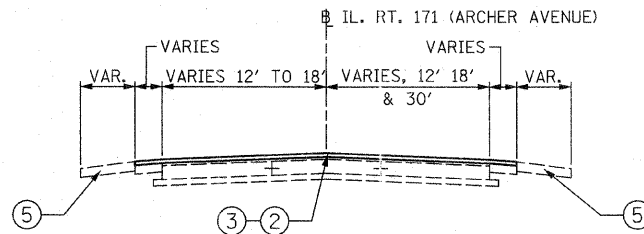
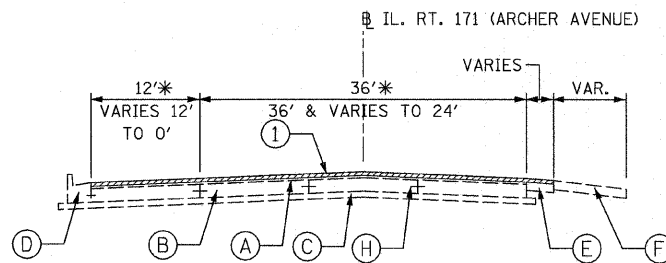


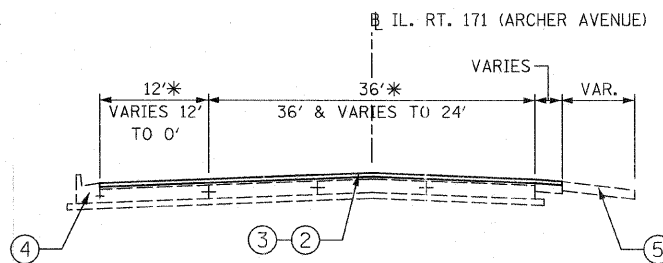
EXISTING TYPICAL SECTION
 STA. 25+10 TO STA. 29+10
 STA. 190+10 TO STA. 196+80



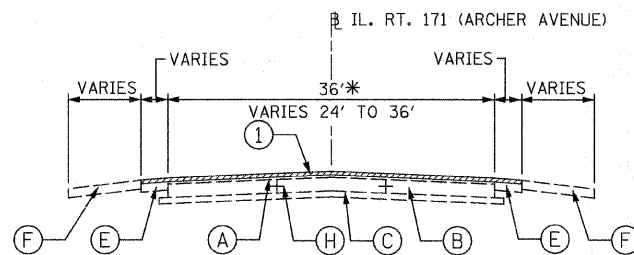
PROPOSED TYPICAL SECTION
 STA. 25+10 TO STA. 29+10
 STA. 190+10 TO STA. 196+80



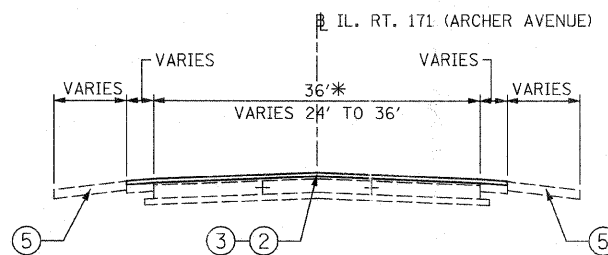
EXISTING TYPICAL SECTION
 * STA. 17+75 TO STA. 25+10
 STA. 97+00 TO STA. 100+90



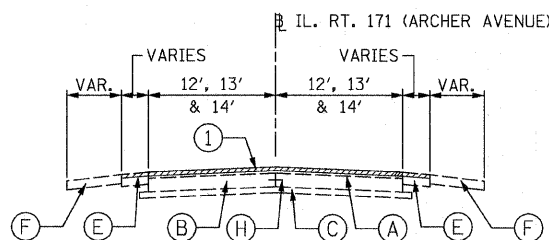
PROPOSED TYPICAL SECTION
 * STA. 17+75 TO STA. 25+10
 STA. 97+00 TO STA. 100+90



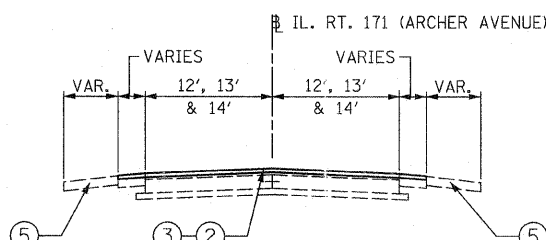
EXISTING TYPICAL SECTION
 * STA. 15+50 TO STA. 17+75
 STA. 89+50 TO STA. 97+00
 STA. 108+00 TO STA. 11600
 STA. 143+10 TO STA. 152+15
 STA. 161+18 TO STA. 181+50
 STA. 183+00 TO STA. 187+70
 STA. 202+50 TO STA. 211+10



PROPOSED TYPICAL SECTION
 * STA. 15+50 TO STA. 17+75
 STA. 89+50 TO STA. 97+00
 STA. 108+00 TO STA. 11600
 STA. 143+10 TO STA. 152+15
 STA. 161+18 TO STA. 181+50
 STA. 183+00 TO STA. 187+70
 STA. 202+50 TO STA. 211+10



EXISTING TYPICAL SECTION
 STA. 13+41 TO STA. 15+50
 STA. 35+55 TO STA. 56+44
 STA. 56+44 TO STA. 71+00 - PAVING OMISSION
 STA. 71+00 TO STA. 89+50
 STA. 100+90 TO STA. 108+00
 STA. 123+10 TO STA. 124+60
 STA. 127+10 TO STA. 143+10
 STA. 187+70 TO STA. 190+10
 STA. 211+10 TO STA. 254+00



PROPOSED TYPICAL SECTION
 STA. 13+41 TO STA. 15+50
 STA. 35+55 TO STA. 56+44
 STA. 56+44 TO STA. 71+00 - PAVING OMISSION
 STA. 71+00 TO STA. 89+50
 STA. 100+90 TO STA. 108+00
 STA. 123+10 TO STA. 124+60
 STA. 127+10 TO STA. 143+10
 STA. 187+70 TO STA. 190+10
 STA. 211+10 TO STA. 254+00

EXISTING CONDITIONS

- (A) HOT-MIX ASPHALT SURFACE AND BINDER COURSE, 3" & VARIES
- (B) P.C. CONCRETE BASE COURSE / HMA PAVEMENT, 9"
- (C) AGGREGATE SUBGRADE
- (D) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- (E) HMA SHOULDER
- (F) AGGREGATE SHOULDER
- (G) CONCRETE MEDIAN, TYPE SB-6
- (H) TIE BARS

PROPOSED IMPROVEMENTS

- (1) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- (2) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- (4) COMB. CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATIONS AS DETERMINED BY THE ENGINEER)
- (5) AGGREGATE SHOULDERS, TY. B (SEE NOTE 2)

NOTES:

1. THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.
2. THE PAY ITEM "AGGREGATE SHOULDERS, TYPE B" SHALL BE USED AT LOCATIONS WHERE THERE IS VERTICAL ELEVATION DIFFERENCE BETWEEN EDGE OF PAVEMENT AND TOP OF EXISTING SHOULDER. THE WIDTH AND LOCATION OF THE SHOULDER RESTORATION SHALL BE AS DETERMINED IN THE FIELD BY THE ENGINEER. ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS.

ROADWAY NAME	HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
	MIXTURE TYPE	AC TYPE	AIR VOIDS
IL. RTE. 171	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	PG 64-22	4% @ 70 GYR.
	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	SBS/SBR PG 76-28/-22	4% @ 50 GYR.
	CLASS D PATCHES (HMA BINDER IL-19 mm)	* PG 64-22	4% @ 70 GYR.
	HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	* PG 64-22	4% @ 70 GYR.
	DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm)	PG 64-22 * PG 64-22 / 58-22	4% @ 50 GYR. 4% @ 50 GYR.

* WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.
 THE UNIT WEIGHT USED TO CALCULATE ALL HMA MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

FILE NAME = D168H43-shit-typical.dgn
 PLOT DATE = 4/22/2009

CHRISTIAN-ROGE & ASSOCIATES, INC.
 ENGINEERS-PLANNERS-SURVEYORS
 211 WEST WACKER DRIVE
 CHICAGO, ILLINOIS 60606
 PHONE: (312)372-2023 FAX: (312)372-5274

DESIGNED - G.F.L.
 DRAWN - B.K.
 CHECKED - M.P.
 DATE - APRIL 2009

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
IL. RTE. 171 (WILL COUNTY LINE TO IL. RTE. 83)

SCALE: NONE SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	3114 RS-1	COOK	26	4
CONTRACT NO. 60H43			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	