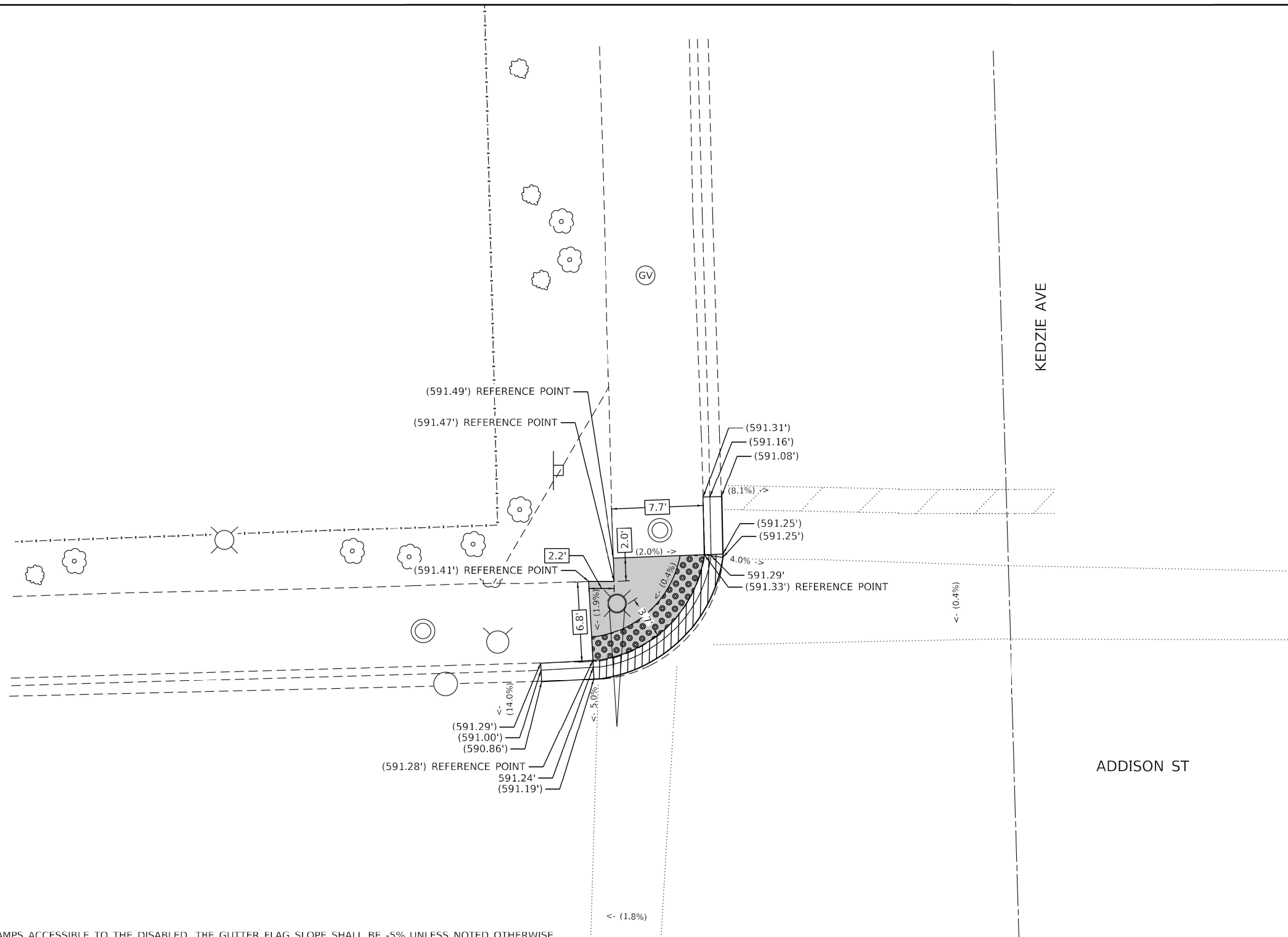
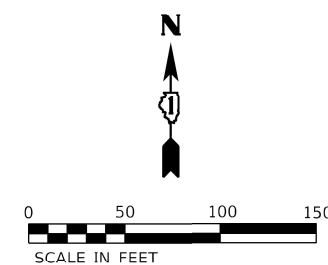
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| USER NAME = skierpiec | DESIGNED - SK | REVISED - |
| | DRAWN - SK | REVISED - |
| PLOT SCALE = 10.0000' / 1" | CHECKED - RG | REVISED - |
| PLOT DATE = 2/2/2022 | DATE - 02/01/2022 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS
SE CORNER ADDISON ST AND N ELSTON AVE**

| | | | | | | |
|--------|-------|----|--------|------|----|------|
| SCALE: | SHEET | OF | SHEETS | STA. | TO | STA. |
|--------|-------|----|--------|------|----|------|

| | | | | |
|---------------------------|------------------------|-------------|-----------------|--------------------|
| F.A.U. RTE. 1372 | SECTION 2018-019-RS-SW | COUNTY COOK | TOTAL SHEETS 49 | SHEET NO. 28 |
| | | | | CONTRACT NO. 62G71 |
| ILLINOIS FED. AID PROJECT | | | | |



NOTES:

1. AT DEPRESSED CURBS, ADJACENT TO CURB RAMP ACCESSIBLE TO THE DISABLED, THE GUTTER FLAG SLOPE SHALL BE -5% UNLESS NOTED OTHERWISE.

REFERENCE BENCHMARK 3954 EL 592.43 (NOT SHOWN)
 BENCHMARK: CUT SQUARE NW CORNER TCB
 LOCATION: SW CORNER OF ADDISON ST AND KEDZIE AVE

LEGEND

- XX.XX' EXISTING LENGTH
- () EXISTING ELEVATION / SLOPE
- [Solid Grey Box] PROPOSED SIDEWALK
- [Dotted Box] DETECTABLE WARNINGS
- [Cross-hatched Box] SIDEWALK REMOVAL
REPLACE WITH TOPSOIL AND SOD
- [Diagonal-hatched Box] DEPRESSED CURB AND GUTTER

MODEL: Default
 FILE NAME: 20220305.06 DOT D1 IPE 199.01.2 WO 6 Addison ADA Ramps.DGN
 Design: Prelim/Sheet: D162G71_Sht: Detail - ADA Ramp_06.dgn



| | | |
|----------------------------|-------------------|-----------|
| USER NAME = skierpiec | DESIGNED - SK | REVISED - |
| | DRAWN - SK | REVISED - |
| PLOT SCALE = 10.0000' / 1" | CHECKED - RG | REVISED - |
| PLOT DATE = 2/2/2022 | DATE - 02/01/2022 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

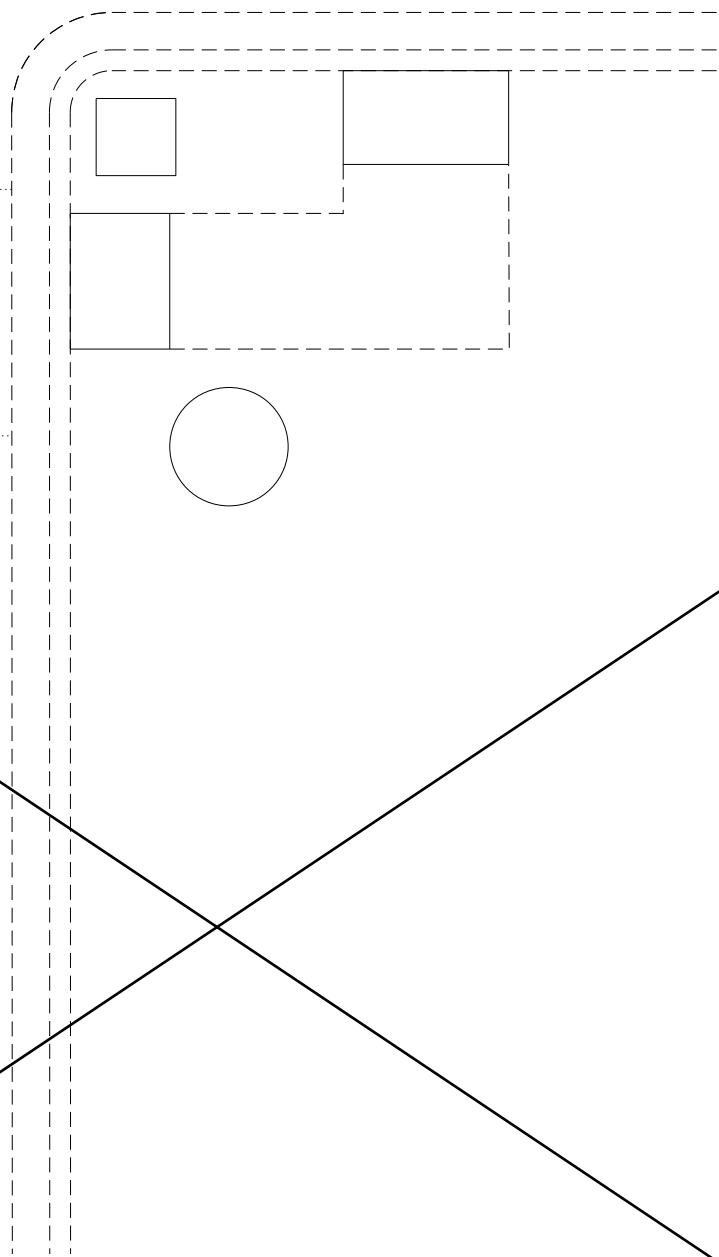
**ADA RAMP DETAILS
 NW CORNER ADDISON ST AND KEDZIE AVE**

SCALE: SHEET OF SHEETS STA. TO STA.

| | | | | |
|---------------------------|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1372 | 2018-019-RS-SW | COOK | 49 | 29 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 62G71 | |



KEDZIE AVE



DELETED LOCATION
ALREADY IMPROVED

NOTES

AT DEPRESSED CURBS, ADJACENT TO CURB RAMPS ACCESSABLE TO THE DISABLED, THE GUTTER FLAG SLOPE SHALL BE -5% UNLESS NOTED OTHERWISE

LEGEND

- XX.XX' EXISTING LENGTH
- DETECTABLE WARNING
- PROPOSED SIDEWALK
- SIDEWALK REMOVAL WITH TOPSOIL AND SOD
- DEPRESSED CURB AND GUTTER

MODEL: Default
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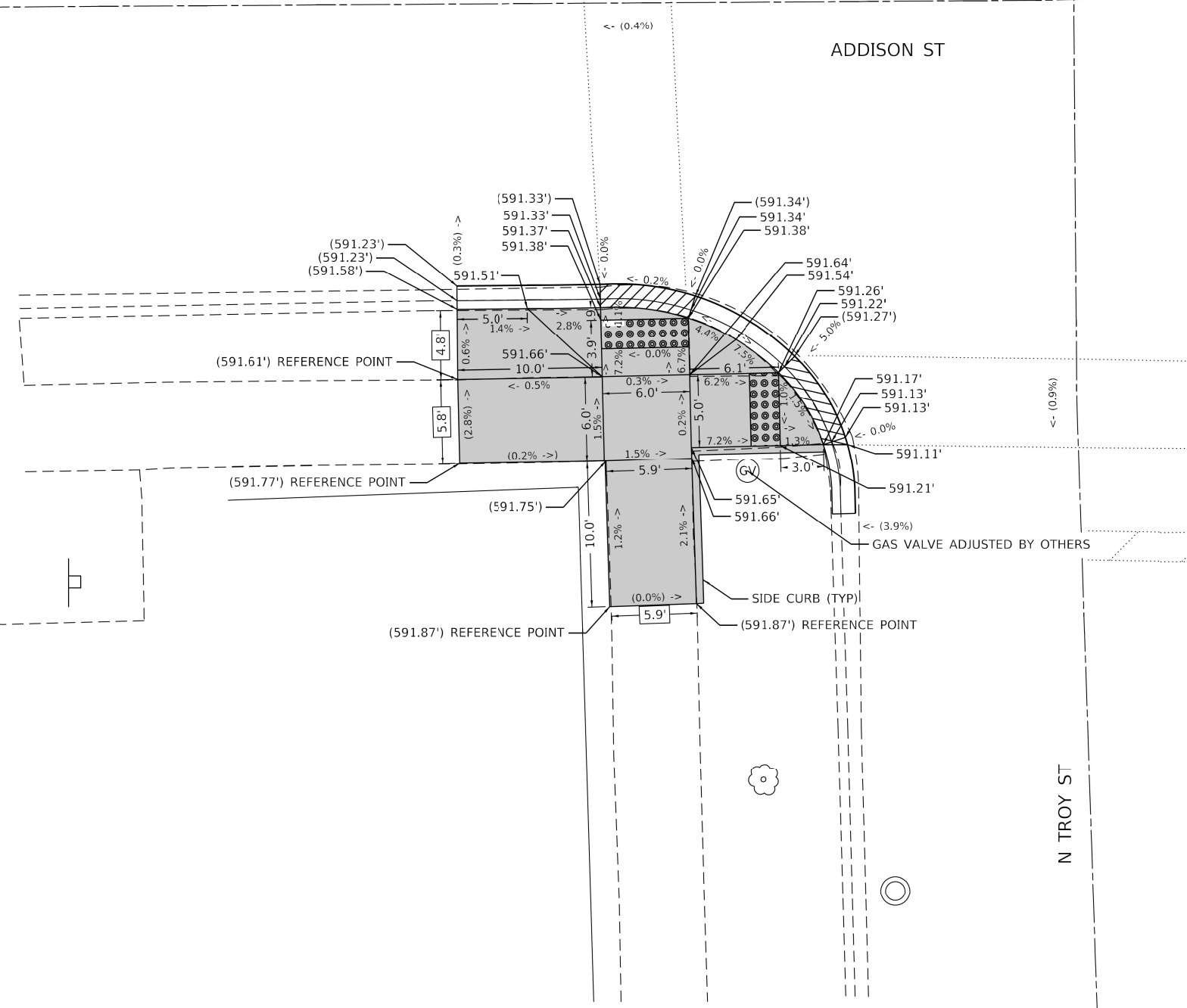
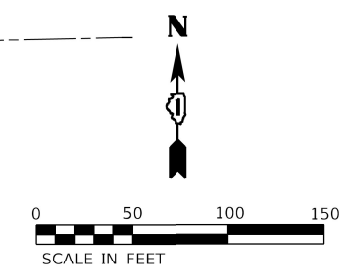
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|---------------------------|------------|-----------|
| USER NAME = mullanecd | DESIGNED - | REVISED - |
| | DRAWN - | REVISED - |
| PLOT SCALE = 1/4" = 1'-0" | CHECKED - | REVISED - |
| PLOT DATE = 2/7/2022 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS
NE CORNER OF DDISON ST AND KENNEDY WB ENTRANCE**

SCALE: SHEET OF SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------------|-----------|
| 1372 | 2018-019-RS-SW | COOK | 49 | 30 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 62G71 | |



NOTES:

1. AT DEPRESSED CURBS, ADJACENT TO CURB RAMPS ACCESSIBLE TO THE DISABLED, THE GUTTER FLAG SLOPE SHALL BE -5% UNLESS NOTED OTHERWISE.

LEGEND

- XX.XX' EXISTING LENGTH
- () EXISTING ELEVATION / SLOPE
- [Solid Gray Box] PROPOSED SIDEWALK
- [Grid Pattern Box] DETECTABLE WARNINGS
- [Cross-hatch Pattern Box] SIDEWALK REMOVAL
REPLACE WITH TOPSOIL AND SOD
- [Diagonal Hatch Pattern Box] DEPRESSED CURB AND GUTTER

REFERENCE BENCHMARK 4660 EL 593.11 (NOT SHOWN)
 BENCHMARK: NW BOLT ON FH
 LOCATION: NE CORNER OF ADDISON ST AND TROY ST

MODEL: Default
 FILE NAME: 20220305.06 DOT D1 IPE 199.012.WO B Addison ADA Ramps.DGN
 Design: Prelim/Plat/Sheet/D 162/GT1-Sht-detail-ADA-Ramp-07.dgn



| | | |
|----------------------------|-------------------|-----------|
| USER NAME = skierpiec | DESIGNED - SK | REVISED - |
| | DRAWN - SK | REVISED - |
| PLOT SCALE = 10.0000' / 1" | CHECKED - RG | REVISED - |
| PLOT DATE = 2/2/2022 | DATE - 02/01/2022 | REVISED - |

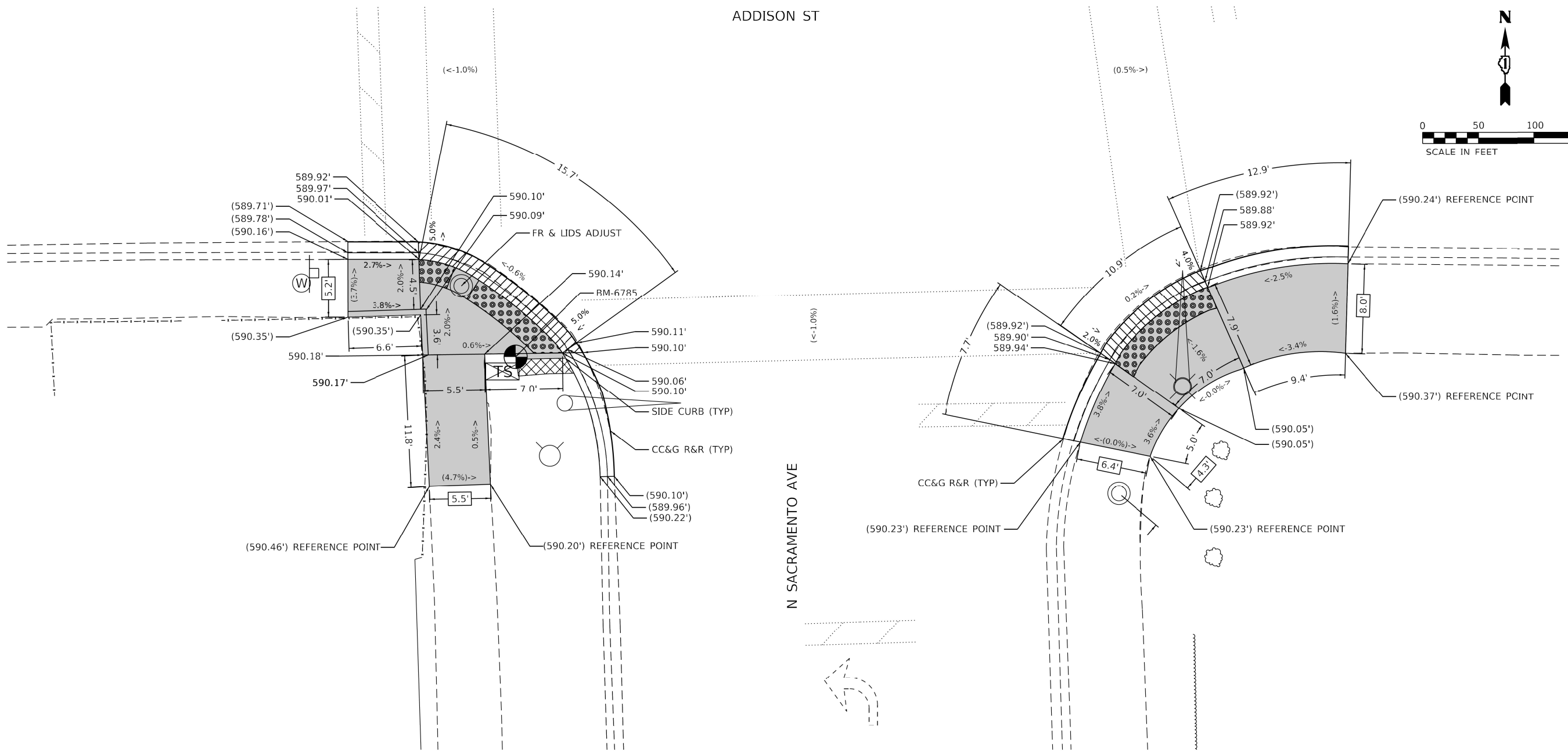
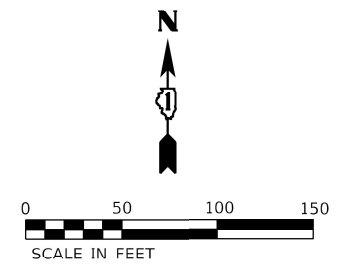
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS
SW, SE CORNERS ADDISON ST AND N TROY ST**

| | | | | | | |
|--------|-------|----|--------|------|----|------|
| SCALE: | SHEET | OF | SHEETS | STA. | TO | STA. |
|--------|-------|----|--------|------|----|------|

| | | | | |
|---------------------------|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1372 | 2018-019-RS-SW | COOK | 49 | 31 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 62G71 | |

ADDISON ST



1. AT DEPRESSED CURBS, ADJACENT TO CURB RAMPS ACCESSIBLE TO THE DISABLED, THE GUTTER FLAG SLOPE SHALL BE -5% UNLESS NOTED OTHERWISE.

REFERENCE BENCHMARK 6785 EL 591.781
 BENCHMARK: CUT SQUARE AT NE COR TCB
 LOCATION: SW CORNER ADDISON ST AND N SACRAMENTO AVE

LEGEND

- XX.XX' EXISTING LENGTH
- () EXISTING ELEVATION / SLOPE
- [Solid Grey Box] PROPOSED SIDEWALK
- [Dotted Box] DETECTABLE WARNINGS
- [Cross-hatched Box] SIDEWALK REMOVAL
REPLACE WITH TOPSOIL AND SOD
- [Diagonal-hatched Box] DEPRESSED CURB AND GUTTER



| | | |
|----------------------------|-------------------|-----------|
| USER NAME = skierpiec | DESIGNED - SDH | REVISED - |
| | DRAWN - SDH | REVISED - |
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| PLOT DATE = 2/2/2022 | DATE - 02/01/2022 | REVISED - |

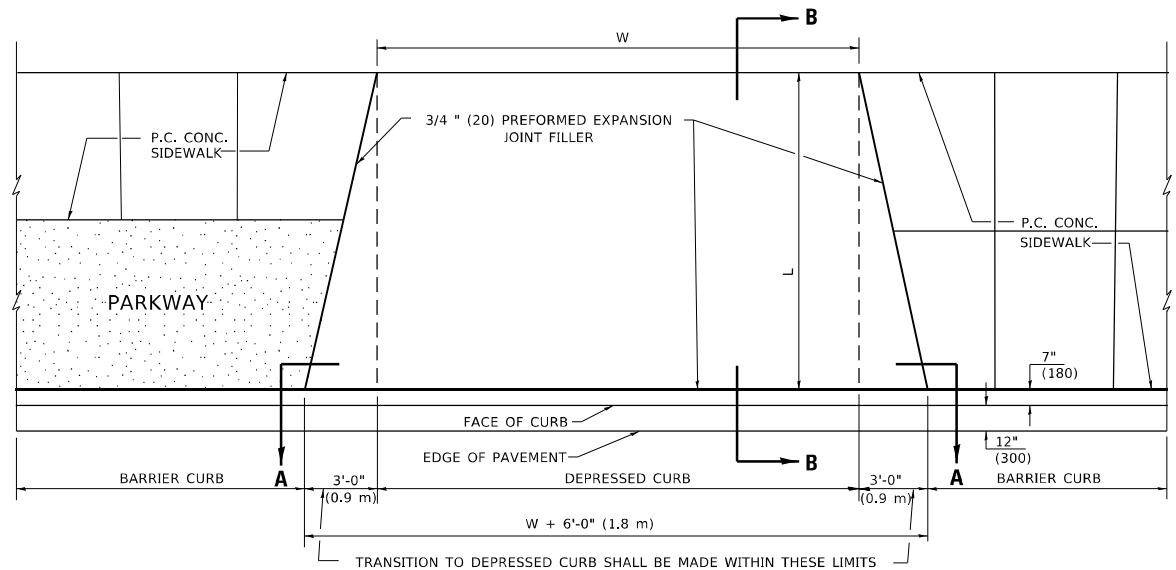
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS
SW, SE CORNERS ADDISON ST AND N SACRAMENTO AVE

| | | | | | | |
|--------|-------|----|--------|------|----|------|
| SCALE: | SHEET | OF | SHEETS | STA. | TO | STA. |
|--------|-------|----|--------|------|----|------|

| | | | | |
|---------------------------|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1372 | 2018-019-RS-SW | COOK | 49 | 32 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 62G71 | |

MODEL: Default
FILE NAME: 20220305.06 DOT D1 IPE 199.01.2 WO 6 Addison ADA Ramps.DGN
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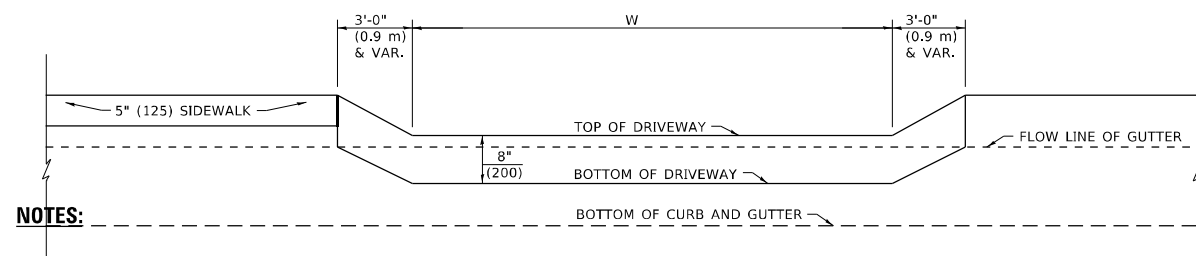
PLAN VIEW

NOTES:

1. EXPANSION JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE DETAILS FOR P.C.C. SIDEWALK.
2. THE CURB BETWEEN ADJACENT DRIVEWAYS SHALL BE FULL HEIGHT FOR A DISTANCE OF AT LEAST FOUR FEET (1.2 METERS).
3. P.C. CONCRETE DRIVEWAYS SHALL BE CONSTRUCTED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

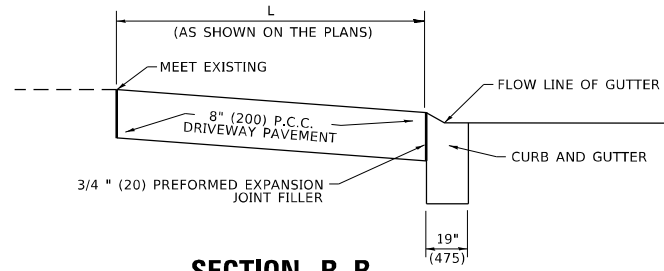
BASIS OF PAVEMENT

1. 3/4" (20) PREFORMED EXPANSION JOINTS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN P.C.C. DRIVEWAY PAVEMENT 8" (200).
2. COMBINATION CONC. CURB AND GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE TRANSITION CURB AND GUTTER.

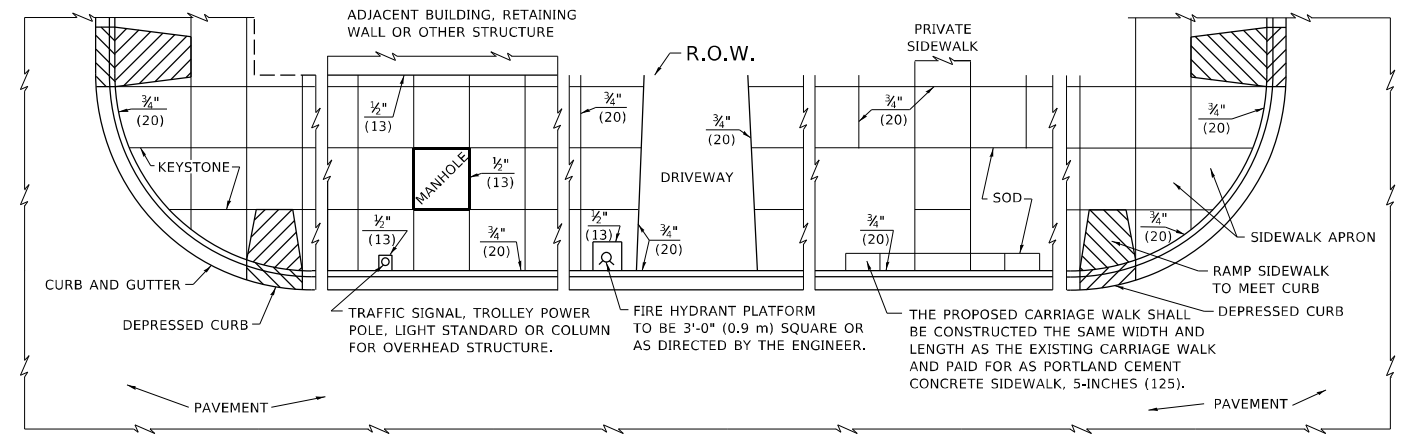


SECTION A-A

P.C.C. DRIVEWAY PAVEMENT DETAIL

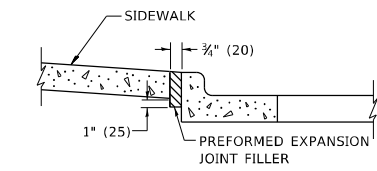


SECTION B-B



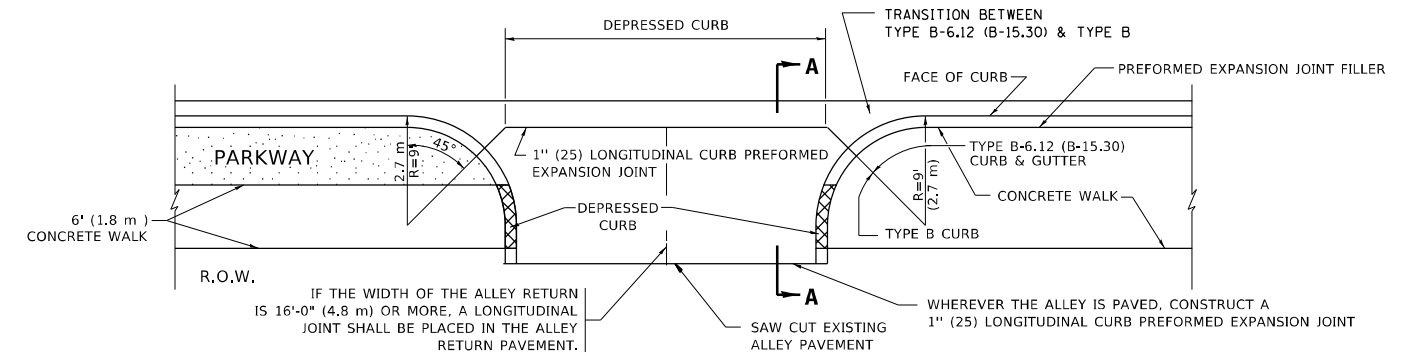
NOTES:

1. ONE-HALF INCH THICK EXPANSION JOINTS SHALL BE PLACED BETWEEN THE SIDEWALK AND ALL STRUCTURES SUCH AS LIGHT STANDARDS, TRAFFIC LIGHT STANDARDS, MANHOLES, WHICH EXTEND THROUGH THE SIDEWALK.
2. 3/4" (20) THICK EXPANSION JOINTS SHALL BE PLACED AT INTERVALS OF NOT MORE THAN 100 FEET (30 METERS) IN THE SIDEWALK. WHERE THE SIDEWALK IS CONSTRUCTED ADJACENT TO PAVEMENT OR CURB HAVING EXPANSION JOINTS, THE EXPANSION JOINTS IN THE SIDEWALK SHALL BE PLACED OPPOSITE THE EXISTING EXPANSION JOINTS AS NEARLY AS PRACTICABLE. EXPANSION JOINTS SHALL ALSO BE PLACED WHERE THE SIDEWALK ABUTS EXISTING SIDEWALKS, BETWEEN DRIVEWAY PAVEMENT AND SIDEWALK, AND BETWEEN SIDEWALK AND CURBS WHERE THE SIDEWALK ABUTS THE CURB.



SLOPE FOR SIDEWALK
1" (25) IN 3'-0" (0.9 m) IN CHICAGO

PORTLAND CEMENT CONCRETE SIDEWALK DETAILS



SECTION A-A

ALLEY RETURN DETAIL

BASIS OF PAVEMENT

1. SUB-BASE GRANULAR MATERIAL TYPE B SHALL BE PAID FOR SEPARATELY.
2. NO EXTRA COMPENSATION SHALL BE ALLOWED FOR THE GUTTER FLARE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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| USER NAME = nullanecd | DESIGNED - M. DE YONG | REVISED - K. SMITH 02-01-22 |
| PLOT SCALE = 100,0001' / in. | CHECKED - | REVISED - |
| PLOT DATE = 2/7/2022 | DATE - 06-13-90 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | | |
|--|---------------------|--------------------------|
| CITY OF CHICAGO | | |
| DETAILS FOR P.C. CONCRETE DRIVEWAY, ALLEY RETURN AND SIDEWALK | | |
| SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. 11+43 TO STA. 57+12 |

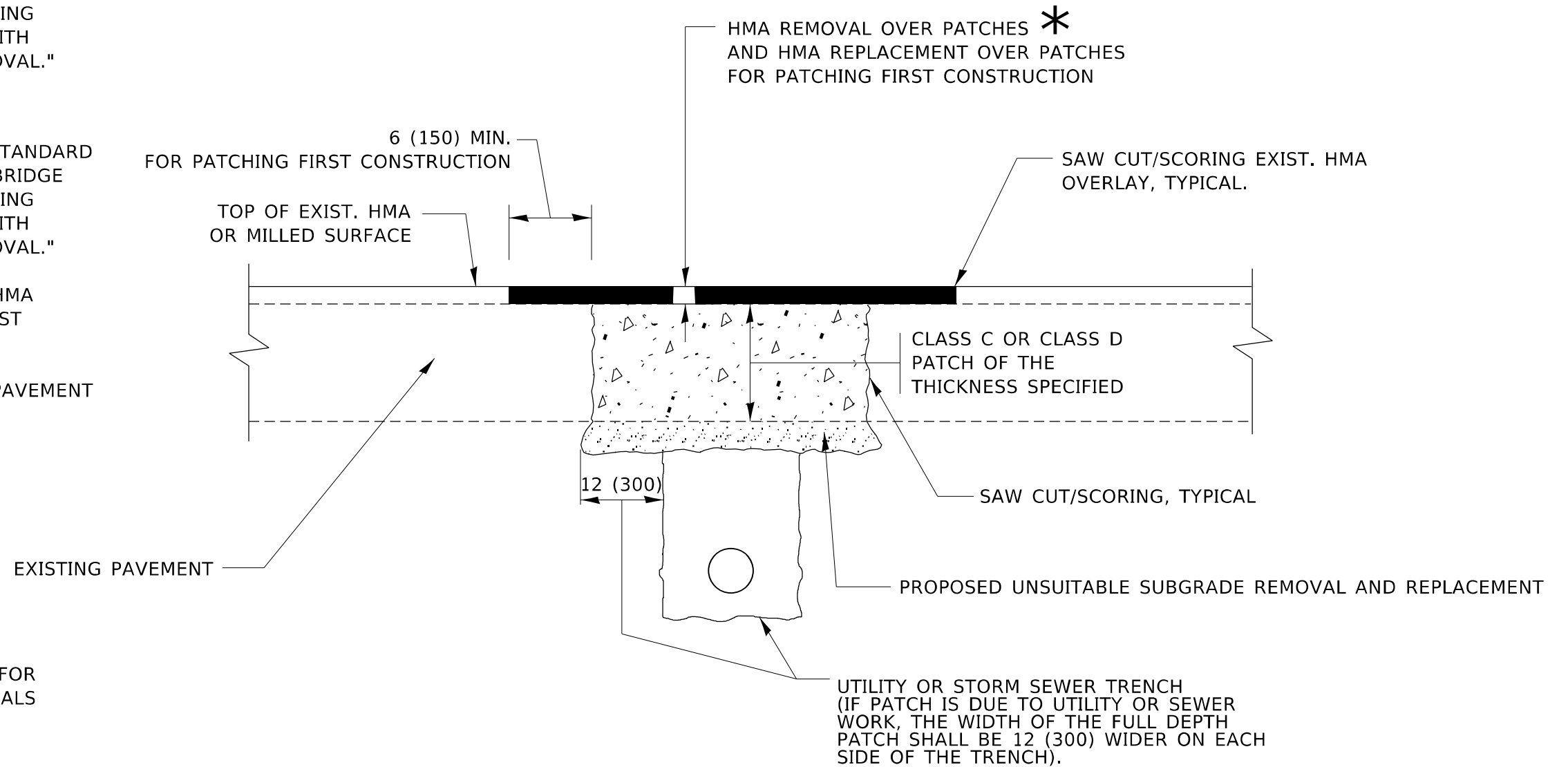
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|---------------------------|----------------|--------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1372 | 2018-019-R5-SW | COOK | 49 | 34 |
| BD400-03 | (BD-17) | CONTRACT NO. | 62G71 | |
| ILLINOIS FED. AID PROJECT | | | | |

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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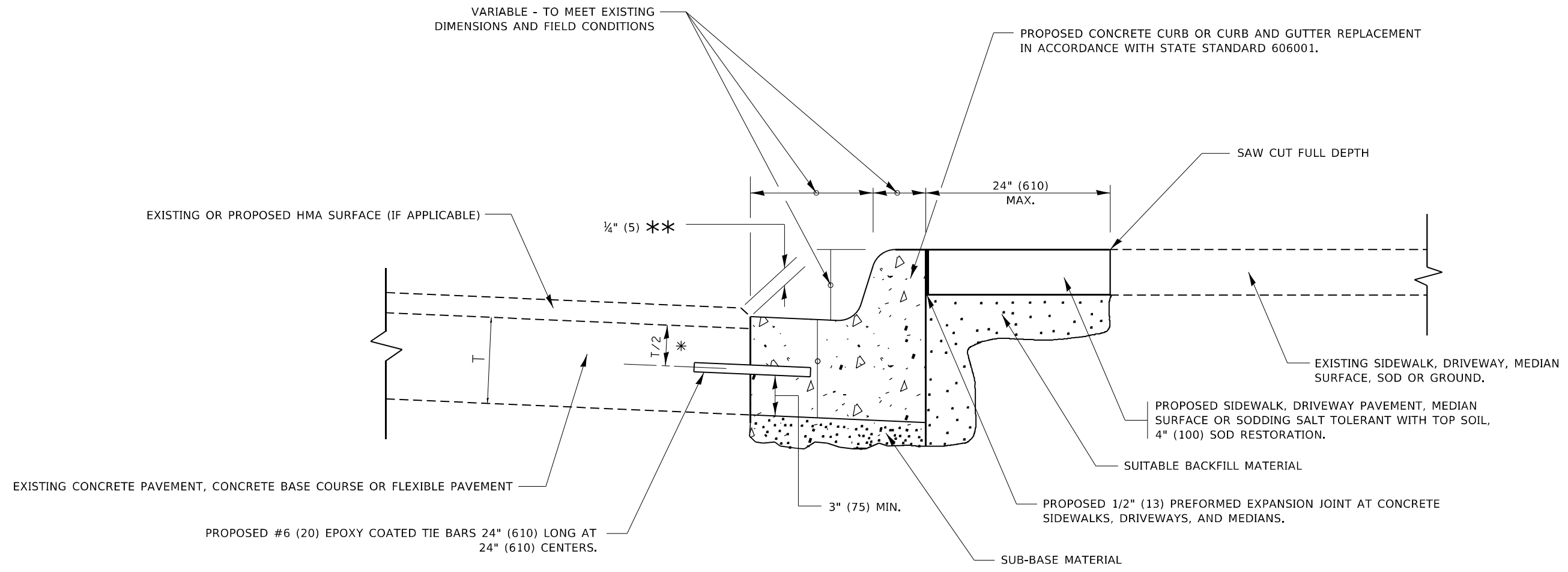
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|------------------------------|--------------------|-----------------------------|
| USER NAME = mullanecd | DESIGNED - R. SHAH | REVISED - R. BORO 01-01-07 |
| PLOT SCALE = 100,0001' / in. | CHECKED - | REVISED - R. BORO 09-04-07 |
| PLOT DATE = 2/7/2022 | DATE - 10-25-94 | REVISED - K. ENG 10-27-08 |
| | | REVISED - K. SMITH 02-01-22 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. 11+43 TO STA. 57+12

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------------------|--------------|-----------|
| 1372 | 2018-019-R5-SW | COOK | 49 | 35 |
| BD400-04 (BD-22) | | CONTRACT NO. 62G71 | | |
| ILLINOIS FED. AID PROJECT | | | | |



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
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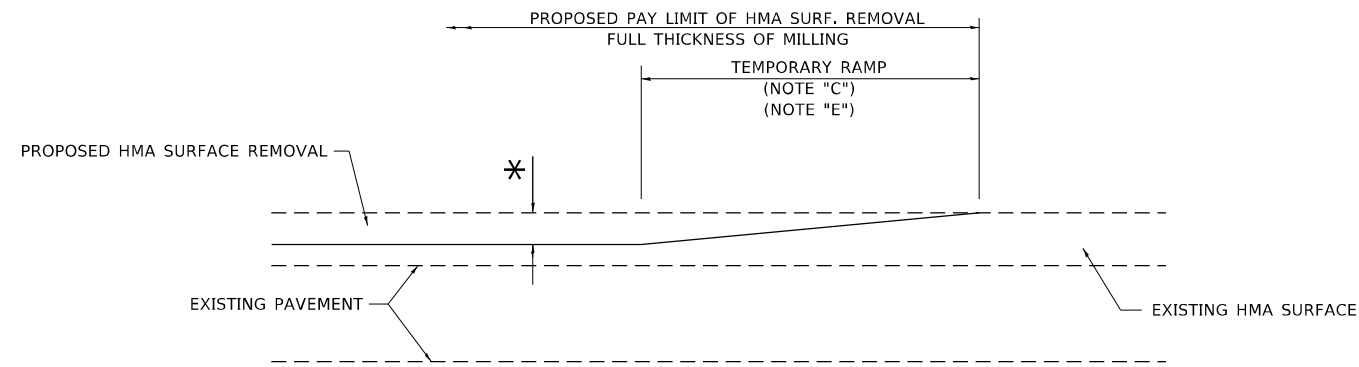
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| USER NAME = nullanecd | DESIGNED - A. HOUSEH | REVISED - A. ABBAS 03-21-97 |
| | DRAWN - | REVISED - M. GOMEZ 01-22-01 |
| PLOT SCALE = 100,0001' / in. | CHECKED - | REVISED - R. BORO 12-15-09 |
| PLOT DATE = 2/7/2022 | DATE - 03-11-94 | REVISED - K. SMITH 07-11-19 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

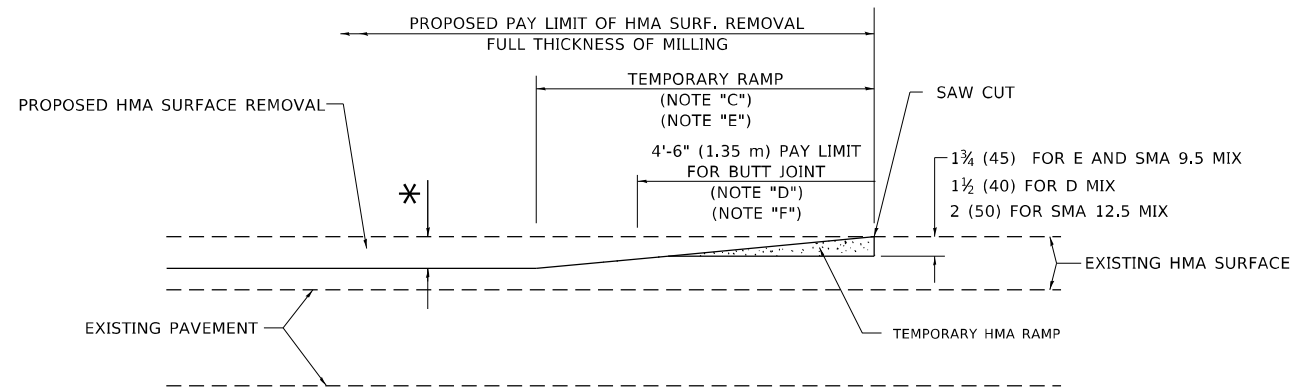
SCALE: NONE SHEET 1 OF 1 SHEETS STA. 11+43 TO STA. 57+12

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------------------|--------------|-----------|
| 1372 | 2018-019-R5-SW | COOK | 49 | 36 |
| BD600-06 (BD-24) | | CONTRACT NO. 62G71 | | |
| ILLINOIS FED. AID PROJECT | | | | |



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

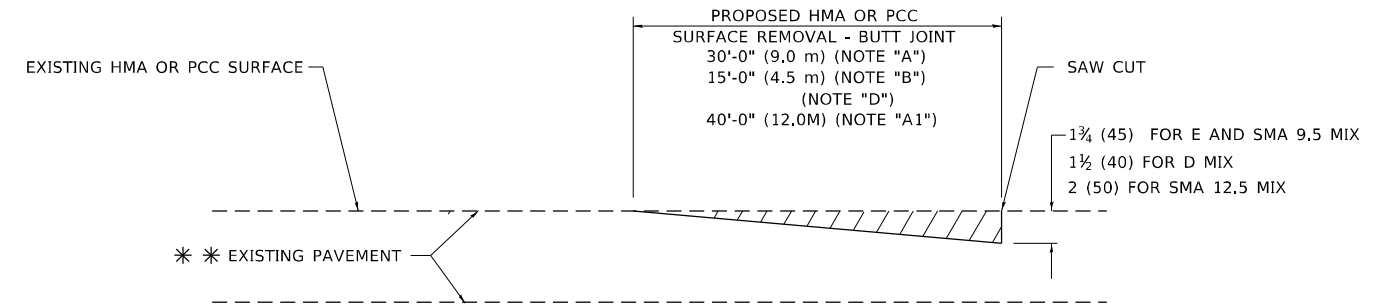
OPTION 1



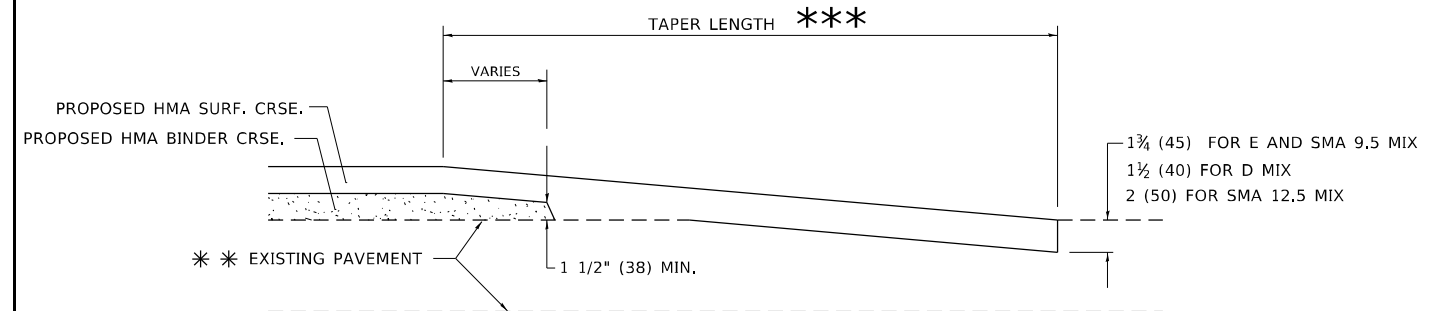
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

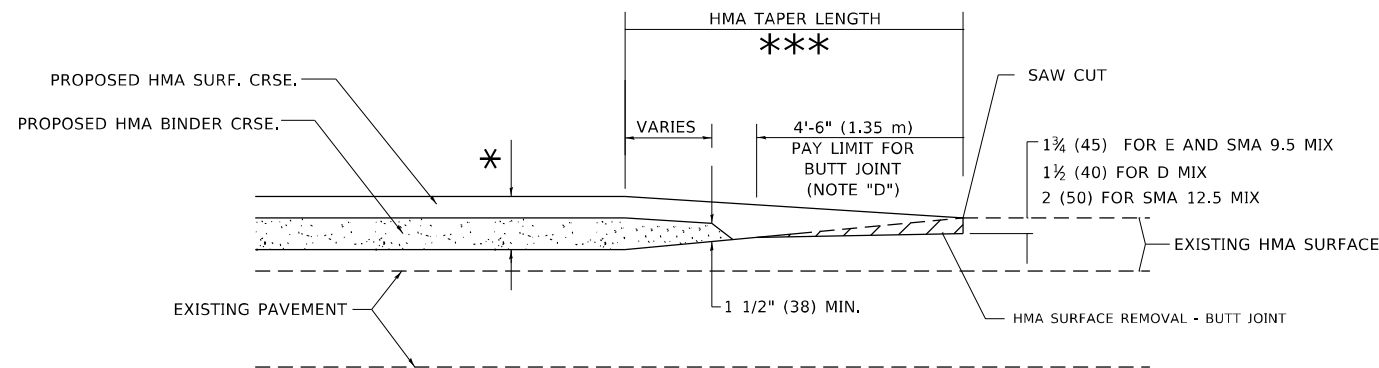
GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

MODEL: Default
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| | | |
|------------------------------|-----------------------|-----------------------------|
| USER NAME = mullanecd | DESIGNED - M. DE YONG | REVISED - A. ABBAS 03-21-97 |
| | DRAWN - | REVISED - M. GOMEZ 04-06-01 |
| PLOT SCALE = 100,0001' / in. | CHECKED - | REVISED - R. BORO 01-01-07 |
| PLOT DATE = 2/7/2022 | DATE - 06-13-90 | REVISED - K. SMITH 02-01-22 |

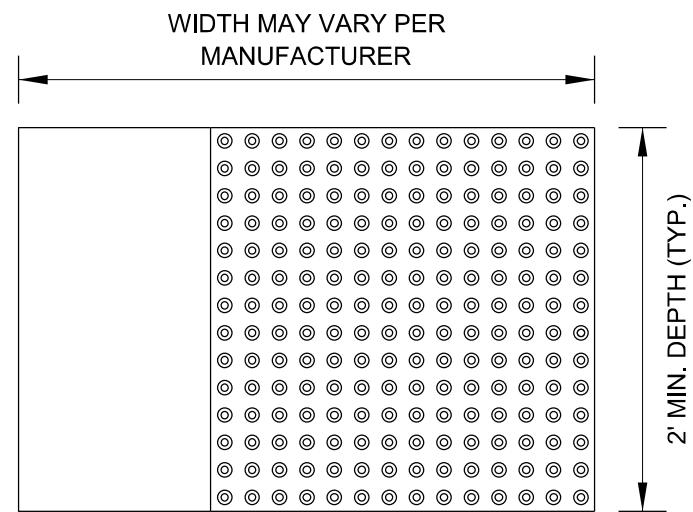
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

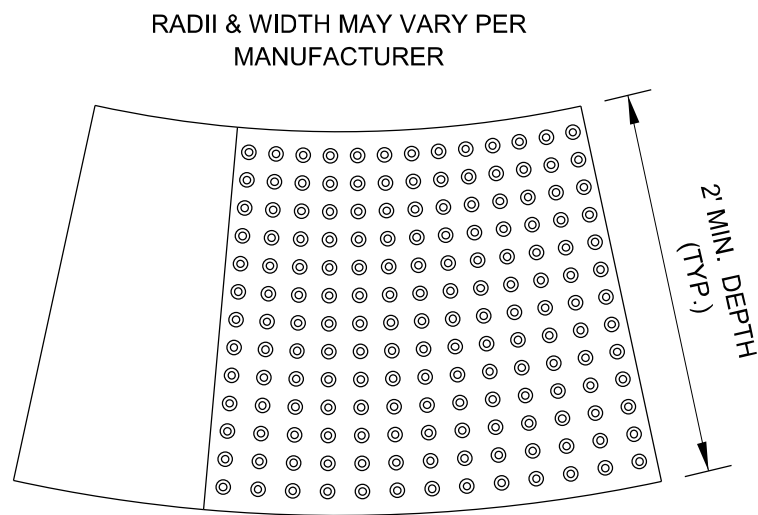
SCALE: NONE SHEET 1 OF 1 SHEETS STA. 11+43 TO STA. 57+12

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------------------|--------------|-----------|
| 1372 | 2018-019-R5-SW | COOK | 49 | 37 |
| BD400-05 BD-32 | | CONTRACT NO. 62G71 | | |
| ILLINOIS FED. AID PROJECT | | | | |

STRAIGHT DETECTABLE WARNING UNITS



RADIAL DETECTABLE WARNING UNITS

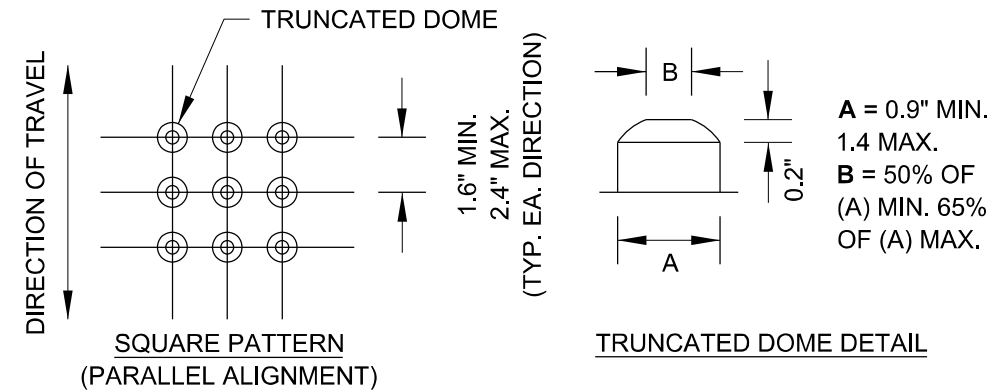


DETECTABLE WARNING UNIT SIZES

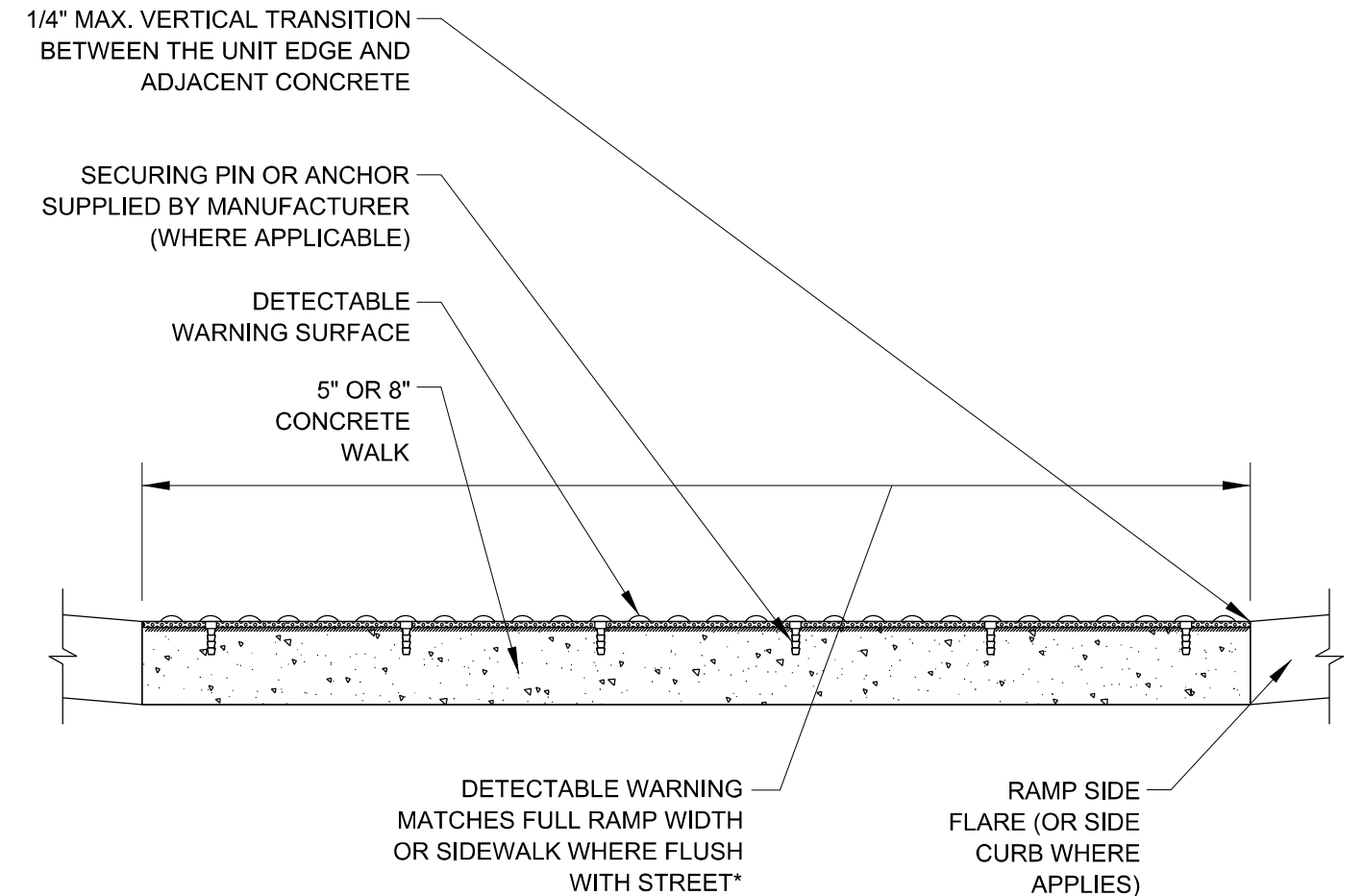
- VERIFY ALL DIMENSIONS WITH THE PRODUCT MANUFACTURER.
- IF USING RADIAL UNITS, VERIFY THAT THE CURB RADIUS MATCHES AVAILABLE UNIT RADII WITH THE PRODUCT MANUFACTURER.

GENERAL NOTE:

THE ROWS OF DOMES IN THE DETECTABLE WARNING MATERIAL MUST BE ALIGNED WITH THE PATH OF WHEELCHAIR TRAVEL WHICH IS REQUIRED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE BOTTOM OF THE RAMP TO PERMIT TRACKING BETWEEN DOME ROWS. ON BLENDED TRANSITIONS OR FLUSH TRANSITIONS, WHERE RADIAL UNITS ARE SITUATED ABOUT THE CURB RADIUS, DOME ORIENTATION IS NOT SIGNIFICANT.



UNIT PATTERN & DOME DETAIL



*A BORDER OF 2 INCHES OR LESS AROUND THE DETECTABLE WARNING SURFACE IS ACCEPTABLE IF REQUIRED FOR PROPER INSTALLATION OF THE DETECTABLE WARNING SURFACE PRODUCT

DETECTABLE WARNING UNIT SECTION

MODEL: Default
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|------------------------------|-------------------|-----------|
| USER NAME = mullanecd | DESIGNED - | REVISED - |
| | DRAWN - | REVISED - |
| PLOT SCALE = 100,0001' / in. | CHECKED - | REVISED - |
| PLOT DATE = 2/7/2022 | DATE - 06-20-2017 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
DETECTABLE WARNINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. 11+43 TO STA. 57+12

| | | | | |
|---------------------------|------------------------|--------------------|-----------------|--------------|
| F.A.U. RTE. 1372 | SECTION 2018-019-RS-SW | COUNTY COOK | TOTAL SHEETS 49 | SHEET NO. 38 |
| BD 58 | | CONTRACT NO. 62G71 | | |
| ILLINOIS FED. AID PROJECT | | | | |

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

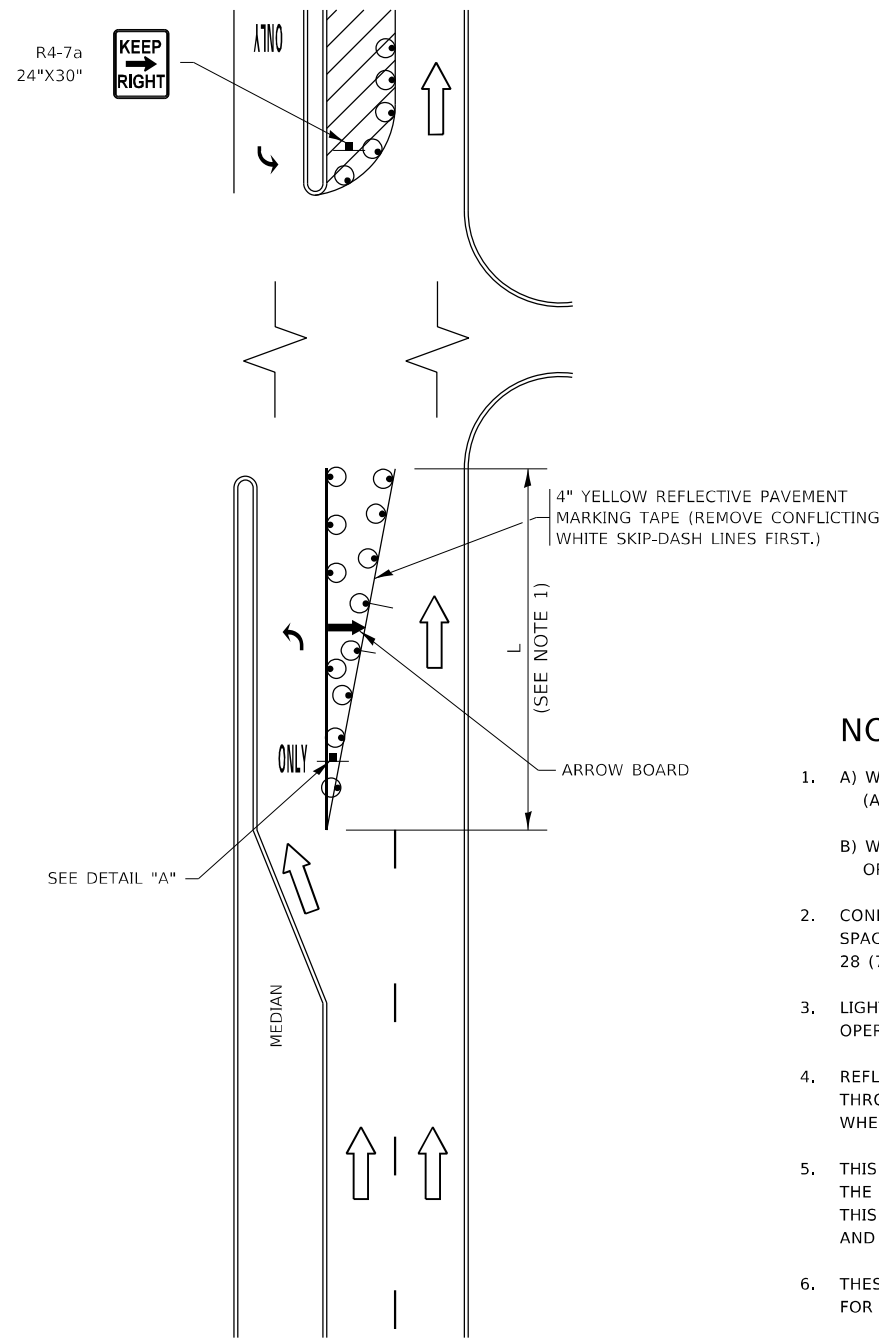


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

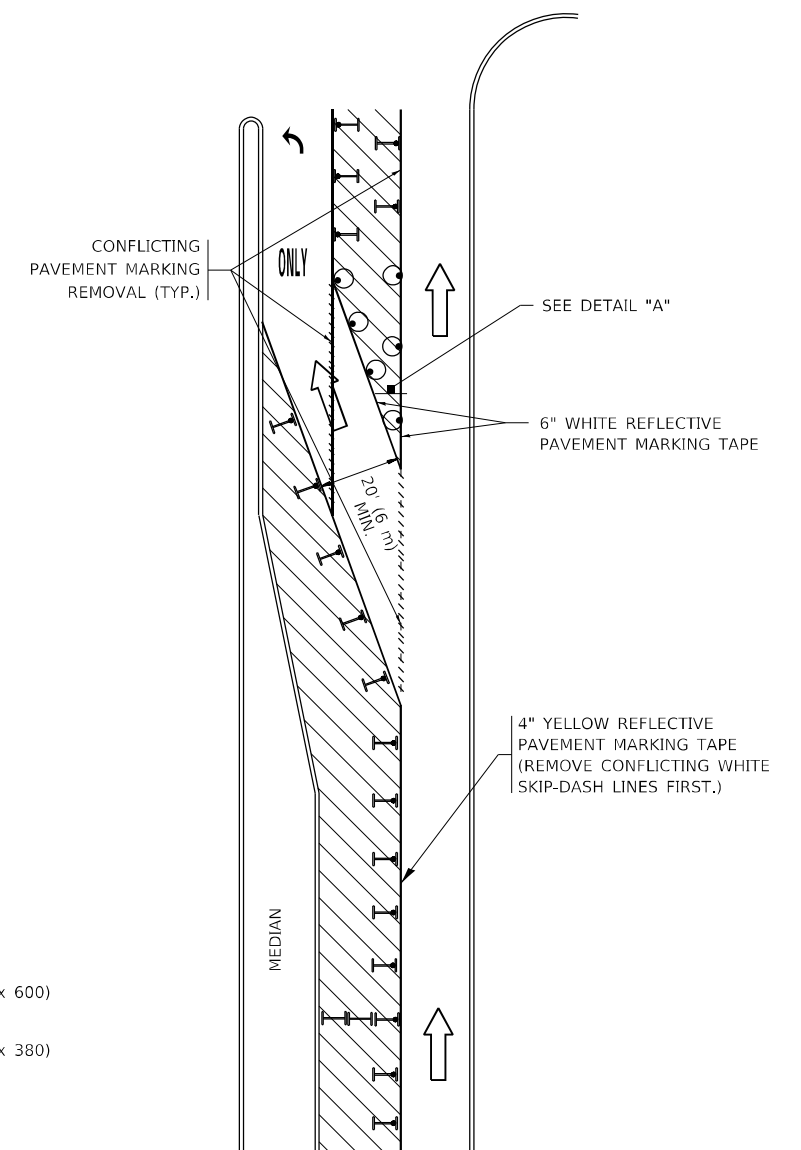
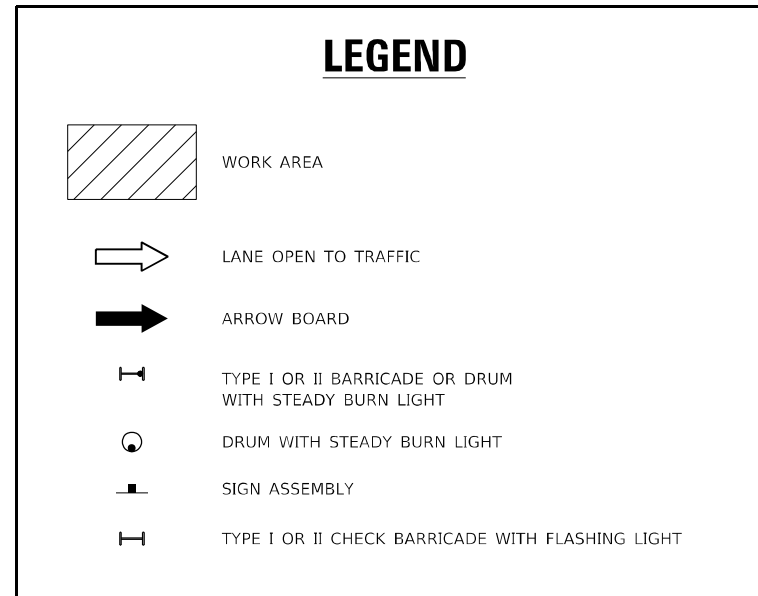
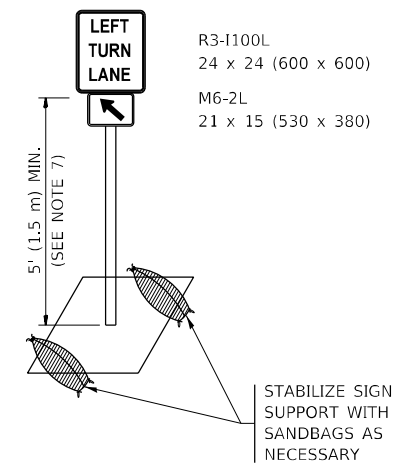


FIGURE 2



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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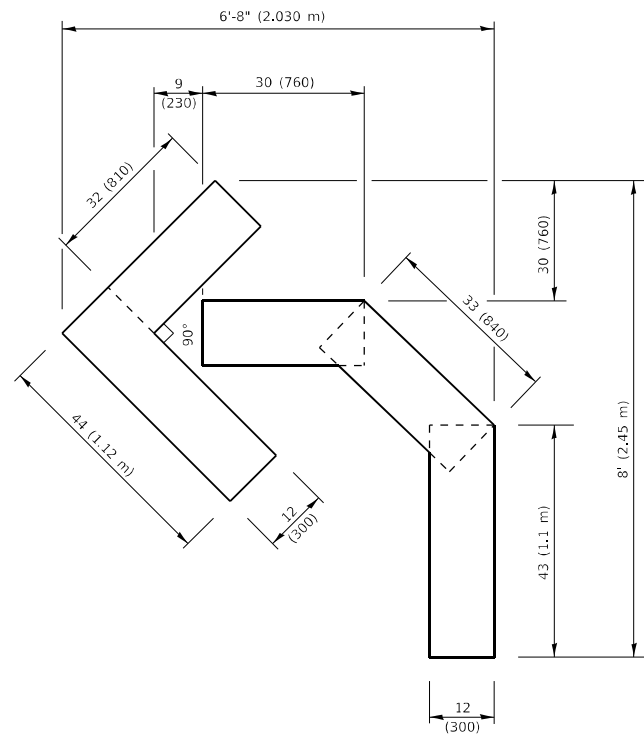
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|------------------------------|----------------------------------|--------------------------------|
| USER NAME = nullanecd | DESIGNED - T. RAMMACHER 09-08-94 | REVISED - R. BORO 09-14-09 |
| | DRAWN - A. HOUSEH 11-07-95 | REVISED - A. SCHUETZE 07-01-13 |
| PLOT SCALE = 100,0000' / in. | CHECKED - A. HOUSEH 10-12-96 | REVISED - A. SCHUETZE 09-15-16 |
| PLOT DATE = 2/7/2022 | DATE - T. RAMMACHER 01-06-00 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

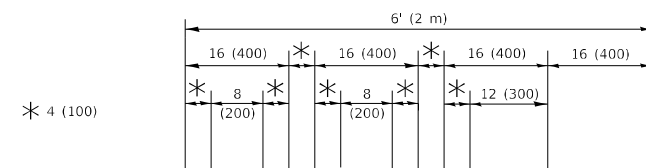
SCALE: NONE SHEET 1 OF 1 SHEETS STA. 11+43 TO STA. 57+12

| | | | | |
|---------------------------|----------------|--------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1372 | 2018-019-R5-SW | COOK | 49 | 40 |
| TC-14 | | CONTRACT NO. 62G71 | | |
| ILLINOIS FED. AID PROJECT | | | | |



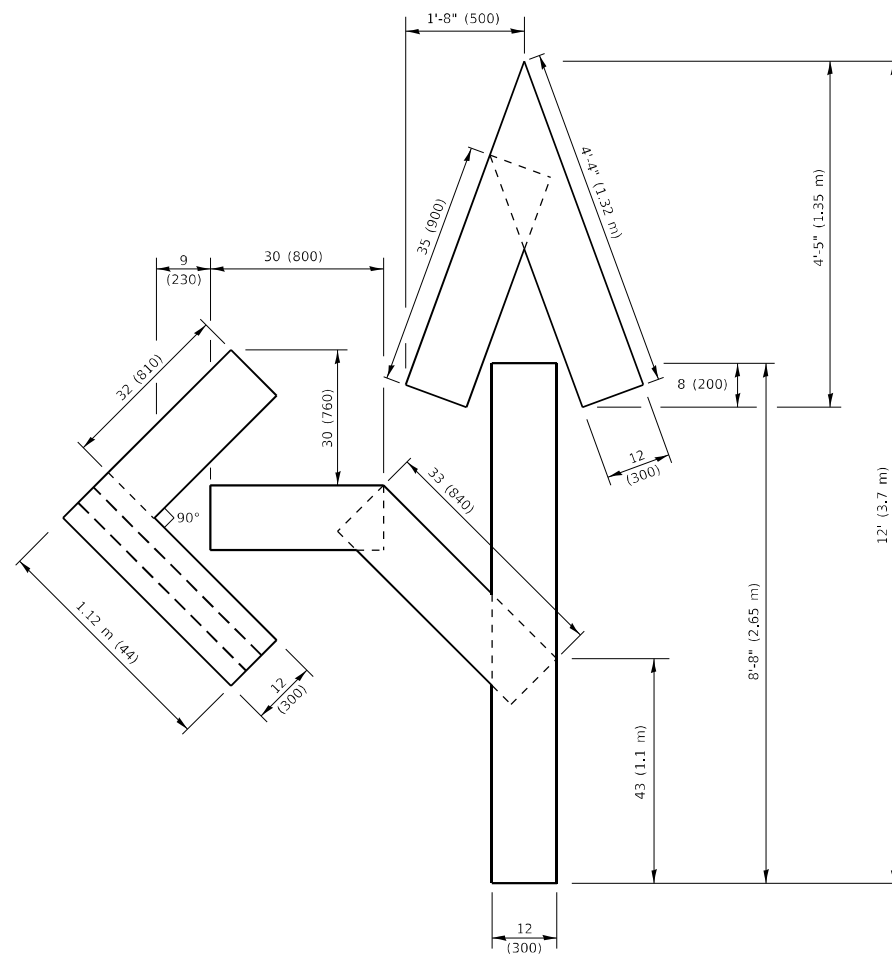
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

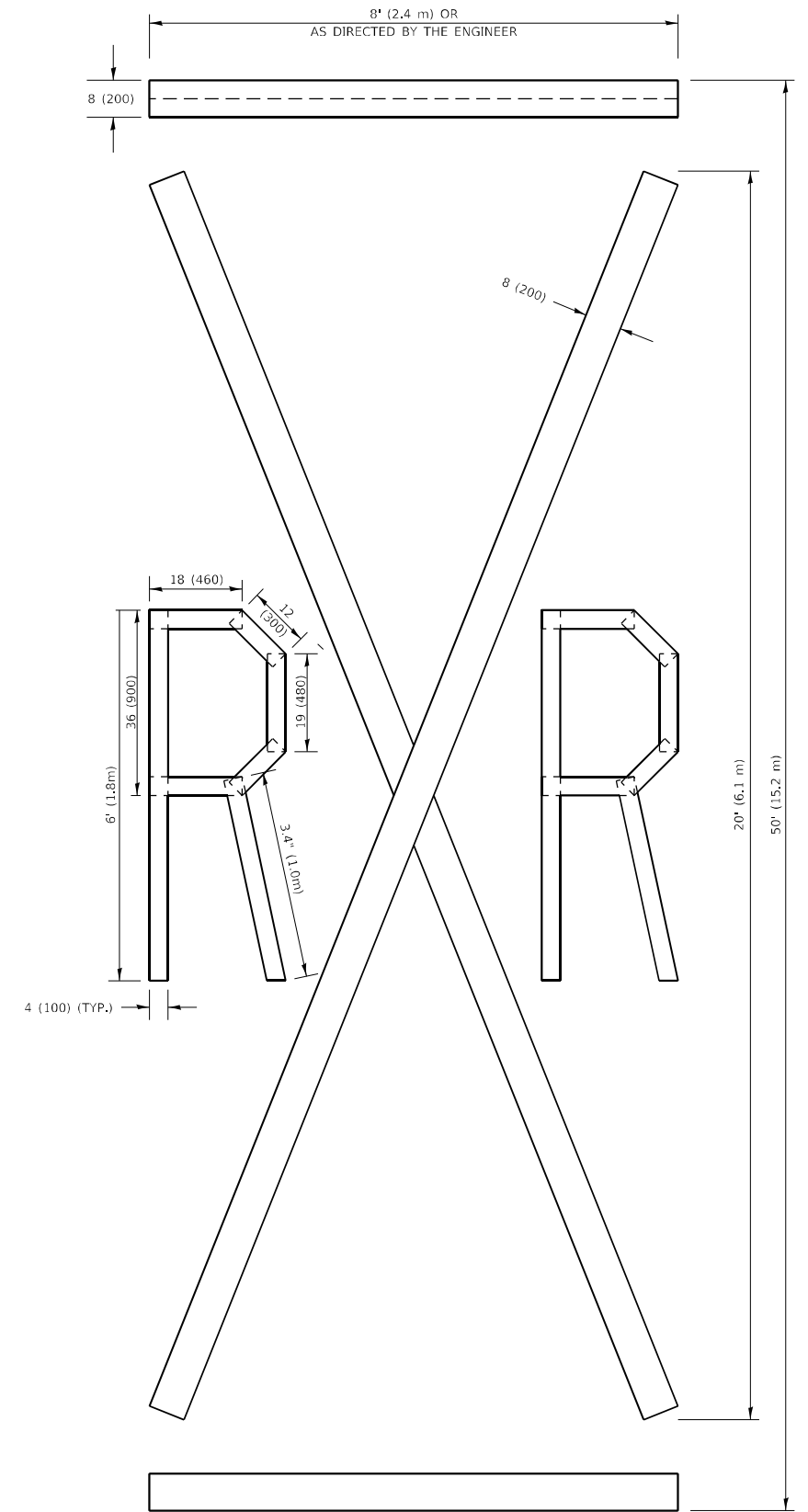


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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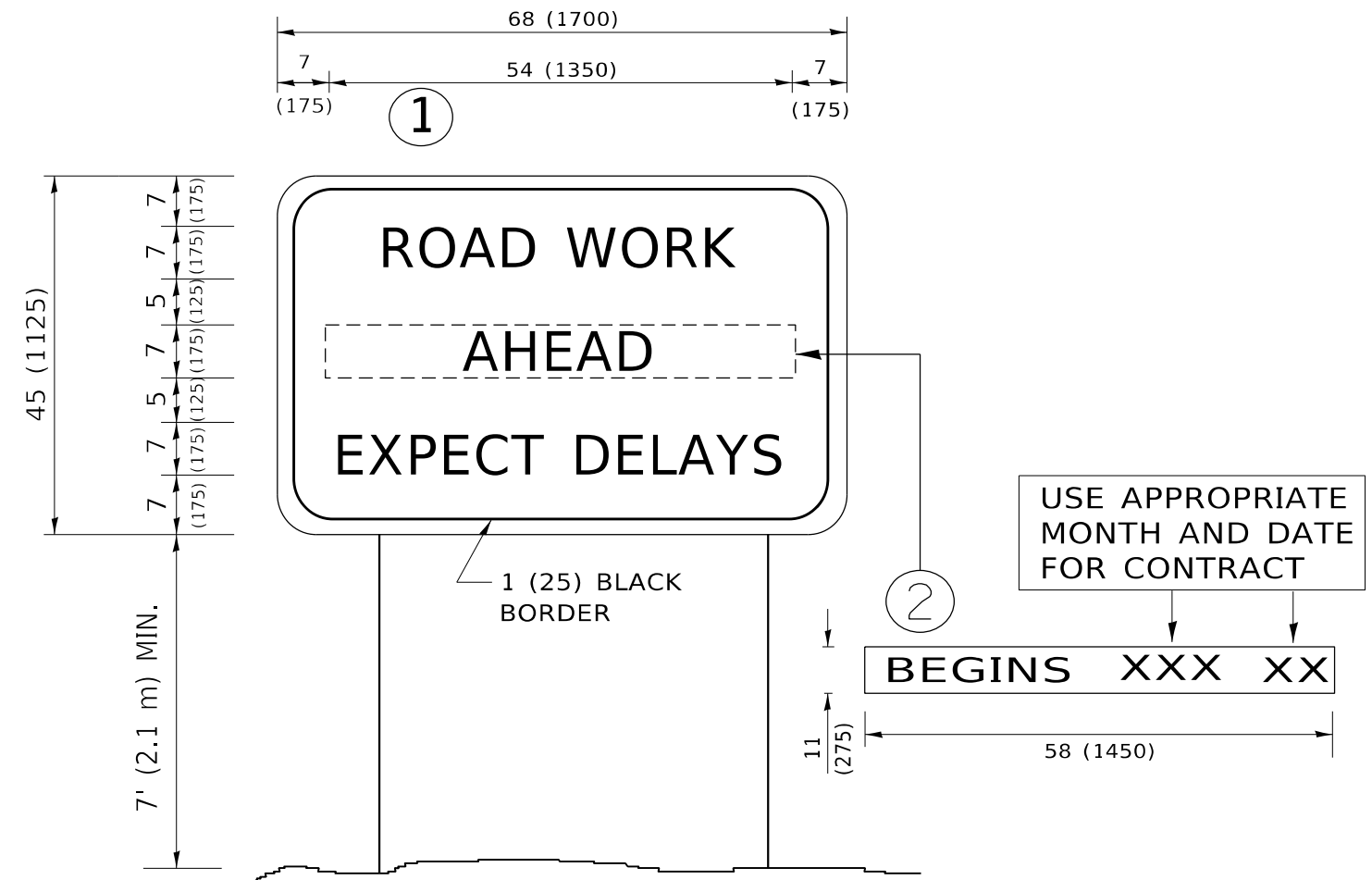
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| USER NAME = nullanecd | DESIGNED - | REVISED - T. RAMMACHER 03-02-98 |
| | DRAWN - | REVISED - E. GOMEZ 08-28-00 |
| PLOT SCALE = 100,0010' / in. | CHECKED - | REVISED - E. GOMEZ 08-28-00 |
| PLOT DATE = 2/7/2022 | DATE - 09-18-94 | REVISED - A. SCHUETZE 09-15-16 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. 11+43 TO STA. 57+12

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------------------|--------------|-----------|
| 1372 | 2018-019-RS-SW | COOK | 49 | 41 |
| TC-16 | | CONTRACT NO. 62G71 | | |
| ILLINOIS FED. AID PROJECT | | | | |



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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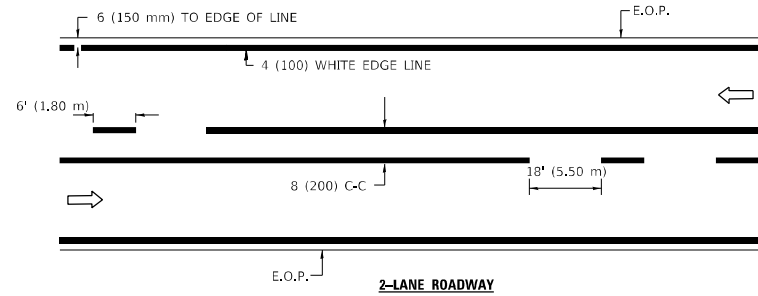
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| USER NAME = nullanecd | DESIGNED - | REVISED - R. MIRS 09-15-97 |
| | DRAWN - | REVISED - R. MIRS 12-11-97 |
| PLOT SCALE = 100,0000 ' / in. | CHECKED - | REVISED - T. RAMMACHER 02-02-99 |
| PLOT DATE = 2/7/2022 | DATE - | REVISED - C. JUCIUS 01-31-07 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

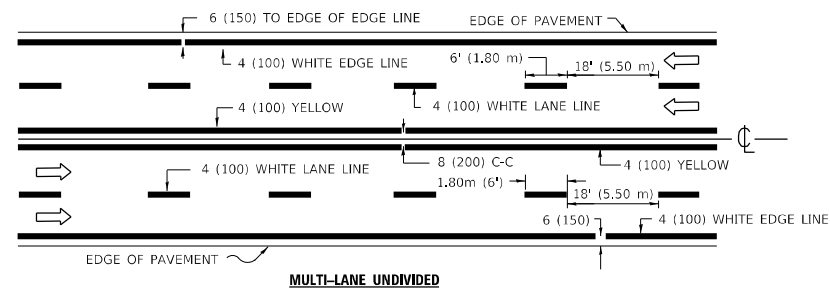
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. 11+43 TO STA. 57+12

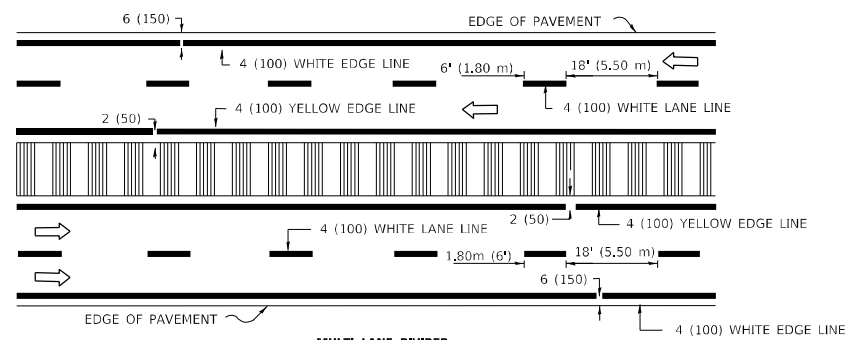
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------------|-----------|
| 1372 | 2018-019-R5-SW | COOK | 49 | 42 |
| TC-22 | | | CONTRACT NO. 62G71 | |
| ILLINOIS FED. AID PROJECT | | | | |



2-LANE ROADWAY



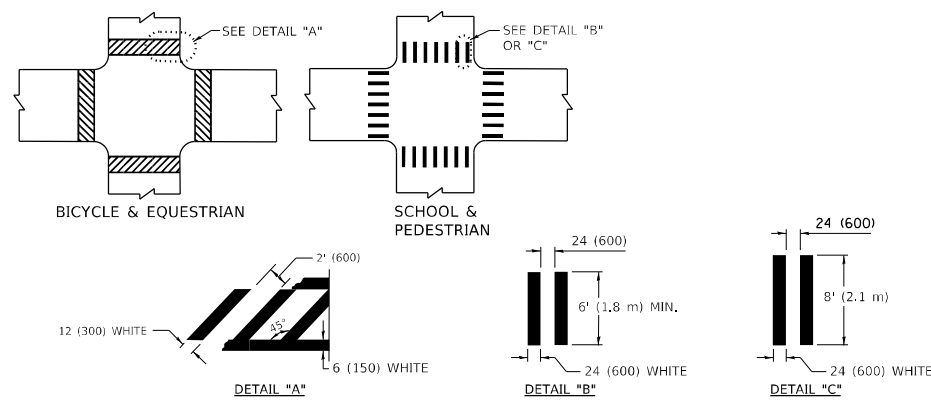
MULTI-LANE UNDIVIDED



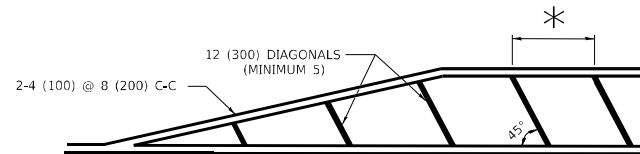
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



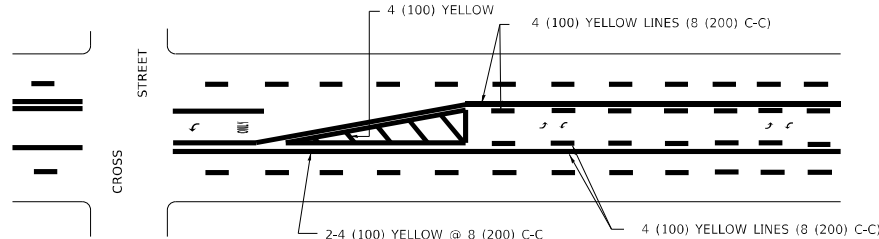
TYPICAL CROSSWALK MARKING



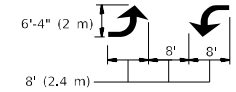
* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

* DIAGONAL LINE SPACING: 20' (6.1 m) C-C

PAINTED MEDIANS

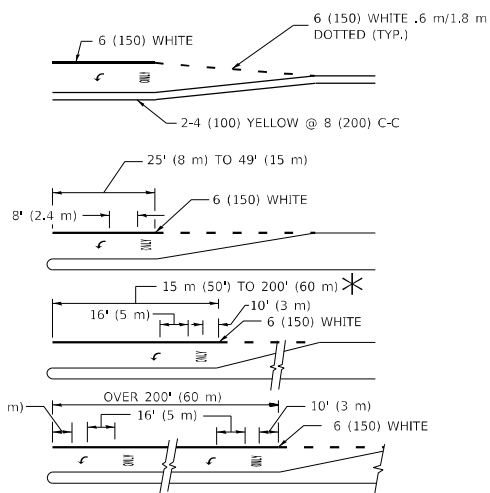


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

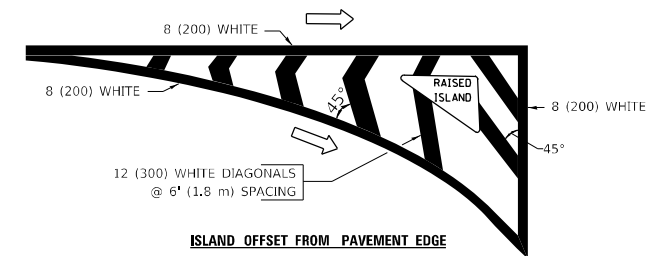


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)

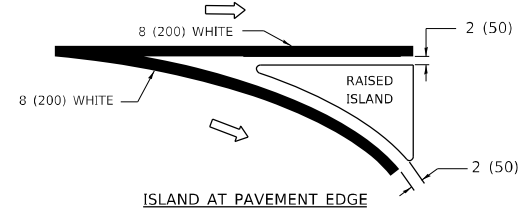
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|--|--|---------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 6' (1.80 m) LINE WITH 18' (5.50 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 8 (200) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS | 4 (100) 2 @ 4 (100) | SOLID SOLID | YELLOW YELLOW | 8 (200) C-C |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 6' (1.80 m) LINE WITH 18' (5.50 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES A. DIAGONAL (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN) | 12 (300) @ 45° 24 (600) @ 90° | SOLID SOLID | WHITE WHITE | 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° | SOLID | YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC | 8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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|------------------------------|------------|---------------------------------|
| USER NAME = mullanecd | DESIGNED - | REVISED - T. RAMMACHER 12-07-00 |
| | DRAWN - | REVISED - K. ENG 02-28-12 |
| PLOT SCALE = 100,0000' / in. | CHECKED - | REVISED - |
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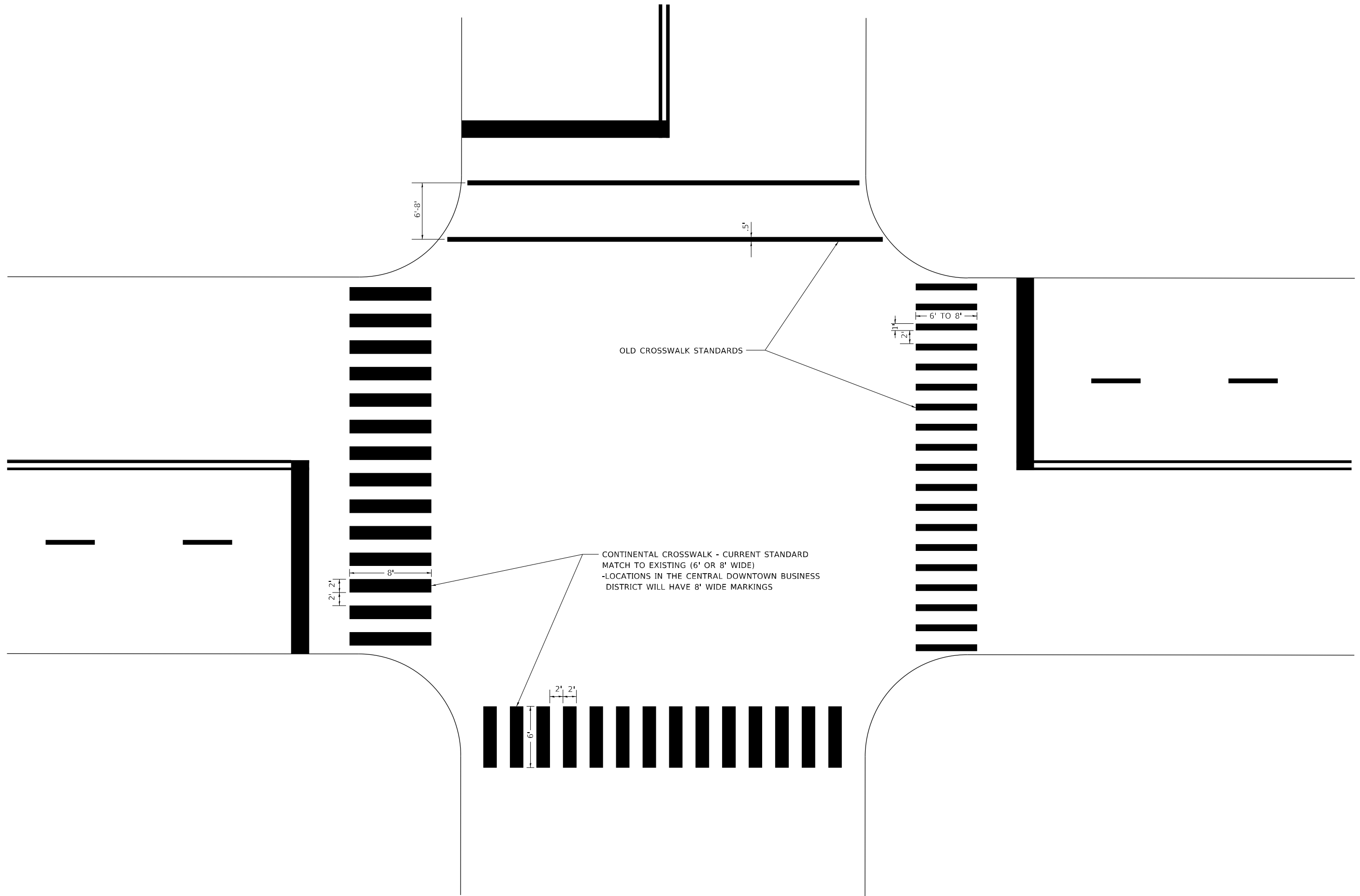
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 3 SHEETS STA. 11+43 TO STA. 57+12

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------------|----------------|--------------------|--------------|-----------|
| 1372 | 2018-019-RS-SW | COOK | 49 | 43 |
| TC-24 | | CONTRACT NO. 62G71 | | |
| ILLINOIS / FED. AID PROJECT | | | | |

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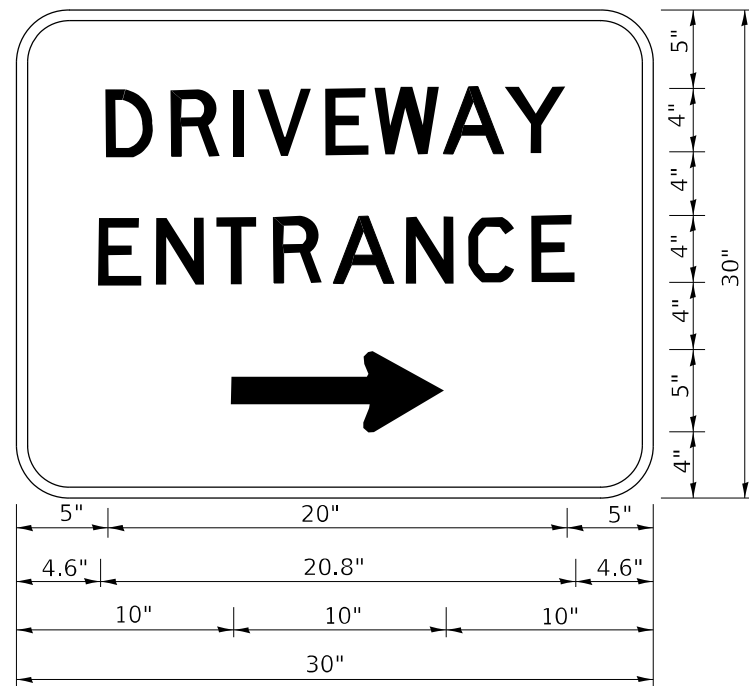
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| | DRAWN - | REVISED - |
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| PLOT DATE = 2/7/2022 | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO
 TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 3 OF 3 SHEETS STA. 11+43 TO STA. 57+12

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------------------|--------------|-----------|
| 1372 | 2018-019-RS-SW | COOK | 49 | 45 |
| TC-24 | | CONTRACT NO. 62G71 | | |
| ILLINOIS FED. AID PROJECT | | | | |



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL: Default
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| | | |
|-------------------------------|------------|------------------------------|
| USER NAME = mullanecd | DESIGNED - | REVISED - C. JUCIUS 02-15-07 |
| | DRAWN - | REVISED - |
| PLOT SCALE = 100,0000 ' / in. | CHECKED - | REVISED - |
| PLOT DATE = 2/7/2022 | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

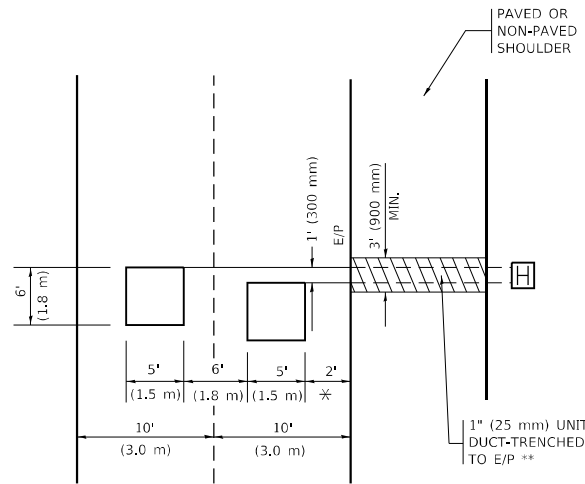
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. 11+43 TO STA. 57+12

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------|----------------|----------|--------------------|-----------|
| 1372 | 2018-019-RS-SW | COOK | 49 | 46 |
| TC-26 | | | CONTRACT NO. 62G71 | |
| | | ILLINOIS | FED. AID PROJECT | |

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

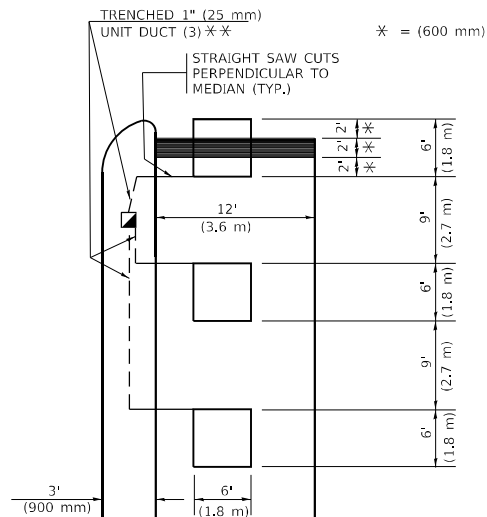


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

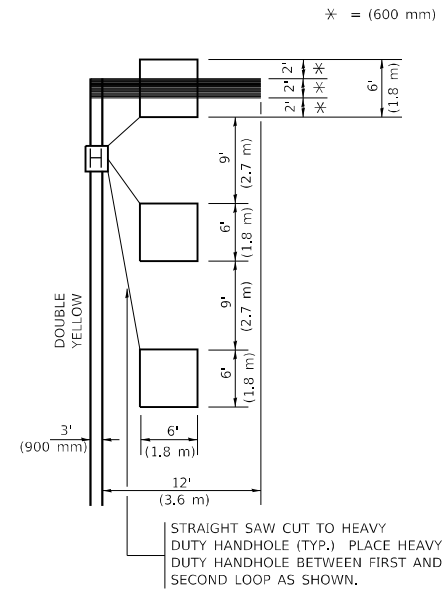


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

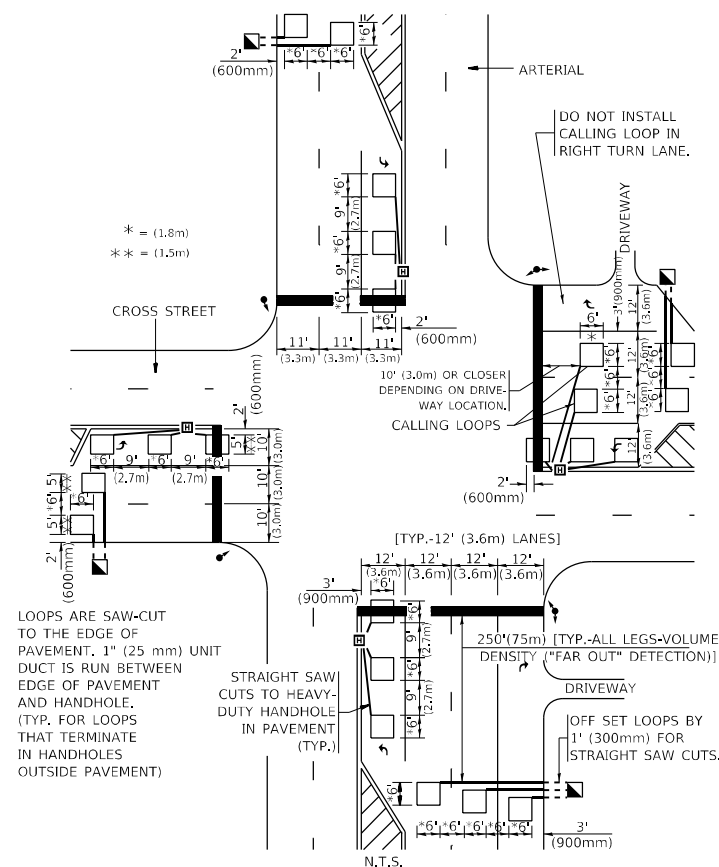
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

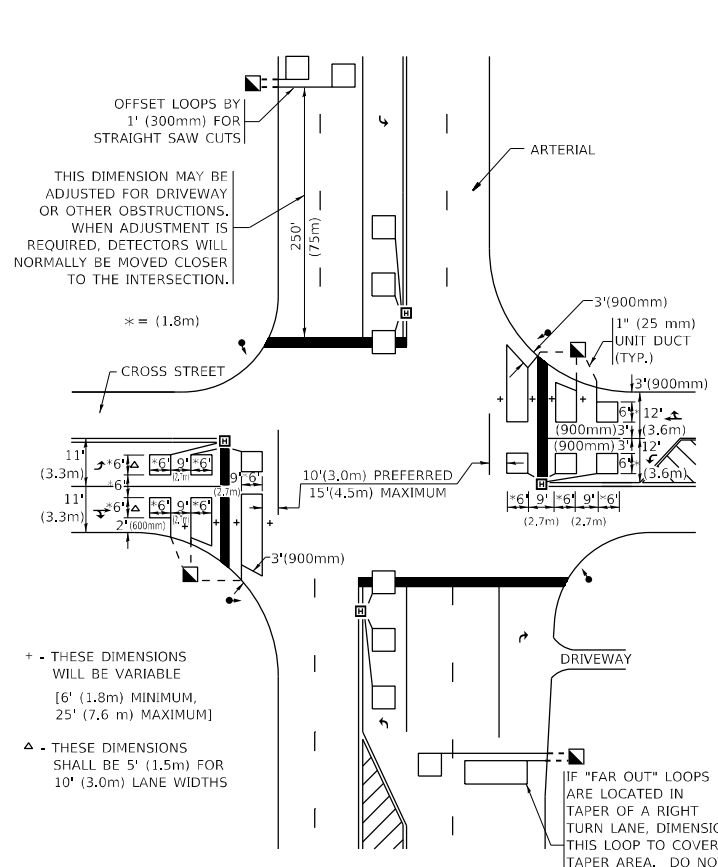
ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



* = (1.8m)
 ** = (1.5m)

DETAIL 1
 N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

+ THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]
 Δ THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

DETAIL 2
 N.T.S.

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| | | |
|------------------------------|------------------|-----------|
| USER NAME = nullanecd | DESIGNED - | REVISED - |
| PLOT SCALE = 100,0000' / in. | DRAWN - | REVISED - |
| PLOT DATE = 2/7/2022 | CHECKED - R.K.F. | REVISED - |
| | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | |
|--|---------|-------------|--------------------------|
| DISTRICT 1 - DETECTOR LOOP INSTALLATION | | | |
| DETAILS FOR ROADWAY RESURFACING | | | |
| SCALE: NONE | SHEET 1 | OF 1 SHEETS | STA. 11+43 TO STA. 57+12 |

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------------------|--------------|-----------|
| 1372 | 2018-019-RS-SW | COOK | 49 | 48 |
| TS-07 | | CONTRACT NO. 62G71 | | |
| ILLINOIS FED. AID PROJECT | | | | |

