F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10B-I	DUPAGE	20	1
FED ROAD DIST No. 1	ILLINOIS	CONTRACT No. 60J45		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED
HIGHWAY PLANS

FAP 311 / US 34 (OGDEN AVE) AT EAST BRANCH DUPAGE RIVER
BRIDGE DECK OVERLAY, JOINT REPAIR
SECTION 10 B–I
PROJECT NO. F-0311(043)
DUPAGE COUNTY
C–91–235–10

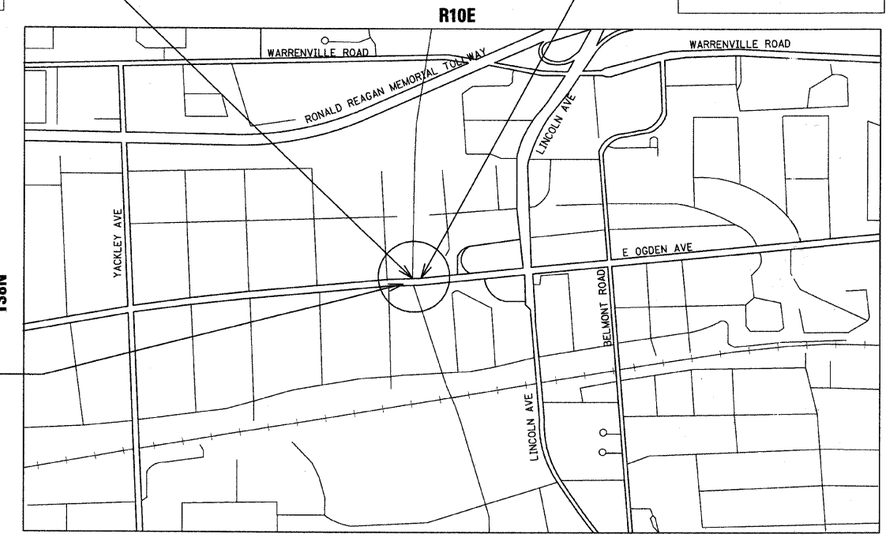
FOR INDEX OF SHEETS, SEE SHEET NO. 2

TRAFFIC DATA:
EXISTING ADT US 34 34400 (2007)
POSTED SPEED = 40 MPH

PROJECT LOCATION
US 34 (OGDEN AVE) OVER
EAST BRANCH DUPAGE RIVER
STRUCTURE NO. 022–0148

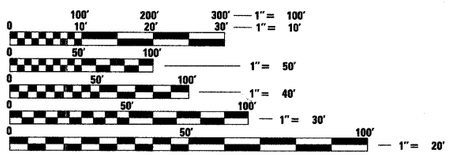
IMPROVEMENT ENDS
STA. 634 + 75.24

IMPROVEMENT BEGINS
STA. 633 + 47.16



LISLE TOWNSHIP
LOCATION MAP
NOT TO SCALE

PROJECT IS LOCATED IN THE VILLAGE OF LISLE

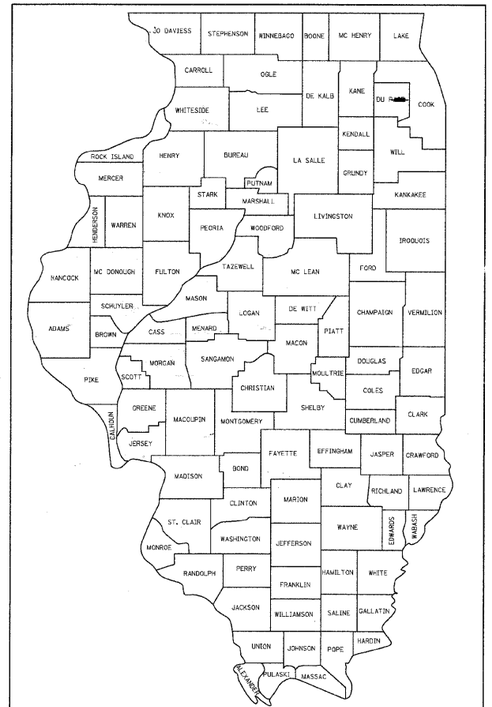


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1–800–892–0123 OR 811

GROSS LENGTH OF PROJECT = 128.08 FEET = 0.024 MILES
NET LENGTH OF PROJECT = 128.08 FEET = 0.024 MILES

D–91–235–10



LOCATION OF SECTION INDICATED THUS: –

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED JANUARY 22, 2010
Diane M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
March 19, 2010
Scott E. Stitt PE /for
ACTING ENGINEER OF DESIGN AND ENVIRONMENT
March 19, 2010
Christine M. Reed /for
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PROJECT MANAGER RAJENDRA SHAH (847) 705–4555
PROJECT ENGINEER MICHELLE AQUINO (847) 705–4606

CONTRACT NO. 60J45

TED W. LACHUS
062-040941
LICENSED PROFESSIONAL ENGINEER
STATE OF ILLINOIS

TED W. LACHUS, P.E.
EXPIRES 11-30-2011

1-29-2010
DATE

Primera
100 S. WACKER DRIVE SUITE 700. CHICAGO IL 60606.
P:312–606–0910 F:312–606–0415

INDEX OF SHEETS

STATE STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
~~701311-03~~	~~LANE CLOSURE 2 LANE 2 WAY MOVING OPERATIONS DAY ONLY~~
701606-02 ~~701602-04~~	URBAN LANE CLOSURE, MULTILANE, 2W WITH ~~BIDIRECTIONAL LEFT TURN LANE~~ MOUNTABLE MEDIAN
701901-01	TRAFFIC CONTROL DEVICES

GENERAL NOTES:

1. THESE PLANS HAVE BEEN PREPARED FROM INFORMATION ACQUIRED FROM EXISTING PLANS AND NOTES RECEIVED FROM IDOT FIELD MAINTENANCE ENGINEERS.
2. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO VARIATIONS FOUND IN THE FIELD. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. ANY ADJUSTMENTS PROPOSED BY THE CONTRACTOR MUST BE APPROVED BY THE ENGINEER. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED UPON THE UNIT PRICE.
3. QUANTITIES FOR DECK SLAB REPAIR ARE APPROXIMATE. LOCATIONS WILL BE DETERMINED BY THE ENGINEER FOLLOWING REMOVAL OF THE HMA SURFACE COURSE AND HYDRO-SCARIFICATION. ACTUAL REPAIR LOCATIONS SHALL BE SHOWN ON THE AS-BUILT PLANS.
4. FORTY- EIGHT HOURS BEFORE STARTING EXCAVATION, THE CONTRACTOR WILL CALL J.U.L.I.E. (1-800-892-0123) OR 811 FOR LOCATIONS OF THE EXISTING UTILITIES.
5. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
6. SAW CUTTING PRIOR TO ANY REMOVAL ITEMS NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS BEING REMOVED.
7. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
8. THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
9. WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AS WELL AS ADJOINING RESIDENTIAL AREAS.
10. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS, AS REQUIRED, PRIOR TO COMMENCING WITH CONSTRUCTION.
11. THE CONTRACTOR SHALL TAKE ALL NECESSARY MEASURES TO ASSURE THAT NO DEBRIS FALLS INTO THE WATERWAY. THE COST OF THIS WORK SHALL BE INCLUDED IN THE VARIOUS PAY ITEMS.
12. THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.
13. THE CONTRACTOR SHALL CONTACT DON CHIARUGI, THE AREA TRAFFIC FIELD ENGINEER, AT (847) 741-9857 TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
14. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OUTSIDE THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
15. ALL RAISED REFLECTIVE PAVEMENT MARKERS (BRIDGE) SHALL BE LOW PROFILE.
16. ACCESS SHALL BE PROVIDED AT ALL TIMES TO PROPERTIES ABUTTING THE PROPOSED IMPROVEMENT.

COMMITMENTS

NONE

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	**AIR VOIDS**
SURFACE COURSE	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	4% @ 90 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

The "AC Type" for Polymerized HMA Mixes SHALL BE "SBS/SBR PG 70 -22" AND
FOR NON-POLYMERIZED HMA THE "AC TYPE" shall be "PG 64 -22" UNLESS modified by District ONE Special Provisions.
FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

Note: For proposed section see sheet number S6.

TEMPORARY PAVEMENT

HOT-MIX ASPHALT BINDER IL-19MM 4%@50GYR

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 4% @50GYR (IL 9.5MM)

FILE NAME = $FILEL$

Primera

DESIGNED	VEA	REVISED -
DRAWN	VEA	REVISED -
CHECKED	TWL	REVISED -
DATE	1/29/2010	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEET, STATE STANDARDS, GENERAL NOTES AND COMMITMENTS

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10B-1	DUPAGE	20	2
		CONTRACT NO. 60J45		
	ILLINOIS FED. AID PROJECT			

SUMMARY OF QUANTITIES

CODE NO.	ITEM DESCRIPTION	UNIT	80% FED. 20% STATE TOTAL QUANTITY	CONTRUCTION TYPE CODE 022-0148 SFTY-2A QUANTITY	
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	22	22	
42001300	PROTECTIVE COAT	SQ YD	578	578	
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	254	254	
44000100	PAVEMENT REMOVAL	SQ YD	24	24	
	~~BRIDGE DECK OVERLAY REMOVAL~~	~~SQ YD~~	~~249~~	~~249~~	
50102400	CONCRETE REMOVAL	CU YD	44	44	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	46	46	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	9750	9750	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3	
67100100	MOBILIZATION	L SUM	1	1	
70101800	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	20	20	
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	10	10	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	2027	2027	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	610	610	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	500	500	
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	73	73	
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2311	2311	
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	200	200	
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	78	78	
*78006110	PREFORMED THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	392	392	

SUMMARY OF QUANTITIES

CODE NO.	ITEM DESCRIPTION	UNIT	80% FED. 20% STATE TOTAL QUANTITY	CONTRUCTION TYPE CODE 022-0148 SFTY-2A QUANTITY	
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	77	77	
*78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	6	6	
78300100	PAVEMENT MARKING REMOVAL	SQ FT	1152	1152	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	83	83	
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	51	51	
~~X0322276~~	~~CLEANING AND PAINTING EXPOSED REBAR~~	~~L SUM~~	~~1~~	~~1~~	
X0322944	BRIDGE DECK THIN POLYMER OVERLAY 3/8"	SQ YD	565	565	
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	121	121	
X0325775	WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4 INCH	FOOT	6080	6080	
X0326766	CLEAN & RESEAL RELIEF JOINT	FOOT	114	114	
44004600	SIDEWALK REMOVAL AND REPLACEMENT	SQ FT	304	304	
44001700	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	69	69	
Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	42	42	
Z0016001	DECK SLAB REPAIR (FULL DEPTH, TYPE I)	SQ YD	4	4	
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	18	18	
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	4	4	
X0326346	BRIDGE DECK LATEX CONCRETE OVERLAY, 3 INCHES	SQ YD	250	250	
Z0006199	BRIDGE DECK HYDRO-SCARIFICATION 3/8"	SQ YD	249	249	
Z0006229	BRIDGE DECK HYDRO-SCARIFICATION 3"	SQ YD	249	249	
X0320887	POLYMER CONCRETE	CU FT	4.0	4.0	

*SPECIALTY ITEMS

FILE NAME = #FILEL#

Primera

DESIGNED	VEA	REVISED	-
DRAWN	VEA	REVISED	-
CHECKED	TWL	REVISED	-
DATE	1/29/2010	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10B-1	DUPAGE	20	3

CONTRACT NO. 60J45

ILLINOIS FED. AID PROJECT

MAINTENANCE OF TRAFFIC GENERAL NOTES

1. THE MAINTENANCE OF TRAFFIC CONTROL (MOT) PLANS SHALL SERVE AS A GUIDE FOR SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT. HOWEVER, THE CONTRACTOR MAY MODIFY THE MOT PLANS TO MEET CONSTRUCTION NEEDS BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE MOT PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

2. THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY CHANGE TO THE MOT PLANS.

3. ALL EXISTING PAVEMENT MARKINGS IN CONFLICT WITH THE MAINTENANCE OF TRAFFIC STRIPING SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT, "PAVEMENT MARKING REMOVAL".

4. THE CONTRACTOR SHALL REMOVE ALL TEMPORARY PAVEMENT MARKING TAPE WHICH CONFLICTS WITH THE NEXT STAGE OR FINAL STRIPING. REMOVAL OF TEMPORARY PAVEMENT MARKING TAPE WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT, "WORK ZONE PAVEMENT MARKING REMOVAL".

5. ALL TRAFFIC CONTROL DEVICES USED FOR THE MAINTENANCE OF TRAFFIC, AS DETAILED ON THE PLANS, OR HIGHWAY STANDARD SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS SPECIFIED IN MAINTENANCE OF TRAFFIC SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.

6. ALL DRUMS, VERTICAL PANELS AND BARRICADES ADJACENT TO THE EDGE OF TRAVELED WAY SHALL BE EQUIPPED WITH STEADY-BURNING LIGHTS.

7. ALL EXISTING SIGNS WITHIN THE LIMITS OF MAINTENANCE OF TRAFFIC WHICH ARE OBSCURED BY OR OTHERWISE INTERFERED WITH BY THE CONSTRUCTION OPERATIONS AND MAINTENANCE OF TRAFFIC, SHALL BE COVERED OR REMOVED BY THE CONTRACTOR UNLESS SPECIFIED IN THE PLANS OR WHEN DIRECTED BY THE ENGINEER. THIS WORK SHALL BE IN ACCORDANCE WITH ARTICLE 107.25 OF THE IDOT STANDARD SPECIFICATIONS.

8. TEMPORARY, OFF-PEAK HOUR LANE CLOSURES MUST BE REQUESTED THROUGH THE ENGINEER AND AS SPECIFIED IN THE SPECIAL PROVISIONS, WHEN OFF-PEAK HOUR OR WEEKEND LANE CLOSURES ARE REQUIRED, A PORTABLE CHANGEABLE MESSAGE SIGN SHALL BE INSTALLED ONE WEEK PRIOR TO THE CLOSURE. THE MESSAGE SIGN WORDING AND LOCATION WILL BE DETERMINED BY THE ENGINEER.

9. THE CONTRACTOR SHALL PLACE A CHANGEABLE MESSAGE SIGN AT EACH END OF THE PROJECT AND/OR AS DIRECTED BY THE ENGINEER TO INFORM MOTORISTS OF UPCOMING CONSTRUCTION ACTIVITIES. THE MESSAGE SIGNS WITH THE APPROPRIATE INFORMATION SHALL BE IN PLACED TWO WEEKS BEFORE START OF CONSTRUCTION ACTIVITY. THIS WORK IS TO BE PAID FOR AT THE CONTRACT UNIT PRICE PER CALENDAR MONTH, "CHANGEABLE MESSAGE SIGN".

10. ALL TEMPORARY INFORMATION SIGNS SHALL BE PAID FOR SEPARATELY AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR "TEMPORARY INFORMATION SIGNING".

11. FOR ADDITIONAL BRIDGE CONSTRUCTION STAGING INFORMATION, SEE STRUCTURAL PLANS.

SUGGESTED CONSTRUCTION SEQUENCING

PRESTAGE

CONSTRUCTION:
IMPLEMENT STAGE 1 MOT PAVEMENT MARKING AND TRAFFIC CONTROL

MAINTENANCE OF TRAFFIC:
UTILIZE STANDARDS 701606-06.

STAGE 1

CONSTRUCTION:
ELIMINATE EXISTING LONGITUDINAL OPEN JOINT.

MAINTENANCE OF TRAFFIC:
UTILIZE MAINTENANCE OF TRAFFIC DETAILS IN THE PLANS AND STANDARD 701606-06.

STAGE 2

CONSTRUCTION:
WESTBOUND LANES: REMOVE BITUMINOUS OVERLAY, HYDROSCARIFY CONCRETE SLAB SURFACE, DECK REPAIRS, PLACE LATEX CONCRETE OVERLAY, AND SUBSTRUCTURE REPAIRS.

MAINTENANCE OF TRAFFIC:
UTILIZE MAINTENANCE OF TRAFFIC DETAILS IN THE PLANS AND STANDARD 701606-06.

STAGE 3

CONSTRUCTION:
EASTBOUND LANES: REMOVE BITUMINOUS OVERLAY, HYDROSCARIFY CONCRETE SLAB SURFACE & FULL DEPTH SLAB REPAIRS, PLACE LATEX CONCRETE OVERLAY, AND SUBSTRUCTURE REPAIRS.

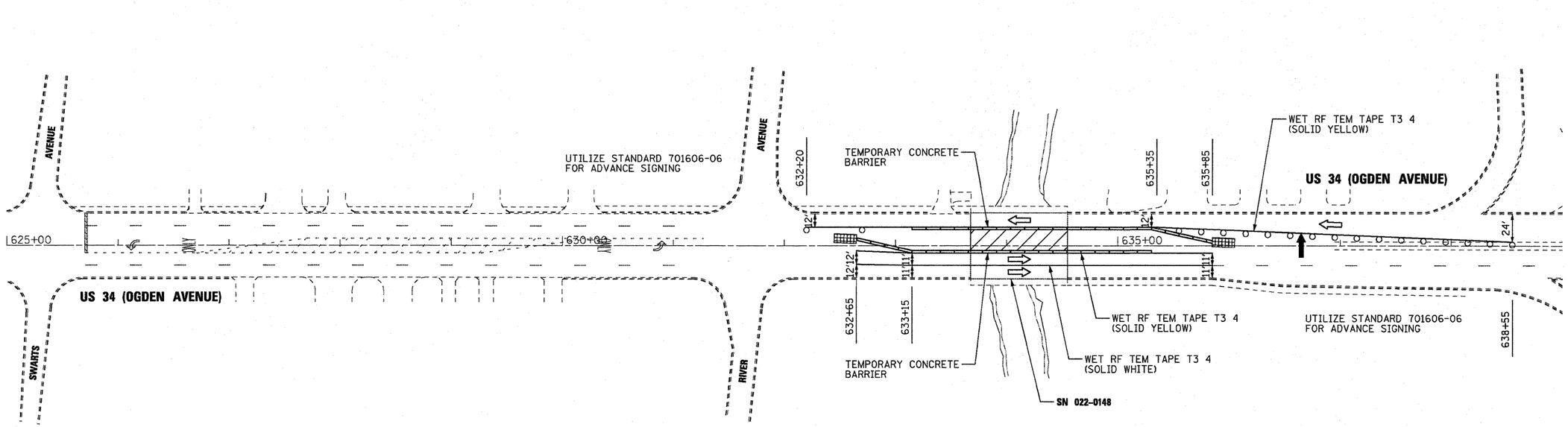
MAINTENANCE OF TRAFFIC:
UTILIZE MAINTENANCE OF TRAFFIC DETAILS IN THE PLANS AND STANDARD 701606-06.

STAGE 4

CONSTRUCTION:
PLACE FINAL PAVEMENT MARKINGS AND RAISED REFLECTIVE MARKERS.

1 MAINTENANCE OF TRAFFIC:
UTILIZE STANDARDS 701311-03 AND 701606-06.

FILE NAME = $FILEL$	Primera	DESIGNED	TWL	REVISED - 2/3/2010	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MOT GENERAL NOTES AND SUGGESTED CONSTRUCTION SEQUENCING	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN	VEA	REVISED -			311	10B-I	DUPAGE	20	4
		CHECKED	SF	REVISED -					CONTRACT NO. 60J45		
		DATE	1/29/2010	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT		



LEGEND:

- WORK AREA
- TEMPORARY CONCRETE BARRIER
- ARROW BOARD
- SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURNING LIGHTS @ 50 FT CENTERS ALONG ROADWAY (TANGENT) AND 20 FT CENTERS ALONG TAPERS
- TYPE III BARRICADE
- IMPACT ATTENUATOR, TEMPORARY NON-REDIRECTIVE
- DIRECTION OF TRAFFIC

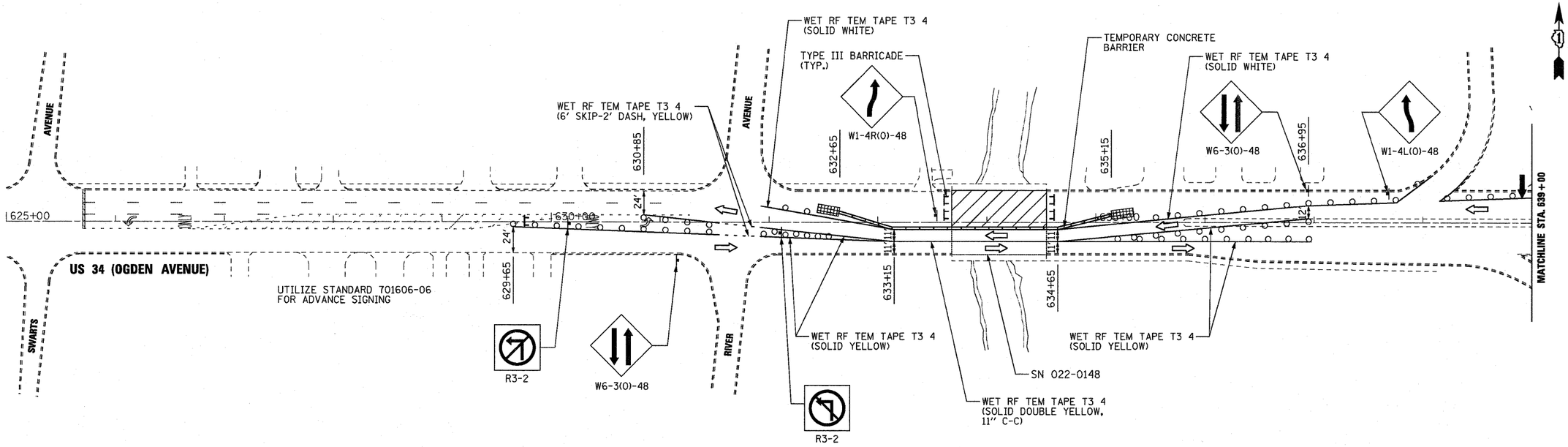
FILE NAME = ⊕FILEL⊕	Primera	DESIGNED TWL	REVISED - 2/3/2010
		DRAWN JLS	REVISED -
		CHECKED SF	REVISED -
		DATE 1/29/2010	REVISED -

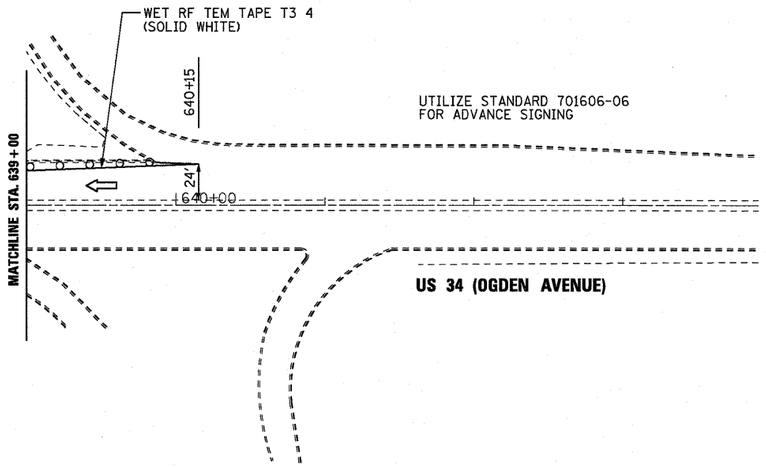
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGE OF CONSTRUCTION & TRAFFIC CONTROL
STAGE 1
US 34 (OGDEN AVENUE)

SCALE: 1" = 50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10B-I	DUPAGE	20	5
		CONTRACT NO. 60J45		
	ILLINOIS FED. AID PROJECT			

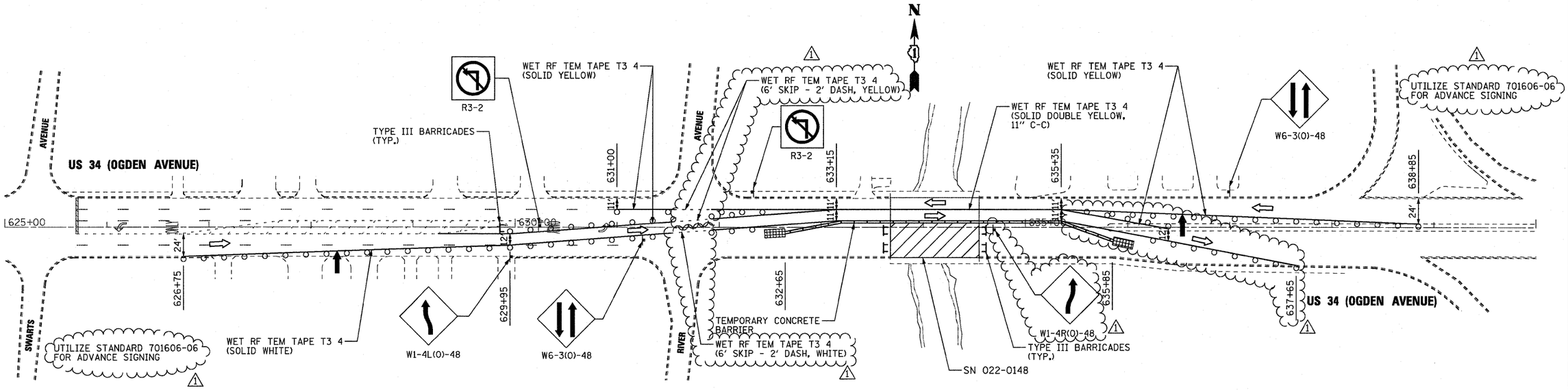




LEGEND:

- WORK AREA
- TEMPORARY CONCRETE BARRIER
- ARROW BOARD
- SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURNING LIGHTS @ 50 FT CENTERS ALONG ROADWAY (TANGENT) AND 20 FT CENTERS ALONG TAPERS
- TYPE III BARRICADE
- IMPACT ATTENUATOR, TEMPORARY NON-REDIRECTIVE
- DIRECTION OF TRAFFIC

FILE NAME = $FILEL$	Primera	DESIGNED TWL	REVISED - 2/3/2010	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED STAGE OF CONSTRUCTION & TRAFFIC CONTROL STAGE 2 US 34 (OGDEN AVENUE)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN JLS	REVISED -			311	10B-I	DUPAGE	20	6
		CHECKED SF	REVISED -					CONTRACT NO. 60J45		
		DATE 1/29/2010	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT		



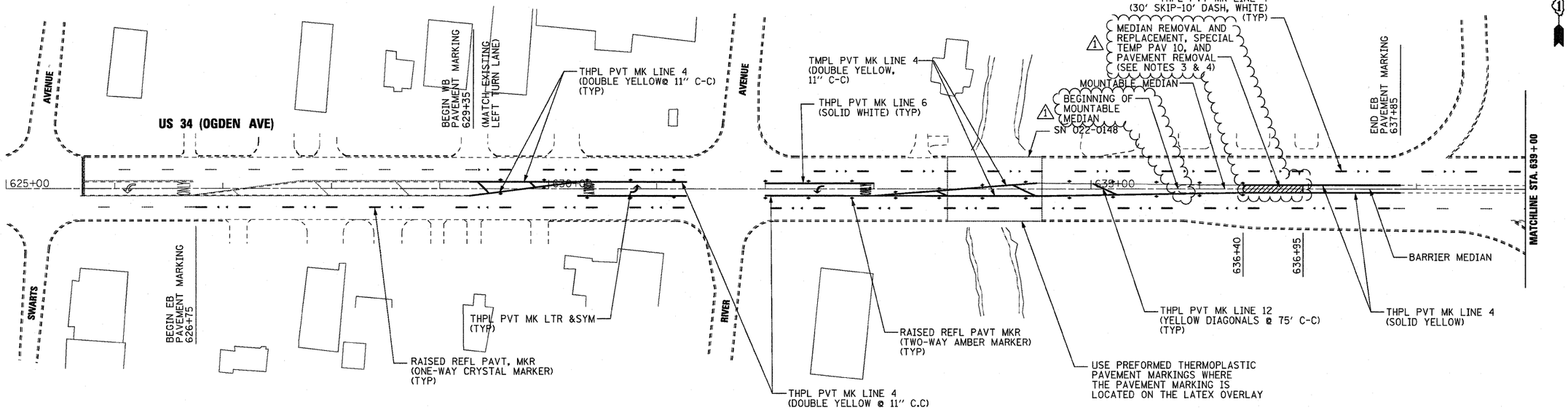
LEGEND:

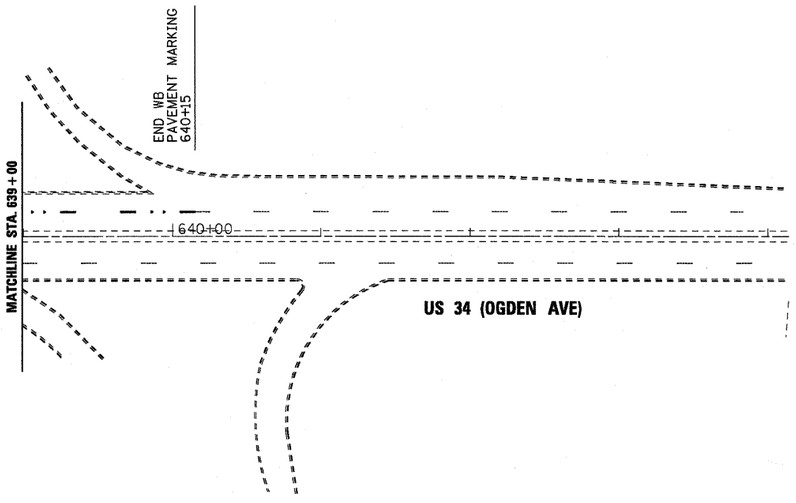
- WORK AREA
- TEMPORARY CONCRETE BARRIER
- ARROW BOARD
- SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURNING LIGHTS @ 50 FT CENTERS ALONG ROADWAY (TANGENT) AND 20 FT CENTERS ALONG TAPERS
- TYPE III BARRICADE
- IMPACT ATTENUATOR, TEMPORARY NON-REDIRECTIVE
- DIRECTION OF TRAFFIC

FILE NAME = $FILEL$	Primera		
DESIGNED TWL	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED STAGE OF CONSTRUCTION & TRAFFIC CONTROL STAGE 3 US 34 (OGDEN AVENUE)
DRAWN JLS	REVISED -		
CHECKED SF	REVISED -		
DATE 1/29/2010	REVISED -		

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10B-I	DUPAGE	20	7
		CONTRACT NO. 60J45		
	ILLINOIS FED. AID PROJECT			





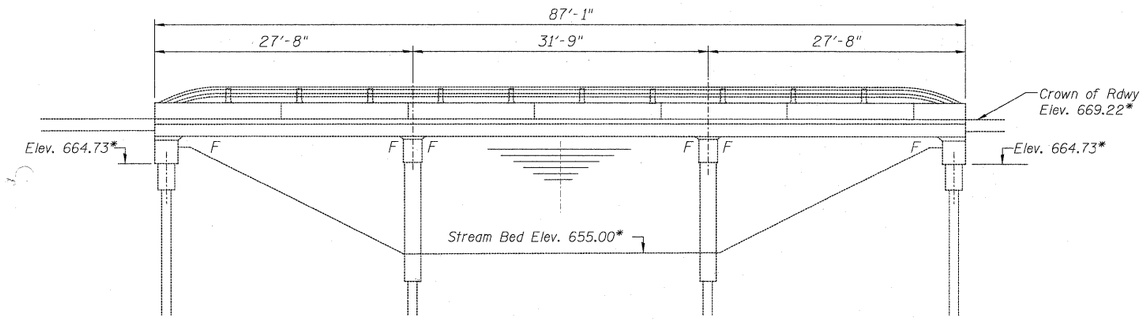
NOTES:

1. REFER TO DISTRICT 1 DETAILS "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" AND "TYPICAL PAVEMENT MARKING" FOR ADDITIONAL INFORMATION.
2. THE PROPOSED PAVEMENT MARKINGS SHALL BE PLACED IN THE SAME LOCATIONS AS THE EXISTING PAVEMENT MARKINGS OR AS DIRECTED BY THE ENGINEER
3. THE TEMPORARY PAVEMENT 10" WILL BE COMPRISED OF: 8 1/2" HOT-MIX ASPHALT BINDER IL-19MM & 1 1/2" HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5MM)
4. ANY EARTH EXCAVATION REQUIRED TO CONSTRUCT THE TEMPORARY PAVEMENT OR CONSTRUCT THE NEW MEDIAN WILL BE CONSIDERED INCIDENTAL TO THE PAY ITEMS, TEMPORARY PAVEMENT, 10", AND MEDIAN REMOVAL AND REPLACEMENT, SPECIAL

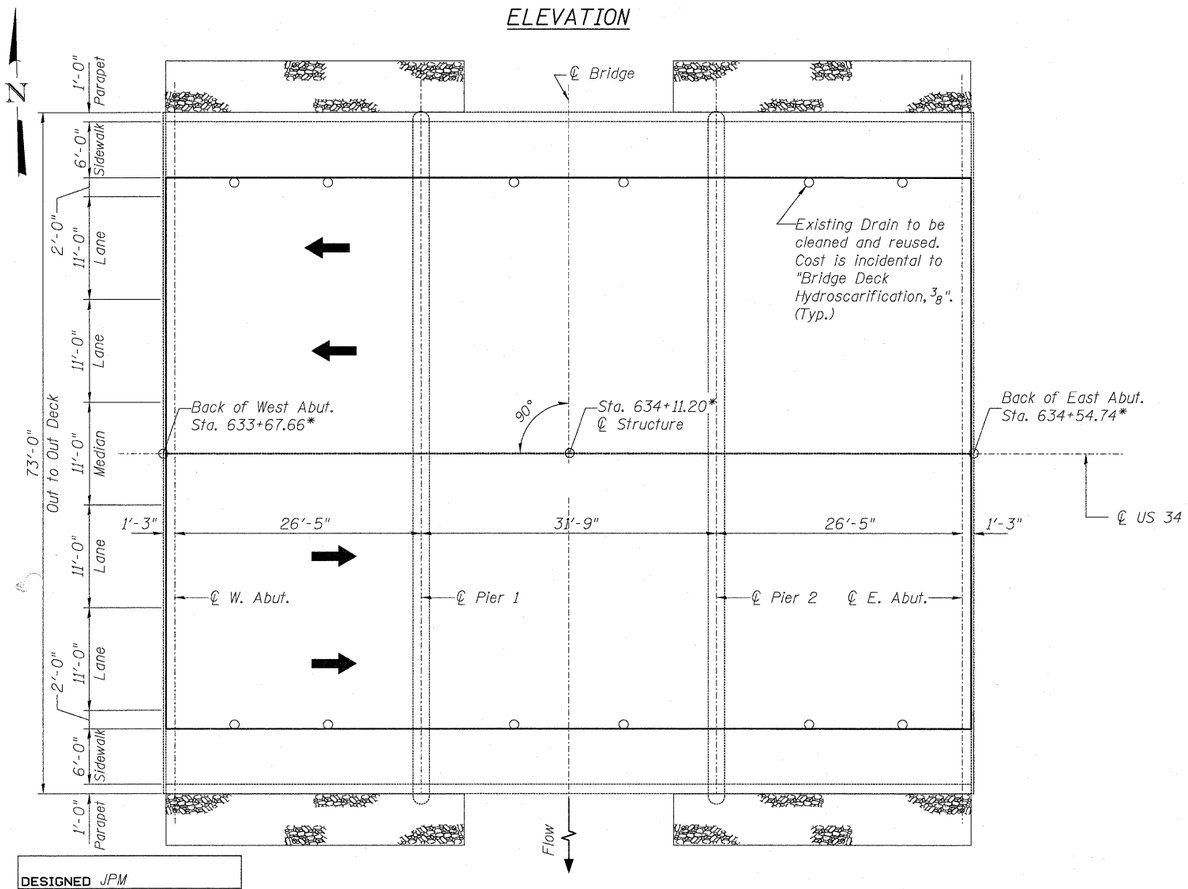
FILE NAME = $FILEL$	Primera	DESIGNED TWL	REVISED - 2/3/2010	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING AND ROADWAY PLAN US 34 (OGDEN AVENUE)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN JLS	REVISED -			311	10B-I	DUPAGE	20	8
		CHECKED SF	REVISED -					CONTRACT NO. 60J45		
		DATE 1/29/2010	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT		

Existing Structure: SN 022-0148 - In 1982 the bridge was completely replaced with shorter and wider structure with three-span Reinforced Concrete slab. The structure measures 73'-0" Out to Out Deck and 87'-1" Bk. to Bk. Abutments. The substructures consists of Reinforced Concrete integral abutments and two wall type piers.
Traffic is to be maintained utilizing stage construction. One lane for both directions will be provided.

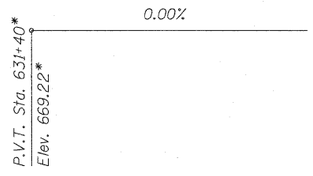
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ELEVATION



PLAN



PROFILE GRADE AT ℄ STRUCTURE

Information taken from 1982 plans

SCOPE OF WORK

1. Remove existing latex concrete overlay (WB lane).
2. Hydroscarify 3/8 inch slab surface.
3. Structural repairs of concrete to parapets.
4. Full & partial depth slab repairs.
5. Eliminate longitudinal joint in median.
6. 3 inch latex concrete overlay (WB lane).
7. Patch & Overlay approaches with Thin Polymer.
8. Reconstruct Pavement Relief Joints.

INDEX OF SHEETS

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

DESIGN SPECIFICATIONS

2002 AASHTO Standard Specifications, 17th Edition.

DESIGN STRESSES

FIELD UNITS

f'c = 3,500 psi
fy = 60,000 psi

LAST DELAMINATION SURVEY

October 2009

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Bridge Deck Hydro-Scarification 3"	Sq. Yd.	249	-	249
Concrete Removal	Cu. Yd.	44	-	44
Concrete Superstructure	Cu. Yd.	46	-	46
Reinforcement Bars, Epoxy Coated	Pound	9,750	-	9,750
Bridge Deck Hydro-scarification 3/8"	Sq. Yd.	249	-	249
Bridge Deck Thin Polymer Overlay, 3/8"	Sq. Yd.	565	-	565
Protective Coat	Sq. Yd.	578	-	578
Polymer Concrete	Cu. Ft.	4	-	4
Bridge Deck Latex Concrete Overlay, 3"	Sq. Yd.	250	-	250
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	121	-	121
Approach Slab Repair (Partial Depth)	Sq. Yd.	42	-	42
Deck Slab Repair (Full Depth Type I)	Sq. Yd.	4	-	4
Deck Slab Repair (Full Depth Type II)	Sq. Yd.	18	-	18
Sidewalk Removal & Replacement	Sq. Ft.	304	-	304
Comb. Curb & Gutter Removal & Replacement	Ft.	69	-	69
Polymerized Hot-Mix Asphalt Surface Course, Mix "F", N-90, 2"	Tons	22	-	22
Hot-Mix Asphalt Surface Removal, 2"	Sq. Yd.	254	-	254
Clean and Reseal Relief Joint	Foot	114	-	114

1

THEODORE P. GEORGAS
081-004609
REGISTERED STRUCTURAL ENGINEER
STATE OF ILLINOIS

3-26-10
Theodore P. Georgas Date
Licensed Structural Engineer
State of Illinois 081-4609
Expires 11/30/2010

* Information taken from 1982 existing plans.

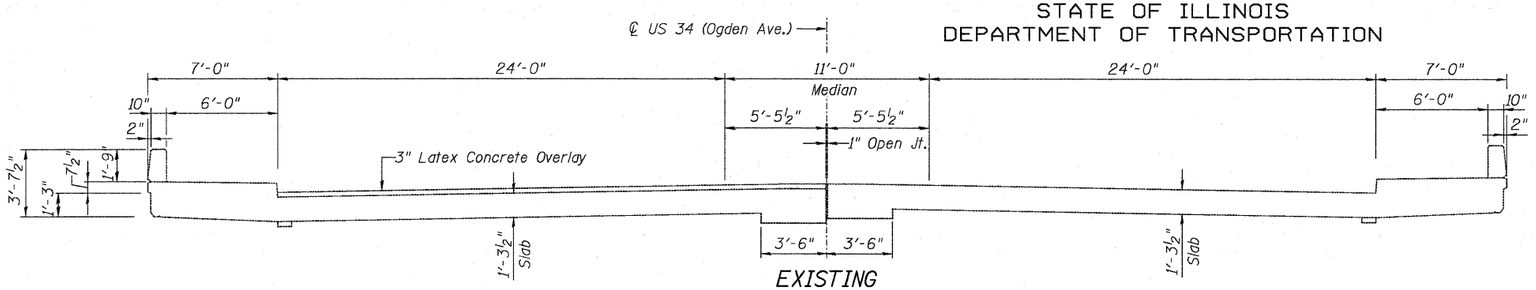
DESIGNED	JPM
CHECKED	TG
DRAWN	MPS
CHECKED	JPM, TG

Primera
100 S. WACKER DRIVE SUITE 700 . CHICAGO IL 60606 . P:312-606-0900 F:312-606-0415

GENERAL PLAN AND ELEVATION
FAP 311/US34 (OGDEN AVE) OVER EAST BRANCH DUPAGE RIVER
DUPAGE COUNTY
STATION 634+11.20
STRUCTURE NO. 022-0148

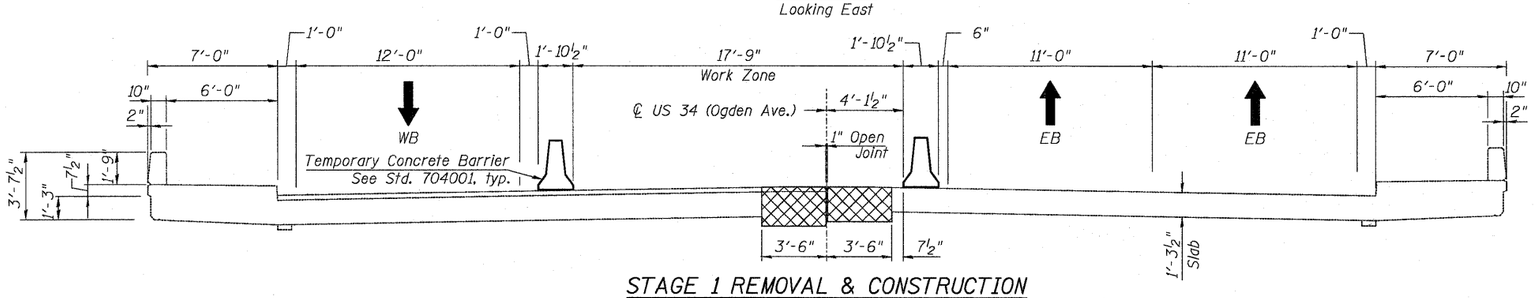
SHEET NO. S1	F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
S7 SHEETS	311	10 B-I	DUPAGE	20	9
			CONTRACT NO. 60J45		
		ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



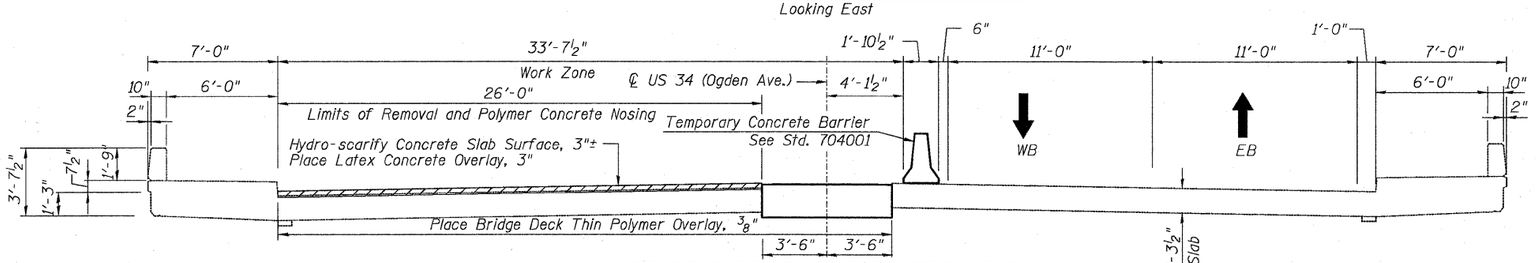
EXISTING

Looking East



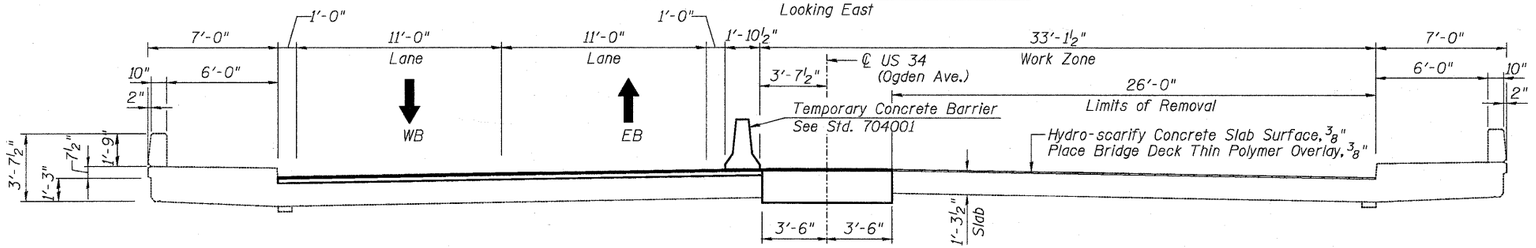
STAGE 1 REMOVAL & CONSTRUCTION

Looking East



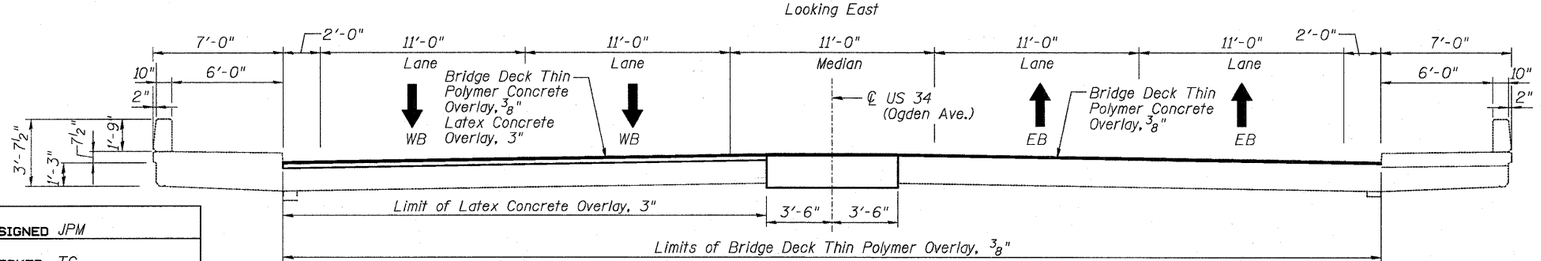
STAGE 2 REMOVAL & CONSTRUCTION

Looking East



STAGE 3 REMOVAL & CONSTRUCTION

Looking East



FINAL

Looking East

LEGEND:

- Bridge Deck Hydro-Scarification 3"±
- Bridge Deck Hydro-scarification ⅜"
- Concrete Removal

STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 022-0148

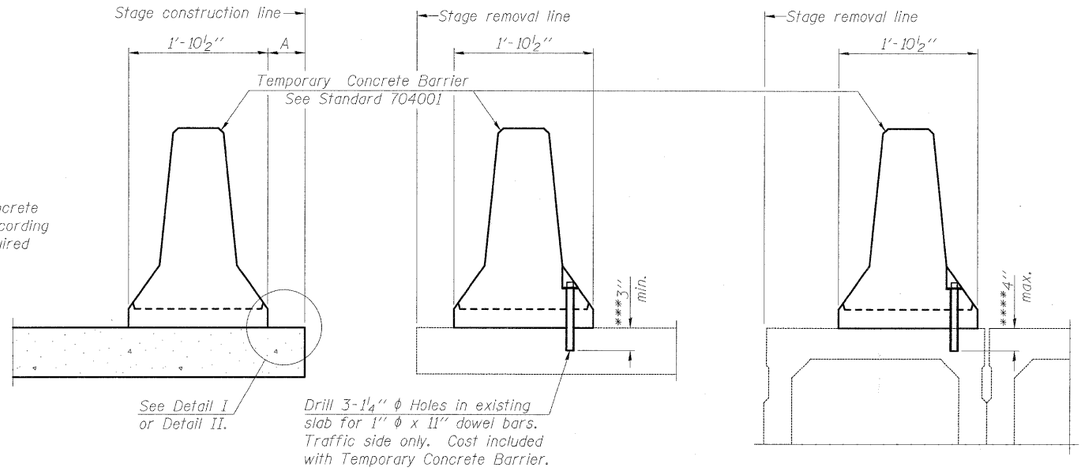
DESIGNED	JPM
CHECKED	TG
DRAWN	MPS
CHECKED	JPM, TG

Primera
188 S. WACKER DRIVE SUITE 700 . CHICAGO IL. 60606 . P:312-686-8700 F:312-686-8415

SHEET NO. S2	F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
S7 SHEETS	311	10 B-I	DUPAGE	20	10
			CONTRACT NO. 60J45		
		ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



NEW SLAB | EXISTING SLAB | EXISTING DECK BEAM

NOTES

Detail I - With Bar Splicer or Couplers:
Connect one (1) 1"x7"x10" steel ℄ to the top layer of couplers with 2-⅝" ϕ bolts screwed to coupler at approximate ℄ of each barrier panel.

Detail II - With Extended Reinforcement Bars:
Connect one (1) 1"x7"x 10" steel ℄ to the concrete slab or concrete wearing surface with 2-⅝" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate ℄ of each barrier panel.

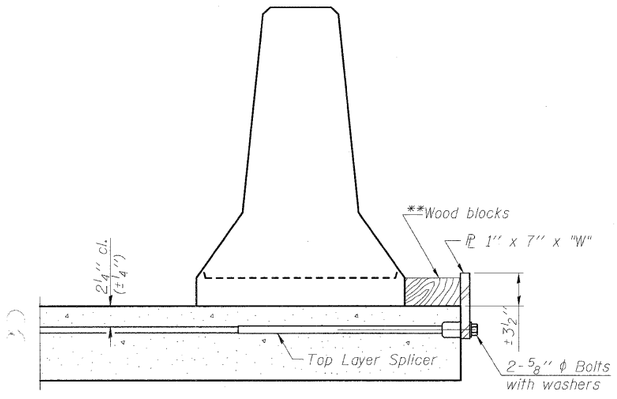
Cost of anchorage is included with Temporary Concrete Barrier.

The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

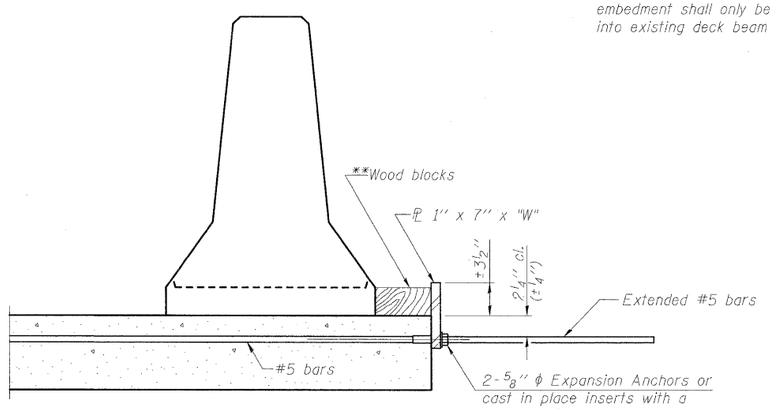
SECTIONS THRU SLAB OR DECK BEAM

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



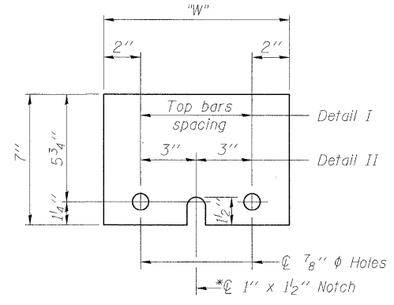
DETAIL I



DETAIL II

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

"W" = Top bars spacing + 4"



STEEL RETAINER ℄ 1" x 7" x 10"

* Required only with Detail II

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION STRUCTURE NO. 022-0148

DESIGNED	JPM
CHECKED	TG
DRAWN	MPS
CHECKED	JPM, TG

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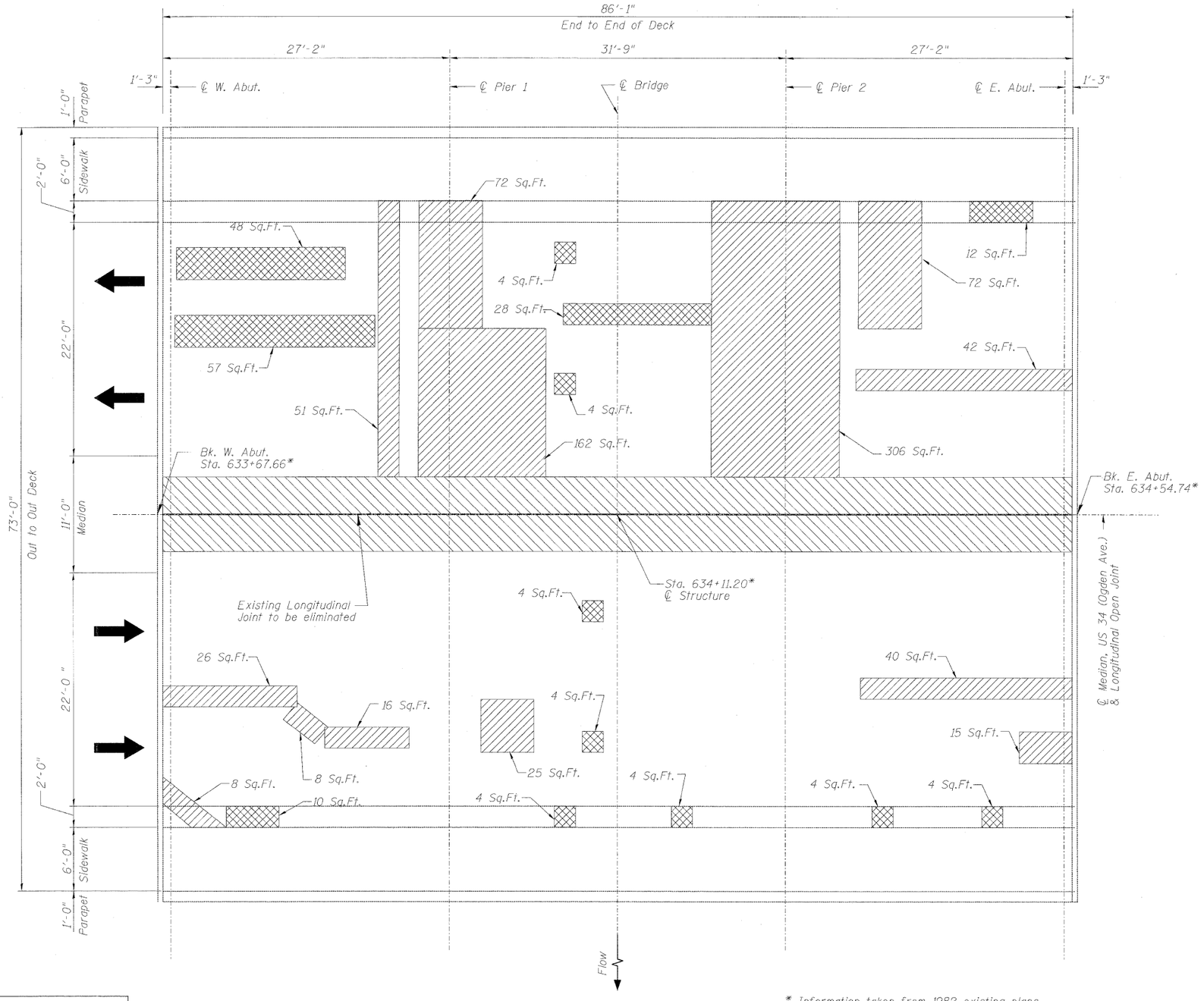
100 S. WACKER DRIVE SUITE 700 . CHICAGO IL 60606 . P:312-686-0900 F:312-686-0415

R-27 11 1 09

SHEET NO. S3	F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
S7 SHEETS	311	10 B-I	DUPAGE	20	11
			CONTRACT NO. 60J45		
		ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



PLAN

Note:
Repair of the existing bridge deck shall include but may not be limited to the areas shown. The actual areas to be repaired will be determined by the Engineer at the time of construction.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	4
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	18
Concrete Removal	Cu. Yd.	44

LEGEND:

- Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth)
- Concrete Removal

* Information taken from 1982 existing plans.

BRIDGE DECK PATCHING PLAN
STRUCTURE NO. 022-0148

DESIGNED	JPM
CHECKED	TG
DRAWN	MPS
CHECKED	JPM, TG

Primera
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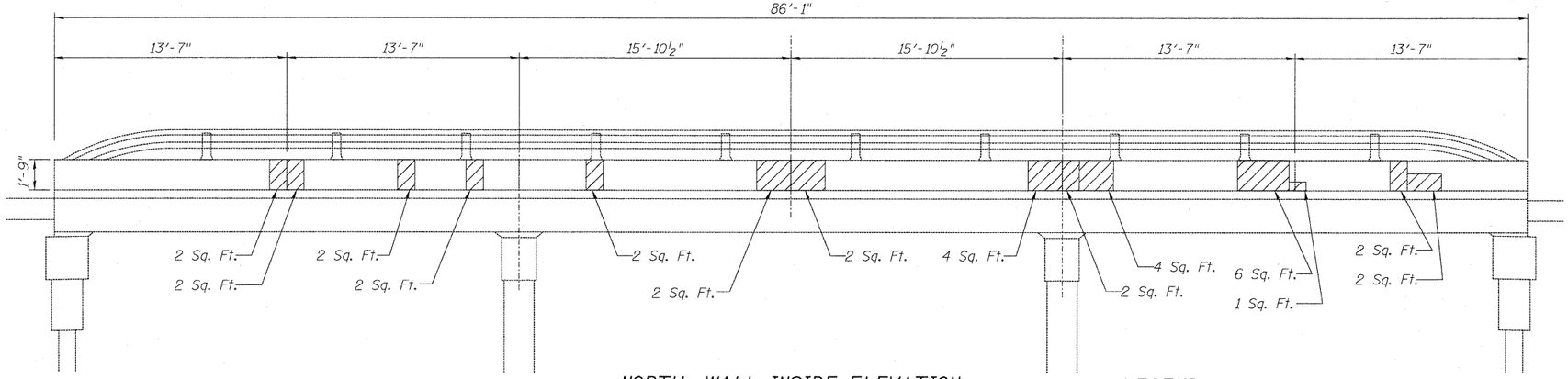
SHEET NO. 54	F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57 SHEETS	311	10 B-I	DUPAGE	20	12
			CONTRACT NO. 60J45		
		ILLINOIS	FED. AID PROJECT		

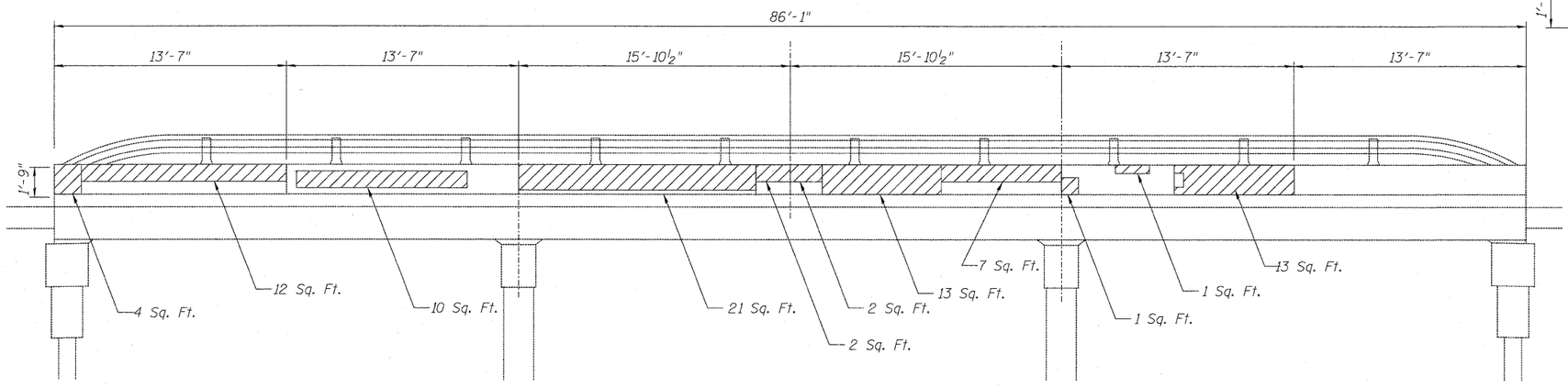
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



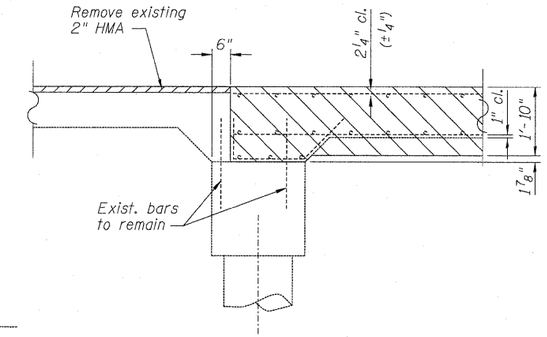
NORTH WALL INSIDE ELEVATION

LEGEND

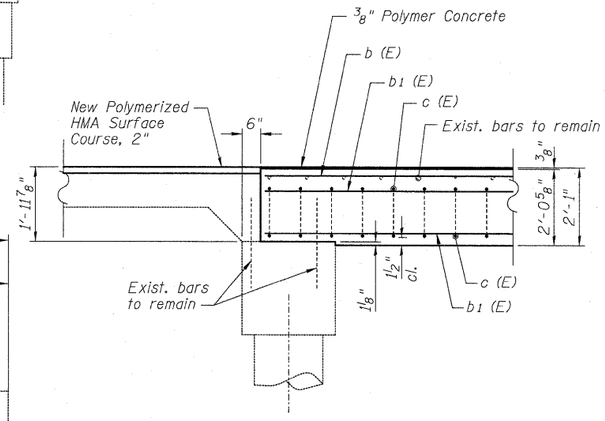
Structural Repair of Concrete
(Depth equal to or less than 5 inches)



SOUTH WALL INSIDE ELEVATION

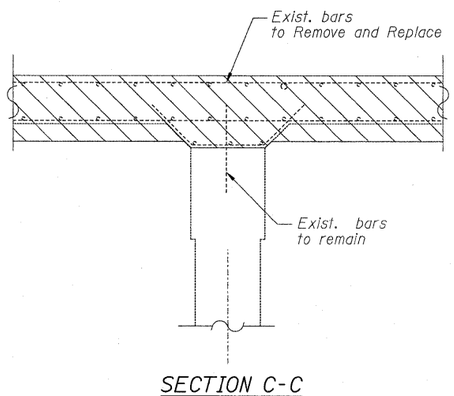


SECTION B-B
EXISTING

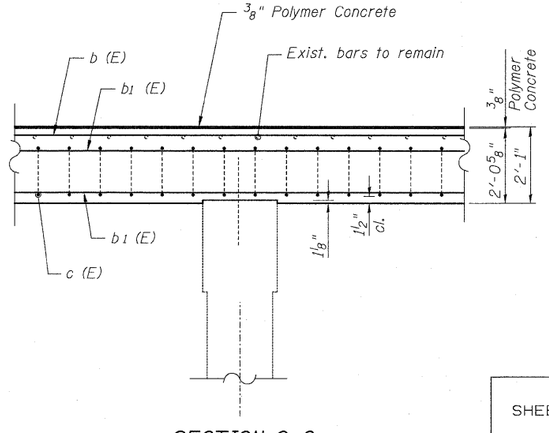


SECTION B-B
PROPOSED

Notes:
1. Repair of the existing parapet shall include but may not be limited to the areas shown. The actual areas to be repaired will be determined by the Engineer at the time of construction.
2. Exist. bars to remain shall be cleaned, straightened and incorporated into new concrete.
3. Work this Sheet with Sheet S7.
4. Exposed and rusty rebars in the spalled areas parapets shall be cleaned and painted prior to patching.



SECTION C-C
EXISTING



SECTION C-C
PROPOSED

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq.Ft.	121

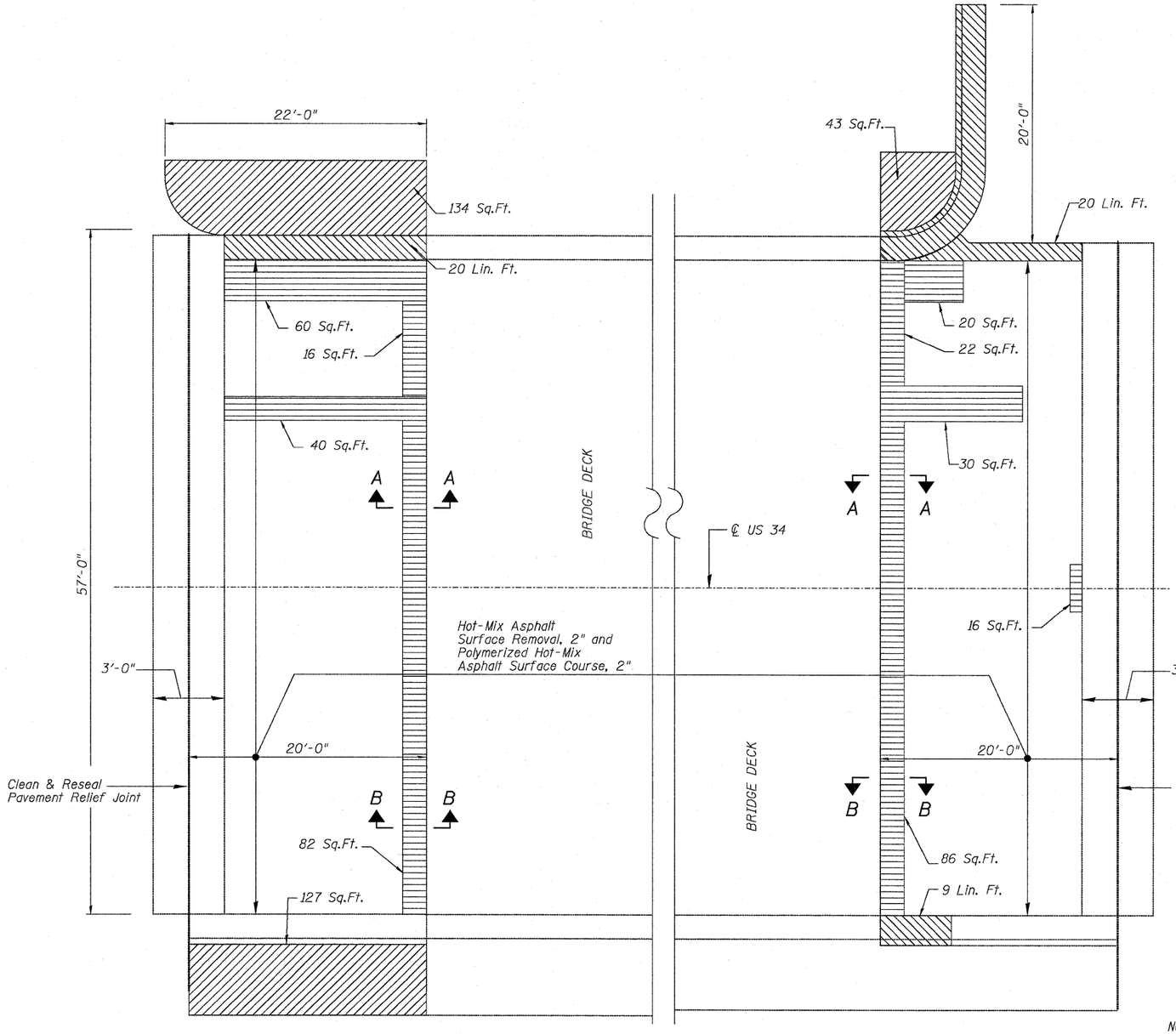
PARAPET REPAIR & MEDIAN SECTION DETAILS
STRUCTURE NO. 022-0148

DESIGNED	JPM
CHECKED	TG
DRAWN	MPS
CHECKED	JPM, TG

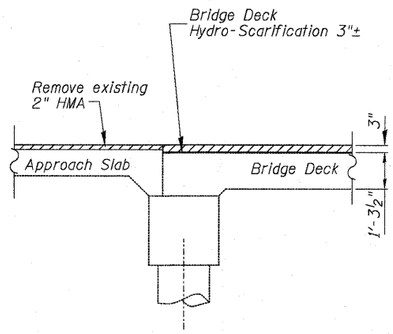
Primera
120 S. WACKER DRIVE SUITE 700 . CHICAGO IL 60606 . P:312-686-8900 F:312-686-8415

SHEET NO. S5	F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
S7 SHEETS	311	10 B-I	DUPAGE	20	13
			CONTRACT NO. 60J45		
		ILLINOIS	FED. AID PROJECT		

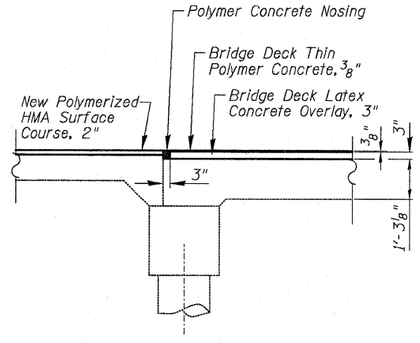
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



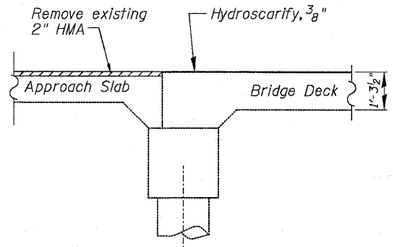
PLAN



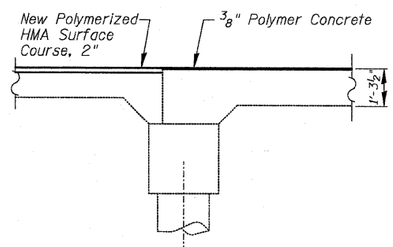
SECTION A-A
EXISTING



SECTION A-A
PROPOSED



SECTION B-B
EXISTING



SECTION B-B
PROPOSED

Note:
For median section details,
see Sheet S5.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Approach Slab Repair (Partial Depth)	Sq. Yd.	42
Sidewalk Removal & Replacement	Sq. Ft.	304
Comb. Curb & Gutter Removal & Replacement	Ft.	69
Hot-Mix Asphalt Surface Removal, 2"	Sq. Yd.	254
Polymerized Hot-Mix Asphalt Surface Course, Mix "F", N90, 2"	Tons	22
Polymer Concrete	Cu. Ft.	4
Clean and Reseal Relief Joint	Foot	114

Note:
Repair of the existing bridge approaches shall include but may not be limited to the areas shown. The actual areas to be repaired will be determined by the Engineer at the time of construction.

LEGEND:

Sidewalk Removal and Replacement

Approach Slab Repair (Partial Depth)

Curb & Gutter Removal and Replacement

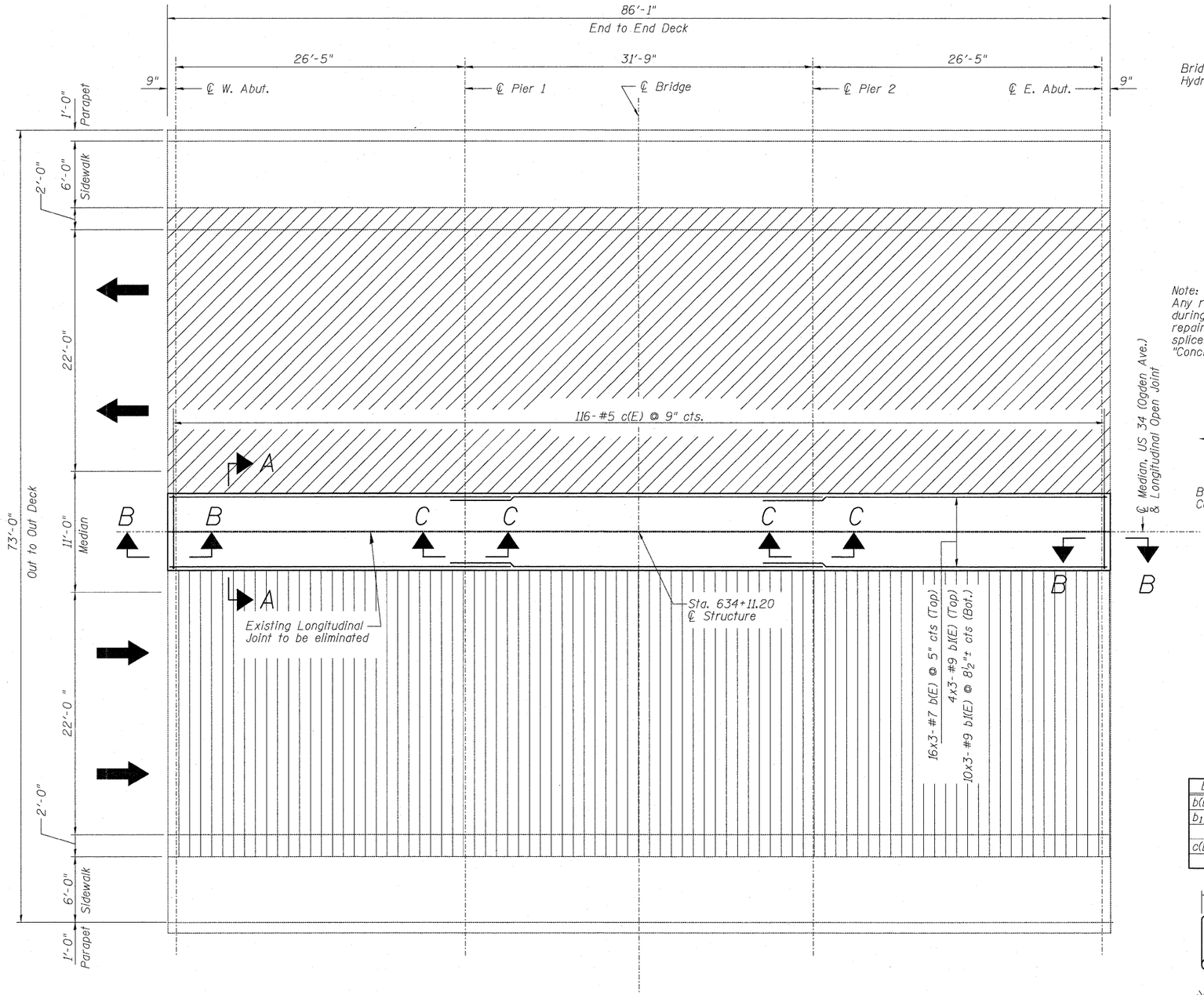
APPROACH SLAB REPAIR DETAILS
STRUCTURE NO. 022-0148

DESIGNED	JPM
CHECKED	TG
DRAWN	MPS
CHECKED	JPM, TG

Primera
188 S. WACKER DRIVE SUITE 700 , CHICAGO IL 60606 , P:312-606-8900 F:312-606-8415

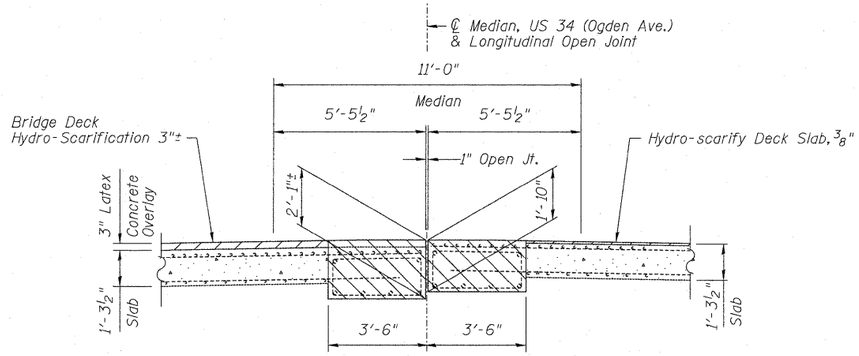
SHEET NO. S6 S7 SHEETS	F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	311	10 B-I	DUPAGE	20	14
			CONTRACT NO. 60J45		
		ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



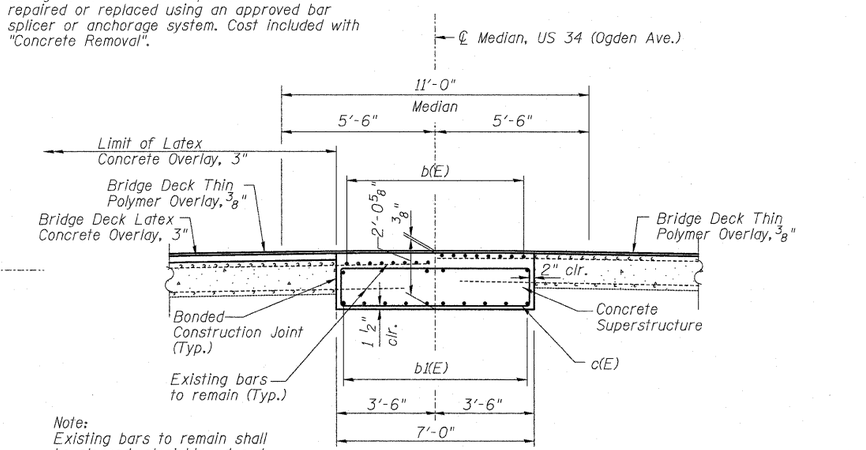
PLAN

Note:
See Sheet S5 for Section B-B and Section C-C Details.



SECTION A-A
EXISTING

Note:
Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".



Note:
Existing bars to remain shall be cleaned, straightened and incorporated into new concrete.

SECTION A-A
PROPOSED

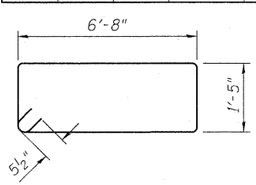
BAR LIST

Bar	No.	Size	Length	Shape
b(E)	48	#7	31'-0"	
b_1(E)	42	#9	32'-6"	
c(E)	116	#5	17'-1"	

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Bridge Deck Hydro-scarification 3/8"	Sq. Yd.	249
Bridge Deck Hydro-Scarification 3"	Sq. Yd.	249
Reinforcement Bar, Epoxy Coated	Pound	9,750
Concrete Superstructure	Cu. Yd.	46
Bridge Deck Thin Polymer Overlay 3/8"	Sq. Yd.	565
Bridge Deck Latex Concrete Overlay 3"	Sq. Yd.	250

1



Bar c(E)

Note:
Bars indicated thus 4x3-#9 etc. indicates 4 lines of bars with 3 lengths per line.

MINIMUM BAR LAP

#7 bars = 3'-5"
#9 bars = 5'-9"

LEGEND:

- Bridge Deck Hydro-Scarification 3"±
- Concrete Removal
- Bridge Deck Hydro-Scarification 3/8"

DESIGNED	JPM
CHECKED	TG
DRAWN	MPS
CHECKED	JPM, TG

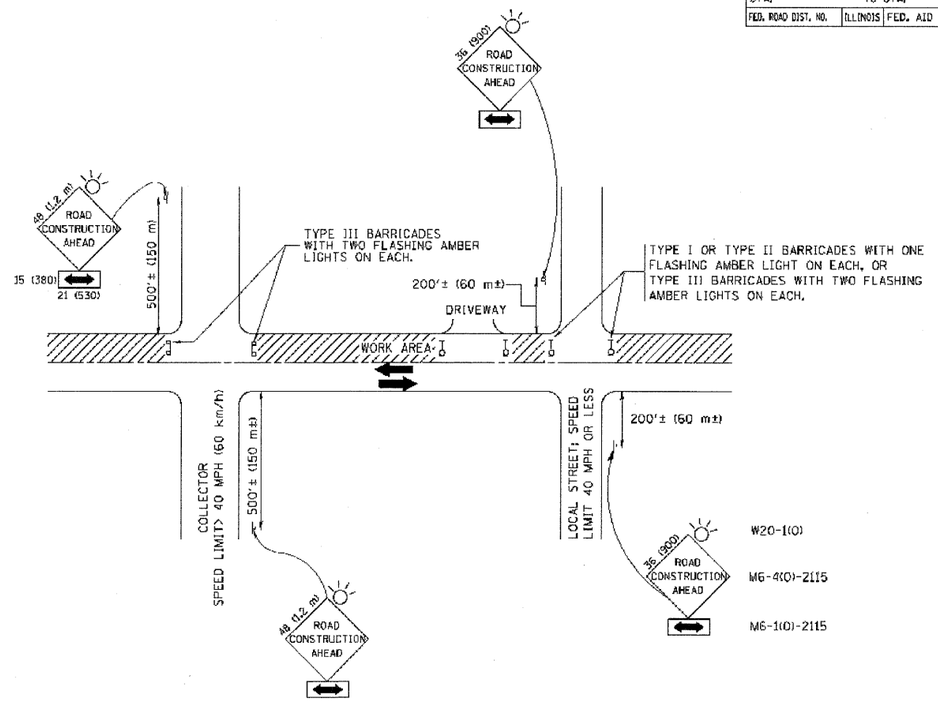
Primera
100 S. WACKER DRIVE SUITE 700 . CHICAGO IL 60606 . P:312-686-0900 F:312-686-0415

DECK PLAN & MEDIAN RECONSTRUCTION DETAILS STRUCTURE NO. 022-0148

SHEET NO. S7 S7 SHEETS	F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	311	10 B-I	DUPAGE	20	15
			CONTRACT NO. 60J45		
		ILLINOIS FED. AID PROJECT			

CONTRACT NO.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE

DRAWN BY

CHECKED BY

TC-10

USER NAME = bauerdl

FILE NAME = $FILEL$

Primera

DESIGNED	VEA	REVISED	-
DRAWN	VEA	REVISED	-
CHECKED	TWL	REVISED	-
DATE	1/29/2010	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

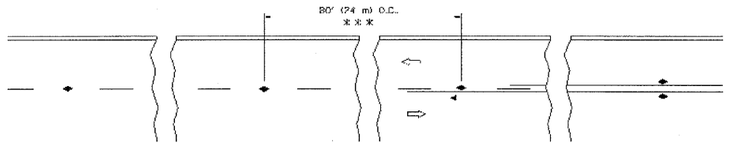
DISTRICT ONE STANDARD DETAILS
TC-10

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10B-I	DUPAGE	20	16
		CONTRACT NO. 60J45		
	ILLINOIS	FED. AID PROJECT		

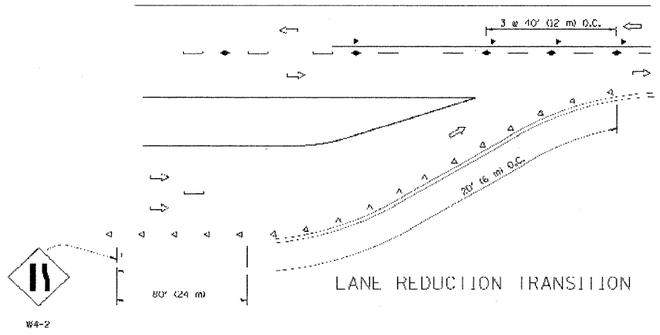
CONTRACT NO.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

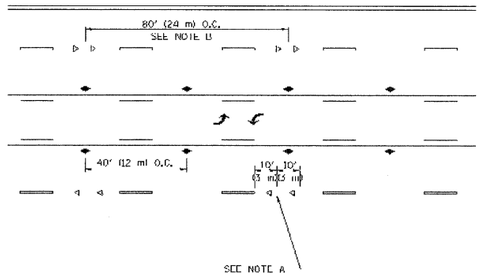


* * * REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

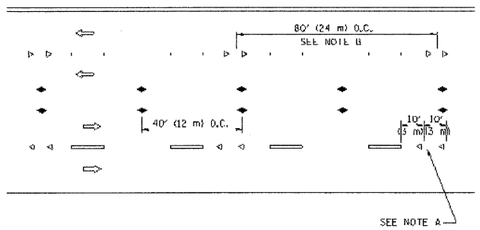
TWO-LANE/TWO-WAY



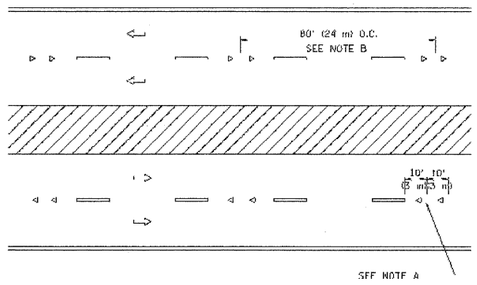
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

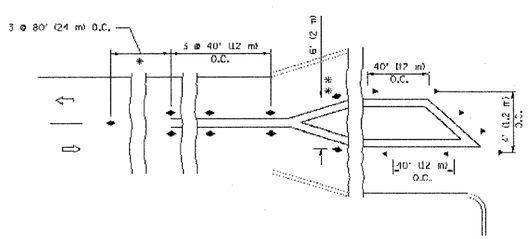
LANE MARKER NOTES

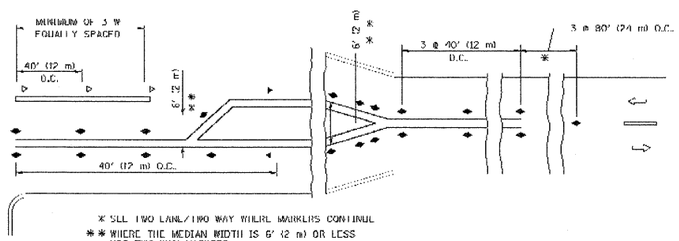
A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.





* SEE TWO LANE/TWO WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO WAY MARKERS.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00
C. JUCIUS	09 09 09

ILLINOIS DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT
MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE

DRAWN BY CADD
CHECKED BY
TC 11

USER NAME = $USERNAME$

FILE NAME = $FILEL$

Primera

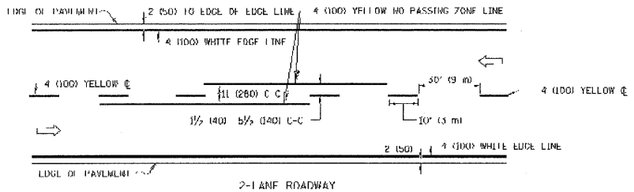
DESIGNED	VEA	REVISED	-
DRAWN	VEA	REVISED	-
CHECKED	TWL	REVISED	-
DATE	1/29/2010	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

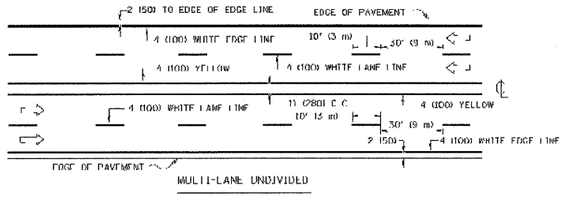
DISTRICT ONE STANDARD DETAILS
TC-11

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

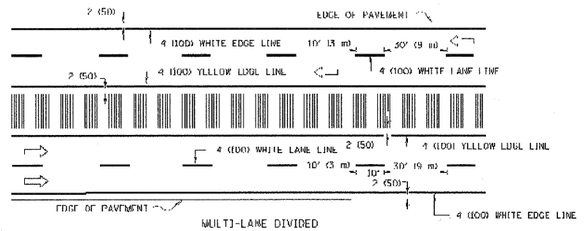
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10B-I	DUPAGE	20	17
		CONTRACT NO. 60J45		
	ILLINOIS	FED. AID PROJECT		



2-LANE ROADWAY



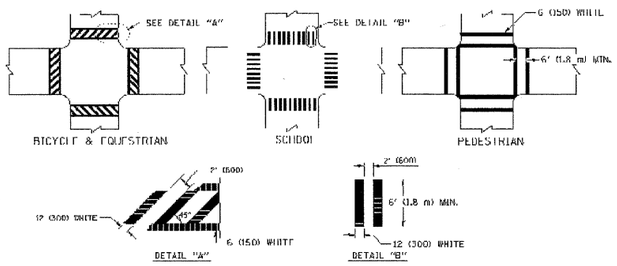
MULTI-LANE UNDIVIDED



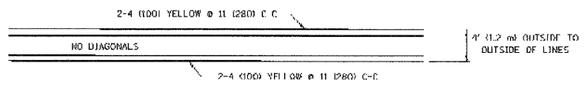
MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

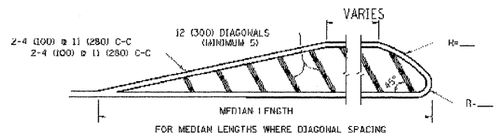
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



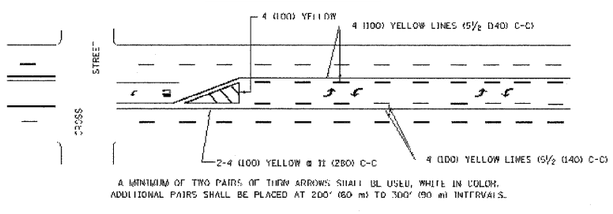
4' (1.2 m) WIDE MEDIANS ONLY



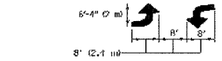
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

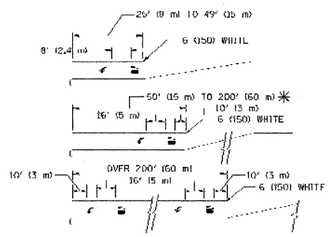


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

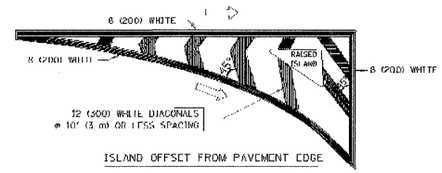


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
(AREA - 15.6 SQ. FT. (1.5 m²)) ONLY AREA - 20.8 SQ. FT. (1.9 m²)

✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C C ONLY SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE, FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
BIKE MARKING AND CHANNELLIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 18 (450) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R" 3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
H.VEHS	03-19-90
L. RAMMACHER	10 27 94
C. JUCIUS	09-09-09

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE

DRAWN BY CADD
CHECKED BY
TC-13

CONTRACT NO.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

USER NAME = dvivesag

FILE NAME = #FILEL#

Primera

DESIGNED	VEA	REVISED	-
DRAWN	VEA	REVISED	-
CHECKED	TWL	REVISED	-
DATE	1/29/2010	REVISED	-

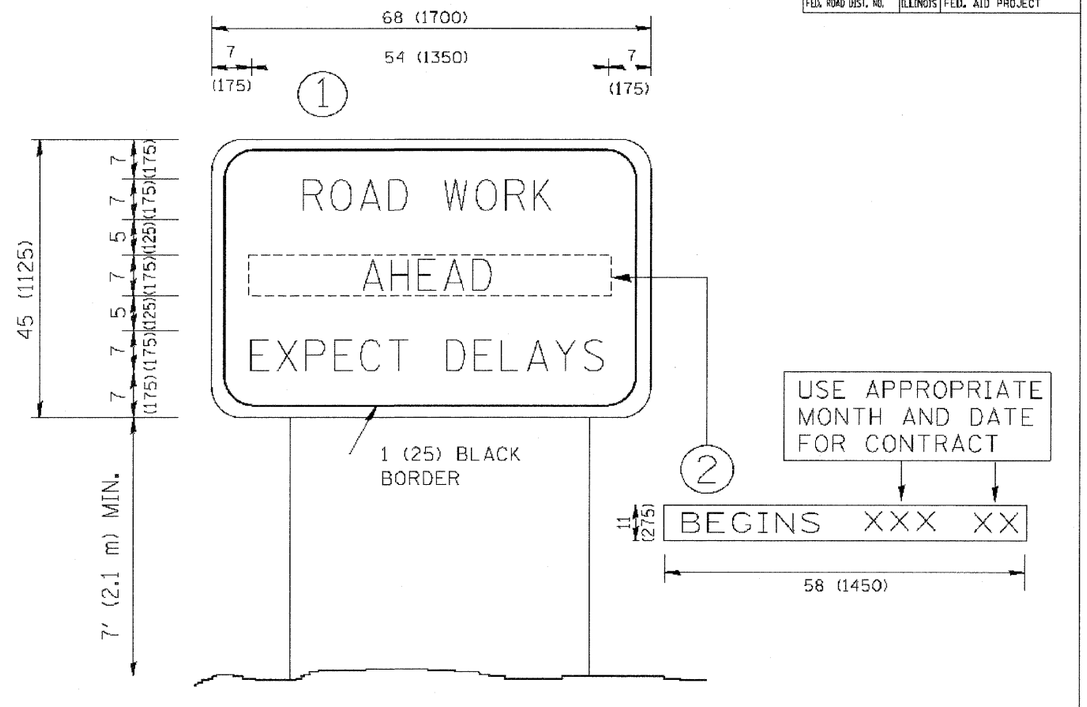
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE STANDARD DETAILS
TC-13

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10B-I	DUPAGE	20	18
		CONTRACT NO. 60J45		
	ILLINOIS	FED. AID PROJECT		

CONTRACT NO.				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. MIRS	9-15-97
R. MIRS	12-11-97
T. RAMMACHER	2-2-99
C. JUCIUS	1-31-07

ILLINOIS DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE

DRAWN BY DESIGN

CHECKED BY TC22

USER NAME = bsheardi

FILE NAME = $FILE1$

Primera

DESIGNED	VEA	REVISED	-
DRAWN	VEA	REVISED	-
CHECKED	TWL	REVISED	-
DATE	1/29/2010	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

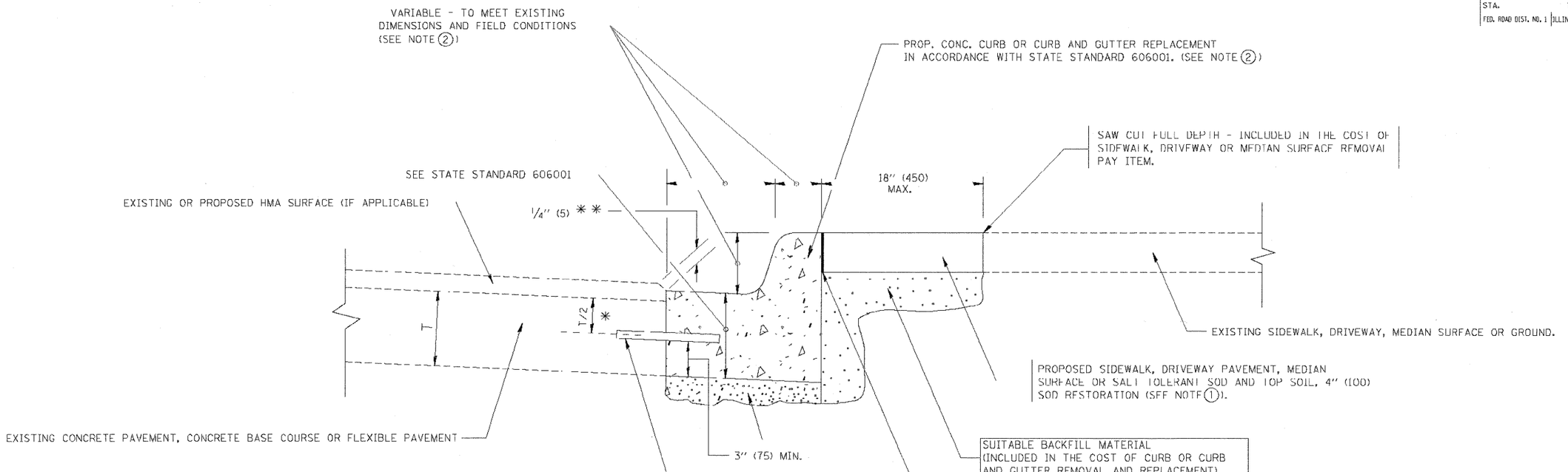
DISTRICT ONE STANDARD DETAILS
TC-22

SCALE:	SHEET NO. OF SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10B-I	DUPAGE	20	19
		CONTRACT NO. 60J45		
	ILLINOIS	FED. AID PROJECT		

CONTRACT NO.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

(2) FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

(3) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(4) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(5) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(7) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(8) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
H. SHAH	09/12/96
R. SHAH	09/19/96
H. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	12/15/09

ILLINOIS DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: VERT. NONE HORIZ. DRAWN BY CHECKED BY

BD600-06 (BD-24)

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

FILE NAME = *FILEL*

Primera

DESIGNED	VEA	REVISED	-
DRAWN	VEA	REVISED	-
CHECKED	TWL	REVISED	-
DATE	1/29/2010	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE STANDARD DETAILS
BD-24

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10B-I	DUPAGE	20	20
		CONTRACT NO. 60J45		
	ILLINOIS	FED. AID PROJECT		