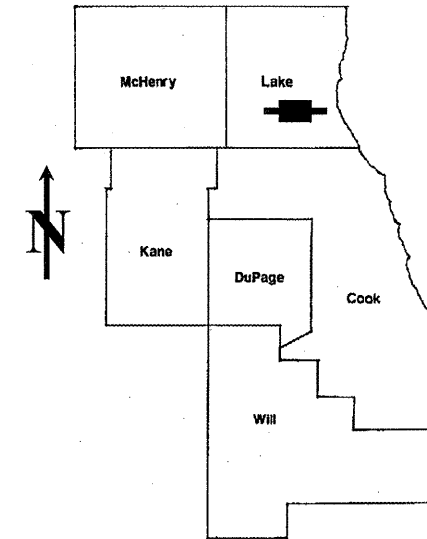


ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NUMBER
VARIOUS	2010-021PP	LAKE & MCHENRY	25	1

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**  
**DISTRICT ONE**  
**PROPOSED HIGHWAY PLANS**

**CONTRACT NO. 60K40**

D-91-479-10



LOCATION OF IMPROVEMENT INDICATED THUS:

FOR INDEX OF SHEETS SEE SHEET 2

VARIOUS ROUTES  
 SECTION: 2010-021PP  
 VARIOUS LOCATIONS IN LAKE COUNTY  
 PCC PAVEMENT PATCHING

C-91-479-10

**CONTRACT NO. 60K40**

DISTRICT ONE - DESIGN - PLAN PREPARATION ENGINEER:  
 KEN ENG / (847) 705-4247

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**  
 SUBMITTED: March 22 2010  
  
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER  
May 7 2010  
Scott E. Stitt, P.E.  
 acting ENGINEER OF DESIGN AND ENVIRONMENT  
May 7 2010  
Christine M. Beedler  
 DIRECTOR, DIVISION OF HIGHWAYS

**PRINTED BY THE AUTHORITY**  
**OF THE STATE OF ILLINOIS**

**J.U.L.I.E.: JOINT UTILITY LOCATION**  
**INFORMATION FOR EXCAVATION**  
**(312) 744-7000**

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

<u>SHEET NO.</u>	<u>DESCRIPTION</u>	<u>STANDARD NO.</u>	<u>DESCRIPTION</u>
1	TITLE SHEET	000001-05	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	420001-07	PAVEMENT JOINTS
3	SUMMARY OF QUANTITIES	420701-02	PAVEMENT FABRIC
4	GENERAL LOCATION MAP	421001-02	BAR REINFORCEMENT FOR CRC PAVEMENT
5	SUMMARY OF PATCHING SCHEDULE	442001-04	CLASS A PATCHES
6-7	PATCHING SCHEDULE	442101-07	CLASS B PATCHES
8-9	TEMPORARY DETOUR PLAN (PETERSON RD. RAMPS)	602306-02	INLET - TYPE B
10-12	TEMPORARY DETOUR PLAN (CLAVEY RD. RAMPS)	701400-04	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
13	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	701401-05	LANE CLOSURE, FREEWAY/EXPRESSWAY
14	FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS	701426-03	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS
15	FREEWAY SINGLE AND MULTI-LANE WEAVE	701446-01	TWO LANE CLOSURE, FREEWAY/EXPRESSWAY
16	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS	701901-01	TRAFFIC CONTROL DEVICES
17	TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)		
18-19	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS		
20	DISTRICT ONE TYPICAL PAVEMENT MARKINGS		
21	FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES		
22	TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC)		
23	DETOUR SIGNING FOR CLOSING STATE HIGHWAYS		
24	ARTERIAL ROAD INFORMATION SIGN		
25	STANDARD TRAFFIC SIGNAL DESIGN DETAILS		

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE (OR TOLLWAY) PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (OR ISTHA)

WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT MS. DEBBIE HANLON, AREA TRAFFIC FIELD ENGINEER AT (847) 438-2300 MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

THE EXISTING ROADWAY TYPICAL SECTION IS ASSUMED TO BE 9 INCHES OF PORTLAND CEMENT CONCRETE (PCC) PAVEMENT.

NO PATCHING IS TO BE DONE WITH IN 50 FEET OF ANY RAILROAD.

THE MINIMUM CLASS A PATCH DIMENSIONS SHALL BE A LENGTH OF 4.5 FEET AND A WIDTH THAT INCLUDES HALF THE WIDTH OF THE TRAVEL WAY. THE MINIMUM CLASS B PATCH DIMENSIONS SHALL BE A LENGTH OF 6 FEET AND A WIDTH THAT INCLUDES THE FULL WIDTH OF THE TRAVEL WAY.

FOR ALL PATCHING ON US 41, CLAVEY RD. RAMPS AND PETERSON RD. RAMPS, CLASS PP-5 PORTLAND CEMENT CONCRETE IS TO BE USED.

JOINT SEALING FOR CLASS B PATCHES IS TO BE REPLACED WITH A SOLID PLASTIC BOND BREAKER (1/8" X T/3"; WHERE T IS EQUAL TO THE THICKNESS OF THE PATCH). THE COST OF THE SOLID PLASTIC BOND BREAKER IS TO BE INCLUDED IN THE COST OF THE CLASS B PATCH.

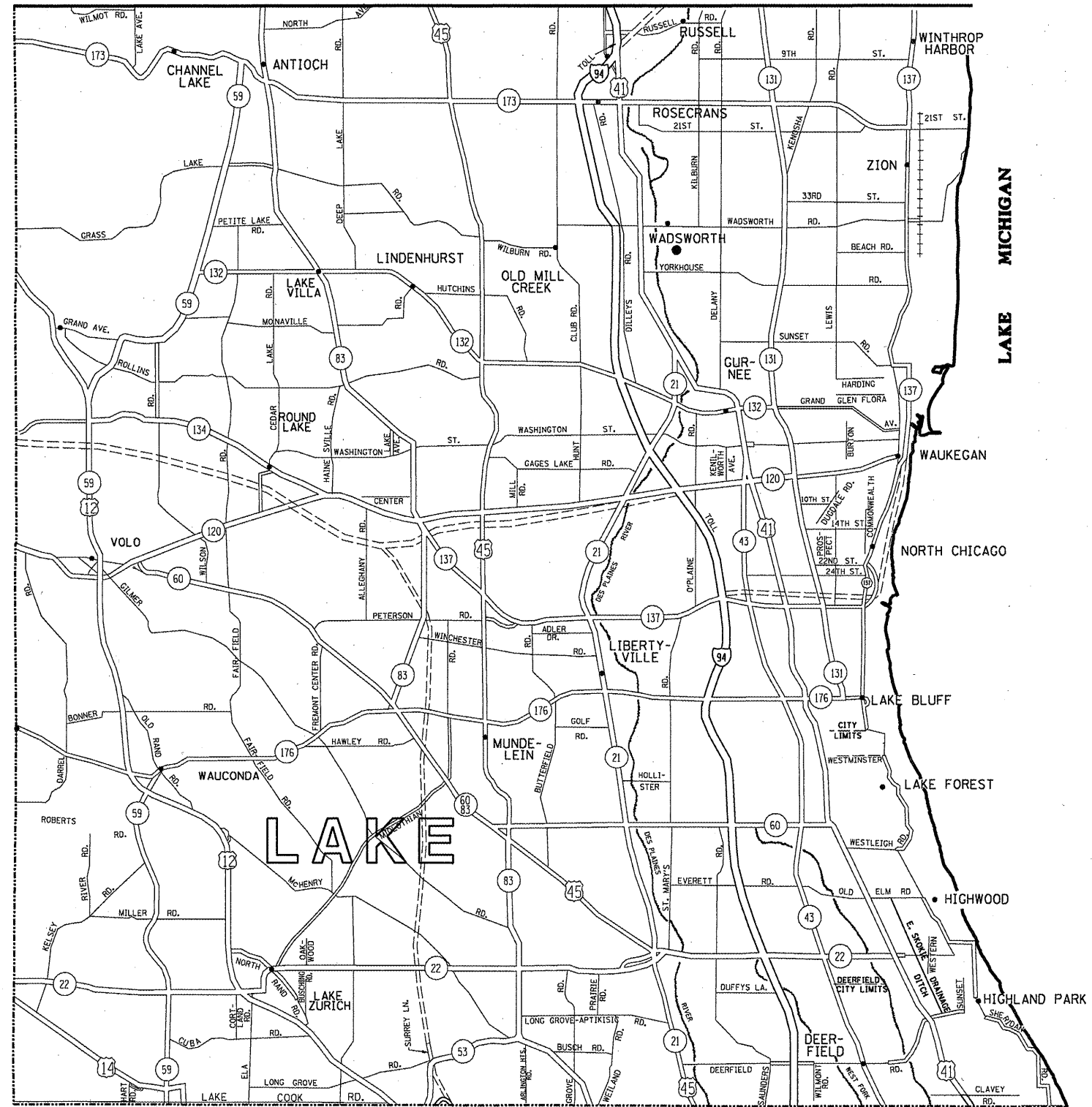
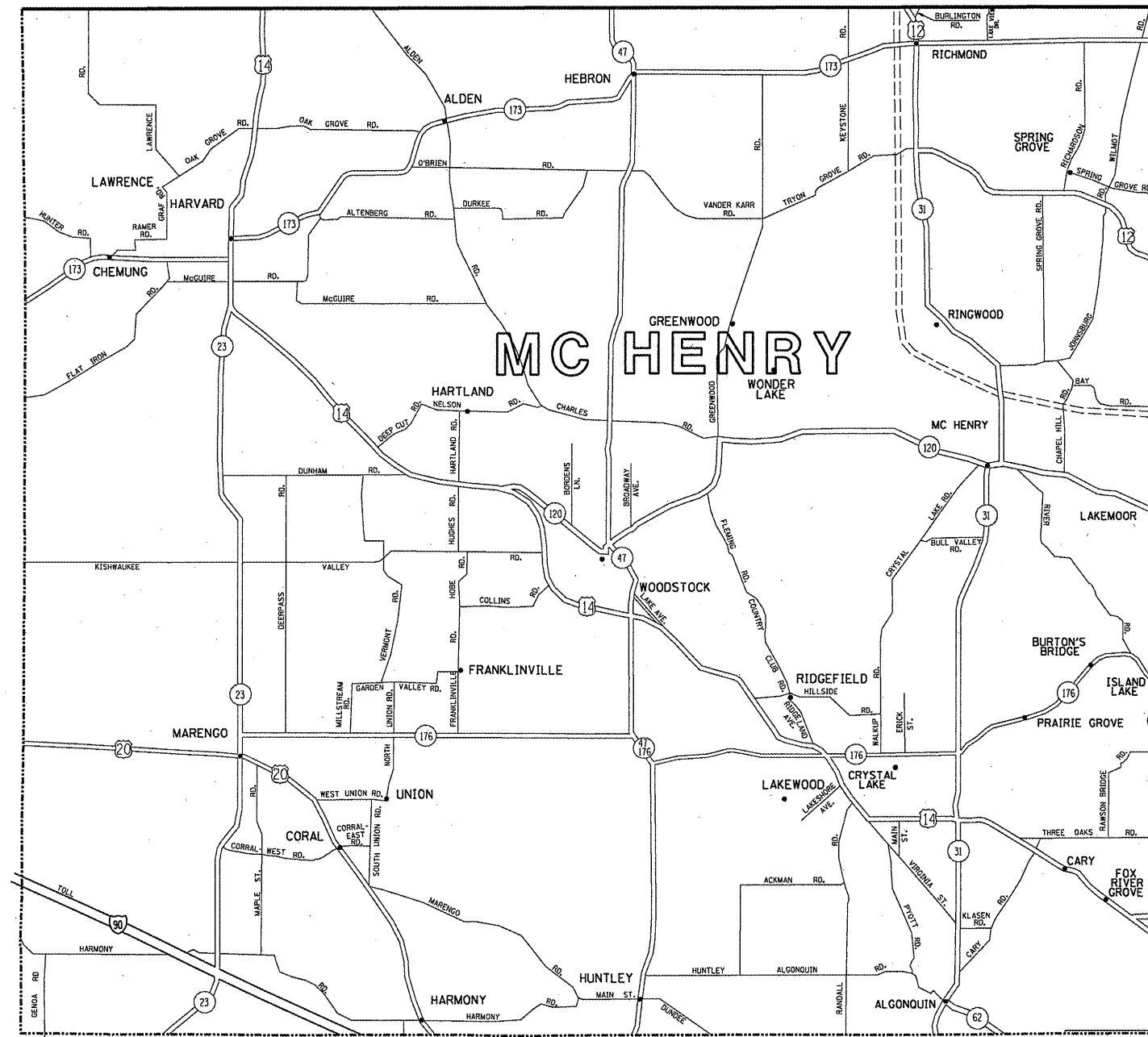
NO ROUTES WERE IDENTIFIED TO BE PATCHED IN MCHENRY COUNTY.

FILE NAME =	USER NAME = wjgreendp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
at\pwwork\PW1D0T\WJLGREENDP\0183633\0183633.dgn	DESIGNED -	REVISED -	VAR.			2010-021PP	L.K. & MCH.	25	2	
PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60K40							
PLOT DATE = 4/12/2010	DATE -	REVISED -	SCALE: SHEET NO. OF SHEETS STA. TO STA. FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT							

SUMMARY OF QUANTITIES			URBAN 100% STATE		CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	LAKE CD.						CODE NO	ITEM	UNIT	TOTAL QUANTITIES						
				1000															
42101300	PROTECTIVE COAT	SO YD	1583	1583															
<del>44001700</del>	<del>COMBINATION CONCRETE CURB AND CUTTER REMOVAL AND REPLACEMENT</del>	<del>FOOT</del>	<del>50</del>	<del>50</del>															
44200541	CLASS A PATCHES, TYPE II, 9 INCH	SO YD	56	56															
44200545	CLASS A PATCHES, TYPE III, 9 INCH	SO YD	92	92															
44200956	CLASS B PATCHES, TYPE II, 9 INCH	SO YD	614	614															
44200962	CLASS B PATCHES, TYPE III, 9 INCH	SO YD	304	304															
44200964	CLASS B PATCHES, TYPE IV, 9 INCH	SO YD	505	505															
44213000	PATCHING REINFORCEMENT	SO YD	148	148															
44213100	PAVEMENT FABRIC	SO YD	809	809															
44213200	SAW CUTS	FOOT	4352	4352															
60262700	INLETS TO BE RECONSTRUCTED	EACH	1	1															
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6															
67100100	MOBILIZATION	L SUM	1	1															
70102550	TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR	EACH	6	6															
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	1	1															
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	1840	1840															
* 78008220	POLYUREA PAVEMENT MARKING TYPE I - LINE 5"	FOOT	25	25															
* 78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	25	25															
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	50	50															
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	50	50															
X0322256	TEMPORARY INFORMATION SIGNING	SO FT	104	104															
X0325702	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1															
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1															
Z0017202	DOWEL BARS 1 1/2"	EACH	1820	1820															
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1															
Z0075310	TIE BARS 3/4"	EACH	190	190															
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	50	50															
60603000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-G.12	FOOT	50	50															
	* SPECIALTY ITEM																		

Rev.

FILE NAME =	USER NAME = wlgreendp	DESIGNED -	REVISIONS -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\WILGREENDP\0183633\Designdp	DRAWN -	REVISIONS -	VAR.			2010-021PP	LK. & MCH.	25	3	
PLOT SCALE = 100.0000' / 1" IN.	CHECKED -	REVISIONS -	CONTRACT NO. 60K40							
PLOT DATE = 4/12/2010	DATE -	REVISIONS -	SCALE:			SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT		



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		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GENERAL LOCATION MAP - LAKE AND MCHENRY COUNTIES**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE. VAR.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2010-021PP	LK. & MCH.	25	4
CONTRACT NO. 60K40				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PCC PATCHING SUMMARY - LAKE AND MCHENRY COUNTIES	CLASS A	CLASS A	CLASS B	CLASS B	CLASS B
	PATCHES, 9"	PATCHES, 9"	PATCHES, 9"	PATCHES, 9"	PATCHES, 9"
	TYPE II (SY)	TYPE III (SY)	TYPE II (SY)	TYPE III (SY)	TYPE IV (SY)
US 41 AT CLAVEY RD. RAMPS - LAKE COUNTY			614	304	401
US 41 MAINLINE (NORTH OF LAKE COOK RD. AND SOUTH OF CLAVEY RD.) - LAKE COUNTY	56	92			
PETERSON RD. RAMPS AT IL 137 - LAKE COUNTY					104
MCHENRY COUNTY - NO PATCHING					
<b>SUMMARY TOTALS:</b>	<b>56</b>	<b>92</b>	<b>614</b>	<b>304</b>	<b>505</b>
	(SY)	(SY)	(SY)	(SY)	(SY)

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED -
c:\pw_work\PW1001\WILGREENDP\0183633\design.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 4/9/2010	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF PATCHING SCHEDULE  
LAKE AND MCHENRY COUNTIES**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2010-021PP	LK. & MCH.	25	5
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60K40	



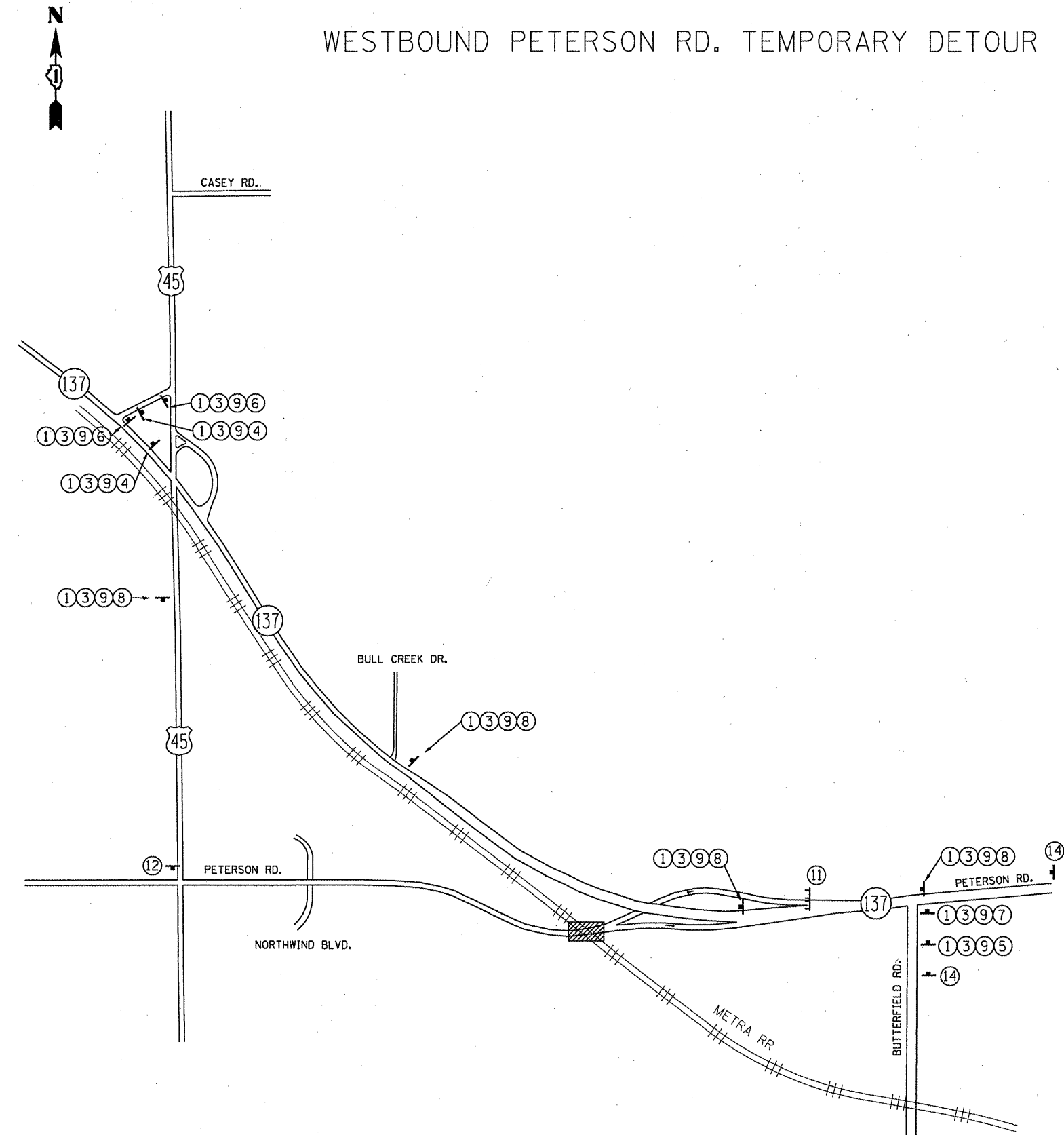
ROUTE: US 41 Mainline (N/O Lake Cook Rd. & S/O Clavey Rd.) - CRC Pavement				PATCHING = CLASS A			
CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
North of Lake Cook Rd.		SB	1	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	1	12	6	72	8
South of Clavey Rd.		SB	3	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	1	12	15	180	20
		NB	2	12	15	180	20
		NB	3	12	15	180	20
		NB	3	12	6	72	8
<b>TOTALS:</b>						<b>111</b>	<b>148</b>
						<b>FT</b>	<b>SY</b>

ROUTE: Peterson Rd. Ramps to IL 137 - PCC				PATCHING = CLASS B			
CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
EB Peterson Rd. Ramp under Railroad		EB	1	14	60	840	93
WB Peterson Rd. Ramp under Railroad		WB	1	14	30	420	47
<b>TOTALS:</b>						<b>90</b>	<b>104</b>
						<b>FT</b>	<b>SY</b>

NOTE: PETERSON RD. RAMPS TO BE DETOURED DURING CONSTRUCTION.

# WESTBOUND PETERSON RD. TEMPORARY DETOUR

## DETOUR LEGEND



- ① [DETOUR] (M1-7-219)
- ② [EAST] (M3-2-219)
- ③ [WEST] (M3-4-219)
- ④ [RIGHT TURN] (M5-1R-2115)
- ⑤ [LEFT TURN] (M5-1L-2115)
- ⑥ [RIGHT ARROW] (M6-1R-2115)
- ⑦ [LEFT ARROW] (M6-1L-2115)
- ⑧ [UP ARROW] (M6-1L-2115)
- ⑨ [PETERSON ROAD]
- ⑩ [ROAD CLOSED TO THRU TRAFFIC] R11-4-6030 (WITH 2 TYPE III BARICADES)
- ⑪ [ROAD CLOSED] R11-2-4830 (WITH 2 TYPE III BARICADES)
- ⑫ [END DETOUR] M4-8A-3618
- ⑬ [ROAD CLOSED AHEAD] C24-14-4848
- ⑭ [DETOUR AHEAD] C24-13-4848
- ⑮ [ROAD CLOSED 500 FT] C24-12-4848
- [Hatched Box] WORK AREA (WESTBOUND LANE)

**NOTES:**

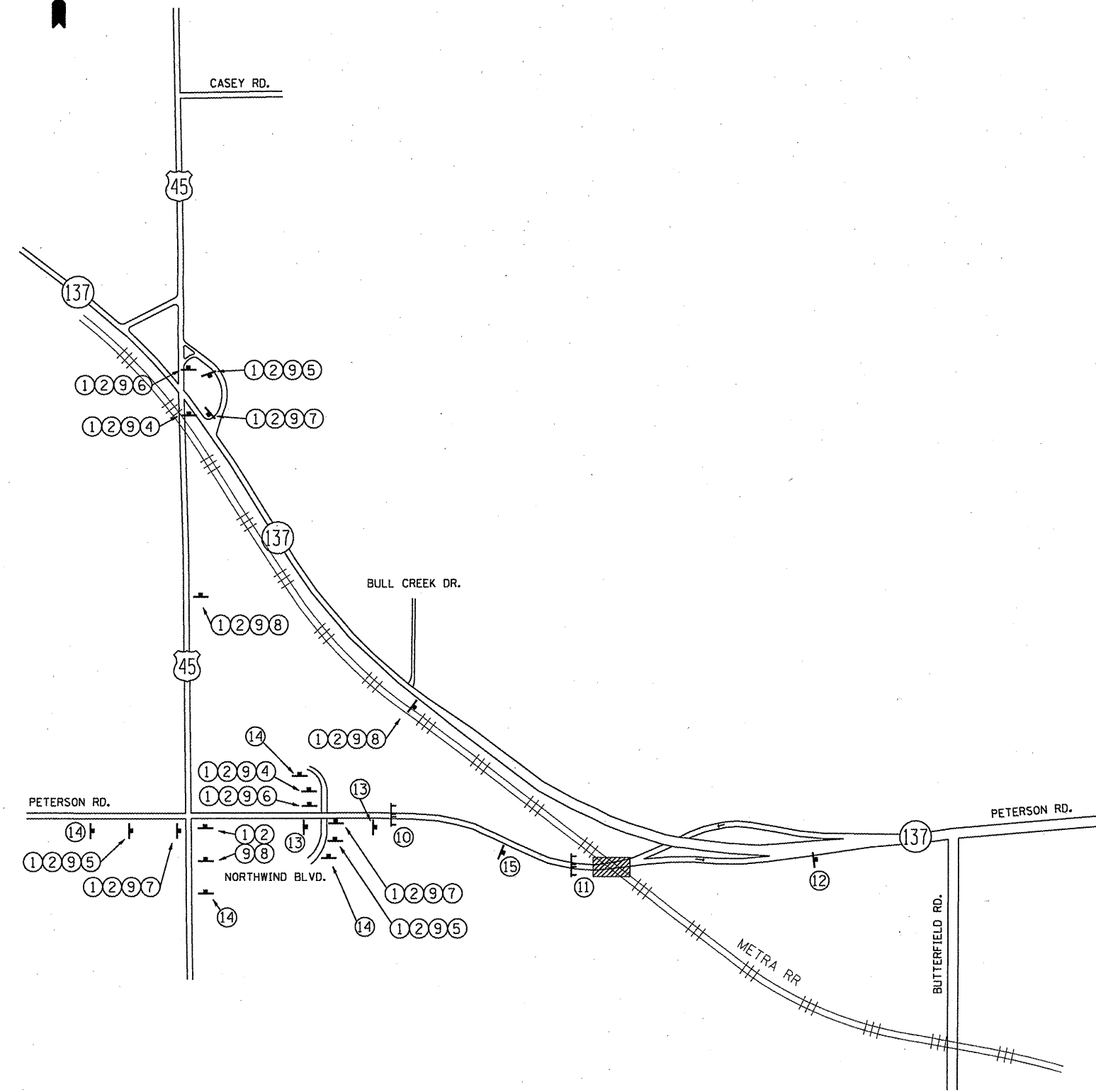
SEE TYPICAL MARKINGS FOR CLOSING STATE HIGHWAYS DETAIL (TC-21) FOR SIGN SPACING.

EXACT SIGN LOCATIONS TO BE APPROVED BY THE ENGINEER.

FILE NAME =	USER NAME = wlgreendp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TEMPORARY DETOUR PLAN PETERSON RD. RAMP (WESTBOUND)</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pwwork\PWIDOT\WILGREENDP\d0183633\Design.dgn	PLOT SCALE = 100.0000' / IN.	DRAWN -	REVISED -			VAR.	2010-021PP	L.K. & MCH.	25	8	
PLOT DATE = 4/12/2010	DATE -	CHECKED -	REVISED -			CONTRACT NO. 60K40					
		DATE -	REVISED -			FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT					
SCALE:		SHEET NO. OF SHEETS		STA. TO STA.							



# EASTBOUND PETERSON RD. TEMPORARY DETOUR



## DETOUR LEGEND

- ① [DETOUR] (M1-7-219)
- ② [EAST] (M3-2-219)
- ③ [WEST] (M3-4-219)
- ④ [RIGHT TURN] (M5-1R-2115)
- ⑤ [LEFT TURN] (M5-1L-2115)
- ⑥ [RIGHT ARROW] (M6-1R-2115)
- ⑦ [LEFT ARROW] (M6-1L-2115)
- ⑧ [UP ARROW] (M6-1L-2115)
- ⑨ [PETERSON ROAD]
- ⑩ [ROAD CLOSED TO THRU TRAFFIC] R11-4-6030 (WITH 2 TYPE III BARICADES)
- ⑪ [ROAD CLOSED] R11-2-4830 (WITH 2 TYPE III BARICADES)
- ⑫ [END DETOUR] M4-8A-3618
- ⑬ [ROAD CLOSED AHEAD] C24-14-4848
- ⑭ [DETOUR AHEAD] C24-13-4848
- ⑮ [ROAD CLOSED 500 FT] C24-12-4848
- [Hatched Box] WORK AREA (EASTBOUND LANE)

**NOTES:**  
 SEE TYPICAL MARKINGS FOR CLOSING STATE HIGHWAYS DETAIL (TC-21) FOR SIGN SPACING.  
 EXACT SIGN LOCATIONS TO BE APPROVED BY THE ENGINEER.

FILE NAME =	USER NAME = wjgreendp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TEMPORARY DETOUR PLAN PETERSON RD. RAMP (EASTBOUND)</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pwwork\pwwid\DOT\WILGREENDP\d0183633\d0183633.dgn		DRAWN -	REVISED -			VAR.	2010-021PP	L.K. & MCH.	25	9	
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 60K40					
PLOT DATE = 4/12/2010		DATE -	REVISED -			FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT					

## DETOUR LEGEND

- |   |   |  |
|---|---|--|
| <p>① <span style="border: 1px solid black; padding: 2px;">DETOUR</span> (M1-7-219)</p> <p>② <span style="border: 1px solid black; padding: 2px;">NORTH</span> (M3-1-2412)</p> <p>③ <span style="border: 1px solid black; padding: 2px;">SOUTH</span> (M3-3-2412)</p> <p>④ <span style="border: 1px solid black; padding: 2px;"></span> (M5-1R-2115)</p> <p>④A <span style="border: 1px solid black; padding: 2px;"></span> (M5-2-2115)</p> <p>④B <span style="border: 1px solid black; padding: 2px;"></span> (M6-2-2115)</p> <p>⑤ <span style="border: 1px solid black; padding: 2px;"></span> (M5-1L-2115)</p> <p>⑥ <span style="border: 1px solid black; padding: 2px;"></span> (M6-1R-2115)</p> <p>⑦ <span style="border: 1px solid black; padding: 2px;"></span> (M6-1L-2115)</p> <p>⑧ <span style="border: 1px solid black; padding: 2px;"></span> (M6-1L-2115)</p> <p>⑨ <span style="border: 1px solid black; padding: 2px; text-align: center;">CLAVEY<br/>ROAD</span></p> <p>⑩ <span style="border: 1px solid black; padding: 2px; text-align: center;"></span> (M1-50-2424)</p> | <p>⑪ <span style="border: 1px solid black; padding: 2px;">RAMP<br/>CLOSED</span> (R11-2-4830) (WITH 2 TYPE III BARICADES)</p> <p>⑫ <span style="border: 1px solid black; padding: 2px;">END<br/>DETOUR</span> (M4-8A-3618)</p> <p>⑬ <span style="border: 1px solid black; padding: 2px; text-align: center;"></span> (W 20-1(O)-48)</p> <p>⑭ <span style="border: 1px solid black; padding: 2px; text-align: center;"></span> (C24-14-4848)</p> <p>⑮ <span style="border: 1px solid black; padding: 2px; text-align: center;"></span> (C24-13-4848)</p> <p style="text-align: center;"> WORK AREA</p> |  |
|---|---|--|

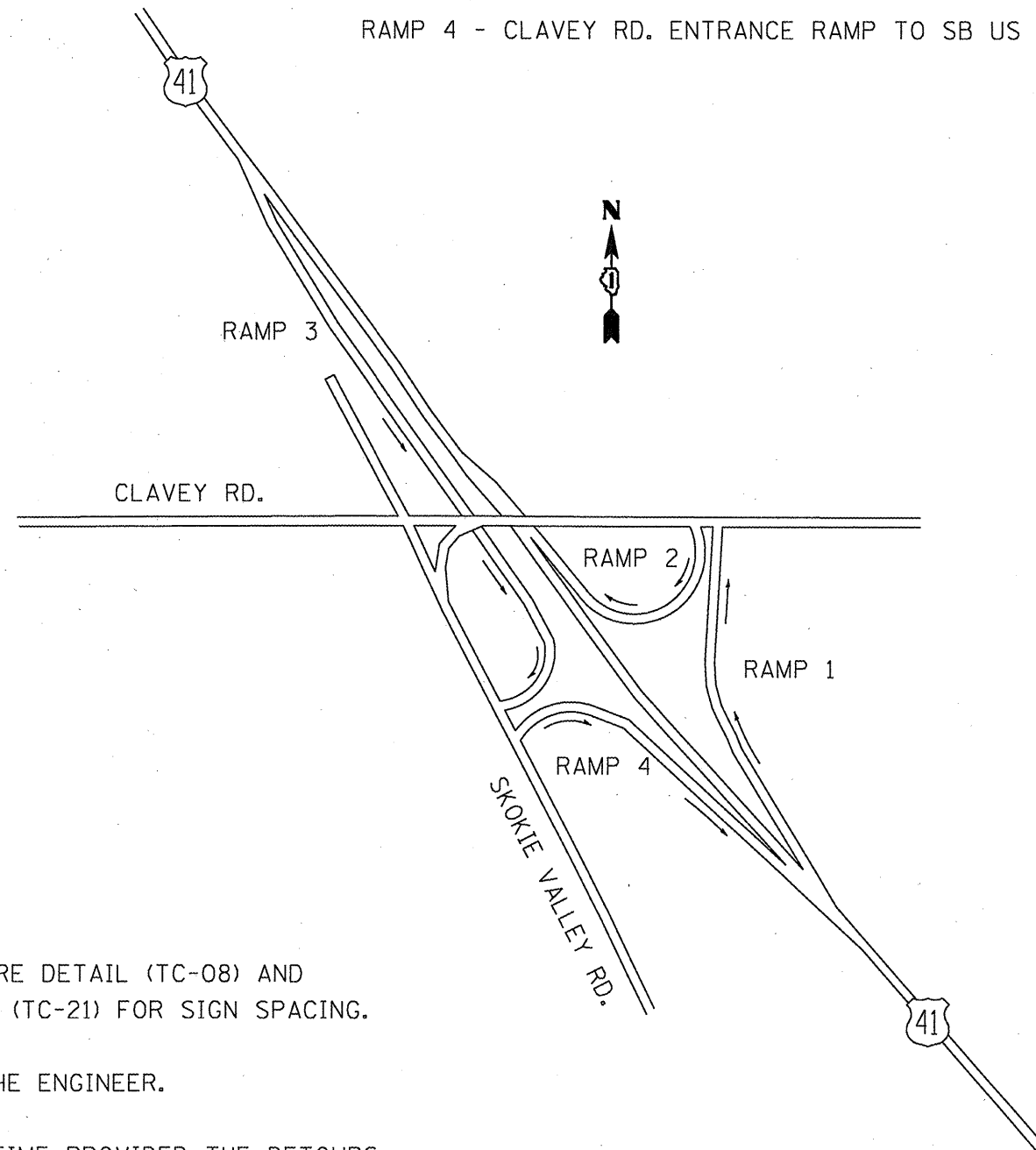
**NOTES:**

SEE FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAIL (TC-08) AND  
DETOUR SIGNING FOR CLOSING STATE HIGHWAYS (TC-21) FOR SIGN SPACING.

EXACT SIGN LOCATIONS TO BE APPROVED BY THE ENGINEER.

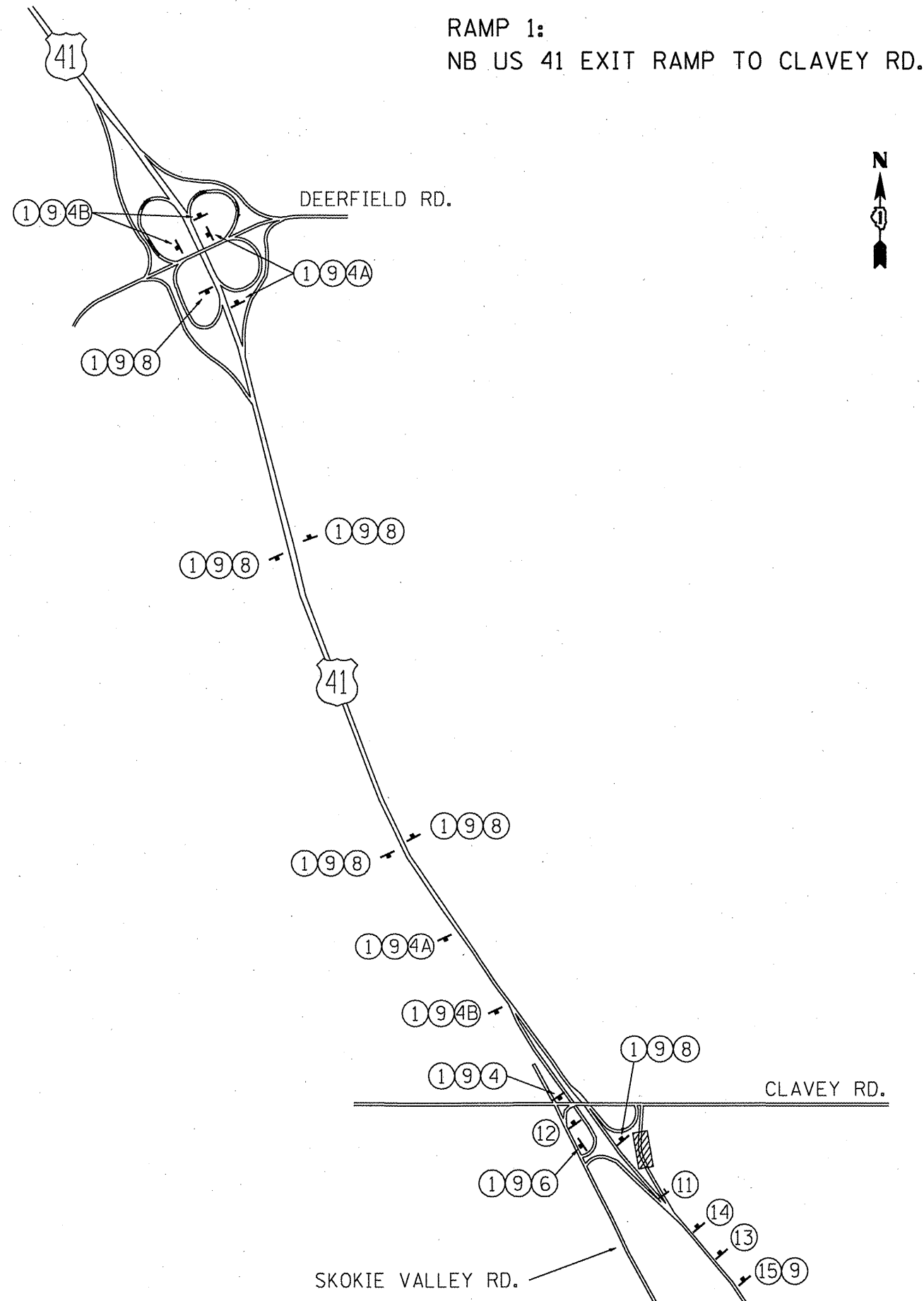
TWO (2) RAMPS MAY BE CLOSED AT THE SAME TIME PROVIDED THE DETOURS  
MOVE TRAFFIC IN OPPOSITE DIRECTIONS.

- RAMP 1 - NB US 41 EXIT TO CLAVEY RD.
- RAMP 2 - CLAVEY RD. ENTRANCE RAMP TO NB US 41
- RAMP 3 - SB US 41 EXIT RAMP TO CLAVEY RD.
- RAMP 4 - CLAVEY RD. ENTRANCE RAMP TO SB US 41

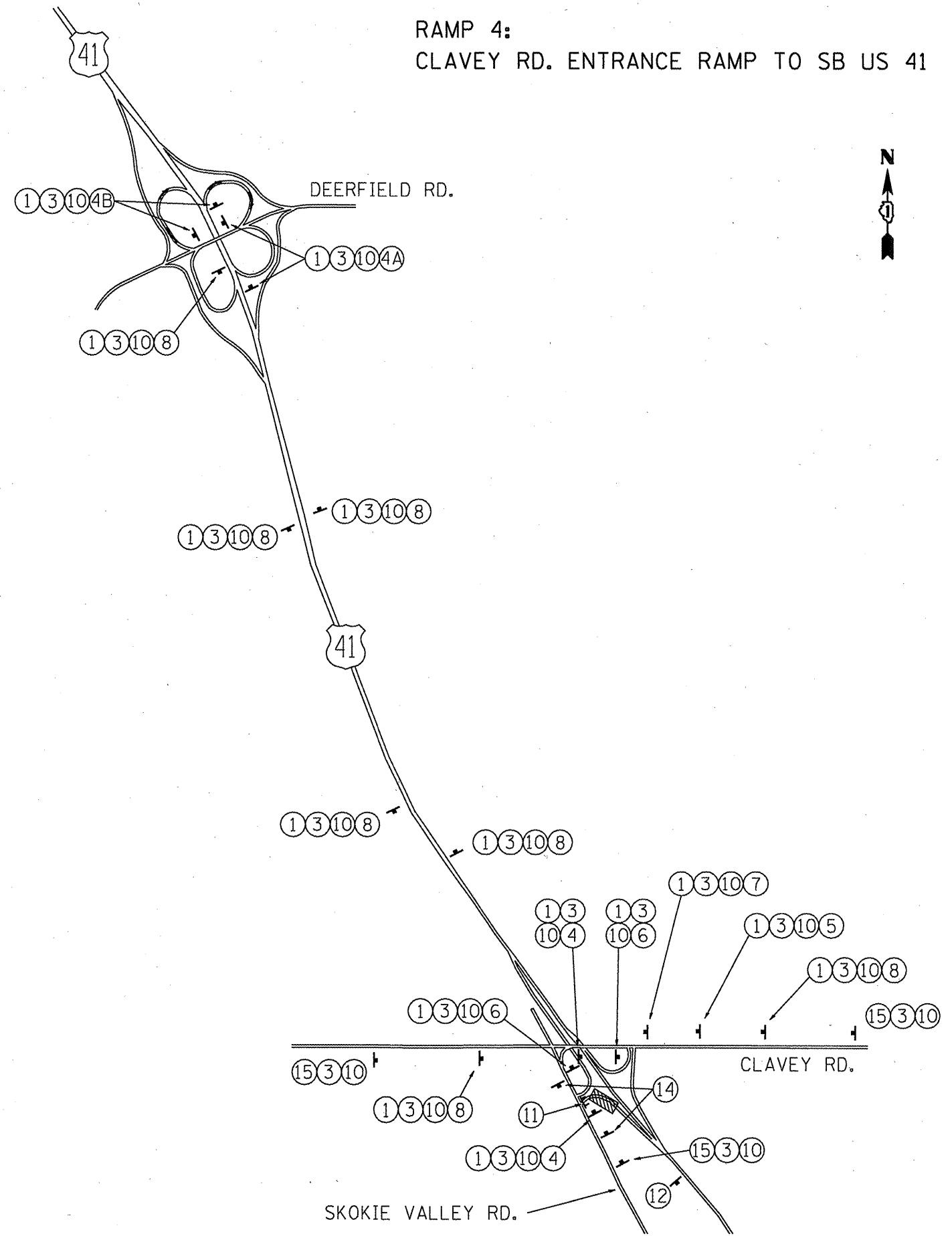


FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TEMPORARY DETOUR PLAN US 41 AT CLAVEY RD. RAMPS</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pwwork\pwwid\WILGREENDP\0183633\0183633.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	ILLINOIS	25	10
		CHECKED -	REVISED -								L.K. & MCH.		
		DATE -	REVISED -								CONTRACT NO. 60K40		
										ILLINOIS FED. AID PROJECT			

RAMP 1:  
NB US 41 EXIT RAMP TO CLAVEY RD.



RAMP 4:  
CLAVEY RD. ENTRANCE RAMP TO SB US 41



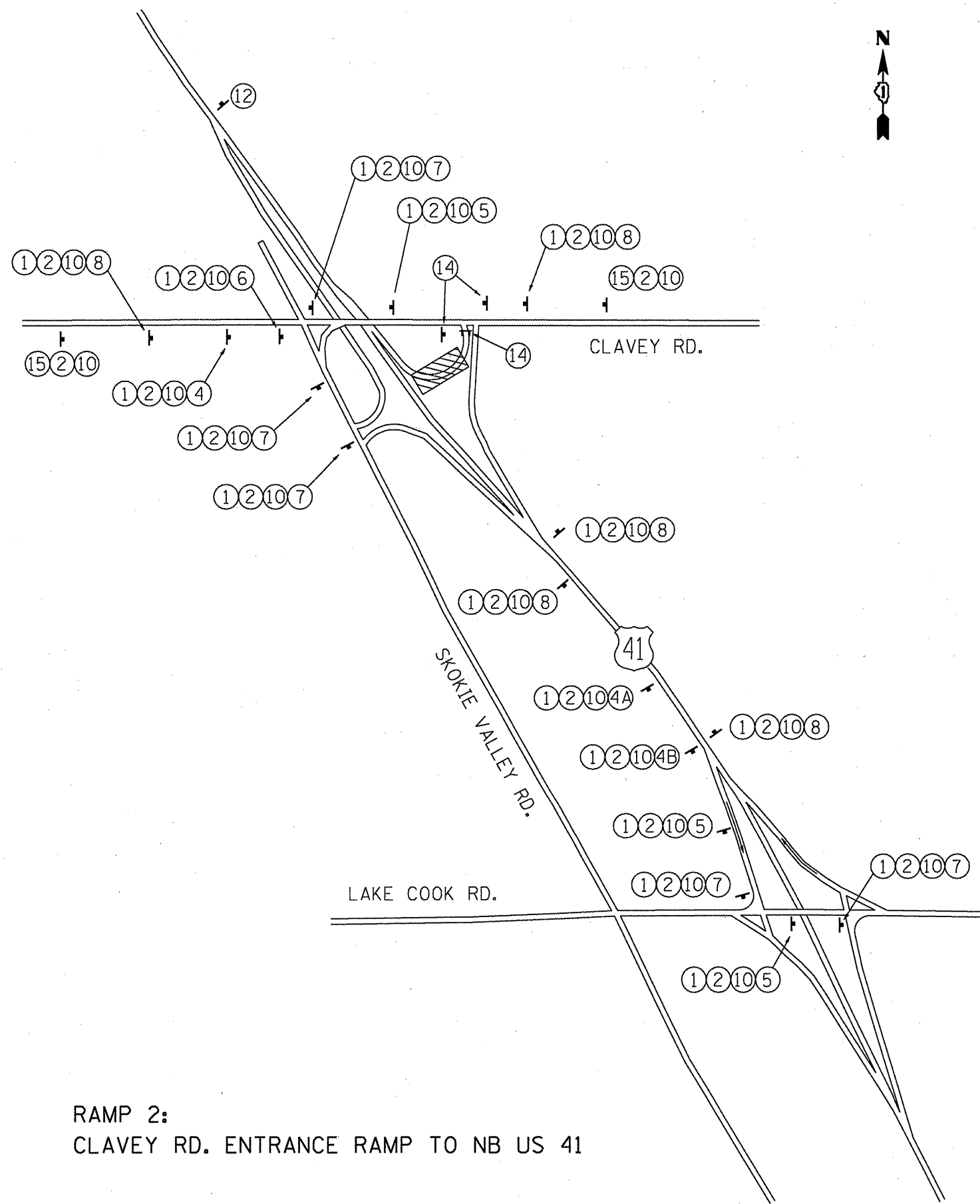
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ca:\pwork\pwidot\WILGREENDP\d0183633\design.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 4/12/2010	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

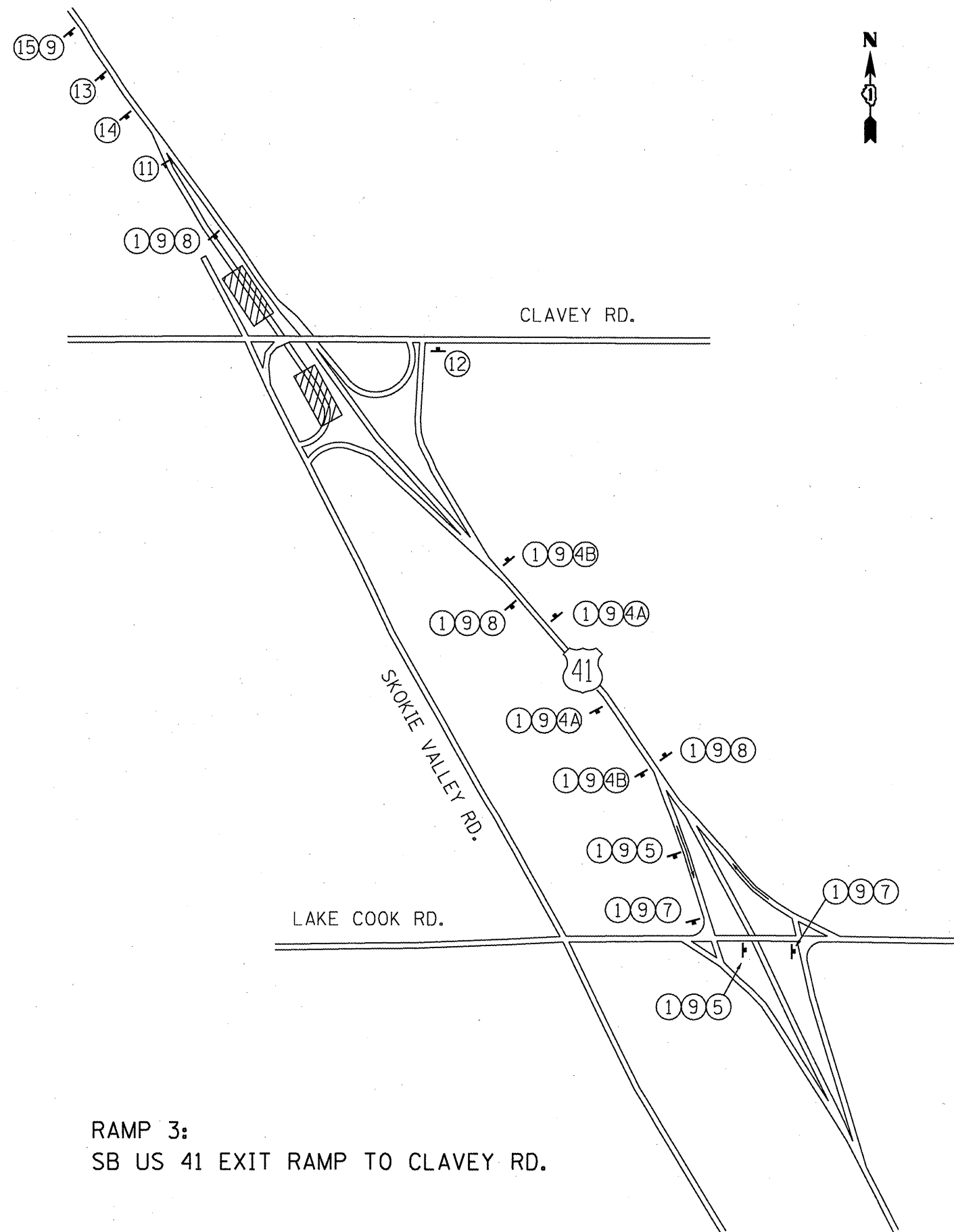
TEMPORARY DETOUR PLAN  
US 41 AT CLAVEY RD. RAMPS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2010-02IPP	L.K. & MCH.	25	11
			CONTRACT NO. 60K40	
ILLINOIS FED. AID PROJECT				

SCALE: SHEET NO. OF SHEETS STA. TO STA.



RAMP 2:  
CLAVEY RD. ENTRANCE RAMP TO NB US 41



RAMP 3:  
SB US 41 EXIT RAMP TO CLAVEY RD.

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED -
ca:\ph_work\PWIDOT\WILGREENDP\d0183633\design.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 4/12/2010	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TEMPORARY DETOUR PLAN  
US 41 AT CLAVEY RD. RAMPS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2010-021PP	LK. & MCH.	25	12
CONTRACT NO. 60K40			ILLINOIS FED. AID PROJECT	

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\* \* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY. SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

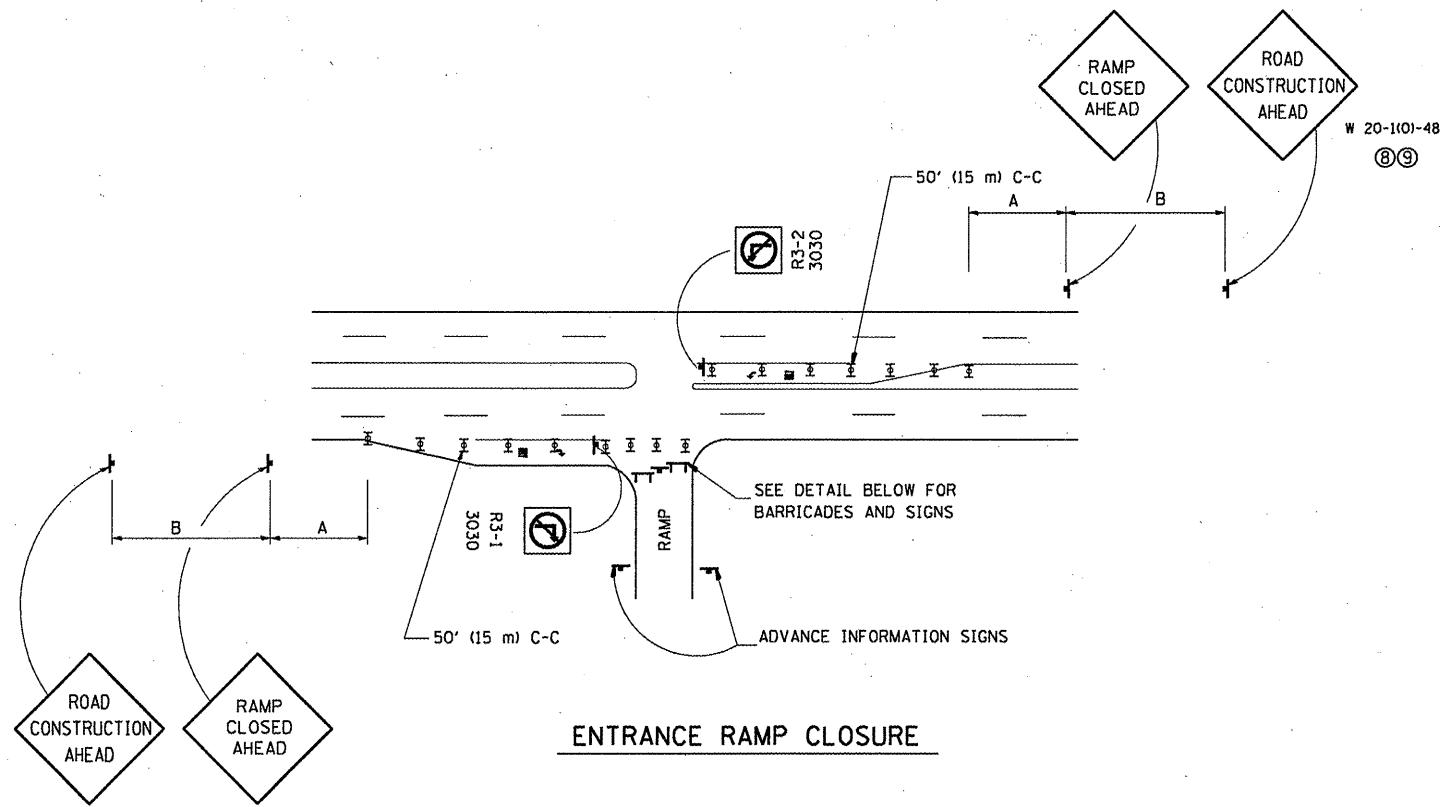
**BASIS OF PAYMENT:**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = wilgreendp	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\PW1DOT\WILGREENDP\d0183633\01st5td.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97			VAR.	2010-021PP	L.K. & MCH	25	13
PLOT SCALE = 1/8" = 1' / IN.		CHECKED -	REVISED - M. GOMEZ 01-22-01			BD600-06 (BD-24)		CONTRACT NO. 60K40		
PLOT DATE = 3/30/2010		DATE - 03-11-94	REVISED - R. BORO 12-15-09			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
				SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		

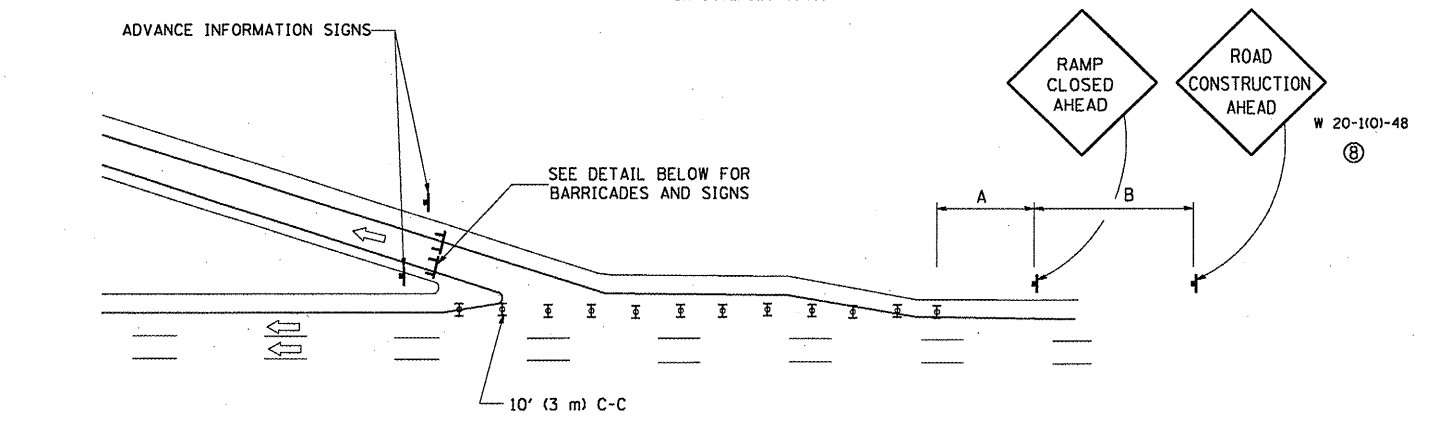


**ENTRANCE RAMP CLOSURE**

**SIGN SPACING TABLE**

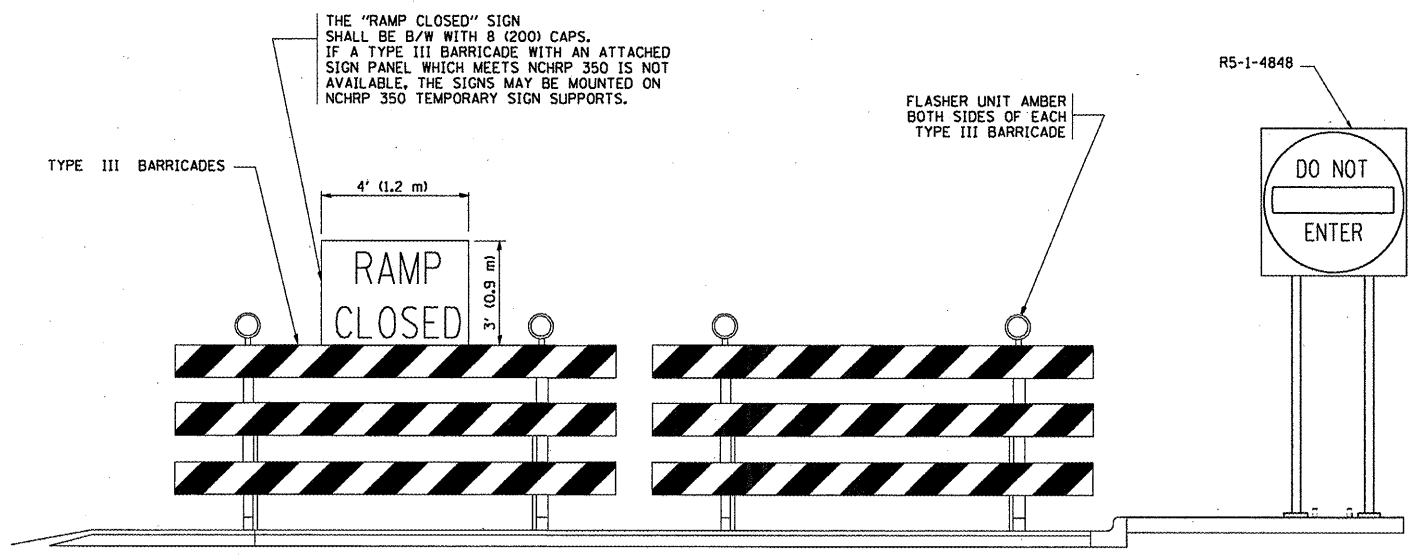
FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL >45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	150' (45 m)	150' (45 m)

DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.

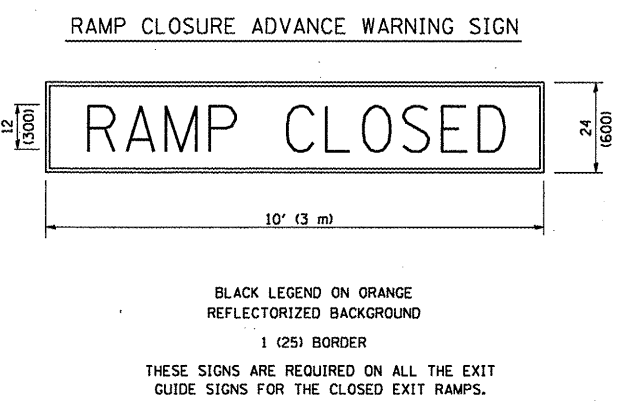


**EXIT RAMP CLOSURE**

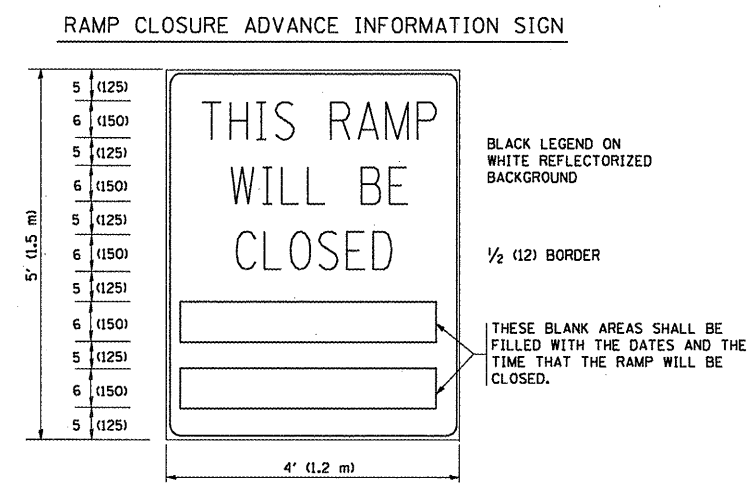
- SYMBOLS**
- ▬ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
  - ▬ TYPE III BARRICADE WITH FLASHING LIGHT



**DETAIL FOR REQUIRED BARRICADES & SIGNS**



BLACK LEGEND ON ORANGE REFLECTORIZED BACKGROUND  
1 (25) BORDER  
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR THE CLOSED EXIT RAMPS.



THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

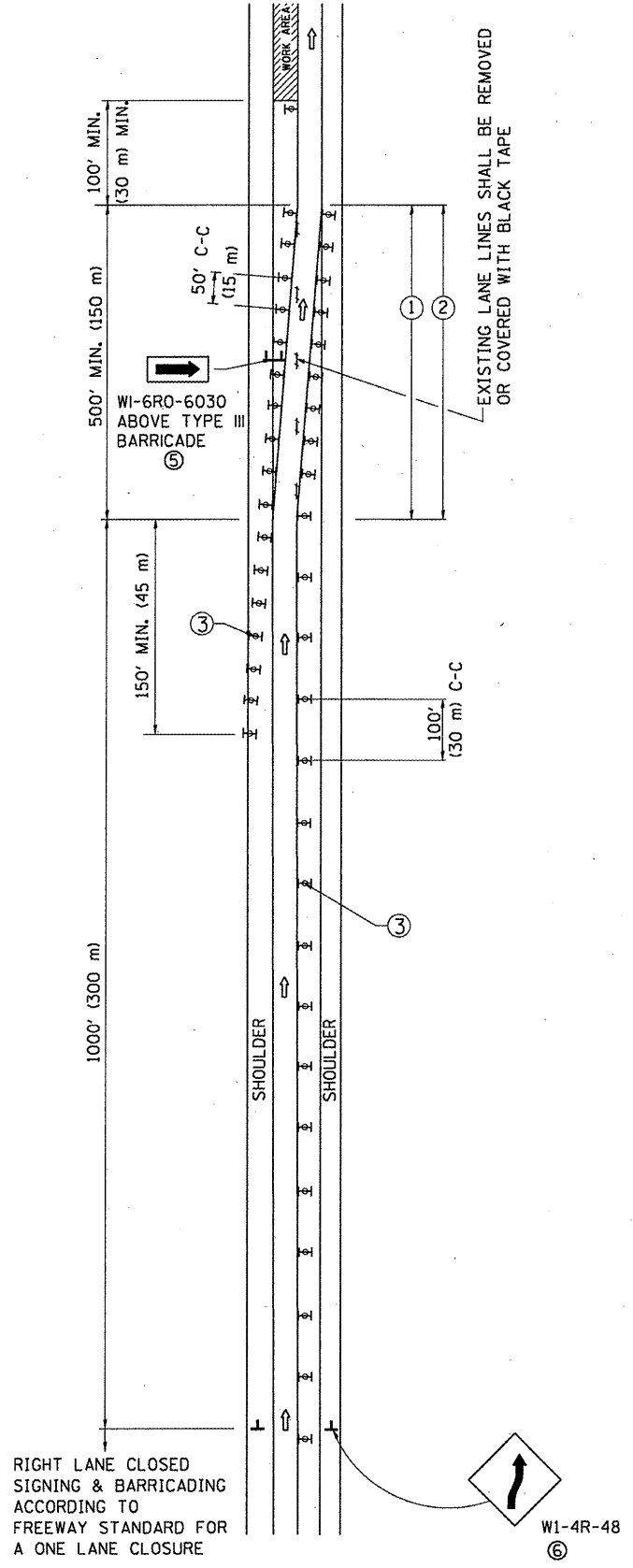
**GENERAL NOTES:**

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY-FOUR (24) HOURS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED ON CLOSURES LESS THAN 24 HOURS IN DURATION.

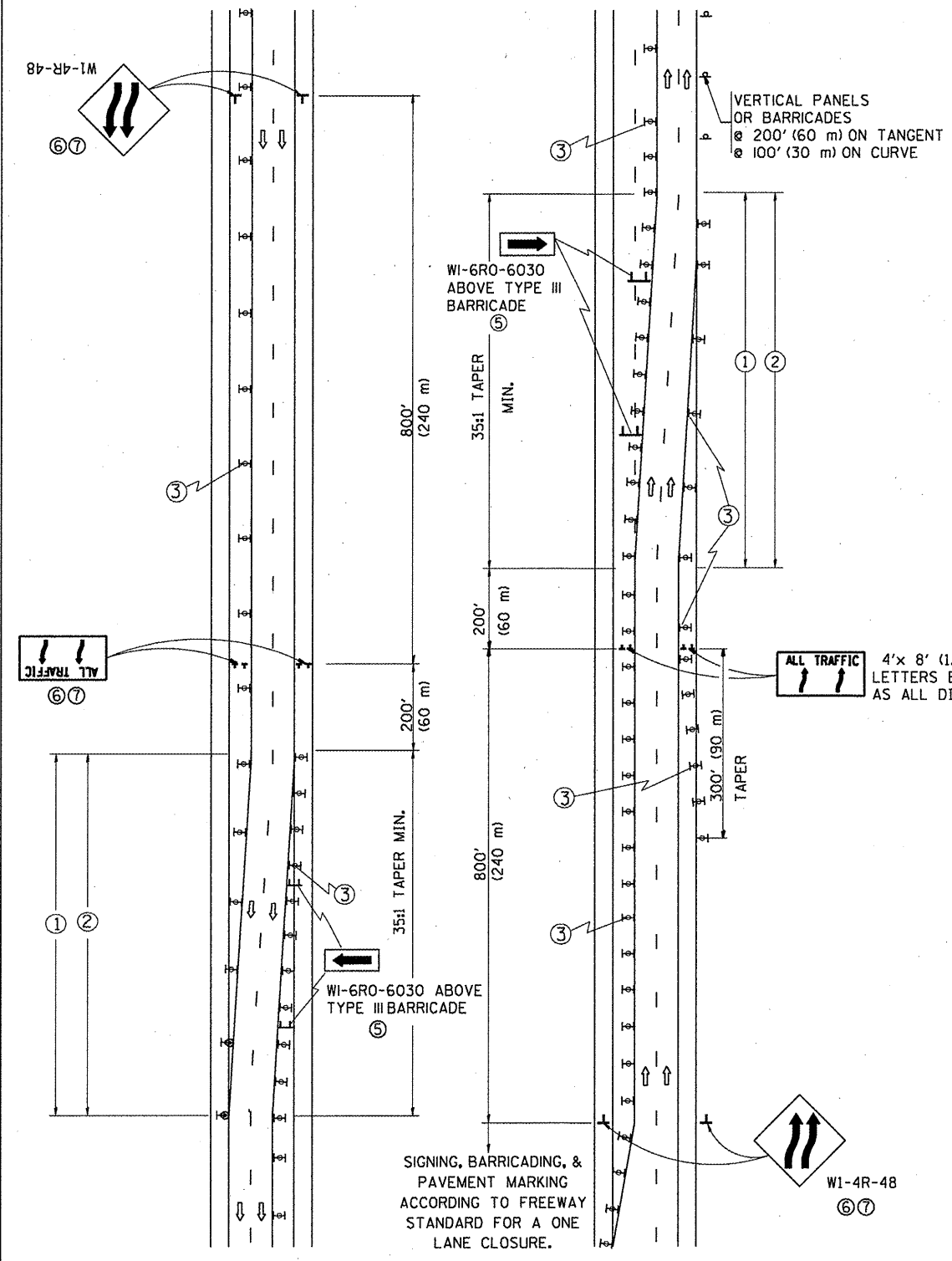
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = wilgreendp	DESIGNED - DWS	REVISED - DWS/JAF 12-02	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>FREWAY ENTRANCE AND EXIST RAMP CLOSURE DETAILS</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cd:\p\work\PW100T\WILGREENDP\d0183633\01std.dgn	DRAWN -	REVISED - JAF 02-06	VAR.			2010-02IPP	LK. & MCH	25	14	
PLOT SCALE = 1/8" = 1' / IN.	CHECKED -	REVISED - SPB 01-07	<b>TC-08</b>			<b>CONTRACT NO. 60K40</b>				
PLOT DATE = 3/30/2010	DATE - 02-83	REVISED - SPB 12-09	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT							
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			

# SINGLE LANE WEAVE



# MULTI-LANE WEAVE



### GENERAL NOTES

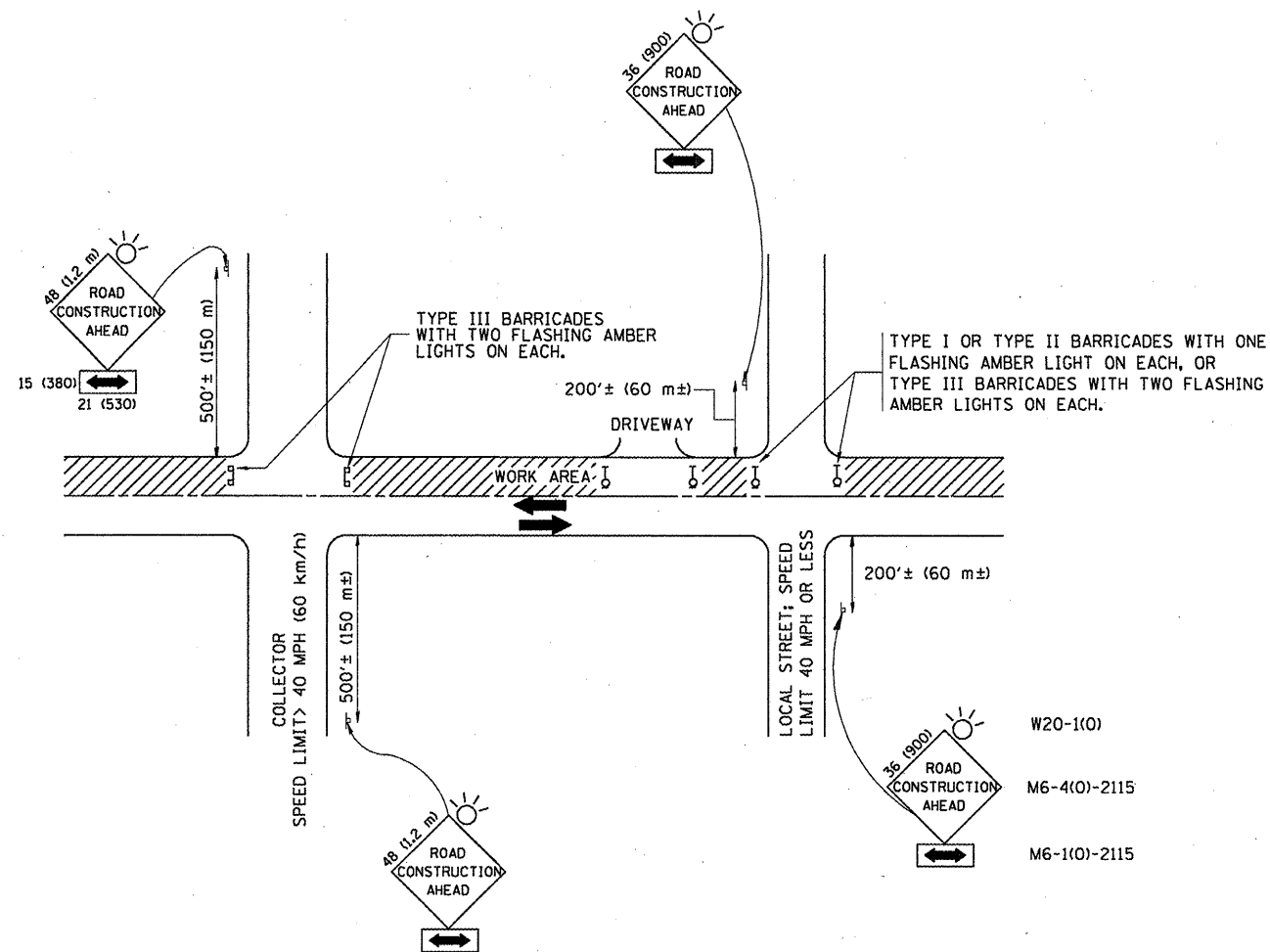
- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 24 HOURS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS. TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

### SYMBOLS

- ↑ DIRECTION OF TRAFFIC
- ▨ WORK AREA
- ⊥ SIGN ON PORTABLE OR PERMANENT SUPPORT
- ⊥ TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)  
UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = wilgreendp	DESIGNED - DWS	REVISED - JAF 01-03	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE &amp; MULTI-LANE WEAVE</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pwwork\pwwid\ot\WILGREENDP\d8183633\d	atStd.dgn	DRAWN -	REVISED - JAF 02-06		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	VAR.	2010-021PP	LK. & MCH	25	15
		PLOT SCALE = 100.0000' / IN.	REVISED - SPB 01-07					<b>TC-09</b>		<b>CONTRACT NO. 60K40</b>			
		PLOT DATE = 3/30/2010	REVISED - SPB 12-09					FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT					



**TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

**NOTES:**

**A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1,2 m x 1,2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

**B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = wilgreendp	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
c:\pwork\PWIDOT\WILGREENDP\d0183633\01std.dgn	std.dgn	DRAWN -	REVISED - A. HOUSEH 03-06-96
PLOT SCALE = 100.0000 ' / IN.		CHECKED -	REVISED - A. HOUSEH 10-15-96
PLOT DATE = 3/30/2010		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

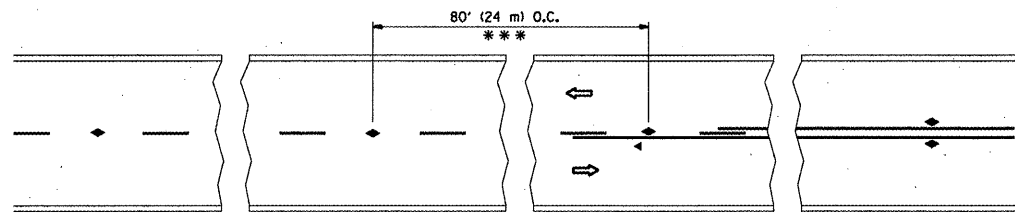
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

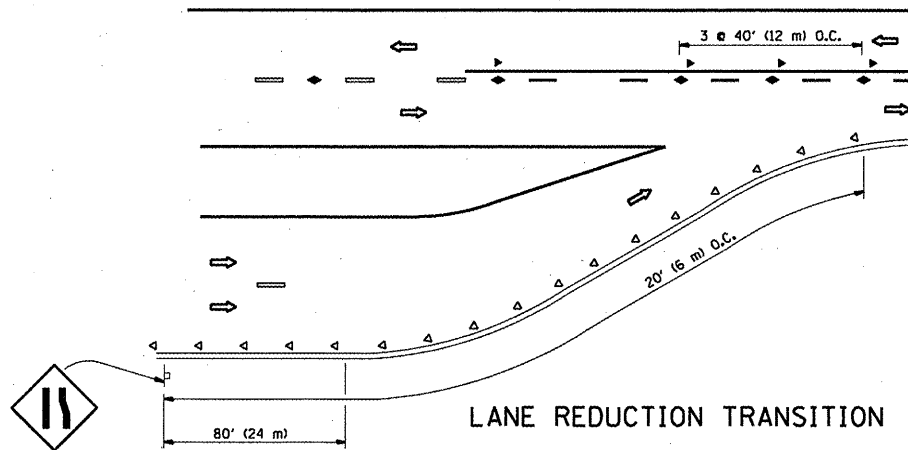
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2010-021PP	LK. & MCH	25	16
TC-10			CONTRACT NO. 60K40	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



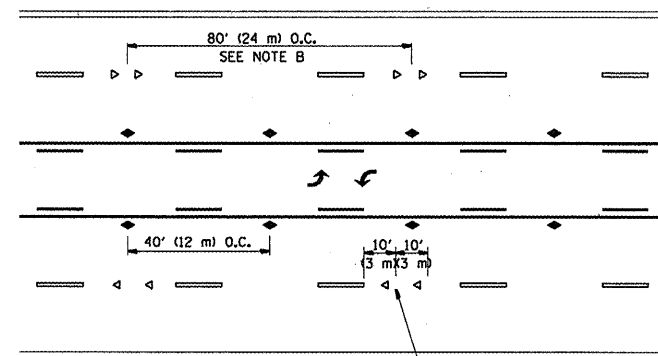


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

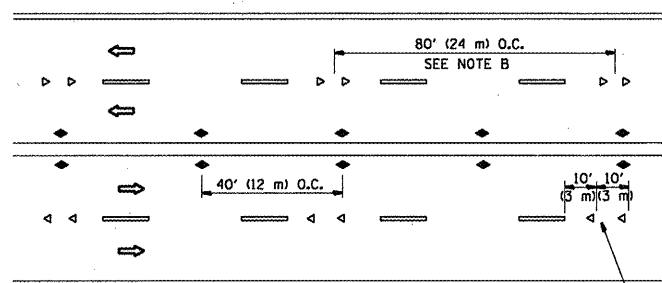
TWO-LANE/TWO-WAY



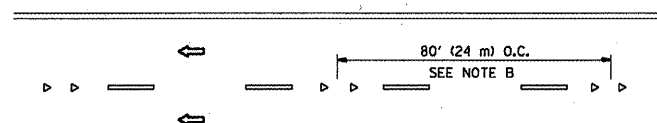
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

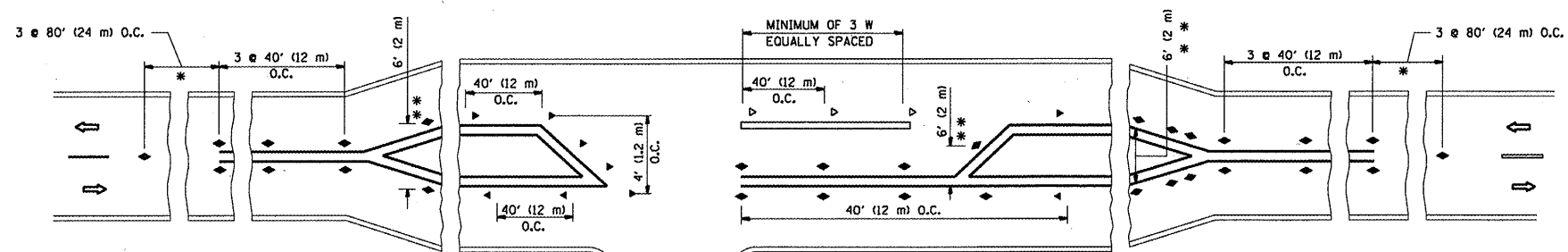
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = wlgreendp
atpwwork\FWID001\WILGREENDP\08183633\BstStd.dgn	
PLOT SCALE = 100.0000 / IN.	
PLOT DATE = 3/30/2010	

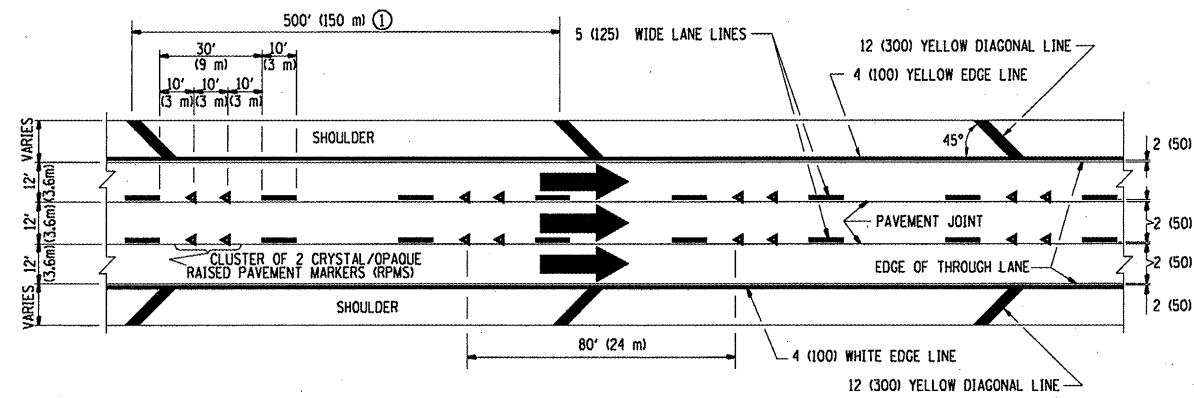
DESIGNED -	REVISOR - T. RAMMACHER 09-19-94
DRAWN -	REVISOR - T. RAMMACHER 03-12-99
CHECKED -	REVISOR - T. RAMMACHER 01-06-00
DATE -	REVISOR - C. JUCIUS 09-09-09

STATE OF ILLINOIS	TOTAL SHEETS	SHEET NO.
DEPARTMENT OF TRANSPORTATION	25	17
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLow RESISTANT)	CONTRACT NO. 60K40	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2010-021PP	LK. & MCH	25	17
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.
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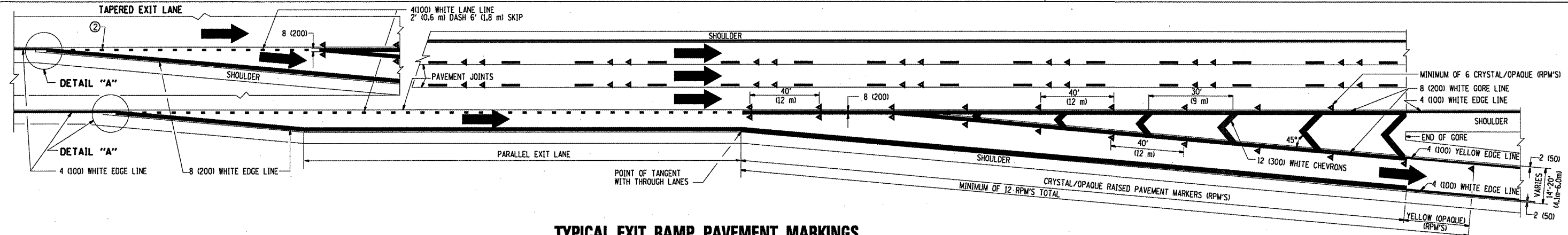
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2010-021PP	LK. & MCH	25	17
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



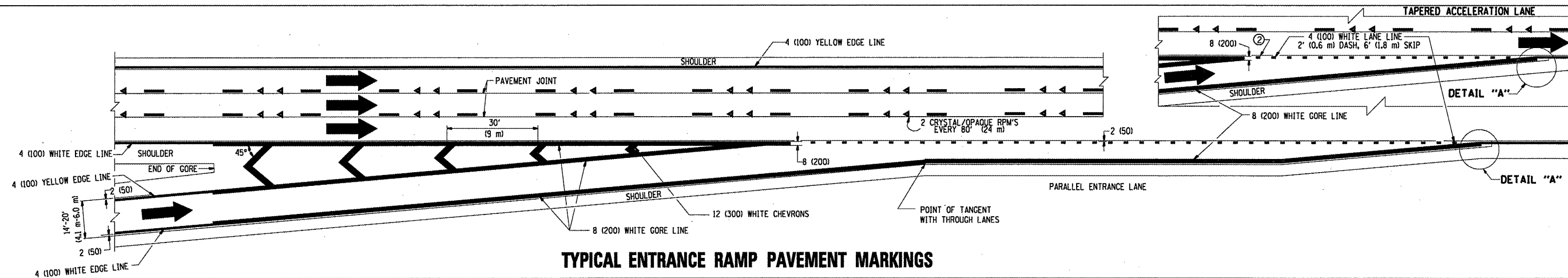
**TYPICAL EDGE LINES & LANE LINES**

**PAVEMENT MARKING MATERIALS**

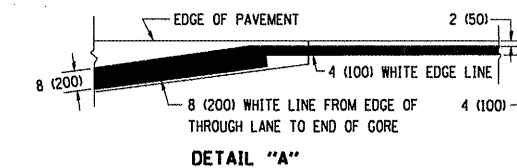
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE SHALL BE USED FOR ALL LANE LINES ON BITUMINOUS PAVEMENT.
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC.



**TYPICAL EXIT RAMP PAVEMENT MARKINGS**

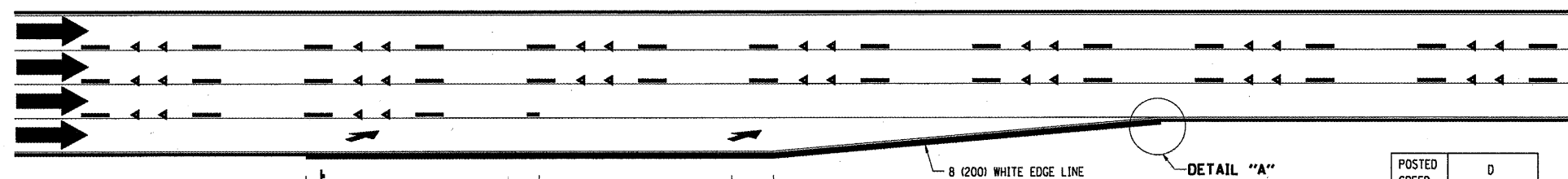


**TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS**



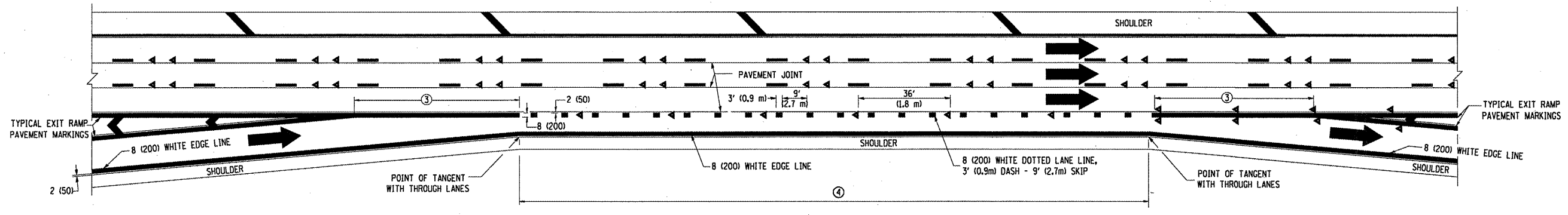
**NOTES:**

- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
- ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

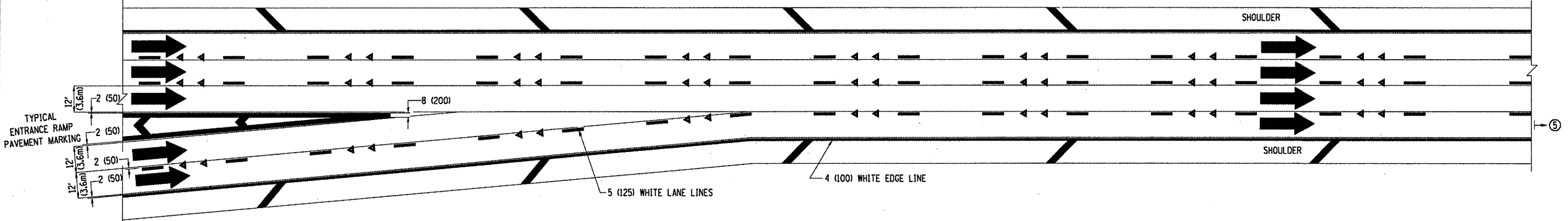


**LANE REDUCTION PAVEMENT MARKINGS**

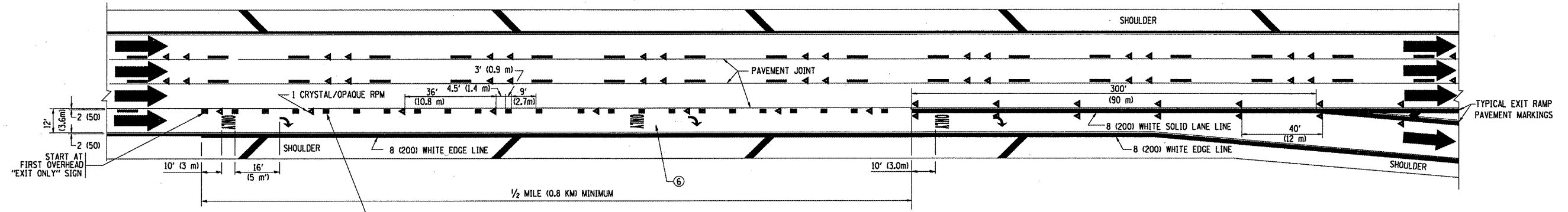
POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)



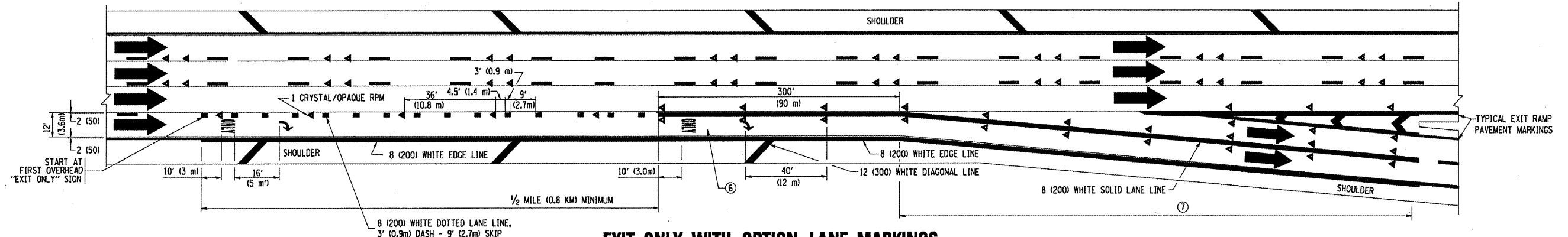
**AUXILIARY LANE MARKINGS**



**TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS**



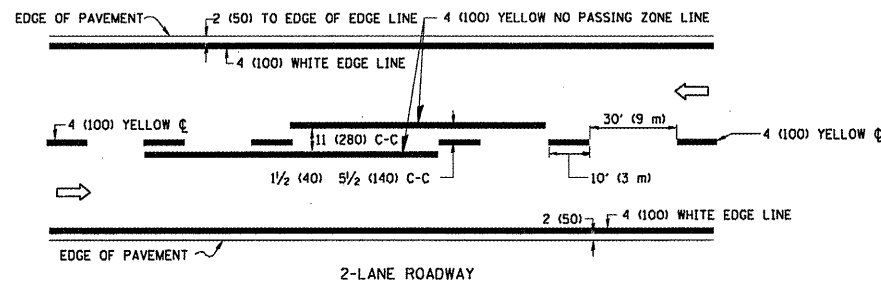
**EXIT ONLY LANE MARKINGS**



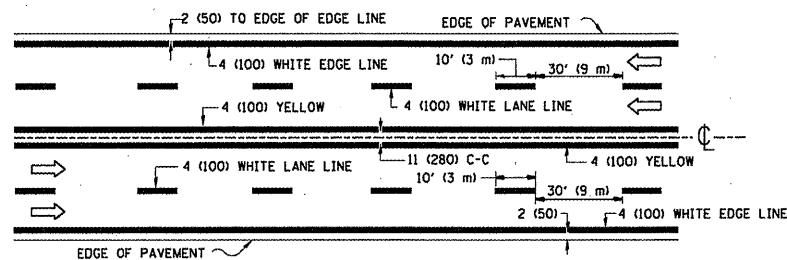
**EXIT ONLY WITH OPTION LANE MARKINGS**

- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
  - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
  - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
  - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
  - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED CORE.

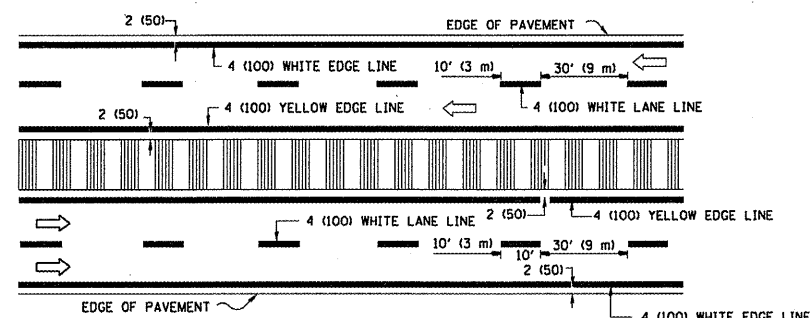
FILE NAME =	USER NAME = wilgreendp	DESIGNED - D.W.S.	REVISED - D.W.S. 07-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
at:\pw\work\PW1001\WILGREENDP\d0183633\	st5td.dgn	DRAWN -	REVISED - J.A.F. 02-06		SCALE: NONE	SHEET NO. 2 OF 2 SHEETS	STA.	VAR.	2010-021PP	L.K. & MCH	25	79
	PLOT SCALE = 1/8" = 100.0000' / IN.	CHECKED -	REVISED - S.P.B. 01-07					<b>TC-12</b>		<b>CONTRACT NO. 60K40</b>		
	PLOT DATE = 3/30/2010	DATE - 01-90	REVISED - S.P.B. 01-10					FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



2-LANE ROADWAY



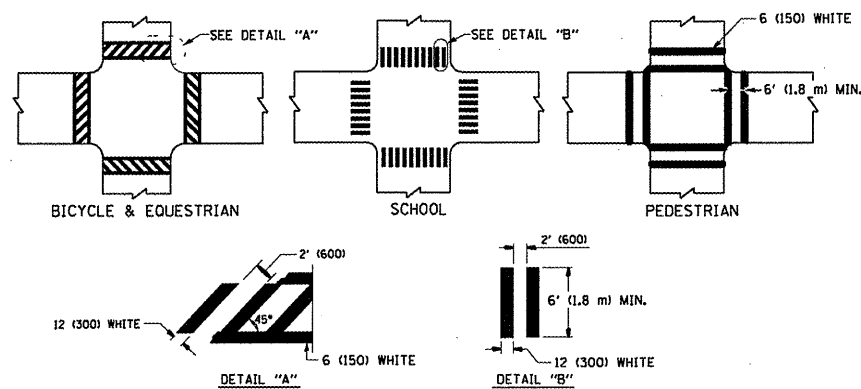
MULTI-LANE UNDIVIDED



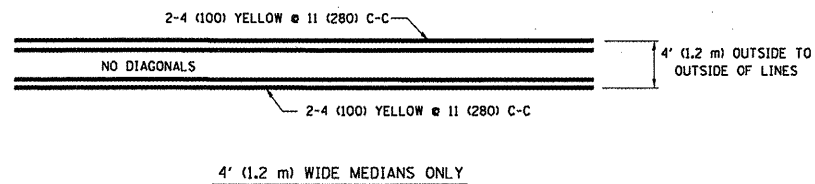
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

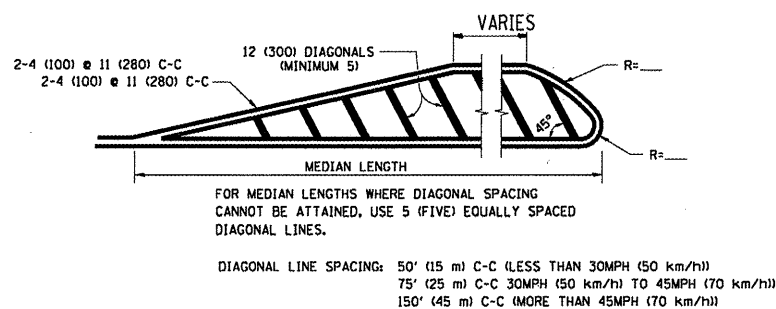
TYPICAL LANE AND EDGE LINE MARKING



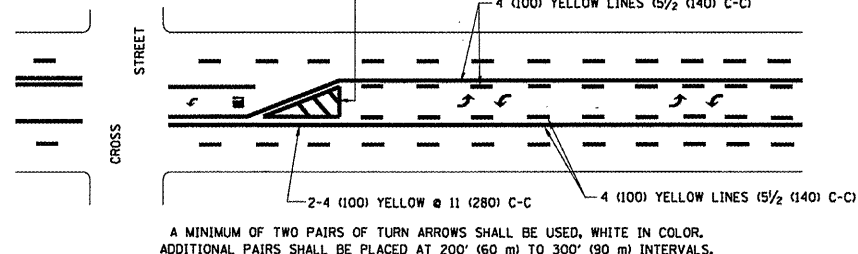
TYPICAL CROSSWALK MARKING



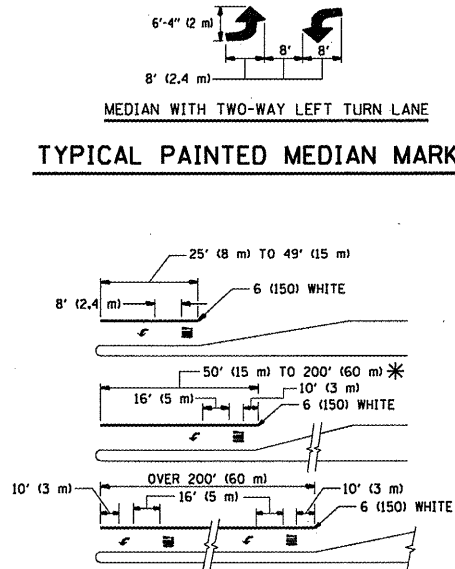
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING

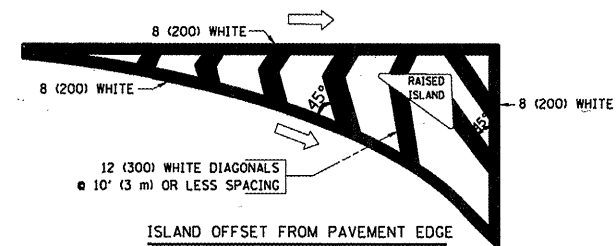


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

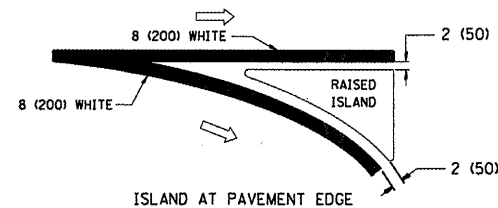
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = wilgreenp	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
ct:\pwork\PW1001\WILGREENP\8183633\st5td.dgn		DRAWN -	REVISED -C. JUCIUS 09-09-09
		CHECKED -	REVISED -
		DATE - 03-19-90	REVISED -

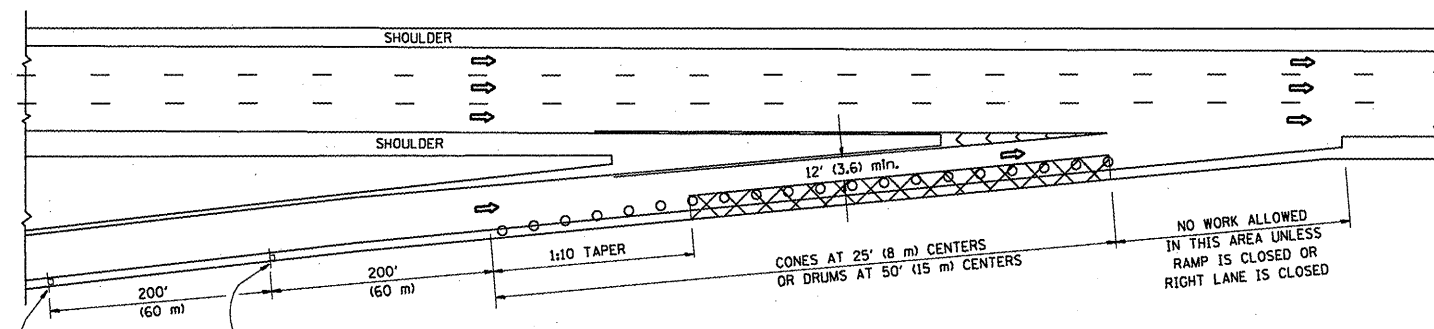
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

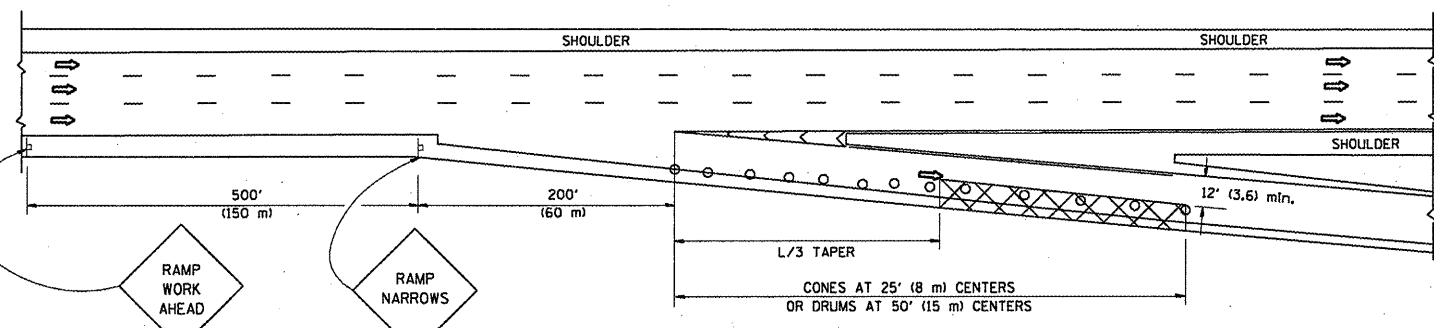
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2010-021PP	LK. & MCH	25	20
TC-13		CONTRACT NO. 60K40		
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

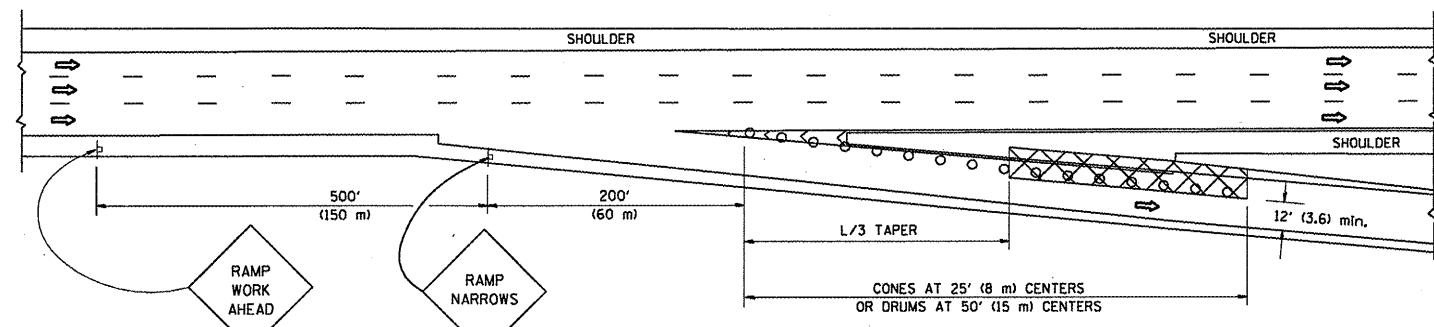
**PARTIAL RAMP CLOSURE DETAILS**



**TYPICAL ENTRANCE RAMP**



**TYPICAL EXIT RAMP**



**TYPICAL EXIT RAMP**

**SYMBOLS**

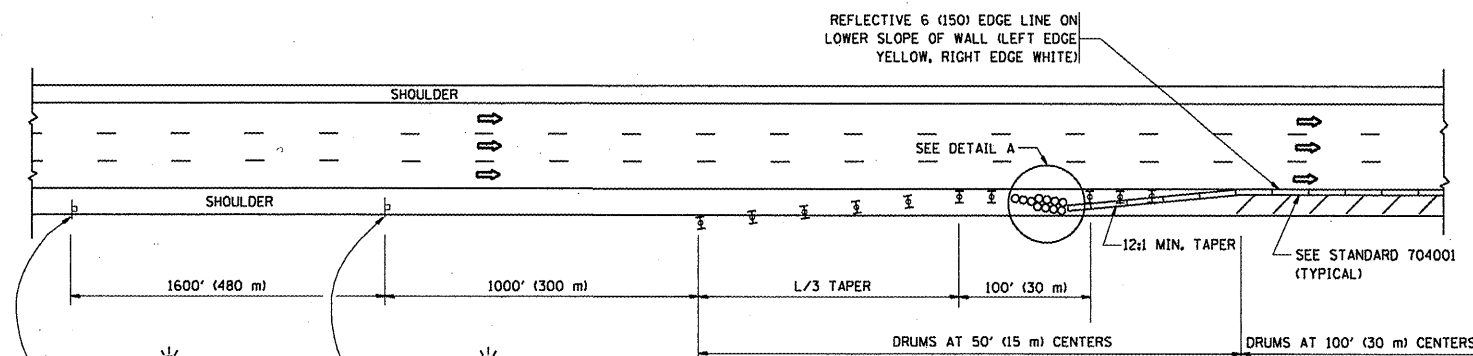
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

**GENERAL NOTES**

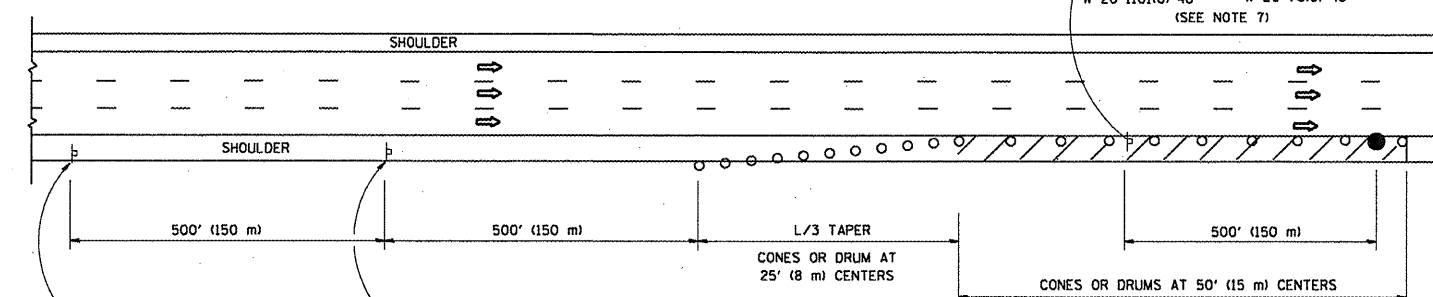
1. THE "L" DISTANCE EQUALS:  

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC: $L=0.65(W)(S)$ ENGLISH: $L=(W)(S)$
	W = WIDTH OF OFFSET IN FEET (METERS)
	S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

**SHOULDER CLOSURE DETAILS**

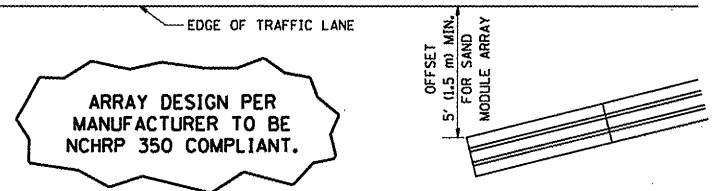


**PERMANENT SHOULDER CLOSURE**



**DAYTIME SHOULDER CLOSURE**

THIS DETAIL IS USED WHERE:  
 1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCOACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAYEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



**DETAIL "A" IMPACT ATTENUATOR, TEMPORARY (SEE NOTE 5)**

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
  - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
  - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCOACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

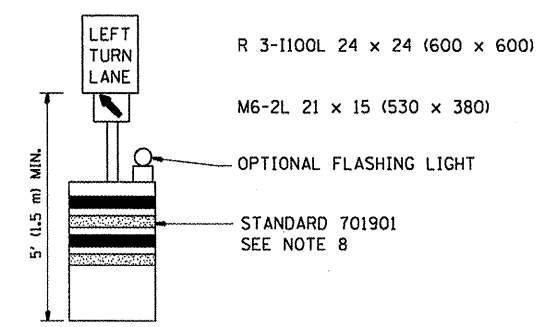
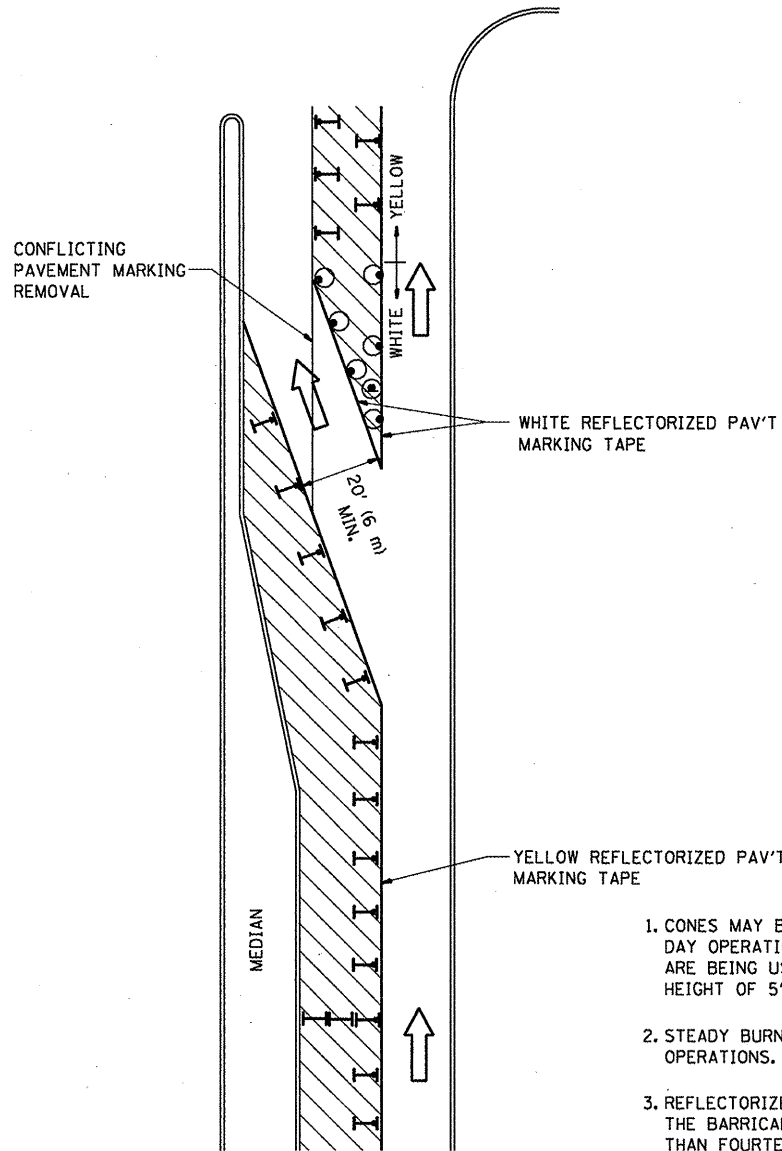
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = w1greendp	DESIGNED -	REVISED - 04-03
atp\work\PWIDOT\WILGREENDP\d0183633\d	st5td.dgn	DRAWN - D.W.S.	REVISED - J.A.F. 12-06
		CHECKED -	REVISED - S.P.B. 01-07
		DATE - 11-96	REVISED - S.P.B. 12-09

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2010-021PP	LK. & MCH	25	21
TC-17			CONTRACT NO. 60K40	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

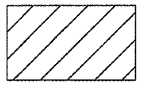
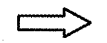






**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = wilgreendp	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
c:\pwork\p\1001\WILGREENDP\d0183633\01std.dgn	std.dgn	REVISED - A. HOUSEH 11-07-95	REVISED -
PLOT SCALE = 100.0000' / IN.		REVISED - A. HOUSEH 10-12-96	REVISED -
PLOT DATE = 3/30/2010		REVISED - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2010-02IPP	L.K. & MCH	25	22
TC-14		CONTRACT NO. 60K40		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

**ROUTE MARKERS**

FOR U.S. ROUTES  
MI-40-2424

FOR ILLINOIS ROUTES  
MI-50-2424

R.R. UNMARKED ROUTES  
SPECIAL 24" x 18" VARIABLE  
4" BLACK LETTERS ON WHITE  
REFLECTIVE BACKGROUND

**ARROWS SIGNS**

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-1-2115

M6-3-2115

**CARDINAL DIRECTION & DETOUR SIGNS**

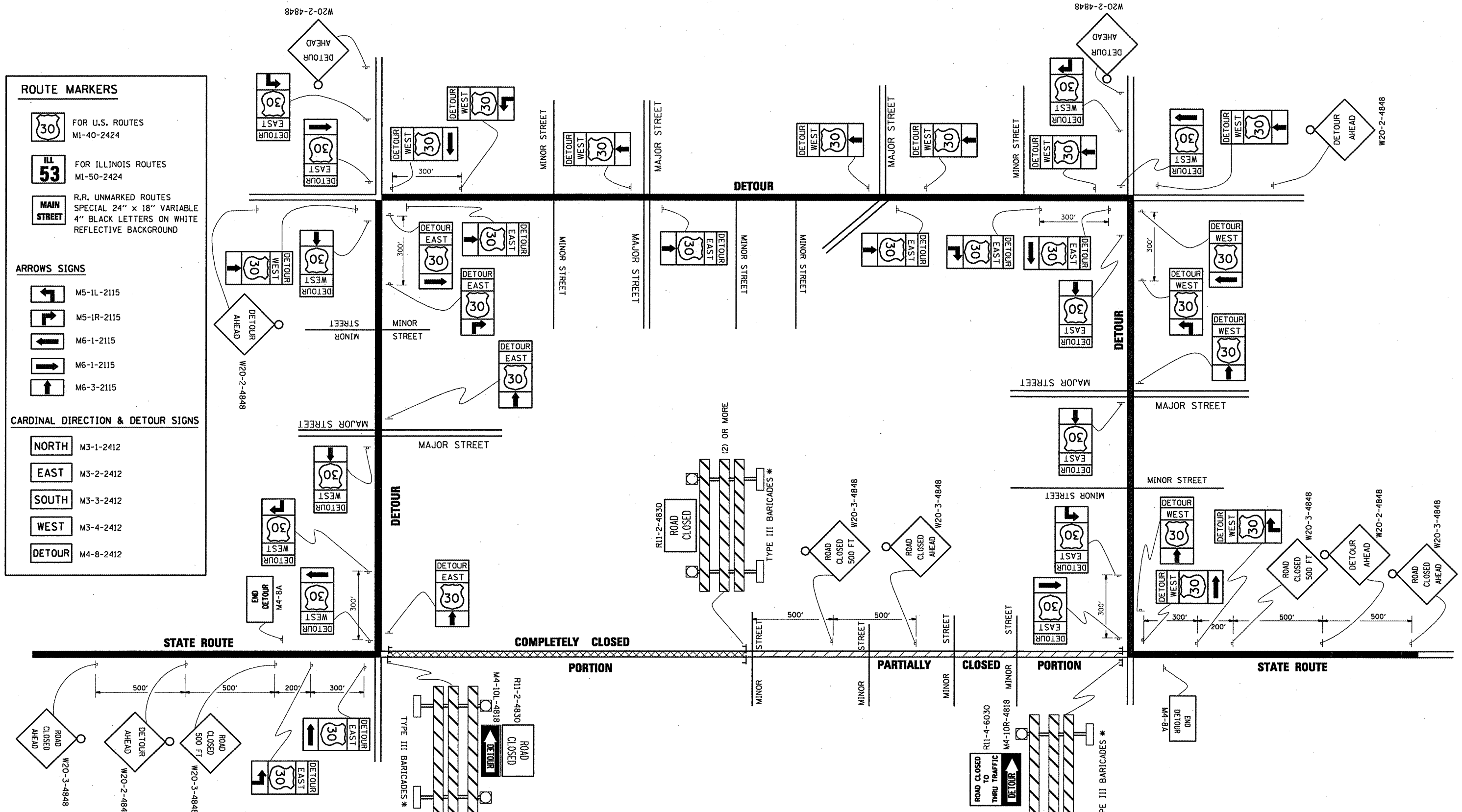
NORTH M3-1-2412

EAST M3-2-2412

SOUTH M3-3-2412

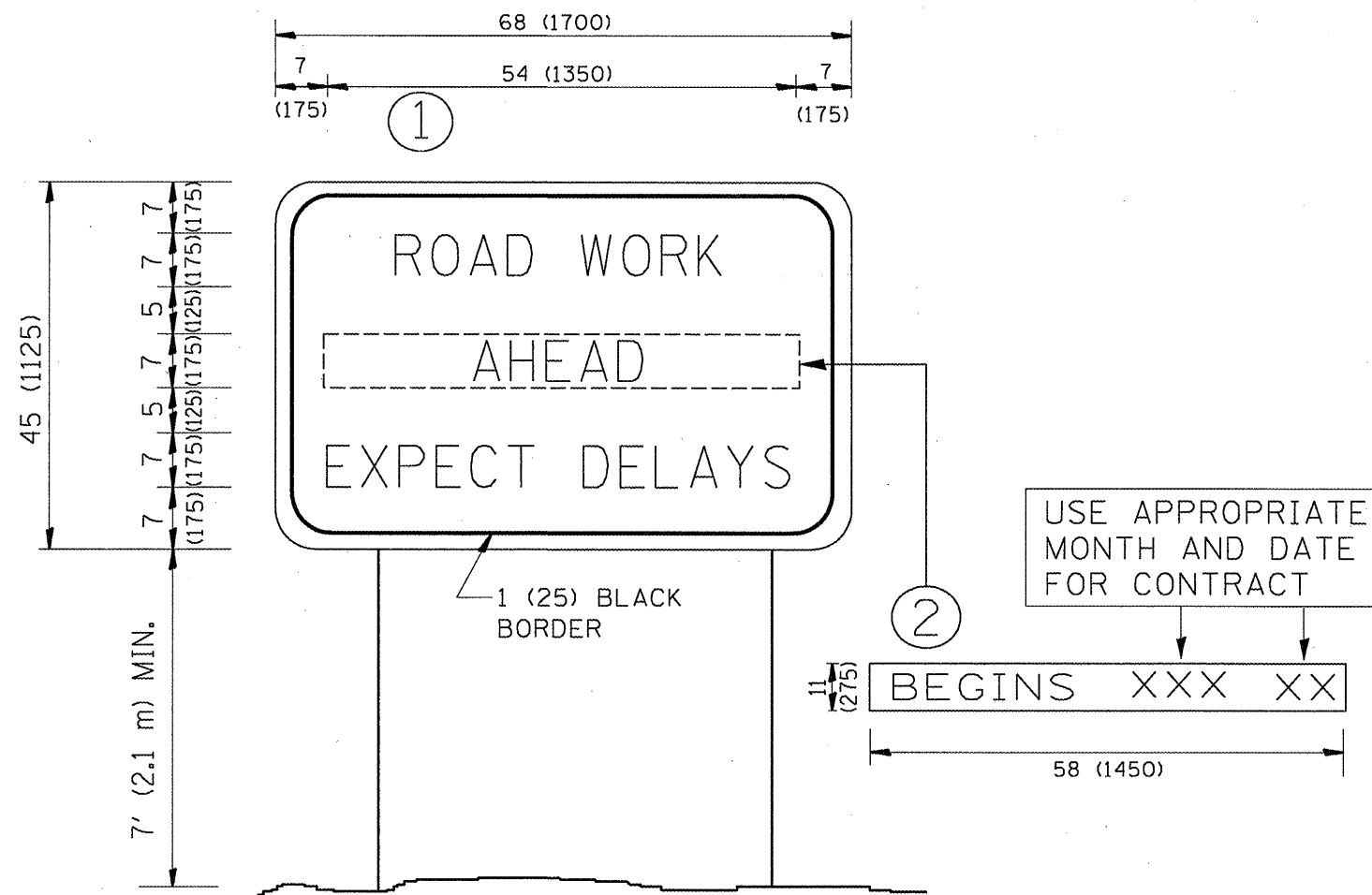
WEST M3-4-2412

DETOUR M4-8-2412



\* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

FILE NAME *	USER NAME = wlgreendp	DESIGNED -	REVISED - 10-18-02	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETOUR SIGNING FOR CLOSING STATE HIGHWAYS</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pwwork\PIWIDOT\WILGREENDP\80183633\	ststd.dgn	DRAWN -	REVISED - R. BORO 09-14-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	VAR.	2010-021PP	LK. & MCH	25	23
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -					<b>TC-21</b>			<b>CONTRACT NO. 60K40</b>		
	PLOT DATE = 3/30/2010	DATE -	REVISED -					FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT					



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED - R. MIRS 09-15-97
ct:\pw_work\FW100T\WILGREENDP\d8183633\01st5td.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 3/30/2010	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

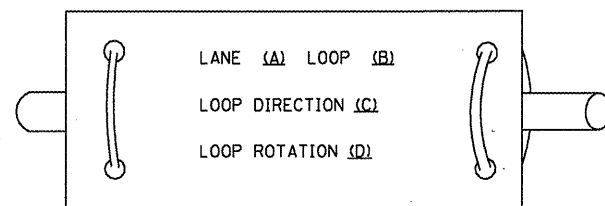
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2010-021PP	LK. & MCH	25	24
TC-22		CONTRACT NO. 60K40		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				



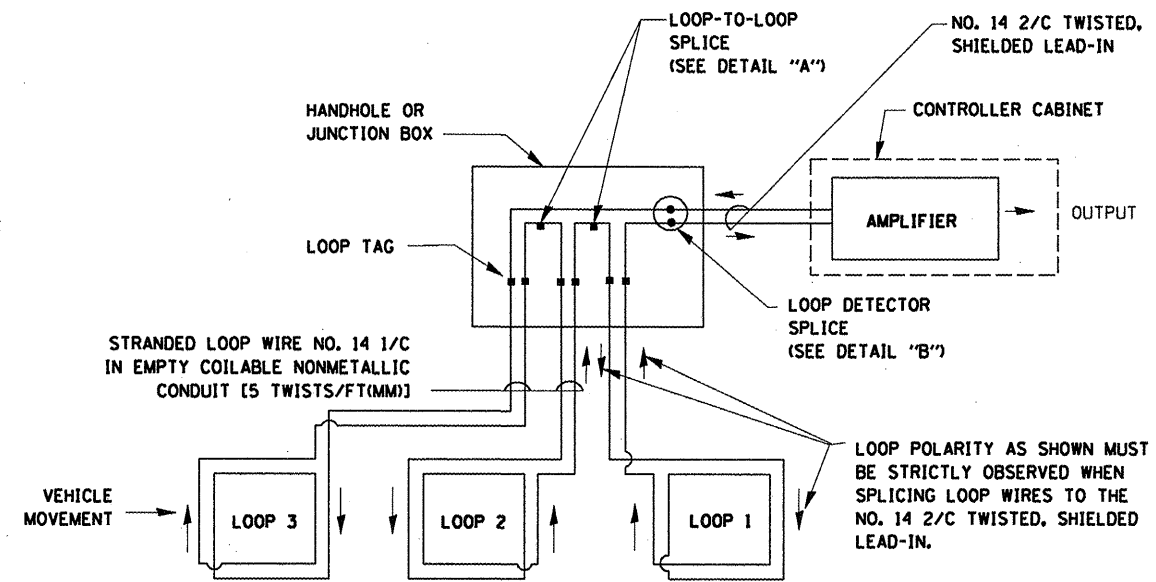
## LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

### LOOP LEAD-IN CABLE TAG

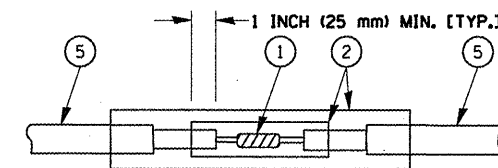


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

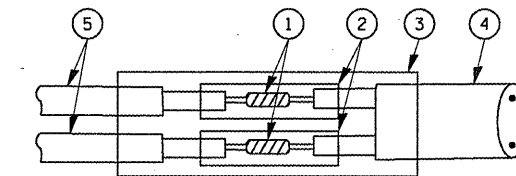


### DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

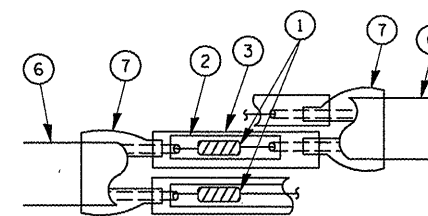


DETAIL "A"  
LOOP-TO-LOOP SPLICE

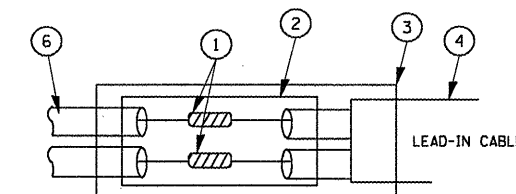


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

### TYPE I LOOP



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

### LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE. MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE. MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = wilgreendp	DESIGNED - DAD	REVISED -
ca:\ppl_work\PWIDOT\WILGREENDP\d0183633\d0183633.dgn		DRAWN - BCK	REVISED -
PLOT SCALE = 1/8" = 1" / IN.		CHECKED - DAD	REVISED -
PLOT DATE = 3/30/2010		DATE - 10-28-09	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE	
STANDARD TRAFFIC SIGNAL DESIGN DETAILS	
SCALE: NONE	SHEET NO. 1 OF 6 SHEETS STA. TO STA.

F.A. RTE. VAR.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2010-021PP	LK. & MCH	25	25
TS-05			CONTRACT NO. 60K40	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				