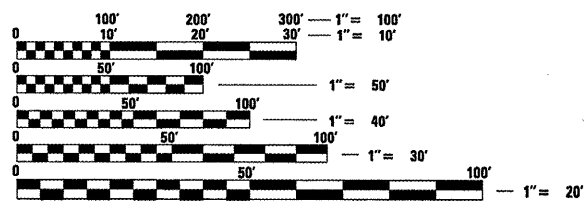


**INDEX OF SHEETS**

- 1 COVER SHEET
- 2 GENERAL NOTES & HIGHWAY STANDARDS
- 3 SUMMARY OF QUANTITIES
- 4-11 TYPICAL SECTIONS
- 12 SCHEDULE OF QUANTITIES
- 13-15 ALIGNMENT LAYOUT
- 16-19 DETAILS FOR RURAL/URBAN ENT.,  
MAILBOX TURNOUT & SIDEROADS
- 20 RUMBLE STRIP DETAILS

CLASSIFICATION = MINOR ARTERIAL  
 CURRENT ADT: 3100 (2007)  
 PV: 84%  
 SU: 8%  
 MU: 8%



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811

PROJECT ENGINEER: JOHN NEGANGARD (217) 782-6990  
 TEAM ENGINEER: MARK DUST (217) 785-0597

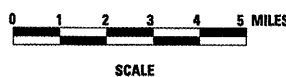
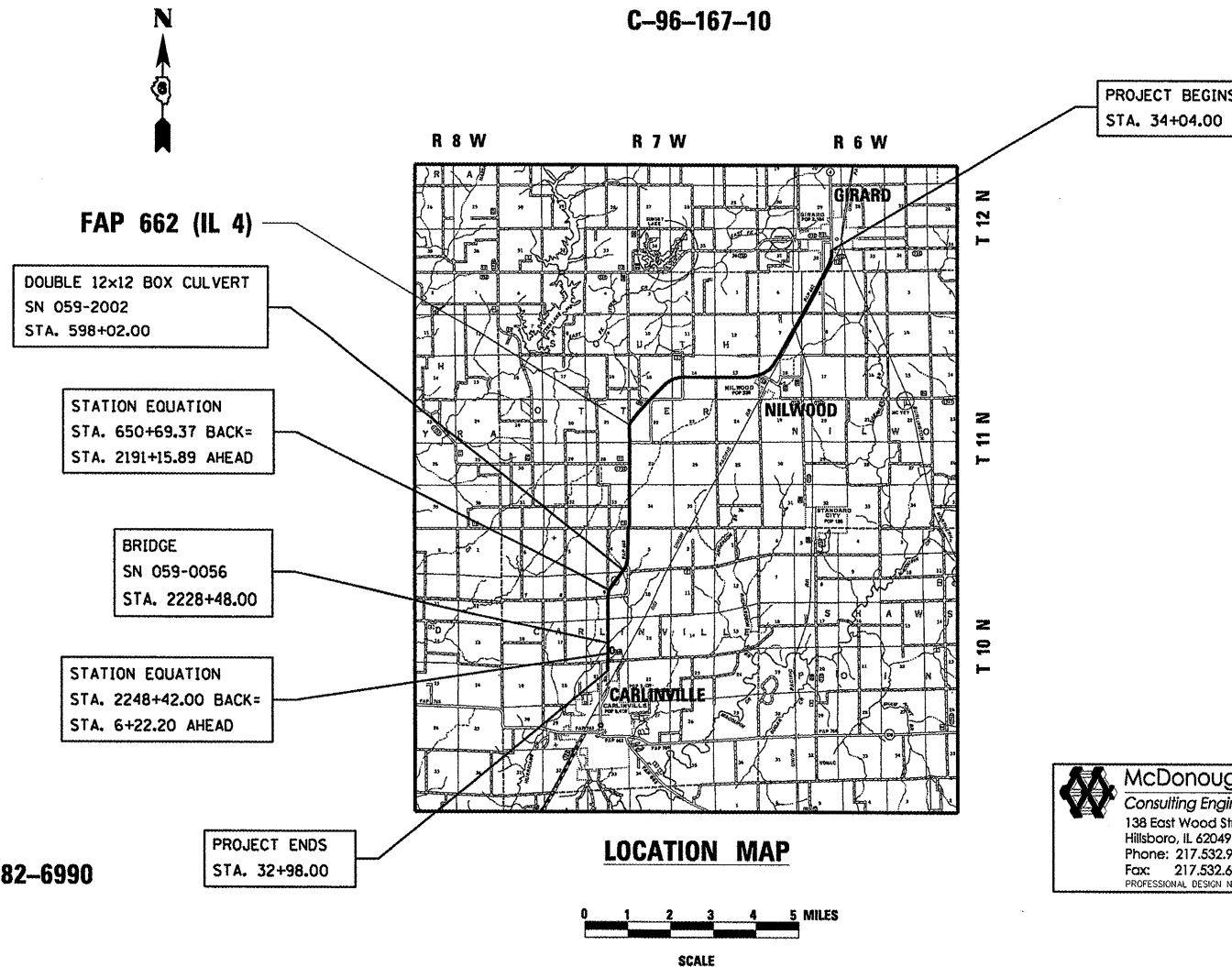
CONTRACT NO. 72D39

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**  
**PROPOSED**  
**HIGHWAY PLANS**

FAP ROUTE: 662 (IL 4)  
 SECTION (D, DX, E, F) I

**NEW SHOULDERS,  
 MILLED RUMBLE STRIPS, PATCHING  
 MACOUPIN COUNTY**

C-96-167-10



**McDonough-Whitlow, P.C.**  
 Consulting Engineers & Land Surveyors  
 138 East Wood Street  
 Hillsboro, IL 62049  
 Phone: 217.532.9233  
 Fax: 217.532.6300  
 PROFESSIONAL DESIGN No. 184-002754



*Tom M. McDonough* 3/26/2010  
 Signature Date

11/30/11  
 Expires

**PRINTED BY THE AUTHORITY  
 OF THE STATE OF ILLINOIS**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
662	(D,DX,E,F) I	MACOUPIN	20	1
ILLINOIS CONTRACT NO. 72D39				



STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS

SUBMITTED April 1 20 10  
*Roger J. Pihl*  
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 7 20 10  
*Scott E. Stitt P.E.*  
 acting ENGINEER OF DESIGN AND ENVIRONMENT

May 7 20 10  
*Christine M. Reed*  
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

NET LENGTH = 70,067.28 FT. = 13.270 MILE

LIST OF ILLINOIS DOT HIGHWAY STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442101-07	CLASS B PATCHES
442201-03	CLASS C and D PATCHES
482006-03	HMA SHOULDER ADJACENT TO RIGID PAVEMENT
701006-03	OFF-ROAD OPERATIONS, 2L, 2W, 15' MIN. TO 24" FROM PAVEMENT EDGE
701201-03	LANE CLOSURE, 2L, 2W, DAY ONLY FOR SPEEDS > 45 MPH
701306-02	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS > 45 MPH
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS-DAY ONLY
701326-03	LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS > 45 MPH
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS

HOT MIX ASPHALT CRITERIA

MIXTURE USE(S):	SHOULDERS	PATCHES	INCIDENTAL HMA SURFACING
PG	PG 58-22	PG 64-22	PG 64-22
DESIGN AIR VOIDS	2.0% @ N DESIGN = 30	4.0% @ N DESIGN = 50	4.0% @ N DESIGN = 50
MIXTURE COMPOSITION (GRADATION MIXTURE)	BAM	IL. 19.0	IL. 9.5 OR 12.5
FRICTION AGGREGATE	N/A	N/A	MIX C

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED FOR QUANTITY CALCULATIONS.

HOT MIX ASPHALT MATERIALS	0.056 TON/SQ. YD. • IN
AGG. SHLDS./BASE CSE.	2.05 TON/CU.YD.
BIT. MATLS. (PRIME COAT)	0.00038 TON/SQ. YD.
AGG. (PRIME COAT)	0.001425 TON/SQ. YD.
TEMPORARY ACCESS AGGREGATE	36 TONS - TOTAL

GENERAL NOTES

- 1.) THE THICKNESS OF BITUMINOUS MIXTURES SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
- 2.) ALL SAW CUTS, NECESSARY TO COMPLETE THE WORK DETAILED IN THESE PLANS, SHALL BE INCLUDED IN THE COST FOR THE VARIOUS PAY ITEMS INVOLVED. THE MINIMUM SAW CUT DEPTH IN THE PAVEMENT SHALL BE 1 1/2" UNLESS UNLESS OTHERWISE SPECIFIED IN A DETAIL SHOWN IN THE PLANS.
- 3.) UNLESS DIRECTED BY THE ENGINEER, PAVEMENT MARKING LINES SHALL NOT BE LAID DIRECTLY OVER A LONGITUDINAL CRACK OR JOINT NOR OVER A TAR OR ASPHALT PAINTED LINE. THE EDGE OF A CENTERLINE OR LANE LINE SHALL BE OFFSET A MINIMUM DISTANCE OF 2" FROM A LONGITUDINAL CRACK OR JOINT. EDGE LINES SHALL BE APPROXIMATELY 2" FROM THE EDGE LINE OF PAVEMENT. SEE SECTION 780 OF THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS.
- 4.) ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OUTSIDE THE LIMITS OF RIGHT OF WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPERATELY BUT SHALL BE INCLUDED IN THE COST FOR EXCAVATION AND GRADING. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- 5.) ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER LISTED IN THE INDEX OF SHEETS OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.
- 6.) IN ADDITION TO THE FIELD SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
- 7.) THE EXISTING ROAD SIGNS THAT INTERFERE WITH CONSTRUCTION WILL BE REMOVED OR RELOCATED AS DIRECTED BY THE ENGINEER. AFTER THE CONSTRUCTION IS COMPLETED, THE CONTRACTOR WILL REPLACE THE SIGNS AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT, AND NO COMPENSATION WILL BE ALLOWED.
- 8.) THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. THE J.U.L.I.E. NUMBER IS 800-892-0123. A MINIMUM OF FORTY-EIGHT HOURS ADVANCE NOTICE IS REQUIRED. ANY DAMAGE TO UNDERGROUND FACILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTOR'S EXPENSE, INCLUDING TEMPORARY REPAIRS WHICH MAY BE REQUIRED TO KEEP THE FACILITY OPERATIONAL WHILE MATERIAL IS BEING OBTAINED TO MAKE PERMANENT REPAIRS.
- 9.) THE DISTRICT BUREAU OF OPERATIONS SHALL BE NOTIFIED AT LEAST 14 DAYS PRIOR TO PLACEMENT OF FINAL PAVEMENT MARKINGS (PH: 217-782-7314)
- 10.) THE CONTRACTOR SHALL PROVIDE ADEQUATE DRAINAGE AND EROSION PROTECTION DURING THE LENGTH OF THE PROJECT.

COMMITMENTS

THE RESIDENT ENGINEER SHALL CONTACT STUDIES AND PLANS CONCERNING ANY MAJOR CHANGES.

<b>DISTRICT SIX</b>	
EXAMINED <u>March 16</u> 20 <u>10</u>	<u>Chris Wolter</u>
OPERATIONS ENGINEER	
EXAMINED <u>MARCH 18</u> 20 <u>10</u>	<u>Jimmy Z...</u>
PROGRAM IMPLEMENTATION ENGINEER	
EXAMINED <u>March 15</u> 20 <u>14</u>	<u>ARMLI</u>
PROGRAM DEVELOPMENT ENGINEER	

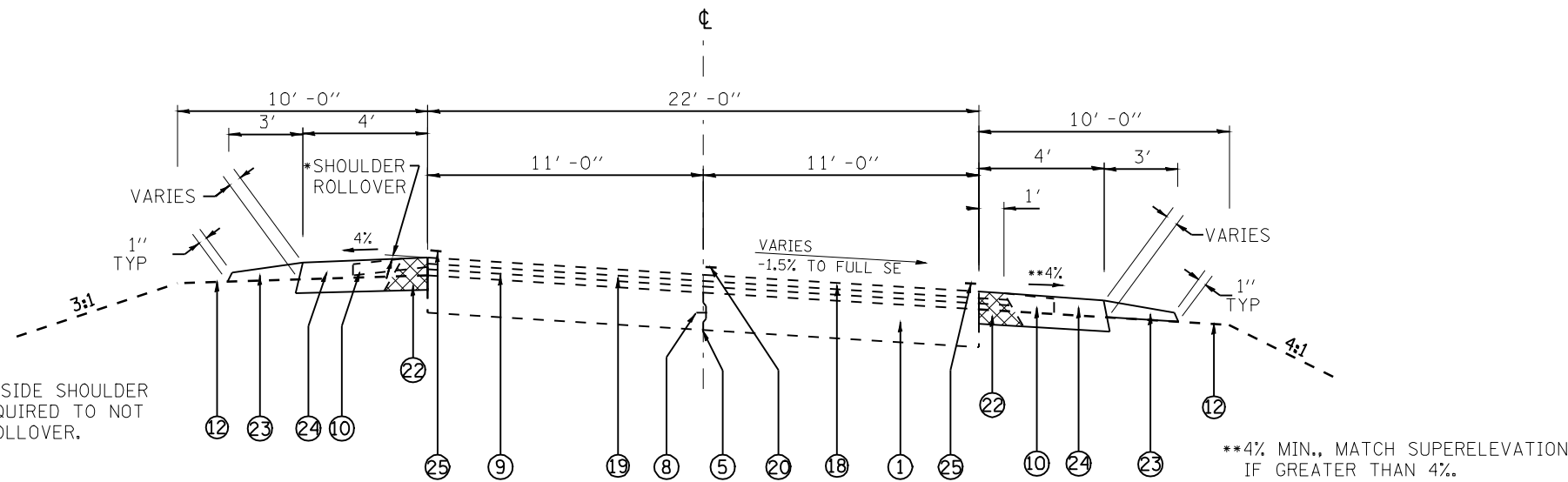
SUMMARY OF QUANTITIES

CODE NO.	ITEM	Unit	Total Quantity	CONST. TYPE CODE 1000 (100% STATE)
20200600	EXCAVATING AND GRADING EXISTING SHOULDER	UNIT	1353	1353
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	42	42
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	0.4	0.4
40600300	AGGREGATE (PRIME COAT)	TON	2	2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	749	749
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	183	183
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	166	166
44200972	CLASS B PATCHES, TYPE II, 10 1/2 INCH	SQ. YD.	44	44
44200994	CLASS B PATCHES, TYPE II, 12 INCH	SQ. YD.	392	392
44200998	CLASS B PATCHES, TYPE III, 12 INCH	SQ. YD.	25	25
44201000	CLASS B PATCHES, TYPE IV, 12 INCH	SQ. YD.	31	31
44201785	CLASS D PATCHES, TYPE I, 12 INCH	SQ YD	5	5
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SQ YD	490	490
44201794	CLASS D PATCHES, TYPE III, 12 INCH	SQ YD	124	124
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SQ YD	681	681
44213100	PAVEMENT FABRIC	SQ YD	56	56
44213200	SAW CUTS	FOOT	2782	2782
48101200	AGGREGATE SHOULDERS, TYPE B	TON	5321	5321
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	60,132	60,132
64200105	SHOULDER RUMBLE STRIP	FOOT	131,490	131,490
67100100	MOBILIZATION	L SUM	1	1
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	1
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	20	20
70300230	TEMPORARY PAVEMENT MARKING - LINE 5"	FOOT	140,134	140,134
78300100	PAVEMENT MARKING REMOVAL	SQ FT	68,147	68,147
* X7800620	URETHANE PAVEMENT MARKING - LINE 5"	FOOT	163,552	163,552
Z0017100	DOWEL BARS	EACH	1200	1200
Z0075300	TIE BARS	EACH	12	12

\* SPECIALTY Item

FILE NAME :	USER NAME :	DESIGNED - KCM	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - RNH	REVISED -		SCALE: NTS	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	662	(D, DX, E, FI)	MACOUPIN	20	3
		CHECKED - TMM	REVISED -						CONTRACT NO. 72D39				
		DATE - 3-26-10	REVISED -						FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				





TYPICAL SECTION #3

STA. 48+57.56 TO STA. 50+76.23 SE TRANS.  
 STA. 50+76.23 TO STA. 63+58.99 FULL SE = 4.5%  
 STA. 63+58.99 TO STA. 65+77.66 SE TRANS.

STA. 200+21.11 TO STA. 202+53.45 SE TRANS.  
 STA. 202+53.45 TO STA. 240+08.61 FULL SE = 3.6%  
 STA. 240+08.61 TO STA. 242+40.95 SE TRANS.

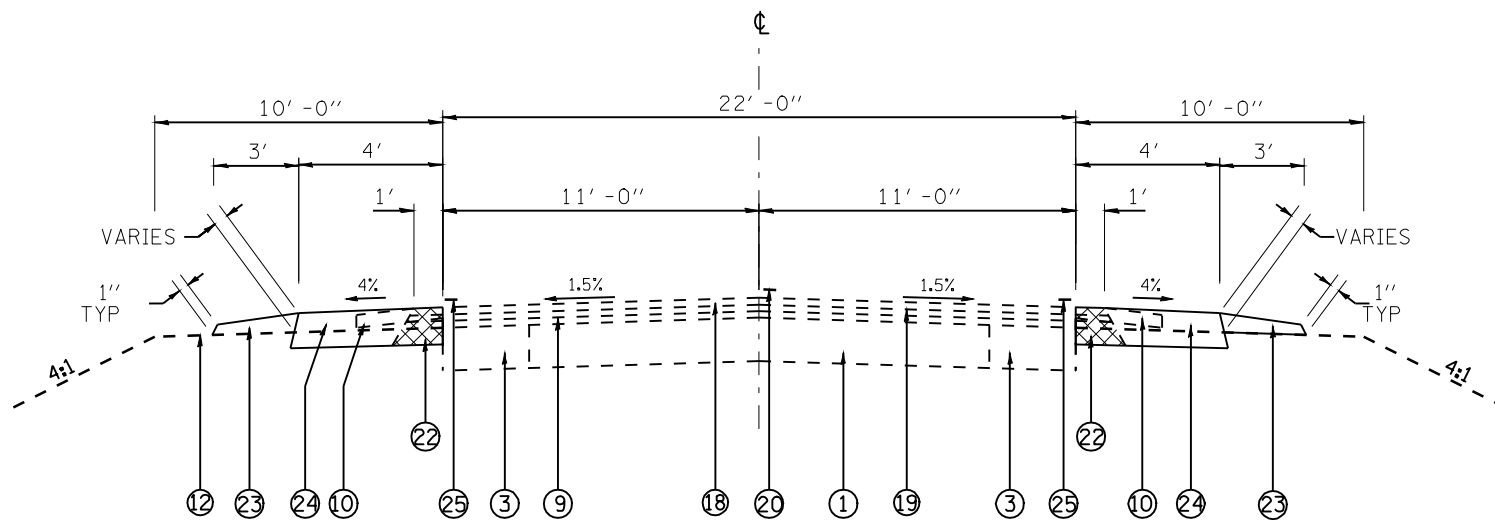
STA. 325+64.56 TO STA. 327+75.87 SE TRANS.  
 STA. 327+75.87 TO STA. 345+54.03 FULL SE = 5.2%  
 STA. 345+54.03 TO STA. 347+65.34 SE TRANS. (SEE TYPICAL SECTION #5 FOR BASE CONSTRUCTION)

STA. 399+14.16 TO STA. 401+25.47 SE TRANS  
 STA. 401+25.47 TO STA. 418+79.33 FULL SE = 5.2%  
 STA. 418+79.33 TO STA. 420+90.64 SE TRANS (SEE TYPICAL SECTION #5 FOR BASE CONSTRUCTION)

STA. 533+60.29 TO STA. 535+76.63 SE TRANS  
 STA. 535+76.63 TO STA. 551+53.76 FULL SE = 4.7%  
 STA. 551+53.76 TO STA. 553+70.1 SE TRANS (SEE TYPICAL SECTION #6 FOR BASE CONSTRUCTION)

STA. 578+15.73 TO STA. 580+27.04 SE TRANS  
 STA. 580+27.04 TO STA. 597+95.50 FULL SE = 5.2%  
 STA. 597+95.50 TO STA. 600+06.81 SE TRANS (SEE TYPICAL SECTION #9 FOR BASE CONSTRUCTION)

STA. 614+84.03 TO STA. 616+95.34 SE TRANS  
 STA. 616+95.34 TO STA. 634+20.90 FULL SE = 5.2%  
 STA. 634+20.90 TO STA. 636+36.21 SE TRANS (SEE TYPICAL SECTION #9 FOR BASE CONSTRUCTION)



TYPICAL SECTION #4

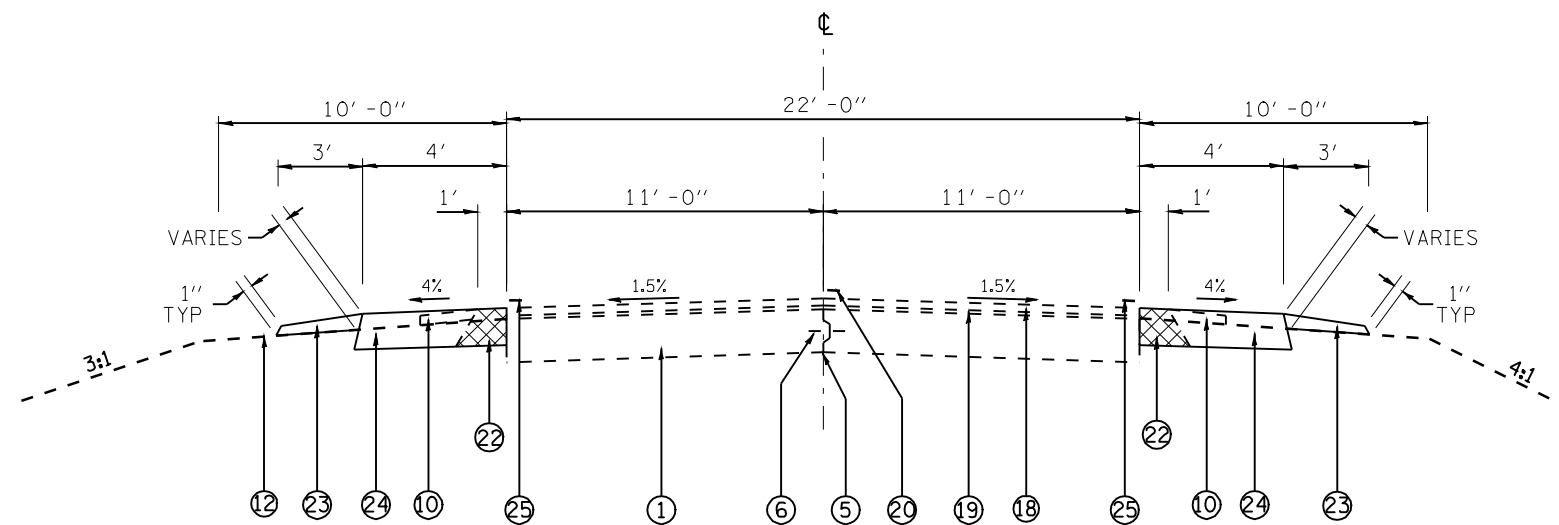
STA. 242+40.95 TO STA. 319+21  
 STA. 444+00 TO STA. 499+50  
 STA. 508+75 TO STA. 533+60.29  
 STA. 553+70.1 TO STA. 578+15.73

LEGEND

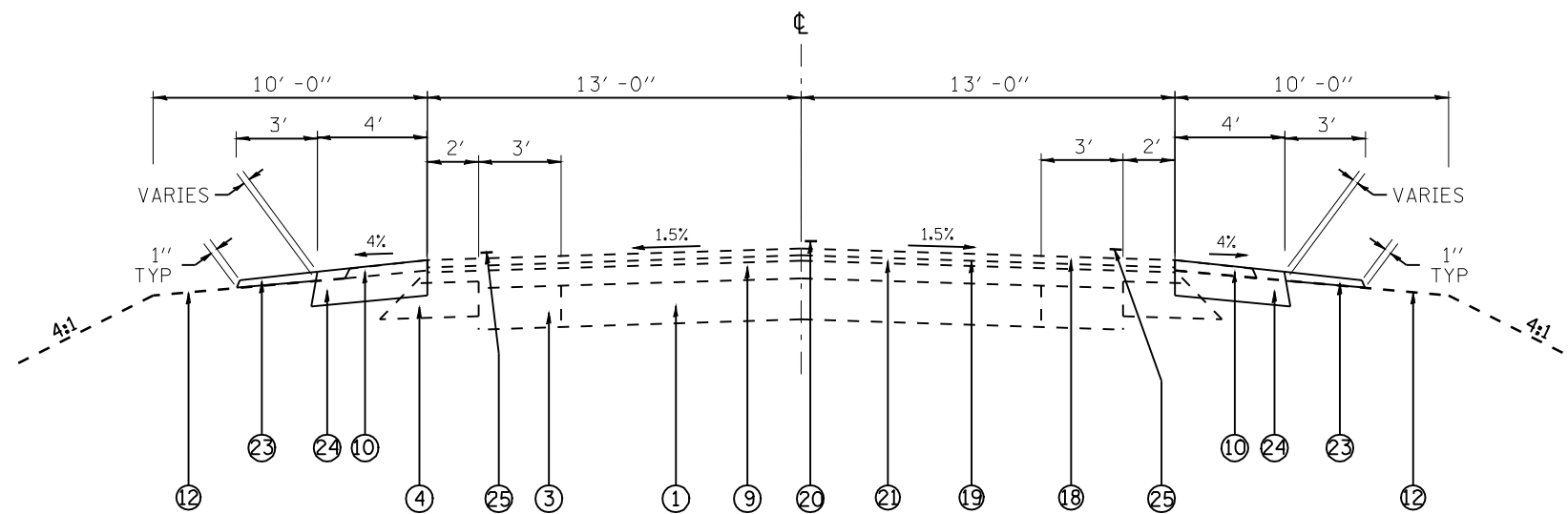
- ① EX. PCC PAVEMENT - 8"
- ② EX. PCC BASE COURSE - 8"
- ③ EX. PCC BASE COURSE WIDENING - 8"
- ④ EX. BIT. BASE COURSE WIDENING - 9"
- ④a EX. BIT. BASE COURSE - VAR. DEPTH
- ⑤ EX. LONGITUDINAL METAL JOINT
- ⑥ EX. #4 DEFORMED TIE BAR
- ⑦ EX. CONSTRUCTION JOINT (CIRC. CR 1-1/4")
- ⑧ EX. STEEL TIE BAR
- ⑨ EX. BITUMINOUS OVERLAY - 2" to 4"
- ⑩ EX. AGGREGATE SHOULDER
- ⑪ EX. BITUMINOUS SHOULDER - 8"
- ⑪a EX. PCC SHOULDER - 8"
- ⑫ EX. EARTH SHOULDER
- ⑬ EX. SUB-BASE GRANULAR MATERIAL, TYPE A
- ⑭ EX. SIDEWALK
- ⑮ EX. GUTTER, TYPE B
- ⑯ EX. COMBINATION CONCRETE CURB & GUTTER
- ⑰ EX. PCC CURB
- ⑱ EX. BITUMINOUS CONC. SURF. COURSE, SUPERPAVE, MIX C, N50, 1-1/2"
- ⑲ EX. LEVELING BINDER (MACHINE METHOD), SUPERPAVE, N50, 3/4 "
- ⑳ PROPOSED PAVEMENT MARKING LINE - 5"
- ㉑ EX. LEVELING BINDER (MACHINE METHOD), SUPERPAVE, N50, 1"
- ㉒ REMOVE EX. BITUMINOUS SHOULDER WIDENING 8", SUPERPAVE, N30 AND SURFACE COURSE
- ㉓ PROPOSED AGGREGATE SHOULDER, TYPE B
- ㉔ PROPOSED HOT-MIX ASPHALT SHOULDERS - 8" (WITH SHOULDER RUMBLE STRIP)
- ㉕ PROPOSED PAVEMENT MARKING LINE - 5"
- ㉖ PROPOSED SHOULDER RUMBLE STRIP







TYPICAL SECTION #9  
 STA. 600+06.81 TO STA. 614+84.03



TYPICAL SECTION #10  
 STA. 636+36.21 TO STA. 650+69.37  
 STA. 2191+15.89 TO STA. 2194+28.3

STA. EQUATION  
 STA. 650+69.37 BK = STA. 2191+15.89 AH

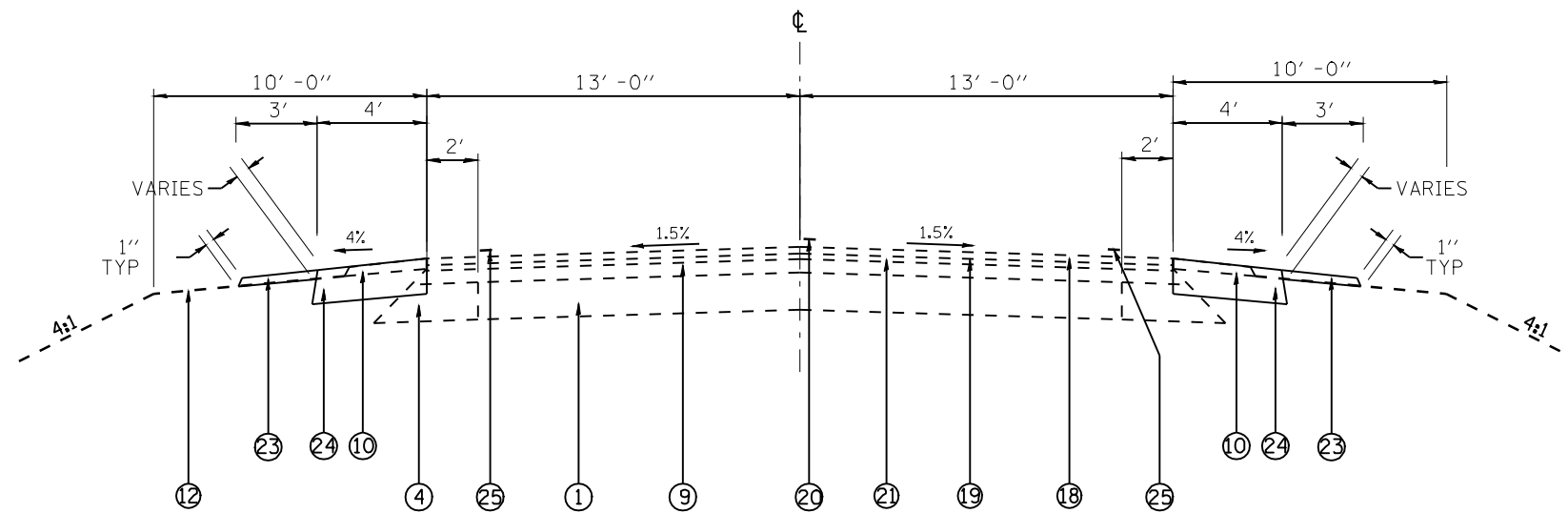
LEGEND

- ① EX. PCC PAVEMENT - 8"
- ② EX. PCC BASE COURSE - 8"
- ③ EX. PCC BASE COURSE WIDENING - 8"
- ④ EX. BIT. BASE COURSE WIDENING - 9"
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- ㉖ PROPOSED SHOULDER RUMBLE STRIP

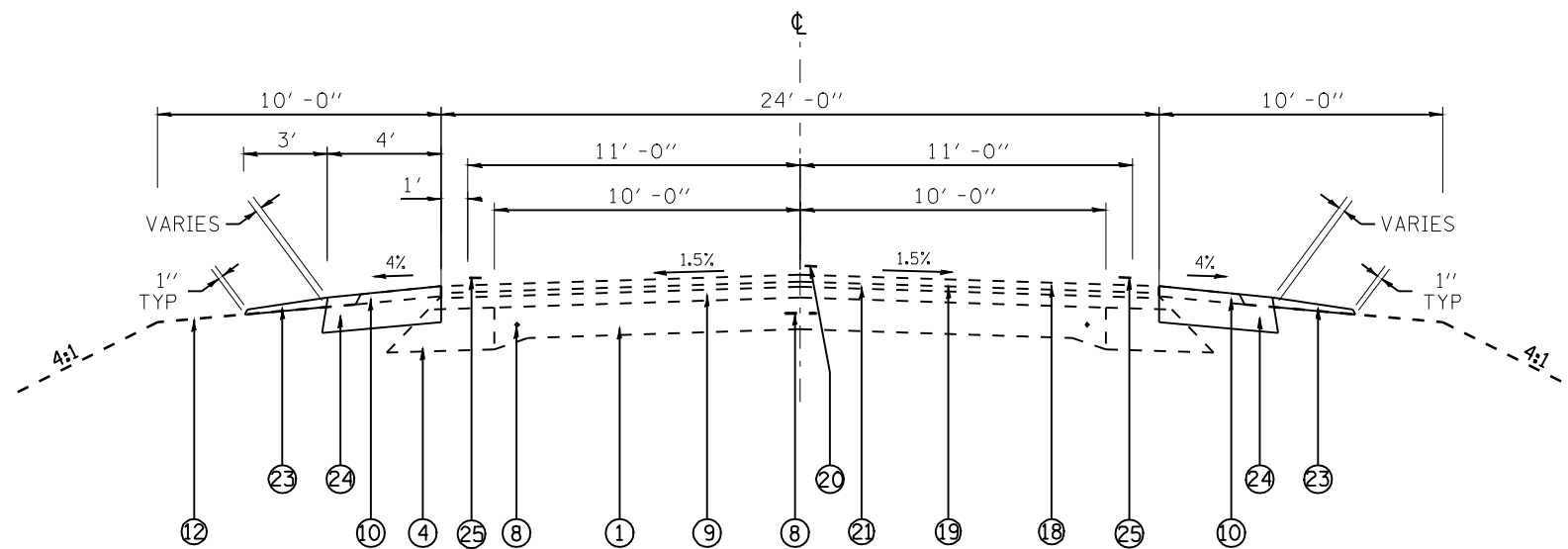
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		DRAWN - JLM	REVISED -		SCALE: NTS	SHEET NO. 5 OF 8 SHEETS	STA. TO STA.	CONTRACT NO. 72D39				
		CHECKED - TMM	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE - 3-26-10	REVISED -									







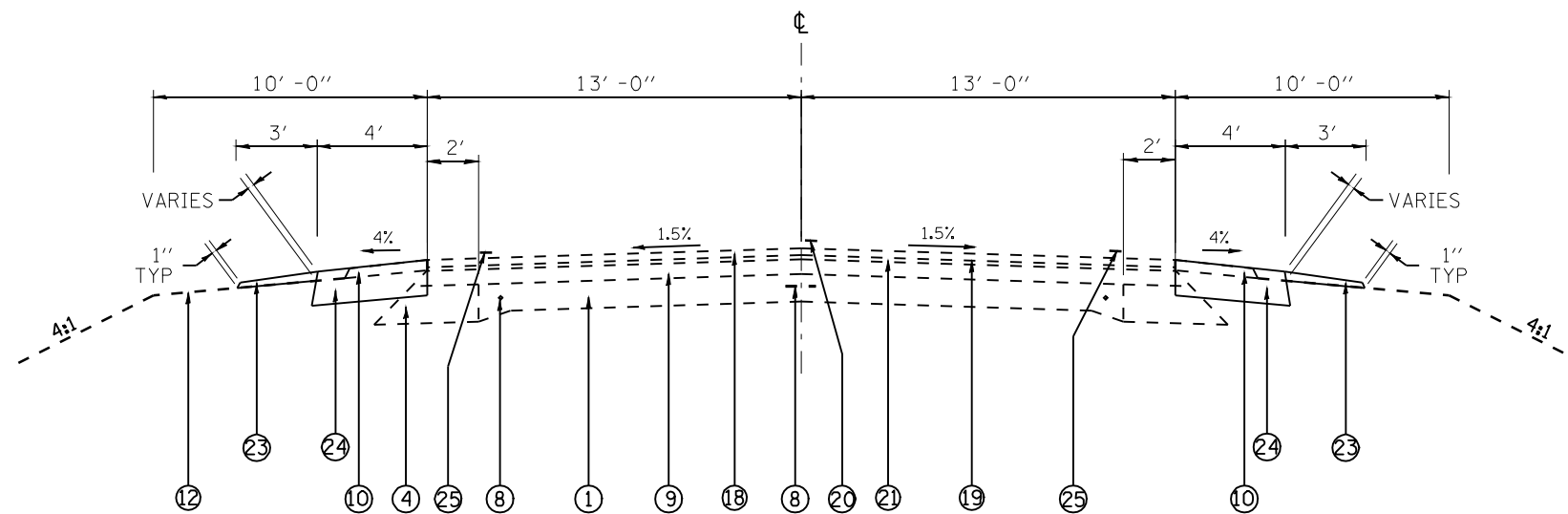
**TYPICAL SECTION #13**  
 STA. 2233+19.14 TO STA. 2233+88.4



**TYPICAL SECTION #14**  
 STA. 6+22.2 TO STA. 9+94.3

**LEGEND**

- ① EX. PCC PAVEMENT - 8"
- ② EX. PCC BASE COURSE - 8"
- ③ EX. PCC BASE COURSE WIDENING - 8"
- ④ EX. BIT. BASE COURSE WIDENING - 9"
- ④a EX. BIT. BASE COURSE - VAR. DEPTH
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- ⑰ EX. PCC CURB
- ⑱ EX. BITUMINOUS CONC. SURF. COURSE, SUPERPAVE, MIX C, N50, 1-1/2"
- ⑲ EX. LEVELING BINDER (MACHINE METHOD), SUPERPAVE, N50, 3/4 "
- ⑳ PROPOSED PAVEMENT MARKING LINE - 5"
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- ㉒ EX. BITUMINOUS SHOULDER WIDENING 8", SUPERPAVE, N30
- ㉓ PROPOSED AGGREGATE SHOULDER, TYPE B
- ㉔ PROPOSED HOT-MIX ASPHALT SHOULDERS - 8" (WITH SHOULDER RUMBLE STRIP)
- ㉕ PROPOSED PAVEMENT MARKING LINE - 5"
- ㉖ PROPOSED SHOULDER RUMBLE STRIP



TYPICAL SECTION #15

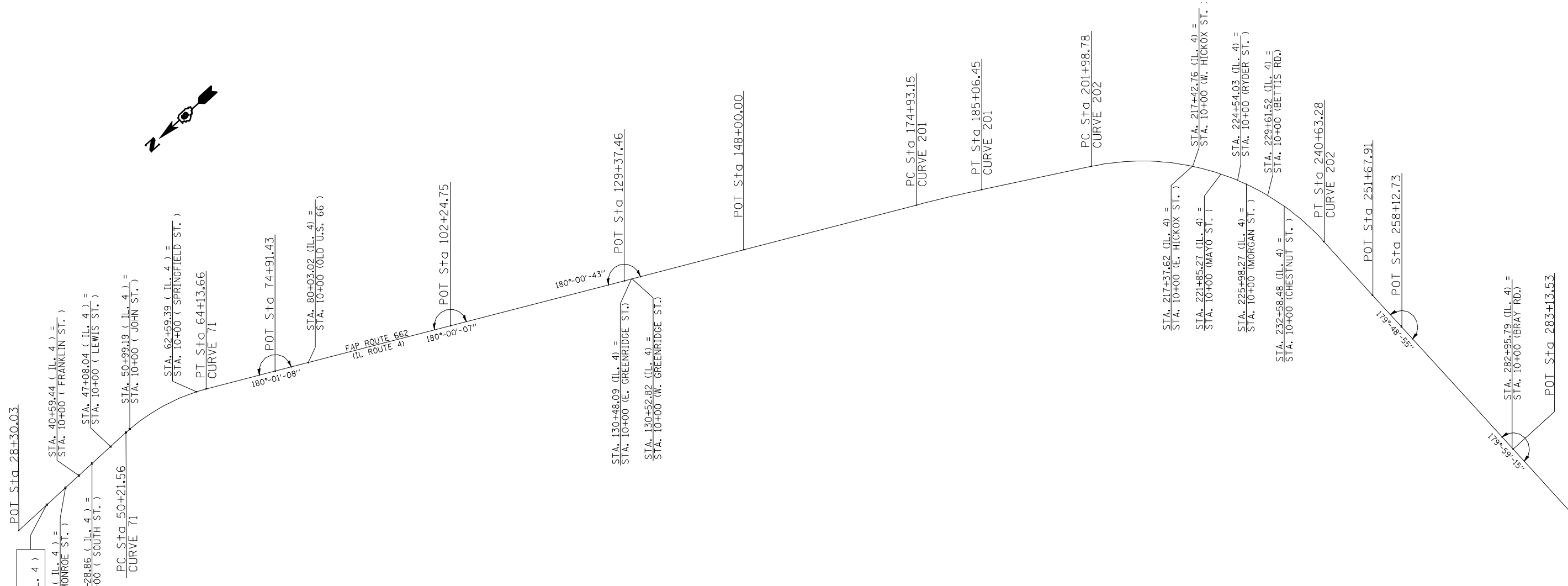
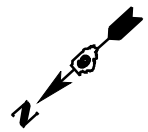
STA. 9+94.3 TO STA. 32+98

LEGEND

- ① EX. PCC PAVEMENT - 8"
- ② EX. PCC BASE COURSE - 8"
- ③ EX. PCC BASE COURSE WIDENING - 8"
- ④ EX. BIT. BASE COURSE WIDENING - 9"
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- ㉖ PROPOSED SHOULDER RUMBLE STRIP

FILE NAME =	USER NAME =	DESIGNED - KCM	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SECTIONS</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JLM	REVISED -		662	(D, DX, E, F)I	MACOUPIN	20	11			
		CHECKED - TMM	REVISED -		CONTRACT NO. 72D39							
		DATE - 3-26-10	REVISED -		SCALE: NTS	SHEET NO. 8 OF 8 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			





EXIST. CURVE 201  
 PI STA. = 179+99.88  
 $\Delta = 2^\circ 31' 21''$  (RT)  
 $D = 0^\circ 14' 56''$   
 $R = 23,016.96'$   
 $T = 506.73'$   
 $L = 1,013.30'$   
 $E = 5.58'$   
 $e = \text{NORMAL}$   
 $T.R. = \text{-----}$   
 $S.E. RUN = \text{-----}$   
 $P.C. STA. = 174+93.15$   
 $P.T. STA. = 185+06.45$

EXIST. CURVE 202  
 PI STA. = 223+27.93  
 $\Delta = 59^\circ 48' 10''$  (RT)  
 $D = 1^\circ 32' 51''$   
 $R = 3,702.50'$   
 $T = 2,129.15'$   
 $L = 3,864.50'$   
 $E = 568.54'$   
 $e = 3.6\%$   
 $T.R. = 39.13'$   
 $S.E. RUN = 203.13'$   
 $P.C. STA. = 201+98.78$   
 $P.T. STA. = 240+63.28$   
 SE TRANSITION  
 RT. STA 201+57.78 TO RT. STA 202+53.45  
 LT. STA 200+21.11 TO LT. STA 202+53.45  
 RT. STA 240+08.61 TO RT. STA 241+04.28  
 LT. STA 240+08.61 TO LT. STA 242+40.95

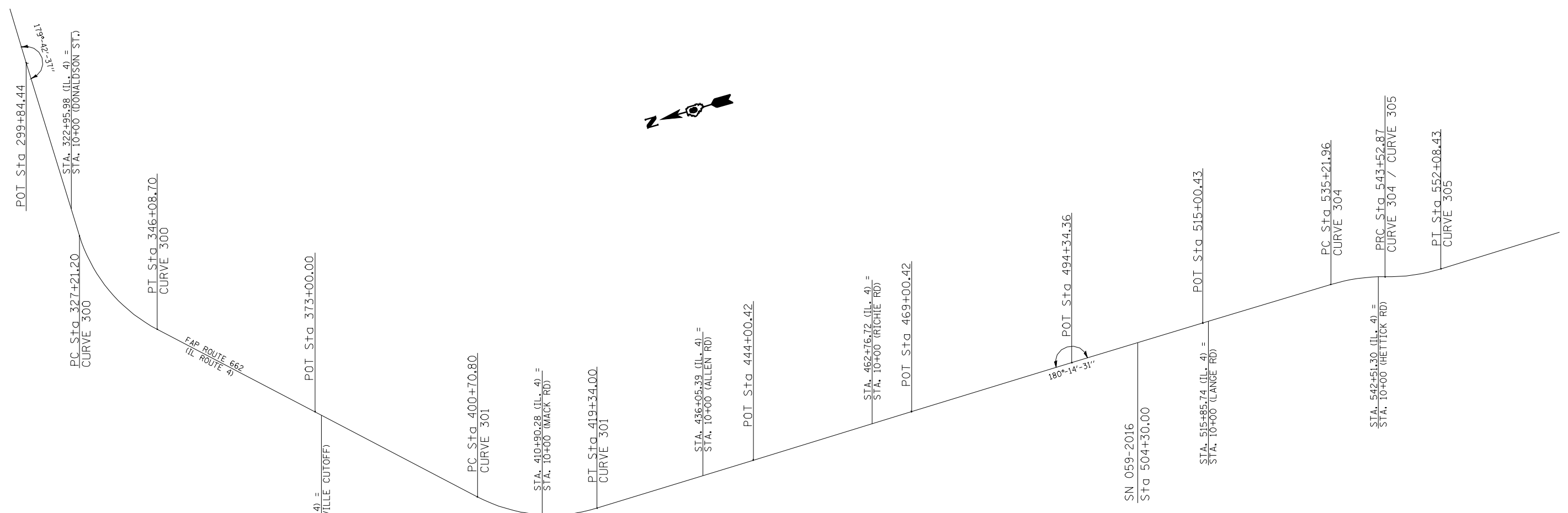
FILE NAME =	USER NAME = laughlinr1	DESIGNED - KCM	REVISED -
e:\pwwork\pwwork\LAUGHLINRL\10194809\10194809-72039-013-015-alignment.dgn		DRAWN - JLM	REVISED -
PLOT SCALE = 1600,0000 "/ IN.		CHECKED - TMM	REVISED -
PLOT DATE = Mar-30-2010 09:51:49AM		DATE - 3-26-10	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ALIGNMENT LAYOUT**

SCALE: 1"=800'      SHEET NO. 1 OF 3 SHEETS      STA. 34+04.00 TO STA. 295+95.87

F.S.P. RTE. 662	SECTION (D,DX,E,F) I	COUNTY MACOUPIN	TOTAL SHEETS 20	SHEET NO. 13
CONTRACT NO. 72039			ILLINOIS FED. AID PROJECT	



EXIST. CURVE 300  
 PI STA. = 337+17.36  
 $\Delta = 45^\circ 17' 06''$  (LT)  
 $D = 2^\circ 23' 57''$   
 $R = 2,388.11'$   
 $T = 996.16'$   
 $L = 1,887.50'$   
 $E = 199.44'$   
 $e = 5.2\%$   
 $T.R. = 39.13'$   
 $S.E. RUN = 203.13'$   
 $P.C. STA. = 327+21.20$   
 $P.T. STA. = 346+08.70$   
 SE TRANSITION  
 RT. STA 325+64.56 TO RT. STA 327+75.87  
 LT. STA 326+59.17 TO LT. STA 327+75.87  
 RT. STA 345+54.03 TO RT. STA 347+65.34  
 LT. STA 345+54.03 TO LT. STA 346+70.73

STA. 374+10.20 (IL. 4) =  
 STA. 10+00 (CARLINVILLE CUTOFF)

EXIST. CURVE 301  
 PI STA. = 410+52.68  
 $\Delta = 44^\circ 41' 03''$  (LT)  
 $D = 2^\circ 23' 54''$   
 $R = 2,389.07'$   
 $T = 981.88'$   
 $L = 1,863.20'$   
 $E = 193.90'$   
 $e = 5.2\%$   
 $T.R. = 39.13'$   
 $S.E. RUN = 203.13'$   
 $P.C. STA. = 400+70.80$   
 $P.T. STA. = 419+34.00$   
 SE TRANSITION  
 RT. STA 399+14.16 TO RT. STA 401+25.47  
 LT. STA 400+08.77 TO LT. STA 401+25.47  
 RT. STA 418+79.33 TO RT. STA 420+90.64  
 LT. STA 418+79.33 TO LT. STA 419+96.03

EXIST. CURVE 304  
 PI STA. = 539+40.70  
 $\Delta = 17^\circ 34' 26''$  (RT)  
 $D = 2^\circ 06' 54''$   
 $R = 2,708.97'$   
 $T = 418.74'$   
 $L = 830.91'$   
 $E = 32.17'$   
 $e = 4.7\%$   
 $T.R. = 39.13'$   
 $S.E. RUN = 203.13'$   
 $P.C. STA. = 535+21.96$   
 $P.T. STA. = 543+52.87$   
 SE TRANSITION  
 RT. STA 534+64.97 TO RT. STA 535+76.63  
 LT. STA 533+60.29 TO LT. STA 535+76.63  
 RT. STA 542+98.20 TO RT. STA 544+09.86  
 LT. STA 542+98.20 TO LT. STA 545+14.54

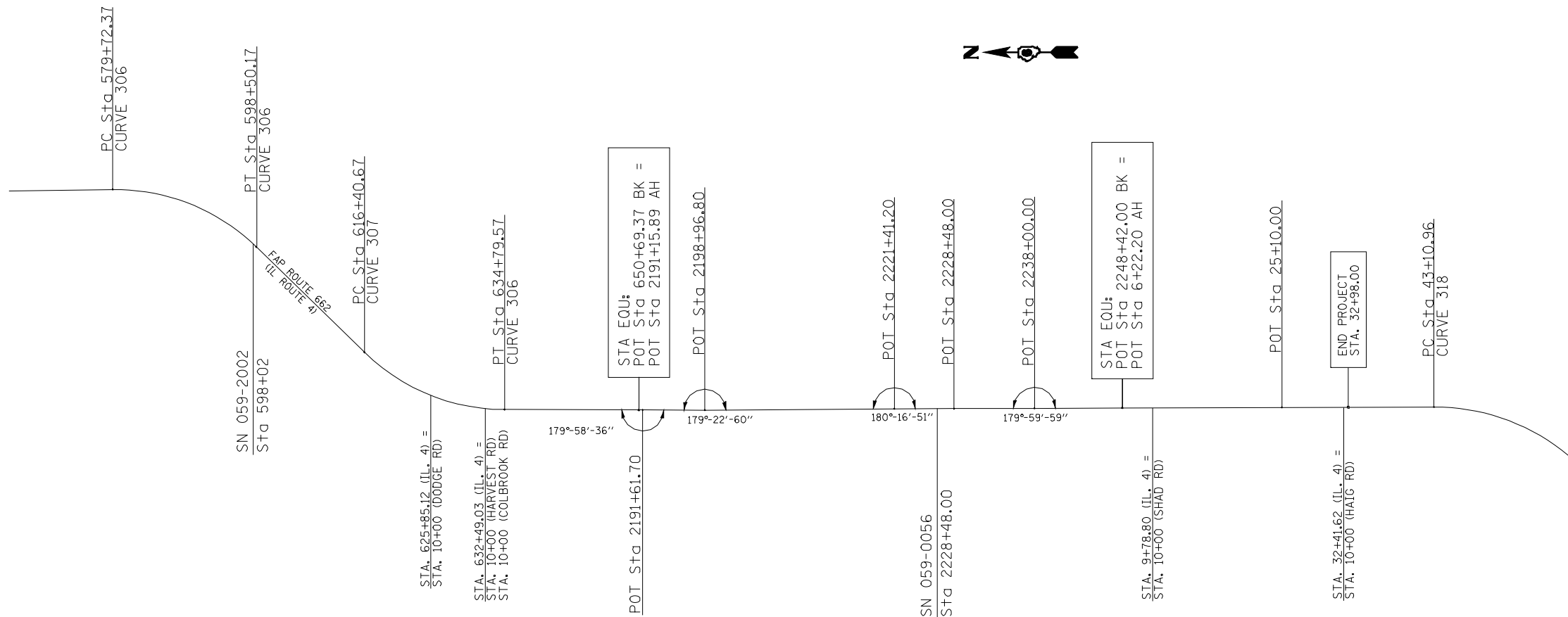
EXIST. CURVE 305  
 PI STA. = 547+84.19  
 $\Delta = 17^\circ 58' 00''$  (LT)  
 $D = 2^\circ 06' 00''$   
 $R = 2,728.38'$   
 $T = 431.32'$   
 $L = 855.56'$   
 $E = 33.88'$   
 $e = 4.7\%$   
 $T.R. = 39.13'$   
 $S.E. RUN = 203.13'$   
 $P.C. STA. = 543+52.87$   
 $P.T. STA. = 552+08.43$   
 SE TRANSITION  
 RT. STA 541+91.20 TO RT. STA 544+07.54  
 LT. STA 542+95.88 TO LT. STA 544+07.54  
 RT. STA 551+53.76 TO RT. STA 553+70.10  
 LT. STA 551+53.76 TO LT. STA 552+65.42

FILE NAME =	USER NAME = laughlinr1	DESIGNED - KCM	REVISED -
es:\pwwork\pwidot\LAUGHLINRL\14809\14809-72039-013-015-alignment.dgn		DRAWN - JLM	REVISED -
PLOT SCALE = 1600.0000 ' / IN.		CHECKED - TMM	REVISED -
PLOT DATE = Mar-30-2010 09:51:51AM		DATE - 3-26-10	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>ALIGNMENT LAYOUT</b>		
SCALE: 1"=800'	SHEET NO. 2 OF 3 SHEETS	STA. 295+95.87 TO STA. 570+84.78

F.S.P. RTE. 662	SECTION (D,DX,E,F) I	COUNTY MACOUPIN	TOTAL SHEETS 20	SHEET NO. 14
CONTRACT NO. 72039			ILLINOIS FED. AID PROJECT	

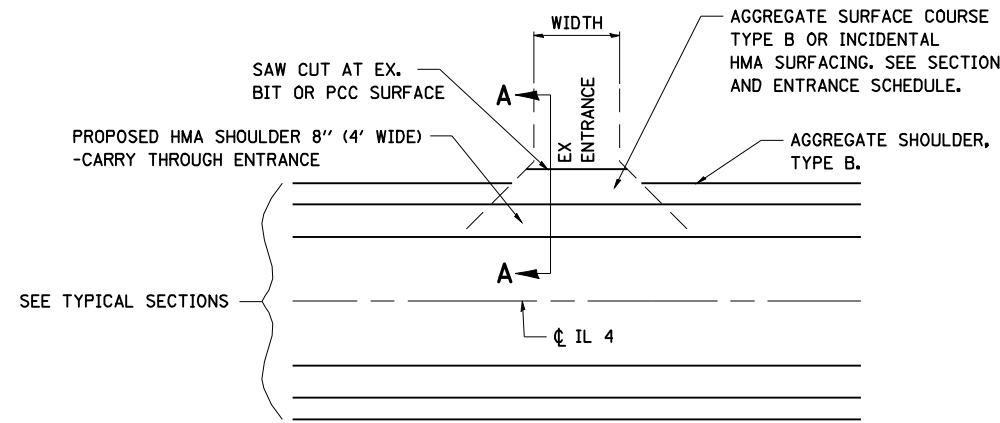


EXIST. CURVE 306  
 PI STA. = 589+62.78  
 $\Delta$  = 45° 01' 51" (RT)  
 D = 2° 23' 53"  
 R = 2,389.25'  
 T = 990.41'  
 L = 1,877.80'  
 E = 197.14'  
 $e$  = 5.2%  
 T.R. = 39.13'  
 S.E. RUN = 203.13'  
 P.C. STA. = 579+72.37  
 P.T. STA. = 598+50.17  
 SE TRANSITION  
 RT. STA 579+10.34 TO RT. STA 580+27.04  
 LT. STA 578+15.73 TO LT. STA 580+27.04  
 RT. STA 597+95.50 TO RT. STA 599+12.20  
 LT. STA 597+95.50 TO LT. STA 600+06.81

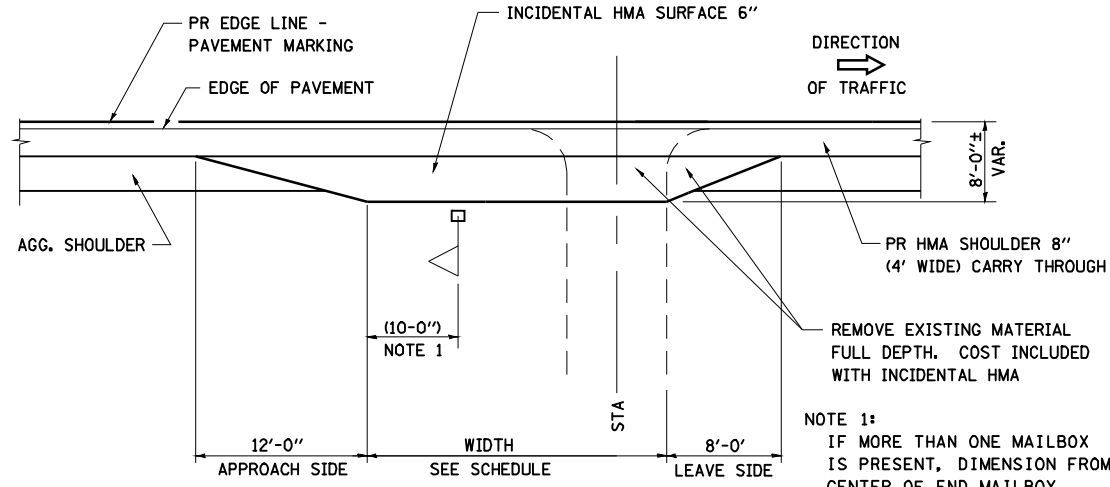
EXIST. CURVE 307  
 PI STA. = 626+08.37  
 $\Delta$  = 44° 06' 03" (LT)  
 D = 2° 23' 54"  
 R = 2,389.10'  
 T = 967.70'  
 L = 1,838.90'  
 E = 188.54'  
 $e$  = 5.2%  
 T.R. = 39.13'  
 S.E. RUN = 203.13'  
 P.C. STA. = 616+40.67  
 P.T. STA. = 634+79.57  
 SE TRANSITION  
 RT. STA 614+84.03 TO RT. STA 616+95.34  
 LT. STA 615+78.64 TO LT. STA 616+95.34  
 RT. STA 634+24.90 TO RT. STA 636+36.21  
 LT. STA 634+20.90 TO LT. STA 635+41.60

EXIST. CURVE 318  
 PI STA. = 52+67.43  
 $\Delta$  = 42° 05' 58" (RT)  
 D = 2° 18' 20"  
 R = 2,485.25'  
 T = 956.48'  
 L = 1,826.10'  
 E = 177.70'  
 $e$  = 5.0%  
 T.R. = 39.13'  
 S.E. RUN = 203.13'  
 P.C. STA. = 43+10.96  
 P.T. STA. = 61+37.06  
 SE TRANSITION  
 RT. STA 42+50.83 TO RT. STA 43+65.63  
 LT. STA 41+52.43 TO LT. STA 43+65.63  
 RT. STA 60+82.39 TO RT. STA 61+97.19  
 LT. STA 60+82.39 TO LT. STA 62+95.59

FILE NAME =	USER NAME = laughlinr1	DESIGNED - KCM	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ALIGNMENT LAYOUT</b>			F.S.P. RTE. 662	SECTION (D,DX,E,F) I	COUNTY MACOUPIN	TOTAL SHEETS 20	SHEET NO. 15
e:\pwwork\pwidot\LAUGHLINRL\0194809\06-72039-013-015-alignment.dgn	DRAWN - JLM	REVISED -	REVISED -		SCALE: 1"=800'	SHEET NO. 3 OF 3 SHEETS	STA. 570+84.78 TO STA. 32+98.00	CONTRACT NO. 72039				
PLOT SCALE = 1600.0000 "/ IN.	CHECKED - TMM	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT							
PLOT DATE = Mar-30-2010 09:51:53AM	DATE - 3-26-10	REVISED -	REVISED -									



**ENTRANCE DETAIL PLAN VIEW**



**PLAN - MAILBOX TURNOUT (WITH OR WITHOUT ENTRANCE)**

**GENERAL NOTES:**

THE RESIDENT ENGINEER WILL DETERMINE THE EXACT TYPE OF IMPROVEMENT TO BE COMPLETED FOR ALL ENTRANCES, SIDEROADS AND MAILBOX TURNOUTS ON THIS PROJECT.

THE PLAN DETAILS AND SCHEDULES SHOULD BE USED AS A GUIDE FOR THE ENGINEER TO IMPLEMENT THE FINAL DESIGN. THE ENGINEER MAY DECIDE TO SALVAGE PORTIONS OF THE EXISTING ENTRANCE PAVEMENT STRUCTURE; THEREFORE, REDUCING PAY ITEM QUANTITIES. NO ADDITIONAL PAYMENT WILL BE ALLOWED FOR THIS REDUCTION IN QUANTITIES.

ANY WORK THE ENGINEER REQUIRES WHICH IS NOT COVERED BY A PAY ITEM CONTAINED IN THE PLANS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

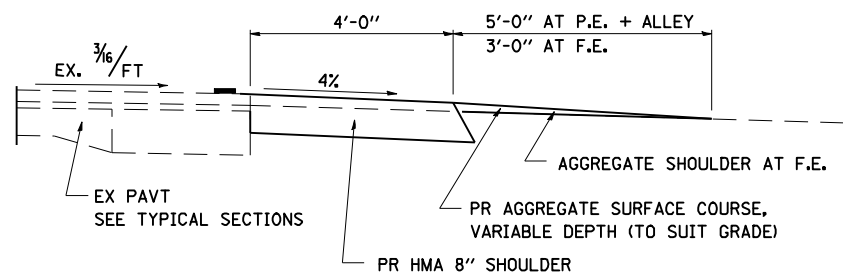
BITUMINOUS CONCRETE REQUIRED TO CONSTRUCT THE ENTRANCES SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 406 AND 408 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.

WHEN THE BITUMINOUS CONCRETE PROPOSED FOR THE IMPROVEMENT IS THICKER THAN 75 mm (3 INCHES) AND REQUIRE PLACEMENT IN MORE THAN ONE LIFT. THE BOTTOM LIFT(S) SHALL MEET THE REQUIREMENTS OF BITUMINOUS BASE COURSE IN SECTION 406 OF THE STANDARD SPECIFICATIONS AND THE TOP LIFT OF 50 mm (2 INCHES) SHALL MEET THE REQUIREMENTS OF BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE.

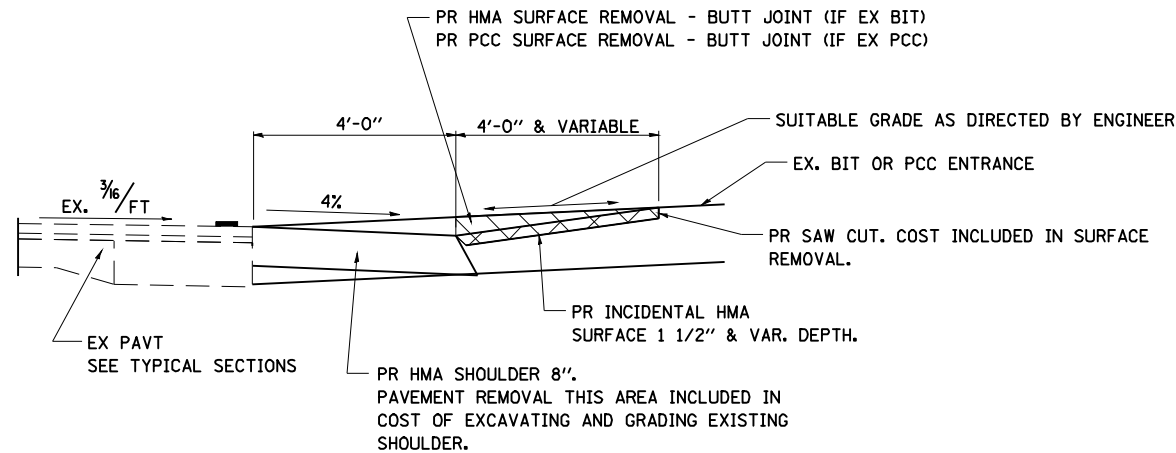
THIS WORK WILL BE PAID FOR IN ACCORDANCE WITH SECTIONS 351, 406, 408, AND 440 OF THE STANDARD SPECIFICATIONS.

PREPARATION OF BASE SHALL BE INCLUDED IN THE COST OF INCIDENTAL HMA SURFACE.

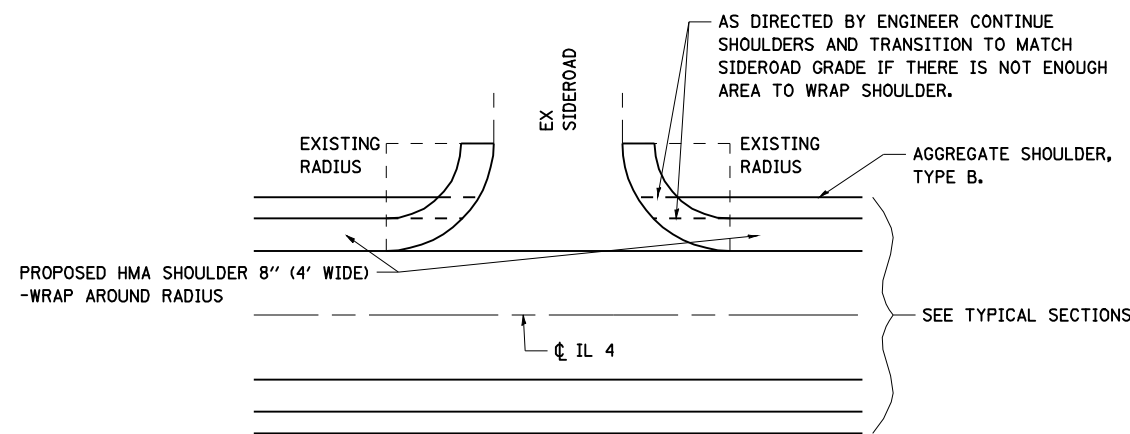
ALL DIMENSIONS ARE IN FEET-INCHES" UNLESS OTHERWISE SHOWN.



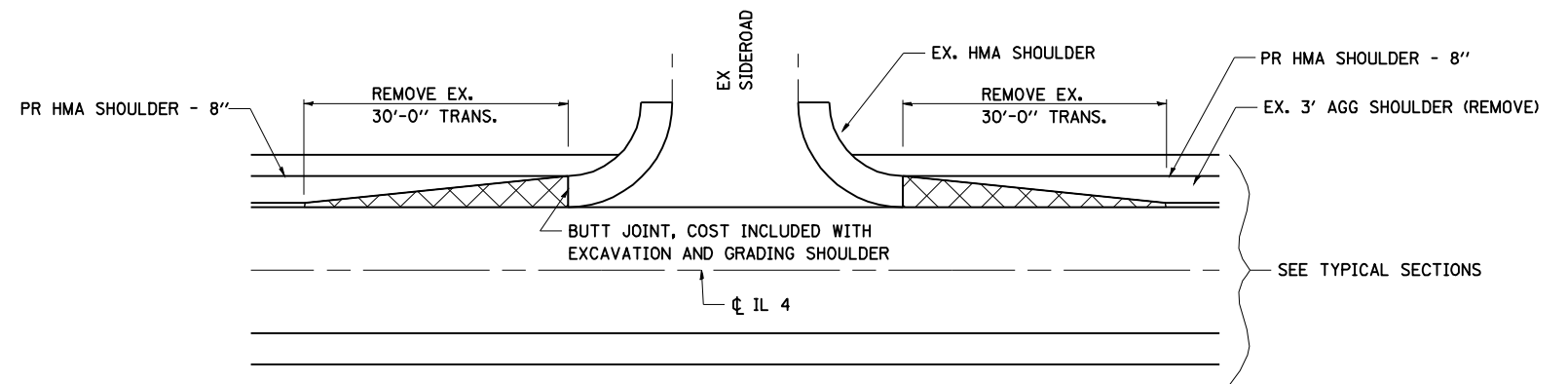
**SECTION A-A FOR EX AGGREGATE ENTRANCES**



**SECTION A-A AT EX BIT OR PCC ENTRANCE**



**SIDEROAD DETAIL PLAN VIEW**



**SIDEROADS WITH EXISTING SHOULDER RADII**

FILE NAME =	USER NAME = laughlinr1	DESIGNED - KCM	REVISED -
er\pwwork\pwwid\DOT\LAUGHLINRL\0194809\06-72039-016-ent.details.dgn		DRAWN - RNH	REVISED -
	PLOT SCALE = 40.000' / IN.	CHECKED - TMM	REVISED -
	PLOT DATE = Mar-30-2010 09:51:55AM	DATE - 3-26-10	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR RURAL/URBAN ENT., MAILBOX  
TURNOUT & SIDEROADS**

SCALE: SHEET NO. 1 OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
662	(D, DX, E, F, I)	MACOUPIN	20	16
CONTRACT NO. 72039				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



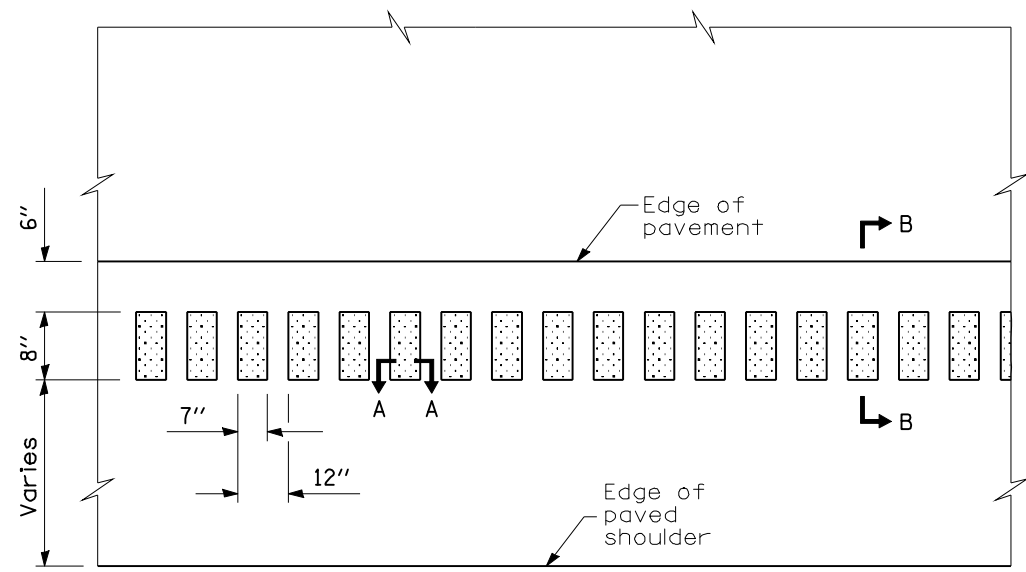
ENTRANCE IMPROVEMENT SCHEDULE FOR RURAL / URBAN "PPP" PROJECTS															
LOCATION	TYPE OF ENTRANCE	EX MATERIAL TYPE	WIDTH	WIDTH AT MAILBOX	LT OFFSET (NOT USED)	LENGTH (FROM EDGE OF PVT/ BIT SHLD TO LIMITS OF IMPROVEMENT)	PR HMA CONC. THICKNESS	HMA SURF. REM. - BUTT JOINT	P. C. C. SURF. REM. - BUTT JOINT	PREP OF BASE	AGG. BASE REPAIR	AGGREGATE SURFACE COURSE TY - B	BIT (PRIME COAT)	AGG (PRIME COAT)	INCIDENTAL HMA SURF.
(LT / RT) (STA) ( + )	(FE / PE / CE / MB) (RURAL / URBAN)	(EARTH / AGG. / BIT. / P. C. C.)	FOOT	FOOT	FOOT	FOOT	INCH	SQ. YD.	SQ. YD.	SQ. YD.	TON	TON	TON	TON	TON
(RT) STA. 45+20	ALLEY	AGG.	20.0			5.0						2.0			
(RT) STA. 47+08.04	SR	BIT.	60.0			4.0	1.5	28					0.0108	0.0405	2.39
(RT) STA. 50+99.19	SR	BIT.	40.0			4.0	1.5	20					0.0074	0.0279	1.64
(RT) STA. 62+59.39	SR	BIT.	40.0			4.0	1.5	20					0.0074	0.0279	1.64
(RT) STA. 74+27	FE/RURAL	EARTH	*												
(RT & LT) STA. 80+03.02	SR	BIT.	110.0			4.0	1.5	101					0.0385	0.1444	8.51
(RT) STA. 88+00	FE/RURAL	EARTH	*												
(RT) STA. 100+88	FE/RURAL	EARTH	*												
(RT) STA. 121+00	FE/RURAL	EARTH	*												
(RT & LT) STA. 130+48.09	SR	BIT.	50.0			4.0	1.5	48					0.0182	0.0684	4.03
(RT) STA. 163+87	FE/RURAL	EARTH	*												
(RT) STA. 190+93	CE	BIT.	65.0			4.0	1.5	31					0.0117	0.0437	2.58
(RT & LT) STA. 208+22	FE/RURAL	EARTH	*												
(RT) STA. 216+70	FE/RURAL	EARTH	*												
(RT) STA. 217+42	SR	BIT.	30.0			4.0	1.5	15					0.0057	0.0215	1.27
(LT) STA. 217+42	SR	BIT.	60.0			4.0	1.5	28					0.0108	0.0405	2.39
(RT) STA. 220+50	PE	BIT.	16.0			4.0	1.5	9					0.0034	0.0127	0.75
(RT) STA. 221+15	PE	BIT.	18.0			4.0	1.5	10					0.0037	0.0139	0.82
(LT) STA. 221+85.27	SR	BIT.	26.0			4.0	1.5	13					0.0051	0.0190	1.12
(RT) STA. 224+54.03	SR	BIT.	34.0			4.0	1.5	17					0.0064	0.0241	1.42
(LT) STA. 224+54.03	SR	BIT.	46.0			4.0	1.5	22					0.0084	0.0317	1.87
(LT) STA. 225+98.27	SR	BIT.	68.0			4.0	1.5	32					0.0122	0.0456	2.69
(RT) STA. 229+61.52	SR	BIT.	32.0			4.0	1.5	16					0.0061	0.0228	1.34
(LT) STA. 230+80	PE	BIT.	24.0			4.0	1.5	12					0.0047	0.0177	1.05
(LT) STA. 232+58.48	SR	BIT.	60.0			4.0	1.5	28					0.0108	0.0405	2.39
(LT) STA. 243+10	FE/RURAL	EARTH	*												
(RT) STA. 256+40	FE/RURAL	EARTH	*												
(LT) STA. 256+82	FE/RURAL	EARTH	*												
(LT) STA. 269+65	FE/RURAL	EARTH	*												
(RT) STA. 270+00	FE/RURAL	EARTH	*												
(LT) STA. 276+12	PE/MB	AGG.				4.0	6.0								8.36
(LT) STA. 282+95.79	SR	BIT.	**	46.0											
(LT) STA. 302+60	PE/MB	BIT.				4.0	6.0								8.66
(RT) STA. 303+25	FE/RURAL	EARTH	*	48.0											
(LT) STA. 309+90	FE/RURAL	EARTH	*												
(RT) STA. 316+37	FE/RURAL	EARTH	*												
(RT) STA. 322+95.98	SR	BIT.	**												
(LT) STA. 322+95.98	SR	BIT.	**												
(RT) STA. 347+00	FE/RURAL	EARTH	*												
(LT) STA. 347+15	FE/RURAL	EARTH	*												
(RT) STA. 354+35	FE/RURAL	EARTH	*												
(RT) STA. 370+00	FE/RURAL	EARTH	*												
(RT) STA. 374+10.2	SR	BIT.	**												
(LT) STA. 374+10.2	SR	BIT.	**												
(RT & LT) STA. 399+00	FE/RURAL	EARTH	*												
(RT) STA. 410+90.28	SR	BIT.	**												
(LT) STA. 436+05.39	SR	BIT.	**												
TOTAL THIS SHEET=	-	-						450				2.0	0.1713	0.6428	54.92

NOTES:  
 \* FIELD ENTRANCES SHOWN FOR INFORMATIONAL PURPOSES ONLY.  
 PROPOSED AGGREGATE SHOULDER, TYPE B TO CONTINUE THROUGH FIELD ENTRANCES.  
 \*\* SIDEROADS WITH EXISTING SHOULDER RADII. SEE DETAIL.

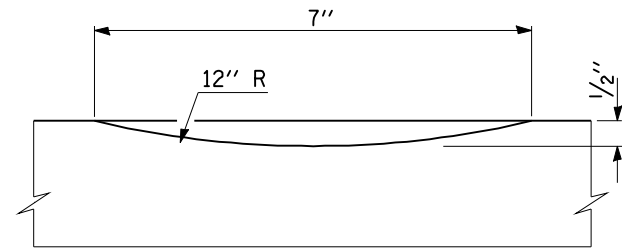
ENTRANCE IMPROVEMENT SCHEDULE FOR RURAL / URBAN "PPP" PROJECTS															
LOCATION	TYPE OF ENTRANCE	EX MATERIAL TYPE	WIDTH	WIDTH AT MAILBOX	LT OFFSET (NOT USED)	LENGTH (FROM EDGE OF PVT/ BIT SHLD TO LIMITS OF IMPROVEMENT)	PR HMA CONC. THICKNESS	HMA SURF. REM. - BUTT JOINT	P. C. C. SURF. REM. - BUTT JOINT	PREP OF BASE	AGG. BASE REPAIR	AGGREGATE SURFACE COURSE TY - B	BIT (PRIME COAT)	AGG (PRIME COAT)	INCIDENTAL HMA SURF.
(LT / RT) (STA) ( + )	(FE / PE / CE / MB) (RURAL / URBAN)	(EARTH / AGG. / BIT. / P. C. C.)	FOOT	FOOT	FOOT	FOOT	INCH	SQ. YD.	SQ. YD.	SQ. YD.	TON	TON	TON	TON	TON
(RT) STA. 449+55	FE/RURAL	EARTH	*												
(LT) STA. 451+10	FE/RURAL	EARTH	*												
(RT) STA. 454+00	PE	BIT.	26.0			4.0	1.5	13				0.0051	0.0190	1.12	
(RT) STA. 462+76	SR	BIT.	**												
(LT) STA. 473+05	PE/MB	BIT.		70.0		4.0	6.0								11.95
(RT) STA. 487+60	CE	PCC	48.0			4.0	1.5		23			0.0088	0.0329	1.94	
(LT) STA. 489+10	FE/RURAL	EARTH	*												
(LT) STA. 494+20	FE/RURAL	EARTH	*												
(LT) STA. 498+35	FE/RURAL	EARTH	*												
(LT) STA. 499+80	FE/RURAL	EARTH	*												
(LT) STA. 505+95	FE	BIT.	34.0			4.0	1.5	17				0.0064	0.0241	1.42	
(LT) STA. 507+40	PE/MB	BIT.		46.0		4.0	6.0	2							8.36
(RT) STA. 513+95	FE/RURAL	EARTH	*												
(RT) STA. 515+85.74	SR	BIT.	**												
(RT) STA. 529+00	FE/RURAL	EARTH	*												
(LT) STA. 531+50	FE/RURAL	EARTH	*												
(RT) STA. 542+51.3	SR	BIT.	**												
(LT) STA. 542+51.3	SR	BIT.	**												
(RT & LT) STA. 556+00	FE/RURAL	EARTH	*												
(LT) STA. 560+10	PE/MB	BIT.		56.0		4.0	6.0								9.86
(LT) STA. 568+30	FE/RURAL	EARTH	*												
(RT) STA. 579+20	FE/RURAL	EARTH	*												
(LT) STA. 582+75	FE/RURAL	AGG.	*												
(RT) STA. 582+90	FE/RURAL	EARTH	*												
(LT) STA. 588+00	SR	BIT.	**												
(LT) STA. 595+40	FE/RURAL	EARTH	*												
(LT) STA. 604+90	FE/RURAL	EARTH	*												
(RT & LT) STA. 609+90	FE/RURAL	EARTH	*												
(RT) STA. 625+85.12	SR	BIT.	**												
(RT) STA. 632+49.03	SR	BIT.	**												
(LT) STA. 632+49.03	SR	BIT.	**												
(LT) STA. 644+00	MB	BIT.		20.0		4.0	6.0								4.48
(RT) STA. 644+50	PE	BIT.	34.0			4.0	1.5	17				0.0064	0.0241	1.42	
(LT) STA. 645+20	FE/RURAL	EARTH	*												
(LT) STA. 2199+65	PE	AGG.	36.0			5.0					4.0				
(RT) STA. 2203+80	FE/RURAL	EARTH	*												
(LT) STA. 2205+95	MB	BIT.		20.0		4.0	6.0								4.48
(RT) STA. 2206+10	PE	BIT.	16.0			4.0	1.5	9				0.0034	0.0127	0.75	
(LT) STA. 2210+90	PE/MB	BIT.		52.0		4.0	6.0								9.26
(RT) STA. 2214+80	CE	BIT.	22.0			4.0	1.5	12				0.0044	0.0165	0.97	
(RT) STA. 2216+90	CE	PCC	34.0			4.0	1.5		17			0.0064	0.0241	1.42	
(RT) STA. 2219+10	PE	BIT.	26.0			4.0	1.5	13				0.0051	0.0190	1.12	
(LT) STA. 2219+10	MB	BIT.		20.0		4.0	6.0								4.48
(RT) STA. 2220+80	CE	PCC	36.0			4.0	1.5		18			0.0068	0.0253	1.49	
(LT) STA. 2220+90	CE/MB	BIT.		75.0		4.0	6.0								12.69
(RT) STA. 2221+45	FE/RURAL	AGG.	*												
(LT) STA. 2225+30	CE	BIT.	40.0			4.0	1.5	20				0.0074	0.0279	1.64	
TOTAL THIS SHEET=	-	-						103	58			4.0	0.0602	0.2256	78.85

NOTES:  
 \* FIELD ENTRANCES SHOWN FOR INFORMATIONAL PURPOSES ONLY.  
 PROPOSED AGGREGATE SHOULDER, TYPE B TO CONTINUE THROUGH FIELD ENTRANCES.  
 \*\* SIDEROADS WITH EXISTING SHOULDER RADII. SEE DETAIL.

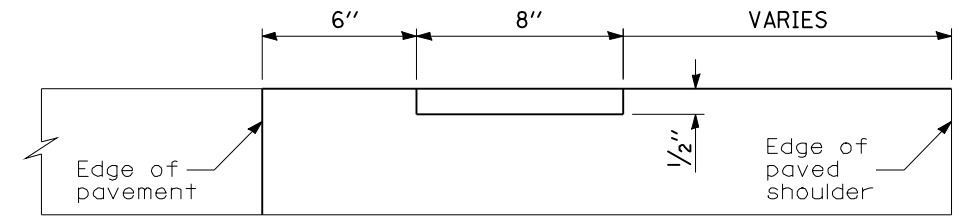




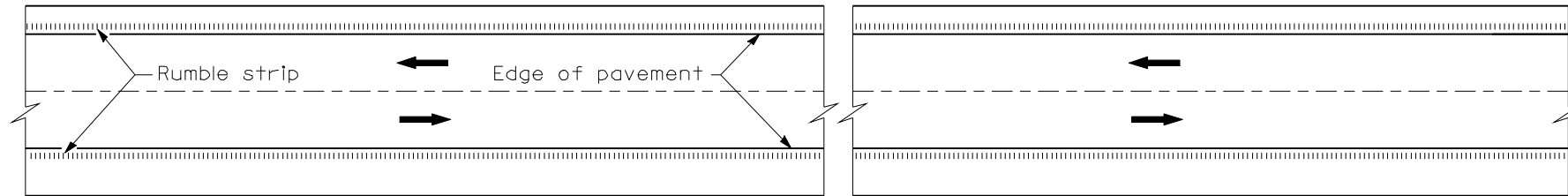
PLAN



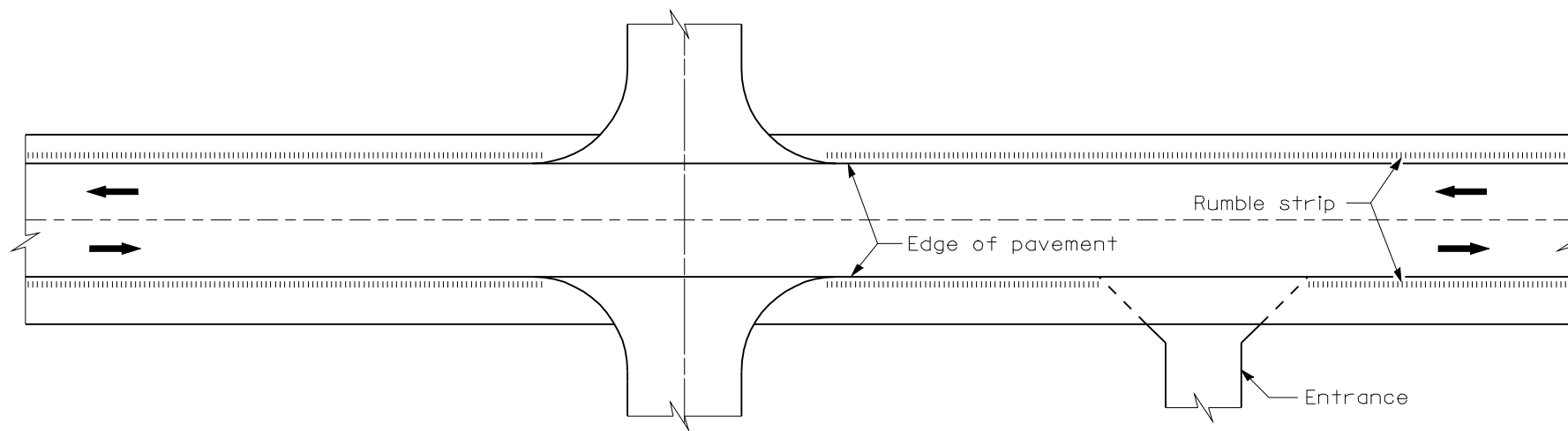
SECTION A-A



SECTION B-B



PLAN  
TYPICAL APPLICATION FOR RURAL AREAS



PLAN  
TYPICAL APPLICATION AT SIDEROADS AND ENTRANCES

FILE NAME =	USER NAME = laughlinr1	DESIGNED - KCM	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>RUMBLE STRIP DETAILS</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
e:\pwwork\pwwid\LAUGHLINRL\0194809\06-72039-020-rumble-strip-details.dgn	DRAWN - JLM	REVISED -	REVISED -		662	(D, DX, E, F) I	MACOUPIN	20	20				
PLOT SCALE = 2.0000' / IN.	CHECKED - TMM	REVISED -	REVISED -		CONTRACT NO. 72039								
PLOT DATE = Mar-30-2010 09:52:04AM	DATE - 3-26-10	REVISED -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			