

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
316	(10&1-D-2)RS-4	WILL	36	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 62778		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS**

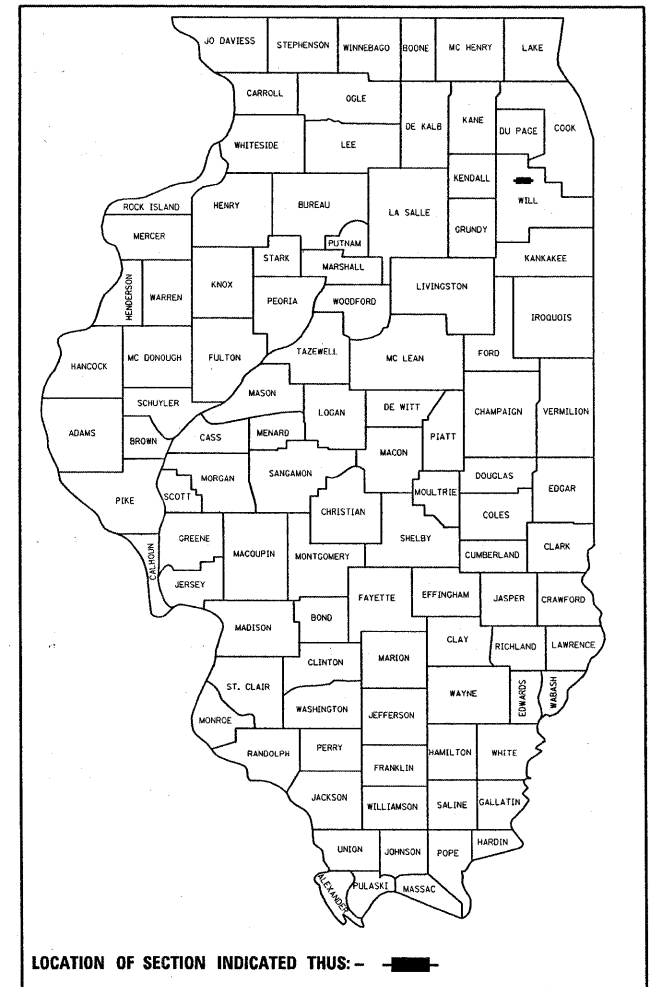
FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED IN
THE CITY OF JOLIET

**PROPOSED
HIGHWAY PLANS**

**FAU ROUTE 316: CENTER ST.
WORTMAN DR. TO PLEASANT ST.
SECTION (10 & 1-D-2) RS-4
RESURFACING (3 P) & BRIDGE JOINT REPAIR**

D-91-245-04



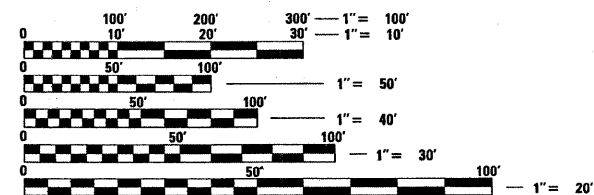
WILL COUNTY

C-91-245-04

1. SN: 099-0186

2. SN: 099-0187

3. SN: 099-0188



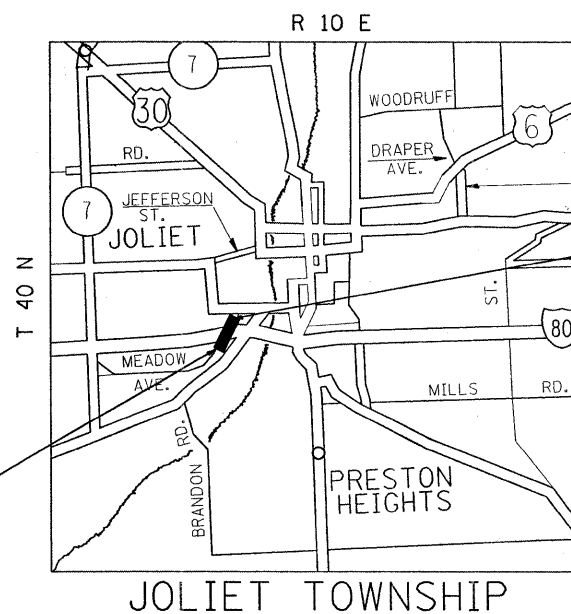
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

OMISSION ON RAMP BA BETWEEN
STA. 64+61 TO 70+22

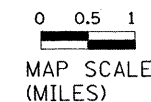
OMISSION ON RAMP DB BETWEEN
STA. 29+31 TO 30+45

PROJECT BEGINS STA. 53+00



PROJECT ENDS STA. 91+30

TRAFFIC DATA
SPEED LIMIT: 35 MPH
2004 ADT: 11,200



PROJECT ENGINEER: JENPAI CHANG (847) 705-4432
PROJECT MANAGER: KEN ENG

CONTRACT NO. 62778

NET LENGTH ALONG RAMP = 11,718 LINEAL FEET = 2.219 MILES
GROSS LENGTH OF PROJECT = 3830 LINEAL FEET = 0.725 MILES
NET LENGTH OF PROJECT = 3269 LINEAL FEET = 0.619 MILES

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS**

SUBMITTED MARCH 20, 2009

Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 1, 2009
Charles G. Ingersoll
ENGINEER OF DESIGN AND ENVIRONMENT

May 1, 2009
Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

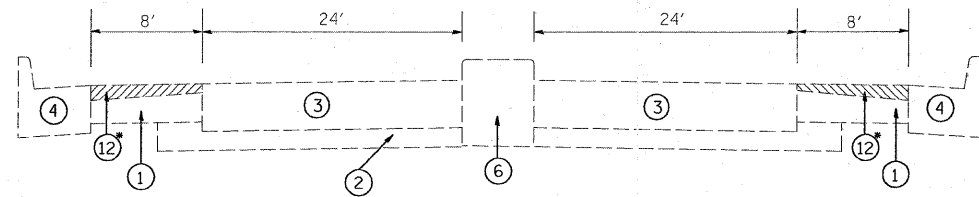
INDEX OF SHEETS

SHEET NO.	DESCRIPTION	STATE STANDARDS	GENERAL NOTES:
1	TITLE SHEET	000001-05 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS	1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OR BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED) 2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF JOLIET. 3. THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT. 4. ALL HMA PAVEMENT PATCHING SHALL BE CLASS D. 5. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED. 6. THE RESIDENT ENGINEER SHALL CONTACT MS. CORA MATHIS, AT (815) 485-6475 (OFFICE) OR AT (847) 715-8428 (CELL) A MINIMUM OF 72 HOURS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS. 7. 3 METER (10 FEET) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURB AND GUTTERS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED. 8. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H) 9. THE UNIT WEIGHT (CONVERSION FACTOR) QUOTED IS FOR THE ESTIMATING PLAN QUANTITIES ONLY. ACTUAL QUANTITIES TO FULFILL CONTRACT REQUIREMENTS WILL BE DETERMINED BASED ON UNIT WEIGHT OF APPROVED MIX DESIGN, PLAN DIMENSIONS, AND DENSITY LIMITATIONS. MAXIMUM PAYMENT WILL BE COMPUTED BASED ON WEIGHT AVERAGE DENSITIES OF THE IN-PLACE MIXTURE. 10. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER. 11. THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE INSTALLATION OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES	420001-07 PAVEMENT JOINTS	
3	SUMMARY OF QUANTITIES	442201-03 CLASS C AND D PATCHES	
4-8	TYPICAL SECTIONS	606001-04 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB & GUTTER	
9	GENERAL PLAN	606201-02 TYPE B GUTTER (INLET, OUTLET, AND ENTRANCE)	
10-13	ROADWAY & PAVEMENT MARKING PLANS	606301-04 PC CONCRETE ISLANDS AND MEDIANS	
14-19	STRUCTURE GENERAL PLAN & ELEVATION & BRIDGE JOINT REPAIR PLANS	701421-02 LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS >45 MPH TO 55 MPH	
20-24	DETOUR PLANS	701426-03 LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS > 45 MPH	
25	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)	701601-04 URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN	
26	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)	701901-01 TRAFFIC CONTROL DEVICES	
27	BUTT JOINT AND HMA TAPER DETAILS (BD-32)	780001-02 TYPICAL PAVEMENT MARKINGS	
28	HMA TAPER AT EDGE OF PCC PAVEMENT (BD-33)	781001-03 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS	
29	ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-08)	630001-08	
30	RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) (TC-11)	630201-06	
31-32	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS (TC12A-TC12B)	631031-07	
33	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)	630301-05	
34	TRAFFIC CONTROL FOR SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES (TC-17)	635001-01	
35	SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS (TC-18)	635006-03	
36	ARTERIAL ROAD INFORMATION SIGNING (TC-22)	635011-02	

SUMMARY OF QUANTITIES			URBAN 100% STATE	CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES			URBAN 100% STATE	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000	SFTY-2A				CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000	SFTY-2A			
20201006	GRADING AND SHAPING SHOULDERS	UNIT	77	77					70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	37	37					70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	10	10				
40600300	AGGREGATE (PRIME COAT)	TON	186	186					70300100	SHORT-TERM PAVEMENT MARKING	FOOT	2545	2545				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	15	15					70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	36.4	36.4				
40600895	CONSTRUCTING TEST STRIP	EACH	3	3					70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	35525	35525				
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SO YD	727	727					70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	85	85				
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	12	12					70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	715	715				
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	1651	1651					70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	170	170				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	3792	3792					70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	285	285				
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	4787	4787					* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	36.4	36.4				
44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SO YD	7337	7337					* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	35525	35525				* SPECIALTY ITEMS
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	575	575					* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	85	85				
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SO YD	39	39					* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	715	715				
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SO FT	1940	1940					* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	170	170				
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	436	436					* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	150	150				
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	50	50					* 78200420	RAISED REFLECTIVE PAVEMENT MARKER	EACH	10	10				
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	89	89					* 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	120	120				
48102100	AGGREGATE ^{WEDGE} SHOULDER TYPE B	TON	118	118					* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4				
55039700	STORM SEWERS TO BE CLEANED	FOOT	740	740					X0321743	SILICONE JOINT SEALER, 1"	FOOT	245		245			
60255410	CATCH BASINS TO BE CLEANED	EACH	37	37					X0322256	TEMPORARY INFORMATION SIGNING	SO FT	103	103				
60404940	FRAMES AND GRATES, TYPE 23	EACH	9	9					X0323076	SILICONE JOINT SEALER, 1 3/4"	FOOT	134		134			
60404950	FRAMES AND GRATES, TYPE 24	EACH	21	21					X0323077	SILICONE JOINT SEALER, 2 3/4"	FOOT	28		28			
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	2	2					X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1965	1965				
* 63000001	STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS	FOOT	250	250					X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SO YD	2122	2122				
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE C	EACH	3	3					XX001908	PORTLAND CEMENT CONCRETE MEDIAN REMOVAL & REPLACEMENT	SO FT	144	144				
63200310	GUARDRAIL REMOVAL	FOOT	250	250					Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	36	36				
* 63100169	TRAFFIC BARRIER TERMINAL, TYPE 1, (SPECIAL) FLARED	EACH	5	5					Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	5	5				
64200105	SHOULDER RUMBLE STRIP	FOOT	4622	4622					Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	2	2				
63500105	DELINEATORS	EACH	314	314					X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6													
67100100	MOBILIZATION	L SUM	1	1													
70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	1	1													
70102550	TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR	EACH	5	5													

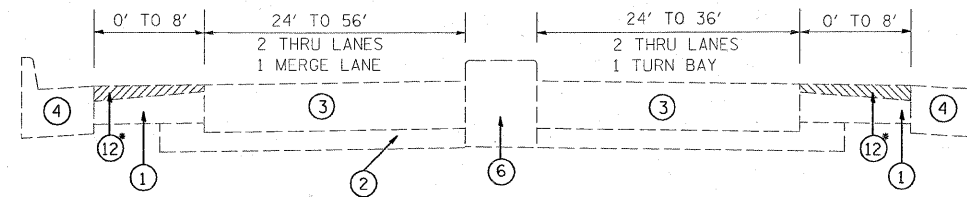
LEGEND

- ① EXISTING STABILIZED SHOULDERS
- ② EXISTING SUB-BASE 4"
- ③ EXISTING 10" PCC PAVEMENT
- ④ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- ⑤ EXISTING TYPE B GUTTER
- ⑥ EXISTING RAISED MEDIAN
- ⑦ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- ⑧ EXISTING AGGREGATE SHOULDER
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (1")
- ⑩ PROPOSED HOT MIX ASPHALT SURFACE COURSE, MIX "D" N70, (1 1/2")
- ⑪ PROPOSED AGGREGATE SHOULDER, TYPE B
- ⑫ PROPOSED HOT MIX ASPHALT SURFACE REMOVAL, (VARIABLE DEPTH)
- ⑬ PROPOSED HOT MIX ASPHALT BINDER COURSE, IL-19.0, N70 (2 1/4")
- ⑭ PROPOSED HOT MIX ASPHALT SURFACE REMOVAL 2 1/4"
- ⑮ PROPOSED PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VAR. DEPTH)
- ⑯ PROPOSED MEDIAN REMOVAL PARTIAL DEPTH



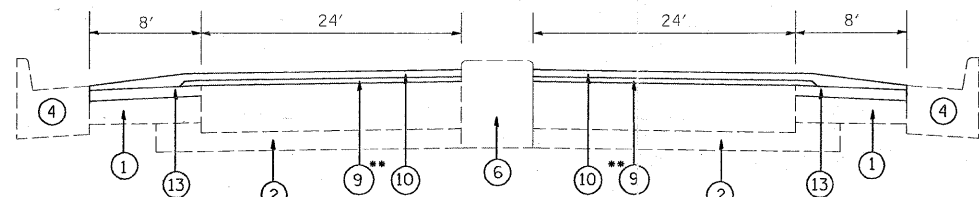
EXISTING TYPICAL SECTION
MEADOW AVE.

CENTERLINE STA. (53+00 TO 56+00) & STA. (82+75 TO 88+95)



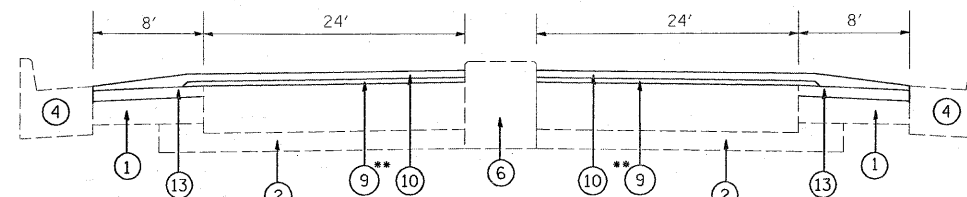
EXISTING TYPICAL SECTION
MEADOW AVE. & CENTER ST.

CENTERLINE STA. 88+95 TO 91+30
(LOOKING EAST)



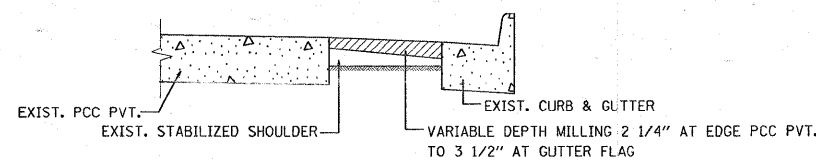
PROPOSED TYPICAL SECTION
MEADOW AVE.

CENTERLINE STA. (53+00 TO 56+00) & STA. (82+75 TO 88+95)

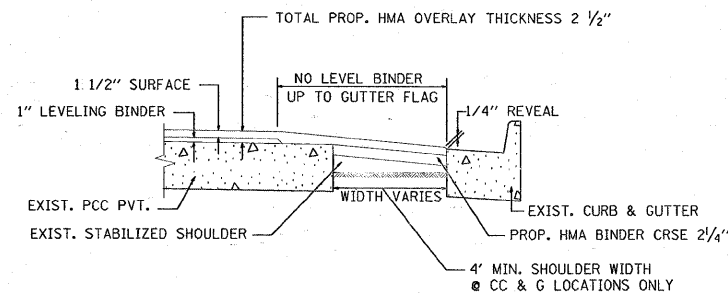


PROPOSED TYPICAL SECTION
MEADOW AVE. & CENTER ST.

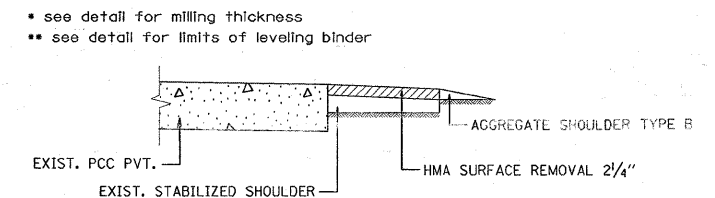
CENTERLINE STA. 88+95 TO 91+30
(LOOKING EAST)



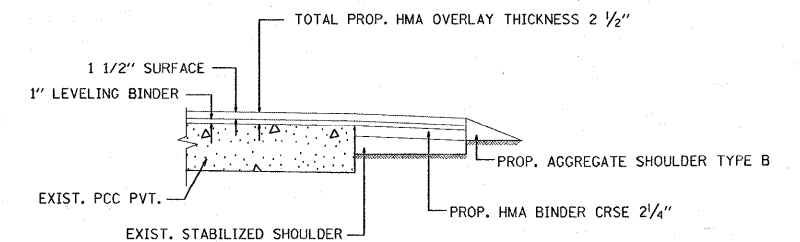
MILLING OF HMA SHOULDERS
ADJACENT TO CURB AND GUTTER



RESURFACING OF HMA SHOULDERS
ADJACENT TO CURB AND GUTTER



MILLING OF HMA SHOULDERS
ADJACENT TO AGGREGATE SHOULDERS



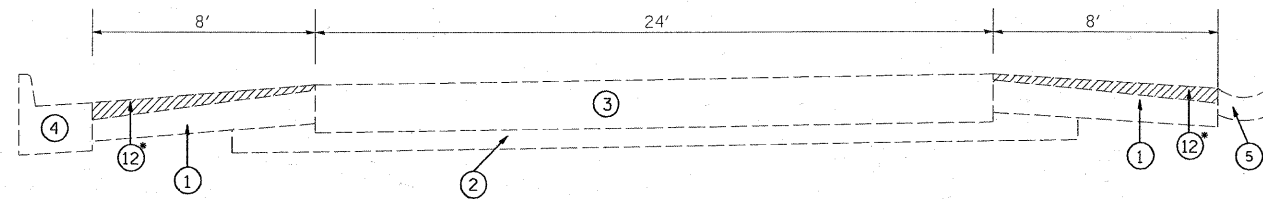
RESURFACING OF HMA SHOULDERS
ADJACENT TO AGGREGATE SHOULDERS

HOT-MIX ASPHALT MIXTURE REQUIREMENTS				
MIXTURE USES	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 MM)	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES (HMA BINDER IL-19MM)	CLASS D PATCHES (HMA BINDER IL-19MM)
AC TYPE	SBS/SBR PG 76-28/-22	PG 64-22	PG 64-22 *	PG 64-22 *
DESIGN AIR VOIDS	4.0% @ 50 GYR	4.0% @ 70 GYR	4% @ 70 GYR	4% @ 70 GYR

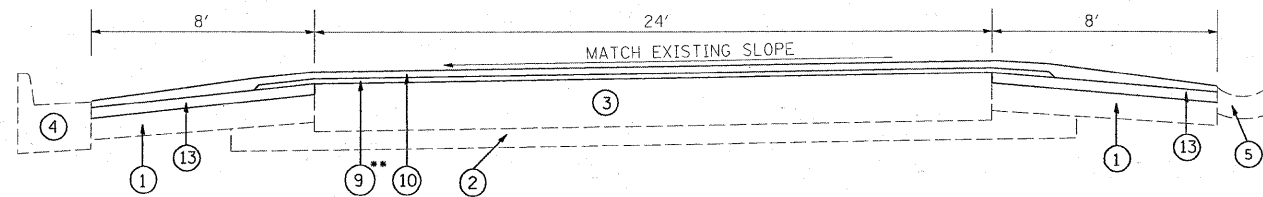
THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIX QUANTITIES IS 112 LBS/SY/IN
* WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER SHALL BE PG 58/22

NOTE: PATCH FIRST

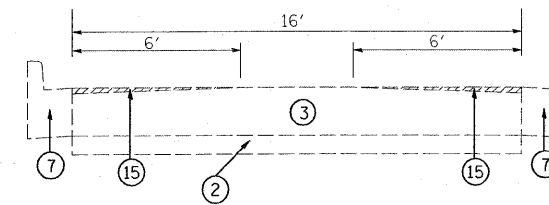
LEGEND



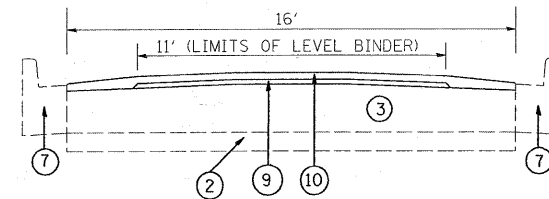
EXISTING TYPICAL SECTION
RAMP AB STA. 27+22 TO 32+00 (LOOKING NORTH)
RAMP AB STA. 24+15 TO 27+22 (LOOKING SOUTH)



PROPOSED TYPICAL SECTION
RAMP AB STA. 27+22 TO 32+00 (LOOKING NORTH)
RAMP AB STA. 24+15 TO 27+22 (LOOKING SOUTH)

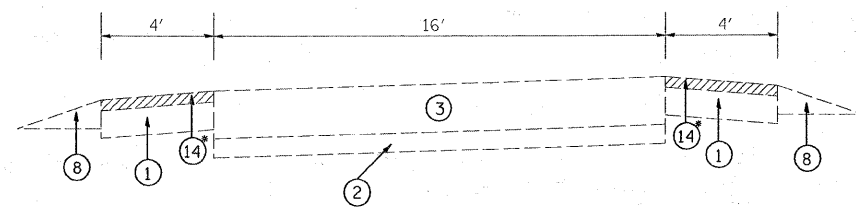


EXISTING TYPICAL SECTION
RAMP AB STA 33+00 TO 30+45

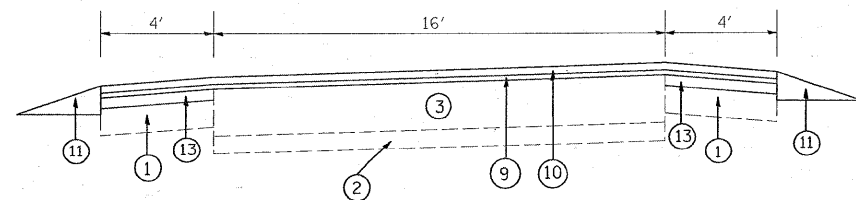


PROPOSED TYPICAL SECTION
RAMP AB STA 33+00 TO 30+45

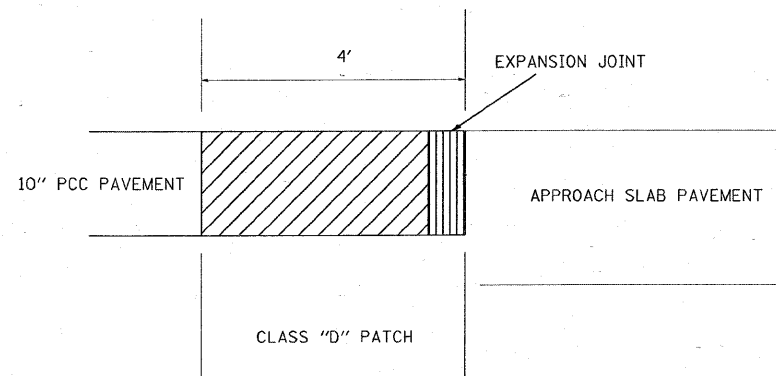
- ① EXISTING STABILIZED SHOULDERS
- ② EXISTING SUB-BASE 4"
- ③ EXISTING 10" PCC PAVEMENT
- ④ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- ⑤ EXISTING TYPE B GUTTER
- ⑥ EXISTING RAISED MEDIAN
- ⑦ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- ⑧ EXISTING AGGREGATE SHOULDER
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (1")
- ⑩ PROPOSED HOT MIX ASPHALT SURFACE COURSE, MIX "D" N70, (1 1/2")
- ⑪ PROPOSED AGGREGATE SHOULDER, TYPE B
- ⑫ PROPOSED HOT MIX ASPHALT SURFACE REMOVAL, (VARIABLE DEPTH)
- ⑬ PROPOSED HOT MIX ASPHALT BINDER COURSE, IL-19.0, N70 (2 1/4")
- ⑭ PROPOSED HOT MIX ASPHALT SURFACE REMOVAL 2 1/4"
- ⑮ PROPOSED PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VAR. DEPTH)
- ⑯ PROPOSED MEDIAN REMOVAL PARTIAL DEPTH



EXISTING TYPICAL SECTION
RAMP BA STA 60+73 TO 64+61
RAMP BA STA 70+22 TO 73+00



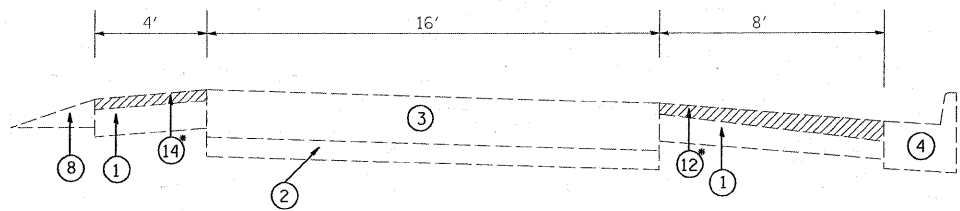
PROPOSED TYPICAL SECTION
RAMP BA STA 60+73 TO 64+61
RAMP BA STA 70+22 TO 73+00



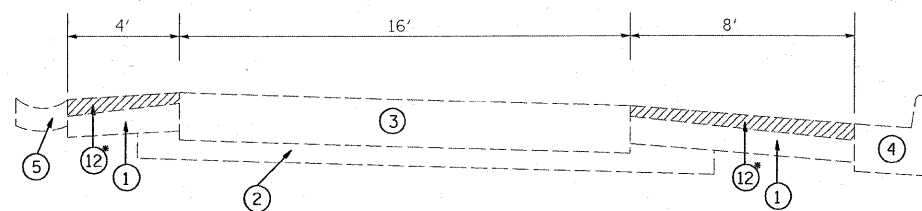
CLASS "D" PATCHES FOR EXPANSION JOINT REMOVAL.

Remove expansion joints at bridge approach pavement locations for Structure 5 (SN: 099-0188) (Northbound Center St. bridge over I-80) and place CLASS "D" PATCHES 10". The removal of expansion joints and placement of CLASS "D" PATCHES 10" will be paid for with the pay item Class "D" Patches 10".

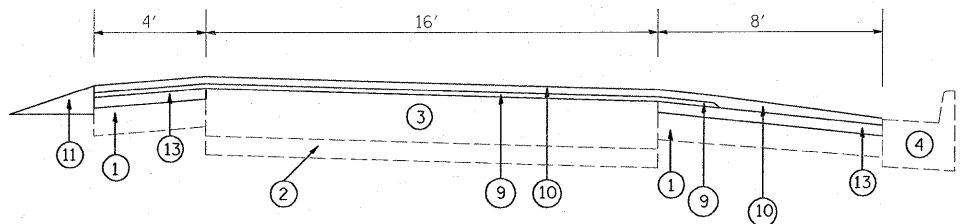
FILE NAME =	USER NAME = aqeelff	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RAMP AB/RAMP BA TYPICAL SECTIONS		F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwork\pwidot\AQEELFF\d0120663\012	504-design.dgn	DRAWN -	REVISED -		316	(10 & 1 D-2) RS-4	WILL	36	5		
PLOT SCALE = 50,000' / IN.		CHECKED -	REVISED -		CONTRACT NO. 62778						
PLOT DATE = 3/24/2009		DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						
				SCALE: NTS	SHEET NO. 2 OF 5 SHEETS	STA. X	TO STA. X				



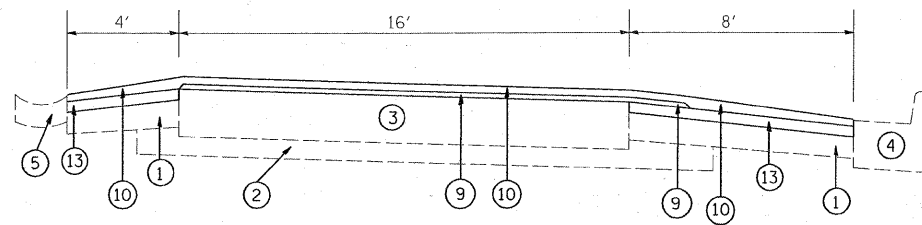
EXISTING TYPICAL SECTION
RAMP BD STA 23+55 TO 27+30 (LOOKING NORTH)



EXISTING TYPICAL SECTION
RAMP BD STA 27+30 TO 29+80



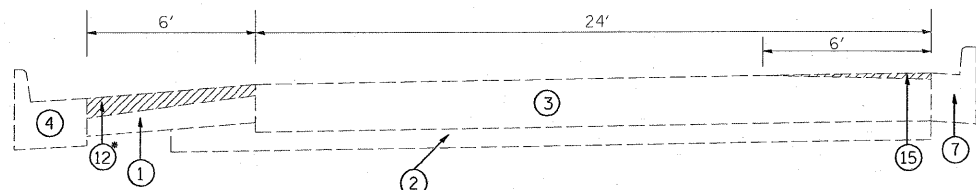
PROPOSED TYPICAL SECTION
RAMP BD STA 23+55 TO 27+30 (LOOKING NORTH)



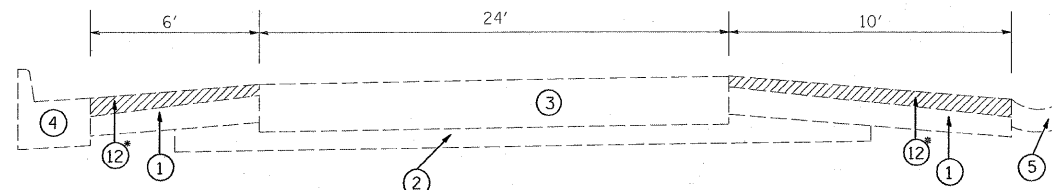
PROPOSED TYPICAL SECTION
RAMP BD STA 27+30 TO 29+80

LEGEND

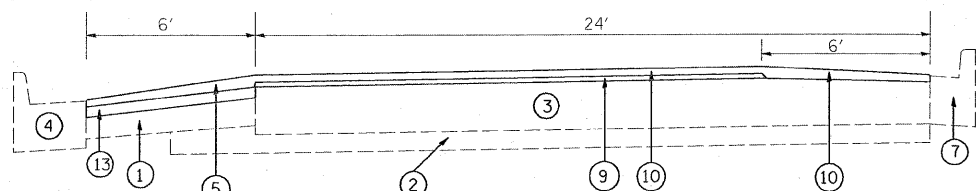
- ① EXISTING STABILIZED SHOULDERS
- ② EXISTING SUB-BASE 4"
- ③ EXISTING 10" PCC PAVEMENT
- ④ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- ⑤ EXISTING TYPE B GUTTER
- ⑥ EXISTING RAISED MEDIAN
- ⑦ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- ⑧ EXISTING AGGREGATE SHOULDER
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (1")
- ⑩ PROPOSED HOT MIX ASPHALT SURFACE COURSE, MIX "D" N70, (1 1/2")
- ⑪ PROPOSED AGGREGATE SHOULDER, TYPE B
- ⑫ PROPOSED HOT MIX ASPHALT SURFACE REMOVAL, (VARIABLE DEPTH)
- ⑬ PROPOSED HOT MIX ASPHALT BINDER COURSE, IL-19.0, N70 (2 1/4")
- ⑭ PROPOSED HOT MIX ASPHALT SURFACE REMOVAL 2 1/4"
- ⑮ PROPOSED PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VAR. DEPTH)
- ⑯ PROPOSED MEDIAN REMOVAL PARTIAL DEPTH



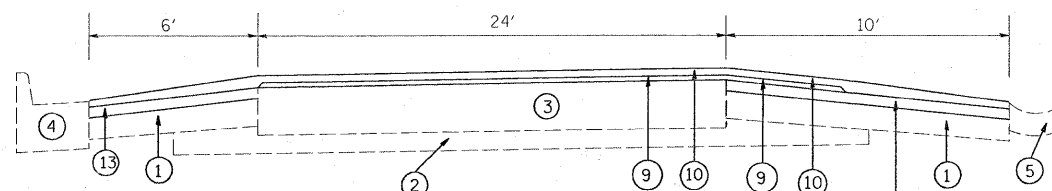
EXISTING TYPICAL SECTION
RAMP AD STA 24+00 TO 28+91 (LOOKING WEST)



EXISTING TYPICAL SECTION
RAMP AD STA 28+91 TO 35+50 (LOOKING SOUTH)



PROPOSED TYPICAL SECTION
RAMP AD STA 24+00 TO 28+91 (LOOKING WEST)

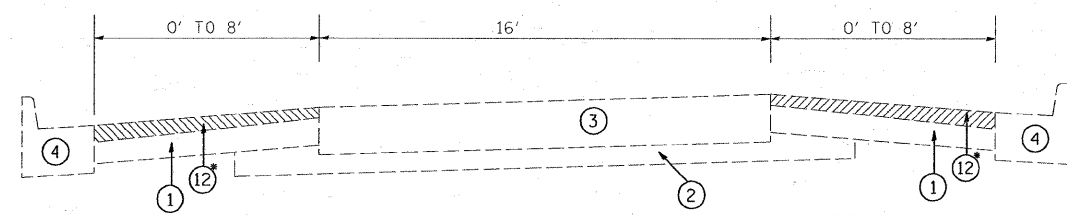


PROPOSED TYPICAL SECTION
RAMP AD STA 28+91 TO 35+50 (LOOKING SOUTH)

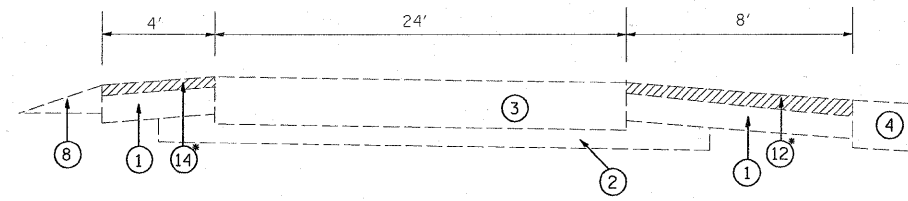
FILE NAME =	USER NAME = oqueeff	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RAMP BD/RAMP AD TYPICAL SECTIONS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pwork\PWID01\AQUEEFF\08120663\012	504-design.dgn	DRAWN -	REVISED -		316	(10 & 1 D-2) RS-4	WILL	36	6			
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED -		CONTRACT NO. 62778							
	PLOT DATE = 3/21/2009	DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
				SCALE: NTS	SHEET NO. 3 OF 5 SHEETS	STA. X	TO STA. X					

LEGEND

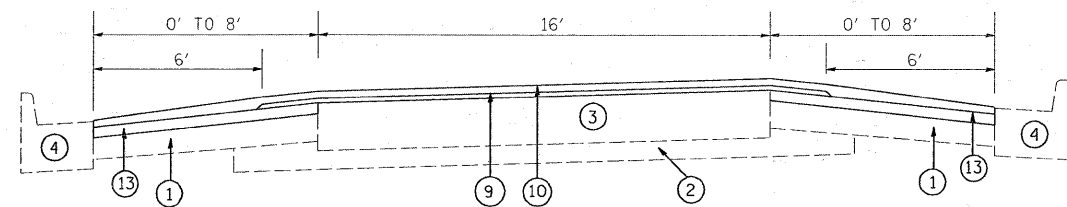
- ① EXISTING STABILIZED SHOULDERS
- ② EXISTING SUB-BASE 4"
- ③ EXISTING 10" PCC PAVEMENT
- ④ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- ⑤ EXISTING TYPE B GUTTER
- ⑥ EXISTING RAISED MEDIAN
- ⑦ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- ⑧ EXISTING AGGREGATE SHOULDER
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (1")
- ⑩ PROPOSED HOT MIX ASPHALT SURFACE COURSE, MIX "D" N70, (1 1/2")
- ⑪ PROPOSED AGGREGATE SHOULDER, TYPE B
- ⑫ PROPOSED HOT MIX ASPHALT SURFACE REMOVAL, (VARIABLE DEPTH)
- ⑬ PROPOSED HOT MIX ASPHALT BINDER COURSE, IL-19.0, N70 (2 1/4")
- ⑭ PROPOSED HOT MIX ASPHALT SURFACE REMOVAL 2 1/4"
- ⑮ PROPOSED PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VAR. DEPTH)
- ⑯ PROPOSED MEDIAN REMOVAL PARTIAL DEPTH



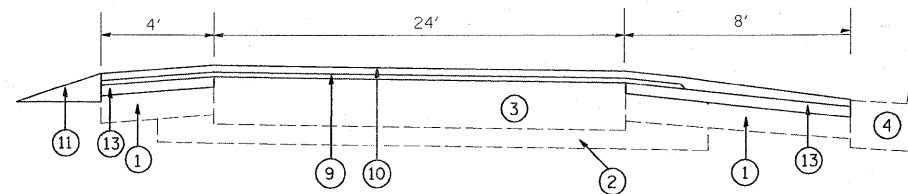
EXISTING TYPICAL SECTION
RAMP DB STA 30+45 TO 31+77



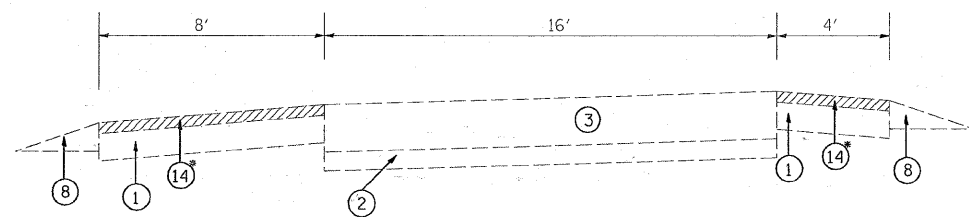
EXISTING TYPICAL SECTION
RAMP DA STA. 24+75 TO 29+50 (LOOKING NORTH)



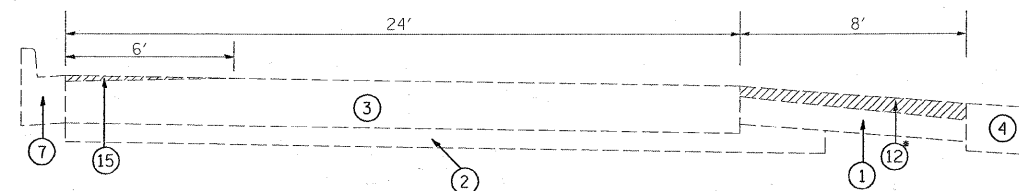
PROPOSED TYPICAL SECTION
RAMP DB STA 30+45 TO 31+77



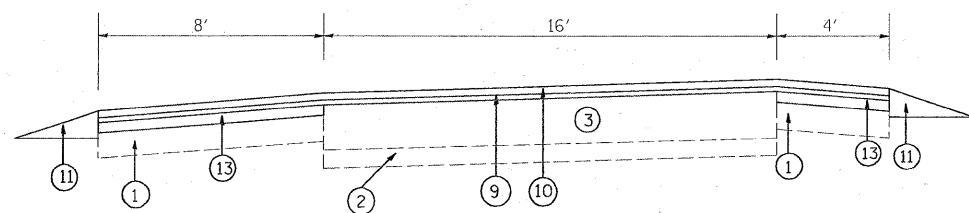
PROPOSED TYPICAL SECTION
RAMP DA STA. 24+75 TO 29+50 (LOOKING NORTH)



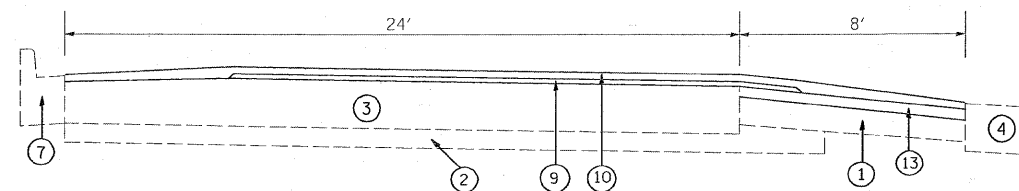
EXISTING TYPICAL SECTION
RAMP DB STA 23+70 TO 29+31
RAMP AC STA 35+00 TO 43+90 (LOOKING SOUTH)



EXISTING TYPICAL SECTION
RAMP DA STA. 29+50 TO 31+96
RAMP AB STA 32+00 TO 34+04 (LOOKING SOUTH)

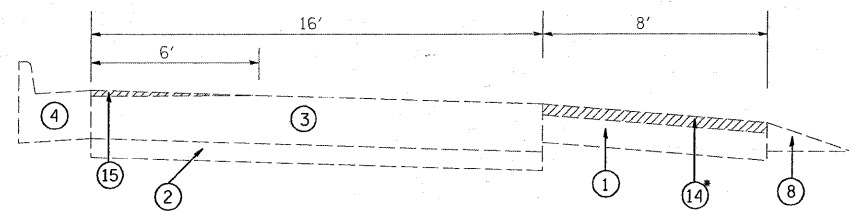


PROPOSED TYPICAL SECTION
RAMP DB STA 23+70 TO 29+31
RAMP AC STA 35+00 TO 43+90 (LOOKING SOUTH)

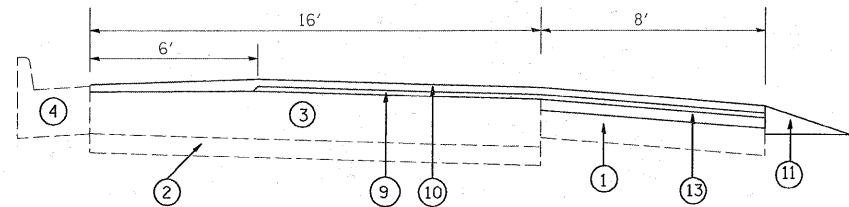


PROPOSED TYPICAL SECTION
RAMP DA STA. 29+50 TO 31+96
RAMP AB STA 32+00 TO 34+04 (LOOKING SOUTH)

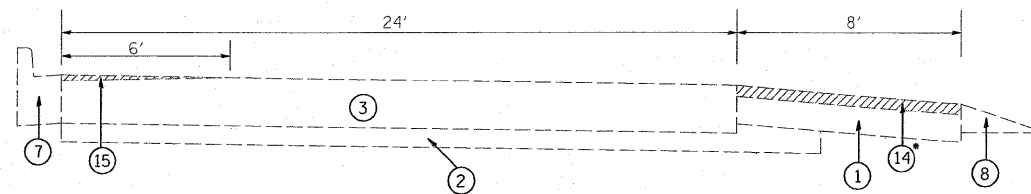
FILE NAME =	USER NAME = aqeueiff	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RAMP DB/ RAMP DA/ RAMP AC/ RAMP AB TYPICAL SECTIONS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwwork\pwwid01\AQEUEIFF\8120663\DI2	504-design.dgn	DRAWN -	REVISED -		316	(10 & 1 D-2) RS-4	WILL	36	7			
PLOT SCALE = 50,000' / IN.		CHECKED -	REVISED -		CONTRACT NO. 62778			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
PLOT DATE = 3/21/2009		DATE -	REVISED -		SCALE: NTS	SHEET NO. 4 OF 5 SHEETS	STA. X	TO STA. X				



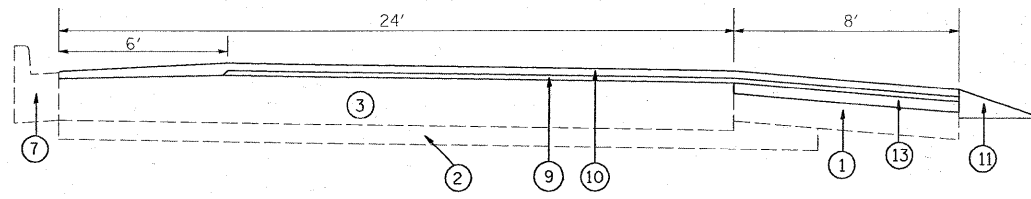
EXISTING TYPICAL SECTION
RAMP AC STA 30+15 TO 35+00 (LOOKING SOUTH)



PROPOSED TYPICAL SECTION
RAMP AC STA 30+15 TO 35+00 (LOOKING SOUTH)



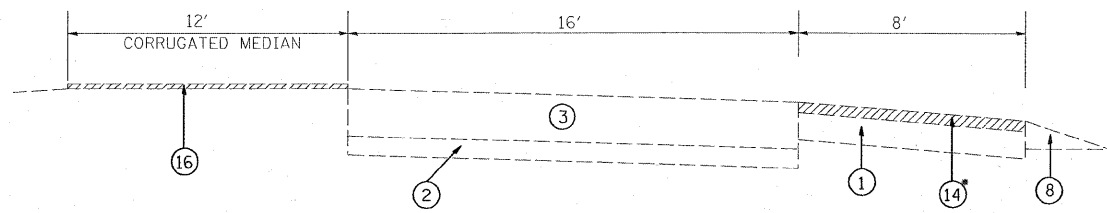
EXISTING TYPICAL SECTION
RAMP AC STA. 25+60 TO 28+67 (LOOKING WEST)



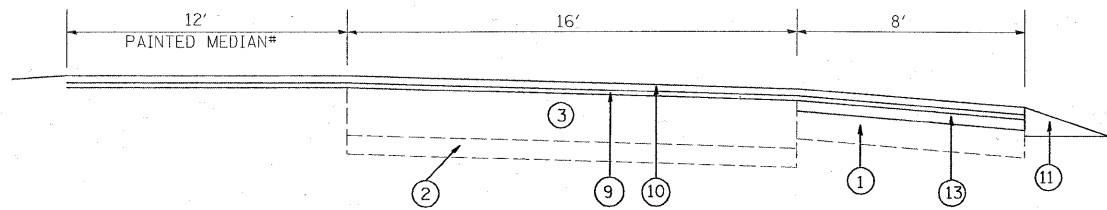
PROPOSED TYPICAL SECTION
RAMP AC STA. 25+60 TO 28+67 (LOOKING WEST)

LEGEND

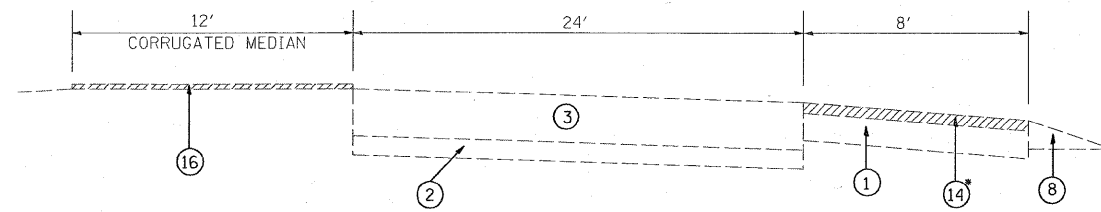
- ① EXISTING STABILIZED SHOULDERS
- ② EXISTING SUB-BASE 4"
- ③ EXISTING 10" PCC PAVEMENT
- ④ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- ⑤ EXISTING TYPE B GUTTER
- ⑥ EXISTING RAISED MEDIAN
- ⑦ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- ⑧ EXISTING AGGREGATE SHOULDER
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (1")
- ⑩ PROPOSED HOT MIX ASPHALT SURFACE COURSE, MIX "D" N70, (1 1/2")
- ⑪ PROPOSED AGGREGATE SHOULDER, TYPE B
- ⑫ PROPOSED HOT MIX ASPHALT SURFACE REMOVAL, (VARIABLE DEPTH)
- ⑬ PROPOSED HOT MIX ASPHALT BINDER COURSE, IL-19.0, N70 (2 1/4")
- ⑭ PROPOSED HOT MIX ASPHALT SURFACE REMOVAL 2 1/4"
- ⑮ PROPOSED PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VAR. DEPTH)
- ⑯ PROPOSED MEDIAN REMOVAL PARTIAL DEPTH



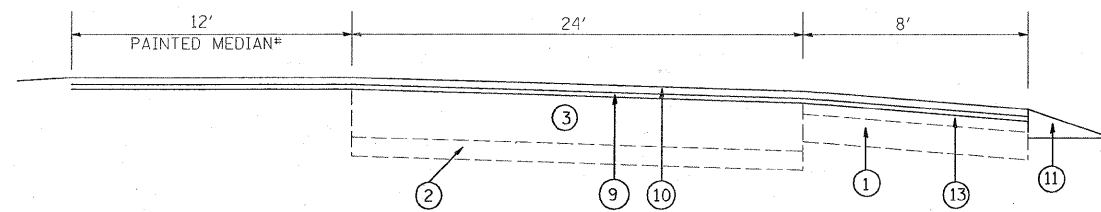
EXISTING TYPICAL SECTION
RAMP AC STA 28+67 TO 30+15 (LOOKING WEST)



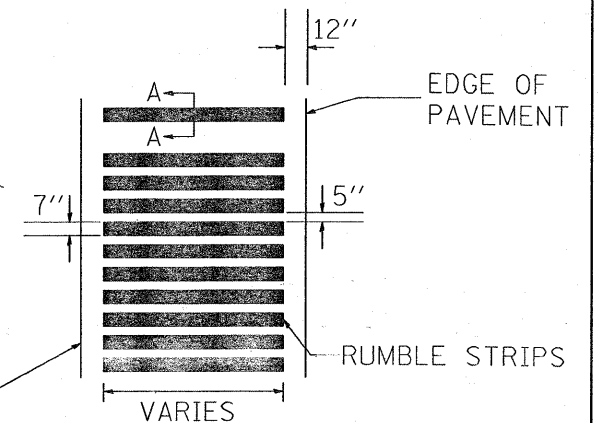
PROPOSED TYPICAL SECTION
RAMP AC STA 28+67 TO 30+15 (LOOKING WEST)



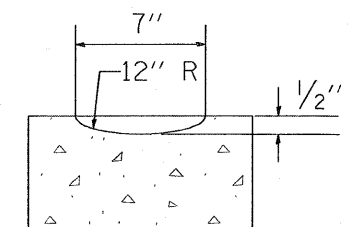
EXISTING TYPICAL SECTION
RAMP AC STA 24+09 TO 25+60 (LOOKING WEST)



PROPOSED TYPICAL SECTION
RAMP AC STA 24+09 TO 25+60 (LOOKING WEST)

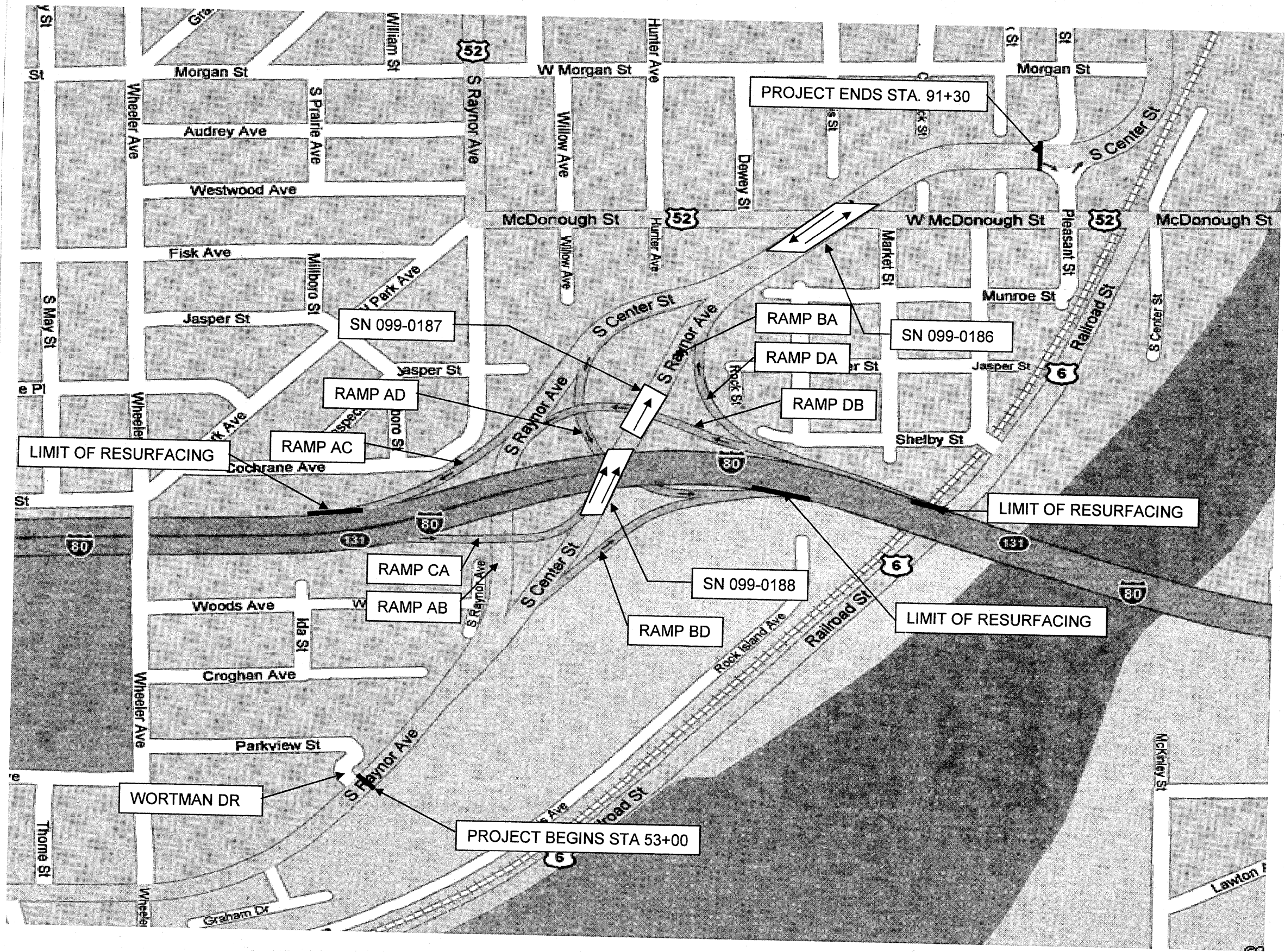


#DETAIL: BIRD-EYE VIEW
OF MEDIAN WITH RUMBLE STRIPS



SECTION A-A

FILE NAME =	USER NAME = aqeelff	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RAMP AC TYPICAL SECTIONS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pwork\p\1001\A0UEELFF\0120663\0124504-design.dgn		DRAWN -	REVISED -		316	(10 & 1 D-2) RS-4	WILL	36	8			
PLOT SCALE = 50,000 / IN.		CHECKED -	REVISED -		CONTRACT NO. 62778							
PLOT DATE = 3/21/2009		DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
				SCALE: NTS	SHEET NO. 5 OF 5 SHEETS	STA. X	TO STA. X					



**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CENTER ST.
GENERAL PLAN**

FILE NAME =	USER NAME = equeiff	DESIGNED -	REVISED -
cr:\pw_work\VP\DOT\ADUEELFF\0128663\0128663-584-design.dgn		DRAWN -	REVISED -
PLT SCALE = 100,000' / IN.		CHECKED -	REVISED -
PLT DATE = 3/28/2009		DATE -	REVISED -

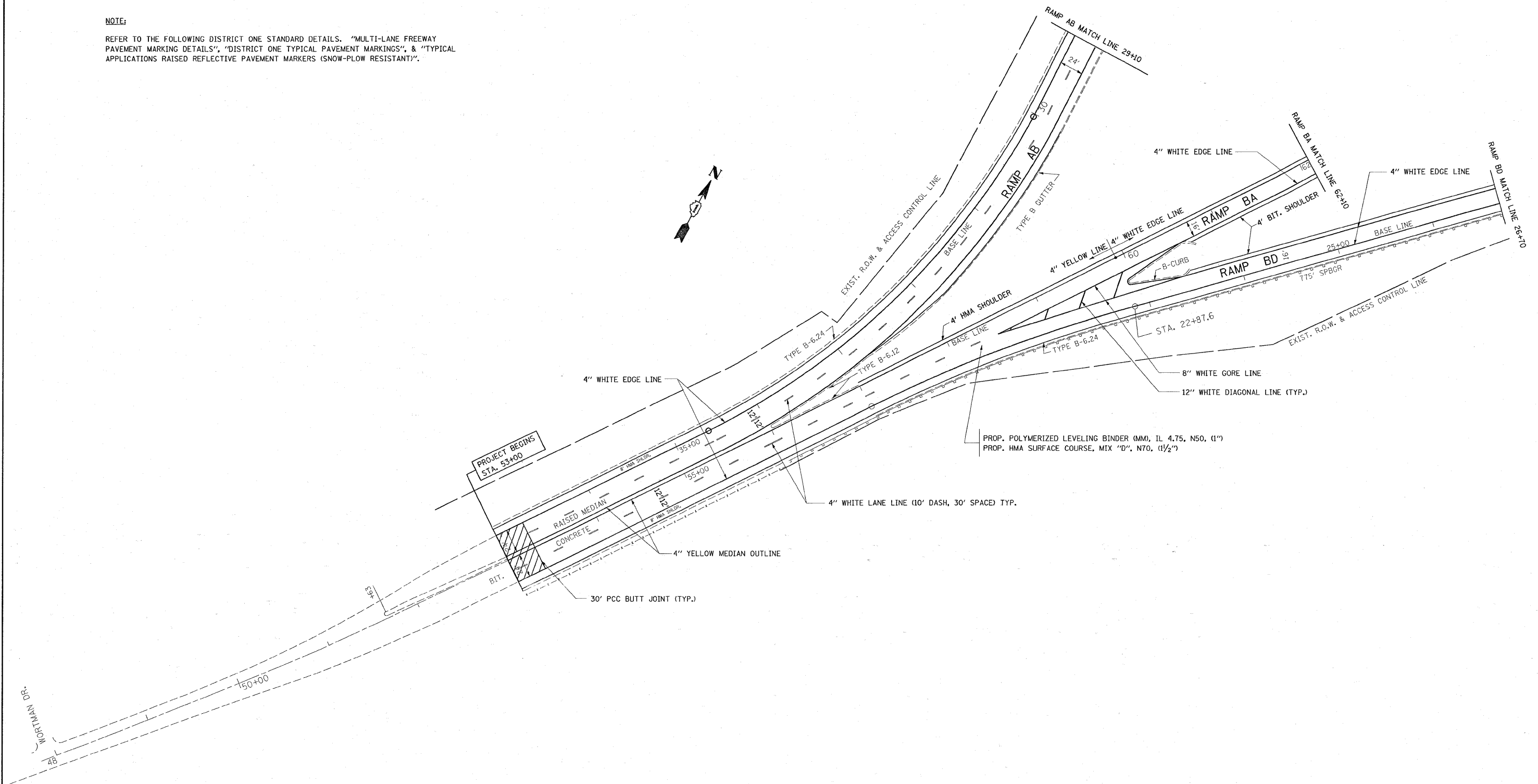
SCALE:	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.
--------	-------------------------	------	---------

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
316	(10&1-D-2)RS-4	WILL	36	9
CONTRACT NO. 62778			ILLINOIS FED. AID PROJECT	

D124504-design.dgn 3/20/2009 11:24:00 AM User=equeiff

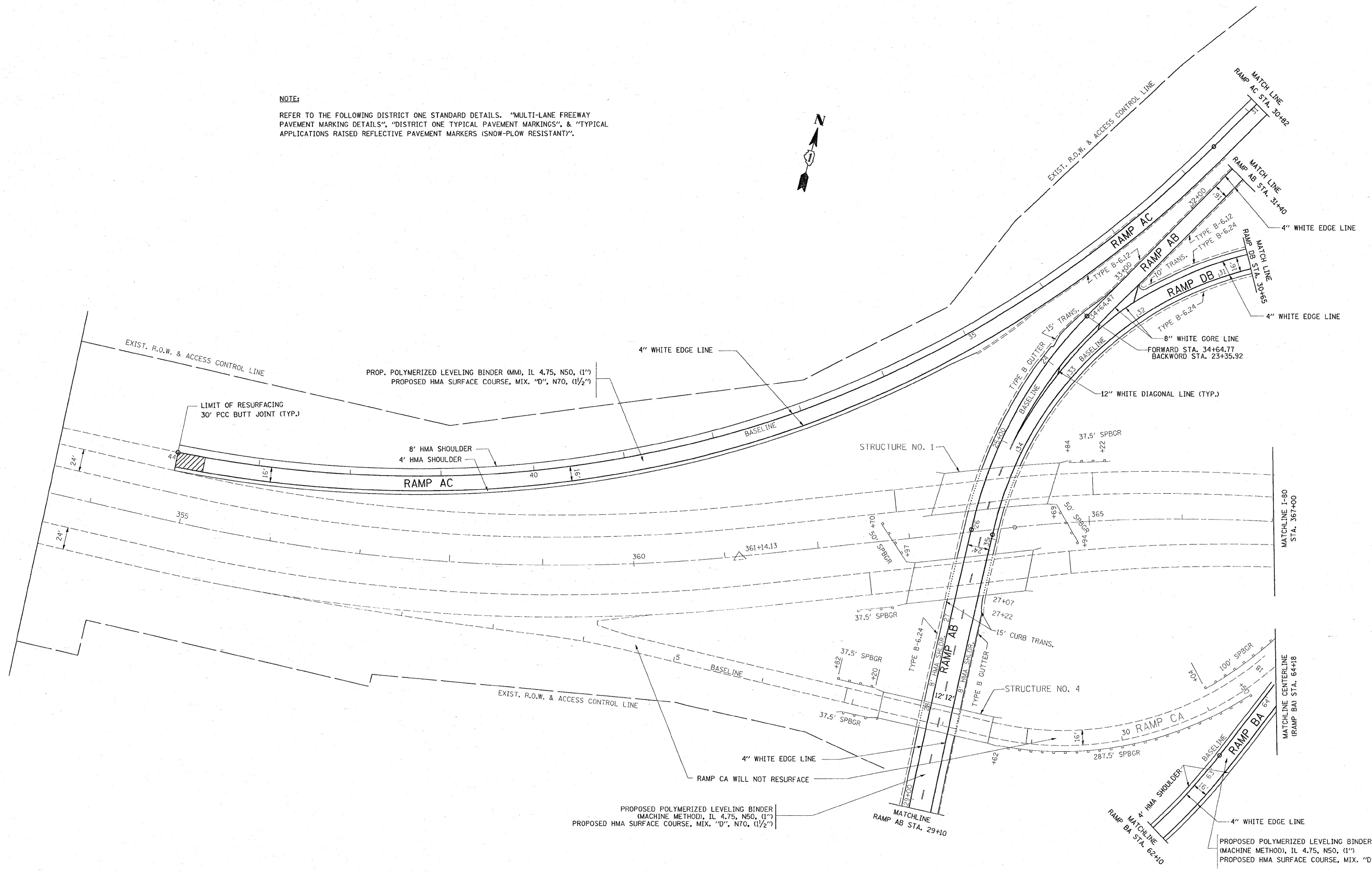
NOTE:

REFER TO THE FOLLOWING DISTRICT ONE STANDARD DETAILS. "MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS", "DISTRICT ONE TYPICAL PAVEMENT MARKINGS", & "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)".

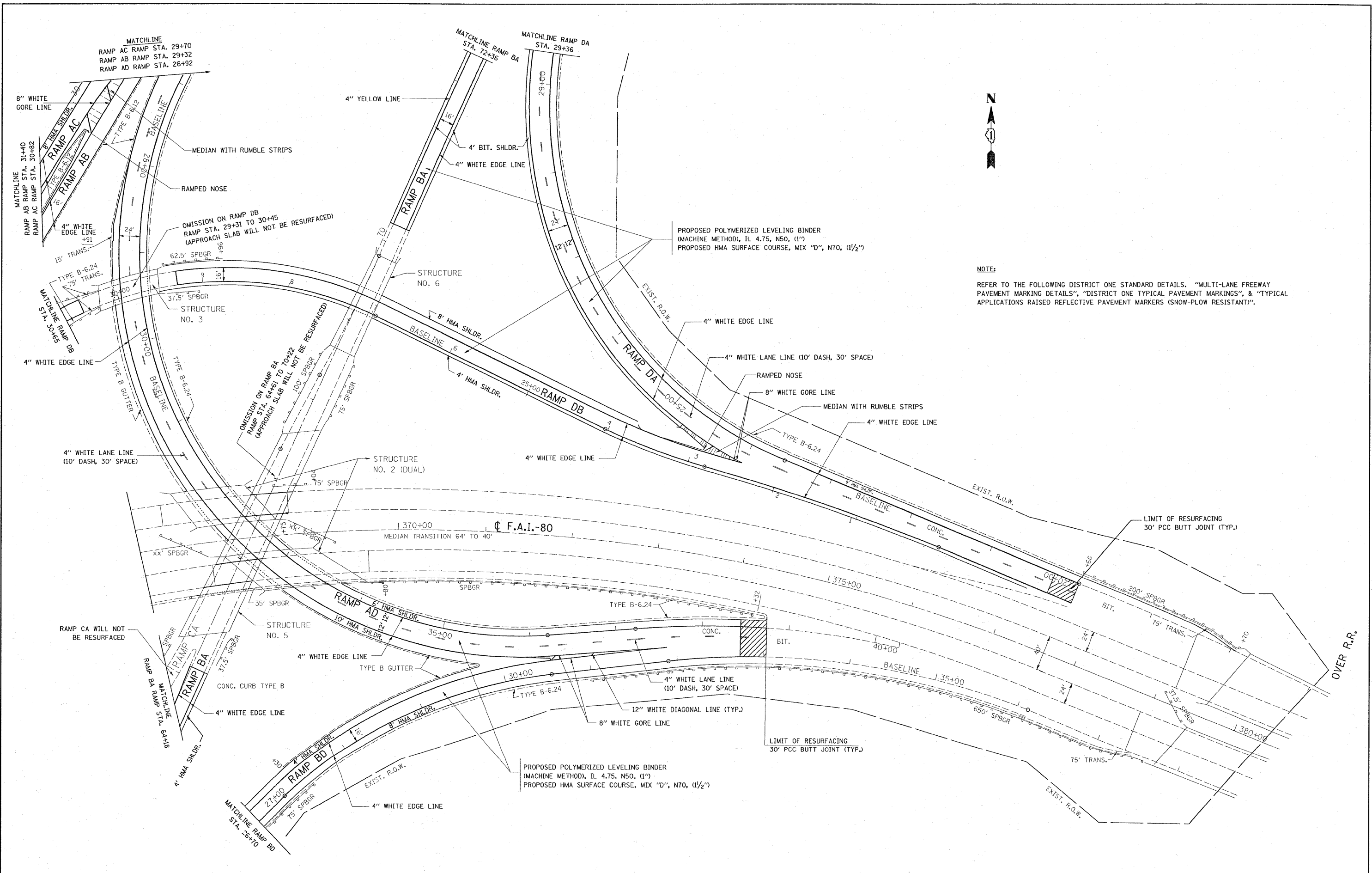


FILE NAME =	USER NAME = aqeelff	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTER ST. ROADWAY PLAN		F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pwork\PWIDOT\AQUEELFF\0120663\0120663-01.dgn	504-design.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO. 1 OF 4 SHEETS	STA.	TO STA.	316	(10&I-D-2)RS-4	WILL	36 10
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED -					CONTRACT NO. 62778				
	PLOT DATE = 3/21/2009	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

NOTE:
 REFER TO THE FOLLOWING DISTRICT ONE STANDARD DETAILS. "MULTI-LANE FREEWAY
 PAVEMENT MARKING DETAILS", "DISTRICT ONE TYPICAL PAVEMENT MARKINGS", & "TYPICAL
 APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)".



FILE NAME =	USER NAME = equelff	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTER ST. ROADWAY PLAN		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pw\work\p\id07\AQUEELFF\d0120663\0124504-design.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO. 2 OF 4 SHEETS	STA.	316	(10&1-D-2IRS-4)	WILL	36	11
		CHECKED -	REVISED -				TO STA.					
		DATE -	REVISED -									
										CONTRACT NO. 62778		
										ILLINOIS FED. AID PROJECT		



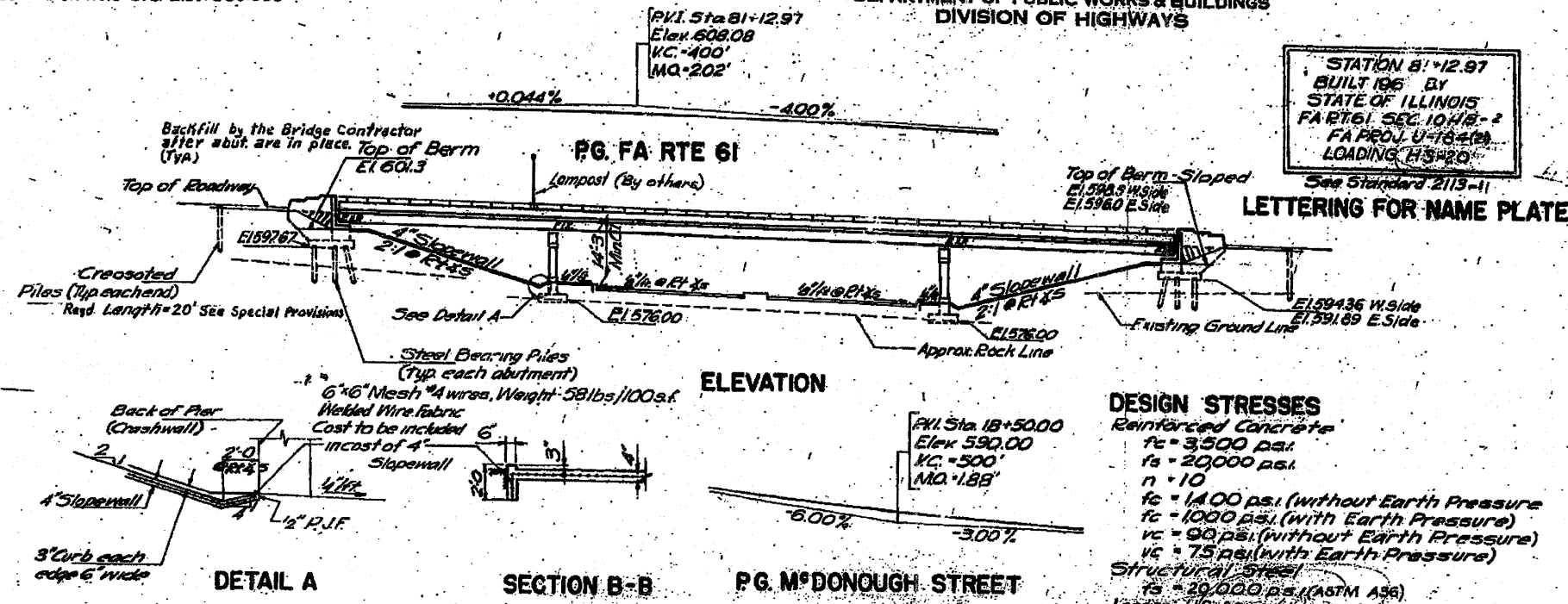
NOTE:
 REFER TO THE FOLLOWING DISTRICT ONE STANDARD DETAILS. "MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS", "DISTRICT ONE TYPICAL PAVEMENT MARKINGS", & "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)".

FILE NAME =	USER NAME = aequalff	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTER ST. ROADWAY PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
02:\pwork\p\WID01\AQUELEFF\0120663\012	504-design.dgn	DRAWN -	REVISED -			316	(10&1-D-2)RS-4	WILL	36	12	
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 62778					
	PLOT DATE = 3/21/2009	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
						SCALE:	SHEET NO. 3 OF 4 SHEETS	STA.	TO STA.		

Bench Mark No. 46B: Top of Ring (for chains to caps on outlets) on Fire Hydrant S. Side Hyd. N.W. Corner Munroe and Illinois Sts. Elev. 585.365

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

DATE	BY	CHKD	APP'D	SHEET NO. 1
1/17	WILL	1/17	1/17	1/17
PROJECT U-184(2)				SHEETS 10



GENERAL NOTES

Class X Concrete shall be used throughout. Coarse aggregate used in parapets and end posts shall be free of chert, flint, limestone, lignite and soft sandstone.

The concrete floor slab shall be finished in accordance with Article 51.19 of the Standard Specifications.

All welding shall comply with the requirements of the Standard Specifications for Welded Highway and Railway Bridges, of the American Welding Society.

All Bolters, Bolshers, Bearing Plates, Lead Plates, Pintles, and Anchor Bolts shall be fabricated and set in accordance with Article 51.15 of the Standard Specifications and are included in quantity of Structural Steel.

Anchor Bolts shall be set before connecting diaphragms over support. Space reinforcing to miss Anchor Bolts.

Expansion guards and plates shall be fabricated and erected in accordance with Article 51.13(d) of the Standard Specifications and are in quantity of Structural Steel.

Except as otherwise provided all Structural Steel shall receive one shop coat of red lead paint and two field coats of aluminum paint. See Articles 56.1 to 56.5 inclusive of the Standard Specifications.

All surfaces of expansion guards inaccessible after erection shall be given two shop coats of red lead paint. The 3/4" welded studs shall not be painted.

The Contractor shall drive two (2) test piles in permanent locations as directed by the Engineer, before ordering remainder of piles. One each of either one of the East and West Abutments.

No rock larger than 3" shall be placed in fills in the area where piles are to be driven.

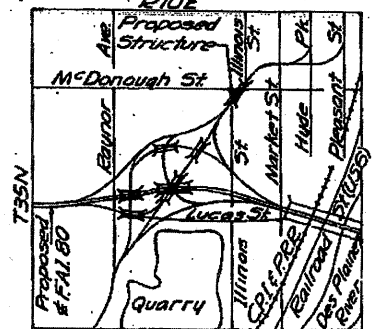
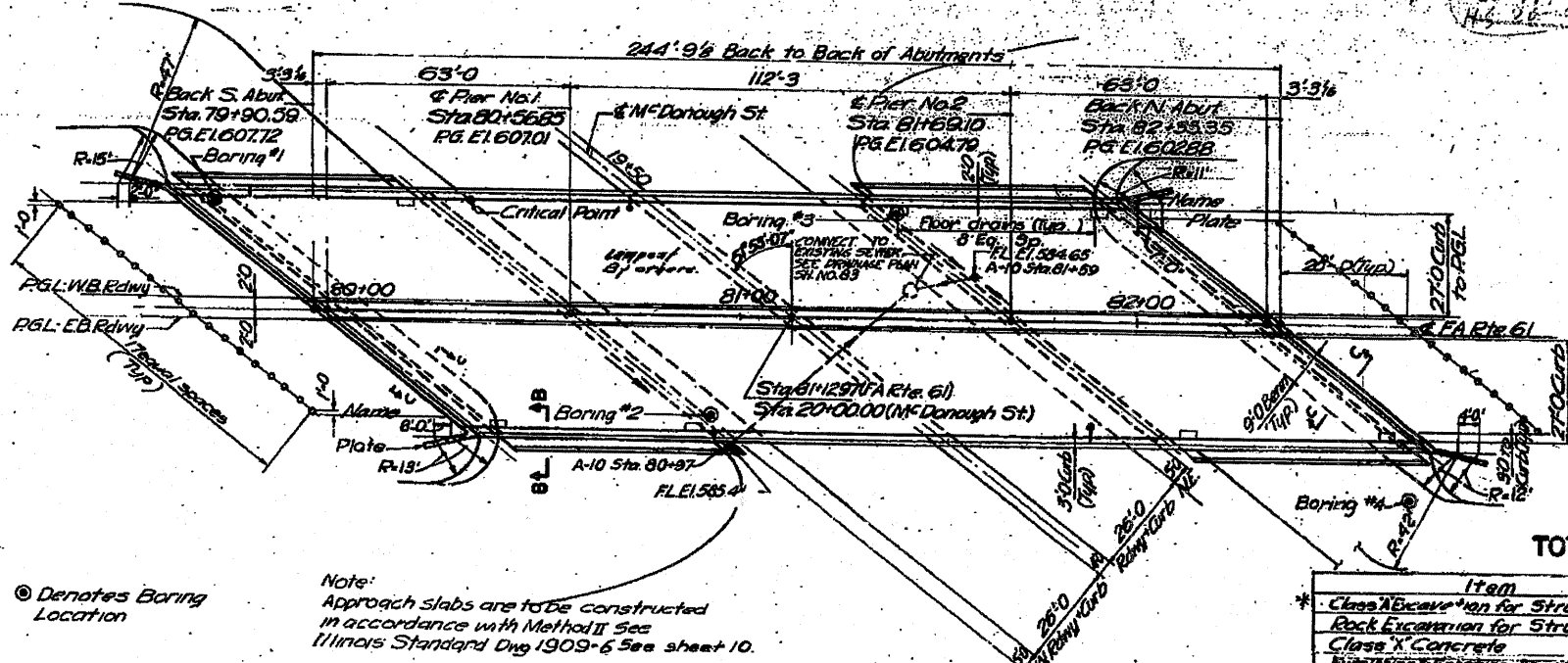
All steel bearing piles are to be driven to refusal. Piles shall be 8" with 1/2" open holes unless noted.

Structural steel shall conform to the specifications for structural steel, ASTM - A36.

DESIGN STRESSES

Reinforced Concrete
 $f_c = 3500$ psi.
 $f_s = 20000$ psi.
 $n = 10$
 $f_c = 1400$ psi. (without Earth Pressure)
 $f_c = 1000$ psi. (with Earth Pressure)
 $f_s = 90$ psi. (without Earth Pressure)
 $f_s = 75$ psi. (with Earth Pressure)

Structural Steel
 $f_s = 20,000$ psi. (ASTM A36)
 Loading: HS-20-44



TOTAL BILL OF MATERIALS

Item	Unit	Super	Sub	Total
Class A Excavation for Structures	Cu. Yd.		361	361
Rock Excavation for Structures	Cu. Yd.		27	27
Class X Concrete	Cu. Yd.	136.0	594.8	1030.8
Evaluating Erecting Structural Steel	Lb.	608,970		608,970
Reinforcement Bars	Lb.	118,500	57,770	176,270
Pre-Cast Piles up to 20ft	Lin. Ft.		720	720
Driving Timber Piles	Lin. Ft.		720	720
Furnishing Steel Piles 108P42	Lin. Ft.		1040	1040
Test Piles Steel 108P42	Each		2	2
Driving Steel Piles	Lin. Ft.		1040	1040
Name Plates	Each		2	2
Slapewall A	Sq. Yd.		1134	1134
Aluminum Handrail	Lin. Ft.		483	483
Bridge Seat Sealant	Linear Yd.		0.1	0.1
Protective Coat	Sq. Yd.		1880	1880

FOR INFORMATION ONLY

DESIGNED	G.M.N.
CHECKED	C.S.
DRAWN	G.M.N.
CHECKED	H.S.B.

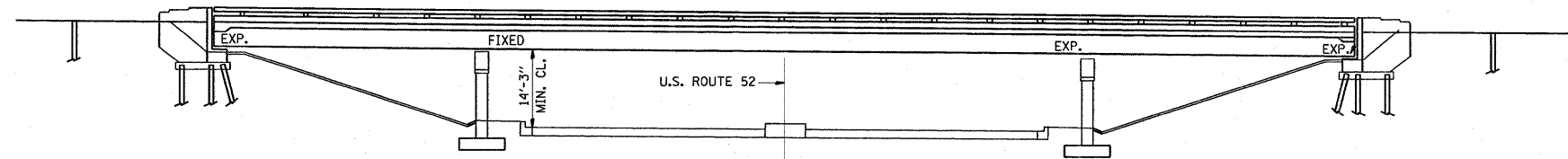
Prepared and recommended by Blauvelt Engineering Co. Structural Engineer #81-2251
 J. A. Wauguard

GENERAL PLAN & ELEVATION
FA 61 OVER
M'DONOUGH STREET

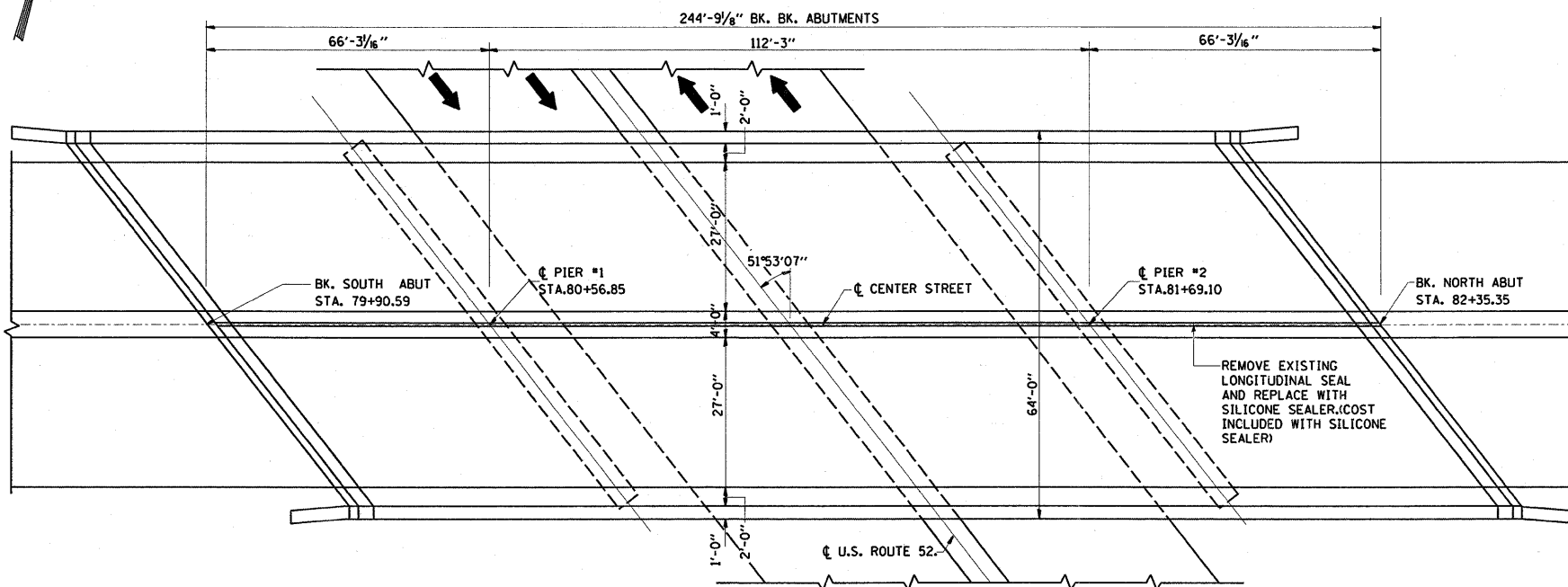
FA ROUTE 61 SECTION 10 PROJECT U-184(2)
 STA. 81+12.97 WILL COUNTY
 STR. 7 DATE NOV 30, 1961
 BLAUVELT ENGINEERING CO. CONSULTING ENGINEERS
 WOODBURY, N. J. NEW YORK, N. Y. CRYSTAL LAKE, ILL.

Revised: Change lang. of Dr. Piles from 22" to 28"
 2-17-65 Change Furn. & driving Cr. Timber Piles from 118 to 720 lin. Ft.

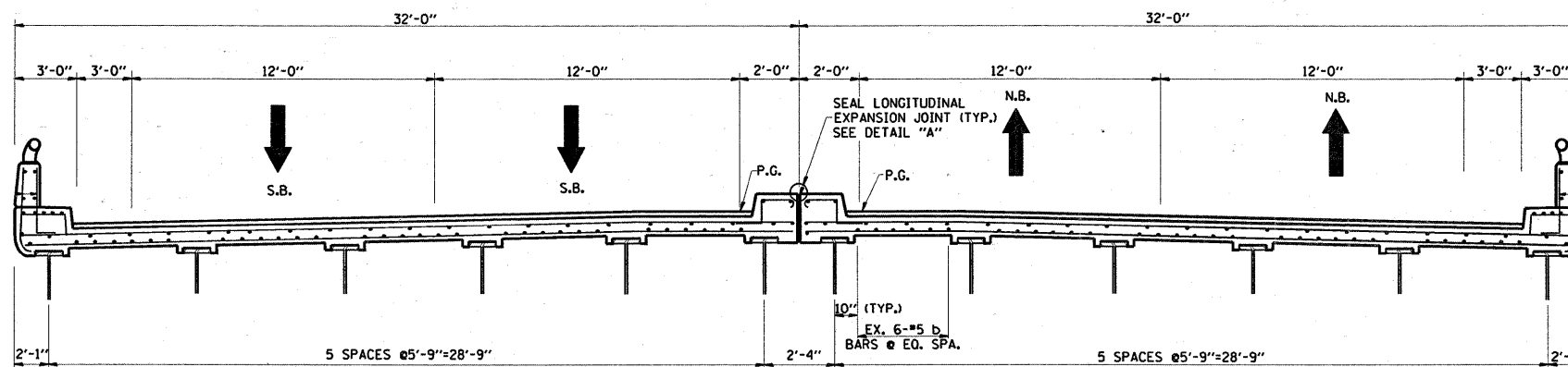
Rev. 12-19-62 Proj. Cost - 1880 Sq. Yd. 37K
 12-1-64 J.M.J. Rev. class X conc. 1030.4 to 1030.0 cu yds, rein bars from 168,170 to 176,270
 Slapewall from 1134 to 1134 sq. yd. added bridge seat sealant.



ELEVATION



PLAN

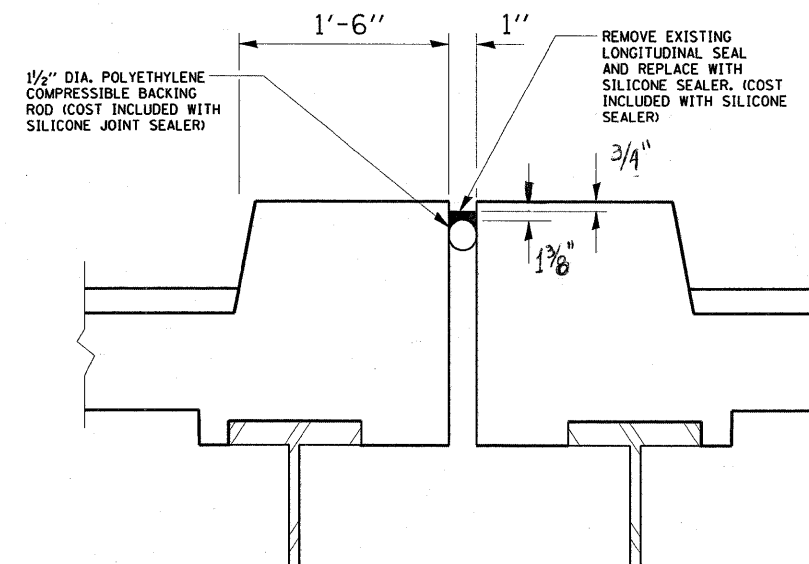


TYPICAL CROSS SECTION

NOTE: EXISTING REINFORCEMENT IS SHOWN FOR INFORMATIONAL PURPOSES ONLY

GENERAL NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED UPON THE UNIT PRICE BID FOR THE WORK.



DETAIL "A"

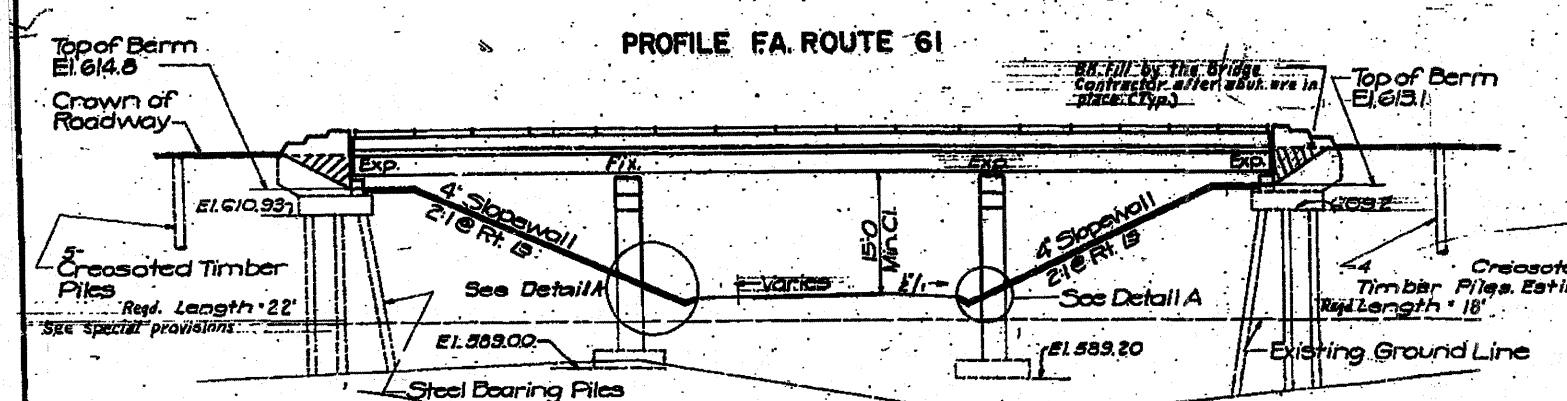
TOTAL BILL OF MATERIALS

ITEM	UNIT	QUANTITY
SILICONE JOINT SEALER, 1"	FOOT	245

Bench Mark No. 468. Top of "0" in open fire hydrant NE corner Hunter St. & Jasper St. El. 599.66
No Existing Structure.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

DATE	BY	CHKD	APP'D	SHEET NO.
10/24/61	J.M.J.	WILL	197	125
PROJECT U-184-211				10 SHEETS



Design Stresses
Reinforced Concrete
fc = 3500 P.S.I.
fs = 20000 P.S.I.
n = 10
fc = 1400 P.S.I. (except footings)
fs = 10000 P.S.I. (footings)
fc = 90 (except footings)
fs = 75 (footings)

Structural Steel
fs = 20000 P.S.I.
Max. Foundation Pressure = 3 1/2 Tons/S.F.
Loading: HS-20-44

GENERAL NOTES

STRUCTURAL STEEL SHALL CONFORM TO THE SPECIFICATIONS FOR STRUCTURAL STEEL, A.S.T.M. DESIGNATION A36.

RIVETS SHALL BE 3/4" WITH 13/16" OPEN HOLES UNLESS NOTED.

CLASS X CONCRETE SHALL BE USED THROUGHOUT. COARSE AGGREGATE USED IN FANAPERS AND RIB POSTS SHALL BE FREE OF CHERT, FLINT, LIMONITE, LIGHTS AND SOFT SANDSTONE.

THE CONCRETE FLOOR SHALL BE FINISHED IN ACCORDANCE WITH ARTICLE 51.19 OF THE STANDARD SPECIFICATIONS.

ALL WELDING SHALL COMPLY WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR WELDED HIGHWAY AND RAILWAY BRIDGES, OF THE AMERICAN WELDING SOCIETY.

ALL ROCKERS, BOLSTERS, BEARING PLATES, LEAD PLATES, PIPES, AND ANCHOR BOLTS SHALL BE FABRICATED AND SET IN ACCORDANCE WITH ARTICLE 51.15 OF THE STANDARD SPECIFICATIONS AND ARE INCLUDED IN QUANTITY OF STRUCTURAL STEEL.

ANCHOR BOLTS SHALL BE SET BEFORE CONNECTING DIAPHRAGMS OVER SUPPORT. SPACES RESPONDING TO MISS ANCHOR BOLTS.

EXPANSION GUARDS AND PLATES SHALL BE FABRICATED AND ERECTED IN ACCORDANCE WITH ARTICLE 51.13 (d) OF THE STANDARD SPECIFICATIONS AND ARE INCLUDED IN QUANTITY OF STRUCTURAL STEEL.

EXCEPT AS OTHERWISE PROVIDED, ALL STRUCTURAL STEEL SHALL RECEIVE ONE SHOP COAT OF RED LEAD PAINT AND TWO FIELD COATS OF ALUMINUM PAINT. SEE ARTICLES 56.1 TO 56.5 INCLUSIVE OF THE STANDARD SPECIFICATIONS.

ALL SURFACES OF EXPANSION GUARDS IMACCESSIBLE AFTER ERECTION SHALL BE GIVEN TWO SHOP COATS OF RED LEAD PAINT. THE 3/4" WELDED SEAMS SHALL NOT BE PAINTED.

THE CONTRACTOR SHALL DRIVE TWO (2) TEST PILES IN PERMANENT LOCATIONS AS SHOWN ON THE PLANS, BEFORE ORDERING REMAINDER OF PILES, WHERE PILES ARE TO BE DRIVEN.

NO ROCK LARGER THAN 3" SHALL BE PLACED IN FILLS IN THE AREA WHERE PILES ARE TO BE DRIVEN.

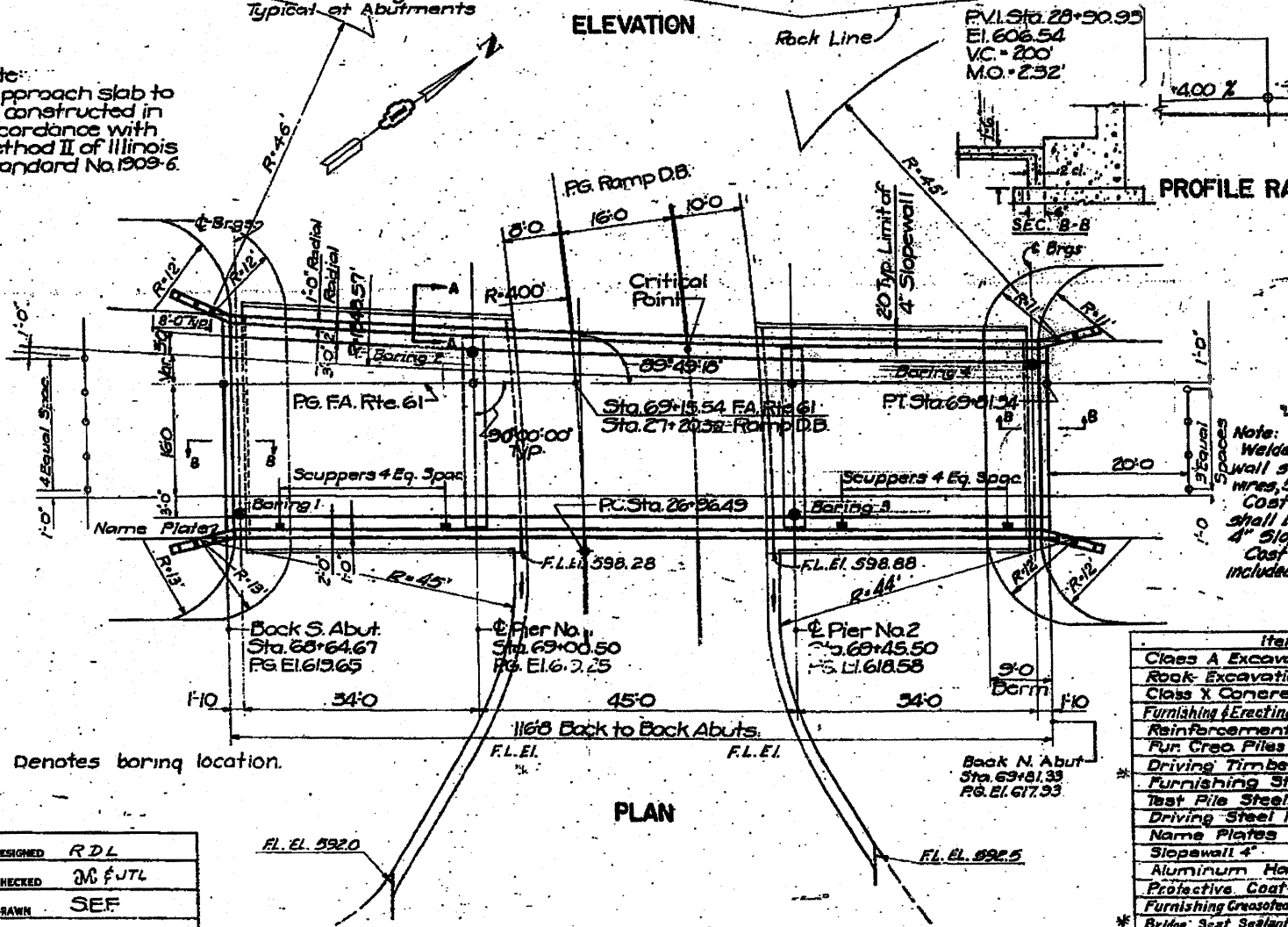
Excavation for portions of structure in the embankments shall not be classified.

ALL STEEL BEARING PILES ARE TO BE DRIVEN TO REFUSAL.

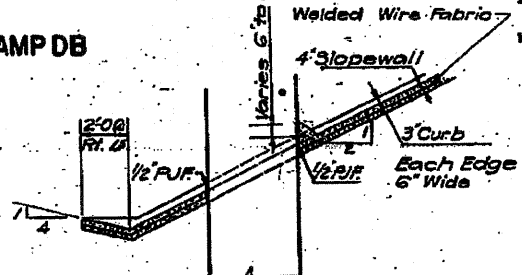
PERMANENT FORMS WILL NOT BE PERMITTED IN FORMING THE CONCRETE FLOOR.

STATION 69+15.64
BUILT 1961 BY
STATE OF ILLINOIS
FA. RT. 61 SEC. 10/18-1
FA. PROJ. U-184-211
LOADING HS-20

Note: Approach slab to be constructed in accordance with Method II of Illinois Standard No. 1909-6.



LETTERING FOR NAME PLATE

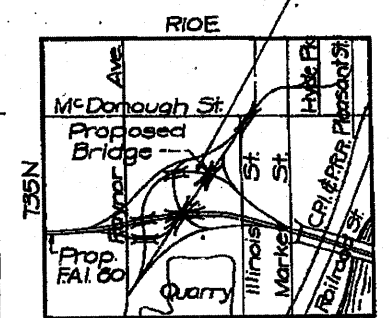


Note: Welded Wire Fabric in Slopewall shall be 6"x6" mesh #4 wires, 58 lbs./100'. Cost of welded wire fabric shall be included in cost of 4' Slopewall. Cost of cutoff wall shall be included in cost of 4' Slopewall.

BILL OF MATERIALS
Sec. 10 C 311.67

Item	Unit	Super	Sub	Total
Class A Excavation for Structures	Cu. Yd.		120	120
Rock Excavation for Structures	Cu. Yd.		6	6
Class X Concrete	Cu. Yd.	108.7	170.0	278.7
Furnishing & Erecting Structural Steel	Lb.	63900		63900
Reinforcement Bars	Lb.	24,370	13850	38220
Run Curo Piles 20.1 to 38 feet	Lin. Ft.		110	110
Driving Timber Piles	Lin. Ft.		182	182
Furnishing Steel Piles 10BP42	Lin. Ft.		490	490
Test Pile Steel 10BP42	Each		2	2
Driving Steel Piles	Lin. Ft.		490	490
Name Plates	Each		1	1
Slopewall 4'	Sq. Yd.		291	291
Aluminum Handrail	Lin. Ft.		229	229
Protective Coat	Sq. Yd.		436	436
Furnishing Crossoed Piles up to 20 ft	Lin. Ft.		72	72
Bridge Seat Sealant	Lump Sum		0.1	0.1

Note: This route has been selected as a 15'-0" vertical clearance route.



SW 1/4 Sec. 14
LOCATION PLAN
GENERAL PLAN & ELEVATION
FA. ROUTE 61 OVER
RAMP DB
FA. ROUTE 61 STA. 69+15.64
PROJECT U-184-211
SECTION 10 (S116)
WILL COUNTY
Scale: NO SCALE Date: Nov. 30, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY, N. J. NEW YORK, N. Y. CHRYSTAL LAKE, ILL.

FOR INFORMATION ONLY

DESIGNED	RDL
CHECKED	J.C.F.T.L.
DRAWN	SEF
CHECKED	J.C.F.T.L.

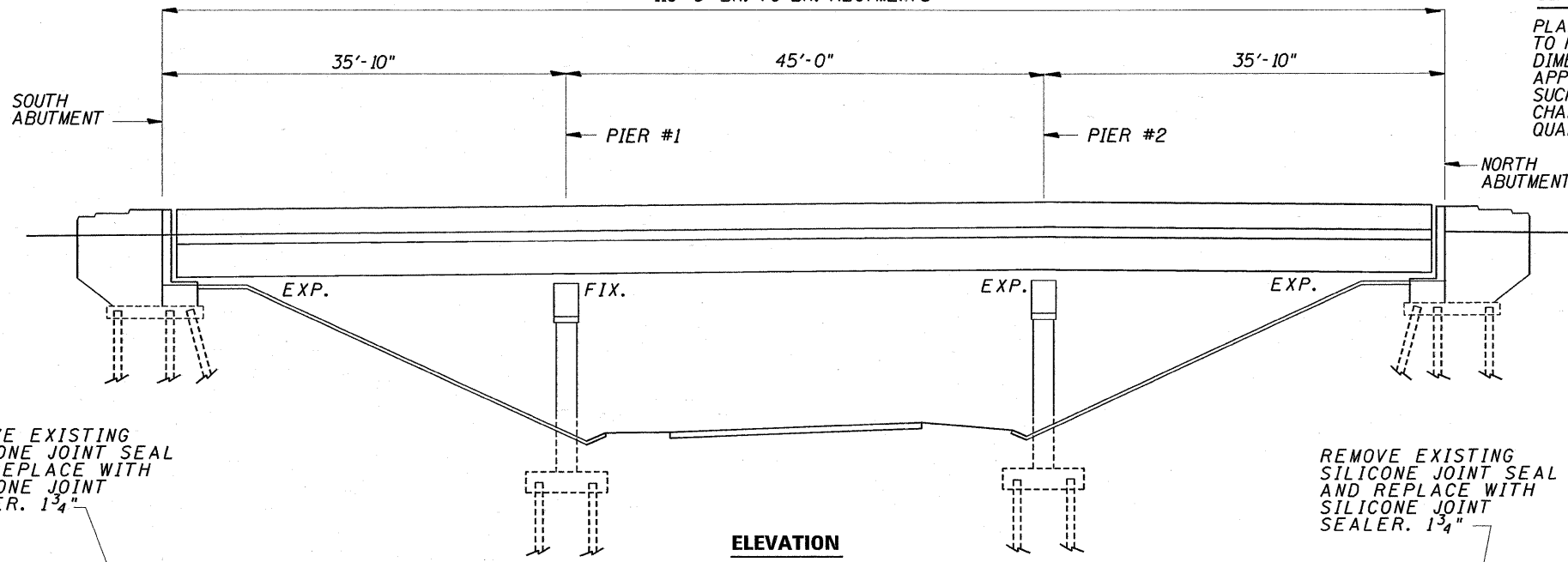
Prepared and Recommended by
Blauvelt Engineering Co.
Structural Engineer #1-2251 Russ. H. Wengert

Revised: Revise Bill of Mat'l: Curo Piles from 261 to 110 lin. ft.; Driving Tim. Piles: 261 to 182 lin. ft.; Add: Curo Piles 160' 72 lin. ft.

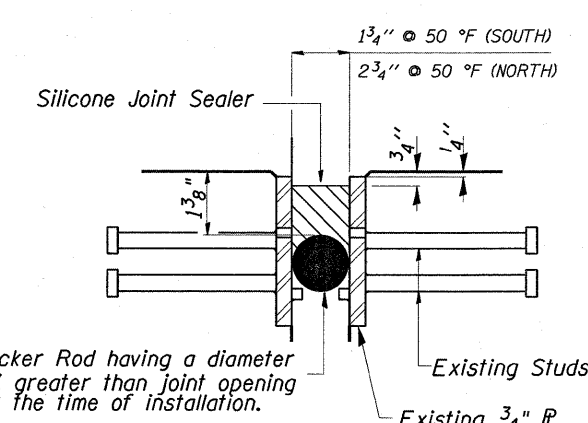
116'-8" BK. TO BK. ABUTMENTS

GENERAL NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. THE CONTRACTOR SHALL VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED UPON THE UNIT PRICE BID FOR THE WORK.



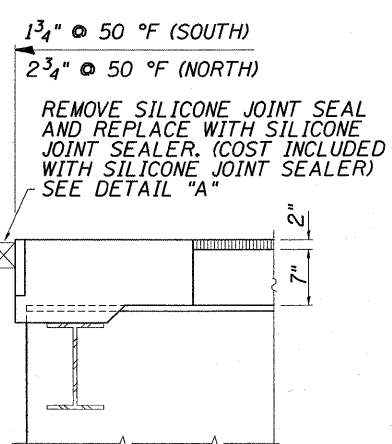
ELEVATION



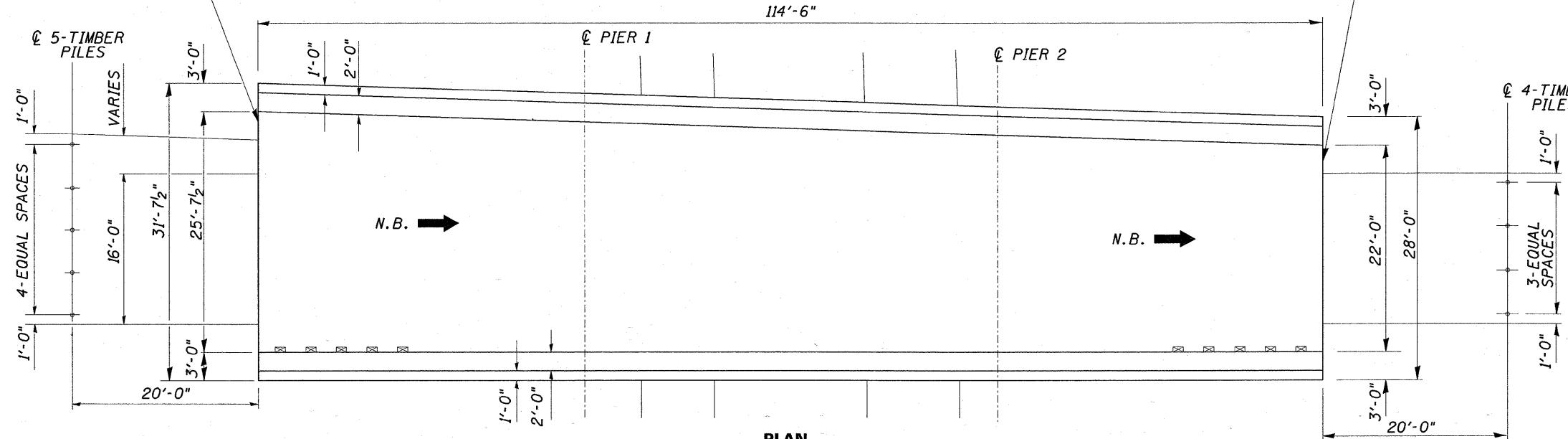
Backer Rod having a diameter 25% greater than joint opening at the time of installation.

REMOVE EXISTING SILICONE JOINT SEAL AND REPLACE WITH SILICONE JOINT SEALER. 1 3/4"

REMOVE EXISTING SILICONE JOINT SEAL AND REPLACE WITH SILICONE JOINT SEALER. 1 3/4"



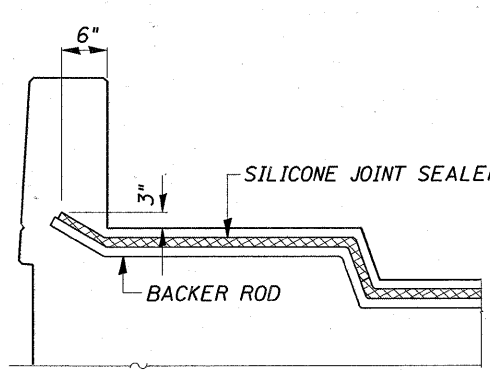
EXISTING TRANSVERSE JOINT NORTH AND SOUTH ABUTMENT



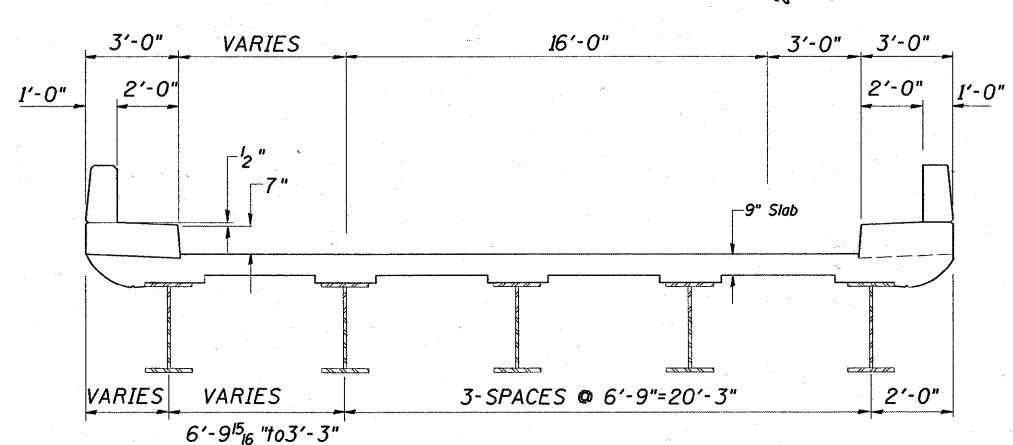
PLAN

TOTAL BILL OF MATERIALS

ITEM	UNIT	QUANTITY
SILICONE JOINT SEALER, 1 3/4"	Foot	32
SILICONE JOINT SEALER, 2 3/4"	Foot	28



END OF SEAL TREATMENT



CROSS SECTION

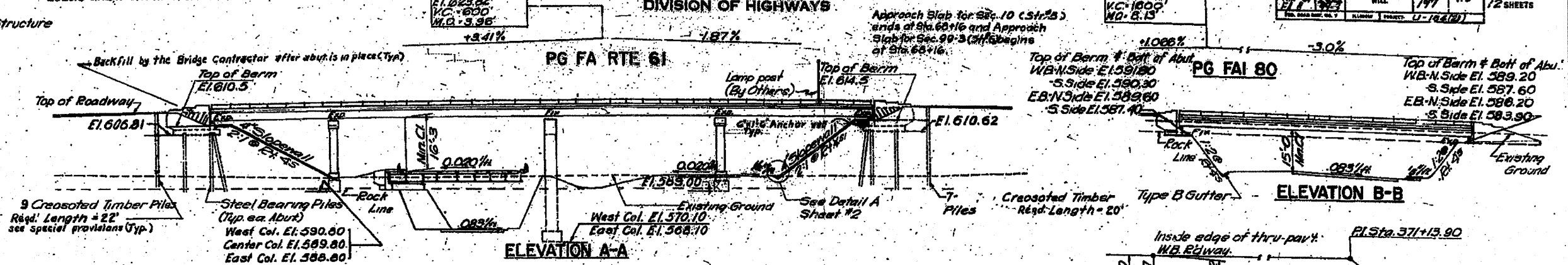
Bench Mark No. 44A: 22.5pk. in power pole NW Corner Lucas and Hunter Sts. Elev. 390.454

No Existing Structure

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

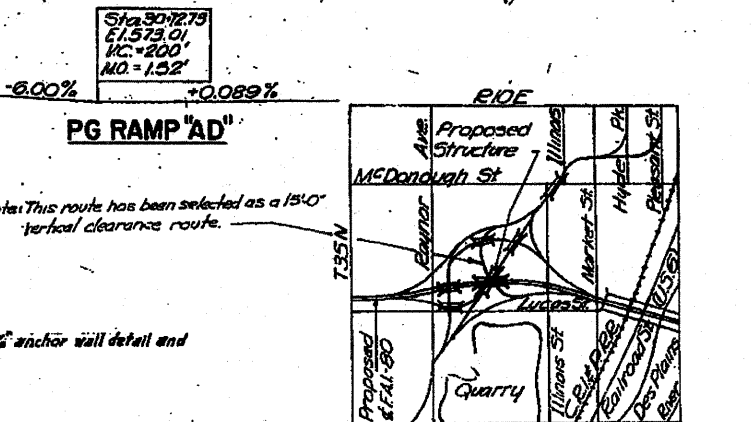
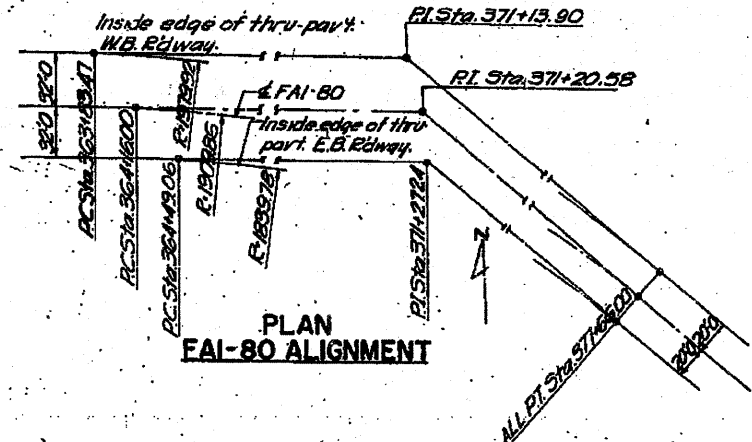
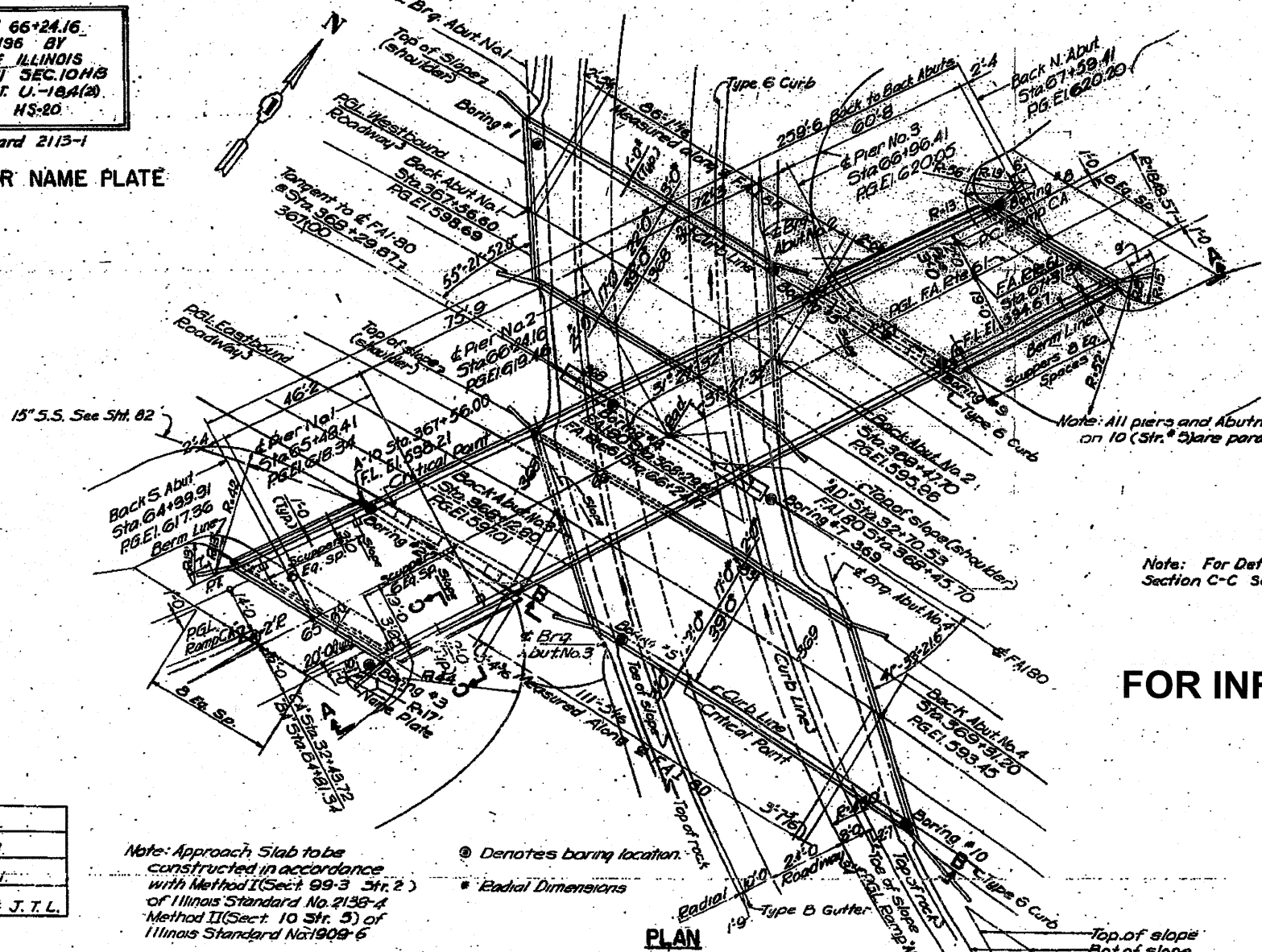
RI STA. 371+80
E.I. 625.00
V.C. 1000
M.O. 8.15

DATE	NO.	COUNTY	SECTION	SHEET NO.
1/97	113	WILL	197	12 SHEETS



STATION 66+24.16
BUILT 196 BY
STATE OF ILLINOIS
F.A. RTE. 61 SEC. 10 NB
F.A. PROJ. U-184(2)
LOADING HS-20
See Standard 2113-1

LETTERING FOR NAME PLATE



FOR INFORMATION ONLY

DESIGNED	C. S.
CHECKED	R. H.
DRAWN	G. N.
CHECKED	R. H. & J. T. L.

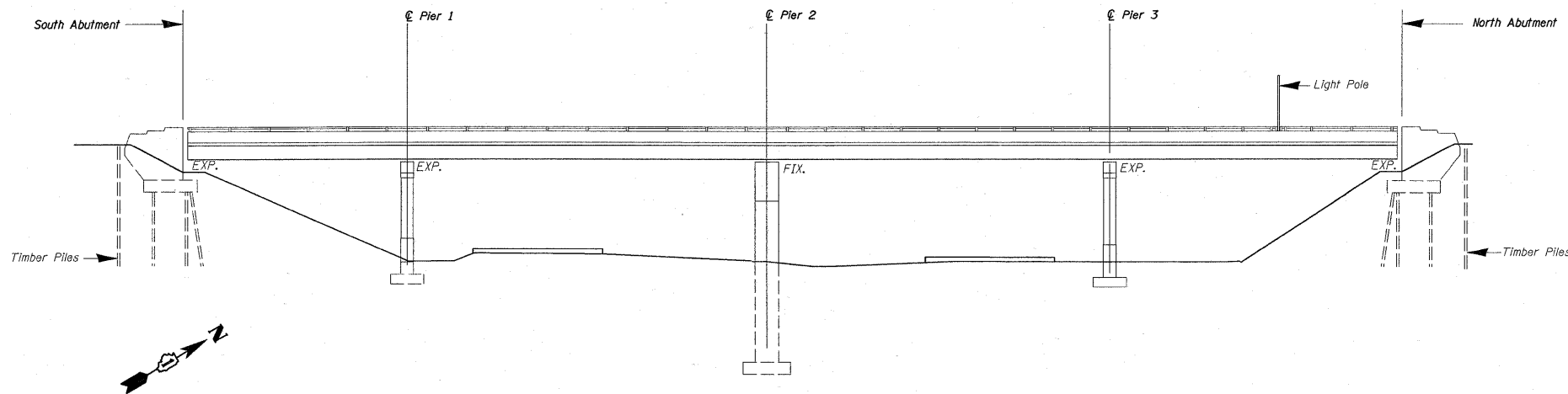
Note: Approach Slab to be constructed in accordance with Method I (Sect. 99-3 Str. 2) of Illinois Standard No. 2135-4 Method II (Sect. 10 Str. 5) of Illinois Standard No. 1909-6

Prepared and Recommended By: Blauvelt Engineering Co. Structural Engineer # 81-2251
 Renee H. Wengert

**GENERAL PLAN & ELEVATION
 F.A. ROUTE 61 OVER F.A.I. ROUTE 80
 F.A. RTE. 61 STA. 66+24.16**
 F.A. ROUTE 61 SECTION 10 (STR. 5)
 PROJECT U-184(21) WILL COUNTY
 DATE NOV. 30, 1981
 BLAUVELT ENGINEERING CO. CONSULTING ENGINEERS
 WOODBURY, N. J. NEW YORK, N. Y. CRYSTAL LAKE, ILL.

Revised: Change length of 18 Ton Cross Timber Piles from 24' to 30' & 10' V.D.

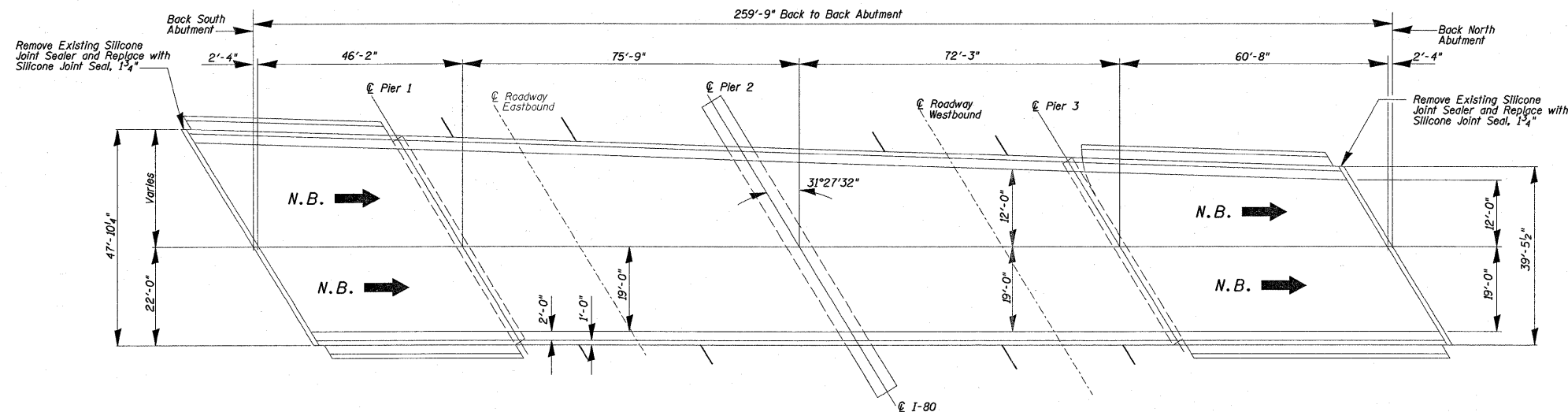
FILE NAME =	USER NAME = tmtakosmv	DESIGNED -	REVISED -	CENTER ST. NB OVER I-80	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\TINIAKOSMV\d0101671\18-01.dgn		DRAWN -	REVISED -	GENERAL PLAN, ELEVATION AND CROSS SECTION	316	(10&1-D-2) RS-4	WILL	36	18
		CHECKED -	REVISED -	SN 099-0188					
		DRAWN -	REVISED -	SCALE:	SHEET NO. 1 OF 2 SHEETS	STA.	TO STA.		
		CHECKED -	REVISED -						ILLINOIS FED. AID PROJECT
		DATE	REVISED						CONTRACT NO. 62778



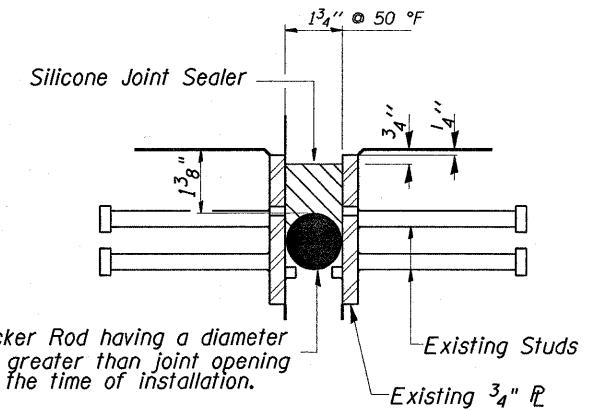
ELEVATION

GENERAL NOTES

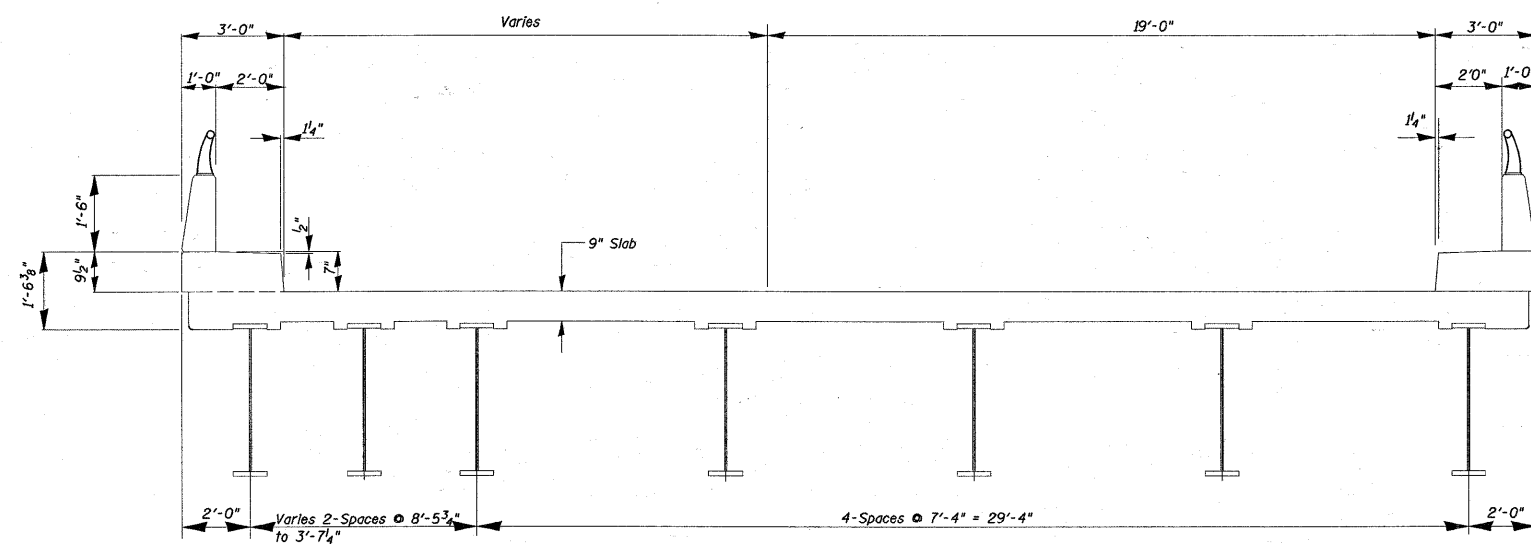
PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED UPON THE UNIT PRICE BID FOR THE WORK.



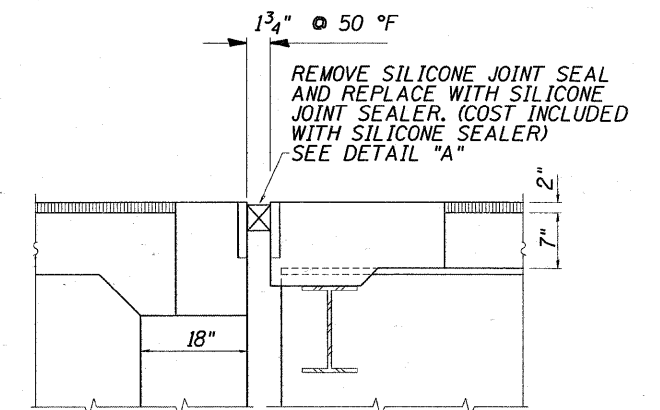
PLAN



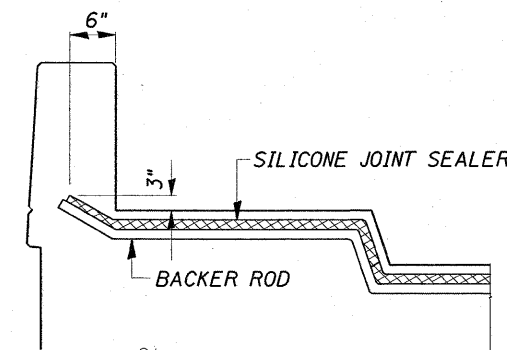
DETAIL "A"



CROSS SECTION



EXISTING TRANSVERSE JOINT NORTH AND SOUTH ABUTMENT



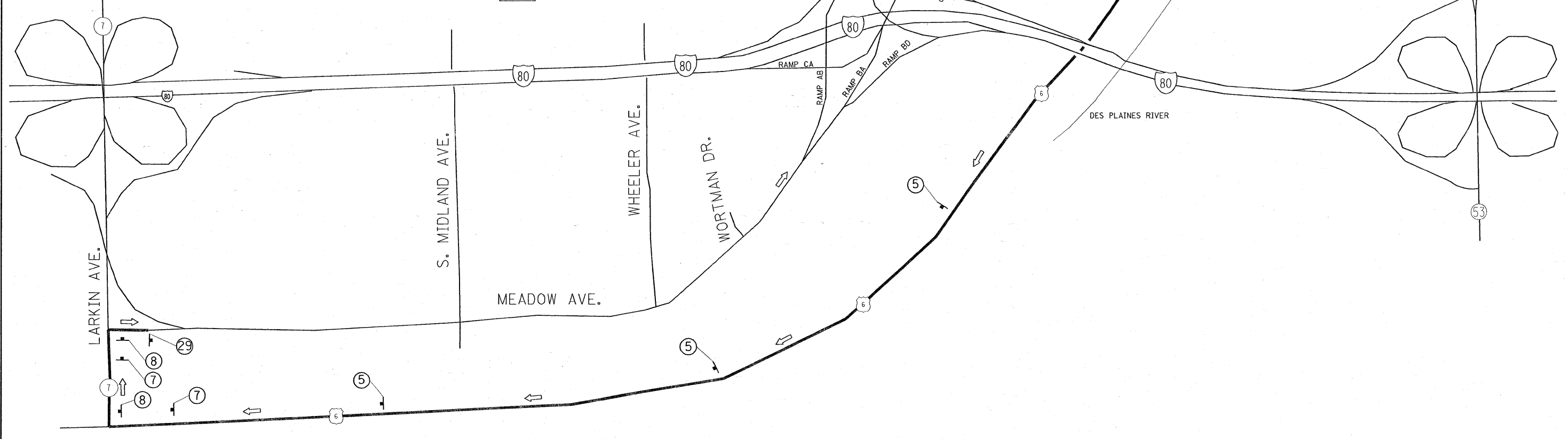
END OF SEAL TREATMENT

TOTAL BILL OF MATERIALS

ITEM	UNIT	QUANTITY
SILICONE JOINT SEALER, 1 3/4"	FOOT	102

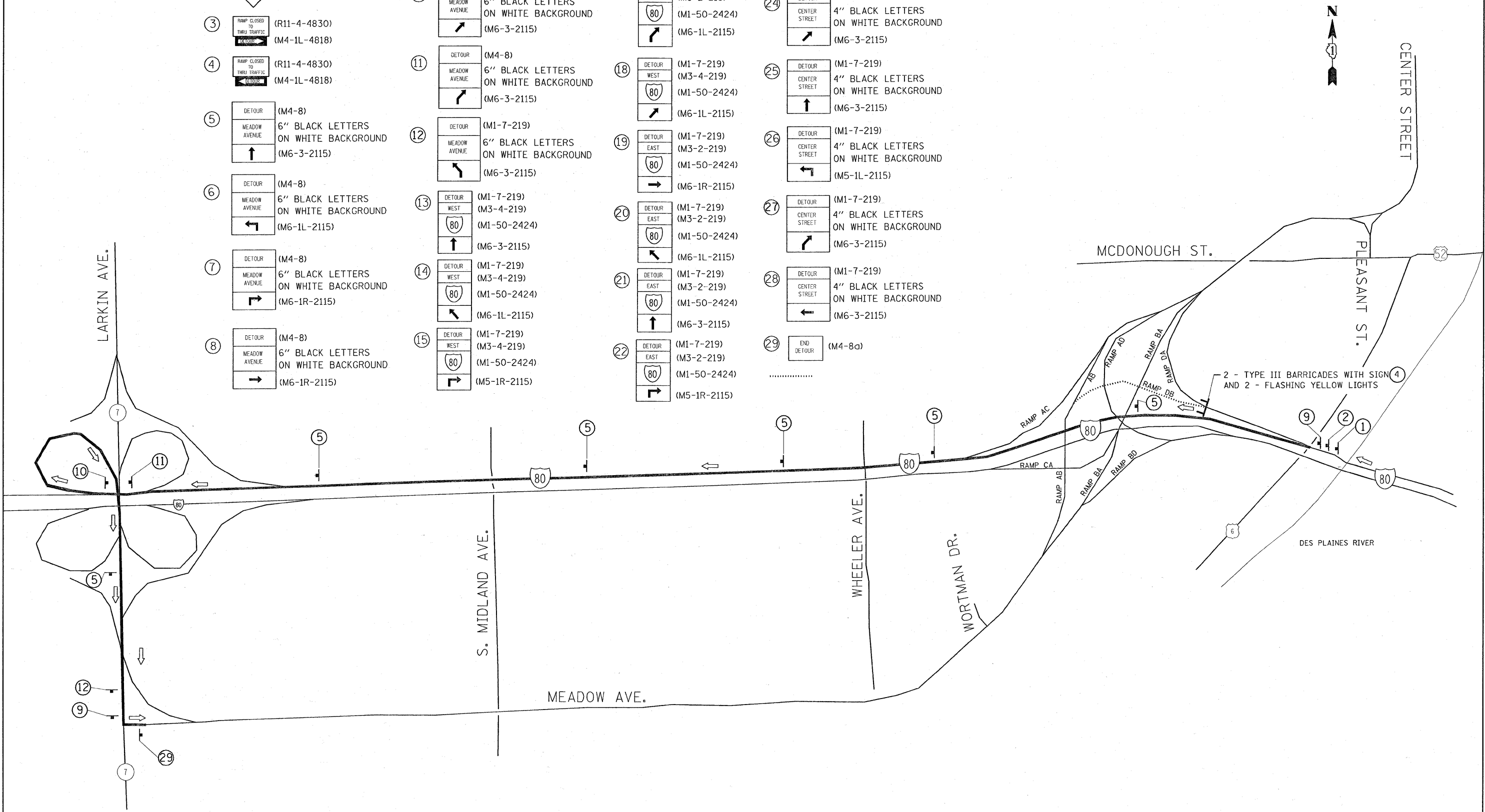
- ① RAMP CLOSED AHEAD (W 20-2)
- ② DETOUR AHEAD (W 20-2)
- ③ RAMP CLOSED TO THRU TRAFFIC (R11-4-4830) (M4-1L-4818)
- ④ RAMP CLOSED TO THRU TRAFFIC (R11-4-4830) (M4-1L-4818)
- ⑤ DETOUR (M4-8) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ⑥ DETOUR (M4-8) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-1L-2115)
- ⑦ DETOUR (M4-8) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-1R-2115)
- ⑧ DETOUR (M4-8) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-1R-2115)
- ⑨ DETOUR (M1-7-219) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-1L-2115)
- ⑩ DETOUR (M4-8) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ⑪ DETOUR (M4-8) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ⑫ DETOUR (M1-7-219) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ⑬ DETOUR WEST (M1-7-219) (M3-4-219) (M1-50-2424) (M6-3-2115)
- ⑭ DETOUR WEST (M1-7-219) (M3-4-219) (M1-50-2424) (M6-1L-2115)
- ⑮ DETOUR WEST (M1-7-219) (M3-4-219) (M1-50-2424) (M5-1R-2115)
- ⑯ DETOUR WEST (M1-7-219) (M3-4-219) (M1-50-2424) (M6-1L-2115)
- ⑰ DETOUR WEST (M1-7-219) (M3-2-219) (M1-50-2424) (M6-1L-2115)
- ⑱ DETOUR EAST (M1-7-219) (M3-2-219) (M1-50-2424) (M6-1R-2115)
- ⑲ DETOUR EAST (M1-7-219) (M3-2-219) (M1-50-2424) (M6-1L-2115)
- ⑳ DETOUR EAST (M1-7-219) (M3-2-219) (M1-50-2424) (M6-1L-2115)
- ㉑ DETOUR EAST (M1-7-219) (M3-2-219) (M1-50-2424) (M6-3-2115)
- ㉒ DETOUR EAST (M1-7-219) (M3-2-219) (M1-50-2424) (M5-1R-2115)
- ㉓ DETOUR EAST (M1-7-219) (M3-2-219) (M1-50-2424) (M5-1R-2115)
- ㉔ DETOUR (M1-7-219) 4" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ㉕ DETOUR (M1-7-219) 4" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ㉖ DETOUR (M1-7-219) 4" BLACK LETTERS ON WHITE BACKGROUND (M5-1L-2115)
- ㉗ DETOUR (M1-7-219) 4" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ㉘ DETOUR (M1-7-219) 4" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ㉙ END DETOUR (M4-8a)

..... RAMP CLOSED



FILE NAME =	USER NAME = aqeelff	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETOUR PLAN FOR CLOSING RAMP AB		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
ca:\pwork\VPWID01\AQUEELFF\08120663\012	#504-design.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO. 1 OF 5 SHEETS	STA.	TO STA.	316	(10&1-D-2)RS-4	WILL	36	20
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED -		CONTRACT NO. 62778								
	PLOT DATE = 3/21/2009	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

- ① RAMP CLOSED AHEAD (W 20-2)
- ② DETOUR AHEAD (W 20-2)
- ③ RAMP CLOSED TO THRU TRAFFIC (R11-4-4830)
M4-1L-4818
- ④ RAMP CLOSED TO THRU TRAFFIC (R11-4-4830)
M4-1L-4818
- ⑤ DETOUR (M4-8)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-3-2115
- ⑥ DETOUR (M4-8)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-1L-2115
- ⑦ DETOUR (M4-8)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-1R-2115
- ⑧ DETOUR (M4-8)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-1R-2115
- ⑨ DETOUR (M1-7-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-1L-2115
- ⑩ DETOUR (M4-8)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-3-2115
- ⑪ DETOUR (M4-8)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-3-2115
- ⑫ DETOUR (M1-7-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-3-2115
- ⑬ DETOUR (M1-7-219)
WEST (M3-4-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-3-2115
- ⑭ DETOUR (M1-7-219)
WEST (M3-4-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-1L-2115
- ⑮ DETOUR (M1-7-219)
WEST (M3-4-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M5-1R-2115
- ⑯ DETOUR (M1-7-219)
WEST (M3-4-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-1L-2115
- ⑰ DETOUR (M1-7-219)
WEST (M3-2-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-1L-2115
- ⑱ DETOUR (M1-7-219)
WEST (M3-2-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-1L-2115
- ⑲ DETOUR (M1-7-219)
EAST (M3-2-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-1R-2115
- ⑳ DETOUR (M1-7-219)
EAST (M3-2-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-1L-2115
- ㉑ DETOUR (M1-7-219)
EAST (M3-2-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-3-2115
- ㉒ DETOUR (M1-7-219)
EAST (M3-2-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M5-1R-2115
- ㉓ DETOUR (M1-7-219)
EAST (M3-2-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M5-1R-2115
- ㉔ DETOUR (M1-7-219)
EAST (M3-2-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-3-2115
- ㉕ DETOUR (M1-7-219)
EAST (M3-2-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-3-2115
- ㉖ DETOUR (M1-7-219)
EAST (M3-2-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M5-1L-2115
- ㉗ DETOUR (M1-7-219)
EAST (M3-2-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-3-2115
- ㉘ DETOUR (M1-7-219)
EAST (M3-2-219)
MEADOW AVENUE
6" BLACK LETTERS ON WHITE BACKGROUND
M6-3-2115
- ㉙ END DETOUR (M4-8a)



FILE NAME =	USER NAME = oqueelff	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETOUR PLAN FOR CLOSING RAMP DB		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
c:\pwork\FW100T\OQUEELFF\d0120663\0120664-design.dgn	DESIGNED -	REVISED -	REVISED -		SCALE:	SHEET NO. 3 OF 5 SHEETS	STA.	TO STA.	316	(10&1-D-2)RS-4	WILL	36	22
PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED -	REVISED -		CONTRACT NO. 62778								
PLOT DATE = 3/21/2009	DATE -	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT								

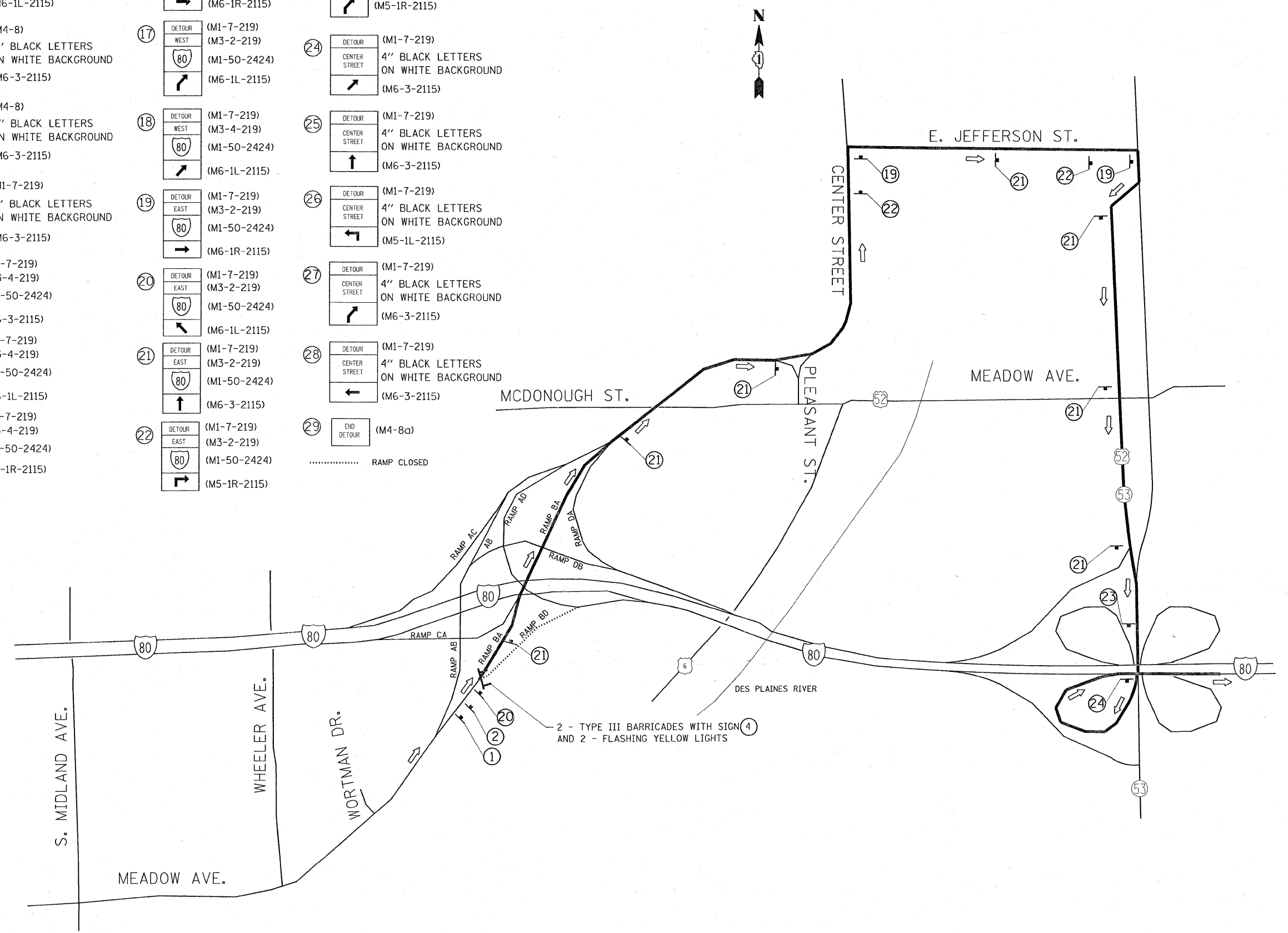
- ① RAMP CLOSED AHEAD (W 20-2)
- ② DETOUR AHEAD (W 20-2)
- ③ RAMP CLOSED TO THRU TRAFFIC (R11-4-4830) (M4-1L-4818)
- ④ RAMP CLOSED TO THRU TRAFFIC (R11-4-4830) (M4-1L-4818)
- ⑤ DETOUR (M4-8) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ⑥ DETOUR (M4-8) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-1L-2115)
- ⑦ DETOUR (M4-8) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-1R-2115)
- ⑧ DETOUR (M4-8) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-1R-2115)

- ⑨ DETOUR (M1-7-219) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-1L-2115)
- ⑩ DETOUR (M4-8) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ⑪ DETOUR (M4-8) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ⑫ DETOUR (M1-7-219) 6" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ⑬ DETOUR (M1-7-219) (M3-4-219) (M1-50-2424) (M6-3-2115)
- ⑭ DETOUR (M1-7-219) (M3-4-219) (M1-50-2424) (M6-1L-2115)
- ⑮ DETOUR (M1-7-219) (M3-4-219) (M1-50-2424) (M5-1R-2115)

- ⑯ DETOUR (M1-7-219) (M3-4-219) (M1-50-2424) (M6-1R-2115)
- ⑰ DETOUR (M1-7-219) (M3-2-219) (M1-50-2424) (M6-1L-2115)
- ⑱ DETOUR (M1-7-219) (M3-2-219) (M1-50-2424) (M6-1R-2115)
- ⑳ DETOUR (M1-7-219) (M3-2-219) (M1-50-2424) (M6-1L-2115)
- ㉑ DETOUR (M1-7-219) (M3-2-219) (M1-50-2424) (M6-3-2115)
- ㉒ DETOUR (M1-7-219) (M3-2-219) (M1-50-2424) (M5-1R-2115)

- ㉓ DETOUR (M1-7-219) (M3-2-219) (M1-50-2424) (M5-1R-2115)
- ㉔ DETOUR (M1-7-219) 4" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ㉕ DETOUR (M1-7-219) 4" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ㉖ DETOUR (M1-7-219) 4" BLACK LETTERS ON WHITE BACKGROUND (M5-1L-2115)
- ㉗ DETOUR (M1-7-219) 4" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ㉘ DETOUR (M1-7-219) 4" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ㉙ END DETOUR (M4-8a)

..... RAMP CLOSED

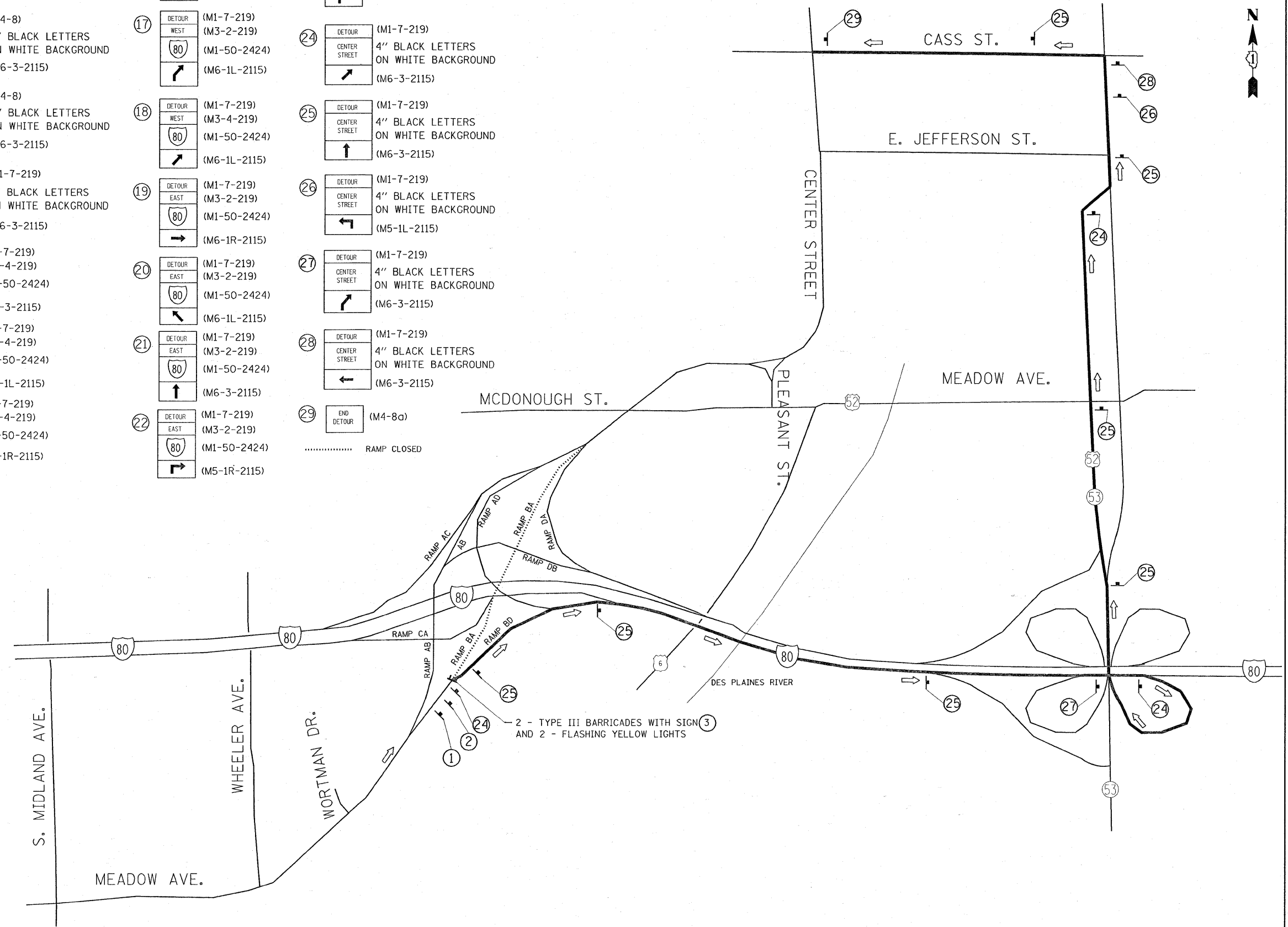


2 - TYPE III BARRICADES WITH SIGN ④ AND 2 - FLASHING YELLOW LIGHTS

FILE NAME =	USER NAME = aqeolff	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETOUR PLAN FOR CLOSING RAMP BD			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pw\work\PW1001\AQUEELFF\0120663\012	#504-design.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO. 4 OF 5 SHEETS	STA.	TO STA.	316	(10&1-D-2)RS-4	WILL	36	23
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED -		CONTRACT NO. 62778								
	PLOT DATE = 3/21/2009	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

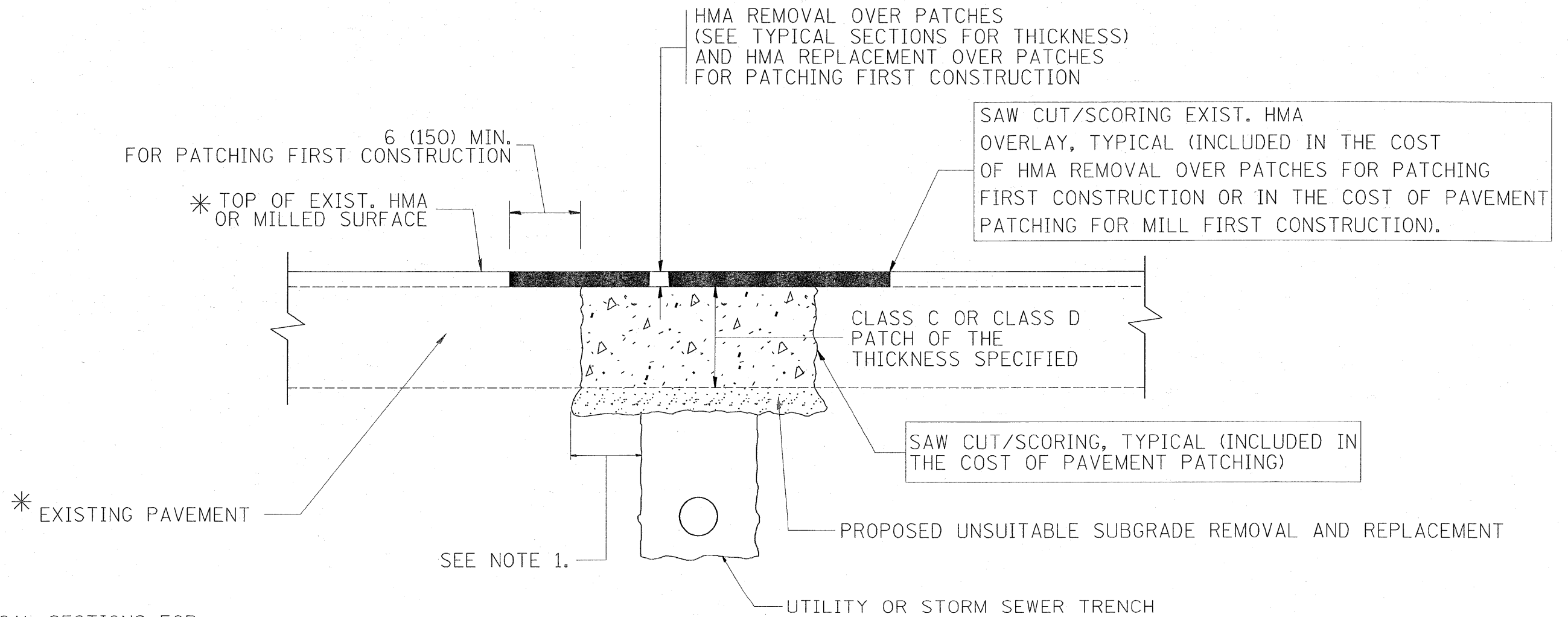
- ① RAMP CLOSED AHEAD (W 20-2)
- ② DETOUR AHEAD (W 20-2)
- ③ RAMP CLOSED TO THRU TRAFFIC (R11-4-4830) (M4-1L-4818)
- ④ RAMP CLOSED TO THRU TRAFFIC (R11-4-4830) (M4-1L-4818)
- ⑤ DETOUR (M4-8) MEADOW AVENUE 6" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ⑥ DETOUR (M4-8) MEADOW AVENUE 6" BLACK LETTERS ON WHITE BACKGROUND (M6-1L-2115)
- ⑦ DETOUR (M4-8) MEADOW AVENUE 6" BLACK LETTERS ON WHITE BACKGROUND (M6-1R-2115)
- ⑧ DETOUR (M4-8) MEADOW AVENUE 6" BLACK LETTERS ON WHITE BACKGROUND (M6-1R-2115)
- ⑨ DETOUR (M1-7-219) MEADOW AVENUE 6" BLACK LETTERS ON WHITE BACKGROUND (M6-1L-2115)
- ⑩ DETOUR (M4-8) MEADOW AVENUE 6" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ⑪ DETOUR (M4-8) MEADOW AVENUE 6" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ⑫ DETOUR (M1-7-219) MEADOW AVENUE 6" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ⑬ DETOUR (M1-7-219) WEST (M3-4-219) (M1-50-2424) (M6-3-2115)
- ⑭ DETOUR (M1-7-219) WEST (M3-4-219) (M1-50-2424) (M6-1L-2115)
- ⑮ DETOUR (M1-7-219) WEST (M3-4-219) (M1-50-2424) (M5-1R-2115)
- ⑯ DETOUR (M1-7-219) WEST (M3-4-219) (M1-50-2424) (M6-1L-2115)
- ⑰ DETOUR (M1-7-219) WEST (M3-2-219) (M1-50-2424) (M6-1L-2115)
- ⑱ DETOUR (M1-7-219) WEST (M3-4-219) (M1-50-2424) (M6-1L-2115)
- ⑲ DETOUR (M1-7-219) EAST (M3-2-219) (M1-50-2424) (M6-1R-2115)
- ⑳ DETOUR (M1-7-219) EAST (M3-2-219) (M1-50-2424) (M6-1L-2115)
- ㉑ DETOUR (M1-7-219) EAST (M3-2-219) (M1-50-2424) (M6-3-2115)
- ㉒ DETOUR (M1-7-219) EAST (M3-2-219) (M1-50-2424) (M5-1R-2115)
- ㉓ DETOUR (M1-7-219) EAST (M3-2-219) (M1-50-2424) (M5-1R-2115)
- ㉔ DETOUR (M1-7-219) CENTER STREET 4" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ㉕ DETOUR (M1-7-219) CENTER STREET 4" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ㉖ DETOUR (M1-7-219) CENTER STREET 4" BLACK LETTERS ON WHITE BACKGROUND (M5-1L-2115)
- ㉗ DETOUR (M1-7-219) CENTER STREET 4" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ㉘ DETOUR (M1-7-219) CENTER STREET 4" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ㉙ DETOUR (M1-7-219) CENTER STREET 4" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ㉚ DETOUR (M1-7-219) CENTER STREET 4" BLACK LETTERS ON WHITE BACKGROUND (M6-3-2115)
- ㉛ END DETOUR (M4-8a)

..... RAMP CLOSED



③ - 2 - TYPE III BARRICADES WITH SIGN AND 2 - FLASHING YELLOW LIGHTS

FILE NAME =	USER NAME = aqeelff	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETOUR PLAN FOR CLOSING RAMP BA		F.A.U. RTE. 316	SECTION (10&1-D-2)RS-4	COUNTY WILL	TOTAL SHEETS 36	SHEET NO. 24	
ca:\pw\work\PIWIDOT\AQEELFF\08120663\0124504-design.dgn	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED -		SCALE:	SHEET NO. 5 OF 5 SHEETS	STA. TO STA.	CONTRACT NO. 62778				
	PLOT DATE = 3/21/2009	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = equeiff	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\WIDOT\AQUEELFF\0120653\Dis	Std.dgn	DRAWN -	REVISED - R. BORO 01-01-07			316	(10&1-D-2)RS-4	WILL	36	25
	PLOT SCALE = 50,0000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07			BD400-04 (BD-22)		CONTRACT NO. 62778		
	PLOT DATE = 3/21/2009	DATE - 10-25-94	REVISED - K. ENG 10-27-08			FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT			
SCALE: NONE						SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SALT TOLERANT SOD AND TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

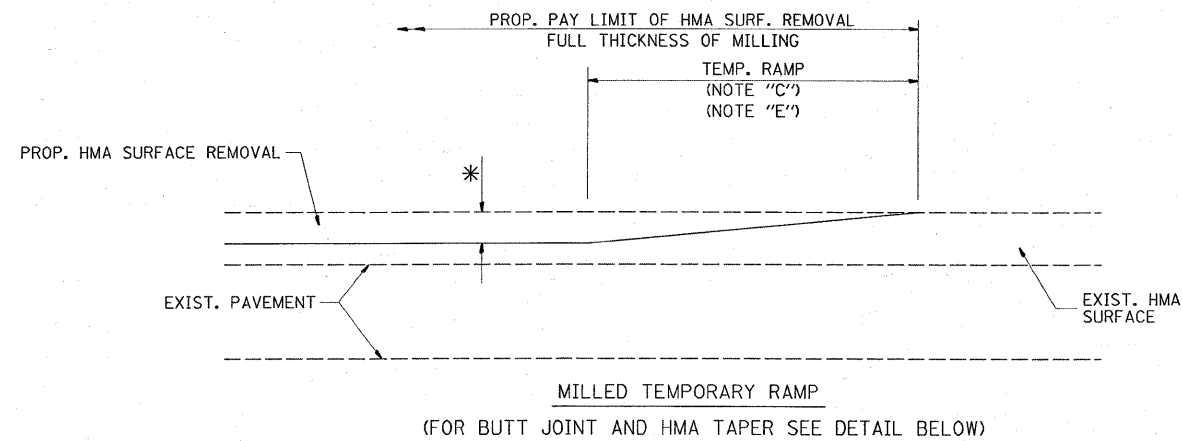
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

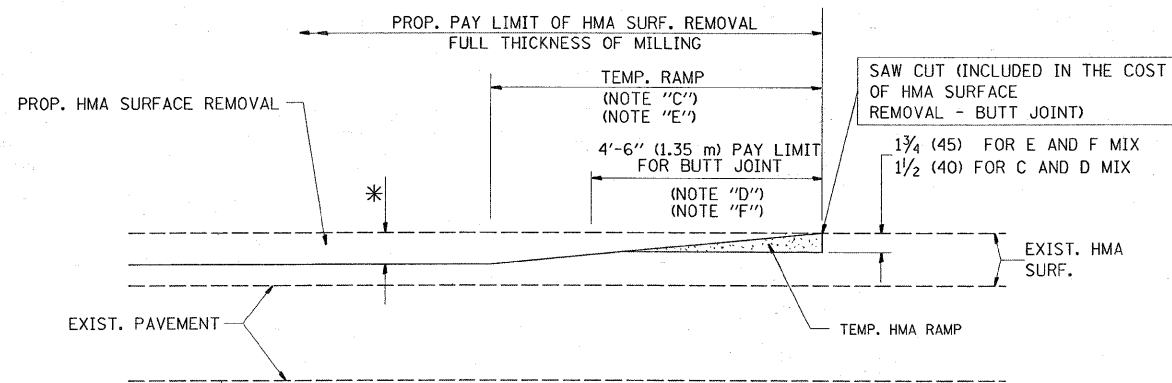
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

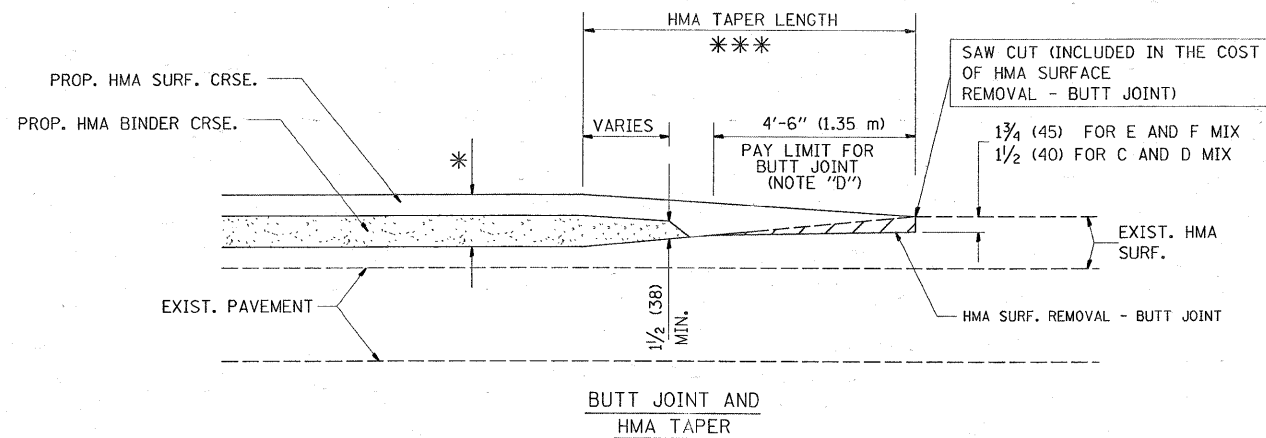
FILE NAME =	USER NAME = aqueslff	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pwork\pwork\1001\ADUEELFF\0120663\Di	Std.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97			316	(10&1-D-2)RS-4	WILL	36	26
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01			BD600-06 (BD-24)		CONTRACT NO. 62778		
	PLOT DATE = 3/21/2009	DATE - 03-11-94	REVISED - R. BORO 01-01-07			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



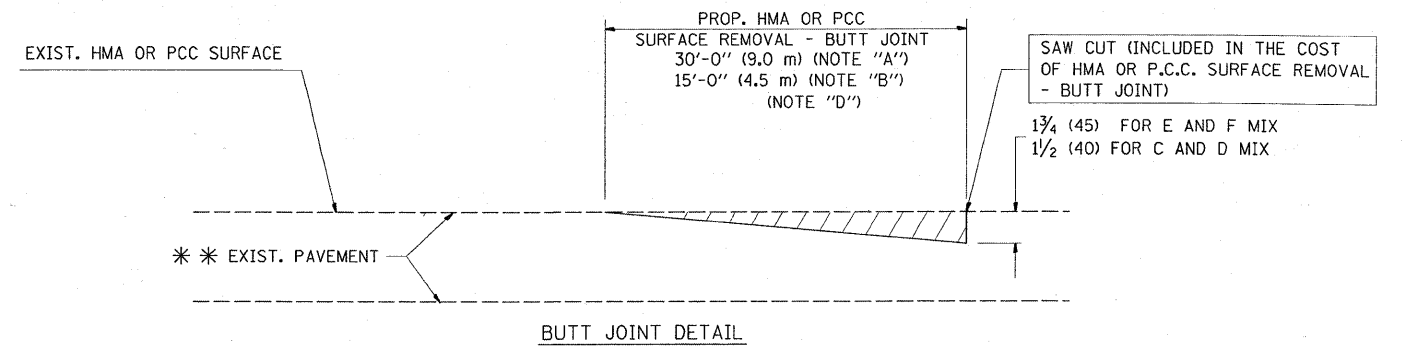
OPTION 1



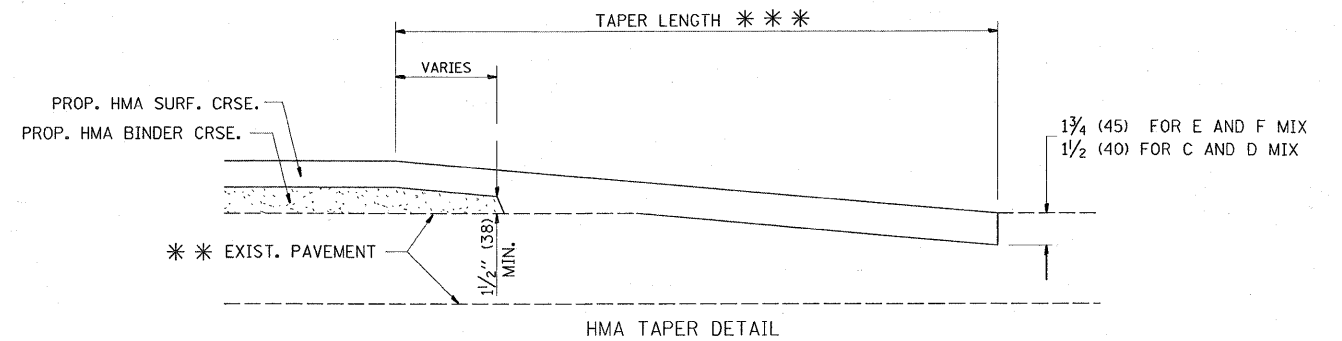
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

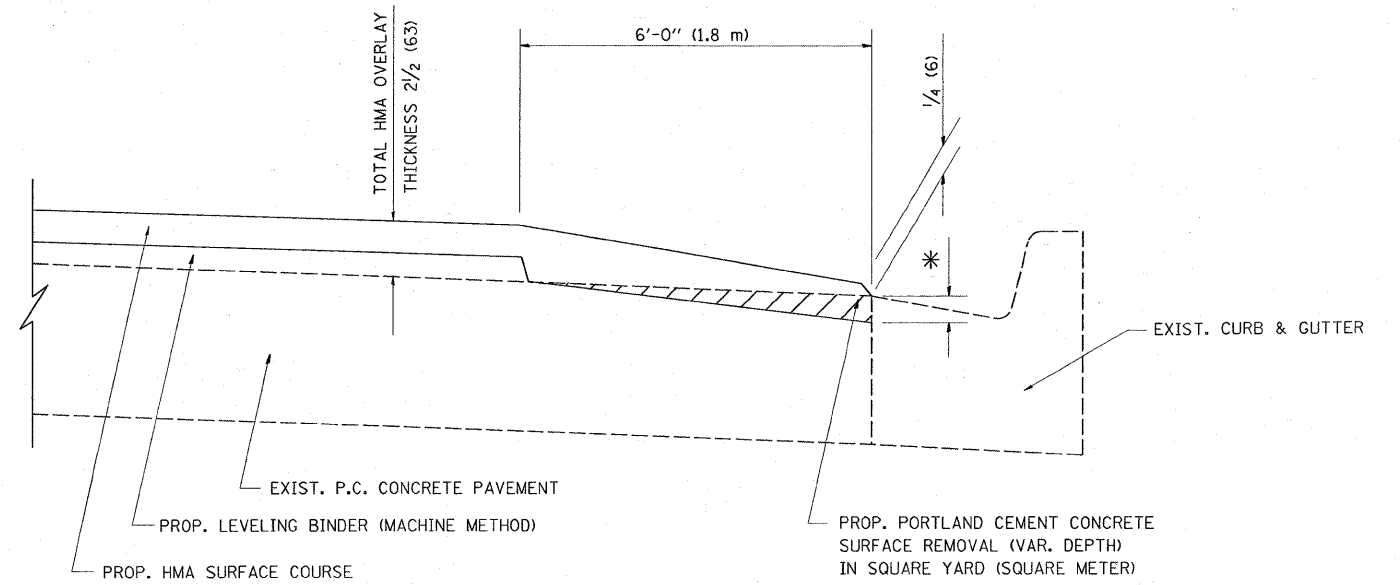
FILE NAME =	USER NAME = aquesiff	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
ca:\pwork\pwork\pwork\AQUEELFF\d0120663\Di\Std.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 3/21/2009	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
316	110&1-D-2RS-4	WILL	36	27
BD400-05 BD32		CONTRACT NO. 62778		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**HMA TAPER AT
EDGE OF P.C.C. PAVEMENT**

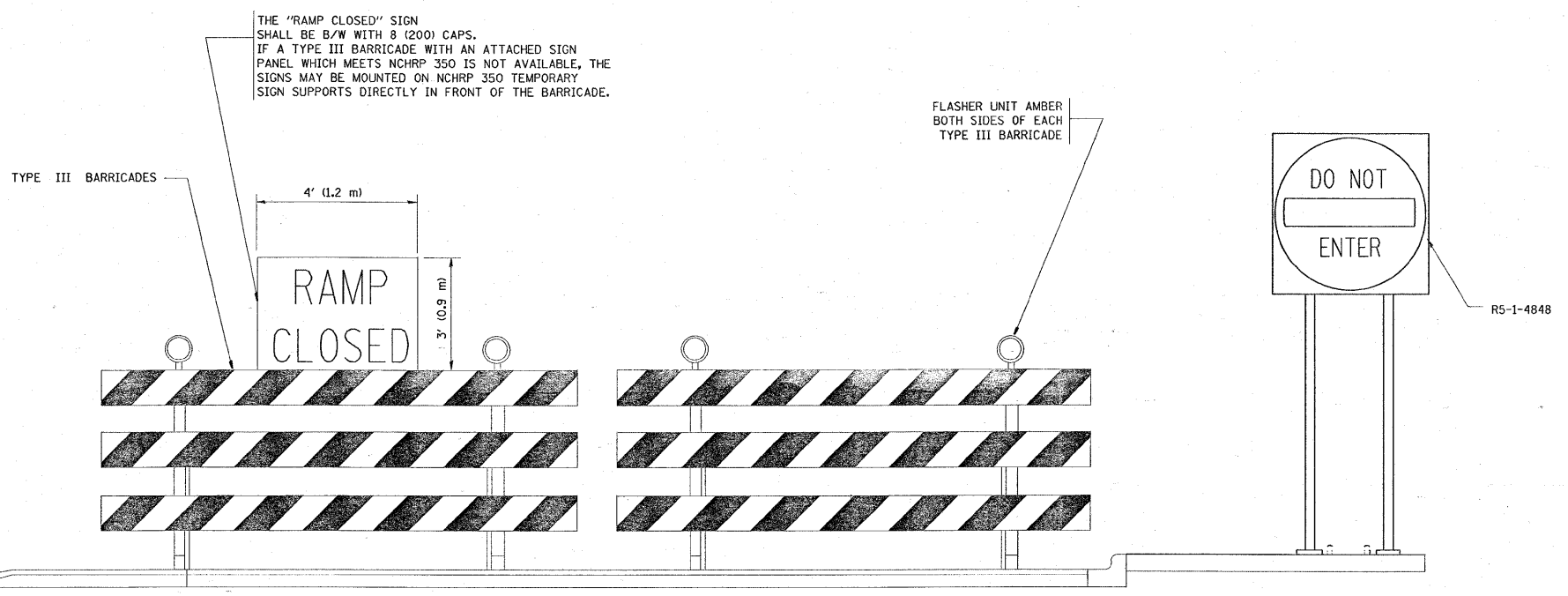
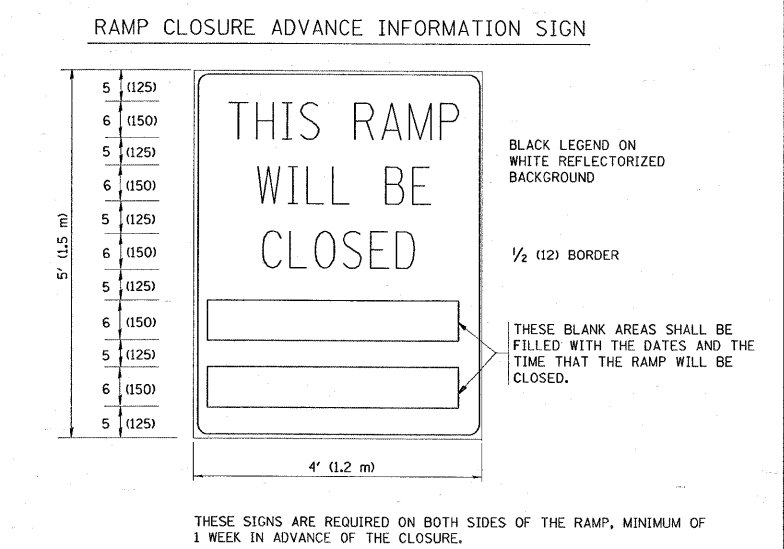
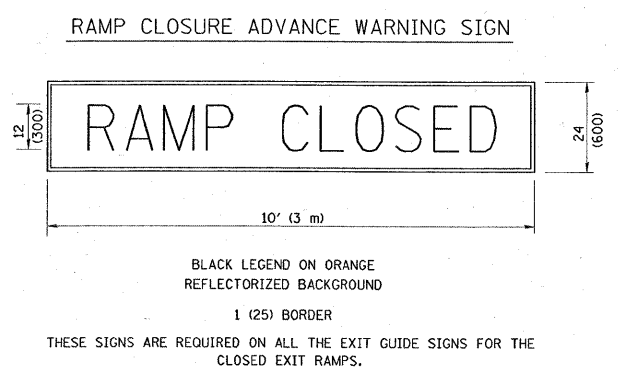
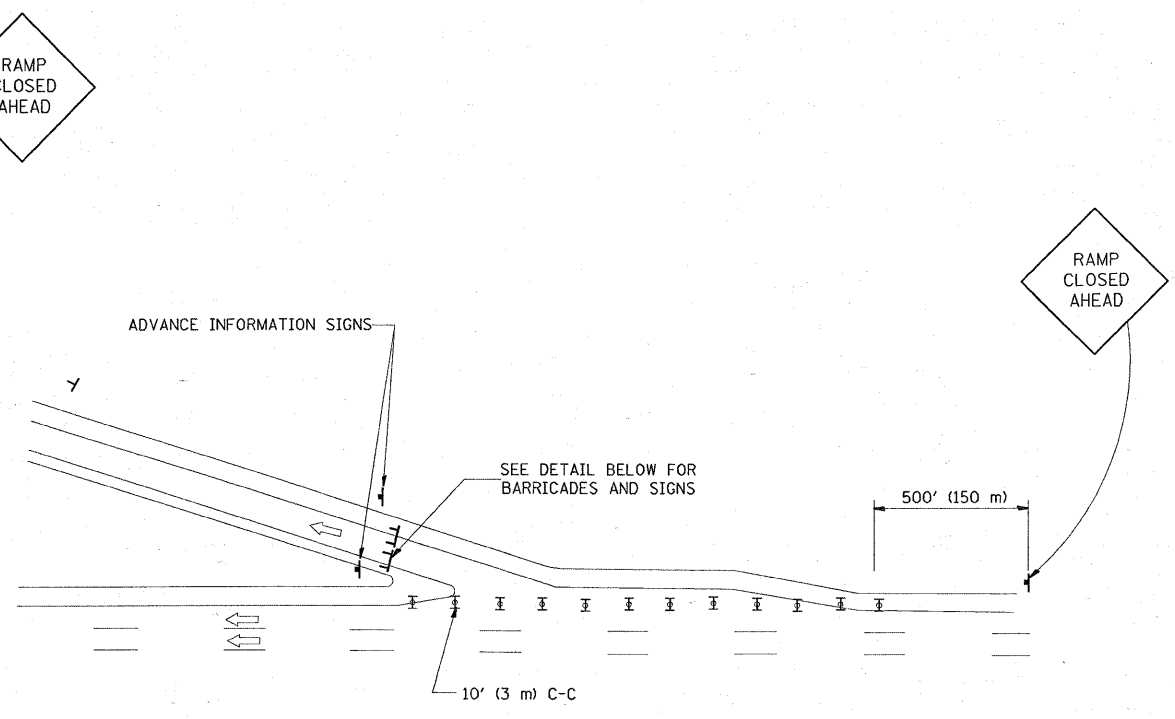
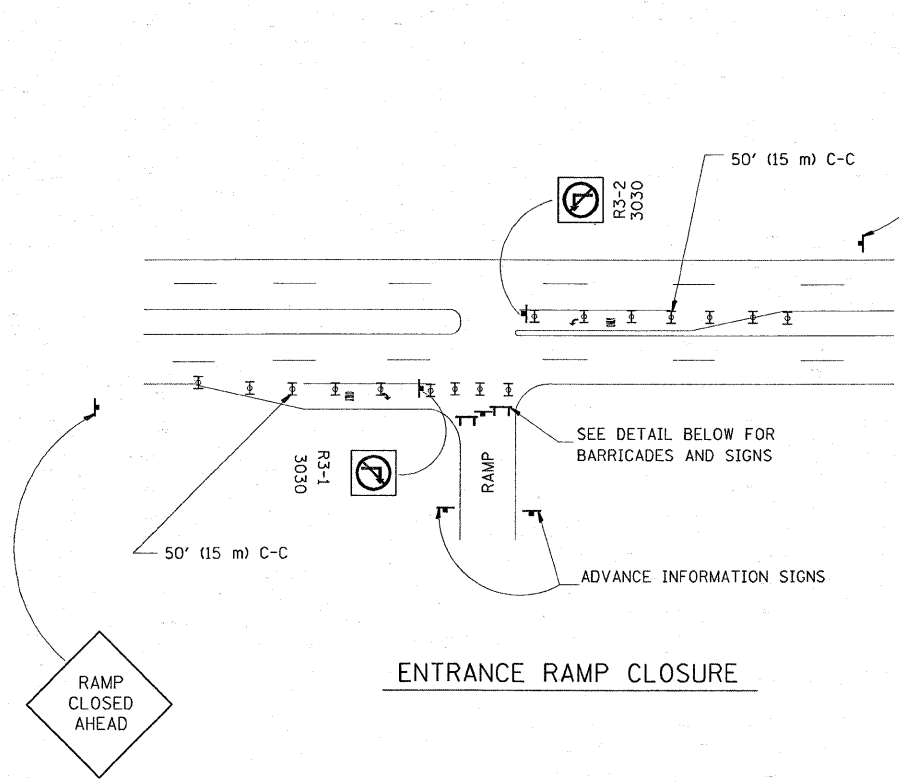
HMA SURFACE MIX	THICKNESS	LEVELING BINDER THICKNESS	* MILLING AT GUTTER FLAG
C OR D	1 1/2 (38)	1 (25)	1/4 (33)
F	1 3/4 (44)	3/4 (19)	1/2 (38)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = aqoolff	DESIGNED - R. SHAH	REVISED - R. SHAH 10-25-94
c:\pwork\pwidot\AQUEELFF\d0120663\0195std.dgn		DRAWN - JIS	REVISED - A. ABBAS 05-05-99
PLOT SCALE = 50,0000' / IN.		CHECKED - A. ABBAS	REVISED - E. GOMEZ 12-21-00
PLOT DATE = 3/21/2009		DATE - 09-10-94	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
						316	(10&1-D-2)RS-4	WILL	36	28
						BD400-06	(BD33)	CONTRACT NO. 62778		
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

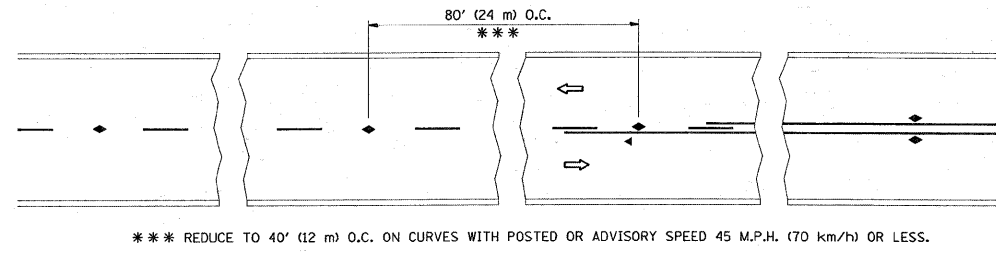


- SYMBOLS**
- ▬ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
 - ▬ TYPE III BARRICADE WITH FLASHING LIGHT

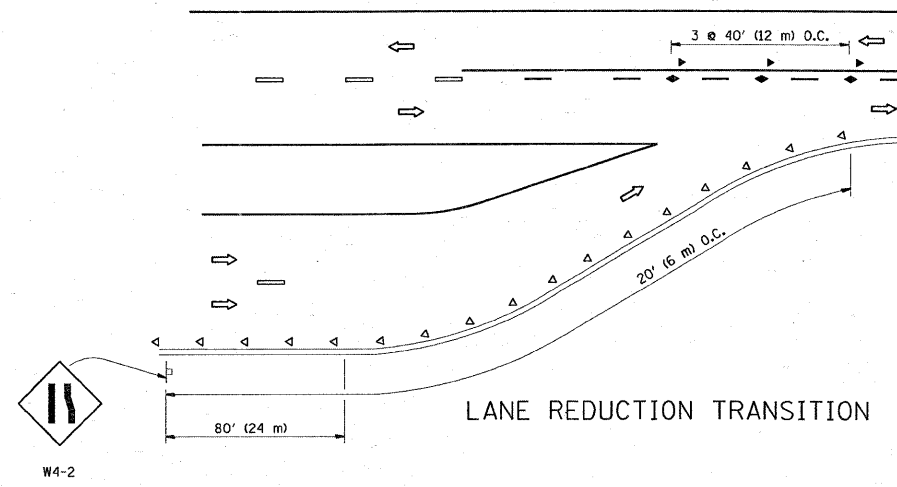
- GENERAL NOTES:**
1. CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
 2. STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
 3. A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
 4. ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
 5. THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
 6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
 7. THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY FOUR (24) HOURS IN LENGTH.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

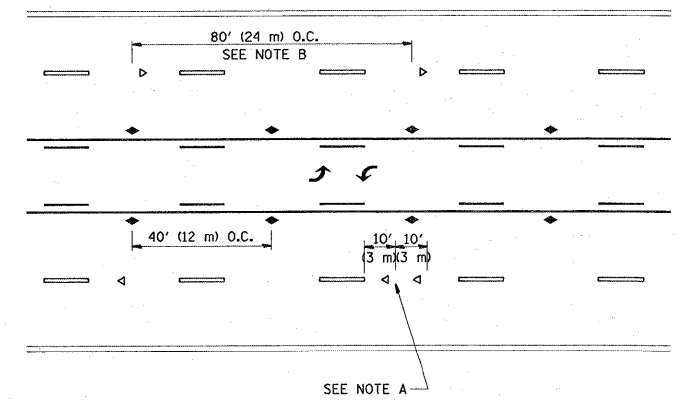
FILE NAME =	USER NAME = aqeeeff	DESIGNED - DWS	REVISED - DWS 12-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FREWAY ENTRANCE AND EXIST RAMP CLOSURE DETAILS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\PWIDOT\AQEEEFF\0120663\010Std.dgn	DRAWN -	REVISED - DWS/JAF 12-02	316			(10&1-D-2)RS-4	WILL	36	29	
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - JAF 02-06	TC-08			CONTRACT NO. 62778				
PLOT DATE = 3/21/2009	DATE - 02-83	REVISED - SPB 01-07	FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT							
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			



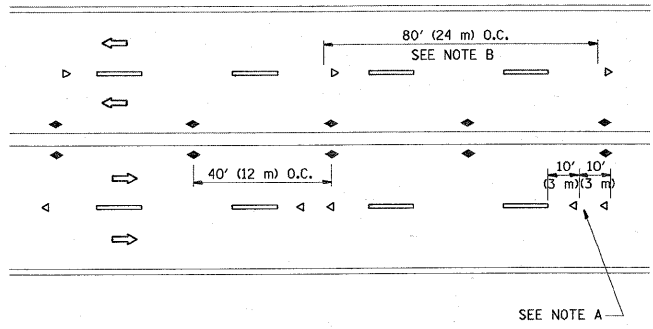
TWO-LANE/TWO-WAY



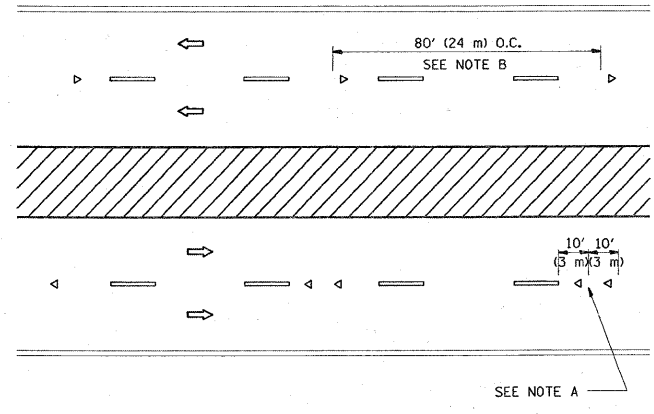
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

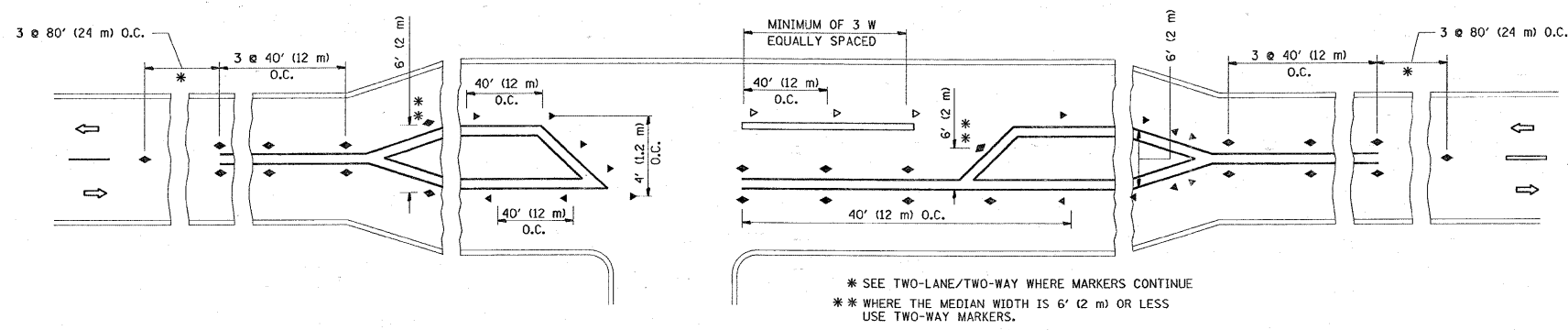
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

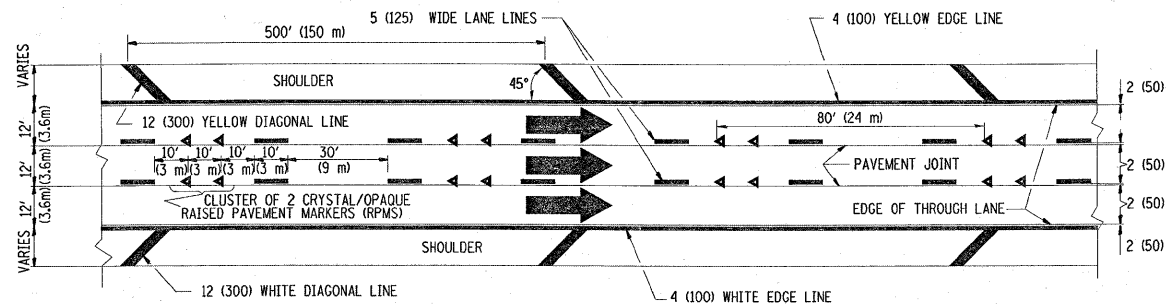


LEFT TURN

All dimensions are in Inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = aqueliff	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\p\work\PWID01\AQUEELFF\d0120663\Std.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99		RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			316	(10&1-D-2)RS-4	WILL	36	30
		CHECKED -	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-11		CONTRACT NO. 62778		
		DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

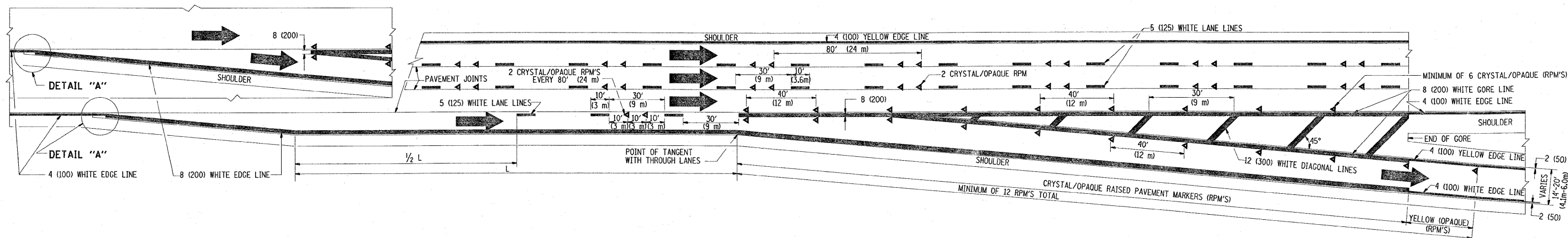
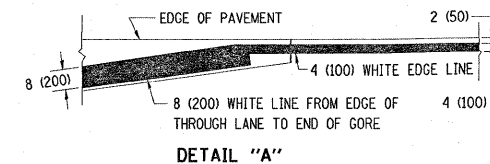
THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH
 THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH



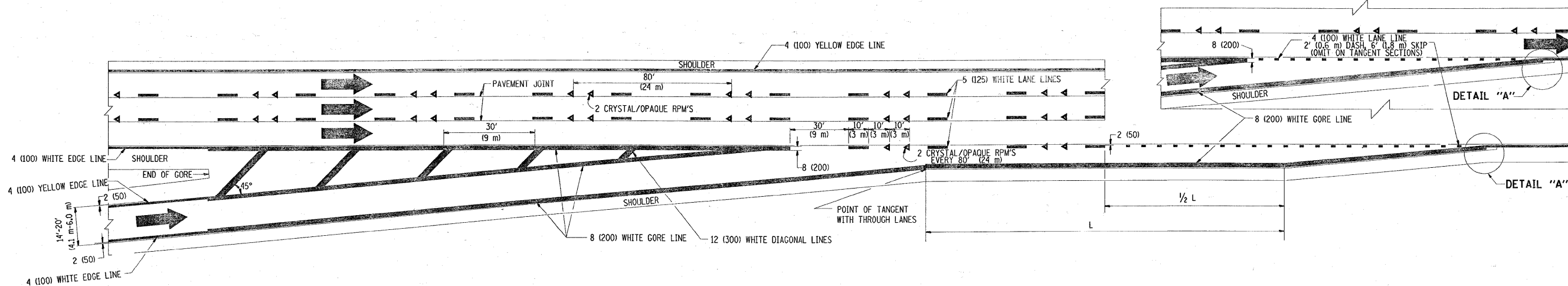
TYPICAL EDGE LINES & LANE LINES

NOTES:

1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE SHALL BE USED FOR ALL LANE LINES ON BITUMINOUS PAVEMENT
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC

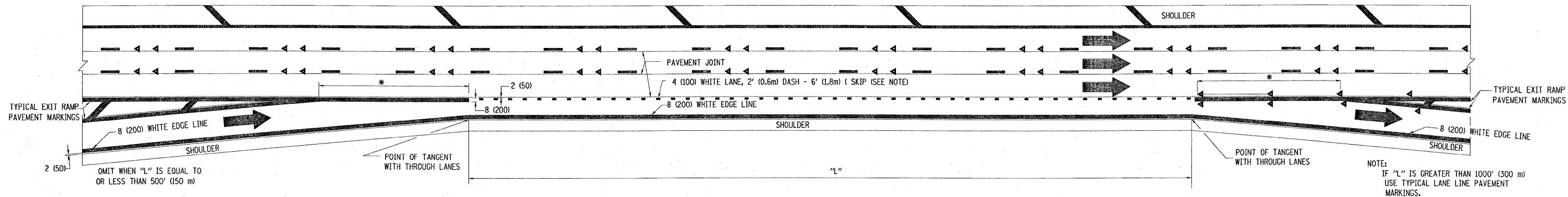


TYPICAL EXIT RAMP PAVEMENT MARKINGS

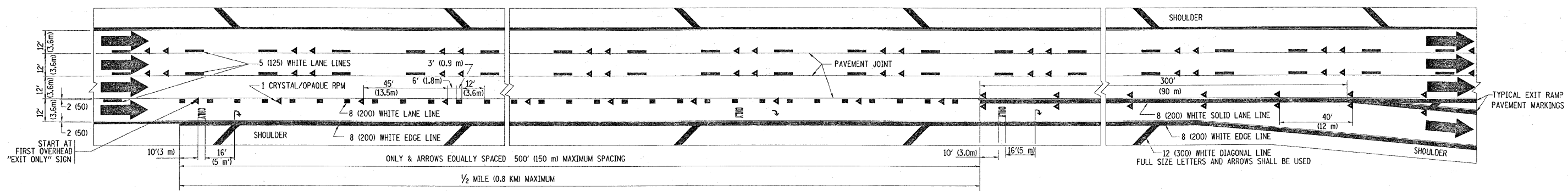


TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS

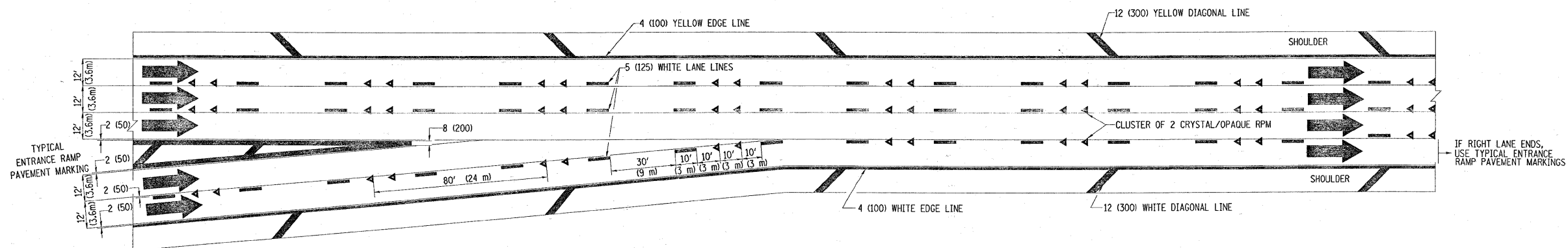
FILE NAME =	USER NAME = oqueelff	DESIGNED - D.W.S.	REVISED - A.H. 03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS	F.A.U. RTE. = 316	SECTION = (10&1-D-2)RS-4	COUNTY = WILL	TOTAL SHEETS = 36	SHEET NO. = 31
Plot Scale = 50,000' / IN.	CHECKED -	DATE - 01-90	REVISED - D.W.S. 07-96			SCALE: NONE	SHEET NO. 1 OF 2 SHEETS	STA. TO STA.	CONTRACT NO. 62778	
PLOT DATE = 3/21/2009	DATE -	REVISED - J.A.F. 02-06	REVISED - S.P.B. 01-07					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



TYPICAL ENTRANCE/EXIT RAMP COMBINATION PAVEMENT MARKINGS

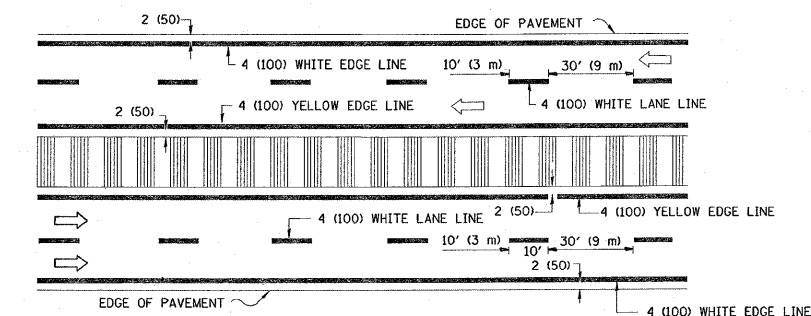
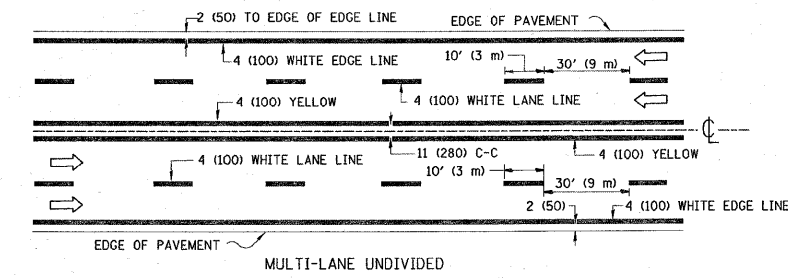
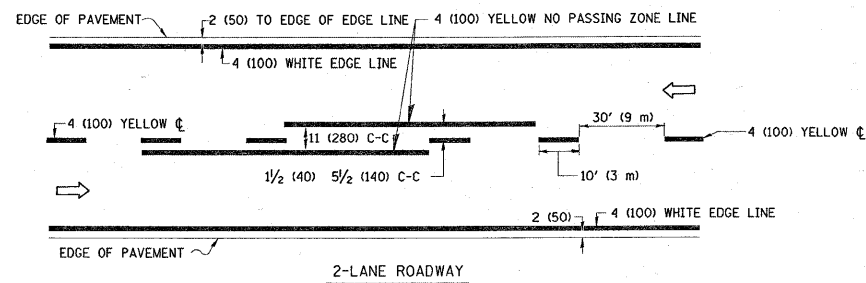


TYPICAL EXIT ONLY LANE PAVEMENT MARKINGS



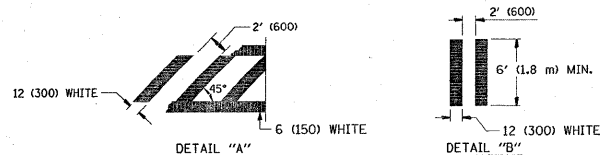
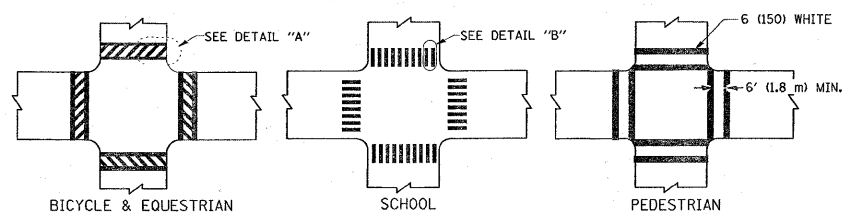
TYPICAL TWO LANE ENTRANCE RAMP PAVEMENT MARKINGS

FILE NAME =	USER NAME = oqueiff	DESIGNED - D.W.S.	REVISED - A.H. 03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS	F.A.U. RTE. 316	SECTION (10&1-D-2)RS-4	COUNTY WILL	TOTAL SHEETS 36	SHEET NO. 32	
ea:\pw\work\NPWID07VAQUEIFF\08120663\01-90.dgn	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED - D.W.S. 07-96			SCALE: NONE	SHEET NO. 2 OF 2 SHEETS	TC-12		CONTRACT NO. 62778	
	PLOT DATE = 3/21/2009	CHECKED -	REVISED - J.A.F. 02-06			STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			
		DATE - 01-90	REVISED - S.P.B. 01-07								

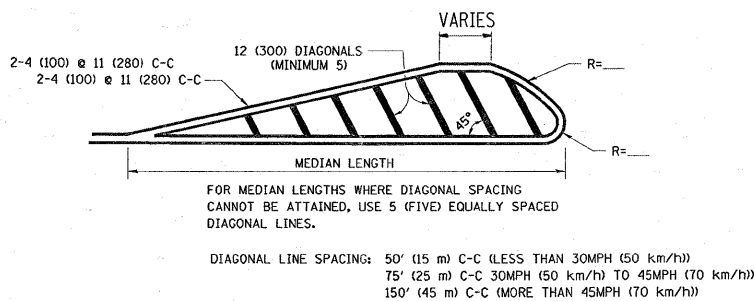
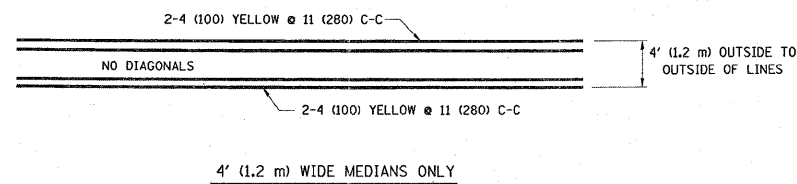


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

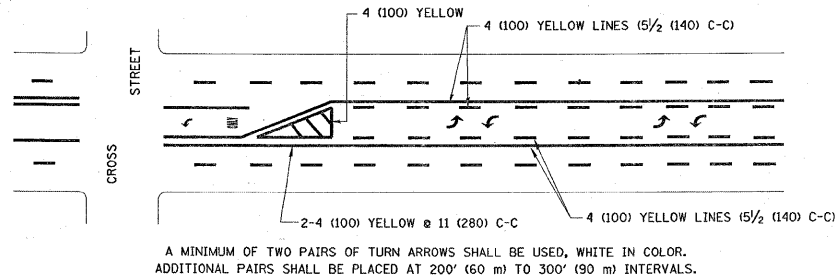
TYPICAL LANE AND EDGE LINE MARKING



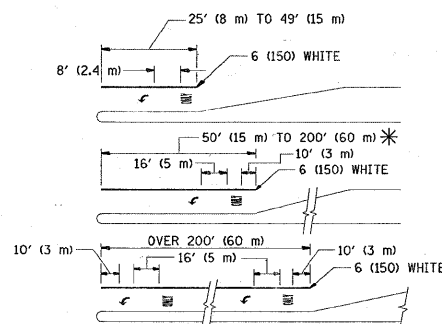
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING

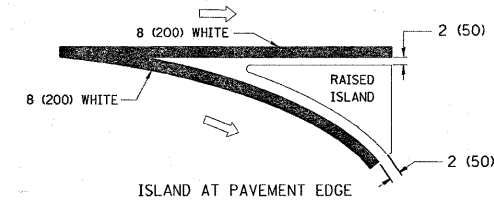
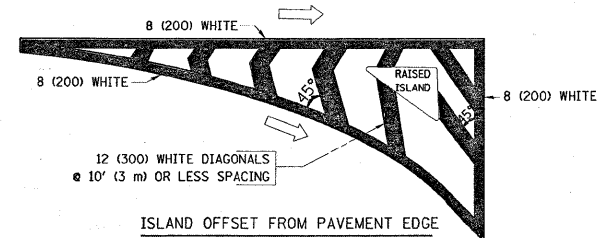


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = aqueselff	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
ca:\p_w\work\PIWIDOT\AQUESELFf\0120663\0120663.dgn	Std.dgn	DRAWN -	REVISED - A. HOUSEH 10-09-96
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - A. HOUSEH 10-17-96
PLOT DATE = 3/21/2009		DATE - 03-19-90	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

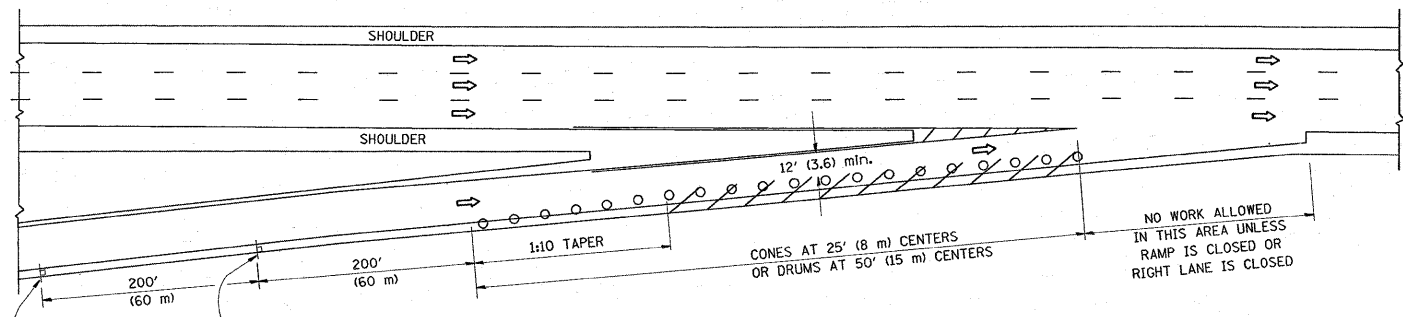
DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

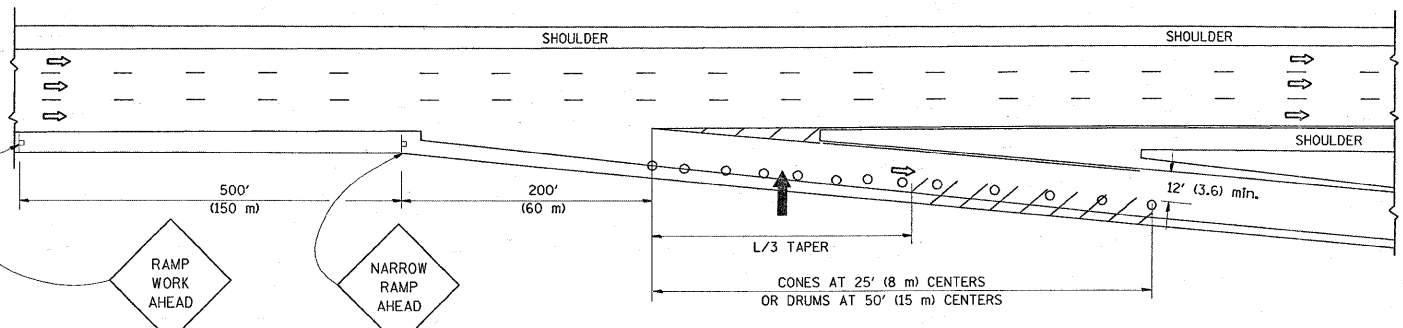
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
316	(10&1-D-2)RS-4	WILL	36	33
TC-13		CONTRACT NO. 62778		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PARTIAL RAMP CLOSURE DETAILS

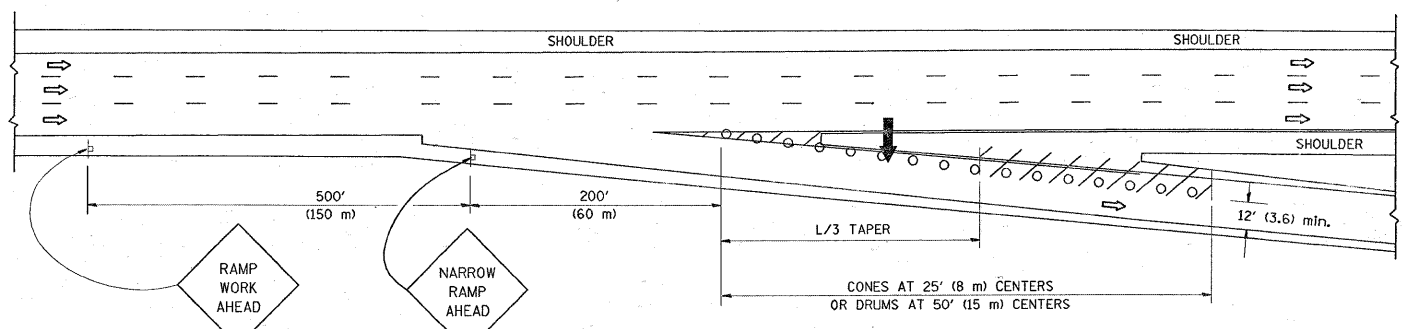
SHOULDER CLOSURE DETAILS



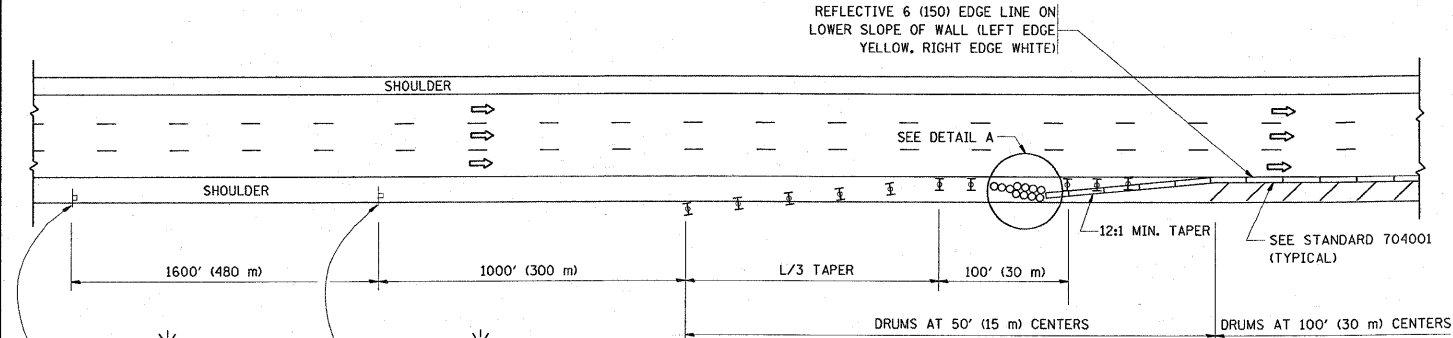
TYPICAL ENTRANCE RAMP



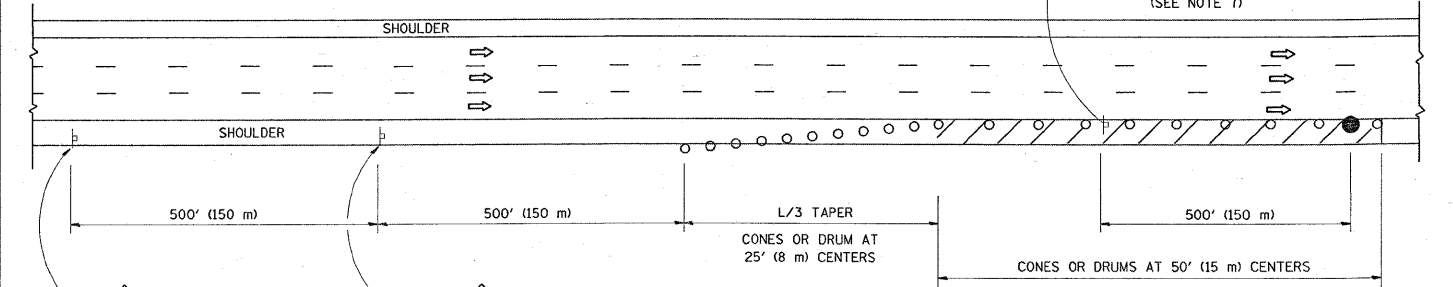
TYPICAL EXIT RAMP



TYPICAL EXIT RAMP



PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

SYMBOLS

- ARROWBOARD
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

GENERAL NOTES

1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h)	METRIC ENGLISH
OR GREATER:	$L=0.65(W)(S)$ $L=(W)(S)$
	W = WIDTH OF OFFSET IN FEET (METERS)
	S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE "TRAFFIC BARRIER TERMINAL, TYPE III, TEMPORARY" DEVICE TO MEET NCHRP350 FOR POSTED SPEED.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350 COMPLIANT FOR POSTED SPEED.

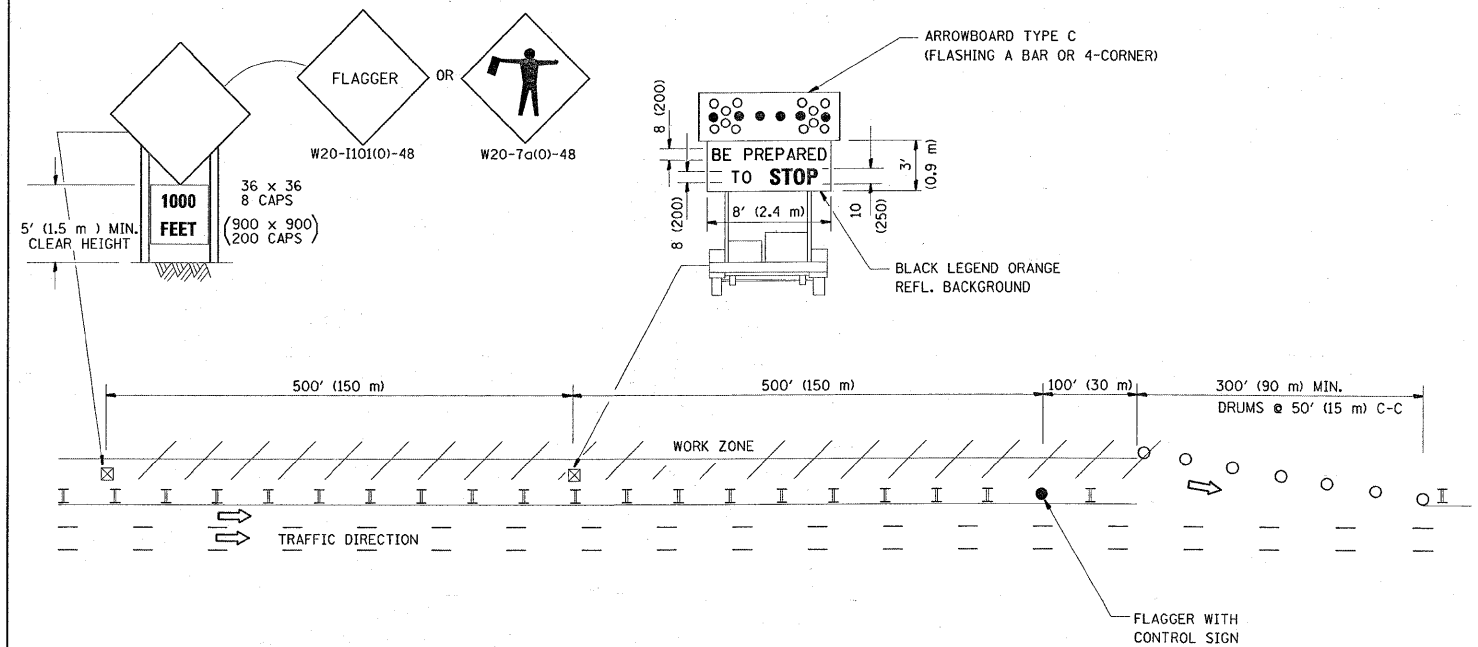
DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

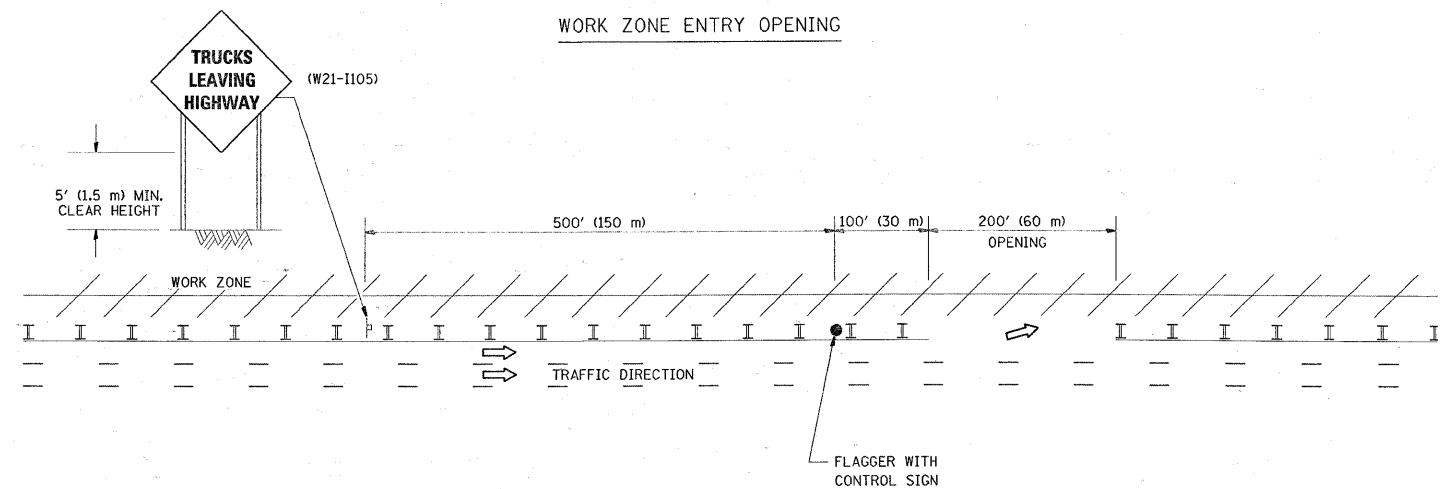
FILE NAME =	USER NAME = aqeelfff	DESIGNED -	REVISED - J.A.F. 12-02	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES	F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\K\PWIDOT\ADUEELFF\3120663\103	Std.dgn	DRAWN - D.W.S.	REVISED - 04-03			316	(10&1-D-2)RS-4	WILL	36	34
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - J.A.F. 12-06			TC-17		CONTRACT NO. 62778		
PLOT DATE = 3/21/2009		DATE - 11-96	REVISED - S.P.B. 01-07			SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. The Arrowboard, the Flagger Ahead trailer mounted sign, and the Trucks Leaving Highway sign shall be removed or turned away from traffic and the exit and entry openings shall be closed when the flagging operation ceases.
2. Work Zone Exit Openings should be a minimum of one half mile apart.
3. Exiting the work zone at any place other than at a Work Zone Exit Opening will be prohibited.
4. All vehicles shall enter the work zone at entry openings, using their turn signals to warn motorists

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

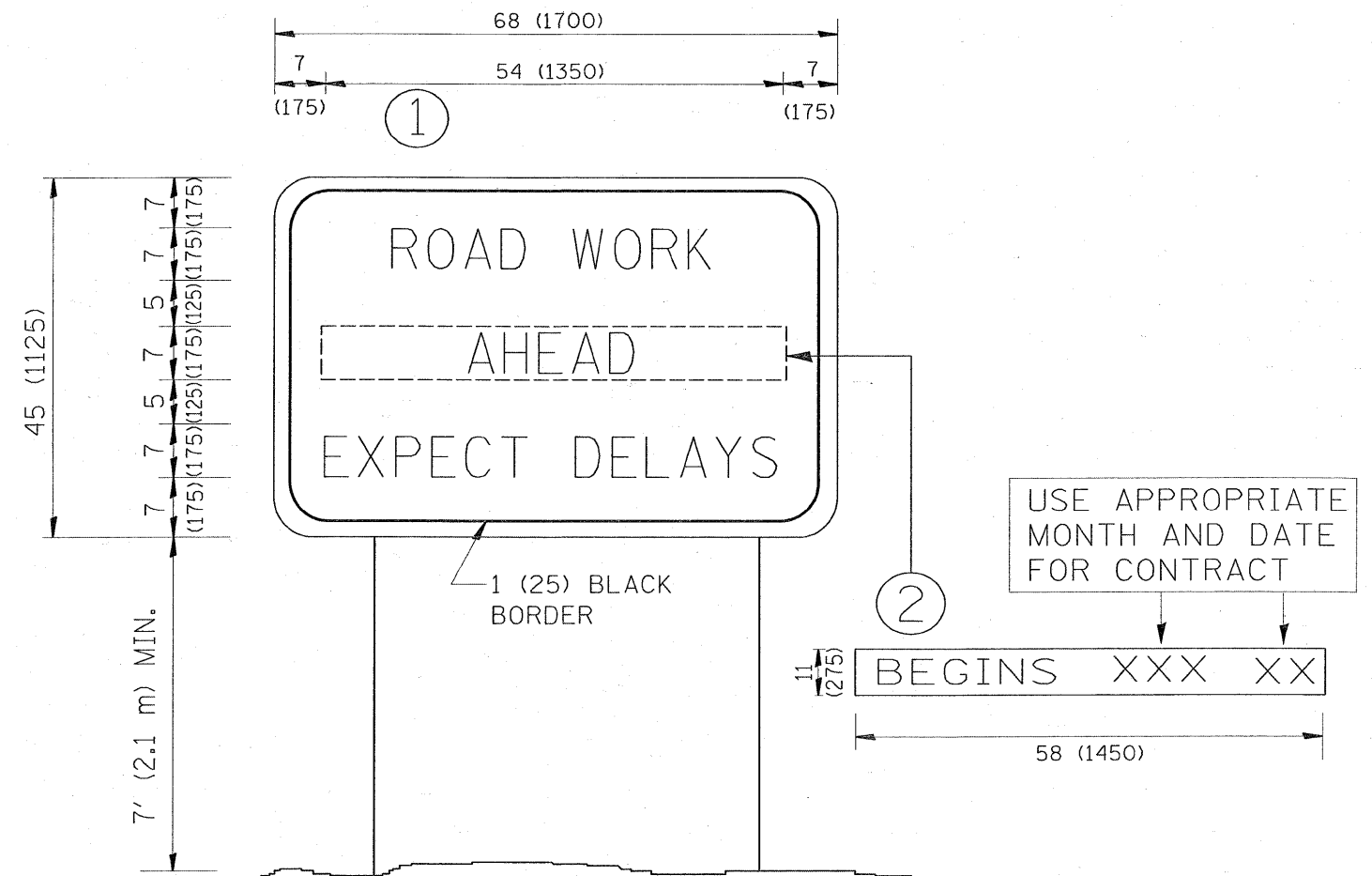
FILE NAME =	USER NAME = aquoiff	DESIGNED -	REVISED - D.W.S. 08-98
ct:\p\work\p\WIDOT\ADUEELFF\d0120663\01-13-03.dgn		DRAWN -	REVISED - J.A.F. 04-03
	PLOT SCALE = 50.0000 "/ IN.	CHECKED -	REVISED - J.A.F. 02-06
	PLOT DATE = 3/21/2003	DATE -	REVISED - S.P.B. 01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
316	(10&1-D-2)RS-4	WILL	36	35
TC-18		CONTRACT NO. 62778		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = equeo1ff	DESIGNED -	REVISED - R. MIRS 09-15-97
ca:\pki\work\pwwid01\AQUEELFF\0120663\Dis	Std.dgn	DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 3/21/2009	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
316	(10&1-D-2)RS-4	WILL	36	36
TC-22			CONTRACT NO. 62778	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				