06-10-2016 LETTING ITEM 025

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DU PAGE	35	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 62A51		

35+1=36 TOTAL

D-91-237-15

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

PROPOSED HIGHWAY PLANS

FAU 1321 IL 19 (IRVING PARK ROAD)
EMMERSON AVE. TO IL RTE. 83
SECTION 32RS-6(15)
RESURFACING (3P), PEDESTRIAN RAMPS
DUPAGE COUNTY

C-91-237-15

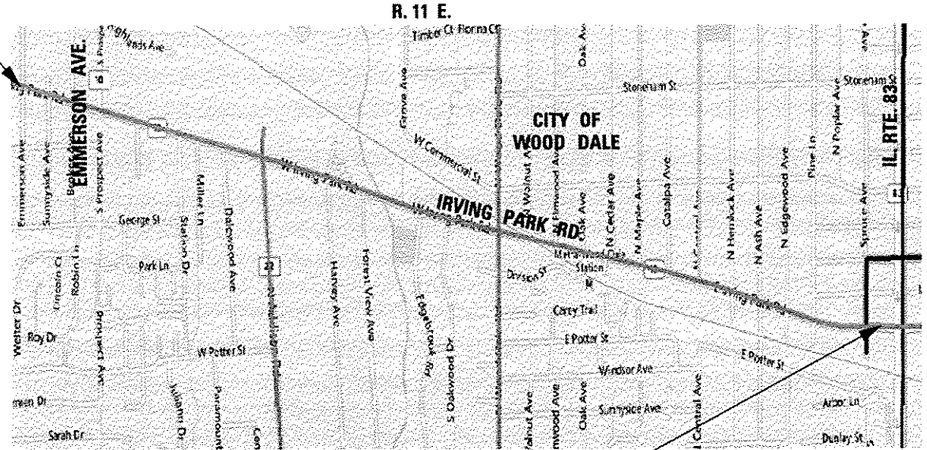
FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT IS LOCATED IN THE CITY WOOD DALE

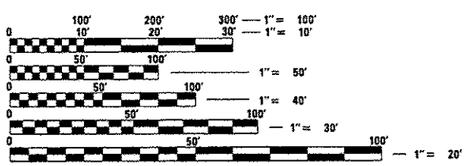
LOCATION OF SECTION INDICATED THUS: –

PROJECT BEGINS STA. 224+48.05

ADDISON TOWNSHIP
R. 11 E.



PROJECT ENDS STA. 343+82.64



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

N

LOCATION MAP NOT TO SCALE

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT MANAGER : ISSAM RAYYAN (847) 705-4178
PROJECT ENGINEER : J. ALAIN MIDY (847) 221-3056

MILHOUSE ENGINEERING & CONSTRUCTION

CONTRACT NO. 62A51

GROSS LENGTH OF PROJECT = 11,934.59 FEET = 2.26 MILES
NET LENGTH OF PROJECT = 9,496.19 FEET = 1.80 MILES

CRAIG TURNER
062-054031
LICENSED PROFESSIONAL ENGINEER OF
STATE OF ILLINOIS

Craig Turner 3/22/16
SIGNATURE DATE
11/30/2017
EXPIRES

TRAFFIC DATA
F.A.U. 1321
IL RTE. 19
CLASS: MINOR ARTERIAL
EXISTING ADT: 20,900 (2015)
DESIGN ADT: 20,900 (2015)
DESIGN SPEED: 35 MPH
POSTED SPEED: 30 MPH

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED MARCH 24 20 16
REGIONAL ENGINEER
May 6 20 16
Maureen M. Addis, PE, for
ENGINEER OF DESIGN AND ENVIRONMENT
May 6 20 16
DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-02	DIAGONAL CURB RAMPS FOR SIDEWALKS
424026-01	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
424031-01	MEDIAN PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
602001-02	CATCH BASIN, TYPE A
604001-04	FRAME AND LIDS, TYPE 1
604086-03	FRAME AND GRATE TYPE 23
604091-03	FRAME AND GRATE TYPE 24
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF-RD OPERATIONS, 2L2W, 15' TO 24' FROM PAVEMENT EDGE
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' TO 24' FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L2W, MOVING OPERATIONS -- DAY ONLY
701427-04	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS < OR = 40 MPH
701501-06	URBAN LANE CLOSURE, 2L2W, UNDIVIDED
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701602-07	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN LANE CLOSURE, MULTI LANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTI LANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-05	TRAFFIC CONTROL DEVICES
814001-03	HANDHOLES
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, COMMUNICATION, AND GAS FACILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
2. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKING SHALL BE AS DIRECTED BY THE ENGINEER.
3. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKING AND TEMPORARY PAVEMENT MARKING ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE TYPE III SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKINGS.
4. LOCATIONS OF PAVEMENT PATCHING AND CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE AS DETERMINED BY THE ENGINEER AND AS SHOWN ON THE PLANS.
5. 10 FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND CONDITIONS IN THE FIELD UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
6. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE VILLAGE OF ITASCA, AND THE CITY OF WOOD DALE.
7. WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
8. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
9. SAW CUTTING PRIOR TO ANY REMOVAL ITEMS NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS BEING REMOVED.
10. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
11. THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
13. THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.
15. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTUES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
16. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1½ INCHES (40mm) WHERE THE SPEED LIMIT IS 40 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
17. UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
18. THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
19. THE CONTRACTOR SHALL CONTACT DON CHIARUGI AT (847) 741-9857 AT LEAST TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
20. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
21. REFER TO THE DISTRICT ONE TYPICAL PAVEMENT MARKINGS AND TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS FOR DETAILS. PLANS DO NOT SHOW EXACT STATIONS OF MARKERS, ONLY THE PATTERN. PATTERN MUST MATCH THOSE IN AREAS ADJACENT TO PROJECT LIMITS.
22. MATCH EXISTING PAVEMENT MARKINGS AT THE PROJECT AND OMISSION LIMITS.
23. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
24. TEMPORARY RAMPS AT BUTT JOINTS SHALL BE INCLUDED IN THE COST OF THE BUTT JOINT AS SHOWN ON THE BUTT JOINT DETAIL SHEET.

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Default	PLOT DATE = 3/21/2016	DATE - 4-16-2015	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, LIST OF STATE STANDARDS AND GENERAL NOTES
E. IRVING PARK ROAD

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	2
		CONTRACT NO. 62A51		
		ILLINOIS FED. AID PROJECT		

CODE NO.	ITEM	UNIT	URBAN TOTAL QUANTITY	CONSTRUCTION CODE 0005 ROADWAY 100% STATE					
20200100	EARTH EXCAVATION	CU YD	259	259					
20800150	TRENCH BACKFILL	CU YD	90	90					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	605	605					
25200110	SODDING, SALT TOLERANT	SQ YD	605	605					
25200200	SUPPLEMENTAL WATERING	UNIT	5	5					
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	185	185					
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	1497	1497					
40600290	BITUMINOUS MATERIALS, TACK COAT	POUND	47795	47795					
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	188	188					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	140	140					
40600827	POLYMERIZED LEVELING BINDER, MACHINE METHOD, IL-4.75, N50	TON	2920	2920					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	1937	1937					
40600985	PORTLAND CEMENT CONCRETE REMOVAL - BUTT JOINT	SQ YD	419	419					
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	1966	1966					

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	PLOT DATE = 3/21/2016	DATE - 4-16-2015	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
E. IRVING PARK ROAD

SCALE: NONE | SHEET 1 OF 5 SHEETS | STA. N/A | TO STA. N/A

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(115)	DUPAGE	35	3
		CONTRACT NO. 62A51		
	ILLINOIS	FED. AID PROJECT		

URBAN

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 0005 ROADWAY 100% STATE
40603565	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70	TON	7379	7379
42001300	PROTECTIVE COAT	SQ YD	551	551
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	96	96
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	205	205
42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SQ FT	10110	10110
42400800	DETECTABLE WARNINGS	SQ FT	700	700
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"	SQ YD	70807	70807
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1982	1982
44000600	SIDEWALK REMOVAL	SQ FT	10110	10110
44002220	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 5"	SQ YD	7022	7022
44201749	CLASS D PATCHES, TYPE I, 9 INCH	SQ YD	70	70
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	2809	2809
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	2809	2809
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	1334	1334

			URBAN	CONSTRUCTION CODE					
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	0005					
				ROADWAY					
				100% STATE					
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SQ YD	320	320					
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	124	124					
60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	2	2					
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1					
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	10	10					
60406000	FRAMES AND LIDS, TYPE I, OPEN LID	EACH	10	10					
60406100	FRAMES AND LIDS, TYPE I, CLOSED LID	EACH	10	10					
60602500	CONCRETE GUTTER, TYPE A	FOOT	59	59					
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	200	200					
* 66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1					
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6					
67100100	MOBILIZATION	L SUM	1	1					
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1					

14 *Specialty Items

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SUMMARY OF QUANTITIES
E. IRVING PARK ROAD

SCALE: NONE | SHEET 3 OF 5 SHEETS | STA. N/A | TO STA. N/A

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	5
		CONTRACT NO. 62A51		
	ILLINOIS FED. AID PROJECT			

URBAN

	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 0005 ROADWAY 100% STATE					
	70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					
	70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1					
	70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1					
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					
	70300100	SHORT TERM PAVEMENT MARKINGS	FOOT	5602	5602					
	70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1452	1452					
	70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	25566	25566					
	70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	6712	6712					
	70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	703	703					
	70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	477	477					
	70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	16854	16854					
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1452	1452					
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	25566	25566					

14

* Specialty Items

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
E. IRVING PARK ROAD

SCALE: NONE | SHEET 4 OF 5 SHEETS | STA. N/A | TO STA. N/A

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(115)	DUPAGE	35	6
		CONTRACT NO. 62A51		
	ILLINOIS	FED. AID PROJECT		

URBAN

	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 0005 ROADWAY 100% STATE
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	6712	6712
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	703	703
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	477	477
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1138	1138
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1138	1138
*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	2847	2847
	X5537800	STORM SEWER TO BE CLEANED, 12"	FOOT	2700	2700
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	10	10
	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1652	1652
	Z0018100	DRAINAGE STRUCTURES ADJUSTMENT (SPECIAL)	EACH	83	83
	Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	135	135
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	1163	1163

.12 * Specialty Items

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
E. IRVING PARK ROAD

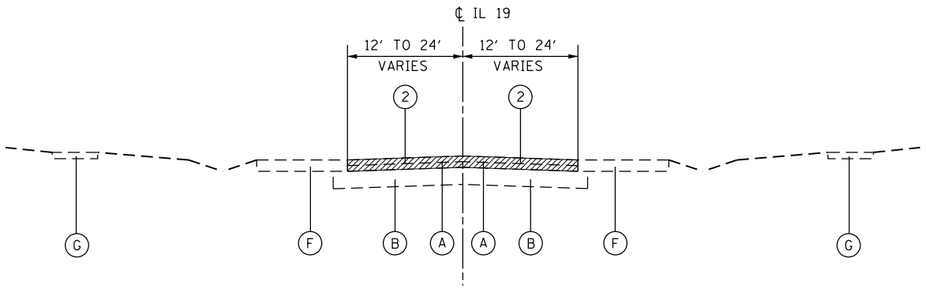
SCALE: NONE | SHEET 5 OF 5 SHEETS | STA. N/A | TO STA. N/A

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	7
		CONTRACT NO. 62A51		
	ILLINOIS FED. AID PROJECT			

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ Ndes	QMP
PAVEMENT RESURFACING		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70 (IL 9.5 mm); 1¾"	4% @ 70 GYR.	QCP
POLMERIZED LEVELING BINDER, (MACHINE METHOD), IL 4.75, N50, ¾"	3.5% @ 50 GYR.	QCP
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm); 9"	4% @ 70 GYR.	QC/QA
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR.	QC/QA
HMA DRIVEWAYS 8" & 10"		
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2"	4% @ 50 GYR.	QC/QA
HMA BASE COURSE (HMA BINDER IL-19 mm); PE-6" & CE-8"	4% @ 50 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP); QUALITY CONTROL/QUALITY ASSURANCE (QC/QA)		

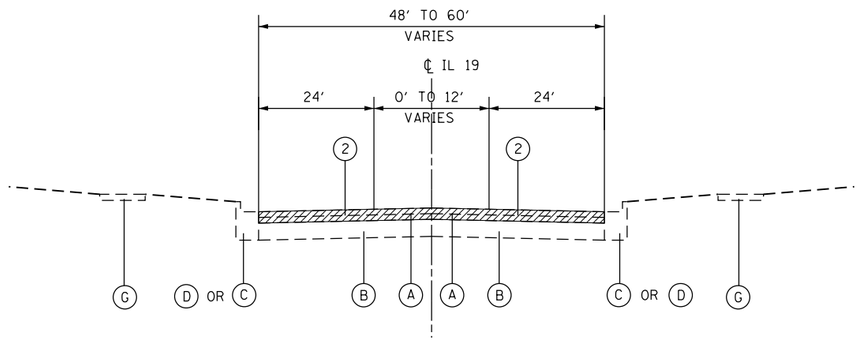
NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE AC TYPE FOR POLYMERIZED MIXTURES SHALL BE "SBS/SBR PG 76-22"; AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.
3. CONTRACTOR SHALL PATCH BEFORE MILLING.
4. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.



EXISTING TYPICAL SECTION

STA. 224+48 TO STA. 228+00



EXISTING TYPICAL SECTION

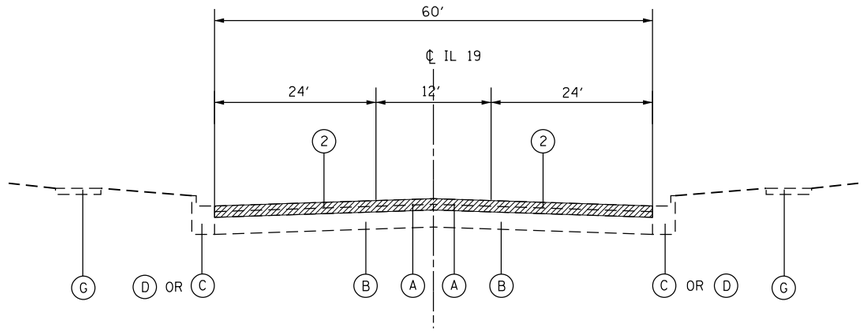
STA. 228+00 TO STA. 233+00

EXISTING CONDITIONS:

- (A) HOT-MIX ASPHALT SURFACE AND BINDER COURSE, 3" & VARIES
- (B) EXISTING P.C.C. PAVEMENT, 9" & VARIES
- (C) COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.12
- (D) COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.24
- (E) NOT USED
- (F) HOT-MIX ASPHALT SHOULDER, VARIABLE DEPTH
- (G) EXISTING P.C.C. SIDEWALK, 5" & VARIES

PROPOSED IMPROVEMENTS:

- (1) NOT USED
- (2) HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
- (3) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1¾"
- (4) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
- (5) COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)
- (6) SIDEWALK REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)
- (7) HOT-MIX ASPHALT SHOULDER REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)



EXISTING TYPICAL SECTION

STA. 233+00 TO 343+82.65
OMISSION: 272+57 TO 276+14 (SALT CREEK)
OMISSION: 293+51 TO 294+74 (SOO LINE RR)

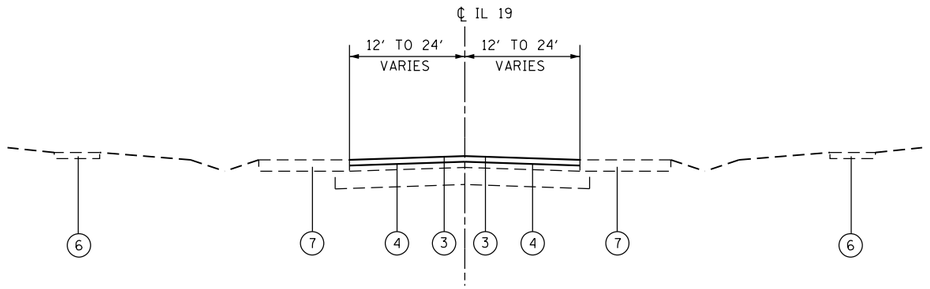
FILE NAME = D162A51_typ sect.dgn	USER NAME = mdrane	DESIGNED - WDC	REVISED -
		DRAWN - WDC	REVISED -
	PLOT SCALE = 200.0000 ' / in.	CHECKED - CTT	REVISED -
Default	PLOT DATE = 3/18/2016	DATE - 4-16-2015	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
E. IRVING PARK ROAD

SCALE: NONE | SHEET 1 OF 2 SHEETS | STA. 224+48.05 TO STA. 343+82.65

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	8
		CONTRACT NO. 62A51		
	ILLINOIS FED. AID PROJECT			



PROPOSED TYPICAL SECTION

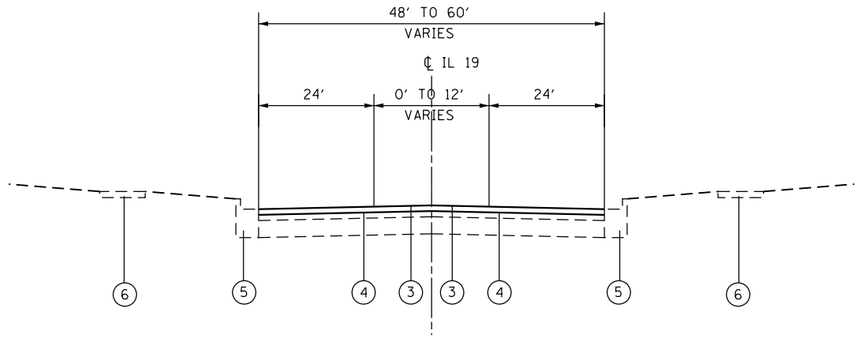
STA. 224+48 TO STA. 228+00

EXISTING CONDITIONS:

- (A) HOT-MIX ASPHALT SURFACE AND BINDER COURSE, 3″ & VARIES
- (B) EXISTING P.C.C. PAVEMENT, 9″ & VARIES
- (C) COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.12
- (D) COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.24
- (E) NOT USED
- (F) AGGREGATE SHOULDER, VARIABLE DEPTH
- (G) EXISTING P.C.C. SIDEWALK, 5″ & VARIES

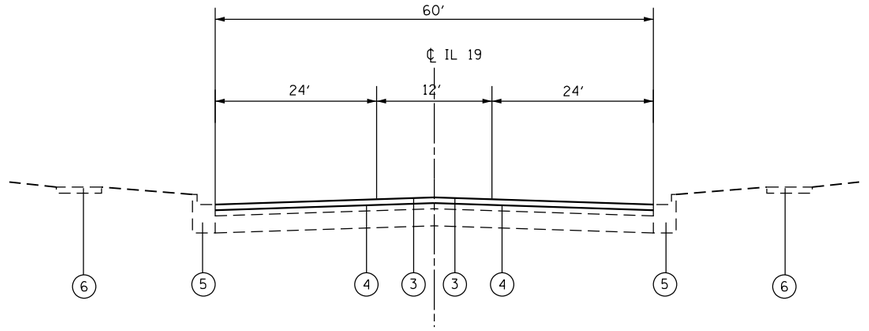
PROPOSED IMPROVEMENTS:

- (1) NOT USED
- (2) HOT-MIX ASPHALT SURFACE REMOVAL, 2½″
- (3) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX ″E″, N70, 1¾″
- (4) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾″
- (5) COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)
- (6) SIDEWALK REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)
- (7) AGGREGATE SHOULDER REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)



PROPOSED TYPICAL SECTION

STA. 228+00 TO STA. 233+00



PROPOSED TYPICAL SECTION

STA. 233+00 TO 343+82.65

OMISSION: 272+57 TO 276+14 (SALT CREEK)

OMISSION: 293+51 TO 294+74 (SOO LINE RR)

FILE NAME = D162A51_typ sect02.dgn	USER NAME = mdrane	DESIGNED - WDC	REVISED -
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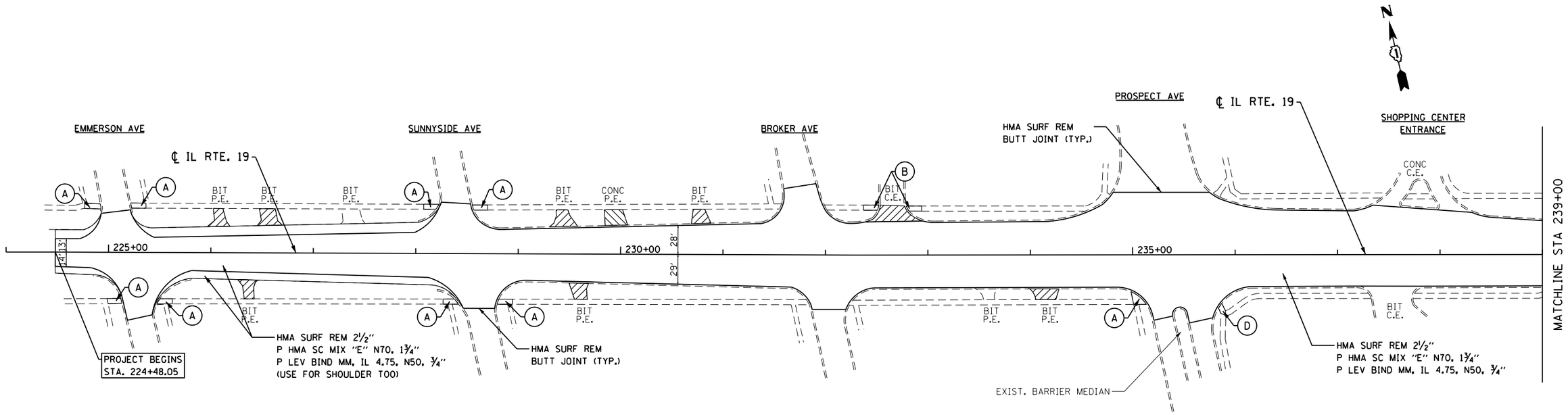
	PLOT SCALE = 200.0000 ′ / in.	CHECKED - CTT	REVISED -
Default	PLOT DATE = 3/18/2016	DATE - 4-16-2015	REVISED -

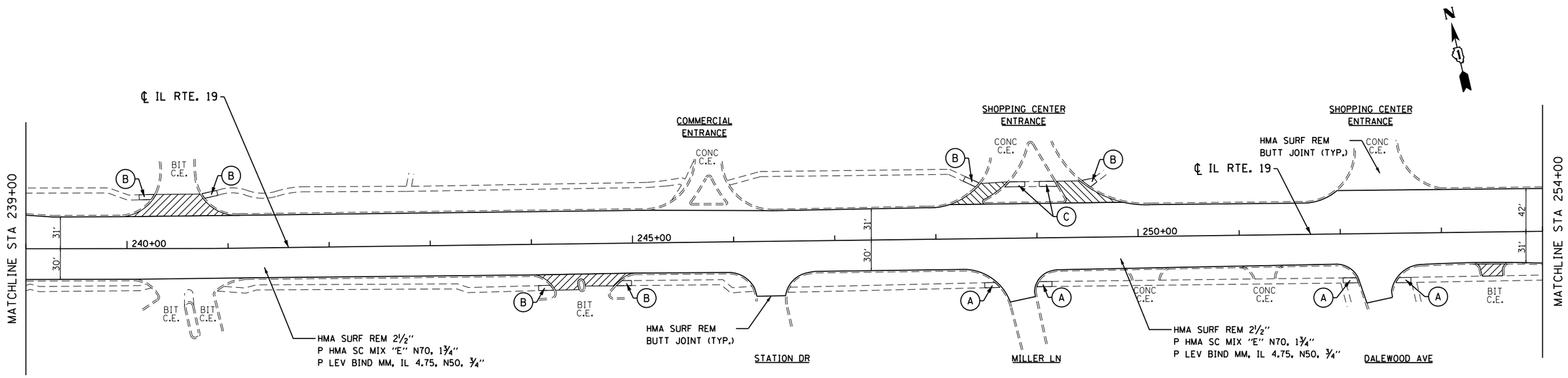
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
E. IRVING PARK ROAD

SCALE: NONE | SHEET 2 OF 2 SHEETS | STA. 224+48.05 TO STA. 343+82.65

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	9
		CONTRACT NO. 62A51		
	ILLINOIS	FED. AID PROJECT		





R&R BIT DRIVEWAY R&R PCC DRIVEWAY

(A) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424001

(B) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424026

(C) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424031

(D) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424006

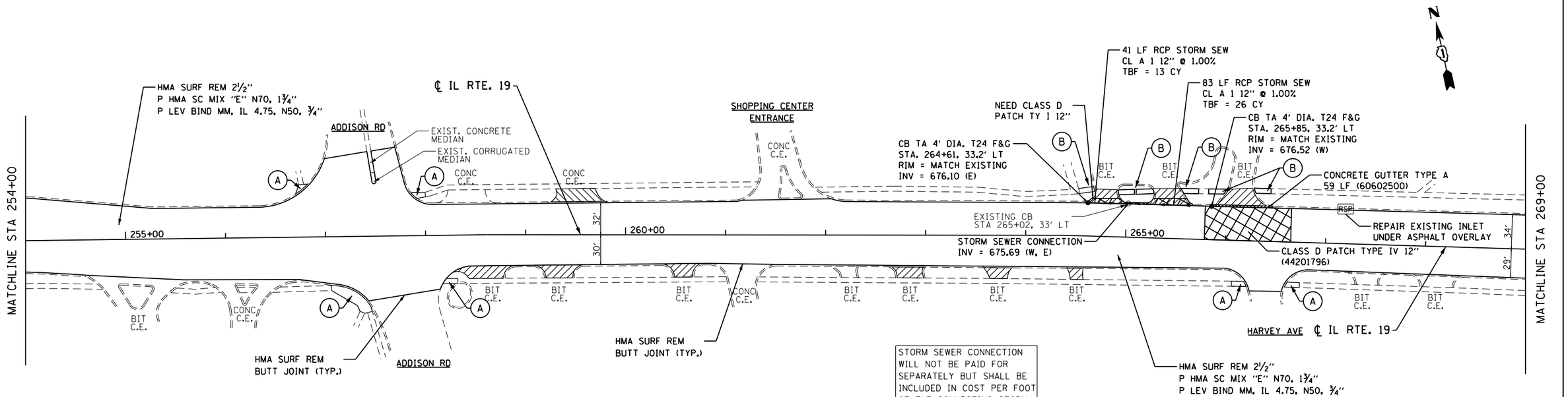
FILE NAME = D162A51_roadway01.dgn	USER NAME = mdrane	DESIGNED - WDC	REVISED -
		DRAWN - WDC	REVISED -
	PLOT SCALE = 100.000001 ' / in.	CHECKED - CTT	REVISED -
SHT_DOUBLE PLAN	PLOT DATE = 3/18/2016	DATE - 4-16-2015	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN – E. IRVING PARK ROAD

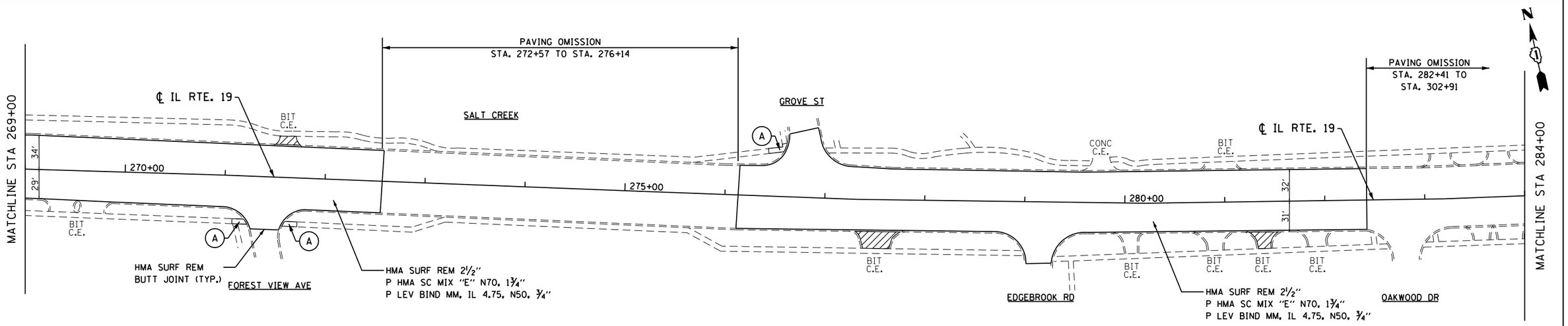
SCALE: 1" = 50' | SHEET 1 OF 4 SHEETS | STA. 224+48 TO STA. 254+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	10
		CONTRACT NO. 62A51		
		ILLINOIS FED. AID PROJECT		



STORM SEWER CONNECTION WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN COST PER FOOT OF THE CONNECTING STORM SEWER SIZE SPECIFIED

NOTE: CONTRACTOR TO FIELD VERIFY RIM AND INVERT ELEVATIONS PRIOR TO COMMENCING STORM SEWER CONSTRUCTION.



R&R BIT DRIVEWAY R&R PCC DRIVEWAY

(A) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424001

(B) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424026

(C) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424031

(D) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424006

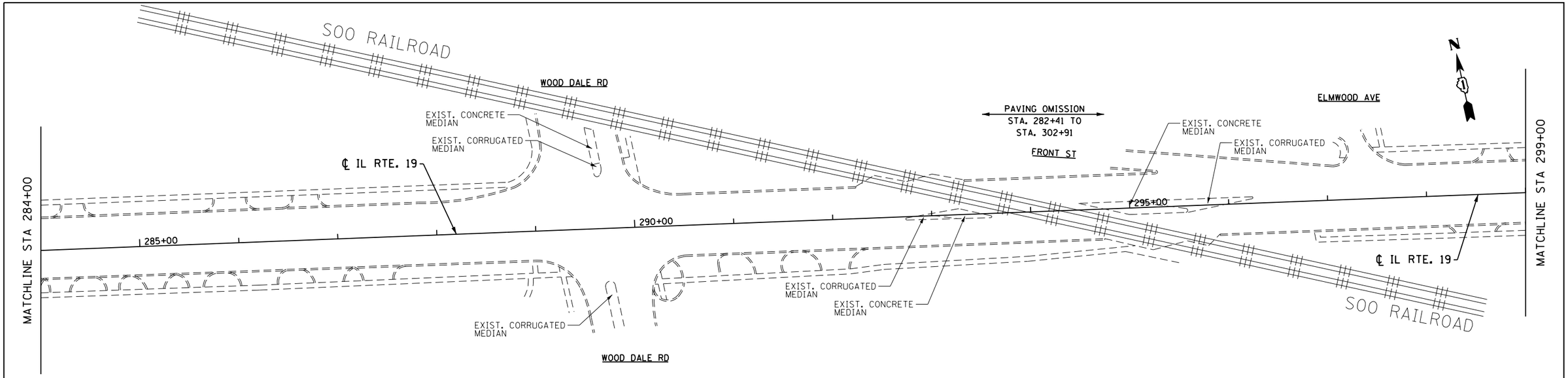
FILE NAME = D162A51_roadway02.dgn	USER NAME = mdrane	DESIGNED - WDC	REVISED -
		DRAWN - WDC	REVISED -
	PLOT SCALE = 100.000001 ' / in.	CHECKED - CTT	REVISED -
SHT_DOUBLE PLAN	PLOT DATE = 3/24/2016	DATE - 4-16-2015	REVISED -

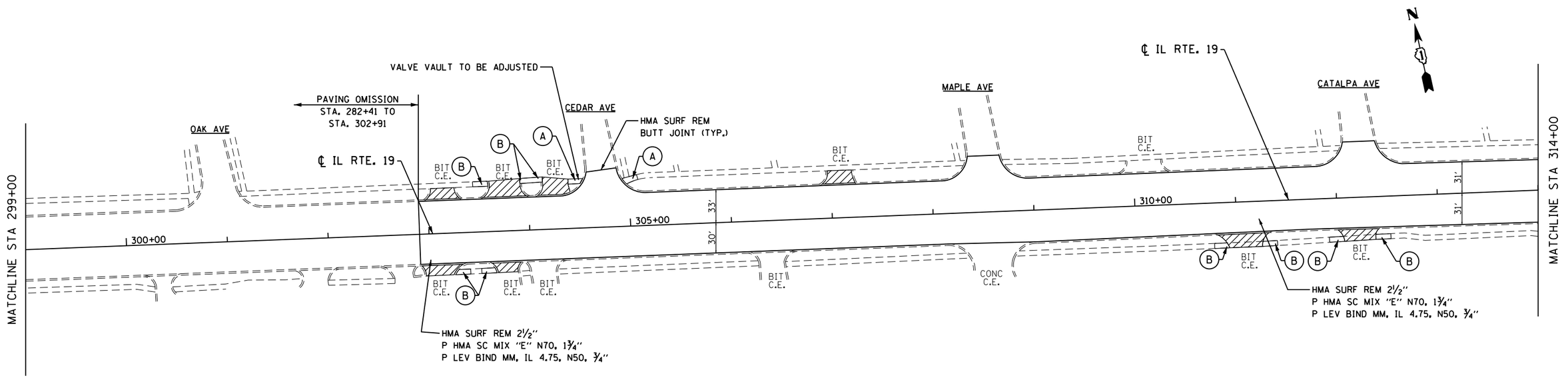
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN – E. IRVING PARK ROAD

SCALE: 1" = 50' | SHEET 2 OF 4 SHEETS | STA. 254+00 TO STA. 284+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	11
		CONTRACT NO. 62A51		
	ILLINOIS	FED. AID PROJECT		





R&R BIT DRIVEWAY R&R PCC DRIVEWAY

(A) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424001

(B) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424026

(C) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424031

(D) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424006

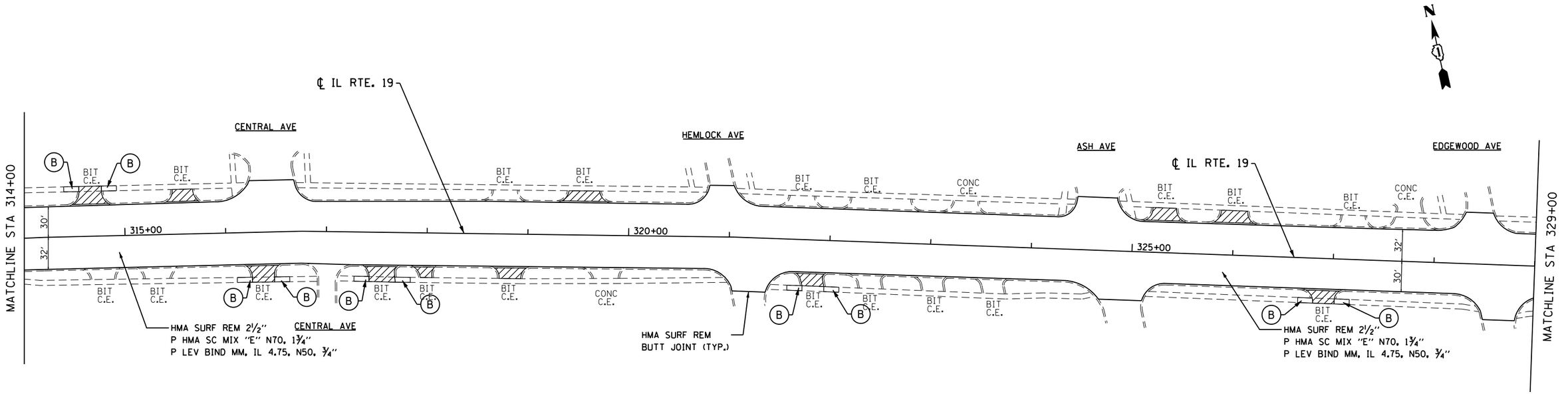
FILE NAME = D162A51_roadway03.dgn	USER NAME = mdrane	DESIGNED - WDC	REVISED -
		DRAWN - WDC	REVISED -
	PLOT SCALE = 100.000001 ' / in.	CHECKED - CTT	REVISED -
SHT_DOUBLE PLAN	PLOT DATE = 3/18/2016	DATE - 4-16-2015	REVISED -

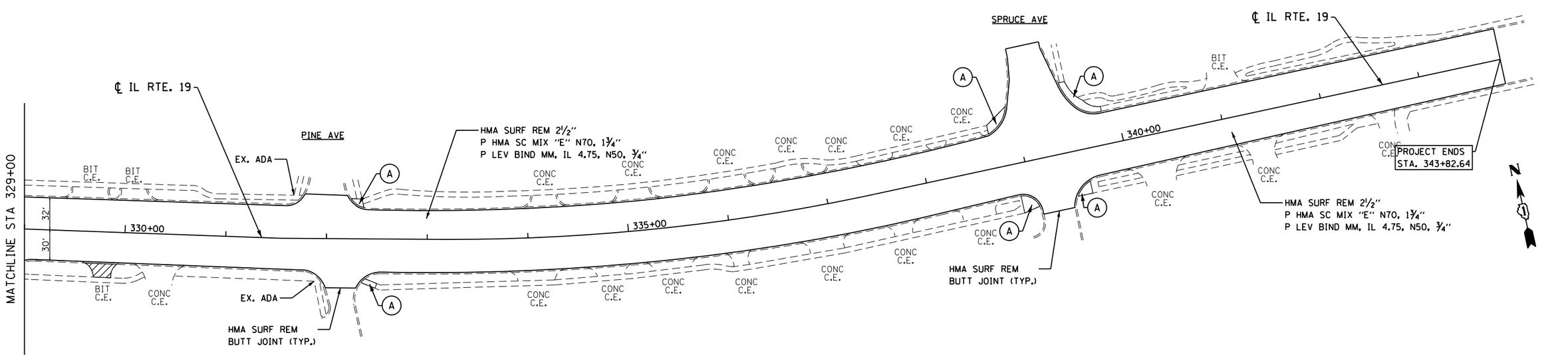
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN – E. IRVING PARK ROAD

SCALE: 1" = 50' | SHEET 3 OF 4 SHEETS | STA. 284+00 TO STA. 314+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	12
		CONTRACT NO. 62A51		
	ILLINOIS	FED. AID PROJECT		





R&R BIT DRIVEWAY R&R PCC DRIVEWAY

(A) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424001

(B) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424026

(C) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424031

(D) CURB RAMPS FOR SIDEWALKS WITH DETECTABLE WARNINGS STANDARD 424006

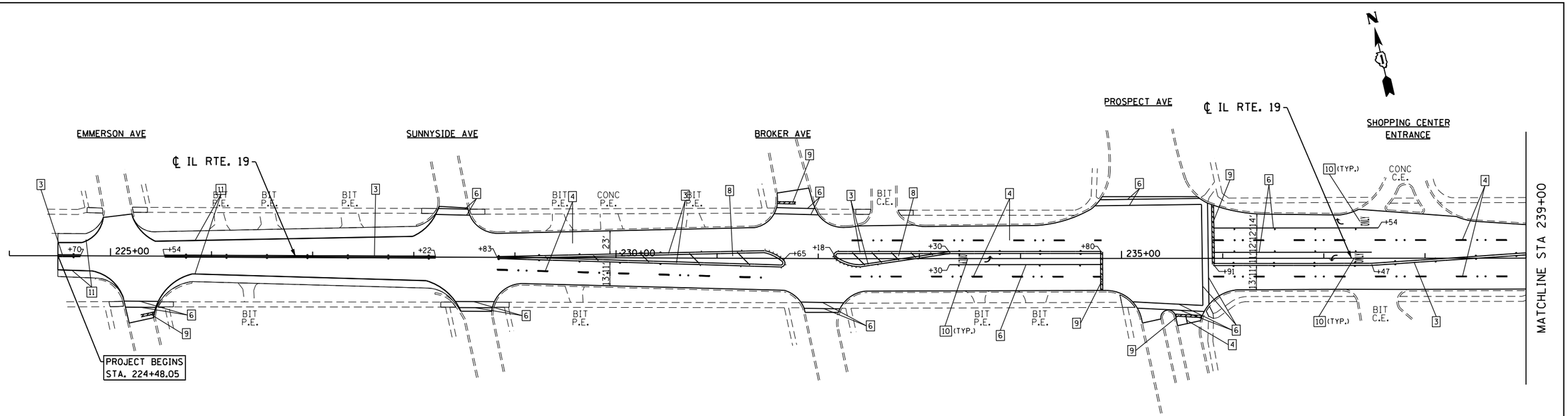
FILE NAME = D162A51_roadway04.dgn	USER NAME = mdrane	DESIGNED - WDC	REVISED -
		DRAWN - WDC	REVISED -
SHT_DOUBLE PLAN	PLOT SCALE = 100.000001 ' / in.	CHECKED - CTT	REVISED -
	PLOT DATE = 3/18/2016	DATE - 4-16-2015	REVISED -

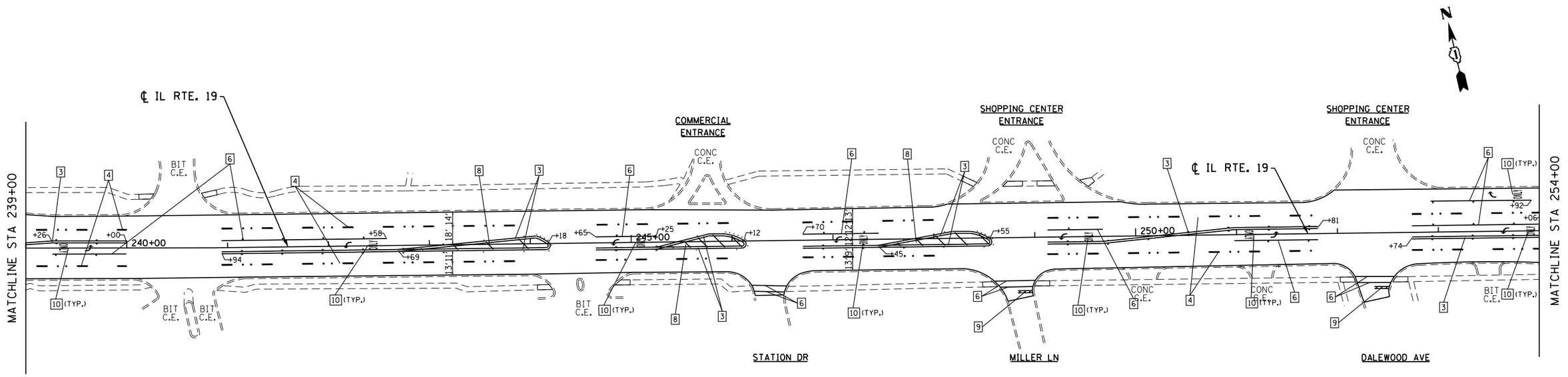
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN – E. IRVING PARK ROAD

SCALE: 1" = 50' | SHEET 4 OF 4 SHEETS | STA. 314+00 TO STA. 343+82.64

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	13
		CONTRACT NO. 62A51		
		ILLINOIS	FED. AID PROJECT	





NOTE:

1. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE IMPROVEMENT LIMITS. SEE DISTRICT DETAIL TC-11.

LEGEND

1. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW SOLID LINE)
2. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW - 10' DASH, 30' SKIP)
3. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW SOLID LINE)
4. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP W/ 2 CRYSTAL / OPAQUE RRPM'S)
5. THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)
6. THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE SOLID LINE)
7. THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE SOLID LINE)
8. THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONAL LINE)
9. THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE STOP BAR)
10. THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (TYP.)
11. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE SOLID LINE)

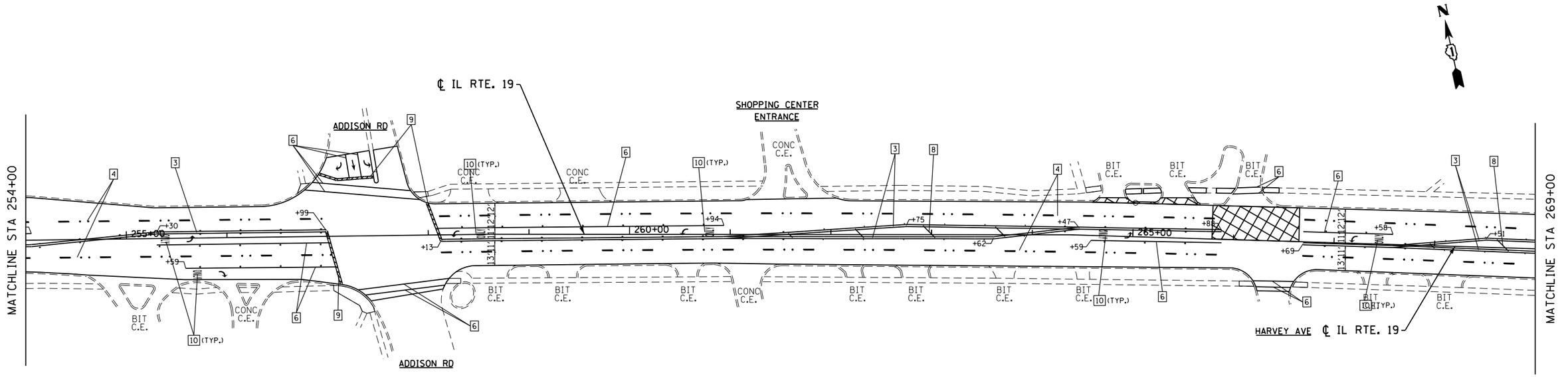
FILE NAME = D162A51_pvmtmarking01.dgn	USER NAME = mdrane	DESIGNED - WDC	REVISED -
		DRAWN - WDC	REVISED -
	PLOT SCALE = 100.000001 ' / in.	CHECKED - CTT	REVISED -
SHT_DOUBLE PLAN	PLOT DATE = 3/18/2016	DATE - 4-16-2015	REVISED -

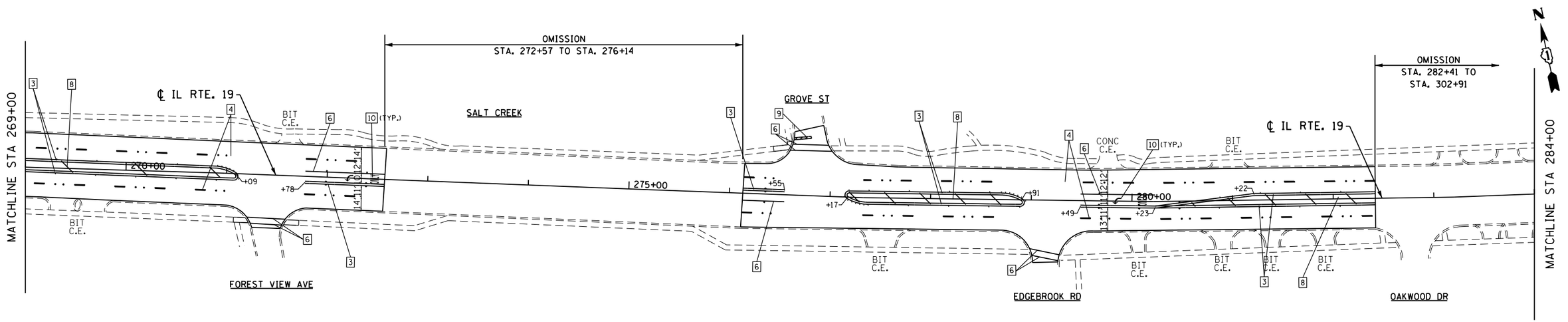
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN – E. IRVING PARK ROAD

SCALE: 1" = 50' | SHEET 1 OF 4 SHEETS | STA. 224+48 TO STA. 254+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	14
		CONTRACT NO. 62A51		
	ILLINOIS FED. AID PROJECT			





NOTE:

1. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE IMPROVEMENT LIMITS. SEE DISTRICT DETAIL TC-11.

LEGEND

1. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW SOLID LINE)
2. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW - 10' DASH, 30' SKIP)
3. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW SOLID LINE)
4. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP W/ 2 CRYSTAL / OPAQUE RRPM'S)
5. THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)
6. THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE SOLID LINE)
7. THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE SOLID LINE)
8. THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONAL LINE)
9. THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE STOP BAR)
10. THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (TYP.)
11. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE SOLID LINE)

FILE NAME = D162A51_pvmtmarking02.dgn	USER NAME = mdrane	DESIGNED - WDC	REVISED -
		DRAWN - WDC	REVISED -
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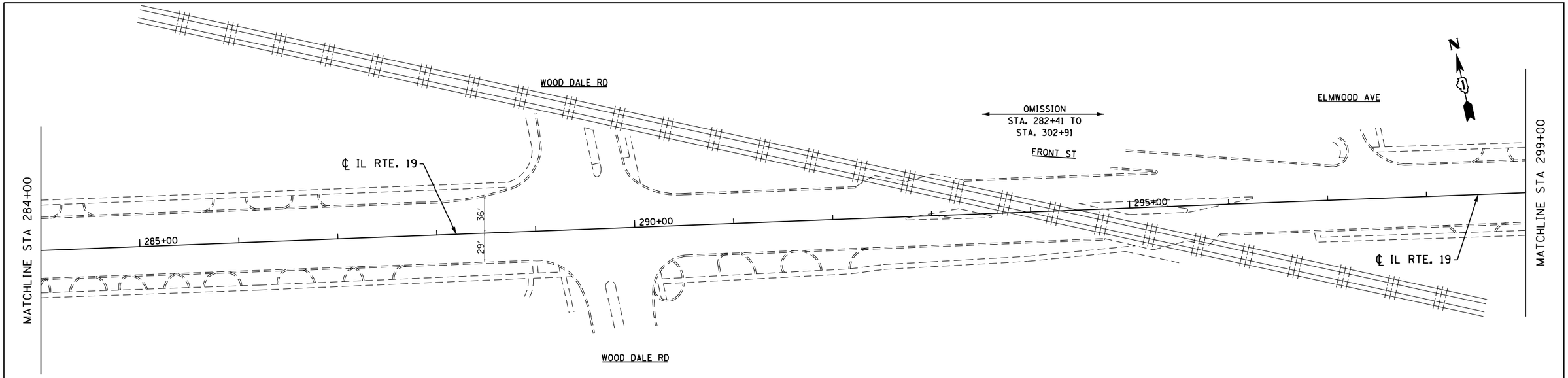
SHT_DOUBLE PLAN	PLOT DATE = 3/18/2016	DATE - 4-16-2015	REVISED -

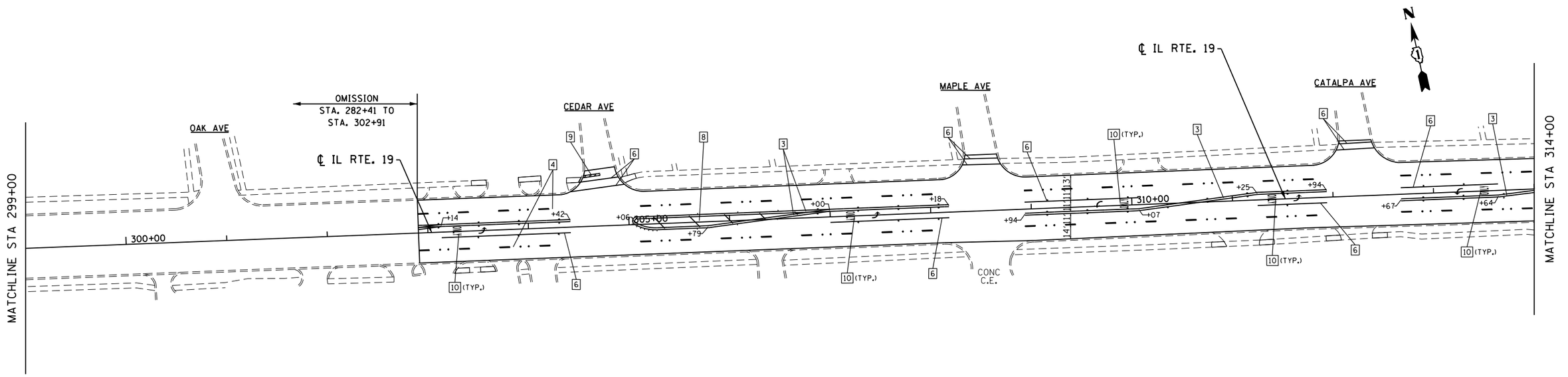
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN – E. IRVING PARK ROAD

SCALE: 1" = 50' | SHEET 2 OF 4 SHEETS | STA. 254+00 TO STA. 284+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	15
		CONTRACT NO. 62A51		
	ILLINOIS FED. AID PROJECT			





NOTE:

1. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE IMPROVEMENT LIMITS. SEE DISTRICT DETAIL TC-11.

LEGEND

1. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW SOLID LINE)
2. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW - 10' DASH, 30' SKIP)
3. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW SOLID LINE)
4. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP W/ 2 CRYSTAL / OPAQUE RRPM'S)
5. THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)
6. THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE SOLID LINE)
7. THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE SOLID LINE)
8. THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONAL LINE)
9. THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE STOP BAR)
10. THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (TYP.)
11. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE SOLID LINE)

FILE NAME = D162A51_pvmtmarking03.dgn	USER NAME = mdrane	DESIGNED - WDC	REVISED -
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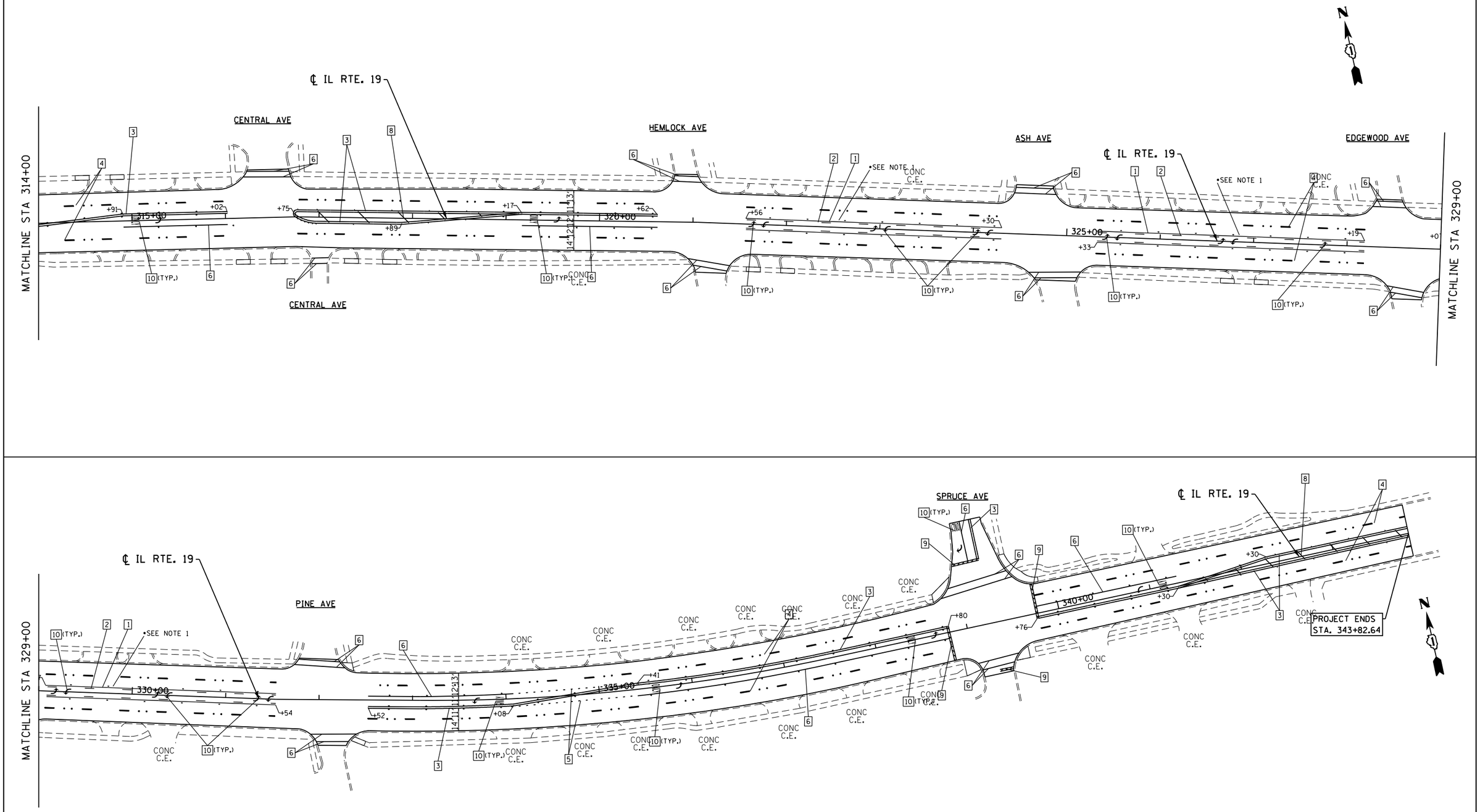
SHT_DOUBLE PLAN	PLOT DATE = 3/18/2016	DATE - 4-16-2015	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN – E. IRVING PARK ROAD

SCALE: 1" = 50' | SHEET 3 OF 4 SHEETS | STA. 284+00 TO STA. 314+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	16
		CONTRACT NO. 62A51		
	ILLINOIS FED. AID PROJECT			



NOTE:

1. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE IMPROVEMENT LIMITS. SEE DISTRICT DETAIL TC-11.

LEGEND

1. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW SOLID LINE)
2. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW - 10' DASH, 30' SKIP)
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11. THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE SOLID LINE)

FILE NAME = D162A51_pvmtmarking04.dgn	USER NAME = mdrane	DESIGNED - WDC	REVISED -
		DRAWN - WDC	REVISED -
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SHT_DOUBLE PLAN	PLOT DATE = 3/18/2016	DATE - 4-16-2015	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

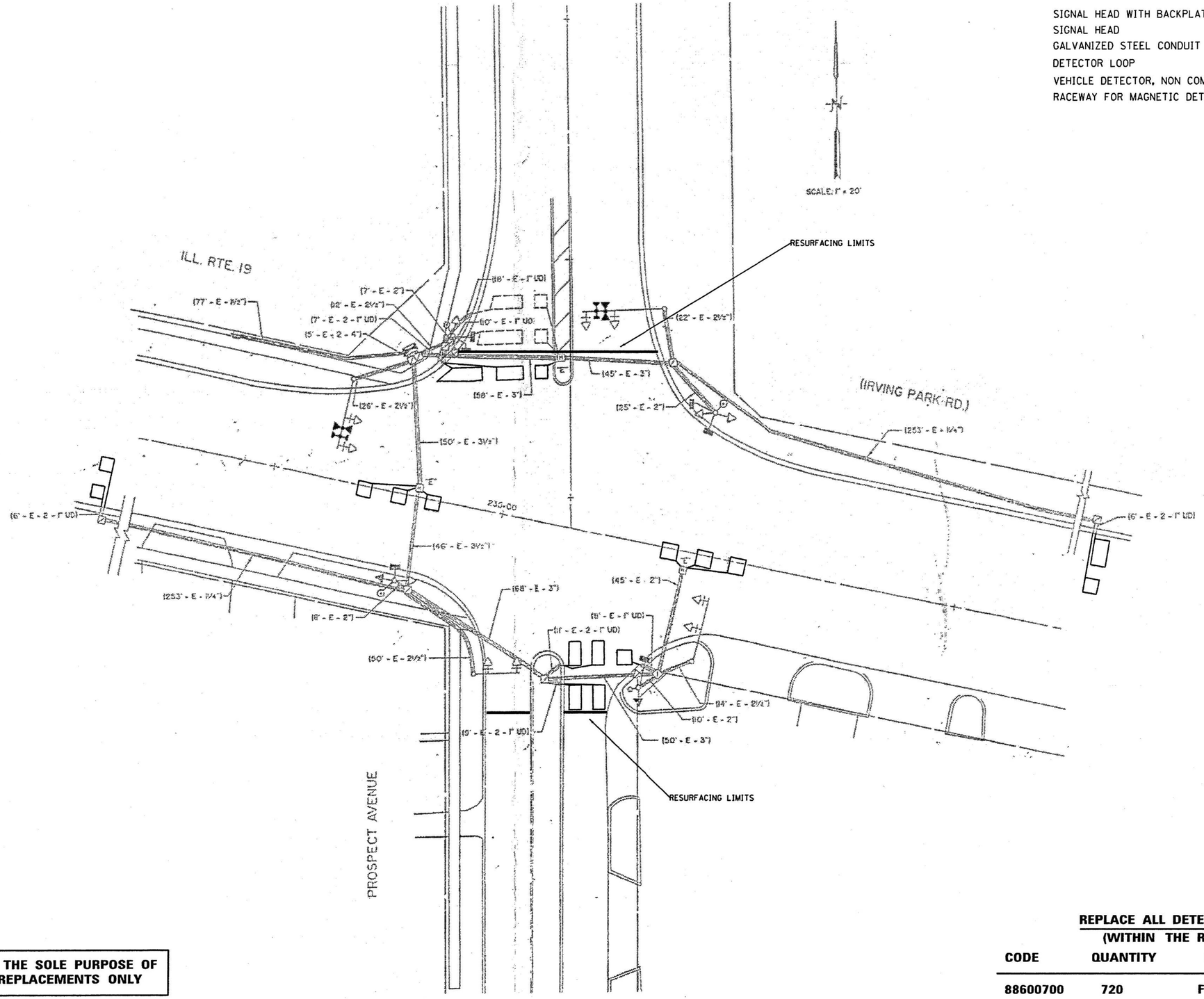
PAVEMENT MARKING PLAN – E. IRVING PARK ROAD

SCALE: 1" = 50' | SHEET 4 OF 4 SHEETS | STA. 314+00 TO STA. 343+82.64

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	17
		CONTRACT NO. 62A51		
	ILLINOIS FED. AID PROJECT			

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	-----	———
DETECTOR LOOP	□	
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		"E"



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600700	720	FOOT	PREFORMED DETECTOR LOOP

FILE NAME = S:\WP\Design\JC\Detector Loop Details\62A51\Detector Loop.dgn	USER NAME = curryja	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 100.0001 ' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/2/2015	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

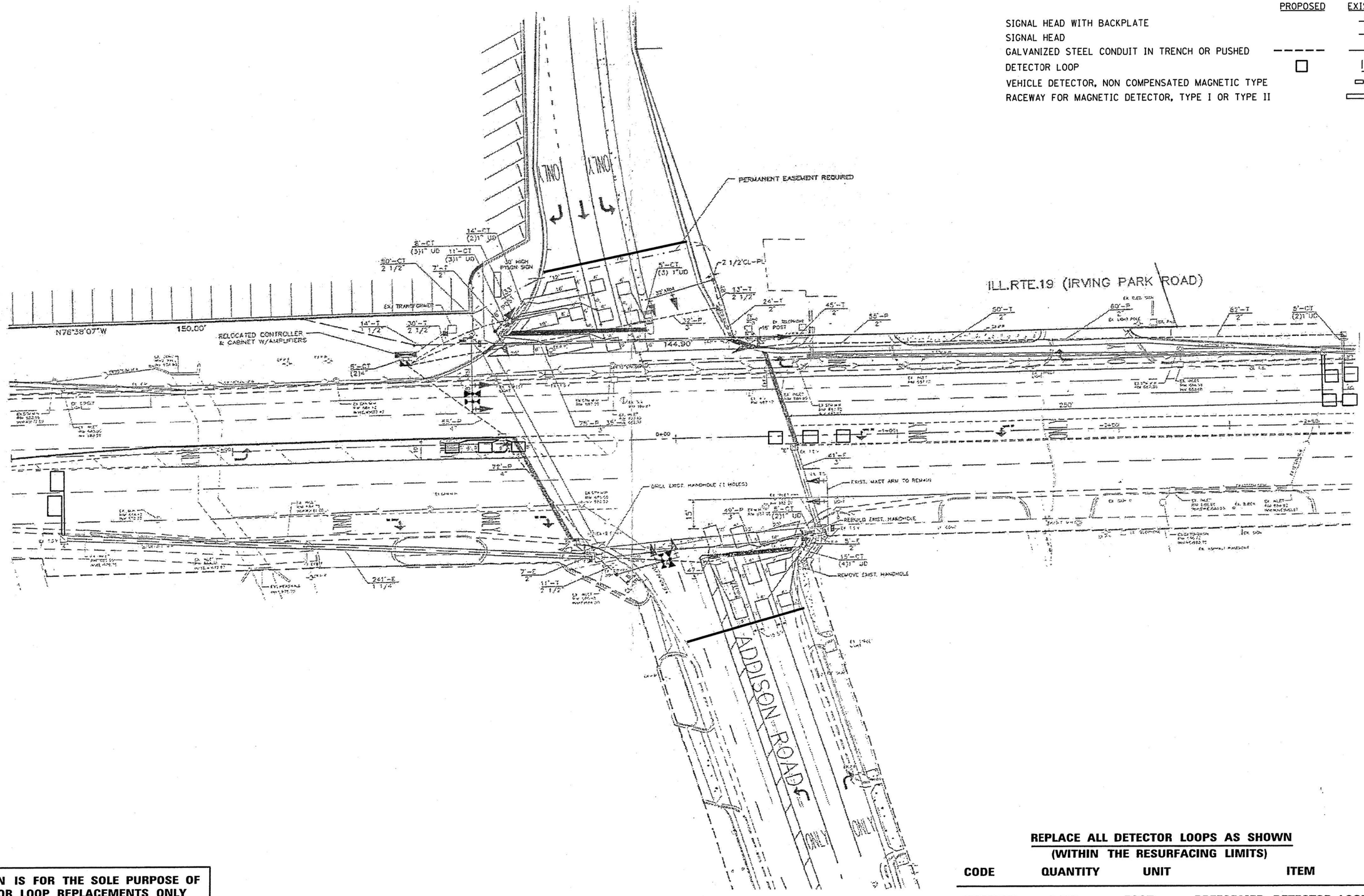
DISTRICT ONE – DETECTOR LOOP REPLACEMENT
IL 19 AT PROSPECT AVE

SCALE: NTS | SHEET NO. OF SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	18
		CONTRACT NO. 62A51		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		"E"



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600700	1,431	FOOT	PREFORMED DETECTOR LOOP

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

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	PLOT DATE = 3/2/2015	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

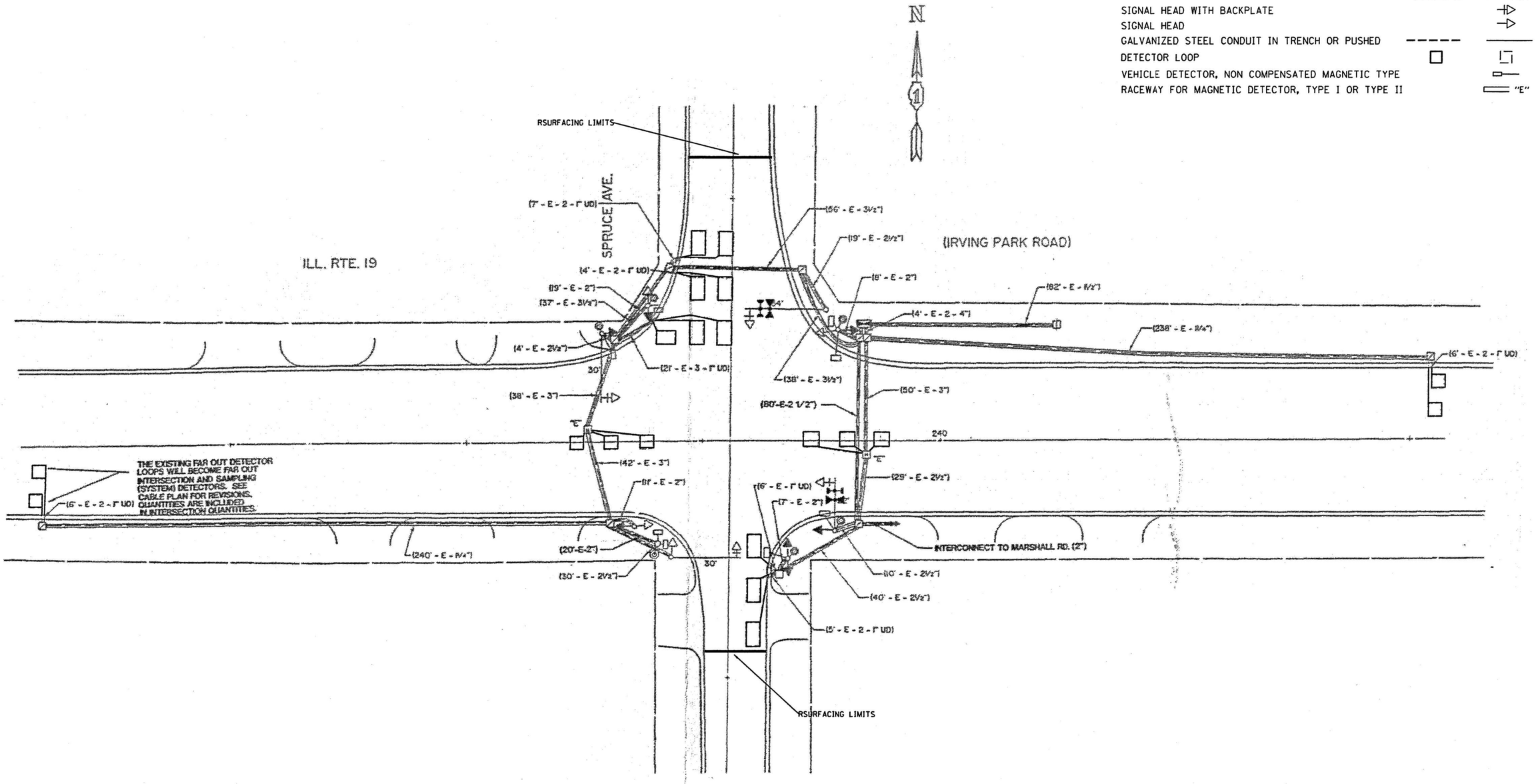
DISTRICT ONE – DETECTOR LOOP REPLACEMENT
IL 19 AT ADDISON RD

SCALE: NTS | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	19
		CONTRACT NO. 62A51		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		"E"



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE – DETECTOR LOOP REPLACEMENT
IL 19 AT SPRUCE AVE

SCALE: NTS | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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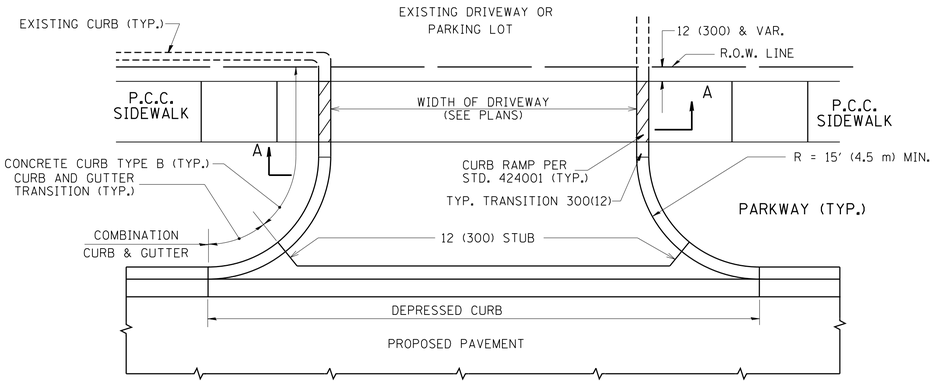
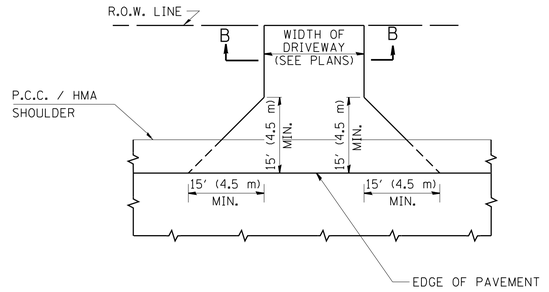
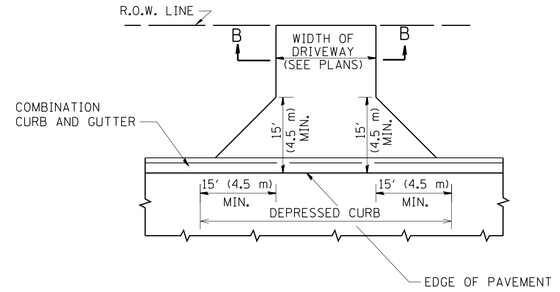
		CONTRACT NO. 62A51		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



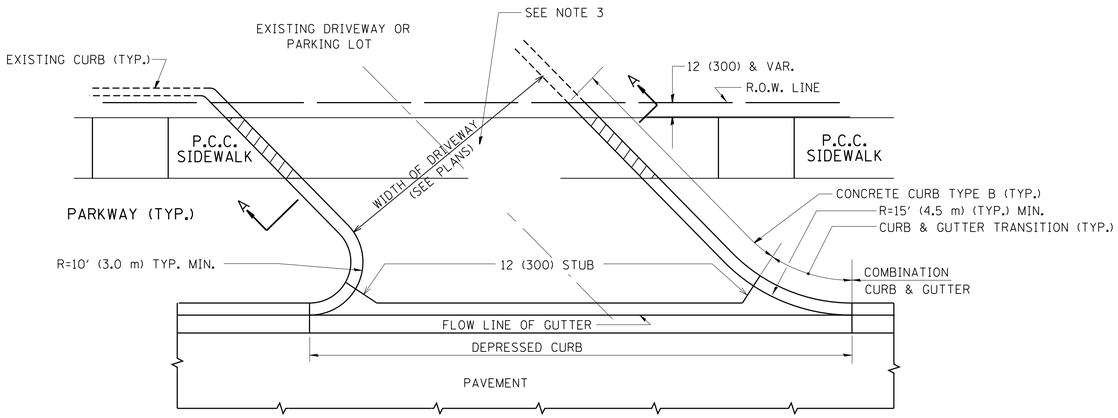
WITH CONCRETE CURB, TYPE B



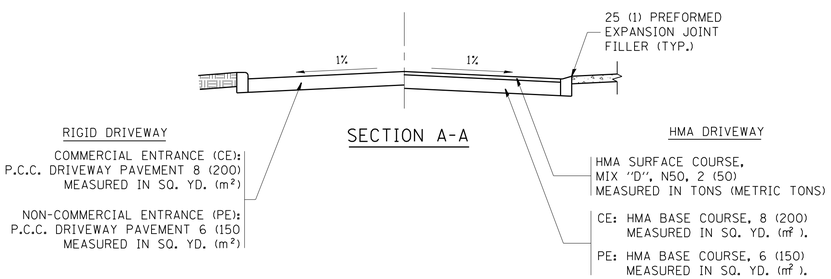
ADJACENT TO P.C.C. / HMA SHOULDER

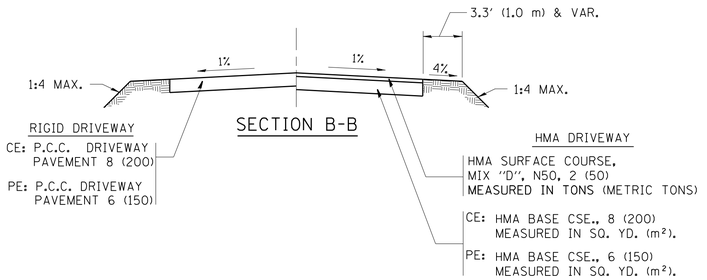


ADJACENT TO CURB AND GUTTER



WITH CONCRETE CURB, TYPE B





RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m^2).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

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		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - R. BORO 06-11-08
	PLOT DATE = 9/6/2011	DATE - 11-04-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS – DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER > = 15' (4.5 m)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	21
	BD0156-07 (BD-01)	CONTRACT NO. 62A51		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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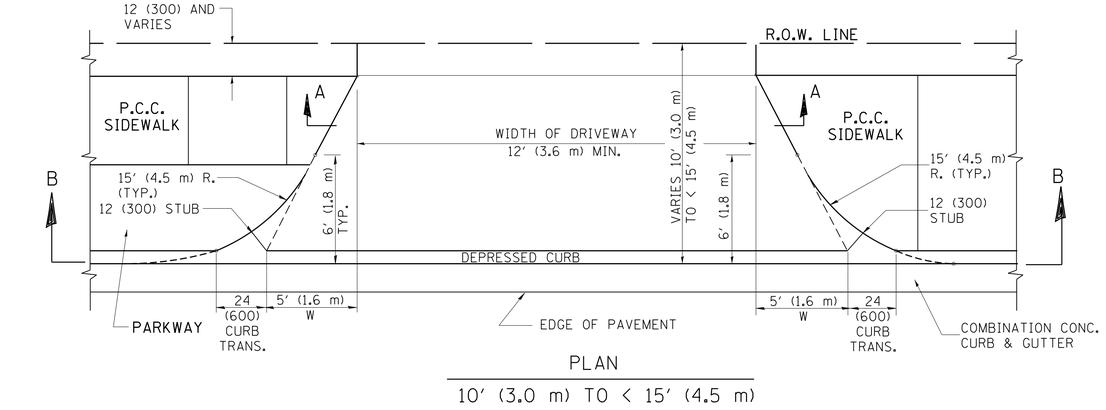
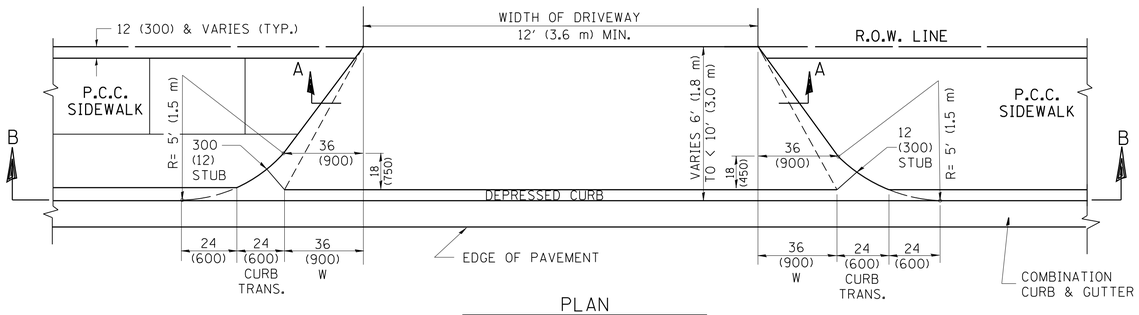


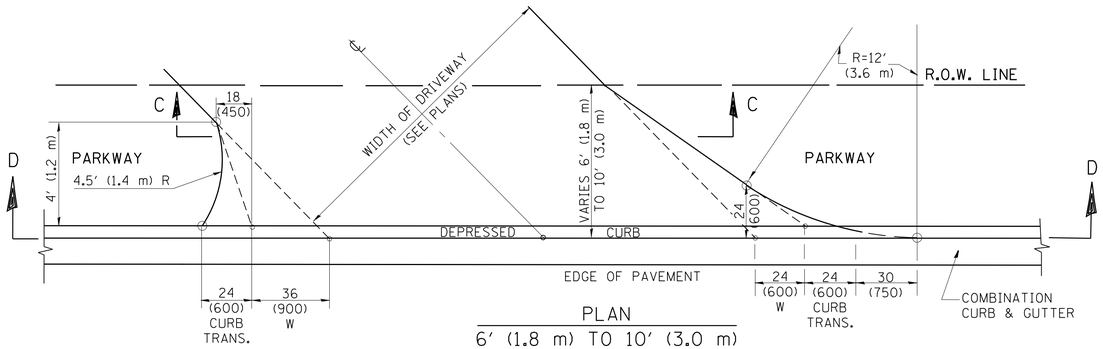
PLAN

10' (3.0 m) TO < 15' (4.5 m)



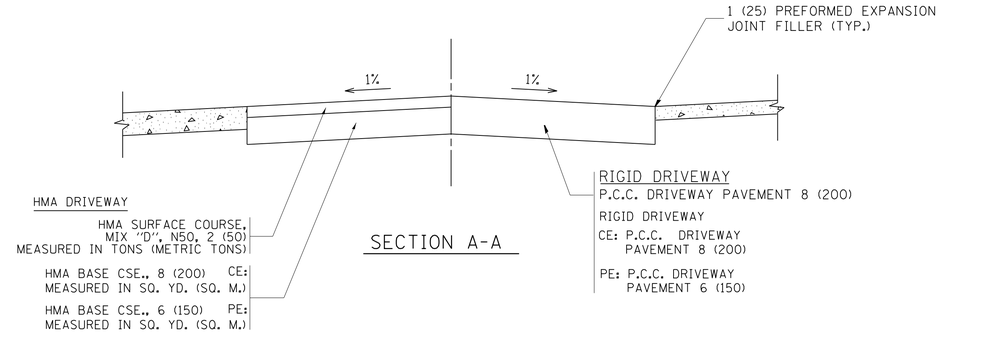
PLAN

6' (1.8 m) TO < 10' (3.0 m)

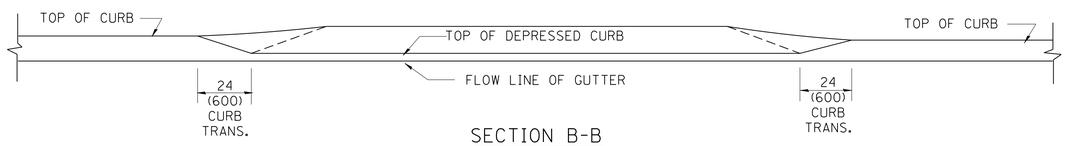


PLAN

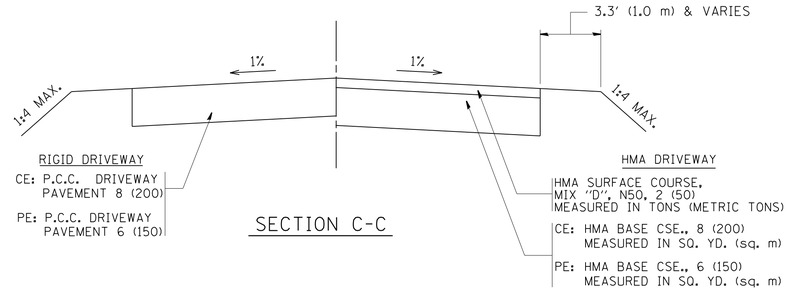
6' (1.8 m) TO 10' (3.0 m)



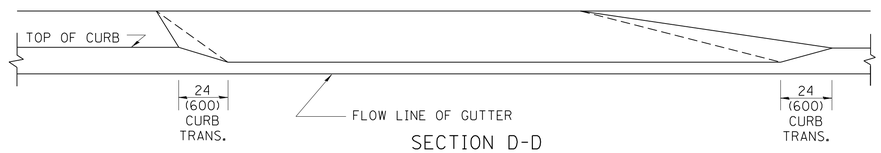
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

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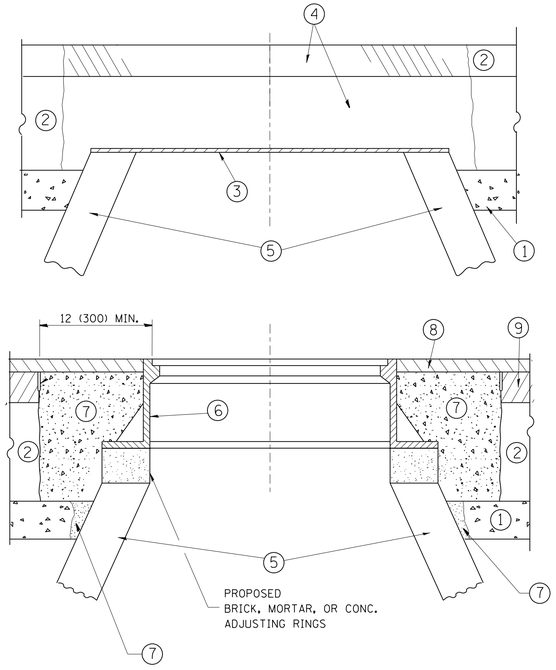
	PLOT DATE = 10/28/2011	DATE - 11-06-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	22
	BD400-02 (BD-02)	CONTRACT NO. 62A51		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS PP-1* CONCRETE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 1968.5000 ' / m	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	23
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FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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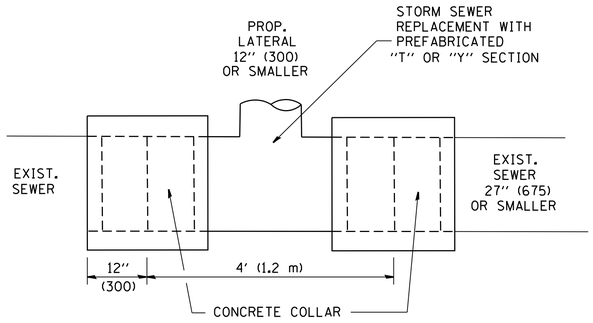
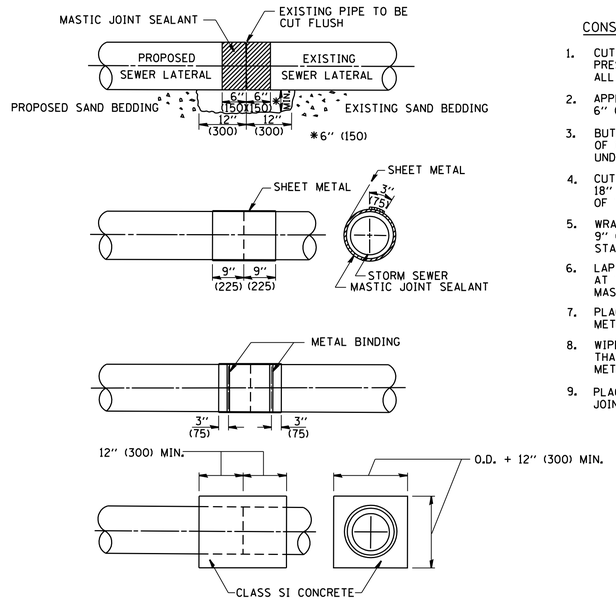


DETAIL ''A''

LATERAL CONNECTION TO EXISTING SEWER
OF 27'' (675) OR SMALLER

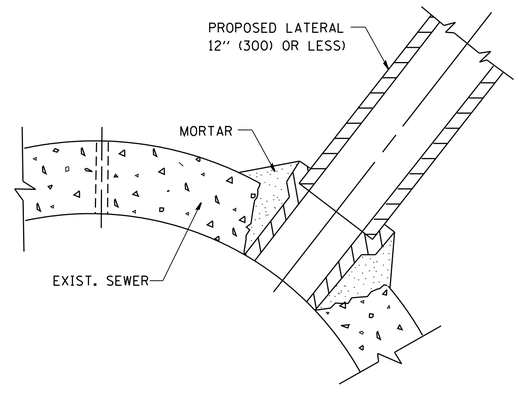


DETAIL ''B''

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6'' (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' × 6' (300 × 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18'' (450) WIDE BY THE OUTSIDE CIRCUMFERANCE OF THE PIPE PLUS 3'' (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9'' (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3'' (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL ''C''

PROPOSED LATERAL
CONNECTION TO EXISTING SEWER
OF 30'' (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.

II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:

A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27'' (675) OR SMALLER SEE DETAIL ''A'' AND ''B''.

B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30'' (750) OR LARGER SEE DETAIL ''C''.

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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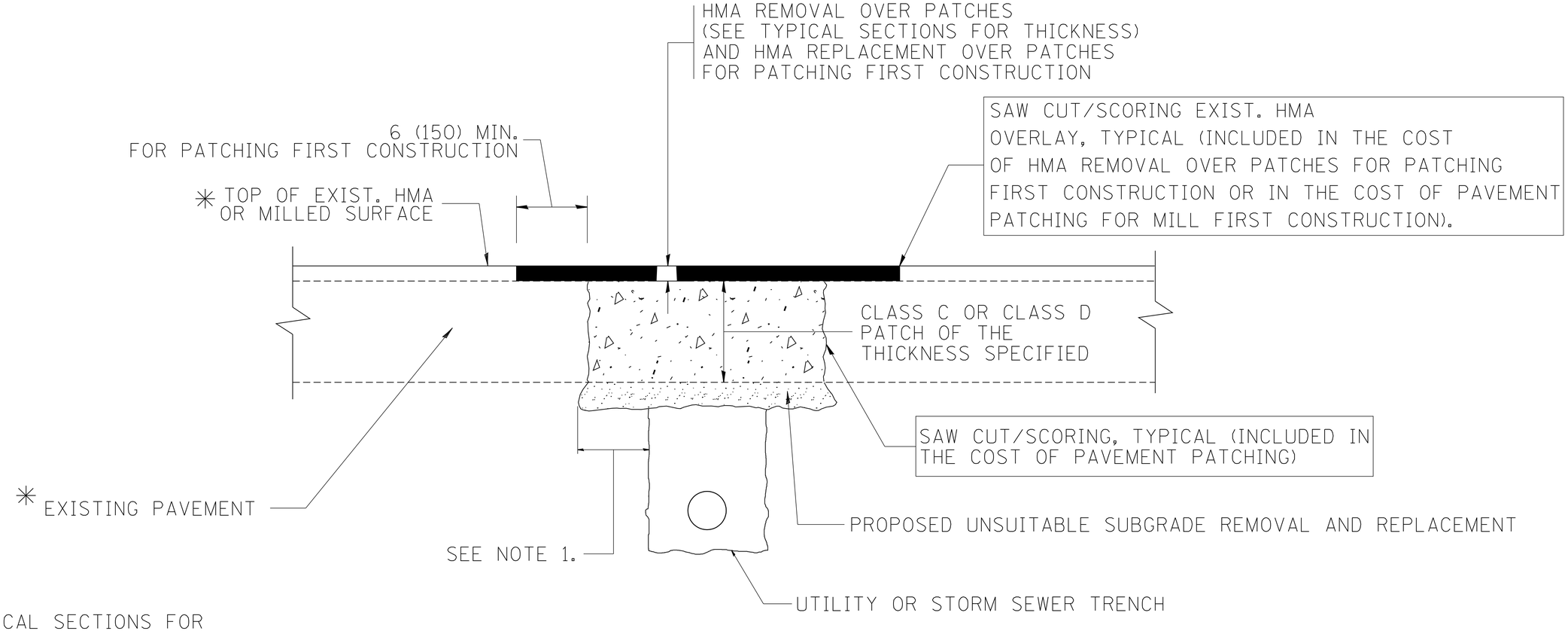
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - R. SHAH 10-25-94					BD500-01 (BD-7)		CONTRACT NO.		
	PLOT DATE = 1/4/2008	DATE - 07-25-90	REVISED - R. SHAH 06-12-96		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



✳ SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST $4\frac{1}{2}$ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

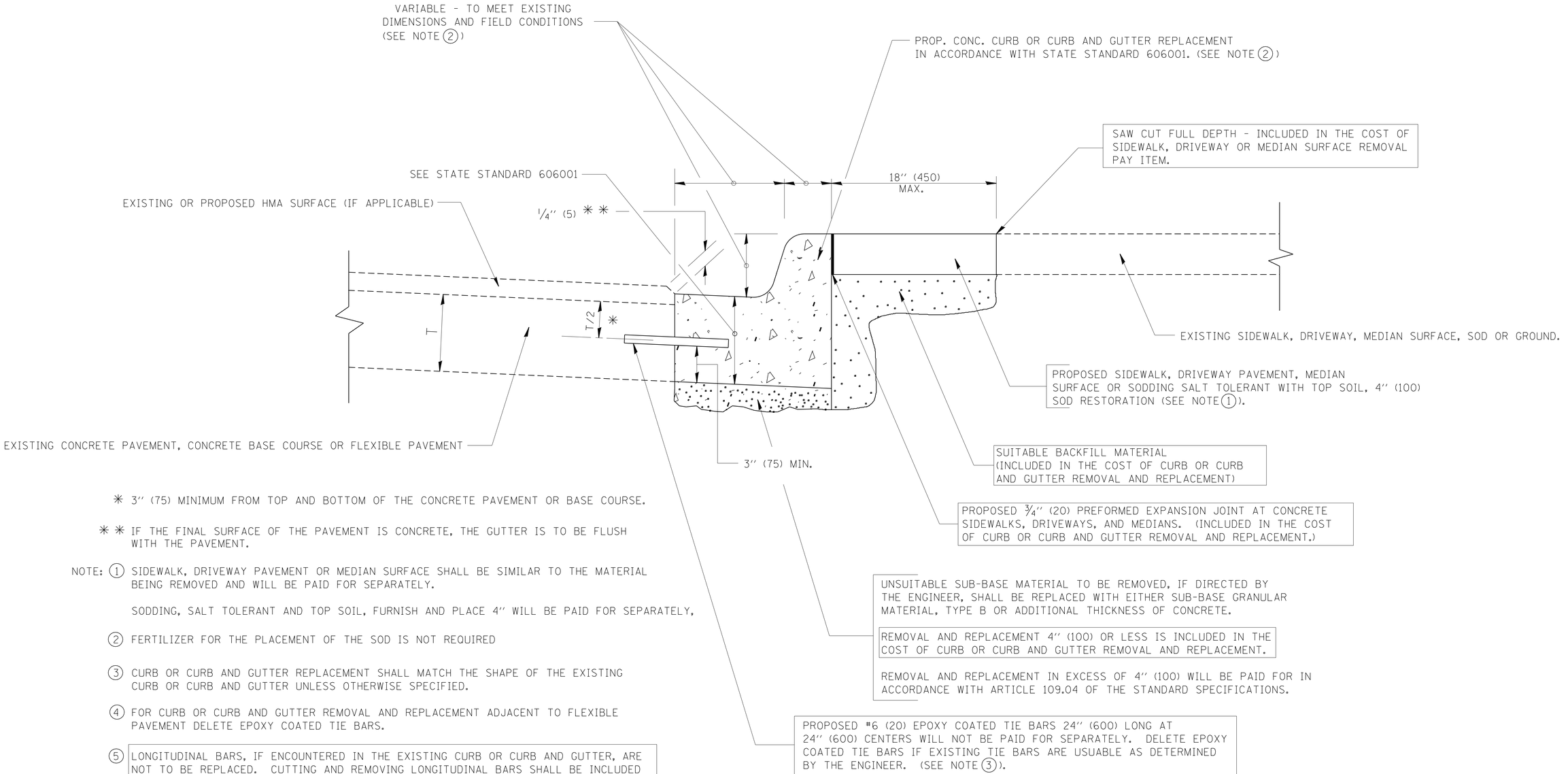
FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	24
	BD400-04 (BD-22)	CONTRACT NO. 62A51		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

(2) FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

(3) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(4) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(5) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(7) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(8) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

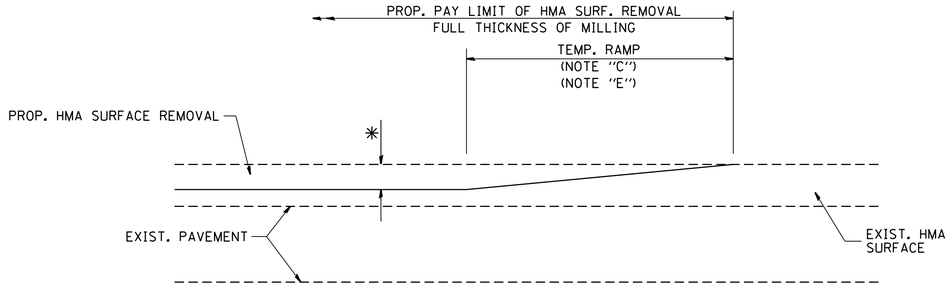
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\pw_work\pwidot\drivakosgn\d0108315\bd24.dgn	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT**
		DRAWN -	REVISED - A. ABBAS 03-21-97		
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01		SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.
	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09		

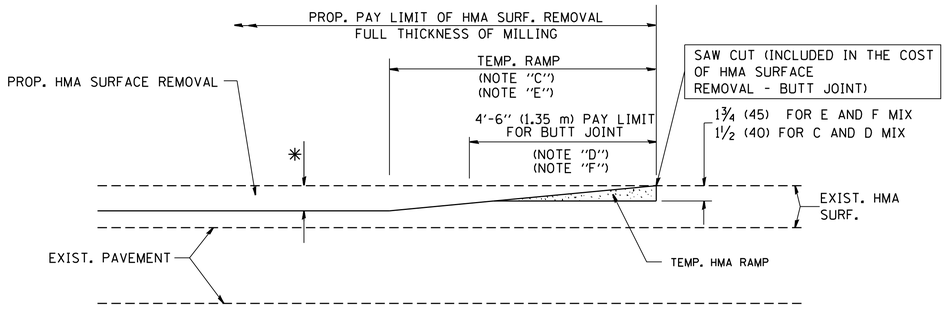
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	25
	BD600-06 (BD-24)	CONTRACT NO. 62A51		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

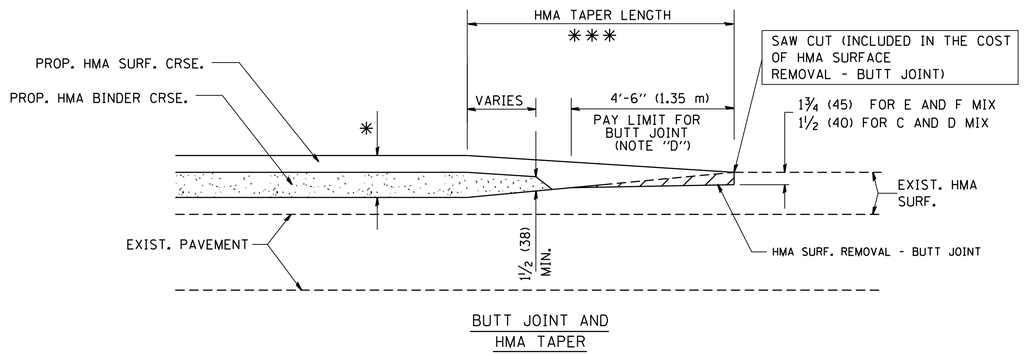


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

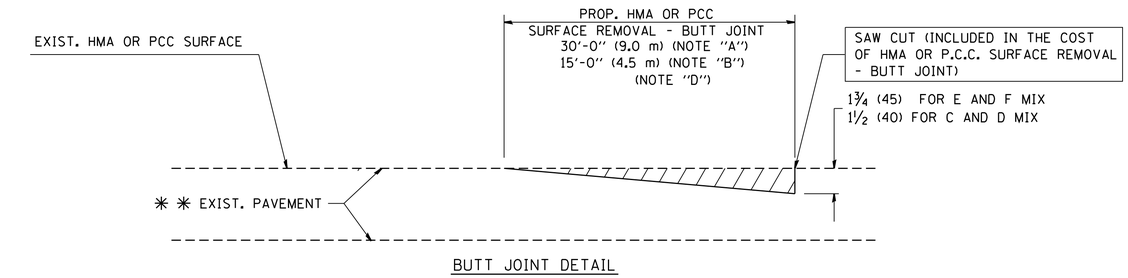
OPTION 2

TYPICAL TEMPORARY RAMP

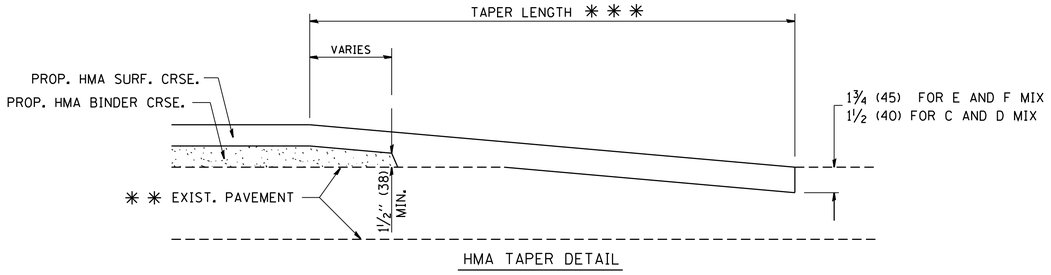


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

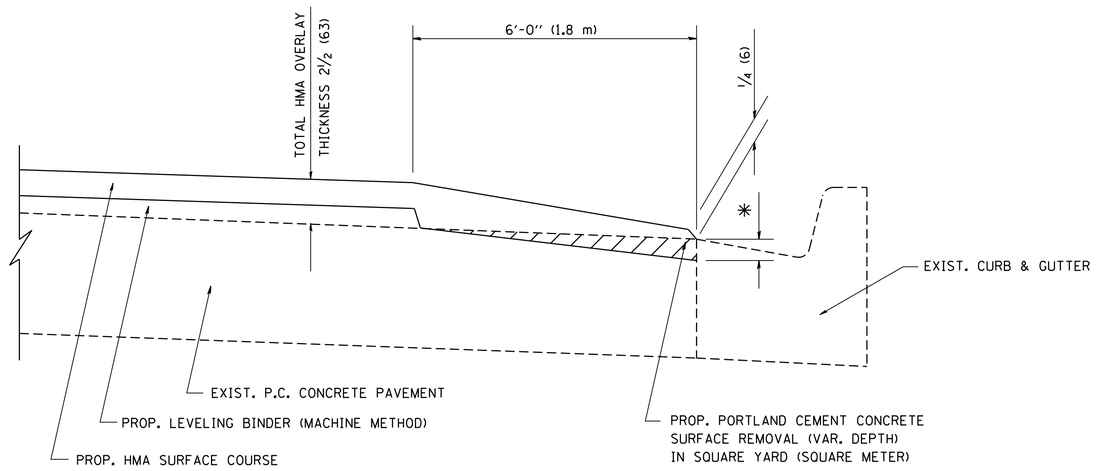
FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	26
	BD400-05 BD32	CONTRACT NO. 62A51		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



HMA TAPER AT EDGE OF P.C.C PAVEMENT

HMA SURFACE		LEVELING BINDER	
MIX	THICKNESS	THICKNESS	* MILLING AT GUTTER FLAG
C OR D	1½ (38)	1 (25)	1¼ (33)
F	1¾ (44)	¾ (19)	1½ (38)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

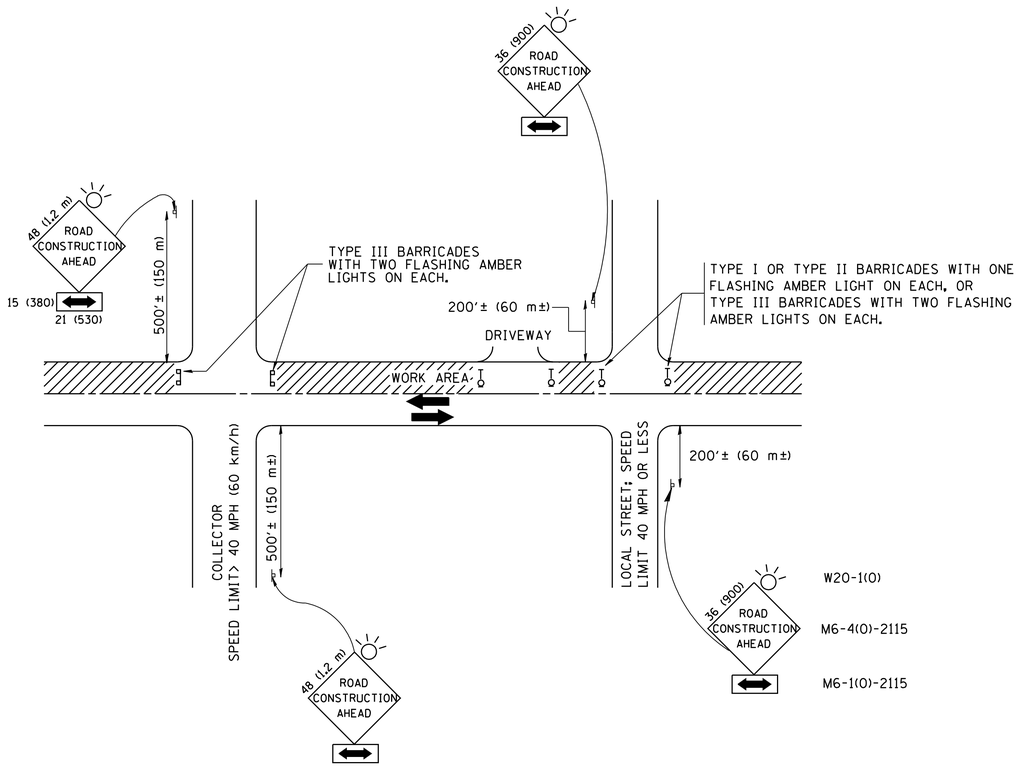
FILE NAME = W:\diststd\22x34\bd33.dgn	USER NAME = gaglianobt	DESIGNED - R. SHAH	REVISED - R. SHAH 10-25-94
		DRAWN - JIS	REVISED - A. ABBAS 05-05-99
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - A. ABBAS	REVISED - E. GOMEZ 12-21-00
	PLOT DATE = 1/4/2008	DATE - 09-10-94	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HMA TAPER AT
EDGE OF P.C.C. PAVEMENT

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	27
	BD400-06 (BD33)	CONTRACT NO. 62A51		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

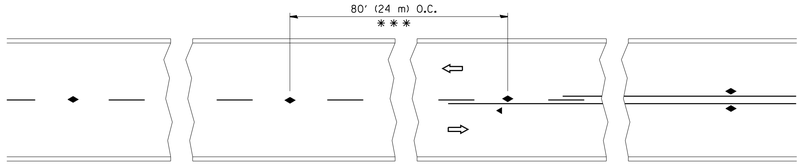
FILE NAME = W:\diststd\22x34\tc10.dgn	USER NAME = gaglianobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

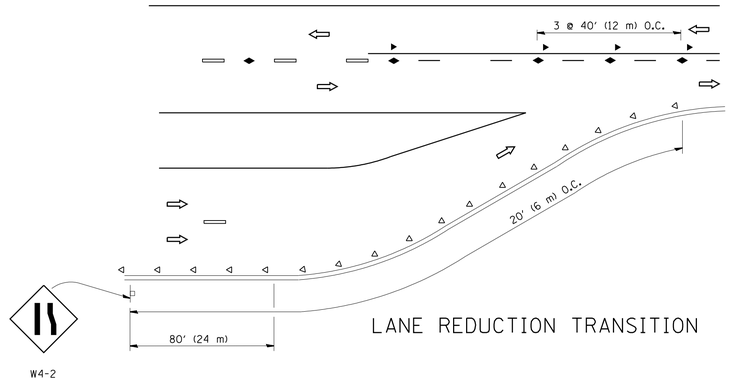
SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	28
	TC-10	CONTRACT NO. 62A51		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

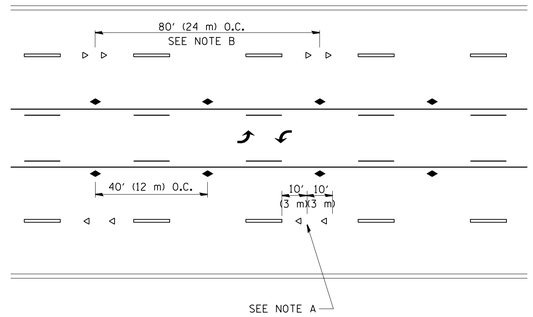


* * * REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

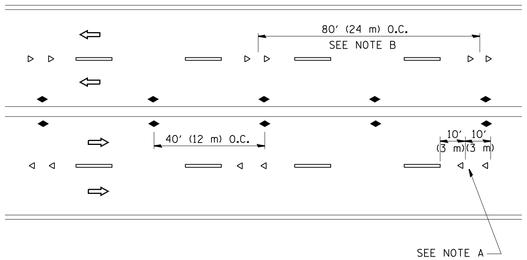
TWO-LANE/TWO-WAY



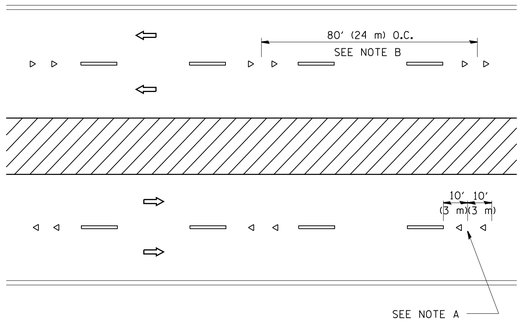
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

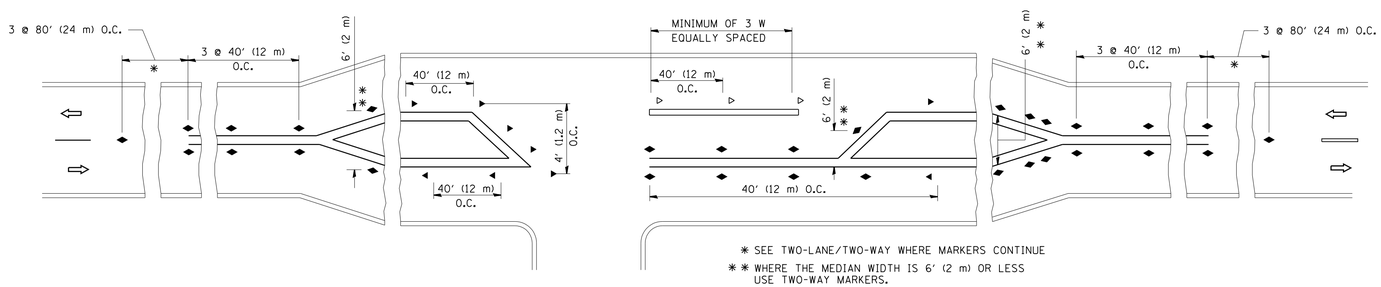
LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

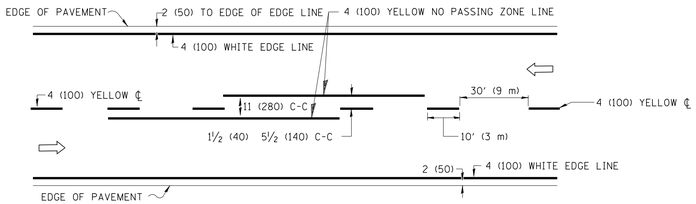
** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

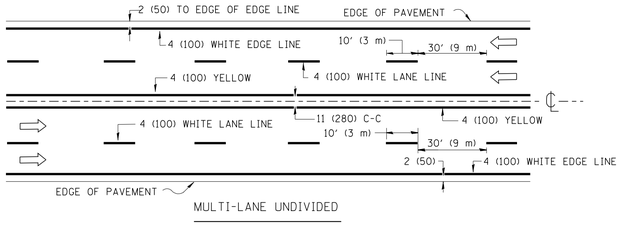
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = c:\pw_work\pwidot\leysa\d0108315\tc11.dgn	USER NAME = leysa	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**
		DRAWN -	REVISED - T. RAMMACHER 03-12-99		
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00		
	PLOT DATE = 3/2/2011	DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

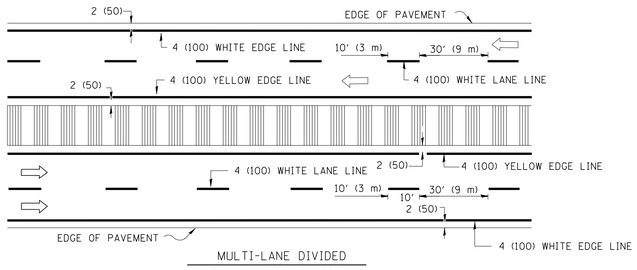
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	29
	TC-11	CONTRACT NO. 62A51		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



2-LANE ROADWAY



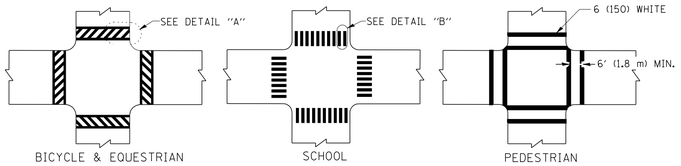
MULTI-LANE UNDIVIDED

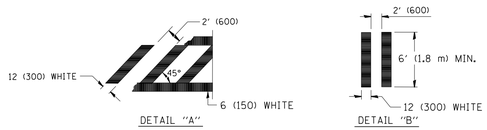


MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

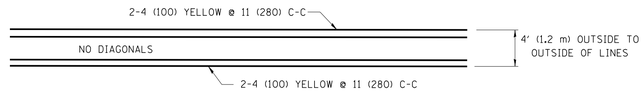
NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

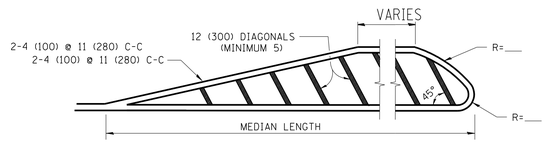




TYPICAL CROSSWALK MARKING



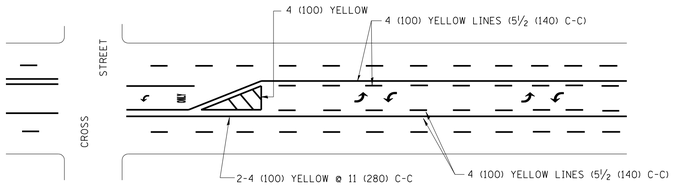
4' (1.2 m) WIDE MEDIANS ONLY



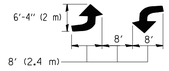
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

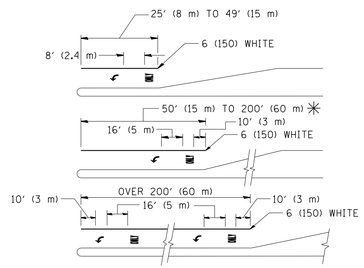


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

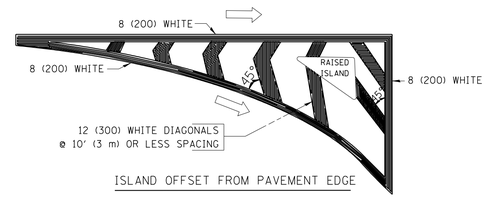


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1.9 m^2)

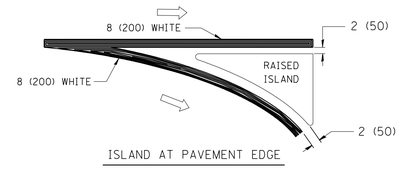
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 ø 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 ø 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 ø 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 ø 6 (150) 12 (300) ø 45° 12 (300) ø 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 ø 4 (100) WITH 12 (300) DIAGONALS ø 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS ø 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS	12 (300) ø 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

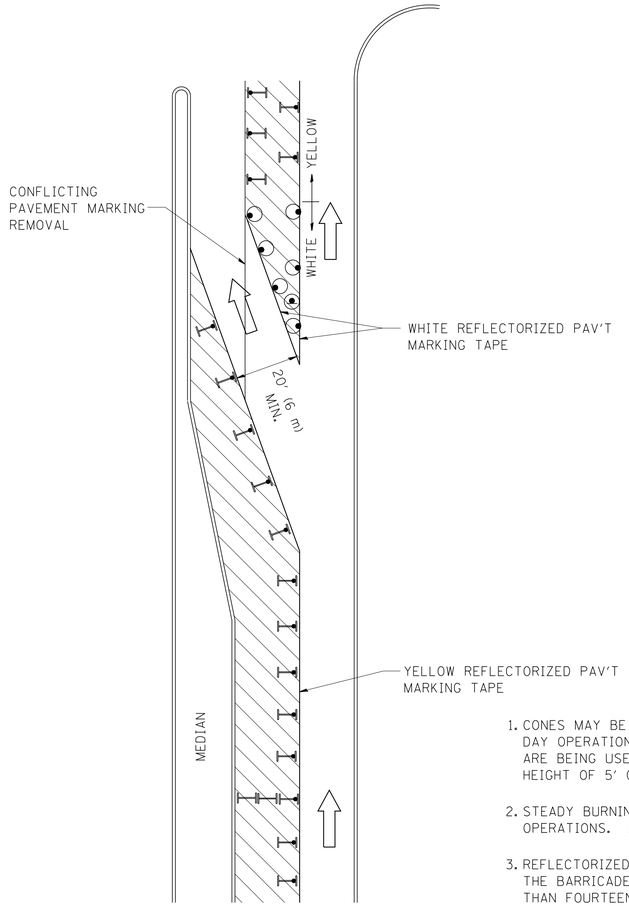
FILE NAME = c:\pw_work\pwidot\drivakosgn\d0108315\tc13.dgn	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
		DRAWN -	REVISED -C. JUCIUS 09-09-09
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

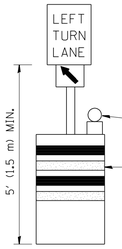
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	30
	TC-13	CONTRACT NO. 62A51		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		





R 3-1100L 24 x 24 (600 x 600)

M6-2L 21 x 15 (530 x 380)

OPTIONAL FLASHING LIGHT

STANDARD 701901
SEE NOTE 8

GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

WORK AREA

LANE OPEN TO TRAFFIC

TYPE I OR II BARRICADE WITH
STEADY BURN LIGHT

DRUM WITH STEADY BURN LIGHT

DRUM WITH SIGN (WITH OPTIONAL FLASHING
LIGHT) SEE DETAIL

TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters)
unless otherwise shown.

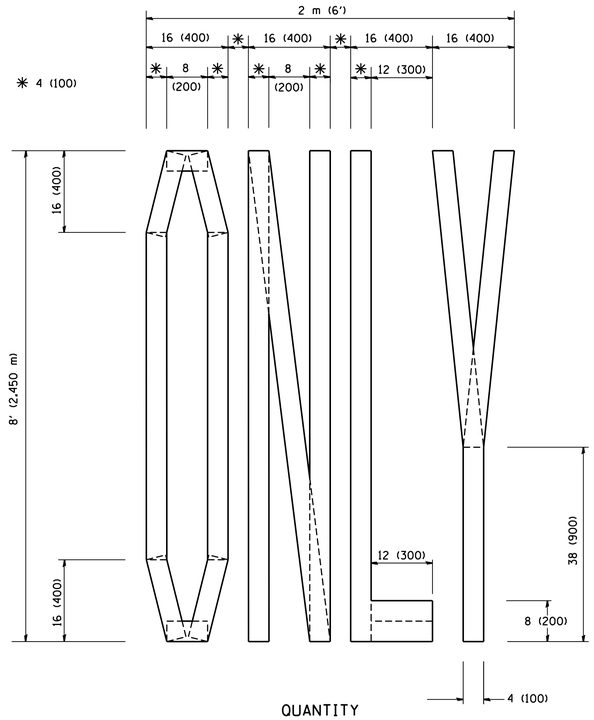
FILE NAME = c:\pw_work\PWIDOT\DRIVAKOSGN\d0108315\tc14.dgn	USER NAME = drivakosgn	REVISED -T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
		REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 49.9999 '/ IN.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 9/14/2009	REVISED -T. RAMMACHER 01-06-00	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

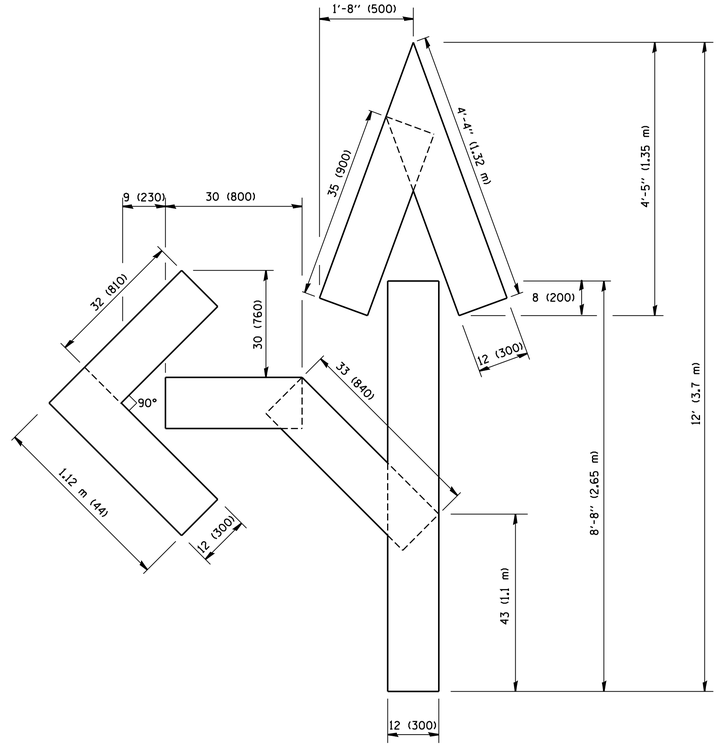
TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

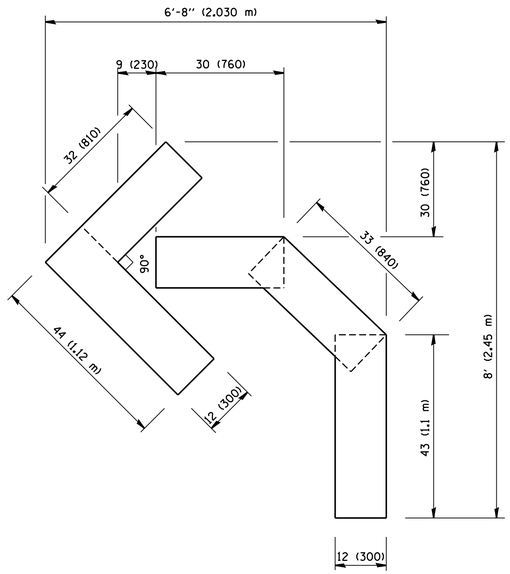
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	31
	TC-14	CONTRACT NO. 62A51		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

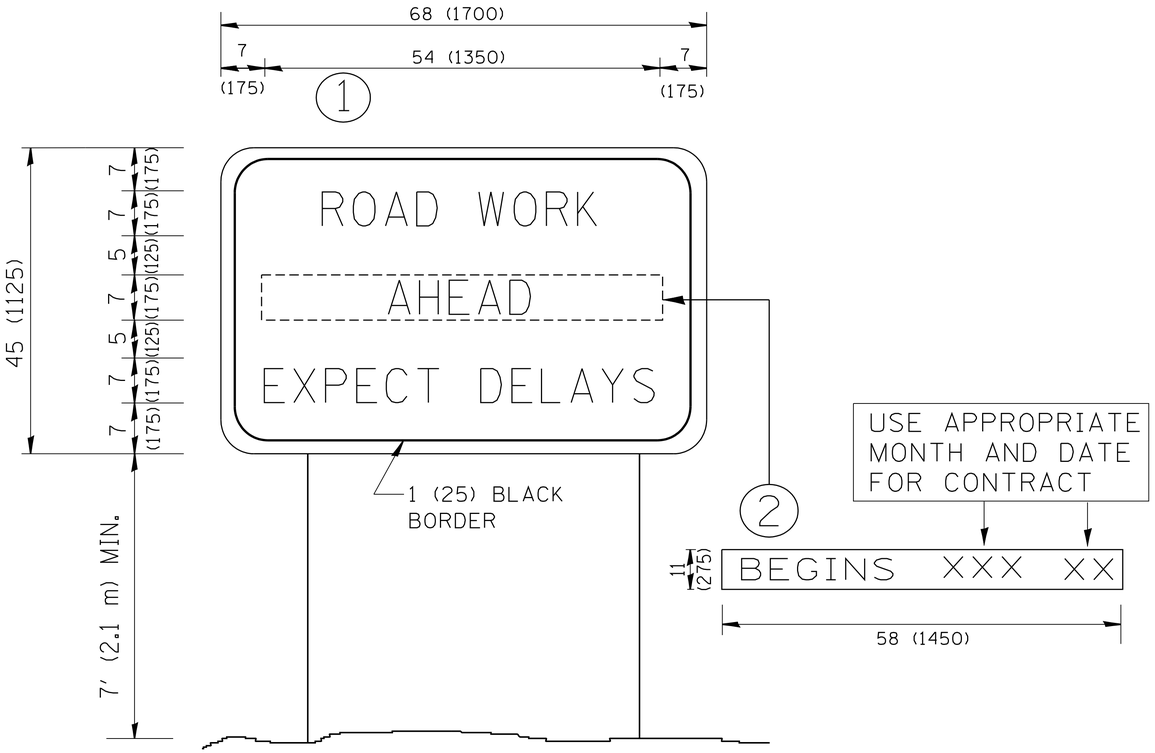
FILE NAME = W:\diststd\22x34\tc16.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000 '/ IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	32
	TC-16	CONTRACT NO. 62A51		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE ''ROAD CONSTRUCTION AHEAD'' SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR ''TEMPORARY INFORMATION SIGNING'' FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

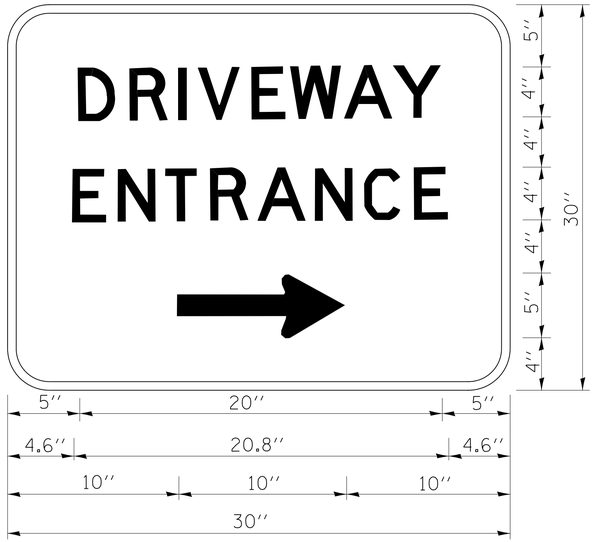
FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	33
	TC-22	CONTRACT NO. 62A51		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



3.0'' RADIUS, 0.5'' BORDER, WHITE ON GREEN; REFLECTORIZED
''DRIVEWAY'' D; ''ENTRANCE'' D; STANDARD ARROW CUSTOM 12.0'' x 5.0''

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM ''TEMPORARY INFORMATION SIGNING''.

FILE NAME = c:\pw_work\pwidot\gaglianobt\d0108315\tc26.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
		DRAWN -	REVISED -
	PLOT SCALE = 50.000 ' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/13/2012	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

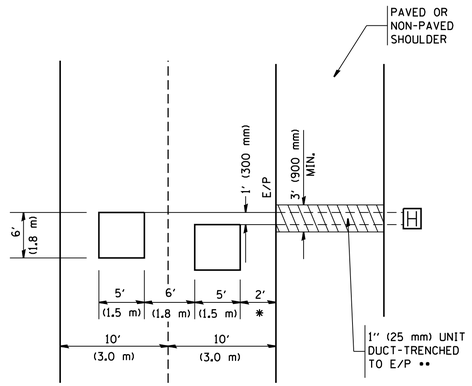
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	34
	TC-26	CONTRACT NO. 62A51		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

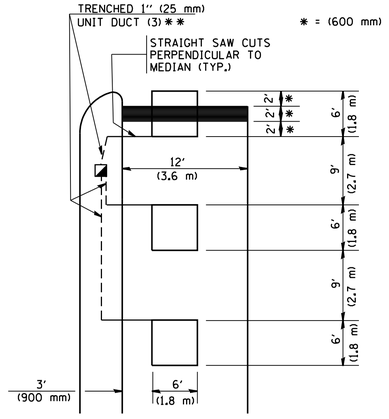


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

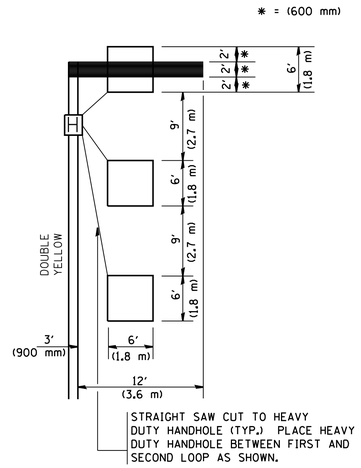
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

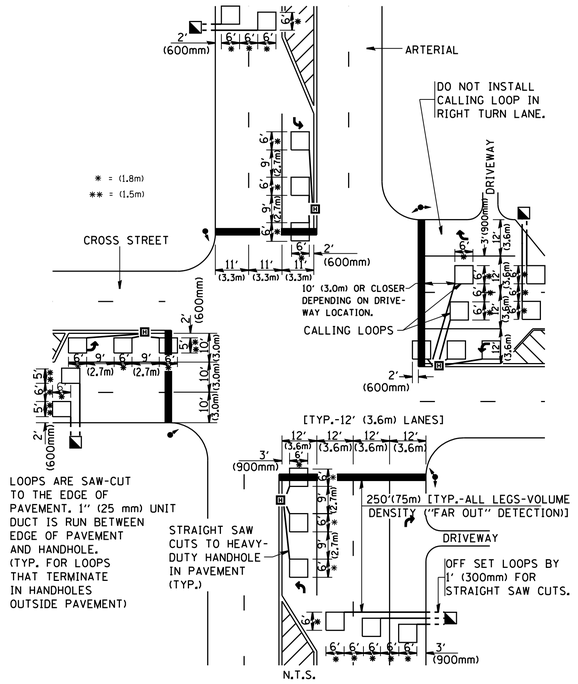
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



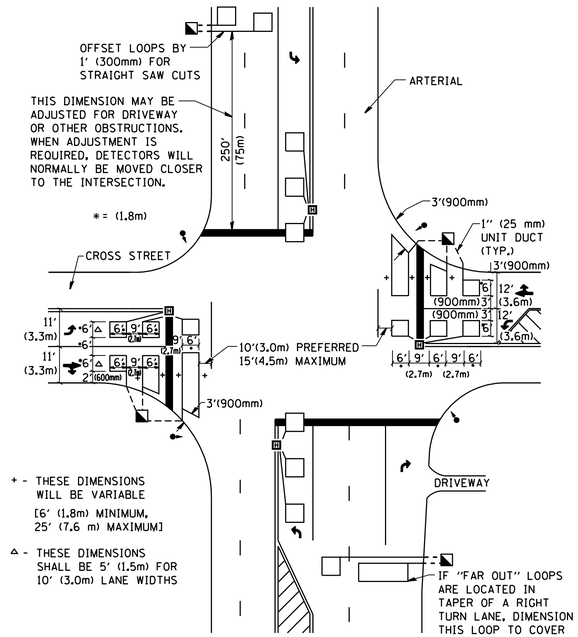
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\diststd\22x34\ts07.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - R.K.F.	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS-6(15)	DUPAGE	35	35
	TS-07	CONTRACT NO. 62A51		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		