

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

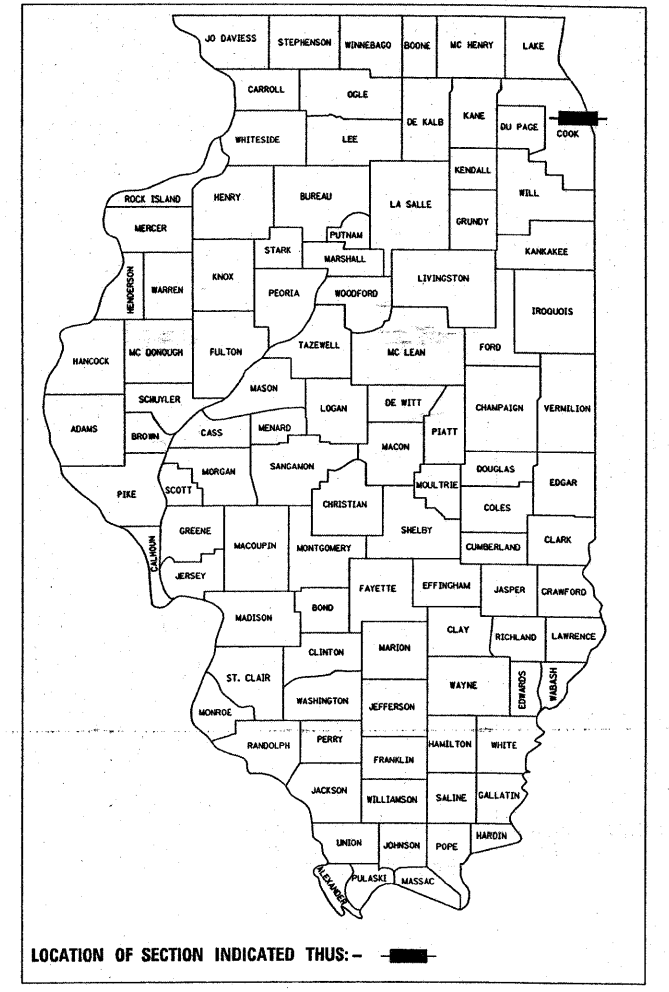
**PROPOSED
 HIGHWAY PLANS**

O.R. ROUTE 98: OAKWOOD BLVD
 MARTIN LUTHER KING DR. TO COTTAGE GROVE AVE.
 SECTION 2004-045 RS
 RESURFACING
 COOK COUNTY
 C-91-223-04

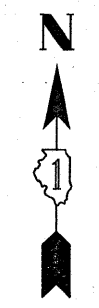
| O.R. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------|-------------|--------|--------------------|-----------|
| 98 | 2004-045 RS | COOK | 17 | 1 |
| ILLINOIS | | | CONTRACT NO. 62761 | |

FOR INDEX OF SHEETS, SEE SHEET NO. 2

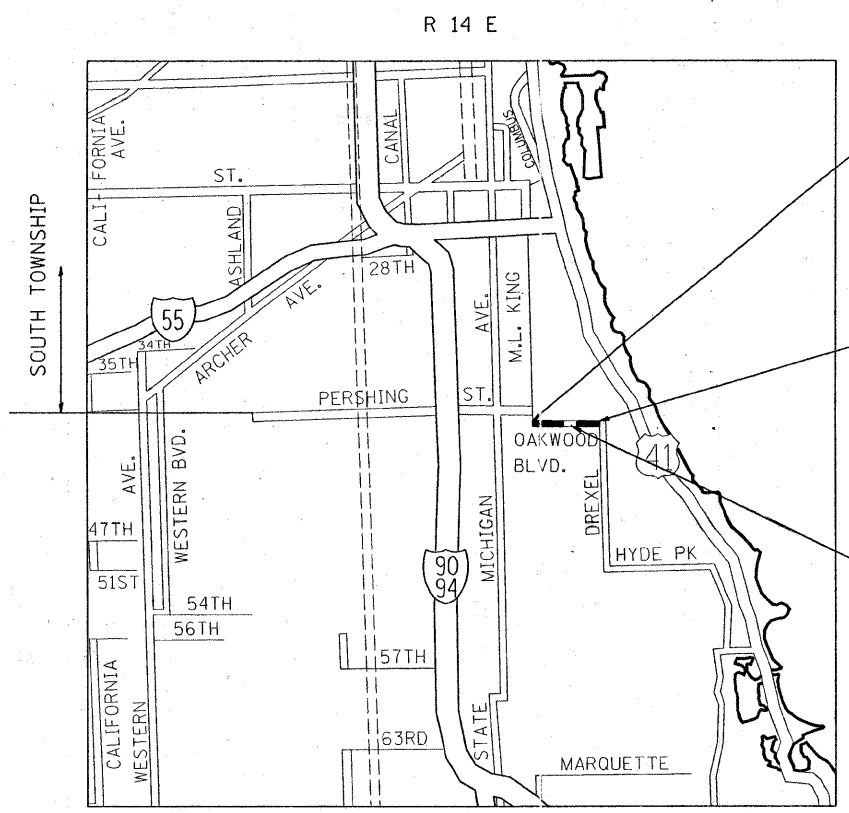
D-91-223-04



THIS IMPROVEMENT IS LOCATED IN THE CITY OF CHICAGO.



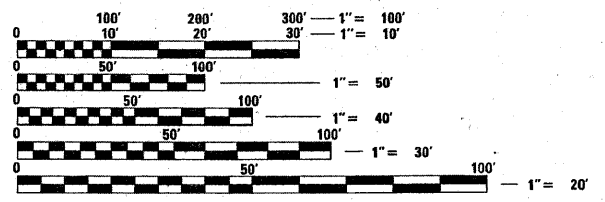
T 38 N



OAKWOOD BLVD: IMPROVEMENT BEGINS STA. 10+28.

OAKWOOD BLVD: IMPROVEMENT ENDS STA. 37+29

OMISSION STA. 21+36 TO 29+31



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N.
 CHICAGO UTILITY ALERT NETWORK
 1-312-744-7000

PROJECT ENGINEER ROBERT BORO (847) 705-4178
 PROJECT MANAGER KEN ENG

CONTRACT NO. 62761

GROSS LENGTH = 2701 FT. = 0.51 MILE
 NET LENGTH = 1906 FT. = 0.36 MILE

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED APRIL 21, 2009

Diana O'Keefe DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
 May 1, 2009

Charles J. Ingessoll ENGINEER OF DESIGN AND ENVIRONMENT
 May 1, 2009

Christine M. Reed DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "CUAN" AT (312) 744-7000 AND "JULIE" AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

10 FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO.

THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND BITUMINOUS TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.

THE RESIDENT ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD ENGINEER, AT (708) 597-9800 A MINIMUM OF 72 HOURS PRIOR TO PLACEMENT OF FINAL PAVEMENT MARKINGS.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H)

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE PRIOR TO BEGINNING WORK.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER

LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER

IT SHALL BE THE CONTRACTOR 5/32 S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL 7/32 TYPICAL APPLICATIONS 13/32 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) 9/32 SHOWN IN THE PLANS

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

STATE STANDARDS

| STANDARD NO. | DESCRIPTION |
|--------------|---|
| 000001-05 | TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 442201-03 | CLASS C AND D PATCHES |
| 606001-04 | CONCRETE CURB AND COMBINACION CONCRETE CURB AND GUTTER |
| 701411-05 | LANE CLOSURE, MULTILANE AT ENTRANCE OR EXIT RAMP FOR SPEEDS >= 45 MPH |
| 701606-06 | URBAN LANE CLOSURE, MULTILANE 2W WITH MOUNTABLE MEDIAN |
| 701701-06 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 701901-01 | TRAFFIC CONTROL DEVICES |
| 886001-01 | DETECTOR LOOP INSTALLATION |
| 886006-01 | TYPICAL LAYOUT FOR DETECTION LOOPS |

INDEX OF SHEETS

| SHEET NO. | DESCRIPTION |
|-----------|---|
| 1 | TITLE SHEET |
| 2 | INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES. |
| 3 | SUMMARY OF QUANTITIES |
| 4-5 | EXISTING AND PROPOSED TYPICAL SECTIONS |
| 6 | ROADWAY AND PAVEMENT MARKING PLANS |
| 7 | FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8) |
| 8 | CITY OF CHICAGO DETAILS FOR P.C.C. CONCRETE DRIVEWAY, ALLEY AND RETURN SIDEWALK (BD-17) |
| 9 | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22) |
| 10 | CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24) |
| 11 | BUTT JOINT AND HMA TAPER DETAILS (BD-32) |
| 12 | TRAFFIC CONTROL & PROTECTION FOR SIDE ROADS, INTERSECTION AND DRIVEWAYS (TC-10) |
| 13 | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) (TC-11) |
| 14 | DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13) |
| 15 | TRAFFIC CONTROL & PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14) |
| 16 | PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16) |
| 17 | ARTERIAL ROAD INFORMATION SIGN (TC-22) |

GENERAL NOTES

IN CASE OF DAMAGE TO CITY OF CHICAGO SEWERS, PRIVATE AND PUBLIC DRAINS, SEWER STRUCTURES AND/OR BENCH MONUMENTS, THE CONTRACTOR SHALL IMMEDIATELY CONTACT THE DEPARTMENT OF SEWERS AT (312) 747-7892 OR (312) 747-7893

ALL BROKEN, CRACKED, WORN OR OTHERWISE DAMAGED OR BICYCLE UNSAFE FRAMES AND GRATES OR LIDS ON SEWER STRUCTURES SHALL BE REPLACED WITH NEW DEPARTMENT OF SEWERS' STANDARD FRAMES AND GRATES OR LIDS. OLD FRAMES AND GRATES OR LIDS SHALL BE DELIVERED TO THE DEPARTMENT OF SEWERS AT 39TH STREET AND ASHLAND AVENUE.

PERMITS FROM THE DEPARTMENT OF SEWERS ARE REQUIRED FOR ALL UNDERGROUND STORM, SANITARY OR COMBINED SEWER SYSTEM CONSTRUCTION, AND FOR RESURFACING WORK INVOLVING ADJUSTMENT OF SEWER STRUCTURES. THE DEPARTMENT OF SEWERS 5/32 PERMIT MUST BE OBTAINED BY A LICENSED SEWER DRAIN LAYER PRIOR TO START OF CONSTRUCTION

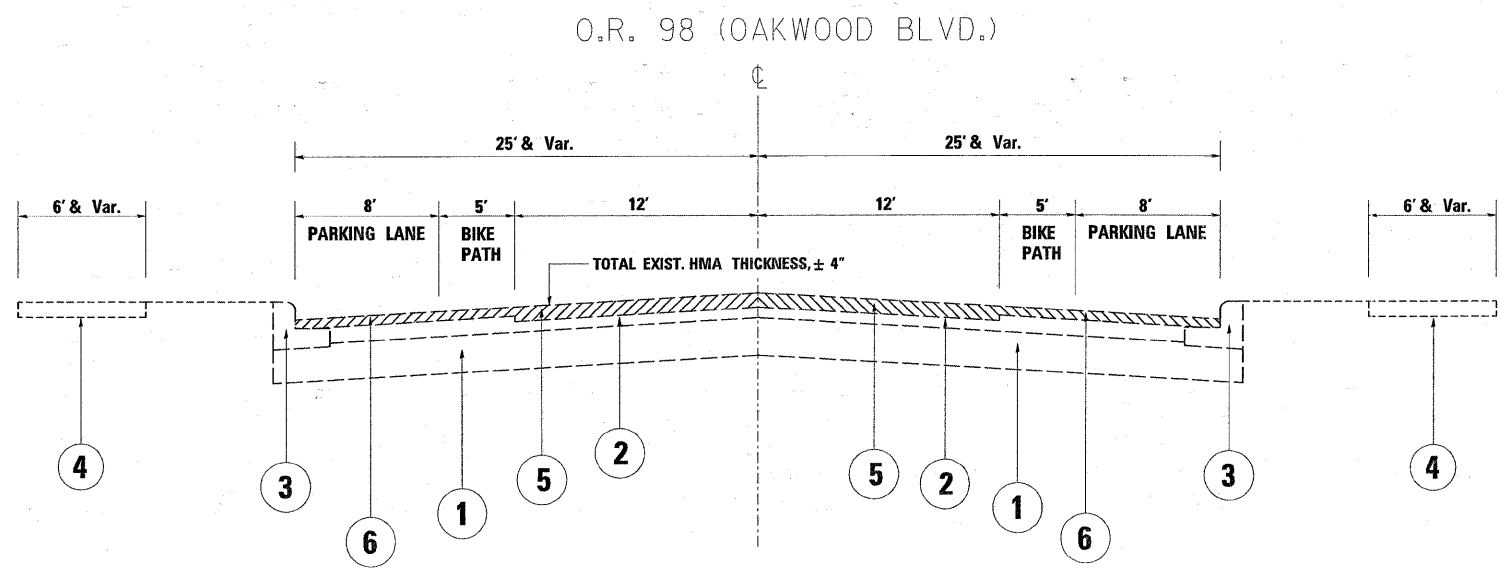
THE LICENSED SEWER CONTRACTOR/SUB-CONTRACTOR MUST SUBMIT TWO (2) SETS OF PLANS APPROVED BY THE DEPARTMENT OF SEWERS FOR THE ISSUE OF THE SEWER PERMIT IN SUITE 410 - 333 SOUTH STATE STREET, CHICAGO, IL 60604. INSPECTION WILL BE PROVIDED BY THE DEPARTMENT OF SEWERS.

PAVEMENT REPLACEMENT AROUND FRAMES AND GRATES OR LIDS WHERE DRAINAGE, WATER MAIN OR ELECTRICAL STRUCTURES ARE ADJUSTED OR RECONSTRUCTED, SHALL BE CLASS SICONCRETE.

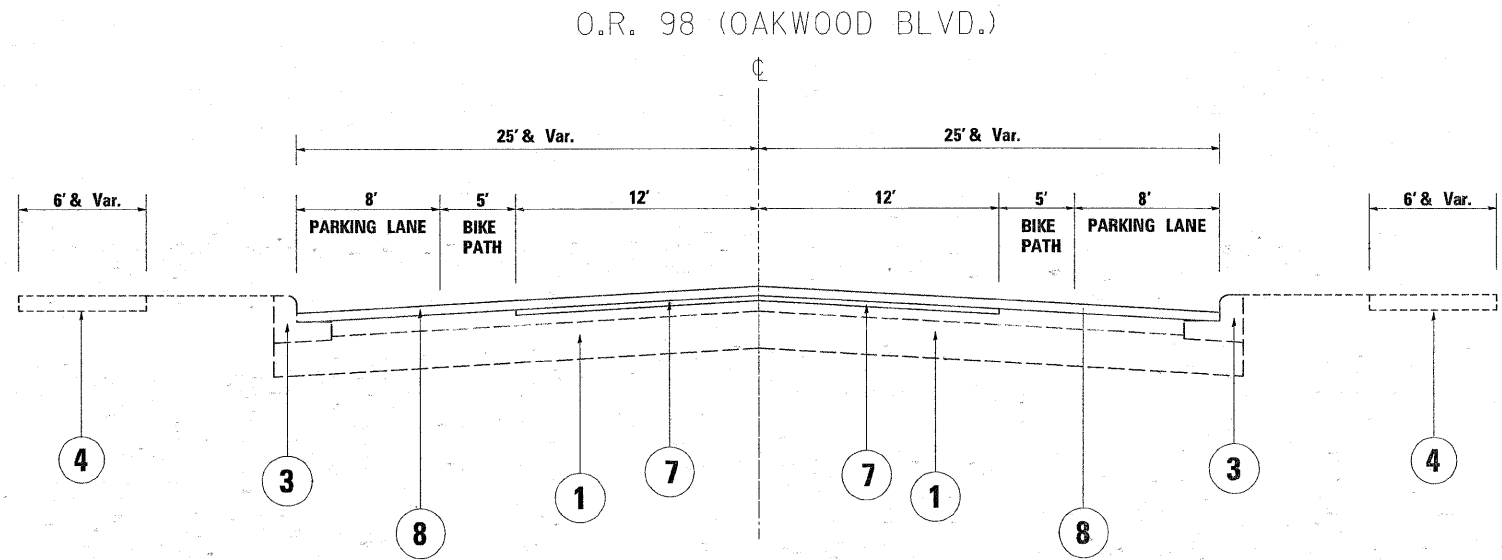
| | | | | | | | | | |
|---|-----------------------------|------------|-----------|---|--------------------|---------------------|--------|--------------|---------------------------|
| FILE NAME = | USER NAME = estimablek | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | O.R. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| es:\pw_work\pwidot\estimablek\d0137176\DI | 2384-sht-plan.dgn | DRAWN - | REVISED - | | 98 | 2004-045 RS | COOK | 17 | 2 |
| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - | | CONTRACT NO. 62761 | | | | |
| | PLOT DATE = 4/22/2009 | DATE - | REVISED - | | SCALE: | SHEET NO. OF SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT |

| SUMMARY OF QUANTITIES | | | URBAN 100% STATE | | | | | CONSTRUCTION TYPE CODE: 1000-2A | | | | | SUMMARY OF QUANTITIES | | | URBAN 100% STATE | | | | | CONSTRUCTION TYPE CODE: 1000-2A | | | | | | | | | | | | | | | |
|-----------------------|--|--------|---------------------|-------|--|--|--|---------------------------------|--|--|--|------------|--|-------|------------------|---------------------|--|--|--|--|---------------------------------|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | | | | | | | | | CODE NO | ITEM | UNIT | TOTAL QUANTITIES | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 40600200 | BITUMINOUS MATERIALS (PRIME COAT) | TON | 10 | 10 | | | | | | | | 70300210 | TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS | SO FT | 155 | 155 | | | | | | | | | | | | | | | | | | | | |
| 40600300 | AGGREGATE (PRIME COAT) | TON | 47 | 47 | | | | | | | | 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 4754 | 4754 | | | | | | | | | | | | | | | | | | | | |
| 40600400 | MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS | TON | 4 | 4 | | | | | | | | 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6" | FOOT | 4158 | 4158 | | | | | | | | | | | | | | | | | | | | |
| 40600895 | CONSTRUCTING TEST STRIP | EACH | 1 | 1 | | | | | | | | 70300250 | TEMPORARY PAVEMENT MARKING - LINE 8" | FOOT | 336 | 336 | | | | | | | | | | | | | | | | | | | | |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SO YD | 234 | 234 | | | | | | | | 70300260 | TEMPORARY PAVEMENT MARKING - LINE 12" | FOOT | 110 | 110 | | | | | | | | | | | | | | | | | | | | |
| 40601005 | HOT-MIX ASPHALT REPLACEMENT OVER PATCHES | TON | 178.4 | 178.4 | | | | | | | | 70300280 | TEMPORARY PAVEMENT MARKING - LINE 24" | FOOT | 197 | 197 | | | | | | | | | | | | | | | | | | | | |
| 40603340 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | TON | 987 | 987 | | | | | | | | * 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SO FT | 155 | 155 | | | | | | | | | | | | | | | | | | | | |
| 42001300 | PROTECTIVE COAT | SO YD | 128 | 128 | | | | | | | | * 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 4114 | 4114 | | | | | | | | | | | | | | | | | | | | |
| 42400200 | PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH | SO FT | 100 | 100 | | | | | | | | * 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 4158 | 4158 | | | | | | | | | | | | | | | | | | | | |
| 44000155 | HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" | SO YD | 2432 | 2432 | | | | | | | | * 78000500 | THERMOPLASTIC PAVEMENT MARKING - LINE 8" | FOOT | 336 | 336 | | | | | | | | | | | | | | | | | | | | |
| 44000158 | HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4" | SO YD | 9314 | 9314 | | | | | | | | * 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 110 | 110 | | | | | | | | | | | | | | | | | | | | |
| 44000600 | SIDEWALK REMOVAL | SO FT | 100 | 100 | | | | | | | | * 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 165 | 165 | | | | | | | | | | | | | | | | | | | | |
| 44001700 | COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT | FOOT | 350 | 350 | | | | | | | | X0322256 | TEMPORARY INFORMATION SIGNING | SO FT | 102.8 | 102.8 | | | | | | | | | | | | | | | | | | | | |
| 44002216 | HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4" | SO YD | 725 | 725 | | | | | | | | X0322748 | BICYCLE LANE MARKING | EACH | 15 | 15 | | | | | | | | | | | | | | | | | | | | |
| 44201753 | CLASS D PATCHES, TYPE II, 9 INCH | SO YD | 230 | 230 | | | | | | | | X4067107 | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | TON | 367 | 367 | | | | | | | | | | | | | | | | | | | | |
| 44201757 | CLASS D PATCHES, TYPE III, 9 INCH | SO YD | 120 | 120 | | | | | | | | Z0018500 | DRAINAGE STRUCTURES TO BE CLEANED | EACH | 11 | 11 | | | | | | | | | | | | | | | | | | | | |
| 44201759 | CLASS D PATCHES, TYPE IV, 9 INCH | SO YD | 375 | 375 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 55039700 | STORM SEWERS TO BE CLEANED | FOOT | 200 | 200 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 60300105 | FRAMES AND GRATES TO BE ADJUSTED | EACH | 8 | 8 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 60300310 | FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) | EACH | 44 | 44 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 60404940 | FRAMES AND GRATES, TYPE 23 | EACH | 1 | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 60406520 | FRAMES AND LIDS, OPEN LID (CITY OF CHICAGO) | EACH | 6 | 6 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 60406530 | FRAMES AND LIDS, CLOSED LID (CITY OF CHICAGO) | EACH | 12 | 12 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 3 | 3 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 70100450 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701201 | L SUM | 1 | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 70100460 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306 | L SUM | 1 | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 70102620 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701501 | L SUM | 1 | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 70300100 | SHORT-TERM PAVEMENT MARKING | FOOT | 1070 | 1070 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

* Specialty Items



EXIST. TYPICAL SECTION
O.R. 98
STA. 10+28 TO 21+36



PROPOSED TYPICAL SECTION
O.R. 98
STA. 10+28 TO 21+36

LEGEND

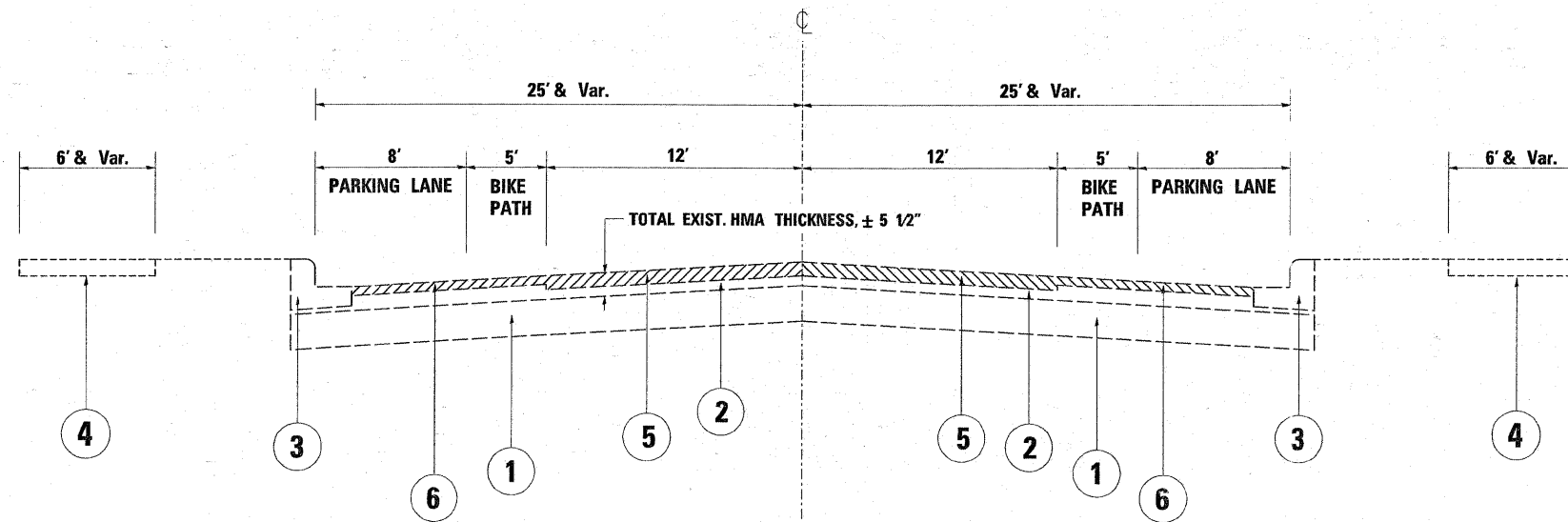
- ① EXISTING AGGREGATE BASE COURSE
- ② EXISTING HOT-MIX ASPHALT AFTER MILLING
- ③ EXISTING COMB. CURB AND GUTTER, TYPE B-6.12
- ④ EXISTING CONCRETE SIDEWALK
- ⑤ PROP. HMA SURFACE REMOVAL, 2 1/4"
- ⑥ PROP. HMA SURFACE REMOVAL BIKE PATH & PARKING LANE, 1 1/2"
- ⑦ PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑧ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

| MIXTURE REQUIREMENTS | | |
|--|----------------------|---------------|
| MIXTURE USES | AC TYPE | VOIDS |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) | PG 64-22 | 4% AT 70 GYR. |
| POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | SBS/SBR PG 76-28/-22 | 4% AT 50 GYR. |
| CLASS D PATCHES (HMA BINDER IL-19 mm) | PG 64-22 * | 4% AT 70 GYR. |

NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE IS 112 LBS/SQ YD/IN
* NOTE 2: WHEN RAP EXCEEDS 20% THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

NOTE:
THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING

O.R. 98 (OAKWOOD BLVD.)



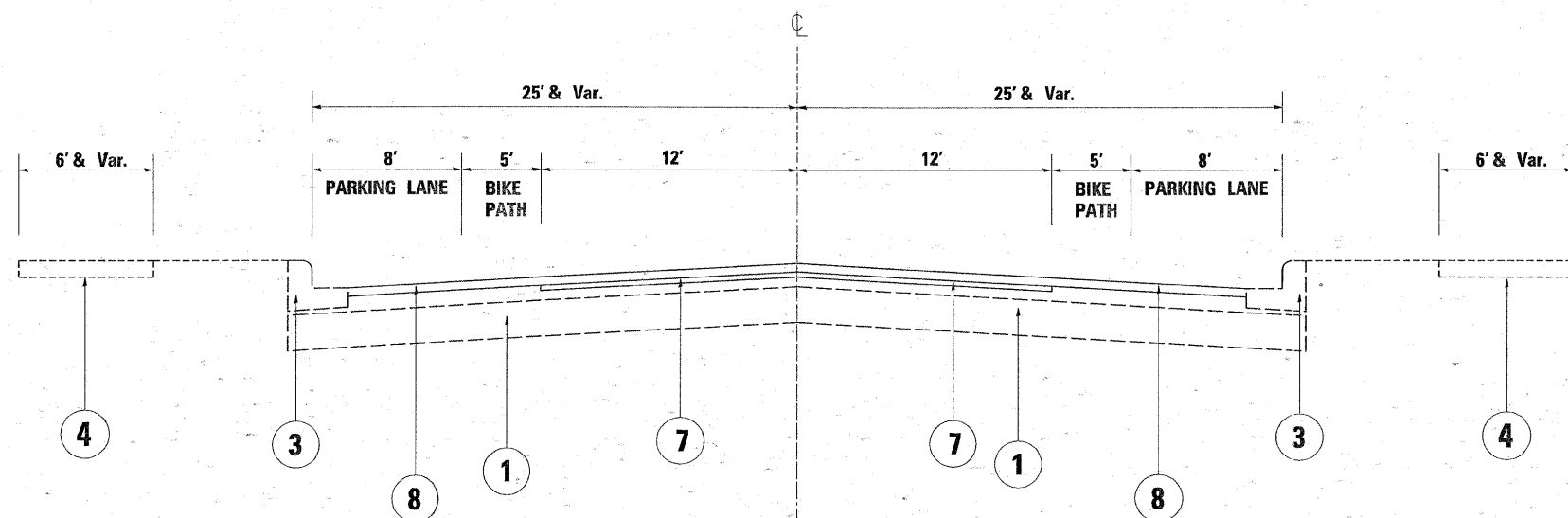
EXIST. TYPICAL SECTION

O.R. 98
STA. 29+31 TO 37+29

LEGEND

- ① EXISTING AGGREGATE BASE COURSE
- ② EXISTING HOT-MIX ASPHALT AFTER MILLING
- ③ EXISTING COMB. CURB AND GUTTER, TYPE B-6.12
- ④ EXISTING CONCRETE SIDEWALK
- ⑤ PROP. HMA SURFACE REMOVAL, 2 1/4"
- ⑥ PROP. HMA SURFACE REMOVAL BIKE PATH & PARKING LANE, 1 1/2"
- ⑦ PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑧ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

O.R. 98 (OAKWOOD BLVD.)



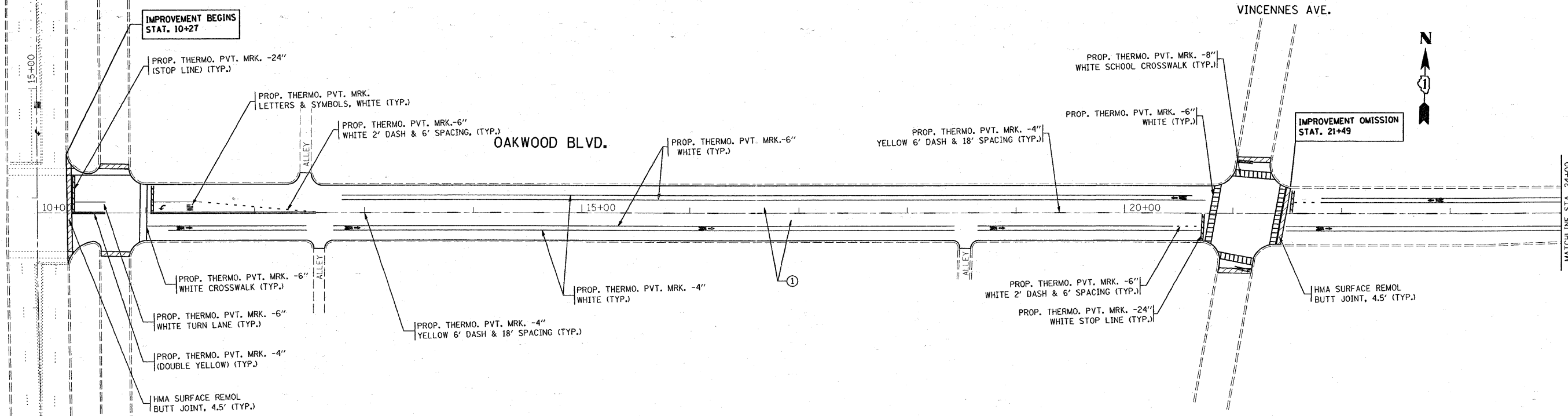
PROPOSED TYPICAL SECTION

O.R. 98
STA. 29+31 TO 37+29

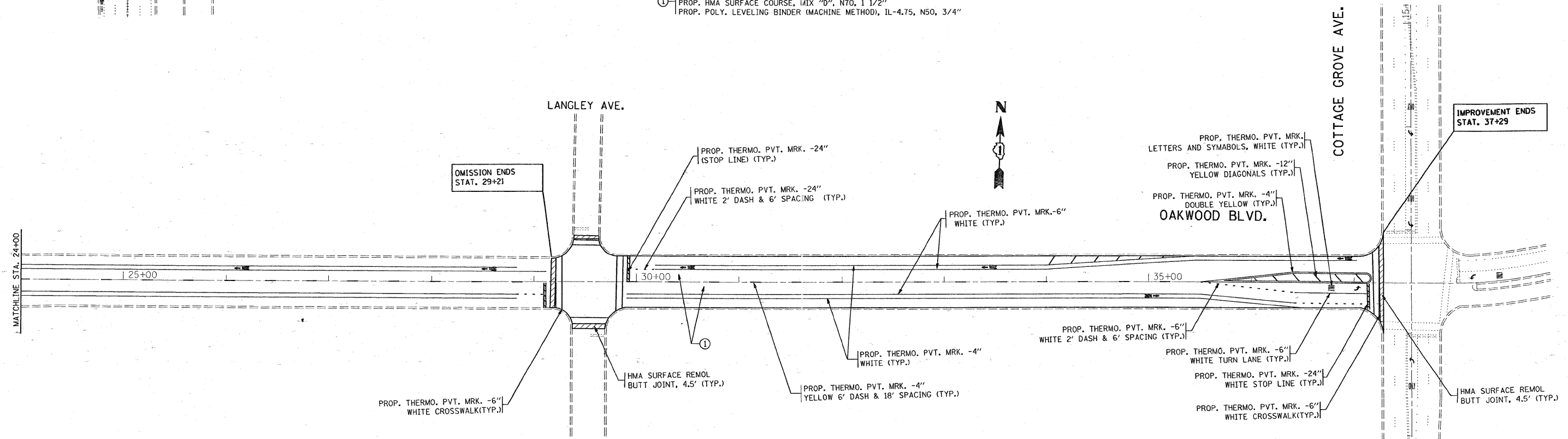
NOTE:
THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING

| | | | | | | | | | |
|---------------------------------------|------------------------------|------------|-----------|---|---------------------------|-------------|--------|--------------|-----------|
| FILE NAME = | USER NAME = estmoblek | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | O.R. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| oz:\pwork\pwork\estmoblek\d0137176\DI | 2304-shr-plan.dgn | DRAWN - | REVISED - | | 98 | 2004-045 RS | COOK | 17 | 5 |
| | PLOT SCALE = 50,0000 ' / IN. | CHECKED - | REVISED - | | CONTRACT NO. 62761 | | | | |
| | PLOT DATE = 4/22/2009 | DATE - | REVISED - | | SCALE: | SHEET NO. | OF | SHEETS | STA. |
| | | | | | ILLINOIS FED. AID PROJECT | | | | |

S. MARTIN LUTHER KING JR. DR.



① PROP. HMA SURFACE REMOVAL, 2 1/4"
 PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"



① PROP. HMA SURFACE REMOVAL, 2 1/4"
 PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

FILE NAME =
 o:\pwork\pmda\estmablek\d0137176\012304-sht-plan.dgn

USER NAME = estmablek
 PLOT SCALE = 50.0000' / IN.
 PLOT DATE = 4/22/2009

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

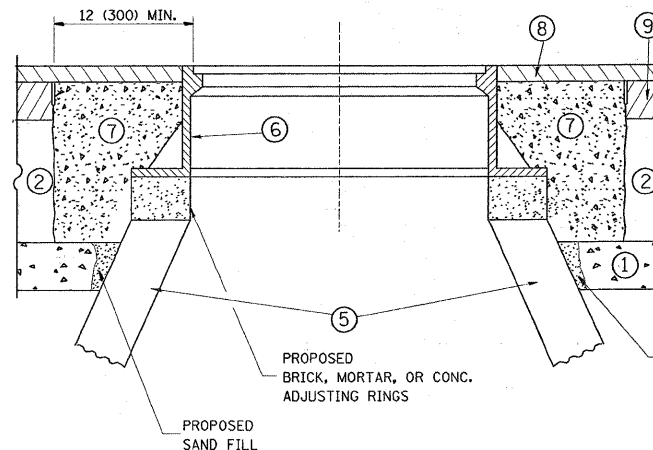
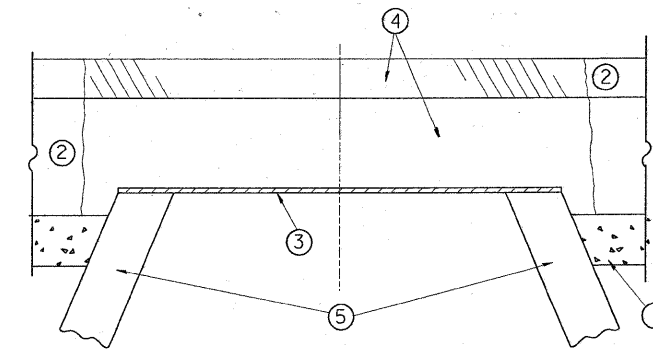
REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLAN
 OAKWOOD BLVD. (MARTIN LUTHER KING JR. DR. - COTTAGE GROVE RD.)

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|--------------------|-------------|--------|---------------------------|-----------|
| O.R. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 98 | 2004-045 RS | COOK | 17 | 6 |
| CONTRACT NO. 62761 | | | ILLINOIS FED. AID PROJECT | |



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

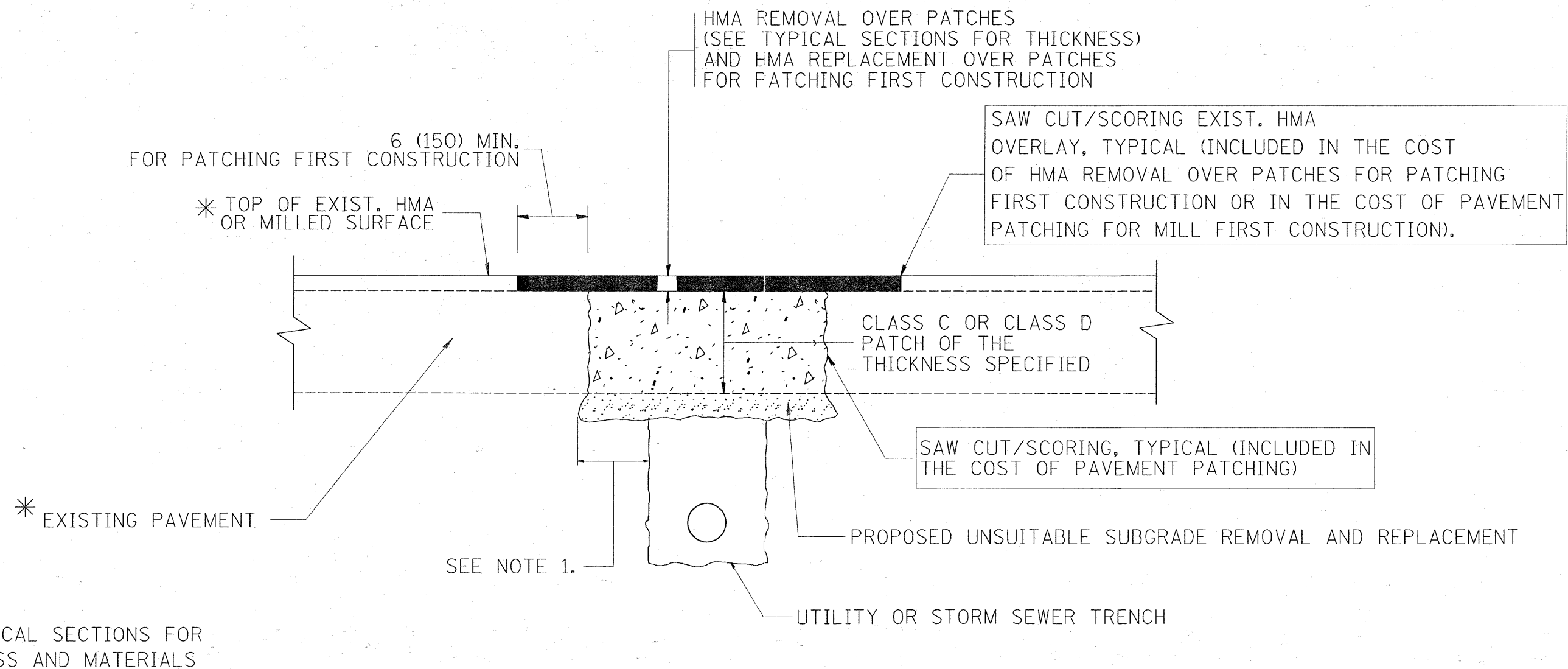
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

| | | | | | | | | | | | | |
|--|-----------------------|--------------------|--------------------------------|---|--|-------------------------|--------------|---|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = estmoblek | DESIGNED - R. SHAH | REVISED - R. SHAH 03-10-95 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ca\pwork\pwork\estmoblek\d8137176\d8137176.dgn | | DRAWN - | REVISED - A. ABBAS 03-21-97 | | 98 | 2004-045-RS | COOK | 17 | 7 | | | |
| PLOT SCALE = 50.00000 ' / IN. | | CHECKED - | REVISED - R. WIEDEMAN 05-14-04 | | BD600-03 (BD-8) | | | CONTRACT NO. 62761 | | | | |
| PLOT DATE = 4/22/2009 | | DATE - 10-25-94 | REVISED - R. BORO 01-01-07 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | | | |
|---|------------------------|----------------------------|-----------------------------|---|--|-------------------------|------|---|---------|--------------------|--------------|-----------|---|
| FILE NAME = | USER NAME = estumablek | DESIGNED - R. SHAH | REVISED - A. ABBAS 04-27-98 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| cr:\pw_work\pwidot\estumablek\d8137176\Drawings | | DRAWN - | REVISED - R. BORO 01-01-07 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | 98 | 2004-045-RS | COOK | 17 | 9 |
| | | PLOT SCALE = 50.0000' / 1" | REVISED - R. BORO 09-04-07 | | | | | BD400-04 (BD-22) | | CONTRACT NO. 62761 | | | |
| | | PLOT DATE = 4/22/2009 | REVISED - K. ENG 10-27-08 | | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | |

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SALT TOLERANT SOD AND TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

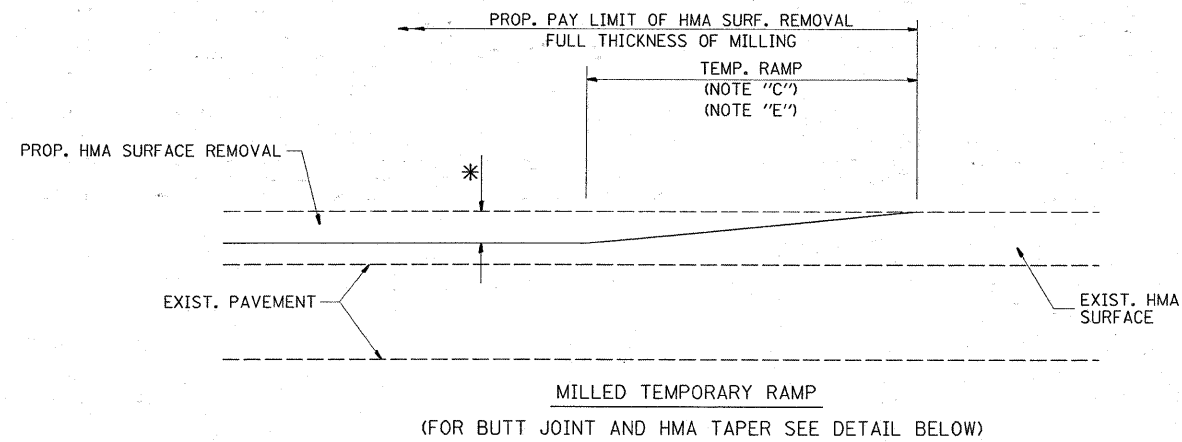
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

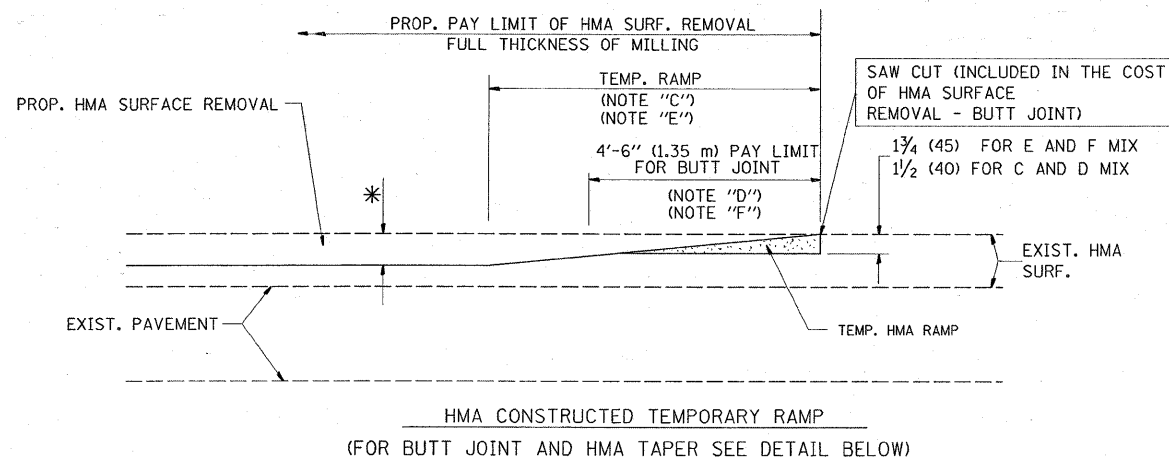
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | | |
|--|--------------------------------|----------------------|-----------------------------|---|--|-------------------------|--------------|--|---------------------|-------------|-----------------|--------------|
| FILE NAME = | USER NAME = estmablek | DESIGNED - A. HOUSEH | REVISED - R. SHAH 10-03-96 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT | | | F.A. RTE. 98 | SECTION 2004-045-RS | COUNTY COOK | TOTAL SHEETS 17 | SHEET NO. 10 |
| c:\pwwork\pwwork\estmablek\d0137176\d0137176.dgn | PLOT SCALE = 50.0000' / 1" IN. | DRAWN - | REVISED - A. ABBAS 03-21-97 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | BD600-06 (BD-24) CONTRACT NO. 62761 | | | | |
| PLOT DATE = 4/22/2009 | DATE - 03-11-94 | CHECKED - | REVISED - M. GOMEZ 01-22-01 | | | | | | | | | |
| | | DATE - 03-11-94 | REVISED - R. BORO 01-01-07 | | | | | | | | | |

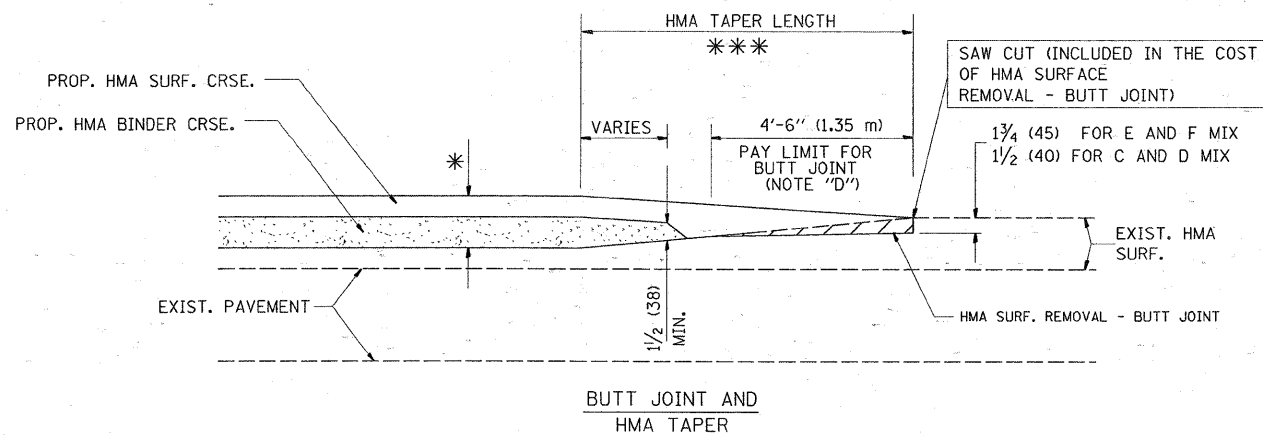


OPTION 1

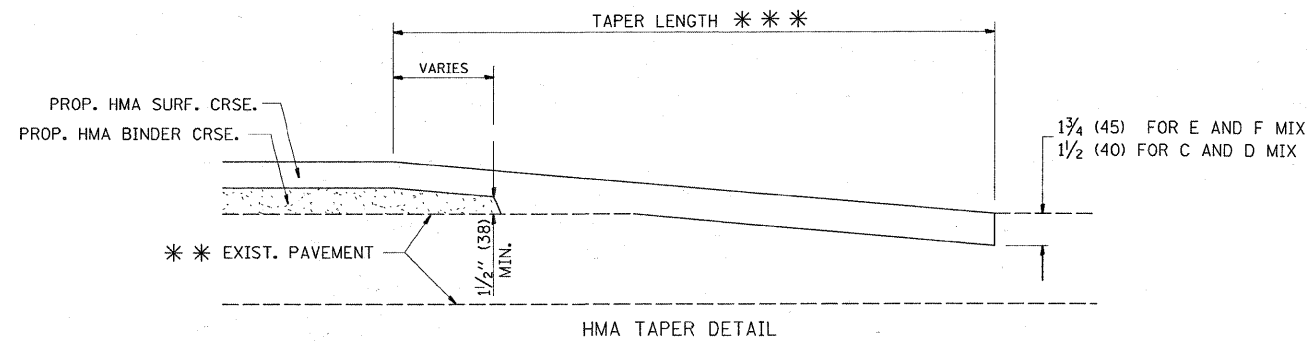
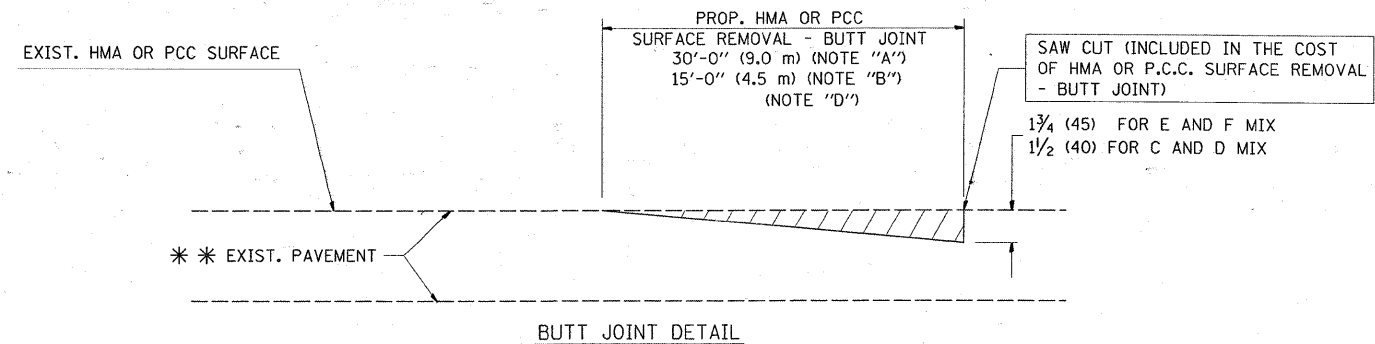


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

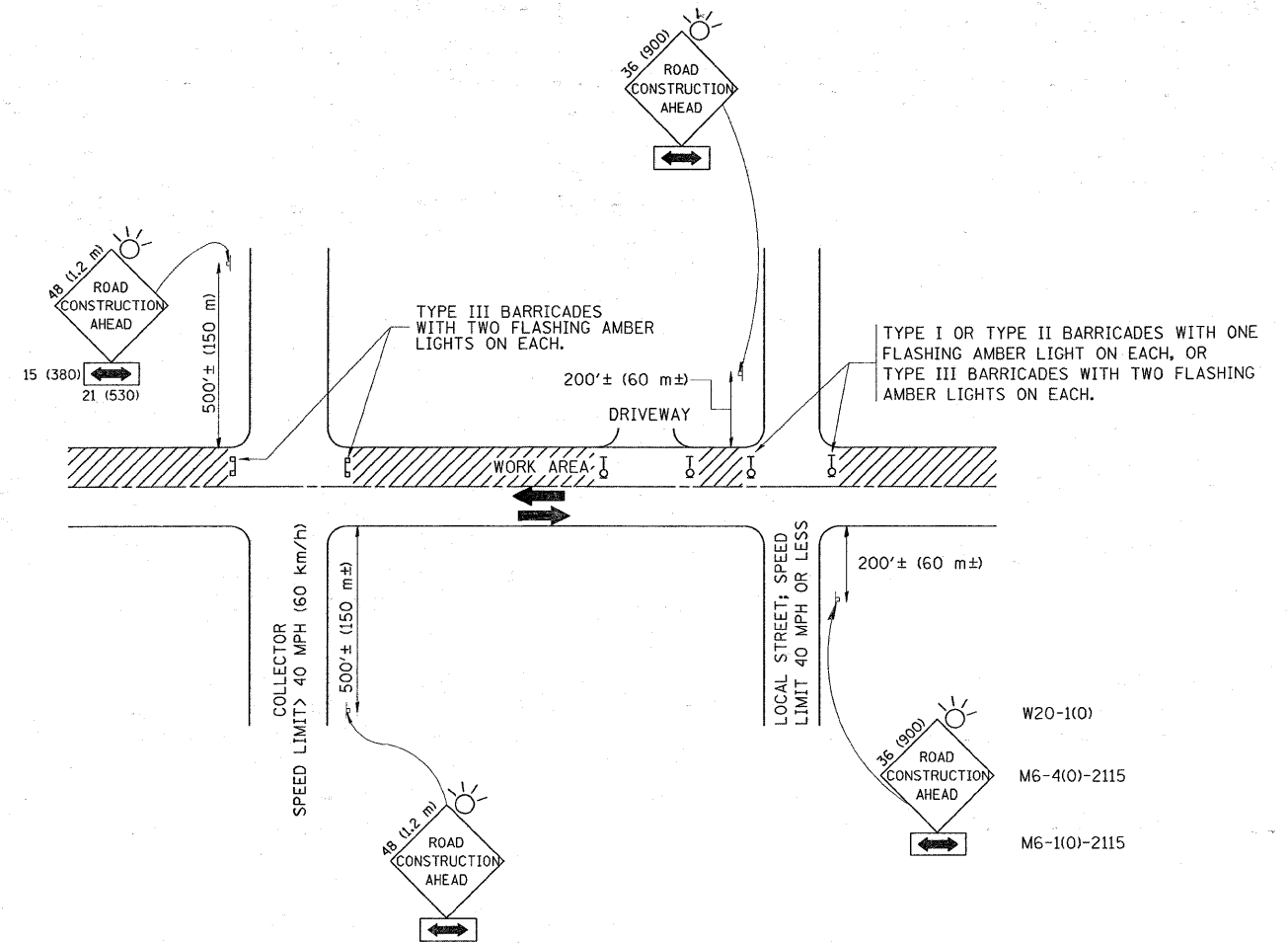
| | | | |
|--|--------------------------------|-----------------------|-----------------------------|
| FILE NAME = | USER NAME = estunoblek | DESIGNED - M. DE YONG | REVISED - R. SHAH 10-25-94 |
| es:\pwwork\pwwork\estunoblek\0137176\0137176.dgn | | DRAWN - | REVISED - A. ABBAS 03-21-97 |
| | PLOT SCALE = 50.0000' / 1" IN. | CHECKED - | REVISED - M. GOMEZ 04-06-01 |
| | PLOT DATE = 4/22/2009 | DATE - 06-13-90 | REVISED - R. BORO 01-01-07 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---|-------------|--------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 98 | 2004-045-RS | COOK | 17 | 11 |
| BD400-05 BD32 | | CONTRACT NO. 62761 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

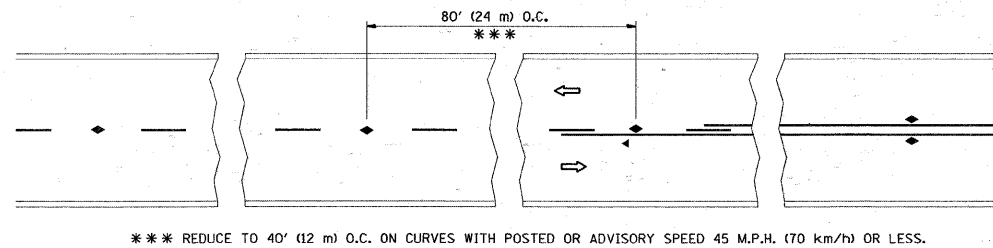
| | | | |
|---|------------------------------|----------------|---------------------------------|
| FILE NAME = | USER NAME = estmoblek | DESIGNED - LHA | REVISED - J. OBERLE 10-18-95 |
| c:\pw\work\pwidat\estmoblek\d0137176\dt | t5td.dgn | DRAWN - | REVISED - A. HOUSEH 03-06-96 |
| | PLOT SCALE = 50.0000 ' / IN. | CHECKED - | REVISED - A. HOUSEH 10-15-96 |
| | PLOT DATE = 4/22/2009 | DATE - 06-89 | REVISED - T. RAMMACHER 01-06-00 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

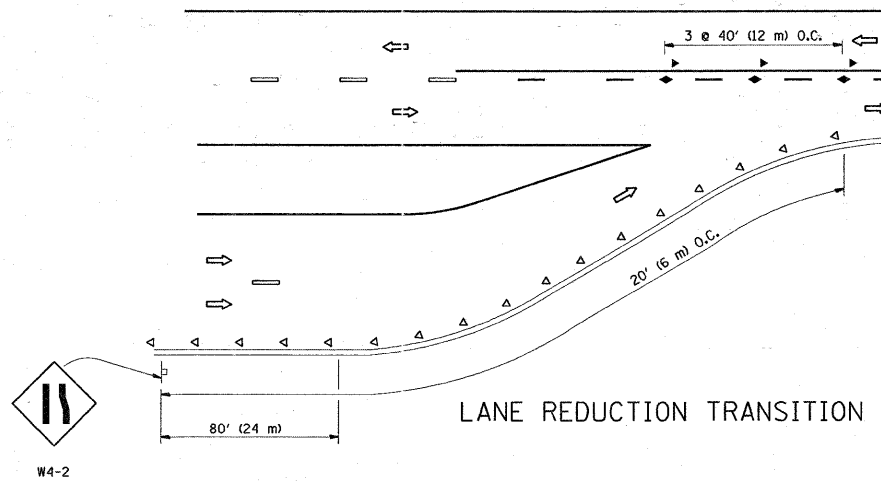
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

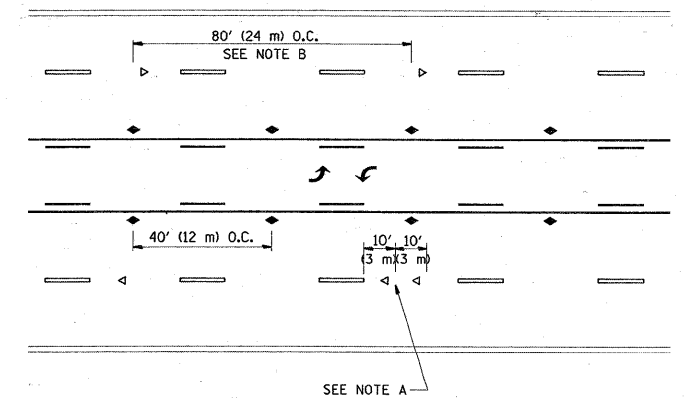
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|-------------|--------|--------------------|-----------|
| 98 | 2004-045-RS | COOK | 17 | 12 |
| TC-10 | | | CONTRACT NO. 62761 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



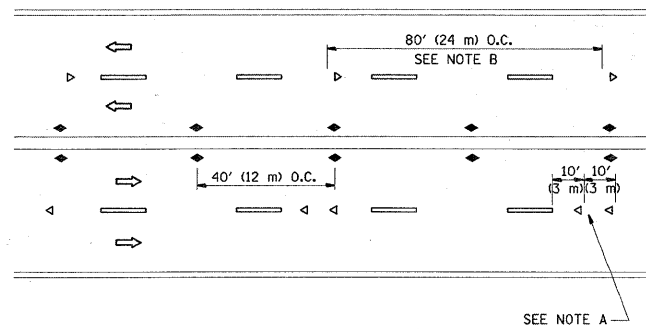
TWO-LANE/TWO-WAY



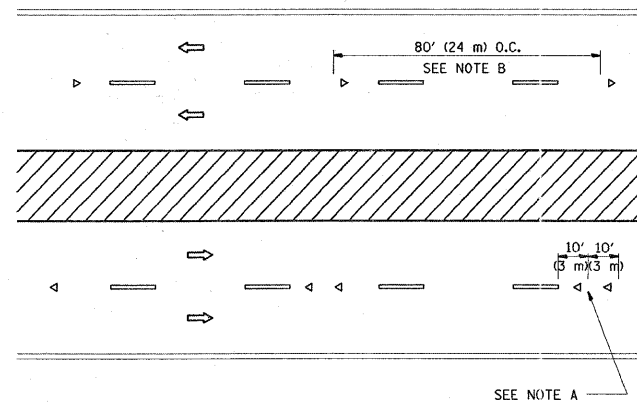
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

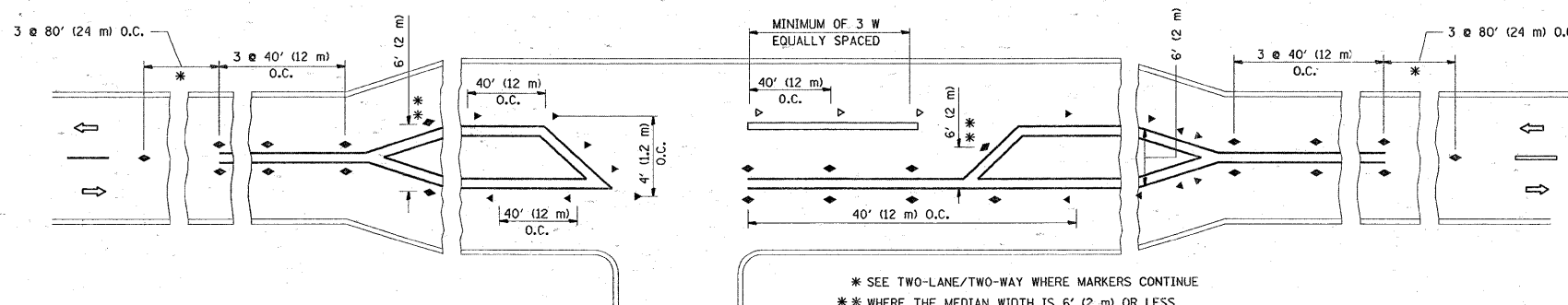
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



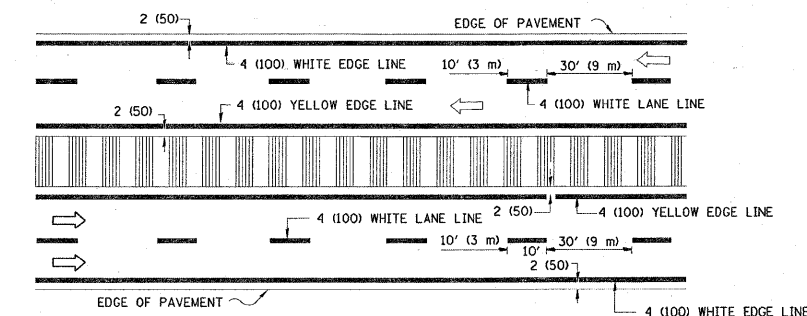
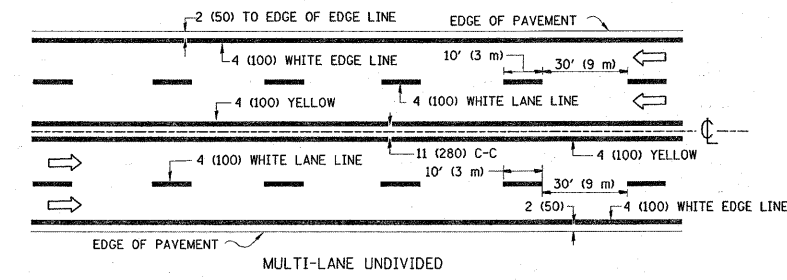
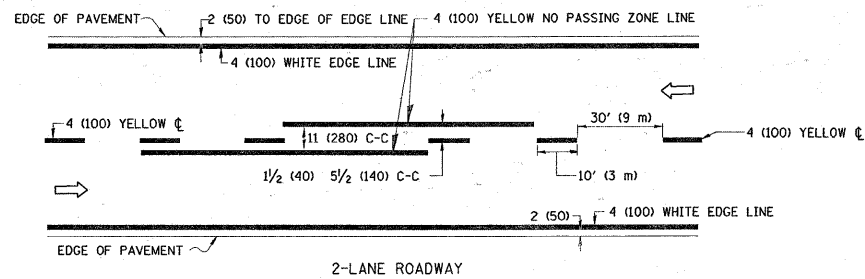
LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

| | | | |
|---|----------------------------|------------|---------------------------------|
| FILE NAME = | USER NAME = estumoblek | DESIGNED - | REVISED - T. RAMMACHER 09-19-94 |
| \\p\work\pwidot\estumoblek\d0137176\1\std.dgn | | DRAWN - | REVISED - T. RAMMACHER 03-12-99 |
| | | CHECKED - | REVISED - T. RAMMACHER 01-06-00 |
| | | DATE - | REVISED - |
| | PLOT SCALE = 50.0000 / IN. | | |
| | PLOT DATE = 4/22/2009 | | |

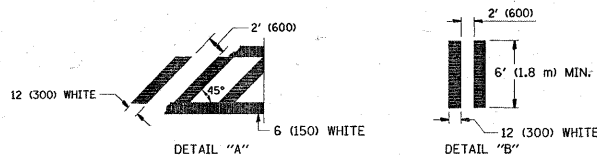
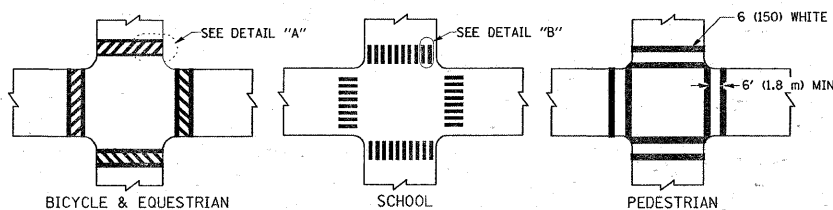
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | | | |
|--|--|--------------|-------------|---|--------------|-----------|
| TYPICAL APPLICATIONS | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) | | 98 | 2004-045-RS | COOK | 17 | 13 |
| SCALE: NONE | | TC-11 | | CONTRACT NO. 62761 | | |
| SHEET NO. 1 OF 1 SHEETS | | STA. TO STA. | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | |

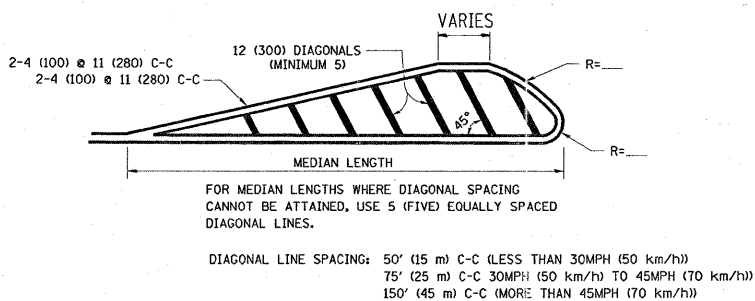
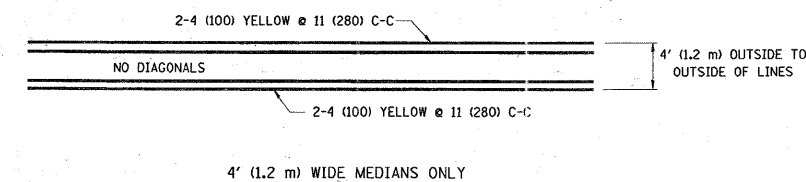


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

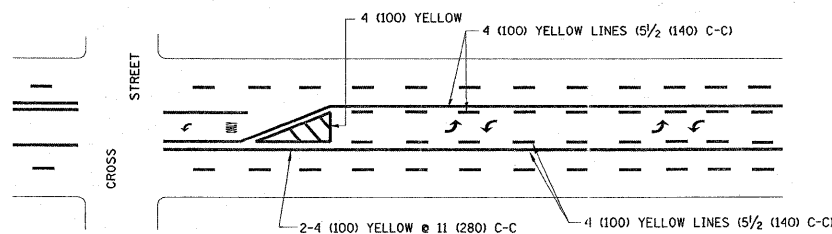
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

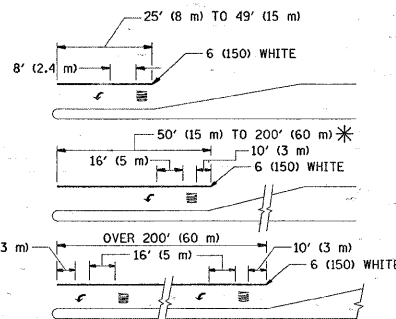


MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

TYPICAL PAINTED MEDIAN MARKING

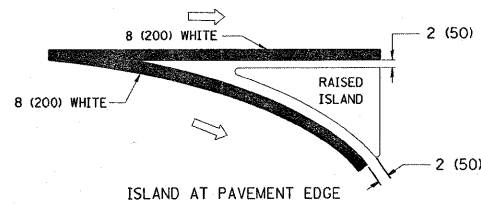
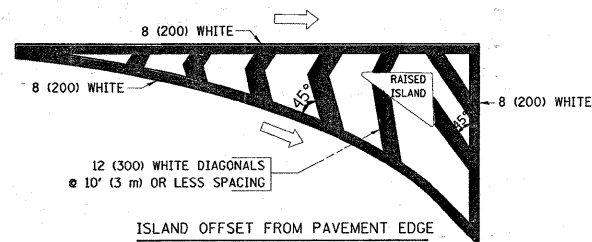


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|--|---------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS | 4 (100) 2 @ 4 (100) | SOLID SOLID | YELLOW YELLOW | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²) |
| SHOULDER DIAGONALS | 12 (300) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

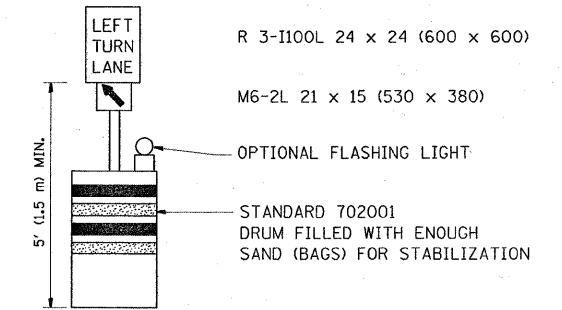
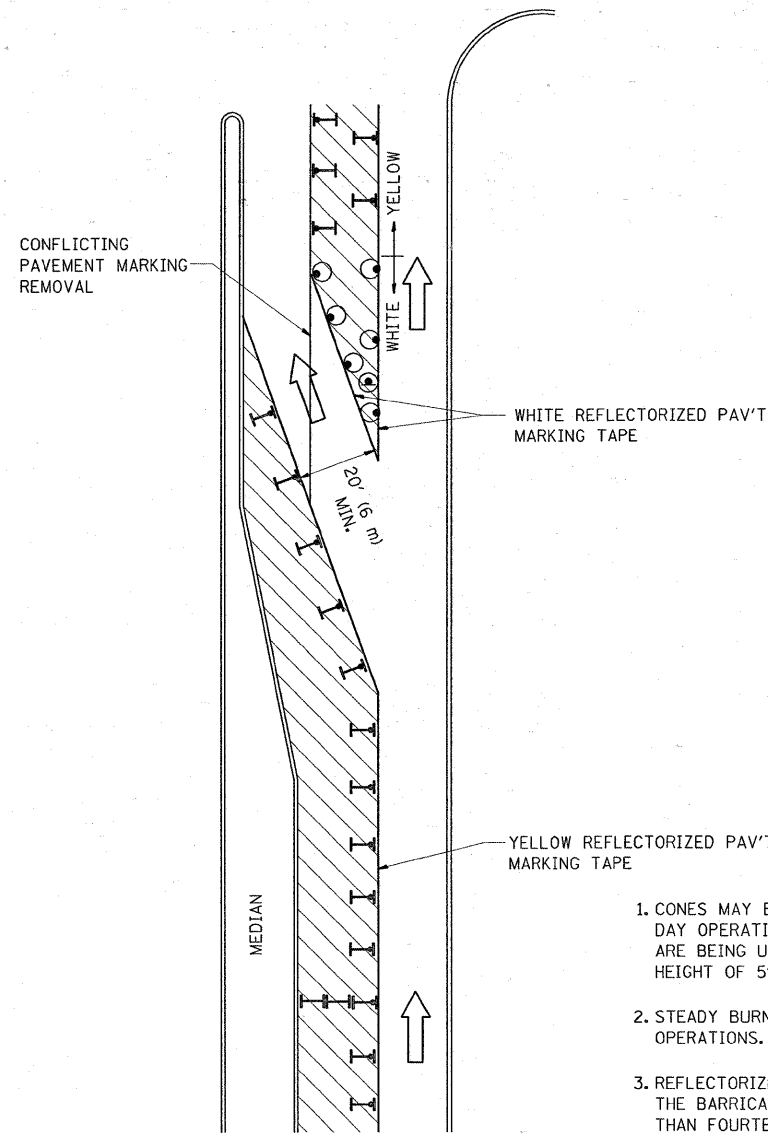
| | | | |
|---------------------------------------|-----------------------------|------------------|---------------------------------|
| FILE NAME = | USER NAME = estmblek | DESIGNED - EVERS | REVISED - T. RAMMACHER 10-27-94 |
| ci:\p\work\p\dot\estmblek\d0137176\01 | estmblek | DRAWN - | REVISED - A. HOUSEH 10-09-96 |
| | PLOT SCALE = 50,0000' / IN. | CHECKED - | REVISED - A. HOUSEH 10-17-96 |
| | PLOT DATE = 4/22/2009 | DATE - 03-19-90 | REVISED - T. RAMMACHER 01-06-00 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---|-------------|--------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 98 | 2004-045-RS | COOK | 17 | 14 |
| TC-13 | | CONTRACT NO. 62761 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

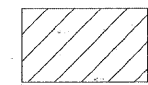







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

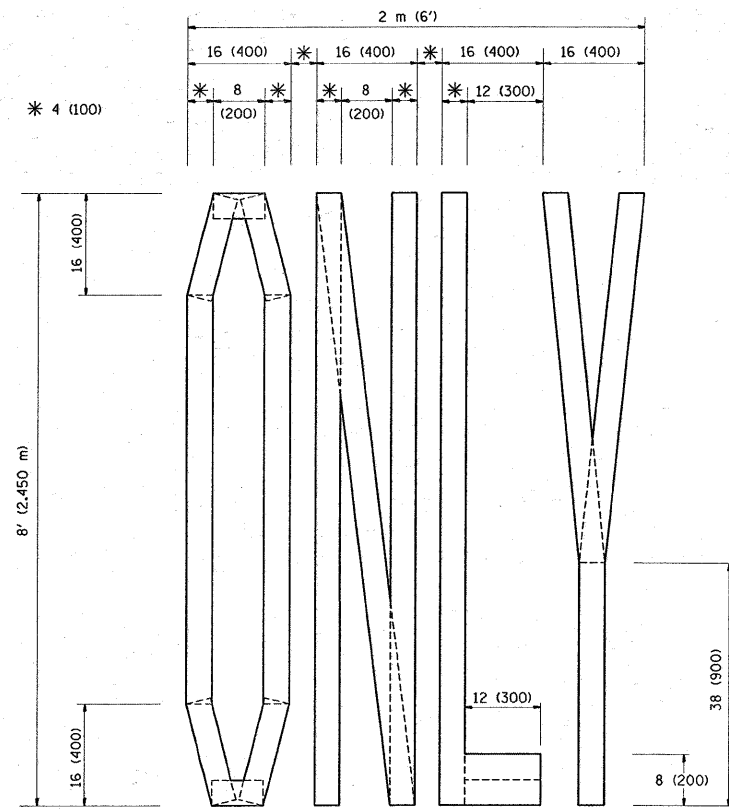
| | | | |
|---|-----------------------|------------|---------------------------------|
| FILE NAME = | USER NAME = estmablek | DESIGNED - | REVISED - T. RAMMACHER 09-08-94 |
| cs:\pw_work\pwi\dot\estmablek\d0137176\DotStd.dgn | | DRAWN - | REVISED - A. HOUSEH 11-07-95 |
| | | CHECKED - | REVISED - A. HOUSEH 10-12-96 |
| | | DATE - | REVISED - T. RAMMACHER 01-06-00 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

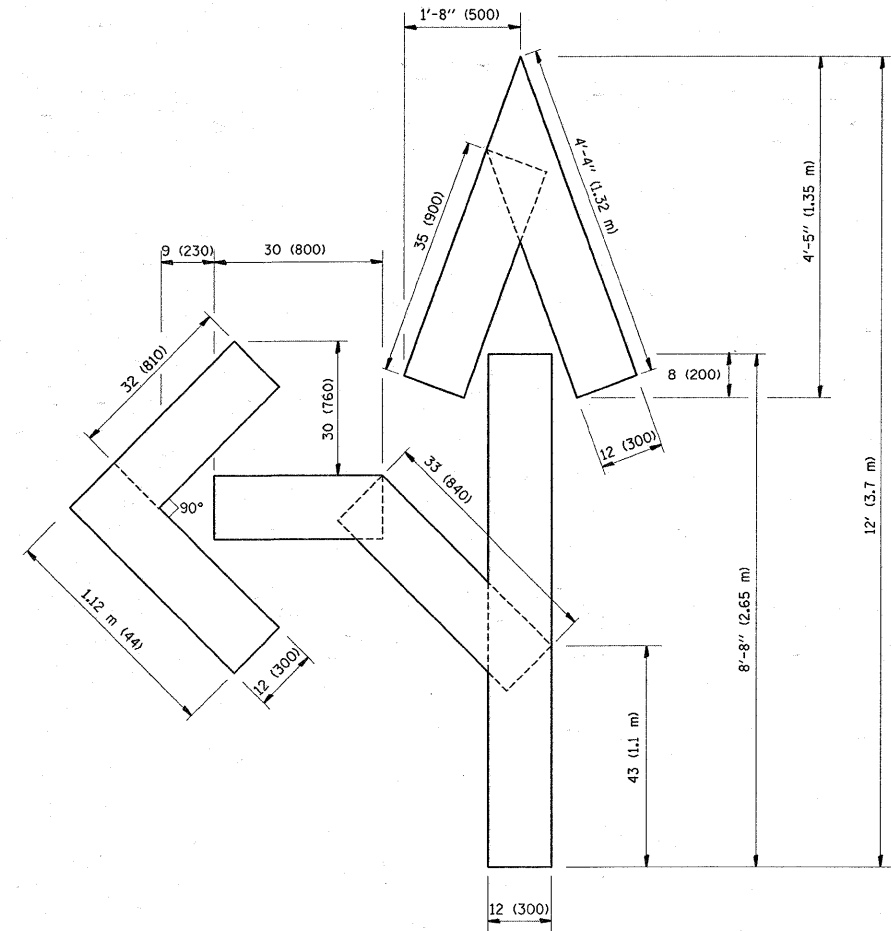
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

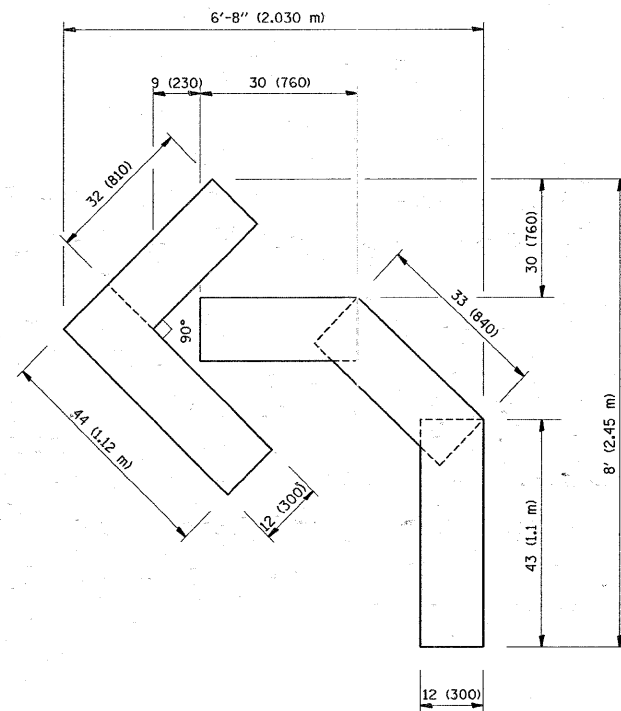
| | | | | |
|---|-------------|--------|--------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 98 | 2004-045-RS | COOK | 17 | 15 |
| TC-14 | | | CONTRACT NO. 62761 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in Inches (millimeters) unless otherwise shown.

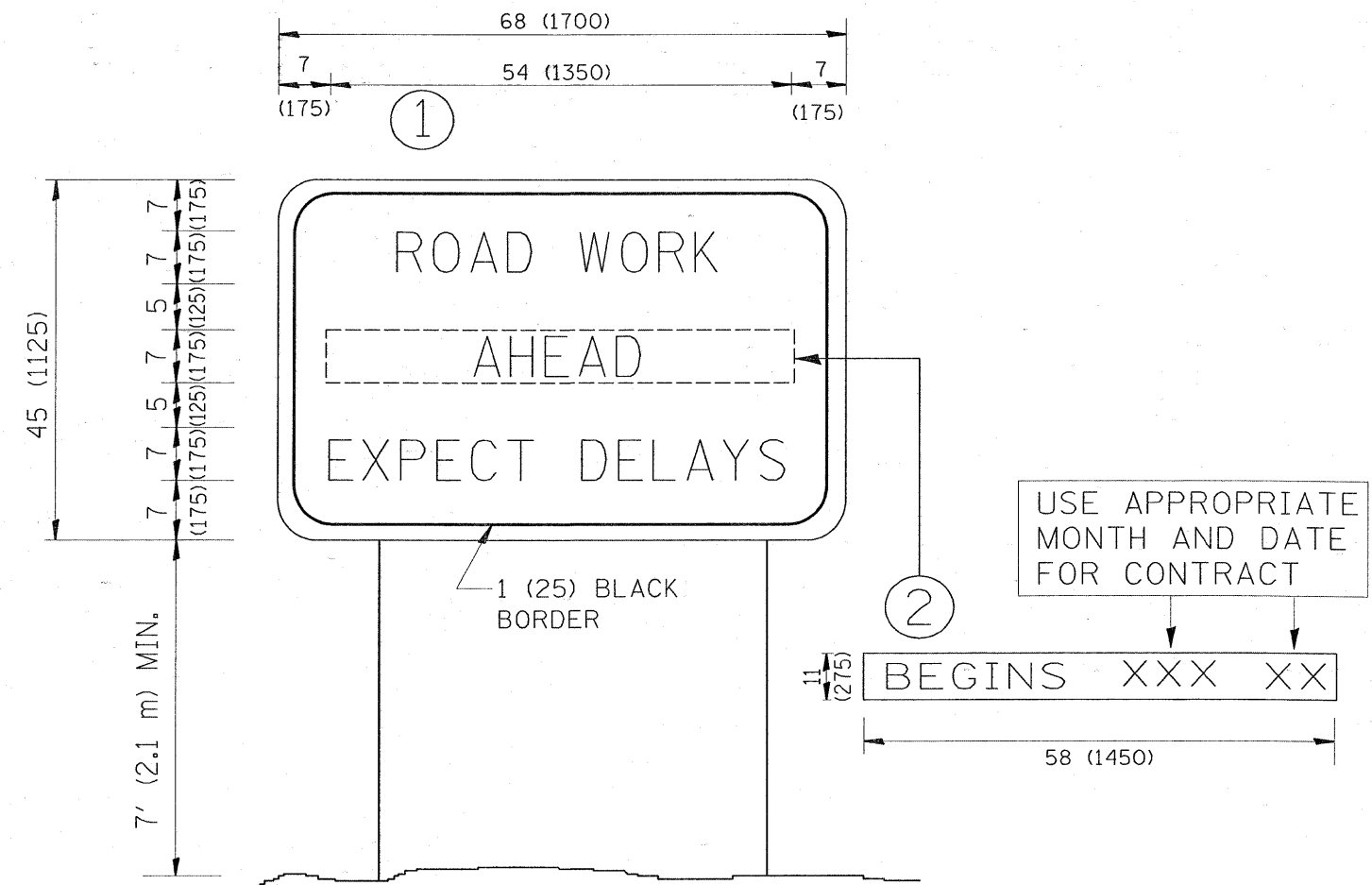
| | | | |
|---|------------------------------|-----------------|--------------------------------|
| FILE NAME = | USER NAME = estumablek | DESIGNED - | REVISED -T. RAMMACHER 06-05-96 |
| ca:\pw\work\p\dot\estumablek\d0137176\dot\std.dgn | | DRAWN - | REVISED -T. RAMMACHER 11-04-97 |
| | PLOT SCALE = 50.0000 ' / IN. | CHECKED - | REVISED -T. RAMMACHER 03-02-98 |
| | PLOT DATE = 4/22/2009 | DATE - 09-18-94 | REVISED -E. GOMEZ 08-28-00 |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---|-------------|--------|--------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 98 | 2004-045-RS | COOK | 17 | 16 |
| TC-16 | | | CONTRACT NO. 62761 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | | |
|---|-----------------------------|------------|---------------------------------|---|---|-------------|-------------|---------------------------|-------------|---|--------------|-----------|
| FILE NAME = | USER NAME = estunoblek | DESIGNED - | REVISED - R. MIRS 09-15-97 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | ARTERIAL ROAD INFORMATION SIGN | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| estunoblek\work\p\dot\estunoblek\d0137176\dot | estunoblek | DRAWN - | REVISED - R. MIRS 12-11-97 | | | | | 98 | 2004-045-RS | COOK | 17 | 17 |
| | PLOT SCALE = 50,0000' / IN. | CHECKED - | REVISED - T. RAMMACHER 02-02-99 | | TC-22 | | | CONTRACT NO. 62761 | | | | |
| | PLOT DATE = 4/22/2009 | DATE - | REVISED - C. JUCIUS 01-31-07 | | SCALE: NONE | SHEET NO. 1 | OF 1 SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | |