

CONSTRUCTION PLANS FOR VERMILION REGIONAL AIRPORT AUTHORITY

VERMILION REGIONAL AIRPORT DANVILLE, ILLINOIS

IL. PROJ. NO. DNV-4445; IL. CONTRACT NO. VE052

FED PROJ. NO. 3-17-SBGP-XX

REHABILITATE T-HANGAR TAXIWAYS AND ASSOCIATED ACCESS ROAD IMPROVEMENTS

MAY 6, 2016

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| SUMMARY OF QUANTITIES | | | |
|-----------------------|----------------------------------|----------|------|
| ITEM | DESCRIPTION | QUANTITY | UNIT |
| AR150510 | ENGINEER'S FIELD OFFICE | 1 | LS |
| AR152410 | UNCLASSIFIED EXCAVATION | 570 | CY |
| AR152480 | SHOULDER ADJUSTMENT | 600 | SY |
| AR152511 | SUBGRADE REPAIR | 1,532 | SY |
| AR162570 | DETECTOR LOOP | 1 | LS |
| AR209600 | GEOTEXTILE FABRIC | 1,532 | SY |
| AR209604 | CRUSHED AGG. BASE COURSE - 4" | 715 | SY |
| AR209606 | CRUSHED AGG. BASE COURSE - 6" | 3,800 | SY |
| AR401610 | BITUMINOUS SURFACE COURSE | 2,040 | TON |
| AR401650 | BITUMINOUS PAVEMENT MILLING | 12,800 | SY |
| AR403610 | BITUMINOUS BASE COURSE | 350 | TON |
| AR501905 | REMOVE PAVEMENT | 2,500 | SY |
| AR501911 | REMOVE AND REPLACE PCC PAVEMENT | 25 | SF |
| AR602510 | BITUMINOUS PRIME COAT | 5,900 | GAL |
| AR603510 | BITUMINOUS TACK COAT | 1,190 | GAL |
| AR620520 | PAVEMENT MARKING - WATERBORNE | 2,080 | SF |
| AR625511 | SEAL COAT | 400 | SY |
| AR705524 | 4" PERFORATED UNDERDRAIN W/ SOCK | 285 | LF |
| AR705640 | UNDERDRAIN CLEANOUT | 1 | EACH |
| AR705645 | UNDERDRAIN CONNECTION | 1 | EACH |
| AR751940 | ADJUST INLET | 2 | EACH |
| AR901510 | SEEDING | 0.9 | AC |
| AR908525 | KNITTED STRAW MAT | 600 | SY |

811 Know what's below. Call before you dig. COMMON GROUND ALLIANCE www.call811.com or Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

DESIGN INFORMATION

GEOMETRIC CRITERIA

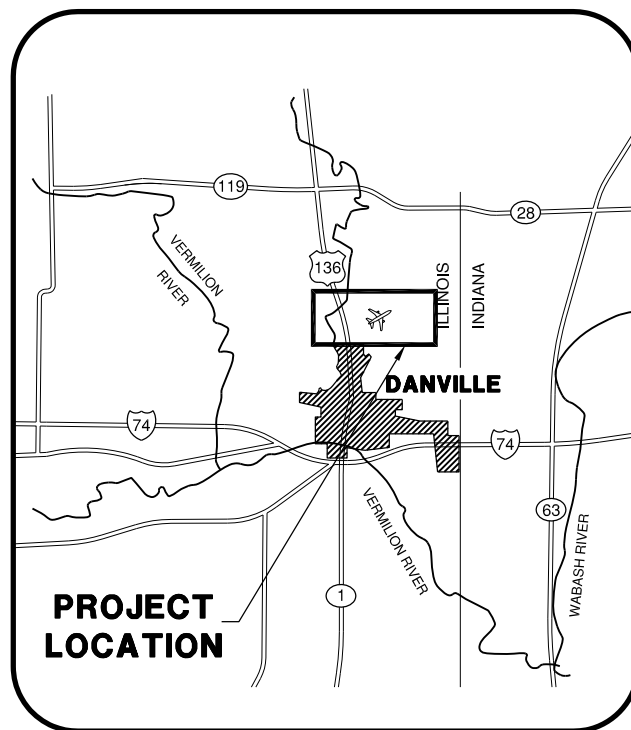
AIRPLANE DESIGN GROUP I
AIRCRAFT APPROACH CATEGORY B

PAVEMENT DESIGN CRITERIA

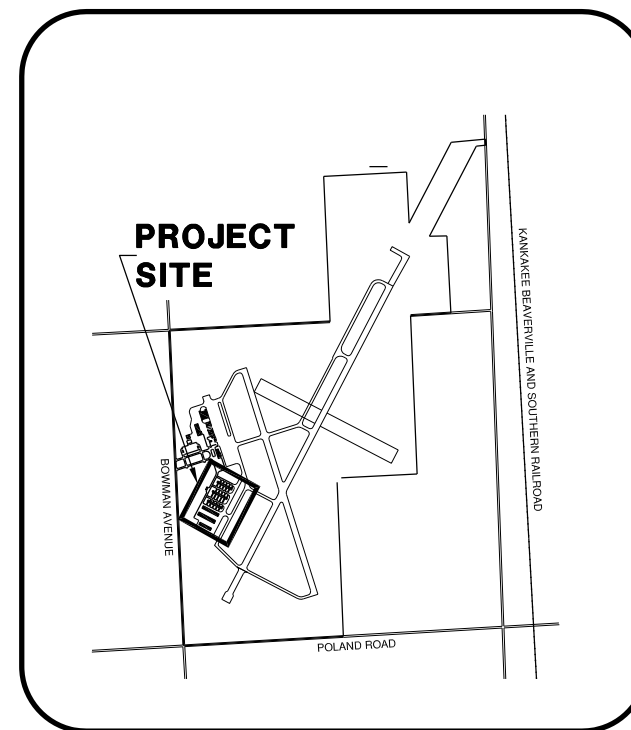
DEPARTURE WEIGHT = 11,500 LBS.
SINGLE WHEEL GEAR

CALL J.U.L.I.E.
BEFORE EXCAVATING
1-800-892-0123

TOWNSHIP: 20 NORTH
RANGE: 11 WEST
SECTION: 15
COUNTY: VERMILION
CIVIL TOWNSHIP: NEWELL



LOCATION MAP



SITE PLAN



Chris B. Groth
Exp. 11-30-2017

GROUND CONTROL RADIO FREQUENCY - 121.8
ATIS FREQUENCY - 124.85
APPROXIMATE MAXIMUM HEIGHT OF EQUIPMENT
ABOVE GROUND IS 25 FT.

VERMILION REGIONAL AIRPORT AUTHORITY
VERMILION REGIONAL AIRPORT

APPROVED: *Michael J. Poff*
DATE: 5-2-2016 2016

CMT
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SUBMITTED BY: *Chris B. Groth*
DATE: May 3, 2016

CMT JOB NUMBER: 15042-02-00

CRITICAL POINT TABLE

| POINT NUMBER | LATITUDE | LONGITUDE | GROUND ELEVATION | OBSTRUCTION HEIGHT | ABOVE GROUND ELEVATION |
|--------------|-----------------|-----------------|------------------|--------------------|------------------------|
| 1 | N40° 11' 57.85" | W87° 36' 08.45" | 651.000 | 25' | 676.000 |
| 2 | N40° 11' 59.80" | W87° 36' 06.73" | 658.000 | 25' | 683.000 |
| 3 | N40° 11' 52.52" | W87° 35' 58.44" | 660.361 | 25' | 685.361 |
| 4 | N40° 11' 46.71" | W87° 36' 01.71" | 656.408 | 25' | 681.408 |
| 5 | N40° 11' 44.62" | W87° 36' 03.24" | 654.699 | 25' | 679.699 |
| 6 | N40° 11' 45.33" | W87° 36' 07.96" | 657.271 | 25' | 682.271 |
| 7 | N40° 11' 48.14" | W87° 36' 08.11" | 656.560 | 25' | 681.560 |
| 8 | N40° 11' 53.64" | W87° 36' 03.49" | 658.356 | 25' | 683.356 |
| 9 | N40° 11' 55.48" | W87° 36' 02.72" | 657.000 | 25' | 682.000 |

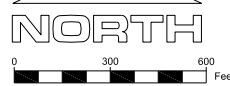
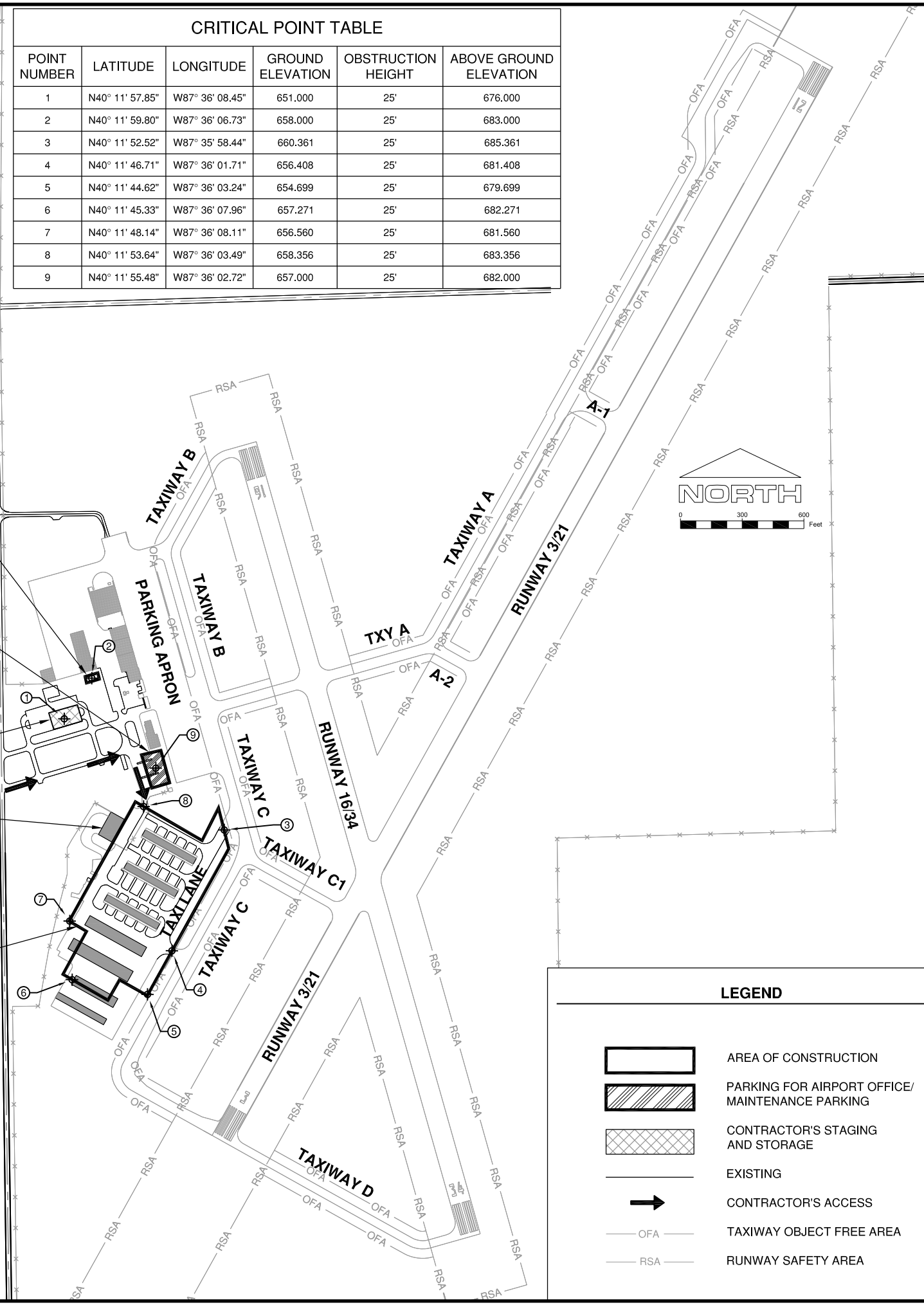
ENGINEER'S FIELD OFFICE (AIRPORT BUILDING)

AIRPORT PARKING

CONTRACTOR'S STAGING, STORAGE, AND PARKING AREA

AIRPORT OFFICE/ MAINTENANCE BUILDING

AREA OF CONSTRUCTION



LEGEND

- AREA OF CONSTRUCTION
- PARKING FOR AIRPORT OFFICE/ MAINTENANCE PARKING
- CONTRACTOR'S STAGING AND STORAGE
- EXISTING
- CONTRACTOR'S ACCESS
- TAXIWAY OBJECT FREE AREA
- RUNWAY SAFETY AREA

UNICOM FREQUENCY - 122.70

GENERAL NOTES

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN INDEX SHEET. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION. AT NO POINT SHALL EQUIPMENT OR MATERIALS BE ALLOWED TO BE STORED ON RUNWAYS.
- MEN, VEHICLES AND EQUIPMENT WILL NOT BE ALLOWED WITHIN THE FOLLOWING AREAS:
 - 250' FROM THE CENTERLINE OF RUNWAY 16/34 WHEN ACTIVE.
 - 250' FROM THE CENTERLINE OF RUNWAY 3/21 WHEN ACTIVE.
 - 93' FROM THE CENTERLINE OF AN ACTIVE TAXIWAY.
- ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING A TAXIWAY OR RUNWAY OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY AN ESCORT OR A FLAGMAN IN RADIO CONTACT WITH AIR AND GROUND TRAFFIC AT ALL TIMES. CONTRACTOR SHALL PROVIDE THE FLAGMAN AND RADIO EQUIPMENT UNDER THE MOBILIZATION PAY ITEM.
- ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR AT THEIR EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTINUOUSLY CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY THE CONTRACTOR'S VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.
- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE THEIR OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THEIR OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - THE CONTRACTOR SHALL MAINTAIN A COMPLETE LIST OF PERSONNEL AUTHORIZED BY THE CONTRACTOR TO WORK ON THE AIRFIELD. THIS LIST SHALL BE PROVIDED TO THE AIRPORT AND RESIDENT ENGINEER BY THE CONTRACTOR UPON REQUEST.
 - THE CONTRACTOR SHALL USE AN EXISTING GATE(S) FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. THE CONTRACTOR SHALL PROVIDE KEYS FOR HIS PADLOCK TO THE RESIDENT ENGINEER AND VRAA MAINTENANCE SUPERVISOR. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE AIRPORT.
 - CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS.
 - THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.

NOTE:

- THE BARRICADES, TAXIWAY CLOSURE TEMPORARY MARKING, AND SUBSEQUENT REMOVAL OF TEMPORARY MARKING UPON PROJECT COMPLETION SHALL BE INCIDENTAL TO THE MOBILIZATION PAY ITEM.



License No. 184-000613
CONSULTANTS

100% SUBMITTAL
MAY 6, 2016

**REHABILITATE T-HANGAR
TAXIWAYS AND ASSOCIATED
ACCESS ROAD IMPROVEMENTS**

OWNER



**VERMILION REGIONAL AIRPORT
VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, ILLINOIS**

| MARK | DATE | DESCRIPTION |
|------|------|-------------|
| | | |
| | | |
| | | |

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| FED PROJ. NO. 3-17-SBGP-XX |
| IL PROJ. NO. DNV-4445; IL CONTRACT NO. VE052 |
| CMT PROJECT NO: 15042-02-00 |
| CAD DWG FILE: DNV4445-1504202-GH101.DWG |
| DESIGNED BY: NRF |
| DRAWN BY: DPA |
| CHECKED BY: JEF |
| APPROVED BY: CBG |
| COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |

**SHEET TITLE
AIRPORT SITE PLAN**

GH101

Path: K:\Danville\1504202\Draw\Sheets\DNV4445-1504202-GH101.dwg
Date: Tuesday, May 10, 2016 8:22:14 PM

GENERAL NOTES

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLANS OR AS DIRECTED BY THE AIRPORT MANAGER.
- ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING ACTIVE RUNWAYS, TAXIWAYS AND APRONS SHALL BE UNDER CONTROL OF A FLAGGER IN RADIO CONTACT WITH AIR & GROUND TRAFFIC & UNICOM AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS & FLAGGING PERSONNEL.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CLOSING AND OPENING PAVEMENTS AND CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS AT THE LOCATION SHOWN FOR THE CONTRACTOR'S STAGING, STORAGE, PARKING SITE.
- BROKEN OR WASTE CONCRETE AND ASPHALT IN EXCESS OF WHAT IS TO BE INCORPORATED INTO THE PROJECT SHALL BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY, UNLESS DIRECTED BY THE AIRPORT MANAGER.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN AREAS 93' (ADG IV - OFA) FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
- ALL PAVEMENTS, DRIVES AND OTHER AREAS USED BY THE CONTRACTOR FOR HAUL ROADS AND STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE AIRPORT MANAGER. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- EXISTING TURF & AGRICULTURAL AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS OUTSIDE OF THE TURFING LIMITS SHALL BE COMPLETELY RESTORED BY THE CONTRACTOR AT THEIR EXPENSE TO THE SATISFACTION OF THE AIRPORT MANAGER. DAMAGE TO EXISTING CROPS ADJACENT TO THE WORK AREA SHALL BE QUANTIFIED BY THE AIRPORT AND COST TO COMPENSATE THE FARMER REIMBURSED BY THE CONTRACTOR AT (\$2500/AC).
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS PRIOR TO OPENING TO AIR TRAFFIC.
- REFER TO THE CONSTRUCTION ACTIVITY PLANS AND THE SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS CONCERNING COORDINATION OF CONSTRUCTION ACTIVITIES.
- THE AIRFIELD RESCUE AND FIREFIGHTING VEHICLES SHALL HAVE COMPLETE ACCESS TO THE ENTIRE AIRFIELD INCLUDING THE CLOSURE AREAS.
- THE CONTRACTOR IS REQUIRED TO GIVE TEN (10) FULL WORKING DAYS NOTICE TO THE AIRPORT MANAGER PRIOR TO CLOSING WORK AREAS TO AIRCRAFT.
- AT THE PRECONSTRUCTION MEETING, CONTRACTOR SHALL SUPPLY THE AIRPORT MANAGER WITH PROPOSED CLOSURE AND PHASING DATES FOR THEIR REVIEW AND APPROVAL. THE RESIDENT ENGINEER SHALL KEEP THE AIRPORT MANAGER ADVISED OF ANY PROPOSED CHANGES IN CLOSURE AND PHASING DATES.
- ALL CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL DISPLAY AN ORANGE AND WHITE CHECKED AVIATION SIGNAL FLAG, EXCEPT HAUL VEHICLES.
- ANY VEHICLE OPERATING WITHIN A MOVEMENT AREA DURING THE HOURS OF DARKNESS SHOULD BE EQUIPPED WITH AN AMBER REVOLVING OR FLASHING DOME-TYPE LIGHT AS SPECIFIED IN THE SPECIAL PROVISIONS.
- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- IF DURING THE CONSTRUCTION OPERATIONS NOTICEABLE DESTRUCTION TO THE SUBBASE OCCURS AFTER MILLING, CORRECTIVE ACTION SHALL BE TAKEN TO MITIGATE THE ISSUE. CONTRACTOR MAY BE REQUIRED TO REDUCE FULLY LOADED DELIVERY TRUCKS TO LIMIT DAMAGE PER SECTION 50-12.

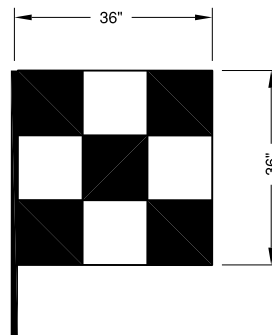
CONTRACTOR'S RESPONSIBILITY FOR UTILITY SERVICE

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDER- GROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

CONTRACTOR'S ACCESS

- CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - THE CONTRACTOR'S ACCESS TO THE WORK SHALL BE AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLANS.
 - DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK SITE AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
 - THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR THIS PADLOCK TO THE RESIDENT ENGINEER, AIRPORT SECURITY, AND MAINTENANCE SUPERVISOR. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE AIRPORT MANAGER.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED & SECURED AT ALL TIMES INCLUDING WORK HOURS. IF THE CONTRACTOR CHOOSES TO LEAVE THE GATE OPEN, HE SHALL POST A COMPETENT, FULL TIME SECURITY GUARD TO PREVENT UNAUTHORIZED ENTRIES. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS IF SO DIRECTED BY THE AIRPORT MANAGER OR ENGINEER.
 - THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATES UPON LEAVING THE SITE.
 - THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGE TO THE ACCESS GATES OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE AIRPORT MANAGER.
 - ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS TO NOT DISTURB AGRICULTURAL AREAS OR VIOLATE THE PART 77 APPROACH AND PRIMARY SURFACES.
 - EMPLOYEE PERSONAL VEHICLES SHALL NOT BE ALLOWED BEYOND THE CONTRACTOR'S PARKING AREA. CONTRACTOR PERSONNEL SHALL PARK IN THE CONTRACTOR'S STAGING & STORAGE CONSISTENT AREA. PERSONNEL SHALL BE TRANSPORTED TO THE WORK SITE BY COMPANY OWNED VEHICLES.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL ACCESS ROADS WITH THE APPROPRIATE LOCAL AGENCY RESPONSIBLE FOR THE ROADWAY.
 - THE CONTRACTOR SHALL HAVE A VACUUM TYPE SWEEPER AVAILABLE AT ALL TIMES.
- AN AREA WILL BE PROVIDED BY THE AIRPORT TO THE CONTRACTOR AS THE STAGING, STORAGE AND EMPLOYEE PARKING SITE, AT THE LOCATION SHOWN.
- A LIST OF AUTHORIZED PERSONNEL PERMITTED TO USE THE GATE SHALL BE PROVIDED BY THE CONTRACTOR TO THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL & CONSTRUCTION ACCESS ONLY".
- SUPERINTENDENT, SUPERVISING FOREMEN, ESCORT PERSONNEL AND SECURITY GUARDS ON THE AIRFIELD WILL BE REQUIRED TO SUBMIT A TEN YEAR BACKGROUND CHECK TO AIRPORT SECURITY AND WILL BE REQUIRED TO BE TRAINED BY THE AIRPORT TO GAIN ACCESS PER SECTION 80-13 OF THE SPECIFICATIONS. CONTRACTOR SHALL COORDINATE THE BADGING PROCESS A MINIMUM OF 10 DAYS PRIOR TO REQUESTING ACCESS TO THE FIELD.



VEHICLE SIGNAL FLAG
(ORANGE / WHITE)
N.T.S.

GROUND CONTROL FREQUENCY 122.70 MHZ

MAXIMUM EQUIPMENT HEIGHT 25 FEET



License No. 184-000613
CONSULTANTS

100% SUBMITTAL
MAY 6, 2016

REHABILITATE T-HANGAR TAXIWAYS AND ASSOCIATED ACCESS ROAD IMPROVEMENTS

OWNER



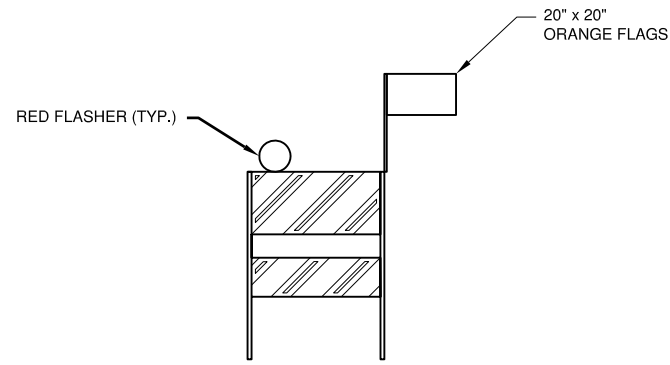
VERMILION REGIONAL AIRPORT
VERMILION REGIONAL AIRPORT AUTHORITY
DANVILLE, ILLINOIS

| MARK | DATE | DESCRIPTION |
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| FED PROJ. NO. 3-17-SBGP-XX |
| IL PROJ. NO. DNV-4445; IL CONTRACT NO. VE052 |
| CMT PROJECT NO: 15042-02-00 |
| CAD DWG FILE: DNV4445-1504202-GI501.DWG |
| DESIGNED BY: NRF |
| DRAWN BY: DPA |
| CHECKED BY: JEF |
| APPROVED BY: CBG |
| COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |

SHEET TITLE
GENERAL NOTES

GI501



FLASHER BARRICADE DETAIL-IDOT TYPE 1

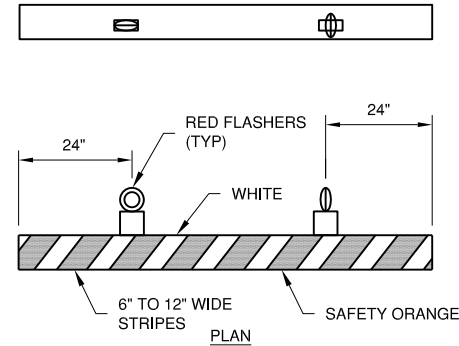
N.T.S.

FLASHER BARRICADE NOTES

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 15' INTERVALS.

GENERAL NOTES

1. ALL CONSTRUCTION BARRICADES SHALL BE IN PLACE PRIOR TO INITIATING WORK.
2. THE CONTRACTOR SHALL GIVE THE AIRPORT A MINIMUM 14 DAYS NOTIFICATION PRIOR TO THE INITIATION OF THE WORK.
3. THE CONTRACTOR SHALL MAINTAIN ALL MINIMUM DISTANCES SHOWN IN THE CONSTRUCTION ACTIVITY PLANS OR AS REQUIRED FOR THE CRITICAL OPERATING AIRCRAFT THROUGH THESE AREAS.
4. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THE CONSTRUCTION SET BACK LINE SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND WILL NOT BE MEASURED FOR PAYMENT.
5. ALL PAVEMENTS OR TURF AREAS UTILIZED BY THE CONTRACTOR FOR AN ACCESS ROUTE, STAGING, OR STORAGE SHALL BE REPAIRED AND RESTORED TO THEIR ORIGINAL CONDITION TO THE SATISFACTION OF THE AIRPORT. NO ADDITIONAL COMPENSATION TO PROTECT, REPAIR, OR RESTORE THESE AREAS SHALL BE MADE.

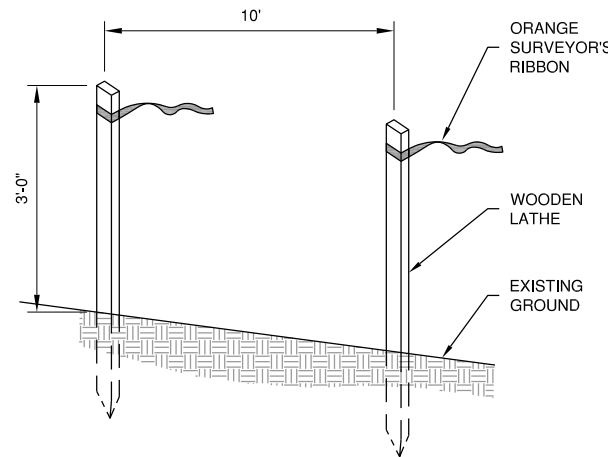


LOW PROFILE LIGHTED BARRICADE

NTS

BARRICADE NOTES

1. FLASHERS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES ARE TO BE PLACED WITH A MAXIMUM OF 15' SPACING BETWEEN ENDS OF BARRICADES ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION OR AS DIRECTED BY THE RESIDENT ENGINEER. ROTATE EVERY OTHER FLASHER LENS 90°.
4. FLASHERS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS, AND WEIGHTED TO THE SURFACE.



CONSTRUCTION SETBACK LINE DETAIL

N.T.S.

CONSTRUCTION SETBACK LINE NOTES

1. CONSTRUCTION SETBACK LINE SHALL BE INSTALLED WHERE SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS.
2. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

SEQUENCE OF CONSTRUCTION NOTES

THE GENERAL PROGRESSION OF THE WORK SHALL BE AS FOLLOWS:

- A. SUBMIT EQUIPMENT AND BUILDING SHOP, PLAN AND WORKING DRAWINGS FOR REVIEW. INCLUDE WITH THE SUBMITTALS ALL BUY AMERICAN CERTIFICATIONS FOR ALL MATERIALS.
- B. SUBMIT NOTICE OF OBSTRUCTION EVALUATION- AIRPORT AIRSPACE ANALYSIS (OE/AAA) INFORMATION FOR ANTICIPATED EQUIPMENT HEIGHTS IF IN EXCESS OF 25'. NOTE THAT THIS PROCESS MAY REQUIRE UP TO 90 DAYS FOR FAA APPROVAL. EQUIPMENT ABOVE 25' HEIGHT SHALL NOT BE UTILIZED UNTIL FAA APPROVAL HAS BEEN PROVIDED.
- C. SUBMIT PROJECT SCHEDULE SHOWING RELATIONSHIP BETWEEN CONSTRUCTION DURATION FOR PAY ITEMS IN RELATION TO THE PHASES OF WORK WHERE THEY ARE BEING PERFORMED. CLEARLY IDENTIFY DATES OF TAXIWAY CLOSURES AND WHAT PHASES WILL BE WORKED IN DURING THAT CLOSURE.
- D. SUBMIT PRELIMINARY MATERIALS CERTIFICATIONS INCLUDING BUY AMERICAN CERTIFICATIONS AND WAIVER REQUEST FOR MATERIALS THAT DO NOT MEET THE CONTRACT REQUIREMENTS.
- E. INSTALL BARRICADES AS OUTLINED ON THE CONSTRUCTION ACTIVITY PLANS. INITIATE DEMOLITION AND REMOVAL OF EXISTING PAVEMENTS. FIELD-VERIFY LOCATION OF EXISTING CIRCUITS, AND PERFORM TESTING ON EXISTING AIRFIELD CIRCUITS TO VERIFY CONDITION OF CIRCUIT CABLES. THE R.E. SHALL BE PRESENT AT THE TIME OF TESTING AND SHALL BE GIVEN A COPY OF THE TEST RESULTS.
- F. UPON COMPLETION OF ALL PHASES, THE CONTRACTOR SHALL REQUEST A FINAL INSPECTION OF THE PROJECT.

APRON / TAXIWAY OBJECT FREE AREAS

1. WORK IN THE TAXIWAY OBJECT FREE AREAS SHALL REQUIRE THAT TAXIWAY TO BE CLOSED. WORK WITHIN THE TAXIWAY OBJECT FREE AREAS AT THE HARD SURFACE WILL INCLUDE PAVEMENT MARKING AND PAVEMENT MARKING REMOVAL, UNDERDRAIN INSTALLATION AND LANDSCAPING OPERATIONS.
2. NO EQUIPMENT, OPEN TRENCHES OR EXCAVATIONS SHALL REMAIN INSIDE THE TAXIWAY OBJECT FREE AREAS AFTER WORKING HOURS.
3. THE TAXIWAYS SHALL BE CLOSED WITH BARRICADES AT 15' MAXIMUM SPACING PRIOR TO WORKING IN THE CRITICAL WORK AREAS.

RUNWAY SAFETY AREAS

1. EQUIPMENT OR PERSONNEL SHALL REMAIN CLEAR OF THE RUNWAY PAVEMENTS AT ALL TIMES UNLESS INSTRUCTED BY A PERSON IN RADIO CONTACT WITH THE AIR GROUND, & UNICOM.
2. NO EQUIPMENT, STOCKPILES OR EXCAVATIONS SHALL REMAIN INSIDE THE RUNWAY SAFETY AREAS AFTER WORKING HOURS.



License No. 184-000613

CONSULTANTS

100% SUBMITTAL
MAY 6, 2016

REHABILITATE T-HANGAR
TAXIWAYS AND ASSOCIATED
ACCESS ROAD IMPROVEMENTS

OWNER



VERMILION REGIONAL AIRPORT
VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, ILLINOIS

| MARK | DATE | DESCRIPTION |
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FED PROJ. NO. 3-17-SBGP-XX

IL PROJ. NO. DNV-4445; IL CONTRACT NO. VE052

CMT PROJECT NO: 15042-02-00

CAD DWG FILE: DNV4445-1504202-GC001.DWG

DESIGNED BY: NRF

DRAWN BY: DPA

CHECKED BY: JEF

APPROVED BY: CBG

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SHEET TITLE

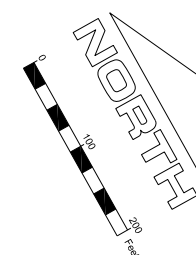
CONSTRUCTION
ACTIVITY NOTES &
DETAILS

GC001

SHEET 4 OF 26

CRITICAL POINT TABLE

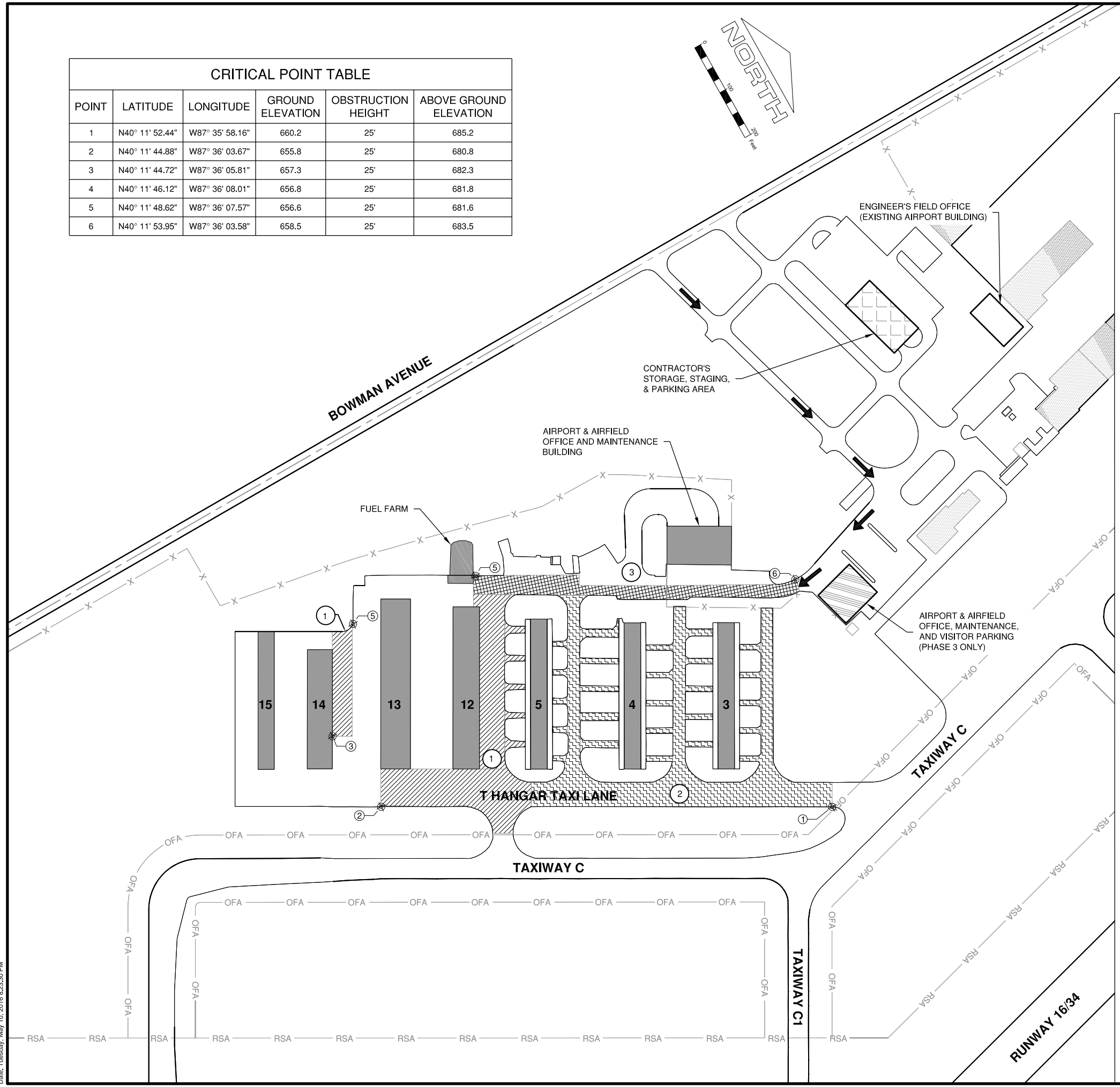
| POINT | LATITUDE | LONGITUDE | GROUND ELEVATION | OBSTRUCTION HEIGHT | ABOVE GROUND ELEVATION |
|-------|-----------------|-----------------|------------------|--------------------|------------------------|
| 1 | N40° 11' 52.44" | W87° 35' 58.16" | 660.2 | 25' | 685.2 |
| 2 | N40° 11' 44.88" | W87° 36' 03.67" | 655.8 | 25' | 680.8 |
| 3 | N40° 11' 44.72" | W87° 36' 05.81" | 657.3 | 25' | 682.3 |
| 4 | N40° 11' 46.12" | W87° 36' 08.01" | 656.8 | 25' | 681.8 |
| 5 | N40° 11' 48.62" | W87° 36' 07.57" | 656.6 | 25' | 681.6 |
| 6 | N40° 11' 53.95" | W87° 36' 03.58" | 658.5 | 25' | 683.5 |



LEGEND

- AIRPORT BUILDINGS AND HANGARS
- PHASE #1
- PHASE #2
- PHASE #3
- AIRPORT & AIRFIELD OFFICE, MAINTENANCE, AND VISITOR PARKING
- CONTRACTORS STORAGE, STAGING, & PARKING AREA
- BARRICADE LINE
- CONSTRUCTION SETBACK LINE (SEE CONSTRUCTION ACTIVITY PLAN & NOTES)
- CONTRACTOR'S ACCESS
- 15** HANGAR NUMBERS

- PHASING NOTES**
1. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN THE AREA DURING HAULING OPERATIONS.
 2. RUNWAY 3/21 WILL REMAIN OPEN FOR THE DURATION OF THE CONTRACT.
 3. THE CONTRACTOR SHALL PLACE ALL BARRICADES AND SETBACK LINES PRIOR TO STARTING WORK. CONSTRUCTION OPERATIONS SHALL REMAIN CLEAR OF THE CONSTRUCTION SETBACK LINE AT ALL TIMES.
 4. THE AIRPORT WILL REQUIRE A 14 CALENDAR DAY NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING THE CLOSURE.
 5. TAXIWAY C BETWEEN THE APRON AND T-HANGARS SHALL REMAIN OPEN FOR THE DURATION OF THE CONTRACT. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL YIELD TO ALL ONCOMING AIRCRAFT AND SHALL NOT RESUME MOVEMENT IN THIS AREA UNTIL THE AIRCRAFT HAS CLEARED THE TAXIWAY.
 6. THE AIRPORT IS SCHEDULED TO HAVE AN EVENT DURING THE TIME FRAME OF THIS PROJECT. THERE SHOULD BE NO IMPACT ON THE CONSTRUCT OF THIS PROJECT FROM THAT EVENT. HOWEVER COORDINATION SHALL BE MAINTAINED WITH THE AIRPORT TO ENSURE NO IMPACTS WILL BE MADE.
 7. THE CONTRACTOR SHALL COORDINATE THE SCHEDULE OF THIS PROJECT CLOSELY WITH OTHER CONTRACTORS ON SITE. IT IS OF PARTICULAR CONCERN THAT CONSTRUCTION SCHEDULING BE COORDINATED WITH THE AIRPORT.
 8. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT TO SCHEDULE THE CLOSURE. ITEMS SUCH AS THE EXTENDED WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT DEPENDABILITY, AND MANPOWER AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THE CLOSURE.
 9. THE PURPOSE OF FLAGMEN AND STOP SIGNS IS TO CONTROL THE TRAFFIC ON TAXIWAY C BETWEEN THE APRON AND T-HANGARS. THE CONTRACTOR SHALL YIELD TO ALL AIRCRAFT OPERATIONS UNTIL THE TAXIWAY IS CLEAR.
 10. BARRICADES SHOWN ON THIS SHEET SHALL BE PLACED 250' FROM THE RUNWAY CENTERLINE OR 93' FROM A TAXIWAY CENTERLINE.
 11. ANY PERSONNEL ON SITE MAY NOT SMOKE OR HAVE ANY OPEN FLAME ON PROJECT LIMITS AT ANYTIME.
 12. MAXIMUM HEIGHT OF EQUIPMENT SHALL BE 25 FEET.



100% SUBMITTAL
MAY 6, 2016

REHABILITATE T-HANGAR TAXIWAYS AND ASSOCIATED ACCESS ROAD IMPROVEMENTS



**VERMILION REGIONAL AIRPORT
VERMILION REGIONAL AIRPORT AUTHORITY
DANVILLE, ILLINOIS**

| MARK | DATE | DESCRIPTION |
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| FED PROJ. NO. | 3-17-SBGP-XX |
| IL PROJ. NO. | DNV-4445; IL CONTRACT NO. VE052 |
| CMT PROJECT NO. | 15042-02-00 |
| CAD DWG FILE: | DNV4445-1504202-GC100.DWG |
| DESIGNED BY: | NRF |
| DRAWN BY: | DPA |
| CHECKED BY: | JEF |
| APPROVED BY: | CBG |
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CONSTRUCTION ACTIVITY PLAN INDEX

GC100

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REHABILITATE T-HANGAR
TAXIWAYS AND ASSOCIATED
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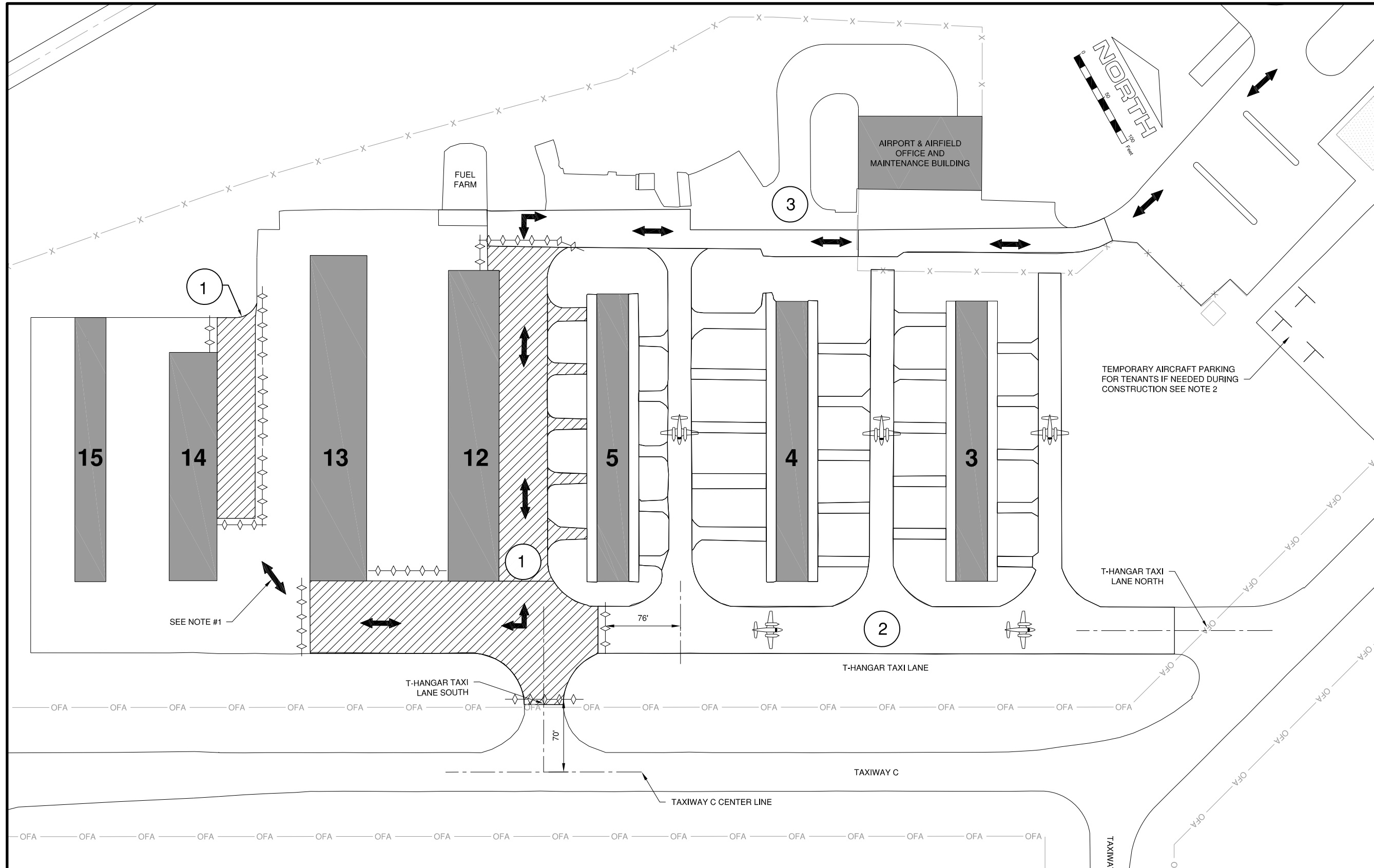
VERMILION REGIONAL AIRPORT
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IL PROJ. NO. DNV-4445; IL CONTRACT NO. VE052
CMT PROJECT NO: 15042-02-00
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SHEET TITLE
**CONSTRUCTION
ACTIVITY PLAN -
PHASE 1**

GC101



- PHASE #1
1. SAW CUT ALL EDGES
 2. MILL 2" OF BITUMINOUS SURFACE
 3. CLEAN AND APPLY TACK COAT
 4. PAVE 2" NEW BITUMINOUS SURFACE

LEGEND

- AIRCRAFT ACCESS
- TRUCK ROUTE
- AIRPORT BUILDINGS AND HANGARS
- 15** HANGAR NUMBERS
- EXISTING FENCE LINE

- NOTES**
1. TRUCKING WILL ACCESS THE SOUTHERN MOST PHASE ONE PAVEMENT FROM THE EAST GOING AROUND HANGAR 12 AND 13 TO PREVENT FURTHER DAMAGE TO THE PAVEMENT ON THE WEST ENDS.
 2. HANGAR TENANTS IN HANGARS 15, 14, 13, 12 AND SOUTH SIDE OF 5 WILL NOT BE ABLE TO TAXI TO TAXIWAY C DURING PHASE 1.

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REHABILITATE T-HANGAR TAXIWAYS AND ASSOCIATED ACCESS ROAD IMPROVEMENTS

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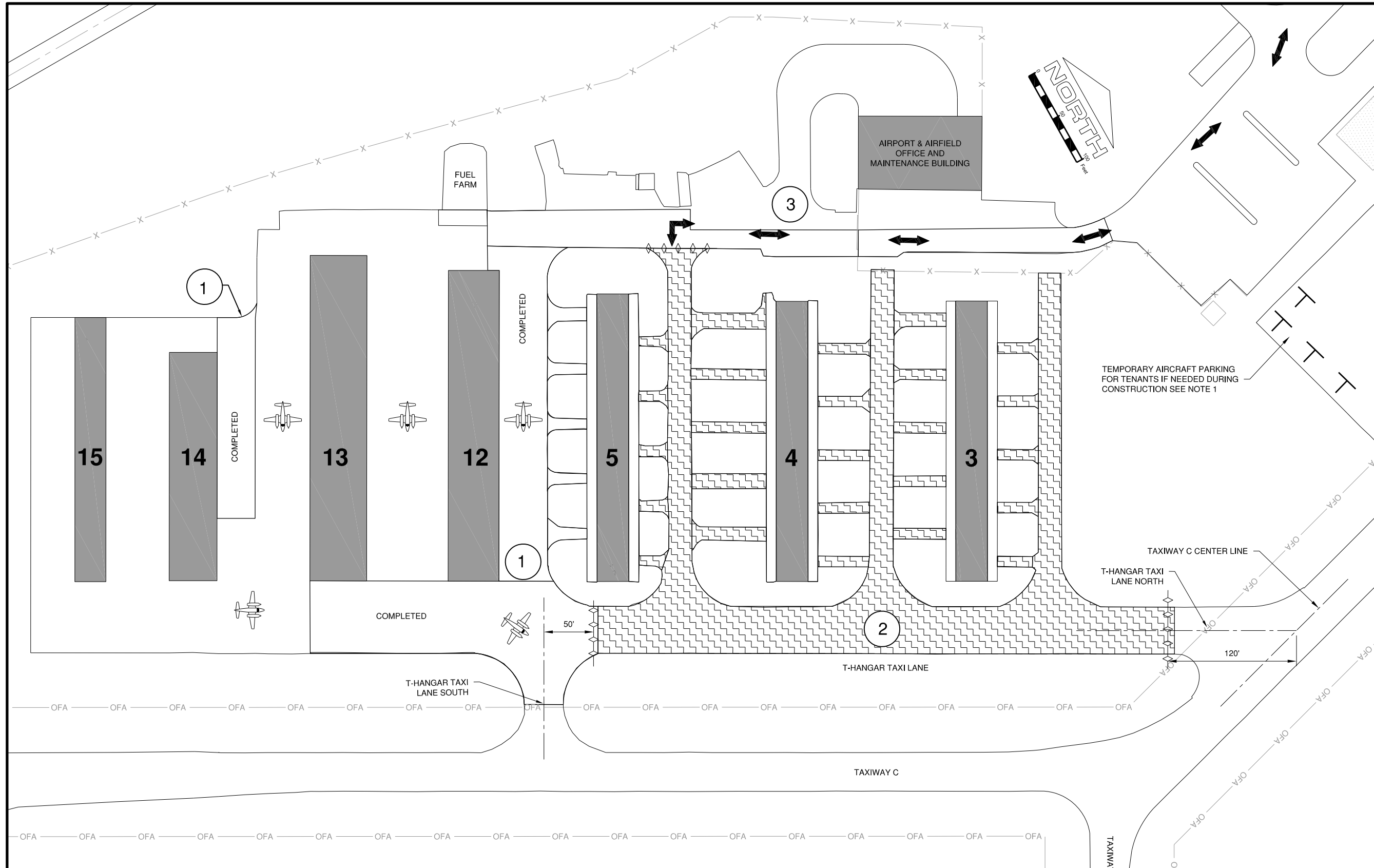
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AIRPORT AUTHORITY
DANVILLE, ILLINOIS**

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**CONSTRUCTION
ACTIVITY PLAN -
PHASE 2**

GC102



- PHASE #2**
1. SAW CUT ALL EDGES 2"
 2. MILL 2" OF BITUMINOUS SURFACE
 3. CLEAN AND APPLY TACK COAT
 4. PAVE 2" NEW BITUMINOUS SURFACE
 5. SEAL COAT THE ENDS OF THE TAXI LANES NEAR HANGERS 3 & 4 AS LABELED

LEGEND

- AIRCRAFT ACCESS
- TRUCK ROUTE
- AIRPORT BUILDINGS AND HANGARS
- 15** HANGAR NUMBERS
- EXISTING FENCE LINE

- NOTES**
1. HANGAR TENANTS IN HANGARS 3, 4, AND NORTH SIDE OF 5 WILL NOT HAVE ACCESS TO THEIR HANGARS UNTIL PHASE 2 IS COMPLETE.

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TAXIWAYS AND ASSOCIATED
ACCESS ROAD IMPROVEMENTS

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AIRPORT AUTHORITY
DANVILLE, ILLINOIS

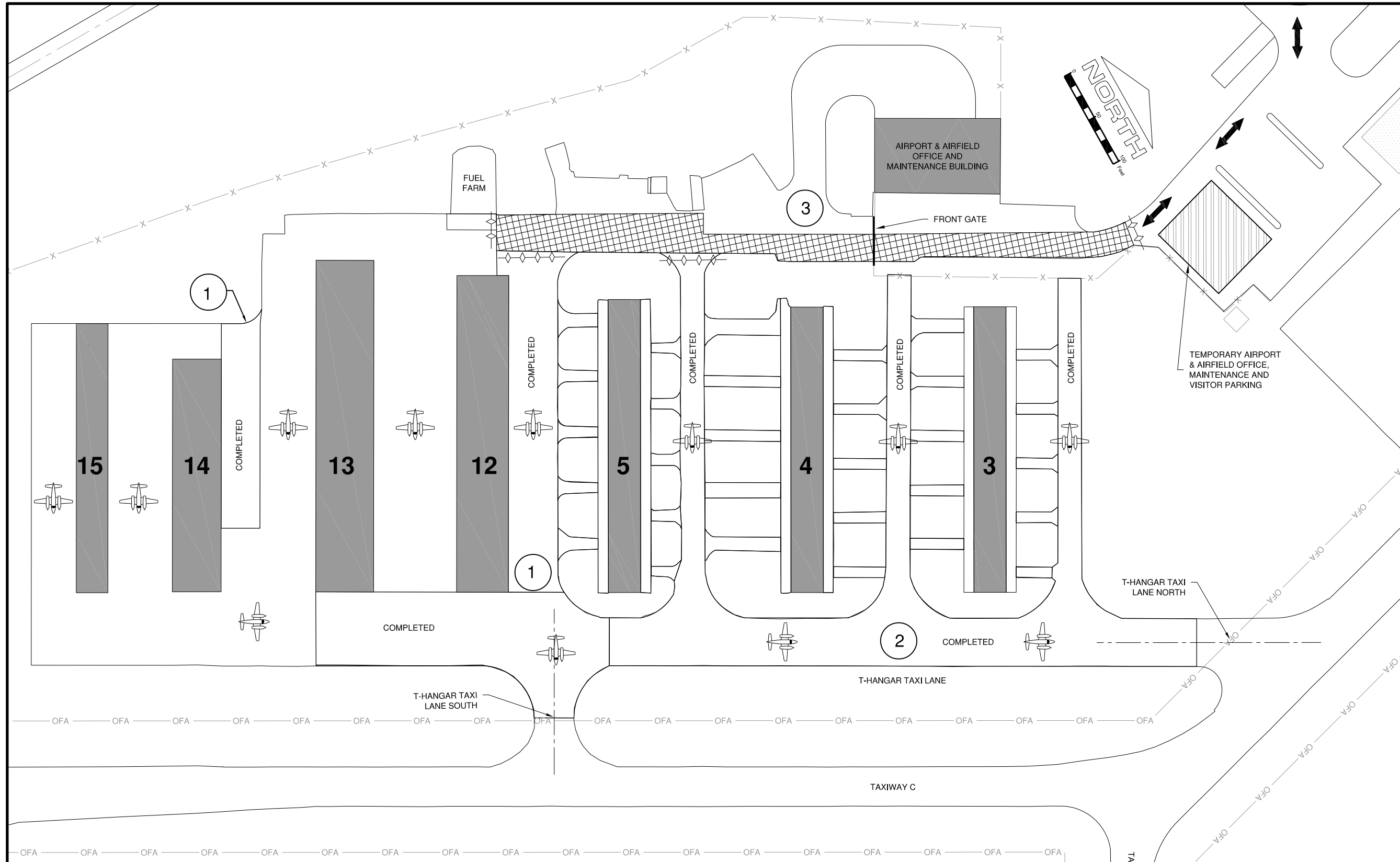
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IL PROJ. NO. DNV-4445; IL CONTRACT NO. VE052
CMT PROJECT NO: 15042-02-00
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SHEET TITLE
**CONSTRUCTION
ACTIVITY PLAN -
PHASE 3**

GC103

SHEET 8 OF 26



- PHASE #3**
- FOR PAVEMENT NORTH OF FRONT GATE**
1. SAW CUT ALL EDGES
 2. REMOVE PAVEMENT FULL-DEPTH
 3. CONSTRUCT NEW 6" CRUSHED AGGREGATE BASE, APPLY PRIME COAT
 4. CONSTRUCT NEW 3" BITUMINOUS PAVEMENT
 5. SEAL COAT LABELED SECTIONS OF PAVEMENT
- FOR PAVEMENT SOUTH OF FRONT GATE**
1. SAW CUT ALL EDGES
 2. MILL 6" OF BITUMINOUS SURFACE & AGGREGATE
 3. APPLY TACK COAT
 4. CONSTRUCT NEW BITUMINOUS PAVEMENT
 5. SEAL COAT LABELED SECTIONS OF PAVEMENT

LEGEND

- AIRCRAFT ACCESS
- TRUCK ROUTE
- AIRPORT BUILDINGS AND HANGARS
- 15** HANGAR NUMBERS
- EXISTING FENCE LINE

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AIRPORT AUTHORITY
DANVILLE, ILLINOIS

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IL PROJ. NO. DNV-4445; IL CONTRACT NO. VE052
CMT PROJECT NO: 15042-02-00
CAD DWG FILE: DNV4445-1504202-GC501.DWG
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SHEET TITLE

CONSTRUCTION
SAFETY PHASING
NOTES

GC501

SHEET 9 OF 26

16. HAZARD MARKING AND LIGHTING

- 1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- 2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- 3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER.
- 4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

17. PROTECTION

- 1. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 93' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
- 2. ALL WORK REQUIRED INSIDE OF THE RUNWAY 3/21 SAFETY AREAS, WHICH EXTENDS 250' FROM THE RUNWAY CENTERLINE, WILL REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 14 CALENDAR DAYS PRIOR TO THE REQUESTED CLOSURE TIME.

18. OTHER LIMITATIONS ON CONSTRUCTION

- 1. IF DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- 2. BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- 1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- 1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- 2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
- 3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- 4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911 AND SAFETY.
- 5. CONTACTS FOR THIS PROJECT ARE AS DISCUSSED IN THE PRE-CONSTRUCTION MEETING.

10. INSPECTION REQUIREMENTS

- 1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
- 2. THE CONTRACTOR AND AIRPORT SHALL ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

- 1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
- 2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. PENALTIES

- 1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

- 1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- 1. THE CONTRACTOR SHALL USE MARKING, LIGHTING, AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2F.
- 2. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

15. MARKING AND SIGNS FOR ACCESS ROUTES

- 1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THIS SHEET AND THE CONSTRUCTION ACTIVITY PLAN SHEET.

5. CONTRACTOR ACCESS

- 1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
- 2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN.
- 3. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, ASPHALT, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE.
- 4. CONTRACTOR WORK CREWS MUST MONITOR THE UNICOM AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS. BE AWARE OF TENANT AIRCRAFT MOVEMENT NEAR THE WORK AREAS.
- 5. THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN.
- 6. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
- 7. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
- 8. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
- 9. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
- 10. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- 11. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.

6. WILDLIFE MANAGEMENT

- 1. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- 2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
- 3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- 1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
- 2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

1. GENERAL

- 1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- 2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- 3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL.
- 4. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED, THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS TO THE ENGINEEERS.
- 5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
- 6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

2. COORDINATION

- 1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- 2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- 3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.
- 4. IT IS ANTICIPATED THAT THE AIRPORT MAY HAVE OTHER CONSTRUCTION PROJECTS ONGOING. THE CONTRACTOR SHALL COORDINATE THE CONSTRUCTION SCHEDULE/PHASING CLOSELY WITH OTHER CONTRACTORS PRIOR TO THE START OF CONSTRUCTION. THE AIRPORT WILL HAVE THE FINAL SAY REGARDING ALL CONSTRUCTION SCHEDULING AND PHASING.

3. PHASING

- 1. TOTAL CONTRACT TIME SHALL BE XXXX CALENDAR DAYS.
- 2. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.

4. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- 1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
- 2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- 3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

100% SUBMITTAL
MAY 6, 2016

**REHABILITATE T-HANGAR
TAXIWAYS AND ASSOCIATED
ACCESS ROAD IMPROVEMENTS**

OWNER



**VERMILION REGIONAL AIRPORT
VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, ILLINOIS**

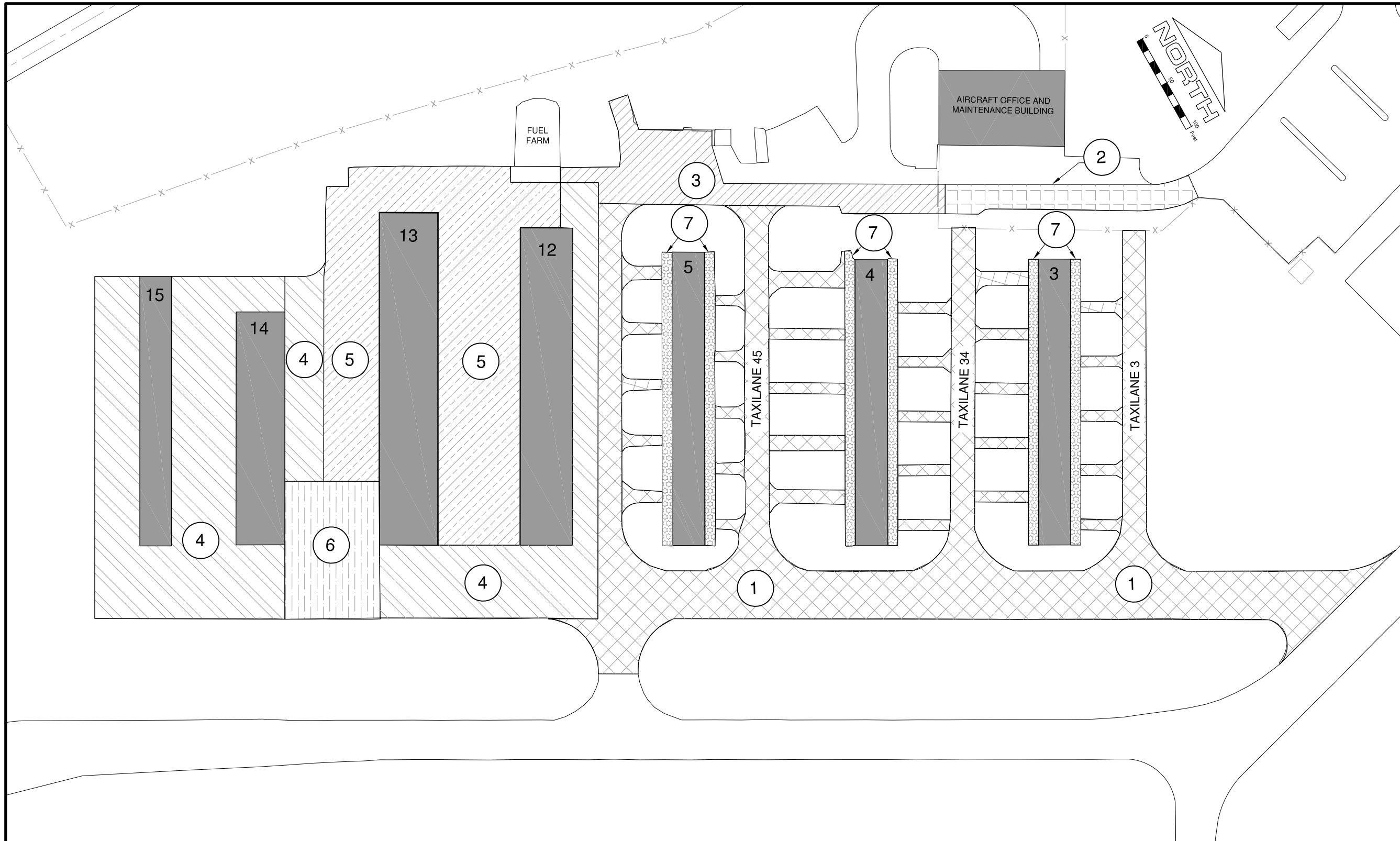
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FED PROJ. NO. 3-17-SBGP-XX
IL. PROJ. NO. DNV-4445; IL. CONTRACT NO. VE052
CMT PROJECT NO: 15042-02-00
CAD DWG FILE: DNV4445-1504202-CD101.DWG
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**EXISTING PAVEMENT
STRUCTURES**

CD101

SHEET 10 OF 26



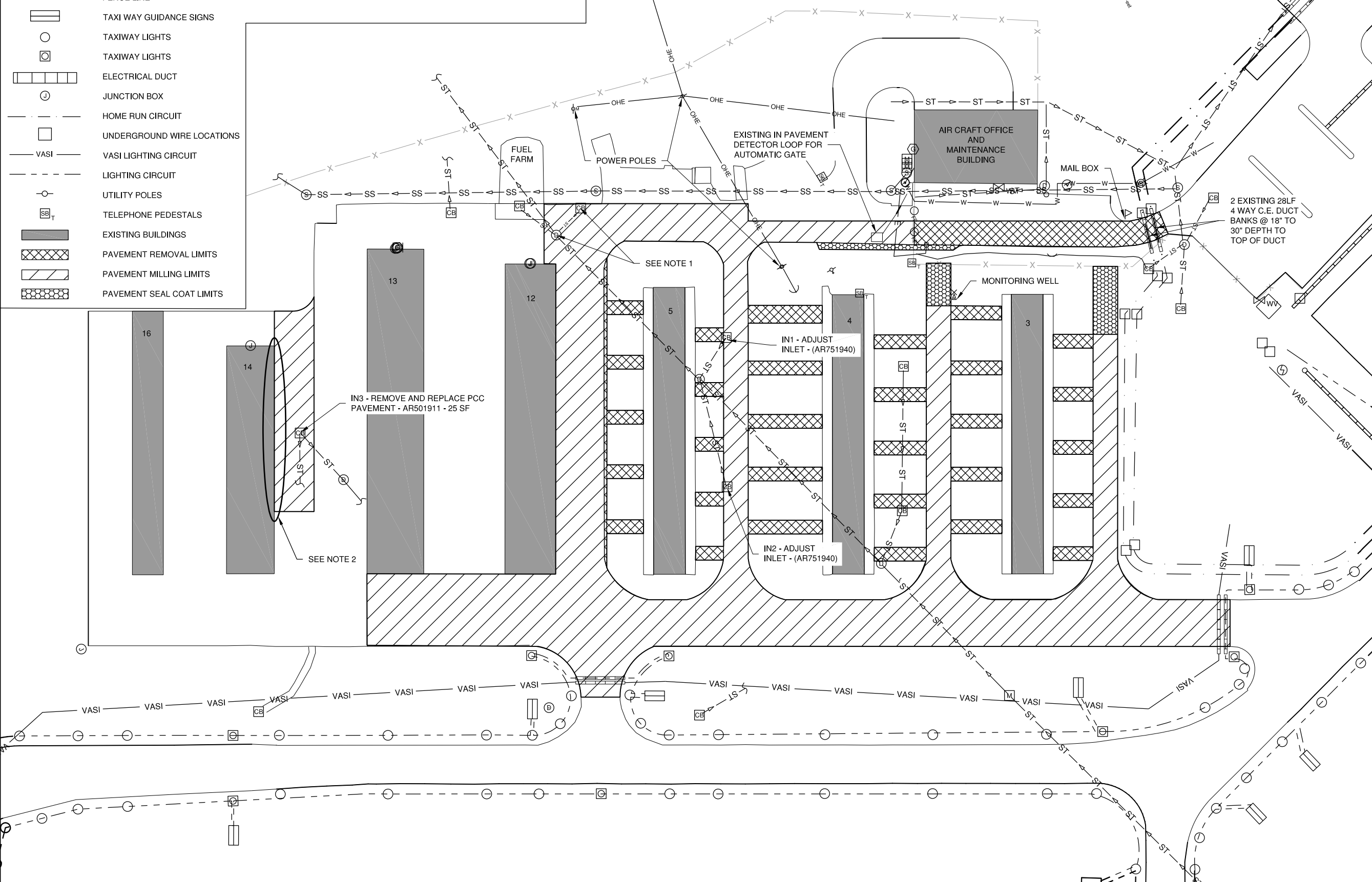
EXISTING PAVEMENT STRUCTURE LEGEND

| | | | | | | | | |
|---|--|---|---|--|--|---|--|---|
| 1 | | 2" BITUMINOUS SURFACE COURSE 5" CRUSHED AGG. BASE COURSE | 3 | | 3" BITUMINOUS SURFACE COURSE 12" CRUSHED AGG. BASE COURSE | 5 | | UNKNOWN 2" BITUMINOUS OVERLAY |
| 2 | | 7" BITUMINOUS SURFACE COURSE COMPACTED SUBGRADE | 4 | | 4" BITUMINOUS SURFACE COURSE 6" CRUSHED AGG. BASE COURSE | 6 | | 4" BITUMINOUS SURFACE COURSE 6" CRUSHED AGG. BASE COURSE |
| | | | | | | 7 | | 4" BITUMINOUS SURFACE COURSE COURSE ON SUBGRADE |

- LEGEND**
- ST— STORM SEWER MAIN
 - SS— SANITARY SEWER MAIN
 - CB CATCH BASIN
 - ⊙ SANITARY SEWER MANHOLE
 - ⊕ STORM SEWER MANHOLE
 - OHE— OVER HEAD ELECTRIC
 - ××× FENCE LINE
 - ▭ TAXI WAY GUIDANCE SIGNS
 - TAXIWAY LIGHTS
 - ⊖ TAXIWAY LIGHTS
 - ▬ ELECTRICAL DUCT
 - ⊙ JUNCTION BOX
 - HOME RUN CIRCUIT
 - UNDERGROUND WIRE LOCATIONS
 - VASI VASI LIGHTING CIRCUIT
 - LIGHTING CIRCUIT
 - UTILITY POLES
 - SB_T TELEPHONE PEDESTALS
 - ▭ EXISTING BUILDINGS
 - ▨ PAVEMENT REMOVAL LIMITS
 - ▧ PAVEMENT MILLING LIMITS
 - ▩ PAVEMENT SEAL COAT LIMITS

NOTE

1. BOTH INLETS TO REMAIN IN PLACE AND WILL BE PROTECTED DURING ALL PAVING AND MILLING OPERATIONS. NO ADJUSTMENTS WILL BE MADE.
2. PAVEMENT IS ADJACENT TO EXISTING HANGAR WITH SLIDING DOORS. DOOR HARDWARE IS WITHIN AND ADJACENT TO THE PAVEMENT REMOVAL LIMITS. CONTRACTOR SHALL REMOVE HARDWARE PRIOR TO PAVEMENT REMOVAL TO ALLOW REMOVAL EQUIPMENT ACCESS TO THE BUILDING FOUNDATION. HARDWARE SHALL BE RE-INSTALLED AFTER THE PAVEMENT IMPROVEMENTS HAVE BEEN CONSTRUCTED. WORK SHALL BE INCIDENTAL TO THE CONSTRUCTION.



CMT
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 CONSULTANTS

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 MAY 6, 2016

**REHABILITATE T-HANGAR
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VERMILION REGIONAL AIRPORT
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 IL PROJ. NO. DNV-4445; IL CONTRACT NO. VE052
 CMT PROJECT NO: 15042-02-00
 CAD DWG FILE: DNV4445-1504202-CD102.DWG
 DESIGNED BY: NRF
 DRAWN BY: DPA
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**EXISTING
 CONDITIONS &
 REMOVALS**

CD102
 SHEET 11 OF 26

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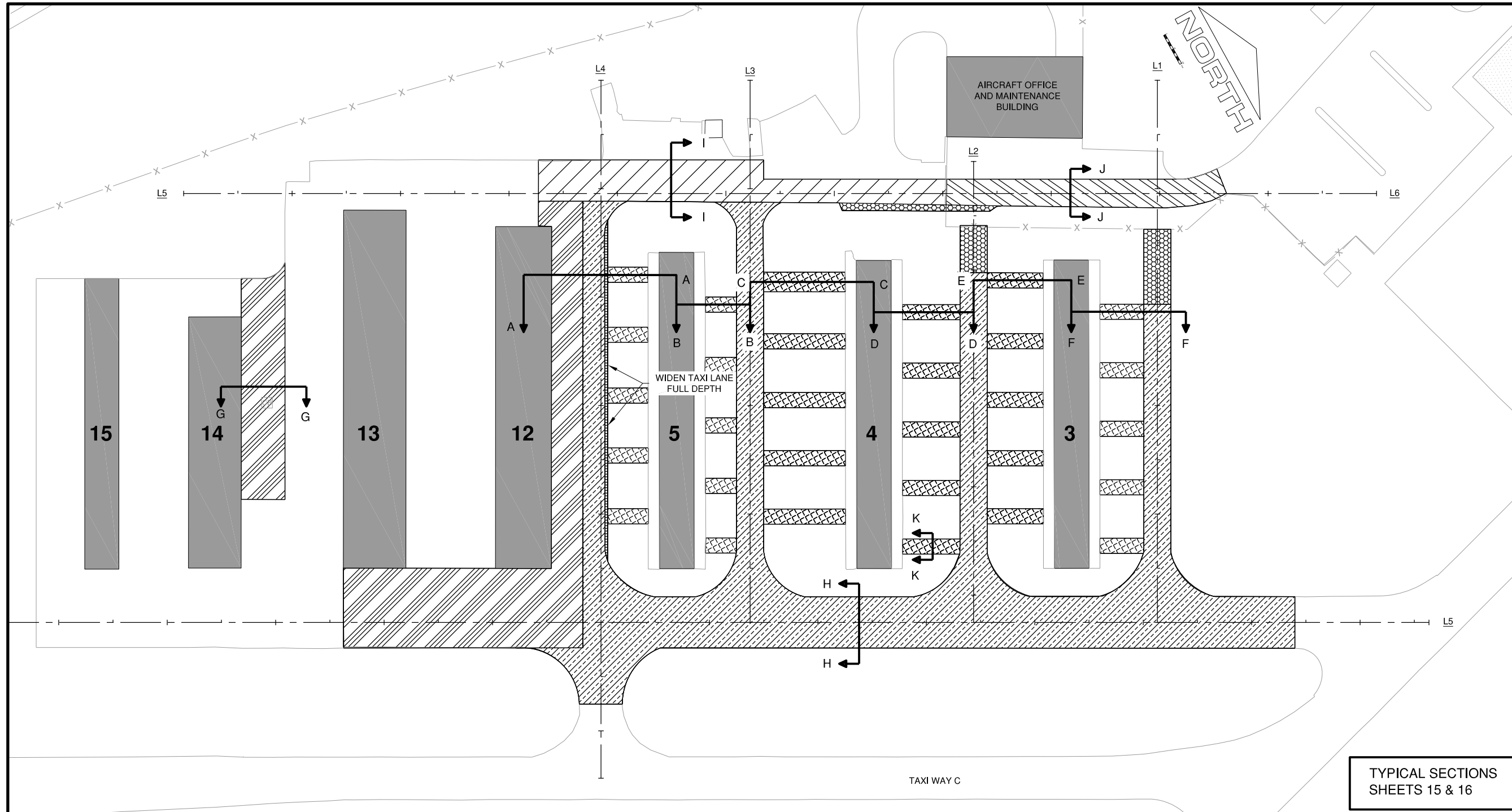
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TAXIWAYS AND ASSOCIATED
ACCESS ROAD IMPROVEMENTS

OWNER



VERMILION REGIONAL AIRPORT
VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, ILLINOIS

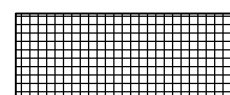
TYPICAL SECTIONS
SHEETS 15 & 16



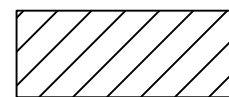
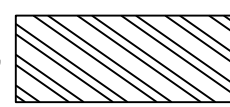
LEGEND



- MILL 2" BITUMINOUS SURFACE
- APPLY TACK COAT (603)
- CONSTRUCT NEW 2" OF BITUMINOUS SURFACE (401)



- REMOVE PAVEMENT FULL-DEPTH & WIDEN TO 14' MINIMUM
- CONSTRUCT NEW 6" CRUSHED AGGREGATE BASE (209)
- APPLY PRIME COAT (602)
- CONSTRUCT NEW 3" BITUMINOUS SURFACE (401)



- H-H**
- MILL 3" OF BITUMINOUS SURFACE AND EXCAVATE 3" OF AGGREGATE (152)
 - APPLY PRIME COAT
 - CONSTRUCT NEW BITUMINOUS BASE (403)
 - APPLY TACK COAT (603)
 - CONSTRUCT NEW BITUMINOUS SURFACE (401)



TAXI LANE WIDENING

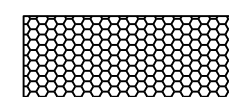
- EXCAVATE TO ACCOMMODATE NEW STRUCTURE (152)
- CONSTRUCT NEW 6" CRUSHED AGGREGATE BASE (209)
- APPLY PRIME COAT (602)
- CONSTRUCT NEW 2" BITUMINOUS BASE (403)
- APPLY TACK COAT (603)
- CONSTRUCT NEW 2" BITUMINOUS SURFACE (401)

J-J

- REMOVE PAVEMENT FULL-DEPTH
- CONSTRUCT NEW 4" AGGREGATE BASE (209)
- APPLY PRIME COAT (602)
- CONSTRUCT NEW 3" BITUMINOUS BASE (403)
- APPLY TACK COAT (603)
- CONSTRUCT NEW 3" BITUMINOUS SURFACE (401)

H-H

- MILL 2" OF BITUMINOUS SURFACE TO EXISTING AGGREGATE BASE
- ROLL AND PROOF ROLL EXISTING AGGREGATE BASE
- REPAIR SUBGRADE (AS NEEDED, SEE NOTE 2) (152)
- PRIME EXISTING AGGREGATE BASE (602)
- CONSTRUCT NEW 2" BITUMINOUS SURFACE (401)



SEAL COAT (625)

NOTES

- AREAS WHERE THE BITUMINOUS SURFACE HAS BEEN FULLY REMOVED SHALL BE ROLLED TO THE SATISFACTION OF THE RESIDENT ENGINEER, PRIOR TO PAVING. NO DIRECT PAYMENT WILL BE MADE FOR ROLLING OPERATIONS, IT SHALL BE INCIDENTAL TO THE CONSTRUCTION.
- SUBGRADE REPAIR IS ASSUMED AT 20% OF THE TOTAL AREA SPECIFIED TO BE MILLED TO THE AGGREGATE BASE LAYER. ACTUAL QUANTITY WILL DEPEND ON THE AREAS IDENTIFIED AND AGREED UPON DURING THE PROOF ROLL OF THE EXISTING AGGREGATE LAYER.

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CMT PROJECT NO: 15042-02-00
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SHEET TITLE
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IMPROVEMENTS**

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MAY 6, 2016

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TAXIWAYS AND ASSOCIATED
ACCESS ROAD IMPROVEMENTS

OWNER

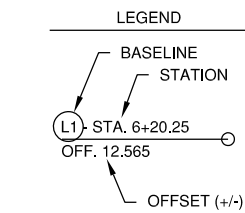
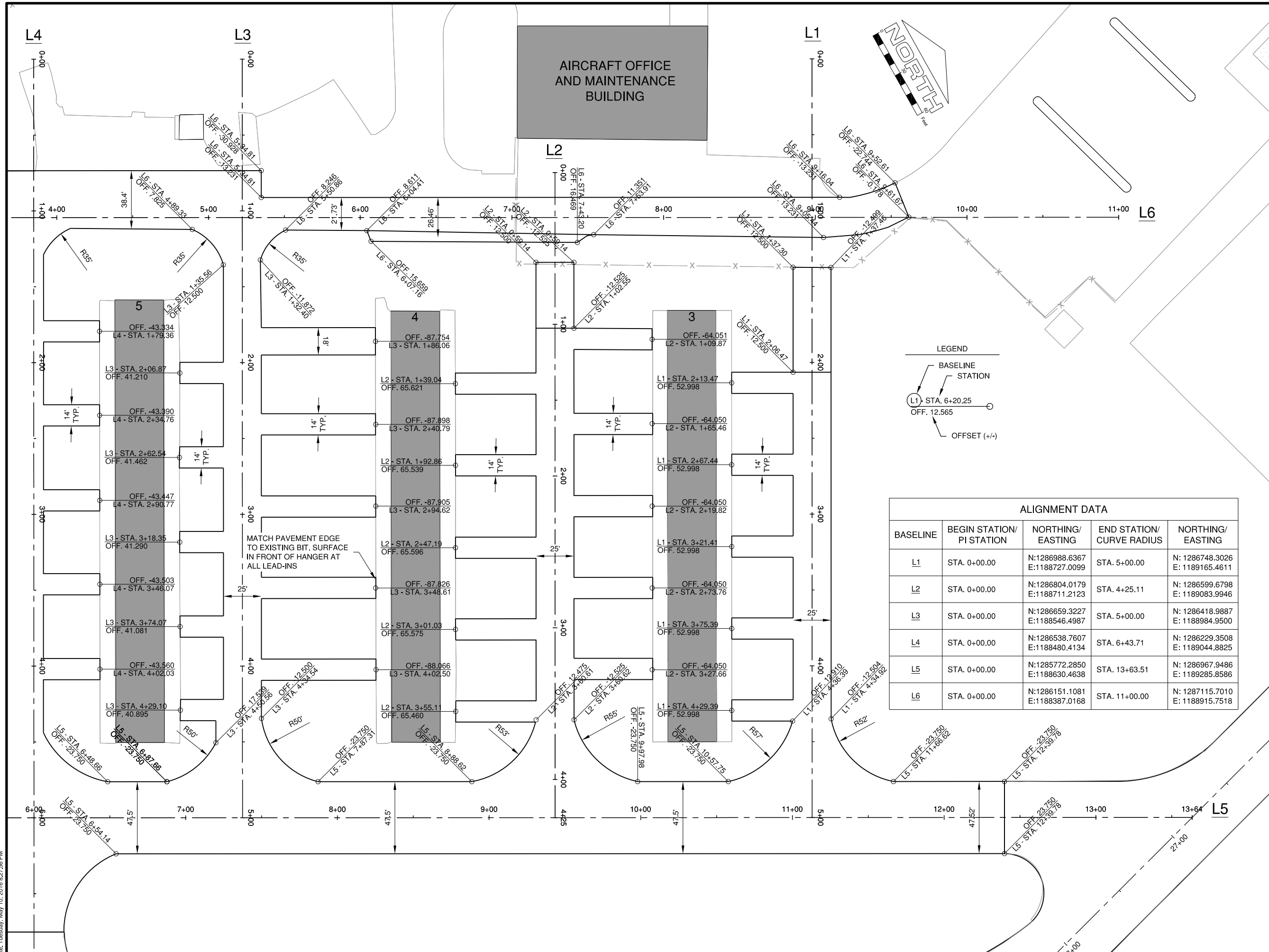


VERMILION REGIONAL AIRPORT
VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, ILLINOIS

MARK DATE DESCRIPTION

FED PROJ. NO. 3-17-SBGP-XX
IL PROJ. NO. DNV-4445; IL CONTRACT NO. VE052
CMT PROJECT NO: 15042-02-00
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SHEET TITLE
**PROPOSED
GEOMETRY 1**

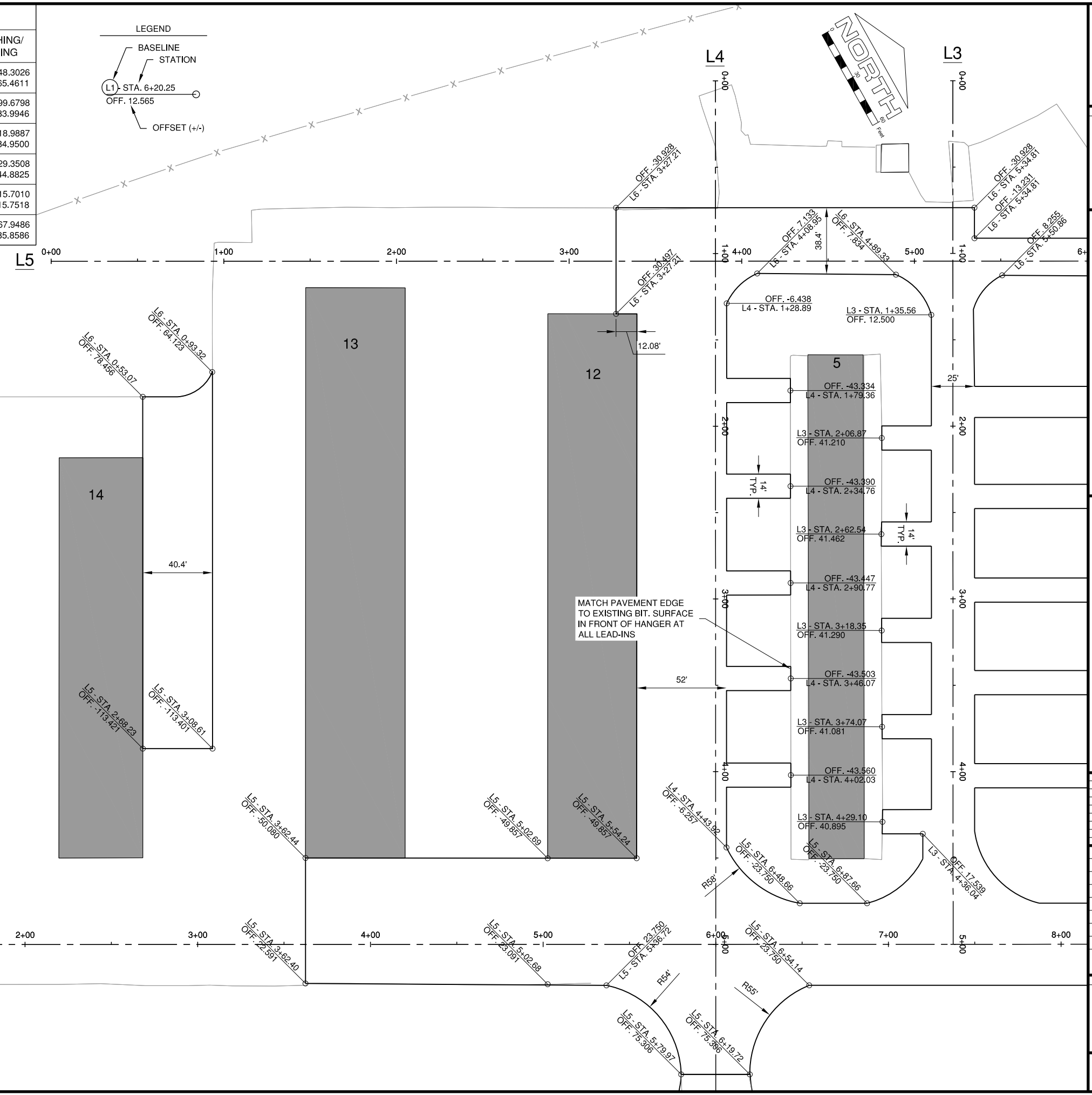
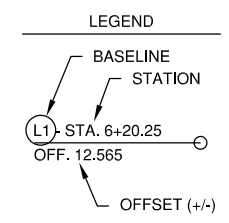


ALIGNMENT DATA

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| L2 | STA. 0+00.00 | N:1286804.0179 E:1188711.2123 | STA. 4+25.11 | N: 1286599.6798 E: 1189083.9946 |
| L3 | STA. 0+00.00 | N:1286659.3227 E:1188546.4987 | STA. 5+00.00 | N: 1286418.9887 E: 1188984.9500 |
| L4 | STA. 0+00.00 | N:1286538.7607 E:1188480.4134 | STA. 6+43.71 | N: 1286229.3508 E: 1189044.8825 |
| L5 | STA. 0+00.00 | N:1285772.2850 E:1188630.4638 | STA. 13+63.51 | N: 1286967.9486 E: 1189285.8586 |
| L6 | STA. 0+00.00 | N:1286151.1081 E:1188387.0168 | STA. 11+00.00 | N: 1287115.7010 E: 1188915.7518 |

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| L2 | STA. 0+00.00 | N:1286804.0179 E:1188711.2123 | STA. 4+25.11 | N: 1286599.6798 E: 1189083.9946 |
| L3 | STA. 0+00.00 | N:1286659.3227 E:1188546.4987 | STA. 5+00.00 | N: 1286418.9887 E: 1188984.9500 |
| L4 | STA. 0+00.00 | N:1286538.7607 E:1188480.4134 | STA. 6+43.71 | N: 1286229.3508 E: 1189044.8825 |
| L5 | STA. 0+00.00 | N:1286151.1081 E:1188387.0168 | STA. 11+00.00 | N: 1287115.7010 E: 1188915.7518 |
| L6 | STA. 0+00.00 | N:1285772.2850 E:1188630.4638 | STA. 13+63.51 | N: 1286967.9486 E: 1189285.8586 |



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CONSULTANTS

100% SUBMITTAL
MAY 6, 2016

REHABILITATE T-HANGAR
TAXIWAYS AND ASSOCIATED
ACCESS ROAD IMPROVEMENTS



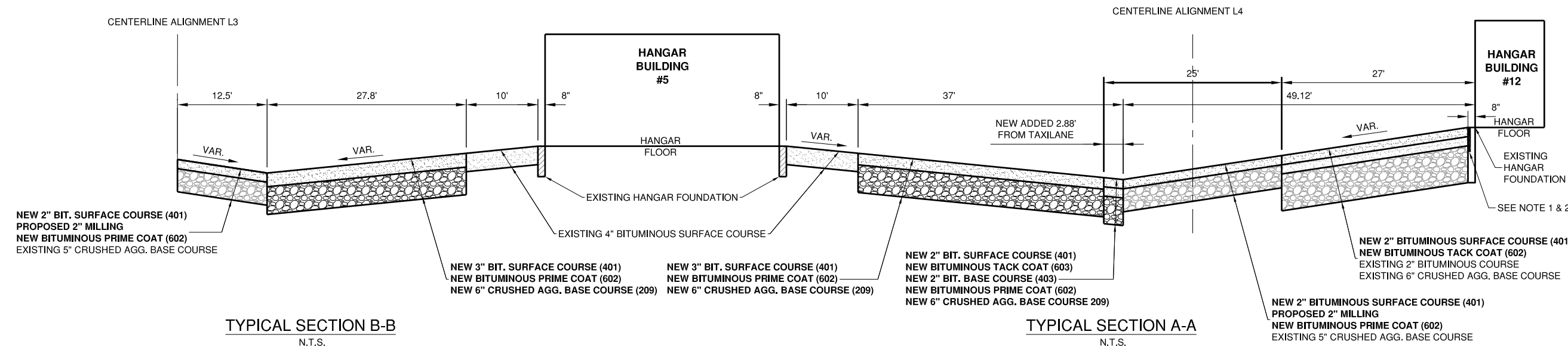
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DANVILLE, ILLINOIS

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CMT PROJECT NO: 15042-02-00
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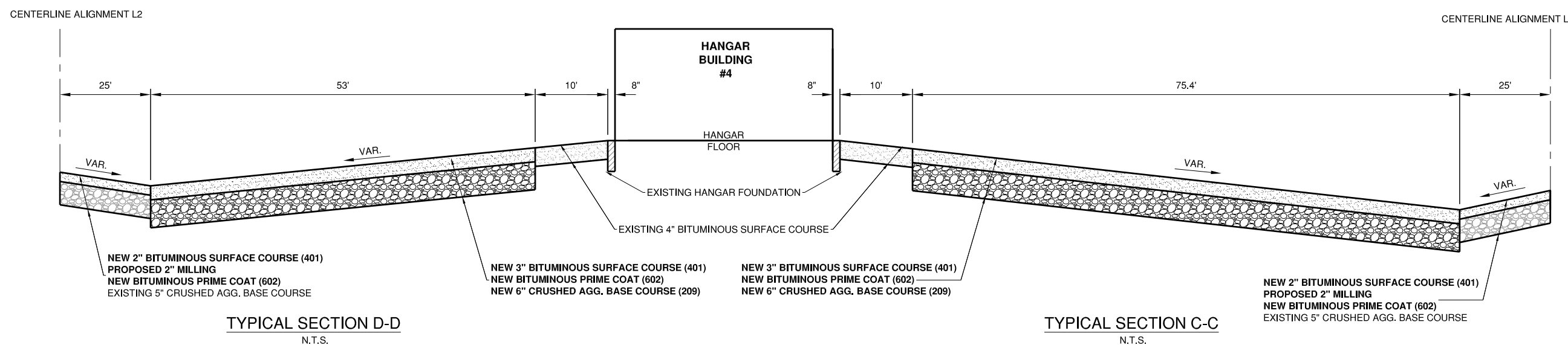
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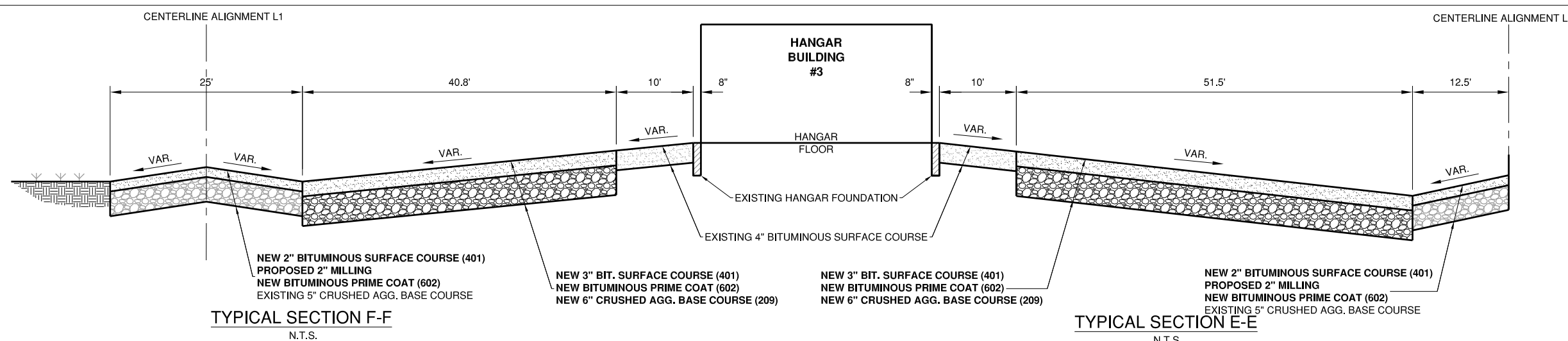
TYPICAL SECTION B-B
N.T.S.

TYPICAL SECTION A-A
N.T.S.



TYPICAL SECTION D-D
N.T.S.

TYPICAL SECTION C-C
N.T.S.



TYPICAL SECTION F-F
N.T.S.

TYPICAL SECTION E-E
N.T.S.

NOTES FOR TYPICAL SECTIONS:

- FULL DEPTH SAW CUT THROUGH BITUMINOUS SURFACE LAYER PRIOR TO MILLING.
- SAW CUT SHALL BE AT THE EDGE OF THE BUILDING FOUNDATION WHERE IT MEETS THE EXISTING PAVEMENT.
- WHEN CONSTRUCTING THE BITUMINOUS OVERLAY AT ALONG HANGAR DOORS, THE CONTRACTOR SHALL MATCH THE EXISTING FOUNDATION ELEVATION + 0.00'
- PRIME COAT SHALL BE APPLIED TO EXPOSED AGGREGATE SURFACE BEFORE ANY BITUMINOUS LAYERS ARE CONSTRUCTED.
- TACK COAT SHALL BE APPLIED BETWEEN LIFTS OF BITUMINOUS PAVEMENT LAYERS.
- CONTRACTOR SHALL PROVIDE RUBBER BUTT JOINT RAMPS TO THE AIRPORT FOR TENANT USE. RAMPS ARE TO REMAIN PROPERTY OF THE CONTRACTOR AFTER CONSTRUCTION.

100% SUBMITTAL
MAY 6, 2016

REHABILITATE T-HANGAR TAXIWAYS AND ASSOCIATED ACCESS ROAD IMPROVEMENTS

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TYPICAL SECTIONS 1

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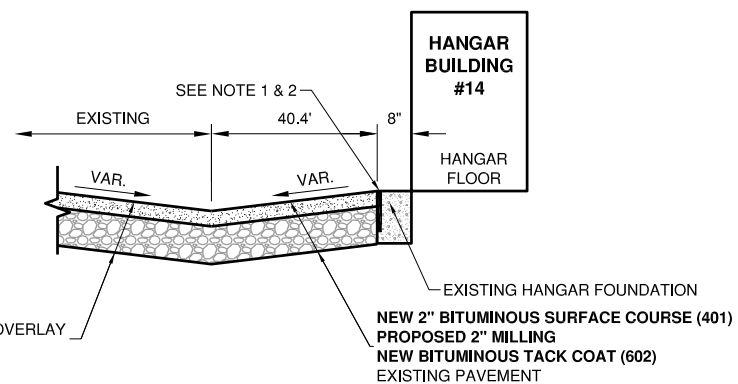


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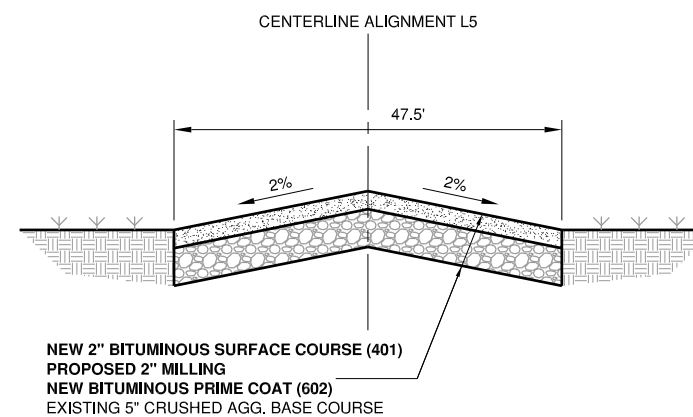
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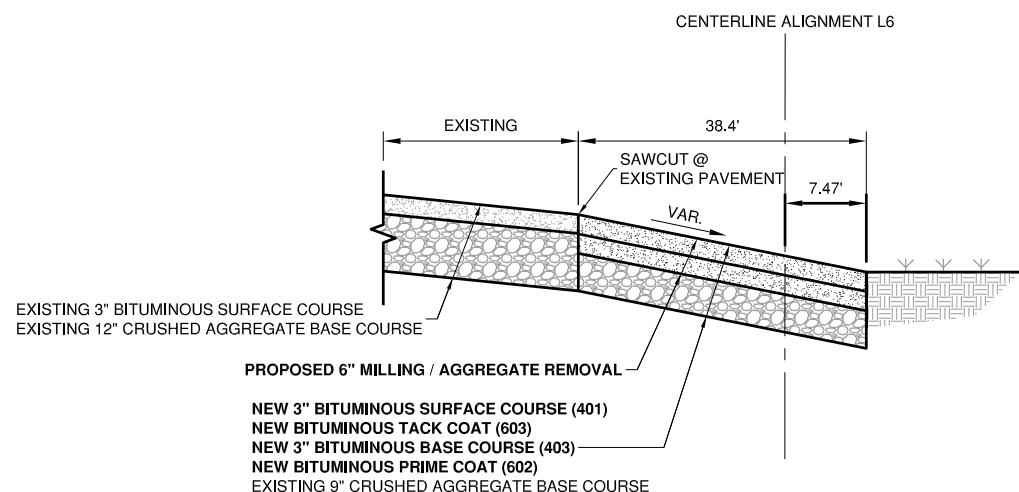
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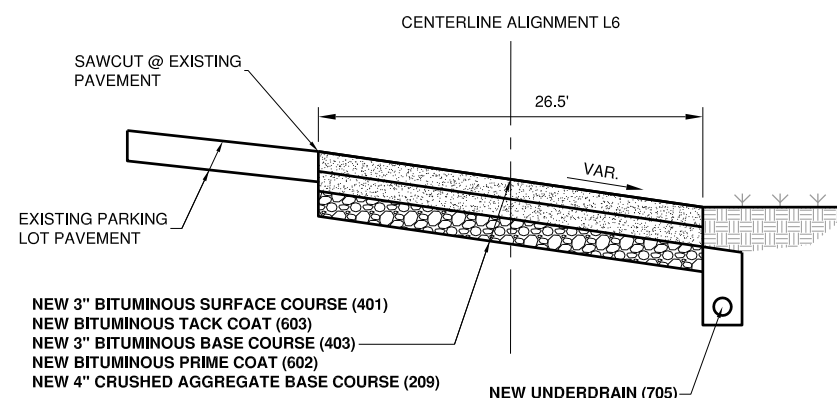
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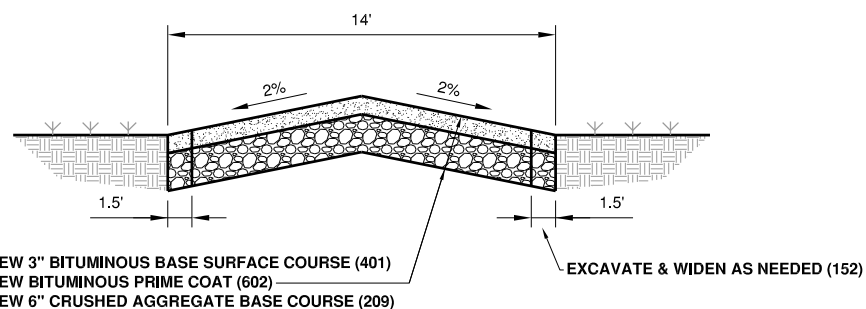
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TYPICAL SECTION I-I
N.T.S.



TYPICAL SECTION J-J
N.T.S.



TYPICAL SECTION K-K
N.T.S.

NOTES FOR TYPICAL SECTIONS:

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100% SUBMITTAL
MAY 6, 2016

REHABILITATE T-HANGAR
TAXIWAYS AND ASSOCIATED
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OWNER



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SHEET TITLE
STAKING PLAN

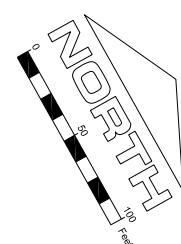
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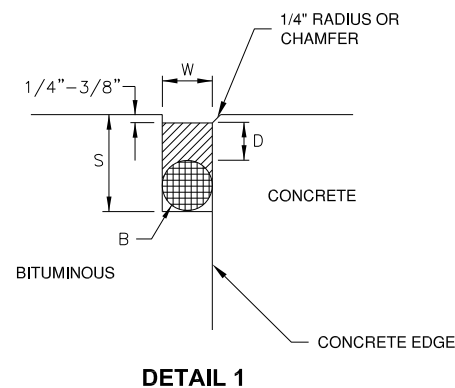


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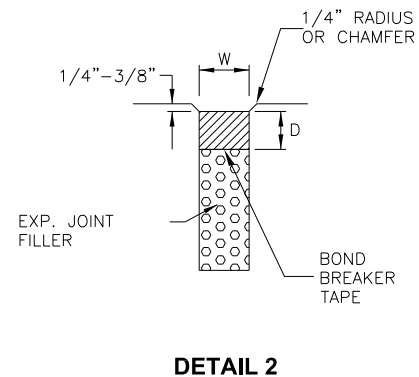
- PAVEMENT ELEVATION
- EXISTING INLET
- PAVEMENT CONTOURS



| JOINT SEALING DIMENSIONS | | |
|-------------------------------------|----------------|----------------|
| | DETAIL 1 | DETAIL 2 |
| HOT/ COLD POUR | HOT/ COLD POUR | HOT/ COLD POUR |
| W=WIDTH OF SEALANT RESERVOIR (IN.) | 1/2 | 3/4 |
| D=DEPTH OF SEALANT RESERVOIR (IN.) | 1/4 | 3/4 |
| B=BACKER ROD DIAMETER (IN.) | 5/8 | N/A |
| S=SECOND SAWCUT DEPTH (IN.) MINIMUM | 1-3/8 | N/A |



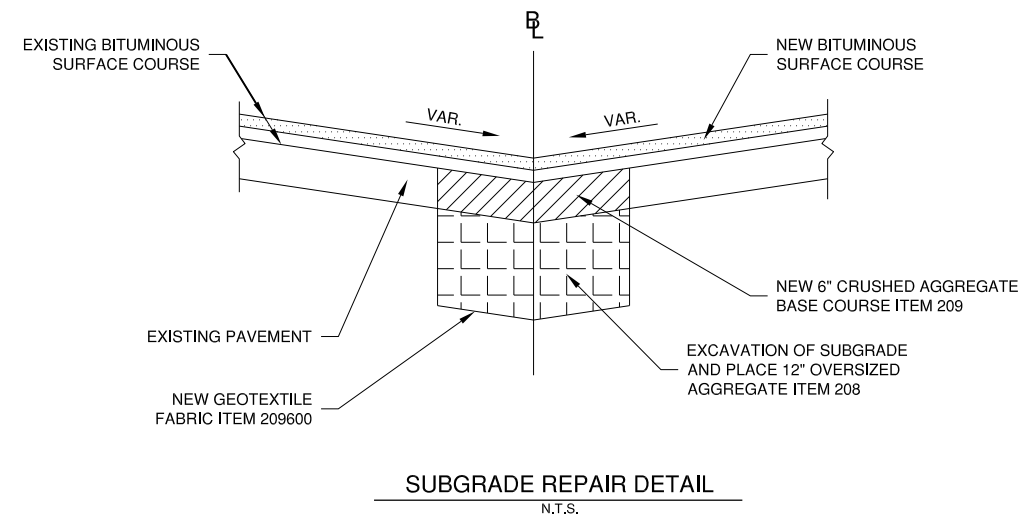
DETAIL 1



DETAIL 2

JOINT NOTES

1. ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
2. SAW CUT FOR JOINT SEALANT SHALL BE CONSTRUCTED IN THE BITUMINOUS PAVEMENT MATERIAL ADJACENT TO THE PCC PAVEMENT.
3. EXPANSION JOINT MATERIAL SHALL BE PLACED AROUND FRAME PRIOR TO CONCRETE POUR.

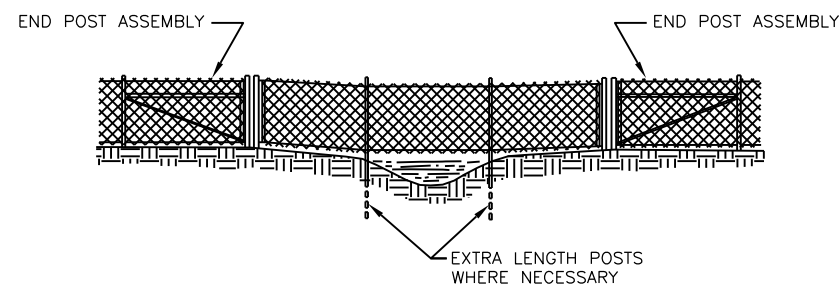


SUBGRADE REPAIR DETAIL

N.T.S.

NOTES:

1. LOCATION OF SUBGRADE REPAIR SHALL BE DETERMINED AT THE TIME OF CONSTRUCTION AND APPROVED BY THE RESIDENT ENGINEER.
2. LENGTH AND WIDTH OF SUBGRADE REPAIR SHALL VARY DEPENDENT ON THE AFFECT AREA IN NEED OF REPAIR.
3. THE FOLLOWING SHALL BE THE STEPS TAKEN TO REPAIR THE SUBGRADE:
 - 3.A. REMOVE ADDITIONAL 2" OF BITUMINOUS SURFACE COURSE - (IF NEEDED)
 - 3.B. REMOVE EXISTING AGGREGATE SUBBASE IN AREAS THAT REQUIRE SUBGRADE REPAIR.
 - 3.C. EXCAVATE 18" DEPTH OF SUBGRADE.
 - 3.D. COMPACT EXPOSED SUBGRADE.
 - 3.E. PLACE GEOTEXTILE FABRIC ITEM AR209600 ON TOP OF COMPACTED SUBGRADE.
 - 3.F. PLACE AND COMPACT OVERSIZED AGGREGATE ITEM 208 AT A THICKNESS OF 12".
 - 3.G. PLACE AND COMPACT 6" CRUSHED AGGREGATE ITEM 209, MEETING THE GRADES OF THE ADJACENT EXISTING AGGREGATE LAYER.
4. APPLY PRIME COAT AND CONSTRUCT BITUMINOUS LAYER AS SPECIFIED IN THE PLANS.
5. METHODS OF CONSTRUCTION SHALL MEET THE REQUIREMENTS SPECIFIED IN THE SPECIAL PROVISIONS.
6. ITEMS TO BE INCLUDED IN PAYMENT FOR SUBGRADE REPAIR ARE 3A, 3B, 3C, 3D, AND 3F. (AR152511)



THE CHAIN LINK FABRIC SHALL BE REPLACED BY BARBED WIRE STRANDS AT 12" MAXIMUM CENTERS BETWEEN THE DOUBLE POSTS SHOWN ON DETAIL A WHEN SHOWN ON THE PLANS.

FENCING DETAIL

100% SUBMITTAL
MAY 6, 2016

REHABILITATE T-HANGAR
TAXIWAYS AND ASSOCIATED
ACCESS ROAD IMPROVEMENTS

OWNER



VERMILION REGIONAL AIRPORT
VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, ILLINOIS

MARK | DATE | DESCRIPTION

FED PROJ. NO. 3-17-SBGP-XX
IL PROJ. NO. DNV-4445; IL CONTRACT NO. VE052
CMT PROJECT NO: 15042-02-00
CAD DWG FILE: DNV4445-1504202-CJ501.DWG
DESIGNED BY: NRF
DRAWN BY: DPA
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SHEET TITLE

PAVING &
MISCELLANEOUS
DETAILS

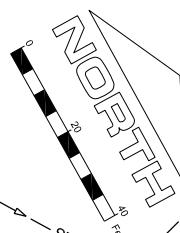
CJ501

SHEET 18 OF 26

NOTE

1. WHEN PLACING UNDERDRAIN AND GRADING SWALE ROLL BACK FENCE LINE WHERE NECESSARY. PLACE THE CENTER OF THE UNDERDRAIN AND SWALE AT THE MID POINT OF FENCE POSTS WHERE THE UNDERDRAIN AND SWALE ARE BEST SUITED.
2. AFTER GRADING IF NEEDED ADD BARBED WIRE TO THE BOTTOM OF FENCE LINE TO CLOSE GAP BETWEEN TURF AND FENCING SEE DETAIL ON - (PAVING AND MISCELLANEOUS DETAILS). ALL FENCE ALTERATIONS WILL BE CONSIDERED INCIDENTAL TO THE UNDERDRAIN PAY ITEM - (AR705524)
3. USE EXTREME CAUTION WHEN PERFORMING GRADING IN THIS AREA, FIELD LOCATE ALL HOMERUN CABLING PRIOR TO ANY EXCAVATION AND OR GRADING.
4. UNCLASSIFIED EXCAVATION REQUIRED TO CONSTRUCT BOTH SWALES IS 65 CY.

AIRPORT & AIRFIELD
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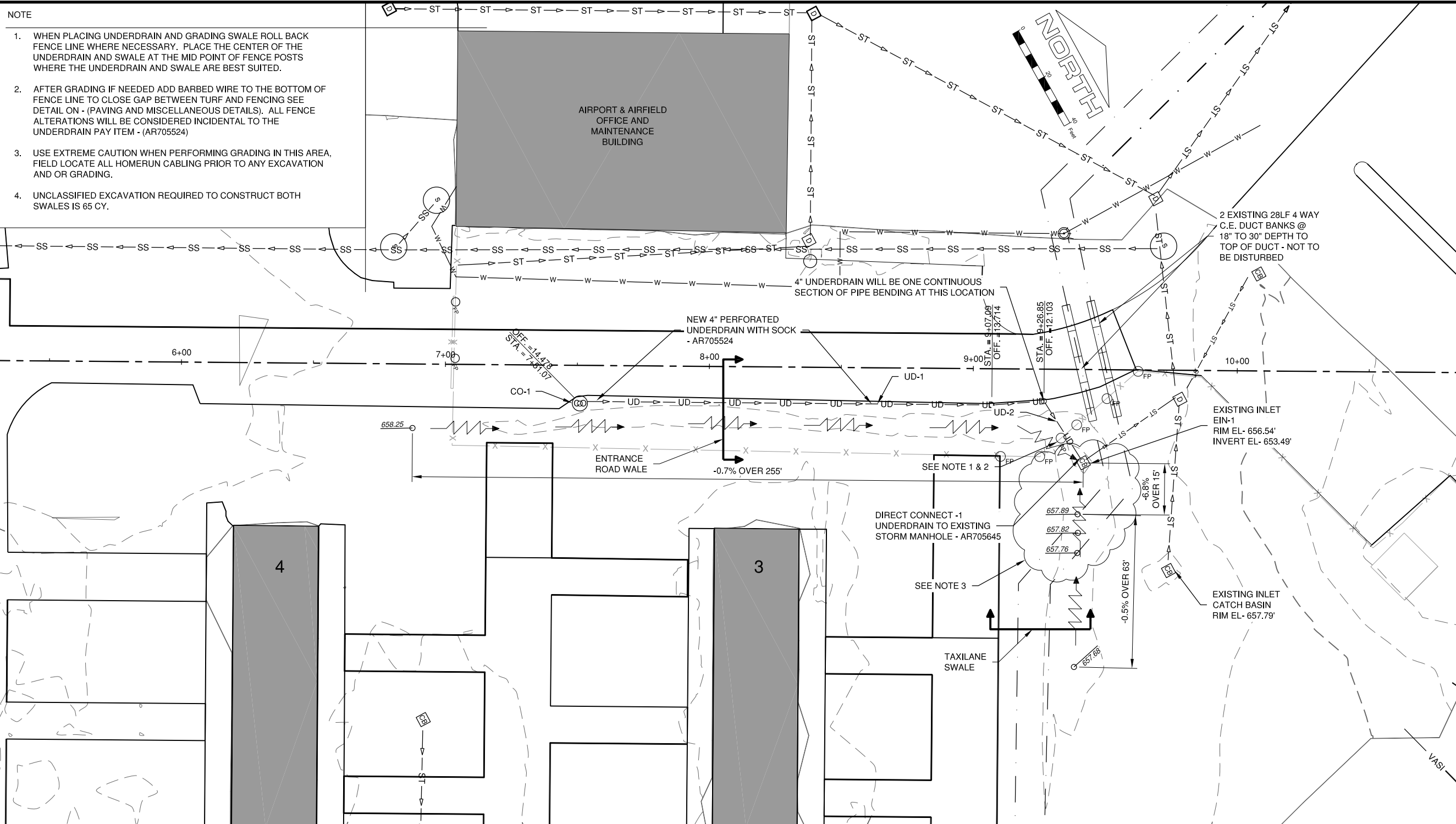
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DANVILLE, ILLINOIS



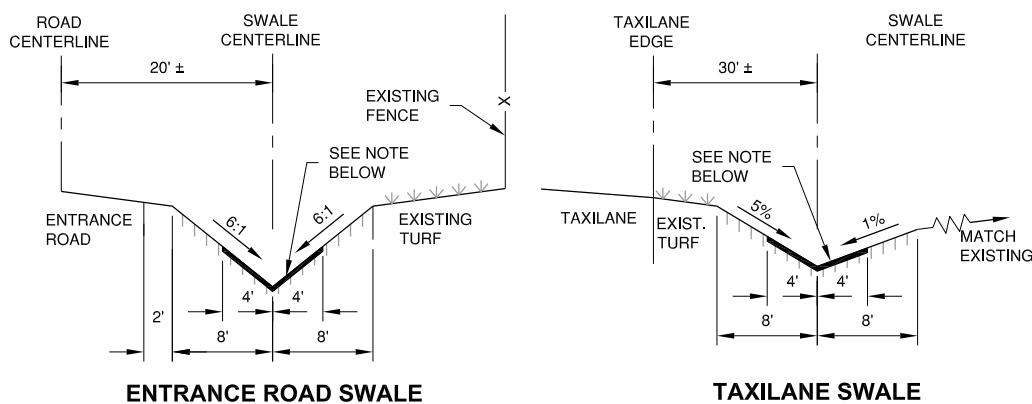
LEGEND

- ST — ST — EXISTING STORM SEWER
- CB EXISTING INLET
- D EXISTING MANHOLE
- CC PROPOSED CLEANOUT
- UD — UD — PROPOSED UNDERDRAIN
- x x x x EXISTING FENCE LINE
- - - - - EXISTING HOME RUN CABLING
- EXISTING CABLING MARKERS
- SWALES GRADING
- 657.25 EXISTING ELEVATION
- FP FENCE POST

| PIPE | MATERIAL | UP STR | DOWN STR | UP INV | DOWN INV | LENGTH | SLOPE | NOTES |
|------|---------------|--------|----------|--------|----------|--------|-------|----------|
| UD-1 | 4" PERFORATED | CO-1 | BEND | 656.5 | 654.76 | 176 | 1.0% | AR705524 |
| UD-2 | 4" PERFORATED | BEND | EIN-1 | 654.76 | 654.24 | 26 | 2.0% | AR705524 |

| STRUCTURE | RIM | INVERT | STATION | OFFSET | PIPE IN | PIPE OUT | NOTES |
|-----------|-------|--------|---------|--------|---------|----------|----------|
| CO-1 | 658.2 | 656.7 | 7+51.07 | 14.48 | UD-1 | UD-1 | AR705640 |

| CONNECTION | RECVING SIZE | PIPE IN | RECVING STRUCTURE | RECVING PIPE INVERT | RECVING STRUCTURE STATION | RECVING STRUCTURE OFFSET | NOTES |
|--------------------|--------------|---------|-------------------|---------------------|---------------------------|--------------------------|----------|
| DIRECT CONNECT - 1 | 4" | UD-2 | EIN-1 | 654.24 | 9+41.96 | 35.339 | AR705645 |



ENTRANCE ROAD SWALE

TAXILANE SWALE

NOTE:
AFTER GRADING IS COMPLETE COVER AFFECTED
GROUND WITH KNITTED STRAW MAT & SEEDING.

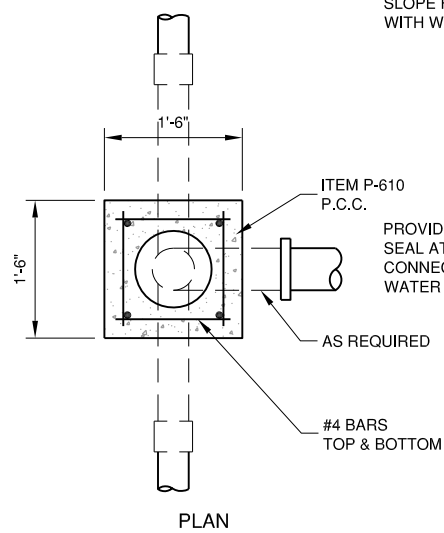
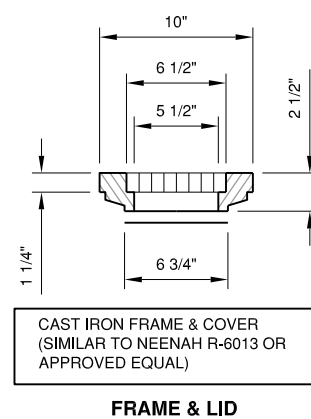
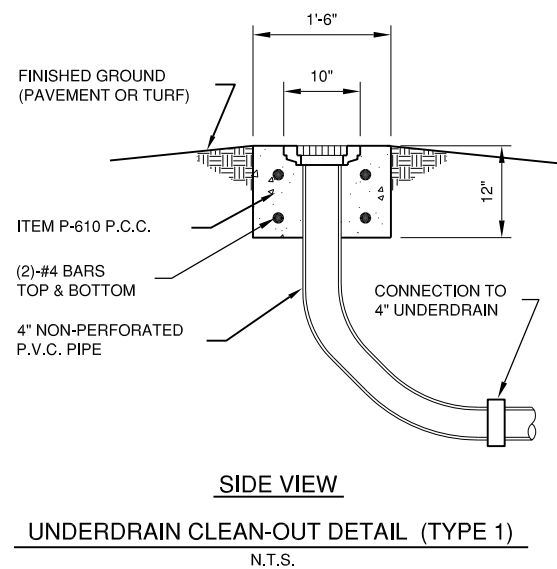
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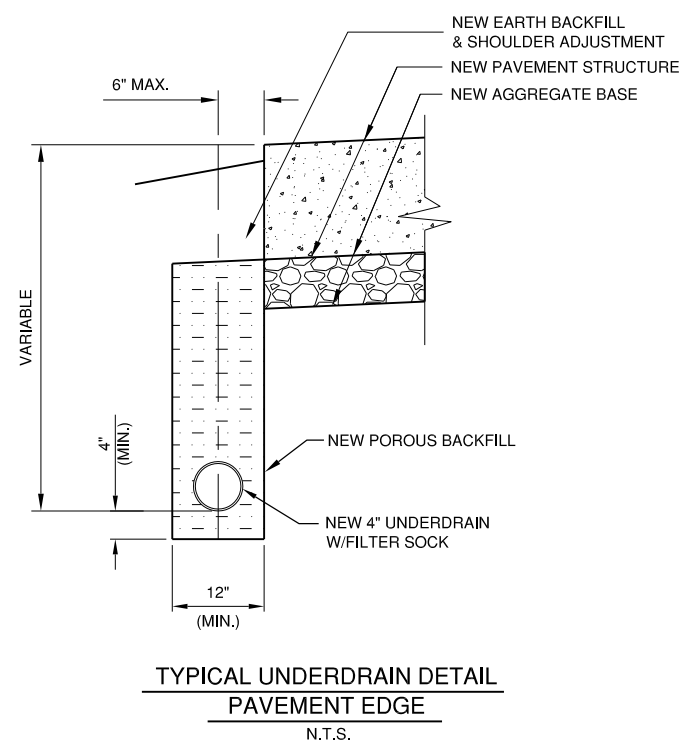
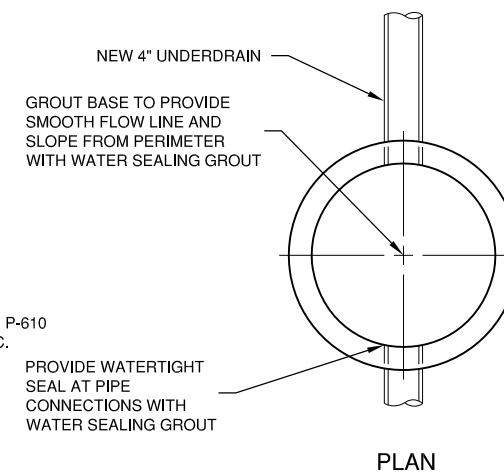
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CMT PROJECT NO: 15042-02-00
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SHEET TITLE
**GRADING &
DRAINAGE PLAN
SHEET**

CU101



UNDERDRAIN CLEAN-OUT DETAILS
N.T.S.



100% SUBMITTAL
MAY 6, 2016

REHABILITATE T-HANGAR
TAXIWAYS AND ASSOCIATED
ACCESS ROAD IMPROVEMENTS

OWNER



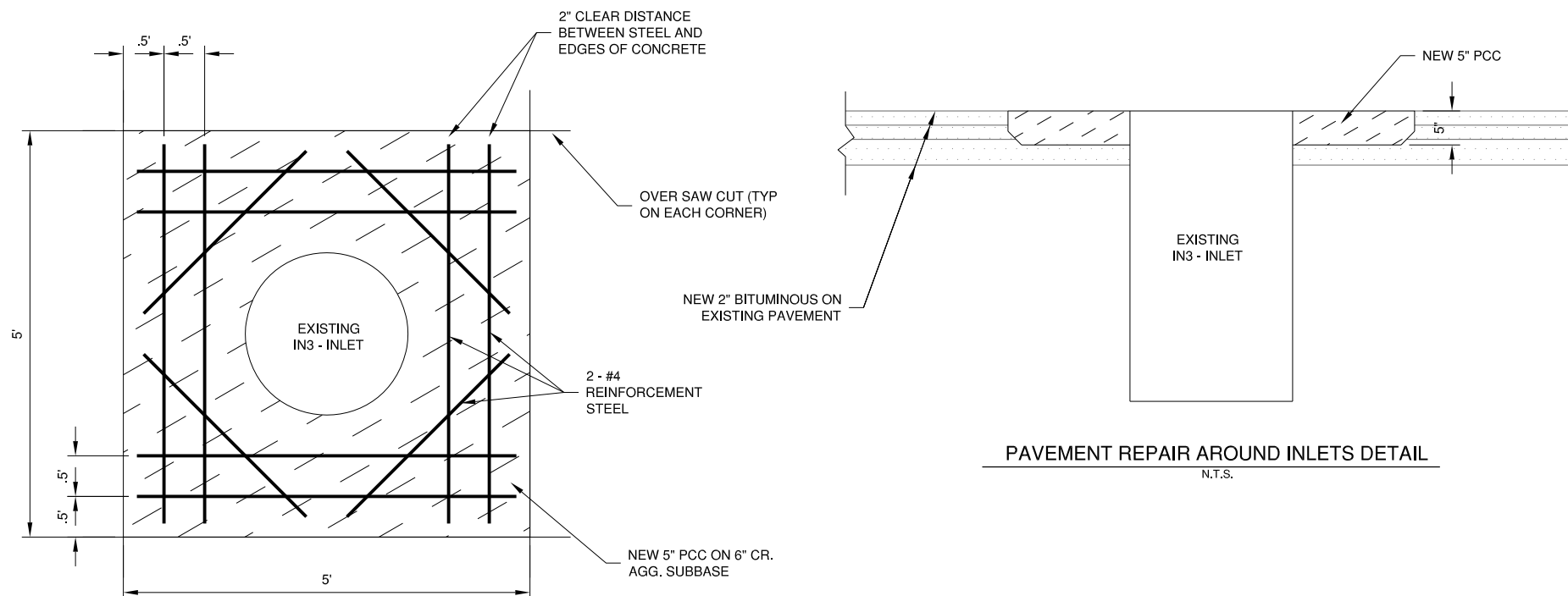
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DANVILLE, ILLINOIS

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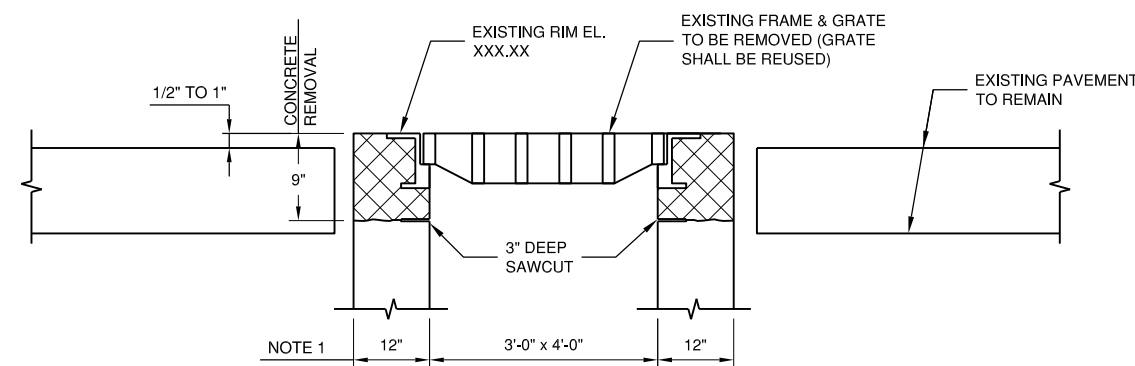
SHEET TITLE
**UNDERDRAIN
DETAILS**

CU501
SHEET 20 OF 26



NOTES

1. PAVEMENT REPAIR AROUND SELECT INLETS ARE CALLED OUT ON PROPOSED IMPROVEMENTS SHEET.
2. PAVING OF NEW 2" BITUMINOUS SURFACE SHALL BE COMPLETED AND FINISHED TO EDGE OF INLET CONCRETE APRON PRIOR TO CONSTRUCTING THIS DETAIL.
3. A 5'x5' SQUARE CENTERED ON THE INLET/MANHOLE SHALL BE SAW CUT AND THE 4" BITUMINOUS LAYER AND EXISTING CONCRETE APRON SHALL BE REMOVED.
4. REMOVE ADDITIONAL 1" OF EXISTING AGGREGATE SUBBASE. USE CARE TO NOT UNDERMINE BITUMINOUS PAVEMENT SURFACE.
5. PLACE EXPANSION JOINT MATERIAL AROUND INLET FRAME.
6. PLACE STEEL AS SHOWN IN THE DETAIL AND PLACE 5" PCC LAYER PER ITEM 610. REBAR SHALL BE PLACED AT A DEPTH OF T/2, T = THICKNESS OF PCC PAVEMENT. KEEP 2" CLEAR DISTANCE BETWEEN REBAR AND EDGES OF PCC PAVEMENT.
7. ENSURE THAT GRADES ARE MET ALONG EXISTING EDGES AND INLET WHILE FINISHING THE CONCRETE.
8. AFTER CONSTRUCTING THE CONCRETE LAYER, SAW CUT THE BITUMINOUS PAVEMENT ALONG THE EDGE OF THE CONCRETE. DIMENSIONS OF SAW CUT ARE SHOWN IN THE DETAIL.
9. PLACE BACKER ROD AND SEAL WITH JOINT SEALANT PER ITEM 605. DETAIL OF JOINT IS SHOWN ON THIS SHEET. JOINT SHALL BE SAWED IN BITUMINOUS PAVEMENT ADJACENT TO THE PCC PAVEMENT.
10. ALL ITEMS ABOVE SHALL BE INCLUDED IN (AR501911) PAY ITEM



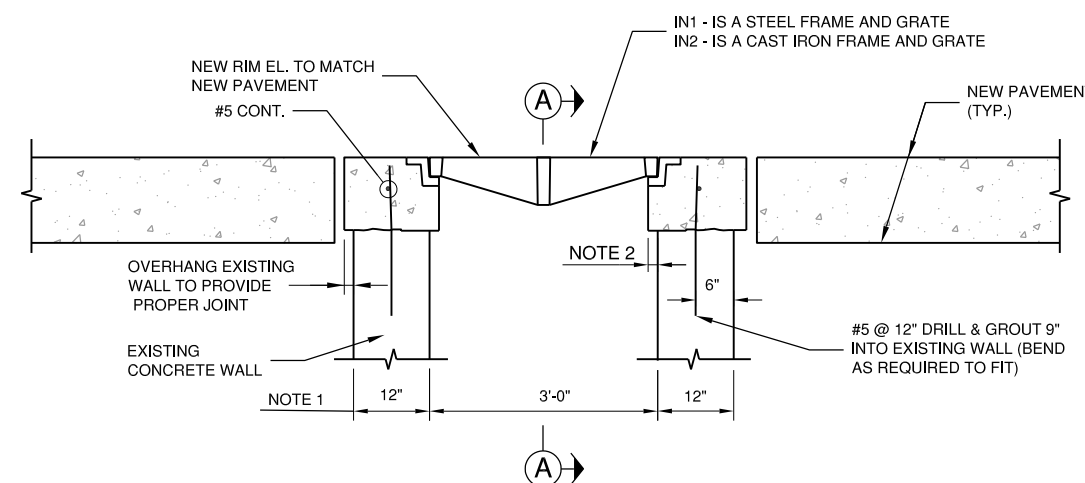
NOTES

1. EXISTING STRUCTURE DIMENSIONS ARE TAKEN FROM RECORD DRAWINGS. ALL DIMENSIONS SHALL BE MEASURED IN FIELD BY CONTRACTOR PRIOR TO SUBMITTING SHOP DRAWINGS.
2. NEW CONCRETE WALL REPLACEMENT SHALL BE TAPERED OR OVERHANG EXISTING WALL TO ACCOMMODATE NEW GRATE FRAME.
3. EXISTING CONCRETE SURFACES WHICH ARE TO RECEIVE NEW CONCRETE SHALL BE CLEANED PRIOR TO APPLYING BONDING AGENT.
4. ALL ITEMS SHOWN IN THIS DETAIL SHALL BE PAID FOR UNDER ITEM AR751940 - ADJUST INLET.

| INLET ADJUSTMENT ELEVATION | | |
|----------------------------|--------------------|--------------------|
| | EXISTING ELEVATION | PROPOSED ELEVATION |
| IN1 | 657.63 | 657.63 |
| IN2 | 657.64 | 657.64 |

REUSE

CONTRACTOR SHALL REUSE THE EXISTING GRATE. NEW FRAME SHALL BE SIZED TO MEET INLET FRAME. PAY ITEM NUMBER (AR751940) INLET ADJUST



REPLACEMENT

100% SUBMITTAL
MAY 6, 2016

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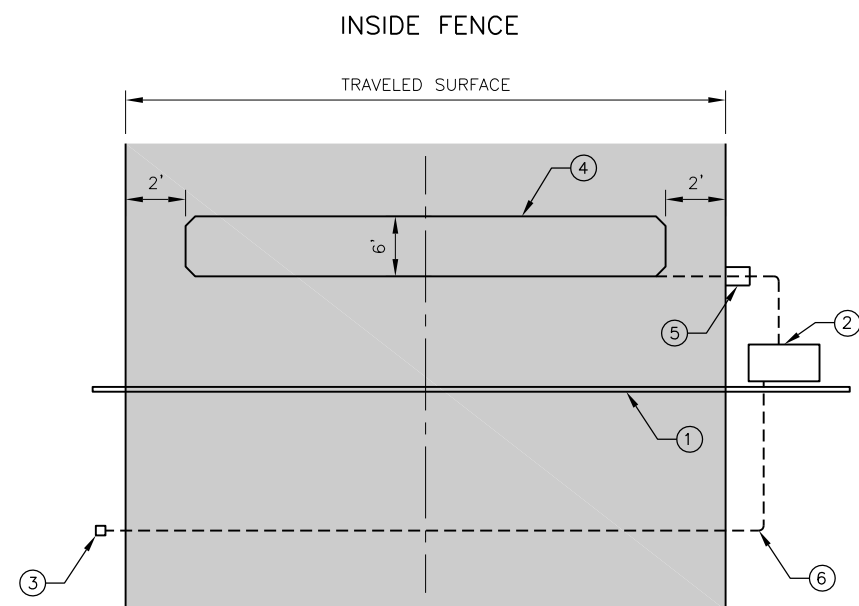


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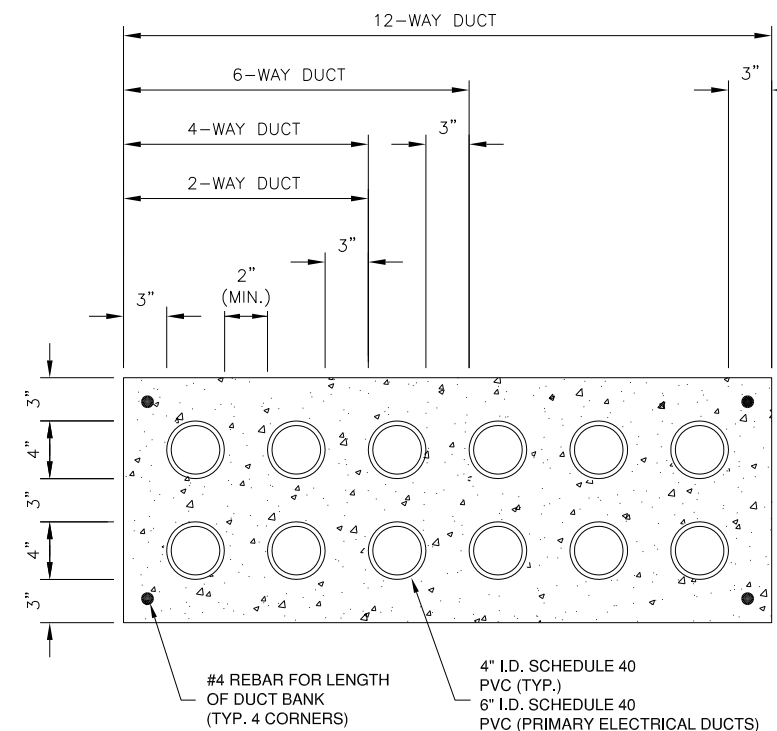
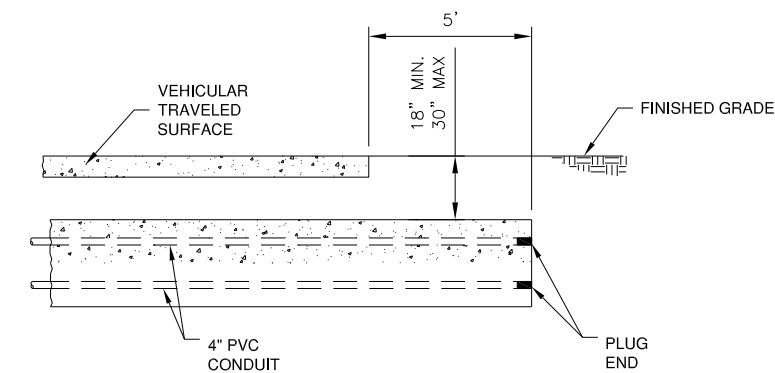
SHEET TITLE
INLET DETAILS



GATE PLAN
N.T.S.

GATE PLAN NOTES

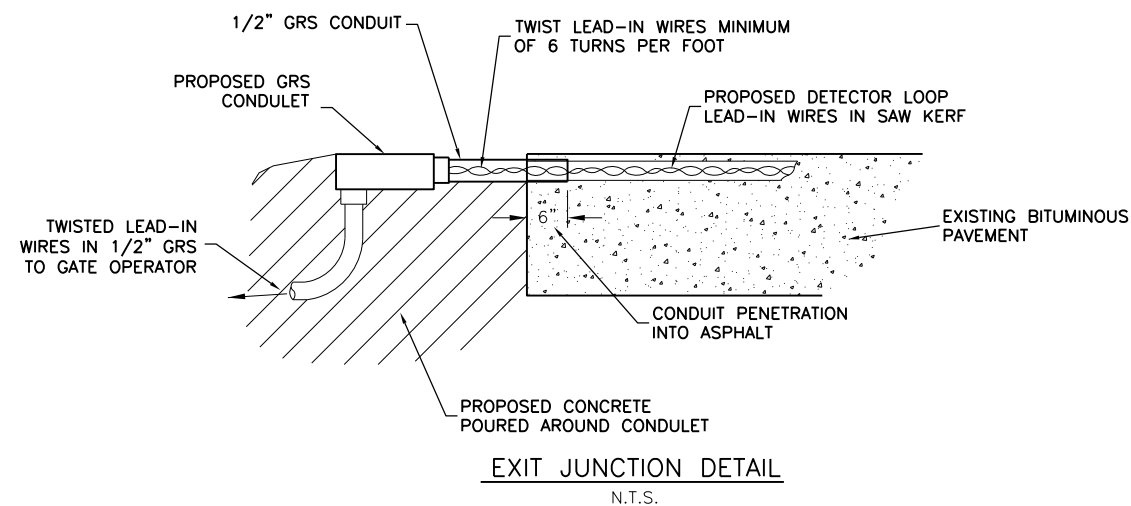
1. EXISTING CANTILEVER SLIDE GATE TO REMAIN IN SERVICE.
2. EXISTING MOTORIZED GATE OPERATOR TO REMAIN IN SERVICE
3. EXISTING KEYPAD TO REMAIN IN PLACE
4. NEW DETECTOR LOOP 10' FROM GATE, CONNECTED TO EXISTING LOOP DETECTOR IN GATE OPERATOR.
5. LOOP LEAD, TWO #16 XHHW, MINIMUM 6-TWISTS PER FOOT. SEE EXIT JUNCTION DETAIL.
6. EXISTING GATE CONTROL CABLE TO REMAIN UNDISTURBED DURING CONSTRUCTION. CONTRACTOR SHALL VERIFY LOCATION AND DEPTH PRIOR TO REMOVAL OF PAVEMENT.
7. ALL ITEMS ABOVE AND SHOWN IN DETAIL SHALL BE PAID FOR UNDER ITEM AR162570 - DETECTOR LOOP.



DUCT BANK DETAILS
N.T.S.

DUCT BANK NOTES

1. DIMENSIONS SHOWN ARE MINIMUM.
2. TOP OF CONCRETE ENCASEMENT TO BE NOT LESS THAN 18" BELOW FINISHED SUBGRADE.
3. DUCT CONCRETE TO BE ITEM 610 STRUCTURAL P.C.C. CONCRETE.
4. ALL DUCT WILL BE 4" INSIDE DIAMETER.
5. LOCATIONS SHOWN ARE APPROXIMATE. DUCT BANKS TO BE INSTALLED AT LOCATIONS DESIGNATED BY THE ENGINEER.
6. CONTRACTOR WILL INSTALL DUCT BANKS AT A DEPTH WHICH WILL NOT CONFLICT WITH ELEVATION SENSITIVE UTILITIES.
7. A PULL WIRE MUST BE PROVIDED IN EACH UNUSED CONDUIT. UNUSED DUCT MUST BE SEALED WITH PVC PLUGS/CAPS TO THE SATISFACTION OF THE ENGINEER.
8. DUCT BANKS MUST EXTEND A MINIMUM OF 5' BEYOND THE PROPOSED EDGE OF PAVEMENT.



EXIT JUNCTION DETAIL
N.T.S.

100% SUBMITTAL
MAY 6, 2016

REHABILITATE T-HANGAR TAXIWAYS AND ASSOCIATED ACCESS ROAD IMPROVEMENTS

OWNER



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| CMT PROJECT NO: 15042-02-00 |
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ELECTRICAL DETAILS

EL501

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MAY 6, 2016

REHABILITATE T-HANGAR TAXIWAYS AND ASSOCIATED ACCESS ROAD IMPROVEMENTS

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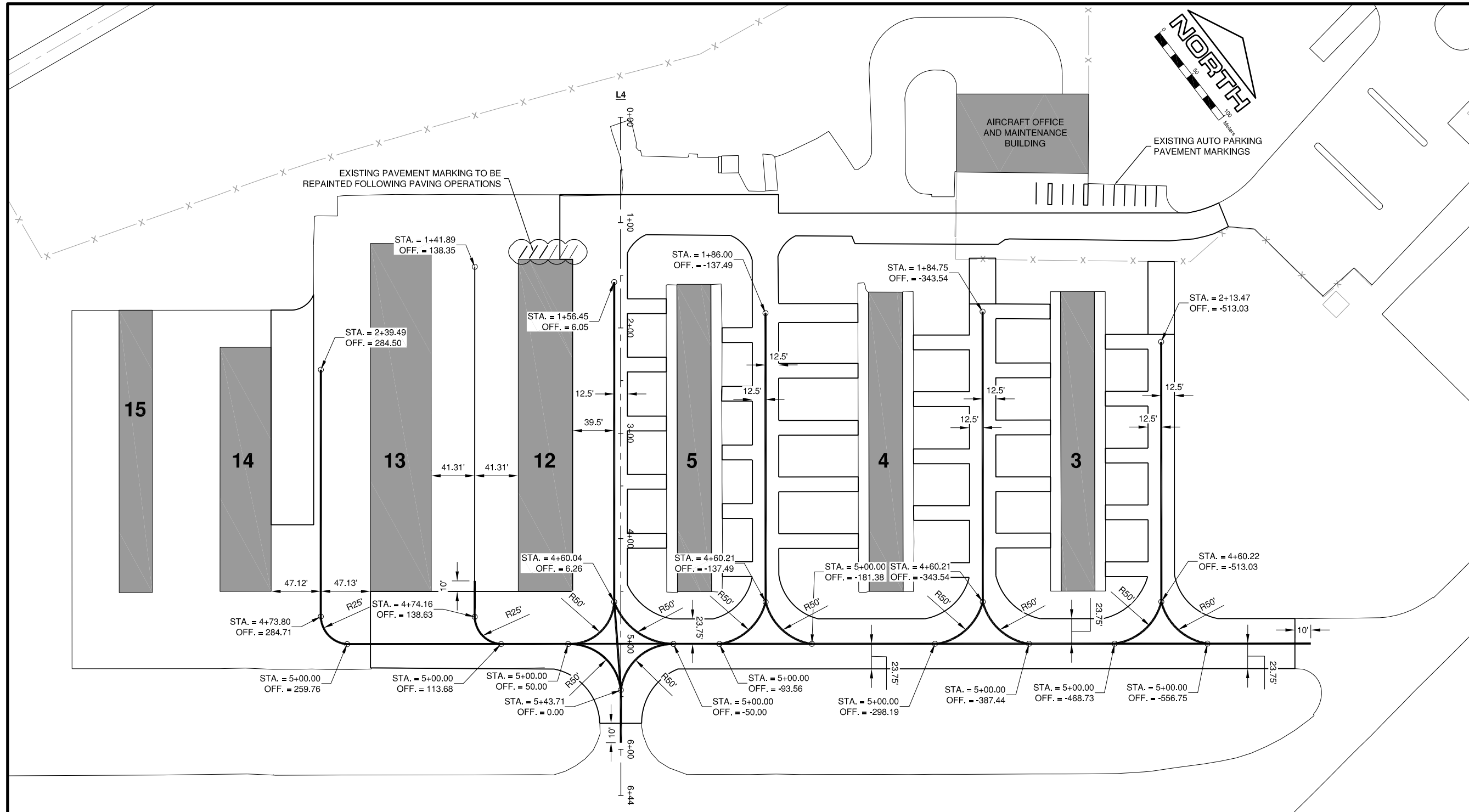
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**MARKING PLAN &
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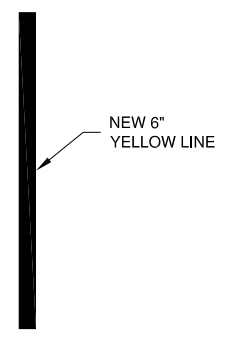
CM101

SHEET 24 OF 26



LEGEND

- EXISTING MARKINGS
- NEW 6" TAXIWAY CENTERLINE MARKINGS



**HANGER TAXI LANES AND
TAXI LANE CENTERLINE
CONTINUOUS
N.T.S.**

MARKING NOTE

1. ALL NEW AIRFIELD MARKING SHALL BE 6" WIDE AND CONTAIN REFLECTIVE MEDIA.

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Date: Tuesday, May 10, 2016 8:32:47 PM

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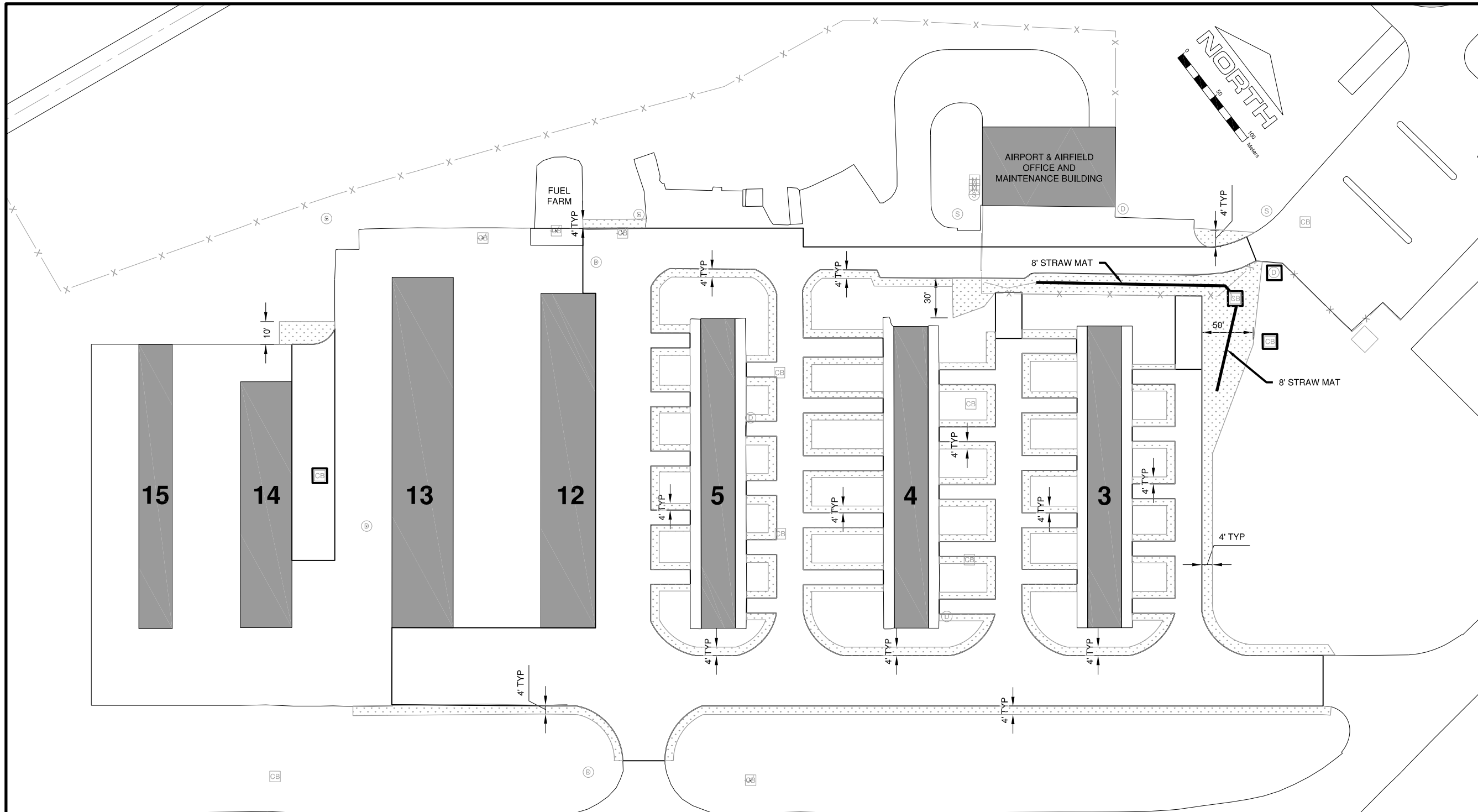


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DANVILLE, ILLINOIS**

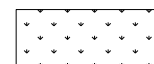
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TURFING PLAN



LEGEND



NEW SEEDING (901) AND
NEW KNITTED STRAW MAT (908) LIMITS



INLET PROTECTION

NOTES

1. THE RESIDENT ENGINEER SHALL DETERMINE THE LIMITS AND NEED OF SEEDING/KNITTED STRAW MAT PER THE ACTUAL DISTURBANCE TO THE TURF ADJACENT TO THE MILLING AND PAVEMENT REMOVAL/CONSTRUCTION.
2. INLET PROTECTION SHOWN SHALL BE INCIDENTAL TO THE CONTRACT.

100% SUBMITTAL
MAY 6, 2016

REHABILITATE T-HANGAR
TAXIWAYS AND ASSOCIATED
ACCESS ROAD IMPROVEMENTS

OWNER

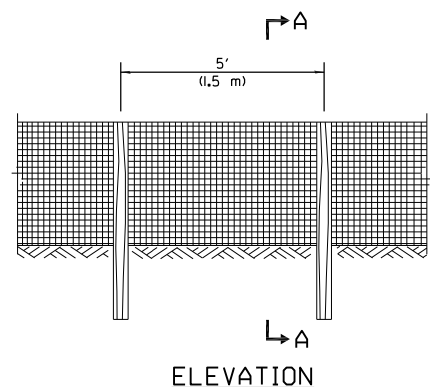


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DANVILLE, ILLINOIS

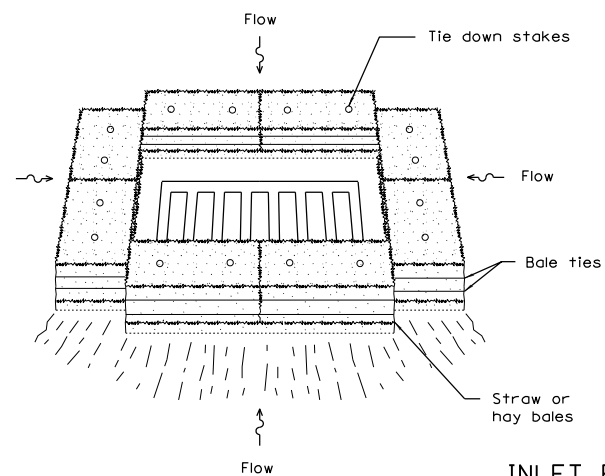
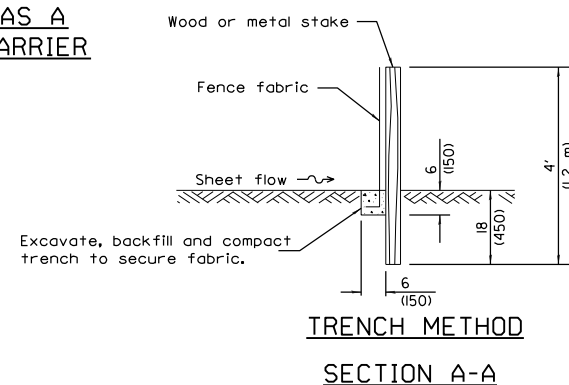
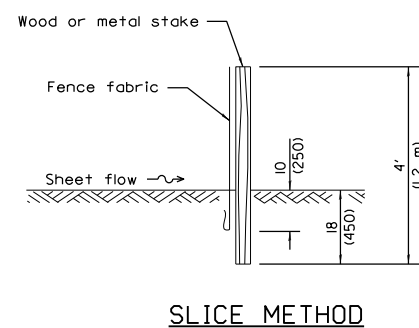
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SHEET TITLE
**EROSION CONTROL
DETAILS**



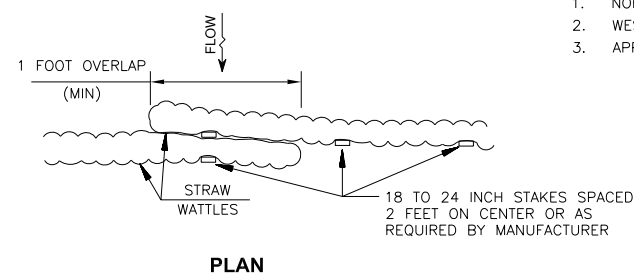
**SILT FILTER FENCE AS A
PERIMETER EROSION BARRIER**



INLET PROTECTION

MANUFACTURERS:

1. NORTH AMERICAN GREEN
2. WESTERN EXCELSIOR
3. APPROVED EQUAL



STRAW WATTLES
N.T.S.

