

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2790	10-00212-00-RS	COOK	16	1
F.H.W.A. REG.		ILLINOIS	PROJECT ARA-9003(645)	

CONTRACT NO. 63464

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STATE STANDARDS

- 000001-05 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
- 424001-05 CURB RAMPS FOR SIDEWALK
- 442201-03 CLASS C AND D PATCHES
- 602601-02 PRECAST REINFORCED CONCRETE FLAT SLAB TOP
- 602701-02 MANHOLE STEPS
- 604001-03 FRAMES & LIDS-TYPE 1
- 701301-03 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701311-03 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
- 701501-05 URBAN LANE CLOSURE, 2 L, 2 W UNDIVIDED
- 701701-06 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701801-04 LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
- 701901-01 TRAFFIC CONTROL DEVICES
- 780001-02 TYPICAL PAVEMENT MARKINGS
- 814001-02 HANDHOLES
- 814006-02 DOUBLE HANDHOLES

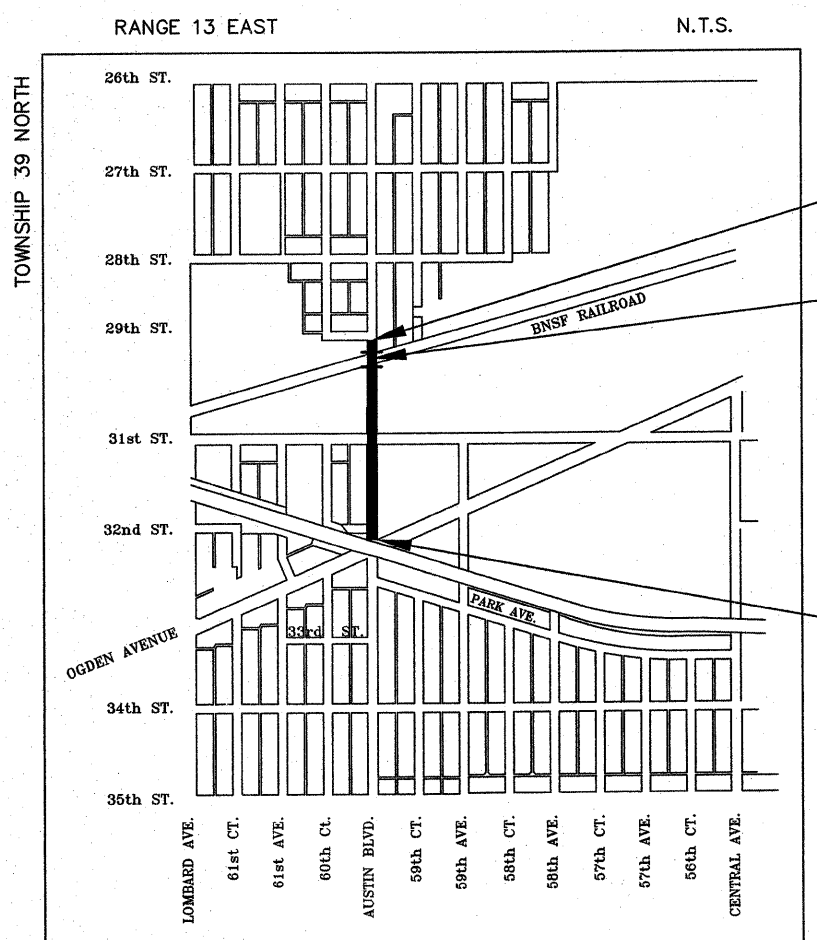
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
 FAU 2790 (AUSTIN BOULEVARD)

OGDEN AVE. (FAP 311) TO 29TH STREET
 RESURFACING
 PROJECT ARA-9003(645)
 SECTION 10-00212-00-RS
 TOWN OF CICERO
 COOK COUNTY
 C-91-551-10

PROJECT LOCATION MAP



LOCATION OF SECTION INDICATED THIS: [Symbol]



AUSTIN BOULEVARD
 PROJECT ENDS
 AT STA. 61+25
 (29TH STREET)

BNSF OVERHEAD STRUCTURE
 STA. 55+84 TO STA. 60+60

AUSTIN BOULEVARD
 PROJECT BEGINS
 AT STA. 45+40
 (OGDEN AVENUE)

TRAFFIC DATA

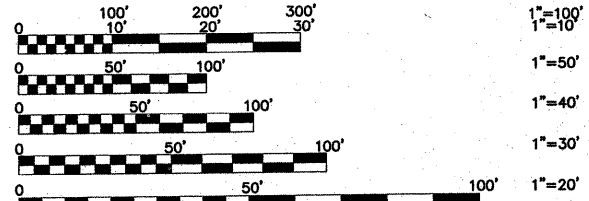
ADT: AUSTIN BOULEVARD 15,000 (2010) 15,000 (2010) MINOR ARTERIAL 1.25(COMP-20)

POSTED SPEED

30 MPH (EXISTING)
 30 MPH (PROPOSED)

DESIGN DESIGNATION:

30 MPH (EXISTING)
 30 MPH (PROPOSED)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 CALL 811

Know what's below.
 Call before you dig.

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 Civil Engineers/Municipal Consultants
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 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000928

R E V I S I O N S			
NO.	BY	DATE	DESCRIPTION
1	AMS	3/9/10	PER IDOT REVIEW
2	THK	3/12/10	PER IDOT REVIEW
3	AMS	3/23/10	PER IDOT REVIEW

CONTRACT NO. 63464

— DENOTES LOCATION OF IMPROVEMENT IN CICERO TOWNSHIP

LENGTH OF PROJECT
 GROSS LENGTH OF PROJECT 1585 FEET (0.3002 MILES)
 NET LENGTH OF PROJECT 1585 FEET (0.3002 MILES)

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

APPROVED: 3-23 2010
 TOWN OF CICERO: Larry Downick
 LARRY DOWNICK, TOWN PRESIDENT

PASSED: MARCH 31 2010
Chad Allen
 DISTRICT #1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID
 BASED ON LIMITED REVIEW: APRIL 2, 2010
Diane M. O'Leary
 DEPUTY DIRECTOR OF HIGHWAYS, REGION #1 ENGINEER

Timothy P. Geary
 TIMOTHY P. GEARY, P.E.
 E. NO. 062-043796
 EXPIRES 11-30-2011
3-23-10
 DATE

**PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS**

FIELD ENGINEER: MARILIN SOLOMON (847)705-4407

BENCHMARKS

DESCRIPTION	ELEVATION
NNE BOLT ON F.H. AT 2822 S AUSTIN BOULEVARD	609.08
SW BOLT ON F.H. AT NW CORNER OF PARK AVENUE AND AUSTIN BOULEVARD	607.33

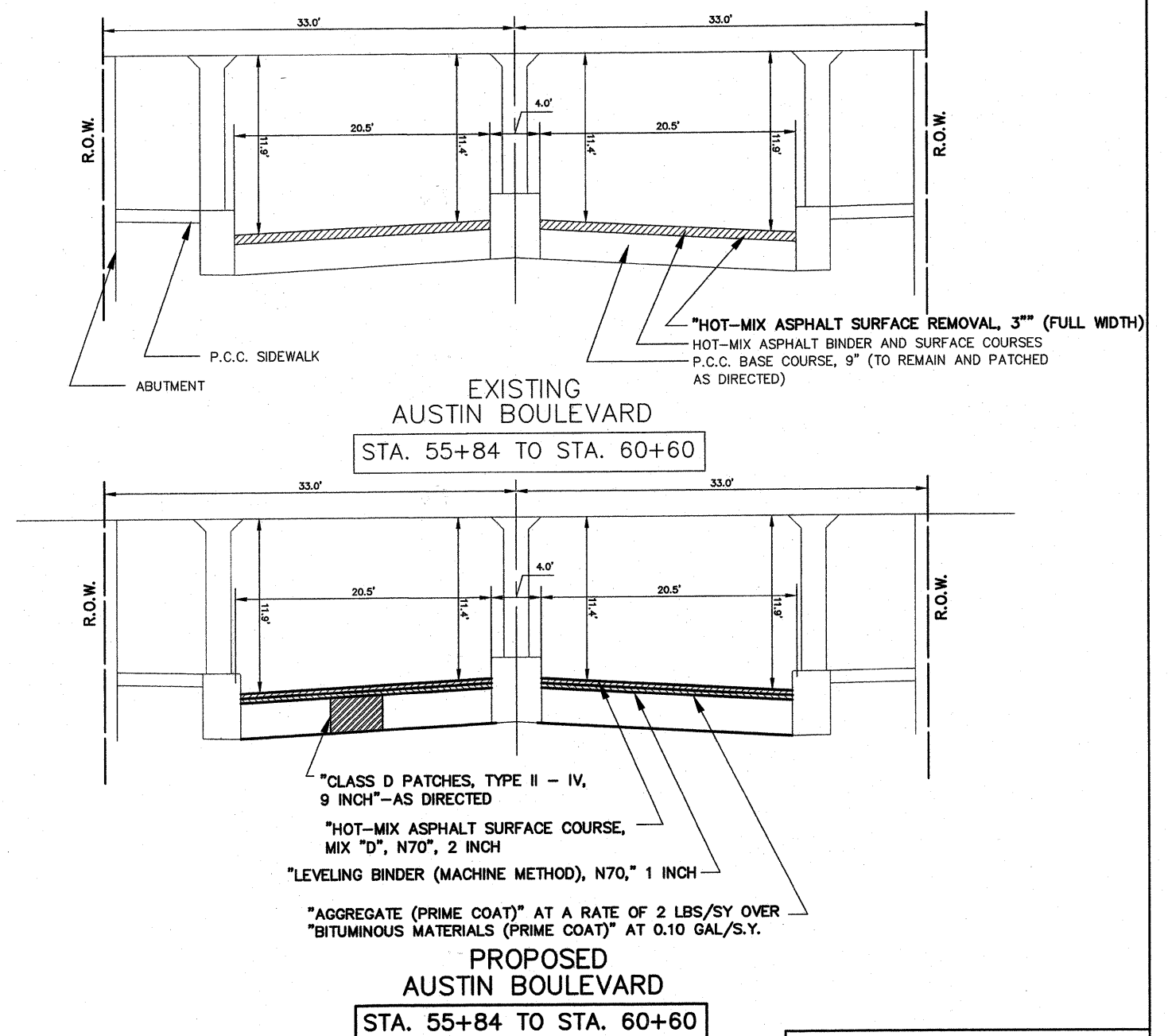
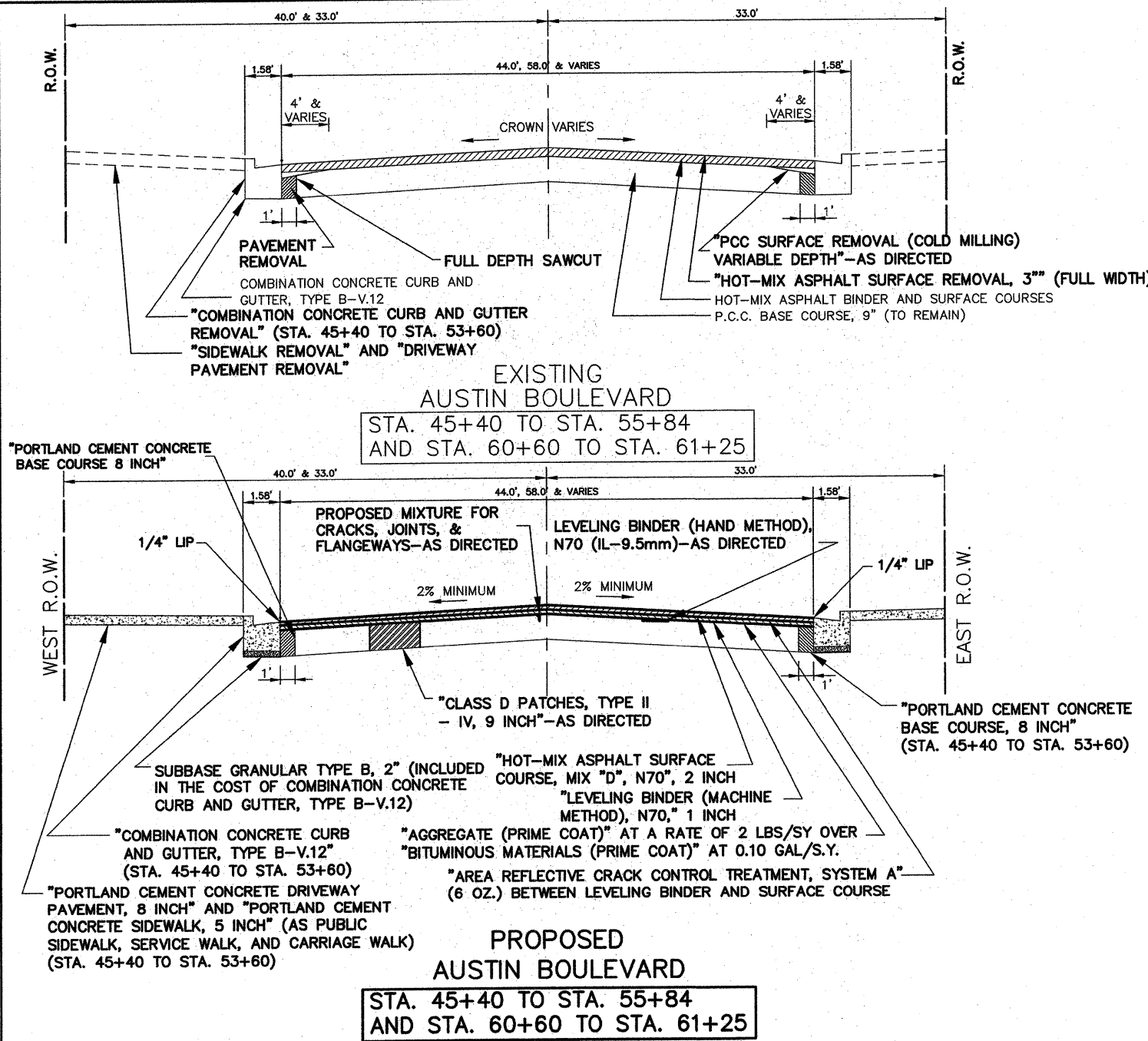
HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	PERCENT AIR VOIDS
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2" (IL-9.5mm)	4% @ 70 GYR
LEVELING BINDER (MACHINE METHOD), N70, 1" (IL-9.5mm)	4% @ 70 GYR
LEVELING BINDER (HAND METHOD), N70 (IL-9.5mm)	4% @ 70 GYR
PATCHING	
CLASS D PATCHES, TYPE II - IV, 9", (HMA BINDER IL-19.0mm)	4% @ 70 GYR
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19.0mm)	

THE UNIT WEIGHT TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
 THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG70-22" AND NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
 FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZE PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.



TYPICAL CROSS SECTIONS

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

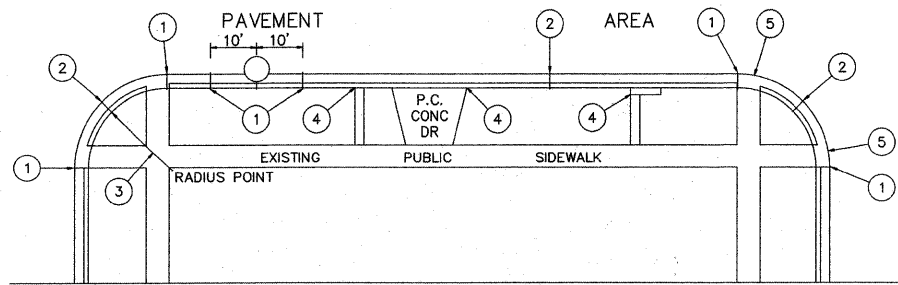
TYPICAL CROSS SECTIONS, BENCHMARKS, AND
 HOT-MIX ASPHALT MIXTURE REQUIREMENTS

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		DATE - 1/28/10	REVISED -

SCALE: 1"=6' SHEET NO. OF SHEETS STA. TO STA.

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F.A. -	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2790	10-00212-00-RS	COOK	16	3
CONTRACT NO. 63464				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT ARA-9003(645)		



1. EXPANSION JOINTS AT TANGENT POINTS AND 150 FOOT INTERVALS, CONSISTING OF ONE INCH PREMOLDED JOINT FILLER MATERIAL WITH #8 DOWEL BARS, 18" IN LENGTH, GREASED, PROVIDE EXPANSION CAP ON ONE END. ALSO CONSTRUCT THIS JOINT TEN FEET EACH SIDE OF PROPOSED UNDERGROUND STRUCTURE.
2. CONTRACTION JOINTS AT TWENTY-FIVE FOOT INTERVALS AND AT THE CENTER OF RETURNS.
3. ALL RADII SHALL BE 25 FEET TO THE BACK OF CURB UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
4. LONGITUDINAL EXPANSION JOINT CONSISTING OF ONE INCH PREMOLDED JOINT FILLER.
5. DEPRESS CURB AT LOCATIONS WHERE PUBLIC WALKS INTERSECT CURB LINE AT STREET INTERSECTIONS, ALLEYS, AND OTHER LOCATIONS AS DIRECTED, FOR THE CONSTRUCTION OF RAMPED SIDEWALKS FOR ACCESS BY THE HANDICAPPED.

JOINT DETAILS

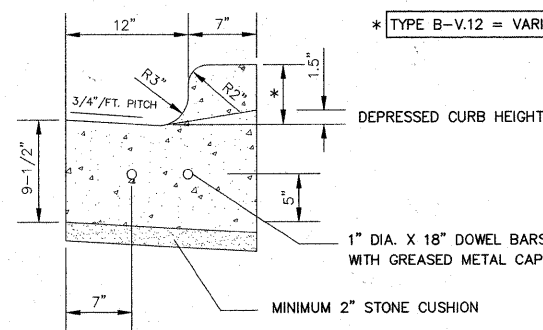
NOTES:

1" PREFORMED EXPANSION MOLDING SHALL BE PLACED WITH TWO SMOOTH 1" DIA. DOWEL BARS WITH GREASED CAPS AT ALL POINTS OF CURVATURE AND CORNERS.

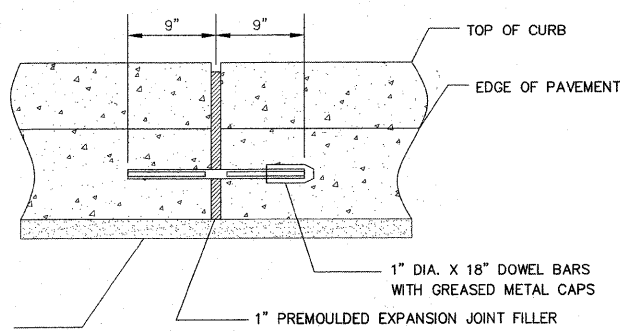
CONTRACTION JOINTS SHALL BE SAW CUT OR TOOLED TO A DEPTH OF 2" @ 15' MINIMUM SPACING. SAW CUT CONTRACTION JOINTS SHALL BE DONE WITHIN 24 HR.. ALL CONTRACTION JOINTS SHALL BE SEALED WITH AN IDOT APPROVED JOINT SEALANT.

AN IDOT APPROVED CURING COMPOUND SHALL BE USED ON ALL PROPOSED CONCRETE CURB AND GUTTER.

3/4" TIE ANCHOR BARS SHALL BE INSTALLED WHERE CURB AND GUTTER IS PLACED AGAINST RIGID BASE MATERIALS AND IN THE ENDS OF EXISTING CURB AND GUTTER TO TIE THE NEW CURB TO THE EXISTING. TIE ANCHOR BARS WILL BE PAID FOR SEPARATELY.

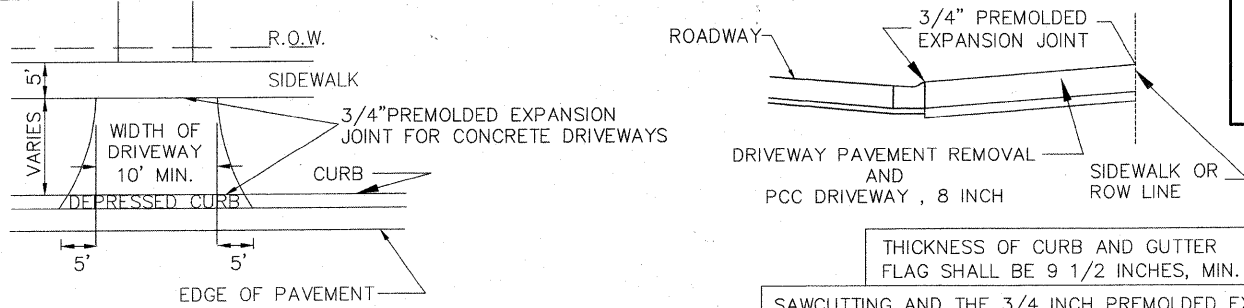


* TYPE B-V.12 = VARIABLE HEIGHT CURB VARIES FROM 4" TO 6"



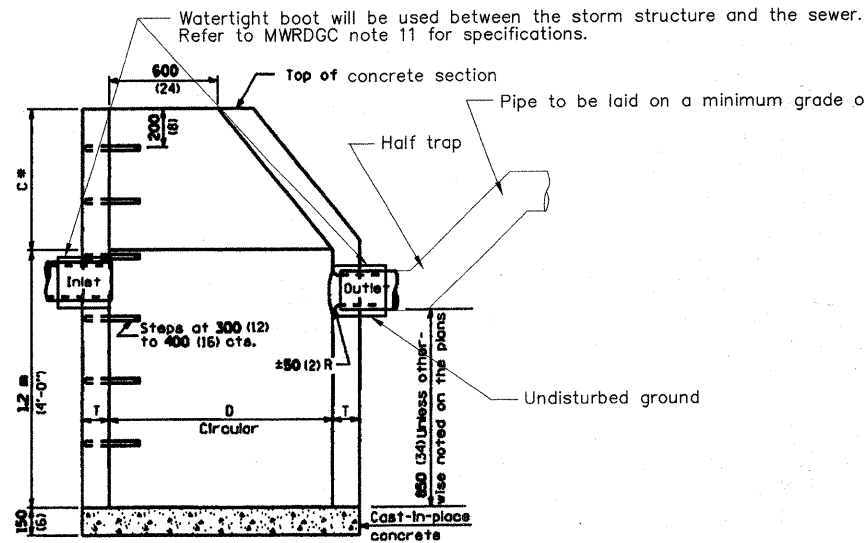
EXPANSION JOINT DETAIL

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-V.12



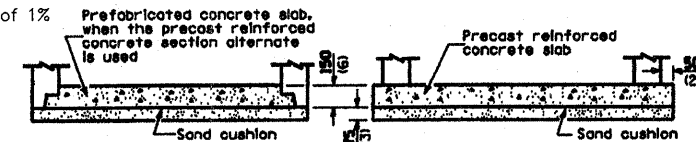
TYPICAL DRIVEWAY DETAIL

THICKNESS OF CURB AND GUTTER FLAG SHALL BE 9 1/2 INCHES, MIN.
SAWCUTTING AND THE 3/4 INCH PREMOLDED EXPANSION JOINT WILL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF PCC DRIVEWAY PAVEMENT, 8 INCH



ELEVATION

NOTE: CATCH BASIN, TYPE C, SPECIAL SHALL BE 2'-0" IN DIAMETER WITH A 15" SUMP



ALTERNATE BOTTOM SLAB

ALTERNATE MATERIALS FOR WALLS	D	C	T (min.)
Precast Reinforced Concrete Section	1.2 m (4'-0") 1.5 m (5'-0")	750 (30) 1,125 (45)	100 (4) 125 (5)
Cast-in-place Concrete	1.2 m (4'-0") 1.5 m (5'-0")	750 (30) 1,125 (45)	100 (4) 125 (5)

GENERAL NOTES

All catch basins shall be 1.2m (4'-0") in diameter unless otherwise noted on the plans.

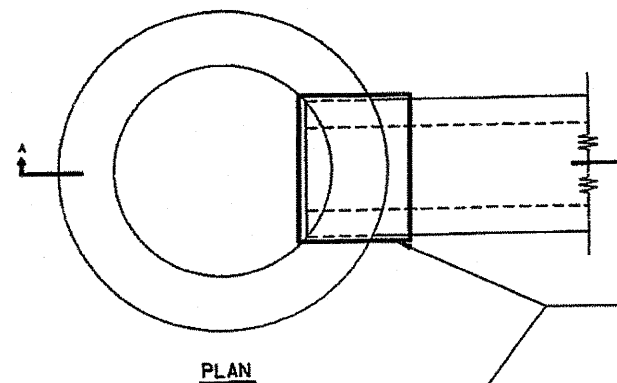
See Standard IDOT 602601 for optional precast reinforced concrete flat slab top.

*Dimension C for precast reinforced concrete section may vary form the dimension given to plus 150mm (6").

See Standard IDOT 602701 for details of steps.

All dimensions are in millimeters (inches) unless otherwise shown.

CATCH BASIN, TYPE A, SPECIAL

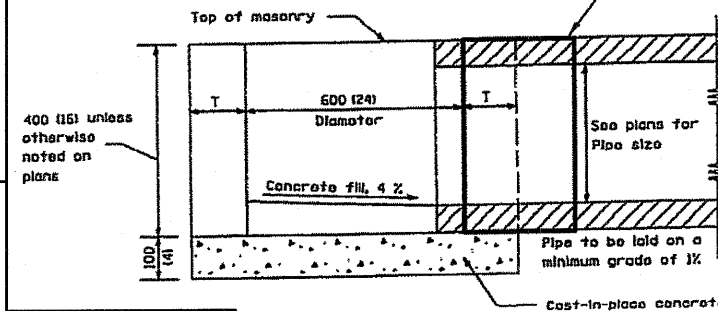


PLAN

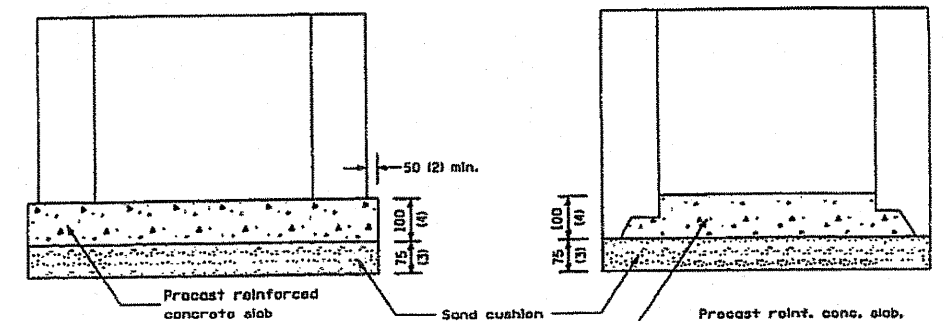
TOP OF PRECAST STRUCTURE

ALTERNATE MATERIALS FOR WALLS	T
BRICK MASONRY	200 (8)
CAST-IN-PLACE CONCRETE	150 (6)
CONCRETE MASONRY UNIT	125 (5)
PRECAST REINFORCED CONCRETE SECTION	75 (3)

A watertight boot will be used between the storm structure and the sewer. Refer to MWRDGC note 11 for specifications.



SECTION A-A



ALTERNATE METHODS

INLET, TYPE A, SPECIAL

All dimensions are in millimeters (inches) unless otherwise shown.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SPECIAL PROJECT DETAILS

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

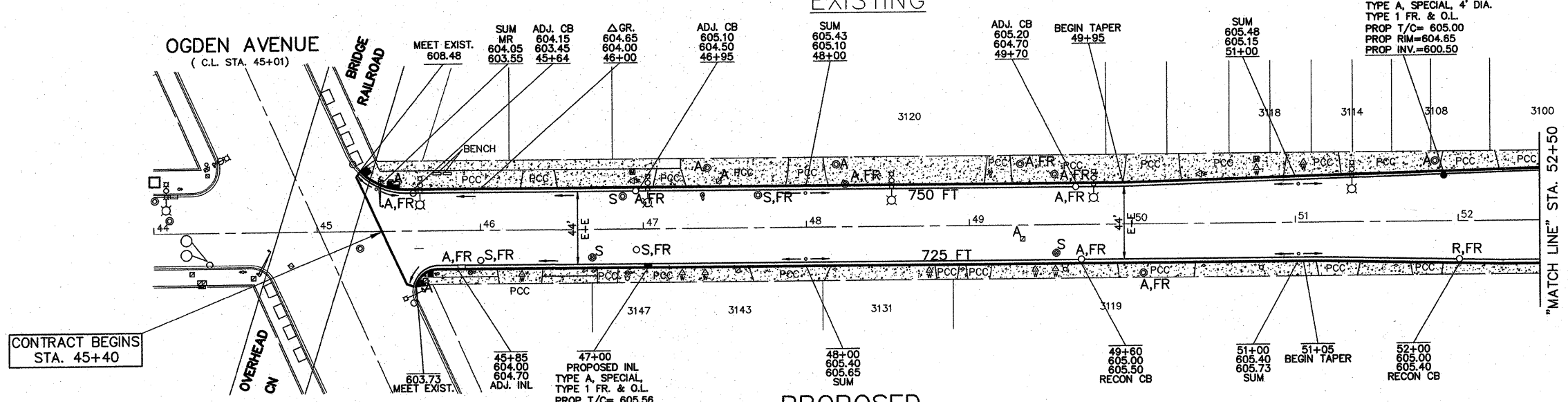
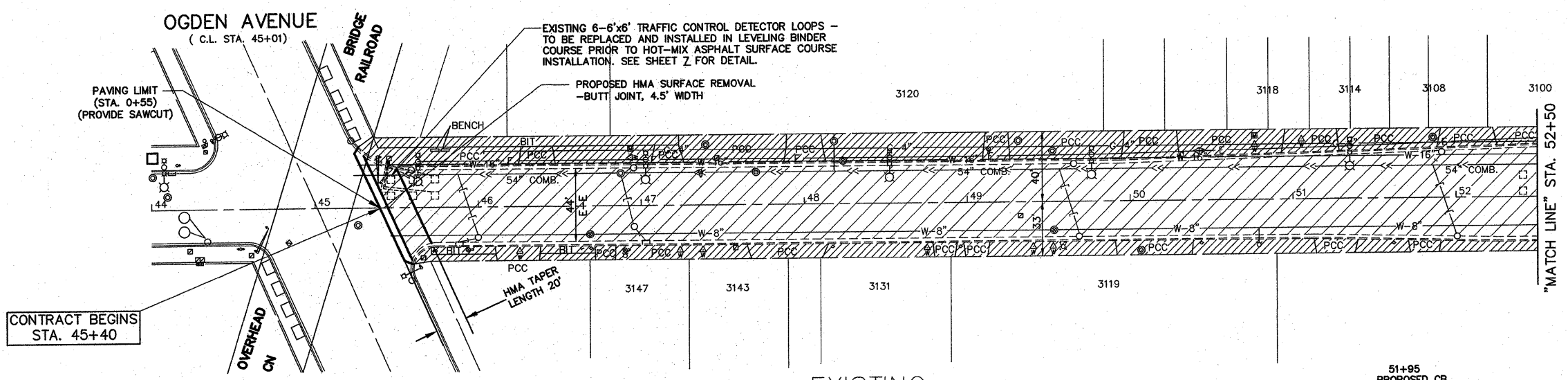
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 Civil Engineer
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000205

F.A. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2790	10-00212-00-RS	COOK	16	4
CONTRACT NO. 63464				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT ARA-9003(645)				



LEGEND

- "A" DENOTES EXISTING VALVE VAULTS, VALVE BOXES, INLETS, CATCH BASINS, HANDHOLES AND MANHOLES TO BE CLEANED, GROUTED, AND ADJUSTED TO GRADE WITH NEW ADJUSTING RINGS. (ALL TOP BANDS OF FALLEN, COMMON OR CONCRETE BRICK TO BE REPLACED WITH PRECAST CONCRETE ADJUSTING RINGS)
- "S" DENOTES EXISTING STRUCTURES TO BE ADJUSTED (SPECIAL)
- "R" DENOTES EXISTING STRUCTURES TO BE RECONSTRUCTED
- "FR" DENOTES FRAME AND LID TO BE REPLACED
- [Hatched pattern] DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, 3" (FULL WIDTH), TO BE REPLACED WITH LEVELING BINDER (MACHINE METHOD), N70, 1 INCH, AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A AND HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2 INCH
- [Diagonal hatched pattern] DENOTES DRIVEWAY PAVEMENT REMOVAL AND SIDEWALK REMOVAL (AS DIRECTED BY THE ENGINEER)
- [Solid black] DENOTES P.C. CONCRETE SIDEWALK, 5 INCH, WITH DETECTABLE WARNINGS
- [Stippled pattern] DENOTES P.C. CONCRETE DRIVEWAY PAVEMENT, 8 INCH AND P.C. CONCRETE SIDEWALK, 5 INCH ON SUBBASE GRANULAR MATERIAL, TYPE B, 2 INCH
- [Dashed line] DENOTES COMBINATION CURB AND GUTTER REMOVAL
- [Solid line] DENOTES COMBINATION CURB AND GUTTER, TYPE B-V.12
- 100.50 PROPOSED TOP OF CURB ELEVATION
- 100.00 PROPOSED GUTTER FLOW LINE ELEVATION
- 25+50 STATION



FOR CONTINUATION
SEE SHEET 2

ELEVATION	PROPOSED & EXISTING ELEVATION															
	44	45	+40	46	47	48	49	50	51	52	53				610	
610																
605																
600																
595	604.39	603.98	604.10 604.10	604.45 604.45	605.20 605.20	605.65 605.65	605.56 605.56	605.69 605.69	605.93 605.93	605.85 605.85	605.82 605.82					595

NOTE:

- 1 ALL EXISTING FRAMES AND LIDS THAT ARE TO BE REPLACED (AS DIRECTED BY THE ENGINEER) SHALL BE SALVAGED TO THE CONTRACTOR.
- 2 ALL SAWCUTS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE REMOVAL PAY ITEMS

FOR TYPICAL CROSS SECTION
OF NEW PAVEMENT WORK
SEE SHEET 4

IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

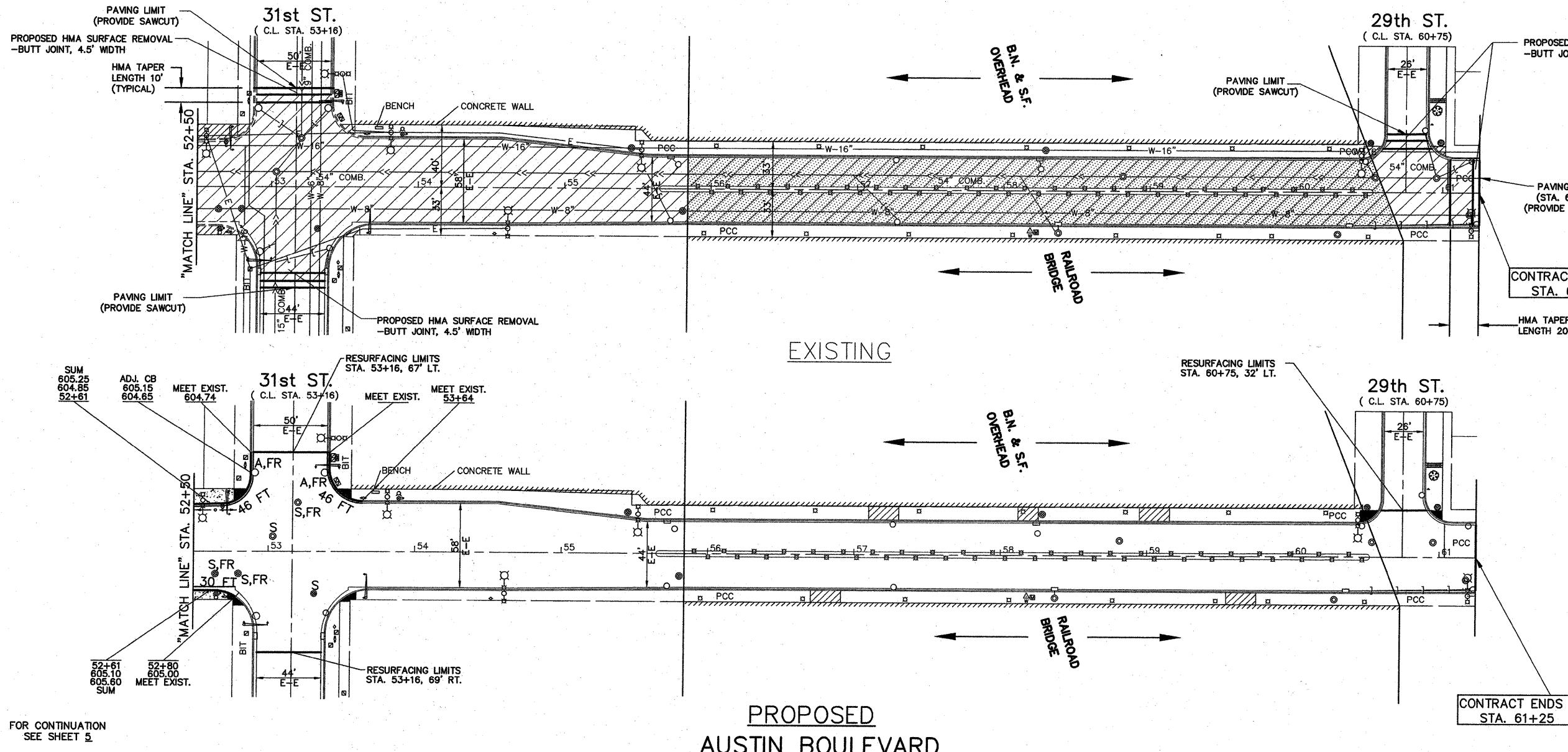
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PLAN: AUSTIN BOULEVARD
STA. 45+40 TO STA. 52+50 (RESURFACING)**
SCALE: 1" = 40' SHEET NO. OF SHEETS STA. 45+40 TO STA. 52+50

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F.A. RTE. 2790	SECTION 10-00212-00-RS	COUNTY COOK	TOTAL SHEETS 16	SHEET NO. 5
CONTRACT NO. 63464				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT AREA-9003(645)				



- LEGEND**
- "A" DENOTES EXISTING VALVE VAULTS, VALVE BOXES, INLETS, CATCH BASINS, HANDHOLES AND MANHOLES TO BE CLEANED, GROUTED, AND ADJUSTED TO GRADE WITH NEW ADJUSTING RINGS. (ALL TOP BANDS OF FALLEN, COMMON OR CONCRETE BRICK TO BE REPLACED WITH PRECAST CONCRETE ADJUSTING RINGS)
 - "S" DENOTES EXISTING STRUCTURES TO BE ADJUSTED (SPECIAL)
 - "R" DENOTES EXISTING STRUCTURES TO BE RECONSTRUCTED
 - "FR" DENOTES FRAME AND LID TO BE REPLACED
 - (Hatched pattern) DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, 3" (FULL WIDTH), TO BE REPLACED WITH "LEVELING BINDER (MACHINE METHOD), N70, 1 INCH, "AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A" AND "HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 2 INCH"
 - (Diagonal line pattern) DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, 3" (FULL WIDTH), TO BE REPLACED WITH "LEVELING BINDER (MACHINE METHOD), N70, 1 INCH, AND "HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 2 INCH"
 - (Dotted pattern) DENOTES "DRIVEWAY PAVEMENT REMOVAL" AND "SIDEWALK REMOVAL" (AS DIRECTED BY THE ENGINEER)
 - (Solid black) DENOTES P.C. CONCRETE SIDEWALK, 5 INCH, WITH DETECTABLE WARNINGS
 - (Stippled) DENOTES "P.C. CONCRETE DRIVEWAY PAVEMENT, 8 INCH" AND "P.C. CONCRETE SIDEWALK, 5 INCH" ON SUBBASE GRANULAR MATERIAL, TYPE B, 2 INCH"
 - (Dashed lines) DENOTES "COMBINATION CURB AND GUTTER REMOVAL"
 - (Double lines) DENOTES "COMBINATION CURB AND GUTTER, TYPE B-V.12"
 - 100.50 PROPOSED TOP OF CURB ELEVATION
 - 100.00 PROPOSED GUTTER FLOW LINE ELEVATION
 - 25+50 STATION

610																									610
605																									605
600																									600
595	EXISTING @ ELEVATION PROPOSED @ ELEVATION																								595
		52	53+16	54	55	+55	56	57	58	59	60	+75	61	+25											
			605.82 605.82	606.00 606.00	604.96 604.96	603.55 603.55	601.66 601.66	600.32 600.32	599.87 599.87	599.75 599.75	599.70 599.70	599.72 599.72	599.71 599.71	600.34 600.34	600.72 600.72										

- NOTE:**
- 1 ALL EXISTING FRAMES AND LIDS THAT ARE TO BE REPLACED (AS DIRECTED BY THE ENGINEER) SHALL BE SALVAGED TO THE CONTRACTOR.
 - 2 ALL SAWCUTS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE REMOVAL PAY ITEMS

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 4

IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

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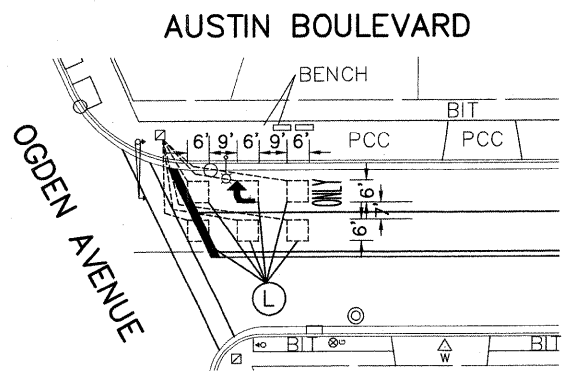
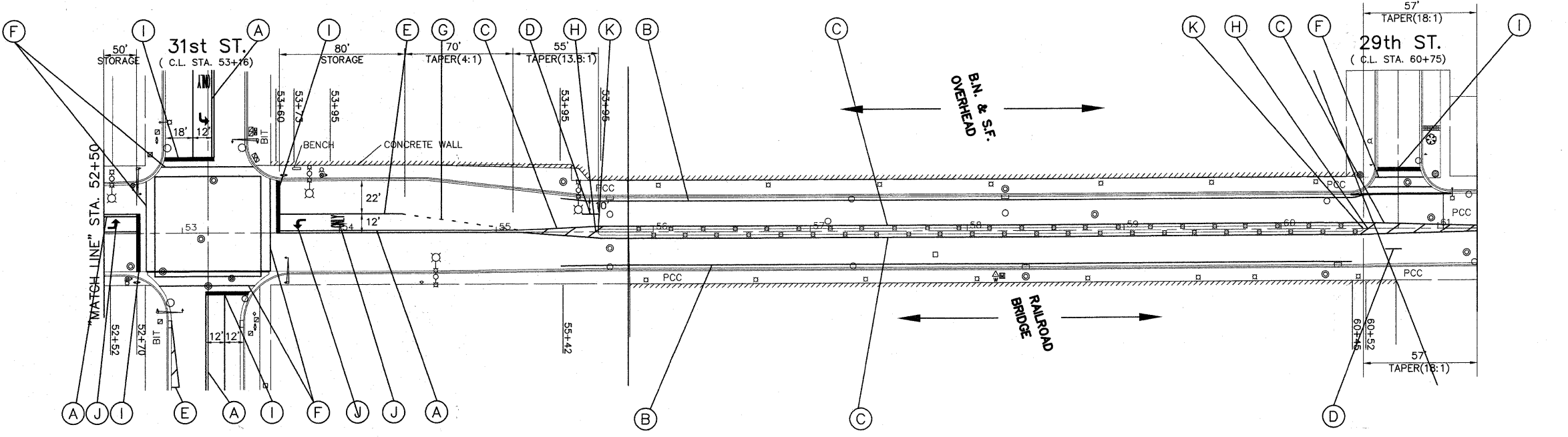
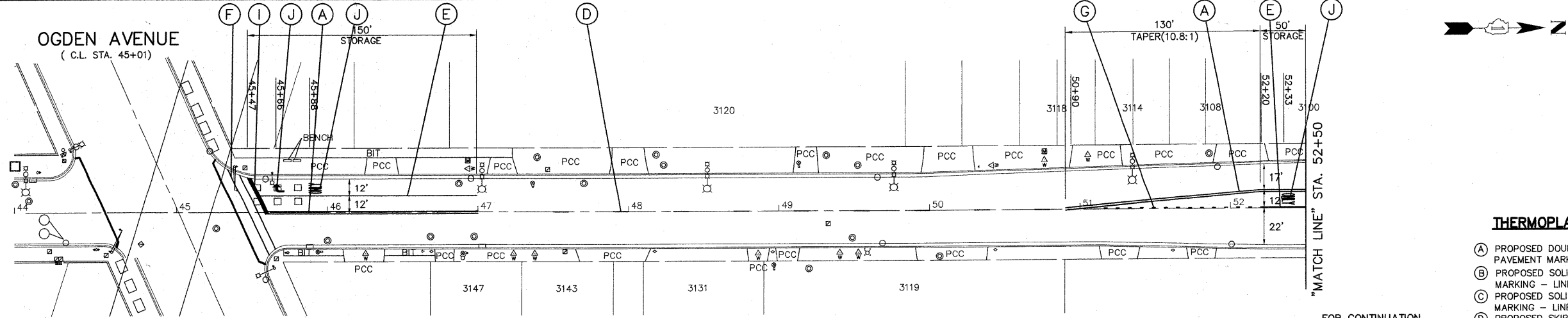
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PLAN: AUSTIN BOULEVARD
STA. 52+50 TO STA. 61+25 (RESURFACING)**

SCALE: 1"=40' SHEET NO. OF SHEETS STA. 52+50 TO STA. 61+25

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2790	10-00212-00-RS	COOK	16	6
CONTRACT NO. 63464				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT ARA-9003(645)				



DETECTOR LOOP REPLACEMENT AT OGDEN AVENUE (NORTH)

THERMOPLASTIC STRIPING CODE

- (A) PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - CENTER LINE 4", 11" O/C
- (B) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 4"
- (C) PROPOSED SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 4"
- (D) PROPOSED SKIP-DASH YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 4" - 10' DASH, 30' SKIP (LANE LINE)
- (E) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (LANE LINE)
- (F) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (CROSSWALK LINE)
- (G) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" - 2' DASH, 6' SKIP (LANE LINE)
- (H) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 12"
- (I) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 24" (STOP BAR)
- (J) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS
- (K) PAINT PAVEMENT MARKING

DETECTOR LOOPS

- (L) PROPOSED DETECTOR LOOP REPLACEMENT

NOTE: ALL "ARROW" AND "ONLY" MARKINGS SHALL BE 8" IN HEIGHT.

NOTE: PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - CENTER LINE 4", 11" O/C (NOTE: (A)) IS MEASURED PER LINE. TOTAL QUANTITY AND PAYMENT LENGTH IS FOR EACH LINEAR FOOT OF SINGLE 4" STRIPE INSTALLED.

NOTE: SHORT-TERM PAVEMENT MARKING IS PROPOSED ON THE MILLED PAVEMENT, NEW BINDER AND ON THE NEW SURFACE. SEE ARTICLE 703.04 OF THE LATEST EDITION OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

NOTE: PROPOSED STRIPING IS BEING REPLACED IN THE SAME LOCATION AS EXISTING STRIPING.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET X

IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

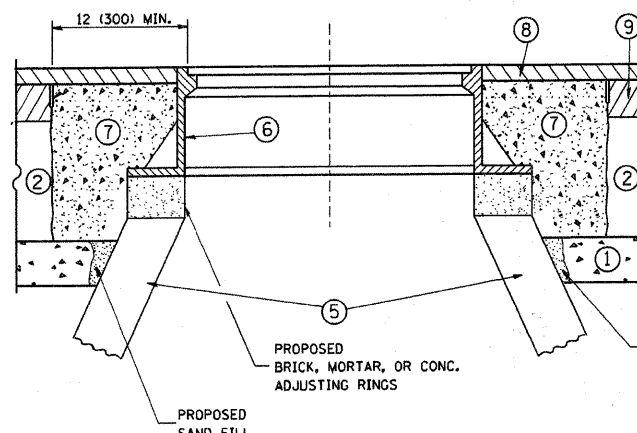
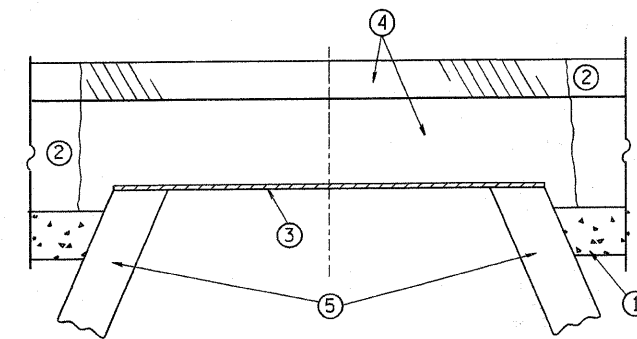
Frank Novotny & Associates, Inc.
225 Midway Drive • Willmetts, IL • 60127 • Telephone: (630) 837-8660 • Fax: (630) 837-0132
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 194-00028

FILE NAME =	USER NAME =	DESIGNED - AMS	REVISED - AMS 3/09/10
		DRAWN - TRB	REVISED -
		CHECKED - TPG	REVISED -
		DATE - 1/28/10	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN: AUSTIN BOULEVARD -
STA. 45+40 TO STA. 61+25 (STRIPING)
SCALE: 1" = 40' SHEET NO. OF SHEETS STA. 45+40 TO STA. 61+25

F.A. -	SECTION	COUNTY	TOTAL SHEET
RTE. -	10-00212-00-RS	COOK	SHEETS NO.
2790		16	7
CONTRACT NO. 63464			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT ARA-9003(645)	



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL". NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

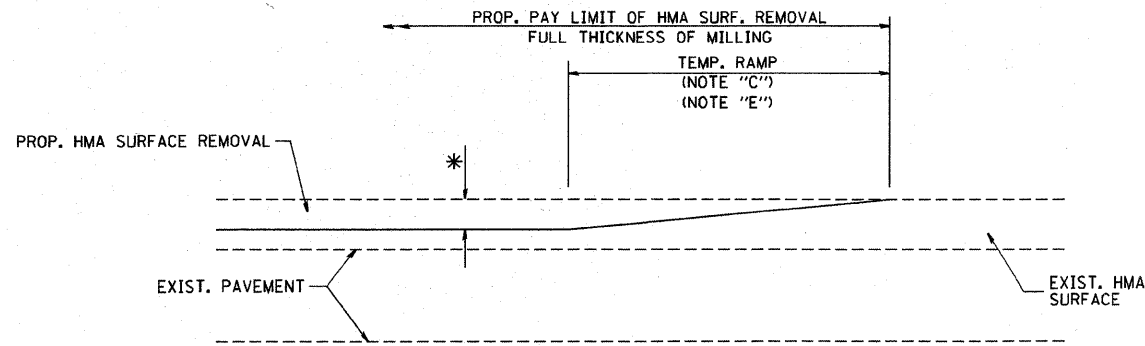
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

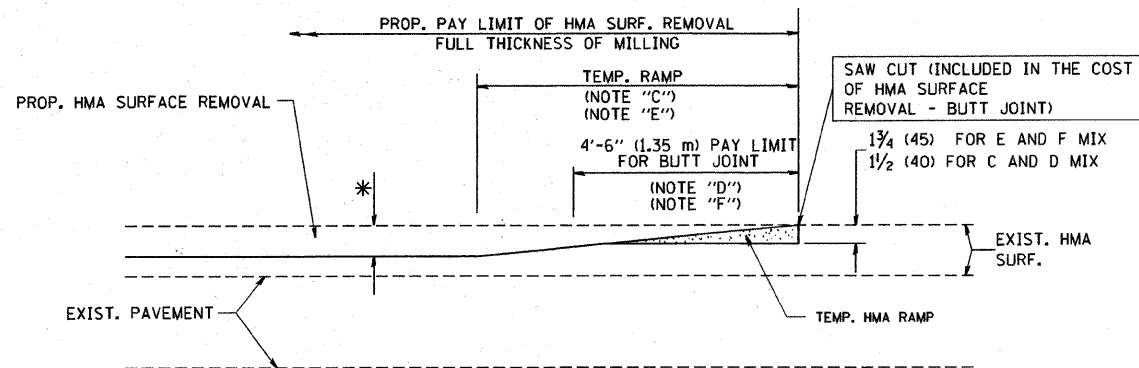
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\dststd\22x34\bd08.dgn	USER NAME = gaglianobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING		F.A. RTE. = 2790	SECTION = 10-00212-00-RS	COUNTY = COOK	TOTAL SHEETS = 16	SHEET NO. = 8
	PLOT SCALE = 50.0000 / IN.	CHECKED -	REVISED - A. ABBAS 03-21-97		REVISED - R. WIEDEMAN 05-14-04	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	BD600-03 (BD-8)		CONTRACT NO. 63464
PLOT DATE = 1/4/2008	DATE = 10-25-94	REVISED - R. BORO 01-01-07					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(645)				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

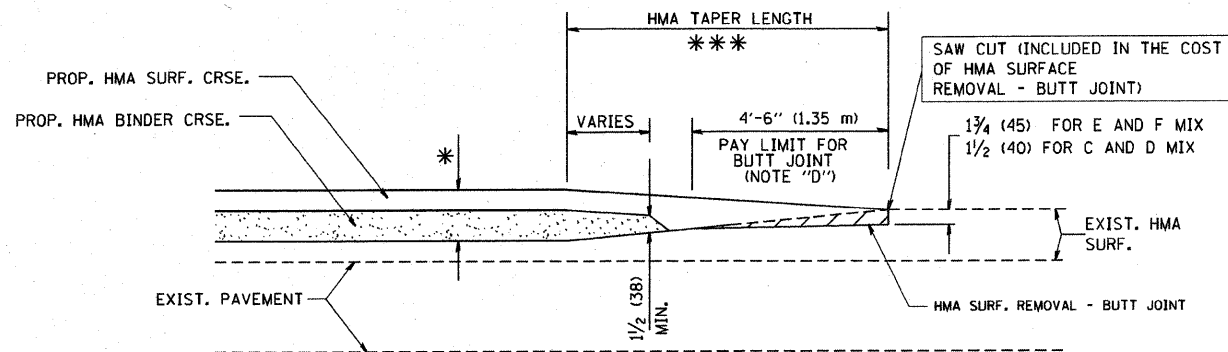
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

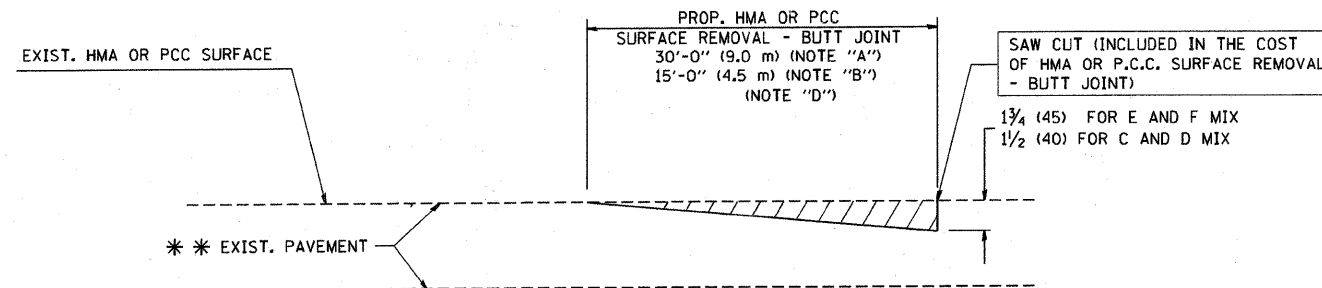
OPTION 2

TYPICAL TEMPORARY RAMP

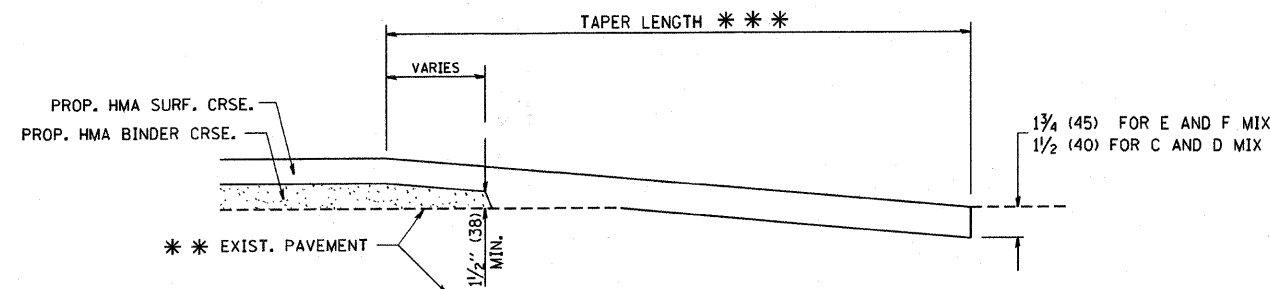


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

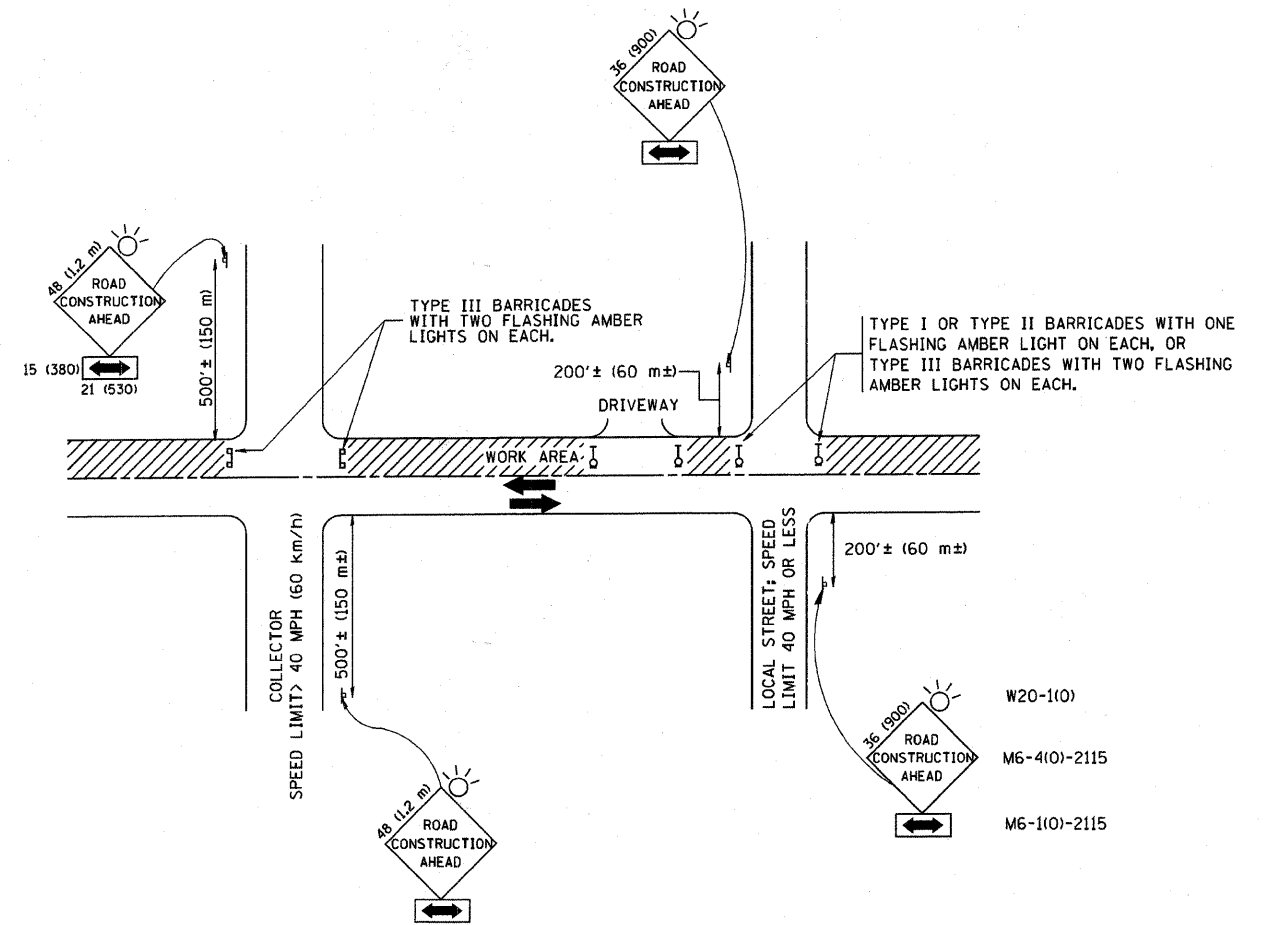
THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE		2790	10-00212-00-RS	COOK	16	9
SHEET NO. 1 OF 1 SHEETS		BD400-05 BD32		CONTRACT NO. 63464		
STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(645)				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
 - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
 - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

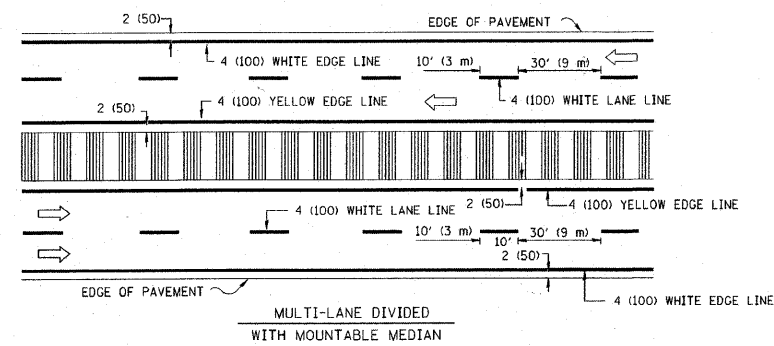
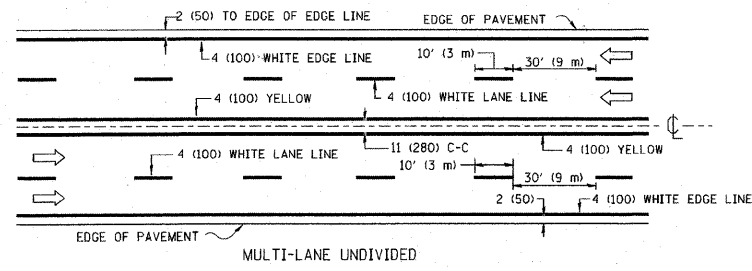
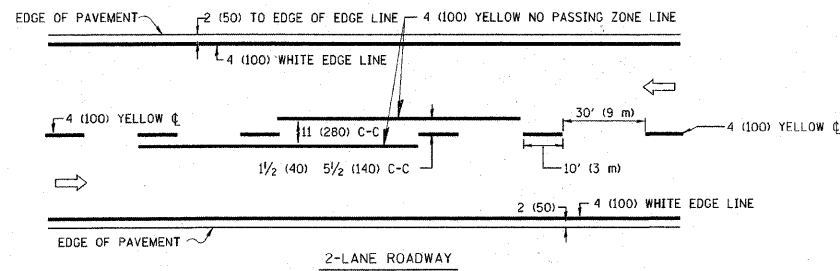
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		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

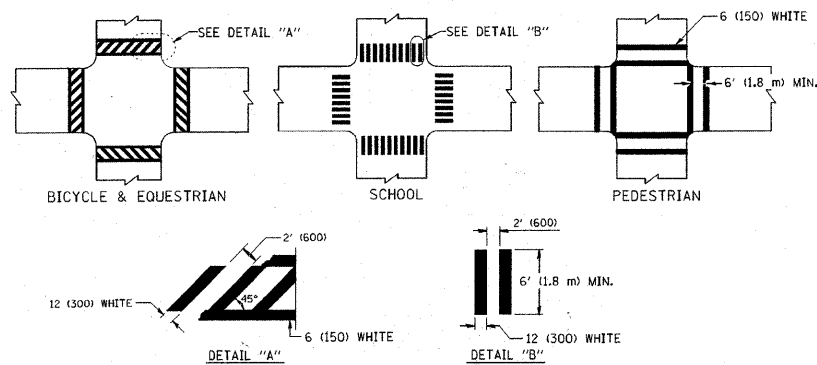
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 63464	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(645)				

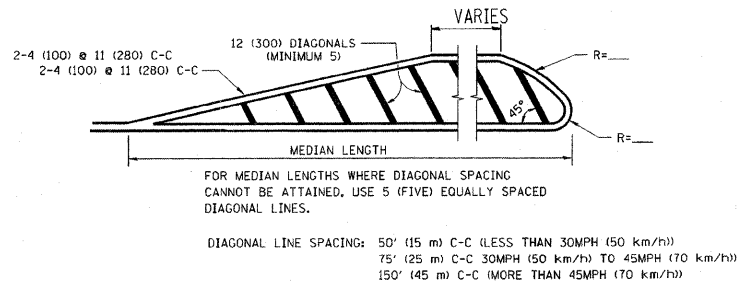
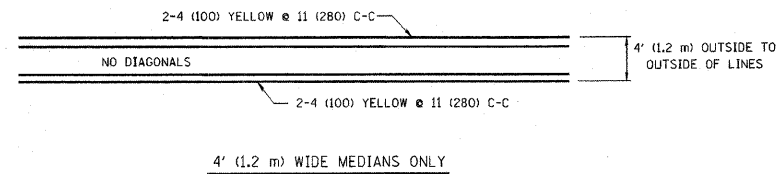


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

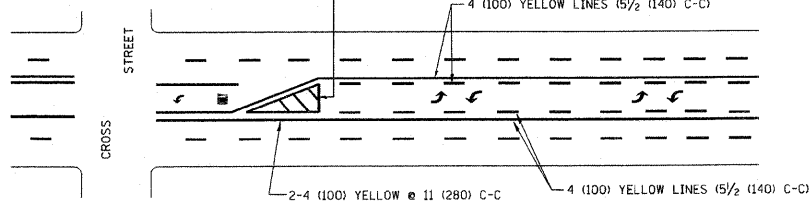
TYPICAL LANE AND EDGE LINE MARKING



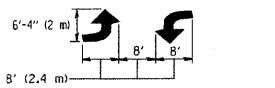
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE

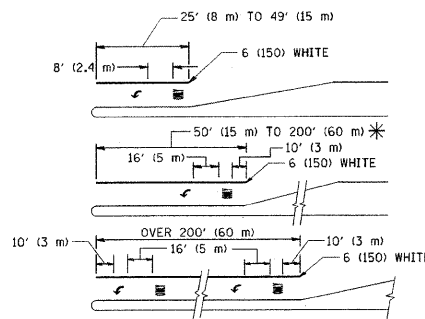


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

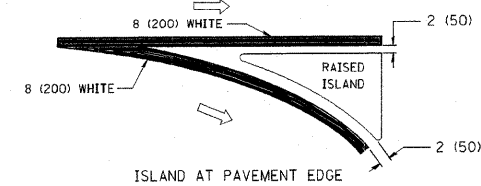
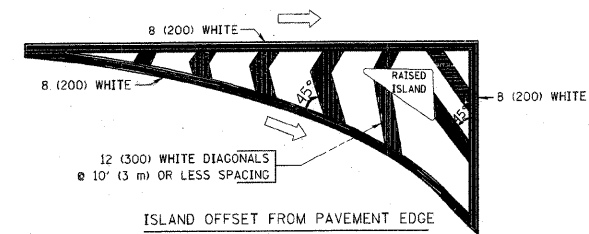
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
* AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

	LARGE SIZE	SMALL SIZE
THROUGH ARROW	1.07 (11.5)	0.60 (6.5)
LEFT OR RIGHT ARROW	1.47 (15.6)	0.60 (6.5)
COMBINATION LEFT (RIGHT) AND THROUGH ARROW	2.42 (26.0)	1.37 (14.7)
RAILROAD "R" 1.8m (6ft.)	0.33 (3.6)	—
RAILROAD "X" 6.1m (20ft.)	5.02(54.0)	—
HANDICAPPED SYMBOL	0.43 (4.6)	—

All dimensions are in inches (millimeters) unless otherwise shown.

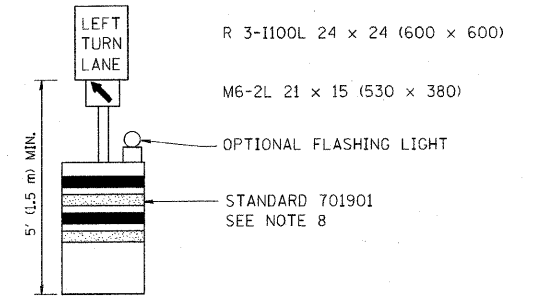
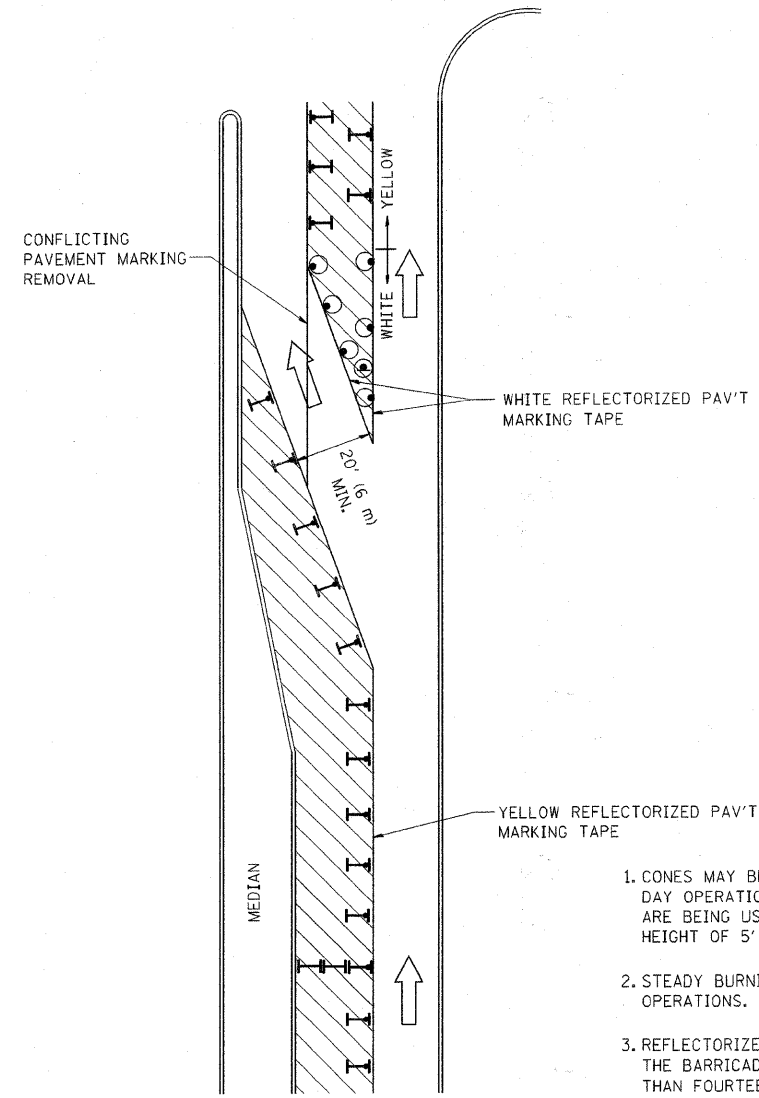
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2790	10-00212-00-RS	COOK	16	11
TC-13		CONTRACT NO. 63464		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT ARA-9003(645)				

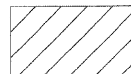







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

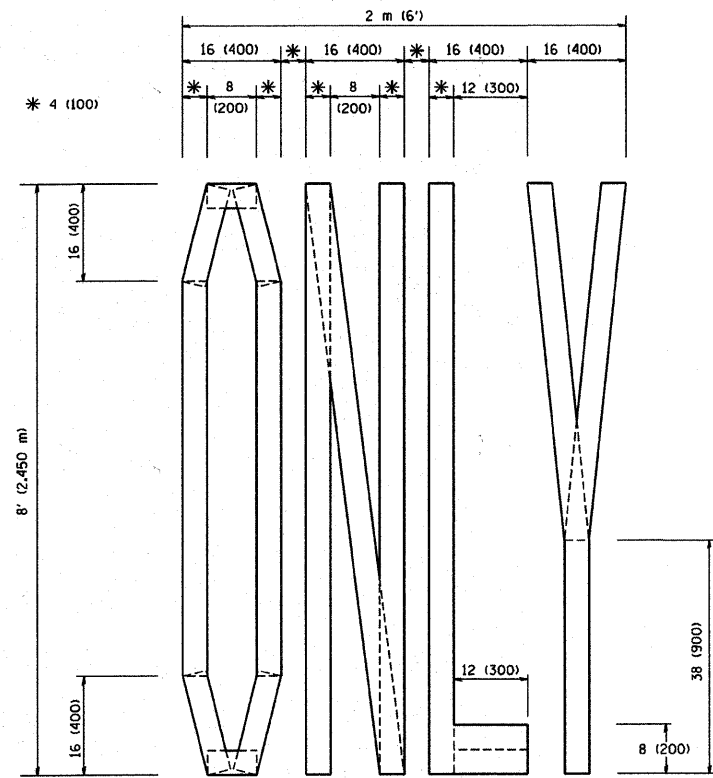
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

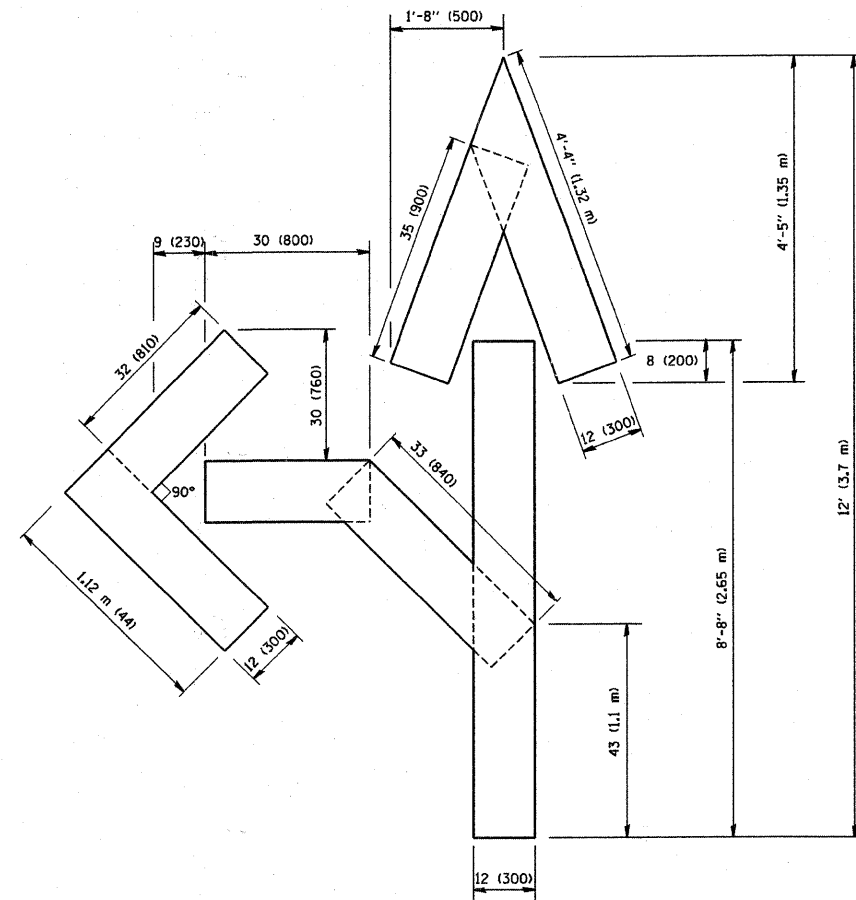
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

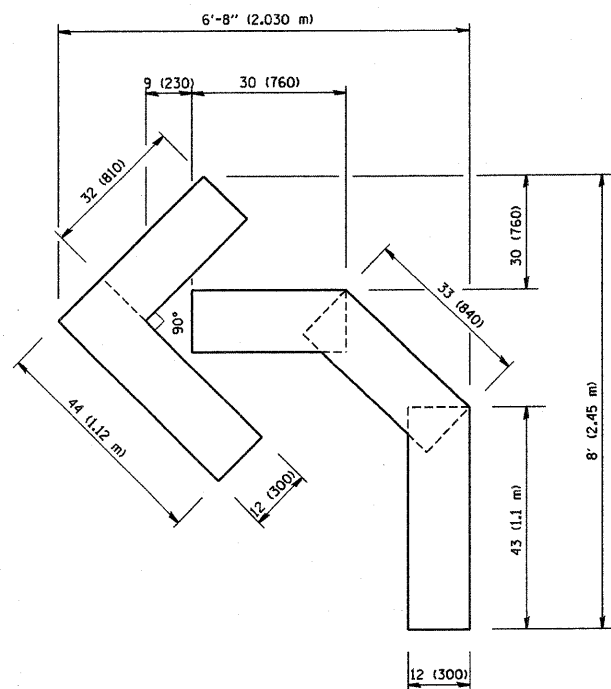
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2790	10-00212-00-RS	COOK	16	12
TC-14		CONTRACT NO. 63464		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(645)				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in Inches (millimeters) unless otherwise shown.

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		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000 / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

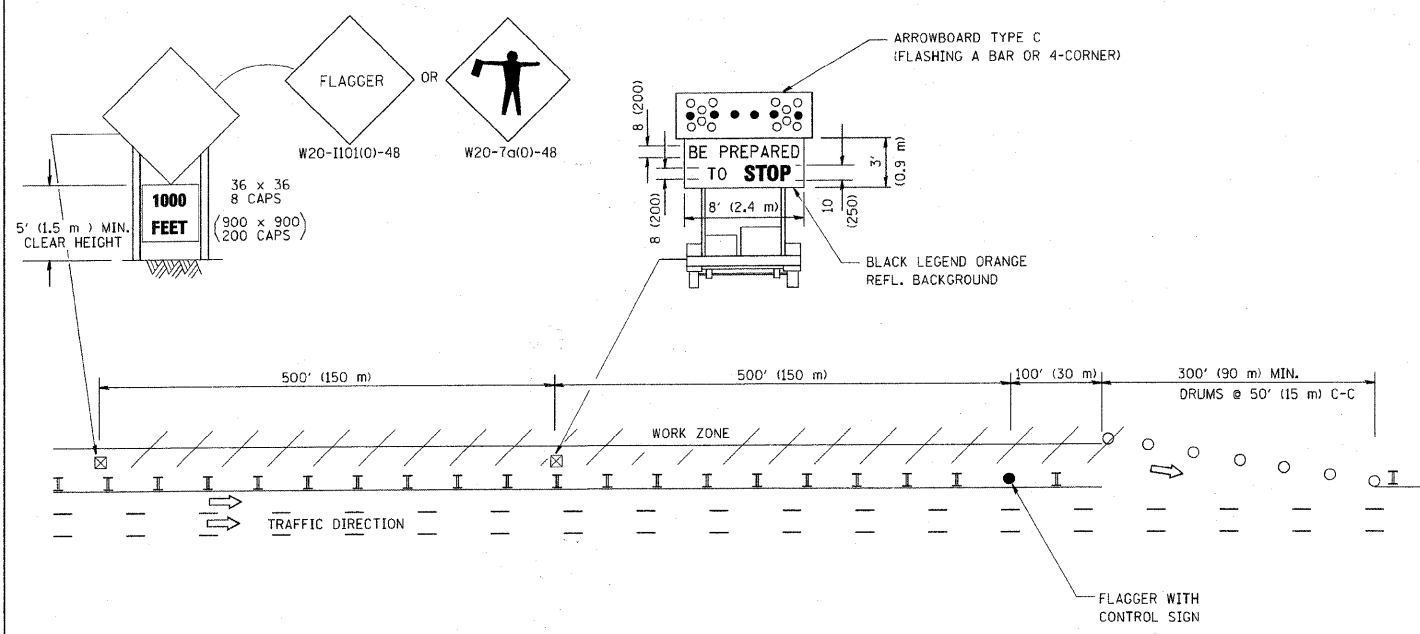
**PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

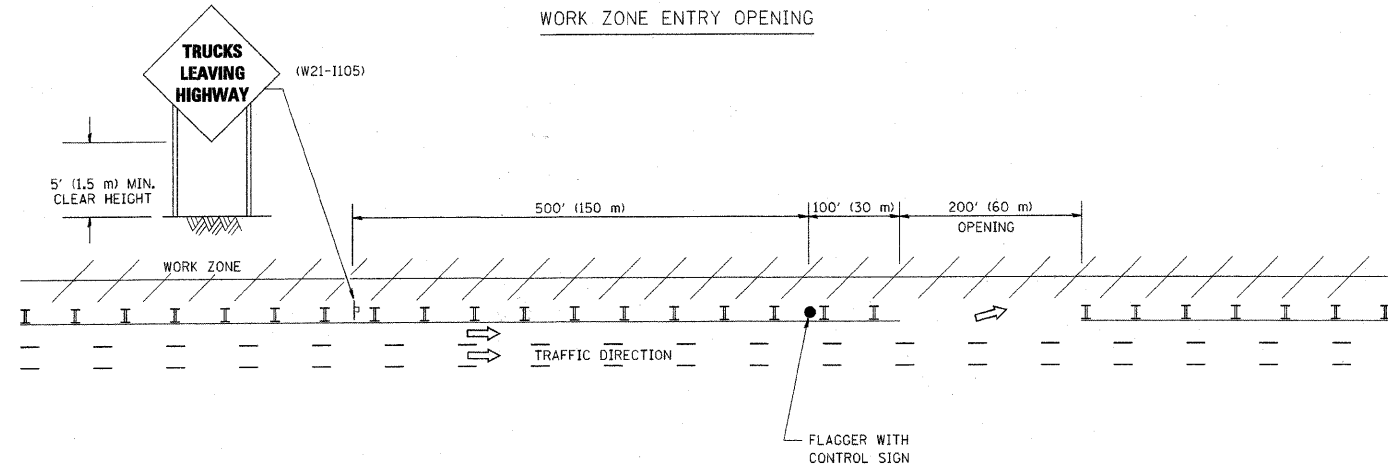
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2790	10-00212-00-RS	COOK	16	13
TC-16			CONTRACT NO. 63464	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(645)				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



- NOTES:
1. THE ARROWBOARD, THE FLAGGER AHEAD SIGN AND THE TRUCKS LEAVING HIGHWAY SIGN SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
 2. WORK ZONE EXIT OPENINGS SHOULD BE A MINIMUM OF ONE HALF MILE APART.
 3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
 4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS

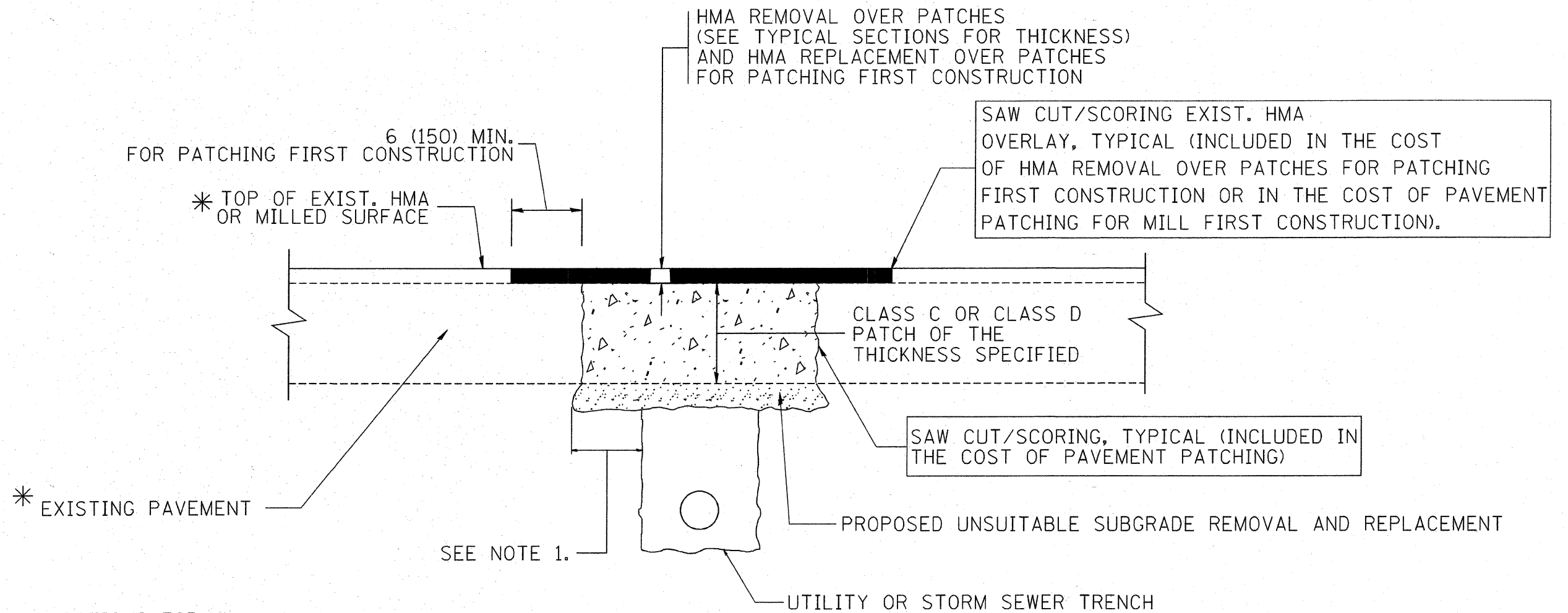
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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		DRAWN -	REVISED - J.A.F. 02-06
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - S.P.B. 01-07
	PLOT DATE = 1/26/2010	DATE -	REVISED - S.P.B. 12-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2790	10-00212-00-RS	COOK	16	14
TC-18		CONTRACT NO. 63464		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT ARA-9003(645)				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

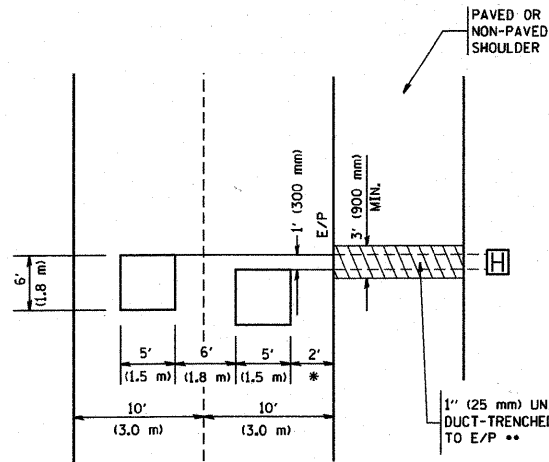
**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 2790	SECTION 10-00212-00-RS	COUNTY COOK	TOTAL SHEETS 16	SHEET NO. 15
BD400-04 (BD-22)			CONTRACT NO. 63464	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT ARA-9003(645)				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

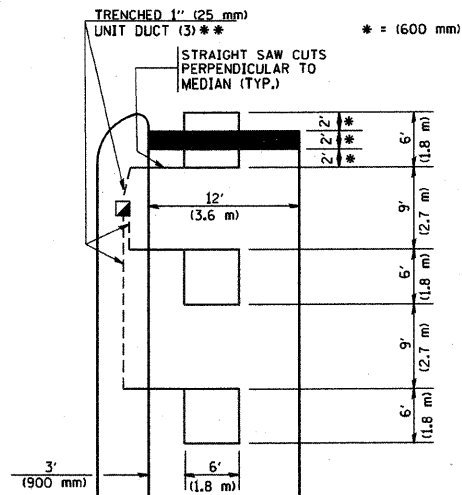


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



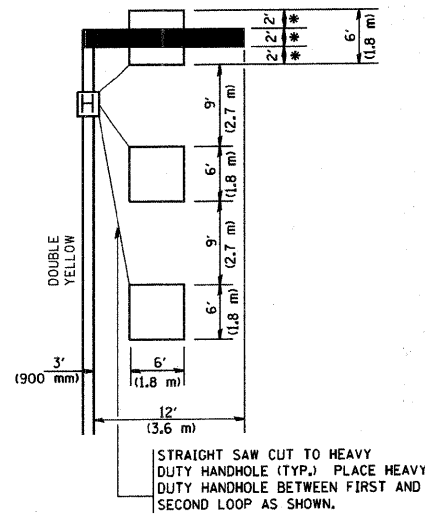
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

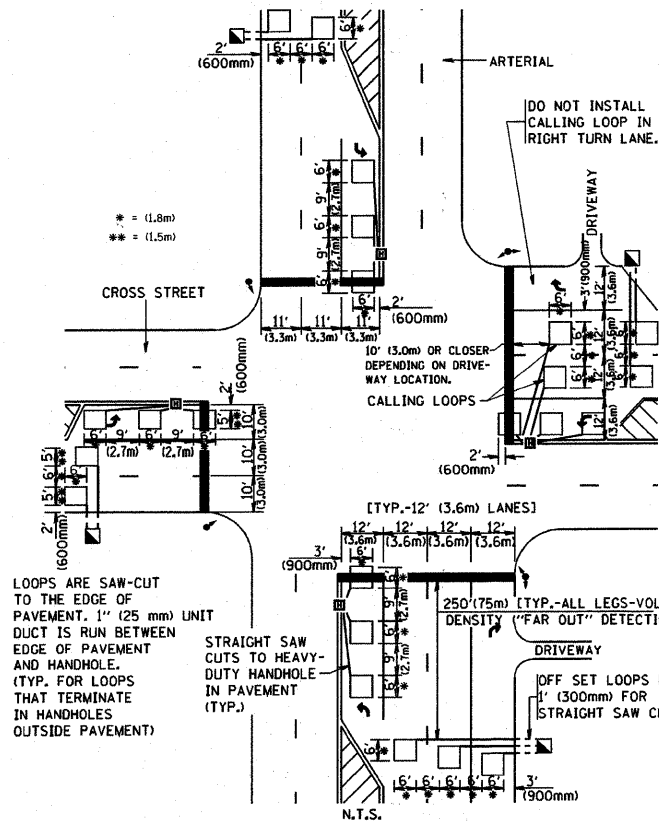
**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

* = (600 mm)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**

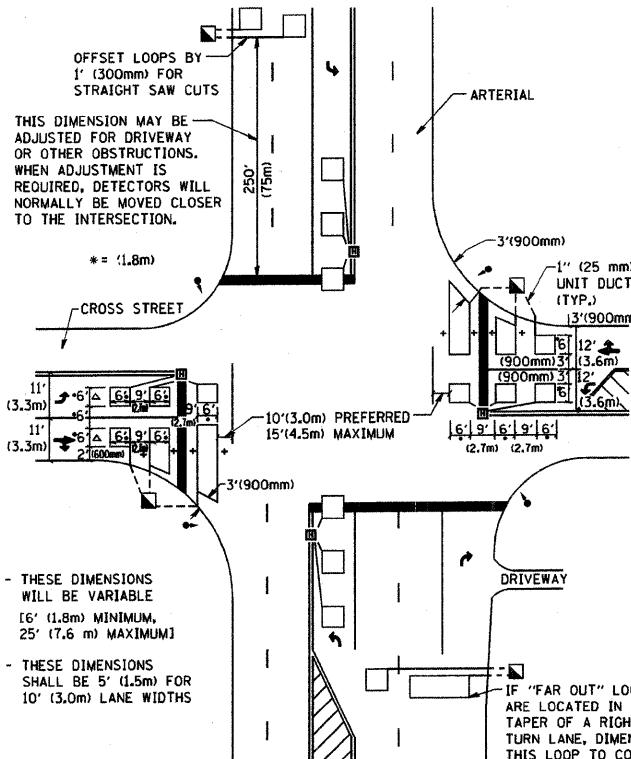


LOOPS ARE SAW-CUT
TO THE EDGE OF
PAVEMENT. 1" (25 mm) UNIT
DUCT IS RUN BETWEEN
EDGE OF PAVEMENT
AND HANDHOLE.
(TYP. FOR LOOPS
THAT TERMINATE
IN HANDHOLES
OUTSIDE PAVEMENT)

STRAIGHT SAW
CUTS TO HEAVY-
DUTY HANDHOLE
IN PAVEMENT
(TYP.)

DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



+- THESE DIMENSIONS
WILL BE VARIABLE
[6' (1.8m) MINIMUM,
25' (7.6 m) MAXIMUM]

▲ - THESE DIMENSIONS
SHALL BE 5' (1.5m) FOR
10' (3.0m) LANE WIDTHS

DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = w:\dststd\22x34\ts07.dgn	USER NAME = gaglianob	DESIGNED -	REVISIONS -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING		F.A. RTE. 2790	SECTION 10-00212-00-RS	COUNTY COOK	TOTAL SHEETS 16	SHEET NO. 16
PLOT SCALE = 50.0000' / IN.	CHECKED - R.K.F.	DATE -	REVISIONS -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TS-07		CONTRACT NO. 63464	
PLOT DATE = 1/4/2008	DATE -	REVISIONS -	REVISIONS -				FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT ARA-9003(645)				