

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|-----------|----------|--------------------|-----------|
| 322 | (58-20)RS | MACON | 151 | 1 |
| | | ILLINOIS | CONTRACT NO. 74150 | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

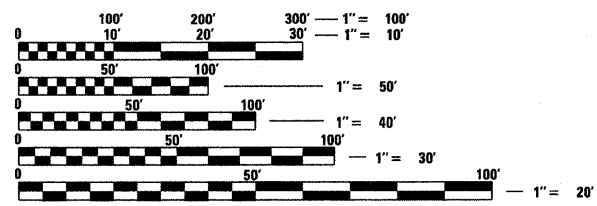
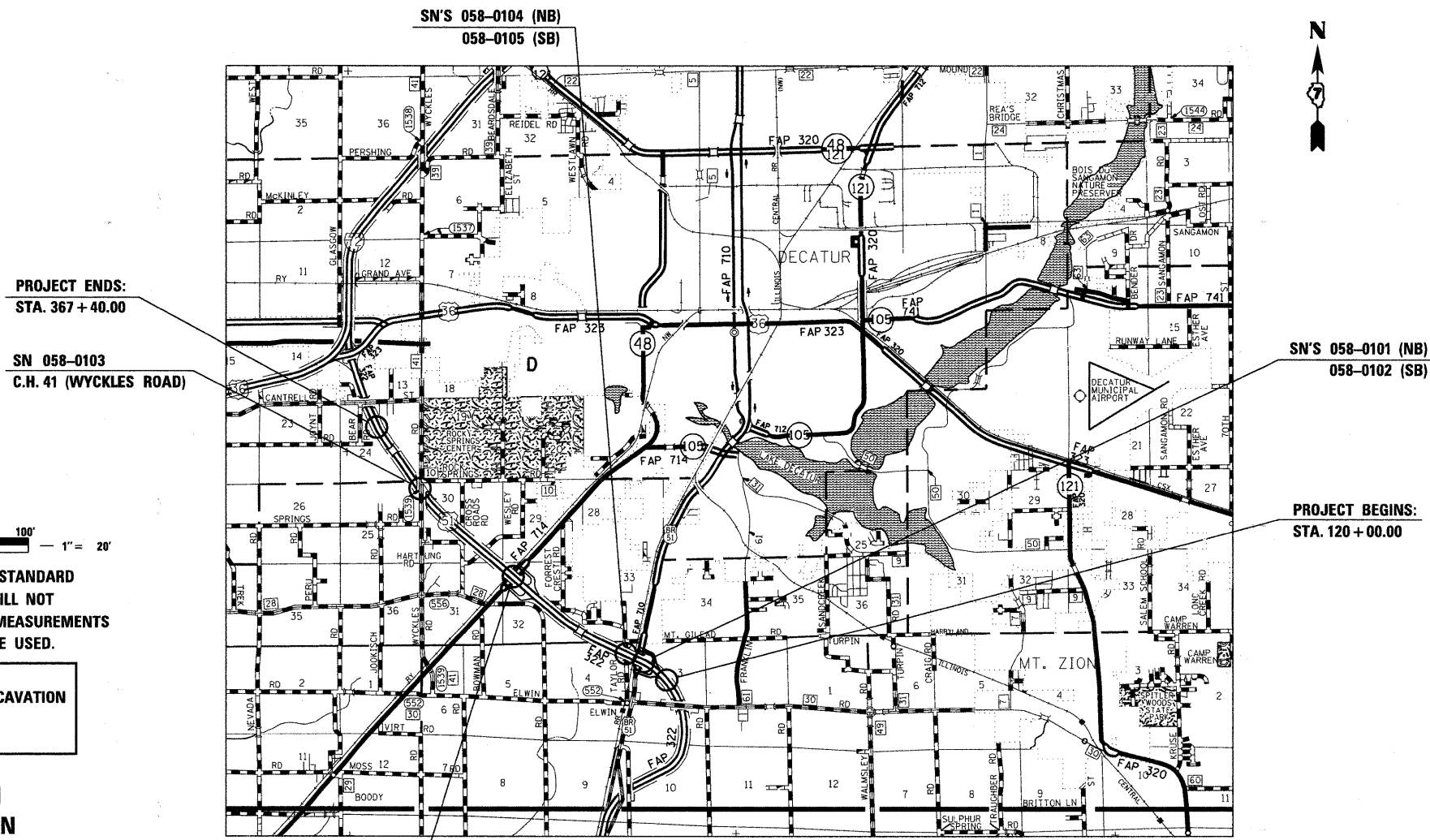
F.A.P. ROUTE 322 (U.S. RTE. 51)
SECTION (58-20)RS
PROJECT --
(3R) RESURFACING, BRIDGE REPAIR
MACON COUNTY

C-97-043-06

FOR INDEX OF SHEETS, SEE SHEET NO. 2

ADT = 3,500 (2009)

D-97-020-06



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: TOM RONAN
PROJECT MANAGER: JEFF DAVISON
PHONE 217-342-8320
CONTRACT NO. 74150

GROSS LENGTH = 24,816.5 FT. = 4.7 MILE
NET LENGTH = 23,237.6 FT. = 4.4 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED 3-18 2010
Roger J. Orskell
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 7 2010
Scott E. Stitt P.E.
acting ENGINEER OF DESIGN AND ENVIRONMENT

May 7 2010
Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

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STANDARDS IN THE PLANS

| | |
|-----------|--|
| 000001-05 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 001001-02 | AREAS OF REINFORCEMENT BARS |
| 001006 | DECIMAL OF AN INCH AND OF A FOOT |
| 202001-01 | EARTH MEDIAN DITCH CHECK |
| 280001-05 | TEMPORARY EROSION CONTROL SYSTEMS |
| 420401-08 | BRIDGE APPROACH PAVEMENT CONNECTOR |
| 420701-02 | PAVEMENT FABRIC |
| 442001-04 | CLASS A PATCHES |
| 442201-03 | CLASS C AND D PATCHES |
| 542531-03 | INLET BOX TYPE G, 600mm (24") |
| 601101-01 | CONCRETE HEADWALL FOR PIPE DRAINS |
| 630001-08 | STEEL PLATE BEAM GUARDRAIL |
| 630301-05 | SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS |
| 631011-06 | TRAFFIC BARRIER TERMINAL, TYPE 2 |
| 631031-08 | TRAFFIC BARRIER TERMINAL, TYPE 6 |
| 635001-01 | DELINEATORS |
| 635006-03 | REFLECTOR AND TERMINAL MARKER PLACEMENT |
| 635011-02 | REFLECTOR MARKER AND MOUNTING DETAILS |
| 642001-01 | SHOULDER RUMBLE STRIPS |
| 701101-02 | OFF-ROAD OPERATIONS, MULTILANE, 4.5m (15'), TO 600mm (24") FROM PAVEMENT EDGE |
| 701106-02 | OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 4.5m (15") AWAY |
| 701400-04 | APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY |
| 701401-05 | LANE CLOSURE, FREEWAY/EXPRESSWAY |
| 701402-07 | LANE CLOSURE, FREEWAY/EXPRESSWAY, WITH BARRIER |
| 701406-05 | LANE CLOSURE, FREEWAY/EXPRESSWAY, DAY OPERATIONS ONLY |
| 701411-06 | LANE CLOSURE, MULTILANE AT ENTRANCE OR EXIT RAMP, FOR SPEEDS > 45 MPH |
| 701426-03 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS > 45 MPH |
| 701456 | PARTIAL EXIT RAMP CLOSURE, FREEWAY/EXPRESSWAY |
| 701901-01 | TRAFFIC CONTROL DEVICES |
| 704001-06 | TEMPORARY CONCRETE BARRIER |
| 781001-03 | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS |
| BLR 21-8 | TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES FOR CONSTRUCTION ON RURAL LOCAL HIGHWAYS |

GENERAL NOTES

THIS SECTION SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE PLANS, THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2007; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" INDICATED ON THE CHECK SHEET, AND "THE SPECIAL PROVISIONS" INCLUDED IN THE PROPOSAL.

THE WORK IN SECTION (58-20)RS INCLUDES PATCHING, MILLING AND RESURFACING THE EXISTING PAVEMENT, AGGREGATE AND BITUMINOUS SHOULDERS, ERECTION OF STEEL PLATE BEAM GUARDRAIL, INSTALLATION OF IMPACT ATTENUATORS, BRIDGE REPAIR AND ALL OTHER WORK NECESSARY TO COMPLETE THIS SECTION.

THE PROPOSED PROJECT BEGINS APPROXIMATELY 1.0 MILES SOUTH OF IL 36 ON US 51 AND EXTENDS IN AN SOUTHERLY DIRECTION A DISTANCE OF 4.7 MILES.

SHORT TERM PAVEMENT MARKING ON THE P.C.C. SURFACE SHALL BE PAINT. ALL OTHER SHORT TERM PAVEMENT MARKINGS SHALL BE TAPE.

THE QUANTITIES OF PAVEMENT PATCHING ARE ESTIMATES. THE ACTUAL LOCATIONS AND QUANTITIES SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

ALL ELEVATIONS SHOWN IN PLANS ARE BASED ON U.S.G.S. DATUM.

ALL DISTURBED AREAS WITHIN THE CONSTRUCTION LIMITS SHALL BE FERTILIZED AND SEEDED. SEEDING SHALL BE CLASS 2 IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND THE APPLICABLE ARTICLES OF SECTION 250 OF THE STANDARD SPECIFICATIONS.

THE PAVEMENT SHALL BE CLEANED IN ACCORDANCE WITH SECTION 406 OF THE STANDARD SPECIFICATIONS PRIOR TO THE PLACING OF THE HOT-MIX ASPHALT OVERLAYS. AREAS TO BE CLEANED SHALL BE DETERMINED BY THE ENGINEER AND WILL INCLUDE:

1. BROKEN OR DELAMINATED CONCRETE
2. PATCHES OF TAR OR COLD MIX
3. EXISTING UNSTABLE PATCHES
4. DETERIORATED AREAS BEYOND EXISTING PATCHES
5. CENTERLINE JOINT

THIS WORK SHALL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL PROVIDE INTERNET ACCESS TO THE BITUMINOUS PLANT QUALITY CONTROL LAB SO THAT BITUMINOUS PLANT REPORTS CAN BE E-MAILED TO THE DISTRICT HEADQUARTERS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICES FOR OTHER ITEMS IN THE CONTRACT.

IN ADDITION TO THE REQUIREMENTS FOR PORTABLE CHANGEABLE MESSAGE SIGNS IN ARTICLE 701.15(1), A LAPTOP COMPUTER SHALL ALSO BE SUPPLIED BY THE CONTRACTOR. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE RESIDENT ENGINEER SHALL BE THE SOLE JUDGE CONCERNING THE CURING TIME FOR THE HOT-MIX ASPHALT SURFACE COURSE.

THE CONTRACTOR SHALL USE EITHER RC-70, SS1H, OR SS1HP, APPLIED AT THE RATE DIRECTED BY THE ENGINEER, FOR THE PAY ITEM: BITUMINOUS MATERIALS (PRIME COAT).

AGGREGATE FOR AGGREGATE SHOULDERS ON INTERSTATE ROUTES SHALL BE CRUSHED STONE, CRUSHED CONCRETE, OR RAP.

COST OF REMOVAL OF TEMPORARY RAMP SHALL BE INCLUDED IN THE UNIT PRICE FOR TEMPORARY RAMP.

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE TO THIS PROJECT:

| APPLICATION | AC/PG | DESIGN AIR VOIDS | MIXTURE COMPOSITION | FRICTION AGGREGATE |
|--|--------------|---------------------|------------------------|-----------------------|
| BINDER COURSE (2 1/4") POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 | SBS PG 70-22 | 4.0% e N=90 | IL-19.0 | N/A |
| SURFACE COURSE & MEDIAN CROSSEOVERS (1 1/2") POLYMERIZED HOT MIX-ASPHALT SURFACE COURSE, MIX "D", N90 | SBS PG 70-22 | 4.0% e N=90 | IL-9.5 | MIXTURE D |
| HMA SHOULDERS (3 3/4") HOT-MIX ASPHALT SHOULDERS | PG 58-22 | 4.0% e N=30 | IL-19.0L | N/A |
| PARTIAL DEPTH PATCHING HOT-MIX ASPHALT SURFACE COURSE, PG 64-22 MIX "C", N90 | | 4.0% e N=90 | IL-9.5 | N/A |
| HOT-MIX ASPHALT BASE COURSE, 10" HOT MIX ASPHALT BINDER COURSE, PG 64-22 IL-19.0, N70 | | 4.0% e N=70 | IL-19.0 | N/A |
| CLASS D PATCHES, TYPES II, III & IV, 10" HOT-MIX ASPHALT BINDER COURSE, PG 64-22 IL-19.0, N70 | | 4.0% e N=70 | IL-19.0 | N/A |
| LEVEL BINDER LEVEL BINDER (MACHINE METHOD) | PG 64-22 | 4.0% e N=90 | IL-9.5 | N/A |
| MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS HOT-MIX ASPHALT SURFACE COURSE, PG 64-22 MIX "C", N70 | | 4.0% e N=70 | IL-9.5 | N/A |

ONCE THE FINAL SURFACE MIX IS PLACED ON THE HOT MIX ASPHALT SHOULDERS, THE LOCATIONS OF THE PIPE UNDERDRAIN OUTLETS SHALL BE RE-ESTABLISHED WITH A "T" PAINTED ON THE OUTSIDE EDGE OF THE SHOULDERS. THE COST OF RE-ESTABLISHING THE OUTLET LOCATIONS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PAVEMENT MARKING.

THE COST FOR REMOVAL OF THE EXISTING DELINEATORS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED DELINEATORS.

| | | | | | | | | | | | |
|--|------------------------------|------------|-----------|---|--|--------------------|-------------------------|--------|-----------------|---------------------------|--|
| FILE NAME = | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | INDEX OF SHEETS AND GENERAL NOTES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ci:\pw\work\PIWIDOT\SWARTZRW\d8138944\d7 | 4150-shr-index.dgn | DRAWN - | REVISED - | | | 322 | (58,20)RS | MACON | 151 | 2 | |
| | PLOT SCALE = 100.0000 "/ IN. | CHECKED - | REVISED - | | | CONTRACT NO. 74150 | | | | | |
| | PLOT DATE = 3/17/2010 | DATE - | REVISED - | | | SCALE: NA | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT | |

Rev.

| SUMMARY OF QUANTITIES | | | URBAN | | CONSTRUCTION TYPE CODE | | | | | | |
|-----------------------|--|--------|------------------|------------------|------------------------|---------------------|---------------------|---------------------|---------------------|---------------------|------------------|
| | | | 100% STATE | TOTAL QUANTITIES | URBAN | | | | | | |
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | 1000 | 058-0101 NB SFTY-2A | 058-0102 SB SFTY-2A | 058-0104 NB SFTY-2A | 058-0105 SB SFTY-2A | 058-0109 NB SFTY-2A | 058-0108 SB SFTY-2A | 058-0103 SFTY-2A |
| 20200100 | EARTH EXCAVATION | CU YD | 9 | 9 | | | | | | | |
| 20400800 | FURNISHED EXCAVATION | CU YD | 1403 | 1403 | | | | | | | |
| 20700400 | POROUS GRANULAR EMBANKMENT, SPECIAL | CU YD | 736 | | 209 | 167 | | | 200 | 160 | |
| 25000200 | SEEDING, CLASS 2 | ACRE | 1.15 | 1.15 | | | | | | | |
| 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 103 | 103 | | | | | | | |
| 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 103 | 103 | | | | | | | |
| 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 103 | 103 | | | | | | | |
| 25100115 | MULCH, METHOD 2 | ACRE | 1.15 | 1.15 | | | | | | | |
| 28000250 | TEMPORARY EROSION CONTROL SEEDING | POUND | 115 | 115 | | | | | | | |
| 28000500 | INLET AND PIPE PROTECTION | EACH | 1 | 1 | | | | | | | |
| 35501324 | HOT-MIX ASPHALT BASE COURSE, 10" | SQ YD | 5018 | 5018 | | | | | | | |
| 40600100 | BITUMINOUS MATERIALS (PRIME COAT) | GALLON | 25631 | 25631 | | | | | | | |
| 40600300 | AGGREGATE (PRIME COAT) | TON | 510 | 510 | | | | | | | |
| 40600645 | LEVELING BINDER (MACHINE METHOD), N90 | TON | 151 | 151 | | | | | | | |
| 40600895 | CONSTRUCTING TEST STRIP | EACH | 2 | 2 | | | | | | | |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 2393 | 2393 | | | | | | | |
| 40600985 | PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT | SQ YD | 3002 | 3002 | | | | | | | |
| 40600990 | TEMPORARY RAMP | SQ YD | 2576 | 2576 | | | | | | | |
| 40603240 | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 | TON | 19975 | 19975 | | | | | | | |
| 40603545 | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 | TON | 13401 | 13401 | | | | | | | |
| 42001420 | BRIDGE APPROACH PAVEMENT CONNECTOR (PCC) | SQ YD | 257 | 257 | | | | | | | |
| 44000155 | HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" | SQ YD | 4142 | 4142 | | | | | | | |
| 44000300 | CURB REMOVAL | FOOT | 3888 | 3888 | | | | | | | |
| 44000700 | APPROACH SLAB REMOVAL | SQ YD | 1102 | 1102 | | | | | | | |
| 44000915 | HOT-MIX ASPHALT SURFACE REMOVAL (DECK) | SQ YD | 8555 | | 1603 | 1327 | 581 | 581 | 1422 | 1166 | 1875 |
| 44004250 | PAVED SHOULDER REMOVAL | SQ YD | 3994 | 3994 | | | | | | | |
| 44200525 | CLASS A PATCHES, TYPE I, 8 INCH | SQ YD | 1084 | 1084 | | | | | | | |
| 44200529 | CLASS A PATCHES, TYPE II, 8 INCH | SQ YD | 1534 | 1534 | | | | | | | |
| 44200533 | CLASS A PATCHES, TYPE III, 8 INCH | SQ YD | 798 | 798 | | | | | | | |
| 44200535 | CLASS A PATCHES, TYPE IV, 8 INCH | SQ YD | 2692 | 2692 | | | | | | | |
| 44201765 | CLASS D PATCHES, TYPE II, 10 INCH | SQ YD | 192 | 192 | | | | | | | |

| SUMMARY OF QUANTITIES (Cont'd) | | | URBAN 100% STATE | CONSTRUCTION TYPE CODE | | | | | | | | | |
|--------------------------------|---|-------|---------------------|------------------------|---------------------------|---------------------------|---------------------------|---------------------------|---------------------------|---------------------------|---------------------|--|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | URBAN | | | | | | | | | |
| | | | | 1000 | 058-0101 NB SFTY-2A | 058-0102 SB SFTY-2A | 058-0104 NB SFTY-2A | 058-0105 SB SFTY-2A | 058-0109 NB SFTY-2A | 058-0108 SB SFTY-2A | 058-0103 SFTY-2A | | |
| 44201769 | CLASS D PATCHES, TYPE III, 10 INCH | SQ YD | 25 | 25 | | | | | | | | | |
| 44201771 | CLASS D PATCHES, TYPE IV, 10 INCH | SQ YD | 31 | 31 | | | | | | | | | |
| 44212900 | PAVEMENT PATCHING (PARTIAL DEPTH) | SQ YD | 13633 | 13633 | | | | | | | | | |
| 44213000 | PATCHING REINFORCEMENT | SQ YD | 6107 | 6107 | | | | | | | | | |
| 44213200 | SAW CUTS | FOOT | 32485 | 32485 | | | | | | | | | |
| 48101200 | AGGREGATE SHOULDERS, TYPE B | TON | 2728 | 2728 | | | | | | | | | |
| 48203100 | HOT-MIX ASPHALT SHOULDERS | TON | 21021 | 21021 | | | | | | | | | |
| 50102400 | CONCRETE REMOVAL | CU YD | 315.7 | | 63.1 | 52.7 | 23.1 | 23.1 | 65.1 | 53.5 | 35.1 | | |
| 50157300 | PROTECTIVE SHIELD | SQ YD | 3574 | | 558 | 664 | 155 | 155 | 733 | 610 | 699 | | |
| 50200100 | STRUCTURE EXCAVATION | CU YD | 736 | | 209 | 167 | | | 200 | 160 | | | |
| 50300225 | CONCRETE STRUCTURES | CU YD | 213.4 | | 58.4 | 50.8 | | | 56.2 | 48 | | | |
| 50300255 | CONCRETE SUPERSTRUCTURE | CU YD | 802 | | 197.4 | 162.7 | 25.6 | 25.6 | 195.6 | 159.2 | 35.9 | | |
| 50300260 | BRIDGE DECK GROOVING | SQ YD | 9542 | | 1855 | 1523 | 551 | 551 | 1772 | 1440 | 1850 | | |
| 50300300 | PROTECTIVE COAT | SQ YD | 11202 | | 2184 | 1851 | 682 | 682 | 2085 | 1753 | 1965 | | |
| 50500405 | FURNISHING AND ERECTING STRUCTURAL STEEL | POUND | 7660 | | 1260 | 1100 | 140 | 140 | 1240 | 1090 | 2690 | | |
| 50500715 | JACK AND REMOVE EXISTING BEARINGS | EACH | 42 | | 8 | 7 | | | 8 | 7 | 12 | | |
| 50800205 | REINFORCEMENT BARS, EPOXY COATED | POUND | 190,380 | | 48,040 | 40,390 | 4360 | 4420 | 47,590 | 39,580 | 6000 | | |
| 50800515 | BAR SPLICERS | EACH | 1,656 | | 408 | 391 | 32 | 32 | 405 | 388 | | | |
| 51205200 | TEMPORARY SHEET PILING | SQ FT | 1303 | | 334 | 328 | | | 323 | 318 | | | |
| 52000110 | PREFORMED JOINT STRIP SEAL | FOOT | 1159 | | 213 | 178 | 90 | 90 | 209 | 173 | 206 | | |
| 52100010 | ELASTOMERIC BEARING ASSEMBLY, TYPE I | EACH | 42 | | 8 | 7 | | | 8 | 7 | 12 | | |
| 52100220 | ELASTOMERIC BEARING ASSEMBLY, TYPE II | EACH | 12 | | | | | | | | 12 | | |
| 52100520 | ANCHOR BOLTS, 1" | EACH | 104 | | 16 | 14 | 10 | 10 | 16 | 14 | 24 | | |
| 54205059 | PIPE CULVERTS, SPECIAL 24" | FOOT | 8 | 8 | | | | | | | | | |
| 54215547 | METAL END SECTIONS 12" | EACH | 7 | 7 | | | | | | | | | |
| 54246405 | INLET BOX, STANDARD 542531 | EACH | 1 | 1 | | | | | | | | | |
| 542A0229 | PIPE CULVERTS, CLASS A, TYPE 1 24" | FOOT | 155 | 155 | | | | | | | | | |
| 59100100 | GEOCOMPOSITE WALL DRAIN | SQ YD | 323 | | 89 | 75 | | | 87 | 72 | | | |
| 60100945 | PIPE DRAINS 12" | FOOT | 468 | 468 | | | | | | | | | |
| 60109580 | PIPE UNDERDRAINS FOR STRUCTURES 4" | FOOT | 950 | | 260 | 243 | | | 235 | 218 | | | |
| 60242809 | INLETS, SPECIAL, TYPE C | EACH | 7 | 7 | | | | | | | | | |
| 60260100 | INLETS TO BE ADJUSTED | EACH | 8 | 8 | | | | | | | | | |
| 60900515 | CONCRETE THRUST BLOCKS | EACH | 7 | 7 | | | | | | | | | |
| * 63000001 | STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS | FOOT | 2062.5 | 2062.5 | | | | | | | | | |

Rev. *SPECIALTY ITEMS

| SUMMARY OF QUANTITIES (Cont'd) | | | | CONSTRUCTION TYPE CODE | | | | | | | | | |
|--------------------------------|---|--------|------------------|------------------------|---------------------|---------------------|---------------------|---------------------|---------------------|---------------------|------------------|--|---|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | URBAN | | | | | | | | | |
| | | | | 1000 | 058-0101 NB SFTY-2A | 058-0102 SB SFTY-2A | 058-0104 NB SFTY-2A | 058-0105 SB SFTY-2A | 058-0109 NB SFTY-2A | 058-0108 SB SFTY-2A | 058-0103 SFTY-2A | | |
| * 6300003 | STEEL PLATE BEAM GUARD RAIL, TYPE A, 9 FOOT POSTS | FOOT | 4650 | 4650 | | | | | | | | | |
| * 6310045 | TRAFFIC BARRIER TERMINAL, TYPE 2 | EACH | 7 | 7 | | | | | | | | | |
| * 6310085 | TRAFFIC BARRIER TERMINAL, TYPE 6 | EACH | 19 | 19 | | | | | | | | | |
| * 63100167 | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT | EACH | 18 | 18 | | | | | | | | | |
| 63200310 | GUARDRAIL REMOVAL | FOOT | 6523 | 6523 | | | | | | | | | |
| 63400105 | GUARD POSTS | EACH | 10 | 10 | | | | | | | | | |
| 63500105 | DELINEATORS | EACH | 294 | 294 | | | | | | | | | |
| 64200105 | SHOULDER RUMBLE STRIP | FOOT | 88657 | 88657 | | | | | | | | | |
| 66101150 | HOT-MIX ASPHALT SHOULDER CURB | FOOT | 5545 | 5545 | | | | | | | | | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 18 | 18 | | | | | | | | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | | | | | | | | | |
| 70100207 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701402 | EACH | 6 | | 1 | 1 | 1 | 1 | 1 | 1 | | | |
| 70100420 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701411 | EACH | 10 | | 2 | 2 | | | 3 | 3 | | | |
| 70100700 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701406 | L SUM | 1 | 1 | | | | | | | | | |
| 70100800 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701401 | L SUM | 1 | 1 | | | | | | | | | |
| 70101830 | TRAFFIC CONTROL AND PROTECTION, STANDARD BLR 21 | L SUM | 1 | | | | | | | | | | 1 |
| 70103815 | TRAFFIC CONTROL SURVEILLANCE | CAL DA | 45 | 45 | | | | | | | | | |
| 70300100 | SHORT-TERM PAVEMENT MARKING | FOOT | 10603 | 10603 | | | | | | | | | |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 123463 | 123463 | | | | | | | | | |
| 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6" | FOOT | 1032 | 1032 | | | | | | | | | |
| 70300250 | TEMPORARY PAVEMENT MARKING - LINE 8" | FOOT | 3743 | 3743 | | | | | | | | | |
| 70300260 | TEMPORARY PAVEMENT MARKING - LINE 12" | FOOT | 224 | 224 | | | | | | | | | |
| 70300280 | TEMPORARY PAVEMENT MARKING - LINE 24" | FOOT | 144 | 144 | | | | | | | | | |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SO FT | 45630 | 45630 | | | | | | | | | |
| 70400100 | TEMPORARY CONCRETE BARRIER | FOOT | 3860 | 3860 | | | | | | | | | |
| 70400200 | RELOCATE TEMPORARY CONCRETE BARRIER | FOOT | 3694 | 3694 | | | | | | | | | |
| * 78004230 | PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 6" | FOOT | 11684 | 11684 | | | | | | | | | |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 1473 | 1473 | | | | | | | | | |

*SPECIALTY ITEMS

| SUMMARY OF QUANTITIES (Cont'd) | | | | CONSTRUCTION TYPE CODE | | | | | | | | | |
|--------------------------------|---|-----------------|------------------|------------------------|---------------------|---------------------|---------------------|---------------------|---------------------|---------------------|------------------|------|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | URBAN | | | | | | | | | |
| | | | | 1000 | 058-0101 NB SFTY-2A | 058-0102 SB SFTY-2A | 058-0104 NB SFTY-2A | 058-0105 SB SFTY-2A | 058-0109 NB SFTY-2A | 058-0108 SB SFTY-2A | 058-0103 SFTY-2A | | |
| * 78100105 | RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) | EACH | 79 | 79 | | | | | | | | | |
| * 78200410 | GUARDRAIL MARKERS, TYPE A | EACH | 119 | 119 | | | | | | | | | |
| * 78201000 | TERMINAL MARKER - DIRECT APPLIED | EACH | 18 | 18 | | | | | | | | | |
| 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 619 | 619 | | | | | | | | | |
| X0300136 | BRIDGE APPROACH SHOULDER REMOVAL | SO YD | 211 | 211 | | | | | | | | | |
| X0322729 | MATERIAL TRANSFER DEVICE | TON | 33292 | 33292 | | | | | | | | | |
| X0325305 | STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES) | SO FT | 62 | | 22 | 24 | 5 | | 10 | 1 | | | |
| X4400220 | CURB REMOVAL AND REPLACEMENT | FOOT | 42 | 42 | | | | | | | | | |
| X4401705 | PARTIAL DEPTH REMOVAL | SO YD | 13633 | 13633 | | | | | | | | | |
| X6050700 | REMOVE INLET BOX | EACH | 7 | 7 | | | | | | | | | |
| * X7800610 | URETHANE PAVEMENT MARKING - LINE 4" | FOOT | 123463 | 123463 | | | | | | | | | |
| * X7800630 | URETHANE PAVEMENT MARKING - LINE 6" | FOOT | 1032 | 1032 | | | | | | | | | |
| * X7800640 | URETHANE PAVEMENT MARKING - LINE 8" | FOOT | 3743 | 3743 | | | | | | | | | |
| * X7800650 | URETHANE PAVEMENT MARKING - LINE 12" | FOOT | 224 | 224 | | | | | | | | | |
| * X7800680 | URETHANE PAVEMENT MARKING - LINE 24" | FOOT | 144 | 144 | | | | | | | | | |
| XZ193500 | BRIDGE DECK MICROSILICA CONCRETE OVERLAY 2 1/4" | SO YD | 8286 | | 1507 | 1248 | 534 | 534 | 1422 | 1166 | | 1875 | |
| Z0002005 | ATTENUATOR BASE | SO YD | 93 | 93 | | | | | | | | | |
| Z0006204 | BRIDGE DECK HYDRO-SCARIFICATION 1/2" | SO YD | 8286 | | 1507 | 1248 | 534 | 534 | 1422 | 1166 | | 1875 | |
| Z0016002 | DECK SLAB REPAIR (FULL DEPTH, TYPE II) | SO YD | 52 | | 17 | 15 | 1 | 8 | | | | 11 | |
| Z0030150 | IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3 | EACH | 2 | 2 | | | | | | | | | |
| Z0030250 | IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3 | EACH | 6 | 6 | | | | | | | | | |
| Z0030255 | IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2 | EACH | 2 | 2 | | | | | | | | | |
| Z0030350 | IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3 | EACH | 4 | 4 | | | | | | | | | |
| Z0075300 | TIE BARS | EACH | 2641 | 2641 | | | | | | | | | |
| Z0076600 | TRAINEES | HOUR | 1000 | 1000 | | | | | | | | | |

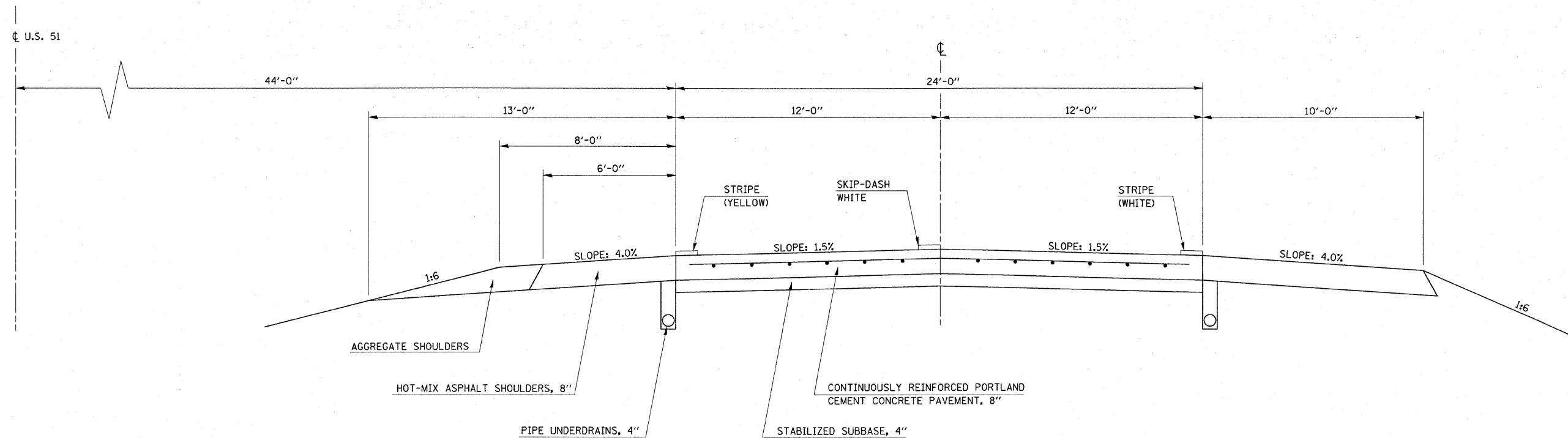
EXISTING TYPICAL CROSS SECTION

F.A.P. 322

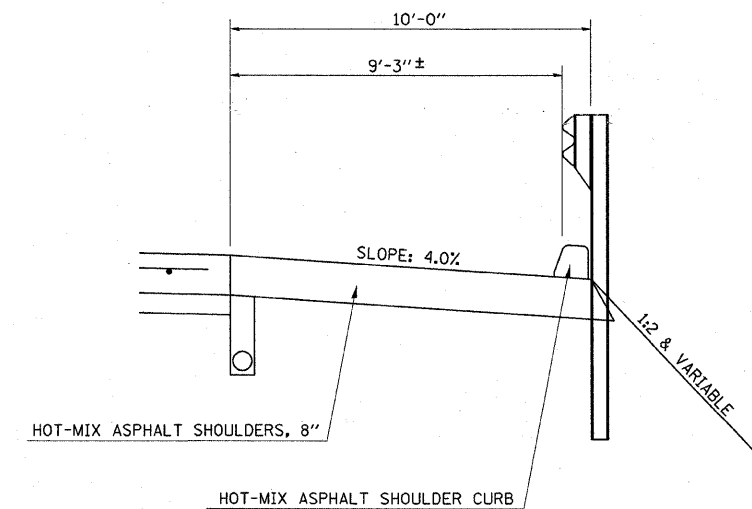
| STATION | TO | STATION |
|-----------|----|-----------|
| 120+00.00 | | 367+40.00 |

BRIDGE OMISSION

| STATION | TO | STATION |
|-----------|----|-----------|
| 132+49.30 | | 135+39.34 |
| 141+84.12 | | 143+20.27 |
| 233+12.40 | | 235+86.50 |



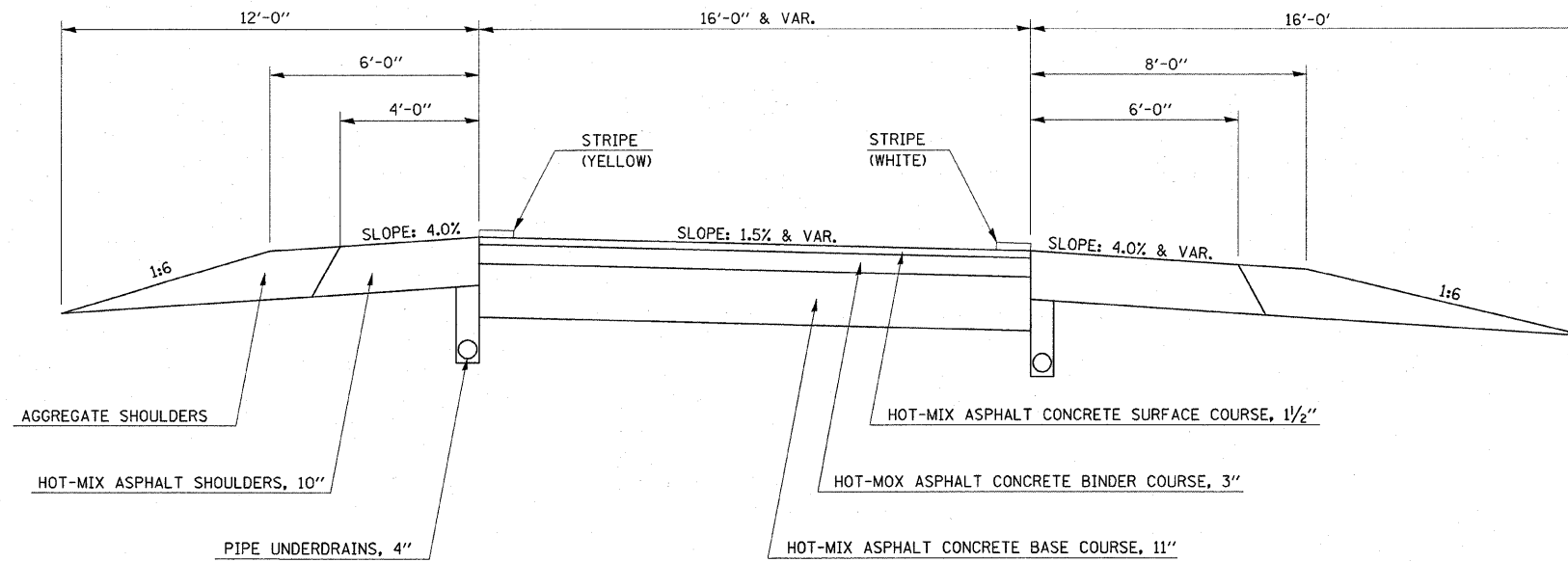
| STATION | TO | STATION |
|-----------|----|-----------|
| 135+75.34 | | 141+74.74 |
| 236+22.50 | | 243+19.77 |



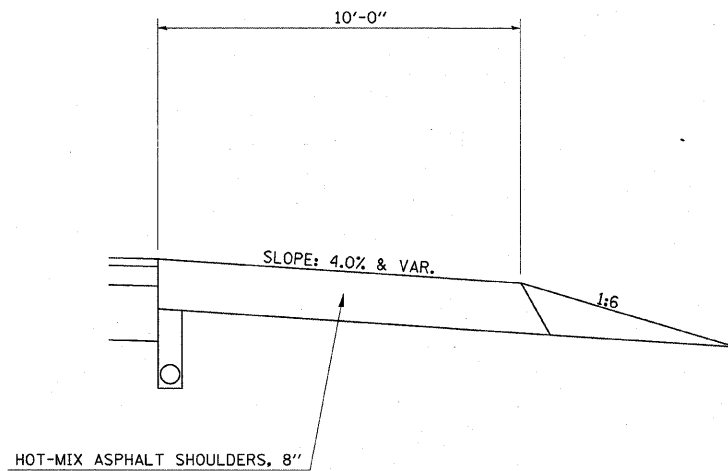
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|--|-----------------------------|------------|-----------|---|-------------------------------|--|--|-----------------|-------------------------|--------------|--------------------|-------------|
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| | 4150-ahst-typical1.dgn | DRAWN - | REVISED - | | | | | SCALE: NA | SHEET NO. 1 OF 2 SHEETS | STA. TO STA. | CONTRACT NO. 74150 | |
| | PLOT SCALE = 28.0000' / IN. | CHECKED - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |
| | PLOT DATE = 3/17/2010 | DATE - | REVISED - | | | | | | | | | |

EXISTING RAMP TYPICAL CROSS SECTION

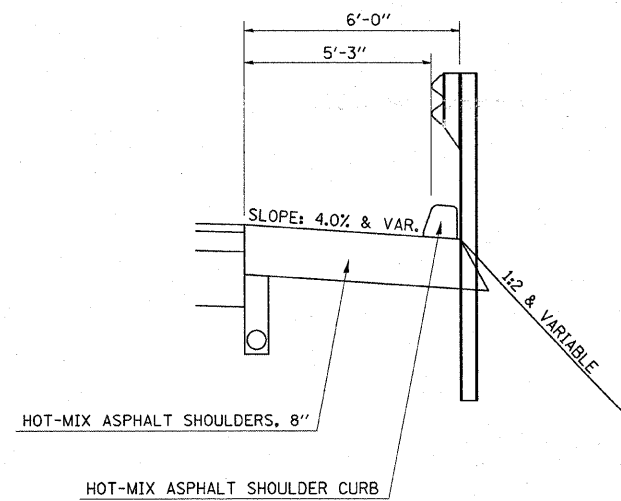
RAMPS A, B, C & D (U.S. 51 / U.S. (BUS.) 51)
 RAMPS A, B, C & D (U.S. 51 / IL. 48)



STATION TO STATION
 316+55.93 TO 322+13.18 (RAMP B U.S. 51 / U.S. (BUS.) 51)
 700+65.96 TO 706+00.00 (RAMP C U.S. 51 / U.S. (BUS.) 51)



STATION TO STATION
 301+74.99 TO 307+96.00 (RAMP B U.S. 51 / U.S. (BUS.) 51)
 714+50.00 TO 718+43.14 (RAMP C U.S. 51 / I.S. (BUS.) 51)



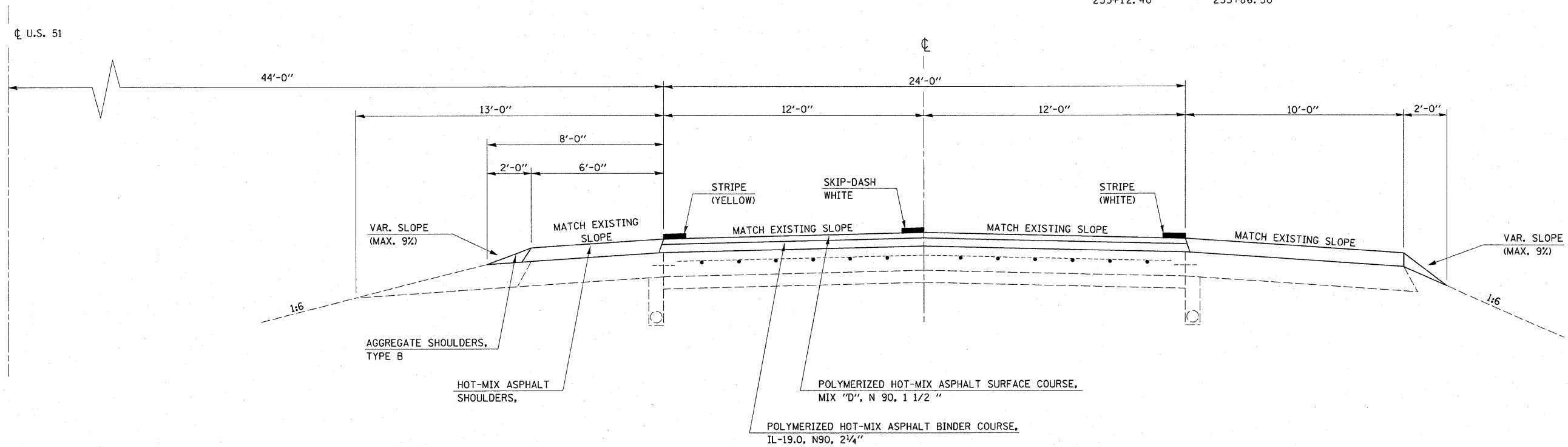
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|---|------------------------------|------------|-----------|---|-------------------------------|-------------------------|------|-------------|--------------------|--------|--------------|-----------|
| FILE NAME = c:\pwork\PWIDOT\SWARTZRW\d0138944\d7 | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL CROSS SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 1150-shr-typicals.dgn | DRAWN - | REVISED - | | | | | 322 | (58-20)RS | MACON | 151 | 8 |
| | PLOT SCALE = 28.0000 ' / IN. | CHECKED - | REVISED - | | SCALE: | SHEET NO. 2 OF 2 SHEETS | STA. | TO STA. | CONTRACT NO. 74150 | | | |
| | PLOT DATE = 3/17/2010 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |

PROPOSED TYPICAL CROSS SECTION

F.A.P. 322

| STATION | TO | STATION |
|-----------|----|-----------|
| 120+00.00 | | 367+40.00 |

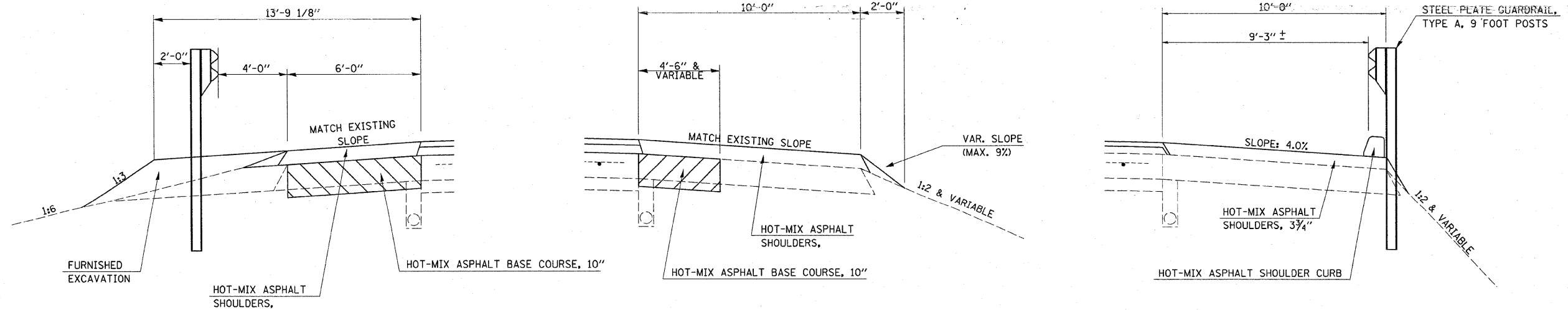
| BRIDGE OMISSION | | |
|-----------------|----|-----------|
| STATION | TO | STATION |
| 132+49.30 | | 135+39.34 |
| 141+84.12 | | 143+20.27 |
| 233+12.40 | | 235+86.50 |



| STATION | TO | STATION |
|-----------|----|-----------|
| 129+62.68 | | 132+38.76 |
| 135+58.14 | | 141+74.79 |
| 143+30.12 | | 146+06.54 |
| 229+61.23 | | 232+93.66 |
| 236+04.77 | | 239+07.43 |

| STATION | TO | STATION |
|-----------|----|-----------|
| 135+41.24 | | 141+74.76 |
| 143+29.87 | | 146+56.20 |
| 236+09.18 | | 239+04.19 |

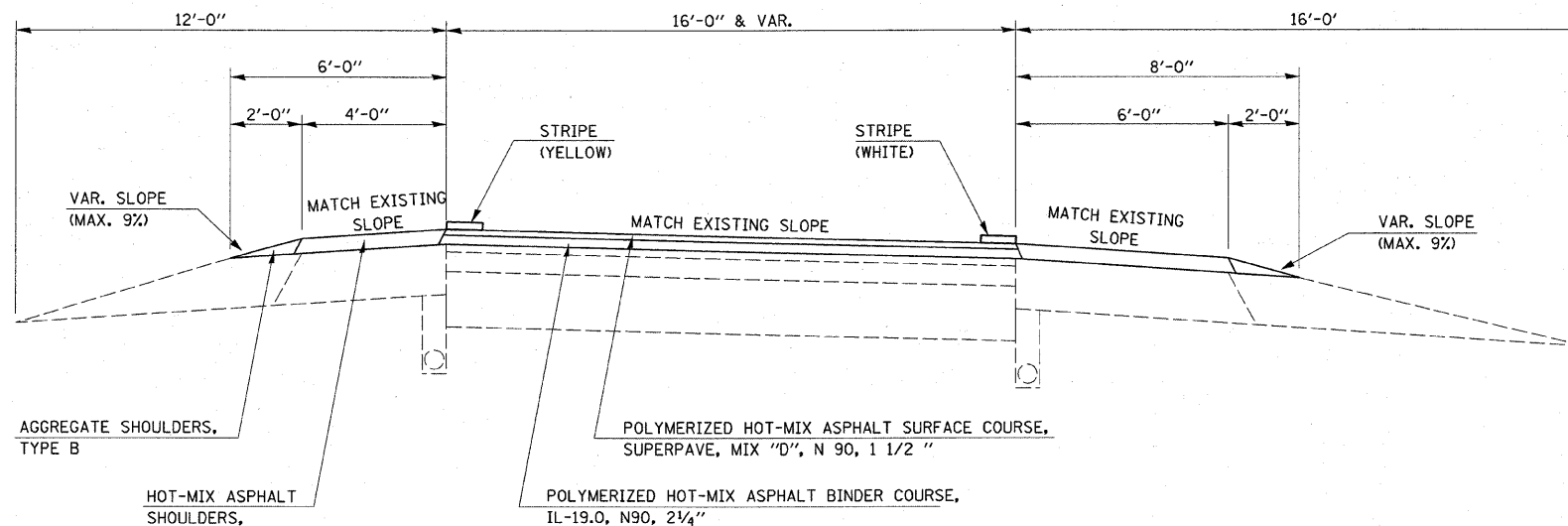
| STATION | TO | STATION |
|-----------|----|-----------|
| 135+58.65 | | 142+11.49 |
| 232+51.04 | | 232+78.12 |
| 236+12.67 | | 244+22.55 |



| | | | | | | | | | | | | |
|---------------------------------------|-----------------------------|------------|-----------|---|-------------------------------|-------------------------|------|-----------------|--------------------|--------------|------------------|-------------|
| FILE NAME = | USER NAME = swartzw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL CROSS SECTIONS | | | F.A.P. RTE. 322 | SECTION (58-20)RS | COUNTY MACON | TOTAL SHEETS 151 | SHEET NO. 9 |
| es:\pwork\PW\DOT\SWARTZRW\d0138944\d7 | 4150-sh-t-typicals.dgn | DRAWN - | REVISED - | | SCALE: NA | SHEET NO. 1 OF 2 SHEETS | STA. | TO STA. | CONTRACT NO. 74150 | | | |
| | PLOT SCALE = 20,0000' / IN. | CHECKED - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |
| | PLOT DATE = 3/17/2018 | DATE - | REVISED - | | | | | | | | | |
| | | | | | | | | | | | | |

PROPOSED RAMP TYPICAL CROSS SECTION

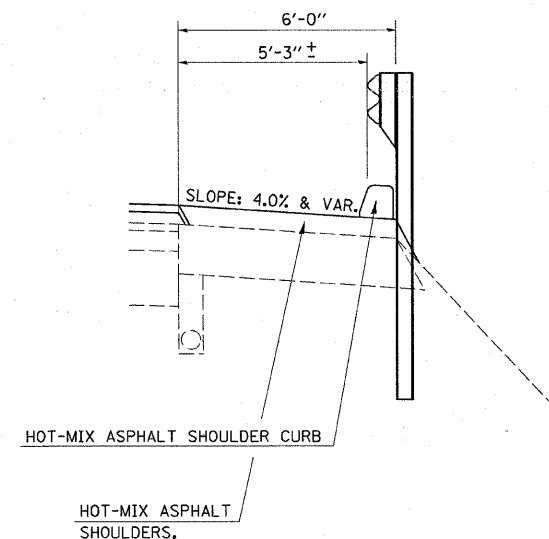
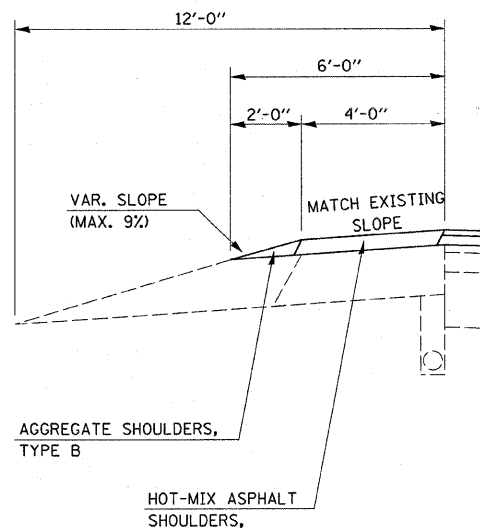
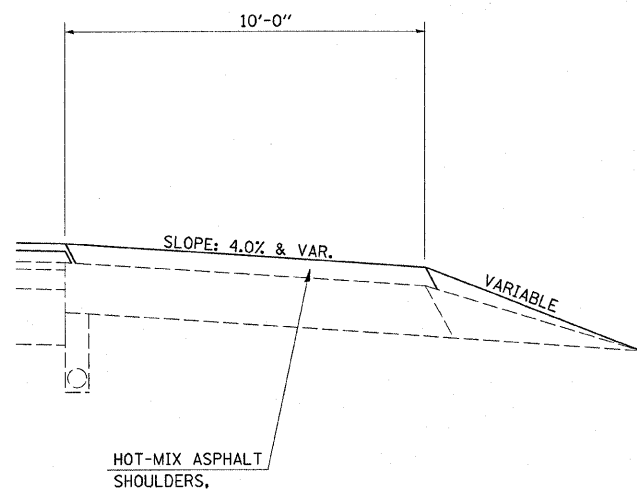
RAMPS A, B, C & D (U.S. 51 / U.S. (BUS.) 51)
RAMPS A, B, C & D (U.S. 51 / IL. 48)



| STATION | TO | STATION |
|-----------|----|---|
| 316+55.93 | | 322+13.18 (RAMP B U.S. 51 / U.S. (BUS.) 51) |
| 700+65.96 | | 706+00.00 (RAMP C U.S. 51 / U.S. (BUS.) 51) |

| STATION | TO | STATION |
|-----------|----|---|
| 714+19.35 | | 715+36.59 (RAMP C U.S. 51 / U.S. (BUS.) 51) |
| 512+22.95 | | 513+15.85 (RAMP C U.S. 51 / IL RTE 48) |

| STATION | TO | STATION |
|-----------|----|---|
| 301+92.80 | | 307+94.64 (RAMP B U.S. 51 / U.S. (BUS.) 51) |
| 713+98.75 | | 718+25.56 (RAMP C U.S. 51 / U.S. (BUS.) 51) |
| 401+75.18 | | 401+97.37 (RAMP B U.S. 51 / IL. 48) |



| | | | | | | | | | | | | |
|---------------------------------------|-----------------------------|------------|-----------|---|--|-----------|-------|--------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL CROSS SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| cs:\pwork\PWIDOT\SWARTZRW\d0138944\d7 | 4150-sh-t-typicals.dgn | DRAWN - | REVISED - | | 322 | (58-20)RS | MACON | 151 | 10 | | | |
| | PLOT SCALE = 20,0000' / IN. | CHECKED - | REVISED - | | SCALE: NA SHEET NO. 2 OF 2 SHEETS STA. TO STA. | | | CONTRACT NO. 74150 | | | | |
| | PLOT DATE = 3/17/2010 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | | | | | | | | |

| PAVING SCHEDULE | | LENGTH | PAVEMENT WIDTH | SHOULDER WIDTH | AREA | BITUMINOUS MATERIALS (PRIME COAT) | AGGREGATE (PRIME COAT) | LEVELING BINDER (MACHINE METHOD), N90 | POLYMERIZED HOT MIX ASPHALT SURFACE COURSE, MIX "D", N90 | POLYMERIZED HOT MIX ASPHALT BINDER COURSE, 1L-19.0, N90 | HOT MIX ASPHALT SHOULDERS | SHOULDER RUMBLE STRIPS | AGGREGATE SHOULDERS, TYPE B | HOT-MIX ASPHALT BASE COURSE, 10" |
|-----------------|--------------|---------|----------------|----------------|----------|-----------------------------------|------------------------|---------------------------------------|--|---|---------------------------|------------------------|-----------------------------|----------------------------------|
| STATION | TO STATION | FEET | FEET | FEET | SQ YD | GALLON | TON | TON | TON | TON | TON | FOOT | TON | SQ YD |
| 118+87.50 | TO 120+00.00 | 112.5 | 48.0 | 32.0 | 600.0 | 100.0 | 2.0 | | 50.4 | 75.6 | 84.0 | 450.0 | 10.7 | |
| 120+00.00 | TO 127+11.51 | 711.5 | 48.0 | 32.0 | 3,794.7 | 632.5 | 12.6 | | 318.8 | 478.1 | 531.3 | 2,846.0 | 67.5 | |
| 127+11.51 | TO 129+45.54 | 234.0 | 48.0 | 22.0 | 1,248.2 | 156.0 | 3.1 | | 104.8 | 157.3 | 194.9 | 702.1 | 16.7 | 15.1 |
| 129+45.54 | TO 130+63.26 | 117.7 | 48.0 | 12.0 | 627.8 | 78.5 | 1.6 | | 52.7 | 79.1 | 79.9 | 235.4 | 5.6 | 281.0 |
| 130+63.26 | TO 131+06.04 | 42.8 | 48.0 | 12.0 | 228.2 | 28.5 | 0.6 | | 19.2 | 28.7 | 19.5 | 85.6 | 2.0 | 92.8 |
| 131+06.04 | TO 131+50.00 | 44.0 | 48.0 | 12.0 | 234.5 | 29.3 | 0.6 | 2.0 | 19.7 | 29.5 | 18.9 | 87.9 | 2.1 | 90.0 |
| 131+50.00 | TO 132+13.05 | 63.0 | 48.0 | 12.0 | 336.3 | 42.0 | 0.8 | 18.9 | 28.2 | 42.4 | 17.7 | 274.2 | 3.0 | 78.1 |
| 132+13.05 | TO 135+75.47 | 362.4 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 4.4 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 29.0 |
| 135+75.47 | TO 136+82.34 | 106.9 | 48.0 | 32.0 | 570.0 | 95.0 | 1.9 | 14.4 | 47.9 | 71.8 | 79.8 | 450.0 | 5.1 | 190.2 |
| 136+82.34 | TO 140+71.07 | 388.7 | 48.0 | 32.0 | 2,073.2 | 345.5 | 6.9 | 1.5 | 174.2 | 261.2 | 290.3 | 1,554.9 | 18.4 | 792.4 |
| 140+71.07 | TO 141+84.12 | 113.1 | 48.0 | 32.0 | 602.9 | 100.5 | 2.0 | 16.2 | 50.6 | 76.0 | 84.4 | 450.0 | 5.4 | 299.0 |
| 141+84.12 | TO 143+20.27 | 136.2 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.2 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 71.0 |
| 143+20.27 | TO 144+33.32 | 113.0 | 48.0 | 32.0 | 602.9 | 100.5 | 2.0 | 10.0 | 50.6 | 76.0 | 84.4 | 450.0 | 10.7 | 222.2 |
| 144+33.32 | TO 162+35.00 | 1,801.7 | 48.0 | 32.0 | 9,609.0 | 1,601.5 | 32.0 | 7.7 | 807.2 | 1,210.7 | 1,345.3 | 6,988.2 | 171.0 | 545.0 |
| 162+35.00 | TO 204+67.14 | 4,232.1 | 48.0 | 32.0 | 22,571.4 | 3,761.9 | 75.2 | | 1,896.0 | 2,844.0 | 3,160.0 | 16,854.0 | 401.7 | |
| 204+67.14 | TO 212+17.27 | 750.1 | 48.0 | 22.0 | 4,000.7 | 583.4 | 11.7 | | 336.1 | 504.1 | 385.1 | 2,250.4 | 53.4 | |
| 212+17.27 | TO 212+38.70 | 21.4 | 48.0 | 32.0 | 114.3 | 19.0 | 0.4 | | 9.6 | 14.4 | 17.1 | 42.9 | 1.5 | |
| 212+38.70 | TO 215+59.71 | 321.0 | 48.0 | 12.0 | 1,712.1 | 214.0 | 4.3 | | 143.8 | 215.7 | 132.1 | 642.0 | 15.2 | |
| 215+59.71 | TO 216+49.66 | 90.0 | 48.0 | 32.0 | 479.7 | 80.0 | 1.6 | | 40.3 | 60.4 | 104.2 | 179.9 | 4.3 | |
| 216+49.66 | TO 219+00.03 | 250.4 | 48.0 | 22.0 | 1,335.3 | 194.7 | 3.9 | | 112.2 | 168.2 | 206.7 | 751.1 | 17.8 | |
| 219+00.03 | TO 227+92.37 | 892.3 | 48.0 | 32.0 | 4,759.1 | 793.2 | 15.9 | | 399.8 | 599.7 | 666.3 | 3,569.4 | 84.7 | |
| 227+92.37 | TO 229+67.39 | 175.0 | 48.0 | 22.0 | 933.4 | 136.1 | 2.7 | | 78.4 | 117.6 | 152.7 | 525.1 | 12.5 | 4.1 |
| 229+67.39 | TO 231+33.02 | 165.6 | 48.0 | 12.0 | 883.4 | 110.4 | 2.2 | | 74.2 | 111.3 | 113.2 | 331.3 | 11.8 | 398.6 |
| 231+33.02 | TO 231+69.78 | 36.8 | 48.0 | 12.0 | 196.1 | 24.5 | 0.5 | | 16.5 | 24.7 | 20.1 | 73.5 | 3.5 | 77.5 |
| 231+69.78 | TO 231+95.62 | 25.8 | 48.0 | 12.0 | 137.8 | 17.2 | 0.3 | | 11.6 | 17.4 | 7.2 | 51.7 | 2.5 | 52.6 |
| 231+95.62 | TO 232+76.17 | 80.5 | 48.0 | 12.0 | 429.6 | 53.7 | 1.1 | 22.7 | 36.1 | 54.1 | 22.6 | 172.1 | 7.6 | 106.1 |
| 232+76.17 | TO 236+22.50 | 346.8 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 9.5 |
| 236+22.50 | TO 237+29.51 | 107.0 | 48.0 | 22.0 | 570.7 | 83.2 | 1.7 | 15.4 | 47.9 | 71.9 | 54.9 | 337.5 | 5.1 | 194.8 |
| 237+29.51 | TO 241+49.81 | 420.3 | 48.0 | 22.0 | 2,241.6 | 326.9 | 6.5 | | 188.3 | 282.4 | 215.8 | 1,260.9 | 39.9 | 278.0 |
| 241+49.81 | TO 268+70.00 | 2,720.2 | 48.0 | 32.0 | 14,507.7 | 2,417.9 | 48.4 | | 1,218.6 | 1,828.0 | 2,031.1 | 10,805.9 | 258.2 | |
| 268+70.00 | TO 331+45.00 | 6,275.0 | 48.0 | 32.0 | 33,466.7 | 5,577.8 | 111.6 | | 2,811.2 | 4,216.8 | 4,685.3 | 25,025.1 | 595.5 | |
| 331+45.00 | TO 341+54.50 | 1,009.5 | 48.0 | 32.0 | 5,384.0 | 897.3 | 17.9 | 0.1 | 452.3 | 678.4 | 753.8 | 3,962.8 | 95.8 | |
| 341+54.50 | TO 342+67.00 | 112.5 | 48.0 | 32.0 | 600.0 | 100.0 | 2.0 | 17.9 | 50.4 | 75.6 | 84.0 | 374.8 | 10.7 | |
| 342+67.00 | TO 350+22.00 | 755.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 13.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| 350+22.00 | TO 351+34.50 | 112.5 | 48.0 | 32.0 | 600.0 | 100.0 | 2.0 | 5.0 | 50.4 | 75.6 | 84.0 | 450.0 | 10.7 | |
| 351+34.50 | TO 367+40.00 | 1,605.5 | 48.0 | 32.0 | 8,562.7 | 1,427.1 | 28.5 | | 719.3 | 1,078.9 | 1,198.8 | 6,422.0 | 152.4 | |
| SHEET TOTALS | | 24,853 | | | 124,014 | 20,228 | 405 | 151 | 10,417 | 15,626 | 16,925 | 88,657 | 2,103 | 3,827 |

| | | | | | | | | | | | | | |
|---------------------------------------|----------------------|------------|-----------|---|-------------------------------|--------------------------|------|---------------------------|---------|-----------|--------------|-----------|----|
| FILE NAME = | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SCHEDULE OF QUANTITIES | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| c:\pwork\PWIDOT\SWARTZRW\d0138944.dwg | 4158-ahh-sch.dgn | DRAWN - | REVISED - | | SCALE: NA | SHEET NO. 1 OF 11 SHEETS | STA. | TO STA. | 322 | (58,20)RS | MACON | 151 | 11 |
| | | CHECKED - | REVISED - | | | | | CONTRACT NO. 74150 | | | | | |
| | | DATE - | REVISED - | | | | | ILLINOIS FED. AID PROJECT | | | | | |
| | | | | | | | | | | | | | |

| PAVING SCHEDULE (Cont'd) | | | LENGTH | PAVEMENT WIDTH | SHOULDER WIDTH | AREA | BITUMINOUS MATERIALS (PRIME COAT) | AGGREGATE (PRIME COAT) | LEVELING BINDER (MACHINE METHOD), N90 | POLYMERIZED HOT MIX ASPHALT SURFACE COURSE, MIX "D", N90 | POLYMERIZED HOT MIX ASPHALT BINDER COURSE, IL-19, 0, N90 | HOT MIX ASPHALT SHOULDERS | SHOULDER RUMBLE STRIPS | AGGREGATE SHOULDERS, TYPE B | HOT-MIX ASPHALT BASE COURSE, 10" |
|--------------------------|--------------|---------|------------|----------------|----------------|---------|-----------------------------------|------------------------|---------------------------------------|--|--|---------------------------|------------------------|-----------------------------|----------------------------------|
| STATION TO | STATION | FEET | FEET | FEET | SQ YD | GALLON | TON | TON | TON | TON | TON | TON | FOOT | TON | SQ YD |
| Ramp A US (Bus) 51 | | | | | | | | | | | | | | | |
| 200+38.53 | TO 201+13.53 | 75.0 | *Var Table | *Var Table | 9,000.0 | 50.2 | 1.0 | | 30.7 | 46.0 | 28.7 | | | 3.6 | |
| 201+13.53 | TO 201+57.02 | 43.5 | *Var Table | 10.0 | 56,920.8 | 13.4 | 0.3 | | 7.2 | 10.8 | 10.1 | | | 2.1 | |
| 201+57.02 | TO 215+79.76 | 1,422.7 | 16.0 | 10.0 | 14,041.8 | 411.0 | 8.2 | | 212.5 | 318.7 | 332.0 | | | 67.5 | |
| 215+79.76 | TO 216+92.26 | 112.5 | 16.0 | 10.0 | 7,063.2 | 32.5 | 0.7 | | 16.8 | 25.2 | 26.3 | | | 5.3 | |
| Ramp B US (Bus) 51 | | | | | | | | | | | | | | | |
| 301+95.88 | TO 303+05.85 | 110.0 | *Var Table | 6.0 | 3,955.2 | 43.9 | 0.9 | | 30.2 | 45.3 | 16.8 | | | 0.0 | |
| 303+05.85 | TO 303+22.35 | 16.5 | *Var Table | 6.0 | 395.8 | 4.4 | 0.1 | | 2.8 | 4.2 | 2.3 | | | 0.0 | |
| 303+22.35 | TO 306+62.39 | 340.0 | 16.0 | 6.0 | 7,480.9 | 83.1 | 1.7 | | 50.8 | 76.2 | 47.6 | | | 0.0 | |
| 306+62.39 | TO 316+55.93 | 993.5 | 16.0 | 6.0 | 21,857.9 | 242.9 | 4.9 | | 148.4 | 222.6 | 139.1 | | | 47.1 | |
| 316+55.93 | TO 320+79.65 | 423.7 | 16.0 | 10.0 | 11,016.7 | 122.4 | 2.4 | | 63.3 | 94.9 | 98.9 | | | 20.1 | |
| 320+79.65 | TO 321+11.38 | 31.7 | *Var Table | 10.0 | 881.4 | 9.8 | 0.2 | | 5.3 | 7.9 | 7.4 | | | 1.5 | |
| 321+11.38 | TO 321+86.38 | 75.0 | *Var Table | *Var Table | 5,217.7 | 58.0 | 1.2 | | 32.5 | 48.7 | 40.5 | | | 3.6 | |
| Ramp C US (Bus) 51 | | | | | | | | | | | | | | | |
| 700+36.24 | TO 701+11.24 | 75.0 | *Var Table | *Var Table | 4,822.2 | 53.6 | 1.1 | | 29.5 | 44.3 | 38.7 | | | 3.6 | |
| 701+11.24 | TO 701+51.09 | 39.8 | *Var Table | 10.0 | 1,095.2 | 12.2 | 0.2 | | 6.5 | 9.8 | 9.3 | | | 1.9 | |
| 701+51.09 | TO 706+00.00 | 448.9 | 16.0 | 10.0 | 11,671.7 | 129.7 | 2.6 | | 67.0 | 100.6 | 104.7 | | | 21.3 | |
| 706+00.00 | TO 715+34.74 | 934.7 | 16.0 | 10.0 | 24,303.2 | 270.0 | 5.4 | | 139.6 | 209.4 | 218.1 | | | 0.0 | 114.9 |
| 715+34.74 | TO 717+17.96 | 183.2 | 16.0 | 6.0 | 4,030.8 | 44.8 | 0.9 | | 27.4 | 41.0 | 25.7 | | | 0.0 | |
| 717+17.96 | TO 718+24.03 | 106.1 | *Var Table | 6.0 | 2,543.5 | 28.3 | 0.6 | | 17.5 | 26.2 | 15.7 | | | 0.0 | |
| 718+24.03 | TO 721+79.87 | 355.8 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | 0.0 | 0.0 | 0.0 | | | 0.0 | |
| 721+79.87 | TO 727+42.81 | 562.9 | *Var Table | 6.0 | 6,584.3 | 73.2 | 1.5 | | 29.9 | 44.9 | 78.8 | | | 0.0 | |
| Ramp D US (Bus) 51 | | | | | | | | | | | | | | | |
| 901+68.28 | TO 902+80.78 | 112.5 | 16.0 | 10.0 | 2,925.0 | 32.5 | 0.7 | | 16.8 | 25.2 | 26.3 | | | 5.3 | |
| 902+80.78 | TO 923+46.16 | 2,065.4 | 16.0 | 10.0 | 53,699.9 | 596.7 | 11.9 | | 308.4 | 462.6 | 481.9 | | | 98.0 | |
| 923+46.16 | TO 923+72.45 | 26.3 | *Var Table | 10.0 | 702.8 | 7.8 | 0.2 | | 4.1 | 6.2 | 6.1 | | | 1.2 | |
| 923+72.45 | TO 924+47.45 | 75.0 | *Var Table | *Var Table | 5,027.2 | 55.9 | 1.1 | | 33.3 | 50.0 | 34.0 | | | 3.6 | |
| Ramp A IL 48 | | | | | | | | | | | | | | | |
| 300+35.00 | TO 301+10.00 | 75.0 | *Var Table | *Var Table | 3,289.1 | 36.5 | 0.7 | | 26.4 | 39.6 | 10.7 | | | 3.6 | |
| 301+10.00 | TO 301+38.47 | 28.5 | *Var Table | 10.0 | 761.7 | 8.5 | 0.2 | | 4.5 | 6.7 | 6.6 | | | 1.4 | |
| 301+38.47 | TO 320+98.54 | 1,960.1 | 16.0 | 10.0 | 50,961.8 | 566.2 | 11.3 | | 292.7 | 439.1 | 457.3 | | | 93.0 | |
| 320+98.54 | TO 325+28.35 | 429.8 | 16.0 | 6.0 | 9,455.8 | 105.1 | 0.0 | | 64.2 | 96.3 | 60.2 | | | 0.0 | |
| 325+28.35 | TO 332+78.35 | 750.0 | *Var Table | 6.0 | 10,876.2 | 120.8 | 2.4 | | 59.5 | 89.3 | 105.0 | | | 0.0 | |
| SHEET TOTALS | | | 11,873 | | | 330,582 | 3,213 | 62 | 0 | 1,728 | 2,591 | 2,429 | 0 | 384 | 115 |

| | | | | | | | | | | | | |
|--------------------------------------|-----------------------------|------------|-----------|---|-------------------------------|--------------------------|------|-------------|---------|--------------------|---------------------------|-----------|
| FILE NAME = | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SCHEDULE OF QUANTITIES | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwork\PWIDOT\SWARTZRW\d0138944\d7 | 4150-sht-sch.dgn | DRAWN - | REVISED - | | SCALE: NA | SHEET NO. 2 OF 11 SHEETS | STA. | TO STA. | 322 | (58,20)RS | MACON | 151 12 |
| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - | | | | | | | CONTRACT NO. 74150 | | |
| | PLOT DATE = 3/17/2010 | DATE - | REVISED - | | | | | | | | ILLINOIS FED. AID PROJECT | |

| PAVING SCHEDULE (Cont' d) | | | LENGTH | PAVEMENT WIDTH | SHOULDER WIDTH | AREA | BITUMINOUS MATERIALS (PRIME COAT) | AGGREGATE (PRIME COAT) | LEVELING BINDER (MACHINE METHOD), N90 | POLYMERIZED HOT MIX ASPHALT SURFACE COURSE, MIX "D", N90 | POLYMERIZED HOT MIX ASPHALT BINDER COURSE, IL-19.0, N90 | HOT MIX ASPHALT SHOULDERS | SHOULDER RUMBLE STRIPS | AGGREGATE SHOULDERS, TYPE B | HOT-MIX ASPHALT BASE COURSE, 10" |
|---------------------------|----|-----------|---------|----------------|----------------|---------|-----------------------------------|------------------------|---------------------------------------|--|---|---------------------------|------------------------|-----------------------------|----------------------------------|
| STATION | TO | STATION | FEET | FEET | FEET | SQ YD | GALLON | TON | TON | TON | TON | TON | FOOT | TON | SQ YD |
| Ramp B IL 48 | | | | | | | | | | | | | | | |
| 401+75.68 | TO | 402+83.44 | 107.8 | *Var Table | 6.0 | 232.5 | 23.2 | 0.5 | | 13.2 | 19.7 | 15.9 | | 0.0 | |
| 402+83.44 | TO | 403+22.46 | 39.0 | *Var Table | 6.0 | 99.6 | 10.0 | 0.2 | | 6.2 | 9.3 | 5.5 | | 0.0 | |
| 403+22.46 | TO | 406+62.40 | 339.9 | 16.0 | 6.0 | 831.0 | 83.1 | 1.7 | | 50.8 | 76.1 | 47.6 | | 0.0 | |
| 406+62.40 | TO | 423+94.12 | 1,731.7 | 16.0 | 10.0 | 5,002.7 | 500.3 | 10.0 | | 258.6 | 387.9 | 404.1 | | 82.2 | |
| 423+94.12 | TO | 424+26.78 | | *Var Table | 10.0 | 98.0 | 9.8 | 0.0 | | 5.2 | 7.8 | 7.6 | | 1.5 | |
| 424+26.78 | TO | 425+01.78 | 75.0 | *Var Table | *Var Table | 672.9 | 67.3 | 1.3 | | 29.5 | 44.2 | 67.6 | | 3.6 | |
| Ramp C IL 48 | | | | | | | | | | | | | | | |
| 500+35.00 | TO | 501+10.00 | 75.0 | *Var Table | *Var Table | 501.3 | 50.1 | 1.0 | | 26.4 | 39.6 | 39.3 | | 3.6 | |
| 501+10.00 | TO | 501+38.47 | 28.5 | *Var Table | 10.0 | 84.6 | 8.5 | 0.2 | | 4.5 | 6.7 | 6.6 | | 1.4 | |
| 501+38.47 | TO | 513+15.85 | 1,177.4 | 16.0 | 10.0 | 3,401.3 | 340.1 | 6.8 | | 175.8 | 263.7 | 274.7 | | 55.9 | |
| 501+38.47 | TO | 515+12.69 | 1,374.2 | 16.0 | 6.0 | 3,359.2 | 335.9 | 6.7 | | 205.2 | 307.8 | 192.4 | | 0.0 | 71.7 |
| 515+12.69 | TO | 516+18.17 | 105.5 | *Var Table | 6.0 | 284.4 | 28.4 | 0.6 | | 17.7 | 26.5 | 15.5 | | 0.0 | |
| 516+18.17 | TO | 519+59.42 | | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | 0.0 | 0.0 | 0.0 | | 0.0 | |
| 519+59.42 | TO | 524+96.33 | 536.9 | *Var Table | 6.0 | 738.1 | 73.8 | 1.5 | | 31.9 | 47.9 | 75.2 | | 0.0 | |
| Ramp D IL 48 | | | | | | | | | | | | | | | |
| 599+81.00 | TO | 603+21.61 | 340.6 | *Var Table | 6.0 | 568.6 | 56.9 | 1.1 | | 28.7 | 43.0 | 47.7 | | 0.0 | |
| 603+21.61 | TO | 606+62.40 | 340.8 | 16.0 | 6.0 | 833.0 | 83.3 | 1.7 | | 50.9 | 76.3 | 47.7 | | 0.0 | |
| 606+62.40 | TO | 622+50.78 | 1,588.4 | 16.0 | 10.0 | 4,588.7 | 458.9 | 9.2 | | 237.2 | 355.8 | 370.6 | | 75.4 | |
| 622+50.78 | TO | 622+85.52 | 34.7 | *Var Table | 10.0 | 38.6 | 3.9 | 0.1 | | 0.0 | 0.0 | 8.1 | | 1.6 | |
| 622+85.52 | TO | 623+60.52 | 75.0 | *Var Table | *Var Table | 557.3 | 55.7 | 1.1 | | 30.5 | 45.8 | 40.7 | | 3.6 | |
| MEDIAN CROSS-OVERS | | | | | | | | | | | | | | | |
| 162+34.00 | | | | *Var Table | *Var Table | | | | | 28.1 | | | | 4.2 | 334.9 |
| 268+70.00 | | | | *Var Table | *Var Table | | | | | 28.1 | | | | 4.2 | 334.8 |
| 331+45.00 | | | | *Var Table | *Var Table | | | | | 28.1 | | | | 4.2 | 334.6 |
| SHEET TOTALS | | | 7,970 | | | 21,892 | 2,189 | 44 | 0 | 1,257 | 1,758 | 1,667 | 0 | 241 | 1,076 |
| PROJECT TOTALS | | | 44,697 | | | 476,488 | 25,631 | 510 | 151 | 13,401 | 19,975 | 21,021 | 88,657 | 2,728 | 5,018 |

| PATCHING SCHEDULE | | | | CLASS A PATCHES, TYPE I, 8" | CLASS A PATCHES, TYPE II, 8" | CLASS A PATCHES, TYPE III, 8" | CLASS A PATCHES, TYPE IV, 8" | PAVEMENT PATCHING (PARTIAL DEPTH) | PAVEMENT REMOVAL (PARTIAL DEPTH) | CLASS D PATCHES, TYPE II, 10" | CLASS D PATCHES, TYPE III, 10" | CLASS D PATCHES, TYPE IV, 10" | PATCHING REINFORCEMENT | SAW CUTS | TIE BARS | |
|-------------------------|----|-----------|--------|-----------------------------|------------------------------|-------------------------------|------------------------------|-----------------------------------|----------------------------------|-------------------------------|--------------------------------|-------------------------------|------------------------|----------|----------|---------|
| LOCATION | | | | SQ YD | SQ YD | SQ YD | SQ YD | SQ YD | SQ YD | SQ YD | | | SQ YD | FOOT | EACH | |
| US 51 | | | | | | | | | | | | | | | | |
| NB / SB | | | | 1,084.4 | 1,533.6 | 797.9 | 2,691.5 | 13,632.7 | 13,632.7 | 0.0 | | | | 6,107.4 | 32,484.5 | 2,641.0 |
| 131+33.19 | TO | 131+37.19 | RAMP B | | | | | | | 11.7 | | | | | | |
| 131+57.40 | TO | 131+61.40 | NB-DL | | | | | | | 5.3 | | | | | | |
| 131+57.40 | TO | 131+61.40 | NB-PL | | | | | | | 5.3 | | | | | | |
| 131+33.19 | TO | 131+37.19 | SB-PL | | | | | | | 5.3 | | | | | | |
| 136+27.24 | TO | 136+31.24 | SB-DL | | | | | | | 5.3 | | | | | | |
| 136+27.24 | TO | 136+31.24 | SB-PL | | | | | | | 5.3 | | | | | | |
| 136+51.44 | TO | 136+55.44 | NB | | | | | | | 9.7 | | | | | | |
| 136+51.44 | TO | 136+55.44 | NB-PL | | | | | | | 5.3 | | | | | | |
| 140+53.73 | TO | 140+57.73 | SB-DL | | | | | | | 5.3 | | | | | | |
| 140+53.73 | TO | 140+57.73 | SB-PL | | | | | | | 5.3 | | | | | | |
| 141+06.52 | TO | 141+10.52 | NB-DL | | | | | | | 5.3 | | | | | | |
| 141+06.52 | TO | 141+10.52 | NB-PL | | | | | | | 5.3 | | | | | | |
| 143+93.87 | TO | 143+97.87 | SB-DL | | | | | | | 5.3 | | | | | | |
| 143+93.87 | TO | 143+97.87 | SB-PL | | | | | | | 5.3 | | | | | | |
| 144+46.67 | TO | 144+50.67 | NB-DL | | | | | | | 5.3 | | | | | | |
| 144+46.67 | TO | 144+50.67 | NB-PL | | | | | | | 5.3 | | | | | | |
| 232+11.23 | TO | 232+15.23 | RAMP B | | | | | | | 11.5 | | | | | | |
| 232+05.62 | TO | 232+09.62 | NB-DL | | | | | | | 5.3 | | | | | | |
| 232+05.62 | TO | 232+09.62 | NB-PL | | | | | | | 5.3 | | | | | | |
| 232+05.62 | TO | 232+09.62 | SB-PL | | | | | | | 5.3 | | | | | | |
| 232+11.23 | TO | 232+15.23 | NB-DL | | | | | | | 9.9 | | | | | | |
| 236+83.70 | TO | 236+87.70 | NB-PL | | | | | | | 5.3 | | | | | | |
| 236+83.70 | TO | 236+87.70 | SB-DL | | | | | | | 5.3 | | | | | | |
| 236+89.31 | TO | 236+93.31 | SB-PL | | | | | | | 5.3 | | | | | | |
| 236+89.31 | TO | 236+93.31 | SB-DL | | | | | | | 5.3 | | | | | | |
| 341+43.15 | TO | 341+47.15 | SB-PL | | | | | | | 5.3 | | | | | | |
| 341+43.15 | TO | 341+47.15 | NB-DL | | | | | | | 5.3 | | | | | | |
| 341+83.92 | TO | 341+87.92 | NB-PL | | | | | | | 5.3 | | | | | | |
| 341+83.92 | TO | 341+87.92 | SB-DL | | | | | | | 5.3 | | | | | | |
| 351+01.09 | TO | 351+05.09 | SB-PL | | | | | | | 5.3 | | | | | | |
| 351+01.09 | TO | 351+05.09 | NB-DL | | | | | | | 5.3 | | | | | | |
| 351+41.84 | TO | 351+45.84 | NB-PL | | | | | | | 5.3 | | | | | | |
| NB US 51/BUS. 51-RAMP C | | | | | | | | | | | | | | | | |
| 717+43.10 | TO | 717+54.22 | | | | | | | | | 24.6 | | | | | |
| NB US 51/IL. 48-RAMP C | | | | | | | | | | | | | | | | |
| 515+41.94 | TO | 515+55.95 | | | | | | | | | 31.1 | | | | | |
| PROJECT TOTALS | | | | 1,084 | 1,534 | 798 | 2,692 | 13,633 | 13,633 | 192 | 25 | 31 | 6,107 | 32,485 | 2,641 | |

| GUARDRAIL SCHEDULE | | | LOCATION | STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS | STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS | TRAFFIC BARRIER TERMINAL, TYPE 1 SPECIAL (TANGENT) | TRAFFIC BARRIER TERMINAL, TYPE 6 | TRAFFIC BARRIER TERMINAL, TYPE 2 | GUARDRAIL MARKERS, TYPE A (CRYSTAL) | GUARDRAIL MARKERS, TYPE A (AMBER) | TERMINAL MARKERS-DIRECT APPLIED |
|--------------------|----|-----------|----------|--|--|--|----------------------------------|----------------------------------|-------------------------------------|-----------------------------------|---------------------------------|
| STATION | TO | STATION | | FEET | FEET | EACH | EACH | EACH | EACH | | EACH |
| US 51 | | | | | | | | | | | |
| 128+77.73 | TO | 129+27.15 | NB MED | | | 1.0 | | | | | 1.0 |
| 129+27.15 | TO | 130+75.44 | NB MED | 150.0 | | | | | | 2.0 | |
| 130+75.44 | TO | 132+00.10 | *NB MED | 125.0 | | | | | | 2.0 | |
| 132+00.26 | TO | 132+46.51 | NB MED | | | | 1.0 | | | 1.0 | |
| 135+33.43 | TO | 135+79.68 | SB O/S | | | | 1.0 | | | | |
| 135+79.68 | TO | 140+99.54 | SB O/S | | 520.0 | | | | 7.0 | | |
| 140+99.54 | TO | 141+45.78 | SB O/S | | | | 1.0 | | 1.0 | | |
| 135+42.13 | TO | 135+88.37 | SB MED | | | | 1.0 | | | 1.0 | |
| 135+88.38 | TO | 136+00.87 | SB MED | 12.5 | | | | | | | |
| 136+00.87 | TO | 137+50.77 | *SB MED | 150.0 | | | | | | 2.0 | |
| 137+50.77 | TO | 138+63.27 | SB MED | 112.5 | | | | | | 2.0 | |
| 138+63.27 | TO | 139+13.27 | SB MED | | | 1.0 | | | | | 1.0 |
| 135+58.65 | TO | 136+04.90 | NB MED | | | | 1.0 | | | 1.0 | |
| 136+04.90 | TO | 135+17.40 | NB MED | 12.5 | | | | | | 1.0 | |
| 136+17.40 | TO | 136+29.90 | NB MED | | | | | 1.0 | | | |
| 135+69.10 | TO | 136+15.32 | NB O/S | | | | 1.0 | | 4.0 | | |
| 136+15.32 | TO | 141+72.99 | NB O/S | | 557.8 | | | | 3.0 | | |
| 141+72.99 | TO | 142+19.24 | NB O/S | | | | 1.0 | | 1.0 | | |
| 138+29.34 | TO | 138+79.34 | NB MED | | | 1.0 | | | | | 1.0 |
| 138+79.34 | TO | 140+41.84 | NB MED | 162.5 | | | | | | 2.0 | |
| 140+41.84 | TO | 141+54.25 | *NB MED | 112.5 | | | | | | 2.0 | |
| 141+54.25 | TO | 141+00.54 | NB MED | | | | 1.0 | | | 1.0 | |
| 142+85.31 | TO | 143+31.56 | SB O/S | | | | 1.0 | | 1.0 | | |
| 143+31.51 | TO | 145+44.01 | SB O/S | | 212.5 | | | | 3.0 | | |
| 145+44.01 | TO | 145+94.01 | SB O/S | | | | 1.0 | | | | 1.0 |
| 143+04.26 | TO | 143+50.51 | SB MED | | | | 1.0 | | | 1.0 | |
| 143+50.46 | TO | 144+62.96 | *SB MED | 112.5 | | | | | | 1.0 | |
| 144+62.96 | TO | 146+25.46 | SB MED | 162.5 | | | | | | 2.0 | |
| 146+25.46 | TO | 146+75.46 | SB MED | | | | 1.0 | | | | 1.0 |
| 229+28.26 | TO | 229+78.26 | NB MED | | | 1.0 | | | | | 1.0 |
| 229+78.26 | TO | 231+03.26 | NB MED | 125.0 | | | | | | 2.0 | |
| 231+03.26 | TO | 232+53.26 | *NB MED | 150.0 | | | | | | 2.0 | |
| 232+53.26 | TO | 232+99.51 | NB MED | | | | 1.0 | | | 1.0 | |
| SHEET TOTALS | | | | 1,387.5 | 1,290.3 | 6.0 | 11.0 | 1.0 | 20.0 | 26.0 | 6.0 |

* DENOTES 1:30 FLARE RATE TO 4 FOOT OFF OF PAVED SHOULDER

| GUARDRAIL SCHEDULE (Cont'd) | | | LOCATION | STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS | STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS | TRAFFIC BARRIER TERMINAL, TYPE 1 SPECIAL (TANGENT) | TRAFFIC BARRIER TERMINAL, TYPE 6 | TRAFFIC BARRIER TERMINAL, TYPE 2 | GUARDRAIL MARKERS, TYPE A (CRYSTAL) | GUARDRAIL MARKERS, TYPE A (AMBER) | TERMINAL MARKERS-DIRECT APPLIED |
|-----------------------------|----|-----------|----------|--|--|--|----------------------------------|----------------------------------|-------------------------------------|-----------------------------------|---------------------------------|
| STATION | TO | STATION | | FEET | FEET | EACH | EACH | TON | EACH | EACH | EACH |
| US 51 | | | | | | | | | | | |
| 232+44.57 | TO | 232+57.07 | SB MED | | | | | 1.0 | | 1.0 | |
| 232+57.07 | TO | 233+03.31 | SB MED | | | | | 1.0 | | 1.0 | |
| 235+93.10 | TO | 236+39.32 | NB O/S | | | | | 1.0 | | | |
| 236+39.45 | TO | 247+64.40 | NB O/S | | 1,125.0 | | | | | 15.0 | |
| 247+64.38 | TO | 247+76.88 | NB O/S | | | | | 1.0 | | | |
| 235+99.43 | TO | 236+45.68 | SB MED | | | | | 1.0 | | 1.0 | |
| 236+45.57 | TO | 237+83.07 | *SB MED | 137.5 | | | | | | 2.0 | |
| 237+83.07 | TO | 239+20.57 | SB MED | 137.5 | | | | | | 2.0 | |
| 239+20.57 | TO | 239+70.57 | SB MED | | | | | 1.0 | | | 1.0 |
| 236+01.43 | TO | 236+47.67 | SB O/S | | | | | 1.0 | | | |
| 236+47.67 | TO | 247+60.17 | SB O/S | | 1,112.5 | | | | | 15.0 | |
| 247+60.07 | TO | 248+10.07 | SB O/S | | | | | 1.0 | | | 1.0 |
| 311+02.11 | TO | 311+14.48 | SB O/S | | | | | 1.0 | 1.0 | | |
| 311+14.48 | TO | 313+12.59 | SB O/S | 200.0 | | | | | | 3.0 | |
| 313+12.47 | TO | 313+62.33 | SB O/S | | | | | 1.0 | | | 1.0 |
| 311+35.16 | TO | 311+85.68 | NB O/S | | | | | 1.0 | | | 1.0 |
| 311+85.16 | TO | 313+86.98 | NB O/S | 200.0 | | | | | | 3.0 | |
| 313+87.12 | TO | 313+99.62 | NB O/S | | | | | 1.0 | 1.0 | | |
| 341+66.00 | TO | 342+16.00 | NB O/S | | | | | 1.0 | | | 1.0 |
| 350+72.00 | TO | 351+22.00 | SB O/S | | | | | 1.0 | | | 1.0 |
| US 51/BUS 51 RAMP B | | | | | | | | | | | |
| 301+67.24 | TO | 302+13.48 | RT | | | | | 1.0 | | | |
| 302+13.48 | TO | 308+27.86 | RT | | 612.5 | | | | | 7.0 | |
| 308+27.86 | TO | 308+40.45 | RT | | | | | 1.0 | 1.0 | | |
| US 51/BUS 51 RAMP C | | | | | | | | | | | |
| 713+51.30 | TO | 714+02.30 | RT | | | | | 1.0 | | | 1.0 |
| 714+02.30 | TO | 718+04.66 | RT | | 400.0 | | | | | 5.0 | |
| 718+04.66 | TO | 718+50.95 | RT | | | | | 1.0 | 1.0 | | |
| US 51/IL 48 RAMP C | | | | | | | | | | | |
| 514+46.81 | TO | 514+96.81 | RT | | | | | 1.0 | | | 1.0 |
| 514+96.81 | TO | 515+96.74 | RT | | 100.0 | | | | | 2.0 | |
| 515+96.74 | TO | 516+43.05 | RT | | | | | 1.0 | | 2.0 | |
| SHEET TOTALS | | | | 675.0 | 3,350.0 | 8.0 | 7.0 | 5.0 | 56.0 | 7.0 | 8.0 |

* DENOTES 1:30 FLARE RATE TO 4 FOOT OFF OF PAVED SHOULDER

FILE NAME =
c:\pwork\PWIDDT\SWARTZRW\d0138944.dwg

USER NAME = swartzrw
4150-sht-sch.dgn
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 3/17/2010

DESIGNED -
DRAWN -
CHECKED -
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: NA SHEET NO. 5 OF 11 SHEETS STA. TO STA.

| | | | | |
|---------------------------|-----------|--------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 322 | (58,20)RS | MACON | 151 | 15 |
| CONTRACT NO. 74150 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| GUARDRAIL SCHEDULE (Cont'd) | | LOCATION | STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS | STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS | TRAFFIC BARRIER TERMINAL, TYPE 1 SPECIAL (TANGENT) | TRAFFIC BARRIER TERMINAL, TYPE 6 | TRAFFIC BARRIER TERMINAL, TYPE 2 | GUARDRAIL MARKERS, TYPE A (CRYSTAL) | GUARDRAIL MARKERS, TYPE A (AMBER) | TERMINAL MARKERS-DIRECT APPLIED |
|---------------------------------|--------------|----------|--|--|--|----------------------------------|----------------------------------|-------------------------------------|-----------------------------------|---------------------------------|
| STATION TO | STATION | | FEET | FEET | EACH | EACH | TON | EACH | EACH | EACH |
| US 51/IL 48 RAMP B | | | | | | | | | | |
| 401+50.03 | TO 401+96.27 | RT | | | | 1.0 | | 1.0 | | |
| 401+96.27 | TO 402+08.77 | RT | | | | | 1.0 | 1.0 | | |
| CH 41 (WYCKLES RD.) SN 058-0103 | | | | | | | | | | |
| 435+30.11 | TO 435+80.11 | RT | | | 1.0 | | | 2.0 | | 1.0 |
| 436+14.29 | TO 436+64.29 | LT | | | 1.0 | | | 2.0 | | 1.0 |
| 441+68.27 | TO 442+18.27 | RT | | | 1.0 | | | 2.0 | | 1.0 |
| 442+27.45 | TO 442+77.45 | LT | | | 1.0 | | | 2.0 | | 1.0 |
| SHEET TOTALS | | | 0.0 | 0.0 | 4.0 | 1.0 | 1.0 | 10.0 | 0.0 | 4.0 |
| PROJECT TOTALS | | | 2,062.5 | 4,640 | 18.0 | 19.0 | 7.0 | 86.0 | 33.0 | 18.0 |

HOT MIX ASPHALT SHOULDER CURB SCHEDULE

| LOCATION | | LENGTH (FEET) |
|----------------------|--------------|--------------------|
| US 51 | | |
| 135+58.65 | TO 141+38.03 | LT O/S 579.4 |
| 135+66.97 | TO 135+87.63 | LT MEDIAN 20.7 |
| 135+83.71 | TO 136+14.25 | RT MEDIAN 30.5 |
| 135+93.57 | TO 142+11.49 | RT O/S 617.9 |
| 232+51.04 | TO 232+74.24 | RT O/S & MED. 46.4 |
| 232+57.58 | TO 232+78.12 | LT MEDIAN 20.5 |
| 236+12.67 | TO 244+22.50 | RT O/S 1,619.7 |
| 236+26.37 | TO 244+22.55 | LT O/S 1,592.4 |
| US 51/BUS. 51 RAMP B | | |
| 301+92.80 | TO 303+29.16 | RT 136.4 |
| 303+39.16 | TO 307+94.64 | RT. 455.5 |
| US 51/BUS. 51 RAMP C | | |
| 713+98.75 | TO 714+36.16 | RT 37.4 |
| 714+49.95 | TO 717+83.57 | RT 333.6 |
| 717+93.57 | TO 718+25.56 | RT 32.0 |
| US 51/IL 48 RAMP B | | |
| 401+75.18 | TO 401+97.73 | RT 22.6 |
| PROJECT TOTAL | | 5,545 |

GUARDRAIL REMOVAL SCHEDULE

| LOCATION | | | | LENGTH (FEET) |
|-------------------|----|-----------|-----------------|------------------|
| US 51/BUS. 51 | | | | |
| 301+69.75 | TO | 308+26.43 | Ramp B | 657 |
| 713+98.76 | TO | 718+48.41 | Ramp C | 450 |
| 130+78.97 | TO | 132+44.01 | Median | 165 |
| 135+35.99 | TO | 141+42.73 | SB O/S Shoulder | 607 |
| 135+44.63 | TO | 137+16.02 | Median | 171 |
| 135+71.60 | TO | 142+16.18 | NB O/S Shoulder | 645 |
| 140+27.39 | TO | 141+97.49 | Median | 170 |
| 142+88.37 | TO | 144+01.36 | SB O/S Shoulder | 113 |
| 143+07.32 | TO | 144+76.02 | Median | 169 |
| US 51/IL 48 | | | | |
| 231+24.30 | TO | 232+97.01 | Median | 173 |
| 515+36.87 | TO | 516+40.55 | Ramp C | 104 |
| 235+95.60 | TO | 246+86.00 | NB O/S Shoulder | 1,090 |
| 236+01.87 | TO | 237+70.00 | Median | 168 |
| 236+03.93 | TO | 246+71.47 | SB O/S Shoulder | 1,068 |
| US 51/SN 058-0103 | | | | |
| 309+80.21 | TO | 312+54.42 | SB O/S Shoulder | 274 |
| 312+13.14 | TO | 314+89.00 | NB O/S Shoulder | 276 |
| SN 058-0103 CH 41 | | | | |
| 435+55.11 | TO | 435+80.11 | SE QUAD | 25 |
| 436+14.29 | TO | 436+64.29 | SW QUAD | 50 |
| 441+68.27 | TO | 441+93.27 | NE QUAD | 25 |
| 442+27.45 | TO | 442+52.45 | NW QUAD | 25 |
| SN 058-0098/0099 | | | | |
| 341+66.00 | TO | 342+16.00 | RT O/S | 50 |
| 350+72.00 | TO | 351+22.00 | LT O/S | 50 |
| PROJECT TOTAL | | | | 6,523 |

CURB REMOVAL AND REPLACEMENT SCHEDULE

| LOCATION | | | | LENGTH (FOOT) |
|------------------------|----|-----------|----------------|------------------|
| US 51-SN 058-0105/0104 | | | | |
| 141+38.03 | TO | 141+43.28 | LT. O/S | 5.3 |
| 141+56.79 | TO | 141+62.05 | LT. MED. SHLDR | 5.3 |
| 141+92.78 | TO | 141+98.04 | RT. MED. SHLDR | 5.3 |
| 142+11.48 | TO | 142+16.34 | LT. O/S | 4.9 |
| 142+87.81 | TO | 142+93.06 | LT. O/S | 5.3 |
| 143+06.76 | TO | 143+12.01 | LT. MED. SHLDR | 5.3 |
| 143+42.43 | TO | 143+47.68 | RT. MED. SHLDR | 5.3 |
| 143+61.43 | TO | 143+66.68 | RT. O/S | 5.3 |
| TOTAL= | | | | 41.6 |

| DELINEATOR SCHEDULE | | | | | |
|---------------------|-------|------|--------------|-------|------|
| LOCATION | | FACH | LOCATION | | FACH |
| 115+39.98 | LT | 1.0 | 227+40.00 | RT | 1.0 |
| 117+39.99 | RT/LT | 2.0 | 251+40.00 | RT/LT | 2.0 |
| 118+89.99 | RT/LT | 2.0 | 255+40.00 | RT/LT | 2.0 |
| 120+39.99 | RT/LT | 2.0 | 259+40.00 | RT/LT | 2.0 |
| 121+89.99 | RT/LT | 2.0 | 263+40.00 | RT/LT | 2.0 |
| 123+39.99 | RT/LT | 2.0 | 267+40.00 | RT/LT | 2.0 |
| 124+89.99 | RT/LT | 2.0 | 271+40.00 | RT/LT | 2.0 |
| 126+39.99 | RT/LT | 2.0 | 275+40.00 | RT/LT | 2.0 |
| 127+89.99 | RT | 1.0 | 279+40.00 | RT/LT | 2.0 |
| 129+39.99 | RT | 1.0 | 283+40.00 | RT/LT | 2.0 |
| 147+40.00 | RT/LT | 2.0 | 287+40.00 | RT/LT | 2.0 |
| 151+40.00 | RT/LT | 2.0 | 291+40.00 | RT/LT | 2.0 |
| 155+40.00 | RT/LT | 2.0 | 295+40.00 | RT/LT | 2.0 |
| 159+40.00 | RT/LT | 2.0 | 299+40.00 | RT/LT | 2.0 |
| 163+40.00 | RT/LT | 2.0 | 303+40.00 | RT/LT | 2.0 |
| 167+40.00 | RT/LT | 2.0 | 307+40.00 | RT/LT | 2.0 |
| 171+40.00 | RT/LT | 2.0 | 311+40.00 | RT | 1.0 |
| 175+40.00 | RT/LT | 2.0 | 315+40.00 | RT/LT | 2.0 |
| 179+40.00 | RT/LT | 2.0 | 319+40.00 | RT/LT | 2.0 |
| 183+40.00 | RT/LT | 2.0 | 323+40.00 | RT/LT | 2.0 |
| 187+40.00 | RT/LT | 2.0 | 327+40.00 | RT/LT | 2.0 |
| 191+40.00 | RT/LT | 2.0 | 331+41.00 | RT/LT | 2.0 |
| 195+40.00 | RT/LT | 2.0 | 335+40.00 | RT/LT | 2.0 |
| 199+40.00 | RT/LT | 2.0 | 339+40.00 | RT/LT | 2.0 |
| 203+40.00 | RT/LT | 2.0 | 351+40.00 | RT/LT | 2.0 |
| 207+40.00 | RT | 1.0 | 355+40.00 | RT/LT | 2.0 |
| 211+40.00 | RT | 1.0 | 359+40.00 | RT/LT | 2.0 |
| 219+40.00 | LT | 1.0 | 363+40.00 | RT/LT | 2.0 |
| 223+40.00 | RT/LT | 2.0 | 367+40.00 | RT/LT | 2.0 |
| COLUMN TOTAL | | 52.0 | COLUMN TOTAL | | 56.0 |

| DELINEATOR SCHEDULE | | | | | |
|----------------------|----|-----------|----------|--|-------|
| LOCATION | | FACH | LOCATION | | FACH |
| US 51/BUS. 51 RAMP A | | | | | |
| 205+40.56 | TO | 209+40.56 | RT | | 5 |
| 210+40.56 | TO | 216+40.56 | RT/LT | | 14 |
| 217+40.56 | TO | 218+40.56 | RT | | 2 |
| US 51/BUS. 51 RAMP B | | | | | |
| 307+00.00 | TO | 308+00.00 | LT | | 2 |
| 309+00.00 | TO | 310+00.00 | RT/LT | | 4 |
| 311+00.00 | TO | 317+00.00 | LT | | 7 |
| US 51/BUS. 51 RAMP C | | | | | |
| 706+00.00 | TO | 713+00.00 | RT/LT | | 16 |
| 714+00.00 | TO | 715+00.00 | LT | | 2 |
| US 51/BUS. 51 RAMP D | | | | | |
| 895+05.88 | TO | 901+05.88 | RT | | 6 |
| 902+05.88 | TO | 911+05.88 | RT/LT | | 18 |
| 912+05.88 | TO | 919+05.88 | RT | | 8 |
| US 51/IL 48 RAMP A | | | | | |
| 307+73.82 | TO | 313+73.82 | RT | | 7 |
| 314+73.82 | TO | 320+73.82 | RT/LT | | 14 |
| 321+73.82 | TO | 332+73.82 | RT | | 12 |
| US 51/IL 48 RAMP B | | | | | |
| 402+00.00 | TO | 406+00.00 | RT | | 5 |
| 407+00.00 | TO | 408+00.00 | RT/LT | | 4 |
| 409+00.00 | TO | 418+00.00 | LT | | 10 |
| US 51/IL 48 RAMP C | | | | | |
| 504+03.75 | TO | 513+03.75 | RT/LT | | 20 |
| 514+03.75 | | | RT | | 1 |
| US 51/IL 48 RAMP D | | | | | |
| 600+00.00 | TO | 606+00.00 | RT | | 7 |
| 607+00.00 | TO | 614+00.00 | RT/LT | | 16 |
| 615+00.00 | TO | 620+00.00 | RT | | 6 |
| COLUMN TOTAL | | | | | 186 |
| PROJECT TOTAL | | | | | 294.0 |

TEMPORARY CONCRETE BARRIER SCHEDULE

| LOCATION | | | LENGTH | |
|------------------------------|----|-----------|--------|-------------|
| | | | FT. | |
| NB PL WORK AREA SN. 058-0102 | | | | |
| 129+81.20 | TO | 131+32.50 | 151 | 12: 1 TAPER |
| 131+32.50 | TO | 136+77.10 | 545 | |
| SB PL WORK AREA SN. 058-0101 | | | | |
| 131+08.30 | TO | 136+56.20 | 548 | |
| 136+56.20 | TO | 138+09.20 | 153 | 12: 1 TAPER |
| NB PL WORK AREA SN. 058-0104 | | | | |
| 139+28.60 | TO | 140+81.60 | 153 | 12: 1 TAPER |
| 140+81.60 | TO | 144+74.20 | 393 | |
| SB PL WORK AREA SN. 058-0105 | | | | |
| 140+28.80 | TO | 144+22.90 | 394 | |
| 144+22.90 | TO | 145+75.90 | 153 | 12: 1 TAPER |
| NB PL WORK AREA SN. 058-0109 | | | | |
| 230+27.70 | TO | 231+80.70 | 153 | 12: 1 TAPER |
| 231+80.70 | TO | 237+12.70 | 532 | |
| SB PL WORK AREA SN. 058-0108 | | | | |
| 231+86.20 | TO | 237+18.30 | 532 | |
| 237+18.30 | TO | 238+71.20 | 153 | 12: 1 TAPER |
| TOTAL = | | | 3,860 | |

RELOCATE TEMPORARY CONCRETE BARRIER SCHEDULE

| LOCATION | | | LENGTH | |
|------------------------------|----|-----------|--------|-------------|
| | | | FT. | |
| NB DL WORK AREA SN. 058-0102 | | | | |
| 130+63.00 | TO | 131+32.40 | 69 | 12: 1 TAPER |
| 131+32.40 | TO | 136+77.10 | 545 | |
| SB DL WORK AREA SN. 058-0101 | | | | |
| 131+08.30 | TO | 136+56.20 | 548 | |
| 136+56.20 | TO | 138+09.20 | 153 | 12: 1 TAPER |
| NB DL WORK AREA SN. 058-0104 | | | | |
| 139+28.60 | TO | 140+81.50 | 153 | 12: 1 TAPER |
| 140+81.50 | TO | 144+75.70 | 394 | |
| SB DL WORK AREA SN. 058-0105 | | | | |
| 140+28.80 | TO | 144+22.80 | 394 | |
| 144+22.80 | TO | 145+75.90 | 153 | 12: 1 TAPER |
| NB DL WORK AREA SN. 058-0109 | | | | |
| 231+13.00 | TO | 231+80.60 | 68 | 12: 1 TAPER |
| 231+80.60 | TO | 237+12.70 | 532 | |
| SB DL WORK AREA SN. 058-0108 | | | | |
| 231+86.30 | TO | 237+18.30 | 532 | |
| 237+18.30 | TO | 238+71.30 | 153 | 12: 1 TAPER |
| TOTAL = | | | 3,694 | |

EARTHWORK SCHEDULE

| Location | Earth Excavation | | Earthwork Balance | |
|--|------------------|--------------------|-------------------|------------------------|
| | Earth Excavation | Adj. for Shrinkage | Embankment | Waste (+)/Shortage (-) |
| | CU. YD. | CU. YD. | CU. YD. | CU. YD. |
| 128+31.85 TO 132+24.13 (NB MEDIAN SN 058-0101) | | | 75.4 | -75.4 |
| 135+91.66 TO 139+59.27 (SB MEDIAN SN 058-0102) | | | 70.7 | -70.7 |
| 137+83.34 TO 141+67.71 (NB MEDIAN SN 058-0104) | | | 73.9 | -73.9 |
| 143+37.08 TO 147+21.46 (SB MEDIAN SN 058-0105) | | | 73.9 | -73.9 |
| 228+65.62 TO 232+45.57 (NB MEDIAN SN 058-0109) | | | 73.0 | -73.0 |
| 236+30.87 TO 240+16.57 (SB MEDIAN SN 058-0108) | | | 74.1 | -74.1 |
| 309+75.00 TO 315+25.00 (MEDIAN IMPACT ATTEN.) | 9.0 | 6.8 | 969.0 | -962.3 |
| PROJECT TOTAL | 9.0 | 6.8 | 1410.0 | -1,403 |

DRAINAGE SCHEDULE

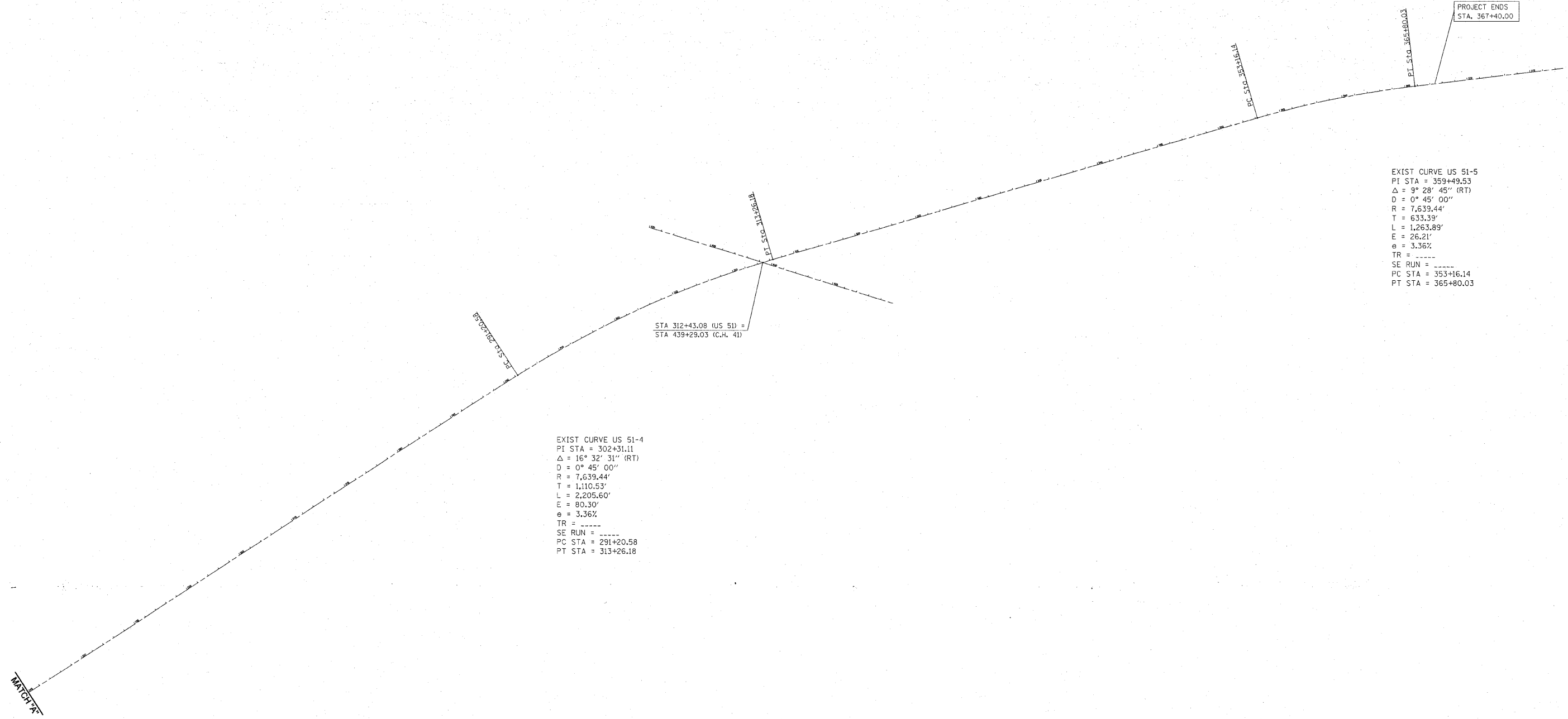
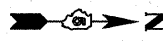
| LOCATION | | METAL END SECTIONS, 12" | PIPE DRAINS, 12" | INLETS, SPECIAL, TYPE C | INLETS TO BE ADJUSTED | CONCRETE THRUST BLOCKS | PIPE CULVERTS, TYPE 1, RCCP, 24" | PIPE CULVERTS, SPECIAL, 24" | INLET BOX, STANDARD 542531 | REMOVE INLET BOXES |
|---------------------------------------|-------------------------------|-------------------------|------------------|-------------------------|-----------------------|------------------------|----------------------------------|-----------------------------|----------------------------|--------------------|
| LOCATION | | EACH | FOOT | EACH | EACH | EACH | FOOT | FOOT | EACH | EACH |
| US 51 | | | | | | | | | | |
| 131+95.75 | RT. 92.2 FT. | 1.0 | 90.5 | 1.0 | | 1.0 | | | | |
| 135+46.50 | SB MED. 39.8' LT | | | | | | | | | 1.0 |
| 135+63.02 | NB MED. 39.7' RT | | | | | | | | | 1.0 |
| 135+92.63 | LT. 38.9 FT. | 1.0 | 33.9 | 1.0 | | 1.0 | | | | |
| 136+19.25 | RT. 38.9 FT. | 1.0 | 33.9 | 1.0 | | 1.0 | | | | |
| 141+41.82 | SB O/S SN 058-0105, 76.4' LT | | | | 1.0 | | | | | |
| 142+13.26 | NB O/S SN 058-0104, 76' RT | | | | 1.0 | | | | | |
| 142+96.41 | SB O/S SN 058-0105, 76.4' LT | | | | 1.0 | | | | | |
| 143+09.44 | SB MED. SN 058-0105, 39.6' LT | | | | 1.0 | | | | | |
| 143+46.32 | NB MED. SN 058-0104, 39.6' RT | | | | 1.0 | | | | | |
| 143+69.88 | NB O/S SN 058-0104, 76.2' RT | | | | 1.0 | | | | | |
| 232+46.04 | RT. 92.2 FT. | 1.0 | 122 | 1.0 | | 1.0 | | | | |
| 232+46.04 | RT. 38.9 FT. | 1.0 | 32 | 1.0 | | 1.0 | | | | |
| 232+52.58 | LT. 38.9 FT. | 1.0 | 32 | 1.0 | | 1.0 | | | | |
| 232+52.85 | LT. 84.8 FT. | 1.0 | 124 | 1.0 | | 1.0 | | | | |
| 232+93.06 | NB MED. 40.7' RT | | | | | | | | | 1.0 |
| 232+98.47 | SB MED. 41.5' LT | | | | | | | | | 1.0 |
| 310+11.00 TO 311+46.00 MEDIAN | | | | | | | 135.0 | 8.0 | 1.0 | |
| 311+50.00 4' RT. TO 311+50.00 20' RT. | | | | | | | 20.0 | | | 1.0 |
| US 51/BUS. 51 RAMP B | | | | | | | | | | |
| 305+34.00 | RT. | | | | 1.0 | | | | | |
| 308+00.00 | RT. | | | | 1.0 | | | | | |
| US 51/IL 48 RAMP B | | | | | | | | | | |
| 401+54.58 | 3' RT | | | | | | | | | 1.0 |
| US 51/IL 48 RAMP C | | | | | | | | | | |
| 516+38.24 | 2.7' RT | | | | | | | | | 1.0 |
| PROJECT TOTALS | | 7.0 | 468.3 | 7.0 | 8.0 | 7.0 | 155.0 | 8.0 | 1.0 | 7.0 |

SEEDING SCHEDULE

| LOCATION | | | | SEEDING, CLASS 2 | NITROGEN FERTILIZER NUTRIENT | PHOSPHOROUS FERTILIZER NUTRIENT | POTASSIUM FERTILIZER NUTRIENT | MULCH, METHOD 2 | TEMPORARY EROSION CONTROL SEEDING |
|---|----------|-----------|------------|------------------|------------------------------|---------------------------------|-------------------------------|-----------------|-----------------------------------|
| US 51 | | | | ACRE | POUND | POUND | POUND | ACRE | POUND |
| 128+31.85 | TO | 132+24.13 | RT. MEDIAN | 0.021 | 1.89 | 1.89 | 1.89 | 0.021 | 2.10 |
| 135+91.66 | TO | 139+59.27 | LT. MEDIAN | 0.021 | 1.89 | 1.89 | 1.89 | 0.021 | 2.10 |
| 137+83.34 | TO | 141+67.71 | RT. MEDIAN | 0.021 | 1.89 | 1.89 | 1.89 | 0.021 | 2.10 |
| 143+37.08 | TO | 147+21.46 | LT. MEDIAN | 0.021 | 1.89 | 1.89 | 1.89 | 0.021 | 2.10 |
| 228+65.62 | TO | 232+45.57 | RT. MEDIAN | 0.021 | 1.89 | 1.89 | 1.89 | 0.021 | 2.10 |
| 236+30.87 | TO | 240+16.57 | LT. MEDIAN | 0.021 | 1.89 | 1.89 | 1.89 | 0.021 | 2.10 |
| SLOPE AREAS DISTURBED BY CONSTRUCTION OF INLETS | | | | | | | | | |
| 131+95.75 | RT. O/S | | | 0.027 | 2.45 | 2.45 | 2.45 | 0.027 | 2.72 |
| 135+92.63 | LT. MED. | | | 0.004 | 0.34 | 0.34 | 0.34 | 0.004 | 0.38 |
| 136+19.25 | RT. MED. | | | 0.004 | 0.35 | 0.35 | 0.35 | 0.004 | 0.38 |
| 232+46.04 | RT. O/S | | | 0.036 | 3.21 | 3.21 | 3.21 | 0.036 | 3.57 |
| 232+46.04 | RT. MED. | | | 0.004 | 0.33 | 0.33 | 0.33 | 0.004 | 0.36 |
| 232+52.58 | LT. MED. | | | 0.004 | 0.33 | 0.33 | 0.33 | 0.004 | 0.36 |
| 232+52.85 | LT. O/S | | | 0.036 | 3.28 | 3.28 | 3.28 | 0.036 | 3.64 |
| SN 058-0103 (IMPACT ATTENUATOR) | | | | | | | | | |
| 309+75.00 | TO | 315+25.00 | | 0.909 | 81.82 | 81.82 | 81.82 | 0.909 | 90.91 |
| PROJECT TOTALS | | | | 1.15 | 103 | 103 | 103 | 1.15 | 115 |

| PAVEMENT MARKING SCHEDULE | | LENGTH | SHORT-TERM PAVEMENT MARKING | TEMPORARY PAVEMENT MARKING-LINE 4" (YELLOW) | TEMPORARY PAVEMENT MARKING-LINE 4" (WHITE) | TEMPORARY PAVEMENT MARKING-LINE 6" | TEMPORARY PAVEMENT MARKING-LINE 8" | TEMPORARY PAVEMENT MARKING-LINE 12" | TEMPORARY PAVEMENT MARKING-LINE 24" | WORK ZONE PAVEMENT MARKING REMOVAL | PREFORMED PLASTIC PAVEMENT MARKING, TYPEB-INLAID-6' | URETHANE PAVEMENT MARKING-LINE 4" (YELLOW) | URETHANE PAVEMENT MARKING-LINE 4" (WHITE) | URETHANE PAVEMENT MARKING-LINE 6" | URETHANE PAVEMENT MARKING-LINE 8" | URETHANE PAVEMENT MARKING-LINE 12" | URETHANE PAVEMENT MARKING-LINE 24" | RAISED REFLECTIVE PAVEMENT MARKER (CRYSTAL) | RAISED REFLECTIVE PAVEMENT MARKER (AMBER) |
|---------------------------|--------------|----------|-----------------------------|---|--|------------------------------------|------------------------------------|-------------------------------------|-------------------------------------|------------------------------------|---|--|---|-----------------------------------|-----------------------------------|------------------------------------|------------------------------------|---|---|
| STATION | TO STATION | FOOT | FOOT | FOOT | | | FOOT | FOOT | FOOT | SQ FT | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | EACH | FOOT |
| US 51 | | | | | | | | | | | | | | | | | | | |
| 118+87.49 | TO 126+60.30 | 772.8 | 340.0 | 1,545.6 | 1,545.6 | | | | | 1,066.5 | 386.4 | 1,545.6 | 1,545.6 | | | | | 38.6 | |
| 126+60.30 | TO 129+45.54 | 285.2 | 125.5 | 570.5 | 285.2 | | 285.2 | | | 488.7 | 142.6 | 570.5 | 285.2 | | 285.2 | | | 14.3 | |
| 129+45.54 | TO 130+14.65 | 69.1 | 30.4 | 138.2 | 69.1 | | 69.1 | | | 118.4 | 81.9 | 138.2 | 69.1 | | 69.1 | | | 3.5 | |
| 130+14.65 | TO 131+09.41 | 94.8 | 41.7 | 189.5 | | | 84.5 | | | 123.9 | 47.4 | 189.5 | | | 84.5 | | | 4.7 | |
| 131+09.41 | TO 132+18.54 | 109.1 | 48.0 | 218.3 | | | | | | 77.8 | 54.6 | 218.3 | | | | | | 5.5 | |
| 132+18.54 | TO 133+87.06 | 168.5 | 74.1 | 337.0 | | | | | | 120.2 | | 337.0 | | | | | | | |
| 133+87.06 | TO 134+49.72 | 62.7 | 27.6 | 125.3 | 62.7 | | | | | 65.6 | | 125.3 | 62.7 | | | | | | |
| 134+49.72 | TO 135+69.84 | 120.1 | 52.9 | 240.2 | 120.1 | | | | | 125.7 | | 240.2 | 120.1 | | | | | | |
| 135+69.84 | TO 140+99.81 | 530.0 | 233.2 | 1,059.9 | 530.0 | | | | | 554.7 | 265.0 | 1,059.9 | 530.0 | | | | | 26.5 | |
| 140+99.81 | TO 141+83.57 | 83.8 | 36.9 | 167.5 | 167.5 | | | | | 115.6 | 41.9 | 167.5 | 167.5 | | | | | 4.2 | |
| 141+83.57 | TO 143+20.82 | 137.3 | 60.4 | 274.5 | 274.5 | | | | | 189.4 | | 274.5 | 274.5 | | | | | 0.0 | |
| 143+20.82 | TO 204+67.14 | 6,146.3 | 2,704.4 | 12,292.6 | 12,292.6 | | | | | 8,481.9 | 3,073.2 | 12,292.6 | 12,292.6 | | | | | 307.3 | |
| 204+67.14 | TO 211+17.15 | 650.0 | 286.0 | 1,300.0 | 650.0 | | | | | 680.3 | 325.0 | 1,300.0 | 650.0 | | | | | 32.5 | |
| 211+17.15 | TO 212+38.70 | 121.5 | 53.5 | 243.1 | 121.5 | | | | | 127.2 | 60.8 | 243.1 | 121.5 | | | | | 6.1 | |
| 212+38.70 | TO 215+15.24 | 276.5 | 121.7 | 553.1 | | | | | | 197.3 | 138.3 | 553.1 | | | | | | 13.8 | |
| 215+15.24 | TO 215+78.41 | 63.2 | 27.8 | 126.3 | | | 63.2 | | | 87.2 | 31.6 | 126.3 | | | 63.2 | | | 3.2 | |
| 215+78.41 | TO 219+50.02 | 371.6 | 163.5 | 743.2 | 371.6 | | 371.6 | | | 636.7 | 185.8 | 743.2 | 371.6 | | 371.6 | | | 18.6 | |
| 219+50.02 | TO 227+42.37 | 792.3 | 348.6 | 1,584.7 | 1,584.7 | | | | | 1,093.4 | 396.2 | 1,584.7 | 1,584.7 | | | | | 39.6 | |
| 227+42.37 | TO 230+36.57 | 294.2 | 129.4 | 588.4 | 294.2 | | 294.2 | | | 504.1 | 147.1 | 588.4 | 294.2 | | 294.2 | | | 14.7 | |
| 230+36.57 | TO 231+77.48 | 140.9 | 62.0 | 281.8 | | | 140.9 | | | 194.5 | 70.5 | 281.8 | | | 140.9 | | | 7.0 | |
| 231+77.48 | TO 232+81.68 | 104.2 | 45.8 | 208.4 | | | | | | 74.3 | 52.1 | 208.4 | | | | | | 5.2 | |
| 232+81.68 | TO 234+51.03 | 169.3 | 74.5 | 338.7 | | | | | | 120.8 | | 338.7 | | | | | | | |
| 234+51.03 | TO 235+00.71 | 49.7 | 21.9 | 99.4 | | | | | | 35.4 | | 99.4 | | | | | | | |
| 235+00.71 | TO 236+17.01 | 116.3 | 51.2 | 232.6 | | | | | | 83.0 | | 232.6 | | | | | | | |
| 236+17.01 | TO 241+49.81 | 532.8 | 234.4 | 1,065.6 | | | | | | 380.1 | 266.4 | 1,065.6 | | | | | | 26.6 | |
| 241+49.81 | TO 342+67.00 | 10,117.2 | 4,451.6 | 19,783.8 | 20,234.4 | | | | | 13,811.5 | 5,058.6 | 19,783.8 | 20,234.4 | | | | | 505.9 | |
| 342+67.00 | TO 350+22.00 | 755.0 | | | | | | | | | | | | | | | | | |
| 350+22.00 | TO 367+40.00 | 1,718.0 | 755.9 | 3,436.0 | 3,436.0 | | | | | 2,370.8 | 859.0 | 3,436.0 | 3,436.0 | | | | | 85.9 | |
| RAMP A BUS, 51 | | | | | | | | | | | | | | | | | | | |
| 200+38.53 | TO 201+13.53 | 75.0 | | 81.3 | 135.1 | | | | | 72.1 | | 81.3 | 135.1 | | | | | | |
| 201+13.53 | TO 201+57.02 | 43.5 | | 43.5 | 43.9 | | | | | 29.1 | | 43.5 | 43.9 | | | | | | |
| 201+57.02 | TO 216+43.29 | 1,486.3 | | 1,489.9 | 1486.3 | | | | | 992.0 | | 1489.9 | 1486.3 | | | | | | |
| 216+43.29 | TO 216+92.26 | 49.0 | | 50.0 | 49.0 | | 50.0 | | | 66.3 | | 50.0 | 49.0 | | 50.0 | | | | |
| SHEET TOTALS | | 26,506 | 10,603 | 49,409 | 43,754 | 0 | 1,359 | 0 | 0 | 33,085 | 11,684 | 49,409 | 43,754 | 0 | 1,359 | 0 | 0 | 1,164 | 0 |

| PAVEMENT MARKING SCHEDULE (Cont'd) | | LENGTH | SHORT-TERM PAVEMENT MARKING | TEMPORARY PAVEMENT MARKING-LINE 4" (YELLOW) | TEMPORARY PAVEMENT MARKING-LINE 4" (WHITE) | TEMPORARY PAVEMENT MARKING-LINE 6" | TEMPORARY PAVEMENT MARKING-LINE 8" | TEMPORARY PAVEMENT MARKING-LINE 12" | TEMPORARY PAVEMENT MARKING-LINE 24" | WORK ZONE PAVEMENT MARKING REMOVAL | PERFORMED PLASTIC PAVEMENT MARKING, TYPEB-INLAID-6" | URETHANE PAVEMENT MARKING-LINE 4" (YELLOW) | URETHANE PAVEMENT MARKING-LINE 4" (WHITE) | URETHANE PAVEMENT MARKING-LINE 6" | URETHANE PAVEMENT MARKING-LINE 8" | URETHANE PAVEMENT MARKING-LINE 12" | URETHANE PAVEMENT MARKING-LINE 24" | RAISED REFLECTIVE PAVEMENT MARKER (CRYSTAL) | RAISED REFLECTIVE PAVEMENT MARKER (AMBER) |
|------------------------------------|--------------|---------|-----------------------------|---|--|------------------------------------|------------------------------------|-------------------------------------|-------------------------------------|------------------------------------|---|--|---|-----------------------------------|-----------------------------------|------------------------------------|------------------------------------|---|---|
| STATION TO | STATION | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | SQ FT | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | EACH | EACH |
| RAMP B IL 48 | | | | | | | | | | | | | | | | | | | |
| 400+00.00 | TO 401+69.69 | 169.7 | | | 169.7 | | | | | 56.6 | | | 169.7 | | | | | 9.0 | |
| 401+69.69 | TO 402+77.13 | 107.4 | | | 107.4 | | | | | 35.8 | | | 107.4 | | | | | 8.0 | |
| 402+77.13 | TO 407+11.37 | 434.2 | | | 434.2 | | 435.2 | | | 434.9 | | | 434.2 | | 435.2 | | | 45.0 | 1.0 |
| 407+11.37 | TO 417+62.95 | 1,051.6 | | 1100.8 | 1051.6 | | | | | 717.4 | | 1100.8 | 1051.6 | | | | | | 27.0 |
| 417+62.95 | TO 423+94.12 | 631.2 | | 631.2 | 631.2 | | | | | 420.8 | | 631.2 | 631.2 | | | | | | |
| 423+94.12 | TO 424+26.78 | 32.7 | | 32.7 | 32.8 | | | | | 21.8 | | 32.7 | 32.8 | | | | | | |
| 424+26.78 | TO 425+01.78 | 75.0 | | 79.3 | 143.9 | | 163.6 | 56.2 | 36.0 | 311.7 | | 79.3 | 143.9 | | 163.6 | 56.2 | 36.0 | | |
| RAMP C IL 48 | | | | | | | | | | | | | | | | | | | |
| 500+35.00 | TO 501+10.00 | 75.0 | | 75.9 | 141.9 | | | | | 72.6 | | 75.9 | 141.9 | | | | | | |
| 501+10.00 | TO 501+38.47 | 28.5 | | 28.5 | 28.6 | | | | | 19.0 | | 28.5 | 28.6 | | | | | | |
| 501+38.47 | TO 504+03.76 | 265.3 | | 265.3 | 265.3 | | | | | 176.9 | | 265.3 | 265.3 | | | | | | |
| 504+03.76 | TO 512+66.88 | 863.1 | | 908.5 | 863.1 | | | | | 590.5 | | 908.5 | 863.1 | | | | | | |
| 512+66.88 | TO 513+84.34 | 117.5 | | | 117.5 | | 119.8 | | | 119.0 | | | 117.5 | | 119.8 | | | | |
| 513+84.34 | TO 518+47.38 | 463.0 | | | 463.0 | 463.0 | | | | 308.7 | | | 463.0 | 463.0 | | | | | |
| 518+47.38 | TO 524+96.33 | 649.0 | | | 649.0 | | | | | 216.3 | | | 649.0 | | | | | | |
| RAMP D IL 48 | | | | | | | | | | | | | | | | | | | |
| 599+81.00 | TO 602+77.06 | 296.1 | | | 296.1 | | | | | 98.7 | | | 296.1 | | | | | 17.0 | |
| 602+77.06 | TO 607+11.37 | 434.3 | | | 434.3 | | 435.3 | | | 435.0 | | | 434.3 | | 435.3 | | | 45.0 | 1.0 |
| 607+11.37 | TO 622+50.78 | 1,539.4 | | 1538.4 | 1539.4 | | | | | 1025.9 | | 1538.4 | 1539.4 | | | | | | 16.0 |
| 622+50.78 | TO 622+85.52 | 34.7 | | 34.7 | 34.9 | | | | | 23.2 | | 34.7 | 34.9 | | | | | | |
| 622+85.52 | TO 623+60.52 | 75.0 | | 79.3 | 147.7 | | 169.8 | 55.8 | 36.0 | 316.7 | | 79.3 | 147.7 | | 169.8 | 55.8 | 36.0 | | |
| SHEET TOTALS | | 7,343 | 0 | 4,774 | 7,552 | 463 | 1,324 | 112 | 72 | 5,401 | 0 | 4,774 | 7,552 | 463 | 1,324 | 112 | 72 | 124 | 45 |
| PROJECT TOTALS | | 7,343 | 10,603 | 61,512 | 61,951 | 1,032 | 3,743 | 224 | 144 | 45,630 | 11,684 | 61,512 | 61,951 | 1,032 | 3,743 | 224 | 144 | 1,378 | 95 |



EXIST CURVE US 51-4
 PI STA = 302+31.11
 $\Delta = 16^\circ 32' 31''$ (RT)
 D = $0^\circ 45' 00''$
 R = 7,639.44'
 T = 1,110.53'
 L = 2,205.60'
 E = 80.30'
 $e = 3.36\%$
 TR = -----
 SE RUN = -----
 PC STA = 291+20.58
 PT STA = 313+26.18

STA 312+43.08 (US 51) =
 STA 439+29.03 (C.H. 41)

EXIST CURVE US 51-5
 PI STA = 359+49.53
 $\Delta = 9^\circ 28' 45''$ (RT)
 D = $0^\circ 45' 00''$
 R = 7,639.44'
 T = 633.39'
 L = 1,263.89'
 E = 26.21'
 $e = 3.36\%$
 TR = -----
 SE RUN = -----
 PC STA = 353+16.14
 PT STA = 365+80.03

PROJECT ENDS
 STA. 367+40.00

| | |
|--|--|
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| PLOT SCALE = 100.0000' / IN. | PLOT DATE = 3/17/2010 |

| | |
|------------|----------|
| DESIGNED - | REVISD - |
| DRAWN - | REVISD - |
| CHECKED - | REVISD - |
| DATE - | REVISD - |

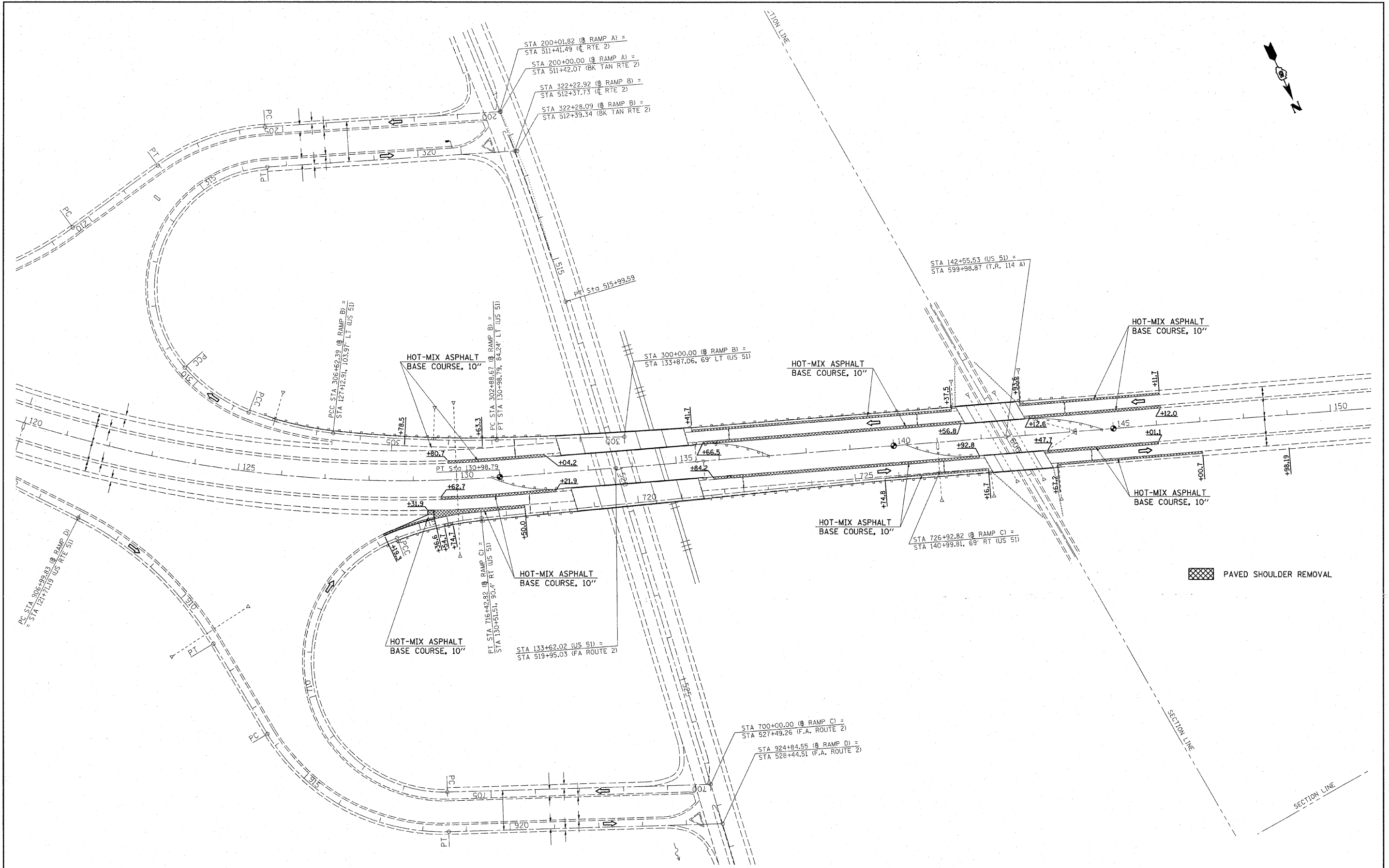
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|------------|----------|
| DESIGNED - | REVISD - |
| DRAWN - | REVISD - |
| CHECKED - | REVISD - |
| DATE - | REVISD - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

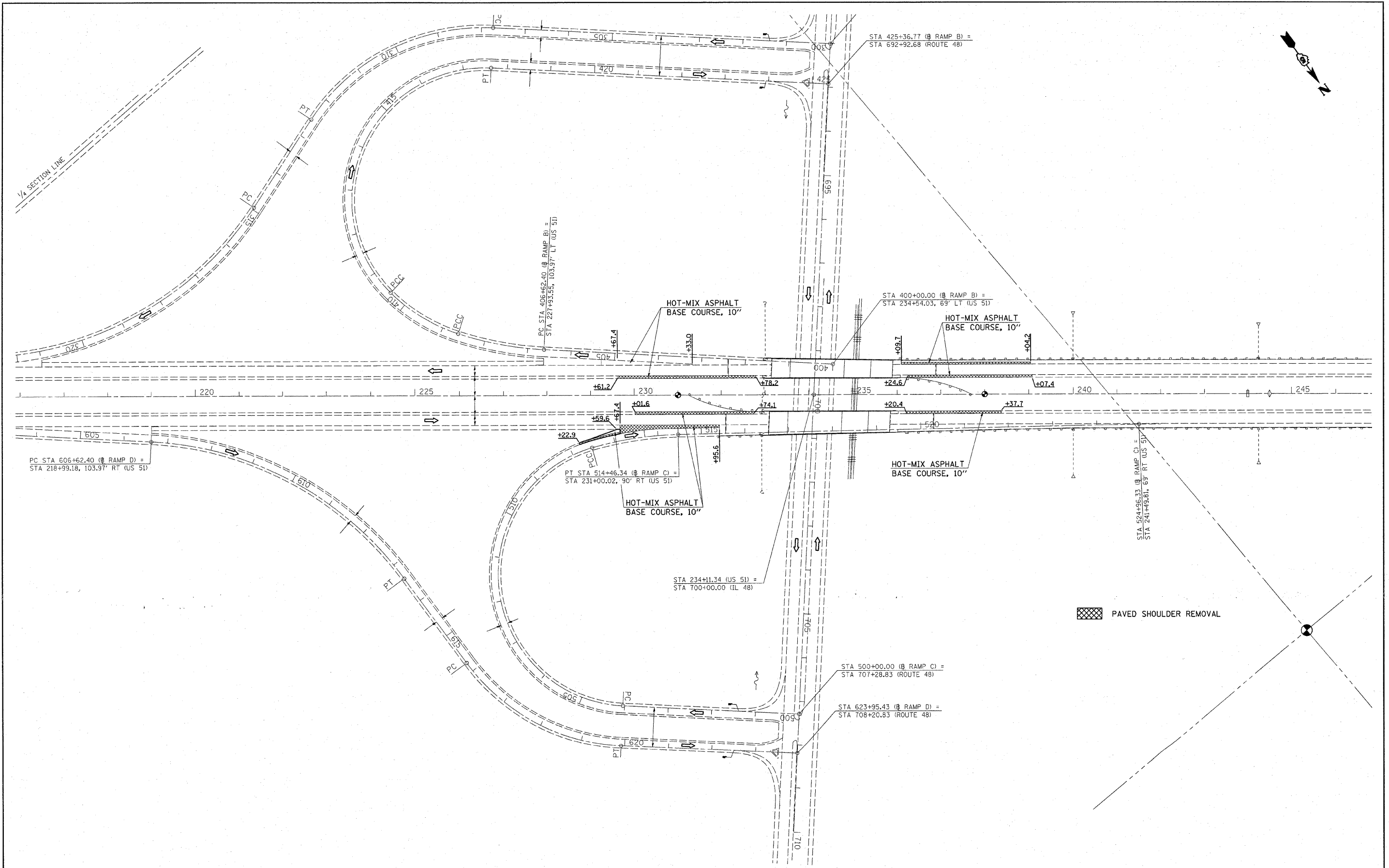
ALIGNMENT DETAIL


SCALE: NA SHEET NO. 2 OF 2 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 322 | (58,20)RS | MACON | 151 | 23 |
| CONTRACT NO. 74150 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

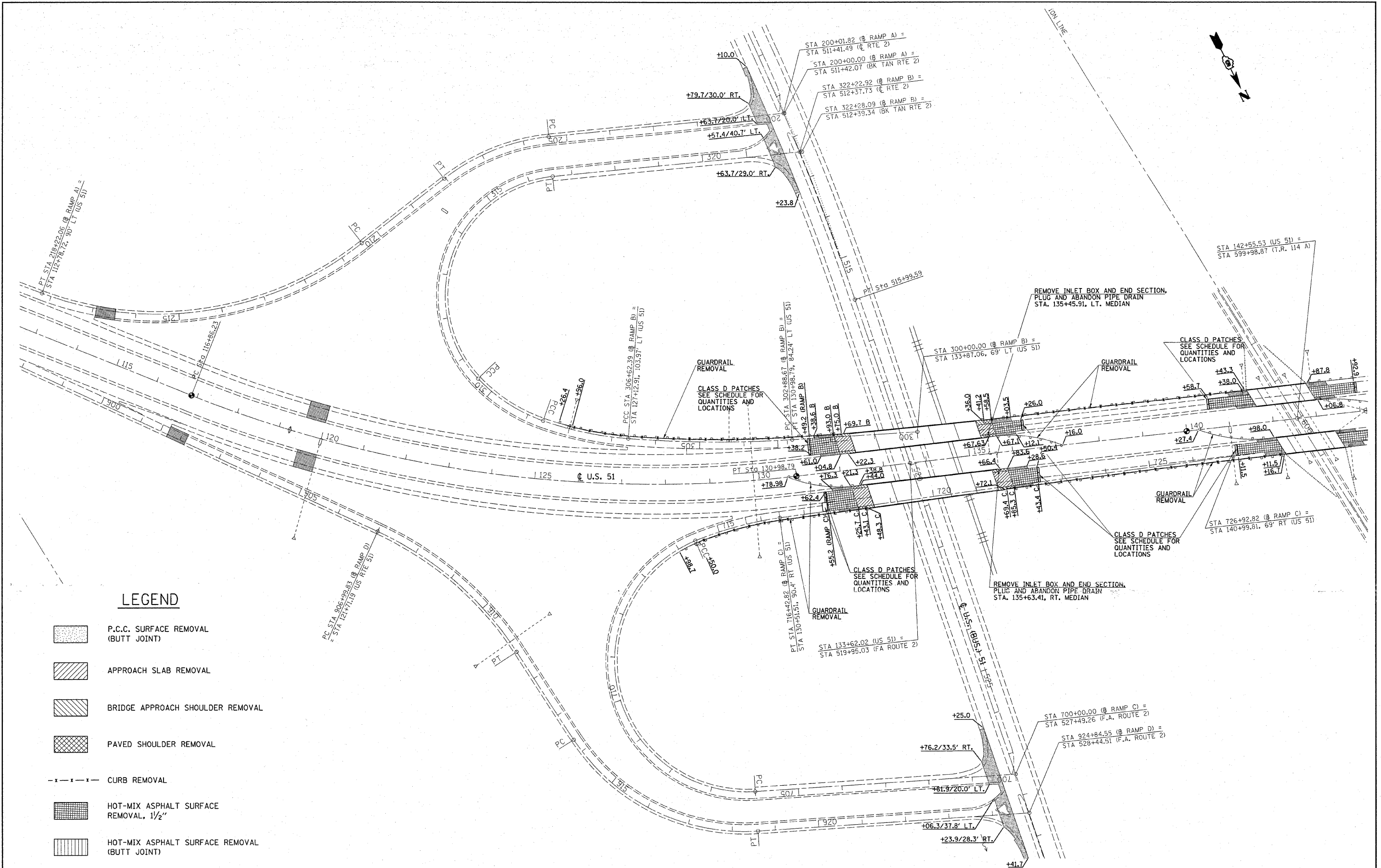


| | | | | | | | | | | |
|---|------------------------------|------------|-----------|---|--|--------------------|-------------------------|----------------------------|---|-----------------|
| FILE NAME = c:\pwork\pwi001\SWARTZR\w\0138944\07 | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PRE-STAGE REMOVAL AND REPLACEMENT | F.A.P. RTE. 322 | SECTION (58,20)RS | COUNTY MACON | TOTAL SHEETS 151 | SHEET NO. 24 |
| | PLOT SCALE = 100.0000' / IN. | CHECKED - | REVISED - | | | SCALE: 100 | SHEET NO. 1 OF 2 SHEETS | STA. 120+00 TO STA. 150+00 | CONTRACT NO. 74150 ILLINOIS FED. AID PROJECT | |
| PLOT DATE = 3/17/2018 | DATE - | REVISED - | REVISED - | | | | | | | |










 PAVED SHOULDER REMOVAL

| | | | | | | | | | | | |
|---|------------------------------|------------|-----------|---|--|-------------|-------------------------|----------------------------|---------------------------|-----------|--|
| FILE NAME = c:\pw_work\FW100T\SWARTZRW\d0138944\07 | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PRE-STAGE REMOVAL AND REPLACEMENT | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| | PLOT SCALE = 100.0000' / IN. | CHECKED - | REVISED - | | | 322 | (58,20)RS | MACON | 151 | 25 | |
| | PLOT DATE = 3/17/2010 | DATE - | REVISED - | | | SCALE: 100 | SHEET NO. 2 OF 2 SHEETS | STA. 216+00 TO STA. 246+00 | ILLINOIS FED. AID PROJECT | | |
| | | | | | | | | | CONTRACT NO. 74150 | | |



LEGEND

-  P.C.C. SURFACE REMOVAL (BUTT JOINT)
-  APPROACH SLAB REMOVAL
-  BRIDGE APPROACH SHOULDER REMOVAL
-  PAVED SHOULDER REMOVAL
-  CURB REMOVAL
-  HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
-  HOT-MIX ASPHALT SURFACE REMOVAL (BUTT JOINT)

FILE NAME =
 c:\pwork\p\1001\SWARTZR\j0138944\d7

USER NAME = swartzw
 4150-ah-removal100.dgn
 PLOT SCALE = 100.0000' / IN.
 PLOT DATE = 3/17/2010

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

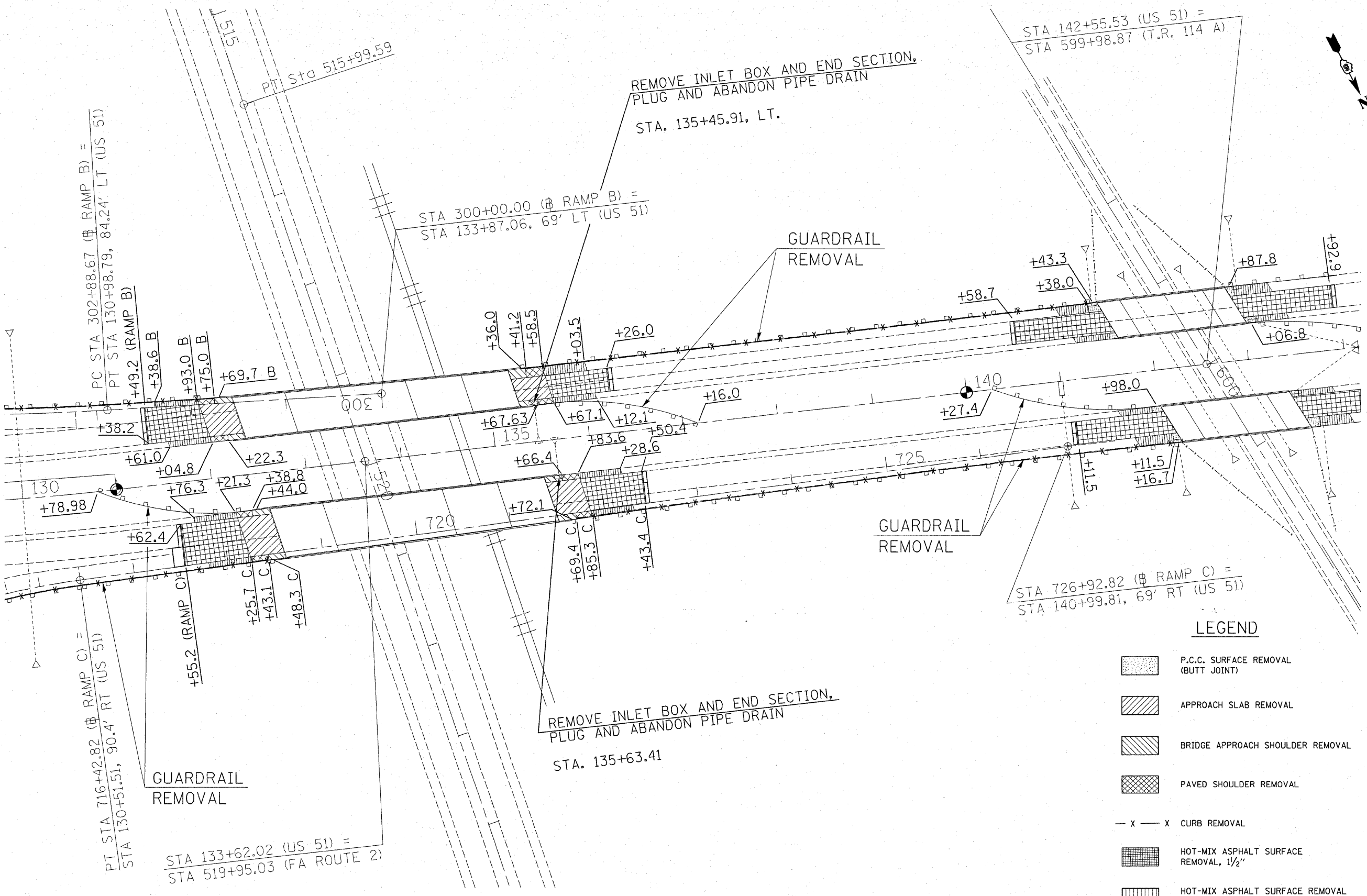
REVISED -
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 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**








REMOVAL SHEETS

SCALE: 100 SHEET NO. 1 OF 8 SHEETS STA. 113+00 TO STA. 144+00

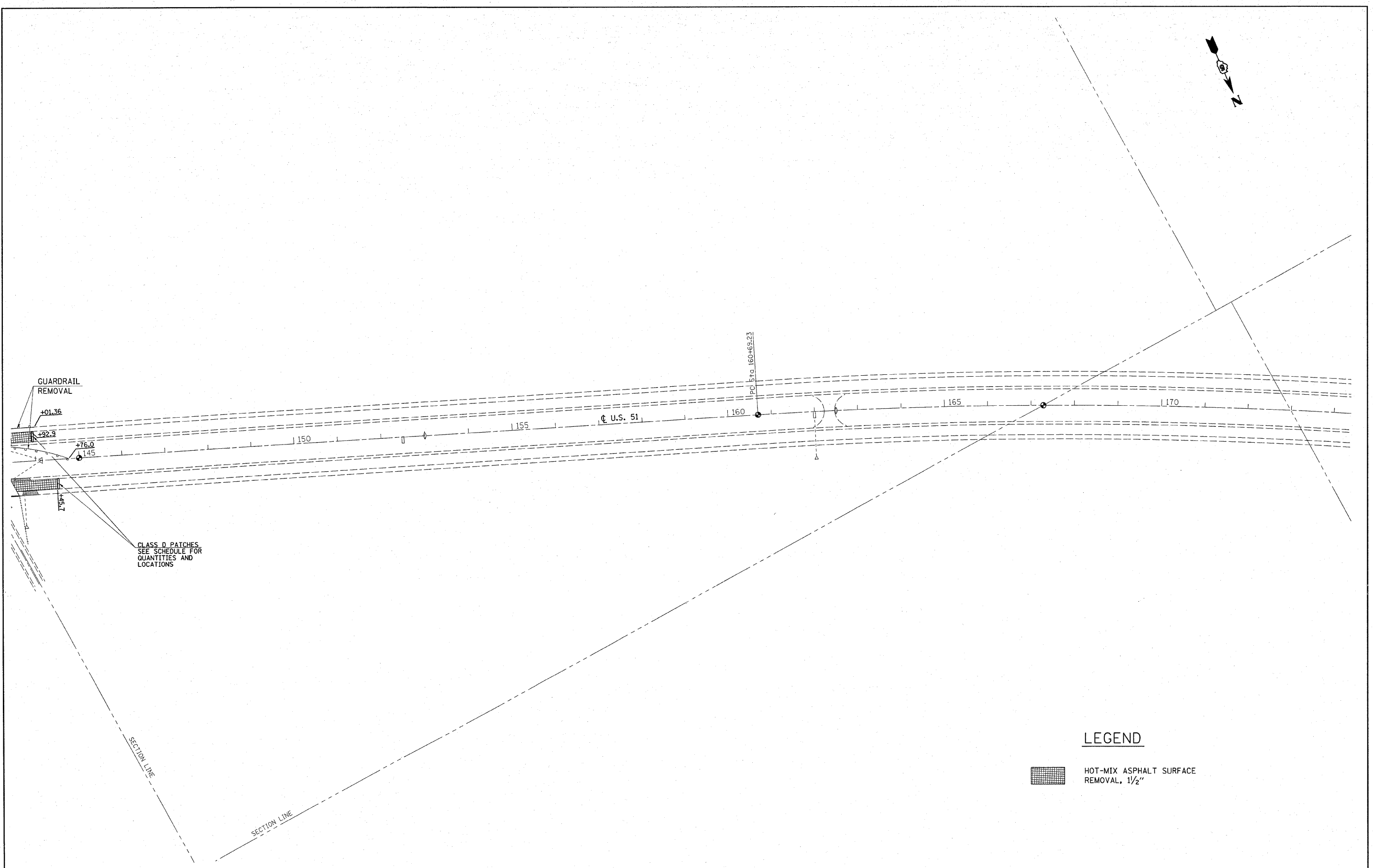
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------|-----------|
| 322 | (58,20)RS | MACON | 151 | 26 |
| CONTRACT NO. 74150 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |




LEGEND

-  P.C.C. SURFACE REMOVAL (BUTT JOINT)
-  APPROACH SLAB REMOVAL
-  BRIDGE APPROACH SHOULDER REMOVAL
-  PAVED SHOULDER REMOVAL
-  - x - x CURB REMOVAL
-  HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
-  HOT-MIX ASPHALT SURFACE REMOVAL (BUTT JOINT)

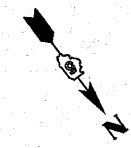
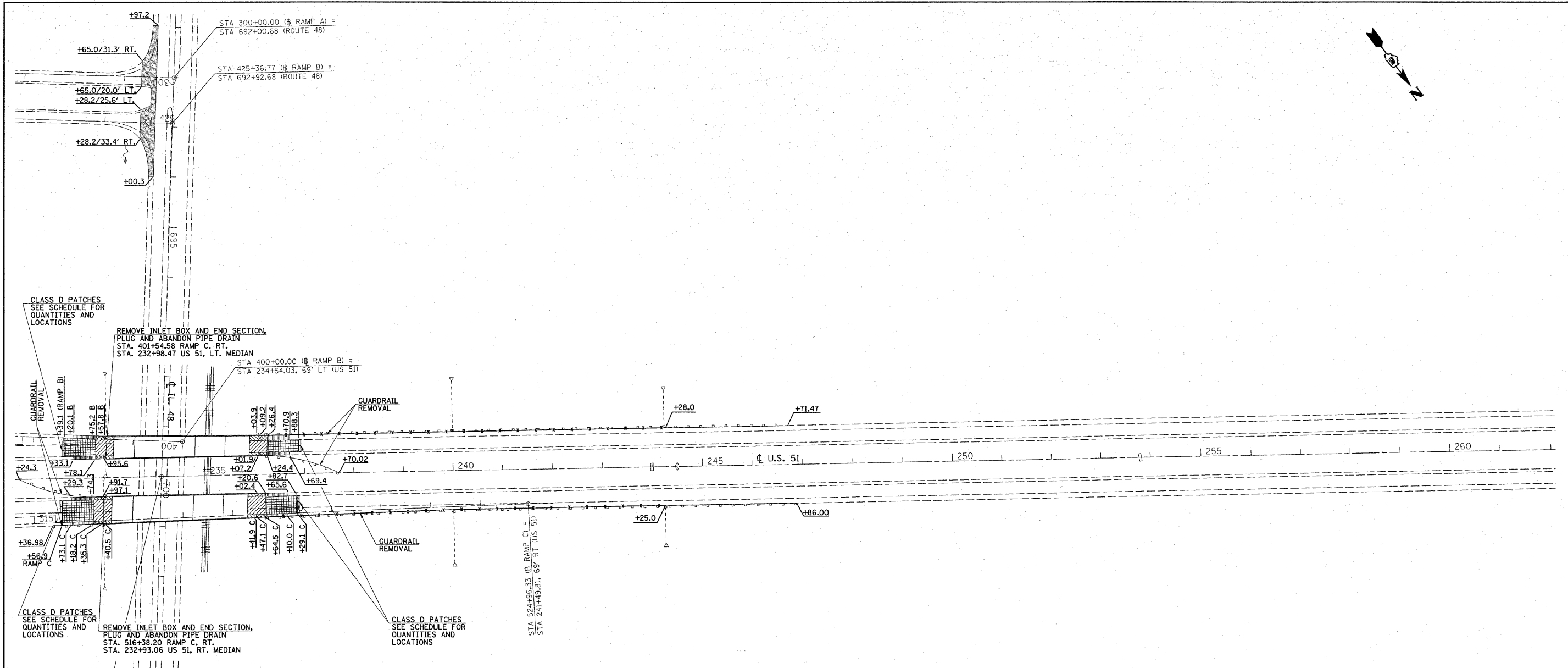
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|---|--------------------------------|------------|-----------|---|--|-------------------------|----------------------------|---------|-----------|--------------|-----------|
| FILE NAME = c:\pwork\pwidot\SWARTZRW\d0138944\d7 | USER NAME = swartzw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REMOVAL SHEETS 50 SCALE DETAIL OF SHEET #26 | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 50.0000' / 1" IN. | CHECKED - | REVISED - | | SCALE: 50 | SHEET NO. 2 OF 8 SHEETS | STA. 130+00 TO STA. 144+00 | 322 | (58,20)RS | MACON | 151 |
| PLOT DATE = 3/17/2018 | DATE - | REVISED - | REVISED - | ILLINOIS FED. AID PROJECT CONTRACT NO. 74150 | | | | | | | |










LEGEND

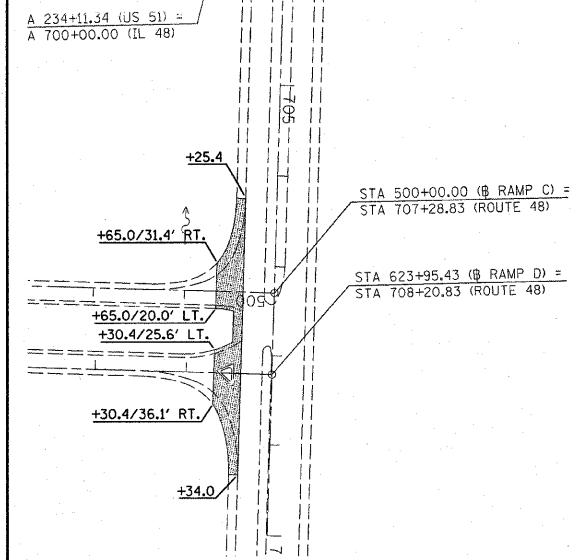
 HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"

| | | | | | | | | | | | | |
|--|---|-----------------------|------------------------|---|---------------------------|-------------|-------------|-----------------------|----------------------|--------------------|------------------------|--------------------|
| FILE NAME = c:\pw_work\PIWIDOT\SWARTZRW\d0138944\d7 | USER NAME = swartzrw 4158-shr-removal120.dgn | DESIGNED - DRAWN - | REVISED - REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REMOVAL SHEETS | | | F.A.P. RTE. 322 | SECTION (58,20)RS | COUNTY MACON | TOTAL SHEETS 151 | SHEET NO. 28 |
| | PLOT SCALE = 120.0000' / IN. | CHECKED - | REVISED - | | SCALE: 100 | SHEET NO. 3 | OF 8 SHEETS | STA. 144+00 | TO STA. 174+00 | CONTRACT NO. 74150 | | |
| | PLOT DATE = 3/17/2018 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | | | | | | | | |



LEGEND

-  P.C.C. SURFACE REMOVAL (BUTT JOINT)
-  APPROACH SLAB REMOVAL
-  BRIDGE APPROACH SHOULDER REMOVAL
-  PAVED SHOULDER REMOVAL
-  CURB REMOVAL
-  HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
-  HOT-MIX ASPHALT SURFACE REMOVAL (BUTT JOINT)



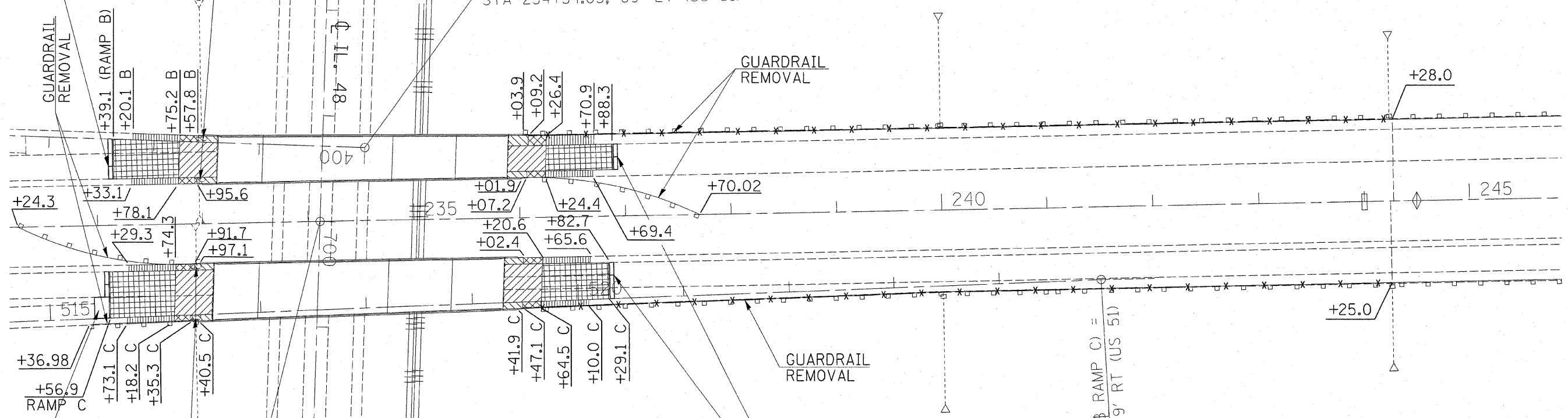
| | | | | | | | | | | | | |
|---------------------------------------|------------------------------|------------|-----------|---|---------------------------|-------------------------|----------------------------|-------------|-----------|--------|--------------|-----------|
| FILE NAME = | USER NAME = swartzw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REMOVAL SHEETS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ca:\pwork\PM1001\SWARTZRW\d0138944\d7 | 4150-shr-removal100.dgn | DRAWN - | REVISED - | | SCALE: 100 | SHEET NO. 4 OF 8 SHEETS | STA. 232+00 TO STA. 262+00 | 322 | (58,20)RS | MACON | 151 | 29 |
| | PLOT SCALE = 100.0000' / IN. | CHECKED - | REVISED - | | CONTRACT NO. 74150 | | | | | | | |
| | PLOT DATE = 3/17/2018 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | | | | | | | | |



CLASS D PATCHES
SEE SCHEDULE FOR
QUANTITIES AND
LOCATIONS

REMOVE INLET BOX AND END SECTION,
PLUG AND ABANDON PIPE DRAIN
STA. 401+54.58 RAMP C, RT.
STA. 232+98.47 US 51, LT. MEDIAN

STA 400+00.00 (RAMP B) =
STA 234+54.03, 69' LT (US 51)



CLASS D PATCHES
SEE SCHEDULE FOR
QUANTITIES AND
LOCATIONS








REMOVE INLET BOX AND END SECTION,
PLUG AND ABANDON PIPE DRAIN
STA. 516+38.20 RAMP C, RT.
STA. 232+93.06 US 51, RT. MEDIAN

CLASS D PATCHES
SEE SCHEDULE FOR
QUANTITIES AND
LOCATIONS

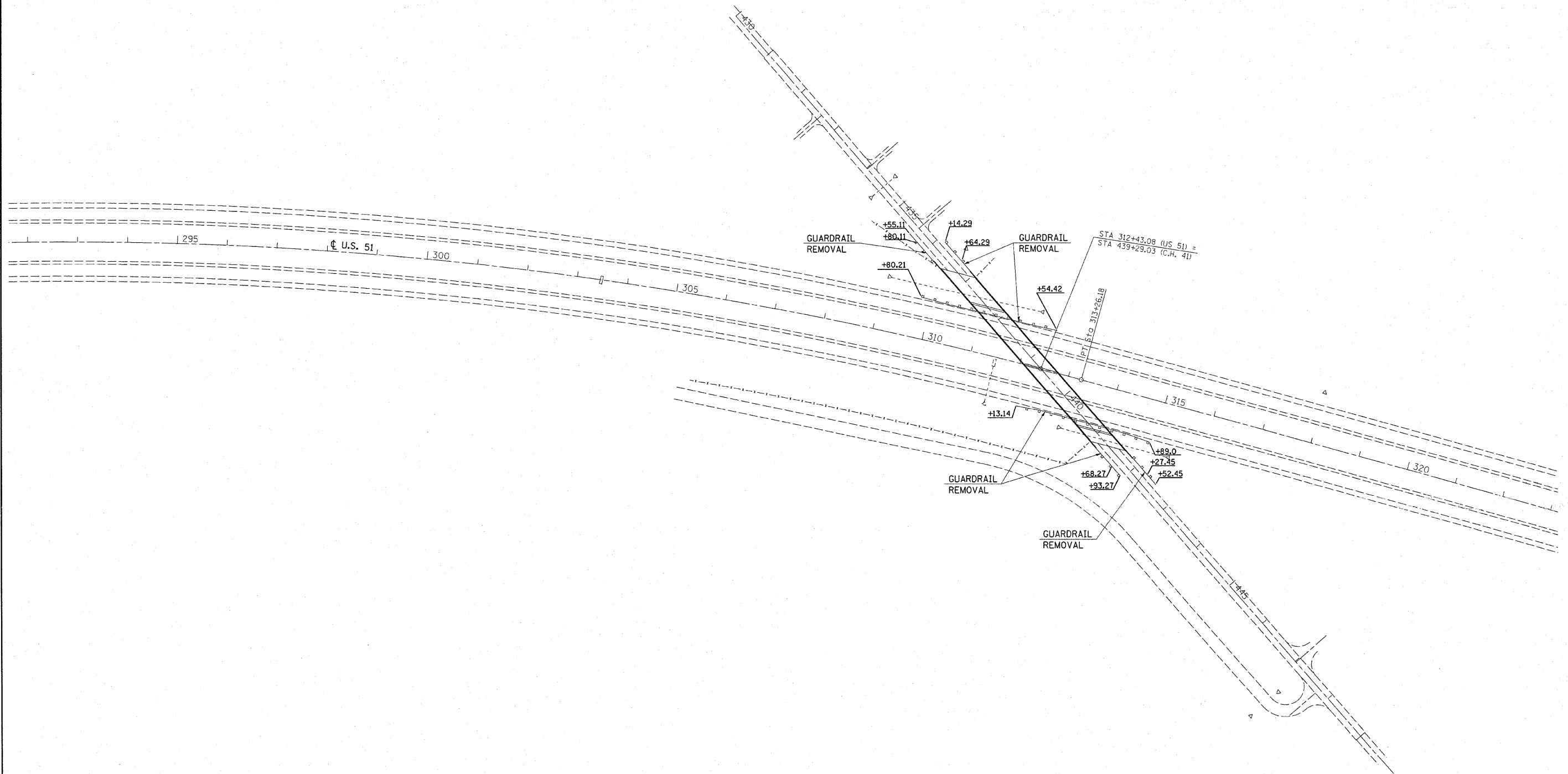
STA 524+96.33 (RAMP C) =
STA 241+49.81, 69' RT (US 51)

STA 234+11.34 (US 51) =
STA 700+00.00 (IL 48)

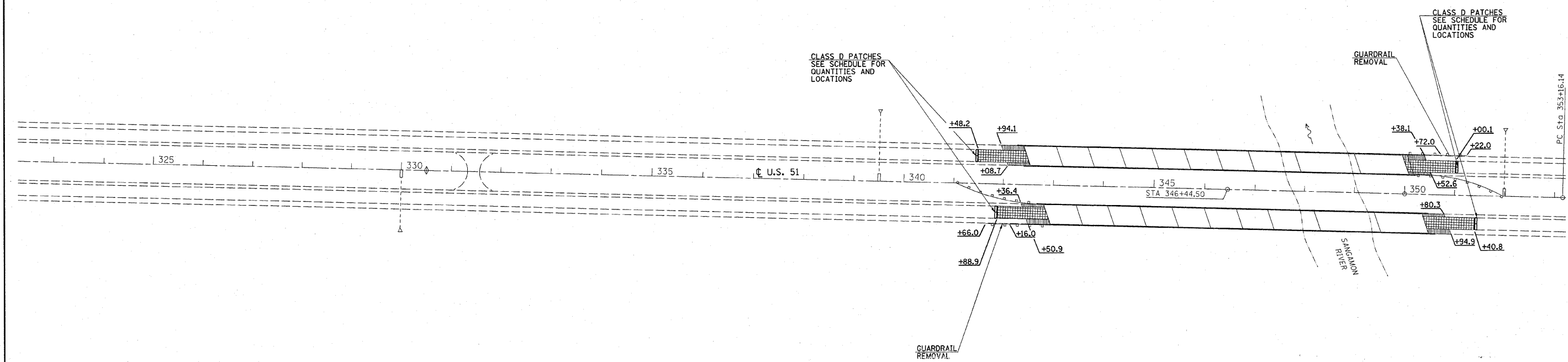
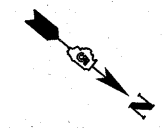
LEGEND

-  P.C.C. SURFACE REMOVAL (BUTT JOINT)
-  APPROACH SLAB REMOVAL
-  BRIDGE APPROACH SHOULDER REMOVAL
-  PAVED SHOULDER REMOVAL
-  CURB REMOVAL
-  HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
-  HOT-MIX ASPHALT SURFACE REMOVAL (BUTT JOINT)



| | | | | | | | | | | | | |
|--|---|-----------------------|------------------------|---|--|-------------------------|----------------------------|----------------------|-----------------|------------------------|--------------------|--|
| FILE NAME = c:\pwork\PWIDOT\SWARTZRW\d013894\d7 | USER NAME = swartzrw 4150-shl-removal100.dgn | DESIGNED - DRAWN - | REVISED - REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REMOVAL SHEETS 50 SCALE DETAIL OF SHEET #29 | | F.A.P. RTE. 322 | SECTION (58,20)RS | COUNTY MACON | TOTAL SHEETS 151 | SHEET NO. 30 | |
| PLOT SCALE = 50,0000' / 1" / INL | | CHECKED - | REVISED - | | SCALE: 50 | SHEET NO. 5 OF 8 SHEETS | STA. 232+00 TO STA. 240+00 | CONTRACT NO. 74150 | | | | |
| PLOT DATE = 3/17/2010 | | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | | | | | | | | |



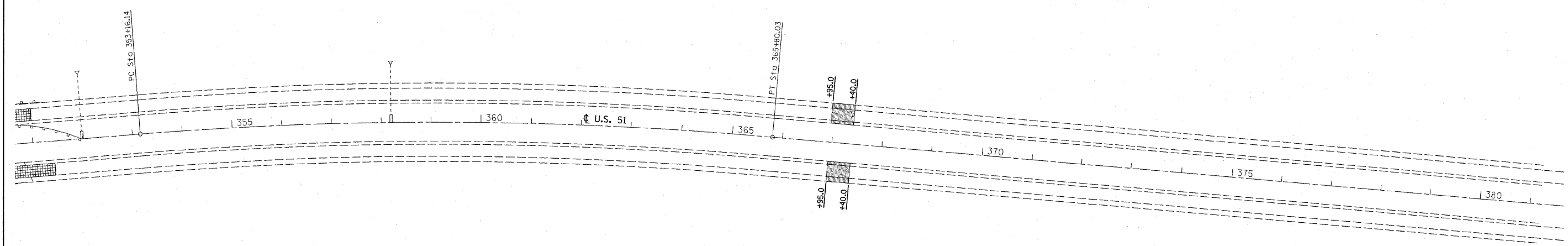
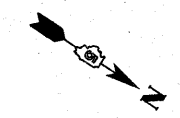
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| | 4150-shr-removal100.dgn | DRAWN - | REVISED - | | 322 | (58,20)RS | MACON | 151 | 31 | | | |
| PLOT SCALE = 100.0000' / IN. | CHECKED - | REVISED - | SCALE: 100 | | | SHEET NO. 6 OF 8 SHEETS | | | STA. 292+00 TO STA. 323+00 | ILLINOIS FED. AID PROJECT | | |
| PLOT DATE = 3/17/2010 | DATE - | REVISED - | CONTRACT NO. 74150 | | | | | | | | | |





LEGEND

-  HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
-  HOT-MIX ASPHALT SURFACE REMOVAL (BUTT JOINT)

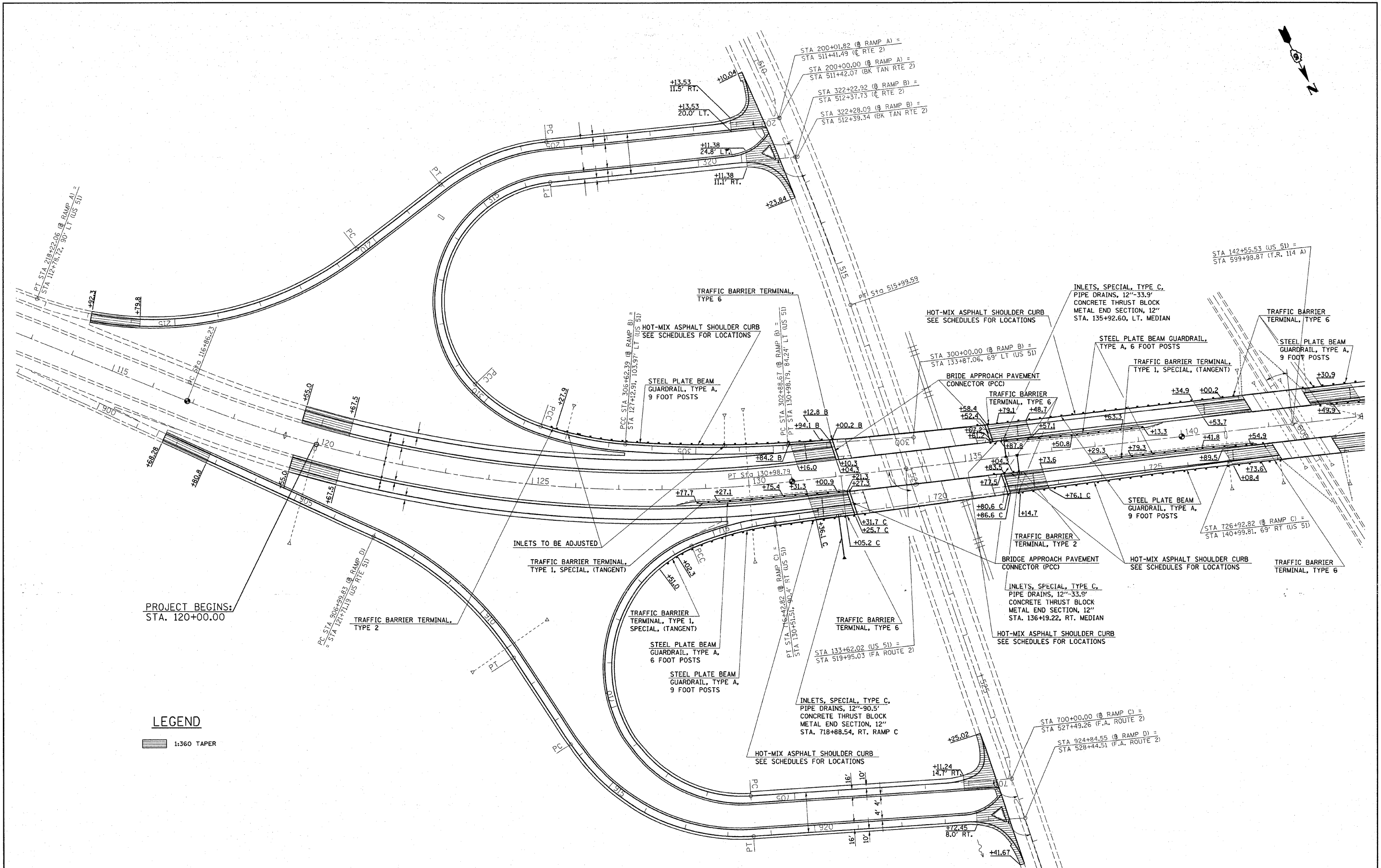
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| FILE NAME = c:\pw_work\PWIDOT\SWARTZRW\d0138944\d7 | USER NAME = swartzw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REMOVAL SHEETS | | | F.A.P. RTE. 322 | SECTION (58,20)RS | COUNTY MACON | TOTAL SHEETS 151 | SHEET NO. 32 |
| | PLOT SCALE = 100,0000' / IN. | DRAWN - | REVISED - | | SCALE: 100 | SHEET NO. 7 OF 8 SHEETS | STA. 323+00 TO STA. 353+00 | CONTRACT NO. 74150 | | | | |
| PLOT DATE = 3/17/2010 | CHECKED - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | | | |
| | DATE - | REVISED - | | | | | | | | | | |



LEGEND

-  PCC SURFACE REMOVAL (BUTT JOINT)
-  HOT-MIX ASPHALT SURFACE REMOVAL (BUTT JOINT)

| | | | | | | | | | | | | |
|---|-------------------------|------------|--|---|-----------------------|--------------------|--|----------------|-----------|--------|-----------------|--------------|
| FILE NAME = c:\pwork\pwork\SWARTZR\08138944\d7 | USER NAME = swartzr | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REMOVAL SHEETS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 4158-aht-removal100.dgn | DRAWN - | REVISED - | | | | | 322 | (58,20)RS | MACON | 151 | 33 |
| PLOT SCALE = 100.0000' / IN. | CHECKED - | REVISED - | SCALE: 100 SHEET NO. 8 OF 8 SHEETS STA. 351+00 TO STA. 381+00 | | | CONTRACT NO. 74150 | | | | | | |
| PLOT DATE = 3/17/2018 | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | | | |



PROJECT BEGINS:
STA. 120+00.00

LEGEND

1:360 TAPER

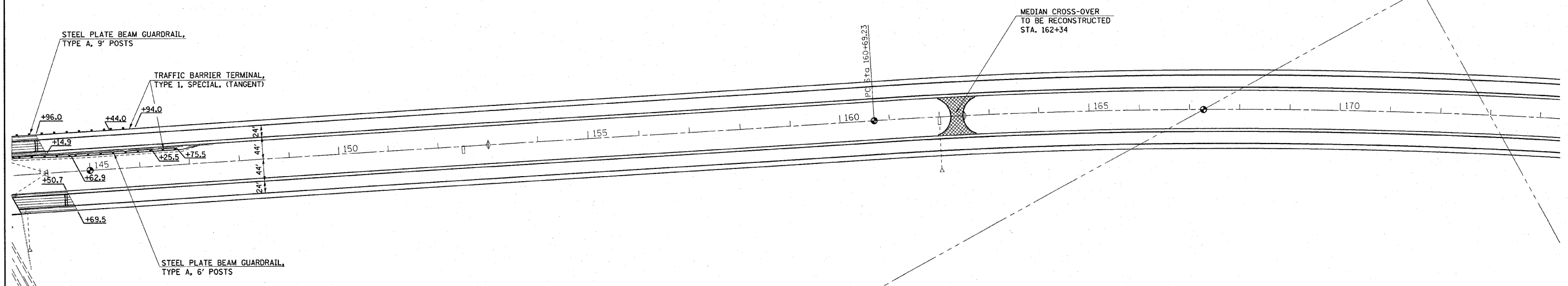
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| c:\pwwork\pwwork\swartzw\d0138944\0774150-sh1-plan100.dgn | | DRAWN - | REVISED - |
| | | CHECKED - | REVISED - |
| | | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: 100 SHEET NO. 1 OF 12 SHEETS STA. 113+00 TO STA. 144+00

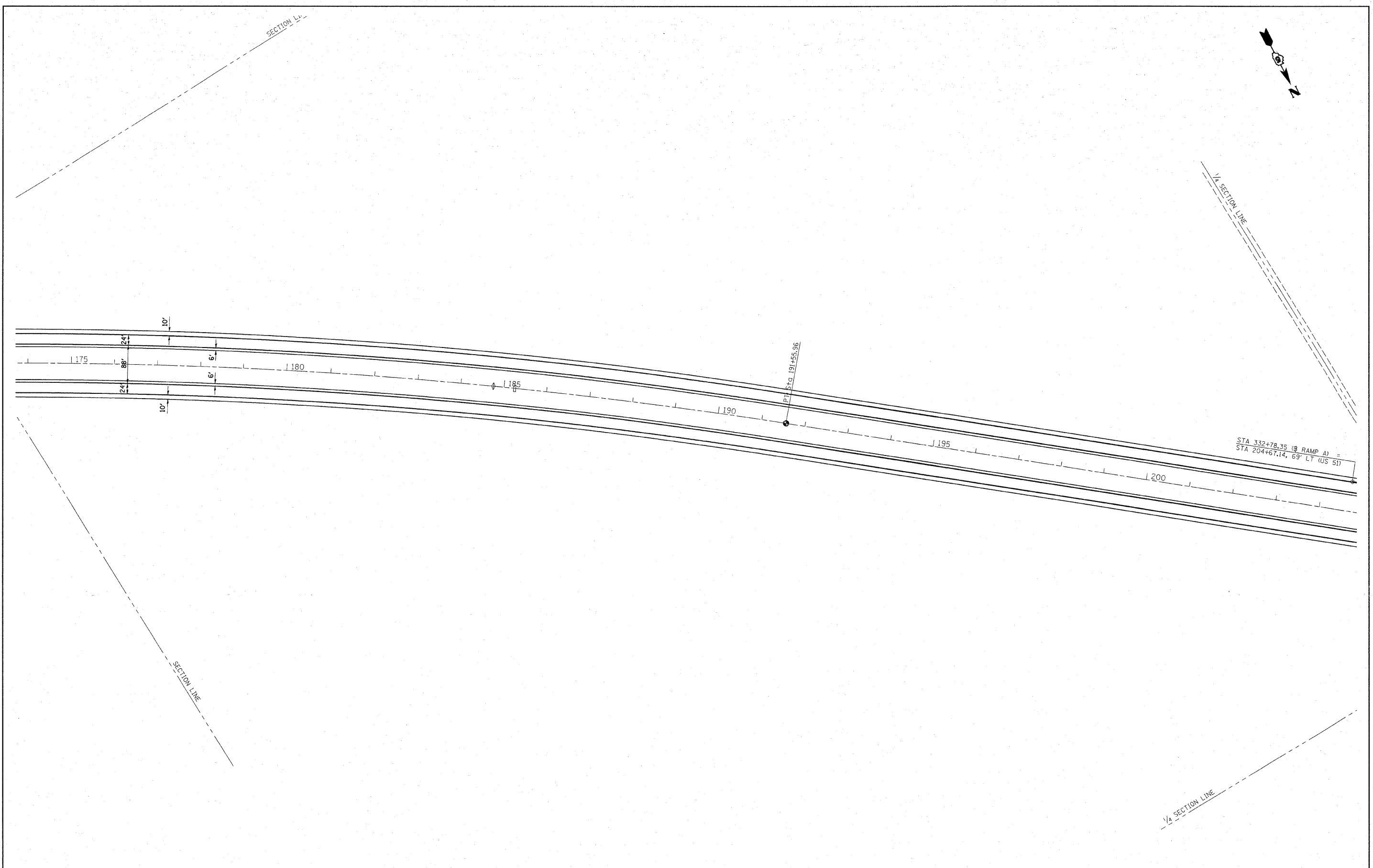
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|---------------------------|-----------|--------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 322 | (58,20)RS | MACON | 151 | 34 |
| CONTRACT NO. 74150 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



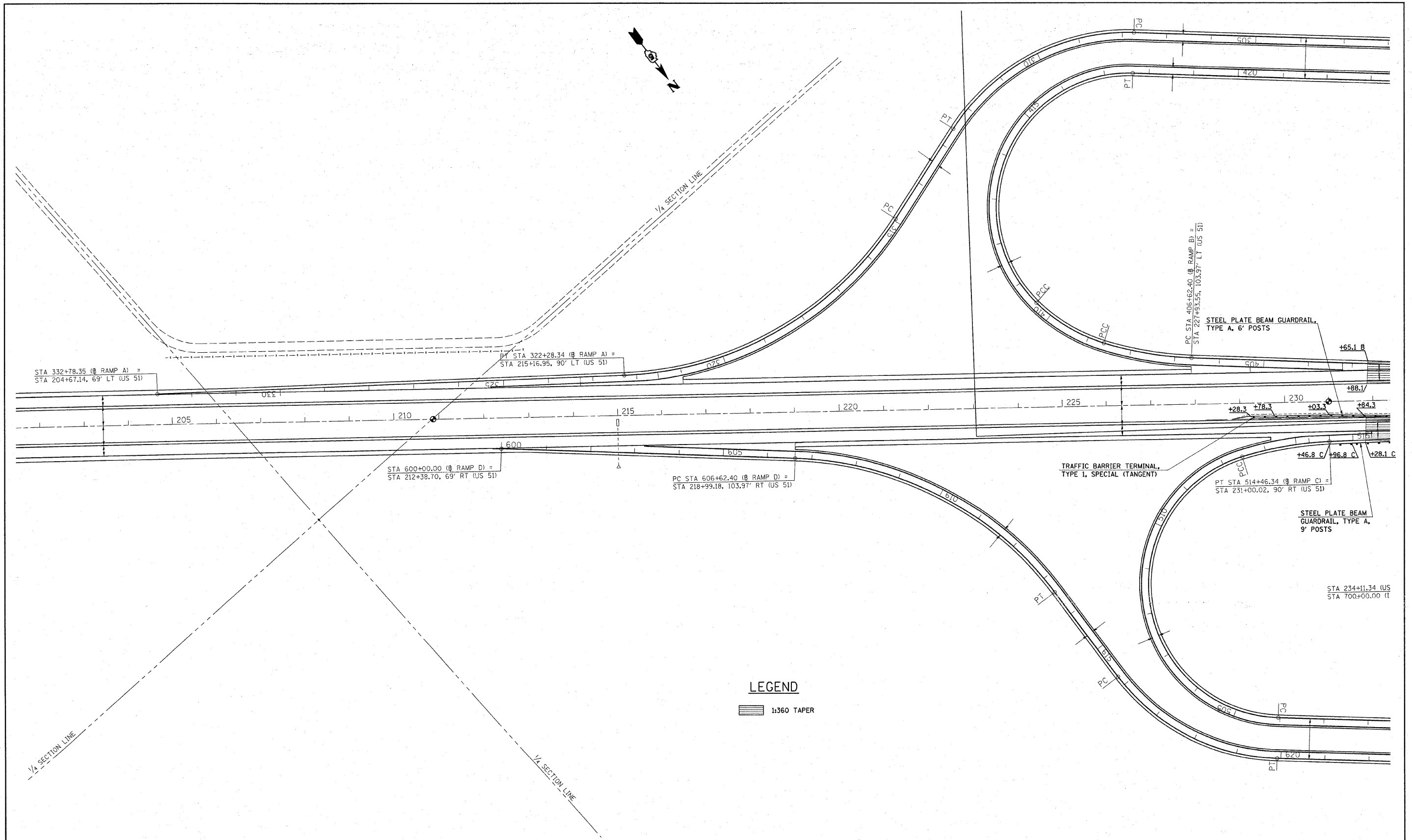
LEGEND

- 1:360 TAPER
- MEDIAN CROSS-OVER RECONSTRUCTION

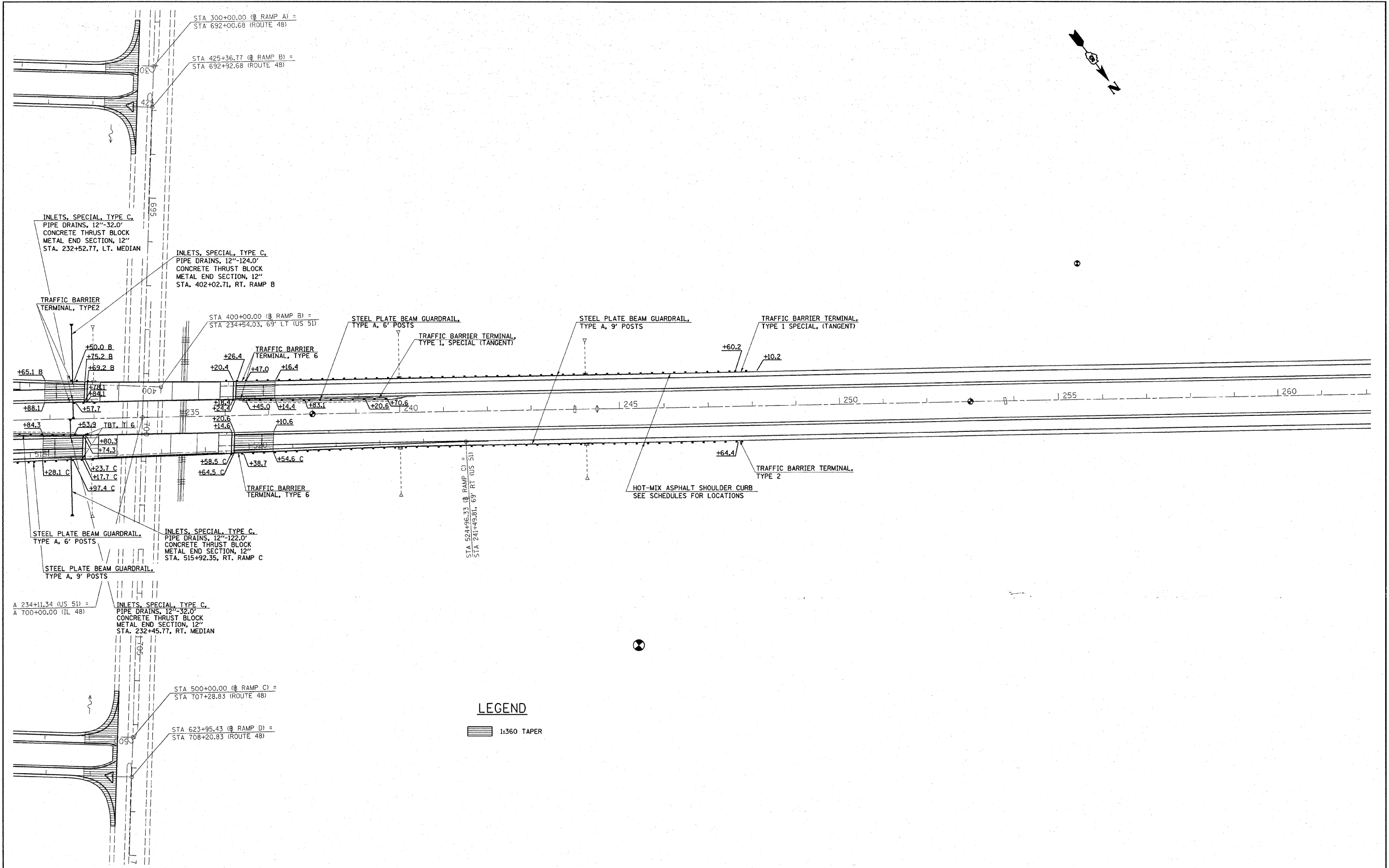
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| FILE NAME = | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN SHEET | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ct:\pw_work\pwidot\swartzrw\0138944\077 | 150-shr-plan100.dgn | DRAWN - | REVISED - | | SCALE: 100 | SHEET NO. 3 OF 12 SHEETS | STA. 144+00 TO STA. 174+00 | 322 | (58,20)RS | MACON | 151 | 36 |
| | PLOT SCALE = 100.0000 ' / IN. | CHECKED - | REVISED - | | | | | CONTRACT NO. 74150 | | | | |
| | PLOT DATE = 3/17/2018 | DATE - | REVISED - | | | | | ILLINOIS FED. AID PROJECT | | | | |



| | | | | | | | | | | | | |
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| FILE NAME = c:\pwork\pwt\dot\swartzw\d0138944\0774158-sht-plan100.dgn | USER NAME = swartzw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN SHEET | | | F.A.P. RTE. 322 | SECTION (58,20)RS | COUNTY MACON | TOTAL SHEETS 151 | SHEET NO. 37 |
| | PLOT SCALE = 100.0000' / IN. | CHECKED - | REVISED - | | SCALE: 100 | SHEET NO. 4 OF 12 SHEETS | STA. 174+00 TO STA. 204+00 | CONTRACT NO. 74150 | | | | |
| | PLOT DATE = 3/17/2010 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |
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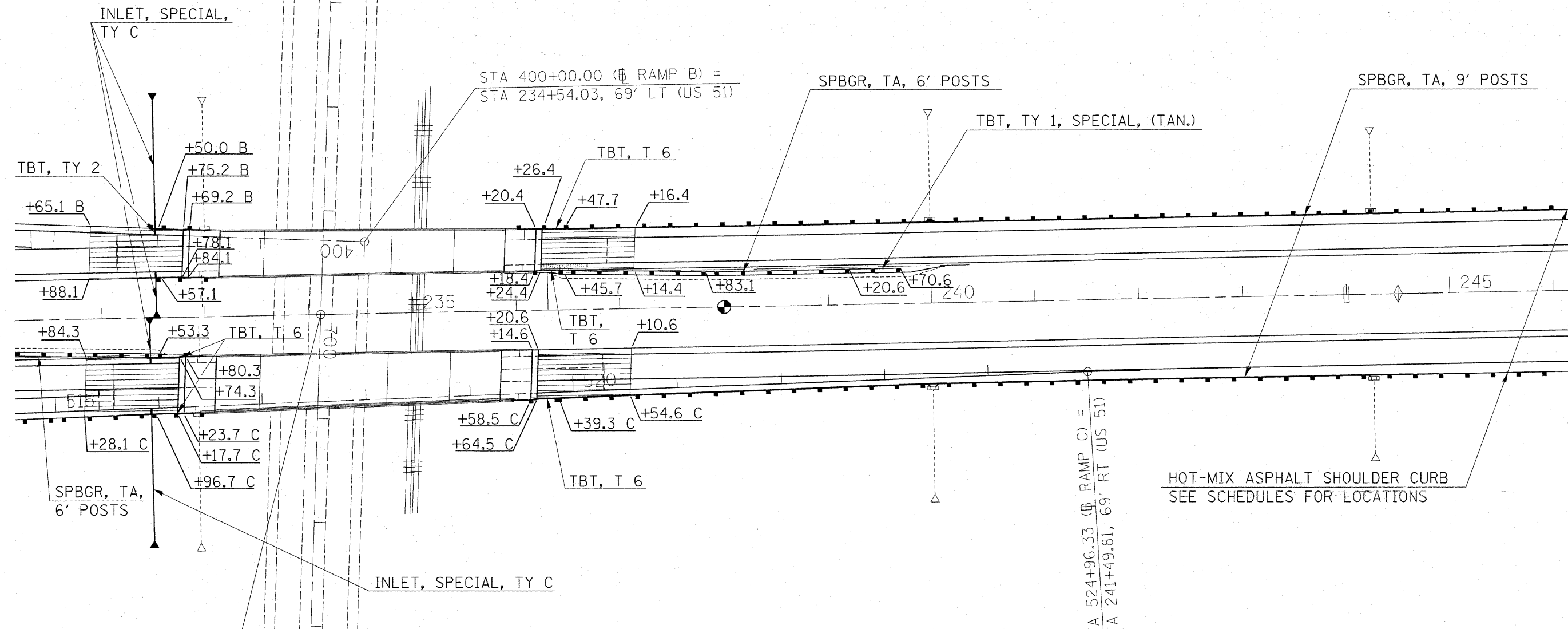


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| FILE NAME = | USER NAME = swartzw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN SHEET | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwwork\pwwork\swartzw\d0130944\d077450-sh1-plen100.dgn | DRAWN - | REVISED - | 322 | | | (58,20RS | MACON | 151 | 38 | |
| PLOT SCALE = 100.0000 / IN. | CHECKED - | REVISED - | CONTRACT NO. 74150 | | | | | | | |
| PLOT DATE = 3/17/2010 | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | SCALE: 100 | SHEET NO. 5 OF 12 SHEETS | | STA. 202+00 TO STA. 232+00 | | |




LEGEND
 1:360 TAPER

| | | | | | | | | | | | | |
|---|---------------------|------------|-----------|---|-------------------|--------------------------|----------------------------|---------------------------|-----------|--------------|-----------|----|
| FILE NAME = | USER NAME = swartzw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN SHEET | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| c:\pwwork\pwwork\swartzw\d0138944\077150-sh-t-plan100.dgn | | DRAWN - | REVISED - | | SCALE: 100 | SHEET NO. 6 OF 12 SHEETS | STA. 232+00 TO STA. 262+00 | 322 | (58,20)RS | MACON | 151 | 39 |
| PLOT SCALE = 100.0000' / IN. | | CHECKED - | REVISED - | | | | | CONTRACT NO. 74150 | | | | |
| PLOT DATE = 3/17/2010 | | DATE - | REVISED - | | | | | ILLINOIS FED. AID PROJECT | | | | |
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LEGEND

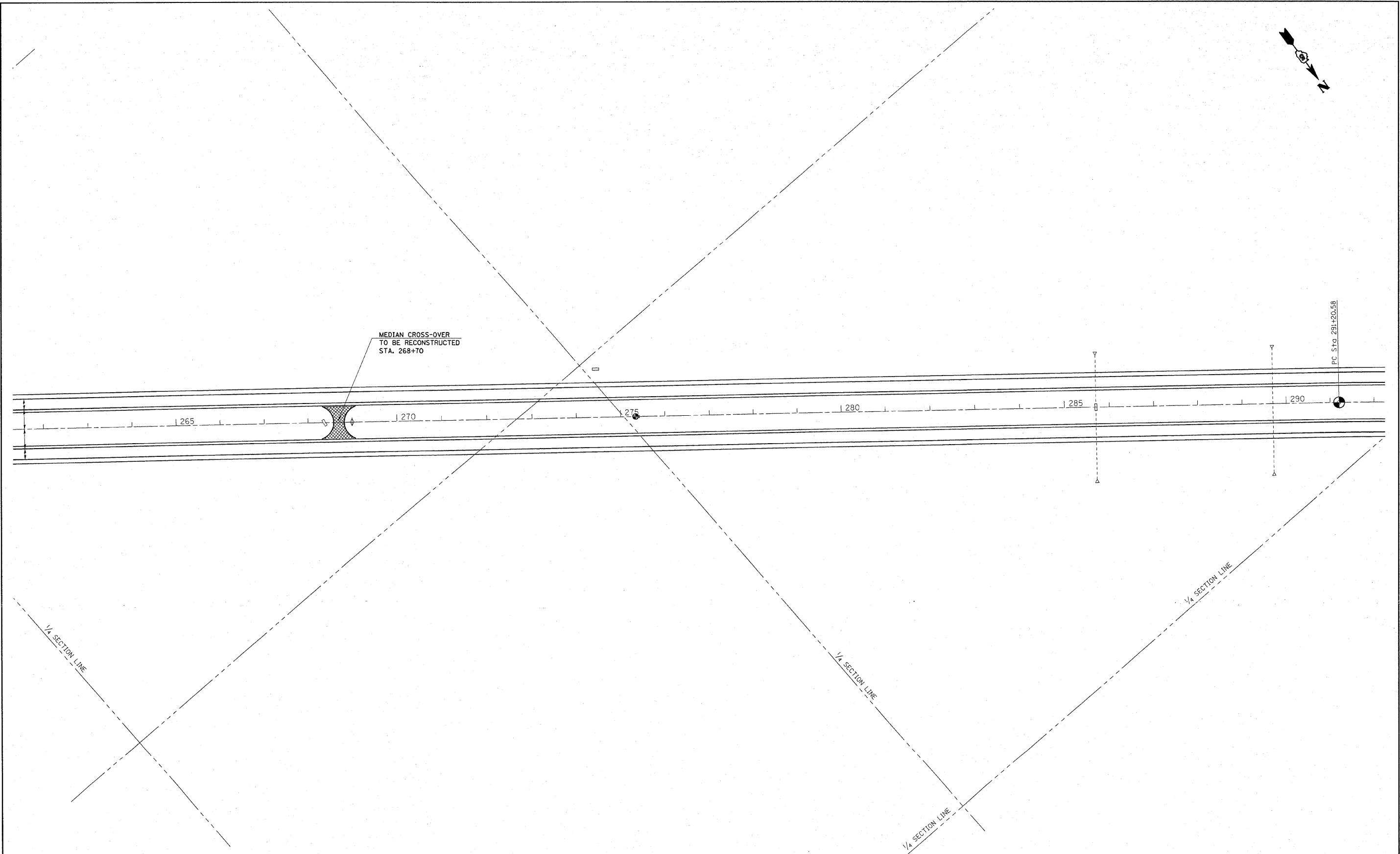
 1:360 TAPER

HOT-MIX ASPHALT SHOULDER CURB
SEE SCHEDULES FOR LOCATIONS

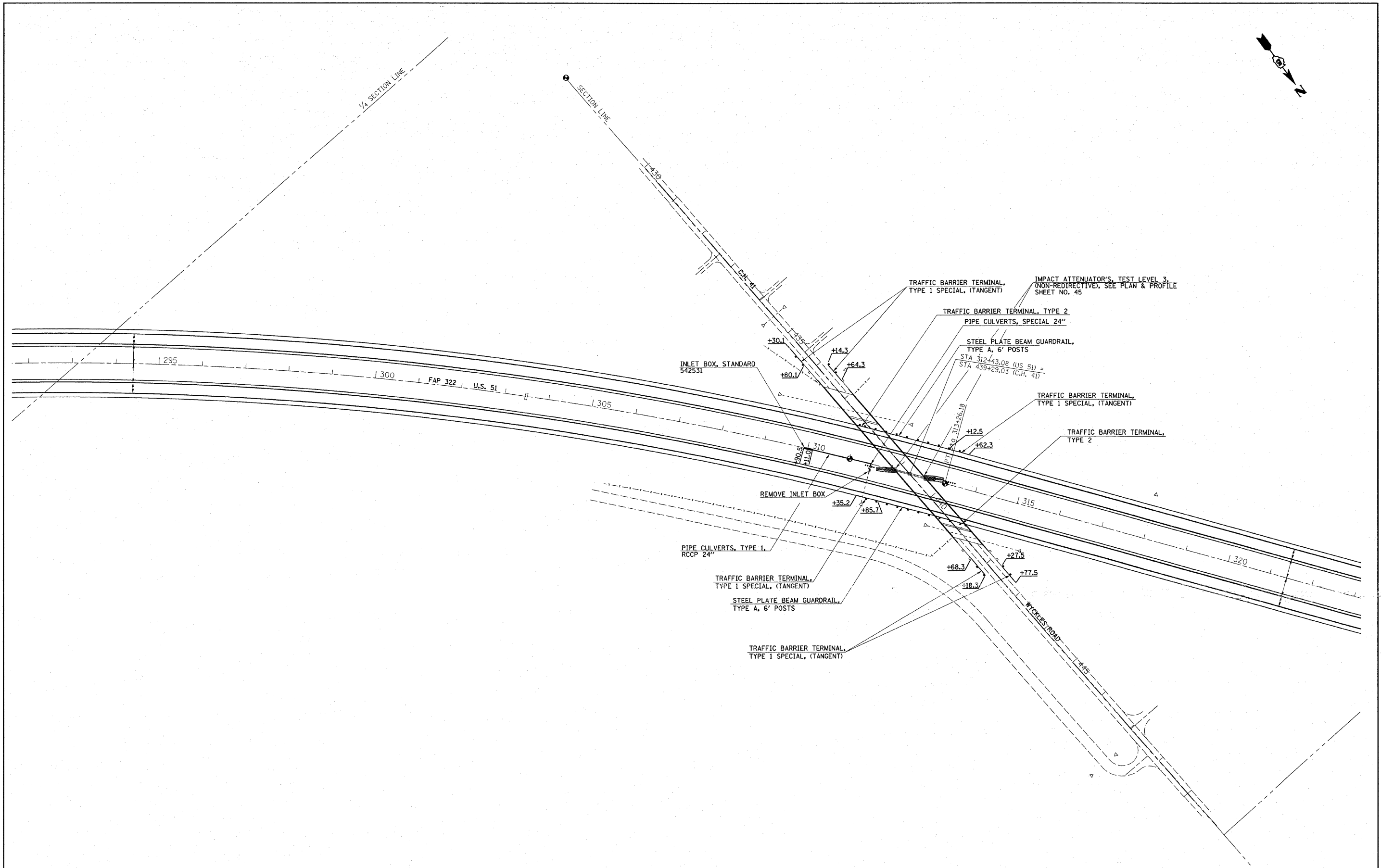
STA 234+11.34 (US 51) =
STA 700+00.00 (IL 48)

STA 524+96.33 (RAMP C) =
STA 241+49.81, 69' RT (US 51)

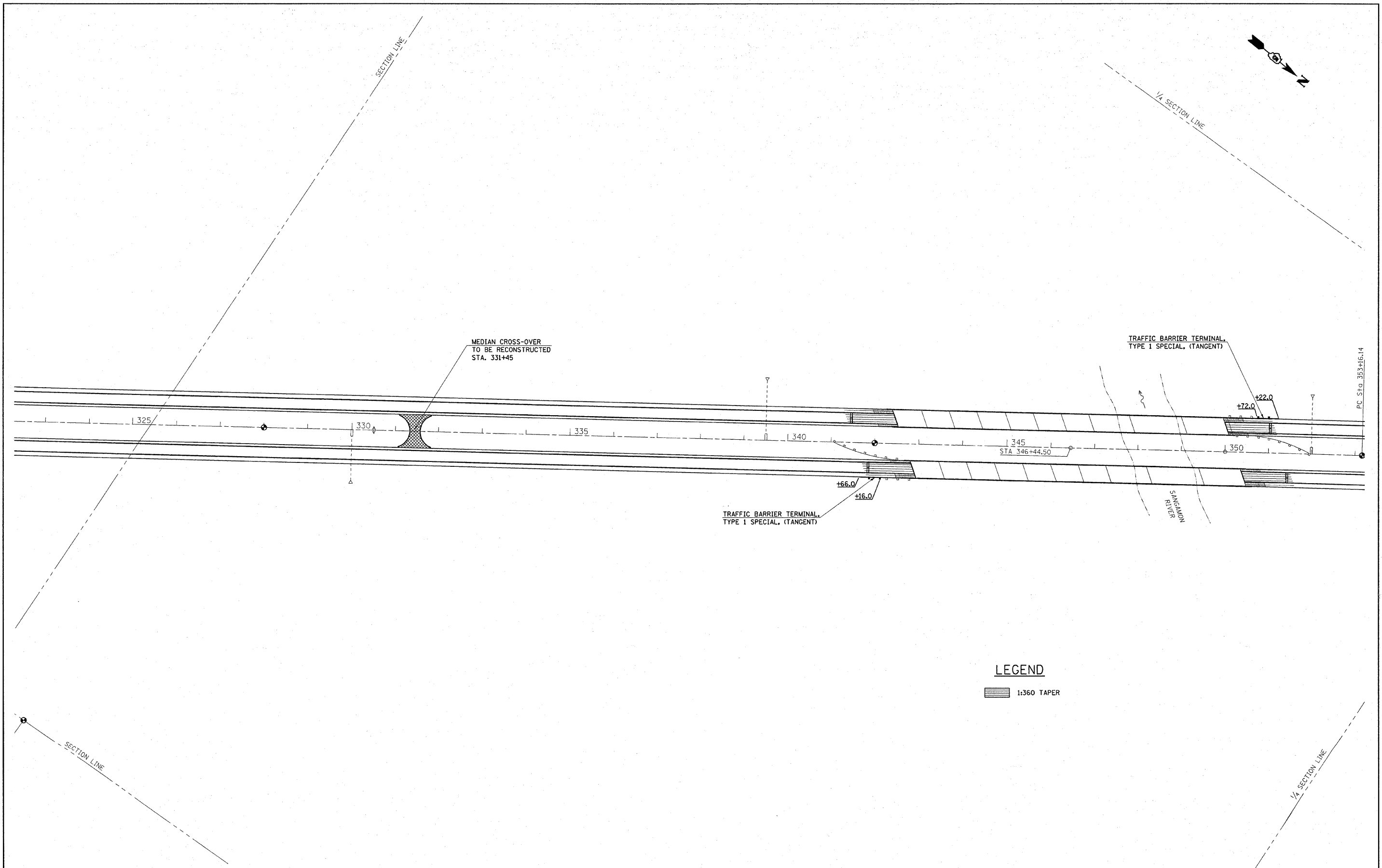
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| c:\pwork\pwork\swartzw\d0138944\077150-shr-plan100.dgn | | DRAWN - | REVISED - | | 322 | (58,20)RS | MACON | 151 | 40 | | |
| PLOT SCALE = 50.0000' / IN. | | CHECKED - | REVISED - | | SCALE: 50 | | | SHEET NO. 7 OF 12 SHEETS STA. 232+00 TO STA. 246+00 | | | |
| PLOT DATE = 3/17/2010 | | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 74150 | | | |




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| | 158-shr-plan100.dgn | DRAWN - | REVISED - | | | | | 322 | (58,20)RS | MACON | 151 | 41 |
| | PLOT SCALE = 100.0000' / IN. | CHECKED - | REVISED - | | | | | CONTRACT NO. 74150 | | | | |
| | PLOT DATE = 3/17/2010 | DATE - | REVISED - | | | | | ILLINOIS FED. AID PROJECT | | | | |
| | | | | SCALE: 100 | SHEET NO. 8 OF 12 SHEETS | STA. 262+00 TO STA. 292+00 | | | | | | |



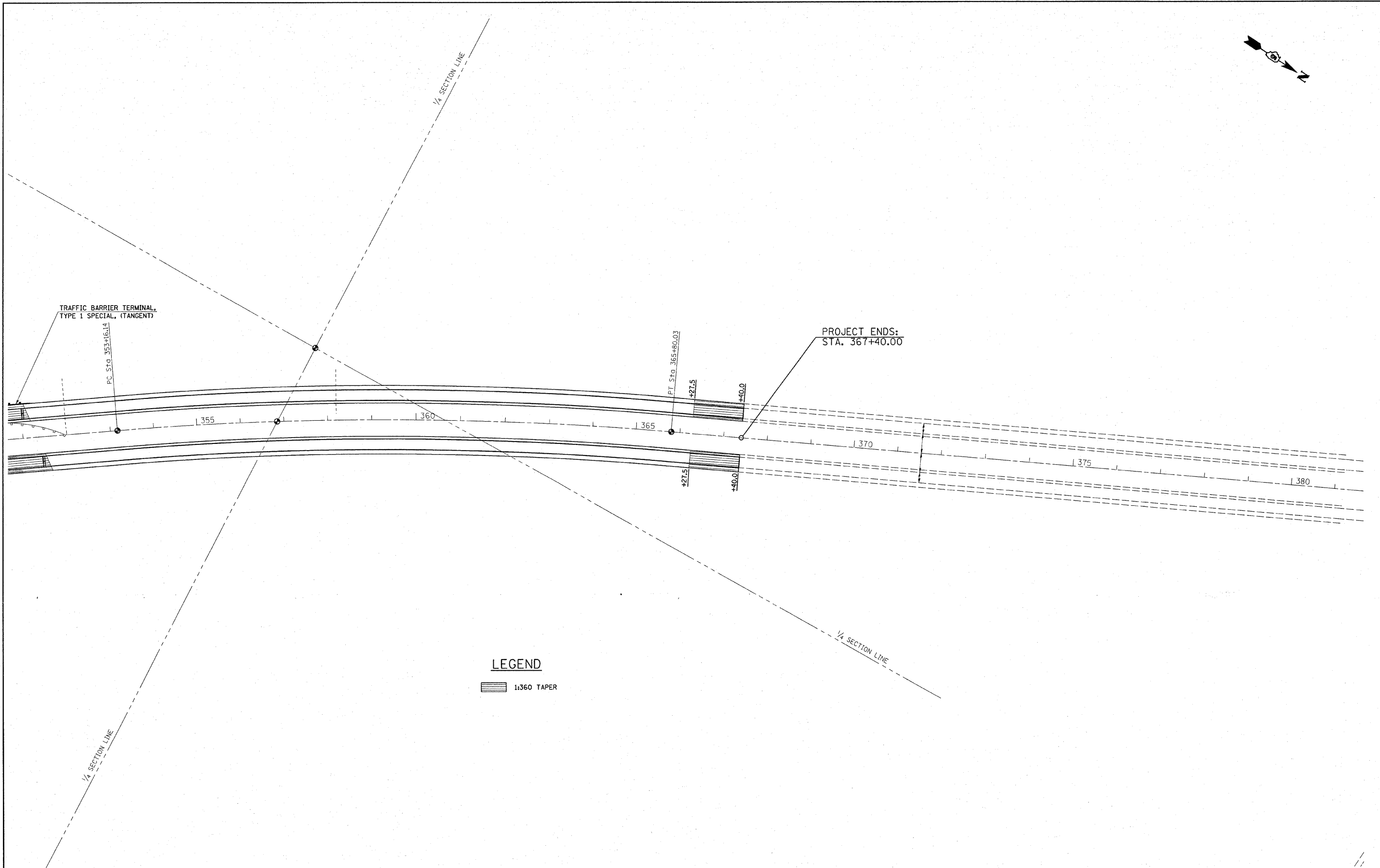
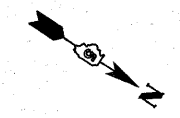
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| FILE NAME = | USER NAME = swartzw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN SHEET | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| c:\pwork\pwork\swartzw\d0138944\077450-sh-t-plan100.dgn | | DRAWN - | REVISED - | | SCALE: 100 | SHEET NO. 9 OF 12 SHEETS | STA. 292+00 TO STA. 323+00 | 322 | (58,20)RS | MACON | 151 | 42 |
| PLOT SCALE = 100.0000' / IN. | | CHECKED - | REVISED - | | | | | | | | CONTRACT NO. 74150 | |
| PLOT DATE = 3/17/2010 | | DATE - | REVISED - | | | | | | | | ILLINOIS FED. AID PROJECT | |



LEGEND

 1:360 TAPER

| | | | | | | | | | | | | |
|---|----------------------|------------|-----------|---|---------------------------|-----------|-------|---------------------------|---------|--------|----------------------------|-----------|
| FILE NAME = | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN SHEET | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwwork\pwwork\swartzrw\d0138944\0774150-shrt-plan100.dgn | | DRAWN - | REVISED - | | 322 | (58,20)RS | MACON | 151 | 43 | | | |
| PLOT SCALE = 100.0000' / IN. | | CHECKED - | REVISED - | | SCALE: 100 | | | SHEET NO. 10 OF 12 SHEETS | | | STA. 323+00 TO STA. 353+00 | |
| PLOT DATE = 3/17/2018 | | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 74150 | | | | |
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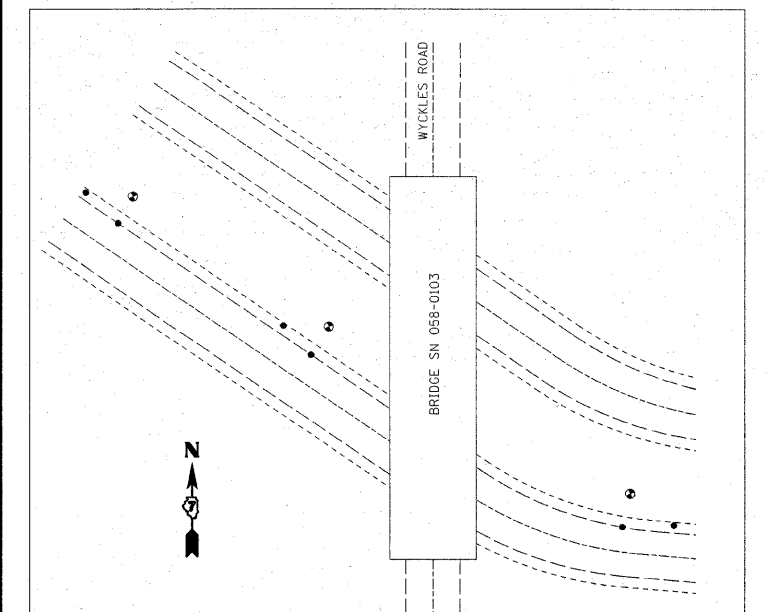
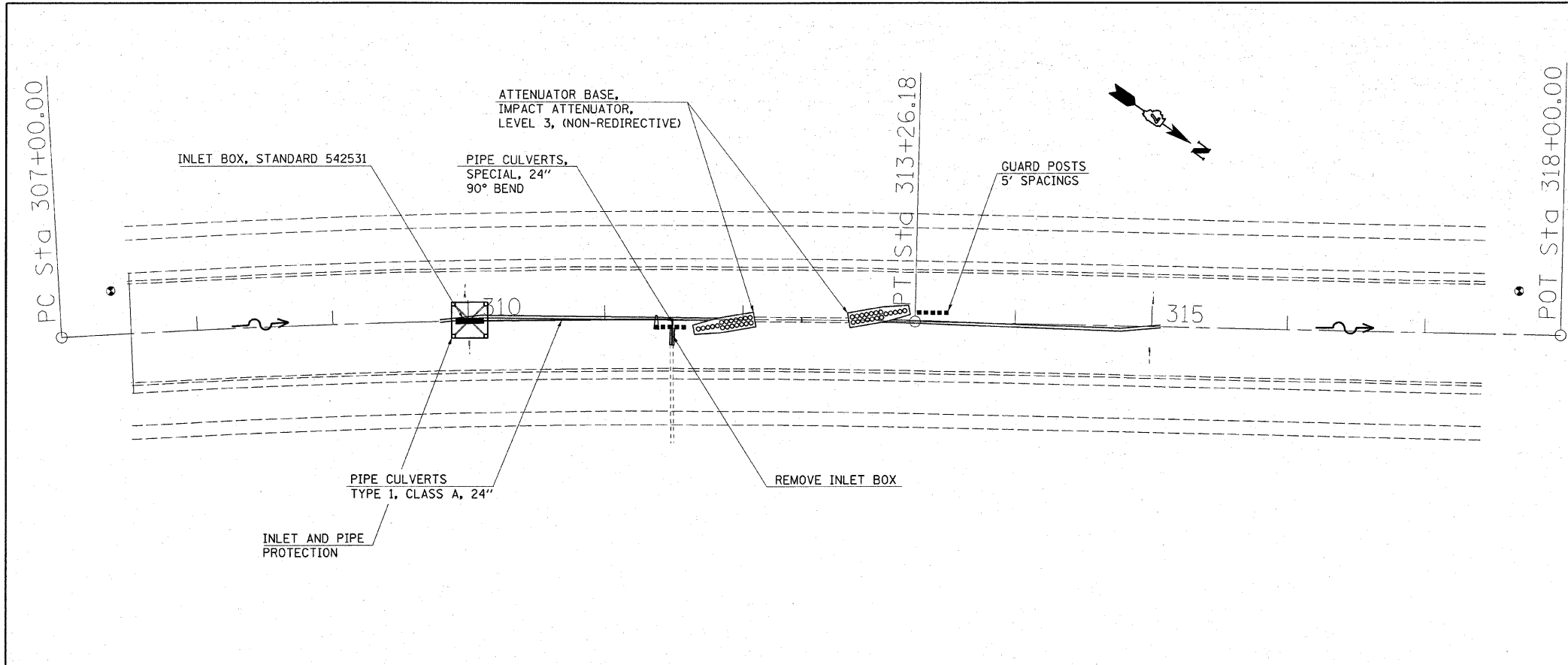
LEGEND

1:360 TAPER

| | | | | | | | | | | | | | |
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| FILE NAME = | USER NAME = swartzw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN SHEET | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| c:\pwork\pwork\swartzw\d8138944\0774150-sh-t-plen100.dgn | | DRAWN - | REVISED - | | SCALE: 100 | SHEET NO. 11 OF 12 SHEETS | STA. 351+00 | TO STA. 381+00 | 322 | (58,20)RS | MACON | 151 | 44 |
| PLOT SCALE = 100.0000 "/> | | | | | | | | | | | | | |
| PLOT DATE = 3/17/2010 | DATE - | REVISED - | REVISED - | | CONTRACT NO. 74150 | | | ILLINOIS FED. AID PROJECT | | | | | |

| | | |
|------|--------------------|------|
| PLAN | SURVEYED | DATE |
| | PLOTTED | BY |
| | CHECKED | |
| | REVISIONS | |
| | NO. OF MAY CHECKED | |
| | CADD FILE NAME | |
| | NO. | |

| | | |
|---------|------------------------|------|
| PROFILE | SURVEYED | DATE |
| | PLOTTED | BY |
| | CHECKED | |
| | REVISIONS | |
| | NO. OF MAY CHECKED | |
| | STRUCTURE NOTATION CHD | |
| | NO. | |

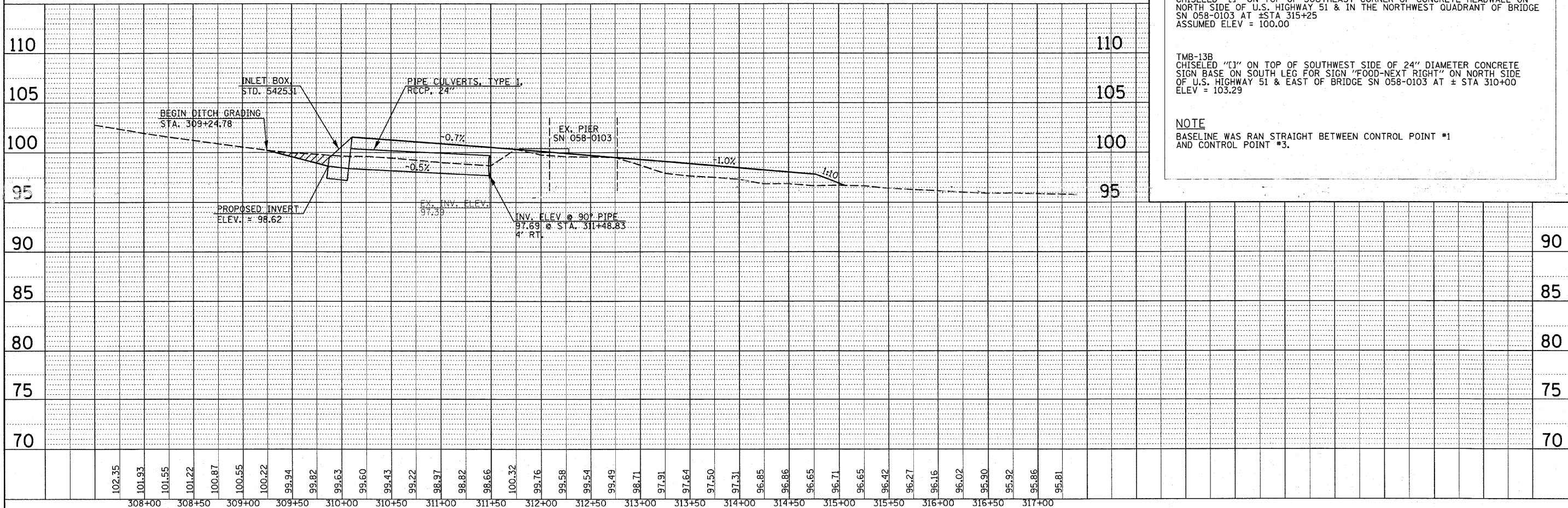


| | | |
|--|---|--|
| CONTROL POINT #1 SET 3/8" IRON ROD ±3" DEEP STA 317+69.23 N 10000.0000 E 10000.0000 ELEV. 98.88' | CONTROL POINT #2 SET 3/8" IRON ROD ±3" DEEP STA 322+61.18 N 10409.4089 E 9727.0242 ELEV. 97.45' | CONTROL POINT #3 SET 3/8" IRON ROD ±3" DEEP STA 307+36.25 N 9153.2583 E 10592.1916 ELEV. 106.24' |
|--|---|--|

BENCHMARKS
 TMB-13A
 CHISELED "CJ" ON TOP OF SOUTHEAST CORNER OF CONCRETE HEADWALL ON NORTH SIDE OF U.S. HIGHWAY 51 & IN THE NORTHWEST QUADRANT OF BRIDGE SN 058-0103 AT ±STA 315+25
 ASSUMED ELEV = 100.00










TMB-13B
 CHISELED "CJ" ON TOP OF SOUTHWEST SIDE OF 24" DIAMETER CONCRETE SIGN BASE ON SOUTH LEG FOR SIGN "FOOD-NEXT RIGHT" ON NORTH SIDE OF U.S. HIGHWAY 51 & EAST OF BRIDGE SN 058-0103 AT ± STA 310+00
 ELEV = 103.29

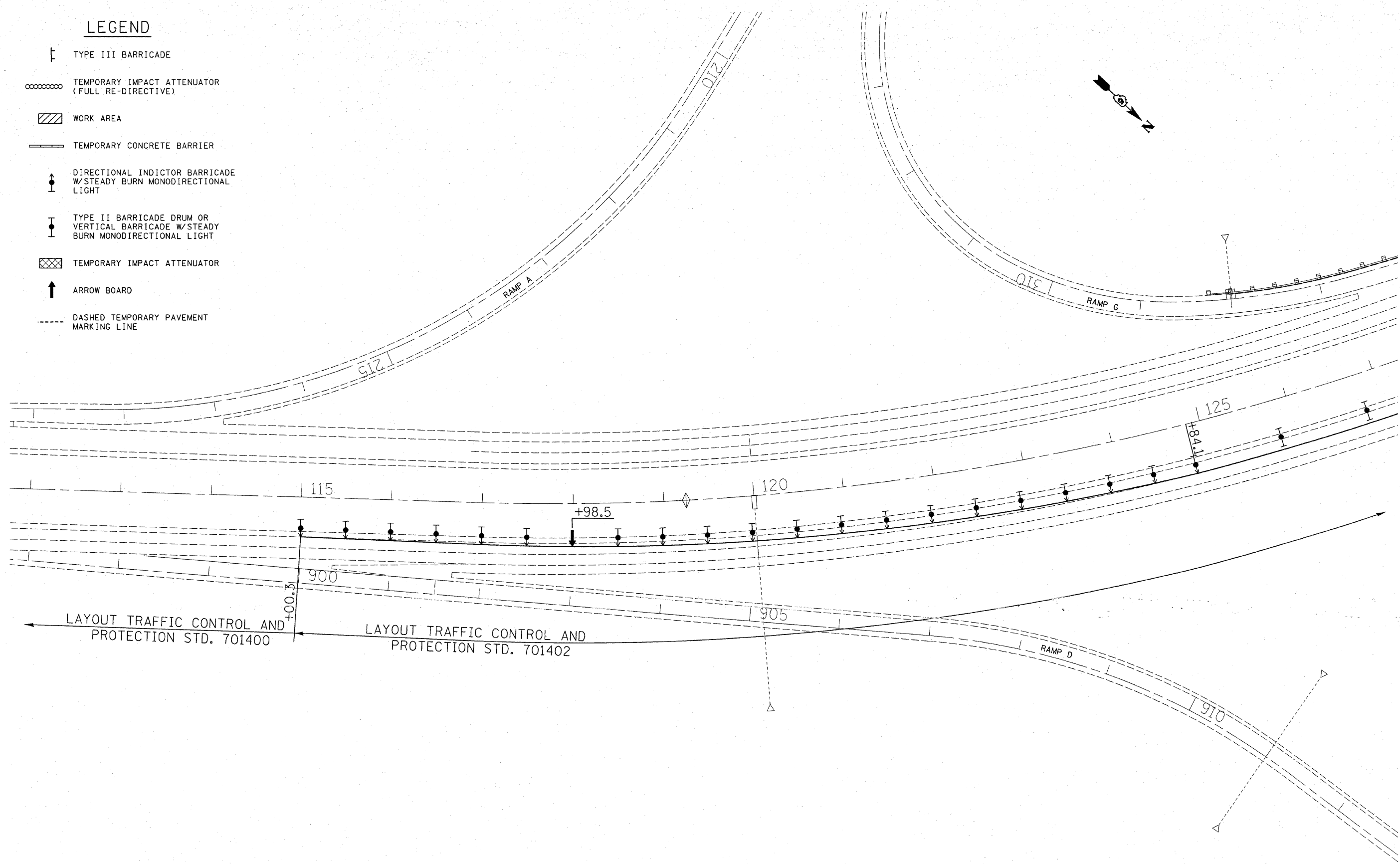
NOTE
 BASELINE WAS RAN STRAIGHT BETWEEN CONTROL POINT #1 AND CONTROL POINT #3.



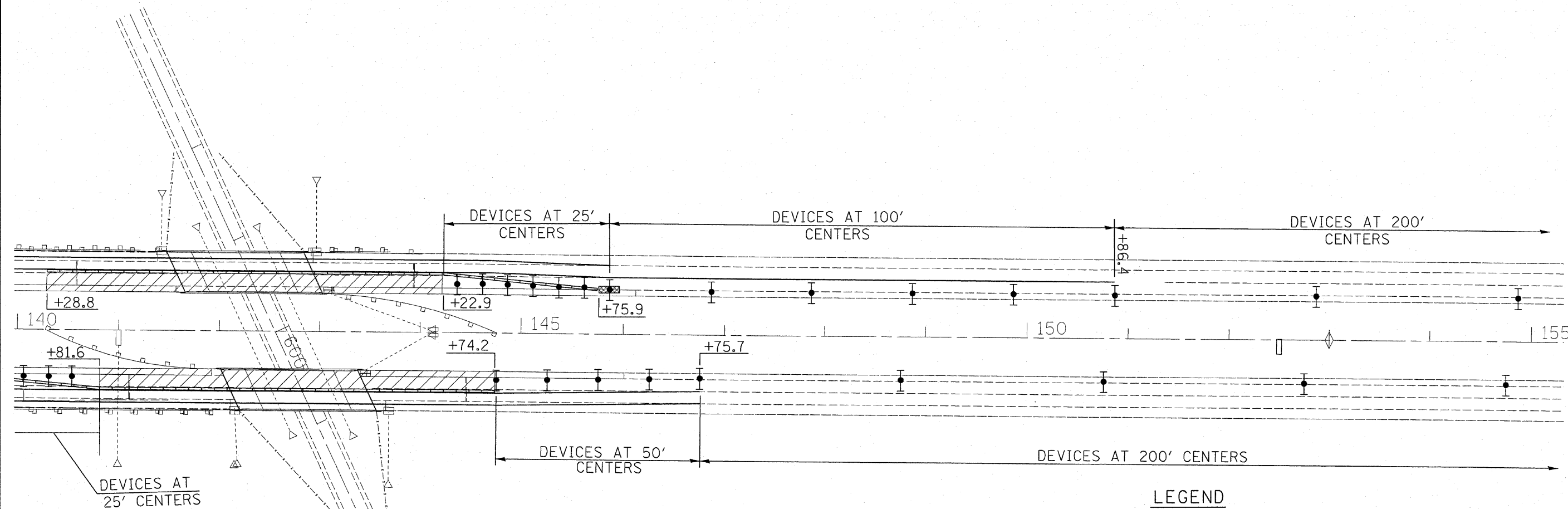
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| FILE NAME = | USER NAME = swartzw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | US 51 PLAN-PROFILE IMPACT ATTENUATOR | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| c:\pwwork\pwwork\swartzw\d8138944\0774150-shp-plan100.dgn | PLOT SCALE = 50.0000' / IN. | DRAWN - | REVISED - | | | 322 | (58,20)RS | MACON | 151 | 45 | |
| PLOT DATE = 3/17/2010 | DATE - | CHECKED - | REVISED - | | | SCALE: 50 | | SHEET NO. 12 OF 12 SHEETS | | STA. TO STA. | |
| | | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | |

LEGEND










-  TYPE III BARRICADE
-  TEMPORARY IMPACT ATTENUATOR (FULL RE-DIRECTIVE)
-  WORK AREA
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR BARRICADE W/ STEADY BURN MONODIRECTIONAL LIGHT
-  TYPE II BARRICADE DRUM OR VERTICAL BARRICADE W/ STEADY BURN MONODIRECTIONAL LIGHT
-  TEMPORARY IMPACT ATTENUATOR
-  ARROW BOARD
-  DASHED TEMPORARY PAVEMENT MARKING LINE



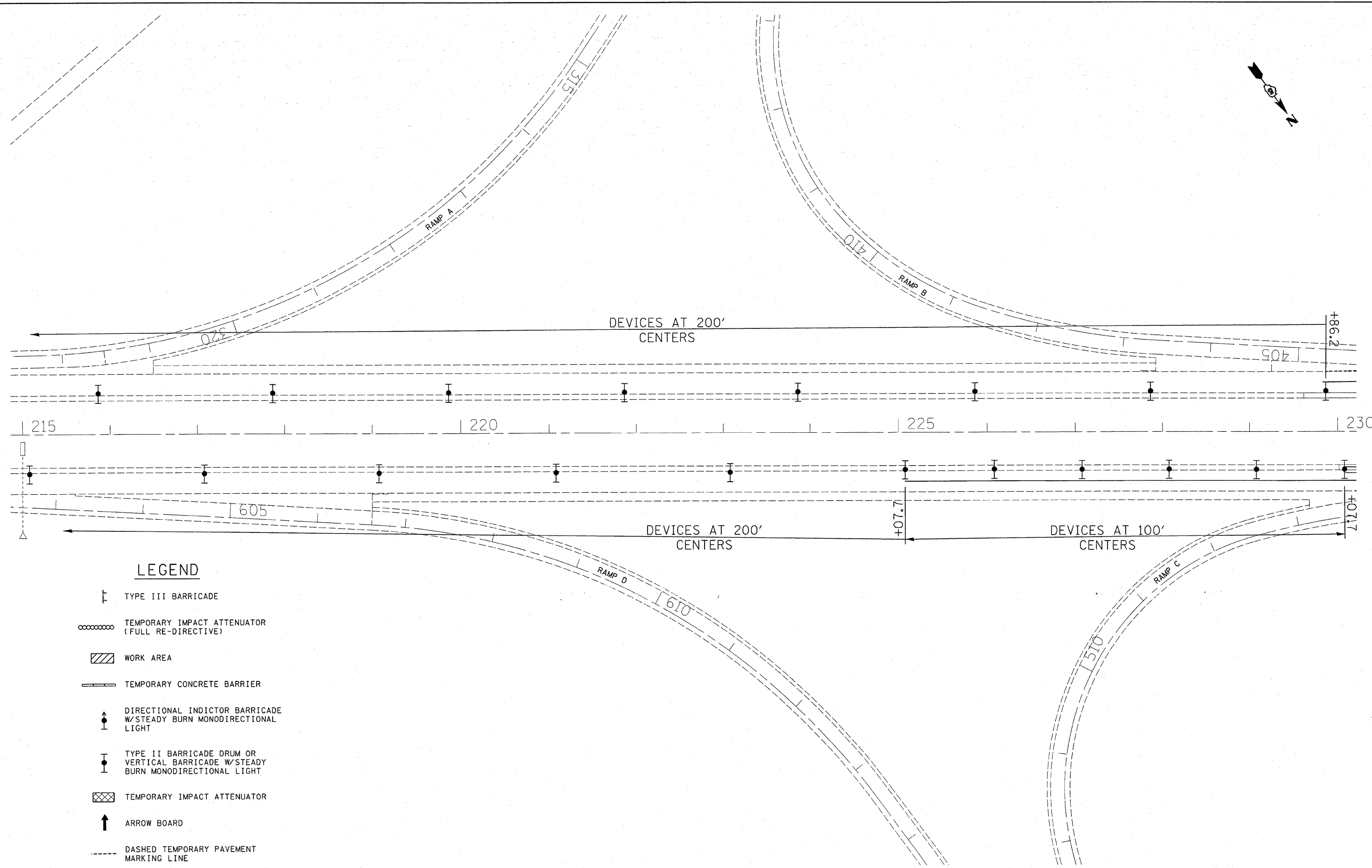
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| FILE NAME = | USER NAME = swartzw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | STAGE 1 TRAFFIC CONTROL | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | | |
| ci:\pw\work\PW1001\SWARTZRW\d0138944\d7 | 4158-sht-stage1trafficcontrol.dgn | DRAWN - | REVISED - | | 322 | (58-20)RS | MACON | 151 | 46 | | | | | |
| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - | | SCALE: 50 | | | SHEET NO. 1 OF 6 SHEETS | | | STA. 112+00 TO STA. 127+00 | | CONTRACT NO. 74150 | |
| | PLOT DATE = 3/17/2018 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | | | |




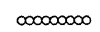







LEGEND

-  TYPE III BARRICADE
-  TEMPORARY IMPACT ATTENUATOR (FULL RE-DIRECTIVE)
-  WORK AREA
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR BARRICADE W/STEADY BURN MONODIRECTIONAL LIGHT
-  TYPE II BARRICADE DRUM OR VERTICAL BARRICADE W/STEADY BURN MONODIRECTIONAL LIGHT
-  TEMPORARY IMPACT ATTENUATOR
-  ARROW BOARD
-  DASHED TEMPORARY PAVEMENT MARKING LINE

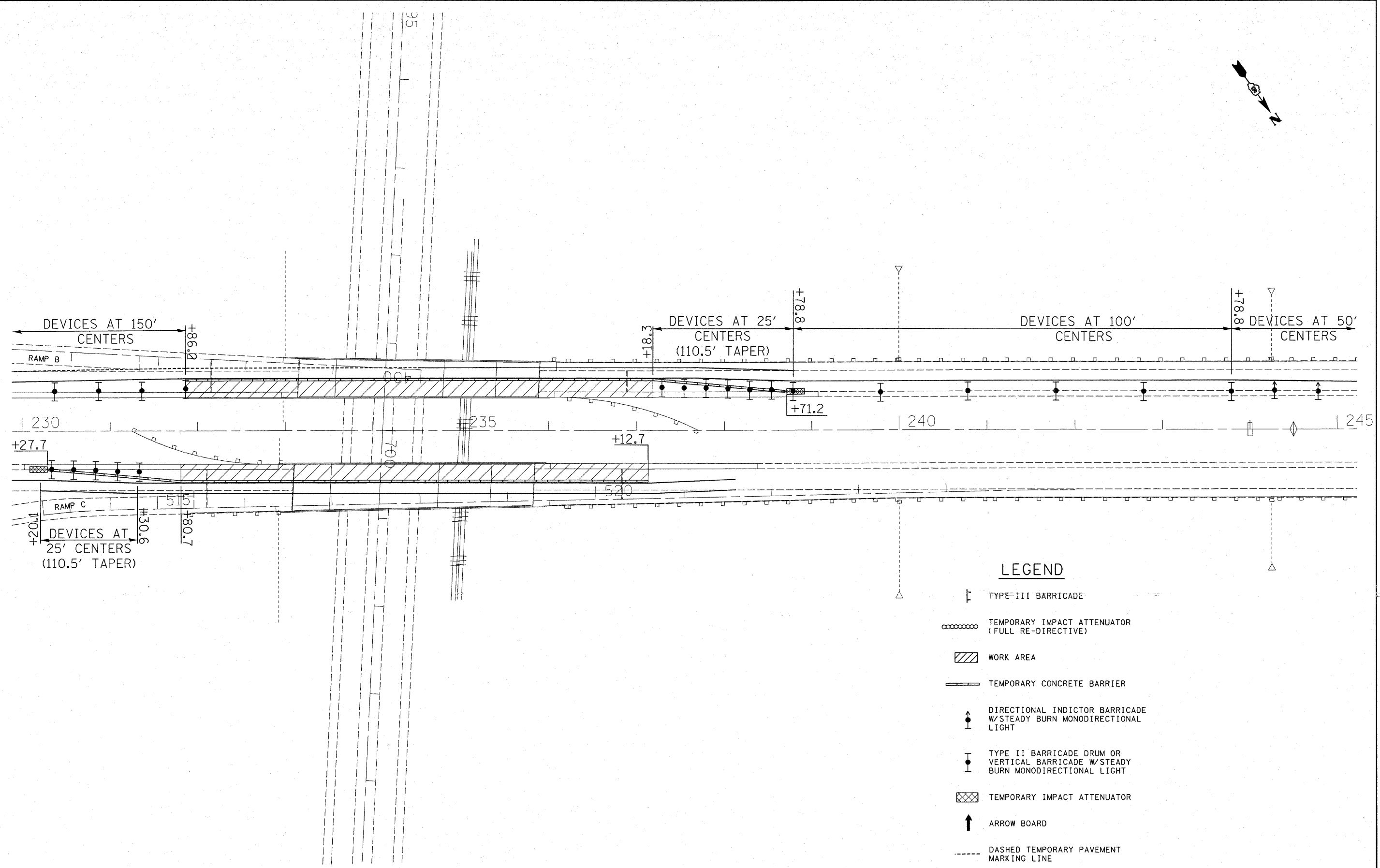
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| FILE NAME = c:\pwwork\p\1001\SWARTZRW\d0138944\d7 | USER NAME = swartzrw 4150-sh1-stage1trafficcontrol.dgn | DESIGNED - DRAWN - | REVISED - REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | STAGE 1 TRAFFIC CONTROL | | | F.A.P. RTE. 322 | SECTION (58-20)RS | COUNTY MACON | TOTAL SHEETS 151 | SHEET NO. 48 |
| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - | | SCALE: 50 | SHEET NO. 3 | OF 6 SHEETS | STA. 140+00 | TO STA. 155+00 | CONTRACT NO. 74150 | | |
| | PLOT DATE = 3/17/2010 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | | | | | | | | |












LEGEND

-  TYPE III BARRICADE
-  TEMPORARY IMPACT ATTENUATOR (FULL RE-DIRECTIVE)
-  WORK AREA
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR BARRICADE W/STEADY BURN MONODIRECTIONAL LIGHT
-  TYPE II BARRICADE DRUM OR VERTICAL BARRICADE W/STEADY BURN MONODIRECTIONAL LIGHT
-  TEMPORARY IMPACT ATTENUATOR
-  ARROW BOARD
-  DASHED TEMPORARY PAVEMENT MARKING LINE

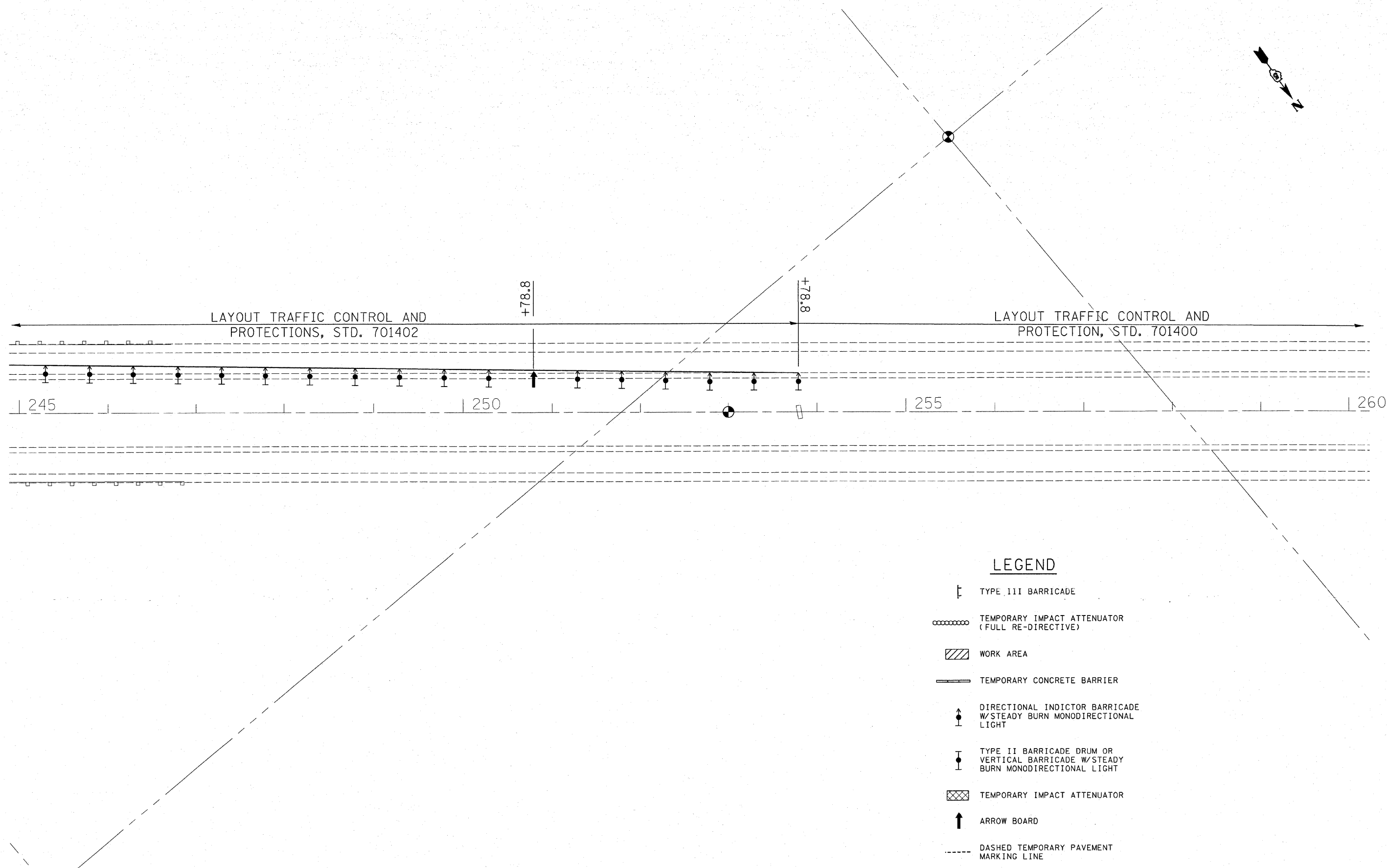
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|---|-------------------------------------|------------|-----------|---|--------------------------------|-------------------------|----------------------------|-------------|-----------|--------|--------------|-----------|
| FILE NAME = | USER NAME = swartzw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | STAGE 1 TRAFFIC CONTROL | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ca:\pw_work\PW\DOT\SWARTZRW\d0138944\d7 | 4150-sh1-stage1-traffic-control.dgn | DRAWN - | REVISED - | | SCALE: 50 | SHEET NO. 4 OF 6 SHEETS | STA. 215+00 TO STA. 230+00 | 322 | (58-20)RS | MACON | 151 | 49 |
| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - | | CONTRACT NO. 74150 | | | | | | | |
| | PLOT DATE = 3/17/2018 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | | | | | | | | |












LEGEND

-  TYPE III BARRICADE
-  TEMPORARY IMPACT ATTENUATOR (FULL RE-DIRECTIVE)
-  WORK AREA
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR BARRICADE W/ STEADY BURN MONODIRECTIONAL LIGHT
-  TYPE II BARRICADE DRUM OR VERTICAL BARRICADE W/ STEADY BURN MONODIRECTIONAL LIGHT
-  TEMPORARY IMPACT ATTENUATOR
-  ARROW BOARD
-  DASHED TEMPORARY PAVEMENT MARKING LINE

| | | | | | | | | | | | | |
|---|---|-----------------------|------------------------|---|--------------------------------|-------------------------|----------------------------|-----------------------|----------------------|-----------------|------------------------|--------------------|
| FILE NAME = c:\pw_work\PW\DOT\SWARTZRW\d0138944\d7 | USER NAME = swartzrw 4150-sh1-stage1trafficcontrol.dgn | DESIGNED - DRAWN - | REVISED - REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | STAGE 1 TRAFFIC CONTROL | | | F.A.P. RTE. 322 | SECTION (58-20)RS | COUNTY MACON | TOTAL SHEETS 151 | SHEET NO. 50 |
| | | | | | SCALE: 50 | SHEET NO. 5 OF 6 SHEETS | STA. 230+00 TO STA. 245+00 | CONTRACT NO. 74150 | | | | |
| | | | | | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | | | | | | | | |












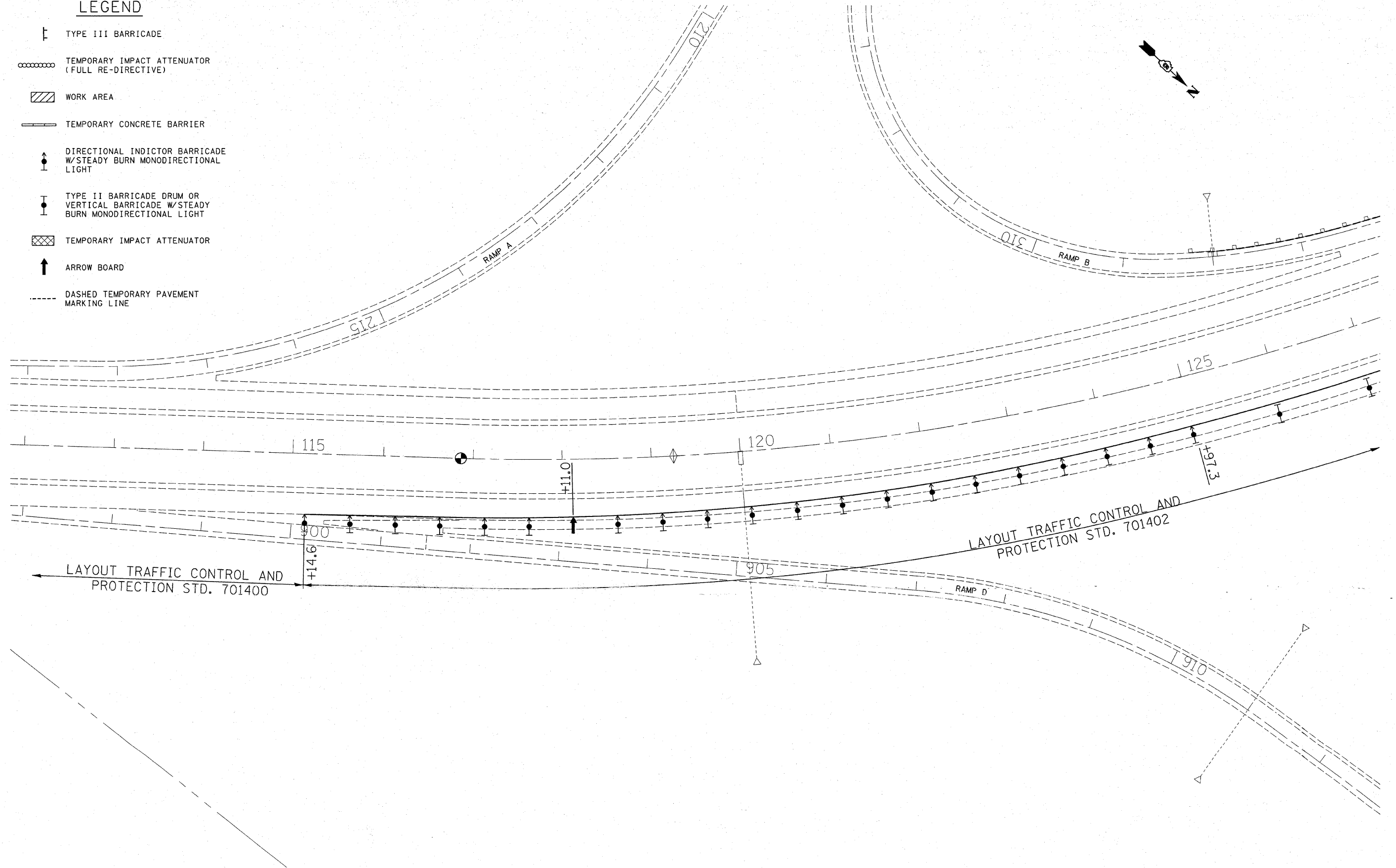
LEGEND

-  TYPE III BARRICADE
-  TEMPORARY IMPACT ATTENUATOR (FULL RE-DIRECTIVE)
-  WORK AREA
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR BARRICADE W/ STEADY BURN MONODIRECTIONAL LIGHT
-  TYPE II BARRICADE DRUM OR VERTICAL BARRICADE W/ STEADY BURN MONODIRECTIONAL LIGHT
-  TEMPORARY IMPACT ATTENUATOR
-  ARROW BOARD
-  DASHED TEMPORARY PAVEMENT MARKING LINE

| | | | | | | | | | | | | |
|--|-----------------------------------|------------|---------------------------|---|--------------------------------|----------------------------|--------------------|-------------|-----------|--------|--------------|-----------|
| FILE NAME = c:\pwork\PW100T\SWARTZRW\d0138944.dgn | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | STAGE 1 TRAFFIC CONTROL | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 4150-sht-stage1trafficcontrol.dgn | DRAWN - | REVISED - | | | | | 322 | (58-20)RS | MACON | 151 | 51 |
| PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - | SCALE: 50 | | SHEET NO. 6 OF 6 SHEETS | STA. 245+00 TO STA. 260+00 | CONTRACT NO. 74150 | | | | | |
| PLOT DATE = 3/17/2010 | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | | | |

LEGEND

-  TYPE III BARRICADE
-  TEMPORARY IMPACT ATTENUATOR (FULL RE-DIRECTIVE)
-  WORK AREA
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR BARRICADE W/STEADY BURN MONODIRECTIONAL LIGHT
-  TYPE II BARRICADE DRUM OR VERTICAL BARRICADE W/STEADY BURN MONODIRECTIONAL LIGHT
-  TEMPORARY IMPACT ATTENUATOR
-  ARROW BOARD
-  DASHED TEMPORARY PAVEMENT MARKING LINE



LAYOUT TRAFFIC CONTROL AND PROTECTION STD. 701400

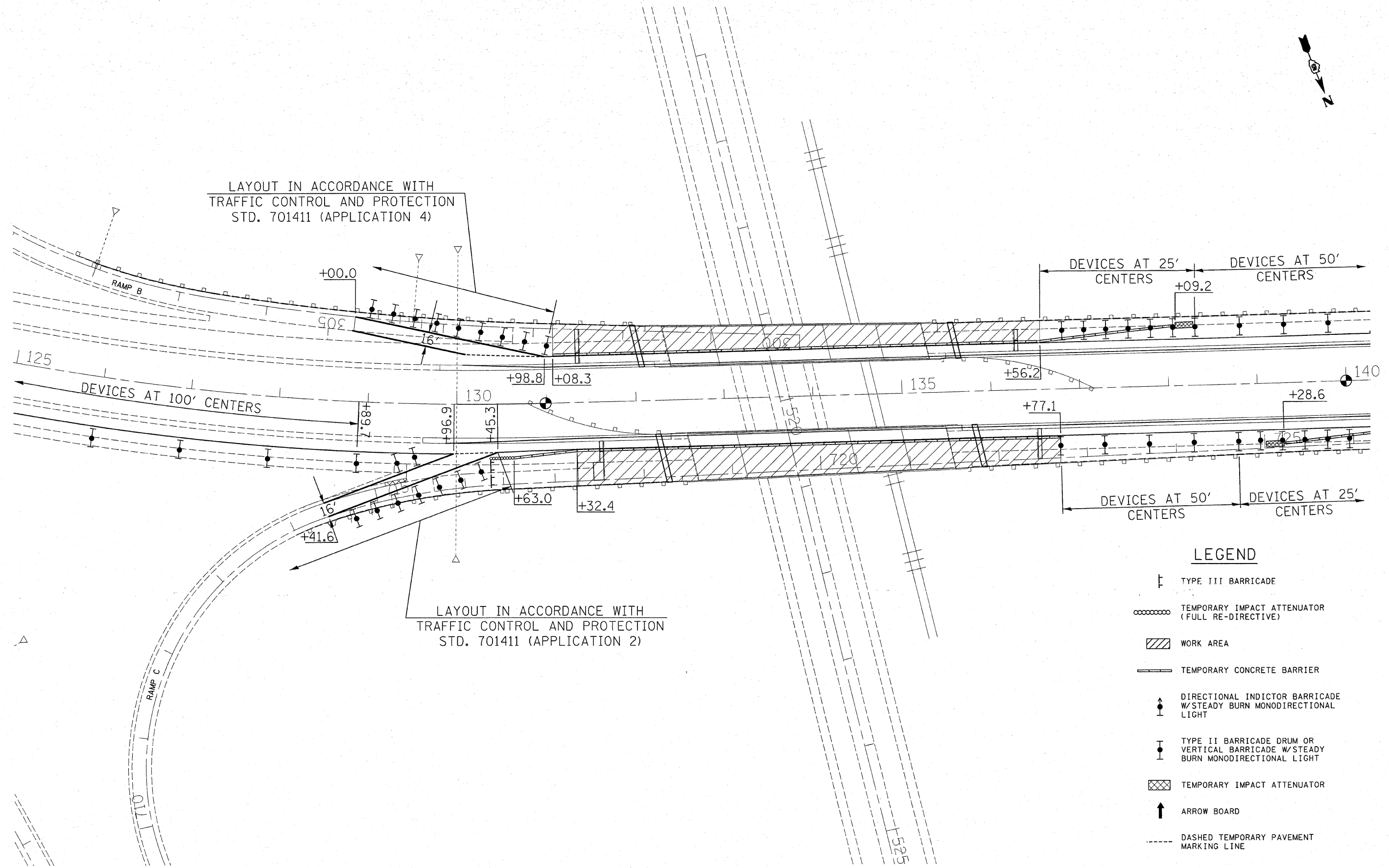
LAYOUT TRAFFIC CONTROL AND PROTECTION STD. 701402

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|---|------------------------------------|------------|-----------|---|--|-----------|-------|--------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | STAGE 2 TRAFFIC CONTROL | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| en:\pw_work\PWIDOT\SWARTZRW\d0138944\d7 | 4150-sh2-stage2trafficocontrol.dgn | DRAWN - | REVISED - | | 322 | (58-20)RS | MACON | 151 | 52 | | | |
| | PLOT SCALE = 50,0000' / 1" IN. | CHECKED - | REVISED - | | SCALE: 50 SHEET NO. 1 OF 6 SHEETS STA. 112+00 TO STA. 127+00 | | | CONTRACT NO. 74150 | | | | |
| | PLOT DATE = 3/17/2018 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |



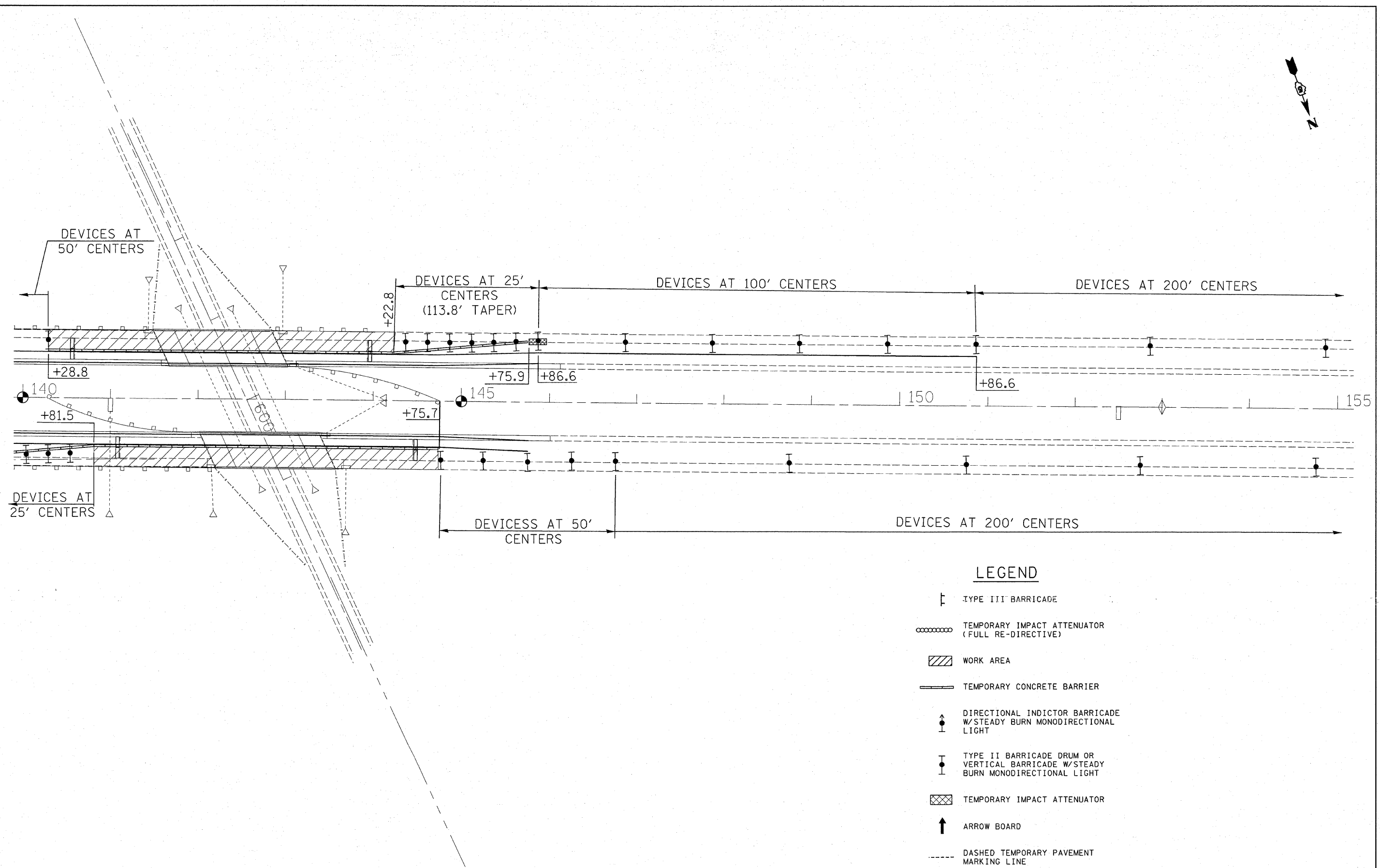
LAYOUT IN ACCORDANCE WITH
TRAFFIC CONTROL AND PROTECTION
STD. 701411 (APPLICATION 4)

LAYOUT IN ACCORDANCE WITH
TRAFFIC CONTROL AND PROTECTION
STD. 701411 (APPLICATION 2)



- LEGEND**
- TYPE III BARRICADE
 - TEMPORARY IMPACT ATTENUATOR (FULL RE-DIRECTIVE)
 - WORK AREA
 - TEMPORARY CONCRETE BARRIER
 - DIRECTIONAL INDICATOR BARRICADE W/ STEADY BURN MONODIRECTIONAL LIGHT
 - TYPE II BARRICADE DRUM OR VERTICAL BARRICADE W/ STEADY BURN MONODIRECTIONAL LIGHT
 - TEMPORARY IMPACT ATTENUATOR
 - ARROW BOARD
 - DASHED TEMPORARY PAVEMENT MARKING LINE

| | | | | | | | | | | | | |
|---|-----------------------------------|------------|-----------|---|--------------------------------|-------------------------|-------------|----------------|--------------------|--------|--------------|-----------|
| FILE NAME = c:\pw\work\PWIDOT\SWARTZRW\d0138944\d7 | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | STAGE 2 TRAFFIC CONTROL | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 4150-sht-stage2trafficcontrol.dgn | DRAWN - | REVISED - | | | | | 322 | (58-20)RS | MACON | 151 | 53 |
| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - | | SCALE: 50 | SHEET NO. 2 OF 6 SHEETS | STA. 125+00 | TO STA. 140+00 | CONTRACT NO. 74150 | | | |
| | PLOT DATE = 3/17/2010 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |



LEGEND

- ⌈ TYPE III BARRICADE
- ⊖ TEMPORARY IMPACT ATTENUATOR (FULL RE-DIRECTIVE)
- ▨ WORK AREA
- TEMPORARY CONCRETE BARRIER
- ↑ DIRECTIONAL INDICATOR BARRICADE W/STEADY BURN MONODIRECTIONAL LIGHT
- ⊙ TYPE II BARRICADE DRUM OR VERTICAL BARRICADE W/STEADY BURN MONODIRECTIONAL LIGHT
- ⊗ TEMPORARY IMPACT ATTENUATOR
- ↑ ARROW BOARD
- DASHED TEMPORARY PAVEMENT MARKING LINE

| | | | |
|--------------------------------------|-----------------------------------|------------|-----------|
| FILE NAME = | USER NAME = swartzrw | DESIGNED - | REVISED - |
| c:\pwork\PWIDOT\SWARTZRW\d0138944\d7 | 4158-sh2-stage2trafficcontrol.dgn | DRAWN - | REVISED - |
| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - |
| | PLOT DATE = 3/17/2018 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE 2 TRAFFIC CONTROL

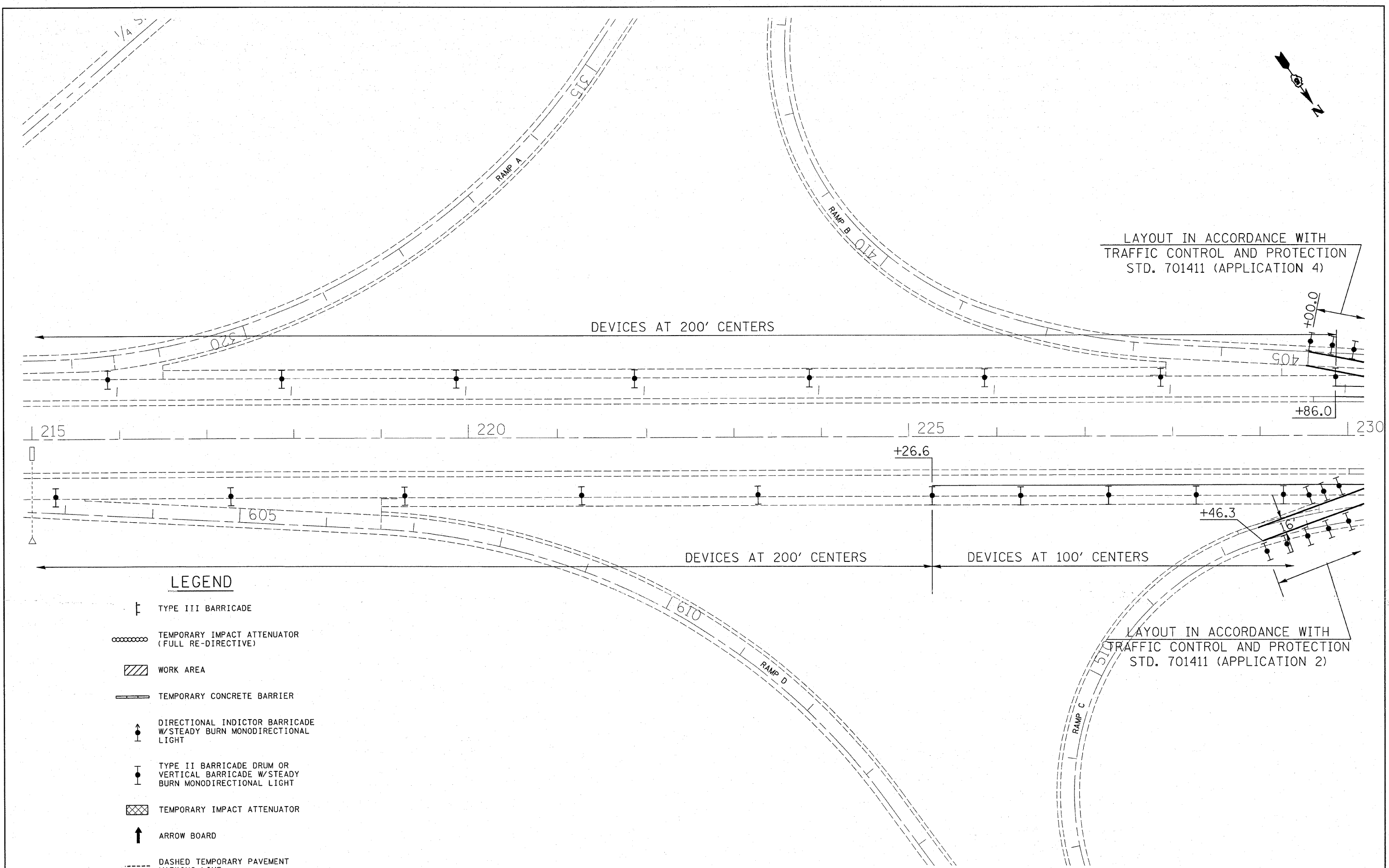
SCALE: 50 SHEET NO. 3 OF 6 SHEETS STA. 140+00 TO STA. 155+00

| | | | | |
|--------------------|-----------|--------|---------------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 322 | (58-20)RS | MACON | 151 | 54 |
| CONTRACT NO. 74150 | | | ILLINOIS FED. AID PROJECT | |












LAYOUT IN ACCORDANCE WITH
TRAFFIC CONTROL AND PROTECTION
STD. 701411 (APPLICATION 4)

LAYOUT IN ACCORDANCE WITH
TRAFFIC CONTROL AND PROTECTION
STD. 701411 (APPLICATION 2)



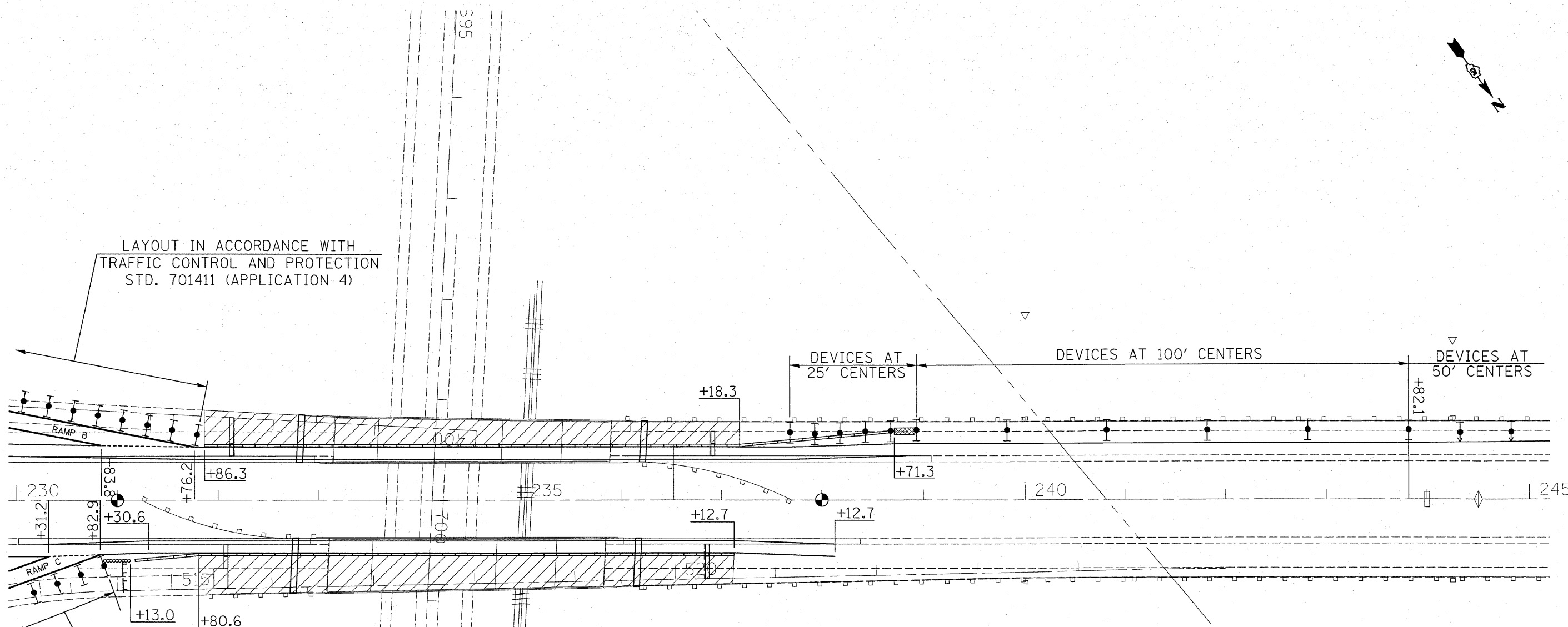
LEGEND

-  TYPE III BARRICADE
-  TEMPORARY IMPACT ATTENUATOR (FULL RE-DIRECTIVE)
-  WORK AREA
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR BARRICADE W/ STEADY BURN MONODIRECTIONAL LIGHT
-  TYPE II BARRICADE DRUM OR VERTICAL BARRICADE W/ STEADY BURN MONODIRECTIONAL LIGHT
-  TEMPORARY IMPACT ATTENUATOR
-  ARROW BOARD
-  DASHED TEMPORARY PAVEMENT MARKING LINE

| | | | | | | | | | | | | |
|---|---------------------------------|------------|-----------|---|--------------------------------|-------------------------|----------------------------|---------------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | STAGE 2 TRAFFIC CONTROL | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ct:\pw_work\PWIDDT\SWARTZRW\d0138944\d7 | 4150-sh2-stage2traffcontrol.dgn | DRAWN - | REVISED - | | 322 | (58-20)RS | MACON | 151 | 55 | | | |
| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - | | CONTRACT NO. 74150 | | | ILLINOIS FED. AID PROJECT | | | | |
| | PLOT DATE = 3/17/2010 | DATE - | REVISED - | | SCALE: 50 | SHEET NO. 4 OF 6 SHEETS | STA. 215+00 TO STA. 230+00 | | | | | |
| | | | | | | | | | | | | |












LAYOUT IN ACCORDANCE WITH
TRAFFIC CONTROL AND PROTECTION
STD. 701411 (APPLICATION 4)



LAYOUT IN ACCORDANCE WITH
TRAFFIC CONTROL AND PROTECTION
STD. 701411 (APPLICATION 2)

LEGEND

-  TYPE III BARRICADE
-  TEMPORARY IMPACT ATTENUATOR (FULL RE-DIRECTIVE)
-  WORK AREA
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR BARRICADE W/ STEADY BURN MONODIRECTIONAL LIGHT
-  TYPE II BARRICADE DRUM OR VERTICAL BARRICADE W/ STEADY BURN MONODIRECTIONAL LIGHT
-  TEMPORARY IMPACT ATTENUATOR
-  ARROW BOARD
-  DASHED TEMPORARY PAVEMENT MARKING LINE

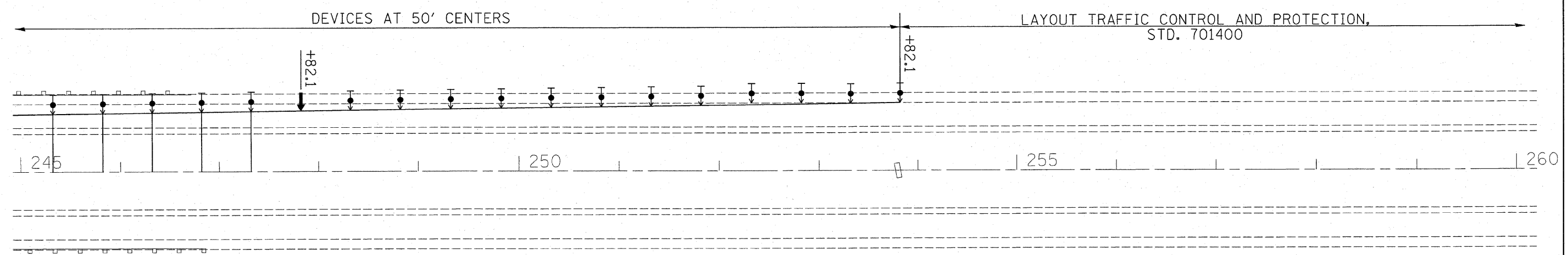
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| FILE NAME = | USER NAME = swartzw | DESIGNED - | REVISED - |
| c:\pwork\PWIDOT\SWARTZRW\d0138944\d7 | 4150-sh2-stage2traffcontrol.dgn | DRAWN - | REVISED - |
| | PLOT SCALE = 50.0000 ' / IN. | CHECKED - | REVISED - |
| | PLOT DATE = 3/17/2018 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**










STAGE 2 TRAFFIC CONTROL

SCALE: 50 SHEET NO. 5 OF 6 SHEETS STA. 230+00 TO STA. 245+00

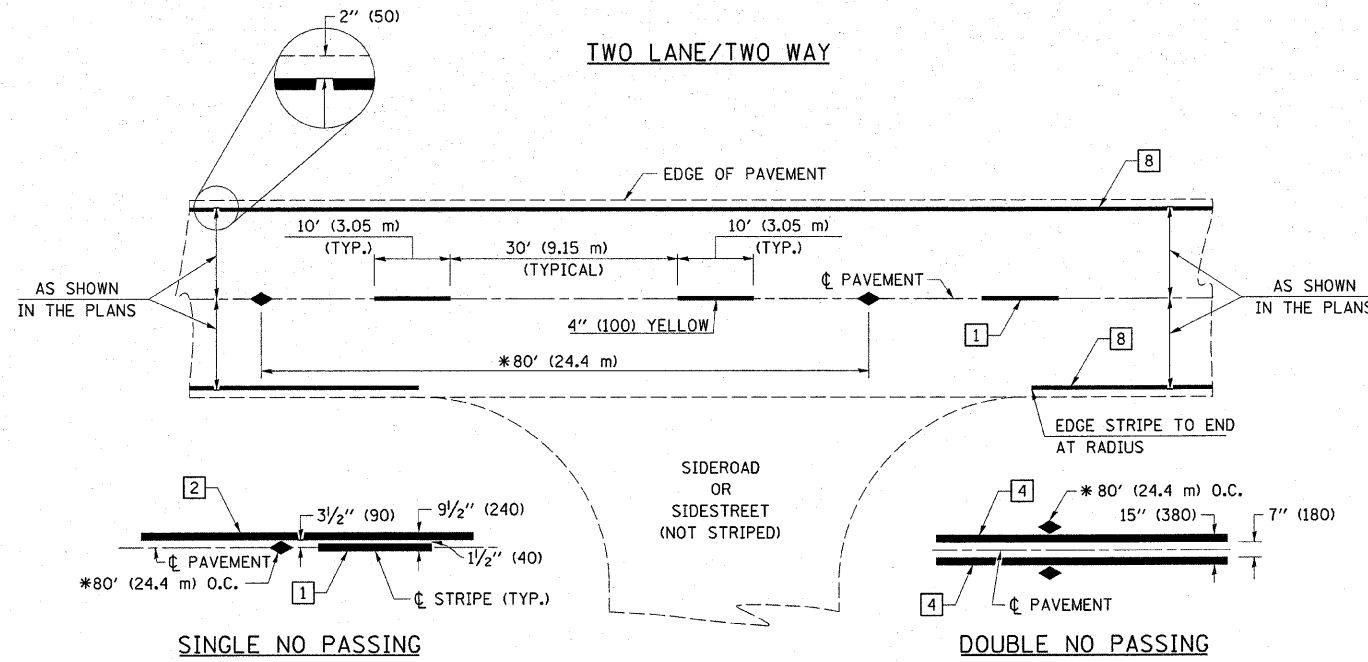
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|--------|--------------------|-----------|
| 322 | (58-20)RS | MACON | 151 | 56 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 74150 | |



LEGEND

-  TYPE III BARRICADE
-  TEMPORARY IMPACT ATTENUATOR (FULL RE-DIRECTIVE)
-  WORK AREA
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR BARRICADE W/STEADY BURN MONODIRECTIONAL LIGHT
-  TYPE II BARRICADE DRUM OR VERTICAL BARRICADE W/STEADY BURN MONODIRECTIONAL LIGHT
-  TEMPORARY IMPACT ATTENUATOR
-  ARROW BOARD
-  DASHED TEMPORARY PAVEMENT MARKING LINE

| | | | | | | | | | | | | |
|---|-------------------------------------|------------|--|---|--------------------------------|--|--|--------------------|-----------|--------|--------------|-----------|
| FILE NAME = c:\pwork\PWIDOT\SWARTZRW\d0138944\d7 | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | STAGE 2 TRAFFIC CONTROL | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 4150-shrt-stage-traffic-control.dgn | DRAWN - | REVISED - | | | | | 322 | (58-20)RS | MACON | 151 | 57 |
| PLOT SCALE = 50.0000 "/ IN. | CHECKED - | REVISED - | SCALE: 50 SHEET NO. 6 OF 6 SHEETS STA. 245+00 TO STA. 260+00 | | | | | CONTRACT NO. 74150 | | | | |
| PLOT DATE = 3/17/2018 | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | | | |



* REDUCE TO 40' (12.2 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEEDS OF 45 mph (70 km/h) OR LESS.

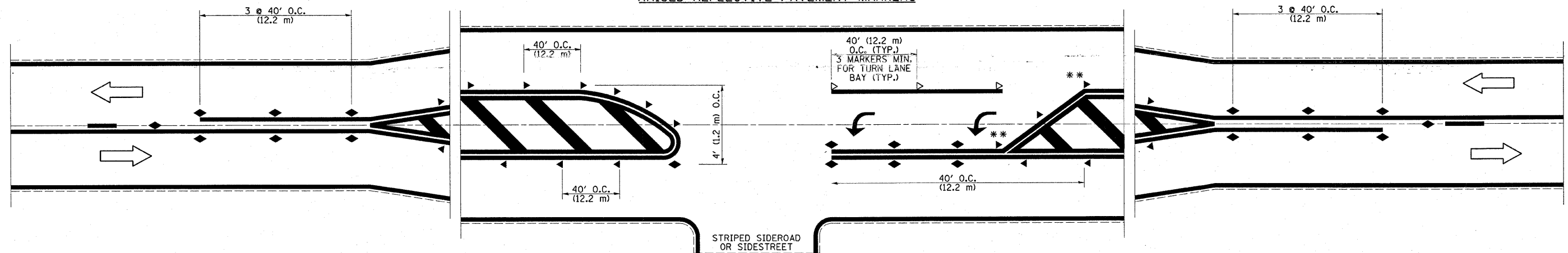
PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 6" (100) LANE LINE EXTENSIONS (WHITE)
- 14 4" (100) PARKING WHITE

TYPICAL PAVEMENT MARKERS LEGEND

- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

RAISED REFLECTIVE PAVEMENT MARKERS

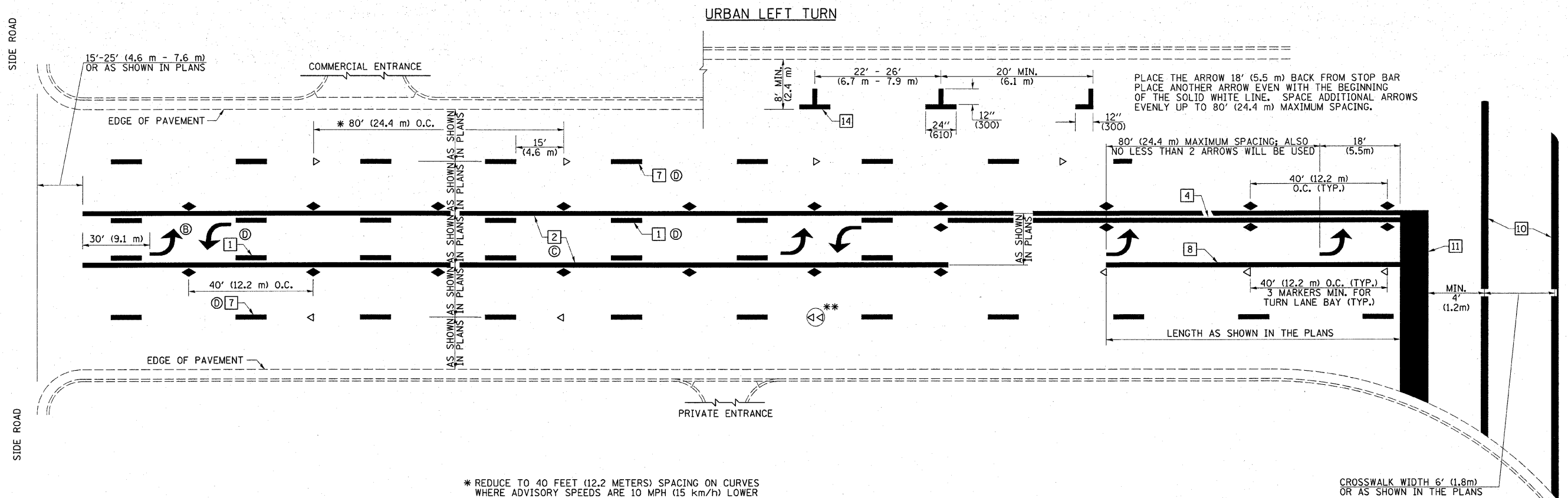


** REDUCE SPACING IF NECESSARY TO ASSURE MARKERS AT CORNER POINTS.

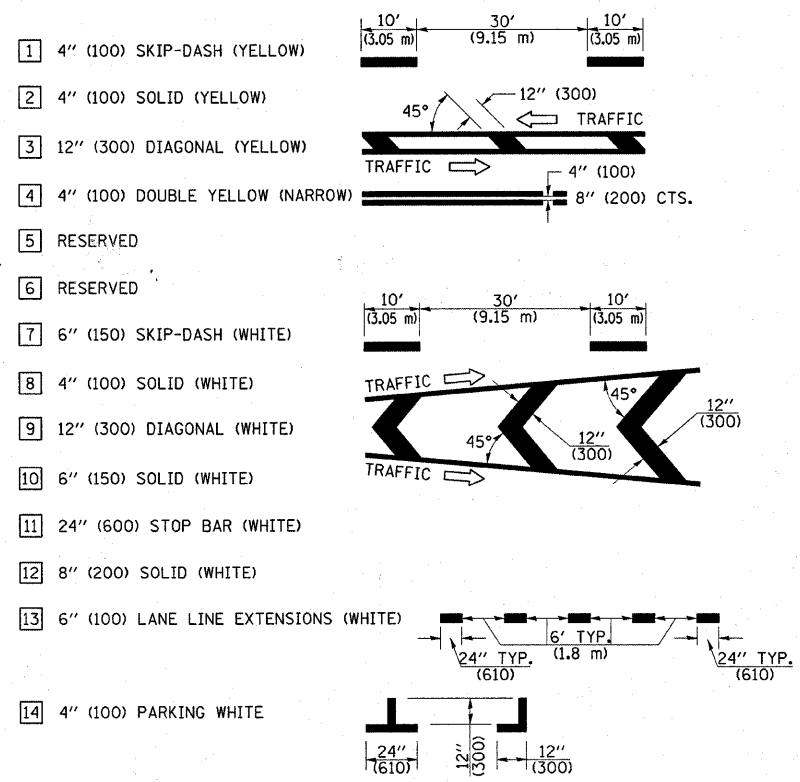
NOT TO SCALE
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 78000001

| | | | | | | | | | | |
|--|----------------------|------------|-----------|---|---|---------------------------|-----------|--------|--------------|-----------|
| FILE NAME = | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| as\pw_work\pwidot\swartzrw\0138928\077450-detais.dgn | | DRAWN - | REVISED - | | | 322 | 158-201RS | MACON | 151 | 58 |
| PLOT SCALE = 20,0000' / IN. | | CHECKED - | REVISED - | | | CONTRACT NO. 74150 | | | | |
| PLOT DATE = 3/17/2018 | | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | |
| | | | | | SCALE: NA | SHEET NO. 1 OF 4 SHEETS | | STA. | TO STA. | |



PAVEMENT MARKING LEGEND

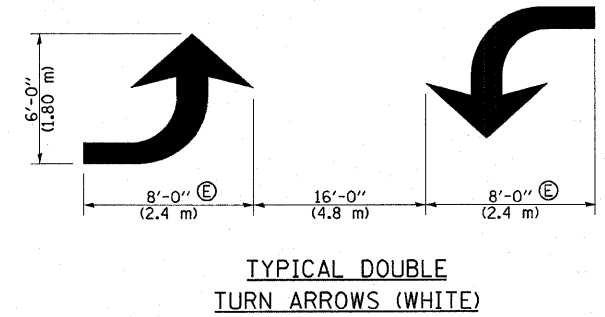
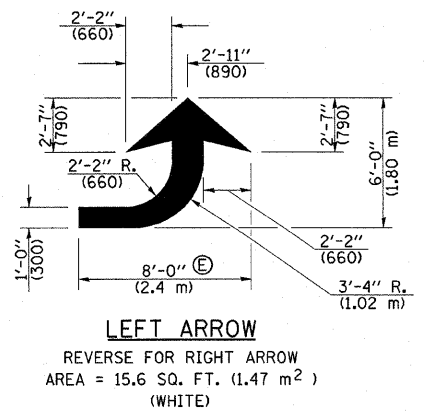
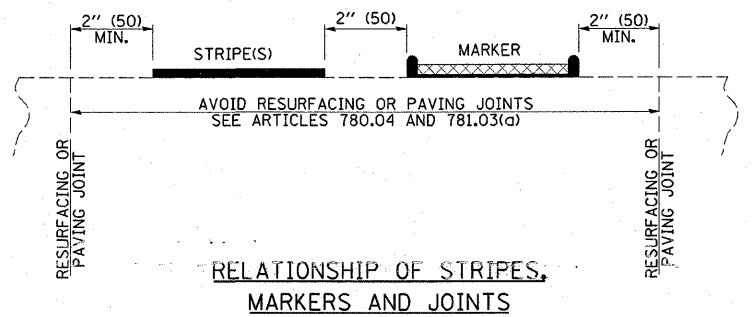


* REDUCE TO 40 FEET (12.2 METERS) SPACING ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER THAN POSTED SPEEDS.

** DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED AND SPACED AS SHOWN IN HIGHWAY STANDARD 781001 FOR MULTI-LANE DIVIDED AND UNDIVIDED HIGHWAYS.

GENERAL NOTES

- ⓑ TURN ARROW PAIRS SHALL BE PLACED AT 250' (75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE. USE A MINIMUM OF TWO PAIRS PER BLOCK.
- ⓒ THE SOLID YELLOW PAVEMENT MARKINGS [2] SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
- ⓓ THE SKIP-DASH PAVEMENT MARKINGS [1] OR [7] SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER.
- ⓔ USE LARGE ARROW SIZE FOR BOTH RURAL AND URBAN LOCATIONS. (SEE LAST PAGE OF SECTION 780x FOR SYMBOLS TABLE)



NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 7800001

| | | | |
|---|---------------------|------------|-----------|
| FILE NAME = | USER NAME = swartzw | DESIGNED - | REVISED - |
| c:\pwwork\pwwork\swartzw\d0138928\0774150-details.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 28.80000 / IN. | | CHECKED - | REVISED - |
| PLOT DATE = 3/17/2018 | | DATE - | REVISED - |

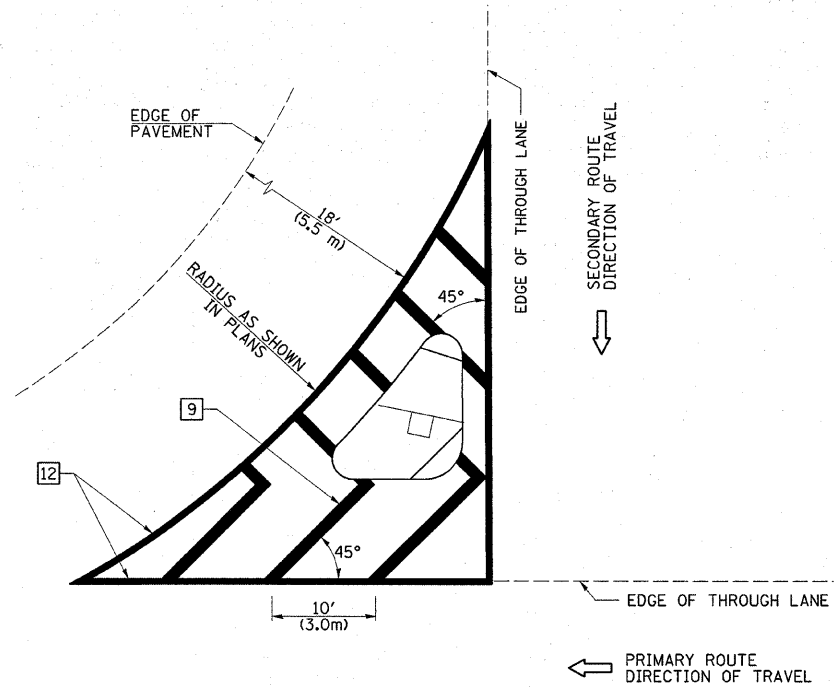
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS
(RURAL & URBAN APPLICATIONS)**

SCALE: NA SHEET NO. 2 OF 4 SHEETS STA. TO STA.

| | | | | |
|---------------------------|-------------------|--------------|------------------|--------------------|
| F.A.P. RTE. 322 | SECTION (58-20)RS | COUNTY MACON | TOTAL SHEETS 151 | SHEET NO. 59 |
| | | | | CONTRACT NO. 74150 |
| ILLINOIS FED. AID PROJECT | | | | |

ISLAND

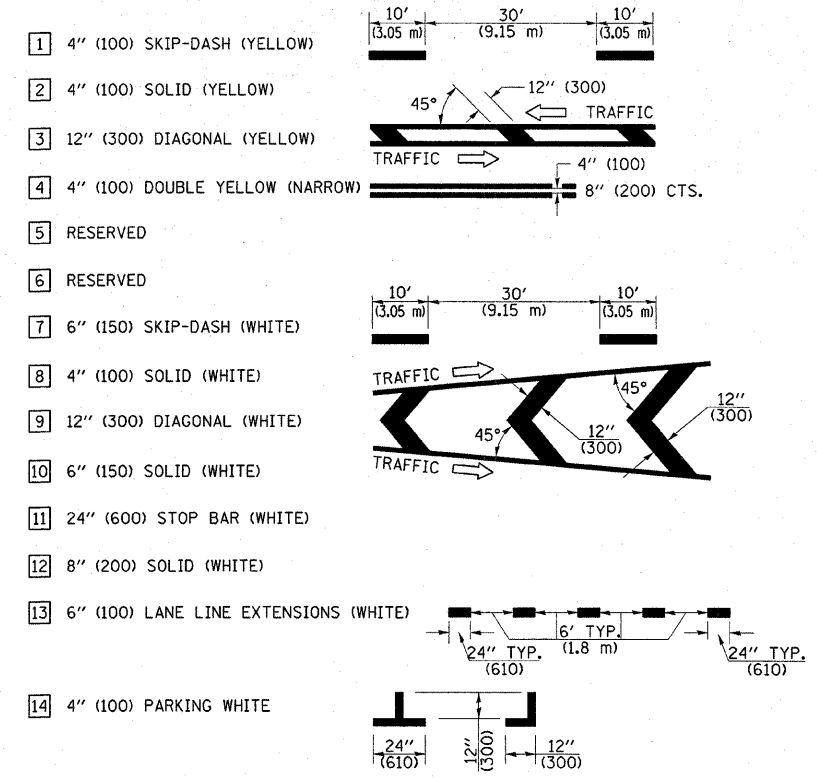


GENERAL NOTES

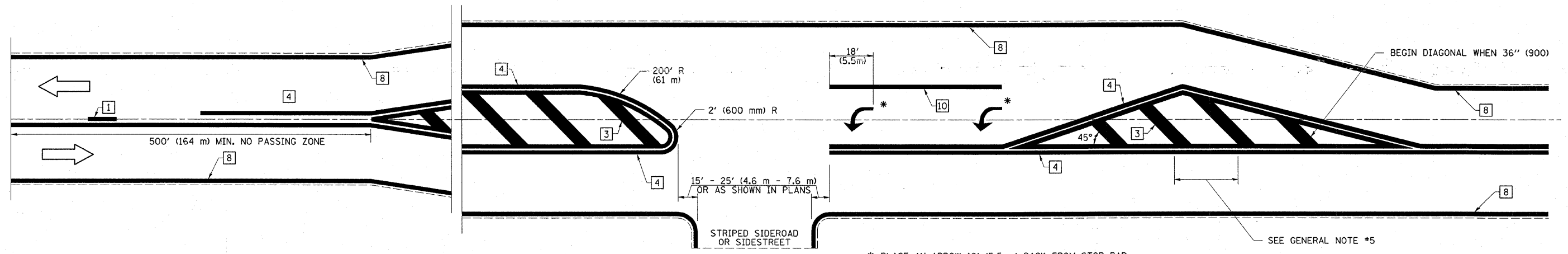
1. RAISED AND CORRUGATED MEDIANS SHALL BE OUTLINED WITH [2] IF PRESENT.
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
5. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING:

| | |
|------------------------|-------------|
| < 30 MPH (< 50 km/h) | 15' (4.5 m) |
| 30-45 MPH (50-75 km/h) | 20' (6.0 m) |
| > 45 MPH (> 75 km/h) | 30' (9.0 m) |

PAVEMENT MARKING LEGEND



RURAL LEFT TURN STRIPING



* PLACE AN ARROW 18' (5.5 m) BACK FROM STOP BAR. PLACE ANOTHER ARROW EVEN WITH THE BEGINNING OF THE SOLID WHITE LINE. SPACE ADDITIONAL ARROWS EVENLY UP TO 80' (24.4 m) MAXIMUM SPACING. USE MINIMUM OF 2 ARROWS.

SEE GENERAL NOTE #5

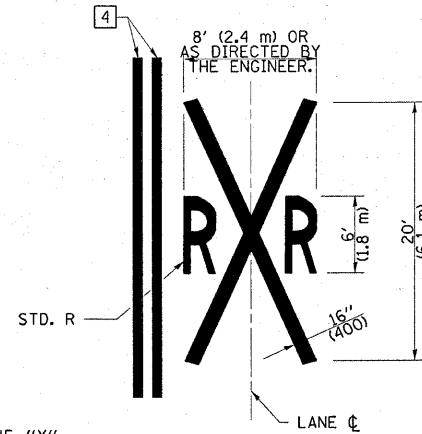
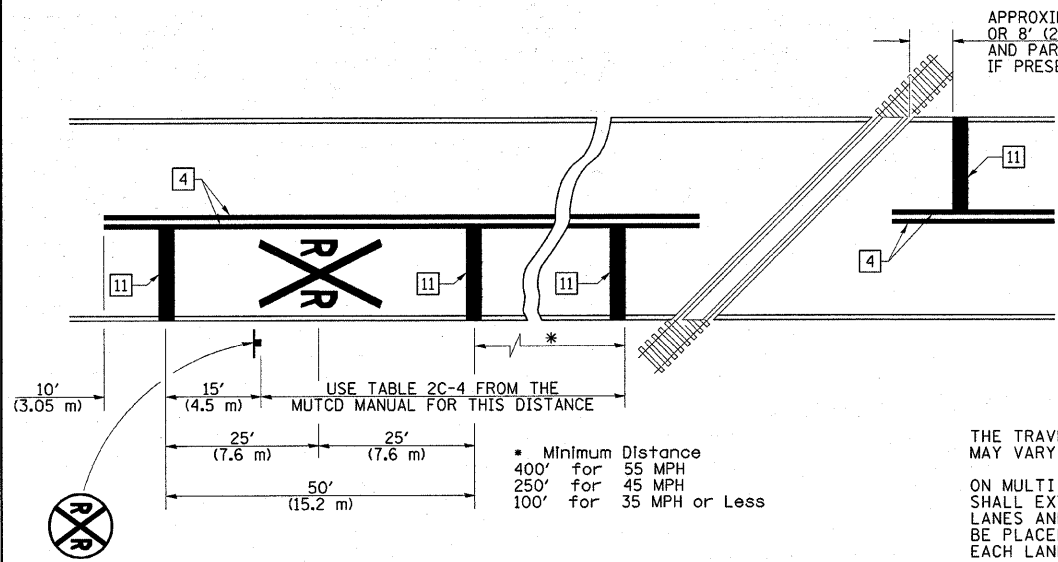
NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

| | | | | | | | | | | |
|--|---------------------|------------|---------------------------|---|---|-------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = swartzw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| es:\pv_work\pwi\dot\swartzw\10138928\0774150-detai1s.dgn | DRAWN - | REVISED - | 322 | | | (58-20)RS | MACON | 151 | 60 | |
| PLOT SCALE = 20.0000 / IN. | CHECKED - | REVISED - | CONTRACT NO. 74150 | | | | | | | |
| PLOT DATE = 3/17/2013 | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |

PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING

PAVEMENT MARKING LEGEND



NOTES

THE TRAVERSE SPREAD OF THE "X" MAY VARY ACCORDING TO LANE WIDTH.

ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE RXR SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE II-1, CONDITION B OF THE MUTCD.

1 4" (100) SKIP-DASH (YELLOW)

2 4" (100) SOLID (YELLOW)

3 12" (300) DIAGONAL (YELLOW)

4 4" (100) DOUBLE YELLOW (NARROW)

5 RESERVED

6 RESERVED

7 6" (150) SKIP-DASH (WHITE)

8 4" (100) SOLID (WHITE)

9 12" (300) DIAGONAL (WHITE)

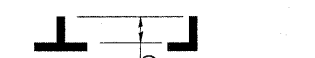
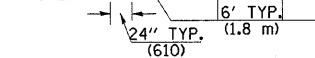
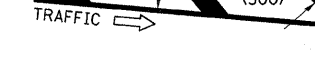
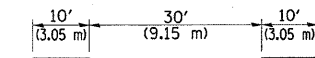
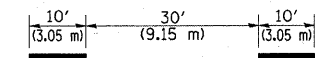
10 6" (150) SOLID (WHITE)

11 24" (600) STOP BAR (WHITE)

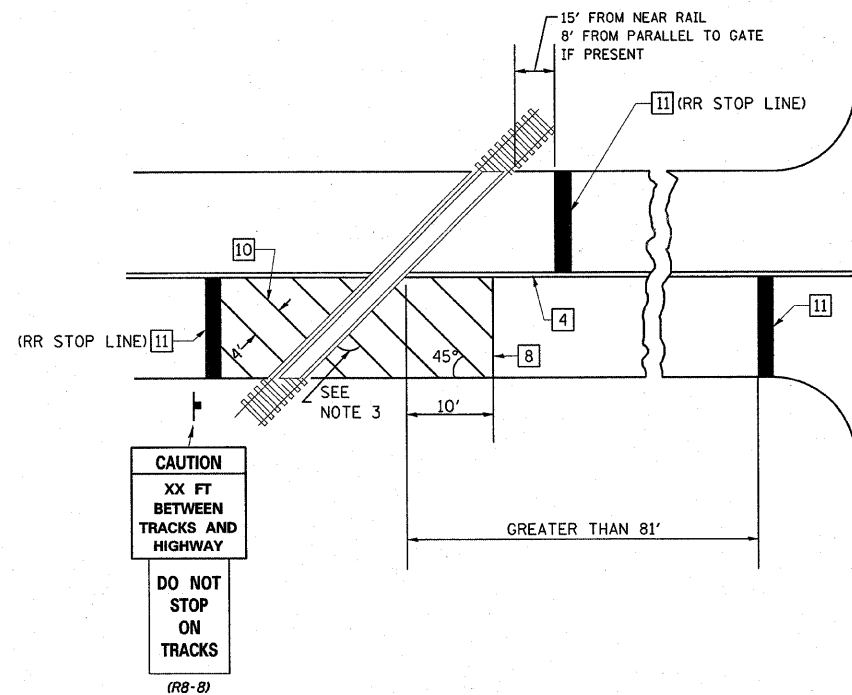
12 8" (200) SOLID (WHITE)

13 6" (100) LANE LINE EXTENSIONS (WHITE)

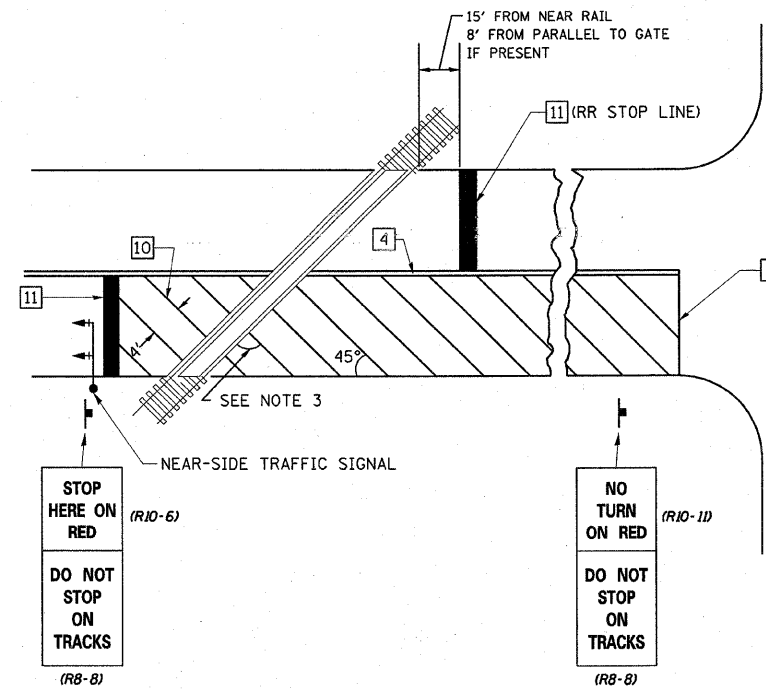
14 4" (100) PARKING WHITE



RAILROAD CROSSING WITH INTERCONNECT ONLY



RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS



GENERAL NOTES

- SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED.
- WHERE THE ANGLE BETWEEN THE DIAGONAL PAVEMENT MARKINGS AND THE TRACK WOULD BE LESS THAN 20°, THE PAVEMENT MARKINGS SHOULD BE PLACED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.

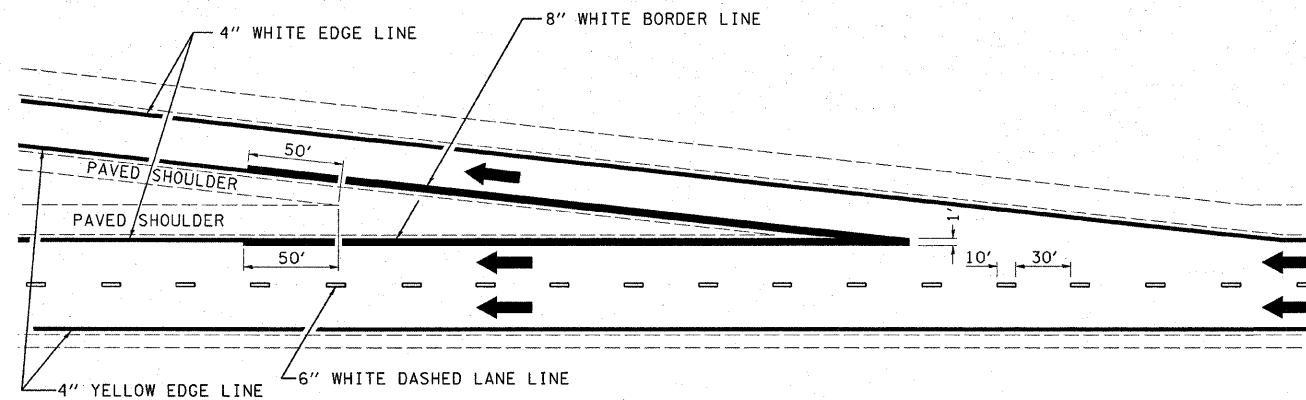
SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING

NOT TO SCALE

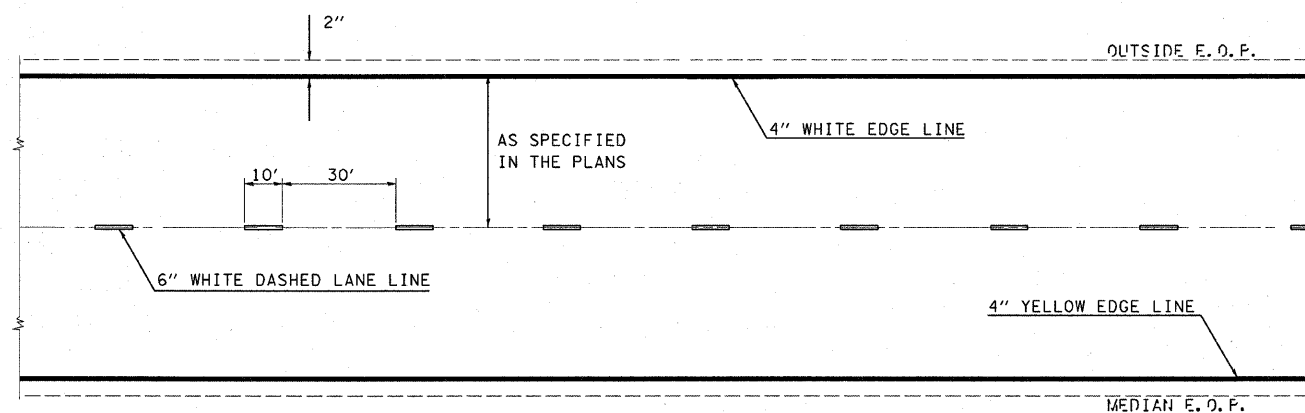
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 7800001

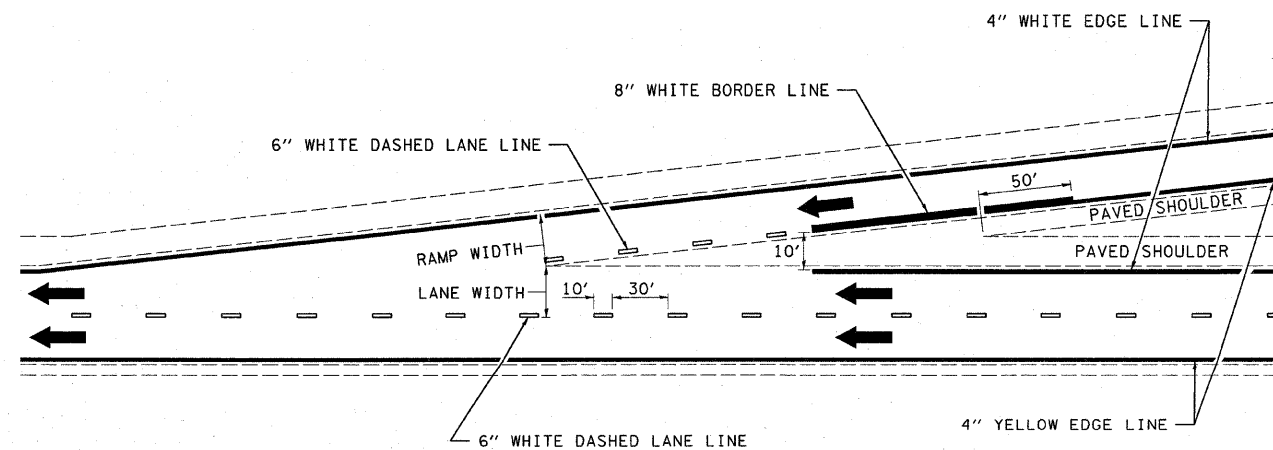
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|--|----------------------|------------|-----------|---|---|---------------------------|-----------|--------|--------------|-----------|
| FILE NAME = | USER NAME = swartzrw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pw\work\pwwork\swartzrw\10138928\0774150-deta11.dgn | | DRAWN - | REVISED - | | | 322 | (58-20)RS | MACON | 151 | 61 |
| PLOT SCALE = 28,00000' / IN. | | CHECKED - | REVISED - | | | CONTRACT NO. 74150 | | | | |
| PLOT DATE = 3/17/2018 | | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | |
| | | | | | SCALE: NA | SHEET NO. 4 OF 4 SHEETS | | STA. | TO STA. | |



TYPICAL EXIT RAMP MARKING



TYPICAL CENTERLINE & EDGELINE MARKINGS

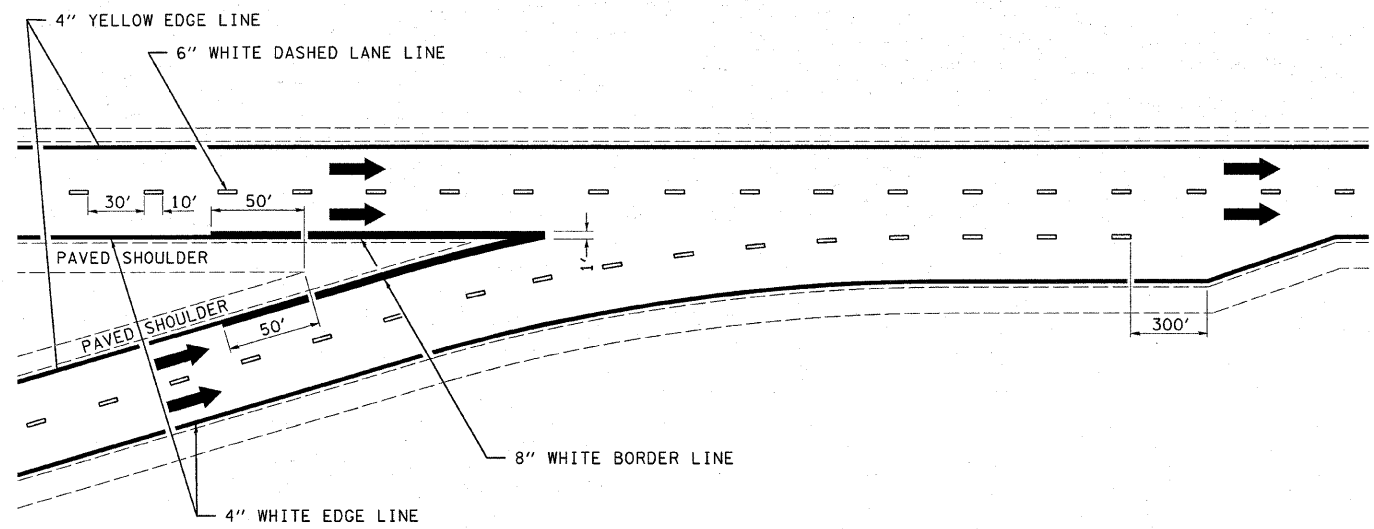


TYPICAL ENTRANCE RAMP MARKING

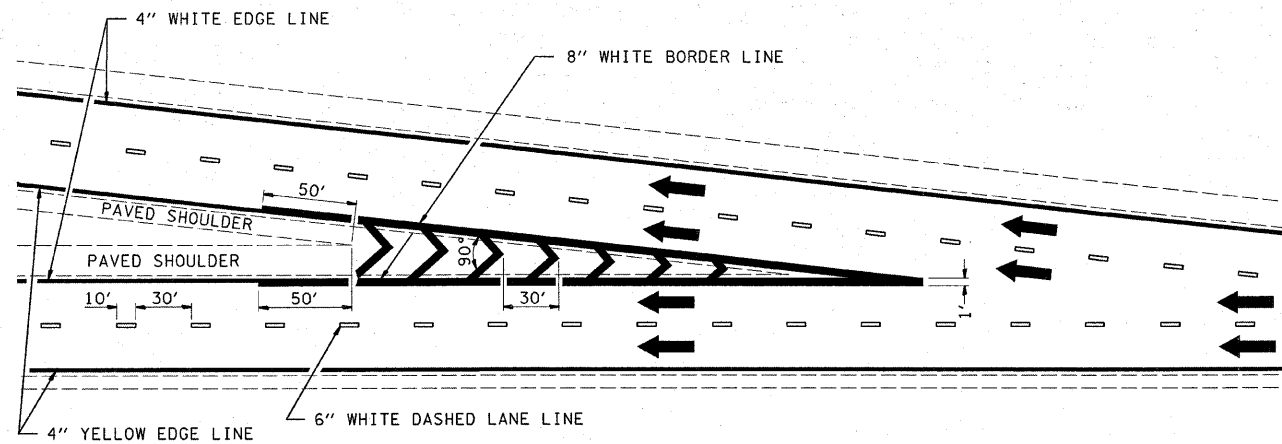
NOT TO SCALE

DISTRICT 7 DETAIL NO. 7800002

| | | | | | | | | | | | | |
|--|---------------------|------------|---------------------|---|--|--|--|---------------------------|-----------|--------|--------------|--------------------|
| FILE NAME = | USER NAME = swartzw | DESIGNED - | REVISED - MMO 12-99 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL APPLICATIONS OF INTERSTATE PAVEMENT MARKING | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwwork\pwwork\swartzw\d8138928\077458-details.dgn | | DRAWN - | REVISED - DRM 08-04 | | | | | 322 | (58-20)RS | MACON | 151 | 62 |
| PLOT SCALE = 20.0000' / IN. | | CHECKED - | REVISED - MKS 04-08 | | SCALE: NA | | | SHEET NO. 1 OF 2 SHEETS | | STA. | TO STA. | CONTRACT NO. 74150 |
| PLOT DATE = 3/17/2010 | | DATE - | REVISED - DRM 01-09 | | FED. ROAD DIST. NO. | | | ILLINOIS FED. AID PROJECT | | | | |

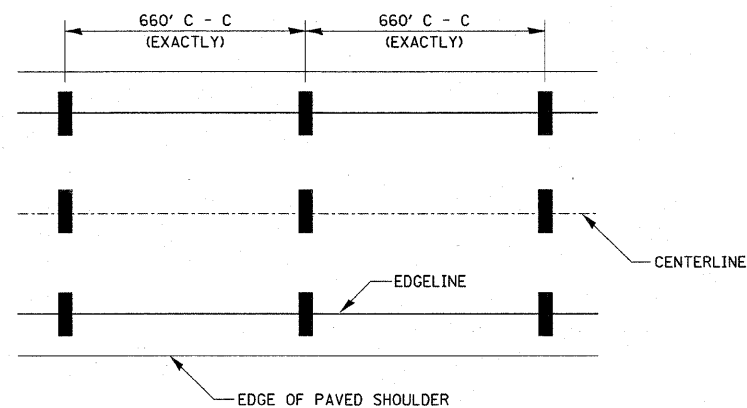


TYPICAL CONVERGENCE MARKING

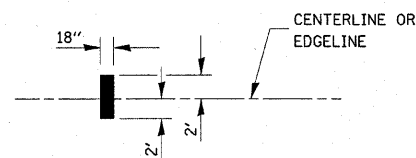


TYPICAL DIVERGENCE MARKING

AERIAL SPEED CHECK ZONES



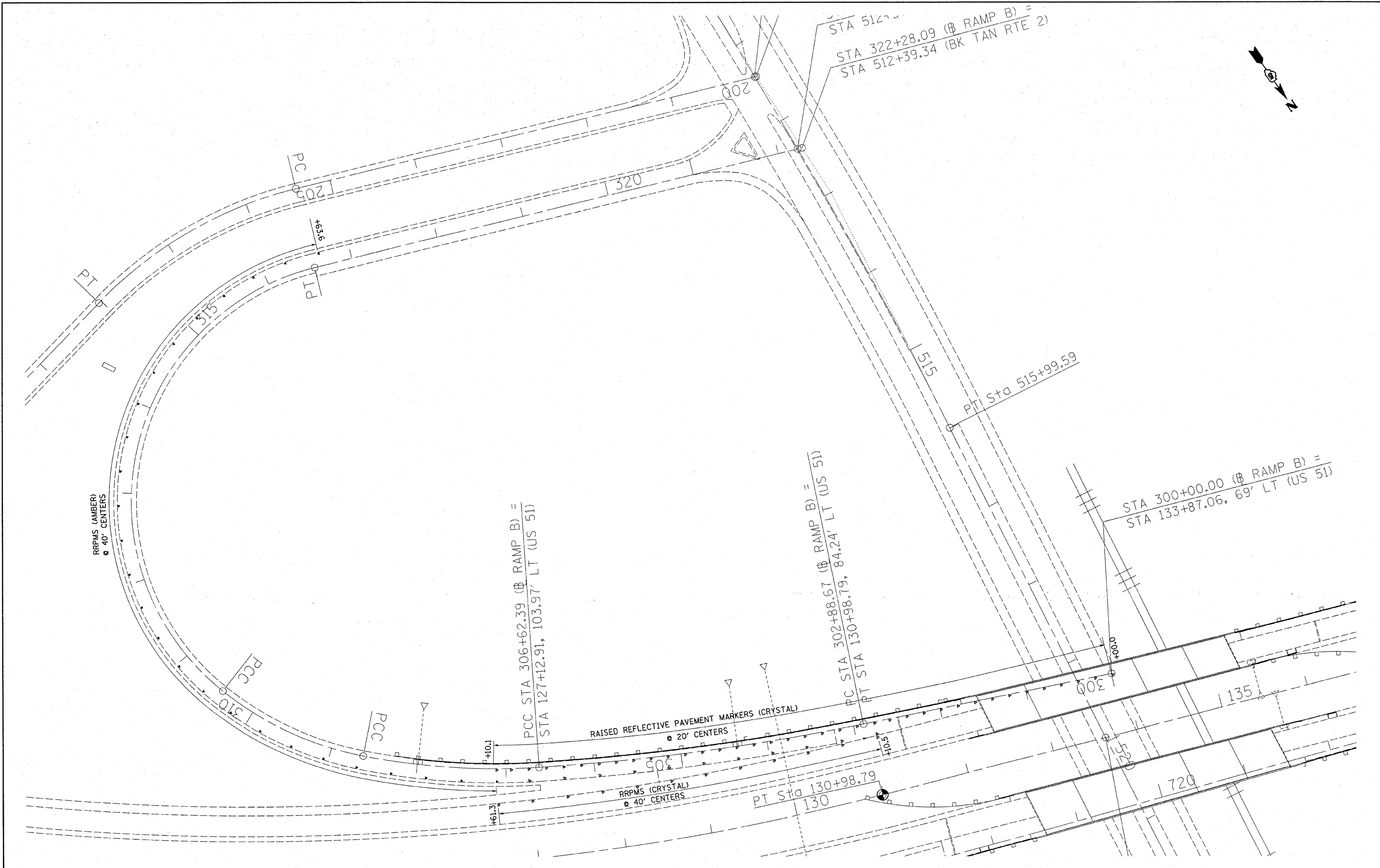
IT WILL BE NECESSARY TO HAVE A REPRESENTATIVE OF THE STATE POLICE PRESENT SO THAT THE ACCURACY OF MEASUREMENT CAN BE ATTESTED TO IN COURT.



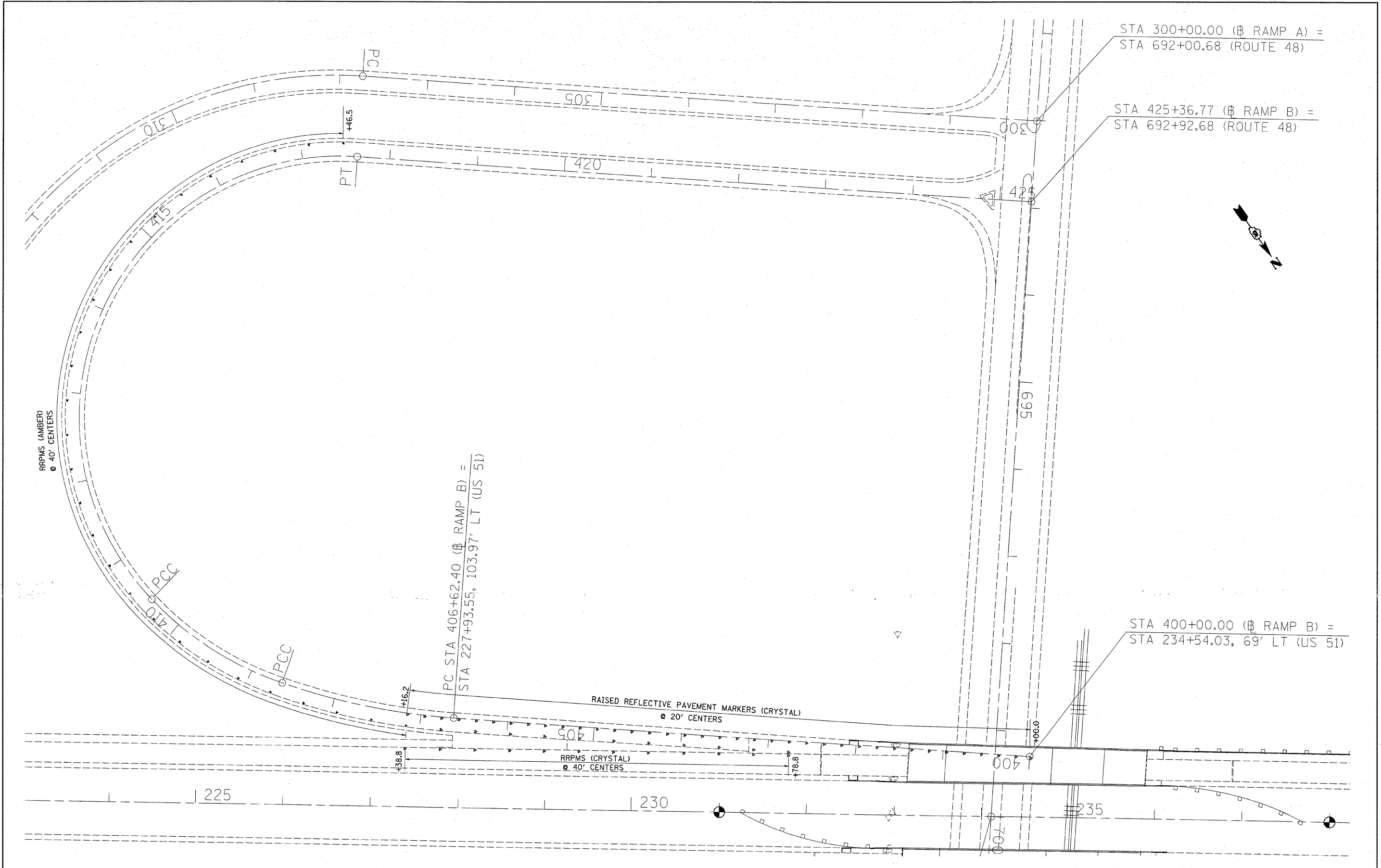
NOT TO SCALE

DISTRICT 7 DETAIL NO. 7800002

| | | | | | | | | | | | | | |
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| FILE NAME = | USER NAME = swartzw | DESIGNED - | REVISED - MMD 12-99 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL APPLICATIONS OF INTERSTATE PAVEMENT MARKING | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ca:\pv_work\pwsdot\swartzw\10138928\077150-details.dgn | | DRAWN - | REVISED - DRM 08-04 | | SCALE: NA | SHEET NO. 2 OF 2 SHEETS | STA. | TO STA. | 322 | (58-20)RS | MACON | 151 | 63 |
| | | CHECKED - | REVISED - MKS 04-08 | | | | | CONTRACT NO. 74150 | | | | | |
| | | DATE - | REVISED - DRM 01-09 | | | | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | | |



| | | | | | | | | | | |
|---|---------------------|------------|---------------------------|---|--|-------------|-------------------------|--------|--------------|-----------|
| FILE NAME = | USER NAME = swartzw | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | RAISED REFLECTIVE PAVEMENT MARKING DETAIL RAMP B / U.S. 51 / U.S. (BUS.) 51 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pw_work\pwidot\swartzw\d8138928\077450-details.dgn | DRAWN - | REVISED - | 322 | | | (58-20)RS | MACON | 151 | 64 | |
| PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - | CONTRACT NO. 74150 | | | | | | | |
| PLOT DATE = 3/17/2018 | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | | SCALE: 50 | SHEET NO. 1 OF 2 SHEETS | STA. | TO STA. | |



| | | | | | | | | | | | | |
|---|---------------------|------------|-----------|---|--|--|-------------------------|----------------------|-----------------|---------------------------|-----------------|--|
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| PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - | SCALE: 50 | | | | SHEET NO. 2 OF 2 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT | | |
| PLOT DATE = 3/17/2018 | DATE - | REVISED - | | | | | | | | | | |
| CONTRACT NO. 74150 | | | | | | | | | | | | |

EXISTING STRUCTURE: S.N. 058-0101(NB) & 0102(SB), originally constructed in 1976 as FA Route 412 Sec. 58-20HVB at Station 133+62.02, using rolled and welded steel I-girders with 8" concrete deck and 1 1/2" bituminous wearing surface, 4 spans, 291'-3 1/2" back-back abutments, variable out-out width, open stub abutments on concrete piles, multi-column piers with footings on concrete piles.

Staged construction shall be used to maintain one lane of traffic in each direction.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS

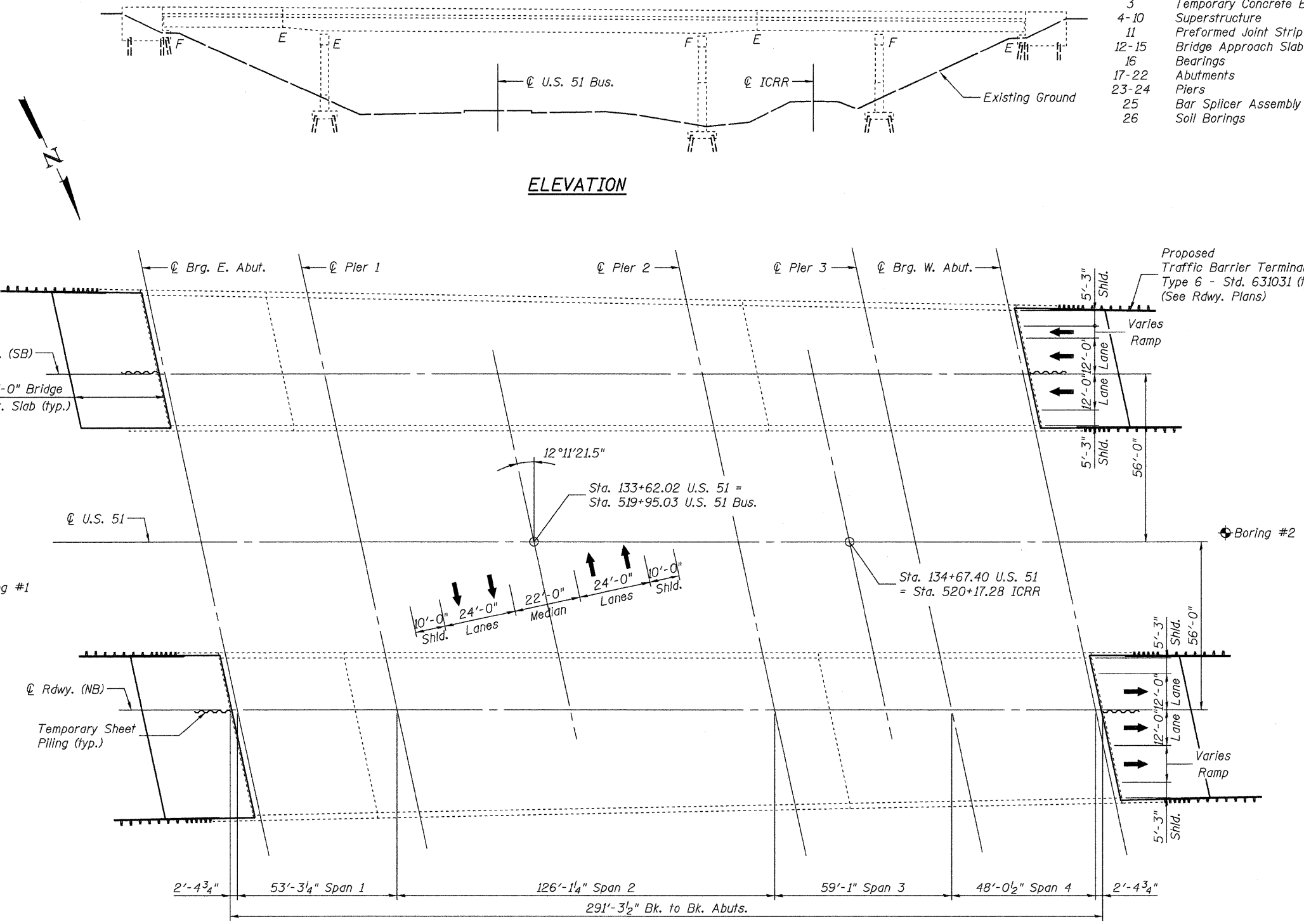
| Sheet No. | Description |
|-----------|------------------------------------|
| 1 | Gen Plan, Gen Notes, Bill of Mat'l |
| 2 | Temporary Sheet Piling |
| 3 | Temporary Concrete Barrier |
| 4-10 | Superstructure |
| 11 | Preformed Joint Strip Seal |
| 12-15 | Bridge Approach Slab Details |
| 16 | Bearings |
| 17-22 | Abutments |
| 23-24 | Piers |
| 25 | Bar Splicer Assembly Details |
| 26 | Soil Borings |

GENERAL NOTES

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.
 No field welding is permitted except as specified in the contract documents.
 Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.
 Reinforcement bars designated (E) shall be epoxy coated.
 Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
 Where shown on the plans, reinforcement bars shall be epoxy grouted into existing concrete according to Section 584 of the Standard Specifications. Cost included with Reinforcement Bars, Epoxy Coated.
 Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.
 Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.
 The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
 Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".
 All structural steel shall be shop painted with the inorganic zinc rich primer per AASHTO M300, Type 1. Cost included with Furnishing and Erecting Structural Steel.
 Protective Shield shall be provided for the full width of the bridge deck, over the roadway below from edge of shoulder to edge of shoulder, and over the railroad for 10 feet each side of the track centerline.
 Elevations shown on these plans are based on the original 1975 plan elevations, not the current datum. The original plan elevations may be used to establish a temporary benchmark for construction.

TOTAL BILL OF MATERIAL

| ITEM | UNIT | SB | NB |
|---|-------|-------|-------|
| Porous Granular Embankment, Special | Cu Yd | 167 | 209 |
| Hot-Mix Asphalt Surface Removal (Deck) | Sq Yd | 1327 | 1603 |
| Concrete Removal | Cu Yd | 52.7 | 63.1 |
| Protective Shield | Sq Yd | 558 | 664 |
| Structure Excavation | Cu Yd | 167 | 209 |
| Concrete Structures | Cu Yd | 50.8 | 58.4 |
| Concrete Superstructure | Cu Yd | 162.7 | 197.4 |
| Bridge Deck Grooving | Sq Yd | 1523 | 1855 |
| Protective Coat | Sq Yd | 1851 | 2184 |
| Furnishing And Erecting Structural Steel | Pound | 1100 | 1260 |
| Jack And Remove Existing Bearings | Each | 7 | 8 |
| Reinforcement Bars, Epoxy Coated | Pound | 40390 | 48040 |
| Bar Splicers | Each | 391 | 408 |
| Temporary Sheet Piling | Sq Ft | 328 | 334 |
| Preformed Joint Strip Seal | Foot | 178 | 213 |
| Elastomeric Bearing Assembly, Type I | Each | 7 | 8 |
| Anchor Bolts, 1" | Each | 14 | 16 |
| Geocomposite Wall Drain | Sq Yd | 75 | 89 |
| Pipe Underdrains For Structures 4" | Foot | 243 | 260 |
| Structural Repair Of Concrete (Depth = < 5") | Sq Ft | 24 | 22 |
| Bridge Deck Microsilica Concrete Overlay 2 1/4" | Sq Yd | 1248 | 1507 |
| Bridge Deck Hydro-Scarification 1/2" | Sq Yd | 1248 | 1507 |
| Deck Slab Repair (Full Depth, Type II) | Sq Yd | 15 | 17 |



PLAN

JD Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

| | |
|---------------|--------------|
| DESIGNED: JDQ | DRAWN: PTR |
| CHECKED: DCD | CHECKED: DCD |



Signed: *David Depp*
Date: 4-9-2010
Lic. Expires: 11-30-2010

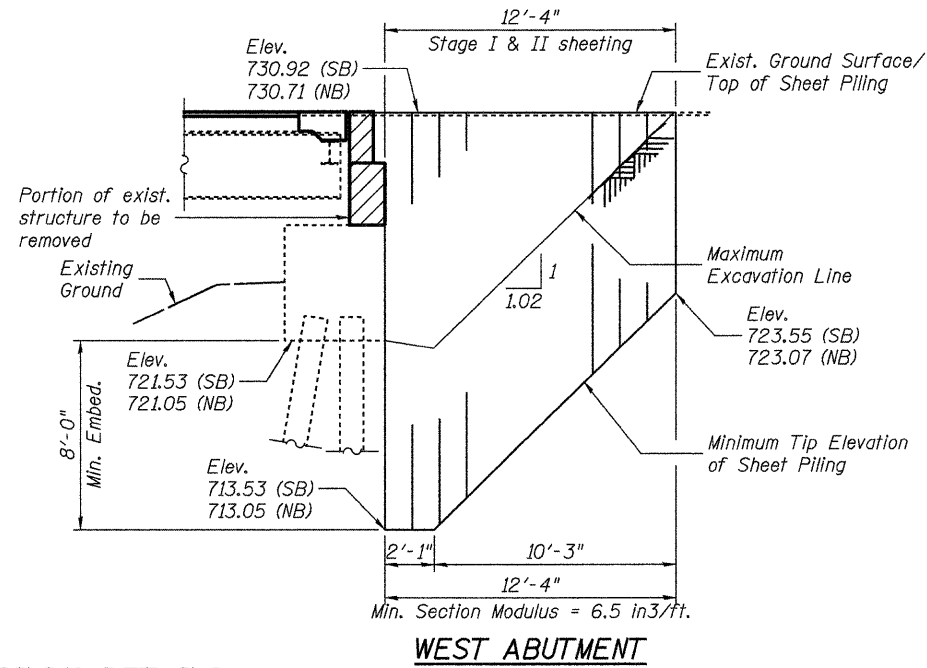
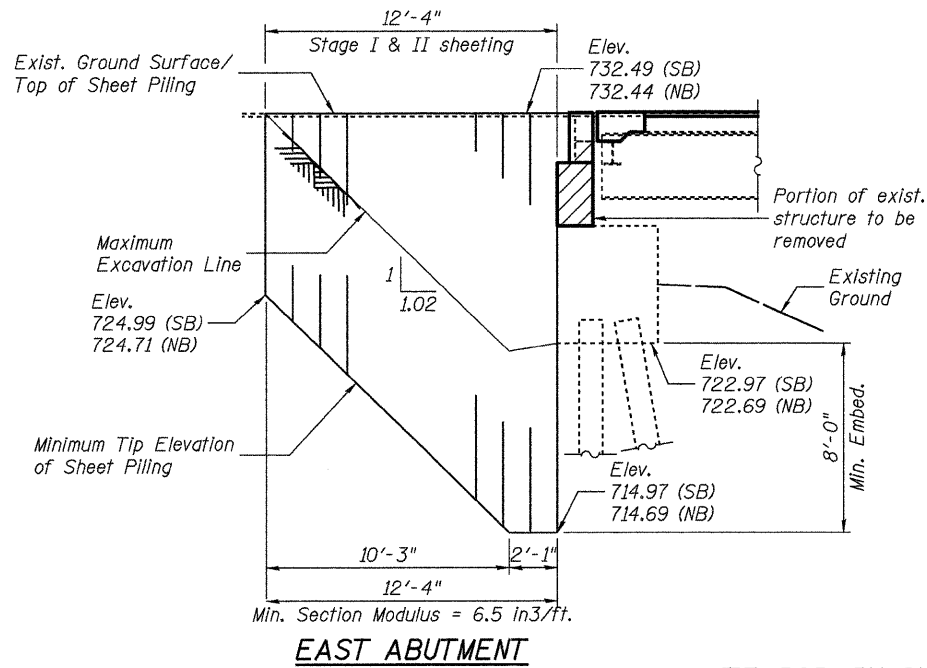


GENERAL PLAN & ELEVATION
U.S. 51 OVER U.S. 51 BUS. & ICRR
F.A.P. RTE. 322 SECTION (58-20)RS
MACON COUNTY
STATION 133+62.02
STRUCTURE NO. 058-0101(NB) & 0102(SB)

| | | | | | |
|------------------|-------------|---------------------|---------------------------|--------------|-----------|
| SHEET 1 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 66 |
| | | STA. 133+62.02 | CONTRACT NO. | 74150 | |
| | | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | |

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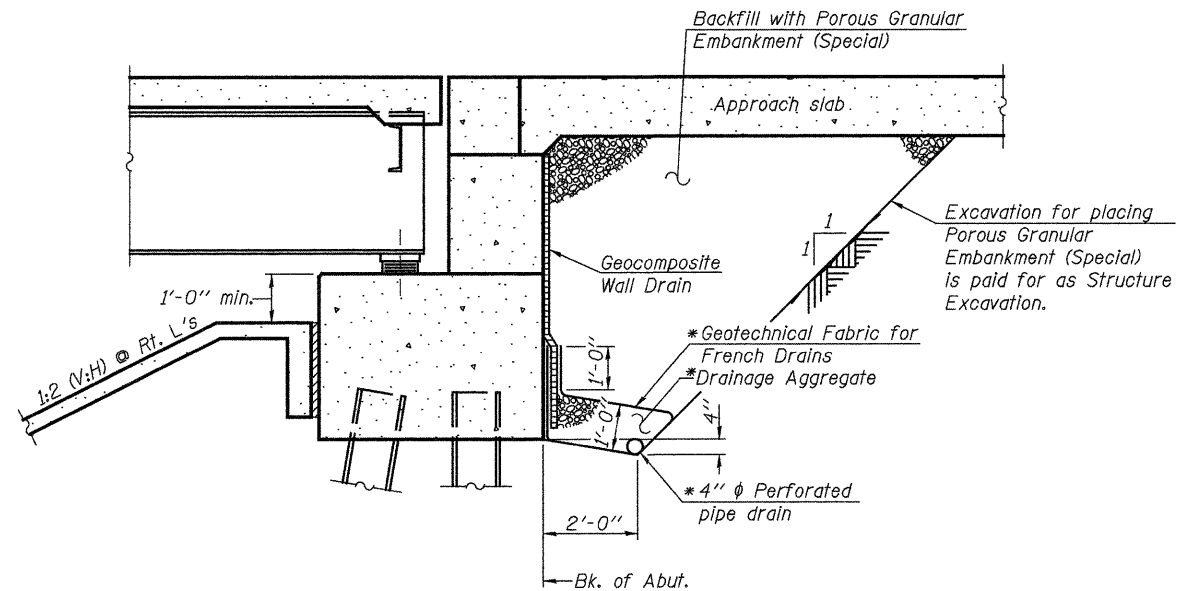
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



TEMPORARY SHEET PILING DETAILS

(Slopes and horizontal dimensions are measured parallel to \perp roadway)

If the Contractor chooses to alter the temporary cantilevered sheet piling design requirements shown on plans, a design submittal including plan details and calculations will be required for review and acceptance by the Engineer.



**SECTION THRU PILE SUPPORTED
STUB ABUTMENT**
(Horiz. dim. \odot Rt. L's)

*Included in the cost of Pipe Underdrains for Structures.

Note:
All drainage system components shall extend parallel to the abutment back wall until they intersect the wingwalls. The pipe shall extend under the wingwall, then to the toe of slope. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).

**TEMPORARY SHEET PILING
STRUCTURE NO. 058-0101(NB) & 0102(SB)**

| | | | | | |
|------------------|----------------|---------------------|---------------------------|-----------------|--------------|
| SHEET 2 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 67 |
| | | STA. 133+62.02 | CONTRACT NO. 74150 | | |
| | | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | |

JD Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

| | |
|---------------|--------------|
| DESIGNED: JDQ | DRAWN: SJS |
| CHECKED: DCD | CHECKED: DCD |

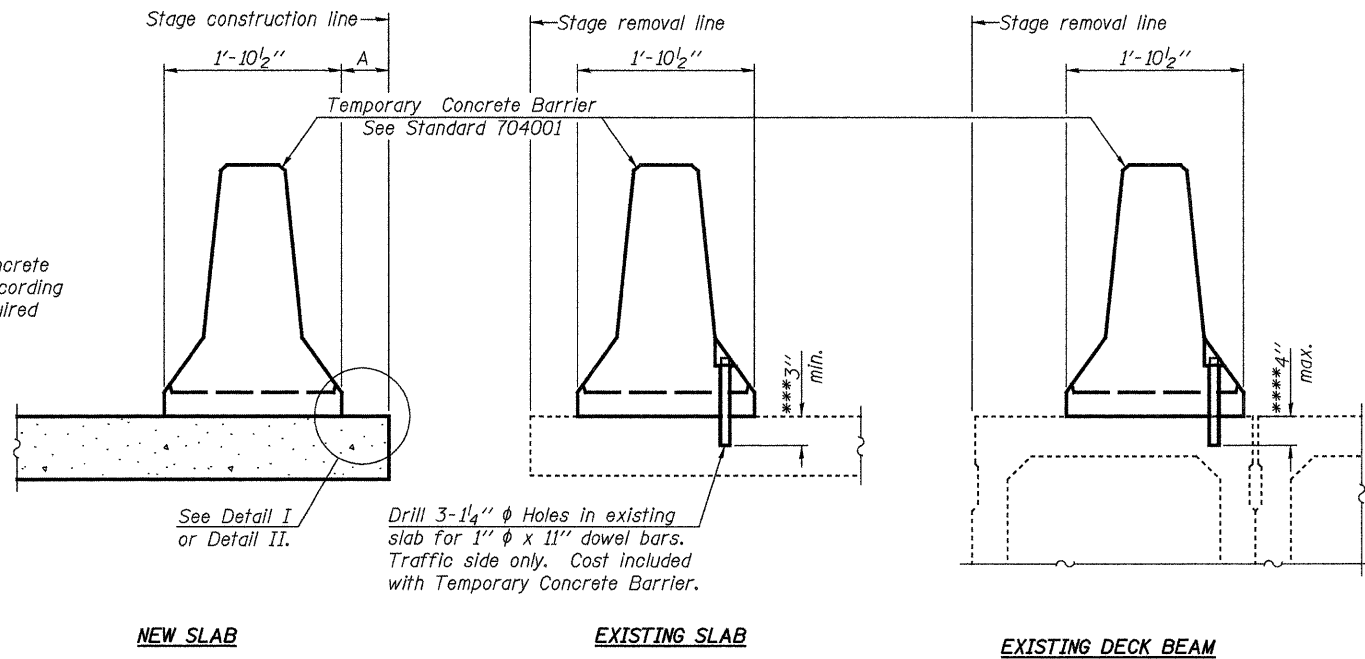
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USER: DCD

DATE: 04/09/2010 12:21:07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



SECTIONS THRU SLAB OR DECK BEAM

NOTES

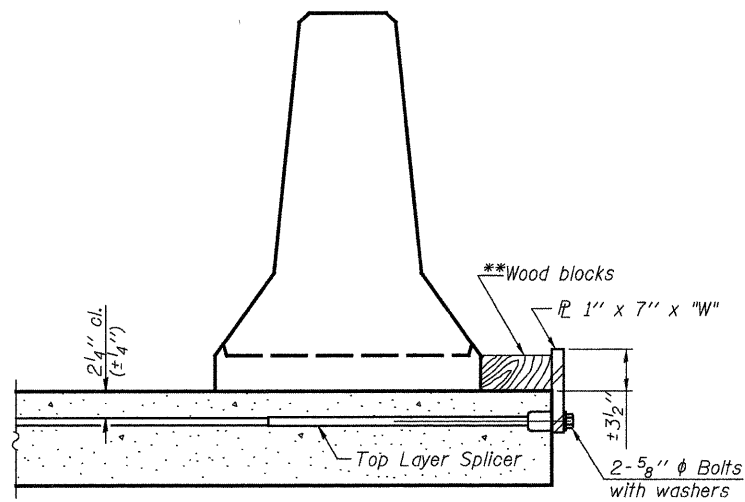
Detail I - With Bar Splicer or Couplers:
Connect one (1) 1"x7"x10" steel PL to the top layer of couplers with 2-5/8" φ bolts screwed to coupler at approximate C of each barrier panel.

Detail II - With Extended Reinforcement Bars:
Connect one (1) 1"x7"x 10" steel PL to the concrete slab or concrete wearing surface with 2-5/8" φ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate C of each barrier panel.

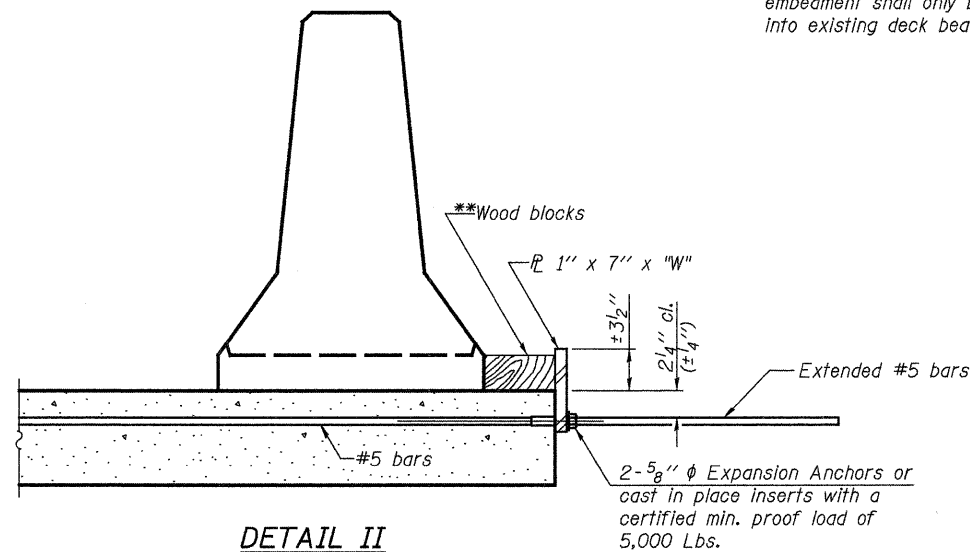
Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



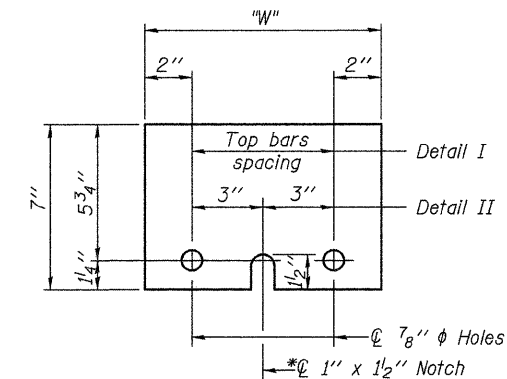
DETAIL I



DETAIL II

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

"W" = Top bars spacing + 4"



STEEL RETAINER PL 1" x 7" x 10"

* Required only with Detail II

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Springfield, Illinois

| | |
|---------------|--------------|
| DESIGNED: JDQ | DRAWN: SJS |
| CHECKED: DCD | CHECKED: DCD |

R-27

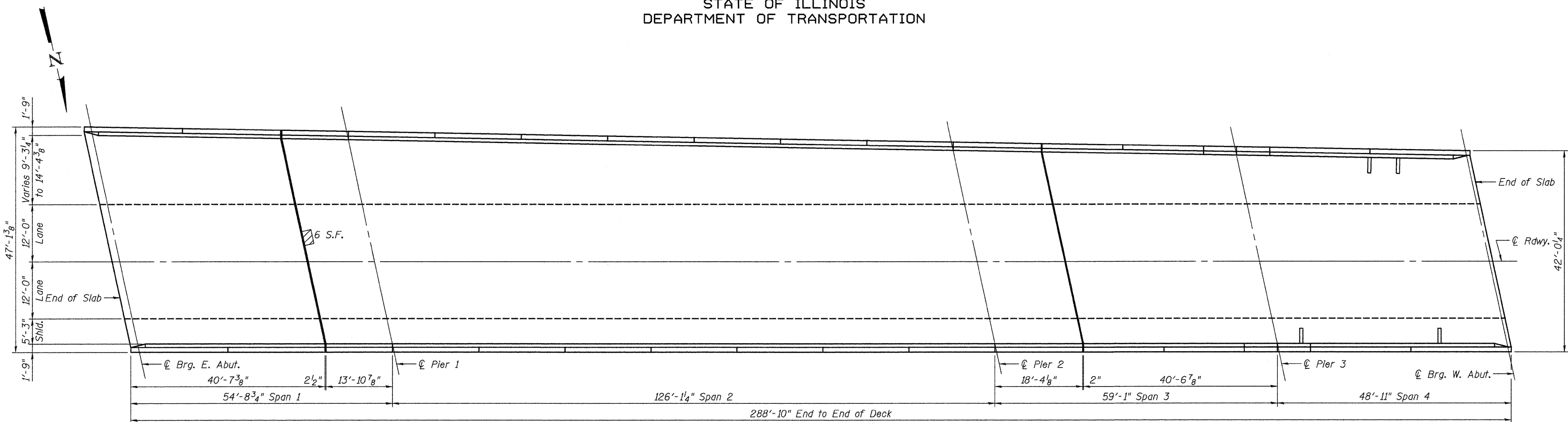
11-1-09

TEMPORARY CONCRETE BARRIER
FOR STAGE CONSTRUCTION
STRUCTURE NO. 058-0101(NB) & 0102(SB)

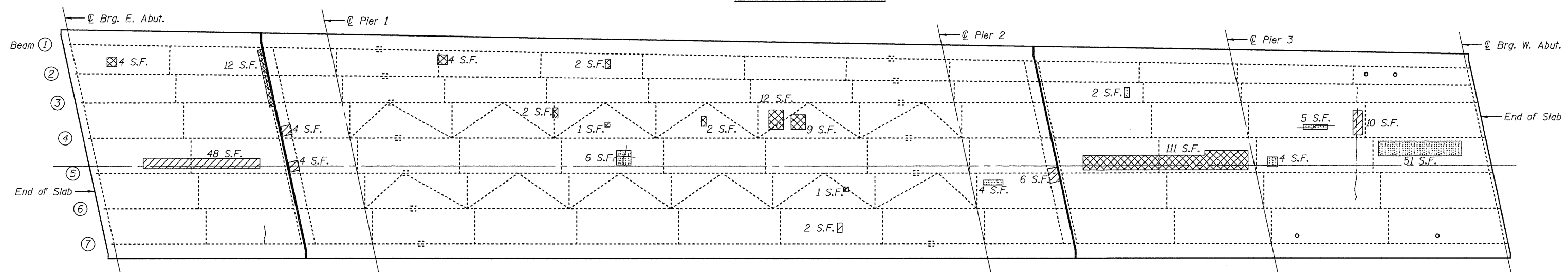
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| SHEET 3 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 68 |
| | | STA. 133+62.02 | CONTRACT NO. 74150 | | |
| | | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

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DATE: 04/09/2010 12:24:22



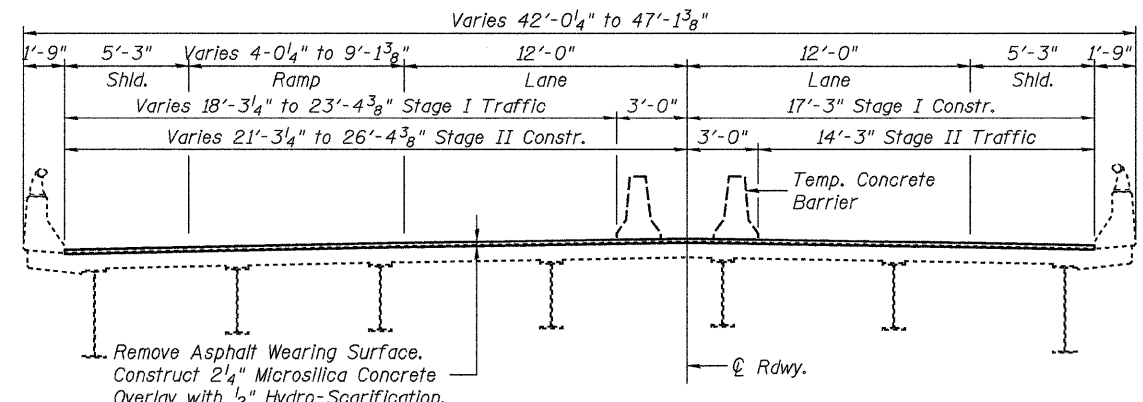
DECK PLAN-TOP (SB)



DECK PLAN-BOTTOM (SB)

LEGEND

- Hollow or Unsound Concrete
- Spalled Concrete
- Spalled Concrete with Exposed Reinf.
- Hairline Crack



CROSS SECTION
(Southbound, Looking West)

Notes:
Deck Condition Survey performed 9/16/2009.
Plan quantities assume that areas of "exposed reinforcing" greater than 5 sq. ft. will use Full-depth Type II repair.
Repair areas are estimated, the Engineer shall determine actual repair locations and record them on the As-Built plans.
Partial depth repairs are included with Bridge Deck Hydro-scarification, see Special Provision for Bridge Deck Microsilica Concrete Overlay.

BILL OF MATERIAL

| Item | Unit | Total |
|--|---------|-------|
| Deck Slab Repair (Full Depth, Type II) | Sq. Yd. | 15 |

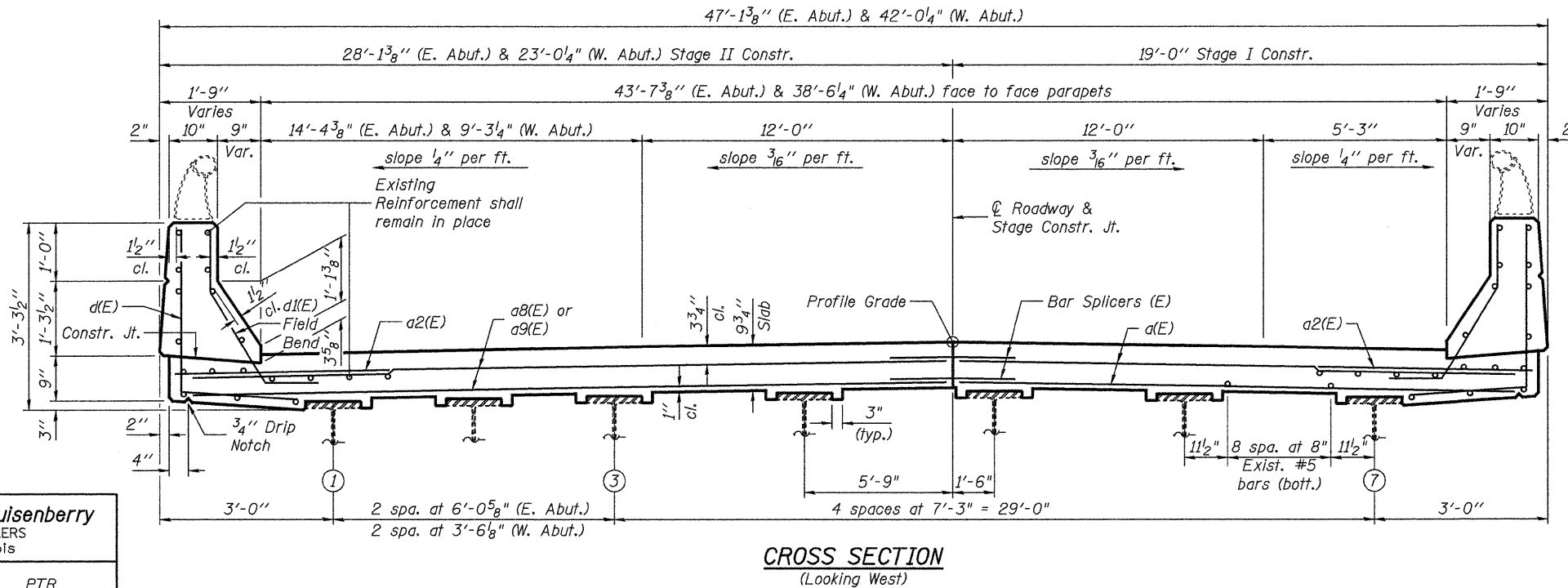
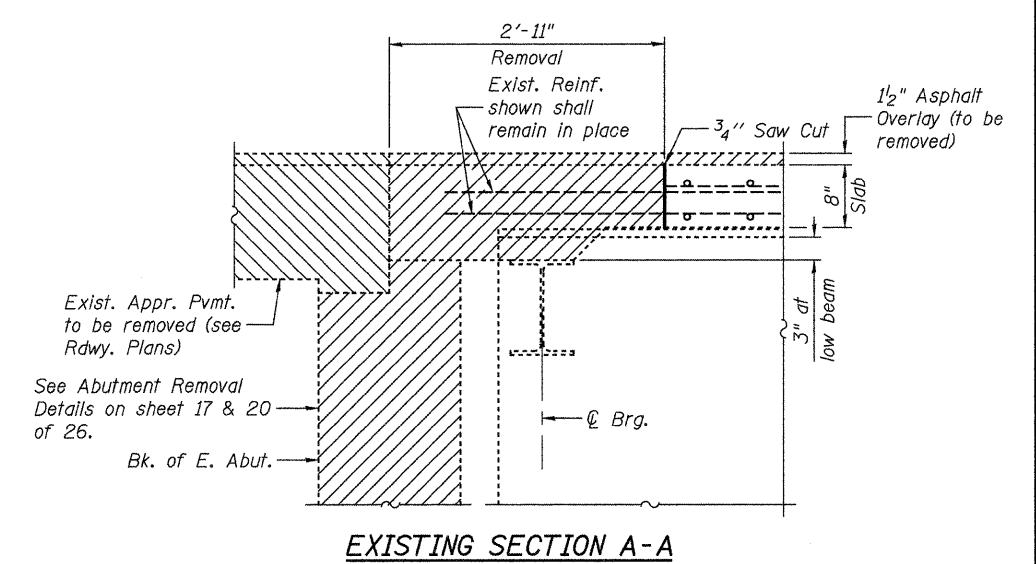
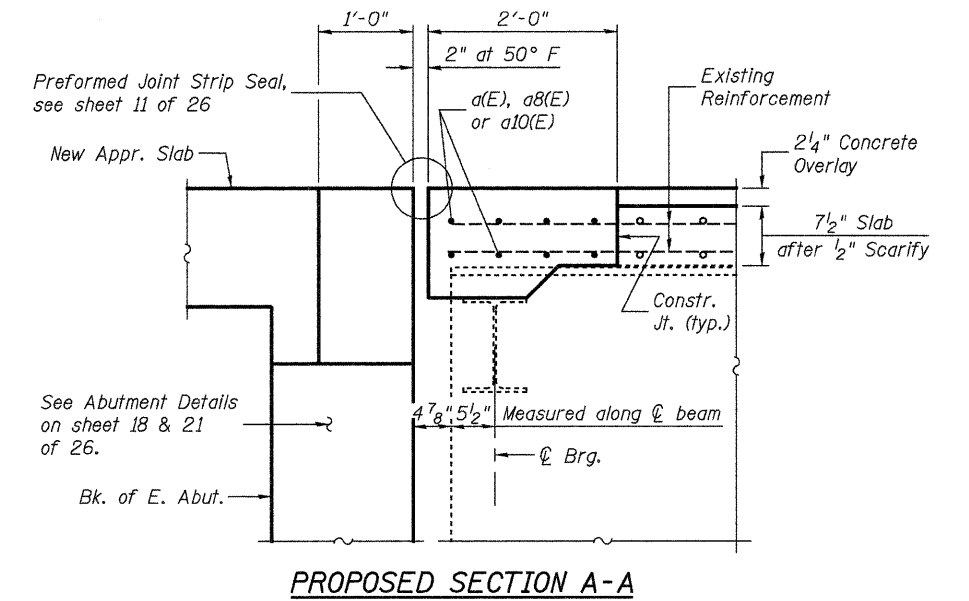
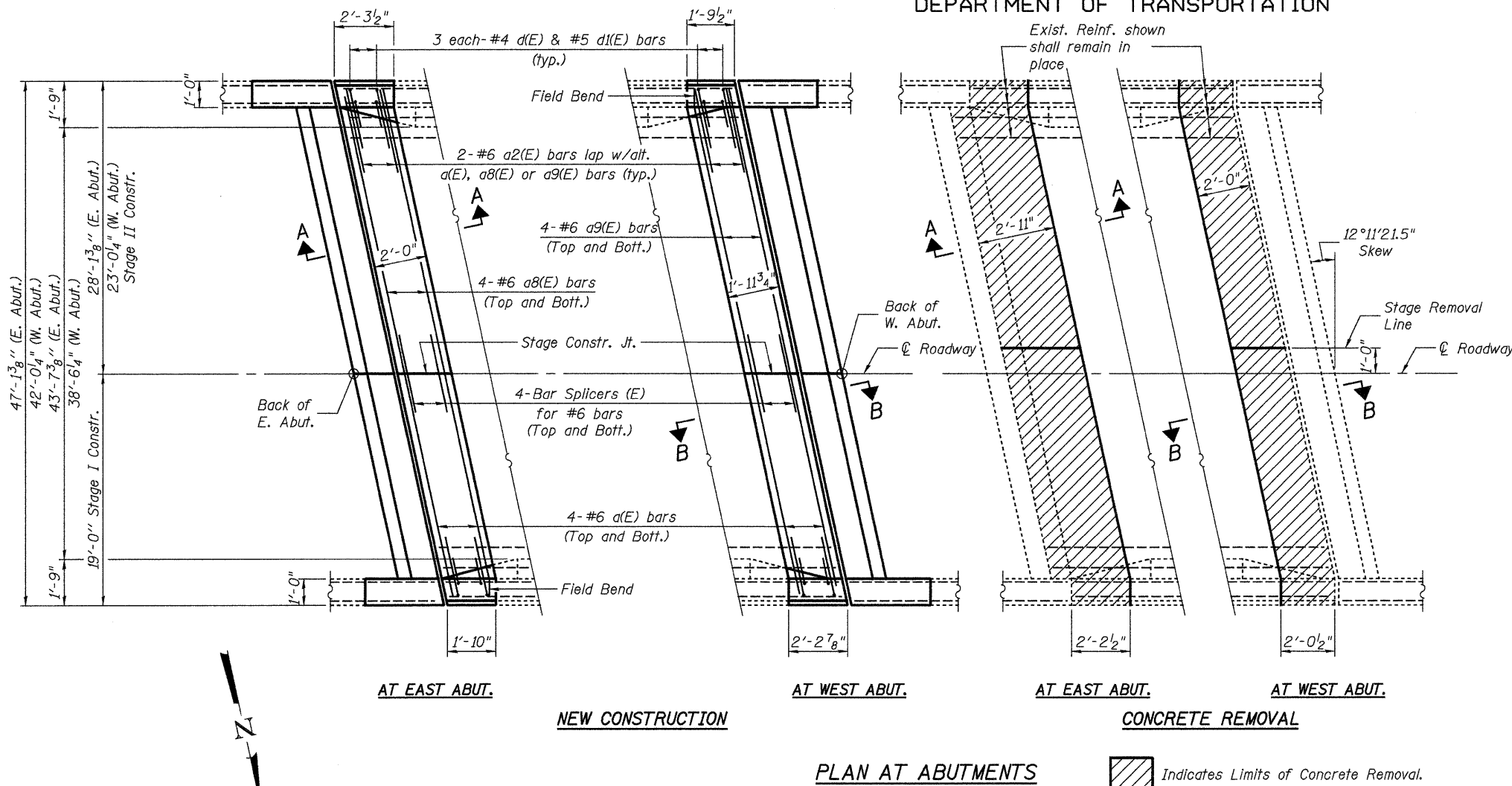
Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

| | |
|---------------|---------------|
| DESIGNED: JDQ | DRAWN: P. Ray |
| CHECKED: DCD | CHECKED: DCD |

DECK PLAN (SB)
STRUCTURE NO. 058-0101(NB) & 0102(SB)

| | | | | | |
|---|-----------------|-------------------|--------------------|------------------|--------------|
| SHEET 4 OF 26 | F.A.P. RTE. 322 | SECTION (58-20)RS | COUNTY MACON | TOTAL SHEETS 151 | SHEET NO. 69 |
| | STA. 133+62.02 | | CONTRACT NO. 74150 | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



Notes:
For Section B-B see sheet 8 of 26.
For Parapet Details and Bill of Material see sheet 10 of 26.

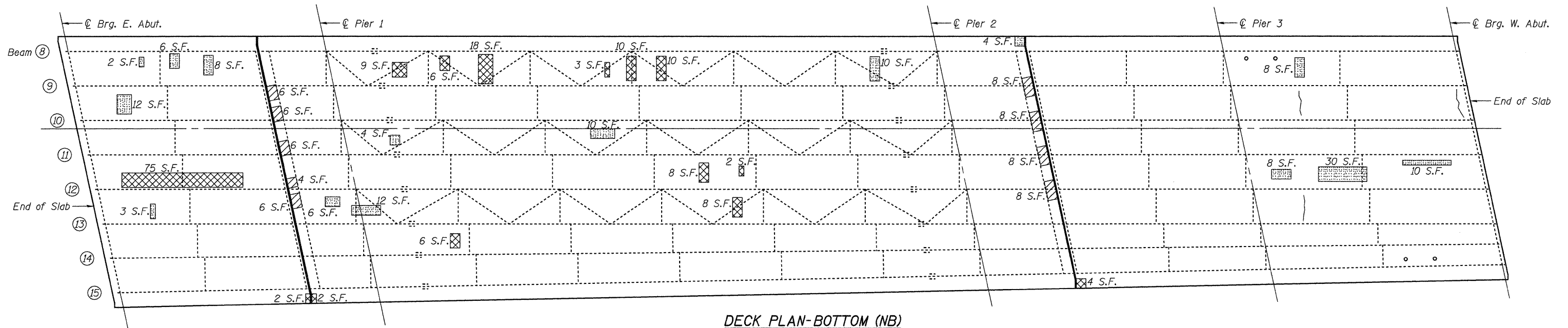
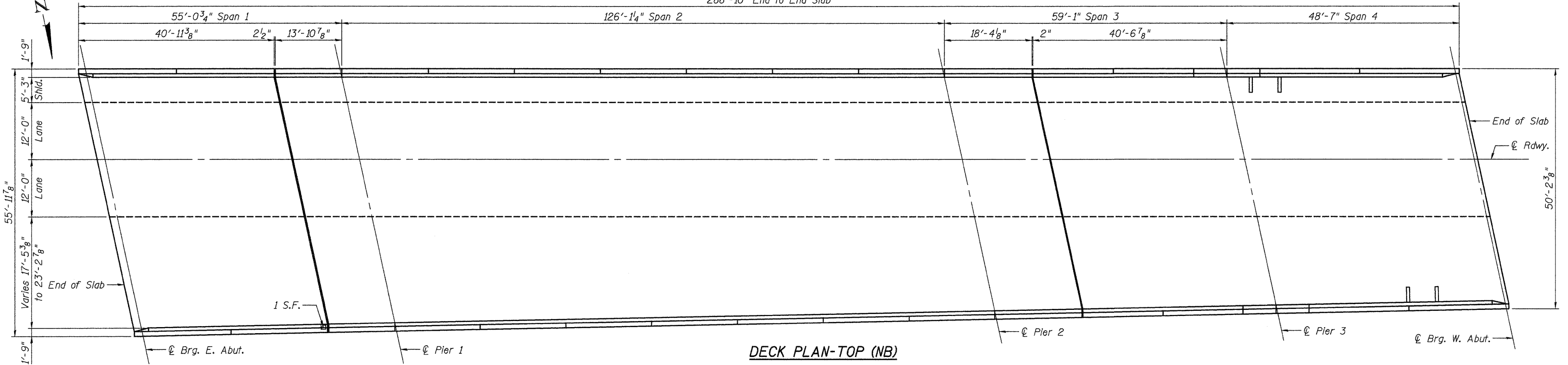
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|--|--------------|
| JD Johnson, Depp & Quisenberry CONSULTING ENGINEERS Springfield, Illinois | |
| DESIGNED: JDQ | DRAWN: PTR |
| CHECKED: DCD | CHECKED: DCD |

| | | | | |
|--|----------------|---------------------------|--------------------|--------------|
| SUPERSTRUCTURE (SB) | | | | |
| STRUCTURE NO. 058-0101(NB) & 0102(SB) | | | | |
| SHEET 5 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS |
| | 322 | (58-20)RS | MACON | 151 70 |
| | STA. 133+62.02 | | CONTRACT NO. 74150 | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

288'-10" End to End Slab

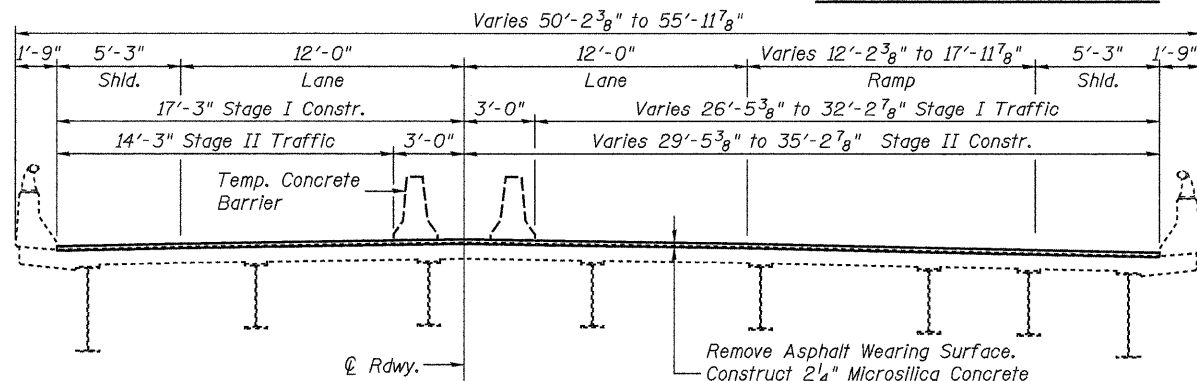


LEGEND

- Hollow or Unsound Concrete
- Spalled Concrete
- Spalled Concrete with Exposed Reinf.
- Hairline Crack

Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

| | |
|---------------|---------------|
| DESIGNED: JDQ | DRAWN: P. Ray |
| CHECKED: DCD | CHECKED: DCD |



Notes:

- Deck Condition Survey performed 9/16/2009.
- Plan quantities assume that areas of "exposed reinforcing" greater than 5 sq. ft. will use Full-depth Type II repair.
- Repair areas are estimated, the Engineer shall determine actual repair locations and record them on the As-Built plans.
- Partial depth repairs are included with Bridge Deck Hydro-scarification, see Special Provision for Bridge Deck Microsilica Concrete Overlay.

BILL OF MATERIAL

| Item | Unit | Total |
|--|---------|-------|
| Deck Slab Repair (Full Depth, Type II) | Sq. Yd. | 17 |

DECK PLAN (NB)
STRUCTURE NO. 058-0101(NB) & 0102(SB)

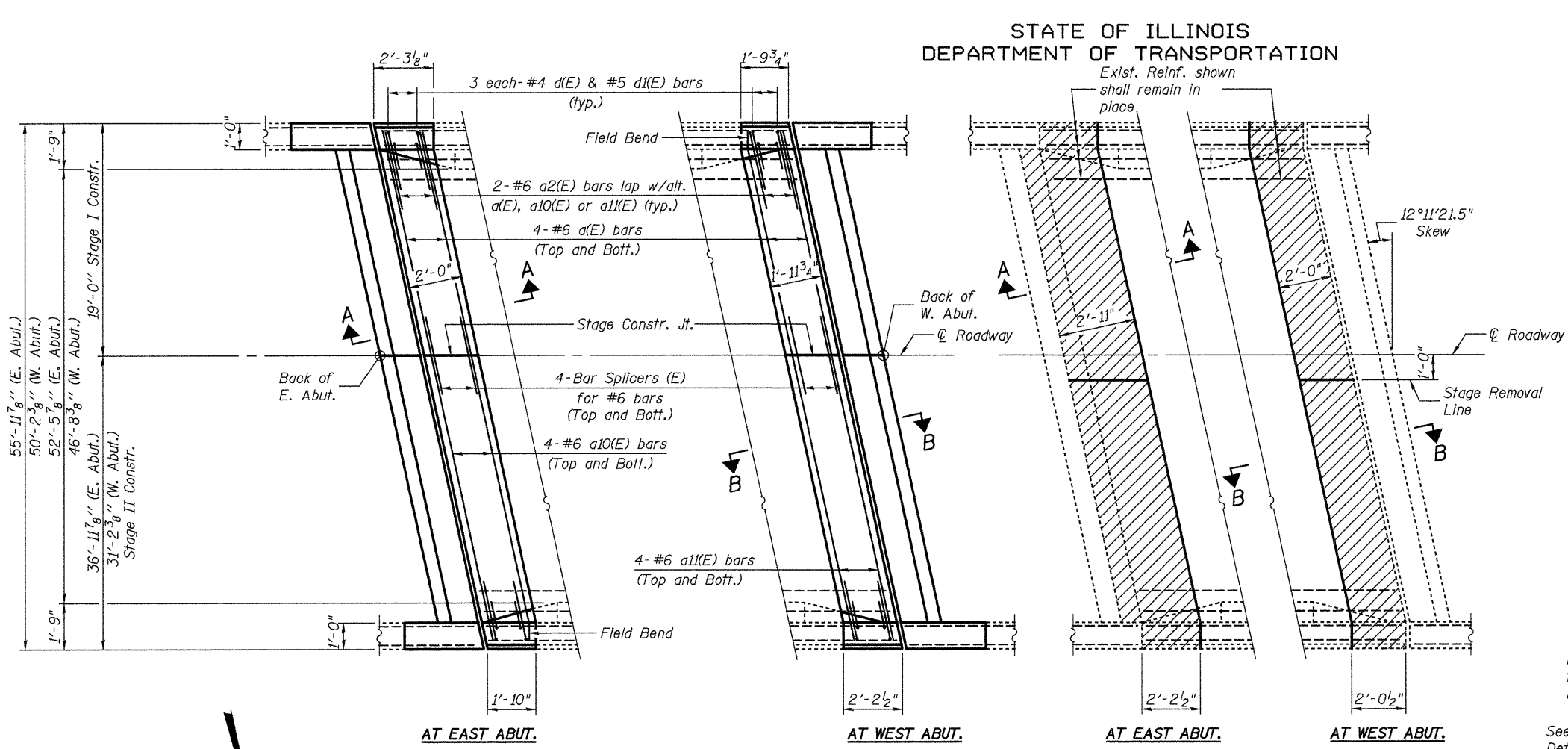
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|---------------------|-------------|---------------------------|--------------------|--------------|-----------|
| SHEET 7 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 72 |
| STA. 133+62.02 | | | CONTRACT NO. 74150 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | |

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

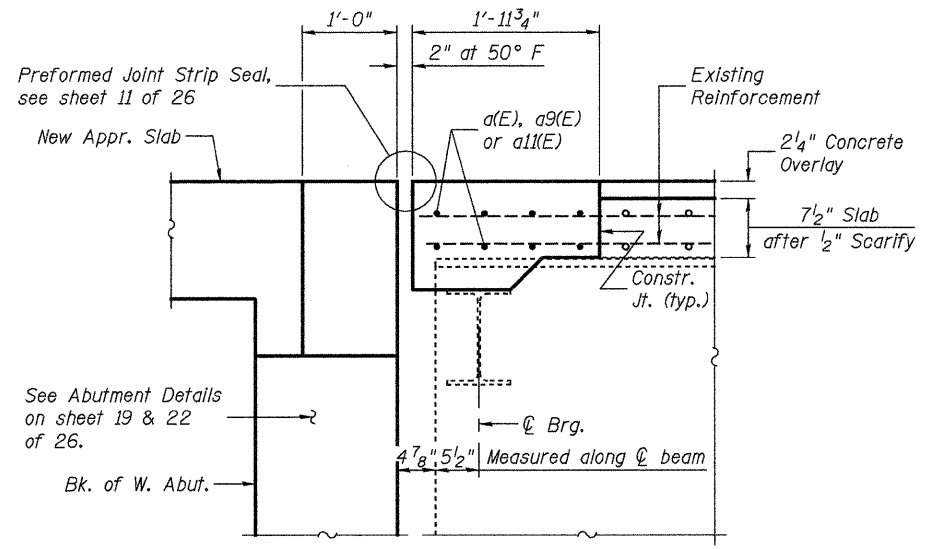


NEW CONSTRUCTION

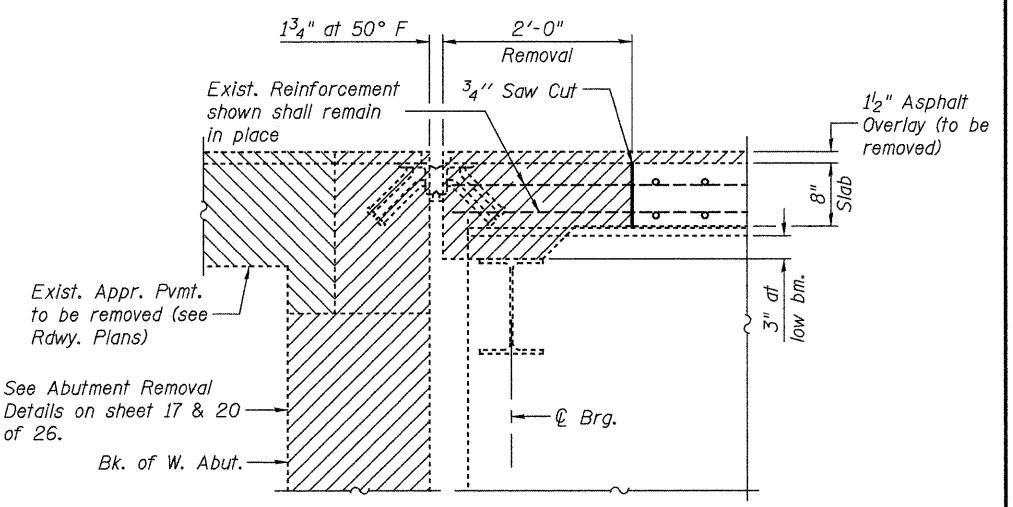
CONCRETE REMOVAL

PLAN AT ABUTMENTS

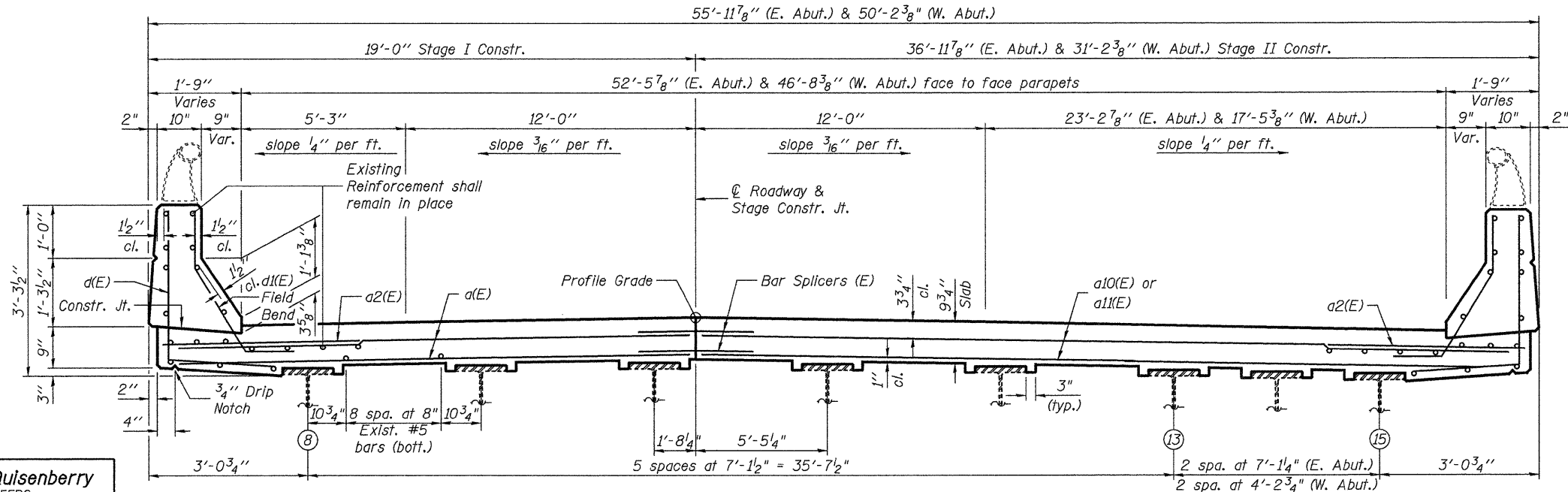
Indicates Limits of Concrete Removal.



PROPOSED SECTION B-B



EXISTING SECTION B-B



CROSS SECTION
(Looking West)

Notes:
For Section A-A see sheet 5 of 26.
For Parapet Details and Bill of Material see sheet 10 of 26.

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Springfield, Illinois

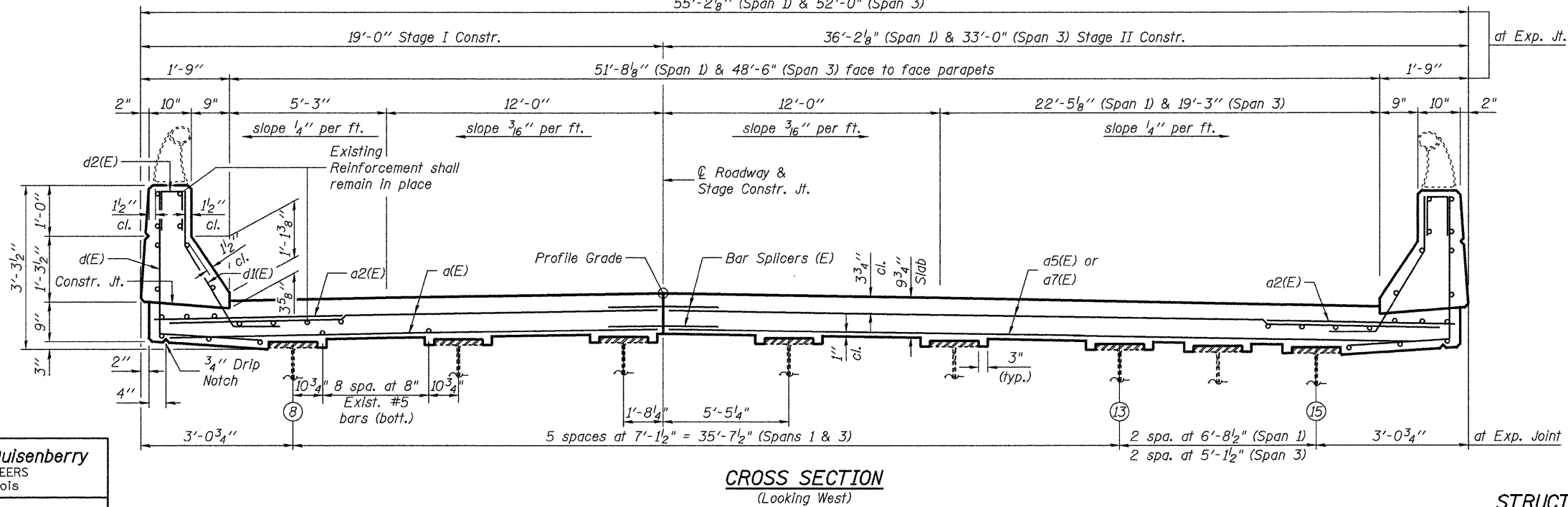
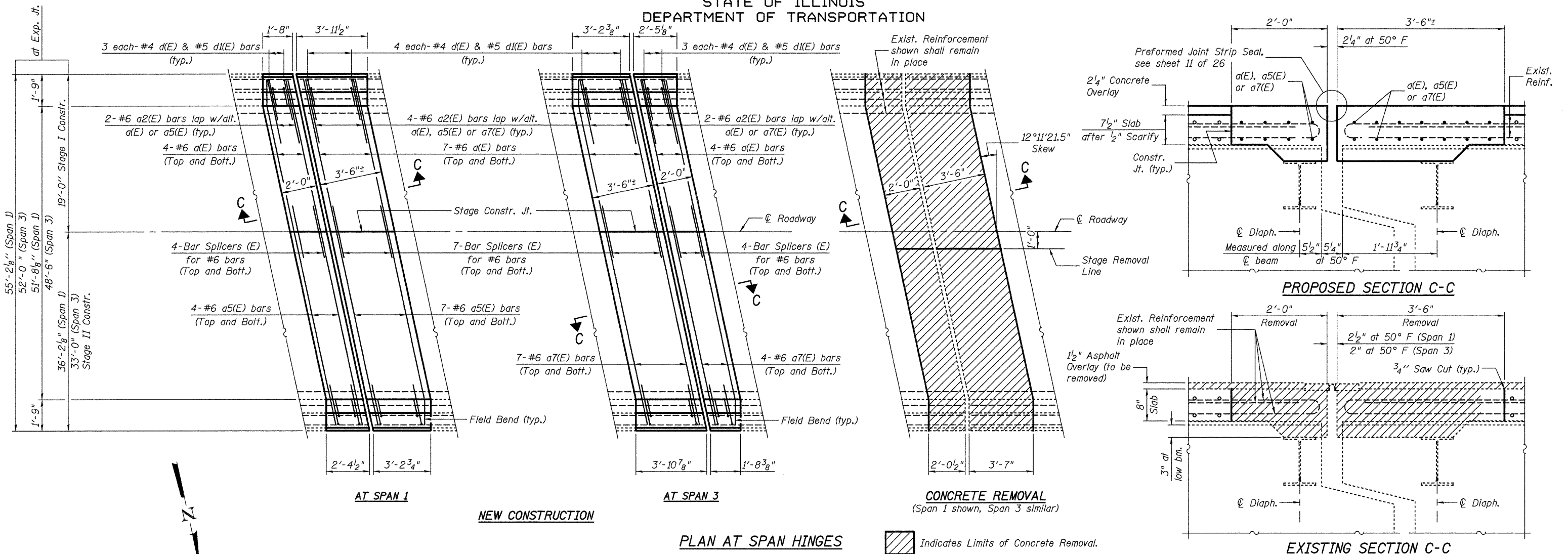
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| DESIGNED: JDQ | DRAWN: PTR |
| CHECKED: DCD | CHECKED: DCD |

SUPERSTRUCTURE (NB)
STRUCTURE NO. 058-0101(NB) & 0102(SB)

| | | | | | |
|------------------|---------------------|-------------------|---------------------------|------------------|--------------|
| SHEET 8 OF 26 | F.A.P. RTE. 322 | SECTION (58-20)RS | COUNTY MACON | TOTAL SHEETS 151 | SHEET NO. 73 |
| | STA. 133+62.02 | | CONTRACT NO. 74150 | | |
| | FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



Notes:
For Parapet Details and Bill of Material see sheet 10 of 26.

JD Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

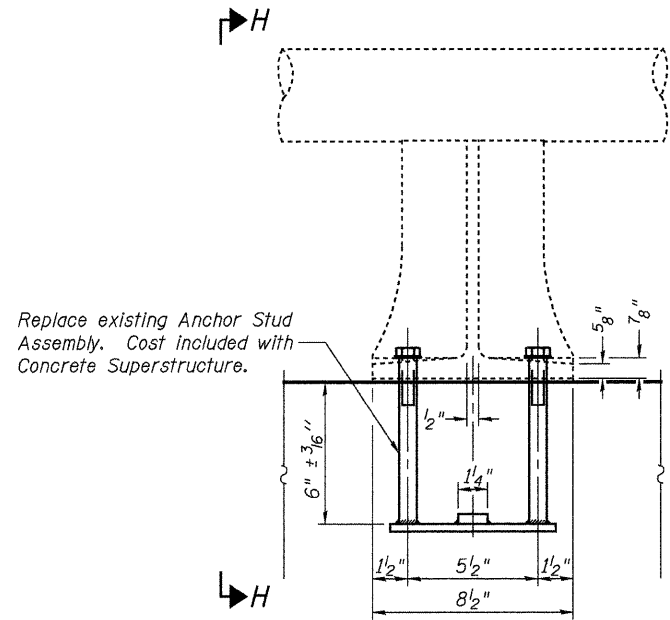
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| CHECKED: DCD | CHECKED: DCD |

SUPERSTRUCTURE (NB)
STRUCTURE NO. 058-0101(NB) & 0102(SB)

| | | | | | |
|---|----------------|-----------|--------------------|--------------|-----------|
| SHEET 9 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 74 |
| | STA. 133+62.02 | | CONTRACT NO. 74150 | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | | |

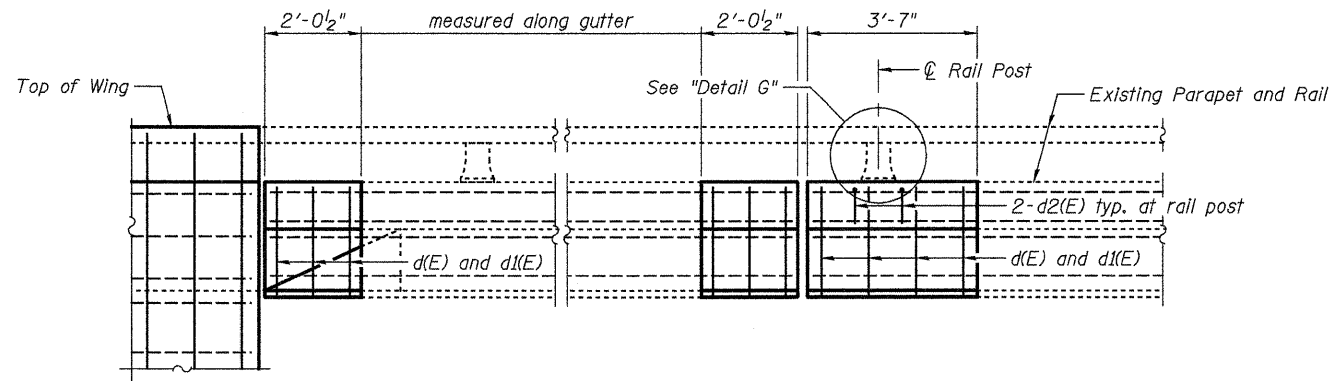
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



Replace existing Anchor Stud Assembly. Cost included with Concrete Superstructure.

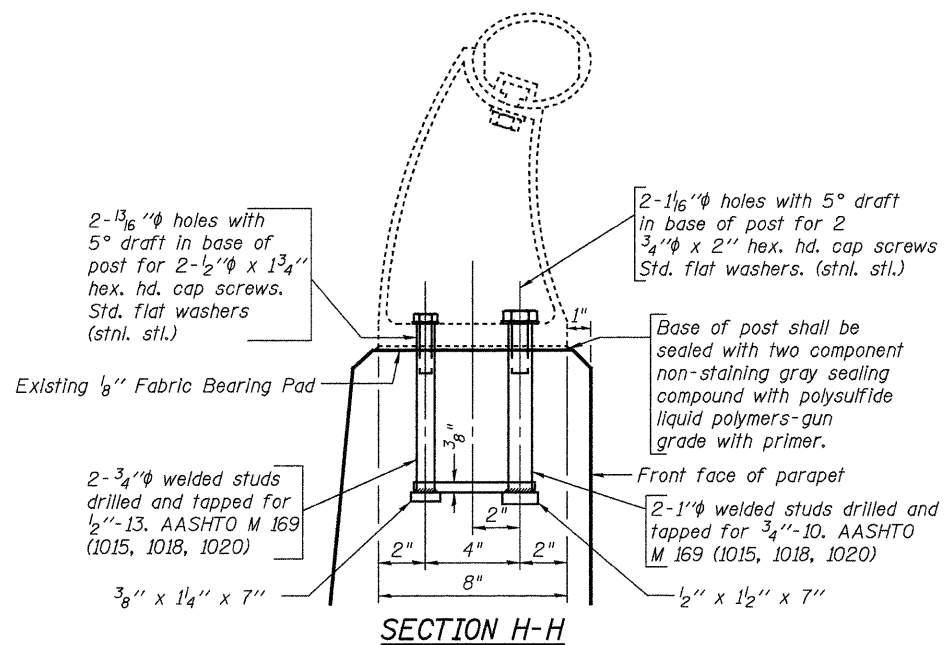
DETAIL G



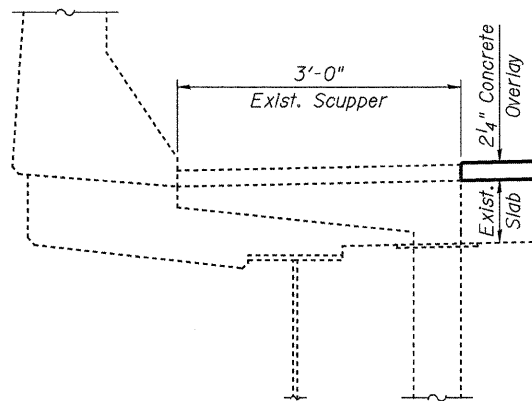
AT EAST ABUTMENT

AT SPAN 1

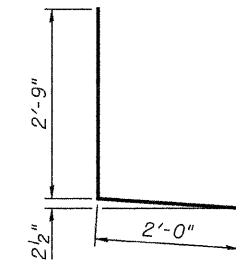
PARTIAL INSIDE ELEVATION OF PARAPET
(Span 3 & West Abutment similar)



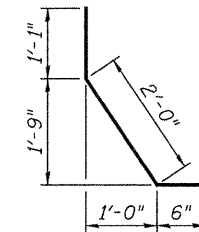
SECTION H-H



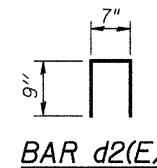
SECTION AT DRAINAGE SCUPPER



BAR d(E)



BAR d1(E)



BAR d2(E)

SUPERSTRUCTURE (SB)
BILL OF MATERIAL

| Bar | No. | Size | Length | Shape |
|----------------------------------|-----|---------|--------|-------|
| a(E) | 60 | #6 | 18'-6" | — |
| a1(E) | 22 | #6 | 27'-3" | — |
| a2(E) | 32 | #6 | 5'-0" | — |
| a4(E) | 22 | #6 | 24'-4" | — |
| a8(E) | 8 | #6 | 28'-0" | — |
| a9(E) | 8 | #6 | 22'-9" | — |
| d(E) | 40 | #4 | 4'-9" | L |
| d1(E) | 40 | #5 | 3'-7" | L |
| d2(E) | 8 | #4 | 2'-1" | Π |
| Reinforcement Bars, Epoxy Coated | | Pound | 4510 | |
| Concrete Superstructure | | Cu. Yd. | 31.8 | |
| Concrete Removal | | Cu. Yd. | 29.1 | |

SUPERSTRUCTURE (NB)
BILL OF MATERIAL

| Bar | No. | Size | Length | Shape |
|----------------------------------|-----|---------|--------|-------|
| a(E) | 60 | #6 | 18'-6" | — |
| a2(E) | 32 | #6 | 5'-0" | — |
| a5(E) | 22 | #6 | 36'-5" | — |
| a7(E) | 22 | #6 | 33'-3" | — |
| a10(E) | 8 | #6 | 37'-3" | — |
| a11(E) | 8 | #6 | 31'-4" | — |
| d(E) | 40 | #4 | 4'-9" | L |
| d1(E) | 40 | #5 | 3'-7" | L |
| d2(E) | 8 | #4 | 2'-1" | Π |
| Reinforcement Bars, Epoxy Coated | | Pound | 5330 | |
| Concrete Superstructure | | Cu. Yd. | 39.4 | |
| Concrete Removal | | Cu. Yd. | 36.2 | |

SUPERSTRUCTURE DETAILS
STRUCTURE NO. 058-0101(NB) & 0102(SB)

| | | | | | |
|---------------------|----------------|---------------------------|--------------------|--------------|-----------|
| SHEET 10 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 75 |
| | STA. 133+62.02 | | CONTRACT NO. 74150 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | |

JD Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

| | |
|---------------|--------------|
| DESIGNED: JDQ | DRAWN: PTR |
| CHECKED: DCD | CHECKED: DCD |

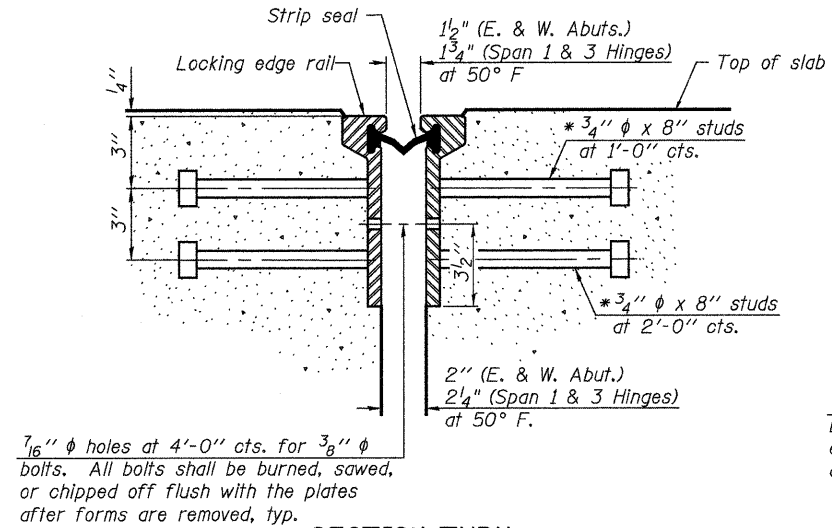
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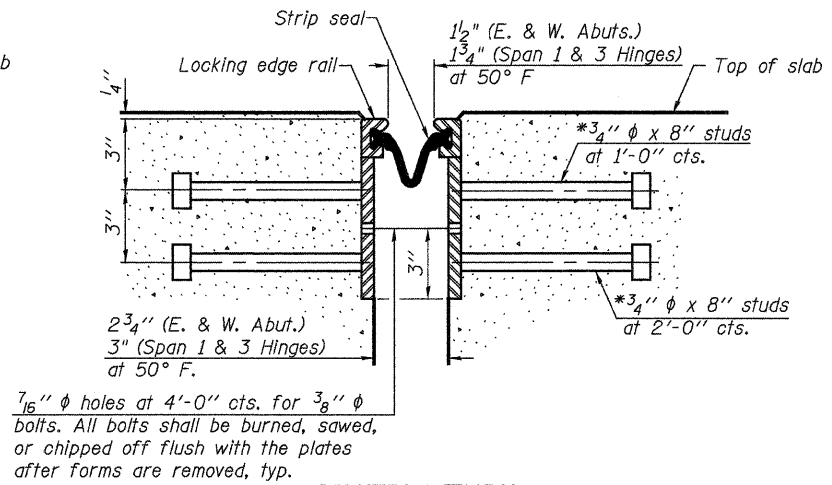
* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

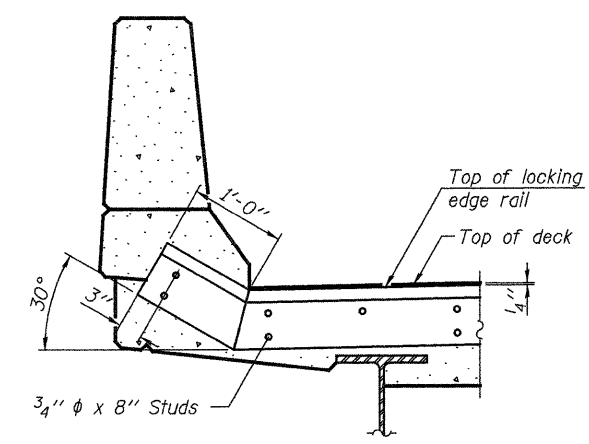


SECTION THRU ROLLED RAIL JOINT

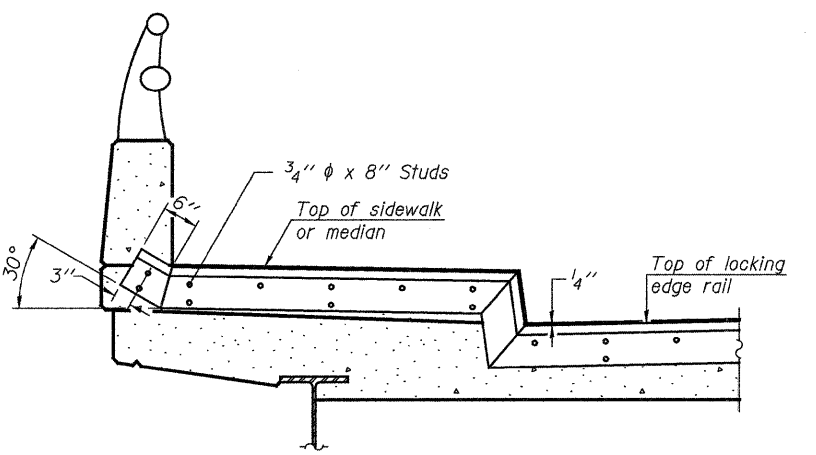
7/16" φ holes at 4'-0" cts. for 3/8" φ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.



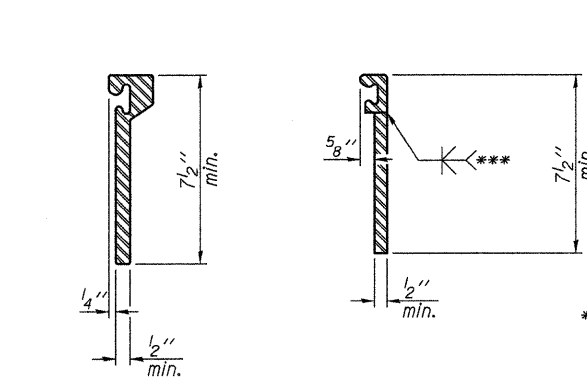
SECTION THRU WELDED RAIL JOINT



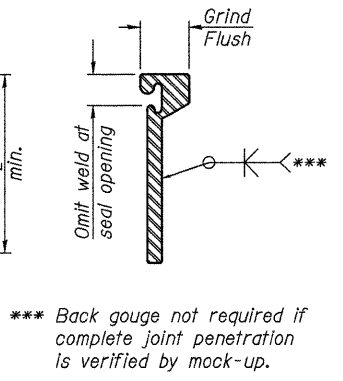
AT PARAPET
See Section A-A for end treatment of skews > 30°.



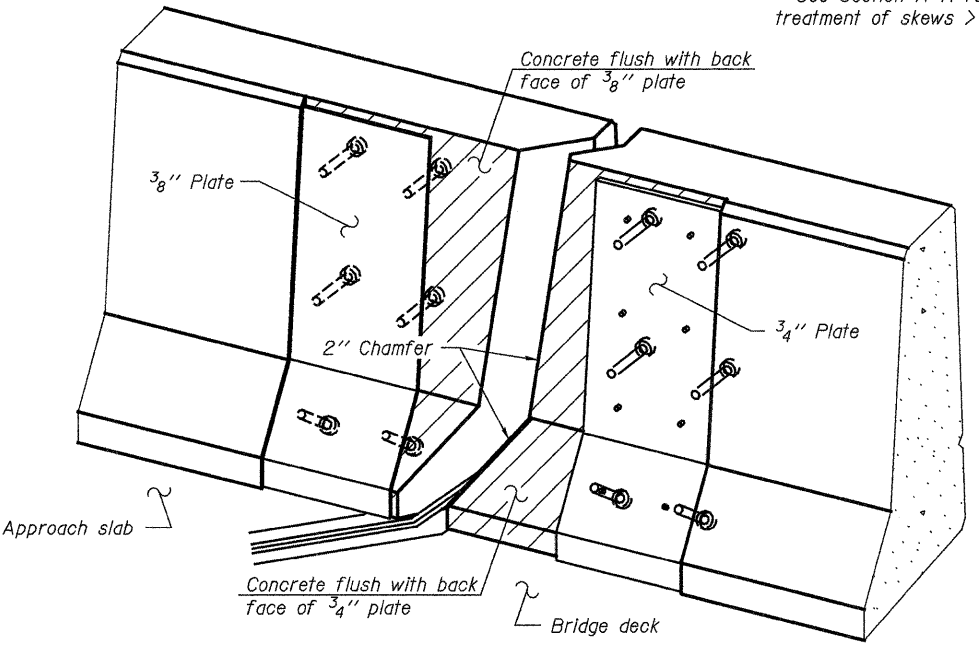
AT SIDEWALK OR MEDIAN
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



ROLLED EXTRUDED RAIL
WELDED RAIL



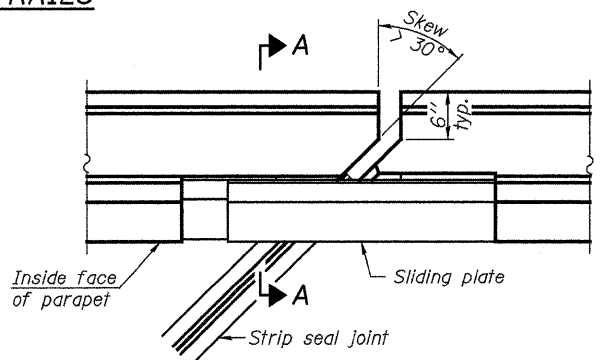
LOCKING EDGE RAIL SPLICE
The inside of the locking edge rail groove shall be free of weld residue.
Rolled rail shown, welded rail similar.



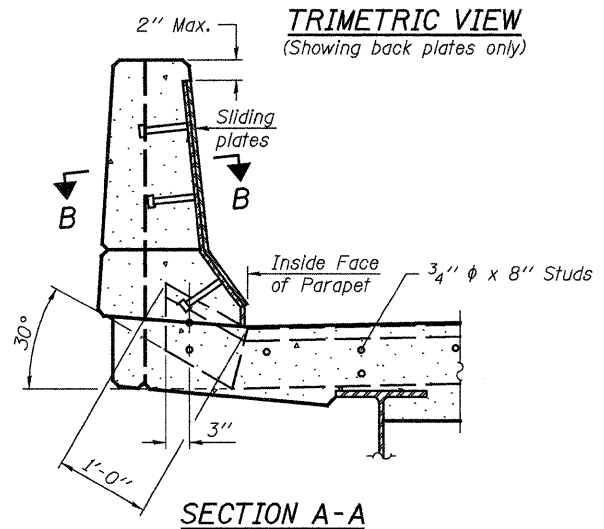
TYPICAL END TREATMENTS

Notes:
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.
The manufacturer's recommended installation methods shall be followed.
The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

LOCKING EDGE RAILS

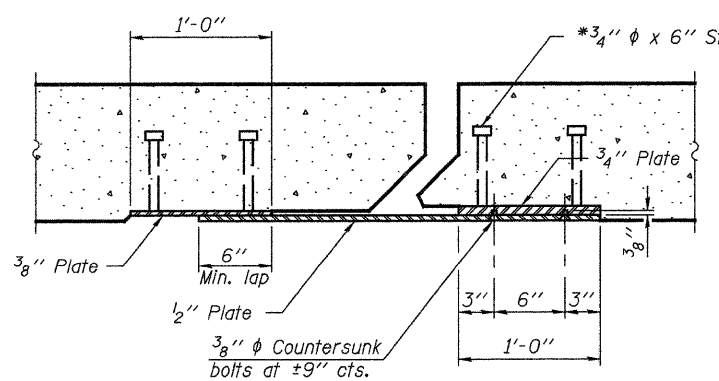


PLAN



SECTION A-A

POINT BLOCK DETAILS (for skews > 30°)



SECTION B-B

BILL OF MATERIAL

| Item | Unit | SB Bridge | NB Bridge |
|----------------------------|------|-----------|-----------|
| Preformed Joint Strip Seal | Foot | 178 | 213 |

PREFORMED JOINT STRIP SEAL
STRUCTURE NO. 058-0101(NB) & 0102(SB)

| SHEET NO. | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------|-------------|---------------------|---------------------------|--------------|-----------|
| 11 OF 26 | 322 | (58-20)RS | MACON | 151 | 76 |
| | | STA. 133+62.02 | CONTRACT NO. 74150 | | |
| | | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | |

JD Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

| | |
|---------------|--------------|
| DESIGNED: JDQ | DRAWN: PTR |
| CHECKED: DCD | CHECKED: DCD |

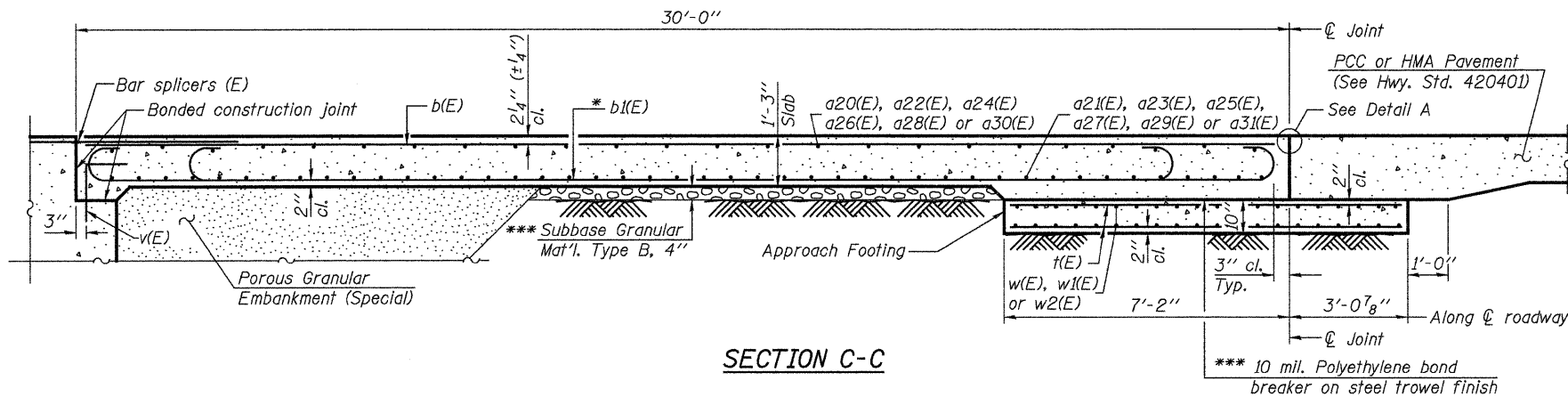
EJ-SSJ 11-1-09

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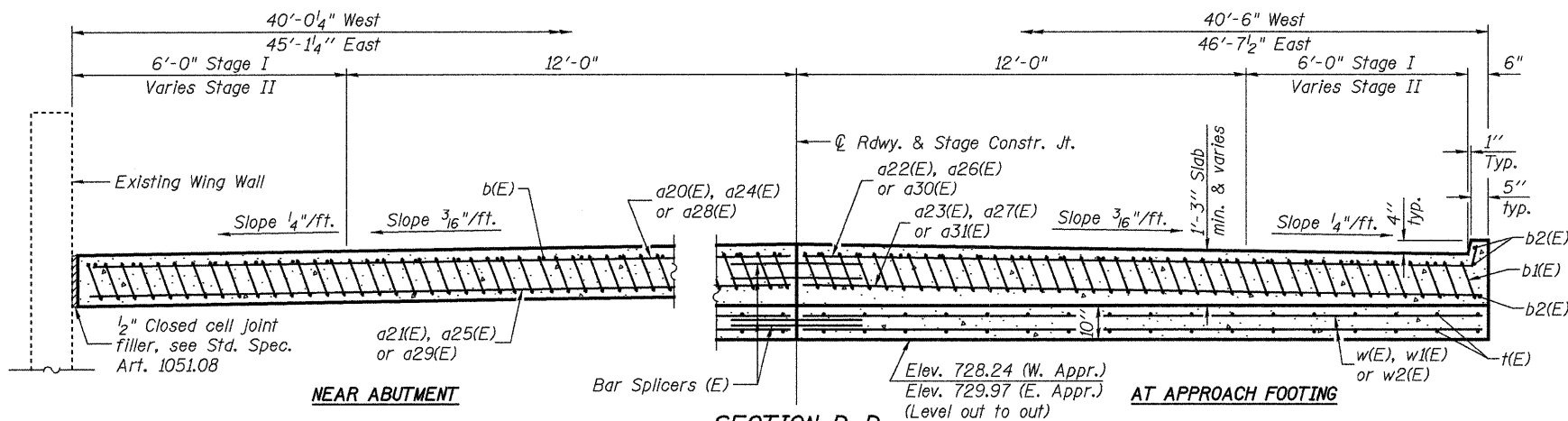
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Notes:

See sheet 12 of 26 for Detail A and View F-F.
Approach slab and parapet concrete shall be paid for as Concrete Superstructure.
Approach footing concrete shall be paid for as Concrete Structures.
Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
For v(E) bar details, see sheet 18 & 19 of 26.
The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.
For bar splicer details, see sheet 25 of 26.
Cost of excavation for approach footing included with Concrete Structures.
For Porous Granular Embankment (Special) and drainage treatment details, see sheet 2 of 26.



SECTION C-C



SECTION D-D

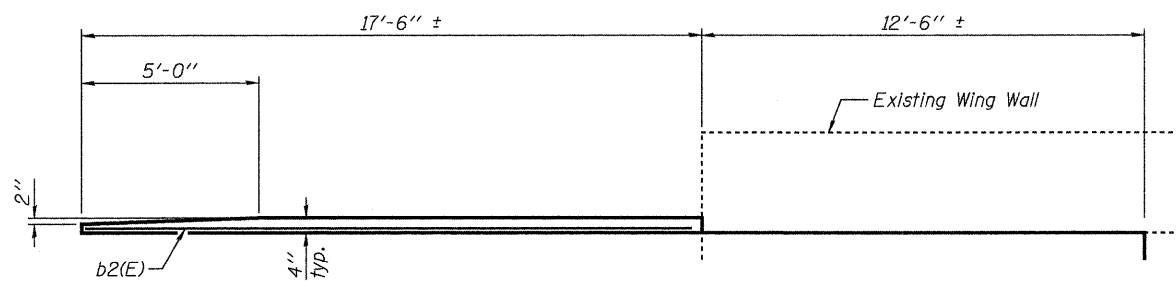
(See Plan for dimensions not shown)

* Tilt #9 b1(E) bars as required to maintain clearance.

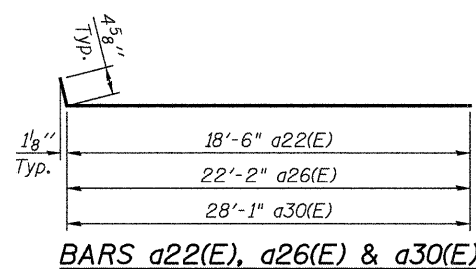
*** Cost included with Concrete Superstructure.

TWO APPROACHES
BILL OF MATERIAL

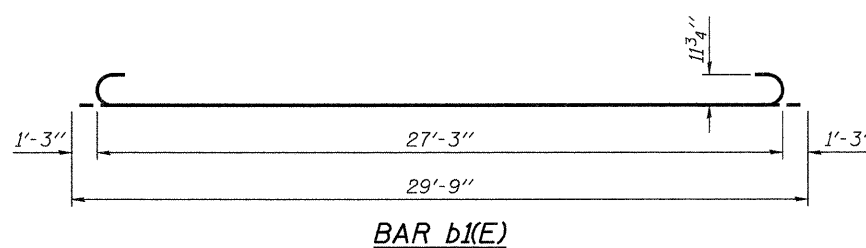
| Bar | No. | Size | Length | Shape |
|----------------------------------|-----|---------|---------|-------|
| a20(E) | 20 | #4 | 18'-2" | — |
| a21(E) | 38 | #5 | 18'-2" | — |
| a22(E) | 28 | #4 | 18'-11" | — |
| a23(E) | 52 | #5 | 18'-8" | — |
| a24(E) | 10 | #4 | 22'-1" | — |
| a25(E) | 19 | #5 | 22'-1" | — |
| a26(E) | 14 | #4 | 22'-6" | — |
| a27(E) | 26 | #5 | 22'-3" | — |
| a28(E) | 10 | #4 | 27'-5" | — |
| a29(E) | 19 | #5 | 27'-5" | — |
| a30(E) | 14 | #4 | 28'-5" | — |
| a31(E) | 26 | #5 | 28'-2" | — |
| b(E) | 68 | #4 | 29'-8" | — |
| b1(E) | 204 | #9 | 29'-9" | — |
| b2(E) | 12 | #4 | 17'-2" | — |
| t(E) | 176 | #4 | 9'-10" | — |
| w(E) | 80 | #5 | 18'-8" | — |
| w1(E) | 40 | #5 | 22'-3" | — |
| w2(E) | 40 | #5 | 28'-5" | — |
| Concrete Superstructure | | Cu. Yd. | 125.8 | |
| Concrete Structures | | Cu. Yd. | 27.5 | |
| Reinforcement Bars, Epoxy Coated | | Pound | 32440 | |



VIEW E-E



BARS a22(E), a26(E) & a30(E)



BAR b1(E)

JD Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

DESIGNED: JDQ DRAWN: P. Ray
CHECKED: DCD CHECKED: DCD

BA-R 11-1-09 (Modified)

(Sheet 2 of 2)
BRIDGE APPROACH SLAB DETAILS (SB)
STRUCTURE NO. 058-0101(NB) & 0102(SB)

| | | | | | |
|---------------------|----------------|---------------------------|--------|-----------------|--------------|
| SHEET 13 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 78 |
| STA. 133+62.02 | | CONTRACT NO. 74150 | | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | |

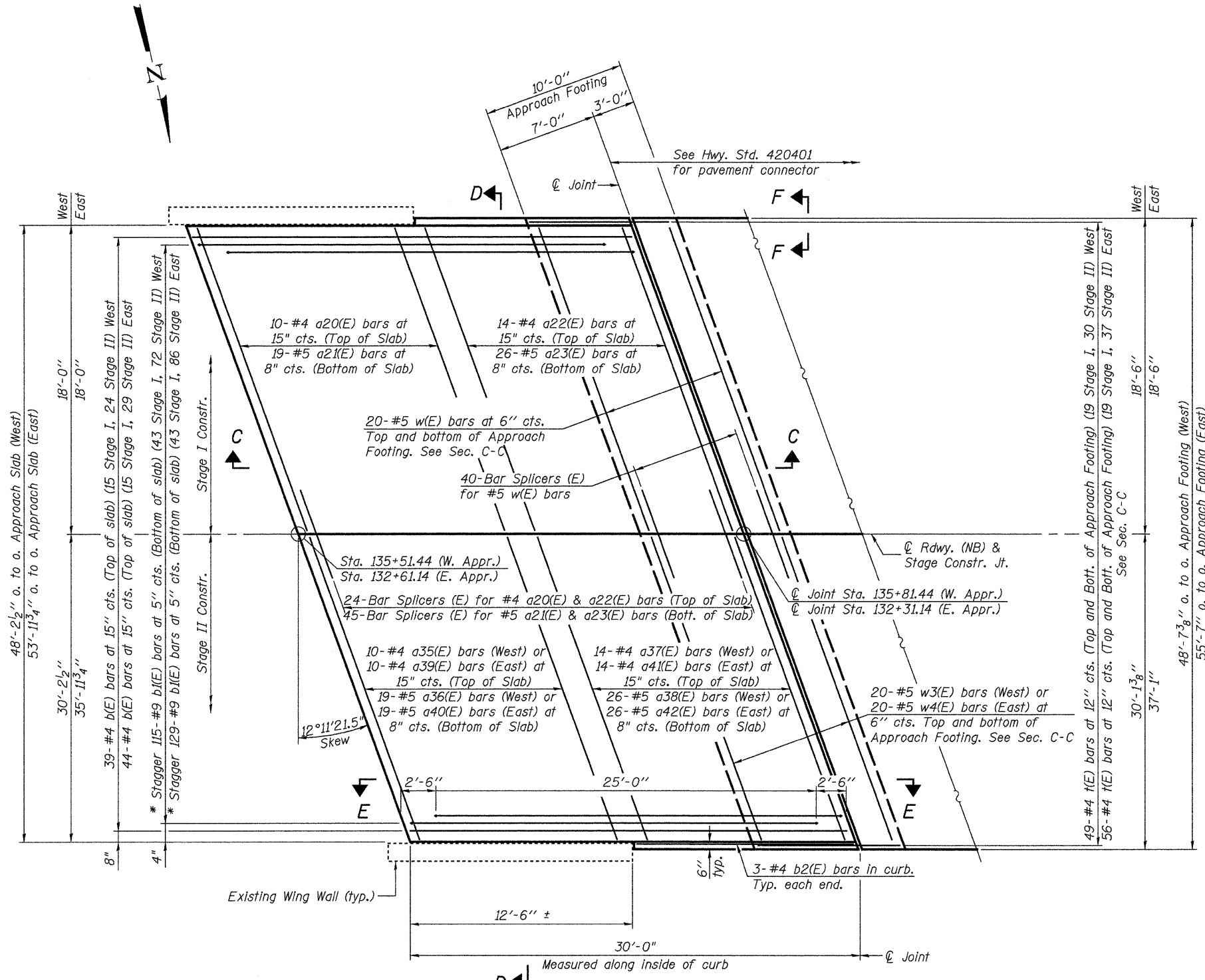
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USER: DCD

DATE: 04/09/2010 12:21:38

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Notes:
See sheet 15 of 26 for Sections C-C & D-D and View E-E.
a20(E) thru a23(E) & a35(E) thru a42(E) bar spacings measured along \varnothing Rdwy.

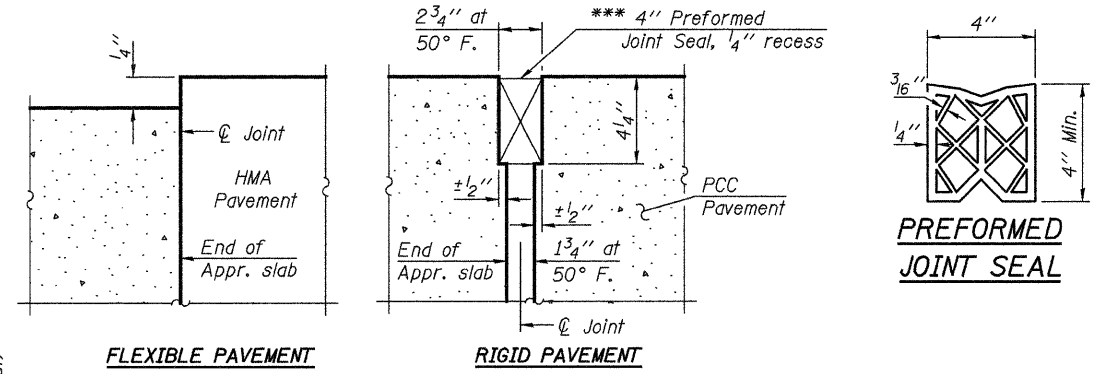


PLAN

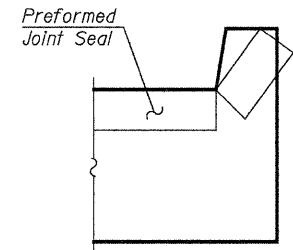
(West Approach shown, East Approach similar)

* Tilt #9 b1(E) bars as required to maintain clearance.

*** Cost included with Concrete Superstructure.

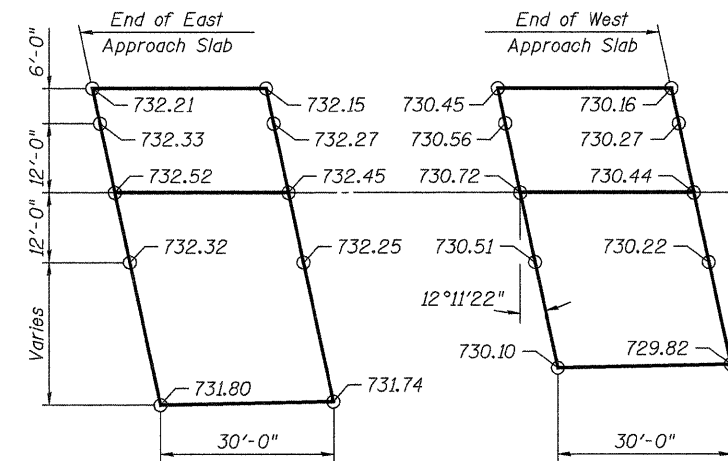


DETAIL A



VIEW F-F

Angle Preformed Joint Seal at 45° at curbs when req'd for drainage.



TOP OF APPROACH SLAB ELEVATIONS

NOTE:
Proposed elevations are based on the original 1975 plan elevations, increased by 0.02' (1/4") to account for the proposed deck overlay. The top of the existing pavement at the abutment backwall can be used as a temporary benchmark by assuming the following elevations:
Back of East Abut. original PG Elev. 732.44
Back of West Abut. original PG Elev. 730.71

| | |
|---------------|--------------|
| | |
| DESIGNED: JDQ | DRAWN: SJS |
| CHECKED: DCD | CHECKED: DCD |

BA-R 11-1-09 (Modified)

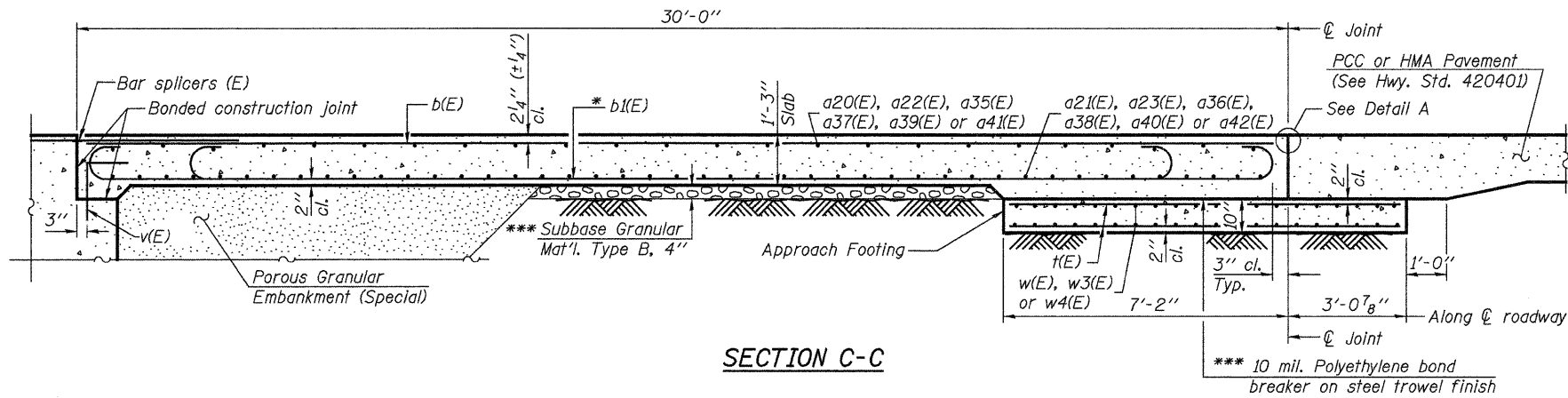
(Sheet 1 of 2)
BRIDGE APPROACH SLAB DETAILS (NB)
STRUCTURE NO. 058-0101(NB) & 0102(SB)

| | | | | | |
|---------------------|-------------|---------------------------|--------------------|--------------|-----------|
| SHEET 14 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | 158-20/RS | MACON | 151 | 79 |
| STA. 133+62.02 | | | CONTRACT NO. 74150 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | |

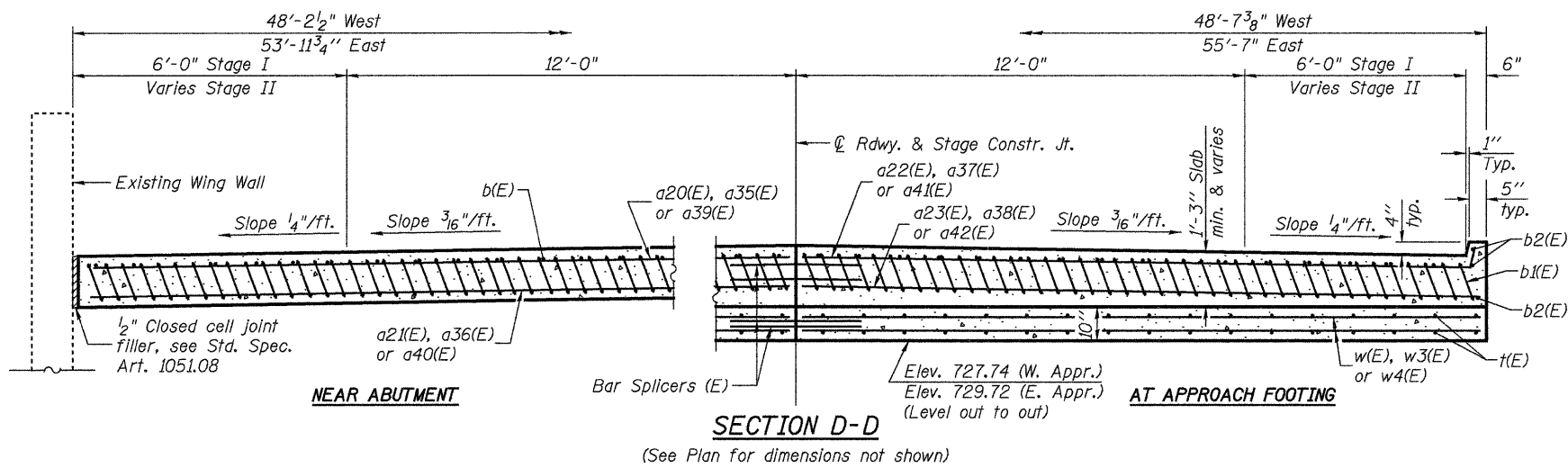
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Notes:

See sheet 14 of 26 for Detail A and View F-F.
Approach slab and parapet concrete shall be paid for as Concrete Superstructure.
Approach footing concrete shall be paid for as Concrete Structures.
Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
For v(E) bar details, see sheet 21 & 22 of 26.
The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.
For bar splicer details, see sheet 25 of 26.
Cost of excavation for approach footing included with Concrete Structures.
For Porous Granular Embankment (Special) and drainage treatment details, see sheet 2 of 26.



SECTION C-C

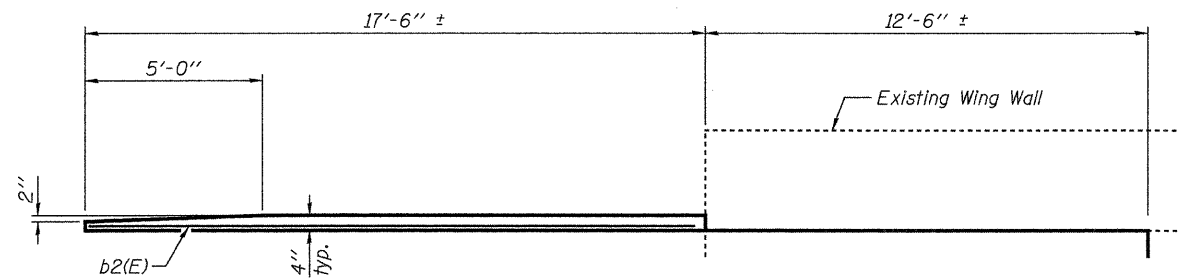


SECTION D-D
(See Plan for dimensions not shown)

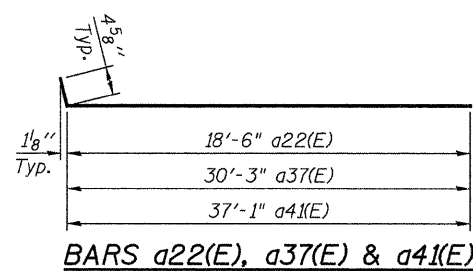
* Tilt #9 b1(E) bars as required to maintain clearance.
*** Cost included with Concrete Superstructure.

TWO APPROACHES
BILL OF MATERIAL

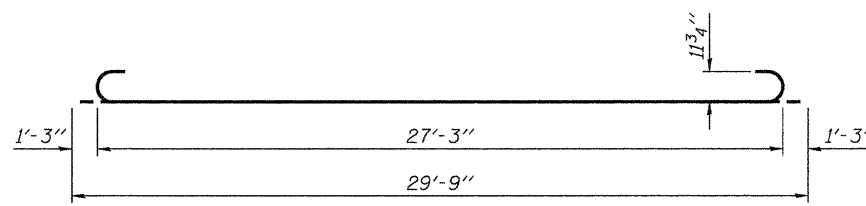
| Bar | No. | Size | Length | Shape |
|----------------------------------|-----|---------|---------|-------|
| a20(E) | 20 | #4 | 18'-2" | — |
| a21(E) | 38 | #5 | 18'-2" | — |
| a22(E) | 28 | #4 | 18'-11" | — |
| a23(E) | 52 | #5 | 18'-8" | — |
| a35(E) | 10 | #4 | 30'-4" | — |
| a36(E) | 19 | #5 | 30'-4" | — |
| a37(E) | 14 | #4 | 30'-8" | — |
| a38(E) | 26 | #5 | 30'-5" | — |
| a39(E) | 10 | #4 | 36'-6" | — |
| a40(E) | 19 | #5 | 36'-6" | — |
| a41(E) | 14 | #4 | 37'-6" | — |
| a42(E) | 26 | #5 | 37'-3" | — |
| b(E) | 83 | #4 | 29'-8" | — |
| b1(E) | 244 | #9 | 29'-9" | — |
| b2(E) | 12 | #4 | 17'-2" | — |
| t(E) | 210 | #4 | 9'-10" | — |
| w(E) | 80 | #5 | 18'-8" | — |
| w3(E) | 40 | #5 | 30'-5" | — |
| w4(E) | 40 | #5 | 37'-6" | — |
| Concrete Superstructure | | Cu. Yd. | 151.5 | |
| Concrete Structures | | Cu. Yd. | 32.9 | |
| Reinforcement Bars, Epoxy Coated | | Pound | 38810 | |



VIEW E-E



BARS a22(E), a37(E) & a41(E)



BAR b1(E)

JD Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

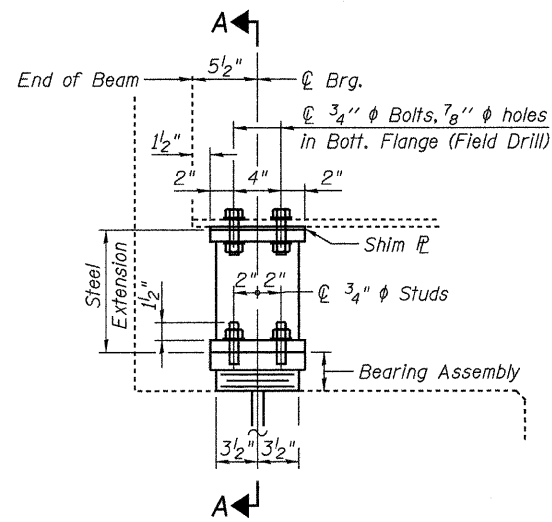
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| DESIGNED: JDQ | DRAWN: SJS |
| CHECKED: DCD | CHECKED: DCD |

BA-R 11-1-09 (Modified)

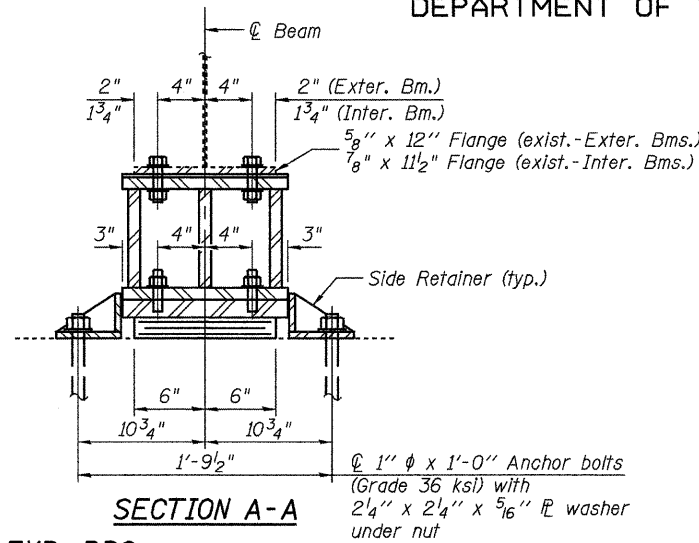
(Sheet 2 of 2)
BRIDGE APPROACH SLAB DETAILS (NB)
STRUCTURE NO. 058-0101(NB) & 0102(SB)

| | | | | | |
|-------------------|----------------|---------------------|---------------------------|--------------|-----------|
| SHEET 15 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 80 |
| | | STA. 133+62.02 | CONTRACT NO. | 74150 | |
| | | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

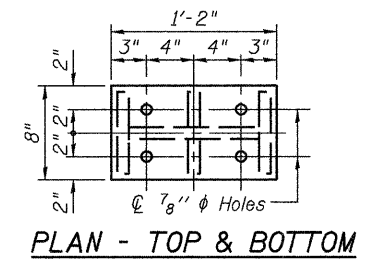


ELEVATION

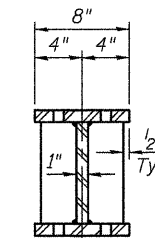


SECTION A-A

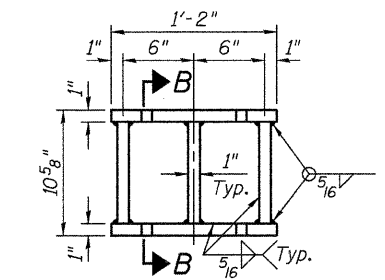
TYPE I ELASTOMERIC EXP. BRG.



PLAN - TOP & BOTTOM

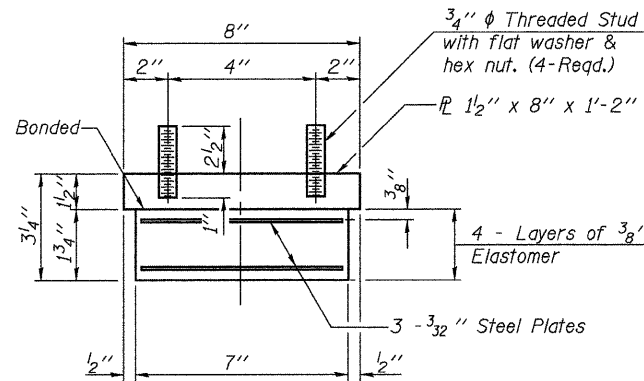


SECTION B-B



ELEVATION

STEEL EXTENSION

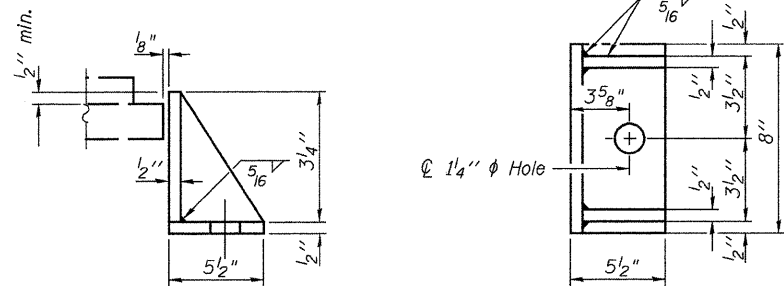


BEARING ASSEMBLY

Note:
Shim plates shall not be placed
under Bearing Assembly.

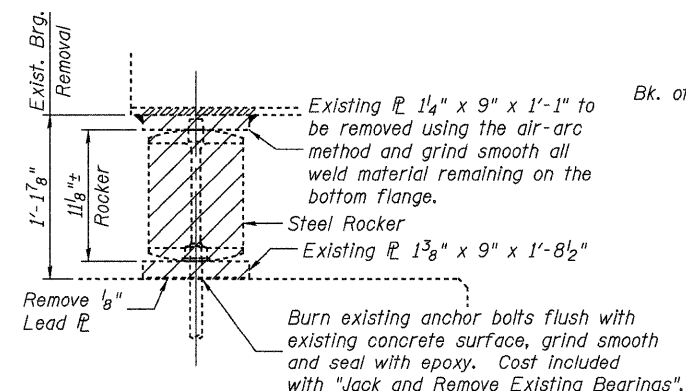
Notes:
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
Anchor bolts for side retainers shall be installed in holes drilled after members are in place.
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
Side retainers and other steel members required for the bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I.

Notes:
Existing expansion bearings shall be removed and replaced according to the plans. Jacking shall be according to the Special Provisions for "JACK AND REMOVE EXISTING BEARINGS". If web stiffeners are not present directly over the Jack location, hardwood timbers shall be installed tightly between top and bottom flanges to prevent rotation.
The abutment bearings shall be in place and the jacks lowered before the new concrete deck is poured at the abutments.
Diaphragm removal and replacement may be required to facilitate drilling holes. Cost shall be included with Furnishing and Erecting Structural Steel.
Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Existing bearing dimensions shown are copied from the original plans.
Two 1/8 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.
New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.
The structural steel bearing plates for the expansion bearings shall conform to the requirements of AASHTO M 270 Grade 36 (min.).

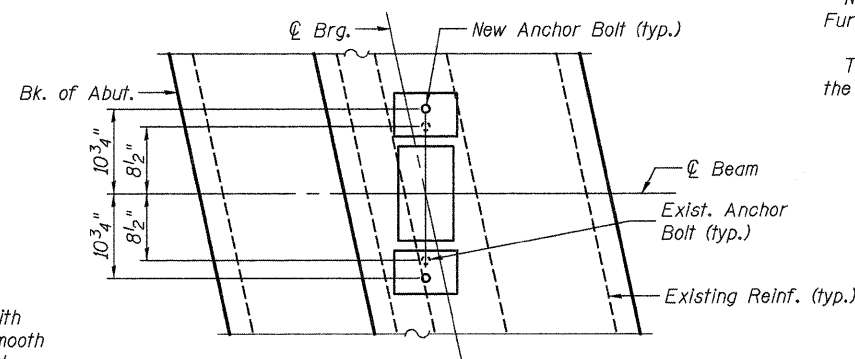


SIDE RETAINER

Equivalent rolled angle with stiffeners
will be allowed in lieu of welded plates.



EXISTING BEARING REMOVAL



ANCHOR BOLT LAYOUT

BILL OF MATERIAL

| Item | Unit | SB Bridge | WB Bridge |
|--|-------|-----------|-----------|
| Jack and Remove Existing Bearings | Each | 7 | 8 |
| Elastomeric Bearing Assembly Type I | Each | 7 | 8 |
| Furnishing and Erecting Structural Steel | Pound | 1100 | 1260 |
| Anchor Bolts, 1" | Each | 14 | 16 |

| INTERIOR GIRDER REACTION TABLE | | |
|--------------------------------|--------|----------|
| | | W. Abut. |
| R (DL) | (K) | 29.1 |
| R (LL) | (K) | 36.0 |
| R (Imp) | (K) | 10.5 |
| R (Total) | (K) | 75.6 |
| Minimum Jack Capacity | (Tons) | 40 |

BEARINGS - WEST ABUTMENT
STRUCTURE NO. 058-010(NB) & 0102(SB)

| SHEET 16 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|-------------|---------------------------|--------------------|--------------|-----------|
| | 322 | (58-20)RS | MACON | 151 | 81 |
| STA. 133+62.02 | | | CONTRACT NO. 74150 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | |

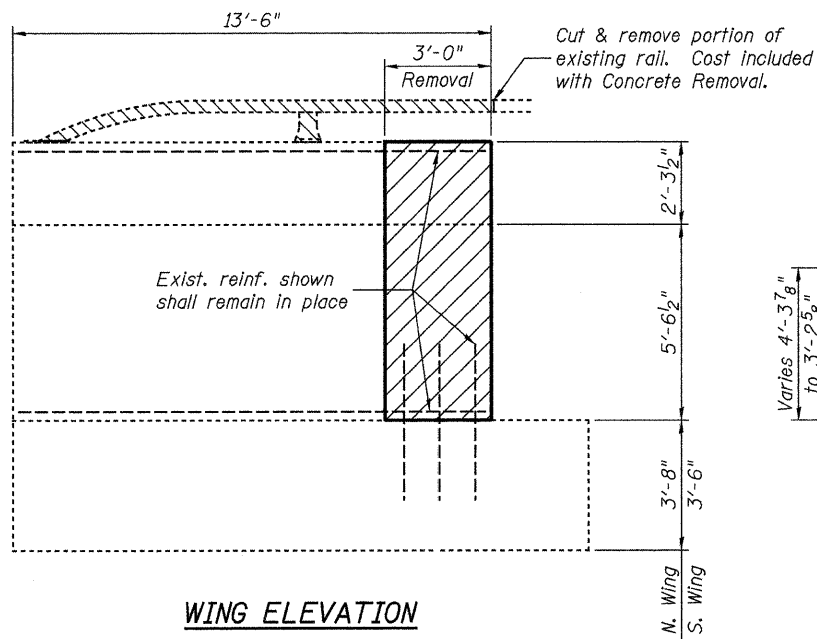
JD Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

| | |
|---------------|--------------|
| DESIGNED: JDQ | DRAWN: PTR |
| CHECKED: DCD | CHECKED: DCD |

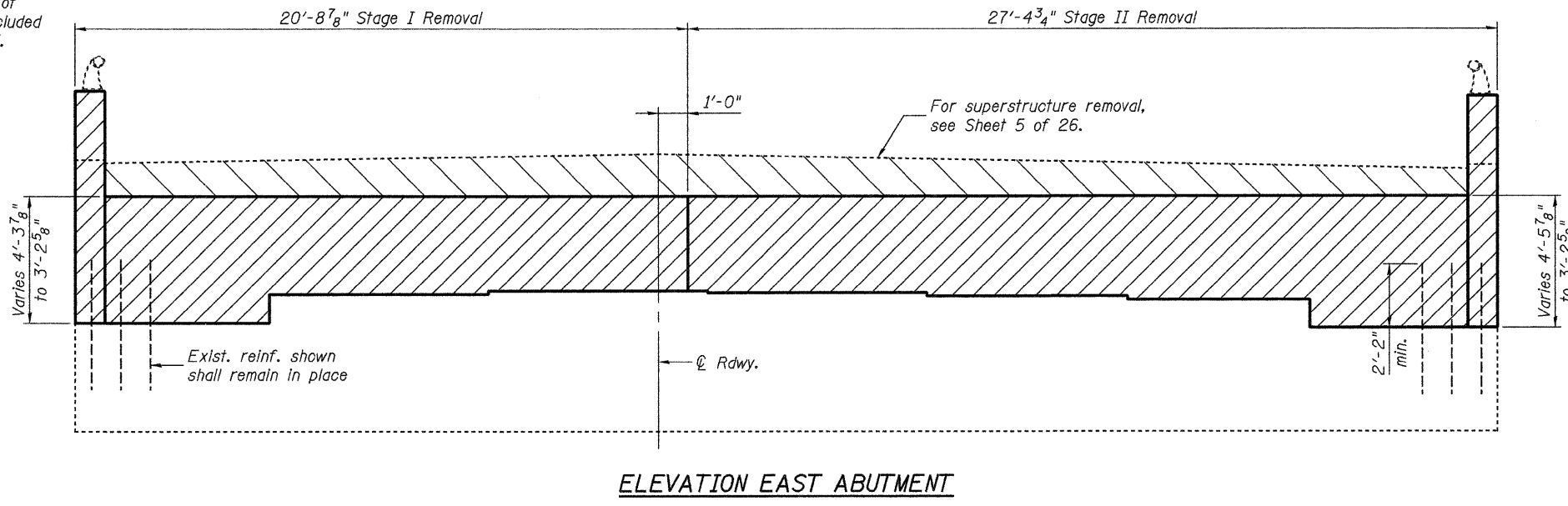
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11-1-09 (Modified)

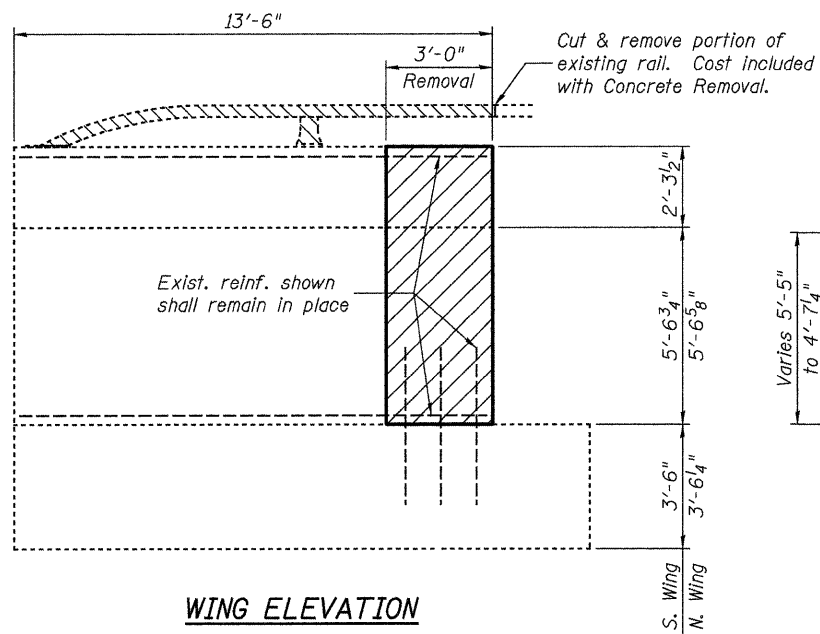
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



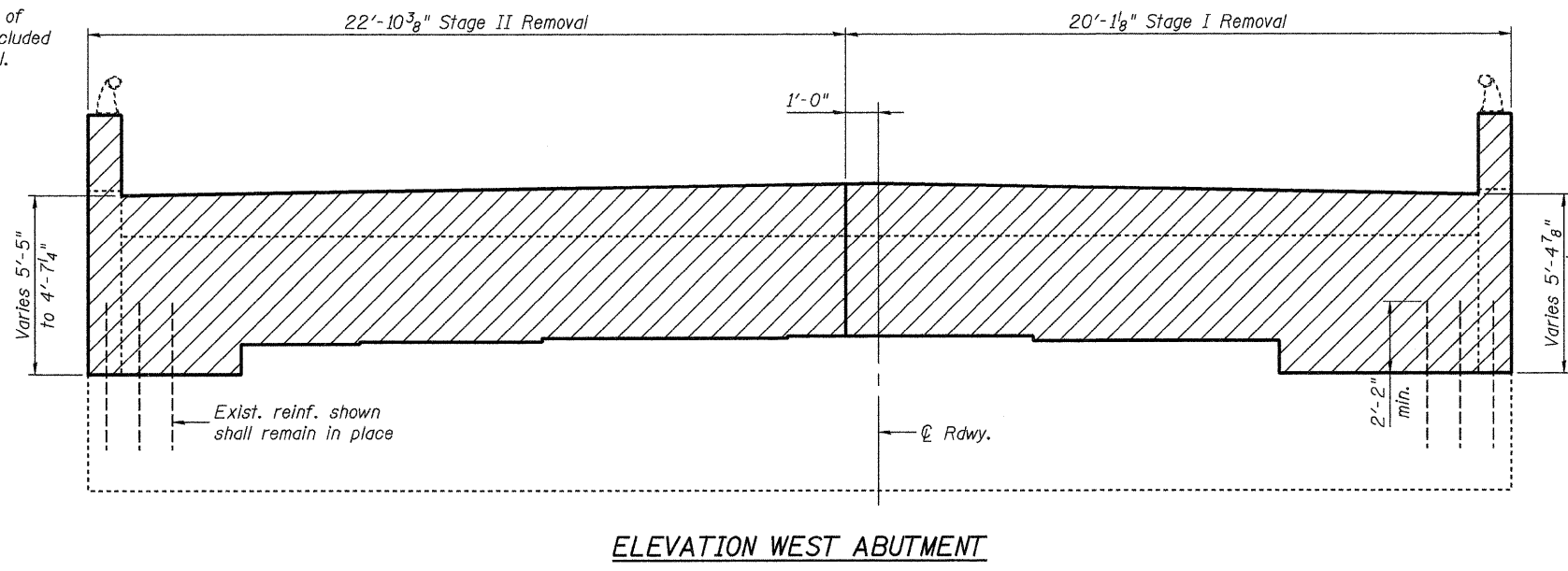
WING ELEVATION



ELEVATION EAST ABUTMENT



WING ELEVATION



ELEVATION WEST ABUTMENT

Indicates Limits of Concrete Removal.

Note:
Quantities for Concrete Removal are included on sheets 18 and 19 of 26.

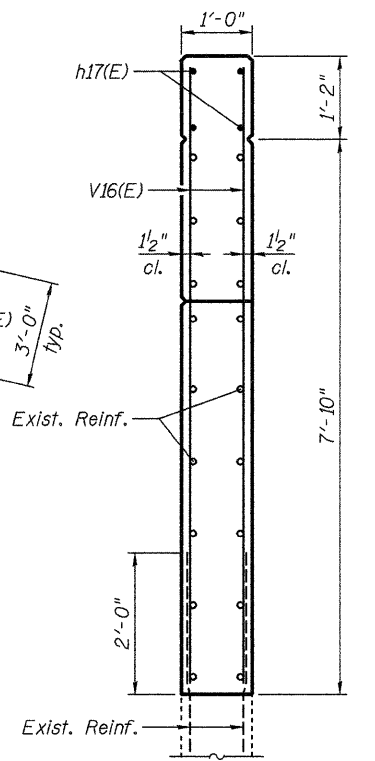
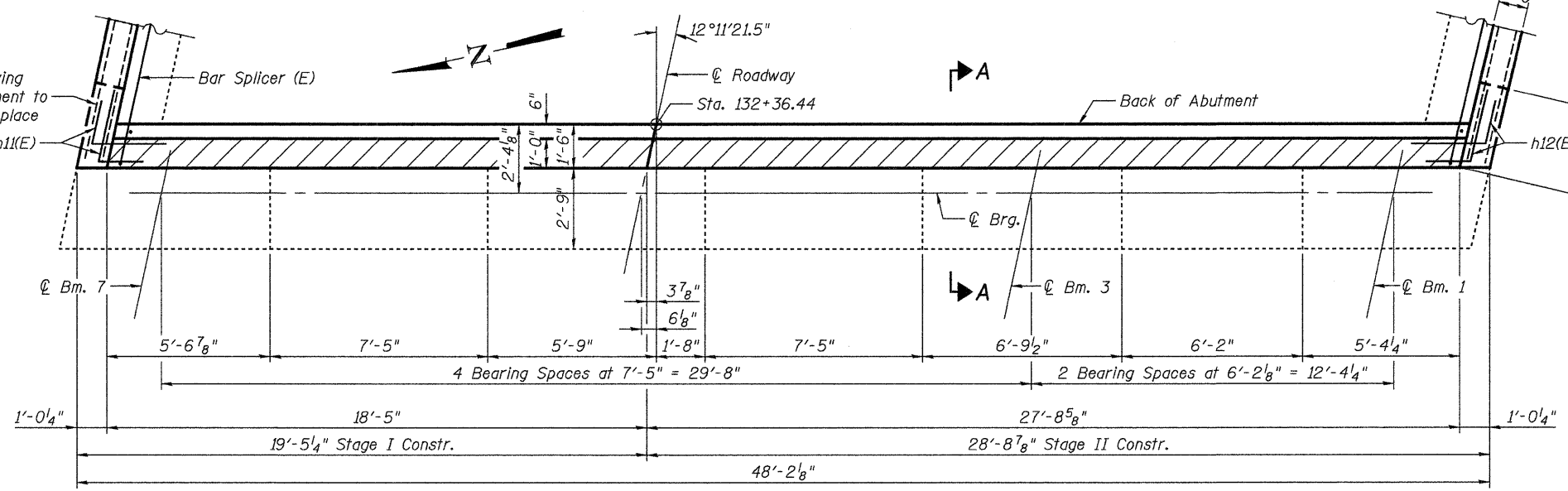
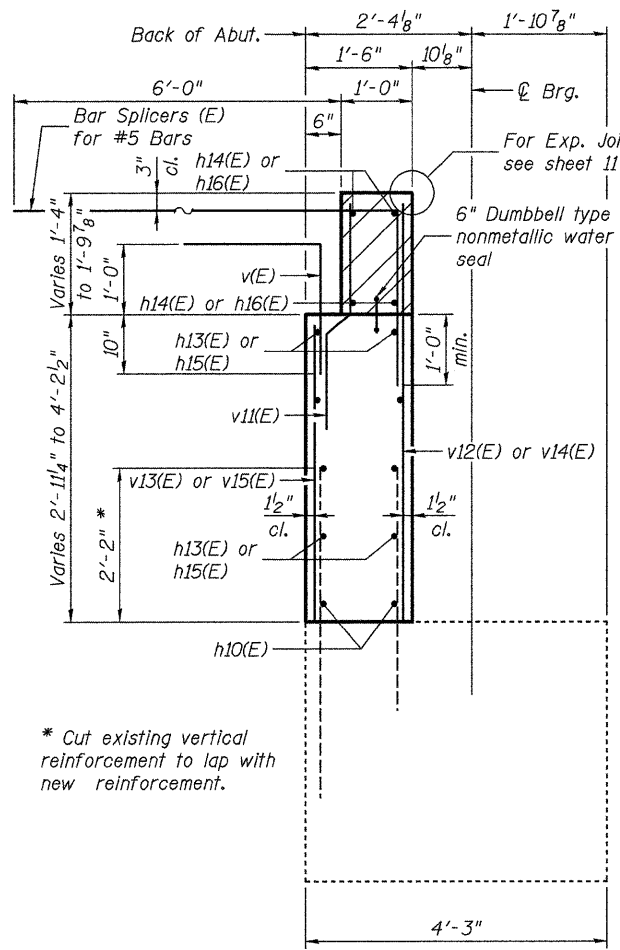
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|---|---------------|
| Johnson, Depp & Quisenberry CONSULTING ENGINEERS Springfield, Illinois | |
| DESIGNED: JDQ | DRAWN: P. Ray |
| CHECKED: DCD | CHECKED: DCD |

ABUTMENT - REMOVAL (SB)
STRUCTURE NO. 058-0101(NB) & 0102(SB)

| | | | | | |
|-------------------|----------------|---------------------|---------------------------|-----------------|--------------|
| SHEET 17 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 82 |
| | | STA. 133+62.02 | CONTRACT NO. 74150 | | |
| | | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | |

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



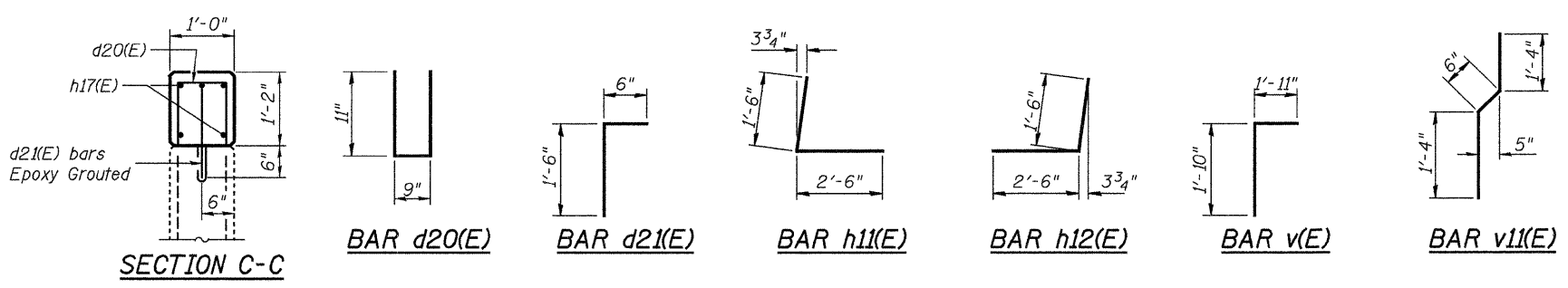
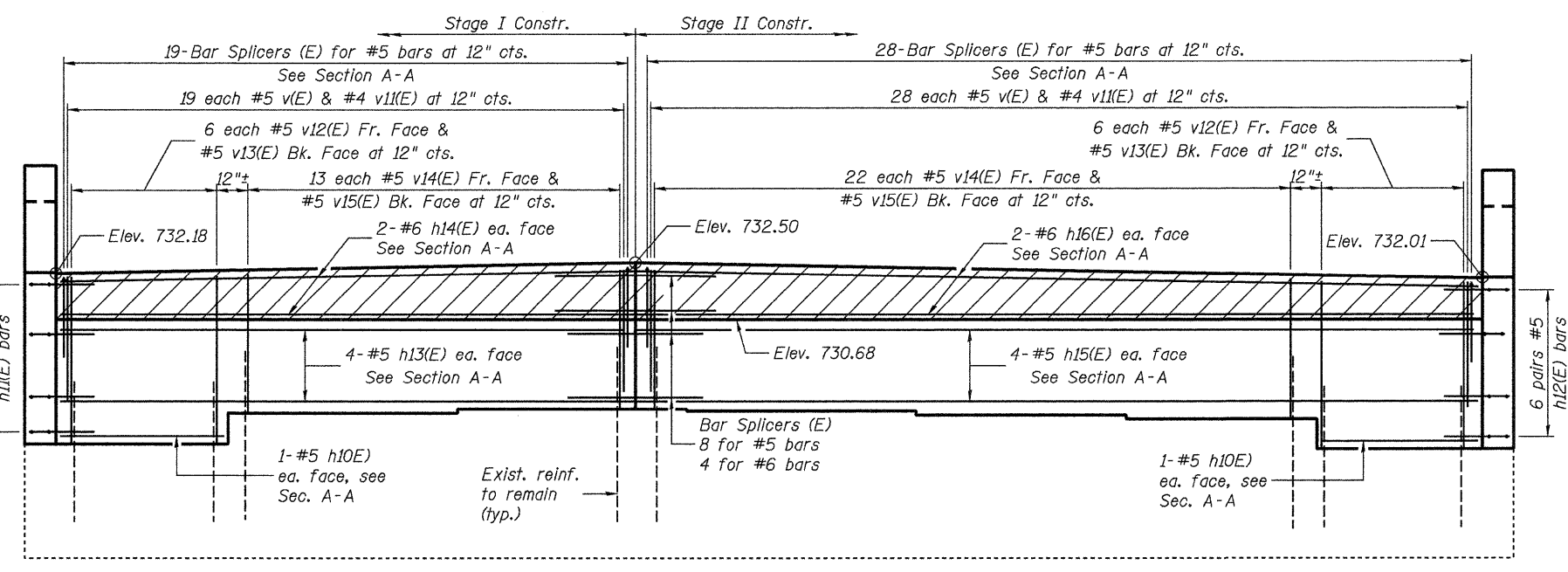
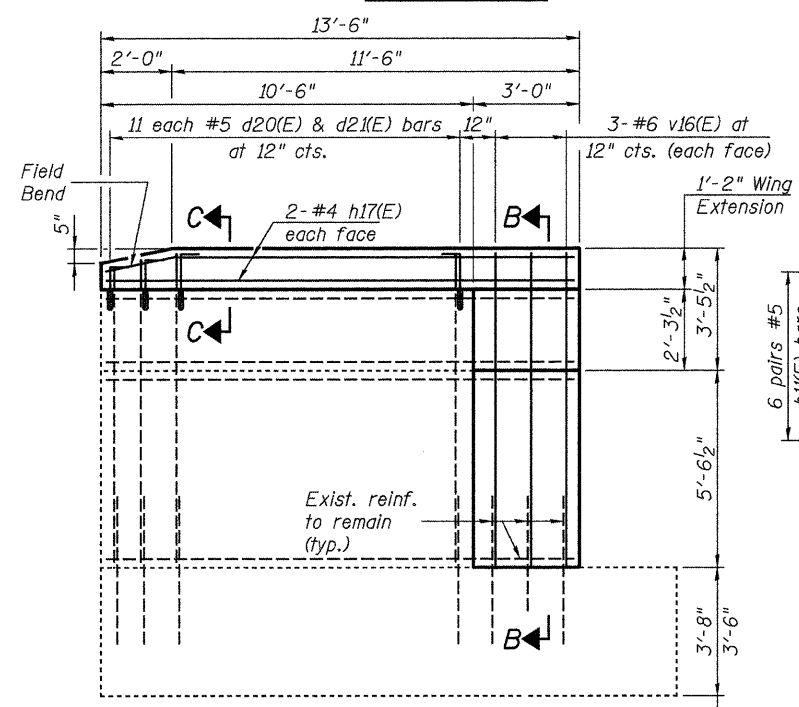
* Cut existing vertical reinforcement to lap with new reinforcement.

Hatched area to be poured after superstructure false work has been removed. Quantity of concrete included with Concrete Superstructure.

ABUTMENT
BILL OF MATERIAL

| Bar | No. | Size | Length | Shape |
|----------------------------------|-----|------|---------|-------|
| d20(E) | 22 | #5 | 2'-7" | □ |
| d21(E) | 22 | #5 | 2'-0" | └ |
| h10(E) | 4 | #5 | 6'-0" | — |
| h11(E) | 12 | #5 | 4'-0" | └ |
| h12(E) | 12 | #5 | 4'-0" | └ |
| h13(E) | 8 | #5 | 19'-0" | — |
| h14(E) | 4 | #6 | 18'-0" | — |
| h15(E) | 8 | #5 | 28'-4" | — |
| h16(E) | 4 | #6 | 27'-4" | — |
| h17(E) | 8 | #4 | 13'-0" | — |
| v(E) | 47 | #5 | 3'-9" | └ |
| v11(E) | 47 | #4 | 3'-2" | └ |
| v12(E) | 12 | #5 | 5'-4" | — |
| v13(E) | 12 | #5 | 3'-10" | — |
| v14(E) | 35 | #5 | 4'-5" | — |
| v15(E) | 35 | #5 | 2'-9" | — |
| v16(E) | 12 | #6 | 8'-10" | — |
| Structure Excavation | | | Cu. Yd. | 89 |
| Concrete Removal | | | Cu. Yd. | 11.5 |
| Concrete Structures | | | Cu. Yd. | 12.1 |
| Concr. Superstructure | | | Cu. Yd. | 2.8 |
| Reinforcement Bars, Epoxy Coated | | | Pound | 1790 |

For details of Bar Splicers, see sheet 25 of 26.



Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

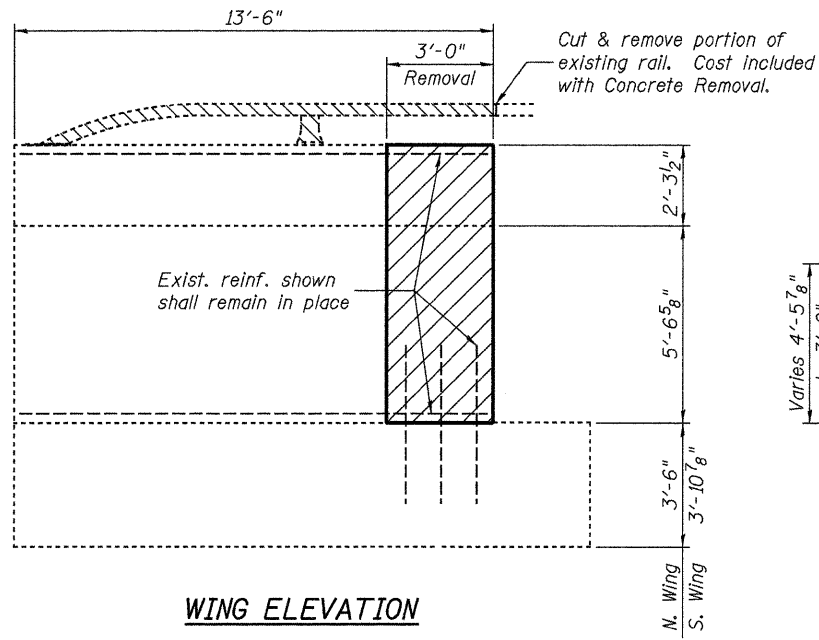
DESIGNED: JDQ DRAWN: P. Ray
CHECKED: DCD CHECKED: DCD

EAST ABUTMENT (SB)
STRUCTURE NO. 058-0101(NB) & 0102(SB)

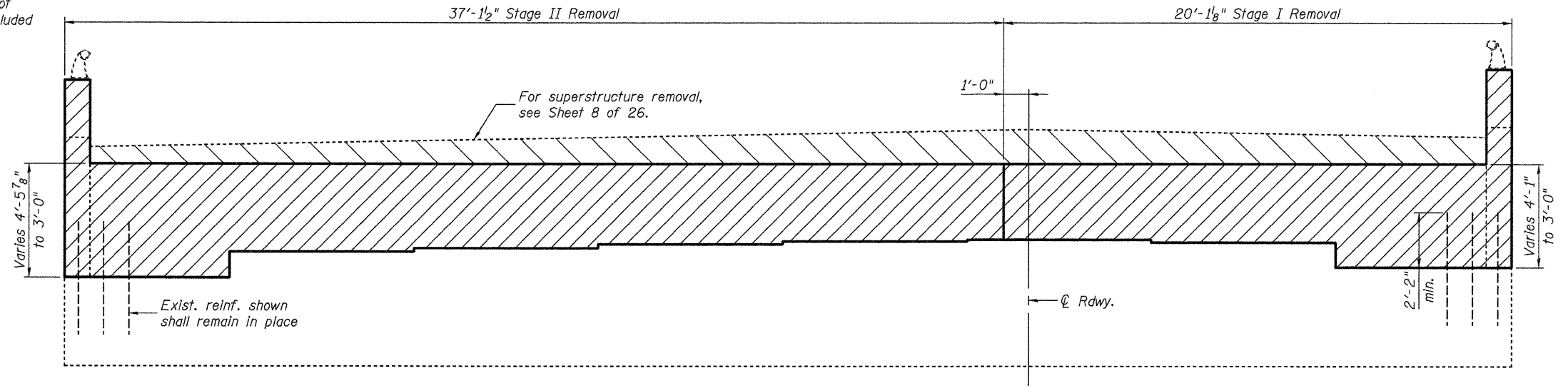
| | | | | | |
|----------------|---------------------|-------------------|--------------------|------------------|--------------|
| SHEET 18 OF 26 | F.A.P. RTE. 322 | SECTION (58-20)RS | COUNTY MACON | TOTAL SHEETS 151 | SHEET NO. 83 |
| | | STA. 133+62.02 | CONTRACT NO. 74150 | | |
| | FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

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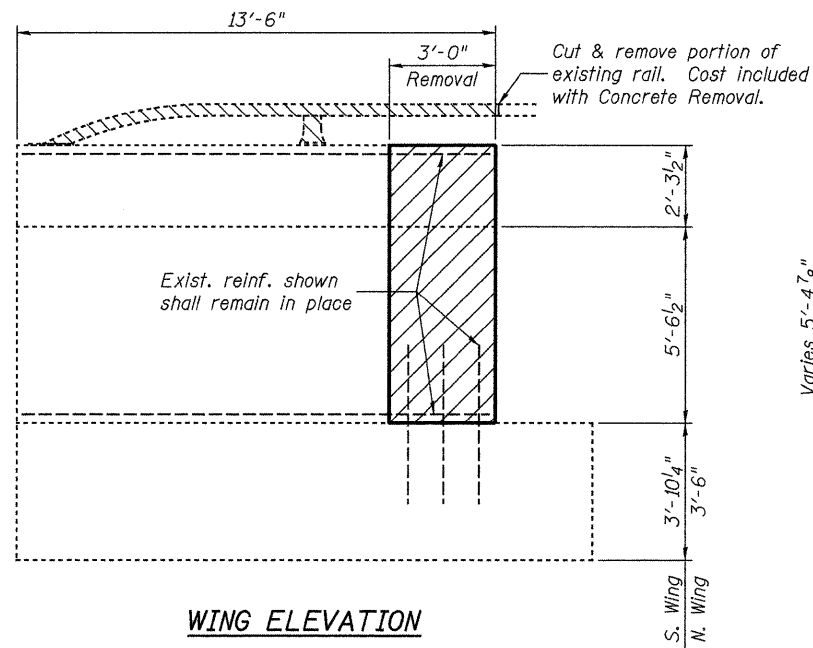
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



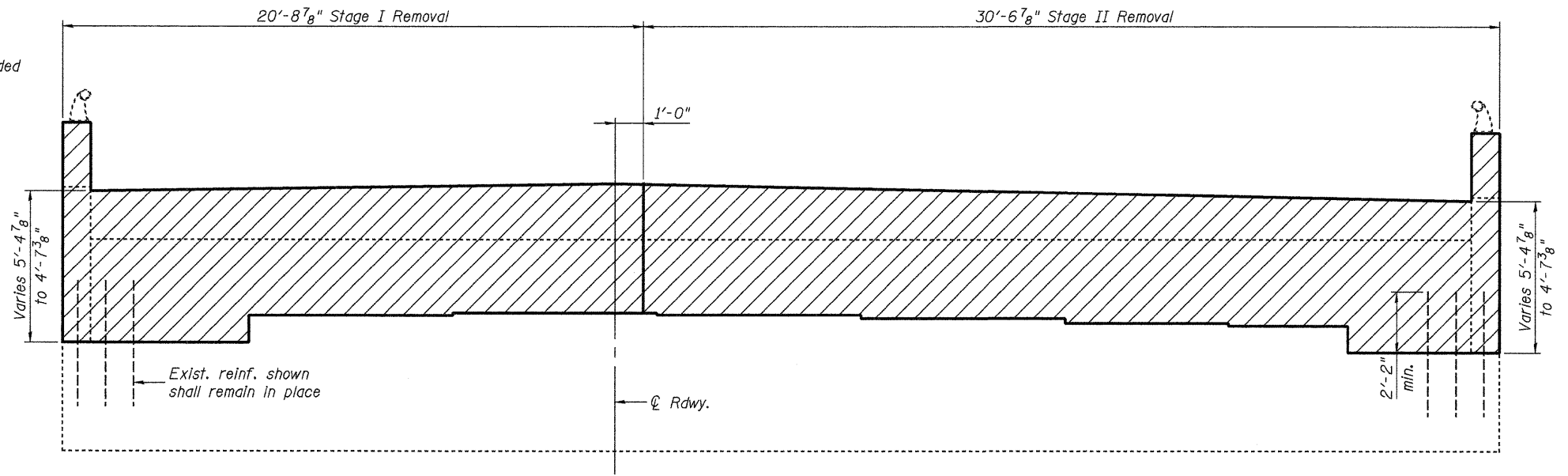
WING ELEVATION




ELEVATION EAST ABUTMENT



WING ELEVATION



ELEVATION WEST ABUTMENT

 Indicates Limits of Concrete Removal.

Note:
Quantities for Concrete Removal are included on sheets 21 and 22 of 26.

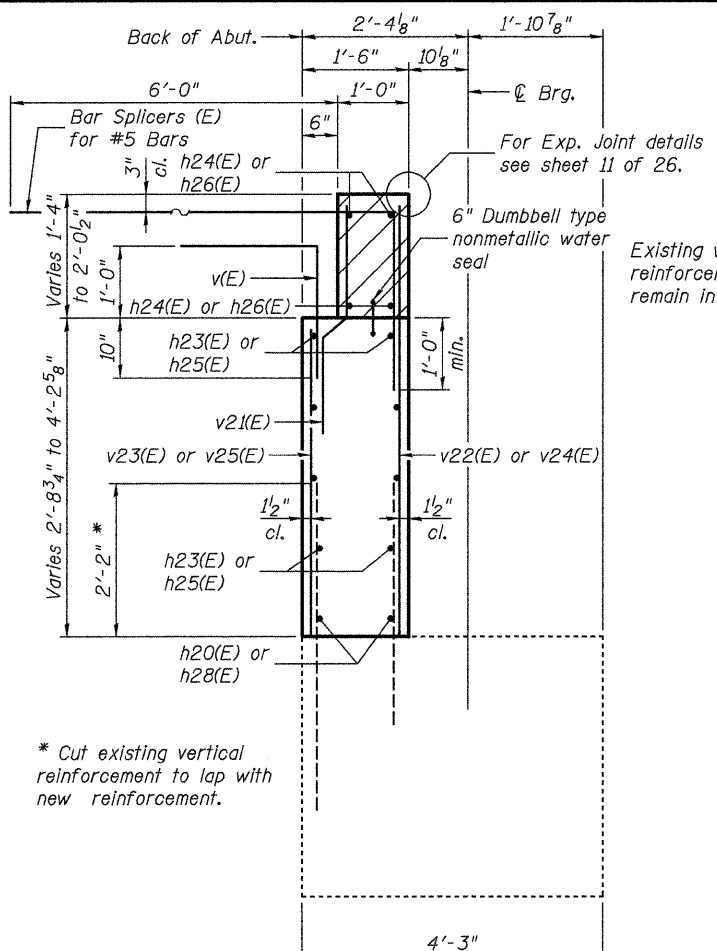
 Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

| | |
|---------------|--------------|
| DESIGNED: JDQ | DRAWN: SJS |
| CHECKED: DCD | CHECKED: DCD |

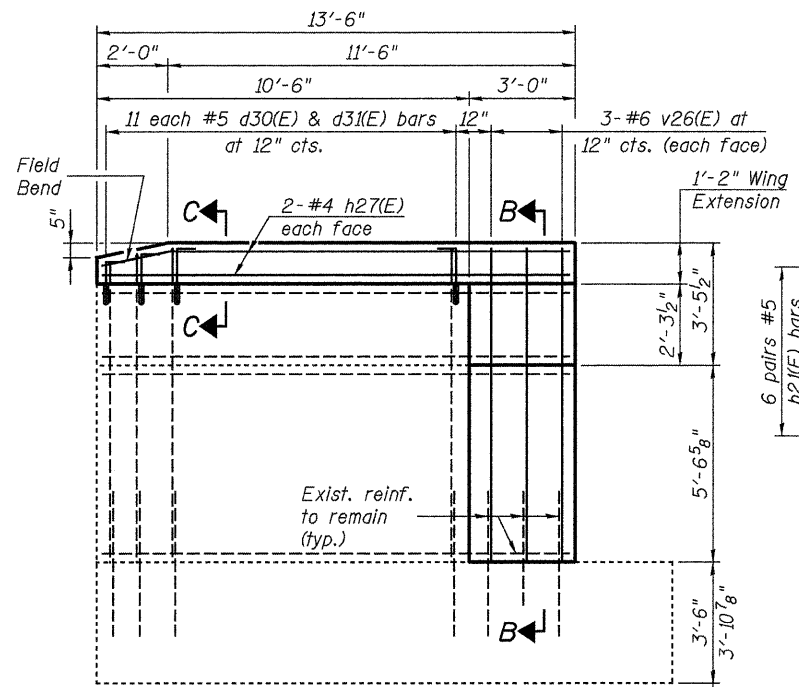
ABUTMENT - REMOVAL (NB)
STRUCTURE NO. 058-0101(NB) & 0102(SB)

| | | | | | |
|-------------------|----------------|---------------------|---------------------------|-----------------|--------------|
| SHEET 20 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 85 |
| | | STA. 133+62.02 | CONTRACT NO. 74150 | | |
| | | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | |

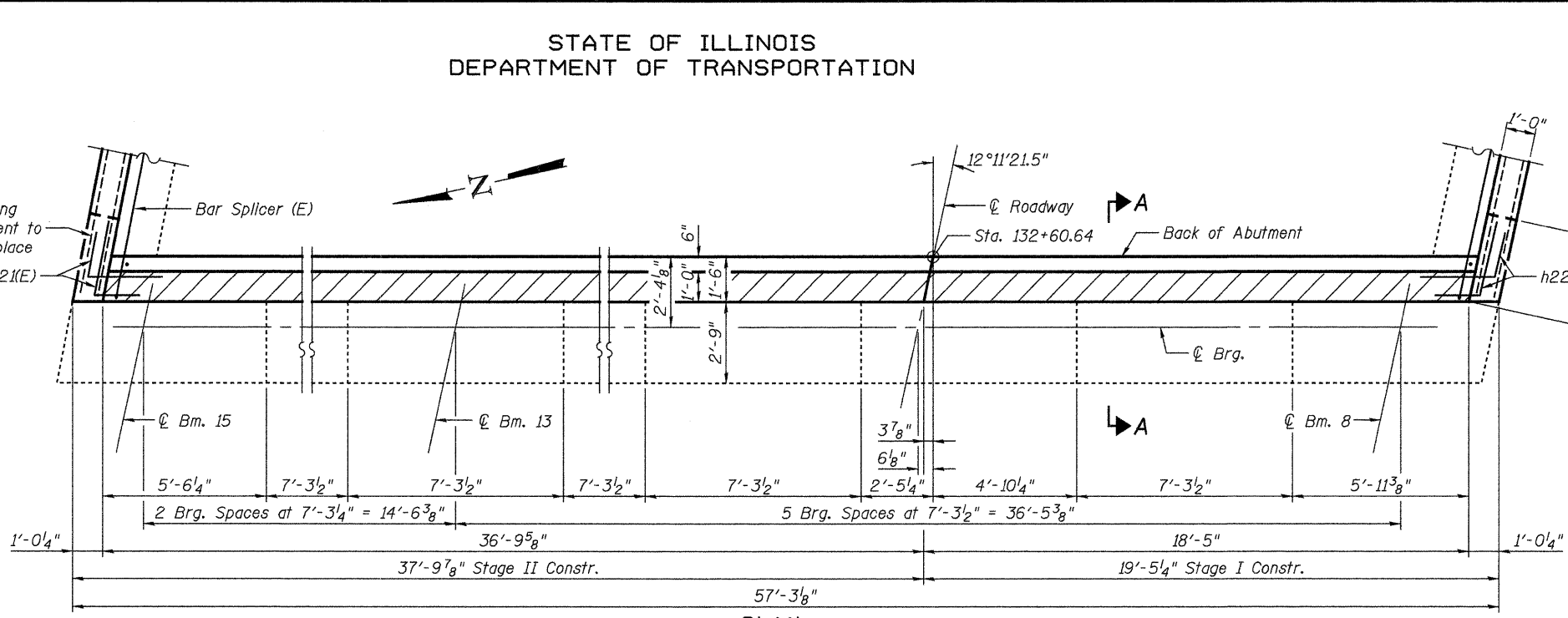
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



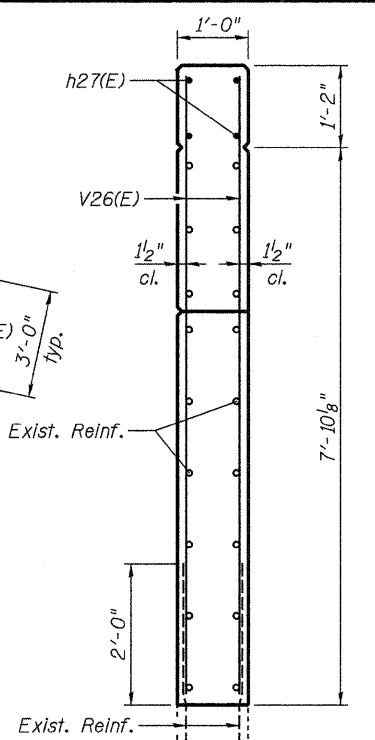
SECTION A-A



WING ELEVATION



PLAN



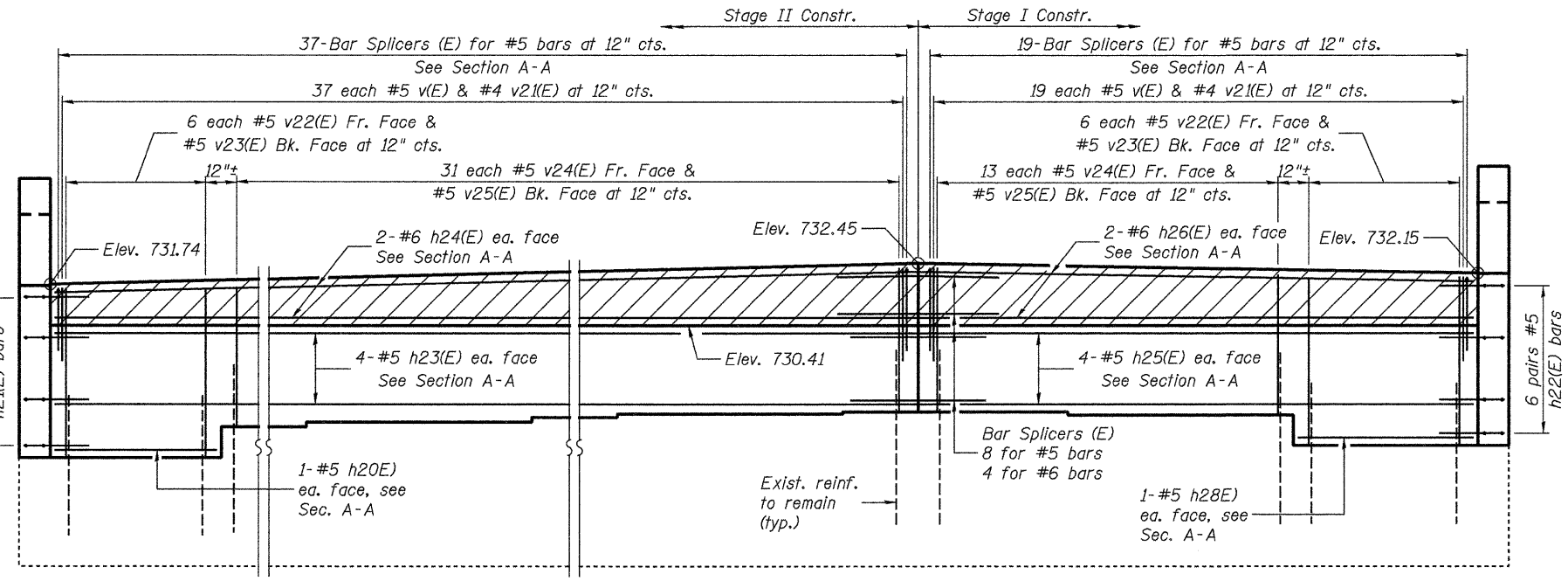
SECTION B-B

Hatched area to be poured after superstructure false work has been removed. Quantity of concrete included with Concrete Superstructure.

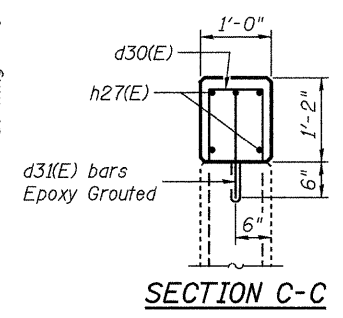
ABUTMENT
BILL OF MATERIAL

| Bar | No. | Size | Length | Shape |
|----------------------------------|-----|------|---------|-------|
| d30(E) | 22 | #5 | 2'-7" | □ |
| d31(E) | 22 | #5 | 2'-0" | └ |
| h20(E) | 2 | #5 | 5'-11" | — |
| h21(E) | 12 | #5 | 4'-0" | └ |
| h22(E) | 12 | #5 | 4'-0" | └ |
| h23(E) | 8 | #5 | 37'-6" | — |
| h24(E) | 4 | #6 | 36'-6" | — |
| h25(E) | 8 | #5 | 19'-0" | — |
| h26(E) | 4 | #6 | 18'-0" | — |
| h27(E) | 8 | #4 | 13'-0" | — |
| h28(E) | 2 | #5 | 6'-8" | — |
| v(E) | 56 | #5 | 3'-9" | └ |
| v21(E) | 56 | #4 | 3'-2" | └ |
| v22(E) | 12 | #5 | 5'-4" | — |
| v23(E) | 12 | #5 | 3'-8" | — |
| v24(E) | 44 | #5 | 4'-6" | — |
| v25(E) | 44 | #5 | 2'-6" | — |
| v26(E) | 12 | #6 | 8'-10" | — |
| Structure Excavation | | | Cu. Yd. | 111 |
| Concrete Removal | | | Cu. Yd. | 13.0 |
| Concrete Structures | | | Cu. Yd. | 13.2 |
| Concr. Superstructure | | | Cu. Yd. | 3.5 |
| Reinforcement Bars, Epoxy Coated | | | Pound | 2030 |

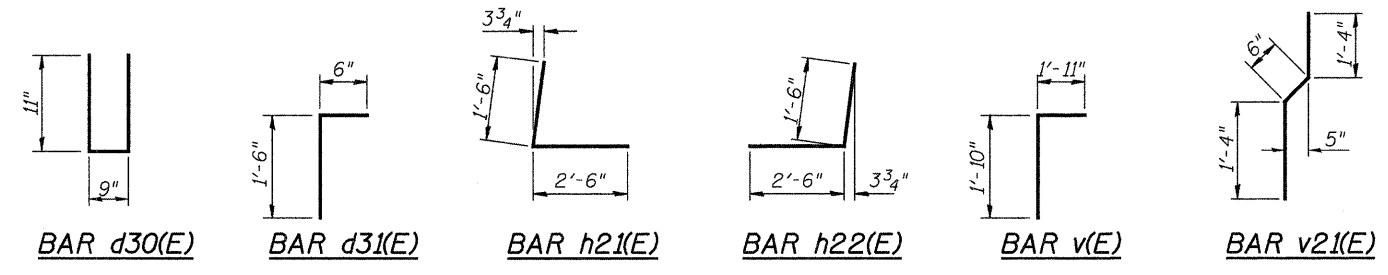
For details of Bar Splicers, see sheet 25 of 26.



ELEVATION



SECTION C-C



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CONSULTING ENGINEERS
Springfield, Illinois

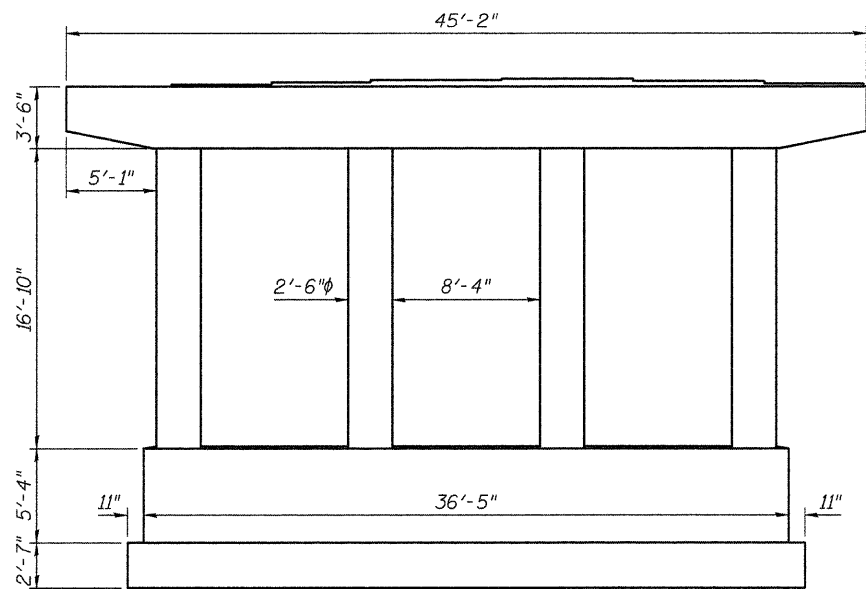
DESIGNED: JDQ DRAWN: P. Ray
CHECKED: DCD CHECKED: DCD

EAST ABUTMENT (NB)
STRUCTURE NO. 058-0101(NB) & 0102(SB)

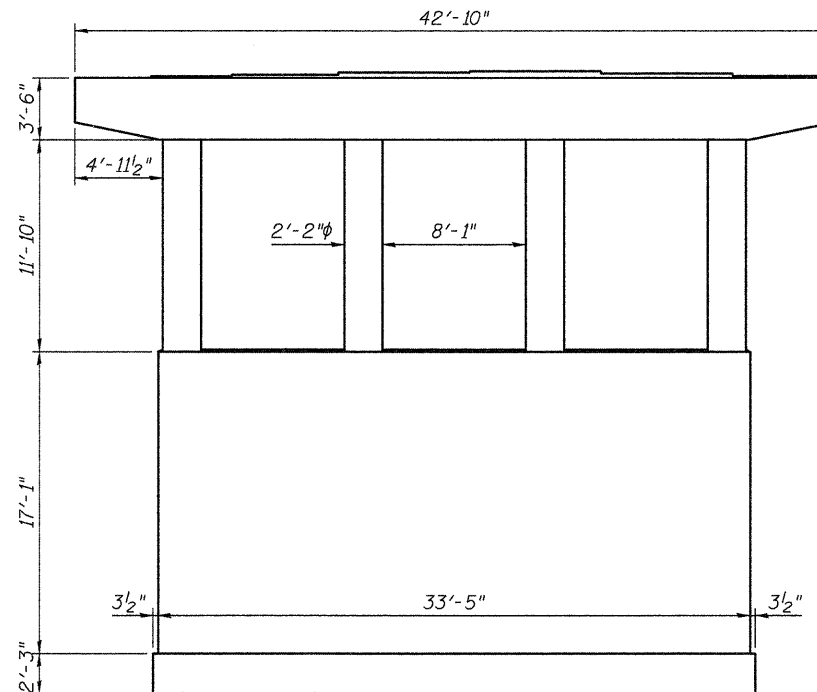
| SHEET NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|-----------|---------------------------|--------------|-----------|
| 21 OF 26 | 158-201RS | MACON | 151 | 86 |
| STA. 133+62.02 | | CONTRACT NO. 74150 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

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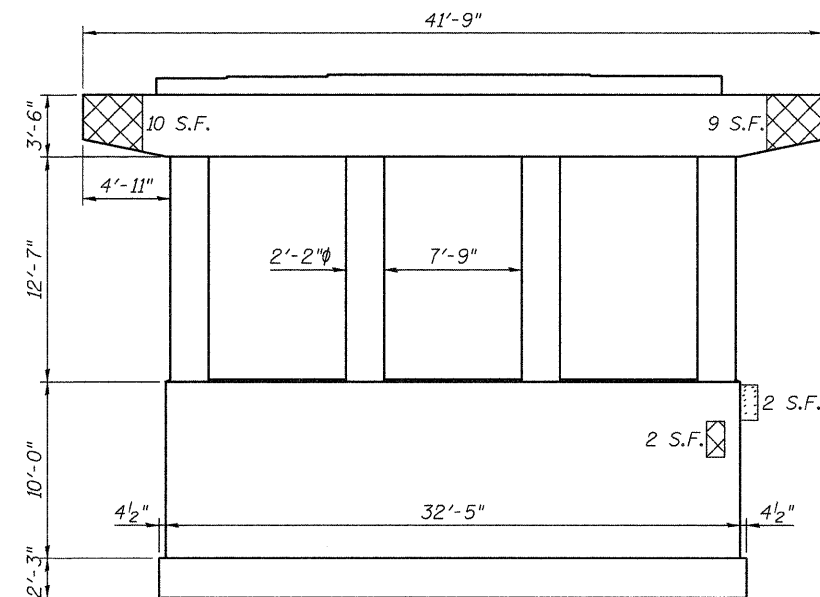
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



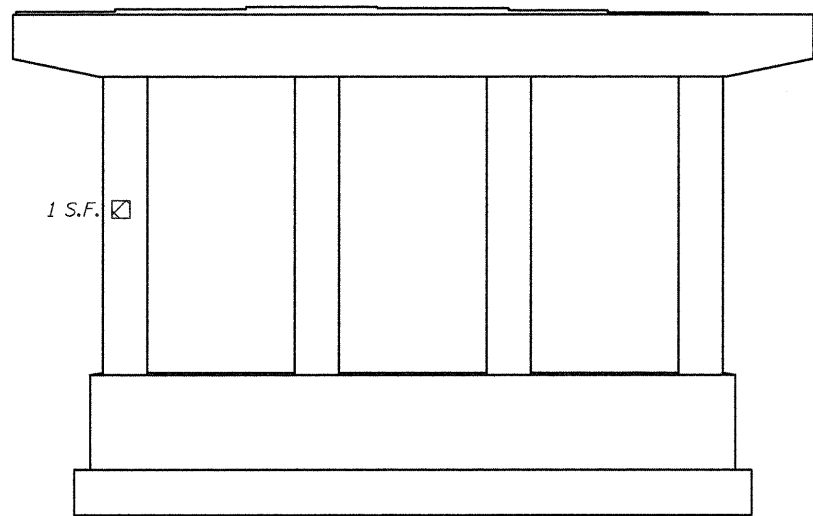
PIER 1 - SB
(Facing North)



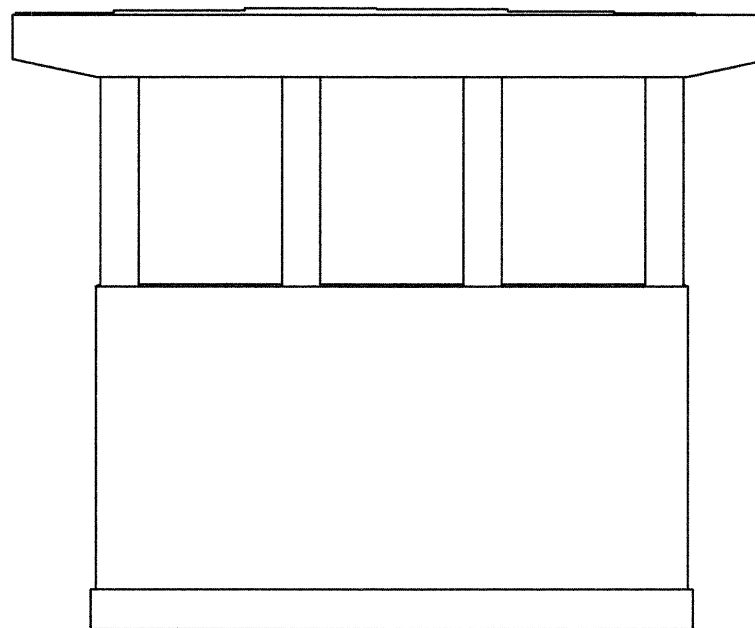
PIER 2 - SB
(Facing North)



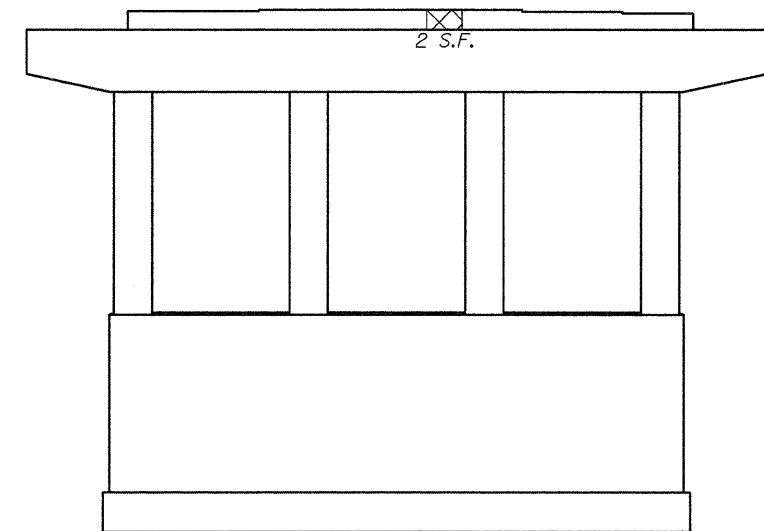
PIER 3 - SB
(Facing North)



PIER 1 - SB
(Facing South)

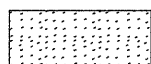





PIER 2 - SB
(Facing South)



PIER 3 - SB
(Facing South)

LEGEND

-  Hollow or Unsound Concrete
-  Spalled Concrete
-  Spalled Concrete with Exposed Reinf.
-  Hairline Crack

Notes:
Pier Condition Survey performed 9/16/2009.

Plan quantities assume that areas of "exposed reinforcing" will be repaired.

Repair areas are estimated, the Engineer shall determine actual repair locations and record them on the As-Built plans.

BILL OF MATERIAL

| Item | Unit | Total |
|--|---------|-------|
| Structural Repair of Concrete (Depth = < 5") | Sq. Ft. | 24 |

Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

| | |
|---------------|--------------|
| DESIGNED: JDQ | DRAWN: SJS |
| CHECKED: DCD | CHECKED: DCD |

**PIERS (SB)
STRUCTURE NO. 058-0101(NB) & 0102(SB)**

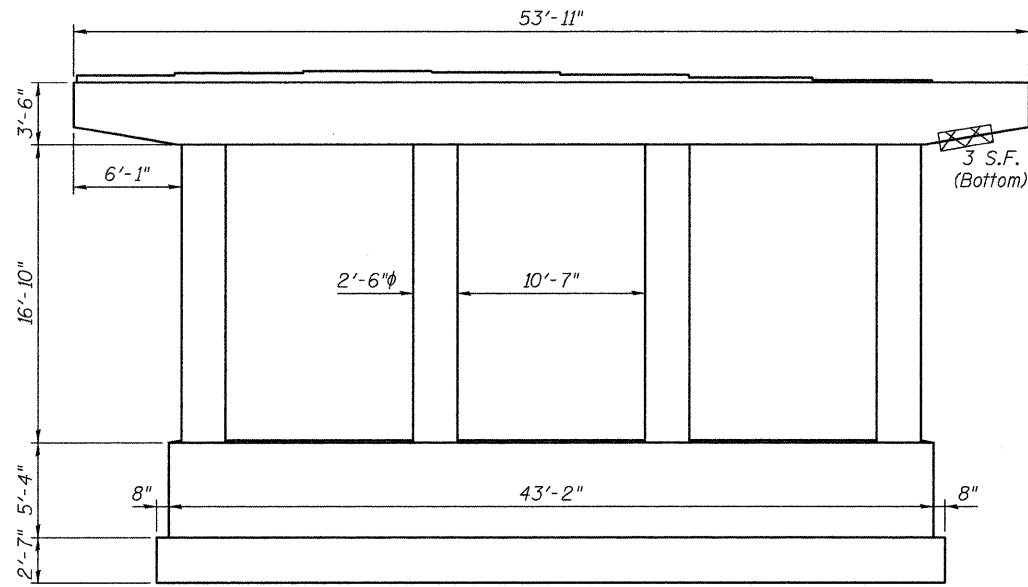
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|---------------------|-------------|---------------------------|--------|--------------|-----------|
| SHEET 23 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | 158-201RS | MACON | 151 | 88 |
| STA. 133+62.02 | | CONTRACT NO. 74150 | | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | |

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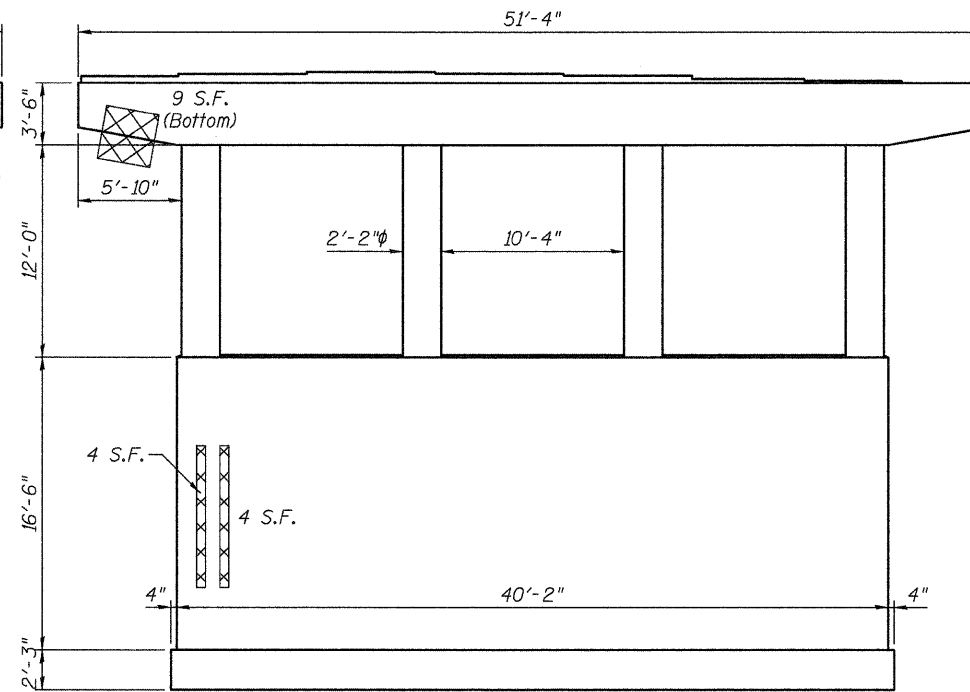
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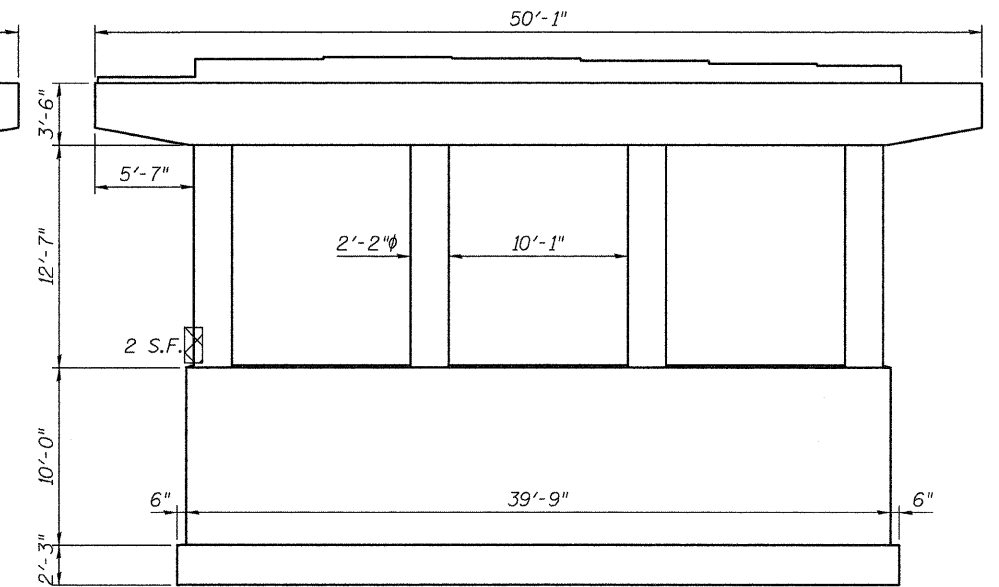
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



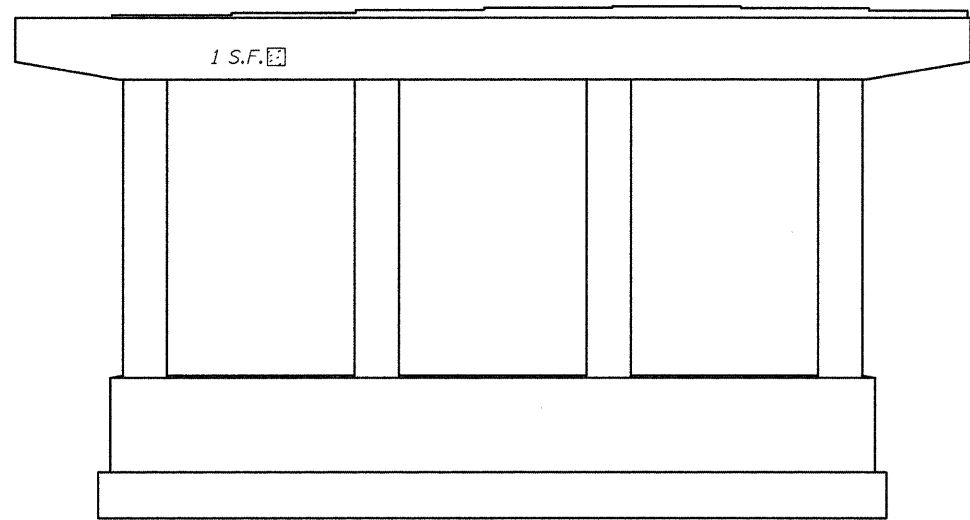
PIER 1 - NB
(Facing North)



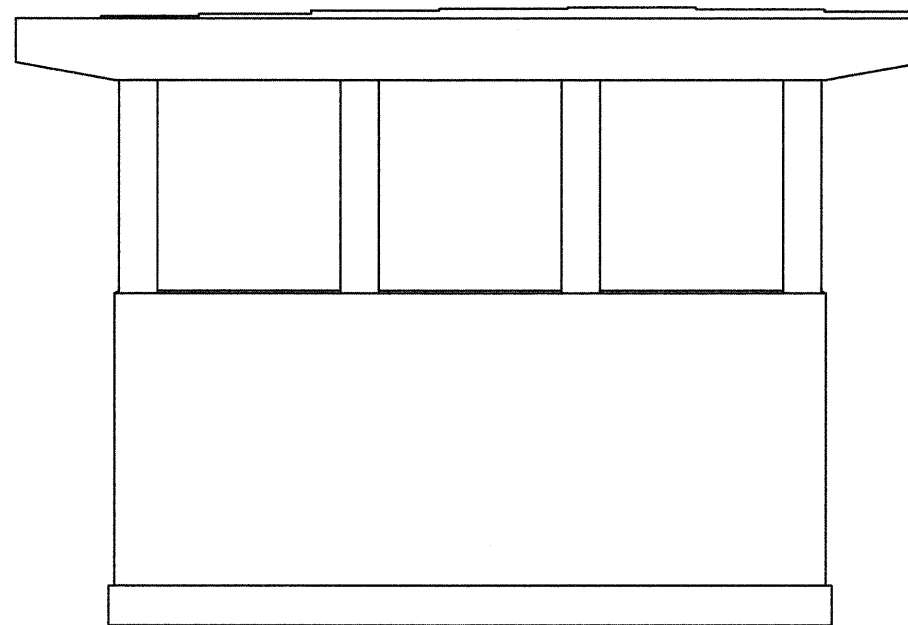
PIER 2 - NB
(Facing North)



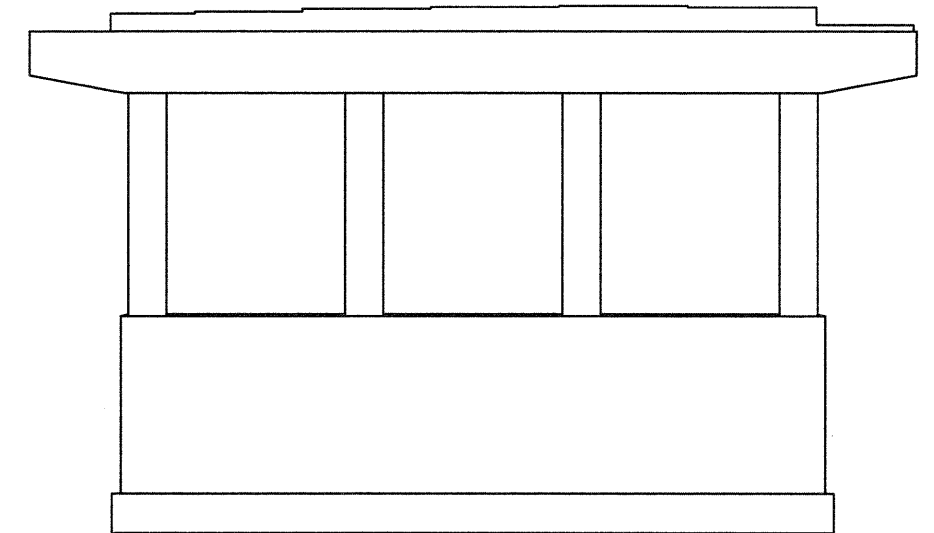
PIER 3 - NB
(Facing North)



PIER 1 - NB
(Facing South)

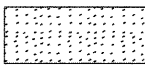





PIER 2 - NB
(Facing South)



PIER 3 - NB
(Facing South)

LEGEND

-  Hollow or Unsound Concrete
-  Spalled Concrete
-  Spalled Concrete with Exposed Reinf.
-  Hairline Crack

Notes:
Pier Condition Survey performed 9/16/2009.

Plan quantities assume that areas of "exposed reinforcing" will be repaired.

Repair areas are estimated, the Engineer shall determine actual repair locations and record them on the As-Built plans.

BILL OF MATERIAL

| Item | Unit | Total |
|--|---------|-------|
| Structural Repair of Concrete (Depth = < 5") | Sq. Ft. | 22 |

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CONSULTING ENGINEERS
Springfield, Illinois

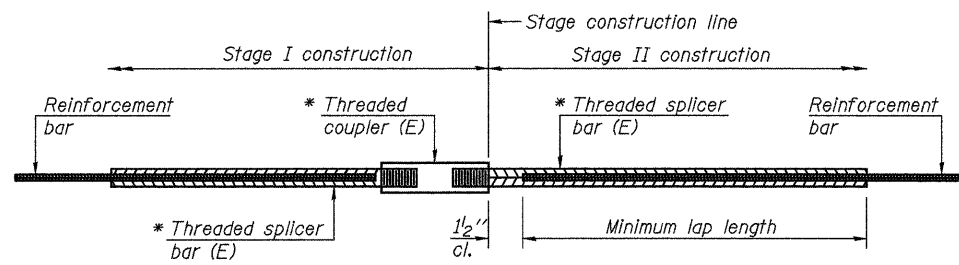
| | |
|---------------|--------------|
| DESIGNED: JDQ | DRAWN: SJS |
| CHECKED: DCD | CHECKED: DCD |

PIERS (NB)
STRUCTURE NO. 058-0101(NB) & 0102(SB)

| | | | | | |
|-------------------|-------------|---------------------|---------------------------|--------------|-----------|
| SHEET 24 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 89 |
| | | STA. 133+62.02 | CONTRACT NO. 74150 | | |
| | | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | |

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



STANDARD BAR SPLICER ASSEMBLY

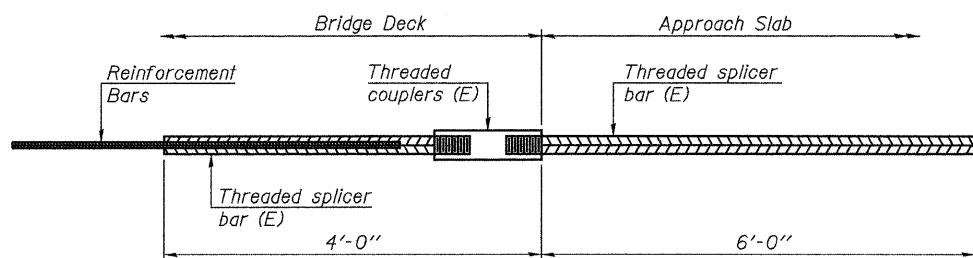
| Bar size to be spliced | Minimum Lap Lengths | | | |
|------------------------|---------------------|---------|---------|---------|
| | Table 1 | Table 2 | Table 3 | Table 4 |
| 3, 4 | 1'-5" | 1'-11" | 2'-1" | 2'-4" |
| 5 | 1'-9" | 2'-5" | 2'-7" | 2'-11" |
| 6 | 2'-1" | 2'-11" | 3'-1" | 3'-6" |
| 7 | 2'-9" | 3'-10" | 4'-2" | 4'-8" |
| 8 | 3'-8" | 5'-1" | 5'-5" | 6'-2" |
| 9 | 4'-7" | 6'-5" | 6'-10" | 7'-9" |

Table 1: Black bar, 0.8 Class C
Table 2: Black bar, Top bar lap, 0.8 Class C
Table 3: Epoxy bar, 0.8 Class C
Table 4: Epoxy bar, Top bar lap, 0.8 Class C

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

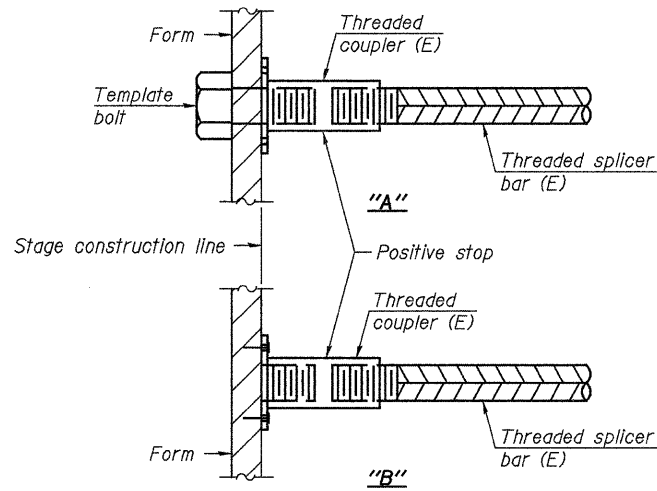
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

| Location | Bar size | No. assemblies required | Table for minimum lap length |
|----------------|----------|-------------------------|------------------------------|
| Deck Exp. Jts. | #6 | 60 (SB) & 60 (NB) | 3 |
| Appr. Slab | #4 | 48 (SB) & 48 (NB) | 3 |
| Appr. Slab | #5 | 170 (SB) & 170 (NB) | 3 |
| Abutments | #5 | 16 (SB) & 16 (NB) | 3 |
| Abutments | #6 | 8 (SB) & 8 (NB) | 3 |



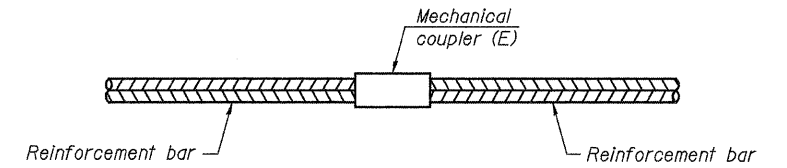
BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required =



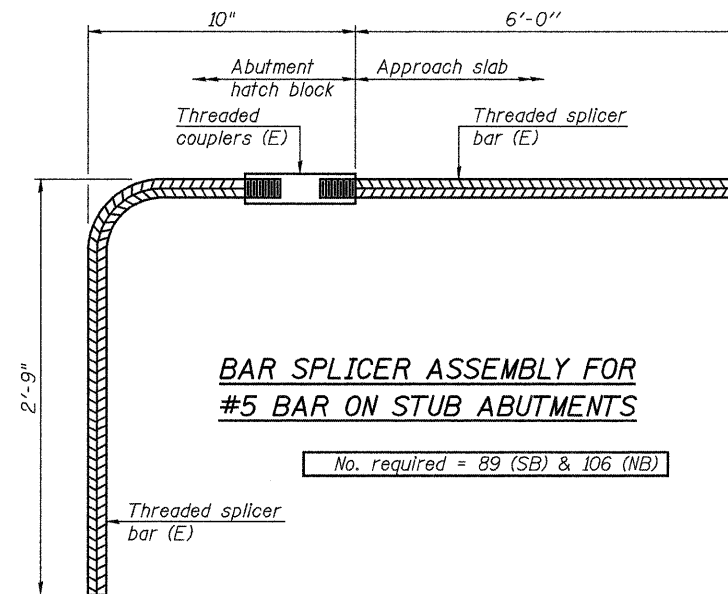
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

| Location | Bar size | No. assemblies required |
|----------|----------|-------------------------|
| | | |
| | | |
| | | |
| | | |



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required = 89 (SB) & 106 (NB)

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
All reinforcement shall be lapped and tied to the splicer bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
See special provision for Mechanical Splicers.
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 058-0101(NB) & 0102(SB)

| SHEET 25 OF 26 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|-------------|---------------------------|--------|--------------|-----------|
| | 322 | 158-201RS | MACON | 151 | 90 |
| STA. 133+62.02 | | CONTRACT NO. 74150 | | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | |

JD Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

DESIGNED: JDQ DRAWN: SJS
CHECKED: DCD CHECKED: DCD

BSD-1 11-1-09

DATE: 04/09/2010 12:22:14 USER: DCD FILE: J:\JDD\101077 IL-DTVM#8 US51Brdge Repairs\I-US51bus-ICRR\058010-74150-025-bar-splicer.dgn

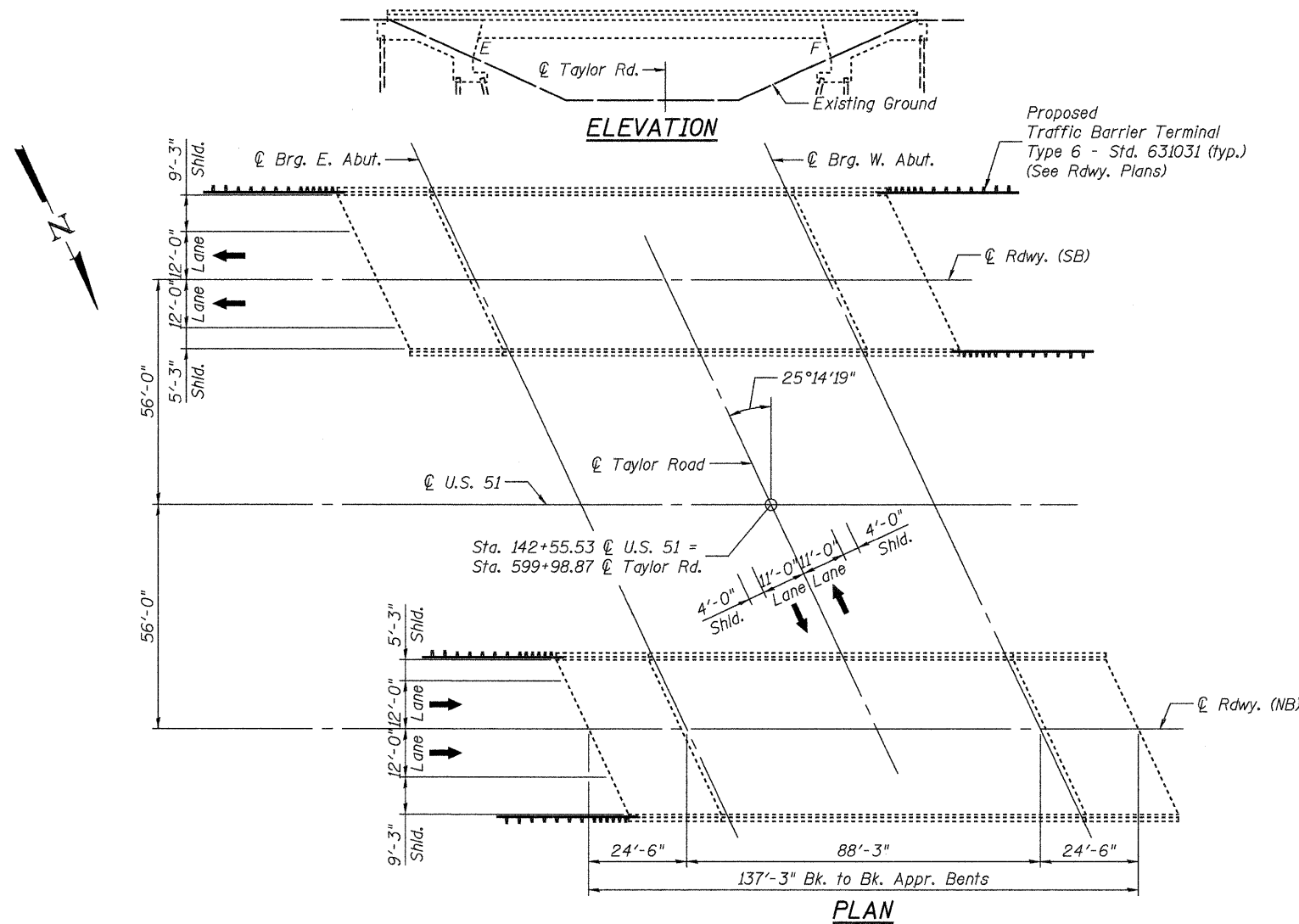
EXISTING STRUCTURE: S.N. 058-0104(NB) & 0105(SB), originally constructed in 1976 as FA Route 412 Sec. 58-20-HB-1 at Station 142+55.53, using 54" PPC I-beams with 7 1/2" concrete deck and 1 1/2" bituminous wearing surface, 1 span (plus abutment spans), 137'-3" back-back approach bents, 42'-0" out-out width, vaulted abutments on concrete piles.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Staged construction shall be used to maintain one lane of traffic in each direction.

INDEX OF SHEETS

| Sheet No. | Description |
|-----------|------------------------------------|
| 1 | Gen Plan, Gen Notes, Bill of Mat'l |
| 2 | Temporary Concrete Barrier |
| 3-6 | Superstructure |
| 7 | Preformed Joint Strip Seal |
| 8 | Bearings |
| 9-10 | Abutments |
| 11 | Bar Splicer Assembly Details |

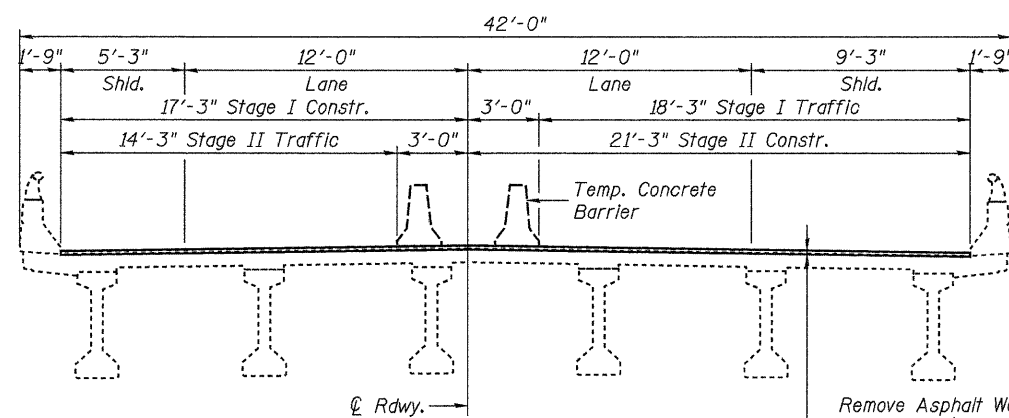


GENERAL NOTES

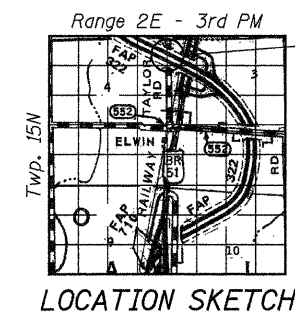
Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.
 Reinforcement bars designated (E) shall be epoxy coated.
 Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost Included with Concrete Removal.
 Where shown on the plans, reinforcement bars shall be epoxy grouted into existing concrete according to Section 584 of the Standard Specifications. Cost included with Reinforcement Bars, Epoxy Coated.
 Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.
 All structural steel shall be shop painted with the inorganic zinc rich primer per AASHTO M300, Type 1. Cost included with Furnishing and Erecting Structural Steel.
 Protective Shield shall be provided for the full width of the bridge deck, over the roadway below from edge of shoulder to edge of shoulder.
 Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.

TOTAL BILL OF MATERIAL

| ITEM | UNIT | SB | NB |
|---|-------|------|------|
| Hot-Mix Asphalt Surface Removal (Deck) | Sq Yd | 581 | 581 |
| Concrete Removal | Cu Yd | 23.1 | 23.1 |
| Protective Shield | Sq Yd | 155 | 155 |
| Concrete Superstructure | Cu Yd | 25.6 | 25.6 |
| Bridge Deck Grooving | Sq Yd | 551 | 551 |
| Protective Coat | Sq Yd | 682 | 682 |
| Furnishing And Erecting Structural Steel | Pound | 140 | 140 |
| Reinforcement Bars, Epoxy Coated | Pound | 4420 | 4360 |
| Bar Splicers | Each | 32 | 32 |
| Preformed Joint Strip Seal | Foot | 90 | 90 |
| Anchor Bolts, 1" | Each | 10 | 10 |
| Structural Repair Of Concrete (Depth = < 5") | Sq Ft | -- | 5 |
| Bridge Deck Microsilica Concrete Overlay 2 1/4" | Sq Yd | 534 | 534 |
| Bridge Deck Hydro-Scarification 1/2" | Sq Yd | 534 | 534 |
| Deck Slab Repair (Full Depth, Type II) | Sq Yd | 8 | 1 |



CROSS SECTION
(Looking with traffic)

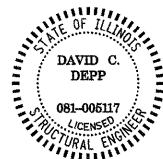


GENERAL PLAN & ELEVATION
 U.S. 51 OVER TAYLOR ROAD
 F.A.P. RTE. 322 SECTION (58-20)RS
 MACON COUNTY
 STATION 142+55.53
 STRUCTURE NO. 058-0104(NB) & 0105(SB)

| SHEET NO. | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|-------------|---------------------------|--------|--------------|-----------|
| 11 | 322 | (58-20)RS | MACON | 151 | 92 |
| STATION 142+55.53 | | CONTRACT NO. 74150 | | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | |

JD Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

DESIGNED: JDQ DRAWN: SJS
CHECKED: DCD CHECKED: DCD



Signed: David Depp
Date: 4-9-2010
Lic. Expires: 11-30-2010

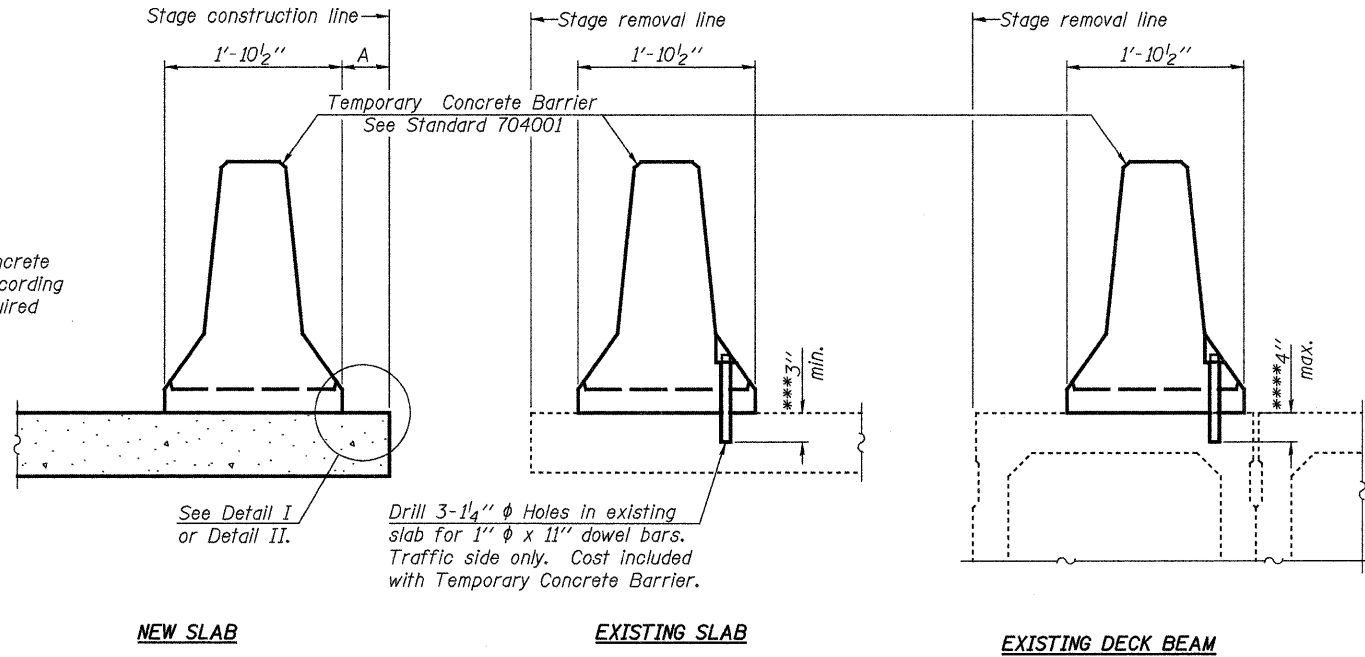
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



SECTIONS THRU SLAB OR DECK BEAM

NOTES

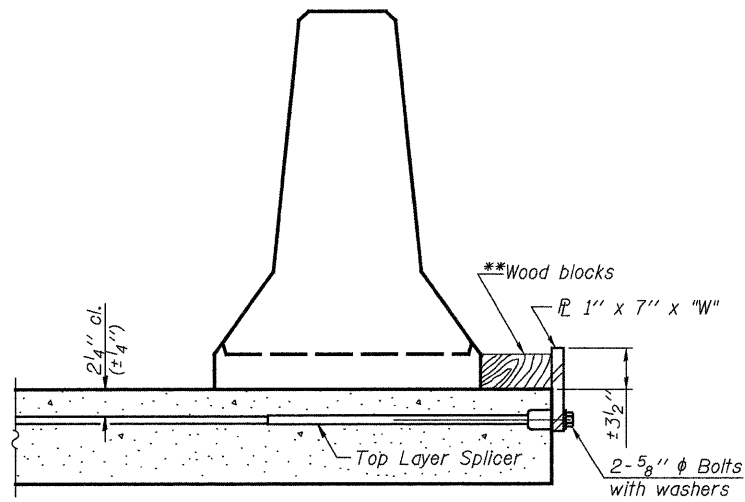
Detail I - With Bar Splicer or Couplers:
Connect one (1) 1"x7"x10" steel PL to the top layer of couplers with 2-5/8" φ bolts screwed to coupler at approximate C of each barrier panel.

Detail II - With Extended Reinforcement Bars:
Connect one (1) 1"x7"x10" steel PL to the concrete slab or concrete wearing surface with 2-5/8" φ Expansion Anchors or cast in place Inserts spaced between the top layer of reinforcement at approximate C of each barrier panel.

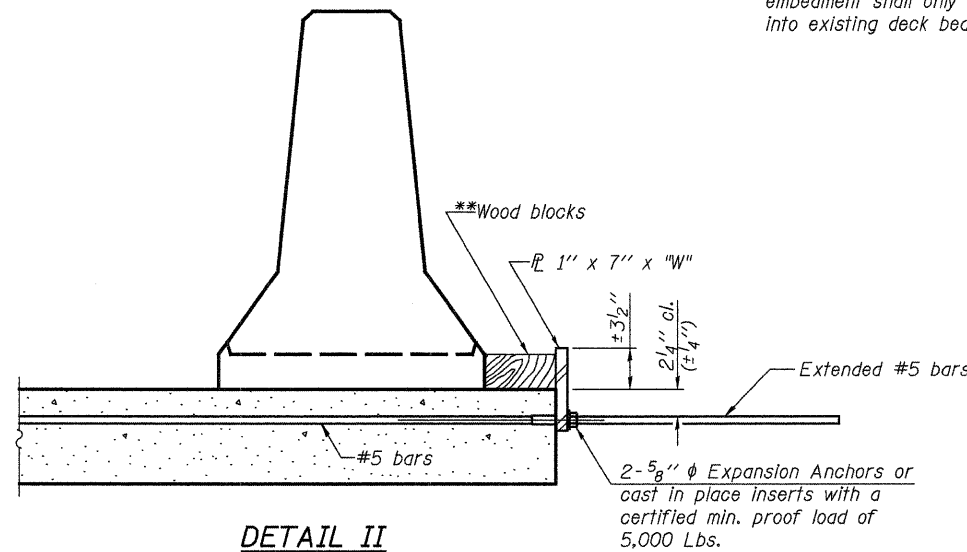
Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

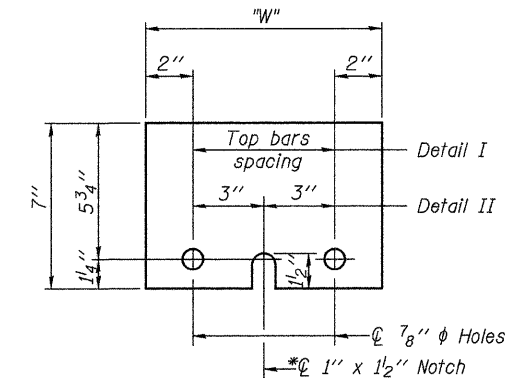
**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER PL 1" x 7" x 10"

* Required only with Detail II

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

"W" = Top bars spacing + 4"

JD Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

| | |
|---------------|--------------|
| DESIGNED: JDQ | DRAWN: SJS |
| CHECKED: DCD | CHECKED: DCD |

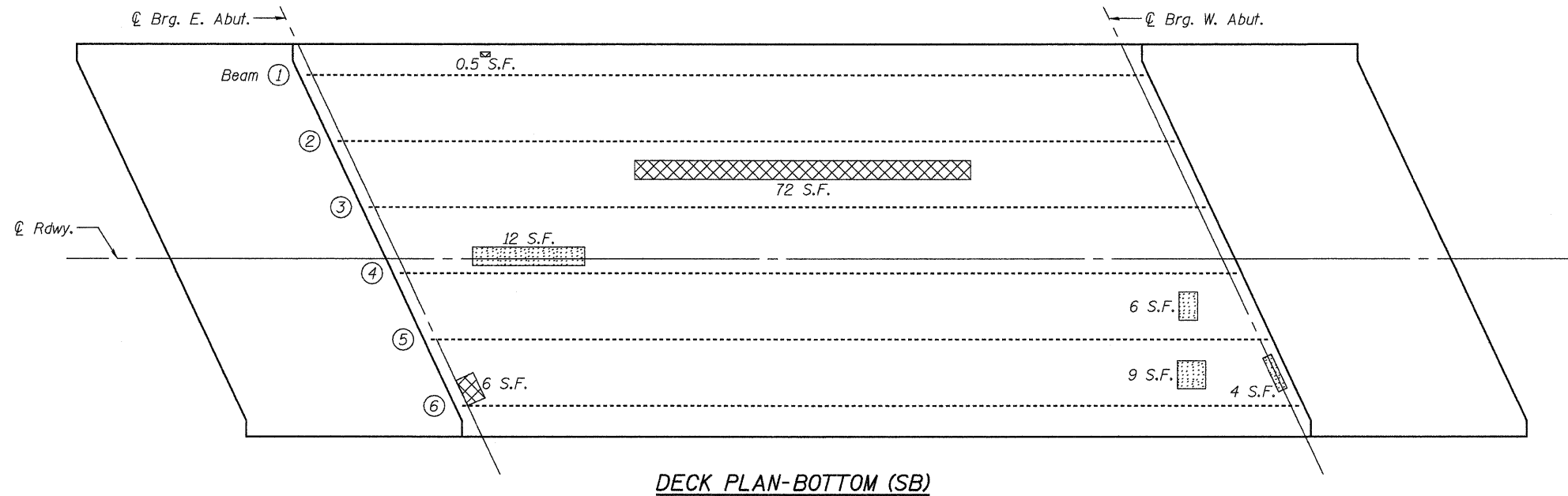
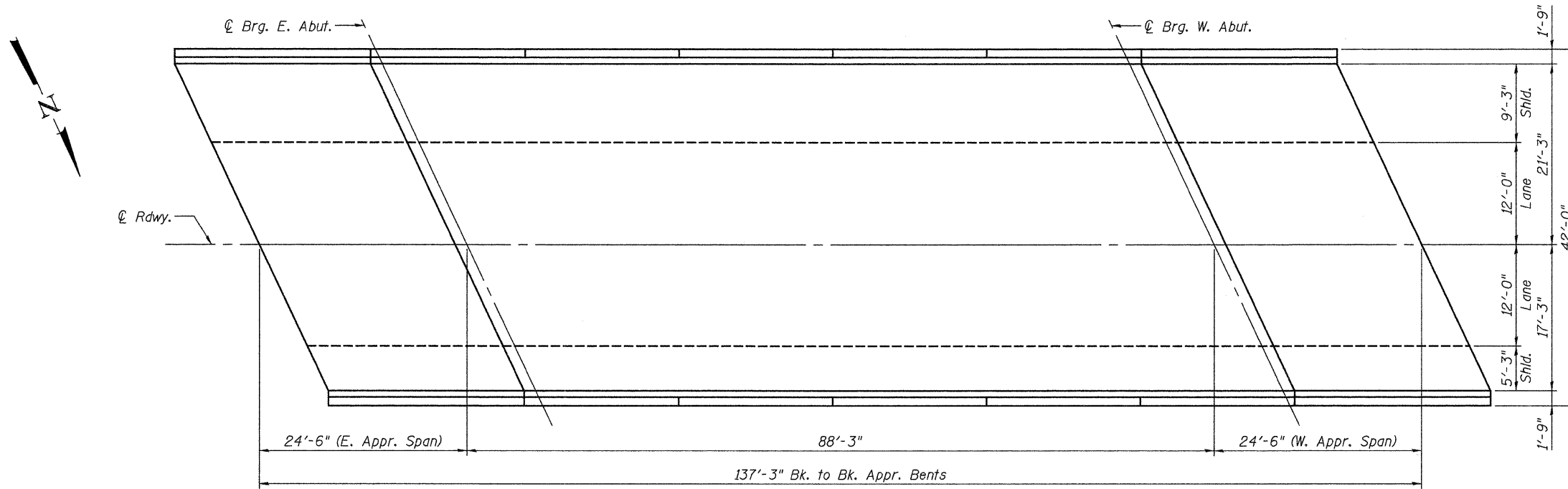
R-27

11-1-09

TEMPORARY CONCRETE BARRIER
FOR STAGE CONSTRUCTION
STRUCTURE NO. 058-0104(NB) & 0105(SB)

| | | | | | |
|------------------|----------------|---------------------|---------------------------|-----------------|--------------|
| SHEET 2 OF 11 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 93 |
| | | STA. 142+55.53 | CONTRACT NO. 74150 | | |
| | | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



DECK PLAN-BOTTOM (SB)

LEGEND

- Hollow or Unsound Concrete
- Spalled Concrete
- Spalled Concrete with Exposed Reinf.
- Hairline Crack

Notes:

Deck Condition Survey performed 9/16/2009.

Plan quantities assume that areas of "exposed reinforcing" greater than 5 sq. ft. will use Full-depth Type II repair.

Repair areas are estimated, the Engineer shall determine actual repair locations and record them on the As-Built plans.

Partial depth repairs are included with Bridge Deck Hydro-scarification, see Special Provision for Bridge Deck Microsilica Concrete Overlay.

BILL OF MATERIAL

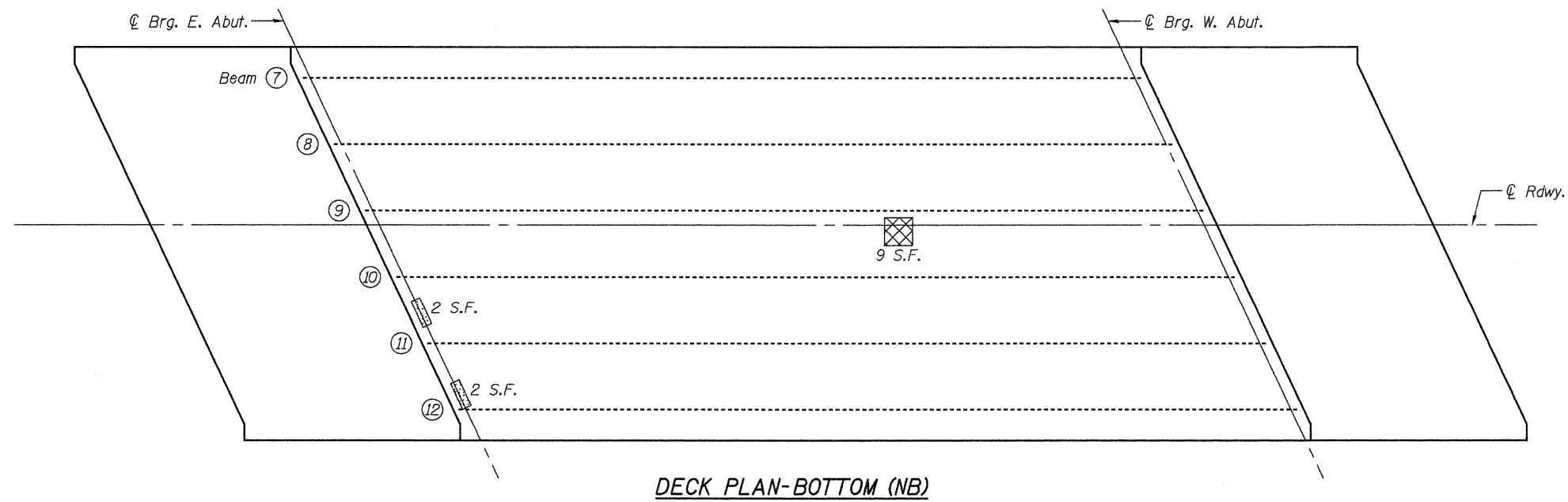
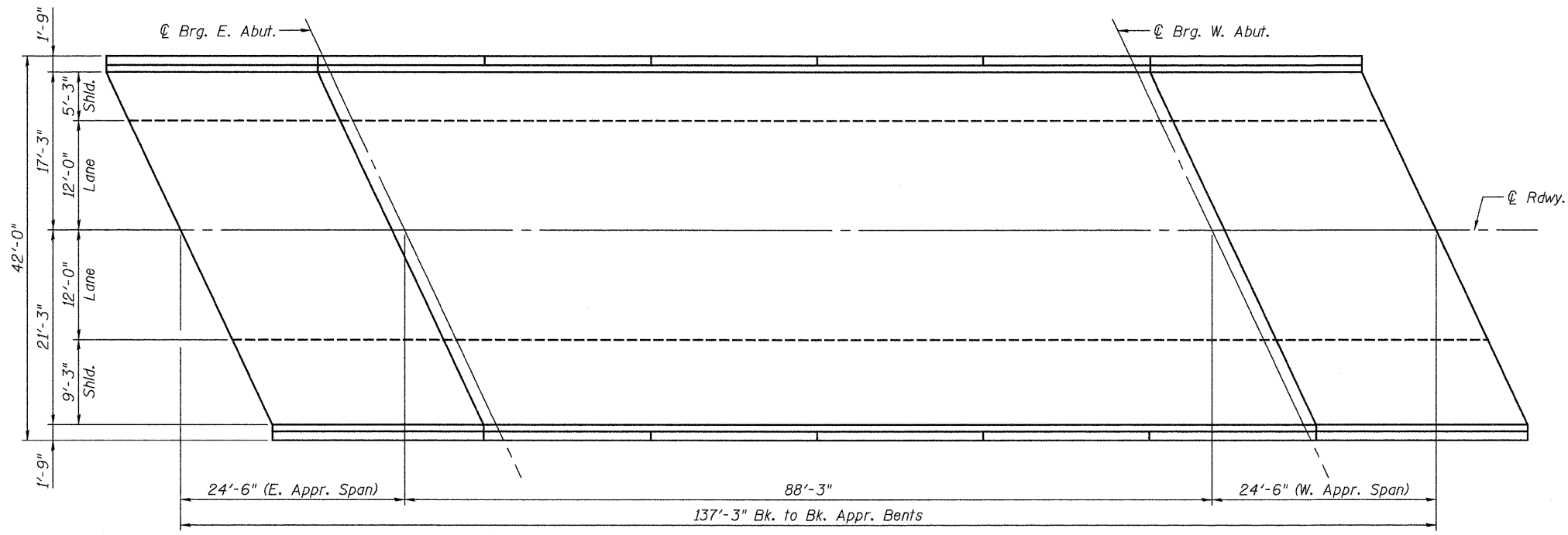
| Item | Unit | Total |
|--|---------|-------|
| Deck Slab Repair (Full Depth, Type II) | Sq. Yd. | 8 |

| | |
|--|--------------|
| Johnson, Depp & Quisenberry CONSULTING ENGINEERS Springfield, Illinois | |
| DESIGNED: JDQ | DRAWN: SJS |
| CHECKED: DCD | CHECKED: DCD |

DECK PLAN (SB)
STRUCTURE NO. 058-0104(NB) & 0105(SB)

| | | | | | |
|---------------------|--------------------|---------------------------|-----------------|---------------------|-----------------|
| SHEET 3 OF 11 | F.A.P. RTE. 322 | SECTION (58-20)RS | COUNTY MACON | TOTAL SHEETS 151 | SHEET NO. 94 |
| | STA. 142+55.53 | | | CONTRACT NO. 74150 | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



LEGEND

- Hollow or Unsound Concrete
- Spalled Concrete
- Spalled Concrete with Exposed Reinf.
- Hairline Crack

Notes:
Deck Condition Survey performed 9/16/2009.

Plan quantities assume that areas of "exposed reinforcing" greater than 5 sq. ft. will use Full-depth Type II repair.

Repair areas are estimated, the Engineer shall determine actual repair locations and record them on the As-Built plans.

Partial depth repairs are included with Bridge Deck Hydro-scarification, see Special Provision for Bridge Deck Microsilica Concrete Overlay.

BILL OF MATERIAL

| Item | Unit | Total |
|--|---------|-------|
| Deck Slab Repair (Full Depth, Type II) | Sq. Yd. | 1 |

Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

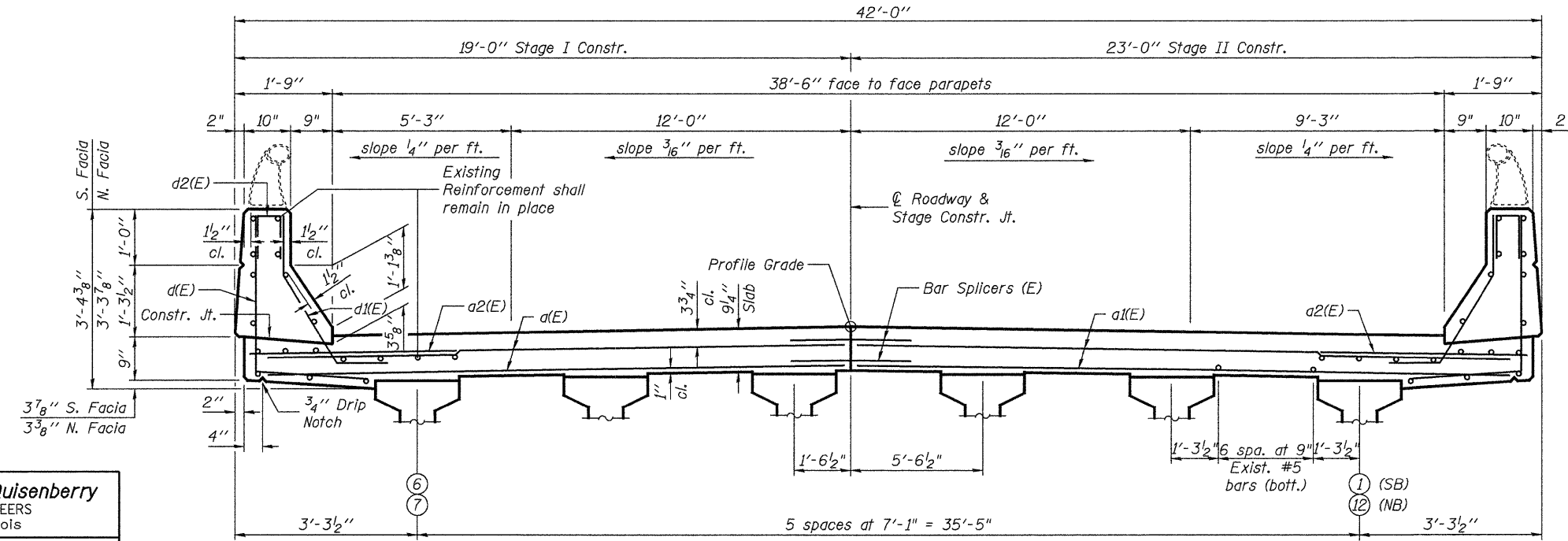
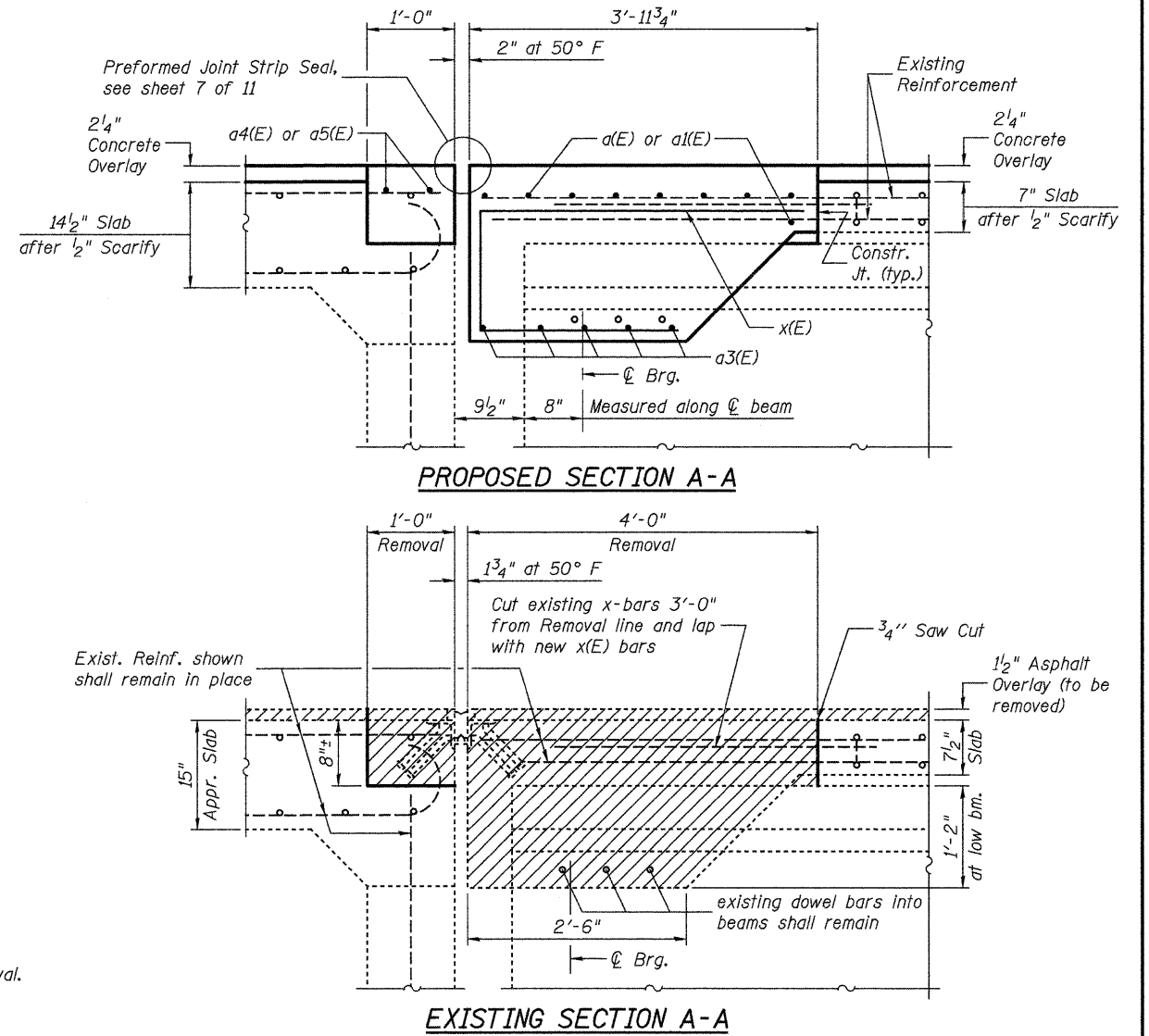
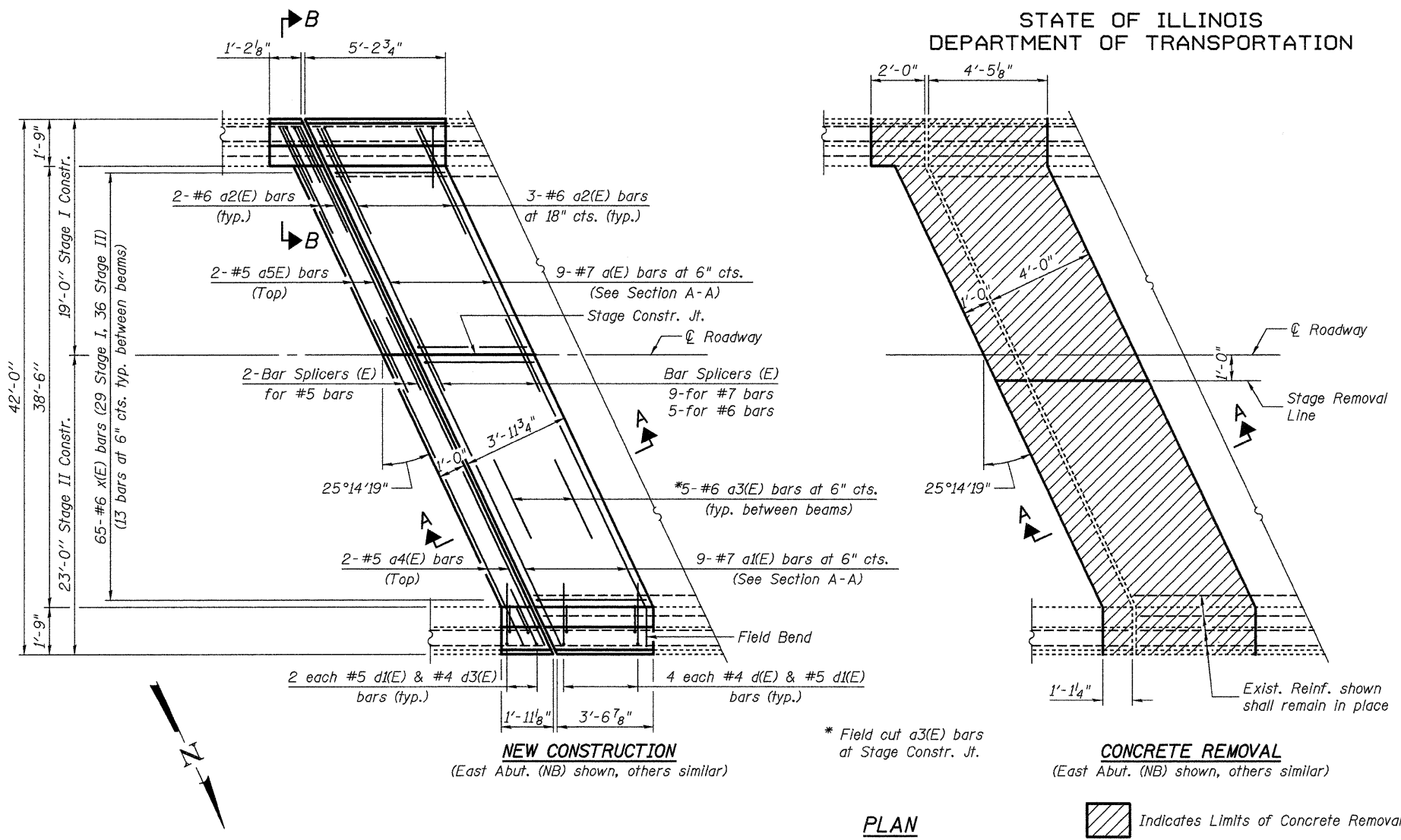
| | |
|---------------|--------------|
| DESIGNED: JDQ | DRAWN: SJS |
| CHECKED: DCD | CHECKED: DCD |

DECK PLAN (NB)
STRUCTURE NO. 058-0104(NB) & 0105(SB)

| | | | | | |
|------------------|-------------|---------------------|---------------------------|--------------|-----------|
| SHEET 4 OF 11 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 95 |
| | | STA. 142+55.53 | CONTRACT NO. 74150 | | |
| | | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | |

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 DATE: 04/09/2010 10:51:00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



Notes:
For Section B-B, Parapet Details and Bill of Material see sheet 6 of 11.
The Contractor shall use extreme care during concrete removal so as not to damage the PPC I-beams. Any damage is to be repaired at the Contractor's expense.

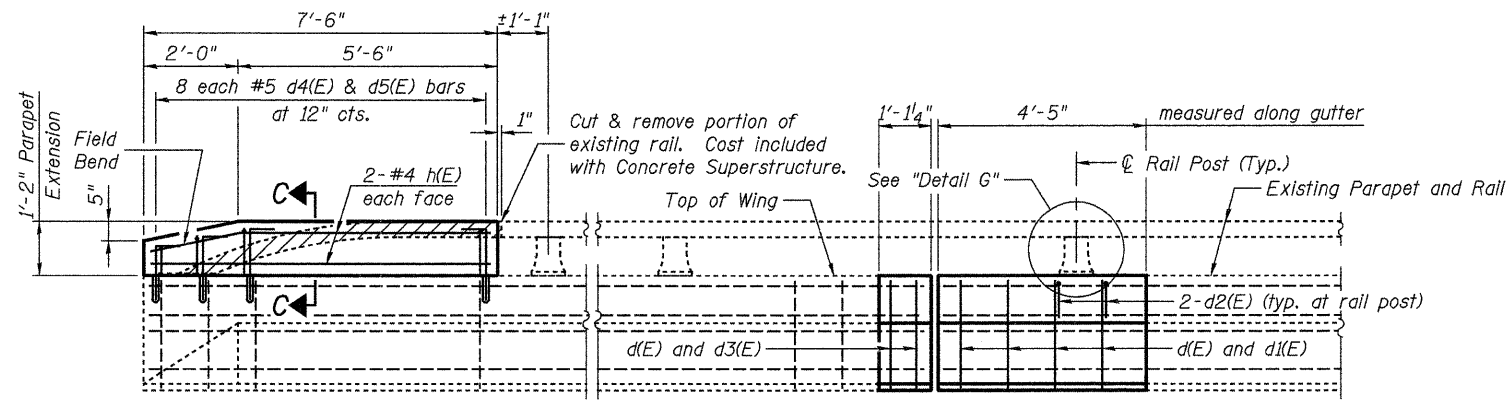
| | |
|--|--------------|
| JD Johnson, Depp & Quisenberry CONSULTING ENGINEERS Springfield, Illinois | |
| DESIGNED: JDQ | DRAWN: PTR |
| CHECKED: DCD | CHECKED: DCD |

SUPERSTRUCTURE (SB & NB)
STRUCTURE NO. 058-0104(NB) & 0105(SB)

| | | | | | |
|------------------|-------------|---------------------|---------------------------|--------------|-----------|
| SHEET 5 OF 11 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 96 |
| | | STA. 142+55.53 | CONTRACT NO. 74150 | | |
| | | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | |

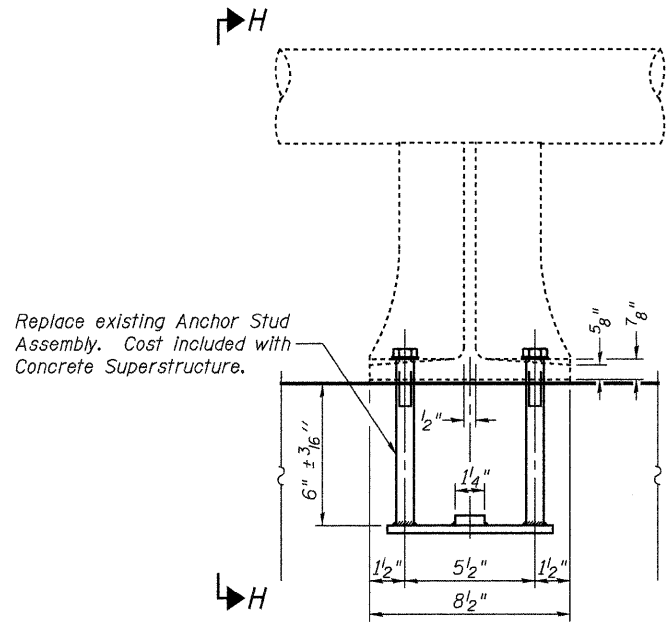
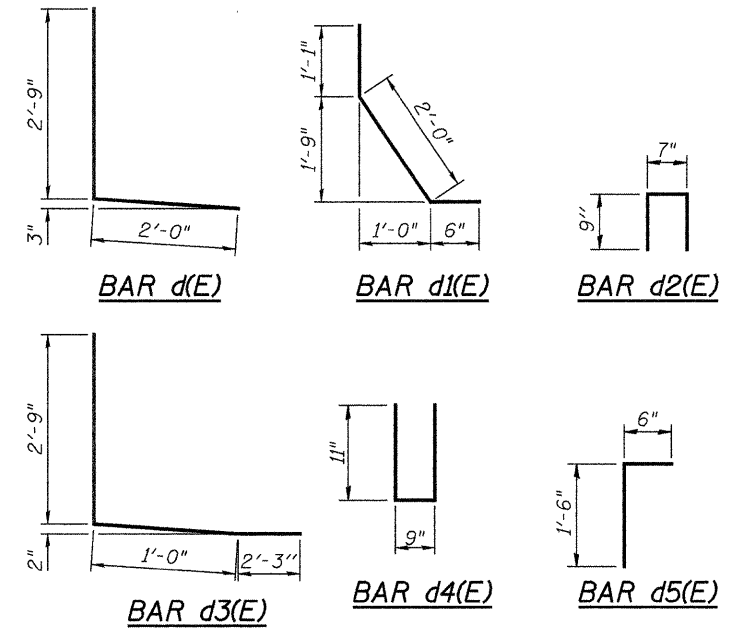
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

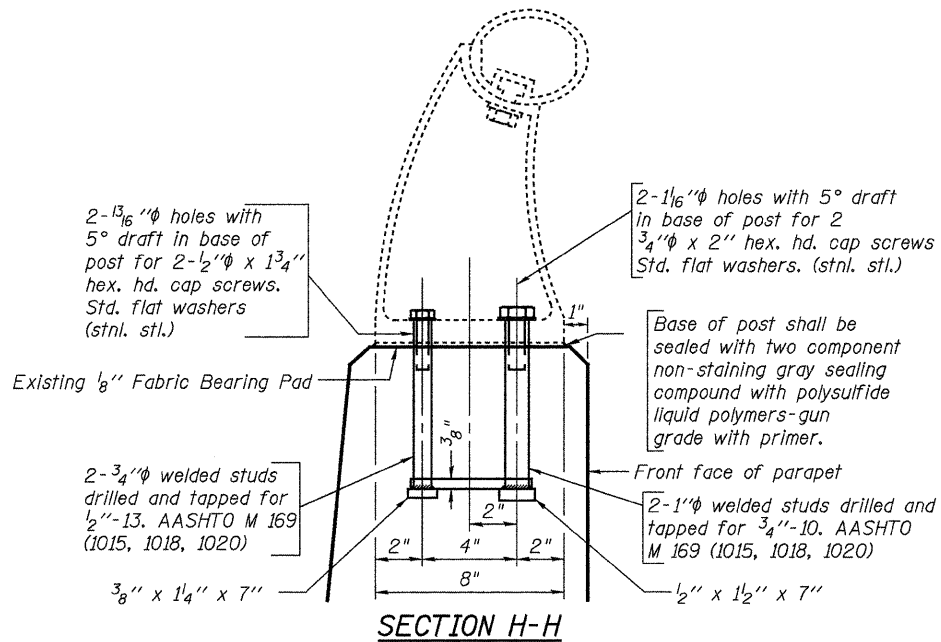


Note:
Wingwall/Railing modification
applies to the following locations:
(SB) E. Abut. - South Wing
(SB) W. Abut. - both wings
(NB) E. Abut. - both wings

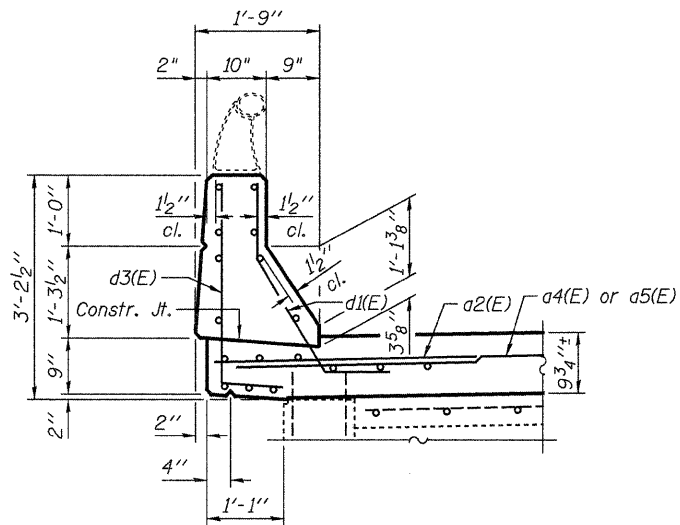
PARTIAL INSIDE ELEVATION OF PARAPET
(East Abutment shown, West Abutment similar)



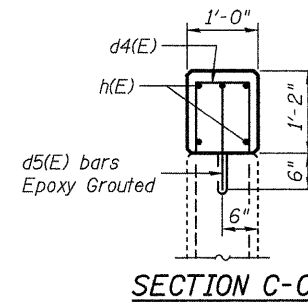
DETAIL G



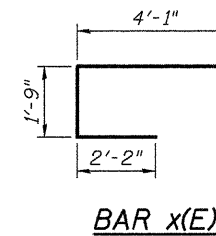
SECTION H-H



SECTION B-B



SECTION C-C



BAR x(E)

**SUPERSTRUCTURE
BILL OF MATERIAL**

| Bar | No.(SB) | No.(NB) | Size | Length | Shape | |
|----------------------------------|---------|---------|------|---------|-------|------|
| d(E) | 18 | 18 | #7 | 20'-3" | — | |
| d1(E) | 18 | 18 | #7 | 24'-8" | — | |
| a2(E) | 20 | 20 | #6 | 4'-6" | — | |
| a3(E) | 50 | 50 | #6 | 6'-11" | — | |
| a4(E) | 4 | 4 | #5 | 25'-0" | — | |
| a5(E) | 4 | 4 | #5 | 20'-7" | — | |
| d(E) | 16 | 16 | #4 | 4'-9" | L | |
| d1(E) | 24 | 24 | #5 | 3'-7" | ┌ | |
| d2(E) | 8 | 8 | #4 | 2'-1" | Π | |
| d3(E) | 8 | 8 | #4 | 6'-0" | L | |
| d4(E) | 24 | 16 | #5 | 2'-7" | U | |
| d5(E) | 24 | 16 | #5 | 2'-0" | Γ | |
| h(E) | 12 | 8 | #4 | 7'-2" | — | |
| x(E) | 130 | 130 | #6 | 8'-0" | └ | |
| | | | | | SB | NB |
| Reinforcement Bars, Epoxy Coated | | | | Pound | 4420 | 4360 |
| Concrete Superstructure | | | | Cu. Yd. | 25.6 | 25.6 |
| Concrete Removal | | | | Cu. Yd. | 23.1 | 23.1 |



| | |
|---------------|--------------|
| DESIGNED: JDQ | DRAWN: PTR |
| CHECKED: DCD | CHECKED: DCD |

**SUPERSTRUCTURE DETAILS
STRUCTURE NO. 058-0104(NB) & 0105(SB)**

| | | | | | |
|---------------------|----------------|---------------------------|--------------------|--------------|-----------|
| SHEET 6 OF 11 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 97 |
| | STA. 142+55.53 | | CONTRACT NO. 74150 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | |

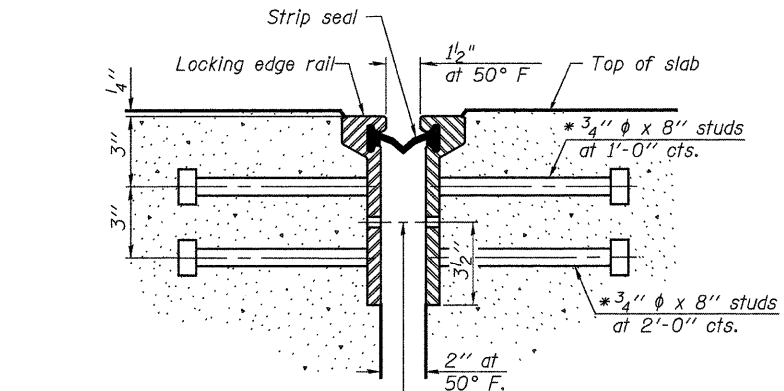
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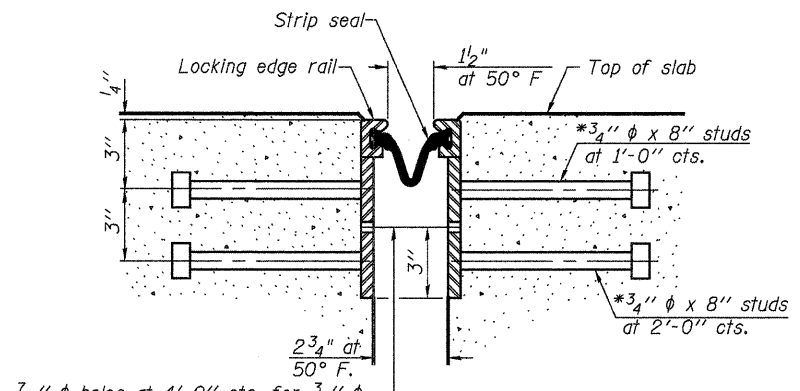
* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



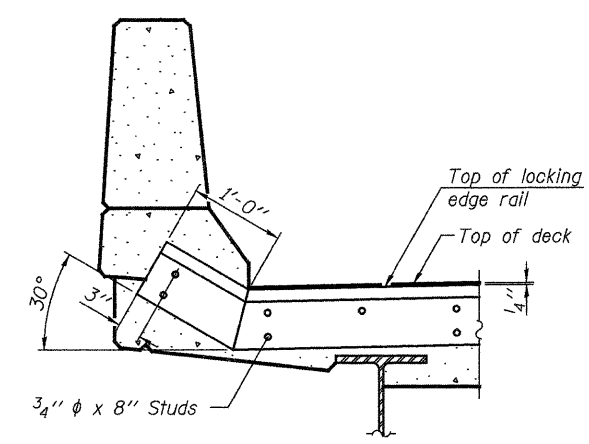
7/16" φ holes at 4'-0" cts. for 3/8" φ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU
ROLLED RAIL JOINT

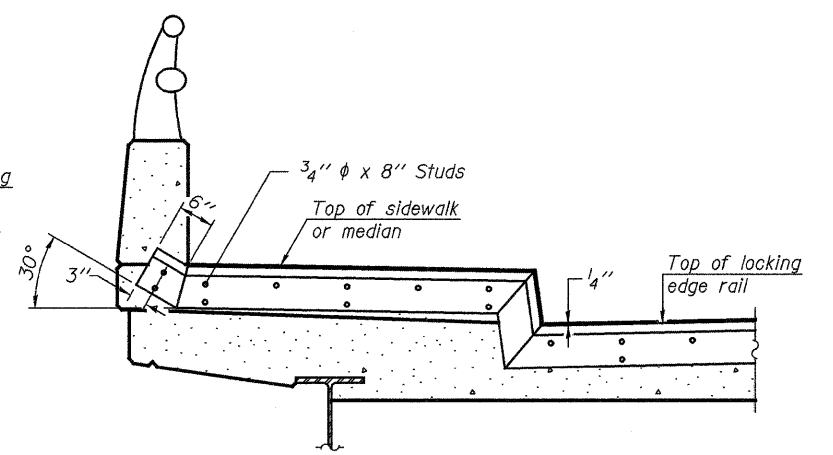


7/16" φ holes at 4'-0" cts. for 3/8" φ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

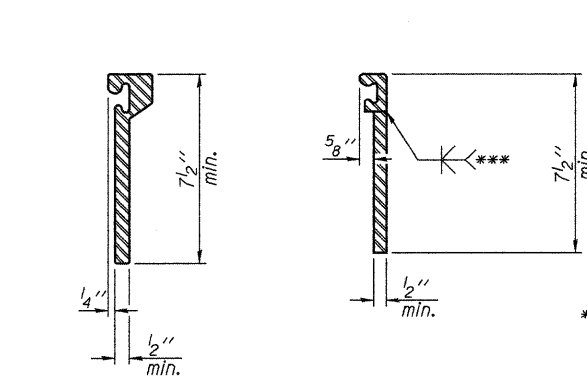
SECTION THRU
WELDED RAIL JOINT



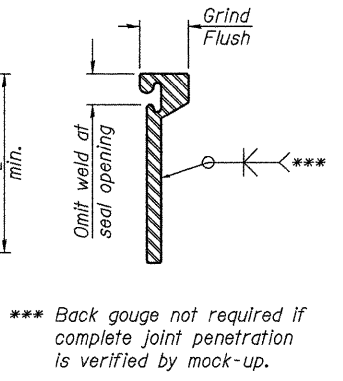
AT PARAPET
See Section A-A for end treatment of skews > 30°.



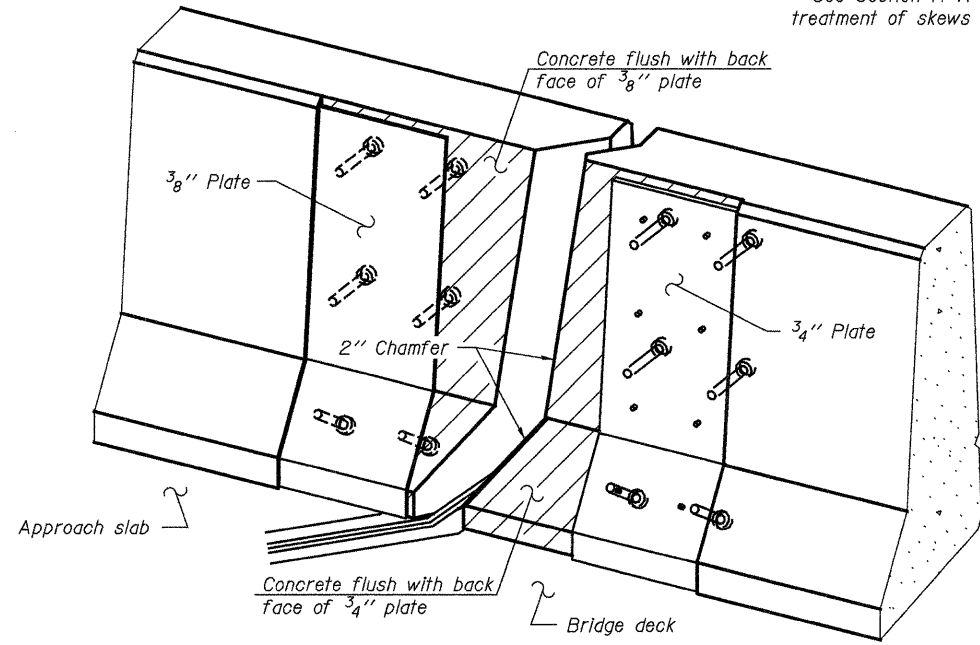
AT SIDEWALK OR MEDIAN
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



ROLLLED
EXTRUDED RAIL WELDED RAIL



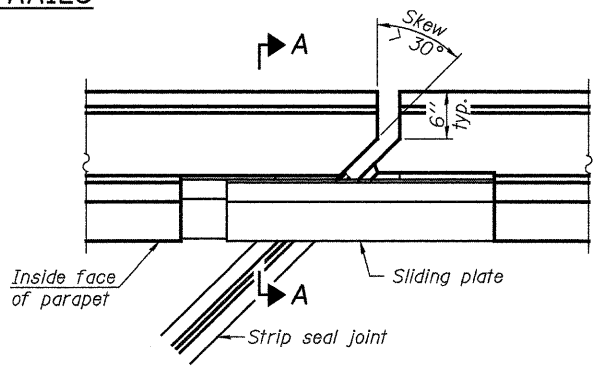
LOCKING EDGE
RAIL SPLICE
The inside of the locking edge rail groove shall be free of weld residue.
Rolled rail shown, welded rail similar.



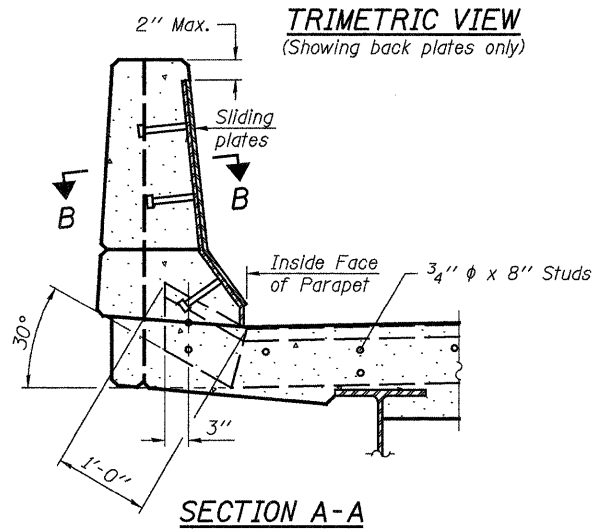
TYPICAL END TREATMENTS

Notes:
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.
The manufacturer's recommended installation methods shall be followed.
The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

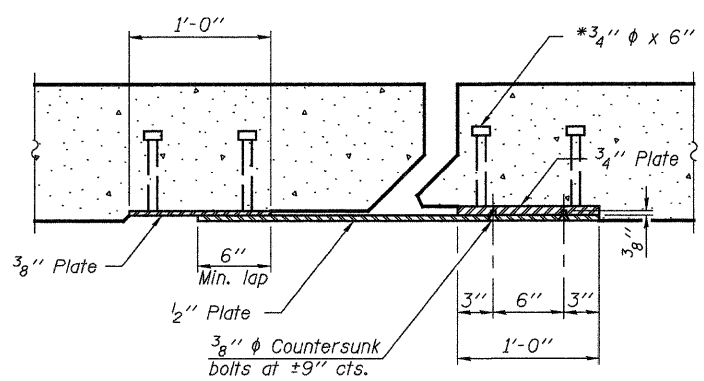
LOCKING EDGE RAILS



PLAN



SECTION A-A
POINT BLOCK DETAILS
(for skews > 30°)



SECTION B-B

BILL OF MATERIAL

| Item | Unit | SB Bridge | NB Bridge |
|----------------------------|------|-----------|-----------|
| Preformed Joint Strip Seal | Foot | 90 | 90 |

PREFORMED JOINT STRIP SEAL
STRUCTURE NO. 058-0104(NB) & 0105(SB)

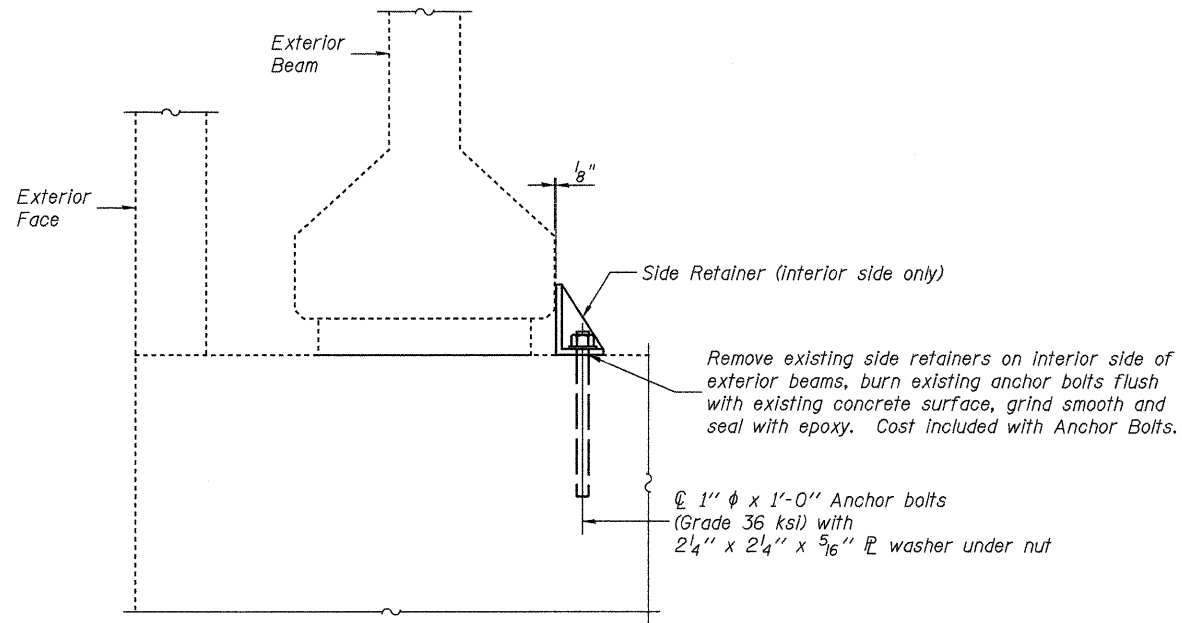
| SHEET 7 OF 11 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|---------------------------|--------|-----------------|--------------|
| | 322 | (58-20)RS | MACON | 151 | 98 |
| STA. 142+55.53 | | CONTRACT NO. 74150 | | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | |

JD Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

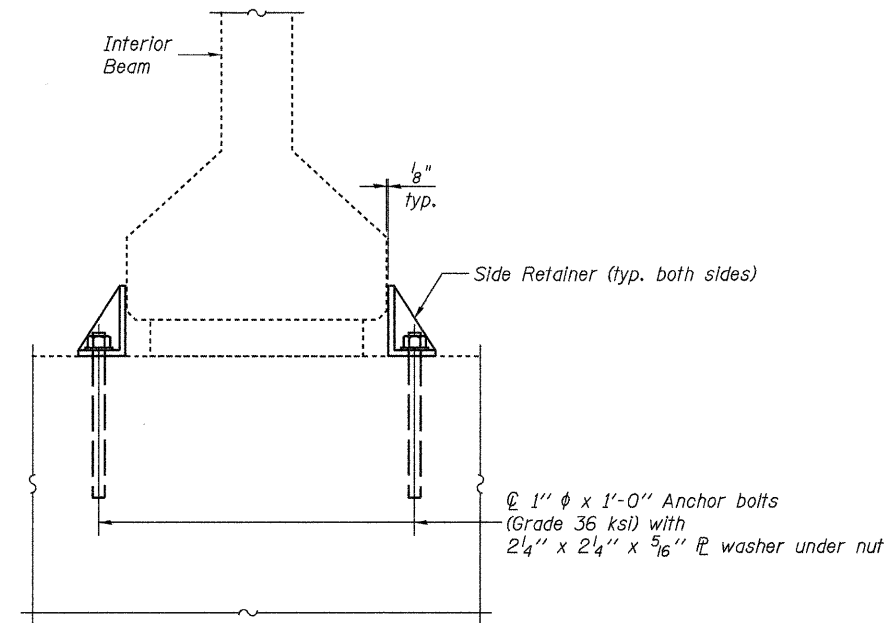
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| DESIGNED: JDQ | DRAWN: PTR |
| CHECKED: DCD | CHECKED: DCD |

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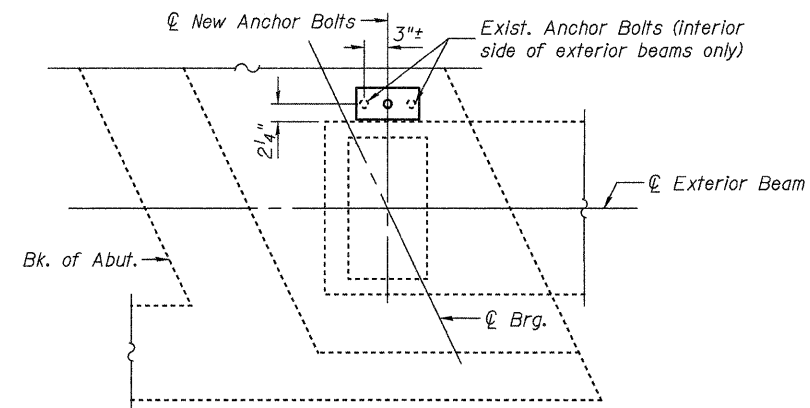
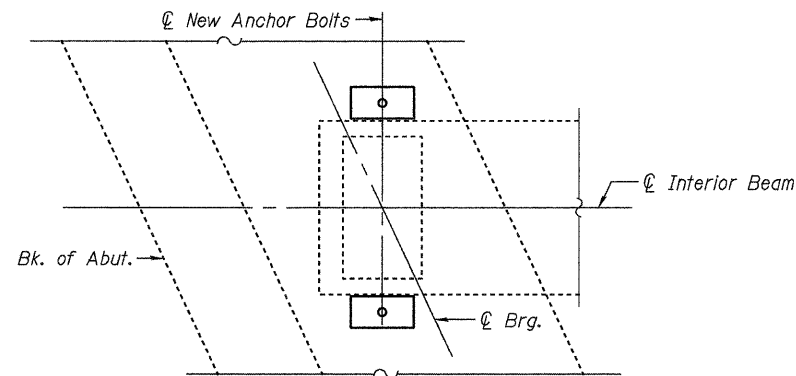
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



SECTION AT EXTERIOR BEAM



SECTION AT INTERIOR BEAM



ANCHOR BOLT LAYOUT

(Typical at East Abutment, North end shown, South end similar)

Notes:

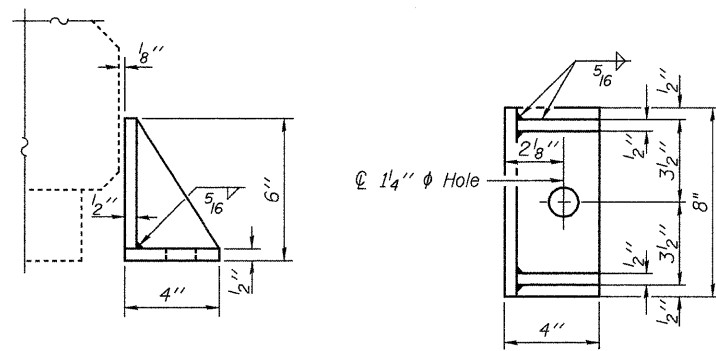
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Anchor bolts for side retainers shall be installed in holes drilled after members are in place.

Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

BILL OF MATERIAL

| Item | Unit | SB | NB |
|--|-------|-----|-----|
| Furnishing and Erecting Structural Steel | Pound | 140 | 140 |
| Anchor Bolts, 1" | Each | 10 | 10 |



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

JD Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois


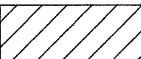

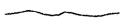
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|---------------|--------------|
| DESIGNED: JDQ | DRAWN: SJS |
| CHECKED: DCD | CHECKED: DCD |

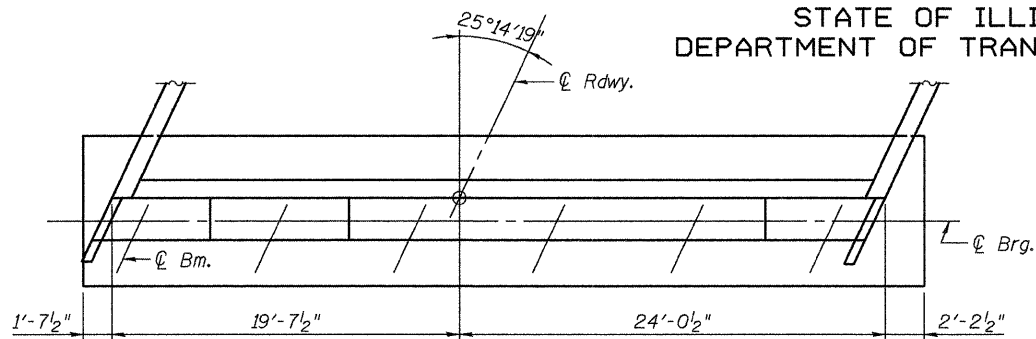
BEARING DETAILS - EAST ABUTMENT
STRUCTURE NO. 058-0104(NB) & 0105(SB)

| | | | | | |
|---------------------|----------------|---------------------------|--------------------|--------------|-----------|
| SHEET 8 OF 11 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 322 | (58-20)RS | MACON | 151 | 99 |
| | STA. 142+55.53 | | CONTRACT NO. 74150 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | |

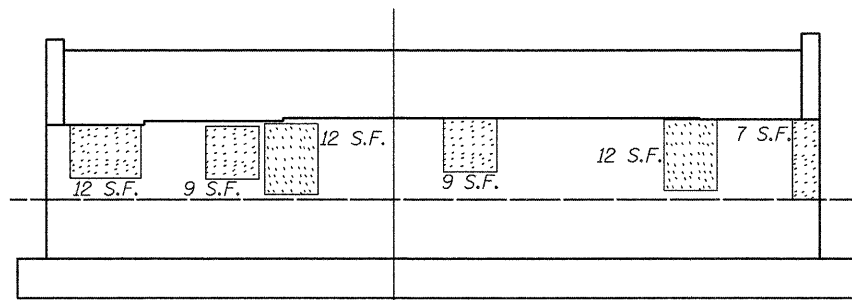
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LEGEND

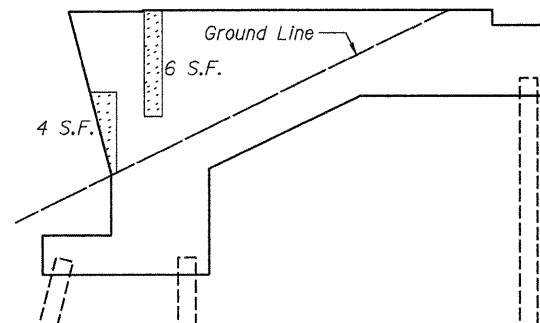
-  Hollow or Unsound Concrete
-  Spalled Concrete
-  Spalled Concrete with Exposed Reinf.
-  Hairline Crack



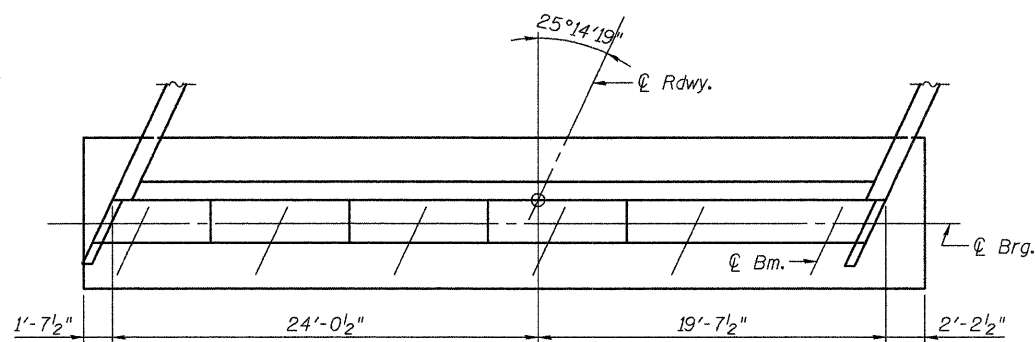
PLAN



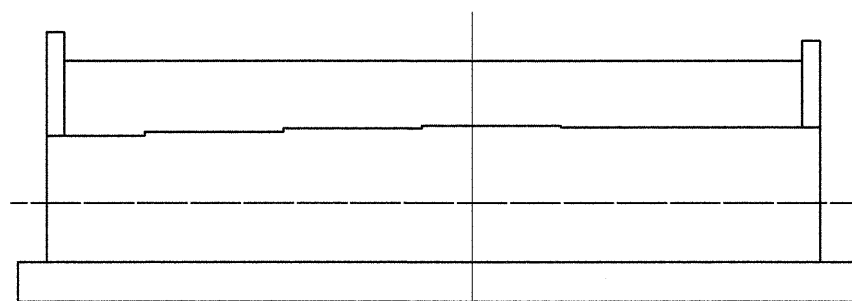
ELEVATION AT EAST ABUTMENT



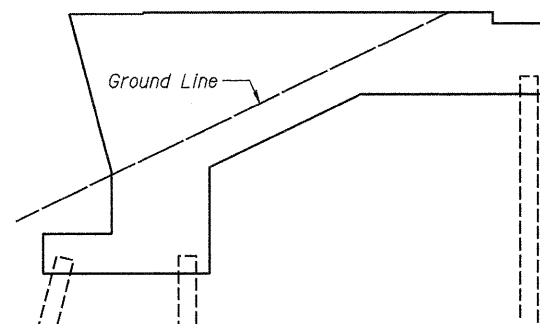
SOUTH WING ELEVATION



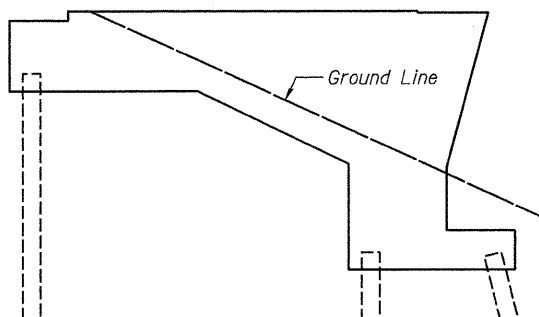
PLAN



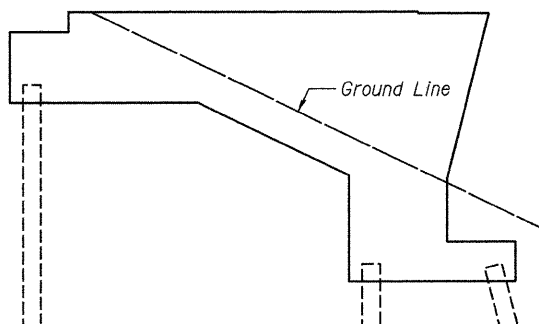
ELEVATION AT WEST ABUTMENT



NORTH WING ELEVATION



NORTH WING ELEVATION



SOUTH WING ELEVATION

BILL OF MATERIAL

| Item | Unit | Total |
|--|---------|-------|
| Structural Repair of Concrete (Depth = < 5") | Sq. Ft. | - |


Notes:
Condition Survey performed 9/16/2009.

Plan quantities assume that areas of "exposed reinforcing" will be repaired.

Repair areas are estimated, the Engineer shall determine actual repair locations and record them on the As-Built plans.

ABUTMENTS (SB)
STRUCTURE NO. 058-0104(NB) & 0105(SB)

| SHEET 9 OF 11 | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|---------------------------|--------|-----------------|--------------|
| | 322 | (58-20)RS | MACON | 151 | 100 |
| STA. 142+55.53 | | CONTRACT NO. 74150 | | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | |

| | |
|--|--------------|
|  Johnson, Depp & Quisenberry CONSULTING ENGINEERS Springfield, Illinois | |
| DESIGNED: JDQ | DRAWN: SJS |
| CHECKED: DCD | CHECKED: DCD |

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