

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS

**PROPOSED  
 HIGHWAY PLANS**

FAP 332 /IL RTE. 1  
 SECTION: 9 Y-RS-8  
 CHURCH ROAD TO IL. 394

**RESURFACING  
 WILL COUNTY  
 C-91-035-10**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
332	9 Y-RS-8	WILL	25	1
		ILLINOIS	CONTRACT NO. 60140	

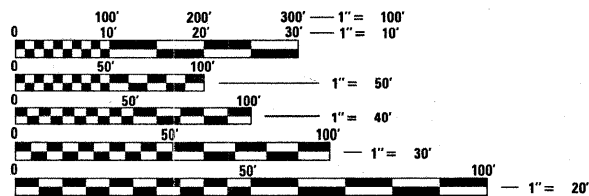
D-91-035-10



LOCATION OF SECTION INDICATED THUS: - [shaded box] -

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED IN THE VILLAGE OF BEECHER.

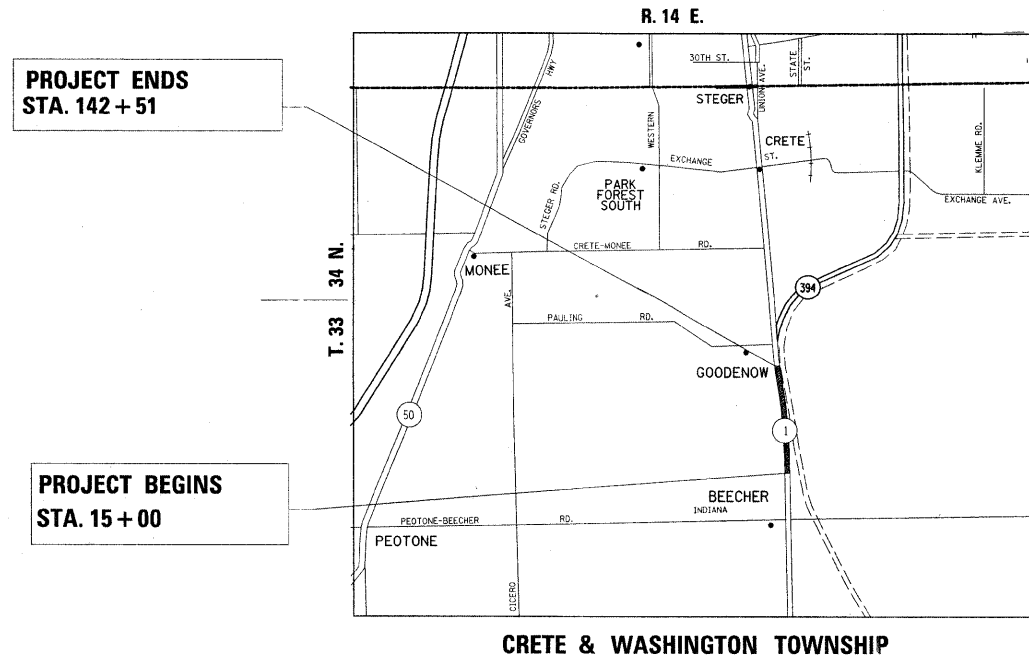


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811

PROJECT ENGINEER DAN WILGREEN (847) 705-4240  
 PROJECT MANAGER KEN ENG (847) 705-4247

CONTRACT NO. 60140



PROJECT ENDS  
 STA. 142 + 51

PROJECT BEGINS  
 STA. 15 + 00

TRAFFIC DATA  
 2007 ADT = 9,200  
 SPEED LIMIT = 40 - 55 MPH

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS

SUBMITTED JANUARY 26, 20 10

Diana M. O'Keefe  
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 9 20 10  
Scott E. Stitt P.E. /es  
 ACTING ENGINEER OF DESIGN AND ENVIRONMENT

March 19 20 10  
Christine M. Reed /es  
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY  
 OF THE STATE OF ILLINOIS**

CROSS AND NET LENGTH OF PROJECT = 12,751 FEET = 2.41 MILES

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS, PLAN NOTES
3	SUMMARY OF QUANTITIES
4 - 8	TYPICAL SECTIONS
9 - 13	ROADWAY AND PAVEMENT MARKING PLANS
14	DETECTOR LOOP REPLACEMENT PLANS
15	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
16	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
17	BUTT JOINT AND HMA TAPER DETAILS
18	H.M.A. TAPER AT EDGE OF P.C.C. PAVEMENT
19	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
20	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
21	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
22	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
23	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
24	ARTERIAL ROAD INFORMATION SIGN
25	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

LIST OF STATE STANDARDS

NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-03	CLASS C AND D PATCHES
642001-01	SHOULDER RUBBER STRIPS
701006-03	OFF-RD OPERATIONS 2L, 2W 4.5m (15') TO PAVEMENT EDGE FOR SPEEDS > 45MPH
701306-02	LANE CLOSURE 2L, 2W SLOW MOVING OPERATIONS-DAY ONLY FOR SPEEDS > 45 MPH
701311-03	LANE CLOSURE 2L, 2W, MOVING OPERATIONS-DAY ONLY
701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-06	URBAN LANE CLOSURE MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR "CUAN" (CHICAGO UTILITY ALERT NETWORK) AT 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOURS NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES WITHIN THE VILLAGE OF BEECHER.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE RESIDENT ENGINEER SHOULD CONTACT MS. CORA MATHIS, AREA TRAFFIC ENGINEER, AT (847) 715-8428. PRIOR TO PLACING ANY PAVEMENT MARKINGS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO START OF WORK.

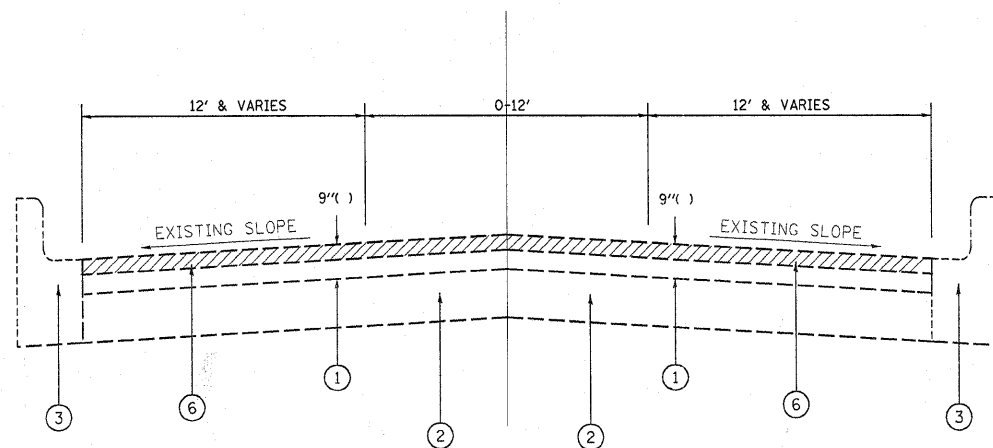
10 FEET (3 METERS) TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H)

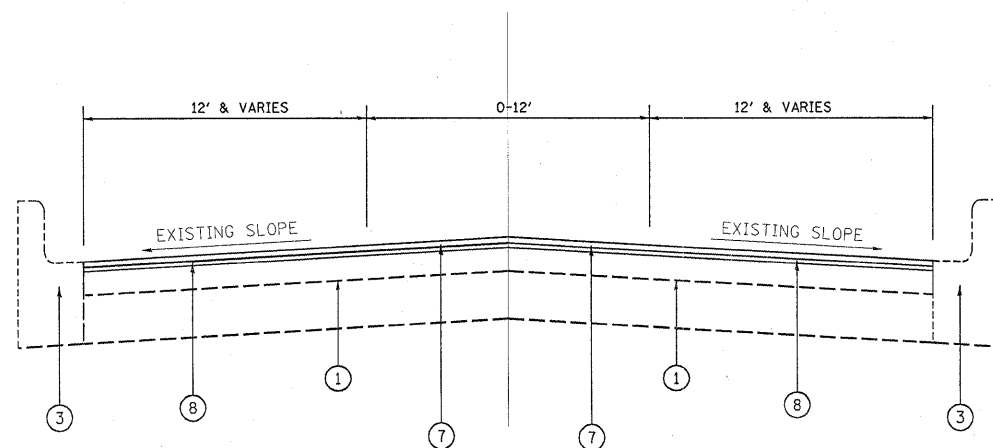
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cr:\pwork\pwork\banks1\20157014\0103510	sh-t-plan\dgn	DRAWN -	REVISED -			332	9 Y-RS-8	WILL	25	2	
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 60140					
	PLOT DATE = 1/25/2010	DATE -	REVISED -			ILL. HOIS FED. AID PROJECT					
					SCALE: NONE	SHEET NO.	OF	SHEETS	STA.	TO STA.	

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	TOTAL QUANTITIES		1000					CODE NO	ITEM	UNIT	TOTAL QUANTITIES		1000				
			100% STATE										100% STATE						
20201006	GRADING AND SHAPING SHOULDERS	UNIT	210		210					70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	264		264				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	83		83					70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	110		110				
25200110	SODDING, SALT TOLERANT	SO YD	83		83					70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	2150		2150				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	34		34				* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	182		182					
40600300	AGGREGATE (PRIME COAT)	TON	168		168				* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	39377		39377					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	63		63				* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	888		888					
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1633		1633				* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	264		264					
40600895	CONSTRUCTING TEST STRIP	EACH	2		2				* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	110		110					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	304		304				* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	315		315					
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SO YD	160		160				78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	284		284					
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	3530		3530				* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	767		767					
42001300	PROTECTIVE COAT	SO YD	111		111				X0322256	TEMPORARY INFORMATION SIGNING	SO FT	51.4		51.4					
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	36430		36430				X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SO YD	2152		2152					
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	500		500														
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	258		258														
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	190		190														
44201827	CLASS D PATCHES, TYPE II, 15 INCH	SO YD	630		630														
44201831	CLASS D PATCHES, TYPE III, 15 INCH	SO YD	1050		1050														
44201833	CLASS D PATCHES, TYPE IV, 15 INCH	SO YD	1235		1235														
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	1138		1138														
64200105	SHOULDER RUMBLE STRIP	FOOT	1447		1447														
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6		6														
67100100	MOBILIZATION	L SUM	1		1														
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1		1														
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1		1														
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1		1														
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	6450		6450														
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	182		182														
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	39377		39377														
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	888		888														

\* SPECIALTY ITEMS



ILL. 1  
EXISTING TYPICAL SECTION  
STA. 15+00 TO STA. 21+38



ILL. 1  
PROPOSED TYPICAL SECTION  
STA. 15+00 TO STA. 21+38

LEGEND

- ① EXISTING HOT-MIX ASPHALT SURFACE, 9" (+/-)
- ② EXISTING P.C.C. BASE COURSE, 8" (+/-)
- ③ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- ④ EXISTING HOT-MIX ASPHALT SHOULDER,
- ⑤ EXISTING AGGREGATE SHOULDER
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 "
- ⑦ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 "
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.5, N50, 3/4 "
- ⑨ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑩ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑪ PROPOSED P.C.C. SURFACE REMOVAL (VARIABLE DEPTH)
- ⑫ PROPOSED SHOULDER RUMBLE STRIPS

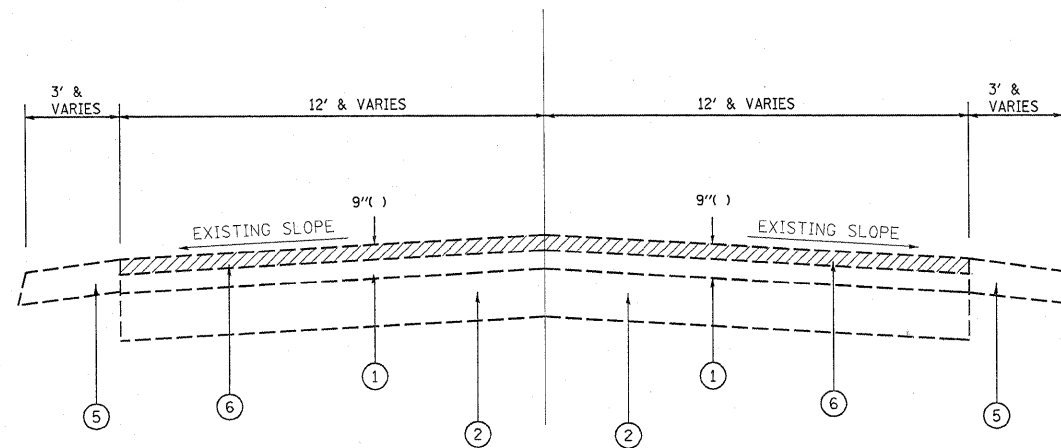
NOTE:

MILLING TO BE DONE PRIOR TO PATCHING.

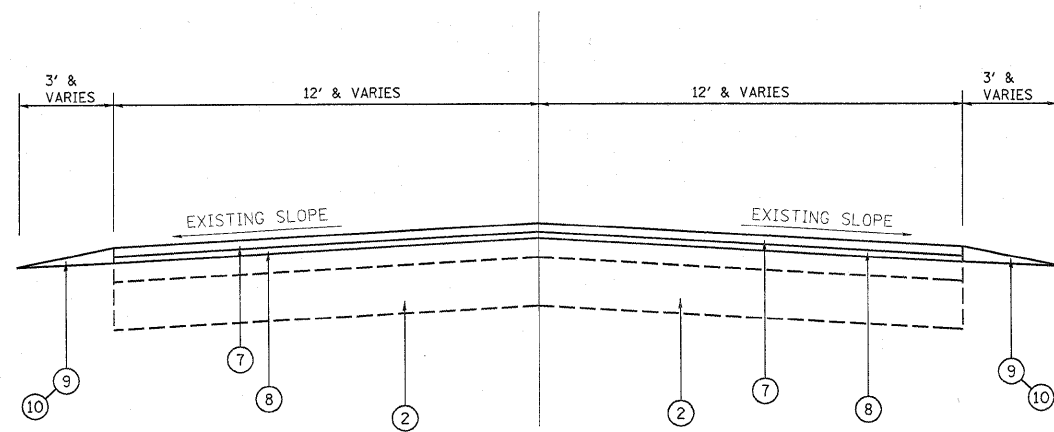
HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS(%)
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5MM)	4% @ 70 GYR.
POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES TYPE II, III & IV, 15", HMA BINDER IL-19 MM	4% @ 70 GYR.

\* THE "AC TYPE " FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON- POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

NOTE: THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.



ILL. 1  
 EXISTING TYPICAL SECTION  
 STA. 21+38 TO STA. 35+00  
 STA. 50+00 TO STA. 60+69  
 STA. 76+83 TO STA. 132+00



ILL. 1  
 PROPOSED TYPICAL SECTION  
 STA. 21+38 TO STA. 35+00  
 STA. 50+00 TO STA. 60+69  
 STA. 76+83 TO STA. 132+00

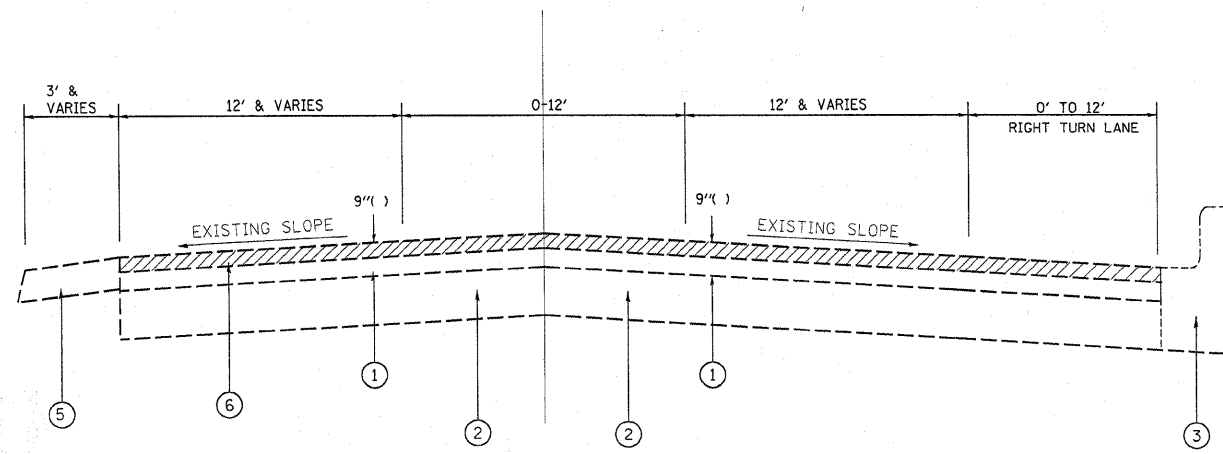
LEGEND

- ① EXISTING HOT-MIX ASPHALT SURFACE, 9" (+/-)
- ② EXISTING P.C.C. BASE COURSE, 8" (+/-)
- ③ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- ④ EXISTING HOT-MIX ASPHALT SHOULDER,
- ⑤ EXISTING AGGREGATE SHOULDER
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 "
- ⑦ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2 "
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.5, N50, 3/4 "
- ⑨ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑩ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑪ PROPOSED P.C.C. SURFACE REMOVAL (VARIABLE DEPTH)
- ⑫ PROPOSED SHOULDER RUMBLE STRIPS

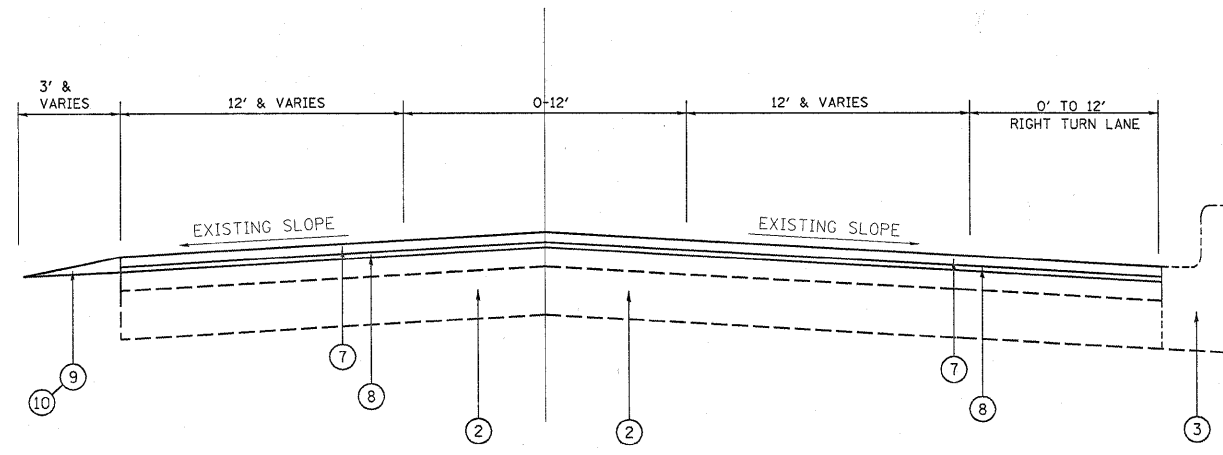
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FILE NAME =	USER NAME = banks1	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL. RTE. 1 (CHURCH RD. - ILL. RTE. 394) EXISTING AND PROPOSED TYPICAL SECTIONS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0-shrt-plan.dgn		DRAWN -	REVISED -			332	9 Y-RS-8	WILL	25	5
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 60140				
PLOT DATE = 1/28/2010		DATE -	REVISED -			SCALE:	SHEET NO. 3 OF 31 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT



ILL. 1  
EXISTING TYPICAL SECTION  
STA. 35+00 TO STA. 50+35



ILL. 1  
PROPOSED TYPICAL SECTION  
STA. 35+00 TO STA. 50+35

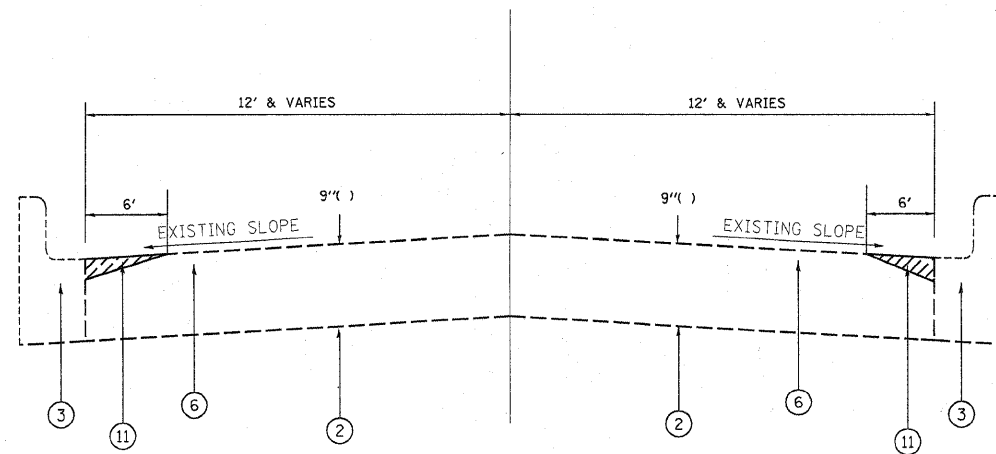
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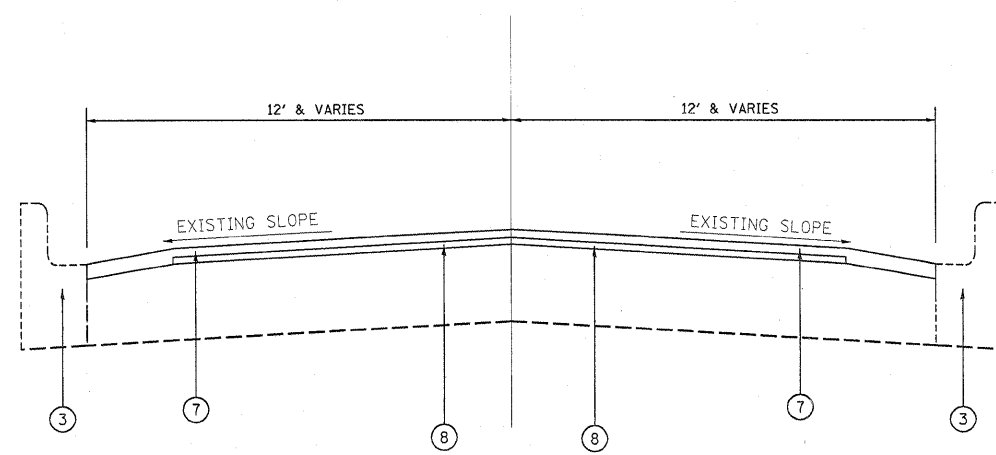
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	PLOT DATE = 1/29/2010	DATE -	REVISED -			FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT					
				SCALE:	SHEET NO. OF SHEETS	STA. TO STA.					



ILL. 1  
 EXISTING TYPICAL SECTION  
 STA. 60+69 TO STA. 76+83

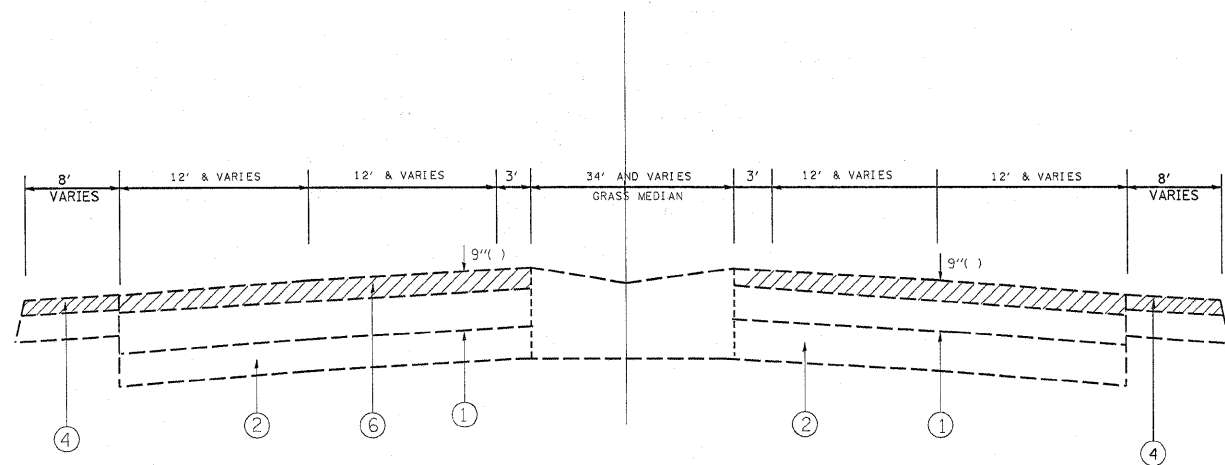


ILL. 1  
 PROPOSED TYPICAL SECTION  
 STA. 60+69 TO STA. 76+83

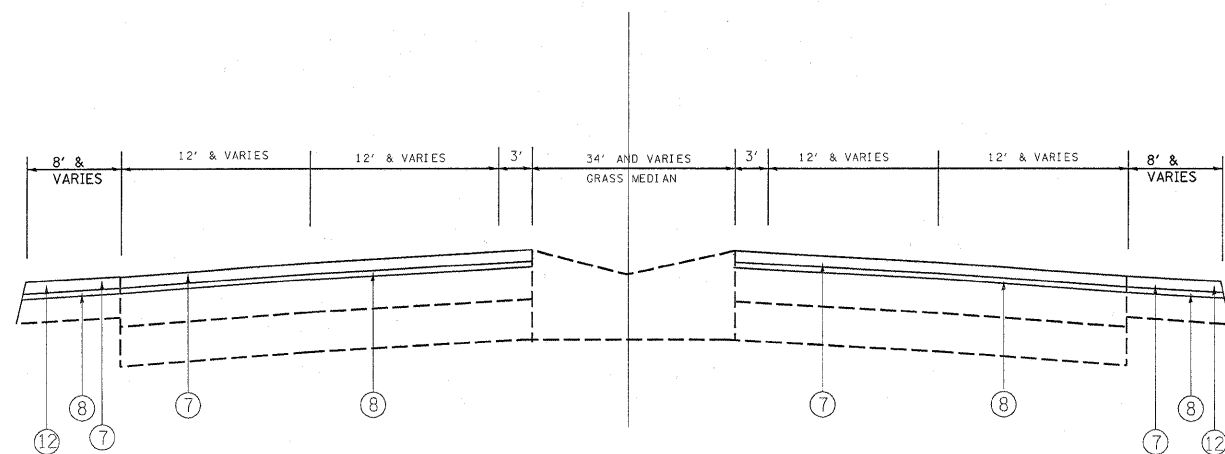
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ct:\pwork\pwidot\BANKSL\0157014\01035	0-shit-plan.dgn	DRAWN -	REVISED -			332	9 Y-RS-8	WILL	25	7
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 60140				
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ILL. 1  
EXISTING TYPICAL SECTION  
STA. 132+00 TO STA. 142+51



ILL. 1  
PROPOSED TYPICAL SECTION  
STA. 132+00 TO STA. 142+51

LEGEND

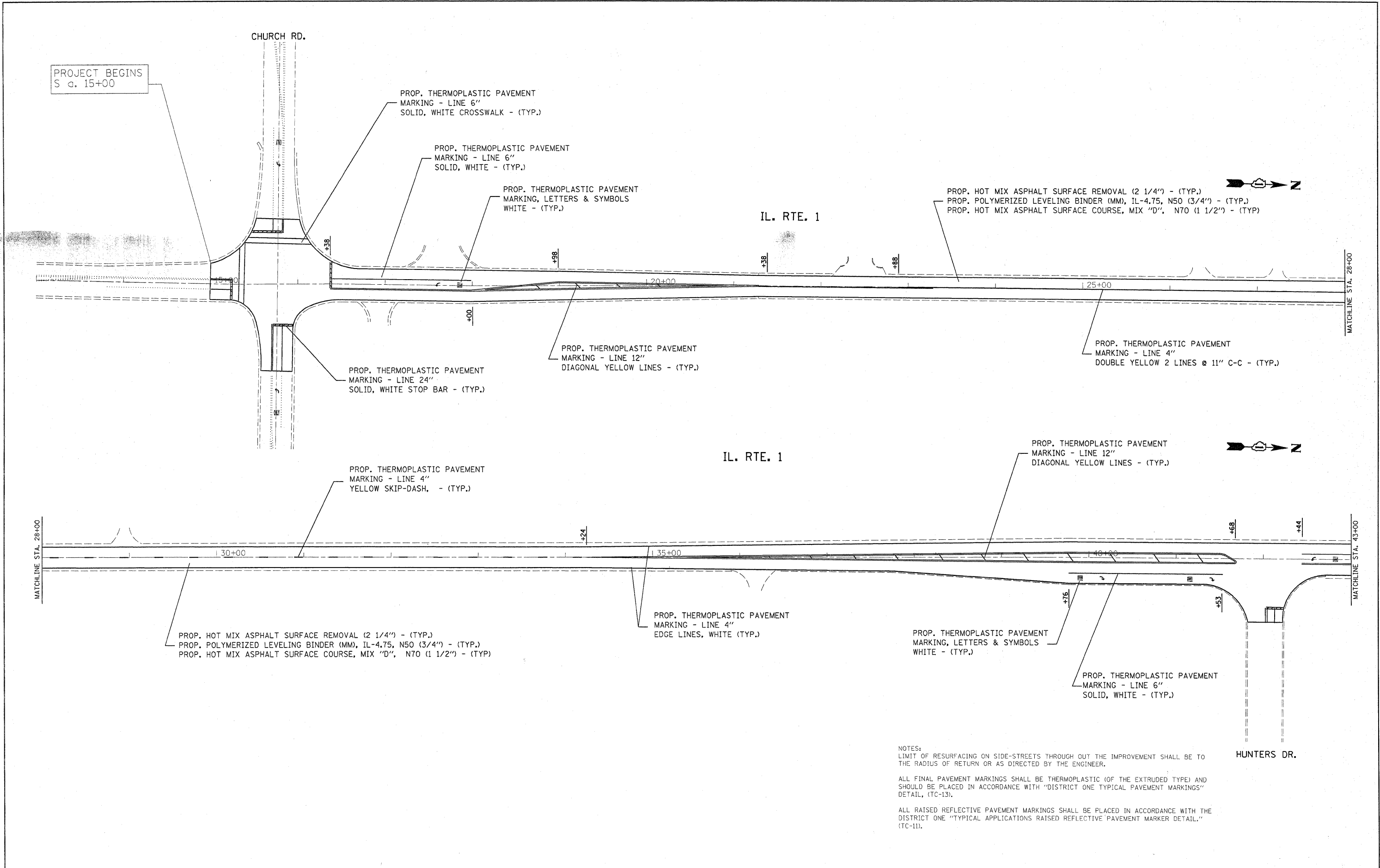
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- ⑩ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑪ PROPOSED P.C.C. SURFACE REMOVAL (VARIABLE DEPTH)
- ⑫ PROPOSED SHOULDER RUMBLE STRIPS

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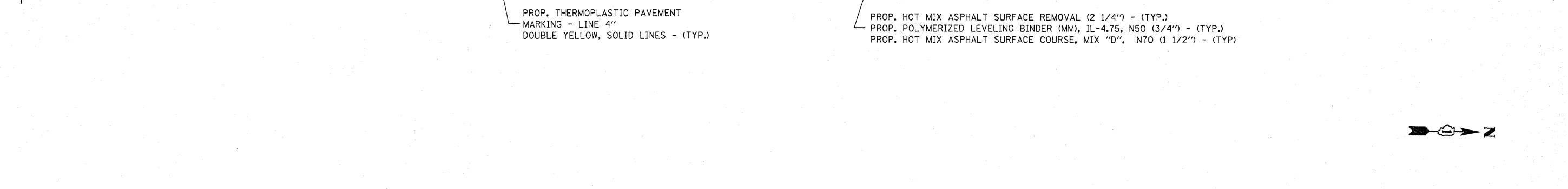
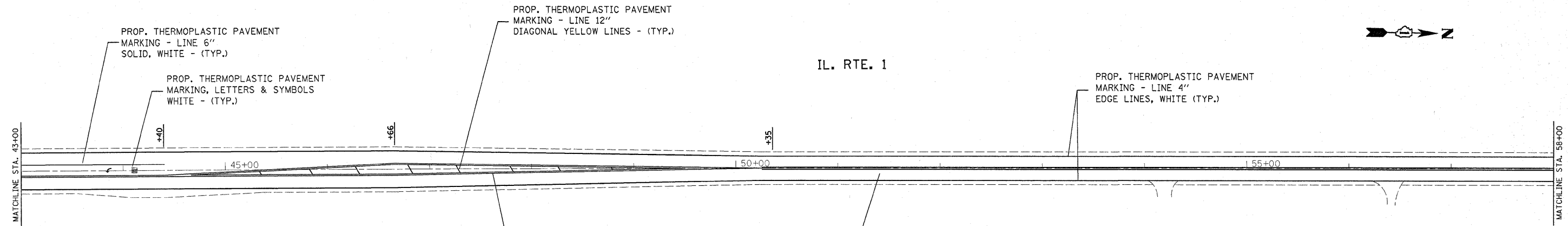


NOTES:  
 LIMIT OF RESURFACING ON SIDE-STREETS THROUGH OUT THE IMPROVEMENT SHALL BE TO THE RADIUS OF RETURN OR AS DIRECTED BY THE ENGINEER.

ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL, (TC-13).

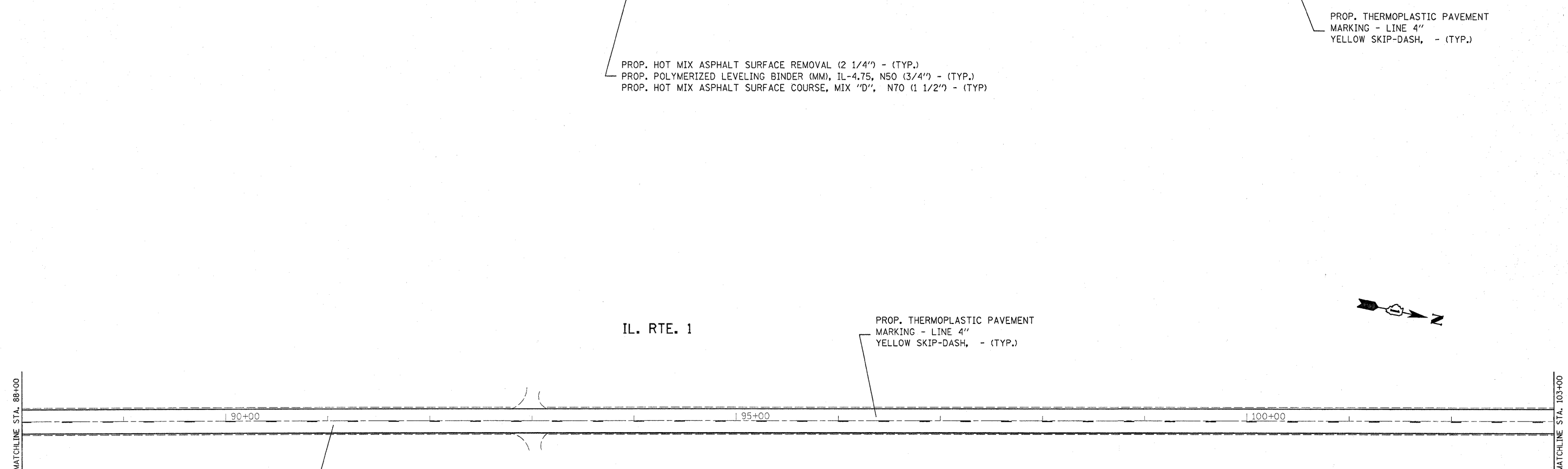
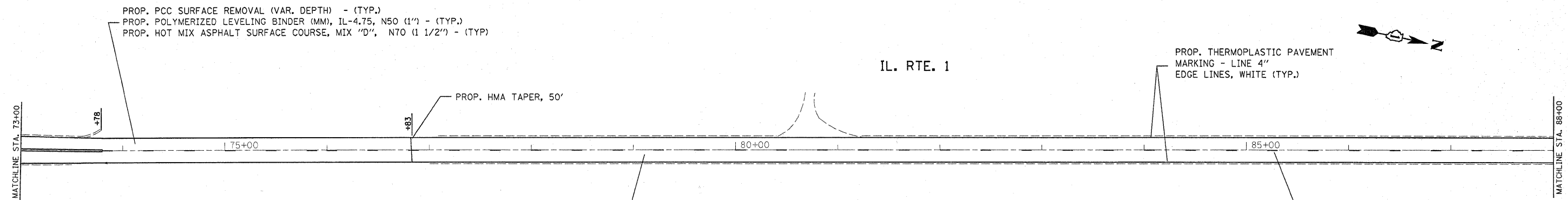
ALL RAISED REFLECTIVE PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKER DETAIL." (TC-11).

FILE NAME =	USER NAME = banks1	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY &amp; PAVEMENT MARKING PLAN IL. RTE. 1 (CHURCH RD. - IL. RTE. 394)</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwid\BANKSL\0157014\01035	2-shr-plan.dgn	DRAWN -	REVISED -					332	9Y-RS-8	WILL	25	9
PLOT SCALE = 50,0000' / IN.	CHECKED -	REVISED -	REVISED -		SCALE: 1"=50'			SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 60140	
PLOT DATE = 1/28/2010	DATE -	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT							



NOTES:  
 LIMIT OF RESURFACING ON SIDE-STREETS THROUGH OUT THE IMPROVEMENT SHALL BE TO THE RADIUS OF RETURN OR AS DIRECTED BY THE ENGINEER.  
 ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL, (TC-13).  
 ALL RAISED REFLECTIVE PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKER DETAIL." (TC-11).

FILE NAME = c:\pw\work\p\WIDOT\BANKSL\0157014\01035	USER NAME = banksl	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY &amp; PAVEMENT MARKING PLAN IL. RTE. 1 (CHURCH RD. - IL. RTE. 394)</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0-shr-plan\dgn	DRAWN -	REVISED -				332	9Y-RS-8	WILL	25	10
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -		SCALE: 1"=50'	SHEET NO. OF SHEETS	STA. TO STA.	CONTRACT NO. 60140			
	PLOT DATE = 1/28/2010	DATE -	REVISED -					ILLINOIS FED. AID PROJECT			



PROP. HOT MIX ASPHALT SURFACE REMOVAL (2 1/4") - (TYP.)  
 PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50 (3/4") - (TYP.)  
 PROP. HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70 (1 1/2") - (TYP.)

NOTES:  
 LIMIT OF RESURFACING ON SIDE-STREETS THROUGH OUT THE IMPROVEMENT SHALL BE TO THE RADIUS OF RETURN OR AS DIRECTED BY THE ENGINEER.  
 ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL, (TC-13).  
 ALL RAISED REFLECTIVE PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKER DETAIL." (TC-11).

FILE NAME =	USER NAME = banks1	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY &amp; PAVEMENT MARKING PLAN IL. RTE. 1 (CHURCH RD. - IL. RTE. 394)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pw\work\p\WIDOT\BANKSL\10157014\01035	0-shht-plan.dgn	DRAWN -	REVISED -			332	9Y-RS-8	WILL	25	11	
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 60140					
	PLOT DATE = 1/28/2010	DATE -	REVISED -			SCALE: 1"=50'	SHEET NO. OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT		

IL. RTE. 1

PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 4" YELLOW SKIP-DASH, - (TYP.)



MATCHLINE STA. 103+00

MATCHLINE STA. 118+00

PROP. HOT MIX ASPHALT SURFACE REMOVAL (2 1/4") - (TYP.)  
 PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50 (3/4") - (TYP.)  
 PROP. HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70 (1 1/2") - (TYP.)

PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 4" YELLOW SOLID LINE, - (TYP.)

PROP. HOT MIX ASPHALT SURFACE REMOVAL (2 1/4") - (TYP.)  
 PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50 (3/4") - (TYP.)  
 PROP. HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70 (1 1/2") - (TYP.)

IL. RTE. 1

PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 4" DOUBLE YELLOW SOLID, - (TYP.)



MATCHLINE STA. 118+00

MATCHLINE STA. 133+00

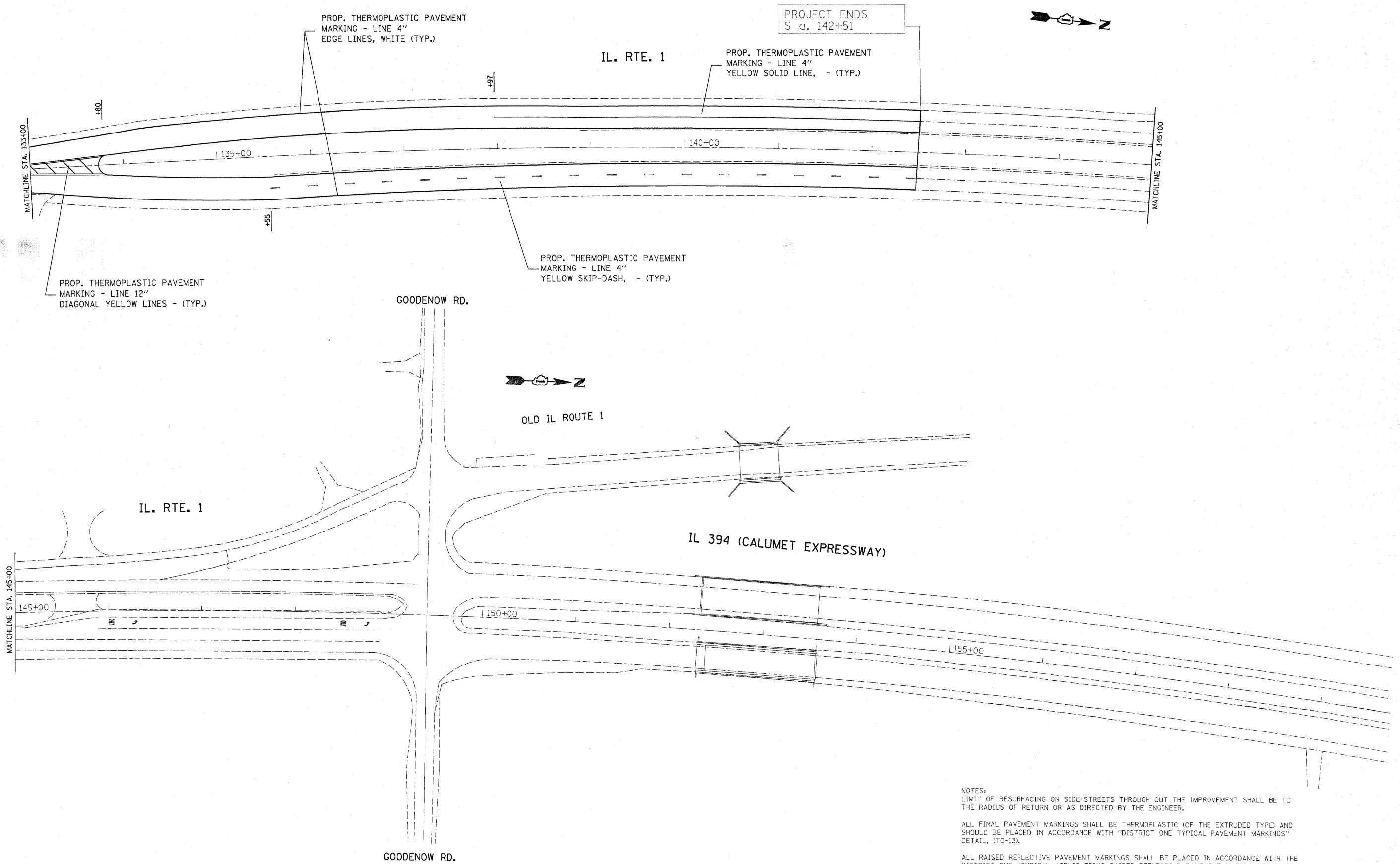
PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 4" EDGE LINES, WHITE (TYP.)

NOTES:  
 LIMIT OF RESURFACING ON SIDE-STREETS THROUGH OUT THE IMPROVEMENT SHALL BE TO THE RADIUS OF RETURN OR AS DIRECTED BY THE ENGINEER.

ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL, (TC-13).

ALL RAISED REFLECTIVE PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKER DETAIL." (TC-11).

FILE NAME = c:\pwwork\pwwid\DOT\BANKSL\08157014\01035	USER NAME = banksl 0-shit.pln.dgn	DESIGNED - DRAWN -	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY &amp; PAVEMENT MARKING PLAN IL. RTE. 1 (CHURCH RD. - IL. RTE. 394)</b>	F.A.P. RTE. 332	SECTION 9Y-RS-8	COUNTY WILL	TOTAL SHEETS 25	SHEET NO. 12	
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -	SCALE: 1"=50'			SHEET NO. OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT			
PLOT DATE = 1/28/2010	DATE -	REVISED -						CONTRACT NO. 60140			

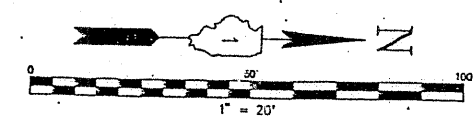
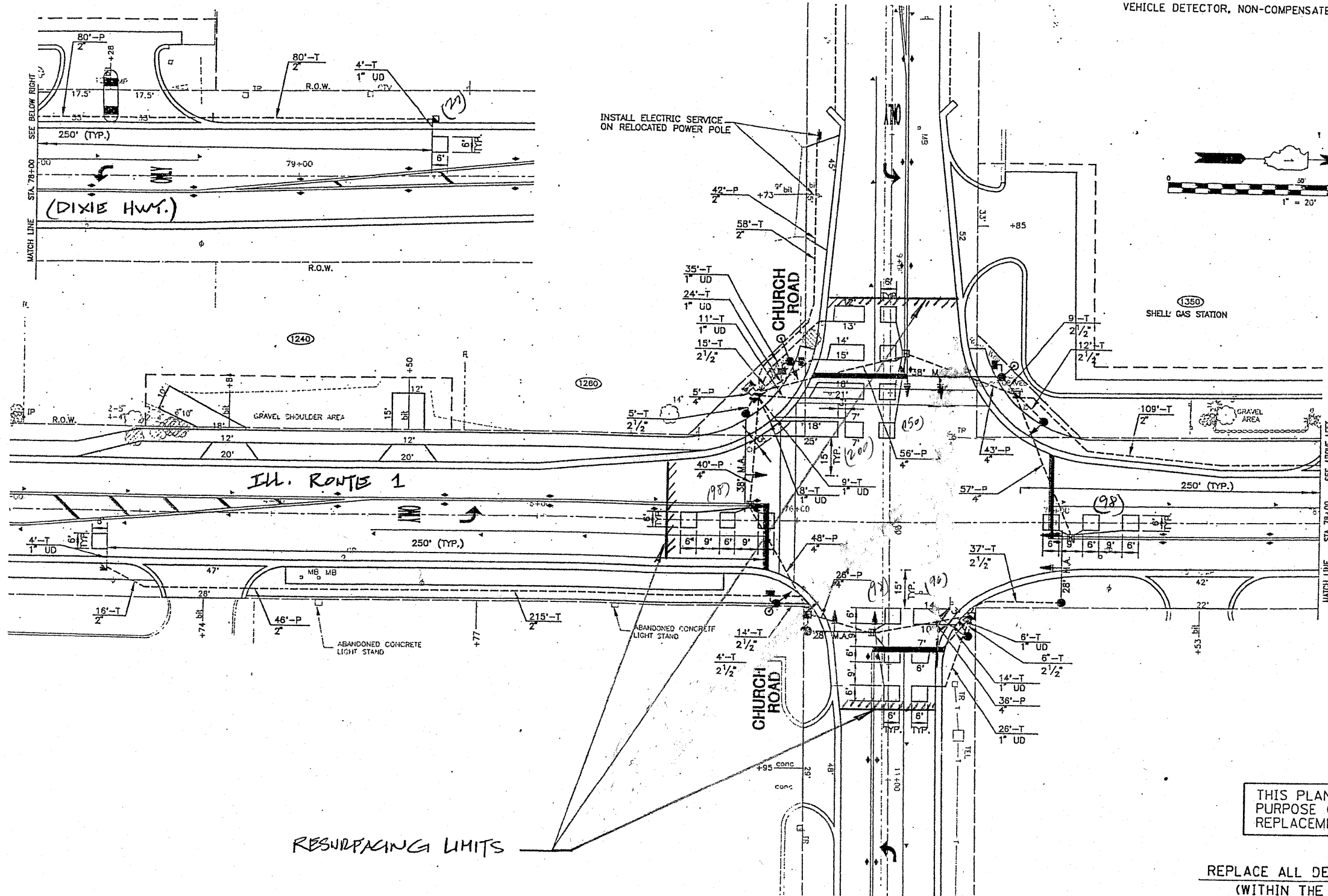


NOTES:  
 LIMIT OF RESURFACING ON SIDE-STREETS THROUGH OUT THE IMPROVEMENT SHALL BE TO THE RADIUS OF RETURN OR AS DIRECTED BY THE ENGINEER.  
 ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL, (TC-13).  
 ALL RAISED REFLECTIVE PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKER DETAIL," (TC-11).

FILE NAME = c:\pwwork\p\1007\BANKSL\0157014\01235	USER NAME = banks1 0-shr-plan.dgn	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY &amp; PAVEMENT MARKING PLAN IL. RTE. 1 (CHURCH RD. - IL. RTE. 394)</b>			F.A.P. RTE. 332	SECTION 9Y-RS-8	COUNTY WILL	TOTAL SHEETS 25	SHEET NO. 13
	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -		SCALE: 1"=50'	SHEET NO.	OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	
	PLOT DATE = 1/28/2010	CHECKED -	REVISED -								CONTRACT NO. 60140	
		DATE -	REVISED -									

**TRAFFIC SIGNAL LEGEND**

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	767	FOOT	DETECTOR LOOP, REPLACEMENT

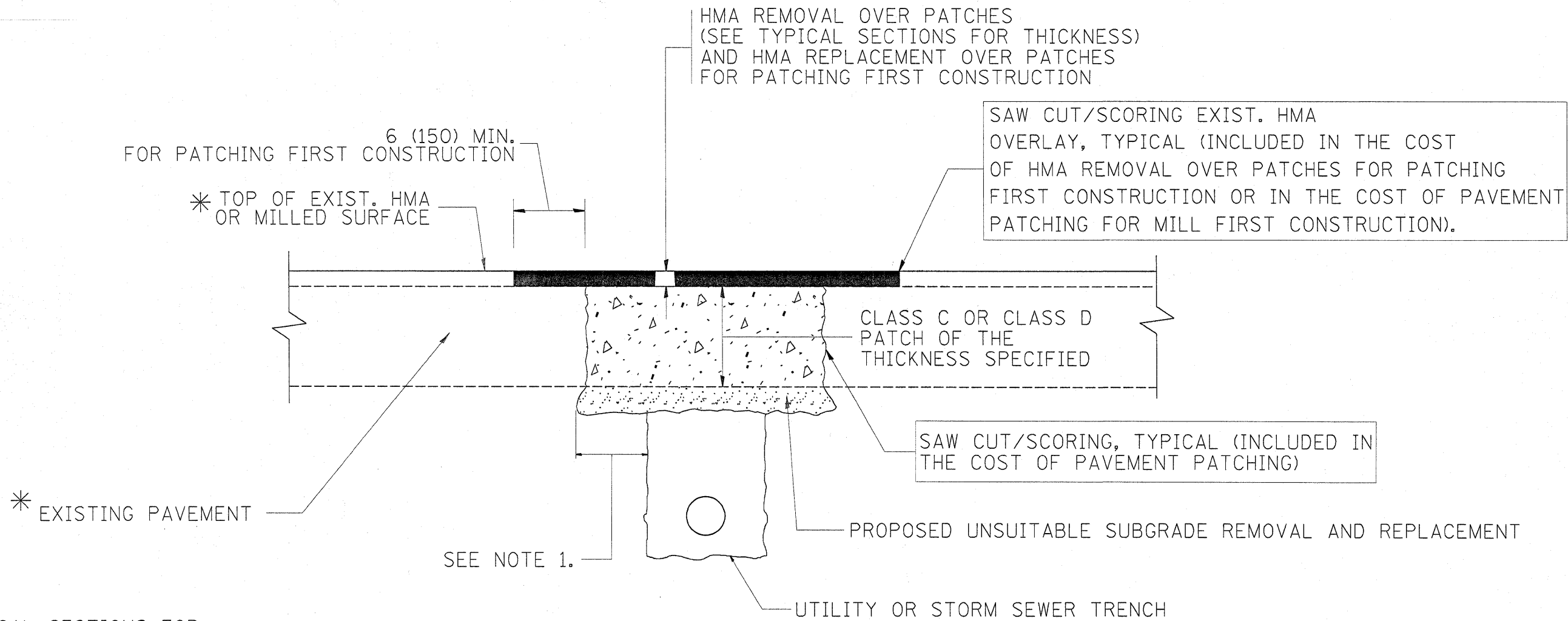
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c:\p\work\p\WIDOT\KANTHAPHIXAYBA\081126	1\traffic_legend.v7.dgn	DRAWN - BCK	REVISED -
	PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 4/3/2009	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - DETECTOR LOOP REPLACEMENT  
ILL. RTE. 1 (DIXIE HWY.) @ CHURCH ROAD

F.A.P. RTE. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
332	9 Y-RS-8	WILL	25	14
CONTRACT NO.				

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA. FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

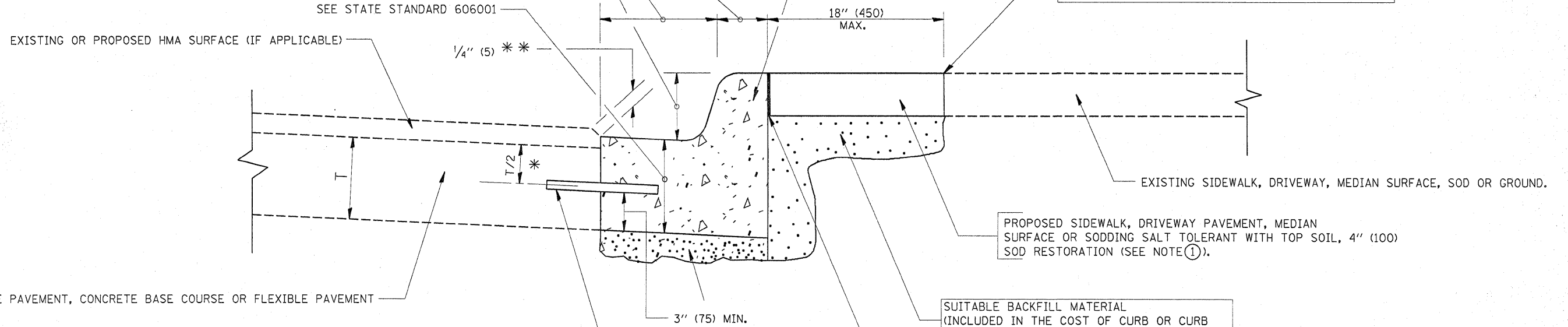
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = banks1	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwwork\PIWIDOT\BANKSL\d0157014\01st5.dgn		DRAWN -	REVISED - R. BORO 01-01-07		332	9 Y-RS-8	WILL	25	15		
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07		<b>BD400-04 (BD-22)</b>		<b>CONTRACT NO. 60140</b>				
	PLOT DATE = 1/23/2010	DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\* \* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

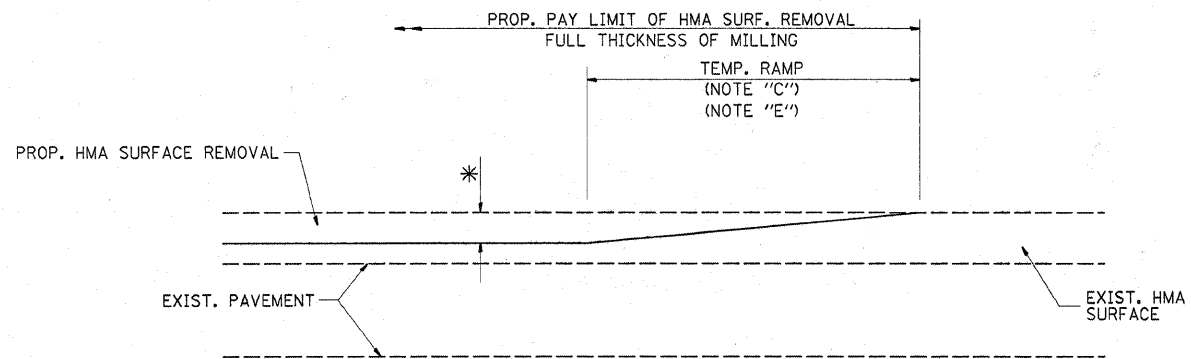
⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

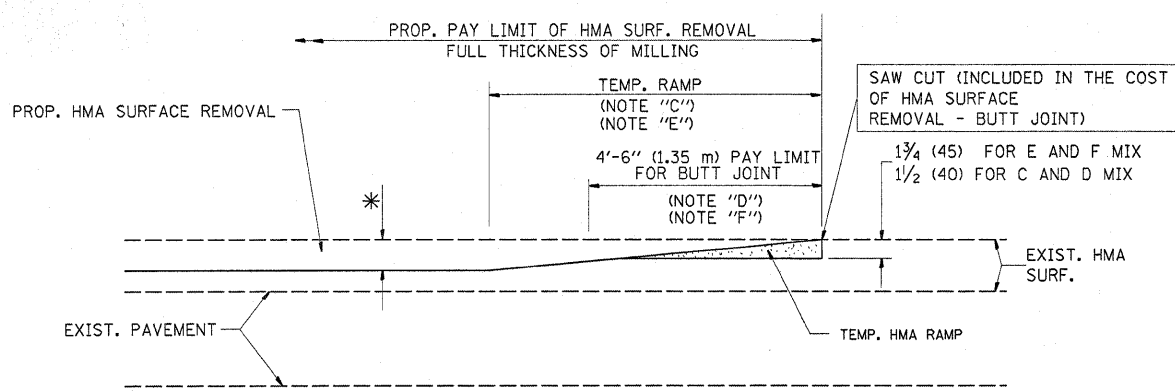
FILE NAME =	USER NAME = banksl	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw\work\PWIDOT\BANKSL\8157014\DistS	d.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97		332	9 Y-RS-8	WILL	25	16			
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01	REMOVED		<b>BD600-06 (BD-24)</b>		CONTRACT NO. 60140					
PLOT DATE = 1/23/2010	DATE - 03-11-94	REVISED - R. BORO 12-15-09			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			





MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

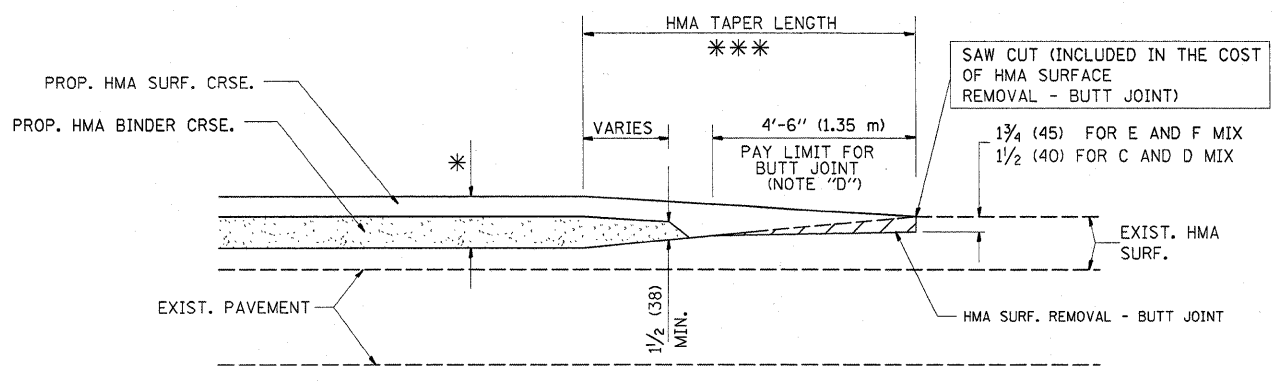
**OPTION 1**



HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

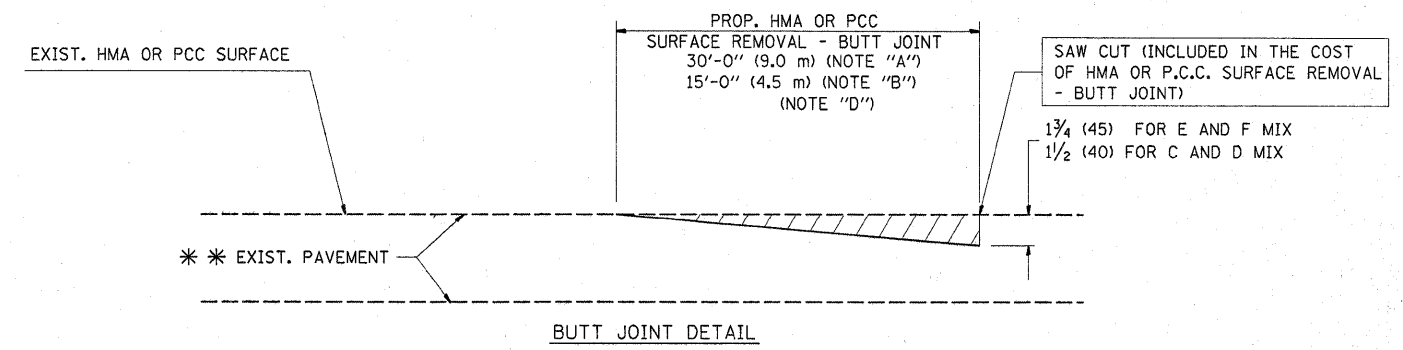
**OPTION 2**

**TYPICAL TEMPORARY RAMP**

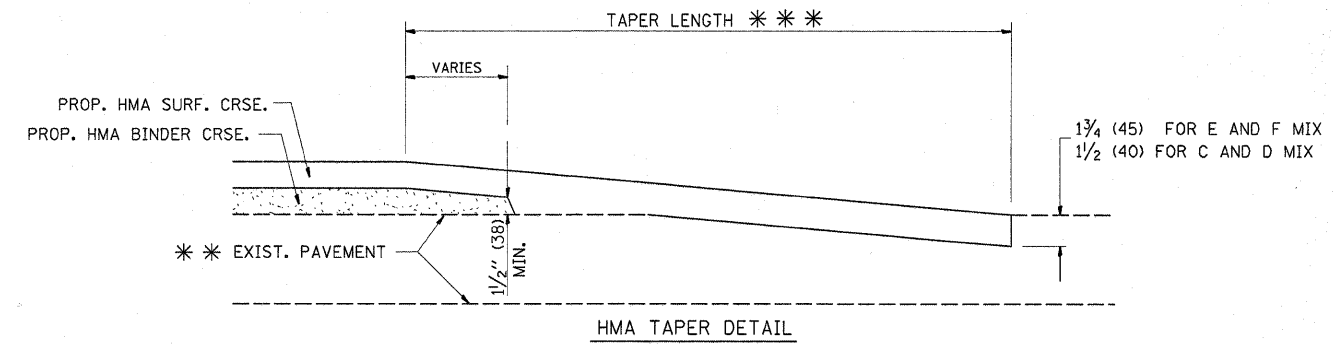


BUTT JOINT AND HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

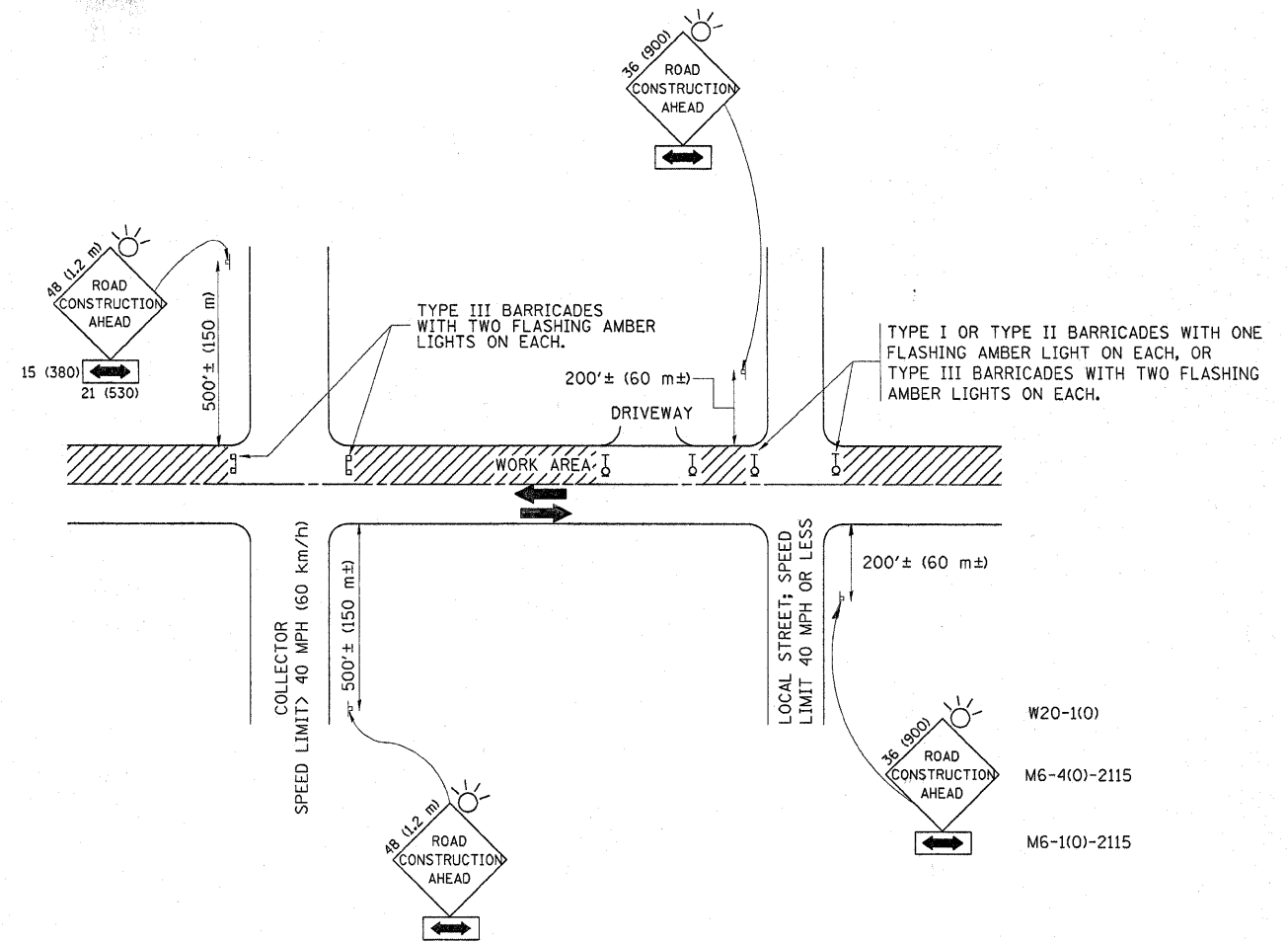
**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = banks1	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BUTT JOINT AND HMA TAPER DETAILS</b>		F.A.P. RTE. 332	SECTION 9 Y-RS-8	COUNTY WILL	TOTAL SHEETS 25	SHEET NO. 17	
ct:\pwwork\pwwid\BANKSL\d8157014\01st5.dgn	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	<b>BD400-05 BD32</b>		CONTRACT NO. 60140		
	PLOT DATE = 1/23/2010	CHECKED -	REVISED - M. GOMEZ 04-06-01		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE - 06-13-90	REVISED - R. BORO 01-01-07									





TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

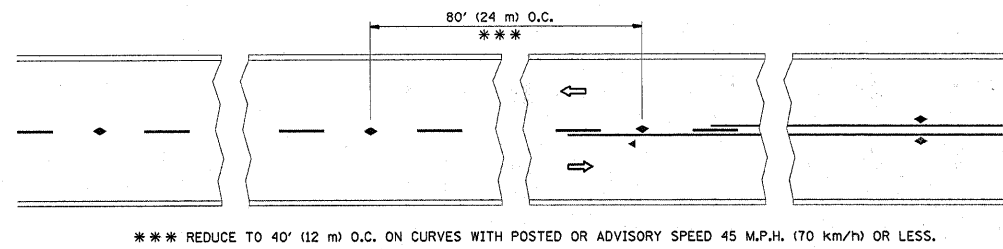
NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

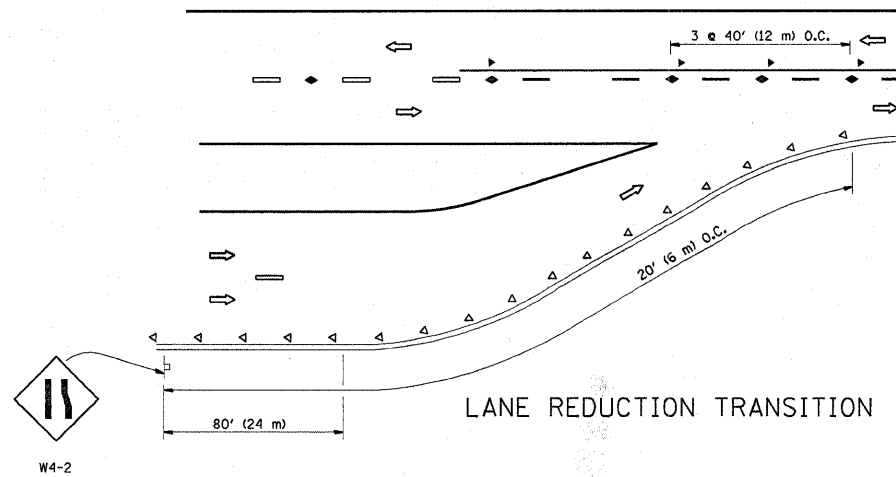
USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

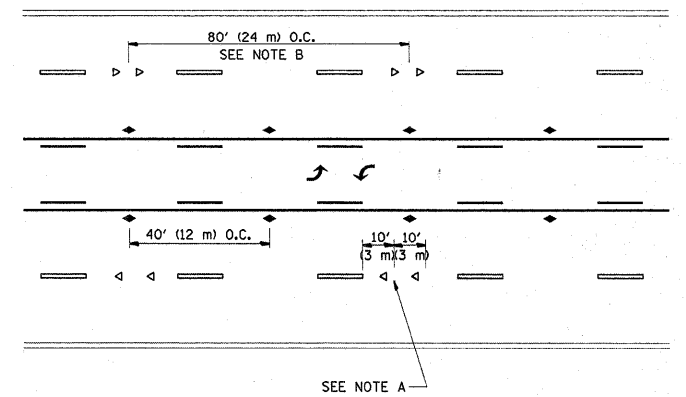
FILE NAME =	USER NAME = banks1	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et\pw\work\VPWIDOT\BANKSL\d8157814\DistS	d.dgn	DRAWN -	REVISED - A. HOUSEH 03-06-96		332	9 Y-RS-8	WILL	25	19			
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISIED - A. HOUSEH 10-15-96			<b>TC-10</b>		<b>CONTRACT NO. 60140</b>					
PLOT DATE = 1/23/2010	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00	SCALE: NONE		SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



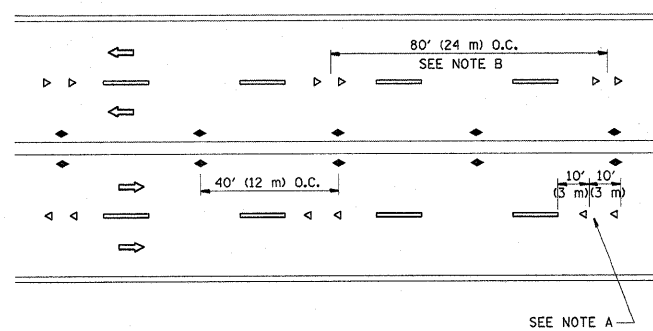
TWO-LANE/TWO-WAY



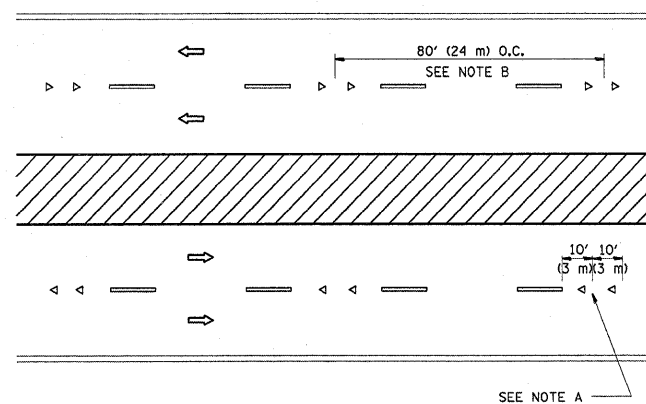
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

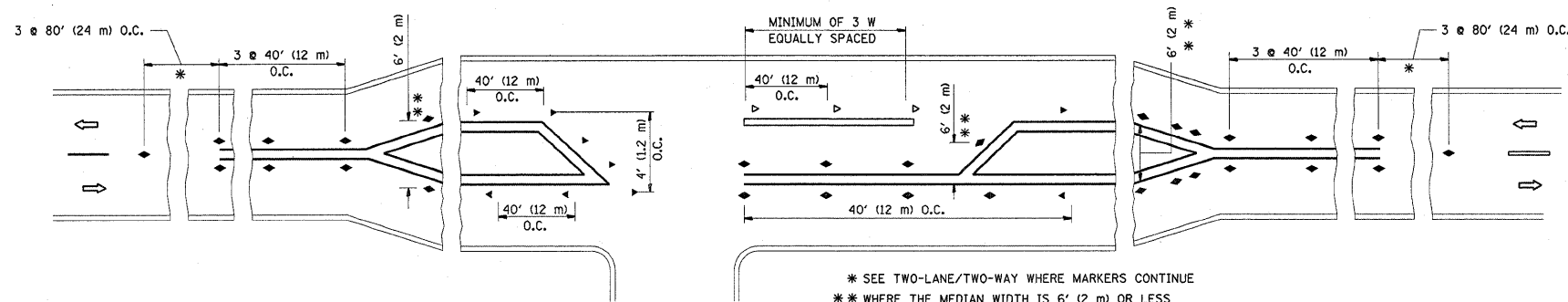
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

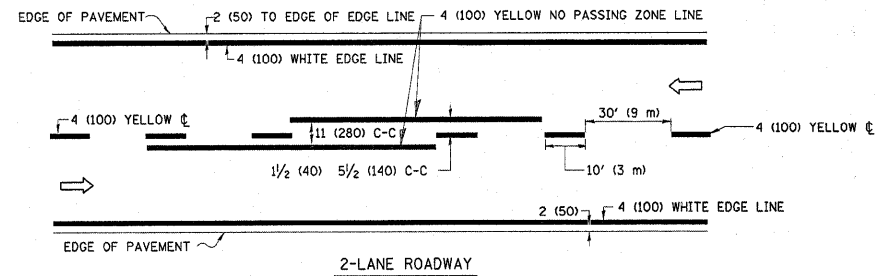


LEFT TURN

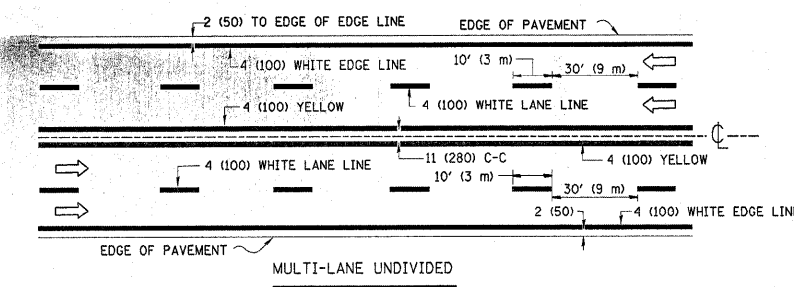
\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

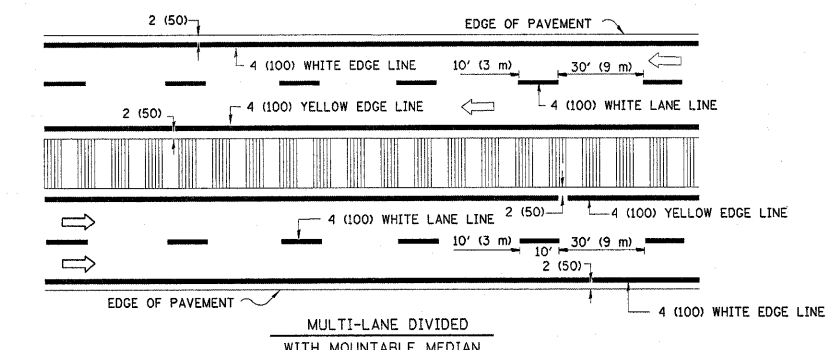
FILE NAME =	USER NAME = bankal	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL APPLICATIONS</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
esi\pwwork\pwwid\BANKSL\d0157014\Dist5.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99		332	9 Y-RS-8	WILL	25	20		
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - T. RAMMACHER 01-06-00		<b>RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)</b>		<b>TC-11</b>		CONTRACT NO. 60140		
PLOT DATE = 1/23/2010		DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



2-LANE ROADWAY



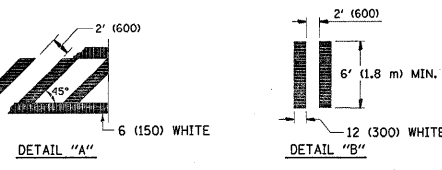
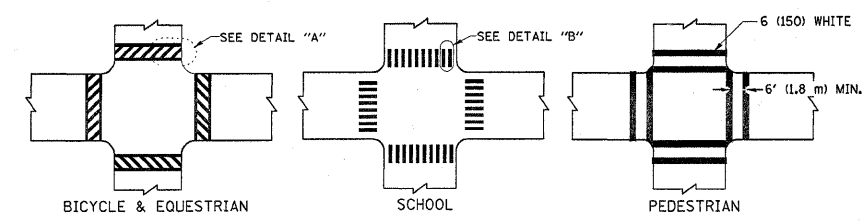
MULTI-LANE UNDIVIDED



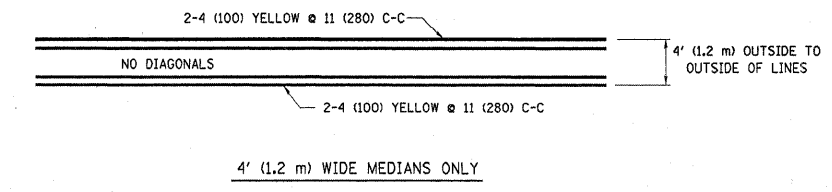
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

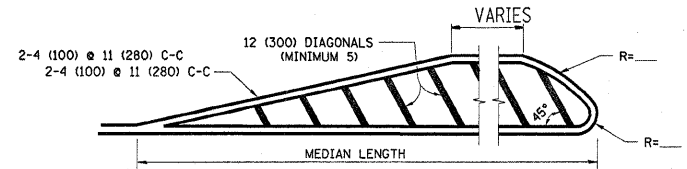
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

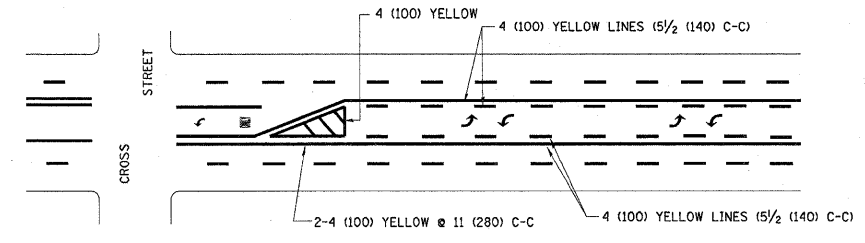


4' (1.2 m) WIDE MEDIANS ONLY

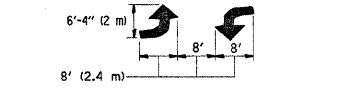


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

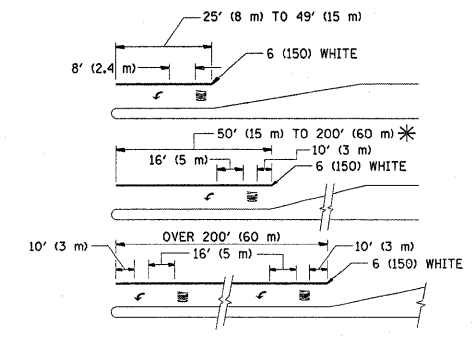


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

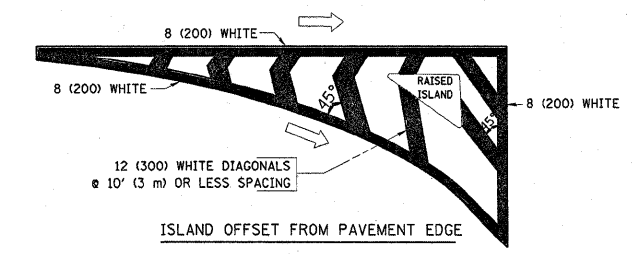
TYPICAL PAINTED MEDIAN MARKING



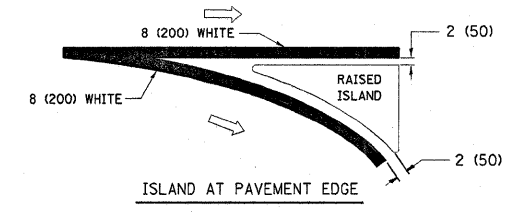
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2' (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2' (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL))	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

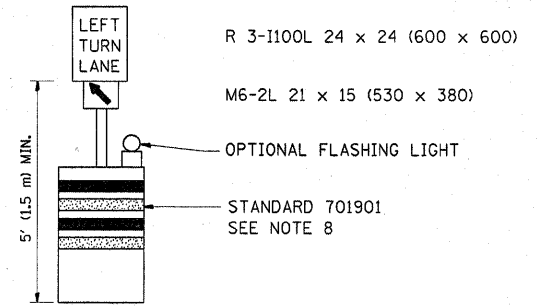
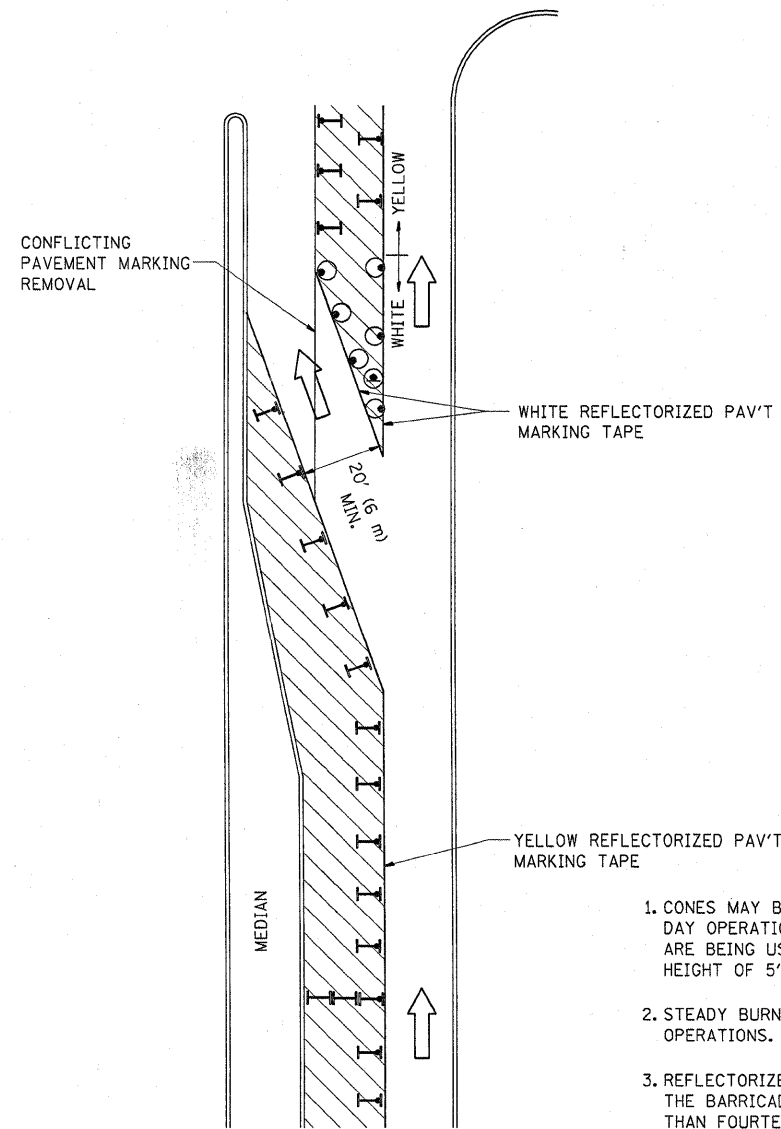
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PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = 1/23/2010		DATE - 03-19-90	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
332	9 Y-RS-8	WILL	25	21
TC-13		CONTRACT NO. 60140		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

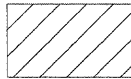







**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

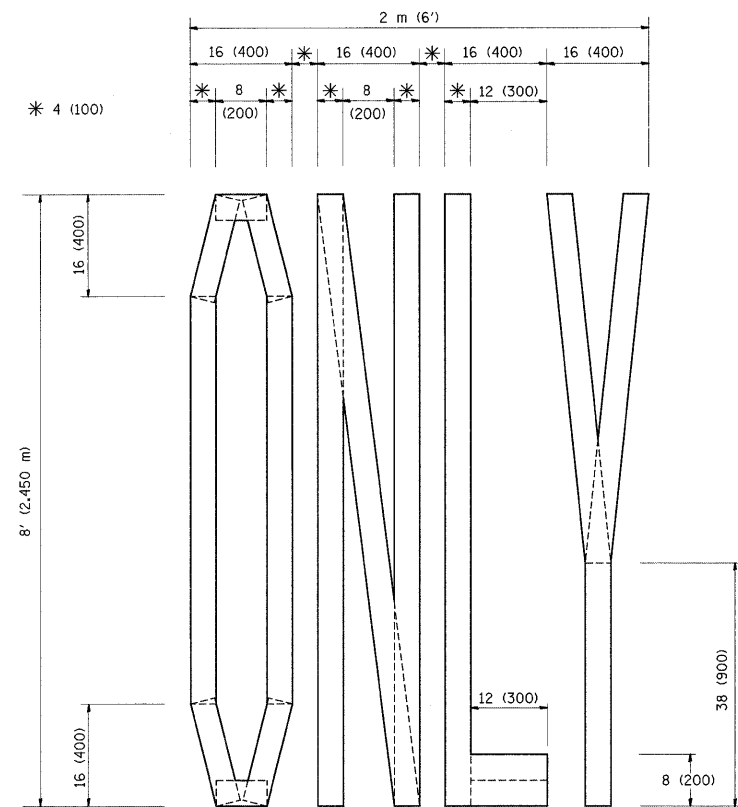
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	PLOT DATE = 1/23/2010	REVISED -T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

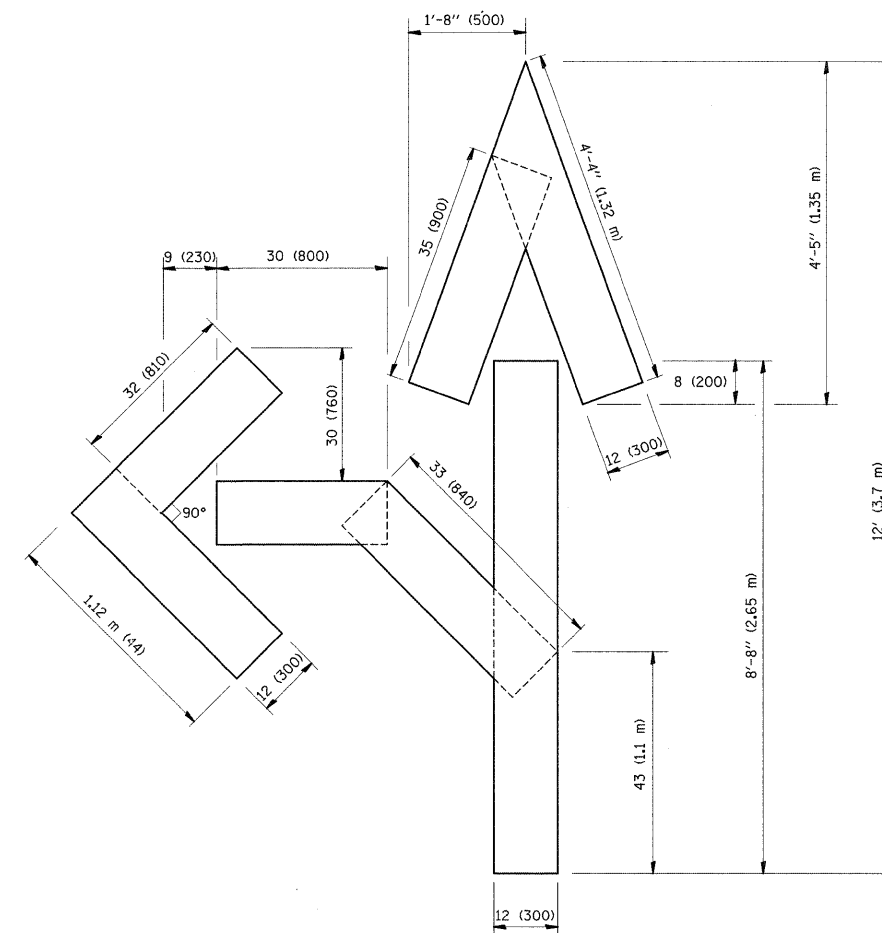
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

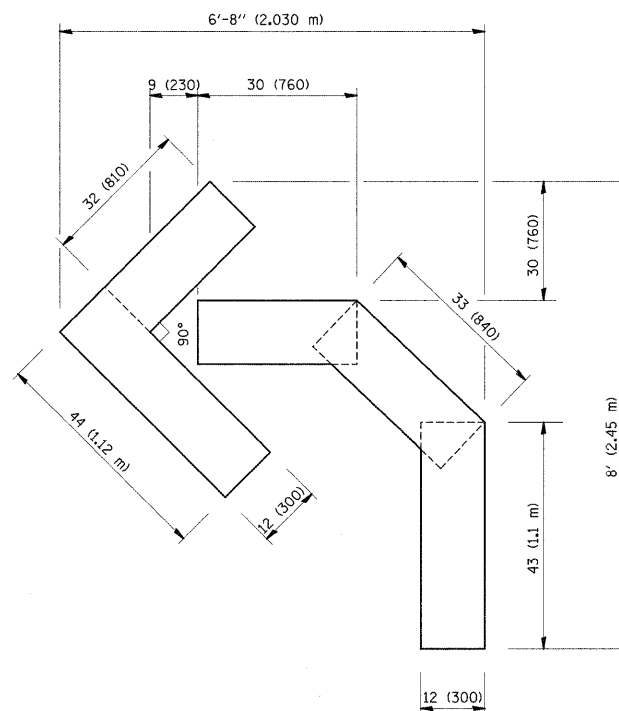
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
332	9 Y-RS-8	WILL	25	22
<b>TC-14</b>			<b>CONTRACT NO. 60140</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

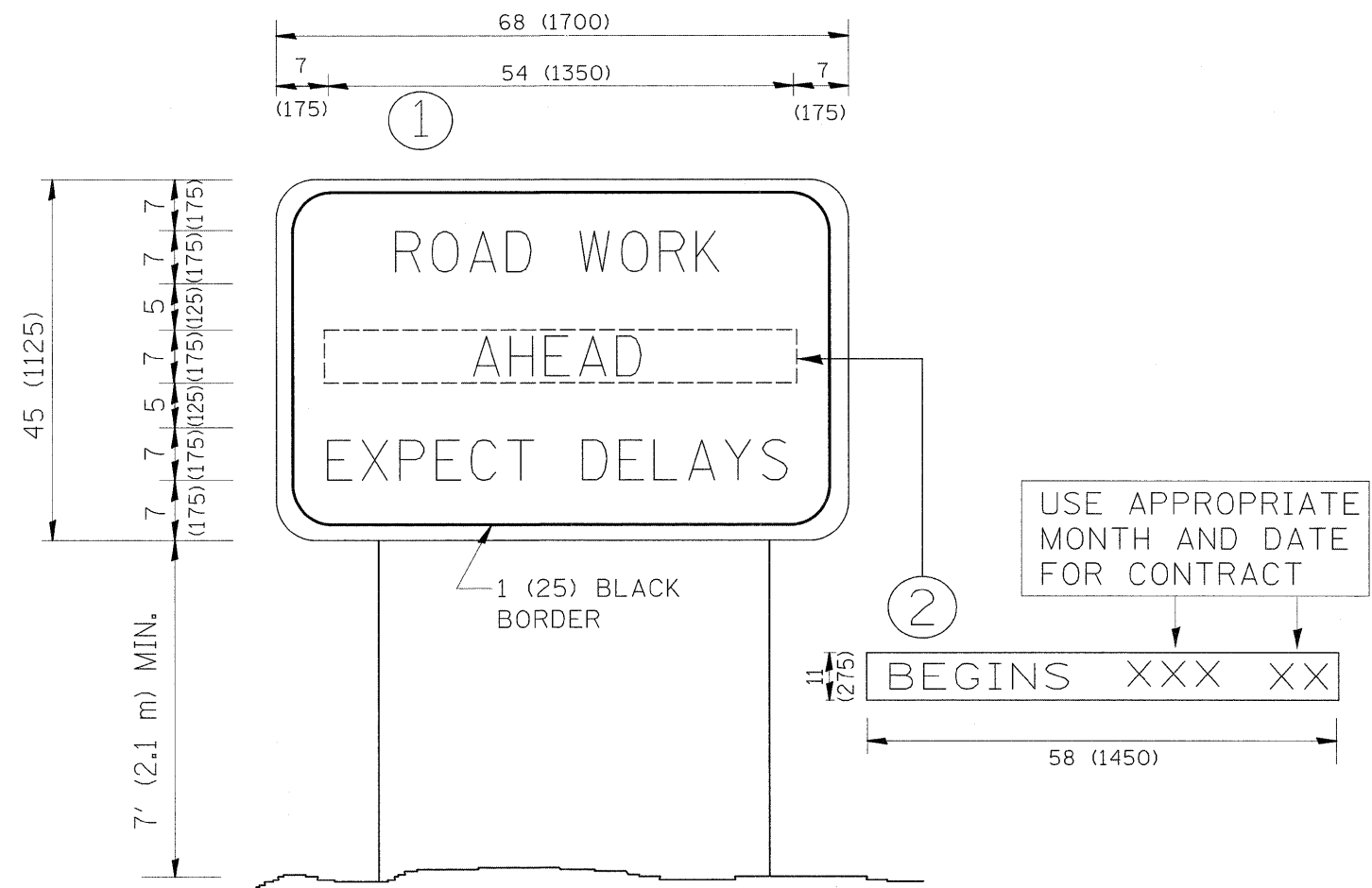
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		CHECKED -	REVISED - T. RAMMACHER 03-02-98
		DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
332	9 Y-RS-8	WILL	25	23
TC-16		CONTRACT NO. 60140		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

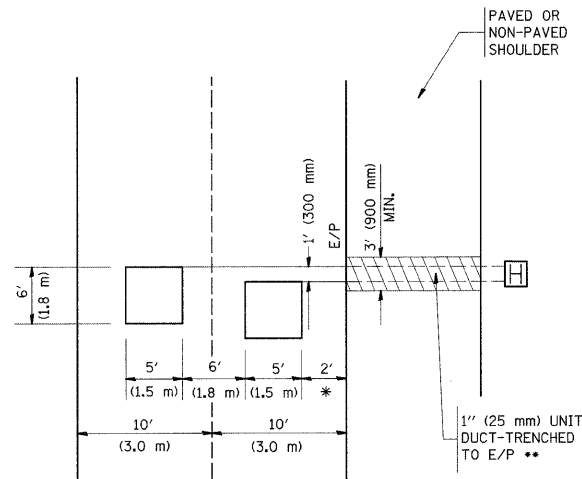
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = banks	DESIGNED -	REVISED - R. MIRS 09-15-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\PWIDOT\BANKSL\d0157014\DatS	d.dgn	DRAWN -	REVISED - R. MIRS 12-11-97		332	9 Y-RS-8	WILL	25	24			
		PLOT SCALE = 50.0000' / IN.	REVISED - T. RAMMACHER 02-02-99		SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	
		PLOT DATE = 1/25/2010	REVISED - C. JUCIUS 01-31-07		TC-22		CONTRACT NO.		60140			



**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

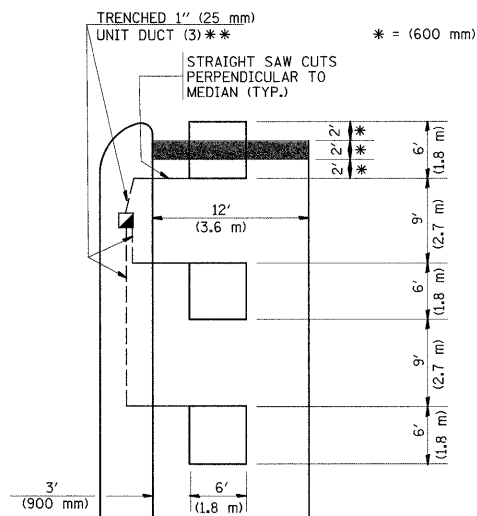


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**

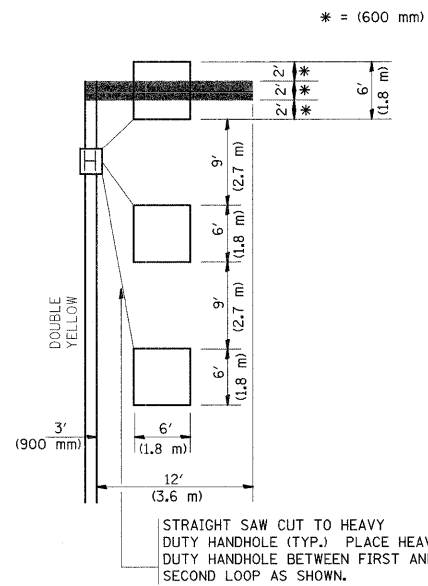
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

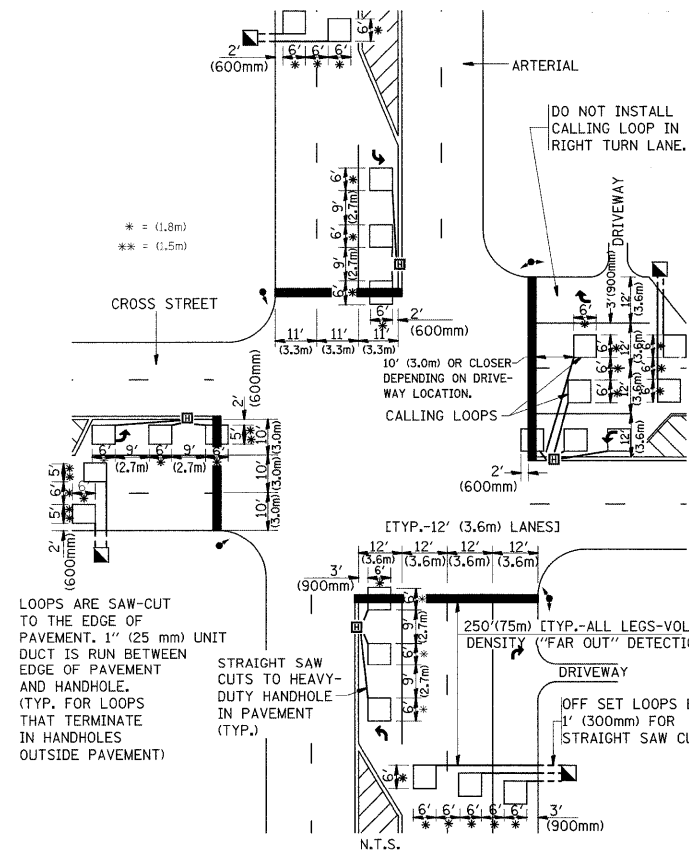
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**



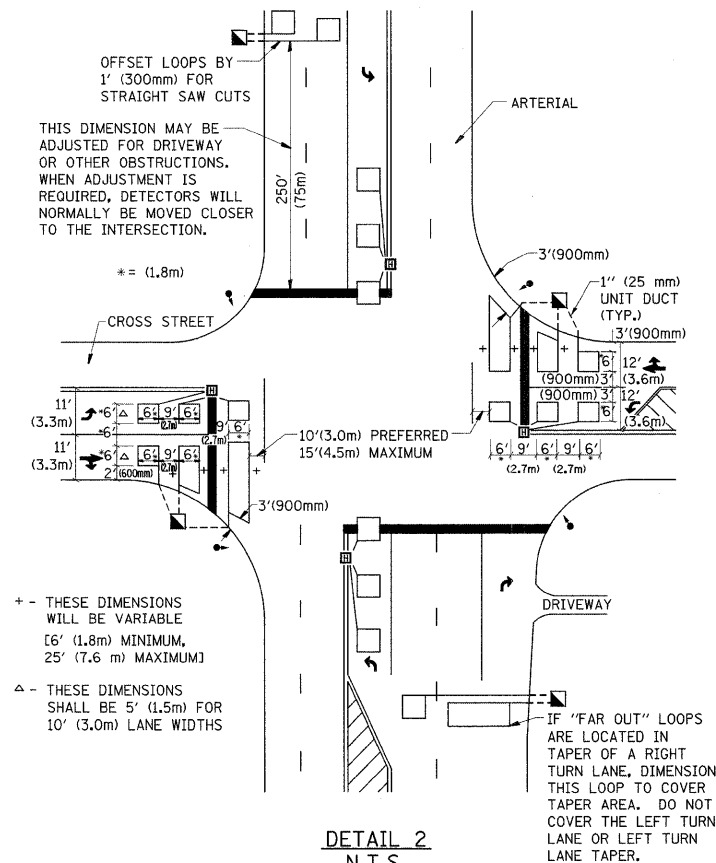
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1  
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2  
N.T.S.**

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = banks1	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\PIWIDOT\BANKSL\d0157014\DistS4.dgn		DRAWN -	REVISED -					332	9 Y-RS-8	WILL	25	25
PLOT SCALE = 50.0000' / IN.		CHECKED - R.K.F.	REVISED -					<b>TS-07</b>		<b>CONTRACT NO. 60140</b>		
PLOT DATE = 1/25/2010		DATE -	REVISED -					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	