

**INDEX OF SHEETS**

- 1. COVER SHEET
- 2. GENERAL NOTES
- 3. SUMMARY OF QUANTITIES
- 4. TYPICAL SECTIONS
- 5 - 6. SCHEDULES - NBL
- 7 - 9. SCHEDULES - SBL
- 10. TRAFFIC CONTROL AND PROTECTION, STANDARD 701400 (SPECIAL)
- 11. TRAFFIC CONTROL AND PROTECTION, STANDARD 701401 (SPECIAL)
- 12 - 13. PATCHING DETAILS
- 14. PAVEMENT MARKING
- 15 - 16. EXISTING APPROACH PAVEMENT FOR INFORMATION ONLY

**STANDARDS**

- 000001-05 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 001001-02 AREAS OF REINFORCEMENT BARS
- 001006 DECIMAL OF AN INCH AND OF A FOOT
- 420001-07 PAVEMENT JOINTS
- 420101-04 24' (7.2 m) JOINTED PCC PAVEMENT
- 420111-02 PCC PAVEMENT ROUNDOUTS
- 442001-04 CLASS A PATCHES
- 701101-02 OFF-ROAD OPERATIONS MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
- 701106-02 OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY
- 701411-06 LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP FOR SPEEDS  $\Delta$  45 MPH
- 701426-03 LANE CLOSURE, MULTILANE INTERMITTENT OR MOVING OPERATION, FOR SPEEDS  $\Delta$  45 MPH
- 701901-01 TRAFFIC CONTROL DEVICES

MICROFILMED \_\_\_\_\_  
 REEL NUMBER \_\_\_\_\_  
 AWARDED \_\_\_\_\_  
 RESIDENT ENGINEER \_\_\_\_\_  
 AS BUILT CHANGES WERE MADE  
 ON THE FOLLOWING SHEETS \_\_\_\_\_

J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123

**DISTRICT 3 NO. (815) 434-6131**

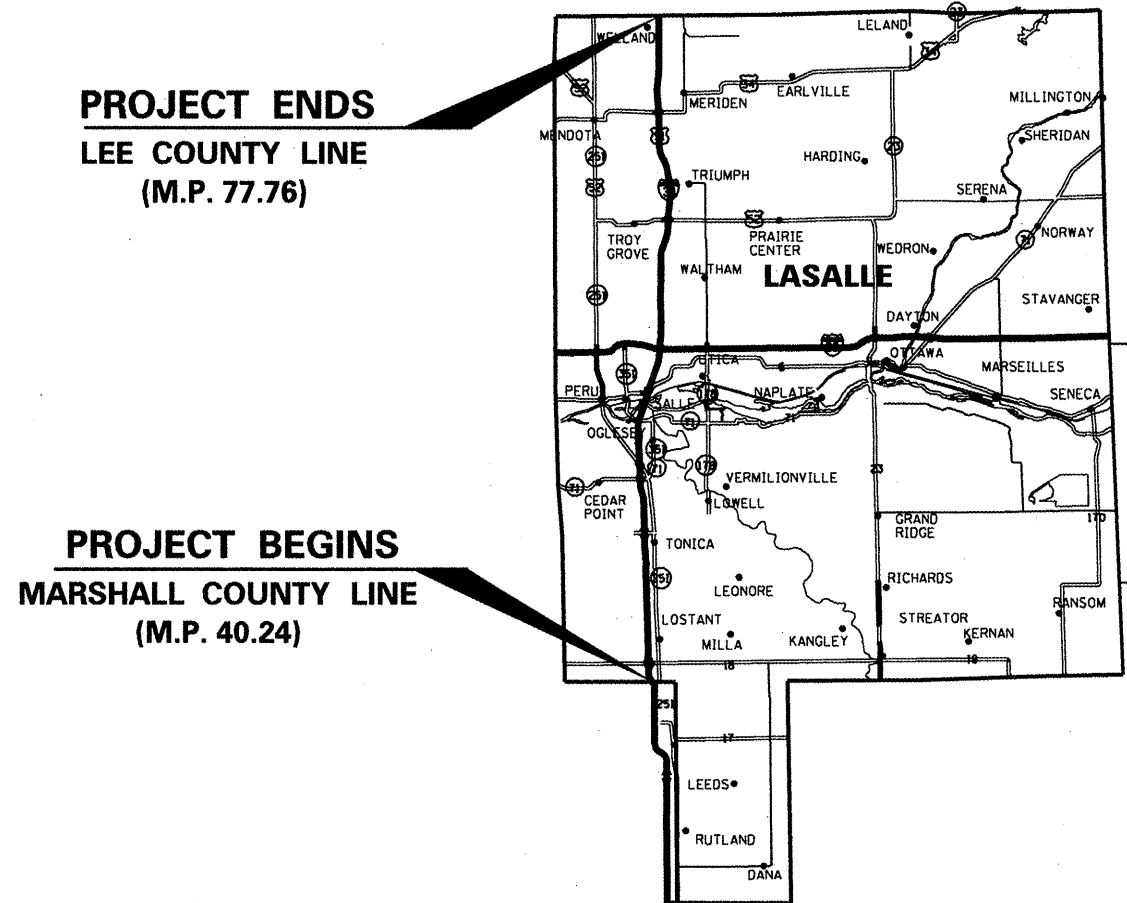
**PROJECT ENGINEER: JOE KANNEL**  
**UNIT CHIEF: RON WOODSHANK**  
**TOWNSHIP: VARIOUS**

**CONTRACT NO. 66A31**

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**  
**PROPOSED**  
**HIGHWAY PLANS**

**FAI ROUTE 39 (I-39)**  
**SECTION D3 PATCHING 2011-2**

**LASALLE COUNTY**  
**C-93-116-10**  
**PAVEMENT PATCHING**

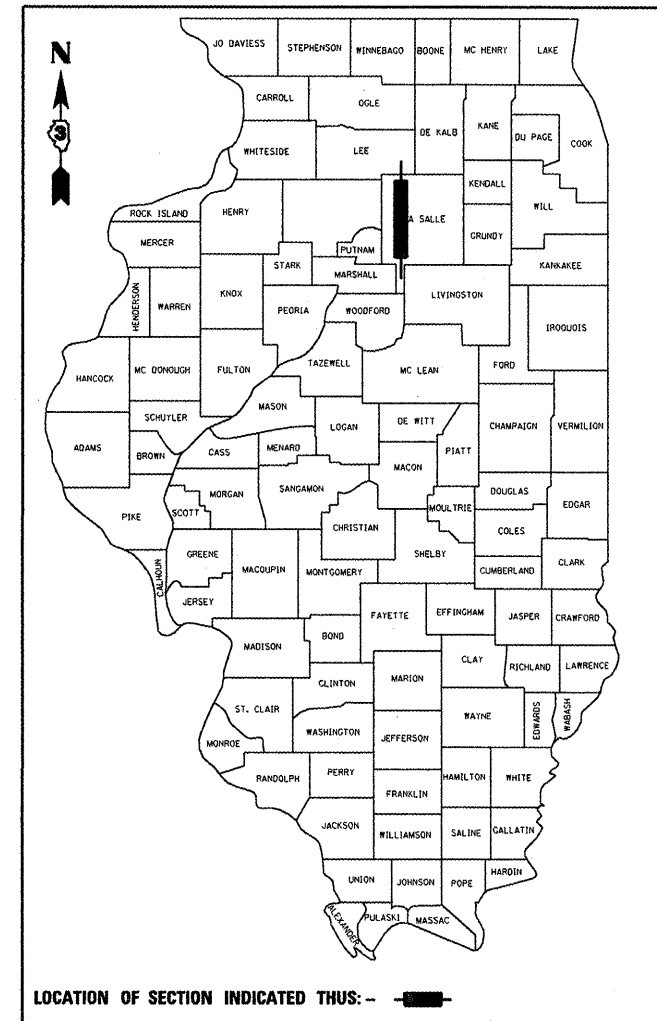


**LOCATION MAP**

NOT TO SCALE  
 GROSS LENGTH = 198105 FT. = 37.52 MI.  
 NET LENGTH = 1644 FT. = 0.12 MI

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	D3 PATCHING 2011-2	LASALLE	16	1
		ILLINOIS	CONTRACT NO. 66A31	

D-93-002-10



LOCATION OF SECTION INDICATED THUS: - - -

**FUNCTIONAL CLASSIFICATION**  
 RURAL - INTERSTATE  
 F.A.I. ROUTE 39 (I-39)  
 2007 ADT = 20444  
 P.V. = 59.2% M.U. = 36.2% S.U. = 4.6%

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**

SUBMITTED 4/11 2010  
*Morgan F. Ryan*  
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER  
May 7 2010  
*Scott E. Stitt, P.E.*  
 ACTING ENGINEER OF DESIGN AND ENVIRONMENT  
May 7 2010  
*Christine M. Roedel*  
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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**OF THE STATE OF ILLINOIS**

**GENERAL NOTES**

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING ROADWAY OR STRUCTURE HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD AND TO MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT BID FOR THE WORK.

EXISTING REINFORCEMENT BARS EXTENDING INTO REMOVAL AREA SHALL BE CLEANED, STRAIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL SHALL BE REPLACED WITH AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM. COST INCLUDED WITH CONCRETE REMOVAL.

FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. THE "JULIE" NUMBER IS 1-800-892-0123. A MINIMUM OF FORTY-EIGHT (48) HOURS ADVANCE NOTICE IS REQUIRED.

ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT OF WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF CLASS A PATCHING.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER LISTED ON THE INDEX OF SHEETS OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.

THE EXISTING SHOULDERS ARE LOW. IN THE EVENT THAT CONSTRUCTION OPERATIONS RESULT IN A DROP OFF GREATER THAN 3" BETWEEN THE EDGE OF PAVEMENT OR EDGE OF SHOULDER WITHIN 3' OF THE EDGE OF PAVEMENT AND THE EARTH OR AGGREGATE SHOULDERS, THEN THE DROP OFF SHALL BE PROTECTED ACCORDING TO ARTICLE 701.04(B)(1) OF THE STANDARD SPECIFICATIONS. THIS WORK WILL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICES FOR TRAFFIC CONTROL.

DATE: 4/1/10

PREPARED BY: Don Bernil  
ACTING DISTRICT STUDIES & PLANS ENGINEER

EXAMINED BY: Herbert K. Jurgis  
DISTRICT CONSTRUCTION ENGINEER

Wayne D. Phillips  
DISTRICT MATERIALS ENGINEER

Bruce A. Huchler  
DISTRICT OPERATIONS ENGINEER

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL NOTES</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pwwork\WOODSHANKR\08143168	D366A31-details.dgn	DRAWN - RON WOODSHANK	REVISED -			39	D3 PATCHING 2011-2	LASALLE	16	2	
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 66A31					
	PLOT DATE = Mar 31, 2010 - 09:13:32 AM	DATE -	REVISED -			ILLINOIS					

## SUMMARY OF QUANTITIES

CONSTRUCTION CODE TYPE: J000

RURAL  
100% STATE  
TOTAL  
QUANTITY

CODE NO.	ITEM	UNIT	TOTAL QUANTITY
44200553	CLASS A PATCHES, TYPE II, 10 INCH	SQ YD	693
44200557	CLASS A PATCHES, TYPE III, 10 INCH	SQ YD	50
44200559	CLASS A PATCHES, TYPE IV, 10 INCH	SQ YD	166
44213000	PATCHING REINFORCEMENT	SQ YD	909
44213200	SAW CUTS	FOOT	6050
67100100	MOBILIZATION	L SUM	1
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	16
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	25
* 78005110	EPOXY PAVEMENT MARKING - LINE 4"	FOOT	623
* 78005130	EPOXY PAVEMENT MARKING - LINE 6"	FOOT	172
X7010805	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401 (SPECIAL)	L SUM	1
Z0075310	TIE BARS 3/4"	EACH	52

\* SPECIALITY ITEM

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED - -----
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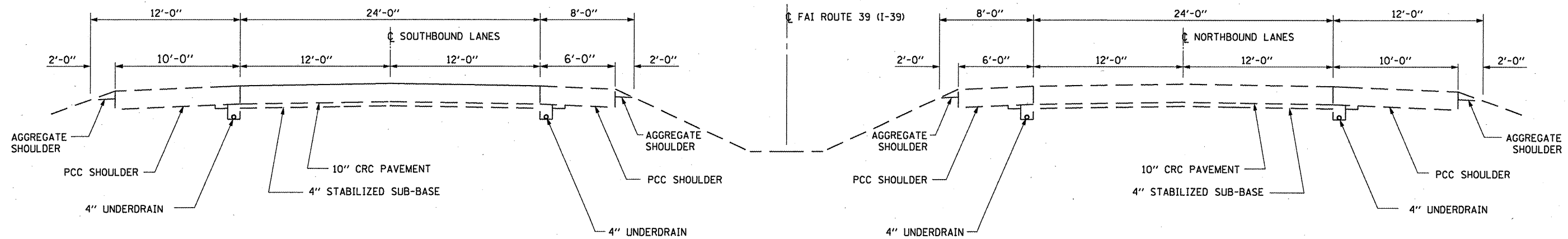
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

### SUMMARY OF QUANTITIES

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_ OF \_\_\_\_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

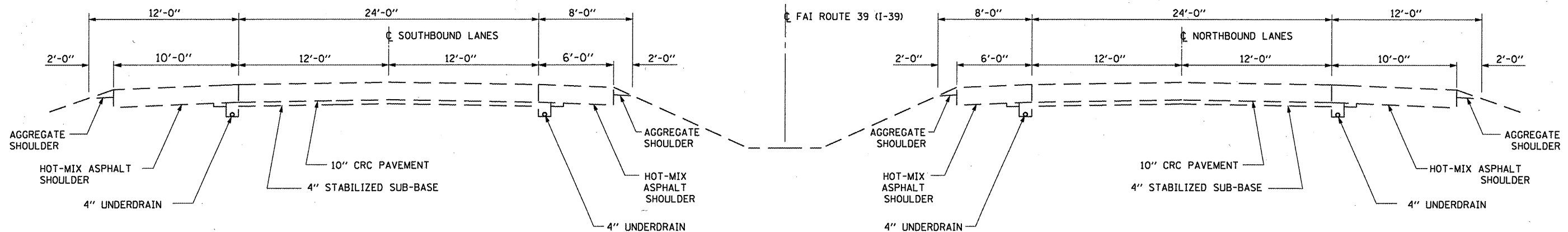
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	D3 PATCHING 2011-2	LASALLE	16	3
			CONTRACT NO. 66A31	

ILLINOIS



**TYPICAL SECTIONS**

M.P. 41.24 TO M.P. 55.36



**TYPICAL SECTIONS**

M.P. 55.36 TO M.P. 77.76

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SECTIONS</b>	SCALE: _____	SHEET NO. _____ OF _____ SHEETS	STA. _____ TO STA. _____	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\FWIDOT\WOODSHANKR1\0143168	D366A31-details.dgn	DRAWN - RON WOODSHANK	REVISED -			39	D3 PATCHING 2011-2	LASALLE	16	4		
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -			ILLINOIS				CONTRACT NO. 66A31		
	PLOT DATE = Mar 31, 2010 - 08:19:20 AM	DATE -	REVISED -									

PATCHING SCHEDULE - NBL

LOCATION BY LOG MILE	PASSING LANE					DRIVING LANE					REMARKS
	CLASS A PATCHING TYPE II SQ YD	TYPE IV SQ YD	PATCH REINF. SQ YD	SAW CUTS FOOT	TIE BARS EACH	CLASS A PATCHING TYPE II SQ YD	TYPE IV SQ YD	PATCH REINF. SQ YD	SAW CUTS FOOT	TIE BARS EACH	
40.24							47	47	153	17	MARSHALL COUNTY LINE
40.60						8		8	66		
40.60						11		11	72		
42.28											IL RTE. 18
47.90	8		8	66							
48.50						11		11	72		
49.34											RAY RICHARDSON ROAD
49.50	8		8	66							
50.70						8		8	66		
52.37											IL RTE. 71
53.13											IL RTE. 251
54.10	8		8	60							
54.46											OGLESBY SPUR
56.20						8		8	60		
56.30							67	67	198	25	
57.42											US RTE. 6
58.30						8		8	66		
59.70											FAI RTE. 80
61.00						8		8	60		
61.10	8		8	60							
62.20						8		8	60		
66.60						8		8	60		
66.81											US RTE. 52
68.40						8		8	60		
71.80							27	27	88	10	
72.50						8		8	60		
72.74											US RTE. 34
73.00						8		8	60		
73.10						8		8	60		
73.15						13		13	68		
74.30						13		13	68		
74.60						8		8	60		
74.90						8		8	60		
75.60	8		8	60							
75.90						8		8	60		
76.00						8		8	60		
76.50						8		8	60		
77.76											LEE COUNTY LINE
TOTALS	40		40	312		176	141	317	1697	52	

PAVEMENT MARKING SCHEDULE - NBL

LOCATION  MILE POST	EPOXY PAVEMENT MARKING TYPE 1, LINE 4"		EPOXY PAVEMENT MARKING TYPE 1, LINE 6"	REMARKS
	YELLOW	WHITE	WHITE	
40.24		35	9	MARSHALL COUNTY LINE
40.60		6	2	
40.60		8	2	
42.28				IL RTE. 18
47.90	6		2	
48.50		8	2	
49.34				RAY RICHARDSON ROAD
49.50	6		2	
50.70		6	2	
52.37				IL RTE. 71
53.13				IL RTE. 251
54.10	6		2	
54.46				OGLESBY SPUR
56.20		6	2	
56.30		50	13	
57.42				US RTE. 6
58.30		6	2	
59.70				FAI RTE. 80
61.00		6	2	
61.10	6		2	
62.20		6	2	
66.60		6	2	
66.81				US RTE. 52
68.40		6	2	
71.80		20	5	
72.50		6	2	
72.74				US RTE. 34
73.00		6	2	
73.10		6	2	
73.15		10	3	
74.30		10	3	
74.60		6	2	
74.90		6	2	
75.60	6		2	
75.90		6	2	
76.00		6	2	
76.50		6	2	
77.76				LEE COUNTY LINE
NB SUBTOTALS	30	237		
TOTALS	267		79	

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -
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	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = Mar 31, 2010 - 08:19:32 AM	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCHEDULES - NBL

SCALE: SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	D3 PATCHING 2011-2	LASALLE	16	6
CONTRACT NO. 66A31			ILLINOIS	

PATCHING SCHEDULE - SBL

LOCATION BY MILE MARKER	PASSING LANE					DRIVING LANE					REMARKS
	CLASS A PATCHING TYPE II SQ YD	TYPE IV SQ YD	PATCH REINF. SQ YD	SAW CUTS FOOT	TIE BARS EACH	CLASS A PATCHING TYPE II SQ YD	TYPE IV SQ YD	PATCH REINF. SQ YD	SAW CUTS FOOT	TIE BARS EACH	
77.76											LEE COUNTY LINE
76.35						13		13	68		
76.05						8		8	60		
76.00						8		8	60		
75.05						13		13	68		
75.00						8		8	60		
74.20						8		8	60		
74.10						8		8	60		
72.80						8		8	60		
72.74											US RTE. 34
72.40						8		8	60		
72.30						8		8	60		
71.50						8		8	60		
71.30						8		8	60		
71.20						8		8	60		
71.00						8		8	60		
70.80						8		8	60		
70.10						8		8	60		
69.80						0	25	25	86		
68.60						8		8	60		
68.10						8		8	60		
67.10						8		8	60		
66.90						11		11	64		
66.81											US RTE. 52
63.80						8		8	60		
63.70						8		8	60		
63.60						8		8	60		
62.90						8		8	60		
62.20						8		8	60		
61.20						8		8	60		
60.80						8		8	60		
60.70						8		8	60		
59.95						8		8	60		
59.90						8		8	60		
59.70											FAI RTE. 80
59.60						8		8	60		
58.40						11		11	66		ON RAMP
57.55						13		13	78		
57.50						13		13	78		
SUBTOTALS						299	25	324	2188		

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PLOT DATE = Mar 31, 2010 - 08:19:39 AM		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SCHEDULES - SBL**

SCALE: \_\_\_\_\_ SHEET NO. 1 OF 2 SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	D3 PATCHING 2011-2	LASALLE	16	7
CONTRACT NO. 66A31			ILLINOIS	

PATCHING SCHEDULE - SBL

LOCATION BY MILE MARKER	PASSING LANE					DRIVING LANE					REMARKS
	CLASS A PATCHING TYPE II SQ YD	CLASS A PATCHING TYPE IV SQ YD	PATCH REINF. SQ YD	SAW CUTS FOOT	TIE BARS EACH	CLASS A PATCHING TYPE II SQ YD	CLASS A PATCHING TYPE IV SQ YD	PATCH REINF. SQ YD	SAW CUTS FOOT	TIE BARS EACH	
57.42											US RTE. 6
56.80						13	0	13	78		
55.37											IL RTE. 351
54.46											OGLESBY SPUR
54.20						8	0	8	66		
53.13											IL RTE. 251
52.85						8		8	66		
52.80						13		13	78		
52.40	8		8	60							
52.37											IL RTE. 71
52.20						13		13	78		
51.30						8		8	66		
50.50						8		8	66		
50.40						8		8	66		
48.70						8		8	66		
48.50											RAY RICHARDSON ROAD
48.20	8		8	60							
48.10	8		8	60							
47.70						11		11	72		
47.30						8		8	66		
44.70	8		8	60							
43.70						11		11	72		
42.30						13		13	78		
42.20						8		8	66		
41.40											IL RTE. 18
40.70	8		8	60							
40.24											MARSHALL COUNTY LINE
SUBTOTALS	40	0	40	300	0	139	0	139	984		
TOTALS	40	0	80	600	0	437	25	463	3172		

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 PLOT DATE = Mar 31, 2010 - 08:19:45 AM

DESIGNED - RON WOODSHANK  
 DRAWN - RON WOODSHANK  
 CHECKED -  
 DATE -

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SCHEDULES - SBL

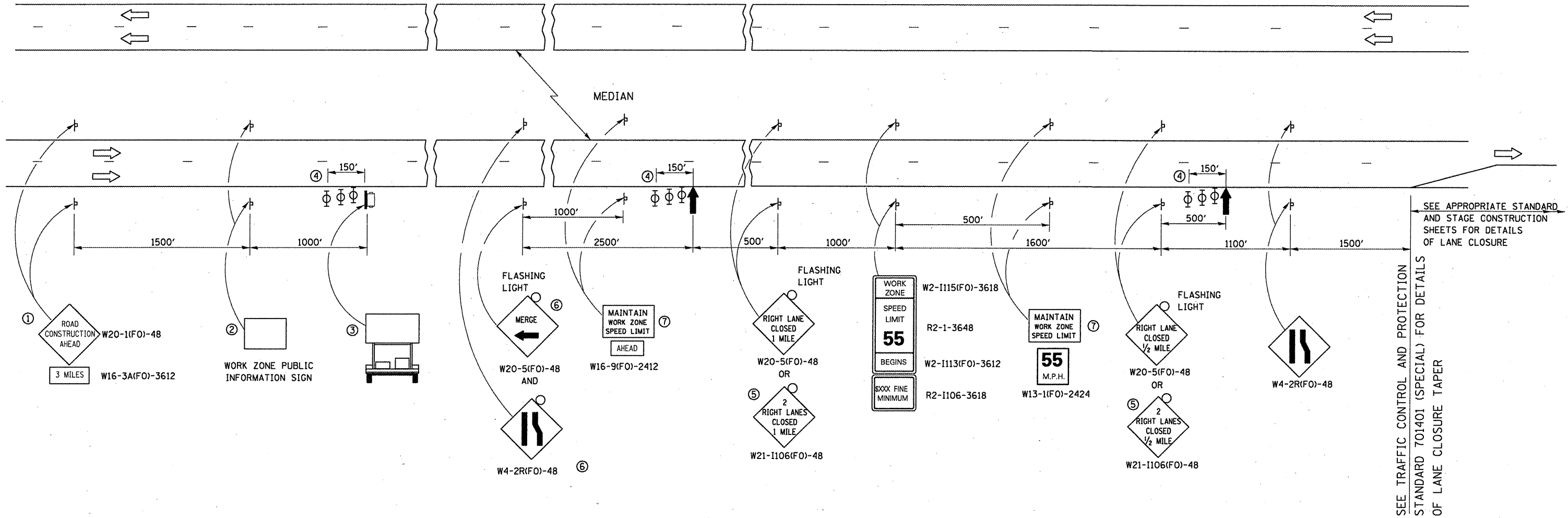
SCALE: SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	D3 PATCHING 2011-2	LASALLE	16	8
CONTRACT NO. 66A31			ILLINOIS	



PAVEMENT MARKING SCHEDULE - SBL				
LOCATION	EPOXY PAVEMENT MARKING TYPE 1, LINE 4"		EPOXY PAVEMENT MARKING TYPE 1, LINE 6"	REMARKS
	YELLOW	WHITE	WHITE	
MILE POST				
77.76				LEE COUNTY LINE
76.35		10	3	
76.05		6	2	
76.00		6	2	
75.05		10		
75.00		6	2	
74.20		6	2	
74.10		6		
72.80		6	2	
72.74				US RTE. 34
72.40		6	2	
72.30		6		
71.50		6	2	
71.30		6	2	
71.20		6	2	
71.00		6	2	
70.80		6	2	
70.10		6	2	
69.80				
68.60		6	2	
68.10		6		
67.10		6	2	
66.90		8	2	
66.81				US RTE. 52
63.80		6	2	
63.70		6	2	
63.60		6	2	
62.90		6		
62.20		6	2	
61.20		6	2	
60.80		6	2	
60.70		6	2	
59.95		6	2	
59.90		6	2	
59.70				FAI RTE. 80
59.60		6	2	
58.40		6	2	ON RAMP
57.55		10	3	
57.50		10	3	
57.42				US RTE. 6

PAVEMENT MARKING SCHEDULE - SBL				
LOCATION	EPOXY PAVEMENT MARKING TYPE 1, LINE 4"		EPOXY PAVEMENT MARKING TYPE 1, LINE 6"	REMARKS
	YELLOW	WHITE	WHITE	
MILE POST				
56.80		10	3	
54.46				OGLESBY SPUR
54.20		6	2	
53.13				IL RTE. 251
52.85		6	2	
52.80		10	3	
52.40	6			
52.37				IL RTE. 71
52.20		10	3	
51.30		6	2	
50.50		6	2	
50.40		6	2	
48.70		6	2	
48.50				RAY RICHARDSON ROAD
48.20	6			
48.10	6			
47.70		8	2	
47.30		6	2	
44.70	6			
43.70		8	2	
42.30		10	3	
42.20		6	2	
41.40				IL RTE. 18
40.70	6			
40.24				MARSHALL COUNTY LINE
NB SUBTOTALS	30	326		
TOTALS		356	93	

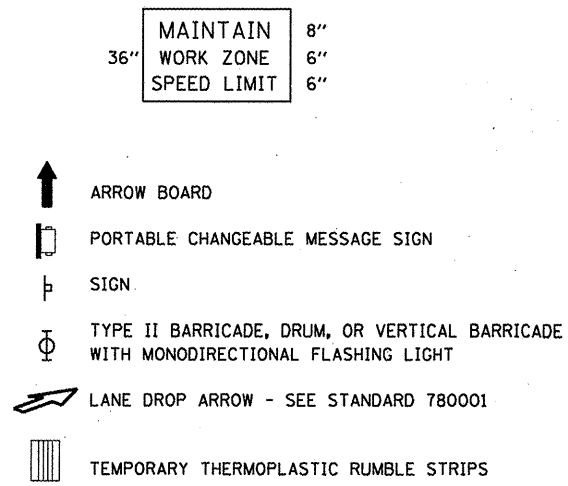


SEE APPROPRIATE STANDARD AND STAGE CONSTRUCTION SHEETS FOR DETAILS OF LANE CLOSURE

SEE TRAFFIC CONTROL AND PROTECTION STANDARD 701401 (SPECIAL) FOR DETAILS OF LANE CLOSURE TAPER

- ① THE ROAD CONSTRUCTION AHEAD SIGN SHALL BE LOCATED 3 MILES IN ADVANCE OF THE PROJECT LIMITS.
- ② THE MESSAGE AND SIZE OF THE WORK ZONE PUBLIC INFORMATION SIGN SHALL BE AS SPECIFIED BY THE DEPARTMENT.
- ③ TO BE PLACED IN THE MEDIAN WHEN FEASIBLE, THE MESSAGE BOARD SHALL BE USED TO DISPLAY STATUS OF LANES WITHIN THE PROJECT. THE PRIMARY MESSAGES SHALL BE:  
"RIGHT LANE CLOSED" / " x MILES AHEAD"  
"LEFT LANE CLOSED" / " x MILES AHEAD"  
"ALL LANES OPEN"
- ④ THREE, TYPE II BARRICADES, DRUMS, OR VERTICAL BARRICADES AT 50' CENTERS.
- ⑤ THIS SIGN SHALL BE USED WHEN 2 LANES ARE CLOSED.
- ⑥ WHEN THE LEFT LANE IS CLOSED, SWITCH THESE TWO SIGNS AND THE DIRECTION OF THE MERGE ARROW.

⑦ 48"x36" FLUORESCENT ORANGE SIGN WITH BLACK LETTERS.



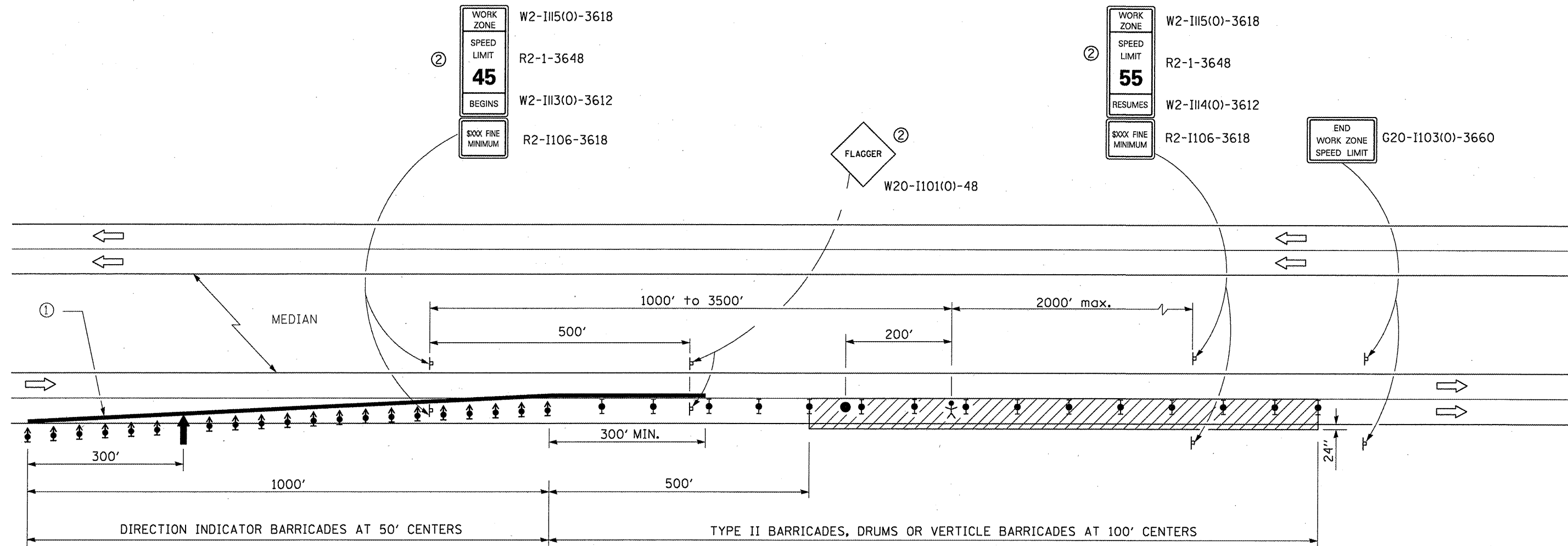
**GENERAL NOTE:**

THIS STANDARD IS USED WHERE AT ANY TIME A LANE IS CLOSED ON A FREEWAY/EXPRESSWAY.

WHEN THE LEFT LANE IS CLOSED, LEFT LANE CLOSED SIGNS SHALL BE SUBSTITUTED FOR THE RIGHT LANE CLOSED SIGNS.


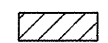





THE FIRST TWO SIGNS AND THE MESSAGE BOARD ARE STATIONARY. THE OTHER SIGNS AND ARROWBOARDS SHALL BE MOVED AS NECESSARY TO MAINTAIN THE REQUIRED DISTANCE FROM THE START OF THE LANE CLOSURE TAPER(S).

WHEN WORK OPERATIONS ARE NEAR MAJOR INTERCHANGES THE CONTRACTOR SHALL FURNISH ADDITIONAL MESSAGE BOARDS AND UTILIZED AT THE DIRECTION OF THE ENGINEER.



SEE TRAFFIC CONTROL AND PROTECTION STANDARD 701400 (SPECIAL) FOR APPROACH TO LANE CLOSURE DETAILS

**SYMBOLS**

-  ARROW BOARD
-  WORK AREA
-  WORKER
-  SIGN
-  DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
-  TYPE II BARRICADES, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
-  ● Flagger with traffic control sign

- ① REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' ALONGSIDE THE WORK AREA WHEN THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE EDGE LINE SHALL BE WHITE FOR RIGHT LANE CLOSURES AND YELLOW FOR LEFT LANE CLOSURES. TEMPORARY RAISED REFLECTORIZED PAVEMENT MARKERS AT 25' CENTERS SHALL BE USED TO SUPPLEMENT THE PAVEMENT MARKING TAPE.
- ② WORK ZONE SPEED LIMITS SIGNS AND FLAGGER SIGNS SHALL BE MOVED AS NECESSARY TO MAINTAIN THE REQUIRED SPACING BETWEEN THE SIGNS AND THE WORKERS IN EACH SEPARATE WORK ACTIVITY.

**GENERAL NOTES**

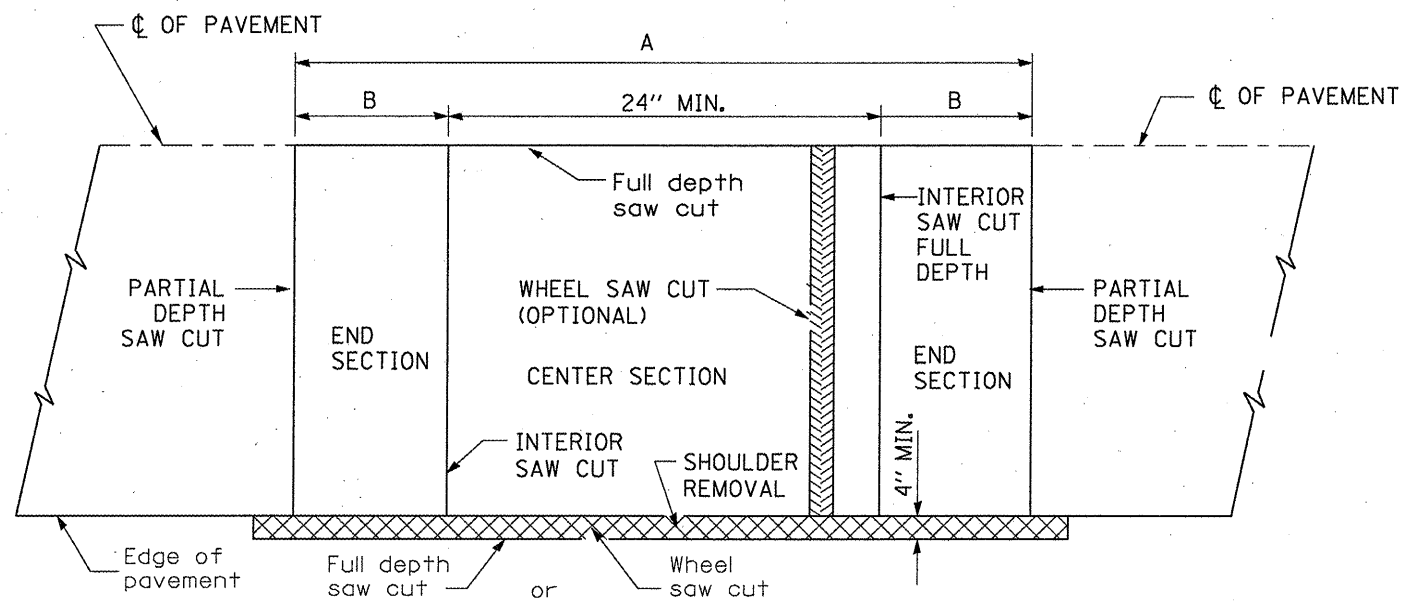
THIS STANDARD IS USED WHERE AT ANY TIME ANY VEHICLE, EQUIPMENT, WORKERS OR THEIR ACTIVITIES WILL ENCROACH ON THE LANE ADJACENT TO THE SHOULDER OR ON THE SHOULDER WITHIN 24" OF THE EDGE OF PAVEMENT.

THIS STANDARD MUST ALWAYS BE USED IN COMBINATION WITH TRAFFIC CONTROL AND PROTECTION DETAIL 701400 (SPECIAL).

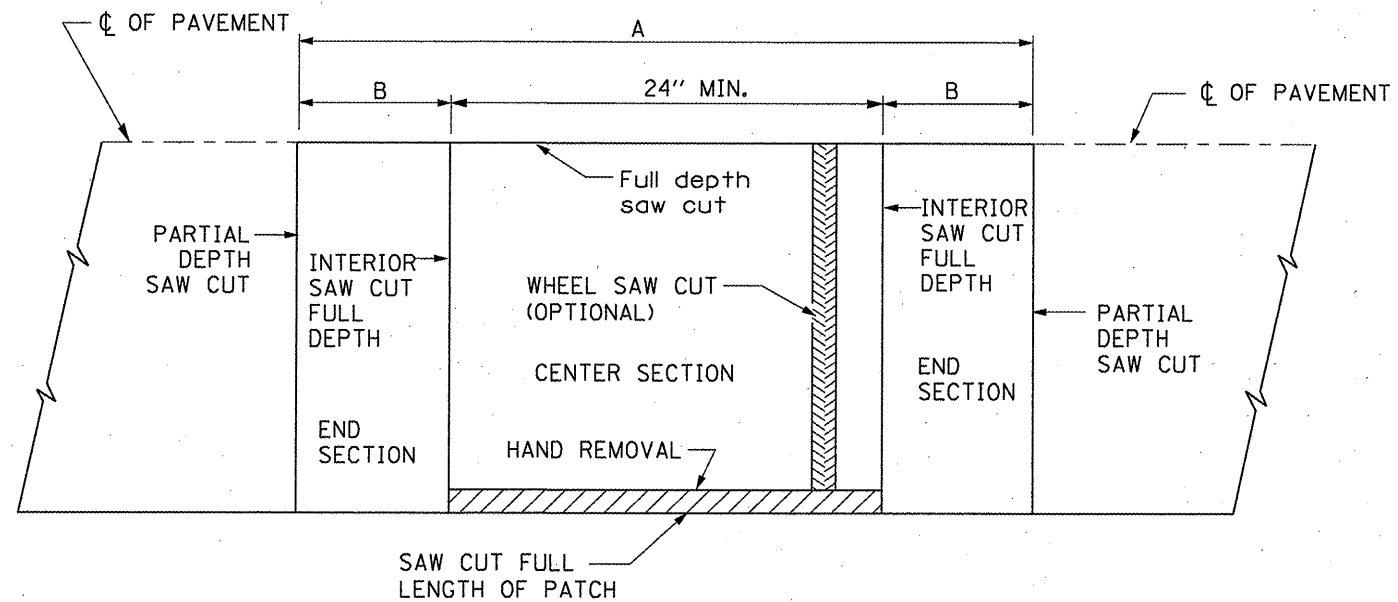
THIS STANDARD ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE LEFT LANE. UNDER THESE CONDITIONS, THE SET UP WOULD BE A MIRROR IMAGE TO WHAT IS SHOWN.

A CHECK BARRICADE SHALL BE PLACED IN THE MIDDLE OF THE CLOSED LANE AND AT THE SHOULDER AT 1000' CENTER.

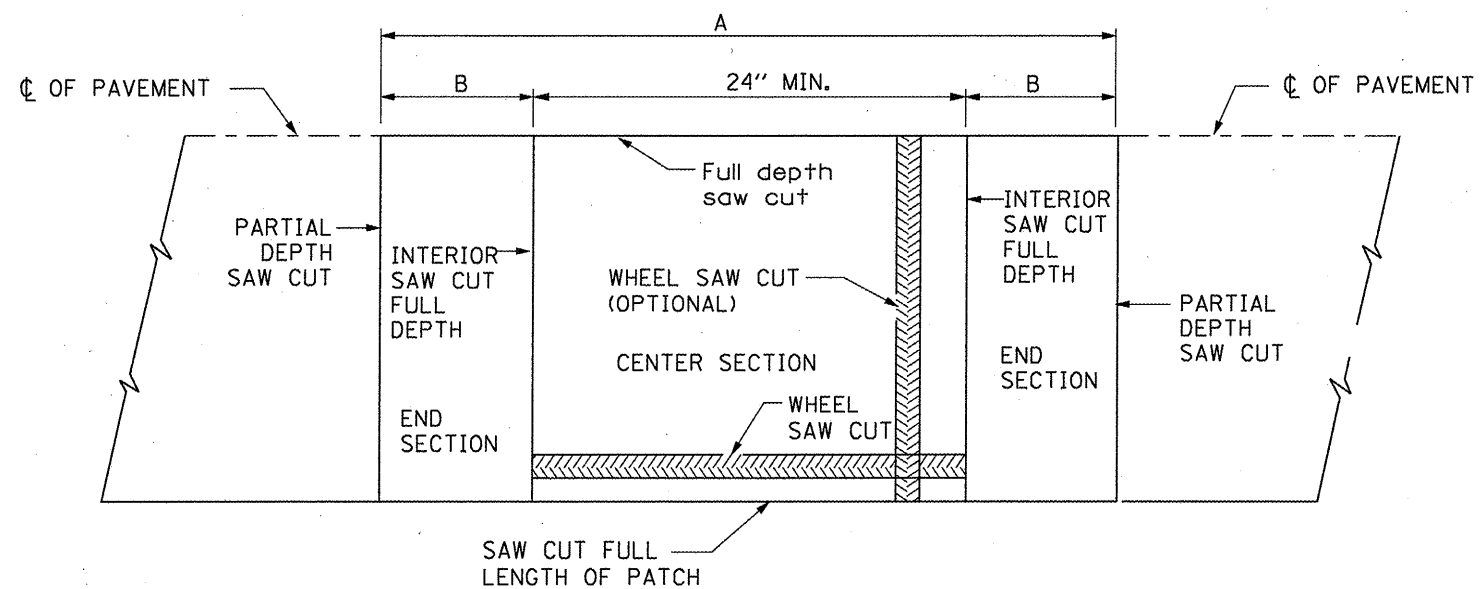
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c:\pwwork\PW1DDT\WOODSHANKR\0143168	D366A31-details.dgn	DRAWN - RON WOODSHANK	REVISED -			39	D3 PATCHING 2011-2	LASALLE	16	11	
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 66A31					
	PLOT DATE = Mar 31, 2010 - 08:20:00 AM	DATE -	REVISED -			[ILLINOIS]					



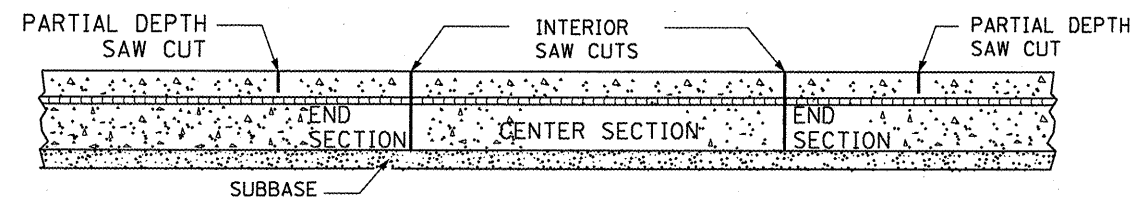
**PAVEMENT SAWING DETAIL**  
(HMA SHOULDER)



**PAVEMENT SAWING DETAIL**  
(PCC SHOULDER)



**ALTERNATE SAWING DETAIL**  
(PCC SHOULDER)



**SAW CUT DETAIL**

EXISTING LONGITUDINAL REINFORCEMENT BARS	A (min.)	B (min.)	C (min.)
No. 7	6'-0"	24"	22"

**NOTES:**

- EVERY 3rd INTERSECTION MUST BE TIED
- LAP NEW REINFORCEMENT WITH EXISTING LONGITUDINAL REINFORCEMENT
- VARIABLES: WHERE  $S_1$  AND  $S_2$  ARE  $2\frac{1}{2}$ " MIN. AND 12" MAX.  $D_1 = 2(S_1)$  AND  $D_2 = 2(S_2)$

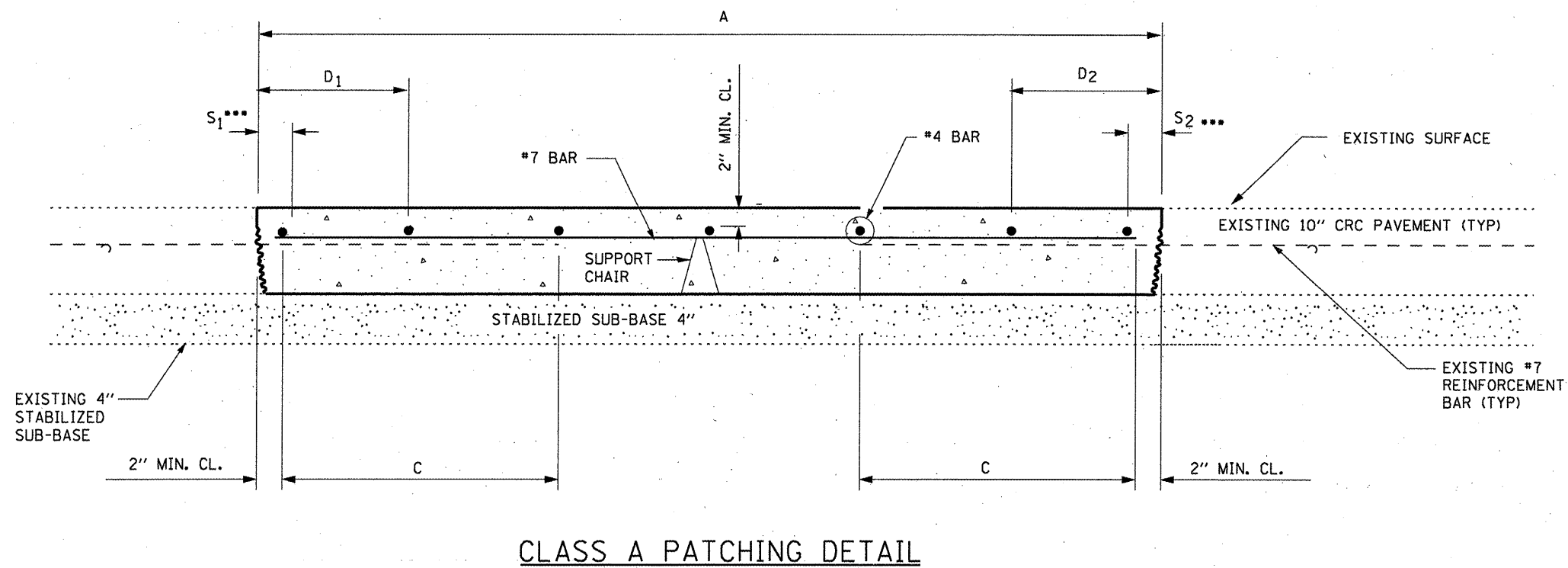
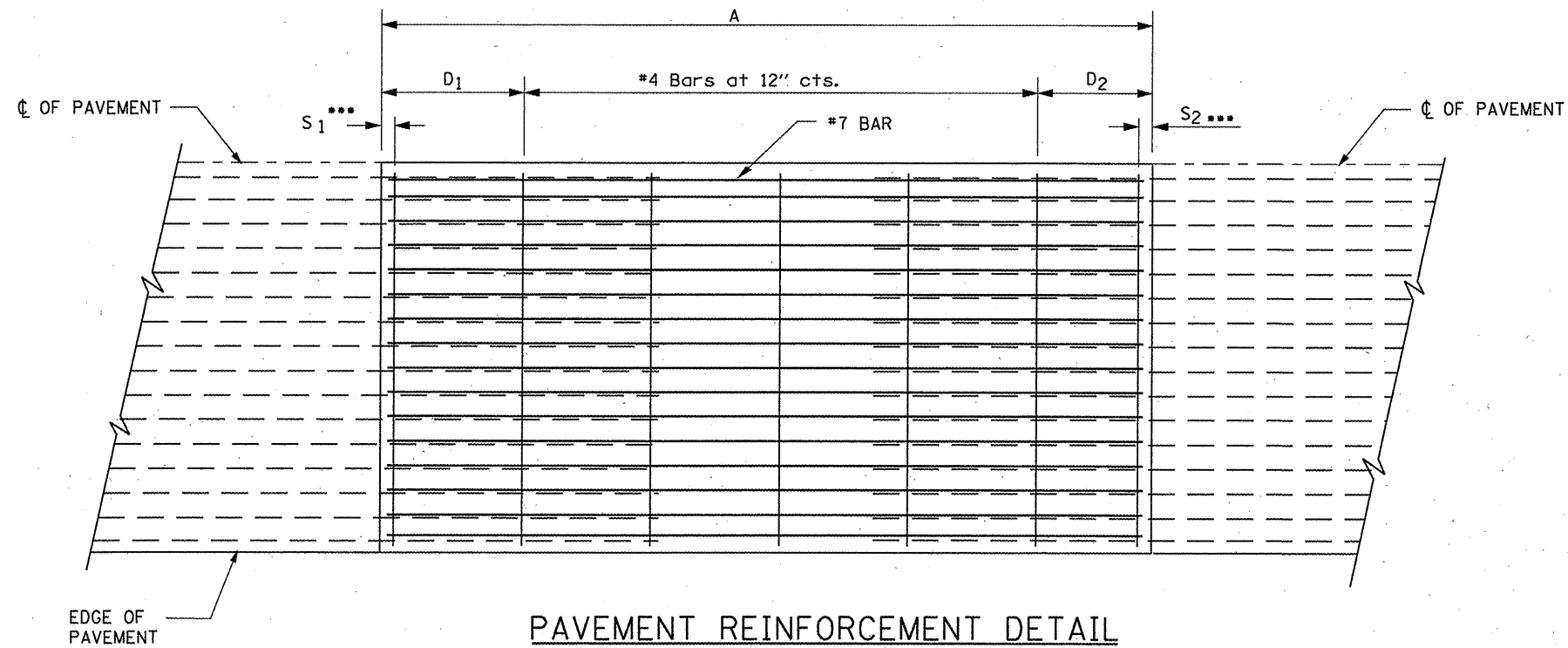
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PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -	REVISED -
PLOT DATE = Mar 31, 2010 - 08:20:06 AM	DATE -	REVISED -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PATCHING DETAILS**

SCALE: SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	D3 PATCHING 2011-2	LASALLE	16	12
CONTRACT NO. 66A31			ILLINOIS	



**NOTES:**

- EVERY 3rd INTERSECTION MUST BE TIED
- LAP NEW REINFORCEMENT WITH EXISTING LONGITUDINAL REINFORCEMENT
- VARIABLES: WHERE  $S_1$  AND  $S_2$  ARE  $2\frac{1}{2}$ " MIN. AND 12" MAX.  $D_1 = 2(S_1)$  AND  $D_2 = 2(S_2)$

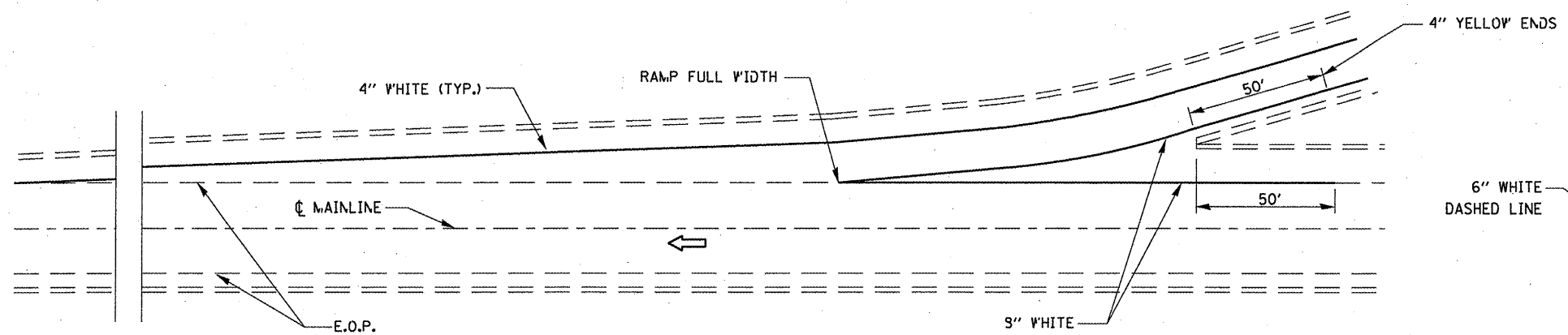
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	PLOT SCALE = 100,0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = Mar 31, 2010 08:20:11 AM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

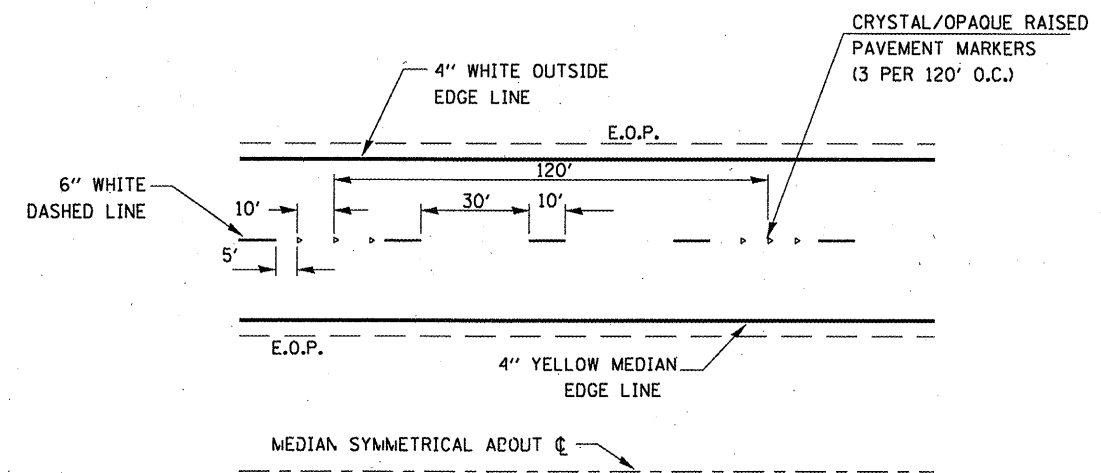
**PATCHING DETAILS**

SCALE: \_\_\_\_\_ SHEET NO. 2 OF 2 SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	D3 PATCHING 2011-2	LASALLE	16	13
CONTRACT NO. 66A31				
ILLINOIS				



**TYPICAL PAVEMENT MARKING FOR ENTRANCE RAMP TERMINALS**

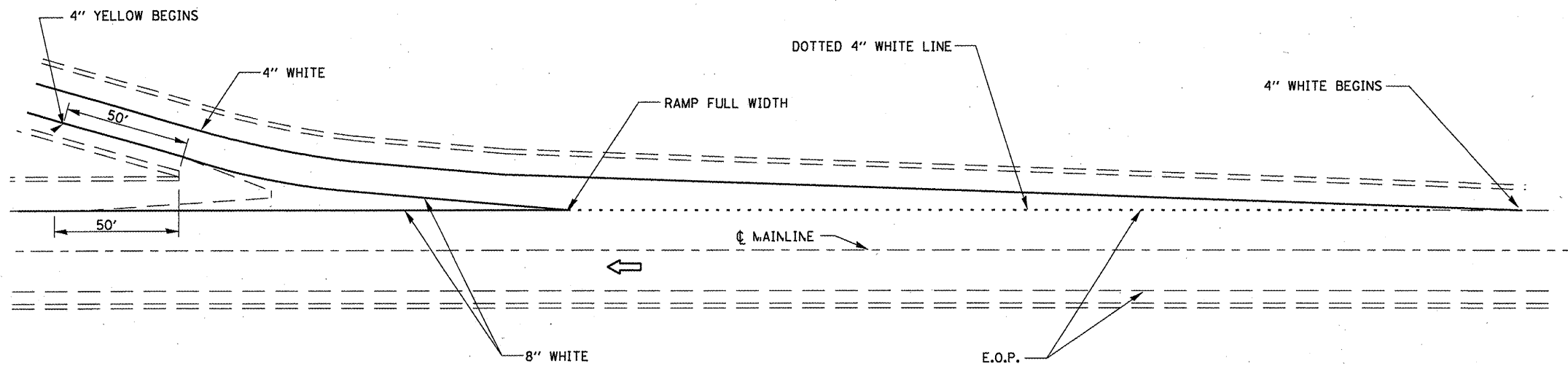


**TYPICAL PAVEMENT MARKINGS**

**NOTE:**

WHEN PATCHING IS ADJACENT TO 8" PAVEMENT MARKING LINE THE 8" STRIP SHALL BE REPLACED WITH TWO 4" LINES.

RAISED REFLECTIVE PAVEMENT MARKERS THAT FALL WITHIN PATCHED BOUNDARIES WILL NOT BE REPLACED.



**TYPICAL PAVEMENT MARKINGS FOR EXIT RAMP TERMINALS**

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -
cs:\pwork\VPWIDOT\WOODSHANKRL\d0143168	0366A31-details.dgn	DRAWN - RON WOODSHANK	REVISED -
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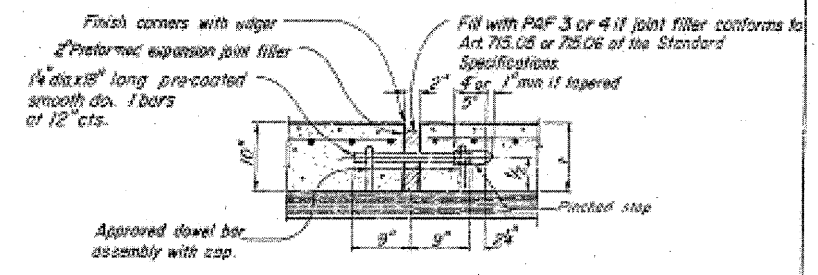
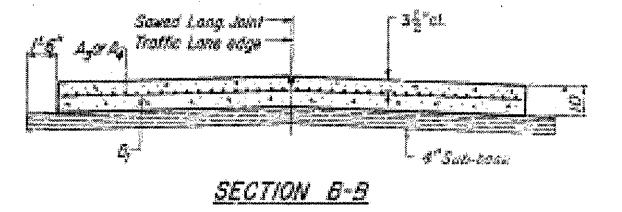
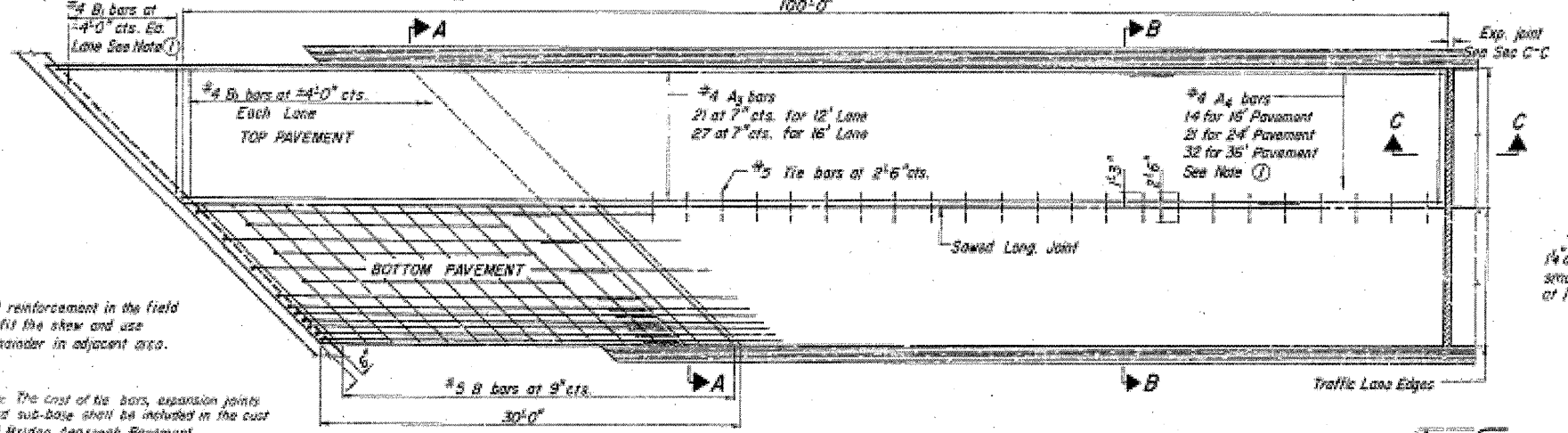
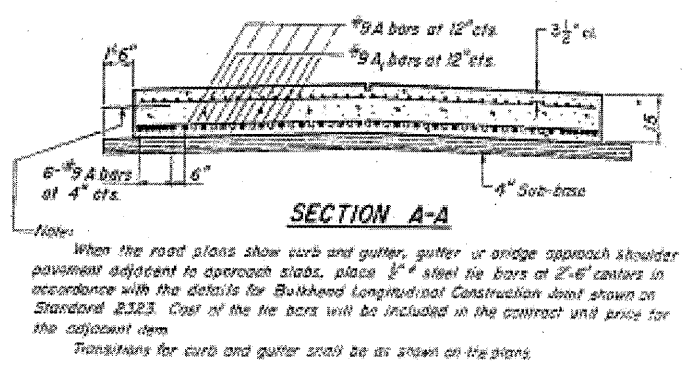
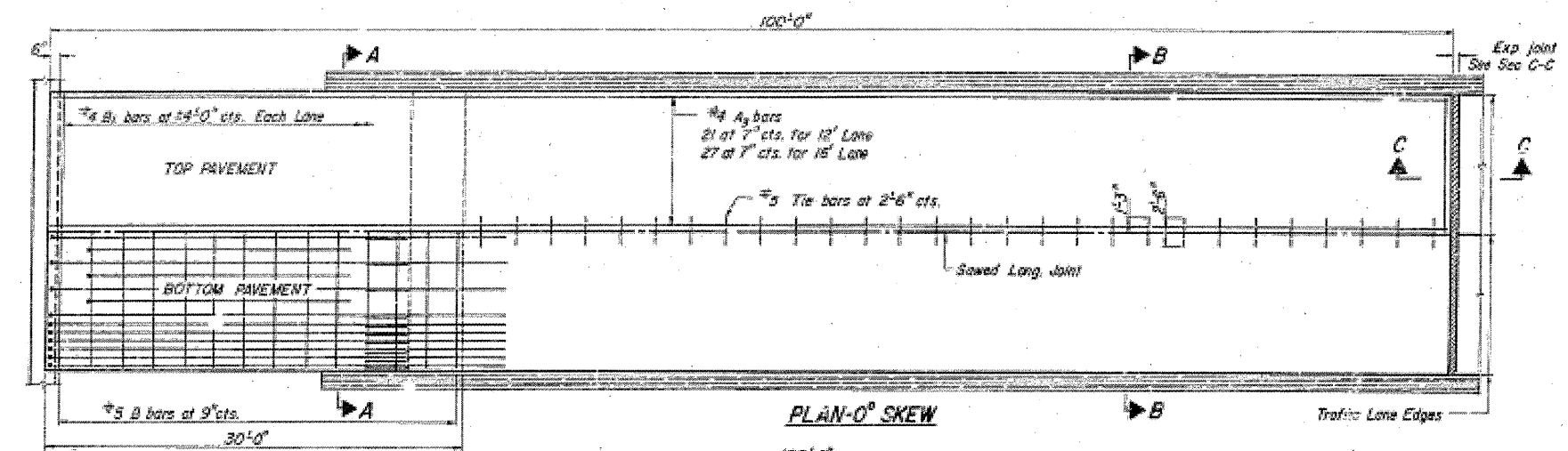
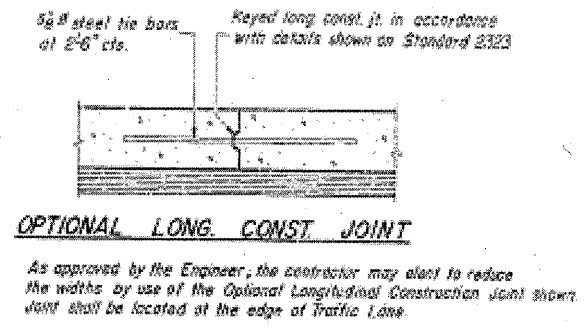
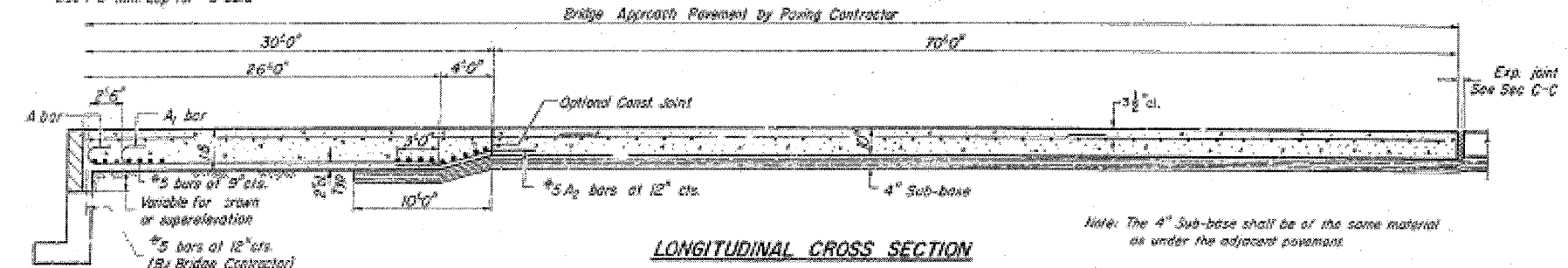
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**PAVEMENT MARKING**

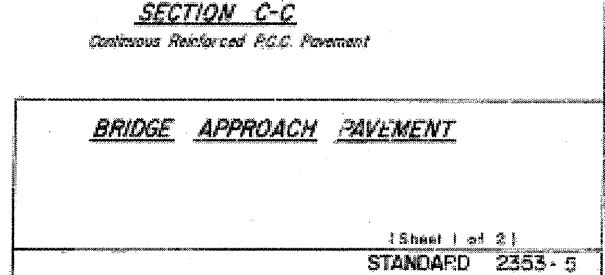
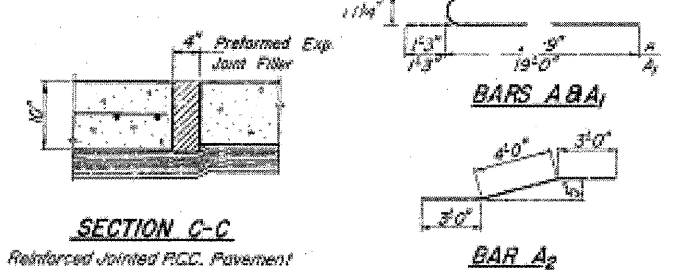
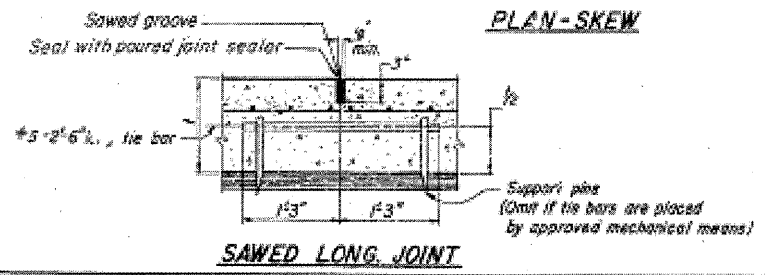
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	D3 PATCHING 2011-2	LASALLE	16	14
ILLINOIS			CONTRACT NO. 66A31	

Use 1/4" dia. #4 bars for min. 3/4" dia. Use 1/4" Min. Lap for #4 bars.  
Use 1/2" Min. Lap for #5 bars



Note: The cost of tie bars, expansion joints and sub-base shall be included in the cost of Bridge Approach Pavement.



Illinois Department of Transportation  
 DESIGNED: Ron Woodshank  
 DRAWN: Ron Woodshank  
 CHECKED: \_\_\_\_\_  
 DATE: \_\_\_\_\_

FOR INFORMATION ONLY

FILE NAME =	USER NAME = woodshank-r	DESIGNED - RON WOODSHANK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING APPROACH PAVEMENT	F.A.I. RT#	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pw\work\PI\DOT\WOODSHANK\1\0143168\0366A31-details.dgn		DRAWN - RON WOODSHANK	REVISED -			39	D3 PATCHING 2011-2	LASALLE	16	15	
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 66A31					
PLOT DATE = Mar 31, 2010 - 08:20:23 AM		DATE -	REVISED -			ILLINOIS					

H. 5.20 e

QUANTITIES FOR 100-FOOT APPROACH SLAB

Skew Angle	Bottom Reinforcement			Top Reinforcement				Total Weight bars-lbs.
	#5 B bars		A	#4 A <sub>1</sub> bars	#4 A <sub>2</sub> bars	B <sub>1</sub>	B <sub>2</sub>	
	No.	Length						

Skew Angle	Bottom Reinforcement			Top Reinforcement				Total Weight bars-lbs.
	#5 B bars		A	#4 A <sub>1</sub> bars	#4 A <sub>2</sub> bars	B <sub>1</sub>	B <sub>2</sub>	
	No.	Length						

Skew Angle	Bottom Reinforcement			Top Reinforcement				Total Weight bars-lbs.
	#5 B bars		A	#4 A <sub>1</sub> bars	#4 A <sub>2</sub> bars	B <sub>1</sub>	B <sub>2</sub>	
	No.	Length						

16-FOOT WIDTH PAVEMENT

Skew Angle	No.	Length	A	B <sub>1</sub>	B <sub>2</sub>	Total Weight bars-lbs.
0°	40	15'-6"	—	81	34'-3"	5980
5°	40	15'-7"	—	81	34'-0"	6010
10°	40	15'-9"	—	81	33'-10"	6020
15°	40	15'-1'	—	81	33'-7"	6030
20°	40	15'-6"	—	81	33'-5"	6050
25°	40	17'-1"	—	81	33'-2"	6070
30°	40	17'-11"	—	81	32'-11"	6110
35°	40	18'-11"	—	81	32'-7"	6150
40°	40	20'-3"	—	81	32'-3"	6200
45°	40	21'-11"	—	81	31'-11"	6280
50°	40	24'-1"	—	81	31'-5"	6370
55°	40	27'-0"	—	81	30'-11"	6490
60°	40	31'-0"	—	81	30'-2"	6660

Bridge Approach Pavement  
178 Sq. Yds.

24-FOOT WIDTH PAVEMENT

Skew Angle	No.	Length	A	B <sub>1</sub>	B <sub>2</sub>	Total Weight bars-lbs.
0°	40	25'-0"	—	126	34'-3"	8840
5°	40	23'-7"	—	126	33'-10"	8890
10°	40	23'-10"	—	126	33'-6"	8890
15°	40	24'-4"	—	126	33'-2"	8910
20°	40	25'-0"	—	126	32'-9"	8940
25°	40	25'-11"	—	126	32'-4"	8980
30°	40	27'-2"	—	126	31'-11"	9030
35°	40	28'-8"	—	126	31'-5"	9090
40°	40	30'-8"	—	126	30'-10"	9170
45°	40	33'-3"	—	126	30'-3"	9290
50°	80	19'-1"	—	186	25'-5"	9490
55°	80	21'-4"	—	186	24'-5"	9680
60°	80	24'-4"	—	186	27'-3"	9940

Bridge Approach Pavement  
267 Sq. Yds.

36-FOOT WIDTH PAVEMENT

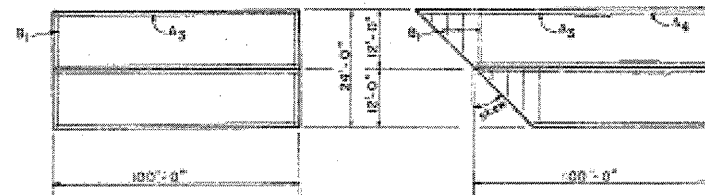
Skew Angle	No.	Length	A	B <sub>1</sub>	B <sub>2</sub>	Total Weight bars-lbs.
0°	40	35'-6"	—	189	34'-3"	13040
5°	40	35'-6"	—	189	33'-8"	13090
10°	40	36'-0"	—	189	33'-2"	13110
15°	80	19'-4"	—	189	32'-8"	13230
20°	80	19'-10"	—	189	32'-0"	13280
25°	80	20'-7"	—	189	31'-5"	13330
30°	80	21'-8"	—	189	30'-9"	13410
35°	80	22'-8"	—	189	30'-0"	13510
40°	80	24'-2"	—	189	29'-2"	13630
45°	80	25'-2"	—	189	28'-3"	13840
50°	80	26'-8"	—	189	27'-1"	14050
55°	80	28'-0"	—	189	25'-8"	14330
60°	80	30'-7"	—	189	23'-10"	14710

Bridge Approach Pavement  
400 Sq. Yds.

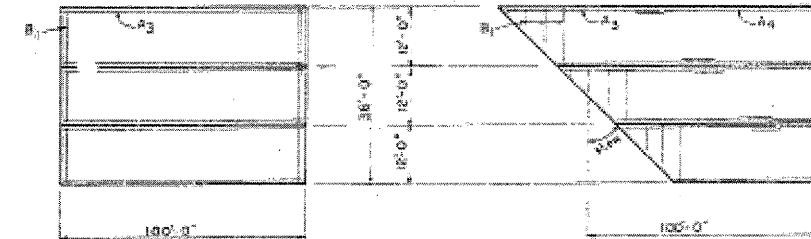
TOP OF SLAB BAR ARRANGEMENT



TOP OF SLAB BAR ARRANGEMENT



TOP OF SLAB BAR ARRANGEMENT



Illinois Department of Transportation  
 PASSED: May 20, 1981  
 APPROVED: May 20, 1981  
 Engineer of Bridge

BRIDGE APPROACH PAVEMENT  
 1 Sheet 2 of 2  
 STANDARD 2353-5  
 Full Set OWS/S-

FOR INFORMATION ONLY

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING APPROACH PAVEMENT	F.A.I. RITE. 39	SECTION D3 PATCHING 2011-2	COUNTY LASALLE	TOTAL SHEETS 16	SHEET NO. 16
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	PLOT DATE = Mar 31, 2010 - 08:20:29 AM	DATE	REVISOR							
SCALE: SHEET NO. 2 OF 2 SHEETS STA. TO STA.						ILLINOIS CONTRACT NO. 66A31				