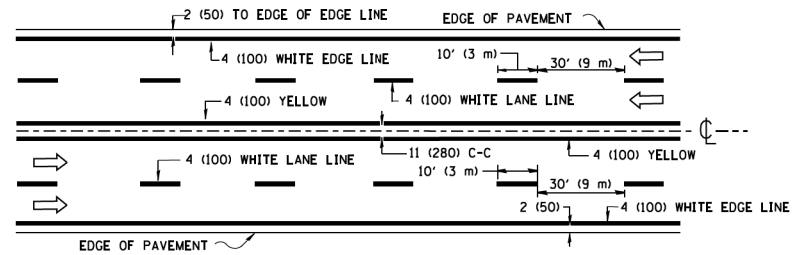
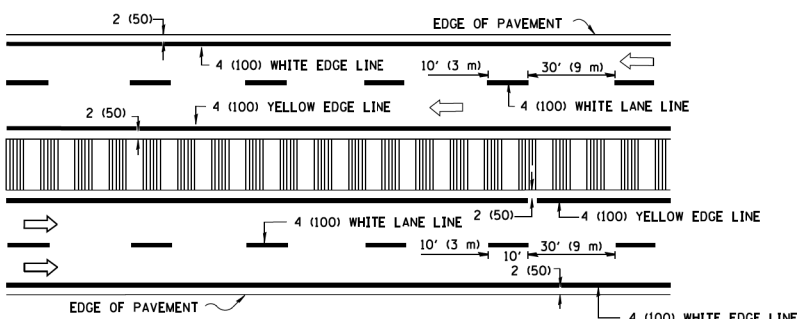


2-LANE ROADWAY

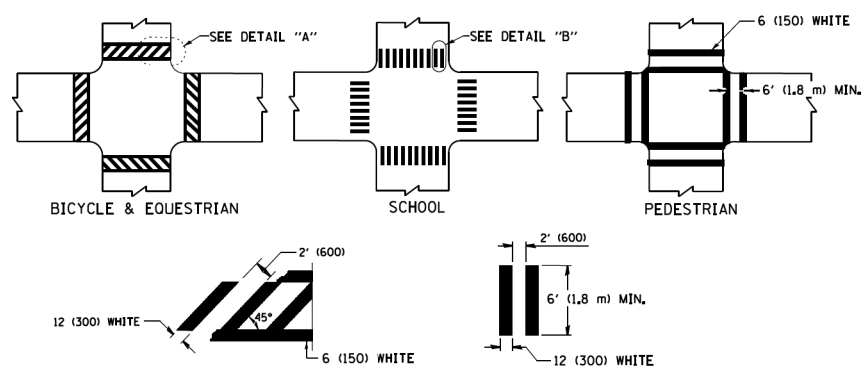


MULTI-LANE UNDIVIDED



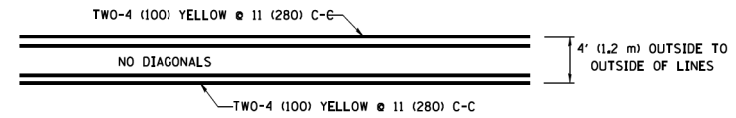
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

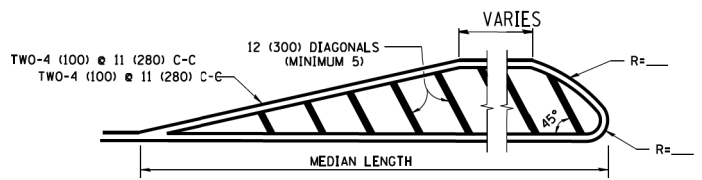


TYPICAL CROSSWALK MARKING

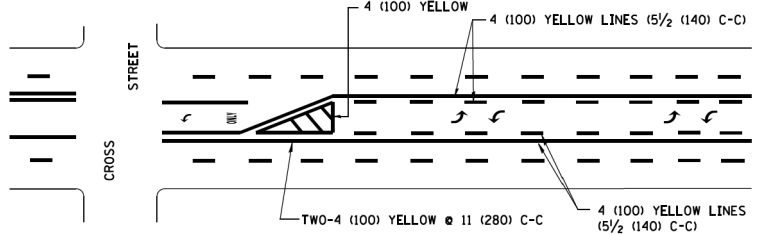
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



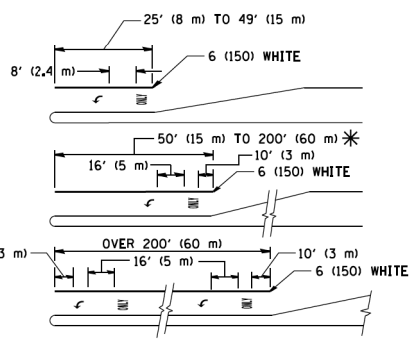
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE



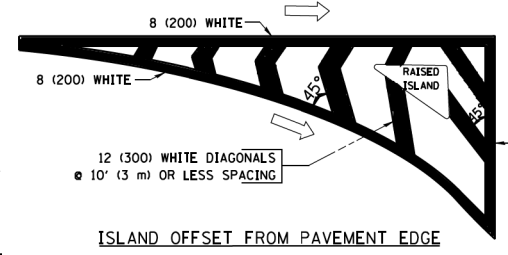
MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING



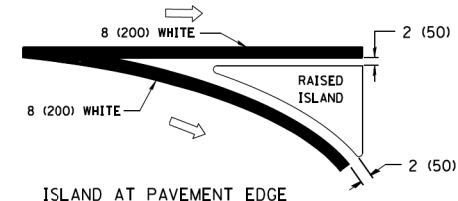
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

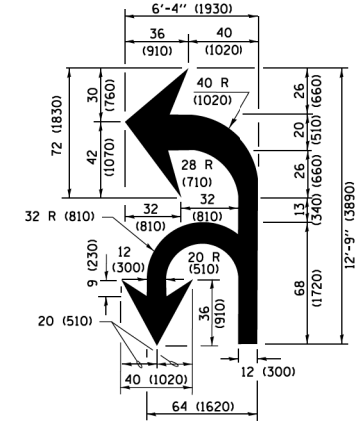


ISLAND OFFSET FROM PAVEMENT EDGE

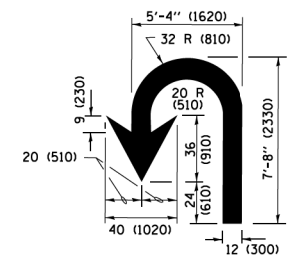


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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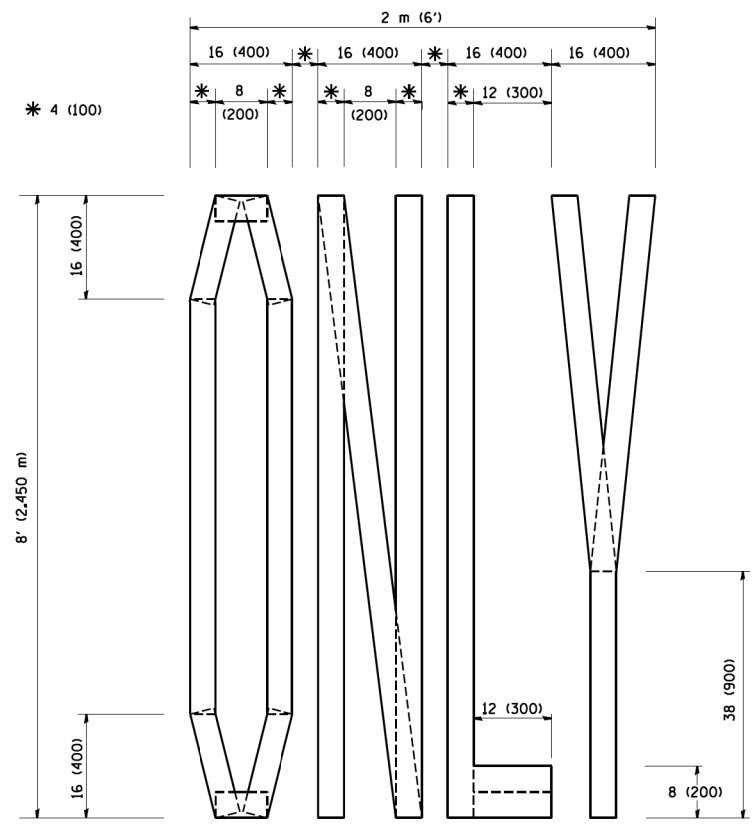
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	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

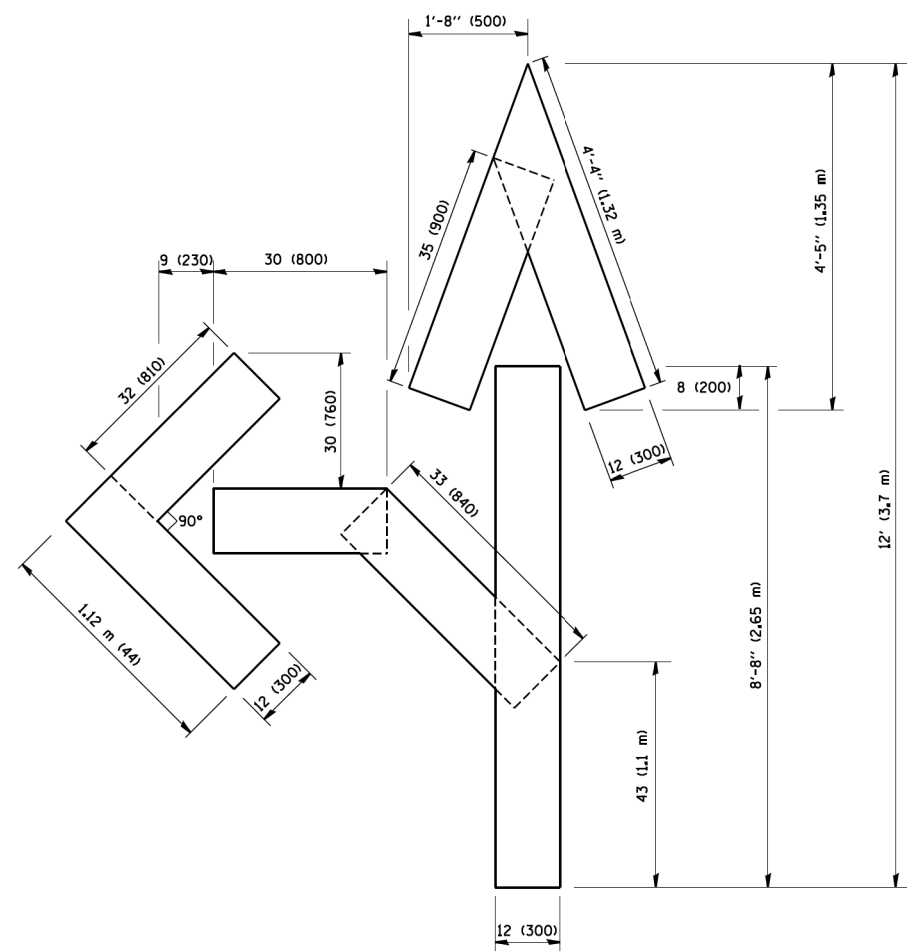
DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NTS SHEET 2 OF 14 SHEETS STA. N/A TO STA. N/A

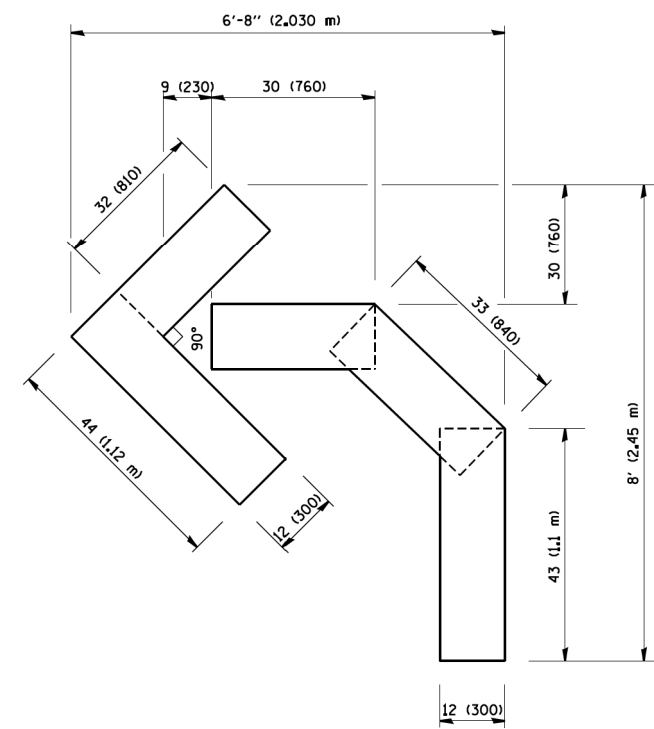
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02&03	WILL	580	101
TC-13			CONTRACT NO. 62B33	
ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT DATE = 3/23/2016	CHECKED - NJB	REVISED -
	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			
SCALE: NTS	SHEET 3 OF 14 SHEETS	STA. N/A TO STA. N/A	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02&03	WILL	580	102
TC-16			CONTRACT NO. 62B33	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FILE NAME = c:\ywork\ch\mick_becker\transportation\m11335\01628233-ah1-des-01-15.dwg

ROUTE MARKERS

FOR U.S. ROUTES
M1-40-2424

FOR ILLINOIS ROUTES
M1-50-2424

R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

ARROWS SIGNS

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-2-2115

M6-3-2115

CARDINAL DIRECTION & DETOUR SIGNS

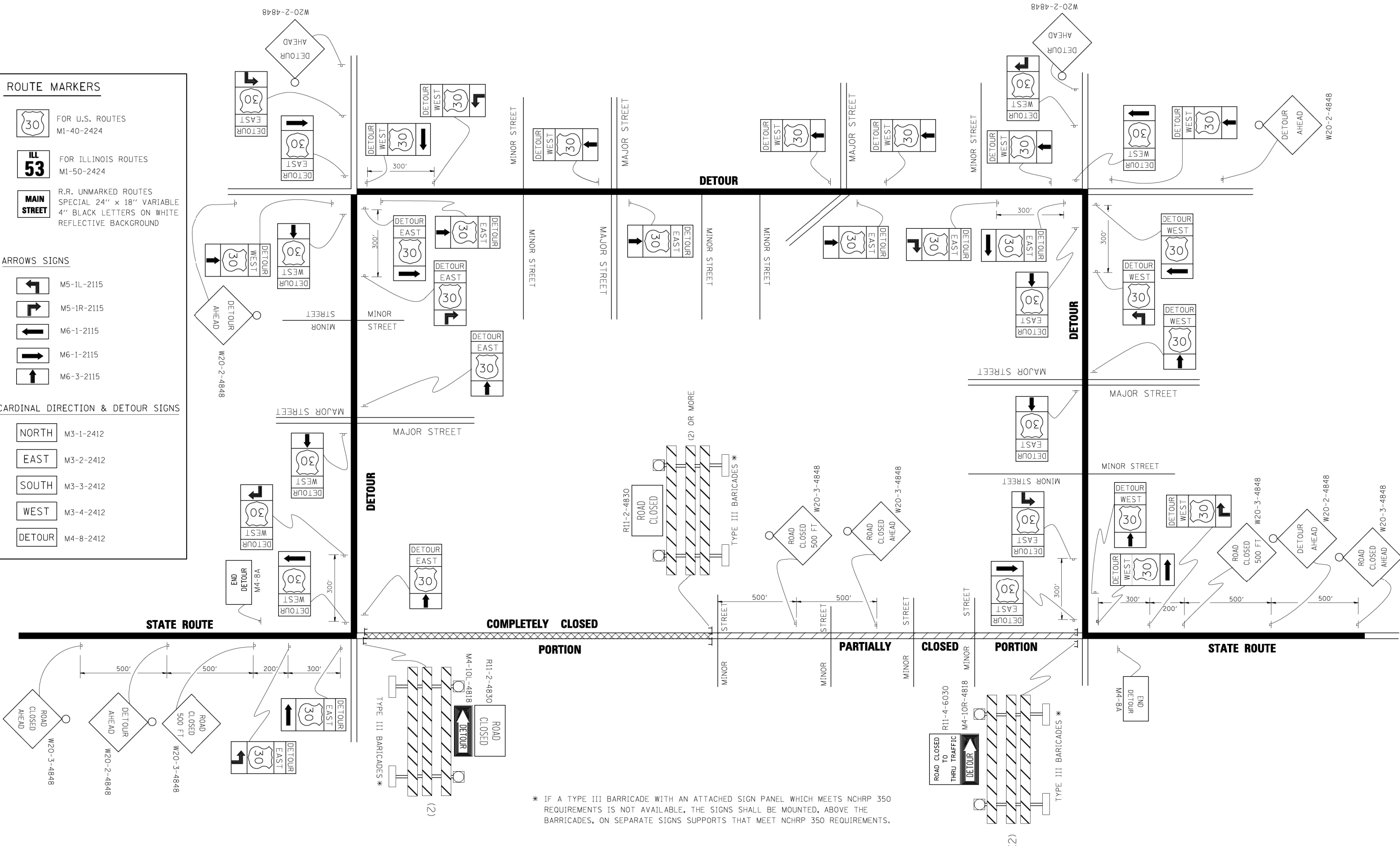
NORTH M3-1-2412

EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

DETOUR M4-8-2412



* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.



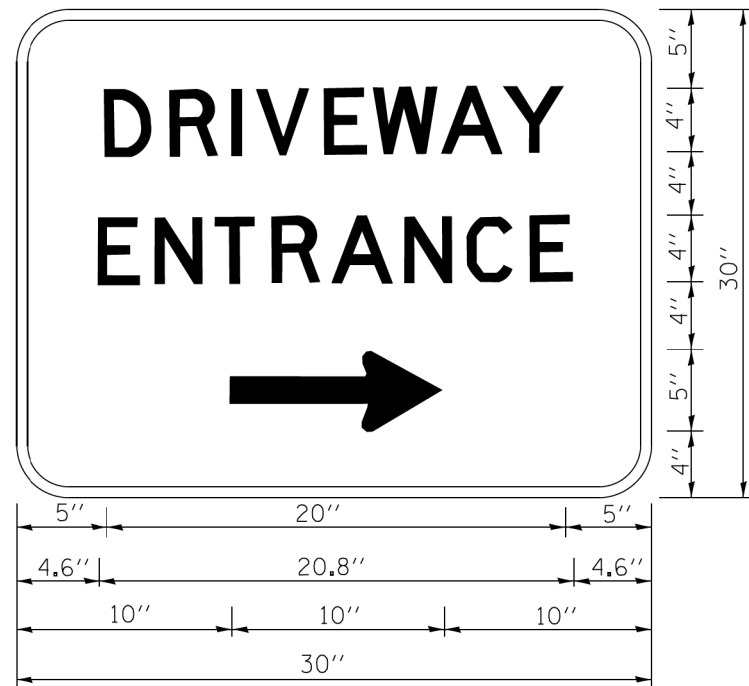
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PLOT SCALE = 48.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETOUR SIGNING
FOR CLOSING STATE HIGHWAYS**

SCALE: NTS SHEET 4 OF 14 SHEETS STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02&03	WILL	580	103
TC-21			CONTRACT NO. 62B33	
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				

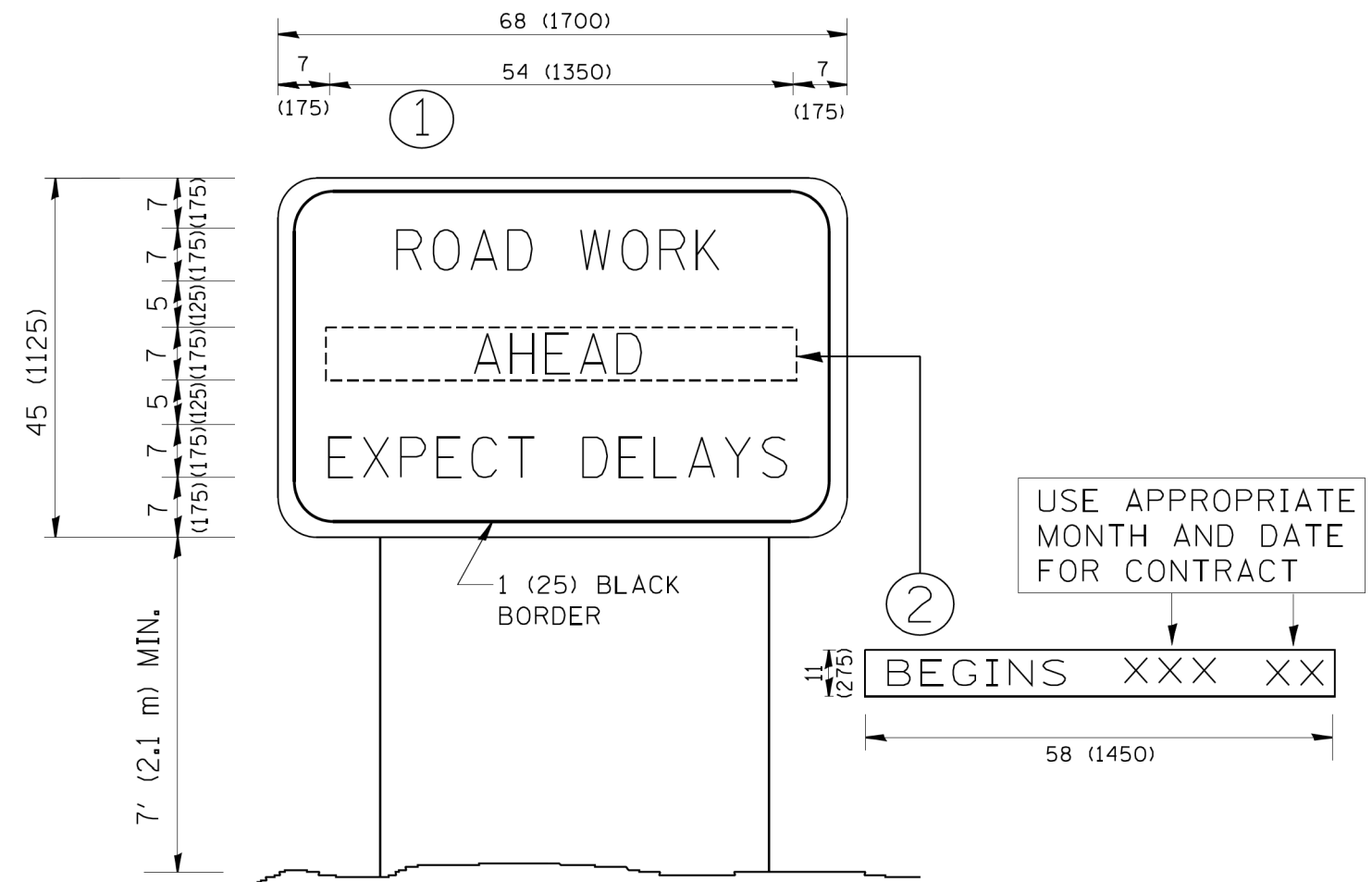


3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

**DRIVEWAY ENTRANCE SIGNING
 TC-26**



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
 UNLESS OTHERWISE SHOWN.

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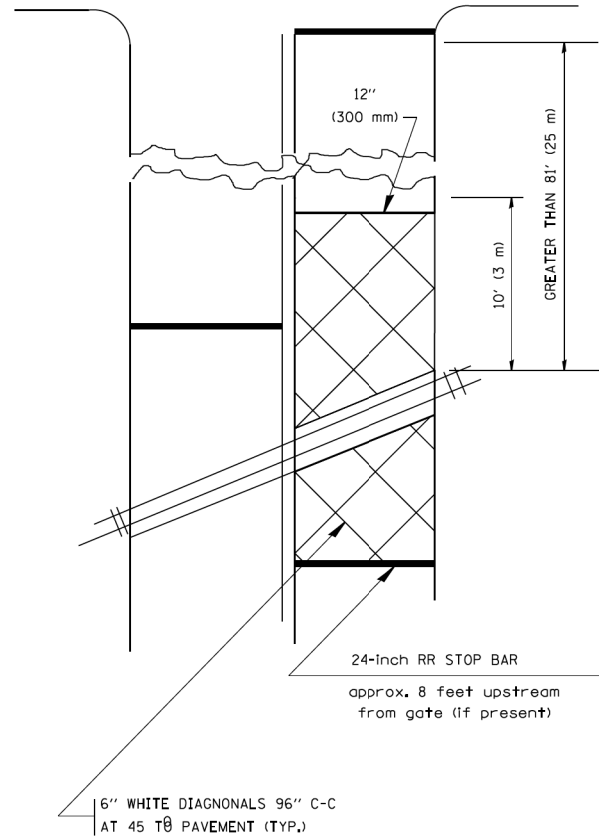
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PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN			
SCALE: NTS	SHEET 5	OF 14 SHEETS	STA. N/A TO STA. N/A

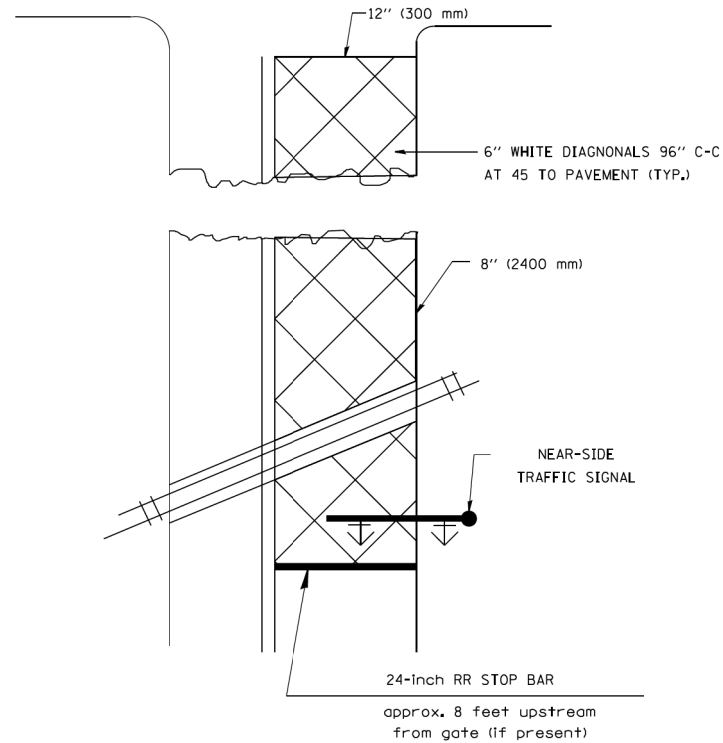
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846	DIHSRR2016-02&03	WILL	580	104
TC-22			CONTRACT NO. 62B33	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

WITH INTERSECTION TRAFFIC SIGNALS
(SEE NOTE 1)



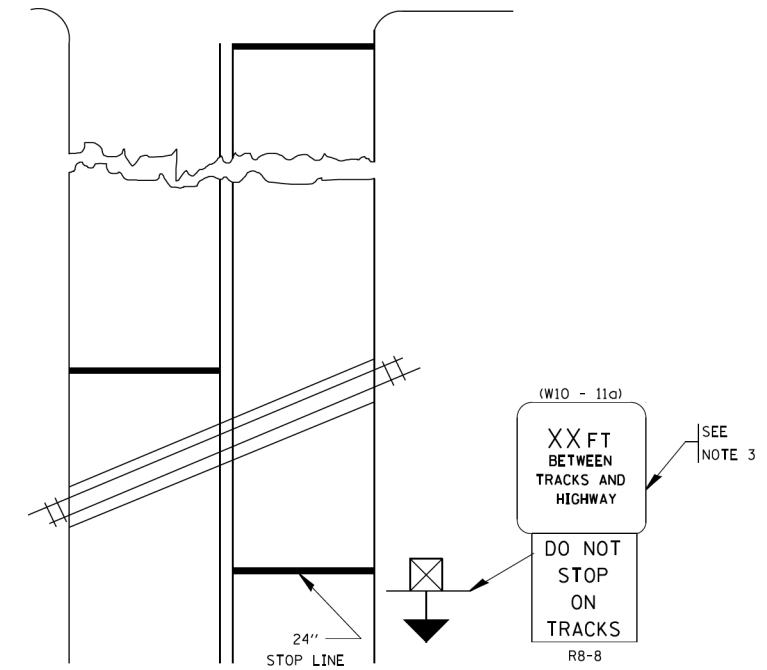
PLAN
N. T. S

WITH NEAR-SIDE TRAFFIC SIGNALS
(SEE NOTE 1 & 2)



PLAN
N. T. S

WITH NONSIGNALIZED INTERSECTION
81' (25 m) OR LESS TO CLOSEST RAIL



PLAN
N. T. S

NOTES:

- PAVEMENT MARKINGS TO BE INSTALLED ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED, THE PAVEMENT MARKINGS EXTENDS TO THE INTERSECTION.
- DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET (1.8 m) FROM THE RAIL CLOSEST TO THE INTERSECTION TO THE STOP LINE OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET (1.5 m). WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE THE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6-FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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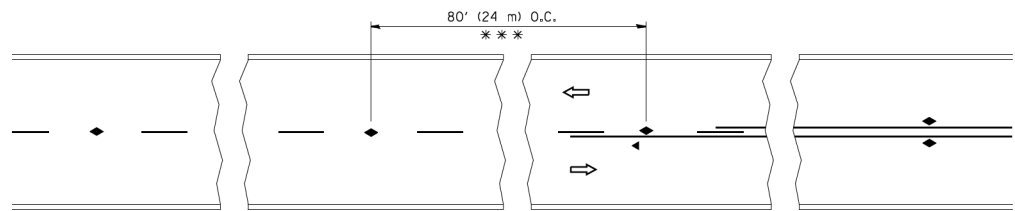
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PLOT SCALE = 40.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

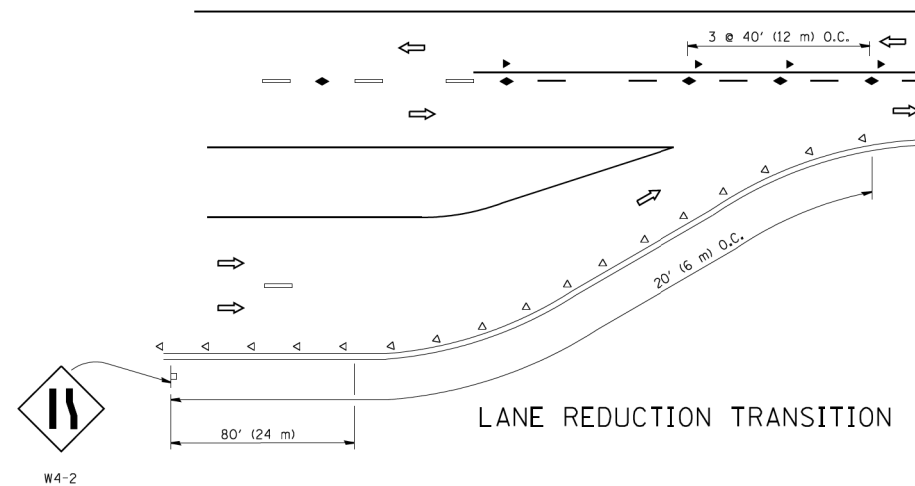
TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING
TREATMENT FOR RAILROAD CROSSINGS

SCALE: NTS SHEET 6 OF 14 SHEETS STA. N/A TO STA. N/A

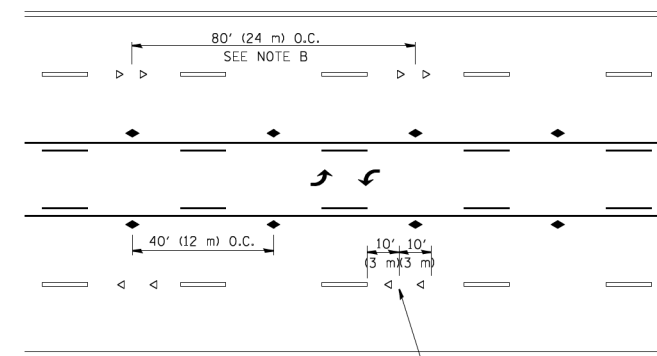
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TC-23			CONTRACT NO. 62B33	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



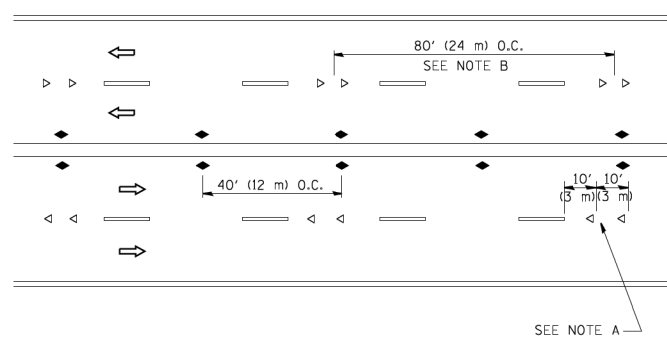
TWO-LANE/TWO-WAY



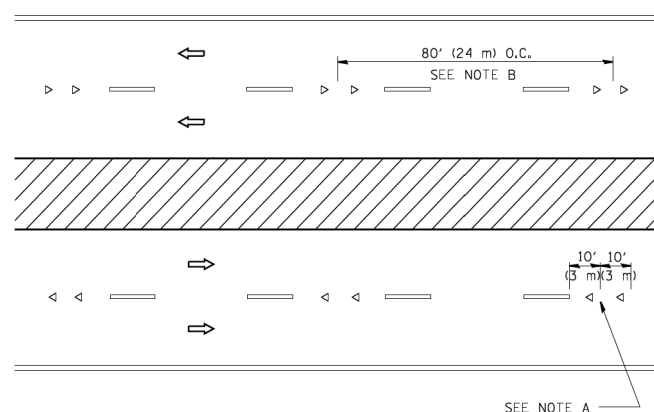
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

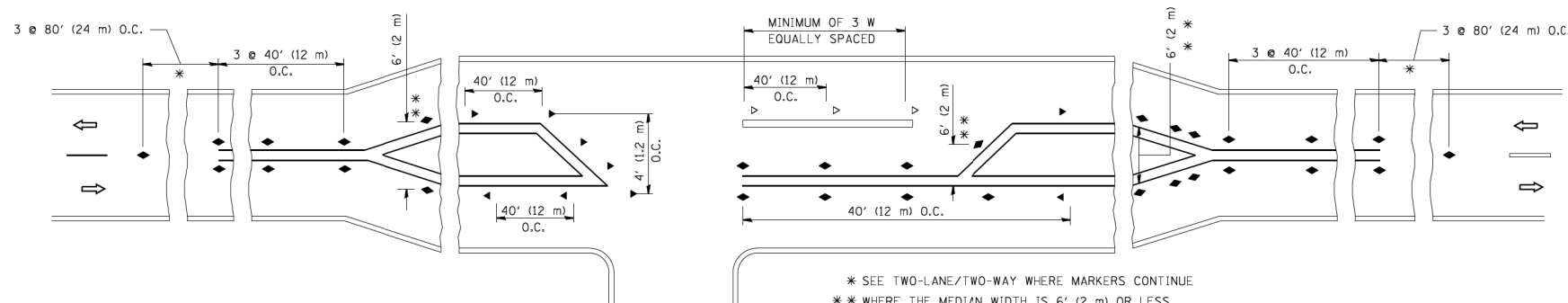
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- < ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

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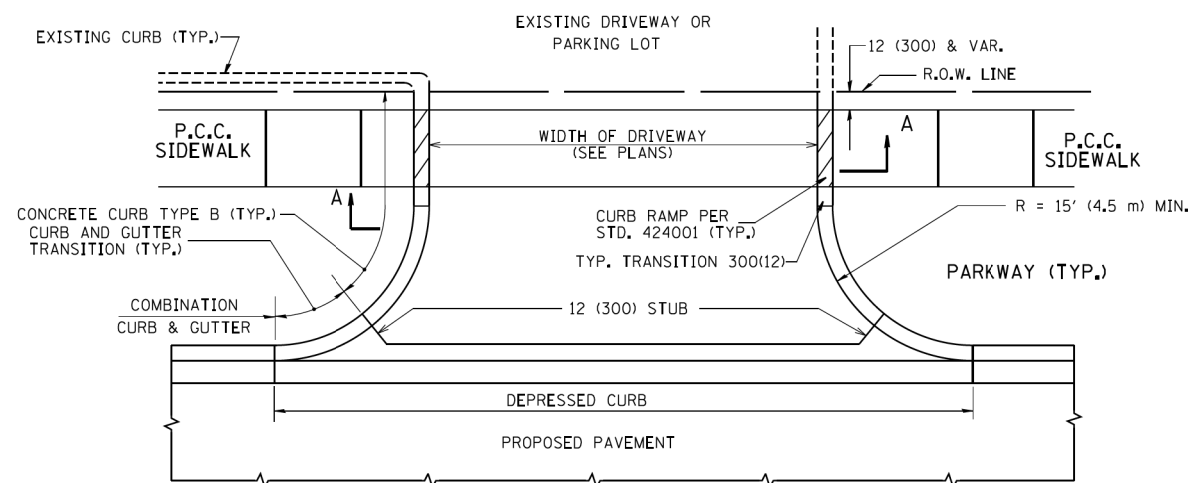


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DRAWN - BWL, EKS	REVISOR -	
PLOT SCALE = 40.0000' / in.	CHECKED - NJB	REVISOR -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISOR -

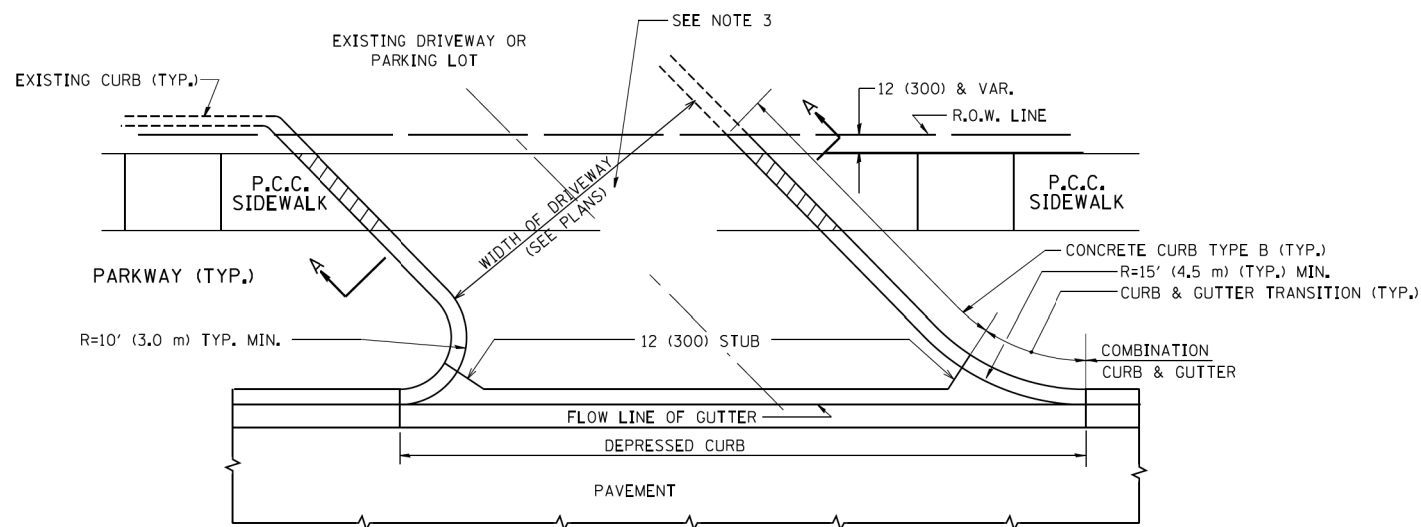
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS		
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)		
SCALE: NTS	SHEET 7 OF 14 SHEETS	STA. N/A TO STA. N/A

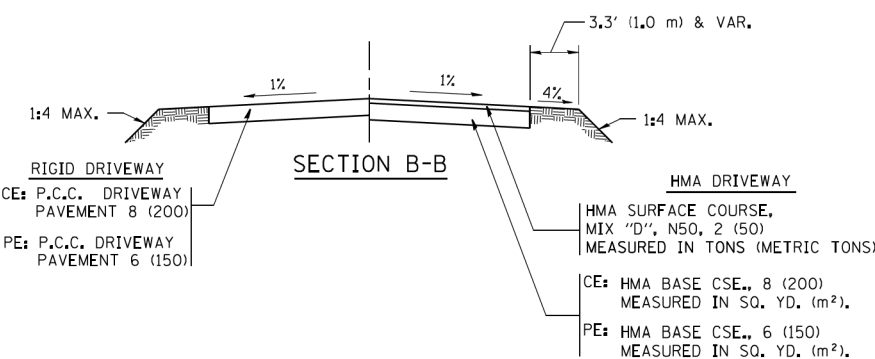
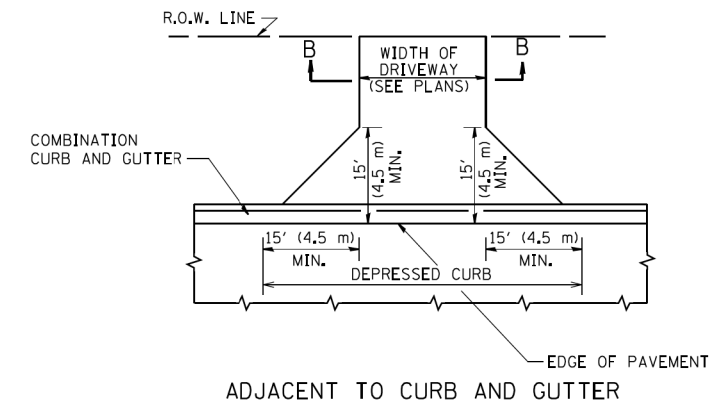
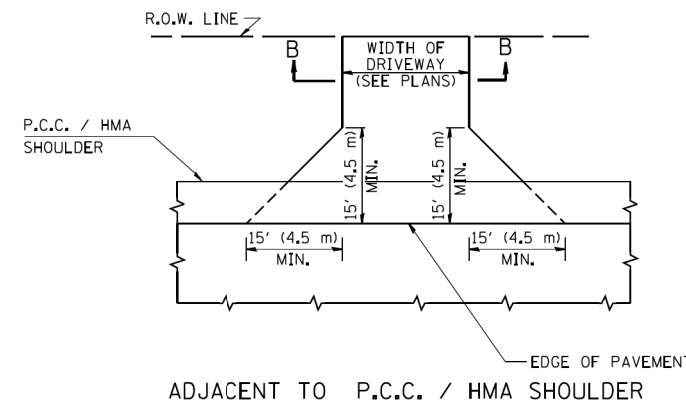
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02&03	WILL	580	106
TC-11			CONTRACT NO. 62B33	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX 'D', N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE 'HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS'. FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

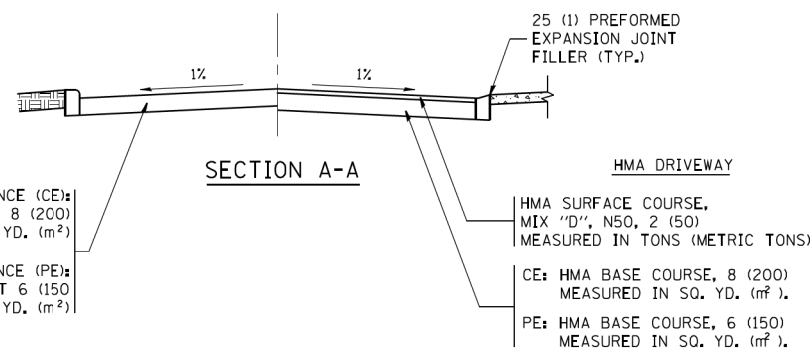
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



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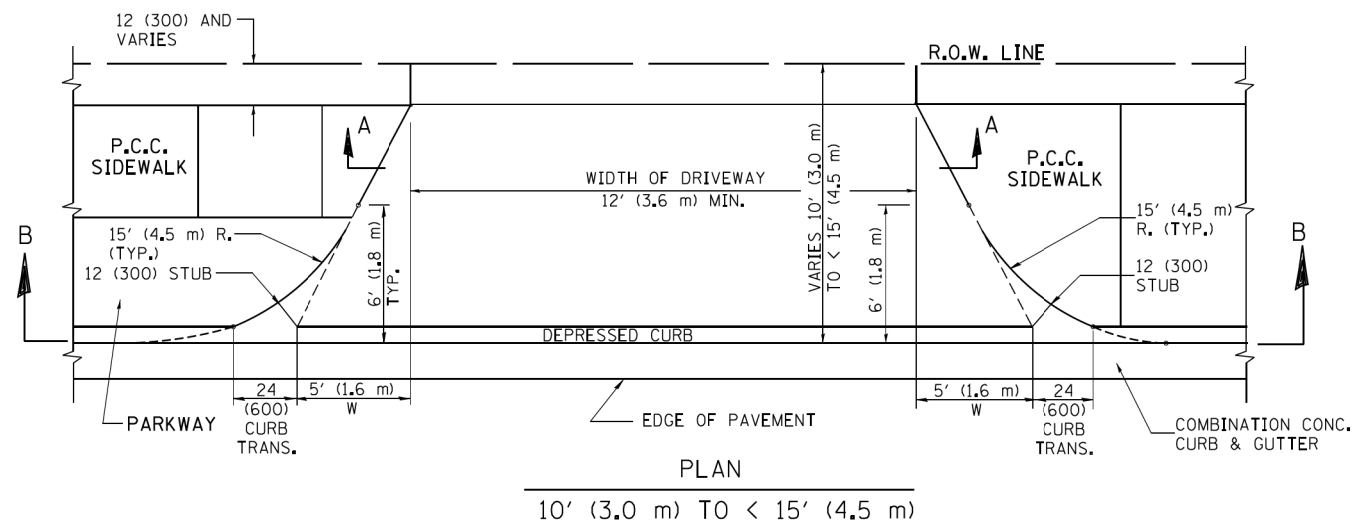


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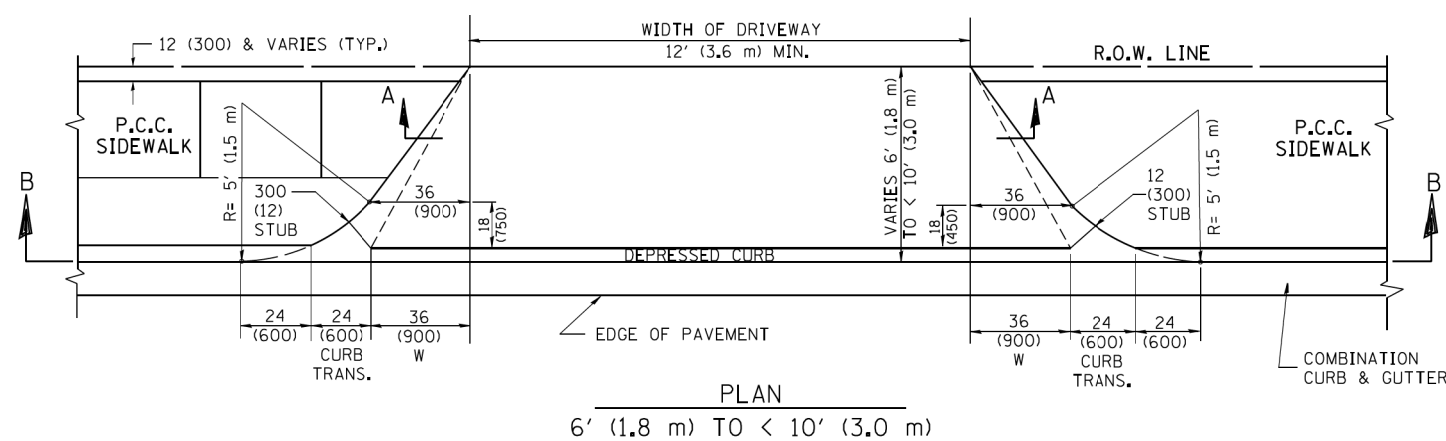
STATE OF ILLINOIS
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DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)		
SCALE: NTS	SHEET 8 OF 14 SHEETS	STA. N/A TO STA. N/A

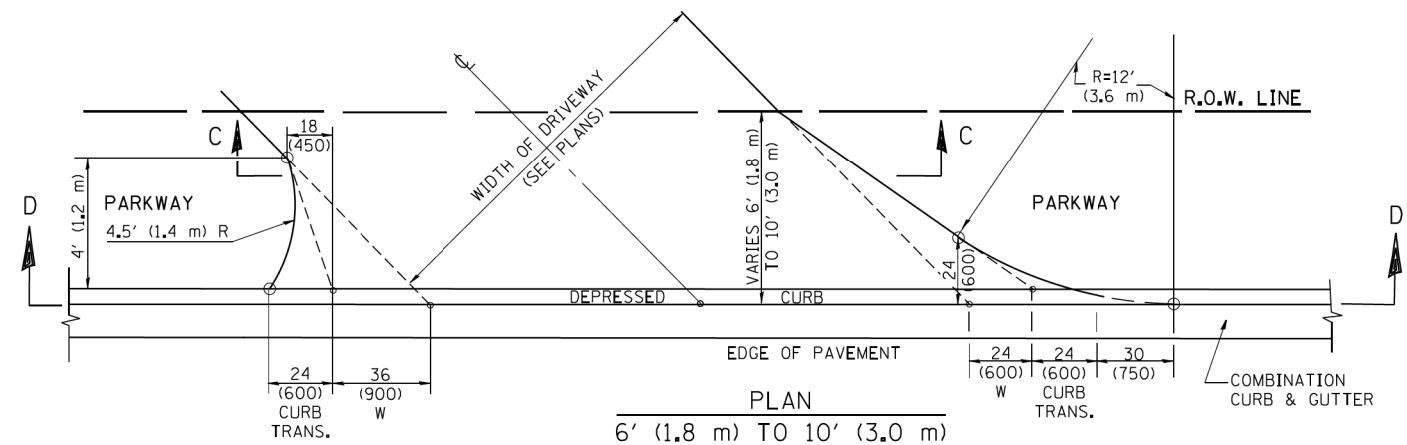
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD0156-07 (BD-01)			CONTRACT NO. 62B33	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



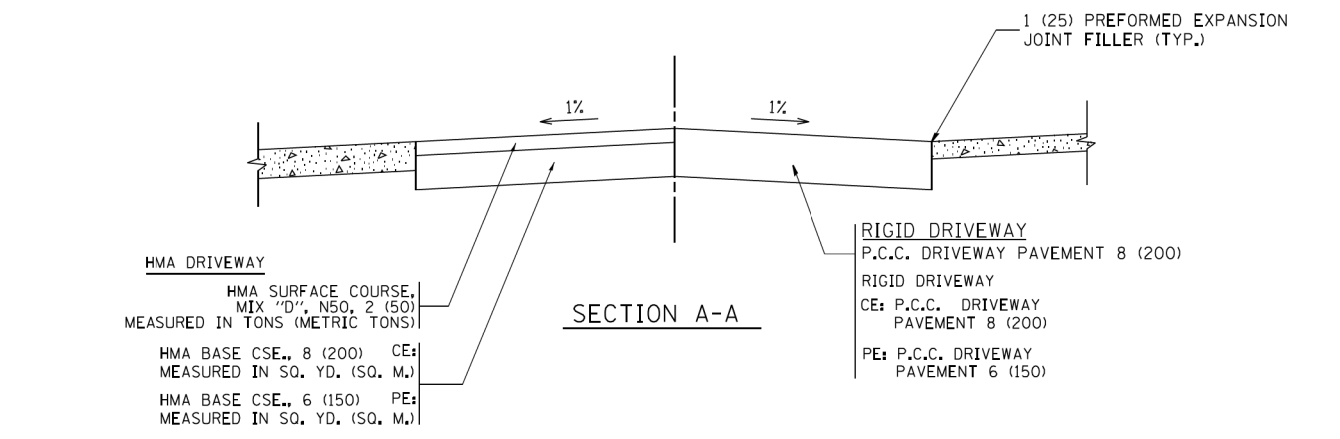
PLAN
10' (3.0 m) TO < 15' (4.5 m)



PLAN
6' (1.8 m) TO < 10' (3.0 m)

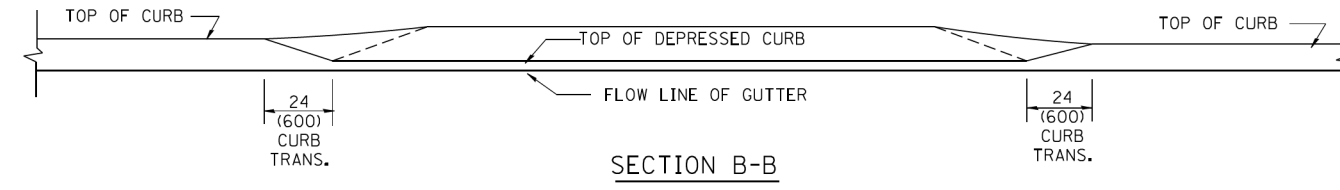


PLAN
6' (1.8 m) TO 10' (3.0 m)

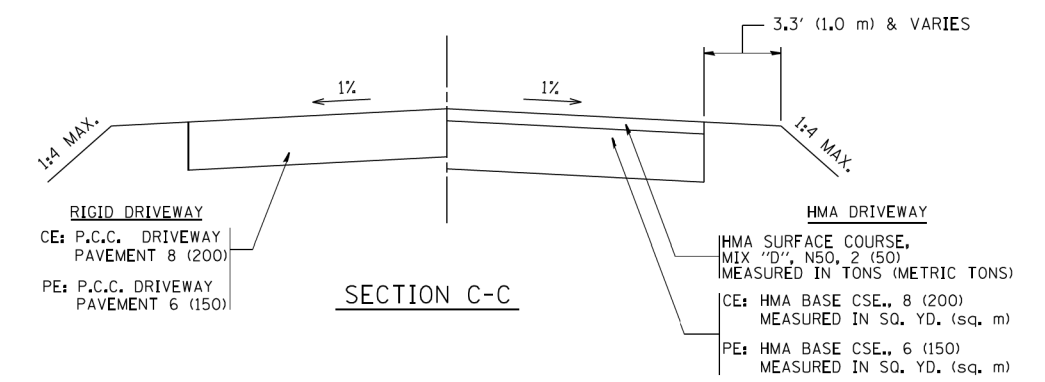


HMA DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)
HMA BASE CSE., 8 (200) MEASURED IN SQ. YD. (SQ. M.)

RIGID DRIVEWAY
P.C.C. DRIVEWAY PAVEMENT 8 (200)
RIGID DRIVEWAY CE: P.C.C. DRIVEWAY PAVEMENT 8 (200)
PE: P.C.C. DRIVEWAY PAVEMENT 6 (150)

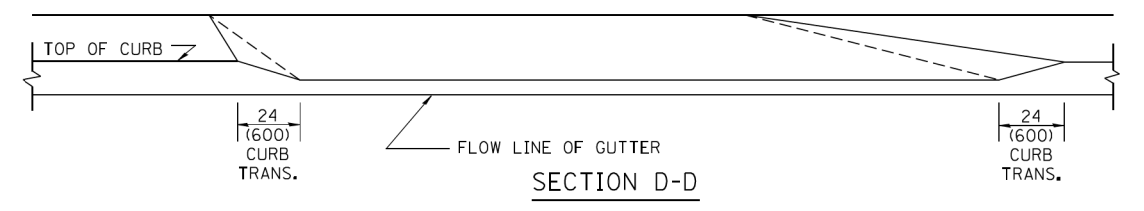


SECTION B-B



RIGID DRIVEWAY
CE: P.C.C. DRIVEWAY PAVEMENT 8 (200)
PE: P.C.C. DRIVEWAY PAVEMENT 6 (150)

HMA DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)
CE: HMA BASE CSE., 8 (200) MEASURED IN SQ. YD. (SQ. M.)
PE: HMA BASE CSE., 6 (150) MEASURED IN SQ. YD. (SQ. M.)



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

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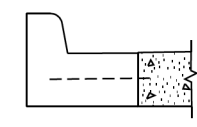
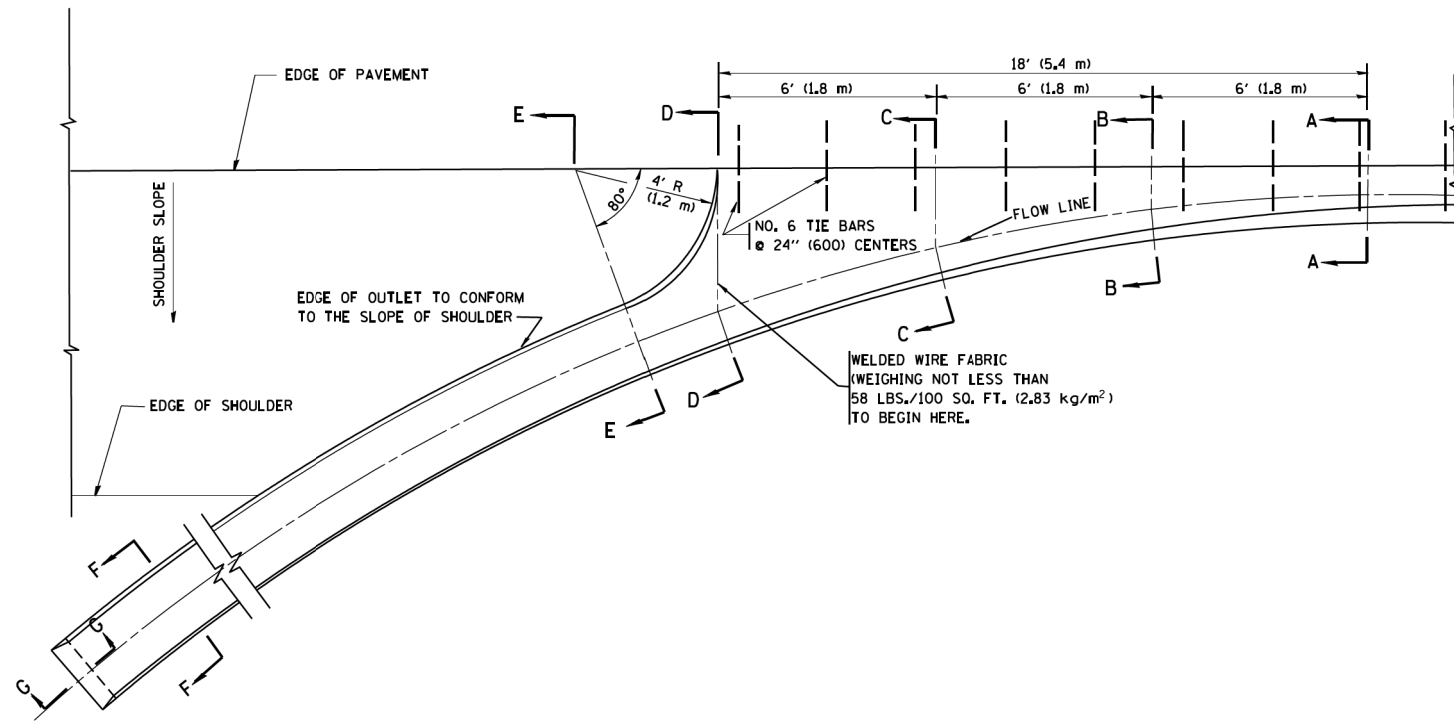


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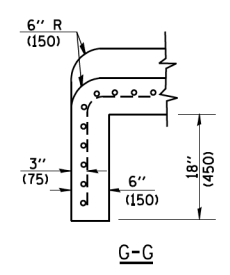
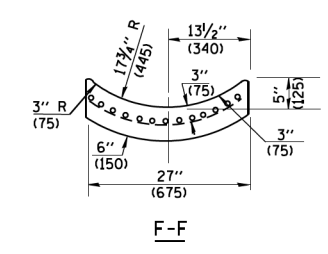
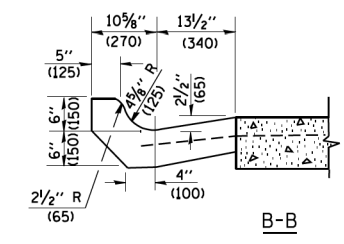
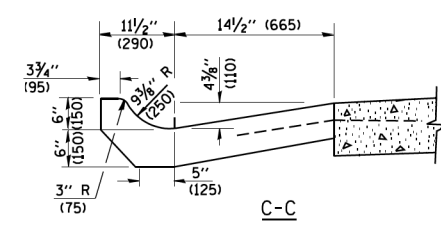
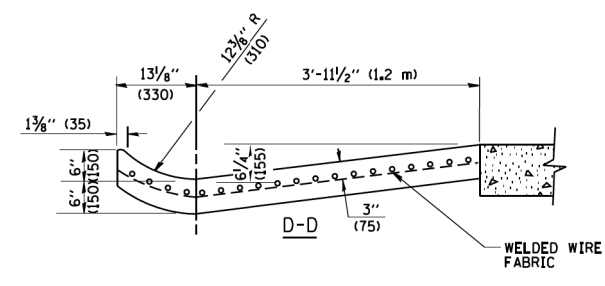
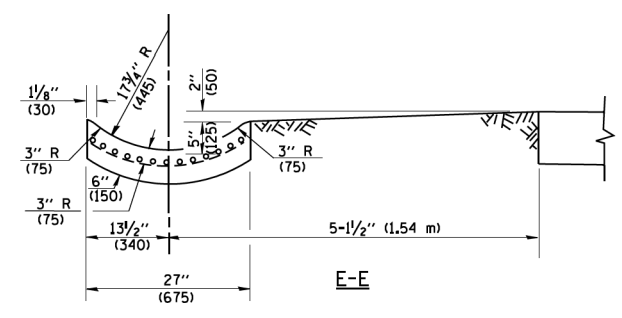
DRIVEWAY DETAILS			
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)			
SCALE: NTS	SHEET 9 OF 14 SHEETS	STA. N/A TO STA. N/A	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02&03	WILL	580	108
BD400-02 (BD-02)			CONTRACT NO. 62B33	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



A-A *

* DIMENSIONS OF THE CURB & GUTTER AT SECTION A-A ARE SHOWN ON STATE STANDARD 606001. FOR DETAILS OF OUTLET FOR CONCRETE CURB & GUTTER, TYPE B-6.24 (B-15.60) SEE STATE STANDARD 606006.



GENERAL NOTES

GUTTER OUTLET SHALL BE TIED TO THE PAVEMENT IN ACCORDANCE WITH DETAILS FOR LONGITUDINAL CONSTRUCTION JOINT SHOWN ON STANDARD 420001.

TIE BARS SHALL BE NO. 20 (NO.6) AT 24\"/>

IF THE AVERAGE GRADE OF PAVEMENT FOR THE DISTANCE FROM SECTION A-A TO D-D EXCEEDS 2%, THIS DISTANCE SHALL BE INCREASED 6' (1.8 m) FOR EACH 1% INCREASE IN GRADE.

QUANTITIES

FOR SECTION A-A TO E-E AND CURTAIN WALL =
 1.25 CU. YDS. (0.96 m³) CLASS SI CONCRETE (OUTLET) FOR 9\"/>

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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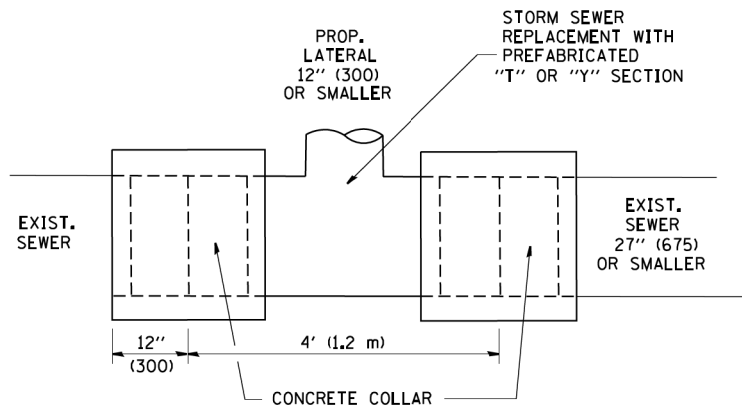
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DEPARTMENT OF TRANSPORTATION

OUTLET FOR CONCRETE
CURB AND GUTTER

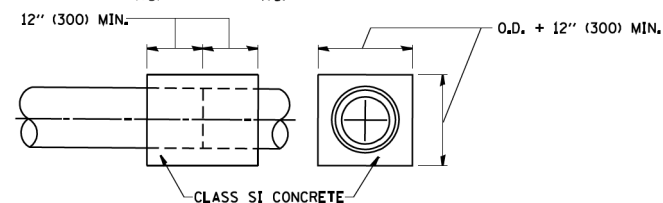
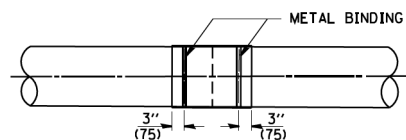
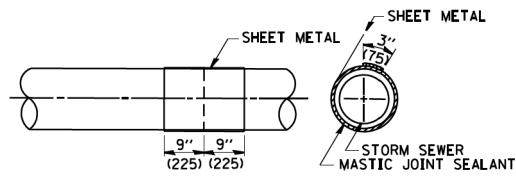
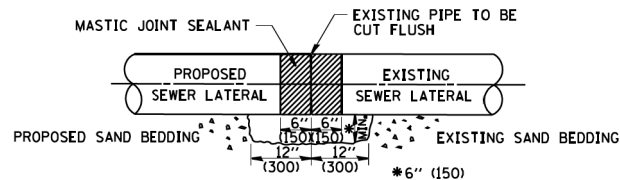
SCALE: NTS	SHEET 10	OF 14 SHEETS	STA. N/A	TO STA. N/A
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02&03	WILL	580	109
BD600-01 (BD-03)		CONTRACT NO. 62B33		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

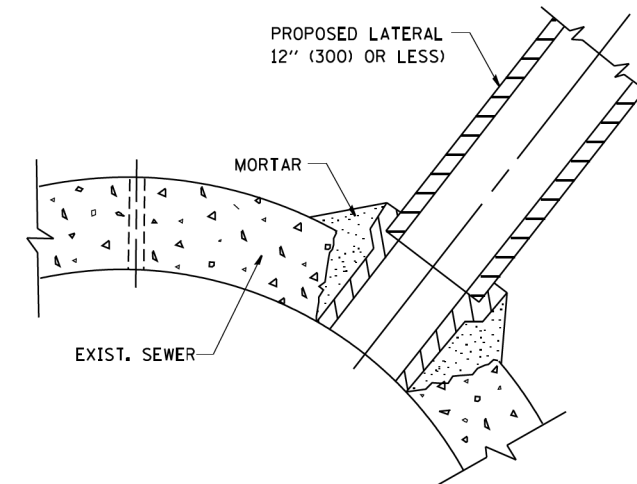


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6' (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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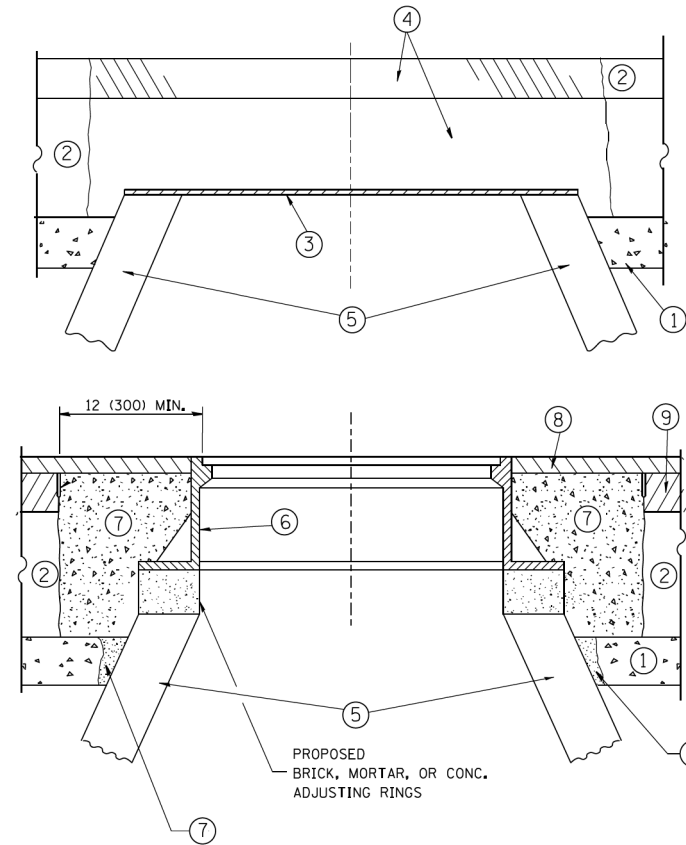
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER

SCALE: NTS SHEET 11 OF 14 SHEETS STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02&03	WILL	580	110
BD500-01 (BD-7)			CONTRACT NO. 62B33	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN



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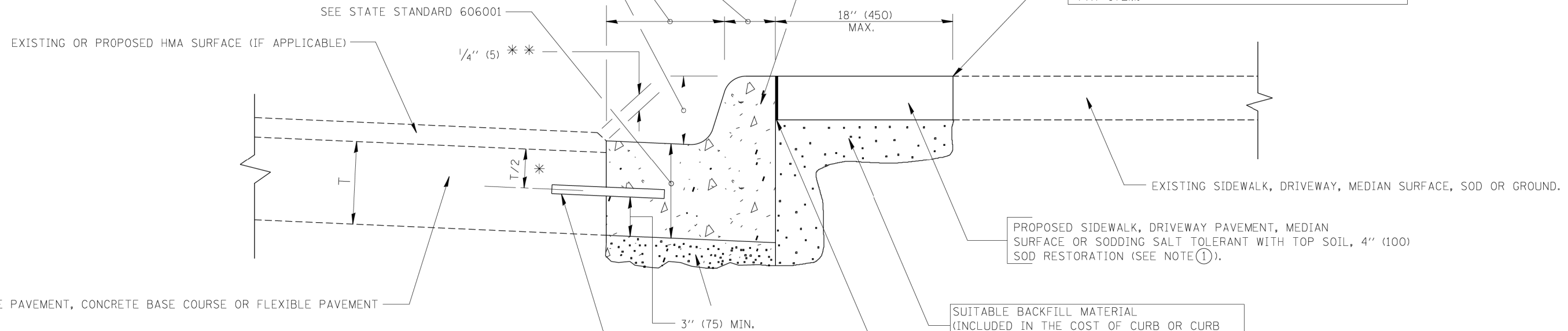
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NTS	SHEET 12	OF 14 SHEETS	STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02&03	WILL	580	111
BD600-03 (BD-8)		CONTRACT NO. 62B33		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME: c:\ywork\working\nick.becker\transportation\dm1135\0162833-ah-1-detailed.dwg

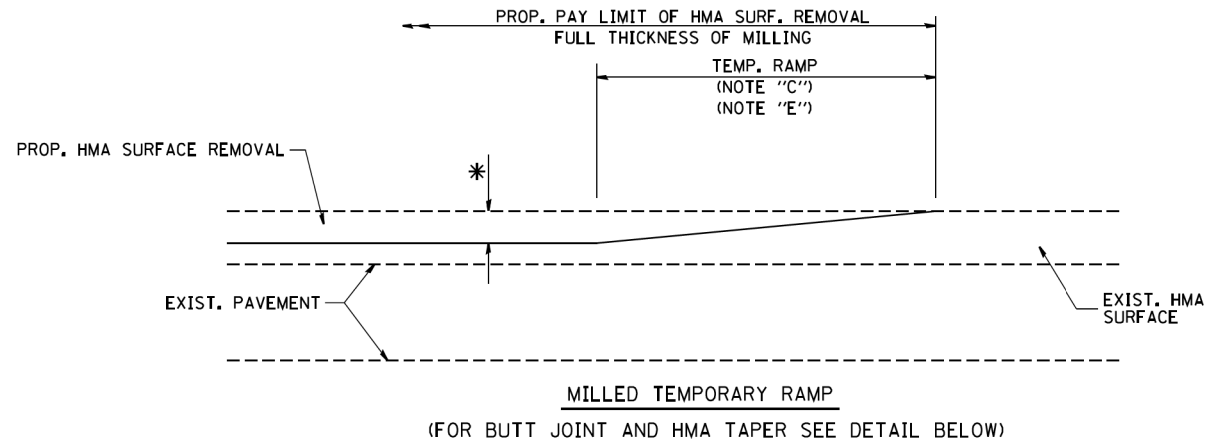


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	DRAWN - BWL, EKS	REVISED -
PLOT SCALE = 48.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

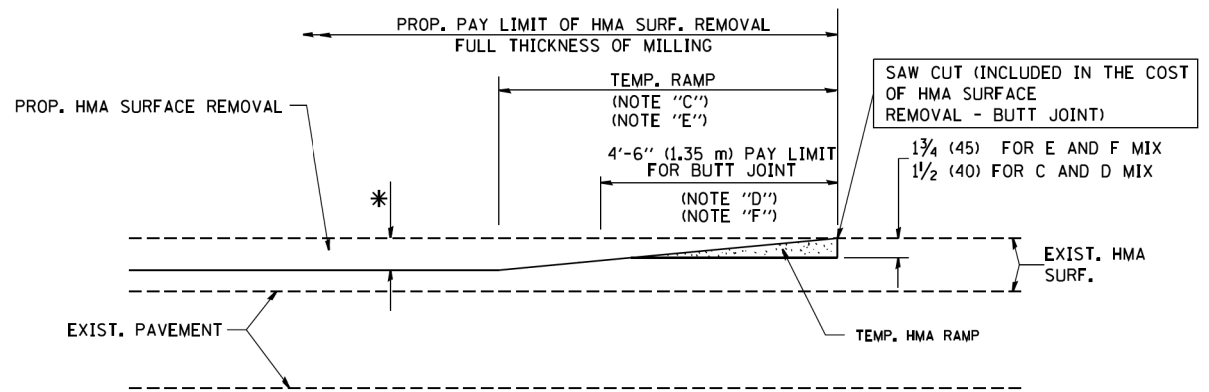
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			
SCALE: NTS	SHEET 13 OF 14 SHEETS	STA. N/A TO STA. N/A	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02&03	WILL	580	112
BD600-06 (BD-24)			CONTRACT NO. 62B33	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



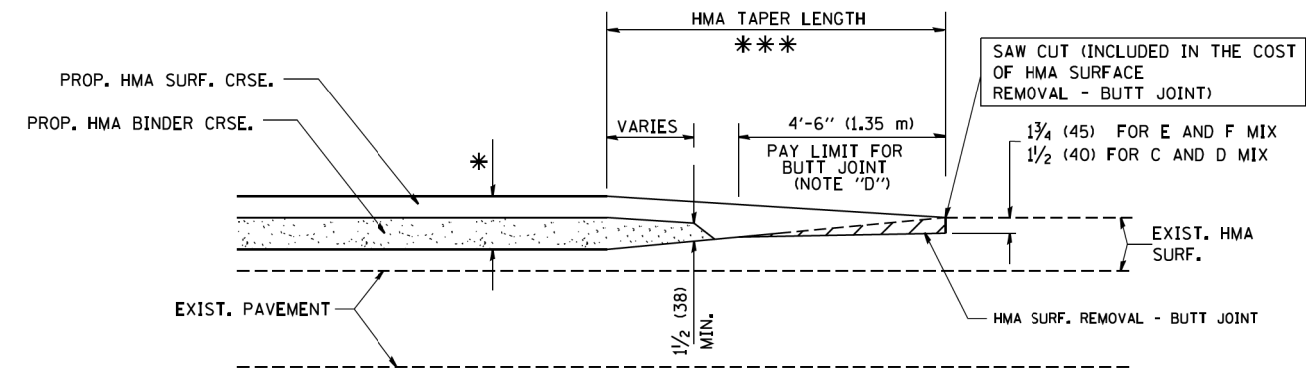
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

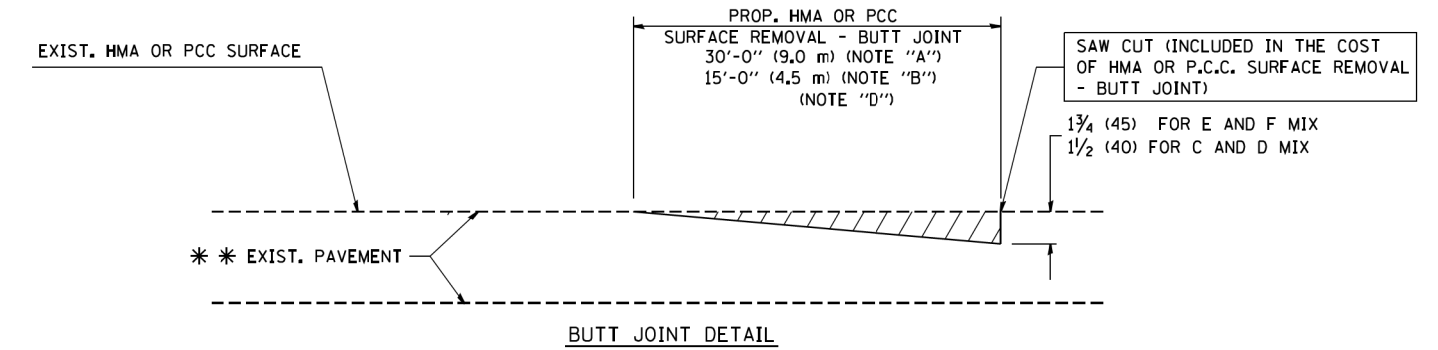


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

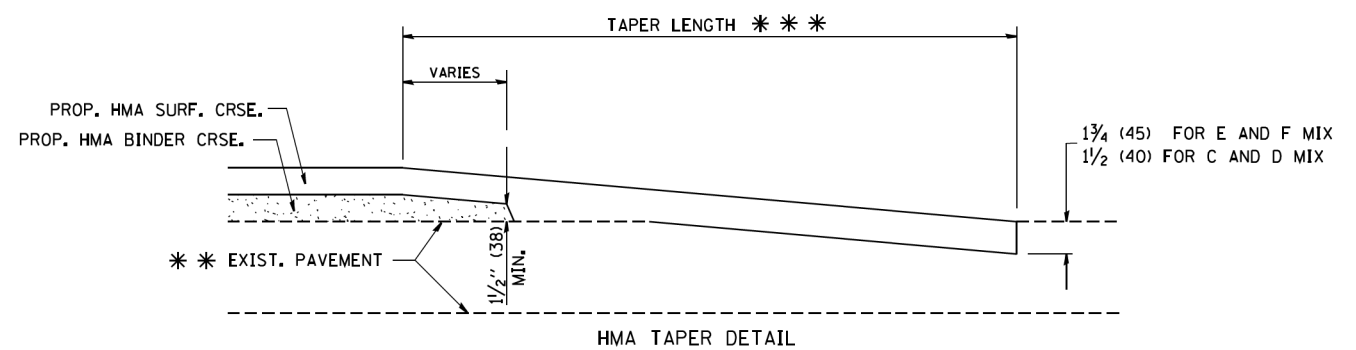
OPTION 2
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER
TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PLOT DATE = 3/23/2016	CHECKED - NJB	REVISED -
	DATE - 02/05/2016	REVISED -

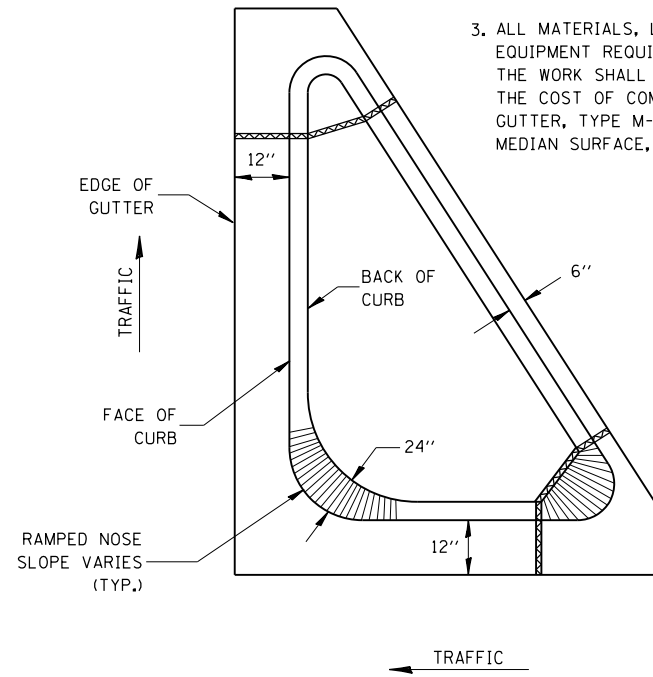
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS			
SCALE: NTS	SHEET 14	OF 14 SHEETS	STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02&03	WILL	580	113
BD400-05 BD32		CONTRACT NO. 62B33		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

GENERAL NOTES:

1. THE CURB AND GUTTER CONFIGURATION SHALL BE M-4.12.
2. SEE STD. 606001 & AND 606301 FOR ADDITIONAL DETAILS.
3. ALL MATERIALS, LABOR, AND EQUIPMENT REQUIRED TO COMPLETE THE WORK SHALL BE INCLUDED IN THE COST OF COMB. CONC. CURB AND GUTTER, TYPE M-4.12 AND CONCRETE MEDIAN SURFACE, 4 INCH.



606-1

FILE NAME = c:\ywork\hunting\nick.becker\transportation\dm1135\11628233-ah-1-details.dwg



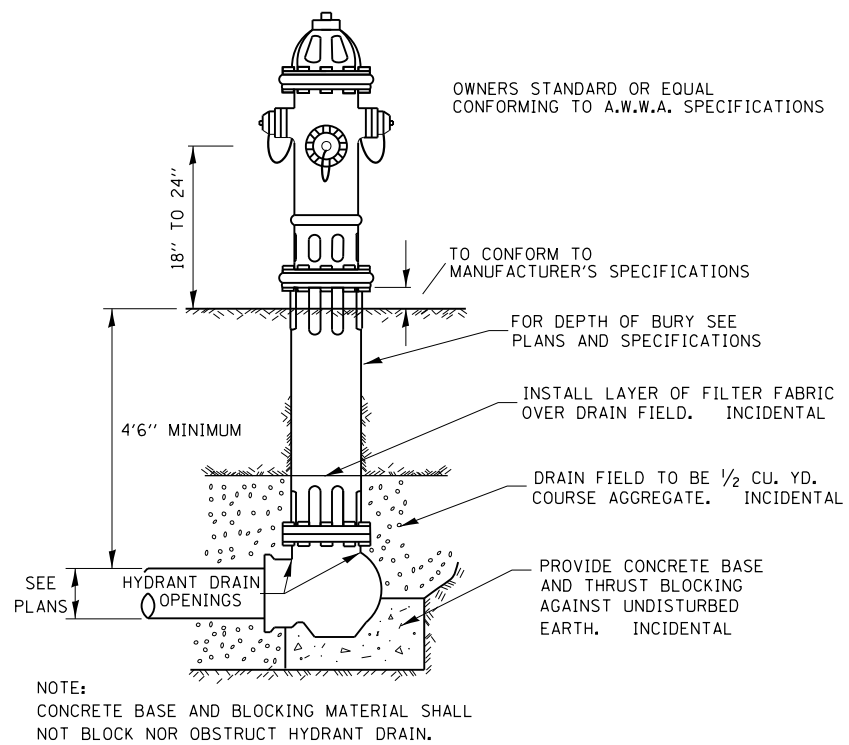
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	DRAWN - BWL, EKS	REVISED -
PLOT SCALE = 40.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

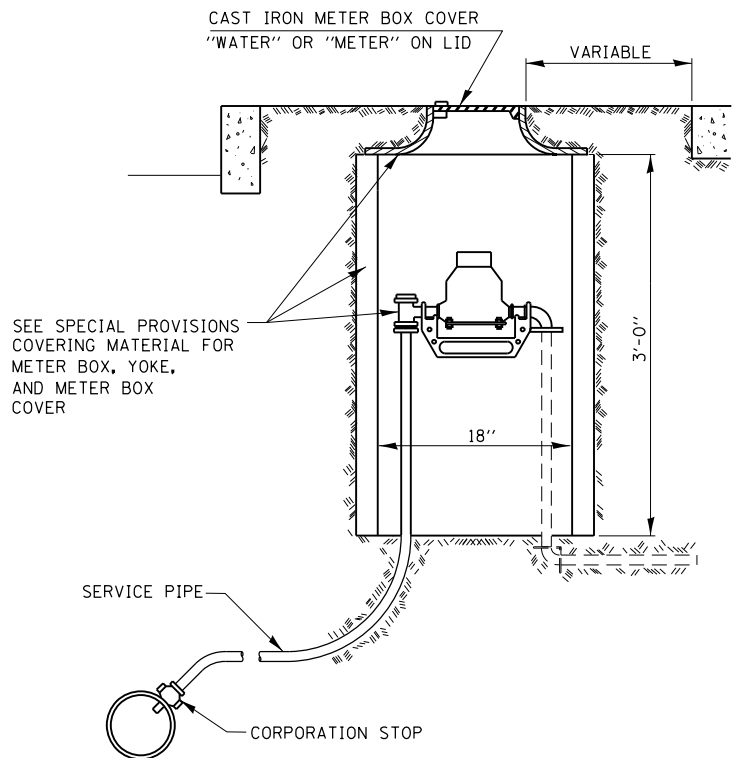
CONSTRUCTION DETAILS

SCALE: NTS SHEET 1 OF 3 SHEETS STA. N/A TO STA. N/A

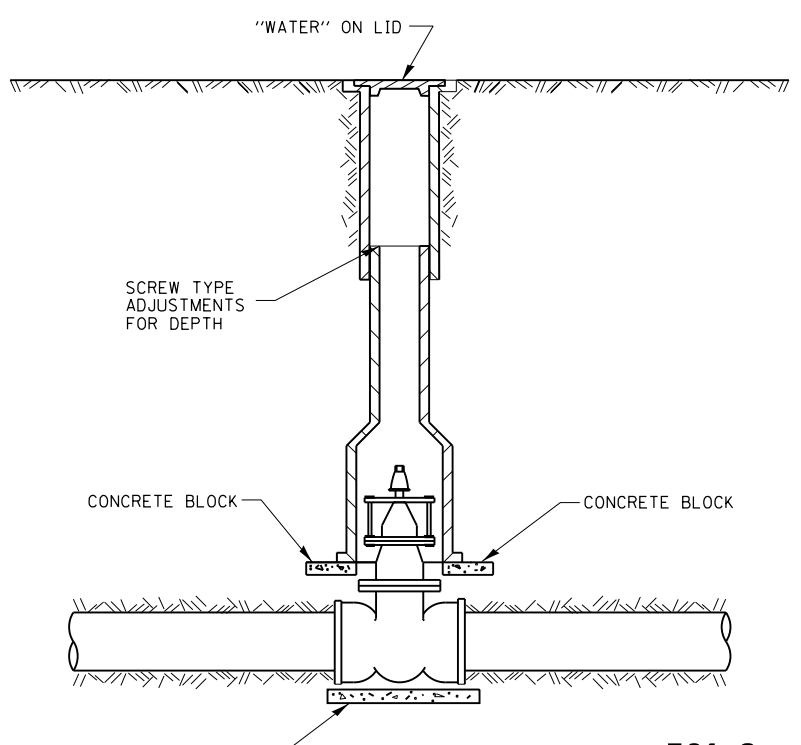
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02&03	WILL	580	114
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	



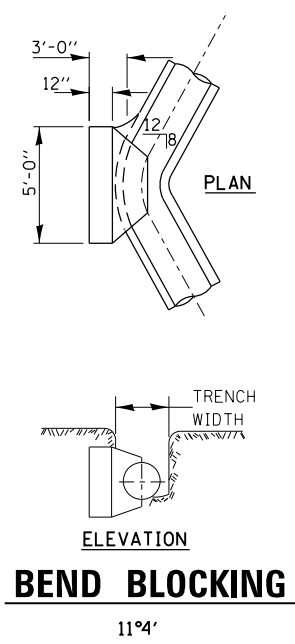
TYPICAL HYDRANT INSTALLATION 561-1



TYPICAL SINGLE METER BOX INSTALLATION 561-2

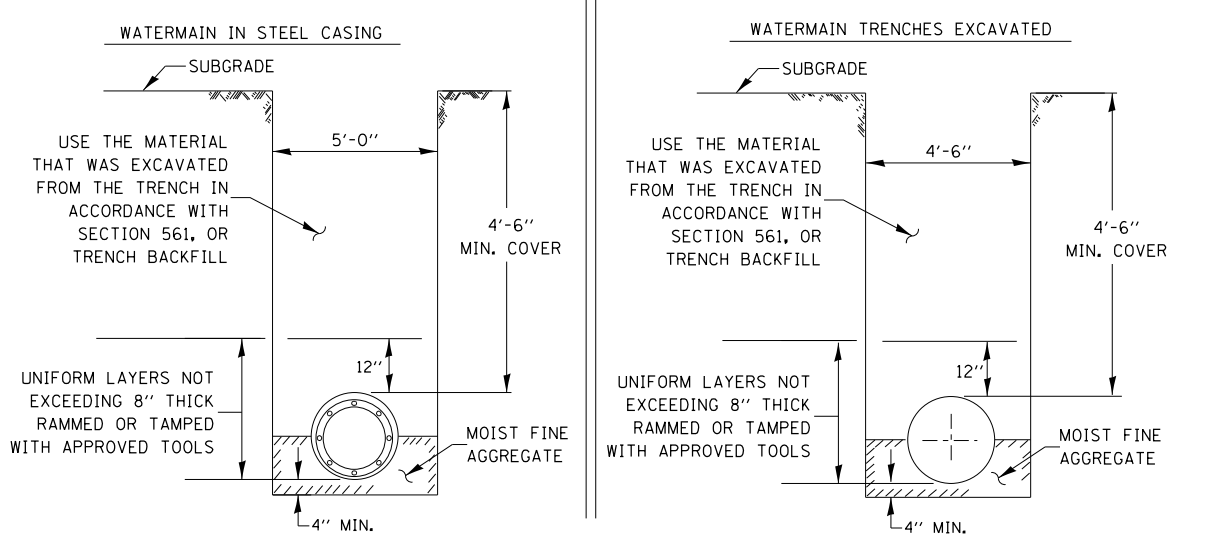


TYPICAL VALVE BOX INSTALLATION 561-3



561-6

WATERMAIN INSTALLATION REQUIREMENTS



GENERAL NOTES:

1. ANY SOFT OR SPONGY MATERIAL ENCOUNTERED BELOW THE ELEVATION OF THE PIPE SHALL BE REMOVED AND REPLACED WITH WELL COMPACTED MOIST FINE AGGREGATE.
2. ANY ROCK ENCOUNTERED IN THE TRENCH SHALL BE REMOVED TO A DEPTH OF AT LEAST 8 INCHES BELOW THE PIPE GRADE AND REPLACED WITH WELL COMPACTED MOIST FINE AGGREGATE.
3. THE SIDES OF THE TRENCH MAY BE SLOPED OR BENCHED ABOVE A 5 FT. TRENCH DEPTH OR ABOVE THE ELEVATION OR THE TOP OF PIPE, WHICHEVER IS GREATER, IN LIEU OF COMPLETE SHORING OR SHEETING OF THE FULL TRENCH DEPTH.

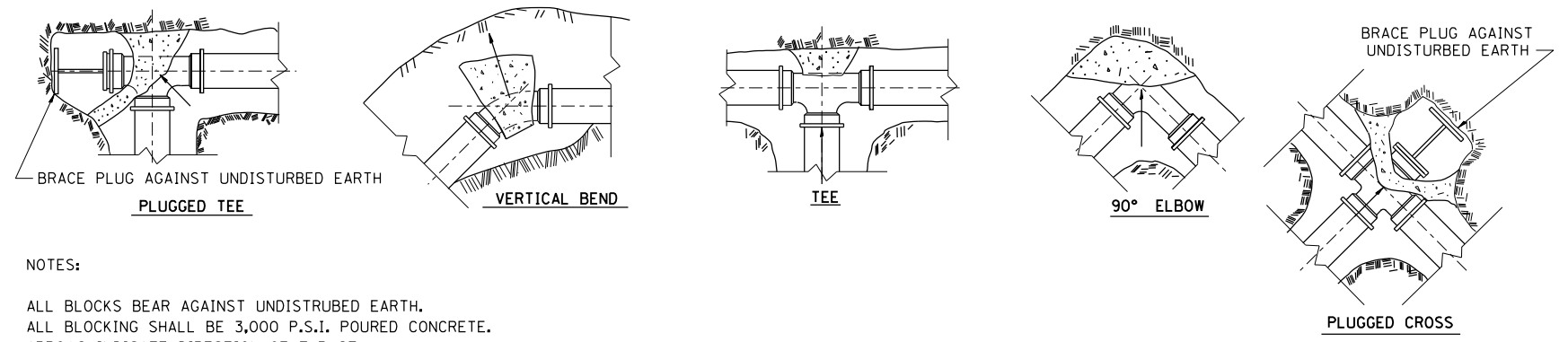
BACKFILL OPTIONS:

METHOD 1: UNIFORM LAYERS NOT EXCEEDING 12" THICK RAMMED OR TAMPED WITH APPROVED TOOLS

METHOD 2: UNIFORM LAYERS NOT EXCEEDING 12" THICK INUNDATED OR DEPOSITED IN WATER

METHOD 3: FILL TRENCH WITH LOOSE MATERIAL THEN JET WITH WATER, 6 FT. MINIMUM SPACING OF HOLES

561-4



NOTES:

ALL BLOCKS BEAR AGAINST UNDISTURBED EARTH.

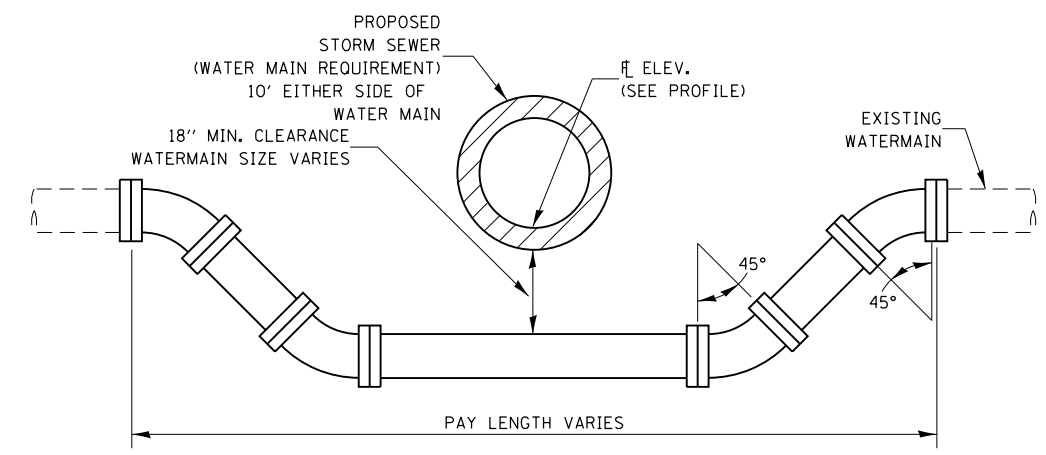
ALL BLOCKING SHALL BE 3,000 P.S.I. POURED CONCRETE.

ARROWS INDICATE DIRECTION OF THRUST.

ALL FITTINGS SHOWN IN PLAN EXCEPT VERTICAL BEND.

TYPICAL THRUST BLOCK INSTALLATIONS

561-5



ADJUSTING WATERMAIN DETAIL 561-7

FILE NAME: c:\pwworking\nick.becker\transportation\dm11335\0162833-ah-1.dwg



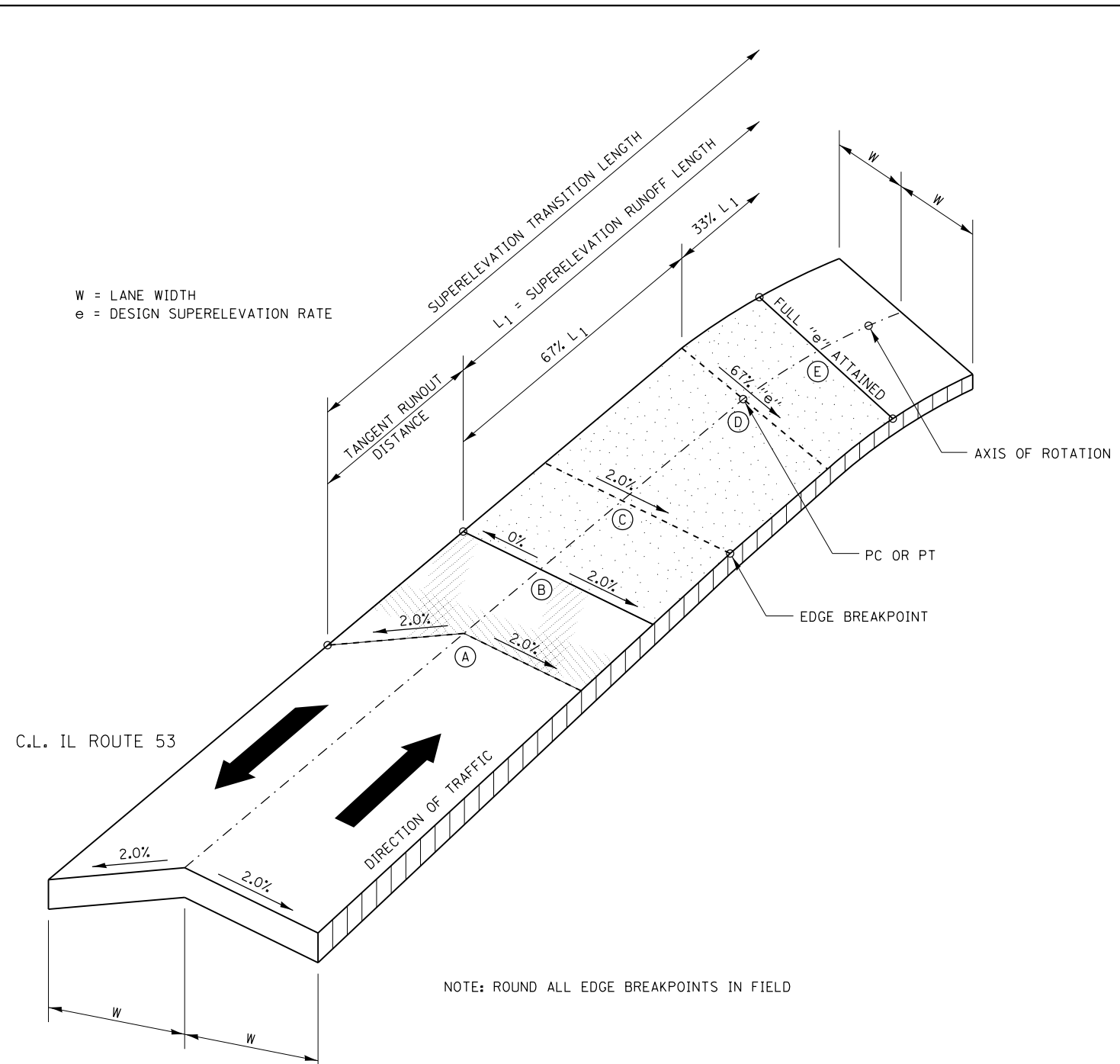
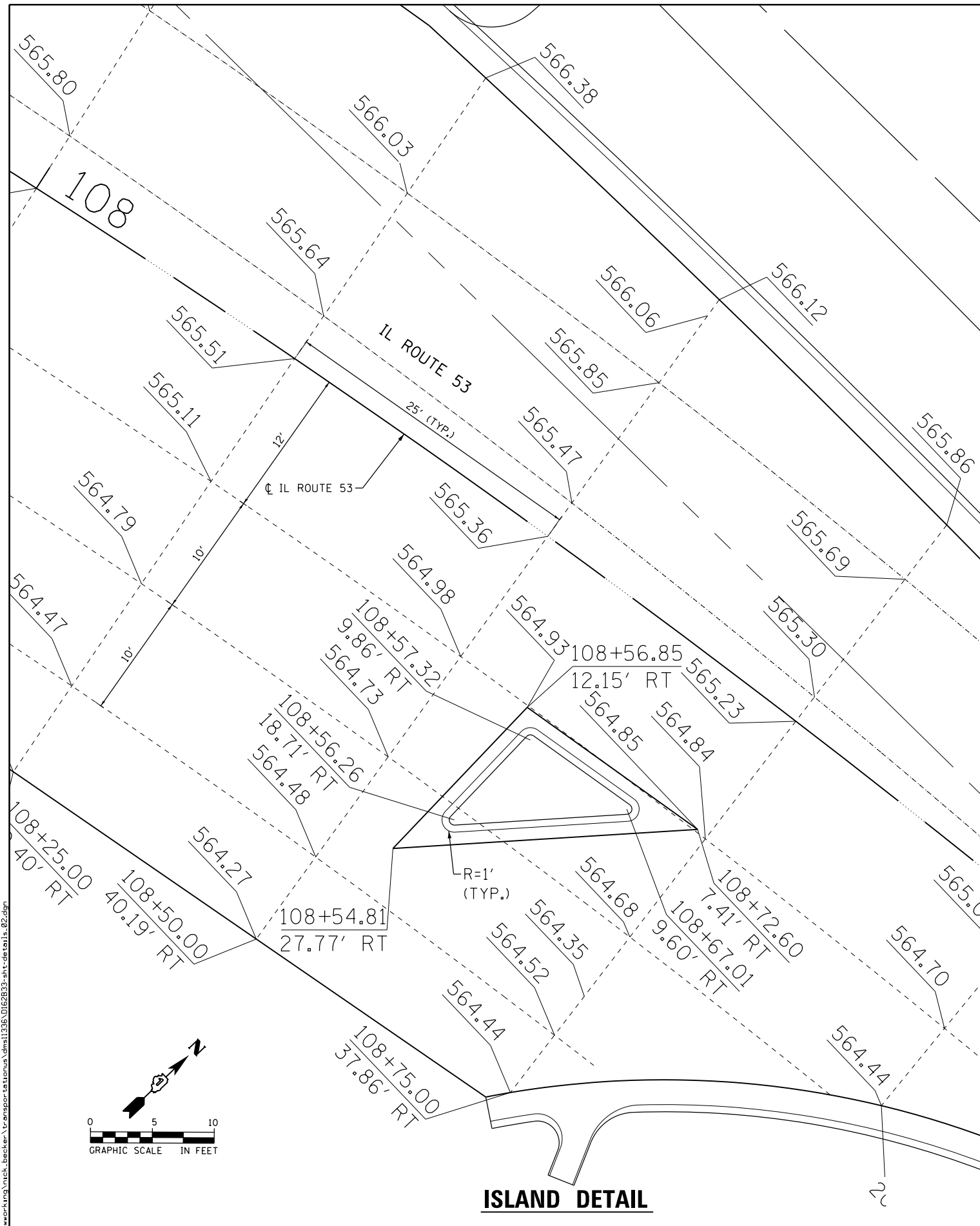
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PLOT SCALE = 40.0000' / in.	DRAWN - BWL, EKS	REVISED -
PLOT DATE = 3/23/2016	CHECKED - NJB	REVISED -
	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

SCALE: NTS SHEET 2 OF 3 SHEETS STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02&03	WILL	580	115
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	



NOTE: ROUND ALL EDGE BREAKPOINTS IN FIELD

TRANSITION CURVE TABLE

CURVE PI STA.	SUPERELEVATION "e"	W	SUPERELEVATION TRANSITION LENGTH	TANGENT RUNOUT DISTANCE	SUPERELEVATION RUNOUT LENGTH
106+30.13	3.2%	12.00'	101'	39.00'	62.00'

SUPERELEVATION TRANSITION ON TWO-LANE HIGHWAY



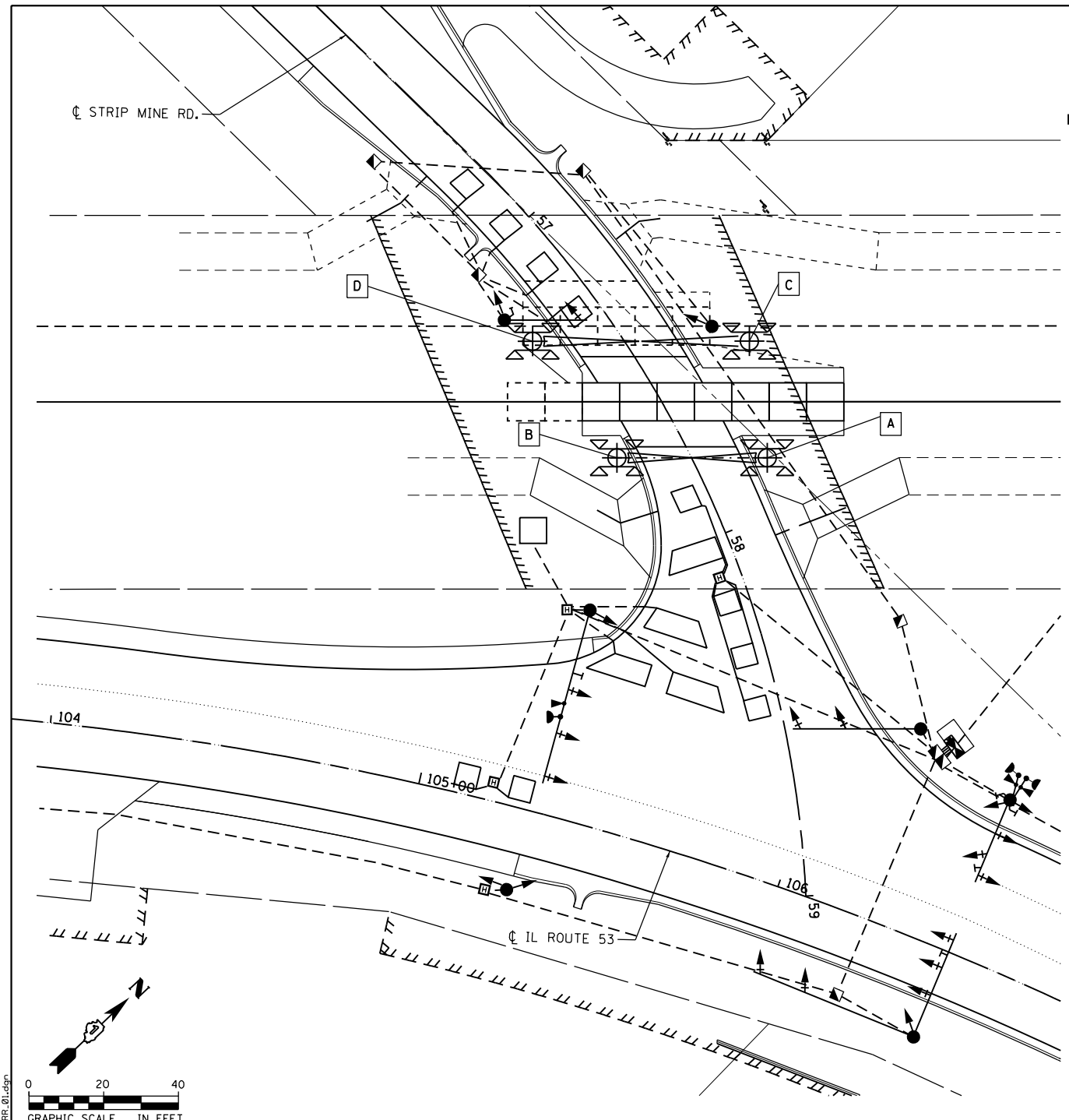
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PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

SCALE: 1" = 10' SHEET 3 OF 3 SHEETS STA. N/A TO STA. N/A

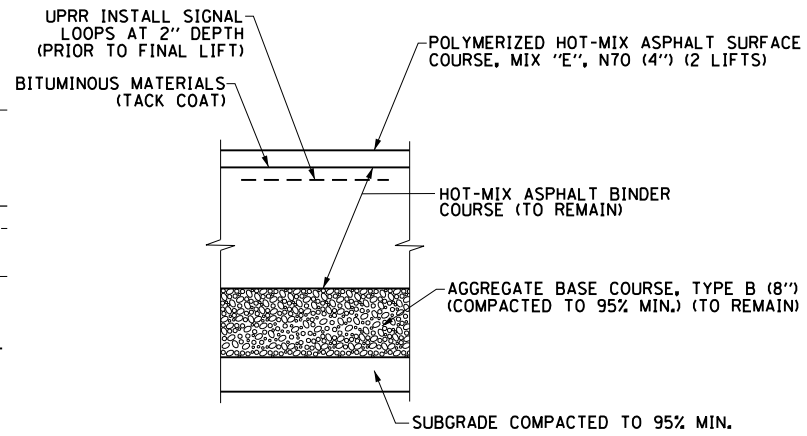
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CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	



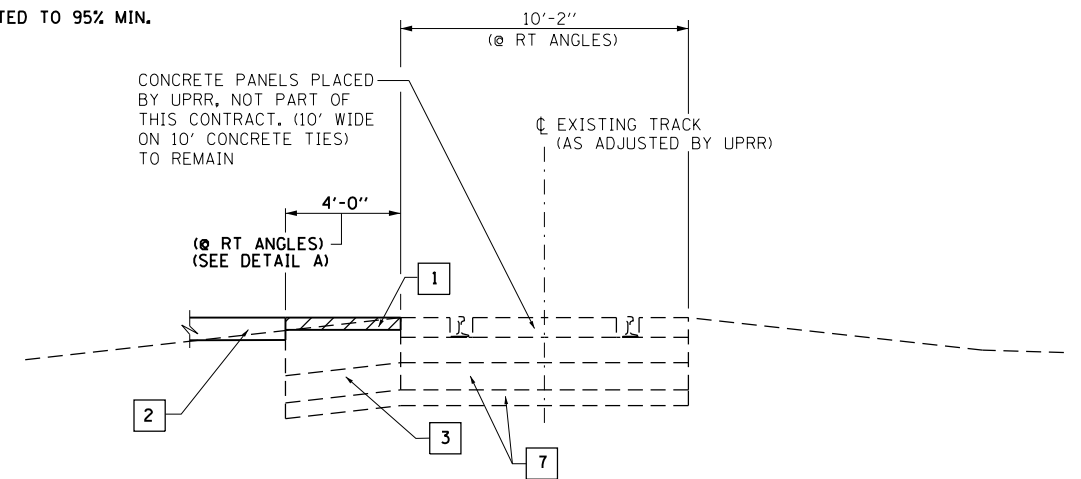
GRADE CROSSING DETAIL

*POST CONSTRUCTION FOR CRACK CONTROL

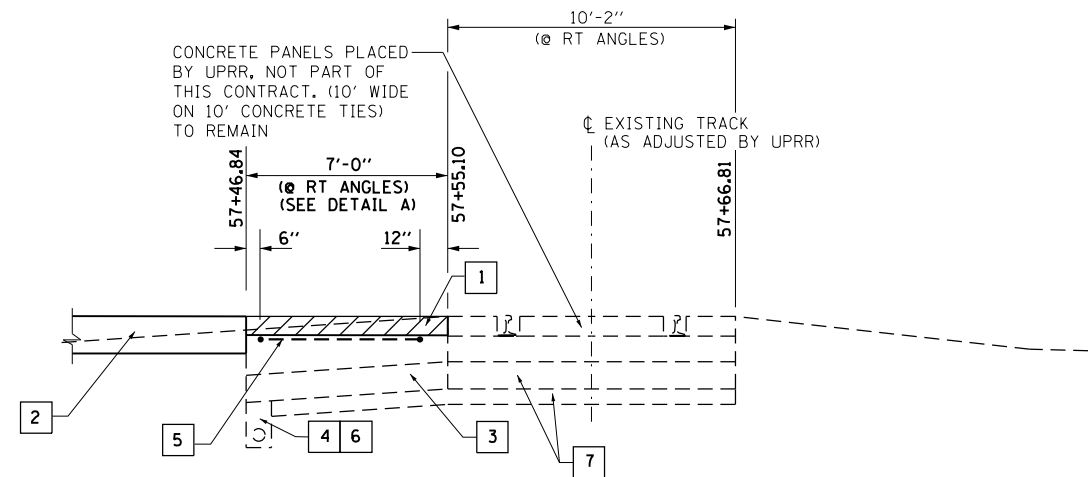
- A STA. 57+86.45, 18.25' LT. (N 1689421.7821, E 1031843.7168)
- B STA. 57+68.48, 17.75' RT. (N 1689392.6163, E 1031816.0131)
- C STA. 57+58.76, 28.43' LT. (N 1689439.8040, E 1031817.8126)
- D STA. 57+27.40, 20.16' RT. (N 1689397.7482, E 1031777.8666)



DETAIL A
TYPICAL PAVEMENT STRUCTURE
AT RAIL CROSSING
NTS



SHOULDER AT RAIL CROSSING DETAIL



PAVEMENT AT RAIL CROSSING DETAIL

- 1 CONTRACTOR TO MILL PAVEMENT TO 4" BELOW PROPOSED ROADWAY PGL. PLACE 4" HMA SURFACE COURSE.
- 2 SEE PAVEMENT SECTION SHOWN ON ROADWAY TYPICAL SECTION SHEET.
- 3 AGGREGATE BASE COURSE, TYPE B. (8") (TO REMAIN)
- 4 POROUS GRANULAR BACKFILL WITH PIPE UNDERDRAIN 6". (TO REMAIN)
- 5 CONTRACTOR TO COORDINATE WITH UPRR SIGNAL REGARDING PLACEMENT OF DETECTOR LOOPS.
- 6 GEOTECHNICAL FABRIC AND UNDERDRAIN. (TO REMAIN)
- 7 BALLAST AND SUBBALLAST. (TO REMAIN)



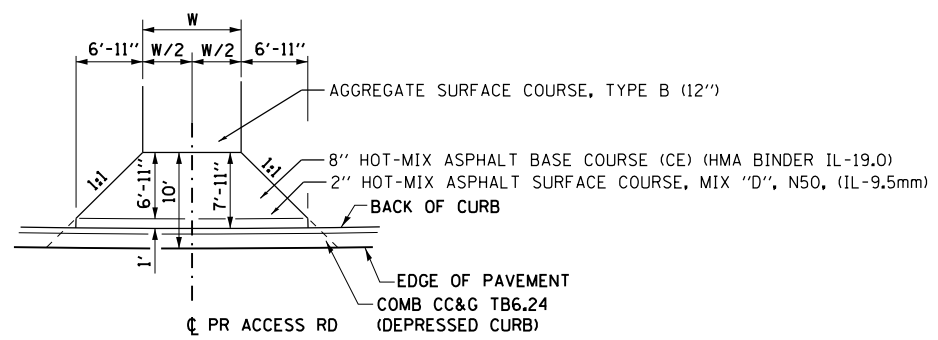
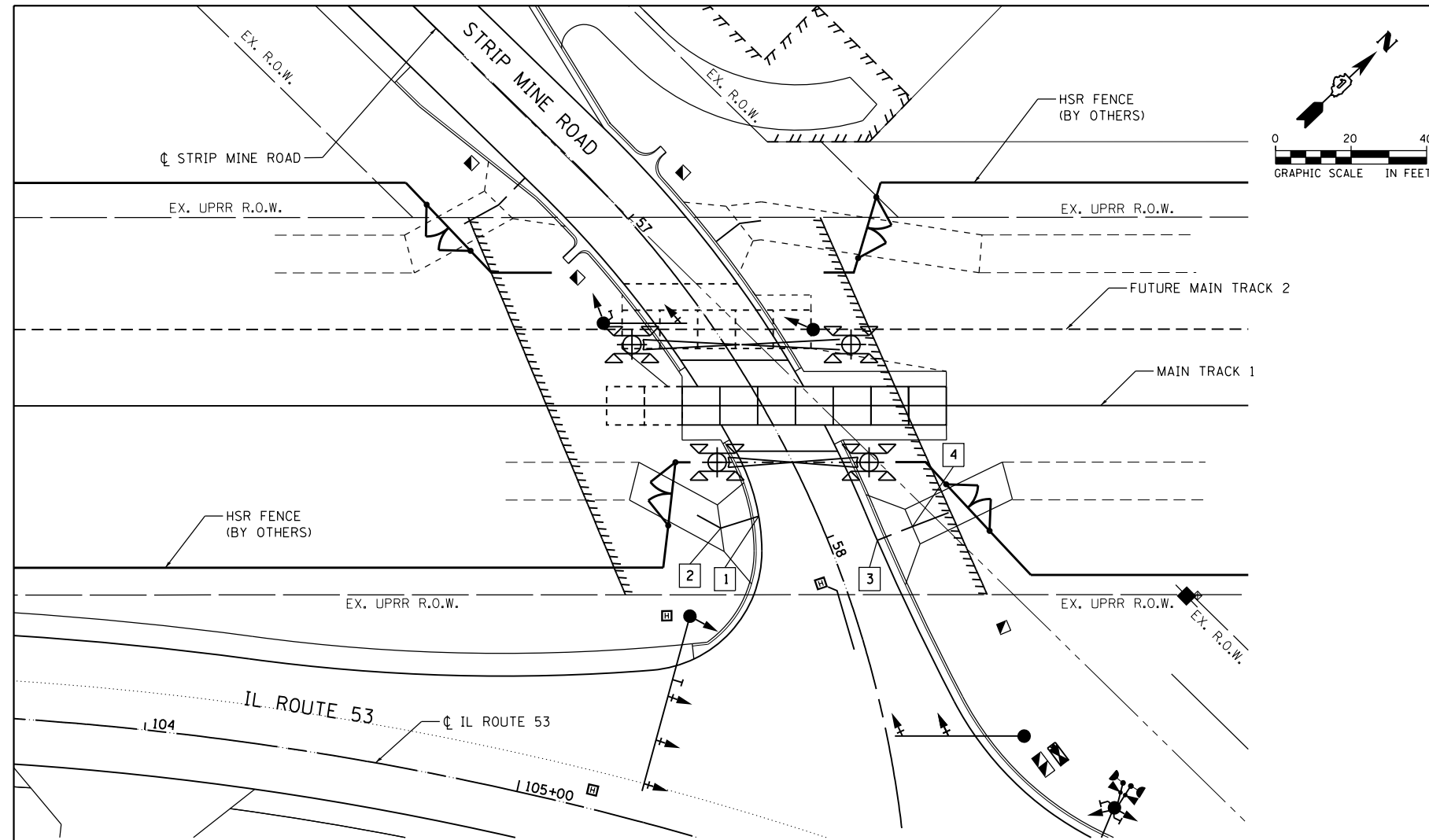
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PLOT SCALE = 40.0018 ft / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

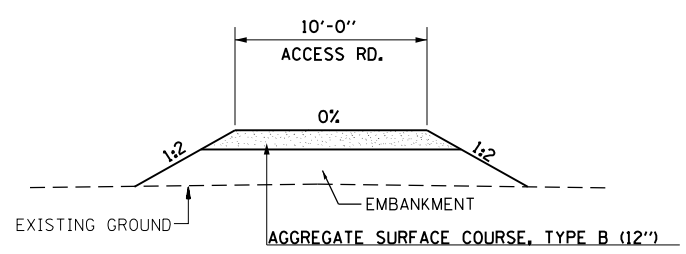
CONSTRUCTION DETAILS
RAILROAD CROSSING - STRIP MINE ROAD

SCALE: 1" = 20' SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02&03	WILL	580	117
			CONTRACT NO. 62B33	
ILLINOIS FED. AID PROJECT				



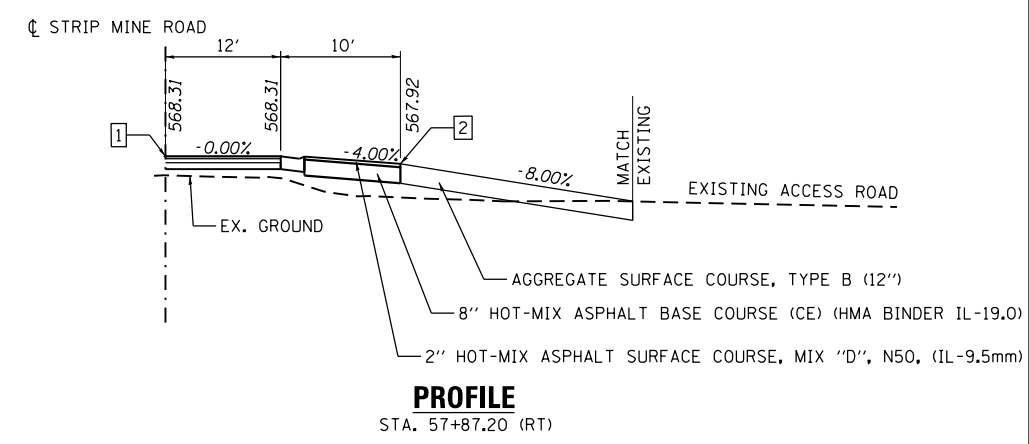
ACCESS ROAD APRON DETAIL
N.T.S



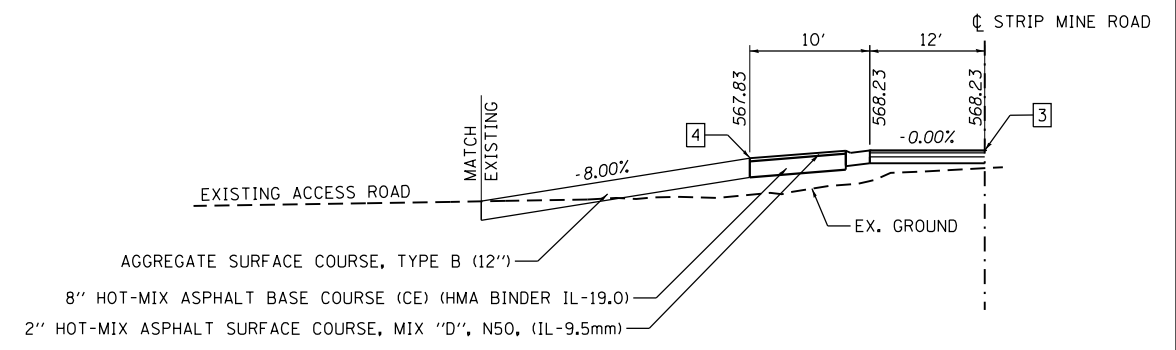
ACCESS ROAD TYPICAL SECTION
N.T.S

DETAIL OF ACCESS ROADS
(STRIP MINE ROAD)

- 1 STA. 57+87.20, 14.24' RT. (N 1689390.7795, E 1031833.9601)
- 2 STA. 57+85.75, 24.70' RT. (N 1689381.3073, E 1031829.3244)
- 3 STA. 58+05.35, 12.17' LT. (N 1689409.1136, E 1031860.1872)
- 4 STA. 58+05.35, 22.19' LT. (N 1689418.3855, E 1031864.0065)



PROFILE
STA. 57+87.20 (RT)



PROFILE
STA. 58+05.35 (LT)

FILE NAME: c:\ywork\hunting\nick.becker\transportation\dm11335\DI628333-ahf-detail1-FR-02.dgn



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	DATE - 02/05/2016	REVISED -

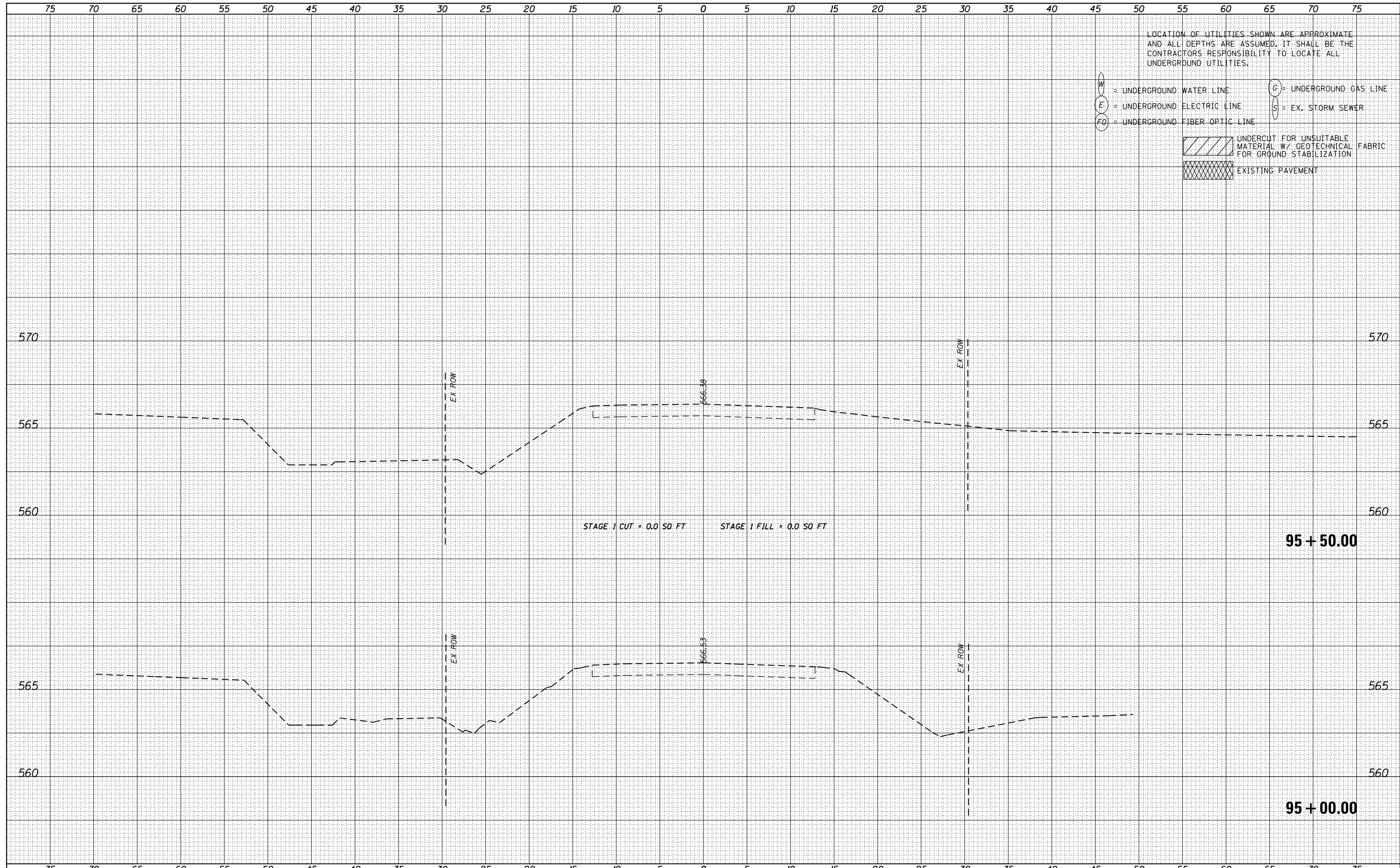
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS	
ACCESS ROAD - STRIP MINE ROAD	
SCALE: 1" = 20'	SHEET 2 OF 2 SHEETS
STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02&03	WILL	580	118
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
FINISH SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
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PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

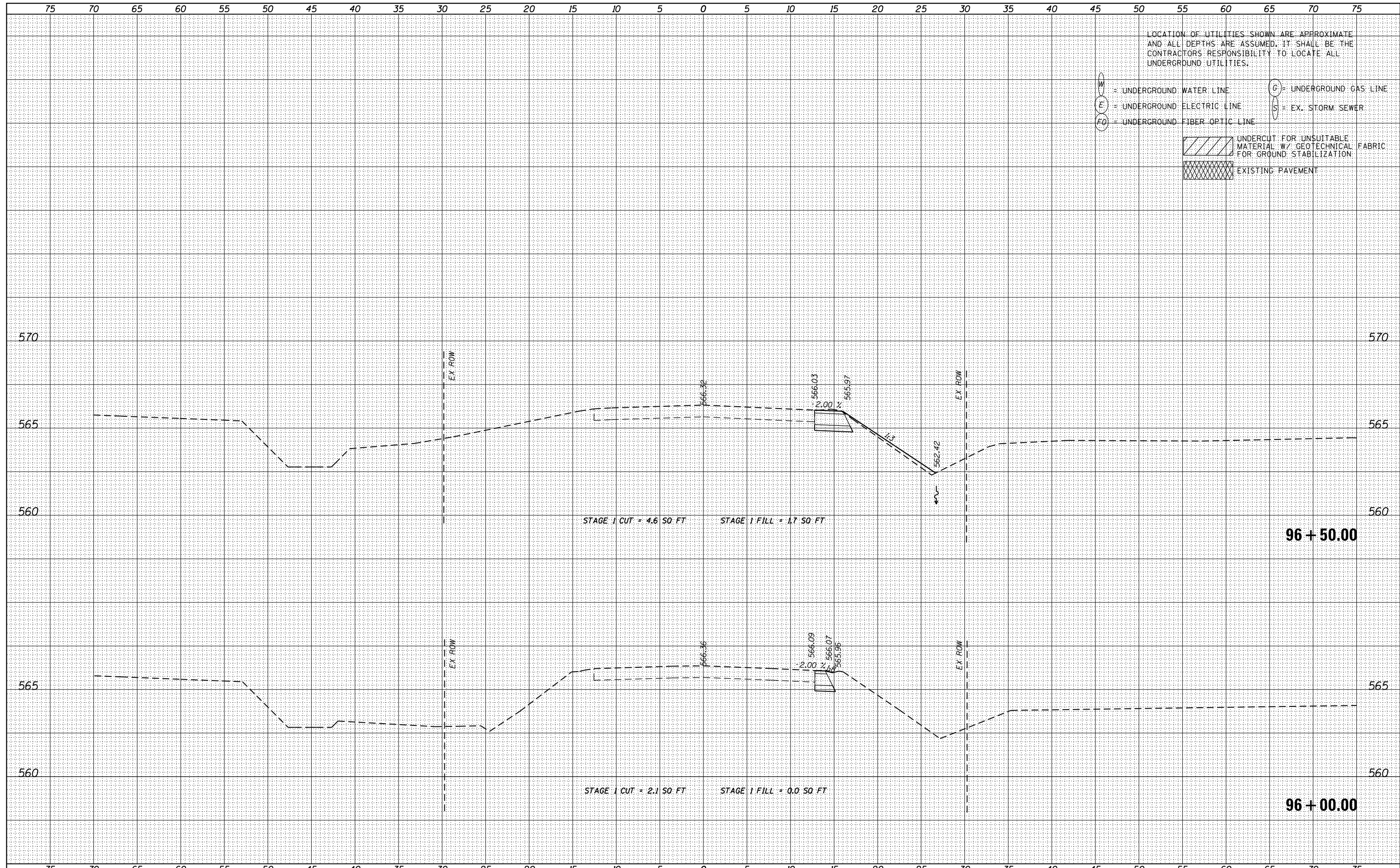
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 1
SCALE: 1"=5'4,2,5"V SHEET 1 OF 20 SHEETS STA. 95+00.00 TO STA. 95+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	119
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

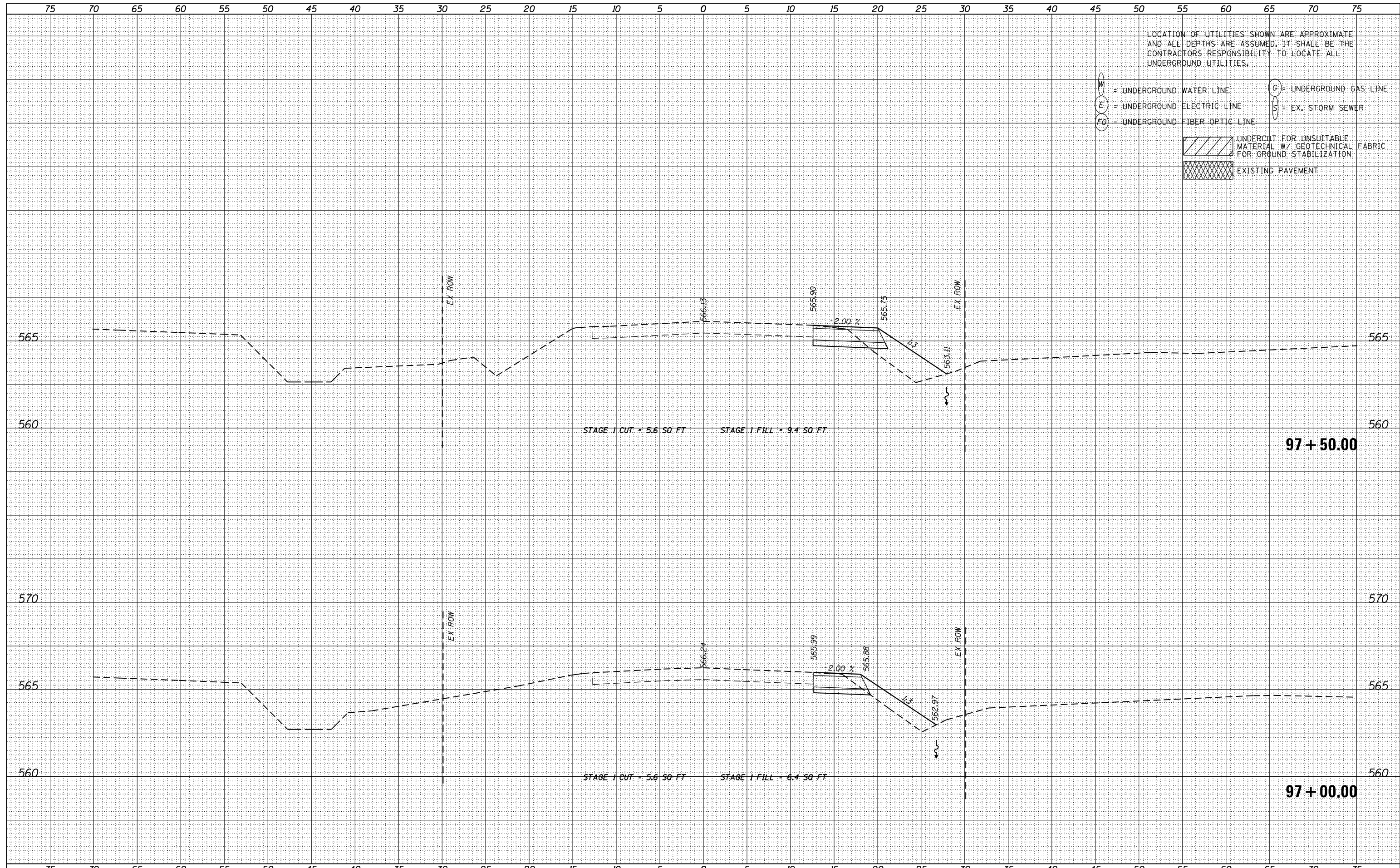
**CROSS SECTIONS
IL ROUTE 53 STAGE 1**

SCALE: 1"=5'H, 2.5'V SHEET 2 OF 20 SHEETS STA. 96+00.00 TO STA. 96+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	120
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

- W = UNDERGROUND WATER LINE
- E = UNDERGROUND ELECTRIC LINE
- FO = UNDERGROUND FIBER OPTIC LINE
- G = UNDERGROUND GAS LINE
- S = EX. STORM SEWER
- UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 1

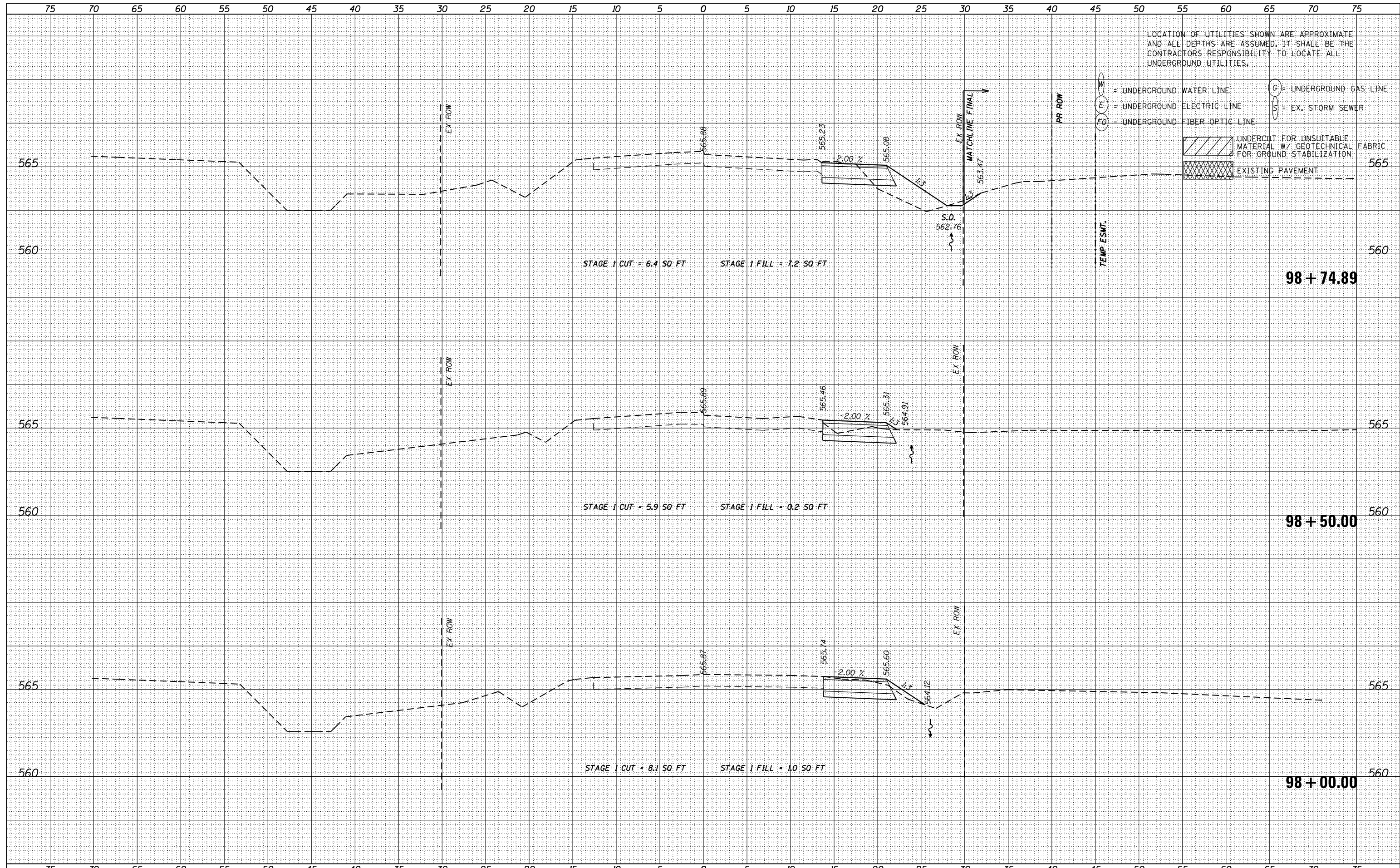
SCALE: 1"=5'H, 2.5'V SHEET 3 OF 20 SHEETS STA. 97+00.00 TO STA. 97+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	121
				CONTRACT NO. 62B33

ILLINOIS FED. AID PROJECT

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

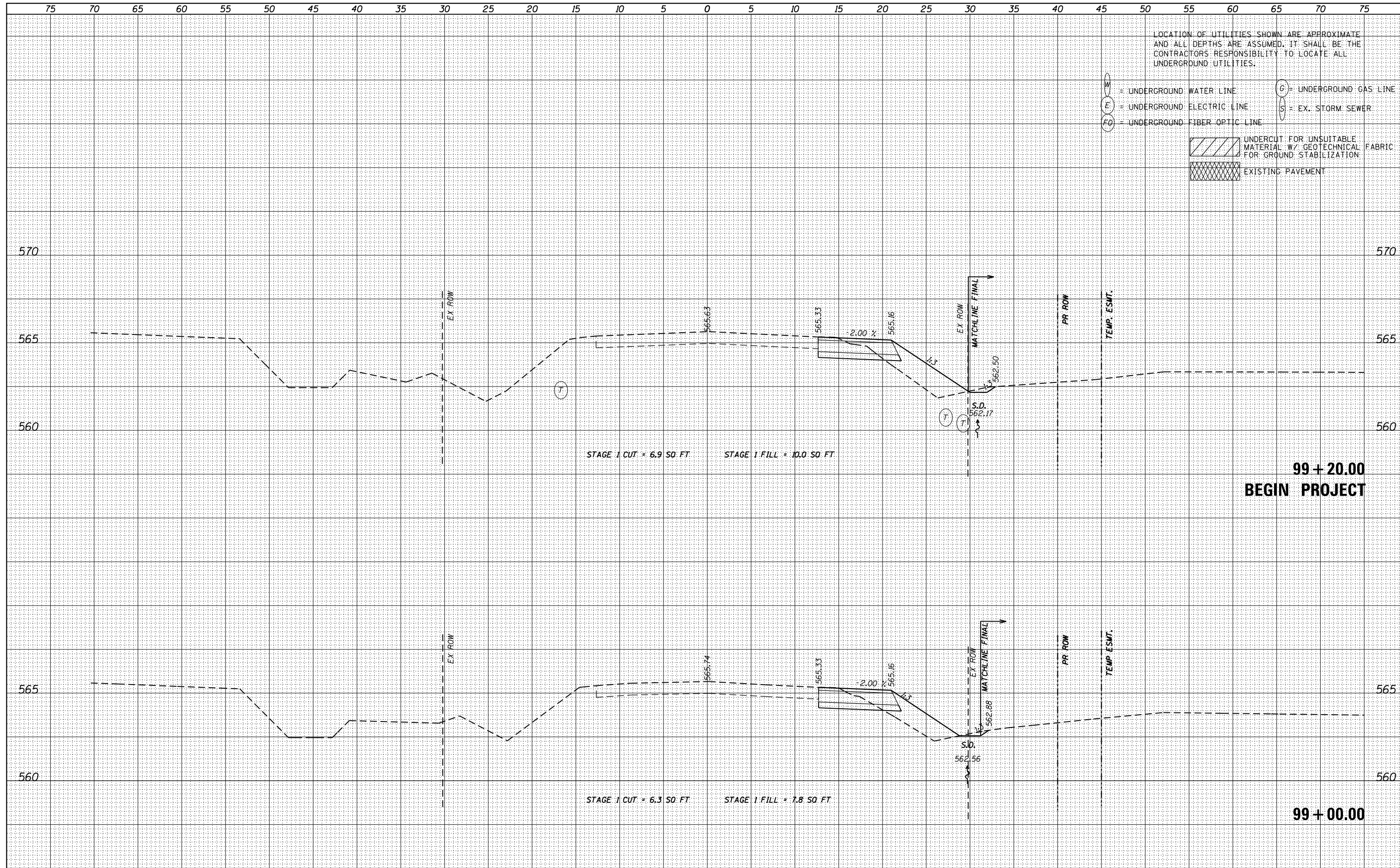
CROSS SECTIONS
IL ROUTE 53 STAGE 1

SCALE: 1"=5'4,2,5'V SHEET 4 OF 20 SHEETS STA. 98+00.00 TO STA. 98+74.89

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	122
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

W = UNDERGROUND WATER LINE
 E = UNDERGROUND ELECTRIC LINE
 FO = UNDERGROUND FIBER OPTIC LINE
 G = UNDERGROUND GAS LINE
 S = EX. STORM SEWER

[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 1

SCALE: 1"=5'H, 2.5'V SHEET 5 OF 20 SHEETS STA. 99+00.00 TO STA. 99+20.00

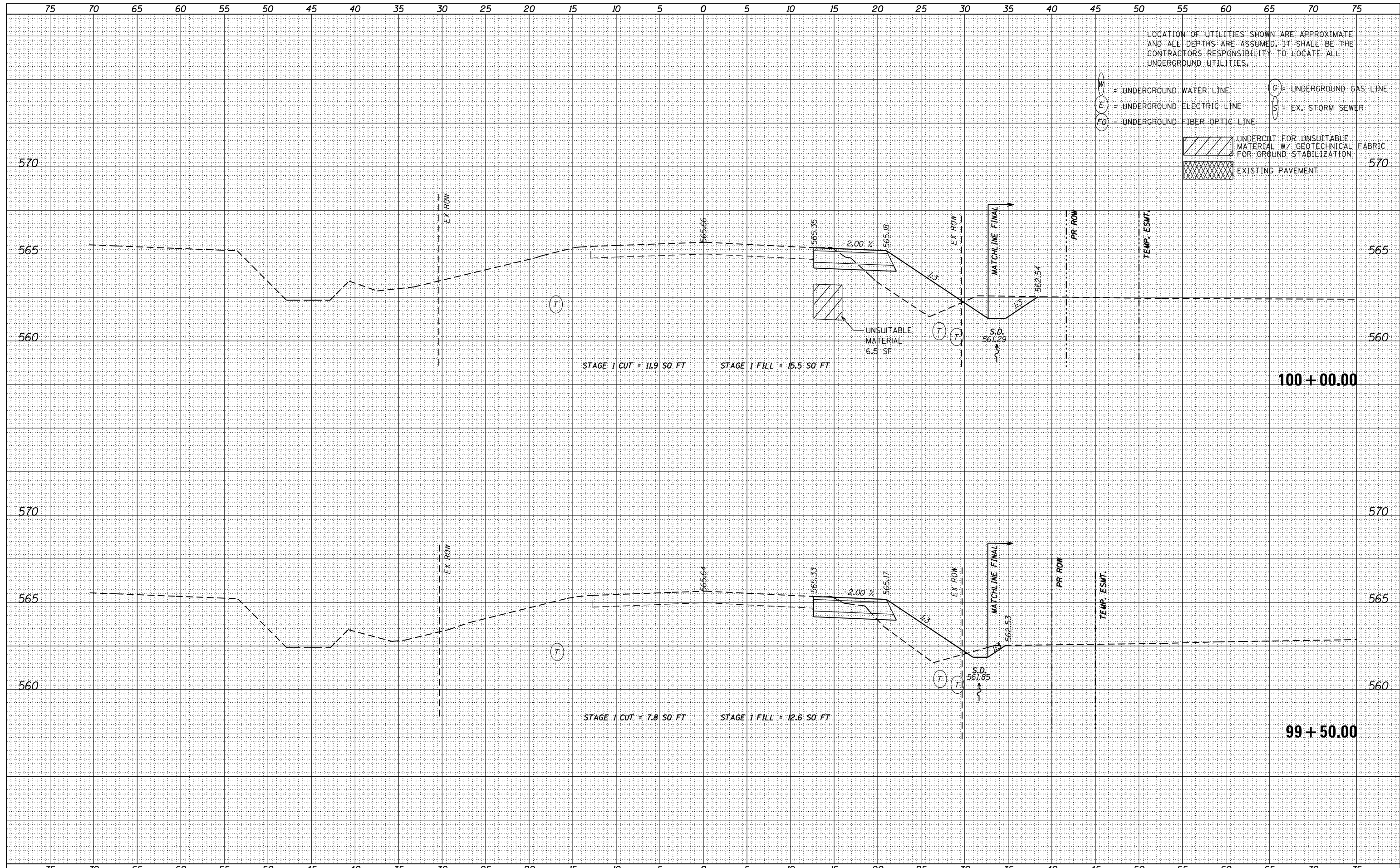
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	123
				CONTRACT NO. 62B33

99+20.00
BEGIN PROJECT

99+00.00

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

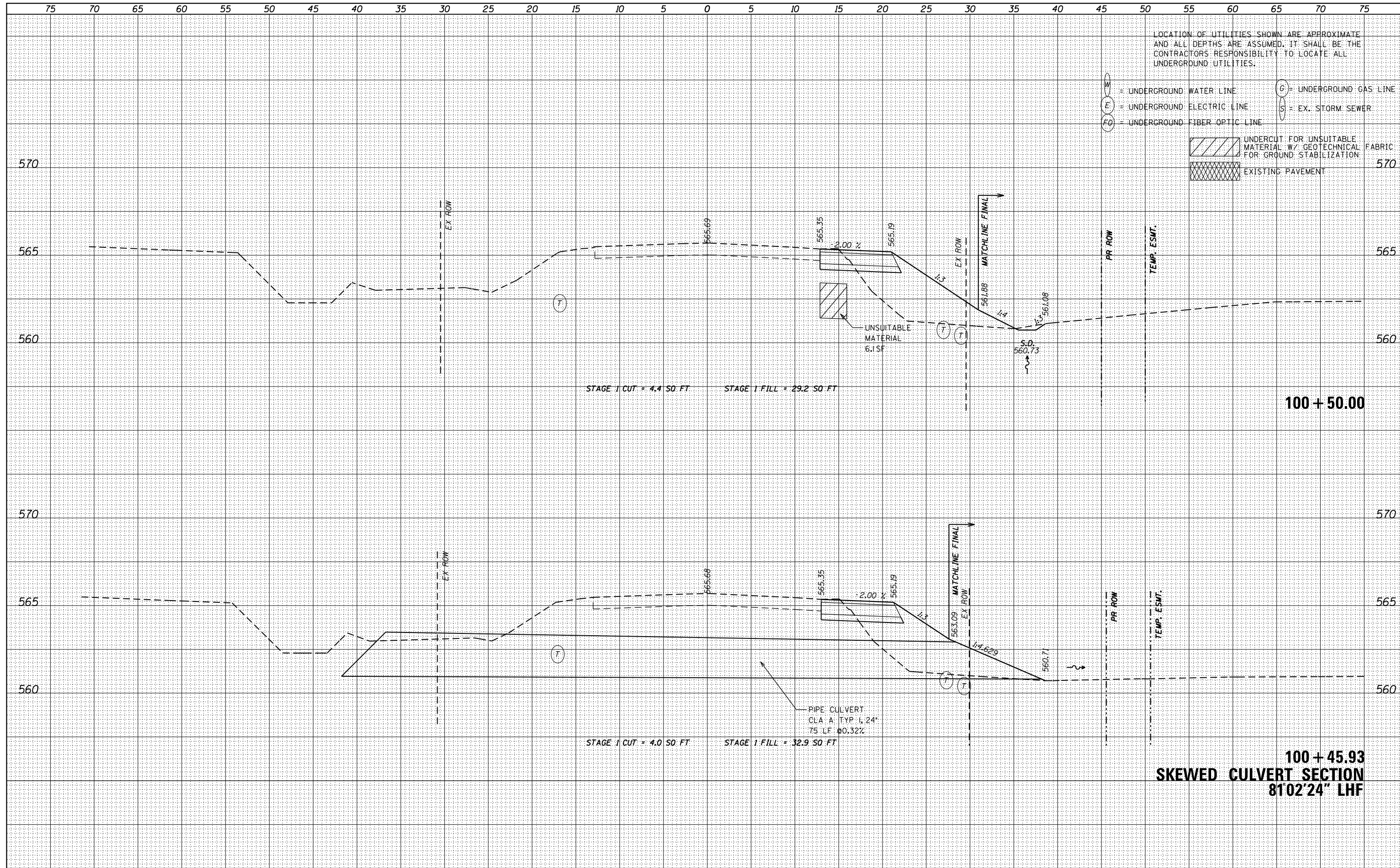
**CROSS SECTIONS
IL ROUTE 53 STAGE 1**

SCALE: 1"=5'4,2.5'V SHEET 6 OF 20 SHEETS STA. 99+50.00 TO STA.100+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	124
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

W = UNDERGROUND WATER LINE
 E = UNDERGROUND ELECTRIC LINE
 FO = UNDERGROUND FIBER OPTIC LINE
 G = UNDERGROUND GAS LINE
 S = EX. STORM SEWER

[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 1

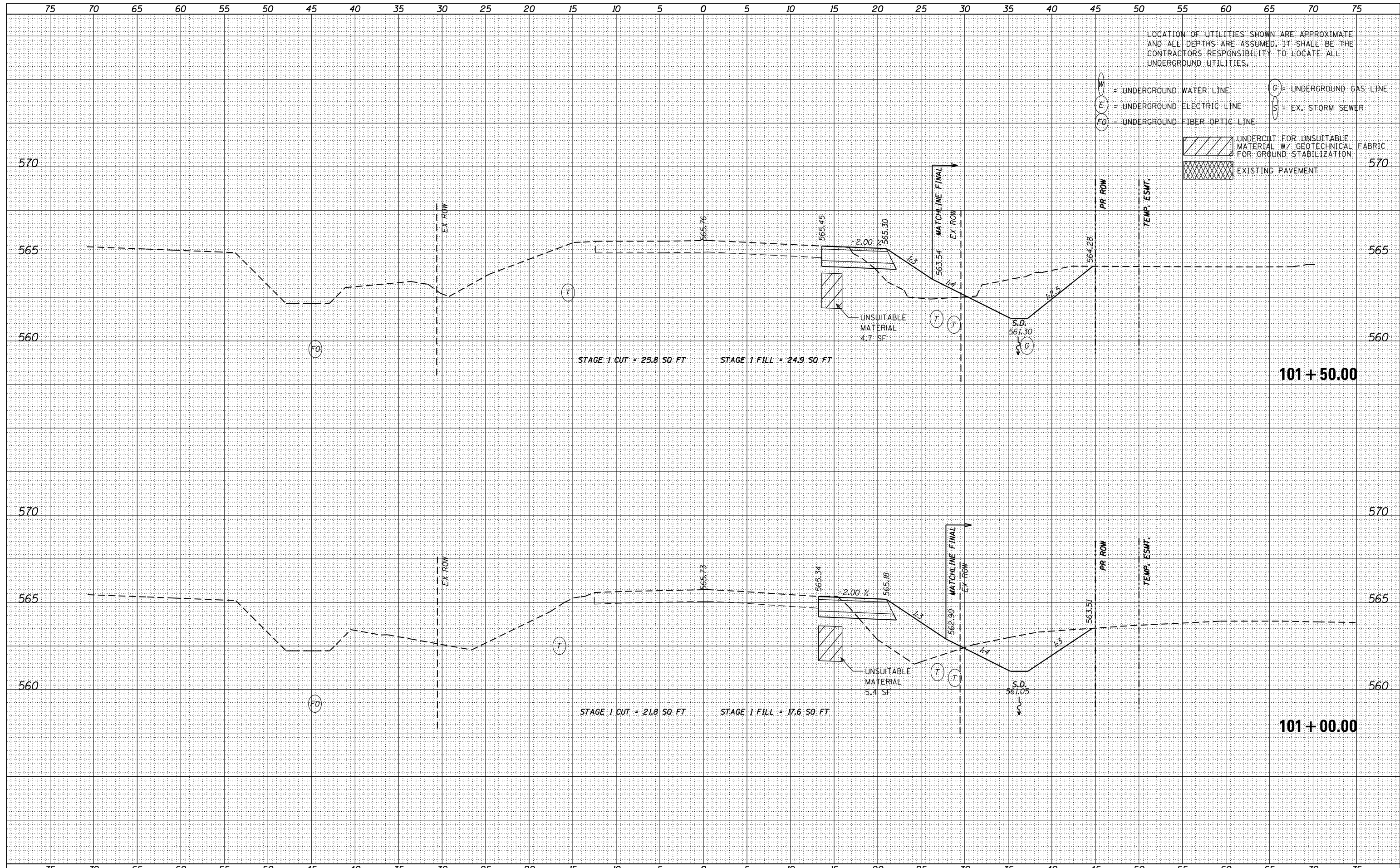
SCALE: 1"=5'4,2,5"V SHEET 7 OF 20 SHEETS STA. 100+45.93 TO STA. 100+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	125
				CONTRACT NO. 62B33

ILLINOIS FED. AID PROJECT

DATE	
BY	
FINAL SURVEY	
SURVEY PLOTTED	
NOTE BOOK	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
SURVEY PLOTTED	
NOTE BOOK	
AREAS CHECKED	



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

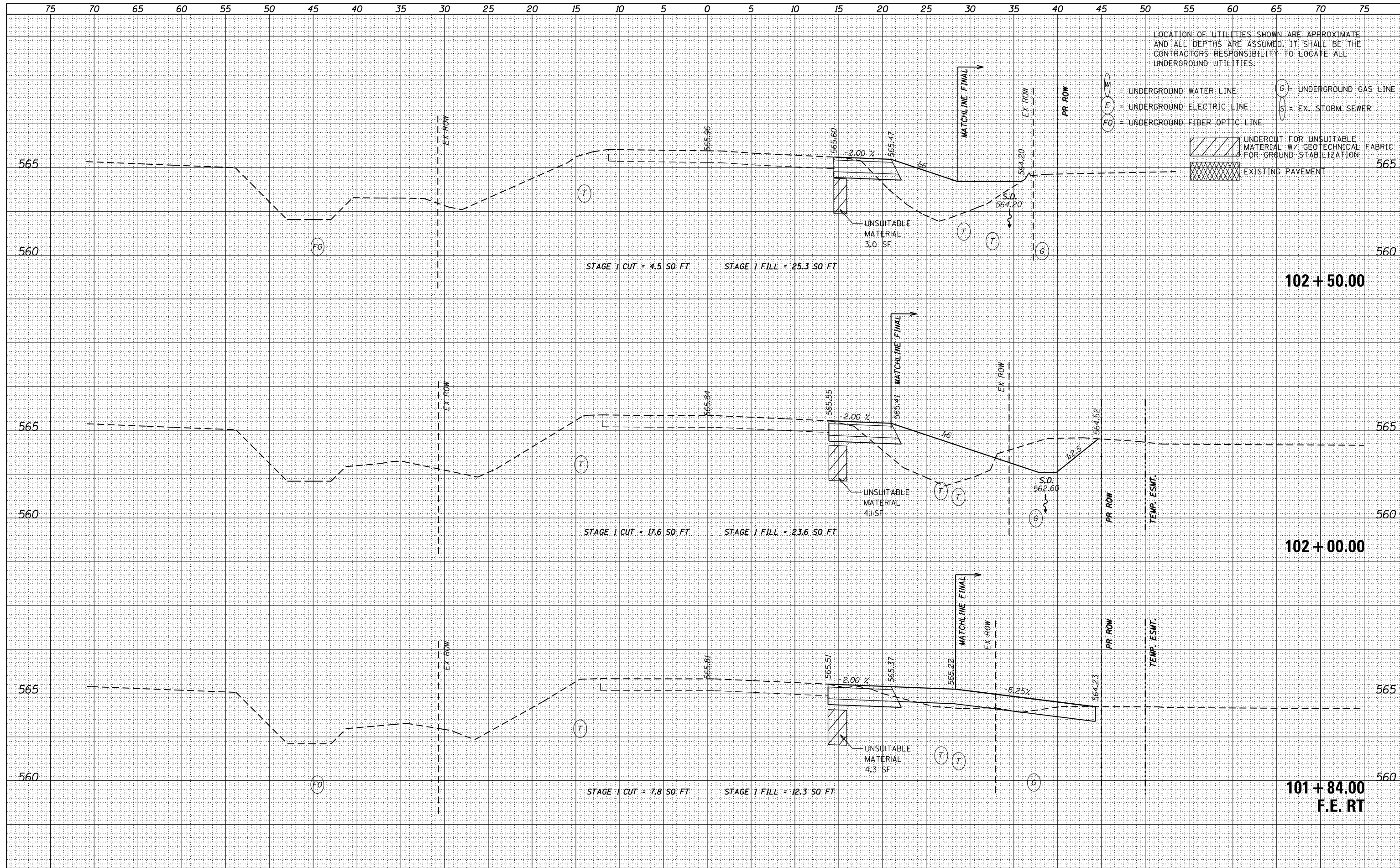
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS	
IL ROUTE 53 STAGE 1	
SCALE: 1"=5'4,2,5'V	SHEET 8 OF 20 SHEETS
STA. 101+00.00	TO STA. 101+50.00

F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 126
			CONTRACT NO. 62B33	
ILLINOIS FED. AID PROJECT				

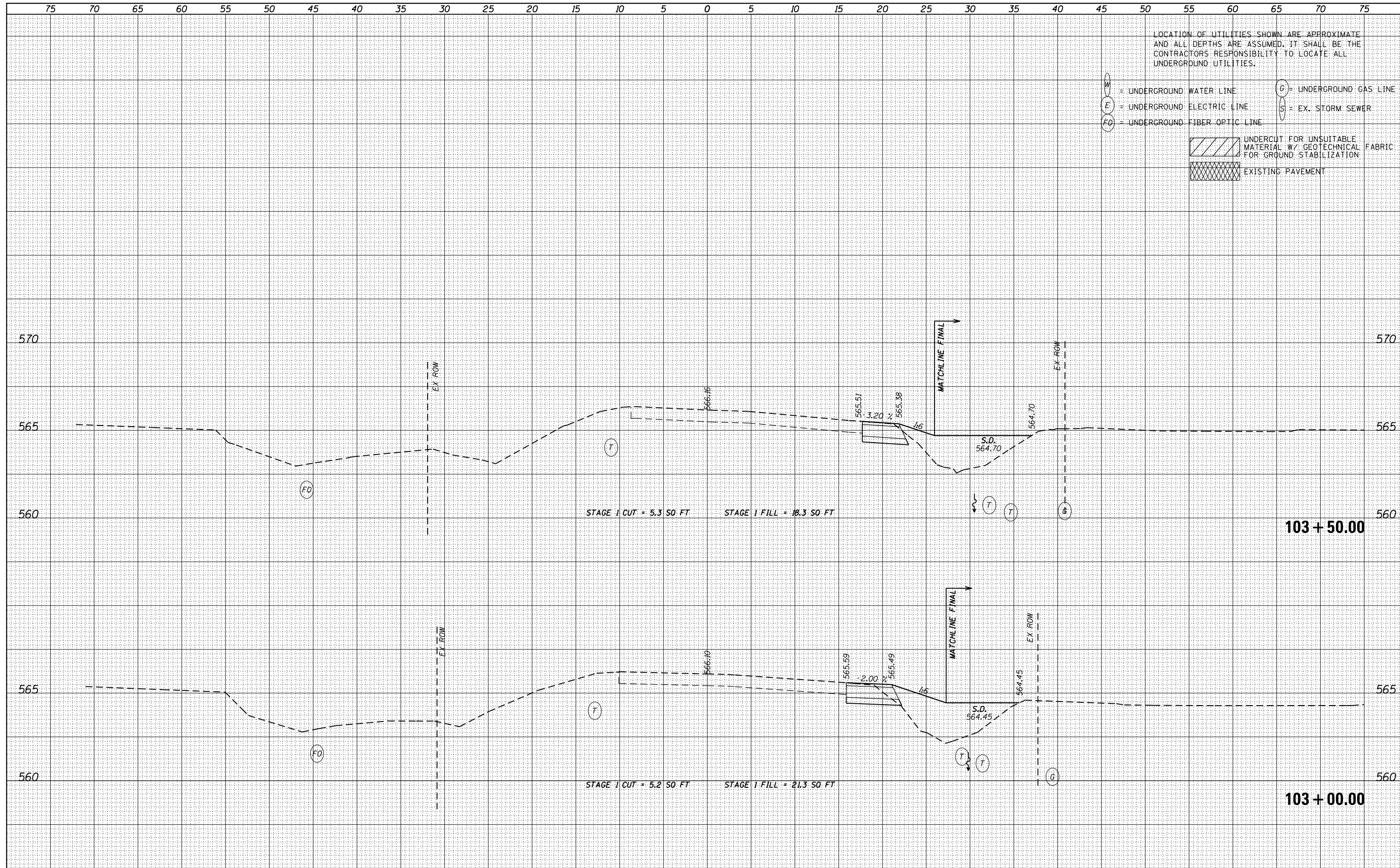
DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
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DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
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DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

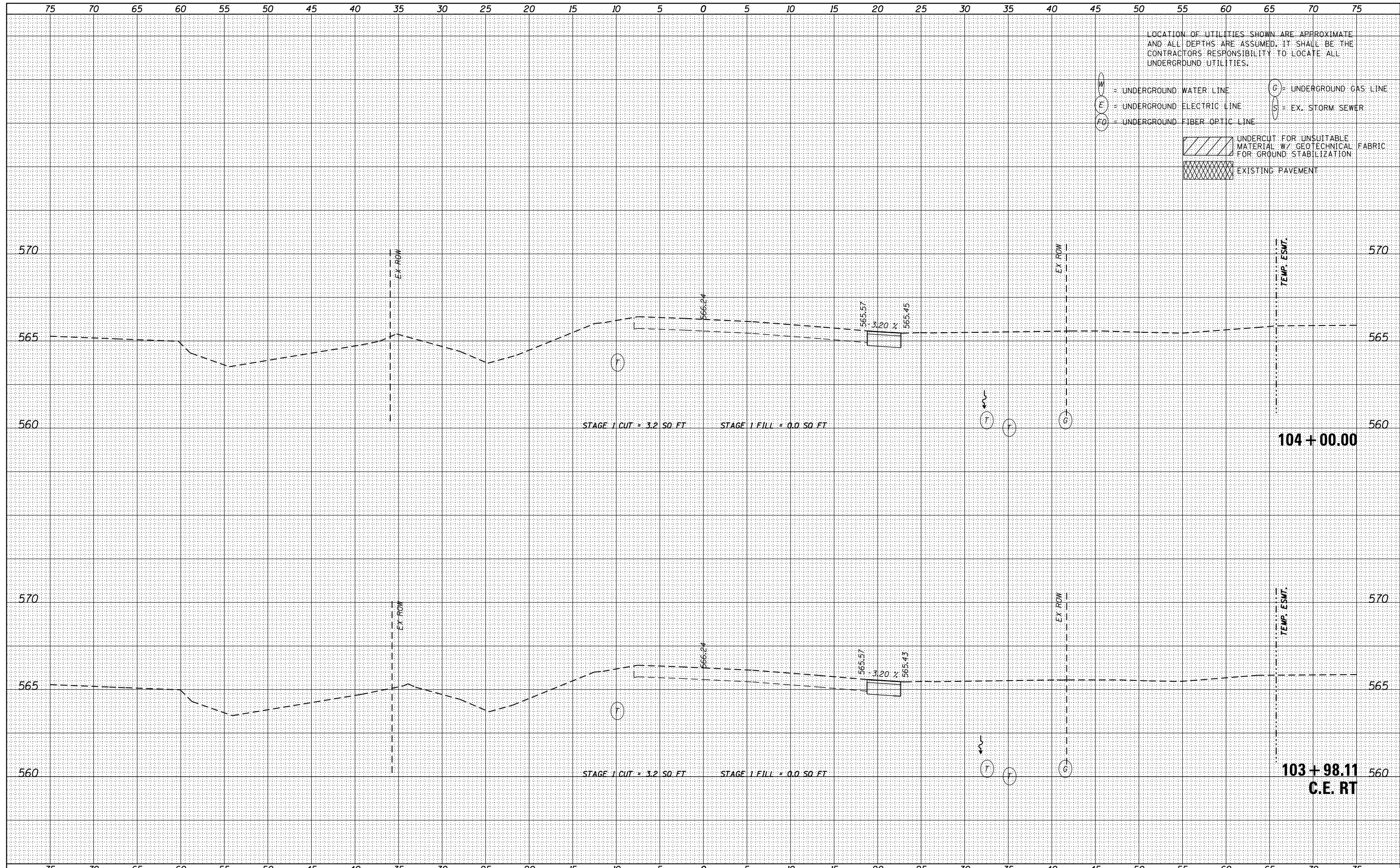
**CROSS SECTIONS
IL ROUTE 53 STAGE 1**

SCALE: 1"=5'4,2,5'V SHEET 10 OF 20 SHEETS STA. 103+00.00 TO STA.103+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	128
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



USER NAME = nick.becker	DESIGNED - JLM	REVISIED -
	DRAWN - BWL	REVISIED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISIED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISIED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

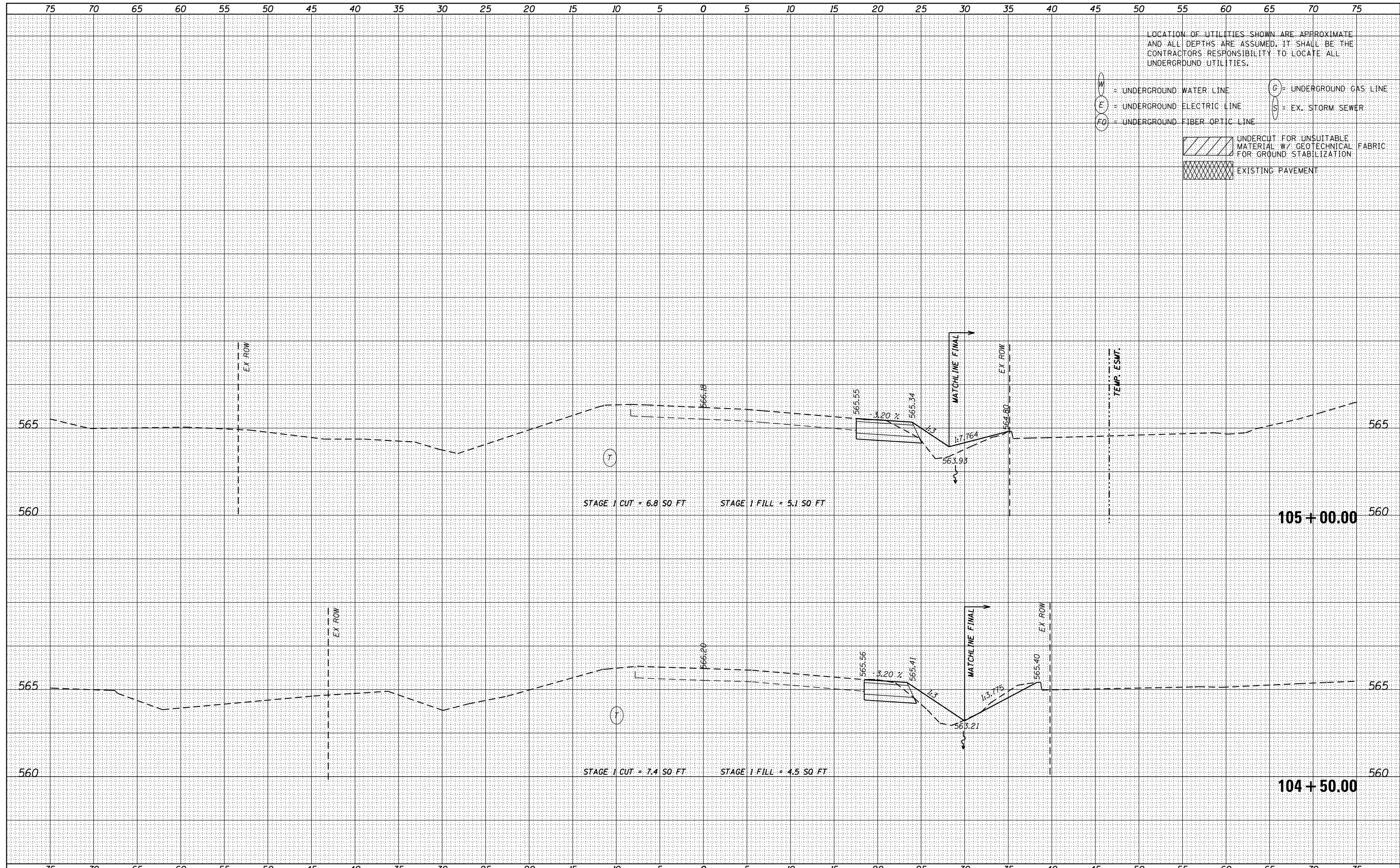
CROSS SECTIONS
IL ROUTE 53 STAGE 1

SCALE: 1"=5'H, 2.5'V SHEET 11 OF 20 SHEETS STA. 103+98.11 TO STA. 104+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	129
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
	TEMPLATE
	AREAS CHECKED



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

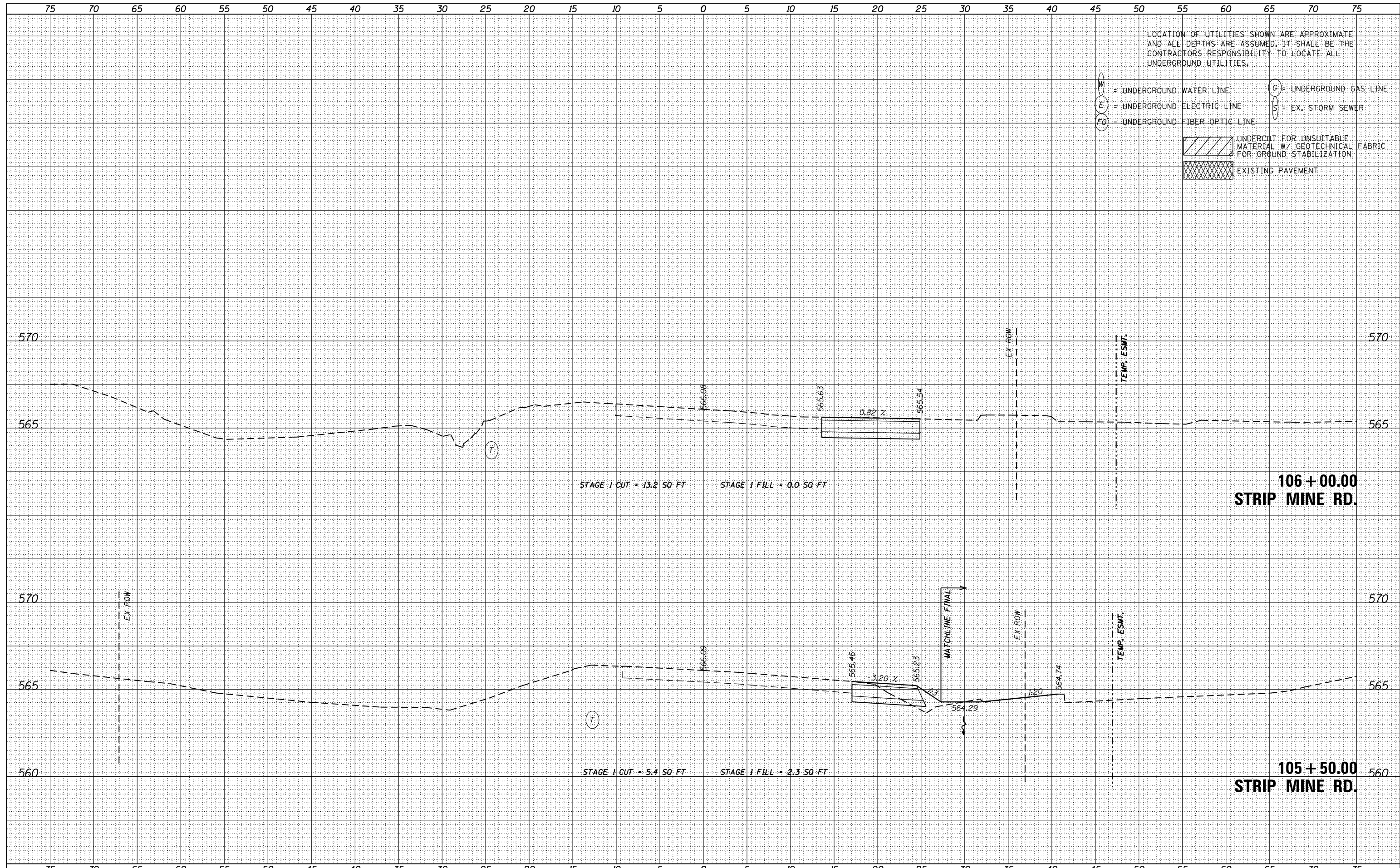
**CROSS SECTIONS
IL ROUTE 53 STAGE 1**

SCALE: 1"=5'4,2,5"V SHEET 12 OF 20 SHEETS STA. 104+50.00 TO STA.105+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	130
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	



USER NAME = nick.becker	DESIGNED - JLM	REVISIONS -
PLLOT SCALE = 10.0000' / in.	DRAWN - BWL	REVISIONS -
PLLOT DATE = 3/23/2016	CHECKED - NJB	REVISIONS -
	DATE - 02/05/2016	REVISIONS -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

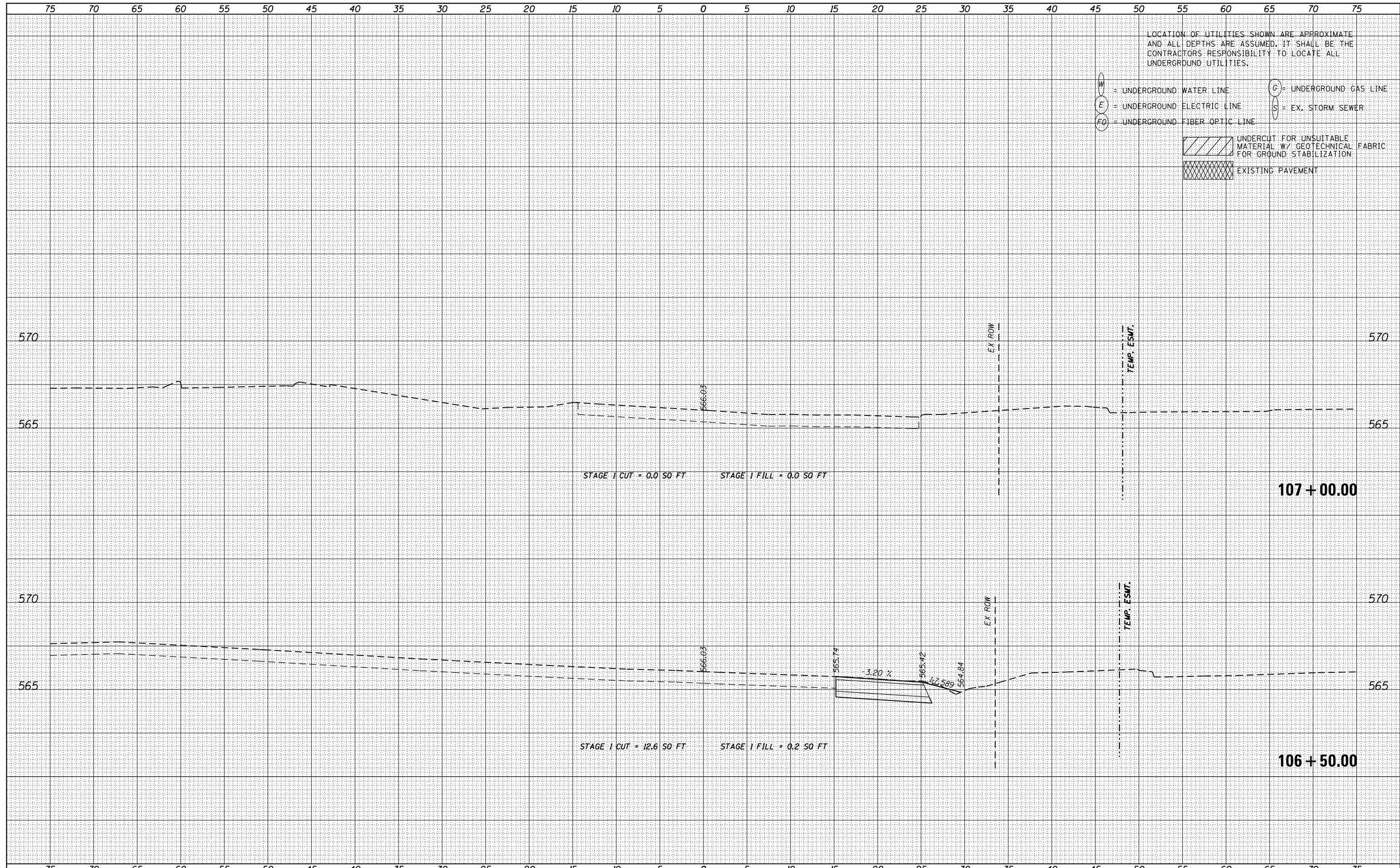
**CROSS SECTIONS
IL ROUTE 53 STAGE 1**

SCALE: 1"=5'4,2,5'V SHEET 13 OF 20 SHEETS STA. 105+50.00 TO STA.106+00.00

F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 131
CONTRACT NO. 62B33				ILLINOIS FED. AID PROJECT

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
FINISH SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

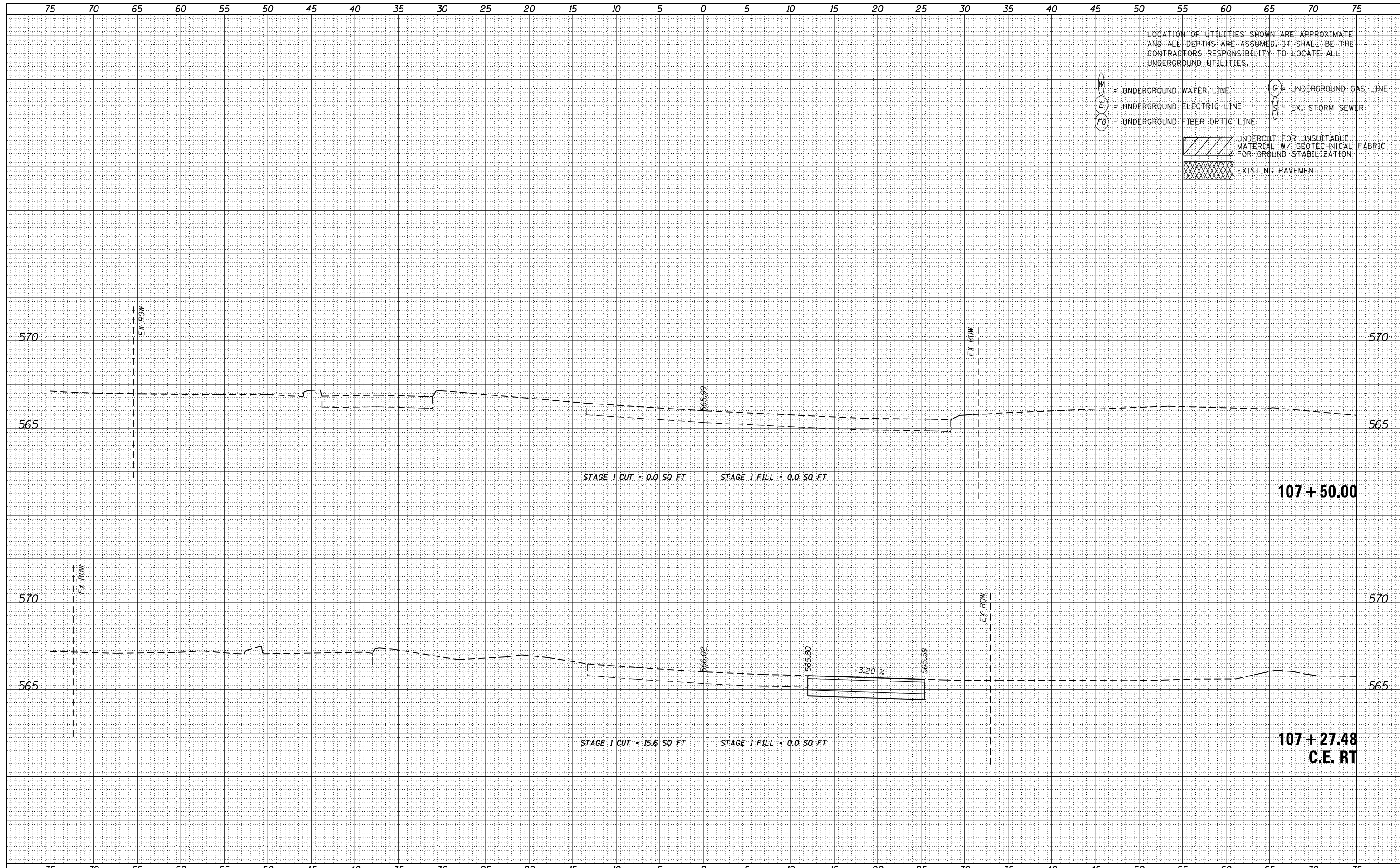
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS	
IL ROUTE 53 STAGE 1	
SCALE: 1"=5'4,2,5'V	SHEET 14 OF 20 SHEETS
STA. 106+50.00	TO STA. 107+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	132
CONTRACT NO. 62B33				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

- W = UNDERGROUND WATER LINE
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- G = UNDERGROUND GAS LINE
- S = EX. STORM SEWER
- UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 1

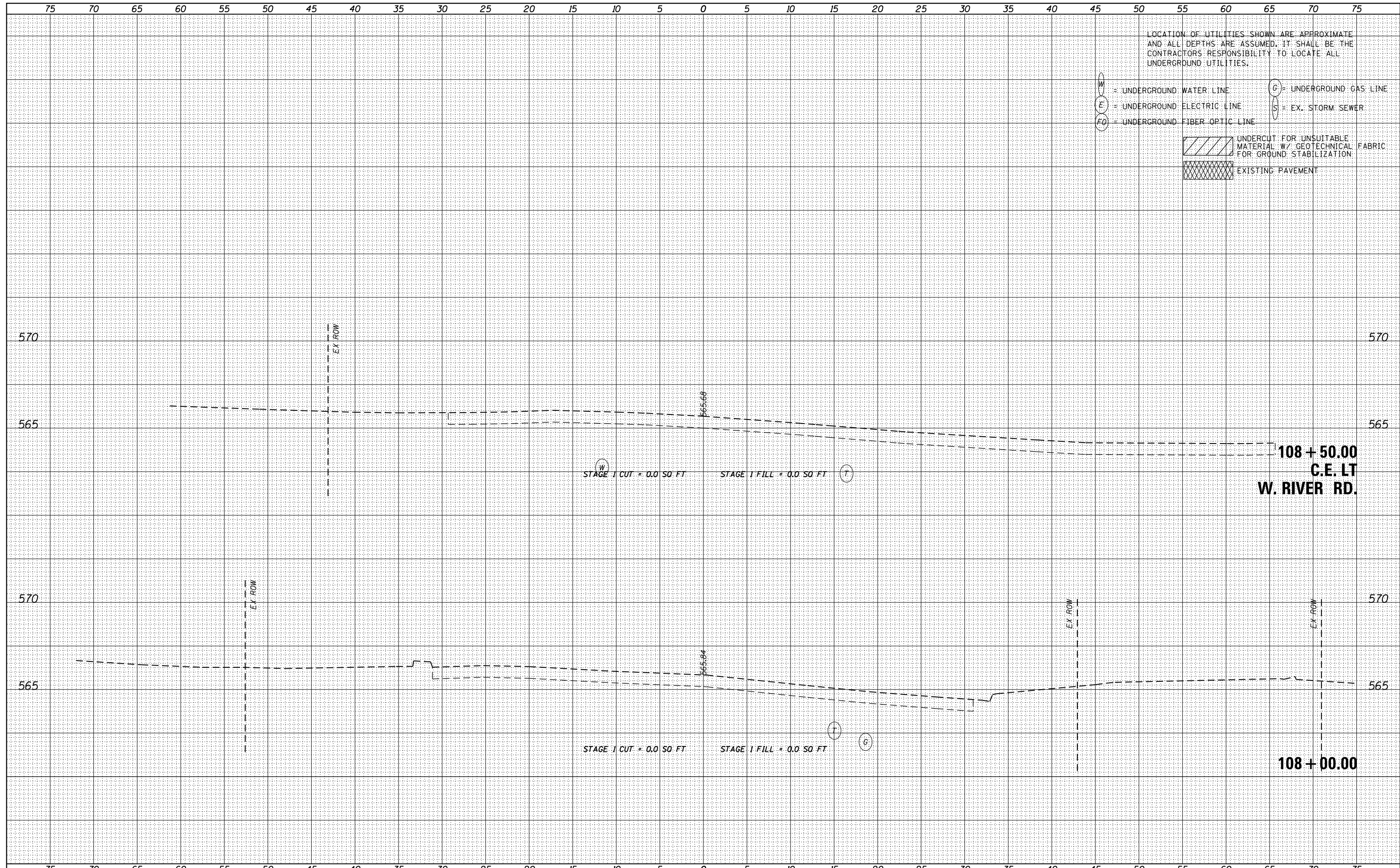
SCALE: 1"=5'H, 2.5'V SHEET 15 OF 20 SHEETS STA. 107+27.48 TO STA. 107+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	133
				CONTRACT NO. 62B33

ILLINOIS FED. AID PROJECT

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



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- G = UNDERGROUND GAS LINE
- S = EX. STORM SEWER
- UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 1

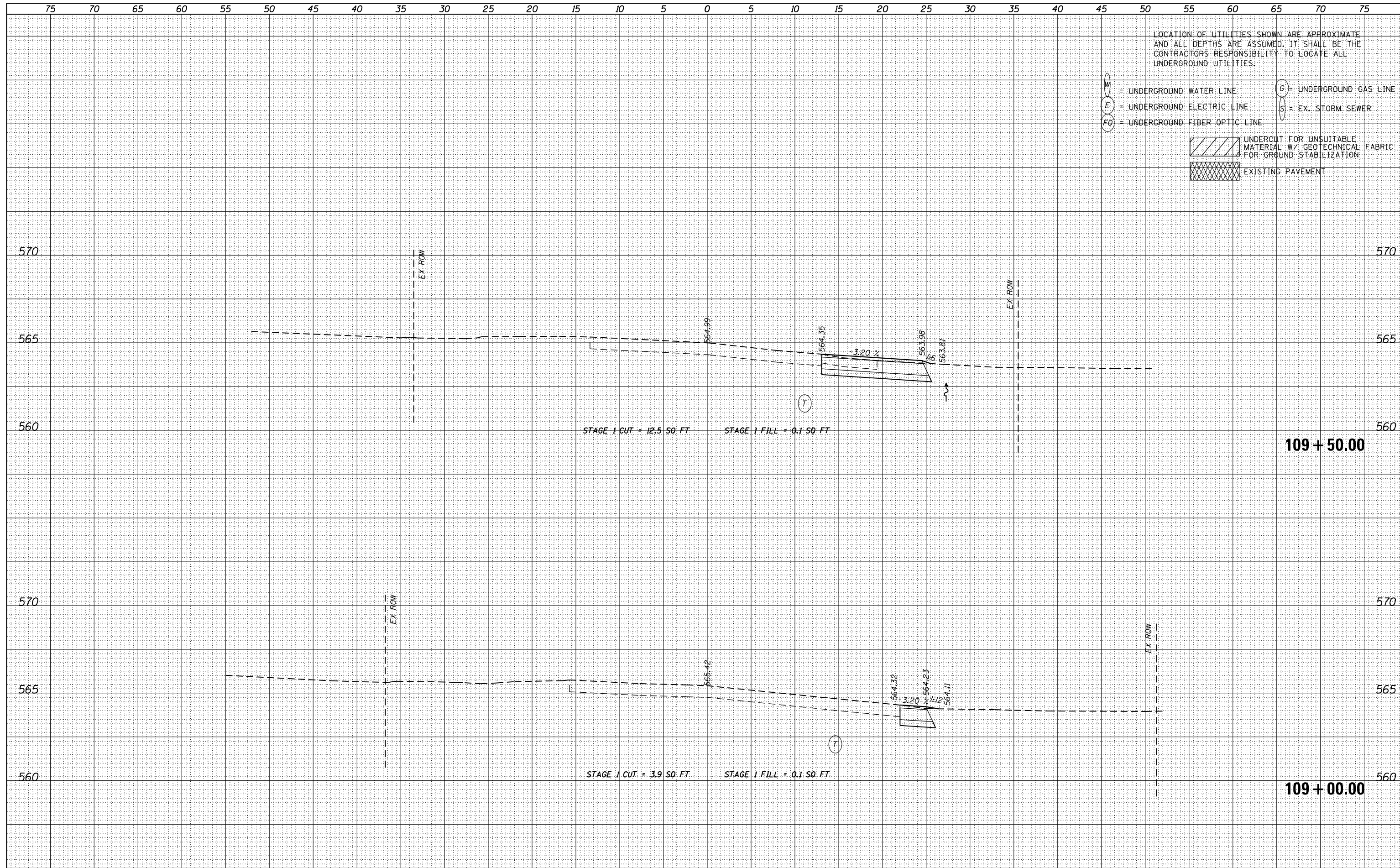
SCALE: 1"=5'4,2,5"V SHEET 16 OF 20 SHEETS STA. 108+00.00 TO STA.108+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	134
CONTRACT NO. 62B33				

ILLINOIS FED. AID PROJECT

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
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NO.	TEMPLATE
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 G = UNDERGROUND GAS LINE
 S = EX. STORM SEWER

[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

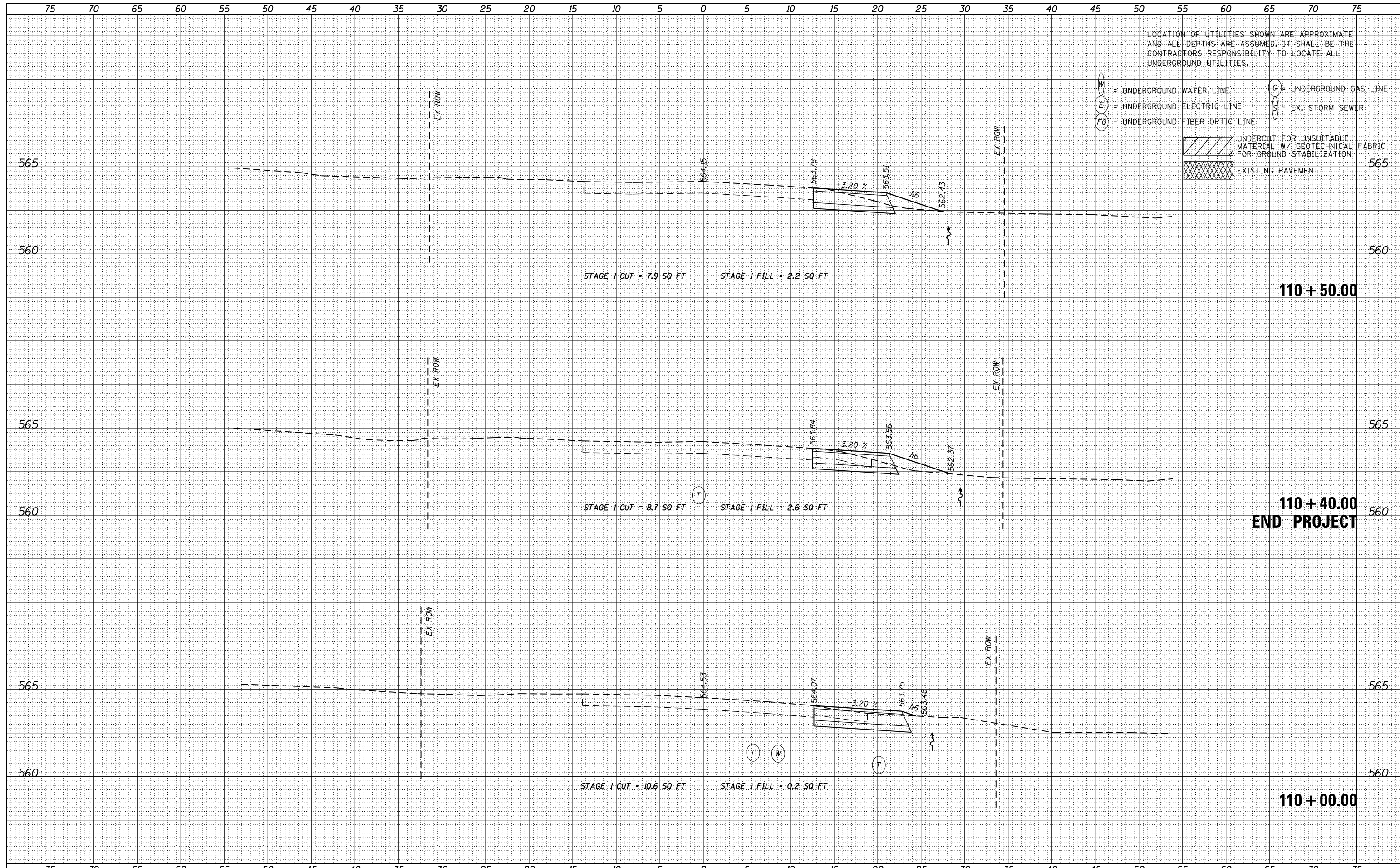
**CROSS SECTIONS
IL ROUTE 53 STAGE 1**

SCALE: 1"=5'4,2,5'V SHEET 17 OF 20 SHEETS STA. 109+00.00 TO STA.109+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	135
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
IL ROUTE 53 STAGE 1**

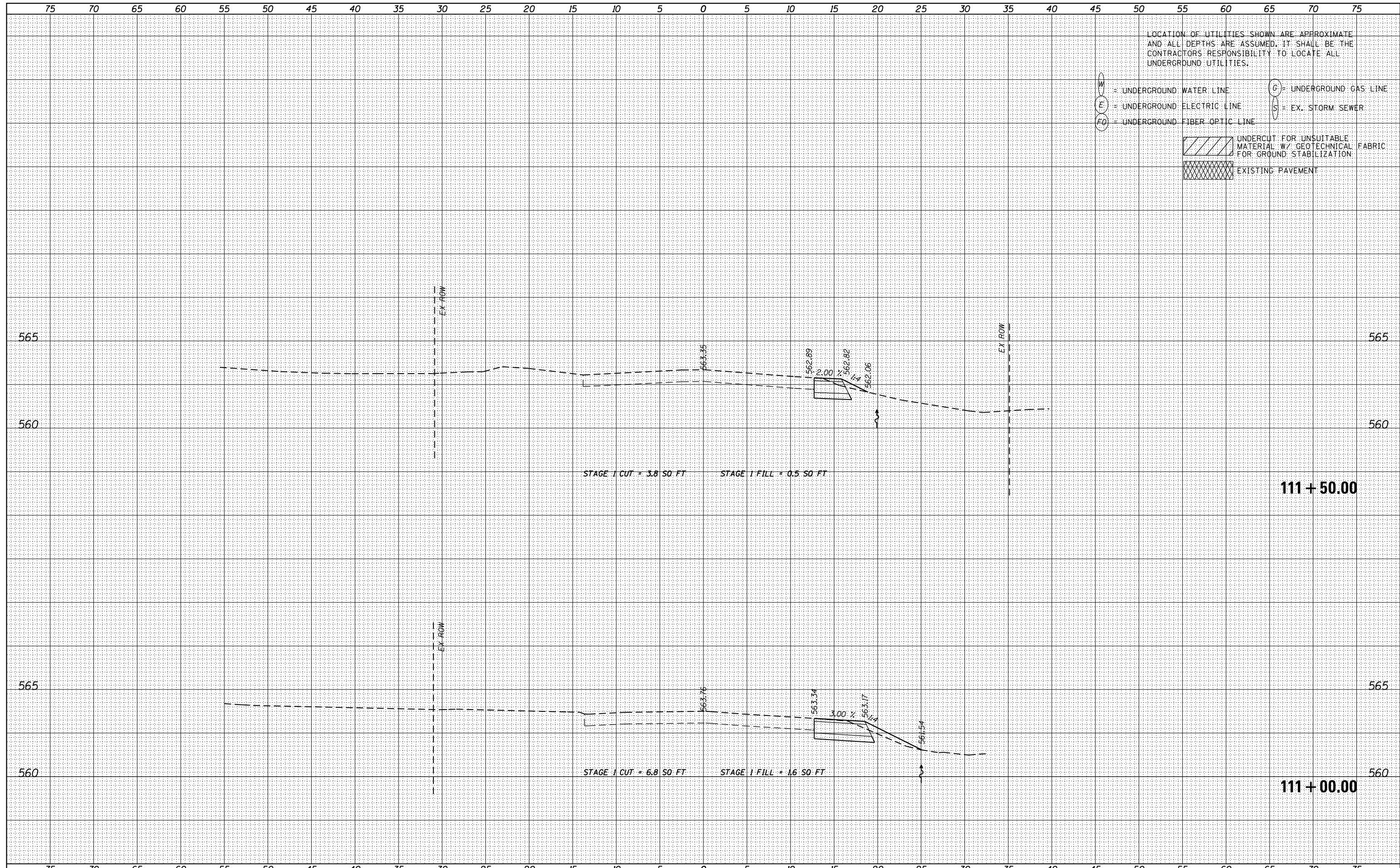
SCALE: 1"=5'4,2.5'V SHEET 18 OF 20 SHEETS STA. 110+00.00 TO STA. 110+50.00

F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 136
			CONTRACT NO. 62B33	

ILLINOIS FED. AID PROJECT

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
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DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

- W = UNDERGROUND WATER LINE
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- FO = UNDERGROUND FIBER OPTIC LINE
- G = UNDERGROUND GAS LINE
- S = EX. STORM SEWER
- UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISD -
	DRAWN - BWL	REVISD -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISD -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISD -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 1

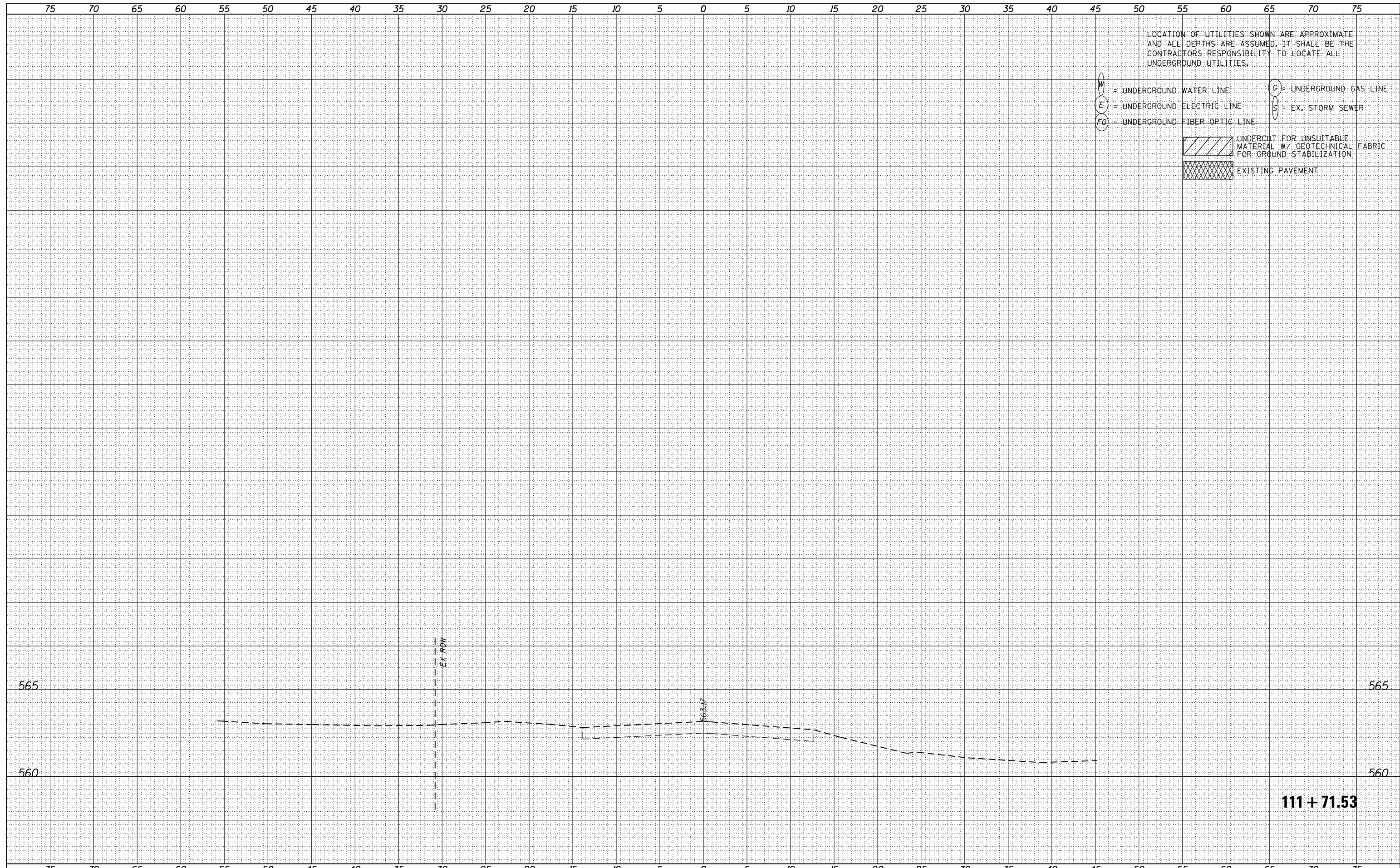
SCALE: 1"=5'4,2,5'V SHEET 19 OF 20 SHEETS STA. 111+00.00 TO STA. 111+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	137
				CONTRACT NO. 62B33

ILLINOIS FED. AID PROJECT

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
	TEMPLATE
	AREAS CHECKED



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

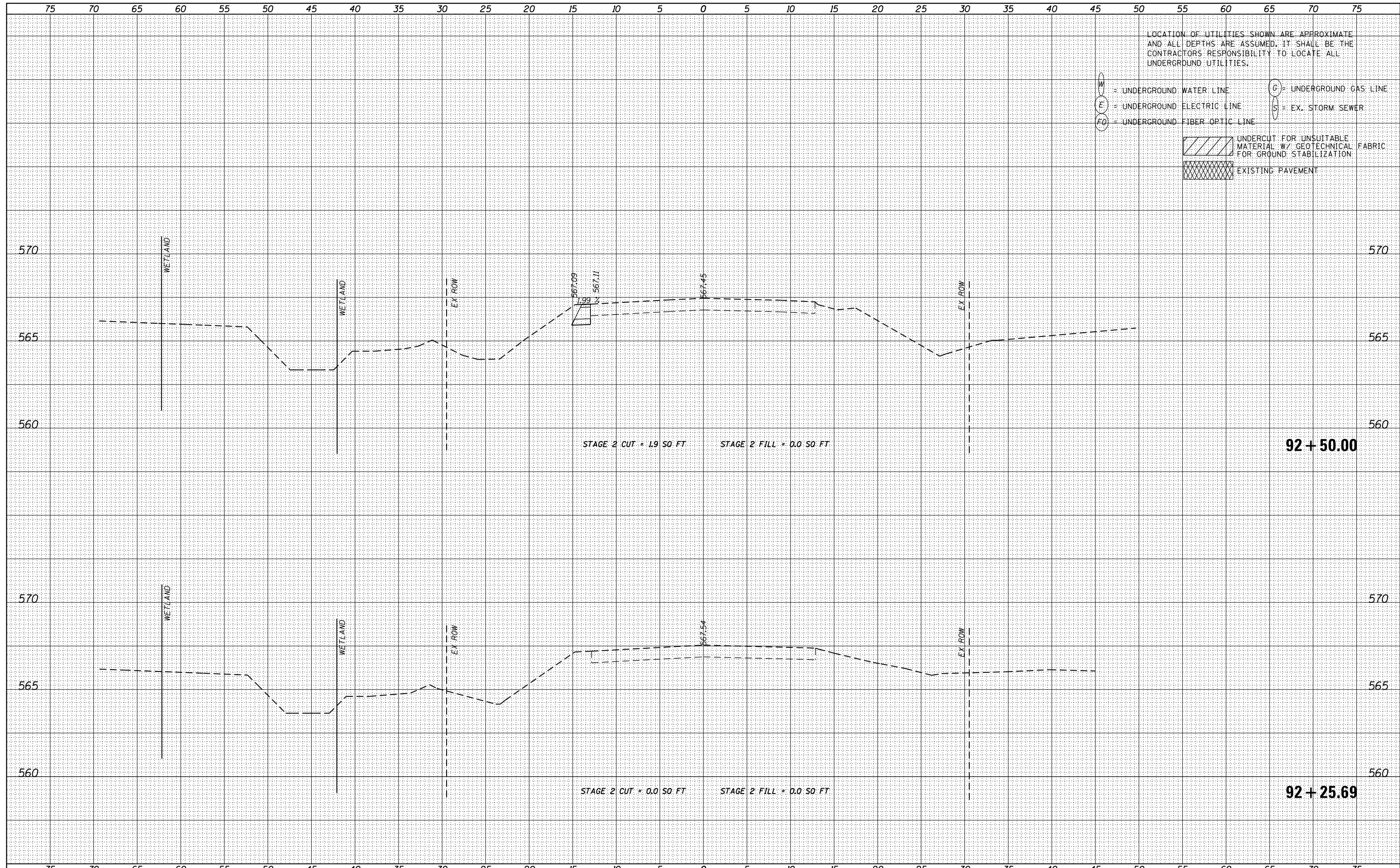
**CROSS SECTIONS
IL ROUTE 53 STAGE 1**

SCALE: 1"=5'H, 2.5"V SHEET 20 OF 20 SHEETS STA. 111+71.53 TO STA. 111+71.53

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	138
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

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- S = EX. STORM SEWER
- UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

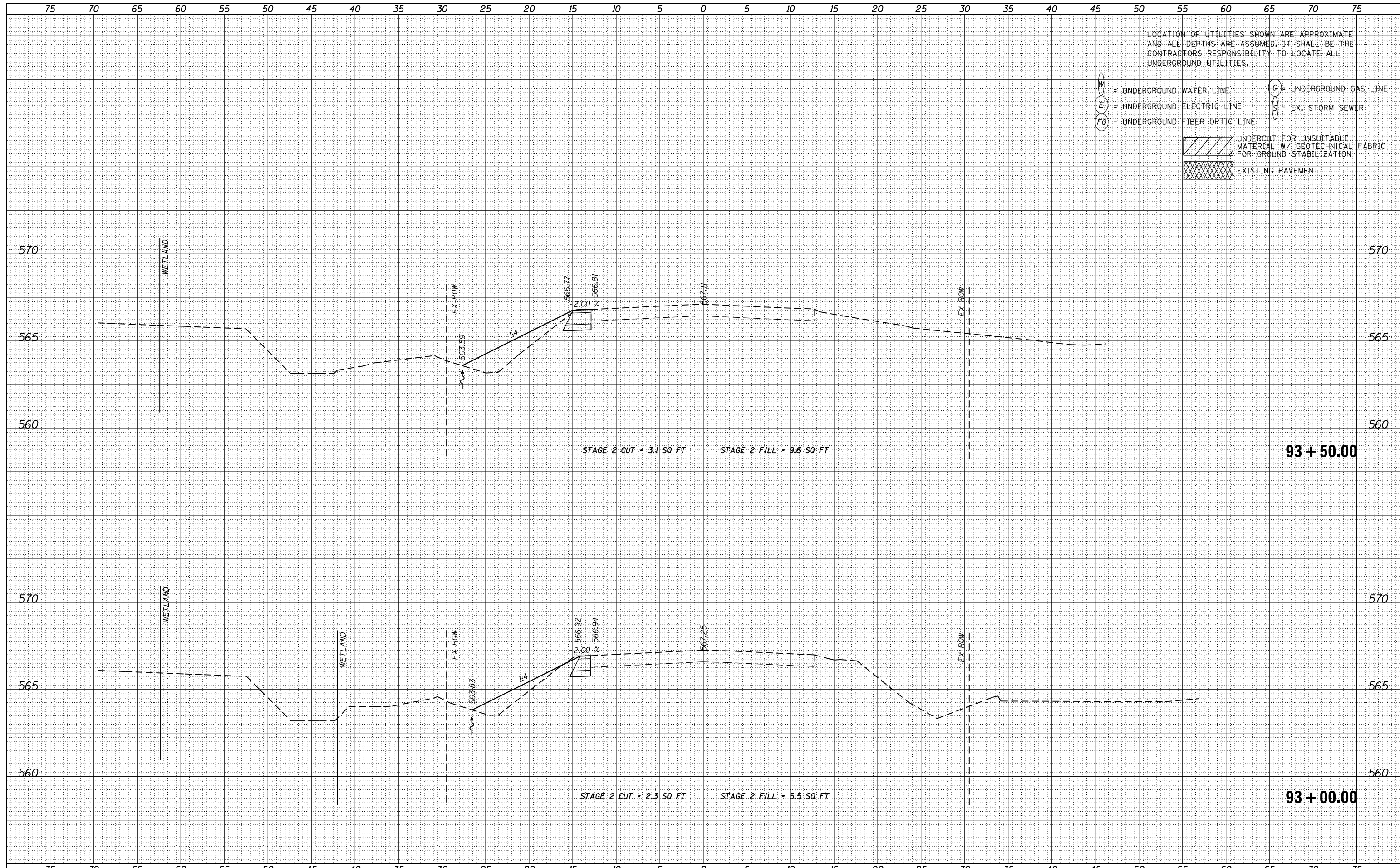
CROSS SECTIONS
IL ROUTE 53 STAGE 2
SCALE: 1"=5'4,2,5"V SHEET 1 OF 22 SHEETS STA. 92+25.69 TO STA. 92+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	139
				CONTRACT NO. 62B33

ILLINOIS FED. AID PROJECT

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

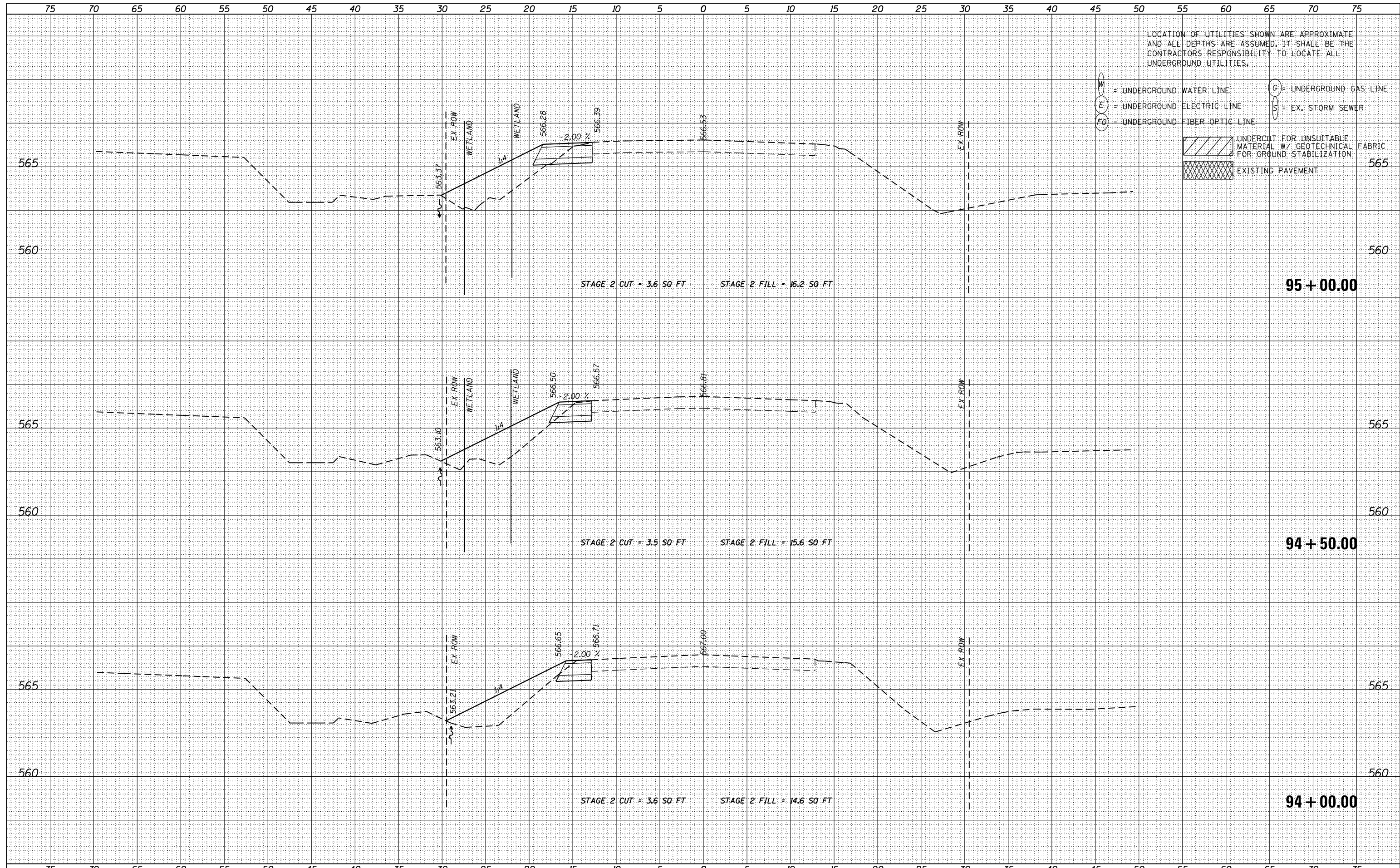
**CROSS SECTIONS
IL ROUTE 53 STAGE 2**

SCALE: 1"=5'H, 2.5'V SHEET 2 OF 22 SHEETS STA. 93+00.00 TO STA. 93+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	140
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

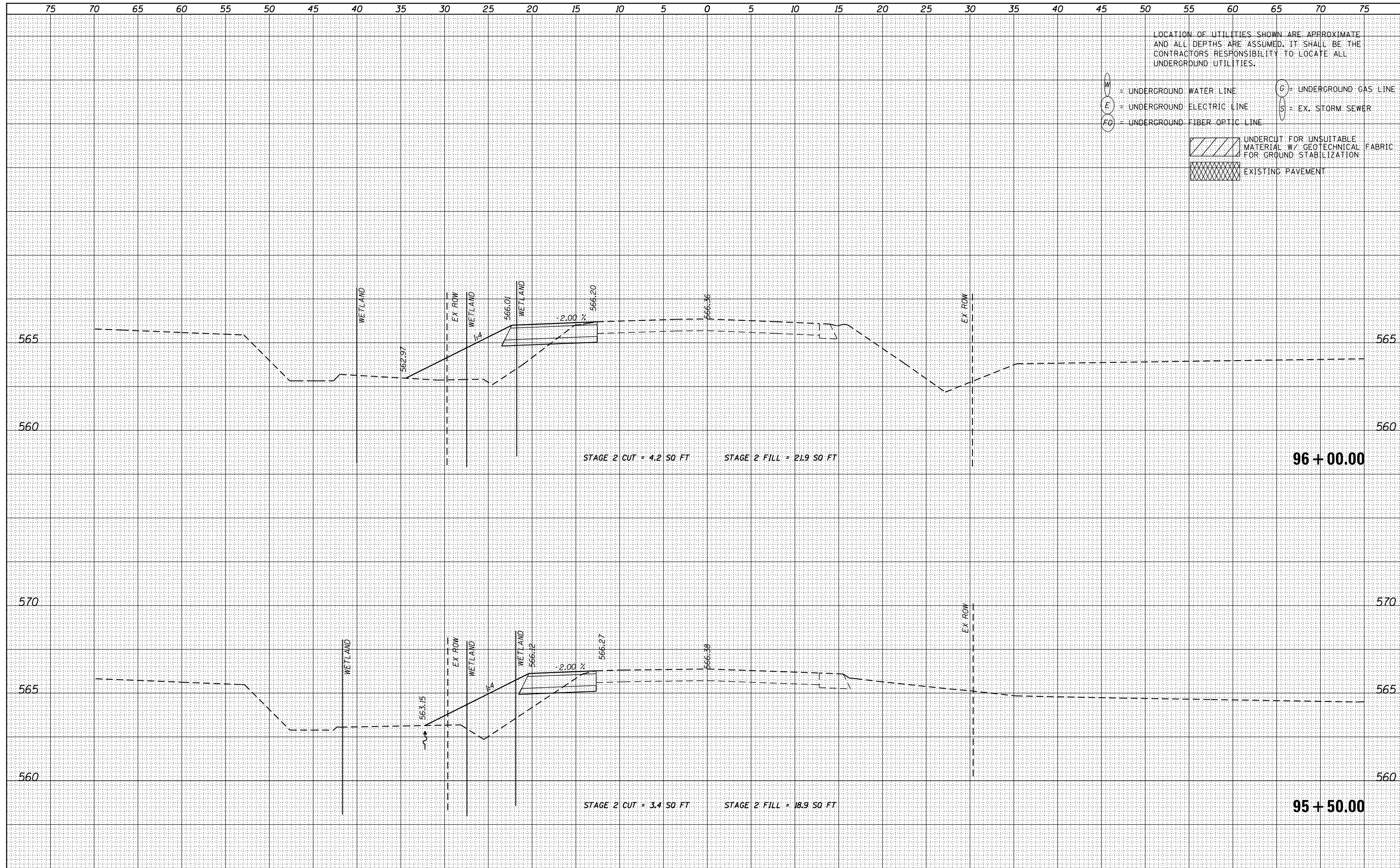
**CROSS SECTIONS
IL ROUTE 53 STAGE 2**

SCALE: 1"=5'4,2,5'V SHEET 3 OF 22 SHEETS STA. 94+00.00 TO STA. 95+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	141
			CONTRACT NO. 62B33	
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
	TEMPLATE
	AREAS CHECKED



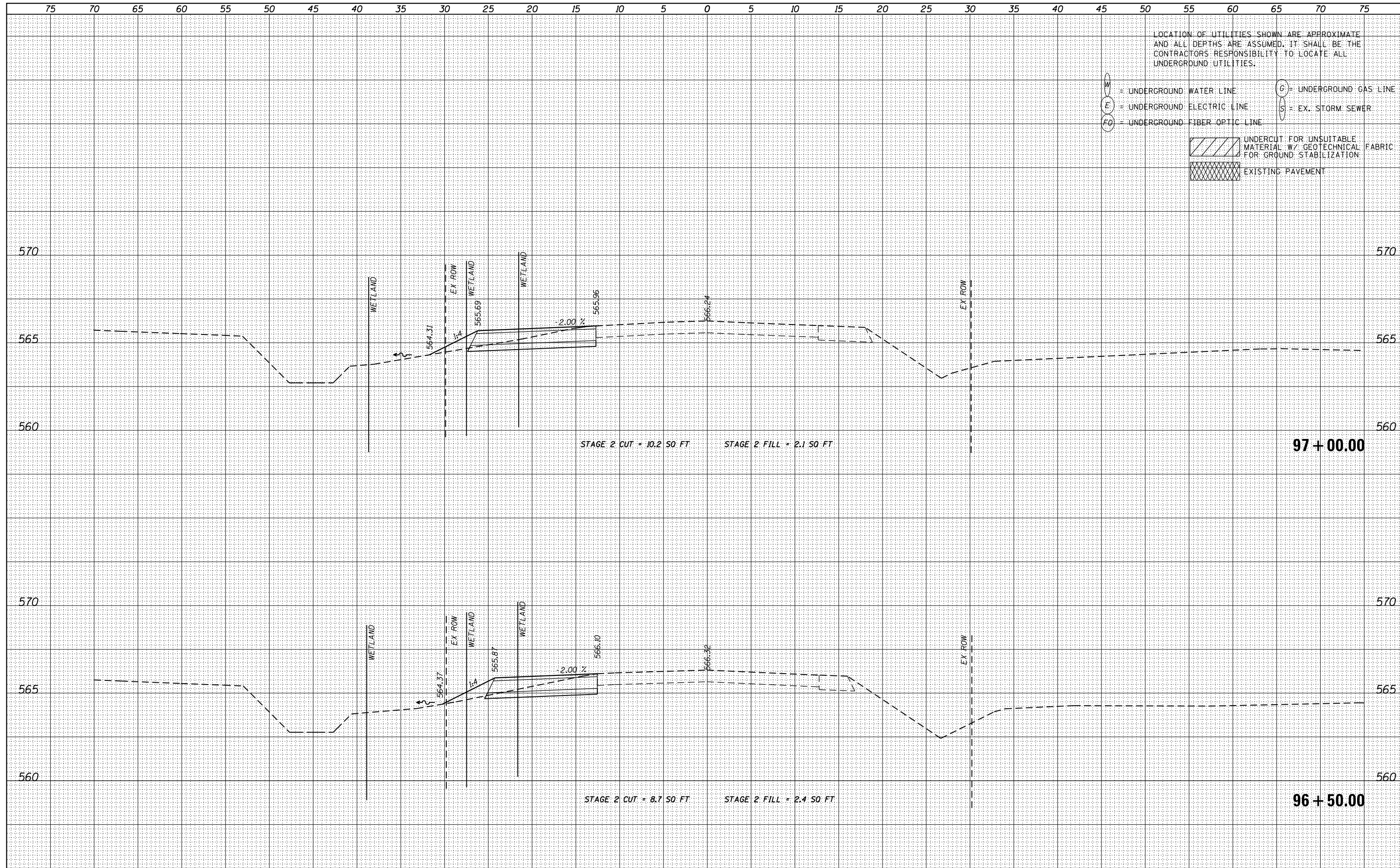
LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

W = UNDERGROUND WATER LINE
 E = UNDERGROUND ELECTRIC LINE
 FO = UNDERGROUND FIBER OPTIC LINE
 G = UNDERGROUND GAS LINE
 S = EX. STORM SEWER

[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT

DATE	
BY	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
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DATE	
BY	
ORIGINAL SURVEY	
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NOTE BOOK	
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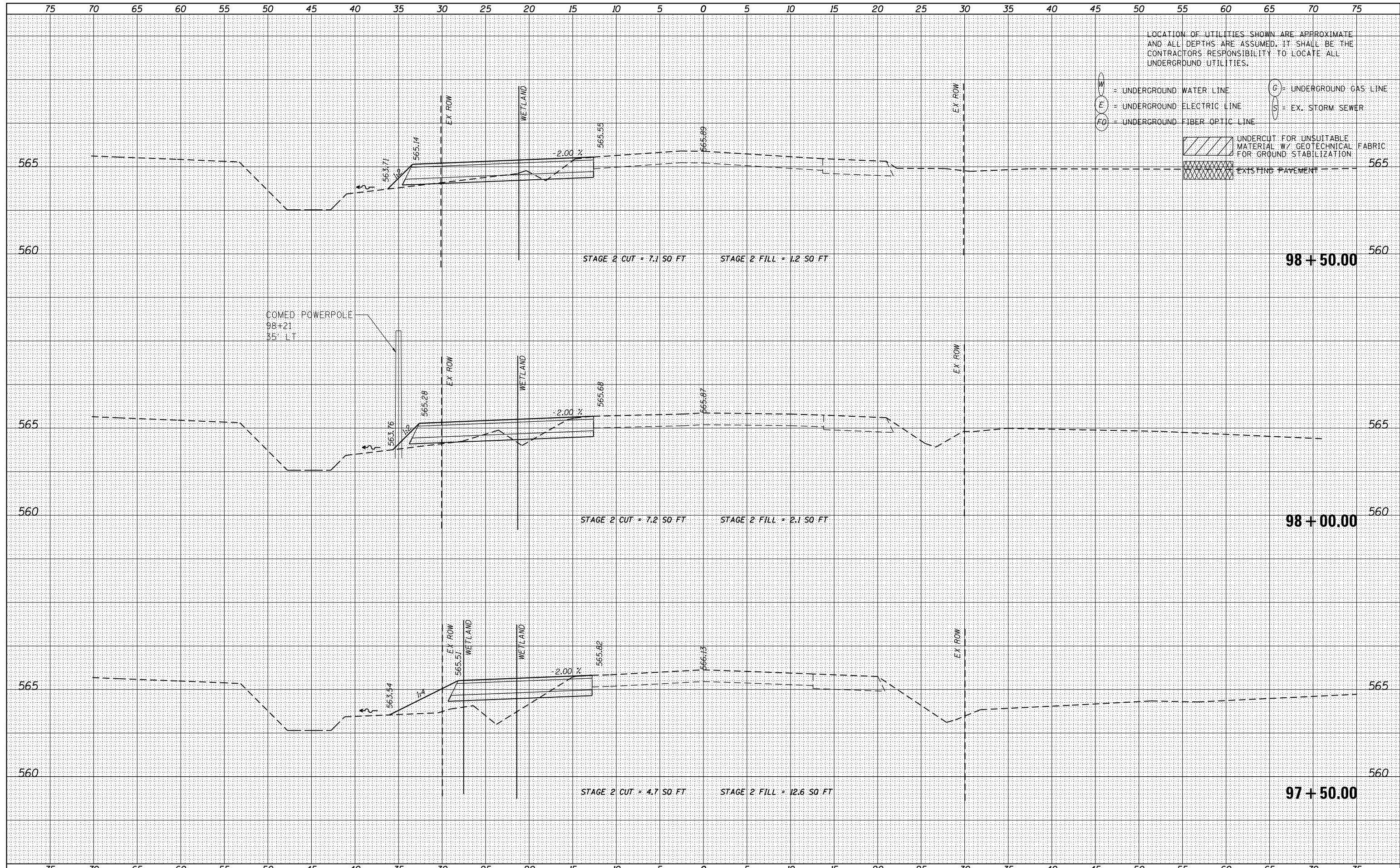


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- S = EX. STORM SEWER
- UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
FINISH SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
PLOT SCALE - 10.0000' / in.	DRAWN - BWL	REVISED -
PLOT DATE - 3/23/2016	CHECKED - NJB	REVISED -
	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
IL ROUTE 53 STAGE 2**

SCALE: 1"=5'H, 2.5'V SHEET 6 OF 22 SHEETS STA. 97+50.00 TO STA. 98+50.00

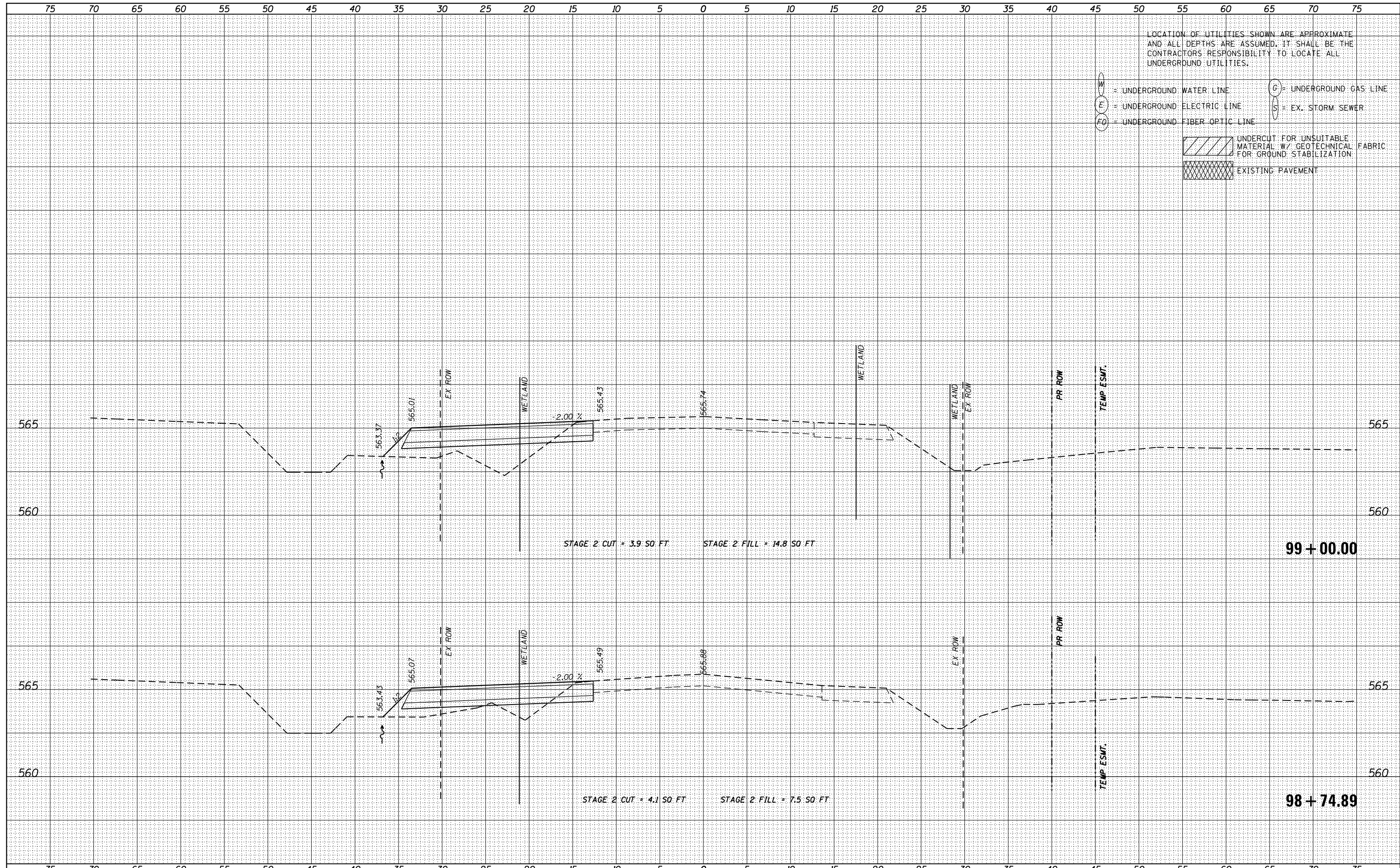
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	144
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINISH	
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DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL	
NO.	

LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

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- G = UNDERGROUND GAS LINE
- S = EX. STORM SEWER
- UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

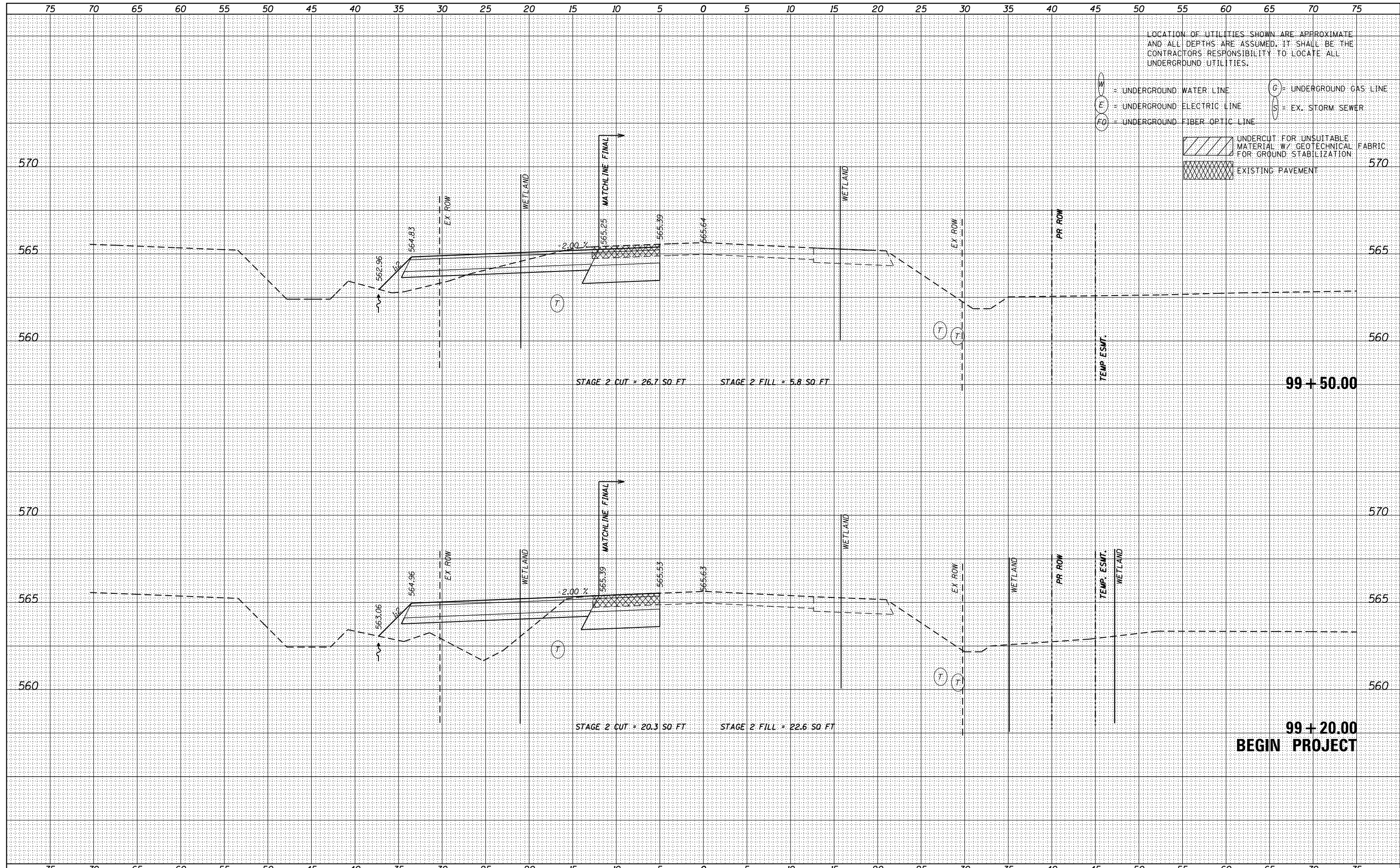
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS	
IL ROUTE 53 STAGE 2	
SCALE: 1"=5'H, 2.5'V	SHEET 7 OF 22 SHEETS
STA. 98+74.89	TO STA. 99+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	145
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

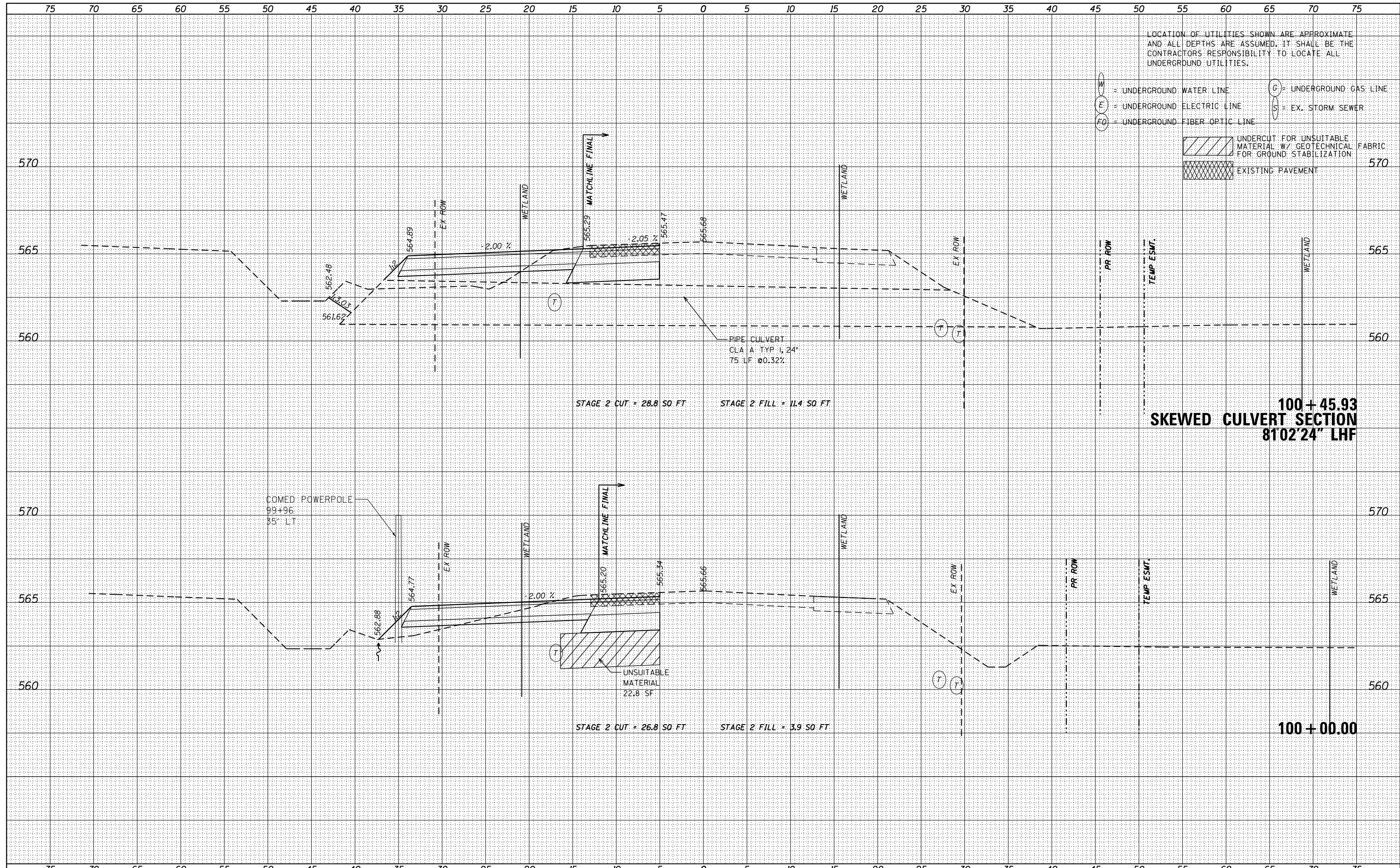
CROSS SECTIONS
IL ROUTE 53 STAGE 2
SCALE: 1"=5'4,2.5'V SHEET 8 OF 22 SHEETS STA. 99+20.00 TO STA. 99+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	146
				CONTRACT NO. 62B33

ILLINOIS FED. AID PROJECT

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

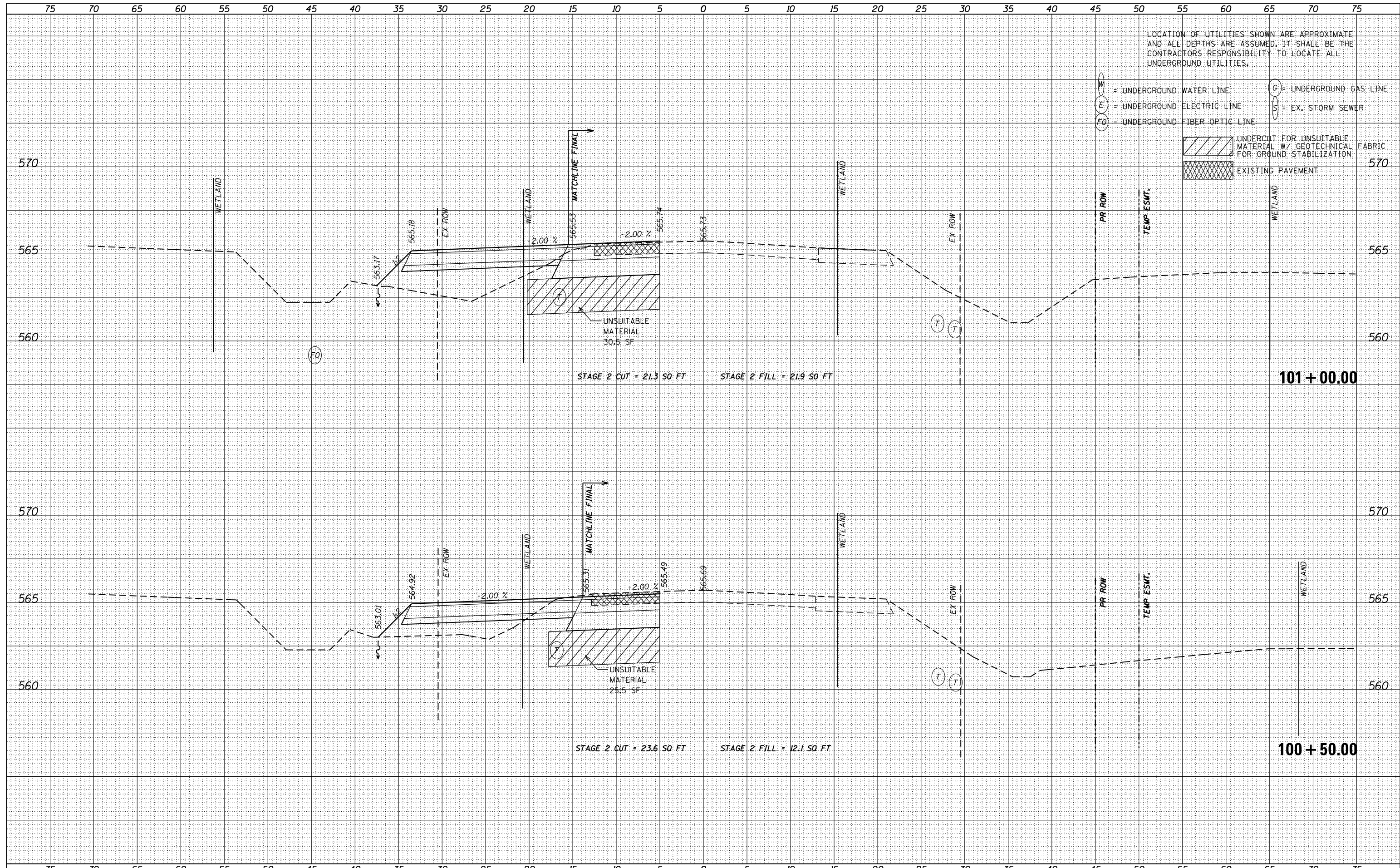
CROSS SECTIONS
IL ROUTE 53 STAGE 2

SCALE: 1"=5'4,2,5'V SHEET 9 OF 22 SHEETS STA. 100+00.00 TO STA. 100+45.93

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	147
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

W = UNDERGROUND WATER LINE
 E = UNDERGROUND ELECTRIC LINE
 FO = UNDERGROUND FIBER OPTIC LINE
 G = UNDERGROUND GAS LINE
 S = EX. STORM SEWER

[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

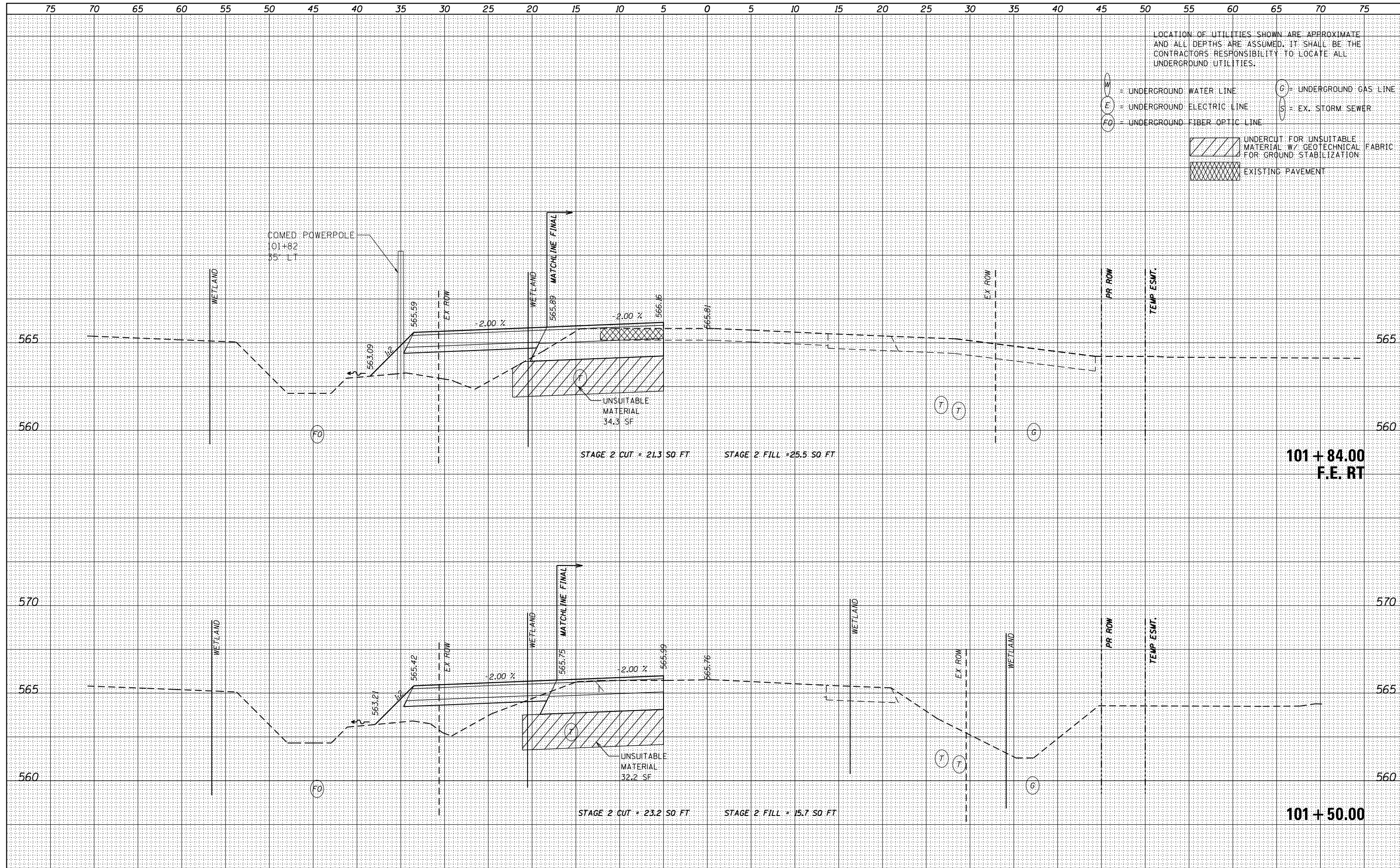
CROSS SECTIONS
IL ROUTE 53 STAGE 2

SCALE: 1"=5'H, 2.5'V SHEET 10 OF 22 SHEETS STA. 100+50.00 TO STA. 101+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	148
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



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- S = EX. STORM SEWER
- UNDCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

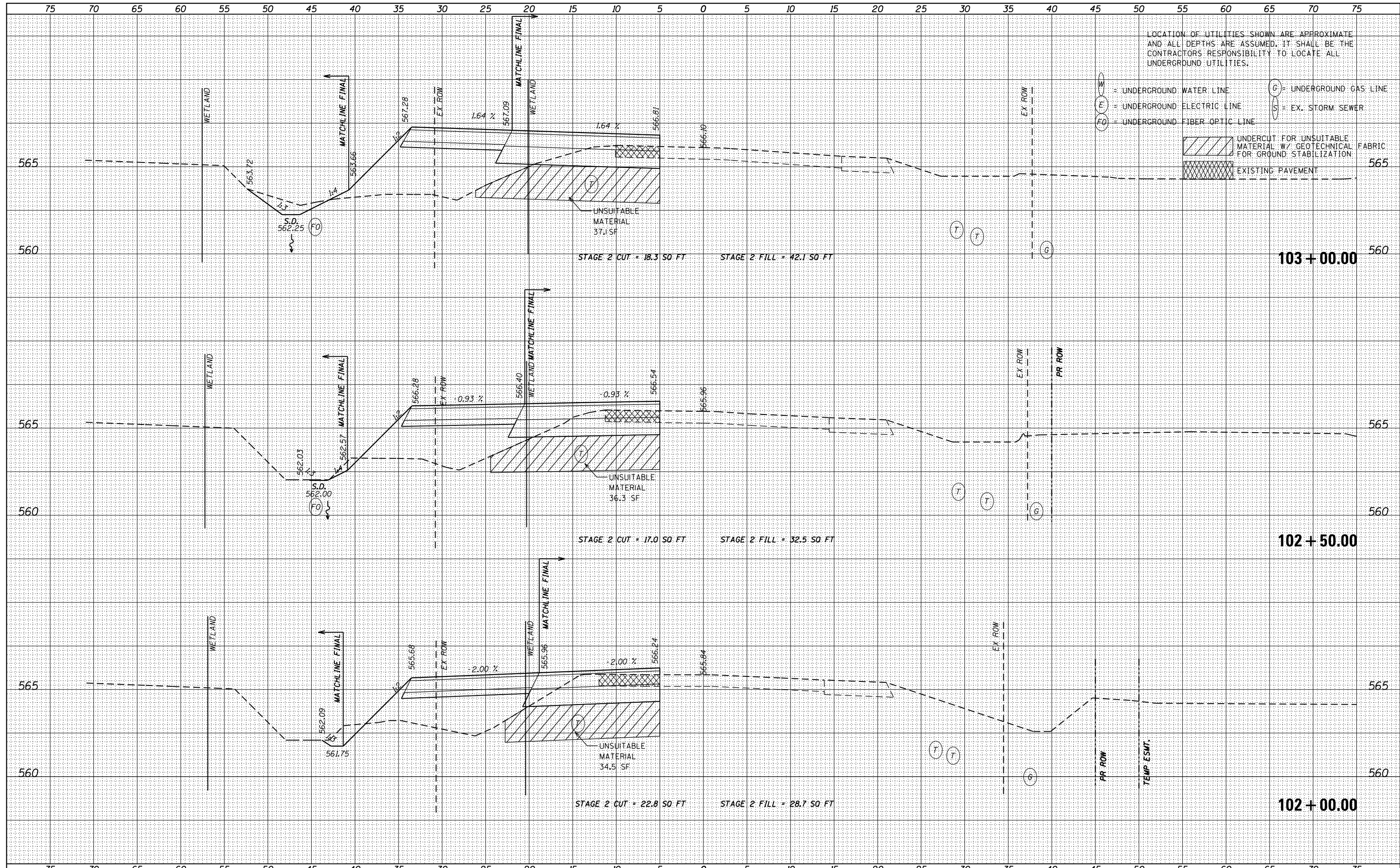
CROSS SECTIONS
IL ROUTE 53 STAGE 2
SCALE: 1"=5'4,2,5"V SHEET 11 OF 22 SHEETS STA. 101+50.00 TO STA.101+84.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	149
				CONTRACT NO. 62B33

ILLINOIS FED. AID PROJECT

DATE	
BY	
NO.	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	
FINAL SURVEY	
NOTE BOOK	
AREAS	
CHECKED	

DATE	
BY	
NO.	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	
FINAL SURVEY	
NOTE BOOK	
AREAS	
CHECKED	



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

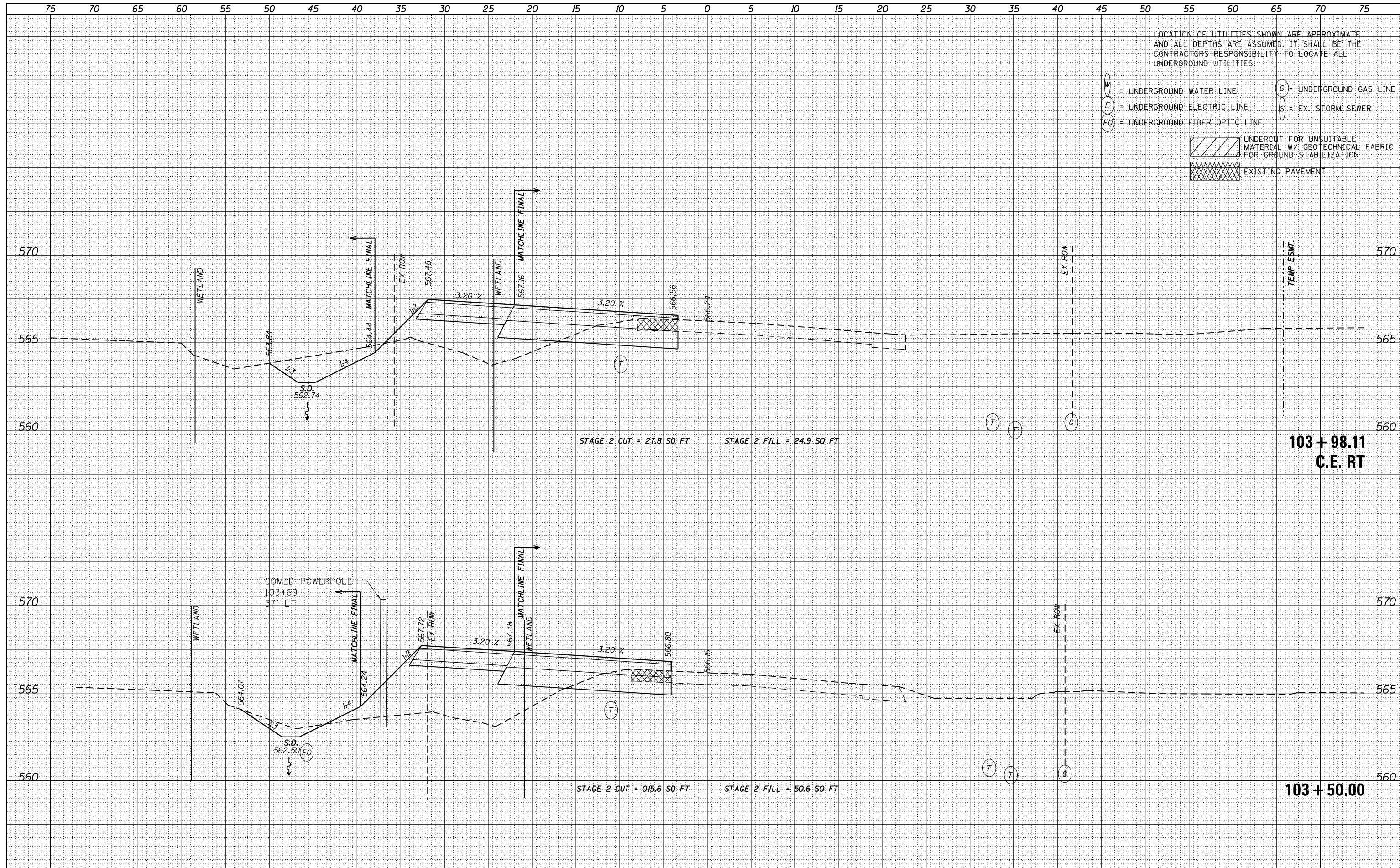
CROSS SECTIONS
IL ROUTE 53 STAGE 2

SCALE: 1"=5'H, 2.5'V SHEET 12 OF 22 SHEETS STA. 102+00.00 TO STA. 103+00.00

F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 150
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



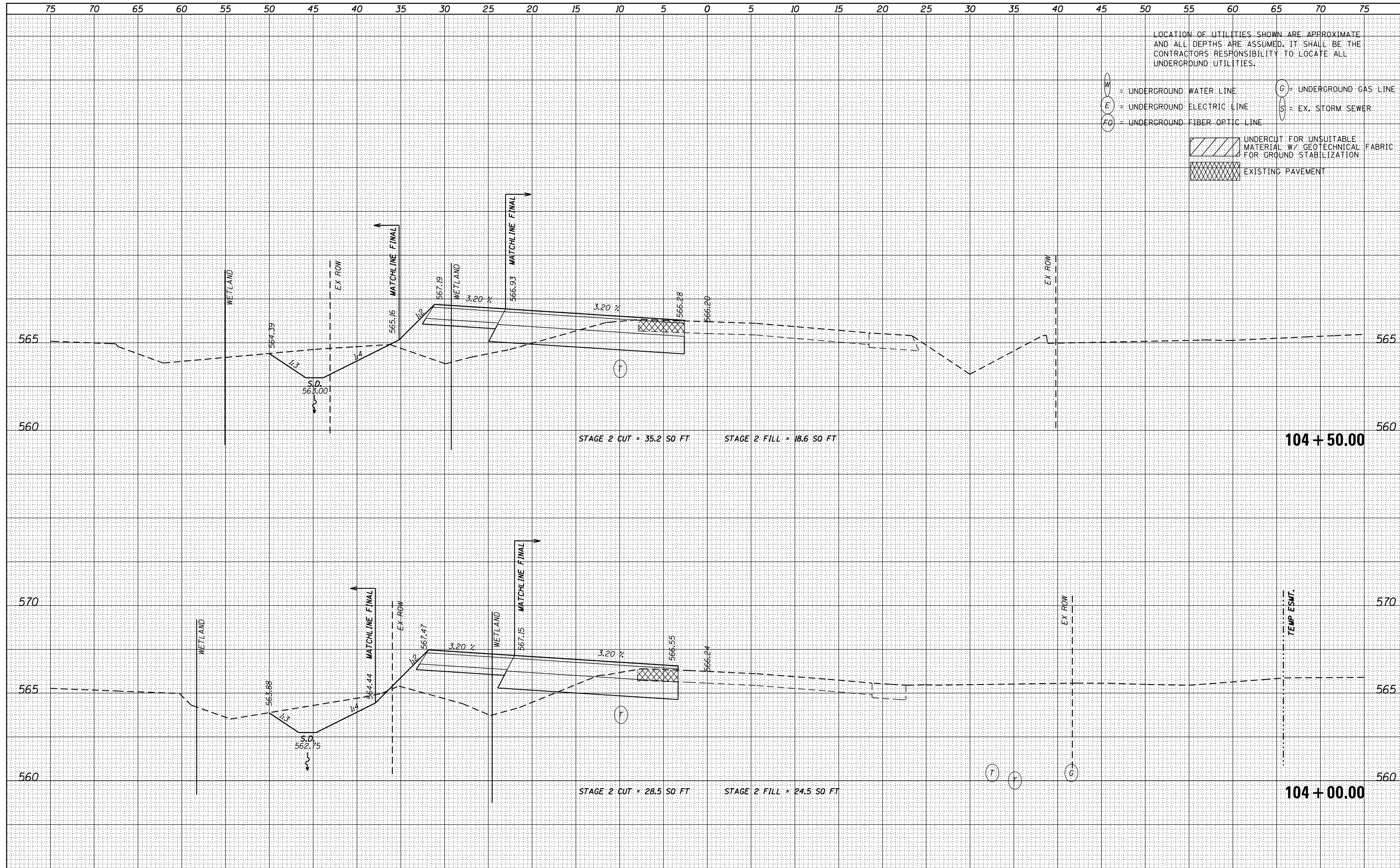
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 G = UNDERGROUND GAS LINE
 S = EX. STORM SEWER

[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT

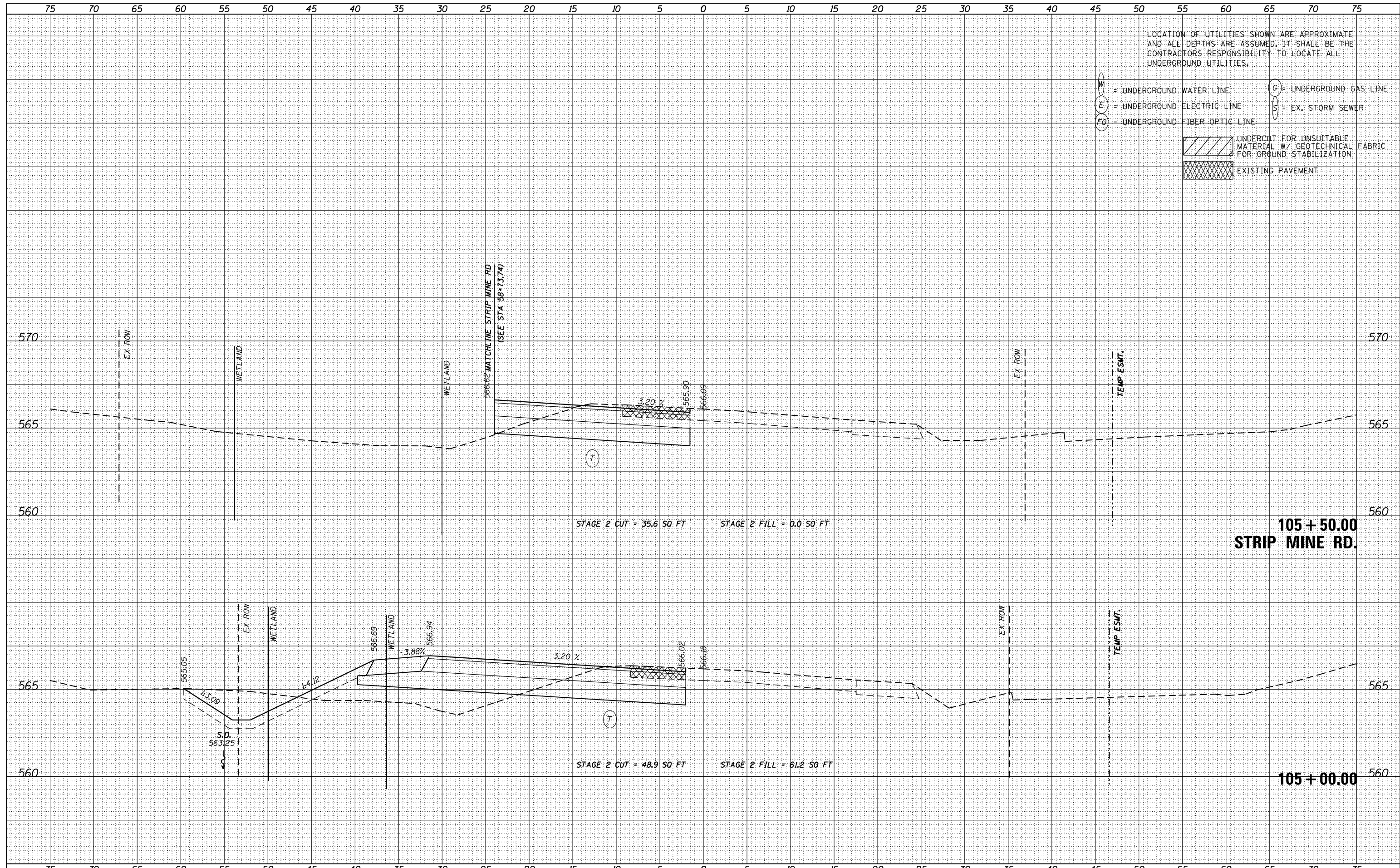
DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
	TEMPLATE
	AREAS CHECKED



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

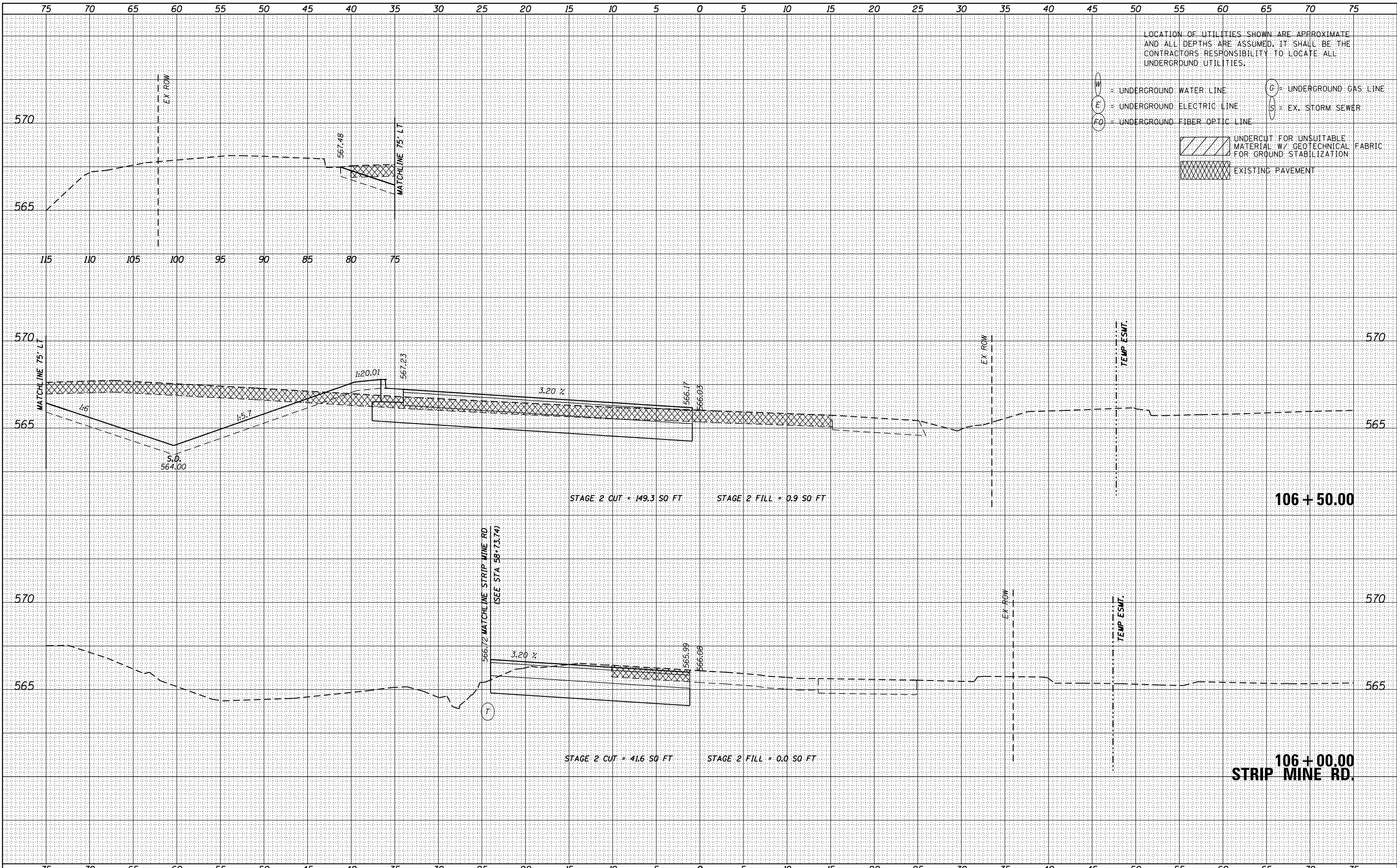
CROSS SECTIONS
IL ROUTE 53 STAGE 2

SCALE: 1"=5'4,2.5'V SHEET 15 OF 22 SHEETS STA. 105+00.00 TO STA.105+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	153
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



USER NAME = nick.becker	DESIGNED - JLM	REVISÉD -
	DRAWN - BWL	REVISÉD -
PLOT SCALE = 10.0000' / 1"	CHECKED - NJB	REVISÉD -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISÉD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
IL ROUTE 53 STAGE 2**
SCALE: 1"=5'H, 2.5'V SHEET 16 OF 22 SHEETS STA. 106+00.00 TO STA. 106+50.00

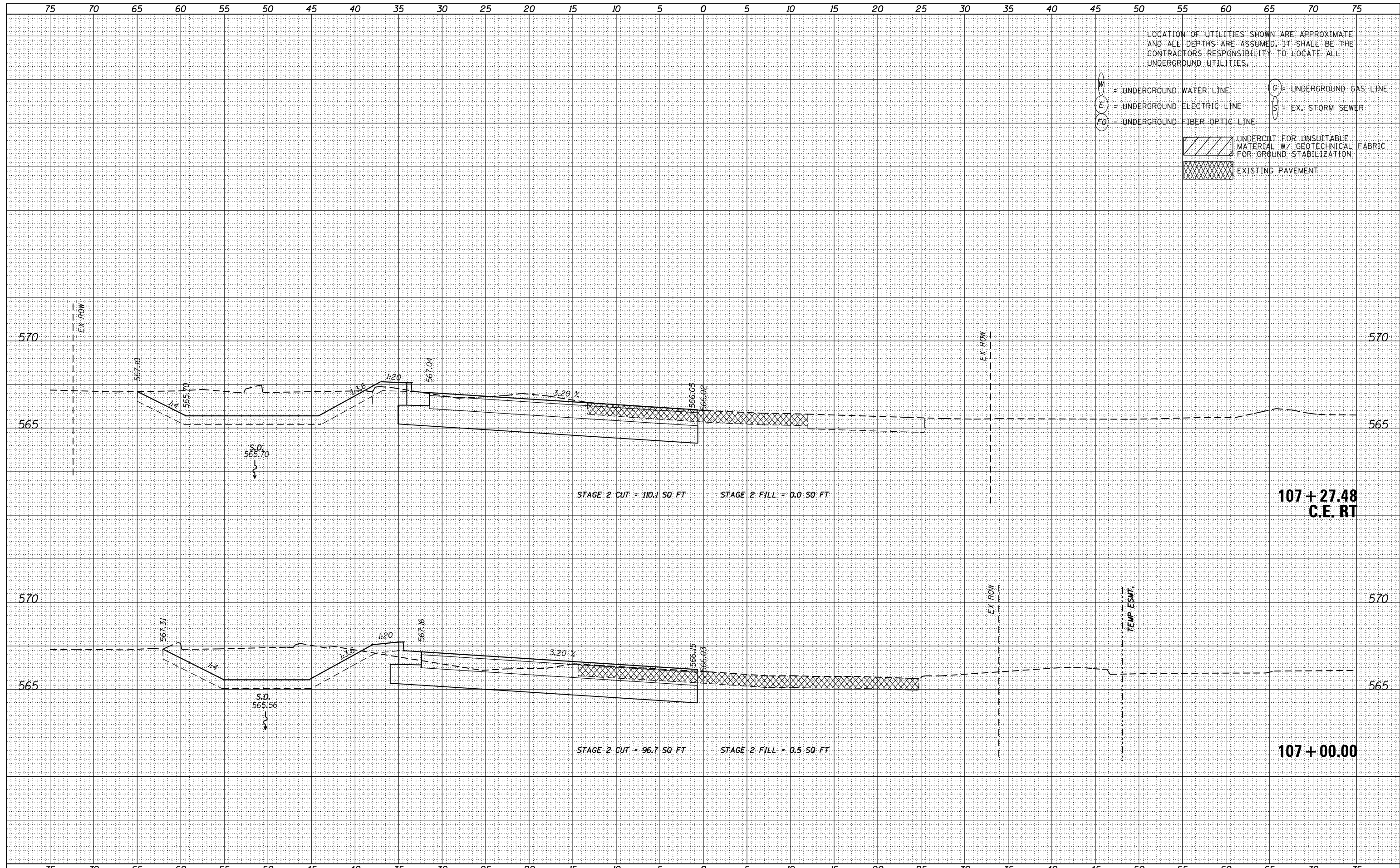
F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 154
			CONTRACT NO. 62B33	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
FINISH SURVEY	
NOTE BOOK	
NO.	

DATE	
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SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	

LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

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- EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
PLOT SCALE - 10.0000' / in.	DRAWN - BWL	REVISED -
PLOT DATE - 3/23/2016	CHECKED - NJB	REVISED -
	DATE - 02/05/2016	REVISED -

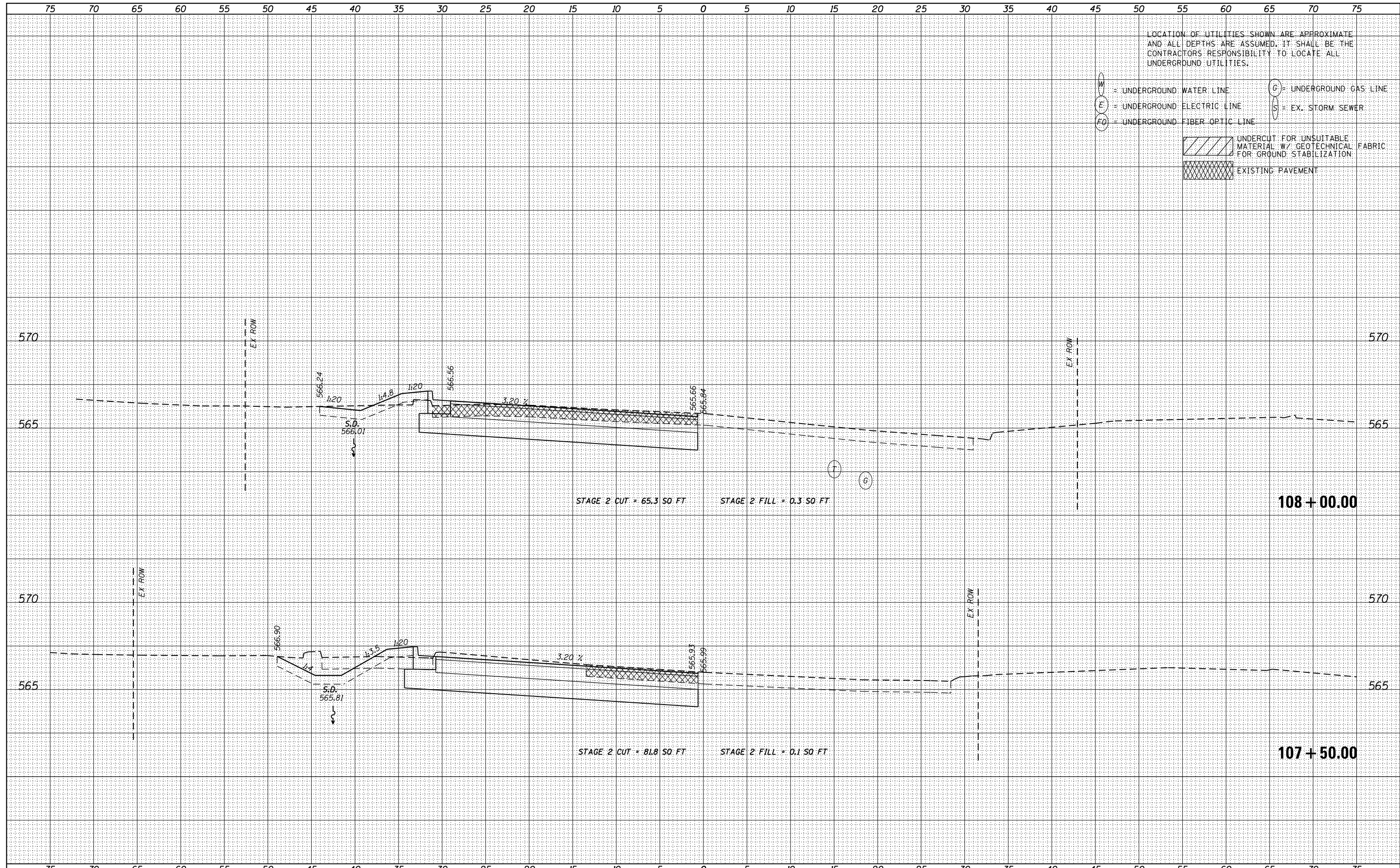
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 2
SCALE: 1"=5'H, 2.5'V SHEET 17 OF 22 SHEETS STA. 107+00.00 TO STA. 107+27.48

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	155
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
FINISH SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



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- EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

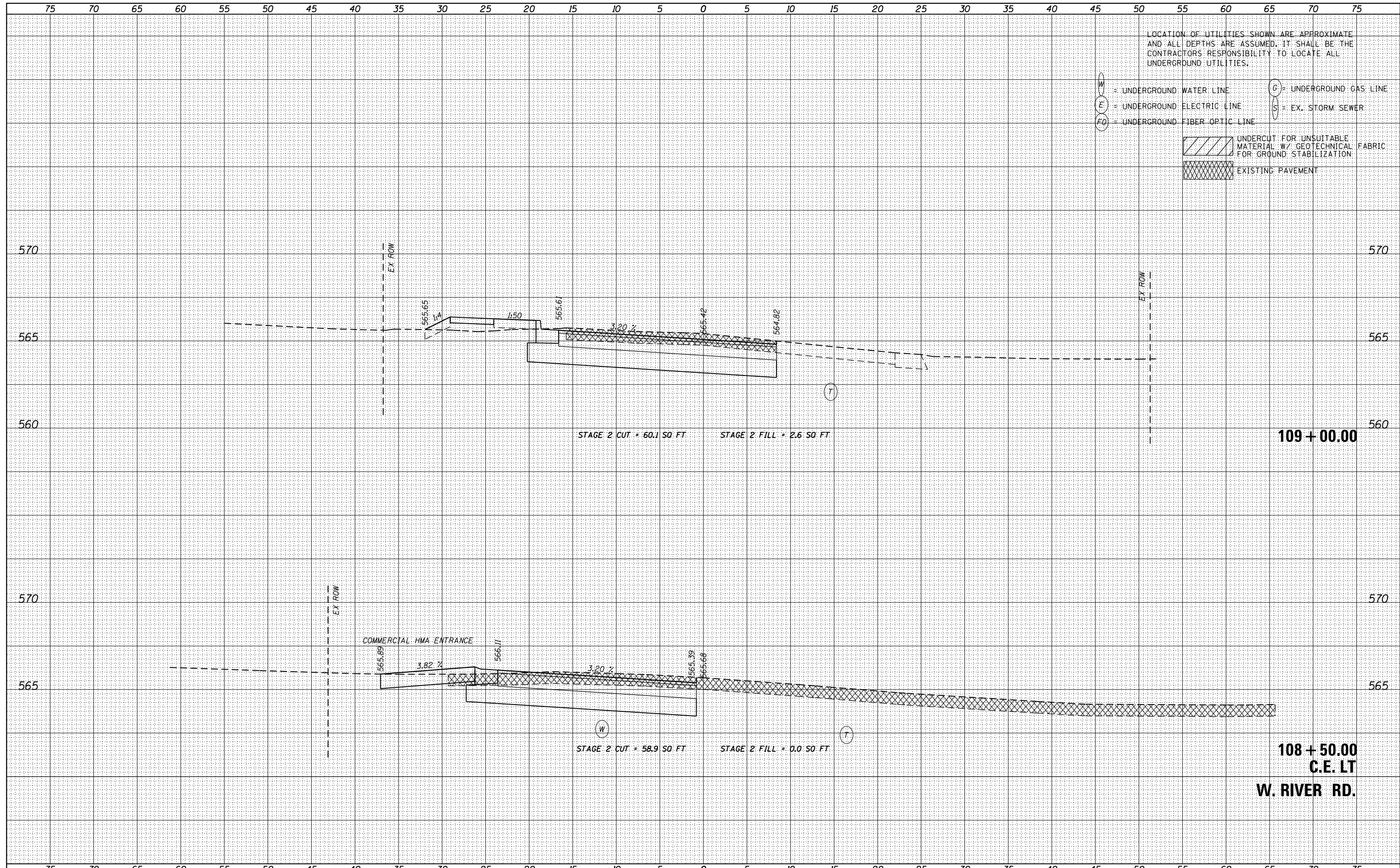
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 2
SCALE: 1"=5'4,2.5'V SHEET 18 OF 22 SHEETS STA. 107+50.00 TO STA.108+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	156
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
IL ROUTE 53 STAGE 2**

SCALE: 1"=5'4,2,5"V SHEET 19 OF 22 SHEETS STA. 108+50.00 TO STA. 109+00.00

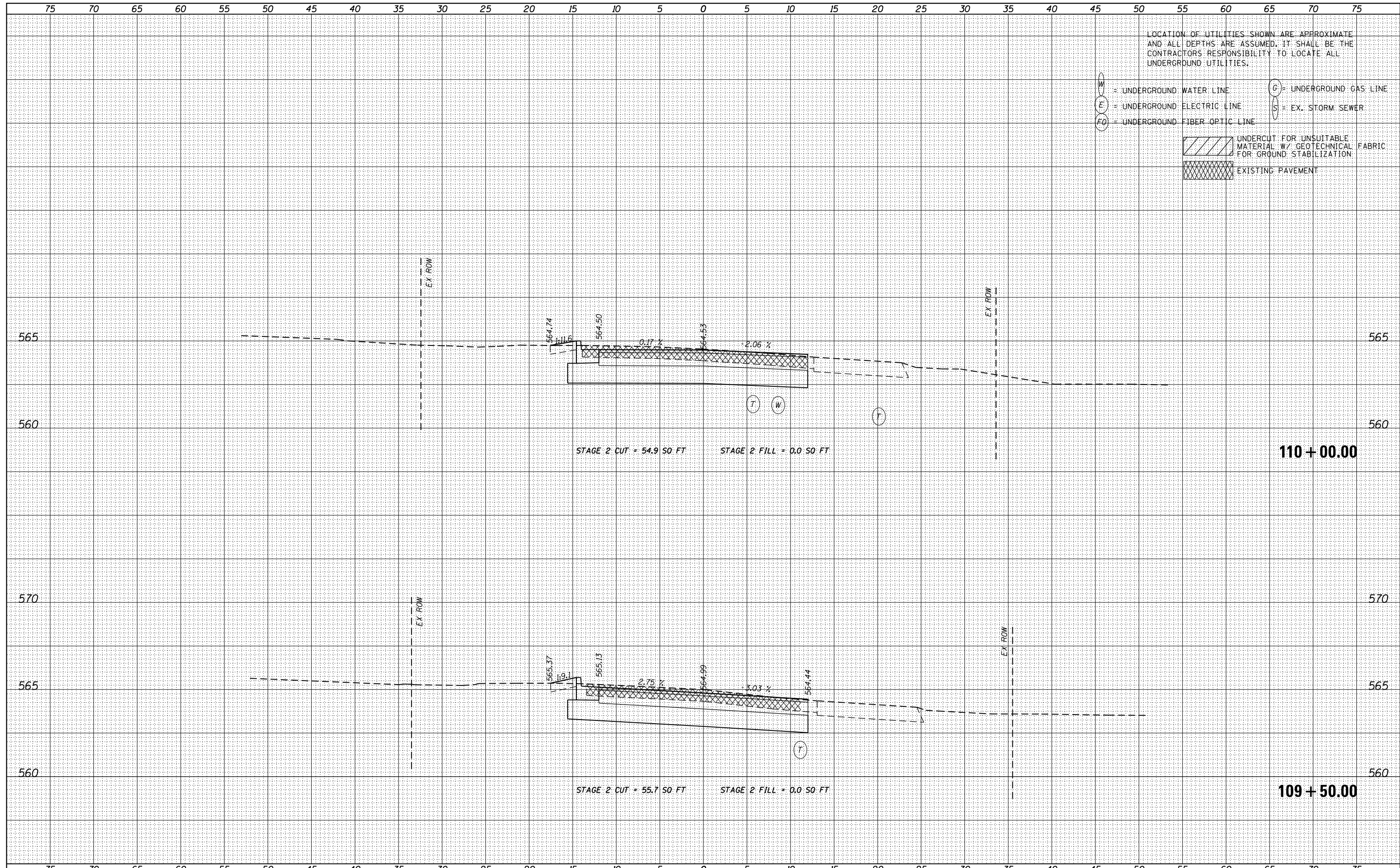
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	157
			CONTRACT NO. 62B33	
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

- (W) = UNDERGROUND WATER LINE
- (E) = UNDERGROUND ELECTRIC LINE
- (FO) = UNDERGROUND FIBER OPTIC LINE
- (G) = UNDERGROUND GAS LINE
- (S) = EX. STORM SEWER
- [Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- [Cross-hatched Box] EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

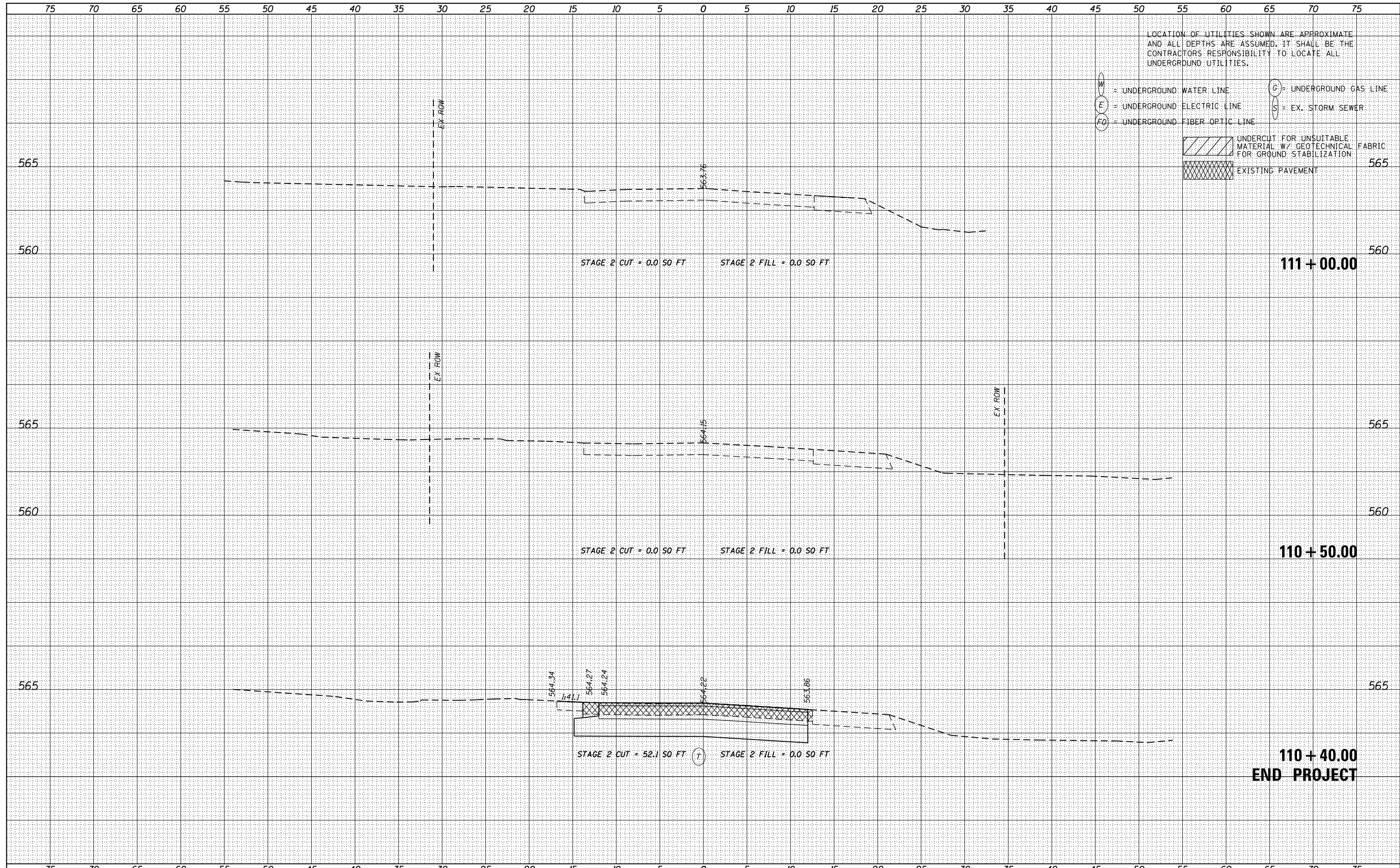
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS	
IL ROUTE 53 STAGE 2	
SCALE: 1"=5'H, 2.5'V	SHEET 20 OF 22 SHEETS
STA. 109+50.00	TO STA. 110+00.00

F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 158
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
PLOT SCALE - 10.0000' / in.	DRAWN - BWL	REVISED -
PLOT DATE - 3/23/2016	CHECKED - NJB	REVISED -
	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

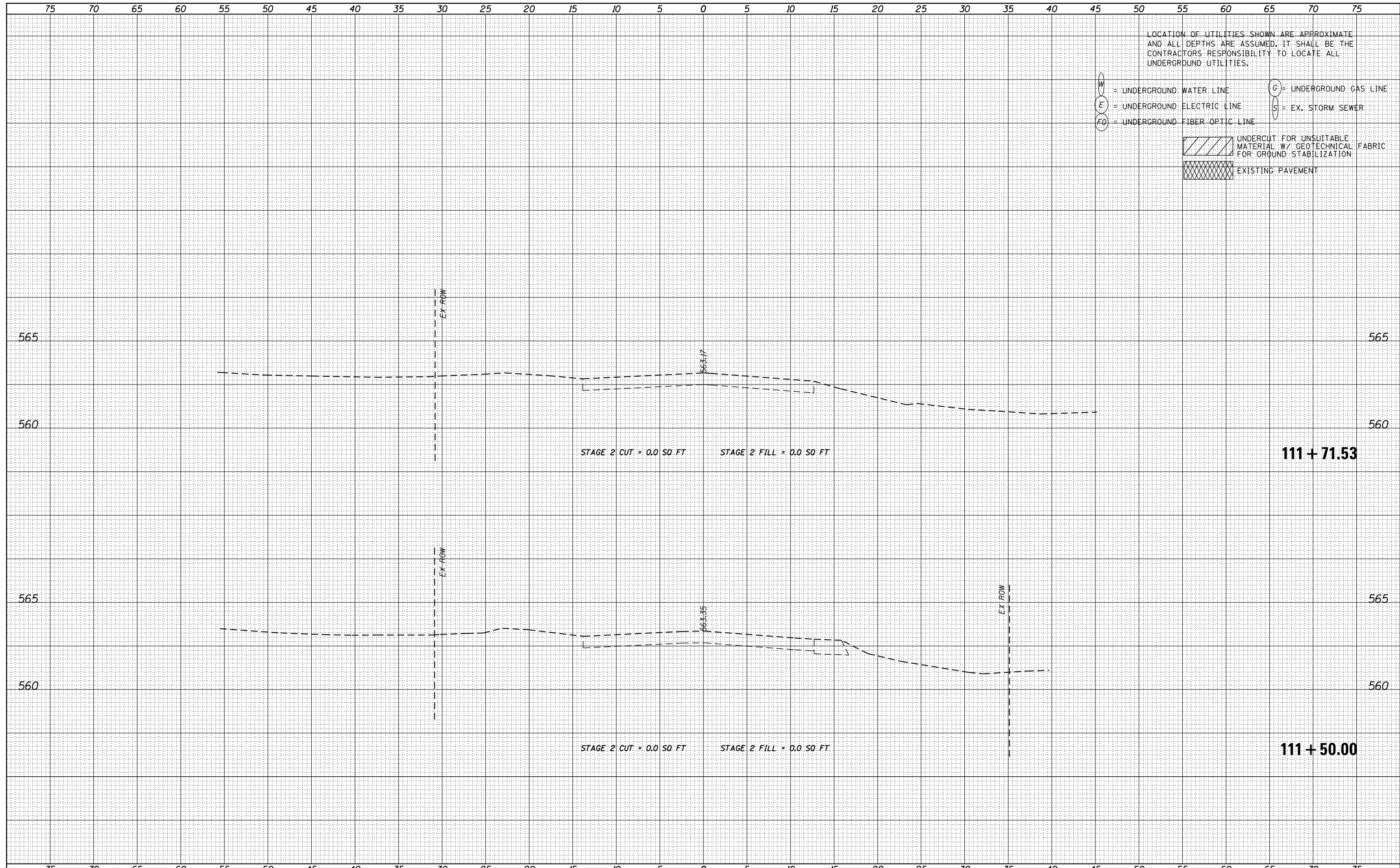
**CROSS SECTIONS
IL ROUTE 53 STAGE 2**

SCALE: 1"=5'4,2,5"V SHEET 21 OF 22 SHEETS STA. 110+40.00 TO STA. 111+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	159
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
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DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

- W = UNDERGROUND WATER LINE
- E = UNDERGROUND ELECTRIC LINE
- FO = UNDERGROUND FIBER OPTIC LINE
- G = UNDERGROUND GAS LINE
- S = EX. STORM SEWER
- UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

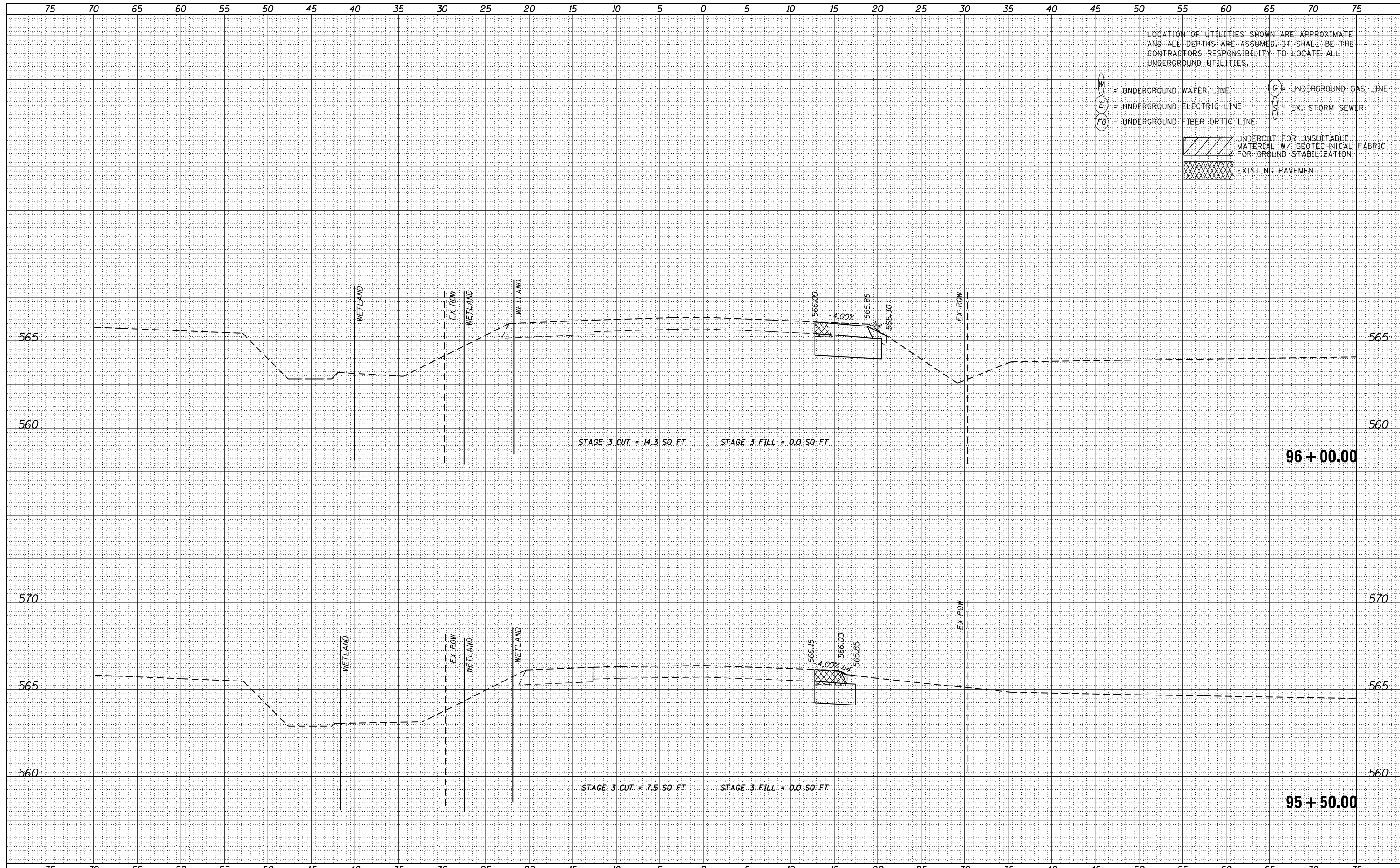
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 2
SCALE: 1"=5'H, 2.5'V SHEET 22 OF 22 SHEETS STA. 111+50.00 TO STA. 111+71.53

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	160
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
	TEMPLATE
	AREAS CHECKED



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- G = UNDERGROUND GAS LINE
- S = EX. STORM SEWER
- UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 3

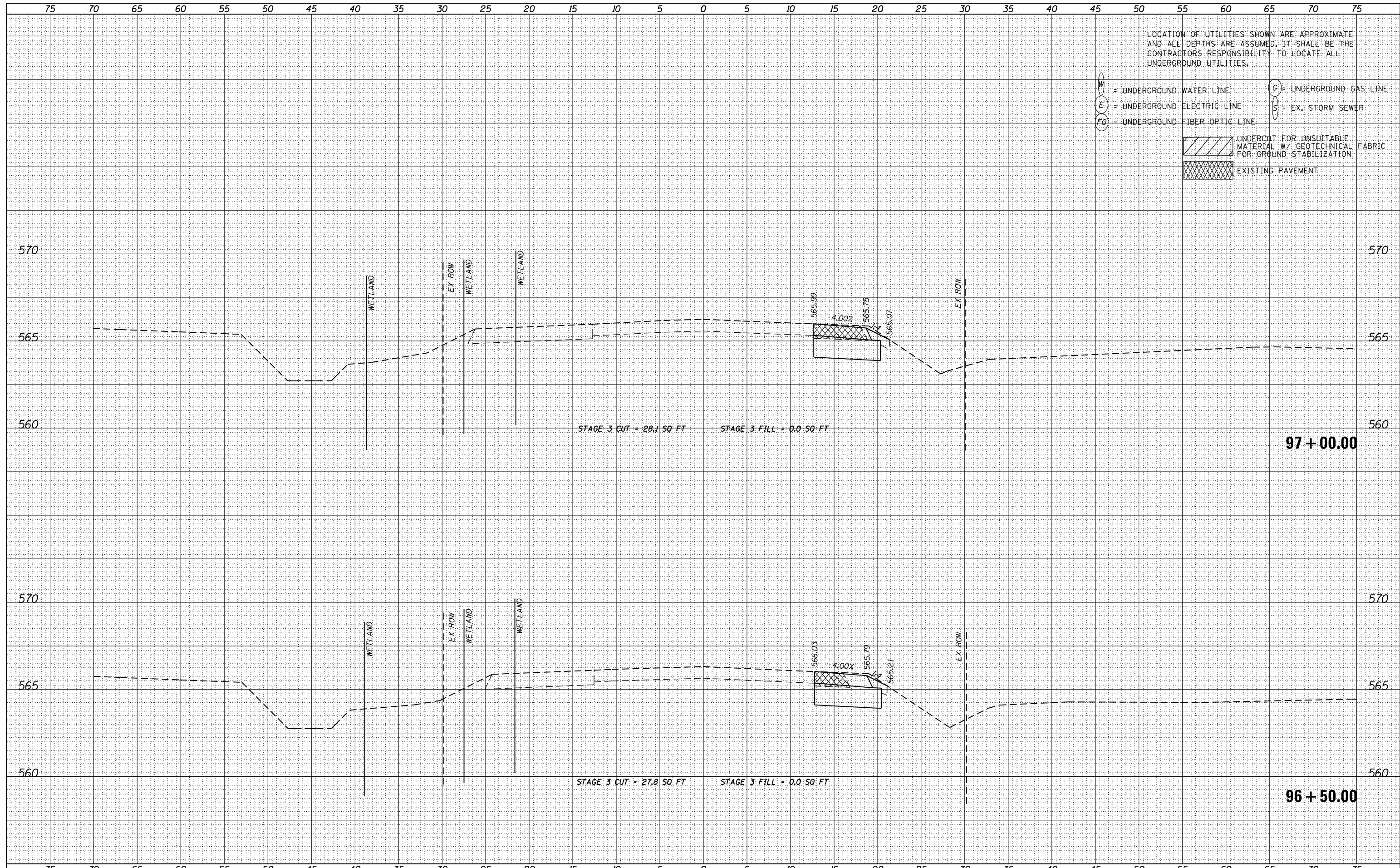
SCALE: 1"=5'H, 2.5'V SHEET 1 OF 19 SHEETS STA. 95+50.00 TO STA. 96+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	161
				CONTRACT NO. 62B33

ILLINOIS FED. AID PROJECT

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
	TEMPLATE
	AREAS CHECKED



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- E = UNDERGROUND ELECTRIC LINE
- FO = UNDERGROUND FIBER OPTIC LINE
- G = UNDERGROUND GAS LINE
- S = EX. STORM SEWER
- UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

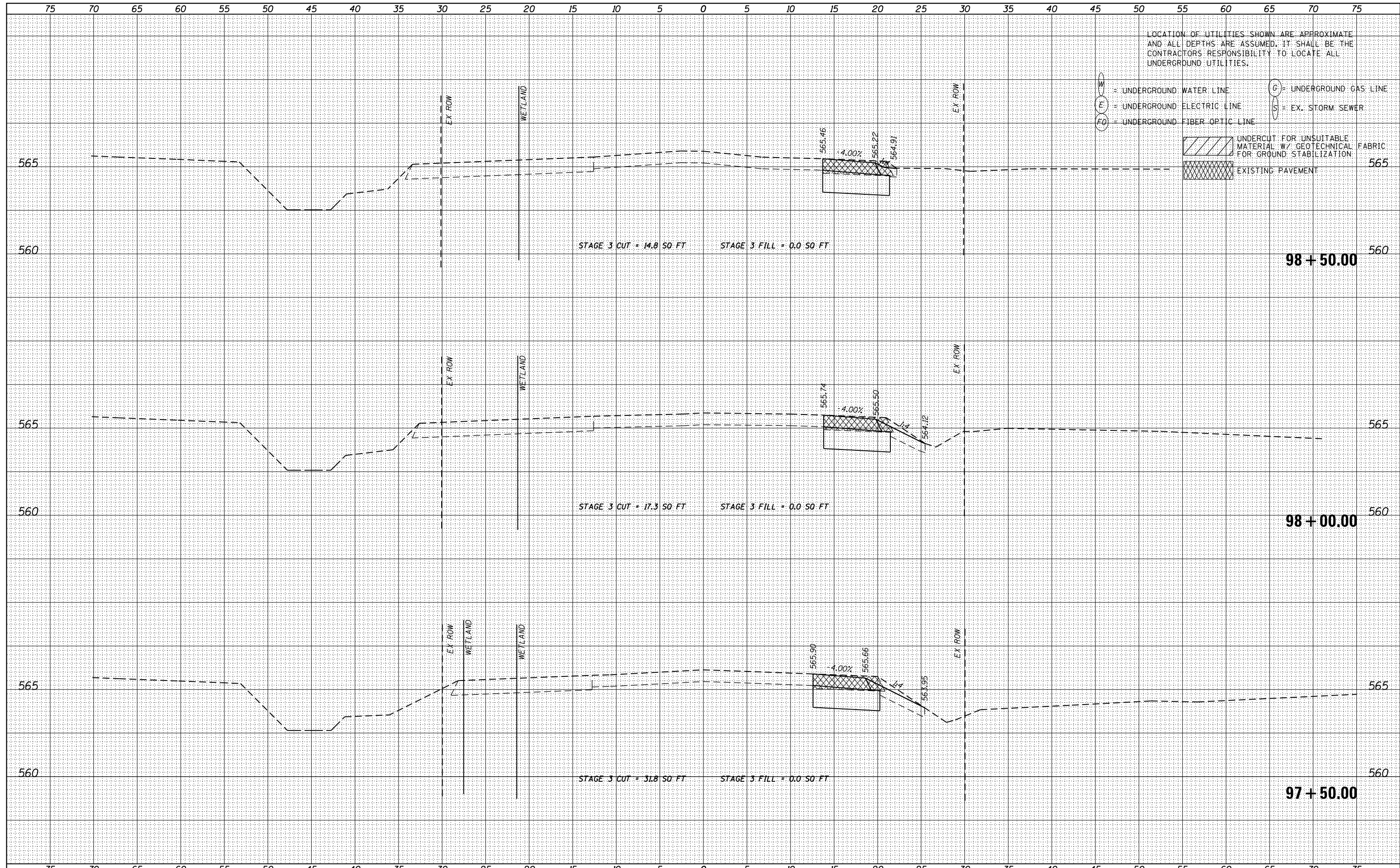
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 3
SCALE: 1"=5'H, 2.5"V
SHEET 2 OF 19 SHEETS
STA. 96+50.00 TO STA. 97+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	162
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	

DATE	
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SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
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DATE	
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SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
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LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

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 - G = UNDERGROUND GAS LINE
 - S = EX. STORM SEWER
- UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 3

SCALE: 1"=5'4,2,5"V SHEET 3 OF 19 SHEETS STA. 97+50.00 TO STA. 98+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	163
				CONTRACT NO. 62B33

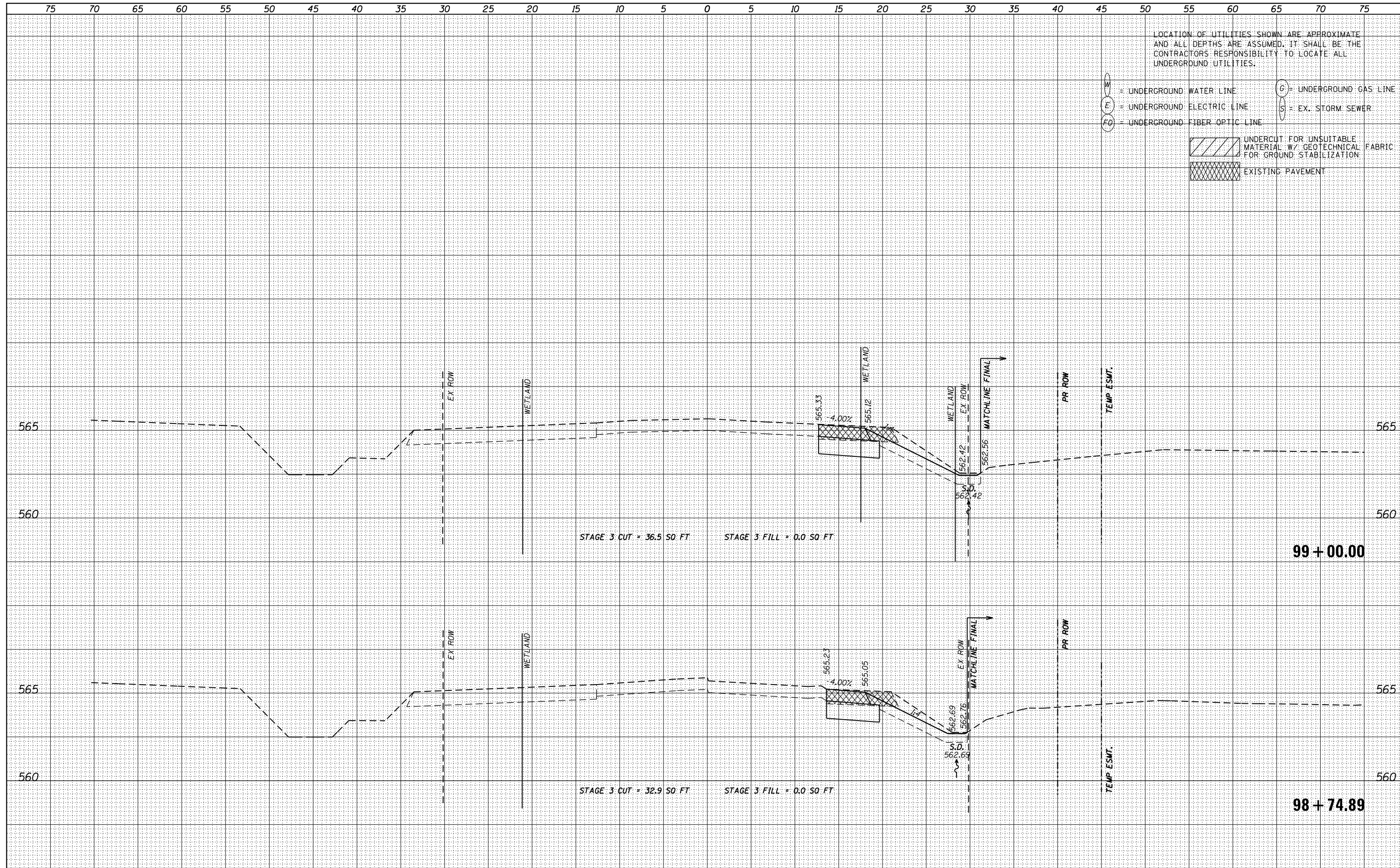
ILLINOIS FED. AID PROJECT

DATE	
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PLOTTED	
TEMPLATE	
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CHECKED	
FINAL SURVEY	
NOTE BOOK	
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ORIGINAL SURVEY	
NOTE BOOK	
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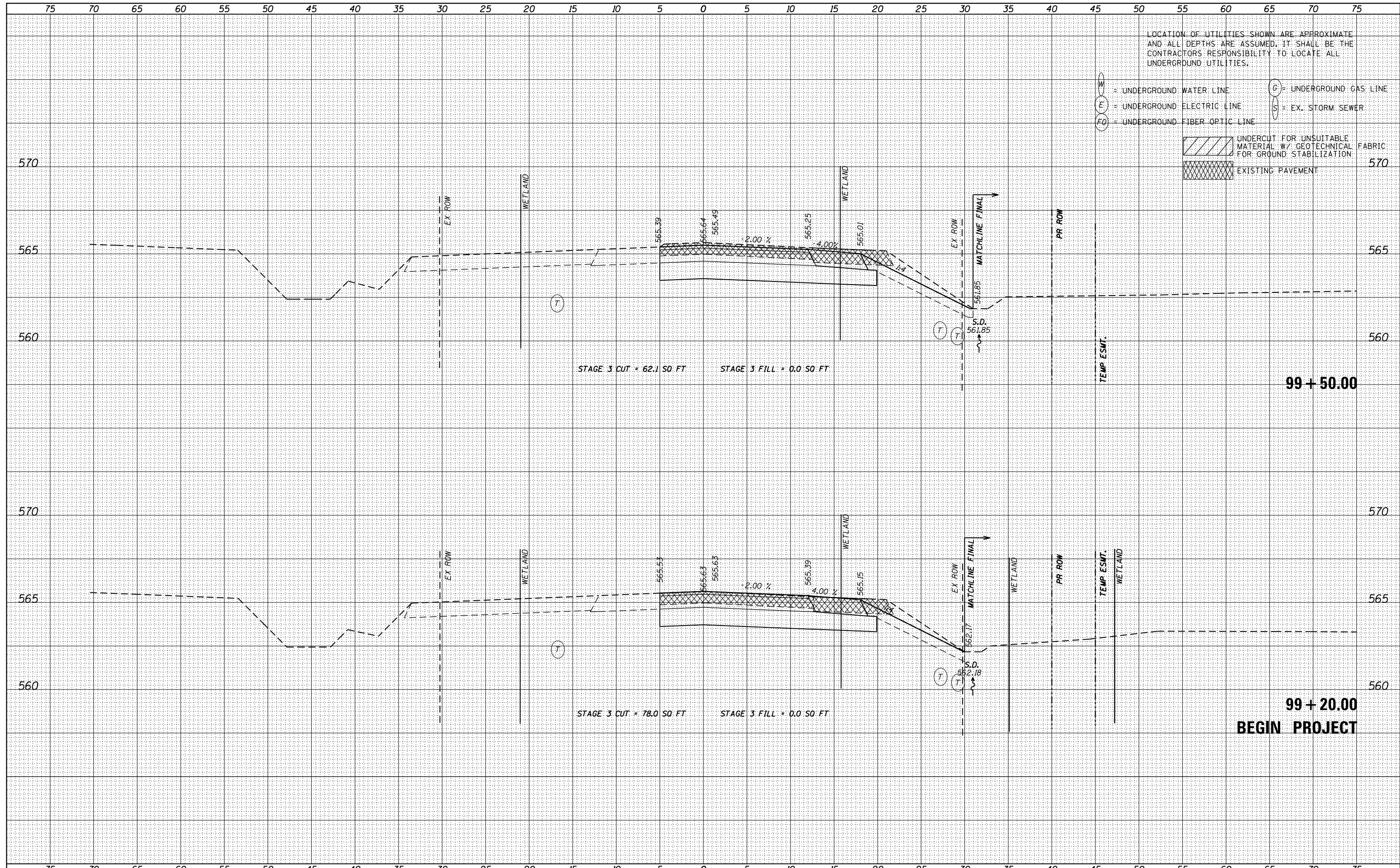
LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

W = UNDERGROUND WATER LINE
 E = UNDERGROUND ELECTRIC LINE
 FO = UNDERGROUND FIBER OPTIC LINE
 G = UNDERGROUND GAS LINE
 S = EX. STORM SEWER
 [Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT



DATE	
BY	
FINAL SURVEY	
SURVEY PLOTTED	
NOTE BOOK	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	



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- G = UNDERGROUND GAS LINE
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- UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

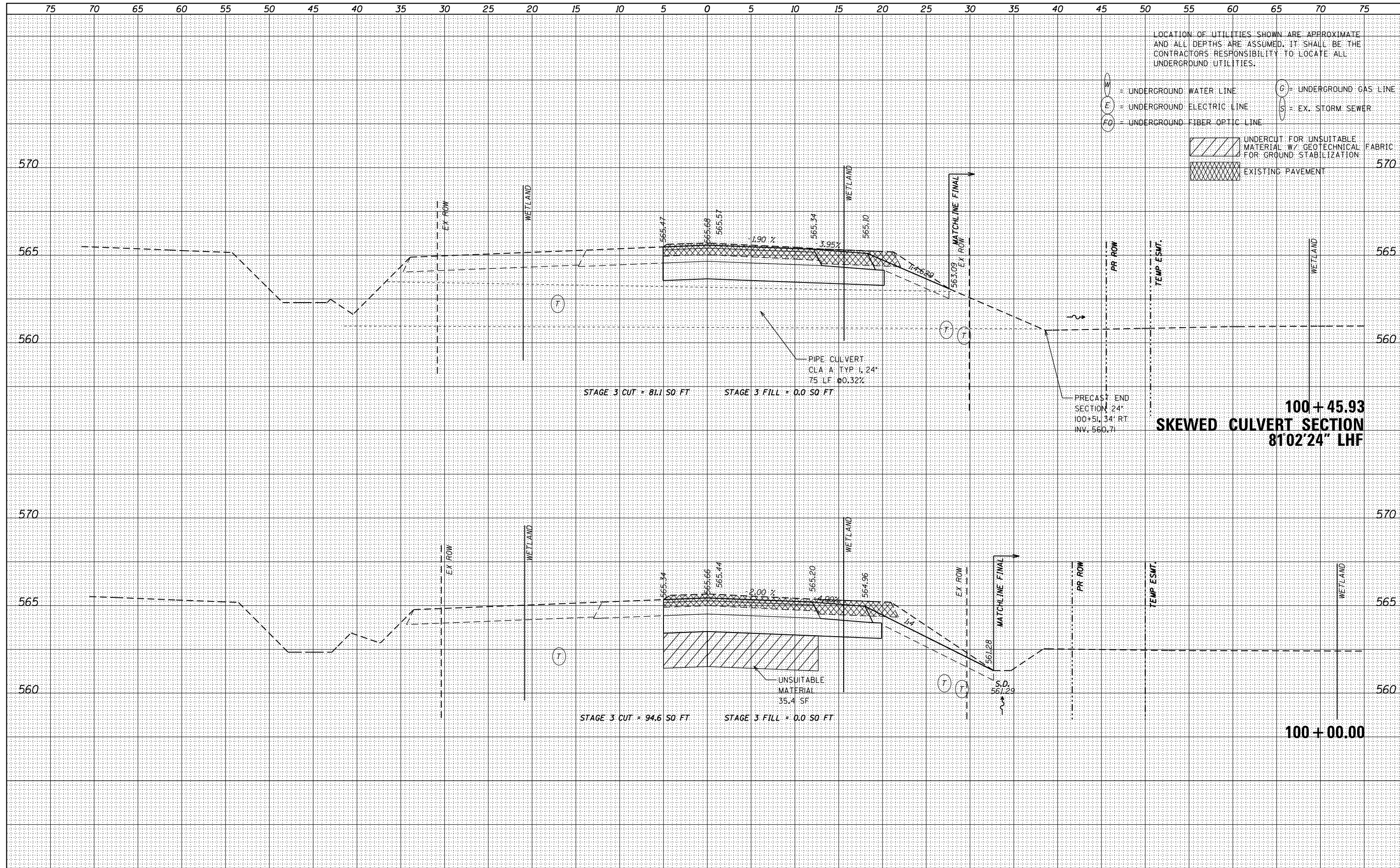
CROSS SECTIONS
IL ROUTE 53 STAGE 3
SCALE: 1"=5'4,2.5'V SHEET 5 OF 19 SHEETS STA. 99+20.00 TO STA. 99+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	165
CONTRACT NO. 62B33				

ILLINOIS FED. AID PROJECT

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



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 FO = UNDERGROUND FIBER OPTIC LINE
 G = UNDERGROUND GAS LINE
 S = EX. STORM SEWER

[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

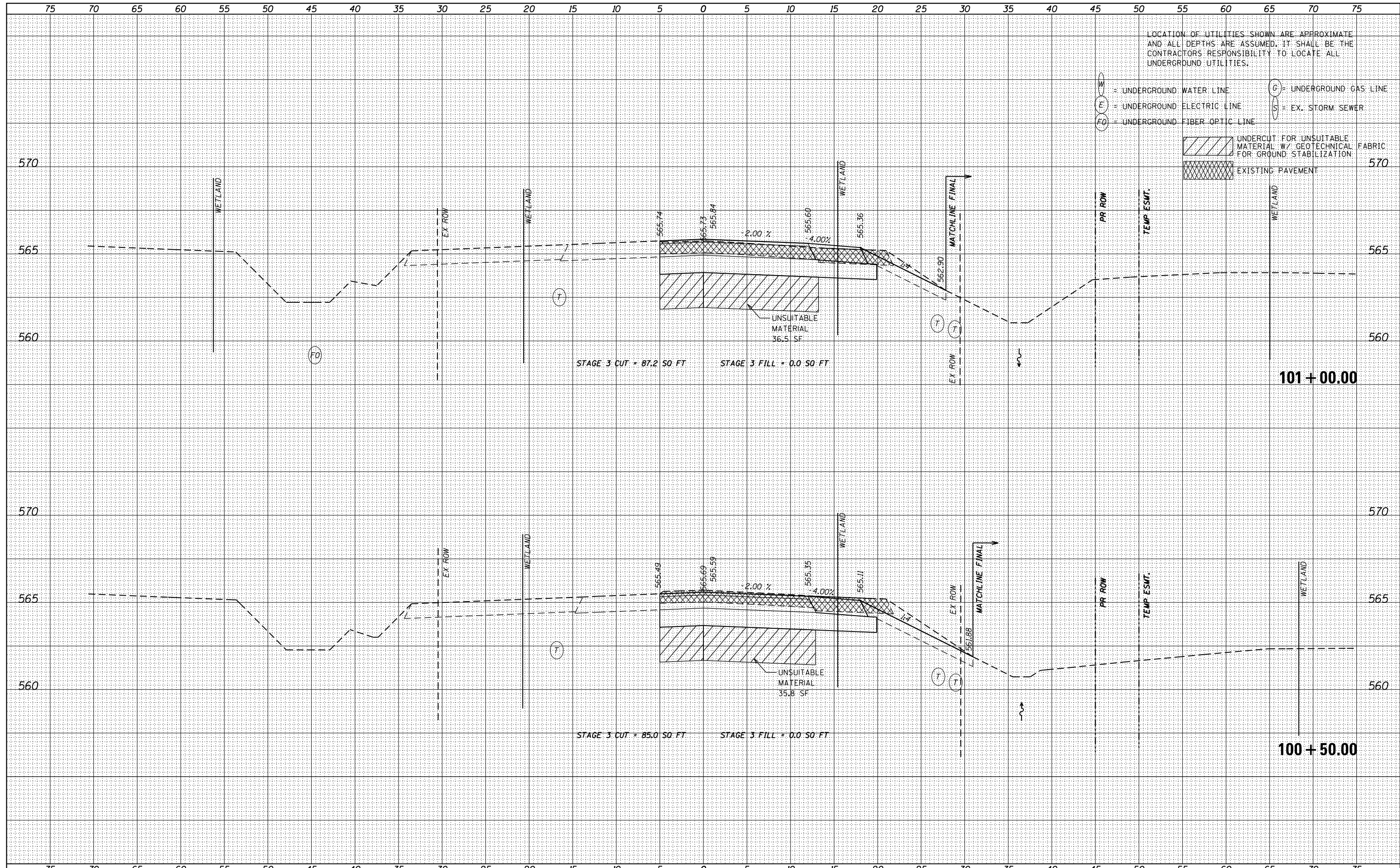
**CROSS SECTIONS
IL ROUTE 53 STAGE 3**

SCALE: 1"=5'4,2.5'V SHEET 6 OF 19 SHEETS STA. 100+00.00 TO STA.100+45.93

F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 166
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED
	AREAS CHECKED



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

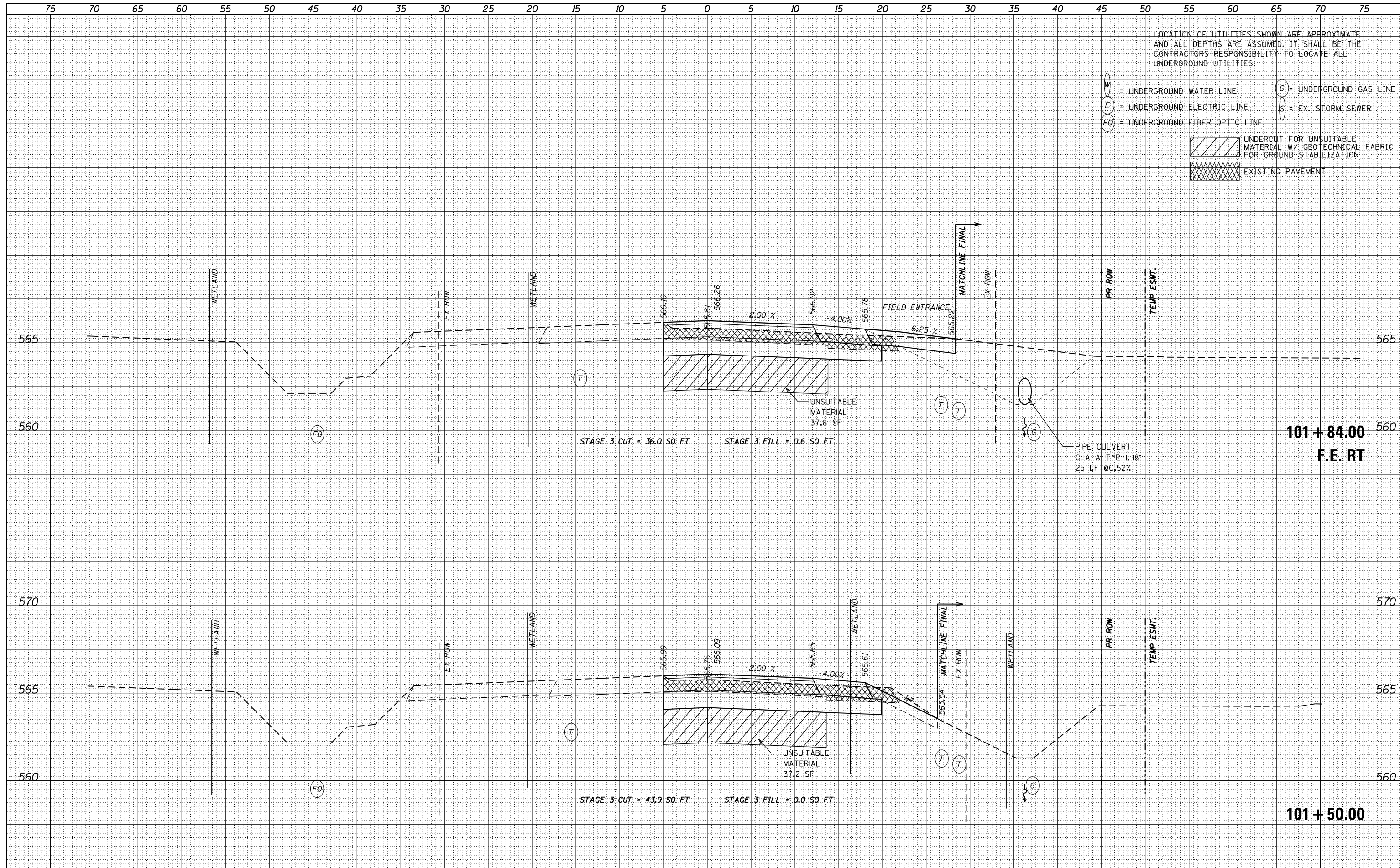
CROSS SECTIONS
IL ROUTE 53 STAGE 3

SCALE: 1"=5'4,2,5"V SHEET 7 OF 19 SHEETS STA. 100+50.00 TO STA.101+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	167
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
AREAS CHECKED	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK NO.	PLOTTED
AREAS CHECKED	TEMPLATE
	AREAS CHECKED



LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

W = UNDERGROUND WATER LINE
 E = UNDERGROUND ELECTRIC LINE
 FO = UNDERGROUND FIBER OPTIC LINE
 G = UNDERGROUND GAS LINE
 S = EX. STORM SEWER

[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

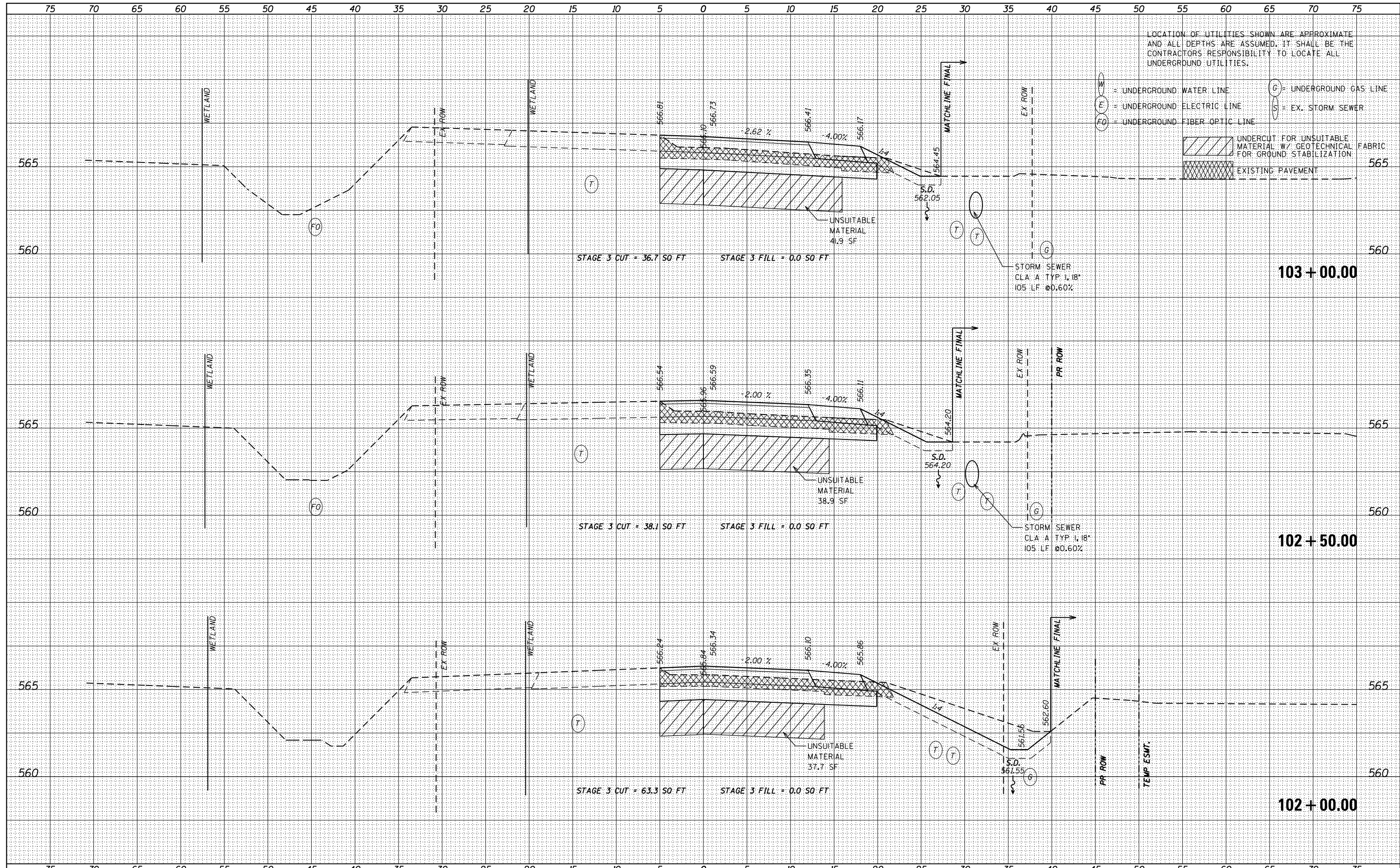
**CROSS SECTIONS
IL ROUTE 53 STAGE 3**

SCALE: 1"=5'-4,2.5'V SHEET 8 OF 19 SHEETS STA. 101+50.00 TO STA.101+84.00

F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 168
			CONTRACT NO. 62B33	
ILLINOIS FED. AID PROJECT				

DATE	
BY	
NO.	
AREAS CHECKED	
TEMPLATES	
PLOTTED	
SURVEY	
FINAL	

DATE	
BY	
NO.	
AREAS CHECKED	
TEMPLATES	
PLOTTED	
SURVEY	
ORIGINAL	



USER NAME	- nick.becker
DESIGNED	- JLM
DRAWN	- BWL
CHECKED	- NJB
DATE	- 02/05/2016
PLLOT SCALE	- 10.0000' / in.
PLLOT DATE	- 3/23/2016

REVISED	-
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REVISED	-

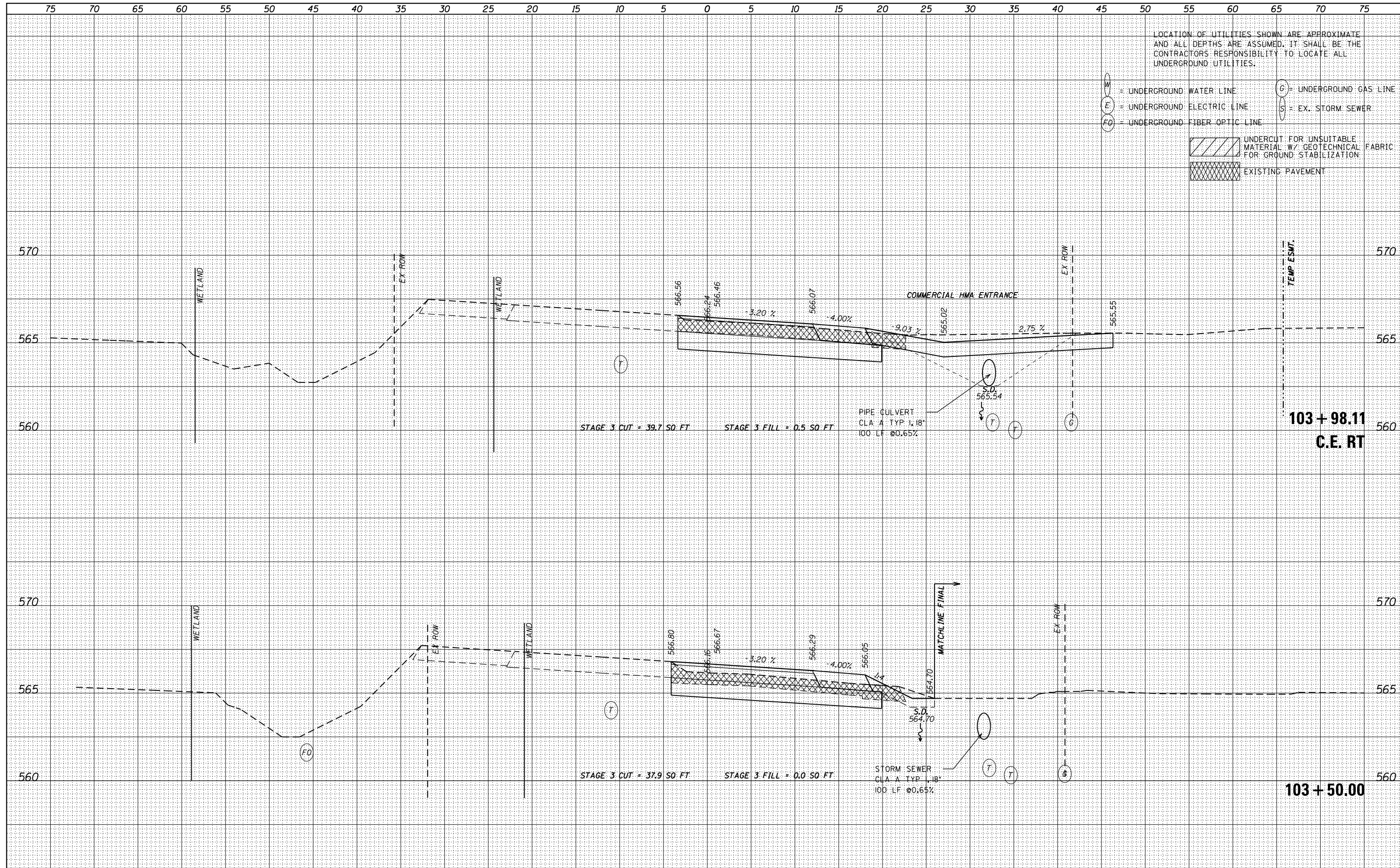
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS	
IL ROUTE 53 STAGE 3	
SCALE: 1"=5'4,2.5'V	SHEET 9 OF 19 SHEETS
STA. 102+00.00	TO STA. 103+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	169
CONTRACT NO. 62B33				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

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 G = UNDERGROUND GAS LINE
 S = EX. STORM SEWER

[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

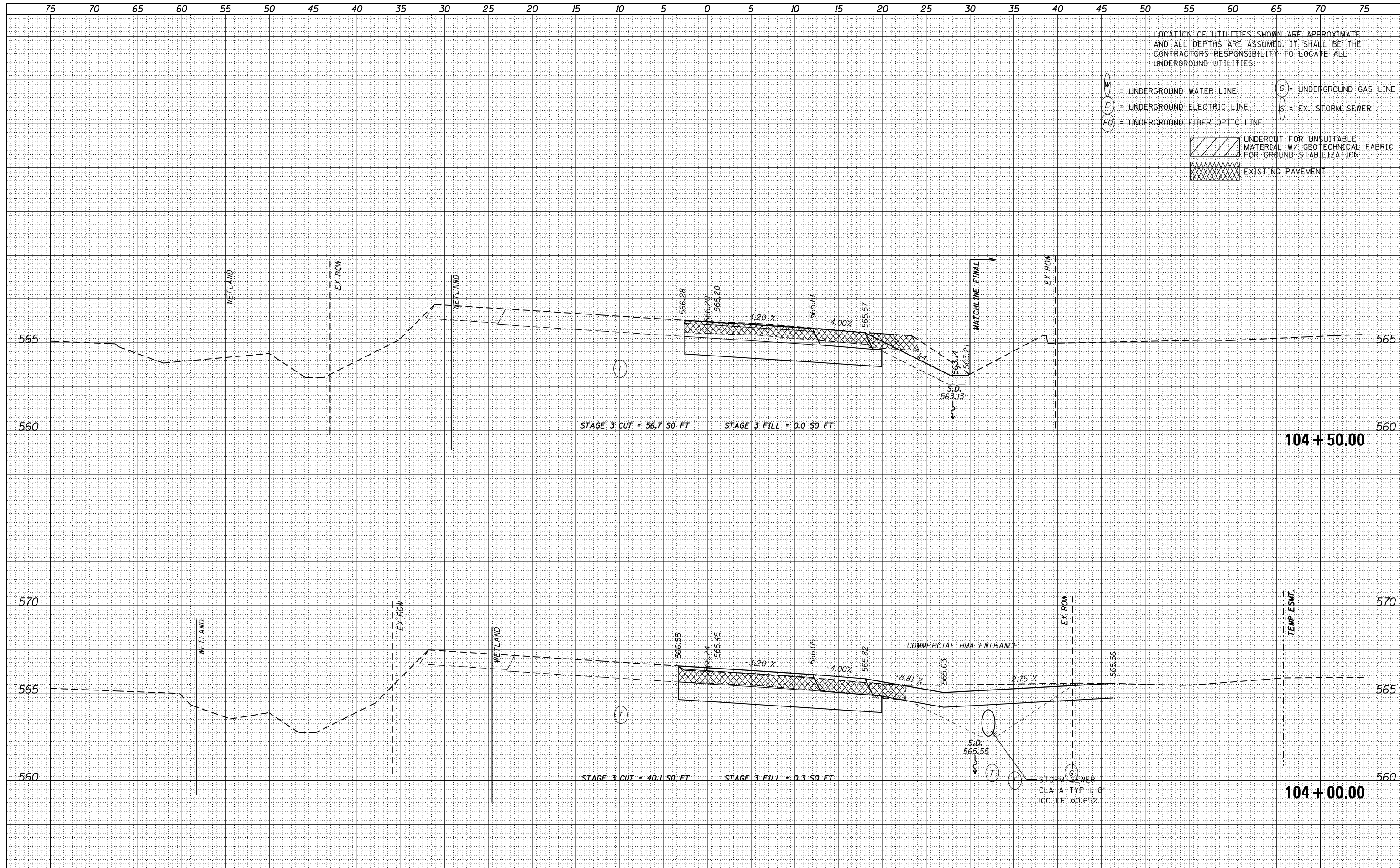
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 3
SCALE: 1"=5'4,2.5'V SHEET 10 OF 19 SHEETS STA. 103+50.00 TO STA.103+98.11

F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 170
			CONTRACT NO. 62B33	
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	
SURVEY PLOTTED	
NOTE BOOK	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
SURVEY PLOTTED	
NOTE BOOK	
AREAS CHECKED	



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- EXISTING PAVEMENT



USER NAME = nick.becker	DESIGNED - JLM	REVISD -
	DRAWN - BWL	REVISD -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISD -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISD -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 3

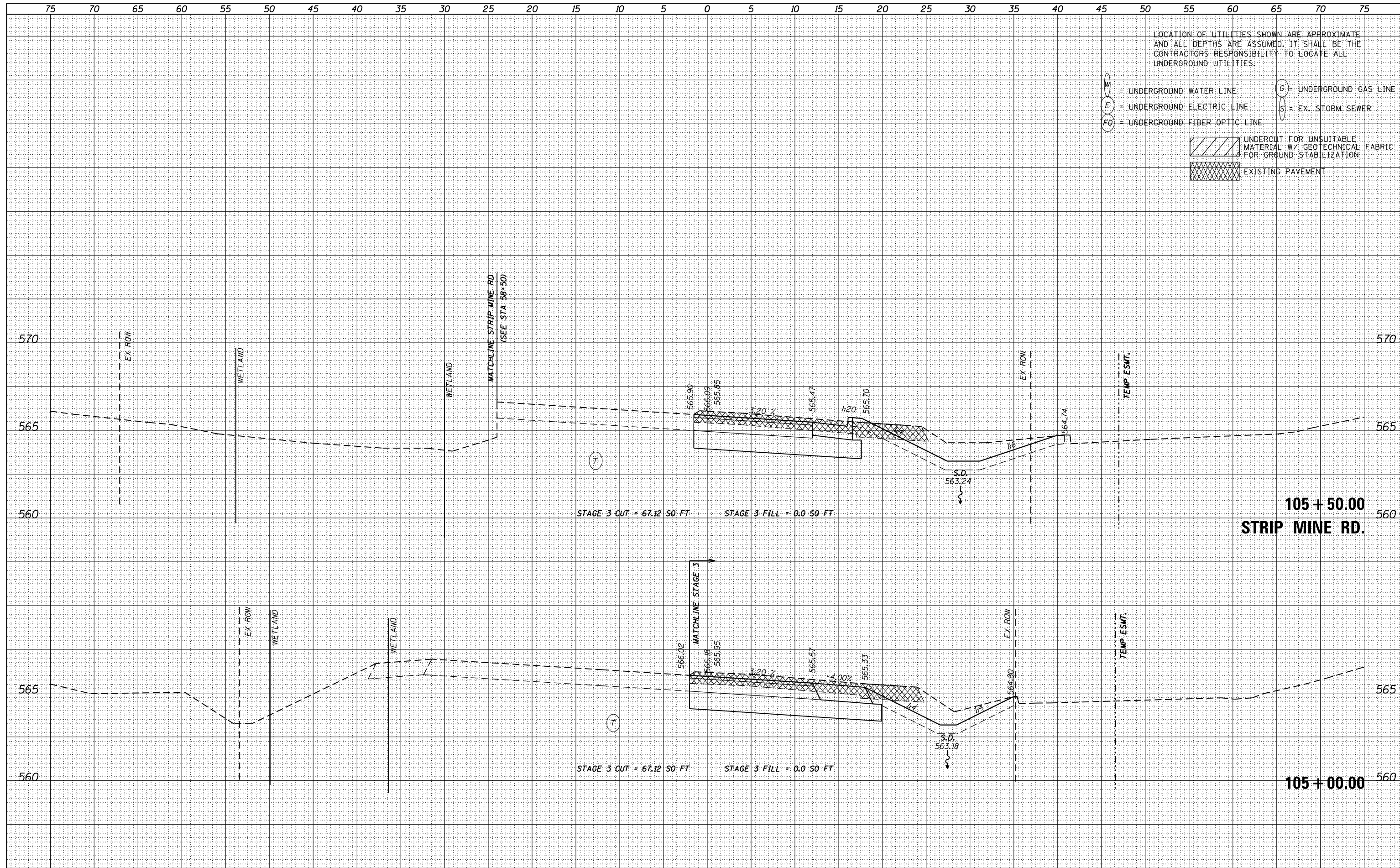
SCALE: 1"=5'4,2.5'V SHEET 11 OF 19 SHEETS STA. 104+00.00 TO STA. 104+50.00

F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 171
CONTRACT NO. 62B33				

ILLINOIS FED. AID PROJECT

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	



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[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

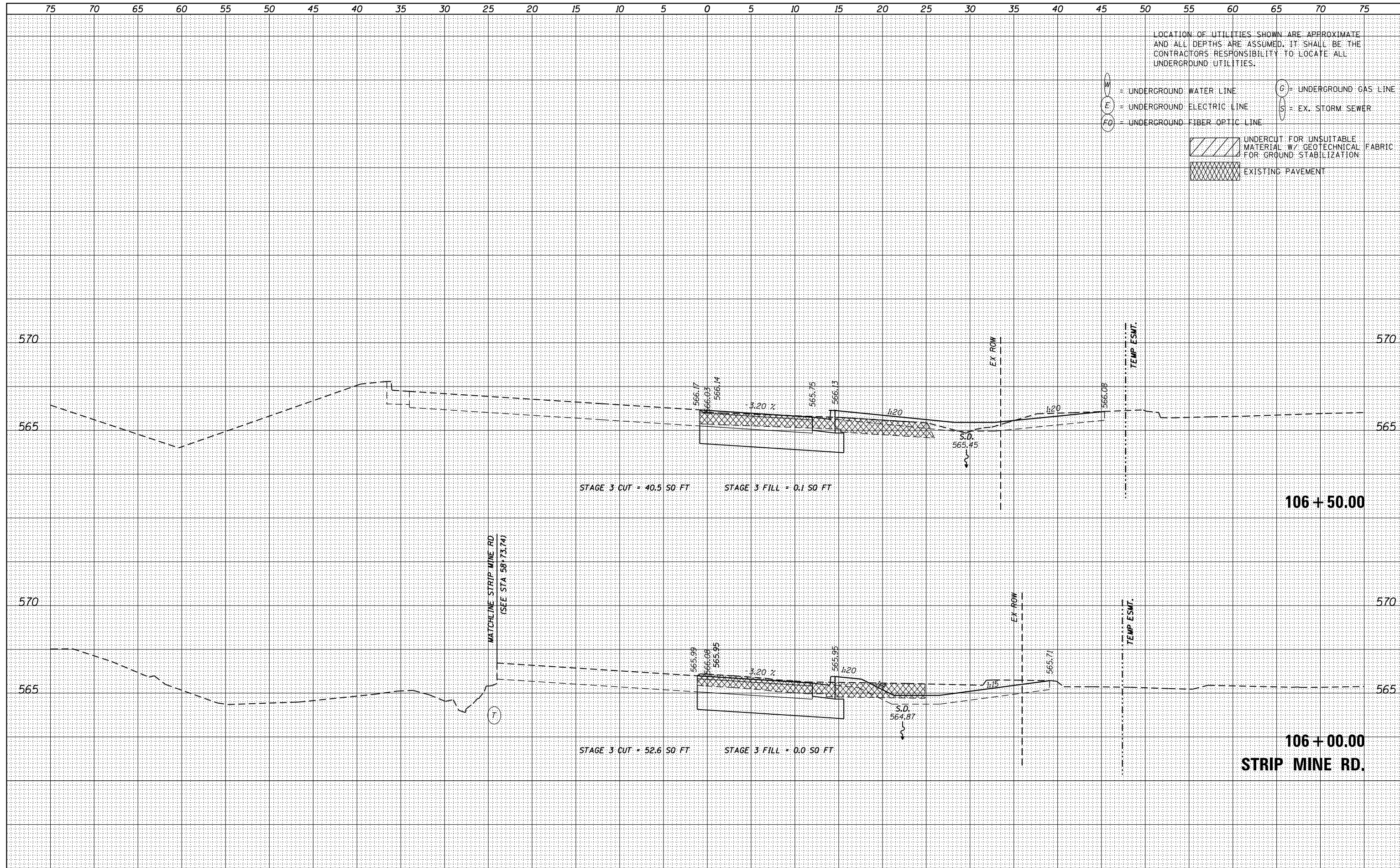
**CROSS SECTIONS
IL ROUTE 53 STAGE 3**

SCALE: 1"=5'4.25"V SHEET 12 OF 19 SHEETS STA. 105+00.00 TO STA. 105+50.00

F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 172
			CONTRACT NO. 62B33	
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINISHED SURVEY	
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NOTE BOOK	
AREAS CHECKED	
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DATE	
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ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
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 S = EX. STORM SEWER

[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 3

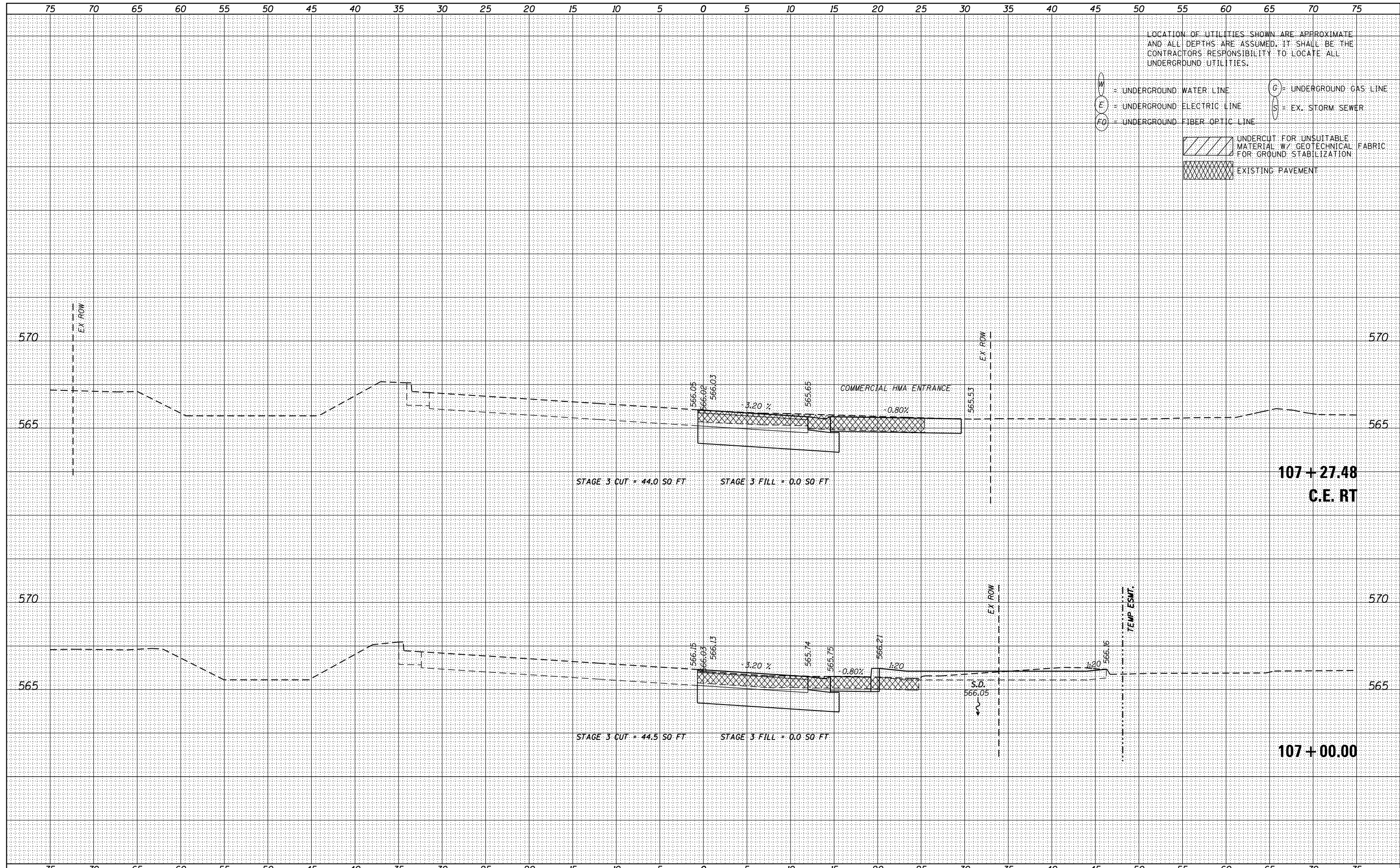
SCALE: 1"=5'4,2,5"V SHEET 13 OF 19 SHEETS STA. 106+00.00 TO STA.106+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	173
				CONTRACT NO. 62B33

ILLINOIS FED. AID PROJECT

DATE	
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TEMPLATE	
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FINAL SURVEY	
NOTE BOOK	
AREAS CHECKED	
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SURVEYED	
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TEMPLATE	
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ORIGINAL SURVEY	
NOTE BOOK	
AREAS CHECKED	
NO.	



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

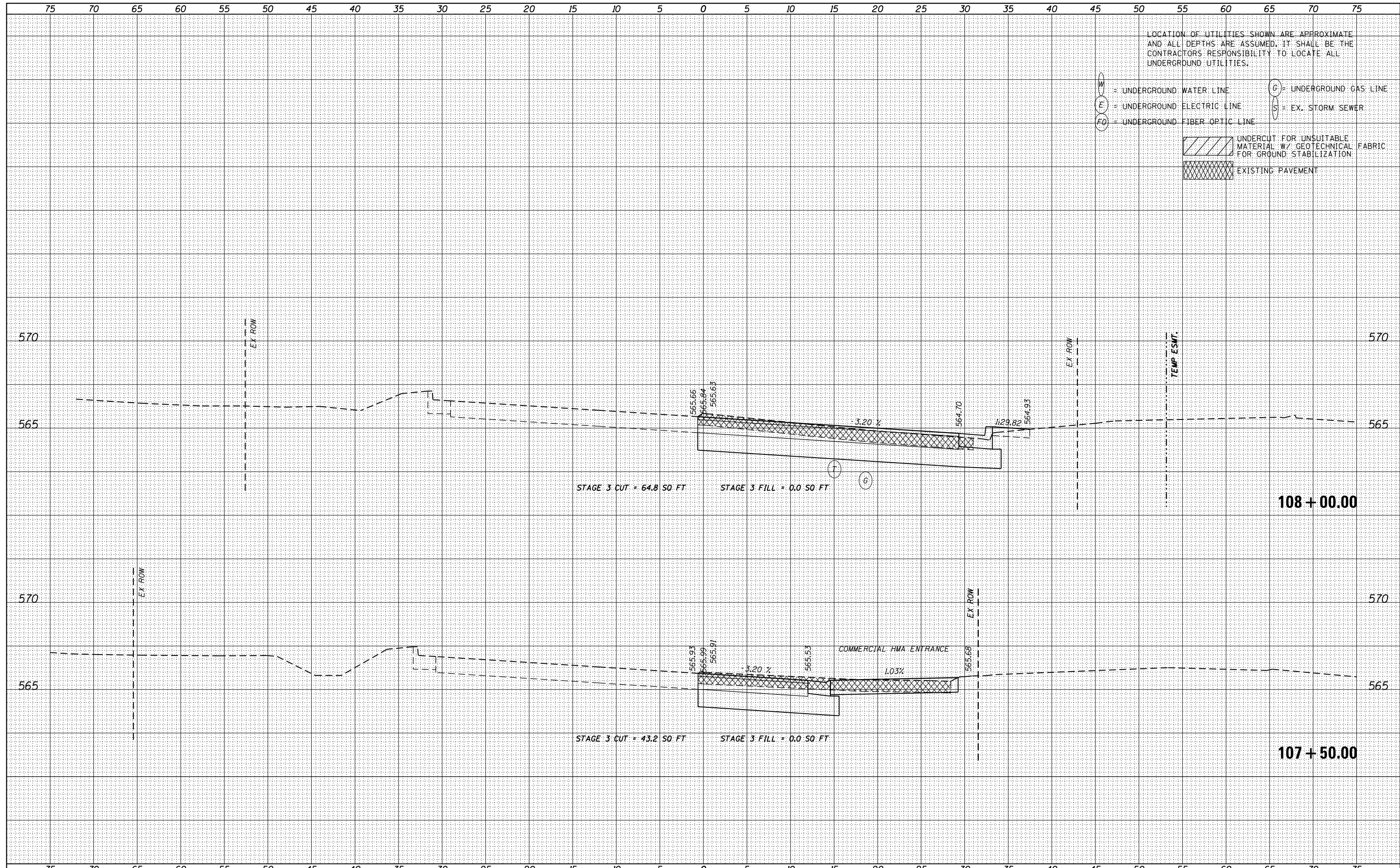
**CROSS SECTIONS
IL ROUTE 53 STAGE 3**

SCALE: 1"=5'H, 2.5"V SHEET 14 OF 19 SHEETS STA. 107+00.00 TO STA. 107+27.48

F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 174
			CONTRACT NO. 62B33	
ILLINOIS FED. AID PROJECT				

DATE	
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SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
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DATE	
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SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



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- EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

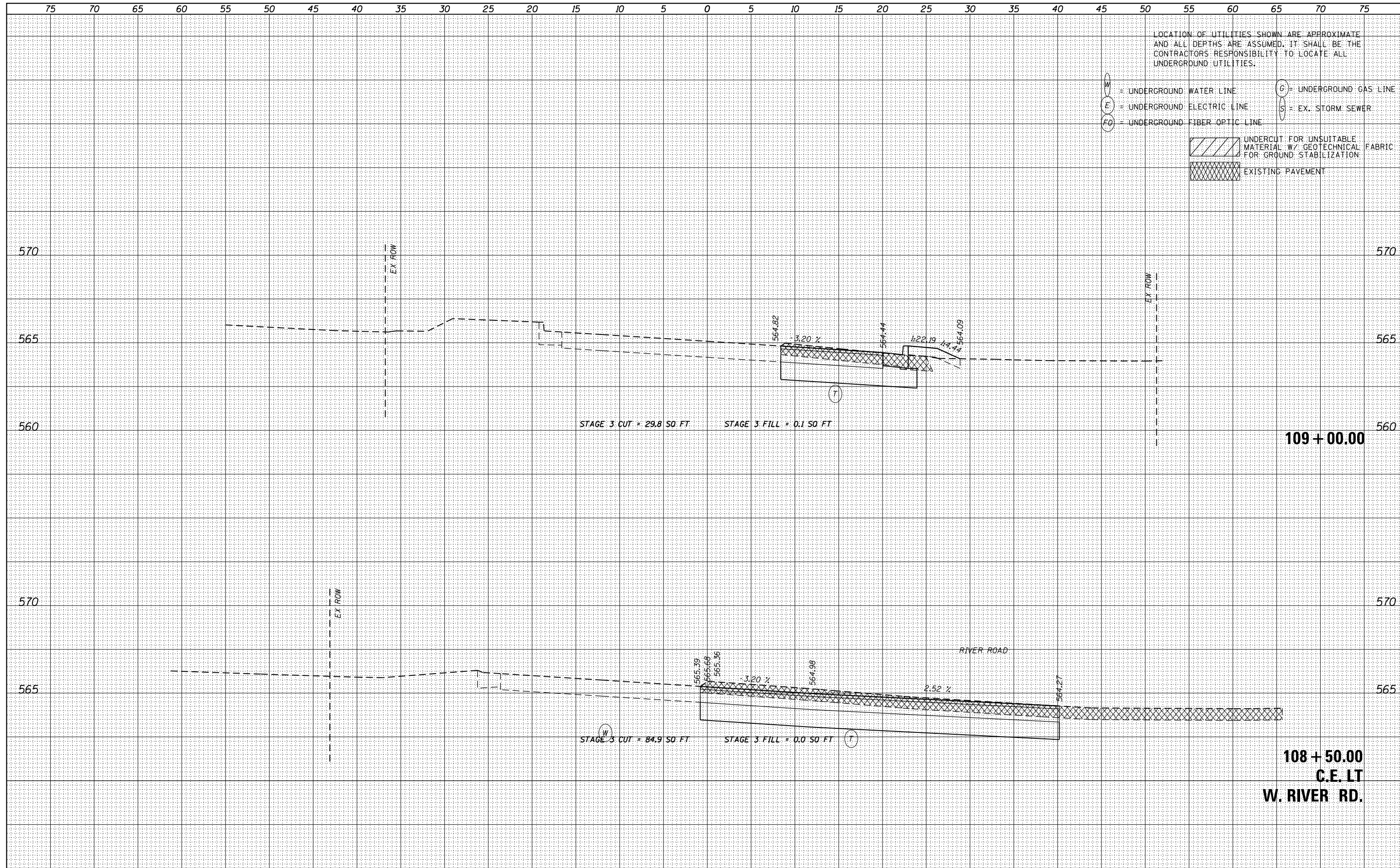
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 3
SCALE: 1"=5'4,2.5'V SHEET 15 OF 19 SHEETS STA. 107+50.00 TO STA.108+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	175
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
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FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

W = UNDERGROUND WATER LINE
 E = UNDERGROUND ELECTRIC LINE
 FO = UNDERGROUND FIBER OPTIC LINE
 G = UNDERGROUND GAS LINE
 S = EX. STORM SEWER

[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 3

SCALE: 1"=5'4,2.5'V SHEET 16 OF 19 SHEETS STA. 108+50.00 TO STA.109+00.00

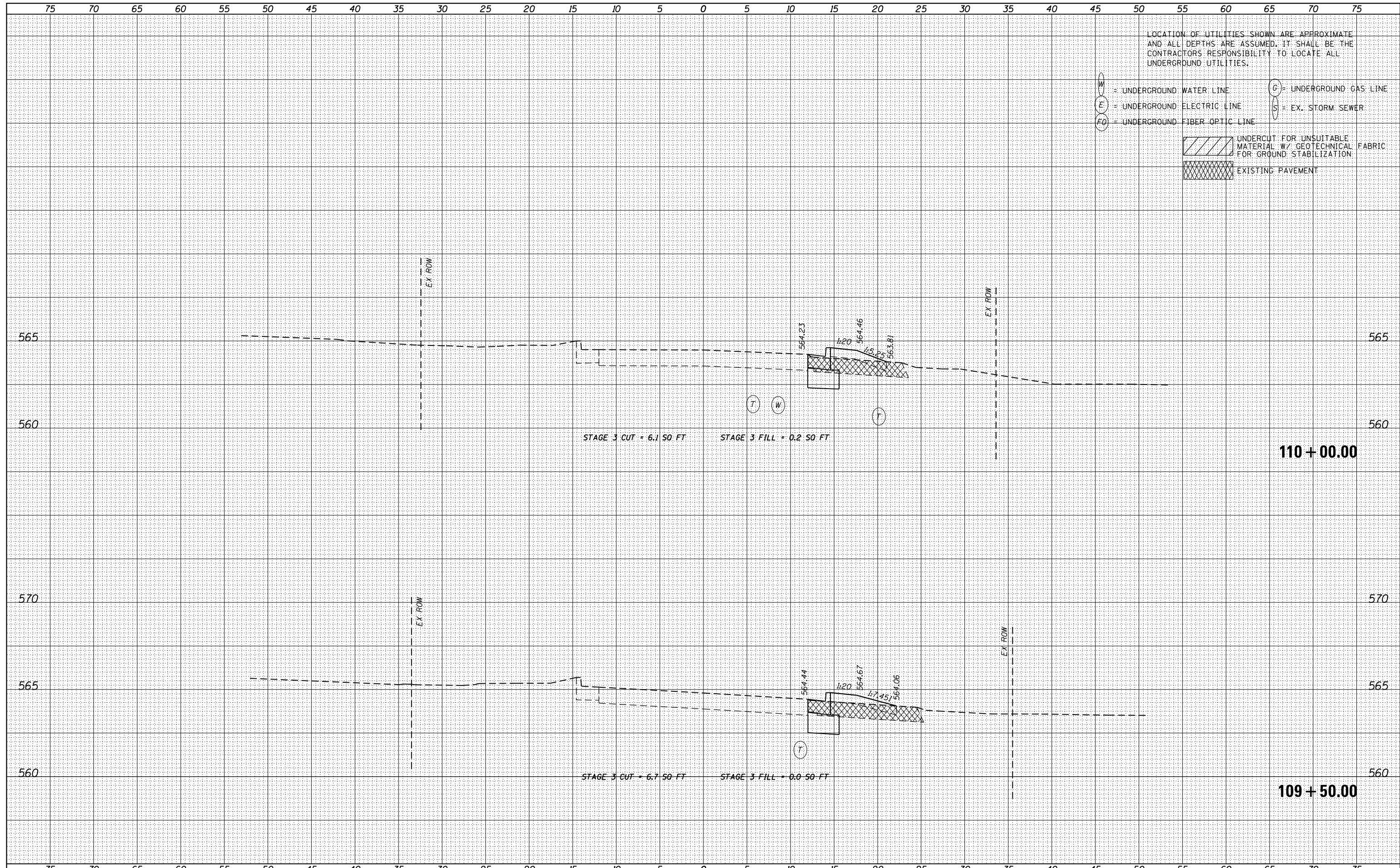
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	176
				CONTRACT NO. 62B33

108 + 50.00
C.E. LT
W. RIVER RD.

109 + 00.00

DATE	
BY	
FINAL SURVEY	
NOTE BOOK NO.	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
NOTE BOOK NO.	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	



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- S = EX. STORM SEWER
- UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
PLOT SCALE - 10.0000' / in.	DRAWN - BWL	REVISED -
PLOT DATE - 3/23/2016	CHECKED - NJB	REVISED -
	DATE - 02/05/2016	REVISED -

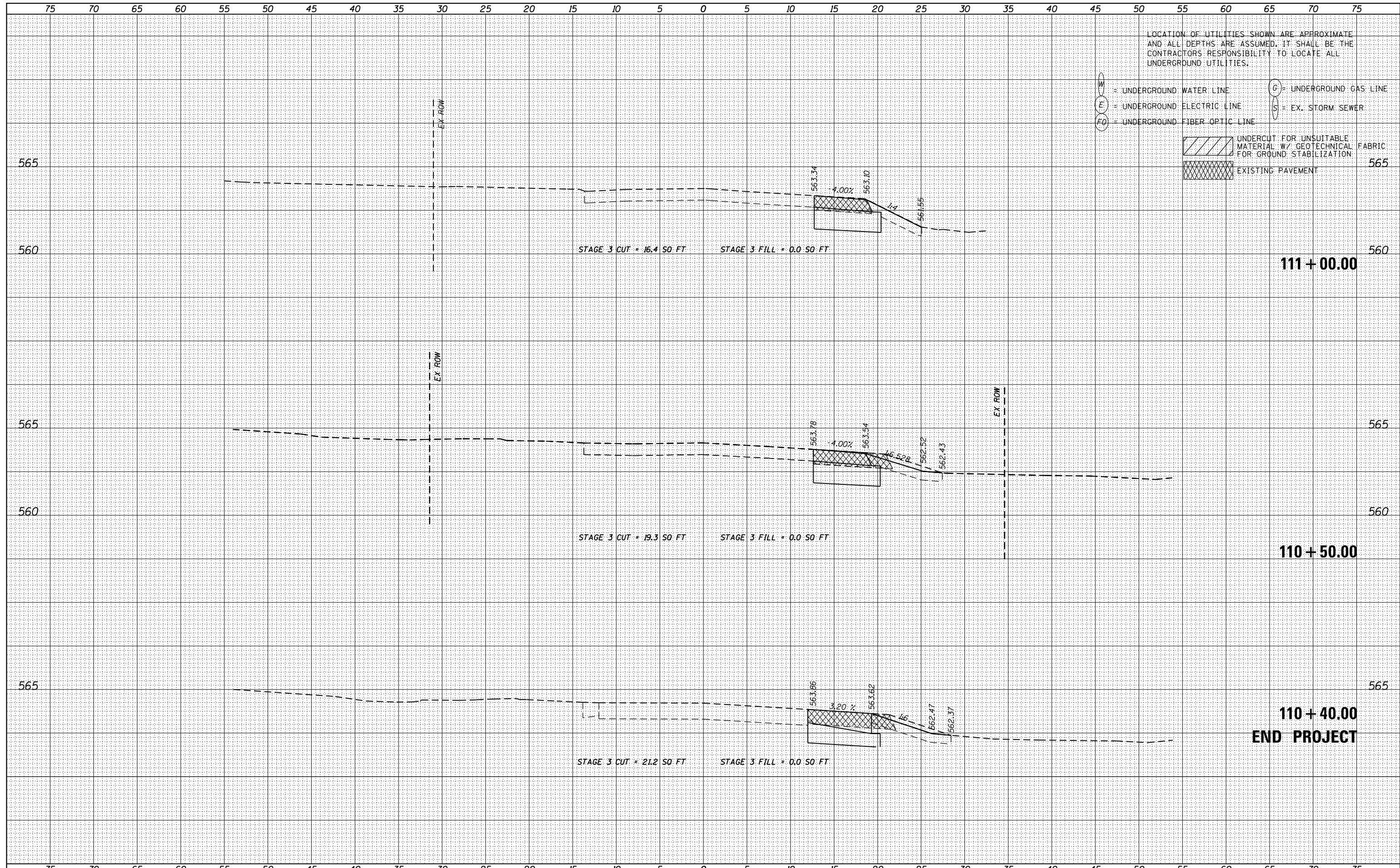
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 3
SCALE: 1"=5'4,2,5"V SHEET 17 OF 19 SHEETS STA. 109+50.00 TO STA. 110+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	177
				CONTRACT NO. 62B33

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
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DATE	
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SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	



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- EXISTING PAVEMENT



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 3

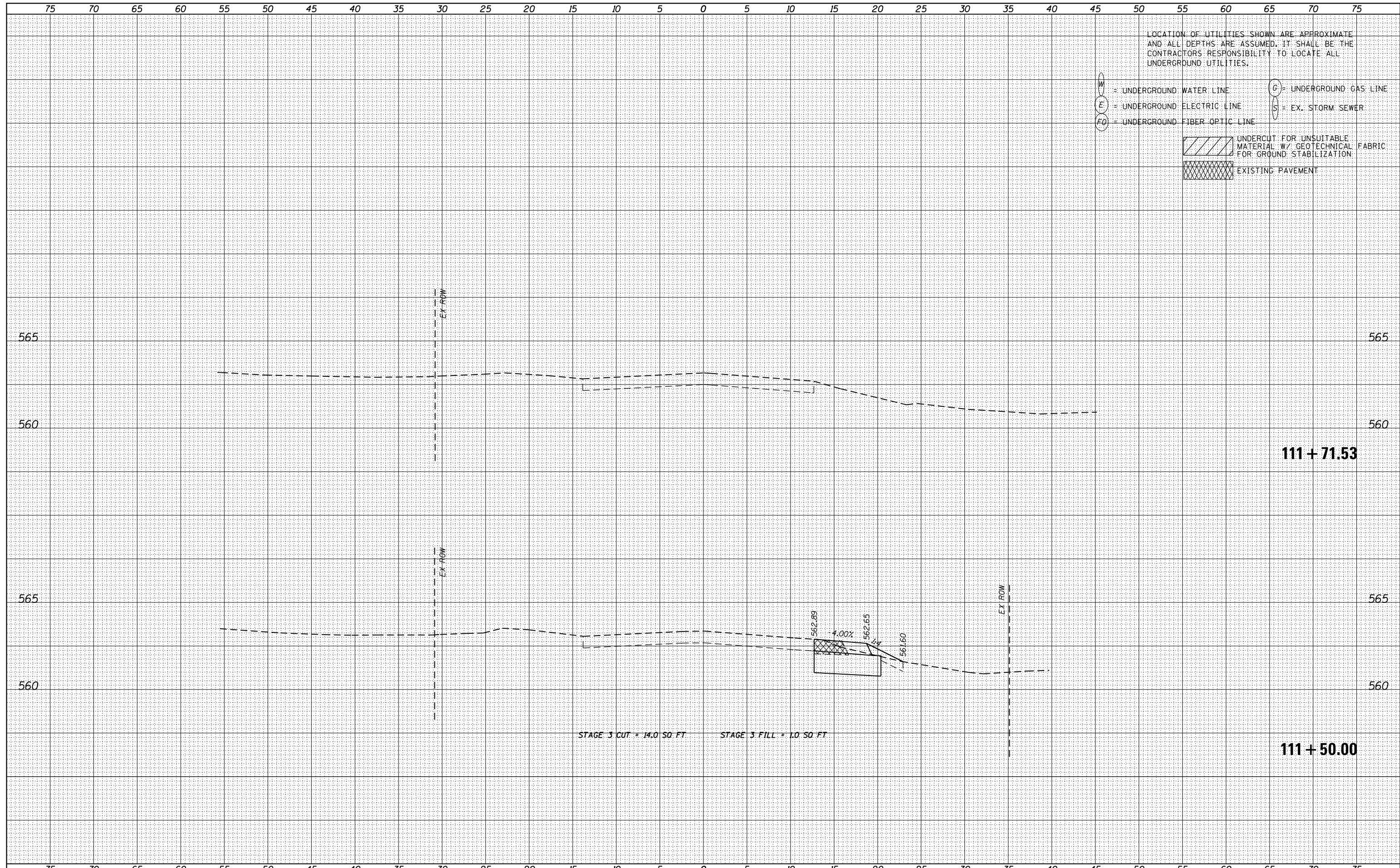
SCALE: 1"=5'4,2,5'V SHEET 18 OF 19 SHEETS STA. 110+40.00 TO STA. 111+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	178
				CONTRACT NO. 62B33

ILLINOIS FED. AID PROJECT

DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

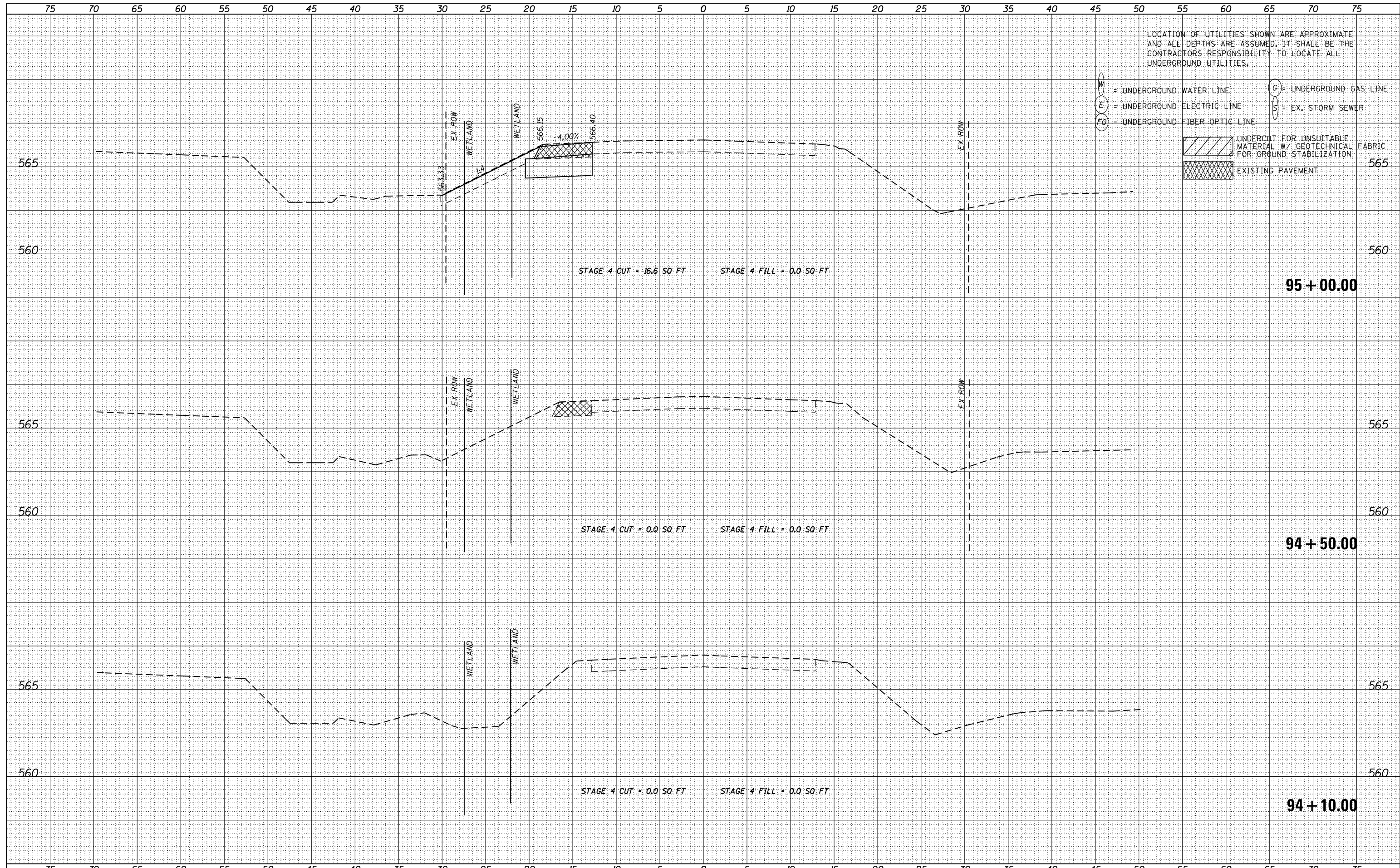
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS		
IL ROUTE 53 STAGE 3		
SCALE: 1"=5'4,2.5'V	SHEET 19 OF 19 SHEETS	STA. 111+50.00 TO STA. 111+71.53

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	179
CONTRACT NO. 62B33				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
ORIGINAL SURVEY	
SURVEYED	
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DATE	
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ORIGINAL SURVEY	
SURVEYED	
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TEMPLATE	
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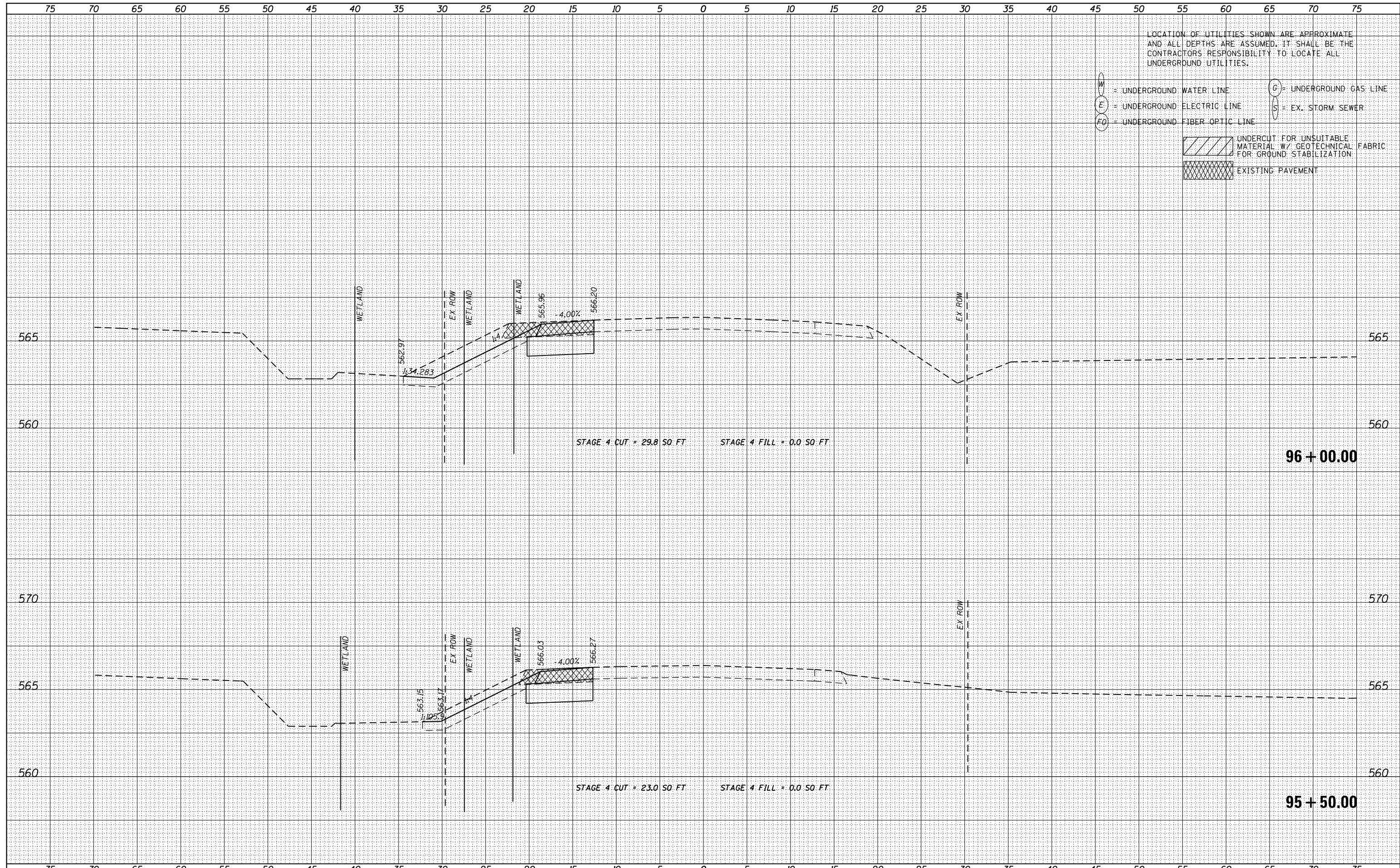


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- UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

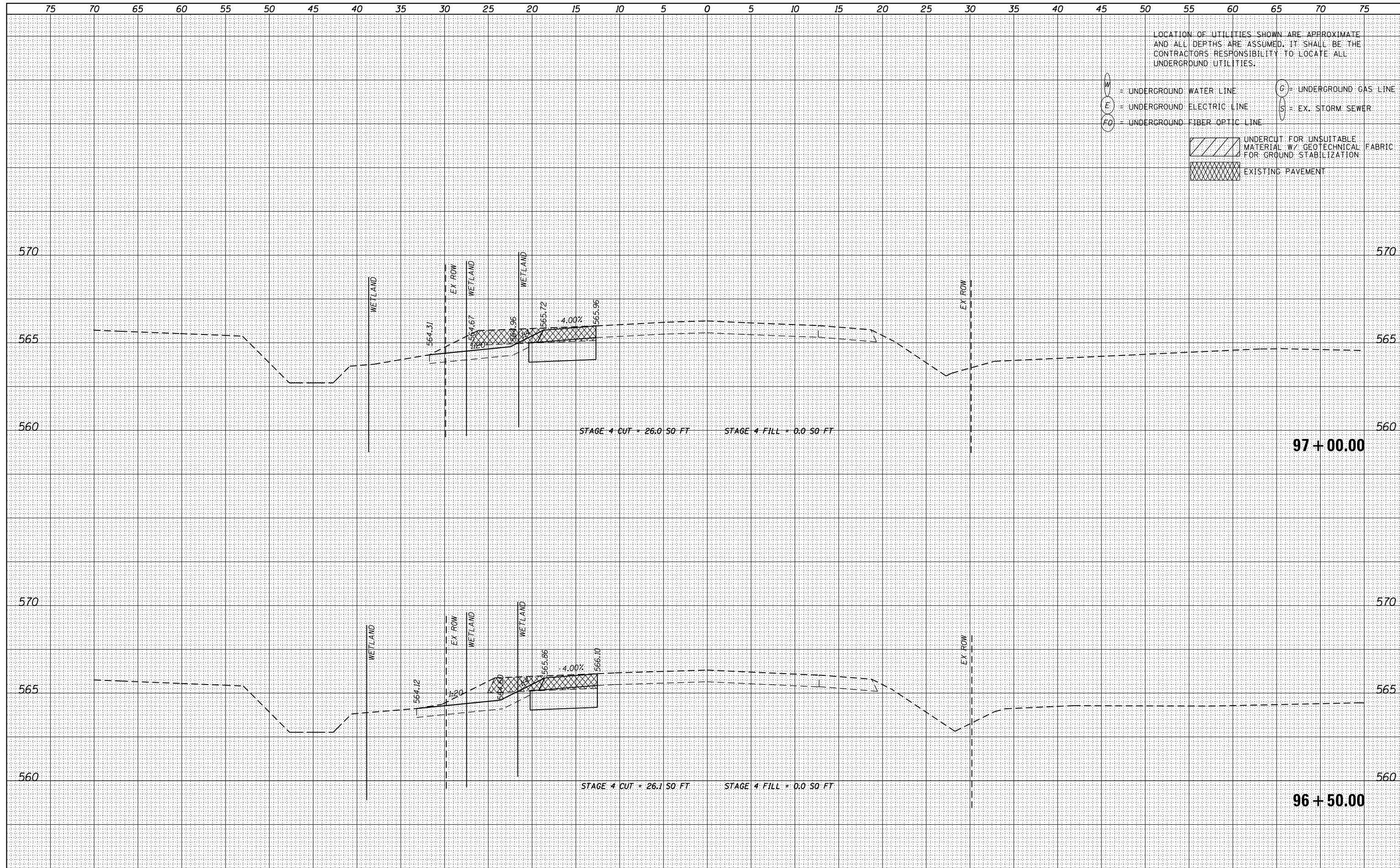
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 4
SCALE: 1"=5'H, 2.5"V SHEET 2 OF 13 SHEETS STA. 95+50.00 TO STA. 96+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	181
			CONTRACT NO. 62B33	
ILLINOIS FED. AID PROJECT				

DATE	
BY	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
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DATE	
BY	
ORIGINAL SURVEY	
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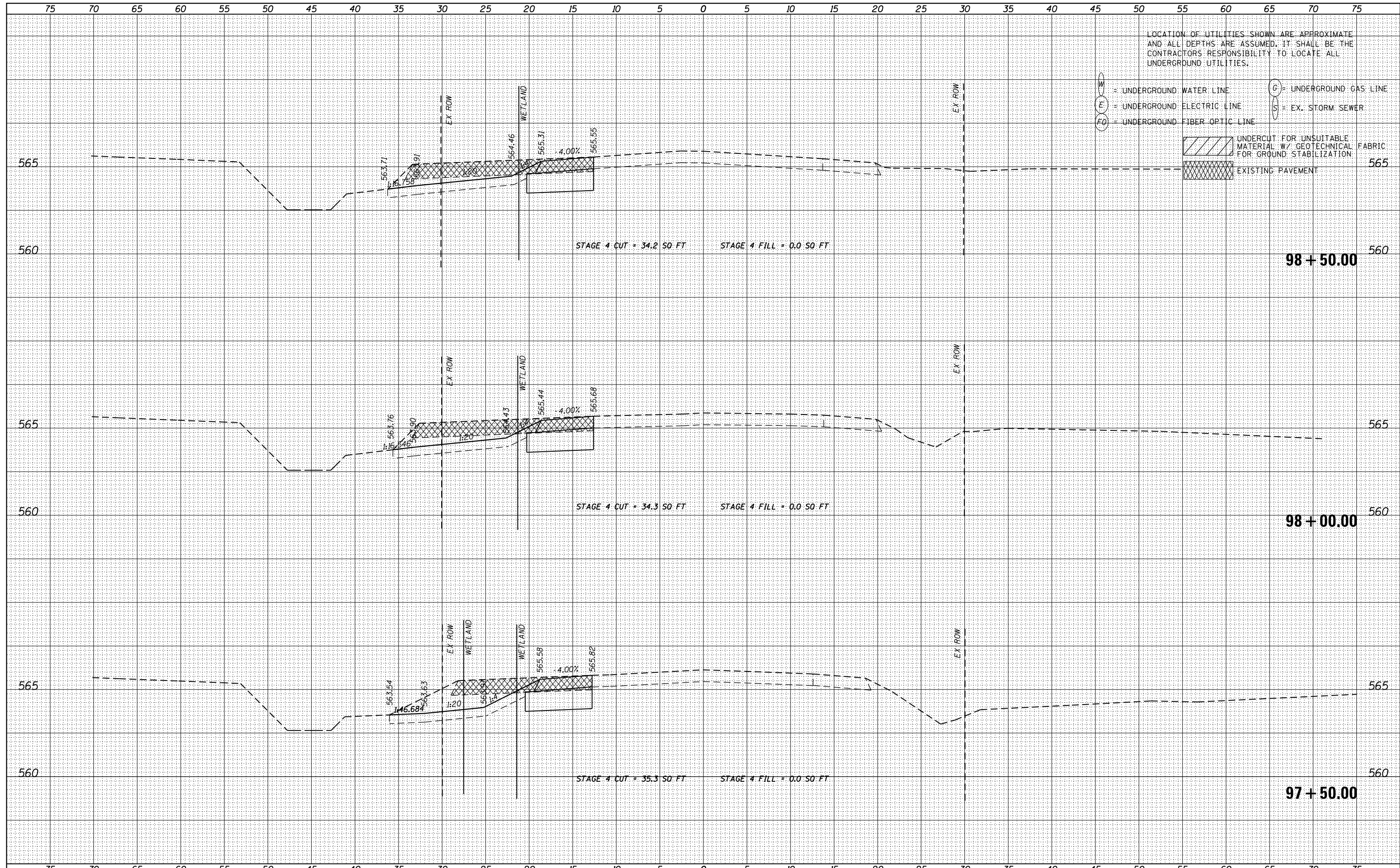
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[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
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DATE	
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SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	



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UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

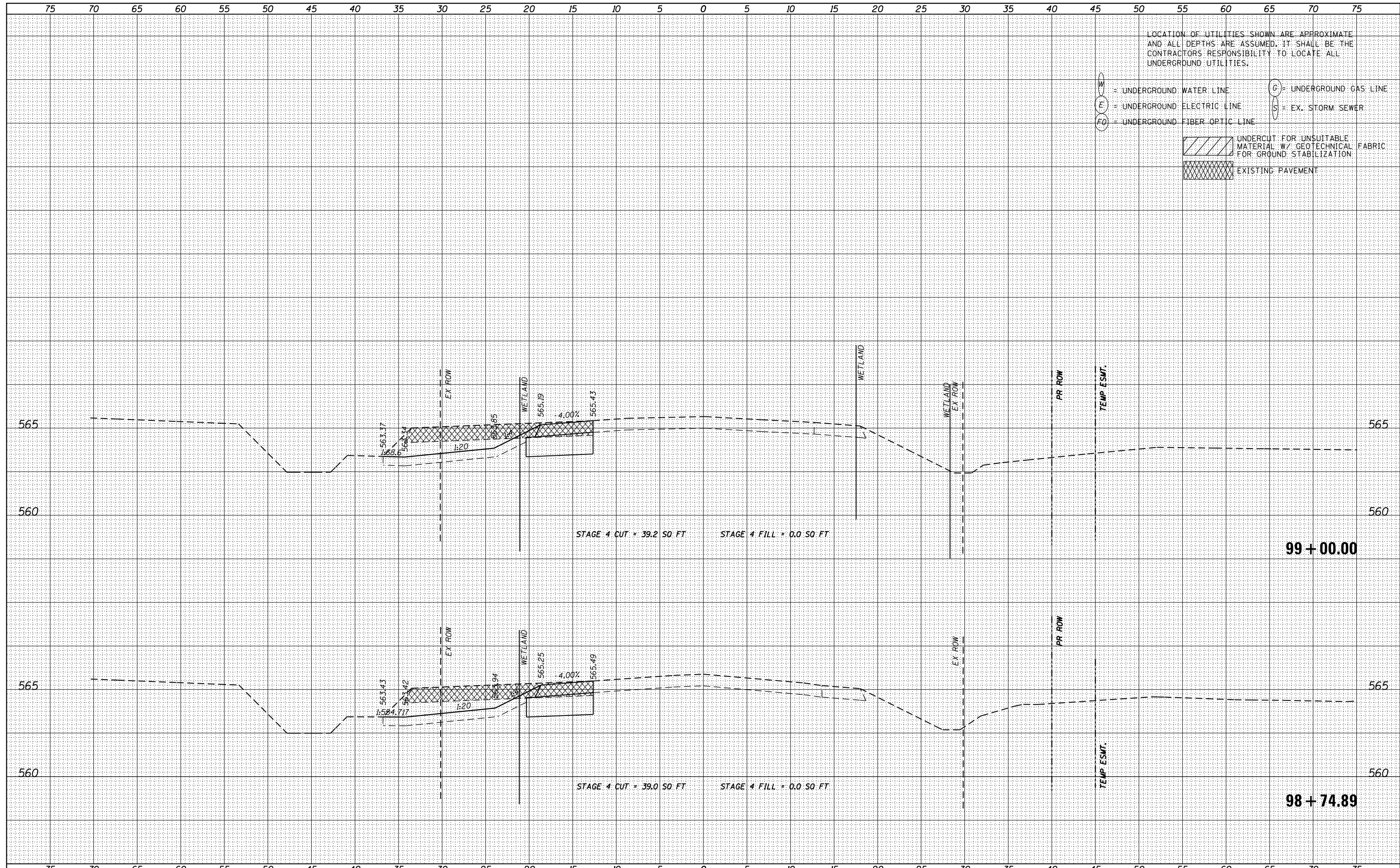
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
 IL ROUTE 53 STAGE 4
 SCALE: 1"=5'4,2.5'V SHEET 4 OF 13 SHEETS STA. 97+50.00 TO STA. 98+50.00

F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 183
			CONTRACT NO. 62B33	
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
NO.	



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- S = EX. STORM SEWER
- [Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- [Cross-hatched Box] EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

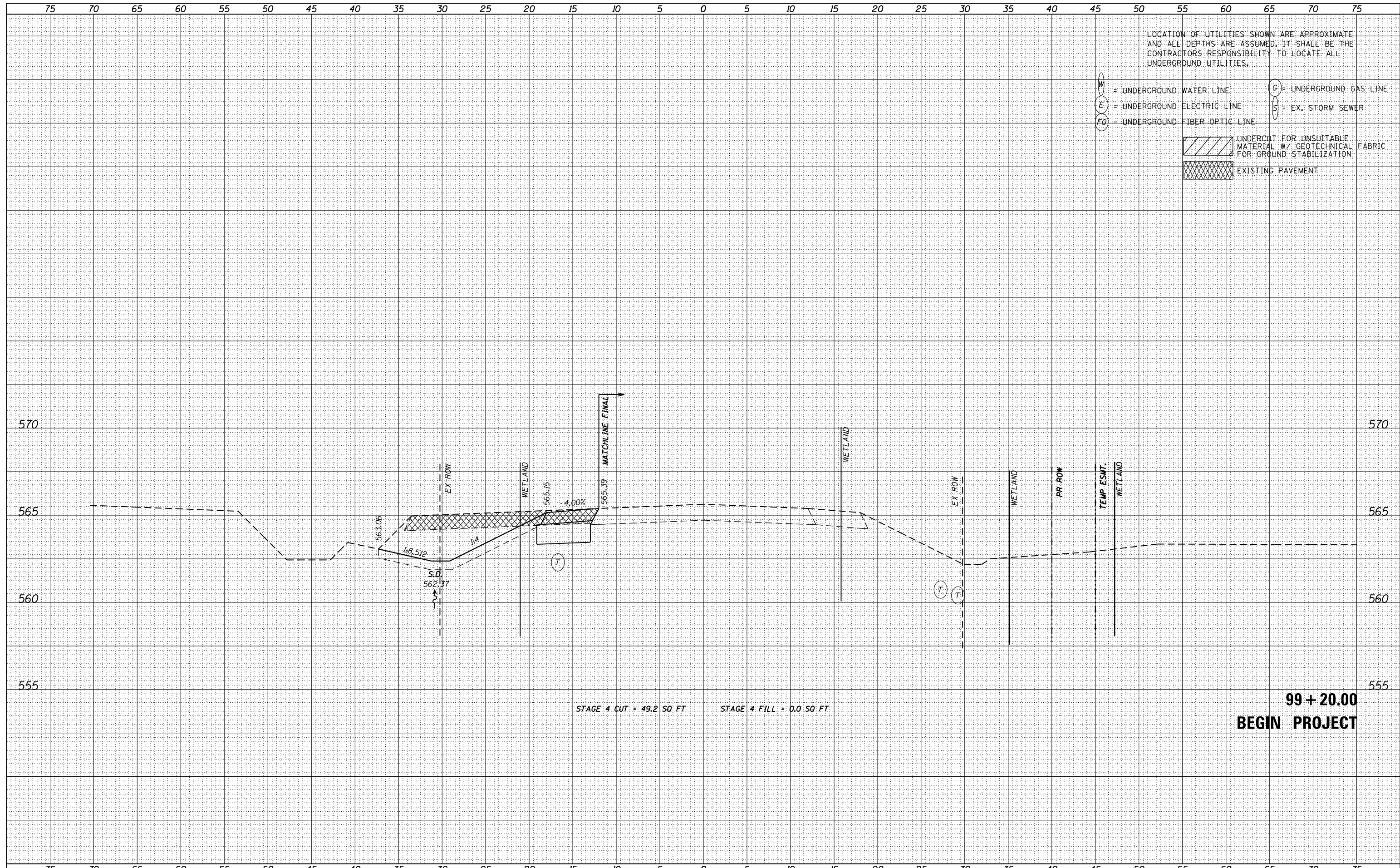
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 4
SCALE: 1"=5'H, 2.5'V SHEET 5 OF 13 SHEETS STA. 98+74.89 TO STA. 99+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	184
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

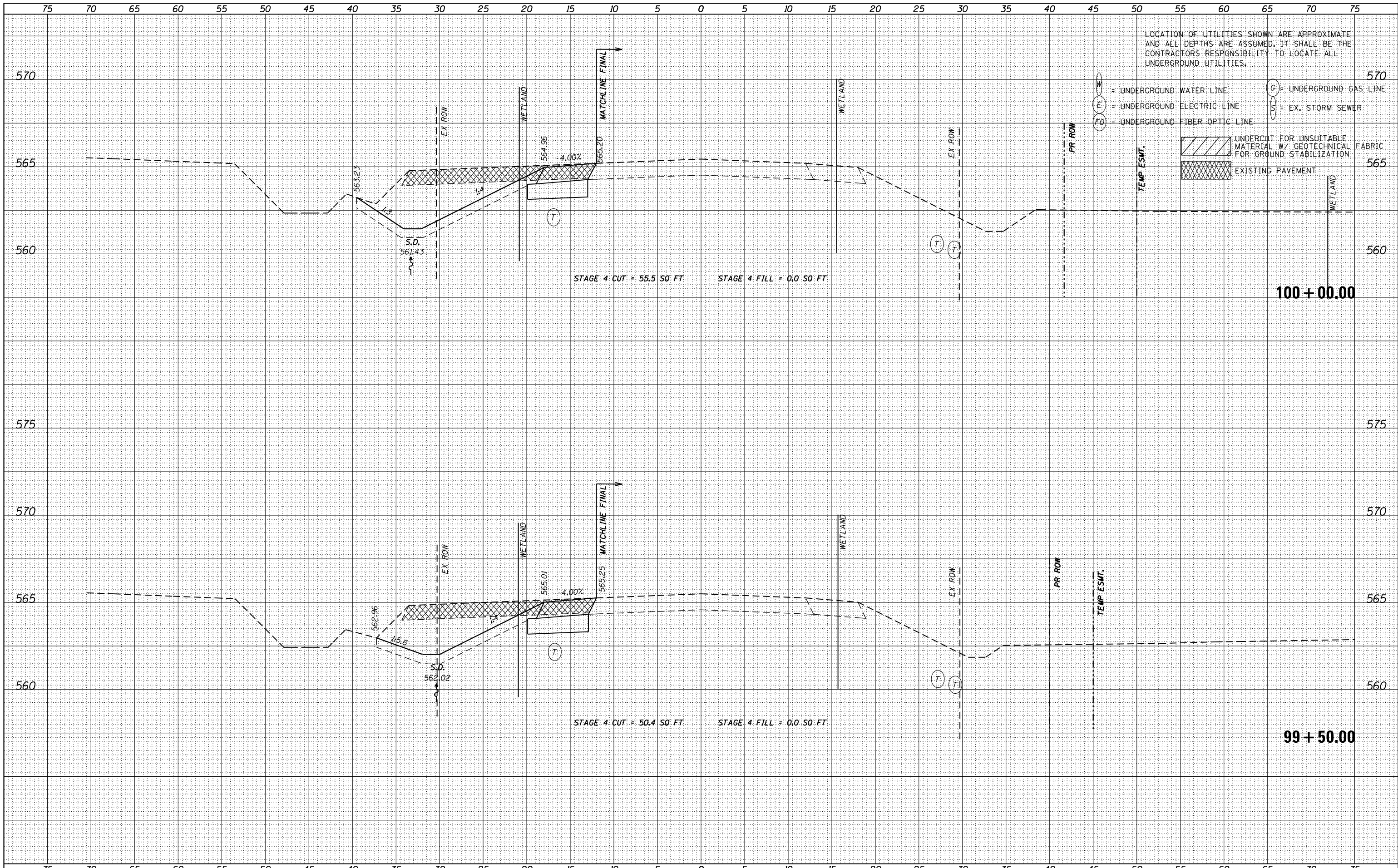
CROSS SECTIONS
IL ROUTE 53 STAGE 4

SCALE: 1"=5'4,2,5"V SHEET 6 OF 13 SHEETS STA. 99+20.00 TO STA. 99+20.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	185
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

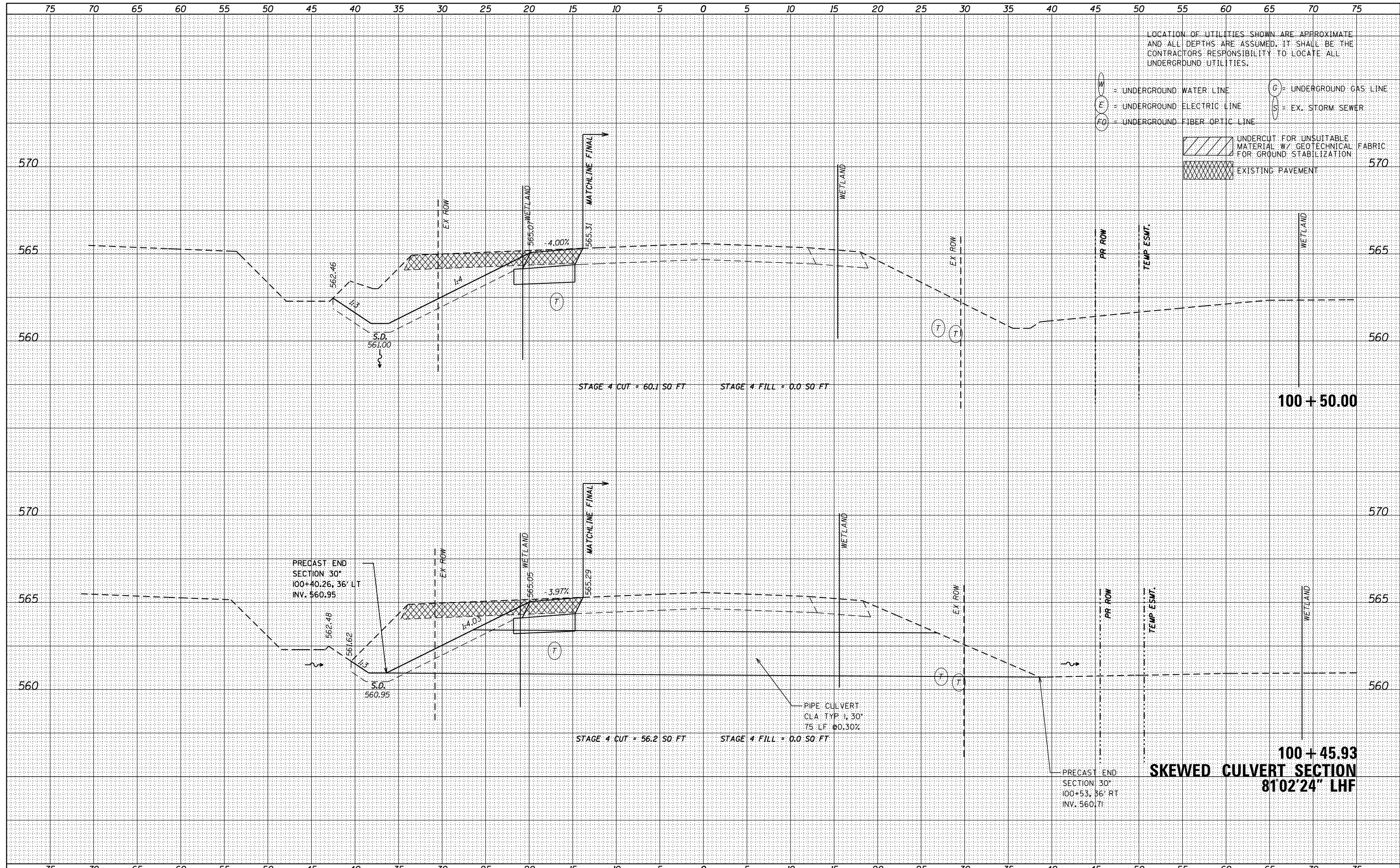
CROSS SECTIONS IL ROUTE 53 STAGE 4			
SCALE: 1"=5'4,2.5'V	SHEET 7	OF 13 SHEETS	STA. 99+50.00 TO STA.100+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	186
				CONTRACT NO. 62B33

ILLINOIS FED. AID PROJECT

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



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[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT



USER NAME	= nick.becker
DESIGNED	- JLM
DRAWN	- BWL
CHECKED	- NJB
DATE	- 02/05/2016
DESIGNED	-
REVISIONS	-
REVISIONS	-
REVISIONS	-
REVISIONS	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

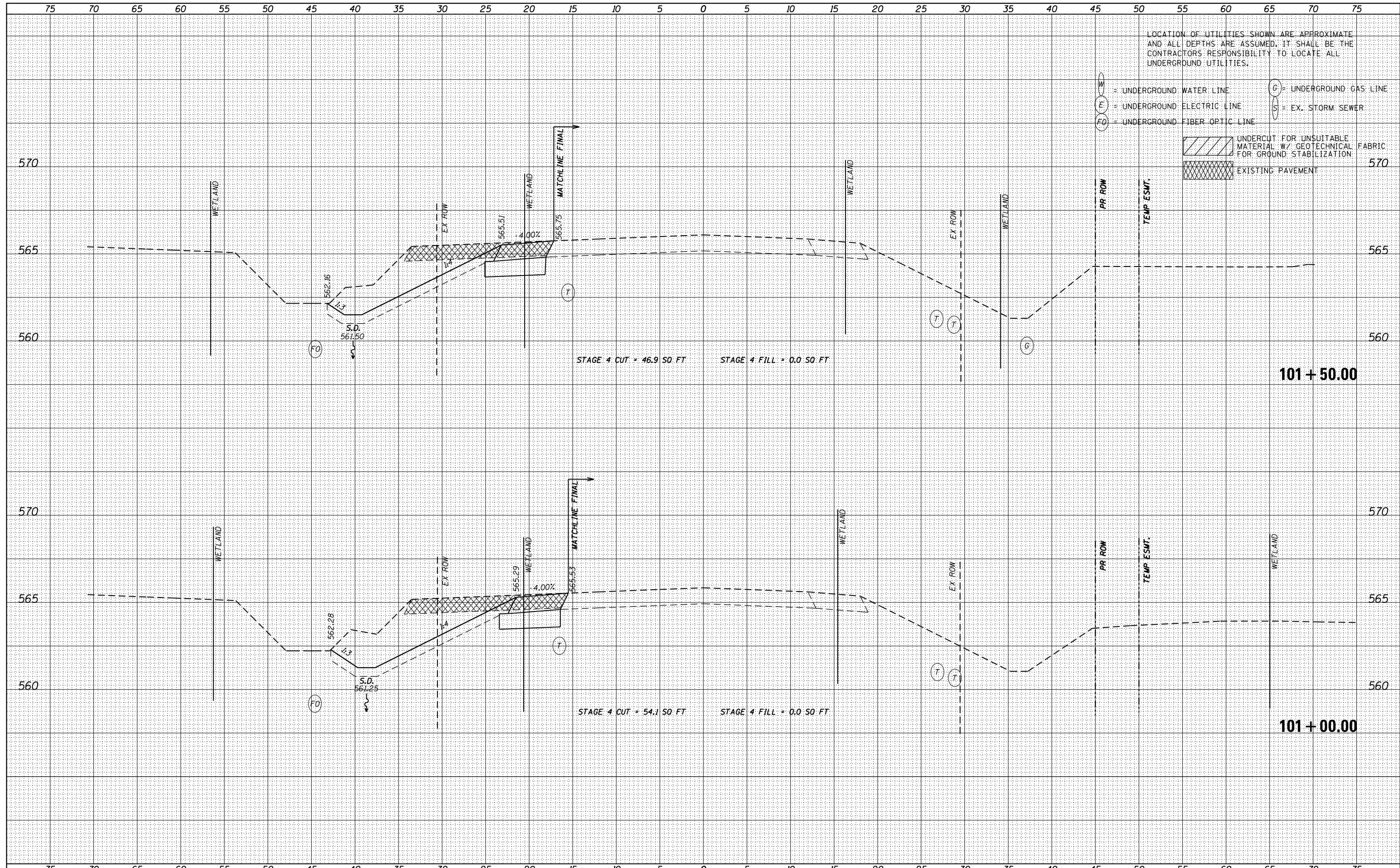
CROSS SECTIONS
IL ROUTE 53 STAGE 4

SCALE: 1"=5'4,2,5'V SHEET 8 OF 13 SHEETS STA. 100+45.93 TO STA.100+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	187
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
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 [Cross-hatched Box] EXISTING PAVEMENT



USER NAME = nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
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PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53 STAGE 4

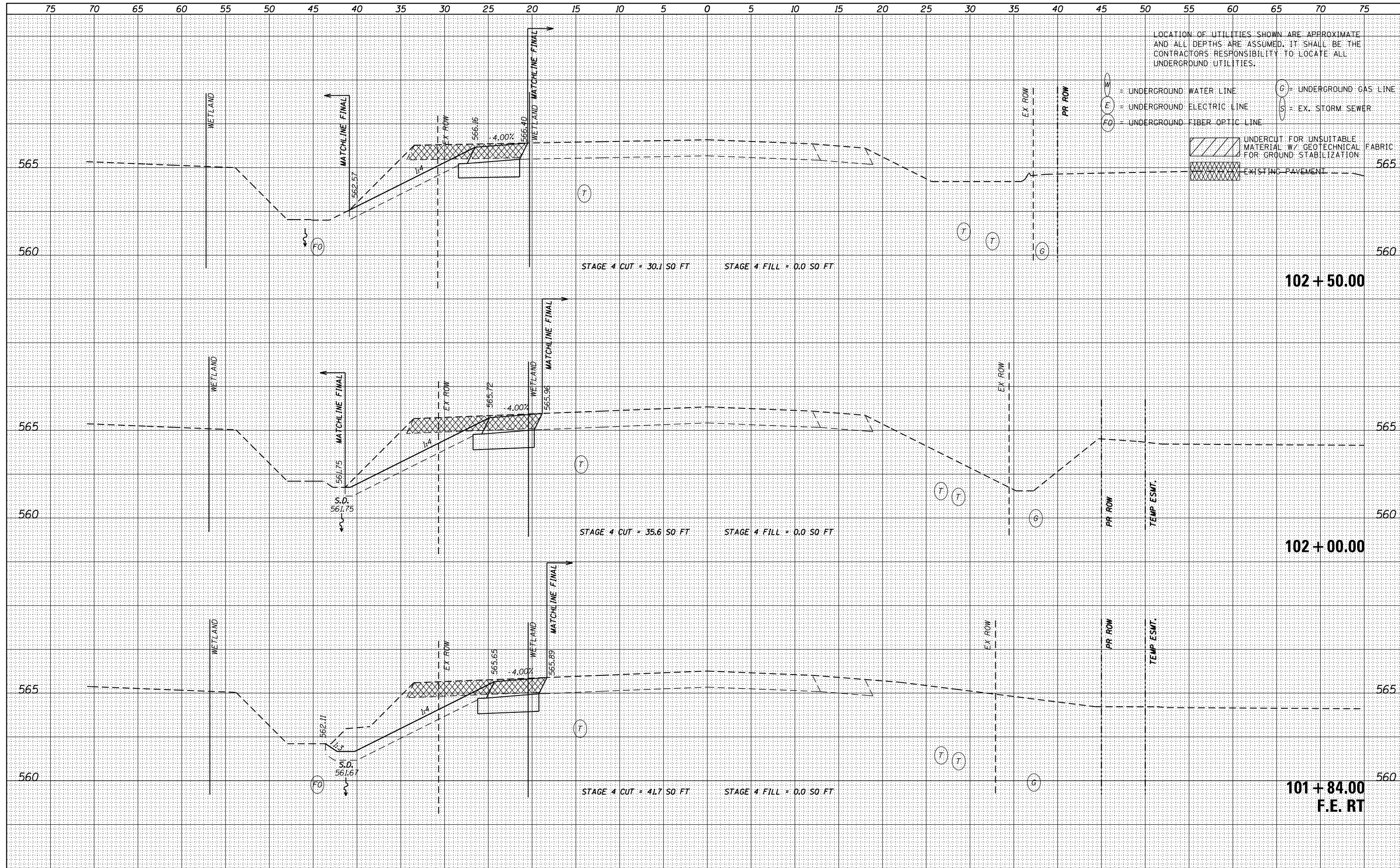
SCALE: 1"=5'H, 2.5"V SHEET 9 OF 13 SHEETS STA. 101+00.00 TO STA. 101+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	188
CONTRACT NO. 62B33				

ILLINOIS FED. AID PROJECT

DATE	
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DATE	
BY	
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AREAS CHECKED	
FINISH	
NO. SURVEY	
NO. SURVEY	
NO. SURVEY	
NO. SURVEY	



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN - BWL	REVISED -
PLOT DATE = 3/23/2016	CHECKED - NJB	REVISED -
	DATE - 02/05/2016	REVISED -

DESIGNED - JLM	REVISED -
DRAWN - BWL	REVISED -
CHECKED - NJB	REVISED -
DATE - 02/05/2016	REVISED -

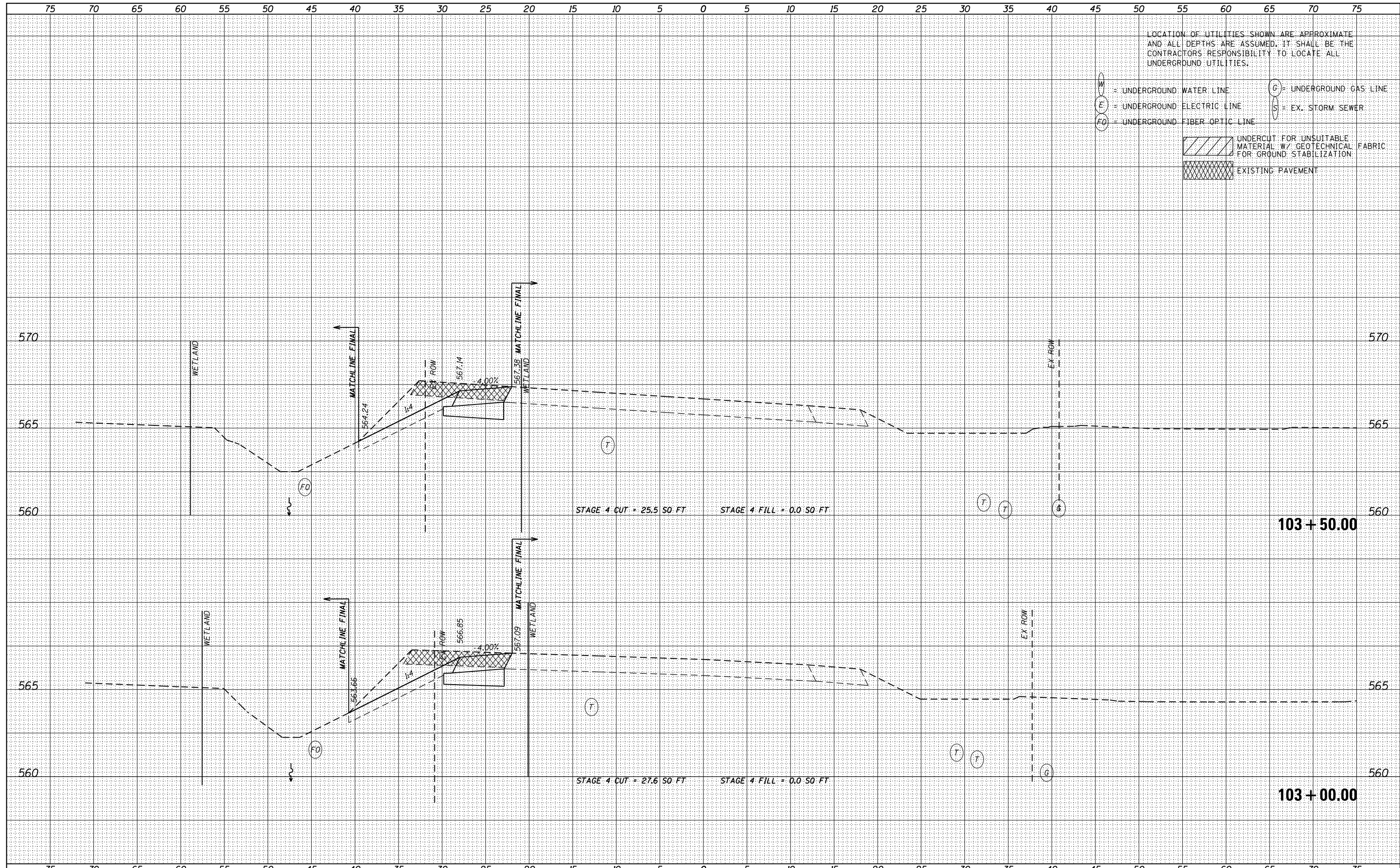
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS IL ROUTE 53 STAGE 4		
SCALE: 1"=5'4,2.5'V	SHEET 10	OF 13 SHEETS
STA. 101+84.00	TO STA. 102+50.00	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	189
CONTRACT NO. 62B33				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



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- EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

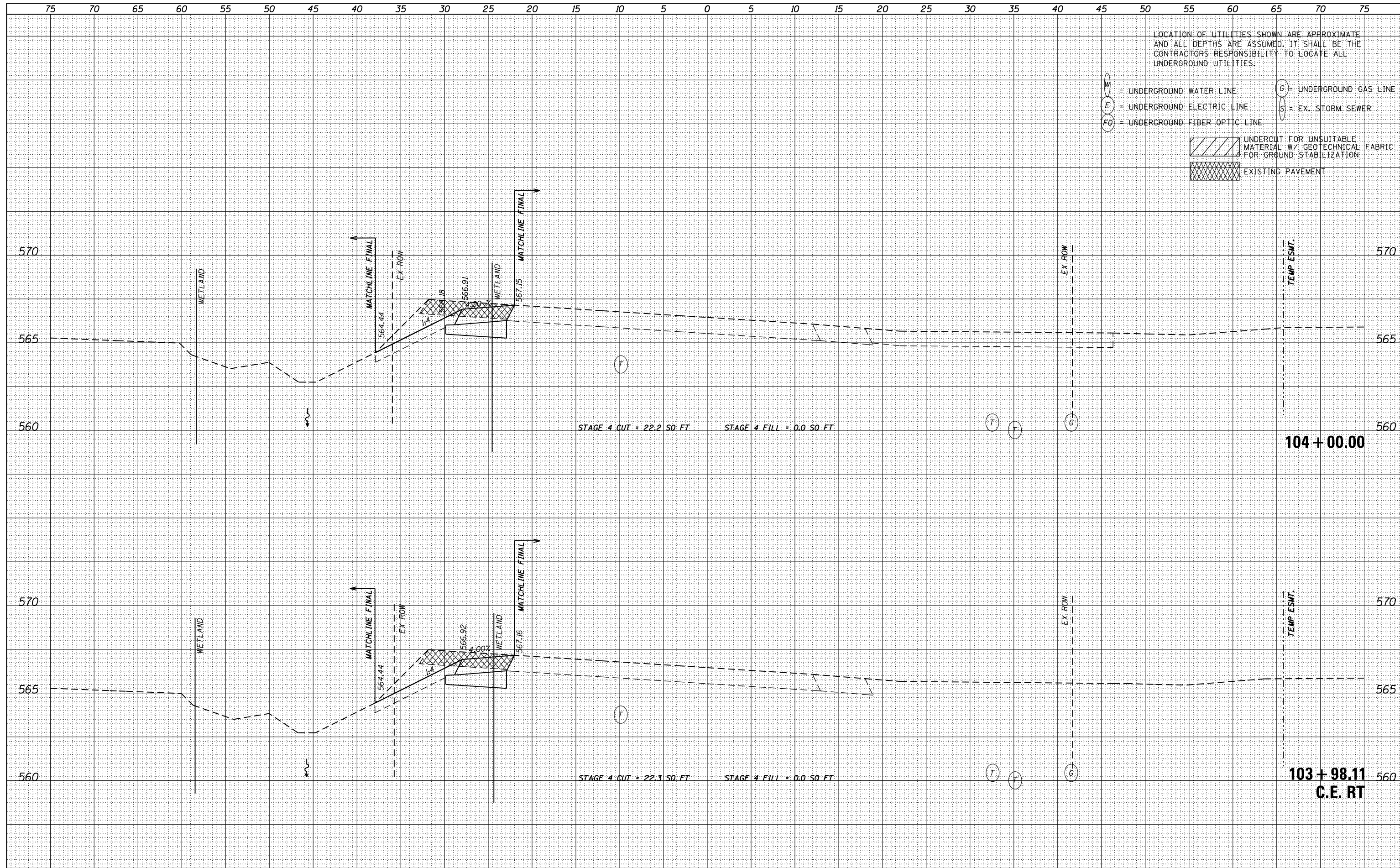
CROSS SECTIONS
IL ROUTE 53 STAGE 4
SCALE: 1"=5'4,2.5'V SHEET 11 OF 13 SHEETS STA. 103+00.00 TO STA. 103+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	190
CONTRACT NO. 62B33				

ILLINOIS FED. AID PROJECT

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
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ORIGINAL SURVEY	
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TEMPLATE	
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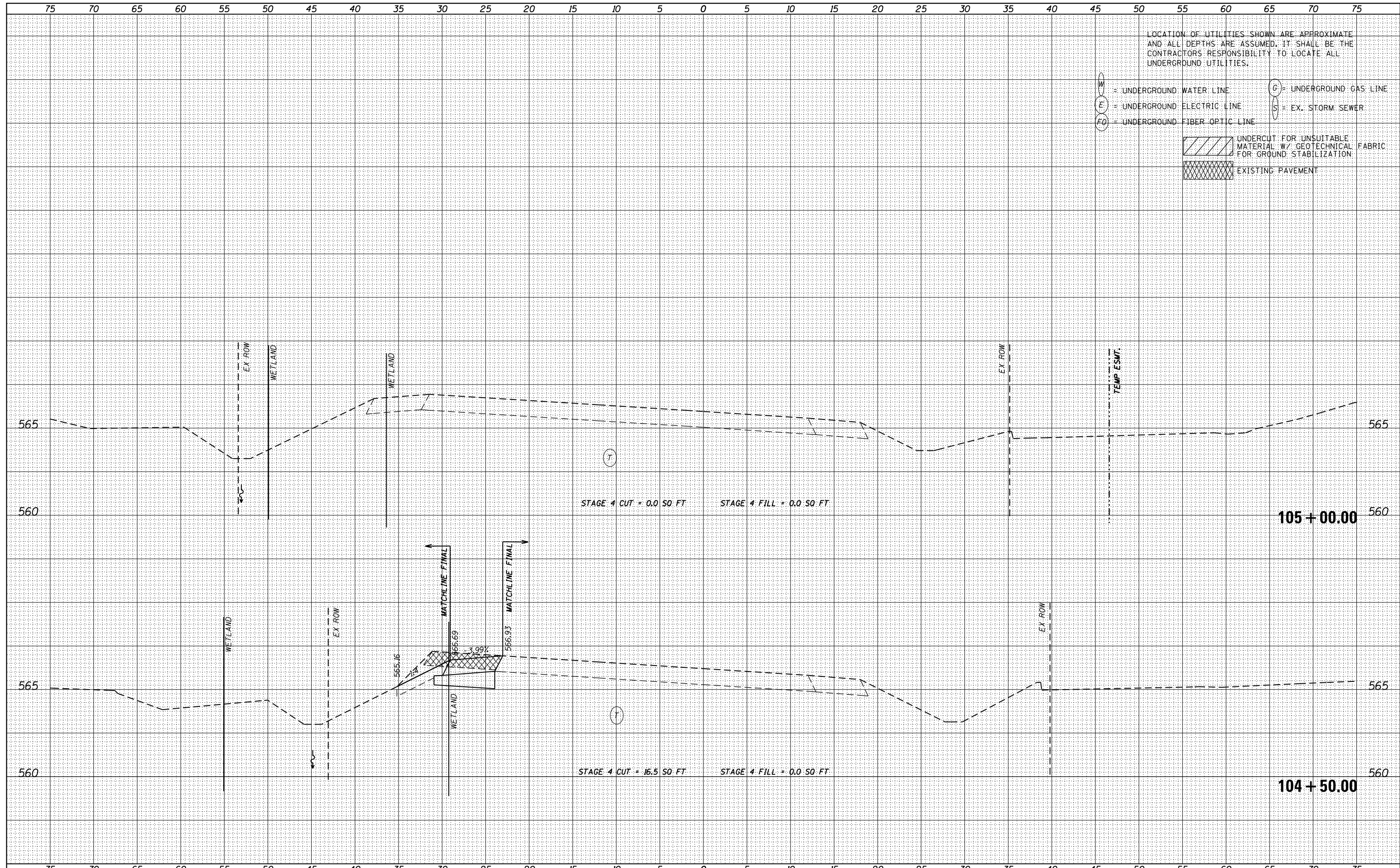
[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
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DATE	
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PLOTTED	
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- EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
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PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

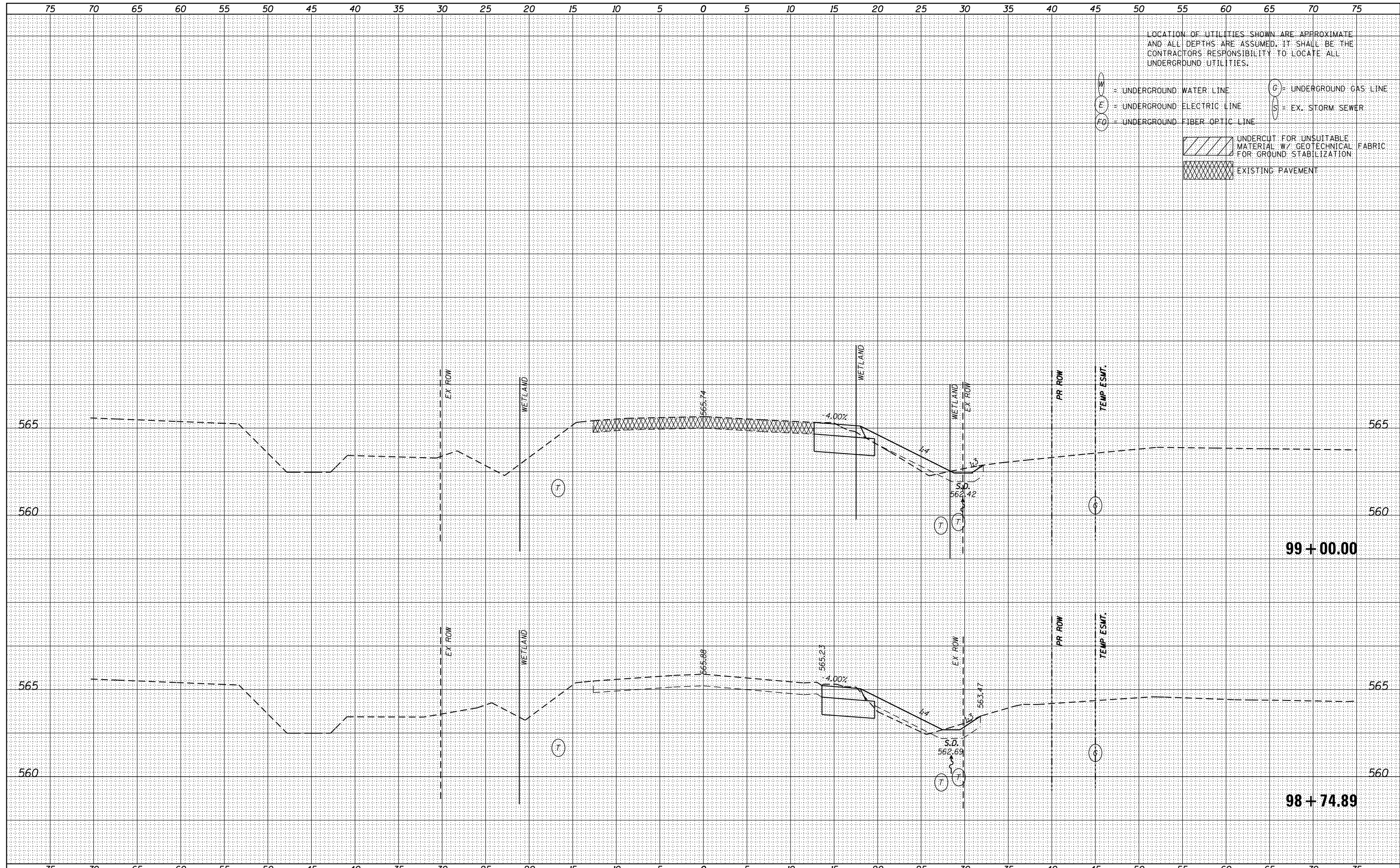
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS	
IL ROUTE 53 STAGE 4	
SCALE: 1"=5'4,2,5'V	SHEET 13 OF 13 SHEETS
STA. 104+50.00	TO STA. 105+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	192
CONTRACT NO. 62B33			ILLINOIS FED. AID PROJECT	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
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DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



LOCATION OF UTILITIES SHOWN ARE APPROXIMATE AND ALL DEPTHS ARE ASSUMED. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

- W = UNDERGROUND WATER LINE
- E = UNDERGROUND ELECTRIC LINE
- FO = UNDERGROUND FIBER OPTIC LINE
- G = UNDERGROUND GAS LINE
- S = EX. STORM SEWER
- UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53

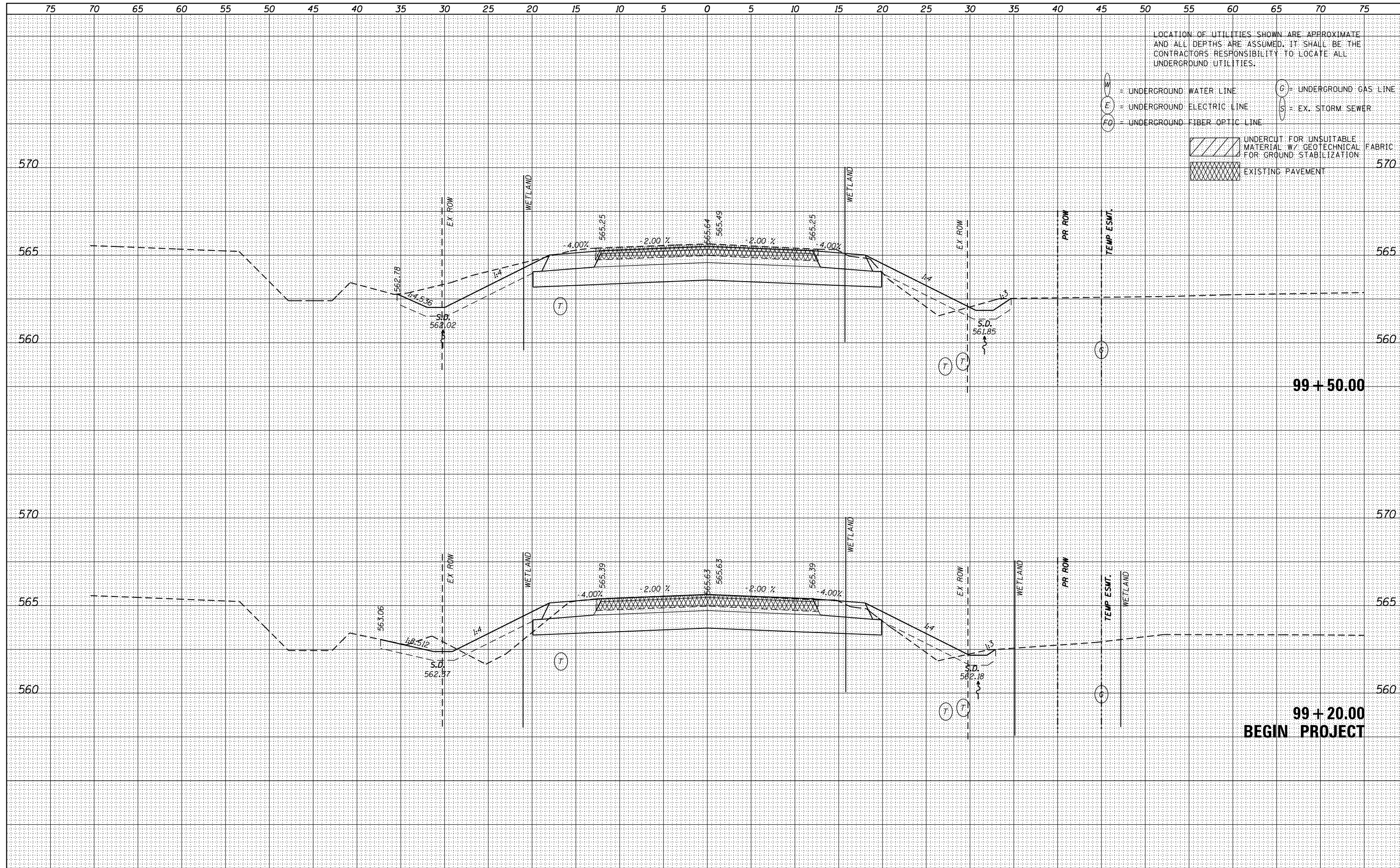
SCALE: 1"=5'4,2,5'V SHEET 1 OF 16 SHEETS STA. 98+74.89 TO STA. 99+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	193
				CONTRACT NO. 62B33

ILLINOIS FED. AID PROJECT

DATE	
BY	
FINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



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 FO = UNDERGROUND FIBER OPTIC LINE
 G = UNDERGROUND GAS LINE
 S = EX. STORM SEWER

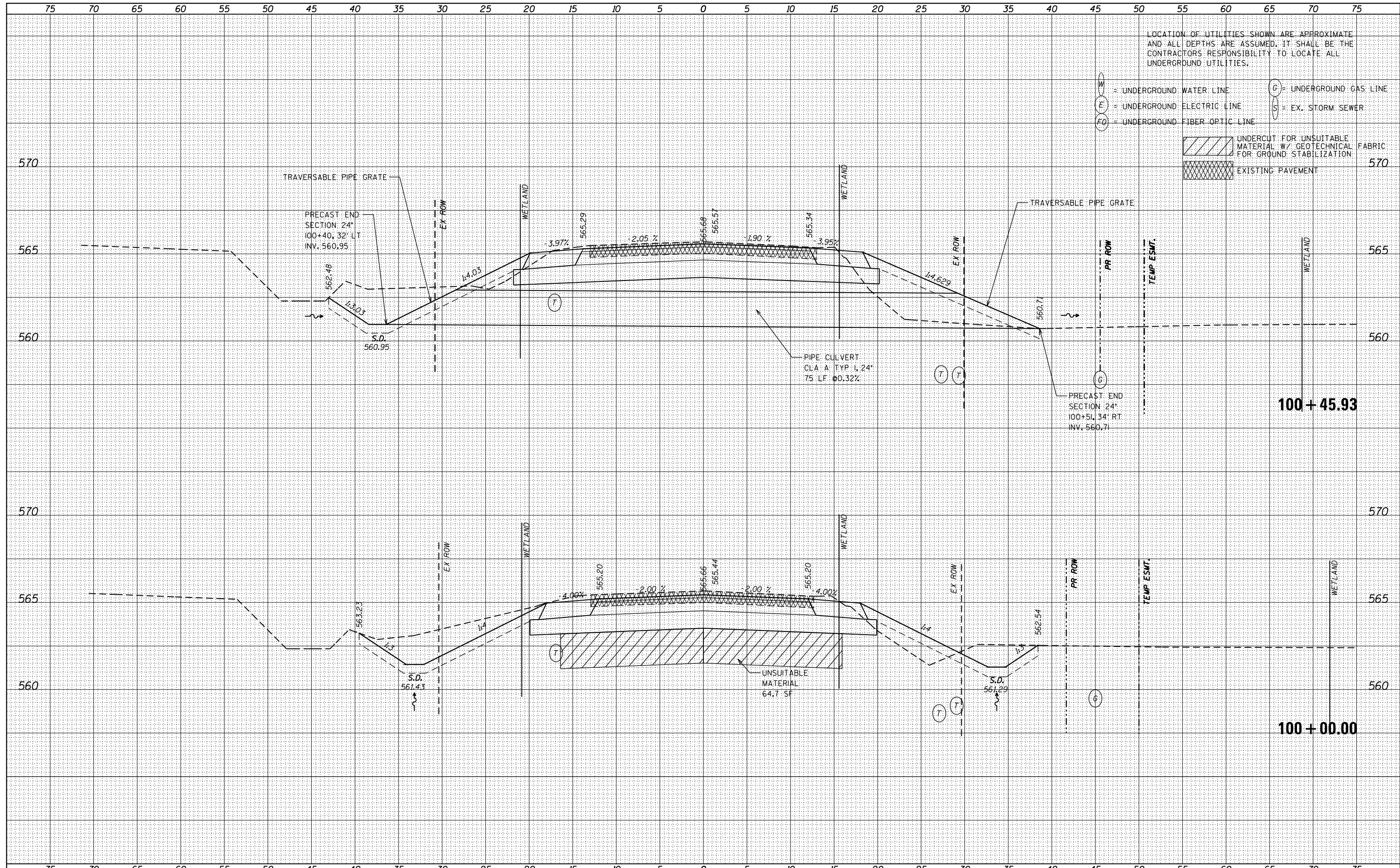
[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT

99 + 50.00

99 + 20.00
BEGIN PROJECT

DATE	
BY	
FINAL SURVEY	
SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS	
CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS	
CHECKED	
NO.	



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 G = UNDERGROUND GAS LINE
 S = EX. STORM SEWER

[Hatched Box] UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Box] EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 3/23/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

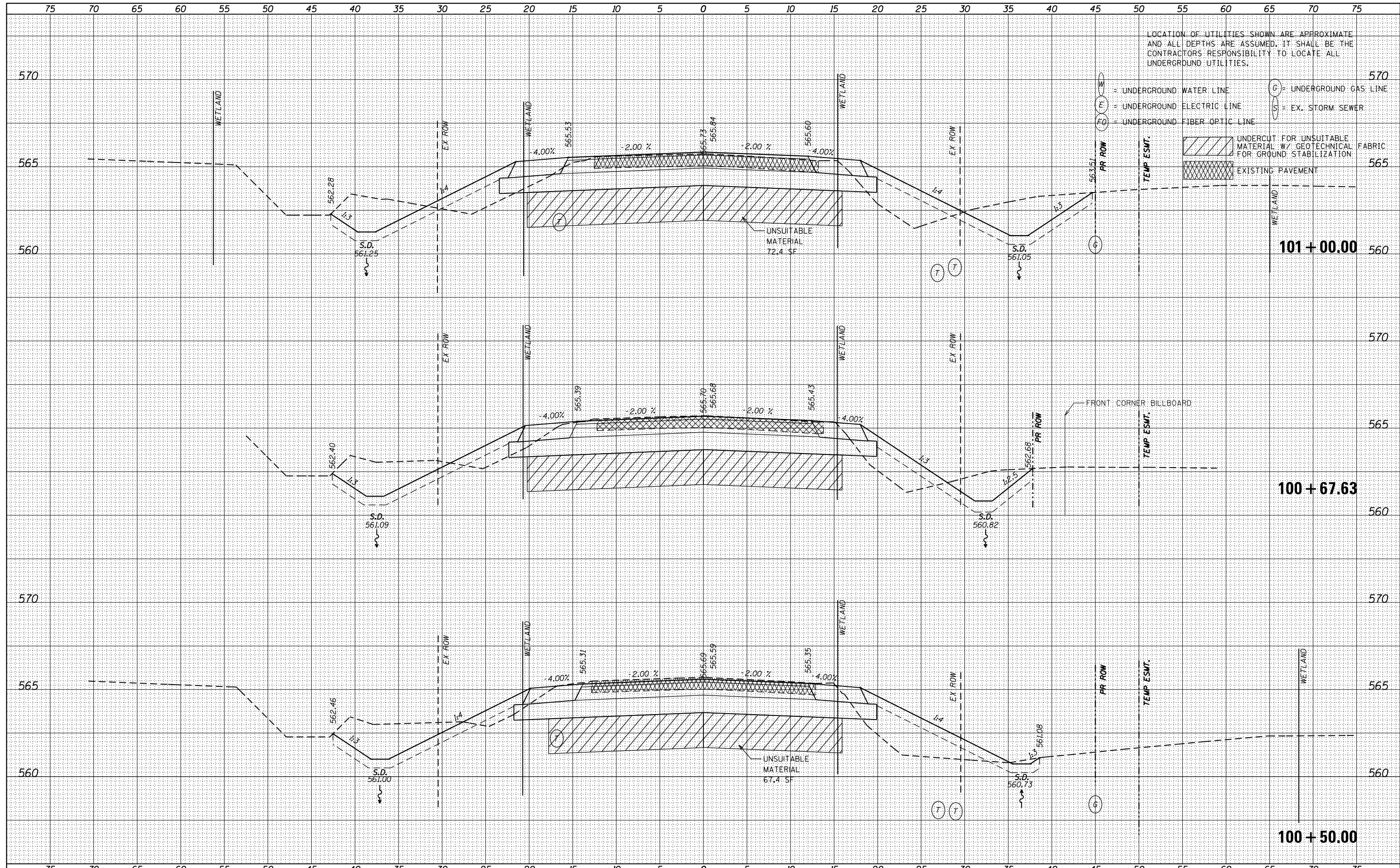
**CROSS SECTIONS
IL ROUTE 53**

SCALE: 1"=5'-4,2,5'V SHEET 3 OF 16 SHEETS STA. 100+00.00 TO STA. 100+45.93

F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 195
				CONTRACT NO. 62B33
ILLINOIS FED. AID PROJECT				

DATE	
BY	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	



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 FO = UNDERGROUND FIBER OPTIC LINE
 S = EX. STORM SEWER
 (G) = UNDERGROUND GAS LINE

[Hatched Area] = UNDERCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 [Cross-hatched Area] = EXISTING PAVEMENT



USER NAME = Erin Swanson	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE = 4/18/2016	DATE - 02/05/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

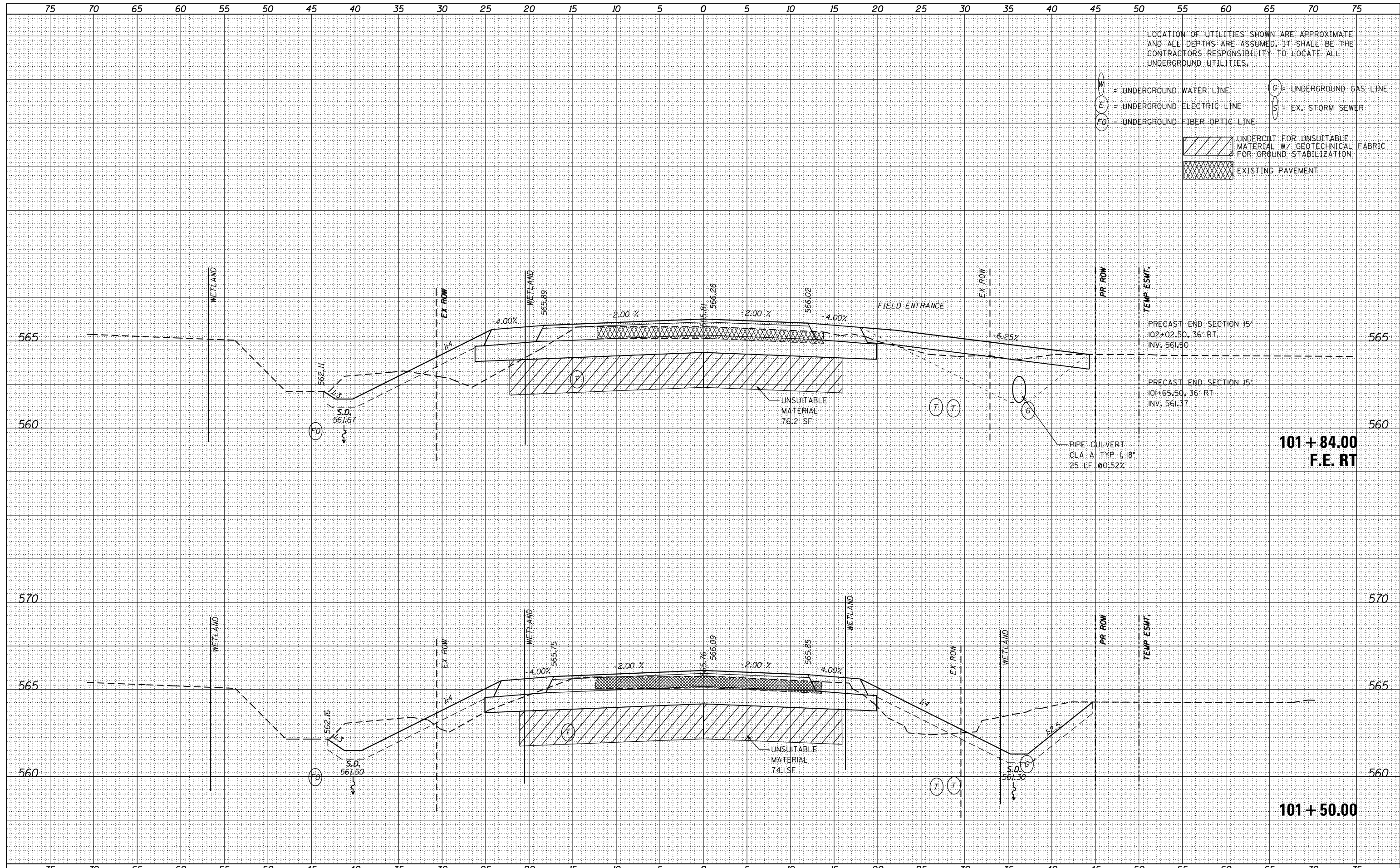
**CROSS SECTIONS
IL ROUTE 53**

SCALE: 1"=5'4,2.5'V SHEET 4 OF 16 SHEETS STA. 100+50.00 TO STA. 101+00.00

F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 196
CONTRACT NO. 62B33				ILLINOIS FED. AID PROJECT

DATE	
BY	
FINAL SURVEY	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	
NO.	



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- UNDCUT FOR UNSUITABLE MATERIAL W/ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- EXISTING PAVEMENT

PRECAST END SECTION 15'
102+02.50, 36' RT
INV. 561.50

PRECAST END SECTION 15'
101+65.50, 36' RT
INV. 561.37

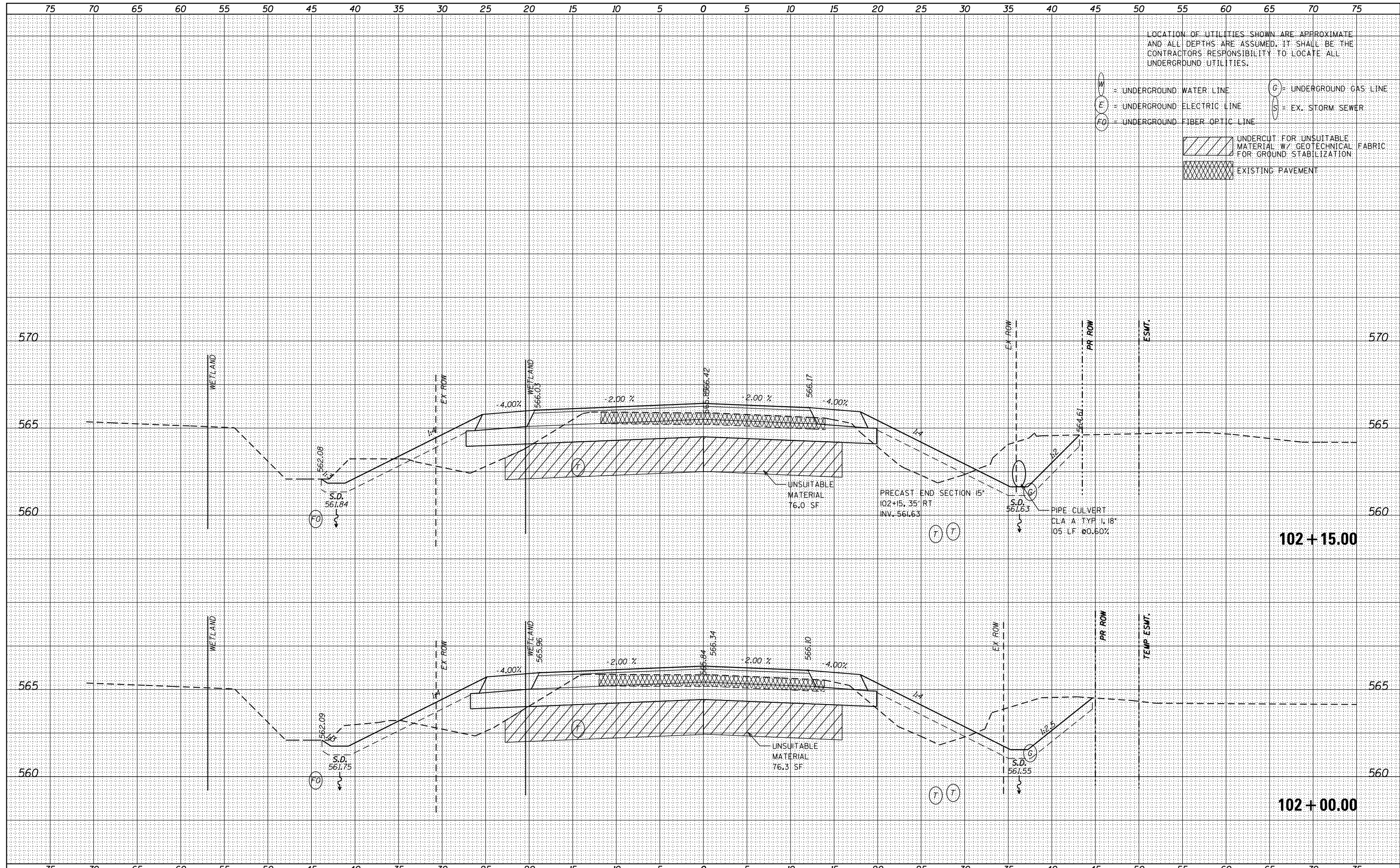
PIPE CULVERT
CLA: A TYP. 1.18'
25' LF @ 0.52%

101 + 84.00
F.E. RT

101 + 50.00

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

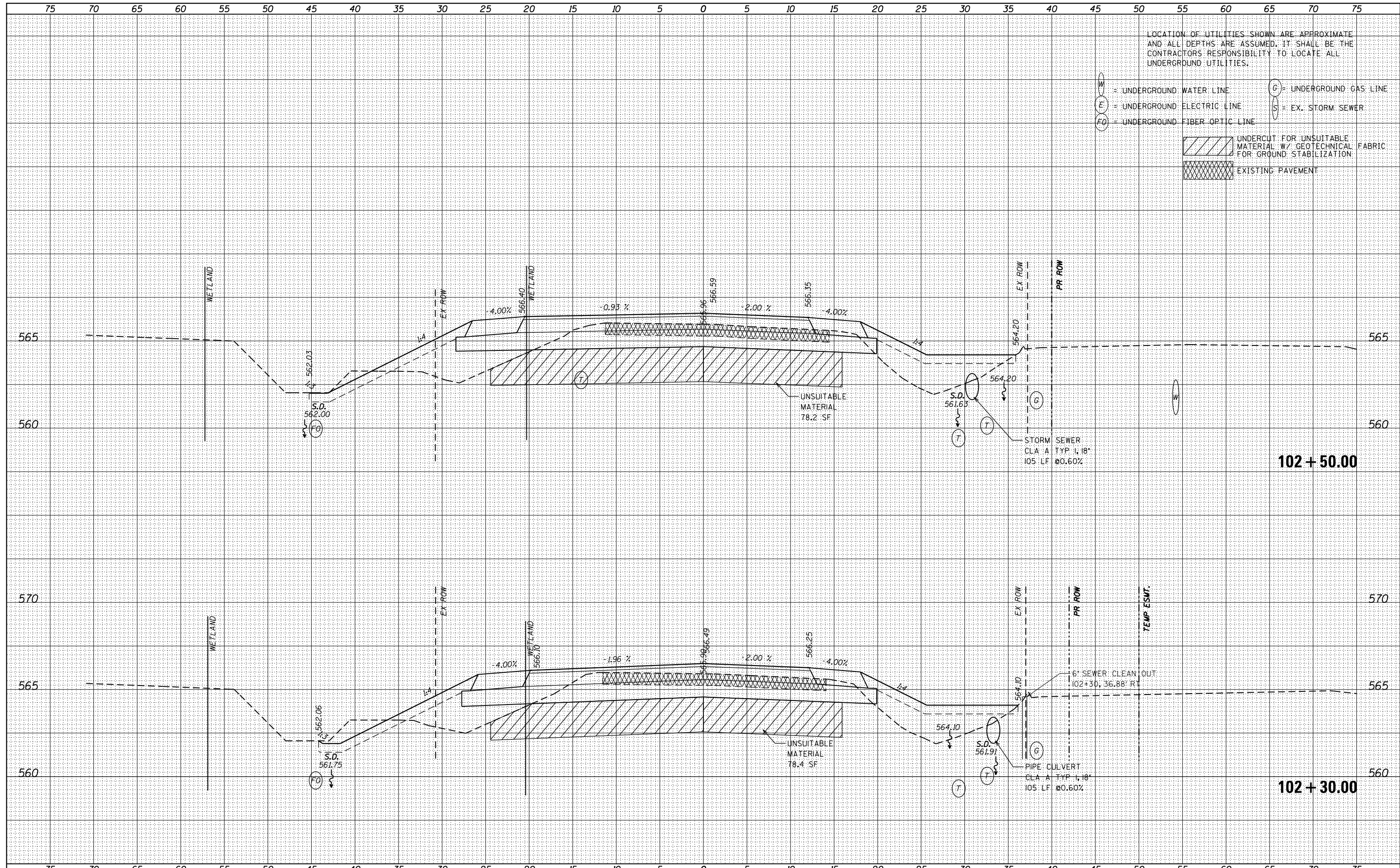
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS	
IL ROUTE 53	
SCALE: 1"=5'4,2.5'V	SHEET 6 OF 16 SHEETS
STA. 102+00.00 TO STA. 102+15.00	

F.A.P. RTE. 846	SECTION DIHSRR2016-02	COUNTY WILL	TOTAL SHEETS 580	SHEET NO. 198
CONTRACT NO. 62B33				ILLINOIS FED. AID PROJECT

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



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- EXISTING PAVEMENT



USER NAME	= nick.becker
DESIGNED	- JLM
DRAWN	- BWL
CHECKED	- NJB
DATE	- 02/05/2016
PLLOT SCALE	= 10.0000' / in.
PLLOT DATE	= 3/23/2016

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53

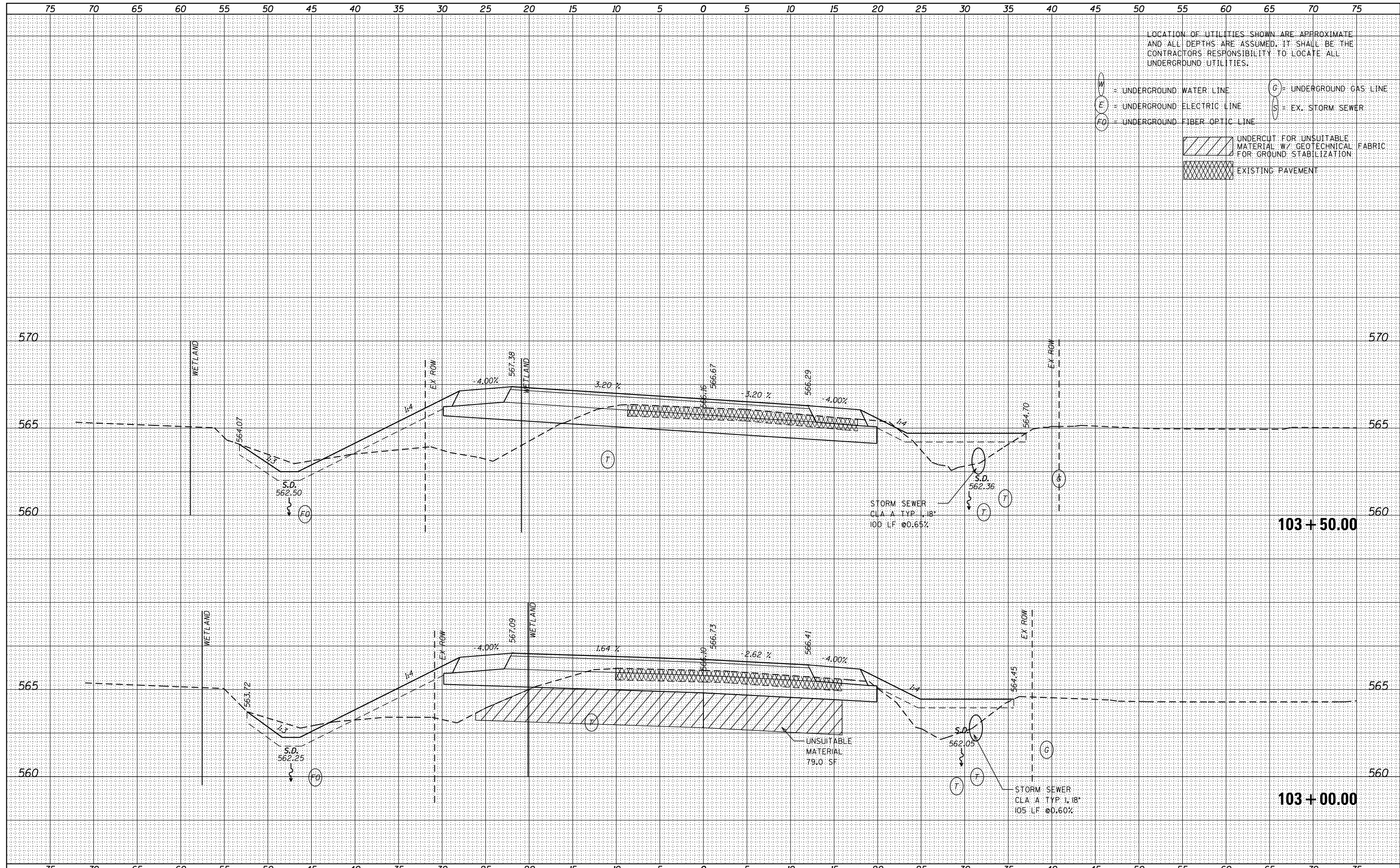
SCALE: 1"=5'H, 2.5'V SHEET 7 OF 16 SHEETS STA. 102+30.00 TO STA. 102+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	199
				CONTRACT NO. 62B33

ILLINOIS FED. AID PROJECT

DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
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AREAS CHECKED	



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 [Cross-hatched Area] EXISTING PAVEMENT



USER NAME - nick.becker	DESIGNED - JLM	REVISED -
	DRAWN - BWL	REVISED -
PLOT SCALE - 10.0000' / in.	CHECKED - NJB	REVISED -
PLOT DATE - 3/23/2016	DATE - 02/05/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 53
SCALE: 1"=5'4,2.5'V SHEET 8 OF 16 SHEETS STA. 103+00.00 TO STA.103+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
846	DIHSRR2016-02	WILL	580	200
				CONTRACT NO. 62B33

ILLINOIS FED. AID PROJECT