

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 ADA-2	CHAMPAIGN	44	1
ILLINOIS			CONTRACT NO. 70A47	

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR SUMMARY OF QUANTITIES, SEE SHEET NO. 3-4

CURRENT TRAFFIC DATA

E.A.P. ROUTE 808 / (US 150)

	ADT (2016)	PV %	SU %	MU %
LEG "A"	= 11,100	98.2	1.6	0.2
LEG "B"	= 12,300	94.9	4.3	0.8
LEG "C"	= 10,300	96.2	2.8	1.0
LEG "D"	= 13,100	96.1	2.8	1.1
LEG "E"	= 9,900	95.5	3.1	1.4

TRAFFIC DATA LOCATIONS

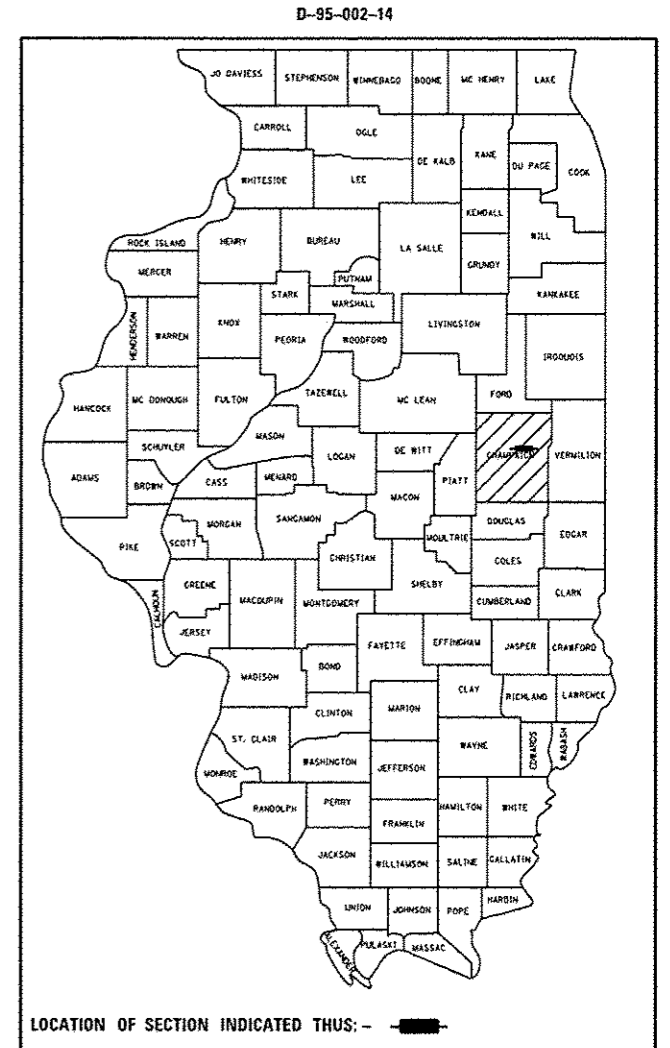
- LEG "A" = WATER ST. TO FIFTH ST.
- LEG "B" = SYCAMORE ST. TO WOODLAND PARK RD.
- LEG "C" = WOODLAND PARK RD. TO IL 130 SPUR
- LEG "D" = IL 130 SPUR TO DODSON DR.
- LEG "E" = DODSON DR. TO BERINGER CIRCLE

**PROPOSED
HIGHWAY PLANS**

F.A.P. ROUTE 808 / (U.S. 150)
SECTION D5 ADA-2
PROJECT: ACNHPP-0808(042)
ADA IMPROVEMENTS
CHAMPAIGN COUNTY

C-95-002-14

WATER STREET TO FIFTH STREET IN CHAMPAIGN AND
SYCAMORE STREET TO BERINGER CIRCLE IN URBANA



DESIGN DESIGNATION

OTHER PRINCIPLE ARTERIAL
(URBAN)

STATION EQUATION:
STA. 108+58.91 (BK) =
STA. 108+57.28 (AH)

STATION EQUATION:
STA. 175+08.29 (BK) =
STA. 12+84.23 (AH)

STATION EQUATION:
STA. 46+34.94 (BK) =
STA. 1+80.75 (AH)

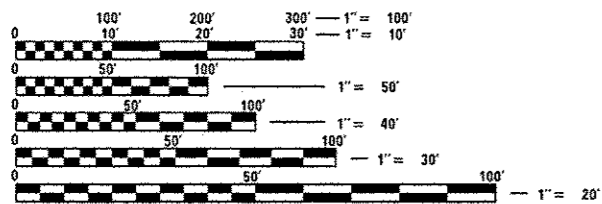
STATION EQUATION:
STA. 14+99.21 (BK) =
STA. 448+57.75 (AH)



SECTION D5 ADA-2
BEGINS: STA. 47+63.21

PROJECT OMISSION:
STA. 72+27.97 TO
STA. 174+16.01

SECTION D5 ADA-2
ENDS: STA. 495+31.55

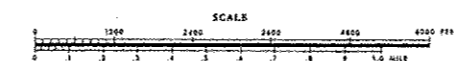


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

TOWNSHIPS: CHAMPAIGN CITY, CUNNINGHAM, AND URBANA

PROJECT ENGINEER: JASON STULTS
SQUAD LEADER: GREG EAGLIN
(217)465-4181
CONTRACT NO. 70A47



GROSS LENGTH = 22,089.68 FT. = 4.184 MILES
NET LENGTH = 11,900.01 FT. = 2.254 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED MARCH 17, 2016
Kensil A. Gurnett
REGION THREE ENGINEER

Maureen M. Addis PE
ENGINEER OF DESIGN AND ENVIRONMENT

Maureen M. Addis
DIRECTOR OF PROGRAM DEVELOPMENT

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OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
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44	DETAIL FOR TRAFFIC CONTROL & PROTECTION DEVICES (ROAD & SIDEROAD / STREET CLOSURES)

LIST OF STANDARDS

STANDARD NO.	NAME OF STANDARD
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
420001-08	PAVEMENT JOINTS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-02	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-02	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424021-03	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
602301-04	INLET - TYPE A
602306-03	INLET - TYPE B
602601-04	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
604001-04	FRAME AND LIDS TYPE 1
604051-04	FRAME AND GRATE TYPE 11
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701502-06	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701602-07	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-05	TRAFFIC CONTROL DEVICES

GENERAL NOTES

G.N. - 100
ENGLISH UNITS OF MEASUREMENT SHALL GOVERN OVER AND SUPERSEDE ANY METRIC UNITS SHOWN IN THIS CONTRACT. WHERE INCLUDED, METRIC UNITS ARE FOR INFORMATION ONLY.

G.N.-100A
ELECTRONIC FILES AND/OR ELECTRONIC SURVEY INFORMATION INCLUDING CADD FILES WILL NOT BE AVAILABLE TO THE CONTRACTOR.

G.N.-105.09A
ALL ELEVATIONS SHOWN IN THE PLANS ARE BASED ON NORTH AMERICAN VERTICAL DATUM OF 1988. (NAVD88)

G.N.-202
GRADING SHALL BE DONE BY HAND AROUND LIGHT POLES, UTILITY POLES SIGN POSTS, SHRUBS, TREES OR OTHER NATURAL OR MAN-MADE OBJECTS WHERE SHALLOW FILLS OR CUTS ARE ADJACENT TO THE ITEMS. ITS IS THE INTENT THAT THE LIMITS OF CONSTRUCTION BE SUCH AS TO PRESERVE IN THE ORIGINAL STATE AS MUCH AREA OF EXISTING RIGHT-OF-WAY AS POSSIBLE. THE DECISION AS TO ITEMS TO REMAIN IN PLACE SHALL BE AS DIRECTED BY THE ENGINEER.

THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED IN THE CONTRACT UNTIL PRICE PER CUBIC YARD FOR EARTH EXCAVATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

G.N.-280 (SPECIAL)
TEMPORARY EROSION CONTROL SEEDING IS INCLUDED IN THIS CONTRACT TO SEED DISTURBED EARTH DURING TIME PERIODS WHEN PERMANENT SODDING IS NOT ALLOWED. SOME OR ALL OF THE TEMPORARY EROSION CONTROL SEEDING WILL BE DELETED IF IT IS POSSIBLE TO PLACE PERMANENT SODDING ON EARTH AT THE TIME OF THEIR COMPLETION.

G.N.-440B
THE EXISTING TIE BARS BETWEEN THE EXISTING PAVEMENT AND EXISTING MEDIANS, GUTTERS AND / OR COMBINATION CURB AND GUTTERS THAT ARE FOUND SUITABLE FOR REUSE SHALL BE CLEANED, STRAIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY EXISTING TIE BARS THAT ARE FOUND UNSUITABLE TO BE INCORPORATED INTO THE PROPOSED CONSTRUCTION DUE TO EXCESSIVE RUSTING OR DISTRESS SHALL BE REMOVED FLUSH WITH THE FACE OF THE EXISTING CONCRETE AND DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY IN ACCORDANCE WITH ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS.

THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE VARIOUS REMOVAL PAY ITEMS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

G.N.-550
BEFORE ORDERING STORM SEWERS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR THE EXACT LENGTHS.

G.N.-551
EXISTING STORM SEWERS SHALL BE REMOVED IN ACCORDANCE WITH SECTION 551 OF THE STANDARD SPECIFICATIONS EXCEPT THAT THE CONTRACTOR WILL NOT BE REQUIRED TO SALVAGE ANY EXISTING SEWER PIPE.

G.N.-1004.01
COARSE AGGREGATE GRADATION CA-10 MAY BE USED WHENEVER COARSE AGGREGATE CA-6 IS SPECIFIED IN THE STANDARD SPECIFICATIONS.

COMMITMENTS

THERE ARE NO COMMITMENTS FOR THIS CONTRACT.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS / LIST OF STANDARDS GENERAL NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pu\131864EBID\INTEG\Illinois.gov\PI007\00	Documents\1007 Offices\District 5\Projects\057	PROGRAM\DATA\Design\0570447-shr-Index.dgn	REVISED -			808	05 ADA-2	CHAMPAIGN	44	2	
PLOT SCALE = 48.0000' / in.	CHECKED -	REVISED -	SCALE:			SHEET 1	OF 1	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT
#MODELNAME#	PLOT DATE = 3/17/2016	DATE -	REVISED -			CONTRACT NO. 70A47					

SUMMARY OF QUANTITIES

SHEET 1 OF 2

LOCATION OF WORK: FAP 808 / US 150
 URBAN
 COUNTY: CHAMPAIGN
 STA. 47+63.21
 TO
 STA. 495+31.55

FUNDING BREAKOUT: 80% FEDERAL
 20% STATE

CONSTRUCTION TYPE CODE: 0021

LOCATION OF WORK: FAP 808 / US 150
 URBAN
 COUNTY: CHAMPAIGN
 STA. 47+63.21
 TO
 STA. 495+31.55

FUNDING BREAKOUT: 80% FEDERAL
 20% STATE

CONSTRUCTION TYPE CODE: 0021

CODE NO	ITEM	UNIT	TOTAL QUANTITY
20200100	EARTH EXCAVATION	CU YD	45.0
20800150	TRENCH BACKFILL	CU YD	33.0
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	5.0
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	5.0
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	5.0
25200110	SODDING, SALT TOLERANT	SQ YD	375.0
25200200	SUPPLEMENTAL WATERING	UNIT	5.0
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	8.0
42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SQ FT	5,391.0
42400800	DETECTABLE WARNINGS	SQ FT	401.0
44000100	PAVEMENT REMOVAL	SQ YD	13.0
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	733.0
44000600	SIDEWALK REMOVAL	SQ FT	4,897.0

* SPECIALTY ITEM

CODE NO	ITEM	UNIT	TOTAL QUANTITY
44201353	CLASS C PATCHES, TYPE II, 10 INCH	SO YD	20.0
44201359	CLASS C PATCHES, TYPE IV, 10 INCH	SO YD	30.0
550A2320	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 12"	FOOT	70.0
55100500	STORM SEWER REMOVAL 12"	FOOT	24.0
56109210	WATER VALVES TO BE ADJUSTED	EACH	4.0
60235300	INLETS, TYPE A, TYPE 1 FRAME, CLOSED LID	EACH	2.0
60236800	INLETS, TYPE A, TYPE 11 FRAME AND GRATE	EACH	2.0
60240310	INLETS, TYPE B, TYPE 11 FRAME AND GRATE	EACH	2.0
60261300	INLETS TO BE ADJUSTED WITH NEW TYPE 11 FRAME AND GRATE	EACH	9.0
60300350	MANHOLE FRAMES TO BE ADJUSTED	EACH	2.0
60500060	REMOVING INLETS	EACH	2.0
60500305	FILLING INLETS	EACH	2.0
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	416.0

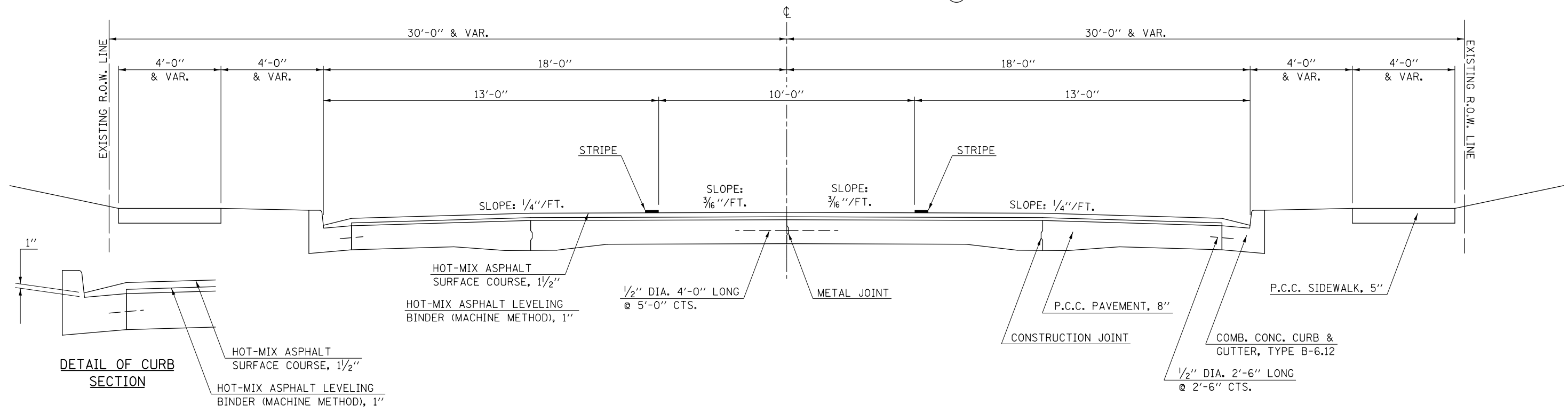
* SPECIALTY ITEM

FILE NAME =	USER NAME = eeginga	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
\\11084EB\INTEG\Illinois.gov\PI\DOT\...	\\11084EB\INTEG\Illinois.gov\PI\DOT\...	CHECKED -	REVISED -			808	05 ADA-2	CHAMPAIGN	44	3
MODELNAME#	DATE -	DATE -	DATE -			CONTRACT NO. 70A47			ILLINOIS FED. AID PROJECT	
						SCALE:	SHEET 1 OF 2 SHEETS	STA.	TO STA.	

(A) EXISTING TYPICAL CROSS SECTION

PROJECT OMISSION:
STA. 72+27.97 TO STA. 174+16.01

STATION TO STATION
47+63.21 TO 72+27.97 (B)

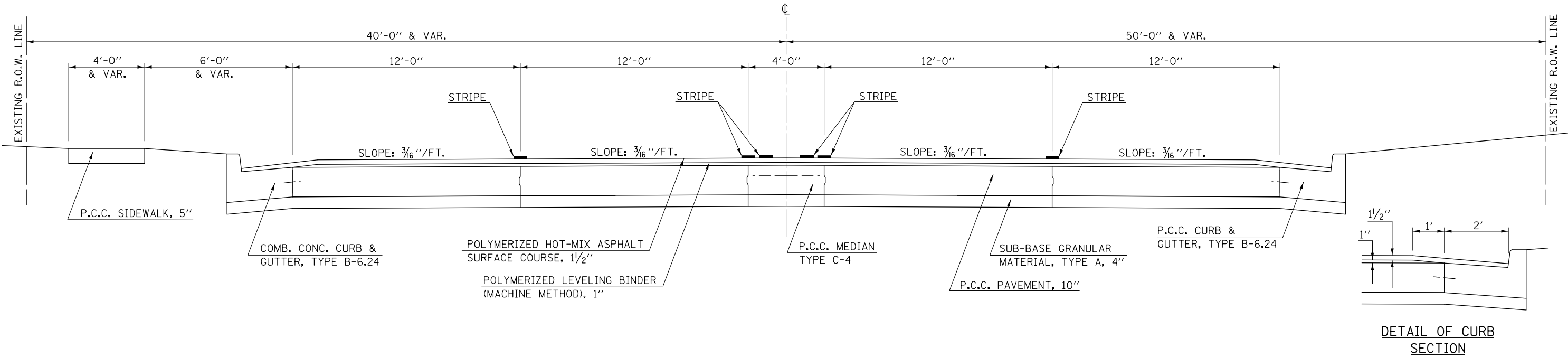


(B) EXISTING TYPICAL CROSS SECTION

PROJECT OMISSION:
STA. 72+27.97 TO STA. 174+16.01

STATION EQUATIONS:
STA. 175+08.29 (BK) = STA. 12+84.23 (AH)
STA. 46+34.94 (BK) = STA. 1+80.75 (AH)
STA. 14+99.21 (BK) = STA. 448+57.75 (AH)

STATION TO STATION
(A) 174+16.01 TO 175+08.29 (BK)
12+84.23 (AH) TO 46+34.94 (BK)
1+80.75 (AH) TO 14+99.21 (BK) (C)

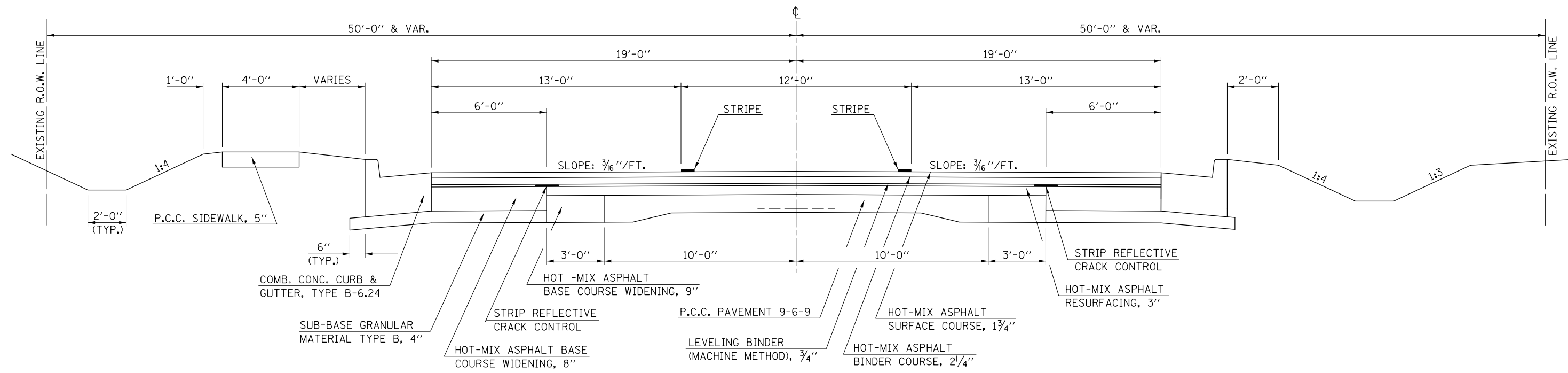


FILE NAME =	USER NAME = eaglino	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL CROSS SECTIONS	F.A.P. RTE. 808	SECTION D5 ADA-2	COUNTY CHAMPAIGN	TOTAL SHEETS 44	SHEET NO. 5
PLOT SCALE = 40.0000' / in. #MODELNAME#				SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.		CONTRACT NO. 70A47 ILLINOIS FED. AID PROJECT				

(C) EXISTING TYPICAL CROSS SECTION

STATION 448+57.75 (AH) TO STATION 495+31.55

STATION EQUATION:	
STA. 14+99.21 (BK) =	
STA. 448+57.75 (AH)	



SCHEDULE OF QUANTITIES

SHEET 1 OF 3

PEDESTRIAN RAMPS (ADA)

			44000500	44000600	44000100	60603800	60605000	42400200	42400800	60261300	60300350	X8140115	56109210
			COMBINATION CURB & GUTTER REMOVAL	SIDEWALK REMOVAL	PAVEMENT REMOVAL	COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12	COMBINATION CONCRETE CURB & GUTTER TYPE B-6.24	PORTLAND CEMENT CONCRETE SIDEWALK, 5"	DETECTABLE WARNINGS	INLETS TO BE ADJUSTED W/ NEW TYPE 11 FRAME & GRATE	MANHOLE FRAMES TO BE ADJUSTED	HANDHOLE TO BE ADJUSTED	WATER VALVES TO BE ADJUSTED
LOCATION			(FT)	(SQ FT)	(SQ YD)	(FT)	(FT)	(SQ FT)	(SQ FT)	(EACH)	(EACH)	(EACH)	(EACH)
1	WATER ST.	N.W. QUADRANT	51.0	275.0		51.0		275.0	17.0				
2	WATER ST.	N.E. QUADRANT	30.0	152.0		30.0		160.0	10.0	1.0			
3	LOCUST ST.	N.W. QUADRANT	31.0	152.0		31.0		164.0	16.0	1.0			
4	LOCUST ST.	N.E. QUADRANT	31.0	256.0		31.0		252.0	17.0				
5	LOCUST ST.	S.W. QUADRANT	33.0	176.0		33.0		176.0	16.0	1.0			
6	LOCUST ST.	S.E. QUADRANT	38.0	254.0		38.0		258.0	20.0				
7	FOURTH ST.	S.W. QUADRANT	27.0	275.0		27.0		298.0	26.0			1.0	
8	FOURTH ST.	S.E. QUADRANT	42.0	340.0		42.0		340.0	19.0			1.0	
9	FIFTH ST.	N.W. QUADRANT	34.0	212.0		34.0		220.0	20.0	1.0			
10	FIFTH ST.	N.E. QUADRANT	38.0	225.0		38.0		234.0	22.0	1.0	2.0		
11	FIFTH ST.	S.W. QUADRANT	31.0	176.0		31.0		178.0	20.0	1.0			
12	FIFTH ST.	S.E. QUADRANT	30.0	190.0		30.0		218.0	20.0	1.0			
13	SYCAMORE ST.	N.W. QUADRANT	20.0	104.0			20.0	90.0	8.0	1.0			
14	SYCAMORE ST.	N.E. QUADRANT	22.0	306.0			22.0	306.0	18.0	1.0			1.0
15	ASH ST.	N.W. QUADRANT	19.0	98.0			19.0	96.0	10.0				
16	ASH ST.	N.E. QUADRANT	62.0	280.0			62.0	280.0	15.0				
17	HICKORY ST.	N.W. QUADRANT	18.0	118.0			18.0	119.0	8.0				
18	HICKORY ST.	N.E. QUADRANT	18.0	168.0			18.0	198.0	8.0				1.0
19	COTTAGE GROVE	N.W. QUADRANT	11.0	172.0			11.0	178.0	9.0				
20	COTTAGE GROVE	N.E. QUADRANT	10.0	30.0			10.0	30.0	8.0				
21	DALE LANE	N.W. QUADRANT	14.0	72.0			14.0	72.0	8.0				
22	DALE LANE	N.E. QUADRANT	19.0	94.0			19.0	100.0	14.0				
23	SMITH ROAD	N.W. QUADRANT	17.0	116.0			17.0	116.0	12.0				
24	SMITH ROAD	N.E. QUADRANT	13.0	80.0			13.0	80.0	8.0				1.0
25	DODSON DRIVE	N.W. QUADRANT	15.0	108.0			15.0	110.0	8.0				
26	DODSON DRIVE	N.E. QUADRANT	15.0	76.0			15.0	78.0	8.0				1.0
27	ABBEY ROAD	N.W. QUADRANT	3.0	83.0	3.0		12.0	83.0	8.0				
28	ABBEY ROAD	N.E. QUADRANT	6.0	88.0	2.0		11.0	88.0	8.0				
29	BERINGER CIRCLE	N.W. QUADRANT	23.0	125.0	4.0		23.0	314.0	10.0				
30	BERINGER CIRCLE	N.E. QUADRANT	12.0	96.0	4.0		24.0	280.0	10.0				
TOTAL =			733.0	4897.0	13.0	416.0	343.0	5391.0	401.0	9.0	2.0	2.0	4.0
			FT	SQ FT	SQ YD	FT	FT	SQ FT	SQ FT	EACH	EACH	EACH	EACH

STORM SEWER

			55100500	550A2320	60235300	60236800	60240310	60500305	60500060	X0325405	20800150	44201353	44201359
			STORM SEWER REMOVAL, 12"	STORM SEWER R. GASKET CLASS A TYPE 1, 12"	INLETS TYPE A TYPE 1 FRAME CLOSED LID	INLETS TYPE A TYPE 11 FRAME & GRATE	INLETS TYPE B TYPE 11 FRAME & GRATE	FILLING INLETS	REMOVING INLETS	FILL EXISTING STORM SEWERS	TRENCH BACKFILL	CLASS C PATCHES, TYPE II, 10"	CLASS C PATCHES, TYPE IV, 10"
LOCATION			(FOOT)	(FOOT)	(EACH)	(EACH)	(EACH)	(EACH)	(EACH)	(CU YD)	(CU YD)	(SQ YD)	(SQ YD)
1	WATER ST.	N.W. QUADRANT	6.0	12.0	1.0	1.0		1.0		0.5	3.0	10.0	
16	ASH ST.	N.E. QUADRANT	8.0	8.0			1.0		1.0		15.0	10.0	
17	HICKORY ST.	N.W. QUADRANT		38.0		1.0		1.0		1.5	8.0		30.0
18	HICKORY ST.	N.E. QUADRANT	10.0	12.0	1.0		1.0		1.0		7.0		
TOTAL =			24.0	70.0	2.0	2.0	2.0	2.0	2.0	2.0	33.0	20.0	30.0
			FOOT	FOOT	EACH	EACH	EACH	EACH	EACH	CU YD	CU YD	SQ YD	SQ YD

SCHEDULE OF QUANTITIES

SHEET 2 OF 3

EARTH EXCAVATION

			25200200			
			EARTH EXCAVATION	EARTH EXCAVATION ADJUSTED FOR SHRINKAGE	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
LOCATION			(CU YD)	(CU YD)	(CU YD)	(CU YD)
1	WATER ST.	N.W. QUADRANT	1.00	0.75	0.00	0.75
2	WATER ST.	N.E. QUADRANT	1.00	0.75	0.00	0.75
3	LOCUST ST.	N.W. QUADRANT	1.00	0.75	0.00	0.75
4	LOCUST ST.	N.E. QUADRANT	1.00	0.75	0.00	0.75
5	LOCUST ST.	S.W. QUADRANT	1.00	0.75	0.00	0.75
6	LOCUST ST.	S.E. QUADRANT	2.00	1.50	0.00	1.50
7	FOURTH ST.	S.W. QUADRANT	2.00	1.50	0.00	1.50
8	FOURTH ST.	S.E. QUADRANT	2.00	1.50	0.00	1.50
9	FIFTH ST.	N.W. QUADRANT	2.00	1.50	0.00	1.50
10	FIFTH ST.	N.E. QUADRANT	1.00	0.75	0.00	0.75
11	FIFTH ST.	S.W. QUADRANT	1.00	0.75	0.00	0.75
12	FIFTH ST.	S.E. QUADRANT	2.00	1.50	0.00	1.50
13	SYCAMORE ST.	N.W. QUADRANT	1.00	0.75	0.00	0.75
14	SYCAMORE ST.	N.E. QUADRANT	1.00	0.75	0.00	0.75
15	ASH ST	N.W. QUADRANT	1.00	0.75	0.00	0.75
16	ASH ST	N.E. QUADRANT	1.00	0.75	0.00	0.75
17	HICKORY ST.	N.W. QUADRANT	1.00	0.75	0.00	0.75
18	HICKORY ST.	N.E. QUADRANT	1.00	0.75	0.00	0.75
19	COTTAGE GROVE	N.W. QUADRANT	1.00	0.75	0.00	0.75
20	COTTAGE GROVE	N.E. QUADRANT	1.00	0.75	0.00	0.75
21	DALE LANE	N.W. QUADRANT	1.00	0.75	0.00	0.75
22	DALE LANE	N.E. QUADRANT	1.00	0.75	0.00	0.75
23	SMITH ROAD	N.W. QUADRANT	1.00	0.75	0.00	0.75
24	SMITH ROAD	N.E. QUADRANT	1.00	0.75	0.00	0.75
25	DODSON DRIVE	N.W. QUADRANT	1.00	0.75	0.00	0.75
26	DODSON DRIVE	N.E. QUADRANT	1.00	0.75	0.00	0.75
27	ABBAY ROAD	N.W. QUADRANT	1.00	0.75	0.00	0.75
28	ABBAY ROAD	N.E. QUADRANT	1.00	0.75	0.00	0.75
29	BERINGER CIRCLE	N.W. QUADRANT	6.00	4.50	0.00	4.50
30	BERINGER CIRCLE	N.E. QUADRANT	6.00	4.50	33.75	-29.25
TOTAL =			45.00	33.75	33.75	0.00
			CUYD	CU YD	CU YD	CU YD

NOTES:

- 1) ALL EARTHWORK REQUIRED TO MATCH THE EXISTING GROUND TO THE PROPOSED SIDEWALK ELEVATIONS WILL BE PAID FOR AS EARTH EXCAVATION. AT ALL LOCATIONS EXCEPT #29 & #30, MINIMAL EARTHWORK WILL BE REQUIRED TO ESTABLISH PROPOSED GRADES. AT LOCATIONS #29 & #30 APPROXIMATELY 6 INCH DEPTHS OF EARTH EXCAVATION WILL BE REQUIRED TO CONSTRUCT THE PROPOSED SIDEWALK TO EXISTING EARTH GRADES.
- 2) ANY EXCESS EARTH EXCAVATION REMAINING FROM LOCATIONS #1 THROUGH #29 SHALL BE USED AT THE NORTHEAST QUADRANT OF BERINGER CIRCLE. THE MATERIAL CAN BE DEPOSITED ADJACENT TO THE PROPOSED SIDEWALK TO PROVIDE A SMOOTHER TRANSITION OVER THE PIPE CULVERT. ALL EXCESS EARTH MUST BE WASTED WITHIN THE PROJECT LIMITS. TRANSPORT OF EARTH MATERIAL OFFSITE WILL NOT BE ALLOWED.
- 3) THE SHRINKAGE FACTOR USED IS 25%
SHRINKAGE, EMBANKMENT, AND BALANCE DATA IS FOR INFORMATION ONLY.

SCHEDULE OF QUANTITIES

SHEET 3 OF 3

SODDING SCHEDULE

LOCATION	AREA (SQ FT)	25200110	25000400	25000500	25000600	28000250	25200200
		SODDING, SALT TOLERANT (SQ YD)	NITROGEN FERTILIZER NUTRIENT (POUND)	PHOSPHORUS FERTILIZER NUTRIENT (POUND)	POTASSIUM FERTILIZER NUTRIENT (POUND)	TEMPORARY EROSION CONTROL SEEDING (POUND)	SUPPLEMENTAL WATERING 3 GAL / SQ YD 4 APPLICATIONS (UNIT)
1 WATER ST.	N.W. QUADRANT	0.0	0.00	0.000	0.000	0.000	0.000
2 WATER ST.	N.E. QUADRANT	120.0	13.33	0.165	0.165	0.165	0.275
3 LOCUST ST.	N.W. QUADRANT	56.0	6.22	0.077	0.077	0.077	0.129
4 LOCUST ST.	N.E. QUADRANT	78.0	8.67	0.107	0.107	0.107	0.179
5 LOCUST ST.	S.W. QUADRANT	94.0	10.44	0.129	0.129	0.129	0.216
6 LOCUST ST.	S.E. QUADRANT	180.0	20.00	0.248	0.248	0.248	0.413
7 FOURTH ST.	S.W. QUADRANT	126.0	14.00	0.174	0.174	0.174	0.289
8 FOURTH ST.	S.E. QUADRANT	184.0	20.44	0.253	0.253	0.253	0.422
9 FIFTH ST.	N.W. QUADRANT	100.0	11.11	0.138	0.138	0.138	0.230
10 FIFTH ST.	N.E. QUADRANT	108.0	12.00	0.149	0.149	0.149	0.248
11 FIFTH ST.	S.W. QUADRANT	70.0	7.78	0.096	0.096	0.096	0.161
12 FIFTH ST.	S.E. QUADRANT	114.0	12.67	0.157	0.157	0.157	0.262
13 SYCAMORE ST.	N.W. QUADRANT	94.0	10.44	0.129	0.129	0.129	0.216
14 SYCAMORE ST.	N.E. QUADRANT	278.0	30.89	0.383	0.383	0.383	0.638
15 ASH ST	N.W. QUADRANT	98.0	10.89	0.135	0.135	0.135	0.225
16 ASH ST	N.E. QUADRANT	94.0	10.44	0.129	0.129	0.129	0.216
17 HICKORY ST.	N.W. QUADRANT	108.0	12.00	0.149	0.149	0.149	0.248
18 HICKORY ST.	N.E. QUADRANT	132.0	14.67	0.182	0.182	0.182	0.303
19 COTTAGE GROVE	N.W. QUADRANT	132.0	14.67	0.182	0.182	0.182	0.303
20 COTTAGE GROVE	N.E. QUADRANT	42.0	4.67	0.058	0.058	0.058	0.096
21 DALE LANE	N.W. QUADRANT	58.0	6.44	0.080	0.080	0.080	0.133
22 DALE LANE	N.E. QUADRANT	86.0	9.56	0.118	0.118	0.118	0.197
23 SMITH ROAD	N.W. QUADRANT	86.0	9.56	0.118	0.118	0.118	0.197
24 SMITH ROAD	N.E. QUADRANT	90.0	10.00	0.124	0.124	0.124	0.207
25 DODSON DRIVE	N.W. QUADRANT	136.0	15.11	0.187	0.187	0.187	0.312
26 DODSON DRIVE	N.E. QUADRANT	80.0	8.89	0.110	0.110	0.110	0.184
27 ABBEY ROAD	N.W. QUADRANT	48.0	5.33	0.066	0.066	0.066	0.110
28 ABBEY ROAD	N.E. QUADRANT	62.0	6.89	0.085	0.085	0.085	0.142
29 BERINGER CIRCLE	N.W. QUADRANT	266.0	29.56	0.366	0.366	0.366	0.611
30 BERINGER CIRCLE	N.E. QUADRANT	245.0	27.22	0.337	0.337	0.337	0.562
TOTAL =		3365.0	373.89	4.635	4.635	4.635	7.725
USE =		3375.0	375.00	5.000	5.000	5.000	8.000
		SQ FT	SQ YD	POUND	POUND	POUND	POUND

NOTES:

- 1) THE SODDING LIMITS CALCULATED EXTEND 2 FT. AROUND THE OUTER PERIMETER OF THE PROPOSED SIDEWALK WHERE THERE IS EXISTING GRASS. SOD QUANTITIES THAT MAY NOT BE REQUIRED AT ALL LOCATIONS WILL BE USED TO SOD EXCESS EARTH EXCAVATION THAT WILL BE PLACED IN THE NORTHEAST QUADRANT OF BERINGER CIRCLE.

BENCHMARKS

- B.M. 4679-3 STA. 42+00.05, (41.88' RT.)
 CHISELED SQUARE
 TO REACH FROM THE INTERSECTION OF NEIL STREET AND SPRINGFIELD AVENUE, GO EAST ON SPRINGFIELD AVENUE FOR ±100' TO MARK ON RIGHT. SAID MARK IS A CHISELED SQUARE LOCATED ON THE NORTH END OF CONCRETE FOUNDATION ON A CANTILEVER SIGN SPANNING OVER SPRINGFIELD AVENUE. ELEVATION = 735.772
- B.M. 4679-2 STA. 45+90.38, (33.22' LT.)
 CHISELED SQUARE
 TO REACH FROM THE INTERSECTION OF NEIL STREET AND SPRINGFIELD AVENUE, GO EAST ON SPRINGFIELD AVENUE FOR ±500' TO MARK ON LEFT. SAID MARK IS A CHISELED SQUARE LOCATED ON EAST END OF THE NORHTEAST WINGWALL OF RAILROAD STRUCTURE. THE MARK IS ON THE SOUTHEAST CORNER OF THE EASTERLY MOST STEP OF WING. ELEVATION = 729.227
- B.M. 4679-1 STA. 52+25.10, (40.67' RT.)
 CHISELED SQUARE
 TO REACH FROM THE INTERSECTION OF FIRST STREET AND SPRINGFIELD AVENUE, GO WEST ON SPRINGFIELD AVENUE FOR ±200' TO MARK ON LEFT. SAID MARK IS A CHISELED SQUARE LOCATED ON THE SOUTHWEST CORNER OF A SIGN FOUNDATION. THE SIGN IS LOCATED IN THE SOUTHEAST CORNER OF A HOUSE LOT. ELEVATION = 4679-1
- B.M. 4985-5 STA. 62+57.74, (29.49' RT.)
 DISK
 IN THE SOUTHWEST QUADRANT OF THE INTERSECTION OF SPRINGFIELD AVENUE AND THIRD STREET. SAID MARK IS A DISK IN CONCRETE IN THE BRICK SIDEWALK. ELEVATION = 724.492
- B.M. 4761-1 STA. 174+50.23, (48.30' LT.)
 CHISELED SQUARE
 TO REACH FROM THE INTERSECTION OF US 150 & CUNNINGHAM AVENUE, GO EAST ALONG US 150 FOR 0.25 MILE TO SYCAMORE STREET AND THE MARK ON THE LEFT. SAID MARK IS A CHISELED SQUARE ON A CONCRETE RETAINING WALL IN THE NORTHEAST QUADRANT OF THE INTERSECTION. ELEVATION = 711.944
- B.M. 4761-2 STA. 17+05.88, (38.60' LT.)
 CHISELED SQUARE
 TO REACH FROM THE INTERSECTION OF US 150 & CUNNINGHAM AVENUE, GO EAST ALONG US 150 FOR 0.35 MILE TO ASH STREET AND THE MARK ON THE LEFT. SAID MARK IS A CHISELED SQUARE ON THE EAST END OF A CONCRETE RETAINING WALL IN THE NORTHEAST QUADRANT OF THE INTERSECTION. ELEVATION = 714.369
- B.M. 4761-3 STA. 24+04.60, (56.20' RT.)
 CHISELED SQUARE
 TO REACH FROM THE INTERSECTION OF US 150 & CUNNINGHAM AVENUE, GO EAST ALONG US 150 FOR 0.5 MILE TO THE MARK ON THE RIGHT. SAID MARK IS A CHISELED SQUARE ON THE NORTH SIDE OF A CONCRETE LIGHT POLE FOUNDATION. ELEVATION = 719.162

- B.M. 4761-4 STA. 35+37.97, (57.20' RT.)
 CHISELED SQUARE
 TO REACH FROM THE INTERSECTION OF US 150 & CUNNINGHAM AVENUE, GO EAST ALONG US 150 FOR 0.75 MILE TO THE MARK ON THE RIGHT. SAID MARK IS A CHISELED SQUARE ON THE SOUTHEAST CORNER OF A CONCRETE HEADWALL EAST OF WAYNE CAIN & SONS ENTRANCE. ELEVATION = 715.097
- B.M. 4761-5 STA. 43+50.00, (53.4' RT.)
 BOLT
 TO REACH FROM THE INTERSECTION OF US 150 & CUNNINGHAM AVENUE, GO EAST ALONG US 150 FOR 0.9 MILE TO THE MARK ON THE RIGHT. SAID MARK IS A 3/4" BOLT ON TOP OF A FIRE HYDRANT NEAR THE WEST ENTRANCE OF FS FAST STOP. ELEVATION = 718.887
- B.M. 4761-6 STA. 12+16.50, (6.30' LT.)
 CHISELED SQUARE
 TO REACH FROM THE INTERSECTION OF US 150 & CUNNINGHAM AVENUE, GO EAST ALONG US 150 FOR 1.15 MILE TO THE MARK ON THE LEFT. SAID MARK IS A CHISELED SQUARE ON THE WEST END OF THE CONCRETE MEDIAN WHERE THE CONCRETE ENDS AND THE GRASS BEGINS. ELEVATION = 728.690
- B.M. 4761-7 STA. 17+98.97, (41.90' RT.)
 CHISELED SQUARE
 TO REACH FROM THE INTERSECTION OF US 150 (EAST) & THE URBANA SPUR, GO EAST ALONG THE SPUR FOR 0.05 MILE TO THE MARK ON THE RIGHT. SAID MARK IS A CHISELED SQUARE IN THE CENTER OF THE SOUTHWEST HEADWALL OF AN ACROSS ROAD BOX CULVERT. ELEVATION = 734.226
- B.M. 4985-3 STA. 464+98.10, (64.77' LT.)
 CHISELED SQUARE
 IN THE NORTHWEST QUADRANT OF THE INTERSECTION OF US 150 & SMITH ROAD. SAID MARK IS A CHISELED SQUARE ON TOP OF THE EAST END OF CONCRETE RETAINING WALL. ELEVATION = 742.044
- B.M. 4985-4 STA. 495+31.40, (54.24' LT.)
 CHISELED SQUARE
 TO REACH FROM THE INTERSECTION OF US 150 & HIGHCROSS ROAD, GO WESTERLY ALONG US 150 0.27 MILE TO MARK ON THE RIGHT. SAID MARK IS A CHISELED SQUARE IN THE CONCRETE MEDIAN OF BERINGER CIRCLE. ELEVATION = 715.026

FILE NAME =	USER NAME = eaglino	DESIGNED -	REVISED -	STATE OF ILLINOIS	BENCHMARKS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBID\INTEG\Illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\0579\DRAWING\Design\0579A47-sh1-Benchmark				DEPARTMENT OF TRANSPORTATION		808	D5 ADA-2	CHAMPAIGN	44	10
PLOT SCALE = 40.0000' / in.						CONTRACT NO. 70A47				
PLOT DATE = 3/17/2016						ILLINOIS FED. AID PROJECT				
DATE -						SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.				

NOTES:

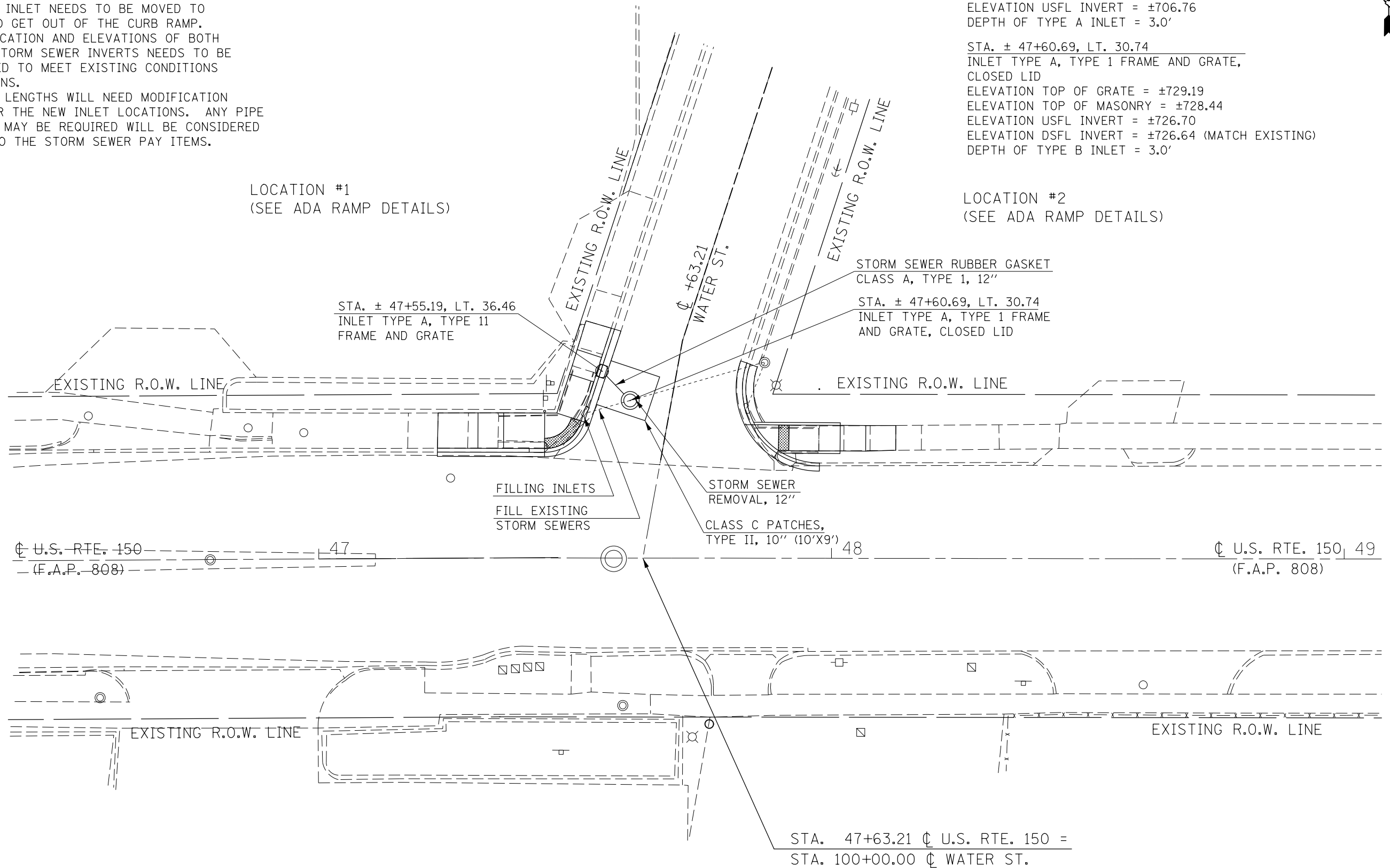
- 1) TO MEET ADA COMPLIANCE ON SIDEWALK WIDTHS, THE EXISTING INLET NEEDS TO BE MOVED TO THE NORTH TO GET OUT OF THE CURB RAMP.
- 2) THE FINAL LOCATION AND ELEVATIONS OF BOTH INLETS AND STORM SEWER INVERTS NEEDS TO BE FIELD VERIFIED TO MEET EXISTING CONDITIONS AND ELEVATIONS.
- 3) STORM SEWER LENGTHS WILL NEED MODIFICATION TO ALLOW FOR THE NEW INLET LOCATIONS. ANY PIPE ELBOWS THAT MAY BE REQUIRED WILL BE CONSIDERED INCIDENTAL TO THE STORM SEWER PAY ITEMS.

STA. ± 47+55.19, LT. 36.46
 INLET TYPE A, TYPE 11 FRAME AND GRATE
 ELEVATION TOP OF GRATE = ±729.20
 ELEVATION TOP OF MASONRY = ±728.36
 ELEVATION USFL = ±706.76
 DEPTH OF TYPE A INLET = 3.0'

STA. ± 47+60.69, LT. 30.74
 INLET TYPE A, TYPE 1 FRAME AND GRATE, CLOSED LID
 ELEVATION TOP OF GRATE = ±729.19
 ELEVATION TOP OF MASONRY = ±728.44
 ELEVATION USFL INVERT = ±726.70
 ELEVATION DSFL INVERT = ±726.64 (MATCH EXISTING)
 DEPTH OF TYPE B INLET = 3.0'

LOCATION #1
 (SEE ADA RAMP DETAILS)

LOCATION #2
 (SEE ADA RAMP DETAILS)



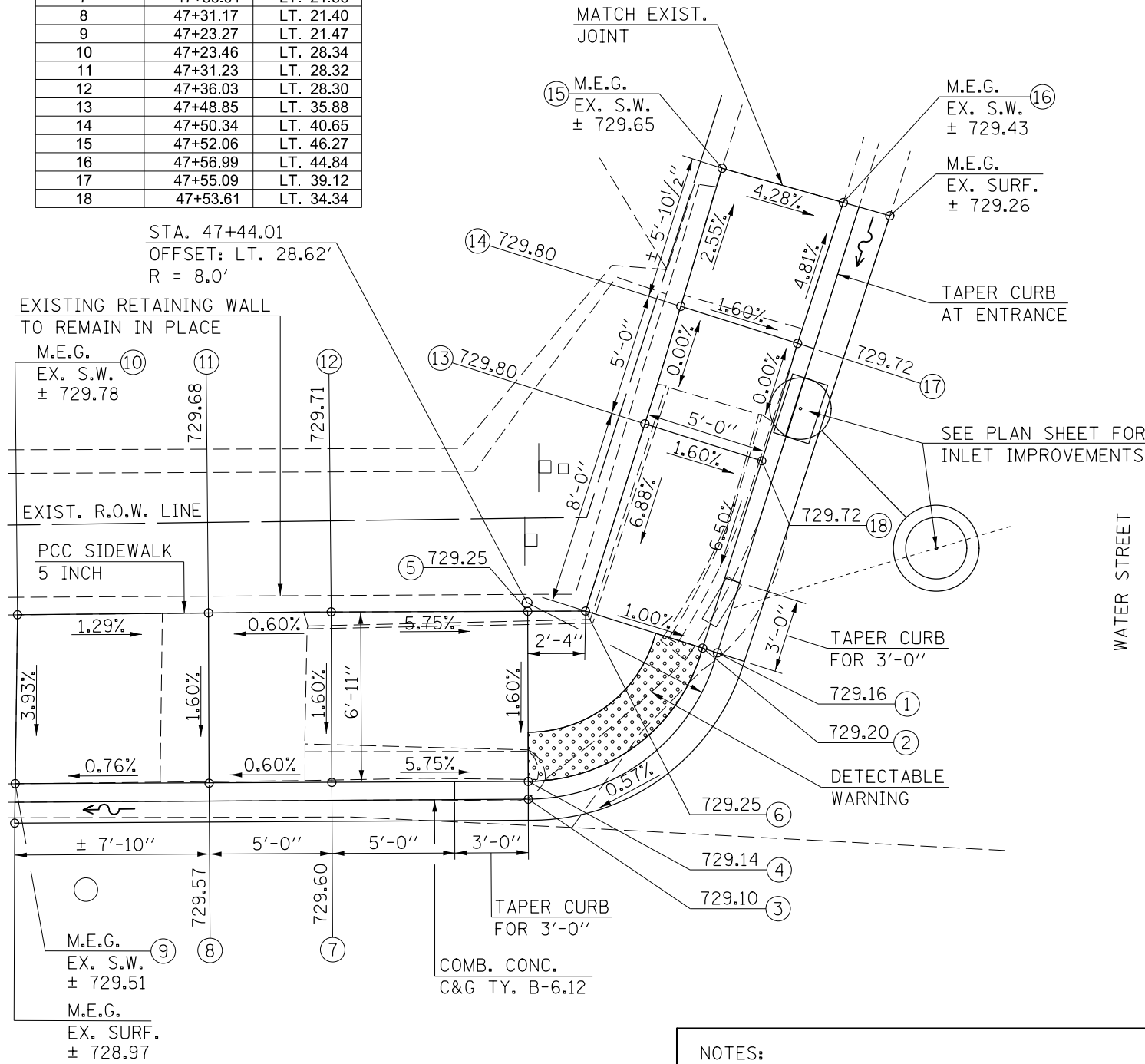
FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. 150 AND WATER STREET INTERSECTION PLAN SHEET	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 70A47							
\$MODELNAME\$	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
				SCALE: 1" = 10'		SHEET 1 OF 3 SHEETS		STA. TO STA.		

LOCATION #1 - N.W. QUADRANT
U.S. RTE. 150 & WATER ST.

LOCATION #1

N.W. QUADRANT
U.S. 150 & WATER ST.

LOCATION	STATION	OFFSET
1	47+51.75	LT. 26.54
2	47+51.15	LT. 26.73
3	47+44.01	LT. 20.62
4	47+44.01	LT. 21.35
5	47+44.01	LT. 28.28
6	47+46.39	LT. 28.28
7	47+36.01	LT. 21.36
8	47+31.17	LT. 21.40
9	47+23.27	LT. 21.47
10	47+23.46	LT. 28.34
11	47+31.23	LT. 28.32
12	47+36.03	LT. 28.30
13	47+48.85	LT. 35.88
14	47+50.34	LT. 40.65
15	47+52.06	LT. 46.27
16	47+56.99	LT. 44.84
17	47+55.09	LT. 39.12
18	47+53.61	LT. 34.34

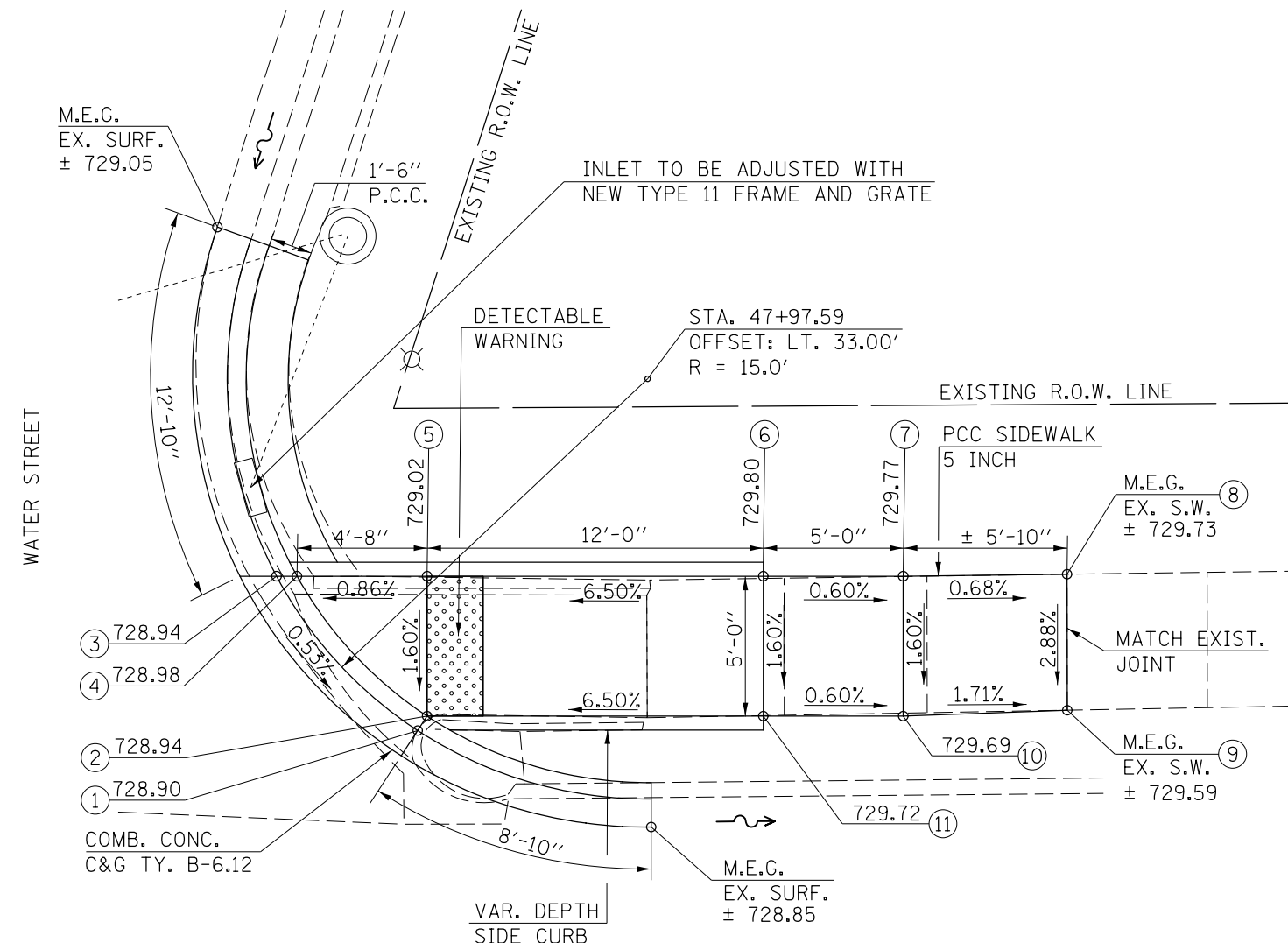


LOCATION #2 - N.E. QUADRANT
U.S. RTE. 150 & WATER ST.

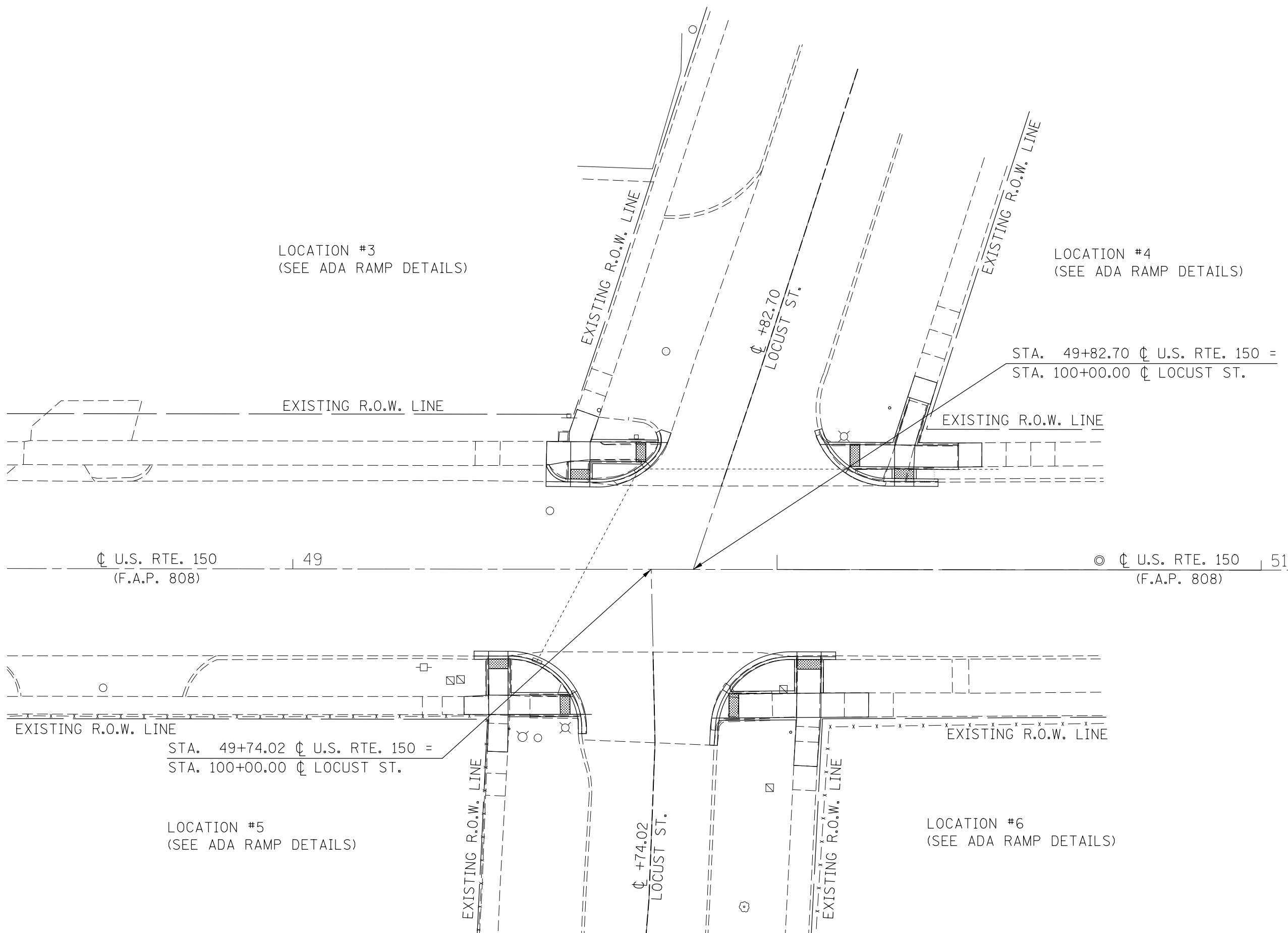
LOCATION #2

N.E. QUADRANT
U.S. 150 & WATER ST.

LOCATION	STATION	OFFSET
1	47+89.31	LT. 20.50
2	47+89.64	LT. 21.01
3	47+84.30	LT. 26.04
4	47+85.03	LT. 26.04
5	47+89.68	LT. 26.01
6	48+01.68	LT. 25.93
7	48+06.67	LT. 25.90
8	48+12.52	LT. 25.94
9	48+12.50	LT. 21.08
10	48+06.64	LT. 20.90
11	48+01.64	LT. 20.93



- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE < 7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.



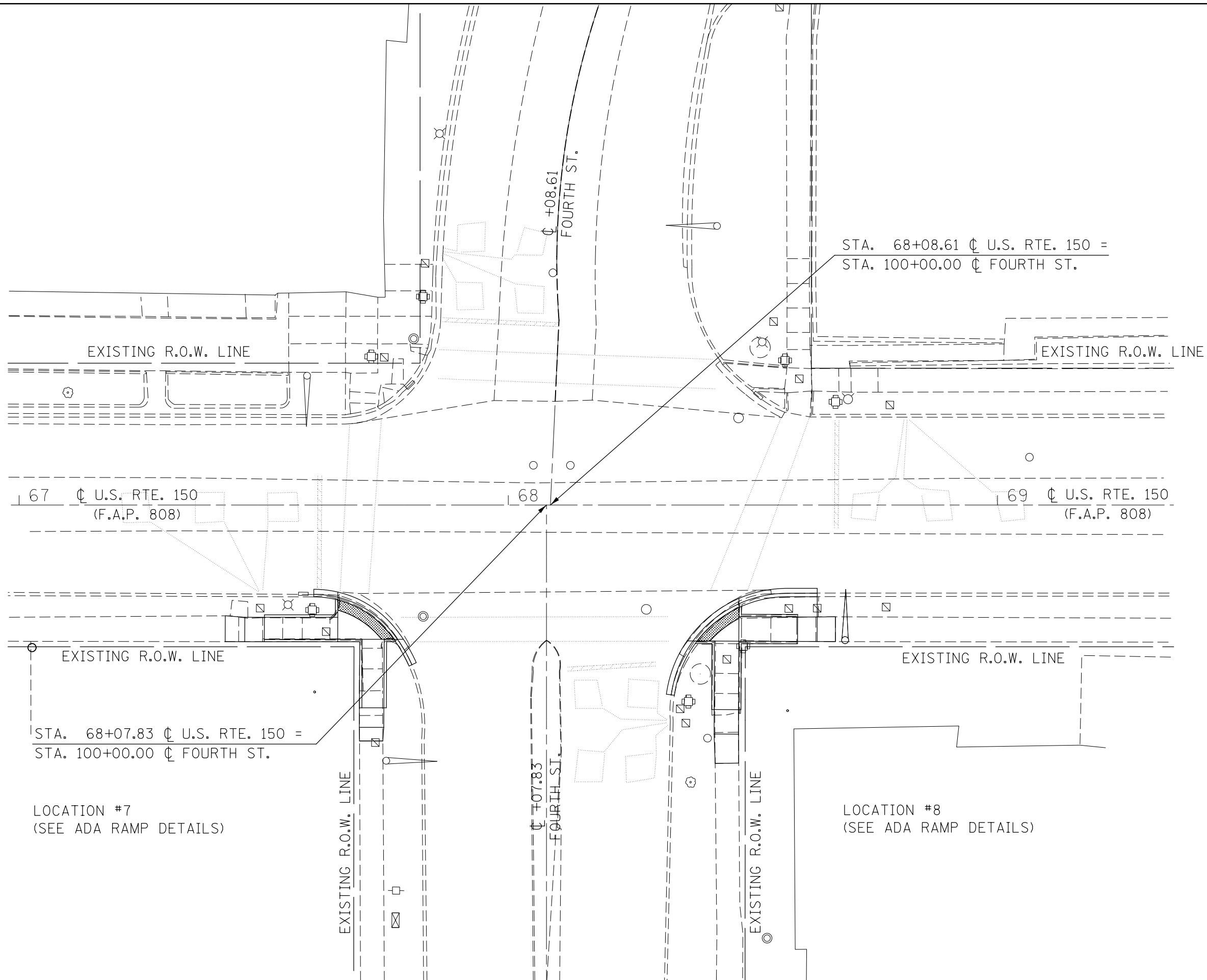
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	PLOT DATE = 3/17/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**U.S. 150 AND LOCUST STREET
INTERSECTION PLAN SHEET**

SCALE: 1" = 10' SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 ADA-2	CHAMPAIGN	44	13
CONTRACT NO. 70A47			ILLINOIS FED. AID PROJECT	



LOCATION #7
(SEE ADA RAMP DETAILS)

LOCATION #8
(SEE ADA RAMP DETAILS)

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. 150 AND FOURTH STREET INTERSECTION PLAN SHEET	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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MODELNAME	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 70A47			ILLINOIS FED. AID PROJECT	
	PLOT DATE = 3/17/2016	DATE -	REVISED -			SCALE: 1" = 10'	SHEET 1	OF 3 SHEETS	STA.	TO STA.

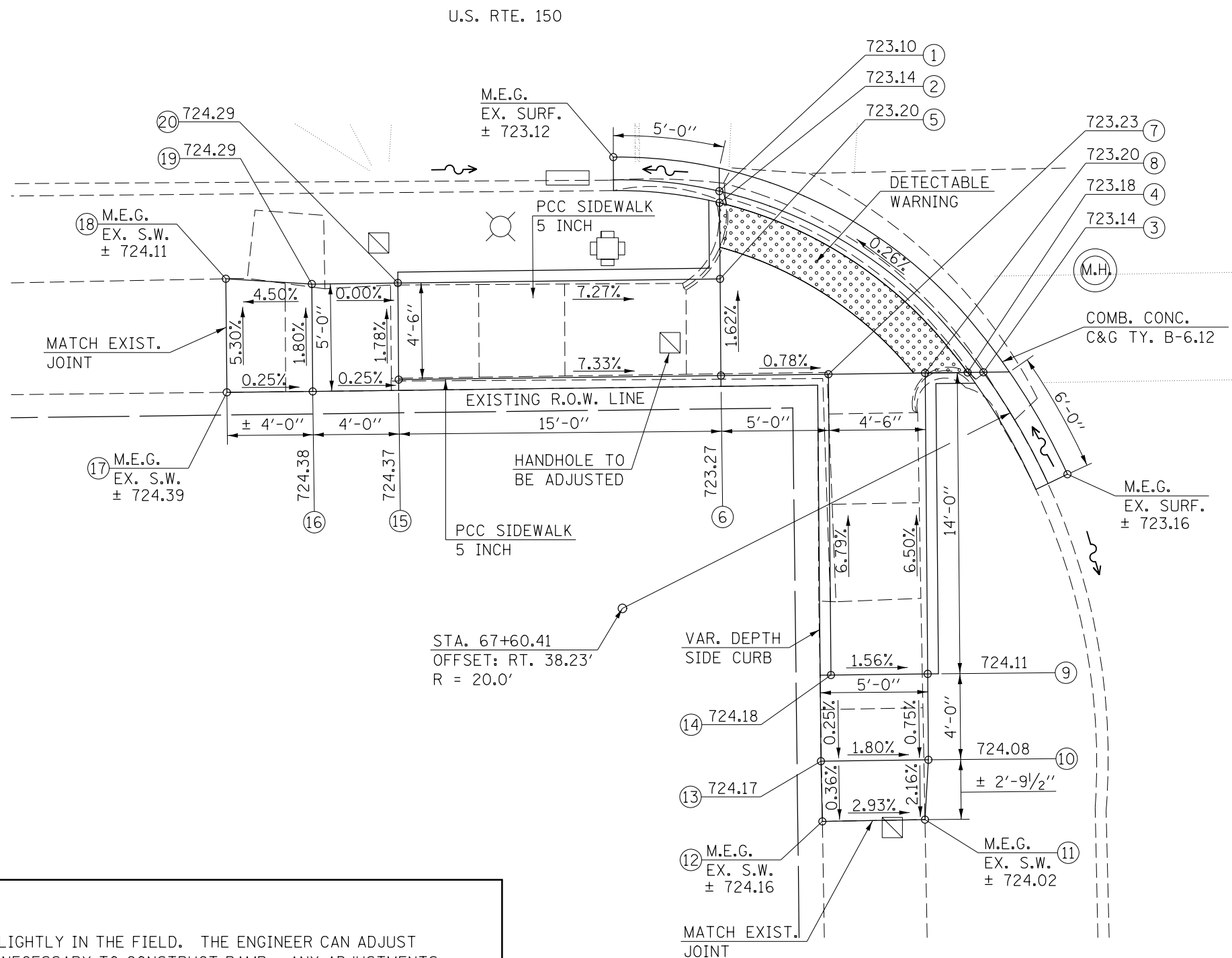


LOCATION #7

S.W. QUADRANT
U.S. 150 & FOURTH ST.

LOCATION #7 - S.W. QUADRANT
U.S. RTE. 150 & FOURTH ST.

LOCATION	STATION	OFFSET
1	67+65.17	RT. 18.87
2	67+65.17	RT. 19.41
3	67+77.35	RT. 27.45
4	67+76.61	RT. 27.45
5	67+65.15	RT. 22.95
6	67+65.14	RT. 27.45
7	67+70.14	RT. 27.45
8	67+74.64	RT. 27.45
9	67+74.58	RT. 41.45
10	67+74.56	RT. 45.45
11	67+74.37	RT. 48.23
12	67+69.59	RT. 48.25
13	67+69.56	RT. 45.45
14	67+70.08	RT. 41.45
15	67+50.14	RT. 27.45
16	67+46.13	RT. 27.95
17	67+42.13	RT. 27.94
18	67+42.15	RT. 22.66
19	67+46.15	RT. 22.95
20	67+50.15	RT. 22.95



FOURTH STREET

NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE < 7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.

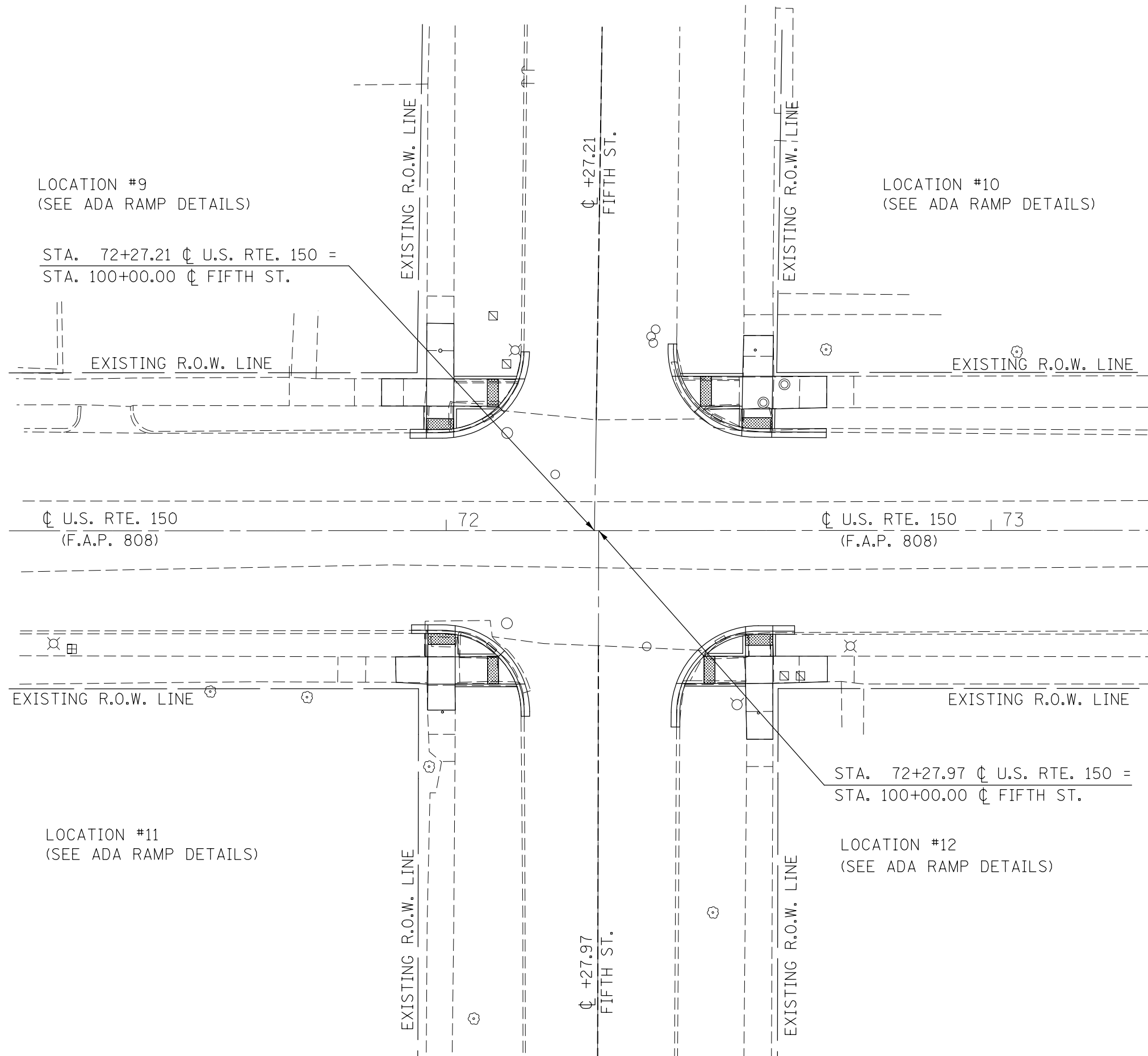
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MODELNAME	PLOT DATE = 3/17/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

U.S. 150 & FOURTH STREET
LOCATION 7 RAMP DETAIL

SCALE: 1" = 3' SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 ADA-2	CHAMPAIGN	44	17
			CONTRACT NO. 70A47	
ILLINOIS FED. AID PROJECT				



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		CHECKED -	REVISED -
		DATE -	REVISED -
\$MODELNAME\$	PLOT DATE = 3/17/2016		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**U.S. 150 AND FIFTH STREET
INTERSECTION PLAN SHEET**

SCALE: 1" = 10' SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 ADA-2	CHAMPAIGN	44	19
CONTRACT NO. 70A47			ILLINOIS FED. AID PROJECT	

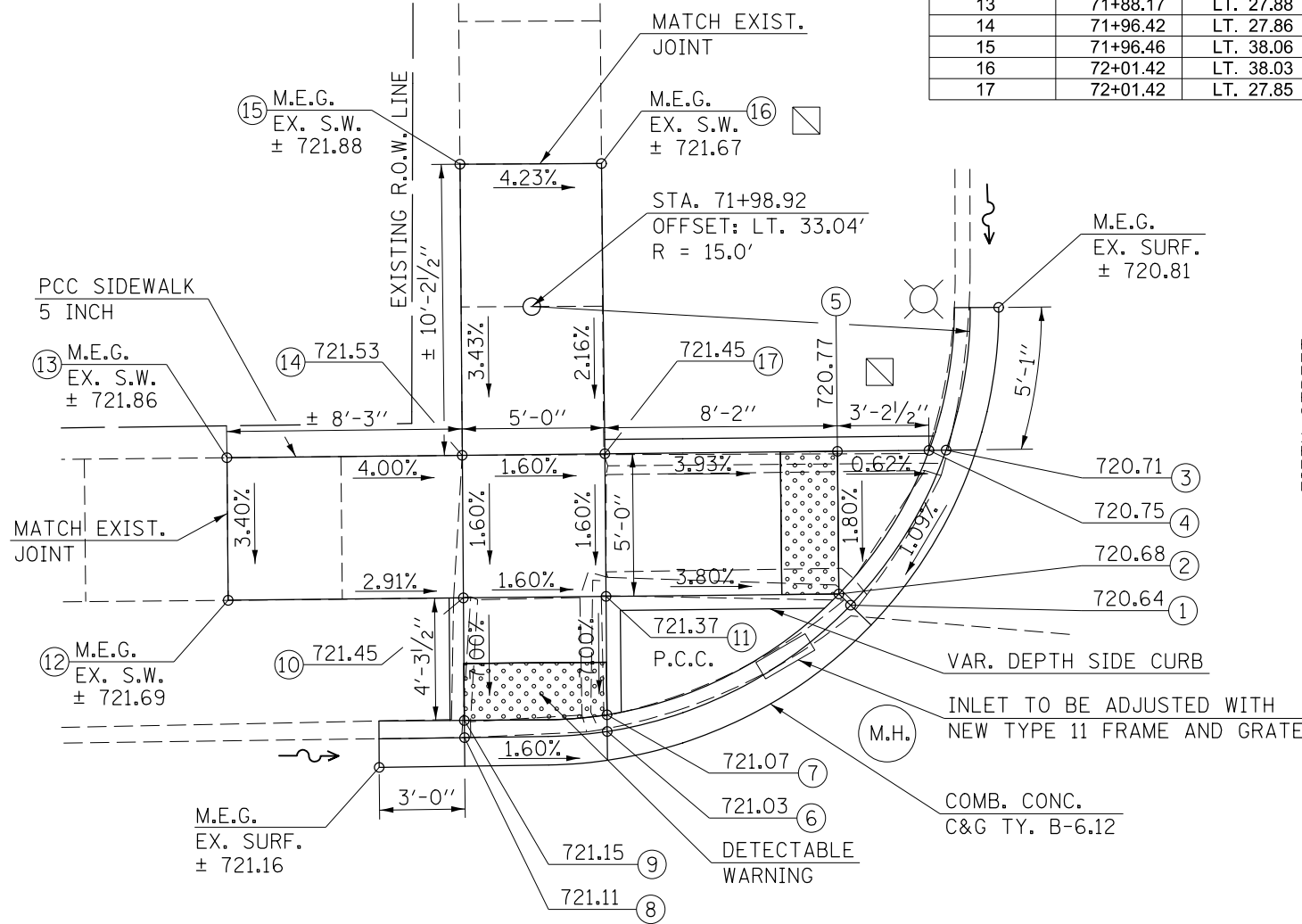


LOCATION #9

N.W. QUADRANT U.S. 150 & FIFTH ST.

LOCATION #9 - N.W. QUADRANT
U.S. RTE. 150 & FIFTH ST.

LOCATION	STATION	OFFSET
1	72+09.96	LT. 22.44
2	72+09.56	LT. 22.84
3	72+13.38	LT. 27.83
4	72+12.78	LT. 27.83
5	72+09.57	LT. 27.84
6	72+01.37	LT. 18.12
7	72+01.38	LT. 18.71
8	71+96.37	LT. 17.97
9	71+96.38	LT. 18.57
10	71+96.40	LT. 22.86
11	72+01.40	LT. 22.85
12	71+88.15	LT. 22.88
13	71+88.17	LT. 27.88
14	71+96.42	LT. 27.86
15	71+96.46	LT. 38.06
16	72+01.42	LT. 38.03
17	72+01.42	LT. 27.85



U.S. RTE. 150

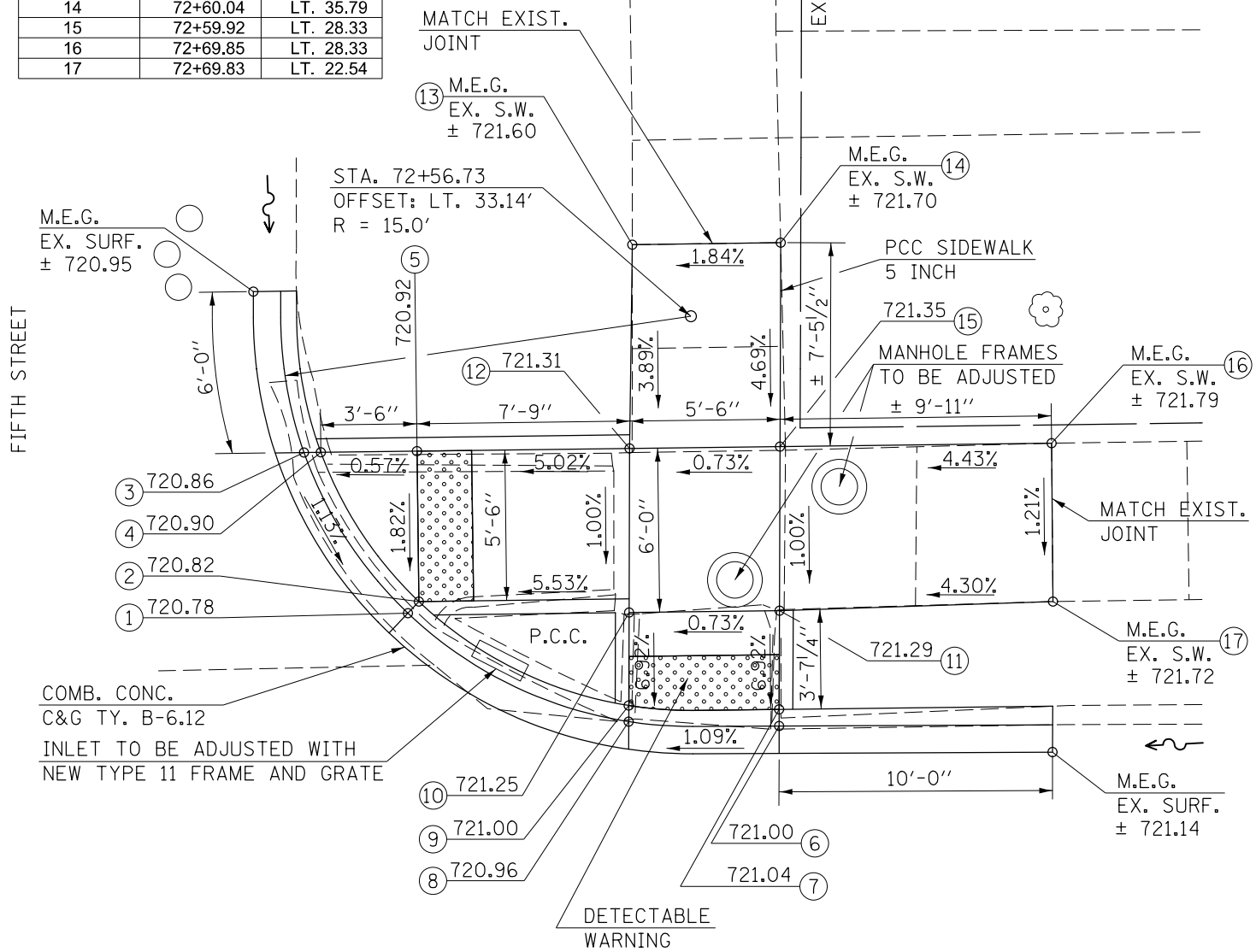
FIFTH STREET

LOCATION #10

N.E. QUADRANT U.S. 150 & FIFTH ST.

LOCATION #10 - N.E. QUADRANT
U.S. RTE. 150 & FIFTH ST.

LOCATION	STATION	OFFSET
1	72+46.24	LT. 22.41
2	72+46.65	LT. 22.83
3	72+42.52	LT. 28.33
4	72+43.14	LT. 28.33
5	72+46.65	LT. 28.33
6	72+59.76	LT. 18.11
7	72+59.77	LT. 18.72
8	72+54.27	LT. 18.34
9	72+54.27	LT. 18.93
10	72+54.33	LT. 22.33
11	72+59.83	LT. 22.33
12	72+54.42	LT. 28.33
13	72+54.62	LT. 35.79
14	72+60.04	LT. 35.79
15	72+59.92	LT. 28.33
16	72+69.85	LT. 28.33
17	72+69.83	LT. 22.54



U.S. RTE. 150



- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE < 7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.



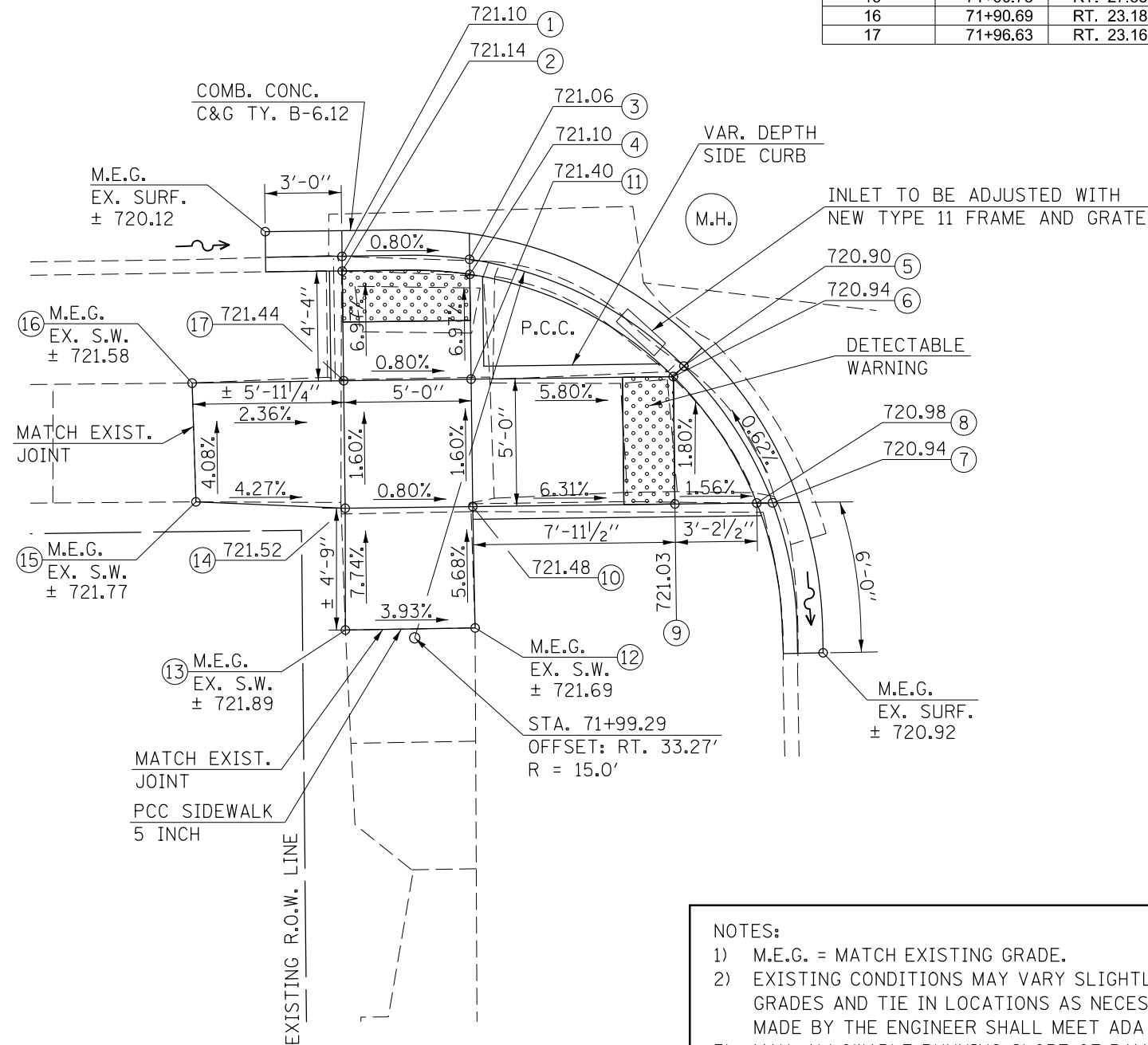
LOCATION #11

S.W. QUADRANT U.S. 150 & FIFTH ST.

LOCATION #11 - S.W. QUADRANT
U.S. RTE. 150 & FIFTH ST.

LOCATION	STATION	OFFSET
1	71+96.63	RT. 18.28
2	71+96.63	RT. 18.85
3	72+01.63	RT. 18.46
4	72+01.63	RT. 19.05
5	72+09.97	RT. 22.75
6	72+09.56	RT. 23.16
7	72+13.39	RT. 28.16
8	72+12.77	RT. 28.16
9	72+09.56	RT. 28.16
10	72+01.63	RT. 28.16
11	72+01.63	RT. 23.16
12	72+01.66	RT. 32.91
13	71+96.57	RT. 32.93
14	71+96.63	RT. 28.16
15	71+90.78	RT. 27.83
16	71+90.69	RT. 23.18
17	71+96.63	RT. 23.16

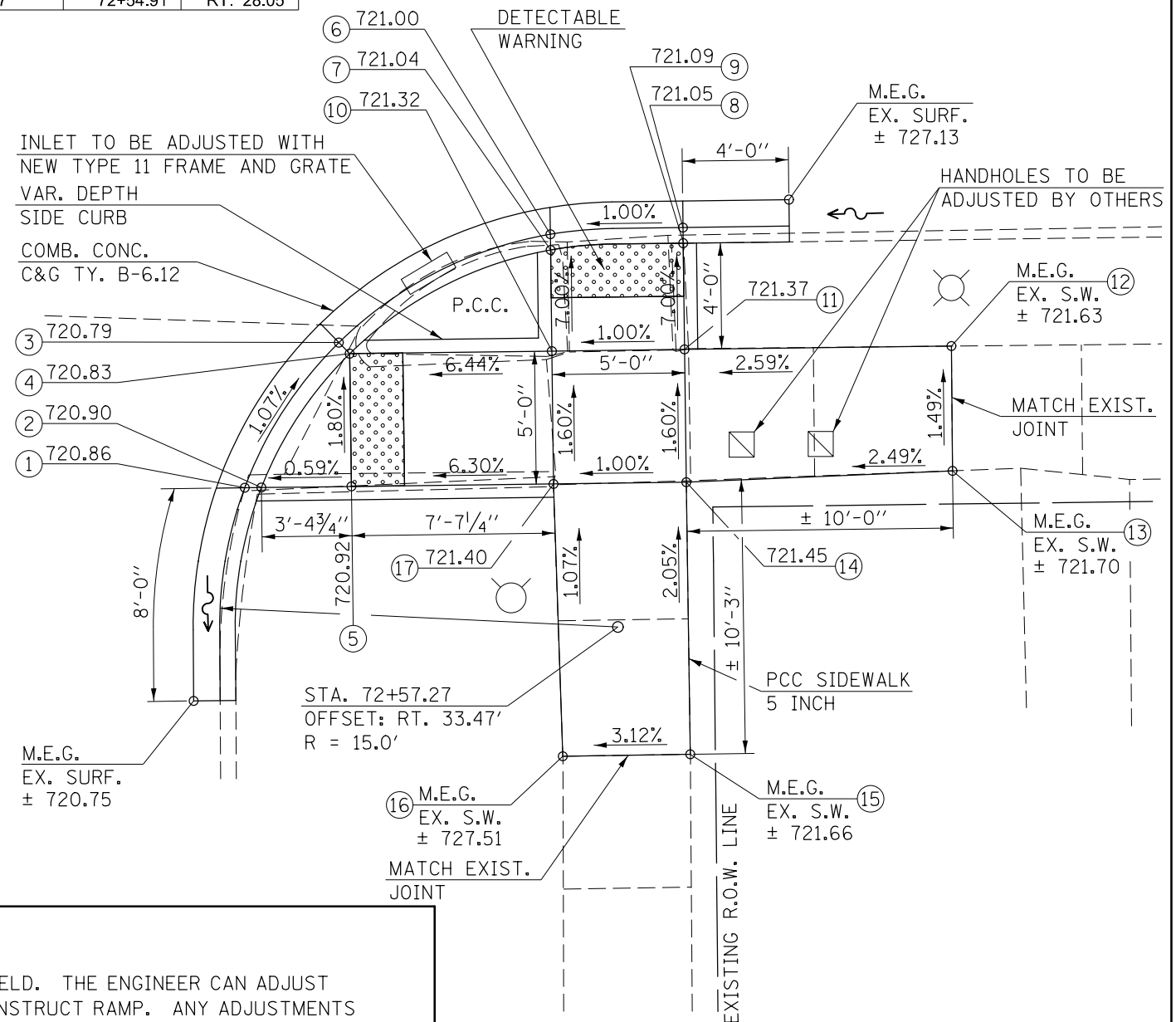
U.S. RTE. 150



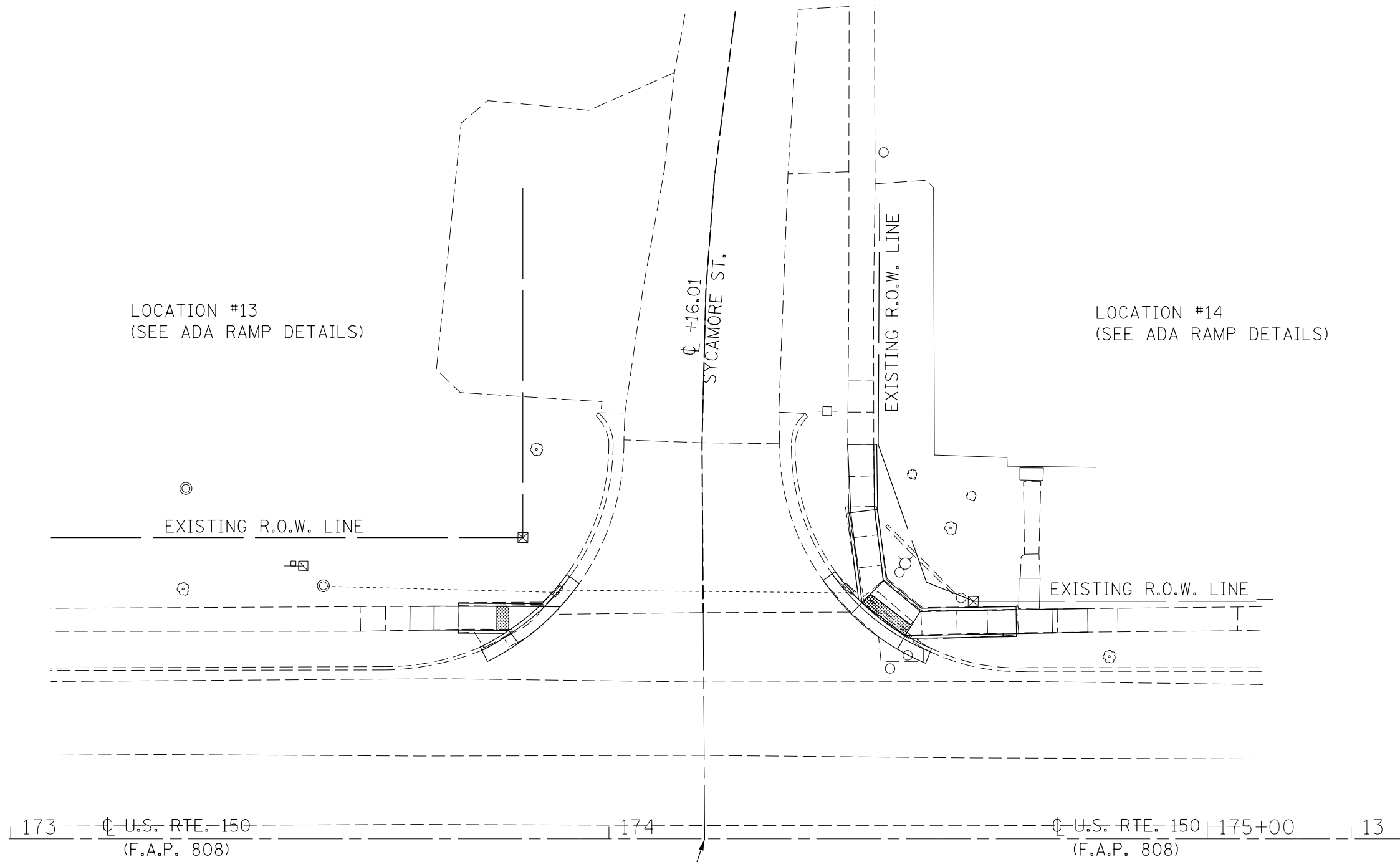
LOCATION #12 - S.E. QUADRANT
U.S. RTE. 150 & FIFTH ST.

LOCATION	STATION	OFFSET
1	72+43.28	RT. 28.05
2	72+43.91	RT. 28.05
3	72+46.90	RT. 22.63
4	72+47.30	RT. 23.05
5	72+47.30	RT. 28.05
6	72+54.91	RT. 18.65
7	72+54.91	RT. 19.24
8	72+59.91	RT. 18.46
9	72+59.91	RT. 19.05
10	72+54.91	RT. 23.05
11	72+59.91	RT. 23.05
12	72+69.96	RT. 23.05
13	72+69.95	RT. 27.75
14	72+59.91	RT. 28.05
15	72+59.93	RT. 38.30
16	72+55.13	RT. 38.31
17	72+54.91	RT. 28.05

U.S. RTE. 150



- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE < 7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.



STA. 174+16.01 CL U.S. RTE. 150 =
 STA. 100+00.00 CL SYCAMORE ST.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. 150 AND SYCAMORE STREET INTERSECTION PLAN SHEET			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 5\Projects\0579\DRAWING\Design\0579A47-sh-ADA.Ramp	DESIGNED -	REVISED -	808					D5 ADA-2	CHAMPAIGN	44	22	
MODELNAME	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 70A47			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 3/17/2016	DATE -	REVISED -		SCALE: 1" = 10'	SHEET 1 OF 3 SHEETS	STA.			TO STA.		

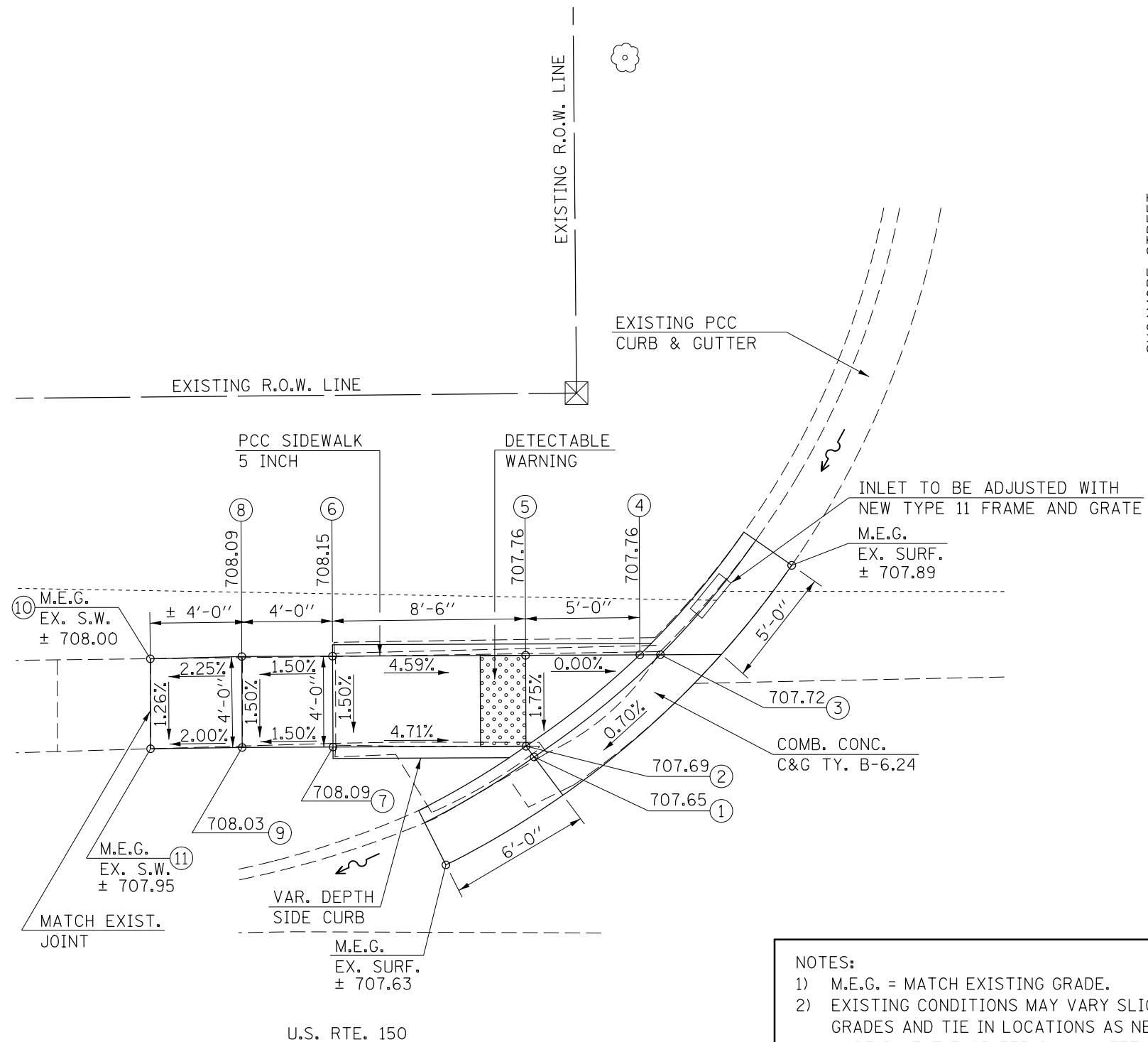


LOCATION #13

N.W. QUADRANT
U.S. 150 & SYCAMORE ST.

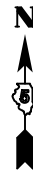
LOCATION #13 - N.W. QUADRANT
U.S. RTE. 150 & SYCAMORE ST.

LOCATION	STATION	OFFSET
1	173+83.60	LT. 34.36
2	173+83.25	LT. 34.83
3	173+89.20	LT. 38.81
4	173+88.30	LT. 38.81
5	173+83.27	LT. 38.83
6	173+74.77	LT. 38.87
7	173+74.75	LT. 34.87
8	173+70.77	LT. 38.89
9	173+70.75	LT. 34.89
10	173+66.75	LT. 38.83
11	173+66.72	LT. 34.87



- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE < 7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. 150 & SYCAMORE STREET LOCATION 13 RAMP DETAIL			F.A.P. RTE. 808	SECTION D5 ADA-2	COUNTY CHAMPAIGN	TOTAL SHEETS 44	SHEET NO. 23
pw:\IL\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\0579\Drawings\Design\0579A47-shr-ADA-Ramp		DRAWN -	REVISED -		SCALE: 1" = 3'	SHEET 2	OF 3 SHEETS	STA.	TO STA.	CONTRACT NO. 70A47		
\$MODELNAME\$	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
	PLOT DATE = 3/17/2016	DATE -	REVISED -									



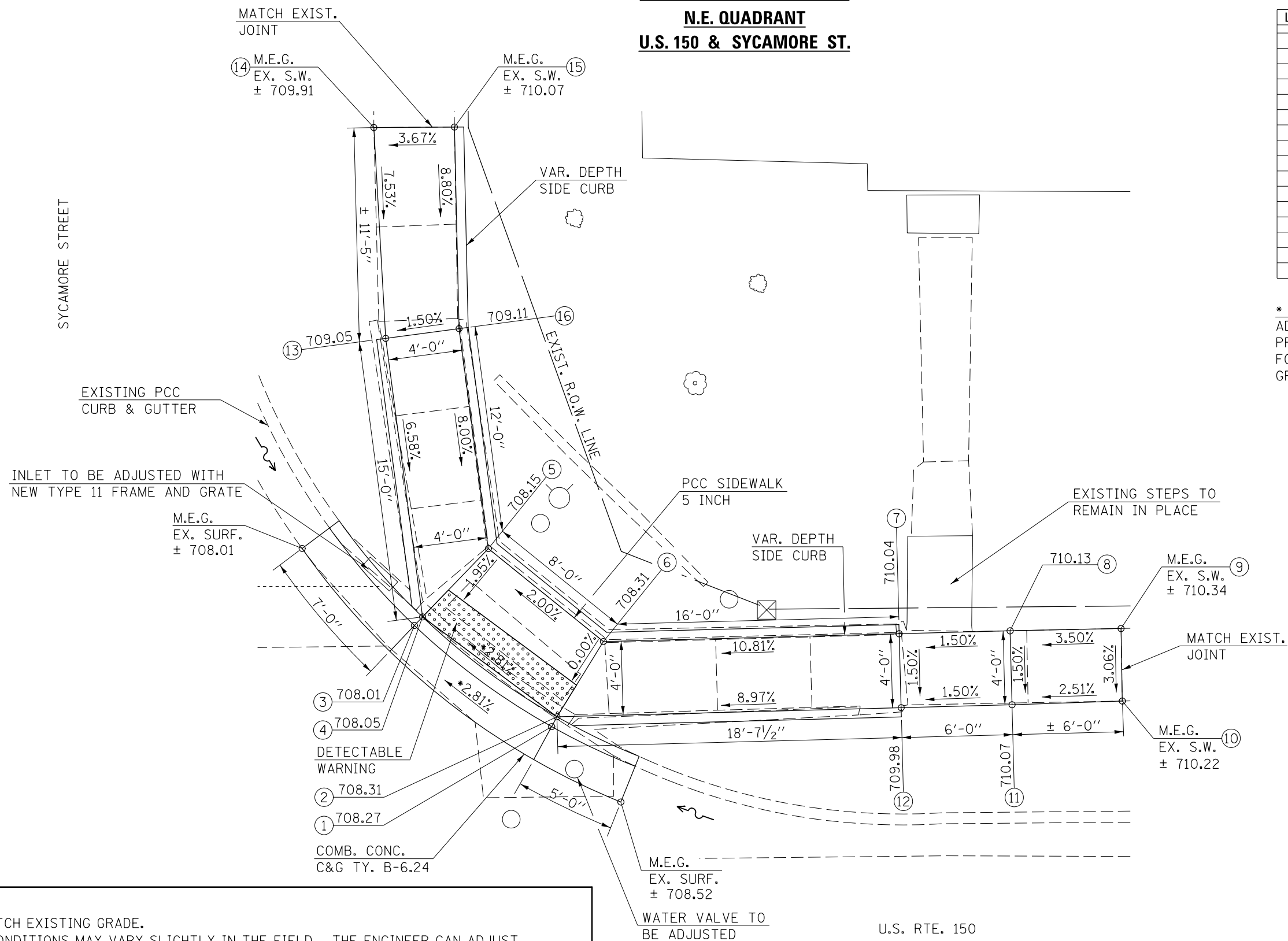
LOCATION #14

N.E. QUADRANT
U.S. 150 & SYCAMORE ST.

LOCATION #14 - N.E. QUADRANT
U.S. RTE. 150 & SYCAMORE ST.

LOCATION	STATION	OFFSET
1	174+49.21	LT. 33.45
2	174+49.51	LT. 33.98
3	174+41.84	LT. 38.99
4	174+42.28	LT. 39.45
5	174+45.88	LT. 43.11
6	174+52.05	LT. 38.02
7	174+68.05	LT. 38.30
8	174+74.05	LT. 38.40
9	174+80.05	LT. 38.47
10	174+80.08	LT. 34.55
11	174+74.12	LT. 34.40
12	174+68.12	LT. 34.30
13	174+40.43	LT. 54.52
14	174+39.89	LT. 65.93
15	174+44.25	LT. 65.92
16	174+44.40	LT. 55.02

* NOTE:
ADA STATEMENT OF MAXIMUM EXTENT PRACTICABLE (MEP) HAS BEEN APPROVED FOR LOWER LANDING CROSS SLOPE GREATER THAN 2.0%.



- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE < 7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.

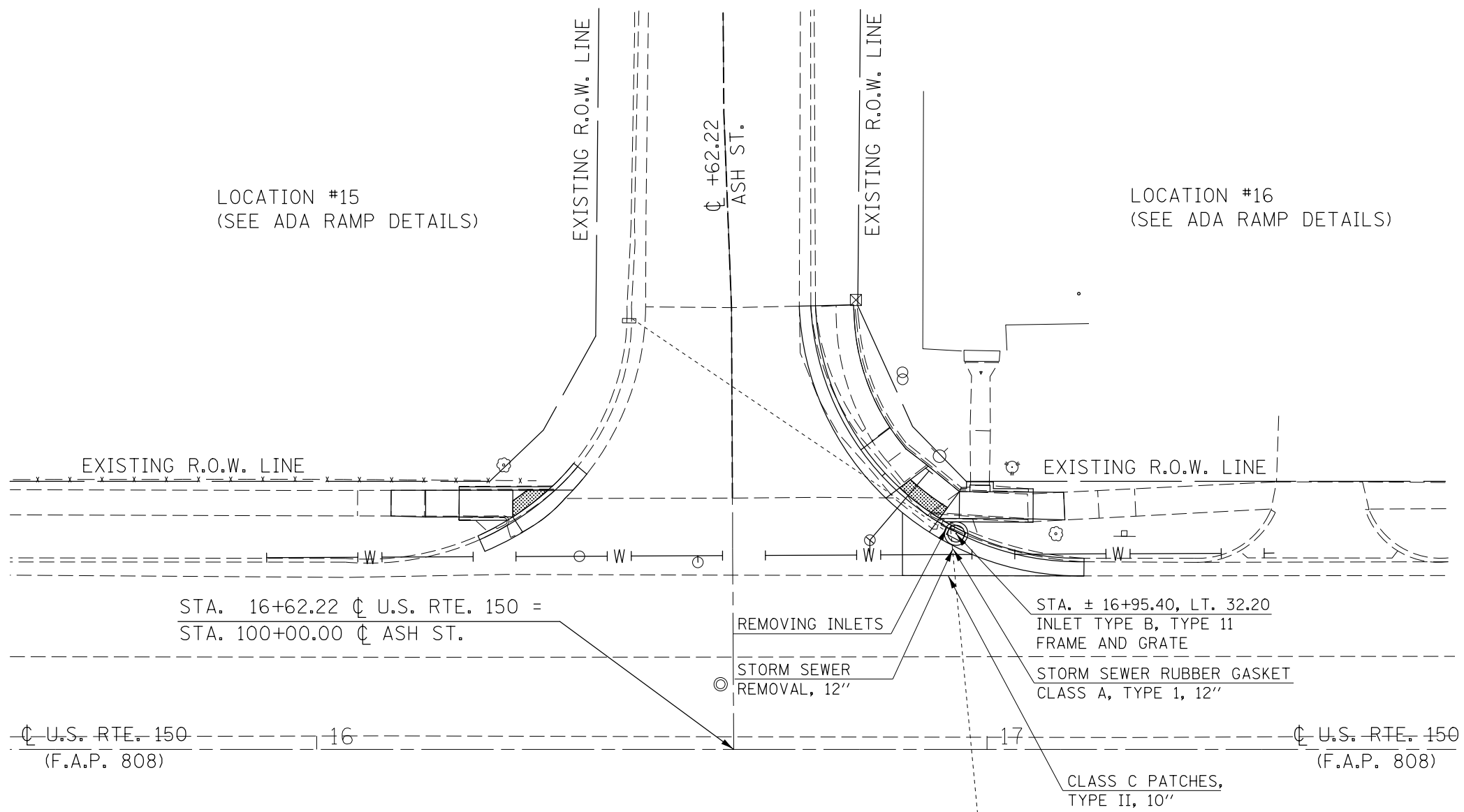
FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -
p:\11\084EBID\INTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\057\DRAWING\Design\0570A47-shr-ADA-Ramp		REVISION	REVISION
	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISED -
\$MODELNAME\$	PLOT DATE = 3/17/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

U.S. 150 & SYCAMORE STREET
LOCATION 14 RAMP DETAIL

SCALE: 1" = 3' SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 ADA-2	CHAMPAIGN	44	24
CONTRACT NO. 70A47			ILLINOIS FED. AID PROJECT	



STA. 16+62.22 CL U.S. RTE. 150 =
 STA. 100+00.00 CL ASH ST.

STA. \pm 16+95.40, LT. 32.20
 INLET TYPE B, TYPE 11
 FRAME AND GRATE

STORM SEWER RUBBER GASKET
 CLASS A, TYPE 1, 12"

CLASS C PATCHES,
 TYPE II, 10"

STA. \pm 16+95.40, LT. 32.20
 INLET TYPE B, TYPE 11 FRAME AND GRATE
 ELEVATION TOP OF GRATE = 713.25
 ELEVATION TOP OF MASONRY = 712.41
 ELEVATION USFL INVERT = 707.54
 ELEVATION DSFL INVERT = 707.40
 DEPTH OF TYPE B INLET = 5.0'

NOTES:

- 1) TO MEET ADA COMPLIANCE ON SIDEWALK WIDTHS, THE EXISTING INLET NEEDS TO BE MOVED TO THE SOUTHEAST.
- 2) THE FINAL LOCATION AND ELEVATIONS OF THE INLET NEEDS TO BE FIELD VERIFIED TO MAKE SURE THERE IS CLEARANCE FROM THE SIDEWALK SIDE CURB AND THAT WATER DRAINS TO THE INLET.
- 3) STORM SEWER LENGTHS WILL NEED MODIFICATION TO ALLOW FOR THE NEW INLET LOCATION. ANY PIPE ELBOWS THAT MAY BE REQUIRED WILL BE CONSIDERED INCIDENTAL TO THE STORM SEWER PAY ITEMS.

FILE NAME =	USER NAME = eaglino	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. 150 AND ASH STREET INTERSECTION PLAN SHEET			F.A.P. RTE. 808	SECTION D5 ADA-2	COUNTY CHAMPAIGN	TOTAL SHEETS 44	SHEET NO. 25
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\057\DRAWING\Design\0570A47-shr-ADA-Ramps		CHECKED -	REVISED -		SCALE: 1" = 10'	SHEET 1	OF 2 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		
\$MODELNAME\$	PLOT DATE = 3/17/2016	DATE -	REVISED -									
CONTRACT NO. 70A47												

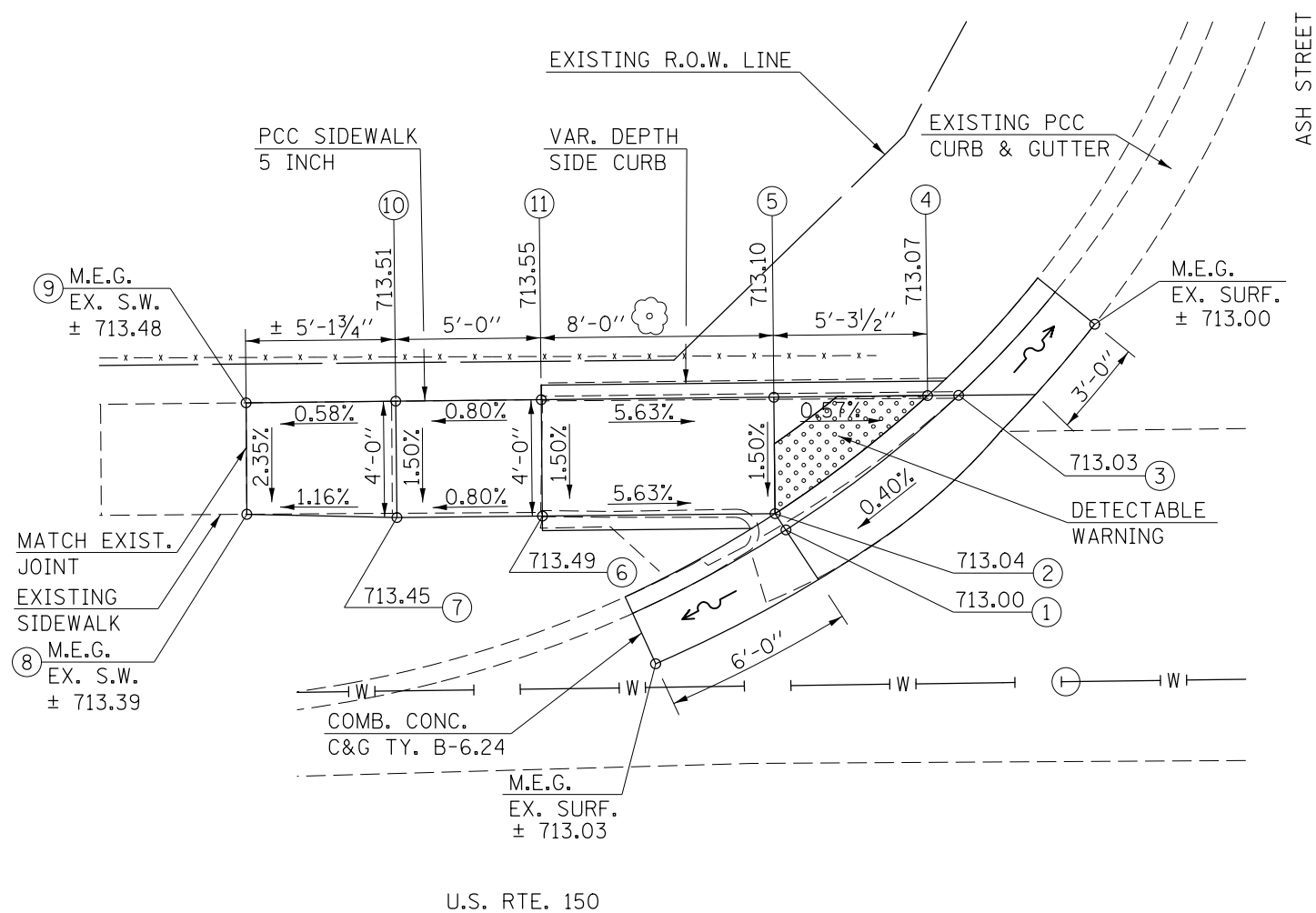


LOCATION #15

N.W. QUADRANT
U.S. 150 & ASH ST.

LOCATION #15 - N.W. QUADRANT
U.S. RTE. 150 & ASH ST.

LOCATION	STATION	OFFSET
1	16+29.63	LT. 34.12
2	16+29.26	LT. 34.69
3	16+35.61	LT. 38.70
4	16+34.54	LT. 38.70
5	16+29.26	LT. 38.69
6	16+21.26	LT. 34.68
7	16+16.26	LT. 34.68
8	16+11.11	LT. 34.84
9	16+11.12	LT. 38.67
10	16+16.26	LT. 38.67
11	16+21.26	LT. 38.68



- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE < 7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -
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\$MODELNAME\$	PLOT DATE = 3/17/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

U.S. 150 & ASH STREET
LOCATION 15 RAMP DETAIL

SCALE: 1" = 3' SHEET 2 OF 3 SHEETS STA. TO STA.

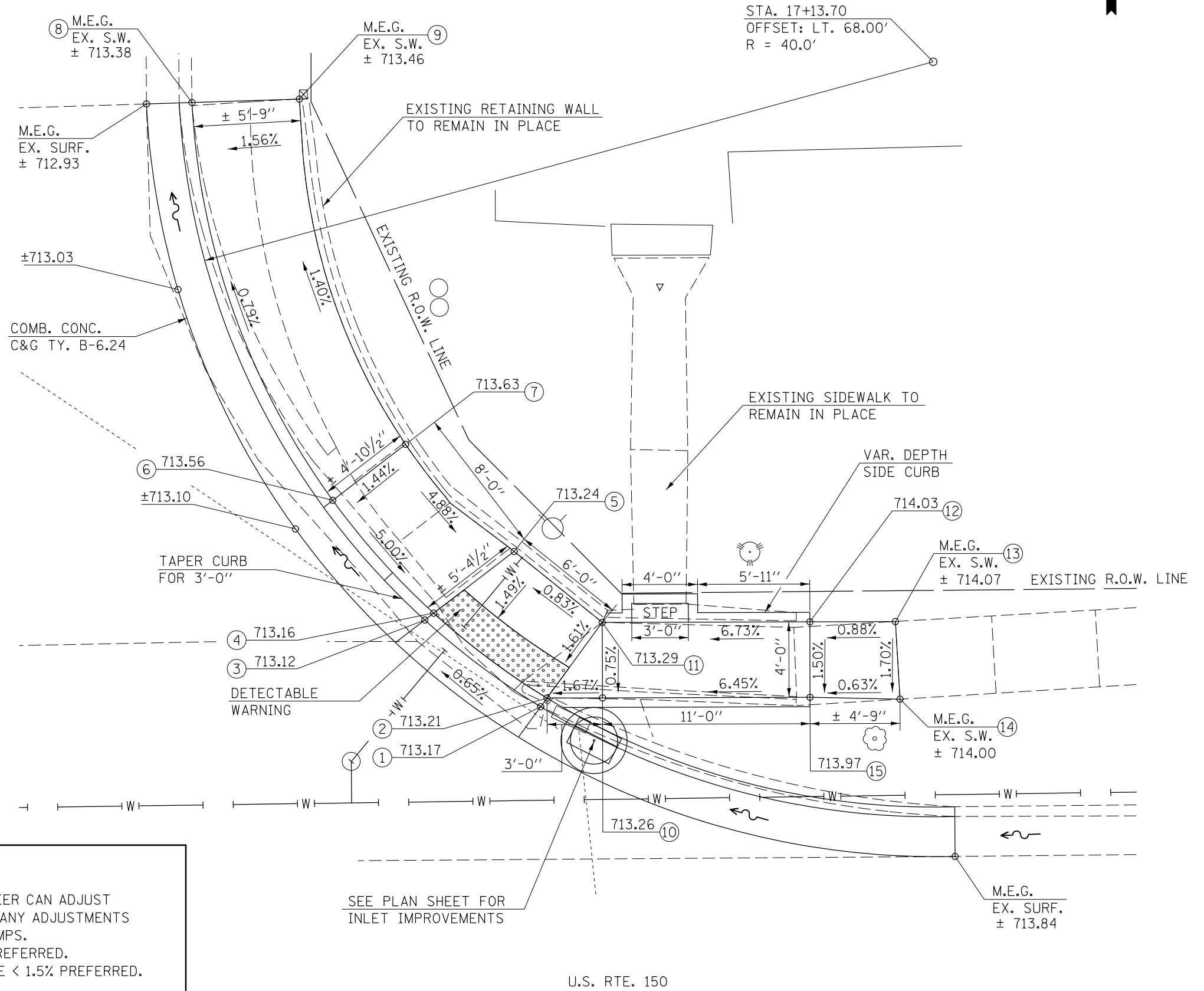
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 ADA-2	CHAMPAIGN	44	26
CONTRACT NO. 70A47			ILLINOIS FED. AID PROJECT	

LOCATION #16 - N.E. QUADRANT
U.S. RTE. 150 & ASH ST.

LOCATION	STATION	OFFSET
1	16+92.59	LT. 34.02
2	16+92.93	LT. 34.48
3	16+86.49	LT. 38.68
4	16+86.97	LT. 39.04
5	16+91.26	LT. 42.27
6	16+81.68	LT. 45.06
7	16+85.56	LT. 48.00
8	16+74.41	LT. 66.21
9	16+80.10	LT. 66.34
10	16+95.87	LT. 34.46
11	16+95.89	LT. 38.46
12	17+06.89	LT. 38.39
13	17+11.43	LT. 38.36
14	17+11.63	LT. 34.25
15	17+06.87	LT. 34.39

LOCATION #16

N.E. QUADRANT
U.S. 150 & ASH ST.



NOTES:

- 1) THE REMOVAL OF THE VARIABLE DEPTH RETAINING WALL AT THE STEP IS CONSIDERED AS SIDEWALK REMOVAL.
- 2) THE CONSTRUCTION OF THE STEP AND VARIABLE DEPTH RETAINING WALL AROUND THE STEP IS CONSIDERED AS PCC SIDEWALK 5".
- 3) THE INSIDE WIDTH OF THE STEP = 3'-0"
- 4) THE RISER HEIGHT AT THE CENTER OF THE STEP = 5 1/2" (MAXIMUM HEIGHT = 7")
- 5) THE TREAD DEPTH = 12"

NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE < 7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -
p:\11\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\0579\DRAWING\Design\0579A47-sh-ADA-Ramp		CHECKED -	REVISED -
		DATE -	REVISED -
#MODELNAME#	PLOT DATE = 3/17/2016		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

U.S. 150 & ASH STREET
LOCATION 16 RAMP DETAIL

SCALE: 1" = 3' SHEET 3 OF 3 SHEETS STA. TO STA.

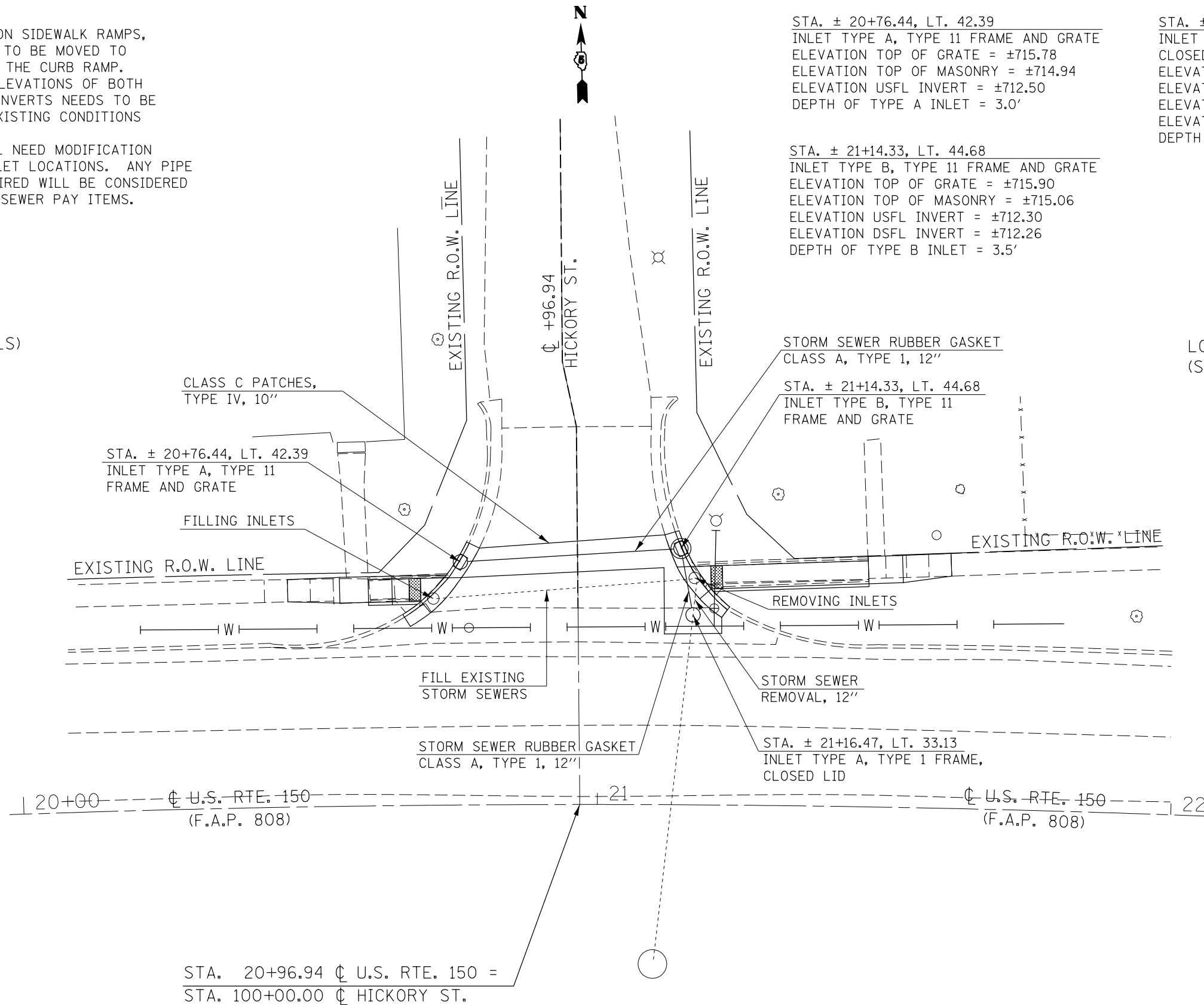
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 ADA-2	CHAMPAIGN	44	27
CONTRACT NO. 70A47			ILLINOIS FED. AID PROJECT	

NOTES:

- 1) TO MEET ADA COMPLIANCE ON SIDEWALK RAMPS, THE EXISTING INLETS NEED TO BE MOVED TO THE NORTH TO GET OUT OF THE CURB RAMP.
- 2) THE FINAL LOCATION AND ELEVATIONS OF BOTH INLETS AND STORM SEWER INVERTS NEEDS TO BE FIELD VERIFIED TO MEET EXISTING CONDITIONS AND ELEVATIONS.
- 3) STORM SEWER LENGTHS WILL NEED MODIFICATION TO ALLOW FOR THE NEW INLET LOCATIONS. ANY PIPE ELBOWS THAT MAY BE REQUIRED WILL BE CONSIDERED INCIDENTAL TO THE STORM SEWER PAY ITEMS.

LOCATION #17
(SEE ADA RAMP DETAILS)

LOCATION #18
(SEE ADA RAMP DETAILS)



FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. 150 AND HICKORY STREET INTERSECTION PLAN SHEET			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0579\Drawings\Design\0579A47-shr-ADA-Ramps	DRAWN	REVISED -	REVISED -					808	D5 ADA-2	CHAMPAIGN	44	28
\$MODELNAME\$	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -					CONTRACT NO. 70A47				
	PLOT DATE = 3/17/2016	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

SCALE: 1" = 10' SHEET 1 OF 3 SHEETS STA. TO STA.

LOCATION #17 - N.W. QUADRANT
U.S. RTE. 150 & HICKORY ST.

LOCATION	STATION	OFFSET
1	20+69.91	LT. 35.18
2	20+69.59	LT. 35.57
3	20+74.37	LT. 39.64
4	20+73.60	LT. 39.63
5	20+69.54	LT. 39.57
6	20+64.61	LT. 39.52
7	20+60.54	LT. 39.98
8	20+55.61	LT. 39.94
9	20+46.65	LT. 39.68
10	20+46.67	LT. 35.80
11	20+55.64	LT. 35.44
12	20+60.57	LT. 35.48
13	20+64.65	LT. 35.52

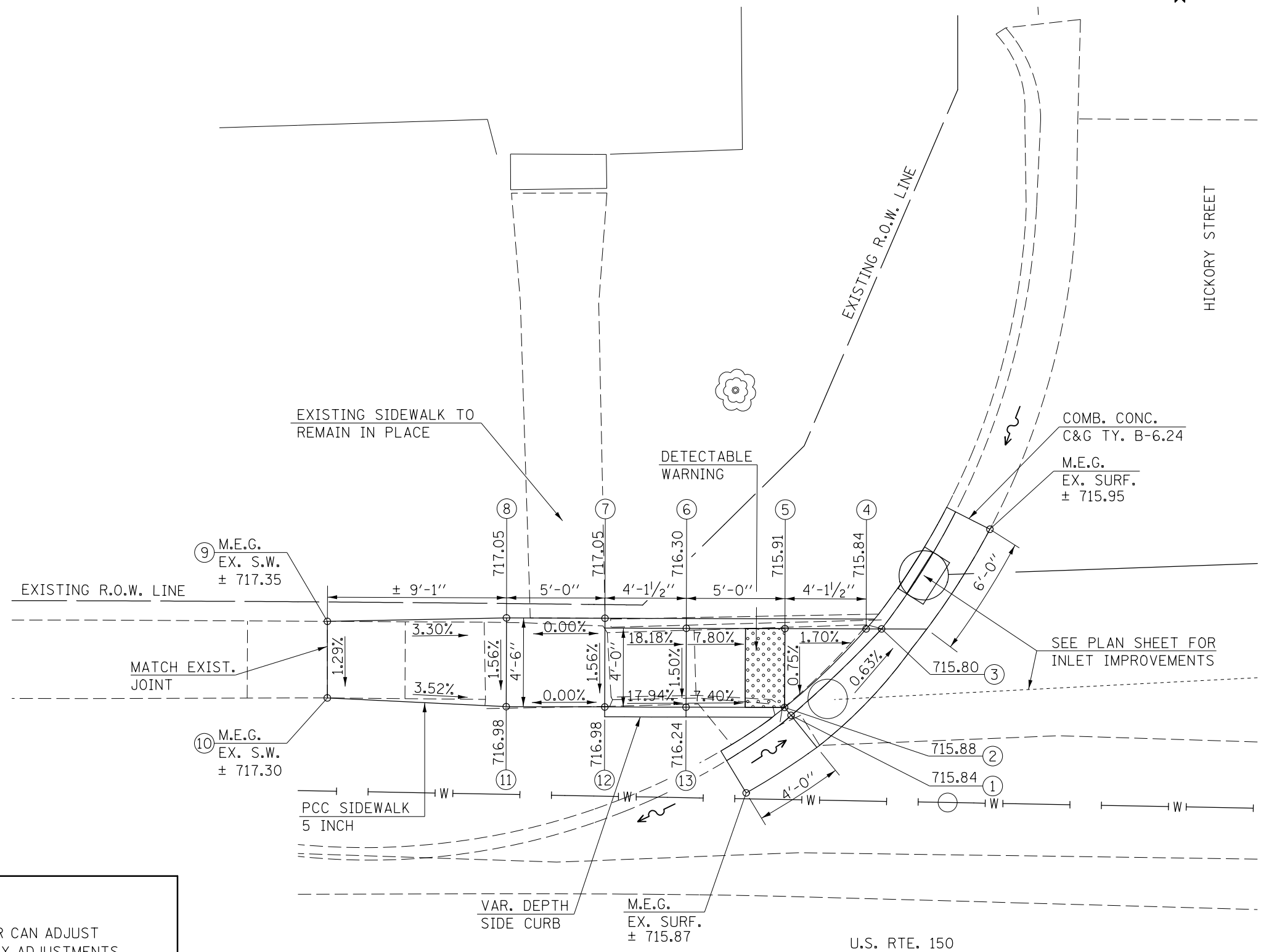
LOCATION #17

N.W. QUADRANT
U.S. 150 & HICKORY ST.



NOTES:

- 1) THE PROPOSED CURB RAMP IS DEFINED AS THE DISTANCE FROM POINT 5 TO POINT 6.
- 2) THE TRANSITION SIDEWALK IS DEFINED AS THE DISTANCE FROM PT 6 TO PT 7.
- 3) THE MAXIMUM RISE AT THE EXISTING STEP IS 7".



NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE < 7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -
p:\1\084EBID\INTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\057\DRAWING\Design\0570A47-shr-ADA-Ramp		DRAWN -	REVISED -
\$MODELNAME\$	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/17/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

U.S. 150 & HICKORY STREET
LOCATION 17 RAMP DETAIL

SCALE: 1" = 3' SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 ADA-2	CHAMPAIGN	44	29
CONTRACT NO. 70A47			ILLINOIS FED. AID PROJECT	



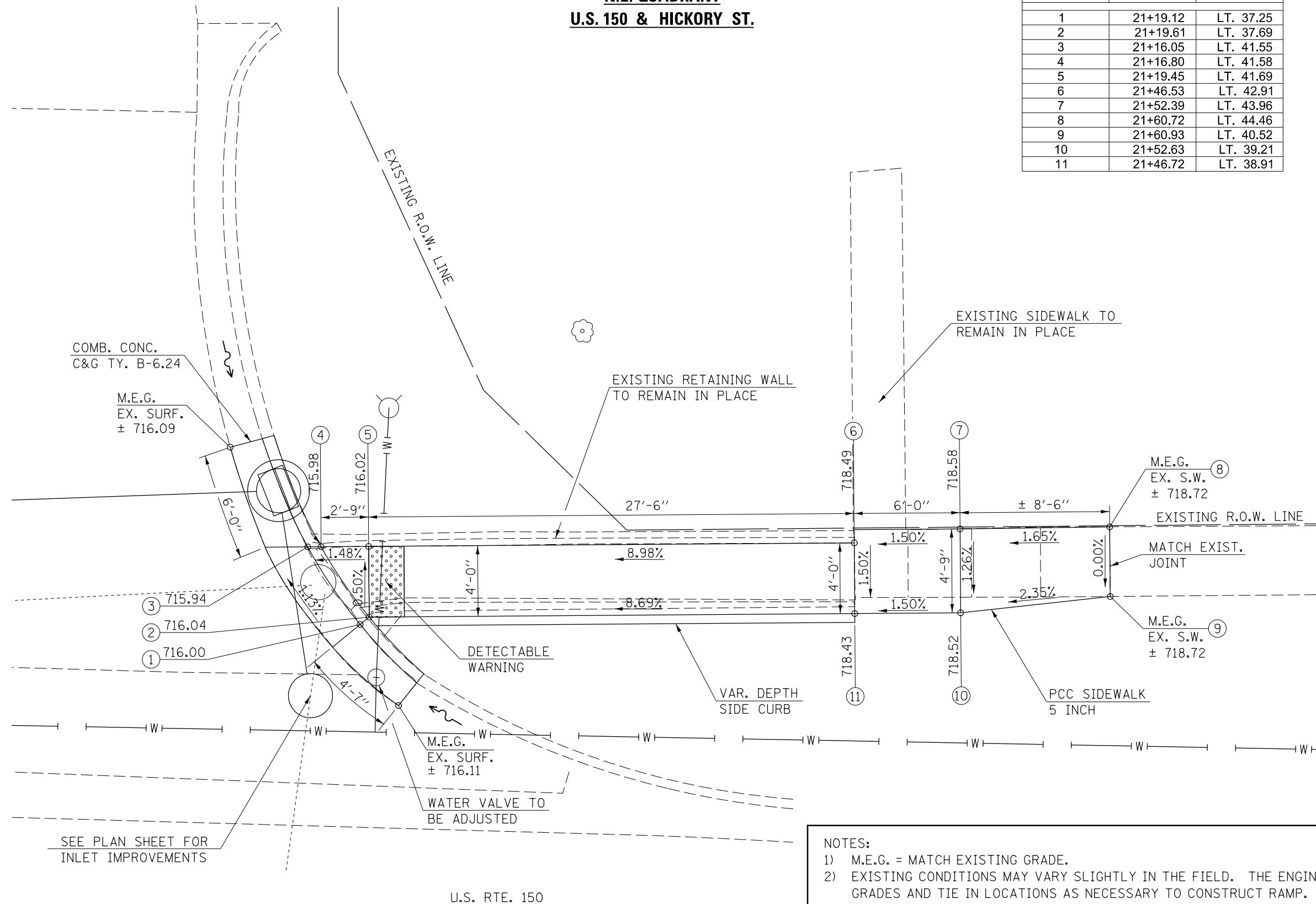
LOCATION #18

**N.E. QUADRANT
U.S. 150 & HICKORY ST.**

LOCATION #18 - N.E. QUADRANT
U.S. RTE. 150 & HICKORY ST.

LOCATION	STATION	OFFSET
1	21+19.12	LT. 37.25
2	21+19.61	LT. 37.69
3	21+16.05	LT. 41.55
4	21+16.80	LT. 41.58
5	21+19.45	LT. 41.69
6	21+46.53	LT. 42.91
7	21+52.39	LT. 43.96
8	21+60.72	LT. 44.46
9	21+60.93	LT. 40.52
10	21+52.63	LT. 39.21
11	21+46.72	LT. 38.91

HICKORY STREET



SEE PLAN SHEET FOR
INLET IMPROVEMENTS

- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE < 7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.

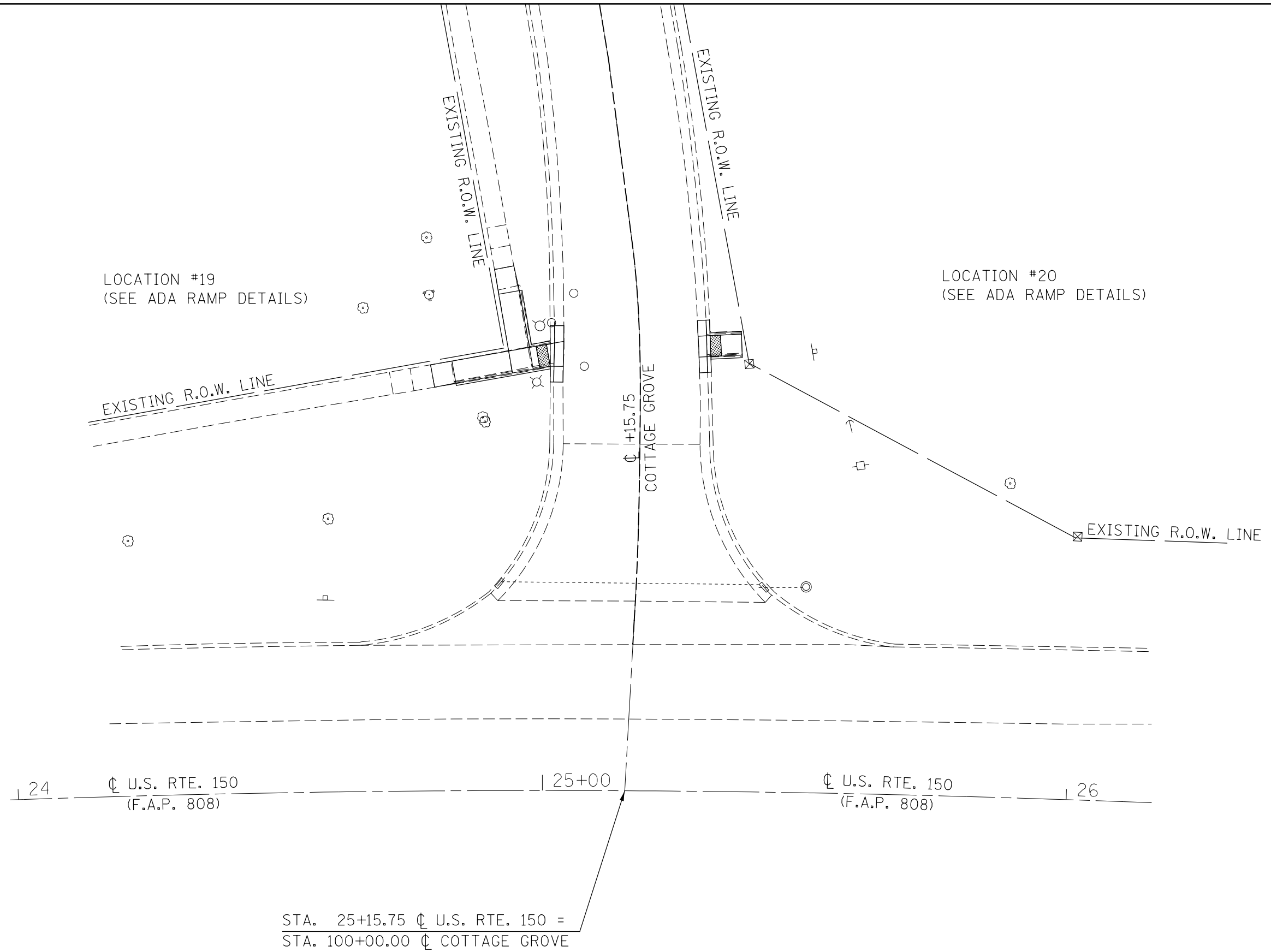
FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -
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	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISED -
\$MODELNAME\$	PLOT DATE = 3/17/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**U.S. 150 & HICKORY STREET
LOCATION 18 RAMP DETAIL**

SCALE: 1" = 3' SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 ADA-2	CHAMPAIGN	44	30
CONTRACT NO. 70A47			ILLINOIS FED. AID PROJECT	



FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\0579\DRAWING\Design\0579A47-shr-ADA.Ramp		CHECKED -	REVISED -
MODELNAME	PLOT DATE = 3/17/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**U.S. 150 AND COTTAGE GROVE
INTERSECTION PLAN SHEET**

SCALE: 1" = 10' SHEET 1 OF 2 SHEETS STA. TO STA.

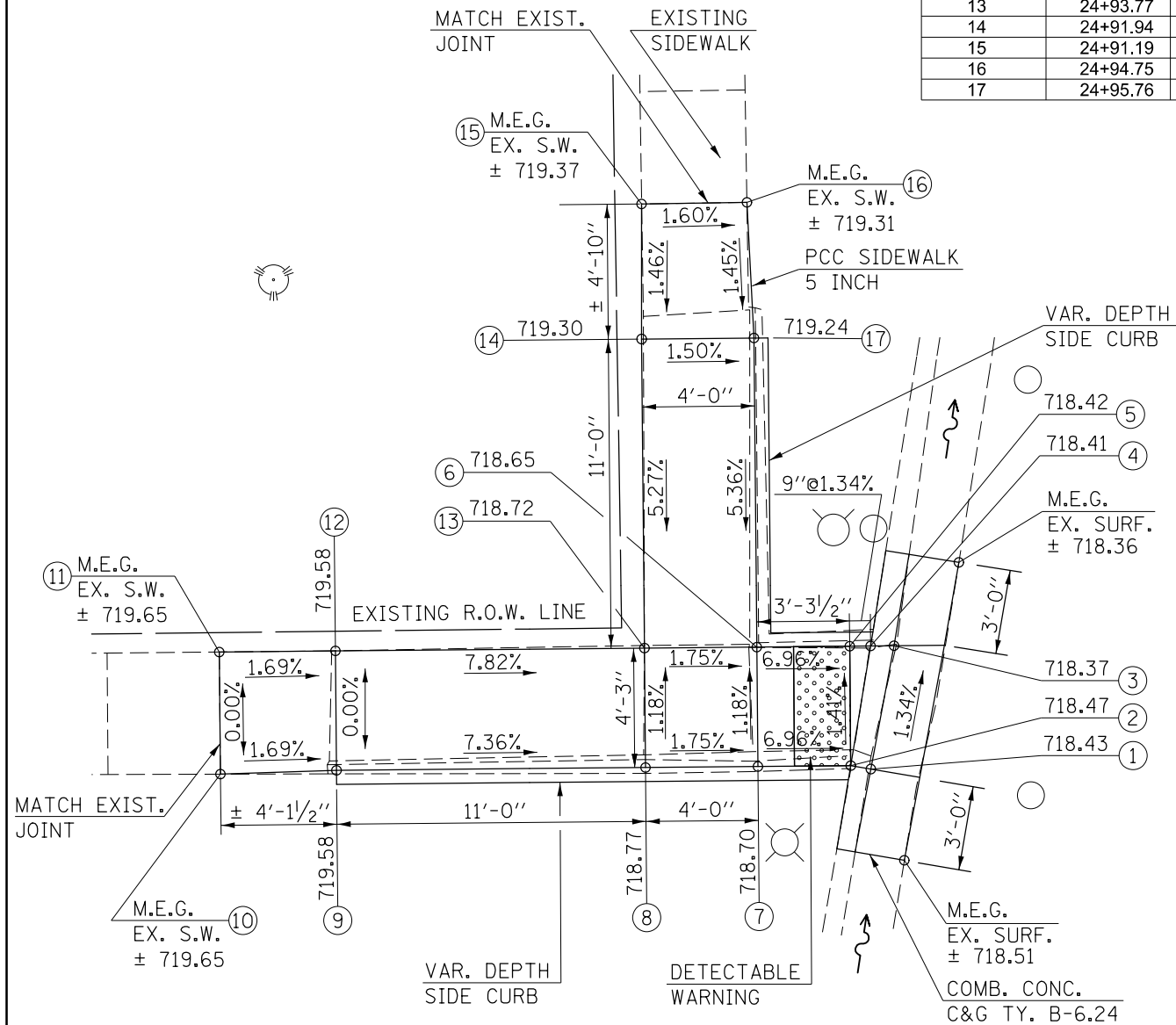
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 ADA-2	CHAMPAIGN	44	31
CONTRACT NO. 70A47			ILLINOIS FED. AID PROJECT	



LOCATION #19
N.W. QUADRANT
U.S. 150 & COTTAGE GROVE

LOCATION #19 - N.W. QUADRANT
 U.S. RTE. 150 & COTTAGE GROVE

LOCATION	STATION	OFFSET
1	25+02.18	LT. 80.98
2	25+01.47	LT. 80.98
3	25+02.27	LT. 85.44
4	25+01.47	LT. 85.29
5	25+00.76	LT. 85.16
6	24+97.59	LT. 84.59
7	24+98.31	LT. 80.40
8	24+94.48	LT. 79.72
9	24+83.93	LT. 77.85
10	24+79.98	LT. 77.06
11	24+79.27	LT. 81.35
12	24+83.23	LT. 82.04
13	24+93.77	LT. 83.90
14	24+91.94	LT. 94.74
15	24+91.19	LT. 99.48
16	24+94.75	LT. 100.15
17	24+95.76	LT. 95.43



COTTAGE GROVE

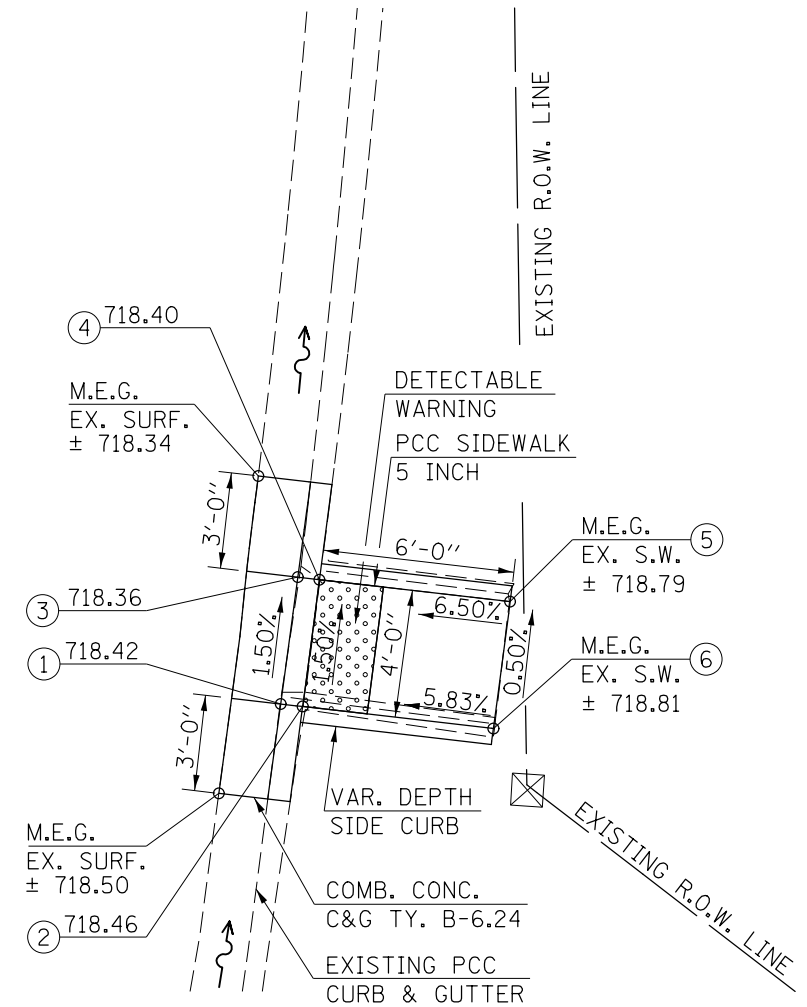
COTTAGE GROVE



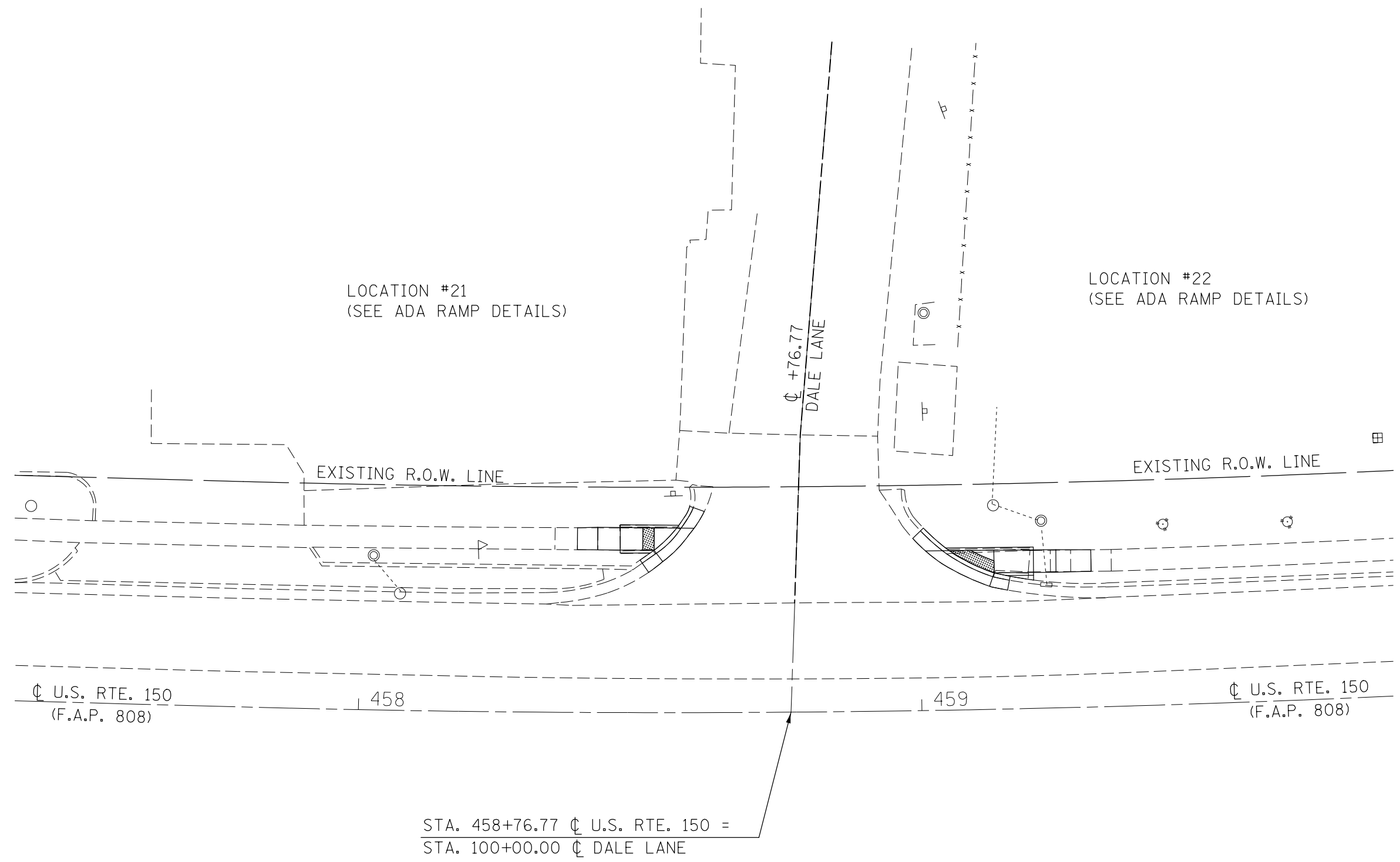
LOCATION #20
N.E. QUADRANT
U.S. 150 & COTTAGE GROVE

LOCATION #20 - N.E. QUADRANT
 U.S. RTE. 150 & COTTAGE GROVE

LOCATION	STATION	OFFSET
1	25+30.58	LT. 82.97
2	25+31.25	LT. 83.01
3	25+30.41	LT. 86.96
4	25+31.08	LT. 87.01
5	25+36.89	LT. 87.38
6	25+37.07	LT. 83.38



- NOTES:
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 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE < 7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.



FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. 150 AND DALE LANE INTERSECTION PLAN SHEET			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PWIDOT\Documents\IDOT Offices\District 5\Projects\0579\DRAWING\Design\0579A47-shr-ADA.Ramp	DRAWN	REVISION	REVISION					808	D5 ADA-2	CHAMPAIGN	44	33
MODELNAME	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -		SCALE: 1" = 10'			SHEET 1 OF 2 SHEETS		CONTRACT NO. 70A47	ILLINOIS FED. AID PROJECT	
	PLOT DATE = 3/17/2016	DATE -	REVISED -		STA. TO STA.							

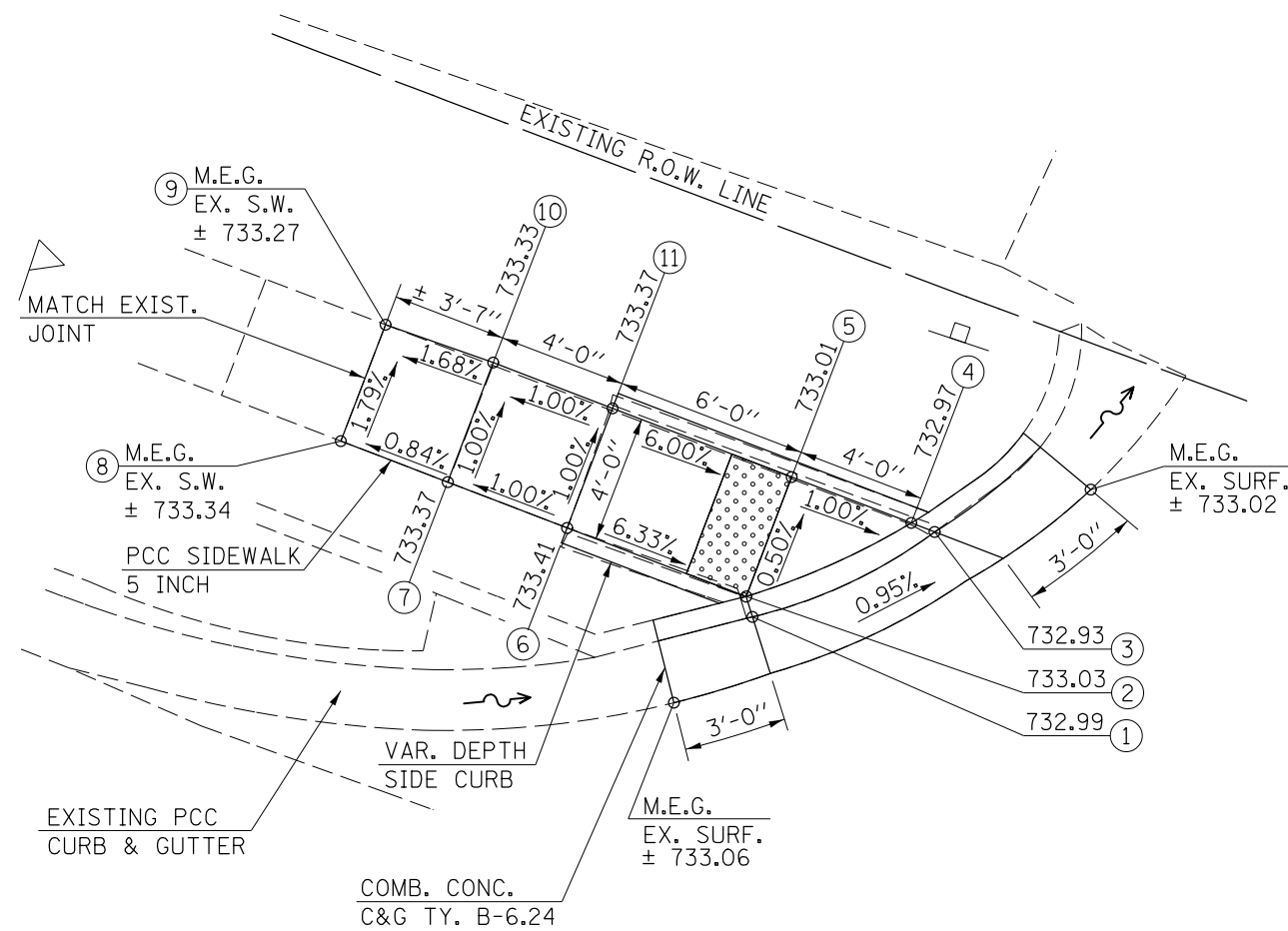


LOCATION #21

N.W. QUADRANT
U.S. 150 & DALE LANE

LOCATION #21 - N.W. QUADRANT
U.S. RTE. 150 & DALE LANE

LOCATION	STATION	OFFSET
1	458+52.85	LT. 28.27
2	458+52.43	LT. 28.80
3	458+57.30	LT. 32.75
4	458+56.51	LT. 32.76
5	458+52.45	LT. 32.80
6	458+46.37	LT. 28.83
7	458+42.33	LT. 28.85
8	458+38.71	LT. 28.87
9	458+38.72	LT. 32.77
10	458+42.34	LT. 32.85
11	458+46.38	LT. 32.84



U.S. RTE. 150

NOTES:

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- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.



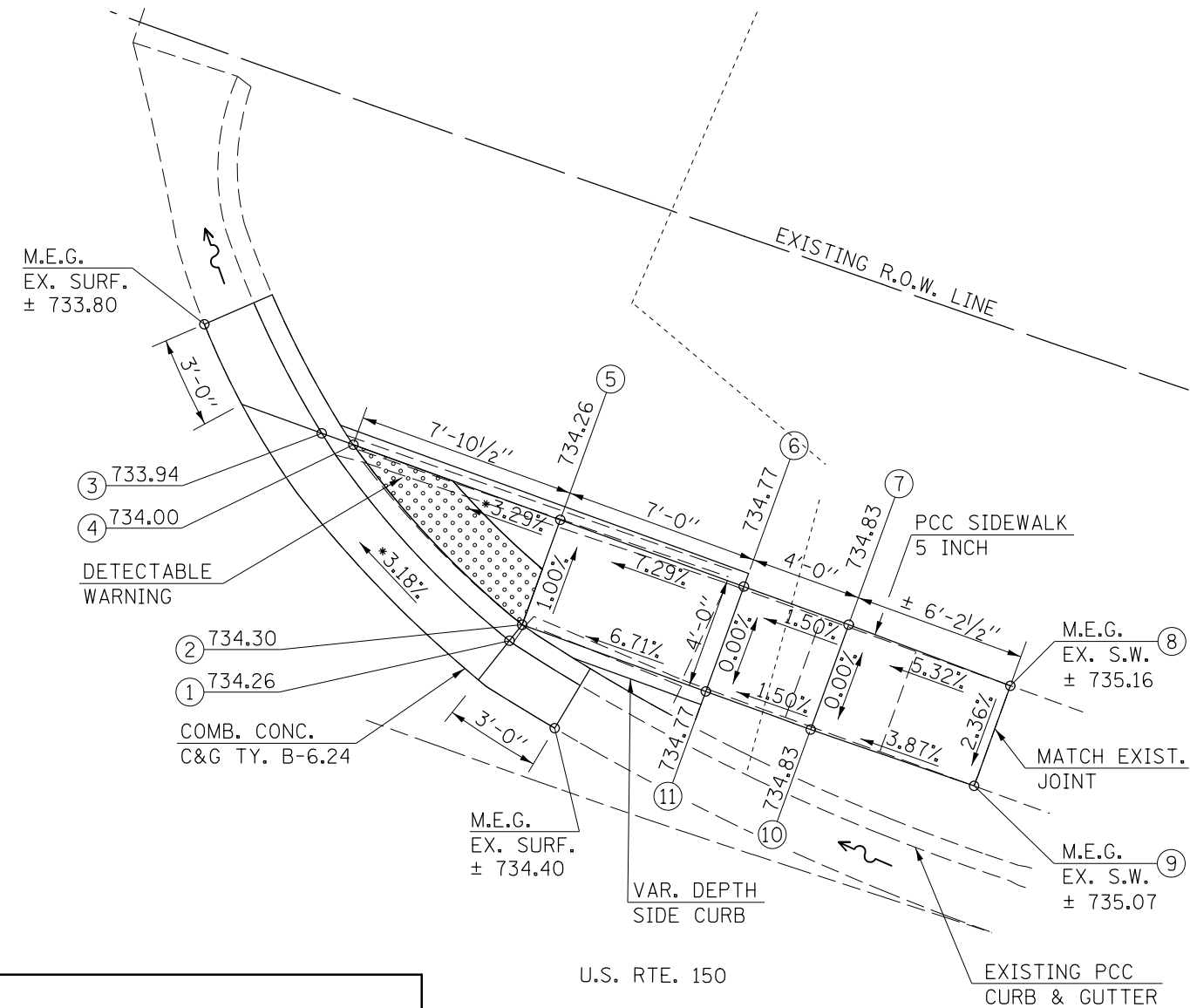
LOCATION #22

N.E. QUADRANT
U.S. 150 & DALE LANE

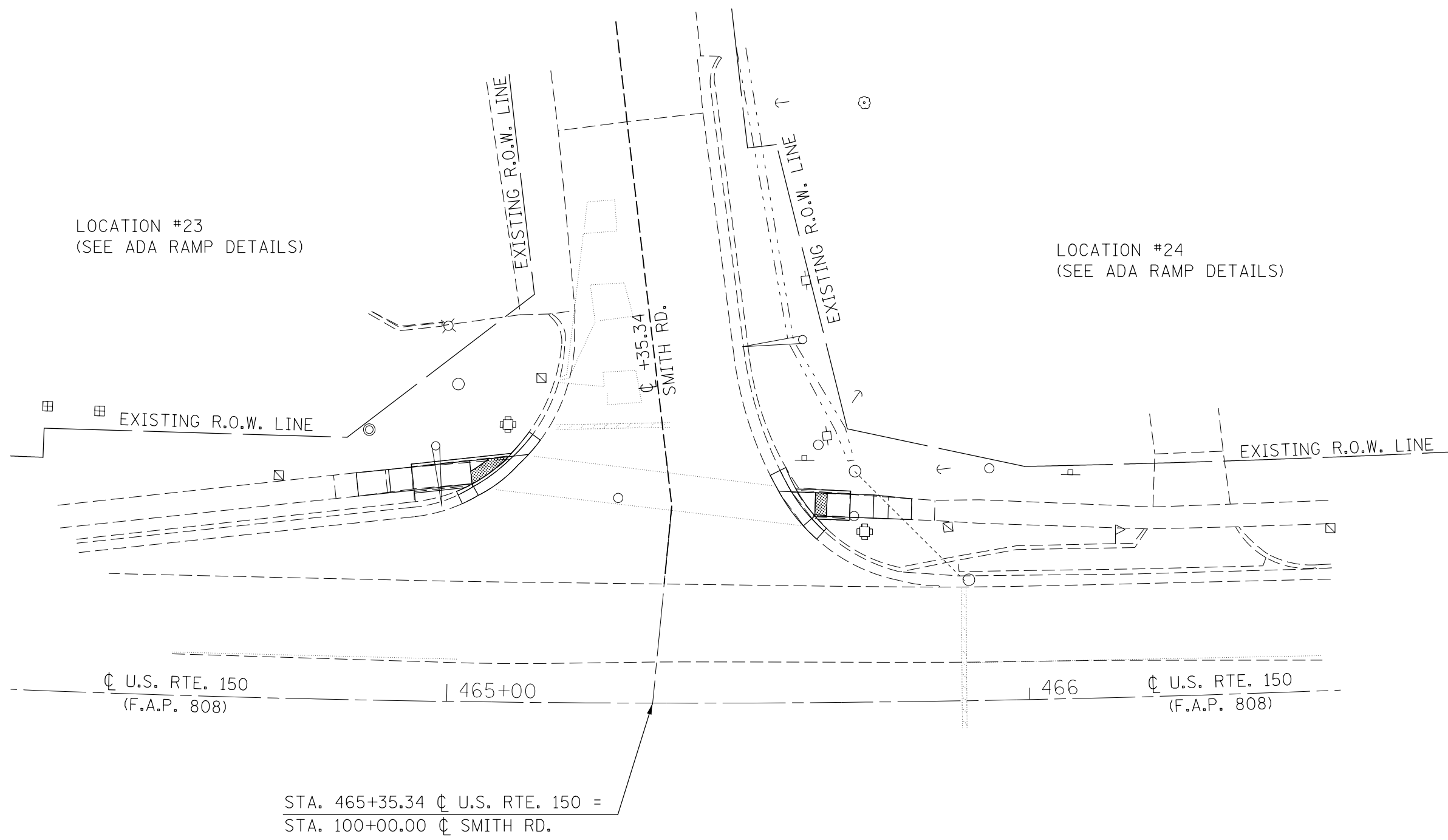
LOCATION #22 - N.E. QUADRANT
U.S. RTE. 150 & DALE LANE

LOCATION	STATION	OFFSET
1	459+13.06	LT. 23.53
2	459+13.30	LT. 24.22
3	459+04.16	LT. 28.31
4	459+05.37	LT. 28.30
5	459+13.34	LT. 28.22
6	459+20.41	LT. 28.14
7	459+24.45	LT. 28.08
8	459+30.71	LT. 27.90
9	459+30.65	LT. 24.08
10	459+24.39	LT. 24.08
11	459+20.36	LT. 24.14

* NOTE:
ADA STATEMENT OF MAXIMUM EXTENT PRACTICABLE (MEP) HAS BEEN APPROVED FOR LOWER LANDING CROSS SLOPE GREATER THAN 2.0%.



U.S. RTE. 150



LOCATION #23
(SEE ADA RAMP DETAILS)

LOCATION #24
(SEE ADA RAMP DETAILS)

+35.34
SMITH RD.

U.S. RTE. 150
(F.A.P. 808)

U.S. RTE. 150
(F.A.P. 808)

STA. 465+35.34 U.S. RTE. 150 =
STA. 100+00.00 SMITH RD.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -
p:\11\084EBID\INTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 5\Projects\057\DRAWING\Design\0570A47-shr-ADA.Ramp		DRAWN -	REVISED -
\$MODELNAME\$	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/17/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**U.S. 150 AND SMITH ROAD
INTERSECTION PLAN SHEET**

SCALE: 1" = 10' SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 ADA-2	CHAMPAIGN	44	35
CONTRACT NO. 70A47			ILLINOIS FED. AID PROJECT	

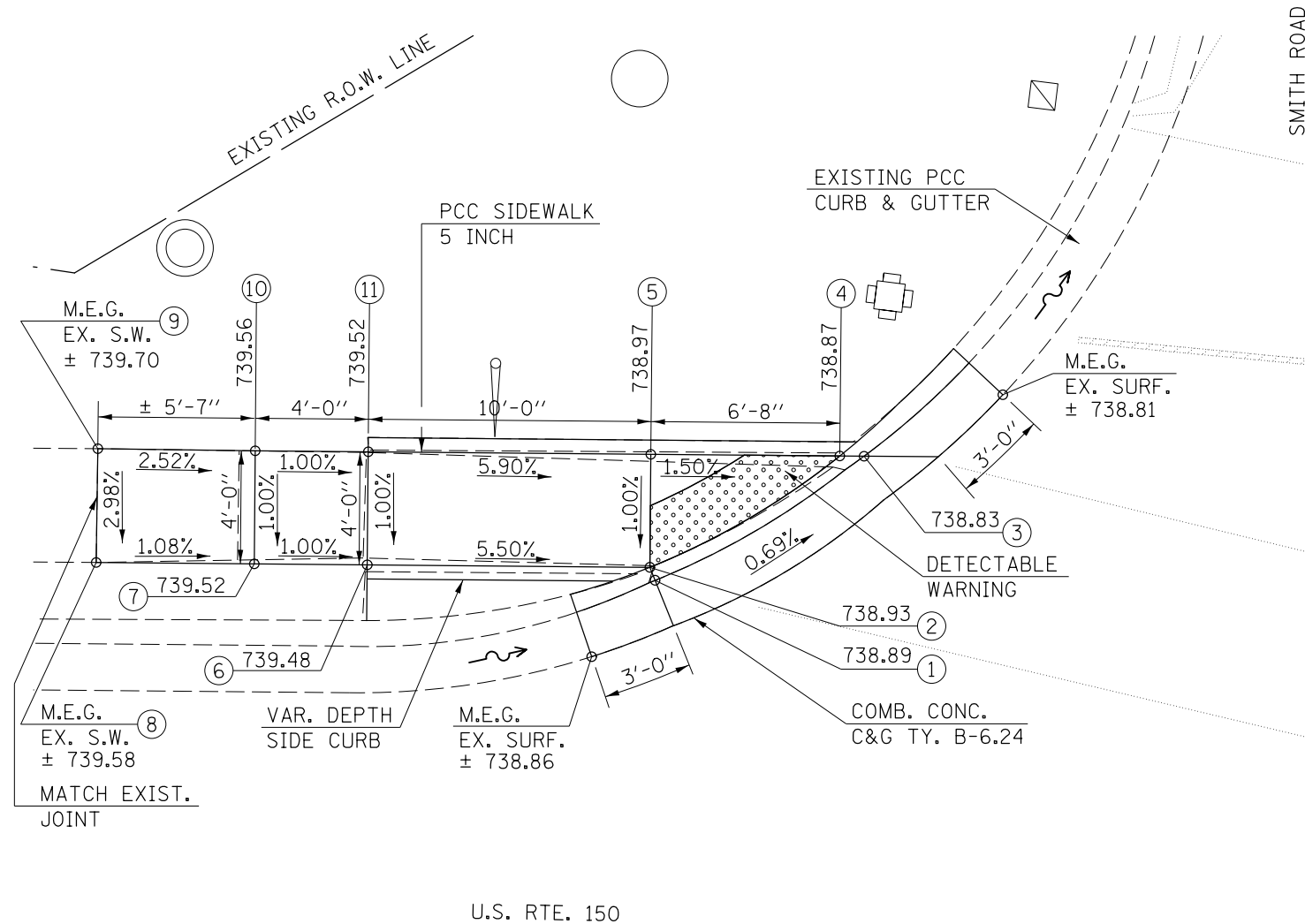


LOCATION #23

N.W. QUADRANT U.S. 150 & SMITH ROAD

LOCATION #23 - N.W. QUADRANT
U.S. RTE. 150 & SMITH ROAD

LOCATION	STATION	OFFSET
1	465+04.12	LT. 36.93
2	465+03.88	LT. 37.36
3	465+10.99	LT. 42.22
4	465+10.13	LT. 42.12
5	465+03.39	LT. 41.33
6	464+93.82	LT. 36.15
7	464+89.80	LT. 35.65
8	464+84.18	LT. 34.95
9	464+83.69	LT. 38.95
10	464+89.29	LT. 39.62
11	464+93.32	LT. 40.12



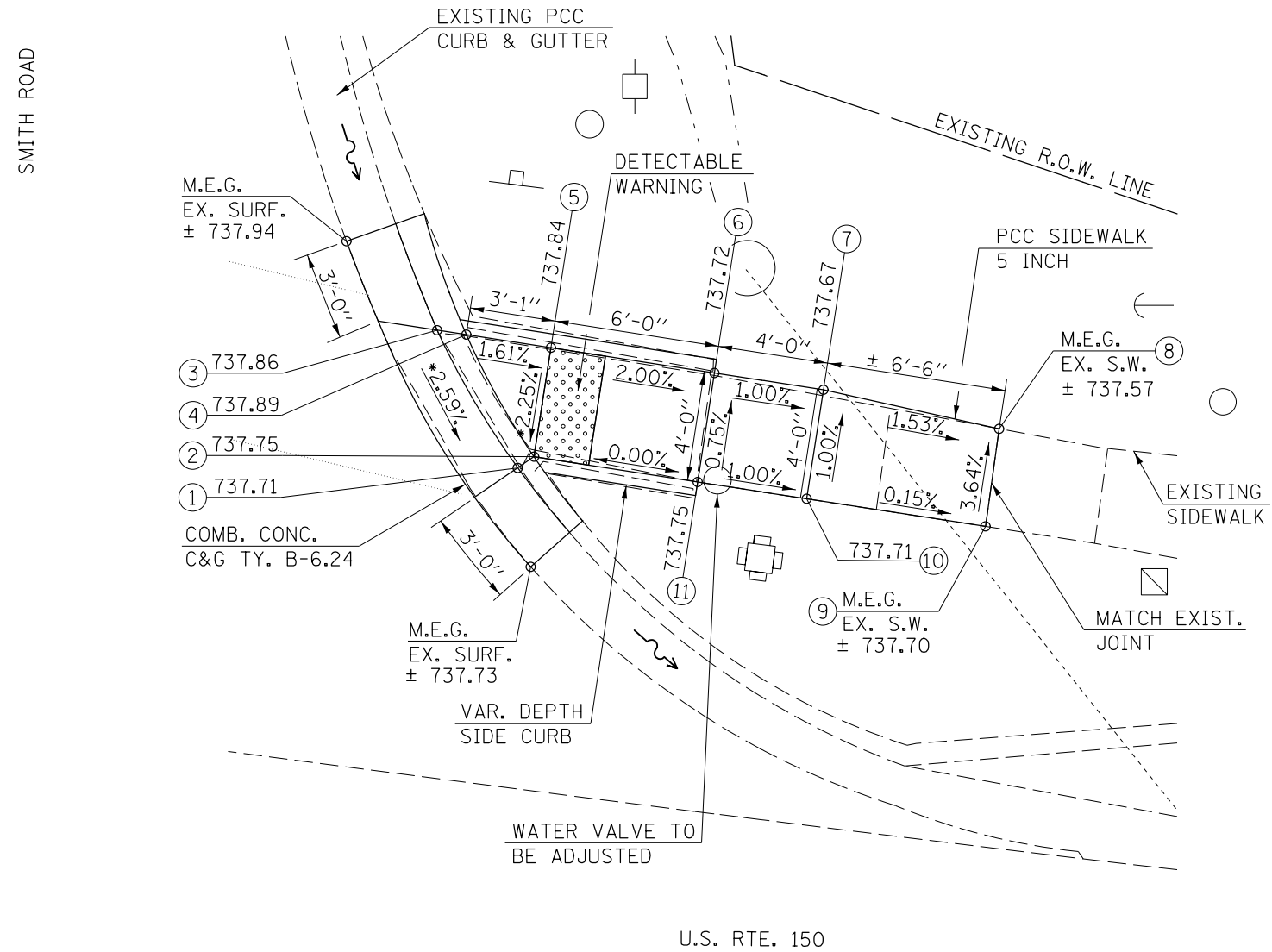
LOCATION #24 - N.E. QUADRANT
U.S. RTE. 150 & SMITH ROAD

LOCATION #24

N.E. QUADRANT U.S. 150 & SMITH ROAD

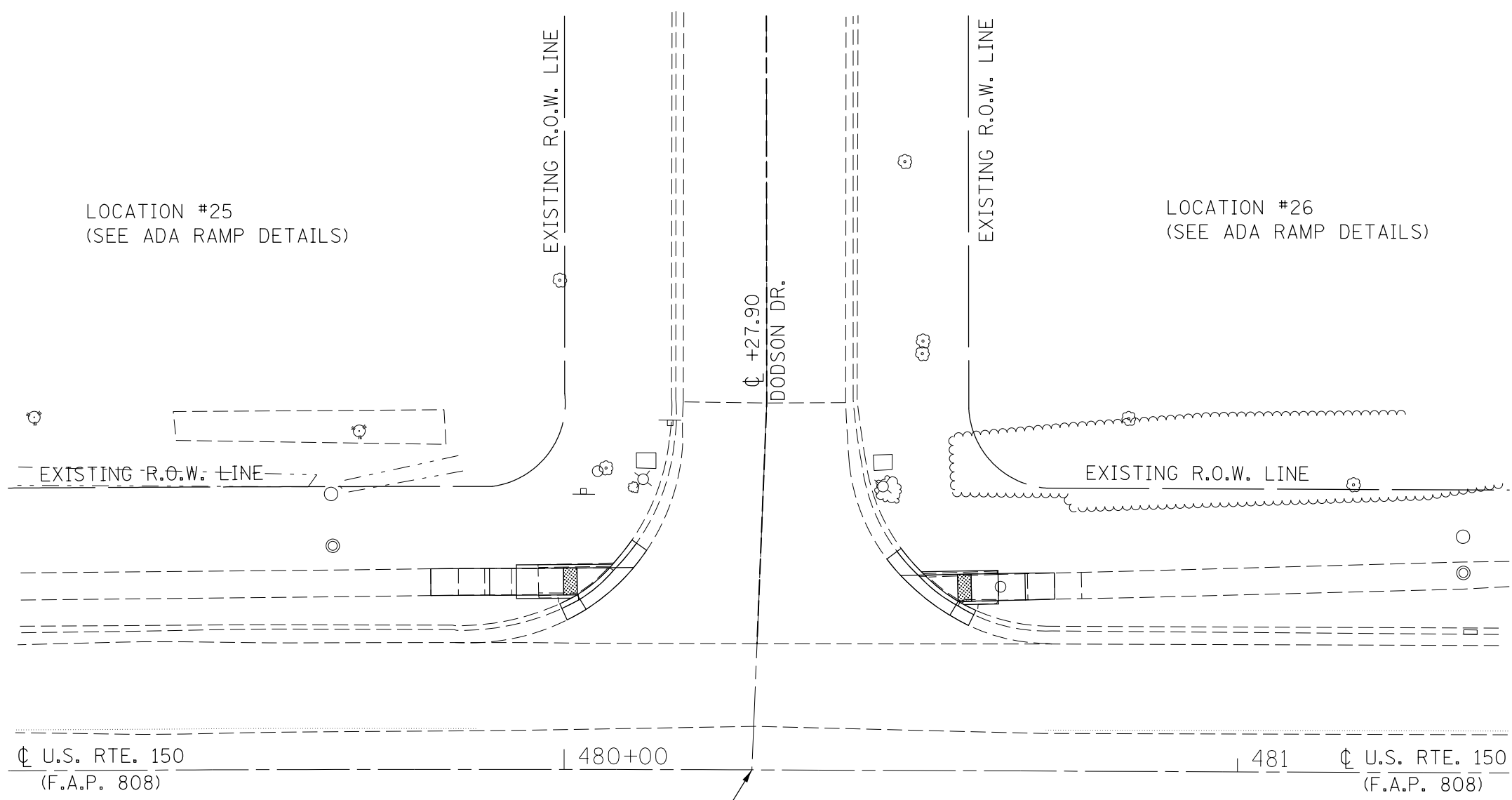
LOCATION	STATION	OFFSET
1	465+62.86	LT. 31.56
2	465+63.41	LT. 32.02
3	465+59.37	LT. 36.21
4	465+60.45	LT. 36.16
5	465+63.59	LT. 36.02
6	465+69.66	LT. 35.74
7	465+73.71	LT. 35.54
8	465+80.26	LT. 34.79
9	465+80.13	LT. 31.22
10	465+73.51	LT. 31.55
11	465+69.47	LT. 31.74

* NOTE:
ADA STATEMENT OF MAXIMUM EXTENT
PRACTICABLE (MEP) HAS BEEN APPROVED
FOR LOWER LANDING CROSS SLOPE
GREATER THAN 2.0%.



- NOTES:
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 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. 150 & SMITH ROAD LOCATIONS 23 & 24 RAMP DETAILS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL084EBIDINTEG.illinois.gov\PIDOT\Documents\DOT Offices\District 5\Projects\0579\Drawings\Design\0579A47-shr-ADA-Ramp	DRAWN -	CHECKED -	REVISED -			808	D5 ADA-2	CHAMPAIGN	44	36	
#MODELNAME#	PLOT SCALE = 6.0000' / in.	DATE -	REVISED -			CONTRACT NO. 70A47					
	PLOT DATE = 3/17/2016		REVISED -			ILLINOIS FED. AID PROJECT					



LOCATION #25
(SEE ADA RAMP DETAILS)

LOCATION #26
(SEE ADA RAMP DETAILS)

$$\begin{aligned} \text{STA. } 480+27.90 \text{ } \zeta \text{ U.S. RTE. 150} &= \\ \text{STA. } 100+00.00 \text{ } \zeta \text{ DODSON DR.} & \end{aligned}$$

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. 150 AND DODSON DRIVE INTERSECTION PLAN SHEET			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PWIDOT\Documents\IDOT Offices\District 5\Projects\0570A47\DRAWING\Design\0570A47-shr-ADA.Ramp	PLotted SCALE = 20.0000' / in.	CHECKED -	REVISED -					808	D5 ADA-2	CHAMPAIGN	44	37
\$MODELNAME\$	PLOT DATE = 3/17/2016	DATE -	REVISED -		SCALE: 1" = 10' SHEET 1 OF 2 SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT				
								CONTRACT NO. 70A47				



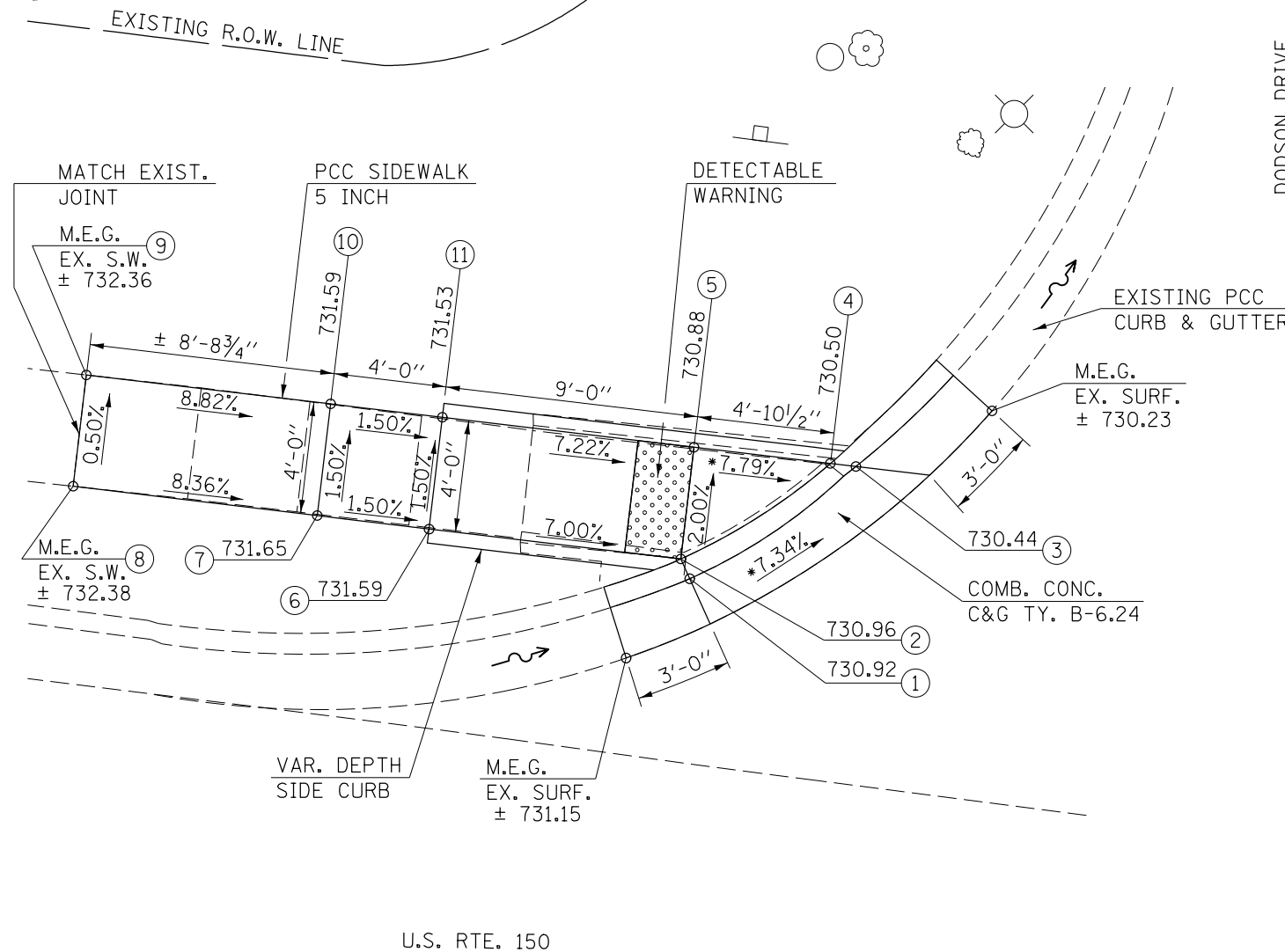
LOCATION #25

N.W. QUADRANT U.S. 150 & DODSON DRIVE

* NOTE:
ADA STATEMENT OF MAXIMUM EXTENT
PRACTICABLE (MEP) HAS BEEN APPROVED
FOR LOWER LANDING CROSS SLOPE
GREATER THAN 2.0%.

LOCATION #25 - N.W. QUADRANT
U.S. RTE. 150 & DODSON DRIVE

LOCATION	STATION	OFFSET
1	480+02.31	LT. 25.27
2	480+01.91	LT. 25.93
3	480+07.65	LT. 29.97
4	480+06.74	LT. 29.97
5	480+01.88	LT. 29.93
6	479+92.93	LT. 25.86
7	479+88.94	LT. 25.84
8	479+80.23	LT. 25.80
9	479+80.21	LT. 29.78
10	479+88.92	LT. 29.84
11	479+92.91	LT. 29.86

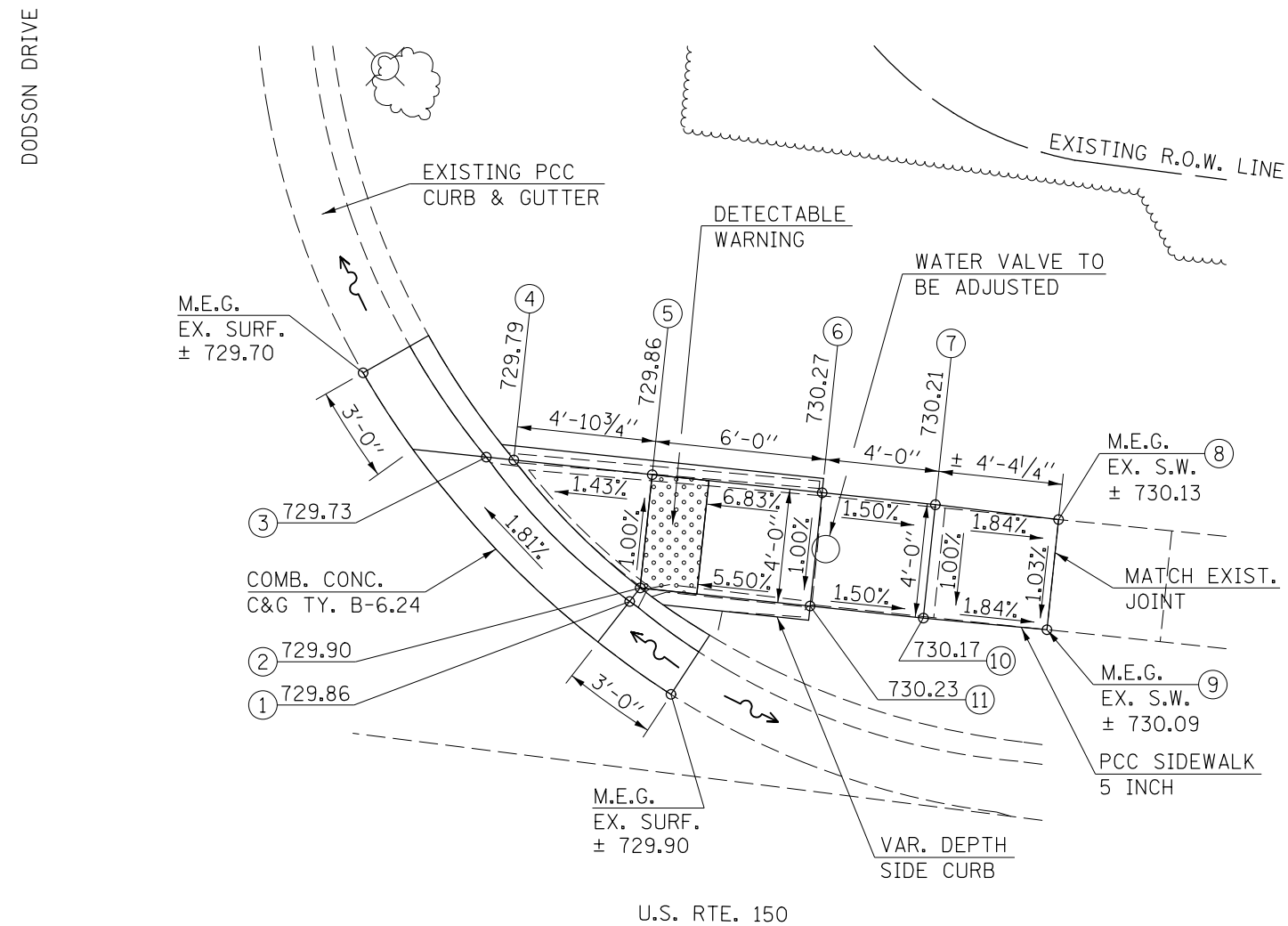


LOCATION #26

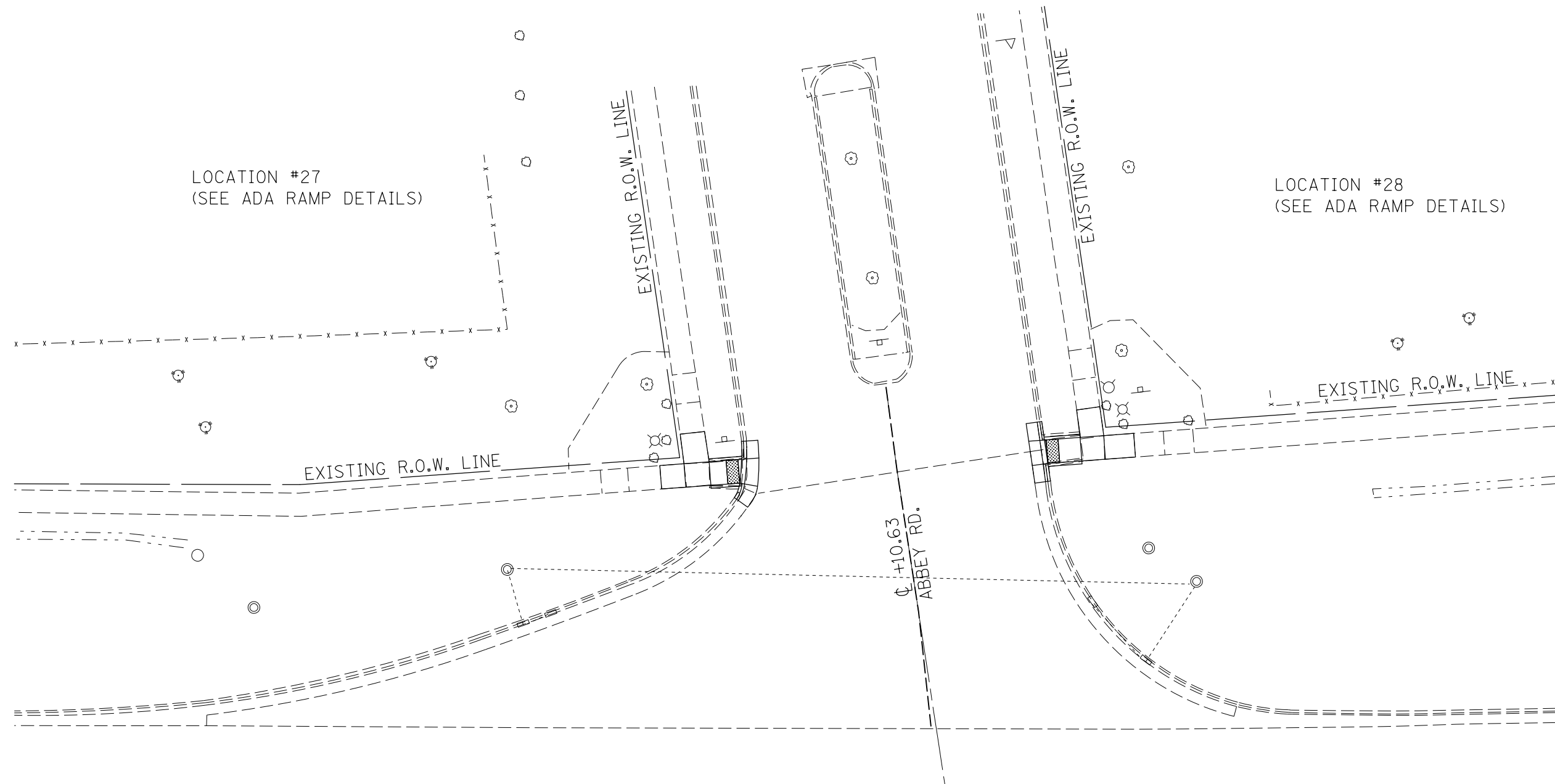
N.E. QUADRANT U.S. 150 & DODSON DRIVE

LOCATION #26 - N.E. QUADRANT
U.S. RTE. 150 & DODSON DRIVE

LOCATION	STATION	OFFSET
1	480+58.12	LT. 24.63
2	480+58.42	LT. 25.14
3	480+52.47	LT. 29.01
4	480+53.45	LT. 29.03
5	480+58.33	LT. 29.14
6	480+64.33	LT. 29.28
7	480+68.33	LT. 29.38
8	480+72.69	LT. 29.42
9	480+72.78	LT. 25.53
10	480+68.42	LT. 25.38
11	480+64.42	LT. 25.28



- NOTES:
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 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE < 7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.



LOCATION #27
(SEE ADA RAMP DETAILS)

LOCATION #28
(SEE ADA RAMP DETAILS)

U.S. RTE. 150
(F.A.P. 808)

489

490+00

U.S. RTE. 150
(F.A.P. 808)

491

STA. 490+10.63 U.S. RTE. 150 =
STA. 100+00.00 ABBEY ROAD

FILE NAME =	USER NAME = eaglino	DESIGNED -	REVISED -
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	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -
MODELNAME	PLOT DATE = 3/17/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**U.S. 150 AND ABBEY ROAD
INTERSECTION PLAN SHEET**

SCALE: 1" = 10' SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 ADA-2	CHAMPAIGN	44	39
CONTRACT NO. 70A47			ILLINOIS FED. AID PROJECT	

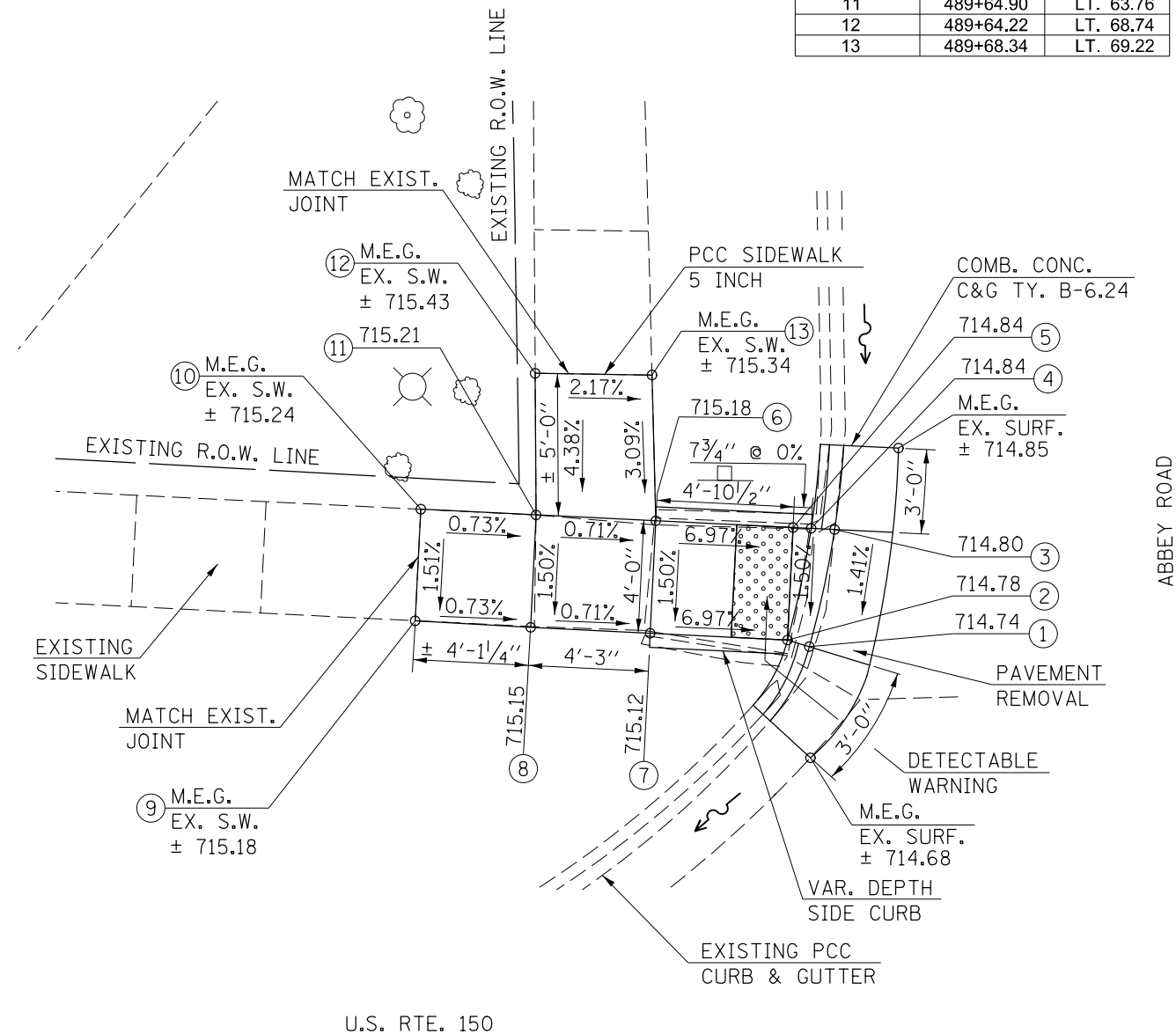


LOCATION #27

N.W. QUADRANT U.S. 150 & ABBEY ROAD

LOCATION #27 - N.W. QUADRANT
U.S. RTE. 150 & ABBEY ROAD

LOCATION	STATION	OFFSET
1	489+75.12	LT. 60.36
2	489+74.32	LT. 60.50
3	489+75.47	LT. 64.61
4	489+74.65	LT. 64.54
5	489+74.00	LT. 64.49
6	489+69.14	LT. 64.10
7	489+69.45	LT. 60.11
8	489+65.21	LT. 59.77
9	489+61.12	LT. 59.48
10	489+60.81	LT. 63.44
11	489+64.90	LT. 63.76
12	489+64.22	LT. 68.74
13	489+68.34	LT. 69.22

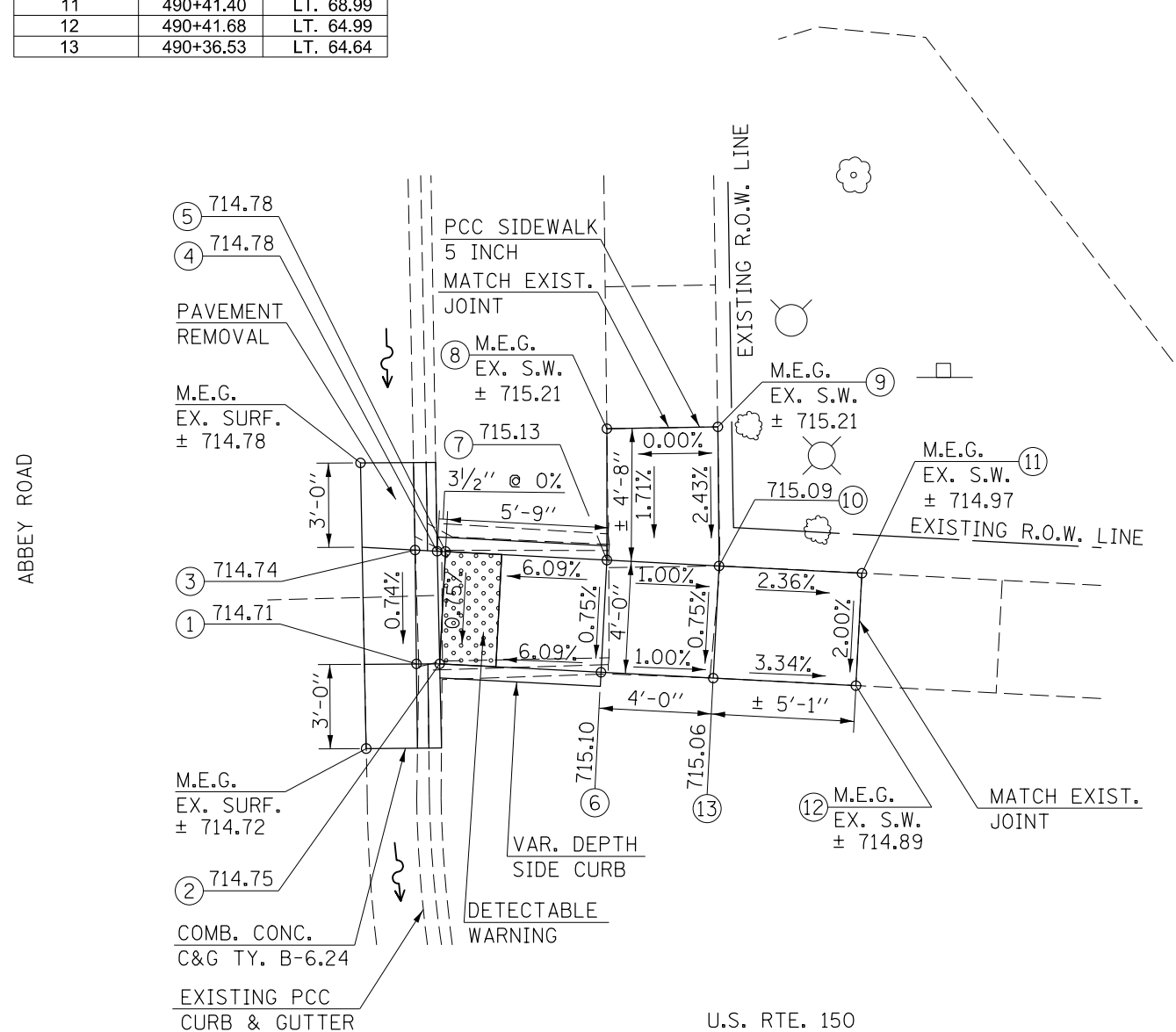


LOCATION #28 - N.E. QUADRANT
U.S. RTE. 150 & ABBEY ROAD

LOCATION	STATION	OFFSET
1	490+25.83	LT. 63.84
2	490+26.66	LT. 63.95
3	490+25.27	LT. 67.86
4	490+26.07	LT. 67.92
5	490+26.37	LT. 67.94
6	490+32.48	LT. 64.36
7	490+32.19	LT. 68.35
8	490+31.61	LT. 72.99
9	490+35.59	LT. 73.54
10	490+36.25	LT. 68.63
11	490+41.40	LT. 68.99
12	490+41.68	LT. 64.99
13	490+36.53	LT. 64.64

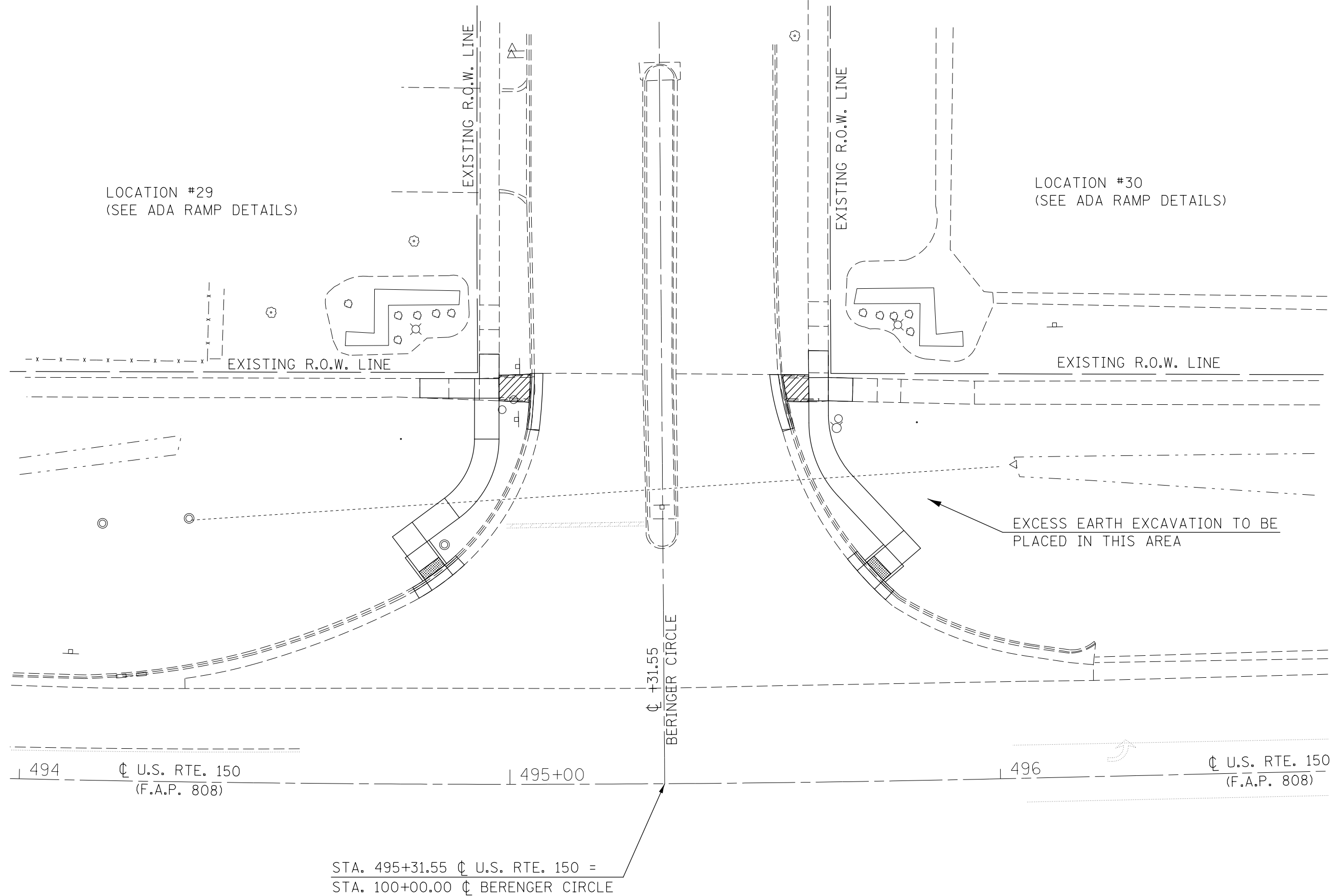
LOCATION #28

N.E. QUADRANT U.S. 150 & ABBEY ROAD



NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
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- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.



STA. 495+31.55 ϕ U.S. RTE. 150 =
 STA. 100+00.00 ϕ BERENGER CIRCLE

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. 150 AND BERINGER CIRCLE INTERSECTION PLAN SHEET			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\0570A47\Drawings\Design\0570A47-shr-ADA-Ramps	DRAWN	REVISION	REVISION					808	D5 ADA-2	CHAMPAIGN	44	41
\$MODELNAME\$	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -		SCALE: 1" = 10'			SHEET 1 OF 3 SHEETS		STA.	TO STA.	CONTRACT NO. 70A47
	PLOT DATE = 3/17/2016	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

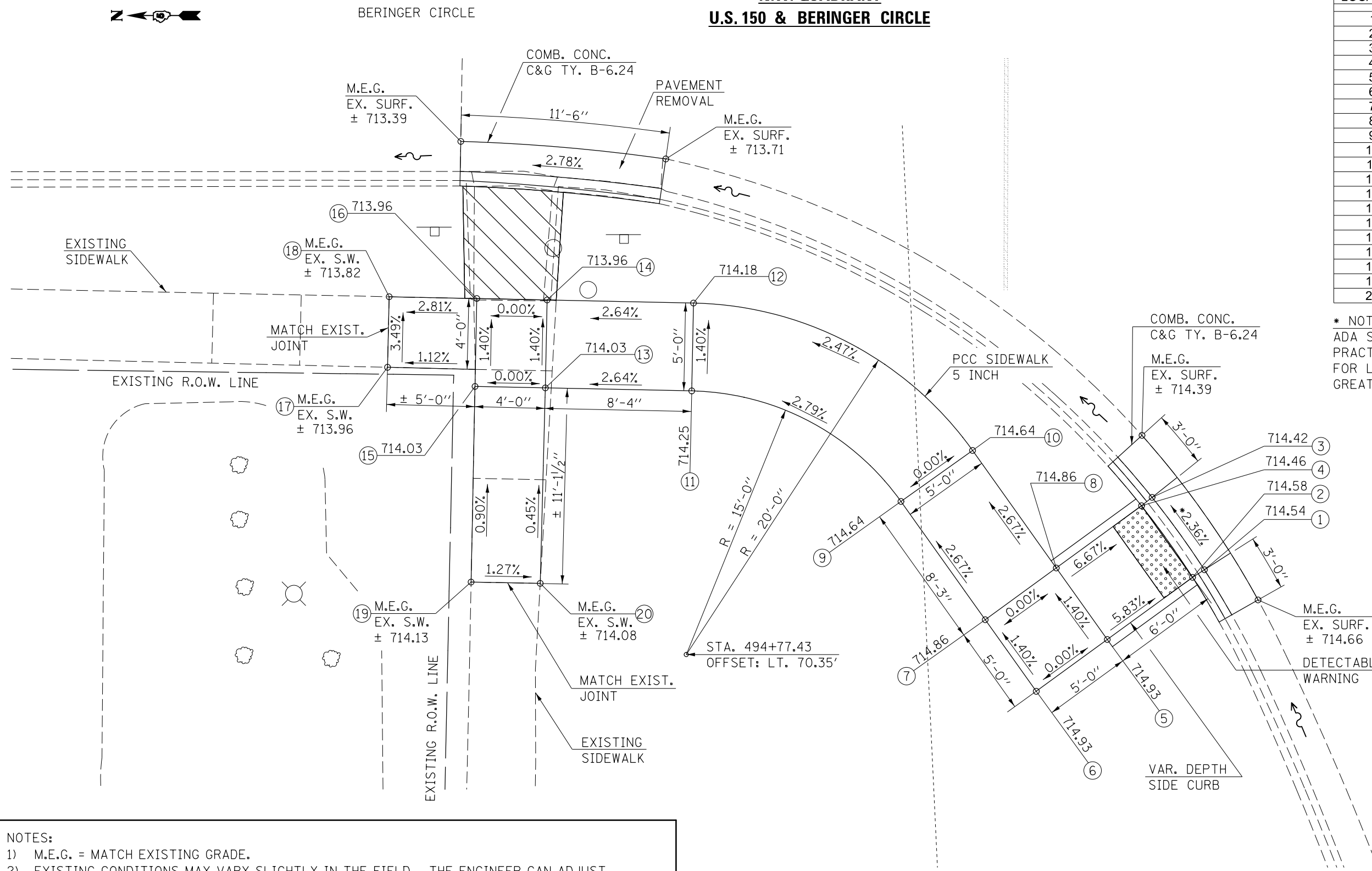
LOCATION #29

N.W. QUADRANT U.S. 150 & BERINGER CIRCLE

LOCATION #29 - N.W. QUADRANT
U.S. RTE. 150 & BERINGER CIRCLE

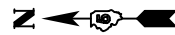
LOCATION	STATION	OFFSET
1	494+83.00	LT. 40.99
2	494+82.55	LT. 41.66
3	494+87.09	LT. 44.06
4	494+86.59	LT. 44.65
5	494+78.87	LT. 46.42
6	494+75.79	LT. 50.39
7	494+79.84	LT. 53.39
8	494+82.91	LT. 49.42
9	494+86.54	LT. 58.33
10	494+89.57	LT. 54.33
11	494+92.67	LT. 70.37
12	494+97.75	LT. 70.37
13	494+92.67	LT. 78.70
14	494+97.76	LT. 78.70
15	494+92.67	LT. 82.70
16	494+97.76	LT. 82.70
17	494+93.68	LT. 87.70
18	494+97.77	LT. 87.69
19	494+81.33	LT. 82.68
20	494+81.34	LT. 78.75

* NOTE:
ADA STATEMENT OF MAXIMUM EXTENT PRACTICABLE (MEP) HAS BEEN APPROVED FOR LOWER LANDING CROSS SLOPE GREATER THAN 2.0%.



- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE < 7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.

 EXISTING SIDEWALK TO BE REMOVED (NO REPLACEMENT AT THIS LOCATION)

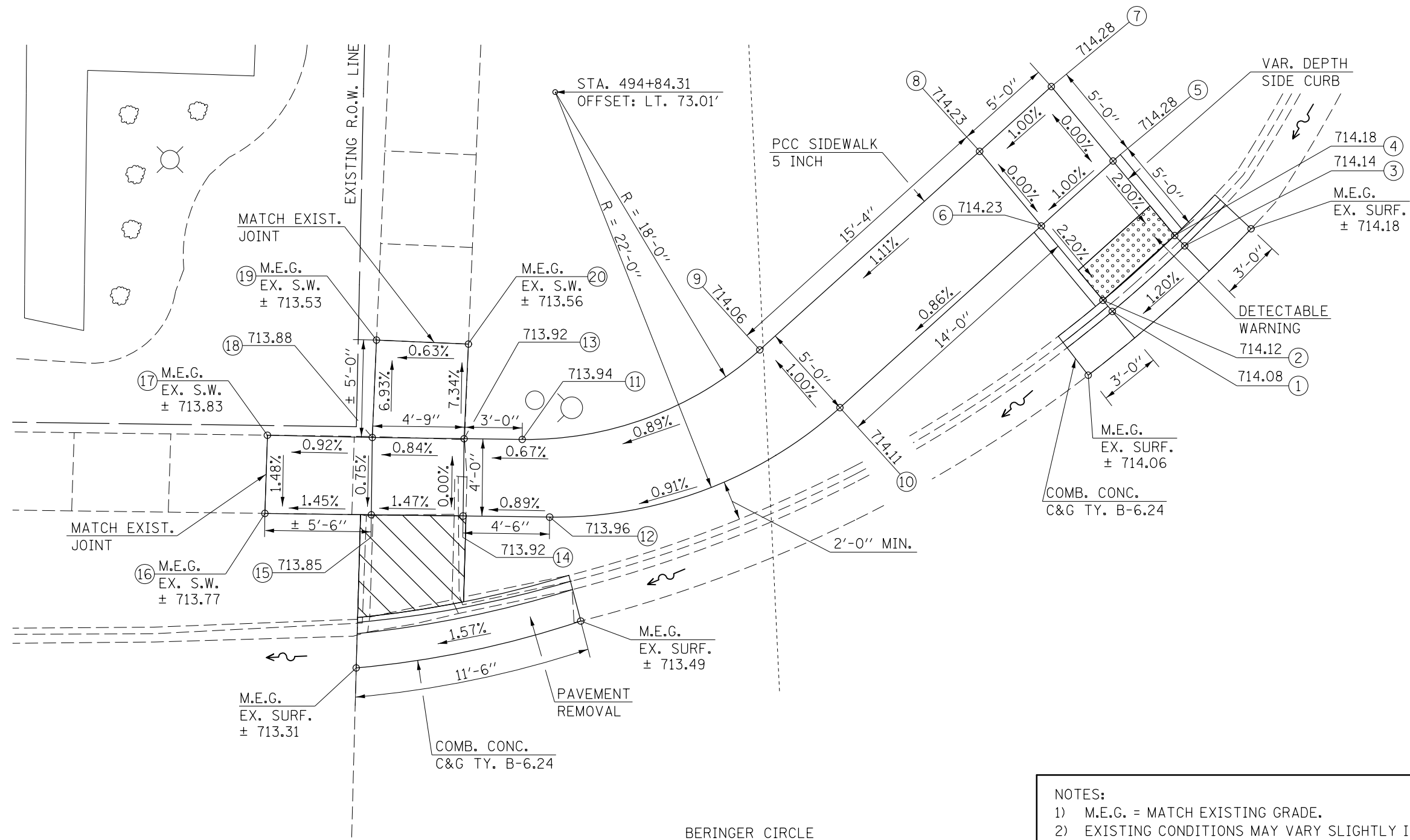


LOCATION #30

N.E. QUADRANT
U.S. 150 & BERINGER CIRCLE

LOCATION #30 - N.E. QUADRANT
U.S. RTE. 150 & BERINGER CIRCLE

LOCATION	STATION	OFFSET
1	495+72.85	LT. 44.16
2	495+73.43	LT. 44.64
3	495+76.28	LT. 40.42
4	495+76.82	LT. 40.93
5	495+80.71	LT. 44.13
6	495+77.32	LT. 47.84
7	495+84.60	LT. 47.33
8	495+81.21	LT. 51.04
9	495+70.78	LT. 62.40
10	495+67.77	LT. 58.24
11	495+66.03	LT. 74.68
12	495+61.95	LT. 73.25
13	495+66.06	LT. 77.70
14	495+61.99	LT. 77.73
15	495+62.03	LT. 82.48
16	495+62.07	LT. 87.99
17	495+66.20	LT. 87.88
18	495+66.10	LT. 82.45
19	495+71.24	LT. 82.24
20	495+71.05	LT. 77.48



U.S. RTE. 150

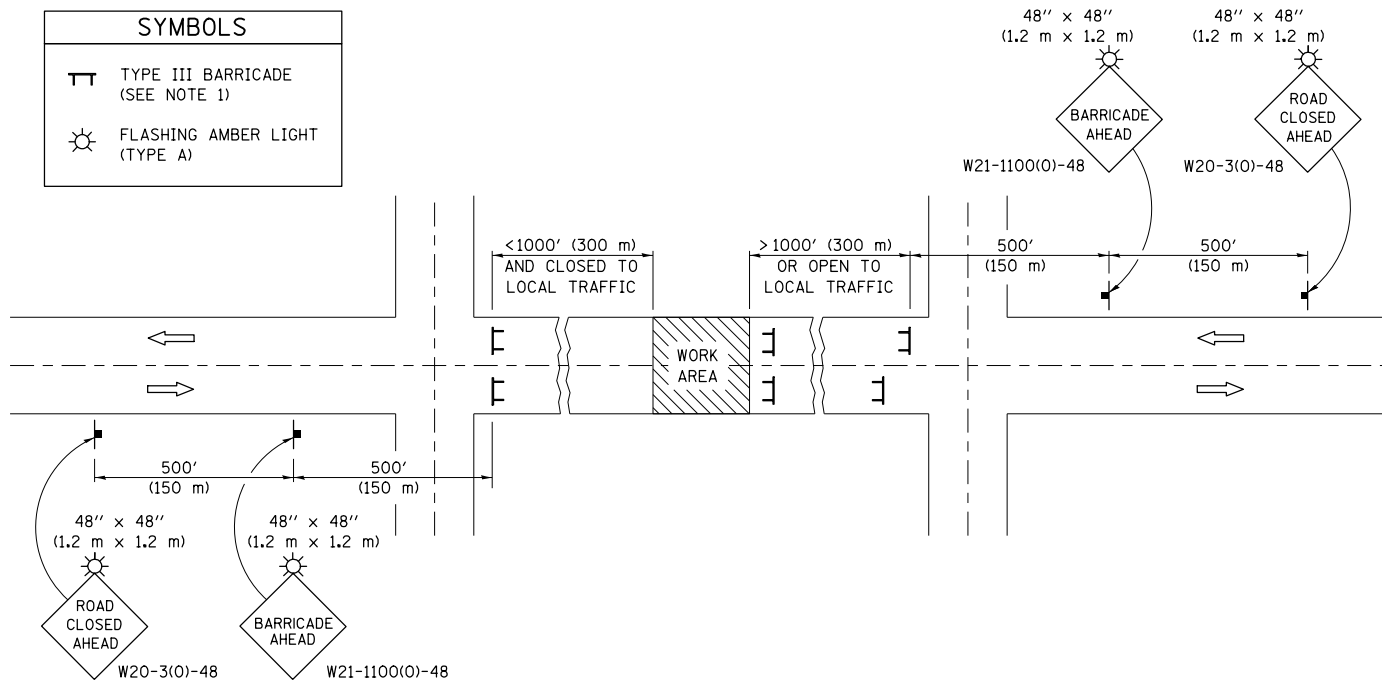
EXISTING SIDEWALK TO BE REMOVED
(NO REPLACEMENT AT THIS LOCATION)

- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE < 7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE < 1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE < 1.5% PREFERRED.

ROAD CLOSURE

SIDEROAD / STREET CLOSURE

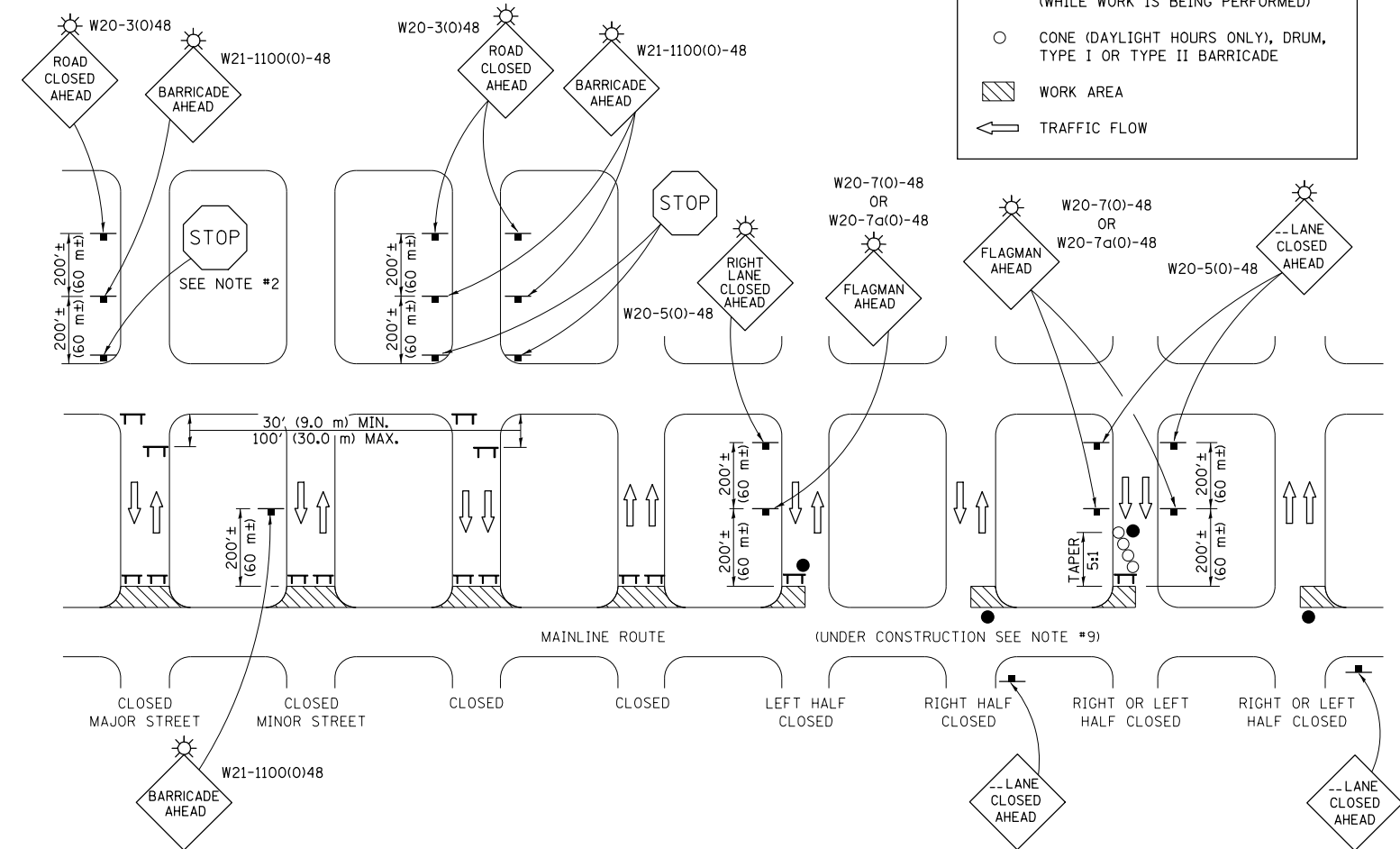
SYMBOLS	
	TYPE III BARRICADE (SEE NOTE 1)
	FLASHING AMBER LIGHT (TYPE A)



GENERAL NOTES

- TYPE III BARRICADES SHALL BE AS SHOWN ON STANDARD 701901 "TYPICAL APPLICATIONS OF TYPE III BARRICADES CLOSING A ROAD". EACH TYPE III BARRICADE SHALL HAVE TWO FLASHING AMBER LIGHTS MOUNTED ABOVE IT.
- IF THE ROAD IS OPEN TO LOCAL TRAFFIC OR EXCEEDS 1000' (300 m), ANOTHER SET OF TYPE III BARRICADES, EQUIPPED AS IN NOTE 1 ABOVE, SHALL BE PLACED AT EACH END OF THE WORK AREA.
- WHEN A STOP CONDITION EXISTS, NO SIGNS ARE REQUIRED IN ADVANCE OF THE "STOP" SIGN WHEN THE ROAD IS CLOSED WITHIN 100' (30 m) OF THE INTERSECTION.
- STANDARD 701901 SHALL APPLY FOR THE PLACEMENT & DESIGN OF TYPE III BARRICADES.
- IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON AN NCHRP 350 TEMPORARY SIGN SUPPORT DIRECTLY IN FRONT OF THE BARRICADE.
- REFLECTORIZED STRIPING SHALL APPEAR ON BOTH SIDES OF THE TYPE III BARRICADES IF ROAD IS OPEN TO LOCAL TRAFFIC.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- A MINIMUM OF TWO FLASHING LIGHTS SHALL BE USED AT NIGHT ON EACH APPROACH IN ADVANCE OF THE WORK AREA. FLASHING LIGHTS SHALL BE INSTALLED ABOVE THE FIRST TWO SIGNS IN THE SERIES.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS.
- FORMS BT. 725 AND BT. 726 ARE REQUIRED.
- WHEN A SIDEROAD INTERSECTS THE HIGHWAY ON WHICH WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC DEVICES SHALL BE ERECTED AND PROVIDED AS DIRECTED BY THE ENGINEER.
- AN ADDITIONAL SIGN MAY BE REQUIRED AT A MAJOR INTERSECTING ROAD IN ADVANCE OF THE CLOSURE. THE ADDITIONAL SIGN SHALL GIVE THE DISTANCE TO THE BARRICADE IN MILES OR FRACTIONS OF A MILE.

SYMBOLS	
	TYPE III BARRICADE (SEE NOTE)
	FLASHING LIGHT
	FLAGGER WITH TRAFFIC CONTROL SIGN (WHILE WORK IS BEING PERFORMED)
	CONES (DAYLIGHT HOURS ONLY), DRUM, TYPE I OR TYPE II BARRICADE
	WORK AREA
	TRAFFIC FLOW



GENERAL NOTES

- TYPE III BARRICADES SHALL BE AS SHOWN ON "TYPICAL APPLICATIONS OF TYPE III BARRICADES CLOSING A ROAD". EACH TYPE III BARRICADE SHALL HAVE TWO FLASHING AMBER LIGHTS MOUNTED ABOVE IT.
- WHERE A STOP CONDITION EXISTS, AS SHOWN ABOVE, WARNING SIGNS MAY BE OMITTED IN ADVANCE OF THE "STOP" SIGN.
- STANDARD 701901 SHALL APPLY FOR THE PLACEMENT & MANUFACTURE OF TYPE III BARRICADES.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ONE FLASHING LIGHT IS REQUIRED ABOVE EACH ADVANCE WARNING SIGN DURING HOURS OF DARKNESS.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS.
- FORMS BT 725 AND BT 726 ARE REQUIRED.
- THE MAINLINE ROUTE TEMPORARY TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE PLANS, SPECIAL PROVISIONS AND STANDARD SPECIFICATIONS.
- ALL FLAGGERS REQUIRED AT SIDE ROADS AND ENTRANCES REMAINING OPEN TO TRAFFIC AND/OR ADDITIONAL BARRICADES REQUIRED BY THE ENGINEER TO CLOSE SIDE ROADS AND ENTRANCES WILL BE PAID FOR ACCORDING TO ARTICLE 109.04.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED - 11/06
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		DATE -	REVISED - 09/09 - KJT
			REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL & PROTECTION DEVICES
(ROAD & SIDEROAD/STREET CLOSURES)

SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA.

DISTRICT 5 DETAIL NO. 7020000

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 ADA-2	CHAMPAIGN	44	44
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 70A47	