

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGE OF BUFFALO GROVE

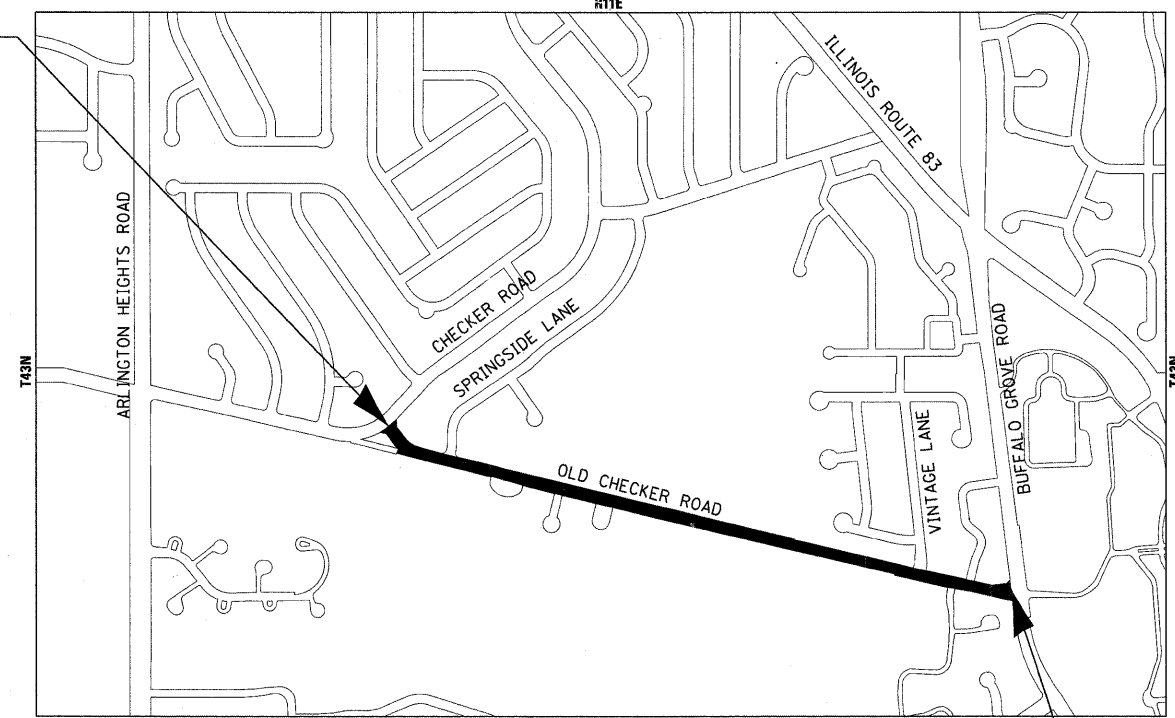
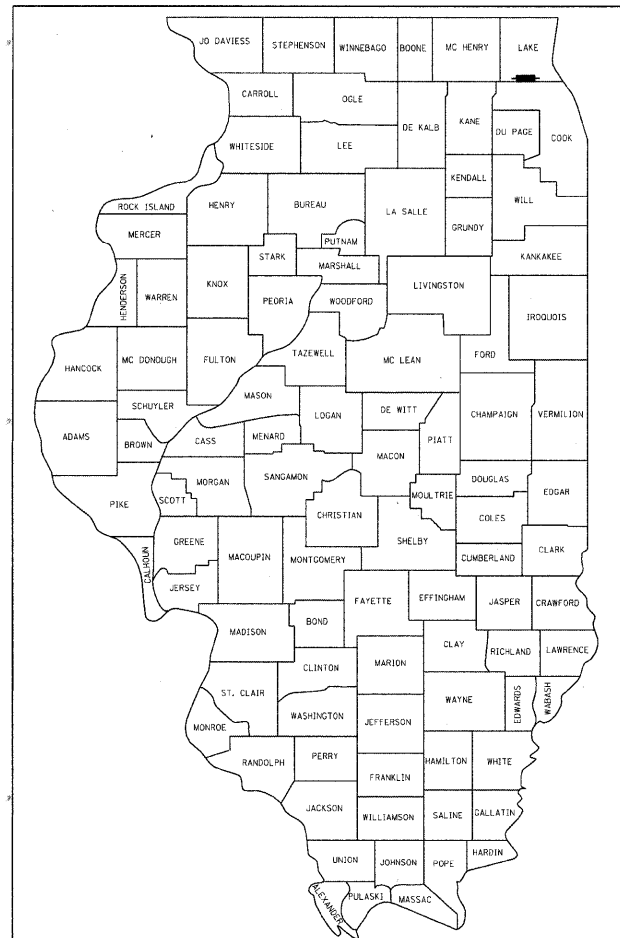
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
F.A.U. ROUTE 1268 OLD CHECKER ROAD
FROM CHECKER DRIVE (F.A.U. 2402) TO BUFFALO GROVE ROAD (F.A.U. 2657)
LAPP RESURFACING

PROJECT NO.: ARA-9003(453)
SECTION NO.: 09-00098-00-RS
JOB NO.: C-91-884-09
VILLAGE OF BUFFALO GROVE
LAKE COUNTY

Table with columns: F.A.U. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO., STA., TO STA., FED. ROAD DIST. NO., ILLINOIS, FED AID PROJECT, CONTRACT NO. 63339

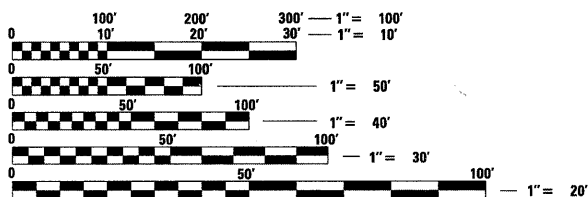
THE REVIEW AND APPROVAL OF THESE FINAL ENGINEERING PLANS AND SPECIFICATIONS BY THE LAKE COUNTY DIVISION OF TRANSPORTATION DOES NOT CONSTITUTE A RELEASE FROM OR GRANT OF VARIATION FROM THE RULES, REGULATIONS, AND SPECIFICATIONS REQUIRED IN THE LAKE COUNTY HIGHWAY UTILITY AND FACILITY PLACEMENT ORDINANCE...



PROJECT NO. ARA-9003(453)
F.A.U. ROUTE 1268
OLD CHECKER ROAD
RESURFACING BEGINS
STA. 10 + 16

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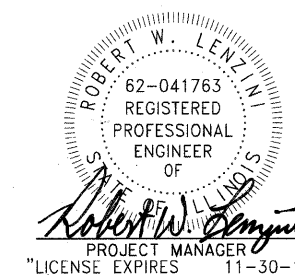
TRAFFIC DATA
OLD CHECKER ROAD
POSTED & DESIGN SPEED LIMIT = 35 MPH
2009 ADT = 4,000
COLLECTOR



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. DESIGN STAGE REQUEST
DIG. No. A1873203

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PROJECT NO. ARA-9003(453)
F.A.U. ROUTE 1268
OLD CHECKER ROAD
RESURFACING ENDS
STA. 51 + 89

LOCATION OF SECTION INDICATED THIS: [Symbol]

Approval stamps from the State of Illinois Department of Transportation, including signatures of Muller (October 13, 2009) and District Engineer of Local Roads and Streets (March 31, 2010), and a releasing stamp for bid (April 1, 2010) by Deputy Director of Highways, Region 1 Engineer.

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GENERAL NOTES

- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL DEVICES", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
- UTILITY LOCATIONS HAVE NOT BEEN SHOWN ON THESE PLANS. THE CONTRACTOR SHALL HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL THEIR FACILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL ALSO VERIFY THE DEPTHS OF THE EXISTING UTILITIES IF NECESSARY. ANY RELOCATION OR LOWERING OF UTILITIES SHALL BE COORDINATED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES, INCLUDING SPRINKLER SYSTEMS, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY OR SPRINKLER SYSTEM THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER OR VILLAGE.
- THE CONTRACTOR SHALL NOTIFY THE VILLAGE PUBLIC WORKS ADMINISTRATOR AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER.
- MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, DRIVEWAYS, CURB AND GUTTER, HOT-MIX ASPHALT SURFACES, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGMENT OF THE VILLAGE, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE VILLAGE WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR WILL BE BILLED (CHARGED) ACCORDINGLY.
- THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING BAN" IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER IF DEEMED NECESSARY.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE VILLAGE WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO CURB AND GUTTER AND/OR DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE VILLAGE TO RESIDENTS. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED.
- ACCESS TO PRIVATE DRIVEWAYS SHALL BE PROVIDED AT ALL TIMES EXCEPT DURING ACTUAL CONSTRUCTION ADJACENT THERE TO.
- THE CONTRACTOR SHALL NOTIFY IDOT BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS PRIOR TO THE PLACEMENT OF HMA OR CONCRETE.
- ANY SIGNS OR MAILBOXES THAT ARE IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED AND REPLACED IN ACCORDANCE WITH VILLAGE STANDARDS AND INCLUDED IN THE PAY ITEM FOR COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT. MAIL SERVICE SHALL BE MAINTAINED AT ALL TIMES.
- EXISTING PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE PRICE OF THE APPROPRIATE REMOVAL PAY ITEM.
- IN AREAS WHERE THE EXISTING SIDEWALK OR CURB AND GUTTER IS TO BE REMOVED AND REPLACED, THE REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL REQUIRED TO ESTABLISH THE PROPOSED SIDEWALK OR CURB AND GUTTER SUBGRADE ELEVATION SHALL BE INCLUDED IN THE PAY ITEMS, SIDEWALK REMOVAL OR COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT.
- CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
- THE FINISHED HOT-MIX ASPHALT SURFACE SHALL BE CONSTRUCTED 0.25 INCH ABOVE THE GUTTER FLAG.
- PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 6-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES DRIVEWAYS. TRANSVERSE EXPANSION JOINTS 3/4" SHALL BE PLACED EVERY 50 FEET OR AS DETERMINED BY THE ENGINEER. TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED EVERY 5-FEET.
- DETECTABLE WARNINGS SHALL CONSIST OF DYED CONCRETE STAMPED WITH APPROPRIATE STAMPING TOOLS. THE PIGMENT SHALL MEET THE REQUIREMENTS OF ASTM C 979, MATCH COLOR 30166 OF FEDERAL STANDARD 595. THE COST OF DYING CONCRETE IN ADDITION TO THE DETECTABLE WARNING AREA SHOWN ON THE DETAIL SHALL BE INCLUDED IN THE COST OF DETECTABLE WARNINGS BUT THE LIMITS OF THE DYED CONCRETE SHALL NOT EXTEND BEYOND THE BACK OF THE DETECTABLE WARNING AREA.

- A PORTABLE BATHROOM(S) SHALL BE PLACED ON THE JOB SITE(S) AND RELOCATED WHEN NECESSARY SO IT IS ACCESSIBLE TO WORKERS. IF WORK IS OCCURRING AT SEVERAL LOCATIONS, ONE PORTABLE BATHROOM SHALL BE PLACED AT EACH LOCATION WITHIN A REASONABLE DISTANCE FROM THE WORK AS DETERMINED BY THE ENGINEER. THIS SHALL BE INCLUDED IN THE PAY ITEM FOR MOBILIZATION.
- ALL CRACKS AND JOINTS SHALL BE CLEANED PRIOR TO FILLING THEM. THIS WORK SHALL BE INCLUDED IN THE ITEM "MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS."
- THE PRIME COAT APPLICATION RATE SHALL BE 0.1 GAL/SY.
- FOR STEEL BARS CERTIFICATION, PLEASE CONTACT IDOT BUREAU OF MATERIALS AT (847) 705-4337.
- THE DISTRICT 1 DETAIL FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT SHOWN IN THE PLANS SHALL BE MODIFIED TO INCLUDE THE FOLLOWING. THE WORK SHALL INCLUDE SAW-CUTTING AND REMOVING THE EXISTING PAVEMENT A MINIMUM OF 6-INCHES MEASURED FROM THE EXISTING EDGE OF PAVEMENT, AND FILLING THE 6" GAP WITH CLASS SJ CONCRETE TO AN ELEVATION 2-1/2" BELOW THE PROPOSED CURB AND GUTTER FLAG. IF THE CONCRETE IS POURED HIGHER THAN 2-1/2" FROM THE GUTTER FLAG FOR STREETS TO BE RESURFACED, THE CONTRACTOR WILL BE REQUIRED TO GRIND ADDITIONAL CONCRETE TO THE REQUIRED 2-1/2" DEPTH. THE AREA BEHIND THE PROPOSED CURB AND GUTTER SHALL BE RESTORED WITH NATIVE SOD IN ACCORDANCE WITH SECTION 1081.03 RATHER THAN SALT TOLERANT SOD.
- ON STREETS TO BE FULL WIDTH MILLED (2" OR MORE), THE STRUCTURES IN THE PAVEMENT SHALL BE ADJUSTED IN ACCORDANCE WITH THE IDOT DETAIL "DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING". THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).
- THE DAYS PAVING OPERATION SHOULD RESULT IN A SINGLE TRANSVERSE JOINT. COLD LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY PAVING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE PAVING OF THE ADJACENT LANE IN THE SAME DAY.
- THE CONTRACTOR SHALL UTILIZE A MECHANICAL SWEEPER TO CLEAN STREETS AFFECTED BY CONTRACTOR'S OPERATIONS, INCLUDING HAUL ROUTES, AT LEAST TWICE PER WEEK AND ADDITIONALLY AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF CONSTRUCTION.
- NO DAMAGE TO PARKWAYS IS PLANNED AS PART OF THESE IMPROVEMENTS EXCEPT WHERE SIDEWALK OR CURB AND GUTTER IS REMOVED AND REPLACED. ANY AND ALL RESTORATION OF DAMAGED PARKWAYS SHALL CONSIST OF PROPERLY PLACED, GRADED, AND COMPACTED SUITABLE MATERIAL AS APPROVED BY THE ENGINEER, 4-INCHES OF TOPSOIL, AND NATIVE SOD IN ACCORDANCE WITH ARTICLE 1081.03. RESTORATION OF DAMAGED PARKWAYS SHALL BE INCLUDED IN PCC SIDEWALK AND COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- WORK AND MATERIALS REQUIRED TO INSTALL 1-INCH UNIT DUCT SHALL BE INCLUDED IN THE PAY ITEM "DETECTOR LOOP REPLACEMENT".
- THE LOCATIONS OF CLASS D PATCHES SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
- ALL AGGREGATE USED ON THIS PROJECT SHALL BE CRUSHED MATERIAL.
- THE QUANTITIES FOR PATCHING WILL NOT EXCEED THE AMOUNT LISTED IN THE SUMMARY OF QUANTITIES.
- EXISTING FRAMES AND GRATES OR LIDS THAT ARE TO BE REPLACED AS PART OF STRUCTURE ADJUSTMENT SHALL BE DELIVERED TO THE VILLAGE OF BUFFALO GROVE PUBLIC WORKS.

LEGEND

TTTTTT	BUTT JOINT
⊙	FRAME AND LIDS TO BE ADJUSTED (SPECIAL)

LAKE COUNTY GENERAL NOTES

(APPLIES TO WORK WITHIN LAKE COUNTY ROW)

- THE CONSTRUCTION, INCLUDING MATERIALS USED, OF THIS UTILITY AND FACILITY SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF THE MOST RECENT EDITIONS OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", AND "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS"; INCLUDING ALL AMENDMENTS AND SUCCESSOR DOCUMENTS TO THE AFOREMENTIONED DOCUMENTS AS PUBLISHED OR ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION UNLESS OTHERWISE STATED IN THESE ENGINEERING PLANS.
- THE CONSTRUCTION OF THIS UTILITY OR FACILITY SHALL ALSO BE IN ACCORDANCE WITH THE "LAKE COUNTY HIGHWAY UTILITY AND FACILITY PLACEMENT ORDINANCE," LATEST EDITION, AS AMENDED, OR ANY SUCCESSOR DOCUMENT OR DOCUMENTS THERETO.
- THE LAKE COUNTY DIVISION OF TRANSPORTATION SHALL NOT BE HELD LIABLE FOR ANY ERRORS OR OMISSIONS IN THESE ENGINEERING PLANS OR FOR ANY ADDITIONAL WORK, WHICH MAY BE NEEDED DUE TO ERRORS OR OMISSIONS IN THESE ENGINEERING PLANS.
- THE PERMITTEE SHALL BE RESPONSIBLE FOR ANY ADDITIONAL WORK, AND ALL COSTS THEREOF, REQUIRED BECAUSE OF ERRORS OR OMISSIONS IN THESE ENGINEERING PLANS AND FOR THE CORRECTION OF ANY CONSTRUCTION, MAINTENANCE, OR SAFETY PROBLEMS, WHICH BECOME APPARENT DURING CONSTRUCTION OR BY INSPECTIONS MADE BY THE PERSON IN CHARGE OR THE COUNTY DIVISION OF TRANSPORTATION.
- THE PERSON IN CHARGE SHALL NOTE ANY CHANGES FROM THESE ENGINEERING PLANS AND SHALL NOTIFY THE PERMIT DEPARTMENT OF THE LAKE COUNTY DIVISION OF TRANSPORTATION ABOUT ANY CHANGES THAT DEVIATE FROM THE INTENT OF THE ENGINEERING PLANS.

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HIGHWAY STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUT FOR DETECTOR LOOPS

**VILLAGE OF BUFFALO GROVE, ILLINOIS
OLD CHECKER ROAD ARRA (LAPP)
RESURFACING**

**INDEX OF SHEETS, HIGHWAY STANDARDS,
GENERAL NOTES**

DESIGNED - DSH	REVISED-10-09-09 PER IDOT REVIEW
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-09-09	FILE - 090715-gen-notes.sh

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
126B	09-00098-00-RS	LAKE	21	2
C-91-884-09			CONTRACT NO. 63339	
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT ARA-9003453	



SUMMARY OF QUANTITIES

CODE NO.	PAY ITEM	UNIT	CONSTRUCTION CODE 1000
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	20
25200100	SODDING	SQ YD	20
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	384
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	1,589
40600300	AGGREGATE (PRIME COAT)	TON	32
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	5
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	674
40600895	CONSTRUCTING TEST STRIP	EACH	1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	237
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1,827
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2,968
42400800	DETECTABLE WARNINGS	SQ FT	488
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	15,646
44000600	SIDEWALK REMOVAL	SQ FT	2,968
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	194
44201729	CLASS D PATCHES, TYPE II, 7 INCH	SQ YD	112
44201733	CLASS D PATCHES, TYPE III, 7 INCH	SQ YD	212
44201735	CLASS D PATCHES, TYPE IV, 7 INCH	SQ YD	312
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	5
60256700	MANHOLES TO BE ADJUSTED WITH NEW TYPE 11 FRAME AND GRATE	EACH	1
67100100	MOBILIZATION	LSUM	1
70102620	TRAFFIC CONTROL AND PROTECTION STANDARD 701501	LSUM	1
70102635	TRAFFIC CONTROL AND PROTECTION STANDARD 701701	LSUM	1
70102640	TRAFFIC CONTROL AND PROTECTION STANDARD 701801	LSUM	1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	650
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	217
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	130
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	1,067
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	450
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	19
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	38
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	234
X4420132	CLASS D PATCHES (SPECIAL)	SQ YD	636
XX007729	DETECTABLE WARNINGS, SPECIAL	SQ FT	48
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	52

* INDICATES SPECIALTY ITEM

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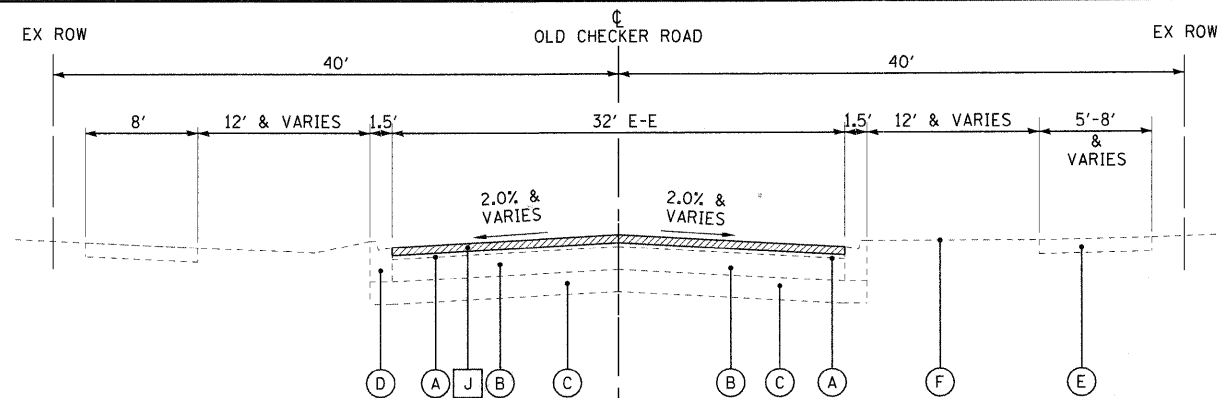
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CHECKED - RWL	REVISED -
DATE - 02-22-10	FILE - 090715-soq.sht

**VILLAGE OF BUFFALO GROVE, ILLINOIS
 OLD CHECKER ROAD ARRA (LAPP)
 RESURFACING**

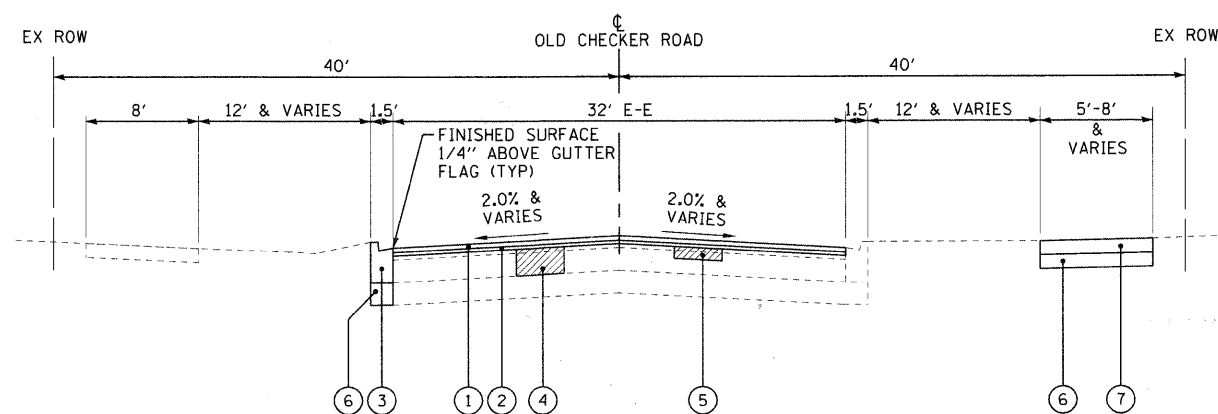
SUMMARY OF QUANTITIES

SCALE: STA. TO STA.

F.A.I.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1268	09-00098-00-RS	LAKE	21	3
C-91-884-09	CONTRACT NO. 63339			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT ARA-9003(453)		



**EXISTING TYPICAL SECTION
OLD CHECKER ROAD
STA. 10+16 TO STA. 51+89**



**PROPOSED TYPICAL SECTION
OLD CHECKER ROAD
STA. 17+75 TO STA. 50+86**

HOT-MIX ASPHALT MIXTURE REQUIREMENTS NOTES:

1. CONTRACTOR SHALL MILL BEFORE PATCHING.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5mm)	4% @ 50 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75mm, N50	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 mm) - 7-INCH & 3-INCH (SPECIAL)	4% @ 70 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ. YD./IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY THE DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

EXISTING LEGEND

- (A) HCT-MIX ASPHALT SURFACE AND BINDER COURSE, 3"
- (B) HOT-MIX ASPHALT BASE COURSE, 6"
- (C) AGGREGATE SUB-BASE, 6"
- (D) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (E) SIDEWALK
- (F) GROUND SURFACE
- (J) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- [Hatched Box] ITEM TO BE REMOVED

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2"
- (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4"
- (3) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS SHOWN ON PLANS OR AS DETERMINED BY THE ENGINEER)
- (4) CLASS D PATCHES, TYPE II-IV, 7" (AS DETERMINED BY THE ENGINEER)
- (5) CLASS D PATCHES, 3" (SPECIAL) (AS DETERMINED BY THE ENGINEER)
- (6) AGGREGATE BASE COURSE, TYPE B - 4" (AS REQUIRED)
- (7) PORTLAND CEMENT CONCRETE SIDEWALK - 5" (SEE NOTE 1)

LEGEND NOTES:

1. LOCATIONS OF SIDEWALK REMOVAL AND PCC SIDEWALK - 5" TO BE AS SHOWN ON PLANS OR AS DETERMINED BY THE ENGINEER.

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DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-09-09	FILE - 090715-tyr_sec.shd

**VILLAGE OF BUFFALO GROVE, ILLINOIS
OLD CHECKER ROAD ARRA (LAPP)
RESURFACING**

**TYPICAL SECTIONS AND
HMA MIXTURE REQUIREMENTS**

SCALE:

STA. 10+00 TO STA. 20+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
126B	09-00098-00-RS	LAKE	21	4
C-91-884-09	CONTRACT NO. 63339			
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT ARA-9003(453)	

HOT-MIX ASPHALT SURFACE REMOVAL – BUTT JOINT

LOCATION	AREA (SQ YD)
OLD CHECKER ROAD (STA 10+16)	36
SPRINGSIDE LANE (STA 14+96 LT)	24
OLD CHECKER ROAD (STA 18+06 RT)	18
OLD CHECKER ROAD (STA 20+20 RT)	19
OLD CHECKER COURT (STA 22+65 RT)	19
OLD CHECKER ROAD (STA 25+19 RT)	17
OLD CHECKER ROAD (STA 25+88 RT)	17
WILLOW STREAM PARK ENTRANCE (STA 29+43 LT)	19
VINTAGE LANE (STA 45+77 LT)	17
MANCHESTER COURT (STA 47+94 RT)	23
OLD CHECKER ROAD (STA 51+89)	28
TOTAL	237

**SIDEWALK REMOVAL
PORTLAND CEMENT CONCRETE SIDEWALK – 5"**

LOCATION	AREA (SQ FT)
OLD CHECKER ROAD (STA 10+30 LT)	327
OLD CHECKER ROAD (STA 10+30 RT)	40
SPRINGSIDE LANE (STA 15+25 LT)	50
OLD CHECKER ROAD (STA 17+90 RT)	89
OLD CHECKER ROAD (STA 18+30 RT)	57
OLD CHECKER ROAD (STA 20+00 RT)	70
OLD CHECKER ROAD (STA 20+35 RT)	42
OLD CHECKER ROAD (STA 20+60 RT)	160
OLD CHECKER ROAD (STA 22+30 RT)	120
OLD CHECKER ROAD (STA 22+30 LT)	70
OLD CHECKER COURT (STA 22+50 RT)	81
OLD CHECKER COURT (STA 22+75 RT)	48
OLD CHECKER ROAD (STA 25+00 RT)	70
OLD CHECKER ROAD (STA 25+40 RT)	48
OLD CHECKER ROAD (STA 25+70 RT)	57
OLD CHECKER ROAD (STA 26+05 RT)	50
WILLOW STREAM PARK ENTRANCE (STA 29+15 LT)	72

LOCATION	AREA (SQ FT)
WILLOW STREAM PARK ENTRANCE (STA 29+75 LT)	106
OLD CHECKER ROAD (STA 29+80 RT)	116
OLD CHECKER ROAD (STA 29+80 LT)	145
OLD CHECKER ROAD (STA 45+55 RT)	128
VINTAGE LANE (STA 45+55 LT)	165
VINTAGE LANE (STA 46+00 LT)	72
MANOR DRIVE (STA 48+75 LT)	56
MANOR DRIVE (STA 49+15 LT)	60
OLD CHECKER ROAD (STA 51+83 RT)	83
OLD CHECKER ROAD (STA 51+83 LT)	65
OLD CHECKER ROAD (STA 51+89 RT)	40
OLD CHECKER ROAD (STA 51+89 LT)	53
OLD CHECKER ROAD (STA 52+66 LT)	66
OLD CHECKER ROAD (STA 52+78 LT)	66
OLD CHECKER ROAD (STA 52+64 RT)	296
TOTAL	2968

**COMBINATION CONCRETE CURB AND GUTTER
REMOVAL AND REPLACEMENT**

LOCATION	LENGTH (FT)
STA 10+25 TO STA 10+49 RT	27
STA 16+70 TO STA 16+77 LT	7
STA 16+94 TO STA 17+10 LT	16
STA 22+02 TO STA 22+17 LT	15
STA 24+24 TO STA 24+38 LT	14
STA 43+30 TO STA 43+40 LT	10
STA 45+34 TO STA 45+66 RT	32
STA 46+03 TO STA 46+25 LT	23
STA 48+67 TO STA 49+17 LT	50
TOTAL	194

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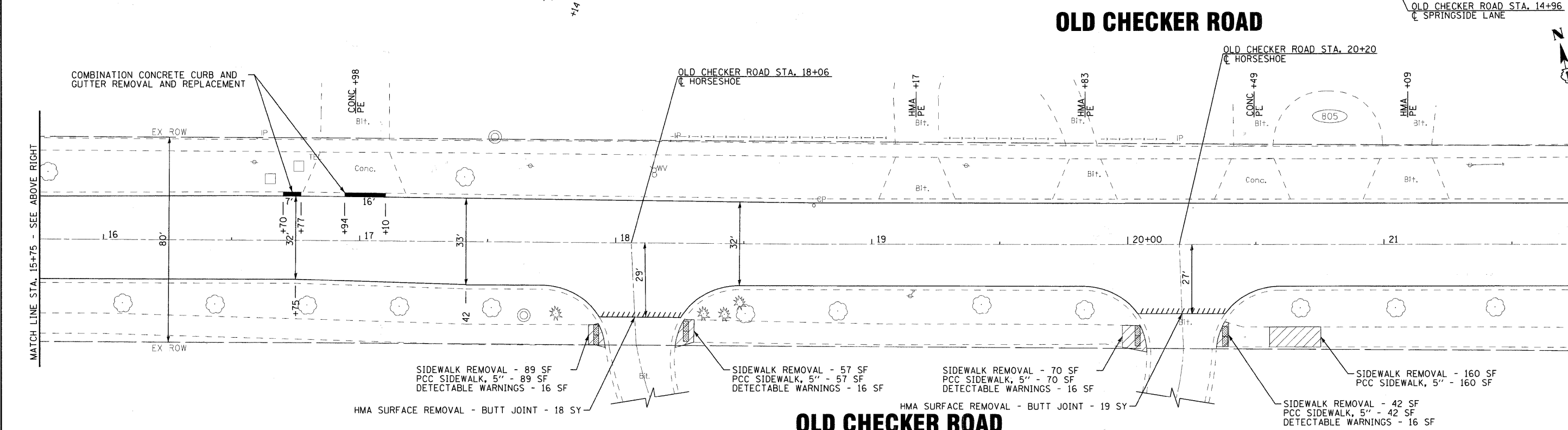
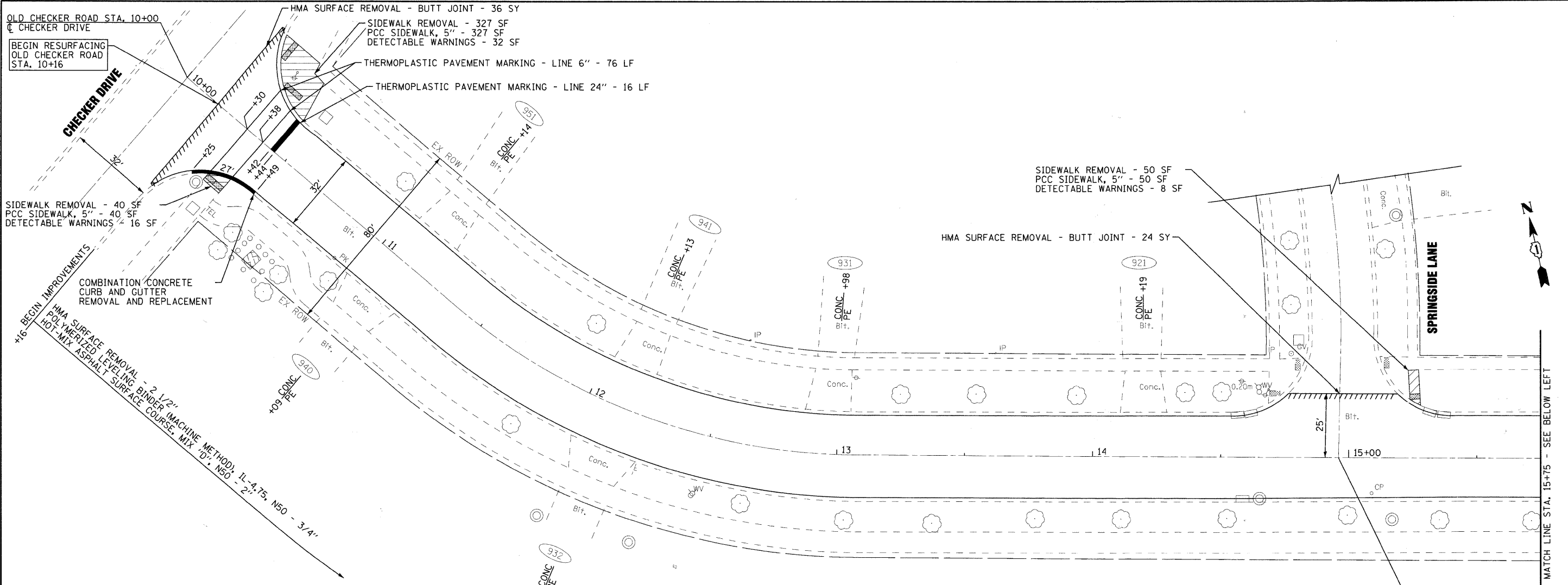
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**VILLAGE OF BUFFALO GROVE, ILLINOIS
OLD CHECKER ROAD ARRA (LAPP)
RESURFACING**

SCHEDULE OF QUANTITIES

SCALE: STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
126B	09-00098-00-RS	LAKE	21	5
C-91-884-09 FED. ROAD DIST. NO. [ILLINOIS]			CONTRACT NO. 63339 FED. AID PROJECT ARRA-90031453	



OLD CHECKER ROAD

**VILLAGE OF BUFFALO GROVE, ILLINOIS
 OLD CHECKER ROAD ARRA (LAPP)
 RESURFACING**

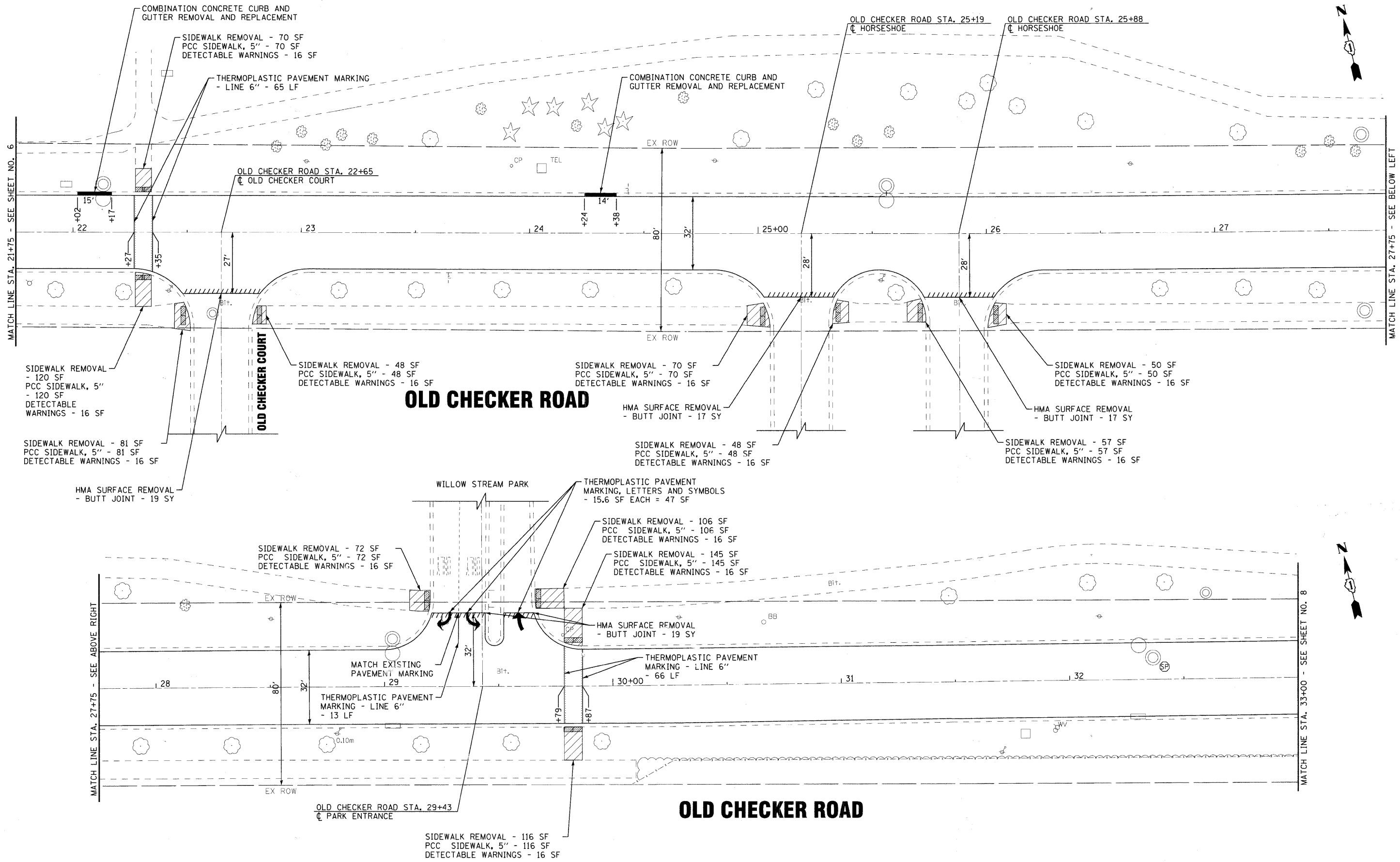
SCHEMATIC PLAN - OLD CHECKER ROAD

DESIGNED - DSH	REVISED-10-09-09 PER IDOT REVIEW
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-09-09	FILE - 090715-ppl.sht

F.A.U. RTE. 126B	SECTION 09-00098-00-RS	COUNTY LAKE	TOTAL SHEETS 21	SHEET NO. 6
SCALE: 1" = 20'		STA. 10+00 TO STA. 21+75		CONTRACT NO. 63339
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT ARRA-9003(453)				

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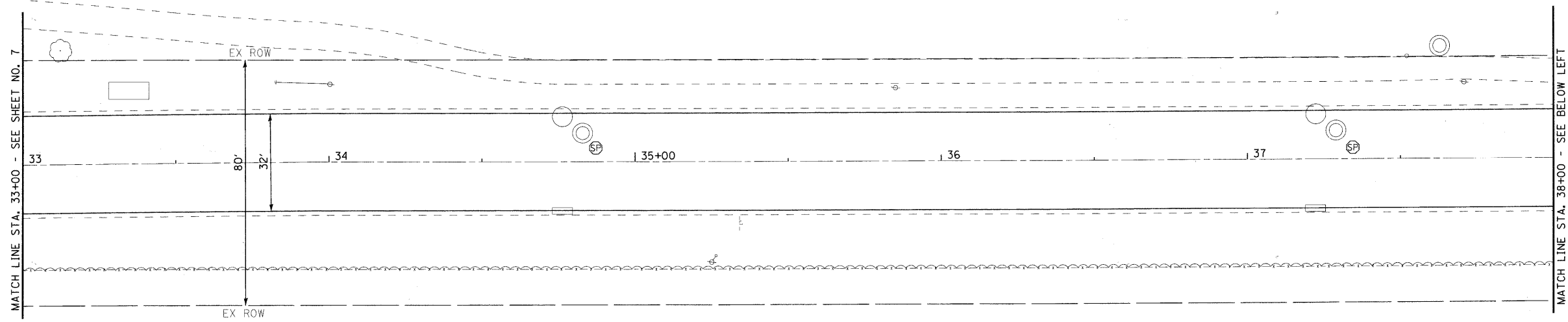
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CHECKED - RWL	REVISED -
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VILLAGE OF BUFFALO GROVE, ILLINOIS
OLD CHECKER ROAD ARRA (LAPP)
RESURFACING

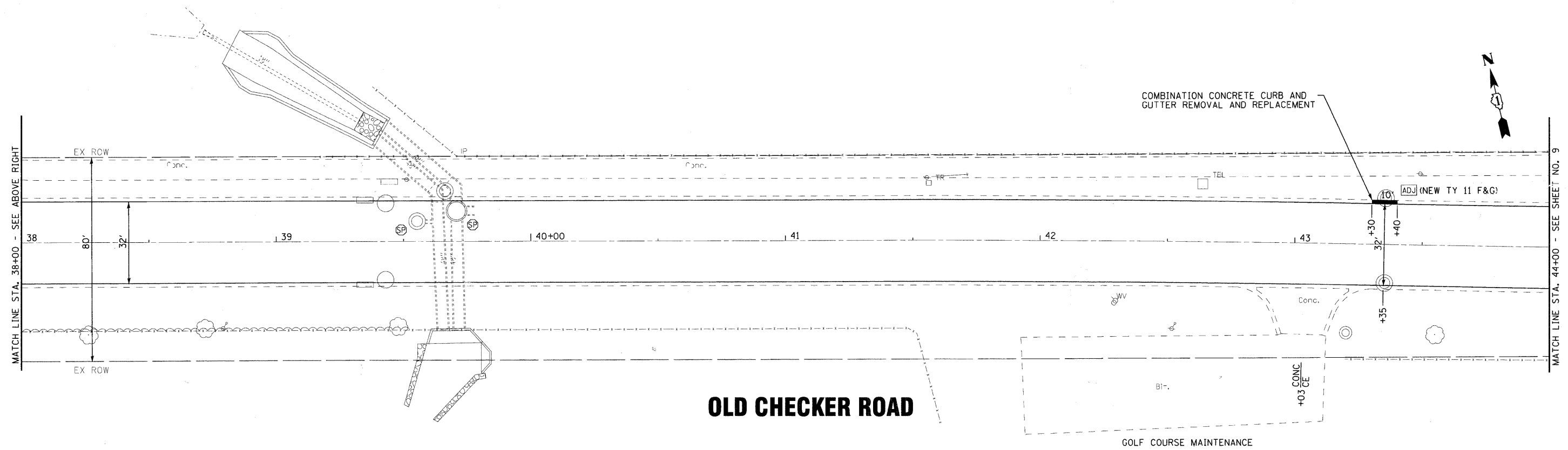
SCHEMATIC PLAN - OLD CHECKER ROAD

SCALE: 1" = 20' STA. 21+75 TO STA. 33+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
126B	09-00098-00-RS	LAKE	21	7
C-91-884-09			CONTRACT NO. 63339	
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT ARA-90034453	



OLD CHECKER ROAD



OLD CHECKER ROAD

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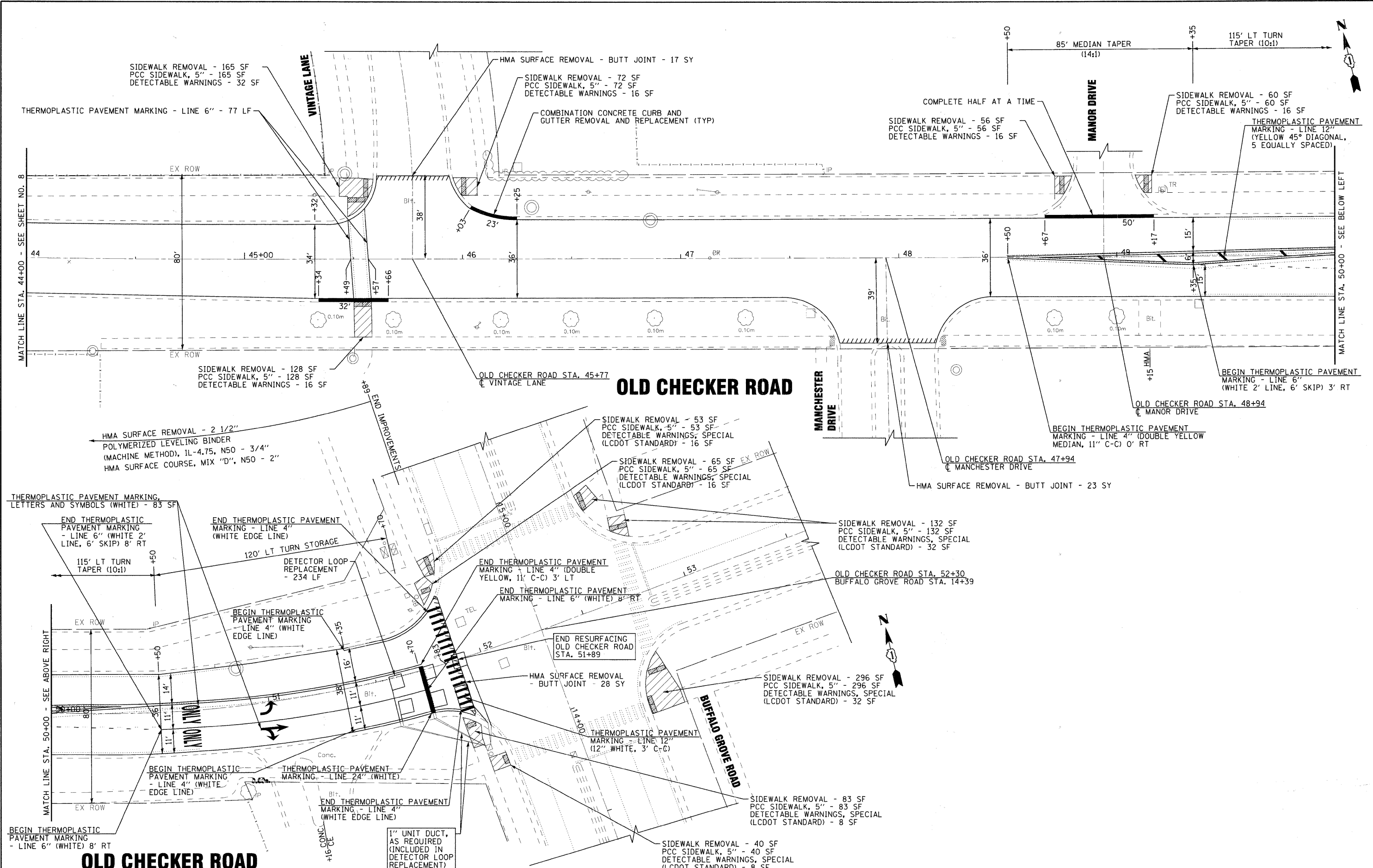
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DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-09-09	FILE - 090715-pp3.sht

**VILLAGE OF BUFFALO GROVE, ILLINOIS
 OLD CHECKER ROAD ARRA (LAPP)
 RESURFACING**

SCHEMATIC PLAN - OLD CHECKER ROAD

SCALE: 1" = 20'
 STA. 33+00 TO STA. 44+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1268	09-00098-00-RS	LAKE	21	8
C-91-884-09	CONTRACT NO. 63339			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT ARA-9003(453)		



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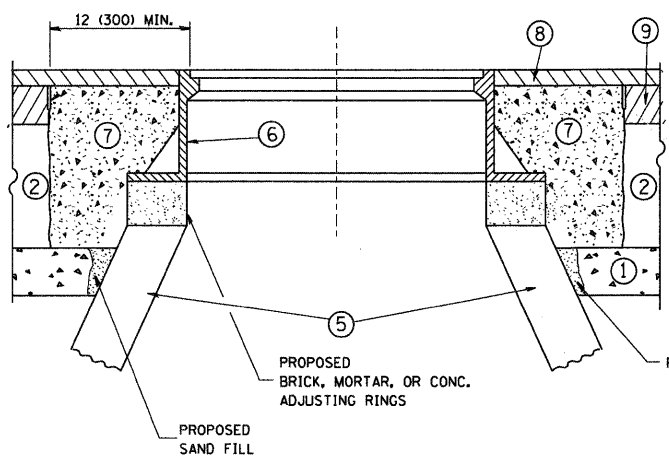
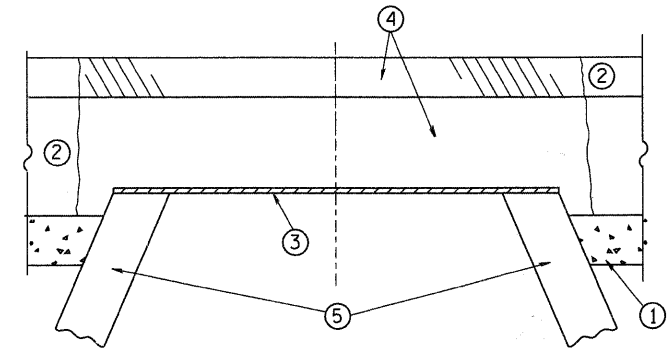
OLD CHECKER ROAD

VILLAGE OF BUFFALO GROVE, ILLINOIS OLD CHECKER ROAD ARRA (LAPP) RESURFACING

SCHEMATIC PLAN - OLD CHECKER ROAD

DESIGNED - DSH	REVISED-10-09-09 PER IDOT REVIEW
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-09-09	FILE - 090715-pp4.shd

SCALE: 1" = 20'	STA. 44+00 TO STA. 53+00	F.A.U. RTE. 1268	SECTION 09-00098-00-RS	COUNTY LAKE	TOTAL SHEETS 21	SHEET NO. 9
			C-91-884-09	CONTRACT NO. 63339		
			FED. ROAD DIST. NO. ILLINOIS	FED. AID PROJECT ARA-900314631		



CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS S1 CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

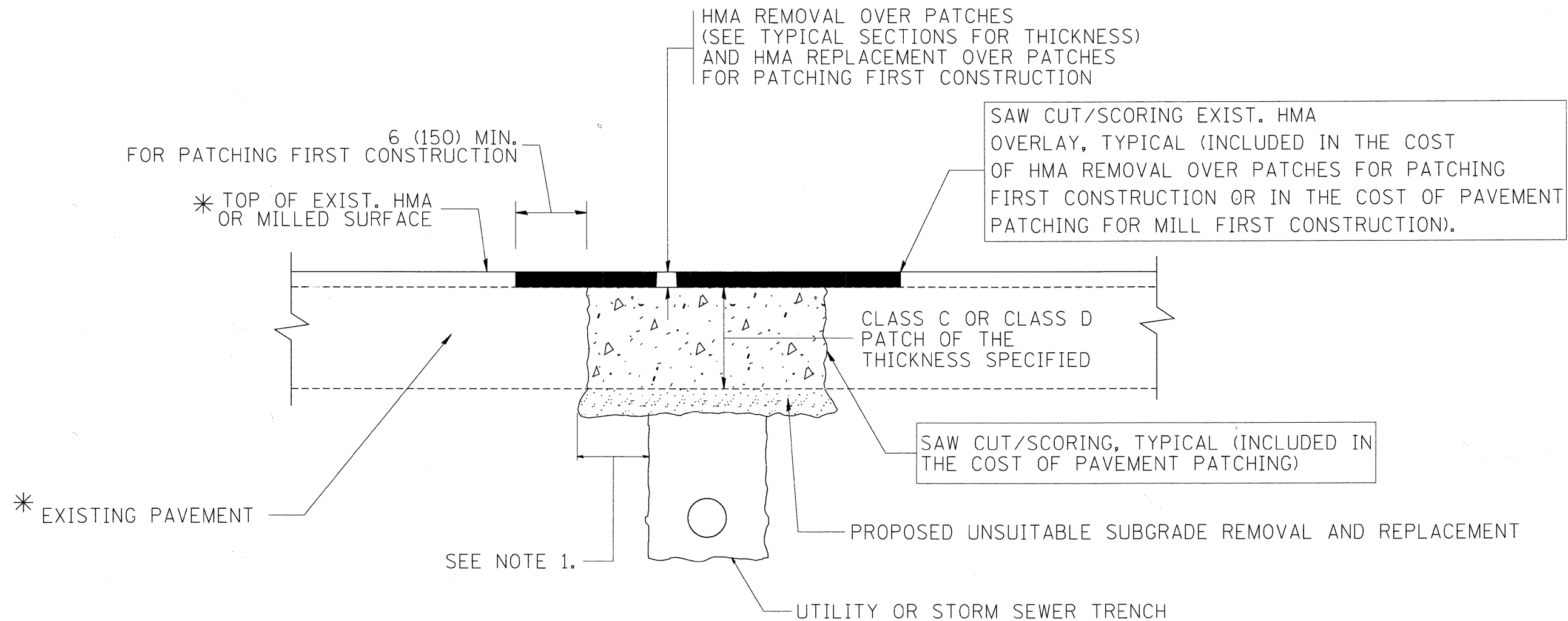
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL". NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\ststet\22x34\bd88.dgn	USER NAME = goglionobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	1268	09-00098-00-RS	LAKE	21	10
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - R. WIEDEMAN 05-14-04				TO STA.	BD600-03 (BD-8)		CONTRACT NO. 63339		
		DATE - 10-25-94	REVISED - R. BORO 01-01-07					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(453)				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

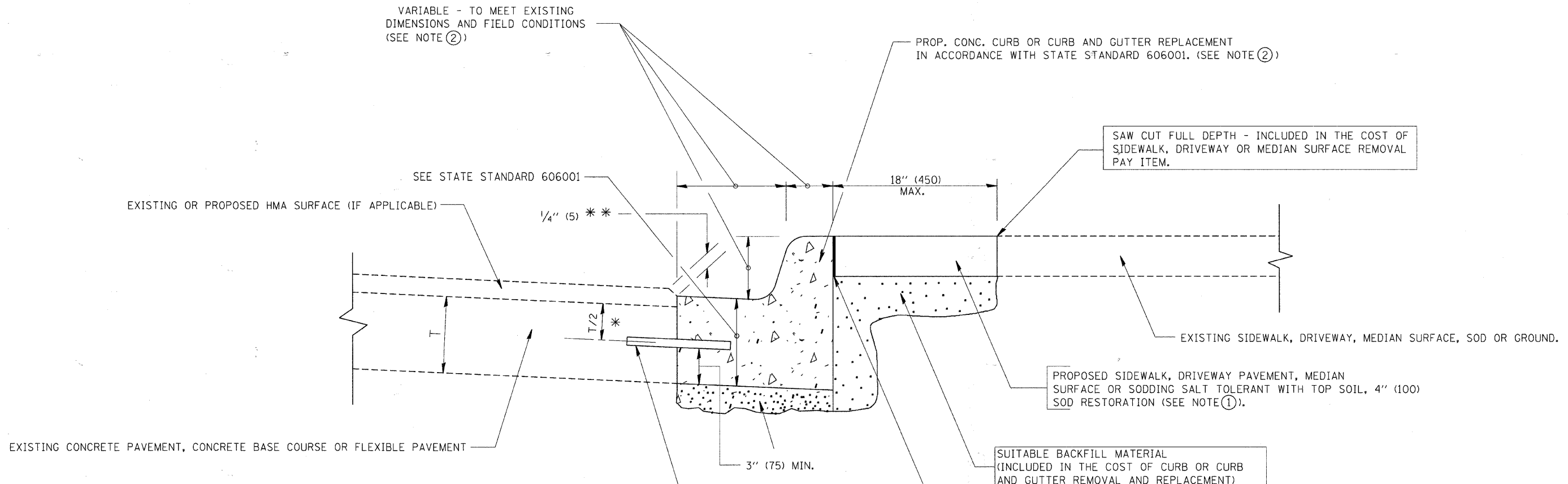
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bouerd1	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A. RTE. 1268	SECTION 09-00098-00-RS	COUNTY LAKE	TOTAL SHEETS 21	SHEET NO. 11
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD400-04 (BD-22)		CONTRACT NO. 63339	
	PLOT DATE = 10/27/2008	CHECKED -	REVISED - R. BORO 09-04-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(453)							
		DATE - 10-25-94	REVISED - K. ENG 10-27-08		C-91-884-09							



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

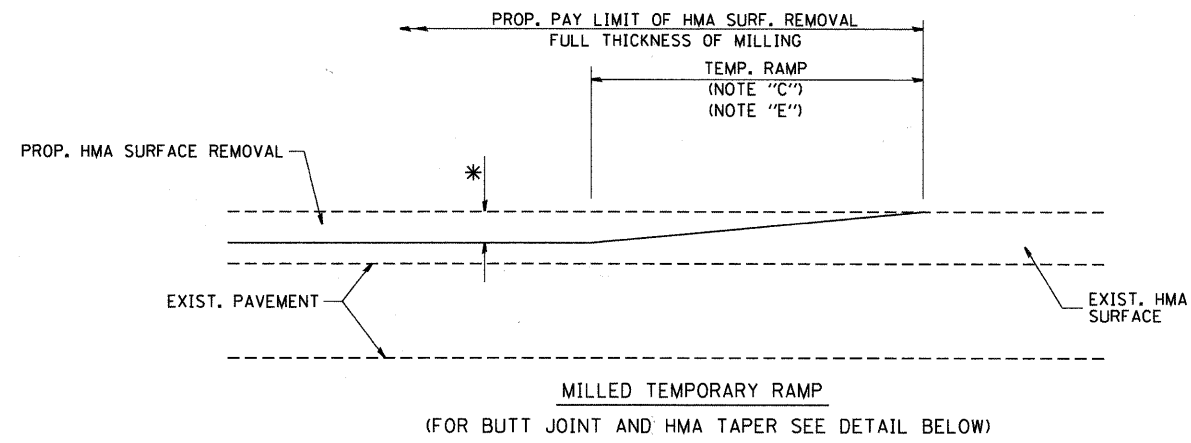
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

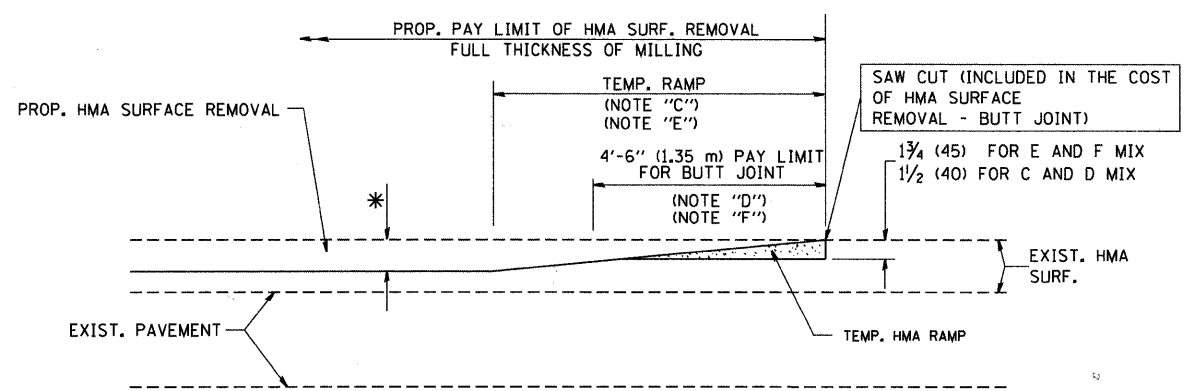
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pwork\pwork\drivakosgn\0108315\bd24.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97			1268	09-00098-00-RS	LAKE	21	12	
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01			BD600-06 (BD-24) CONTRACT NO. 63339					
	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (453)					
						SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.					

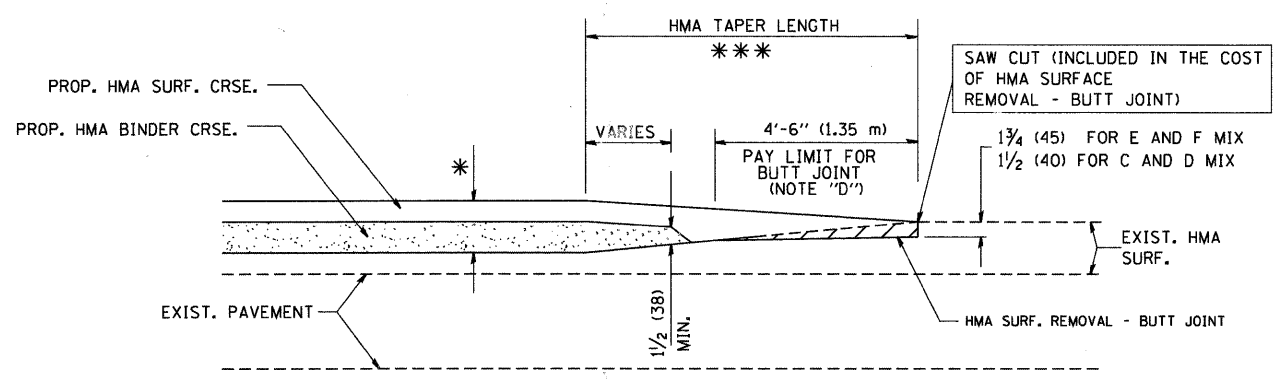


OPTION 1

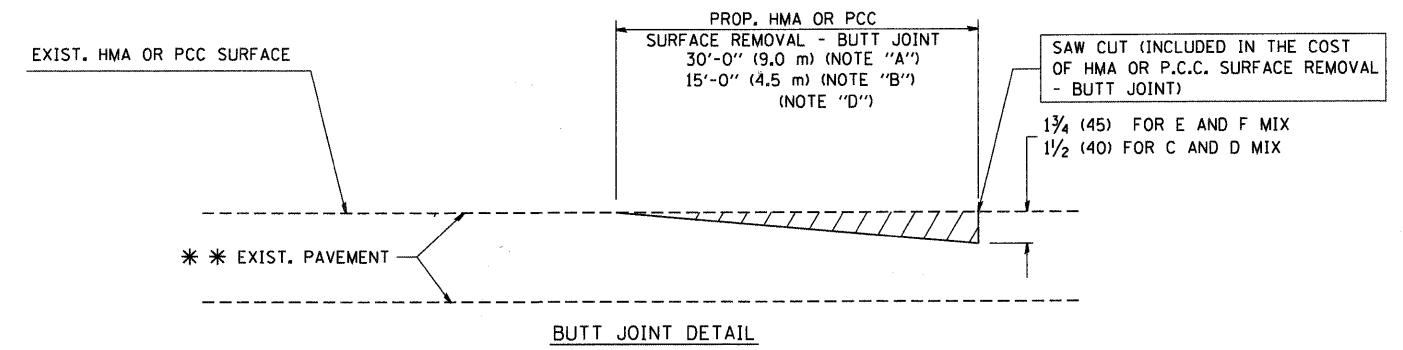


OPTION 2
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

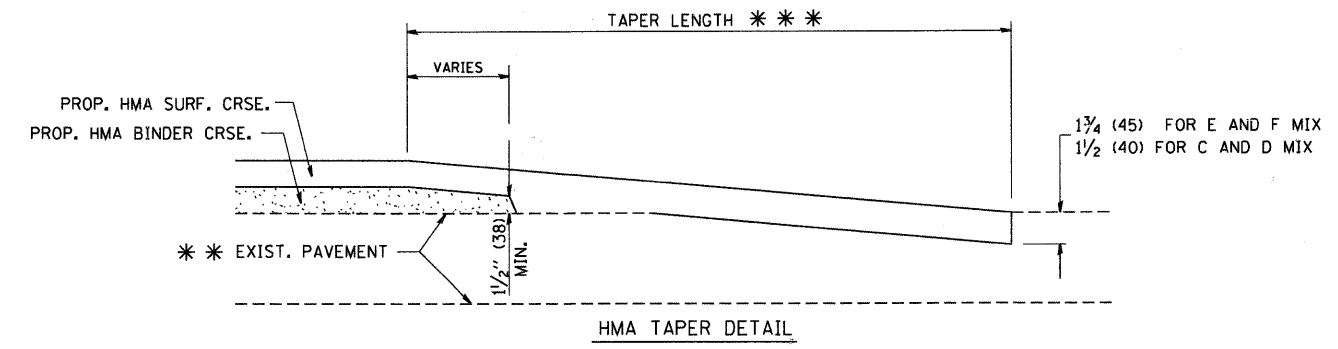
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

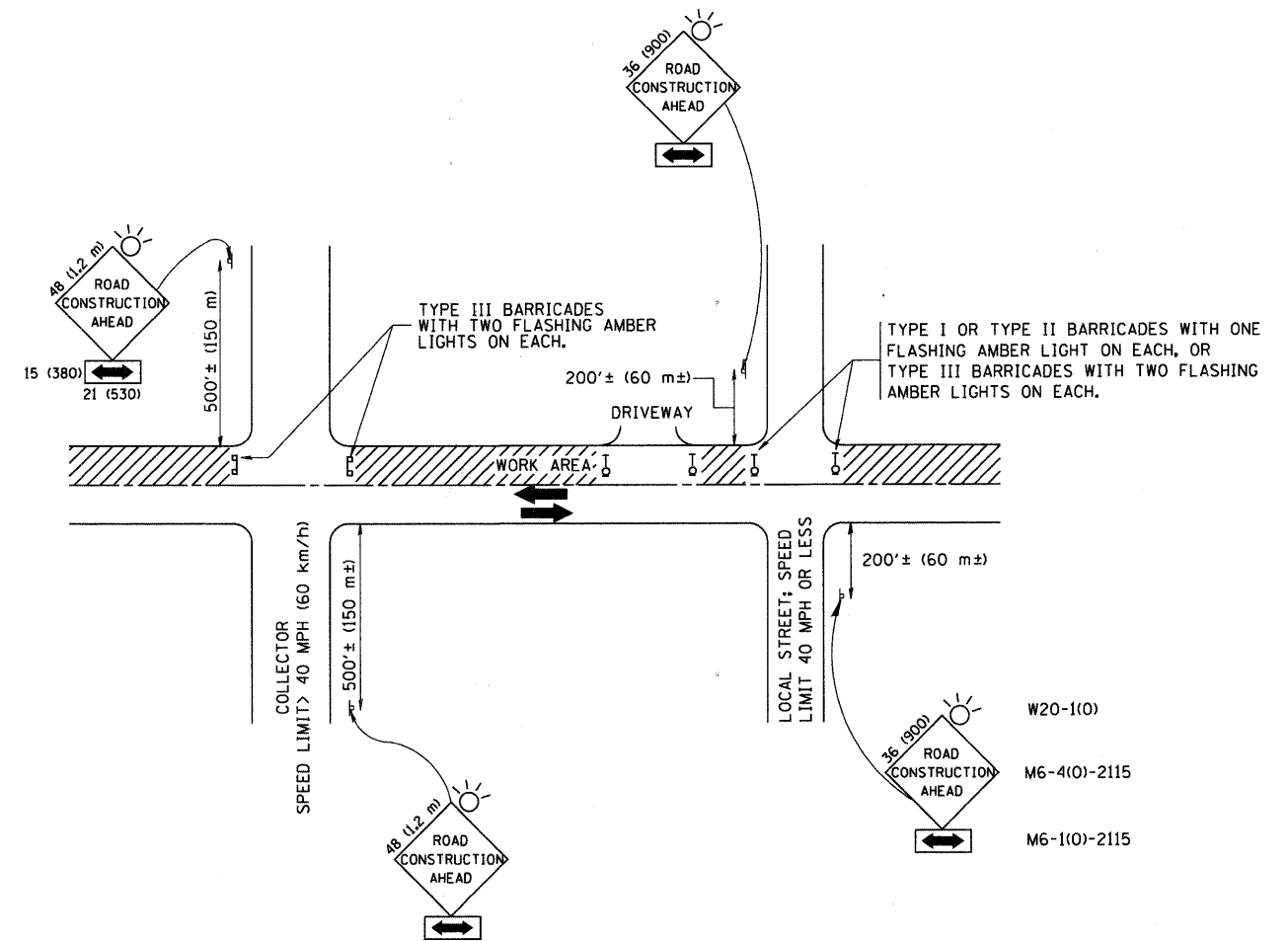
- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\distatd\22x34\bd32.dgn	USER NAME = goglienobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS		F.A.I. RTE. 1268	SECTION 09-00098-00-RS	COUNTY LAKE	TOTAL SHEETS 21	SHEET NO. 13
	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD400-05 BD32		CONTRACT NO. 63339
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - M. GOMEZ 04-06-01		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (453)						
		DATE - 06-13-90	REVISED - R. BORO 01-01-07		C-91-884-09						



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

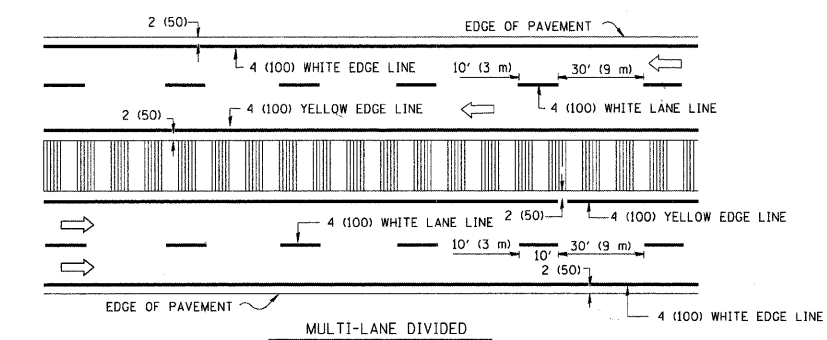
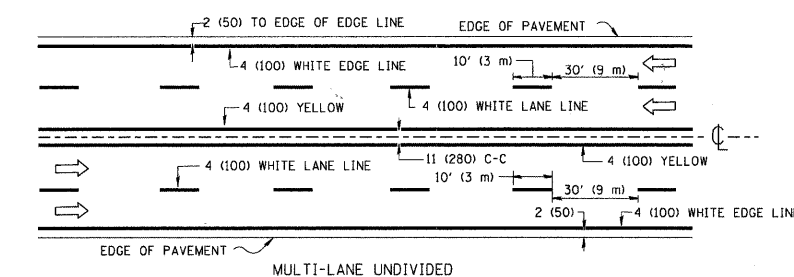
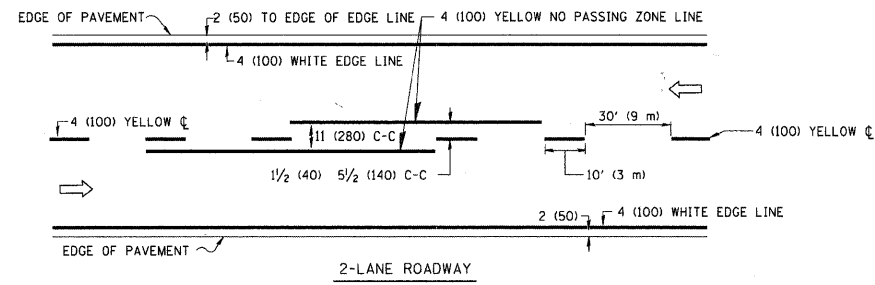
USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

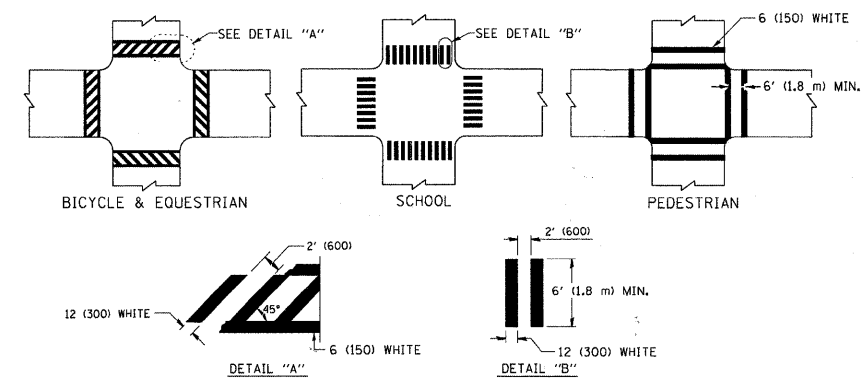
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\diststd\22x34\col0.dgn	USER NAME = geglernobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - A. HOUSEH 03-06-96		1268	09-00098-00-RS	LAKE	21	14			
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			TC-10		CONTRACT NO. 63339		
				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(453) C-91-884-09								

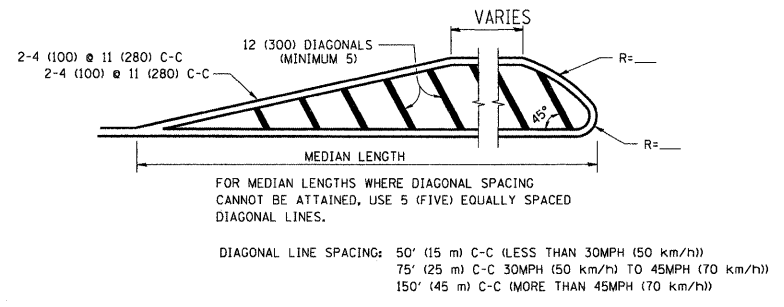
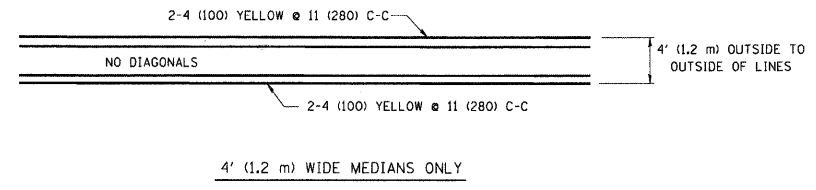


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

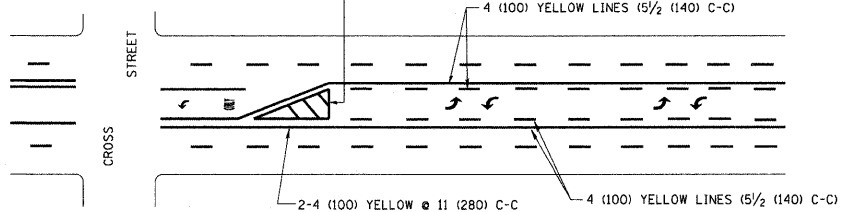
TYPICAL LANE AND EDGE LINE MARKING



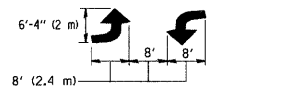
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE

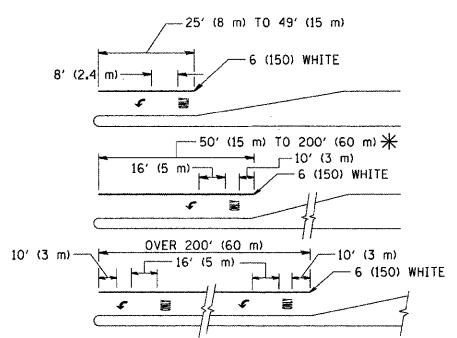


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

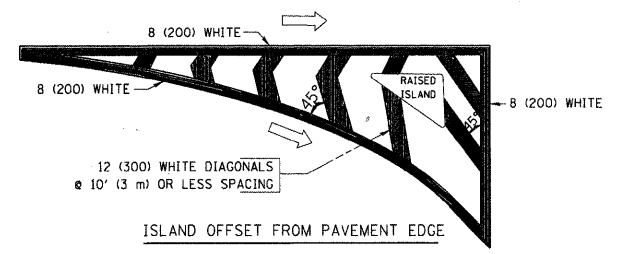


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

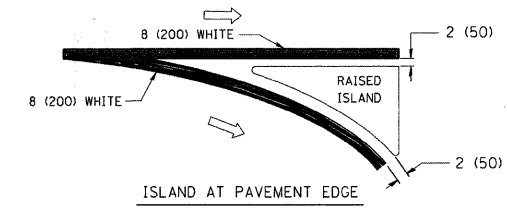
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

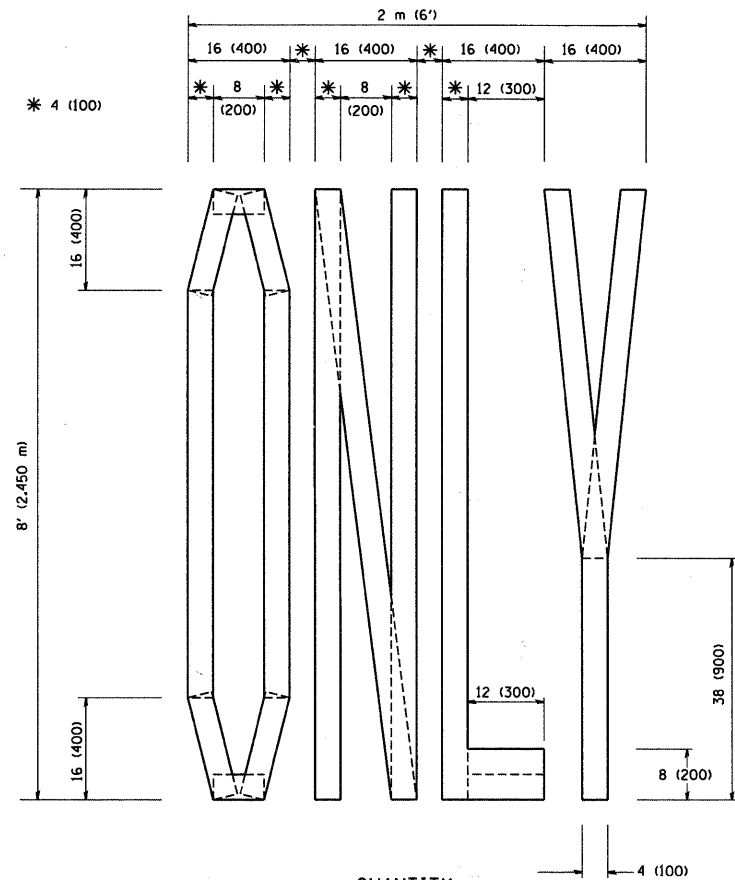
FILE NAME =	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
c:\pwork\pvidot\drivakosgn\0108315\td	3.dgn	DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

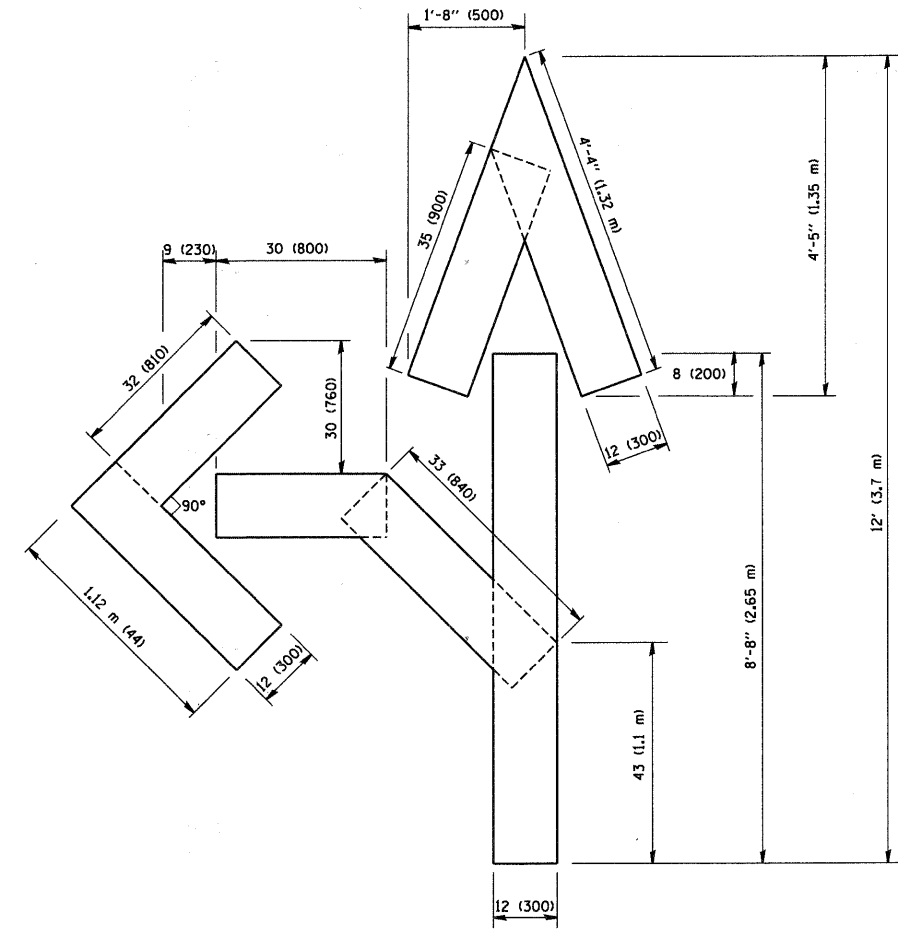
DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

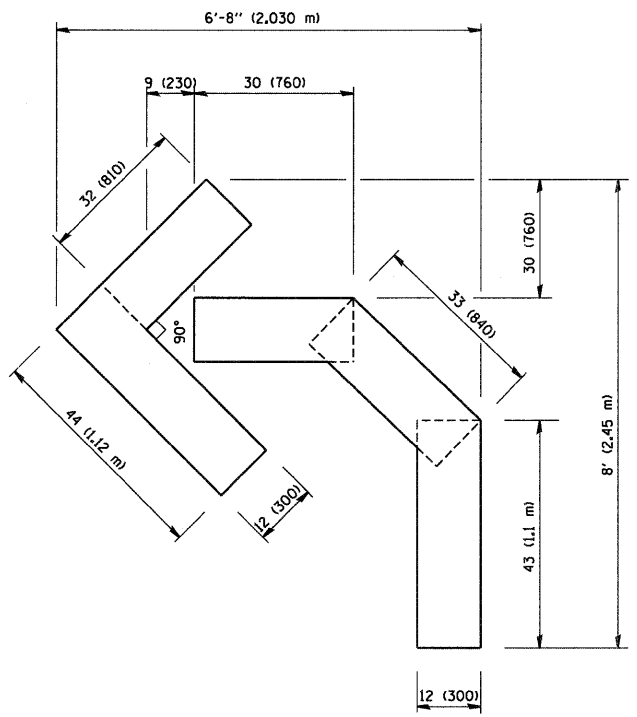
F.A.U. RTE. 1268	SECTION 09-00098-00-RS	COUNTY LAKE	TOTAL SHEETS 21	SHEET NO. 15
TC-13		CONTRACT NO. 63339		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT ARA-9003 (453)				
C-91-884-09				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



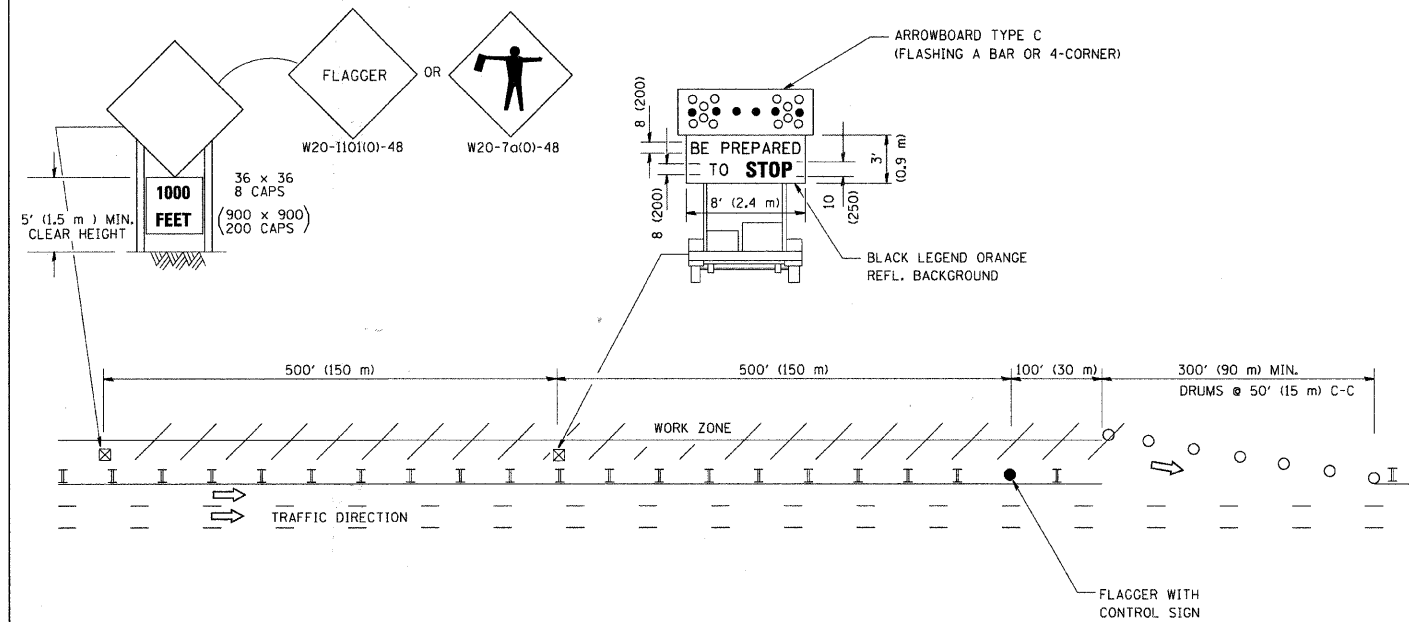
QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

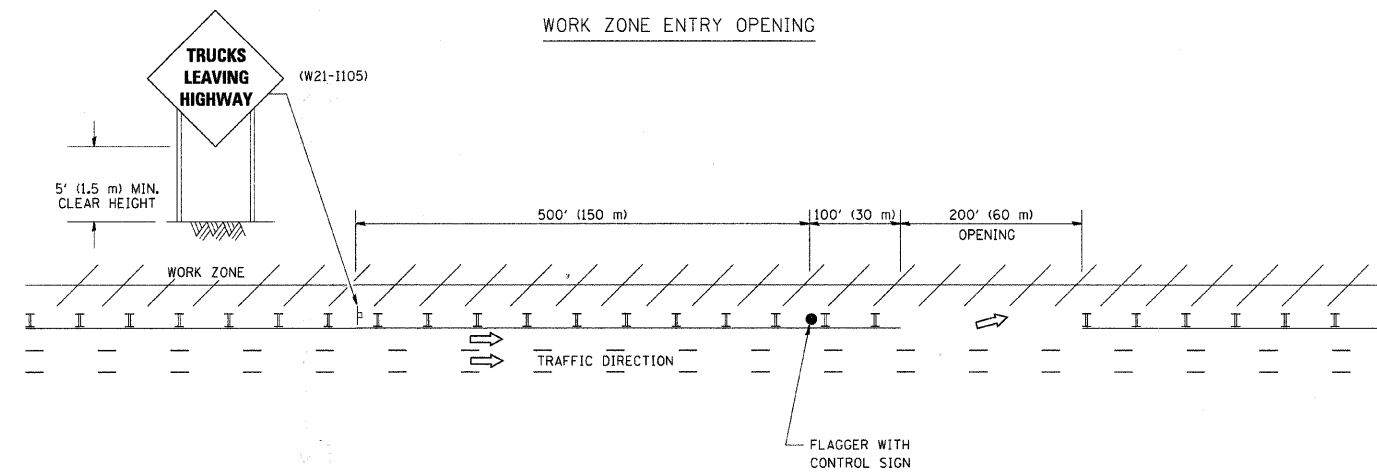
FILE NAME = W:\disto\22x34\16.dgn	USER NAME = goglionob	DESIGNED -	REVISED -T. RAMMACHER 06-05-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			F.A. U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -T. RAMMACHER 11-04-97		1268	09-00098-00-RS	LAKE	21	16			
	PLOT DATE = 1/4/2008	CHECKED -	REVISED -T. RAMMACHER 03-02-98		TC-16			CONTRACT NO. 63339				
		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (453) C-91-884-09				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. THE ARROWBOARD, THE FLAGGER AHEAD SIGN AND THE TRUCKS LEAVING HIGHWAY SIGN SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE EXIT OPENINGS SHOULD BE A MINIMUM OF ONE HALF MILE APART.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\distatd\22x34\1c18.dgn	USER NAME = lcyso	DESIGNED -	REVISED - J.A.F. 04-03
		DRAWN -	REVISED - J.A.F. 02-06
	PLOT SCALE = 50.000 "/ IN.	CHECKED -	REVISED - S.P.B. 01-07
	PLOT DATE = 1/26/2010	DATE -	REVISED - S.P.B. 12-09

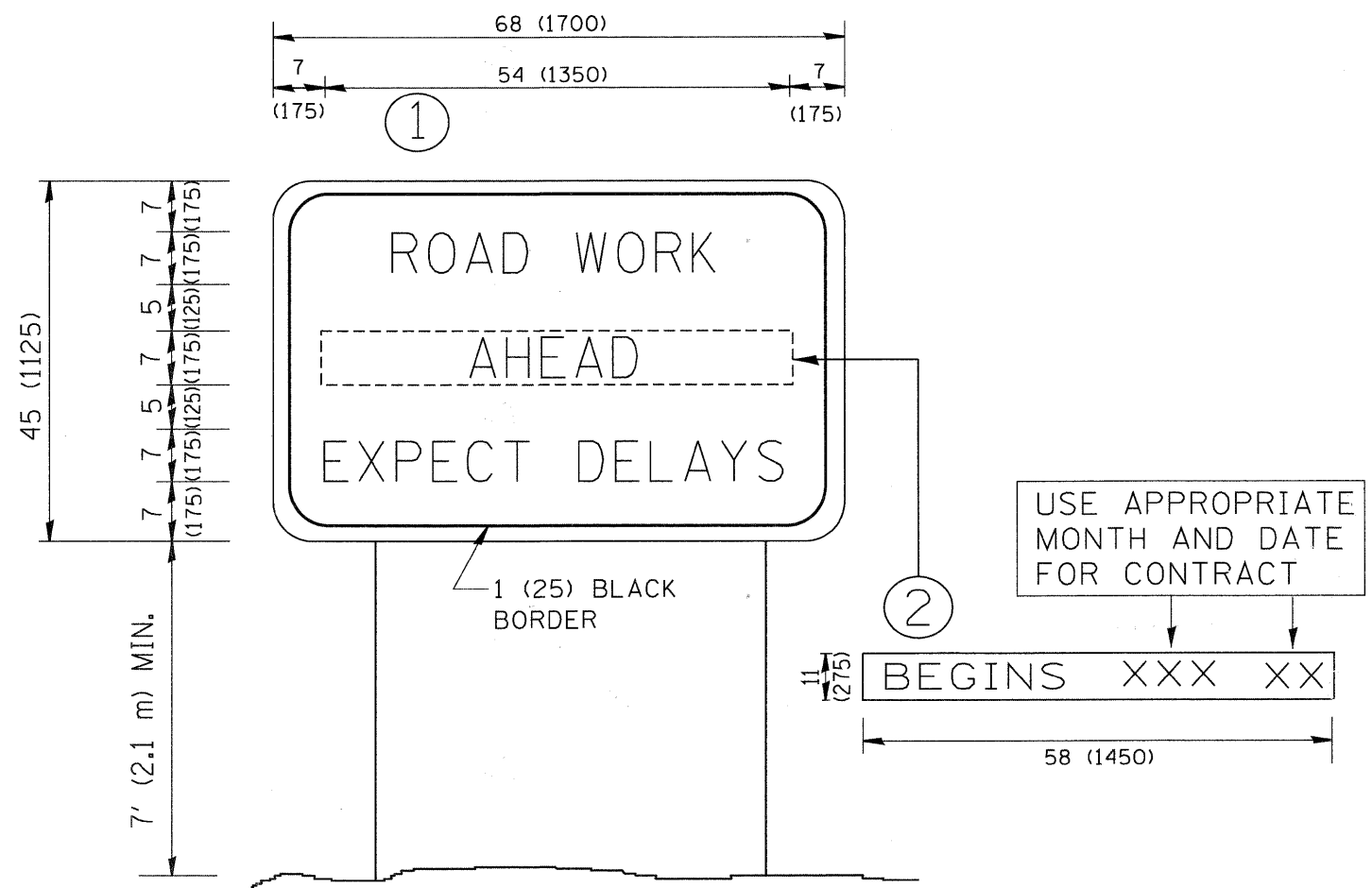
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.D. RTE. 1268	SECTION 09-00098-00-RS	COUNTY LAKE	TOTAL SHEETS 21	SHEET NO. 17
TC-18		CONTRACT NO. 63339		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(453)				

C-91-884-09



NOTES:

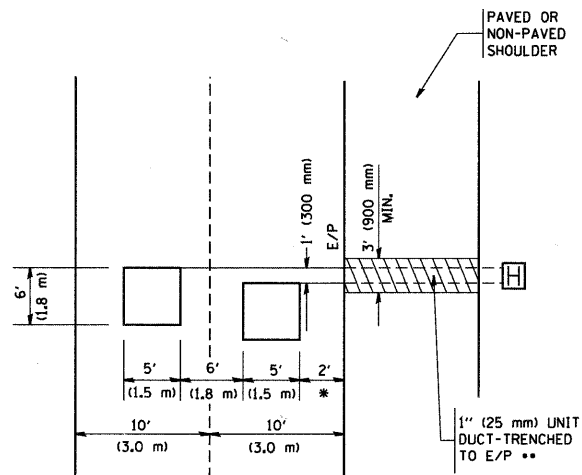
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\distatd\22x34\to22.dgn	USER NAME = goglionobt	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN		F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 50,000' / IN.	DRAWN -	REVISED - R. MIRS 12-11-97		1268	09-00098-00-RS	LAKE	21	18			
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - T. RAMMACHER 02-02-99		SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		TC-22 CONTRACT NO. 63339	
		DATE -	REVISED - C. JUCIUS 01-31-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(453)							

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

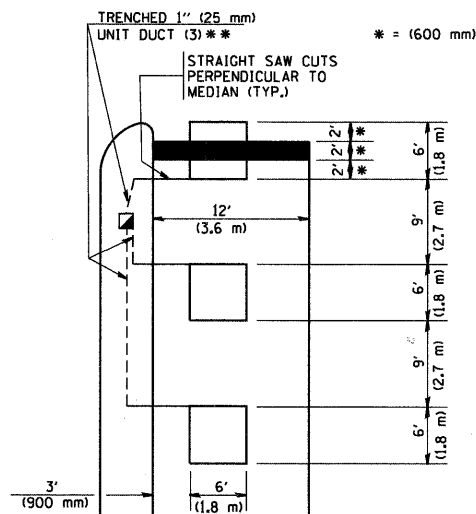


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

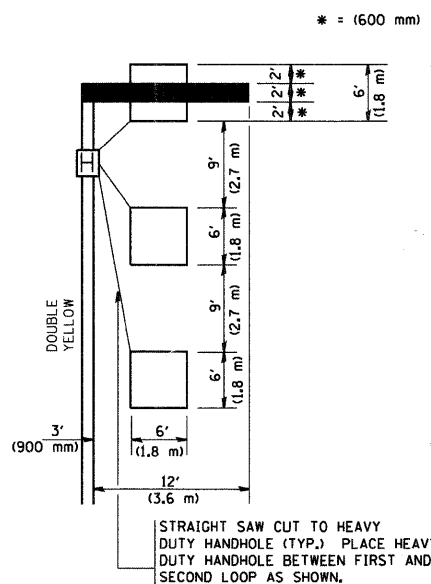


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

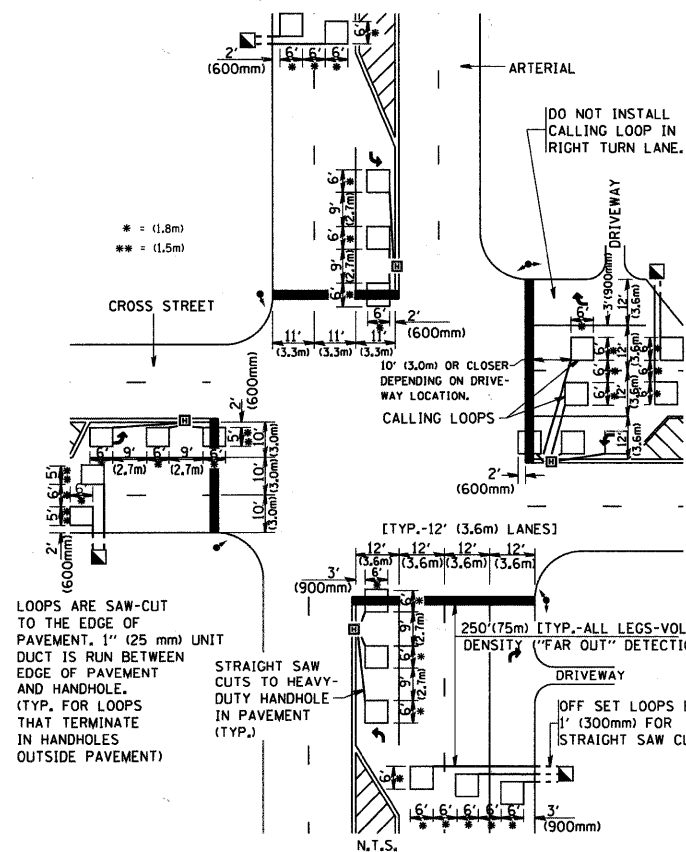
**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



* = (600 mm)

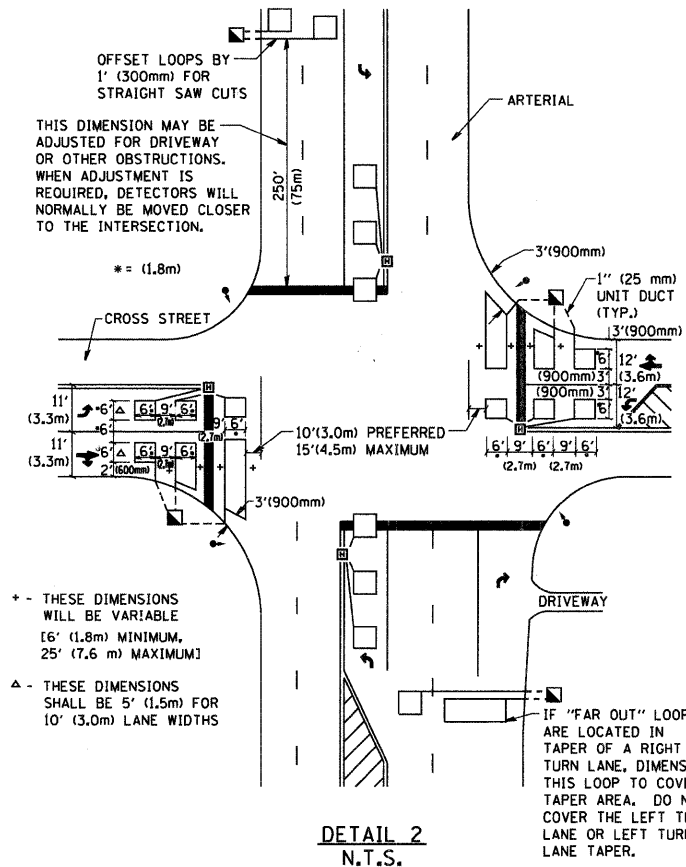
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

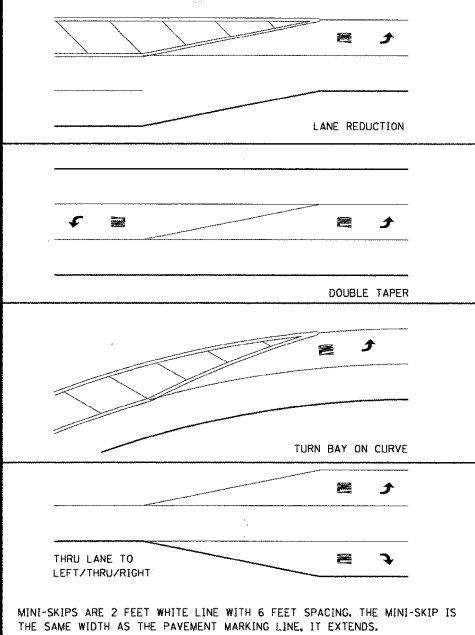
ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\dststd\22x34\ts07.dgn	USER NAME = gajlionob	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE = 50,0000' / IN.	CHECKED - R.K.F.	DRAWN -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	1268	09-00098-00-RS	LAKE	21	19
PLOT DATE = 1/4/2008	DATE -	DRAWN -	REVISED -		TO STA.			TS-07		CONTRACT NO. 63339		
								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(453)				

ROUTE	SECTION	COUNTY	SHEET	SHEETS
CH		LAKE		
TYPICAL PAVEMENT MARKINGS				
ILLINOIS				

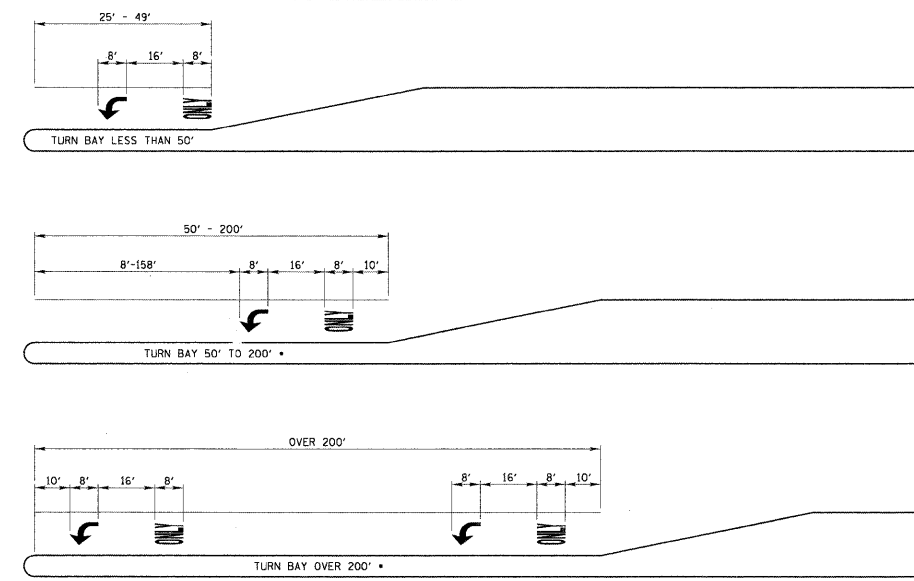
TYPICAL MINI-SKIP PAVEMENT MARKINGS



MINI-SKIPS ARE 2 FEET WHITE LINE WITH 6 FEET SPACING. THE MINI-SKIP IS THE SAME WIDTH AS THE PAVEMENT MARKING LINE, IT EXTENDS.

TYPICAL PAVEMENT MARKINGS

TYPICAL TURN BAY PAVEMENT MARKINGS



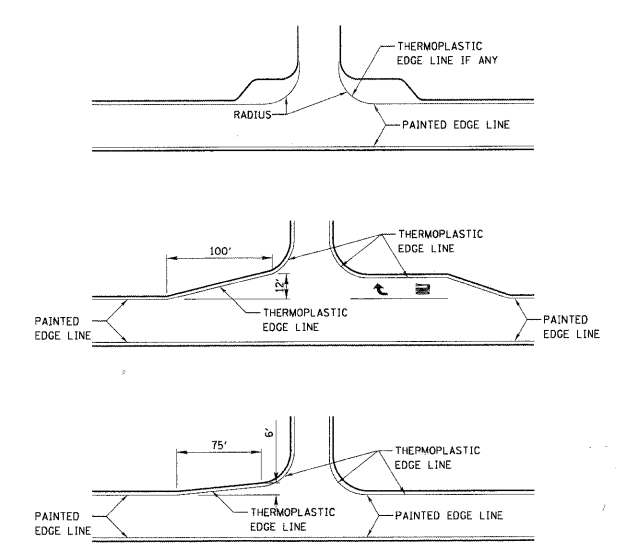
• AT INTERSECTIONS WITH VIDEO DETECTION, THE ARROW AND ONLY PAVEMENT MARKINGS SHALL BE A MINIMUM OF 30' BEHIND THE STOP BAR.

AREA = 15.6 SQ. FT.

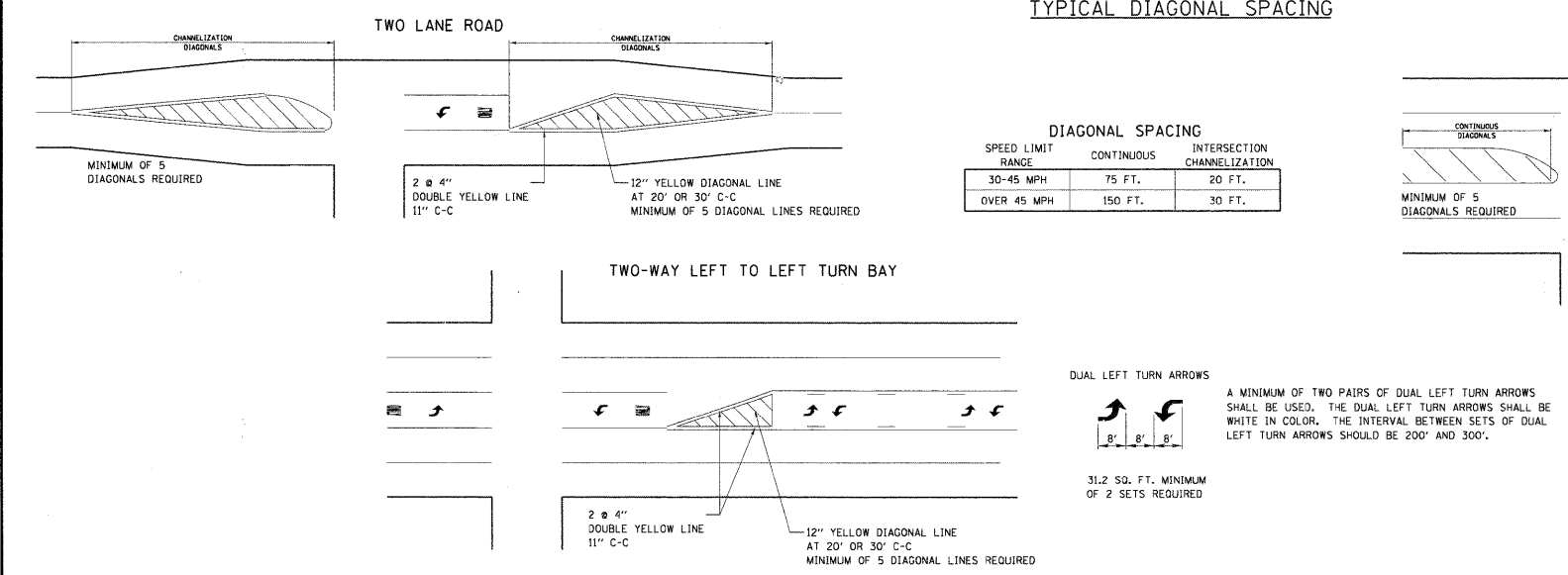
ONLY AREA = 20.8 SQ. FT.

FULL SIZE LETTERS (8") AND ARROWS SHALL BE USED. TURN LANES IN EXCESS OF 400' IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW W/ "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW W/ "ONLY".

EDGE LINE RADIUS AT SIDE STREETS



TYPICAL DIAGONAL SPACING



REVISIONS		Lake County Division of Transportation	
NAME	DATE	TYPICAL PAVEMENT MARKINGS FOR COUNTY HIGHWAYS SCALE: NONE SHEET 1 OF 3 DRAWN BY: JPS DATE: JAN. 12, 1998 CHECKED BY: ANK	
JOHN SAUTER	7/7/99		
JOHN SAUTER	11/01/00		
JOHN SAUTER	5/4/05		

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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 84-00027 - EXPIRES 4/30/2011
 535 PLYMOUTH STREET, CHICAGO, IL 60618
 312.467.2009



DESIGNED - DSH	REVISED-10-09-09 PER IDOT REVIEW
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-09-09	FILE - 090715-PAVEMENT MARKINGS.SHT

**VILLAGE OF BUFFALO GROVE, ILLINOIS
OLD CHECKER ROAD ARRA (LAPP)
RESURFACING**

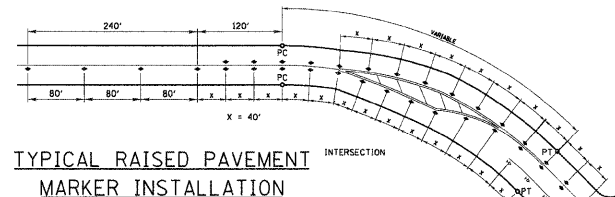
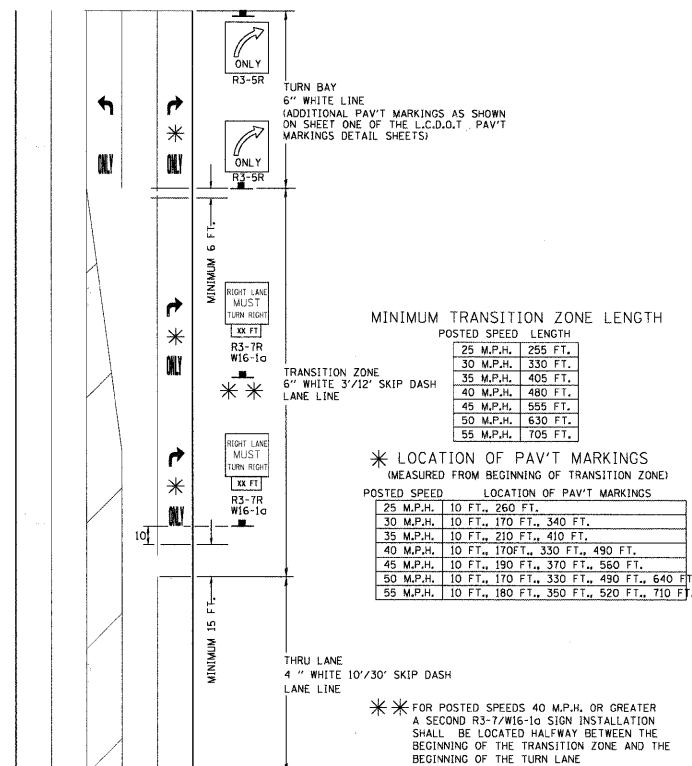
LAKE COUNTY TYPICAL PAVEMENT MARKINGS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1268	09-00098-00-RS	LAKE	21	20
C-91-884-09		CONTRACT NO. 63339		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT ARA-9003(453)		

ROUTE	SECTION	COUNTY	SHEET	SHEETS
CH	LAKE			
TYPICAL PAVEMENT MARKINGS AND MARKERS				
ILLINOIS				

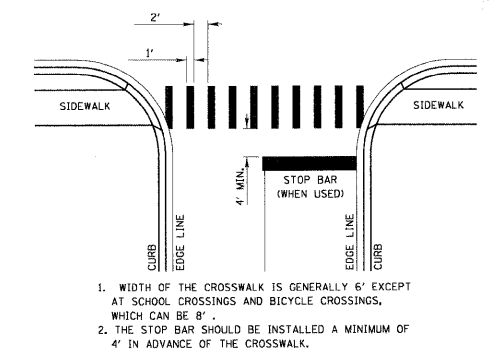
TYPICAL PAVEMENT MARKINGS AND RAISED PAVEMENT MARKERS

THRU LANE TO TURN LANE CONVERSION

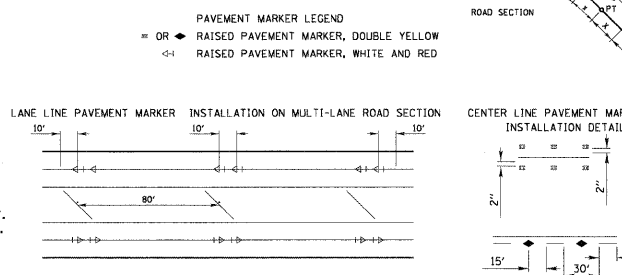


- NOTES:
- CENTERLINE RAISED PAVEMENT MARKERS (RPM'S) SHALL BE PLACED ON ALL CURVES OVER 3 1/2 DEGREES ON ALL TWO AND THREE LANE HIGHWAYS, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
 - SPACING = 40' FOR CENTERLINE MARKERS.
 - ALL RPM'S ON CENTERLINE ARE 2-WAY YELLOW. LANE LINE MARKERS ARE WHITE/RED.
 - MARKERS SHALL BE INSTALLED IN ACCORDANCE WITH F.H.W.A. MEMORANDUM HD-21.
 - MARKERS SHALL BE FIELD ADJUSTED TO BE LOCATED IN CENTER OF THE 30' GAP OF A 30'/10' SKIP-DASH CENTERLINE.
 - RPM'S WHICH ARE TO BE LOCATED WITHIN THE INTERSECTION OF A CROSS STREET, SHALL NOT BE INSTALLED.
 - A MINIMUM OF 4 WHITE/RED MARKERS SHALL BE INSTALLED ALONG THE TURN LANE LINE.
 - RPM'S INSTALLED ON MULTI-LANE ROAD SECTIONS SHALL BE INSTALLED ON THE WHITE SKIP-DASH LANE LINE ONLY. THESE RPM'S SHALL BE INSTALLED IN PAIRS AND SHALL BE 80' CENTER TO CENTER. SPACING WITHIN EACH PAIR SHALL BE 10', CENTERED WITHIN THE 30' SKIP.

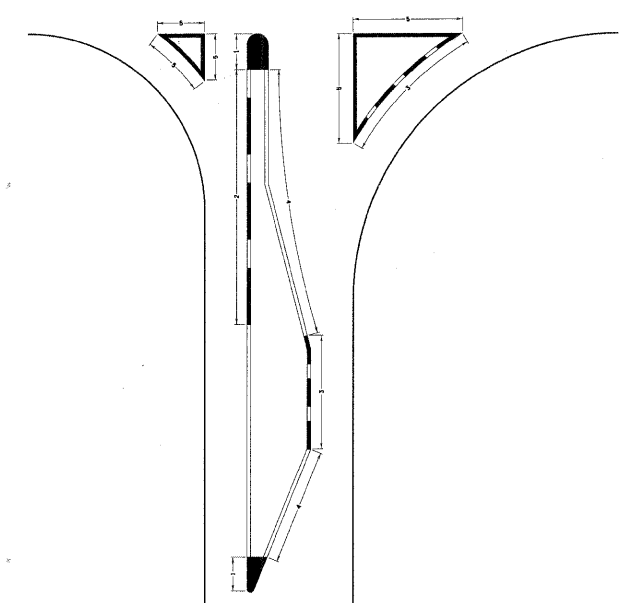
CROSSWALKS



- WIDTH OF THE CROSSWALK IS GENERALLY 6' EXCEPT AT SCHOOL CROSSINGS AND BICYCLE CROSSINGS, WHICH CAN BE 8'.
- THE STOP BAR SHOULD BE INSTALLED A MINIMUM OF 4' IN ADVANCE OF THE CROSSWALK.



CURB MARKING



- NOTES:
- PAINT CURB AND NOSE SOLID FOR 10' OR RADIUS OF NOSE, WHICHEVER IS GREATER.
 - PAINT MINIMUM OF 3 STRIPES IN DIRECTION OF TRAFFIC.
 - REDUCED SPACING USED TO OBTAIN 3 STRIPE MINIMUM.
 - STRIPING RECOMMENDED ONLY WHERE OPERATIONAL PROBLEMS DICTATE.
 - PAINT SOLID WHERE A MINIMUM OF 3 STRIPES CANNOT BE PLACED.

Lake County
Division of Transportation

TYPICAL PAVEMENT MARKINGS FOR COUNTY HIGHWAYS

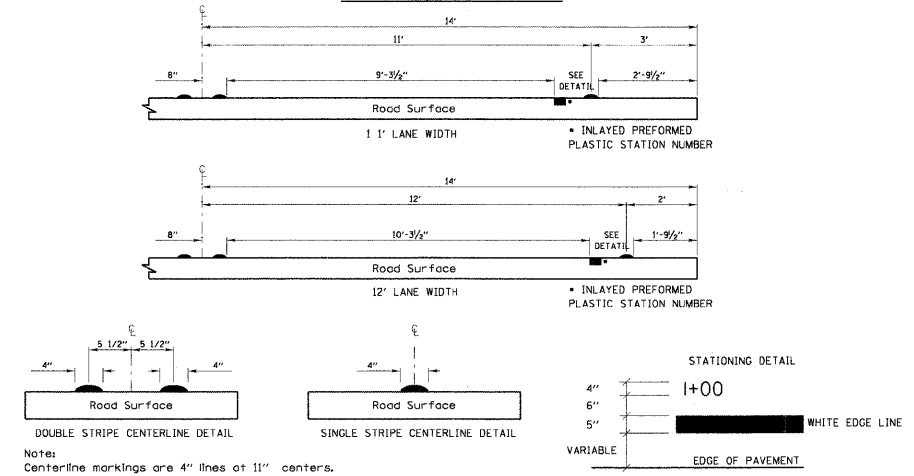
REVISIONS	DATE
NAME	DATE
JOHN SAUTER	7/7/99
JOHN SAUTER	11/01/00
JOHN SAUTER	1/01/02
JOHN SAUTER	5/4/05
JOHN SAUTER	5/24/09

SCALE: NONE SHEET 2 OF 3 DRAWN BY: JPS
DATE: JAN. 12, 1998 CHECKED BY: ANJ

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE OF 2 LANE PAVEMENT	4 IN.	SKIP-DASH	YELLOW	10 FT. LINE WITH 30 FT. SPACE
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 IN.	SOLID	YELLOW	5 1/2 IN. C.C. FROM SKIP-DASH CENTERLINE
CENTERLINE ON MULTI-LANE UNDIVIDED LANE LINES	2 @ 4 IN.	SOLID	YELLOW	11 IN. C.C.
LANE LINES	4 IN.	SKIP-DASH	WHITE	10 FT. LINE WITH 30 FT. SPACE
DOTTED LINES (EXTENSIONS OF CENTERLINE ON TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2 FT. LINE WITH 6 FT. SPACE
EDGE LINES	5 IN. WHITE 4 IN. YELLOW	SOLID	WHITE - RIGHT YELLOW - LEFT	OUTLINE RAISED MEDIAN IN YELLOW
TURN LANE MARKINGS	6 IN. LINE FULL SIZE LETTERS AND SPACED 8 FT.	SOLID	WHITE	TURN ARROW 15.5 SOFT. STRAIGHT ARROW 14.5 SOFT. ONLY SOLID SOFT. COMB. ARROW 20.0 SOFT.
TWO WAY LEFT TURN MARKING	2 @ 4 IN. EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10 FT. LINE WITH 30 FT. SPACE FOR SKIP-DASH AND SOLID LINE
CROSSWALK	12 IN. @ 90°	SOLID	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
STOP BARS	24 IN.	SOLID	WHITE	12 IN. LONGITUDINAL BAR WITH 24 IN. SPACE 6 FT. TO 12 FT. WIDE SEE TYPICAL CROSSWALK MARKING DETAIL
PAINTED MEDIAN	2 @ 4 IN. WITH 11 IN. DIAGONALS @ 45° NO DIAGONALS USED FOR 4 FT. WIDE MEDIAN	SOLID	YELLOW - 2-WAY TRAFFIC WHITE - 1-WAY TRAFFIC	PLACE 4 FT. IN ADVANCE OF AND PARALLEL TO CROSSWALK IF PRESENT OTHERWISE PLACE AT DESIRED STOPPING POINT. 11 IN. C.C. FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING DETAIL
COPE MARKING AND CHANNELIZING LINES	8 IN. WITH 12 IN. DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS 15 FT. C.C. (LESS THAN 30 MPH) 20 FT. C.C. (30 TO 45 MPH) 30 FT. C.C. (OVER 45 MPH) MINIMUM OF 5 DIAGONALS
R.R. CROSSING	24 IN. TRANSVERSE LINES RR IS 6 FT. LETTER 16 IN. LINE FOR "X"	SOLID	WHITE	SEE I.D.D.T. STD. 78000 "SALT AREA OF" 1" - 3/8 SOFT. / "R" 1" - 3/8 SOFT. / "R" 1" - 3/8 SOFT.
SHOULDER DIAGONALS	12 IN. @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50 FT. C.C. (LESS THAN 30 MPH) 75 FT. C.C. (30 TO 45 MPH) 100 FT. C.C. (OVER 45 MPH) MINIMUM OF 5 DIAGONALS

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO PART "MARKINGS" IN THE "LANSING MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES: THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND I.D.D.T. HIGHWAY STANDARD 78000 EFFECTIVE JAN. 9, 1998.

PAVEMENT CROSS SECTION SHOWING TYPICAL PAVEMENT MARKINGS (2-LANE ROADWAY)



Note: Centerline markings are 4" lines at 11' centers.

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 07/13/2009 6:48:29 AM



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VILLAGE OF BUFFALO GROVE, ILLINOIS OLD CHECKER ROAD ARRA (LAPP) RESURFACING

LAKE COUNTY TYPICAL PAVEMENT MARKINGS

SCALE:	STA.	TO STA.	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			1268	09-00098-00-RS	LAKE	21	21
			C-91-884-09	CONTRACT NO. 63339			
			FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	ARA-90034563	