

|                       |          |                    |              |           |
|-----------------------|----------|--------------------|--------------|-----------|
| F.A.U. RTE.           | SECTION  | COUNTY             | TOTAL SHEETS | SHEET NO. |
| 2509                  | 94RS     | KANE               | 20           | 1         |
| FED. ROAD DIST. NO. 1 | ILLINOIS | CONTRACT NO. 60H04 |              |           |

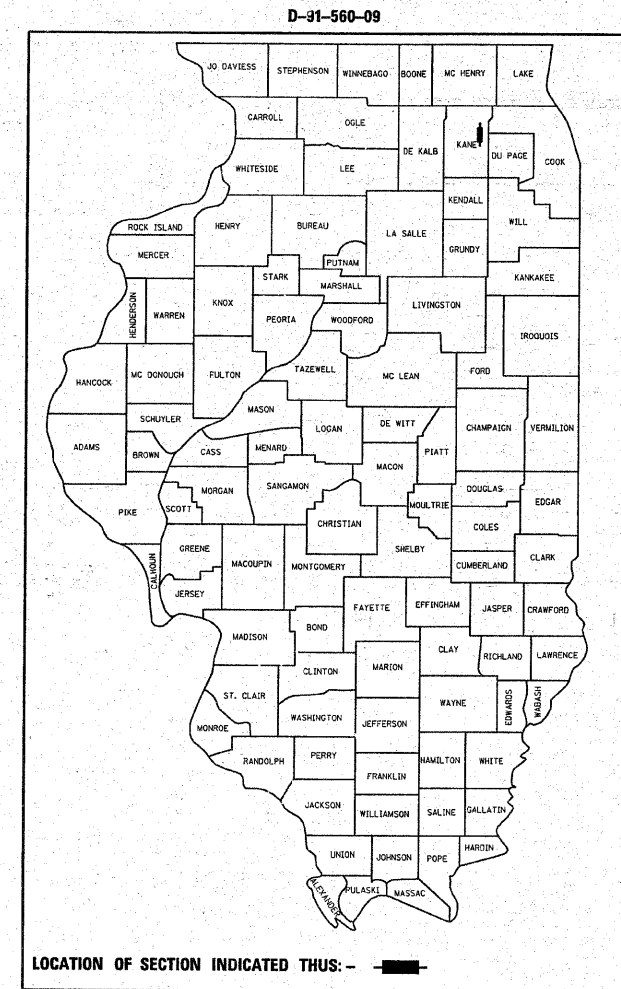
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PROPOSED HIGHWAY PLANS**

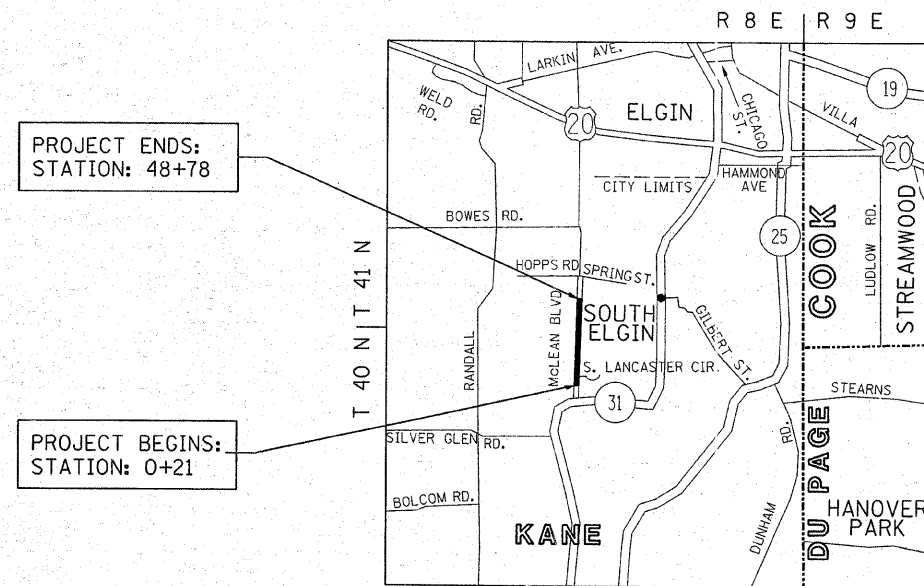
FAU 2509: MCLEAN BLVD  
SPRING ST. TO S. LANCASTER CIRCLE  
SECTION: 94RS  
RESURFACING  
KANE COUNTY  
C-91-560-09

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGES OF SOUTH ELGIN



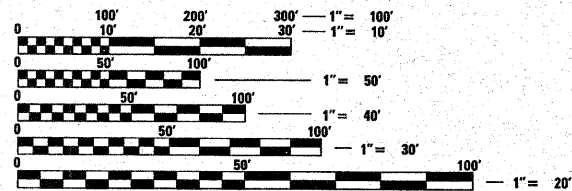
LOCATION OF SECTION INDICATED THUS: - [thick black line] -



PROJECT ENDS:  
STATION: 48+78

PROJECT BEGINS:  
STATION: 0+21

TRAFFIC DATA  
2009 ADT = 20,500  
SPEED LIMIT = 45 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

GROSS AND NET LENGTH OF IMPROVEMENT = 4,857 FEET = 0.92 MILE

PROJECT ENGINEER: JOSE DOMINGUEZ (847) 705-4385  
PROJECT MANAGER: KEN ENG

CONTRACT NO. 60H04

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED APRIL 14, 2009  
Diane M. O'Keefe DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER  
May 1, 2009  
Charles J. Ingersoll ENGINEER OF DESIGN AND ENVIRONMENT  
May 1, 2009  
Christine M. Reed DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

| <u>SHEET NO.</u> | <u>DESCRIPTION</u>   | <u>STANDARD NO.</u> | <u>DESCRIPTION</u>   |
|------------------|--|---------------------|--|
| 1                | TITLE SHEET  | 000001-05           | TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS                |
| 2                | INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES                                 | 442201-03           | CLASS C AND D PATCHES                                      |
| 3                | SUMMARY OF QUANTITIES  | 604086-02           | FRAME AND GRATE, TYPE 23                                   |
| 4-6              | EXISTING AND PROPOSED TYPICAL SECTIONS   | 606001-04           | CONCRETE CURB AND COMBINATION CONCRETE CURB AND GUTTER     |
| 7-9              | ROADWAY AND PAVEMENT MARKING PLANS   | 701201-03           | LANE CLOSURE, 2L, 2W, DAY ONLY                             |
| 10               | DETAILS FOR FRAME AND LIDS ADJUSTMENT WITH MILLING                               | 701426-03           | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS |
| 11               | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT                                      | 701606-06           | URBAN LANE CLOSURE, MULTILANE 2W WITH MOUNTABLE MEDIAN     |
| 12               | CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT                                  | 701701-04           | URBAN LANE CLOSURE, MULTILANE INTERSECTION                 |
| 13               | BUTT JOINT AND HMA TAPER   | 701901-01           | TRAFFIC CONTROL DEVICES                                    |
| 14               | TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS       | 886001-01           | <del>DETECTOR LOOP INSTALLATION</del>                      |
| 15               | TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)   | 886006-01           | <del>TYPICAL LAYOUT FOR DETECTION LOOPS</del>              |
| 16               | DISTRICT ONE TYPICAL PAVEMENT MARKINGS   |                     |  |
| 17               | TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC)          |                     |  |
| 18               | PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING                         |                     |  |
| 19               | ARTERIAL INFORMATION SIGNING   |                     |  |
| 20               | <del>DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</del> |                     |  |

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF LINCOLNWOOD AND SKOKIE.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).

WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISABILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

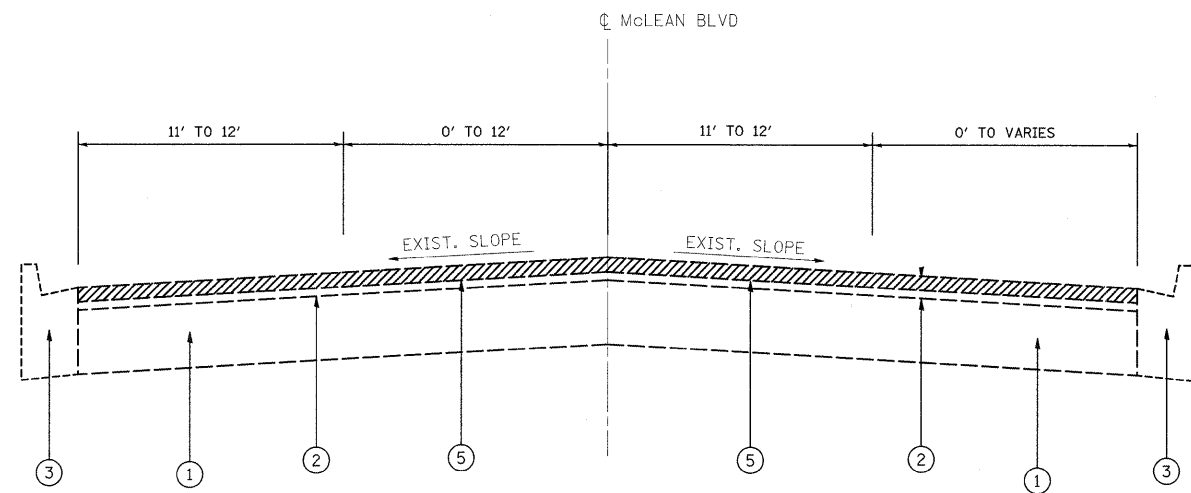
DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

THE RESIDENT ENGINEER SHALL CONTACT MR. DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER, AT (847) 705-4470 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF FINAL PAVEMENT MARKINGS.

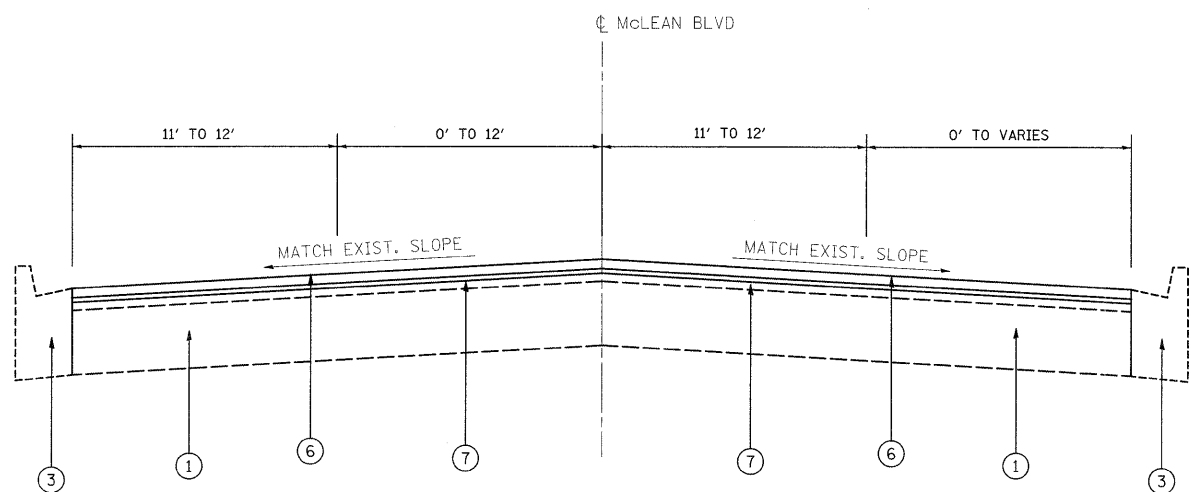
THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE INSTALLATION OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

|  |                    |            |           |   |   |                    |                     |        |              |                       |
|--|--------------------|------------|-----------|---|---|--------------------|---------------------|--------|--------------|-----------------------|
| FILE NAME =                                | USER NAME = goonje | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>MCLEAN BLVD (SPRING ST. TO S. LANCASTER CIRCLE)<br/>INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES</b> | F.A.U. RTE.        | SECTION             | COUNTY | TOTAL SHEETS | SHEET NO.             |
| ci:\pw_work\p\1001\Y00NJE\d0136085\McLe... | en.dgn             | DRAWN -    | REVISED - |   |   | 2509               | 94RS                | KANE   | 20           | 2                     |
| PLOT SCALE = 50,000' / IN.                 |                    | CHECKED -  | REVISED - |   |   | CONTRACT NO. 60H04 |                     |        |              |                       |
| PLOT DATE = 4/18/2009                      |                    | DATE -     | REVISED - |   |   | SCALE:             | SHEET NO. OF SHEETS | STA.   | TO STA.      | FED. ROAD DIST. NO. 1 |

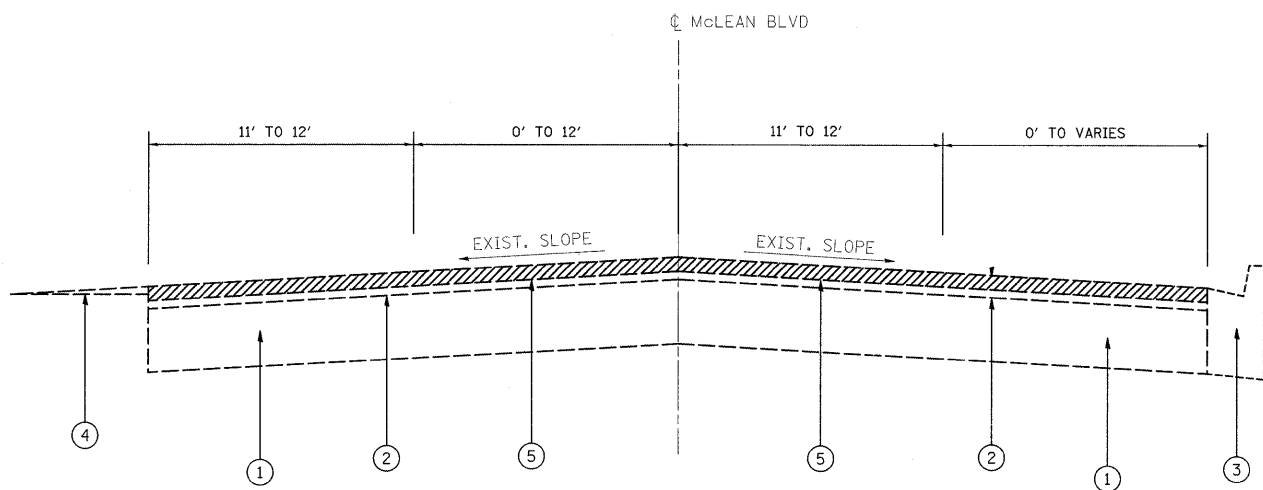




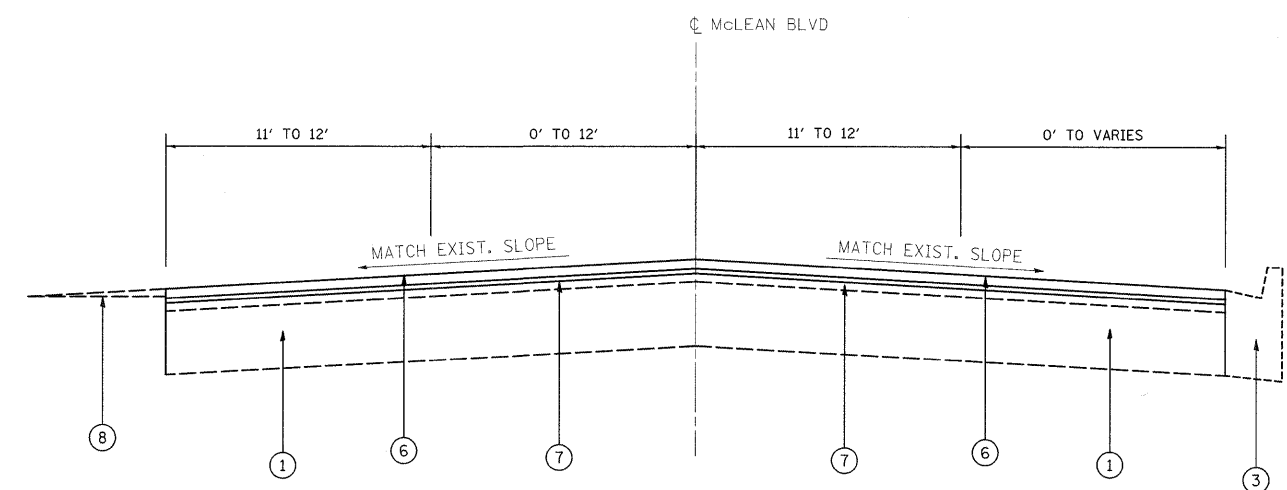
EXISTING TYPICAL SECTION McLEAN BLVD  
STA. 0+21 TO STA. 6+00



PROPOSED TYPICAL SECTION McLEAN BLVD  
STA. 0+21 TO STA. 6+00



EXISTING TYPICAL SECTION McLEAN BLVD  
STA. 6+00 TO STA. 7+90



PROPOSED TYPICAL SECTION McLEAN BLVD  
STA. 6+00 TO STA. 7+90

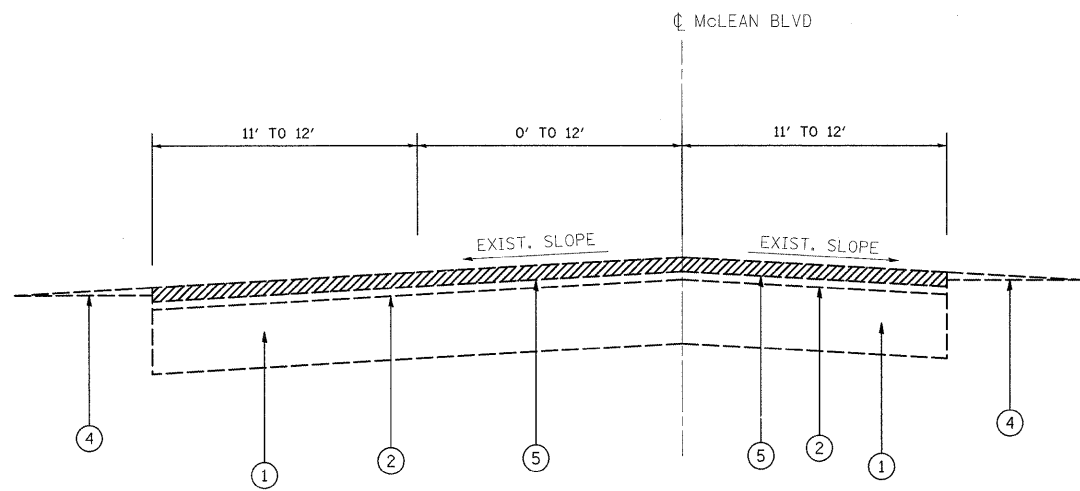
**LEGEND**

- ① EXIST. PCC BASE COURSE, 9''(±)
- ② EXIST. HOT-MIX ASPHALT OVERLAY, 4 1/2''(±)
- ③ EXIST. CONCRETE CURB AND GUTTER
- ④ EXIST. AGGREGATE SHOULDERS
- ⑤ PROP. HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4'' (1/2'' OF HOT-MIX ASPHALT TO REMAIN)
- ⑥ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2''
- ⑦ PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4''
- ⑧ PROP. AGGREGATE WEDGE SHOULDER, TYPE B

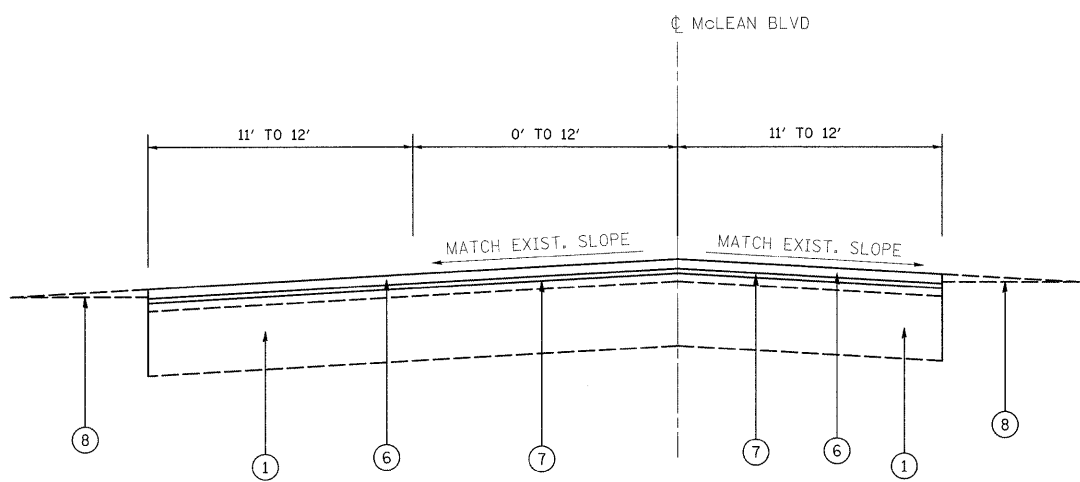
**NOTES:**

1. SEE ROADWAY AND PAVEMENT MARKING PLAN SHEETS FOR LOCATIONS OF LEFT TURN LANES, RIGHT TURN LANES, AND MEDIANS.
2. PAVEMENT PATCHING SHALL BE DONE PRIOR TO MILLING OF THE ROADWAY.

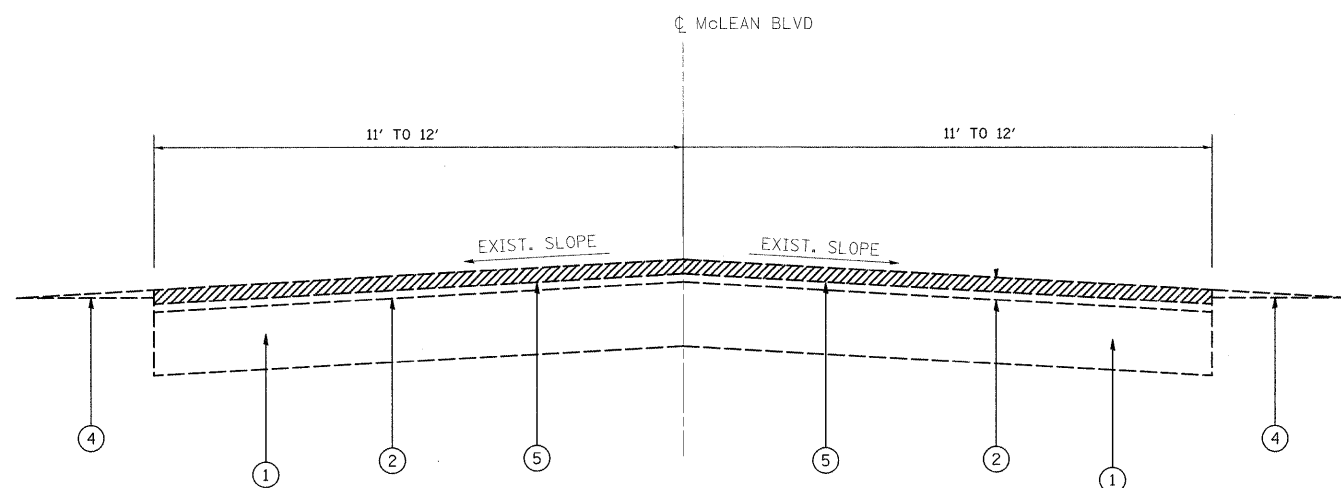
|   |                    |            |           |   |   |                        |                 |                                |                       |                   |  |
|---|--------------------|------------|-----------|---|---|------------------------|-----------------|--------------------------------|-----------------------|-------------------|--|
| FILE NAME =<br>c:\pw_work\PWIDOT\Y00NJE\d0136265\McLean.dgn | USER NAME = yoonje | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>McLEAN BLVD (SPRING ST. TO S. LANCASTER CIR.)<br/>EXISTING AND PROPOSED TYPICAL SECTIONS</b> | F.A.U.<br>RTE.<br>2509 | SECTION<br>94RS | COUNTY<br>KANE                 | TOTAL<br>SHEETS<br>20 | SHEET<br>NO.<br>4 |  |
| PLOT SCALE = 50.0000' / IN.                                 | CHECKED -          | REVISED -  | SCALE:    |   |   | SHEET NO. OF SHEETS    | STA. TO STA.    | FED. ROAD DIST. NO. 1 ILLINOIS |                       |                   |  |
| PLOT DATE = 4/18/2009                                       | DATE -             | REVISED -  |           |   |   |                        |                 | CONTRACT NO. 60H04             |                       |                   |  |
|   |                    |            |           |   |   |                        |                 |                                |                       |                   |  |



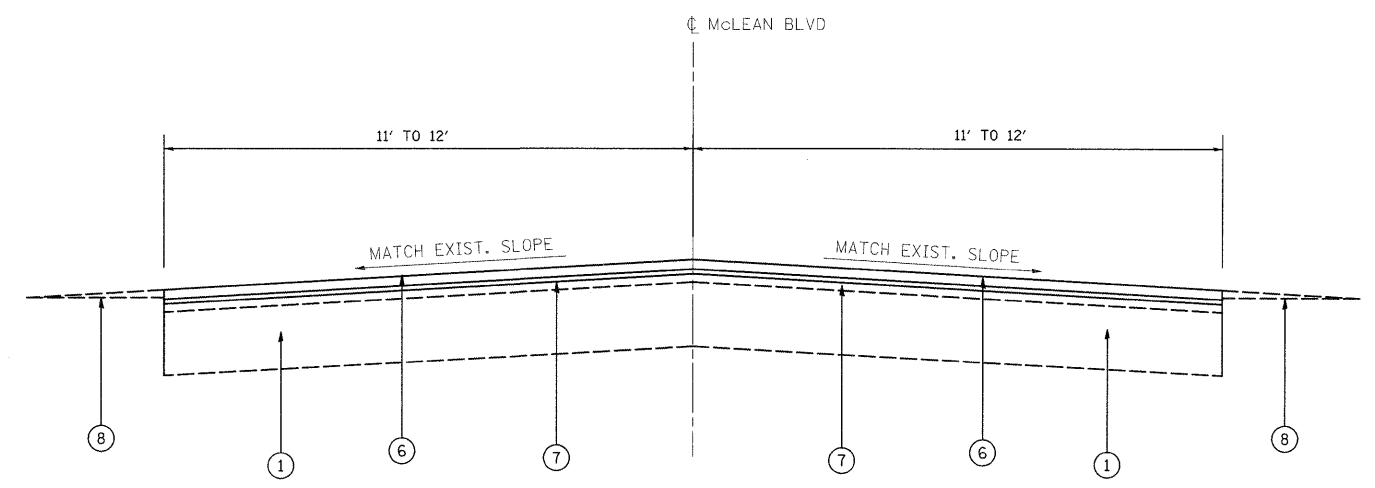
EXISTING TYPICAL SECTION McLEAN BLVD  
STA. 7+90 TO 14+68  
STA. 30+16 TO STA. 38+88



PROPOSED TYPICAL SECTION McLEAN BLVD  
STA. 7+90 TO 14+68  
STA. 30+16 TO STA. 38+88



EXISTING TYPICAL SECTION McLEAN BLVD  
STA. 14+68 TO STA. 23+28  
STA. 38+88 TO STA. 48+78



PROPOSED TYPICAL SECTION McLEAN BLVD  
STA. 14+68 TO STA. 23+28  
STA. 38+88 TO STA. 48+78

**LEGEND**

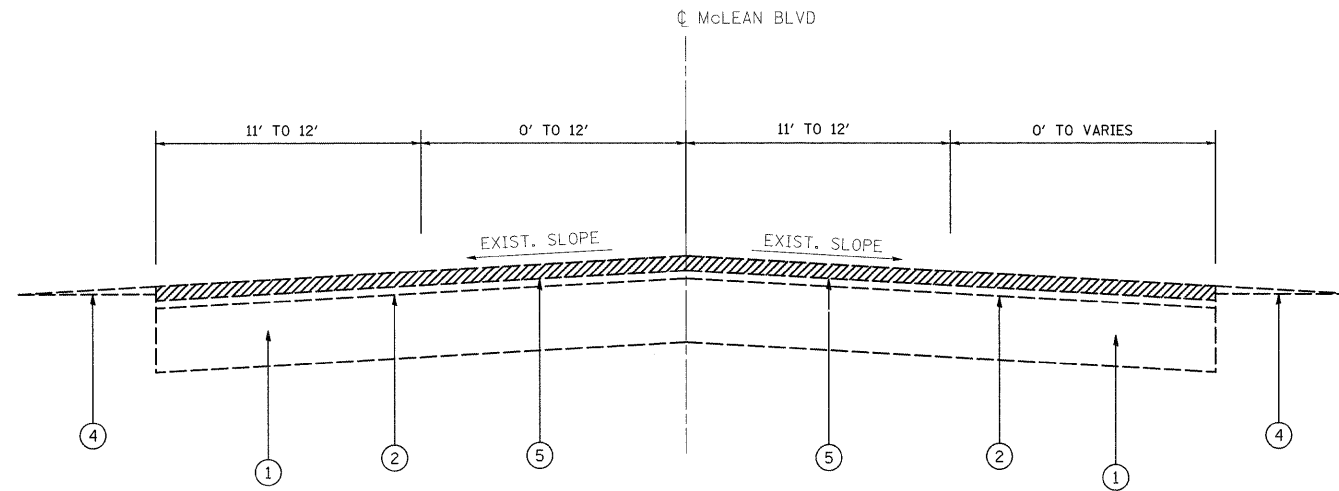
- ① EXIST. PCC BASE COURSE, 9''(±)
- ② EXIST. HOT-MIX ASPHALT OVERLAY, 4 1/2''(±)
- ③ EXIST. CONCRETE CURB AND GUTTER
- ④ EXIST. AGGREGATE SHOULDERS
- ⑤ PROP. HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4'' (1/2'' OF HOT-MIX ASPHALT TO REMAIN)
- ⑥ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2''
- ⑦ PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4''
- ⑧ PROP. AGGREGATE WEDGE SHOULDER, TYPE B

**NOTES:**

1. SEE ROADWAY AND PAVEMENT MARKING PLAN SHEETS FOR LOCATIONS OF LEFT TURN LANES, RIGHT TURN LANES, AND MEDIANS.
2. PAVEMENT PATCHING SHALL BE DONE PRIOR TO MILLING OF THE ROADWAY.

|   |                    |            |           |   |   |   |         |                                |                    |              |
|---|--------------------|------------|-----------|---|---|---|---------|--------------------------------|--------------------|--------------|
| FILE NAME =                                 | USER NAME = yoonje | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>McLEAN BLVD (SPRING ST. TO S. LANCASTER CIR.)<br/>EXISTING AND PROPOSED TYPICAL SECTIONS</b> | F.A.J.J.<br>RTE.                        | SECTION | COUNTY                         | TOTAL<br>SHEETS    | SHEET<br>NO. |
| c:\pwwork\VPWIDOT\Y00NJE\2136055\McLean.dgn |                    | DRAWN -    | REVISED - |   |   | 2509                                    | 94RS    | KANE                           | 20                 | 5            |
| PLOT SCALE = 50.0000' / IN.                 |                    | CHECKED -  | REVISED - |   |   | SCALE: SHEET NO. OF SHEETS STA. TO STA. |         | FED. ROAD DIST. NO. 1 ILLINOIS |                    |              |
| PLOT DATE = 4/18/2009                       |                    | DATE -     | REVISED - |   |   |   |         |                                | CONTRACT NO. 60H04 |              |

LEGEND

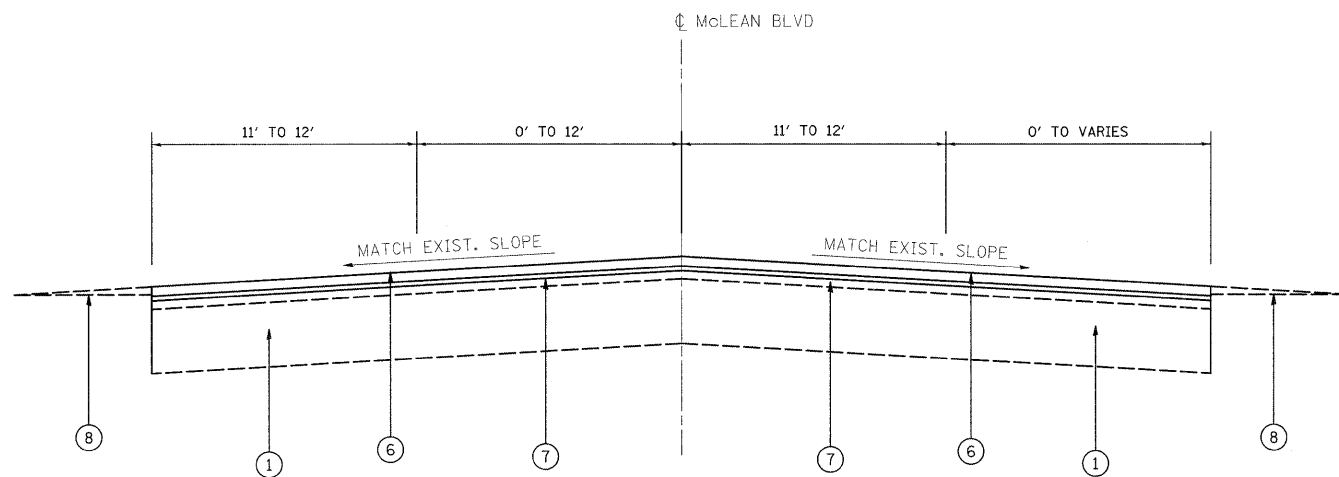


EXISTING TYPICAL SECTION McLEAN BLVD  
STA. 23+28 TO STA. 30+16

- ① EXIST. PCC BASE COURSE, 9''(±)
- ② EXIST. HOT-MIX ASPHALT OVERLAY, 4 1/2''(±)
- ③ EXIST. CONCRETE CURB AND GUTTER
- ④ EXIST. AGGREGATE SHOULDERS
- ⑤ PROP. HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4''  
(1/2'' OF HOT-MIX ASPHALT TO REMAIN)
- ⑥ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D",  
N70, 1 1/2''
- ⑦ PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4''
- ⑧ PROP. AGGREGATE WEDGE SHOULDER, TYPE B

NOTES:

1. SEE ROADWAY AND PAVEMENT MARKING PLAN SHEETS FOR LOCATIONS OF LEFT TURN LANES, RIGHT TURN LANES, AND MEDIANS.
2. PAVEMENT PATCHING SHALL BE DONE PRIOR TO MILLING OF THE ROADWAY.



PROPOSED TYPICAL SECTION McLEAN BLVD  
STA. 23+28 TO STA. 30+16

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

|         | MIXTURE TYPE  | AC TYPE                 | AIR VOIDS (%) |
|---------|---|-------------------------|---------------|
| ROADWAY | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL-9.5MM), 1 1/2 " | PG 64-22                | 4% @ 70 GYR   |
|         | POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"              | SBS/SBR<br>PG 76-28/-22 | 4% @ 50 GYR   |
| PATCHES | CLASS D PATCHES, (BINDER IL-19.0 MM), 11"                         | PG 64-22*               | 4% @ 70 GYR   |
|         | HOT-MIX ASPHALT REPLACEMENT OVER PATCHES, (BINDER IL-19.0 MM)     | PG 64-22*               | 4% @ 70 GYR   |

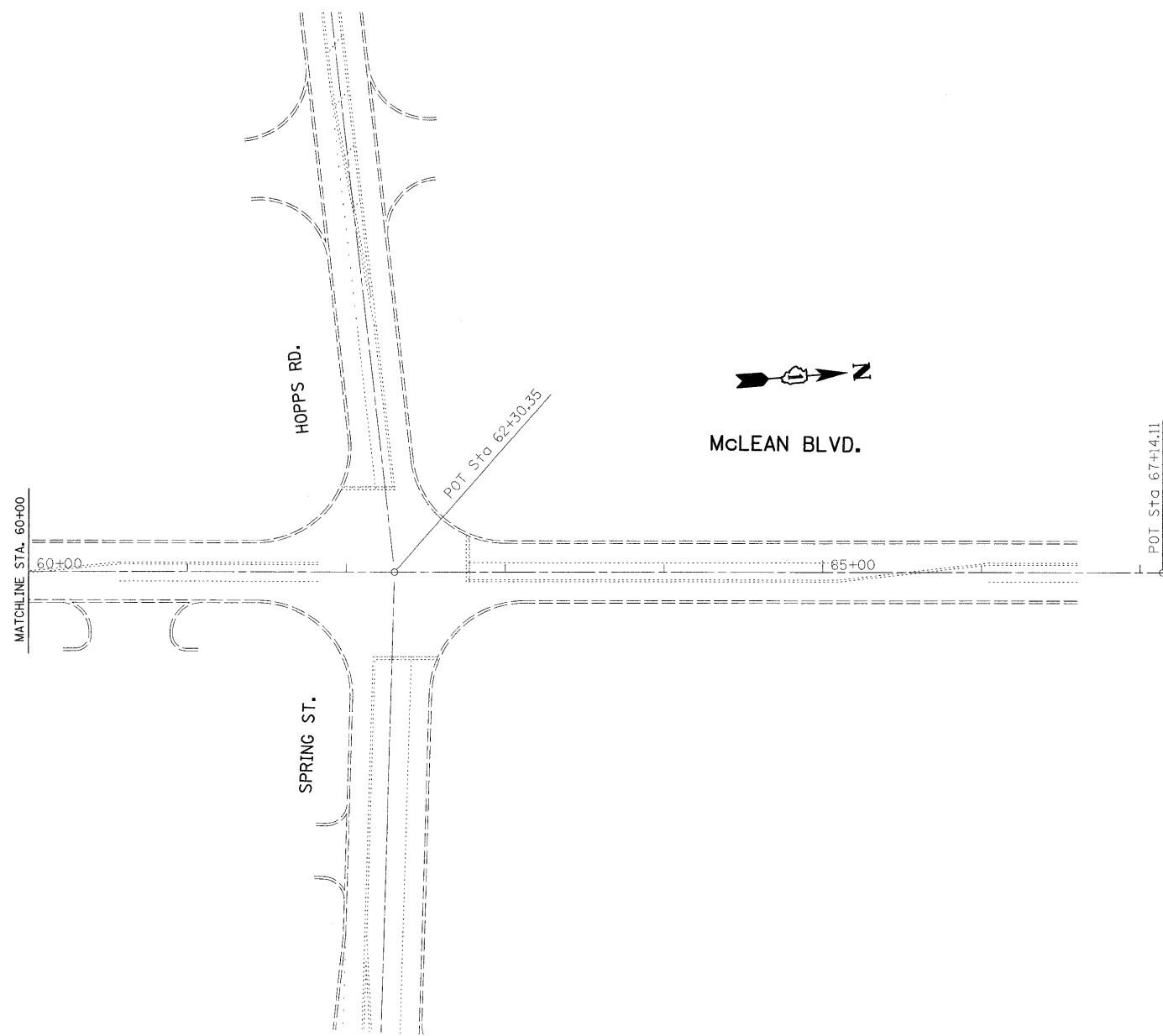
THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

\* WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22









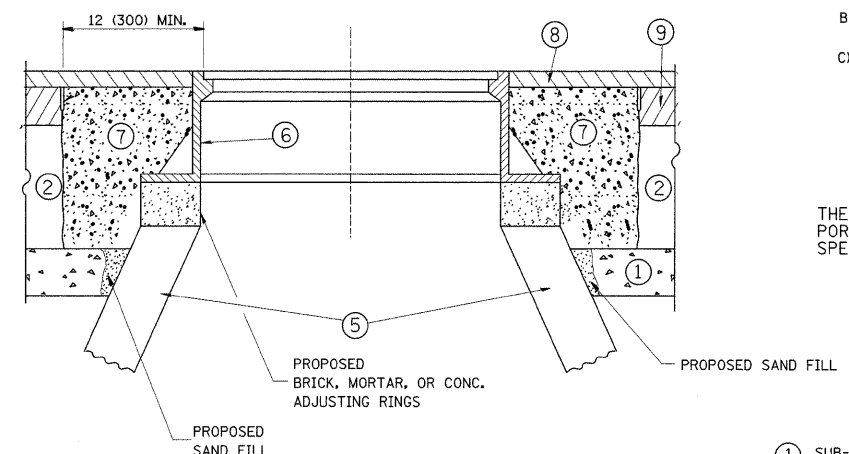
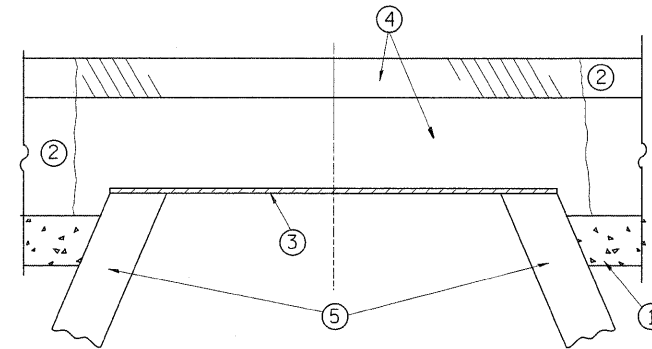
**NOTE**

LIMIT OF RESURFACING ON SIDE-STREETS THROUGH OUT THE IMPROVEMENT SHALL BE TO THE RADIUS OF RETURN OR AS DIRECTED BY THE ENGINEER.

ALL FINAL PAVEMENT MARKINGS SHALL BE THRMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL, (TC-13).

THE WIDTH OF PROP. AGGREGATE WEDGE SHOULDER IS 3 FT ON BOTH SIDE.

|   |                             |            |           |   |   |                           |                     |                    |                       |                   |  |  |
|---|-----------------------------|------------|-----------|---|---|---------------------------|---------------------|--------------------|-----------------------|-------------------|--|--|
| FILE NAME =<br>c:\pw_work\pwidot\yoonje\ad0136065\McLe... | USER NAME = yoonje          | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>ROADWAY AND PAVEMENT MARKING PLAN<br/>MCLEAN AVE. (SPRING ST. - S. LANCASTER CIRCLE)</b> | F.A.U<br>RTE.<br>2509     | SECTION<br>94RS     | COUNTY<br>KANE     | TOTAL<br>SHEETS<br>20 | SHEET<br>NO.<br>9 |  |  |
|   | PLOT SCALE = 50,0000' / IN. | DRAWN -    | REVISED - |   |   | SCALE: 1"=50'             | SHEET NO. OF SHEETS | STA. 60+00 TO STA. | CONTRACT NO. 60H04    |                   |  |  |
|   | PLOT DATE = 4/18/2009       | CHECKED -  | REVISED - |   |   | ILLINOIS FED. AID PROJECT |                     |                    |                       |                   |  |  |
|   |                             | DATE -     | REVISED - |   |   |                           |                     |                    |                       |                   |  |  |



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

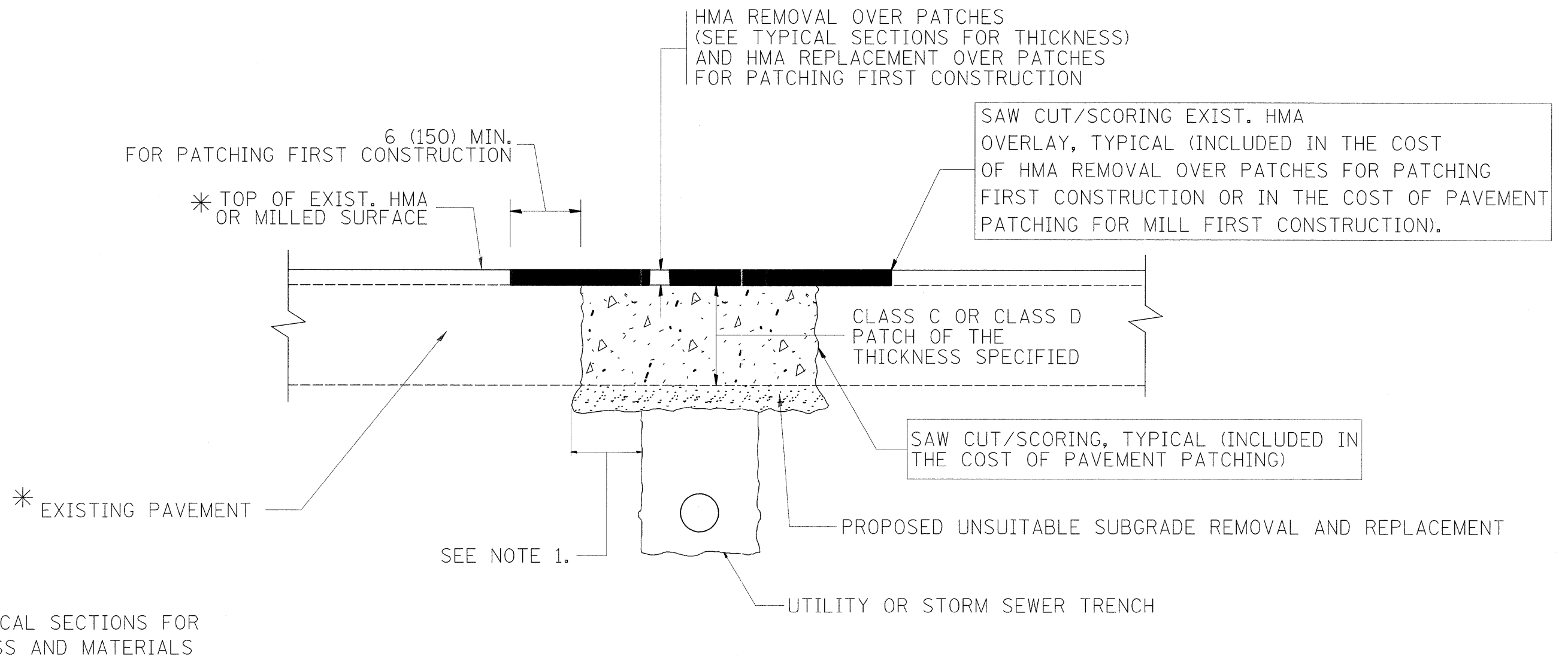
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

|  |                             |                    |                                |   |  |                         |                  |                        |             |                    |              |
|--|-----------------------------|--------------------|--------------------------------|---|--|-------------------------|------------------|------------------------|-------------|--------------------|--------------|
| FILE NAME =                                | USER NAME = yoonje          | DESIGNED - R. SHAH | REVISED - R. SHAH 03-10-95     | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>DETAILS FOR<br/>FRAMES AND LIDS ADJUSTMENT WITH MILLING</b> |                         | F.A.U. RTE. 2509 | SECTION 94RS           | COUNTY KANE | TOTAL SHEETS 20    | SHEET NO. 10 |
| c:\pwork\PWIDOT\Y00NJE\0137198\01stS.d.dgn |                             | DRAWN -            | REVISED - A. ABBAS 03-21-97    |   | SCALE: NONE  | SHEET NO. 1 OF 1 SHEETS | STA. TO STA.     | <b>BD600-03 (BD-8)</b> |             | CONTRACT NO. 60H04 |              |
|  | PLOT SCALE = 50.0000' / IN. | CHECKED -          | REVISED - R. WIEDEMAN 05-14-04 |   | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT                |                         |                  |                        |             |                    |              |
|  | PLOT DATE = 4/18/2009       | DATE - 10-25-94    | REVISED - R. BORO 01-01-07     |   |  |                         |                  |                        |             |                    |              |



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

|   |                             |                    |                             |   |  |   |         |                         |              |              |
|---|-----------------------------|--------------------|-----------------------------|---|--|---|---------|-------------------------|--------------|--------------|
| FILE NAME =                             | USER NAME = yoonje          | DESIGNED - R. SHAH | REVISED - A. ABBAS 04-27-98 | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>PAVEMENT PATCHING FOR<br/>HMA SURFACED PAVEMENT</b> | F.A. RTE.                                       | SECTION | COUNTY                  | TOTAL SHEETS | SHEET NO.    |
| ci:\pw_work\PWIDOT\YDONJE\0137198\DistS | d.dgn                       | DRAWN -            | REVISED - R. BORO 01-01-07  |   |  | 2509  | 94RS    | KANE                    | 20           | 11           |
|   | PLOT SCALE = 50,0000' / IN. | CHECKED -          | REVISED - R. BORO 09-04-07  |   |  | <b>BD400-04 (BD-22)</b>                         |         | CONTRACT NO. 60H04      |              |              |
|   | PLOT DATE = 4/18/2009       | DATE - 10-25-94    | REVISED - K. ENG 10-27-08   |   |  | SCALE: NONE                                     |         | SHEET NO. 1 OF 1 SHEETS |              | STA. TO STA. |
|   |                             |                    |                             |   |  | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |         |                         |              |              |

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001  
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SALT TOLERANT SOD AND TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

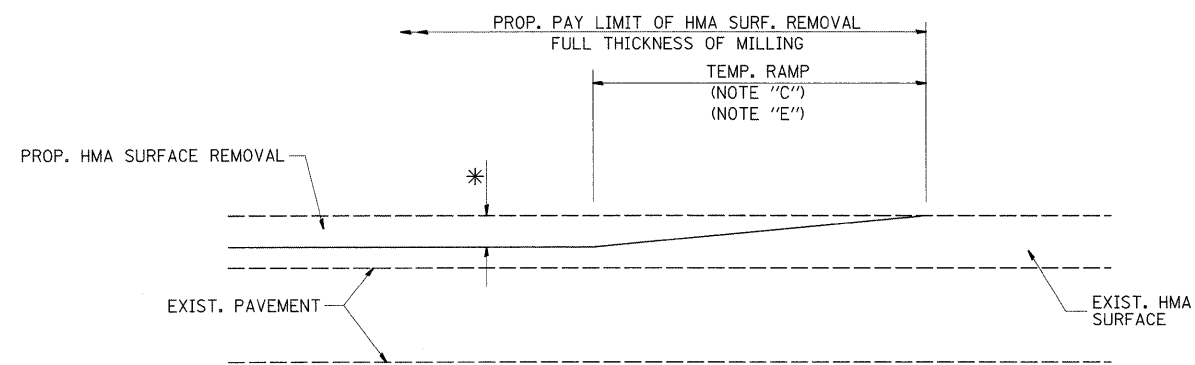
**BASIS OF PAYMENT:**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

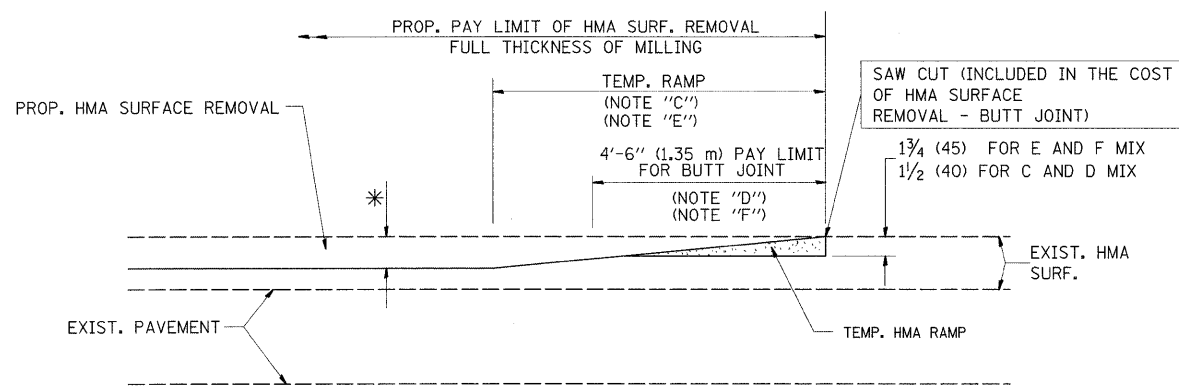
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

|  |                             |                      |                             |   |  |                  |                         |   |                         |              |                    |
|--|-----------------------------|----------------------|-----------------------------|---|--|------------------|-------------------------|---|-------------------------|--------------|--------------------|
| FILE NAME =                              | USER NAME = yoonje          | DESIGNED - A. HOUSEH | REVISED - R. SHAH 10-03-96  | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>CURB OR CURB AND GUTTER<br/>REMOVAL AND REPLACEMENT</b> | F.A.U. RTE. 2509 | SECTION 94RS            | COUNTY KANE                                     | TOTAL SHEETS 20         | SHEET NO. 12 |                    |
| ct:\pw_work\VPWIDOT\Y00NJE\0137198\DistS | sd.dgn                      | DRAWN -              | REVISED - A. ABBAS 03-21-97 |   |  | SCALE: NONE      | SHEET NO. 1 OF 1 SHEETS | STA. TO STA.                                    | <b>BD600-06 (BD-24)</b> |              | CONTRACT NO. 60H04 |
|  | PLOT SCALE = 50,0000' / IN. | CHECKED -            | REVISED - M. GOMEZ 01-22-01 |   |  |                  |                         | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                         |              |                    |
|  | PLOT DATE = 4/18/2009       | DATE - 03-11-94      | REVISED - R. BORO 01-01-07  |   |  |                  |                         |   |                         |              |                    |



MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

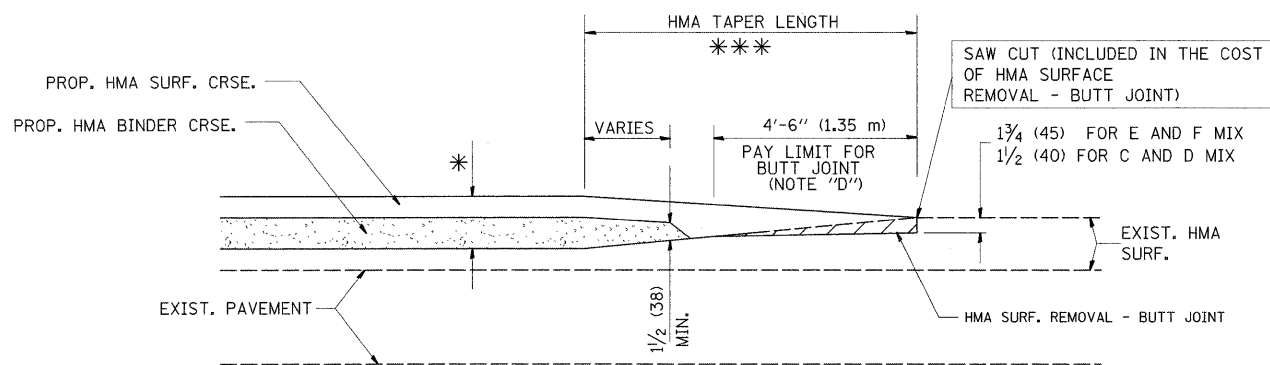
**OPTION 1**



HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

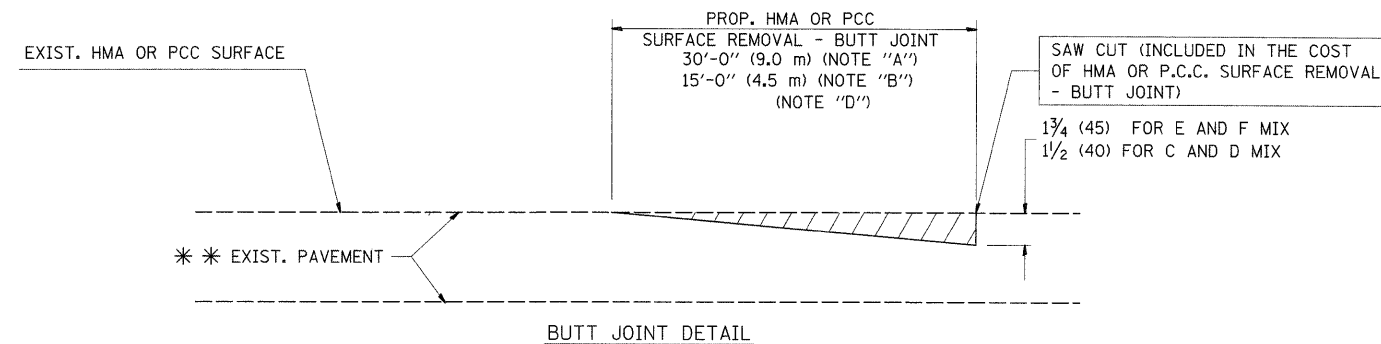
**OPTION 2**

**TYPICAL TEMPORARY RAMP**

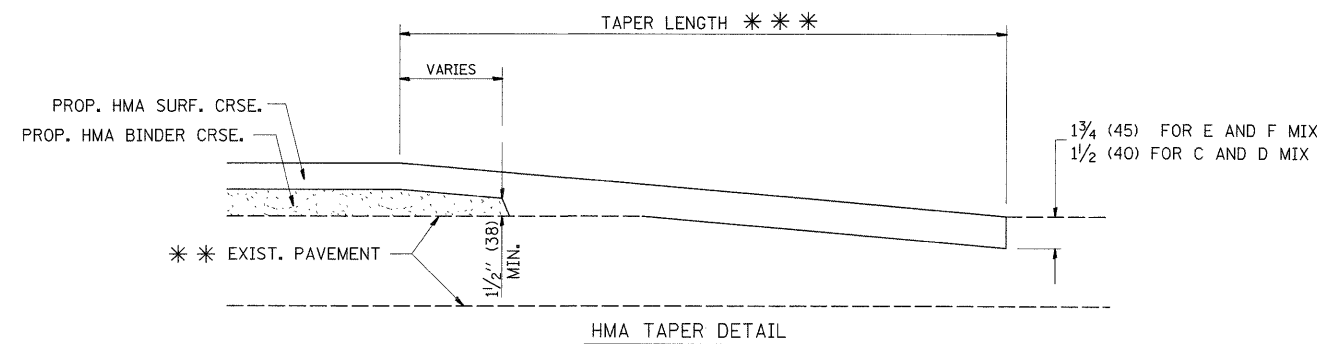


BUTT JOINT AND  
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

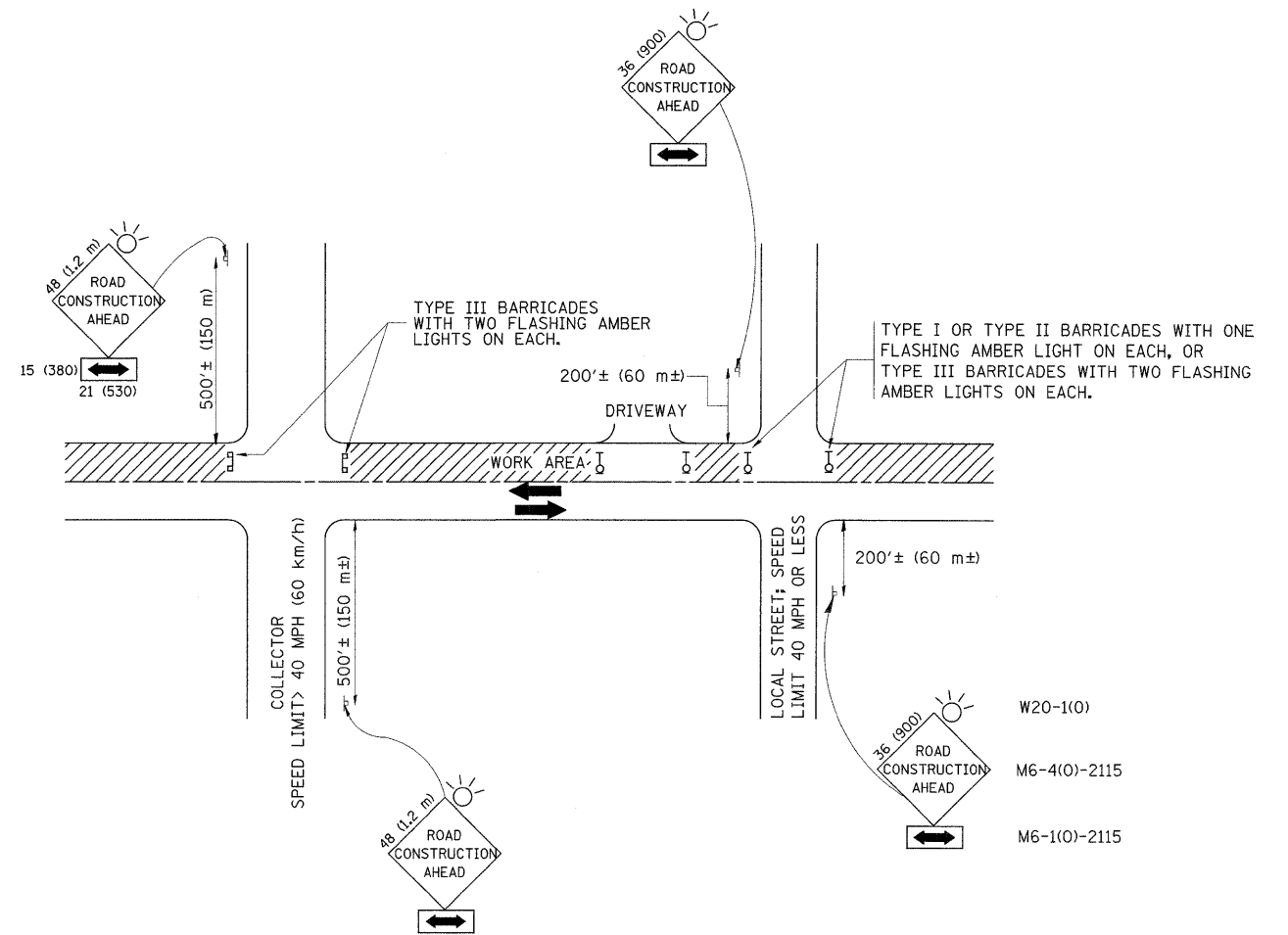
|   |                             |                       |                             |
|---|-----------------------------|-----------------------|-----------------------------|
| FILE NAME =                             | USER NAME = yoonje          | DESIGNED - M. DE YONG | REVISED - R. SHAH 10-25-94  |
| ct:\pwork\VP\WIDOT\Y00NJE\0137198\DistS | sd.dgn                      | DRAWN -               | REVISED - A. ABBAS 03-21-97 |
|   | PLOT SCALE = 50.0000' / IN. | CHECKED -             | REVISED - M. GOMEZ 04-06-01 |
|   | PLOT DATE = 4/18/2009       | DATE - 06-13-90       | REVISED - R. BORO 01-01-07  |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |         |        |                    |           |
|---|---------|--------|--------------------|-----------|
| F.A.U. RTE.                                     | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| 2509  | 94RS    | KANE   | 20                 | 13        |
| BD400-05 BD32                                   |         |        | CONTRACT NO. 60H04 |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |         |        |                    |           |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
  - USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
  - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
  - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

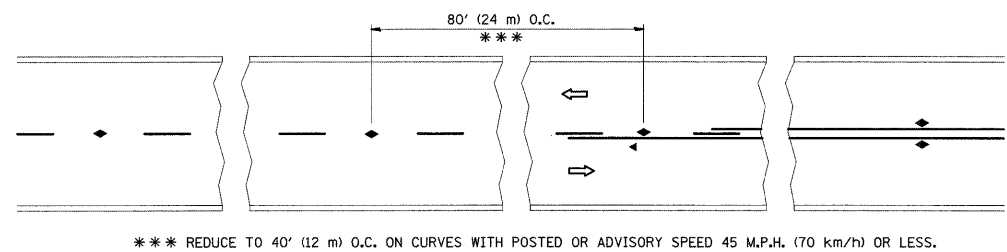
|  |                             |                |                                 |
|--|-----------------------------|----------------|---------------------------------|
| FILE NAME =                              | USER NAME = yoonje          | DESIGNED - LHA | REVISED - J. OBERLE 10-18-95    |
| ct:\pw_work\VPWIDOT\Y00NJE\0137198\DistS | id.dgn                      | DRAWN -        | REVISED - A. HOUSEH 03-06-96    |
|  | PLOT SCALE = 50.0000' / IN. | CHECKED -      | REVISED - A. HOUSEH 10-15-96    |
|  | PLOT DATE = 4/18/2009       | DATE - 06-89   | REVISED - T. RAMMACHER 01-06-00 |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

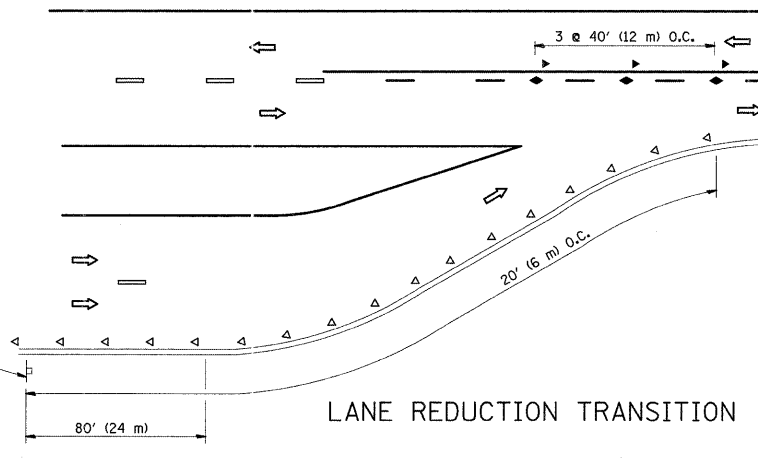
TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

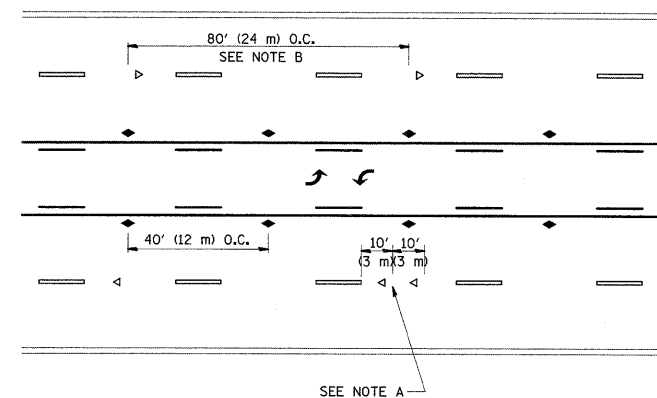
|   |         |        |                    |           |
|---|---------|--------|--------------------|-----------|
| F.A.U. RTE.                                     | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| 2509  | 94RS    | KANE   | 20                 | 14        |
| TC-10   |         |        | CONTRACT NO. 60H04 |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |         |        |                    |           |



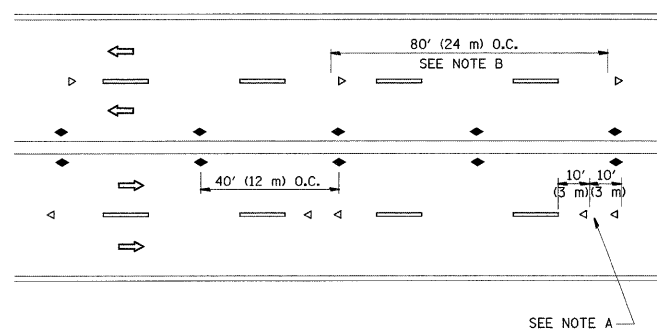
TWO-LANE/TWO-WAY



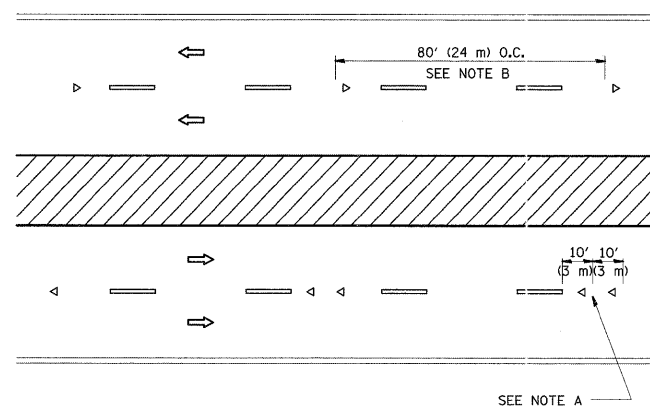
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

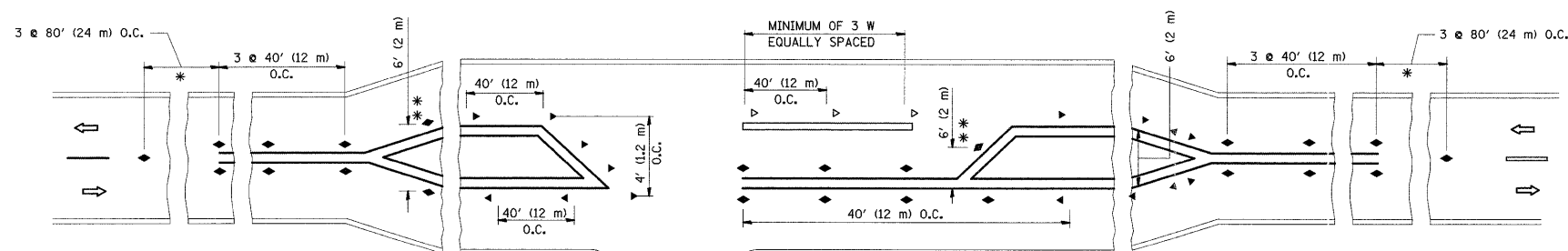
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

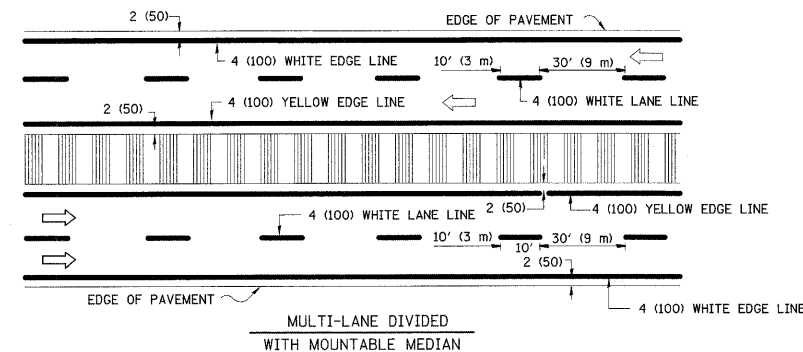
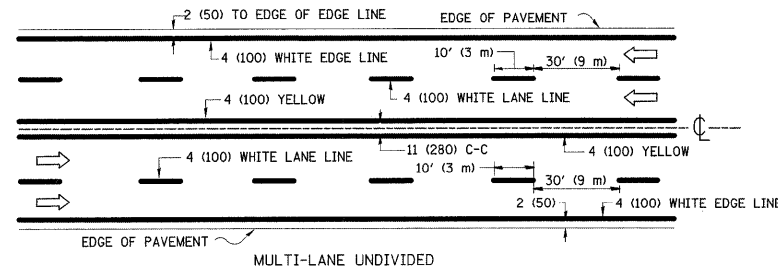
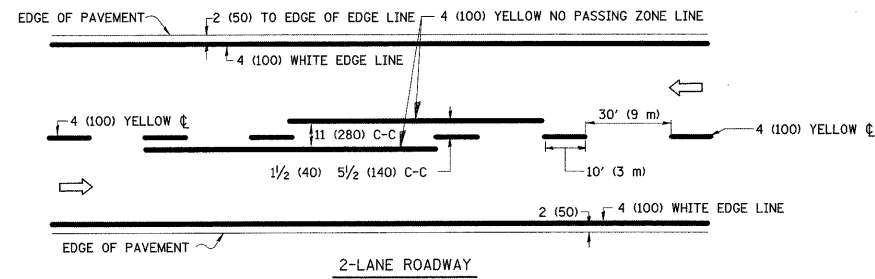


LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

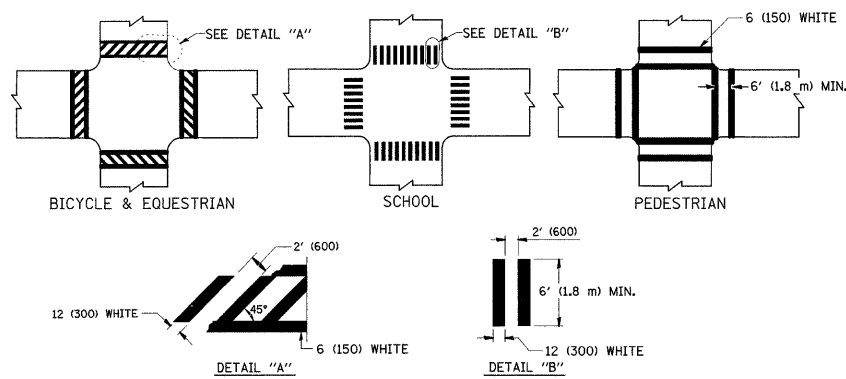
All dimensions are in inches (millimeters) unless otherwise shown.

|  |                             |                       |  |   |  |                         |      |                        |                 |                |                       |   |
|--|-----------------------------|-----------------------|--|---|--|-------------------------|------|------------------------|-----------------|----------------|-----------------------|---|
| FILE NAME =<br>c:\pwork\NPWIDOT\Y00NJE\0137198\DistS | USER NAME = yoonje<br>d.dgn | DESIGNED -<br>DRAWN - | REVISED - T. RAMMACH 09-19-94<br>REVISED - T. RAMMACH 03-12-99<br>REVISED - T. RAMMACH 01-06-00<br>REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>TYPICAL APPLICATIONS<br/>RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)</b> |                         |      | F.A.U.<br>RTE.<br>2509 | SECTION<br>94RS | COUNTY<br>KANE | TOTAL<br>SHEETS<br>20 | SHEET<br>NO.<br>15                              |
| PLOT SCALE = 50.0000' / IN.<br>PLOT DATE = 4/18/2009 |                             |                       |  |   | SCALE: NONE  | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.                | <b>TC-11</b>    |                | CONTRACT NO. 60H04    | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |

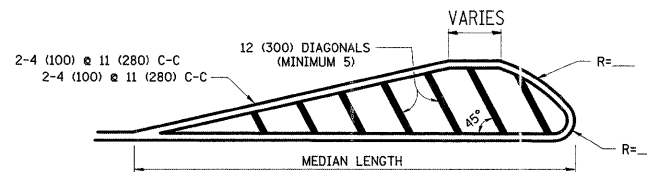
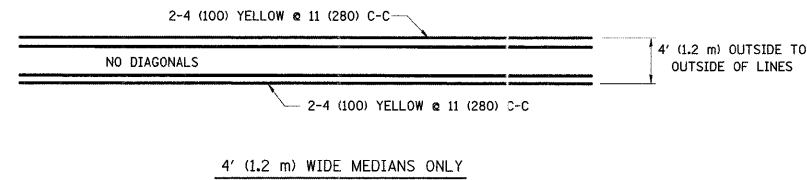


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

**TYPICAL LANE AND EDGE LINE MARKING**

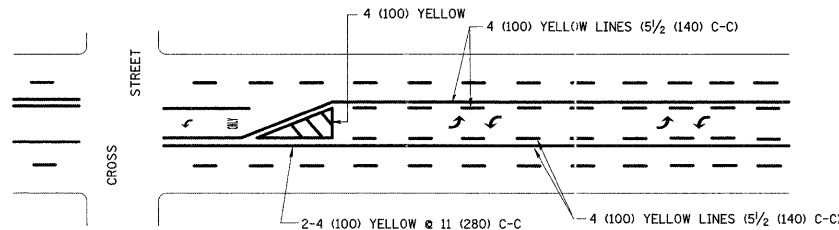


**TYPICAL CROSSWALK MARKING**

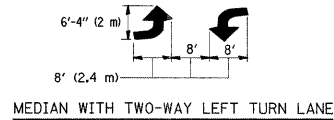


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

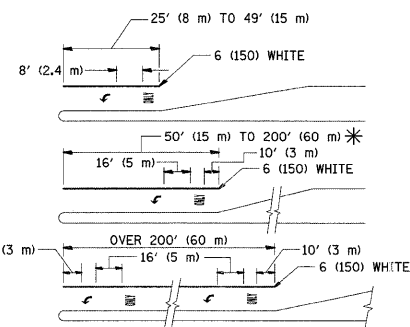
**MEDIANS OVER 4' (1.2 m) WIDE**



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



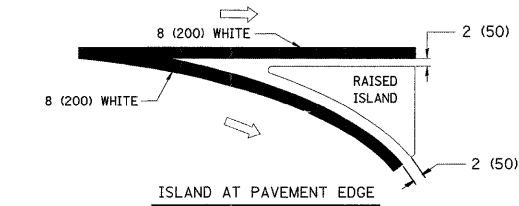
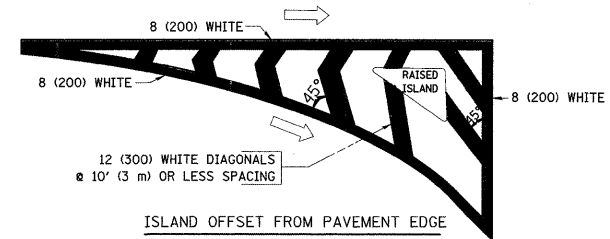
**TYPICAL PAINTED MEDIAN MARKING**



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**



**TYPICAL ISLAND MARKING**

| TYPE OF MARKING   | WIDTH OF LINE  | PATTERN                         | COLOR   | SPACING / REMARKS  |
|---|--|---------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT   | 4 (100)  | SKIP-DASH                       | YELLOW  | 10' (3 m) LINE WITH 30' (9 m) SPACE  |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT   | 2 @ 4 (100)  | SOLID                           | YELLOW  | 11 (280) C-C   |
| NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS                                      | 4 (100)<br>2 @ 4 (100)   | SOLID<br>SOLID                  | YELLOW<br>YELLOW                                  | 5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE<br>11 (280) C-C<br>OMIT SKIP-DASH CENTERLINE BETWEEN   |
| LANE LINES  | 4 (100)<br>5 (125) ON FREEWAYS   | SKIP-DASH<br>SKIP-DASH          | WHITE<br>WHITE                                    | 10' (3 m) LINE WITH 30' (9 m) SPACE  |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)                                   | SAME AS LINE BEING EXTENDED  | SKIP-DASH                       | SAME AS LINE BEING EXTENDED                       | 2' (600) LINE WITH 6' (1.8 m) SPACE  |
| EDGE LINES  | 4 (100)  | SOLID                           | YELLOW-LEFT<br>WHITE-RIGHT                        | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB  |
| TURN LANE MARKINGS  | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))                                      | SOLID                           | WHITE   | SEE TYPICAL TURN LANE MARKING DETAIL   |
| TWO WAY LEFT TURN MARKING   | 2 @ 4 (100) EACH DIRECTION<br>8' (2.4m) LEFT ARROW   | SKIP-DASH AND SOLID<br>IN PAIRS | YELLOW<br>WHITE                                   | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE<br>SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL           |
| CROSSWALK LINES (PEDESTRIAN)<br>A. DIAGONALS (BIKE & EQUESTRIAN)<br>B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150)<br>12 (300) @ 45°<br>12 (300) @ 90°  | SOLID<br>SOLID<br>SOLID         | WHITE<br>WHITE<br>WHITE                           | NOT LESS THAN 6' (1.8 m) APART<br>2' (600) APART<br>2' (600) APART<br>SEE TYPICAL CROSSWALK MARKING DETAILS.   |
| STOP LINES  | 24 (600)   | SOLID                           | WHITE   | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS   | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°<br>NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID                           | YELLOW: TWO WAY TRAFFIC<br>WHITE: ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE<br>SEE TYPICAL PAINTED MEDIAN MARKING.  |
| GORE MARKING AND CHANNELIZING LINES   | 8 (200) WITH 12 (300) DIAGONALS @ 45°  | SOLID                           | WHITE   | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h))<br>20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)<br>30' (9 m) C-C (OVER 45MPH (70 km/h))                 |
| RAILROAD CROSSING   | 24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"               | SOLID                           | WHITE   | SEE STATE STANDARD 780001<br>AREA OF "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH<br>"X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )   |
| SHOULDER DIAGONALS  | 12 (300) @ 45°   | SOLID                           | WHITE - RIGHT<br>YELLOW - LEFT                    | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))<br>75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h))<br>150' (45 m) C-C (OVER 45MPH (70 km/h))                       |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

|   |                             |                  |                                 |
|---|-----------------------------|------------------|---------------------------------|
| FILE NAME =                             | USER NAME = yoonje          | DESIGNED - EVERS | REVISED - T. RAMMACHER 10-27-94 |
| ct:\pwwork\VPWIDOT\Y00NJE\0137198\DistS | jdjgn                       | DRAWN -          | REVISED - A. HOUSEH 10-09-96    |
|   | PLOT SCALE = 50.0000' / IN. | CHECKED -        | REVISED - A. HOUSEH 10-17-96    |
|   | PLOT DATE = 4/18/2009       | DATE - 03-19-90  | REVISED - T. RAMMACHER 01-06-00 |

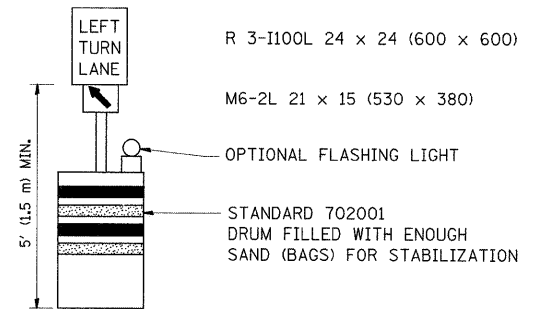
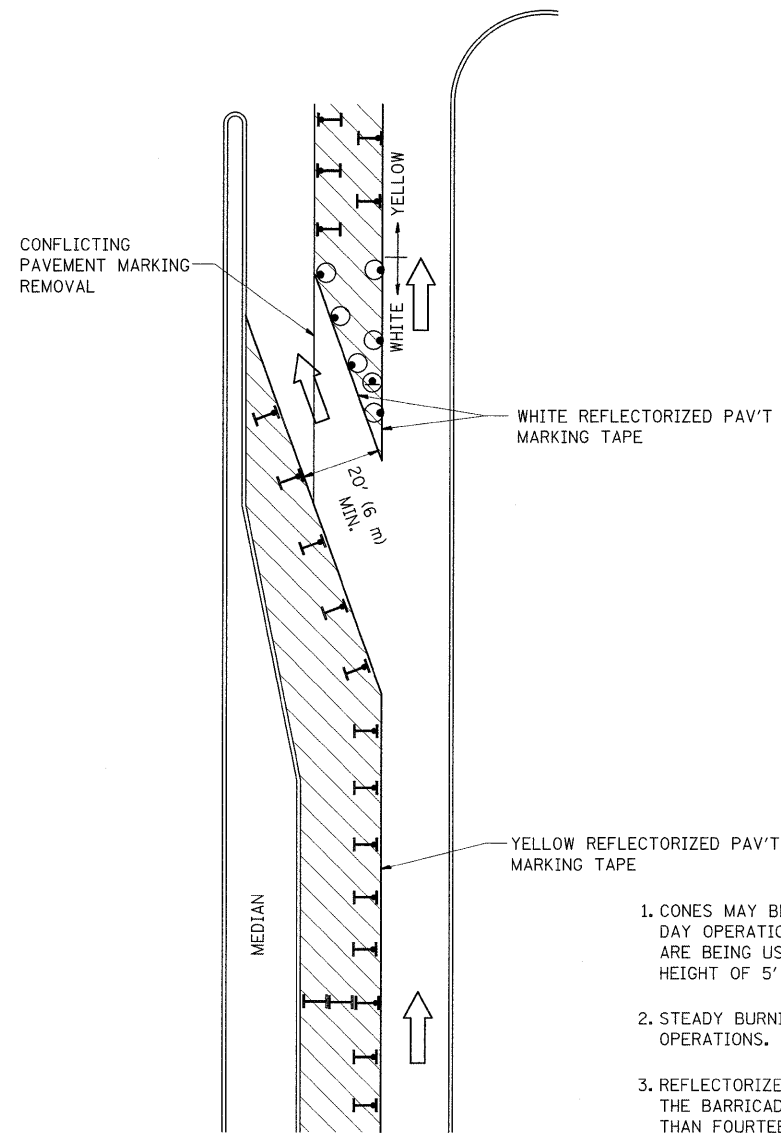
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

|   |         |        |                    |           |
|---|---------|--------|--------------------|-----------|
| F.A.U. RTE.                                     | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| 2509  | 94RS    | KANE   | 20                 | 16        |
| TC-13   |         |        | CONTRACT NO. 60H04 |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |         |        |                    |           |

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.











**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

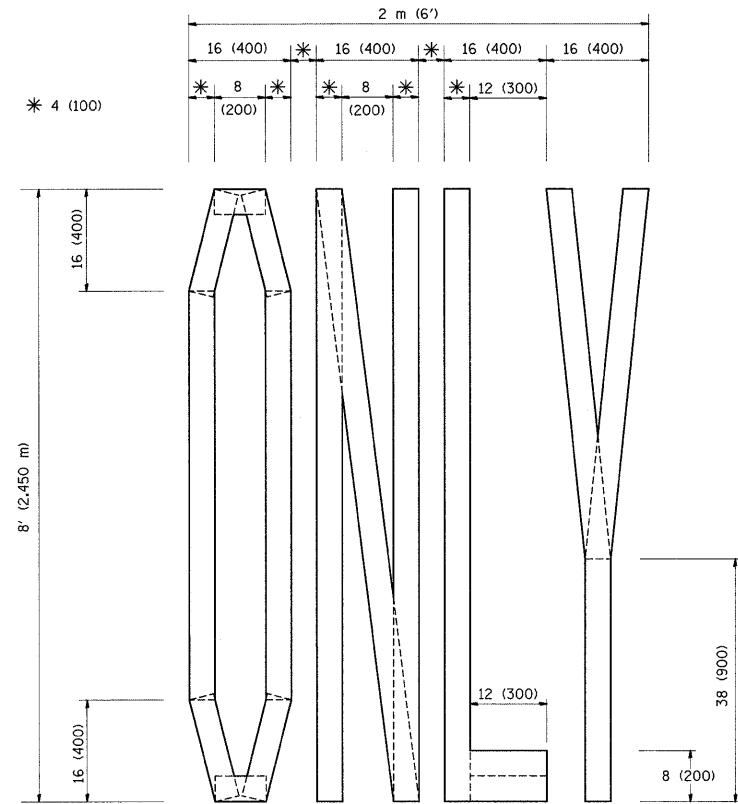
|  |                             |            |                                |
|--|-----------------------------|------------|--------------------------------|
| FILE NAME =                            | USER NAME = yoonje          | DESIGNED - | REVISED -T. RAMMACHER 09-08-94 |
| ca\pw_work\PWIDOT\Y00NJE\0137198\DistS | d.dgn                       | DRAWN -    | REVISED - A. HOUSEH 11-07-95   |
|  | PLOT SCALE = 50.0000' / IN. | CHECKED -  | REVISED - A. HOUSEH 10-12-96   |
|  | PLOT DATE = 4/18/2009       | DATE -     | REVISED -T. RAMMACHER 01-06-00 |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

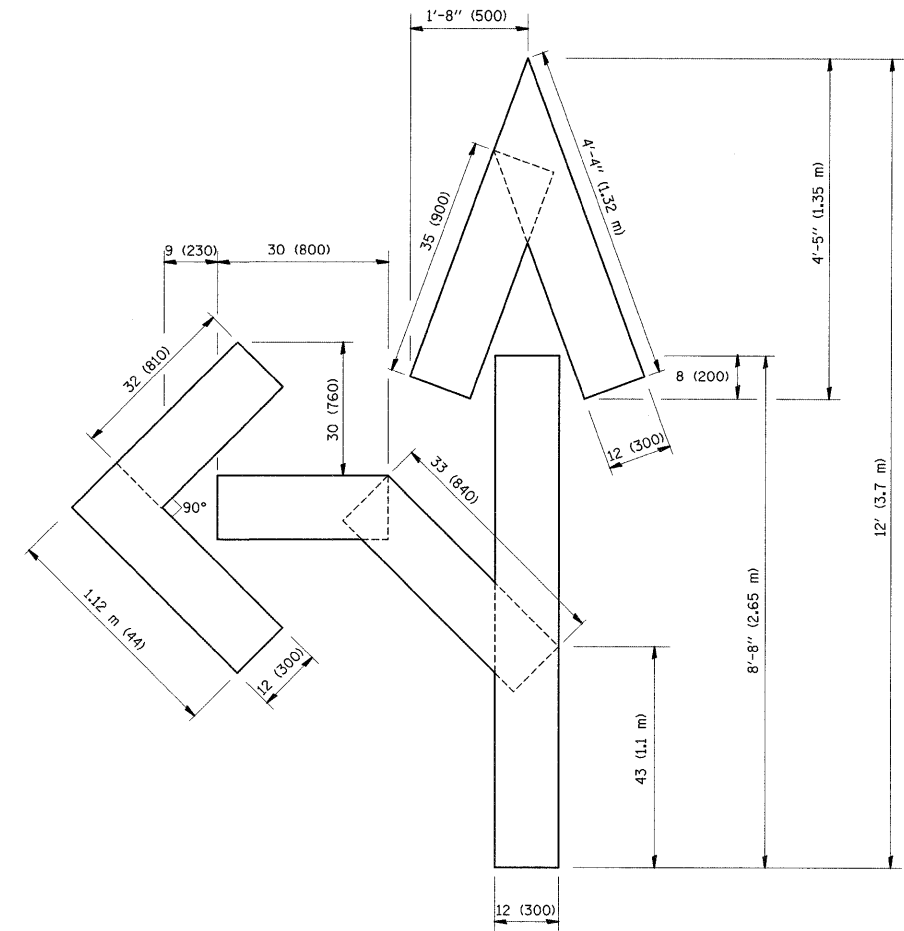
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

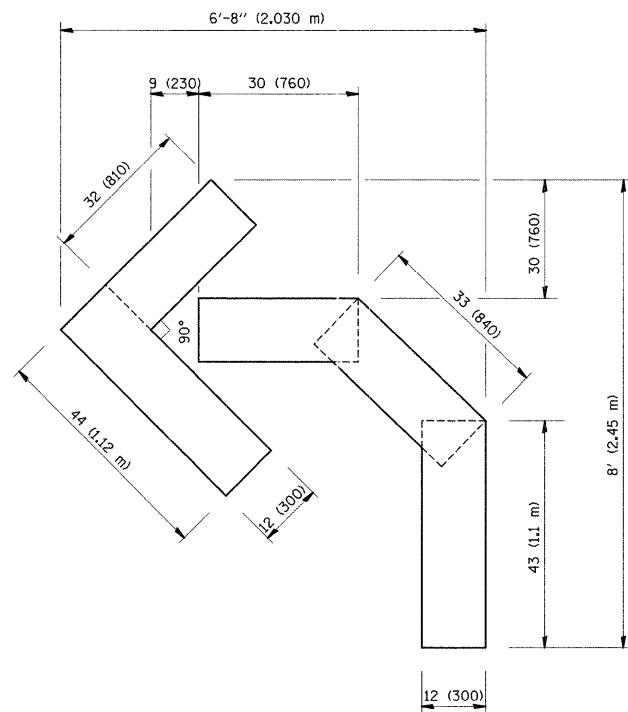
|   |         |        |                    |           |
|---|---------|--------|--------------------|-----------|
| F.A.U. RTE.                                     | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| 2509  | 94RS    | KANE   | 20                 | 17        |
| TC-14   |         |        | CONTRACT NO. 60H04 |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |         |        |                    |           |



QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

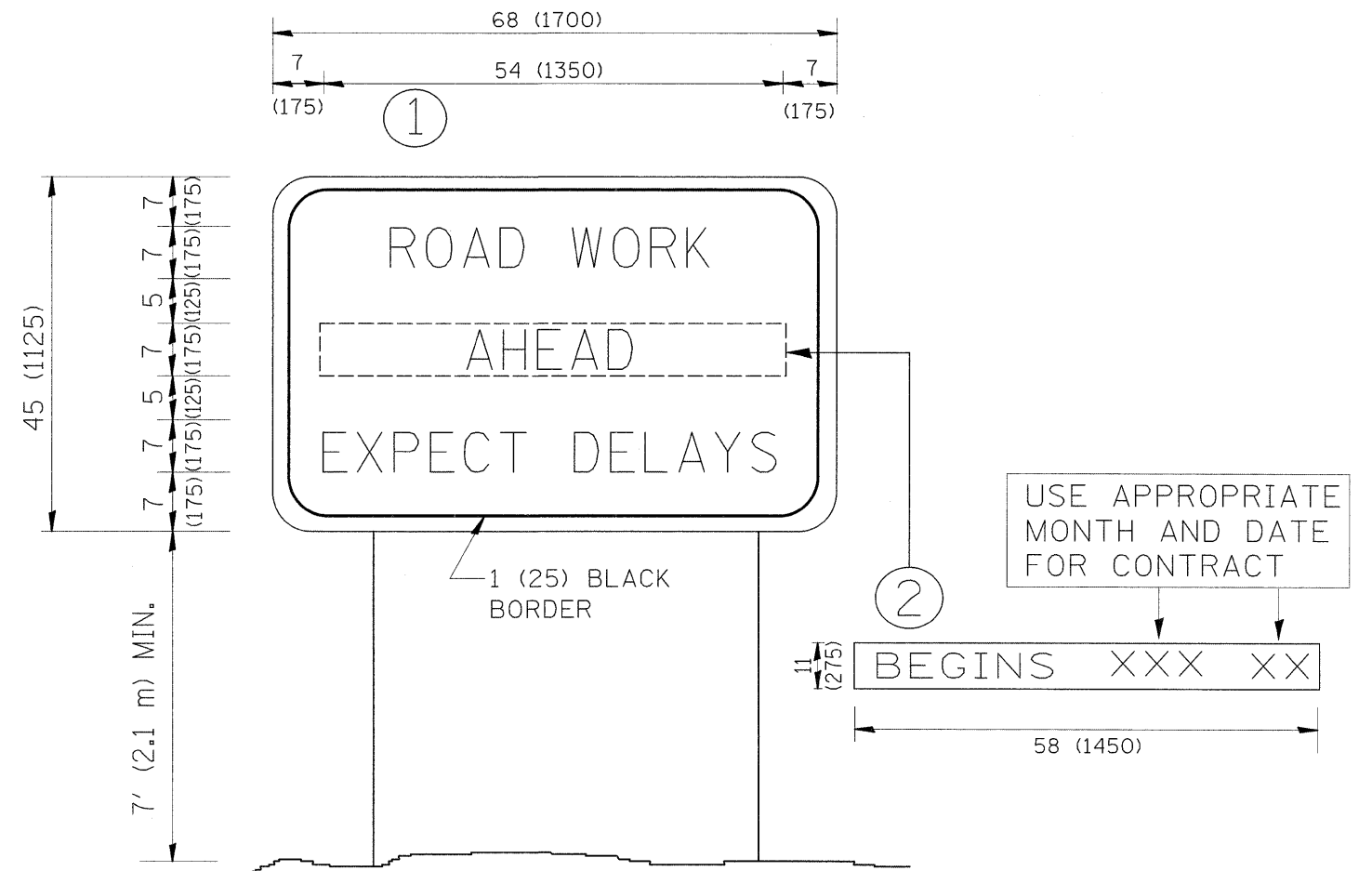
|   |                    |            |                                |
|---|--------------------|------------|--------------------------------|
| FILE NAME =                             | USER NAME = yoonje | DESIGNED - | REVISED -T. RAMMACHER 06-05-96 |
| c:\pwwork\VPWIDOT\Y00NJE\08137198\DistS | d.dgn              | DRAWN -    | REVISED -T. RAMMACHER 11-04-97 |
| PLOT SCALE = 50.0000' / IN.             | CHECKED -          | REVIS      | REVISED -T. RAMMACHER 03-02-98 |
| PLOT DATE = 4/18/2009                   | DATE - 09-18-94    | REVISED    | REVISED -E. GOMEZ 08-28-00     |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |         |        |                    |           |
|---|---------|--------|--------------------|-----------|
| F.A.U. RTE.                                     | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| 2509  | 94RS    | KANE   | 20                 | 18        |
| TC-16   |         |        | CONTRACT NO. 60H04 |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |         |        |                    |           |



**NOTES:**

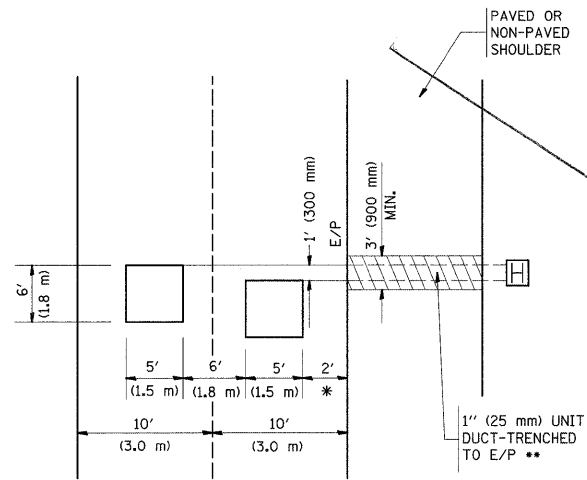
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

|  |                    |            |                                 |   |   |                         |      |                           |   |        |                 |              |
|--|--------------------|------------|---------------------------------|---|---|-------------------------|------|---------------------------|---|--------|-----------------|--------------|
| FILE NAME =                                | USER NAME = yoonje | DESIGNED - | REVISED - R. MIRS 09-15-97      | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>ARTERIAL ROAD<br/>INFORMATION SIGN</b> |                         |      | F.A.U.<br>RTE.            | SECTION   | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO. |
| c:\pw_work\VPWIDOT\Y00NJE\0137198\01st.dgn |                    | DRAWN -    | REVISED - R. MIRS 12-11-97      |   | 2509                                      | 94RS                    | KANE | 20                        | 19  |        |                 |              |
|  |                    | CHECKED -  | REVISED - T. RAMMACHER 02-02-99 |   | <b>TC-22</b>                              |                         |      | <b>CONTRACT NO. 60H04</b> |   |        |                 |              |
|  |                    | DATE -     | REVISED - C. JUCIUS 01-31-07    |   | SCALE: NONE                               | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.                   | FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT |        |                 |              |

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



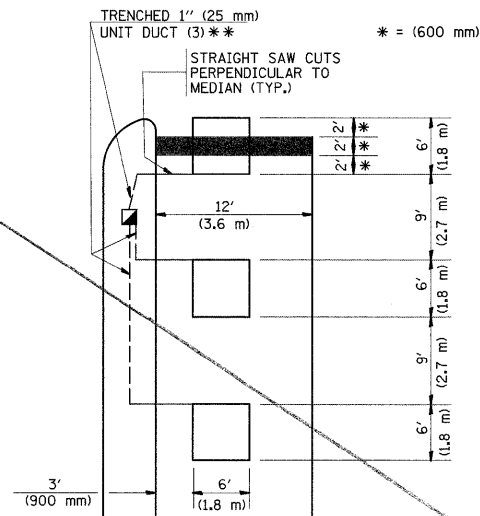
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



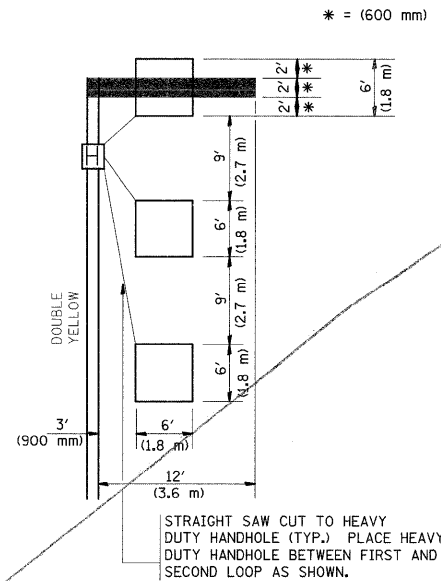
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

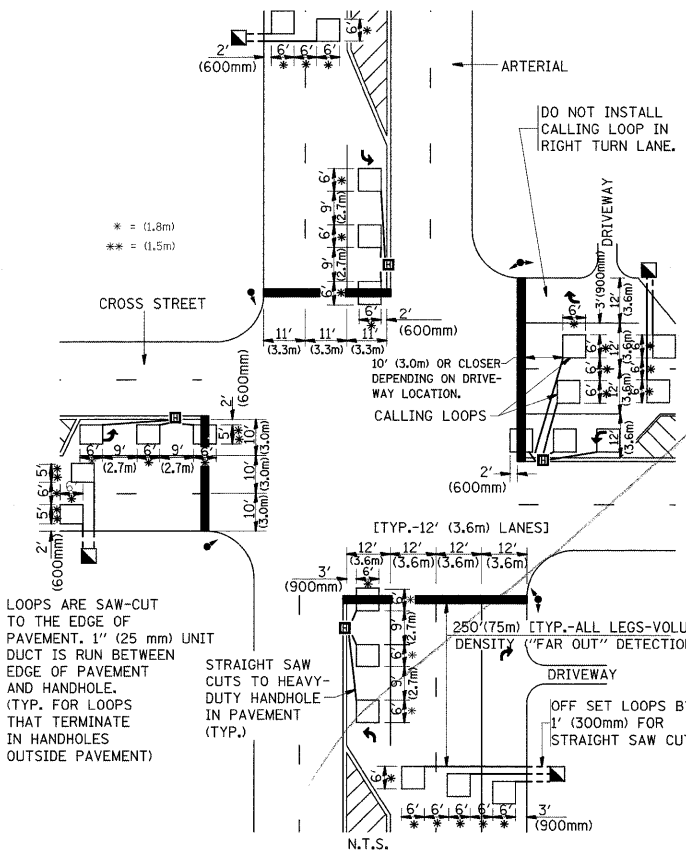
(PROTECTED / PERMITTED LEFT TURN PHASING)



\* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**

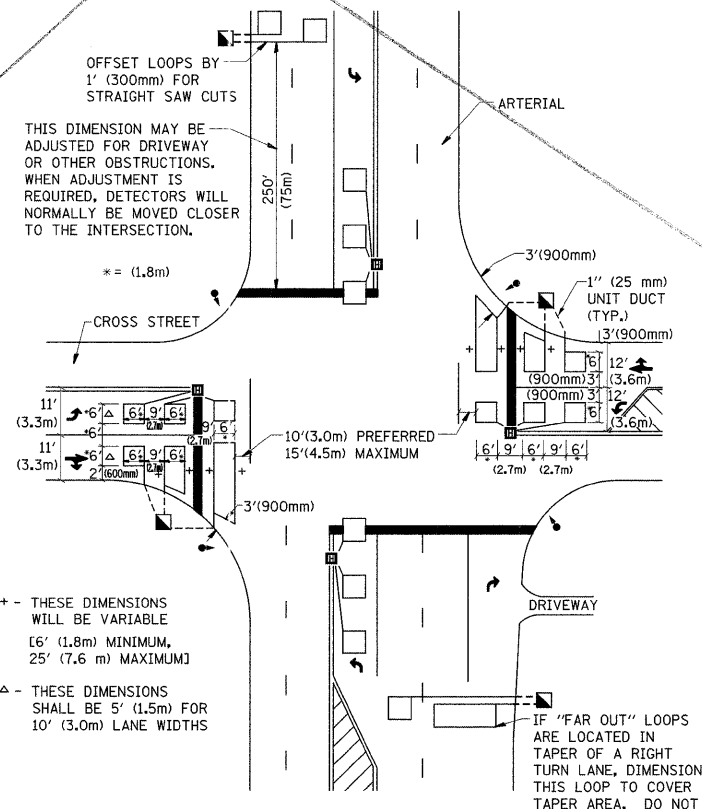


LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

Δ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

**DETAIL 2**  
N.T.S.

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

|   |             |                  |           |   |  |  |  |              |                         |        |              |   |       |
|---|-------------|------------------|-----------|---|--|--|--|--------------|-------------------------|--------|--------------|---|-------|
| FILE NAME =                               | USER NAME = | DESIGNED -       | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>DISTRICT 1 - DETECTOR LOOP INSTALLATION<br/>DETAILS FOR ROADWAY RESURFACING</b> |  |  | F.A.U. RTE.  | SECTION                 | COUNTY | TOTAL SHEETS | SHEET NO.                                       |       |
| ca:\pw_work\p\WIDOT\Y00NJE\d8137198\DistS | ed.dgn      | DRAWN -          | REVISED - |   |  |  |  | 2509         | 94RS                    | KANE   | 20           | 20  |       |
|   |             | CHECKED - R.K.F. | REVISED - |   |  |  |  | <b>TS-07</b> |                         |        |              | CONTRACT NO.                                    | 60H04 |
|   |             | DATE -           | REVISED - |   |  |  |  | SCALE: NONE  | SHEET NO. 1 OF 1 SHEETS | STA.   | TO STA.      | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |       |