

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5; 15RS-2	SCHUYLER	23	1

INDEX OF SHEETS

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

FAP ROUTE 317 (US 24)
SECTION 14RS-5; 15RS-2

SCHUYLER COUNTY
C-96-557-07

IDOT HIGHWAY STANDARDS

- 000001-05
- 442201-03
- 701001-02
- 701006-03
- 701011-02
- 701201-03
- 701301-03
- 701306-02
- 701311-03
- 701326-03
- 701501-05
- 701901-01
- 780001-02
- 781001-03

DESIGN DESIGNATIONS:

STA 362+00 TO 559+54 - 19,754.00 FEET
 STA 563+11.59 TO 763+00 - 19,988.41 FEET
 STA 1290+02.52 TO 1305+70.54 - 1,568.02 FEET
 STA 28+75 TO 30+00 - 125.00 FEET
 NET LENGTH - 41,435.43 FEET = 7.847 MILES
 HIGHWAY CLASSIFICATION - OTHER PRINCIPAL ARTERIAL
 ADT - (2001)=2000
 ADT - (2007)=2150
 ADT - (2027)=2500
 PV=81%; SU=8%; PV=11%

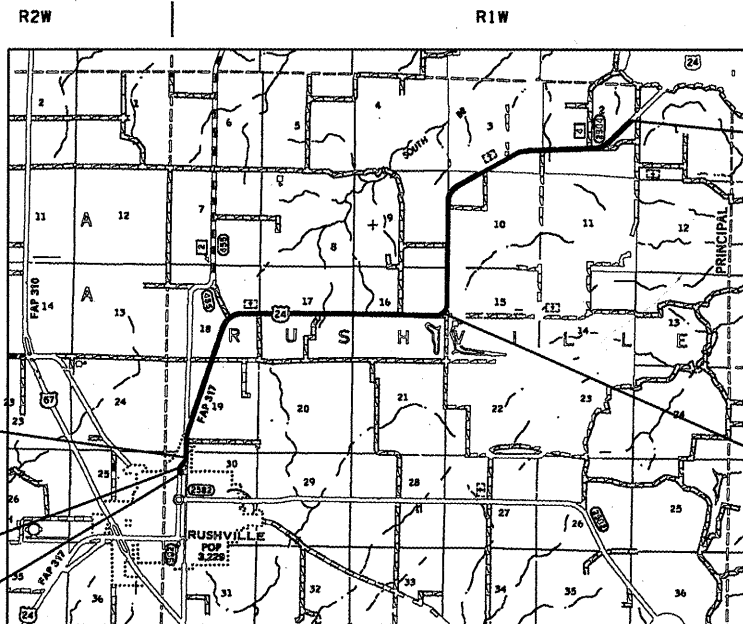
STA EQUATION
 STA 763+00.37 BK =
 STA 1290+02.52 AH

STA EQUATION
 STA 1305+70.54 BK =
 STA 30+00.00 AH
 (STATIONING REVERSES)

PROJECT ENDS
 STA 28+75

PROJECT BEGINS
 STA 362+00.00

STA EQUATION
 STA 559+54 BK =
 STA 563+11.59 AH

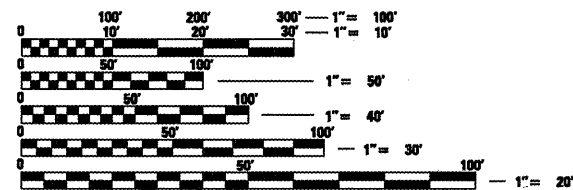


GROSS LENGTH - 41,435.43 FEET = 7.847 MILES
 NET LENGTH - 41,435.43 FEET = 7.847 MILES

D-96-566-03



LOCATION OF SECTION INDICATED THUS: —



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123

CONTRACT NO. 72849

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED April 10, 2009
Royal L. Piskel
 DISTRICT ENGINEER

May 1, 2009
Charles G. Ingenoll
 ENGINEER OF DESIGN AND ENVIRONMENT

May 1, 2009
Christine M. Reed
 DIRECTOR, DIVISION OF HIGHWAYS

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

PROJECT ENGINEER: JOHN MEGANGARD: 217-782-6990
 SQUAD LEADER: MARK DUST 217-785-0597

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5; 15RS-2	SCHUYLER	23	2
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

GENERAL NOTES

- THICKNESS OF RESURFACING:
THE THICKNESS OF BITUMINOUS MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
- ALL ELEVATIONS SHOWN ON THE PLANS ARE ESTABLISHED FROM U.S.G.S. MEAN SEA LEVEL DATUM.
- THE LOCATIONS OF EXISTING WATER MAINS, GAS MAINS, SEWERS, ELECTRIC POWER LINES, TELEPHONE LINES AND OTHER UTILITIES AS SHOWN ON THE PLANS, ARE BASED ON CAREFUL FIELD INVESTIGATION AND THE BEST INFORMATION AVAILABLE, BUT THEY ARE NOT GUARANTEED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN THEIR EXACT LOCATION FROM THE UTILITY COMPANIES AND BY FIELD INSPECTION.
- WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION.
- ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION OF THE COPY OF THE STANDARD INCLUDED IN THE BACK OF THESE PLANS.
- ALL STATION REFERENCES ARE TO THE ROADWAY CENTERLINE OR BASELINE.
- IN ADDITION TO FIELD SURVEYS AND AERIAL SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO A CHANGE IN THE SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
- PRIOR TO THE STRIPING OF NO PASSING ZONES, THE BUREAU OF OPERATIONS SHALL BE NOTIFIED 14 DAYS IN ADVANCE TO FIELD VERIFY SUCH PROPOSED MARKINGS.
- MIXTURE REQUIREMENTS
THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT

MIXTURE USE(S)	HMA SURFACE, INCIDENTAL, HMA SHOULDERS	LEVEL BINDER 3/4" (min)	PATCHING	HOT - MIX ASPHALT SHOULDERS 8"
AC/PG	PG 64-22	PG 64-22	PG 64-22	PG 58-22
DESIGN AIR VOIDS	4.0% @ NDESIGN = 50	4.0% @ NDESIGN = 50	4.0% @ NDESIGN = 50	2.0% @ NDESIGN = 30
MIXTURE COMPOSITION (GRADATION MIXTURE)	IL 9.5 OR 12.5	IL 9.5	IL 19.0	BAM (OTHER)
FRICTION AGGREGATE	MIX C	NA	N/A	NA

COMMITMENTS

- THE FIELD/RESIDENT ENGINEER SHALL CONTACT STUDIES & PLANS CONCERNING ANY MAJOR PLAN CHANGES TO MAKE SURE NO COMMITMENTS (NOT LISTED) WERE MADE AFFECTING THE DESIGN, AND ALLOW IMPROVEMENTS IN THE DESIGN FOR FUTURE PROJECTS.

RATES OF APPLICATION TABLE

THE FOLLOWING APPLICATION RATES HAVE BEEN USED TO DETERMINE PLAN QUANTITIES:

BITUMINOUS MATERIALS PRIME COAT PAVEMENT	0.00038 TON/SQ YD
AGGREGATE	0.001425 TON/SQ YD
AGGREGATE PRIME COAT	0.002 TON/SQ YD
HOT MIX ASPHALT	112 LB/SQ YD/1"
AGGREGATE SHOULDER & SURFACES	2.05 TON/CU YD
NITROGEN FERTILIZER NUTRIENTS	90 LB/ACRE
PHOSPHORUS FERTILIZER NUTRIENTS	90 LB/ACRE
POTASSIUM FERTILIZER NUTRIENTS	90 LB/ACRE
AGRICULTURAL GROUND LIMESTONE	2 TONS/ACRE
MULCH, METHOD 2	2 TONS/ACRE
RIPRAP	1.75 TONS/CU YD

DISTRICT SIX	
EXAMINED <u>April 10</u> 20 <u>09</u>	
<u>Louis J. Heasler</u> KDA	
OPERATIONS ENGINEER	
EXAMINED <u>April 9</u> 20 <u>09</u>	
<u>Louis J. Heasler</u>	
PROGRAM IMPLEMENTATION ENGINEER	
EXAMINED <u>April 10</u> 20 <u>09</u>	
<u>ORMLL</u>	
PROGRAM DEVELOPMENT ENGINEER	

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
GENERAL NOTES AND COMMITMENTS
FAP ROUTE 317 (US RTE 24)
SECTION 14RS-5; 15RS-2
SCHUYLER COUNTY

SCALE: VERT.
HORIZ.
DATE: JUNE, 2004

DRAWN BY DJK
CHECKED BY CAJ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5; 15RS-2	SCHUYLER	23	3
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES

	ITEM	UNIT	TOTAL	CONSTRUCTION TYPE CODE
				1000 100% STATE
20200600	EXCAVATING AND GRADING EXISTING SHOULDER	UNIT	207	207
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	55.2	55.2
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	48.3	48.3
40600300	AGGREGATE (PRIME COAT)	TON	250	250
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	5028	5028
40600895	CONSTRUCTING TEST STRIP	EACH	1	1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	4,918	4,918
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	69	69
40600990	TEMPORARY RAMP	SQ YD	29	29
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	10,203	10,203
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	239	239
44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	119,818	119,818
44200144	PAVEMENT PATCHING, TYPE II, 12 INCH	SQ YD	305	305
44200148	PAVEMENT PATCHING, TYPE III, 12 INCH	SQ YD	55	55
44200150	PAVEMENT PATCHING, TYPE IV, 12 INCH	SQ YD	30	30
48101200	AGGREGATE SHOULDERS, TYPE B	TON	3,926	3,926
48203029	HOT - MIX ASPHALT SHOULDERS, 8"	SQ YD	6,732.7	6,732.7

SHEET 1 OF 2

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
FAP 317 (US 24)
SECTION 14RS-5; 15RS-2
SCHUYLER COUNTY
SCALE: VERT.
HORIZ.
DATE: JUNE, 2004
DRAWN BY DJK
CHECKED BY CAJ

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5; 15RS-2	SCHUYLER	23	4
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES

	ITEM	UNIT	TOTAL	CONSTRUCTION TYPE CODE	
				1000	100% STATE
48203100	HOT-MIX ASPHALT SHOULDERS	TON	15	15	
60255500	MANHOLES TO BE ADJUSTED	EACH	2	2	
60266600	VALVE BOXES TO BE ADJUSTED	EACH	1	1	
67000500	ENGINEER'S FIELD OFFICE, TYPE B	CAL MO	12	12	
67100100	MOBILIZATION	L SUM	1	1	
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1	
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1	
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	1	
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	10	10	
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	12,431	12,431	
70300230	TEMPORARY PAVEMENT MARKING - LINE 5"	FOOT	121,420	121,420	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1,726	1,726	
* 78001120	PAINT PAVEMENT MARKING - LINE 5"	FOOT	121,420	121,420	
* 78004230	PREFORMED PLASTIC PAVEMENT MARKING - TYPE B - INLAID - LINE 6"	FOOT	64	64	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	519	519	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	519	519	

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**Specialty Items*

REVISIONS	
NAME	DATE

SHEET 2 OF 2

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES

FAP 317 (US 24)
SECTION 14RS-5; 15RS-2
SCHUYLER COUNTY

SCALE: VERT. _____
HORIZ. _____
DATE: JUNE, 2004

DRAWN BY DJK
CHECKED BY CAJ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5; 15RS-2	SCHUYLER	23	5
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

RESURFACING SCHEDULE												
LOCATION			DISTANCE	PVT WIDTH / SHLD WIDTH	HMA SURF REMOVAL (BUTT JOINT)	HMA SURF REMOVAL (VAR DEPTH)	TEMP RAMP	BITUMINOUS MATERIALS (P. C.)	AGGREGATE (P. C.)	LEVELING BINDER	HMA SURFACE COURSE	HMA SHLDS
STA	TO	STA	FOOT	FOOT	SQ YD	SQ YD	SQ YD	TON	TON	TON	TON	TON
US 24 (MAINLINE EXISTING PAVEMENT AREA)												
361+70.00		362+00.00	30.00	26.00	86.67	-	14.44	0.03	0.17	-	73.71	-
362+00.00		559+54.09	19,754.08	26.00	-	57,067.34	-	21.69	114.13	2,396.83	4,793.66	-
563+11.59		763+00.39	19,988.80	26.00	-	57,745.42	-	21.94	115.49	2,425.31	4,850.62	-
1290+02.52		1305+70.54	1,568.02	26.00	-	4,529.84	-	1.72	9.06	190.25	380.51	-
30+00.00		28+75.00	125.00	26.00	-	361.11	-	0.14	0.72	15.17	30.33	-
28+75.00		28+45.00	30.00	26.00	86.67	-	14.44	0.03	0.17	-	73.71	-
US 24 (MAINLINE EXISTING SHOULDER AREA)												
RT 760+59.60		RT 763+00.00	240.40	4.00	-	106.84	-	0.04	0.21	-	-	13.46
RT 1290+02.52		RT 1290+18.7	16.18	4.00	-	7.19	-	0.00	0.01	-	-	0.91
ENTRANCES & SIDEROADS					3,985.40	-	-	1.60	5.80	-	-	-
TOTAL =					4,158.73	119,817.75	28.89	47.20	245.78	5,027.56	10,202.53	14.37

PAVEMENT MARKING												
SECTION TYPE	STATION BEGIN	STATION END	DISTANCE (FT)	5" WHITE (FT)	NO PASS ZONE	5"YELLOW (FT)	REFLECTIVE MARKERS 2-WAY AMBER (EACH)	SHORT TERM YELLOW (FT)-3 APPL.	WORK ZONE PAVEMENT MARKING REMOVAL (SQ FT)	PREFORMED PLASTIC PAVE.MARK, TY B - 6" (FT)		
2 LANE	362+00	TO 371+00	900	1,800	RIGHT	1,125	12	270	38			
2 LANE	371+00	TO 394+75	2,375	4,750	DOUBLE	4,750	30	713	99			
2 LANE	394+75	TO 399+35	460	920	LEFT	575	6	138	19			
2 LANE	399+35	TO 422+65	2,330	4,660	NONE	1,165	29	699	97			
2 LANE	422+65	TO 433+75	1,110	2,220	RIGHT	1,388	14	333	46			
2 LANE	433+75	TO 437+75	400	800	DOUBLE	800	5	120	17			
2 LANE	437+75	TO 447+00	925	1,850	LEFT	1,156	12	278	39			
2 LANE	447+00	TO 467+25	2,025	4,050	NONE	506	25	608	84			
2 LANE	467+25	TO 477+25	1,000	2,000	RIGHT	1,250	13	300	42			
2 LANE	477+25	TO 490+80	1,355	2,710	DOUBLE	2,710	17	407	56			
2 LANE	490+80	TO 495+10	430	860	LEFT	538	5	129	18			
2 LANE	495+10	TO 525+80	3,070	6,140	NONE	768	38	921	128			
2 LANE	525+80	TO 534+50	870	1,740	RIGHT	1,088	11	261	36			
2 LANE	534+50	TO 564+20	2,612	5,224	DOUBLE	5,224	33	784	109			
2 LANE	564+20	TO 576+00	1,180	2,360	LEFT	1,475	15	354	49			
2 LANE	576+00	TO 654+80	7,880	15,760	NONE	1,970	99	2,364	328			
2 LANE	654+80	TO 668+00	1,320	2,640	RIGHT	1,650	17	396	55			
2 LANE	668+00	TO 686+75	1,875	3,750	DOUBLE	3,750	23	563	78			
2 LANE	686+75	TO 694+20	745	1,490	LEFT	931	9	224	31			
2 LANE	694+20	TO 737+35	4,315	8,630	NONE	1,079	54	1,295	180			
2 LANE	737+35	TO 743+68	633	1,266	RIGHT	791	8	190	26			
2 LANE	743+68	TO 746+45	277	554	DOUBLE	554	3	83	12			
2 LANE	746+45	TO 748+40	195	390	LEFT	244	2	59	8			
2 LANE	748+40	TO 758+75	1,035	2,070	NONE	259	13	311	43			
2 LANE	758+75	TO 760+65	190	380	RIGHT	238	2	57	8			
2 LANE	760+65	TO 1298+60	1,093	2,186	DOUBLE	2,186	14	328	46			
2 LANE	1298+60	TO 1300+30	170	340	LEFT	213	2	51	7			
2 LANE	1300+30	TO 28+75	666	1,331	NONE	166	8	200	28	64		
TOTAL				82,871		38,549	519	12,431	1,726	64		

REVISIONS		SHEET 1 OF 2	
NAME	DATE	ILLINOIS DEPARTMENT OF TRANSPORTATION	
		SCHEDULE OF QUANTITIES	
		FAP 317 (US 24)	
		SECTION 14RS-5; 15RS-2	
		SCHUYLER COUNTY	
SCALE:	VERT. HORIZ.	DRAWN BY DJK	
DATE: JUNE, 2004		CHECKED BY CAJ	

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5; 15RS-2	SCHUYLER	23	6
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

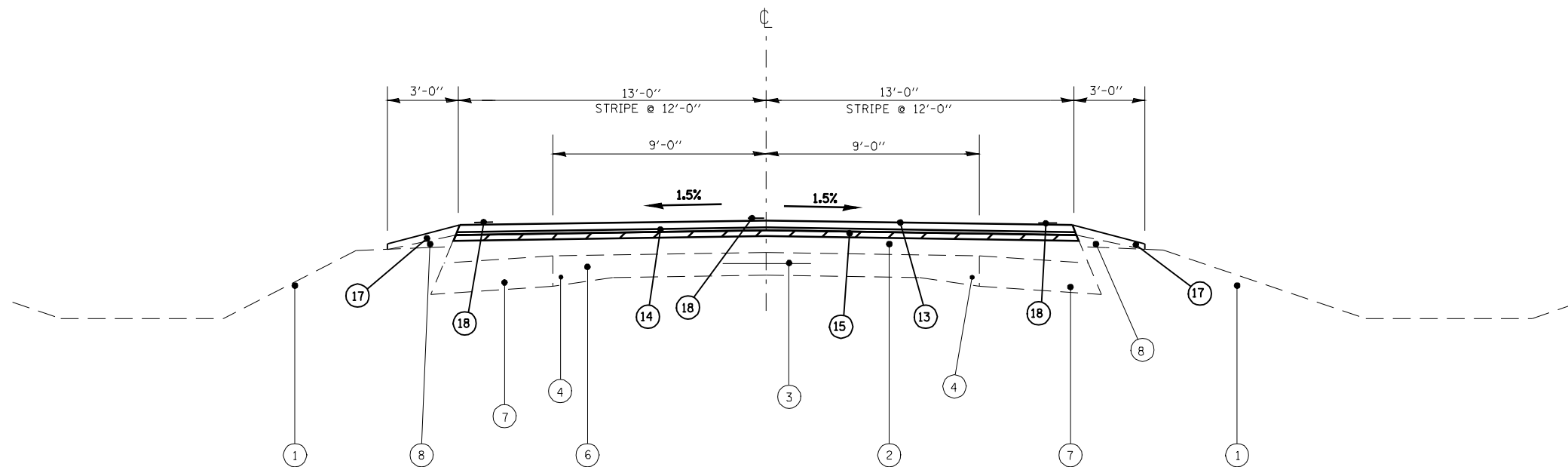
SHOULDER SCHEDULE								
LOCATION			DISTANCE	HMA SHLD WIDTH	AGG SHLD WIDTH	EXCAV & GRADING EX SHLD	HMA SHLD 8 INCH	AGG SHLD TYPE B
STA	TO	STA	FOOT	FOOT	SQ YD	UNIT	SQ YD	TON
<u>US 24 (MAINLINE)</u>								
LT 361+70.00		LT 384+12.83	2242.83	0.0	3.0	0.0	-	106.43
LT 384+12.83		LT 384+62.83	50.00	1.0 TO 3.0	3.0	0.5	11.11	2.37
LT 384+62.83		LT 394+92.19	1029.36	3.0	3.0	10.3	343.12	48.85
LT 394+92.19		LT 395+42.19	50.00	3.0 TO 1.0	3.0	0.5	11.11	2.37
LT 395+42.19		LT 430+38.62	3496.43	0.0	3.0	0.0	-	165.92
LT 430+38.62		LT 430+88.62	50.00	1.0 TO 3.0	3.0	0.5	11.11	2.37
LT 430+88.62		LT 440+53.62	965.00	3.0	3.0	9.7	321.67	45.79
LT 440+53.62		LT 441+03.62	50.00	3.0 TO 1.0	3.0	0.5	11.11	2.37
LT 441+03.62		LT 474+92.72	3389.10	0.0	3.0	0.0	-	160.83
LT 474+92.72		LT 475+42.72	50.00	1.0 TO 3.0	3.0	0.5	11.11	2.37
LT 475+42.72		LT 489+07.86	1365.14	3.0	3.0	13.7	455.05	64.78
LT 489+07.86		LT 489+57.86	50.00	3.0 TO 1.0	3.0	0.5	11.11	2.37
LT 489+57.86		LT 536+39.04	4681.18	0.0	3.0	0.0	-	222.14
LT 536+39.04		LT 536+89.04	50.00	1.0 TO 3.0	3.0	0.5	11.11	2.37
LT 536+89.04		LT 559+54.09	2265.05	3.0	3.0	22.7	755.02	107.49
LT 563+11.59		LT 564+61.59	150.00	3.0	3.0	1.5	50.00	7.12
LT 564+61.59		LT 565+11.59	50.00	3.0 TO 1.0	3.0	0.5	11.11	2.37
LT 565+11.59		LT 666+20.64	10109.05	0.0	3.0	0.0	-	479.71
LT 666+20.64		LT 666+70.64	50.00	1.0 TO 3.0	3.0	0.5	11.11	2.37
LT 666+70.64		LT 682+49.18	1578.54	3.0	3.0	15.8	526.18	74.91
LT 682+49.18		LT 682+99.18	50.00	3.0 TO 1.0	3.0	0.5	11.11	2.37
LT 682+99.18		LT 742+81.15	5981.97	0.0	3.0	0.0	-	283.87
LT 742+81.15		LT 743+31.15	50.00	1.0 TO 3.0	3.0	0.5	11.11	2.37
LT 743+31.15		LT 746+79.13	347.98	3.0	3.0	3.5	115.99	16.51
LT 746+79.13		LT 747+29.13	50.00	3.0 TO 1.0	3.0	0.5	11.11	2.37
LT 747+29.13		LT 758+63.71	1134.58	0.0	3.0	0.0	-	53.84
LT 758+63.71		LT 759+13.71	50.00	1.0 TO 3.0	3.0	0.5	11.11	2.37
LT 759+13.71		LT 763+00.39	386.68	3.0	3.0	3.9	128.89	18.35
LT 1290+02.52		LT 1290+95.8	93.33	3.0	3.0	0.9	31.11	4.43
LT 1290+95.85		LT 1291+45.8	50.00	3.0	3.0	0.5	16.67	2.37
LT 1291+45.85		LT 1291+73.8	28.01	3.0	3.0	0.3	9.34	1.33
LT 1291+73.86		LT 1292+23.8	50.00	3.0	3.0	0.5	16.67	2.37
LT 1292+23.86		LT 1297+11.8	488.02	3.0	3.0	4.9	162.67	23.16
LT 1297+11.88		LT 1297+61.8	50.00	3.0	3.0	0.5	11.11	2.37
LT 1297+61.88		LT 1305+70.5	808.65	3.0	3.0	8.1	269.55	38.37
RT 30+00.00		RT 28+75.00	125.00	3.0	3.0	1.3	41.67	5.93
RT 28+75.00		RT 28+45.00	30.00	3.0	3.0	0.3	10.00	1.42
SUB-TOTAL =						104.61	3,409.14	1,969.13

SHOULDER SCHEDULE								
LOCATION			DISTANCE	HMA SHLD WIDTH	AGG SHLD WIDTH	EXCAV & GRADING EX SHLD	HMA SHLD 8 INCH	AGG SHLD TYPE B
STA	TO	STA	FOOT	FOOT	SQ YD	UNIT	SQ YD	TON
<u>US 24 (MAINLINE)</u>								
RT 361+70.00		RT 384+12.83	2242.83	0.0	3.0	0.0	-	106.43
RT 384+12.83		RT 384+62.83	50.00	1.0 TO 3.0	3.0	0.5	11.11	2.37
RT 384+62.83		RT 394+92.19	1029.36	3.0	3.0	10.3	343.12	48.85
RT 394+92.19		RT 395+42.19	50.00	3.0 TO 1.0	3.0	0.5	11.11	2.37
RT 395+42.19		RT 430+38.62	3496.43	0.0	3.0	0.0	-	165.92
RT 430+38.62		RT 430+88.62	50.00	1.0 TO 3.0	3.0	0.5	11.11	2.37
RT 430+88.62		RT 440+53.62	965.00	3.0	3.0	9.7	321.67	45.79
RT 440+53.62		RT 441+03.62	50.00	3.0 TO 1.0	3.0	0.5	11.11	2.37
RT 441+03.62		RT 474+92.72	3389.10	0.0	3.0	0.0	-	160.83
RT 474+92.72		RT 475+42.72	50.00	1.0 TO 3.0	3.0	0.5	11.11	2.37
RT 475+42.72		RT 489+07.86	1365.14	3.0	3.0	13.7	455.05	64.78
RT 489+07.86		RT 489+57.86	50.00	3.0 TO 1.0	3.0	0.5	11.11	2.37
RT 489+57.86		RT 536+39.04	4681.18	0.0	3.0	0.0	-	222.14
RT 536+39.04		RT 536+89.04	50.00	1.0 TO 3.0	3.0	0.5	11.11	2.37
RT 536+89.04		RT 559+54.09	2265.05	3.0	3.0	22.7	755.02	107.49
RT 563+11.59		RT 564+61.59	150.00	3.0	3.0	1.5	50.00	7.12
RT 564+61.59		RT 565+11.59	50.00	3.0 TO 1.0	3.0	0.5	11.11	2.37
RT 565+11.59		RT 666+20.64	10109.05	0.0	3.0	0.0	-	479.71
RT 666+20.64		RT 666+70.64	50.00	1.0 TO 3.0	3.0	0.5	11.11	2.37
RT 666+70.64		RT 682+49.18	1578.54	3.0	3.0	15.8	526.18	74.91
RT 682+49.18		RT 682+99.18	50.00	3.0 TO 1.0	3.0	0.5	11.11	2.37
RT 682+99.18		RT 742+81.15	5981.97	0.0	3.0	0.0	-	283.87
RT 742+81.15		RT 743+31.15	50.00	1.0 TO 3.0	3.0	0.5	11.11	2.37
RT 743+31.15		RT 746+79.13	347.98	3.0	3.0	3.5	115.99	16.51
RT 746+79.13		RT 747+29.13	50.00	3.0 TO 1.0	3.0	0.5	11.11	2.37
RT 747+29.13		RT 758+63.71	1134.58	0.0	3.0	0.0	-	53.84
RT 758+63.71		RT 759+13.71	50.00	1.0 TO 3.0	3.0	0.5	11.11	2.37
RT 759+13.71		RT 760+59.60	145.89	3.0	3.0	1.5	48.63	6.92
RT 1290+18.70		RT 1290+95.8	77.15	3.0	3.0	0.8	25.72	3.66
RT 1290+95.85		RT 1291+45.8	50.00	3.0	3.0	0.5	16.67	2.37
RT 1291+45.85		RT 1291+73.8	28.01	3.0	3.0	0.3	9.34	1.33
RT 1291+73.86		RT 1292+23.8	50.00	3.0	3.0	0.5	16.67	2.37
RT 1292+23.86		RT 1297+11.8	488.02	3.0	3.0	4.9	162.67	23.16
RT 1297+11.88		RT 1297+61.8	50.00	3.0	3.0	0.5	11.11	2.37
RT 1297+61.88		RT 1305+70.5	808.65	3.0	3.0	8.1	269.55	38.37
LT 30+00.00		LT 28+75.00	125.00	3.0	3.0	1.3	41.67	5.93
LT 28+75.00		LT 28+45.00	30.00	3.0	3.0	0.3	10.00	1.42
SUB-TOTAL =						102.04	3,323.49	1,956.94
TOTAL =						206.65	6,732.63	3,926.07

MANHOLES & WATER VALVES TO BE ADJUSTED				
STATION	DIST	SIDE	MH / WV	
29+32	14'	RT	M. H.	
29+20	20'	RT	M. H.	
28+90	16'	LT	W. V.	

REVISIONS		SHEET 2 OF 2	
NAME	DATE	ILLINOIS DEPARTMENT OF TRANSPORTATION	
		SCHEDULE OF QUANTITIES	
		FAP 317 (US 24)	
		SECTION 14RS-5; 15RS-2	
		SCHUYLER COUNTY	
SCALE:	VERT. DATE: JUNE, 2004	DRAWN BY DJK	
		CHECKED BY CAJ	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5, 15RS-2	SCHUYLER	23	7
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

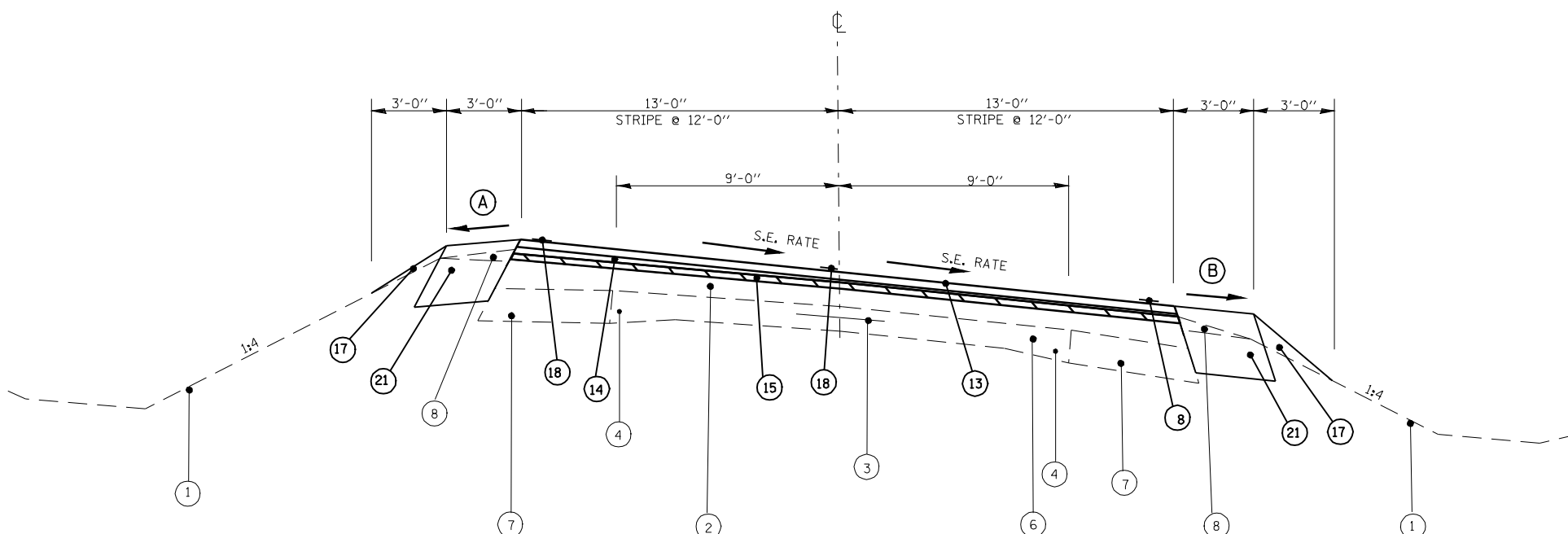


U.S. 24
 STA 362+00.00 TO STA 384+62.83
 STA 394+92.19 TO STA 430+88.62
 STA 440+53.62 TO STA 475+42.72
 STA 489+07.86 TO STA 536+89.09
 STA 564+61.59 TO STA 666+70.64
 STA 682+49.18 TO STA 743+31.15
 STA 1290+95.85 TO STA 1292+23.86 - CONSTRUCT 3' HMA SHLD & 3' AGG SHLDS (PER SE SECTIONS)
 STA 1297+11.88 TO STA 1305+70.53 BK - CONSTRUCT 3' HMA SHLD & 3' AGG SHLDS (PER SE SECTIONS)
 STA 30+00.00 AH TO STA 28+75.00 - CONSTRUCT 3' HMA SHLD & 3' AGG SHLDS (PER SE SECTIONS)

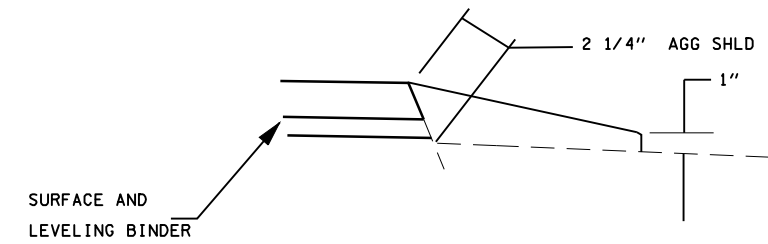
LEGEND

- ① EXISTING EARTH SHOULDERS
- ② EXISTING BITUMINOUS CONCRETE OVERLAY (DEPTH VARIES)
- ③ EXISTING 1/2" Ø TIE BAR
- ④ EXISTING 3/4" Ø SMOOTH BAR
- ⑤ EXISTING PCC PAVEMENT
- ⑥ EXISTING P.C.C. PAVEMENT (9-6-9)
- ⑦ EXISTING BITUMINOUS CONCRETE WIDENING (9")
- ⑧ EXISTING AGGREGATE SHOULDER TY. B
- ⑨ EXISTING BITUMINOUS SHOULDER (8")
- ⑩ EXISTING TYPE A CONCRETE GUTTER
- ⑪ EXISTING BITUMINOUS BASE COURSE 10 1/4"
- ⑫ EXISTING LIME MODIFIED SOIL 12"
- ⑬ PROPOSED HMA SURFACE COURSE, MIX "C" N50 - 1 1/2"
- ⑭ PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- ⑮ PROPOSED HMA SURFACE REMOVAL (VARIABLE DEPTH) - 3/4" AVE
- ⑯ PROPOSED HMA SURFACE REMOVAL 2"
- ⑰ PROPOSED AGGREGATE SHOULDERS, TYPE B (WEDGE)
- ⑱ PROPOSED PAINT PAVEMENT MARKING - LINE 5"
- ⑲ PROPOSED HMA SHOULDER, - 2 1/4" (PAVEMENT) TO 1 1/2" (GUTTER)
- ⑳ PROPOSED SURFACE REMOVAL VAR. DEPTH (3/4" AT PAVEMENT TO 1 1/2" AT GUTTER)
- ㉑ PROPOSED HMA SHOULDER, 8"

- (A) **SHOULDER SLOPE HIGH SIDE OF S.E. :**
 SHOULDER SHALL BE SLOPED AT 1/2 "/FT (4%) IF THE S.E. IS BETWEEN 0% AND 4%. IF THE S.E. IS GREATER THAN 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT CROSS SLOPE AND THE SHOULDER SLOPE DOES NOT EXCEED 8%. WHEN THE S.E. IS 8%, THE SHOULDER SLOPE SHALL BE 1% TOWARD THE LANES OF TRAFFIC TO FACILITATE DRAINAGE.
- (B) **SHOULDER SLOPE LOW SIDE OF S.E. :**
 SHOULDER SHALL BE THE SAME AS S.E., BUT NOT LESS THAN 4%.



US 24
 STA 384+62.83 TO STA 386+92.83 - S.E. TRANSITION
 STA 386+92.83 TO STA 392+62.19 - FULL S.E. - 7.5%
 STA 392+62.19 TO STA 394+92.19 - S.E. TRANSITION
 TRANSITION LENGTH - 230 FEET



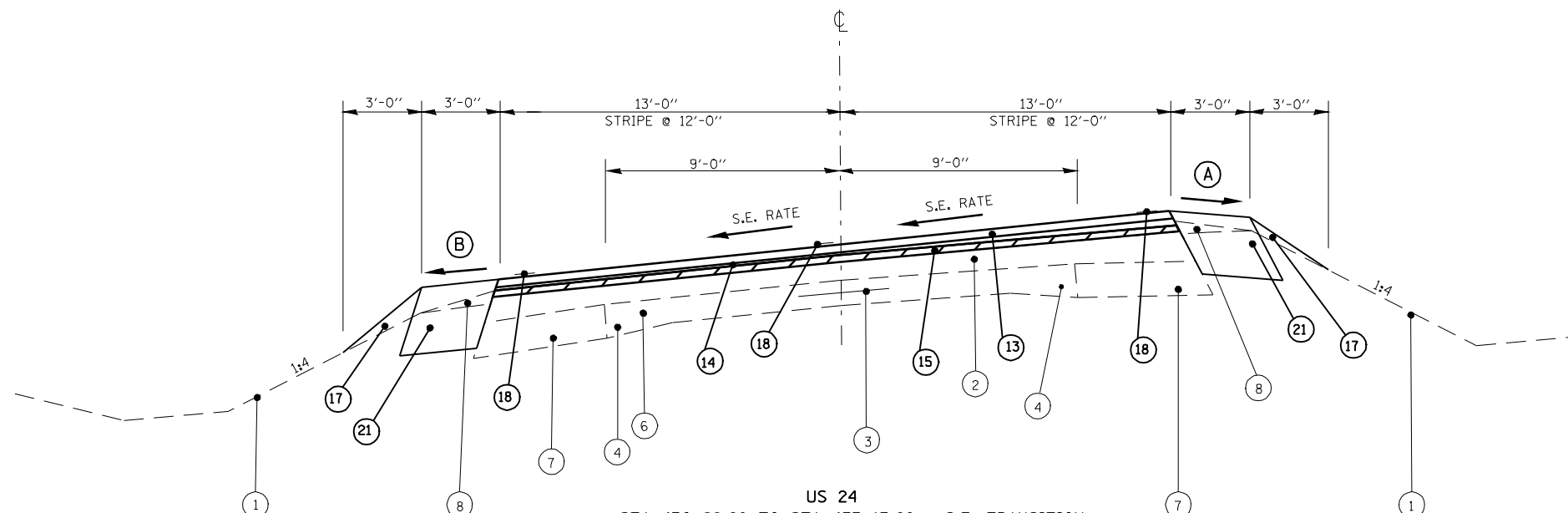
**AGGREGATE SHOULDER
 DETAIL - TYPICAL**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
 FAP 317 (US 24)
 14RS-5, 15RS-2
 SCHUYLER COUNTY
 VERT. SCALE: NONE
 HORIZ. SCALE: NONE
 DATE: JUNE, 2004
 DRAWN BY DJK
 CHECKED BY CAJ

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5, 15RS-2	SCHUYLER	23	8
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



US 24
 STA 430+88.62 TO STA 433+13.62 - S.E. TRANSITION
 STA 433+13.62 TO STA 438+28.62 - FULL S.E. - 6.5%
 STA 438+28.62 TO STA 440+53.62 - S.E. TRANSITION
 TRANSITION LENGTH - 225 FEET

STA 475+42.72 TO STA 477+72.72 - S.E. TRANSITION
 STA 477+72.72 TO STA 486+77.86 - FULL S.E. - 7.5%
 STA 486+77.86 TO STA 489+07.86 - S.E. TRANSITION
 TRANSITION LENGTH - 230 FEET

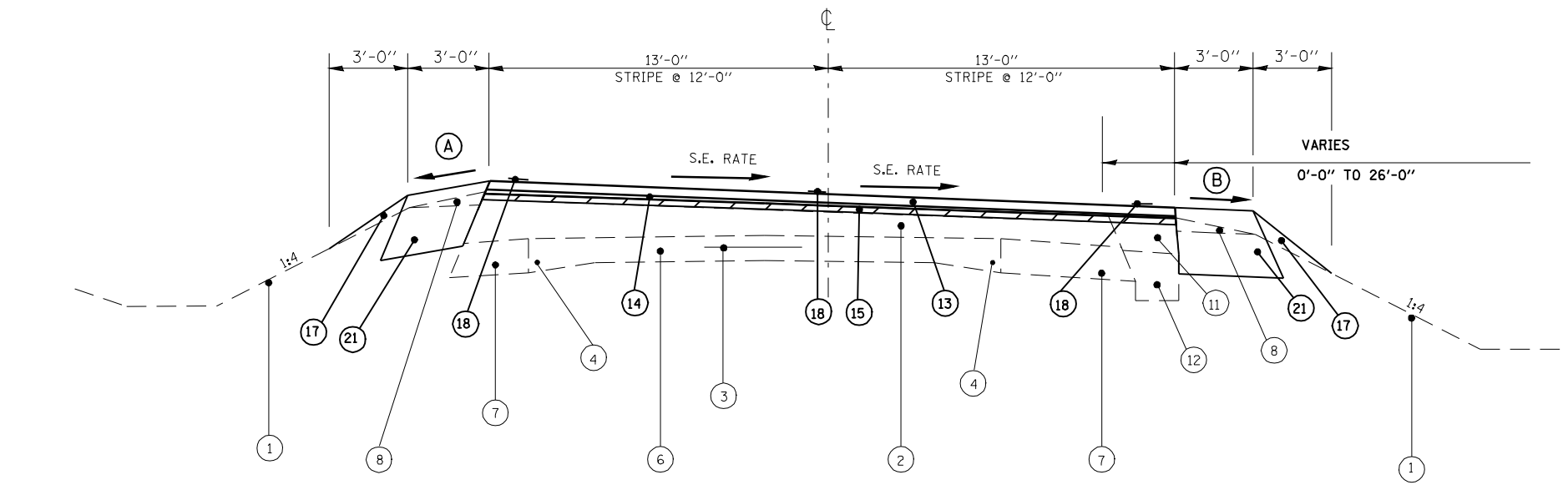
STA 666+70.64 TO STA 669+00.64 - S.E. TRANSITION
 STA 669+00.64 TO STA 680+19.81 - FULL S.E. - 7.5%
 STA 680+19.81 TO STA 682+49.81 - S.E. TRANSITION
 TRANSITION LENGTH - 230 FEET

STA 1292+23.86 TO STA 1293+63.86 - S.E. TRANSITION
 STA 1293+63.86 TO STA 1295+71.88 - FULL S.E. - 3.6%
 STA 1295+71.88 TO STA 1297+11.88 - S.E. TRANSITION
 TRANSITION LENGTH - 140 FEET

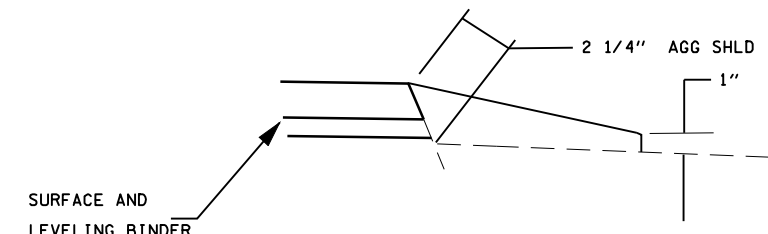
LEGEND

- ① EXISTING EARTH SHOULDERS
- ② EXISTING BITUMINOUS CONCRETE OVERLAY (DEPTH VARIES)
- ③ EXISTING 1/2" Ø TIE BAR
- ④ EXISTING 3/4" Ø SMOOTH BAR
- ⑤ EXISTING PCC PAVEMENT
- ⑥ EXISTING P.C.C. PAVEMENT (9-6-9)
- ⑦ EXISTING BITUMINOUS CONCRETE WIDENING (9")
- ⑧ EXISTING AGGREGATE SHOULDER TY. B
- ⑨ EXISTING BITUMINOUS SHOULDER (8")
- ⑩ EXISTING TYPE A CONCRETE GUTTER
- ⑪ EXISTING BITUMINOUS BASE COURSE 10 1/4"
- ⑫ EXISTING LIME MODIFIED SOIL 12"
- ⑬ PROPOSED HMA SURFACE COURSE, MIX "C" N50 - 1 1/2"
- ⑭ PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- ⑮ PROPOSED HMA SURFACE REMOVAL (VARIABLE DEPTH) - 3/4" AVE
- ⑯ PROPOSED HMA SURFACE REMOVAL 2"
- ⑰ PROPOSED AGGREGATE SHOULDERS, TYPE B (WEDGE)
- ⑱ PROPOSED PAINT PAVEMENT MARKING - LINE 5"
- ⑲ PROPOSED HMA SHOULDER, - 2 1/4" (PAVEMENT) TO 1 1/2" (GUTTER)
- ⑳ PROPOSED SURFACE REMOVAL VAR. DEPTH (3/4" AT PAVEMENT TO 1 1/2" AT GUTTER)
- ㉑ PROPOSED HMA SHOULDER, 8"

- Ⓐ **SHOULDER SLOPE HIGH SIDE OF S.E. †**
 SHOULDER SHALL BE SLOPED AT 1/2 "/FT (4%) IF THE S.E. IS BETWEEN 0% AND 4%. IF THE S.E. IS GREATER THAN 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT CROSS SLOPE AND THE SHOULDER SLOPE DOES NOT EXCEED 8%. WHEN THE S.E. IS 8%, THE SHOULDER SLOPE SHALL BE 1% TOWARD THE LANES OF TRAFFIC TO FACILITATE DRAINAGE.
- Ⓑ **SHOULDER SLOPE LOW SIDE OF S.E. †**
 SHOULDER SHALL BE THE SAME AS S.E., BUT NOT LESS THAN 4%.



US 24
 STA 536+89.04 TO STA 539+14.09 - S.E. TRANSITION
 TRANSITION LENGTH - 140 FEET
 STA 539+14.09 TO STA 541+02.00 - FULL S.E. - 6%
 STA 556+91.00 TO STA 558+79.08 - FULL S.E. - 6%
 STA 558+79.08 TO STA 564+61.59 AHD - S.E. TRANSITION
 TRANSITION LENGTH - 225 FEET
 STA EQUATION 559+54.08 BK =563+11.59 AHD



**AGGREGATE SHOULDER
 DETAIL - TYPICAL**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
 FAP 317 (US 24)
 14RS-5, 15RS-2
 SCHUYLER COUNTY

SCALE: VERT. : NONE
 HORIZ. : NONE
 DATE : JUNE, 2004

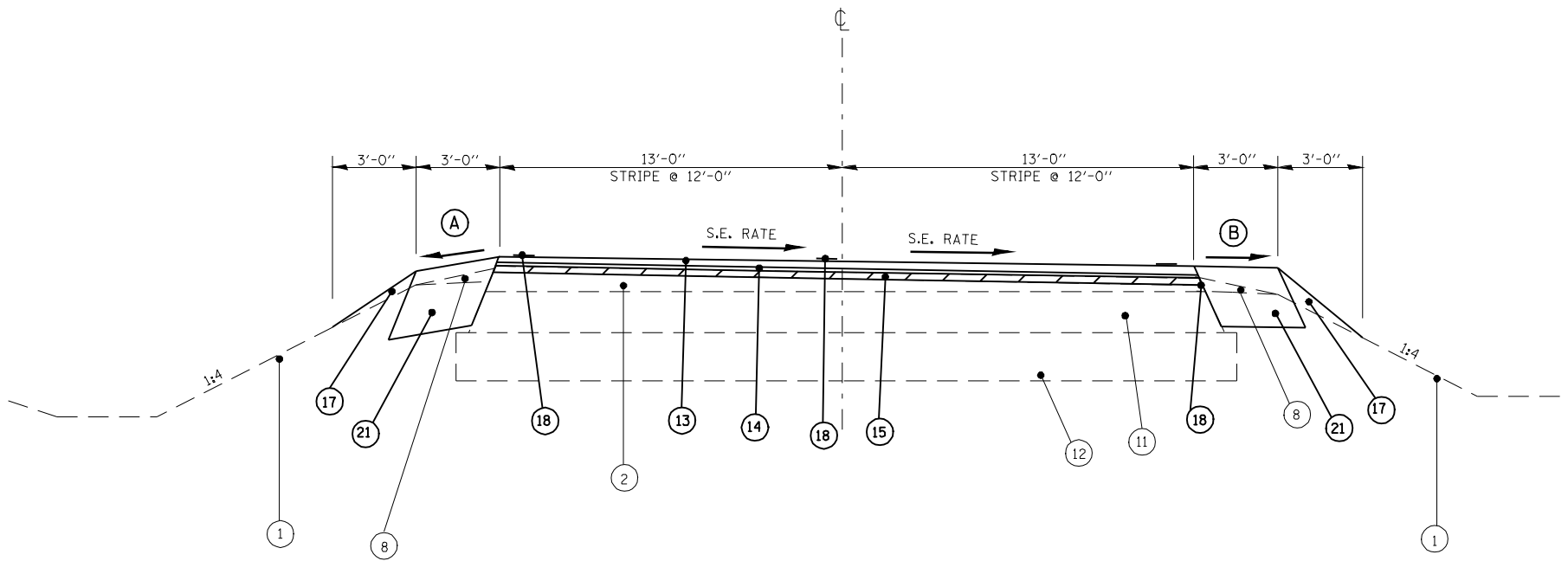
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 CHECKED BY CAJ

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5; 15RS-2	SCHUYLER	23	9
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

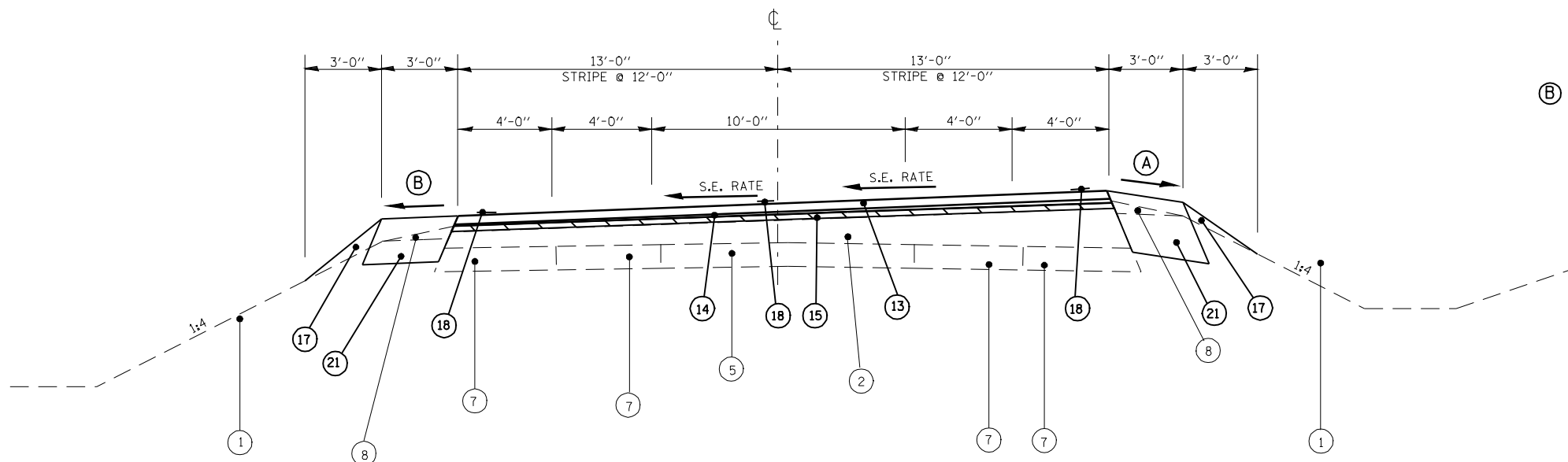
LEGEND

- ① EXISTING EARTH SHOULDERS
- ② EXISTING BITUMINOUS CONCRETE OVERLAY (DEPTH VARIES)
- ③ EXISTING 1/2" Ø TIE BAR
- ④ EXISTING 3/4" Ø SMOOTH BAR
- ⑤ EXISTING PCC PAVEMENT
- ⑥ EXISTING P.C.C. PAVEMENT (9-6-9)
- ⑦ EXISTING BITUMINOUS CONCRETE WIDENING (9")
- ⑧ EXISTING AGGREGATE SHOULDER TY. B
- ⑨ EXISTING BITUMINOUS SHOULDER (8")
- ⑩ EXISTING TYPE A CONCRETE GUTTER
- ⑪ EXISTING BITUMINOUS BASE COURSE 10 1/4"
- ⑫ EXISTING LIME MODIFIED SOIL 12"
- ⑬ PROPOSED HMA SURFACE COURSE, MIX "C" N50 - 1 1/2"
- ⑭ PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- ⑮ PROPOSED HMA SURFACE REMOVAL (VARIABLE DEPTH) - 3/4" AVE
- ⑯ PROPOSED HMA SURFACE REMOVAL 2"
- ⑰ PROPOSED AGGREGATE SHOULDERS, TYPE B (WEDGE)
- ⑱ PROPOSED PAINT PAVEMENT MARKING - LINE 5"
- ⑲ PROPOSED HMA SHOULDER, - 2 1/4" (PAVEMENT) TO 1 1/2" (GUTTER)
- ⑳ PROPOSED SURFACE REMOVAL VAR. DEPTH (3/4" AT PAVEMENT TO 1 1/2" AT GUTTER)
- ㉑ PROPOSED HMA SHOULDER, 8"

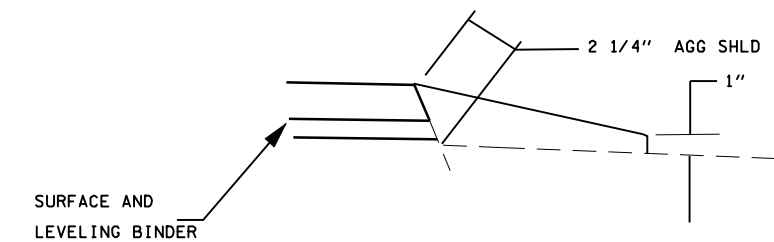


US 24
 STA 541+02.00 TO STA 556+91.00 - FULL S.E. - 6%
 STA 556+91.00 TO STA 558+31.00 - S.E. TRANSITION
 TRANSITION LENGTH - 140 FEET

- Ⓐ SHOULDER SLOPE HIGH SIDE OF S.E. :
 SHOULDER SHALL BE SLOPED AT 1/2 "/FT (4%) IF THE S.E. IS BETWEEN 0% AND 4%.
 IF THE S.E. IS GREATER THAN 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC
 DIFFERENCE BETWEEN THE PAVEMENT CROSS SLOPE AND THE SHOULDER SLOPE
 DOES NOT EXCEED 8%. WHEN THE S.E. IS 8%, THE SHOULDER SLOPE SHALL BE 1%
 TOWARD THE LANES OF TRAFFIC TO FACILITATE DRAINAGE.
- Ⓑ SHOULDER SLOPE LOW SIDE OF S.E. :
 SHOULDER SHALL BE THE SAME AS S.E., BUT NOT LESS THAN 4%.



US 24
 STA 743+31.15 TO STA 744+71.15 - S.E. TRANSITION
 TRANSITION LENGTH - 140 FEET
 STA 744+71.15 TO STA 745+39.13 - FULL S.E. - 3.6%
 STA 745+39.13 TO STA 746+79.13 - S.E. TRANSITION
 TRANSITION LENGTH - 140 FEET



**AGGREGATE SHOULDER
 DETAIL - TYPICAL**

REVISIONS	
NAME	DATE

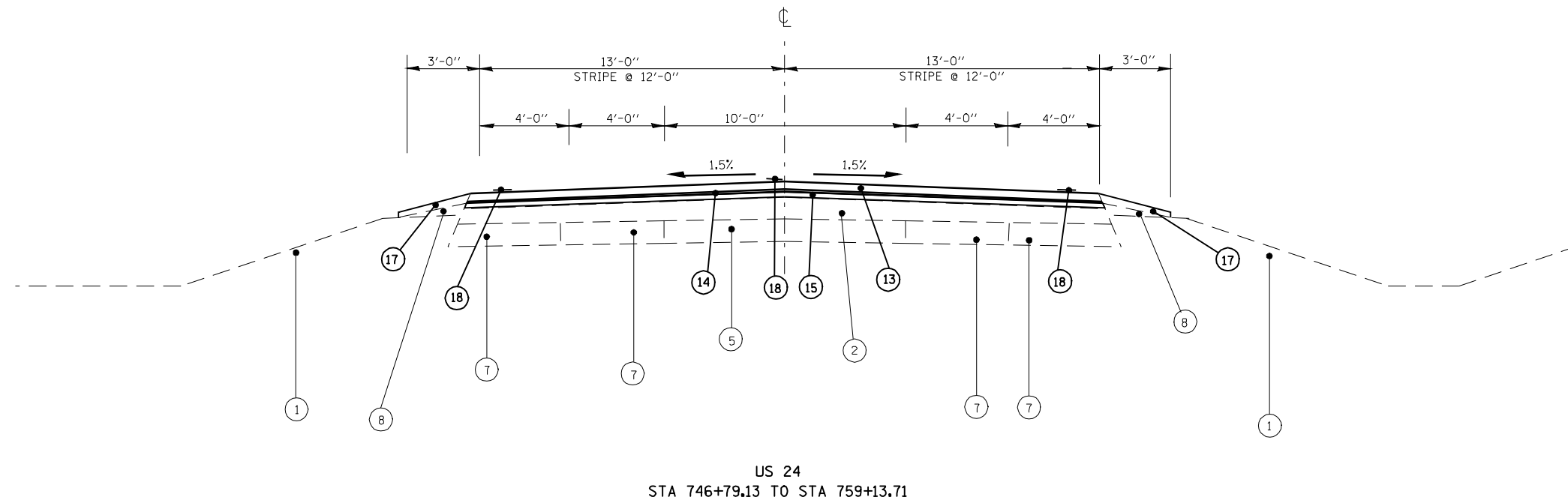
ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
 FAP 317 (US 24)
 14RS-5; 15RS-2
 SCHUYLER COUNTY
 VERT. : NONE
 HORIZ. : NONE
 DATE : JUNE, 2004
 DRAWN BY DJK
 CHECKED BY CAJ

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5, 15RS-2	SCHUYLER	24	10
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

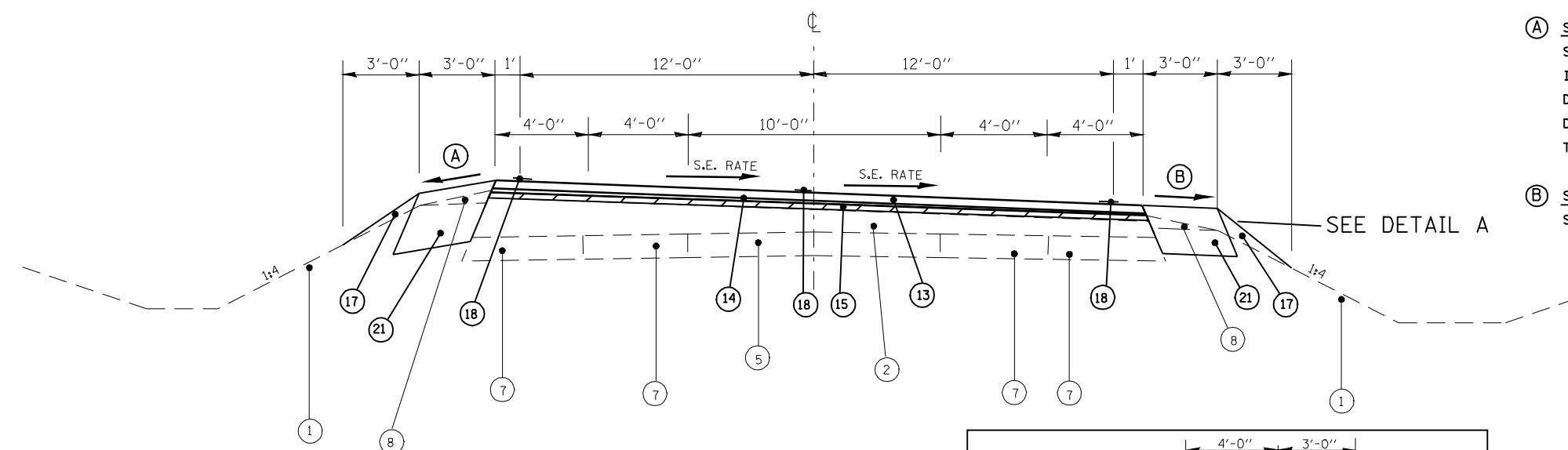
LEGEND

- ① EXISTING EARTH SHOULDERS
- ② EXISTING BITUMINOUS CONCRETE OVERLAY (DEPTH VARIES)
- ③ EXISTING 1/2" Ø TIE BAR
- ④ EXISTING 3/4" Ø SMOOTH BAR
- ⑤ EXISTING PCC PAVEMENT
- ⑥ EXISTING P.C.C. PAVEMENT (9-6-9)
- ⑦ EXISTING BITUMINOUS CONCRETE WIDENING (9")
- ⑧ EXISTING AGGREGATE SHOULDER TY. B
- ⑨ EXISTING BITUMINOUS SHOULDER (8")
- ⑩ EXISTING TYPE A CONCRETE GUTTER
- ⑪ EXISTING BITUMINOUS BASE COURSE 10 1/4"
- ⑫ EXISTING LIME MODIFIED SOIL 12"
- ⑬ PROPOSED HMA SURFACE COURSE, MIX "C" N50 - 1 1/2"
- ⑭ PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- ⑮ PROPOSED HMA SURFACE REMOVAL (VARIABLE DEPTH) - 3/4" AVE
- ⑯ PROPOSED HMA SURFACE REMOVAL 2"
- ⑰ PROPOSED AGGREGATE SHOULDERS, TYPE B (WEDGE)
- ⑱ PROPOSED PAINT PAVEMENT MARKING - LINE 5"
- ⑲ PROPOSED HMA SHOULDER, - 2 1/4" (PAVEMENT) TO 1 1/2" (GUTTER)
- ⑳ PROPOSED SURFACE REMOVAL VAR. DEPTH (3/4" AT PAVEMENT TO 1 1/2" AT GUTTER)
- ㉑ PROPOSED HMA SHOULDER, 8"

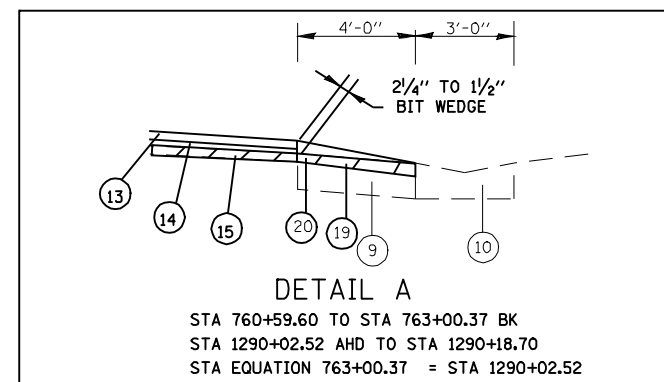


US 24
STA 746+79.13 TO STA 759+13.71

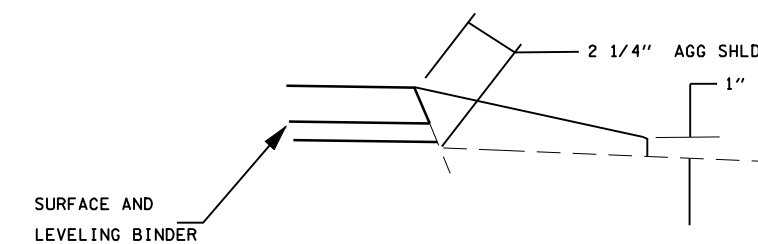
- (A) **SHOULDER SLOPE HIGH SIDE OF S.E. :**
SHOULDER SHALL BE SLOPED AT 1/2 "/ft (4%) IF THE S.E. IS BETWEEN 0% AND 4%. IF THE S.E. IS GREATER THAN 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT CROSS SLOPE AND THE SHOULDER SLOPE DOES NOT EXCEED 8%. WHEN THE S.E. IS 8%, THE SHOULDER SLOPE SHALL BE 1% TOWARD THE LANES OF TRAFFIC TO FACILITATE DRAINAGE.
- (B) **SHOULDER SLOPE LOW SIDE OF S.E. :**
SHOULDER SHALL BE THE SAME AS S.E., BUT NOT LESS THAN 4%.



STA 759+13.71 TO STA 760+53.71 - S.E. TRANSITION
STA 760+53.71 TO STA 761+25.00 - FULL S.E. - 3.6%
TRANSITION LENGTH - 140 FEET



DETAIL A
STA 760+59.60 TO STA 763+00.37 BK
STA 1290+02.52 AHD TO STA 1290+18.70
STA EQUATION 763+00.37 = STA 1290+02.52



AGGREGATE SHOULDER
DETAIL - TYPICAL

REVISIONS	
NAME	DATE

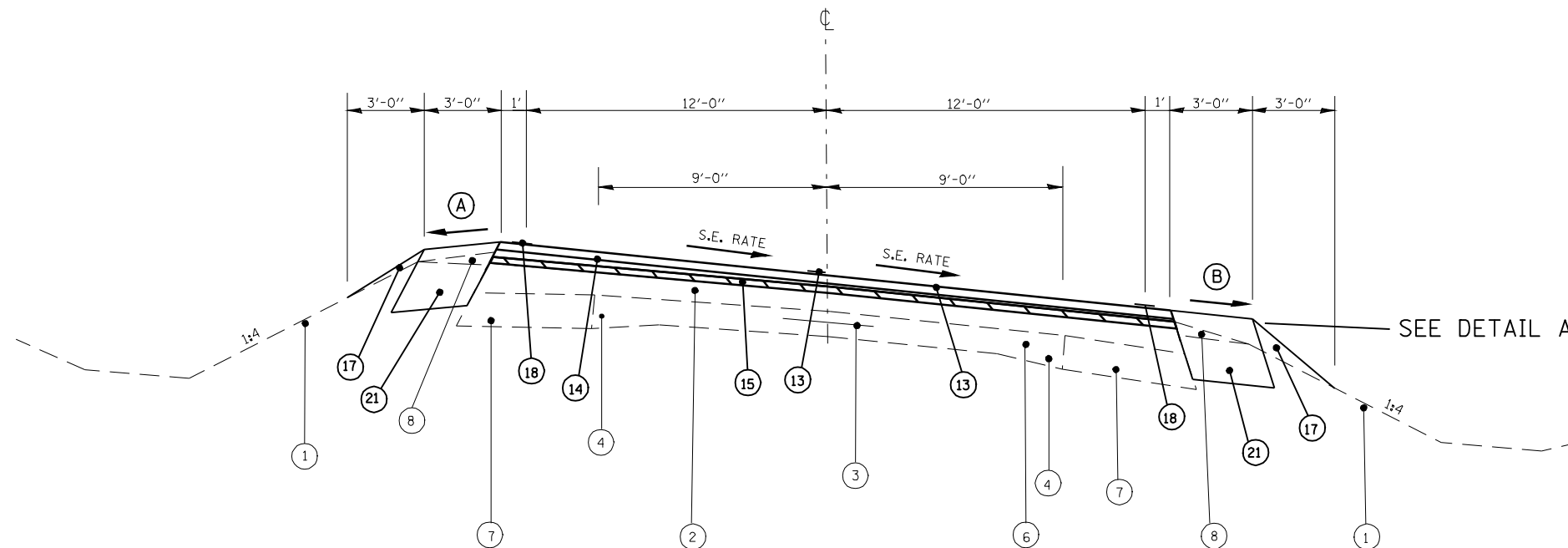
ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
FAP 317 (US 24)
14RS-5, 15RS-2
SCHUYLER COUNTY

VERT. : NONE
HORIZ. : NONE
DATE : JUNE, 2004
DRAWN BY DJK
CHECKED BY CAJ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5; 15RS-2	SCHUYLER	23	11
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

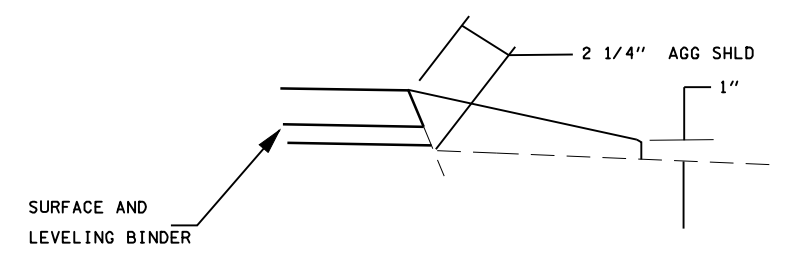
LEGEND

- ① EXISTING EARTH SHOULDERS
- ② EXISTING BITUMINOUS CONCRETE OVERLAY (DEPTH VARIES)
- ③ EXISTING 1/2" Ø TIE BAR
- ④ EXISTING 3/4" Ø SMOOTH BAR
- ⑤ EXISTING PCC PAVEMENT
- ⑥ EXISTING P.C.C. PAVEMENT (9-6-9)
- ⑦ EXISTING BITUMINOUS CONCRETE WIDENING (9")
- ⑧ EXISTING AGGREGATE SHOULDER TY. B
- ⑨ EXISTING BITUMINOUS SHOULDER (8")
- ⑩ EXISTING TYPE A CONCRETE GUTTER
- ⑪ EXISTING BITUMINOUS BASE COURSE 10 1/4"
- ⑫ EXISTING LIME MODIFIED SOIL 12"
- ⑬ PROPOSED HMA SURFACE COURSE, MIX "C" N50 - 1 1/2"
- ⑭ PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- ⑮ PROPOSED HMA SURFACE REMOVAL (VARIABLE DEPTH) - 3/4" AVE
- ⑯ PROPOSED HMA SURFACE REMOVAL 2"
- ⑰ PROPOSED AGGREGATE SHOULDERS, TYPE B (WEDGE)
- ⑱ PROPOSED PAINT PAVEMENT MARKING - LINE 5"
- ⑲ PROPOSED HMA SHOULDER, - 2 1/4" (PAVEMENT) TO 1 1/2" (GUTTER)
- ⑳ PROPOSED SURFACE REMOVAL VAR. DEPTH (3/4" AT PAVEMENT TO 1 1/2" AT GUTTER)
- ㉑ PROPOSED HMA SHOULDER, 8"



US 24
 STA 761+25.00 TO STA 762+53.70 - FULL S.E. - 3.6%
 STA 762+53.70 BK TO STA 1290+95.85 AHD - S.E. TRANSITION
 TRANSITION LENGTH - 140 FEET STA EQUATION 763+00.37 = STA 1290+02.52

- Ⓐ **SHOULDER SLOPE HIGH SIDE OF S. E. †**
 SHOULDER SHALL BE SLOPED AT 1/2 "/ft (4%) IF THE S. E. IS BETWEEN 0% AND 4%. IF THE S. E. IS GREATER THAN 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT CROSS SLOPE AND THE SHOULDER SLOPE DOES NOT EXCEED 8%. WHEN THE S. E. IS 8%, THE SHOULDER SLOPE SHALL BE 1% TOWARD THE LANES OF TRAFFIC TO FACILITATE DRAINAGE.
- Ⓑ **SHOULDER SLOPE LOW SIDE OF S. E. †**
 SHOULDER SHALL BE THE SAME AS S. E., BUT NOT LESS THAN 4%.



**AGGREGATE SHOULDER
 DETAIL - TYPICAL**

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		TYPICAL SECTIONS

FAP 317 (US 24)
 14RS-5; 15RS-2
 SCHUYLER COUNTY

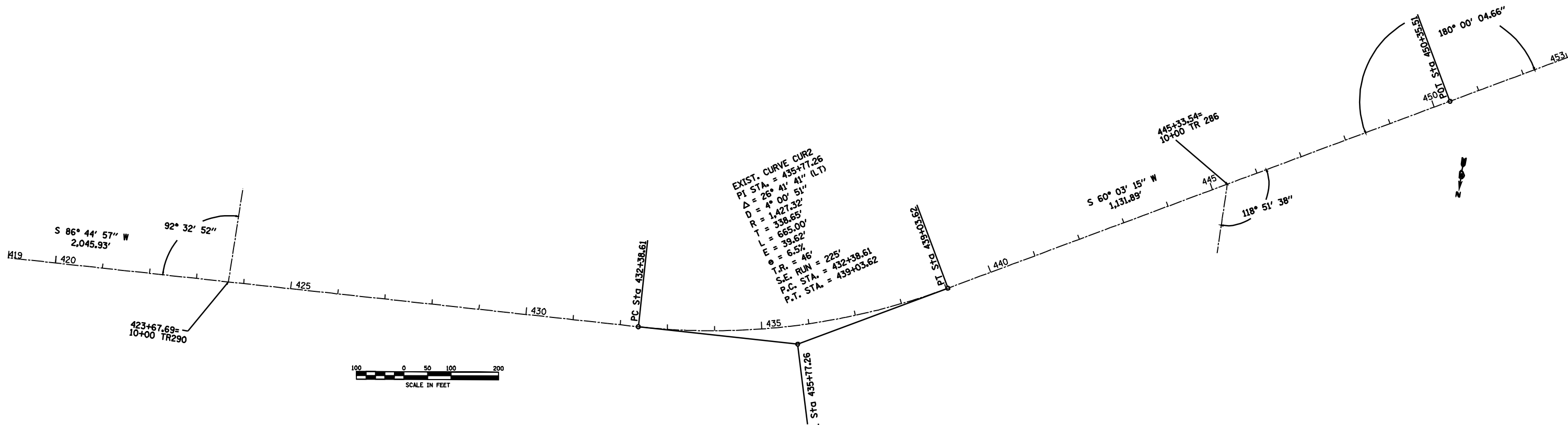
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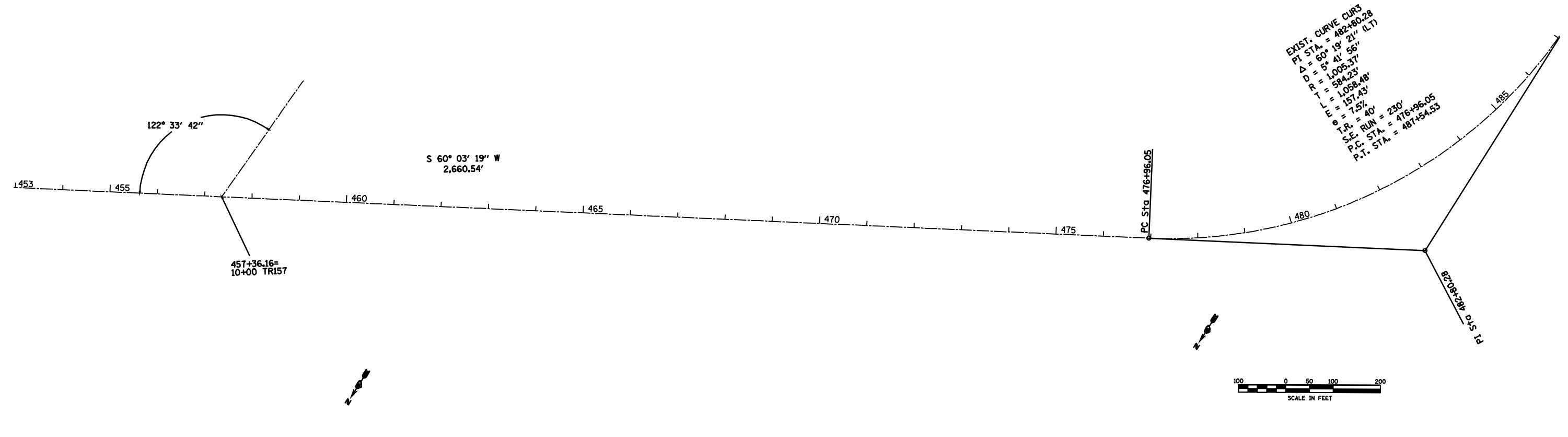
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FAP RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5+15RS-2	SCHUYLER	23	13
STA. 419+00		TO STA. 487+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PLAN	SURVEYED	BY	DATE
NO.	PLOTTED		
	CHECKED		
	BY		
	DATE		



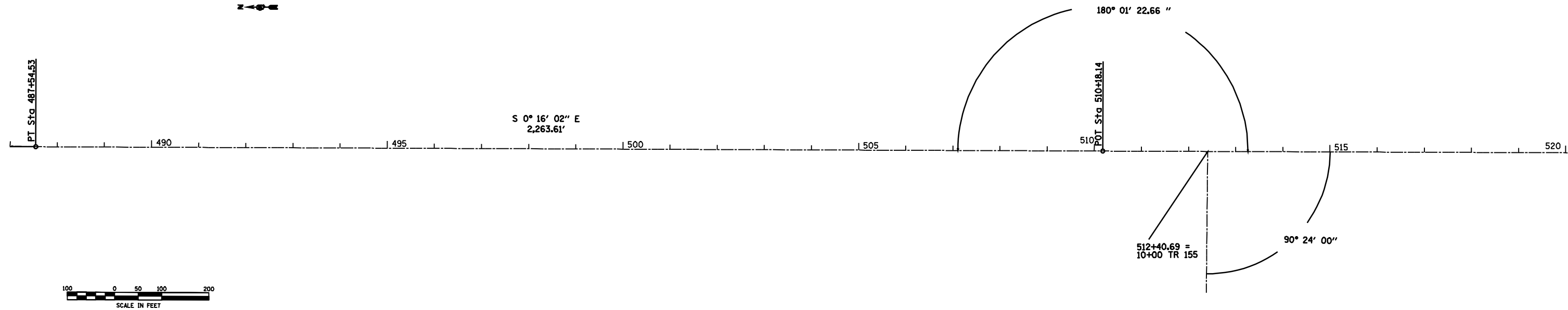
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	BY		
	DATE		



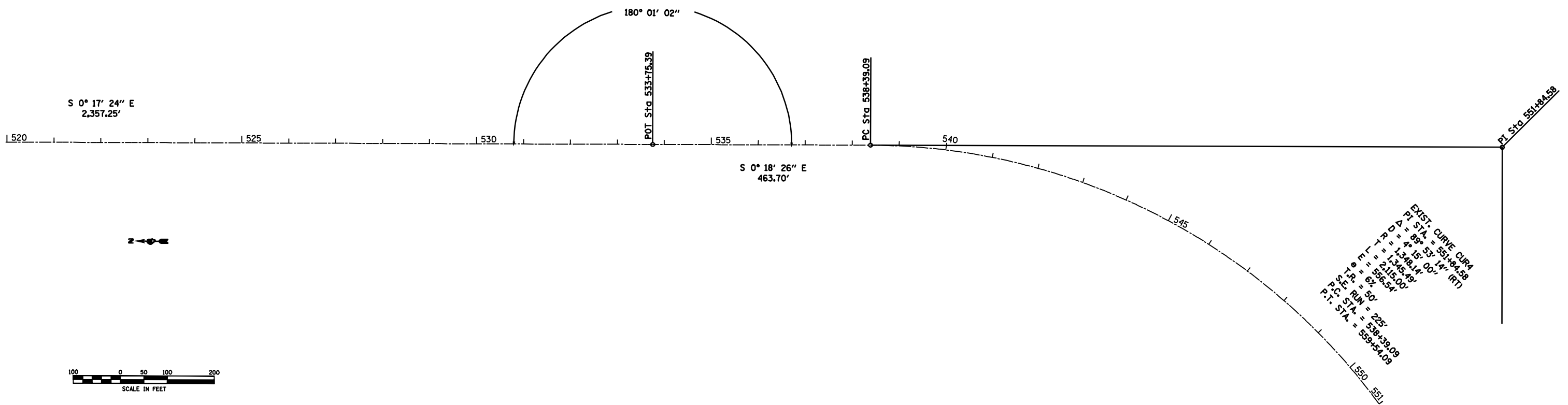
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FAP RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
317	14RS-5+15RS-2	SCHUYLER	23	14
STA. 487+00		TO STA. 551+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	BY		
	NO. OF WAY CHECKED		
	CADD FILE NAME		



PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	BY		
	STRUCTURE NOTATIONS CHRD		

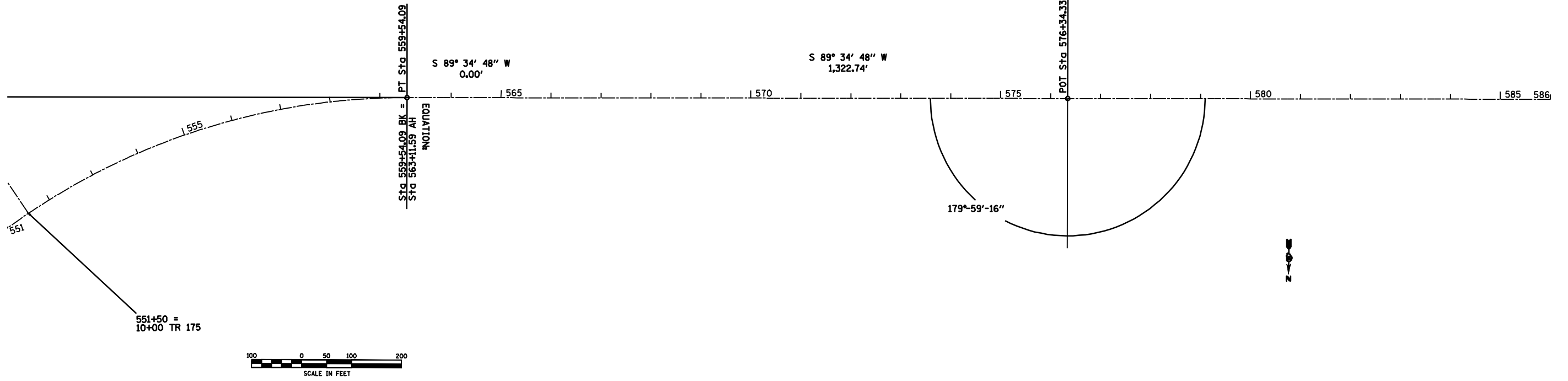


EXIST. CURVE CUR4
 P.T. STA. = 551+34.58
 $\Delta = 89^\circ 53' 14''$ (RT)
 $L = 1,248.14'$
 $T = 2,115.00'$
 $E = 565.54'$
 $P.T. \theta = 61^\circ 17' 00''$
 $P.C. STA. = 538+39.09$
 $P.I. STA. = 559+54.09$
 $P.C. STA. = 559+54.09$

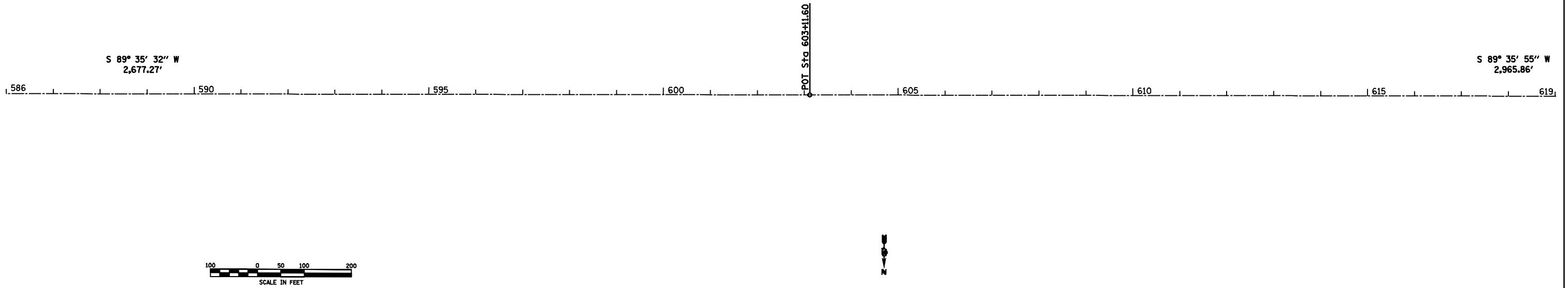
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FAP RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5+15RS-2	SCHUYLER	23	15
STA. 551+00		TO STA. 619+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	CHECKED		
	BY		
	DATE		



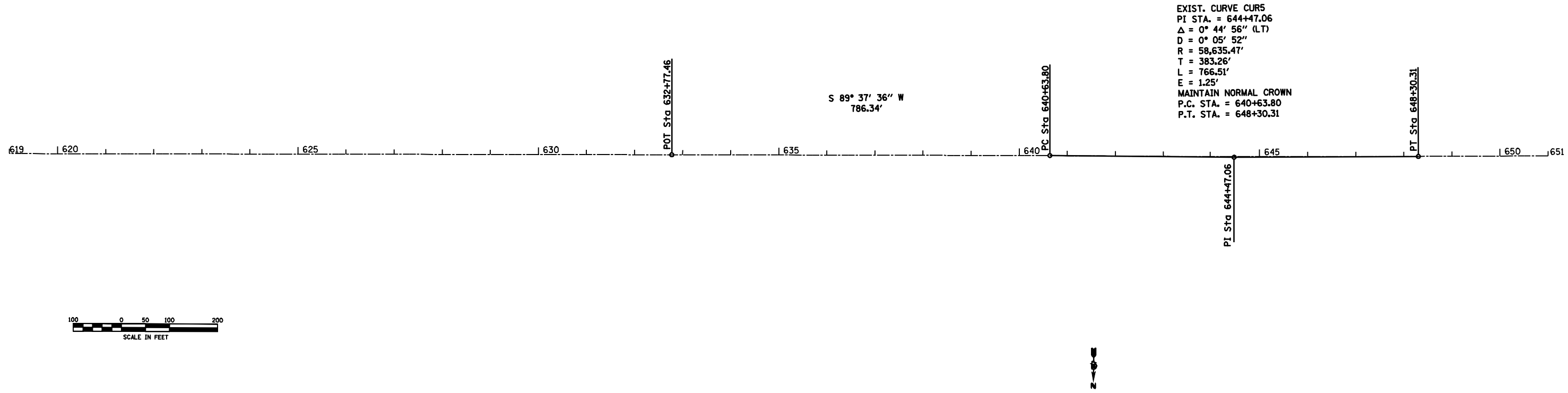
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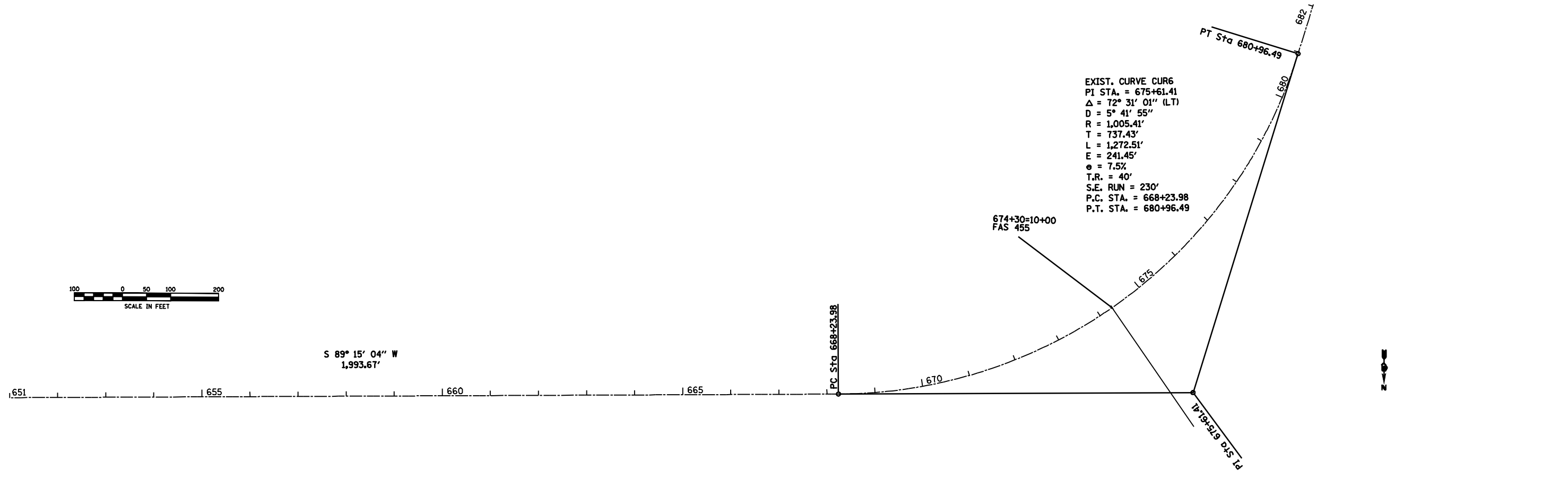
FAP RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5+15RS-2	SCHUYLER	23	16
STA. 619+00		TO STA. 682+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PLAN	SURVEYED	BY	DATE
NOTE BOOK NO. _____	PLOTTED _____		
	CHECKED BY _____		
	DATE _____		
	FILE NAME _____		



EXIST. CURVE CUR5
 PI STA. = 644+47.06
 Δ = 0° 44' 56" (LT)
 D = 0° 05' 52"
 R = 58,635.47'
 T = 383.26'
 L = 766.51'
 E = 1.25'
 MAINTAIN NORMAL CROWN
 P.C. STA. = 640+63.80
 P.T. STA. = 648+30.31

PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO. _____	PLOTTED _____		
	CHECKED BY _____		
	DATE _____		
	FILE NAME _____		

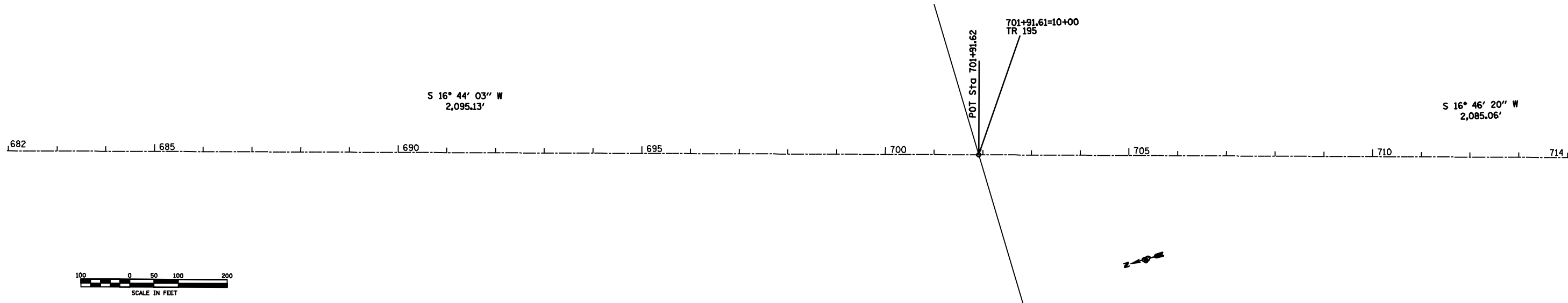


EXIST. CURVE CUR6
 PI STA. = 675+61.41
 Δ = 72° 31' 01" (LT)
 D = 5° 41' 55"
 R = 1,005.41'
 T = 737.43'
 L = 1,272.51'
 E = 241.45'
 e = 7.5%
 T.R. = 40'
 S.E. RUN = 230'
 P.C. STA. = 668+23.98
 P.T. STA. = 680+96.49

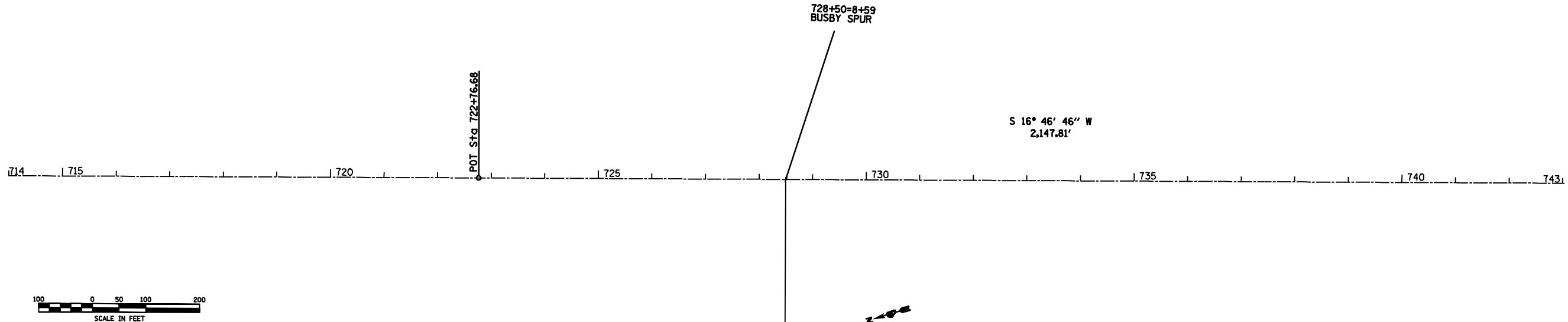
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FAP RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
317	14RS-5+15RS-2	SCHUYLER	23	17
STA. 682+00		TO STA. 743+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PLAN	SURVEYED	BY	DATE
NO. _____	PLOTTED _____	_____	_____
NOTE BOOK _____	CHECKED _____	_____	_____
	BY _____	_____	_____
	DATE _____	_____	_____



PROFILE	SURVEYED	BY	DATE
NO. _____	PLOTTED _____	_____	_____
	CHECKED _____	_____	_____
	BY _____	_____	_____
	DATE _____	_____	_____



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FAP RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
317	14RS-5,15RS-2	SCHUYLER	23	18
STA. 743+00		TO STA. 28+75		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

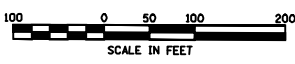
PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	DATE		
	NO.		

EXIST. CURVE CUR7
 PI STA. = 745+05.69
 $\Delta = 16^\circ 10' 08''$ (LT)
 $D = 10^\circ 01' 23''$
 $R = 571.65'$
 $T = 81.20'$
 $L = 161.32'$
 $E = 5.74'$
 $e = 3.6\%$
 $T.R. = 64'$
 $S.E. RUN = 140'$
 $P.C. STA. = 744+24.49$
 $P.T. STA. = 745+85.81$

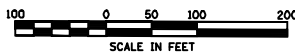
EXIST. CURVE CUR8
 PI STA. = 761+57.71
 $\Delta = 32^\circ 13' 41''$ (RT)
 $D = 10^\circ 59' 13''$
 $R = 521.49'$
 $T = 150.66'$
 $L = 293.33'$
 $E = 21.33'$
 $e = 3.6\%$
 $T.R. = 64'$
 $S.E. RUN = 140'$
 $P.C. STA. = 760+07.05$
 $P.T. STA. = 763+00.39$

EXIST. CURVE CUR9
 PI STA. = 1294+72.20
 $\Delta = 33^\circ 05' 00''$ (LT)
 $D = 10^\circ 58' 41''$
 $R = 521.91'$
 $T = 155.01'$
 $L = 301.36'$
 $E = 22.53'$
 $e = 3.6\%$
 $T.R. = 64'$
 $S.E. RUN = 140'$
 $P.C. STA. = 1293+17.19$
 $P.T. STA. = 1296+18.54$

CONTINUE 3' HMA SHLD THRU TANGENT SECTION BETWEEN CURVES - SHORT DISTANCE NO NEED TO STOP

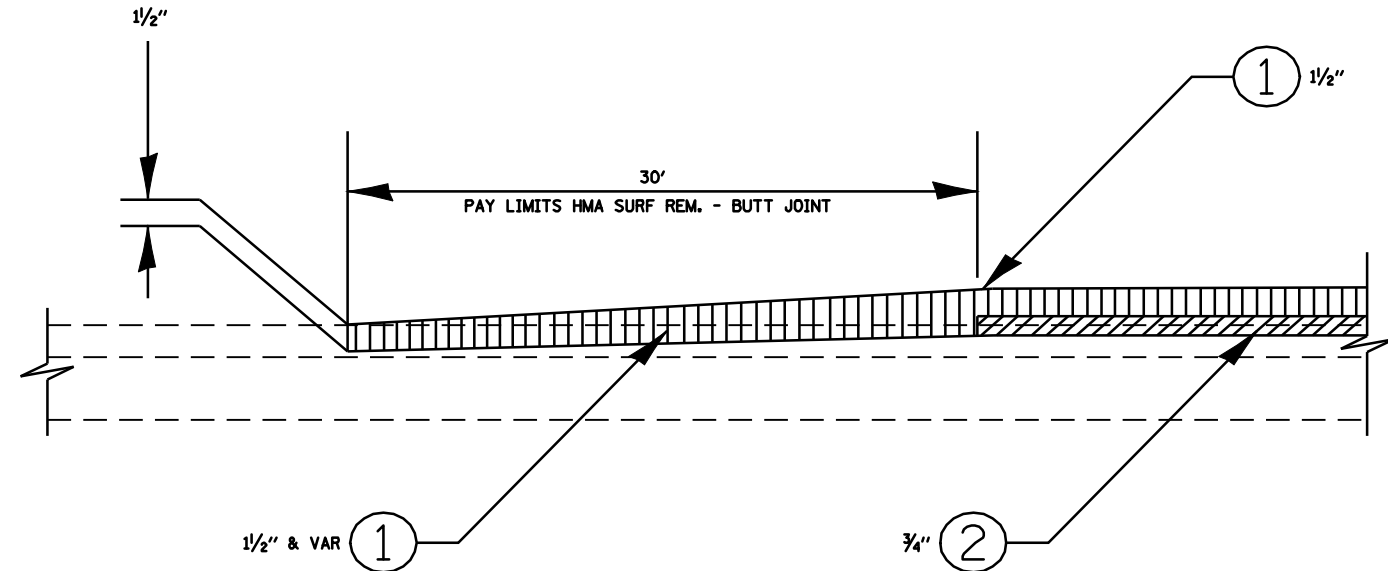


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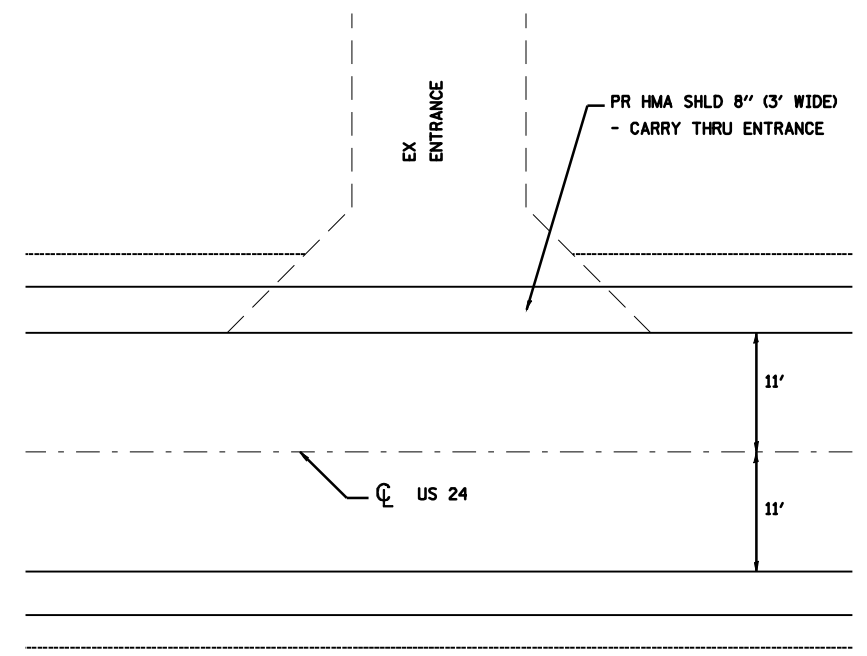
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5; 15RS-2	SCHUYLER	23	19
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



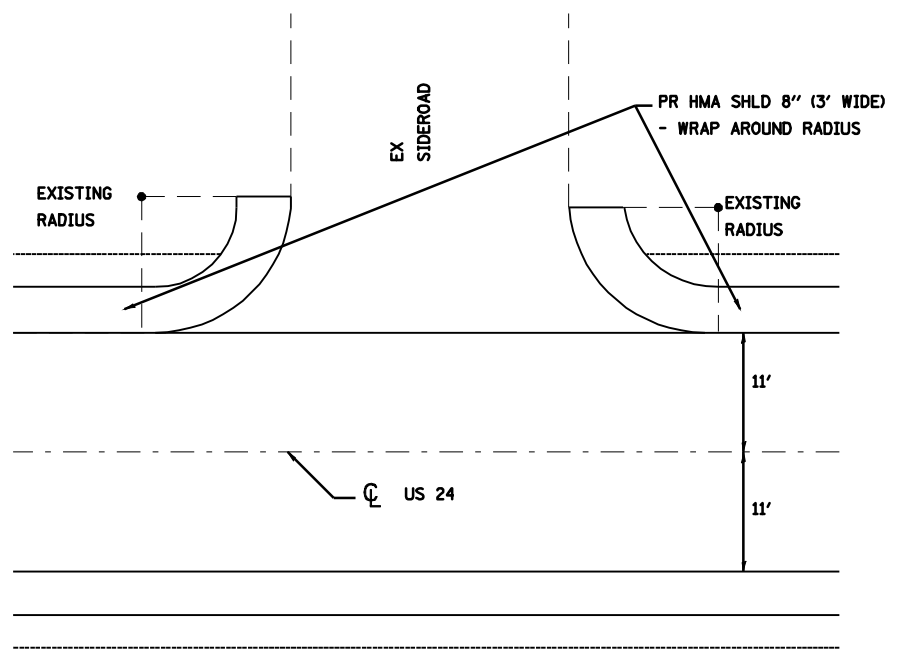
- 1. [Hatched Pattern] PROPOSED BITUMINOUS SURFACE COURSE
- 2. [Diagonal Hatched Pattern] PROPOSED BITUMINOUS LEVELING BINDER

LOCATION	A	B	LENGTH	AREA
			FOOT	SQ YD
FAP 317	362+00	362+30	30.0	86.67
FAP 317	28+70	29+00	30.0	86.67
TOTALS				173.33
USE				173.33

HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT



**ENTRANCE DETAIL
PLAN VIEW**



**SIDEROAD DETAIL
PLAN VIEW**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

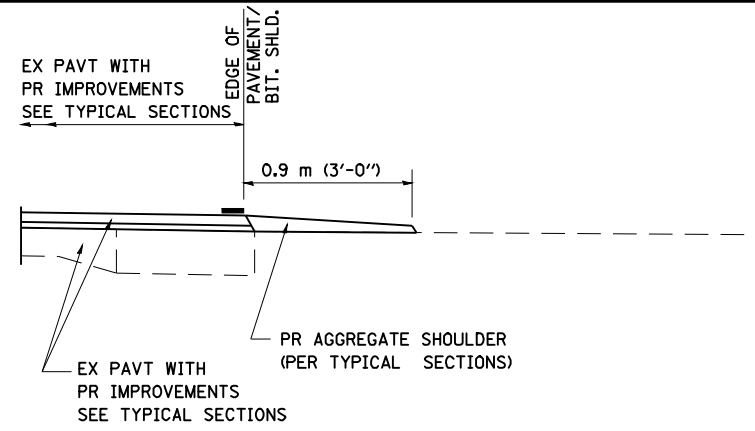
BUTT JOINT DETAIL

FAP 317 (US 24)
SECTION 14RS-5; 15RS-2
SCHUYLER COUNTY

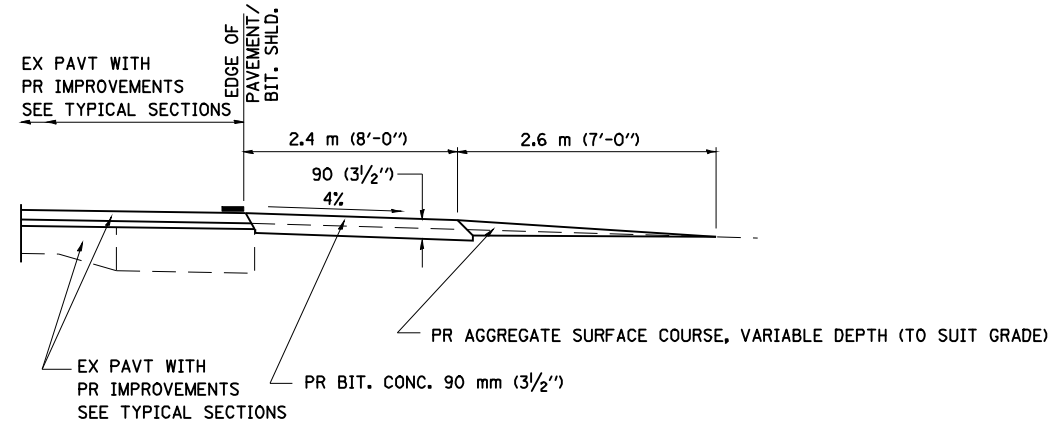
SCALE: VERT. 1"=10'
HORIZ. 1"=40'
DATE: JUNE, 2004

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CHECKED BY CAJ

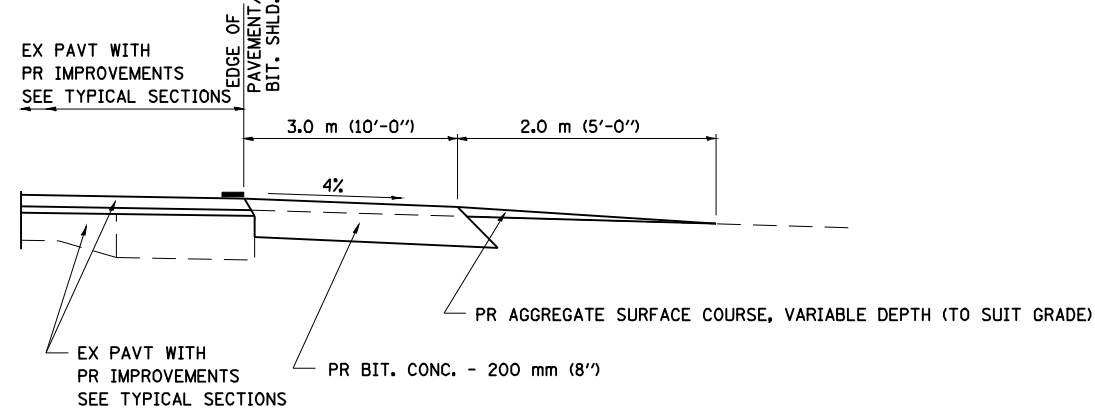
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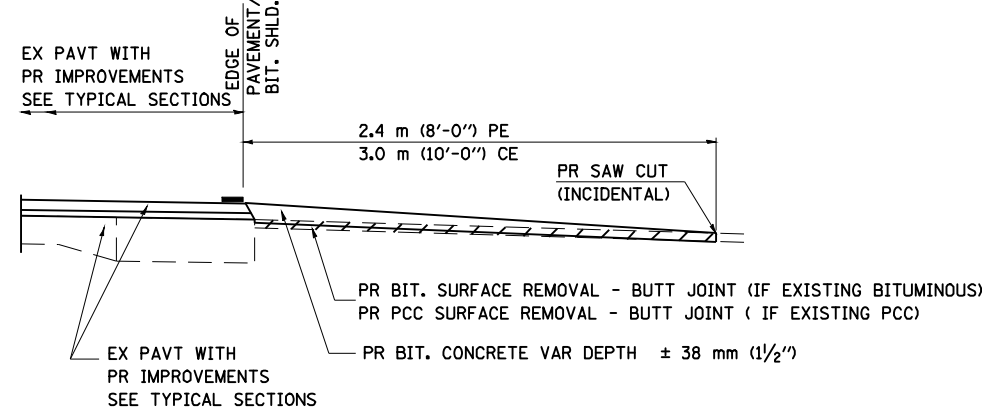
SECTION A-A FOR EX EARTH/AGGREGATE FE



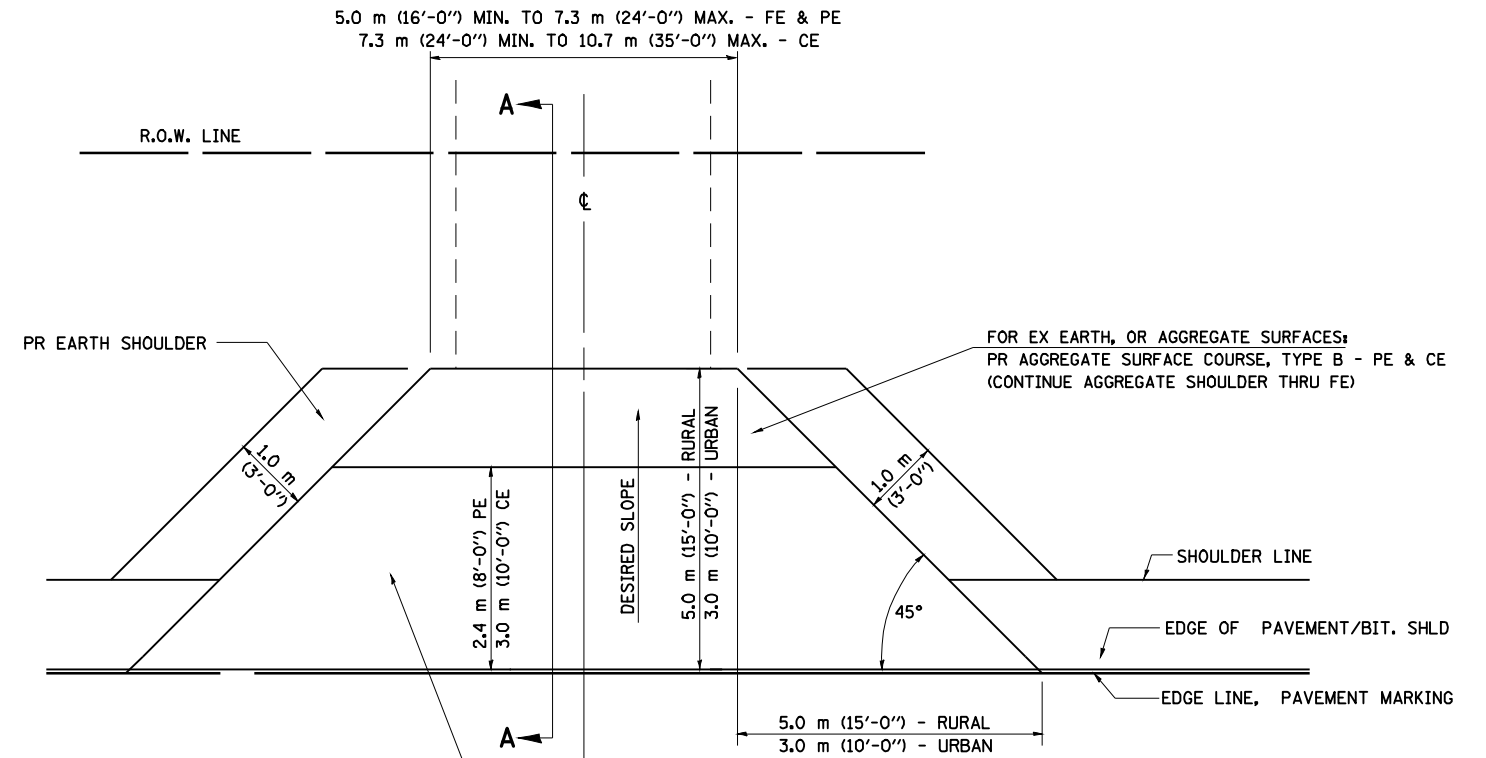
SECTION A-A FOR EX EARTH/AGGREGATE PE



SECTION A-A FOR EX EARTH/AGGREGATE CE & SIDE ROAD



SECTION A-A FOR EX BITUMINOUS/PC CONCRETE PE, CE & SIDE ROAD



FOR EX EARTH OR AGGREGATE SURFACES:
 PR BIT SURFACE REMOVAL (IF APPLICABLE)
 PR AGGREGATE SHOULDER THRU - FE
 PR BITUMINOUS CONCRETE 90 mm (3 1/2") - PE
 PR BITUMINOUS CONCRETE 200mm (8") - CE

FOR EX BITUMINOUS CONCRETE SURFACES:
 PR BITUMINOUS SURFACE REMOVAL-BUTT JOINT

FOR EX PCC SURFACES:
 PR PCC SURFACE REMOVAL-BUTT JOINT

GENERAL NOTES:

THE RESIDENT ENGINEER WILL DETERMINE THE EXACT TYPE OF IMPROVEMENT TO BE COMPLETED FOR ALL ENTRANCES, SIDEROADS AND MAILBOX TURNOUTS ON THIS PROJECT.

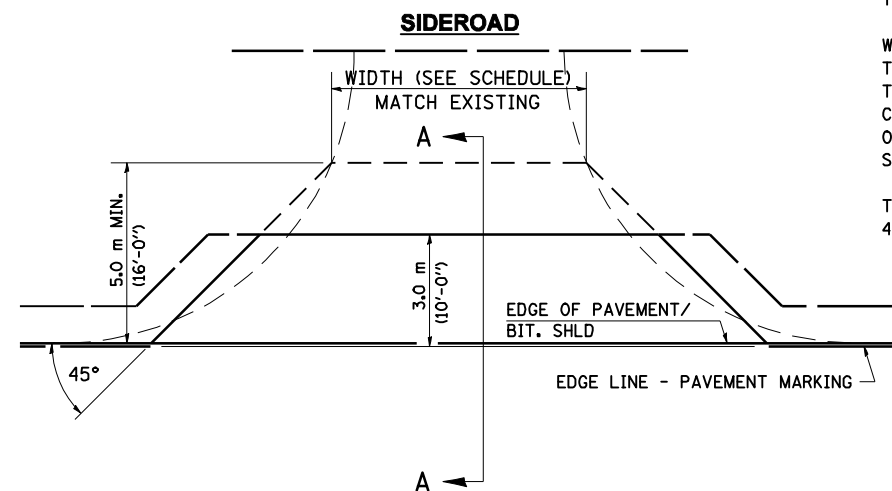
THE PLAN DETAILS AND SCHEDULES SHOULD BE USED AS A GUIDE FOR THE ENGINEER TO IMPLEMENT THE FINAL DESIGN. THE ENGINEER MAY DECIDE TO SALVAGE PORTIONS OF THE EXISTING ENTRANCE PAVEMENT STRUCTURE; THEREFORE, REDUCING PAY ITEM QUANTITIES. NO ADDITIONAL PAYMENT WILL BE ALLOWED FOR THIS REDUCTION IN QUANTITIES.

ANY WORK THE ENGINEER REQUIRES WHICH IS NOT COVERED BY A PAY ITEM CONTAINED IN THE PLANS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

BITUMINOUS CONCRETE REQUIRED TO CONSTRUCT THE ENTRANCES SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 406 AND 408 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.

WHEN THE BITUMINOUS CONCRETE PROPOSED FOR THE IMPROVEMENT IS THICKER THAN 75 mm (3 INCHES) AND REQUIRE PLACEMENT IN MORE THAN ONE LIFT. THE BOTTOM LIFT(S) SHALL MEET THE REQUIREMENTS OF BITUMINOUS BASE COURSE IN SECTION 406 OF THE STANDARD SPECIFICATIONS AND THE TOP LIFT OF 50 mm (2 INCHES) SHALL MEET THE REQUIREMENTS OF BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE.

THIS WORK WILL BE PAID FOR IN ACCORDANCE WITH SECTIONS 351, 358, 408, 423 AND 440 OF THE STANDARD SPECIFICATIONS.



NOTE : IF BIT. SHLDS ARE PROPOSED THEY SHOULD NOT EXTEND THROUGH SIDEROADS

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = laughlinr1	DESIGNED - JCN	REVISED - JCN	2/19/03
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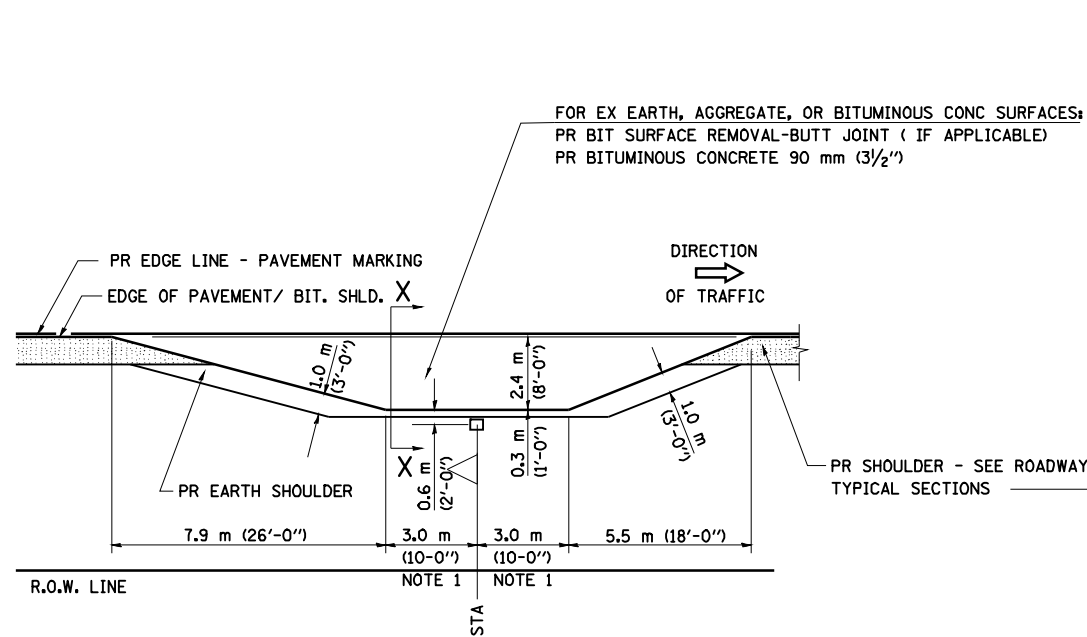
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT SIX DETAILS FOR RURAL / URBAN
ENTRANCE, MAILBOX TURNOUT & SIDEROADS
W / O CONCRETE GUTTER (PPP - PROJECTS)**

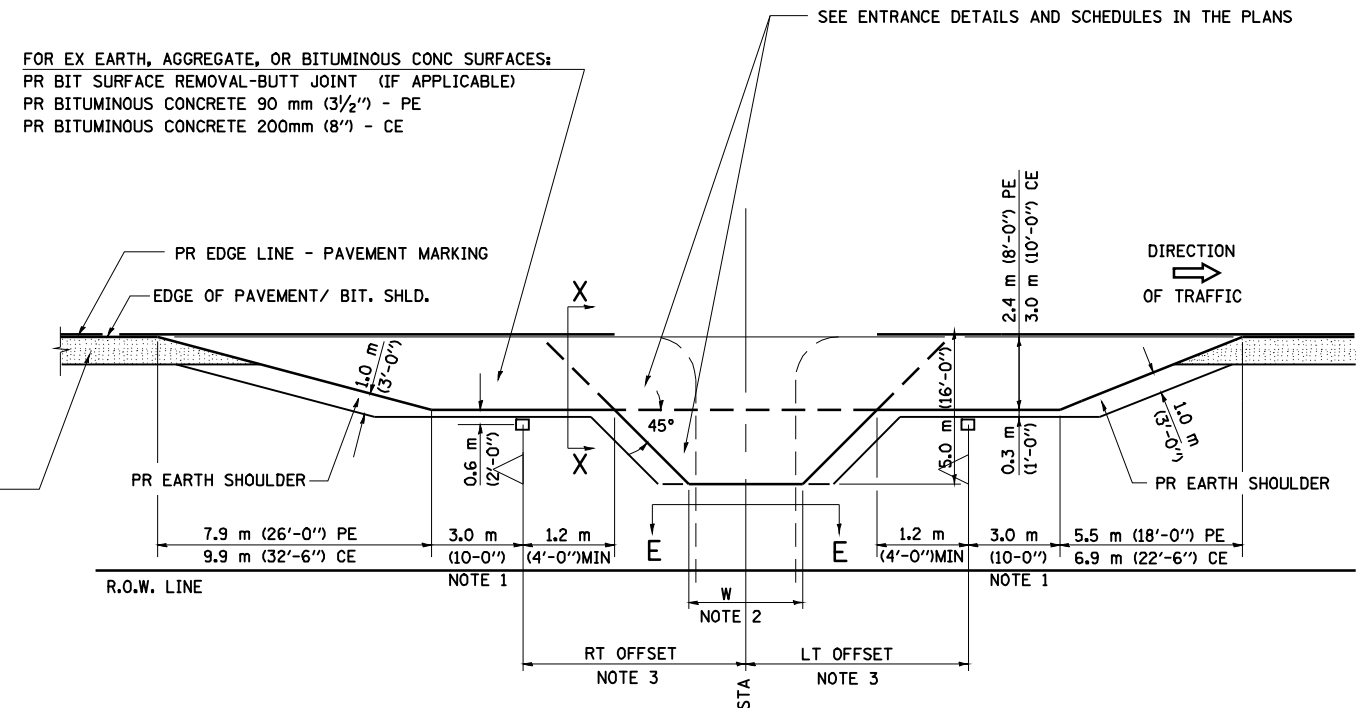
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5, 15RS-2	SCHUYLER	23	20
CONTRACT NO. 72849				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

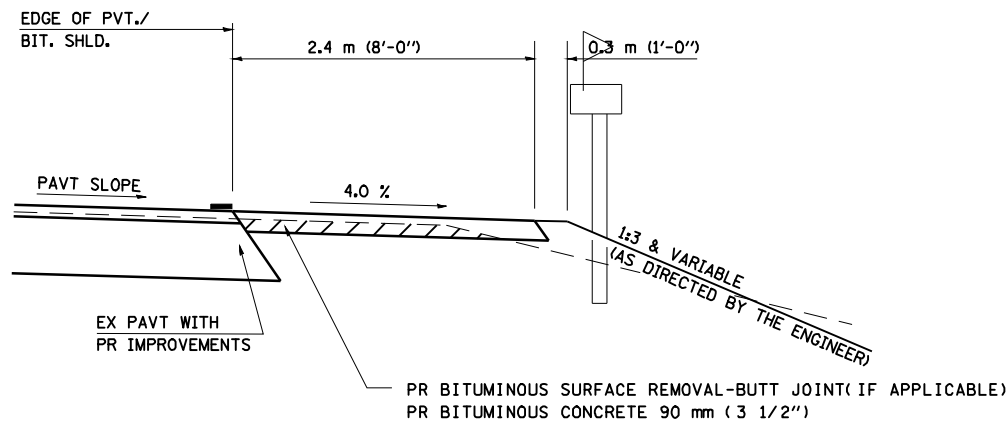
DETAILS OF MAILBOX TURNOUTS



PLAN - MAILBOX TURNOUTS

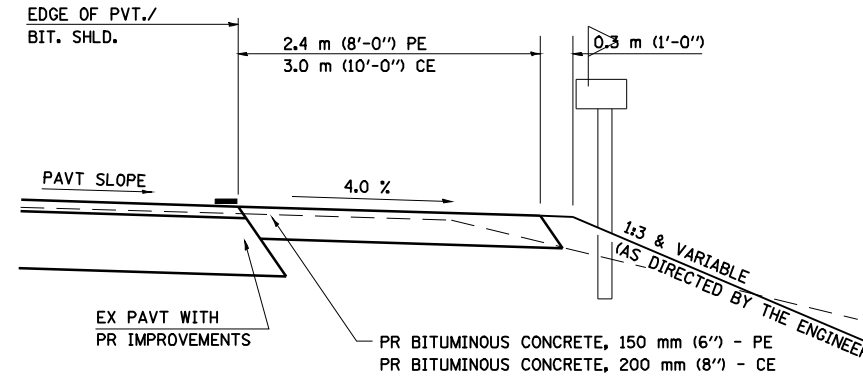


PLAN - COMBINED MAILBOX TURNOUT WITH TRAILING OR LEADING ENTRANCE



SECTION X-X THRU MAILBOX TURNOUT
 ALSO APPLIES TO MAILBOX TURNOUTS COMBINED WITH
 EX EARTH, AGGREGATE, OR BITUMINOUS PE & FE

(DETAIL APPLIES WHEN M.B. TURNOUT DOES NOT EXIST.
 IF EXISTING, TREAT SAME AS ENTRANCE.)



SECTION X-X THRU MAILBOX TURNOUT
 COMBINED WITH EX BITUMINOUS CONC & PC CONC PE & CE

(DETAIL APPLIES WHEN M.B. TURNOUT DOES NOT EXIST.
 IF EXISTING, TREAT SAME AS ENTRANCE.)

- NOTE 1 IF MORE THAN ONE MAILBOX IS PRESENT, DIMENSION FROM CENTER OF END MAILBOX.
- NOTE 2 FOR ENTRANCE LAYOUT DIMENSIONS AND SECTIONS A-A & E-E REFER TO THE SCHEDULES IN THE PLANS.
- NOTE 3 BOTH LT OR RT OFFSETS FOR MAILBOX SHOWN USE OFFSET DIMENSION PER SCHEDULE AND REFER TO LAYOUT SHOWN ON THE PLAN.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES)
 UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = laughlinr1	DESIGNED - JCN	REVISED - JCN	2/19/03
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DISTRICT SIX DETAILS FOR RURAL / URBAN ENTRANCE, MAILBOX TURNOUT & SIDEROADS W / O CONCRETE GUTTER (PPP - PROJECTS)			
SCALE: NONE	SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5, 15RS-2	SCHUYLER	23	21
CONTRACT NO. 72849				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

ENTRANCE IMPROVEMENT SCHEDULE FOR RURAL / URBAN														
LOCATION	TYPE OF ENTRANCE	MATERIAL TYPE	WIDTH (EDGE OF PAVEMENT)	WIDTH (ENTRANCE WIDTH)	RT OFFSET	LT OFFSET	LENGTH (FROM EDGE OF PVT/ BIT SHLD TO LIMITS OF IMPROVEMENT)	PR BIT. CONC. AVERAGE THICKNESS	HMA SURF REM. - BUTT JOINT	P. C. C. SURF. REM. - BUTT JOINT	BIT MATLS (P. C.)	AGG (P. C.)	INCIDENTAL BIT. SURF.	AGG SURF CSE - TY B
(LT / RT) (STA) ((FE / PE / CE / MB) (RURAL / URBAN)	(EX / PR)	FOOT	FOOT	FOOT	FOOT	FOOT	INCH	SQ. YD.	SQ. YD.	TON	TON	TON	TON
LT STA. 362+84	T. R.	BIT	56	16			10	1.5	---	---	0.02	0.07	4.29	1.20
RT STA. 362+84	T. R.	BIT	30	16			10	1.5	---	---	0.01	0.04	2.15	0.76
RT STA 367+89	F. E.	BIT	30	16			8	1.5	20.44	---	0.01	0.03	1.72	0.00
LT STA. 391+00	T. R.	BIT	87	20			10	1.5	---	---	0.03	0.12	7.19	1.78
RT STA. 397+04	T. R.	BIT	50	22			10	1.5	72.22	---	0.03	0.10	6.07	0.00
LT STA. 414+83	P. E.	BIT	29	22			8	1.5	---	---	0.01	0.03	1.90	0.85
LT STA. 417+18	P. E.	BIT	34	16			8	1.5	---	---	0.01	0.03	1.87	0.83
LT STA. 423+67	T. R.	BIT	40	18			10	1.5	---	---	0.01	0.05	2.80	0.96
RT STA. 445+33	T. R.	BIT	20	16			10	1.5	---	---	0.01	0.03	1.68	0.60
RT STA. 449+42	P. E.	BIT	26	16			8	1.5	---	---	0.01	0.03	1.57	0.70
LT STA. 449+52	M. B.	BIT	56	20			8	1.5	---	---	0.02	0.06	3.43	1.26
LT STA. 457+36	T. R.	BIT	25	15			10	1.5	---	---	0.01	0.03	1.87	0.66
RT STA. 460+59	P. E.	BIT	59	16			8	1.5	---	---	0.02	0.06	3.66	1.25
RT STA. 477+73	P. E. & F. E.	BIT	50	20			8	1.5	---	---	0.01	0.05	2.99	1.16
LT STA. 490+83	T. R.	BIT	50	16			10	1.5	---	---	0.02	0.06	3.73	1.10
RT STA. 491+73	P. E.	BIT	24	16			8	1.5	---	---	0.01	0.03	1.49	0.66
RT STA. 492+29	P. E.	BIT	22	16			8	1.5	---	---	0.01	0.02	1.42	0.63
LT STA. 493+00	M. B.	BIT	56	20			8	1.5	---	---	0.02	0.06	3.43	1.26
RT STA. 493+48	M. B. & P. E.	BIT	34	16	22		8	1.5	---	---	0.01	0.03	1.87	0.83
RT STA. 512+40	T. R.	BIT	30	20			10	1.5	---	---	0.01	0.04	2.33	0.83
LT STA. 516+64	M. B. & P. E.	BIT	50	16		23	8	1.5	---	---	0.01	0.05	2.99	1.10
LT STA 532+50	M. B. & P. E.	BIT	54	16		25	8	1.5	---	---	0.01	0.06	3.29	1.16
LT STA 544+40	P. E. & M. B.	BIT	36	16	25		8	1.5	---	---	0.01	0.03	1.94	0.86
LT STA 551+50	T. R.	BIT	24	22			50	1.5	---	---	0.05	0.18	10.73	0.76
LT STA 557+55	P. E. & M. B.	BIT	41	16	27		8	1.5	---	---	0.01	0.04	2.31	0.95
RT STA 576+34	T. R.	BIT	38	21			10	1.5	---	---	0.01	0.05	2.75	0.98
LT STA 587+50	P. E.	BIT	24	16			8	1.5	---	---	0.01	0.03	1.49	0.66
LT STA 597+50	P. E.	BIT	24	16			8	1.5	---	---	0.01	0.03	1.49	0.66
RT STA 590+73	P. E.	BIT	24	16			8	1.5	---	---	0.01	0.03	1.49	0.66
LT STA 594+34	P. E.	BIT	24	16			8	1.5	---	---	0.01	0.03	1.49	0.66
LT STA 595+18	P. E.	BIT	24	16			8	1.5	---	---	0.01	0.03	1.49	0.66
LT STA 596+10	P. E.	BIT	24	16			8	1.5	---	---	0.01	0.03	1.49	0.66
LT STA 622+30	T. R.	BIT	34	15			10	1.5	---	---	0.01	0.04	2.29	0.81
LT STA 655+76	T. R.	BIT	38	17			10	1.5	---	---	0.01	0.04	2.57	0.91
LT STA 661+22	C. E.	BIT	23	16			10	1.5	---	---	0.01	0.03	1.82	0.65
RT STA 674+30	F. A. S.	BIT	100	20			40	1.5	266.67	---	0.10	0.38	22.40	0.00
RT STA 677+00	P. E.	BIT	25	33			8	1.5	---	---	0.01	0.04	2.17	0.96
RT STA 678+13	P. E.	BIT	31	16			8	1.5	---	---	0.01	0.03	1.75	0.78
LT STA 701+91	T. R.	BIT	35	19			10	1.5	---	---	0.01	0.04	2.52	0.90
RT STA 701+91	T. R.	BIT	46	24			10	1.5	---	---	0.02	0.06	3.36	1.16
RT STA 728+50	C. H.	BIT	24	22			50	1.5	---	---	0.05	0.18	10.73	0.76
RT STA 731+00	C. E.	BIT	42	35			10	1.5	---	---	0.02	0.06	3.59	1.28
RT STA 745+85	M. B.	BIT	56	20			8	1.5	---	---	0.02	0.06	3.43	1.26
LT STA 745+85	P. E.	BIT	27	26			8	1.5	---	---	0.01	0.03	1.98	0.88
LT STA 746+65	P. E.	BIT	20	24			8	1.5	---	---	0.01	0.03	1.64	0.73
RT STA 747+82	C. E.	BIT	36	30			10	1.5	---	---	0.01	0.05	3.08	1.10
RT STA 749+94	C. E.	BIT	36	30			10	1.5	---	---	0.01	0.05	3.08	1.10
LT STA 750+68	T. R.	BIT	35	30			10	1.5	---	---	0.01	0.05	3.03	1.08
RT STA 751+00	P. E. & M. B.	BIT	44	16		28	8	1.5	---	---	0.01	0.04	2.24	1.00
RT STA 761+97	P. E.	BIT	16	16			8	1.5	14.22	---	0.01	0.02	1.19	0.00

FILE NAME =	USER NAME = laughlinr1	DESIGNED - JCN	REVISED - JCN 2/19/03
e:\pwork\pwork\LAUGHLINRL\dms26633\ent_ppp.dgn		DRAWN - CAD	REVISED -
		CHECKED - JCN	REVISED -
		DATE - 2/23/99	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT SIX DETAILS FOR RURAL / URBAN ENTRANCE, MAILBOX TURNOUT & SIDEROADS W / O CONCRETE GUTTER (PPP - PROJECTS)			
SCALE: NONE	SHEET NO. 3 OF 5 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	14RS-5, 15RS-2	SCHUYLER	23	22
CONTRACT NO. 72849				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

ENTRANCE IMPROVEMENT SCHEDULE FOR RURAL / URBAN														
LOCATION	TYPE OF ENTRANCE	MATERIAL TYPE	WIDTH (EDGE OF PAVEMENT)	WIDTH (ENTRANCE WIDTH)	RT OFFSET	LT OFFSET	LENGTH (FROM EDGE OF PVT/ BIT SHLD TO LIMITS OF IMPROVEMENT)	PR BIT. CONC. AVERAGE THICKNESS	HMA SURF REM. - BUTT JOINT	P. C. C. SURF. REM. - BUTT JOINT	BIT MATLS (P. C.)	AGG (P. C.)	INCIDENTAL BIT. SURF.	AGG SURF CSE - TY B
(LT / RT) (STA) ((FE / PE / CE / MB) (RURAL / URBAN)	(EX / PR)	FOOT		FOOT	FOOT	FOOT	INCH	SQ. YD.	SQ. YD.	TON	TON	TON	TON
LT STA 762+00	T. R.	BIT	36	20			25	1.5	---	---	0.03	0.11	6.53	0.93
RT STA 762+67	P. E.	BIT	16	16			8	1.5	---	---	0.01	0.02	1.19	0.53
LT STA 1290+10	C. E.	BIT	130	130			10	1.5	144.44	---	0.05	0.21	12.13	0.00
RT STA 1295+12	P. E.	BIT	42	30			8	1.5	32.00	---	0.01	0.05	2.69	0.00
LT STA 1296+15	T. R.	BIT	30	20			10	1.5	---	---	0.01	0.04	2.33	0.83
LT STA 1298+58	P. E.	CONC	26	10			8	1.5	---	16.00	0.01	0.02	1.34	0.00
LT STA 1299+60	P. E.	BIT	80	40			8	1.5	---	---	0.02	0.09	5.23	1.99
LT STA 1301+00	P. E.	BIT	26	20			8	1.5	20.44	---	0.01	0.03	1.72	0.00
RT STA 1301+75	P. E.	CONC	42	28			8	1.5	---	31.11	0.01	0.04	2.61	0.00
LT STA 1302+07	P. E.	BIT	68	30			8	1.5	---	---	0.02	0.07	4.33	1.63
RT STA 1302+79	P. E.	BIT	42	30			8	1.5	---	---	0.01	0.05	2.69	1.20
LT STA 1302+94	P. E.	BIT	22	10			8	1.5	---	---	0.01	0.02	1.19	0.53
LT STA 1303+25	P. E.	CONC	38	10			8	1.5	---	21.33	0.01	0.03	1.79	0.00
LT STA 1303+60	P. E.	BIT	26	12			8	1.5	---	---	0.01	0.02	1.42	0.63
RT STA 1303+60	P. E.	BIT	22	10			8	1.5	---	---	0.01	0.02	1.19	0.53
LT STA 1304+24	P. E.	BIT	96	20			8	1.5	---	---	0.03	0.11	6.42	1.93
RT STA 1304+33	P. E.	BIT	24	16			8	1.5	---	---	0.01	0.03	1.49	0.66
LT STA 1305+57	P. E.	BIT	59	20			8	1.5	---	---	0.02	0.06	3.66	1.31
RT STA 1305+57	P. E.	BIT	20	16			8	1.5	16.00	---	0.01	0.02	1.34	0.00
LT STA 29+15	T. R.	BIT	32	30			25	1.5	86.11	---	0.03	0.12	7.23	0.00
RT STA 29+16	T. R.	BIT	32	30			25	1.5	86.11	---	0.03	0.12	7.23	0.00
TOTAL									758.7	68.4	1.1	4.1	239.1	55.2