5-15-09 Letting, Item 181

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TWP-2-RS-2	KANE	19	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 60G68		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED
HIGHWAY PLANS

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE IMPROVEMENT IS LOCATED IN THE VILLAGE OF MONTGOMERY.

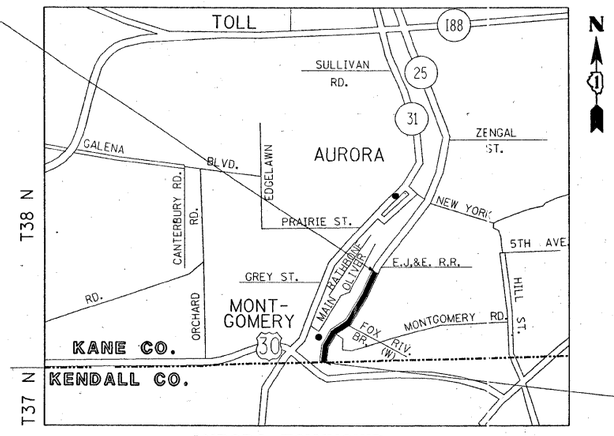
F.A.U. ROUTE 2503: IL 25
0.4 MILE N. OF ASHLAND AVE. TO KENDAL CO LINE
TWP–2–RS–2
RESURFACING
KANE COUNTY
C–91–524–09

D–91–524–09

LOCATION OF SECTION INDICATED THUS: –■–

R 8 E

IMPROVEMENT ENDS STA. 118+12



AURORA TOWNSHIP

IMPROVEMENT BEGINS STA. 31+81

TRAFFIC DATA :
2007 ADT = 13,000
POSTED SPEED LIMIT = 40 MPH

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1–800–892–0123
OR 811

PROJECT ENGINEER ROBERT BORO (847) 705–4178
PROJECT MANAGER KEN ENG

CONTRACT NO. 60G68

GROSS AND NET LENGTH OF IMPROVEMENT = 8,631 FEET = 1.63 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED April 22, 2009
Diane M. O'Keefe /S/
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
May 1, 2009
Charles J. Ingersoll /S/
ENGINEER OF DESIGN AND ENVIRONMENT
May 1, 2009
Christine M. Reed /S/
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

STATE STANDARDS

STANDARD NO.	DESCRIPTION
442201-03	CLASS C AND D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTE
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701501-05	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701701-04	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 or 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE VILLAGE OF MONTGOMERY.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 40 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OFTHE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.

THE RESIDENT ENGINEER SHALL CONTACT MR. DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER AT (847) 741-9857 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKING.

BEFORE BEGINING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERANCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKINGS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKINGS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470. A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINING OF WORK.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS- RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

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		DRAWN - Drawn By	REVISED - Revised By2		
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - Checked By	REVISED - Revised By3		
	PLOT DATE = 4/23/2009	DATE - Checked Date	REVISED - Revised By4		SCALE: Scale / Station To

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TWP-2-RS-2	KANE	19	2
		CONTRACT NO. 60G68		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

SUMMARY OF QUANTITIES			URBAN 100% STATE TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		1000					
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	22	22					
40600300	AGGREGATE (PRIME COAT)	TON	107	107					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	8	8					
40600895	CONSTRUCTING TEST STRIP	EACH	1	1					
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	2238	2238					
42001300	PROTECTIVE COAT	SQ YD	42	42					
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	26642	26642					
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	125	125					
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	235	235					
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	80	80					
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	2	2					
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	2	2					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6					
67100100	MOBILIZATION	L SUM	1	1					
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1					
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	1530	1530					
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	73	73					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	35238	35238					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	260	260					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	189	189					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	197	197					
78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	73	73					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	35238	35238					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	260	260					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	189	189					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	197	197					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	240	240					
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	200	200					
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	450	450					
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	231.3	231.3					
X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1050	1050					
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	1	1					

* SPECIALTY ITEMS

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 25 (KENDALL CO LINE TO 0.4 N. OF ASHLAND AVE.)
SUMMARY OF QUANITIES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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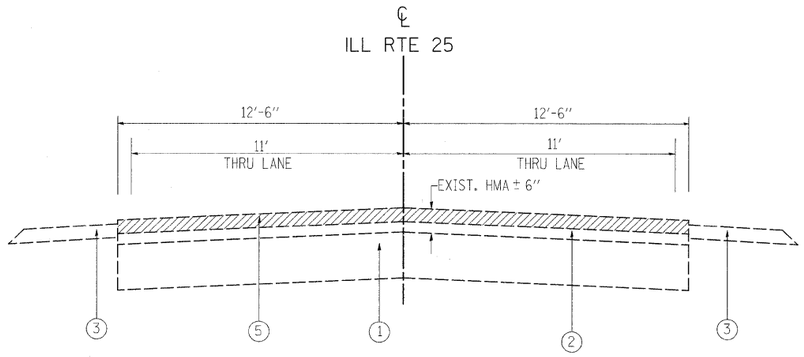
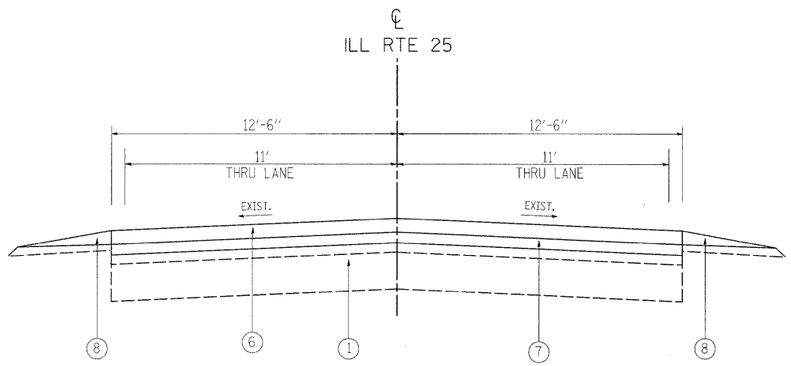
		CONTRACT NO. 60G68		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



ILL RTE 25
EXIST. TYPICAL SECTION
STA. 31+81 TO STA. 90+77
STA. 102+11 TO STA. 118+12



ILL RTE 25
PROP. TYPICAL SECTION
STA. 31+81 TO STA. 90+77
STA. 102+11 TO STA. 118+12

LEGEND

1. EXIST. PCC BASE COURSE, (±)9"
2. EXIST. REMAINING HMA AFTER MILLING, (±)3¾"
3. EXIST. AGGREGATE SHOULDER
4. EXIST. COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.24
5. PROP. HOT-MIX ASPHALT SURFACE REMOVAL - 2¼"
6. PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1½"
7. PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾"
8. PROP. AGGREGATE WEDGE SHOULDER, TYPE B

NOTE: CONTRACTOR SHALL MILL FIRST BEFORE PATCHING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AC TYPE	AIR VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	PG 64-22	4% @ 70 GYR
POLYMERIZED LEVELING BINDER, (MM), IL-4.75, N50	SBS/SBR PG 76-28/-22	4% @ 50 GYR
CLASS D PATCHES, (HMA BINDER IL-19 mm)	PG 64-22*	4% @ 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.

*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

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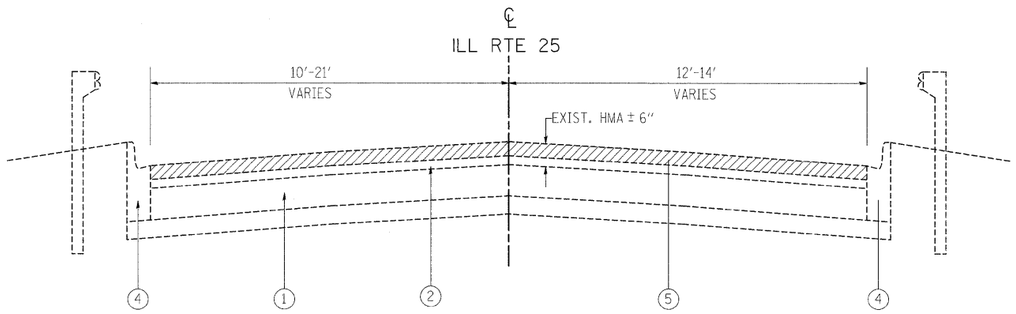
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILL RTE 25
TYPICAL SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TPW-2-RS-2	KANE	19	4
		CONTRACT NO. 60G68		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

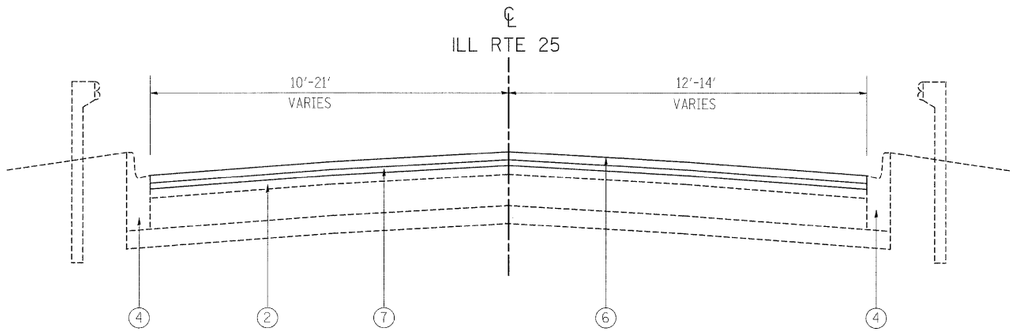


ILL RTE 25
EXIST. TYPICAL SECTION
STA. 90+77 TO STA. 102+11

LEGEND

1. EXIST. PCC BASE COURSE, (±)9"
2. EXIST. REMAINING HMA AFTER MILLING, (±)5"
3. EXIST. AGGREGATE SHOULDER
4. EXIST. COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.24
5. PROP. HOT-MIX ASPHALT SURFACE REMOVAL - 2¼"
6. PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1½"
7. PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾"
8. PROP. AGGREGATE WEDGE SHOULDER, TYPE B

NOTE: CONTRACTOR SHALL MILL FIRST BEFORE PATCHING



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PROP. TYPICAL SECTION
STA. 90+77 TO STA. 118+12

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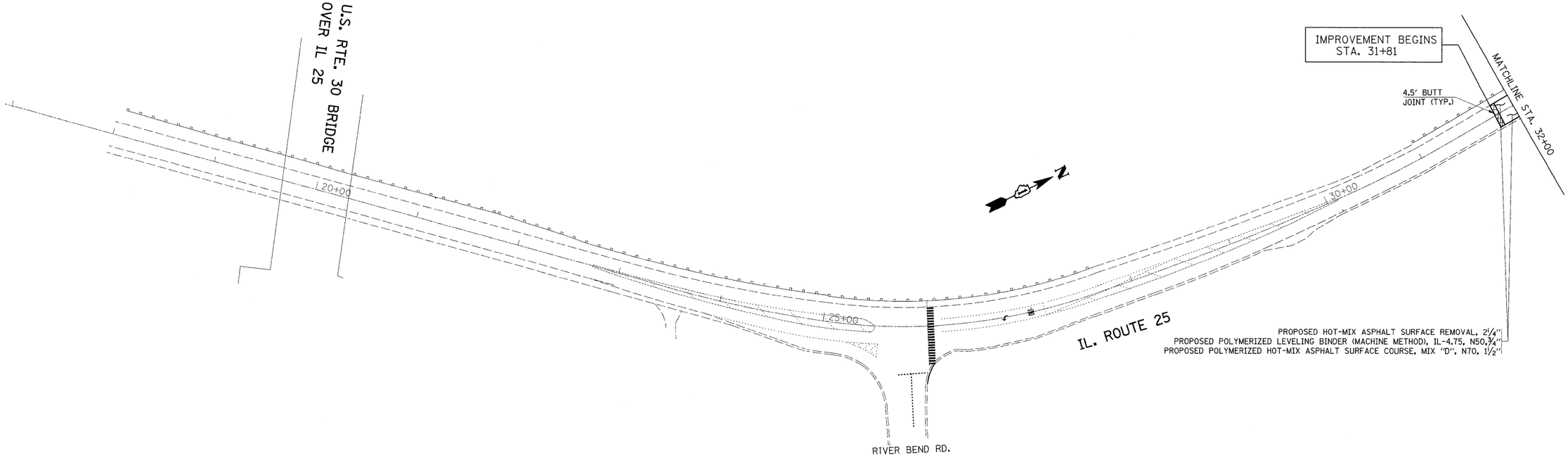
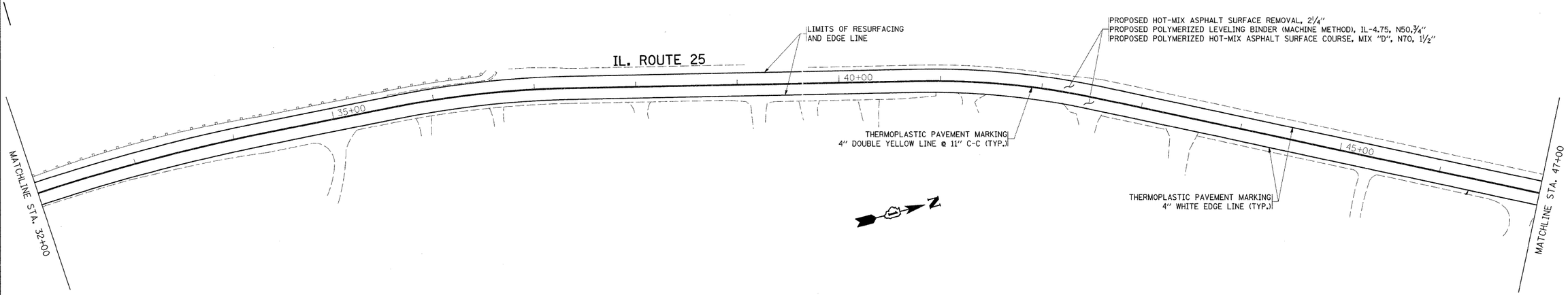
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILL RTE 25
TYPICAL SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TWP-2-RS-2	KANE	19	5
		CONTRACT NO. 60G68		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		





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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 25 (KENDALL CO LINE TO 0.4 MILE N. OF ASHLAND AVE.)
ROADWAY AND PAVEMENT MARKING PLAN

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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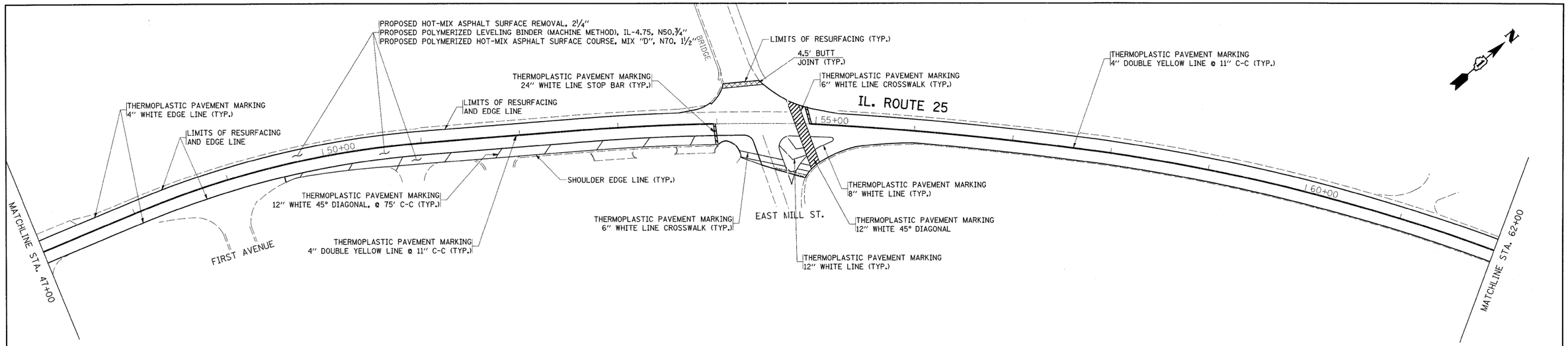
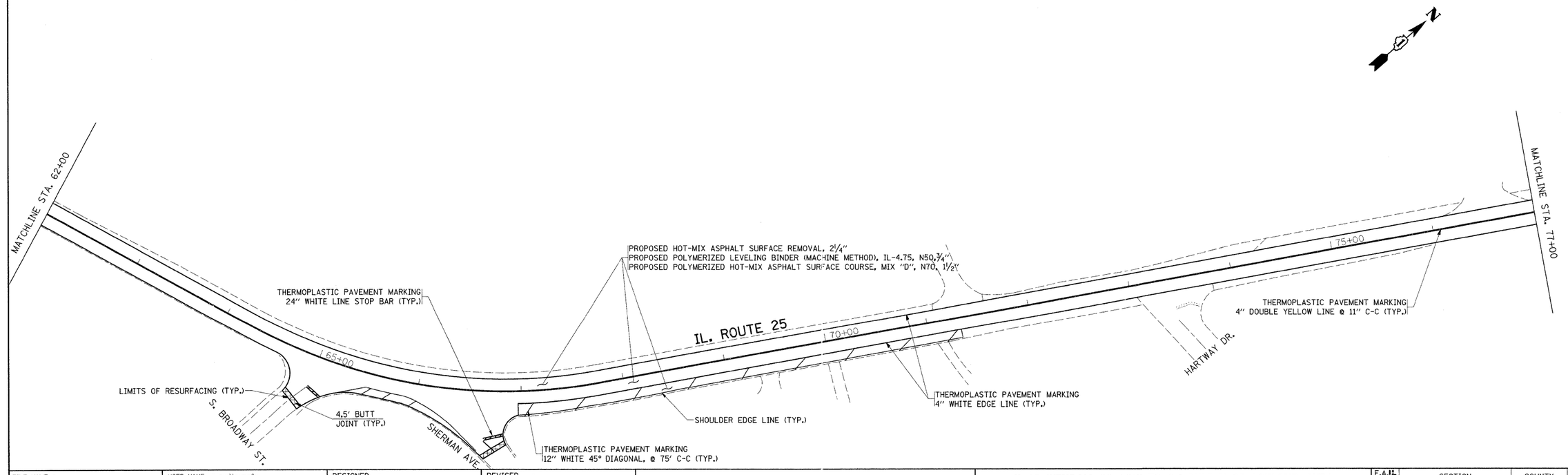
	ILLINOIS	FED. AID PROJECT		





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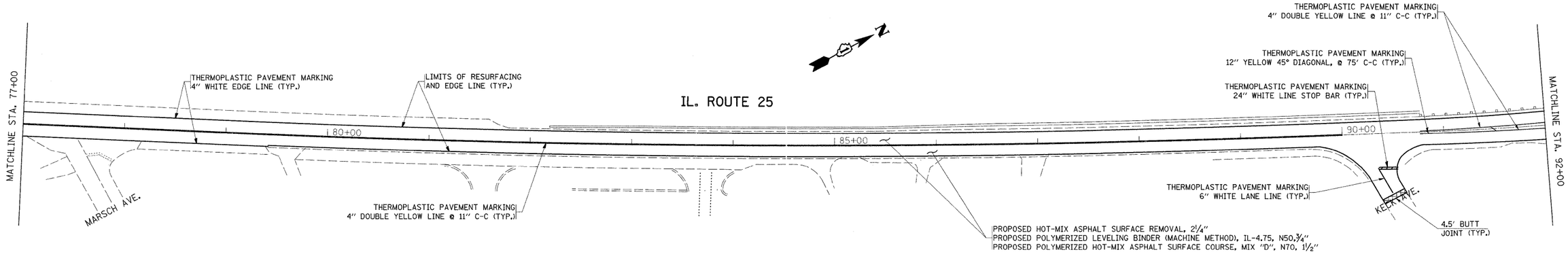
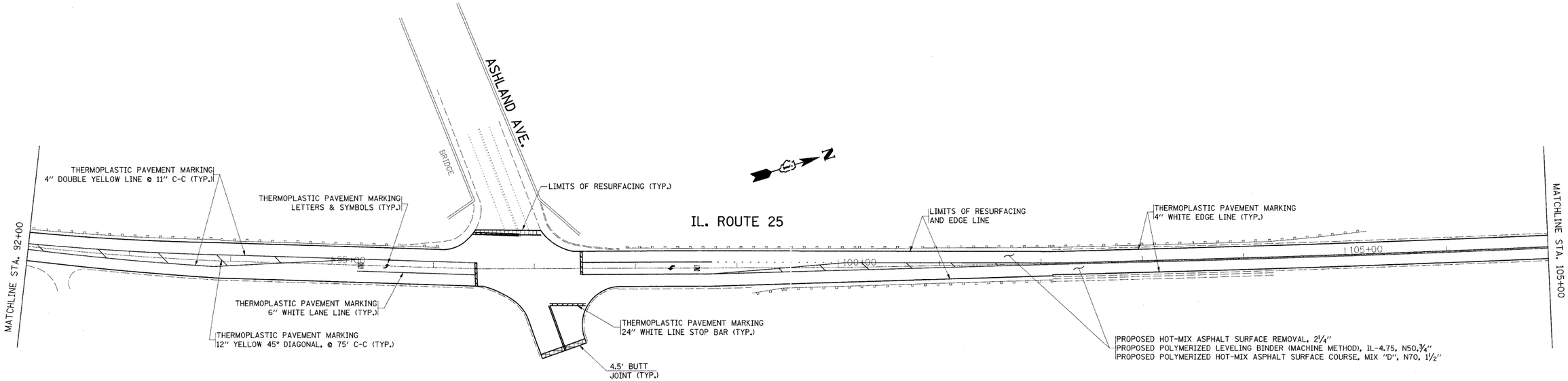
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 25 (KENDALL CO LINE TO 0.4 MILE N. OF ASHLAND AVE.)
ROADWAY AND PAVEMENT MARKING PLAN

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TWP-2-RS-2	KANE	19	7
		CONTRACT NO. 60G68		
	ILLINOIS FED. AID PROJECT			





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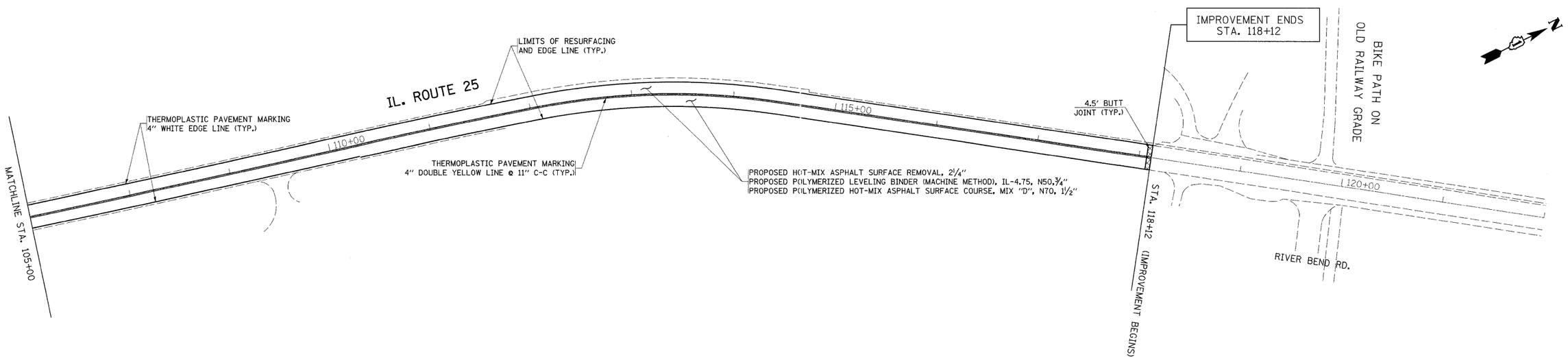
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 25 (KENDALL CO LINE TO 0.4 MILE N. OF ASHLAND AVE.)
ROADWAY AND PAVEMENT MARKING PLAN

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TWP-2-RS-2	KANE	19	8
		CONTRACT NO. 60G68		
	ILLINOIS FED. AID PROJECT			



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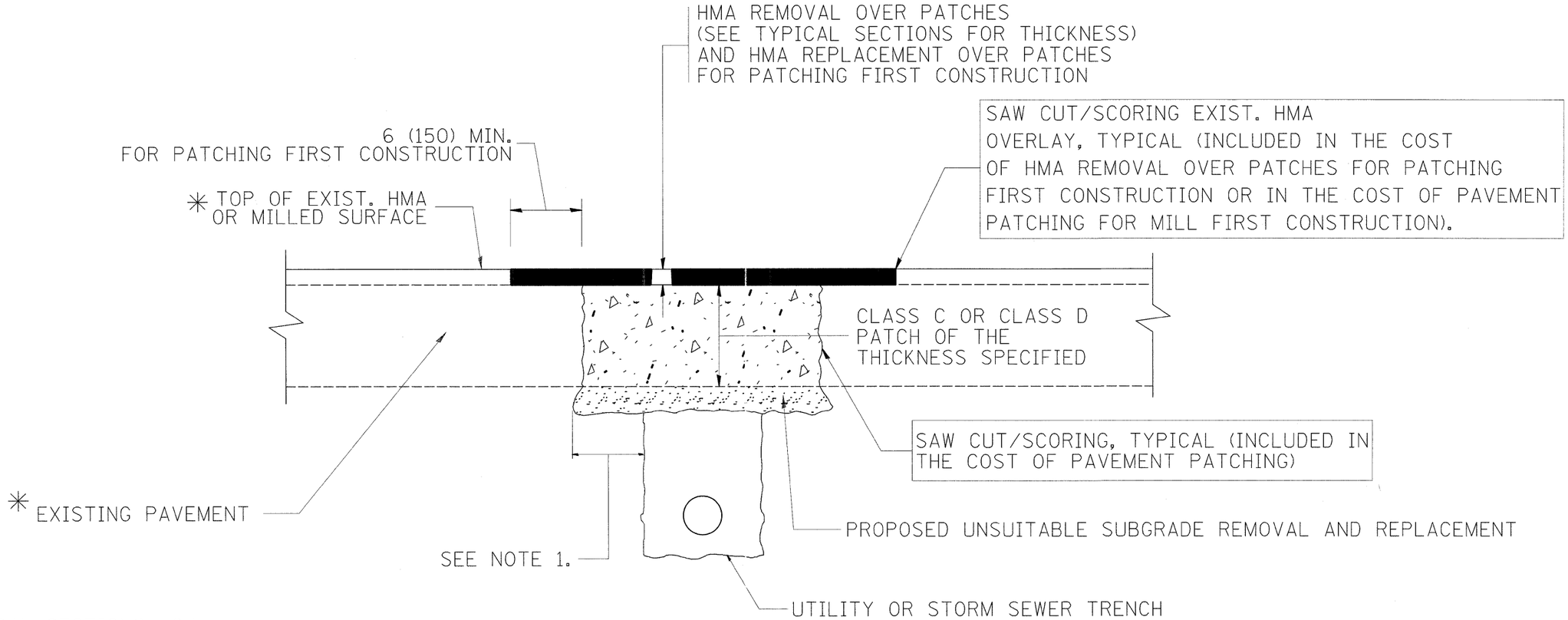
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 25 (KENDALL CO LINE TO 0.4 MILE N. OF ASHLAND AVE.)
ROADWAY AND PAVEMENT MARKING PLAN

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TWP-2-RS-2	KANE	19	9
		CONTRACT NO. 60G68		
		ILLINOIS FED. AID PROJECT		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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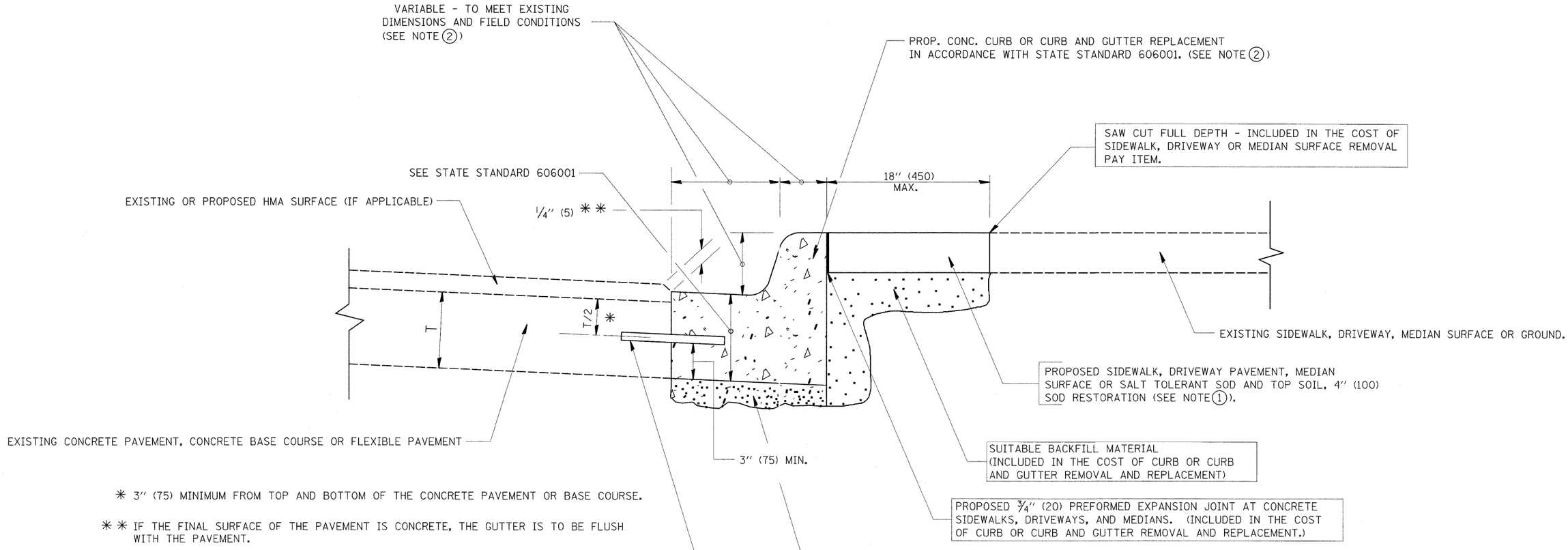
	PLOT SCALE = 50.0000 ' / IN.	DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.
	PLOT DATE = 4/23/2009	CHECKED -	REVISED - R. BORO 09-04-07		
		DATE - 10-25-94	REVISED - K. ENG 10-27-08		

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TWP-2-RS-2	KANE	19	10
	BD400-04 (BD-22)	CONTRACT NO. 60G68		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(2) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(3) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(4) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(5) THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(7) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

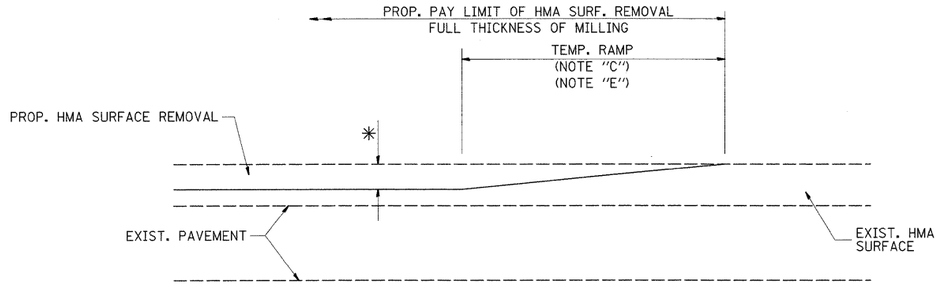
FILE NAME = c:\pw_work\PWIDOT\GUILLAUMEFP\d0137752\D_st5td.dgn	USER NAME = guillaumefp	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96
	DRAWN -		REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01
	PLOT DATE = 4/23/2009	DATE - 03-11-94	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

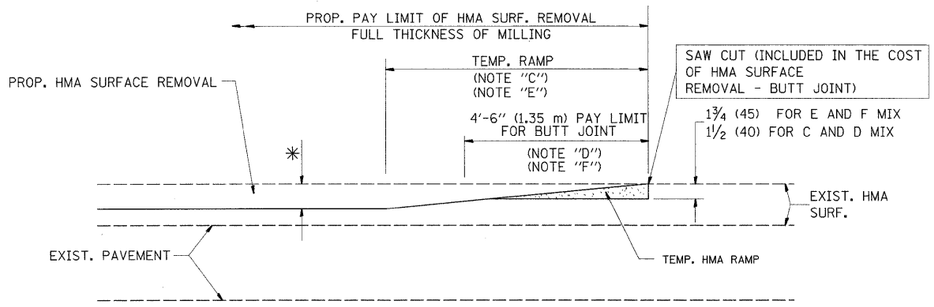
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TWP-2-RS-2	KANE	19	11
	BD600-06 (BD-24)	CONTRACT NO. 60G68		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

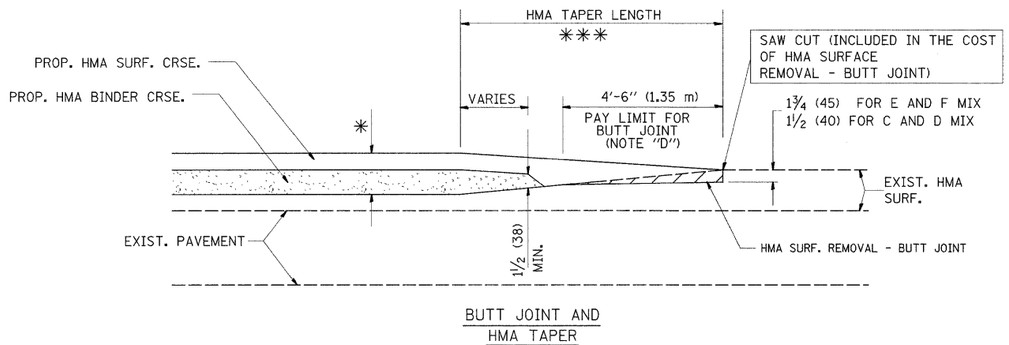


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

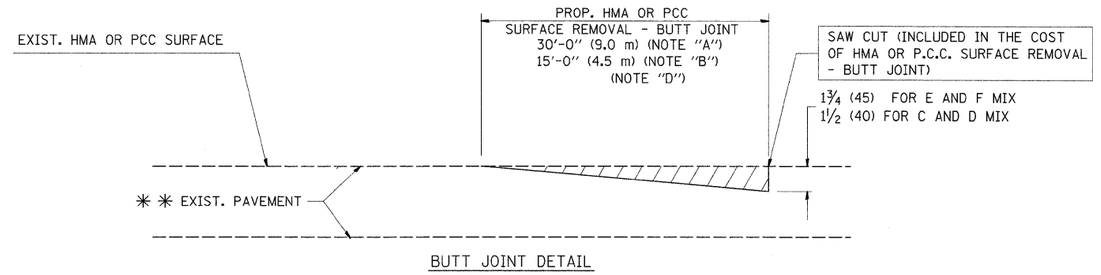
OPTION 2

TYPICAL TEMPORARY RAMP

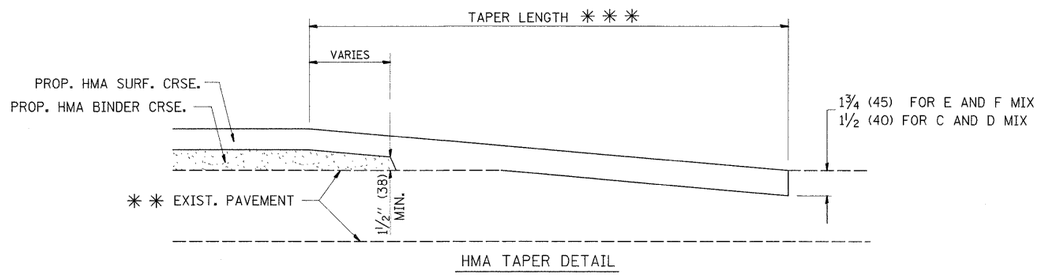


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

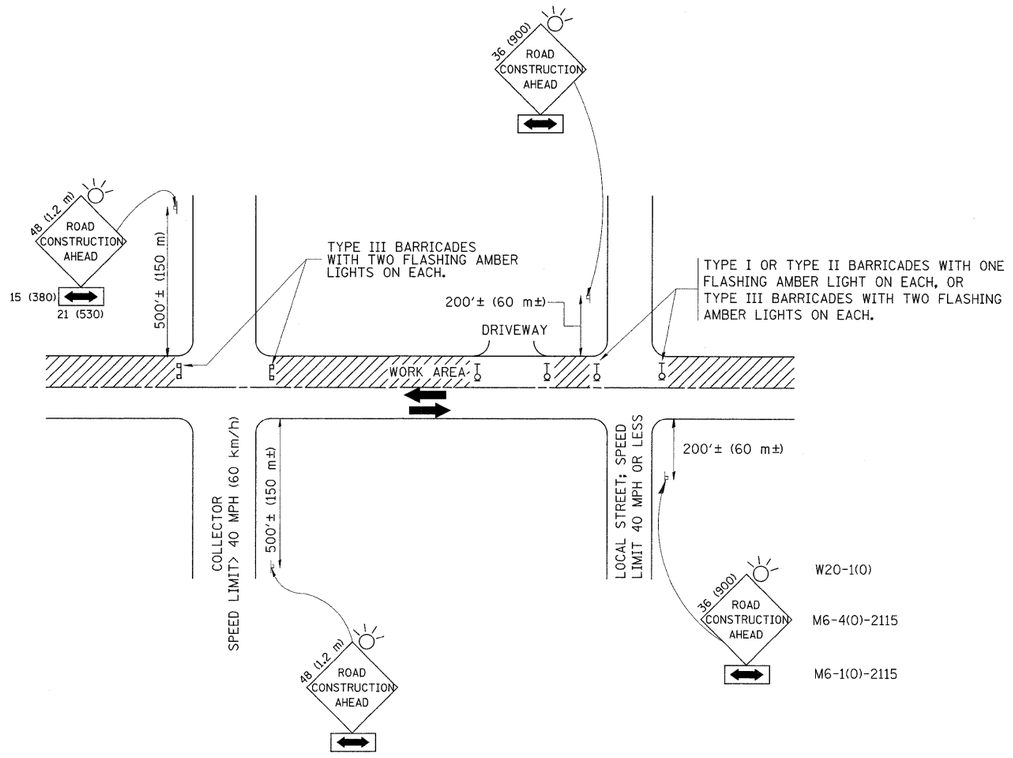
FILE NAME = c:\pw_work\PWIDOT\GUILLAUMEFP\d0137752\D\stStd.dgn	USER NAME = guillaumefp	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 4/23/2009	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TWP-2-RS-2	KANE	19	12
	BD400-05 BD32	CONTRACT NO. 60G68		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

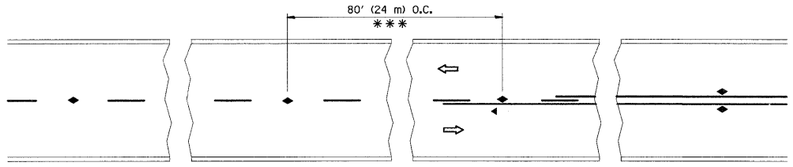
FILE NAME = c:\pw_work\PWIDOT\GUILLAUMEFP\d0137752\Dist5td.dgn	USER NAME = guillaumefp	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 4/23/2009	DATE - 06-89	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

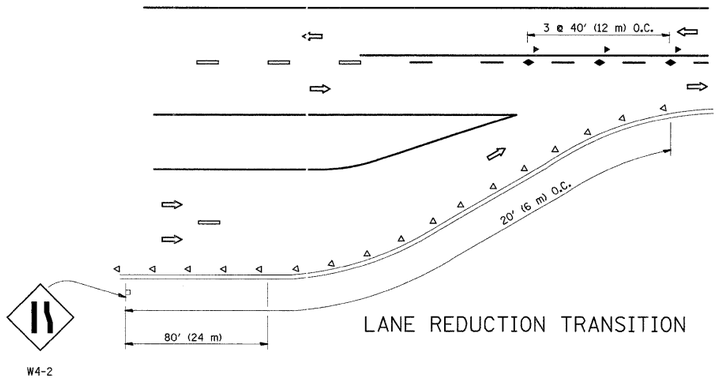
SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TWP-2-RS-2	KANE	19	13
	TC-10	CONTRACT NO. 60G68		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

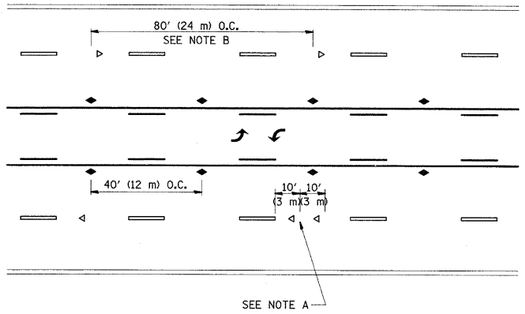


*** REDUCE TO 40′ (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

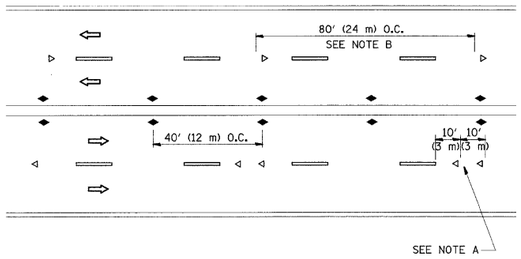
TWO-LANE/TWO-WAY



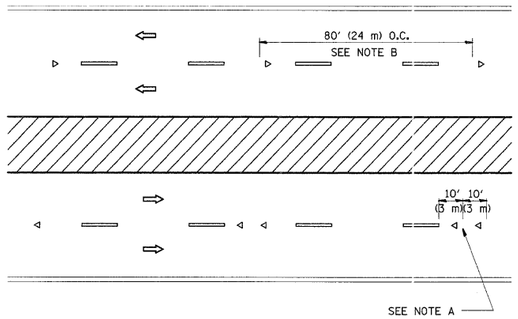
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500′ (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

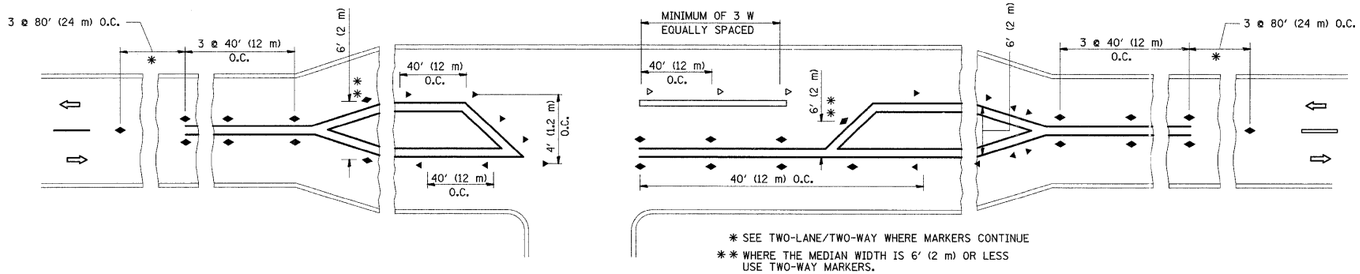
LANE MARKER NOTES

B. REDUCE TO 40′ (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6′ (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

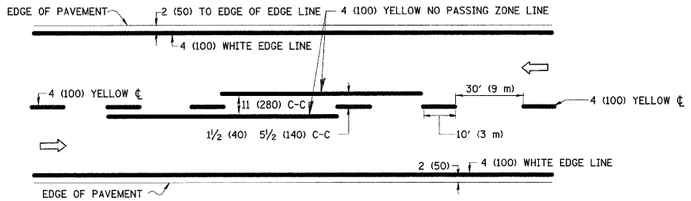
FILE NAME = c:\pw_work\PWIDOT\GUILLAUMEFP\d0137752\DistStd.dgn	USER NAME = guillaumefp	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
		DRAWN -	REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 50.0000 ′ / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 4/23/2009	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

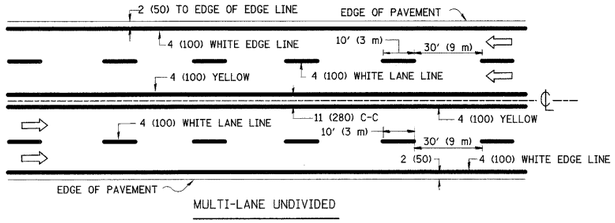
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

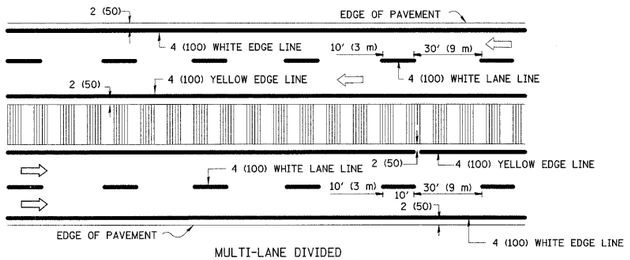
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TWP-2-RS-2	KANE	19	14
	TC-11	CONTRACT NO. 60G68		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



2-LANE ROADWAY



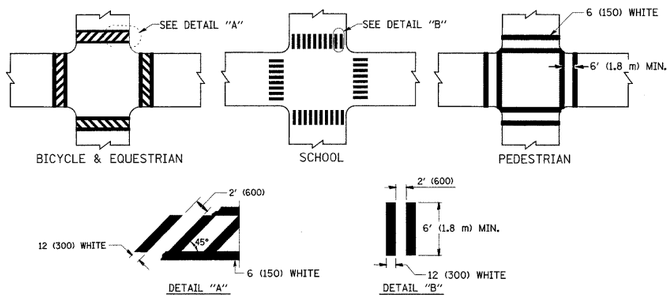
MULTI-LANE UNDIVIDED



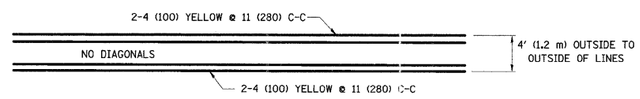
MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

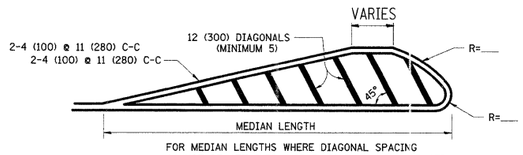
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



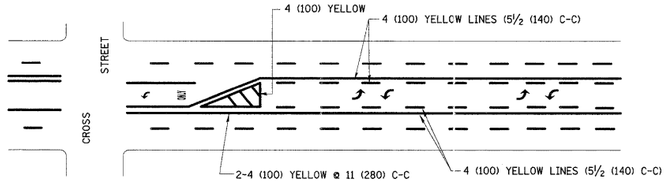
4' (1.2 m) WIDE MEDIANS ONLY



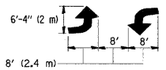
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

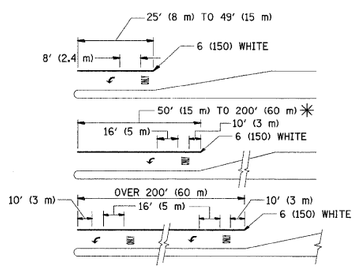


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

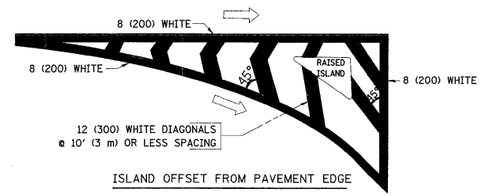


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1.9 m^2)

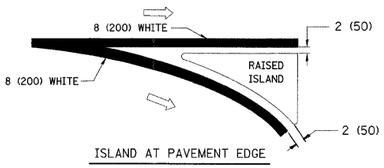
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVEDED PAVEMENT	2 ℄ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 ℄ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 ℄ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 ℄ 6 (150) 12 (300) ℄ 45° 12 (300) ℄ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 ℄ 4 (100) WITH 12 (300) DIAGONALS ℄ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS ℄ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS	12 (300) ℄ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

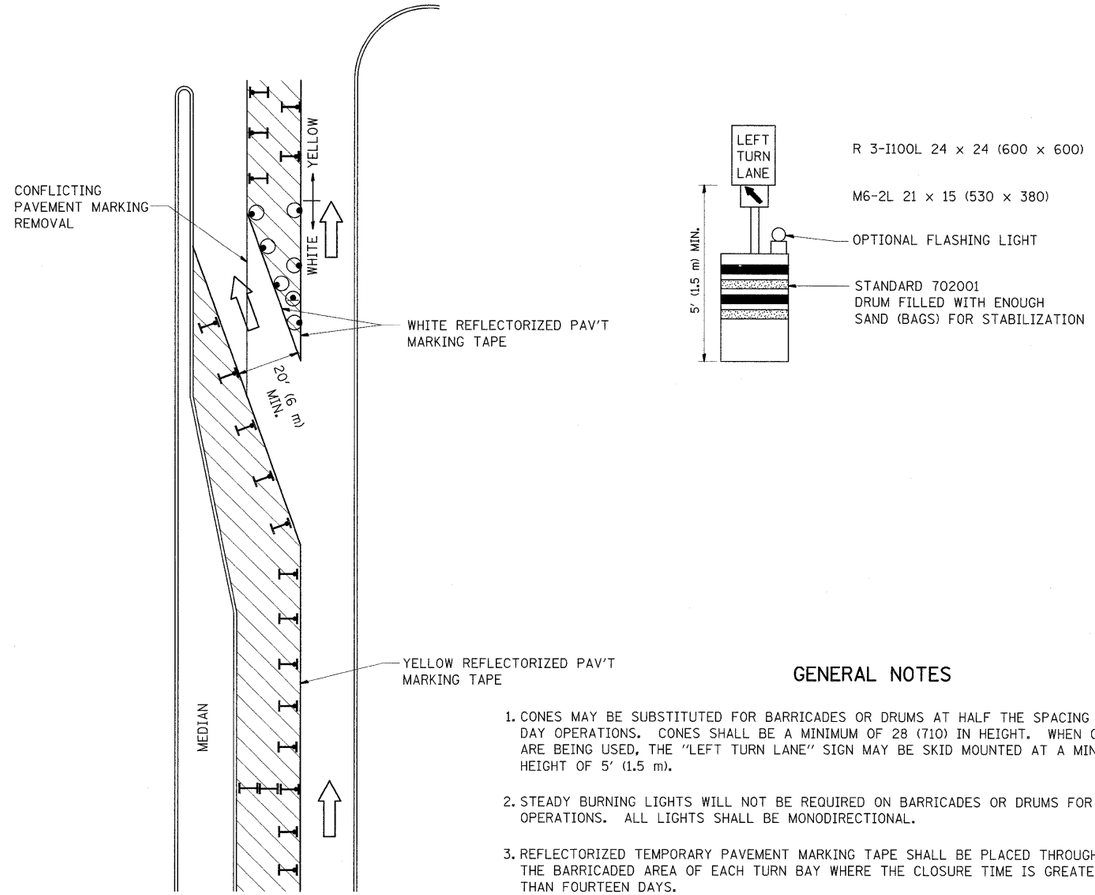
FILE NAME = c:\pw_work\PWIDOT\GUILLAUMEFP\d0137752\DistStd.dgn	USER NAME = guillaumefp	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
		DRAWN -	REVISED -A. HOUSEH 10-09-96
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -A. HOUSEH 10-17-96
	PLOT DATE = 4/23/2009	DATE - 03-19-90	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TWP-2-RS-2	KANE	19	15
	TC-13	CONTRACT NO. 60G68		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

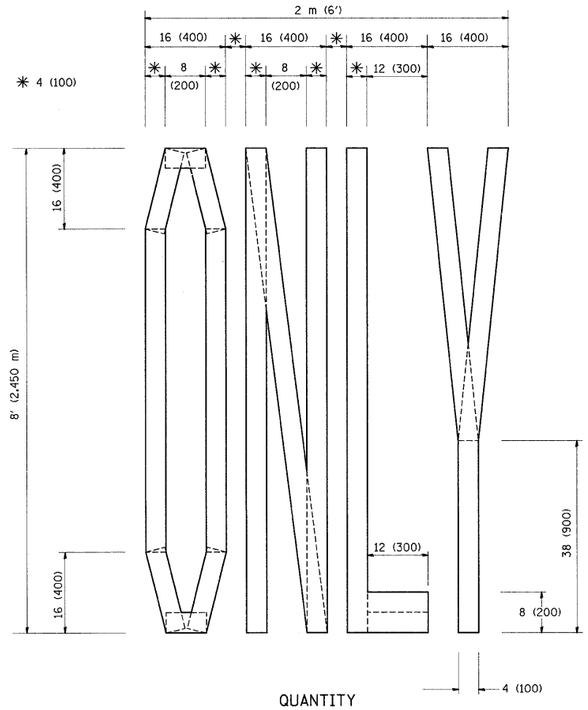
FILE NAME = c:\pw_work\PWIDOT\GUILLAUMEFP\d0137752\DistStd.dgn	USER NAME = guillaumefp	DESIGNED -	REVISED -T. RAMMACHER 09-08-94
		DRAWN -	REVISED - A. HOUSEH 11-07-95
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-12-96
	PLOT DATE = 4/23/2009	DATE -	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

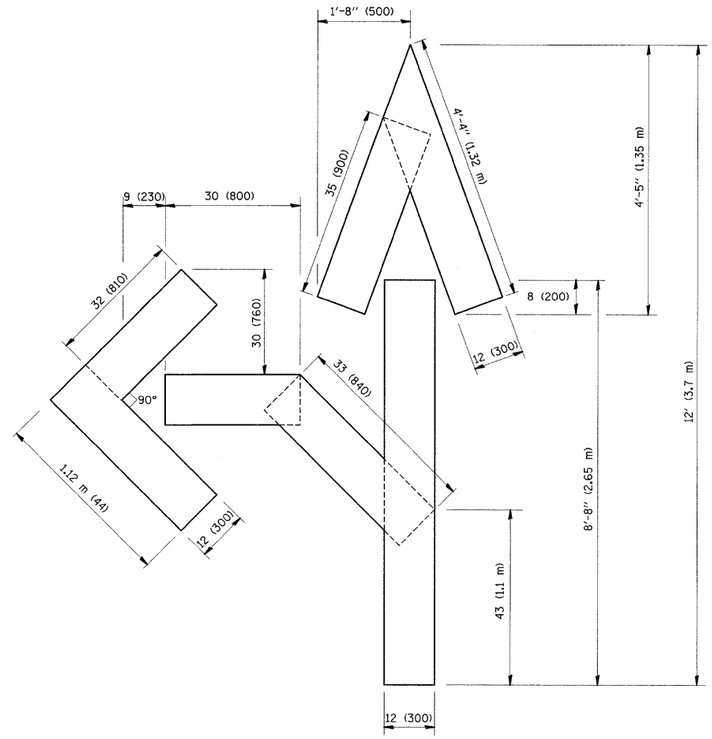
TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

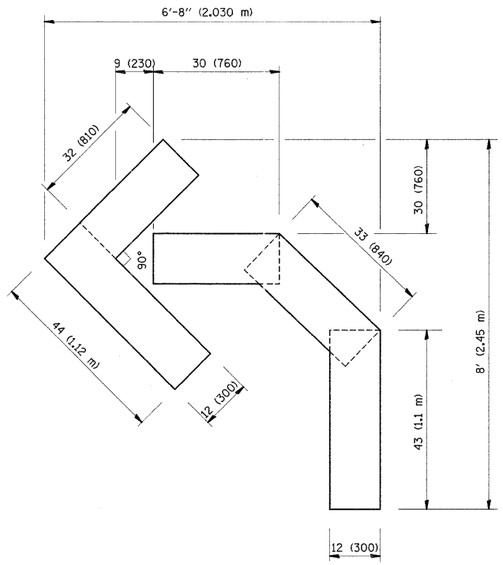
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TWP-2-RS-2	KANE	19	16
	TC-14	CONTRACT NO. 60G68		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



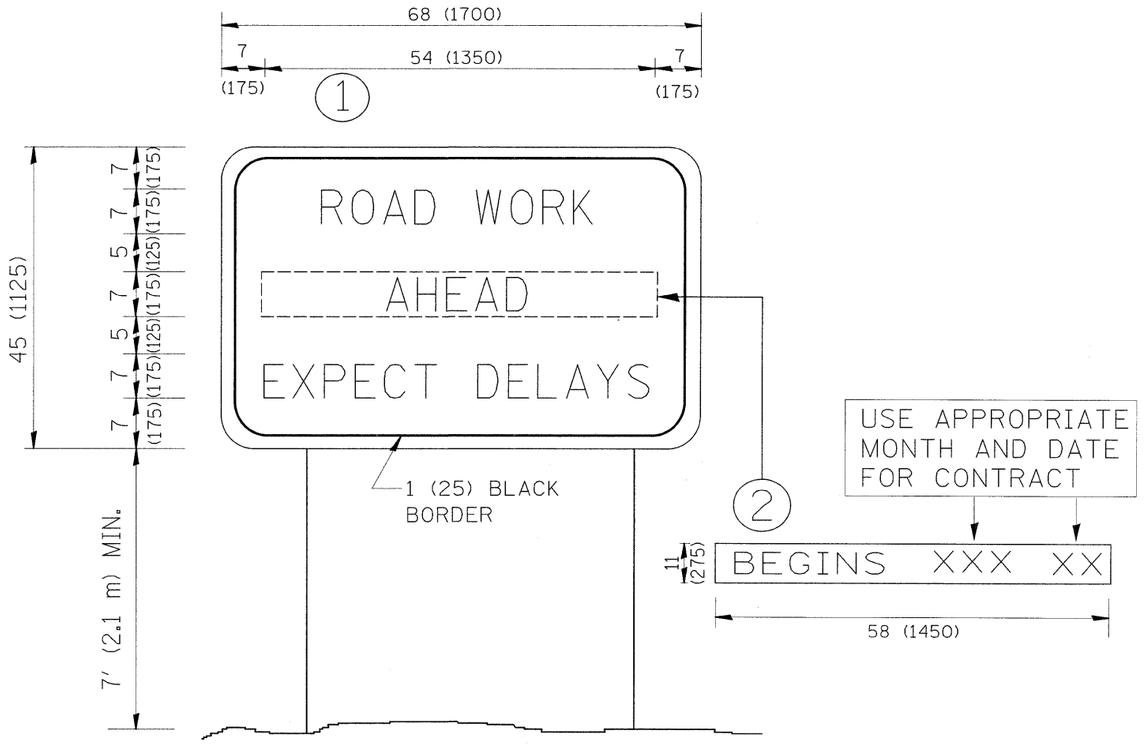
QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = c:\pw_work\PWIDOT\GUILLAUMEFP\d0137752\DistStd.dgn	USER NAME = guillaumefp	DESIGNED -	REVISED -T. RAMMACHER 06-05-96	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING**	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -T. RAMMACHER 11-04-97			2503	TWP-2-RS-2	KANE	19	17
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98				TC-16		CONTRACT NO. 60G68	
	PLOT DATE = 4/23/2009	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\pw_work\PWIDOT\GUILLAUMEFP\d0137752\DistStd.dgn	USER NAME = guillaumefp	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 02-02-99
	PLOT DATE = 4/23/2009	DATE -	REVISED - C. JUCIUS 01-31-07

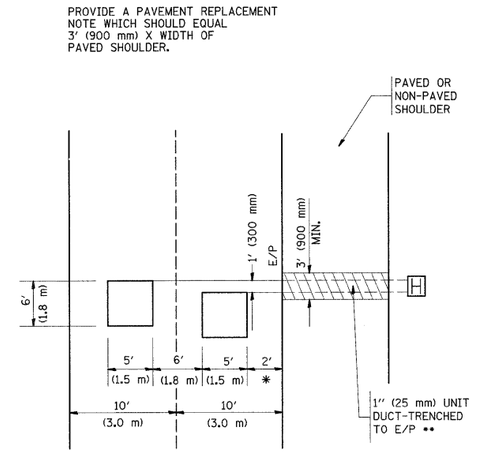
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TWP-2-RS-2	KANE	19	18
	TC-22	CONTRACT NO. 60G68		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

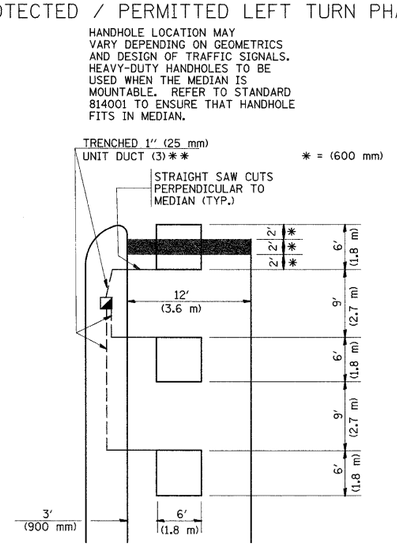
LOOPS NEXT TO SHOULDERS



* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

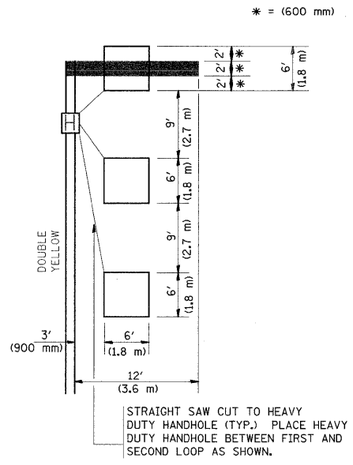
LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

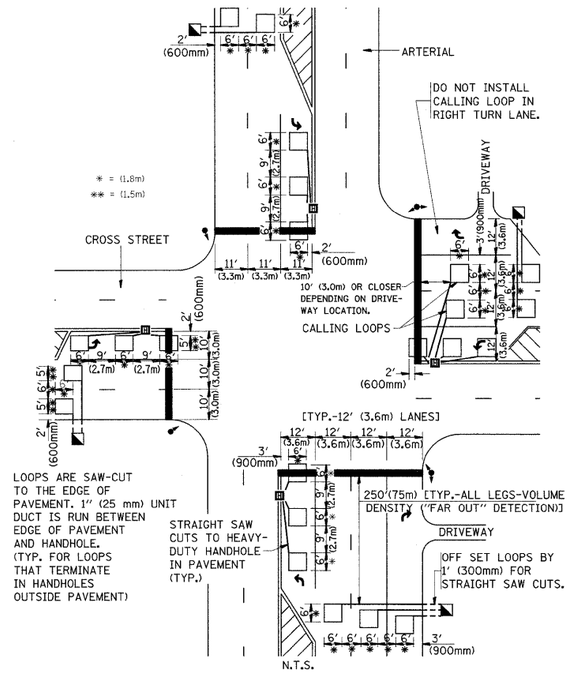
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



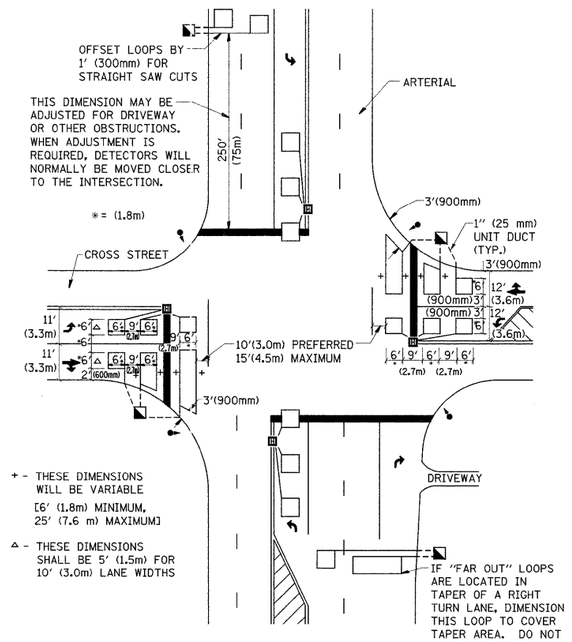
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = c:\pw_work\PWIDOT\GUILLAUMEFP\d0137752\DistStd.dgn	USER NAME = guillaumefp	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - R.K.F.	REVISED -
	PLOT DATE = 4/23/2009	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2503	TWP-2-RS-2	KANE	19	19
	TS-07	CONTRACT NO. 60G68		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		