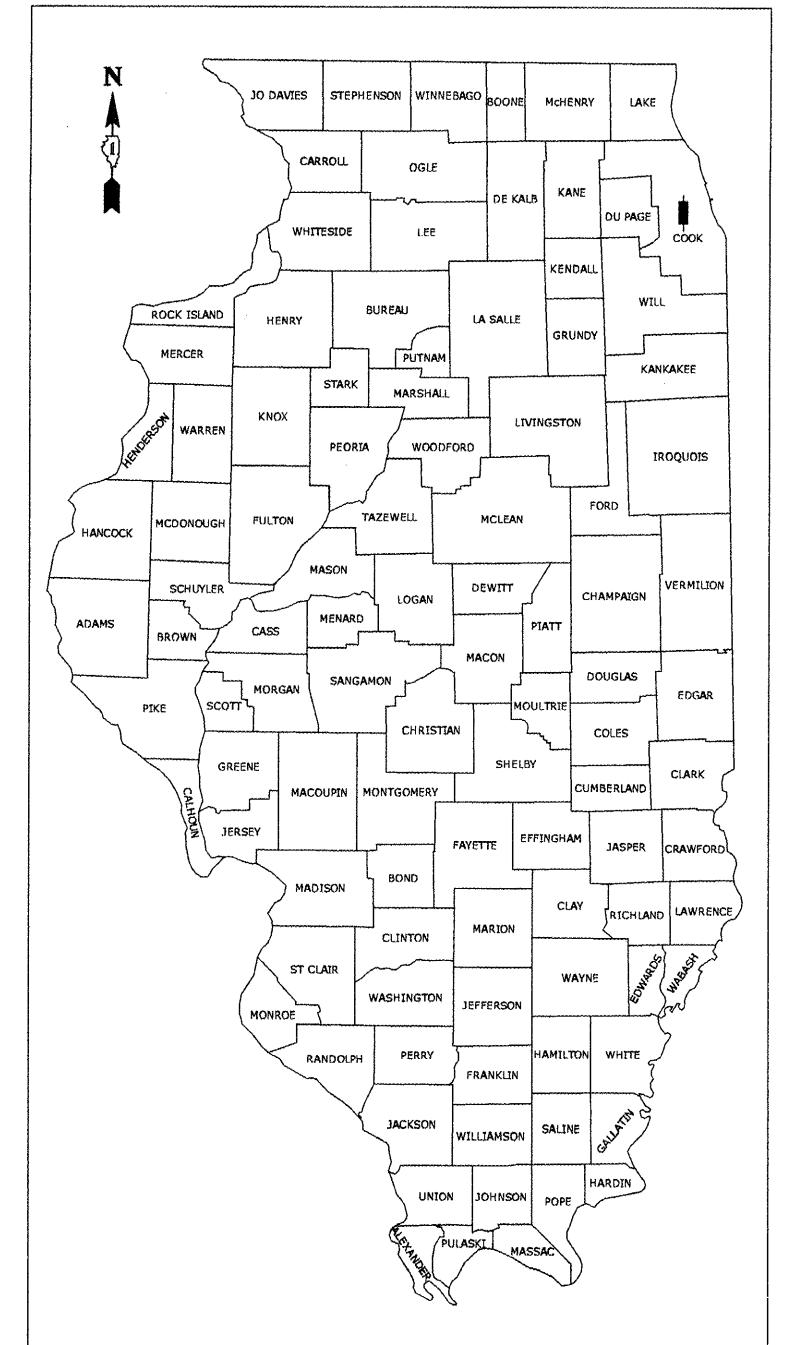


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
RESURFACING (LAPP)
FAU ROUTE 1005 (GARDNER ROAD)
CANTERBURY STREET TO ROOSEVELT ROAD
SECTION NO. 09-00075-00-RS
PROJECT M-9003(578)
VILLAGE OF WESTCHESTER
COOK COUNTY
JOB NO. C-91-353-10

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 1005	09-00075-00-RS	COOK	11	1
ILLINOIS PROJECT				
CONTRACT NO. 63458				



LOCATION OF SECTION INDICATED THUS:

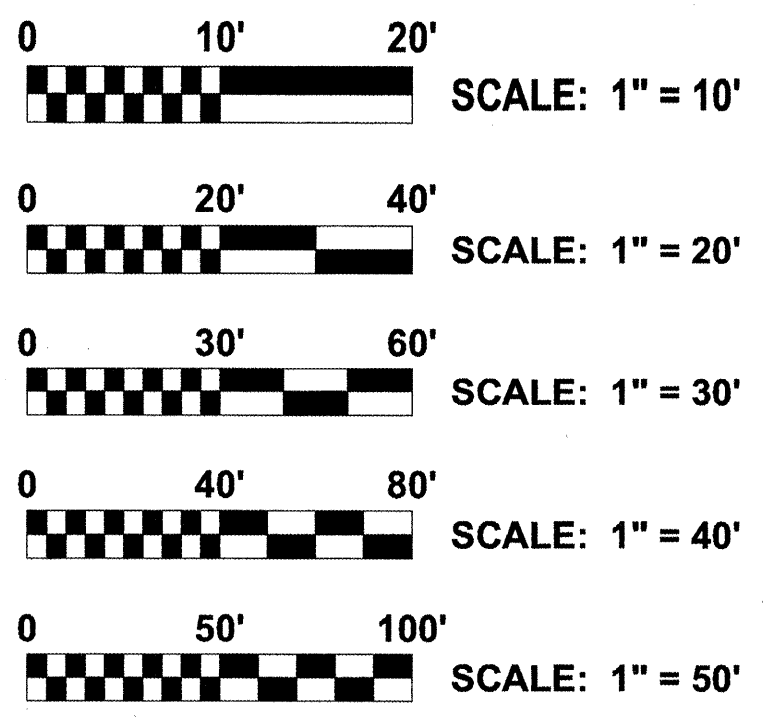
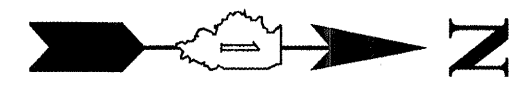
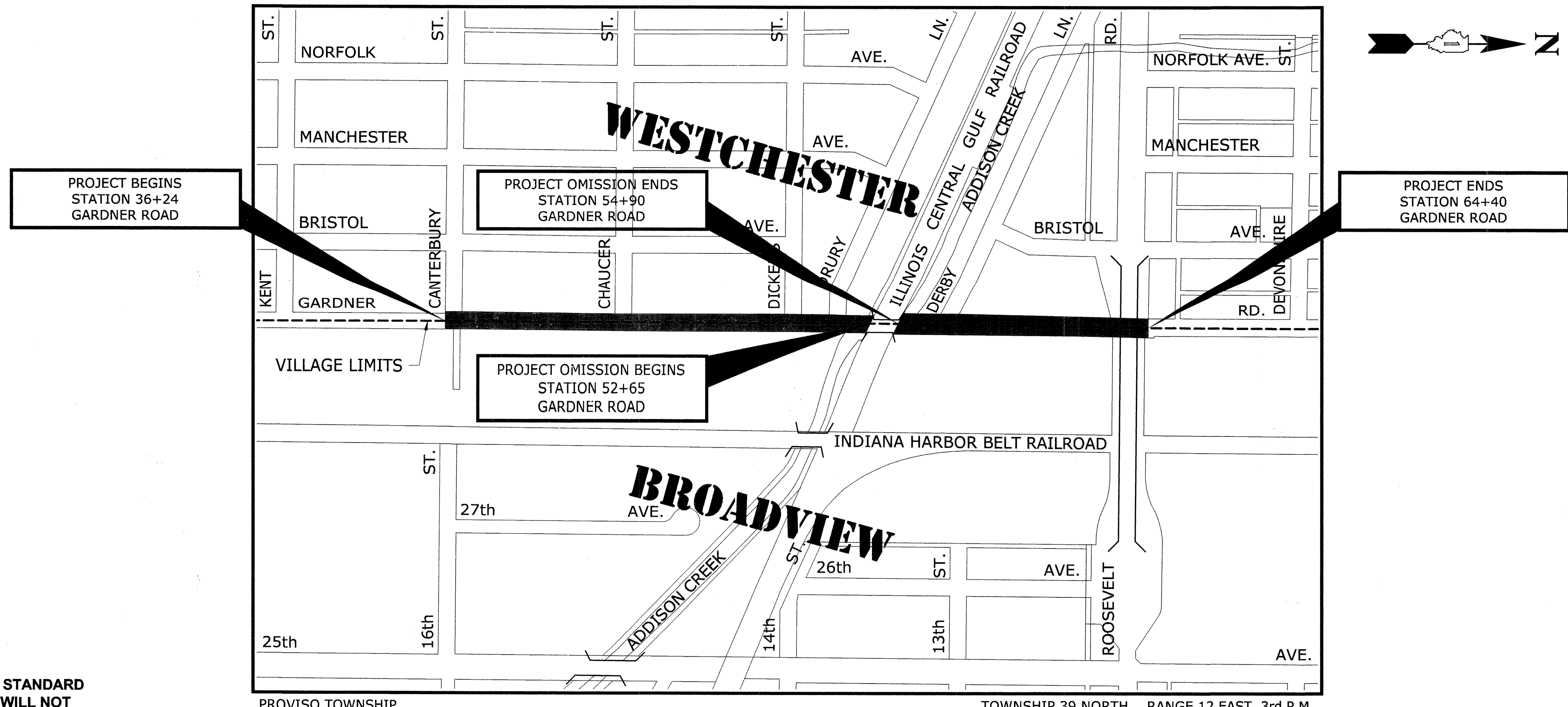
TRAFFIC DATA
 ADT (2009) = 7,000
 POSTED SPEED LIMIT = 25 MPH
 DESIGN SPEED LIMIT = 30 MPH

DESIGN DESIGNATION
 COLLECTOR

PROJECT LOCATED IN THE VILLAGE OF WESTCHESTER AND VILLAGE OF BROADVIEW



Know what's below.
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FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

CONTRACT NO. 63458

FIELD ENGINEER: MARILYN SOLOMON, (847) 705-4407

Drawing file: W:\Projects\63458\245 - Gardner Rd Resurf (LAPP)\Cover-GardnerRd.dwg Mar. 29, 2010 - 9:03am

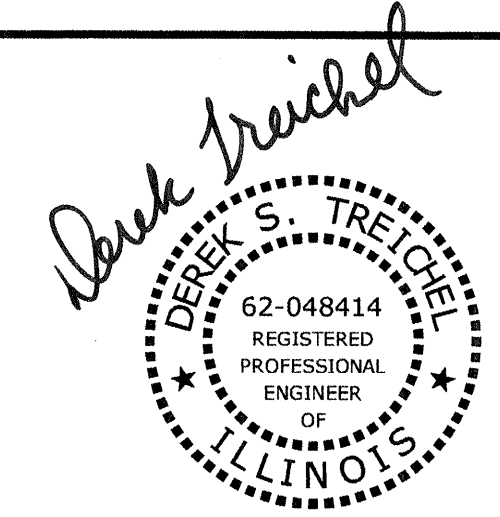
STATE OF ILLINOIS ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED March 31, 2010
Sam D. Rubin
 VILLAGE OF WESTCHESTER, PRESIDENT

PASSED APRIL 07, 2010
Chet A. Chesnut
 DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASED FOR BID BASED ON LIMITED REVIEW APRIL 8, 2010
Diana M. O'Keefe
 DEPUTY DIRECTOR OF HIGHWAYS,
 REGION 1 ENGINEER

PRINTED BY AUTHORITY
OF THE STATE OF ILLINOIS



DATE SIGNED: 3/31/2010
 LICENSE EXPIRES: 11/30/11

EDWIN HANCOCK ENGINEERING COMPANY
 9933 ROOSEVELT ROAD PHONE : (708) 865-0300
 WESTCHESTER, ILLINOIS 60154

- AREA OF IMPROVEMENT
 NOT TO SCALE
 GROSS LENGTH OF IMPROVEMENT = 2,816 FT = 0.53 MI.
 NET LENGTH OF IMPROVEMENT = 2,591 FT = 0.49 MI.

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET, LOCATION MAP
2	INDEX OF SHEETS, I.D.O.T. STANDARD DRAWINGS, LEGEND OF SYMBOLS, AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	EXISTING AND PROPOSED TYPICAL CROSS SECTIONS
5-6	GARDNER ROAD PAVING PLAN
7	DETAILS
8	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD 08)
9	BUTT JOINT AND HMA TAPER DETAILS (BD 32)
10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC 10)
11	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC 13)

I.D.O.T. STANDARD DRAWINGS

STANDARD NO.	TITLE OR DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C&D PATCHES
604001-03	FRAMES AND LIDS, TYPE 1
701501-05	URBAN LANE CLOSURE, 2-LANE, 2-WAY, UNDIVIDED
701801-04	LANE CLOSURE, MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS

LEGEND OF SYMBOLS

EXISTING	PROPOSED	DESCRIPTION
		MANHOLE
		INLET
		CATCH BASIN (RESTRICTED DEPTH)
		FIRE HYDRANT
		WATER MAIN VALVE VAULT
		POWER POLE
		STREET LIGHT
		RIM ELEVATION INVERT ELEVATION
	A	STRUCTURE TO BE ADJUSTED
	A*	STRUCTURE TO BE ADJUSTED (SPECIAL)
	1C	TYPE 1 FRAME & CLOSED LID
	1P	TYPE 1 FRAME & OPEN LID
	RC	STRUCTURE TO BE RECONSTRUCTED

STANDARDS

REFERENCES TO STANDARDS IN THE PLANS AND SPECIAL PROVISIONS SHALL BE THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION AS SHOWN ON THE INDEX OF SHEETS. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED JANUARY 1, 2007, THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2010, THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY 1996 FIFTH EDITION, THE DETAILS IN THE PLANS, AND THE SPECIAL PROVISIONS.

UNDERGROUND UTILITIES

BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 811 FOR FIELD LOCATIONS OF BURIED ELECTRICAL, TELEPHONE AND GAS FACILITIES (48 HOURS NOTIFICATION IS REQUIRED).

THE LOCATIONS OF THE UNDERGROUND UTILITIES, IF SHOWN ON THE PLANS, HAVE BEEN OBTAINED BY FIELD SURVEYS AND SEARCHES OF AVAILABLE RECORDS. IT IS BELIEVED THAT DATA IS ESSENTIALLY CORRECT, BUT THE VILLAGE OF WESTCHESTER, THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND/OR OTHER OFFICES AND AGENCIES ASSOCIATED WITH THE DEVELOPMENT OF THESE PLANS DO NOT GUARANTEE THEIR ACCURACY OR COMPLETENESS. THE CONTRACTOR WILL BE REQUIRED TO VERIFY THE LOCATION OF EACH FACILITY WITH THE UTILITY COMPANY, AND SHALL TAKE DUE CARE IN ALL PHASES OF THE CONSTRUCTION TO PROTECT SUCH FACILITIES THAT MAY BE AFFECTED BY THE WORK. ALL DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE IN ACCORDANCE WITH ARTICLES 107.20 AND 107.31.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY OWNERS AND THE ENGINEER.

FRAMES AND LIDS

THE LOCATION AND TYPE OF FRAMES AND LIDS REQUIRED FOR ALL MANHOLES AND VALVE VAULTS LISTED IN THE SUMMARY OF QUANTITIES MAY BE FOUND ON THE PLANS. WHERE LIDS ARE CALLED FOR ON THE PLANS, THEY SHALL BE IN ACCORDANCE WITH ARTICLE 604.04 OF THE STANDARD SPECIFICATIONS AND THE TERM LID IS USED IN LIEU OF GRATE. ALL LIDS ON SANITARY MANHOLES, COMBINED SEWER MANHOLES, AND VALVE VAULTS SHALL BE OF THE SELF SEALING TYPE.

THE FRAMES AND LIDS OF EXISTING CATCH BASINS, INLETS, MANHOLES, AND VALVE VAULTS THAT ARE TO BE ABANDONED ARE TO REMAIN THE PROPERTY OF THE VILLAGE OF BROADVIEW OR THE VILLAGE OF WESTCHESTER DEPENDING ON THE LOCATION OF THE FRAME AND LID. THE CONTRACTOR IS TO DELIVER FRAMES AND LIDS TO THE VILLAGE OF BROADVIEW PUBLIC WORKS YARD LOCATED AT 2734 SOUTH 9th AVENUE OR TO THE VILLAGE OF WESTCHESTER PUBLIC WORKS YARD LOCATED AT 10300 ROOSEVELT ROAD. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF EACH BID ITEM.

MAINTENANCE OF SEWER FLOWS

THE CONTRACTOR SHALL CONDUCT HIS/HER OPERATIONS TO MAINTAIN FLOW AT ALL TIMES THROUGH EXISTING STORM AND SANITARY SEWER SYSTEMS. HE/SHE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER COLLECTED IN A SAFE MANNER WITHOUT DAMAGE OF ANY KIND TO ADJACENT PROPERTIES. THE ENDS OF EXISTING DRAINAGE LINES THAT ARE NOT TO BE INCORPORATED INTO THE PROJECT ARE TO BE SEALED AS SPECIFIED IN THE SPECIAL PROVISIONS. EXISTING STRUCTURES ARE TO BE INSPECTED BEFORE CONSTRUCTION STARTS - ACCUMULATION OF MATERIAL IN THE STRUCTURE DUE TO CONSTRUCTION OPERATIONS SHALL BE REMOVED BY THE CONTRACTOR AT HIS/HER EXPENSE.

NOTIFICATION OF RESIDENTS

THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING WRITTEN NOTICE TO ALL RESIDENCES AND/OR PLACES OF BUSINESS AT LEAST ONE (1) WORKING DAY PRIOR TO PERFORMING ALL CONSTRUCTION ACTIVITY THAT WILL ELIMINATE ACCESS TO THEIR PROPERTY. THE WRITTEN NOTICE SHALL BE APPROVED BY THE ENGINEER AND A COPY PROVIDED TO THE VILLAGE PRIOR TO THE BEGINNING OF CONSTRUCTION.

NOTIFICATION OF CONSTRUCTION

THE CONTRACTOR SHALL NOTIFY THE ENGINEER, VILLAGE OF WESTCHESTER, AND VILLAGE OF BROADVIEW IN WRITING AT LEAST 48 HOURS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.

PUBLIC UTILITIES - THE CONTRACTOR SHALL NOTIFY ALL PUBLIC AND PRIVATE UTILITY COMPANIES HAVING FACILITIES WITHIN THE LIMITS OF WORK OF THE CONTRACT 48 HOURS PRIOR TO COMMENCING WORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PRESERVATION OF EXISTING UTILITY INSTALLATIONS, DAMAGE DONE BY HIM/HER TO UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS, AND THE COSTS OF PROVIDING PRECAUTIONARY SUPPORTS, BRACES, ETC. TO AVOID DAMAGE TO UTILITY INSTALLATIONS IN ACCORDANCE WITH ARTICLES 107.31 AND 107.20. THE CONTRACTOR SHALL COORDINATE WITH THE UTILITY OWNERS FOR THE RELOCATION OF THEIR FACILITIES.

GENERAL NOTES

MAINTENANCE OF EXISTING DRAINAGE STRUCTURES

WHEN LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF GUTTERS AND DRAINAGE STRUCTURE OBSTRUCTING THE NATURAL FLOW OF WATER, THE LOOSE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE FACILITIES SHALL BE CLEAN AND FREE OF ALL OBSTRUCTIONS CAUSED BY CONSTRUCTION OPERATIONS.

SAW CUTTING

THE CONTRACTOR SHALL SAW CUT ASPHALT PAVEMENT AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING PAVEMENT TO BE REMOVED BY APPROVED MEANS OR AN APPROVED CONCRETE SAW TO A DEPTH AS DIRECTED BY THE ENGINEER. SUITABLE GUIDELINES OR DEVICES SHALL BE USED TO ASSURE CUTTING A NEAT, STRAIGHT LINE AS SHOWN ON THE PLANS. CARE SHALL BE TAKEN BY THE CONTRACTOR NOT TO DAMAGE THE REMAINING PAVEMENT DIRECTLY ADJACENT TO THE PAVEMENT TO BE REMOVED. DAMAGE TO THE EXISTING PAVEMENT RESULTING FROM PAVEMENT REMOVAL OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THE COST OF SAW CUTTING DESCRIBED ABOVE SHALL BE INCLUDED IN THE COST OF ITEM BEING REMOVED.

PRIME COAT

PRIME COAT MUST BE INSTALLED NO EARLIER THAN TWENTY-FOUR (24) HOURS PRIOR TO PLACEMENT OF HOT-MIX ASPHALT.

FIELD OFFICE

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR VILLAGE PROPERTY ROW WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.

BARRICADES

THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED, ONE (1) WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL.

BUTT JOINTS

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER" DETAIL SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

PAVEMENT PATCHING

LOCATIONS OF PATCHES WILL BE DETERMINED IN FIELD BY ENGINEER AFTER MILLING OPERATIONS.

MAINTENANCE OF TRAFFIC

GARDNER ROAD WILL REMAIN OPEN TO THRU TRAFFIC DURING ALL OPERATIONS. FLAGGERS SHALL BE USED FOR THE INSTALLATION OF THE POLYMERIZED LEVELING BINDER, THE HOT-MIX ASPHALT SURFACE, AND THERMOPLASTIC STRIPING.

OPEN EXCAVATIONS

THE CONTRACTOR SHALL NOT BE ALLOWED TO LEAVE EXCAVATION NECESSARY FOR PAVEMENT PATCHES OR STRUCTURE ADJUSTMENTS OPEN OVERNIGHT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETELY BACKFILLING OR INSTALLING A PLATE OVER ALL EXCAVATIONS AT THE END OF EACH DAY IN ACCORDANCE WITH ARTICLE 107.09.

CONCRETE BREAKERS

WHEN REMOVING PAVEMENT AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS SUCH AS DROP HAMMERS, WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES, WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.

FRAMES AND LIDS, TYPE 1

ALL COMBINED SEWER CLOSED LIDS SHALL BE SELF-SEALING WITH RECESSED PICK HOLES. ALL FRAMES WITH CLOSED LIDS TO BE FURNISHED AS PART OF THIS CONTRACT FOR CONSTRUCTION, ADJUSTMENT, OR RECONSTRUCTION OF ANY MANHOLE OR VALVE VAULT SHALL HAVE CAST INTO THE LID ONE OF THE FOLLOWING WORDS:

STRUCTURE TYPE	NEENAH FOUNDRY*		
	FRAME AND LID	SELF SEALING	WORD CAST INTO LID
SANITARY MANHOLE	R-1713-B	YES	SANITARY
COMBINATION MANHOLE	R-1713-B	YES	SANITARY
STORM MANHOLE	R-1713-B	NO	STORM
VALVE VAULT	R-1713-B	YES	WATER
CATCH BASIN	R-2504-D	NO	-----
INLET	R-2504-D	NO	-----

* NOTE: OR ENGINEER APPROVED EQUAL.

PRECAST CONCRETE SECTIONS FOR STRUCTURES

ALL NEW STRUCTURES AND STRUCTURES TO BE RECONSTRUCTED SHALL BE BUILT WITH PRECAST REINFORCED CONCRETE SECTIONS FABRICATED IN ACCORDANCE WITH A.S.T.M. DESIGNATION C-478.

CONNECTION OF DISSIMILAR PIPE MATERIALS

FLEXIBLE CONNECTORS SHALL BE USED WHERE NEW SEWER PIPES ARE TO BE CONNECTED TO EXISTING SEWER PIPES. THE FLEXIBLE CONNECTORS SHALL CONSIST OF A SYNTHETIC RUBBER SLEEVE, WHICH WILL RESIST DETERIORATION CAUSED BY WASTES FOUND IN SEWERS AND TWO STAINLESS STEEL STRAPS. THE CONNECTORS SHALL BE "MISSION" COUPLINGS, "FERNCO" COUPLINGS, OR ENGINEER APPROVED EQUAL. ALL LABOR AND MATERIAL REQUIRED TO CONNECT NEW SEWER PIPE TO EXISTING SEWER PIPE SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

FORMS FOR CONCRETE SIDEWALKS, DRIVEWAYS AND GUTTER FLAGS

A 2" x 6" BOARD WILL BE USED AS A FORM FOR ALL SIDEWALKS TO BE INSTALLED FIVE INCHES (5") IN THICKNESS. A 2" x 8" BOARD WILL BE USED AS A FORM FOR ALL SIDEWALKS AND DRIVEWAYS TO BE INSTALLED SEVEN INCHES (7") IN THICKNESS. ALL FORMS MUST BE OF A MINIMUM HEIGHT OF THE PROPOSED THICKNESS OF THE RESPECTIVE CONCRETE ITEMS TO BE INSTALLED.

AGGREGATE BEDDING FOR CONCRETE WORK

NEW SIDEWALK AND DRIVEWAY PAVEMENT SHALL BE PLACED ON A MINIMUM OF TWO INCHES (2") OF COMPACTED CA-6 STONE BEDDING. ADDITIONAL AGGREGATE REQUIRED TO ADJUST THE EXISTING ELEVATION OF THE SUBGRADE TO THE PROPOSED ELEVATION WILL BE INCLUDED AS PART OF THIS WORK.

SIDEWALK AND DRIVEWAY PAVEMENT REMOVAL

THIS WORK SHALL BE DONE IN ACCORDANCE WITH SECTION 440 OF THE STANDARD SPECIFICATIONS WITH THE EXCEPTION THAT IT SHALL INCLUDE THE REMOVAL OF CONCRETE, BRICK, BLOCK, FLAGSTONE AND HOT-MIX ASPHALT SIDEWALKS AND DRIVEWAYS.

CURB AND GUTTER TRANSITIONS

TRANSITIONS FROM THE PROPOSED CURB AND GUTTERS TO THE DEPRESSED CURB AND GUTTERS AND PROPOSED CURB AND GUTTERS TO EXISTING CURB AND GUTTERS SHALL BE CONSTRUCTED AS DIRECTED BY THE ENGINEER.

LEVELING BINDER (MACHINE METHOD), N50

THIS ITEM SHALL BE APPLIED TO AREAS WHERE THE CROWN OF THE PAVEMENT NEEDS TO BE BUILT UP TO PROVIDE THE DESIRED CROSS SECTION. THE LEVELING BINDER SHALL BE PLACED PRIOR TO THE PLACEMENT OF THE POLYMERIZED LEVELING BINDER. THIS ITEM SHALL ALSO BE APPLIED TO CERTAIN BUTT-JOINT AREAS WHERE THE EXISTING BASE COURSE IS IRREGULAR AND THE ENGINEER INDICATES THAT IT IS DESIRABLE TO FILL IN DEPRESSIONS PRIOR TO PLACING THE BITUMINOUS CONCRETE SURFACE COURSE. THE LOCATION AND THICKNESS OF THE LEVELING BINDER SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER. THE LEVELING BINDER SHALL BE PLACED IN ACCORDANCE WITH SECTION 406 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

CONNECTING EXISTING PIPES TO STRUCTURES

ANY PIPE, UP TO FOUR FEET (4') IN LENGTH PER EACH PIPE AND THE NECESSARY TRENCH BACKFILL, USED TO CONNECT EXISTING PIPES TO THE STRUCTURE TO BE RECONSTRUCTED OR TO THE NEW STRUCTURE SHALL BE INCLUDED IN THE COST OF THE STRUCTURE TO BE RECONSTRUCTED OR THE NEW STRUCTURE.

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9933 Riverside Road
Woodstock, IL 61581-2700
Phone: 708.905.4300
Fax: 708.905.1310

USER NAME -	DESIGNED - TG	REVISED -
	DRAWN - TG	REVISED -
PLOT SCALE -	CHECKED - DT	REVISED -
PLOT DATE -	DATE - 03/26/10	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, I.D.O.T. STANDARD DRAWINGS, LEGEND OF SYMBOLS, AND GENERAL NOTES

SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(578)
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1005	09-00075-00-RS	COOK	11	2
CONTRACT NO. 63458				

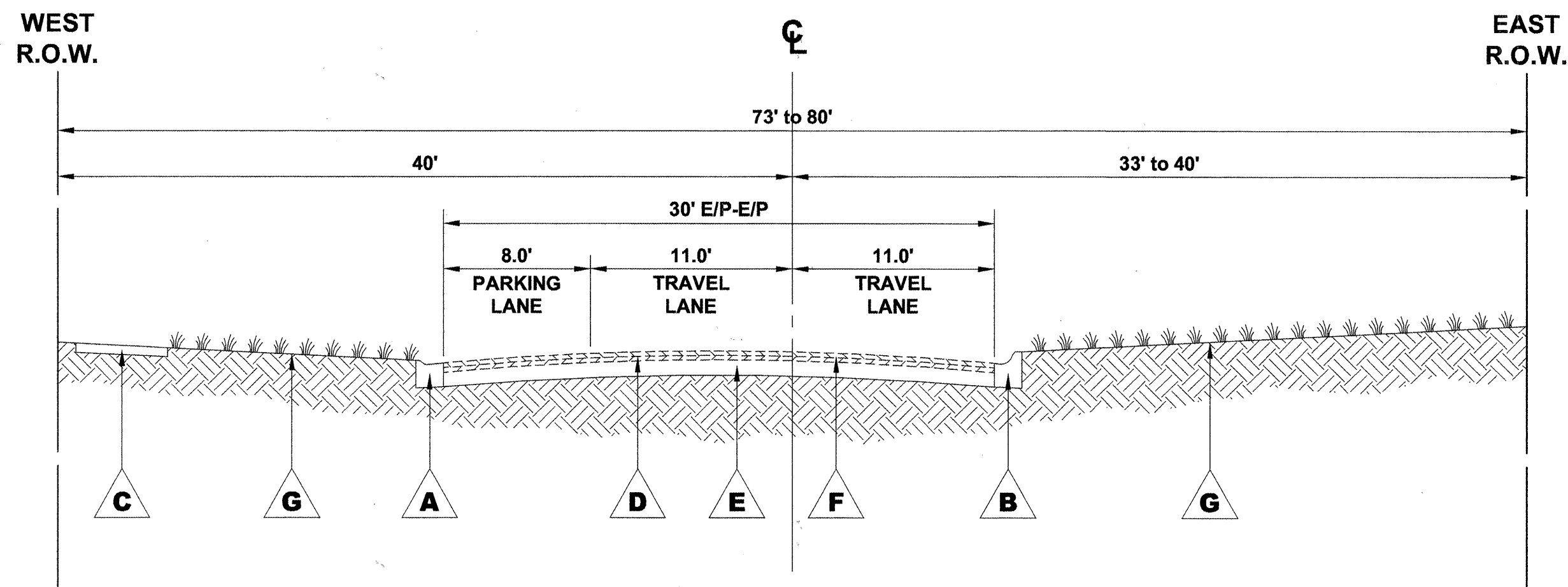
SUMMARY OF QUANTITIES

CODE	PAY ITEM	UNIT	1000 TOTAL QUANTITY
21101610	TOPSOIL FURNISH AND PLACE, 3"	SQ YD	350
25200100	SODDING	SQ YD	350
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	1,000
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	10
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	50
⊙ 40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	435
40600895	CONSTRUCTING TEST STRIP	EACH	1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	175
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1,150
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	5
42101300	PROTECTIVE COAT	SQ YD	200
42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	20
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	180
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2,500
42400800	DETECTABLE WARNINGS	SQ FT	40
⊙ 44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	10,000
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	200
44000600	SIDEWALK REMOVAL	SQ FT	2,500
⊙ 44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	450
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	20
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	50
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	50
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	300
⊙ 60213800	RESTRICTED DEPTH CATCH BASINS, 4' DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	10
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	4
⊙ 60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	20
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	4
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	1
67100100	MOBILIZATION	L SUM	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	500
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3,000
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	400
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	100
⊙ Z0004910	HOT-MIX ASPHALT FOR PATCHING POTHoles (HOT MIX)	TON	5

* DENOTES SPECIALTY ITEM
 ⊙ DENOTES THAT A SPECIAL PROVISION HAS BEEN PROVIDED

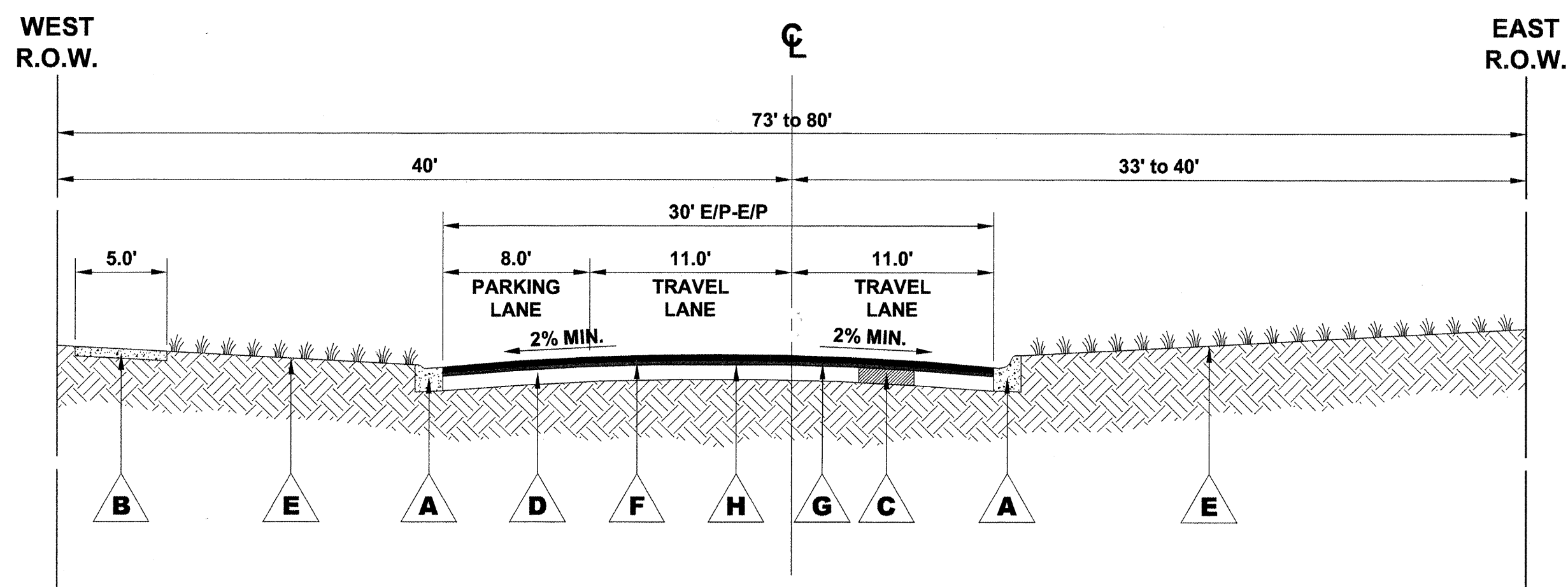
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HANCOCK ENGINEERING ♦ Civil Engineers ♦ Municipal Consultants ♦ Established 1911	USER NAME --	DESIGNED -- TG	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE --	CHECKED -- DT	REVISED --			1005	09-00075-00-RS	COOK	11	3
PLOT DATE --	DATE -- 03/26/10	REVISED --		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(578) CONTRACT NO. 63458				



SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
A	EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE M-3.12	E	EXISTING CONCRETE BASE COURSE
B	EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12	F	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (0"-4")
C	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK	G	EXISTING LANDSCAPED PARKWAY
D	EXISTING HOT-MIX ASPHALT BINDER & SURFACE COURSES (APPROXIMATELY 4")		

EXISTING TYPICAL SECTION



SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
A	PROPOSED INTERMITTENT COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	E	EXISTING LANDSCAPED PARKWAY
B	PROPOSED INTERMITTENT PORTLAND CEMENT CONCRETE SIDEWALK, 5"	F	PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" MINIMUM
C	PROPOSED CLASS D PATCH, 10"	G	PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
D	EXISTING CONCRETE BASE COURSE (10"-12")	H	PROPOSED LEVELING BINDER (MACHINE METHOD), N50 AREAS WHERE CROWN OF THE PAVEMENT NEEDS TO BE BUILT UP

PROPOSED TYPICAL SECTION

HOT-MIX ASPHALT (HMA) MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL - 9.5 mm)	4% @ 50 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL - 4.75, N50	4% @ 50 GYR.
LEVELING BINDER (MACHINE METHOD), N50	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES (HMA BINDER IL - 19mm)	4% @ 70 GYR.
HOT-MIX ASPHALT FOR PATCHING POTHoles (HOT MIX), MIX "D", N50	4% @ 50 GYR.
INCIDENTAL HOT-MIX ASPHALT SURFACING, MIX "C", N50	4% @ 50 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

THE CONTRACTOR SHALL MILL BEFORE PATCHING.

Mar 29, 2010 - 3:09pm

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Westchester, Illinois 60154-2780
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USER NAME --	DESIGNED -- TG	REVISED --
	DRAWN -- TG	REVISED --
PLOT SCALE --	CHECKED -- DT	REVISED --
PLOT DATE --	DATE -- 03/26/10	REVISED --

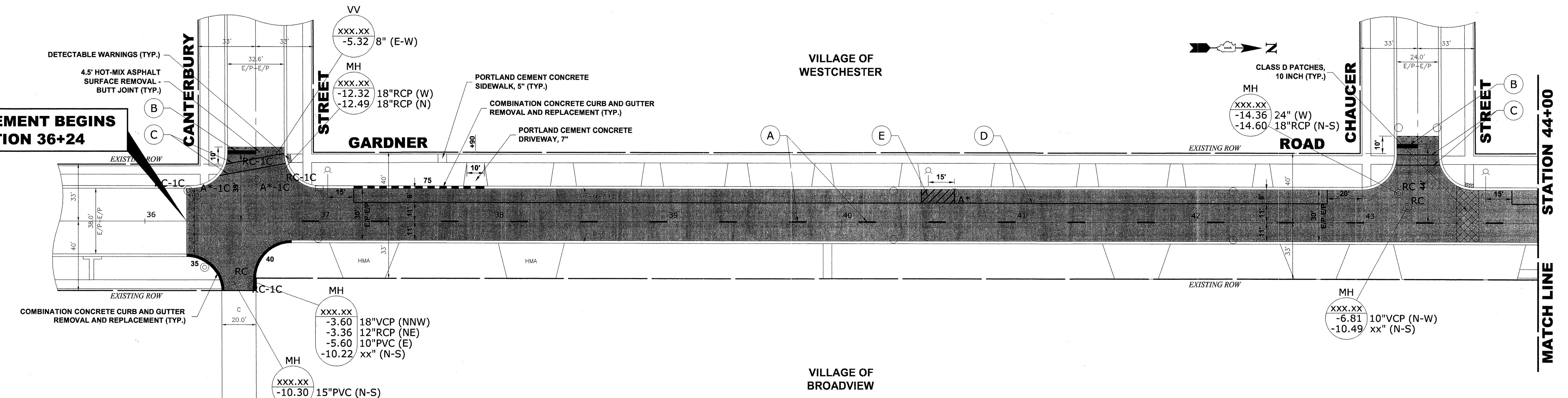
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED
TYPICAL CROSS SECTIONS**

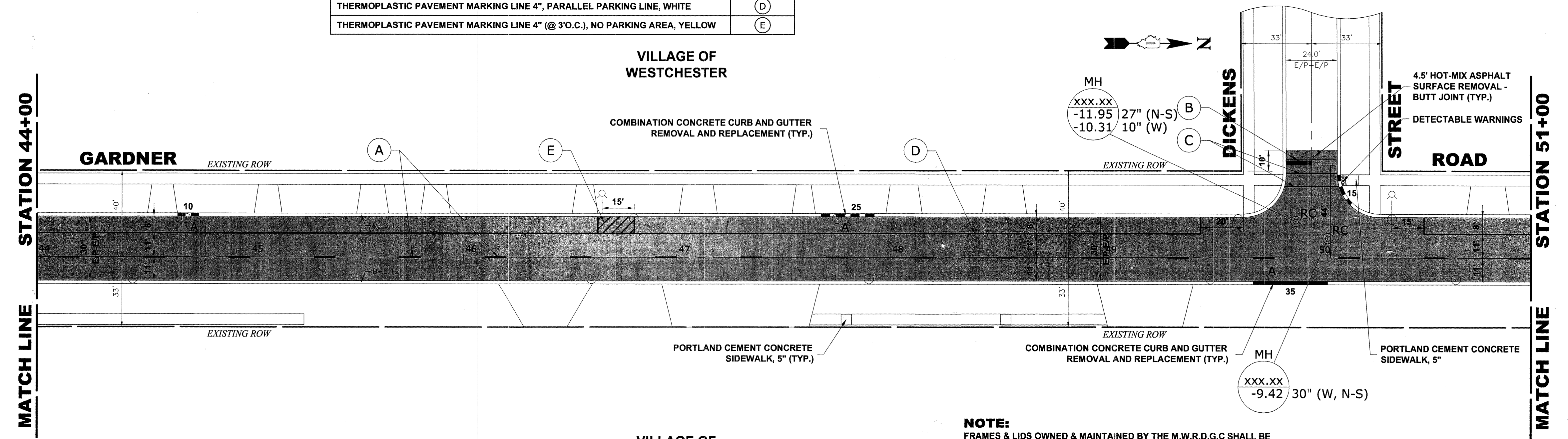
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1005	09-00075-00-RS	COOK	11	4
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 63458	
FED. AID PROJECT			M-9003(578)	

**IMPROVEMENT BEGINS
STATION 36+24**



PAVEMENT MARKING LEGEND	
ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING LINE 4", CENTER LINE, YELLOW	(A)
THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP BAR, WHITE	(B)
THERMOPLASTIC PAVEMENT MARKING LINE 6", CROSS WALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE 4", PARALLEL PARKING LINE, WHITE	(D)
THERMOPLASTIC PAVEMENT MARKING LINE 4" (@ 3'O.C.), NO PARKING AREA, YELLOW	(E)



NOTE:
FRAMES & LIDS OWNED & MAINTAINED BY THE M.W.R.D.G.C SHALL BE ADJUSTED (OR RECONSTRUCTED IF NECESSARY) BY THE M.W.R.D.G.C. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING THIS WORK WITH THE M.W.R.D.G.C. SO AS TO NOT INTERFERE WITH OR DELAY THE PAVING OPERATIONS.

Drawing file: W:\Projects\83809245 - Gardner Rd Resurf (LAPP)\Gardner Rd LAPP.dwg Mar 29, 2010 - 3:03pm

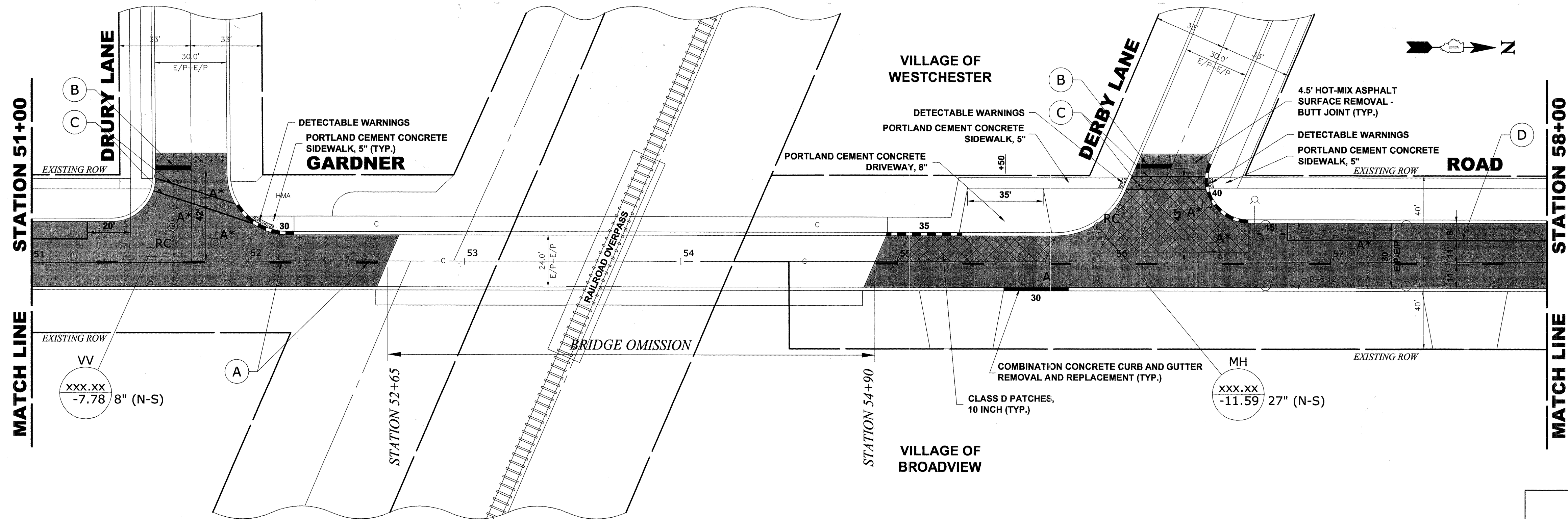


USER NAME --	DESIGNED -- TG	REVISED --
PLOT SCALE --	DRAWN -- TG	REVISED --
PLOT DATE --	CHECKED -- DT	REVISED --
	DATE -- 03/26/10	REVISED --

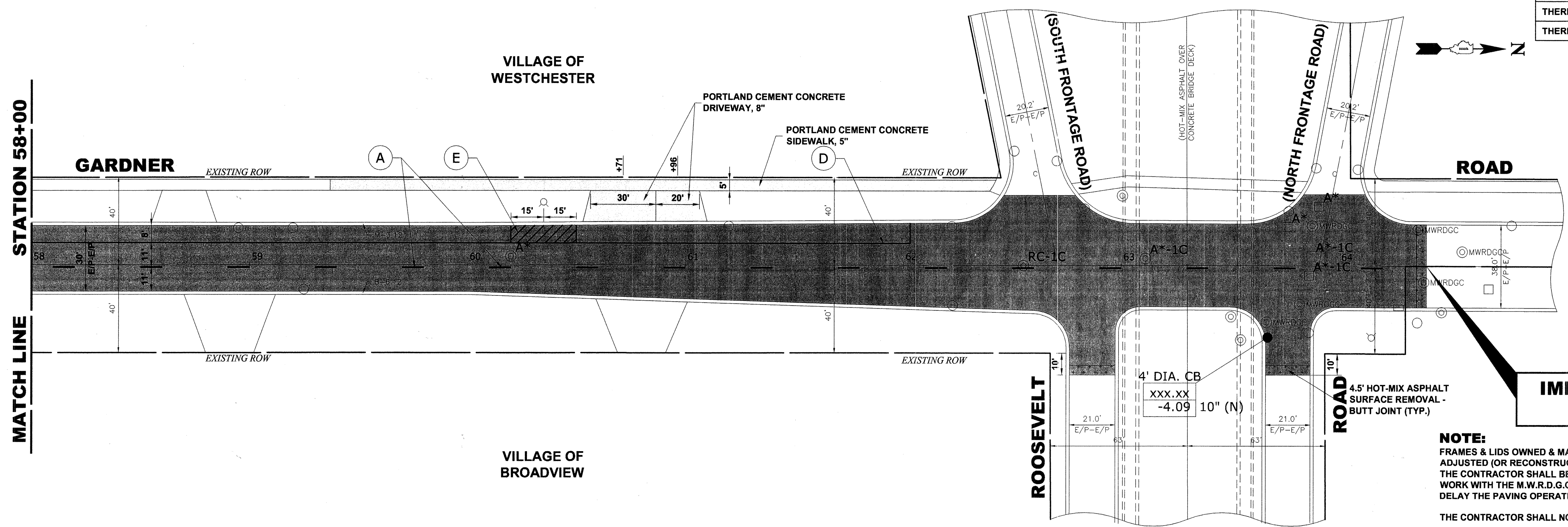
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GARDNER ROAD PAVING PLAN
SCALE: 1" = 30' SHEET NO. 1 OF 2 SHEETS STA. 36+24 TO STA. 51+00

P.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1005	09-00075-00-RS	COOK	11	5
CONTRACT NO. 63458			FED. ROAD DIST. NO. 1 ILLINOIS	
			FED. AID PROJECT M-9003(578)	



PAVEMENT MARKING LEGEND	
ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING LINE 4", CENTER LINE, YELLOW	(A)
THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP BAR, WHITE	(B)
THERMOPLASTIC PAVEMENT MARKING LINE 6", CROSS WALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE 4", PARALLEL PARKING LINE, WHITE	(D)
THERMOPLASTIC PAVEMENT MARKING LINE 4" (@ 3'O.C.), NO PARKING AREA, YELLOW	(E)



Drawing File: W:\Projects\83809245 - Gardner Rd Resurf (LAPP)\Gardner Rd LAPP.dwg
 Mar 29, 2010 - 3:04pm
 9933 Roosevelt Road
 Westchester, Illinois 60154-2780
 Phone: 708/665-0300
 Fax: 708/665-5122

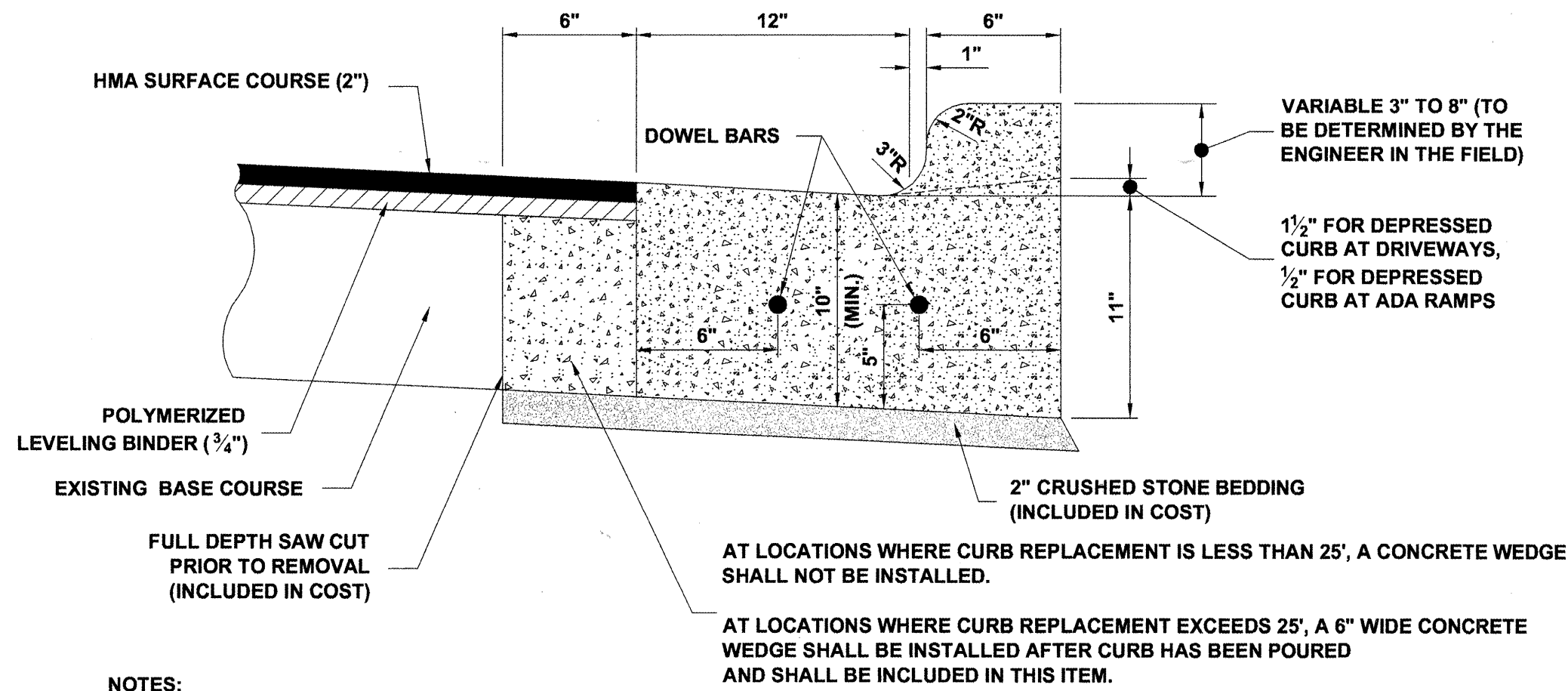
HANCOCK ENGINEERING
 Civil Engineers
 Municipal Consultants
 Established 1911

USER NAME	DESIGNED - TG	REVISED -
PLOT SCALE	DRAWN - TG	REVISED -
PLOT DATE	CHECKED - DT	REVISED -
	DATE - 03/26/10	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

GARDNER ROAD PAVING PLAN
 SCALE: 1" = 30'
 SHEET NO. 2 OF 2 SHEETS
 STA. 51+00 TO STA. 64+40

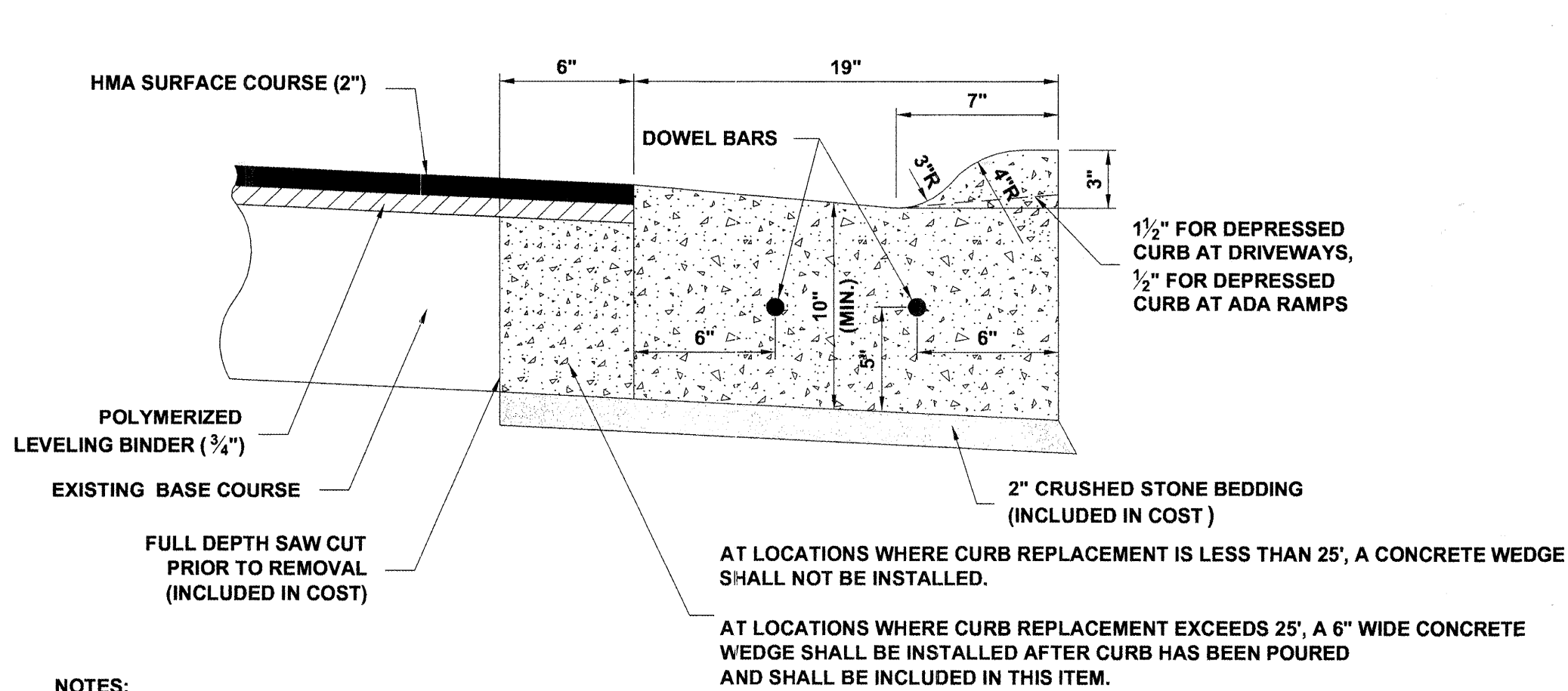
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1005	09-00075-00-RS	COOK	11	6
CONTRACT NO. 63458				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(578)				



NOTES:

1. PROVIDE 2 - 3/4" DIAMETER, 18" LONG EPOXY COATED SMOOTH BARS WITH PLASTIC EXPANSION CAPS AT EACH EXPANSION JOINT.
2. CONTRACTION JOINT - 2" DEEP CONTRACTION JOINTS SHALL BE SAWED AT EQUAL SPACES (NOT EXCEEDING 15 FEET) BETWEEN NORMAL EXPANSION JOINTS, IN THE UPPER 1/3 OF CURB & GUTTERS WITHIN 24 HOURS OF PLACEMENT.
3. A LEAN CONCRETE WEDGE IS TO BE POURED IN THE SPACE BETWEEN THE NEW CURB & THE EXISTING PAVEMENT AFTER THE CURB HAS REACHED ITS INITIAL SET.

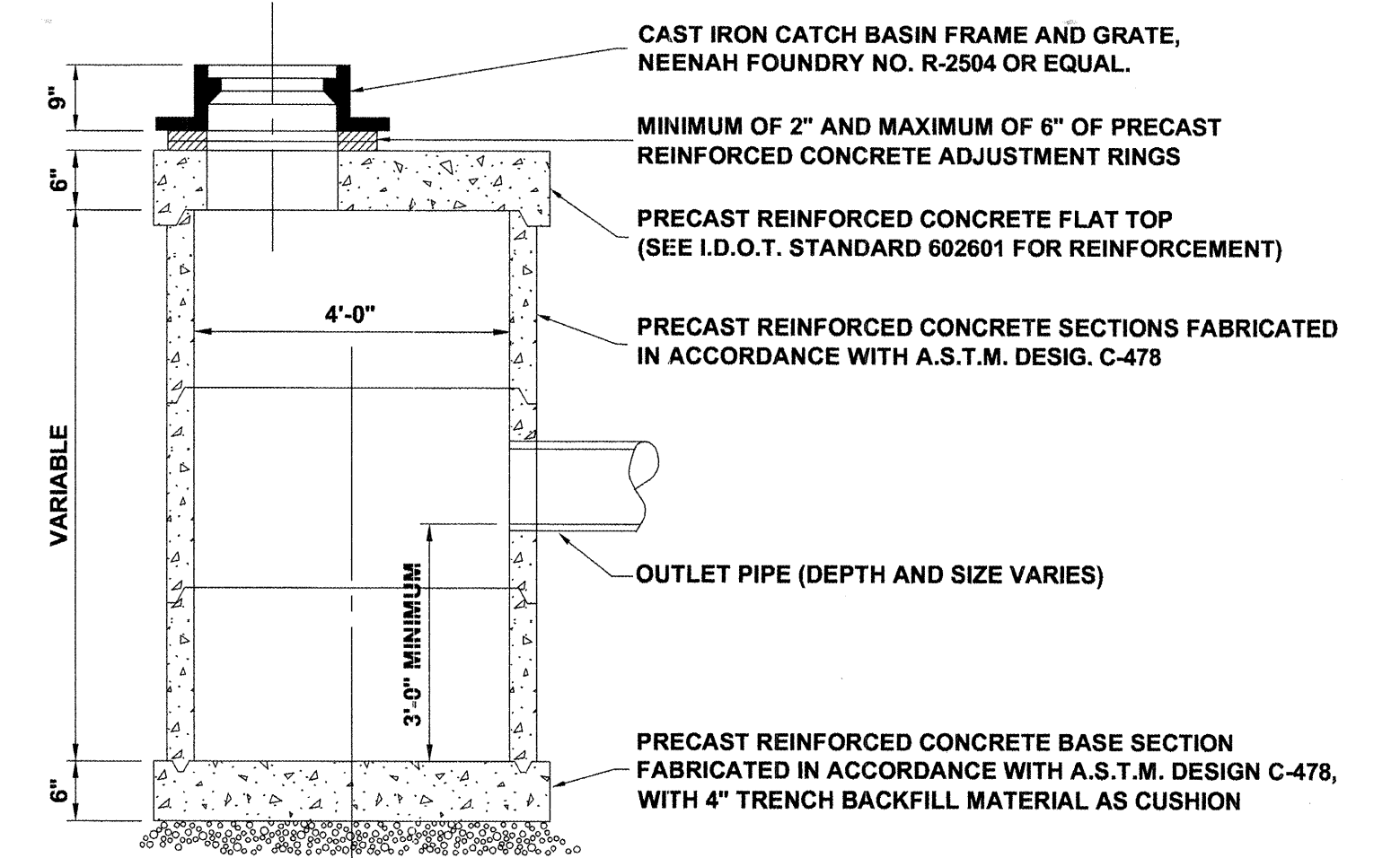
**COMBINATION CONCRETE CURB AND GUTTER
REMOVAL AND REPLACEMENT (EAST SIDE)**



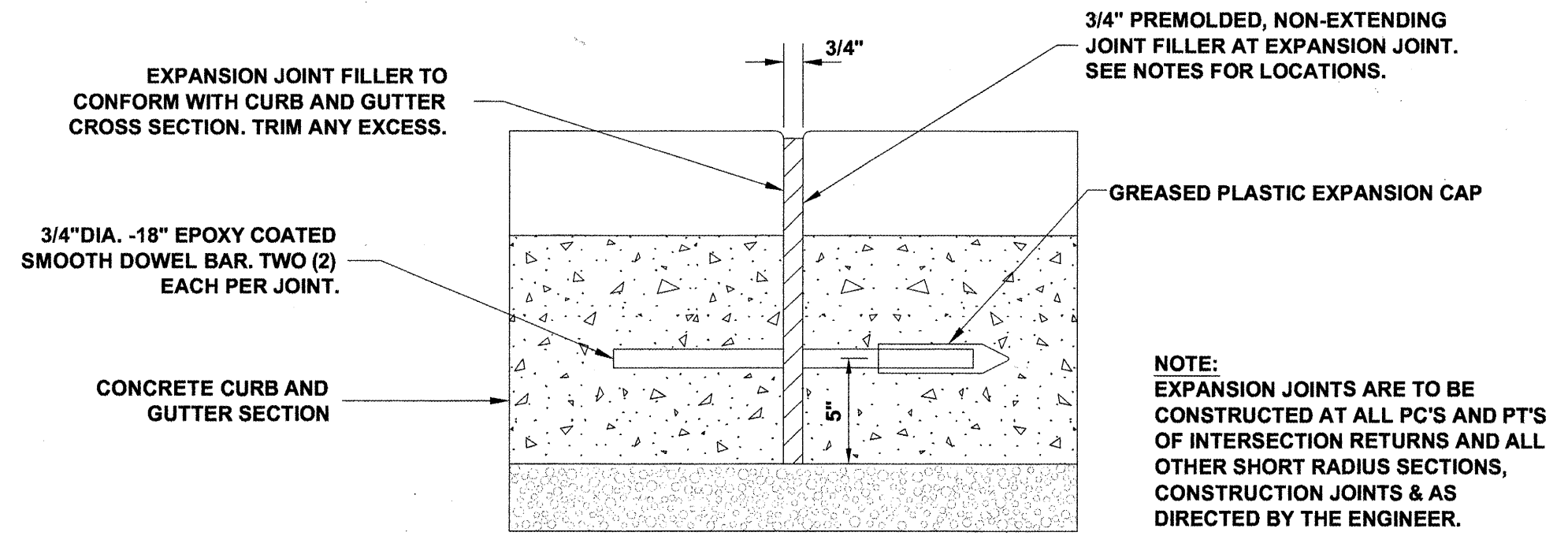
NOTES:

1. PROVIDE 2 - 3/4" DIAMETER, 18" LONG EPOXY COATED SMOOTH BARS WITH PLASTIC EXPANSION CAPS AT EACH EXPANSION JOINT.
2. CONTRACTION JOINT - 2" DEEP CONTRACTION JOINTS SHALL BE SAWED AT EQUAL SPACES (NOT EXCEEDING 15 FEET) BETWEEN NORMAL EXPANSION JOINTS, IN THE UPPER 1/3 OF CURB & GUTTERS WITHIN 24 HOURS OF PLACEMENT.
3. A LEAN CONCRETE WEDGE IS TO BE POURED IN THE SPACE BETWEEN THE NEW CURB & THE EXISTING PAVEMENT AFTER THE CURB HAS REACHED ITS INITIAL SET.

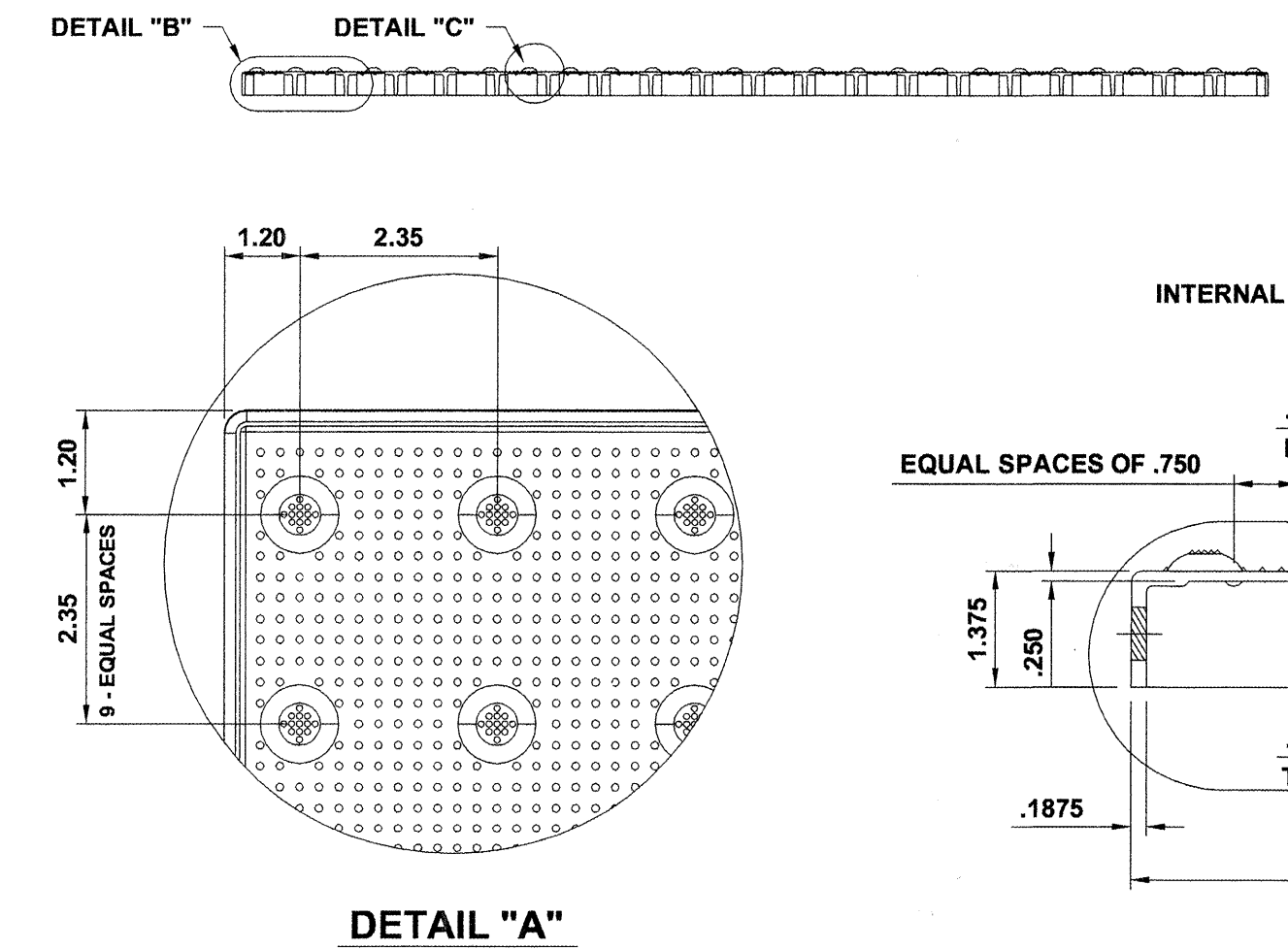
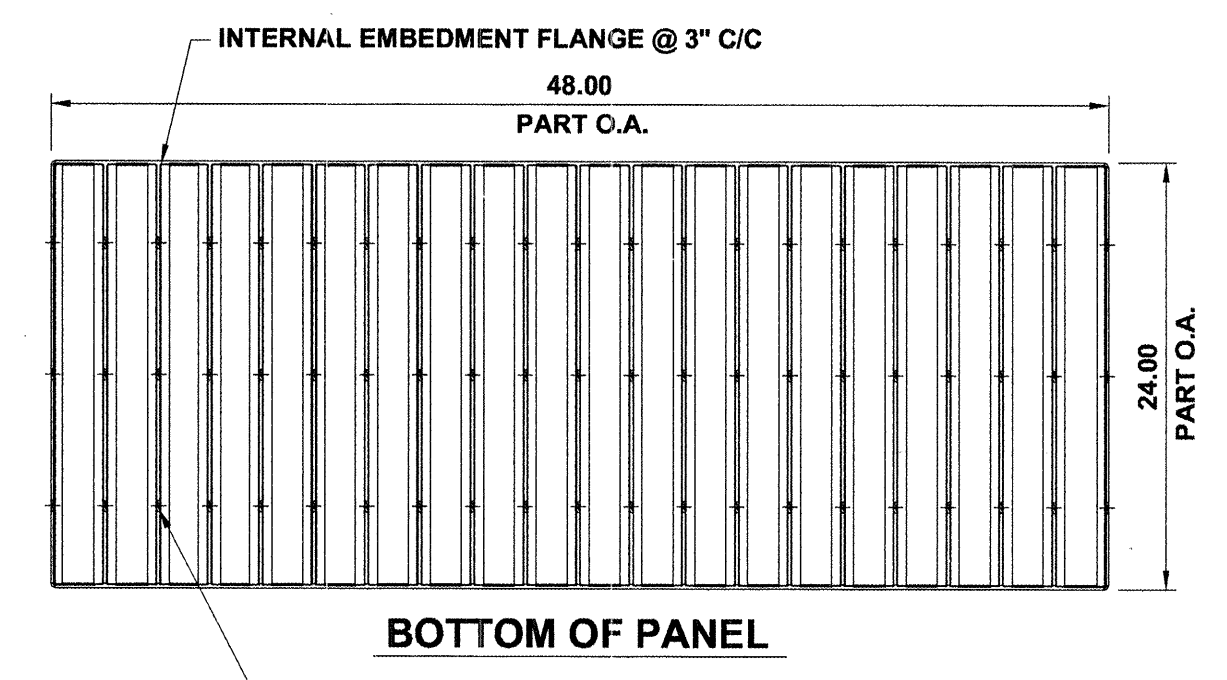
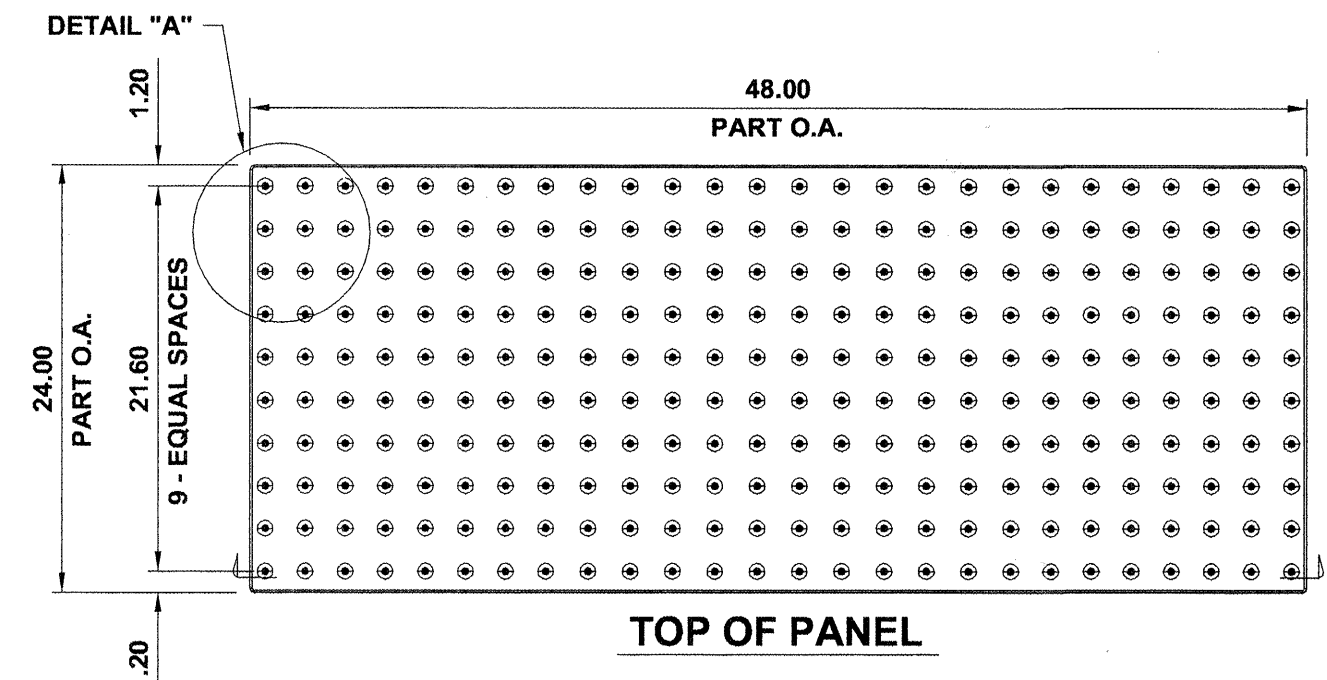
**COMBINATION CONCRETE CURB AND GUTTER
REMOVAL AND REPLACEMENT (WEST SIDE)**



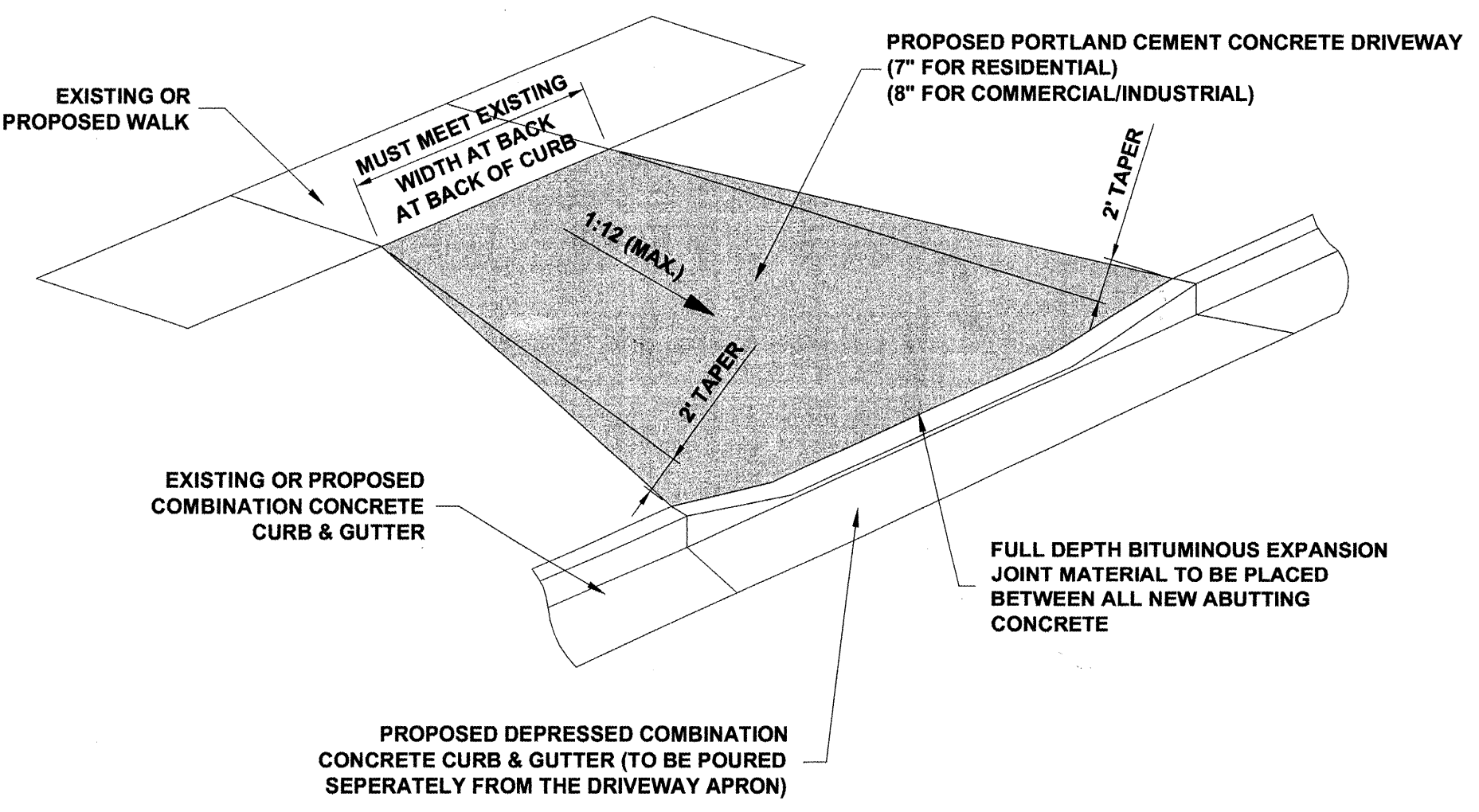
RESTRICTED DEPTH CATCH BASIN



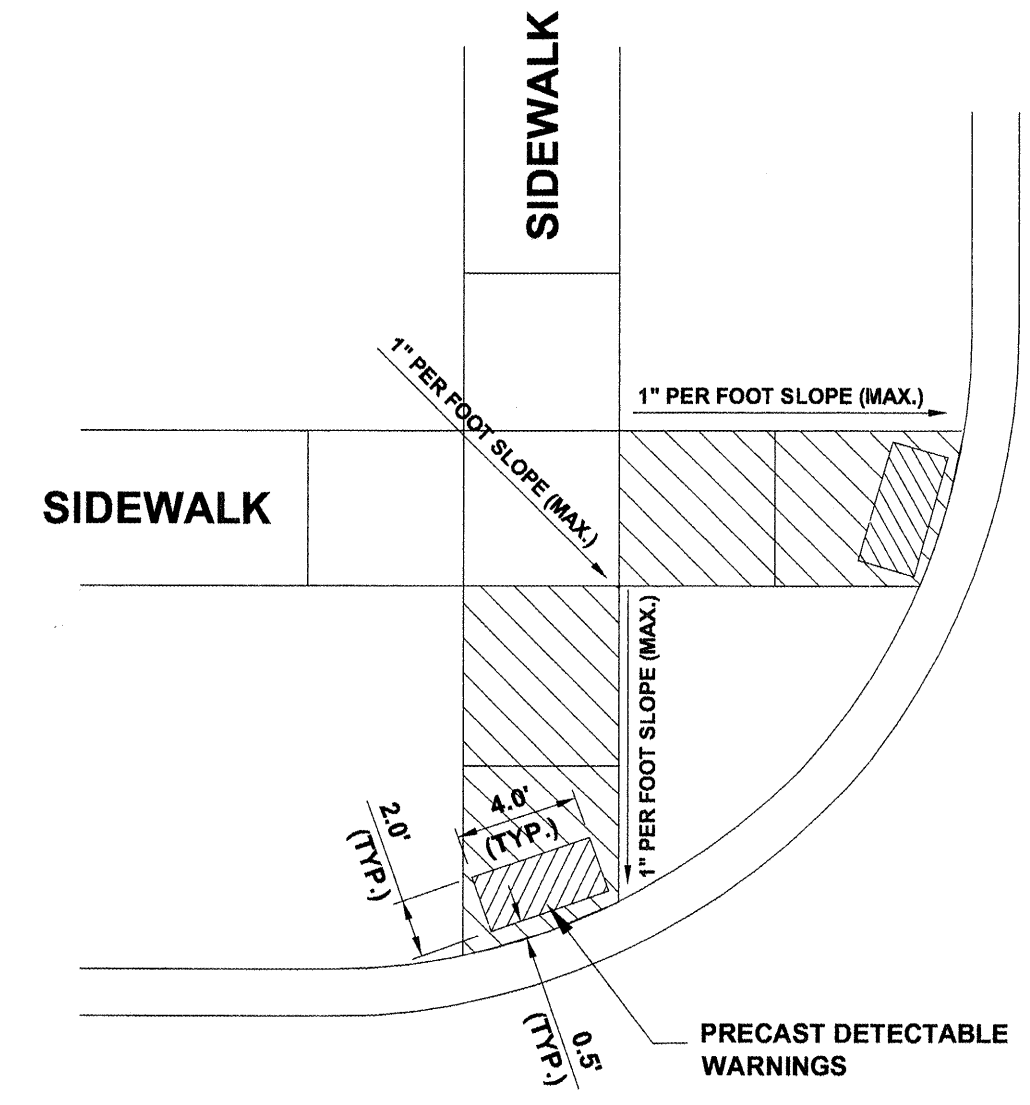
**TYPICAL CURB AND GUTTER
EXPANSION JOINT**



**CAST-IN-PLACE COMPOSITE PAVER TILE
FOR DETECTABLE WARNING**



DRIVEWAY



A.D.A. RAMP

NOTE:
COMPOSITE PAVER TILE USED SHALL BE AS MANUFACTURED BY ADA SOLUTIONS, INC., OR AS APPROVED BY ENGINEER.

Drawing file: W:\Projects\83809245 - Gardner Rd Resurf (APP)\Detail-GardnerRd.dwg Mar 29, 2010 - 3:03pm

HANCOCK ENGINEERING
Civil Engineers
Municipal Consultants
Established 1911

9933 Roosevelt Road
Waukegan, IL 60054-2780
Phone: 708.965.6200
Fax: 708.965.6210

USER NAME --	DESIGNED -- TG	REVISED --
PLOT SCALE --	DRAWN -- TG	REVISED --
PLOT DATE --	CHECKED -- DT	REVISED --
	DATE -- 03/26/10	REVISED --

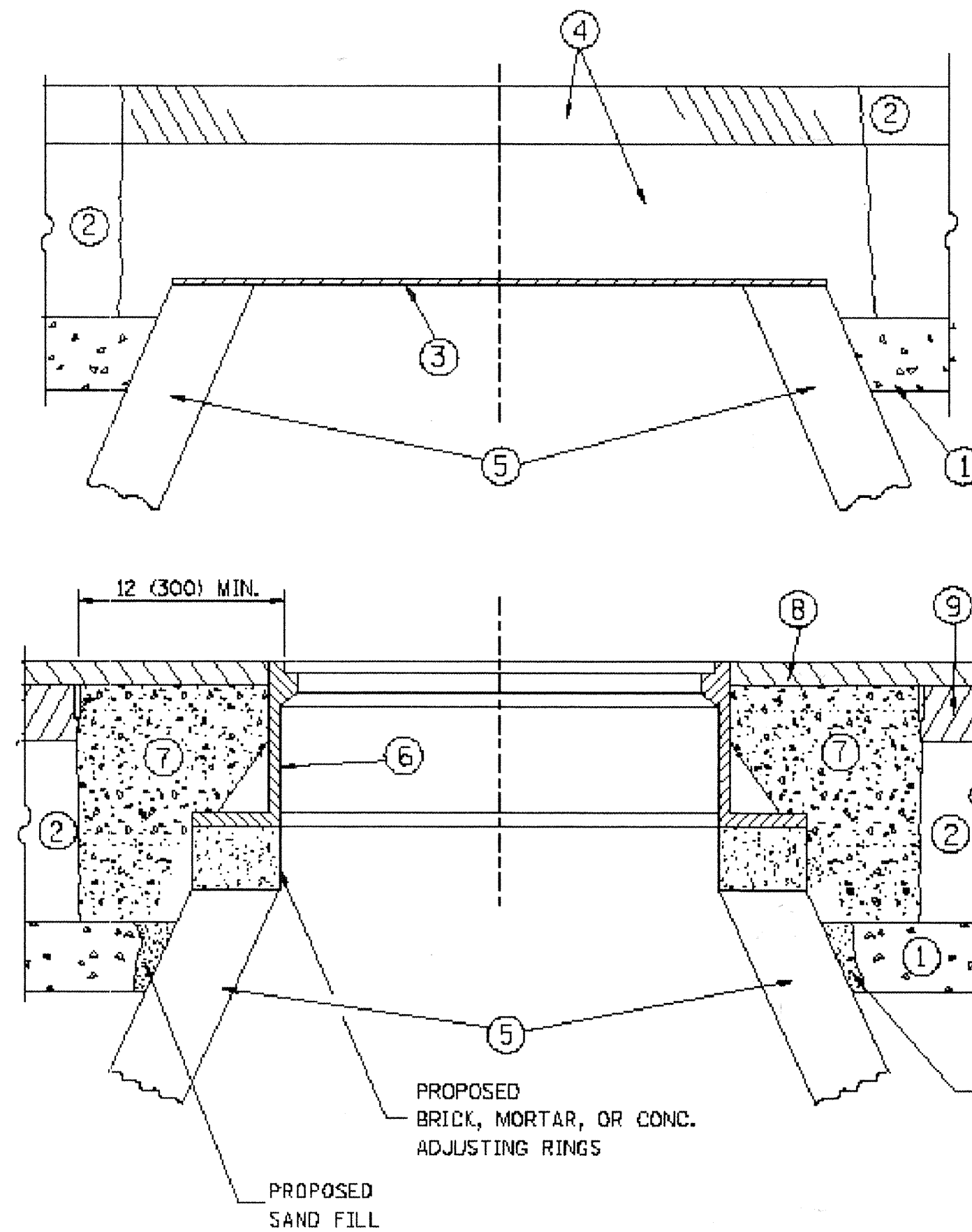
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1005	09-00075-00-RS	COOK	11	7
CONTRACT NO. 63458				
FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT M-9003(578)		

Drawing file: W:\Projects\33699245 - Gardner Rd Resurf (APP)\Workings-GardnerRd.dwg Mar 29, 2010 - 3:05pm



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS S1 CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

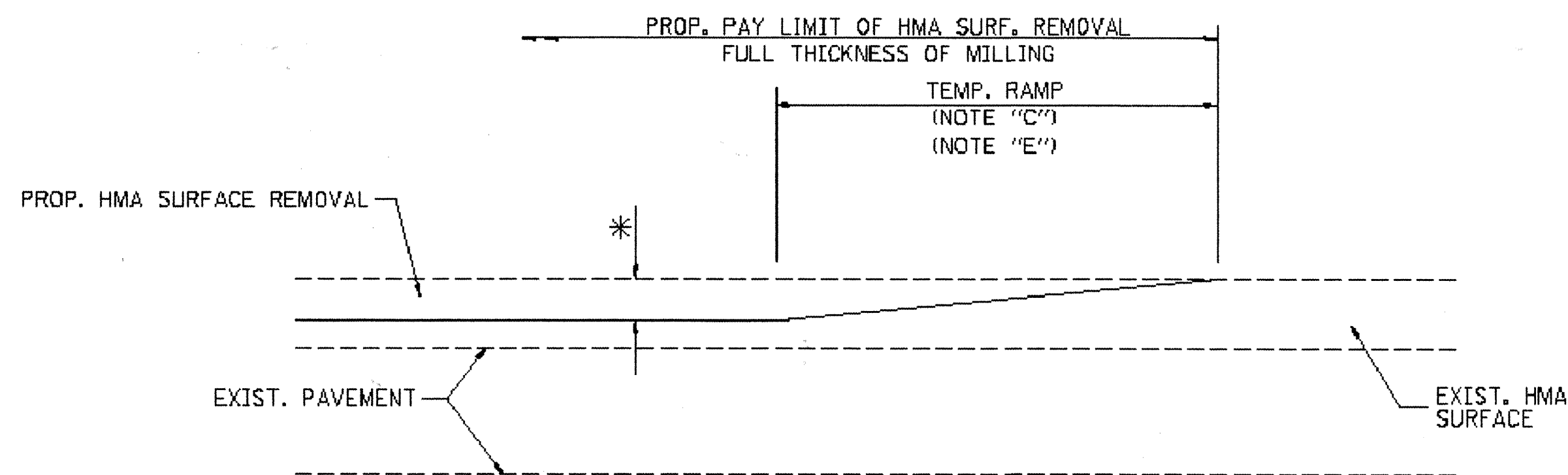
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

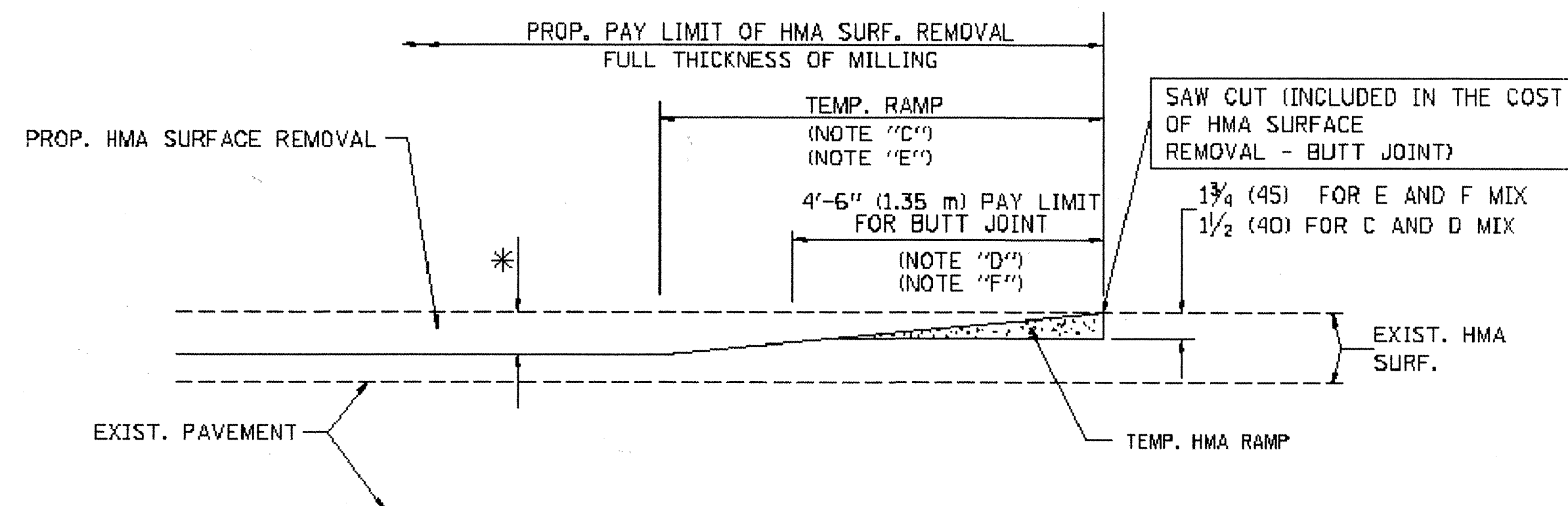
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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PLOT SCALE = 50.0000 / 1 IN					CHECKED -	REVISED - R. WIEDEMAN 05-14-04	BD08-03 (BD-8)		CONTRACT NO. 63458	
PLOT DATE = 1/4/2008					DATE - 10-25-94	REVISED - R. BORO 01-01-07	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(678)		E.H.E. PROJECT NO. 838-09-24501	
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS			STA. TO STA.					



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

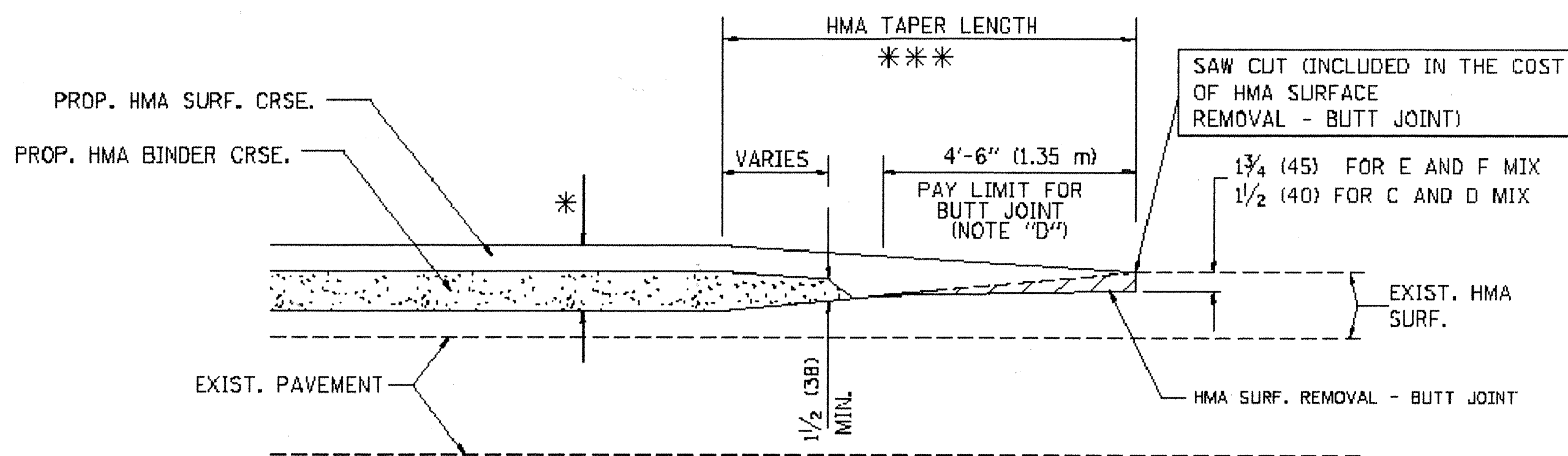
OPTION 1



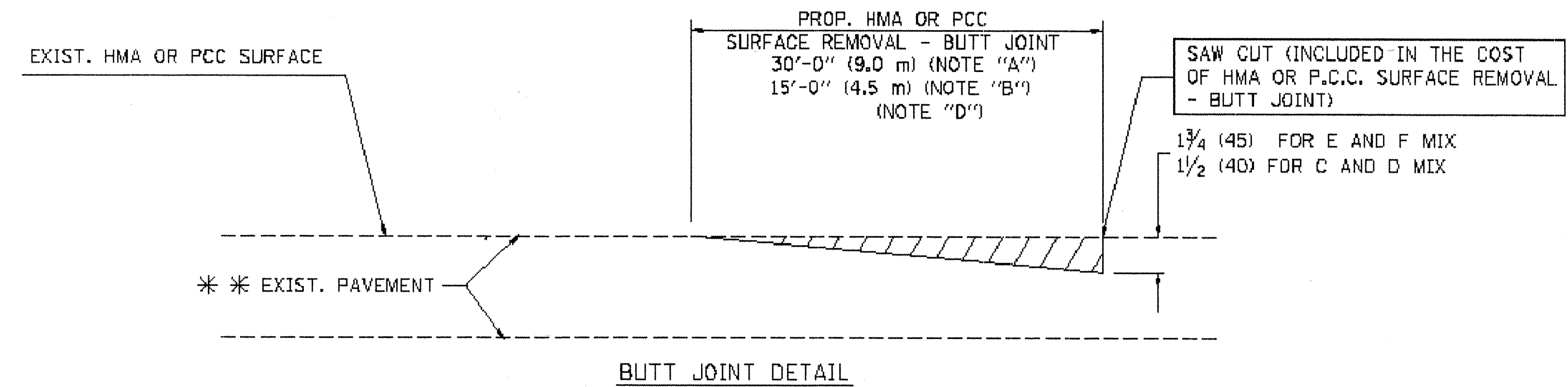
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

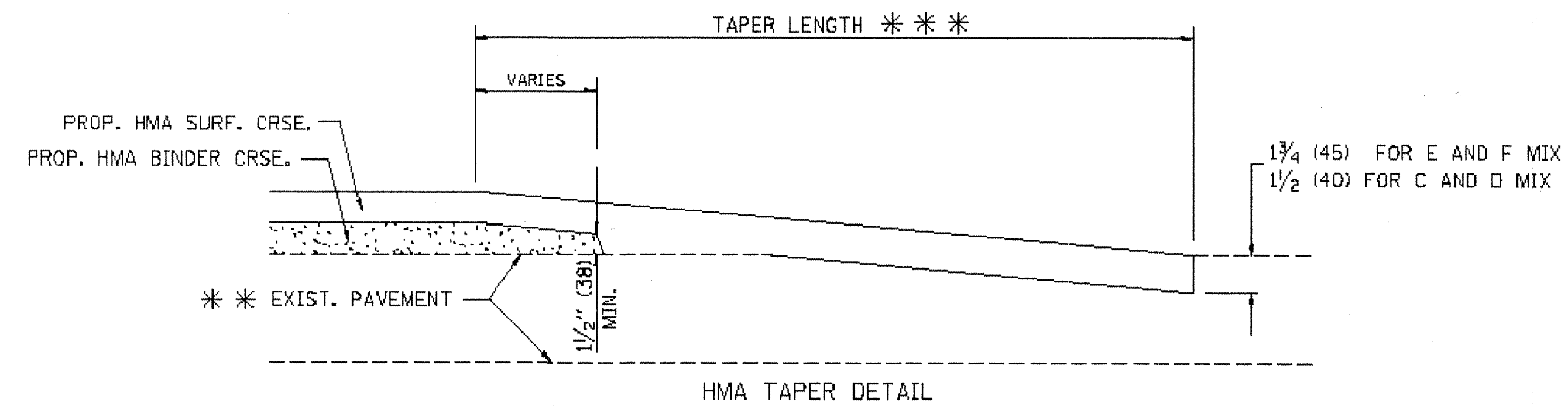
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

Drawing file: W:\Projects\33809245 - Gardner Rd Resurf (APP)\Markings-GardnerRd.dwg Mar 29, 2010 - 3:06pm

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USER NAME = geglennob
PLOT SCALE = 50.0000 / IN
PLOT DATE = 1/4/2008

DESIGNED - M. DE YONG
DRAWN -
CHECKED -
DATE - 06-13-90

REVISED - R. SHAH 10-25-94
REVISED - A. ABBAS 03-21-97
REVISED - M. GOMEZ 04-06-01
REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

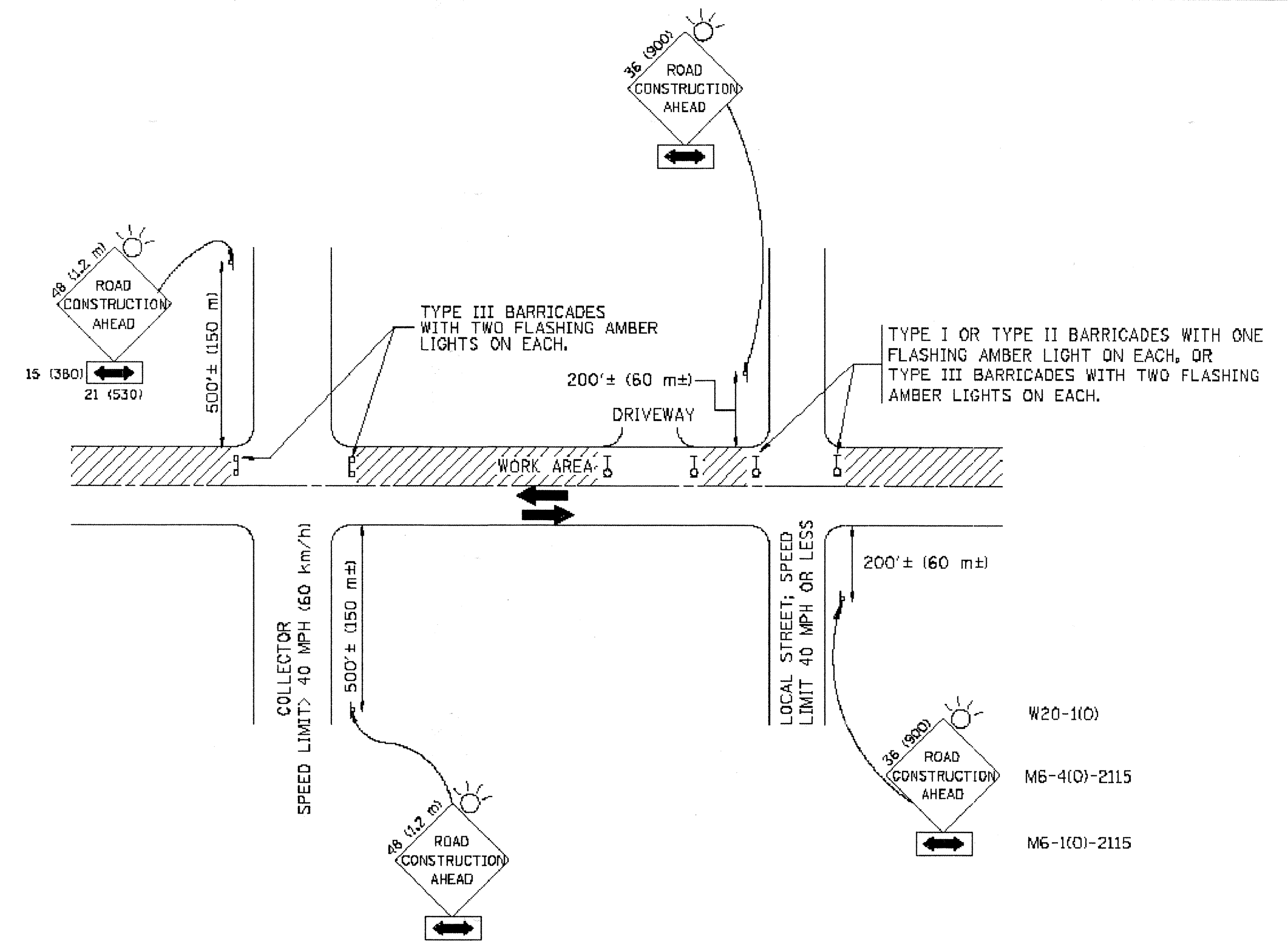
BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1005	09-0075-00-RS	COOK	11	9
BD400-05 B032			CONTRACT NO. 63458	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(578)				

E.H.E. PROJECT NO. 838-09-24501

Drawing File: W:\Projects\33809245 - Gardner Rd Resurf (LAPP)\Markings-GardnerRd.dwg Mar 29, 2010 - 3:06pm



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. T01606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (Inches) unless otherwise shown.

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

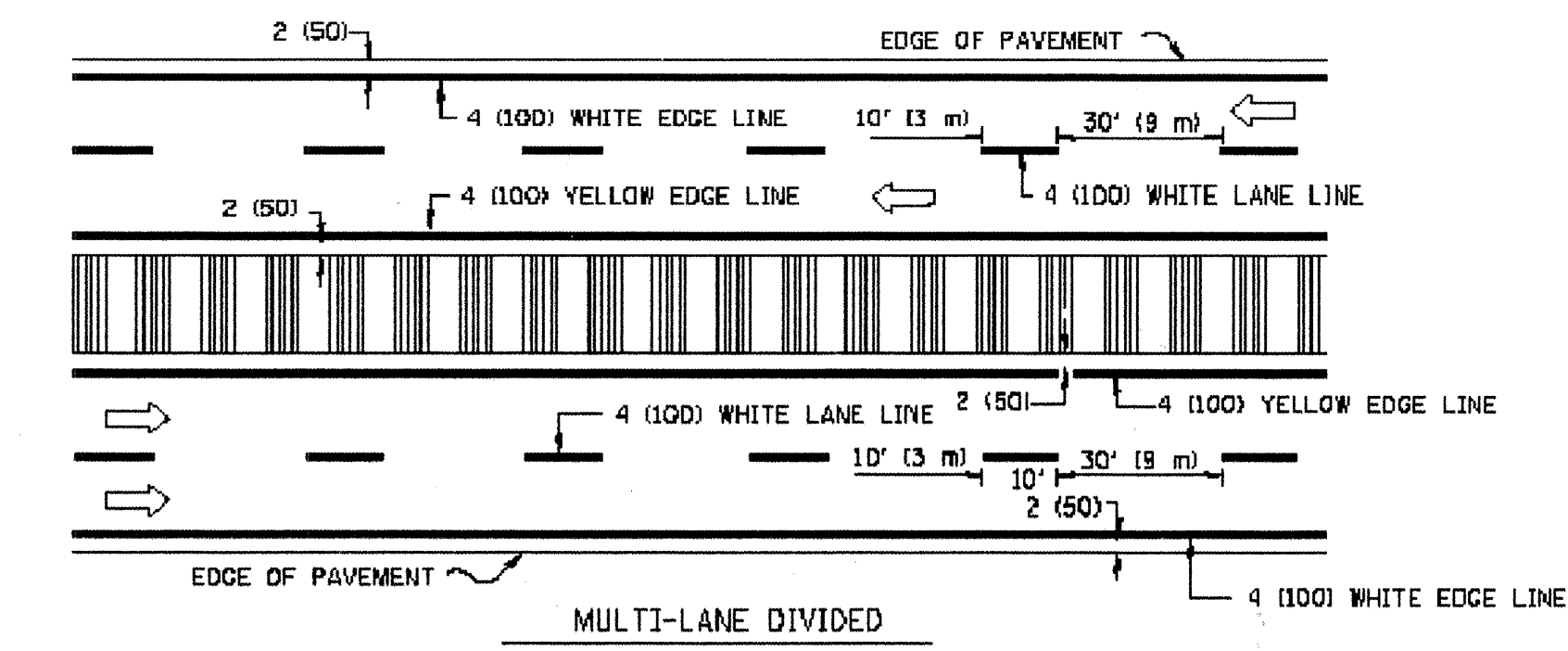
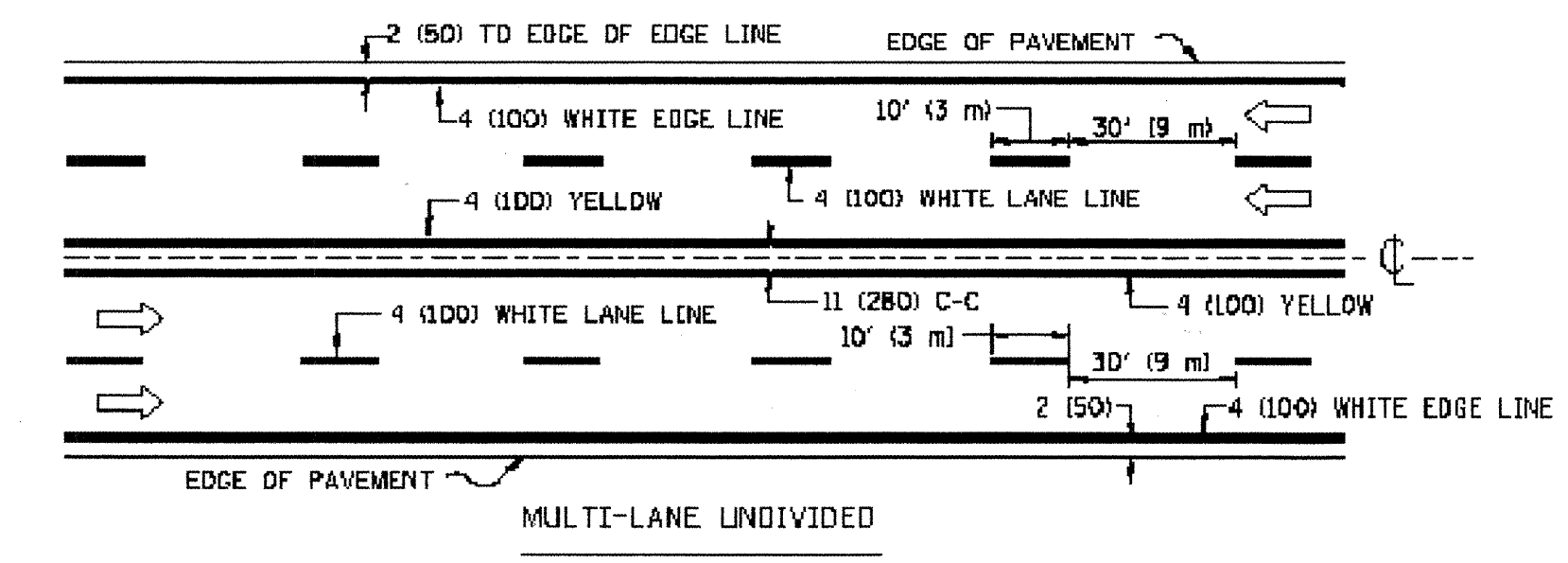
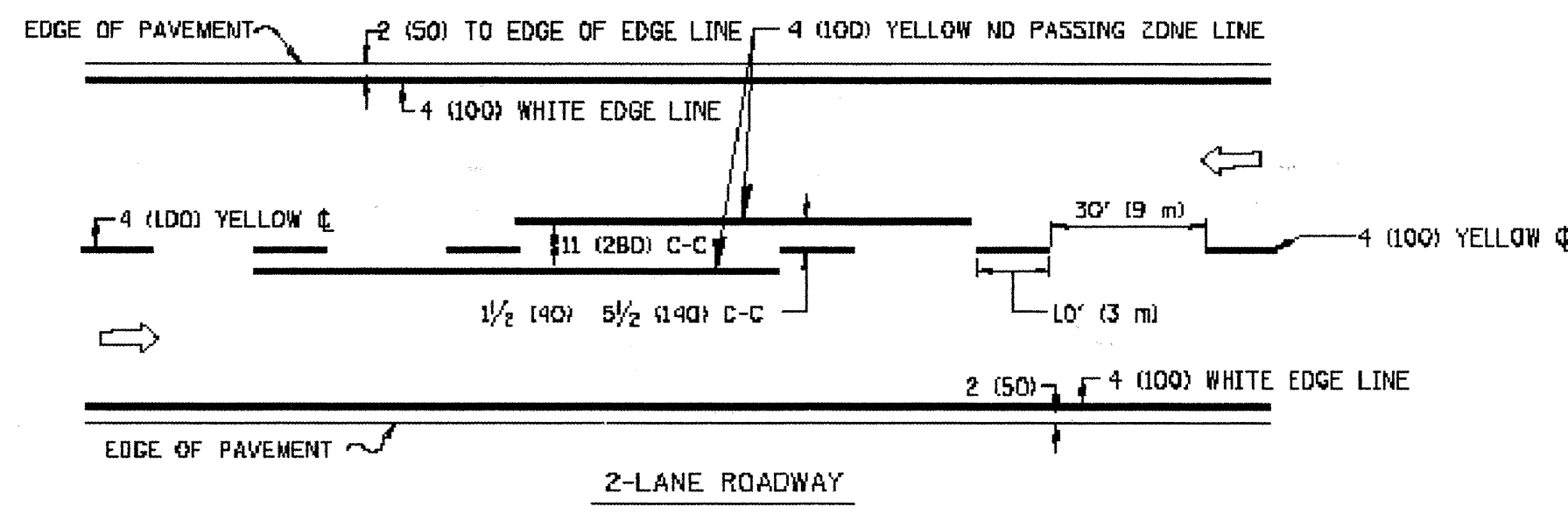
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 60.000 1 / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	F.A.U. RTE. 1005	SECTION 09-00075-00-RS	COUNTY COOK	TOTAL SHEETS 11	SHEET NO. 10
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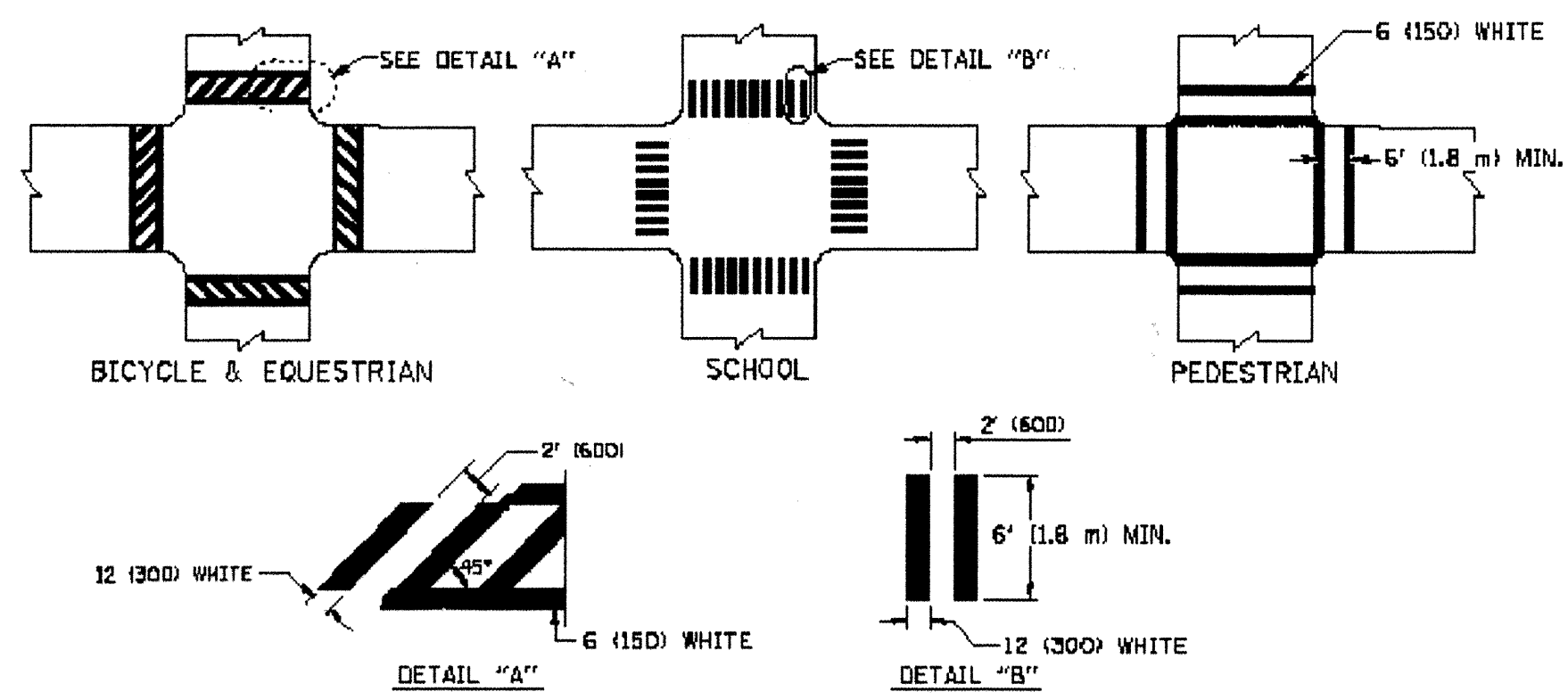
TC-10		CONTRACT NO. 63458
FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT M-9003(578)

E.H.E. PROJECT NO. 838-09-24501

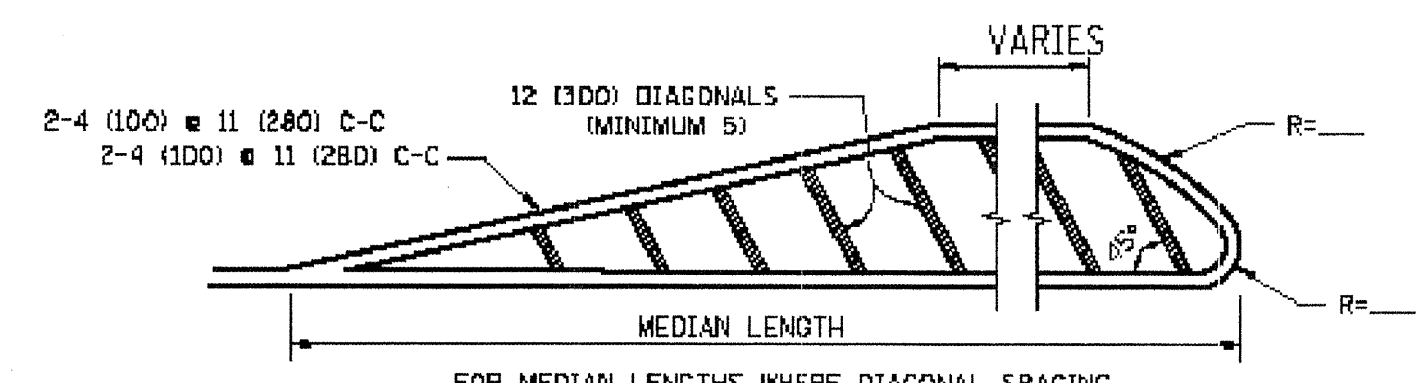
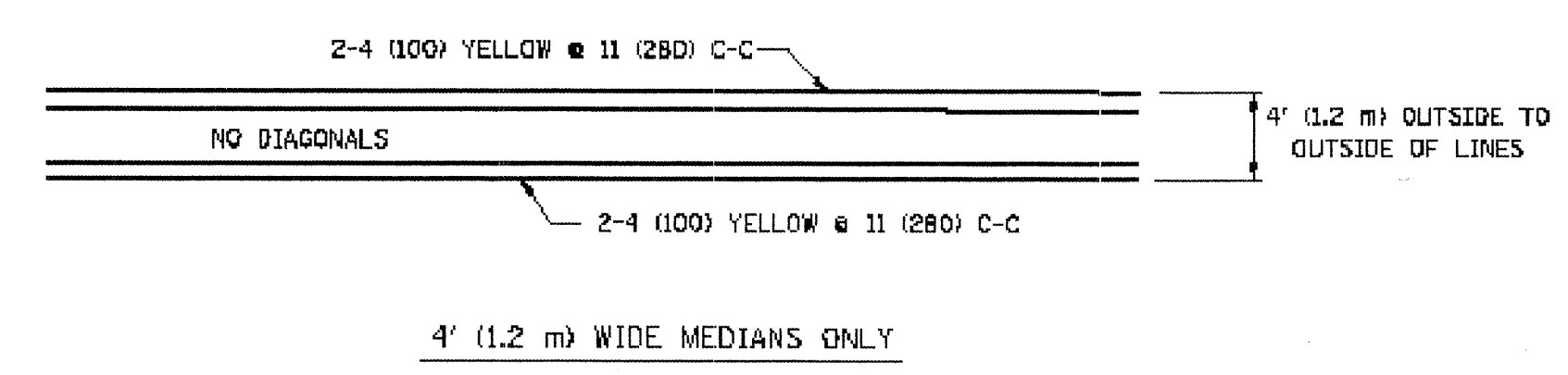


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



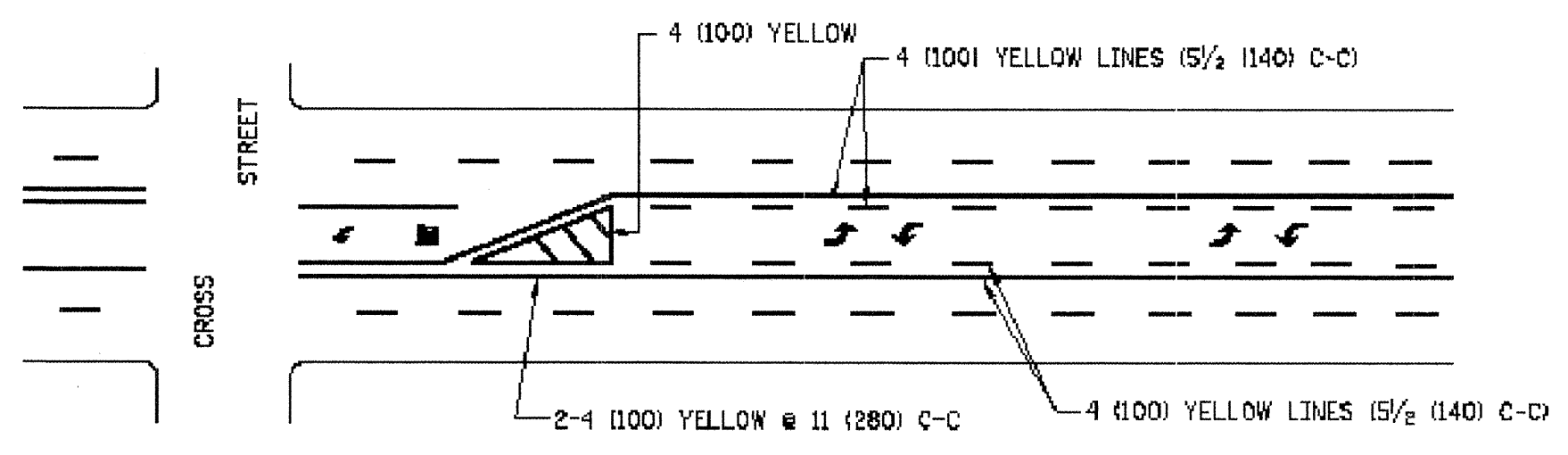
TYPICAL CROSSWALK MARKING



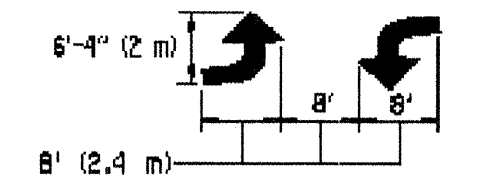
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

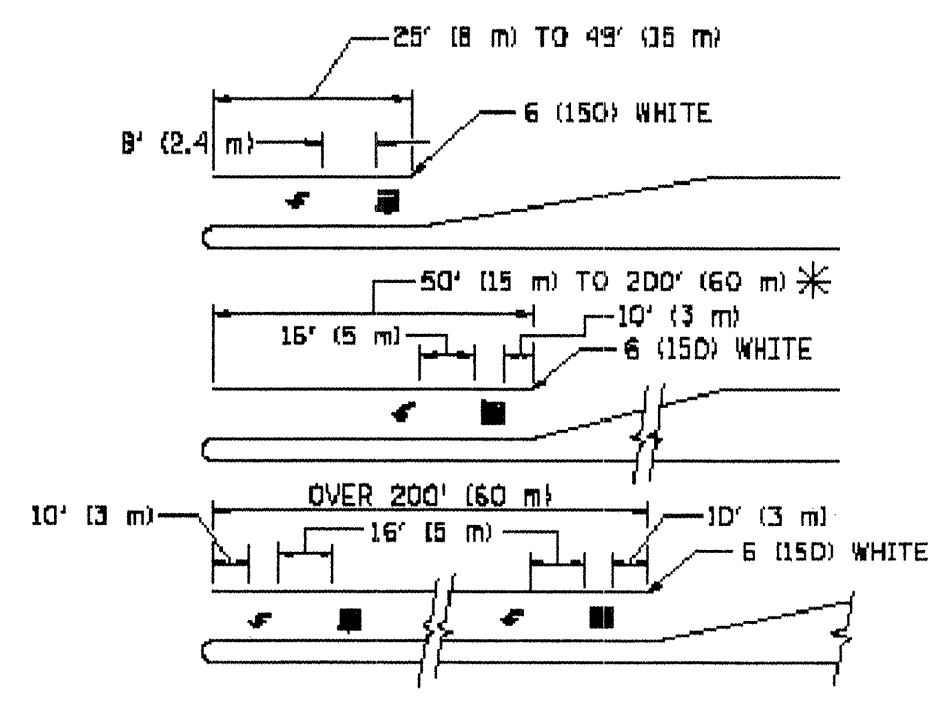


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

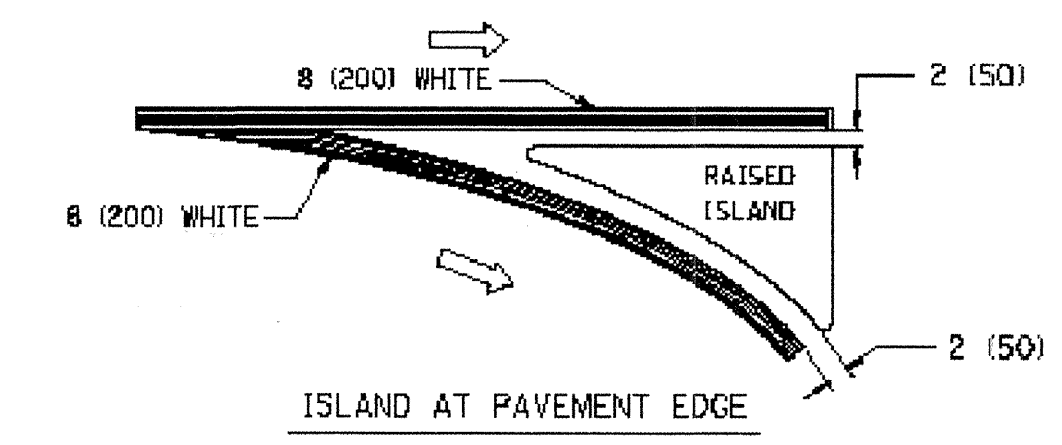
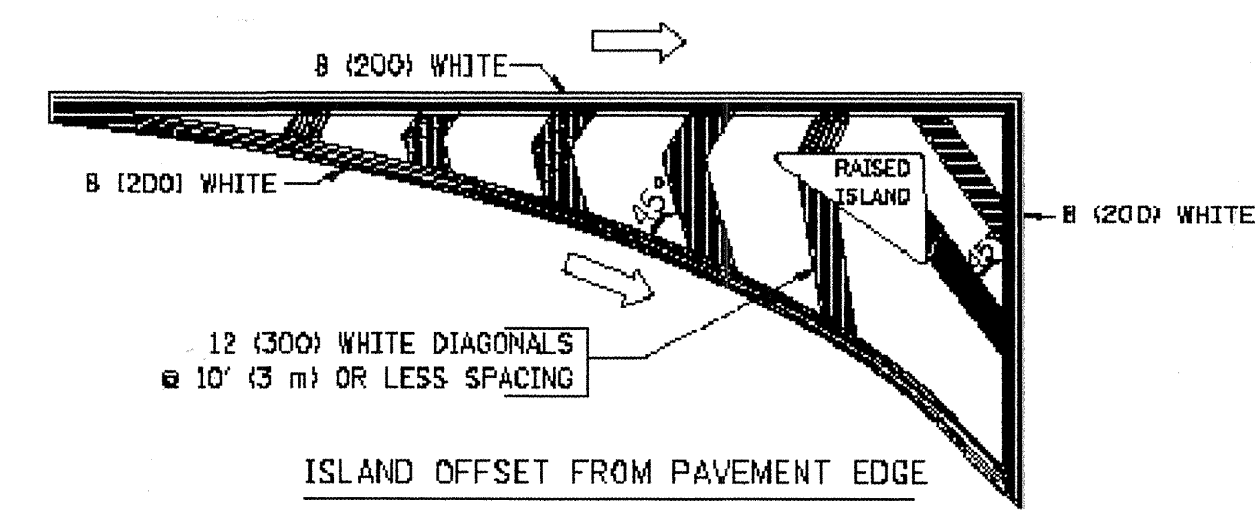


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 * AREA = 15.6 SQ. FT. (1.5 m²) ; □ AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWD-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES, "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 78000L AREA OF: "R"=3.8 SQ. FT. (0.33 m ²) EACH "X"=84.0 SQ. FT. (7.8 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 78000L.

All dimensions are in inches (millimeters) unless otherwise shown.

Drawing File: W:\Projects\83809245 - Gardner Rd Resurf (LAPP)\Markings-Corcher\rd.dwg Mar 29, 2010 - 3:07pm

FILE NAME =	USER NAME = drivakoegn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
or:\pwork\pudot\drivakoegn\0108315\to3.dgn		DRAWN -	REVISED - C. JUCCIUS 09-09-09
PLOT SCALE = 80.000 1/ IN.		CHECKED -	REVISED -
PLOT DATE = 9/9/2009		DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1005	09-00075-00-RS	COOK	11	11
TC-18		CONTRACT NO. 63458		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(578)				