

EXISTING STRUCTURE: S.N. 058-0108(SB) & 0109(NB), originally constructed in 1978 as FA Route 412 Sec. 58-20HV-B-1 at Station 235+00.78, using rolled and welded steel I-girders with 8" concrete deck and 1/2" bituminous wearing surface, 4 spans, 275'-4" back-back abutments, variable out-out width, open stub abutments on concrete piles, multi-column piers with footings on concrete piles.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

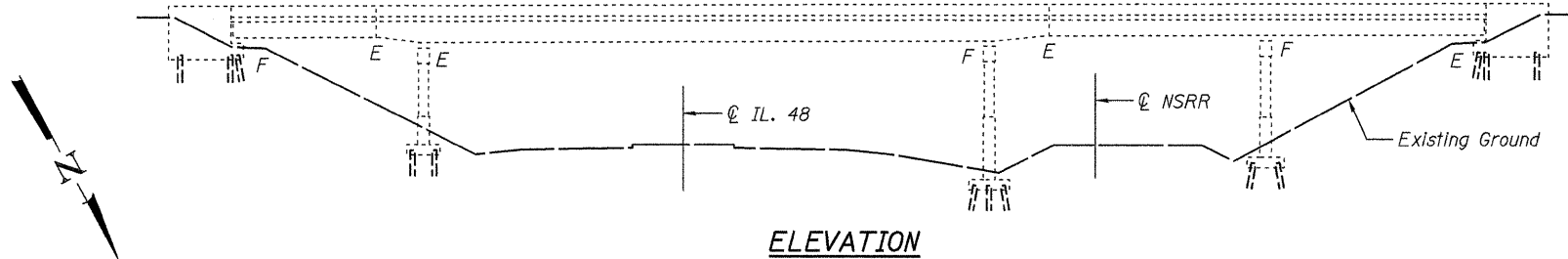
Staged construction shall be used to maintain one lane of traffic in each direction.

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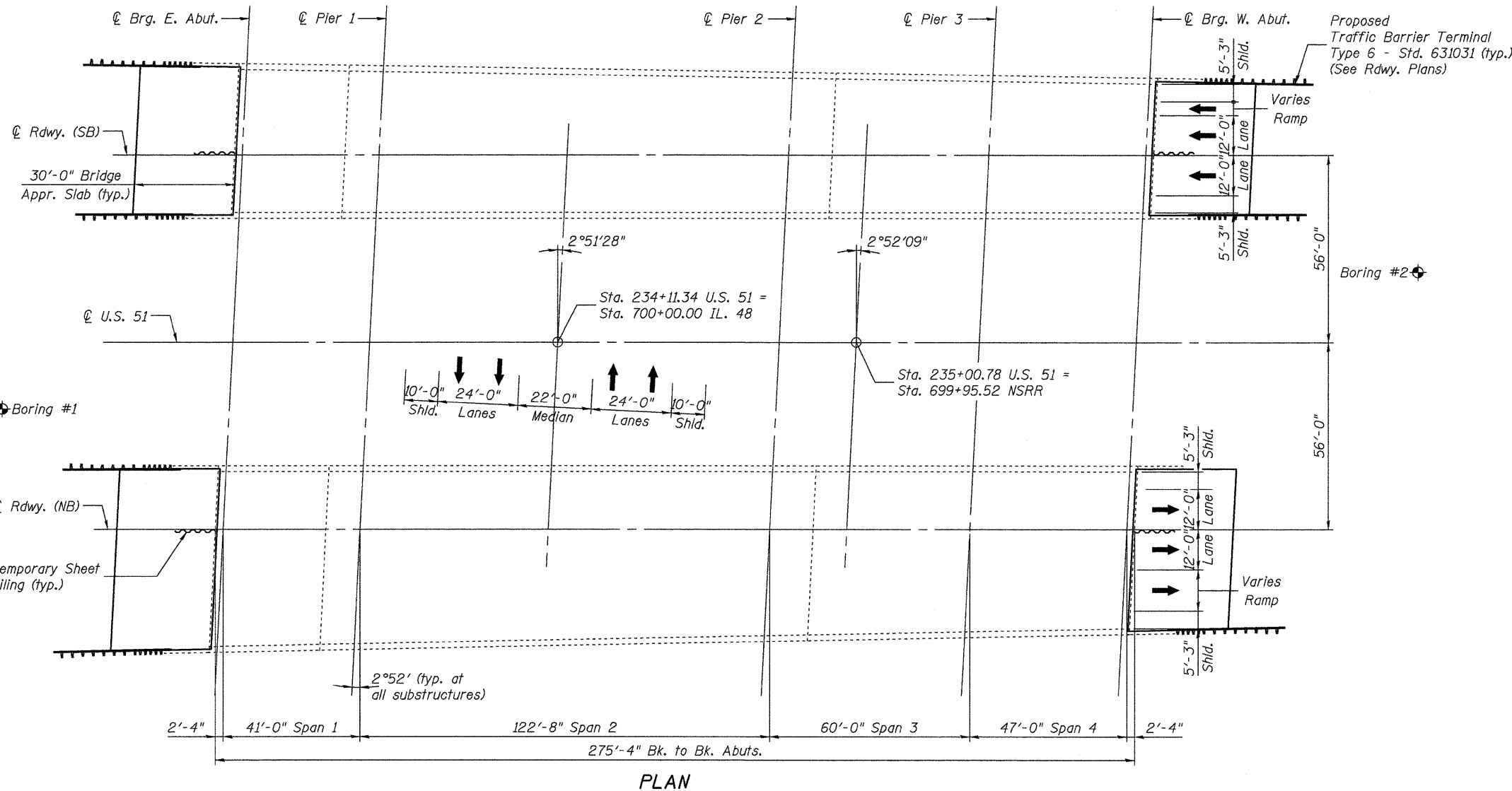
Sheet No.	Description
1	Gen Plan, Gen Notes, Bill of Mat'l
2	Temporary Sheet Piling
3	Temporary Concrete Barrier
4-10	Superstructure
11	Preformed Joint Strip Seal
12-15	Bridge Approach Slab Details
16	Bearings
17-22	Abutments
23-24	Piers
25	Bar Splicer Assembly Details
26	Soil Borings

GENERAL NOTES

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.  
No field welding is permitted except as specified in the contract documents.  
Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.  
Reinforcement bars designated (E) shall be epoxy coated.  
Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.  
Where shown on the plans, reinforcement bars shall be epoxy grouted into existing concrete according to Section 584 of the Standard Specifications. Cost included with Reinforcement Bars, Epoxy Coated.  
Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.



ELEVATION

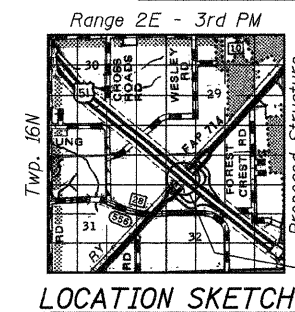


PLAN

Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.  
The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.  
Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".  
All structural steel shall be shop painted with the inorganic zinc rich primer per AASHTO M300, Type 1. Cost included with Furnishing and Erecting Structural Steel.  
Protective Shield shall be provided for the full width of the bridge deck, over the roadway below from edge of shoulder to edge of shoulder, and over the railroad for 10 feet each side of the track centerline.  
Elevations shown on these plans are based on the original 1975 plan elevations, not the current datum. The original plan elevations may be used to establish a temporary benchmark for construction.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SB	NB
Porous Granular Embankment, Special	Cu Yd	160	200
Hot-Mix Asphalt Surface Removal (Deck)	Sq Yd	1166	1422
Concrete Removal	Cu Yd	53.5	65.1
Protective Shield	Sq Yd	610	733
Structure Excavation	Cu Yd	160	200
Concrete Structures	Cu Yd	48.0	56.2
Concrete Superstructure	Cu Yd	159.2	195.6
Bridge Deck Grooving	Sq Yd	1440	1772
Protective Coat	Sq Yd	1753	2085
Furnishing And Erecting Structural Steel	Pound	1090	1240
Jack And Remove Existing Bearings	Each	7	8
Reinforcement Bars, Epoxy Coated	Pound	39580	47590
Bar Splicers	Each	388	405
Temporary Sheet Piling	Sq Ft	318	323
Preformed Joint Strip Seal	Foot	173	209
Elastomeric Bearing Assembly, Type I	Each	7	8
Anchor Bolts, 1"	Each	14	16
Geocomposite Wall Drain	Sq Yd	72	87
Pipe Underdrains For Structures 4"	Foot	218	235
Structural Repair Of Concrete (Depth = < 5")	Sq Ft	1	10
Bridge Deck Microsilica Concrete Overlay 2 1/4"	Sq Yd	1166	1422
Bridge Deck Hydro-Scarification 2"	Sq Yd	1166	1422



LOCATION SKETCH

GENERAL PLAN & ELEVATION  
U.S. 51 OVER IL 48 & NSRR  
F.A.P. RTE. 322 SECTION (58-20)RS  
MACON COUNTY  
STATION 235+00.78  
STRUCTURE NO. 058-0108(SB) & 0109(NB)

SHEET	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1 OF 26	322	(58-20)RS	MACON	151	103
		STA. 235+00.78	CONTRACT NO.	74150	
		FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		

**JD Johnson, Depp & Quisenberry**  
CONSULTING ENGINEERS  
Springfield, Illinois

DESIGNED: JDQ      DRAWN: PTR  
CHECKED: DCD      CHECKED: DCD

STATE OF ILLINOIS  
DAVID G. DEPP  
081-006117  
LICENSED PROFESSIONAL ENGINEER

Signed: *David Depp*  
Date: 4-9-2010  
Lic. Expires: 11-30-2010

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