

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	1

D-98-058-08

FOR INDEX OF SHEETS AND STANDARDS SEE SHEET NO. 2

# STATE OF ILLINOIS

## DEPARTMENT OF TRANSPORTATION

### DIVISION OF HIGHWAYS

# PROPOSED HIGHWAY PLANS

**DESIGN DESIGNATIONS:**

15TH STREET 2,000 (30) AT BAUGH AVE;  
 1,990 (30) AT ST. CLAIR AVE MINOR ARTERIAL TF(PCC-20)  
 BAUGH AVENUE 9,900 (30) AT 15TH ST MINOR ARTERIAL TF(PCC-20)  
 ST. CLAIR AVENUE 5,800 (30) AT 15TH ST MINOR ARTERIAL TF(PCC-20)  
 WB I-64 45,340 (30) FREEWAY TF(PCC-20)  
 EB I-64 49,730 (30) FREEWAY TF(PCC-20)  
 RAMP A 2,250 (30) FREEWAY RAMP 11.17(PCC-20)  
 RAMP B 2,250 (30) FREEWAY RAMP 11.17(PCC-20)  
 RAMP E 2,220 (30) FREEWAY RAMP 11.17(PCC-20)

15TH STREET, BAUGH AVENUE,  
 ST. CLAIR AVENUE, RAMP A & RAMP B:  
 DESIGN SPEED = 30 MPH  
 POSTED SPEED = 30 MPH

WB I-64 & EB I-64:  
 DESIGN SPEED = 60 MPH  
 POSTED SPEED = 50 MPH

RAMP E:  
 DESIGN SPEED = 50 MPH  
 POSTED SPEED = 50 MPH

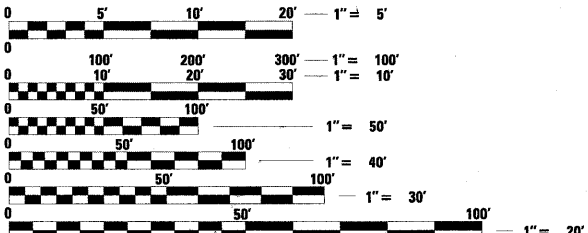


*Danny B. Manojlovski* 3/17/10  
**DANNY B. MANOJLOVSKI** DATE  
 LICENSE EXPIRES 11/30/2011  
 SHEET RANGE 1-31, 45-61, 133-134, 275-282



*Deborah A. Zroka* 3/17/10  
**DEBORAH A. ZROKA** DATE  
 LICENSE EXPIRES 11/30/2010  
 SHEET RANGE 208-251

MICROFILMED \_\_\_\_\_  
 REEL NUMBER \_\_\_\_\_  
 AWARDED \_\_\_\_\_  
 RESIDENT ENGINEER \_\_\_\_\_  
 AS BUILT CHANGES WERE MADE  
 ON THE FOLLOWING SHEETS



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
 ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
 CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
 ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 or 811

**PROJECT ENGINEER MIKE PRITCHETT**  
**SQUAD LEADER CHAD SANDERS**  
**CONTRACT NO. 76C49**



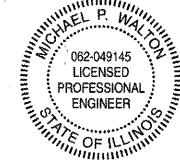
*Robert J. Stern* 3/17/10  
**ROBERT J. STERN** DATE  
 LICENSE EXPIRES 11/30/2011  
 SHEET RANGE 182-187



*James P. Coleman* 3/17/10  
**JAMES P. COLEMAN** DATE  
 LICENSE EXPIRES 11/30/2011  
 SHEET RANGE 192-207



*Tobin T. Bonnell* 3/15/10  
**TOBIN BONNELL** DATE  
 LICENSE EXPIRES 11/30/2011  
 SHEET RANGE 32-44, 62, 76-118,  
 129-132, 135-142, 188-191, 283-345



*Michael P. Walton* 3/15/10  
**MICHAEL WALTON** DATE  
 LICENSE EXPIRES 11/30/2011  
 SHEET RANGE 143-181

**F.A.I. ROUTE 64 (I-64)**  
**SECTION 82-1-2HB**  
**15TH STREET OVER I-64**  
**ST. CLAIR COUNTY**  
**C-98-023-10**

NPDES PERMIT INFORMATION	
NPDES Disturbed	
Area =	13.1 Acres
Approximate Location of Roadway is :	
Longitude	90° 08' 16.87" W
Latitude	38° 37' 38.02" N

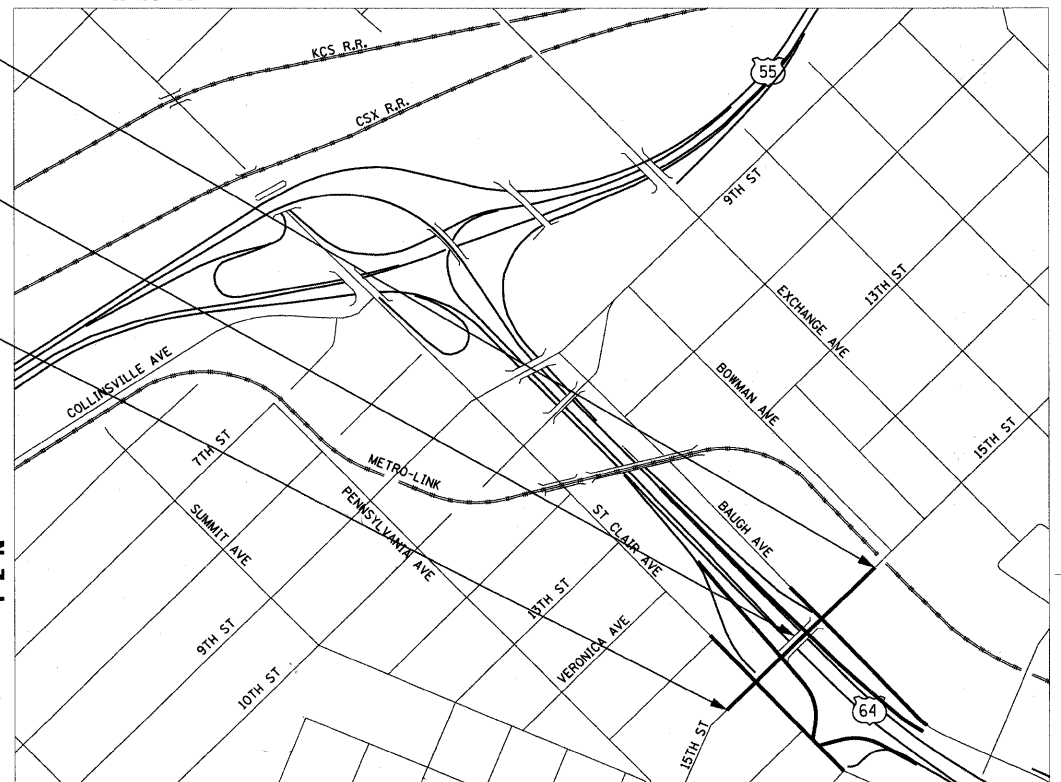


LOCATION OF SECTION INDICATED THIS: - [Symbol]

**SECTION 82-1-2HB END**  
 STA 13+92.79

**15TH STREET OVER I-64**  
 S.N. 082-0377  
 STA 6+80.88 TO STA 8+93.71

**SECTION 82-1-2HB BEGIN**  
 STA 3+00.00



TOWNSHIP: CANTEEN  
 GROSS LENGTH = 1,092.79 FT (0.207 MILES)  
 NET LENGTH = 1,092.79 FT (0.207 MILES)



STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS

SUBMITTED *March 23 2010*  
*My C. Lami*  
 DEPUTY DIRECTOR OF HIGHWAY, REGION ENGINEER

*May 7 2010*  
*Scott E. Stitt, P.E.*  
 ENGINEER OF DESIGN AND ENVIRONMENT

*May 7 2010*  
*Christine M. Road*  
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY  
 OF THE STATE OF ILLINOIS**

PLOT DATE: \*DATE-TIME\*

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HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-05	TEMPORARY EROSION CONTROL SYSTEMS
353001-04	PCC BASE COURSE WITH HMA BINDER AND SURFACE COURSES
420001-07	PAVEMENT JOINTS
420101-04	24' (7.2m) JOINTED PCC PAVEMENT
420106-04	36' (10.8 m) JOINTED PCC PAVEMENT
420111-02	PCC PAVEMENT ROUNDOUTS
420206-08	ENTRANCE RAMP TERMINAL (JOINTED PCC RAMP PAVEMENT ADJACENT TO CRC MAINLINE PAVEMENT)
421001-02	BAR REINFORCEMENT FOR CRC PAVEMENT
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
482006-03	HMA SHOULDER ADJACENT TO RIGID PAVEMENT
483001-04	PCC SHOULDER
515001-03	NAME PLATE FOR BRIDGES
542301-02	PRECAST REINFORCED CONCRETE FLARED END SECTION
601001-03	SUB-SURFACE DRAINS
601101-01	CONCRETE HEADWALL FOR PIPE DRAIN
602001-01	CATCH BASIN TYPE A
602011-01	CATCH BASIN TYPE C
602301-02	INLET - TYPE A
602306-02	INLET - TYPE B
602401-02	MANHOLE TYPE A
602406-03	MANHOLE TYPE A 6' (1.8m) DIAMETER
602411-01	MANHOLE TYPE A 7' (2.1m) DIAMETER
602601-02	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-03	FRAME AND LIDS - TYPE 1
604006-04	FRAME AND GRATE TYPE 3
604036-02	GRATE TYPE 8
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608001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
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631011-06	TRAFFIC BARRIER TERMINAL TYPE 2
631026-05	TRAFFIC BARRIER TERMINAL TYPE 5
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631033-03	TRAFFIC BARRIER TERMINAL, TYPE 6B
631046-04	TRAFFIC BARRIER TERMINAL, TYPE 10
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664001-02	CHAIN LINK FENCE
668001-01	RIGHT OF WAY MARKERS
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 015' (4.5 m) AWAY
701006-03	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701011-02	OFF-RD OPERATIONS, 2L, 2W, DAY ONLY
701101-02	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY
701400-04	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-05	LANE CLOSURE, FREEWAY/EXPRESSWAY
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**GENERAL NOTES**

1. THE OFFSETS TO ALL INLETS AND CATCH BASINS IN CURBED ROADWAYS ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE INDICATED. THE OFFSETS TO MANHOLES ARE TO THE CENTER OF THE DRAINAGE STRUCTURE. A CONCENTRIC DRAINAGE STRUCTURE WAS USED TO CALCULATE THIS DISTANCE.
2. THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HOT-MIX ASPHALT MIXTURE IS PLACED.
3. ALL TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED IN SUCH A MANNER SO AS NOT TO INTERFERE WITH THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
4. ALL AREAS DISTURBED FOR ANY REASON SHALL BE PERMANENTLY SEEDED AS DIRECTED BY THE ENGINEER. ALL AREAS DISTURBED BY CONTRACTOR OUTSIDE THE PROPOSED LIMITS SHALL BE SEEDED AT THE CONTRACTOR'S EXPENSE.
5. HIGH EARLY STRENGTH CONCRETE MIX SHALL BE USED FOR ALL PCC PATCHING AND NO ADDITIONAL PAYMENT WILL BE CONSIDERED.
6. THE CONTRACTOR SHALL BE AWARE THAT THE PROJECT SITE MAY CONTAIN ARCHEOLOGICAL EXCAVATED HOLES. THE CONTRACTOR SHALL FILL THE HOLES WITH EMBANKMENT IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND TO THE SATISFACTION OF THE ENGINEER. PAYMENT FOR THIS WORK SHALL BE ACCORDING TO THE EARTH EXCAVATION PAY ITEM IN THE PLANS.
7. THE IDOT HIGHWAY STANDARDS LATEST REVISION NUMBERS SHALL APPLY TO THIS PROJECT.
8. CONFLICTS MAY OCCUR BETWEEN THE ROADWAY PLANS AND RIGHT-OF-WAY PLANS. THE RIGHT-OF-WAY PLANS SHALL TAKE PRECEDENCE IN CONFLICTS IN RIGHT-OF-WAY OR EASEMENTS. THE ROADWAY PLANS SHALL TAKE PRECEDENCE IN ITEMS FOR CONSTRUCTION.
9. EXCEPT WHERE DESIGNATED OTHERWISE, THE LOCATIONS AND/OR DEPTHS OF UNDERGROUND UTILITIES SHOWN HAVE BEEN TAKEN FROM OFFICE RECORD INFORMATION FURNISHED BY THE UTILITY OWNERS AND THE SUE SURVEYS. ALL UNDERGROUND UTILITIES MUST BE CONSIDERED APPROXIMATE.
10. PROTECTIVE COAT SHALL BE APPLIED TO ALL GUTTER FLAGS, FACE OF CURB, AND MEDIAN SURFACE.
11. AN ESTIMATED QUANTITY OF 200 TONS OF AGGREGATE FOR TEMPORARY ACCESS HAS BEEN INCLUDED IN THIS CONTRACT FOR THE PURPOSE OF MAINTAINING ACCESS TO PRIVATE PROPERTY AND SIDE ROADS THROUGH CONSTRUCTION OPERATIONS DURING THIS CONTRACT.

12. THE PROPOSED EMBANKMENT SHALL BE BENCHED INTO THE EXISTING SLOPES TO THE SATISFACTION OF THE ENGINEER. SEE ROADWAY DETAILS FOR BENCHING DETAIL.
13. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
14. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON PUBLIC PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
15. THE QUANTITY FOR BITUMINOUS MATERIALS (PRIME COAT) WAS DETERMINED USING AN APPLICATION RATE OF 0.0004 TON/SQ YD.
16. THE QUANTITY FOR AGGREGATE (PRIME COAT) WAS DETERMINED USING AN APPLICATION RATE OF 0.002 TON/SQ YD.
17. THE QUANTITY FOR AGGREGATE BASE COURSE, TYPE A WAS DETERMINED USING AN APPLICATION RATE OF 1.95 TON/CU YD.
18. THE QUANTITY FOR AGGREGATE SHOULDERS, TYPE B WAS DETERMINED USING AN APPLICATION RATE OF 4 TON/100 LF.
19. THE UNIT WEIGHT USED TO CALCULATE ALL LEVELING BINDER AND HOT-MIX ASPHALT SURFACE COURSE IS 112 LB / SQ YD / IN.
20. TOPSOIL SHALL BE USED ONLY WHERE SHOWN ON THE LANDSCAPING PLANS. IT WILL NOT BE USED IN ANY OTHER LOCATIONS.
21. ANY CHANGES TO THICKNESS OF AGGREGATE BASE COURSE, TYPE A 12" TO DRAIN TO UNDERDRAINS WILL BE INCLUDED IN THE COST OF THE PAY ITEM. NO ADDITIONAL COMPENSATION WILL BE MADE.
22. FOR ALL PIPES TO BE REMOVED, CONTRACTOR HAS THE OPTION TO GROUT THE PIPES INSTEAD. NO ADDITIONAL COMPENSATION WILL BE PROVIDED FOR GROUTING.
23. DRILL AND GROUT #6 TIE BARS ARE SHALL BE INCLUDED IN THE PRICE FOR BID FOR VARIOUS PCC ITEMS.
24. DEEP WELL MONITORING SHALL BE REQUIRED FOR EXISTING DEEP WELLS 9 & 20.

FILE NAME =	USER NAME = pmsarnc	DESIGNED OP	REVISED - 4/15/2010	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL NOTES / COMMITMENTS</b>				F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN OP	REVISED -		64	82-1-2HB	ST. CLAIR	345	3				
	PLOT SCALE = 1:0000 / IN.	CHECKED DBM	REVISED -		SCALE: NONE SHEET NO. 1 OF 2 SHEETS STA. TO STA.				CONTRACT NO. 76C49				
	PLOT DATE = 4/15/2010	DATE 03/19/10	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT								

**RAILROAD COORDINATION**

SPECIAL ATTENTION IS CALLED TO ARTICLE 107.12. THE NAMES AND TELEPHONE NUMBERS OF THE RAILROADS WITHIN THE PROJECT AREA ARE AS FOLLOWS:

TRENT SMITH  
DIRECTOR OF RAIL SYSTEM MAINTENANCE  
METRO  
(314) 982-1400 EXT. 2813

AFTER COMPLETION OF CONSTRUCTION, RAILROAD DRAINAGE DITCHES SHALL BE CLEANED OF ALL DEBRIS TO THE SATISFACTION OF THE ENGINEER.

**UTILITY COORDINATION**

ILLINOIS STATE LAW REQUIRES A 48-HOUR NOTICE BE GIVEN TO ALL UTILITIES WITHIN THE PROJECT AREA BEFORE DIGGING BY CALLING J.U.L.I.E. AND BY NOTIFYING NON-J.U.L.I.E. MEMBERS INDIVIDUALLY. FIELD MARKING OF FACILITIES MAY ALSO BE OBTAINED BY PROVIDING A MINIMUM OF 96-HOURS NOTICE TO THE RESIDENT ENGINEER SO THAT UTILITY COMPANIES CAN BE NOTIFIED. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:

- AMEREN UE
- AMERITECH
- CHARTER COMMUNICATIONS
- EXPLORER PIPELINE
- ILLINOIS POWER COMPANY
- ILLINOIS AMERICAN WATER
- MARATHON ASHLAND PIPELINE COMPANY
- MCI
- MCLEOD USA
- NORAM TRADING AND TRANSPORTATION COMPANY
- QWEST
- SPRINT
- WORLDCOM
- 360 NETWORKS

CITY OF EAST ST LOUIS - (618) 482-6737  
METRO EAST SANITARY DISTRICT - (618) 452-9400  
ST LOUIS NATIONAL STOCKYARDS COMPANY - (405) 235-8675

(MEMBERS OF J.U.L.I.E. (800)-892-0123 OR 811 ARE INDICATED BY \*. NON-J.U.L.I.E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.)

**COORDINATE SYSTEM AND DATUM**

THIS PROJECT IS BASED ON THE MISSISSIPPI RIVER CROSSING (MRC) COORDINATE SYSTEM. THE MISSISSIPPI RIVER CROSSING (MRC) COORDINATE SYSTEM IS BASED ON A MODIFIED UNIVERSAL TRANSVERSE MERCATOR (UTM) SYSTEM. THE MRC COORDINATE SYSTEM HAS BEEN CONVERTED FROM UTM ZONE 15 NORTH BY AN AVERAGE PROJECTION FACTOR AND ALSO CONVERTED FROM METERS TO U.S. SURVEY FEET.

AVERAGE GRID FACTOR = 1.000339495  
PROJECTION FACTOR = 1/grid = 0.999660620

THE BASE POINT FROM WHICH ALL UTM COORDINATES WERE SCALED WAS PRIMARY CONTROL MONUMENT NUMBER 10, LISTED IN APPENDIX A OF THE MRC SURVEY SUMMARY, AVAILABLE FROM THE ENGINEER. EACH VECTOR FROM MONUMENT NUMBER 10 TO ALL OTHER MONUMENTS WAS MULTIPLIED BY THE PROJECTION FACTOR TO CALCULATE A SURFACE VECTOR AND THEN THIS SURFACE VECTOR WAS USED TO CALCULATE THE SURFACE COORDINATES.

THE VERTICAL CONTROL DATUM IS NAVD 1988.

**TRAFFIC CONTROL**

1. ALL EXCAVATION ADJACENT TO THE EDGE OF PAVEMENT SHALL BE PROTECTED WITH EXTENDED LEG BARRICADES AND APPROPRIATE LIGHTS.
2. ALL TRAFFIC CONTROL SIGNS SHALL BE POST MOUNTED UNLESS OTHERWISE NOTED.
3. "ROAD CONSTRUCTION AHEAD" SIGNS SHALL BE PLACED AT THE BEGINNING AND END OF THE PROJECT AND ON THE INTERSECTING SIDE ROADS, AND WILL BE INCLUDED IN THE TRAFFIC CONTROL PAY ITEMS. ALL CONSTRUCTION SIGNS SHALL BE FLUORESCENT ORANGE, 48".
4. FLAGMEN SHALL BE PRESENT DURING ALL CLOSURE HOURS, INCLUDING LUNCH HOUR, AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED.
5. FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF SIXTEEN SANDBAGS PER BARRICADE.
6. THE CONTRACTOR SHALL FURNISH AND INSTALL WOOD SIGN POST SUPPORTS IN ACCORDANCE WITH SECTION 730 OF THE STANDARD SPECIFICATIONS. HOWEVER, INSTALLATION BY METHOD "A" (ART. 730.04(G)) SHALL BE THE ONLY METHOD PERMITTED.
7. A MINIMUM NOTICE OF 72 HOURS TO THE CITY POLICE, AMBULANCE, AND EMERGENCY CREWS IS NEEDED PRIOR TO ROAD CLOSURES.
8. THE CONTRACTOR MAY NOT ALTER THE SEQUENCE OF CONSTRUCTION WITHOUT THE PRIOR APPROVAL OF THE ENGINEER.
9. TEMPORARY ACCESS TO ALL ENTRANCES (COMMERCIAL AND PRIVATE) SHALL BE MAINTAINED DURING CONSTRUCTION.
10. PRIOR TO THE START OF CONSTRUCTION, ALL REQUIRED TRAFFIC CONTROL DEVICES SHALL BE IN PLACE.

**COMMITMENTS**

1. THE PEDESTRIAN OVERPASS SHALL BE ACCESSIBLE PRIOR TO THE 15TH STREET BRIDGE DEMOLITION FOR A DISABLED/BICYCLE CROSSING OVER THE INTERSTATE LINKING THE MILES DAVIS ELEMENTARY SCHOOL WITH THE EMERSON PARK NEIGHBORHOOD.
2. THE CONTRACTOR SHALL COORDINATE WITH IMMACULATE CONCEPTION AND GESTHEMANE CHURCHES TO MINIMIZE ACCESS ISSUES BOTH TO THE FRONT ENTRY, AND TO THE EXISTING DRIVEWAYS TO THE CHURCH PROPERTIES.  
  
IMMACULATE CONCEPTION CHURCH - (618) 874-0162  
GETHEMANE CHURCH - (618) 874-6203
3. SAINT CLAIR COUNTY TRANSIT DISTRICT (SCCTD) HAS REQUESTED TO BE NOTIFIED 2 WEEKS IN ADVANCE OF ANY ROAD CLOSURES RELATED TO THE 15TH STREET BRIDGE OVER I-64 CONTRACT. THIS WILL GIVE SCCTD TIME TO COORDINATE ALTERNATE BUS ROUTES, AND TO NOTIFY THE PUBLIC OF THE CHANGES.  
  
ST. CLAIR COUNTY TRANSIT DISTRICT - (618) 628-8090
4. 18TH STREET SHALL BE BECOME AN ALTERNATIVE ROUTE FOR TRAFFIC TO GET ACROSS BETWEEN ST. CLAIR AVENUE AND BAUGH AVENUE DURING THE 15TH STREET BRIDGE CLOSURE.

FILE NAME =	USER NAME = pmsdrno	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL NOTES / COMMITMENTS</b>				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILES#		DRAWN OP	REVISED -						64	82-1-2HB	ST. CLAIR	345	4
	PLOT SCALE = 1:8000 1" = 80' IN.	CHECKED DBM	REVISED -		SCALE: NONE SHEET NO. 2 OF 2 SHEETS STA. TO STA.				CONTRACT NO. 76C49				
	PLOT DATE = 3/19/2010	DATE 03/19/10	REVISED -		FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT						



## SUMMARY OF QUANTITIES

					URBAN 100% STATE		
					ROADWAY	TRAFFIC SIGNALS	15TH STREET BRIDGE STR NO 082-0377
SI	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	J000-2A	Y031-1F	X271-2A
	20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	217	217		
	20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	417	417		
	20101700	SUPPLEMENTAL WATERING	UNIT	13	13		
	20200100	EARTH EXCAVATION	CU YD	45,009	45,009		
	20700400	POROUS GRANULAR EMBANKMENT, SPECIAL	CU YD	758	70		688
	20800150	TRENCH BACKFILL	CU YD	535	535		
*	21101615	TOP SOIL FURNISH AND PLACE, 4"	SQ YD	1,284	1,284		
*	25000210	SEEDING, CLASS 2A	ACRE	9.25	9.25		
*	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	852	852		
*	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	852	852		
*	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	852	852		
*	25100115	MULCH, METHOD 2	ACRE	9.25	9.25		
*	25100630	EROSION CONTROL BLANKET	SQ YD	1,109	1,109		
*	25200110	SODDING, SALT TOLERANT	SQ YD	1,284	1,284		
	28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	3,858	3,858		
	28000305	TEMPORARY DITCH CHECKS	FOOT	384	384		
	28000315	AGGREGATE DITCH CHECKS	TON	301	301		
	28000400	PERIMETER EROSION BARRIER	FOOT	4,288	4,288		
	28000500	INLET AND PIPE PROTECTION	EACH	64	64		
	28100107	STONE RIPRAP, CLASS A4	SQ YD	7	7		
	28200200	FILTER FABRIC	SQ YD	7	7		
	31101200	SUB-BASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	28	28		
	35100100	AGGREGATE BASE COURSE, TYPE A	TON	269	269		
	35101100	AGGREGATE BASE COURSE, TYPE A 12"	SQ YD	21,571	21,571		
	35300500	PORTLAND CEMENT CONCRETE BASE COURSE 10"	SQ YD	738	738		
	40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	200	200		
	40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	16	16		
	40600300	AGGREGATE (PRIME COAT)	TON	34	34		
	40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	655	655		
	40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	616	616		

• DENOTES SPECIALTY ITEM

FILE NAME =	USER NAME = pms@rno	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#	PLOT SCALE = 1.0000' / 1"	DRAWN OP	REVISED -			64	82-1-2HB	ST. CLAIR	345	5
	PLOT DATE = 3/19/2010	CHECKED DBM	REVISED -			SCALE: NONE SHEET NO. 1 OF 11 SHEETS STA. TO STA.		CONTRACT NO. 76C49		
						FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

Rev.

## SUMMARY OF QUANTITIES

					URBAN 100% STATE		
SI	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	TRAFFIC SIGNALS	15TH STREET BRIDGE STR NO
					J000-2A	Y031-1F	082-0377 X271-2A
	40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	910	910		
	40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	299	299		
	42000501	PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)	SQ YD	6,451	6,451		
	42000511	PORTLAND CEMENT CONCRETE PAVEMENT 10½" (JOINTED)	SQ YD	4,592	4,592		
	42001300	PROTECTIVE COAT	SQ YD	21,605	21,605		
	42100340	CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 12"	SQ YD	1,516	1,516		
	42100940	PAVEMENT REINFORCEMENT 12"	SQ YD	1,516	1,516		
	42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	66	66		
	42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	208	208		
	42400100	PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH	SQ FT	10,441	10,441		
	42400800	DETECTABLE WARNINGS	SQ FT	138	138		
	44000100	PAVEMENT REMOVAL	SQ YD	14,562	14,562		
	44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 ¼"	SQ YD	1,191	1,191		
	44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	153	153		
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	388	388		
	44000300	CURB REMOVAL	FOOT	881	881		
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	5,779	5,779		
	44000600	SIDEWALK REMOVAL	SQ FT	14,338	14,338		
	44001980	CONCRETE BARRIER REMOVAL	FOOT	59	59		
	44003100	MEDIAN REMOVAL	SQ FT	4,156	4,156		
	44004250	PAVED SHOULDER REMOVAL	SQ YD	5,783	5,783		
	44201349	CLASS C PATCHES, TYPE I, 10 INCH	SQ YD	100	100		
	44201353	CLASS C PATCHES, TYPE II, 10 INCH	SQ YD	100	100		
	44201357	CLASS C PATCHES, TYPE III, 10 INCH	SQ YD	100	100		
	48101200	AGGREGATE SHOULDERS, TYPE B	TON	164	164		
	48101620	AGGREGATE SHOULDERS, TYPE B 10"	SQ YD	56	56		
	48300510	PORTLAND CEMENT CONCRETE SHOULDERS 10½"	SQ YD	2,698	2,698		
	48300700	PORTLAND CEMENT CONCRETE SHOULDERS 12"	SQ YD	1,863	1,863		
	50100300	REMOVAL OF EXISTING STRUCTURES NO. 1	EACH	1			1

• DENOTES SPECIALTY ITEM

FILE NAME =	USER NAME = jmsarno	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN OP	REVISED -		64	82-1-2HB	ST. CLAIR	345	6			
	PLOT SCALE = 1,0000' / IN.	CHECKED DBM	REVISED -		SCALE: NONE	SHEET NO. 2 OF 11 SHEETS	STA.	TO STA.	CONTRACT NO. 76C49			
	PLOT DATE = 3/19/2010	DATE 03/19/10	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							

## SUMMARY OF QUANTITIES

					URBAN 100% STATE		
					ROADWAY	TRAFFIC SIGNALS	15TH STREET BRIDGE STR NO 082-0377
SI	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	J000-2A	Y031-1F	X271-2A
	50100400	REMOVAL OF EXISTING STRUCTURES NO. 2	EACH	1			1
	50157300	PROTECTIVE SHIELD	SQ YD	1,402			1,402
	50200100	STRUCTURE EXCAVATION	CU YD	4,379			4,379
	50300225	CONCRETE STRUCTURES	CU YD	1,254.6			1,254.6
	50300255	CONCRETE SUPERSTRUCTURE	CU YD	825.8			825.8
	50300260	BRIDGE DECK GROOVING	SQ YD	1,521			1,521
	50300285	FORM LINER TEXTURED SURFACE	SQ FT	4,600			4,600
	50300300	PROTECTIVE COAT	SQ YD	1,998			1,998
	50500105	FURNISHING AND ERECTING STRUCTURAL STEEL	L SUM	1			1
	50500505	STUD SHEAR CONNECTORS	EACH	3,771			3,771
	50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	350,190			350,190
	50800515	BAR SPLICERS	EACH	119			119
	50901730	BRIDGE FENCE RAILING	FOOT	286			286
	50901735	BRIDGE FENCE RAILING (SIDEWALK)	FOOT	287			287
	51200959	FURNISHING METAL SHELL PILES 14" x 0.312"	FOOT	9,225			9,225
	51201900	FURNISHING STEEL PILES HP 14X89	FOOT	3,744			3,744
	51202305	DRIVING PILES	FOOT	12,969			12,969
	51204650	PILE SHOES	EACH	39			39
	<del>51203900</del>	<del>TEST PILE STEEL HP 14X89</del>	<del>EACH</del>	<del>39</del>			<del>39</del>
	51500100	NAME PLATES	EACH	1			1
	52000110	PREFORMED JOINT STRIP SEAL	FOOT	122			122
	52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	15			15
	52100530	ANCHOR BOLTS, 1/4"	EACH	16			16
	54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	2	2		
	54213660	PRECAST REINFORCED CONCRETE FLARED END SECTION 15"	EACH	2	2		
	54213663	PRECAST REINFORCED CONCRETE FLARED END SECTION 18"	EACH	2	2		
	54248510	CONCRETE COLLAR	CU YD	10	10		
	550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	178	178		
	550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	129	129		
	550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	74	74		

• DENOTES SPECIALTY ITEM

FILE NAME =	USER NAME = pmsorno	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
%FILE#	PLOT SCALE = 1.0000" / IN.	DRAWN OP	REVISED -			64	82-1-2HB	ST. CLAIR	345	7
	PLOT DATE = 3/19/2010	CHECKED DBM	REVISED -			SCALE: NONE SHEET NO. 3 OF 11 SHEETS STA. TO STA.		CONTRACT NO. 76C49		
						FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

Rev.

## SUMMARY OF QUANTITIES

					URBAN 100% STATE		
SI	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	TRAFFIC SIGNALS	15TH STREET BRIDGE STR NO 082-0377
					J000-2A	Y031-1F	X271-2A
	550A0120	STORM SEWERS, CLASS A, TYPE 1 24"	FOOT	10	10		
	550A0130	STORM SEWERS, CLASS A, TYPE 1 27"	FOOT	243	243		
	550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	947	947		
	550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	318	318		
	550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	216	216		
	550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	510	510		
	550A0430	STORM SEWERS, CLASS A, TYPE 2 30"	FOOT	20	20		
	550A0450	STORM SEWERS, CLASS A, TYPE 2 36"	FOOT	20	20		
	55100500	STORM SEWER REMOVAL 12"	FOOT	1,167	1,167		
	55100900	STORM SEWER REMOVAL 18"	FOOT	213	213		
	55101200	STORM SEWER REMOVAL 24"	FOOT	52	52		
	552A0900	STORM SEWERS JACKED IN PLACE, CLASS A, 24"	FOOT	79	79		
	552A1100	STORM SEWERS JACKED IN PLACE, CLASS A, 30"	FOOT	69	69		
	58700300	CONCRETE SEALER	SQ FT	3,140			3,140
	59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	564			564
	60100060	CONCRETE HEADWALL FOR PIPE DRAINS	EACH	4	4		
	60100915	PIPE DRAINS 6"	FOOT	214	214		
	60100925	PIPE DRAINS 8"	FOOT	42	42		
	60107600	PIPE UNDERDRAINS 4"	FOOT	4,695	4,695		
	60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	244	244		
	60109580	PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	288			288
	60200105	CATCH BASINS, TYPE A, 4' DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	1		
	60200205	CATCH BASINS, TYPE A, 4' DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	6	6		
	60200305	CATCH BASINS, TYPE A, 4' DIAMETER, TYPE 3 FRAME AND GRATE	EACH	4	4		
	60200805	CATCH BASINS, TYPE A, 4' DIAMETER, TYPE 8 GRATE	EACH	4	4		
	60201340	CATCH BASINS, TYPE A, 4' DIAMETER, TYPE 24 FRAME AND GRATE	EACH	16	16		
	60204505	CATCH BASINS, TYPE A, 5' DIAMETER, TYPE 8 GRATE	EACH	1	1		
	60205040	CATCH BASINS, TYPE A, 5' DIAMETER, TYPE 24 FRAME AND GRATE	EACH	4	4		
	60207105	CATCH BASINS, TYPE C, TYPE 3 FRAME AND GRATE	EACH	1	1		
	60208240	CATCH BASINS, TYPE C, TYPE 24 FRAME AND GRATE	EACH	2	2		
	60218300	MANHOLES, TYPE A, 4'- DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	1		

• DENOTES SPECIALTY ITEM

FILE NAME =	USER NAME = pimsarno	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
#FILE#		DRAWN OP	REVISED -		SCALE: NONE	SHEET NO. 4 OF 11 SHEETS	STA.	TO STA.	64	82-1-2HB	ST. CLAIR	345	8
	PLOT SCALE = 1,0000' / IN.	CHECKED DBM	REVISED -							CONTRACT NO. 76C49			
	PLOT DATE = 3/19/2010	DATE 03/19/10	REVISED -										

*Rev.*

## SUMMARY OF QUANTITIES

					URBAN 100% STATE		
					ROADWAY	TRAFFIC SIGNALS	15TH STREET BRIDGE STR NO 082-0377
SI	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	J000-2A	Y031-1F	X271-2A
	60218400	MANHOLES, TYPE A, 4' DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	4	4		
	60221100	MANHOLES, TYPE A, 5' DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	9	9		
	60223800	MANHOLES, TYPE A, 6' DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2	2		
	60224446	MANHOLES, TYPE A, 7' DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1		
	60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	2	2		
	60235700	INLETS, TYPE A, TYPE 3 FRAME AND GRATE	EACH	2	2		
	60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	5	5		
	60237470	INLETS, TYPE A, TYPE 24 FRAME AND GRATE	EACH	8	8		
	60240301	INLETS, TYPE B, TYPE 8 GRATE	EACH	1	1		
	60240328	INLETS, TYPE B, TYPE 24 FRAME AND GRATE	EACH	1	1		
	60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	15	15		
	60258200	MANHOLES TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	4	4		
	60261540	INLETS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE	EACH	6	6		
	60500040	REMOVING MANHOLES	EACH	3	3		
	60500060	REMOVING INLETS	EACH	25	25		
	60602500	CONCRETE GUTTER, TYPE A	FOOT	387	387		
	60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	5,271	5,271		
	60608600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06	FOOT	29	29		
	60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	639	639		
	60622800	CONCRETE MEDIAN, TYPE SM-6.12	SQ FT	446	446		
*	63000001	STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS	FOOT	775.0	775.0		
*	63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	2	2		
*	63100070	TRAFFIC BARRIER TERMINAL, TYPE 5	EACH	1	1		
*	63100089	TRAFFIC BARRIER TERMINAL, TYPE 6B	FOOT	4	4		
*	63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	6	6		
	63200310	GUARDRAIL REMOVAL	FOOT	1,419	1,419		
	63302400	REMOVE AND RE-ERECT  TRAFFIC BARRIER TERMINAL, TYPE 5	EACH	1	1		
	63500105	DELINEATORS	EACH	27	27		
	63500120	DELINEATOR REMOVAL	EACH	11	11		
	63700175	CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT	FOOT	153	153		

\* DENOTES SPECIALTY ITEM

FILE NAME =	USER NAME = pmsarrio	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.I. RTE. 64	SECTION 82-1-2HB	COUNTY ST. CLAIR	TOTAL SHEETS 345	SHEET NO. 9
\$FILEAS		DRAWN OP	REVISED -							
	PLOT SCALE = 1,0000' / IN.	CHECKED DBM	REVISED -		SCALE: NONE	SHEET NO. 5 OF 11 SHEETS	STA.	TO STA.		
	PLOT DATE = 3/19/2010	DATE 03/19/10	REVISED -							
										CONTRACT NO. 76C49

Rev

## SUMMARY OF QUANTITIES

					URBAN 100% STATE		
					ROADWAY	TRAFFIC SIGNALS	15TH STREET BRIDGE STR NO 082-0377
SI	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	J000-2A	Y031-1F	X271-2A
	63700279	CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)	FOOT	498	498		
	63700805	CONCRETE BARRIER TRANSITION	FOOT	10	10		
	63700900	CONCRETE BARRIER BASE	FOOT	36	36		
	66400305	CHAIN LINK FENCE, 6'	FOOT	2,198	2,198		
	66407600	CHAIN LINK GATES, 6' X 12' DOUBLE	EACH	2	2		
	67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	15	15		
	67000600	ENGINEER'S FIELD LABORATORY	CAL MO	15	15		
	67100100	MOBILIZATION	L SUM	1	1		
	70101700	TRAFFIC CONTROL AND PROTECTION	L SUM	1	1		
	70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	300	300		
	70300100	SHORT-TERM PAVEMENT MARKING	FOOT	10,645	10,645		
	70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	89	89		
	70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	38,461	38,461		
	70300250	TEMPORARY PAVEMENT MARKING, LINE 8"	FOOT	1,441	1,441		
	70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1,512	1,512		
	70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	186	186		
	70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	10,976	10,976		
	70400100	TEMPORARY CONCRETE BARRIER	FOOT	4,596	4,596		
	70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	2,754	2,754		
*	72000100	SIGN PANEL - TYPE 1	SQ FT	485	416	69	
*	72000200	SIGN PANEL - TYPE 2	SQ FT	16	16		
*	72000300	SIGN PANEL - TYPE 3	SQ FT	1,501	1,501		
*	72200100	DEMOUNTABLE LEGEND CHARACTERS AND ARROWS	EACH	1	1		
*	72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	17	17		
*	72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	14	14		
*	72400310	REMOVE SIGN PANEL - TYPE 1	SQ FT	30	30		
*	72400320	REMOVE SIGN PANEL - TYPE 2	SQ FT	86	86		
*	72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	SQ FT	9	9		
*	72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	SQ FT	20	20		

\* DENOTES SPECIALTY ITEM

FILE NAME =	USER NAME = pmsarino	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
\$FILE049		DRAWN OP	REVISED -			64	82-1-2HB	ST. CLAIR	345	10	
	PLOT SCALE = 1,0000' / IN.	CHECKED DBM	REVISED -			SCALE: NONE		SHEET NO. 6 OF 11 SHEETS		STA. TO STA.	
	PLOT DATE = 3/19/2010	DATE 03/19/10	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT CONTRACT NO. 76C49					



## SUMMARY OF QUANTITIES

					URBAN 100% STATE		
					ROADWAY	TRAFFIC SIGNALS	15TH STREET BRIDGE STR NO 082-0377
SI	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	J000-2A	Y031-1F	X271-2A
*	72600100	MILEPOST MARKER ASSEMBLY	EACH	1	1		
*	72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	454	454		
*	73300200	OVERHEAD SIGN STRUCTURE - SPAN, TYPE II-A (4'-6" X 5'-3")	FOOT	73	73		
*	73300300	OVERHEAD SIGN STRUCTURE - SPAN, TYPE III-A (5'-0" X 7'-0")	FOOT	87	87		
*	73302170	OVERHEAD SIGN STRUCTURE - CANTILEVER, TYPE II-C-A (36" X 5'-6")	FOOT	29	29		
*	73304000	OVERHEAD SIGN STRUCTURE - BRIDGE MOUNTED	FOOT	20	20		
*	73305000	OVERHEAD SIGN STRUCTURE WALKWAY	FOOT	146	146		
*	73400200	DRILLED SHAFT CONCRETE FOUNDATION	CU YD	71.8	71.8		
*	73600100	REMOVE OVERHEAD SIGN STRUCTURE - SPAN	EACH	2	2		
*	73600200	REMOVE OVERHEAD SIGN STRUCTURE - CANTILEVER	EACH	1	1		
*	73602000	REMOVE OVERHEAD SIGN STRUCTURE - BRIDGE MOUNTED	EACH	1	1		
*	73700300	REMOVE CONCRETE FOUNDATION - OVERHEAD	EACH	9	9		
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	10,982	10,982		
*	78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	2,767	2,767		
*	78001140	PAINT PAVEMENT MARKING - LINE 8"	FOOT	796	796		
*	78001150	PAINT PAVEMENT MARKING - LINE 12"	FOOT	262	262		
*	78003100	PREFORMED PLASTIC PAVEMENT MARKING TYPE B - LETTERS AND SYMBOLS	SQ FT	155	155		
*	78003110	PREFORMED PLASTIC PAVEMENT MARKING TYPE B - LINE 4"	FOOT	8,561	8,561		
*	78003130	PREFORMED PLASTIC PAVEMENT MARKING TYPE B - LINE 6"	FOOT	447	447		
*	78003140	PREFORMED PLASTIC PAVEMENT MARKING TYPE B - LINE 8"	FOOT	1,216	1,216		
*	78003150	PREFORMED PLASTIC PAVEMENT MARKING TYPE B - LINE 12"	FOOT	541	541		
*	78004200	PREFORMED PLASTIC PAVEMENT MARKING TYPE B - INLAID - LETTERS AND SYMBOLS	SQ FT	262	262		
*	78004210	PREFORMED PLASTIC PAVEMENT MARKING TYPE B - INLAID - LINE 4"	FOOT	3,522	3,522		
*	78004230	PREFORMED PLASTIC PAVEMENT MARKING TYPE B - INLAID - LINE 6"	FOOT	102	102		
*	78004240	PREFORMED PLASTIC PAVEMENT MARKING TYPE B - INLAID - LINE 8"	FOOT	110	110		
*	78004250	PREFORMED PLASTIC PAVEMENT MARKING TYPE B - INLAID - LINE 12"	FOOT	113	113		
*	78004280	PREFORMED PLASTIC PAVEMENT MARKING TYPE B - INLAID - LINE 24"	FOOT	72	72		
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	205	205		
*	78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	10	10		
*	78200300	PRISMATIC CURB REFLECTOR	EACH	30	30		

\* DENOTES SPECIALTY ITEM

FILE NAME =	USER NAME = pms@dnrno	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#	PLOT SCALE = 1:8000 1/4" IN.	DRAWN OP	REVISED -			64	82-1-2HB	ST. CLAIR	345	11
	PLOT DATE = 3/19/2010	CHECKED DBM	REVISED -			SCALE: NONE SHEET NO. 7 OF 11 SHEETS STA. TO STA.		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT CONTRACT NO. 76C49		

Rev.

## SUMMARY OF QUANTITIES

					<i>URBAN</i> 100% STATE		
					ROADWAY	TRAFFIC SIGNALS	15TH STREET BRIDGE STR NO 082-0377
SI	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	J000-2A	Y031-1F	X271-2A
*	78200410	GUARDRAIL MARKERS, TYPE A	EACH	17	17		
*	78200530	BARRIER WALL MARKERS, TYPE C	EACH	11	11		
*	78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	12	12		
*	80400100	ELECTRIC SERVICE INSTALLATION	EACH	2		2	
*	81000800	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	10	10		
*	81012300	CONDUIT IN TRENCH, 1" DIA, PVC	FOOT	323		323	
*	81012600	CONDUIT IN TRENCH, 2" DIA, PVC	FOOT	623		623	
*	81012800	CONDUIT IN TRENCH, 3" DIA, PVC	FOOT	284		284	
*	81013000	CONDUIT IN TRENCH, 4" DIA, PVC	FOOT	35		35	
*	81021370	CONDUIT PUSHED, 4" DIA., PVC	FOOT	1,120	1,120		
*	81100200	CONDUIT ATTACHED TO STRUCTURE, 3/4" DIA., <del>PVC COATED</del> GALVANIZED STEEL	FOOT	270	270		
*	81100600	CONDUIT ATTACHED TO STRUCTURE, 2" DIA., <del>PVC COATED</del> GALVANIZED STEEL	FOOT	233		233	
*	81100800	CONDUIT ATTACHED TO STRUCTURE, 3" DIA., <del>PVC COATED</del> GALVANIZED STEEL	FOOT	20	20		
*	81300220	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 6" X 6" X 4"	EACH	5	5		
*	81300550	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 12" X 12" X 6"	EACH	1	1		
*	81400100	HANDHOLE	EACH	7		7	
*	81400300	DOUBLE HANDHOLE	EACH	2		2	
*	81603035	UNIT DUCT, 600V, 2-1C NO.6, 1/C NO.6 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE	FOOT	480	480		
*	81603085	UNIT DUCT, 600V, 3-1C NO.4, 1/C NO.4 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE	FOOT	6,220	6,220		
*	81702450	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/C NO. 10	FOOT	350	350		
*	81800230	AERIAL CABLE, 2-1/C NO. 6 WITH MESSENGER WIRE	FOOT	4,150	4,150		
*	81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	3,123	2,355	768	
*	81900302	TRENCH AND BACKFILL WITH SCREENINGS OR SAND	FOOT	497		497	
*	82102400	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 400 WATT	EACH	20	20		
*	82107300	UNDERPASS LUMINAIRE, 150 WATT, HIGH PRESSURE SODIUM VAPOR	EACH	5	5		
*	83057290	LIGHT POLE, WOOD, 50 FOOT, CLASS 4,	EACH	9	9		
*	83057355	LIGHT POLE, WOOD, 60 FOOT, CLASS 4, WITH 15FT MAST ARM	EACH	20	20		
*	84100110	REMOVAL OF TEMPORARY LIGHTING UNIT	EACH	3	3		
*	84200500	REMOVAL OF EXISTING LIGHTING UNIT, SALVAGE	EACH	17	17		

\* DENOTES SPECIALTY ITEM

FILE NAME =	USER NAME = pmsarino	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#	PLOT SCALE = 1:2000 1" = 10'	DRAWN OP	REVISED -			64	82-1-2HB	ST. CLAIR	345	12
	PLOT DATE = 3/19/2010	CHECKED DBM	REVISED -			SCALE: NONE SHEET NO. 8 OF 11 SHEETS STA. TO STA.		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		CONTRACT NO. 76C49

Rev.

## SUMMARY OF QUANTITIES

					URBAN 100% STATE		
					ROADWAY	TRAFFIC SIGNALS	15TH STREET BRIDGE STR NO 082-0377
SI	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	J000-2A	Y031-1F	X271-2A
*	84200804	REMOVAL OF POLE FOUNDATION	EACH	17	17		
*	85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	2		2	
*	87100160	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, 24F	FOOT	719		719	
*	87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1,246		1,246	
*	87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1,254		1,254	
*	87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	7,064		7,064	
*	87301405	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 16 1 PAIR	FOOT	2,584		2,584	
*	87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	280		280	
*	87502490	TRAFFIC SIGNAL POST, GALVANIZED STEEL 15 FT.	EACH	1		1	
*	87700170	STEEL MAST ARM ASSEMBLY AND POLE, 26 FT.	EACH	1		1	
*	87700190	STEEL MAST ARM ASSEMBLY AND POLE, 30 FT.	EACH	1		1	
*	87700220	STEEL MAST ARM ASSEMBLY AND POLE, 36 FT.	EACH	1		1	
*	87700270	STEEL MAST ARM ASSEMBLY AND POLE, 46 FT.	EACH	1		1	
*	87700280	STEEL MAST ARM ASSEMBLY AND POLE, 48 FT.	EACH	1		1	
*	87700290	STEEL MAST ARM ASSEMBLY AND POLE, 50 FT.	EACH	1		1	
*	87700320	STEEL MAST ARM ASSEMBLY AND POLE, 55 FT.	EACH	1		1	
*	87702730	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 50 FT.	EACH	1		1	
*	87800200	CONCRETE FOUNDATION, TYPE D	FOOT	7		7	
*	87800300	CONCRETE FOUNDATION, TYPE E 24-INCH DIAMETER	FOOT	3		3	
*	87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	10		10	
*	87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	85		85	
*	88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	18		18	
*	88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	11		11	
*	88030070	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, BRACKET MOUNTED	EACH	4		4	
*	88030080	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, MAST ARM MOUNTED	EACH	4		4	
*	88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8		8	
*	88200100	TRAFFIC SIGNAL BACKPLATE	EACH	37		37	
*	88500100	INDUCTIVE LOOP DETECTOR	EACH	8		8	
*	88600100	DETECTOR LOOP, TYPE I	FOOT	2,809		2,809	
*	88800100	PEDESTRIAN PUSH-BUTTON	EACH	8		8	

\* DENOTES SPECIALTY ITEM

FILE NAME =	USER NAME = pmsarno	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
8FILEA*	PLOT SCALE = 1:8000 1/4 IN.	DRAWN OP	REVISED -			64	82-1-2HB	ST. CLAIR	345	13
	PLOT DATE = 3/19/2010	CHECKED DBM	REVISED -			SCALE: NONE SHEET NO. 9 OF 11 SHEETS STA. TO STA.		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		CONTRACT NO. 76C49

*Rev.*

## SUMMARY OF QUANTITIES

					URBAN 100% STATE		
					ROADWAY	TRAFFIC SIGNALS	15TH STREET BRIDGE STR NO 082-0377
SI	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	J000-2A	Y031-1F	X271-2A
*	89502375	REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	10		10	
*	A2005824	TREE, PLATANUS OCCIDENTALIS (SYCAMORE), 3" CALIPER, BALLED AND BURLAPPED	EACH	3	3		
*	C20058G5	SHRUB, RHUS AROMATICA GRO-LOW (GRO-LOW FRAGRANT SUMAC), 5-GALLON	EACH	44	44		
*	K1004467	PERENNIAL PLANTS, ORNAMENTAL HERBACEOUS TYPE	UNIT	1.43	1.43		
	X0320815	SLEEPER SLAB	FOOT	16	16		
	X0320870	BRACED EXCAVATION	CU YD	163			163
	X0321907	STORM SEWERS, TYPE 2, WATER MAIN QUALITY PIPE, 12"	FOOT	82	82		
	X0321908	STORM SEWERS, TYPE 2, WATER MAIN QUALITY PIPE, 15"	FOOT	75	75		
	X0322141	REMOVE TEMPORARY WOOD POLE	EACH	1	1		
	X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	710	710		
	X0322628	FILL EXISTING PIEZOMETER	EACH	4	4		
	X0323080	DRAINAGE SCUPPER, DS-12	EACH	4			4
	X0323255	DRILLED WELL	EACH	3	3		
	X0323988	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	1,521			1,521
*	X0324181	DISCONNECT SIGN LIGHTING AND REMOVE WIRING TO NEAREST SPLICE	EACH	4	4		
	X0712400	TEMPORARY PAVEMENT	SQ YD	1,062	1,062		
*	X0325702	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1		
*	X0326808	INTERSTATE WEEKEND CLOSURE, SPECIAL	L SUM	1	1		
*	X0326690	TOPSOIL PLANTING MIX, 12"	SQ YD	179	179		
	X0462500	SUBMERSIBLE PUMP	EACH	3	3		
	X6370015	CONCRETE BARRIER, SPECIAL, 32" HEIGHT	FOOT	36	36		
*	X7030068	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS, AND SYMBOLS	SQ FT	155	155		
*	X7030070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	8,561	8,561		
*	X7030074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	447	447		
*	X7030076	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	1,216	1,216		
*	X7030078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	541	541		
*	X8730027	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	883		883	
	Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1		
	Z0018800	DRAINAGE SYSTEM	L SUM	1			1

\* DENOTES SPECIALTY ITEM

FILE NAME =	USER NAME = pmearno	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#	PLOT SCALE = 1:8000 1/4 IN.	DRAWN OP	REVISED -			64	82-1-2HB	ST. CLAIR	345	14
	PLOT DATE = 3/19/2010	CHECKED DBM	REVISED -			SCALE: NONE SHEET NO. 10 OF 11 SHEETS STA. TO STA.		CONTRACT NO. 76C49		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

Rev

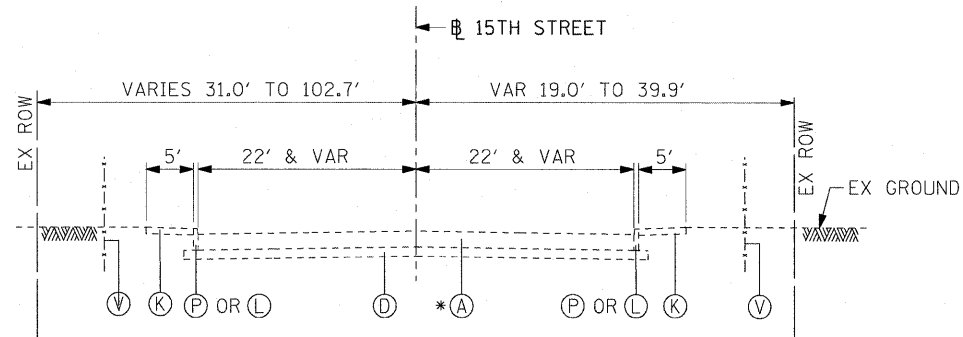
## SUMMARY OF QUANTITIES

				<i>URBAN</i> 100% STATE			
SI	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY J000-2A	TRAFFIC SIGNALS Y031-1F	15TH STREET BRIDGE STR NO 082-0377 X271-2A
*	Z0022800	FENCE REMOVAL	FOOT	2,272	2,272		
*	Z0030270	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, WIDE), TEST LEVEL 3	EACH	8	8		
*	Z0030330	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE), TEST LEVEL 3	EACH	6	6		
*	Z0040000	PIEZOMETERS	EACH	3	3		
*	Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1		
*	<del>Z0076600</del>	<del>TRAINEES</del>	<del>HOURL</del>	<del>500</del>	<del>500</del>		
	48300850	PORTLAND CEMENT CONCRETE SHOULDERS 15½"	SQ YD	529	529		
	48300900	PORTLAND CEMENT CONCRETE SHOULDERS 18"	SQ YD	498	498		
	X5080600	MECHANICAL SPLICERS	EACH	830			830
	52100510	ANCHOR BOLTS, ¾"	EACH	16			16
	52100520	ANCHOR BOLTS, 1"	EACH	14			14
	<del>56300260</del>	<del>CONCRETE ENCASEMENT</del>	<del>CU YD</del>	<del>20.7</del>			<del>20.7</del>
*	Z0024110	FILL DEEP WELL	EACH	4	4		
*	Z0024120	FILL RECORDER WELL	EACH	1	1		
*	X0326931	RECORDER WELL	EACH	1	1		
*	X0326932	REBOUNDABLE REFLECTORIZED VERTICAL MARKERS	EACH	17	17		
*	X0326933	REBOUNDABLE REFLECTORIZED VERTICAL MARKERS REMOVAL	EACH	15	15		
*	X0326934	HIGH DENSITY POLYETHYLENE PIPE 12"	FOOT	215	215		
	60300907	PIPE DRAIN 5"	FOOT	42	42		
*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	6,500	6,500		
	X0326969	DEEP WELL MONITORING	EACH	2	2		
*	66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1		
*	66900530	SOIL DISPOSAL ANALYSIS	EACH	6	6		

• DENOTES SPECIALTY ITEM

FILE NAME =	USER NAME = pmsarno	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#	PLOT SCALE = 1:8000' / IN.	DRAWN OP	REVISED -			64	82-1-2HB	ST. CLAIR	345	14A
	PLOT DATE = 3/19/2010	CHECKED DBM	REVISED -			SCALE: NONE SHEET NO. 11 OF 11 SHEETS STA. TO STA.		CONTRACT NO. 76C49		
		DATE 03/19/10	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

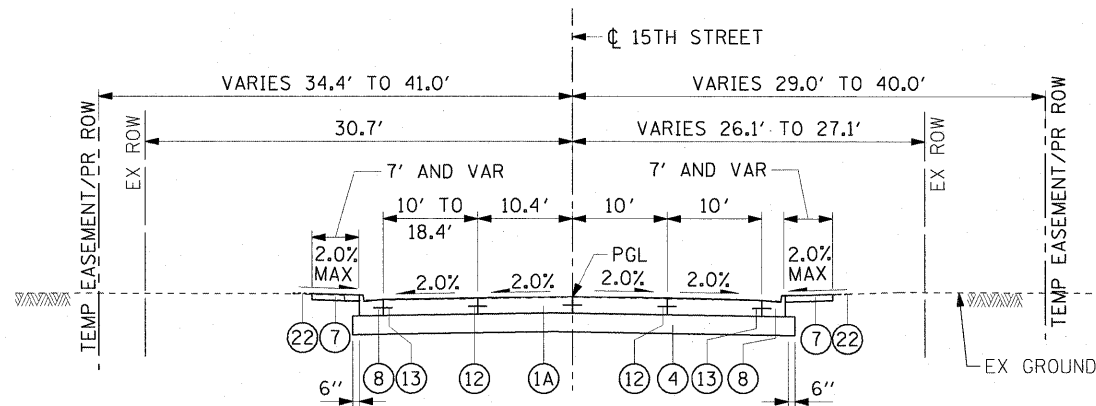
*Rev.*



**EXISTING 15TH STREET**

STA 3+00 TO STA 6+64.19  
STA 9+34.75 TO STA 10+11.18

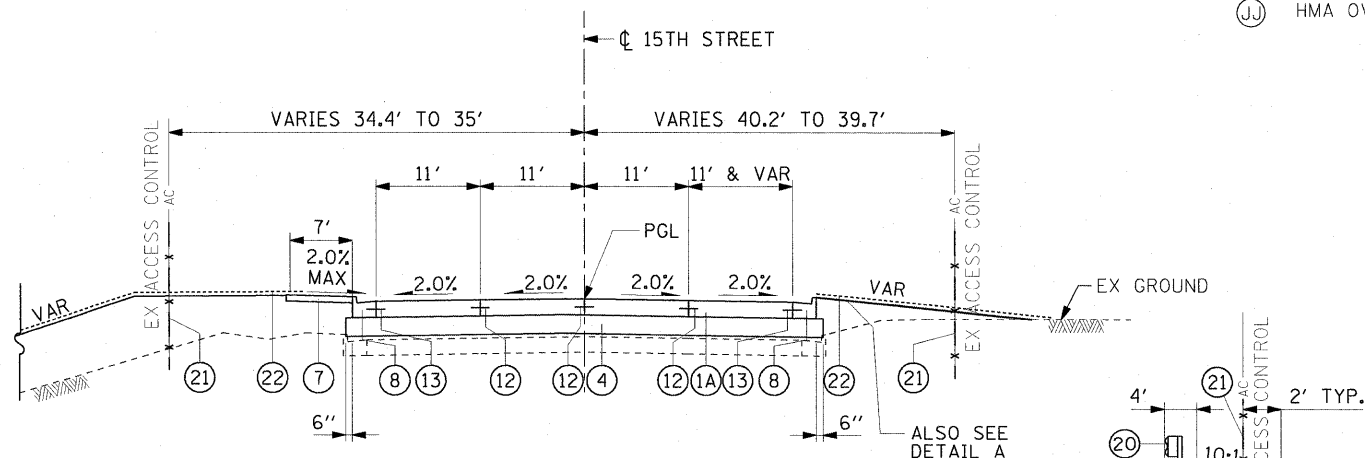
\* WITH PAVEMENT FABRIC



**PROPOSED 15TH STREET**

STA 3+00.00 TO STA 4+02.02

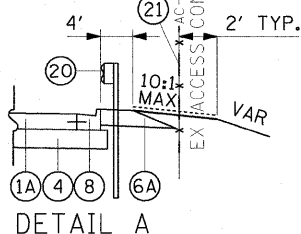
\* SEE INTERSECTION DETAIL PLAN FOR STA 4+02.02 TO STA 5+71.84



**PROPOSED 15TH STREET**

STA 5+71.84 TO STA 6+50.17

\* BRIDGE AND APPROACH PAVEMENT FROM STA 6+50.17 TO STA 9+24.42, SEE STRUCTURAL PLANS FOR STRUCTURAL AND BRIDGE APPROACH PAVEMENT DETAILS



20 - STA 5+51.81 TO STA 6+43.87  
AGGREGATE SHLDS B 10 - STA 5+20.79, 57.4' RT TO STA 6+43.87, 28.6' RT  
SCALE: NONE

**EXISTING LEGEND:**

- (A) PCC PAVEMENT - 10"
- (B) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 3/4"
- (C) SUB-BASE GRANULAR MATERIAL, TYPE A - 4"
- (D) SUB-BASE GRANULAR MATERIAL, TYPE A - 6"
- (E) BITUMINOUS MATERIALS (PRIME COAT)
- (F) AGGREGATE (PRIME COAT)
- (G) LEVELING BINDER (MACHINE METHOD)
- (H) BITUMINOUS CONCRETE BINDER COURSE - 2 1/4"
- (I) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 1 1/2"
- (J) PORTLAND CEMENT CONCRETE BASE COURSE - 8"
- (K) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (L) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (M) COMBINATION CURB AND GUTTER TYPE B-6.12
- (N) COMBINATION CURB AND GUTTER TYPE B-6.12 (SPECIAL)
- (O) PCC PAVEMENT - 10" AND VARIES
- (P) CONCRETE CURB, TYPE B - 6" HIGH
- (Q) TYPE B CURB (MODIFIED)
- (R) PCC MEDIAN SURFACE - 4"
- (S) CONCRETE MEDIAN SURFACE - 4"
- (T) PORTLAND CEMENT CONCRETE BASE COURSE - 9"
- (U) CONCRETE GUTTER
- (V) FENCE
- (W) LONGITUDINAL METAL JOINT
- (X) EXISTING CURB
- (Y) CONCRETE RETAINING WALL
- (Z) BITUMINOUS CONCRETE BINDER COURSE - 1 1/2"
- (AA) STABILIZED SHOULDER
- (BB) AGGREGATE BASE COURSE, TYPE A - 4"
- (CC) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (CRPCCP) - VARIES FROM 12" TO 13 1/4"
- (DD) PIPE UNDERDRAINS - 6"
- (EE) STABILIZED SUB-BASE - 4"
- (FF) AGGREGATE SHOULDER TYPE A
- (GG) INCIDENTAL BITUMINOUS SURFACING
- (HH) CONCRETE GUTTER (MODIFIED)
- (II) HMA SURFACE COURSE, MIX "C", N70 - 2"
- (JJ) HMA OVERLAY

**PROPOSED LEGEND:**

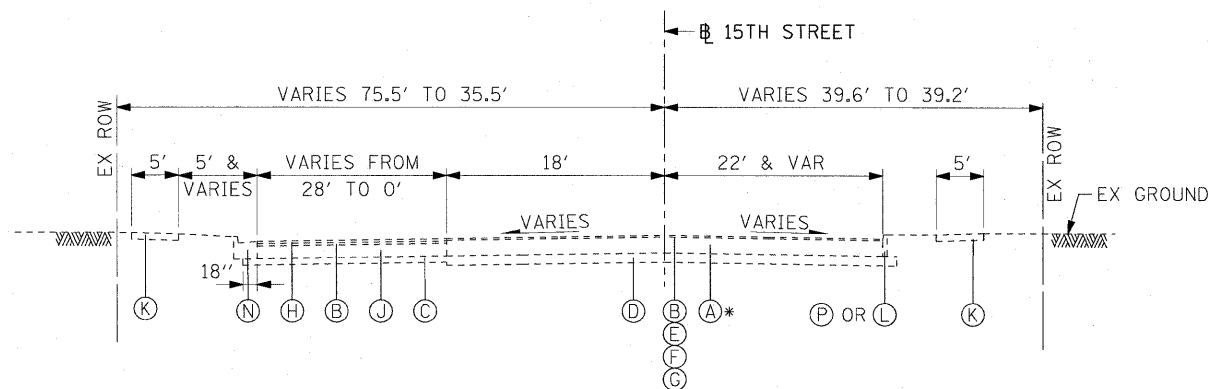
- (1) PORTLAND CEMENT CONCRETE PAVEMENT
  - (1A) - 10" (JOINTED) (LOCAL ROADS)
  - (1B) - 10 1/2" (JOINTED) (RAMPS)
- (2) HOT-MIX ASPHALT OVERLAY - 2 1/4"
  - (2a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 - 1 1/2"
  - (2b) LEVELING BINDER, (MACHINE METHOD), N70 - 3/4"
- (3) PORTLAND CEMENT CONCRETE BASE COURSE - 10"
- (4) AGGREGATE BASE COURSE, TYPE A, 12"
- (5) PORTLAND CEMENT CONCRETE SHOULDERS
  - (5A) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (5B) - 12" (WB & EB I-64 TIE-IN)
  - (5C) - 15 1/2" (WB I-64 GOSES)
  - (5D) - 18" (EB I-64 GOSES)
- (6) AGGREGATE SHOULDERS, TYPE B
  - (6A) - 10" (FOR 10" PCC JT)
  - (6B) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (6C) - 11 1/4" (FOR TEMPORARY PAVEMENT INTERSTATE)
  - (6D) - 15 1/2" (FOR HMA SC "D" N70 - 3 1/2" AND PCC SHOULDERS 12")
  - (6E) - 18" (FOR HMA SC "D" N70 - 6" AND PCC SHOULDERS 12")
- (7) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (8) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (9) POROUS GRANULAR EMBANKMENT, SPECIAL
- (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06
- (11) CONCRETE MEDIAN, TYPE SM-6.12
- (12) #6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / #6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (13) #6 TIE BARS, 24" LONG AT 24" C-C (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (14) PIPE UNDERDRAINS - 4"
- (15) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT - 12"
- (16) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT
- (17) AGGREGATE BASE COURSE, TYPE A, THICKNESS AS SPECIFIED ON SECTION
- (18) CONCRETE GUTTER TYPE A
- (19) CONCRETE MEDIAN SURFACE - 4"
- (20) STEEL PLATE BEAM GUARDRAIL, TYPE A, 6' POSTS OR TRAF BAR TERM
- (21) CHAIN LINK FENCE - 6'
- (22) SEEDING AND MULCHING (SEE SEEDING AND MULCHING SCHEDULE)
- (23) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- (24) CONCRETE CURB AND GUTTER OUTLET SPECIAL
- (25) HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- (26) TEMPORARY PAVEMENT (INTERSTATE) - 11 1/4"
  - (26a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) - 1 3/4"
  - (26b) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - 9/2"
- (27) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 3 1/2"
- (28) BITUMINOUS MATERIALS (PRIME COAT)
- (29) AGGREGATE (PRIME COAT)
- (30) HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3 1/2"
- (32) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 6"

**NOTES:**

1. DUE THE SKEWED NATURE OF THE ROADWAYS IN THIS PROJECT, THE FOLLOWINGS ARE THE COMPASS DESIGNATIONS TO THE ROADWAYS:  
  
15TH STREET (NORTH-SOUTH), BAUGH AVENUE (EAST-WEST), ST. CLAIR AVENUE (EAST-WEST), WB I-64 (WEST)
2. SEE HOT-MIX ASPHALT MIXTURE REQUIREMENTS ON TYPICAL SECTIONS SHEET 3 OF 14.

FILE NAME =	USER NAME = pmsorno	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SECTIONS</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
#FILE#		DRAWN OP	REVISED -			64	82-1-2HB	ST. CLAIR	345	15	
PLOT SCALE = 10,000' / IN.		CHECKED DBM	REVISED -			CONTRACT NO. 76C49					
PLOT DATE = 3/19/2010		DATE 03/19/10	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					
						SCALE: NONE	SHEET NO. 1 OF 14 SHEETS	STA.	TO STA.		

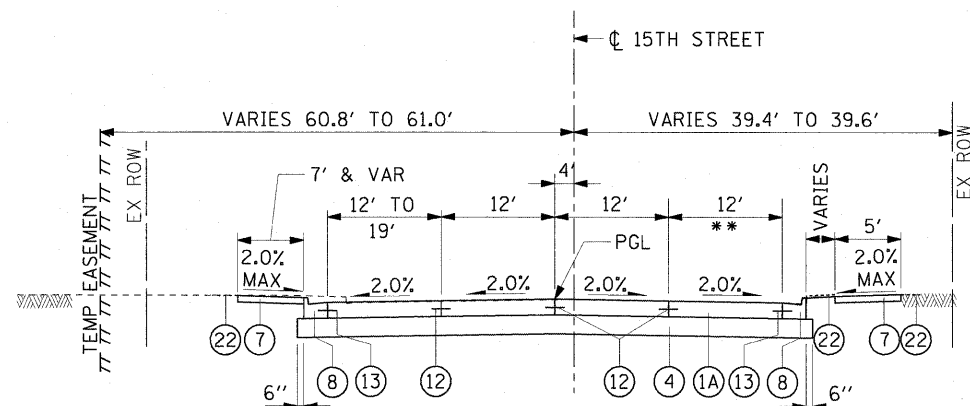




**EXISTING 15TH STREET**

STA 10+11.18 TO STA 13+92.79

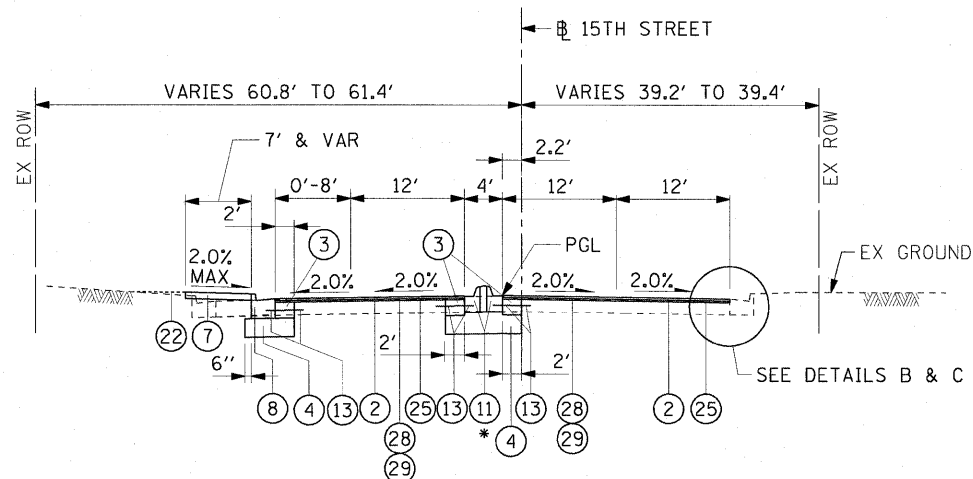
\* WITH PAVEMENT FABRIC



**PROPOSED 15TH STREET**

STA 10+38.77 TO STA 11+60.00

\* SEE INTERSECTION DETAIL PLAN FOR STA 9+24.42 TO STA 10+38.77  
 \*\* 10' PAVEMENT WIDTH TRANSITION TO MEET EXISTING FROM STA 11+50.00 TO STA 11+60.00



**PROPOSED 15TH STREET**

STA 11+60.00 TO STA 13+92.79

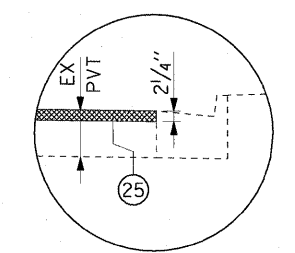
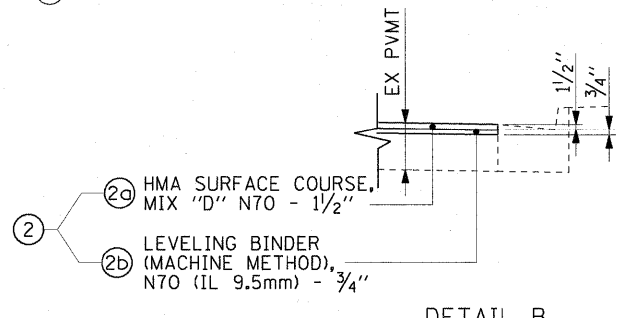
\* FROM STA 12+80.47 TO STA 13+92.79

**EXISTING LEGEND:**

- (A) PCC PAVEMENT - 10"
- (B) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 3/4"
- (C) SUB-BASE GRANULAR MATERIAL, TYPE A - 4"
- (D) SUB-BASE GRANULAR MATERIAL, TYPE A - 6"
- (E) BITUMINOUS MATERIALS (PRIME COAT)
- (F) AGGREGATE (PRIME COAT)
- (G) LEVELING BINDER (MACHINE METHOD)
- (H) BITUMINOUS CONCRETE BINDER COURSE - 2 1/4"
- (I) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 1 1/2"
- (J) PORTLAND CEMENT CONCRETE BASE COURSE - 8"
- (K) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (L) COMBINATION CURB AND GUTTER TYPE B-6.12
- (M) COMBINATION CURB AND GUTTER TYPE B-6.24 (SPECIAL)
- (N) COMBINATION CURB AND GUTTER TYPE B-6.24
- (O) PCC PAVEMENT - 10" AND VARIES
- (P) CONCRETE CURB, TYPE B - 6" HIGH
- (Q) TYPE B CURB (MODIFIED)
- (R) PCC MEDIAN SURFACE - 4"
- (S) CONCRETE MEDIAN SURFACE - 4"
- (T) PORTLAND CEMENT CONCRETE BASE COURSE - 9"
- (U) CONCRETE GUTTER
- (V) FENCE
- (W) LONGITUDINAL METAL JOINT
- (X) EXISTING CURB
- (Y) CONCRETE RETAINING WALL
- (Z) BITUMINOUS CONCRETE BINDER COURSE - 1 1/2"
- (AA) STABILIZED SHOULDER
- (BB) AGGREGATE BASE COURSE, TYPE A - 4"
- (CC) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (CRPCCP) - VARIES FROM 12" TO 13 1/4"
- (DD) PIPE UNDERDRAINS - 6"
- (EE) STABILIZED SUB-BASE - 4"
- (FF) AGGREGATE SHOULDER TYPE A
- (GG) INCIDENTAL BITUMINOUS SURFACING
- (HH) CONCRETE GUTTER (MODIFIED)
- (II) HMA SURFACE COURSE, MIX "C", N70 - 2"
- (JJ) HMA OVERLAY

**PROPOSED LEGEND:**

- (1) PORTLAND CEMENT CONCRETE PAVEMENT
  - (1A) - 10" (JOINTED) (LOCAL ROADS)
  - (1B) - 10 1/2" (JOINTED) (RAMPS)
- (2) HOT-MIX ASPHALT OVERLAY - 2 1/4"
  - (2a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 - 1 1/2"
  - (2b) LEVELING BINDER, (MACHINE METHOD), N70 - 3/4"
- (3) PORTLAND CEMENT CONCRETE BASE COURSE - 10"
- (4) AGGREGATE BASE COURSE, TYPE A, 12"
- (5) PORTLAND CEMENT CONCRETE SHOULDERS
  - (5A) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (5B) - 12" (WB & EB I-64 TIE-IN)
  - (5C) - 15 1/2" (WB I-64 GOSES)
  - (5D) - 18" (EB I-64 GOSES)
- (6) AGGREGATE SHOULDERS, TYPE B
  - (6A) - 10" (FOR 10" PCC JT)
  - (6B) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (6C) - 11 1/4" (FOR TEMPORARY PAVEMENT INTERSTATE)
  - (6D) - 15 1/2" (FOR HMA SC "D" N70 - 3 1/2" AND PCC SHOULDERS 12")
  - (6E) - 18" (FOR HMA SC "D" N70 - 6" AND PCC SHOULDERS 12")
- (7) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (8) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (9) POROUS GRANULAR EMBANKMENT, SPECIAL
- (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06
- (11) CONCRETE MEDIAN, TYPE SM-6.12
- (12) \*6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / \*6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (13) \*6 TIE BARS, 24" LONG AT 24" C-C (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (14) PIPE UNDERDRAINS - 4"
- (15) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT - 12"
- (16) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT
- (17) AGGREGATE BASE COURSE, TYPE A, THICKNESS AS SPECIFIED ON SECTION
- (18) CONCRETE GUTTER TYPE A
- (19) CONCRETE MEDIAN SURFACE - 4"
- (20) STEEL PLATE BEAM GUARDRAIL, TYPE A, 6' POSTS OR TRAF BAR TERM
- (21) CHAIN LINK FENCE - 6'
- (22) SEEDING AND MULCHING (SEE SEEDING AND MULCHING SCHEDULE)
- (23) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- (24) CONCRETE CURB AND GUTTER OUTLET SPECIAL
- (25) HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- (26) TEMPORARY PAVEMENT (INTERSTATE) - 11 1/4"
  - (26a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) - 1 3/4"
  - (26b) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - 9/2"
- (27) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 3 1/2"
- (28) BITUMINOUS MATERIALS (PRIME COAT)
- (29) AGGREGATE (PRIME COAT)
- (30) HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3 1/2"
- (32) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 6"



**NOTES:**

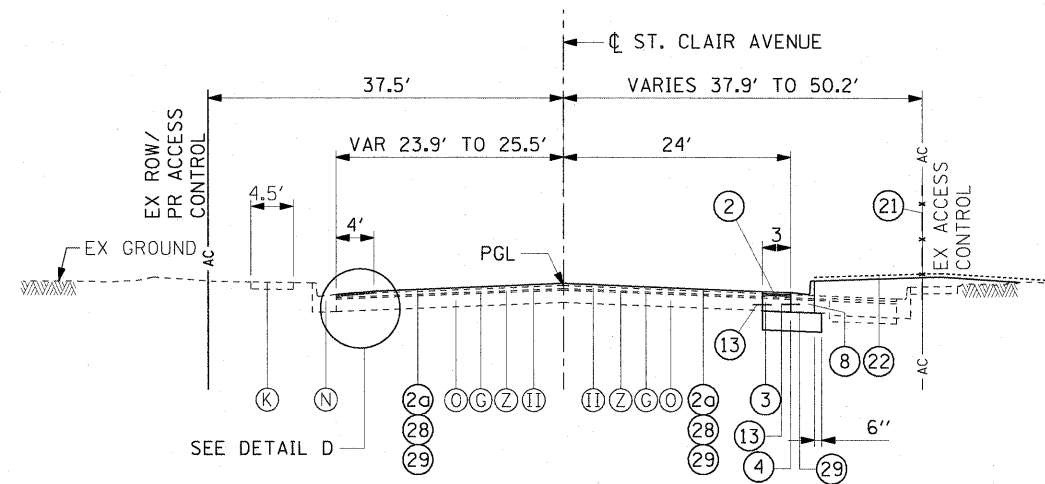
- DUE THE SKEWED NATURE OF THE ROADWAYS IN THIS PROJECT, THE FOLLOWINGS ARE THE COMPASS DESIGNATIONS TO THE ROADWAYS:  
  
15TH STREET (NORTH-SOUTH), BAUGH AVENUE (EAST-WEST), ST. CLAIR AVENUE (EAST-WEST), WB I-64 (WEST)
- SEE HOT-MIX ASPHALT MIXTURE REQUIREMENTS ON TYPICAL SECTIONS SHEET 3 OF 14.

FILE NAME =	USER NAME = pmsarno	DESIGNED OP	REVISED -
*FILE#		DRAWN OP	REVISED -
		CHECKED DBM	REVISED -
		DATE 03/19/10	REVISED -

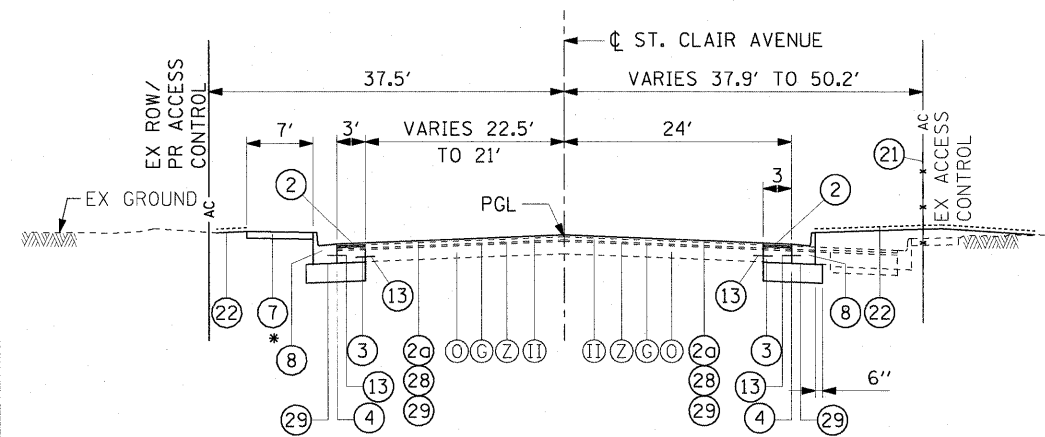
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>TYPICAL SECTIONS</b>	
SCALE: NONE	SHEET NO. 2 OF 14 SHEETS
STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	16
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	



**PROPOSED ST. CLAIR AVENUE**  
STA 28+61.01 TO STA 32+44.05



**PROPOSED ST. CLAIR AVENUE**  
STA 32+44.05 TO STA 34+88.67

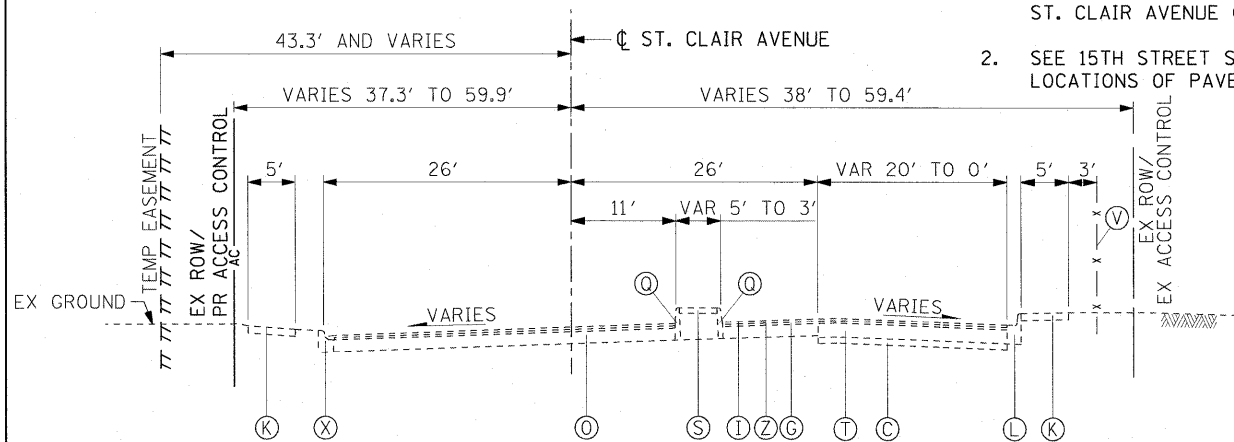
\* 10' TRANSITION FROM 6.5' TO 7'  
STA 32+44.05 TO STA 32+54.13

**NOTES:**

1. DUE THE SKEWED NATURE OF THE ROADWAYS IN THIS PROJECT, THE FOLLOWINGS ARE THE COMPASS DESIGNATIONS TO THE ROADWAYS:

15TH STREET (NORTH-SOUTH), BAUGH AVENUE (EAST-WEST), ST. CLAIR AVENUE (EAST-WEST), WB I-64 (WEST)

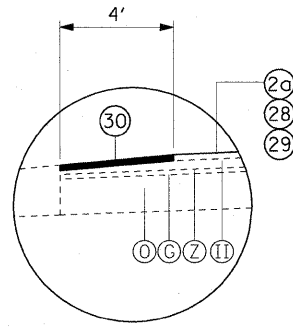
2. SEE 15TH STREET SIGNING AND PAVEMENT MARKING SHEETS FOR THE EXACT LOCATIONS OF PAVEMENT MARKING PLACEMENTS.



**EXISTING ST. CLAIR AVENUE**  
STA 34+88.67 TO STA 36+38.48

- EXISTING LEGEND:**
- (A) PCC PAVEMENT - 10"
  - (B) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 3/4"
  - (C) SUB-BASE GRANULAR MATERIAL, TYPE A - 4"
  - (D) SUB-BASE GRANULAR MATERIAL, TYPE A - 6"
  - (E) BITUMINOUS MATERIALS (PRIME COAT)
  - (F) AGGREGATE (PRIME COAT)
  - (G) LEVELING BINDER (MACHINE METHOD)
  - (H) BITUMINOUS CONCRETE BINDER COURSE - 2 1/4"
  - (I) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 1 1/2"
  - (J) PORTLAND CEMENT CONCRETE BASE COURSE - 8"
  - (K) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
  - (L) COMBINATION CURB AND GUTTER TYPE B-6.12
  - (M) COMBINATION CURB AND GUTTER TYPE B-6.12 (SPECIAL)
  - (N) COMBINATION CURB AND GUTTER TYPE B-6.24
  - (O) PCC PAVEMENT - 10" AND VARIES
  - (P) CONCRETE CURB, TYPE B - 6" HIGH
  - (Q) TYPE B CURB (MODIFIED)
  - (R) PCC MEDIAN SURFACE - 4"
  - (S) CONCRETE MEDIAN SURFACE - 4"
  - (T) PORTLAND CEMENT CONCRETE BASE COURSE - 9"
  - (U) CONCRETE GUTTER
  - (V) FENCE
  - (W) LONGITUDINAL METAL JOINT
  - (X) EXISTING CURB
  - (Y) CONCRETE RETAINING WALL
  - (Z) BITUMINOUS CONCRETE BINDER COURSE - 1 1/2"
  - (AA) STABILIZED SHOULDER
  - (BB) AGGREGATE BASE COURSE, TYPE A - 4"
  - (CC) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (CRPCCP) - VARIES FROM 12" TO 13 1/4"
  - (DD) PIPE UNDERDRAINS - 6"
  - (EE) STABILIZED SUB-BASE - 4"
  - (FF) AGGREGATE SHOULDER TYPE A
  - (GG) INCIDENTAL BITUMINOUS SURFACING
  - (HH) CONCRETE GUTTER (MODIFIED)
  - (II) HMA SURFACE COURSE, MIX "C", N70 - 2"
  - (JJ) HMA OVERLAY

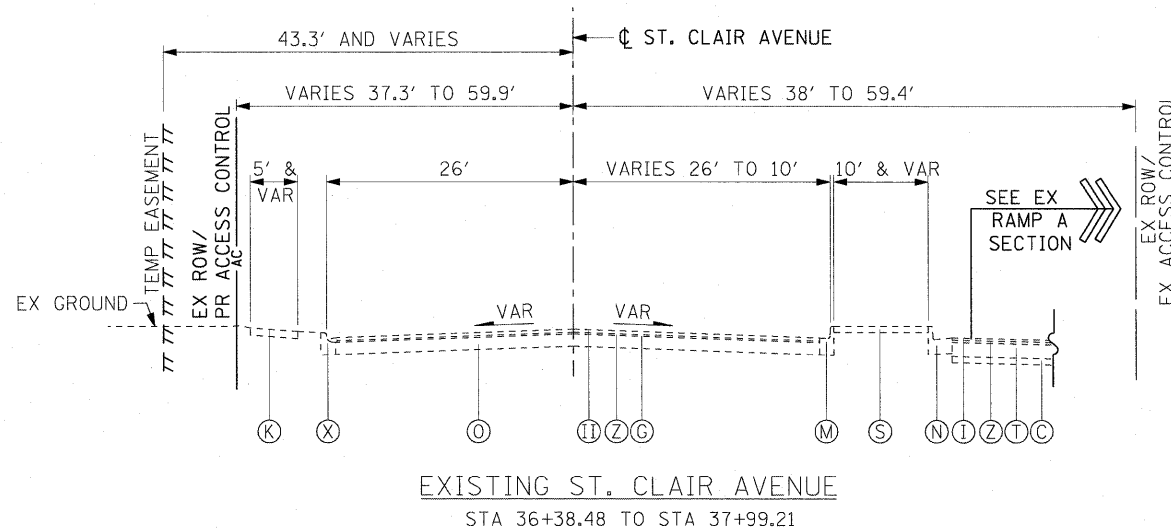
- PROPOSED LEGEND:**
- (1) PORTLAND CEMENT CONCRETE PAVEMENT
    - (1A) - 10" (JOINTED) (LOCAL ROADS)
    - (1B) - 10 1/2" (JOINTED) (RAMPS)
  - (2) HOT-MIX ASPHALT OVERLAY - 2 1/4"
    - (2a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 - 1 1/2"
    - (2b) LEVELING BINDER, (MACHINE METHOD), N70 - 3/4"
  - (3) PORTLAND CEMENT CONCRETE BASE COURSE - 10"
  - (4) AGGREGATE BASE COURSE, TYPE A, 12"
  - (5) PORTLAND CEMENT CONCRETE SHOULDERS
    - (5A) - 10 1/2" (FOR 10 1/2" PCC JT)
    - (5B) - 12" (WB & EB I-64 TIE-IN)
    - (5C) - 15 1/2" (WB I-64 GOSES)
    - (5D) - 18" (EB I-64 GOSES)
  - (6) AGGREGATE SHOULDERS, TYPE B
    - (6A) - 10" (FOR 10" PCC JT)
    - (6B) - 10 1/2" (FOR 10 1/2" PCC JT)
    - (6C) - 11 1/4" (FOR TEMPORARY PAVEMENT INTERSTATE)
    - (6D) - 15 1/2" (FOR HMA SC "D" N70 - 3 1/2" AND PCC SHOULDERS 12")
    - (6E) - 18" (FOR HMA SC "D" N70 - 6" AND PCC SHOULDERS 12")
  - (7) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
  - (8) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
  - (9) POROUS GRANULAR EMBANKMENT, SPECIAL
  - (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06
  - (11) CONCRETE MEDIAN, TYPE SM-6.12
  - (12) #6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / #6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
  - (13) #6 TIE BARS, 24" LONG AT 24" C-C (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
  - (14) PIPE UNDERDRAINS - 4"
  - (15) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT - 12"
  - (16) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT
  - (17) AGGREGATE BASE COURSE, TYPE A, THICKNESS AS SPECIFIED ON SECTION
  - (18) CONCRETE GUTTER TYPE A
  - (19) CONCRETE MEDIAN SURFACE - 4"
  - (20) STEEL PLATE BEAM GUARDRAIL, TYPE A, 6' POSTS OR TRAF BAR TERM
  - (21) CHAIN LINK FENCE - 6'
  - (22) SEEDING AND MULCHING (SEE SEEDING AND MULCHING SCHEDULE)
  - (23) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
  - (24) CONCRETE CURB AND GUTTER OUTLET SPECIAL
  - (25) HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
  - (26) TEMPORARY PAVEMENT (INTERSTATE) - 11 1/4"
    - (26a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) - 1 3/4"
    - (26b) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - 9/2"
  - (27) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 3 1/2"
  - (28) BITUMINOUS MATERIALS (PRIME COAT)
  - (29) AGGREGATE (PRIME COAT)
  - (30) HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
  - (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3 1/2"
  - (32) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 6"



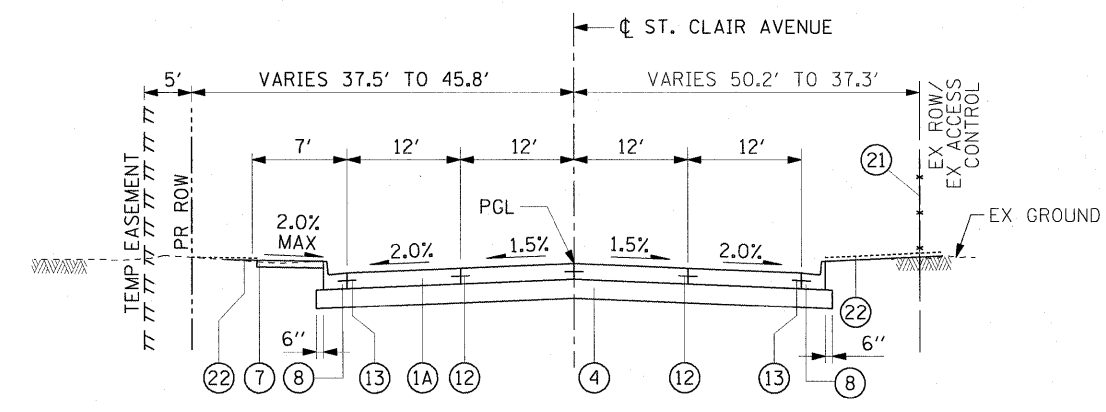
(30) HMA SURFACE REMOVAL - VARIABLE DEPTH SHALL BE 1 1/2" AT THE EDGE OF PAVEMENT AND 0" AT THE DIMENSION SHOWN IN DETAIL D.

MIXTURE TYPE	AC TYPE	AIR VOIDS	RAP %
HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70	PG 64-22	4% @ 70 Gyr	10%
LEVELING BINDER, (MACHINE METHOD), N70	PG 64-22	4% @ 70 Gyr	20%
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	PG 64-22/58-22	4% @ 70 Gyr	20%
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	PG 64-22	4% @ 90 Gyr	20%

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN

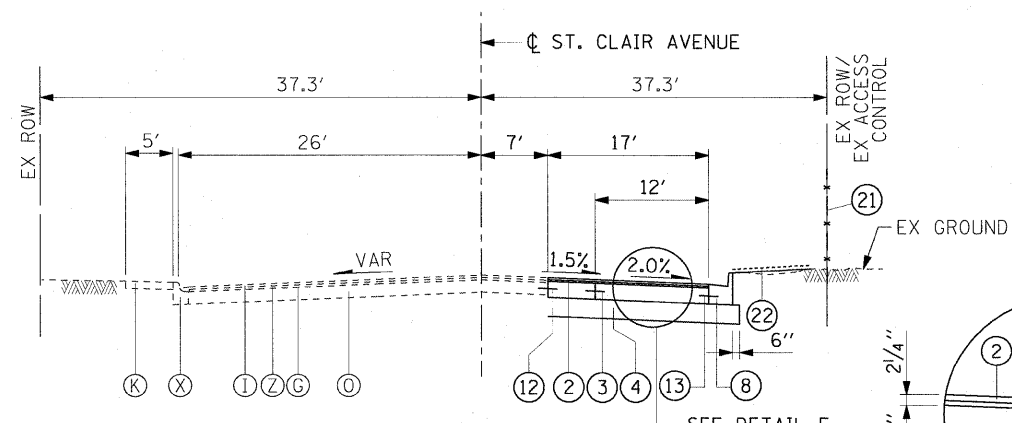


EXISTING ST. CLAIR AVENUE  
STA 36+38.48 TO STA 37+99.21

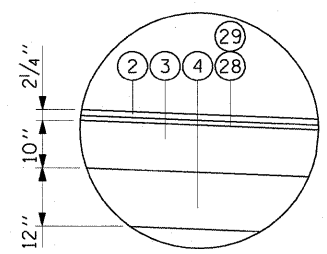


PROPOSED ST. CLAIR AVENUE  
\* STA 34+88.67 TO STA 37+99.21

\* SEE INTERSECTION DETAIL PLAN FOR DETAILS AS WELL.  
CURB AND GUTTER SHOWN WOULD NOT APPLY.  
NOTE ADDITIONAL JOINTS APPLY AT WIDER SECTIONS.



PROPOSED ST. CLAIR AVENUE  
STA 37+99.21 TO STA 39+88.38



DETAIL E

EXISTING LEGEND:

- (A) PCC PAVEMENT - 10"
- (B) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 3/4"
- (C) SUB-BASE GRANULAR MATERIAL, TYPE A - 4"
- (D) SUB-BASE GRANULAR MATERIAL, TYPE A - 6"
- (E) BITUMINOUS MATERIALS (PRIME COAT)
- (F) AGGREGATE (PRIME COAT)
- (G) LEVELING BINDER (MACHINE METHOD)
- (H) BITUMINOUS CONCRETE BINDER COURSE - 2 1/4"
- (I) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 1 1/2"
- (J) PORTLAND CEMENT CONCRETE BASE COURSE - 8"
- (K) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (L) COMBINATION CURB AND GUTTER TYPE B-6.12
- (M) COMBINATION CURB AND GUTTER TYPE B-6.12 (SPECIAL)
- (N) COMBINATION CURB AND GUTTER TYPE B-6.24
- (O) PCC PAVEMENT - 10" AND VARIES
- (P) CONCRETE CURB, TYPE B - 6" HIGH
- (Q) TYPE B CURB (MODIFIED)
- (R) PCC MEDIAN SURFACE - 4"
- (S) CONCRETE MEDIAN SURFACE - 4"
- (T) PORTLAND CEMENT CONCRETE BASE COURSE - 9"
- (U) CONCRETE GUTTER
- (V) FENCE
- (W) LONGITUDINAL METAL JOINT
- (X) EXISTING CURB
- (Y) CONCRETE RETAINING WALL
- (Z) BITUMINOUS CONCRETE BINDER COURSE - 1 1/2"
- (AA) STABILIZED SHOULDER
- (BB) AGGREGATE BASE COURSE, TYPE A - 4"
- (CC) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (CRPCCP) - VARIES FROM 12" TO 13 1/4"
- (DD) PIPE UNDERDRAINS - 6"
- (EE) STABILIZED SUB-BASE - 4"
- (FF) AGGREGATE SHOULDER TYPE A
- (GG) INCIDENTAL BITUMINOUS SURFACING
- (HH) CONCRETE GUTTER (MODIFIED)
- (II) HMA SURFACE COURSE, MIX "C", N70 - 2"
- (JJ) HMA OVERLAY

PROPOSED LEGEND:

- (1) PORTLAND CEMENT CONCRETE PAVEMENT
  - (1A) - 10" (JOINTED) (LOCAL ROADS)
  - (1B) - 10 1/2" (JOINTED) (RAMPS)
- (2) HOT-MIX ASPHALT OVERLAY - 2 1/4"
  - (2a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 - 1 1/2"
  - (2b) LEVELING BINDER, (MACHINE METHOD), N70 - 3/4"
- (3) PORTLAND CEMENT CONCRETE BASE COURSE - 10"
- (4) AGGREGATE BASE COURSE, TYPE A, 12"
- (5) PORTLAND CEMENT CONCRETE SHOULDERS
  - (5A) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (5B) - 12" (WB & EB I-64 TIE-IN)
  - (5C) - 15 1/2" (WB I-64 GOSES)
  - (5D) - 18" (EB I-64 GOSES)
- (6) AGGREGATE SHOULDERS, TYPE B
  - (6A) - 10" (FOR 10" PCC JT)
  - (6B) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (6C) - 11 1/4" (FOR TEMPORARY PAVEMENT INTERSTATE)
  - (6D) - 15 1/2" (FOR HMA SC "D" N70 - 3 1/2" AND PCC SHOULDERS 12")
  - (6E) - 18" (FOR HMA SC "D" N70 - 6" AND PCC SHOULDERS 12")
- (7) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (8) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (9) POROUS GRANULAR EMBANKMENT, SPECIAL
- (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06
- (11) CONCRETE MEDIAN, TYPE SM-6.12
- (12) \*6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / \*6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (13) \*6 TIE BARS, 24" LONG AT 24" C-C (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (14) PIPE UNDERDRAINS - 4"
- (15) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT - 12"
- (16) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT
- (17) AGGREGATE BASE COURSE, TYPE A, THICKNESS AS SPECIFIED ON SECTION
- (18) CONCRETE GUTTER TYPE A
- (19) CONCRETE MEDIAN SURFACE - 4"
- (20) STEEL PLATE BEAM GUARDRAIL, TYPE A, 6' POSTS OR TRAF BAR TERM
- (21) CHAIN LINK FENCE - 6'
- (22) SEEDING AND MULCHING (SEE SEEDING AND MULCHING SCHEDULE)
- (23) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- (24) CONCRETE CURB AND GUTTER OUTLET SPECIAL
- (25) HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- (26) TEMPORARY PAVEMENT (INTERSTATE) - 11 1/4"
  - (26a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) - 1 3/4"
  - (26b) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - 9/2"
- (27) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 3/2"
- (28) BITUMINOUS MATERIALS (PRIME COAT)
- (29) AGGREGATE (PRIME COAT)
- (30) HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3 1/2"
- (32) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 6"

NOTES:

1. DUE TO THE SKEWED NATURE OF THE ROADWAYS IN THIS PROJECT, THE FOLLOWINGS ARE THE COMPASS DESIGNATIONS TO THE ROADWAYS:  
  
15TH STREET (NORTH-SOUTH), BAUGH AVENUE (EAST-WEST), ST. CLAIR AVENUE (EAST-WEST), WB I-64 (WEST)
2. SEE HOT-MIX ASPHALT MIXTURE REQUIREMENTS ON TYPICAL SECTIONS SHEET 3 OF 14.

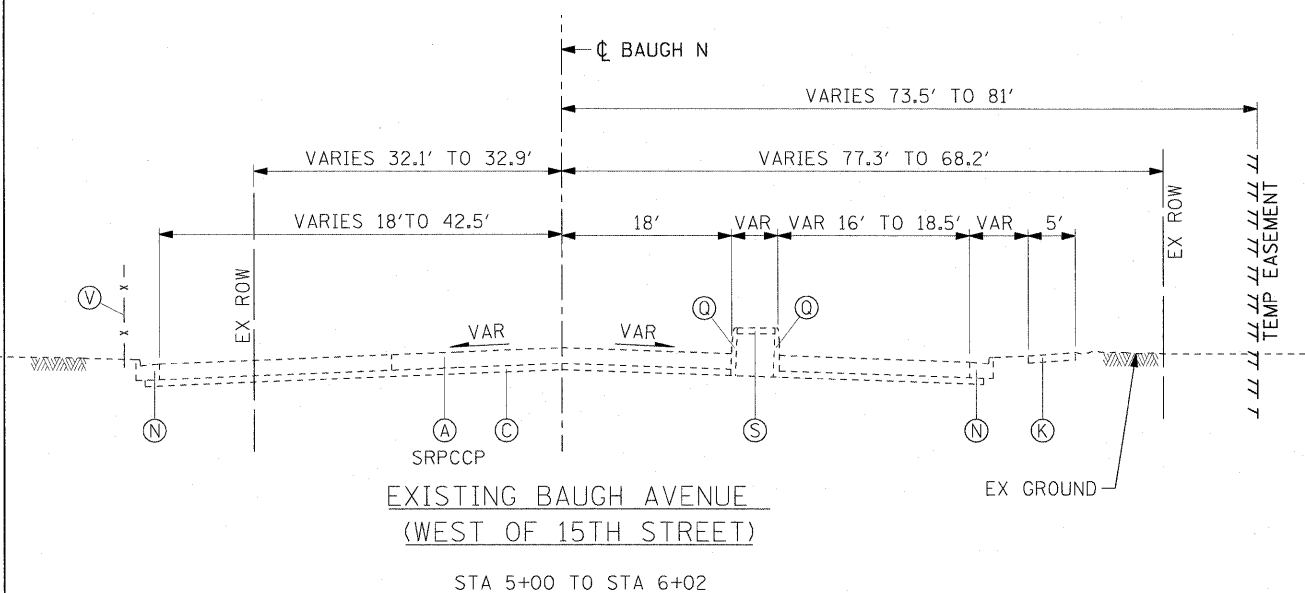
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCALE: NONE		SHEET NO. 4 OF 14 SHEETS		STA.	TO STA.
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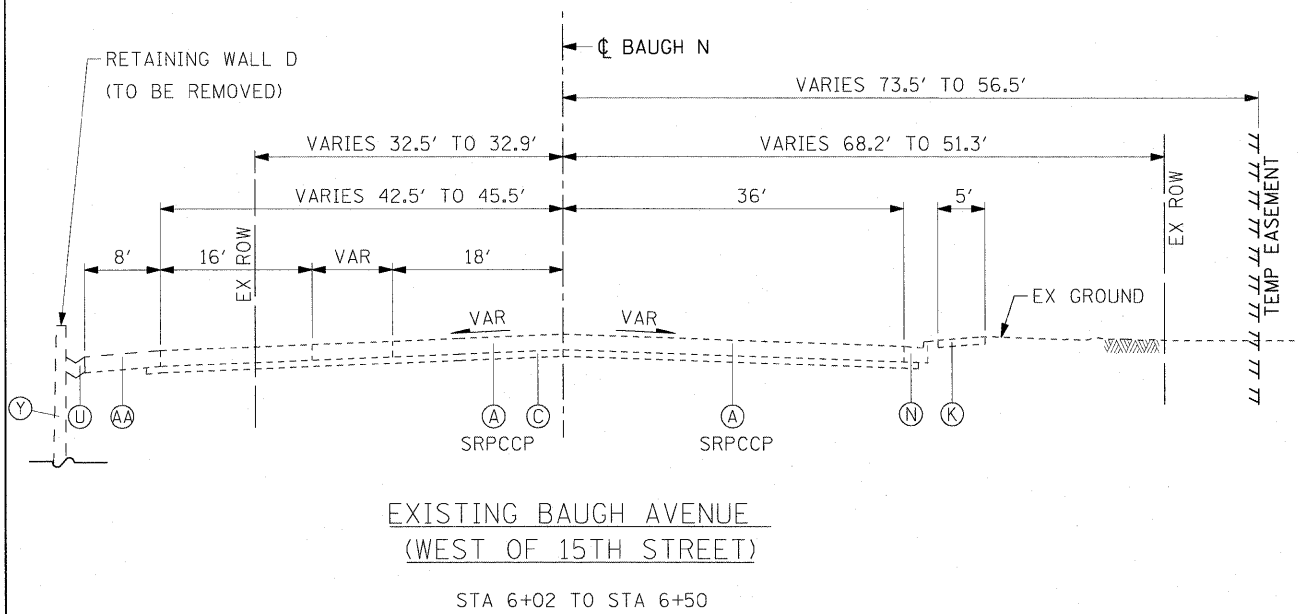
TYPICAL SECTIONS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	B2-1-2HB	ST. CLAIR	345	18
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	



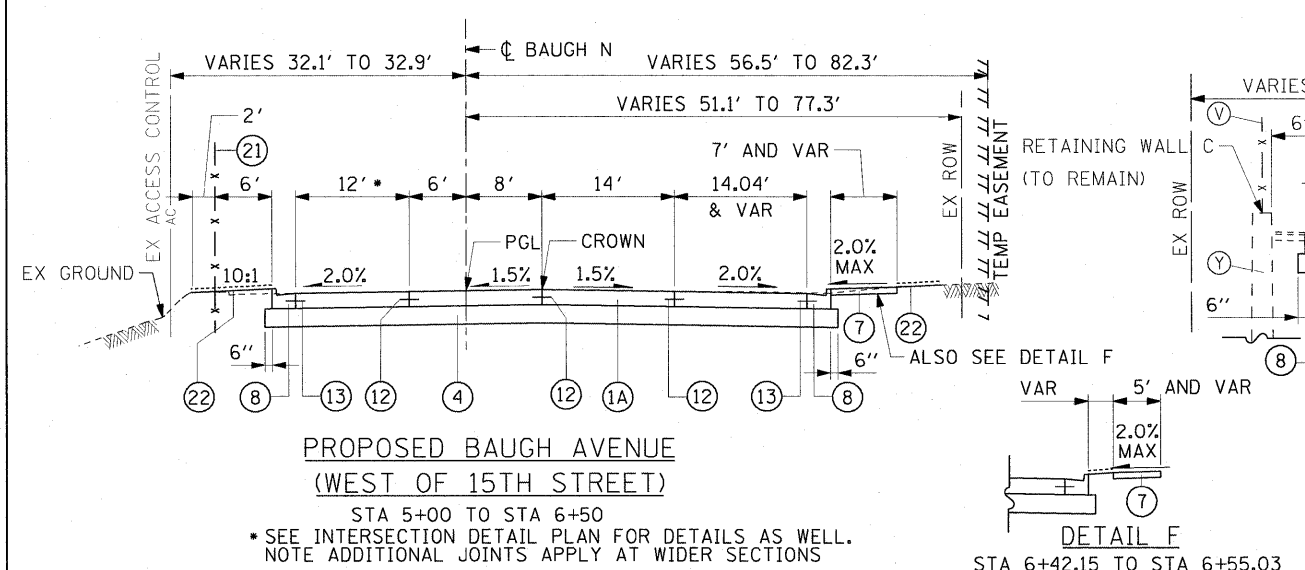
EXISTING BAUGH AVENUE  
(WEST OF 15TH STREET)

STA 5+00 TO STA 6+02



EXISTING BAUGH AVENUE  
(WEST OF 15TH STREET)

STA 6+02 TO STA 6+50



PROPOSED BAUGH AVENUE  
(WEST OF 15TH STREET)

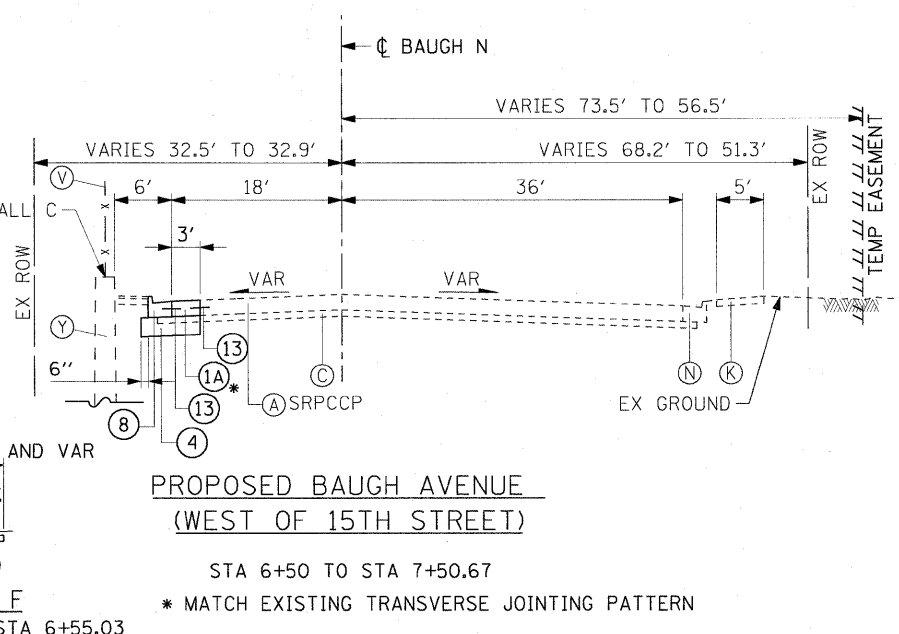
STA 5+00 TO STA 6+50  
\* SEE INTERSECTION DETAIL PLAN FOR DETAILS AS WELL.  
NOTE ADDITIONAL JOINTS APPLY AT WIDER SECTIONS

- EXISTING LEGEND:
- (A) PCC PAVEMENT - 10"
  - (B) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 3/4"
  - (C) SUB-BASE GRANULAR MATERIAL, TYPE A - 4"
  - (D) SUB-BASE GRANULAR MATERIAL, TYPE A - 6"
  - (E) BITUMINOUS MATERIALS (PRIME COAT)
  - (F) AGGREGATE (PRIME COAT)
  - (G) LEVELING BINDER (MACHINE METHOD)
  - (H) BITUMINOUS CONCRETE BINDER COURSE - 2 1/4"
  - (I) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 1 1/2"
  - (J) PORTLAND CEMENT CONCRETE BASE COURSE - 8"
  - (K) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
  - (L) COMBINATION CURB AND GUTTER TYPE B-6.12
  - (M) COMBINATION CURB AND GUTTER TYPE B-6.12 (SPECIAL)
  - (N) COMBINATION CURB AND GUTTER TYPE B-6.24
  - (O) PCC PAVEMENT - 10" AND VARIES
  - (P) CONCRETE CURB, TYPE B - 6" HIGH
  - (Q) TYPE B CURB (MODIFIED)
  - (R) PCC MEDIAN SURFACE - 4"
  - (S) CONCRETE MEDIAN SURFACE - 4"
  - (T) PORTLAND CEMENT CONCRETE BASE COURSE - 9"
  - (U) CONCRETE GUTTER
  - (V) FENCE
  - (W) LONGITUDINAL METAL JOINT
  - (X) EXISTING CURB
  - (Y) CONCRETE RETAINING WALL
  - (Z) BITUMINOUS CONCRETE BINDER COURSE - 1 1/2"
  - (AA) STABILIZED SHOULDER
  - (BB) AGGREGATE BASE COURSE, TYPE A - 4"
  - (CC) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (CRPCCP) - VARIES FROM 12" TO 13 1/4"
  - (DD) PIPE UNDERDRAINS - 6"
  - (EE) STABILIZED SUB-BASE - 4"
  - (FF) AGGREGATE SHOULDER TYPE A
  - (GG) INCIDENTAL BITUMINOUS SURFACING
  - (HH) CONCRETE GUTTER (MODIFIED)
  - (II) HMA SURFACE COURSE, MIX "C", N70 - 2"
  - (JJ) HMA OVERLAY

- PROPOSED LEGEND:
- (1) PORTLAND CEMENT CONCRETE PAVEMENT
    - (1A) - 10" (JOINTED) (LOCAL ROADS)
    - (1B) - 10 1/2" (JOINTED) (RAMPS)
  - (2) HOT-MIX ASPHALT OVERLAY - 2 1/4"
    - (2a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 - 1 1/2"
    - (2b) LEVELING BINDER, (MACHINE METHOD), N70 - 3/4"
  - (3) PORTLAND CEMENT CONCRETE BASE COURSE - 10"
  - (4) AGGREGATE BASE COURSE, TYPE A, 12"
  - (5) PORTLAND CEMENT CONCRETE SHOULDERS
    - (5A) - 10 1/2" (FOR 10 1/2" PCC JT)
    - (5B) - 12" (WB & EB I-64 TIE-IN)
    - (5C) - 15 1/2" (WB I-64 GORES)
    - (5D) - 18" (EB I-64 GORES)
  - (6) AGGREGATE SHOULDERS, TYPE B
    - (6A) - 10" (FOR 10" PCC JT)
    - (6B) - 10 1/2" (FOR 10 1/2" PCC JT)
    - (6C) - 11 1/4" (FOR TEMPORARY PAVEMENT INTERSTATE)
    - (6D) - 15 1/2" (FOR HMA SC "D" N70 - 3 1/2" AND PCC SHOULDERS 12")
    - (6E) - 18" (FOR HMA SC "D" N70 - 6" AND PCC SHOULDERS 12")
  - (7) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
  - (8) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
  - (9) POROUS GRANULAR EMBANKMENT, SPECIAL
  - (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06
  - (11) CONCRETE MEDIAN, TYPE SM-6.12
  - (12) \*6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / \*6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
  - (13) \*6 TIE BARS, 24" LONG AT 24" C-C (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
  - (14) PIPE UNDERDRAINS - 4"
  - (15) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT - 12"
  - (16) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT
  - (17) AGGREGATE BASE COURSE, TYPE A, THICKNESS AS SPECIFIED ON SECTION
  - (18) CONCRETE GUTTER TYPE A
  - (19) CONCRETE MEDIAN SURFACE - 4"
  - (20) STEEL PLATE BEAM GUARDRAIL, TYPE A, 6' POSTS OR TRAF BAR TERM
  - (21) CHAIN LINK FENCE - 6'
  - (22) SEEDING AND MULCHING (SEE SEEDING AND MULCHING SCHEDULE)
  - (23) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
  - (24) CONCRETE CURB AND GUTTER OUTLET SPECIAL
  - (25) HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
  - (26) TEMPORARY PAVEMENT (INTERSTATE) - 1 1/4"
    - (26a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) - 1 3/4"
    - (26b) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - 9 1/2"
  - (27) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 3 1/2"
  - (28) BITUMINOUS MATERIALS (PRIME COAT)
  - (29) AGGREGATE (PRIME COAT)
  - (30) HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
  - (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3 1/2"
  - (32) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 6"

NOTES:

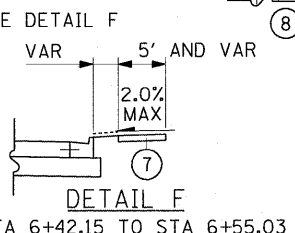
- DUE THE SKEWED NATURE OF THE ROADWAYS IN THIS PROJECT, THE FOLLOWINGS ARE THE COMPASS DESIGNATIONS TO THE ROADWAYS:  
15TH STREET (NORTH-SOUTH), BAUGH AVENUE (EAST-WEST), ST. CLAIR AVENUE (EAST-WEST), WB I-64 (WEST)
- SEE HOT-MIX ASPHALT MIXTURE REQUIREMENTS ON TYPICAL SECTIONS SHEET 3 OF 14.
- SEE SHEET TYPICAL SECTION SHEET 14 OF 14 FOR PAVEMENT STRUCTURAL TABLE.



PROPOSED BAUGH AVENUE  
(WEST OF 15TH STREET)

STA 6+50 TO STA 7+50.67

\* MATCH EXISTING TRANSVERSE JOINTING PATTERN



DETAIL F  
STA 6+42.15 TO STA 6+55.03

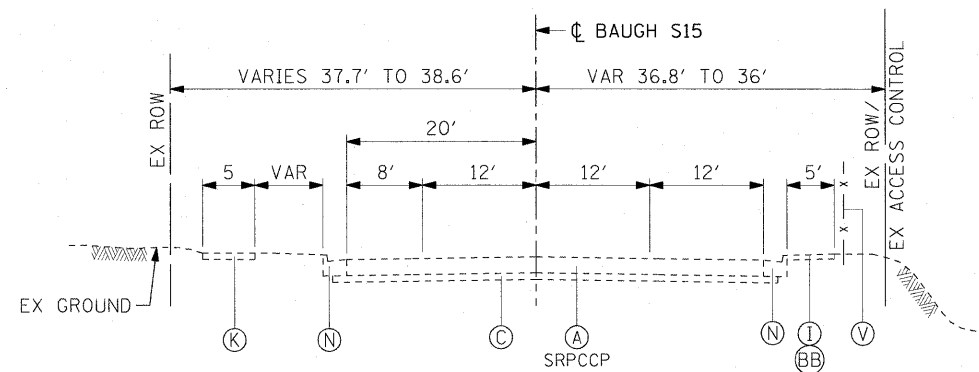
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

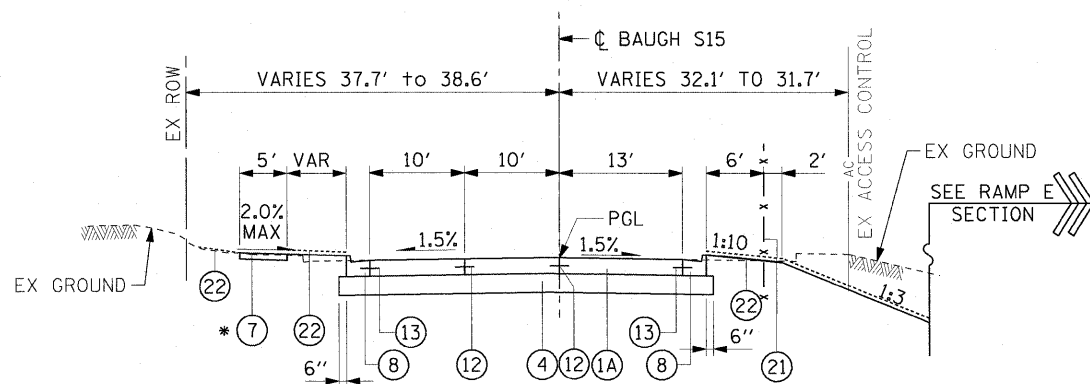
TYPICAL SECTIONS

SCALE: NONE	SHEET NO. 5 OF 14 SHEETS	STA.	TO STA.
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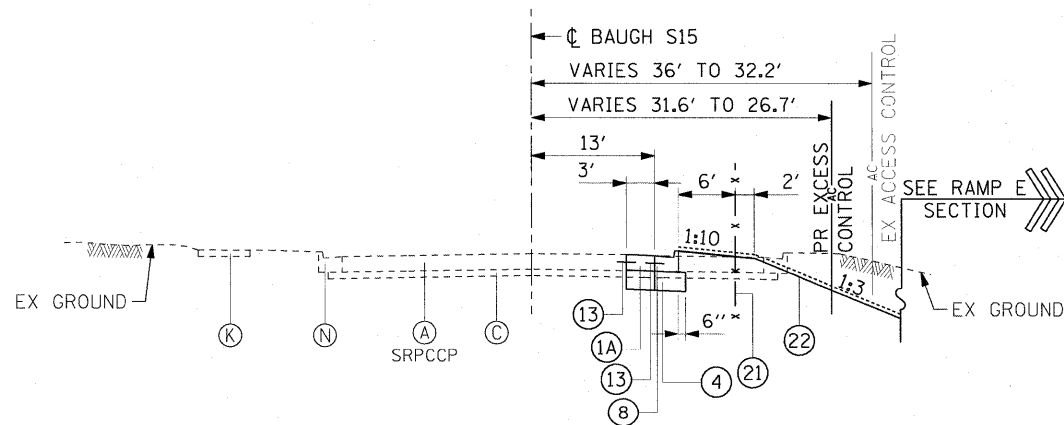
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	19
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	



EXISTING BAUGH AVENUE  
(EAST OF 15TH STREET)  
STA 110+00 TO STA 111+20



PROPOSED BAUGH AVENUE  
(EAST OF 15TH STREET)  
STA 110+00 TO STA 111+20  
\* SIDEWALK ENDS AT STA 112+26.68



PROPOSED BAUGH AVENUE  
(EAST OF 15TH STREET)  
STA 111+20 TO STA 117+35.91

EXISTING LEGEND:

- (A) PCC PAVEMENT - 10"
- (B) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 3/4"
- (C) SUB-BASE GRANULAR MATERIAL, TYPE A - 4"
- (D) SUB-BASE GRANULAR MATERIAL, TYPE A - 6"
- (E) BITUMINOUS MATERIALS (PRIME COAT)
- (F) AGGREGATE (PRIME COAT)
- (G) LEVELING BINDER (MACHINE METHOD)
- (H) BITUMINOUS CONCRETE BINDER COURSE - 2 1/4"
- (I) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 1 1/2"
- (J) PORTLAND CEMENT CONCRETE BASE COURSE - 8"
- (K) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (L) COMBINATION CURB AND GUTTER TYPE B-6.12
- (M) COMBINATION CURB AND GUTTER TYPE B-6.12 (SPECIAL)
- (N) COMBINATION CURB AND GUTTER TYPE B-6.24
- (O) PCC PAVEMENT - 10" AND VARIES
- (P) CONCRETE CURB, TYPE B - 6" HIGH
- (Q) TYPE B CURB (MODIFIED)
- (R) PCC MEDIAN SURFACE - 4"
- (S) CONCRETE MEDIAN SURFACE - 4"
- (T) PORTLAND CEMENT CONCRETE BASE COURSE - 9"
- (U) CONCRETE GUTTER
- (V) FENCE
- (W) LONGITUDINAL METAL JOINT
- (X) EXISTING CURB
- (Y) CONCRETE RETAINING WALL
- (Z) BITUMINOUS CONCRETE BINDER COURSE - 1 1/2"
- (AA) STABILIZED SHOULDER
- (BB) AGGREGATE BASE COURSE, TYPE A - 4"
- (CC) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (CRPCCP) - VARIES FROM 12" TO 13 1/4"
- (DD) PIPE UNDERDRAINS - 6"
- (EE) STABILIZED SUB-BASE - 4"
- (FF) AGGREGATE SHOULDER TYPE A
- (GG) INCIDENTAL BITUMINOUS SURFACING
- (HH) CONCRETE GUTTER (MODIFIED)
- (II) HMA SURFACE COURSE, MIX "C", N70 - 2"
- (JJ) HMA OVERLAY

PROPOSED LEGEND:

- (1) PORTLAND CEMENT CONCRETE PAVEMENT
  - (1A) - 10" (JOINTED) (LOCAL ROADS)
  - (1B) - 10 1/2" (JOINTED) (RAMPS)
- (2) HOT-MIX ASPHALT OVERLAY - 2 1/4"
  - (2a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 - 1 1/2"
  - (2b) LEVELING BINDER, (MACHINE METHOD), N70 - 3/4"
- (3) PORTLAND CEMENT CONCRETE BASE COURSE - 10"
- (4) AGGREGATE BASE COURSE, TYPE A, 12"
- (5) PORTLAND CEMENT CONCRETE SHOULDERS
  - (5A) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (5B) - 12" (WB & EB I-64 TIE-IN)
  - (5C) - 15 1/2" (WB I-64 GOSES)
  - (5D) - 18" (EB I-64 GOSES)
- (6) AGGREGATE SHOULDERS, TYPE B
  - (6A) - 10" (FOR 10" PCC JT)
  - (6B) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (6C) - 11 1/4" (FOR TEMPORARY PAVEMENT INTERSTATE)
  - (6D) - 15 1/2" (FOR HMA SC "D" N70 - 3 1/2" AND PCC SHOULDERS 12")
  - (6E) - 18" (FOR HMA SC "D" N70 - 6" AND PCC SHOULDERS 12")
- (7) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (8) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (9) POROUS GRANULAR EMBANKMENT, SPECIAL
- (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06
- (11) CONCRETE MEDIAN, TYPE SM-6.12
- (12) \*6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / \*6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (13) \*6 TIE BARS, 24" LONG AT 24" C-C (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (14) PIPE UNDERDRAINS - 4"
- (15) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT - 12"
- (16) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT
- (17) AGGREGATE BASE COURSE, TYPE A, THICKNESS AS SPECIFIED ON SECTION
- (18) CONCRETE GUTTER TYPE A
- (19) CONCRETE MEDIAN SURFACE - 4"
- (20) STEEL PLATE BEAM GUARDRAIL, TYPE A, 6' POSTS OR TRAF BAR TERM
- (21) CHAIN LINK FENCE - 6'
- (22) SEEDING AND MULCHING (SEE SEEDING AND MULCHING SCHEDULE)
- (23) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- (24) CONCRETE CURB AND GUTTER OUTLET SPECIAL
- (25) HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- (26) TEMPORARY PAVEMENT (INTERSTATE) - 11 1/4"
  - (26a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) - 1 3/4"
  - (26b) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - 9/2"
- (27) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 3 1/2"
- (28) BITUMINOUS MATERIALS (PRIME COAT)
- (29) AGGREGATE (PRIME COAT)
- (30) HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3 1/2"
- (32) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 6"

NOTES:

1. DUE TO THE SKEWED NATURE OF THE ROADWAYS IN THIS PROJECT, THE FOLLOWINGS ARE THE COMPASS DESIGNATIONS TO THE ROADWAYS:  
  
15TH STREET (NORTH-SOUTH), BAUGH AVENUE (EAST-WEST), ST. CLAIR AVENUE (EAST-WEST), WB I-64 (WEST)
2. SEE HOT-MIX ASPHALT MIXTURE REQUIREMENTS ON TYPICAL SECTIONS SHEET 3 OF 14.

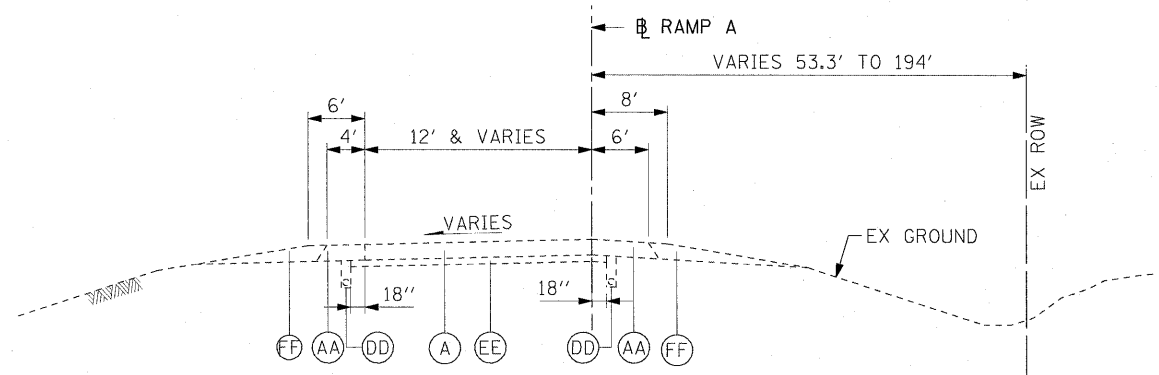
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

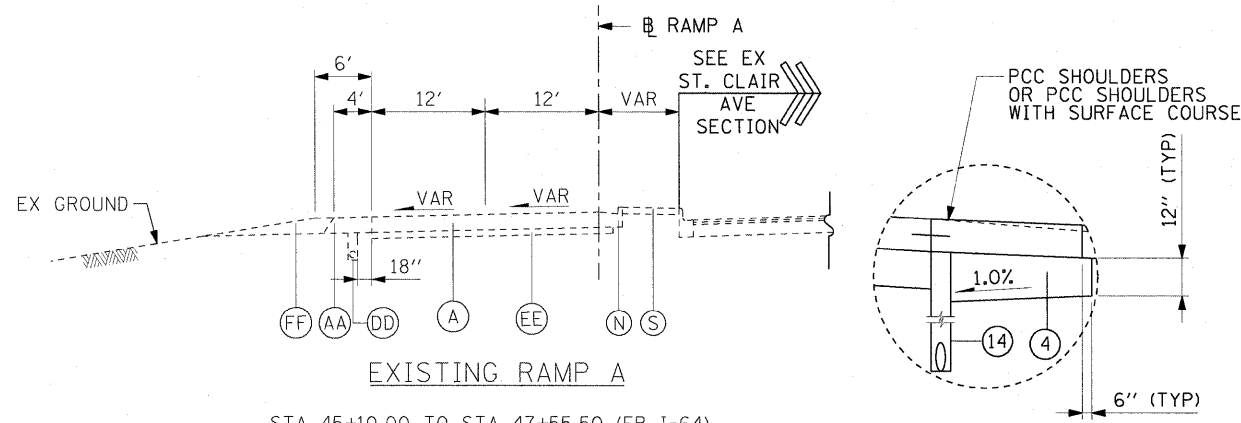
TYPICAL SECTIONS

SCALE: NONE SHEET NO. 6 OF 14 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	B2-1-2HB	ST. CLAIR	345	20
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	

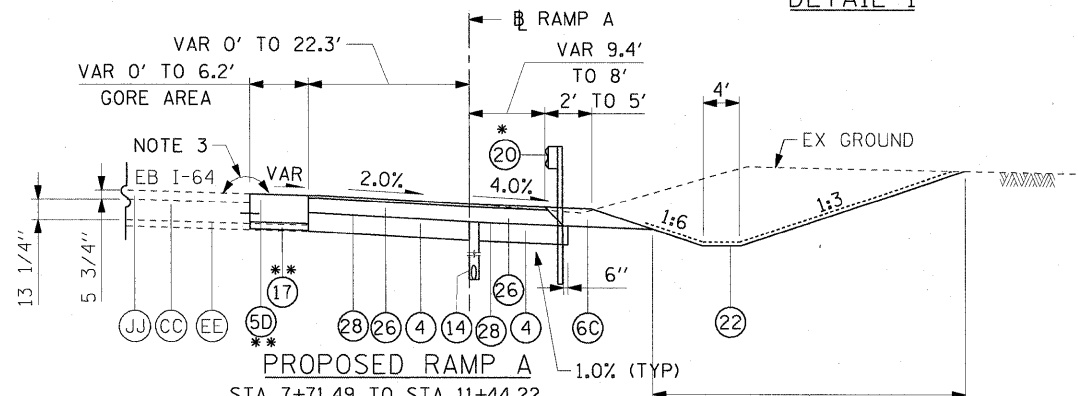


EXISTING RAMP A  
STA 40+00.00 TO STA 45+10.00 (EB I-64)

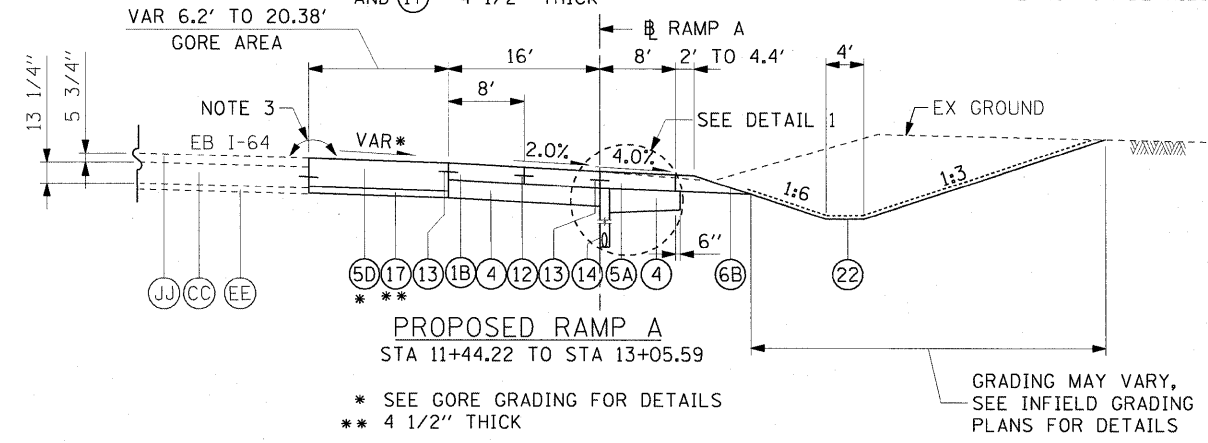


EXISTING RAMP A  
STA 45+10.00 TO STA 47+55.50 (EB I-64)

DETAIL 1



PROPOSED RAMP A  
STA 7+71.49 TO STA 11+44.22  
\* 20 - FROM STA 41+36.35 (EB I-64) TO STA 10+12.54  
\*\* 5D & 17 - FROM STA 10+74.05 TO STA 11+44.22 AND 17 - 4 1/2" THICK  
GRADING MAY VARY, SEE INFIELD GRADING PLANS FOR DETAILS



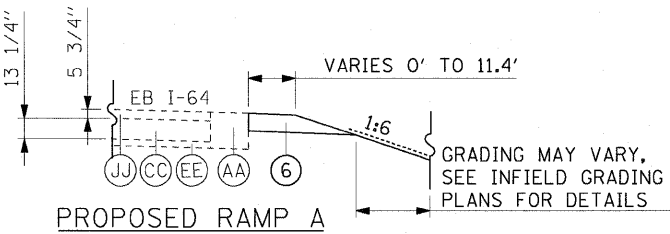
PROPOSED RAMP A  
STA 11+44.22 TO STA 13+05.59  
\* SEE GORE GRADING FOR DETAILS  
\*\* 4 1/2" THICK  
GRADING MAY VARY, SEE INFIELD GRADING PLANS FOR DETAILS

EXISTING LEGEND:

- (A) PCC PAVEMENT - 10"
- (B) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 3/4"
- (C) SUB-BASE GRANULAR MATERIAL, TYPE A - 4"
- (D) SUB-BASE GRANULAR MATERIAL, TYPE A - 6"
- (E) BITUMINOUS MATERIALS (PRIME COAT)
- (F) AGGREGATE (PRIME COAT)
- (G) LEVELING BINDER (MACHINE METHOD)
- (H) BITUMINOUS CONCRETE BINDER COURSE - 2 1/4"
- (I) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 1 1/2"
- (J) PORTLAND CEMENT CONCRETE BASE COURSE - 8"
- (K) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (L) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (M) COMBINATION CURB AND GUTTER TYPE B-6.12
- (N) COMBINATION CURB AND GUTTER TYPE B-6.24
- (O) PCC PAVEMENT - 10" AND VARIES
- (P) CONCRETE CURB, TYPE B - 6" HIGH
- (Q) TYPE B CURB (MODIFIED)
- (R) PCC MEDIAN SURFACE - 4"
- (S) CONCRETE MEDIAN SURFACE - 4"
- (T) PORTLAND CEMENT CONCRETE BASE COURSE - 9"
- (U) CONCRETE GUTTER
- (V) FENCE
- (W) LONGITUDINAL METAL JOINT
- (X) EXISTING CURB
- (Y) CONCRETE RETAINING WALL
- (Z) BITUMINOUS CONCRETE BINDER COURSE - 1 1/2"
- (AA) STABILIZED SHOULDER
- (BB) AGGREGATE BASE COURSE, TYPE A - 4"
- (CC) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (CRPCCP) - VARIES FROM 12" TO 13 1/4"
- (DD) PIPE UNDERDRAINS - 6"
- (EE) STABILIZED SUB-BASE - 4"
- (FF) AGGREGATE SHOULDER TYPE A
- (GG) INCIDENTAL BITUMINOUS SURFACING
- (HH) CONCRETE GUTTER (MODIFIED)
- (II) HMA SURFACE COURSE, MIX "C", N70 - 2"
- (JJ) HMA OVERLAY

PROPOSED LEGEND:

- (1) PORTLAND CEMENT CONCRETE PAVEMENT
  - (1A) - 10" (JOINTED) (LOCAL ROADS)
  - (1B) - 10 1/2" (JOINTED) (RAMPS)
- (2) HOT-MIX ASPHALT OVERLAY - 2 1/4"
  - (2a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 - 1 1/2"
  - (2b) LEVELING BINDER, (MACHINE METHOD), N70 - 3/4"
- (3) PORTLAND CEMENT CONCRETE BASE COURSE - 10"
- (4) AGGREGATE BASE COURSE, TYPE A, 12"
- (5) PORTLAND CEMENT CONCRETE SHOULDERS
  - (5A) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (5B) - 12" (WB & EB I-64 TIE-IN)
  - (5C) - 15 1/2" (WB I-64 GORES)
  - (5D) - 18" (EB I-64 GORES)
- (6) AGGREGATE SHOULDERS, TYPE B
  - (6A) - 10" (FOR 10" PCC JT)
  - (6B) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (6C) - 11 1/4" (FOR TEMPORARY PAVEMENT INTERSTATE)
  - (6D) - 15 1/2" (FOR HMA SC "D" N70 - 3 1/2" AND PCC SHOULDERS 12")
  - (6E) - 18" (FOR HMA SC "D" N70 - 6" AND PCC SHOULDERS 12")
- (7) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (8) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (9) POROUS GRANULAR EMBANKMENT, SPECIAL
- (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06
- (11) CONCRETE MEDIAN, TYPE SM-6.12
- (12) \*6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / \*6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (13) \*6 TIE BARS, 24" LONG AT 24" C-C (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (14) PIPE UNDERDRAINS - 4"
- (15) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT - 12"
- (16) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT
- (17) AGGREGATE BASE COURSE, TYPE A, THICKNESS AS SPECIFIED ON SECTION
- (18) CONCRETE GUTTER TYPE A
- (19) CONCRETE MEDIAN SURFACE - 4"
- (20) STEEL PLATE BEAM GUARDRAIL, TYPE A, 6' POSTS OR TRAF BAR TERM
- (21) CHAIN LINK FENCE - 6'
- (22) SEEDING AND MULCHING (SEE SEEDING AND MULCHING SCHEDULE)
- (23) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- (24) CONCRETE CURB AND GUTTER OUTLET SPECIAL
- (25) HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- (26) TEMPORARY PAVEMENT (INTERSTATE) - 11 1/4"
  - (26a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) - 1 3/4"
  - (26b) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - 9 1/2"
- (27) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 3 1/2"
- (28) BITUMINOUS MATERIALS (PRIME COAT)
- (29) AGGREGATE (PRIME COAT)
- (30) HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3 1/2"
- (32) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 6"



PROPOSED RAMP A  
STA 40+46.46 TO STA 41+83.94 (EB I-64)

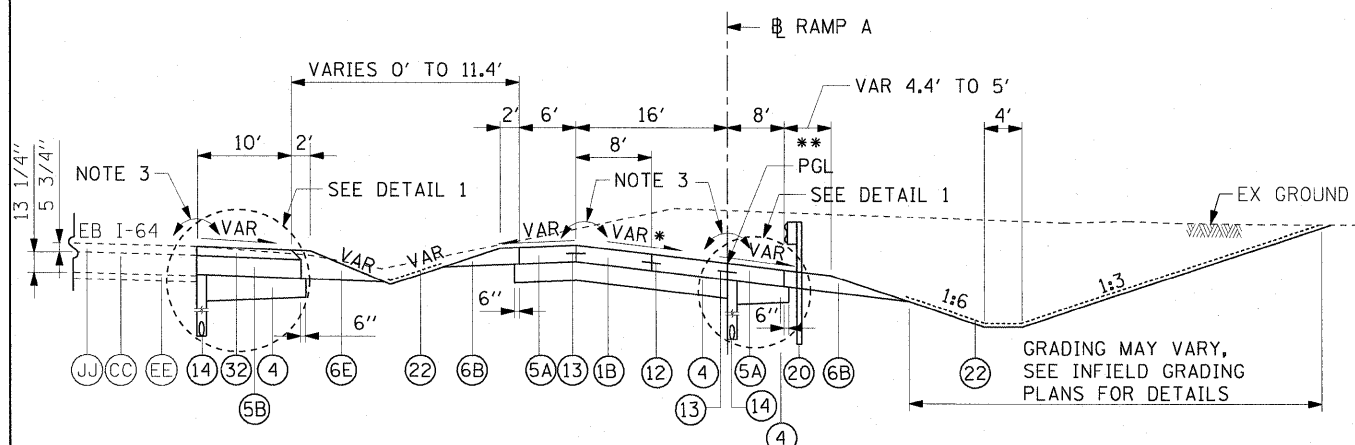
RAMP A

STRUCTURAL DESIGN TRAFFIC:	YEAR	2030
PV= 1,291	SU= 81	MU= 242
ROAD/STREET CLASSIFICATION:	CLASS I	
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:		
P= 80%	S= 5%	M= 15%
TRAFFIC FACTOR:	ACTUAL TF= 3.61	AC TYPE= 20
	MINIMUM TF= 11.17	
PG GRADE:	BINDER= NA	SURFACE= NA
SUBGRADE SUPPORT RATING	SSR= POOR	

NOTES:

- DUE TO THE SKEWED NATURE OF THE ROADWAYS IN THIS PROJECT, THE FOLLOWINGS ARE THE COMPASS DESIGNATIONS TO THE ROADWAYS:  
15TH STREET (NORTH-SOUTH), BAUGH AVENUE (EAST-WEST), ST. CLAIR AVENUE (EAST-WEST), WB I-64 (WEST)
- SEE HOT-MIX ASPHALT MIXTURE REQUIREMENTS ON TYPICAL SECTIONS SHEET 3 OF 14.
- WHEN THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER SLOPES WILL NOT BE GREATER THAN 8%.

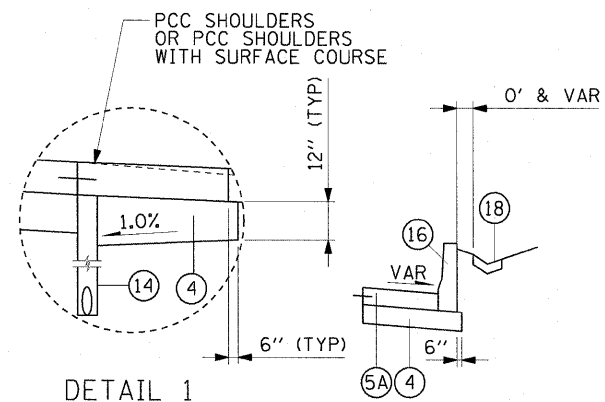




**PROPOSED RAMP A**

STA 13+05.59 TO STA 14+26.79

- \* SEE SE TRANSITION DATA TABLE FOR SUPERELEVATION TRANSITIONS
- \*\* AGGREGATE SHOULDER & STEEL PLATE BEAM GUARDRAIL END AT STA 14+12.39 RT, BEGIN CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT, SEE DETAIL G1 & G2



**DETAIL 1**

**DETAIL G1**

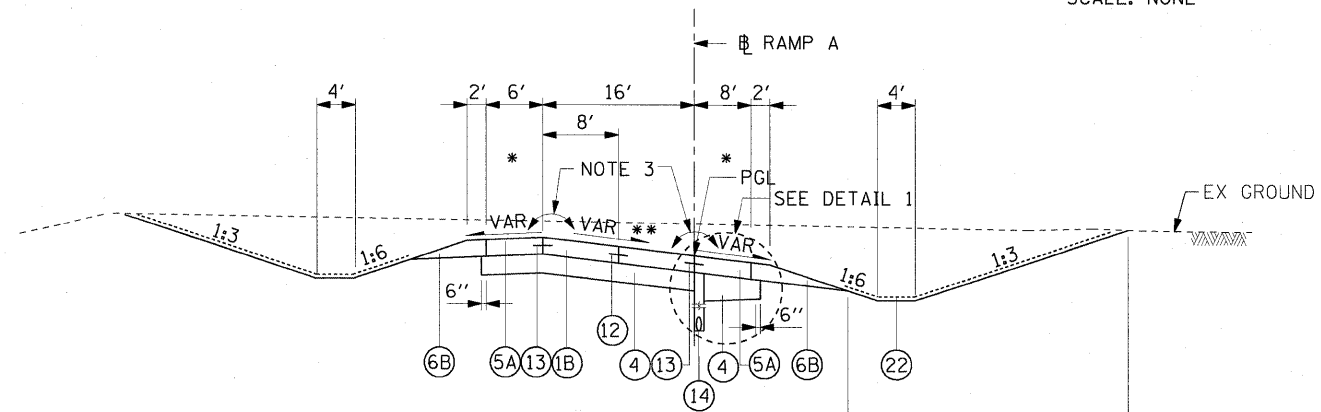
STA 14+12.50 TO STA 14+31.92  
STA 14+90.36 TO STA 15+31.11  
SCALE: NONE

**EXISTING LEGEND:**

- (A) PCC PAVEMENT - 10"
- (B) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 3/4"
- (C) SUB-BASE GRANULAR MATERIAL, TYPE A - 4"
- (D) SUB-BASE GRANULAR MATERIAL, TYPE A - 6"
- (E) BITUMINOUS MATERIALS (PRIME COAT)
- (F) AGGREGATE (PRIME COAT)
- (G) LEVELING BINDER (MACHINE METHOD)
- (H) BITUMINOUS CONCRETE BINDER COURSE - 2 1/4"
- (I) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 1 1/2"
- (J) PORTLAND CEMENT CONCRETE BASE COURSE - 8"
- (K) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (L) COMBINATION CURB AND GUTTER TYPE B-6.12
- (M) COMBINATION CURB AND GUTTER TYPE B-6.12 (SPECIAL)
- (N) COMBINATION CURB AND GUTTER TYPE B-6.24
- (O) PCC PAVEMENT - 10" AND VARIES
- (P) CONCRETE CURB, TYPE B - 6" HIGH
- (Q) TYPE B CURB (MODIFIED)
- (R) PCC MEDIAN SURFACE - 4"
- (S) CONCRETE MEDIAN SURFACE - 4"
- (T) PORTLAND CEMENT CONCRETE BASE COURSE - 9"
- (U) CONCRETE GUTTER
- (V) FENCE
- (W) LONGITUDINAL METAL JOINT
- (X) EXISTING CURB
- (Y) CONCRETE RETAINING WALL
- (Z) BITUMINOUS CONCRETE BINDER COURSE - 1 1/2"
- (AA) STABILIZED SHOULDER
- (BB) AGGREGATE BASE COURSE, TYPE A - 4"
- (CC) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (CRPCCP) - VARIES FROM 12" TO 13 1/4"
- (DD) PIPE UNDERDRAINS - 6"
- (EE) STABILIZED SUB-BASE - 4"
- (FF) AGGREGATE SHOULDER TYPE A
- (GG) INCIDENTAL BITUMINOUS SURFACING
- (HH) CONCRETE GUTTER (MODIFIED)
- (II) HMA SURFACE COURSE, MIX "C", N70 - 2"
- (JJ) HMA OVERLAY

**PROPOSED LEGEND:**

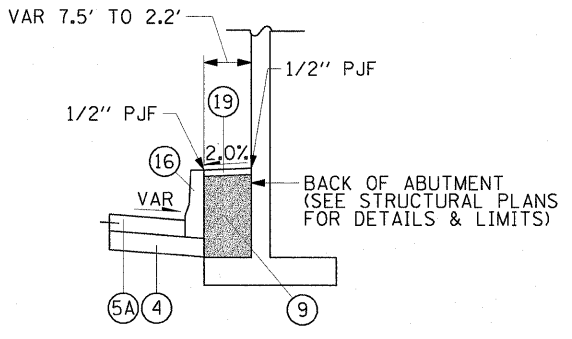
- (1) PORTLAND CEMENT CONCRETE PAVEMENT
  - (1A) - 10" (JOINTED) (LOCAL ROADS)
  - (1B) - 10 1/2" (JOINTED) (RAMPS)
- (2) HOT-MIX ASPHALT OVERLAY - 2 1/4"
  - (2a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 - 1 1/2"
  - (2b) LEVELING BINDER, (MACHINE METHOD), N70 - 3/4"
- (3) PORTLAND CEMENT CONCRETE BASE COURSE - 10"
- (4) AGGREGATE BASE COURSE, TYPE A, 12"
- (5) PORTLAND CEMENT CONCRETE SHOULDERS
  - (5A) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (5B) - 12" (WB & EB I-64 TIE-IN)
  - (5C) - 15 1/2" (WB I-64 GORES)
  - (5D) - 18" (EB I-64 GORES)
- (6) AGGREGATE SHOULDERS, TYPE B
  - (6A) - 10" (FOR 10" PCC JT)
  - (6B) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (6C) - 11 1/4" (FOR TEMPORARY PAVEMENT INTERSTATE)
  - (6D) - 15 1/2" (FOR HMA SC "D" N70 - 3 1/2" AND PCC SHOULDERS 12")
  - (6E) - 18" (FOR HMA SC "D" N70 - 6" AND PCC SHOULDERS 12")
- (7) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (8) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (9) POROUS GRANULAR EMBANKMENT, SPECIAL
- (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06
- (11) CONCRETE MEDIAN, TYPE SM-6.12
- (12) \*6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / \*6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (13) \*6 TIE BARS, 24" LONG AT 24" C-C (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (14) PIPE UNDERDRAINS - 4"
- (15) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT - 12"
- (16) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT
- (17) AGGREGATE BASE COURSE, TYPE A, THICKNESS AS SPECIFIED ON SECTION
- (18) CONCRETE GUTTER TYPE A
- (19) CONCRETE MEDIAN SURFACE - 4"
- (20) STEEL PLATE BEAM GUARDRAIL, TYPE A, 6' POSTS OR TRAF BAR TERM
- (21) CHAIN LINK FENCE - 6'
- (22) SEEDING AND MULCHING (SEE SEEDING AND MULCHING SCHEDULE)
- (23) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- (24) CONCRETE CURB AND GUTTER OUTLET SPECIAL
- (25) HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- (26) TEMPORARY PAVEMENT (INTERSTATE) - 1 1/4"
- (26a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) - 1 3/4"
- (26b) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - 9/2"
- (27) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 3/2"
- (28) BITUMINOUS MATERIALS (PRIME COAT)
- (29) AGGREGATE (PRIME COAT)
- (30) HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3 1/2"
- (32) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 6"



**PROPOSED RAMP A**

STA 14+26.79 TO STA 18+21.16

- \* STA 18+21.16 TO STA 19+62.07 SEE INTERSECTION DETAILS RT SHOULDERS CONTINUE TO STA 18+38.03
- \*\* SEE SE TRANSITION DATA TABLE FOR SUPERELEVATION TRANSITIONS



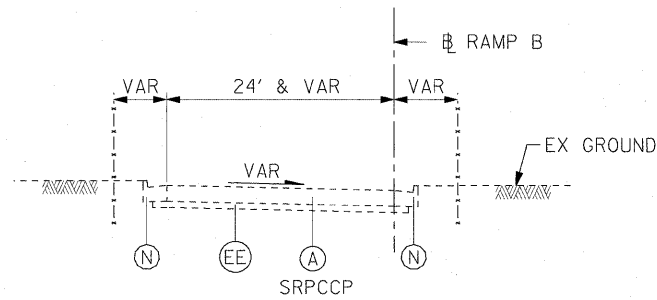
**DETAIL G2**

STA 14+31.74 TO STA 14+90.15  
SCALE: NONE

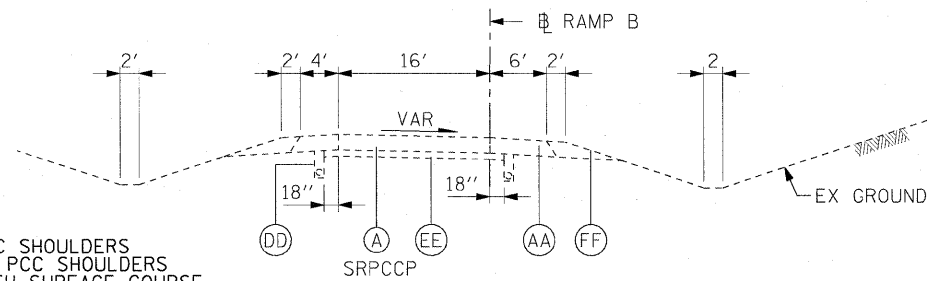
**NOTES:**

1. DUE THE SKEWED NATURE OF THE ROADWAYS IN THIS PROJECT, THE FOLLOWINGS ARE THE COMPASS DESIGNATIONS TO THE ROADWAYS:  
15TH STREET (NORTH-SOUTH), BAUGH AVENUE (EAST-WEST), ST. CLAIR AVENUE (EAST-WEST), WB I-64 (WEST)
2. SEE HOT-MIX ASPHALT MIXTURE REQUIREMENTS ON TYPICAL SECTIONS SHEET 3 OF 14.
3. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER SLOPES WILL NOT BE GREATER THAN 8%.

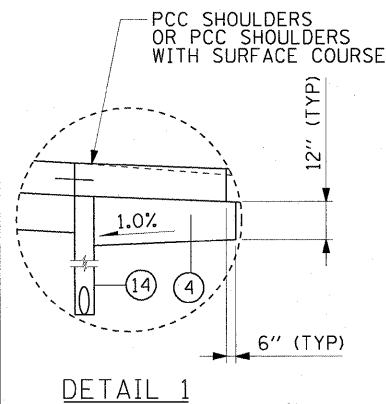
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	PLOT DATE = 4/7/2010	CHECKED DBM	REVISED -			CONTRACT NO. 76C49					
		DATE 03/19/10	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					



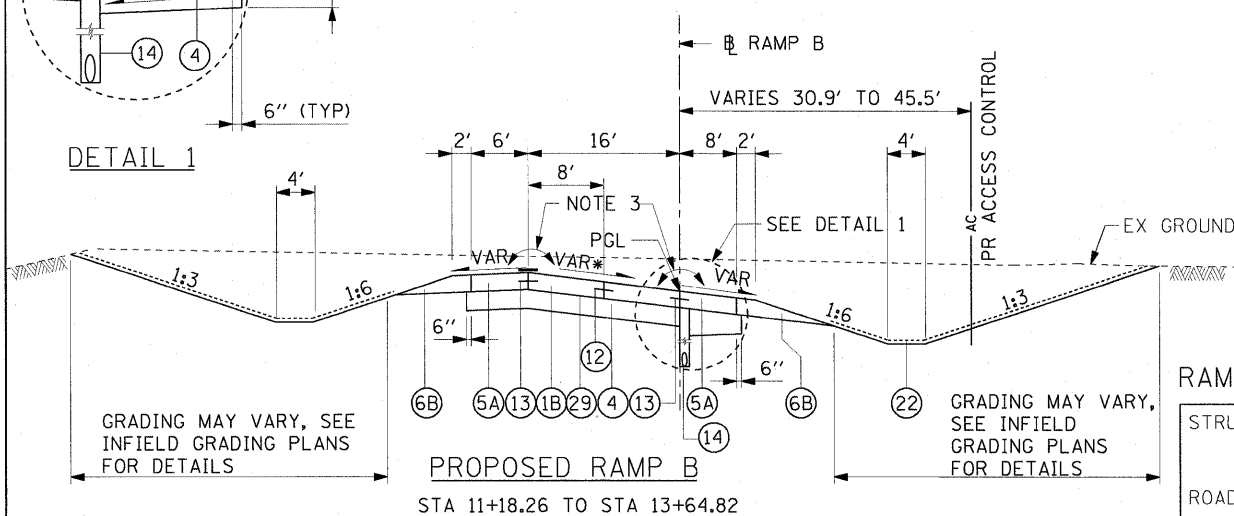
EXISTING RAMP B  
STA 12+11.27 TO STA 13+62.38



EXISTING RAMP B  
STA 13+62.38 TO STA 15+41.39



DETAIL 1



PROPOSED RAMP B  
STA 11+18.26 TO STA 13+64.82

STA 10+00.00 TO STA 11+06.67 SEE INTERSECTION DETAILS  
STA 11+17.76 (BEGIN 16' RAMP WIDTH)

\* SEE SE TRANSITION DATA TABLE FOR SUPERELEVATION TRANSITIONS

EXISTING LEGEND:

- (A) PCC PAVEMENT - 10"
- (B) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 3/4"
- (C) SUB-BASE GRANULAR MATERIAL, TYPE A - 4"
- (D) SUB-BASE GRANULAR MATERIAL, TYPE A - 6"
- (E) BITUMINOUS MATERIALS (PRIME COAT)
- (F) AGGREGATE (PRIME COAT)
- (G) LEVELING BINDER (MACHINE METHOD)
- (H) BITUMINOUS CONCRETE BINDER COURSE - 2 1/4"
- (I) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 1 1/2"
- (J) PORTLAND CEMENT CONCRETE BASE COURSE - 8"
- (K) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (L) COMBINATION CURB AND GUTTER TYPE B-6.12
- (M) COMBINATION CURB AND GUTTER TYPE B-6.12 (SPECIAL)
- (N) COMBINATION CURB AND GUTTER TYPE B-6.24
- (O) PCC PAVEMENT - 10" AND VARIES
- (P) CONCRETE CURB, TYPE B - 6" HIGH
- (Q) TYPE B CURB (MODIFIED)
- (R) PCC MEDIAN SURFACE - 4"
- (S) CONCRETE MEDIAN SURFACE - 4"
- (T) PORTLAND CEMENT CONCRETE BASE COURSE - 9"
- (U) CONCRETE GUTTER
- (V) FENCE
- (W) LONGITUDINAL METAL JOINT
- (X) EXISTING CURB
- (Y) CONCRETE RETAINING WALL
- (Z) BITUMINOUS CONCRETE BINDER COURSE - 1 1/2"
- (AA) STABILIZED SHOULDER
- (BB) AGGREGATE BASE COURSE, TYPE A - 4"
- (CC) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (CRPCCP) - VARIES FROM 12" TO 13 1/4"
- (DD) PIPE UNDERDRAINS - 6"
- (EE) STABILIZED SUB-BASE - 4"
- (FF) AGGREGATE SHOULDER TYPE A
- (GG) INCIDENTAL BITUMINOUS SURFACING
- (HH) CONCRETE GUTTER (MODIFIED)
- (II) HMA SURFACE COURSE, MIX "C", N70 - 2"
- (JJ) HMA OVERLAY

PROPOSED LEGEND:

- (1) PORTLAND CEMENT CONCRETE PAVEMENT
  - (1A) - 10" (JOINTED) (LOCAL ROADS)
  - (1B) - 10 1/2" (JOINTED) (RAMPS)
- (2) HOT-MIX ASPHALT OVERLAY - 2 1/4"
  - [2a] HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 - 1 1/2"
  - [2b] LEVELING BINDER, (MACHINE METHOD), N70 - 3/4"
- (3) PORTLAND CEMENT CONCRETE BASE COURSE - 10"
- (4) AGGREGATE BASE COURSE, TYPE A, 12"
- (5) PORTLAND CEMENT CONCRETE SHOULDERS
  - (5A) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (5B) - 12" (WB & EB I-64 TIE-IN)
  - (5C) - 15 1/2" (WB I-64 GOSES)
  - (5D) - 18" (EB I-64 GOSES)
- (6) AGGREGATE SHOULDERS, TYPE B
  - (6A) - 10" (FOR 10" PCC JT)
  - (6B) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (6C) - 11 1/4" (FOR TEMPORARY PAVEMENT INTERSTATE)
  - (6D) - 15 1/2" (FOR HMA SC "D" N70 - 3 1/2" AND PCC SHOULDERS 12")
  - (6E) - 18" (FOR HMA SC "D" N70 - 6" AND PCC SHOULDERS 12")
- (7) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (8) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (9) POROUS GRANULAR EMBANKMENT, SPECIAL
- (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06
- (11) CONCRETE MEDIAN, TYPE SM-6.12
- (12) \*6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / \*6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (13) \*6 TIE BARS, 24" LONG AT 24" C-C (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (14) PIPE UNDERDRAINS - 4"
- (15) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT - 12"
- (16) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT
- (17) AGGREGATE BASE COURSE, TYPE A, THICKNESS AS SPECIFIED ON SECTION
- (18) CONCRETE GUTTER TYPE A
- (19) CONCRETE MEDIAN SURFACE - 4"
- (20) STEEL PLATE BEAM GUARDRAIL, TYPE A, 6' POSTS OR TRAF BAR TERM
- (21) CHAIN LINK FENCE - 6'
- (22) SEEDING AND MULCHING (SEE SEEDING AND MULCHING SCHEDULE)
- (23) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- (24) CONCRETE CURB AND GUTTER OUTLET SPECIAL
- (25) HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- (26) TEMPORARY PAVEMENT (INTERSTATE) - 11 1/4"
  - [26a] HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) - 1 3/4"
  - [26b] HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - 9/2"
- (27) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 3/2"
- (28) BITUMINOUS MATERIALS (PRIME COAT)
- (29) AGGREGATE (PRIME COAT)
- (30) HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3 1/2"
- (32) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 6"

RAMP B

STRUCTURAL DESIGN TRAFFIC:	YEAR	2030
PV= 3,131	SU= 196	MU= 587
ROAD/STREET CLASSIFICATION:	CLASS	I
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:		
P= 80%	S= 5%	M= 15%
TRAFFIC FACTOR:	ACTUAL TF= 8.75	AC TYPE= 20
	MINIMUM TF= 11.17	
PG GRADE:	BINDER= NA	SURFACE= NA
SUBGRADE SUPPORT RATING:	SSR= POOR	

NOTES:

- DUE TO THE SKEWED NATURE OF THE ROADWAYS IN THIS PROJECT, THE FOLLOWINGS ARE THE COMPASS DESIGNATIONS TO THE ROADWAYS:  
15TH STREET (NORTH-SOUTH), BAUGH AVENUE (EAST-WEST), ST. CLAIR AVENUE (EAST-WEST), WB I-64 (WEST)
- SEE HOT-MIX ASPHALT MIXTURE REQUIREMENTS ON TYPICAL SECTIONS SHEET 3 OF 14.
- WHEN THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER SLOPES WILL NOT BE GREATER THAN 8%.

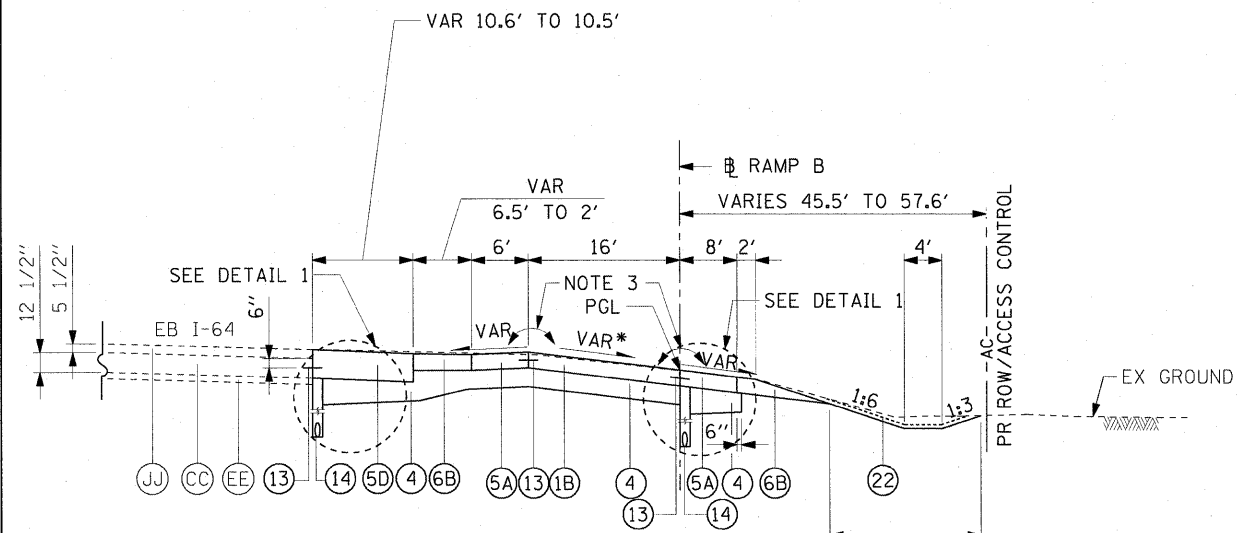
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	PLOT DATE = 3/18/2010	DATE 03/19/10	REVISIONS

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

SCALE: NONE SHEET NO. 9 OF 14 SHEETS STA. TO STA.

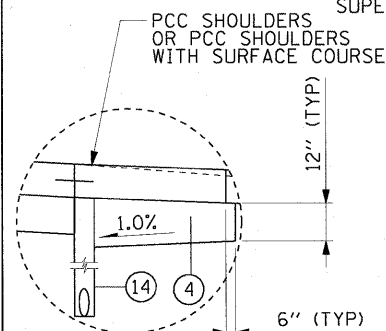
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 76C49	



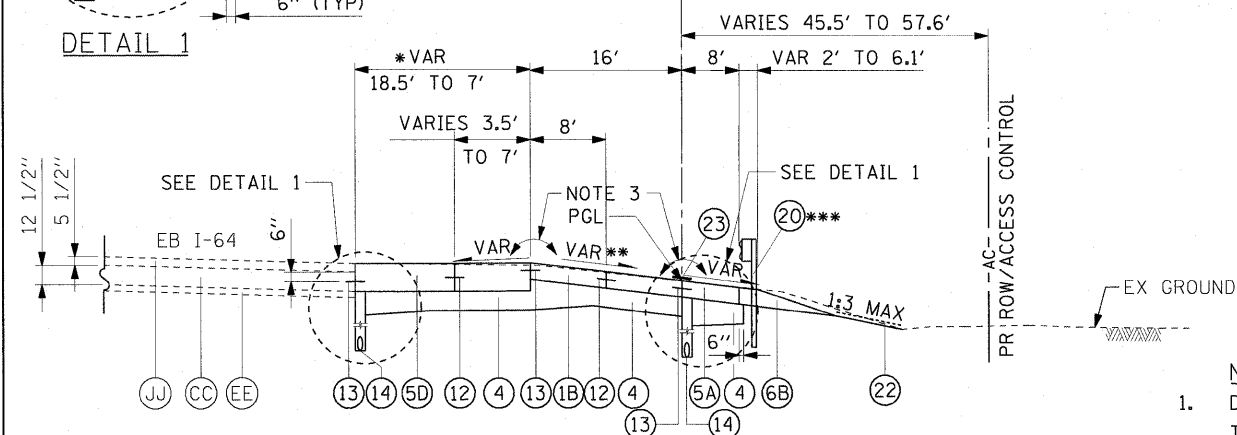
**PROPOSED RAMP B**

STA 13+64.82 TO STA 14+07.53

\* SEE SE TRANSITION DATA TABLE FOR SUPERELEVATION TRANSITIONS



**DETAIL 1**



**PROPOSED RAMP B**

STA 14+07.53 TO STA 15+41.39

\* SEE GORE GRADING FOR DETAILS

\*\* SEE SE TRANSITION DATA TABLE FOR SUPERELEVATION TRANSITIONS

\*\*\* STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS AND TRAFFIC BARRIER TERMINALS FROM STA 15+02.62 TO STA 18+33.27

**EXISTING LEGEND:**

- (A) PCC PAVEMENT - 10"
- (B) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 3/4"
- (C) SUB-BASE GRANULAR MATERIAL, TYPE A - 4"
- (D) SUB-BASE GRANULAR MATERIAL, TYPE A - 6"
- (E) BITUMINOUS MATERIALS (PRIME COAT)
- (F) AGGREGATE (PRIME COAT)
- (G) LEVELING BINDER (MACHINE METHOD)
- (H) BITUMINOUS CONCRETE BINDER COURSE - 2 1/4"
- (I) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 1 1/2"
- (J) PORTLAND CEMENT CONCRETE BASE COURSE - 8"
- (K) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (L) COMBINATION CURB AND GUTTER TYPE B-6.12
- (M) COMBINATION CURB AND GUTTER TYPE B-6.12 (SPECIAL)
- (N) COMBINATION CURB AND GUTTER TYPE B-6.24
- (O) PCC PAVEMENT - 10" AND VARIES
- (P) CONCRETE CURB, TYPE B - 6" HIGH
- (Q) TYPE B CURB (MODIFIED)
- (R) PCC MEDIAN SURFACE - 4"
- (S) CONCRETE MEDIAN SURFACE - 4"
- (T) PORTLAND CEMENT CONCRETE BASE COURSE - 9"
- (U) CONCRETE GUTTER
- (V) FENCE
- (W) LONGITUDINAL METAL JOINT
- (X) EXISTING CURB
- (Y) CONCRETE RETAINING WALL
- (Z) BITUMINOUS CONCRETE BINDER COURSE - 1 1/2"
- (AA) STABILIZED SHOULDER
- (BB) AGGREGATE BASE COURSE, TYPE A - 4"
- (CC) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (CRPCCP) - VARIES FROM 12" TO 13 1/4"
- (DD) PIPE UNDERDRAINS - 6"
- (EE) STABILIZED SUB-BASE - 4"
- (FF) AGGREGATE SHOULDER TYPE A
- (GG) INCIDENTAL BITUMINOUS SURFACING
- (HH) CONCRETE GUTTER (MODIFIED)
- (II) HMA SURFACE COURSE, MIX "C", N70 - 2"
- (JJ) HMA OVERLAY

**NOTES:**

1. DUE TO THE SKEWED NATURE OF THE ROADWAYS IN THIS PROJECT, THE FOLLOWINGS ARE THE COMPASS DESIGNATIONS TO THE ROADWAYS:  
  
15TH STREET (NORTH-SOUTH), BAUGH AVENUE (EAST-WEST), ST. CLAIR AVENUE (EAST-WEST), WB I-64 (WEST)
2. SEE HOT-MIX ASPHALT MIXTURE REQUIREMENTS ON TYPICAL SECTIONS SHEET 3 OF 14.
3. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER SLOPES WILL NOT BE GREATER THAN 8%.

**PROPOSED LEGEND:**

- (1) PORTLAND CEMENT CONCRETE PAVEMENT
  - (1A) - 10" (JOINTED) (LOCAL ROADS)
  - (1B) - 10 1/2" (JOINTED) (RAMPS)
- (2) HOT-MIX ASPHALT OVERLAY - 2 1/4"
  - (2a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 - 1 1/2"
  - (2b) LEVELING BINDER, (MACHINE METHOD), N70 - 3/4"
- (3) PORTLAND CEMENT CONCRETE BASE COURSE - 10"
- (4) AGGREGATE BASE COURSE, TYPE A, 12"
- (5) PORTLAND CEMENT CONCRETE SHOULDERS
  - (5A) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (5B) - 12" (WB & EB I-64 TIE-IN)
  - (5C) - 15 1/2" (WB I-64 GOSES)
  - (5D) - 18" (EB I-64 GOSES)
- (6) AGGREGATE SHOULDERS, TYPE B
  - (6A) - 10" (FOR 10" PCC JT)
  - (6B) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (6C) - 11 1/4" (FOR TEMPORARY PAVEMENT INTERSTATE)
  - (6D) - 15 1/2" (FOR HMA SC "D" N70 - 3 1/2" AND PCC SHOULDERS 12")
  - (6E) - 18" (FOR HMA SC "D" N70 - 6" AND PCC SHOULDERS 12")
- (7) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (8) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (9) POROUS GRANULAR EMBANKMENT, SPECIAL
- (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06
- (11) CONCRETE MEDIAN, TYPE SM-6.12
- (12) \*6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / \*6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (13) \*6 TIE BARS, 24" LONG AT 24" C-C (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (14) PIPE UNDERDRAINS - 4"
- (15) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT - 12"
- (16) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT
- (17) AGGREGATE BASE COURSE, TYPE A, THICKNESS AS SPECIFIED ON SECTION
- (18) CONCRETE GUTTER TYPE A
- (19) CONCRETE MEDIAN SURFACE - 4"
- (20) STEEL PLATE BEAM GUARDRAIL, TYPE A, 6' POSTS OR TRAF BAR TERM
- (21) CHAIN LINK FENCE - 6'
- (22) SEEDING AND MULCHING (SEE SEEDING AND MULCHING SCHEDULE)
- (23) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- (24) CONCRETE CURB AND GUTTER OUTLET SPECIAL
- (25) HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- (26) TEMPORARY PAVEMENT (INTERSTATE) - 11 1/4"
  - (26a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) - 1 3/4"
  - (26b) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - 9 1/2"
- (27) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 3 1/2"
- (28) BITUMINOUS MATERIALS (PRIME COAT)
- (29) AGGREGATE (PRIME COAT)
- (30) HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3 1/2"
- (32) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 6"

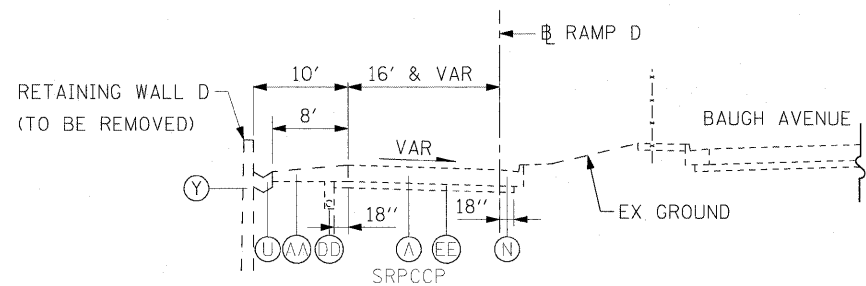
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

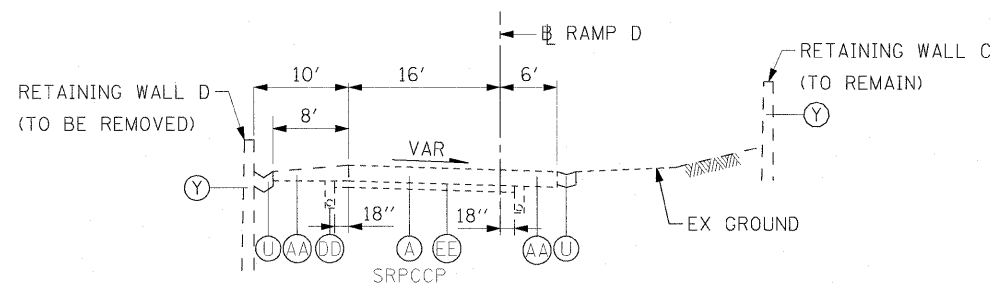
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	24
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	



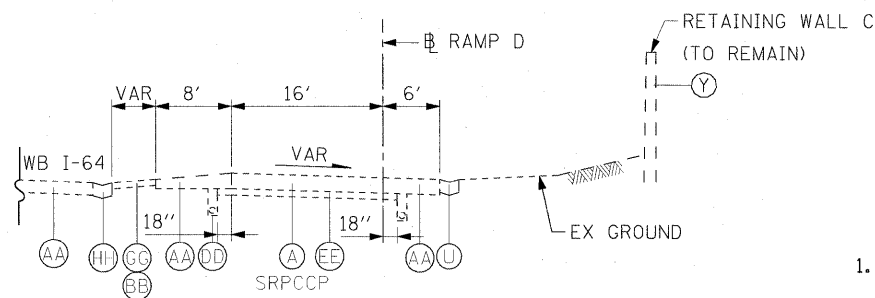
EXISTING RAMP "D" TO BE REMOVED  
(SHOWN FOR INFORMATION ONLY)

STA 94+71.50 TO STA 97+15.50 (LT) (64WB)



EXISTING RAMP "D" TO BE REMOVED  
(SHOWN FOR INFORMATION ONLY)

STA 97+15.50 TO STA 99+30.50 (LT) (64WB)  
STA 95+14.70 TO STA 100+15.40 (RT) (64WB)



EXISTING RAMP "D" TO BE REMOVED  
(SHOWN FOR INFORMATION ONLY)

STA 99+30.50 TO STA 103+66.30 (LT) (64WB)  
STA 100+15.40 TO STA 103+66.30 (RT) (64WB)

EXISTING LEGEND:

- (A) PCC PAVEMENT - 10"
- (B) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 3/4"
- (C) SUB-BASE GRANULAR MATERIAL, TYPE A - 4"
- (D) SUB-BASE GRANULAR MATERIAL, TYPE A - 6"
- (E) BITUMINOUS MATERIALS (PRIME COAT)
- (F) AGGREGATE (PRIME COAT)
- (G) LEVELING BINDER (MACHINE METHOD)
- (H) BITUMINOUS CONCRETE BINDER COURSE - 2 1/4"
- (I) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 1 1/2"
- (J) PORTLAND CEMENT CONCRETE BASE COURSE - 8"
- (K) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (L) COMBINATION CURB AND GUTTER TYPE B-6.12
- (M) COMBINATION CURB AND GUTTER TYPE B-6.12 (SPECIAL)
- (N) COMBINATION CURB AND GUTTER TYPE B-6.24
- (O) PCC PAVEMENT - 10" AND VARIES
- (P) CONCRETE CURB, TYPE B - 6" HIGH
- (Q) TYPE B CURB (MODIFIED)
- (R) PCC MEDIAN SURFACE - 4"
- (S) CONCRETE MEDIAN SURFACE - 4"
- (T) PORTLAND CEMENT CONCRETE BASE COURSE - 9"
- (U) CONCRETE GUTTER
- (V) FENCE
- (W) LONGITUDINAL METAL JOINT
- (X) EXISTING CURB
- (Y) CONCRETE RETAINING WALL
- (Z) BITUMINOUS CONCRETE BINDER COURSE - 1 1/2"
- (AA) STABILIZED SHOULDER
- (BB) AGGREGATE BASE COURSE, TYPE A - 4"
- (CC) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (CRPCCP) - VARIES FROM 12" TO 13 1/4"
- (DD) PIPE UNDERDRAINS - 6"
- (EE) STABILIZED SUB-BASE - 4"
- (FF) AGGREGATE SHOULDER TYPE A
- (GG) INCIDENTAL BITUMINOUS SURFACING
- (HH) CONCRETE GUTTER (MODIFIED)
- (II) HMA SURFACE COURSE, MIX "C", N70 - 2"
- (JJ) HMA OVERLAY

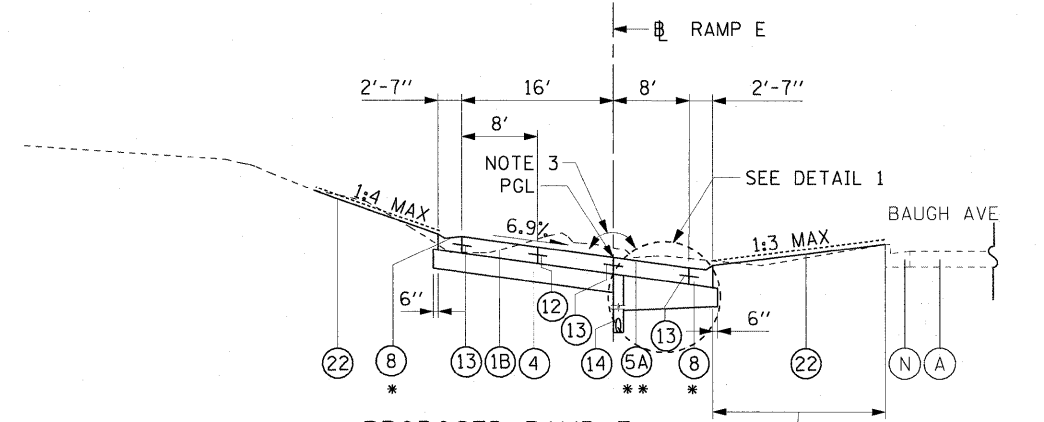
PROPOSED LEGEND:

- (1) PORTLAND CEMENT CONCRETE PAVEMENT
  - (1A) - 10" (JOINTED) (LOCAL ROADS)
  - (1B) - 10 1/2" (JOINTED) (RAMPS)
- (2) HOT-MIX ASPHALT OVERLAY - 2 1/4"
  - (2a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 - 1 1/2"
  - (2b) LEVELING BINDER, (MACHINE METHOD), N70 - 3/4"
- (3) PORTLAND CEMENT CONCRETE BASE COURSE - 10"
- (4) AGGREGATE BASE COURSE, TYPE A, 12"
- (5) PORTLAND CEMENT CONCRETE SHOULDERS
  - (5A) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (5B) - 12" (WB & EB I-64 TIE-IN)
  - (5C) - 15 1/2" (WB I-64 GOSES)
  - (5D) - 18" (EB I-64 GOSES)
- (6) AGGREGATE SHOULDERS, TYPE B
  - (6A) - 10" (FOR 10" PCC JT)
  - (6B) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (6C) - 11 1/4" (FOR TEMPORARY PAVEMENT INTERSTATE)
  - (6D) - 15 1/2" (FOR HMA SC "D" N70 - 3 1/2" AND PCC SHOULDERS 12")
  - (6E) - 18" (FOR HMA SC "D" N70 - 6" AND PCC SHOULDERS 12")
- (7) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (8) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (9) POROUS GRANULAR EMBANKMENT, SPECIAL
- (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06
- (11) CONCRETE MEDIAN, TYPE SM-6.12
- (12) \*6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / \*6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (13) \*6 TIE BARS, 24" LONG AT 24" C-C (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (14) PIPE UNDERDRAINS - 4"
- (15) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT - 12"
- (16) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT
- (17) AGGREGATE BASE COURSE, TYPE A, THICKNESS AS SPECIFIED ON SECTION
- (18) CONCRETE GUTTER TYPE A
- (19) CONCRETE MEDIAN SURFACE - 4"
- (20) STEEL PLATE BEAM GUARDRAIL, TYPE A, 6' POSTS OR TRAF BAR TERM
- (21) CHAIN LINK FENCE - 6'
- (22) SEEDING AND MULCHING (SEE SEEDING AND MULCHING SCHEDULE)
- (23) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- (24) CONCRETE CURB AND GUTTER OUTLET SPECIAL
- (25) HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- (26) TEMPORARY PAVEMENT (INTERSTATE) - 11 1/4"
  - (26a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) - 1 3/4"
  - (26b) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - 9 1/2"
- (27) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 3 1/2"
- (28) BITUMINOUS MATERIALS (PRIME COAT)
- (29) AGGREGATE (PRIME COAT)
- (30) HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3 1/2"
- (32) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 6"

NOTES:

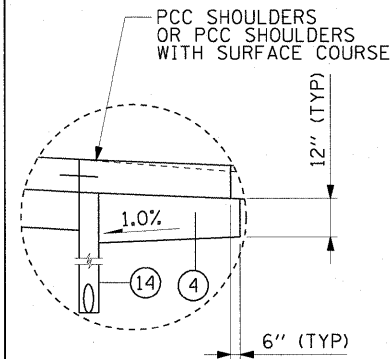
1. DUE THE SKEWED NATURE OF THE ROADWAYS IN THIS PROJECT, THE FOLLOWINGS ARE THE COMPASS DESIGNATIONS TO THE ROADWAYS:  
  
15TH STREET (NORTH-SOUTH), BAUGH AVENUE (EAST-WEST), ST. CLAIR AVENUE (EAST-WEST), WB I-64 (WEST)
2. SEE HOT-MIX ASPHALT MIXTURE REQUIREMENTS ON TYPICAL SECTIONS SHEET 3 OF 14.
3. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER SLOPES WILL NOT BE GREATER THAN 8%.

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		CHECKED DBM	REVISED -										
		DATE 03/19/10	REVISED -										
								CONTRACT NO. 76C49					

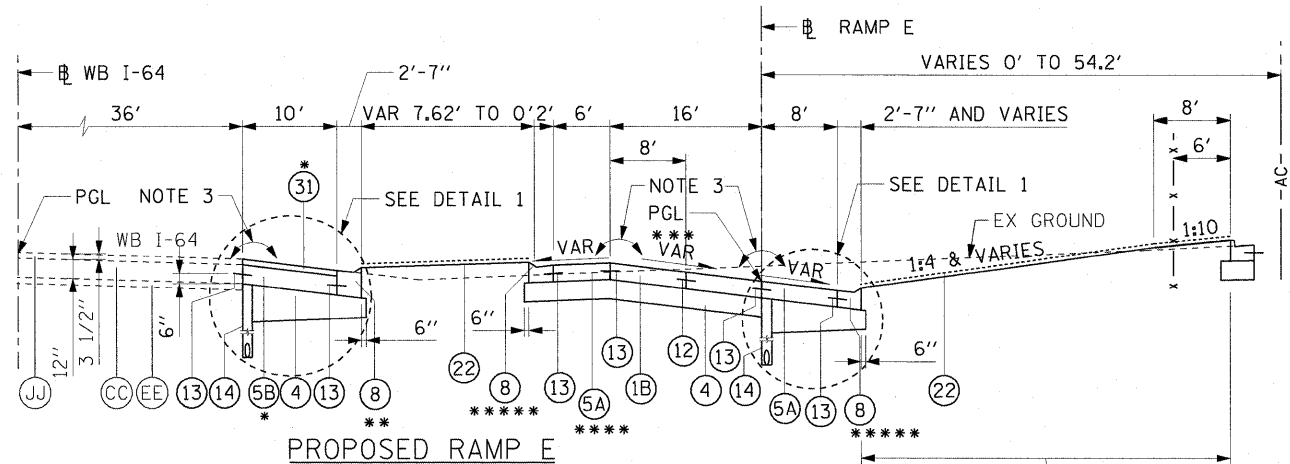


**PROPOSED RAMP E**  
STA 10+91.65 TO STA 11+34.38

- \* (8) LT FROM STA 10+91.65 TO STA 13+00 AND (DEPRESSED FROM STA 11+15.00 TO STA 13+00)
- (8) RT FROM STA 11+06.78 TO STA 15+33.96 (DEPRESSED)
- \*\* BEGIN (5A) (LT) AT 11+40.07, 17' LT



**DETAIL 1**



**PROPOSED RAMP E**  
STA 11+34.38 TO STA 15+38.49

- \* BEGIN (31) & (5B) AT STA 87+42.34 WB I-64 = STA 11+34.74 RAMP E
- \*\* COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (DEPRESSED) BEGINS FROM STA 86+42.47 TO STA 90+57.05 (WB I-64) SEE SE TRANSITION DATA TABLE FOR SUPERELEVATION TRANSITIONS
- \*\*\* (5A) BEGINS AT STA 11+40.07 AND VARIES FROM 1' TO 6' FROM STA 11+40.07 TO STA 12+04.89
- \*\*\*\*\* (8) DEPRESSED LT FROM STA 11+34.38 TO STA 13+00 AND (6B) LT FROM STA 13+00 TO STA 15+38.49
- (8) DEPRESSED RT FROM STA 11+34.38 TO STA 15+33.96 AND (6B) FROM STA 15+33.96 TO STA 15+38.49

**EXISTING LEGEND:**

- (A) PCC PAVEMENT - 10"
- (B) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 3/4"
- (C) SUB-BASE GRANULAR MATERIAL, TYPE A - 4"
- (D) SUB-BASE GRANULAR MATERIAL, TYPE A - 6"
- (E) BITUMINOUS MATERIALS (PRIME COAT)
- (F) AGGREGATE (PRIME COAT)
- (G) LEVELING BINDER (MACHINE METHOD)
- (H) BITUMINOUS CONCRETE BINDER COURSE - 2 1/4"
- (I) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 1 1/2"
- (J) PORTLAND CEMENT CONCRETE BASE COURSE - 8"
- (K) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (L) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (M) COMBINATION CURB AND GUTTER TYPE B-6.12
- (N) COMBINATION CURB AND GUTTER TYPE B-6.12 (SPECIAL)
- (O) COMBINATION CURB AND GUTTER TYPE B-6.24
- (P) PCC PAVEMENT - 10" AND VARIES
- (Q) CONCRETE CURB, TYPE B - 6" HIGH
- (R) TYPE B CURB (MODIFIED)
- (S) PCC MEDIAN SURFACE - 4"
- (T) CONCRETE MEDIAN SURFACE - 4"
- (U) PORTLAND CEMENT CONCRETE BASE COURSE - 9"
- (V) CONCRETE GUTTER
- (W) FENCE
- (X) LONGITUDINAL METAL JOINT
- (Y) EXISTING CURB
- (Z) CONCRETE RETAINING WALL
- (AA) BITUMINOUS CONCRETE BINDER COURSE - 1 1/2"
- (AB) STABILIZED SHOULDER
- (AC) AGGREGATE BASE COURSE, TYPE A - 4"
- (AD) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (CRPCCP) - VARIES FROM 12" TO 13 1/4"
- (AE) PIPE UNDERDRAINS - 6"
- (AF) STABILIZED SUB-BASE - 4"
- (AG) AGGREGATE SHOULDER TYPE A
- (AH) INCIDENTAL BITUMINOUS SURFACING
- (AI) CONCRETE GUTTER (MODIFIED)
- (AJ) HMA SURFACE COURSE, MIX "C", N70 - 2"
- (AK) HMA OVERLAY

**PROPOSED LEGEND:**

- (1) PORTLAND CEMENT CONCRETE PAVEMENT
  - (1A) - 10" (JOINTED) (LOCAL ROADS)
  - (1B) - 10 1/2" (JOINTED) (RAMPS)
- (2) HOT-MIX ASPHALT OVERLAY - 2 1/4"
  - (2a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 - 1 1/2"
  - (2b) LEVELING BINDER, (MACHINE METHOD), N70 - 3/4"
- (3) PORTLAND CEMENT CONCRETE BASE COURSE - 10"
- (4) AGGREGATE BASE COURSE, TYPE A, 12"
- (5) PORTLAND CEMENT CONCRETE SHOULDERS
  - (5A) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (5B) - 12" (WB & EB I-64 TIE-IN)
  - (5C) - 15 1/2" (WB I-64 GOSES)
  - (5D) - 18" (EB I-64 GOSES)
- (6) AGGREGATE SHOULDERS, TYPE B
  - (6A) - 10" (FOR 10" PCC JT)
  - (6B) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (6C) - 11 1/4" (FOR TEMPORARY PAVEMENT INTERSTATE)
  - (6D) - 15 1/2" (FOR HMA SC "D" N70 - 3 1/2" AND PCC SHOULDERS 12")
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- (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06
- (11) CONCRETE MEDIAN, TYPE SM-6.12
- (12) \*6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / \*6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
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- (17) AGGREGATE BASE COURSE, TYPE A, THICKNESS AS SPECIFIED ON SECTION
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- (24) CONCRETE CURB AND GUTTER OUTLET SPECIAL
- (25) HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- (26) TEMPORARY PAVEMENT (INTERSTATE) - 11 1/4"
  - (26a) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) - 1 3/4"
  - (26b) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - 9/2"
- (27) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 3 1/2"
- (28) BITUMINOUS MATERIALS (PRIME COAT)
- (29) AGGREGATE (PRIME COAT)
- (30) HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3 1/2"
- (32) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 6"

**NOTES:**

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2. SEE HOT-MIX ASPHALT MIXTURE REQUIREMENTS ON TYPICAL SECTIONS SHEET 3 OF 14.
3. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER SLOPES WILL NOT BE GREATER THAN 8%.

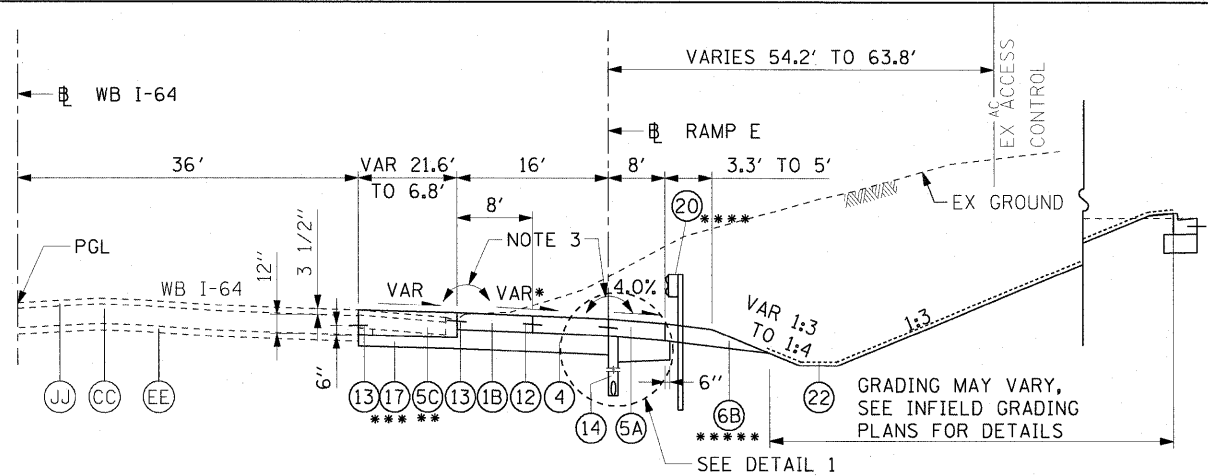
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE: NONE SHEET NO. 12 OF 14 SHEETS STA. TO STA.

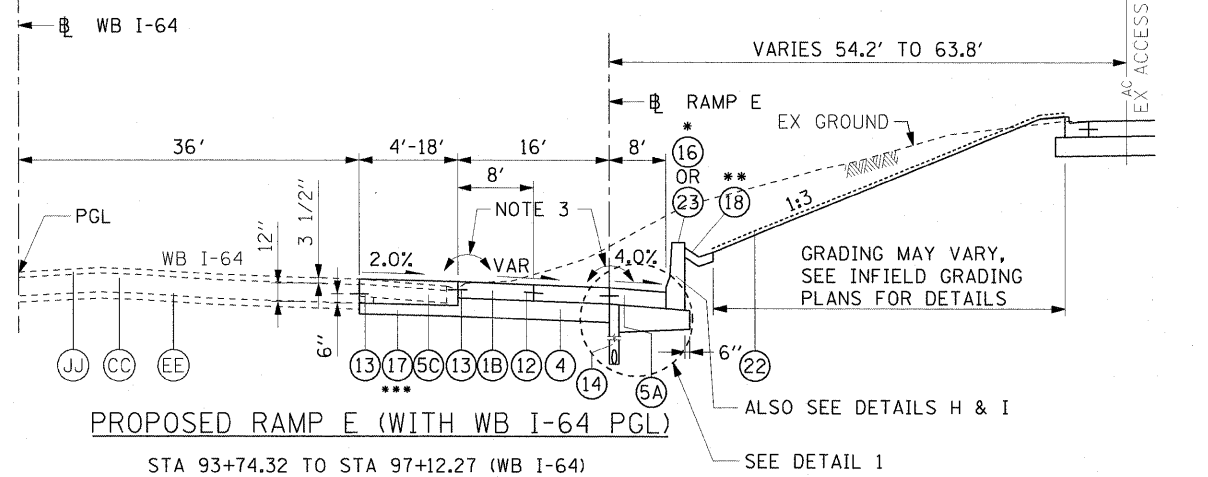
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	26
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	



**PROPOSED RAMP E (WITH WB I-64 PGL)**

STA 90+57.47 TO STA 93+74.32 (WB I-64)  
 = STA 15+38.49 TO STA 18+52.52 (RAMP E)

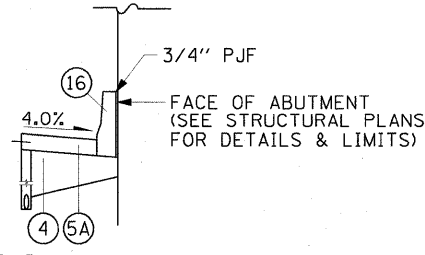
- \* SEE SE TRANSITION DATA TABLE FOR SUPERELEVATION TRANSITIONS
- \*\* 8' GORE WIDTH AT STA 17+91.03
- \*\*\* 17 - 7" THICK
- \*\*\*\* STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS AND TRAFFIC BARRIER TERMINALS FROM STA 15+58.96 TO STA 17+77.70, 23 FROM STA 17+77.70 TO STA 18+52.52
- \*\*\*\*\* 6B WIDTH VARIES AT TRAFFIC BARRIER TERMINAL AND STEEL PLATE BEAM GUARDRAIL, SEE ROADWAY PLANS



**PROPOSED RAMP E (WITH WB I-64 PGL)**

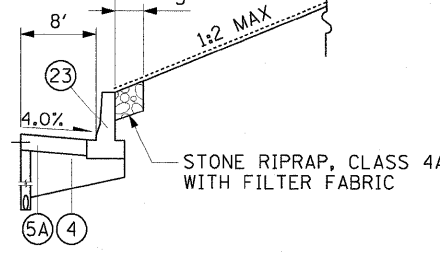
STA 93+74.32 TO STA 97+12.27 (WB I-64)  
 = STA 18+52.52 TO STA 19+91.03 (RAMP E)

- \* 16 CONC BAR 1F 42HT FROM STA 18+64.28 TO 19+29.20 (RT) (ALSO SEE DETAIL H)
- 23 CONC BAR 1F 42HT SPL FROM STA 17+77.70 TO STA 18+64.28 (RT) AND STA 19+29.20 TO STA 22+53.20 (RT) (ALSO SEE DETAIL H & I)
- \*\* CONCRETE GUTTER FROM STA 18+52.50 TO STA 18+64.28 AND STA 19+47.87 TO STA 22+78.25
- \*\*\* 17 - 7" THICK



**DETAIL H**

23 FROM STA 18+52.50 TO STA 18+64.28  
 16 FROM STA 18+64.28 TO STA 19+29.20



**DETAIL I**

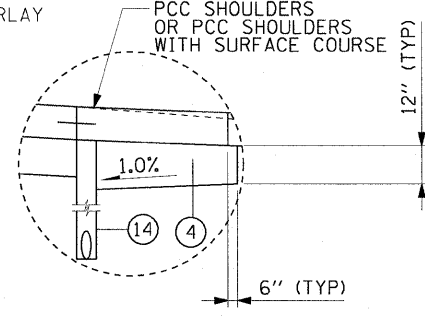
STA 19+29.20 TO STA 19+47.87

**EXISTING LEGEND:**

- (A) PCC PAVEMENT - 10"
- (B) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 3/4"
- (C) SUB-BASE GRANULAR MATERIAL, TYPE A - 4"
- (D) SUB-BASE GRANULAR MATERIAL, TYPE A - 6"
- (E) BITUMINOUS MATERIALS (PRIME COAT)
- (F) AGGREGATE (PRIME COAT)
- (G) LEVELING BINDER (MACHINE METHOD)
- (H) BITUMINOUS CONCRETE BINDER COURSE - 2/4"
- (I) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 1 1/2"
- (J) PORTLAND CEMENT CONCRETE BASE COURSE - 8"
- (K) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (L) COMBINATION CURB AND GUTTER TYPE B-6.12
- (M) COMBINATION CURB AND GUTTER TYPE B-6.12 (SPECIAL)
- (N) COMBINATION CURB AND GUTTER TYPE B-6.24
- (O) PCC PAVEMENT - 10" AND VARIES
- (P) CONCRETE CURB, TYPE B - 6" HIGH
- (Q) TYPE B CURB (MODIFIED)
- (R) PCC MEDIAN SURFACE - 4"
- (S) CONCRETE MEDIAN SURFACE - 4"
- (T) PORTLAND CEMENT CONCRETE BASE COURSE - 9"
- (U) CONCRETE GUTTER
- (V) FENCE
- (W) LONGITUDINAL METAL JOINT
- (X) EXISTING CURB
- (Y) CONCRETE RETAINING WALL
- (Z) BITUMINOUS CONCRETE BINDER COURSE - 1 1/2"
- (AA) STABILIZED SHOULDER
- (BB) AGGREGATE BASE COURSE, TYPE A - 4"
- (CC) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (CRPCCP) - VARIES FROM 12" TO 13 1/4"
- (DD) PIPE UNDERDRAINS - 6"
- (EE) STABILIZED SUB-BASE - 4"
- (FF) AGGREGATE SHOULDER TYPE A
- (GG) INCIDENTAL BITUMINOUS SURFACING
- (HH) CONCRETE GUTTER (MODIFIED)
- (II) HMA SURFACE COURSE, MIX "C", N70 - 2"
- (JJ) HMA OVERLAY

**PROPOSED LEGEND:**

- 1 PORTLAND CEMENT CONCRETE PAVEMENT
  - 1A - 10" (JOINTED) (LOCAL ROADS)
  - 1B - 10 1/2" (JOINTED) (RAMPS)
- 2 HOT-MIX ASPHALT OVERLAY - 2 1/4"
  - 2a HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 - 1 1/2"
  - 2b LEVELING BINDER, (MACHINE METHOD), N70 - 3/4"
- 3 PORTLAND CEMENT CONCRETE BASE COURSE - 10"
- 4 AGGREGATE BASE COURSE, TYPE A, 12"
- 5 PORTLAND CEMENT CONCRETE SHOULDERS
  - 5A - 10 1/2" (FOR 10 1/2" PCC JT)
  - 5B - 12" (WB & EB I-64 TIE-IN)
  - 5C - 15 1/2" (WB I-64 GOES)
  - 5D - 18" (EB I-64 GOES)
- 6 AGGREGATE SHOULDERS, TYPE B
  - 6A - 10" (FOR 10" PCC JT)
  - 6B - 10 1/2" (FOR 10 1/2" PCC JT)
  - 6C - 11 1/4" (FOR TEMPORARY PAVEMENT INTERSTATE)
  - 6D - 15 1/2" (FOR HMA SC "D" N70 - 3 1/2" AND PCC SHOULDERS 12")
  - 6E - 18" (FOR HMA SC "D" N70 - 6" AND PCC SHOULDERS 12")
- 7 PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- 8 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- 9 POROUS GRANULAR EMBANKMENT, SPECIAL
- 10 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06
- 11 CONCRETE MEDIAN, TYPE SM-6.12
- 12 \*6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / \*6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- 13 \*6 TIE BARS, 24" LONG AT 24" C-C (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- 14 PIPE UNDERDRAINS - 4"
- 15 CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT - 12"
- 16 CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT
- 17 AGGREGATE BASE COURSE, TYPE A, THICKNESS AS SPECIFIED ON SECTION
- 18 CONCRETE GUTTER TYPE A
- 19 CONCRETE MEDIAN SURFACE - 4"
- 20 STEEL PLATE BEAM GUARDRAIL, TYPE A, 6' POSTS OR TRAF BAR TERM
- 21 CHAIN LINK FENCE - 6'
- 22 SEEDING AND MULCHING (SEE SEEDING AND MULCHING SCHEDULE)
- 23 CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- 24 CONCRETE CURB AND GUTTER OUTLET SPECIAL
- 25 HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- 26 TEMPORARY PAVEMENT (INTERSTATE) - 1 1/4"
  - 26a HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) - 1 3/4"
  - 26b HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - 9/2"
- 27 POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 3 1/2"
- 28 BITUMINOUS MATERIALS (PRIME COAT)
- 29 AGGREGATE (PRIME COAT)
- 30 HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
- 31 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3 1/2"
- 32 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 6"



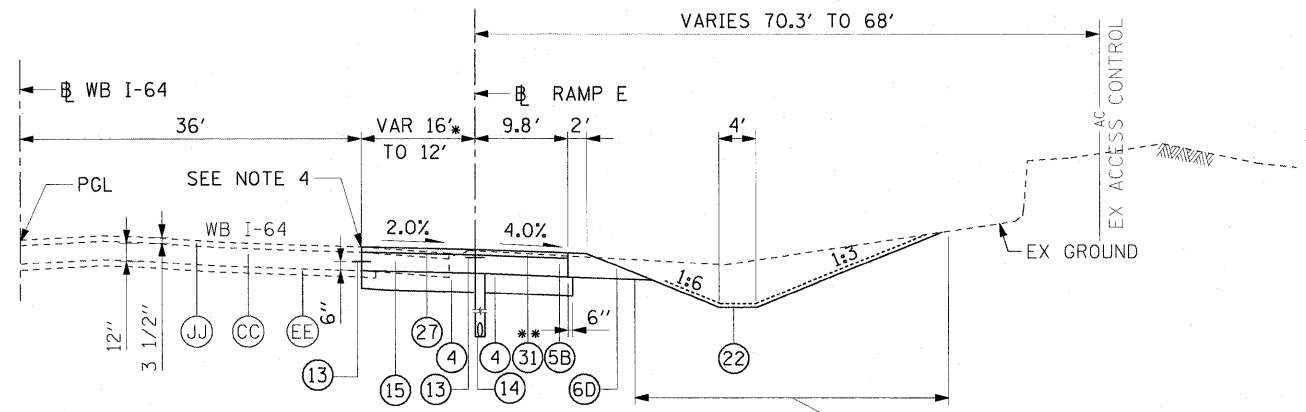
**DETAIL 1**

**NOTES:**

1. DUE THE SKEWED NATURE OF THE ROADWAYS IN THIS PROJECT, THE FOLLOWINGS ARE THE COMPASS DESIGNATIONS TO THE ROADWAYS:  
 15TH STREET (NORTH-SOUTH), BAUGH AVENUE (EAST-WEST), ST. CLAIR AVENUE (EAST-WEST), WB I-64 (WEST)
2. SEE HOT-MIX ASPHALT MIXTURE REQUIREMENTS ON TYPICAL SECTIONS SHEET 3 OF 14.
3. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER SLOPES WILL NOT BE GREATER THAN 8%.

FILE NAME =	USER NAME = pmsarno	DESIGNED OP	REVISED - 4/15/2010	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SECTIONS</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
#FILEA*	PLOT SCALE = 10,000' / IN.	DRAWN OP	REVISED -			64	82-1-2HB	ST. CLAIR	345	27	
	PLOT DATE = 4/7/2010	CHECKED DBM	REVISED -			SCALE: NONE SHEET NO. 13 OF 14 SHEETS STA. TO STA.					
		DATE 03/19/10	REVISED -			CONTRACT NO. 76C49					



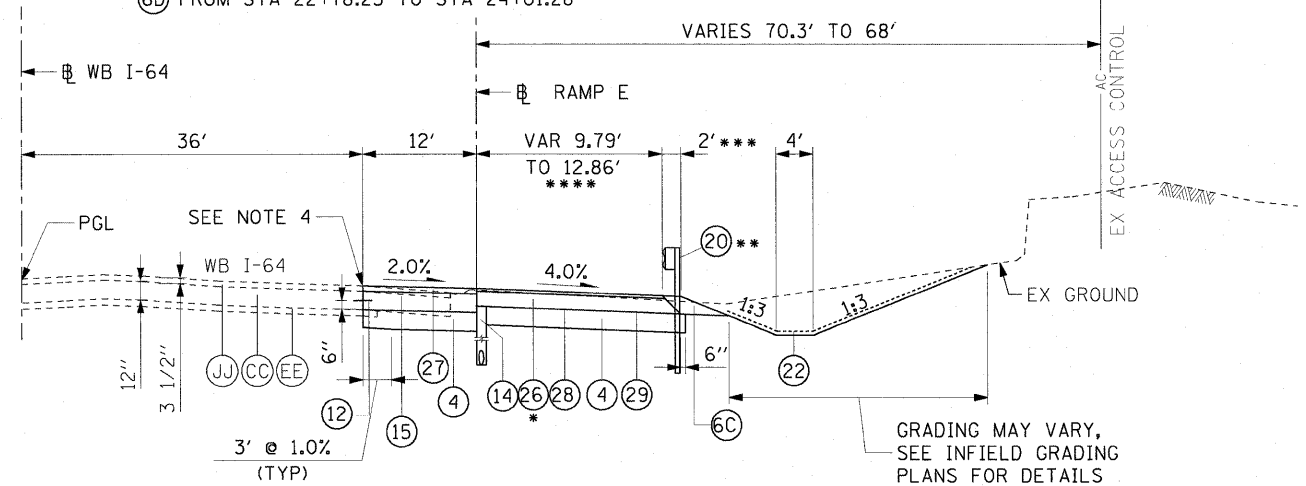


PROPOSED RAMP E (WITH WB I-64 PGL)

STA 97+12.27 TO STA 99+22.80 (WB I-64) = STA 19+91.03 TO STA 24+01.28 (RAMP E)  
 STA 24+01.28 END OF RAMP E BASELINE

- \* LANE WIDTH = 12' FROM STA 23+90.91 TO STA 24+01.28
- \*\* VARIES FROM 8' TO 9.79' STA 23+01.22 TO STA 23+90.91 AND 9.79' FROM STA 23+90.91 TO STA 24+01.28
- ⑥D FROM STA 22+78.25 TO STA 24+01.28

GRADING MAY VARY, SEE INFIELD GRADING PLANS FOR DETAILS



PROPOSED RAMP E (WITH WB I-64 PGL)

STA 99+22.80 TO STA 105+15.26 (WB I-64)

- \* TEMPORARY PAVEMENT BEGINS AT STA 101+65.26
- \*\* STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS, AND TRAFFIC BARRIER TERMINALS FROM STA 104+22.50 TO 106+72.50
- \*\*\* ⑥ WIDTH VARIES AT TRAFFIC BARRIER TERMINAL AND STEEL PLATE BEAM GUARDRAIL, SEE ROADWAY PLANS
- \*\*\*\* 9.79' FROM STA 99+12.43 TO 103+65.26 AND VARIES FROM 9.79' TO 12.86' FROM 103+65.26 TO STA 105+15.26

RAMP E

STRUCTURAL DESIGN TRAFFIC:		YEAR	2030
PV=	1,314	SU=	82
		MU=	246
ROAD/STREET CLASSIFICATION:		CLASS	I
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:			
P=	80%	S=	5%
		M=	15%
TRAFFIC FACTOR:		ACTUAL TF=	3.67
		AC TYPE=	20
MINIMUM TF= 11.17			
PG GRADE:		BINDER=	NA
		SURFACE=	NA
SUBGRADE SUPPORT RATING			
SSR= POOR			

EXISTING LEGEND:

- (A) PCC PAVEMENT - 10"
- (B) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 3/4"
- (C) SUB-BASE GRANULAR MATERIAL, TYPE A - 4"
- (D) SUB-BASE GRANULAR MATERIAL, TYPE A - 6"
- (E) BITUMINOUS MATERIALS (PRIME COAT)
- (F) AGGREGATE (PRIME COAT)
- (G) LEVELING BINDER (MACHINE METHOD)
- (H) BITUMINOUS CONCRETE BINDER COURSE - 2 1/4"
- (I) BITUMINOUS CONCRETE SURFACE COURSE, CLASS I - 1 1/2"
- (J) PORTLAND CEMENT CONCRETE BASE COURSE - 8"
- (K) PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- (L) COMBINATION CURB AND GUTTER TYPE B-6.12
- (M) COMBINATION CURB AND GUTTER TYPE B-6.12 (SPECIAL)
- (N) COMBINATION CURB AND GUTTER TYPE B-6.24
- (O) PCC PAVEMENT - 10" AND VARIES
- (P) CONCRETE CURB, TYPE B - 6" HIGH
- (Q) TYPE B CURB (MODIFIED)
- (R) PCC MEDIAN SURFACE - 4"
- (S) CONCRETE MEDIAN SURFACE - 4"
- (T) PORTLAND CEMENT CONCRETE BASE COURSE - 9"
- (U) CONCRETE GUTTER
- (V) FENCE
- (W) LONGITUDINAL METAL JOINT
- (X) EXISTING CURB
- (Y) CONCRETE RETAINING WALL
- (Z) BITUMINOUS CONCRETE BINDER COURSE - 1 1/2"
- (AA) STABILIZED SHOULDER
- (BB) AGGREGATE BASE COURSE, TYPE A - 4"
- (CC) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (CRPCCP) - VARIES FROM 12" TO 13 1/4"
- (DD) PIPE UNDERDRAINS - 6"
- (EE) STABILIZED SUB-BASE - 4"
- (FF) AGGREGATE SHOULDER TYPE A
- (GG) INCIDENTAL BITUMINOUS SURFACING
- (HH) CONCRETE GUTTER (MODIFIED)
- (II) HMA SURFACE COURSE, MIX "C", N70 - 2"
- (JJ) HMA OVERLAY

PROPOSED LEGEND:

- ① PORTLAND CEMENT CONCRETE PAVEMENT
  - (1A) - 10" (JOINTED) (LOCAL ROADS)
  - (1B) - 10 1/2" (JOINTED) (RAMPS)
- ② HOT-MIX ASPHALT OVERLAY - 2 1/4"
  - (2A) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 - 1 1/2"
  - (2B) LEVELING BINDER, (MACHINE METHOD), N70 - 3/4"
- ③ PORTLAND CEMENT CONCRETE BASE COURSE - 10"
- ④ AGGREGATE BASE COURSE, TYPE A, 12"
- ⑤ PORTLAND CEMENT CONCRETE SHOULDERS
  - (5A) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (5B) - 12" (WB & EB I-64 TIE-IN)
  - (5C) - 15 1/2" (WB I-64 GOES)
  - (5D) - 18" (EB I-64 GOES)
- ⑥ AGGREGATE SHOULDERS, TYPE B
  - (6A) - 10" (FOR 10" PCC JT)
  - (6B) - 10 1/2" (FOR 10 1/2" PCC JT)
  - (6C) - 11 1/4" (FOR TEMPORARY PAVEMENT INTERSTATE)
  - (6D) - 15 1/2" (FOR HMA SC "D" N70 - 3 1/2" AND PCC SHOULDERS 12")
  - (6E) - 18" (FOR HMA SC "D" N70 - 6" AND PCC SHOULDERS 12")
- ⑦ PORTLAND CEMENT CONCRETE SIDEWALK - 4"
- ⑧ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- ⑨ POROUS GRANULAR EMBANKMENT, SPECIAL
- ⑩ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06
- ⑪ CONCRETE MEDIAN, TYPE SM-6.12
- ⑫ #6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / #6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- ⑬ #6 TIE BARS, 24" LONG AT 24" C-C (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- ⑭ PIPE UNDERDRAINS - 4"
- ⑮ CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT - 12"
- ⑯ CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT
- ⑰ AGGREGATE BASE COURSE, TYPE A, THICKNESS AS SPECIFIED ON SECTION
- ⑱ CONCRETE GUTTER TYPE A
- ⑲ CONCRETE MEDIAN SURFACE - 4"
- ⑳ STEEL PLATE BEAM GUARDRAIL, TYPE A, 6' POSTS OR TRAF BAR TERM
- ㉑ CHAIN LINK FENCE - 6'
- ㉒ SEEDING AND MULCHING (SEE SEEDING AND MULCHING SCHEDULE)
- ㉓ CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- ㉔ CONCRETE CURB AND GUTTER OUTLET SPECIAL
- ㉕ HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- ㉖ TEMPORARY PAVEMENT (INTERSTATE) - 11 1/4"
  - (26A) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) - 1 3/4"
  - (26B) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - 9/2"
- ㉗ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 3 1/2"
- ㉘ BITUMINOUS MATERIALS (PRIME COAT)
- ㉙ AGGREGATE (PRIME COAT)
- ㉚ HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
- ㉛ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3 1/2"
- ㉜ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 6"

NOTES:

1. DUE TO THE SKEWED NATURE OF THE ROADWAYS IN THIS PROJECT, THE FOLLOWINGS ARE THE COMPASS DESIGNATIONS TO THE ROADWAYS:  
 15TH STREET (NORTH -SOUTH), BOUGH AVENUE (EAST- WEST), ST. CLAIR AVENUE (EAST- WEST), B I-64 (WEST)
2. SEE HOT-MIX ASPHALT MIXTURE REQUIREMENTS ON TYPICAL SECTIONS SHEET 3 OF 14.
3. WHEN THE SUPER ELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE SUPER ELEVATION RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER SLOPES WILL NOT BE GREATER THAN 8%.
4. DRILL AND GROUT NO. 6 TIE BARS AT 24" CC WHERE TYING INTO EXISTING PAVEMENT. SAW AND SEAL LONGITUDINAL JOINT 1/4" WIDE AND 5/8" DEEP. SAWING AND SEALING IS INCLUDED IN THE COST OF PCC ITEMS AND WILL NOT BE MEASURED.

**REMOVAL SCHEDULE**

SHEET	STATION	TO	STATION	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	TREE REMOVAL (OVER 15 UNITS DIAMETER)	PAVEMENT REMOVAL	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	DRIVEWAY PAVEMENT REMOVAL	CURB REMOVAL	COMBINATION CURB AND GUTTER REMOVAL	SIDEWALK REMOVAL	CONCRETE BARRIER REMOVAL	MEDIAN REMOVAL	PAVED SHOULDER REMOVAL	GUARDRAIL REMOVAL	REMOVAL AND REINSTALLATION OF EXISTING TRAFFIC BARRIER TERMINAL, TYPE 5	FENCE REMOVAL
				UNIT	UNIT	SQ YD	SQ YD	SQ YD	SQ YD	FOOT	FOOT	SQ FT	FOOT	SQ FT	SQ YD	FOOT	EACH	FOOT
15TH STREET																		
PLAN-1	3+00.00	TO	8+50.00	74	72	4,166	-	-	94	781	773	4,333	59	2,024	822	249	1	539
PLAN-2	8+50.00	TO	13+93.34	-	-	3,890	1,191	-	167	-	1,556	4,249	-	1,360	602	101	-	220
RAMP E																		
PLAN-3	84+50.00	TO	111+30.00	15	-	925	-	-	-	-	1,214	2,810	-	-	692	237	-	669
PLAN-4	7+50.00	TO	508+60.00	-	-	1,467	-	-	-	-	646	-	-	-	1,491	-	-	-
PLAN-5	508+60.00	TO	106+72.50	-	-	548	-	-	-	-	-	-	-	-	274	325	-	-
ST. CLAIR AVE AND RAMP A & RAMP B																		
PLAN-6	28+61.00	TO	35+50.00	56	277	735	-	153	127	100	668	2,946	-	-	-	-	-	551
PLAN-7	38+00.00	TO	43+50.00	-	-	1,786	-	-	-	-	586	-	-	772	1,243	164	-	132
PLAN-8	43+50.00	TO	40+04.59	-	-	340	-	-	-	-	82	-	-	-	217	-	-	-
RAMP B																		
PLAN-9	11+00.00	TO	18+33.27	72	68	705	-	-	-	-	254	-	-	-	442	343	-	161
<b>TOTALS</b>				<b>217</b>	<b>417</b>	<b>14,562</b>	<b>1,191</b>	<b>153</b>	<b>388</b>	<b>881</b>	<b>5,779</b>	<b>14,338</b>	<b>59</b>	<b>4,156</b>	<b>5,783</b>	<b>1,419</b>	<b>1</b>	<b>2,272</b>

NOTE:  
1. THE REMOVAL OF THE EXISTING APPROACH SLAB SHALL BE PAID FOR AS PAVEMENT REMOVAL REGARDLESS OF THICKNESS AND REINFORCEMENT.

**PAVEMENT SCHEDULE**

SHEET	STATION	TO	STATION	POROUS GRANULAR EMBANKMENT, SPECIAL	STONE RIPRAP, CLASS A4	FILTER FABRIC	SUB-BASE GRANULAR MATERIAL, TYPE B 4"	AGGREGATE BASE COURSE, TYPE A	AGGREGATE BASE COURSE, TYPE A 12"	PORTLAND CEMENT CONCRETE BASE COURSE 10"	BITUMINOUS MATERIALS (PRIME COAT)	LEVELING BINDER (MACHINE METHOD), N70	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	AGGREGATE (PRIME COAT)	PORTLAND CEMENT CONCRETE PAVEMENT - 10" (JOINTED)	PORTLAND CEMENT CONCRETE PAVEMENT - 10 1/2" (JOINTED)	PROTECTIVE COAT	CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 12"	PAVEMENT REINFORCEMENT 12"	AGGREGATE SHOULDERS, TYPE B	AGGREGATE SHOULDERS, TYPE B 10"	PORTLAND CEMENT CONCRETE SHOULDERS 10 1/2"	PORTLAND CEMENT CONCRETE SHOULDERS 12"	
				CU YD	SQ YD	SQ YD	SQ YD	TON	SQ YD	SQ YD	TON	TON	TON	TON	TON	TON	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	TON	SQ YD	SQ YD	SQ YD
15TH STREET																									
PLAN-1	3+00.00	TO	8+50.00	70	-	-	14	4	5,308	11	3	5	181	-	3	3,649	578	6,508	-	-	-	2,916	56	502	767
PLAN-2	8+50.00	TO	13+92.79	-	7	7	14	63	4,318	153	4	432	123	61	13	2,599	410	5,134	310	310	72	-	206	134	
RAMP E																									
PLAN-3	10+91.65	TO	111+30.00	-	-	-	-	139	3,046	-	1	-	90	-	1	202	1,194	3,597	-	-	-	1,860	-	841	456
PLAN-4	7+50.00	TO	508+60.00	-	-	-	-	-	1,538	-	2	-	100	174	4	1	-	1,437	883	883	1,896	-	-	-	
PLAN-5	508+60.00	TO	105+15.26	-	-	-	-	-	626	-	2	-	-	64	4	-	-	323	323	323	972	-	-	-	
ST. CLAIR AVE AND RAMP A & RAMP B																									
PLAN-6	28+61.00	TO	35+50.00	-	-	-	-	-	2,381	218	1	83	273	-	1	-	1,334	2,251	-	-	-	2,240	-	420	506
PLAN-7	38+00.00	TO	43+50.00	-	-	-	-	63	1,522	356	3	135	30	-	8	-	277	725	-	-	2,280	-	139	-	
PLAN-8	43+50.00	TO	49+00.00	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	932	-	-	-	
RAMP B																									
PLAN-9	11+00.00	TO	15+41.39	-	-	-	-	-	1,672	-	-	-	-	-	-	-	799	1,630	-	-	-	2,808	-	590	-
<b>TOTALS</b>				<b>70</b>	<b>7</b>	<b>7</b>	<b>28</b>	<b>269</b>	<b>20,411</b>	<b>738</b>	<b>16</b>	<b>655</b>	<b>797</b>	<b>299</b>	<b>34</b>	<b>6,451</b>	<b>4,592</b>	<b>21,605</b>	<b>1,516</b>	<b>1,516</b>	<b>15,976</b>	<b>56</b>	<b>2,698</b>	<b>1,863</b>	

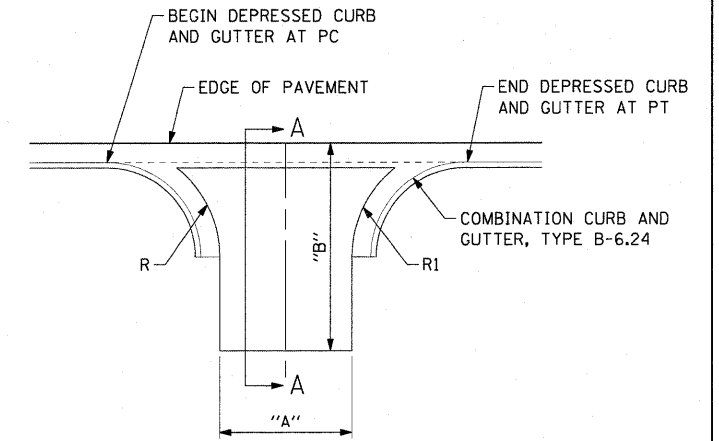
NOTE:  
1. ALSO SEE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS FOR ADDITIONAL PAVEMENT QUANTITIES.

**PAVEMENT SCHEDULE (CONTINUES)**

SHEET	STATION	TO	STATION	PORTLAND CEMENT CONCRETE SHOULDERS - 15"	PORTLAND CEMENT CONCRETE SHOULDERS - 18"	SLEEPER SLAB	TEMPORARY PAVEMENT (INTERSTATE)
				SQ YD	SQ YD	FOOT	SQ YD
15TH STREET							
PLAN-1	3+00.00	TO	8+50.00	-	12	-	-
PLAN-2	8+50.00	TO	13+92.79	162	-	-	-
RAMP E							
PLAN-3	10+91.65	TO	111+30.00	367	-	-	-
PLAN-4	7+50.00	TO	508+60.00	-	-	-	118
PLAN-5	508+60.00	TO	105+15.26	-	-	-	290
ST. CLAIR AVE AND RAMP A & RAMP B							
PLAN-6	28+61.00	TO	35+50.00	-	-	-	-
PLAN-7	38+00.00	TO	43+50.00	-	251	16	654
PLAN-8	43+50.00	TO	49+00.00	-	-	-	-
RAMP B							
PLAN-9	11+00.00	TO	15+41.39	-	235	-	-
<b>TOTALS</b>				<b>529</b>	<b>498</b>	<b>16</b>	<b>1,062</b>

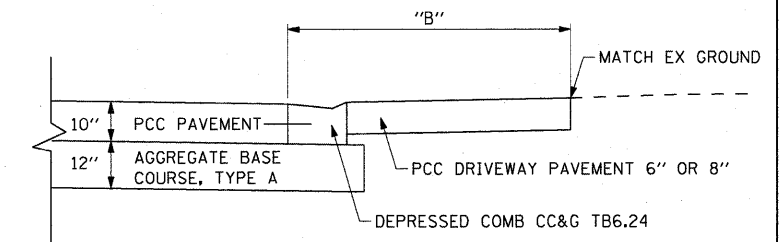
**ENTRANCE SCHEDULE**

STATION	OFFSET	ENTRANCE TYPE	A	B	R	R1	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH
			FOOT	FOOT	FOOT	FOOT	SQ YD	SQ YD
15TH STREET								
11+18.47	LT	CE	32.2	33.1	50	20	-	138
11+26.39	RT	PE	12.0	16.4	10	10	21	-
11+83.77	LT	PE	14.0	29.3	12	12	45	-
BAUGH AVENUE								
6+25.64	RT	CE	29.2	23.0	20	10	-	70
<b>TOTALS</b>							<b>66</b>	<b>208</b>



**TYPICAL COMMERCIAL ENTRANCE DETAIL**

SCALE: NONE



**SECTION A-A**

SCALE: NONE

**ROADSIDE SAFETY SCHEDULE**

SHEET	STATION	TO	STATION	STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS	TRAFFIC BARRIER TERMINAL, TYPE 2	TRAFFIC BARRIER TERMINAL, TYPE 5	TRAFFIC BARRIER TERMINAL, TYPE 6B	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT	CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)	CONCRETE BARRIER TRANSITION	CONCRETE BARRIER BASE	GUARDRAIL MARKERS, TYPE A	BARRIER WALL MARKERS, TYPE C	TERMINAL MARKER - DIRECT APPLIED	CONCRETE BARRIER, SPECIAL, 32" HEIGHT
				FOOT	EACH	EACH	EACH	EACH	FOOT	FOOT	FOOT	FOOT	EACH	EACH	EACH	FOOT
15TH STREET																
PLAN-1	3+00.00	TO	8+50.00	-	-	1	2	2	88	88	10	36	-	9	4	36
PLAN-2	8+50.00	TO	13+92.79	-	-	-	1	-	65	299	-	-	-	2	1	-
RAMP E																
PLAN-3	10+91.65	TO	111+30.00	125.0	-	-	-	1	-	-	-	-	3	-	1	-
PLAN-4	7+50.00	TO	508+60.00	-	-	-	-	-	-	111	-	-	-	-	-	-
PLAN-5	508+60.00	TO	105+15.26	187.5	1	-	-	1	-	-	-	-	4	-	2	-
ST. CLAIR AVE AND RAMP A & RAMP B																
PLAN-6	28+61.00	TO	35+50.00	-	-	-	-	-	-	-	-	-	-	-	-	-
PLAN-7	38+00.00	TO	43+50.00	225.0	1	-	-	-	-	-	-	-	5	-	1	-
PLAN-8	43+50.00	TO	49+00.00	-	-	-	-	1	-	-	-	-	-	-	1	-
RAMP B																
PLAN-9	11+00.00	TO	15+41.39	237.5	-	-	1	1	-	-	-	-	5	-	2	-
<b>TOTALS</b>				<b>775.0</b>	<b>2</b>	<b>1</b>	<b>4</b>	<b>6</b>	<b>153</b>	<b>498</b>	<b>10</b>	<b>36</b>	<b>17</b>	<b>11</b>	<b>12</b>	<b>36</b>

NOTE :

1. ALSO SEE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS FOR ADDITIONAL ROADSIDE SAFETY QUANTITIES.

**FENCE ITEM SCHEDULE**

SHEET	STATION	TO	STATION	CHAIN LINK FENCE, 6'	CHAIN LINK GATES, 6' X
				FOOT	12' DOUBLE
15TH STREET					
PLAN-1	3+00.00	TO	8+50.00	436	-
PLAN-2	8+50.00	TO	13+92.79	313	-
RAMP E					
PLAN-3	10+91.65	TO	111+30.00	624	1
PLAN-4	7+50.00	TO	508+60.00	2	-
PLAN-5	508+60.00	TO	105+15.26	-	-
ST. CLAIR AVE AND RAMP A & RAMP B					
PLAN-6	28+61.00	TO	35+50.00	582	1
PLAN-7	38+00.00	TO	43+50.00	202	-
PLAN-8	43+50.00	TO	49+00.00	-	-
RAMP B					
PLAN-9	11+00.00	TO	15+41.39	39	-

<b>TOTALS</b>	<b>2,198</b>	<b>2</b>
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**CONCRETE CURB AND GUTTER AND MEDIAN SCHEDULE**

SHEET	STATION	TO	STATION	CONCRETE GUTTER, TYPE A	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06	CONCRETE MEDIAN SURFACE, 4 INCH	CONCRETE MEDIAN, TYPE SM-6.12
				FOOT	FOOT	FOOT	SQ FT	SQ FT
15TH STREET								
PLAN-1	3+00.00	TO	8+50.00	43	1,308	-	250	446
PLAN-2	8+50.00	TO	13+92.79	206	1,130	-	-	-
RAMP E								
PLAN-3	10+91.65	TO	111+30.00	-	1,655	-	59	-
PLAN-4	7+50.00	TO	508+60.00	138	1	-	-	-
PLAN-5	508+60.00	TO	105+15.26	-	-	-	-	-
ST. CLAIR AVE AND RAMP A & RAMP B								
PLAN-6	28+61.00	TO	35+50.00	-	959	29	330	-
PLAN-7	38+00.00	TO	43+50.00	-	199	-	-	-
PLAN-8	43+50.00	TO	49+00.00	-	-	-	-	-
RAMP B								
PLAN-9	11+00.00	TO	15+41.39	-	19	-	-	-

<b>TOTALS</b>	<b>387</b>	<b>5,271</b>	<b>29</b>	<b>639</b>	<b>446</b>
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NOTE :  
1. DEPRESSED CURB AND GUTTER THROUGH ENTRANCES SHALL BE MEASURED FOR PAYMENT AS COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24

**SIDEWALK SCHEDULE**

SHEET	STATION	TO	STATION	PORTLAND CEMENT CONCRETE SIDEWALK - 4"	DETECTABLE WARNINGS
				SQ FT	SQ FT
15TH STREET					
PLAN-1	3+00.00	TO	8+50.00	4,142	68
PLAN-2	8+50.00	TO	13+92.79	4,202	70
RAMP E					
PLAN-3	10+91.65	TO	111+30.00	483	-
PLAN-4	7+50.00	TO	508+60.00	-	-
PLAN-5	508+60.00	TO	105+15.26	-	-
ST. CLAIR AVE AND RAMP A & RAMP B					
PLAN-6	28+61.00	TO	35+50.00	1,614	-
PLAN-7	38+00.00	TO	43+50.00	-	-
PLAN-8	43+50.00	TO	49+00.00	-	-
RAMP B					
PLAN-9	11+00.00	TO	15+41.39	-	-

<b>TOTALS</b>	<b>10,441</b>	<b>138</b>
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**PERMANENT SEEDING ITEM (SEEDING AND MULCHING) SCHEDULE**

SHEET	STATION	TO	STATION	SEEDING, CLASS 2A	NITROGEN FERTILIZER NUTRIENT	PHOSPHORUS FERTILIZER NUTRIENT	POTASSIUM FERTILIZER NUTRIENT	MULCH, METHOD 2
				ACRE	POUND	POUND	POUND	ACRE
15TH STREET								
PLAN-1	3+00.00	TO	8+50.00	1.25	113	113	113	1.25
PLAN-2	8+50.00	TO	13+93.34	0.75	68	68	68	0.75
RAMP E								
PLAN-3	10+91.65	TO	111+30.00	0.75	68	68	68	0.75
PLAN-4	7+50.00	TO	508+60.00	0.75	68	68	68	0.75
PLAN-5	508+60.00	TO	106+72.50	0.25	23	23	23	0.25
ST. CLAIR AVE AND RAMP A & RAMP B								
PLAN-6	28+61.00	TO	35+50.00	1.25	113	113	113	1.25
PLAN-7	38+00.00	TO	43+50.00	2.00	180	180	180	2.00
PLAN-8	43+50.00	TO	40+04.59	0.25	23	23	23	0.25
RAMP B								
PLAN-9	11+00.00	TO	18+33.27	2.00	180	180	180	2.00

<b>TOTALS</b>	<b>9.25</b>	<b>836</b>	<b>836</b>	<b>836</b>	<b>9.25</b>
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NOTE :  
1. ALSO SEE LANDSCAPING PLANS FOR ADDITIONAL SEEDING QUANTITIES.

**EXISTING STRUCTURE SCHEDULE**

STATION	OFFSET	MAN ADJ NEW T1F CL	MAN RECON NEW T1F CL	INLETS ADJ NEW T24F&G	REMOVE MANHOLES	REMOVE INLETS	COMMENTS
		EACH	EACH	EACH	EACH	EACH	
<b>15TH STREET</b>							
4+31.32	63.98' LT					1	
4+43.41	25.41' RT					1	
4+63.31	4.74' RT	1					
5+34.34	29.49' LT					1	
5+35.14	28.56' RT					1	
5+85.97	1.92' RT		1				S 1-09
6+14.46	22.11' LT					1	
6+53.01	1.72' RT				1		
9+79.73	1.45' RT	1					
10+12.22	24.43' RT					1	
13+30.83	35.17' LT	1					
<b>BAUGH AVENUE</b>							
5+49.57	11.23' RT		1				S 2-10
5+64.85	69.54' RT					1	
506+54.14	69.26' LT	1					
110+24.40	33.23' LT					1	
112+31.40	28.06' RT	1					
112+38.71	24.94' RT					1	
<b>ST. CLAIR AVENUE</b>							
28+76.22	1.32' RT	1					
29+13.40	20.93' RT					1	
29+17.28	36.77' RT					1	
29+49.89	20.58' RT	1					
29+70.75	42.27' RT					1	
31+45.07	13.20' LT		1				S 6-16
31+58.19	22.69' RT					1	
32+59.79	30.11' LT					1	
32+77.50	26.08' RT					1	
32+77.98	0.00' RT	1					
32+84.24	15.09' LT	1					
32+96.86	23.07' LT		1				S 6-08
35+00.48	36.51' RT					1	
35+44.79	39.01' RT					1	
35+49.81	38.78' LT					1	
36+68.80	20.68' RT	1					
36+87.05	10.90' RT					1	
37+77.15	24.17' LT	1					
37+97.79	25.84' LT			1			
<b>EB I-64</b>							
39+05.64	80.42' RT	1					
42+10.68	125.60' RT					1	
<b>WB I-64</b>							
99+23.43	58.58' RT			1			
100+77.52	48.16' RT	1					
101+34.71	45.75' RT			1			
<b>RAMP A</b>							
9+93.89	4.96' RT	1					
12+40.60	17.53' LT	1					
<b>RAMP B</b>							
12+75.38	122.50' RT					1	
12+82.79	14.85' RT					1	
13+09.74	52.46' RT				1		
14+06.04	43.06' RT					1	
14+40.58	21.86' LT			1			
<b>RAMP E</b>							
11+84.26	7.92' LT	1					
15+33.45	29.24' LT					1	
15+94.60	25.37' LT					1	
18+28.97	12.40' LT			1			
21+21.72	9.69' LT			1			
<b>TOTAL</b>		<b>16</b>	<b>4</b>	<b>6</b>	<b>2</b>	<b>25</b>	

**EXISTING PIPE REMOVAL SCHEDULE**

FROM STATION	TO STATION	STORM SEWER REMOVAL 12"	STORM SEWER REMOVAL 18"	STORM SEWER REMOVAL 24"	CLASS C PATCH TYPE II, 10"	CLASS C PATCH TYPE III, 10"
		FOOT	FOOT	FOOT	SQ YD	SQ YD
<b>15TH STREET</b>						
4+31.32	4+63.31	75				
4+43.41	4+63.31	29				
5+07.57	5+35.14	63				
5+07.57	5+34.34	26				
5+85.97	6+53.01	67				
6+00.56	6+14.46	28				
9+79.73	10+12.22	40				
<b>BAUGH AVENUE</b>						
5+49.57	5+64.85	61				
112+31.40	112+38.71	8				
112+38.71	112+54.77	32				
<b>ST. CLAIR AVENUE</b>						
29+17.28	29+49.89	32				
29+26.07	29+49.89		90			
29+49.89	29+70.99			29		
32+57.25	32+83.53	31			10	
32+77.50	32+77.98	26				16
36+68.80	36+87.05	21				
<b>EB I-64</b>						
42+02.87	42+10.68		50			
43+32.90	44+05.69	77				
<b>RAMP A</b>						
11+81.66	12+40.60	64				
12+40.60	13+74.08	134				
20+82.95	21+18.60	40				
22+93.55	24+00.75	117				
<b>RAMP B</b>						
12+75.51	13+09.43		73			
12+82.79	13+09.43	45				
13+09.43	14+05.04	90				
<b>RAMP E</b>						
11+32.37	11+44.58			23		
18+20.00	18+29.00	33				
18+24.72	18+29.00	28				
<b>ALLOWANCE (NOTE 1)</b>						
<b>TOTAL</b>		<b>1167</b>	<b>213</b>	<b>52</b>	<b>90</b>	<b>4</b>

**UNDERDRAIN SCHEDULE**

STATION OF UNDERDRAIN PIPE LIMIT	UNDERDRAIN PIPE		OUTLET PIPE			CONCRETE HEADWALL FOR PIPE DRAINS EACH
	PIPE UNDERDRAINS 4"	FLOW LINE ELEVATION AT UNDERDRAIN PIPE LIMIT FOOT	PIPE UNDERDRAINS 4" (SPECIAL) FOOT	CONNECT OUTLET PIPE TO STRUCT. NO. ___	OUTLET ELEVATION	
<b>EB I-64</b>						
45+47.37	98	391.18				
46+45.33		390.73				
46+53.02			13	EX STRUCT	390.67	
46+53.02			15	EX STRUCT	390.79	
46+63.05		390.86				
48+36.74	174	392.56				
55+81.34	68	415.80				
56+48.80		415.32				
56+55.05			10	EX STRUCT	415.25	
56+55.05			10	EX STRUCT	417.55	
56+61.94	90	417.83				
57+52.23		420.42				
<b>WB I-64</b>						
87+44.74	691	413.78				
94+40.00	197	392.74				
96+36.98		390.46				
96+43.16			9	EX STRUCT	390.40	
99+22.80	592	392.40				
105+15.26		394.77				
<b>RAMP A</b>						
7+71.49	455	392.91				
12+26.07		390.37				
12+24.20			26		390.22	1
12+41.98			27		390.21	1
12+43.79	90	390.32				
13+34.18		390.68				
13+36.84			4	S 1-16	390.95	
13+40.03	60	391.01				
14+00.00	106	391.88				
14+90.59		394.85				
15+05.42			15	S 1-22	394.29	
15+06.44		394.13				
15+10.00			4	S 1-14	394.14	
15+13.53	311	394.28				
18+37.76	87	406.48				
18+89.00		408.61				
<b>RAMP B</b>						
10+39.66	71	409.63				
10+93.24		410.15				
10+93.24	46	409.03				
11+40.00	402	409.36				
15+41.39		420.38				
<b>RAMP E</b>						
11+95.08	128	411.59				
13+25.00	306	410.21				
16+31.13		400.39				
16+54.32			27		400.28	1
16+42.69	103	400.00				
17+46.06		397.19				
17+45.14			19		397.11	1
17+52.07	95	396.00				
18+46.83		394.02				
18+50.00			4	S 2-17	393.90	
18+53.38	75	393.82				
19+26.38	85	392.15				
19+28.94		394.25				
19+77.42			49	S 2-21	390.86	
20+13.73		390.64				
20+16.32			4	S 2-18	390.57	
20+19.71	237	390.53				
22+57.08		389.59				
22+59.99			4	S 4-04	389.51	
22+59.67			4	S 4-04	390.50	
21+62.88	128	390.81				
24+01.28		392.40				
<b>TOTAL</b>		<b>4695</b>	<b>244</b>			<b>4</b>

NOTES:  
1. PAYMENT FOR INCIDENTAL PAVEMENT PATCHING RELATED TO DRAINAGE ITEMS SHALL BE COVERED UNDER PAVEMENT PATCHING ALLOWANCE OF THE CLASS AND TYPE SPECIFIED.









**STORM SEWER SCHEDULE**

PIPE NO	UPSTREAM STRUCTURE NO	UPSTREAM INVERT ELEVATION	DOWN STREAM STRUCTURE NO	DOWN STREAM INVERT ELEVATION	PIPE SLOPE (%)	CLASS C PATCH TYPE I, 10"	CLASS C PATCH TYPE III, 10"	TRENCH BACKFILL	CONCRETE COLLAR	STORM SEWERS, CLASS A																STORM SEWERS JACKED IN PLACE, 24"	STORM SEWERS JACKED IN PLACE, 30"	HIGH DENSITY POLYETH YLENE PIPE, 12"	STORM SEWERS, TYPE 2, WATER MAIN QUALITY PIPE, 12"	STORM SEWERS, TYPE 2, WATER MAIN QUALITY PIPE, 15"
										TYPE 1					TYPE 2															
										12" DIA	15" DIA	18" DIA	24" DIA	27" DIA	12" DIA	15" DIA	18" DIA	24" DIA	30" DIA	36" DIA										
SQ YD	SQ YD	CU YD	CU YD	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT											
<b>RAMP A</b>																														
P 1-13A	WELL NO. 19	393.88	S 1-13	390.97	5.03																						58			
P 1-13	S 1-13	388.59	S 1-14	388.50	0.17			10.6																						
P 1-14	S 1-14	388.50	S 1-16	388.22	0.17			70.5																						
P 1-15	S 1-15	388.68	S 1-14	388.50	0.55			17.8																						
P 1-16	S 1-16	388.22	S 1-21	388.17	0.17			5.2																						
P 1-16A	WELL NO. 18	388.52	S 1-16	388.32	0.44			0.0																						
P 1-21	S 1-21	388.17	S 1-17	388.04	0.17			0.0																						
P 1-22	S 1-22	389.08	S 1-14	388.89	1.00			7.8																						
P 7-05	S 7-05	389.96	EXISTING	388.48	3.00			4.4																						
P 7-06	S 7-06	388.64	EXISTING	388.54	0.33			1.1																						
<b>RAMP B</b>																														
P 6-11	S 6-15	408.07	S 6-12	404.90	5.88			2.5																						
P 6-12	S 6-12	404.90	S 6-13	404.52	0.26			24.3																						
P 6-15	S 6-11	408.48	S 6-15	408.07	0.88			2.2																						
<b>RAMP E</b>																														
P 1-17	S 2-18	388.13	S 1-17	388.04	0.13																									
P 2-15	S 2-15	388.76	S 2-17	388.67	0.26			6.4																						
P 2-15A	WELL NO. 10	394.45	S 2-15	392.08	4.30			0.0																						
P 2-16	S 2-16	388.35	S 2-24	388.28	0.50			4.3																						
P 2-16A	WELL NO. 9	391.31	S 2-16	389.35	4.78			0.0																						
P 2-17	S 2-17	388.67	S 2-22	388.32	0.27			50.0																						
P 2-21	S 2-21	388.86	S 2-18	388.23	1.50			1.5																						
P 2-22	S 2-22	388.32	S 2-18	388.23	0.24			8.6																						
P 2-23	S 2-23	388.30	S 2-24	388.28	0.15			0.7																						
P 2-24	S 2-24	388.28	S 2-25	388.25	0.15			0.7																						
P 2-25	S 2-25	388.25	S 2-18	388.13	0.15			10.2																						
P 3-06	S 3-06	400.55	S 3-14	400.19	0.49			0.5																						
P 3-07	S 3-07	404.50	S 3-13	402.82	2.00			4.2																						
P 3-08	S 3-08	409.64	S 3-07	406.50	2.01			14.1																						
P 3-09	S 3-09	409.69	S 3-08	409.64	0.50			1.5																						
P 3-10	S 3-10	409.85	S 3-09	409.69	0.50			2.5																						
P 3-11	S 3-11	406.53	S 3-07	406.50	0.50			0.0																						
P 3-12	S 3-12	401.73	S 3-13	401.55	0.50			7.3																						
P 3-13	S 3-13	400.85	S 3-06	400.55	0.49			12.1																						
P 3-15	S 3-15	397.33	S 3-16	397.11	0.50			0.0																						
P 4-01	S 4-01	388.66	S 4-03	388.55	0.32			1.0																						
P 4-03	S 4-03	388.55	S 4-04	388.49	0.32			1.6																						
P 4-04	S 4-04	388.49	S 2-23	388.30	0.15			11.7																						
P 4-04A	WELL NO. 8	396.17	S 4-05	395.47	4.37			0.0																						
P 4-05	S 4-05	389.47	S 4-04	388.82	1.00			2.9																						
ALLOWANCE (NOTE 3)						91	12																							
TOTAL						100	80	535	10	178	129	74	10	243	947	318	216	510	20	20	79	69	215	82	75					

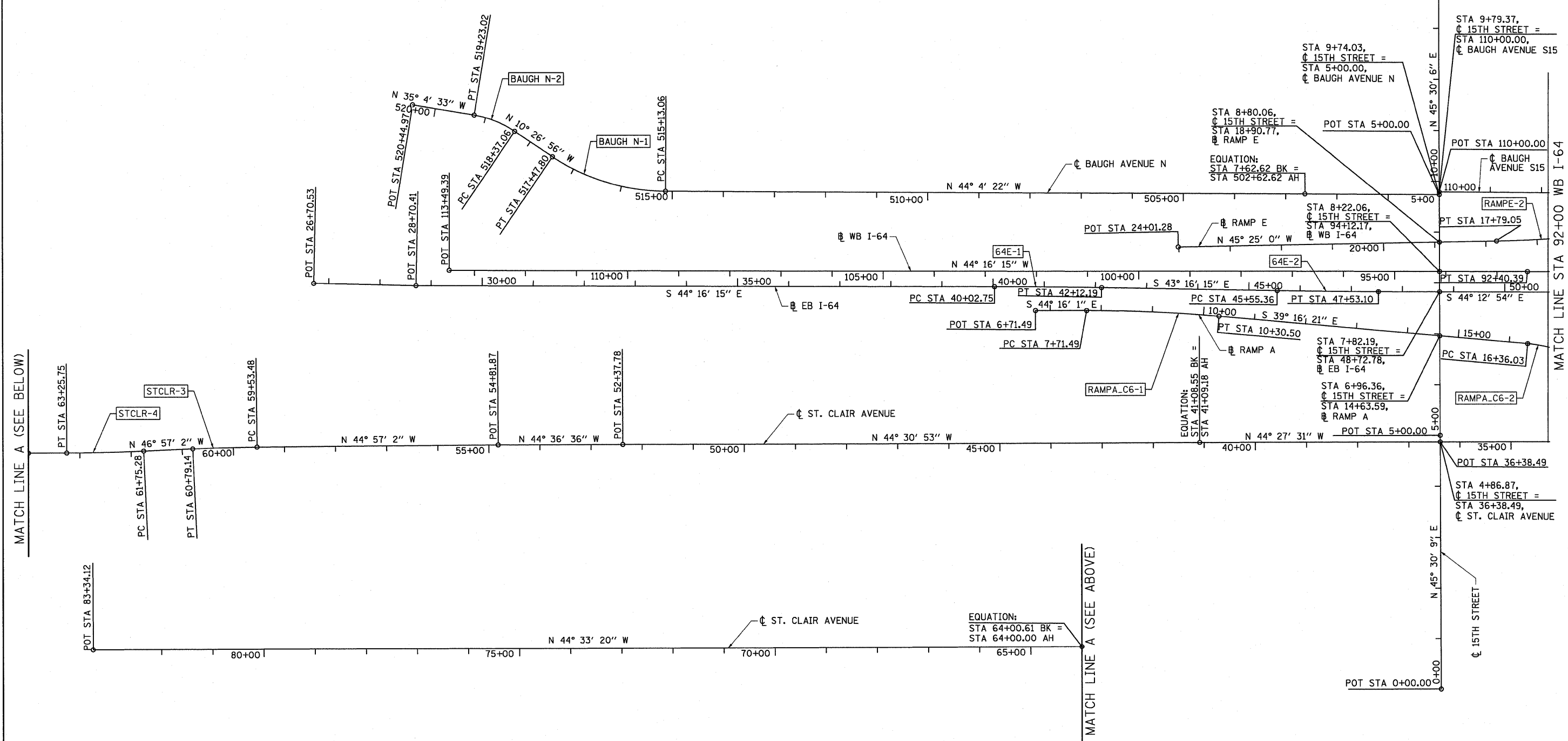
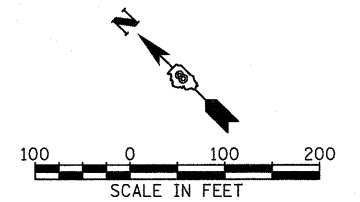
**DITCH CHECK SCHEDULE**

UPSTREAM DITCH CHECK LIMIT	DOWNSTREAM DITCH CHECK LIMIT	DOWN STREAM STRUCTURE NO	AGGREGATE DITCH CHECKS	PIPE DRAIN 5"	PIPE DRAIN 6"	PIPE DRAIN 8"
<b>WB I-64</b>						
98+78.73	98+36.75	S 4-01	43		42	
101+17.03	100+75.04	S 4-02	43		42	
<b>RAMP A</b>						
7+41.31	7+85.45	S 7-07	43		44	
9+53.97	9+97.43	S 7-06	43		43	
11+84.72	12+27.88	S 7-05	43		43	
<b>RAMP B</b>						
11+47.06 R	10+98.64 R	S 6-11	43			42
11+32.47 L	10+97.38 L	S 6-12	43	42		
TOTAL			301	42	214	42

PIPE DRAIN FLOWLINE ELEVATIONS AND PIPE SLOPES TO MATCH FINISHED GRADE OF ASSOCIATED DITCH

- NOTES:
1. TIE-INS TO EXISTING STRUCTURES AND PIPES ARE INCLUDED IN THE COST OF STORM SEWER CONSTRUCTION.
  2. PIPE SLOPES SHOWN ARE CALCULATED USING STRUCTURE INVERTS AND LENGTH OF PIPE. PIPE SLOPES MAY BE HIGHER FOR STRUCTURES USING HALF TRAPS.
  3. PAYMENT FOR INCIDENTAL PAVEMENT PATCHING RELATED TO DRAINAGE ITEMS SHALL BE COVERED UNDER PAVEMENT PATCHING ALLOWANCE OF THE CLASS AND TYPE SPECIFIED.

PR CURVE RAMPA_C6-1 PI STA. = 9+01.08 Δ = 4° 59' 40" (RT) D = 1° 55' 42" R = 2,971.21' T = 129.58' L = 259.00' E = 2.82' e = 5.0% T.R. REMOVAL = 0' S.E. RUN. REMOVAL = 42' PC STA = 7+71.49 PT STA = 10+30.50	PR CURVE RAMPA_C6-2 PI STA. = 18+21.27 Δ = 67° 55' 49" (RT) D = 20° 50' 05" R = 275.00' T = 185.25' L = 326.04' E = 56.57' e = 7.9% T.R. ATTAINMENT = 0' S.E. RUN. ATTAINMENT = 143' T.R. REMOVAL = 0' S.E. RUN. REMOVAL = 106' PC STA = 16+36.03 PT STA = 19+62.07	PR CURVE BAUGH_N-1 PI STA. = 516+33.92 Δ = 33° 37' 26" (RT) D = 14° 19' 26" R = 400.00' T = 120.86' L = 234.74' E = 17.86' PC STA = 515+13.06 PT STA = 517+47.80	PR CURVE BAUGH_N-2 PI STA. = 518+80.72 Δ = 24° 37' 37" (LT) D = 28° 38' 52" R = 200.00' T = 43.66' L = 85.96' E = 4.71' PC STA = 518+37.06 PT STA = 519+23.02	PR CURVE ST_CLR-3 PI STA. = 60+16.32 Δ = 2° 00' 00" (LT) D = 1° 35' 30" R = 3,600.00' T = 62.84' L = 125.66' E = 0.55' PC STA = 59+53.48 PT STA = 60+79.14	PR CURVE ST_CLR-4 PI STA. = 62+50.52 Δ = 2° 23' 42" (RT) D = 1° 35' 30" R = 3,600.00' T = 75.25' L = 150.47' E = 0.79' PC STA = 61+75.28 PT STA = 63+25.75	PR CURVE 64E-1 PI STA. = 41+07.48 Δ = 1° 00' 00" (RT) D = 0° 28' 39" R = 12,000.00' T = 104.72' L = 209.44' E = 0.46' PC STA = 40+02.75 PT STA = 42+12.19	PR CURVE 64E-2 PI STA. = 46+54.23 Δ = 0° 56' 39" (LT) D = 0° 28' 39" R = 12,000.00' T = 98.87' L = 197.74' E = 0.41' PC STA = 45+55.36 PT STA = 47+53.10	PR CURVE RAMPE-2 PI STA. = 17+13.80 Δ = 3° 44' 23" (RT) D = 2° 51' 53" R = 2,000.00' T = 65.29' L = 130.54' E = 1.07' e = 5.1% T.R. ATTAINMENT = 0' S.E. RUN. ATTAINMENT = 115' T.R. REMOVALS = 0' S.E. RUN. REMOVALS = 99' PC STA = 16+48.51 PT STA = 17+79.05
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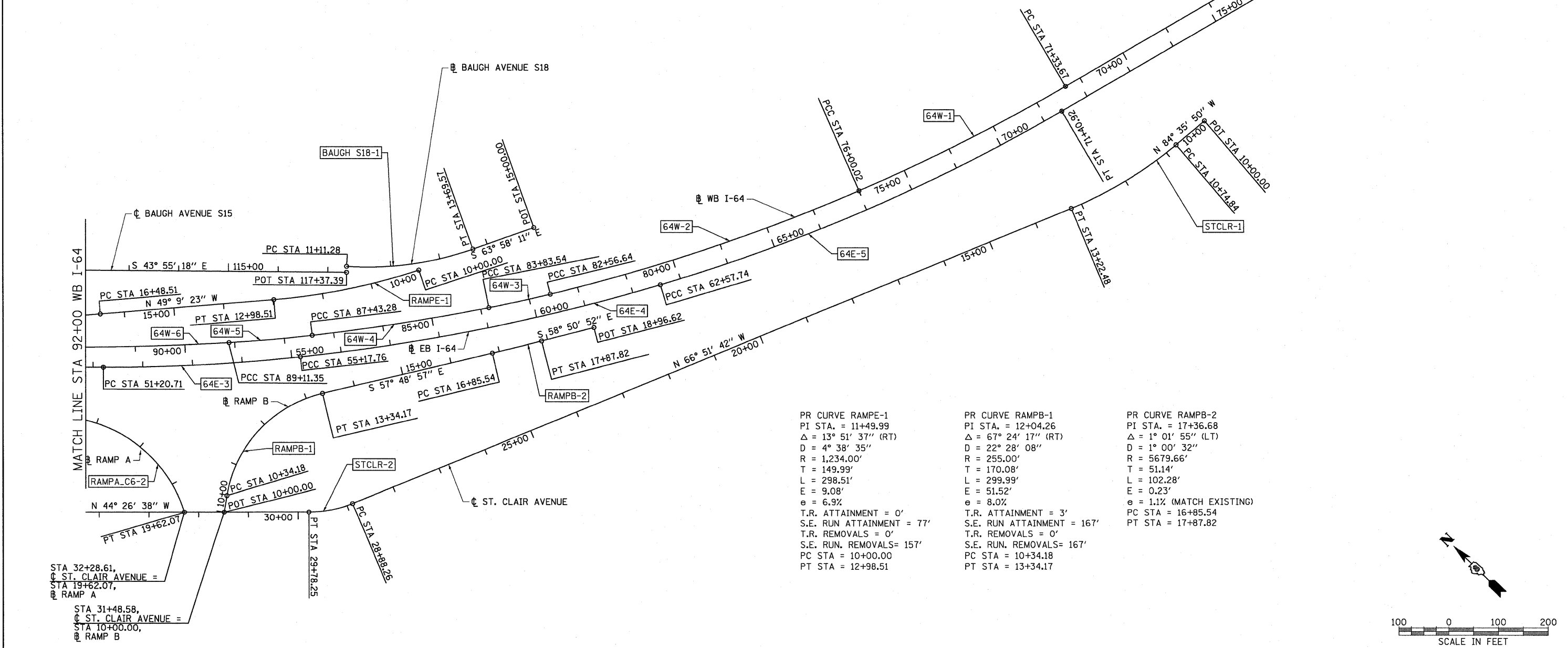
MATCH LINE A (SEE BELOW)

MATCH LINE A (SEE ABOVE)

MATCH LINE STA 92+00 WB I-64

FILE NAME = #FILEA#	USER NAME = IDOT	DESIGNED - TTB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ALIGNMENT, TIES AND BENCHMARKS</b>		F.A.I. RTE. 64	SECTION 82-1-2HB	COUNTY ST. CLAIR	TOTAL SHEETS 345	SHEET NO. 37
	PLOT SCALE = 100.0000' / IN.	DRAWN - TTB	REVISED -				FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
	PLOT DATE = 3/17/2010	CHECKED - JAH	REVISED -				CONTRACT NO. 76C49				
	DATE = 3/19/2010	REVISED -		SCALE: 1"=100'	SHEET NO. 1 OF 8 SHEETS	STA. TO STA.					

PR CURVE ST_CLR-1 PI STA. = 11+99.66 Δ = 17° 44' 08" (RT) D = 7° 09' 43" R = 800.00' T = 124.82' L = 247.64' E = 9.68' PC STA = 10+74.84 PT STA = 13+22.48	PR CURVE ST_CLR-2 PI STA. = 29+33.84 Δ = 22° 25' 05" (RT) D = 24° 54' 40" R = 230.00' T = 45.58' L = 89.99' E = 4.47' PC STA = 28+88.26 PT STA = 29+78.25	PR CURVE 64E-3 PI STA. = 53+19.44 Δ = 6° 19' 10" (LT) D = 1° 35' 30" R = 3,600.00' T = 198.73' L = 397.06' E = 5.48' PC STA = 51+20.71 PCC STA = 55+17.76	PR CURVE 64E-4 PI STA. = 58+88.81 Δ = 10° 35' 58" (LT) D = 1° 25' 57" R = 4,000.00' T = 371.05' L = 739.97' E = 17.17' PCC STA = 55+17.76 PCC STA = 62+57.74	PR CURVE 64E-5 PI STA. = 67+01.39 Δ = 13° 31' 16" (LT) D = 1° 31' 51" R = 3,742.50' T = 443.65' L = 883.18' E = 26.20' PCC STA = 62+57.74 PT STA = 71+40.92	PR CURVE BAUGH_S18-1 PI STA. = 12+42.25 Δ = 23° 23' 50" (LT) D = 9° 03' 31" R = 632.49' T = 130.97' L = 258.29' E = 13.42' PC STA = 11+11.28 PT STA = 13+69.57	PR CURVE 64W-1 PI STA. = 73+67.12 Δ = 6° 45' 48" (RT) D = 1° 27' 01" R = 3,950.66' T = 233.44' L = 466.34' E = 6.89' PC STA = 71+33.67 PCC STA = 76+00.02	PR CURVE 64W-2 PI STA. = 79+29.11 Δ = 9° 39' 54" (RT) D = 1° 28' 19" R = 3,892.58' T = 329.09' L = 656.62' E = 13.89' PCC STA = 76+00.02 PCC STA = 82+56.64	PR CURVE 64W-3 PI STA. = 83+20.10 Δ = 2° 30' 26" (RT) D = 1° 58' 33" R = 2,900.00' T = 63.46' L = 126.91' E = 0.69' PCC STA = 82+56.64 PCC STA = 83+83.54			
PR CURVE 64W-4 PI STA. = 85+63.51 Δ = 4° 34' 49" (RT) D = 1° 16' 24" R = 4,500.00' T = 179.97' L = 359.74' E = 3.60' PCC STA = 83+83.54 PCC STA = 87+43.28	PR CURVE 64W-5 PI STA. = 88+27.34 Δ = 3° 30' 05" (RT) D = 2° 05' 01" R = 2,750.00' T = 84.06' L = 168.06' E = 1.28' PCC STA = 87+43.28 PCC STA = 89+11.35	PR CURVE 64W-6 PI STA. = 90+75.92 Δ = 3° 21' 59" (RT) D = 1° 01' 23" R = 5,600.16' T = 164.57' L = 329.05' E = 2.42' PCC STA = 89+11.35 PT STA = 92+40.39							PR CURVE RAMPE-1 PI STA. = 11+49.99 Δ = 13° 51' 37" (RT) D = 4° 38' 35" R = 1,234.00' T = 149.99' L = 298.51' E = 9.08' e = 6.9% T.R. ATTAINMENT = 0' S.E. RUN ATTAINMENT = 77' T.R. REMOVALS = 0' S.E. RUN. REMOVALS = 157' PC STA = 10+00.00 PT STA = 12+98.51	PR CURVE RAMPB-1 PI STA. = 12+04.26 Δ = 67° 24' 17" (RT) D = 22° 28' 08" R = 255.00' T = 170.08' L = 299.99' E = 51.52' e = 8.0% T.R. ATTAINMENT = 3' S.E. RUN ATTAINMENT = 167' T.R. REMOVALS = 0' S.E. RUN. REMOVALS = 167' PC STA = 10+34.18 PT STA = 13+34.17	PR CURVE RAMPB-2 PI STA. = 17+36.68 Δ = 1° 01' 55" (LT) D = 1° 00' 32" R = 5679.66' T = 51.14' L = 102.28' E = 0.23' e = 1.1% (MATCH EXISTING) PC STA = 16+85.54 PT STA = 17+87.82



FILE NAME =	USER NAME = IDOT	DESIGNED - TTB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ALIGNMENT, TIES AND BENCHMARKS</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*FILE#	PLOT SCALE = 100.0000' / IN.	DRAWN - TTB	REVISED -					64	82-1-2HB	ST. CLAIR	345	38
	PLOT DATE = 3/19/2010	CHECKED - JAH	REVISED -		SCALE: 1"=100'			SHEET NO. 2 OF 8 SHEETS		CONTRACT NO. 76C49		
		DATE = 3/19/2010	REVISED -		STA. TO STA.			FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

COORDINATE TABLES

15TH STREET				
POINT	CURVE	STATION	NORTHING	EASTING
POT		0+00.00	14,039,109.38	2,457,242.23
POT		5+00.00	14,039,459.82	2,457,598.87
POT		15+24.82	14,040,178.10	2,458,329.84

ST. CLAIR AVENUE				
POINT	CURVE	STATION	NORTHING	EASTING
POT		10+00.00	14,038,245.68	2,459,879.01
PC	ST_CLR-1	10+74.84	14,038,252.73	2,459,804.50
PI	ST_CLR-1	11+99.66	14,038,264.48	2,459,680.24
PT	ST_CLR-1	13+22.48	14,038,313.53	2,459,565.46
PC	ST_CLR-2	28+88.26	14,038,928.80	2,458,125.63
PI	ST_CLR-2	29+33.84	14,038,946.71	2,458,083.72
PT	ST_CLR-2	29+78.25	14,038,979.25	2,458,051.81
EQN	41+08.55=BK	41+09.18=AH	14,039,786.13	2,457,260.27
POT		52+37.78	14,040,590.90	2,456,469.02
POT		54+81.87	14,040,764.67	2,456,297.60
PC	ST_CLR-3	59+53.48	14,041,098.44	2,455,964.41
PI	ST_CLR-3	60+16.32	14,041,142.91	2,455,920.02
PT	ST_CLR-3	60+79.14	14,041,185.80	2,455,874.10
PC	ST_CLR-4	61+75.28	14,041,251.43	2,455,803.85
PI	ST_CLR-4	62+50.52	14,041,302.79	2,455,748.86
PT	ST_CLR-4	63+25.75	14,041,356.41	2,455,696.06
EQN	64+00.61=BK	64+00.00=AH	14,041,409.75	2,455,643.54
POT		83+34.12	14,042,787.95	2,454,286.56

BAUGH AVENUE N				
POINT	CURVE	STATION	NORTHING	EASTING
POT		5+00.00	14,039,792.06	2,457,936.98
POT		502+62.62	14,039,980.74	2,457,754.31
PC	BAUGH_N-1	515+13.06	14,040,879.13	2,456,884.54
PI	BAUGH_N-1	516+33.92	14,040,965.96	2,456,800.47
PT	BAUGH_N-1	517+47.80	14,041,084.81	2,456,778.56
PC	BAUGH_N-2	518+37.06	14,041,172.59	2,456,762.37
PI	BAUGH_N-2	518+80.72	14,041,215.53	2,456,754.45
PT	BAUGH_N-2	519+23.02	14,041,251.25	2,456,729.36
POT		520+44.97	14,041,351.05	2,456,659.29

BAUGH AVENUE S15				
POINT	CURVE	STATION	NORTHING	EASTING
POT		110+00.00	14,039,795.80	2,457,940.79
POT		117+37.39	14,039,264.67	2,458,452.30

BAUGH AVENUE S18				
POINT	CURVE	STATION	NORTHING	EASTING
PC	BAUGH_S18-1	11+11.28	14,039,273.11	2,458,461.07
PI	BAUGH_S18-1	12+42.25	14,039,173.63	2,458,546.25
PT	BAUGH_S18-1	13+69.57	14,039,116.16	2,458,663.93
POT		15+00.00	14,039,058.92	2,458,781.13

RAMP E				
POINT	CURVE	STATION	NORTHING	EASTING
PC	RAMPE-1	10+00.00	14,039,164.20	2,458,557.32
PI	RAMPE-1	11+49.99	14,039,232.26	2,458,423.66
PT	RAMPE-1	12+98.51	14,039,330.35	2,458,310.20
PC	RAMPE-2	16+48.51	14,039,559.24	2,458,045.43
PI	RAMPE-2	17+13.80	14,039,601.94	2,457,996.03
PT	RAMPE-2	17+79.05	14,039,647.78	2,457,949.53
POT		24+01.28	14,040,084.55	2,457,506.36

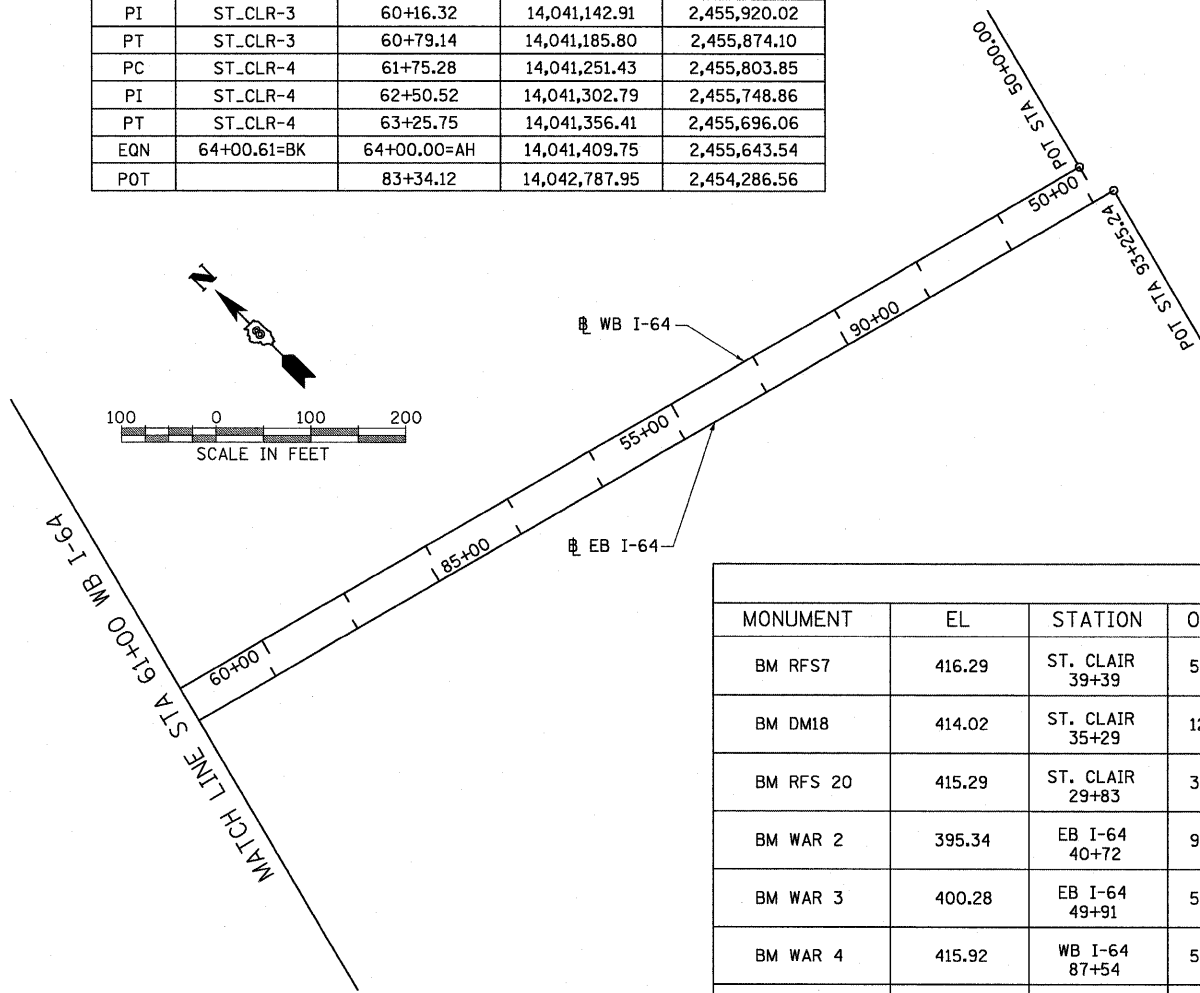
EB I-64				
POINT	CURVE	STATION	NORTHING	EASTING
POT		26+70.53	14,041,244.03	2,456,272.79
POT		28+70.41	14,041,098.15	2,456,409.42
PC	64E-1	40+02.75	14,040,287.34	2,457,199.86
PI	64E-1	41+07.48	14,040,212.35	2,457,272.96
PT	64E-1	42+12.19	14,040,136.10	2,457,344.74
PC	64E-2	45+55.36	14,039,886.23	2,457,579.96
PI	64E-2	46+54.23	14,039,814.24	2,457,647.73
PT	64E-2	47+53.10	14,039,743.38	2,457,716.68
PC	64E-3	51+20.71	14,039,479.90	2,457,973.04
PI	64E-3	53+19.44	14,039,411.53	2,458,039.56
PCC	64E-4	55+17.76	14,039,211.15	2,458,265.04
PI	64E-4	58+88.81	14,038,975.31	2,458,551.49
PCC	64E-5	62+57.74	14,038,796.18	2,458,876.44
PI	64E-5	67+01.39	14,038,582.00	2,459,264.96
PT	64E-5	71+40.92	14,038,464.60	2,459,692.80
POT		93+25.24	14,037,886.56	2,461,799.25

WB I-64				
POINT	CURVE	STATION	NORTHING	EASTING
POT		50+00.00	14,038,555.99	2,459,508.71
PC	64W-1	71+33.67	14,038,494.21	2,459,733.83
PI	64W-1	73+67.12	14,038,555.99	2,459,508.71
PCC	64W-2	76+00.02	14,038,643.84	2,459,292.43
PI	64W-2	79+29.11	14,038,767.70	2,458,987.53
PCC	64W-3	82+56.64	14,038,940.99	2,458,707.76
PI	64W-3	83+20.10	14,038,974.40	2,458,653.81
PCC	64W-4	83+83.54	14,039,010.15	2,458,601.37
PI	64W-4	85+63.51	14,039,111.52	2,458,452.67
PCC	64W-5	87+43.28	14,039,224.43	2,458,312.53
PI	64W-5	88+27.34	14,039,277.17	2,458,247.08
PCC	64W-6	89+11.35	14,039,333.81	2,458,184.97
PI	64W-6	90+75.92	14,039,444.70	2,458,063.37
PT	64W-6	92+40.39	14,039,562.54	2,457,948.49
POT		113+49.39	14,041,072.69	2,456,476.31

RAMP A				
POINT	CURVE	STATION	NORTHING	EASTING
POT		6+71.49	14,040,197.44	2,457,222.50
PC	RAMPA_C6-1	7+71.49	14,040,125.83	2,457,292.30
PI	RAMPA_C6-1	9+01.08	14,040,033.04	2,457,382.75
PT	RAMPA_C6-1	10+30.50	14,039,932.72	2,457,464.77
PC	RAMPA_C6-2	16+36.03	14,039,463.95	2,457,848.08
PI	RAMPA_C6-2	18+21.27	14,039,320.55	2,457,965.34
PT	RAMPA_C6-2	19+62.07	14,039,157.99	2,457,876.50

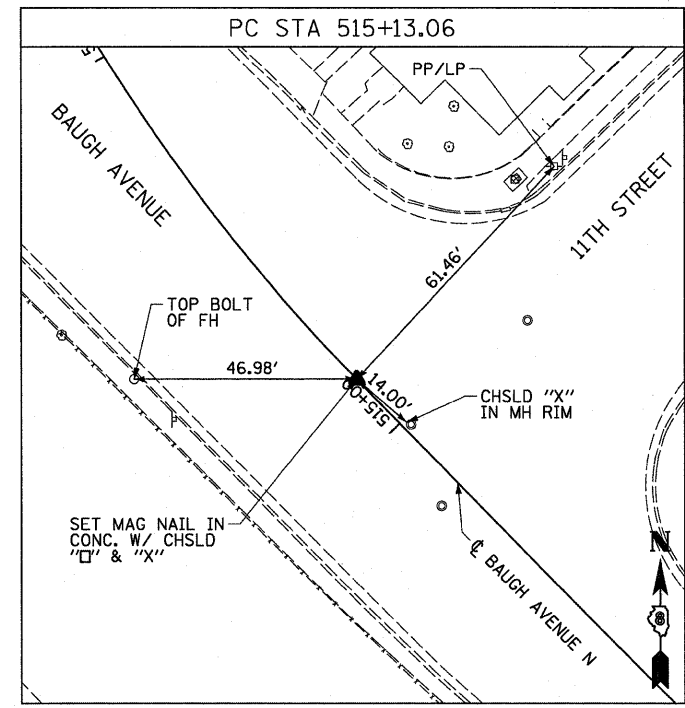
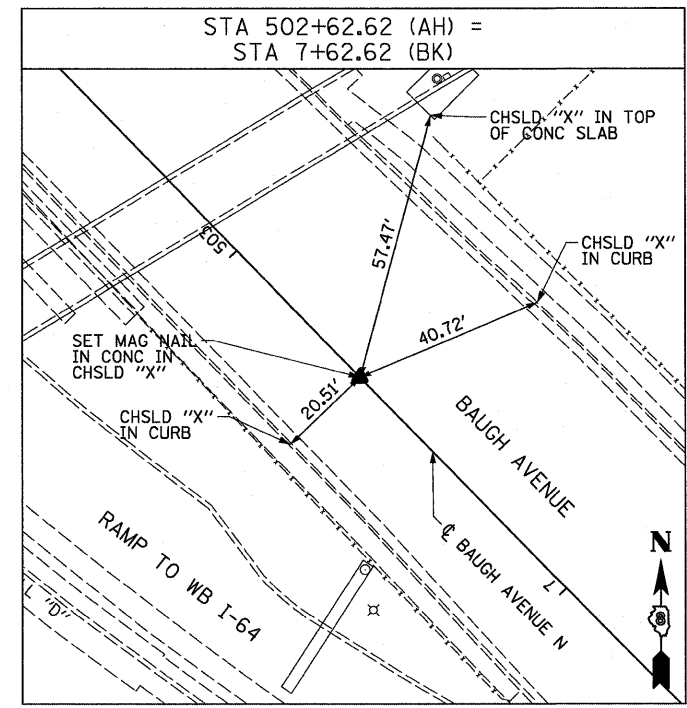
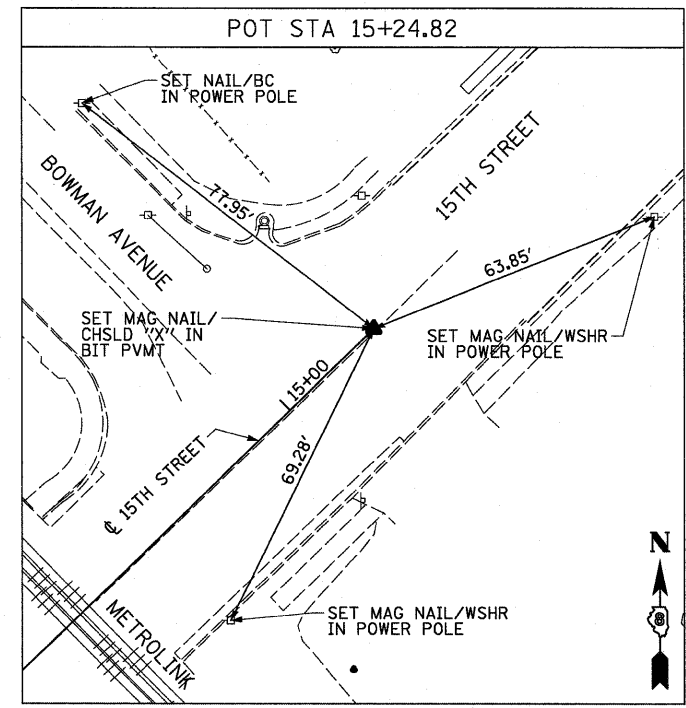
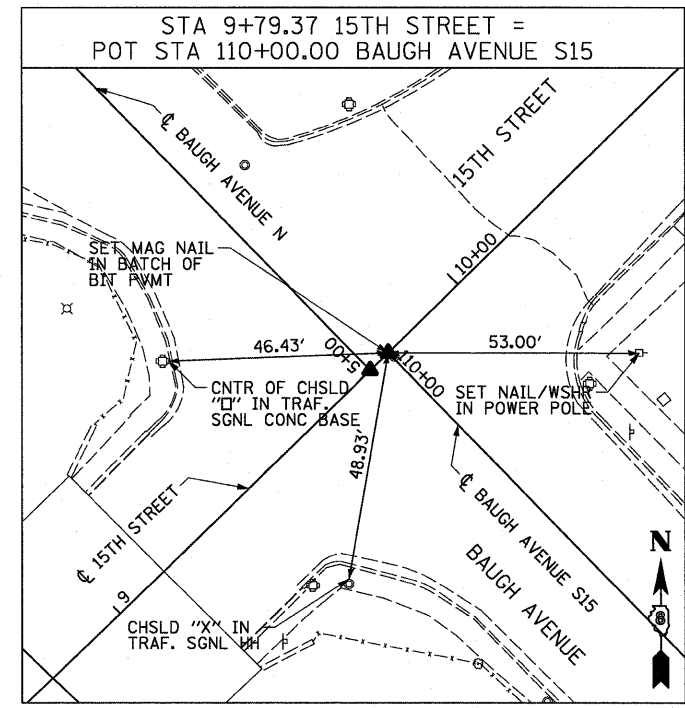
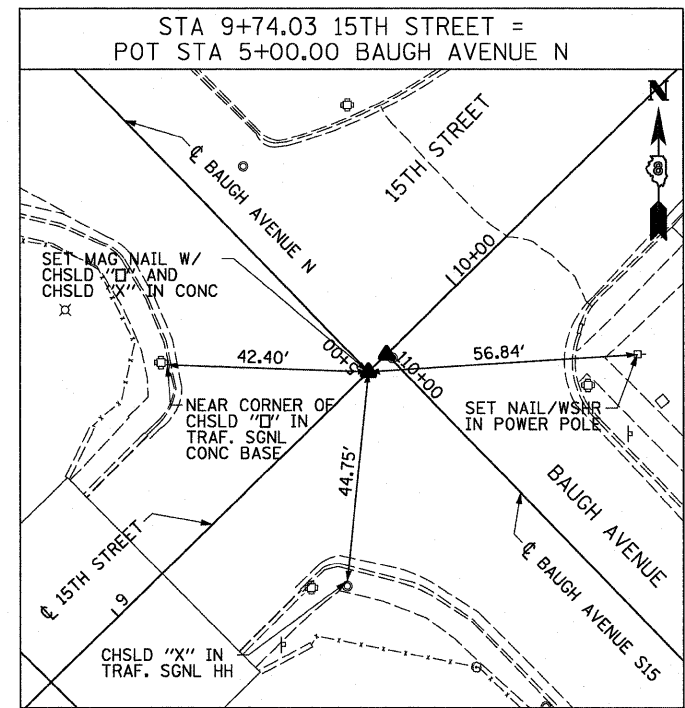
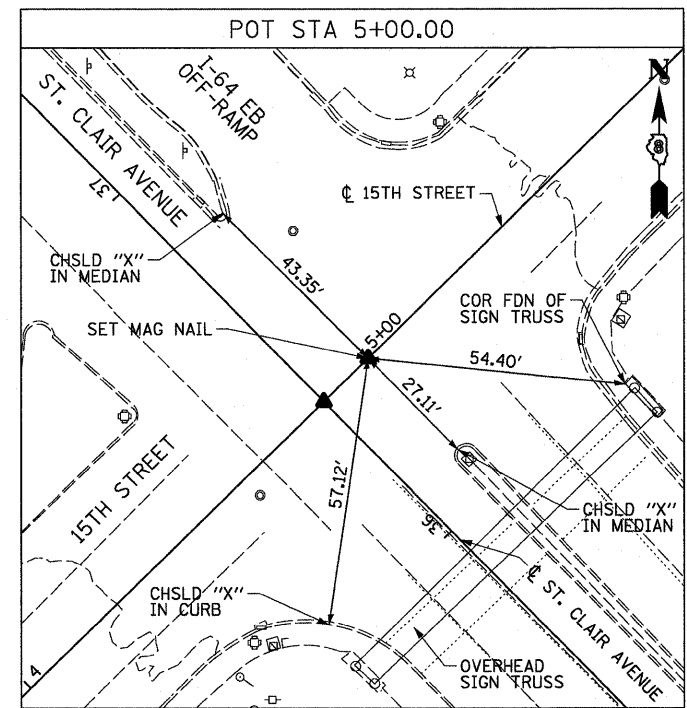
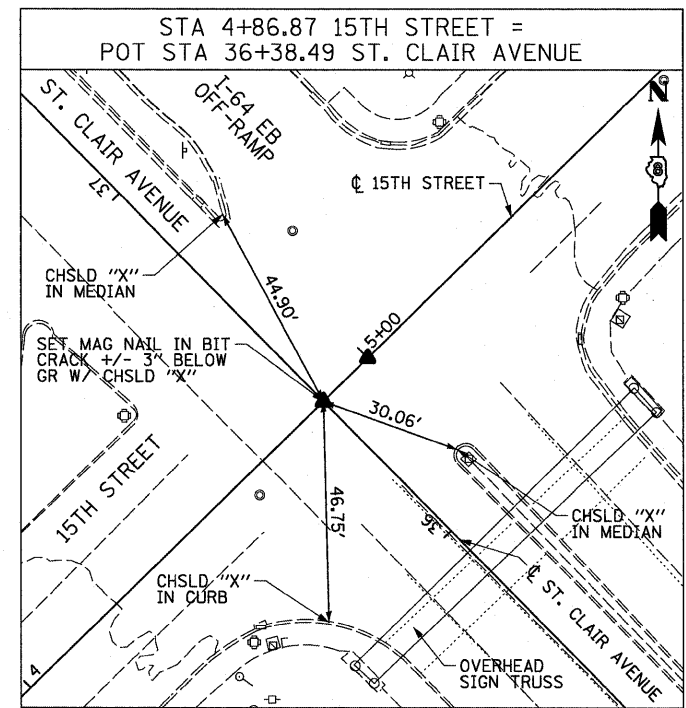
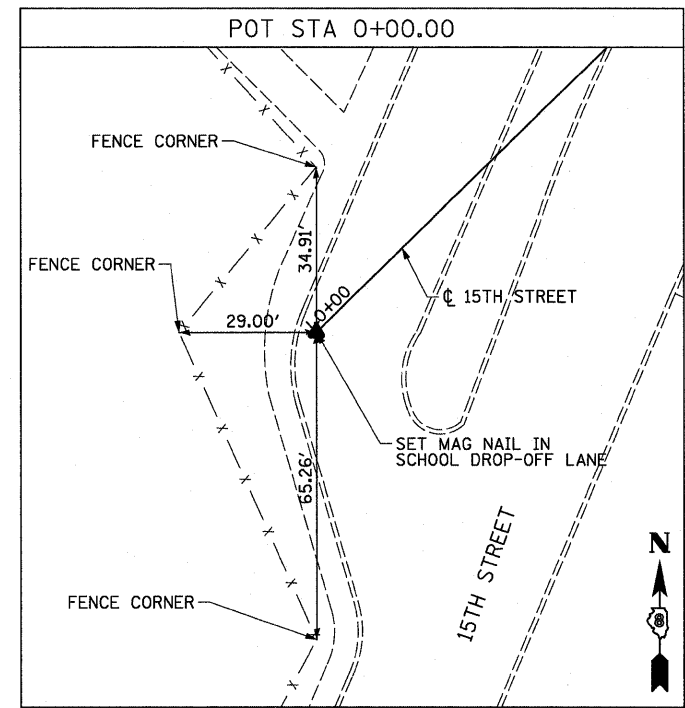
RAMP B				
POINT	CURVE	STATION	NORTHING	EASTING
POT		10+00.00	14,039,100.85	2,457,932.54
PC		10+34.18	14,039,120.56	2,457,960.47
PI	RAMPB-1	12+04.26	14,039,218.65	2,458,099.41
PT	RAMPB-1	13+34.17	14,039,128.06	2,458,243.35
PC	RAMPB-2	16+85.54	14,038,940.91	2,458,540.73
PI	RAMPB-2	17+36.68	14,038,913.67	2,458,584.02
PT	RAMPB-2	17+87.82	14,038,887.21	2,458,627.78
POT		18+96.62	14,038,830.93	2,458,720.89

BENCHMARKS				
MONUMENT	EL	STATION	OFFSET	DESCRIPTION
BM RFS7	416.29	ST. CLAIR 39+39	59.0' LT	CHISELED "X" IN CAP BOLT OF FIRE HYDRANT AT INTERSECTION OF ST. CLAIR AND 14TH STREET
BM DM18	414.02	ST. CLAIR 35+29	12.4' RT	DAVID MASON MONUMENT #18 ALUMINUM DISK IN MEDIAN OF ST. CLAIR AVE. APPROX. 100' SOUTHEAST OF 15TH ST. INTERSECTION
BM RFS 20	415.29	ST. CLAIR 29+83	30.7' LT	CUT "X" ON NORTHERLY CAP BOLT OF FIRE HYDRANT SOUTHWEST QUAD ST. CLAIR AVE. & 16TH STREET
BM WAR 2	395.34	EB I-64 40+72	96' RT	CHISELED "SQUARE" CONCRETE FOUNDATION OF LIGHT POLE I-64 E.B. ALONG 15TH STREET EXIT RAMP
BM WAR 3	400.28	EB I-64 49+91	53.3' RT	CHISELED "SQUARE" CONCRETE FOUNDATION OF LIGHT POLE SOUTH SIDE OF I-64 E.B. 75' EAST OF 15TH STREET BRIDGE
BM WAR 4	415.92	WB I-64 87+54	53.0' RT	CHISELED "SQUARE" CONCRETE FOUNDATION OF LIGHT POLE NORTH SIDE OF I-64 W.B. 500' EAST OF 15TH STREET BRIDGE
BM WAR 5	404.00	WB I-64 93+01	66.74' RT	CHISELED "SQUARE" NORTH CONCRETE FOUNDATION OF SIGN TRUSS OVER I-64 W.B. 75' EAST OF 15TH STREET BRIDGE
BM DAS 2	418.77	18TH 204+43	19.2' RT	CHISELED "X" SOUTHERLY CAP BOLT OF FIRE HYDRANT NORTHEAST QUAD. OF 18TH STREET & BAUGH AVENUE
BM DAS 4	415.74	BAUGH N 5+30	30.1' LT	CHISELED "SQUARE" SOUTHEAST CORNER OF TRAFFIC SIGNAL FOUNDATION N.W. QUAD. OF BAUGH AVE. & 15TH STREET





15TH STREET



BAUGH AVENUE N

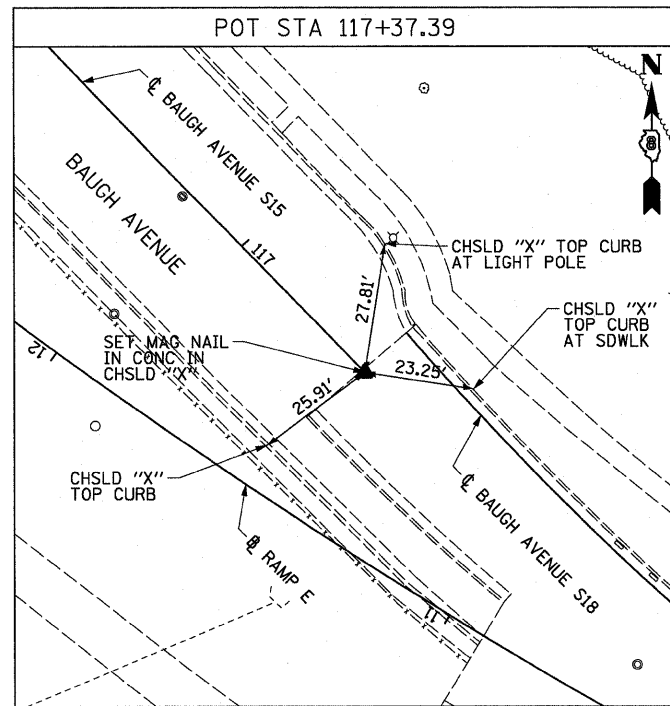
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

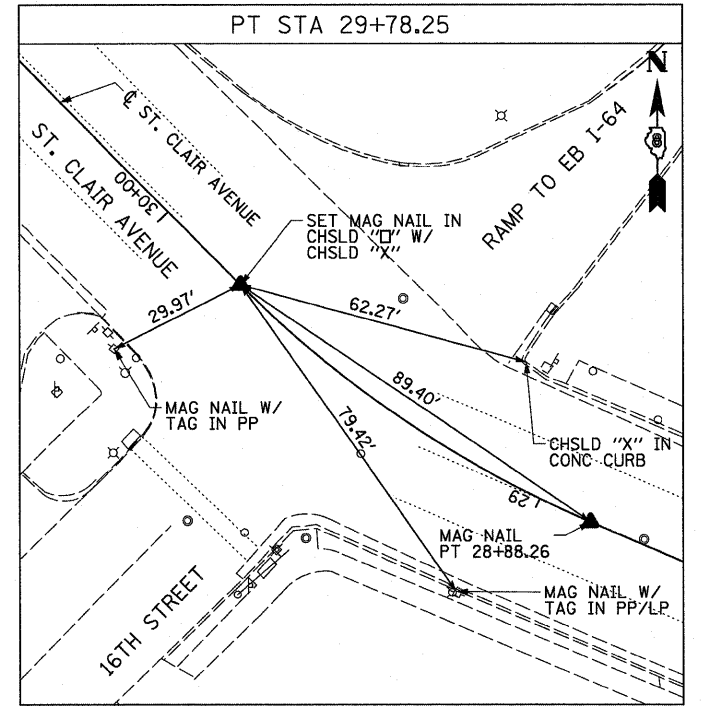
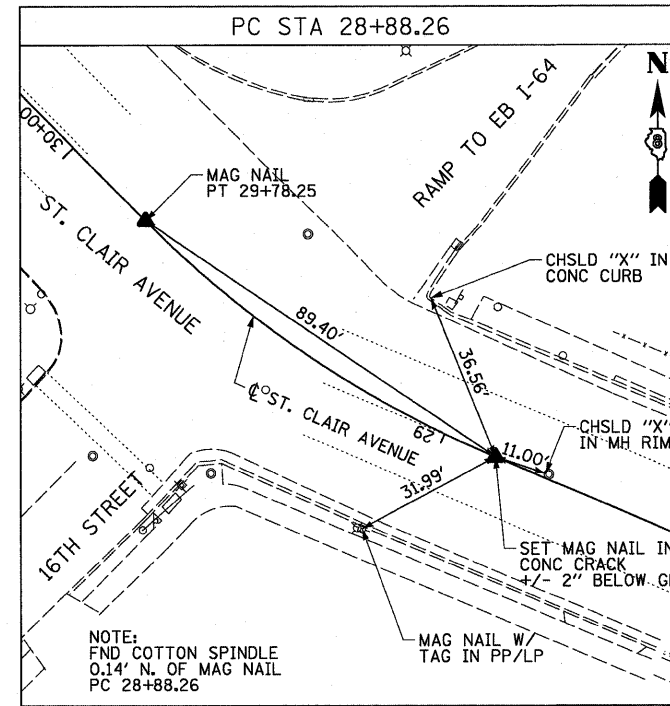
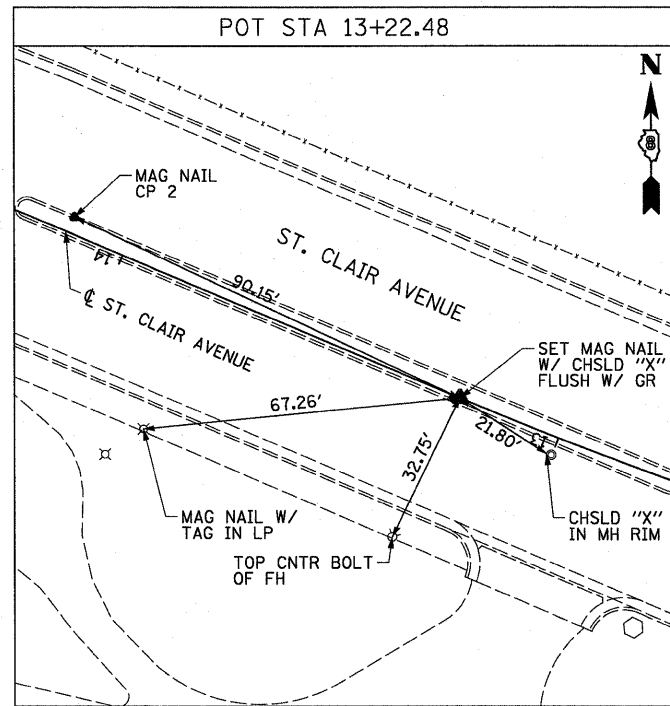
ALIGNMENT, TIES AND BENCHMARKS			
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	40
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 76C49	

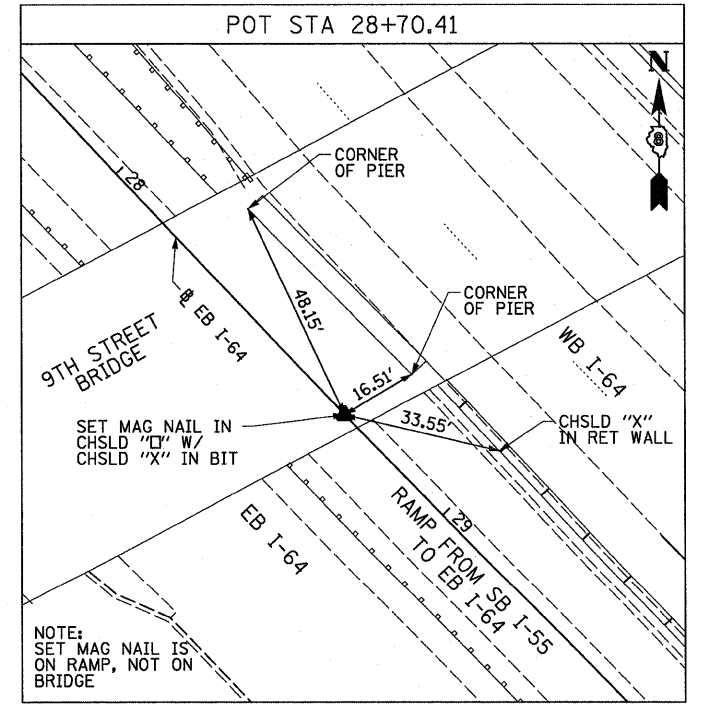
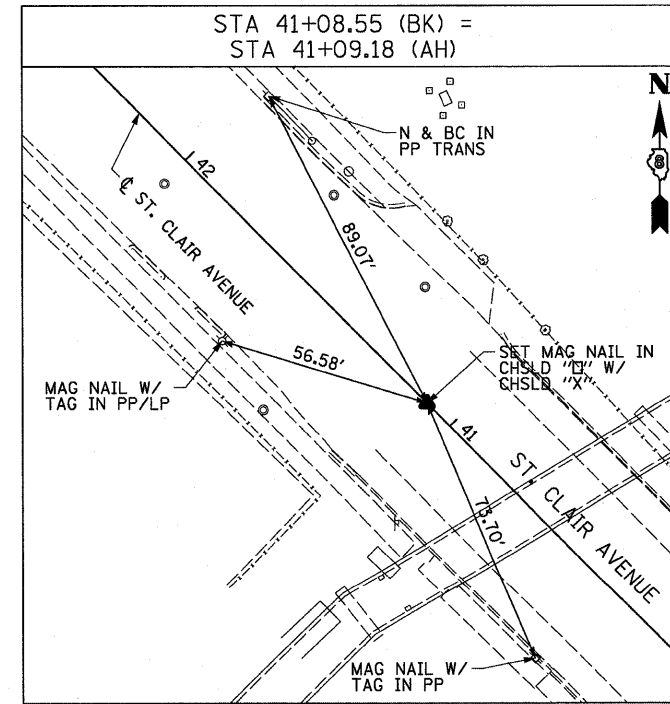
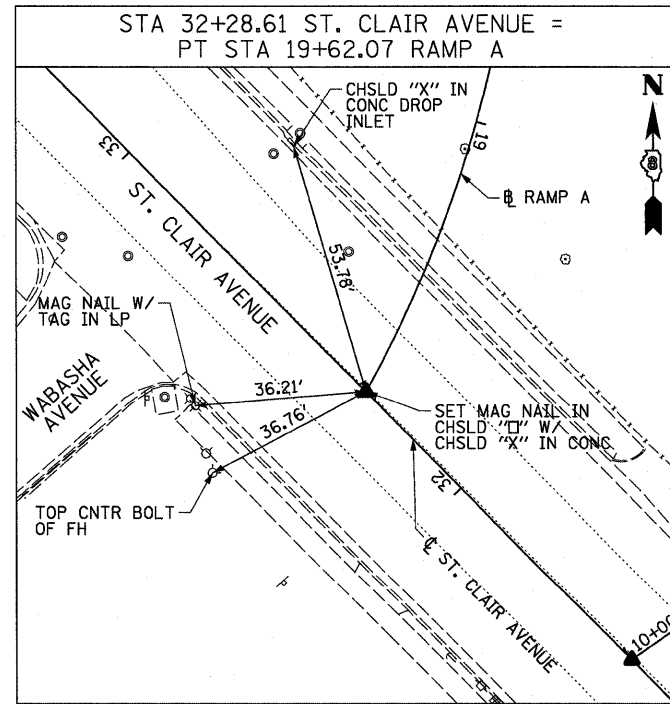
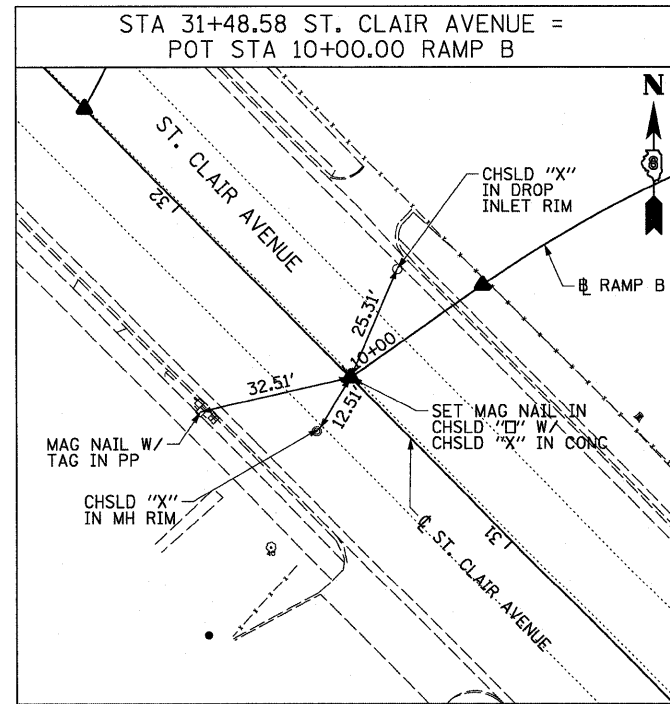
BAUGH AVENUE S15



ST. CLAIR AVENUE

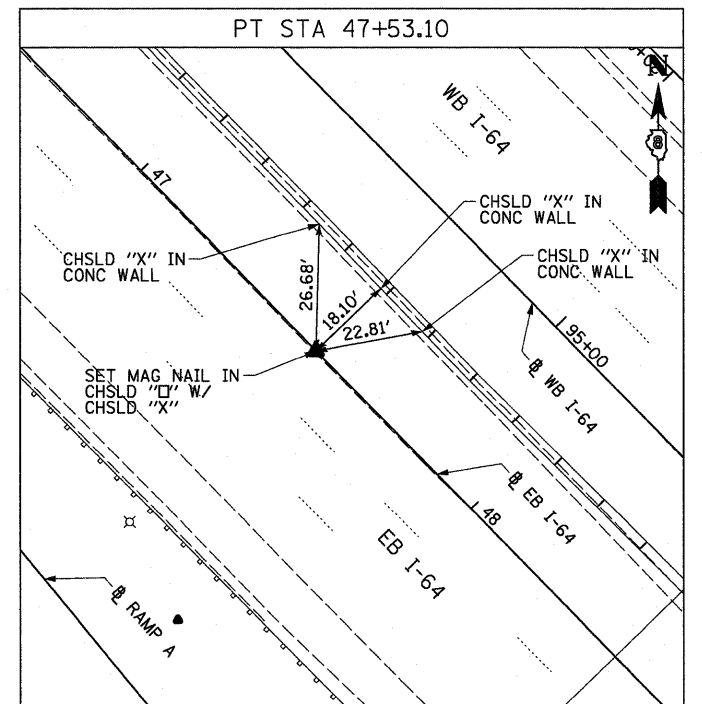
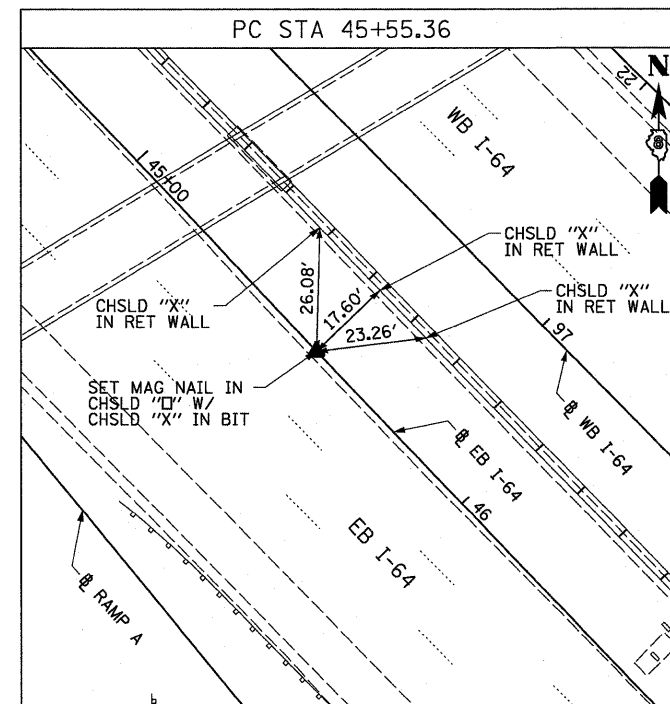
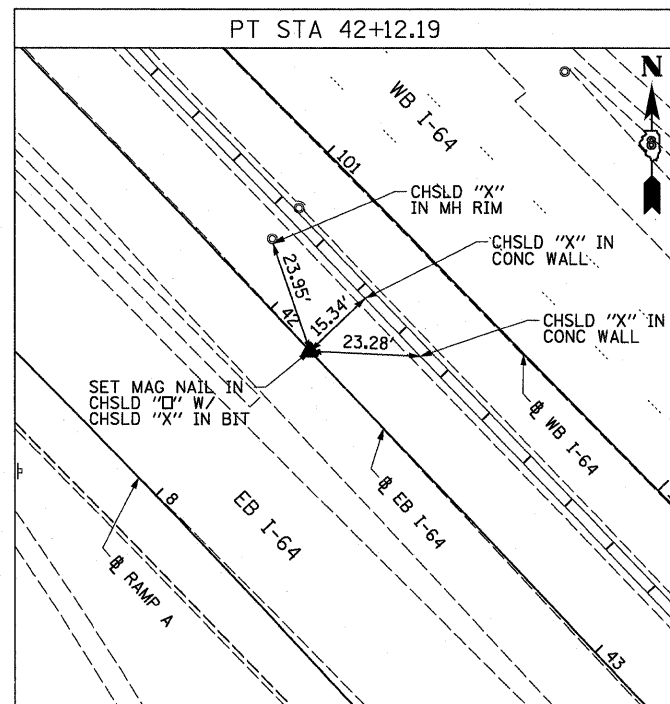
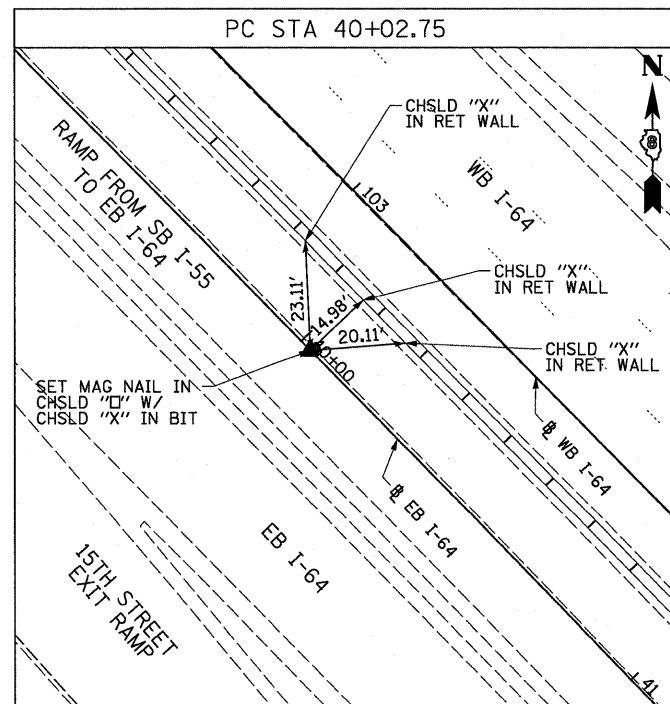


EB I-64

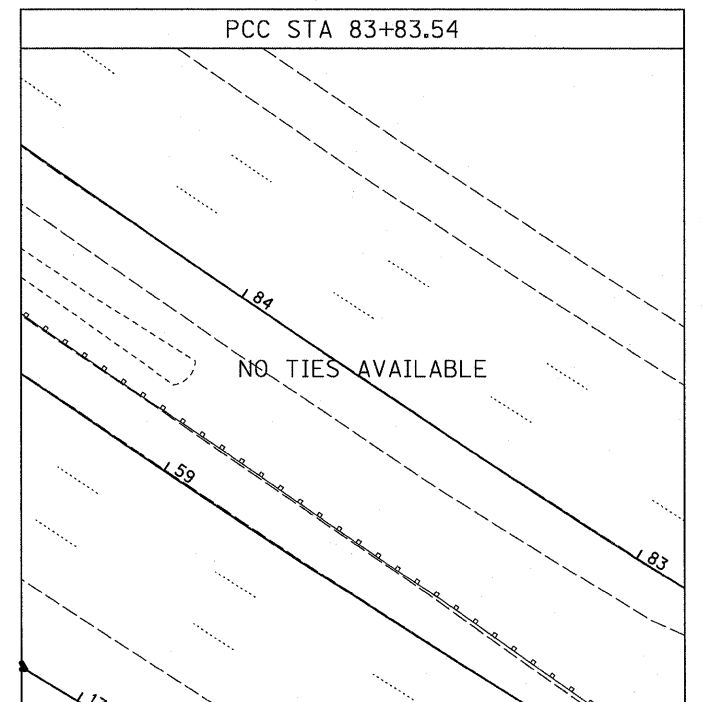
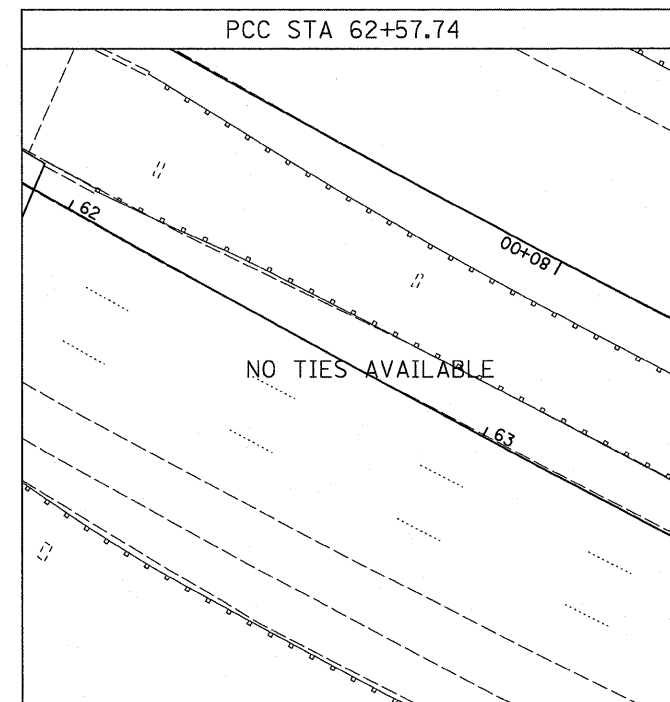
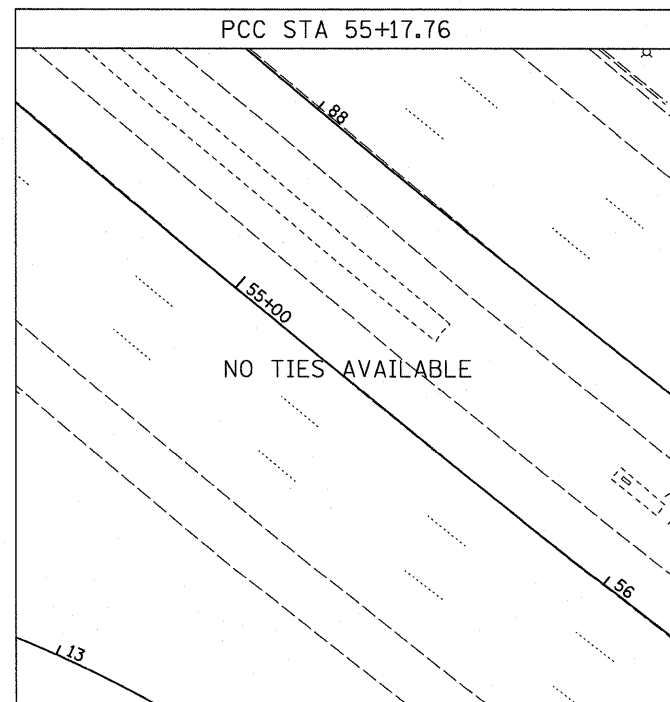
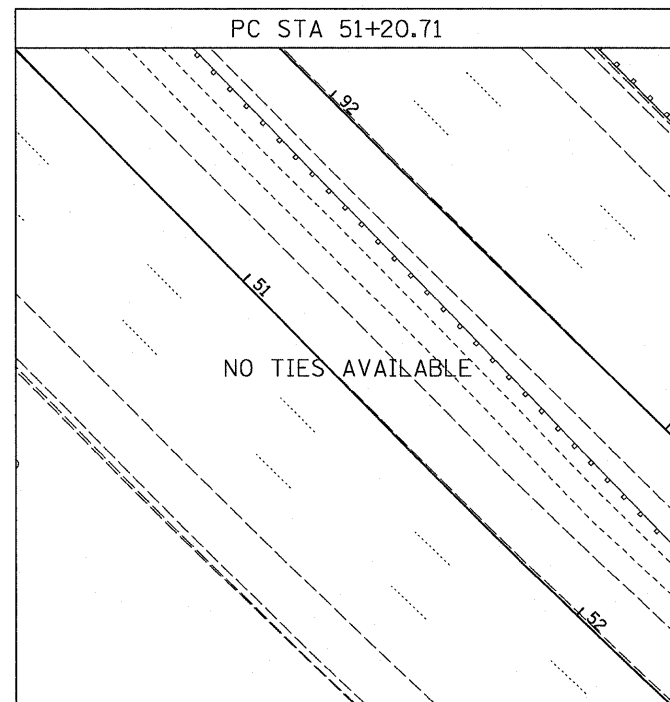


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	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -					FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				
					SCALE: NONE	SHEET NO. 5 OF 8 SHEETS	STA.	TO STA.				

EB I-64



WB I-64



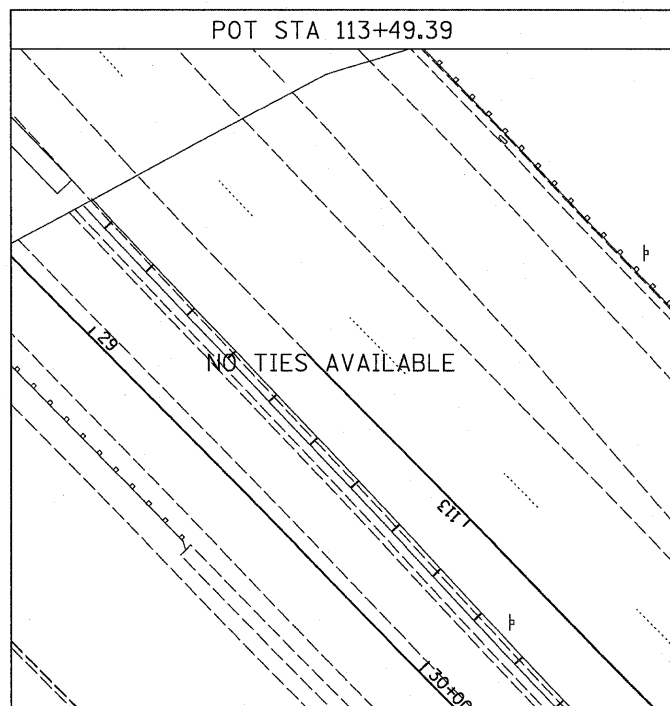
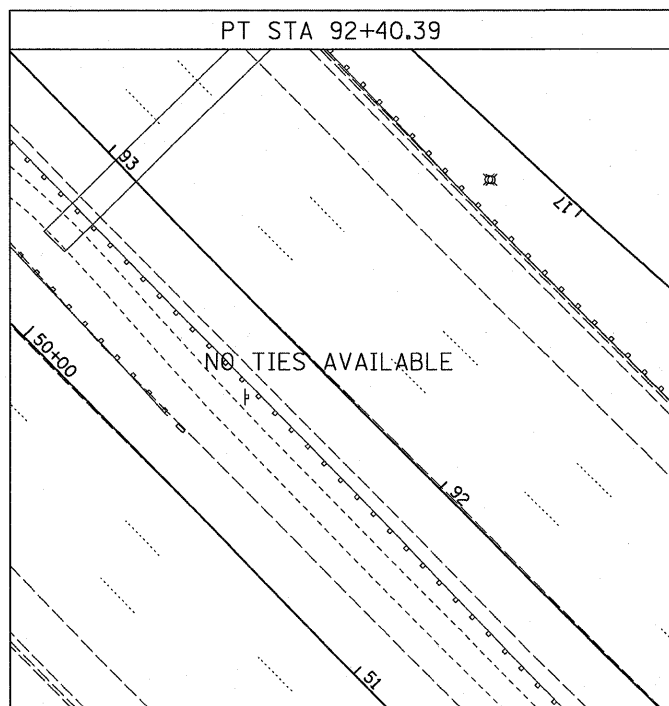
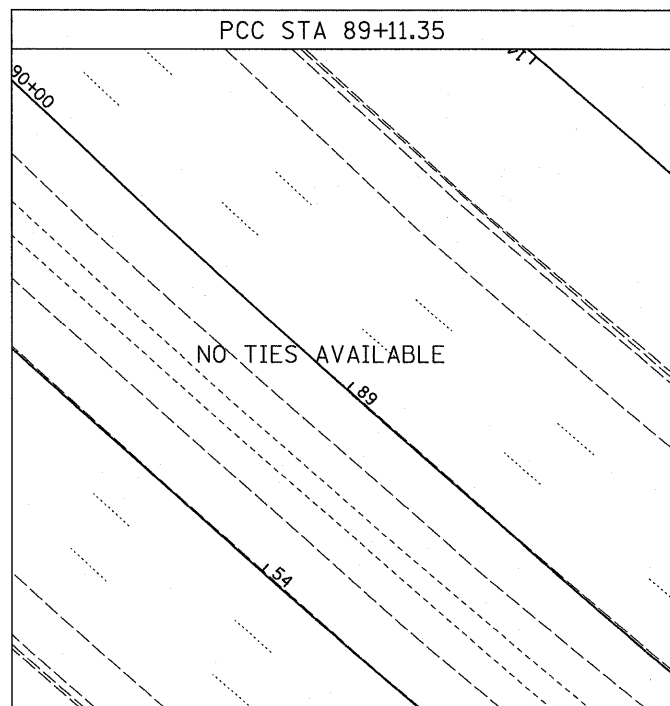
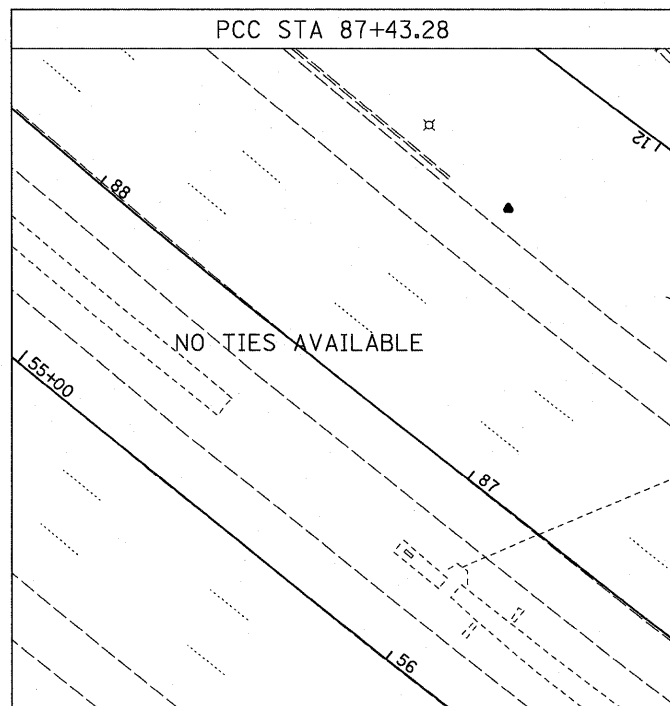
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

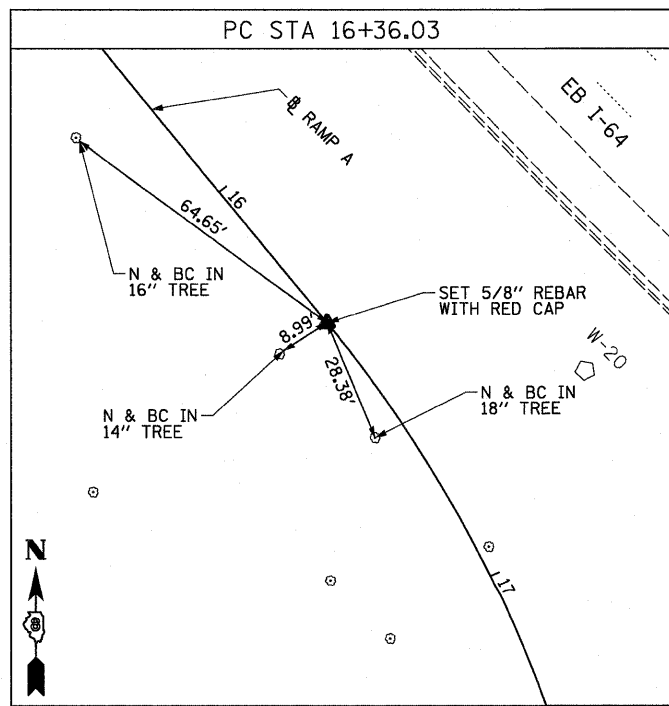
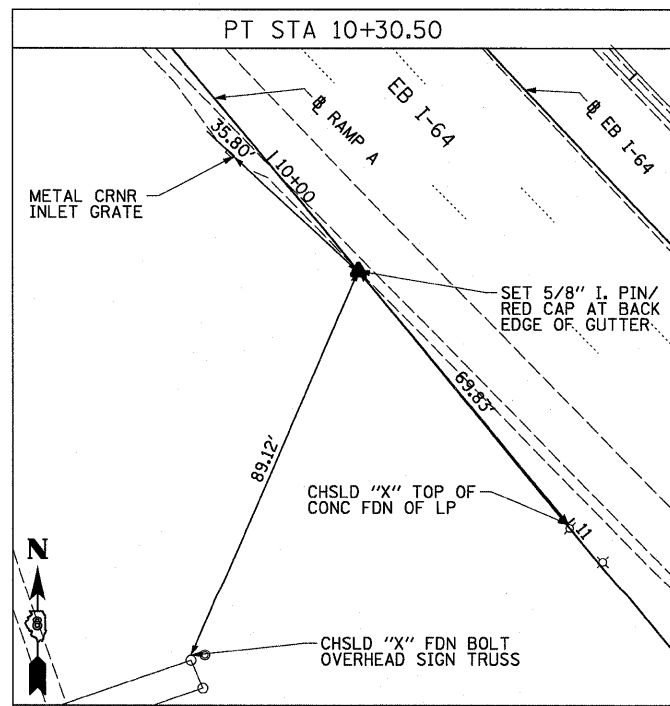
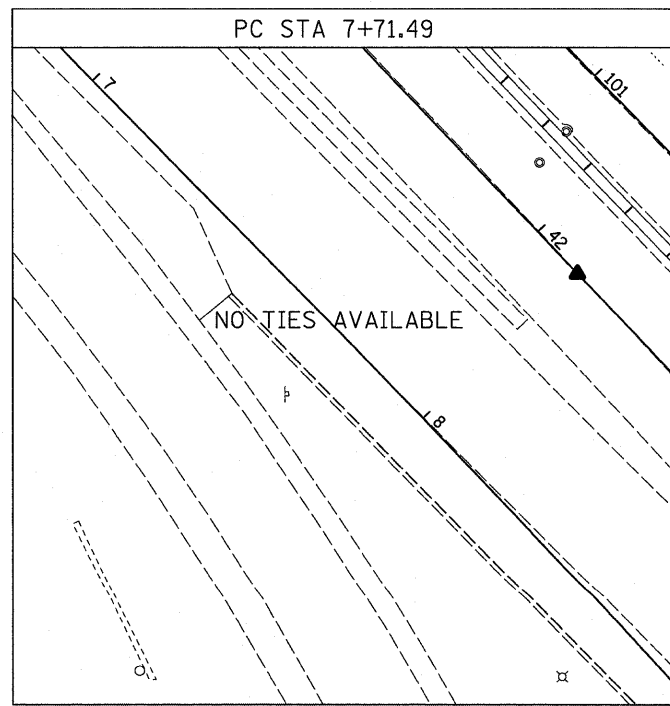
ALIGNMENT, TIES AND BENCHMARKS

SCALE: SHEET NO. 6 OF 8 SHEETS STA. TO STA.

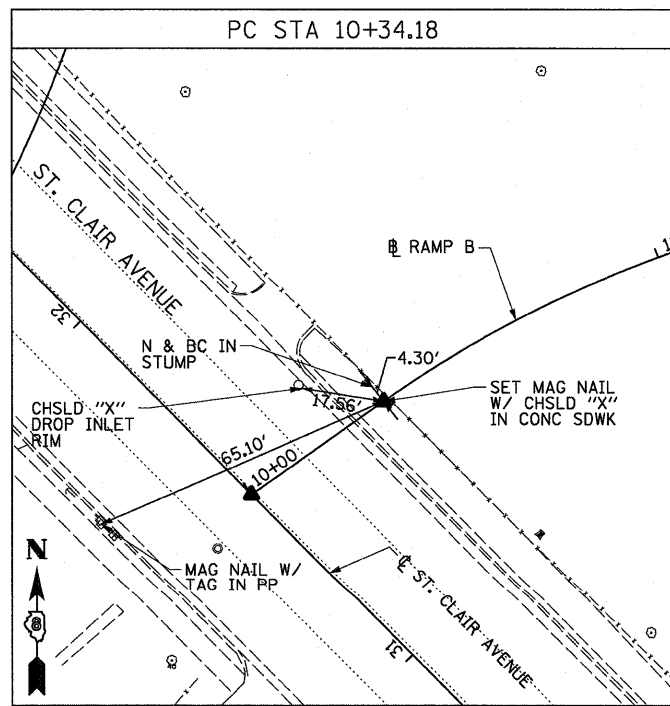
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	42
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	



RAMP A



RAMP B



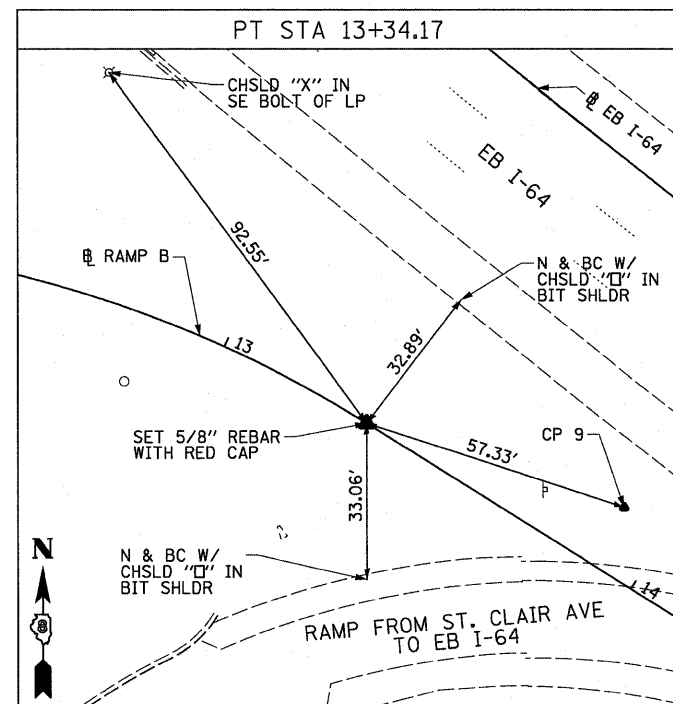
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

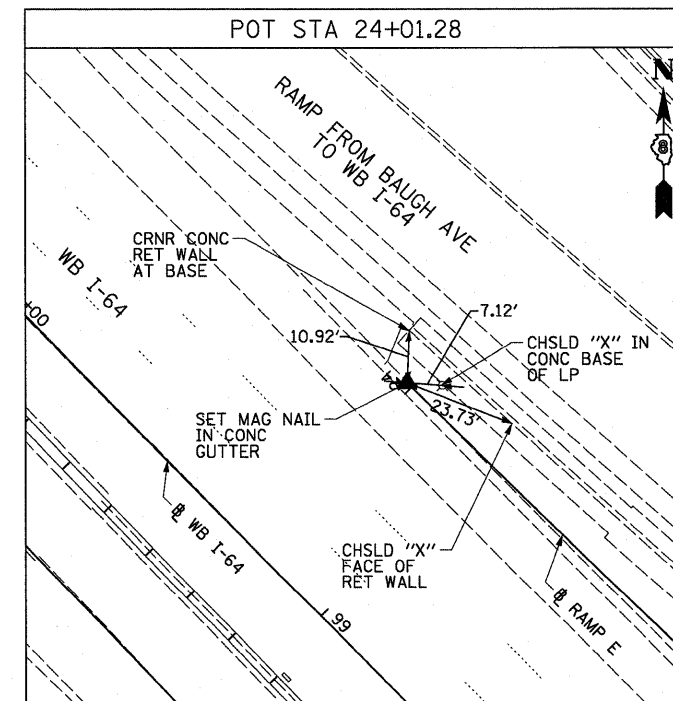
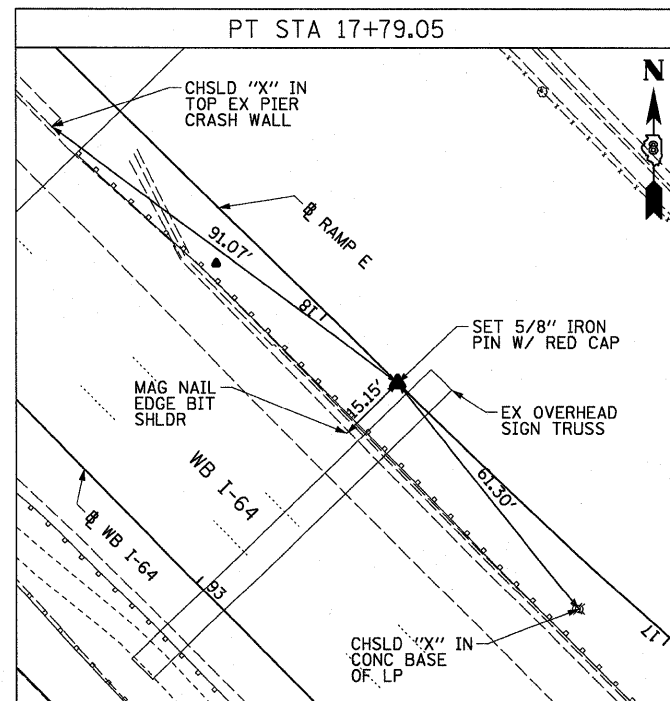
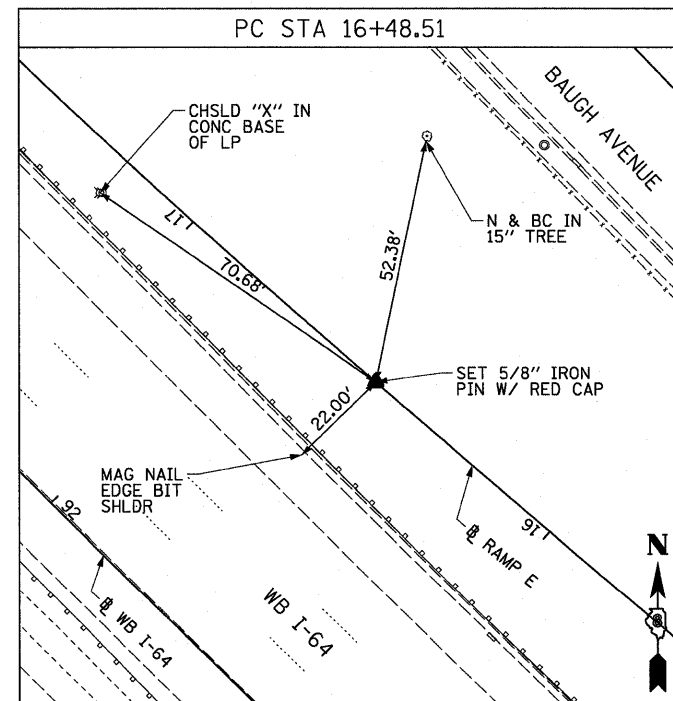
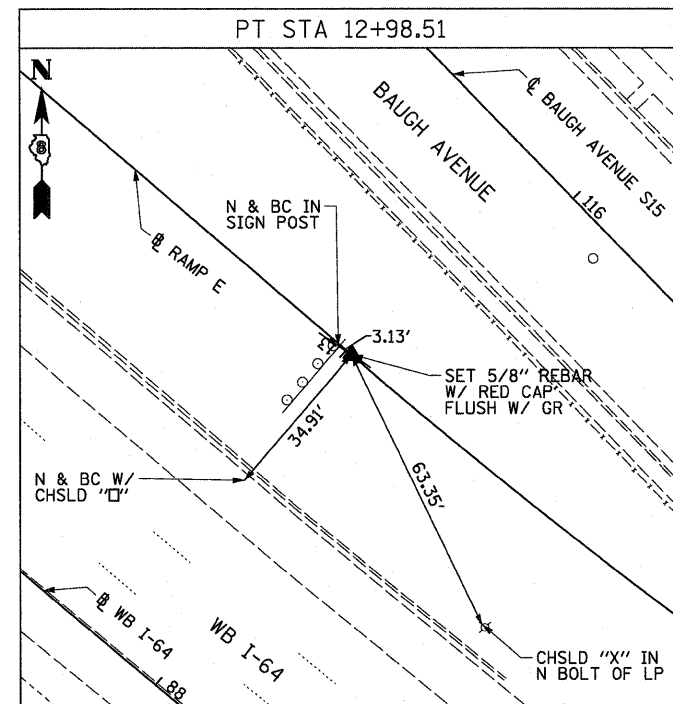
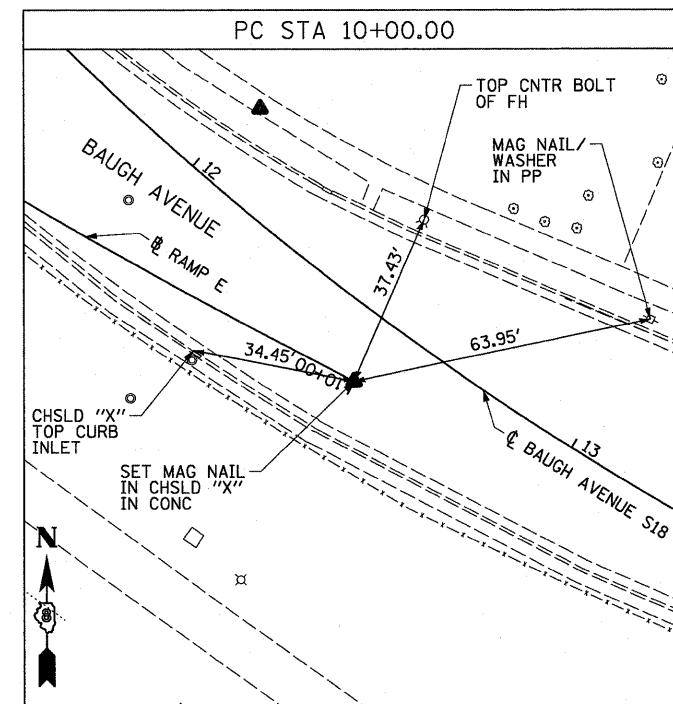
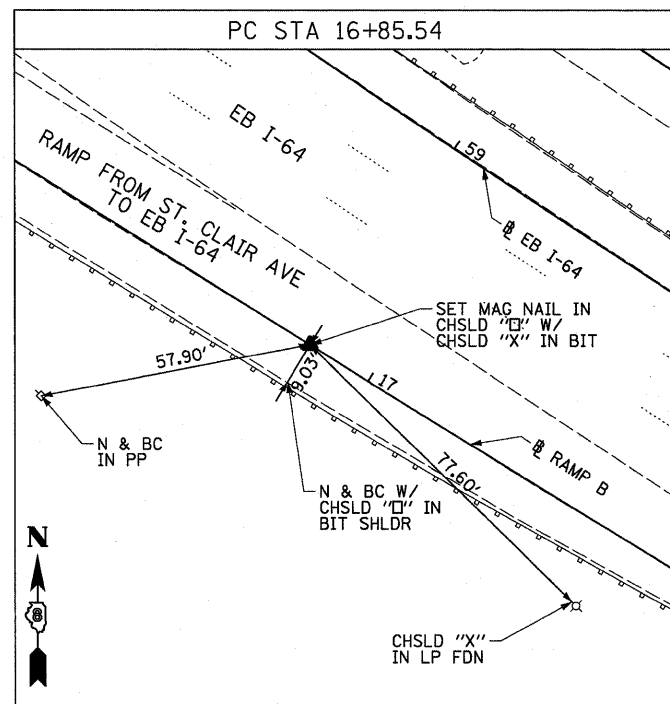
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	43
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	

RAMP B



RAMP E



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	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS

SCALE: NONE SHEET NO. 8 OF 8 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	44
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	

**SUPERELEVATION TRANSITION DATA TABLES**

**CURVE RAMP A\_C6-1**

	STATION	CROSS SLOPE (%)	PGL ELEVATION	LEFT EOP AT 16'
ATTAINMENT	NO ATTAINMENT START AT FULL SUPER AT THE BEGINNING OF THE CURVE			
REMOVAL	10+17.90	5.00%	394.39	395.19
	10+25.00	4.49%	394.35	395.07
	10+50.00	2.71%	394.20	394.63
	10+59.90	2.00%	394.14	394.46

**CURVE RAMPB-1**

	STATION	CROSS SLOPE (%)	PGL ELEVATION	LEFT EOP AT 16'
ATTAINMENT	10+22.43	-0.16%	412.53	412.50
	10+25.00	-0.30%	412.49	412.44
	10+50.00	1.19%	412.14	412.33
	10+75.00	2.41%	411.95	412.34
	11+00.00	3.63%	411.92	412.50
	11+25.00	4.85%	412.04	412.82
	11+50.00	6.07%	412.31	413.28
	11+75.00	7.29%	412.74	413.91
	11+89.43	8.00%	413.06	414.34
	FULL SE	12+00.00	8.00%	413.33
12+25.00		8.00%	414.05	415.33
12+50.00		8.00%	414.77	416.05
12+75.00		8.00%	415.50	416.78
13+00.00		8.00%	416.23	417.51
13+00.93		8.00%	416.26	417.54
REMOVAL	13+25.00	7.01%	416.96	418.08
	13+50.00	5.98%	417.68	418.64
	13+75.00	4.95%	418.41	419.20
	14+00.00	3.92%	419.14	419.77
	14+25.00	2.90%	419.87	420.33
	14+50.00	1.87%	420.59	420.89
	14+67.93	1.13%	421.12	421.30

**CURVE RAMP E-2**

	RAMP E STATION	WB I-64 STATION	CROSS SLOPE (%)	RAMP E PGL ELEVATION	LEFT EOP AT 16'
ATTAINMENT	15+68.01	-	1.50%	405.48	405.72
	15+75.00	-	1.72%	405.24	405.52
	16+00.00	-	2.87%	404.36	404.82
	16+25.00	91+45.78	3.55%	403.49	404.06
	16+50.00	91+71.02	4.23%	402.63	403.31
	16+75.00	91+96.26	4.90%	401.78	402.56
	16+83.01	92+04.35	5.10%	401.50	402.32
	17+00.00	92+21.50	5.10%	400.99	401.81
FULL SE	17+25.00	92+46.68	5.10%	400.23	401.05
	17+49.35	92+71.01	5.10%	399.49	400.31
REMOVAL	17+50.00	92+71.66	5.07%	399.48	400.29
	17+75.00	92+96.65	4.29%	398.83	399.52
	18+00.00	93+21.64	3.51%	398.20	398.76
	18+25.00	93+46.64	2.73%	397.55	397.99
	18+48.35	93+69.98	2.00%	396.98	397.30

\* ALSO SEE GORE GRADING DETAILS FROM STA 16+00.016

**CURVE RAMP A\_C6-2**

	STATION	CROSS SLOPE (%)	PGL ELEVATION	LEFT EOP AT 16'
ATTAINMENT	15+21.63	2.00%	397.56	397.88
	15+25.00	2.14%	397.66	398.00
	15+50.00	3.17%	398.43	398.94
	15+75.00	4.20%	399.27	399.94
	16+00.00	5.23%	400.17	401.01
	16+25.00	6.26%	401.14	402.14
	16+50.00	7.30%	402.12	403.29
	16+64.63	7.90%	402.70	403.96
FULL SE	16+75.00	7.90%	403.10	404.36
	17+00.00	7.90%	404.09	405.35
	17+25.00	7.90%	405.07	406.33
	17+50.00	7.90%	406.06	407.32
	17+75.00	7.90%	407.04	408.30
	18+00.00	7.90%	408.03	409.29
REMOVAL	18+25.00	7.90%	409.01	410.27
	18+32.53	7.90%	409.30	410.56
	18+50.00	6.93%	409.93	411.04
	18+75.00	5.54%	410.74	411.63
	19+00.00	4.14%	411.42	412.08
	19+25.00	2.75%	411.98	412.42
	19+38.53	2.00%	412.25	412.57

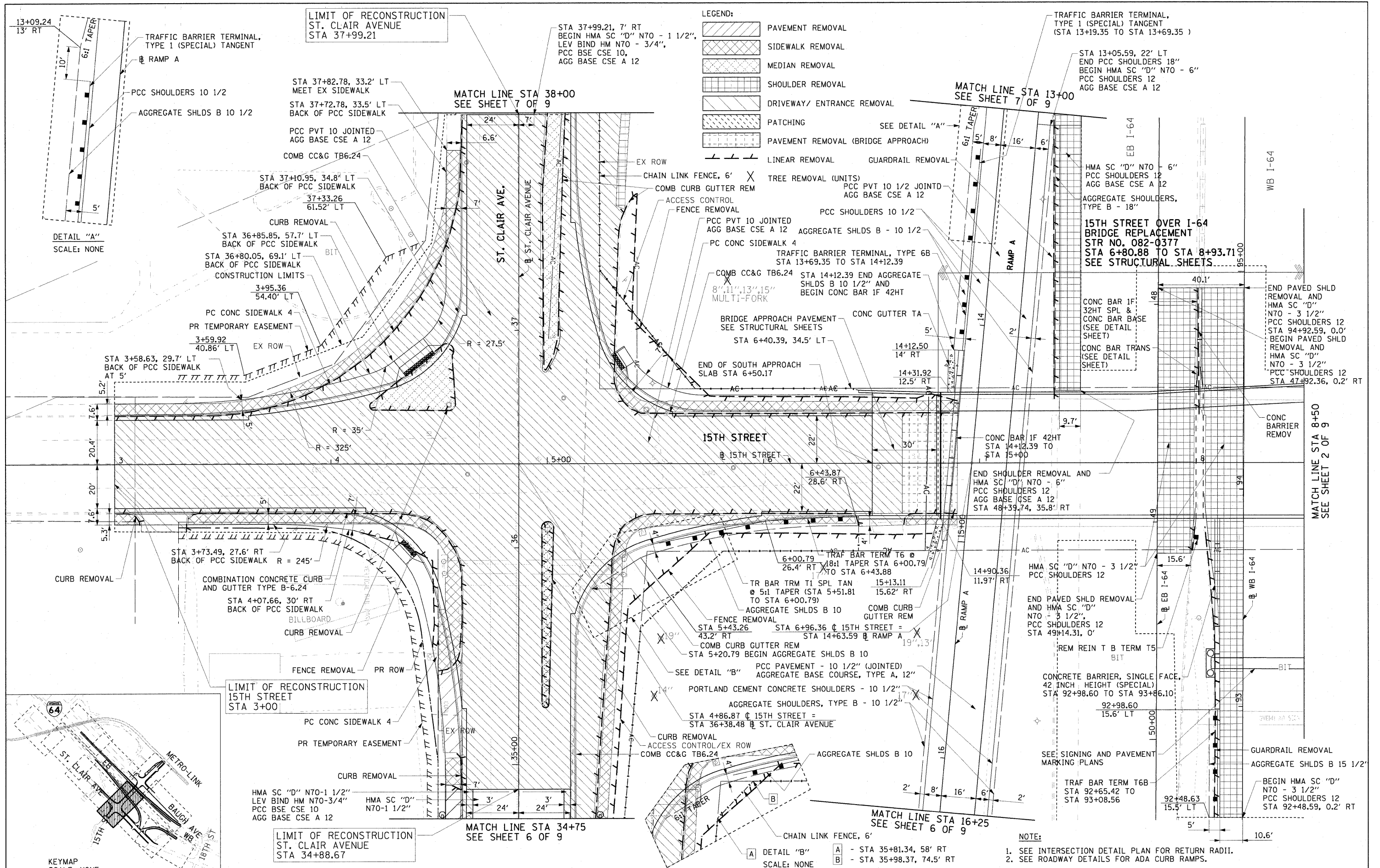
\* ALSO SEE GORE GRADING DETAILS FROM STA 16+00.016

\* ALSO SEE GORE GRADING DETAILS FROM STA 16+00.016

**CURVE RAMP E-1**

	STATION	CROSS SLOPE (%)	PGL ELEVATION	LEFT EOP AT 16'
ATTAINMENT	9+53.80	4.50%	-	-
	10+00.00	5.94%	-	-
	10+30.80	6.90%	-	-
FULL SE	10+50.00	6.90%	-	-
	10+75.00	6.90%	-	-
	11+00.00	6.90%	414.79	415.89
	11+25.00	6.90%	414.70	415.80
	11+50.00	6.90%	414.60	415.70
	11+75.00	6.90%	414.51	415.61
	12+00.00	6.90%	414.41	415.51
	12+25.00	6.90%	414.26	415.36
	12+51.41	6.90%	414.03	415.13
	REMOVAL	13+00.00	5.38%	413.37
13+25.00		4.60%	412.93	413.67
13+50.00		3.82%	412.41	413.02
13+75.00		3.04%	411.81	412.30
14+00.00		2.26%	411.15	411.51
14+08.41		2.00%	410.90	411.22





**LEGEND:**

- PAVEMENT REMOVAL
- SIDEWALK REMOVAL
- MEDIAN REMOVAL
- SHOULDER REMOVAL
- DRIVEWAY/ ENTRANCE REMOVAL
- PATCHING
- PAVEMENT REMOVAL (BRIDGE APPROACH)
- LINEAR REMOVAL
- GUARDRAIL REMOVAL
- TREE REMOVAL (UNITS)
- ACCESS CONTROL FENCE REMOVAL
- CURB REMOVAL
- GUTTER REMOVAL
- CHAIN LINK FENCE, 6'

**DETAIL "A"**  
SCALE: NONE

**DETAIL "B"**  
SCALE: NONE

- NOTE:**
- SEE INTERSECTION DETAIL PLAN FOR RETURN RADII.
  - SEE ROADWAY DETAILS FOR ADA CURB RAMP.

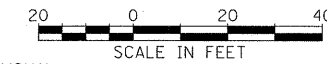
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		DATE 03/19/10	REVISED -									

MATCH LINE STA 7+50  
SEE SHEET 4 OF 9

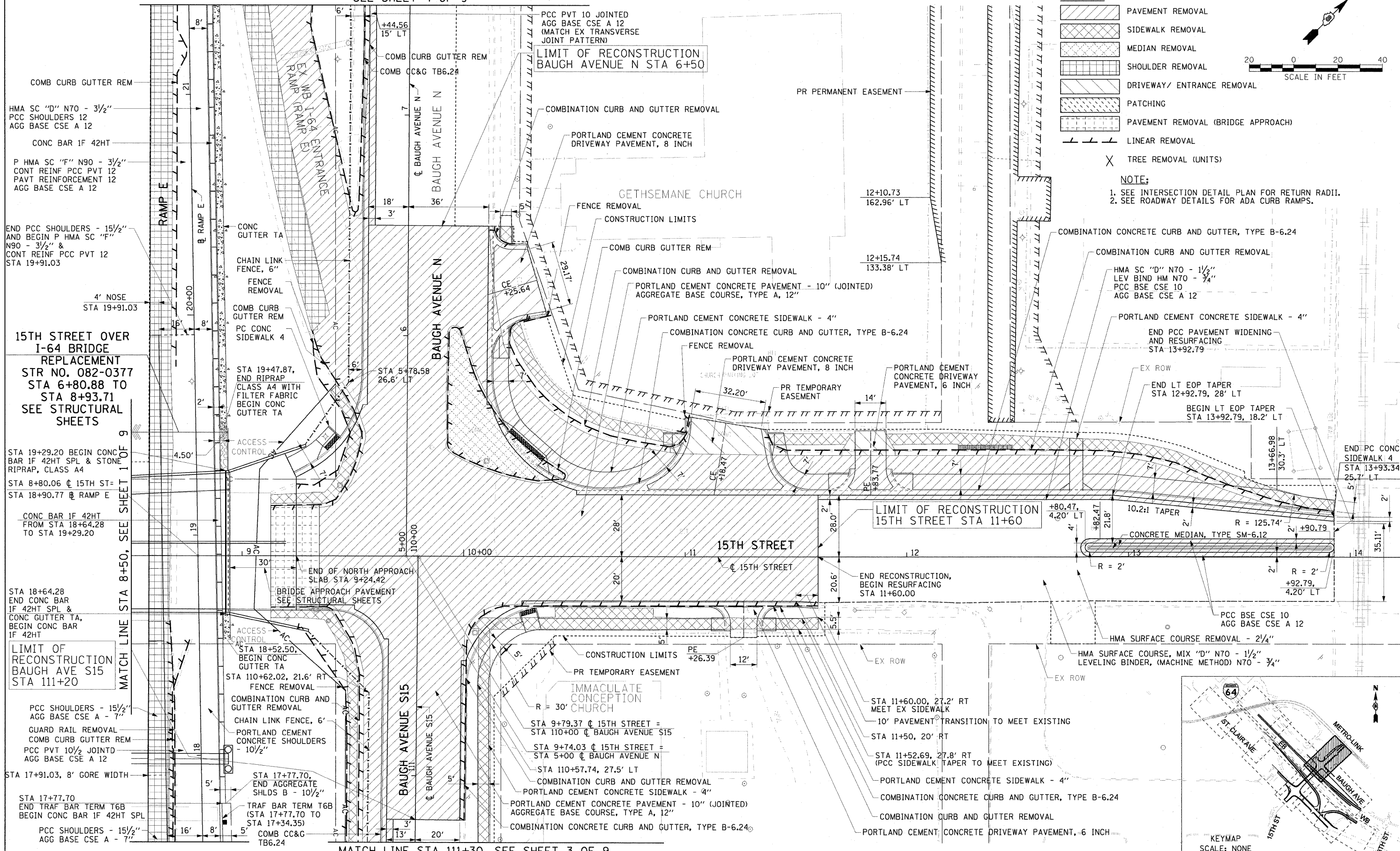
LEGEND:

- PAVEMENT REMOVAL
- SIDEWALK REMOVAL
- MEDIAN REMOVAL
- SHOULDER REMOVAL
- DRIVEWAY/ ENTRANCE REMOVAL
- PATCHING
- PAVEMENT REMOVAL (BRIDGE APPROACH)
- LINEAR REMOVAL
- TREE REMOVAL (UNITS)



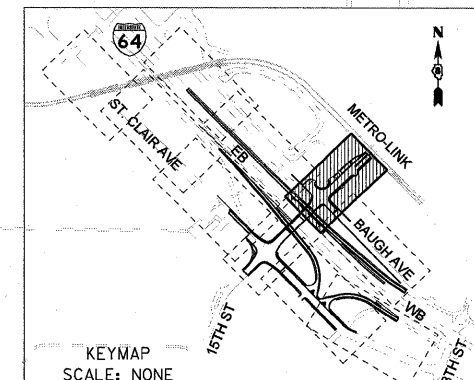
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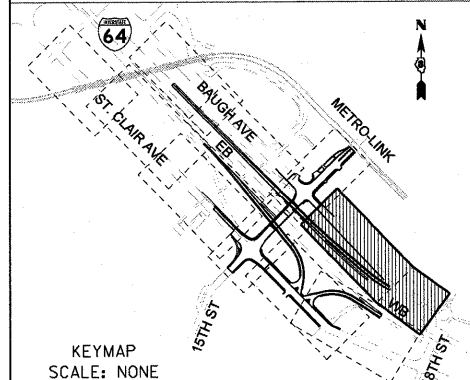
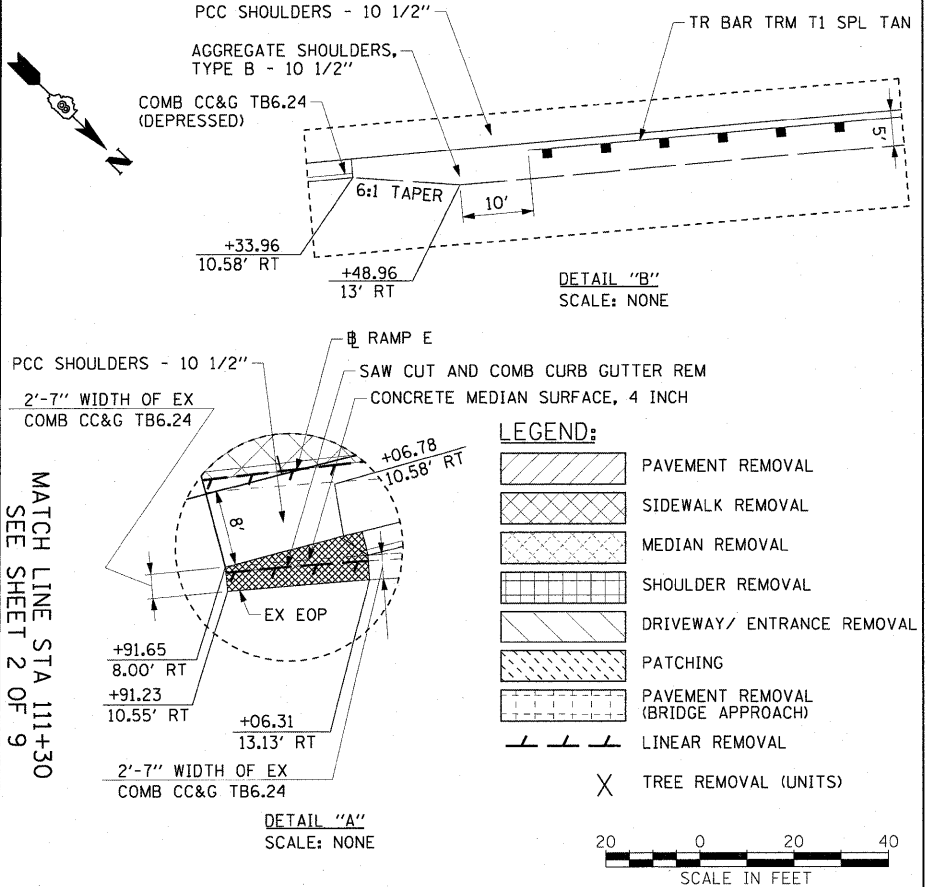
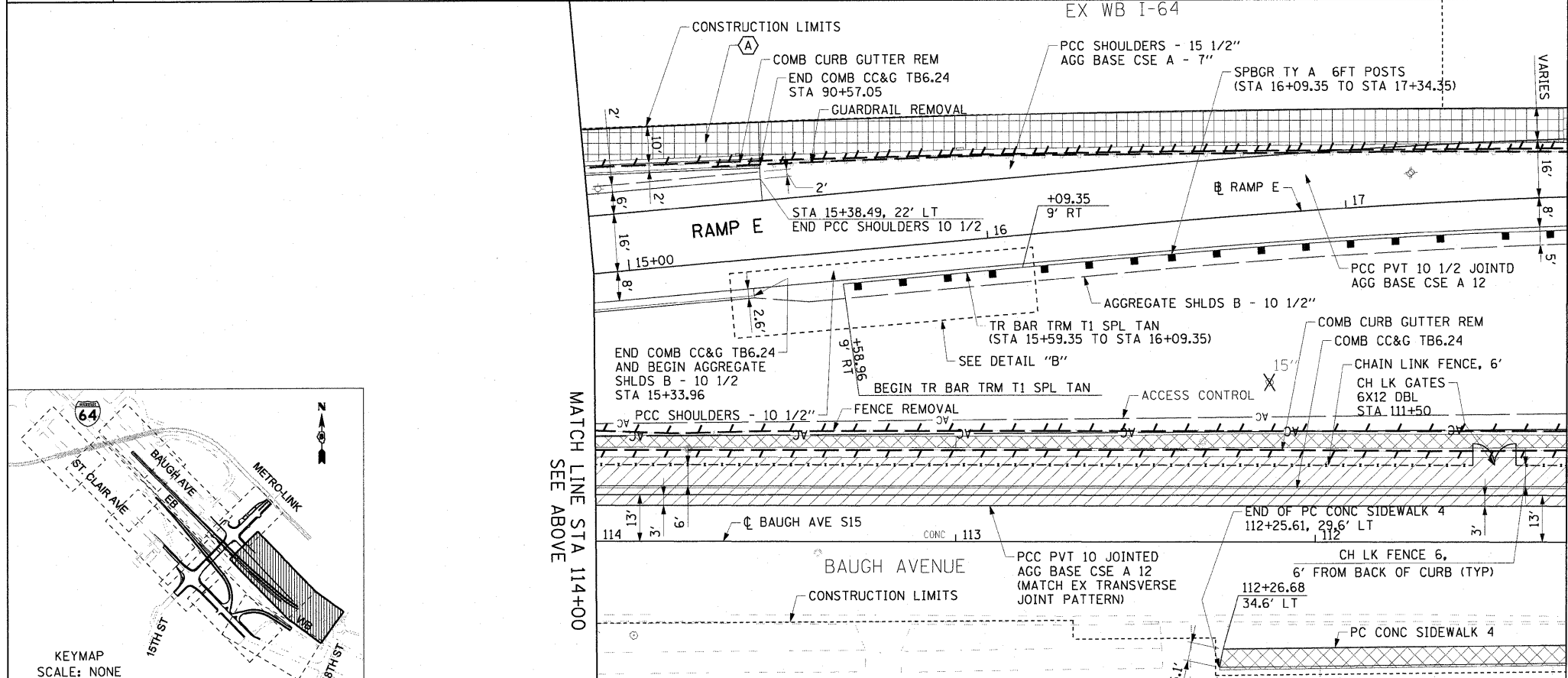
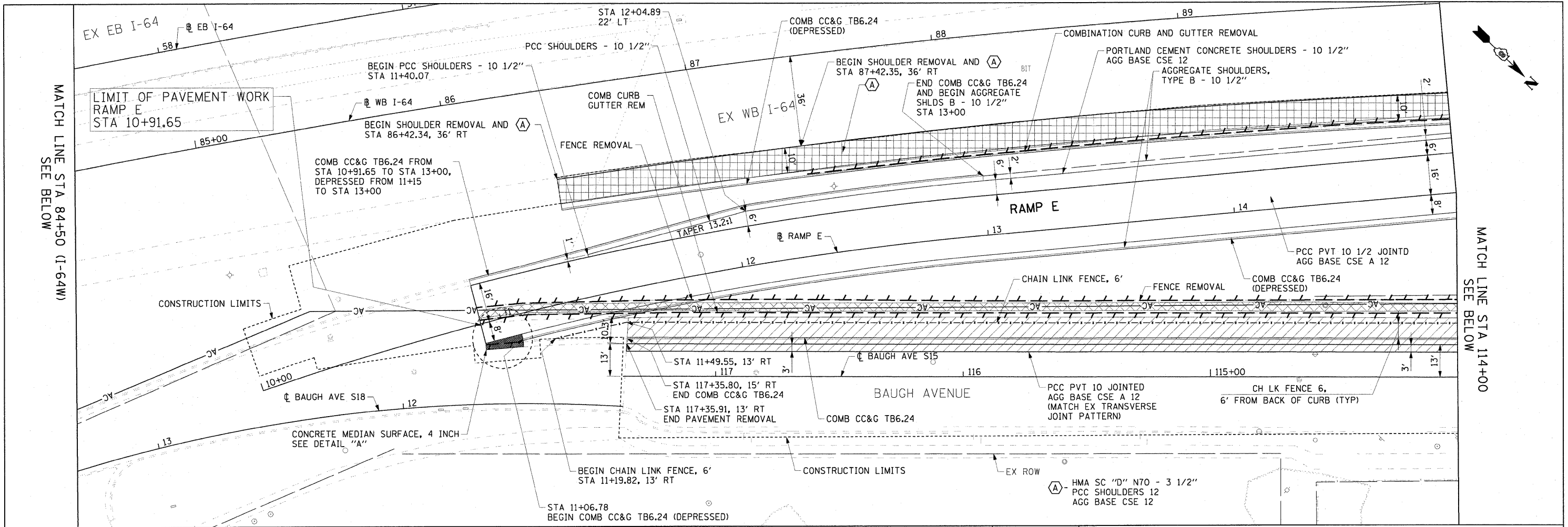
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2. SEE ROADWAY DETAILS FOR ADA CURB RAMPS.



MATCH LINE STA 111+30, SEE SHEET 3 OF 9

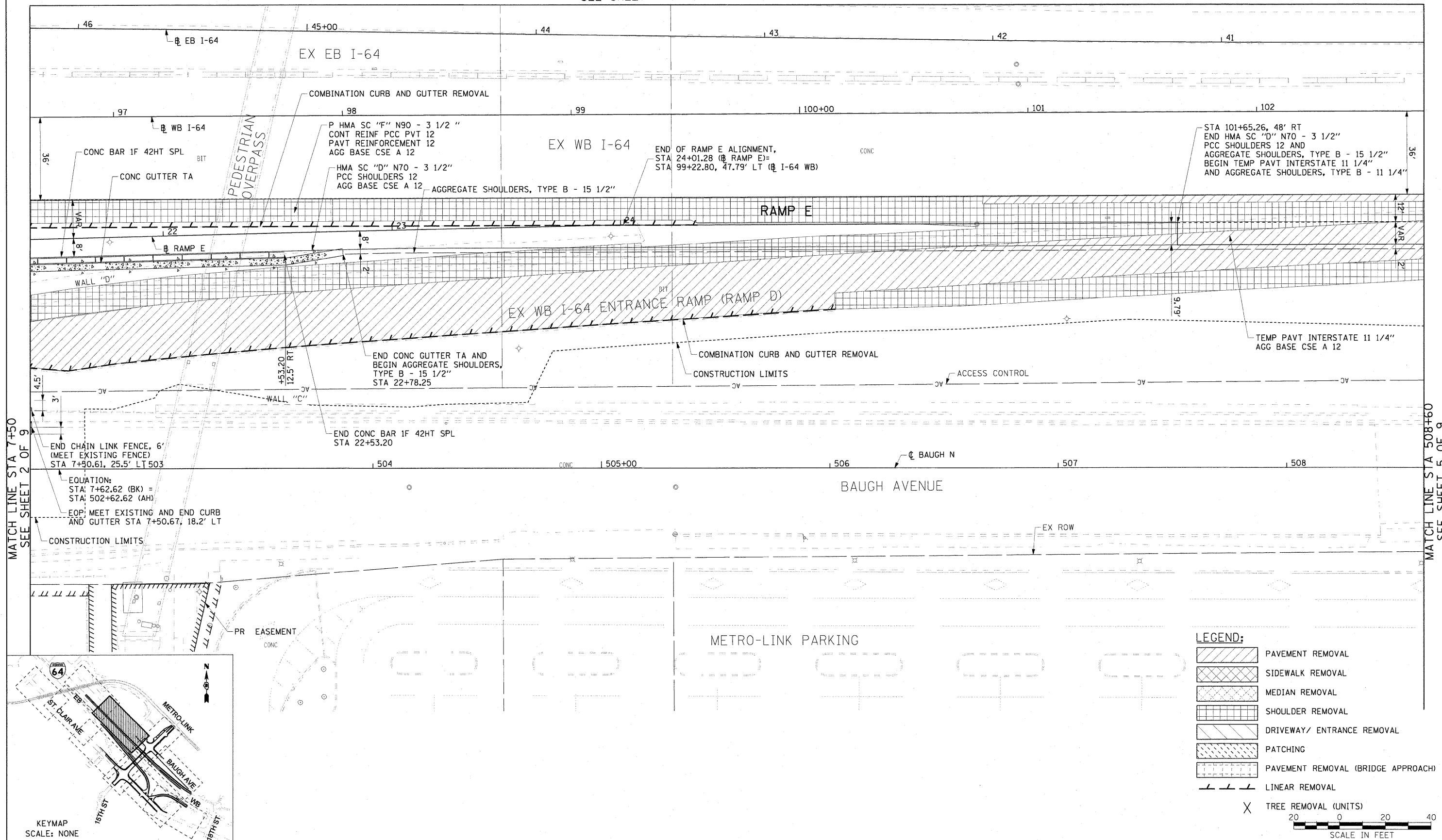
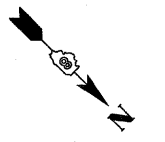
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\$FILEAS	PLOT SCALE = 20,000' / IN.	DRAWN PP	REVISED -			64	82-1-2HB	ST. CLAIR	345	47	
	PLOT DATE = 4/14/2010	CHECKED DBM	REVISED -			SCALE: 1" = 20'		SHEET NO. 2 OF 9 SHEETS		STA. 8+50 TO STA. 13+92.79	
		DATE 03/19/10	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		CONTRACT NO. 76C49			





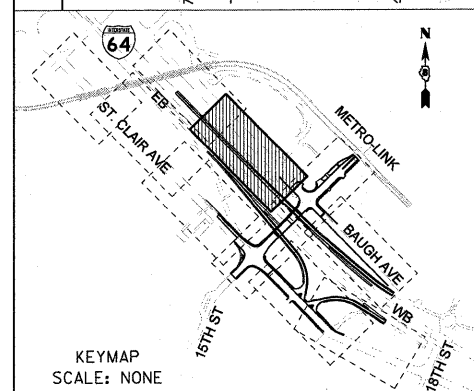
FILE NAME =	USER NAME = pmsarno	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>		<b>ROADWAY PLAN - RAMP E</b>		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#	PLOT SCALE = 20,000' / IN.	DRAWN PP	REVISED -					64	82-1-2HB	ST. CLAIR	345	48
	PLOT DATE = 3/19/2010	CHECKED DBM	REVISED -	SCALE: 1" = 20'		SHEET NO. 3 OF 9 SHEETS		STA. 10+91.65 TO STA. 111+30		CONTRACT NO. 76C49		
		DATE 03/19/10	REVISED -					SCALE IN FEET		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		

MATCH LINE  
SEE SHEET 7 OF 9



MATCH LINE STA 7+50  
SEE SHEET 2 OF 9

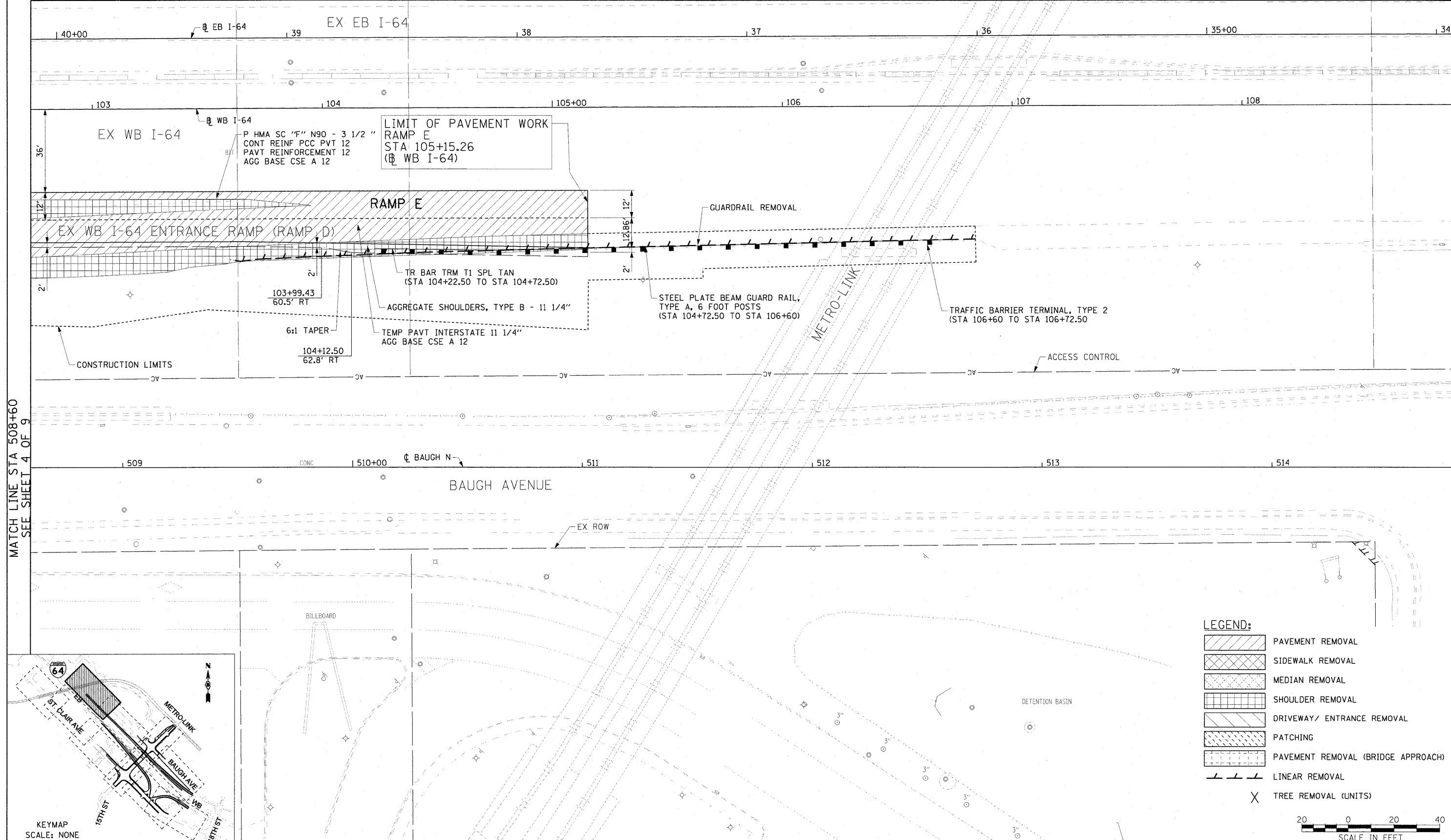
MATCH LINE STA 508+60  
SEE SHEET 5 OF 9



FILE NAME =	USER NAME = pmsarno	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN - RAMP E</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
#FILE#		DRAWN PP	REVISED -		SCALE: 1" = 20'	SHEET NO. 4 OF 9 SHEETS	STA. 7+50 TO STA. 508+60	64	82-1-2HB	ST. CLAIR	345	49	
		CHECKED DBM	REVISED -										
		DATE 03/19/10	REVISED -					FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT				



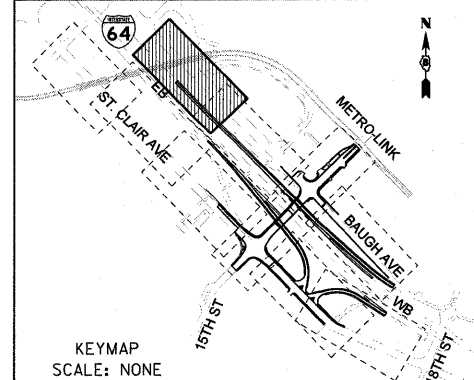
MATCH LINE  
SEE SHEET 8 OF 9



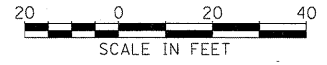
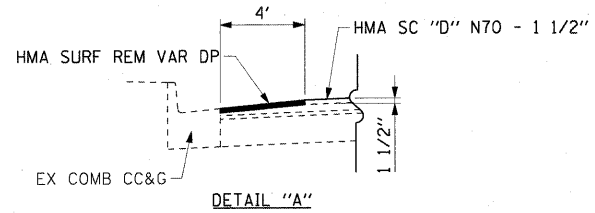
MATCH LINE STA 508+60  
SEE SHEET 4 OF 9

**LEGEND:**

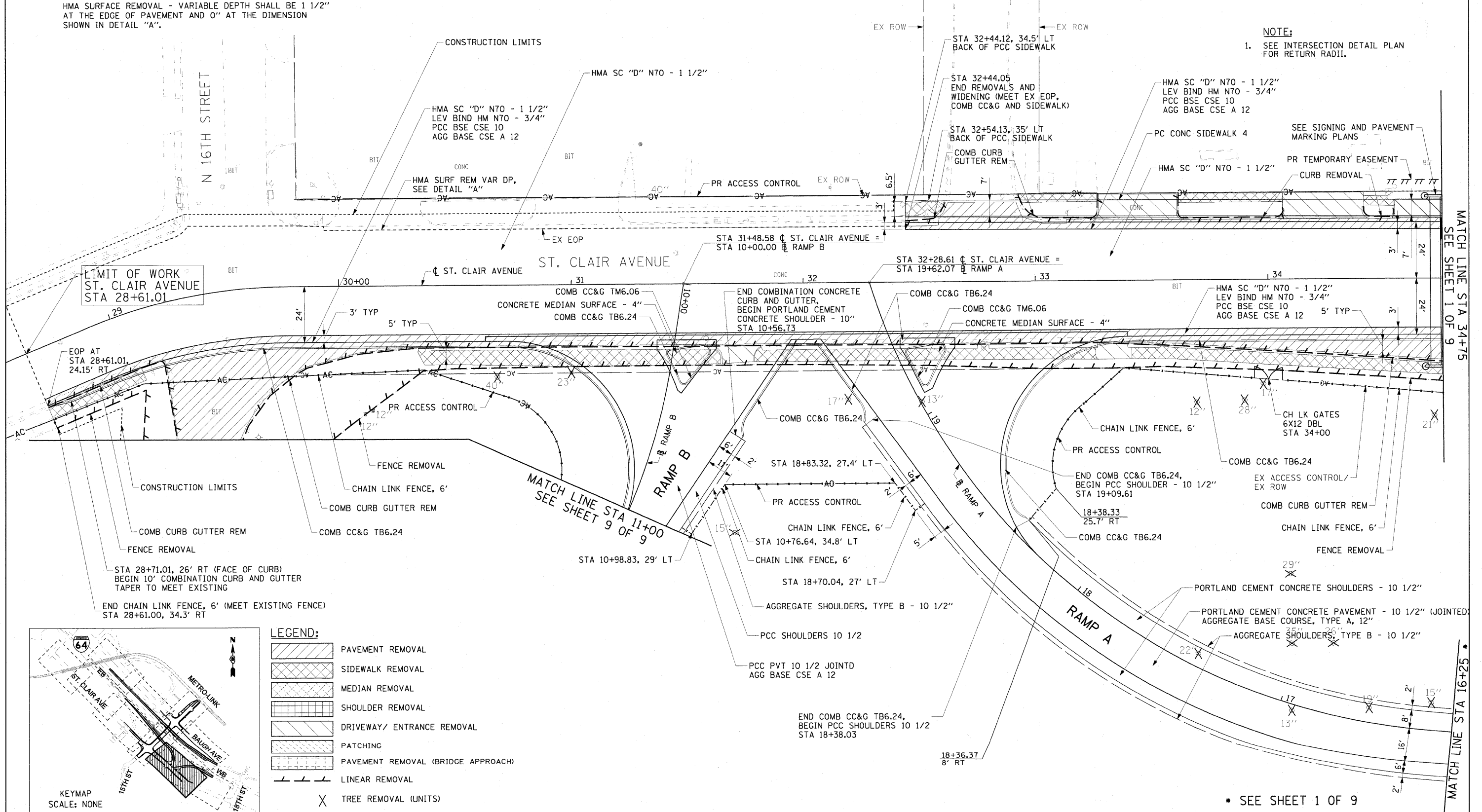
	PAVEMENT REMOVAL
	SIDEWALK REMOVAL
	MEDIAN REMOVAL
	SHOULDER REMOVAL
	DRIVEWAY/ ENTRANCE REMOVAL
	PATCHING
	PAVEMENT REMOVAL (BRIDGE APPROACH)
	LINEAR REMOVAL
	TREE REMOVAL (UNITS)



FILE NAME = #FILE#	USER NAME = pmsarno	DESIGNED OP	REVISOR -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN - RAMP E</b>			F.A.I. RTE. 64	SECTION 82-1-2HB	COUNTY ST. CLAIR	TOTAL SHEETS 345	SHEET NO. 50
	PLOT SCALE = 20,000' / IN.	DRAWN PP	REVISOR -		SCALE: 1" = 20'	SHEET NO. 5 OF 9 SHEETS	STA. 508+60 TO STA. 105+15.26	CONTRACT NO. 76C49				
	PLOT DATE = 3/19/2010	CHECKED DBM	REVISOR -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							
		DATE 03/19/10	REVISOR -									



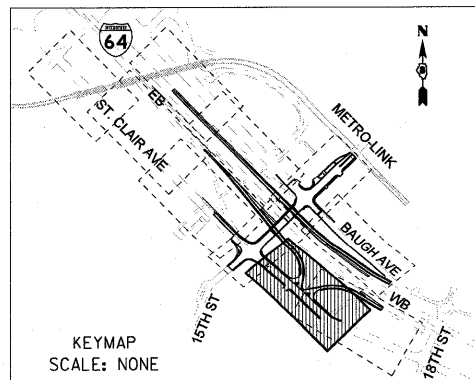
**NOTE:**  
1. SEE INTERSECTION DETAIL PLAN FOR RETURN RADII.



LIMIT OF WORK  
ST. CLAIR AVENUE  
STA 28+61.01

MATCH LINE STA 34+75  
SEE SHEET 1 OF 9

MATCH LINE STA 16+25  
SEE SHEET 1 OF 9



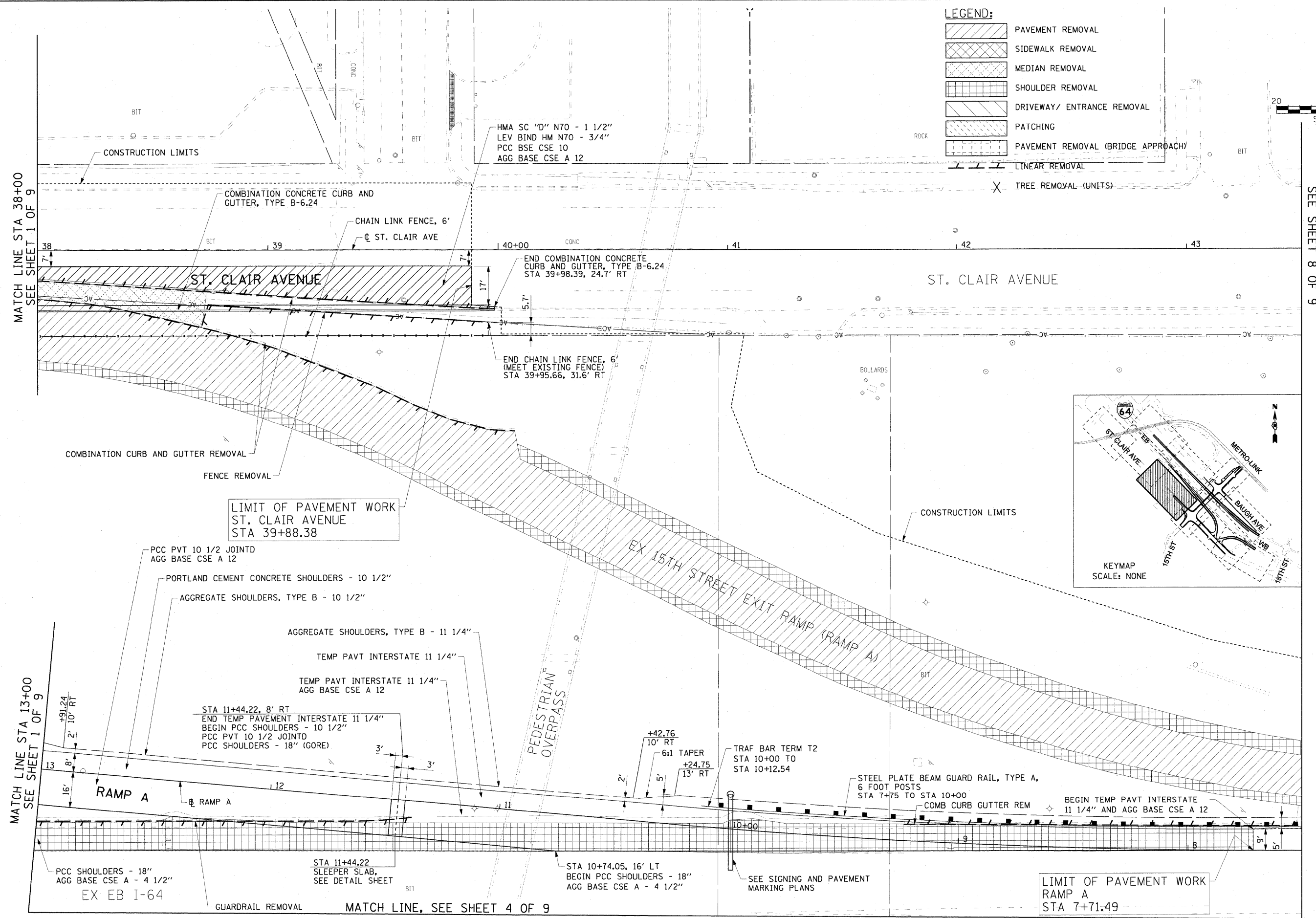
**LEGEND:**

	PAVEMENT REMOVAL
	SIDEWALK REMOVAL
	MEDIAN REMOVAL
	SHOULDER REMOVAL
	DRIVEWAY/ ENTRANCE REMOVAL
	PATCHING
	PAVEMENT REMOVAL (BRIDGE APPROACH)
	LINEAR REMOVAL
	TREE REMOVAL (UNITS)

FILE NAME =	USER NAME = pmsarno	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN - ST. CLAIR AVENUE AND RAMP A &amp; RAMP B</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
#FILEA#		DRAWN PP	REVISED -			64	82-1-2HB	ST. CLAIR	345	51	
		CHECKED DBM	REVISED -			CONTRACT NO. 76C49					
		DATE 03/19/10	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					
					SCALE: 1" = 20'		SHEET NO. 6 OF 9 SHEETS		STA. 28+61 TO STA. 35+50		



- LEGEND:**
- PAVEMENT REMOVAL
  - SIDEWALK REMOVAL
  - MEDIAN REMOVAL
  - SHOULDER REMOVAL
  - DRIVEWAY/ ENTRANCE REMOVAL
  - PATCHING
  - PAVEMENT REMOVAL (BRIDGE APPROACH)
  - LINEAR REMOVAL
  - TREE REMOVAL (UNITS)



MATCH LINE STA 38+00  
SEE SHEET 1 OF 9

MATCH LINE STA 43+50  
SEE SHEET 8 OF 9

MATCH LINE STA 13+00  
SEE SHEET 1 OF 9

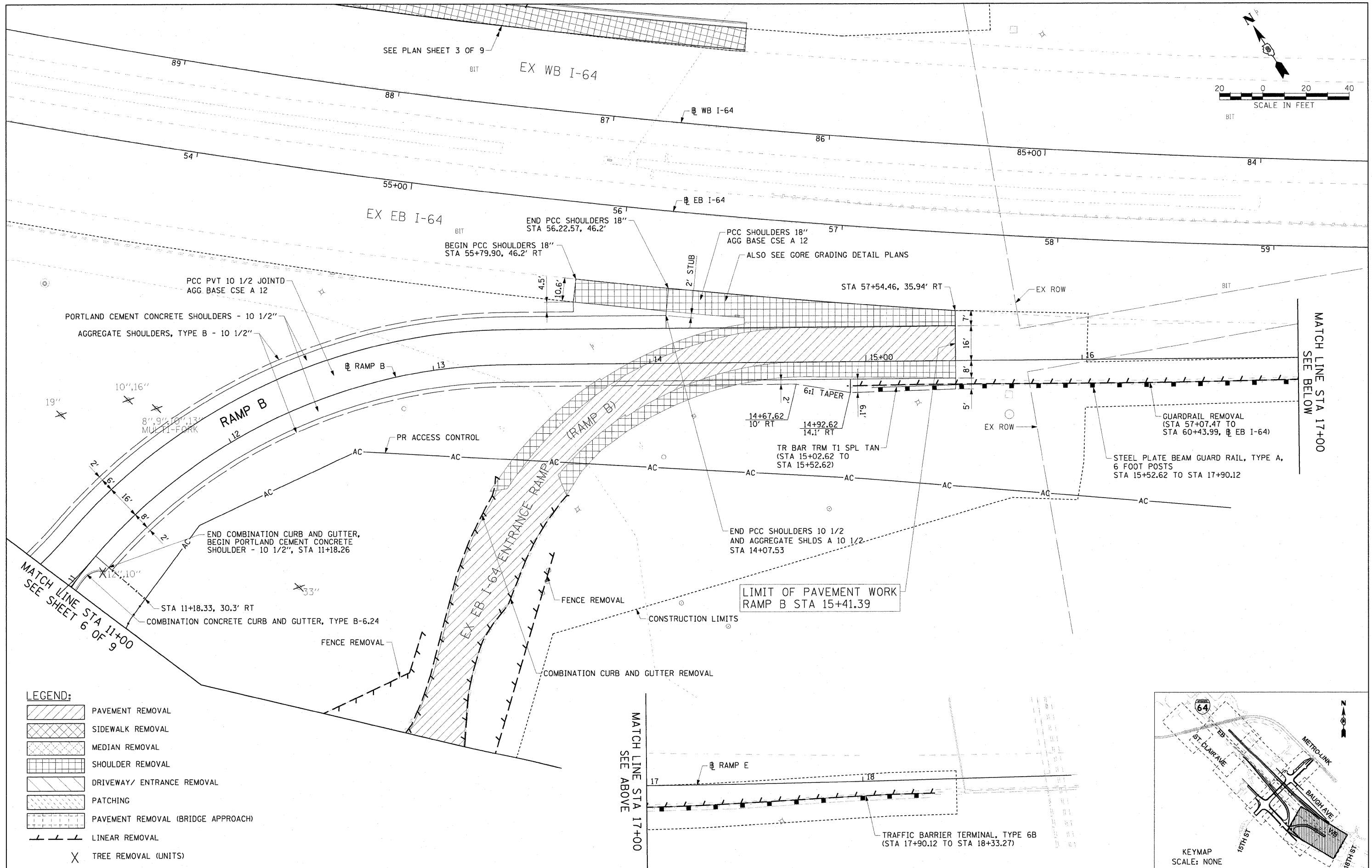
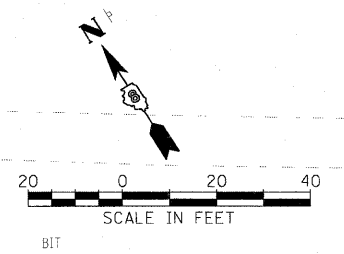
MATCH LINE, SEE SHEET 4 OF 9

LIMIT OF PAVEMENT WORK  
RAMP A  
STA 7+71.49

LIMIT OF PAVEMENT WORK  
ST. CLAIR AVENUE  
STA 39+88.38

FILE NAME = #FILEA#	USER NAME = pmsrno	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN - ST. CLAIR AVENUE</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 20,000' / IN.	CHECKED DBM	REVISED -			64	82-1-2HB	ST. CLAIR	345	52
PLOT DATE = 3/19/2010	DATE 03/19/10	REVISED -		SCALE: 1" = 20'		SHEET NO. 7 OF 9 SHEETS		STA. 7+71.49 TO STA. 13+00		CONTRACT NO. 76C49
						FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				





FILE NAME =  
#F:LEA#

USER NAME = pmsarno  
DESIGNED OP  
DRAWN PP  
CHECKED DBM  
DATE 03/19/10

REVISOR  
REVISOR  
REVISOR  
REVISOR

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN - RAMP B

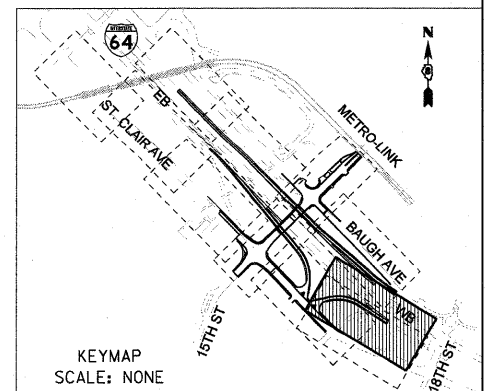
SCALE: 1" = 20'

SHEET NO. 9 OF 9 SHEETS

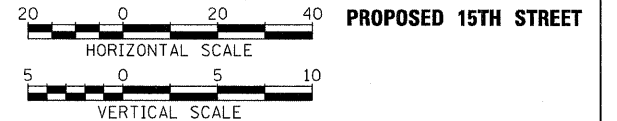
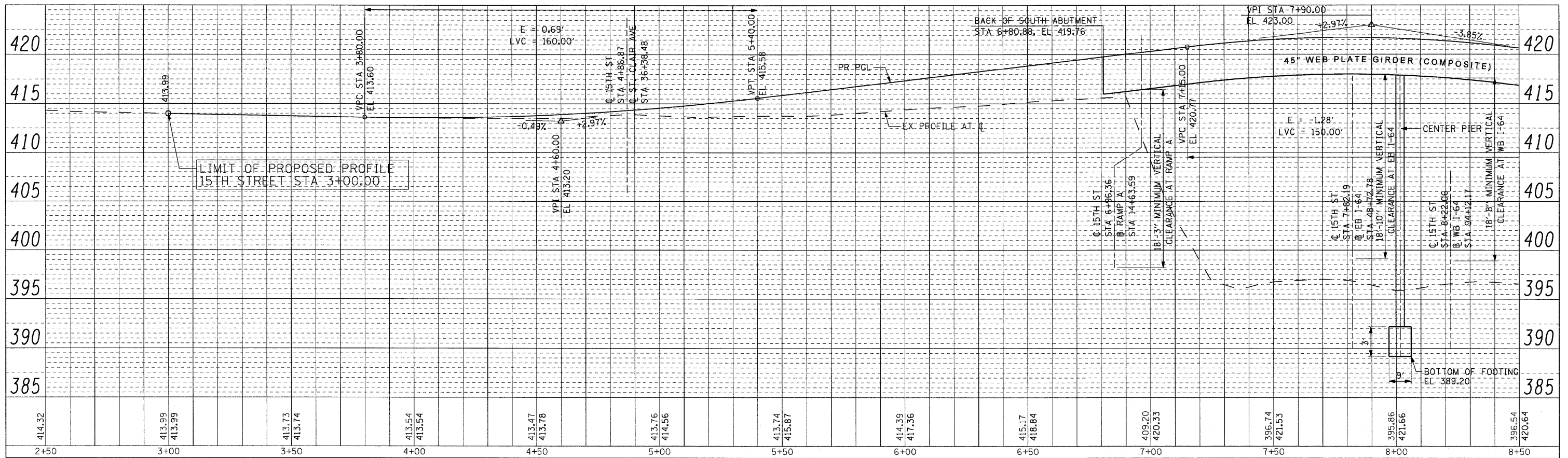
STA. 11+00 TO STA. 15+41.39

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	54

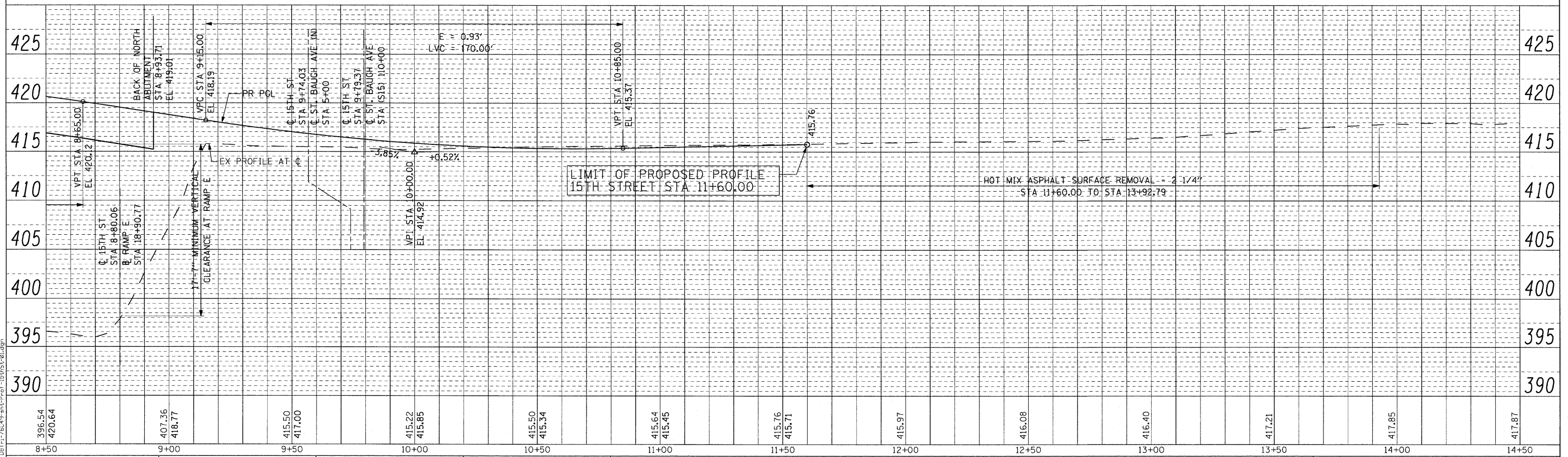
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



PLAN	REVISIONS	DATE
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	

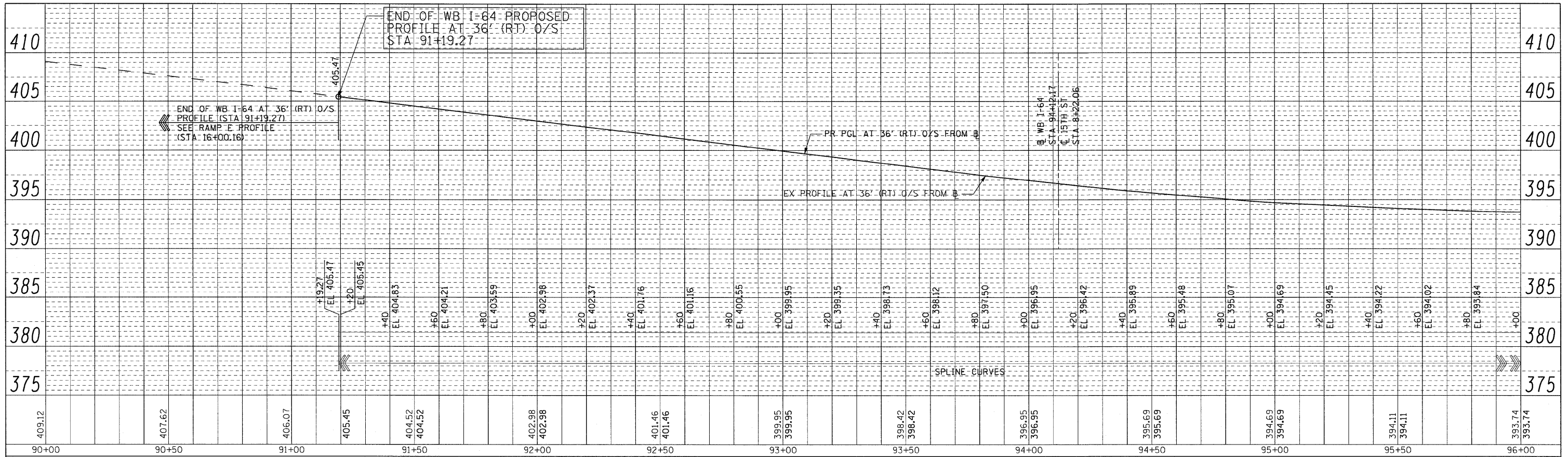


PROFILE	REVISIONS	DATE
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	

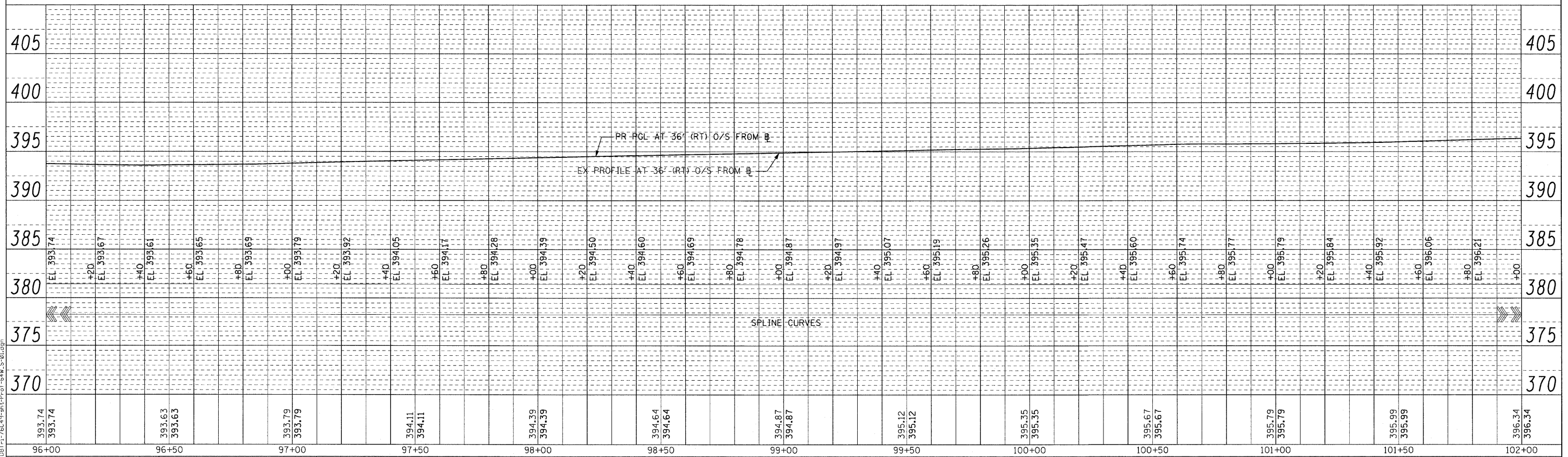


FILE NAME = DBT-76c49-pnt-Prof-15thSt-B.dgn	USER NAME = pimsarno	DESIGNED OP	REVISIONS	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b> <b>PROPOSED ROADWAY PROFILE</b> <b>15TH STREET</b>		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#	DRAWN OP	CHECKED DBM	REVISIONS			64	82-1-2HB	ST. CLAIR	345	55
PLOT SCALE = 40.0000' / IN.	DATE 03/19/10	DATE 03/19/10	REVISIONS			CONTRACT NO. 76C49				
PLOT DATE = 3/18/2010	DATE 03/19/10	REVISIONS	ILLINOIS FED. AID PROJECT							
SCALE: H: 20 V: 5 SHEET NO. 1 OF 1 SHEETS STA. 3+00 TO STA. 11+60										

PLAN	DESIGNED	BY	DATE
	NOTED		
	CHECKED		
	DATE		
	NO.		



PROFILE	DESIGNED	BY	DATE
	NOTED		
	CHECKED		
	DATE		
	NO.		



FILE NAME = DBT1-76C49-ml-P-07-64M-S-01.dgn  
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USER NAME = pimsarno	DESIGNED	OP	REVISED	-
	DRAWN	OP	REVISED	-
PLOT SCALE = 40.0000' / IN.	CHECKED	DBM	REVISED	-
PLOT DATE = 3/18/2010	DATE	03/19/10	REVISED	-

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED ROADWAY PROFILE  
WB I-64 AT 36' (RT) OFFSET**

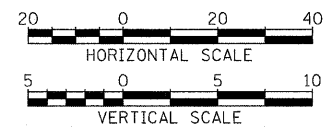
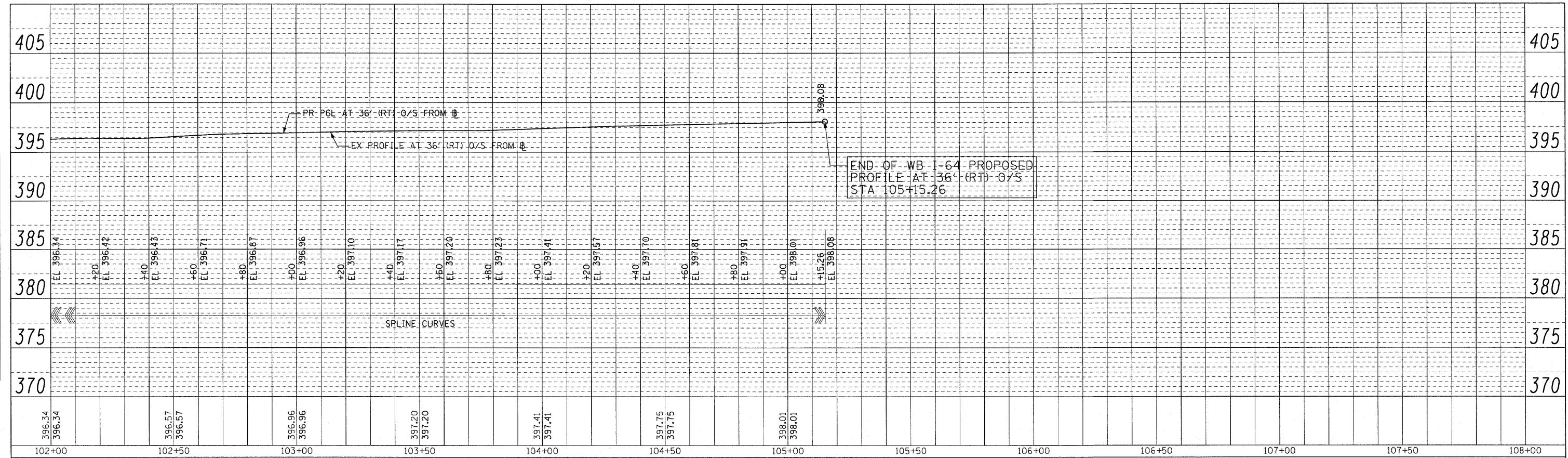
SCALE: H: 20 V: 5    SHEET NO. 1 OF 2 SHEETS    STA. 91+19.27 TO STA. 102+00

F.A.I. RTE. 64	SECTION 82-1-2HB	COUNTY ST. CLAIR	TOTAL SHEETS 345	SHEET NO. 56
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	



PLAN	REVIEWED	DATE
	ALIGNED	
	CHECKED	
	BY	
	NO.	

PROFILE	SAVED	DATE
	GRADES	
	CHECKED	
	BY	
	NO.	



**PROPOSED WB I-64  
AT 36' (RT) OFFSET**

FILE NAME = DB:\r-76c49-pnt-p-05-w-5-02.dgn  
#FILE#

USER NAME = pimerno	DESIGNED	OP	REVISD	-
	DRAWN	OP	REVISD	-
PLOT SCALE = 40.0000' / IN.	CHECKED	DBM	REVISD	-
PLOT DATE = 3/18/2010	DATE	03/19/10	REVISD	-

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

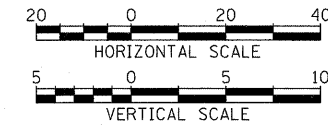
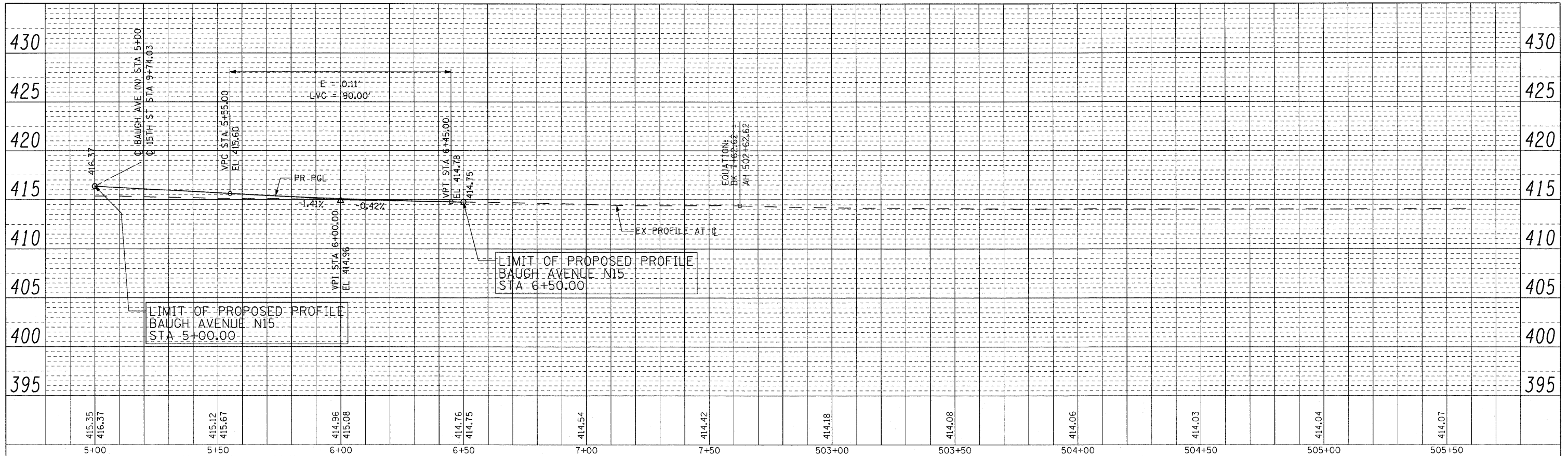
**PROPOSED ROADWAY PROFILE  
WB I-64 AT 36' (RT) OFFSET**

SCALE: H: 20 V: 5 SHEET NO. 2 OF 2 SHEETS STA. 102+00 TO STA. 105+15.26

F.A.I. RTE. 64	SECTION 82-1-2HB	COUNTY ST. CLAIR	TOTAL SHEETS 345	SHEET NO. 57
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	

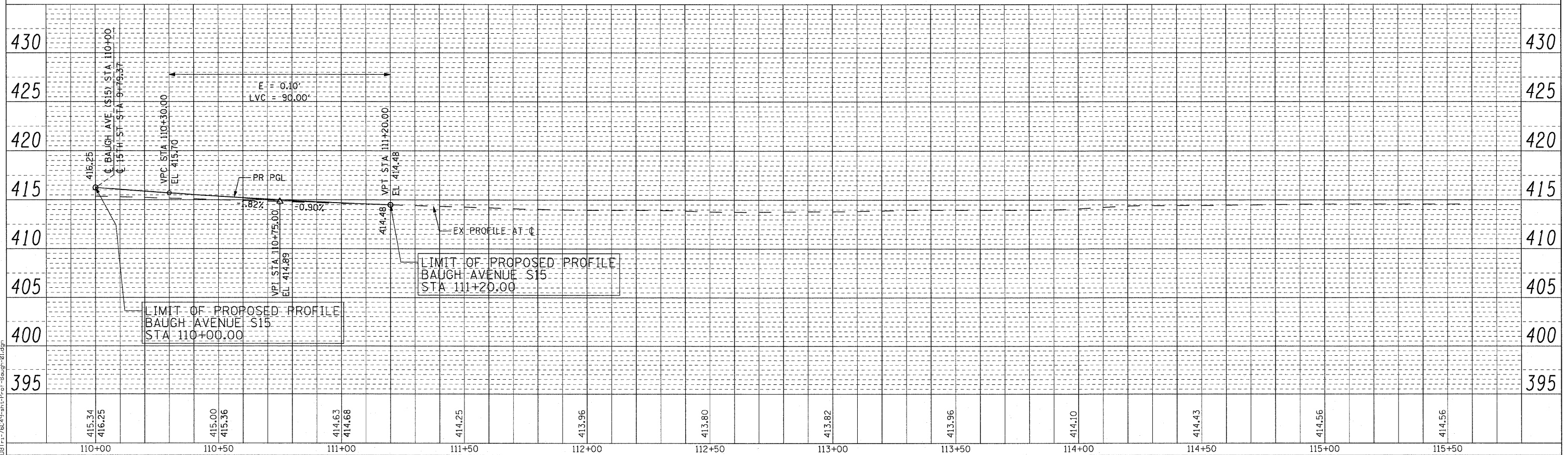


PLAN	REVISED	DATE
NO.	BY	



**PROPOSED BAUGH AVENUE (N15)**  
**PROPOSED BAUGH AVENUE (S15)**

PROFILE	REVISED	DATE
NO.	BY	



FILE NAME = DBT11-76c49-pnt-F-of-Baugh-21.dgn  
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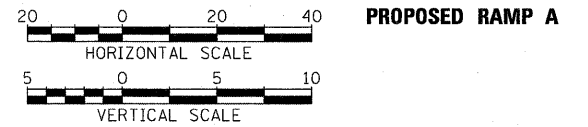
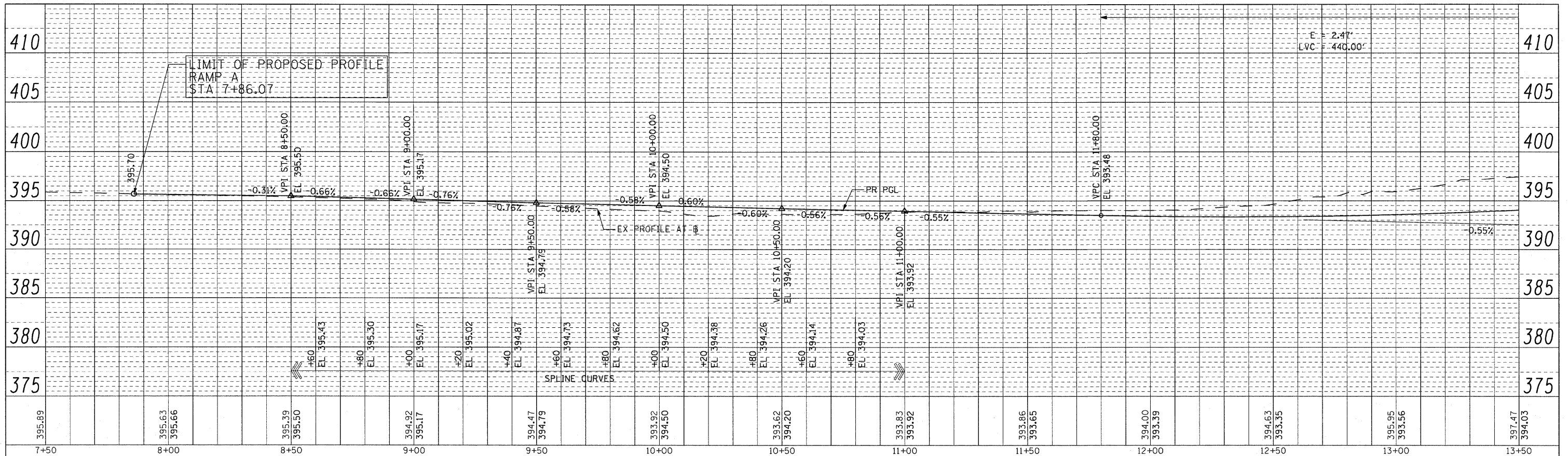
USER NAME = pmsarno	DESIGNED	OP	REVISED	-
	DRAWN	OP	REVISED	-
PLOT SCALE = 40.0000' / IN.	CHECKED	DBM	REVISED	-
PLOT DATE = 3/18/2010	DATE	03/19/10	REVISED	-

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

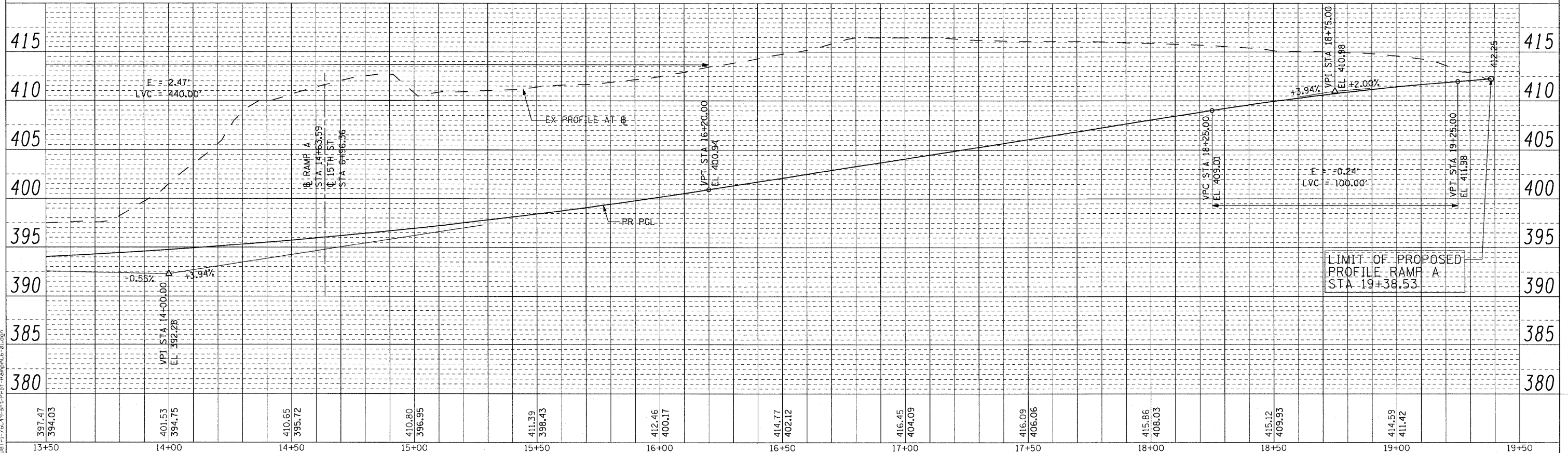
**PROPOSED ROADWAY PROFILE**  
**BAUGH AVENUE (N AND S15)**  
 SCALE: H: 20 V: 5 SHEET NO. 1 OF 1 SHEETS STA. 5+00/110+00 TO STA. 6+50/112+20

F.A.I. RTE. 64	SECTION 82-1-2HB	COUNTY ST. CLAIR	TOTAL SHEETS 345	SHEET NO. 58
CONTRACT NO. 76C49			ILLINOIS FED. AID PROJECT	

PLAN	REVISIONS	DATE
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	



PROFILE	REVISIONS	DATE
NO.	NO.	
NO.	NO.	
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NO.	NO.	

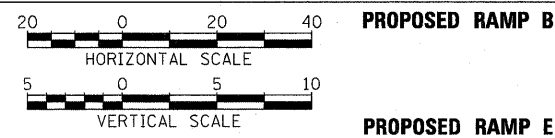
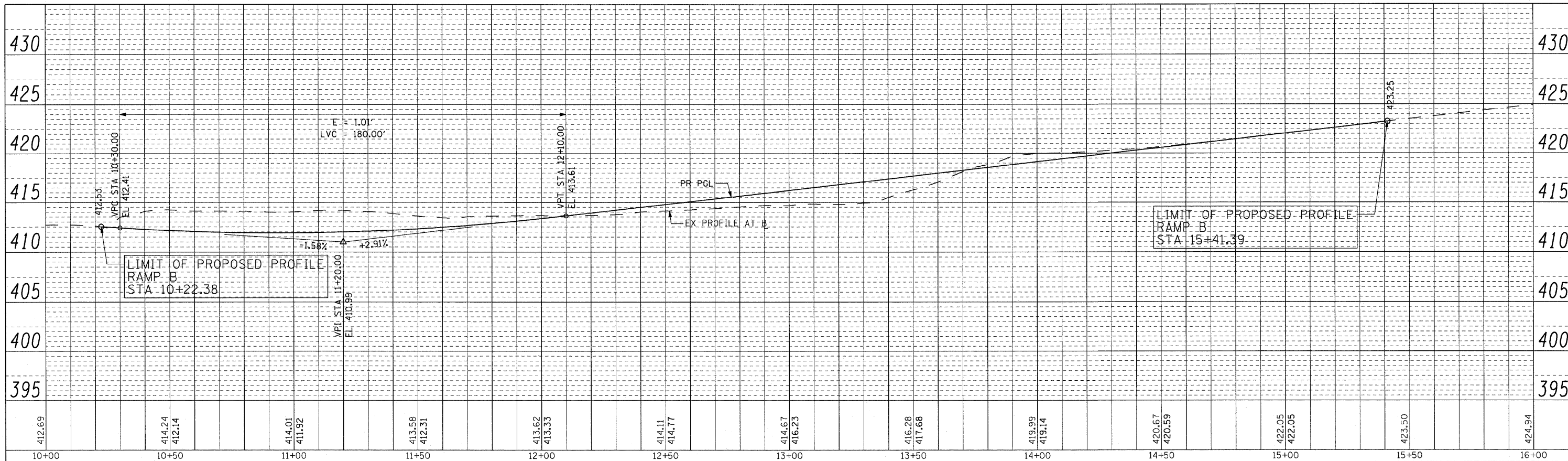


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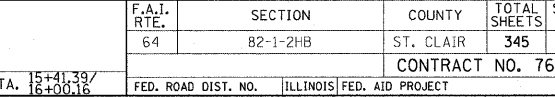
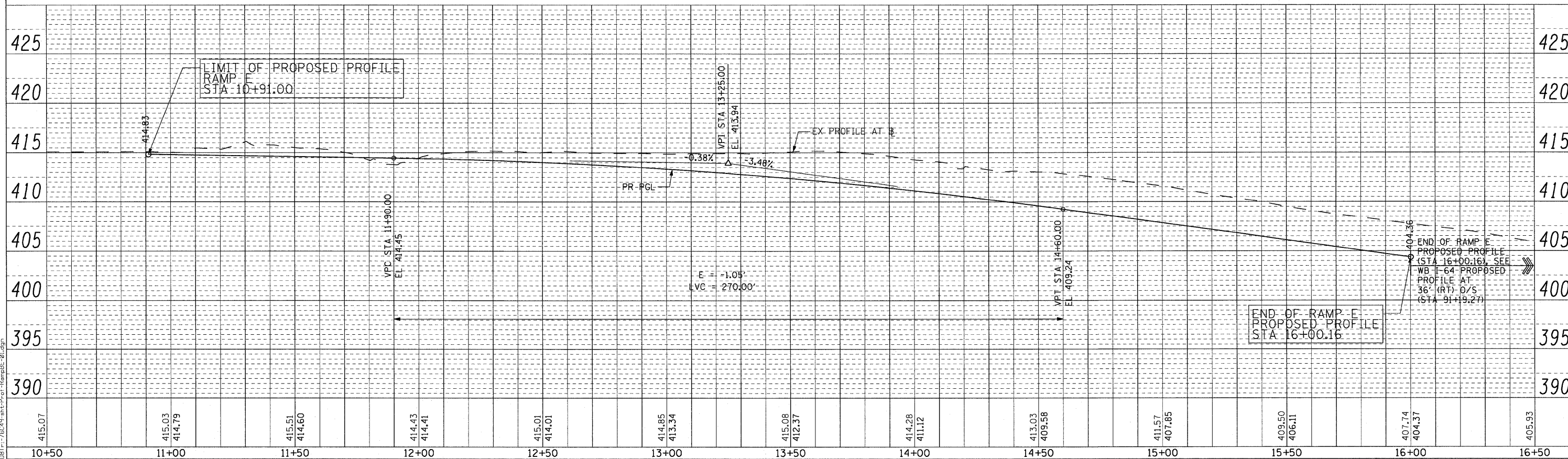
FILE NAME =	USER NAME = pmsarino	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>PROPOSED ROADWAY PROFILE</b> <b>RAMP A</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
#FILE#		DRAWN OP	REVISED -			64	82-1-2HB	ST. CLAIR	345	59	
		CHECKED DBM	REVISED -			CONTRACT NO. 76C49					
		DATE 03/19/10	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					

SCALE: H: 20 V: 5 SHEET NO. 1 OF 1 SHEETS STA. 7+86.07 TO STA. 19+38.53

PLAN	REVIEWED	DATE
	NOTED	
	ALIGNED	
	CHECKED	
	FILED	
	NO.	



PROFILE	REVIEWED	DATE
	NOTED	
	GRADES	
	CHECKED	
	FILED	
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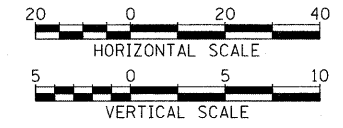
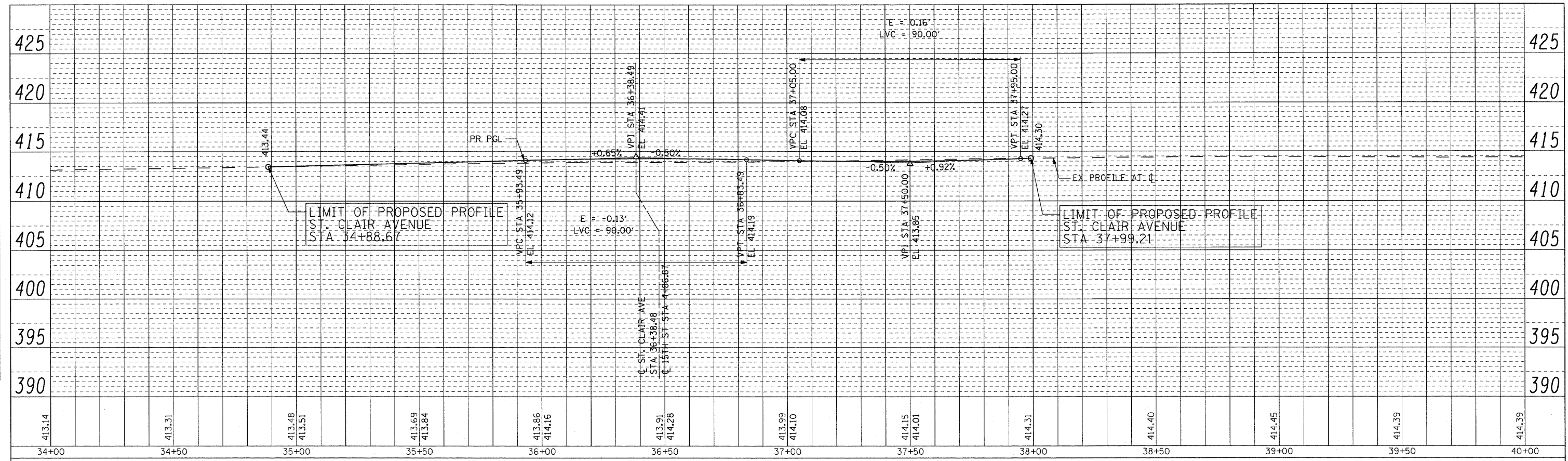


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#FILE#		DRAWN OP	REVISED -		SCALE: H: 20 V: 5	SHEET NO. 1 OF 1 SHEETS	STA. 10+22.38/10.16	TO STA. 15+41.39/16+00.16	CONTRACT NO. 76C49			
		CHECKED DBM	REVISED -		FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT							
		DATE 03/19/10	REVISED -									



PLAN	REVISIONS	DATE
NO.	PLOTTED	
	ALIGNMENT CHECKED	
	PAID FILE NAME	

PROFILE	REVISIONS	DATE
NO.	GRADES CHECKED	
	BLM. NOTED	
	STRUCTURE NOTATIONS OK'D	



PROPOSED ST. CLAIR AVENUE

FILE NAME =	USER NAME = pimsarno	DESIGNED OP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PROPOSED ROADWAY PROFILE ST. CLAIR AVENUE</b>		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*FILEA*		DRAWN OP	REVISED -		64	82-1-2HB	ST. CLAIR	345	61		
PLOT SCALE = 40.0000' / IN.		CHECKED DBM	REVISED -		SCALE: H: 20 V: 5 SHEET NO. 1 OF 1 SHEETS STA. 34+88.67 TO STA. 37+99.21		CONTRACT NO. 76C49				
PLOT DATE = 3/18/2010		DATE 03/19/10	REVISED -				FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

FILE NAME = D:\Tr-76C49-sh-Prof-St.Clair-01.dgn

**ADVANCE WORK (WEEKEND CLOSURE)**

**MAINTENANCE OF TRAFFIC**

- MAINTAIN TRAFFIC ON EXISTING ROUTES, EXCEPT I-64, WHICH IS TO BE CLOSED FOR THE 15TH STREET BRIDGE DEMOLITION. SEE "INTERSTATE 64 WEEKEND CLOSURE" SHEETS AND SPECIAL PROVISION, "INTERSTATE WEEKEND CLOSURE, SPECIAL".
- TRAFFIC UTILIZING 15TH STREET WILL BE DETOURED TO 9TH STREET AND 18TH STREET.

**WORK TO BE COMPLETED THIS STAGE**

- DEMOLISH 15TH STREET BRIDGE, PIERS, AND ABUTMENTS.

**STAGE 1**

**MAINTENANCE OF TRAFFIC**

- MAINTAIN TRAFFIC ON EXISTING ROUTES, EXCEPT FOR 15TH STREET OVER I-64. TRAFFIC UTILIZING 15TH STREET WILL BE DETOURED TO 9TH STREET AND 18TH STREET.
- I-64 LANE CLOSURES WILL BE REQUIRED FOR THE MEDIAN PIER WORK FOR THE NEW 15TH STREET BRIDGE. A MINIMUM OF TWO LANES WILL BE MAINTAINED AT ALL TIMES. LANE CLOSURES TO BE DONE PER STANDARDS 701400 AND 701401. STANDARD 701411 ALSO TO BE FOLLOWED FOR THE EASTBOUND LANES. BEAM INSTALLATION TO USE FIFTEEN MINUTE CLOSURES DURING NON-PEAK HOURS.
- LANES MAY BE REDUCED OR SHIFTED DURING CONSTRUCTION.

**WORK TO BE COMPLETED THIS STAGE**

- BUILD BRIDGE SUBSTRUCTURE AND ABUTMENTS FOR THE 15TH STREET BRIDGE.
- BUILD RAMP A AND RAMP E WHILE MAINTAINING TRAFFIC ON EXISTING RAMPS.
- BUILD PORTION OF RAMP B AND TEMPORARY BITUMINOUS PAVEMENT FOR EXISTING RAMP B WHILE MAINTAINING TRAFFIC ON EXISTING RAMP B.
- REMOVE EASTBOUND I-64 SHOULDER FROM STATION 41+84 TO STATION 48+40. REPLACE WITH RAMP A PAVEMENT AND NEW PCC SHOULDER.
- REMOVE WESTBOUND I-64 SHOULDER FROM STATION 87+42 TO STATION 100+22. REPLACE WITH RAMP E PAVEMENT AND NEW PCC SHOULDER.
- REMOVE OVERHEAD SIGN TRUSS OVER WESTBOUND I-64 AT STA 92+95.
- REMOVE SIDEWALK AND CURB & GUTTER ON THE NORTH SIDE OF ST. CLAIR AVENUE FROM STATION 30+00 TO STATION 34+50. REPLACE WITH NEW RAMP A, RAMP B, AND ST. CLAIR AVENUE PAVEMENT AND CURB & GUTTER.
- REMOVE FENCE, CURB & GUTTER, AND PAVEMENT ON SOUTH SIDE OF BAUGH AVENUE FROM STATION 110+47 TO STATION 117+37. CONSTRUCT NEW CURB FOR SOUTH SIDE OF BAUGH AVENUE AND GRADE BETWEEN RAMP E AND NEW CURB ON BAUGH AVENUE.
- BEGIN PIER AND 15TH STREET BRIDGE CONSTRUCTION.

**STAGE 2**

**MAINTENANCE OF TRAFFIC**

- MAINTAIN TRAFFIC ON EXISTING ROUTES, EXCEPT FOR 15TH STREET OVER I-64. TRAFFIC UTILIZING 15TH STREET WILL BE DETOURED TO 9TH STREET AND 18TH STREET.
- I-64 LANE CLOSURES WILL BE REQUIRED FOR THE MEDIAN PIER WORK FOR THE NEW 15TH STREET BRIDGE. A MINIMUM OF TWO LANES WILL BE MAINTAINED AT ALL TIMES. LANE CLOSURES TO BE DONE PER STANDARDS 701400 AND 701401. STANDARD 701411 ALSO TO BE FOLLOWED FOR THE EASTBOUND LANES. BEAM INSTALLATION TO USE FIFTEEN MINUTE CLOSURES DURING NON-PEAK HOURS.
- OPEN NEW RAMP A BUILT IN PREVIOUS STAGE, WITH ACCESS RESTRICTED TO EASTBOUND ST. CLAIR AVENUE.
- OPEN RAMP E BUILT IN THE PREVIOUS STAGE.
- SHIFT TRAFFIC ON EXISTING RAMP B ONTO TEMPORARY BITUMINOUS PAVEMENT BUILT IN PREVIOUS STAGE.
- LANES MAY BE REDUCED OR SHIFTED DURING CONSTRUCTION.

**STAGE 2 (CONTINUED)**

**WORK TO BE COMPLETED THIS STAGE**

- DEMOLISH EXISTING RAMP A.
- CONSTRUCT NEW CURB & GUTTER ON NORTH SIDE OF ST. CLAIR AVENUE AT EXISTING RAMP A TERMINUS, BETWEEN ST. CLAIR AVENUE STATIONS 36+63 AND 39+98.
- REMOVE AND REPLACE PAVEMENT AND CURB & GUTTER ON THE NORTH SIDE OF ST. CLAIR AVENUE BETWEEN STATION 34+50 AND STATION 39+88, WHILE MAINTAINING TRAFFIC ON ST. CLAIR AVENUE AND 15TH STREET SOUTH OF ST. CLAIR AVENUE.
- REMOVE SIDEWALK AND CURB & GUTTER ON EAST SIDE OF 15TH STREET BETWEEN STATIONS 1+75 AND 4+56, AND PLACE TEMPORARY BITUMINOUS PAVEMENT.
- REMOVE EXISTING RAMP B PAVEMENT BETWEEN RAMP B STATIONS 13+50 AND 15+41. REPLACE WITH THE REMAINING PORTION OF NEW RAMP B.
- DEMOLISH EXISTING RAMP E AND EXISTING WALL 'D'.
- REMOVE WESTBOUND I-64 SHOULDER FROM STATION 100+22 TO STATION 105+15, AND REPLACE WITH NEW PCC PAVEMENT.
- CONSTRUCT NEW CURB ON SOUTH SIDE OF BAUGH AVENUE AT EXISTING RAMP E TERMINUS, BETWEEN BAUGH AVENUE STATIONS 5+31 AND 7+51. GRADE BETWEEN NORTH SHOULDER OF NEW RAMP E AND SOUTH CURB OF BAUGH AVENUE.
- REMOVE AND REPLACE BAUGH AVENUE PAVEMENT BETWEEN STATION 6+50 AND STATION 111+20, WHILE MAINTAINING TRAFFIC ON BAUGH AVENUE AND 15TH STREET NORTH OF BAUGH AVENUE. TO MAINTAIN TRAFFIC ON BAUGH AVENUE, REMOVE PORTION OF CONCRETE ISLAND BETWEEN STATIONS 5+43 AND 6+05 AND REPLACE WITH TEMPORARY PAVEMENT.
- REMOVE AND REPLACE 15TH STREET PAVEMENT, SIDEWALK, AND CURB & GUTTER ON THE EAST SIDE OF 15TH STREET BETWEEN STATION 10+39 AND STATION 11+60. RESURFACE 15TH STREET ON THE EAST SIDE FROM STA 11+60 TO STA 13+93.
- CONTINUE PIER AND 15TH STREET BRIDGE CONSTRUCTION.

**STAGE 3**

**MAINTENANCE OF TRAFFIC**

- MAINTAIN TRAFFIC ON EXISTING ROUTES, EXCEPT FOR 15TH STREET OVER I-64. TRAFFIC UTILIZING 15TH STREET WILL BE DETOURED TO 9TH STREET AND 18TH STREET.
- I-64 LANE CLOSURES WILL BE REQUIRED FOR THE MEDIAN PIER WORK FOR THE NEW 15TH STREET BRIDGE. A MINIMUM OF TWO LANES WILL BE MAINTAINED AT ALL TIMES. LANE CLOSURES TO BE DONE PER STANDARDS 701400 AND 701401. STANDARD 701411 ALSO TO BE FOLLOWED FOR THE EASTBOUND LANES. BEAM INSTALLATION TO USE FIFTEEN MINUTE CLOSURES DURING NON-PEAK HOURS.
- OPEN NEW RAMP B COMPLETED IN PREVIOUS STAGE
- SHIFT NORTHBOUND 15TH STREET TRAFFIC SOUTH OF ST. CLAIR AVENUE ONTO TEMPORARY BITUMINOUS PAVEMENT BUILT IN PREVIOUS STAGE.
- LANES MAY BE REDUCED OR SHIFTED DURING CONSTRUCTION.

**WORK TO BE COMPLETED THIS STAGE**

- REMOVE AND REPLACE PAVEMENT, SIDEWALK, AND CURB & GUTTER ON THE WEST SIDE OF 15TH STREET BETWEEN STATIONS 3+00 AND 4+63.
- REMOVE AND REPLACE SECTIONS OF 15TH STREET PAVEMENT BETWEEN STATIONS 9+83 AND 11+60. CONTINUE RESURFACE SECTIONS OF 15TH STREET FROM STA 11+60 TO STA 13+93.
- REMOVE AND REPLACE SIDEWALK AND CURB & GUTTER ON SOUTH SIDE OF ST. CLAIR AVENUE BETWEEN STATIONS 29+79 AND 35+11.
- REMOVE TEMPORARY BITUMINOUS PAVEMENT PLACED FOR RAMP B.
- COMPLETE PIER AND 15TH STREET BRIDGE CONSTRUCTION.

**STAGE 4**

**MAINTENANCE OF TRAFFIC**

- MAINTAIN TRAFFIC ON EXISTING ROUTES.
- OPEN NEW 15TH STREET BRIDGE COMPLETED IN PREVIOUS STAGE.
- LANES MAY BE REDUCED OR SHIFTED DURING CONSTRUCTION.

**WORK TO BE COMPLETED THIS STAGE**

- REMOVE AND REPLACE PAVEMENT, SIDEWALK, AND CURB & GUTTER ON THE NORTH SIDE OF BAUGH AVENUE BETWEEN STATIONS 110+08 TO 111+20. REMOVE AND REPLACE SIDEWALK ON THE NORTH SIDE OF BAUGH AVENUE BETWEEN STATIONS 111+20 TO 112+27.
- REMOVE SECTIONS OF 15TH STREET PAVEMENT BETWEEN STATIONS 3+00 AND 4+87.

**STAGE 5A**

**MAINTENANCE OF TRAFFIC**

- MAINTAIN TRAFFIC ON EXISTING ROUTES.
- LANES MAY BE REDUCED OR SHIFTED DURING CONSTRUCTION.

**WORK TO BE COMPLETED THIS STAGE**

- REMOVE TEMPORARY BITUMINOUS PAVEMENT ON EAST SIDE OF 15TH STREET BETWEEN STATIONS 1+75 AND 4+56, PLACED DURING STAGE 2. REPLACE WITH NEW SIDEWALK.
- REMOVE AND REPLACE PAVEMENT, SIDEWALK, AND CURB & GUTTER ON SOUTH SIDE OF ST. CLAIR AVENUE BETWEEN STATIONS 34+89 AND 36+19.
- REMOVE AND REPLACE PAVEMENT, SIDEWALK, AND CURB & GUTTER ON SOUTH SIDE OF ST. CLAIR AVENUE BETWEEN STATIONS 36+77 AND 37+99.
- REMOVE AND REPLACE PAVEMENT, SIDEWALK, AND CURB & GUTTER ON WEST SIDE OF 15TH STREET BETWEEN STATIONS 9+83 AND 11+60. COMPLETE ANY REMAINING RESURFACING WORK ON 15TH STREET BETWEEN STA 11+60 TO STA 13+93.

**STAGE 5B (NO DRAWINGS FOR THIS STAGE)**














**MAINTENANCE OF TRAFFIC**

- MAINTAIN TRAFFIC ON EXISTING ROUTES.
- LANES MAY BE REDUCED OR SHIFTED DURING CONSTRUCTION.

**WORK TO BE COMPLETED THIS STAGE**

- BETWEEN 15TH STREET AND 18TH STREET, PLACE PERMANENT PAVEMENT MARKINGS AND SIGNAGE ON ST. CLAIR AVENUE NECESSARY TO CONVERT THE ROAD TO TWO-WAY OPERATIONS. REMOVE ACCESS RESTRICTIONS TO RAMP A AND RAMP B PLACED DURING EARLIER STAGE.
- PLACE ALL OTHER PERMANENT PAVEMENT MARKINGS AND SIGNAGE NECESSARY FOR MAINTENANCE OF TRAFFIC ON IMPROVED ROUTES.

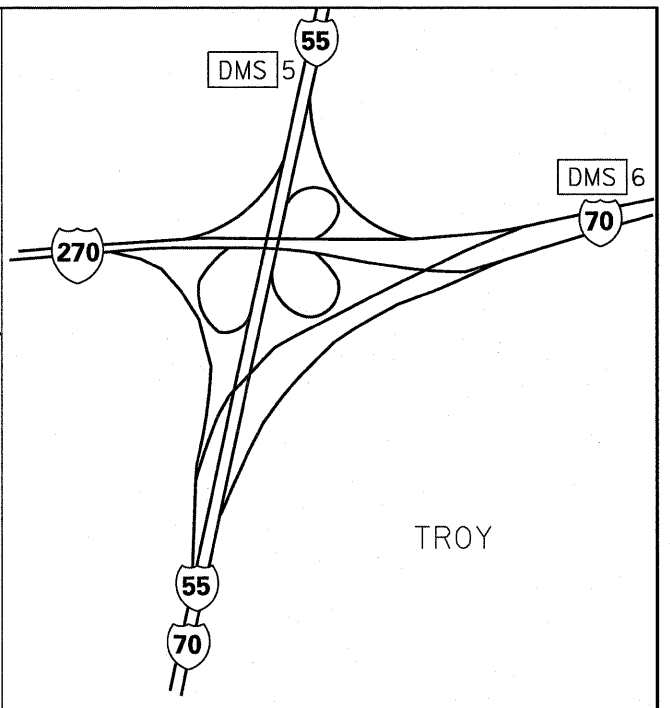
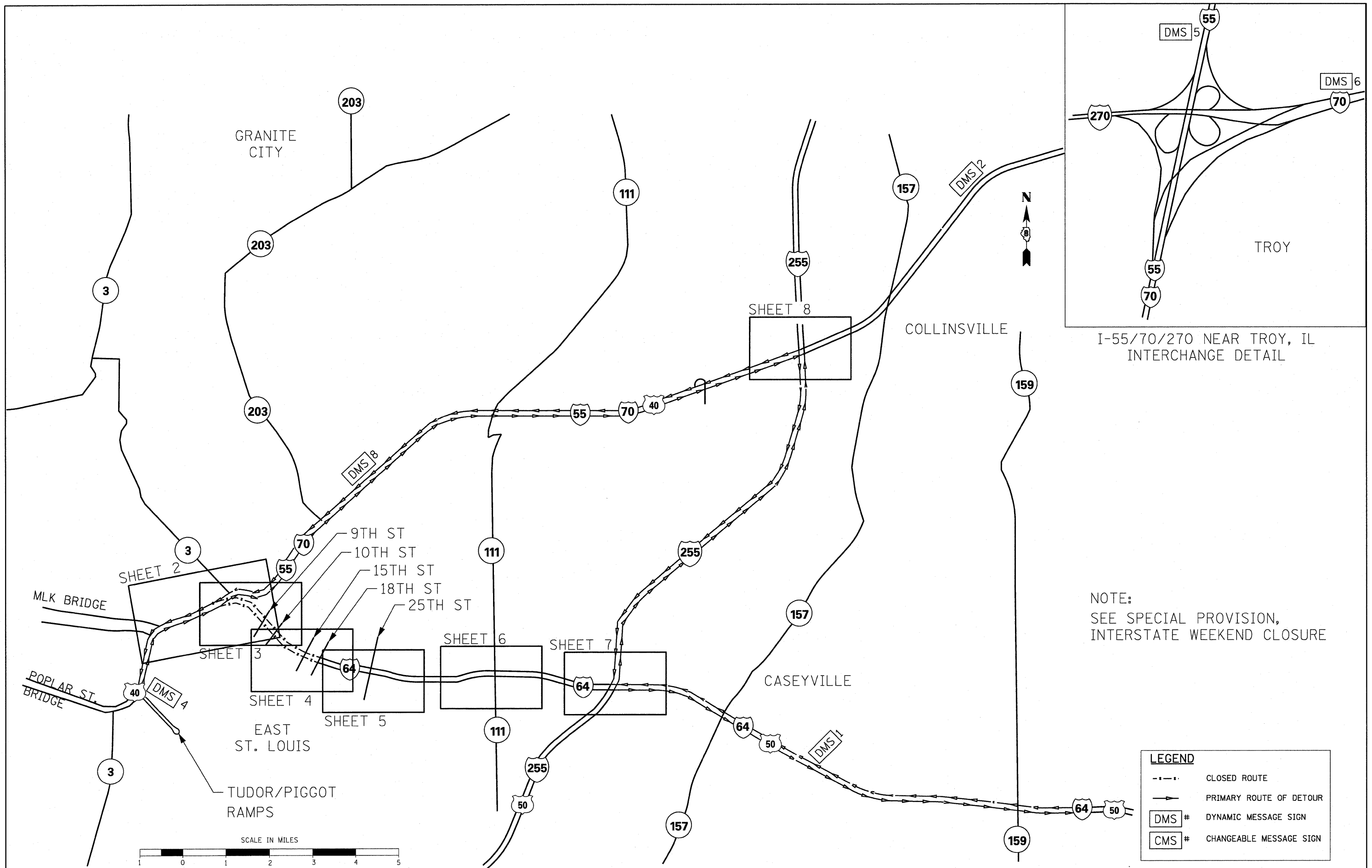
**LEGEND**

-  WORK AREA
-  REMOVAL ITEMS OUTSIDE WORK AREA
-  TEMPORARY PAVEMENT
-  SIGN
-  DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURNING LIGHTS
-  TYPE II BARRICADES WITH STEADY BURNING LIGHTS
-  TYPE III BARRICADE
-  TYPE I BARRICADE
-  TEMPORARY IMPACT ATTENUATOR (FULLY REDIRECTIVE)
-  TEMPORARY CONCRETE BARRIER
-  PROPOSED PAVEMENT MARKING
-  EXISTING PAVEMENT MARKING
-  DIRECTIONAL ARROW (FOR INFORMATIONAL PURPOSES ONLY, DO NOT PAINT ON ROADWAY)

**GENERAL NOTES:**

- TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 701 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- DUE THE SKEWED NATURE OF THE ROADWAYS IN THIS PROJECT, THE FOLLOWINGS ARE THE COMPASS DESIGNATIONS TO THE ROADWAYS:  
  
15TH STREET (NORTH-SOUTH)  
BAUGH AVENUE (EAST-WEST)  
ST. CLAIR AVENUE (EAST-WEST)  
WB I-64 (WEST)
- TEMPORARY ACCESS TO ALL ENTRANCES (COMMERCIAL AND PRIVATE) SHALL BE MAINTAINED DURING CONSTRUCTION USING AGGREGATE SURFACE COURSE, TYPE B. ALSO SEE GENERAL NOTES/COMMITMENT SHEET FOR ESTIMATED QUANTITY.
- TYPE III BARRICADES AND ROAD CLOSURE SIGNS SHALL BE POSITIONED AS SHOWN, ACCORDING TO HIGHWAY STANDARD 701901 AND AS DIRECTED BY THE ENGINEER.
- TYPE A LOW INTENSITY FLASHING WARNING LIGHTS SHALL BE USED ON EACH SIGN IN ADVANCE OF THE WORK DURING HOURS OF DARKNESS.
- ALL WARNING SIGNS SHALL BE A MINIMUM OF 48" x 48" AND HAVE A BLACK LEGEND AND BORDER ON A FLUORESCENT ORANGE REFLECTORIZED BACKGROUND.
- ALL TYPE III BARRICADES UTILIZED FOR ROAD CLOSURES SHALL HAVE A LOW INTENSITY FLASHING LIGHT MOUNTED ON TOP OF EACH BARRICADE.
- EXACT LOCATION OF ALL WARNING SIGNS AND BARRICADES SHALL BE STAKED IN THE FIELD FOR APPROVAL BY THE ENGINEER PRIOR TO INSTALLATION.
- PRIOR TO START OF CONSTRUCTION, ALL REQUIRED TRAFFIC CONTROL DEVICES SHALL BE IN PLACE.
- CONSTRUCTION STAGES SHOWN HEREIN ARE BASED ON THE ASSUMPTION THAT EXCAVATION REQUIRED FOR REMOVAL OF THE EXISTING NORTH BRIDGE ABUTMENT SHALL NOT IMPACT THE INTERSECTION OF BAUGH AVENUE AND 15TH STREET. CONTRACTOR SHALL REFER TO TEMPORARY SHEET PILING PLAN FOR SPECIFIC CONSTRUCTION METHODS REQUIRED TO KEEP THE SAID INTERSECTION OPEN TO TRAFFIC.
- IN AREAS WHERE A MINIMUM OFFSET OF 2 FEET FROM THE TRAVEL LANE TO THE FACE OF TEMPORARY CONCRETE BARRIER CAN NOT BE ACHIEVED, THE BARRIER SHALL BE ANCHORED TO THE PAVEMENT. ANCHORS SHALL ALSO BE PROVIDED WHERE CONSTRUCTION ACTIVITY PLACES A TRAFFIC HAZARD WITHIN 3.5 FEET OF THE TEMPORARY CONCRETE BARRIER. NO ADDITIONAL PAYMENT SHALL BE MADE FOR ANCHORING OF TEMPORARY CONCRETE BARRIER.
- A MINIMUM 11' LANE WIDTH SHALL BE MAINTAINED ON ALL INTERSTATE LANES OPEN TO TRAFFIC DURING CONSTRUCTION.

FILE NAME = #FILE#	USER NAME = IDOT	DESIGNED - GDO	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL - GENERAL NOTES</b>				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - GDO	REVISED -		SCALE: N/A	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	64	82-1-2HB	ST. CLAIR	345	62
		CHECKED - TTB	REVISED -										
		DATE - 3/19/2010	REVISED -						FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



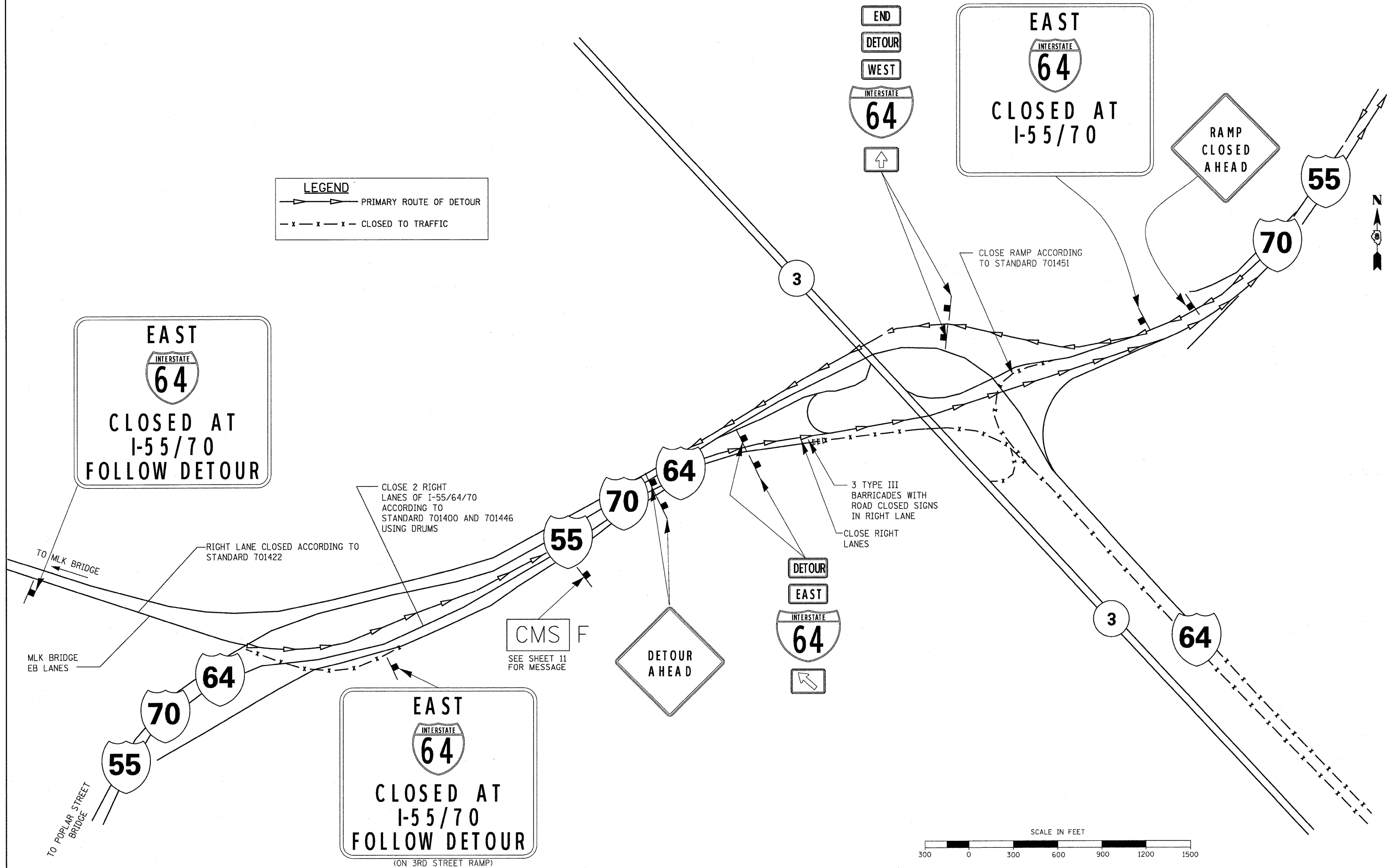
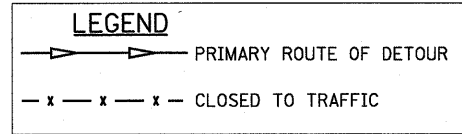
I-55/70/270 NEAR TROY, IL INTERCHANGE DETAIL

NOTE:  
SEE SPECIAL PROVISION,  
INTERSTATE WEEKEND CLOSURE

LEGEND			
- - -	CLOSED ROUTE		
→	PRIMARY ROUTE OF DETOUR		
DMS #	DYNAMIC MESSAGE SIGN		
CMS #	CHANGEABLE MESSAGE SIGN		

FILE NAME = D80overall-00-shr-staging01-64.dgn	USER NAME = osantlog	DESIGNED - DS	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>INTERSTATE 64 WEEKEND CLOSURE DETOUR OVERVIEW</b>		F.A.I. RTE. 64	SECTION 82-1-2HB	COUNTY ST. CLAIR	TOTAL SHEETS 345	SHEET NO. 63
	PLOT SCALE = 10561.6002' / in.	DRAWN - AS	REVISED -		SCALE: 1" = 1 MILE	SHEET NO. 1 OF 13 SHEETS	STA.			CONTRACT NO. 76C49	
	PLOT DATE = 3/19/2010	CHECKED -	REVISED -				TO STA.				
		DATE - 03/19/2010	REVISED -							ILLINOIS FED. AID PROJECT	





FILE NAME = DB0verall-00-sht-staging02-64.dgn

USER NAME = esontag  
 PLOT SCALE = 600.0909 ' / in.  
 PLOT DATE = 3/19/2010

DESIGNED - DS  
 DRAWN - AS  
 CHECKED -  
 DATE - 03/19/2010

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**INTERSTATE 64 WEEKEND CLOSURE  
 INTERSTATE 55/70 AND INTERSTATE 64**  
 SCALE: 1" = 300' SHEET NO. 2 OF 13 SHEETS STA. TO STA.

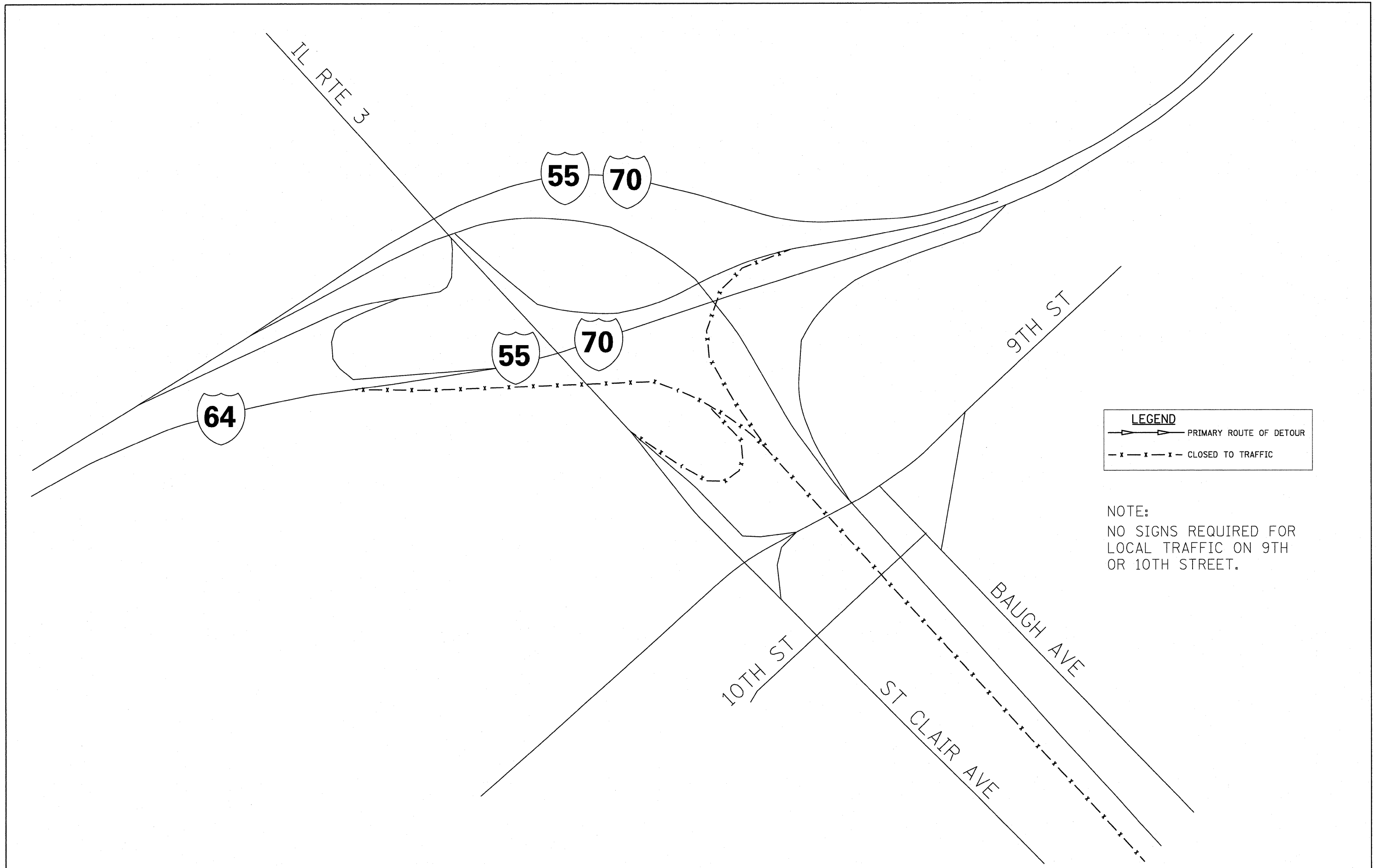
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	64
ILLINOIS FED. AID PROJECT				

CMS F  
 SEE SHEET 11 FOR MESSAGE

**EAST  
 INTERSTATE  
 64  
 CLOSED AT  
 I-55/70  
 FOLLOW DETOUR**  
 (ON 3RD STREET RAMP)

**EAST  
 INTERSTATE  
 64  
 CLOSED AT  
 I-55/70  
 FOLLOW DETOUR**

**EAST  
 INTERSTATE  
 64  
 CLOSED AT  
 I-55/70**



**LEGEND**

→ PRIMARY ROUTE OF DETOUR

- x - x - x - CLOSED TO TRAFFIC

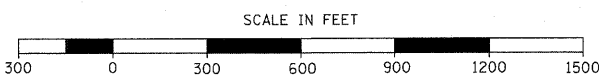
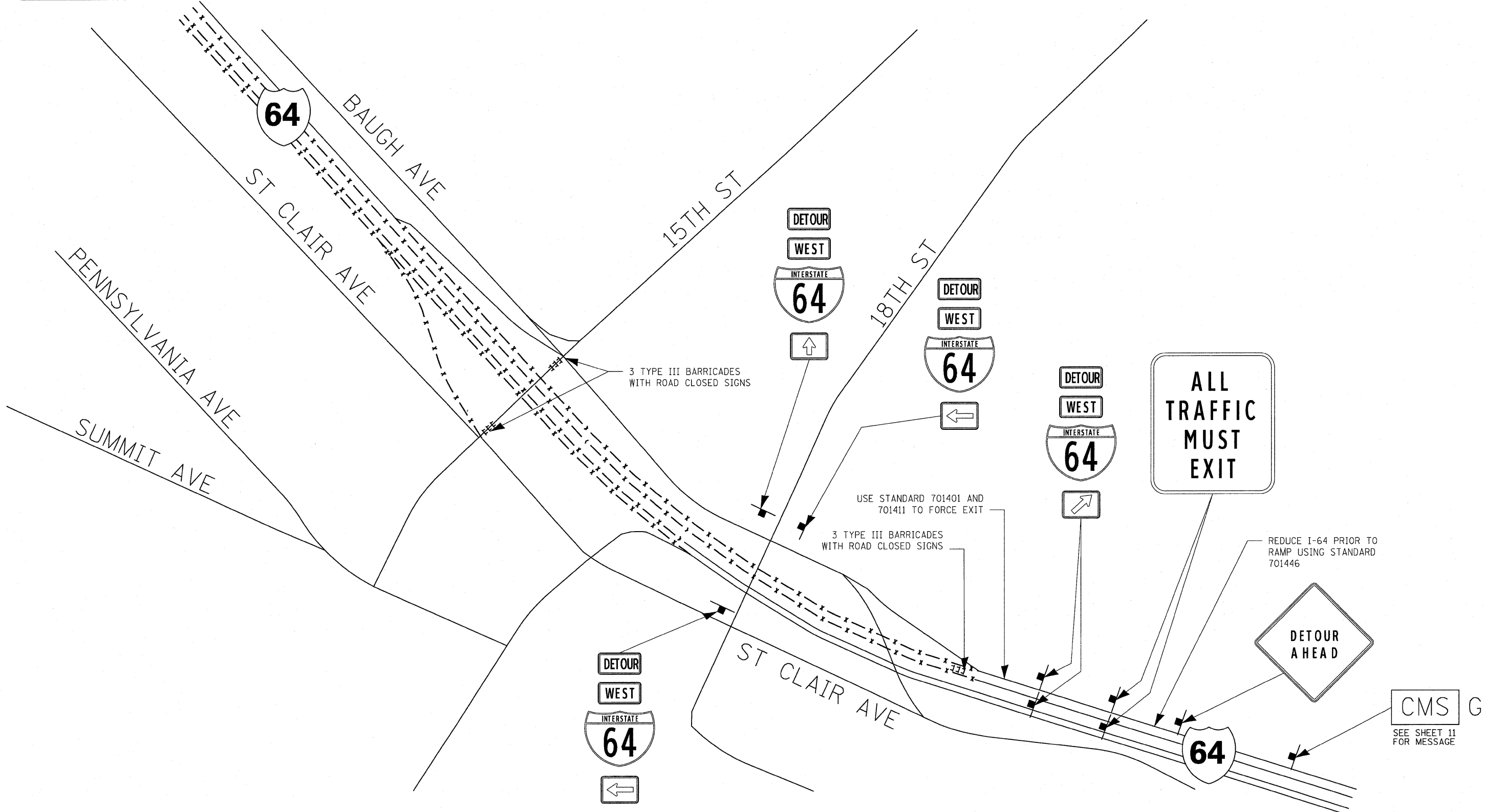
NOTE:  
NO SIGNS REQUIRED FOR  
LOCAL TRAFFIC ON 9TH  
OR 10TH STREET.

FILE NAME = D80overall-80-sht-staging03-64.dgn	USER NAME = asantlog	DESIGNED - DS	REVISD -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>INTERSTATE 55/70 WEEKEND CLOSURE INTERSTATE 64/70 AND 9TH/10TH ST.</b>			F.A.I. RTE. 64	SECTION 82-1-2HB	COUNTY ST. CLAIR	TOTAL SHEETS 345	SHEET NO. 65
	PLOT SCALE = 600.0000' / in.	DRAWN - AS	REVISD -		SCALE: 1" = 300'	SHEET NO. 3 OF 13 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			
	PLOT DATE = 3/19/2010	CHECKED -	REVISD -									
		DATE - 03/19/2010	REVISD -									

**LEGEND**

→ PRIMARY ROUTE OF DETOUR

- x - x - x - CLOSED TO TRAFFIC



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		DRAWN - AS	REVISED -
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	PLOT DATE = 3/19/2010	DATE - 03/19/2010	REVISED -

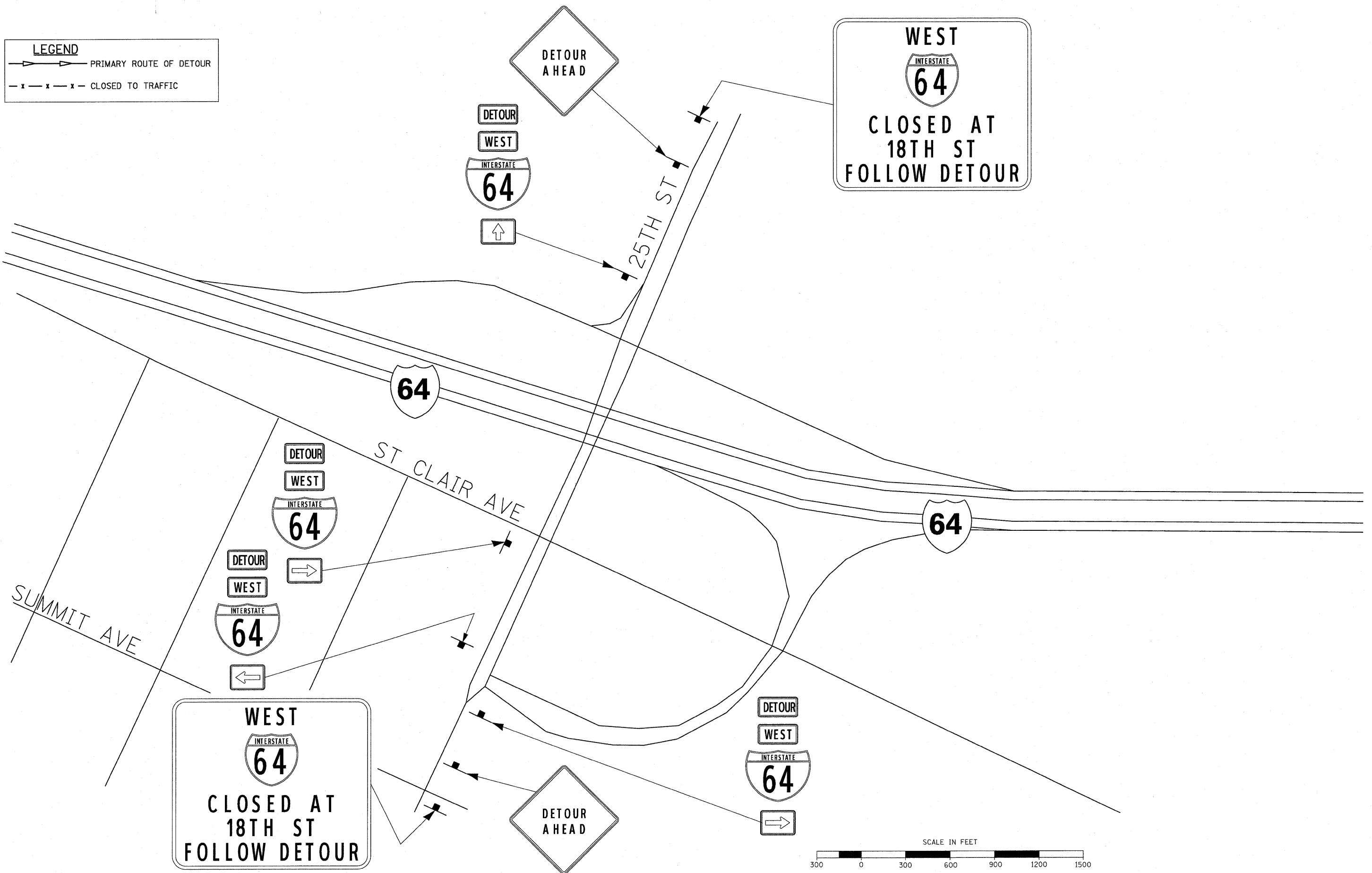
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INTERSTATE 64 WEEKEND CLOSURE  
INTERSTATE 64 AND 15TH/18TH ST.**

SCALE: 1" = 300'    SHEET NO. 4 OF 13 SHEETS    STA.    TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	66
ILLINOIS FED. AID PROJECT				

**LEGEND**  
 —▶—▶—▶— PRIMARY ROUTE OF DETOUR  
 -x-x-x- CLOSED TO TRAFFIC



FILE NAME = DBOverall-00-shrt-staging05-64.dgn	USER NAME = asentiag	DESIGNED - DS	REVISED -
		DRAWN - AS	REVISED -
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	PLOT DATE = 3/19/2010	DATE - 03/19/2010	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>INTERSTATE 64 WEEKEND CLOSURE    INTERSTATE 64 AND 25TH ST</b>			
SCALE: 1" = 300'	SHEET NO. 5 OF 13 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	67
ILLINOIS FED. AID PROJECT				

WEST  
 INTERSTATE  
**64**  
 CLOSED AT  
 18TH ST  
 FOLLOW DETOUR

DETOUR  
 AHEAD

DETOUR  
 WEST  
 INTERSTATE  
**64**  
 ↑

BUNKUM RD

111

64

DETOUR  
 WEST  
 INTERSTATE  
**64**  
 ↗

DETOUR  
 WEST  
 INTERSTATE  
**64**  
 ↗

64

111

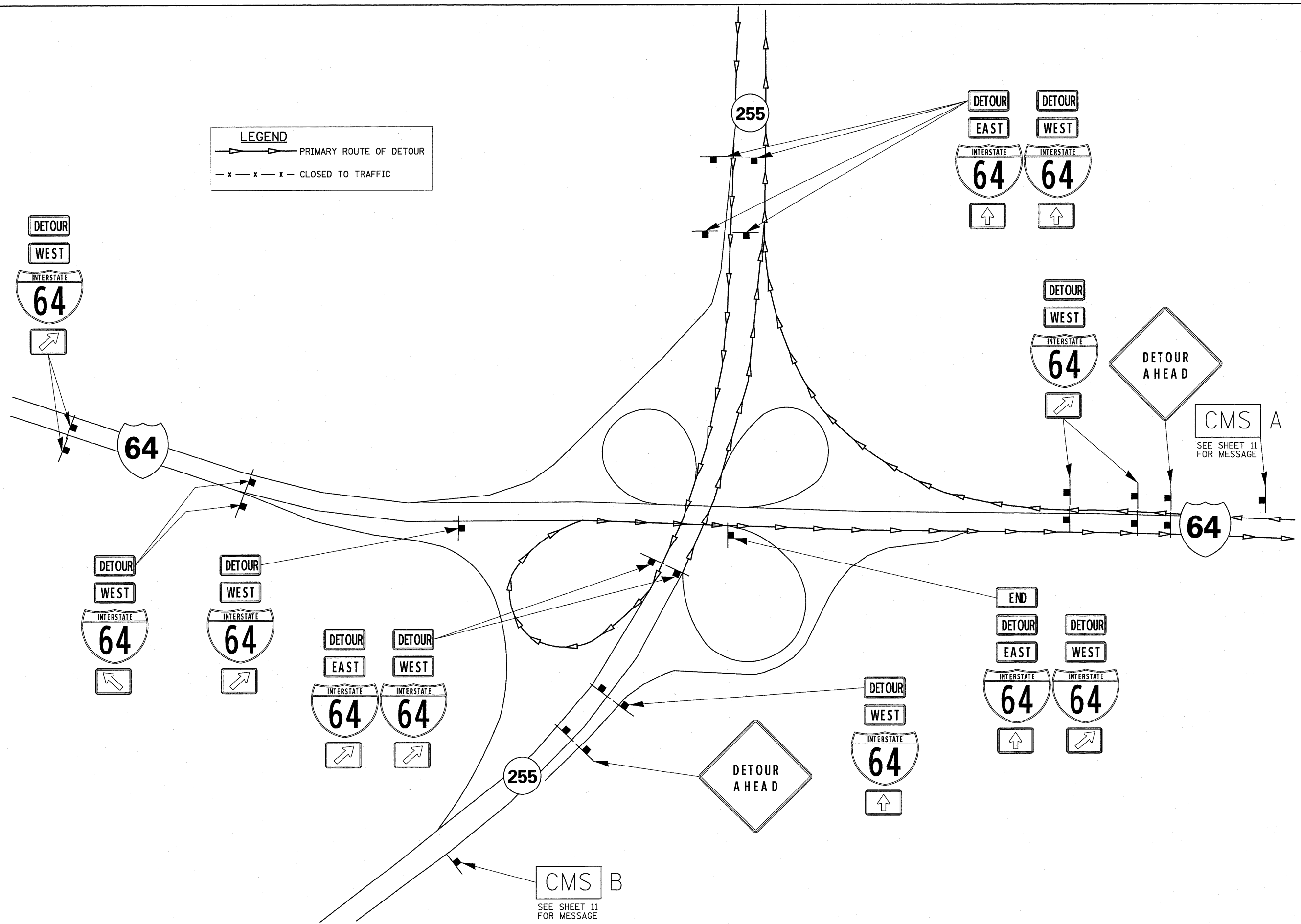
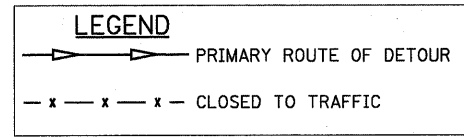
DETOUR  
 AHEAD

**LEGEND**  
 → PRIMARY ROUTE OF DETOUR  
 - x - x - x - CLOSED TO TRAFFIC

ST CLAIR AVE

WEST  
 INTERSTATE  
**64**  
 CLOSED AT  
 18TH ST  
 FOLLOW DETOUR

FILE NAME = DB0overall-80-shr-staging06-64.dgn	USER NAME = osantiog	DESIGNED - DS	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>INTERSTATE 64 WEEKEND CLOSURE INTERSTATE 64 AND ROUTE 111</b>			F.A.I. RTE. 64	SECTION 82-1-2HB	COUNTY ST. CLAIR	TOTAL SHEETS 345	SHEET NO. 68		
	PLOT SCALE = 600.0000' / in.	CHECKED -	REVISED -					ILLINOIS FED. AID PROJECT						
	PLOT DATE = 3/19/2010	DATE - 03/19/2010	REVISED -					SCALE: 1" = 300'	SHEET NO. 6 OF 13 SHEETS	STA.	TO STA.			



FILE NAME = DB0verall-00-shr-staging07-64.dgn	USER NAME = esontlog	DESIGNED - DS	REVISED -
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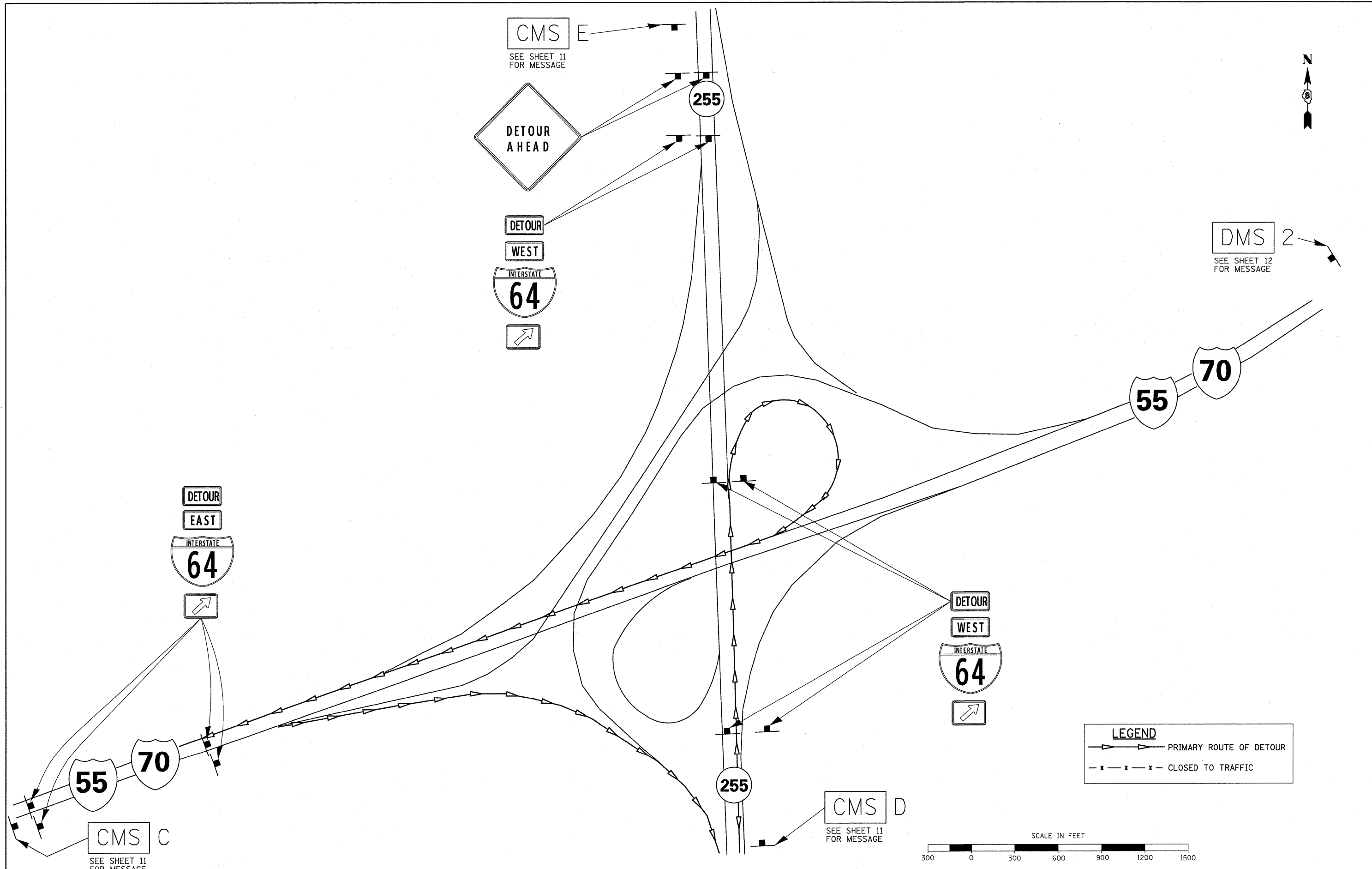
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INTERSTATE 64 WEEKEND CLOSURE  
INTERSTATE 64 AND INTERSTATE 255**

SCALE: 1" = 300'    SHEET NO. 7 OF 13 SHEETS    STA.    TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	69
ILLINOIS FED. AID PROJECT				





FILE NAME = D80overall-00-sht-staging08-64.dgn  
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 PLOT DATE = 3/19/2010

DESIGNED - DS  
 DRAWN - AS  
 CHECKED -  
 DATE - 03/19/2010

REVISED -  
 REVISED -  
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 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

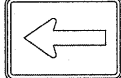
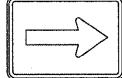
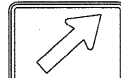








**INTERSTATE 64 WEEKEND CLOSURE  
 INTERSTATE 55/70 AND INTERSTATE 255**

SCALE: 1" = 300' SHEET NO. 8 OF 13 SHEETS STA. TO STA.




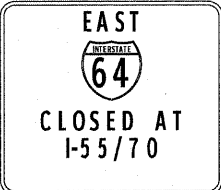
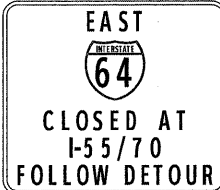




F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	70
CONTRACT NO. 76C49			ILLINOIS FED. AID PROJECT	



DETOUR SIGN SCHEDULE  
ALL SIGNS SHALL BE PROVIDED BY CONTRACTOR

											
SHEET #	ARROW LT	ARROW RT	ARROW DIAGONAL RT	ARROW DIAGONAL LT	ARROW UP	DETOUR	WEST	EAST	SOUTH	NORTH	I-64
2	-	-	-	2	2	4	2	2	-	-	4
3	-	-	-	-	-	-	-	-	-	-	-
4	2	-	2	-	1	5	5	-	-	-	5
5	1	2	-	-	1	4	4	-	-	-	4
6	-	-	2	-	1	3	3	-	-	-	3
7	-	-	12	2	11	25	18	7	-	-	25
8	-	-	10	-	-	10	6	4	-	-	10
TOTAL	3	2	26	4	16	51	38	13	-	-	51

DETOUR SIGN SCHEDULE  
ALL SIGNS SHALL BE PROVIDED BY CONTRACTOR

									
SHEET #	RAMP CLOSED AHEAD	DETOUR AHEAD	CMS	E64 CLOSED	E64 CLOSED FD	W64 CLOSED FD	ALL TRAFFIC MUST EXIT	END	ROAD CLOSED
2	1	2	1	1	2	-	-	2	3
3	-	-	-	-	-	-	-	-	-
4	-	1	1	-	-	-	2	-	9
5	-	2	-	-	-	2	-	-	-
6	-	2	-	-	-	2	-	-	-
7	-	4	2	-	-	-	-	1	-
8	-	2	3	-	-	-	-	-	-
TOTAL	1	13	7	1	2	4	2	3	12

CHANGEABLE MESSAGE SIGN MESSAGES

CMS A (I-64 WB @ I-255)

MESSAGE 1

DETOUR\_\_  
64W\_\_\_\_  
USE\_\_\_\_\_

MESSAGE 2

EXIT\_7\_\_  
TO\_\_\_\_\_  
I-255 NB

CMS F (I-55/64/70 NB @ I-64)

MESSAGE 1

64E\_\_\_\_\_  
CLOSED\_\_  
DETOUR\_\_

MESSAGE 2

REMAIN\_\_  
ON\_\_\_\_\_  
55/70N\_\_

CMS B (I-255 NB @ I-64)

MESSAGE 1

64W\_\_\_\_\_  
CLOSED\_\_  
AT\_18TH\_\_

MESSAGE 2

REMAIN\_\_  
ON\_\_\_\_\_  
255 NB\_\_

CMS G (I-64 WB @ 18TH ST)

MESSAGE 1

64W\_\_\_\_\_  
CLOSED\_\_  
AHEAD\_\_

MESSAGE 2

ALL\_\_\_\_\_  
TRAFFIC\_\_  
MUST\_EXIT

CMS C (I-55/70 EB @ I-255)

MESSAGE 1

DETOUR\_\_  
64E\_\_\_\_\_  
USE\_\_\_\_\_

MESSAGE 2

EXIT\_10\_\_  
TO\_\_\_\_\_  
I-255 SB

CMS D (I-255 NB @ I-55/70)

MESSAGE 1

DETOUR\_\_  
64W\_\_\_\_\_  
USE\_\_\_\_\_

MESSAGE 2

EXIT\_25B  
TO\_I-55/  
70\_SB\_\_

CMS E (I-255 SB @ I-55-70)

MESSAGE 1

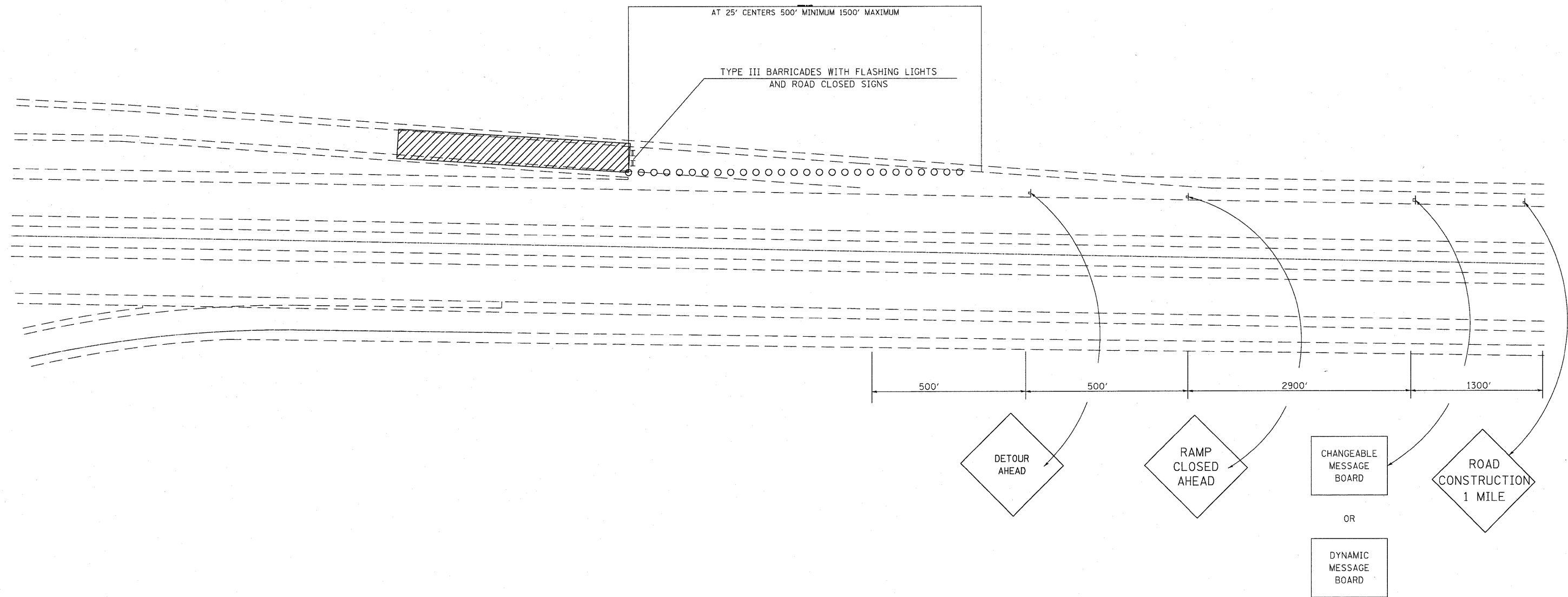
64W\_\_\_\_\_  
CLOSED\_\_  
AT\_18TH\_\_

MESSAGE 2

DETOUR\_\_  
USE\_EXIT  
25\_AHEAD

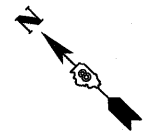
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	PLOT SCALE = 600.0000' / in.	DRAWN -	REVISED -			64	82-1-2HB	ST. CLAIR	345	73
PLOT DATE = 3/19/2010	CHECKED -	DATE 03/19/2010	REVISED -	SCALE:	SHEET NO. 11 OF 13 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 76C49



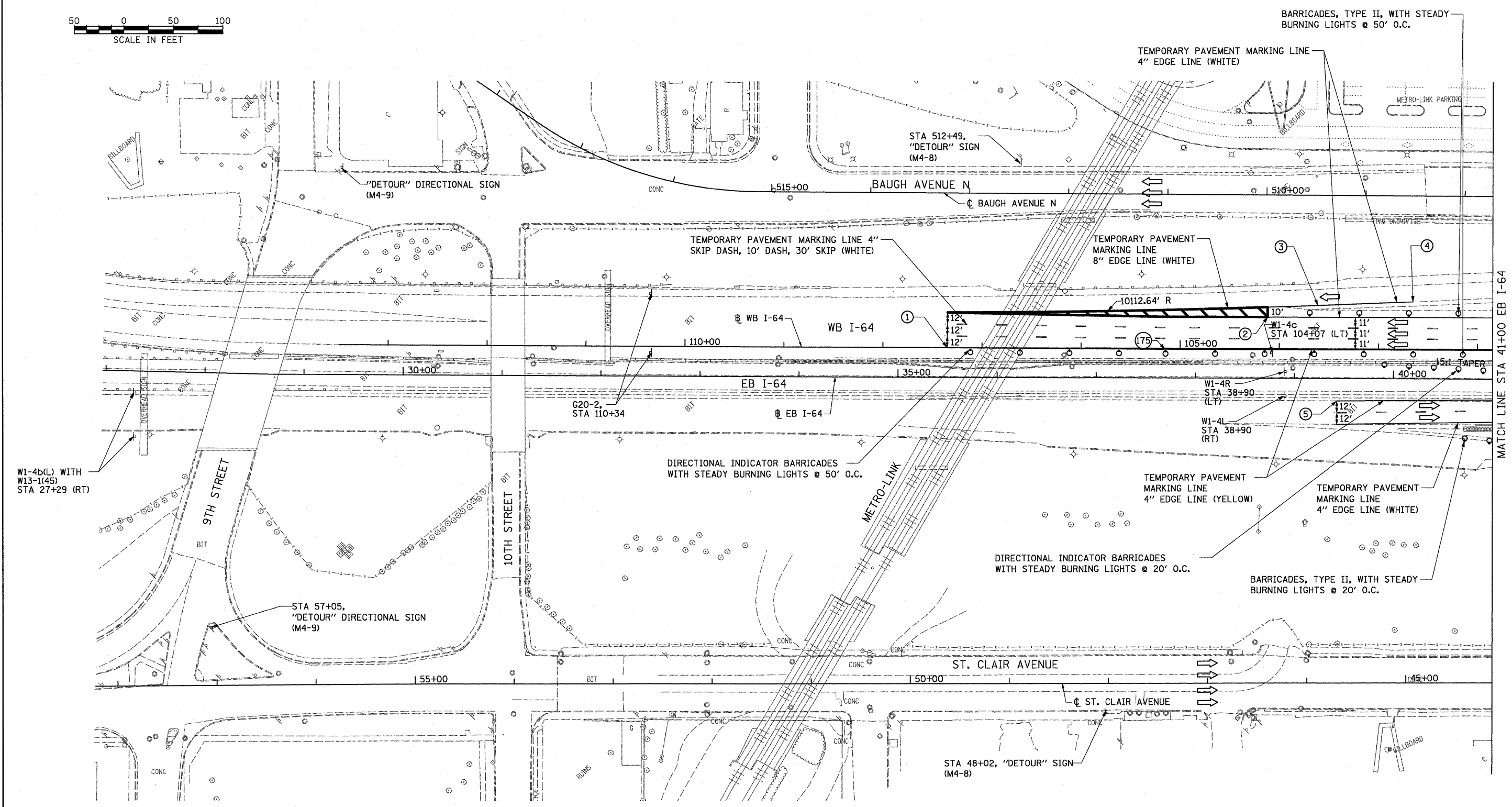
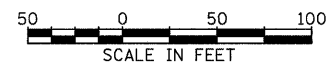


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	PLOT SCALE = 600.0000' / in.	DRAWN -	REVISED -					64	82-1-2HB	ST. CLAIR	345	75
	PLOT DATE = 3/19/2010	CHECKED -	REVISED -					CONTRACT NO. 76C49				
	DATE 03/19/2010				SCALE:	SHEET NO. 13 OF 13 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			





NOTES:  
1. SEE SHEET 4 OF 4 FOR TEMPORARY PAVEMENT MARKING LAYOUT.



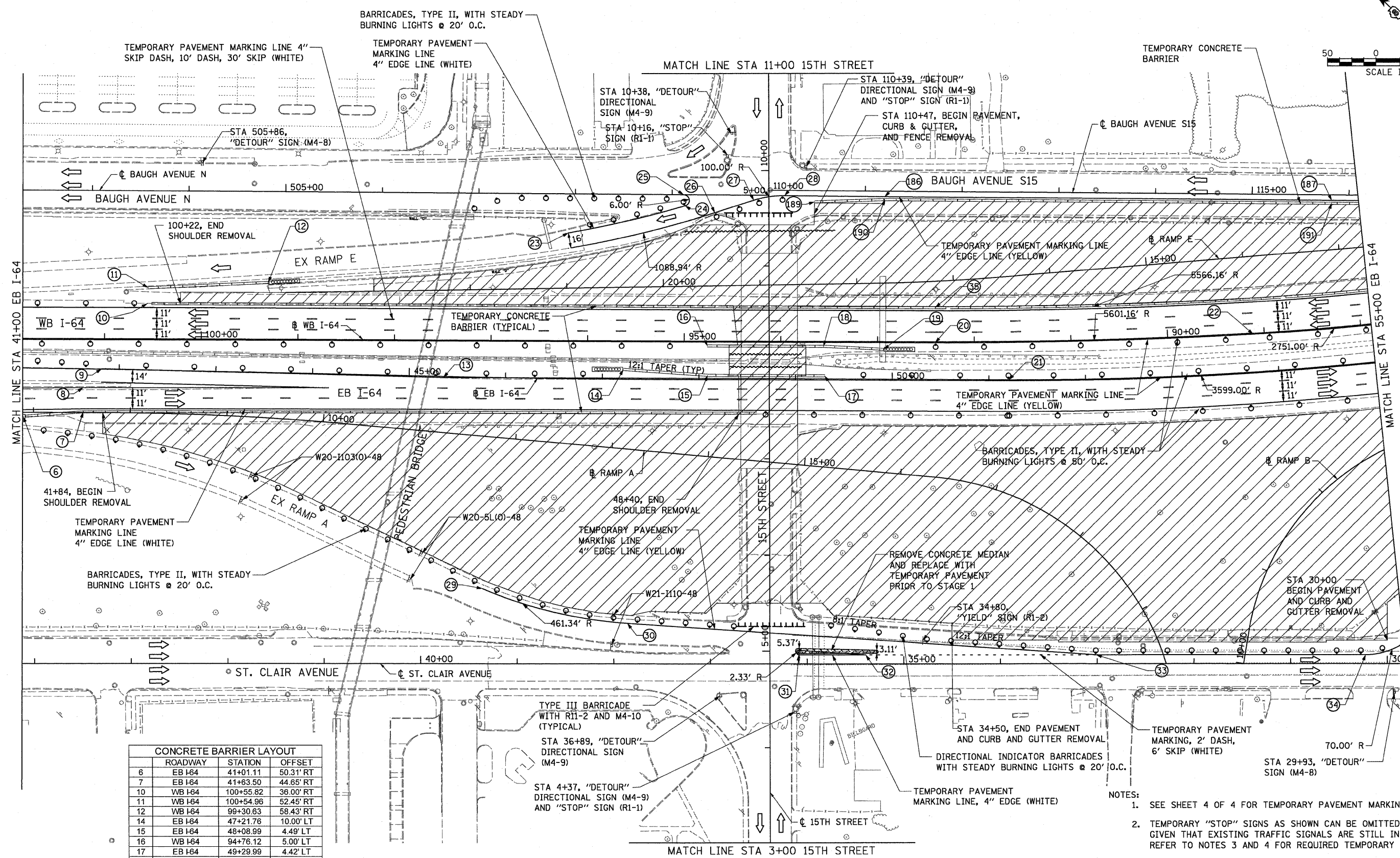
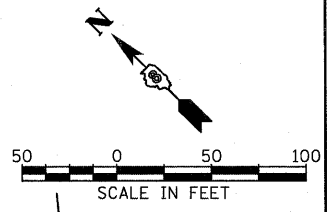
MATCH LINE STA 41+00 EB I-64

FILE NAME =	USER NAME = IDOT	DESIGNED - TTB	REVISED -
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	PLOT SCALE = 50.0000' / IN.	CHECKED - JAH	REVISED -
	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL - STAGE 1			
SCALE: 1" = 50'	SHEET NO. 1 OF 4 SHEETS	STA. 27+00	TO STA. 41+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	76
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	



**CONCRETE BARRIER LAYOUT**

	ROADWAY	STATION	OFFSET
6	EB I-64	41+01.11	50.31' RT
7	EB I-64	41+63.50	44.65' RT
10	WB I-64	100+55.82	36.00' RT
11	WB I-64	100+54.96	52.45' RT
12	WB I-64	99+30.63	58.43' RT
14	EB I-64	47+21.76	10.00' LT
15	EB I-64	48+08.99	4.49' LT
16	WB I-64	94+76.12	5.00' LT
17	EB I-64	49+29.99	4.42' LT
18	WB I-64	93+55.96	5.00' LT
19	WB I-64	92+93.66	7.98' LT
35	WB I-64	92+40.40	35.13' RT
189	BAUGH AVE	110+47.37	11.88' RT
190	BAUGH AVE	111+19.99	8.88' RT
191	BAUGH AVE	115+82.06	8.88' RT

- NOTES:**
- SEE SHEET 4 OF 4 FOR TEMPORARY PAVEMENT MARKING LAYOUT.
  - TEMPORARY "STOP" SIGNS AS SHOWN CAN BE OMITTED FROM CONTRACT GIVEN THAT EXISTING TRAFFIC SIGNALS ARE STILL IN OPERATION. REFER TO NOTES 3 AND 4 FOR REQUIRED TEMPORARY SIGNAL TIMING.
  - INTERSECTION OF ST. CLAIR AVENUE AND 15TH STREET: EXISTING EASTBOUND SIGNAL ON "FLASHING YELLOW" SEQUENCE, EXISTING NORTHBOUND SIGNAL ON "FLASHING RED" SEQUENCE
  - INTERSECTION OF BAUGH AVENUE AND 15TH STREET: EXISTING WESTBOUND SIGNAL ON "FLASHING RED" SEQUENCE, EXISTING SOUTHBOUND SIGNAL ON "FLASHING RED" SEQUENCE

FILE NAME =	USER NAME = IDOT	DESIGNED - TTB	REVISED -
*FILE#		DRAWN - TTB	REVISED -
	PLOT SCALE = 50.0000' / IN.	CHECKED - JAH	REVISED -
	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -

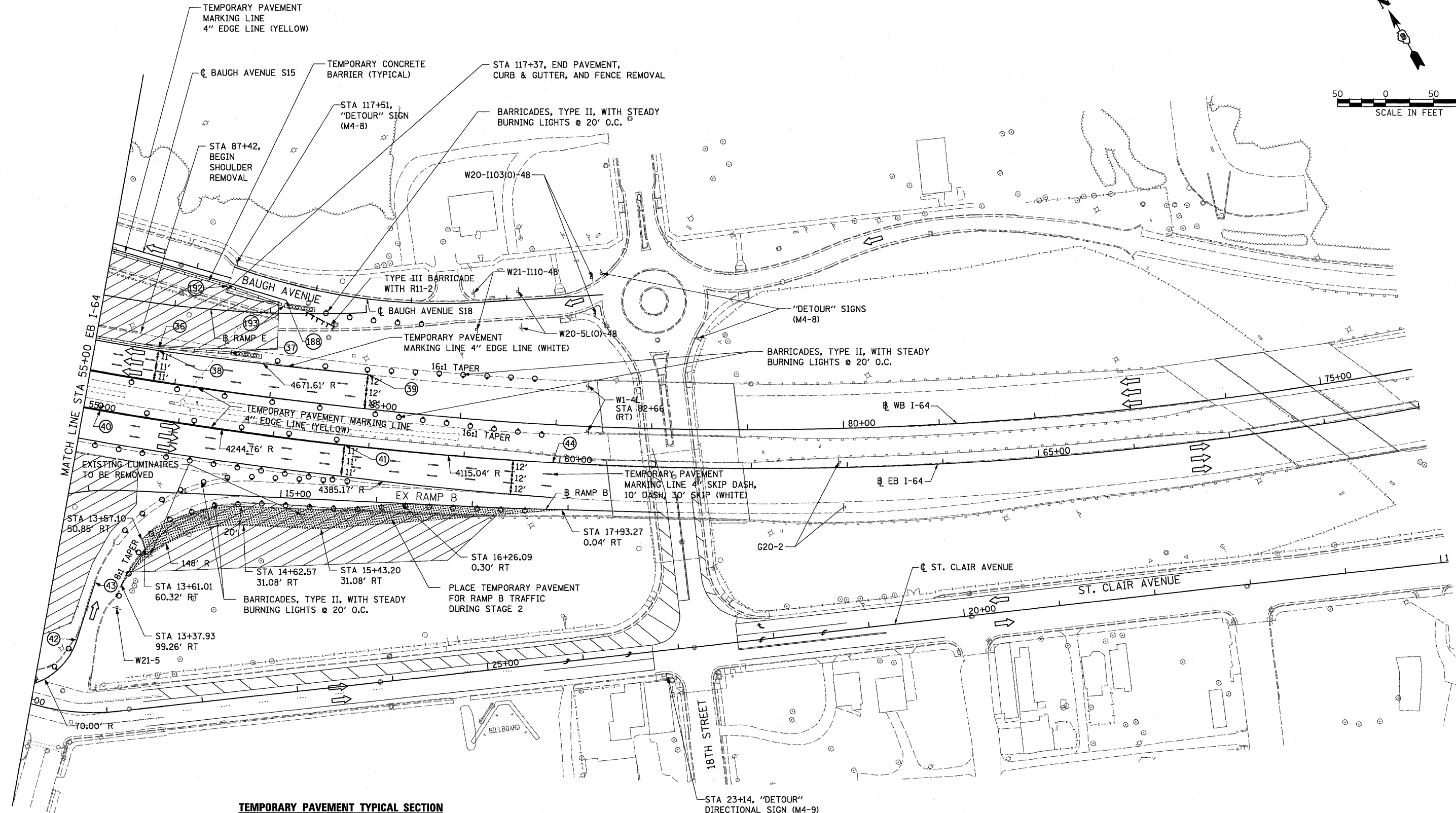
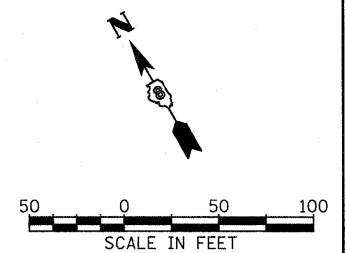
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND  
TRAFFIC CONTROL - STAGE 1**

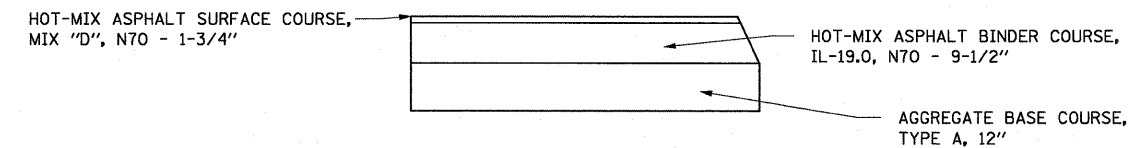
SCALE: 1" = 50'    SHEET NO. 2 OF 4 SHEETS    STA. 41+00 TO STA. 55+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	77
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	

NOTES:  
1. SEE SHEET 4 OF 4 FOR TEMPORARY PAVEMENT MARKING LAYOUT.



**TEMPORARY PAVEMENT TYPICAL SECTION**



CONCRETE BARRIER LAYOUT		
ROADWAY	STATION	OFFSET
36	WB I-64	87+22.33 35.15' RT
37	WB I-64	86+46.88 40.62' RT
192	BAUGH AVE	117+36.09 8.88' RT
193	RAMP E	10+85.91 8.10' RT

FILE NAME =	USER NAME = IDOT	DESIGNED - TTB	REVISED -
#FILE#		DRAWN - TTB	REVISED -
	PLOT SCALE = 50.0000' / IN.	CHECKED - JAH	REVISED -
	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND  
TRAFFIC CONTROL - STAGE 1**

SCALE: 1" = 50' SHEET NO. 3 OF 4 SHEETS STA. 55+00 TO STA. 69+00

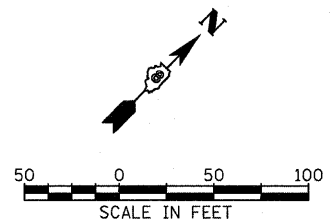
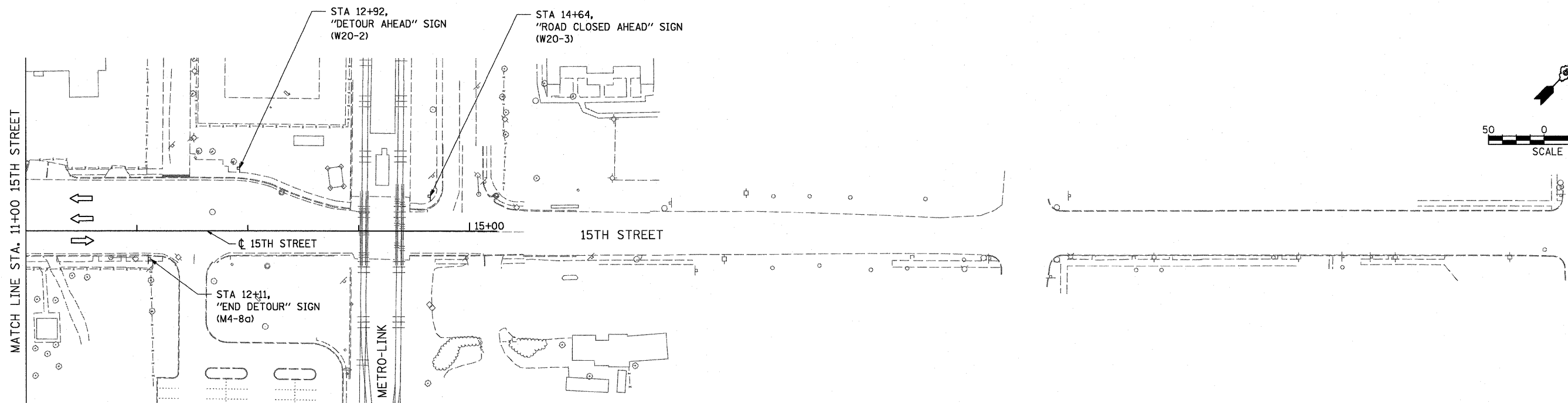
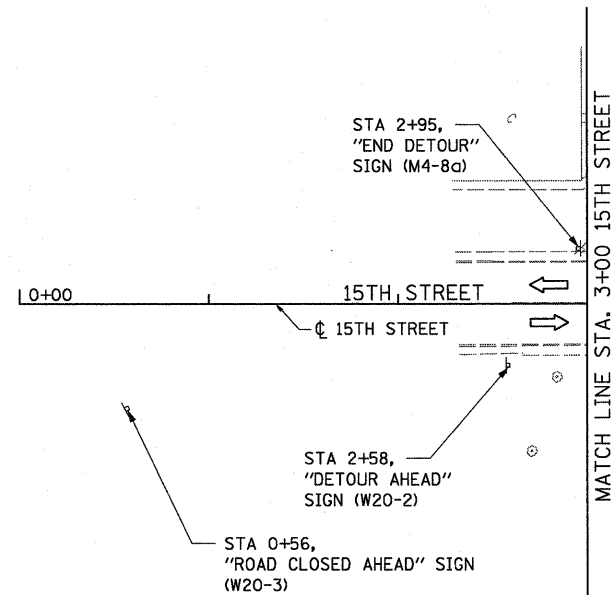
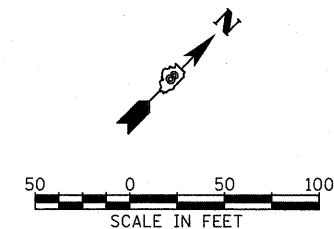
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	78
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	

**STAGE 1 TRAFFIC CONTROL ITEMS**

IMPACT ATTENUATORS, TEMPORARY	TEMPORARY CONCRETE BARRIER	4" MARKING	8" MARKING	12" MARKING	WORK ZONE PAVEMENT MARKING REMOVAL	TEMPORARY INFORMATION SIGNING	AGGREGATE BASE COURSE, TYPE A, 12"	HMA SURFACE COURSE, MIX "D", N70	HMA BINDER COURSE, IL-19.0, N70
EACH	FOOT	FOOT	FOOT	FOOT	SQ FT	SQ FT	SQ YD	TON	TON
6	3430	12849	476	766	3025	490	810	79	427

**PAVEMENT MARKING LAYOUT-STAGE 1**

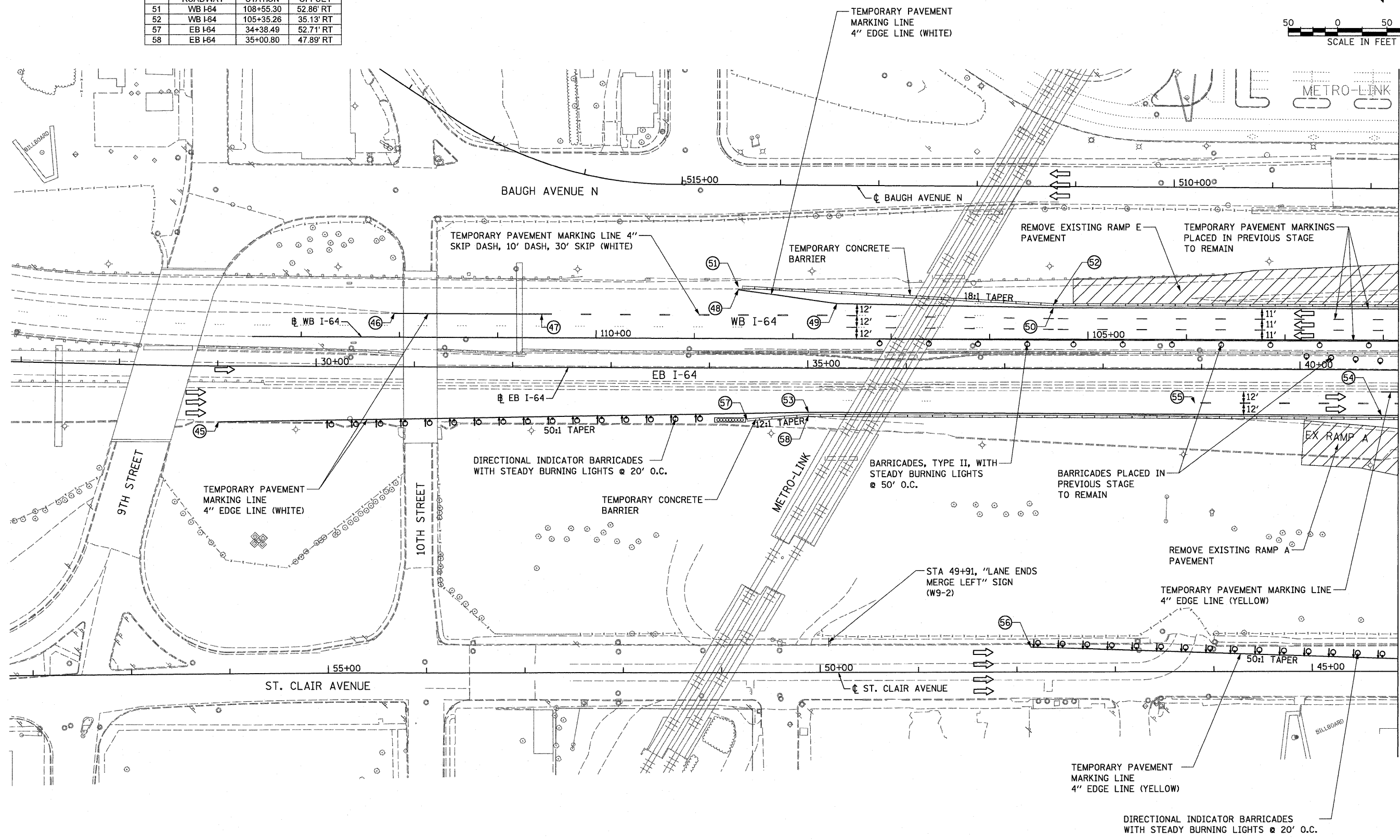
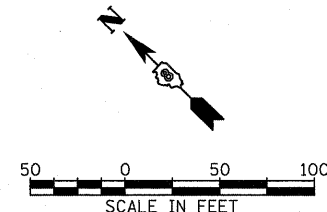
	ROADWAY	STATION	OFFSET
1	WB I-64	107+35.23	0.00' RT
2	WB I-64	104+11.77	32.00' RT
3	WB I-64	103+61.11	43.92' RT
4	WB I-64	102+65.20	47.80' RT
5	EB I-64	39+42.84	22.75' RT
175	WB I-64	105+15.26	1.00' LT
8	EB I-64	41+62.98	19.54' RT
9	EB I-64	41+81.72	0.39' LT
13	EB I-64	45+35.55	1.18' RT
20	WB I-64	92+40.39	1.00' LT
21	EB I-64	51+20.71	1.00' LT
22	WB I-64	89+11.35	1.00' LT
23	BAUGH AVE	7+07.27	41.03' LT
24	BAUGH AVE	5+86.47	11.72' LT
25	BAUGH AVE	5+88.23	0.02' LT
26	BAUGH AVE	5+55.84	18.75' LT
27	BAUGH AVE	5+15.65	5.39' LT
28	BAUGH AVE	110+16.20	5.58' RT
186	BAUGH AVE	111+19.99	5.75' RT
187	BAUGH AVE	115+82.06	5.75' RT
29	ST. CLAIR AVE	39+35.84	78.13' RT
30	ST. CLAIR AVE	37+85.90	42.08' RT
31	ST. CLAIR AVE	36+08.55	9.31' RT
32	ST. CLAIR AVE	35+39.30	9.27' RT
33	ST. CLAIR AVE	33+01.10	9.14' RT
34	ST. CLAIR AVE	30+22.44	9.14' RT
38	WB I-64	87+22.21	32.00' RT
39	WB I-64	85+00.63	35.88' RT
40	EB I-64	55+17.76	1.00' LT
41	EB I-64	57+74.62	0.60' LT
42	ST. CLAIR AVE	29+39.80	75.04' RT
43	ST. CLAIR AVE	29+09.96	135.90' RT
44	EB I-64	59+92.66	0.03' LT
188	RAMP E	10+83.81	13.47' RT



FILE NAME =	USER NAME = IDOT	DESIGNED - TTB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL - STAGE 1</b>		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
#FILE#		DRAWN - TTB	REVISED -				64	82-1-2HB	ST. CLAIR	345	79		
	PLOT SCALE = 50.0000' / IN.	CHECKED - JAH	REVISED -				SCALE: 1" = 50'		SHEET NO. 4 OF 4 SHEETS		STA. XX+XX TO STA. XX+XX		CONTRACT NO. 76C49
	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -				FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT				

- NOTES:
- SEE SHEET 4 OF 4 FOR TEMPORARY PAVEMENT MARKING LAYOUT.
  - DETOUR SIGNS PLACED IN PREVIOUS STAGE TO REMAIN.
  - WARNING SIGNS PLACED ON WESTBOUND I-64 IN PREVIOUS STAGE TO REMAIN.

CONCRETE BARRIER LAYOUT			
ROADWAY	STATION	OFFSET	
51	WB I-64	108+55.30	52.86' RT
52	WB I-64	105+35.26	35.13' RT
57	EB I-64	34+38.49	52.71' RT
58	EB I-64	35+00.80	47.89' RT



MATCH LINE STA 41+00 EB I-64

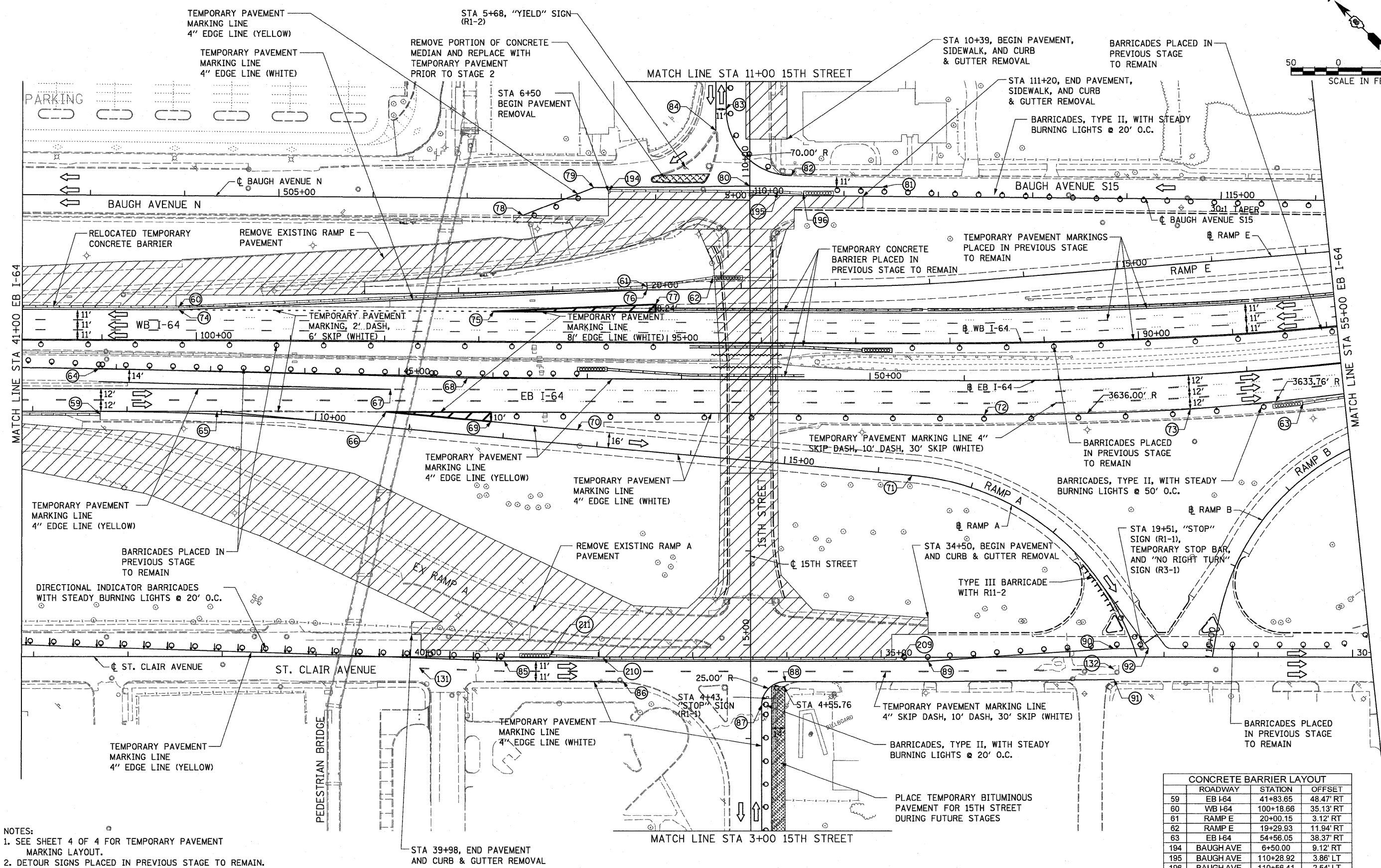
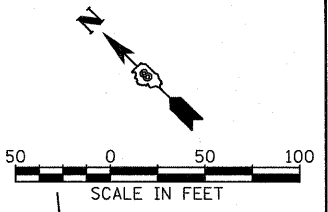
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	PLOT SCALE = 50.0000' / IN.	CHECKED - JAH	REVISED -
	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL - STAGE 2			
SCALE: 1" = 50'	SHEET NO. 1 OF 4 SHEETS	STA. 27+00	TO STA. 41+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	80
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	





- NOTES:
- SEE SHEET 4 OF 4 FOR TEMPORARY PAVEMENT MARKING LAYOUT.
  - DETOUR SIGNS PLACED IN PREVIOUS STAGE TO REMAIN.
  - EXISTING TRAFFIC SIGNALS AT THE INTERSECTIONS OF BAUGH AVENUE AND 15TH STREET AND ST. CLAIR AVENUE AND 15TH STREET NO LONGER IN OPERATION.

CONCRETE BARRIER LAYOUT			
NO.	ROADWAY	STATION	OFFSET
59	EB I-64	41+83.65	48.47' RT
60	WB I-64	100+18.66	35.13' RT
61	RAMP E	20+00.15	3.12' RT
62	RAMP E	19+29.93	11.94' RT
63	EB I-64	54+56.05	38.37' RT
194	BAUGH AVE	6+50.00	9.12' RT
195	BAUGH AVE	110+28.92	3.86' LT
196	BAUGH AVE	110+56.41	2.54' LT
209	ST. CLAIR AVE	34+79.38	1.12' LT
210	ST. CLAIR AVE	37+95.12	1.12' LT
211	ST. CLAIR AVE	38+51.43	1.54' RT

FILE NAME -	USER NAME = IDOT	DESIGNED - TTB	REVISED -
#FILE#		DRAWN - TTB	REVISED -
	PLOT SCALE = 50,0000' / IN.	CHECKED - JAH	REVISED -
	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

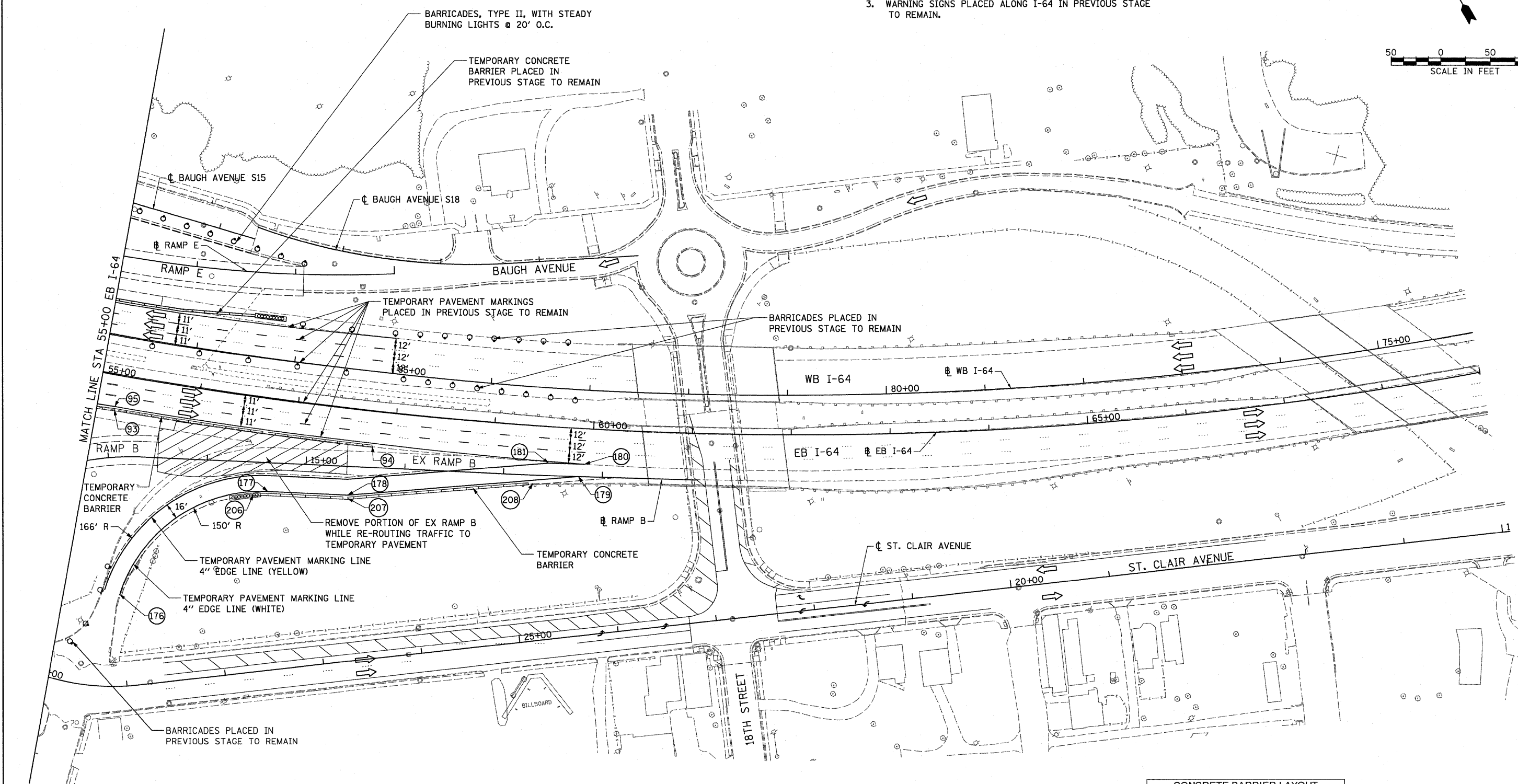
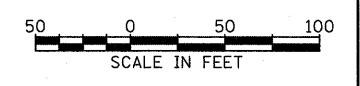
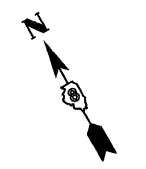
**SUGGESTED STAGES OF CONSTRUCTION AND  
TRAFFIC CONTROL - STAGE 2**

SCALE: 1" = 50' SHEET NO. 2 OF 4 SHEETS STA. 41+00 TO STA. 55+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	81
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 76C49	



- NOTES:
1. DETOUR SIGNS PLACED IN PREVIOUS STAGE TO REMAIN.
  2. WARNING SIGNS PLACED ALONG BAUGH AVENUE IN PREVIOUS STAGE TO REMAIN.
  3. WARNING SIGNS PLACED ALONG I-64 IN PREVIOUS STAGE TO REMAIN.



CONCRETE BARRIER LAYOUT			
MARKER	ROADWAY	STATION	OFFSET
93	EB I-64	55+17.84	35.12' RT
94	EB I-64	57+79.06	35.53' RT
206	RAMP B	14+48.06	32.92' RT
207	RAMP B	15+43.35	32.21' RT
208	RAMP B	17+30.10	8.41' RT

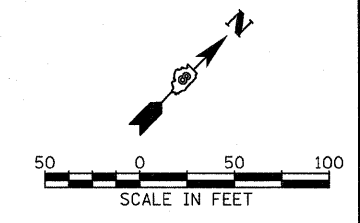
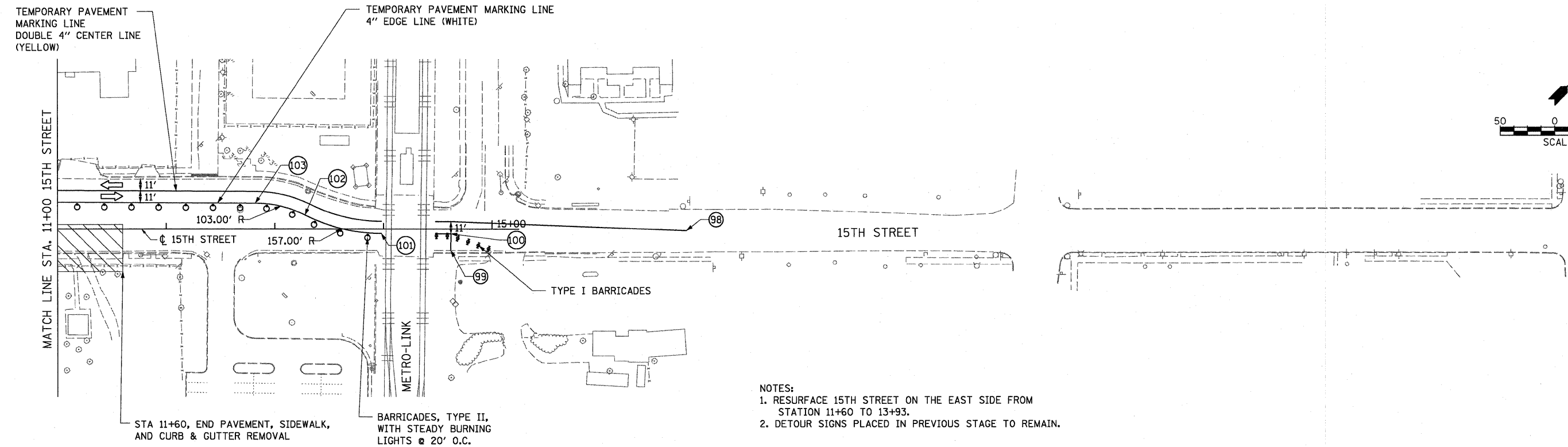
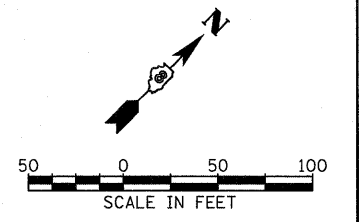
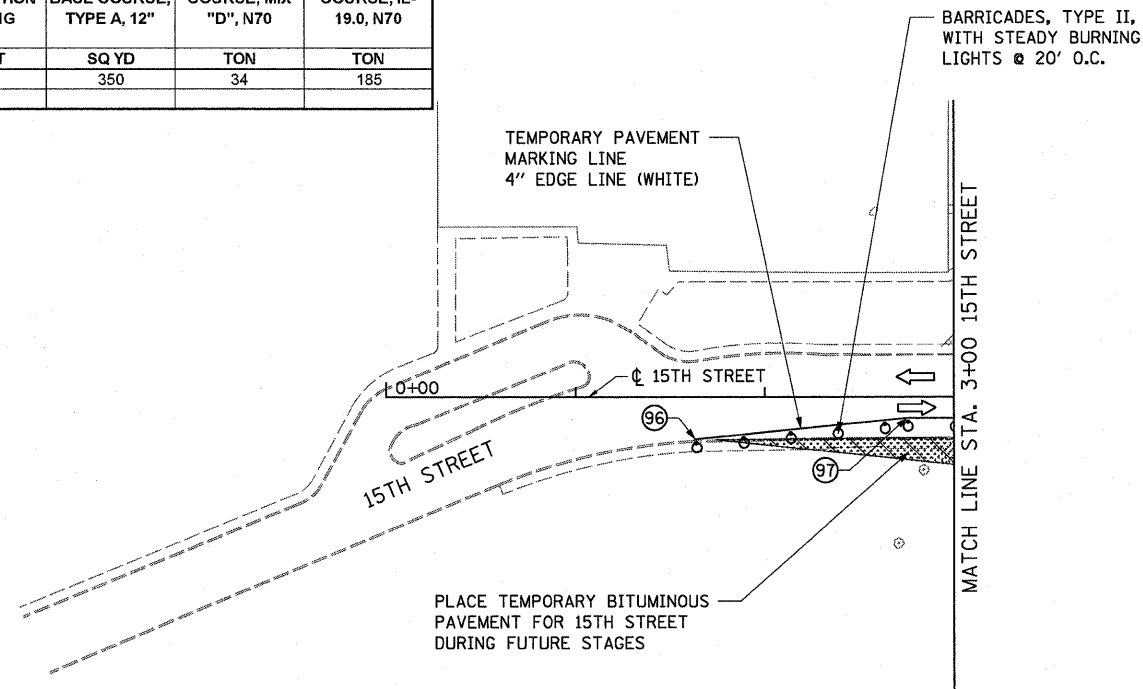
**STAGE 2 TRAFFIC CONTROL ITEMS**

IMPACT ATTENUATORS, TEMPORARY	IMPACT ATTENUATORS, RELOCATE	TEMPORARY CONCRETE BARRIER	RELOCATE TEMPORARY CONCRETE BARRIER	4" MARKING	8" MARKING	12" MARKING	WORK ZONE PAVEMENT MARKING REMOVAL	TEMPORARY INFORMATION SIGNING	AGGREGATE BASE COURSE, TYPE A, 12"	HMA SURFACE COURSE, MIX "D", N70	HMA BINDER COURSE, IL-19.0, N70
EACH	EACH	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT	SQ FT	SQ YD	TON	TON
2	3	1166	2172	11707	965	682	2286	30	350	34	185

**PAVEMENT MARKING LAYOUT - STAGE 2**

ROADWAY	STATION	OFFSET
EB I-64	29+00.97	56.89' RT
WB I-64	112+05.19	24.32' RT
WB I-64	110+55.19	24.17' RT
WB I-64	108+55.46	50.02' RT
WB I-64	107+55.26	35.87' RT
WB I-64	105+35.26	32.00' RT
EB I-64	35+00.81	44.89' RT
EB I-64	40+83.24	46.27' RT
EB I-64	38+93.19	34.56' RT
ST. CLAIR AVE	47+85.77	25.30' RT
EB I-64	41+81.72	0.39' LT
EB I-64	43+07.73	42.58' RT
EB I-64	44+87.12	39.08' RT
EB I-64	44+91.36	14.99' RT
EB I-64	45+72.82	1.35' RT
RAMP A	11+86.05	16.00' LT
RAMP A	12+78.27	16.00' LT
RAMP A	16+36.02	0.00' RT
EB I-64	51+20.71	36.00' RT
EB I-64	53+37.86	36.00' RT
WB I-64	100+18.55	32.00' RT
WB I-64	96+84.32	32.00' RT
RAMP E	20+00.00	0.00' LT
RAMP E	19+91.03	15.76' LT
BAUGH AVE	7+40.75	18.00' LT
BAUGH AVE	6+65.82	11.96' RT
BAUGH AVE	110+00.00	9.50' LT
BAUGH AVE	111+40.00	9.50' LT
BAUGH AVE	110+43.82	21.35' LT
15TH STREET	10+70.63	24.89' LT
15TH STREET	10+48.11	36.01' LT

17	ST. CLAIR AVE	39+00.00	4.00' LT
86	ST. CLAIR AVE	37+73.66	26.00' LT
87	15TH STREET	4+35.96	11.66' RT
88	ST. CLAIR AVE	36+01.52	25.91' LT
89	ST. CLAIR AVE	34+50.00	4.00' LT
90	ST. CLAIR AVE	32+52.90	9.14' RT
91	ST. CLAIR AVE	32+52.90	23.86' LT
92	ST. CLAIR AVE	32+16.40	9.14' RT
131	ST. CLAIR AVE	39+90.00	12.00' LT
132	ST. CLAIR AVE	32+53.02	12.86' LT
95	EB I-64	55+17.76	32.00' RT
176	RAMP B	12+97.68	139.17' RT
177	RAMP B	14+62.57	29.08' RT
178	RAMP B	15+43.20	29.08' RT
179	RAMP B	17+75.82	0.04' LT
180	RAMP B	17+81.53	12.63' LT
181	RAMP B	17+45.25	12.64' LT
96	15TH STREET	1+63.30	22.70' RT
97	15TH STREET	2+75.29	11.42' RT
98	15TH STREET	15+24.82	1.22' RT
99	15TH STREET	14+62.11	19.90' RT
100	15TH STREET	14+62.16	4.30' RT
101	15TH STREET	13+98.07	4.09' RT
102	15TH STREET	13+27.85	12.75' LT
103	15TH STREET	12+81.98	23.79' LT

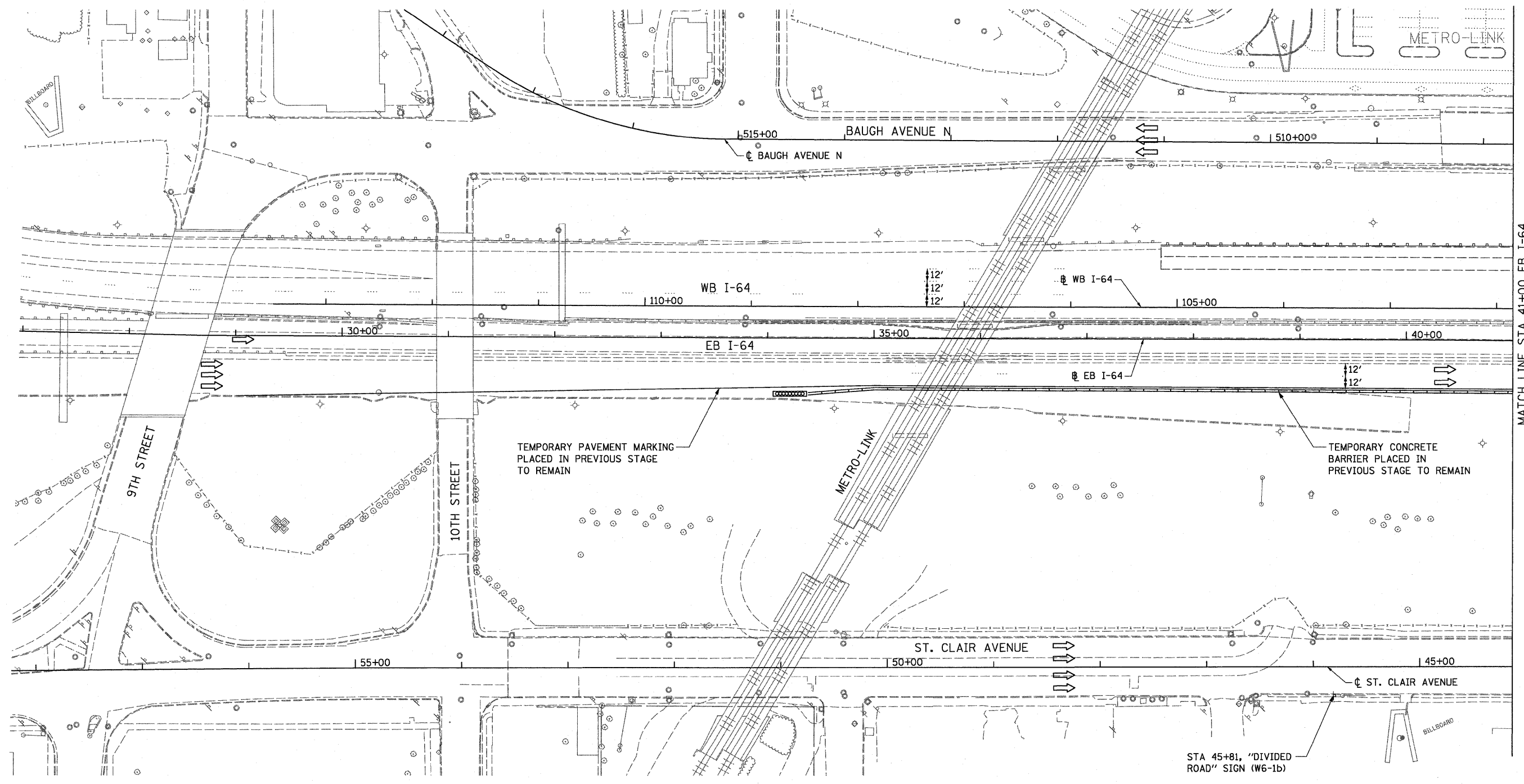
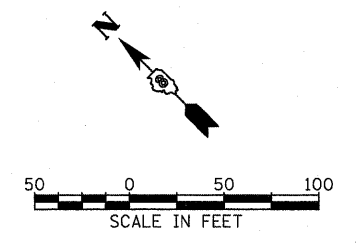


- NOTES:
- RESURFACE 15TH STREET ON THE EAST SIDE FROM STATION 11+60 TO 13+93.
  - DETOUR SIGNS PLACED IN PREVIOUS STAGE TO REMAIN.

FILE NAME =	USER NAME = IDOT	DESIGNED - TTB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL - STAGE 2</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
#FILE#	PLOT SCALE = 50.0000' / IN.	DRAWN - TTB	REVISED -			64	82-1-2HB	ST. CLAIR	345	83	
	PLOT DATE = 3/17/2010	CHECKED - JAH	REVISED -			CONTRACT NO. 76C49					
		DATE - 3/19/2010	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					

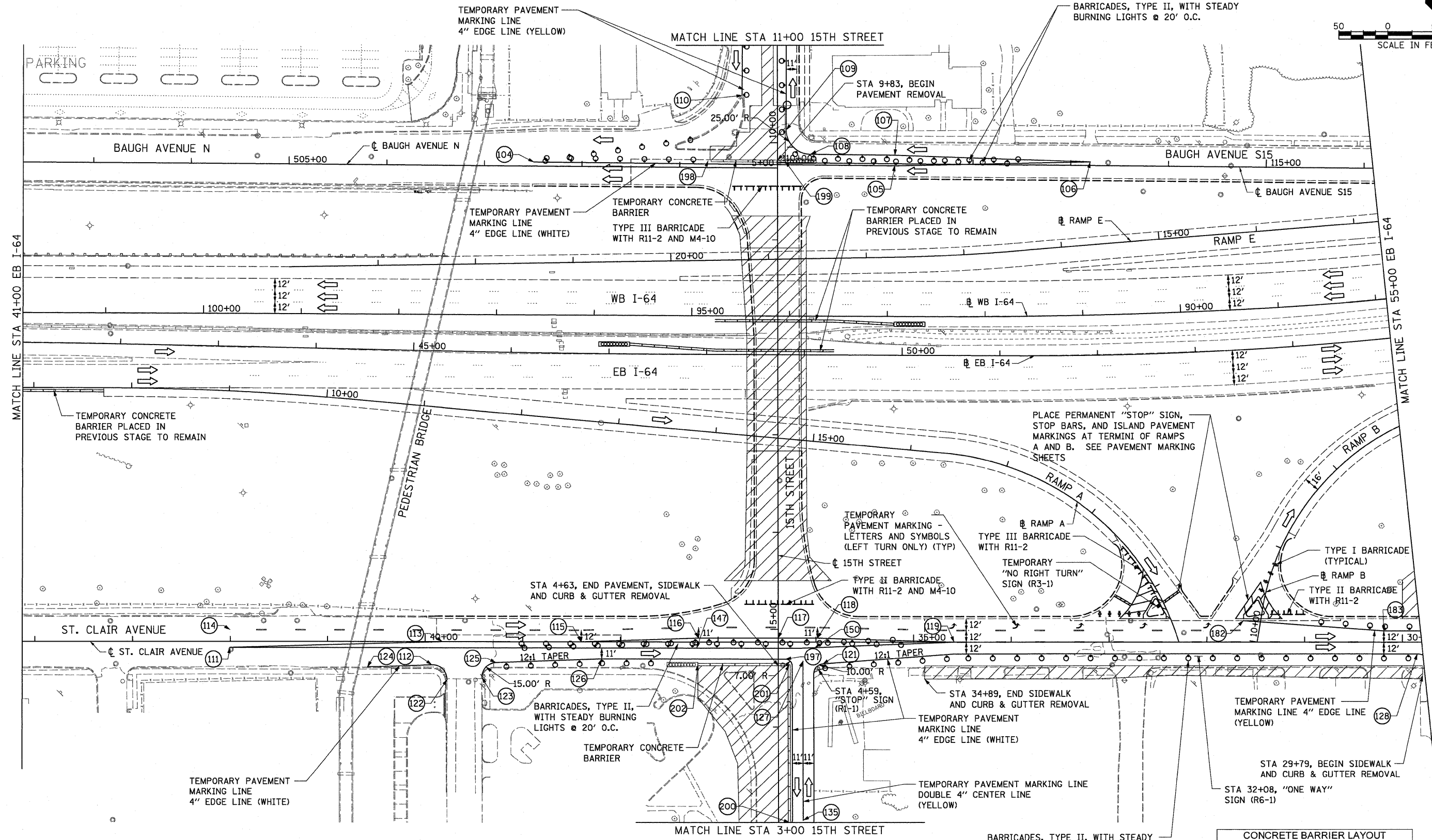
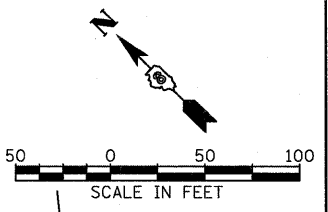
SCALE: 1" = 50' SHEET NO. 4 OF 4 SHEETS STA. XX+XX TO STA. XX+XX

- NOTES:  
 1. DETOUR SIGNS PLACED IN PREVIOUS STAGE TO REMAIN.  
 2. WARNING SIGNS PLACED ALONG EASTBOUND I-64 IN PREVIOUS STAGE TO REMAIN.



MATCH LINE STA 41+00 EB I-64

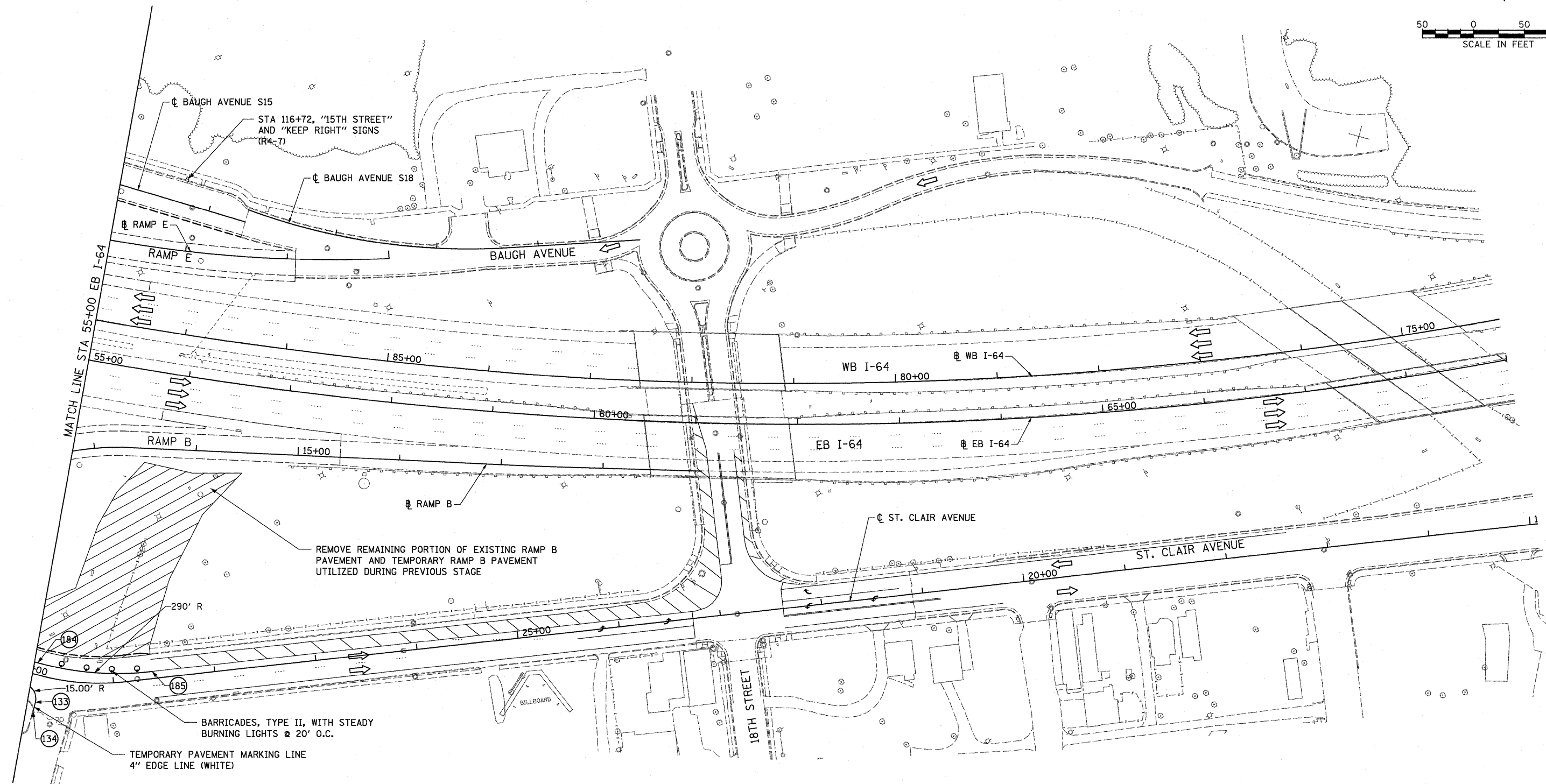
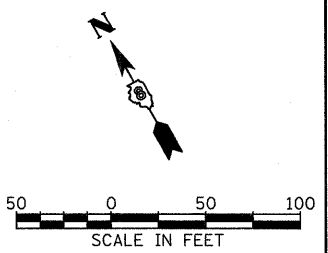
FILE NAME = #FILE#	USER NAME = IDOT	DESIGNED - TTB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL - STAGE 3</b>		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.0000' / IN.	DRAWN - TTB	REVISED -				64	82-1-2HB	ST. CLAIR	345	84
	PLOT DATE = 3/17/2010	CHECKED - JAH	REVISED -				CONTRACT NO. 76C49				
	DATE - 3/19/2010	REVISED -		SCALE: 1" = 50'	SHEET NO. 1 OF 4 SHEETS	STA. 27+00 TO STA. 41+00	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



- NOTES:
- SEE SHEET 4 OF 4 FOR TEMPORARY PAVEMENT MARKING LAYOUT.
  - DETOUR SIGNS PLACED IN PREVIOUS STAGE TO REMAIN.

CONCRETE BARRIER LAYOUT			
ROADWAY	STATION	OFFSET	
198 BAUGH AVE	5+70.99	6.88' RT	
199 BAUGH AVE	110+07.98	1.56' LT	
200 15TH STREET	3+00.00	11.15' RT	
201 15TH STREET	4+64.04	11.15' RT	
202 ST. CLAIR AVE	37+20.85	22.89' LT	

NOTES:  
 1. DETOUR SIGNS PLACED IN PREVIOUS STAGE TO REMAIN.



REMOVE REMAINING PORTION OF EXISTING RAMP B PAVEMENT AND TEMPORARY RAMP B PAVEMENT UTILIZED DURING PREVIOUS STAGE

BARRICADES, TYPE II, WITH STEADY BURNING LIGHTS @ 20' O.C.  
 TEMPORARY PAVEMENT MARKING LINE  
 4" EDGE LINE (WHITE)

FILE NAME =	USER NAME = IDOT	DESIGNED - TTB	REVISED -
#FILE#		DRAWN - TTB	REVISED -
	PLOT SCALE = 50.0000' / IN.	CHECKED - JAH	REVISED -
	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

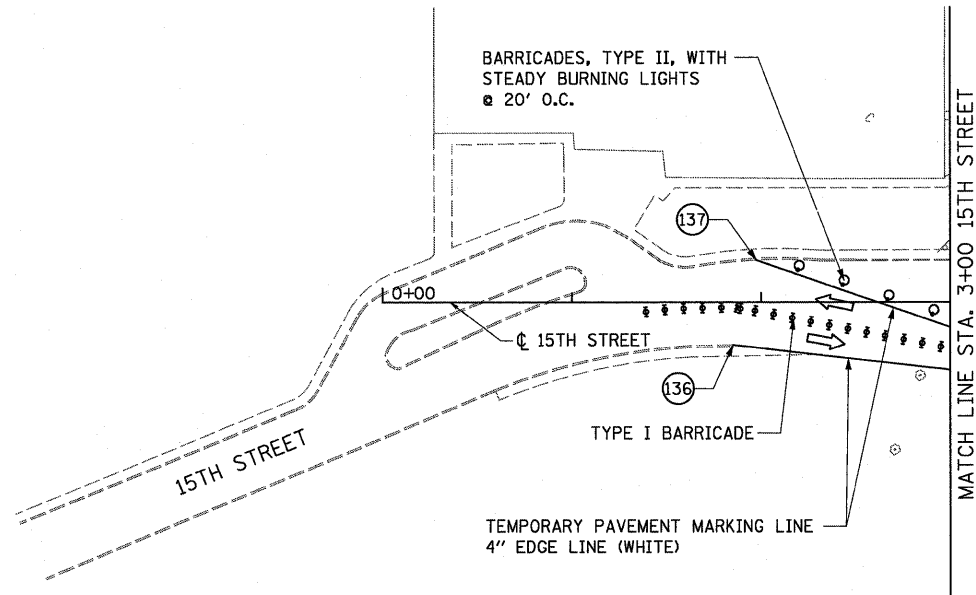
**SUGGESTED STAGES OF CONSTRUCTION AND  
 TRAFFIC CONTROL - STAGE 3**

SCALE: 1" = 50'    SHEET NO. 3 OF 4 SHEETS    STA. 55+00 TO STA. 69+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	86
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	

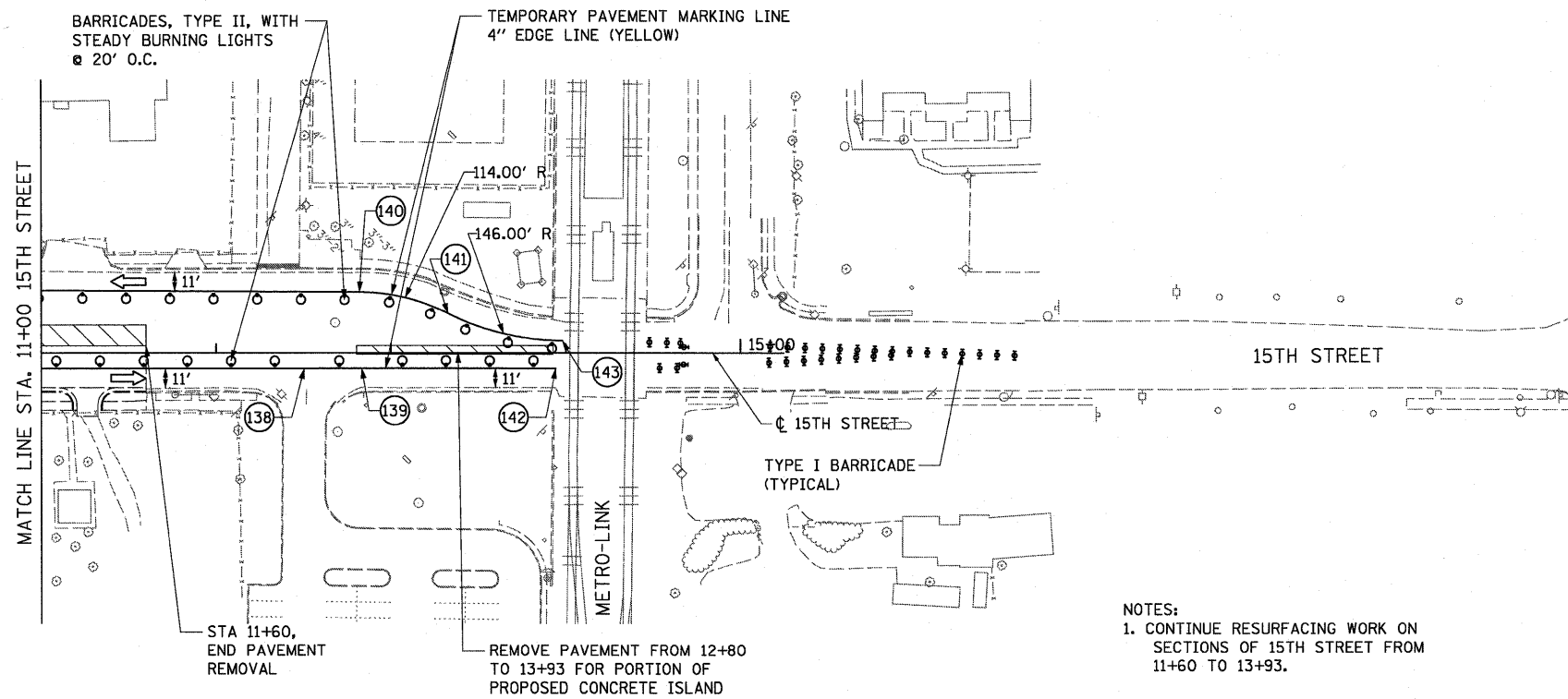
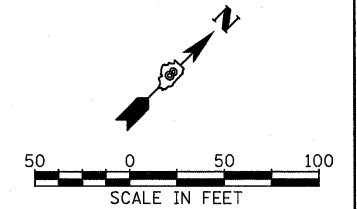
**STAGE 3 TRAFFIC CONTROL ITEMS**

IMPACT ATTENUATORS, RELOCATE	RELOCATE TEMPORARY CONCRETE BARRIER	4" MARKING	TEMPORARY LETTERS AND SYMBOLS	WORK ZONE PAVEMENT MARKING REMOVAL	TEMPORARY INFORMATION SIGNING
EACH	FOOT	FOOT	SQ FT	SQ FT	SQ FT
2	336	5560	57	4485	50

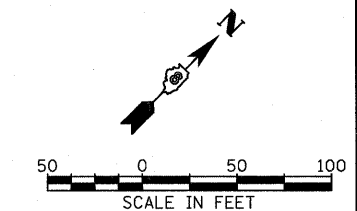


NOTES:  
1. DETOUR SIGNS PLACED IN PREVIOUS STAGE TO REMAIN.

ROADWAY	STATION	OFFSET
104	BAUGH AVE	7+49.31 4.00' RT
105	BAUGH AVE	111+20.00 2.00' RT
106	BAUGH AVE	113+20.00 3.57' LT
107	BAUGH AVE	111+20.00 9.00' LT
108	BAUGH AVE	110+33.66 9.00' LT
109	15TH STREET	10+13.03 9.00' RT
110	15TH STREET	10+48.11 36.01' LT
111	ST. CLAIR AVE	42+00.45 6.21' LT
112	ST. CLAIR AVE	39+93.07 23.49' LT
113	ST. CLAIR AVE	39+82.47 0.00' LT
114	ST. CLAIR AVE	42+00.43 12.09' RT
115	ST. CLAIR AVE	38+40.00 0.00' LT
116	ST. CLAIR AVE	37+20.00 4.00' RT
117	ST. CLAIR AVE	36+38.49 4.00' RT
118	ST. CLAIR AVE	35+98.45 4.00' RT
119	ST. CLAIR AVE	34+58.45 0.00' RT
121	ST. CLAIR AVE	35+92.93 22.12' LT
122	ST. CLAIR AVE	39+77.53 38.47' LT
123	ST. CLAIR AVE	39+41.86 36.16' LT
124	ST. CLAIR AVE	40+59.21 25.85' LT
125	ST. CLAIR AVE	39+27.40 21.15' LT
126	ST. CLAIR AVE	38+19.21 17.30' LT
127	15TH STREET	4+67.53 25.28' RT
128	ST. CLAIR AVE	29+84.83 12.00' LT
135	15TH STREET	3+02.20 25.28' RT
147	ST. CLAIR AVE	37+21.98 6.30' LT
150	ST. CLAIR AVE	36+01.00 5.72' LT
182	ST. CLAIR AVE	31+62.61 21.99' RT
183	ST. CLAIR AVE	30+18.98 11.63' RT
197	ST. CLAIR AVE	36+31.19 17.30' LT
133	ST. CLAIR AVE	29+70.71 26.75' LT
134	ST. CLAIR AVE	29+70.76 36.07' LT
184	ST. CLAIR AVE	29+78.25 11.63' RT
185	ST. CLAIR AVE	28+61.00 7.00' RT
136	15TH STREET	1+85.32 22.88' RT
137	15TH STREET	1+97.61 22.52' LT
138	15TH STREET	12+50.47 9.00' RT
139	15TH STREET	12+83.36 8.78' RT
140	15TH STREET	12+82.04 34.79' LT
141	15TH STREET	13+32.81 22.57' LT
142	15TH STREET	13+94.37 8.93' RT
143	15TH STREET	13+98.10 6.91' LT



NOTES:  
1. CONTINUE RESURFACING WORK ON SECTIONS OF 15TH STREET FROM 11+60 TO 13+93.



FILE NAME =	USER NAME = IDOT	DESIGNED - TTB	REVISED -
#FILE#		DRAWN - TTB	REVISED -
	PLOT SCALE = 50.0000' / IN.	CHECKED - JAH	REVISED -
	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -

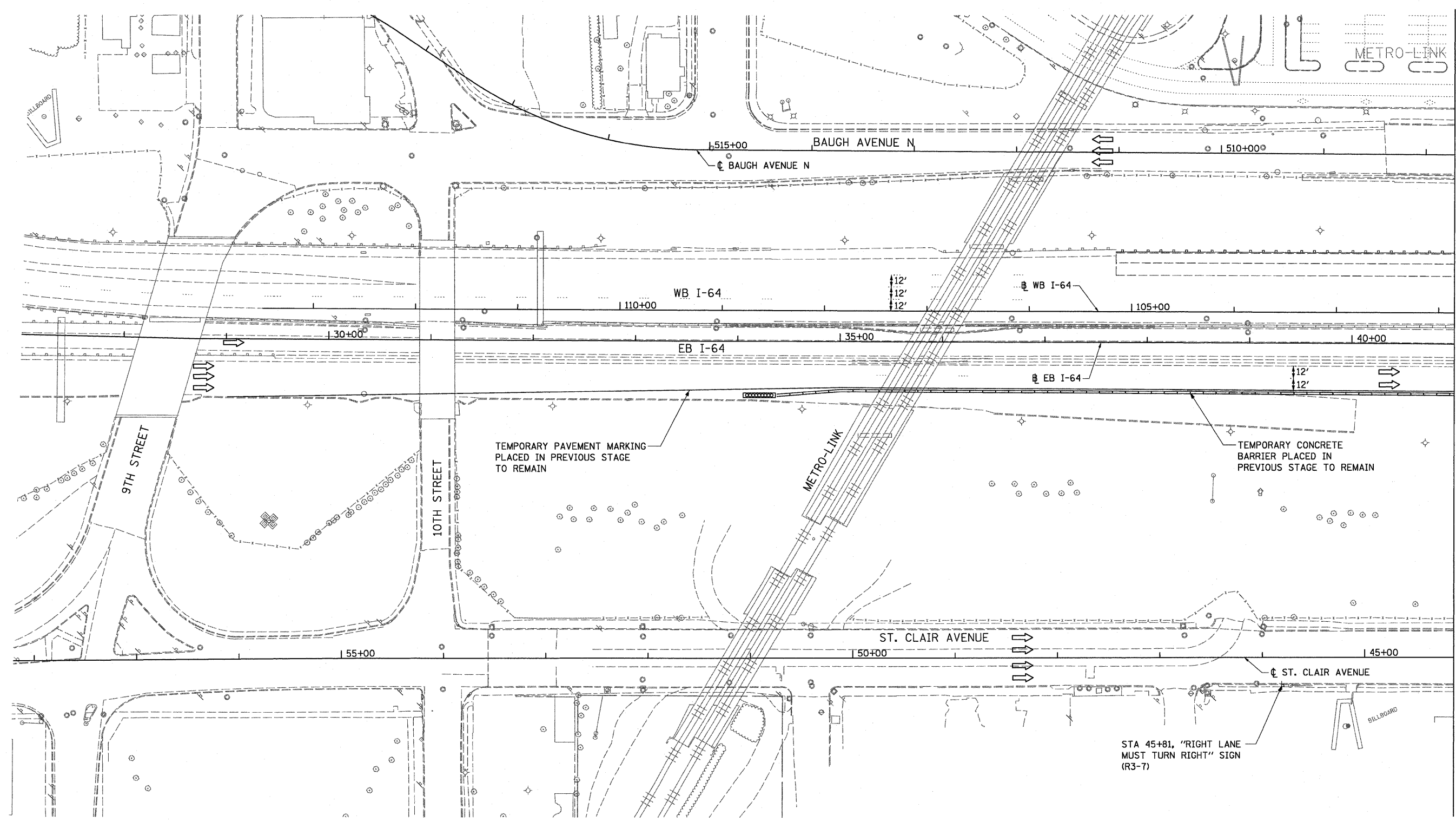
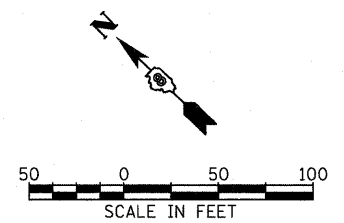
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND  
TRAFFIC CONTROL - STAGE 3**

SCALE: 1" = 50' SHEET NO. 4 OF 4 SHEETS STA. XX+XX TO STA. XX+XX

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	87
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	





MATCH LINE STA 41+00 EB I-64

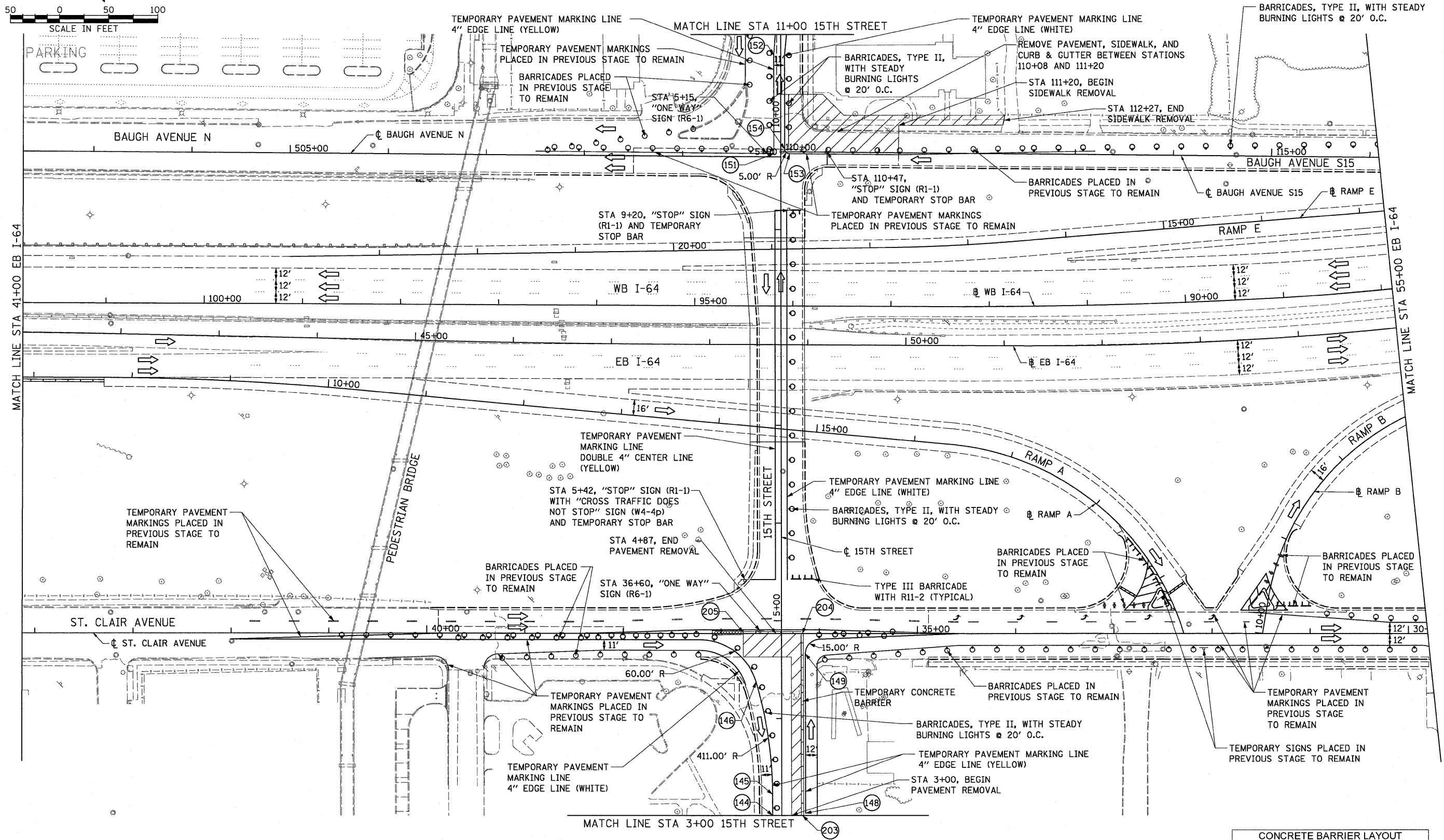
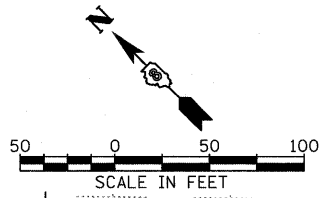
TEMPORARY PAVEMENT MARKING  
PLACED IN PREVIOUS STAGE  
TO REMAIN

TEMPORARY CONCRETE  
BARRIER PLACED IN  
PREVIOUS STAGE TO REMAIN

STA 45+81, "RIGHT LANE  
MUST TURN RIGHT" SIGN  
(R3-7)

FILE NAME = #FILE#	USER NAME = IDOT	DESIGNED - TTB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL - STAGE 4</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.0000' / IN.	DRAWN - TTB	REVISED -					64	82-1-2HB	ST. CLAIR	345	88
	PLOT DATE = 3/17/2010	CHECKED - JAH	REVISED -					CONTRACT NO. 76C49				
	DATE - 3/19/2010	REVISED -		SCALE: 1" = 50'	SHEET NO. 1 OF 4 SHEETS	STA. 27+00 TO STA. 41+00	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					

NOTES:  
 1. SEE SHEET 4 OF 4 FOR TEMPORARY PAVEMENT MARKING LAYOUT.



CONCRETE BARRIER LAYOUT		
ROADWAY	STATION	OFFSET
203	15TH STREET	3+00.00 21.15' RT
204	ST. CLAIR AVE	36+17.33 0.02' LT
205	ST. CLAIR AVE	36+77.35 1.10' RT

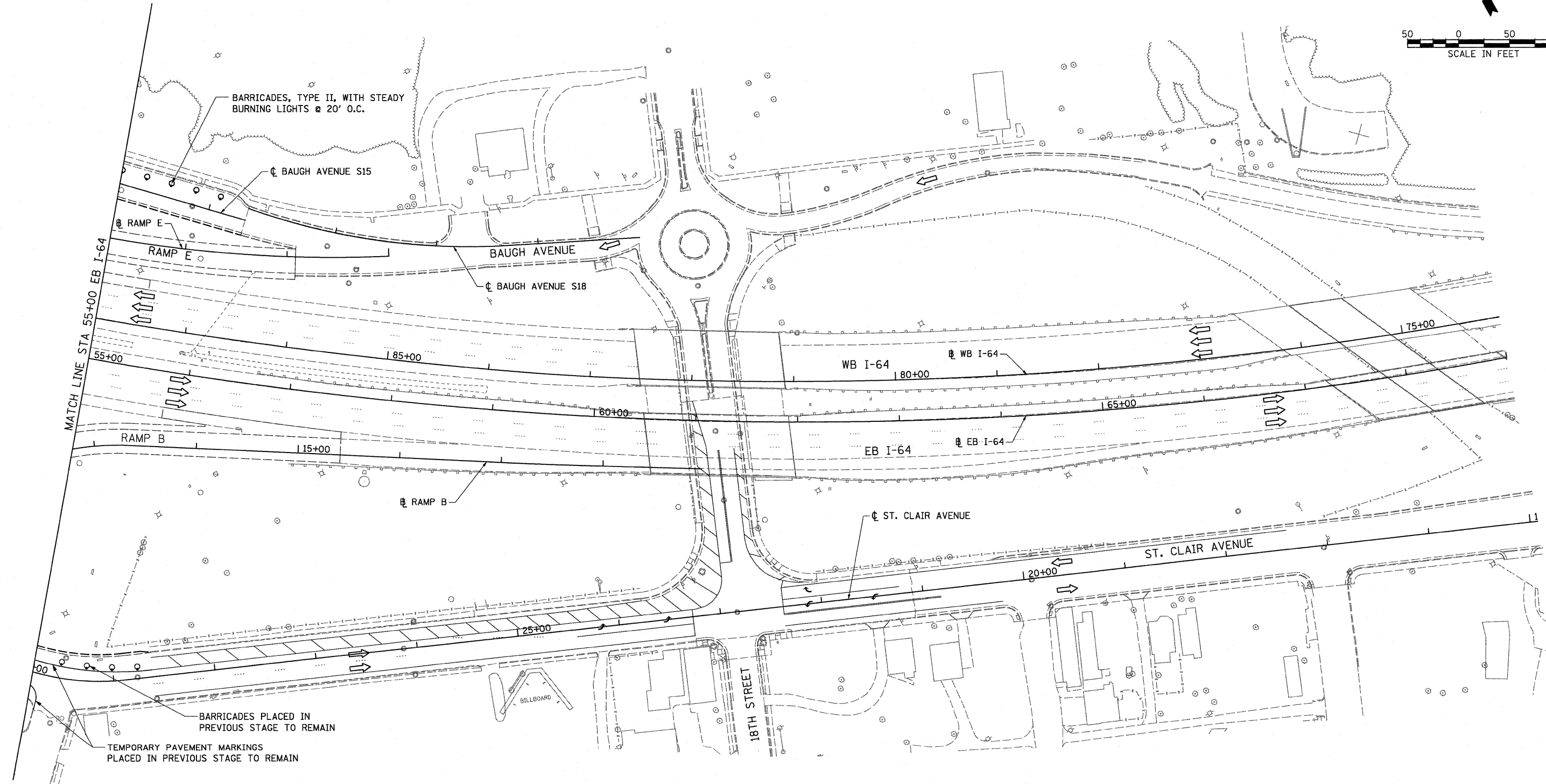
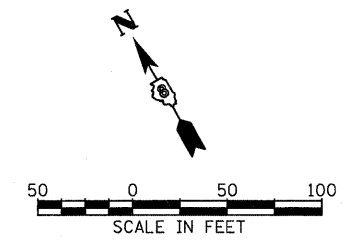
FILE NAME =	USER NAME = IDGT	DESIGNED - TTB	REVISED -
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	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND  
 TRAFFIC CONTROL - STAGE 4**

SCALE: 1" = 50' SHEET NO. 2 OF 4 SHEETS STA. 41+00 TO STA. 55+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	89
CONTRACT NO. 76C49				



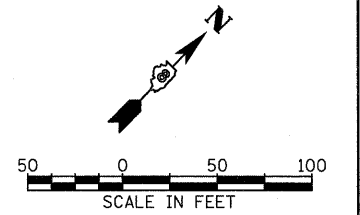
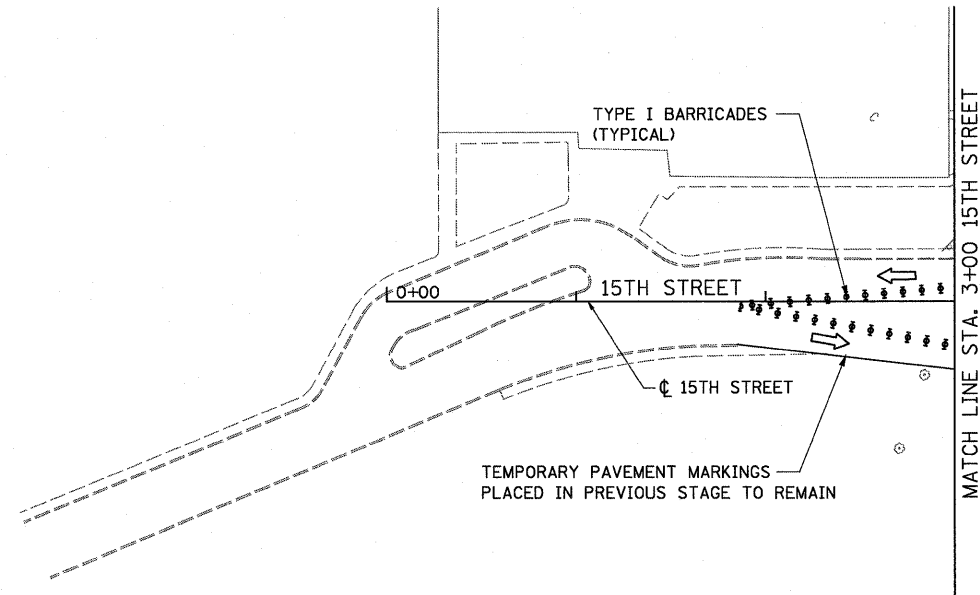
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	PLOT SCALE = 50.0000' / IN.	DRAWN - TTB	REVISED -				64	82-1-2HB	ST. CLAIR	345	90
	PLOT DATE = 3/17/2010	CHECKED - JAH	REVISED -				CONTRACT NO. 76C49				
	DATE - 3/19/2010	REVISED -		SCALE: 1" = 50'	SHEET NO. 3 OF 4 SHEETS	STA. 55+00 TO STA. 69+00	FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

**STAGE 4 TRAFFIC CONTROL ITEMS**

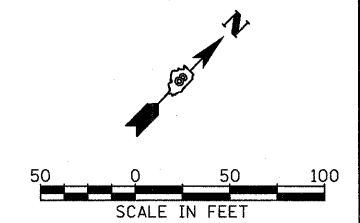
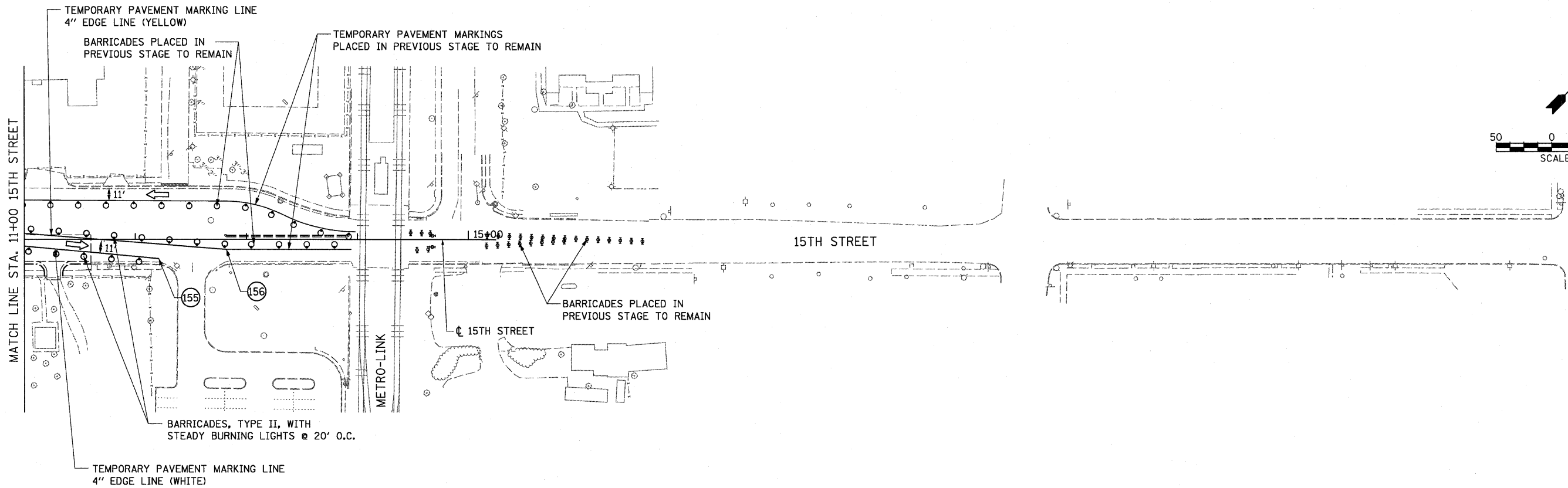
IMPACT ATTENUATORS, RELOCATE	RELOCATE TEMPORARY CONCRETE BARRIER	4" MARKING	24" MARKING (STOP BAR)	WORK ZONE PAVEMENT MARKING REMOVAL	TEMPORARY INFORMATION SIGNING
EACH	FOOT	FOOT	FOOT	SQ FT	SQ FT
1	246	2151	50	430	80

**PAVEMENT MARKING LAYOUT- STAGE 4**

	ROADWAY	STATION	OFFSET
144	15TH STREET	3+00.00	9.38' LT
145	15TH STREET	3+21.96	9.38' LT
146	15TH STREET	4+37.49	25.96' LT
148	15TH STREET	3+02.20	24.28' RT
149	15TH STREET	4+66.18	24.28' RT
151	15TH STREET	9+77.44	7.00' LT
152	15TH STREET	10+79.29	7.00' LT
153	BAUGH AVE	110+08.97	2.00' RT
154	15TH STREET	9+82.28	4.00' RT
155	15TH STREET	12+21.02	17.30' RT
156	15TH STREET	12+82.05	8.93' RT



- NOTES:  
 1. PLACE "NO TRUCKS" SIGN (R12-3) FOR NORTHBOUND TRAFFIC AT INTERSECTION OF 15TH STREET AND SUMMIT AVENUE.



FILE NAME =	USER NAME = IDOT	DESIGNED - TTB	REVISED -
#FILE#		DRAWN - TTB	REVISED -
	PLOT SCALE = 50.0000' / IN.	CHECKED - JAH	REVISED -
	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -

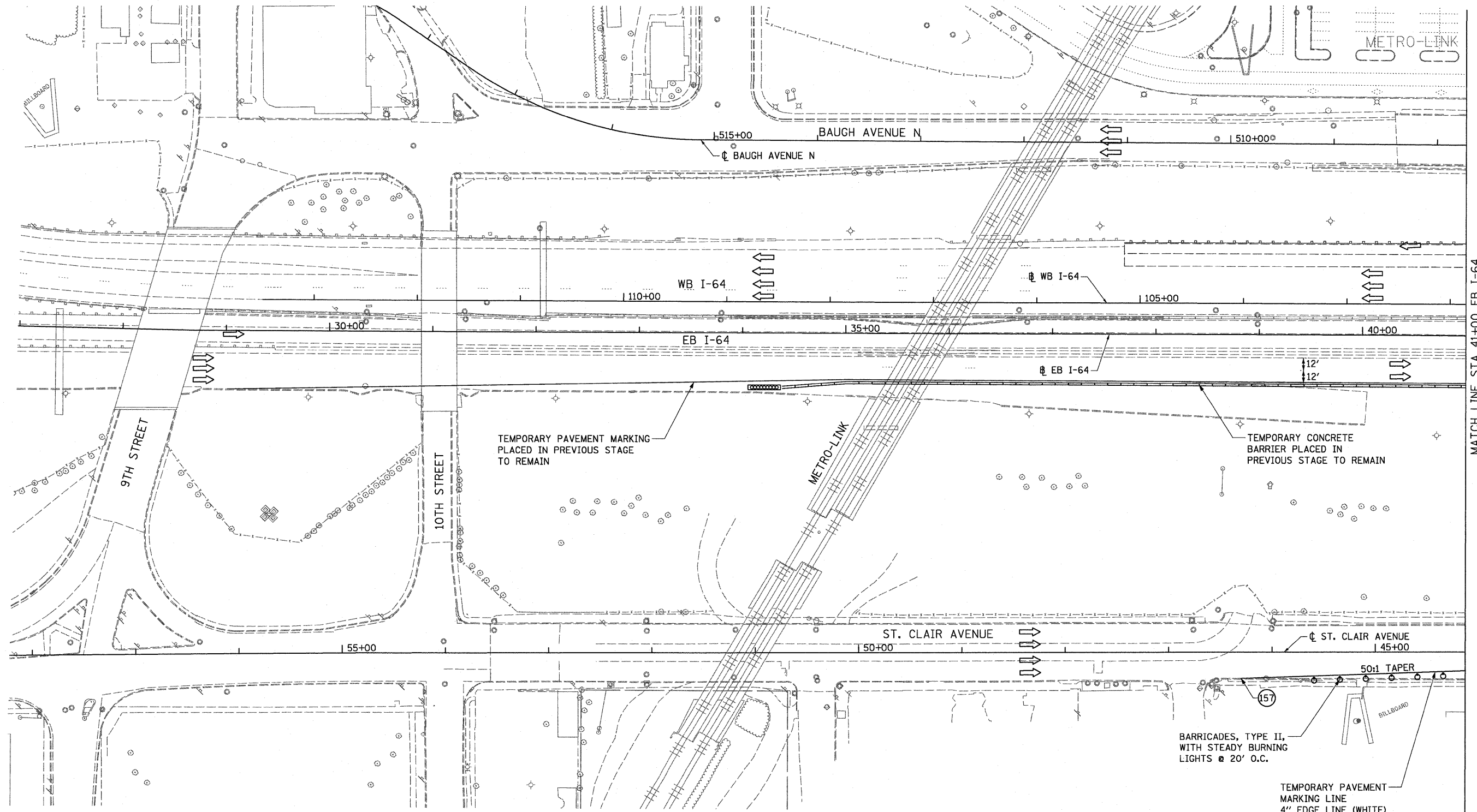
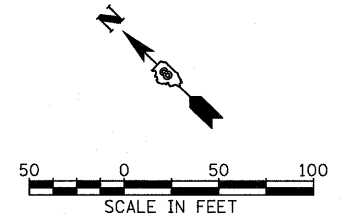
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL - STAGE 4**

SCALE: 1" = 50' SHEET NO. 4 OF 4 SHEETS STA. XX+XX TO STA. XX+XX

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	91
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	

NOTES:  
1. SEE SHEET 4 OF 4 FOR TEMPORARY PAVEMENT MARKING LAYOUT.



MATCH LINE STA 41+00 EB I-64

TEMPORARY PAVEMENT MARKING  
PLACED IN PREVIOUS STAGE  
TO REMAIN

TEMPORARY CONCRETE  
BARRIER PLACED IN  
PREVIOUS STAGE TO REMAIN

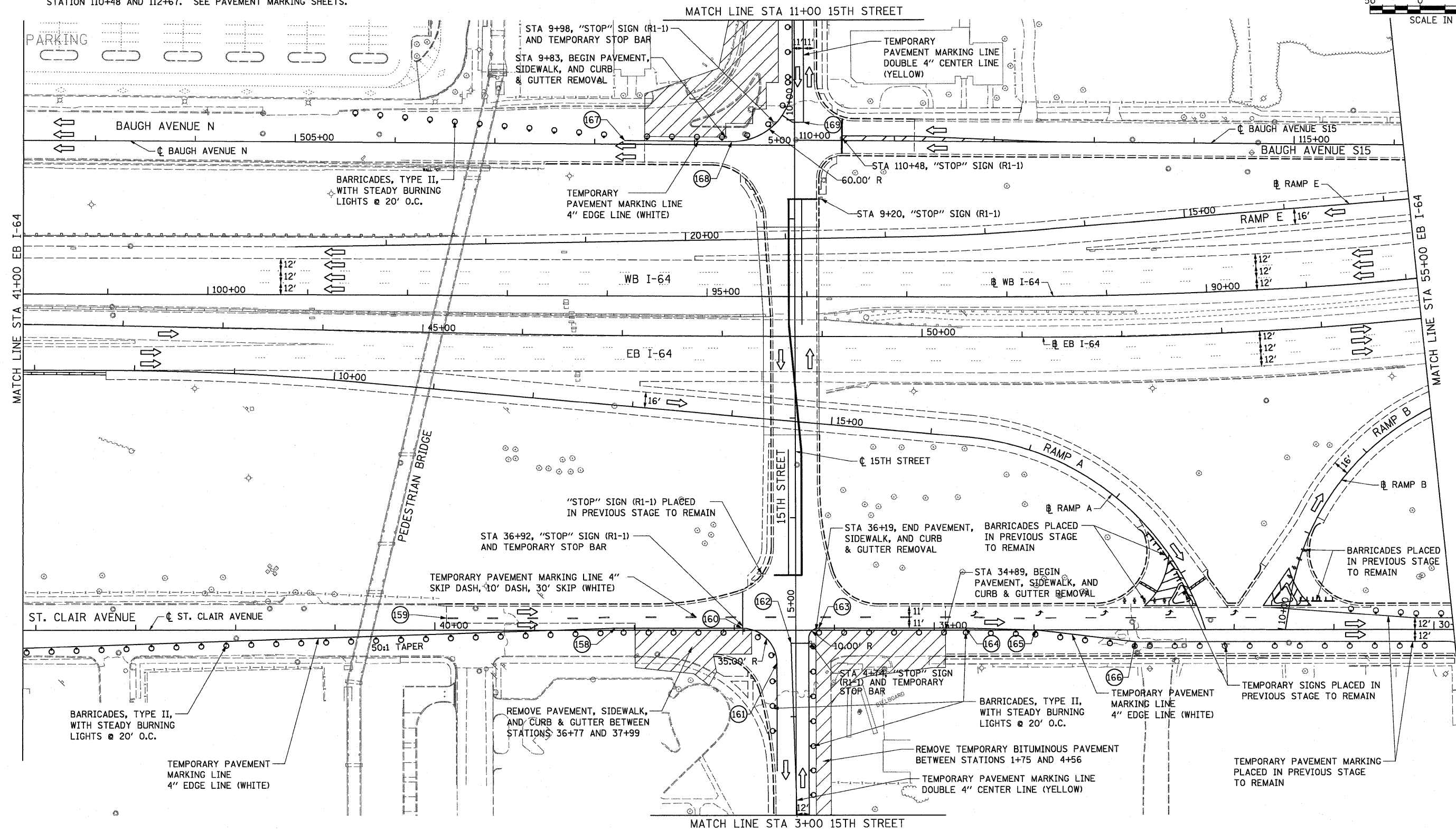
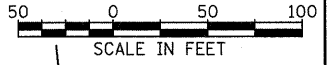
BARRICADES, TYPE II,  
WITH STEADY BURNING  
LIGHTS @ 20' O.C.

TEMPORARY PAVEMENT  
MARKING LINE  
4" EDGE LINE (WHITE)

FILE NAME = #FILE#	USER NAME = IDOT	DESIGNED - TTB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL - STAGE 5A</b>	F.A.I. RTE. 64	SECTION 82-1-2HB	COUNTY ST. CLAIR	TOTAL SHEETS 345	SHEET NO. 92	
	PLOT SCALE = 50,0000' / IN.	DRAWN - TTB	REVISED -			SCALE: 1" = 50'	SHEET NO. 1 OF 4 SHEETS	STA. 27+00 TO STA. 41+00	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	CONTRACT NO. 76C49	
	PLOT DATE = 3/17/2010	CHECKED - JAH	REVISED -			DATE - 3/19/2010	REVISED -				

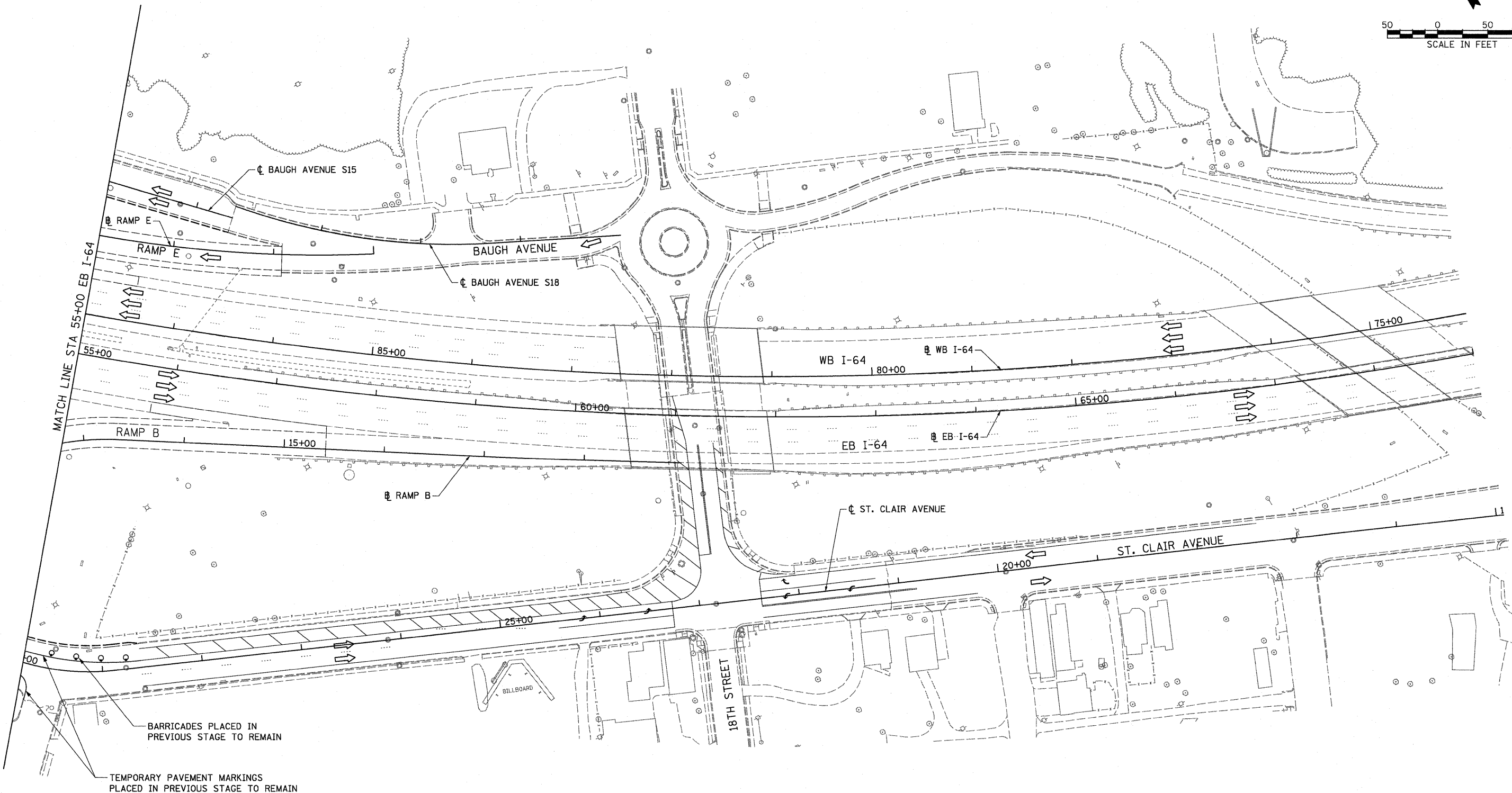
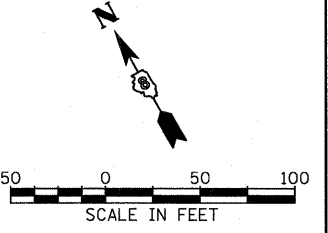
**NOTES:**

1. SEE SHEET 4 OF 4 FOR TEMPORARY PAVEMENT MARKING LAYOUT.
2. PRIOR TO STAGE 5A, PERMANENT CENTER LINE AND EDGE LINE PAVEMENT MARKINGS TO BE PLACED ON 15TH STREET BETWEEN STATION 5+42 AND 9+20. SEE PAVEMENT MARKING SHEETS.
3. PRIOR TO STAGE 5A, PERMANENT CENTER LINE AND EDGE LINE PAVEMENT MARKINGS TO BE PLACED ON BAUGH AVENUE BETWEEN STATION 110+48 AND 112+67. SEE PAVEMENT MARKING SHEETS.



FILE NAME = #FILE#	USER NAME = IDOT	DESIGNED - TTB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL - STAGE 5A</b>	F.A.I. RTE. 64	SECTION 82-1-2HB	COUNTY ST. CLAIR	TOTAL SHEETS 345	SHEET NO. 93
	PLOT SCALE = 50,0000' / IN.	DRAWN - TTB	REVISED -			SCALE: 1" = 50'	SHEET NO. 2 OF 4 SHEETS	STA. 41+00 TO STA. 55+00	FED. ROAD DIST. NO. ILLINOIS	FED. AID PROJECT
PLOT DATE = 3/17/2010	CHECKED - JAH	DATE - 3/19/2010	REVISED -							





FILE NAME =	USER NAME = IDOT	DESIGNED - TTB	REVISED -
*FILE#		DRAWN - TTB	REVISED -
	PLOT SCALE = 50.0000' / IN.	CHECKED - JAH	REVISED -
	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND**  
**TRAFFIC CONTROL - STAGE 5A**

SCALE: 1" = 50'    SHEET NO. 3 OF 4 SHEETS    STA. 55+00    TO STA. 69+00

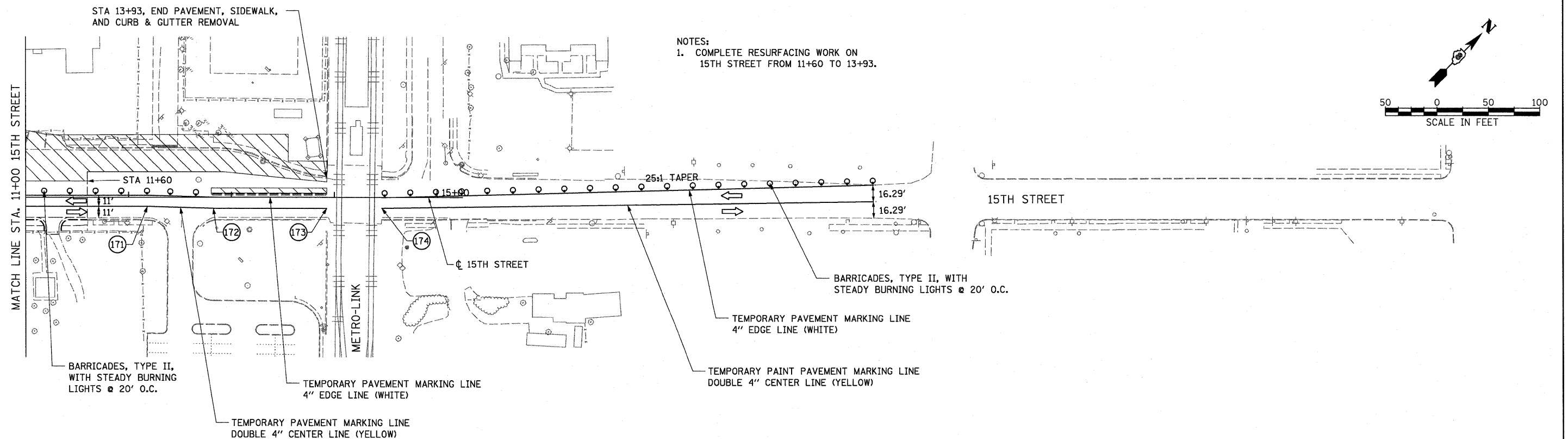
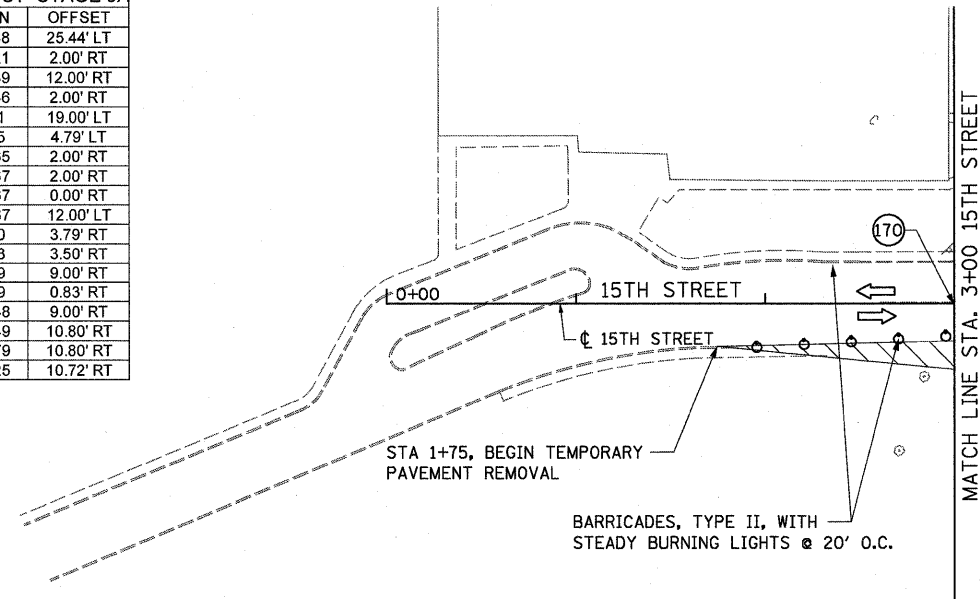
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	94
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	

**STAGE 5A TRAFFIC CONTROL ITEMS**

4" MARKING	24" MARKING (STOP BAR)	WORK ZONE PAVEMENT MARKING REMOVAL	TEMPORARY INFORMATION SIGNING
FOOT	FOOT	SQ FT	SQ FT
4925	72	750	60

**PAVEMENT MARKING LAYOUT-STAGE 5A**

ROADWAY	STATION	OFFSET
157	ST. CLAIR AVE	46+29.88 25.44' LT
158	ST. CLAIR AVE	38+19.21 2.00' RT
159	ST. CLAIR AVE	39+88.39 12.00' RT
160	ST. CLAIR AVE	36+92.46 2.00' RT
161	15TH STREET	4+53.91 19.00' LT
162	15TH STREET	4+73.85 4.79' LT
163	ST. CLAIR AVE	36+15.65 2.00' RT
164	ST. CLAIR AVE	34+68.67 2.00' RT
165	ST. CLAIR AVE	33+98.67 0.00' RT
166	ST. CLAIR AVE	32+98.67 12.00' LT
167	BAUGH AVE	6+70.00 3.79' RT
168	BAUGH AVE	5+62.63 3.50' RT
169	15TH STREET	9+97.59 9.00' RT
170	15TH STREET	2+99.99 0.83' RT
171	15TH STREET	12+18.48 9.00' RT
172	15TH STREET	12+82.49 10.80' RT
173	15TH STREET	13+92.79 10.80' RT
174	15TH STREET	14+46.25 10.72' RT



FILE NAME =	USER NAME = IDOT	DESIGNED - TTB	REVISED -
#FILE#		DRAWN - TTB	REVISED -
	PLOT SCALE = 50.0000' / IN.	CHECKED - JAH	REVISED -
	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL - STAGE 5A			
SCALE: 1" = 50'	SHEET NO. 4 OF 4 SHEETS	STA. XX+XX	TO STA. XX+XX

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	95
FED. ROAD DIST. NO.				ILLINOIS FED. AID PROJECT
				CONTRACT NO. 76C49

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY ON MAY 30, 2003 FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES. THIS PLAN HAS ALSO BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF NPDES PERMIT NUMBER ILR40 FOR DISCHARGES FROM SMALL MUNICIPAL SEPARATE STORM SEWER SYSTEMS IF CHECKED BELOW.

NPDES PERMITS ASSOCIATED WITH THIS PROJECT:

- ILR10
- ILR40 PERMIT NO. 0493

I CERTIFY UNDER PENALTY OF LAW THAT THIS DOCUMENT AND ALL ATTACHMENTS WERE PREPARED UNDER MY DIRECTION OR SUPERVISION IN ACCORDANCE WITH A SYSTEM DESIGNED TO ASSURE THAT QUALIFIED PERSONNEL PROPERLY GATHERED AND EVALUATED THE INFORMATION SUBMITTED. BASED ON MY INQUIRY OF THE PERSON OR PERSONS WHO MANAGE THE SYSTEM, OR THOSE PERSONS DIRECTLY RESPONSIBLE FOR GATHERING THE INFORMATION, THE INFORMATION SUBMITTED IS, TO THE BEST OF MY KNOWLEDGE AND BELIEF, TRUE, ACCURATE AND COMPLETE. I AM AWARE THAT THERE ARE SIGNIFICANT PENALTIES FOR SUBMITTING FALSE INFORMATION, INCLUDING THE POSSIBILITY OF FINE AND IMPRISONMENT FOR KNOWING VIOLATIONS.

MARY C. LAMIE  
 PRINT NAME  
 DEPUTY DIRECTOR OF HIGHWAYS  
 REGION FIVE ENGINEER  
 TITLE  
 IL DEPT. OF TRANSPORTATION  
 AGENCY

*Mary C. Lamie*  
 SIGNATURE  
 March 23, 2010  
 DATE

I. SITE DESCRIPTION:

A. THE FOLLOWING IS A DESCRIPTION OF THE PROJECT LOCATION:

THE PROJECT CONSISTS OF THE PROPOSED IMPROVEMENTS OF 16.9 ACRES OF EAST ST. LOUIS, SPECIFICALLY THE 15TH STREET BRIDGE OVER I-64, ROADWAY CONSTRUCTION OF RAMP A, RAMP B, RAMP E, BAUGH AVENUE, ST. CLAIR AVENUE, AND 15TH STREET APPROACHES.

B. THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

ACTIVITIES WILL INCLUDE THE CONSTRUCTION OF THE 15TH STREET BRIDGE OVER I-64, ROADWAY CONSTRUCTION, STORM SEWER AND DRAINAGE STRUCTURES, PAVED SHOULDERS, COMBINATION CONCRETE CURB AND GUTTER, PAVEMENT MARKING, LIGHTING, LANDSCAPING AND ALL INCIDENTAL AND COLLATERAL WORK NECESSARY TO COMPLETE THE PROJECT AS SHOWN ON THE PLANS.

C. THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

DESCRIPTION OF INTENDED SEQUENCE FOR MAJOR CONSTRUCTION ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE:

- STAGE 1: BEGIN 15TH STREET BRIDGE CONSTRUCTION. RAMP A AND RAMP E CONSTRUCTION. CONSTRUCTION OF TEMPORARY RAMP B PAVEMENT.
- STAGE 2: CONTINUE 15TH STREET BRIDGE CONSTRUCTION. DEMOLISH EXISTING RAMPS A, E, AND PORTIONS OF RAMP B. BEGIN CONSTRUCTION OF INTERSECTIONS OF BAUGH AVENUE AND 15TH STREET AND ST. CLAIR AVENUE AND 15TH STREET.
- STAGE 3: COMPLETE 15TH STREET BRIDGE CONSTRUCTION. DEMOLISH TEMPORARY RAMP B PAVEMENT OF RAMP B. CONTINUE CONSTRUCTION OF INTERSECTIONS OF BAUGH AVENUE AND 15TH STREET AND ST. CLAIR AVENUE
- STAGE 4 & 5: CONTINUE AND COMPLETE STAGED CONSTRUCTION OF INTERSECTIONS OF BAUGH AVENUE AND 15TH STREET AND ST. CLAIR AVENUE

D. THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 15.9 ACRES.

THE TOTAL AREA OF THE SITE THAT IS ESTIMATED WILL BE DISTURBED BY EXCAVATION, GRADING OR OTHER ACTIVITIES IS 13.1 ACRES.

E. THE FOLLOWING IS A WEIGHTED AVERAGE OF THE RUNOFF COEFFICIENT FOR THIS PROJECT AFTER CONSTRUCTION ACTIVITIES ARE COMPLETED: 0.63

F. THE FOLLOWING IS A DESCRIPTION OF THE SOIL TYPES FOUND AT THE PROJECT SITE:

- FINE GRAINED SAND
- MEDIUM GRAINED SAND
- COARSE GRAINED SAND
- SANDY LOAM
- SILTY LOAM
- SILT
- SANDY CLAY
- SANDY CLAY LOAM
- SILTY CLAY
- SILTY CLAY LOAM
- CLAY LOAM
- CLAY

G. THE FOLLOWING IS A DESCRIPTION OF POTENTIALLY EROSION AREAS ASSOCIATED WITH THIS PROJECT:

THE INFIELD AREA BETWEEN BAUGH AVENUE AND PR RAMP E IS POTENTIAL CRITICAL EROSION AREA.

H. THE FOLLOWING IS A DESCRIPTION OF SOIL DISTURBING ACTIVITIES, THEIR LOCATIONS, AND THEIR EROSION FACTORS (E.G. STEEPNESS OF SLOPES, LENGTH OF SLOPES, ETC):

THE NATURE AND PURPOSE OF LAND DISTURBING ACTIVITIES ON THIS PROJECT ARE TO CONSTRUCT THE 15TH BRIDGE OVER I-64 (PROPOSED STRUCTURE NO 082-0377), THE REGRADING OF THE INFIELDS ENCOMPASSED BY RAMP A, RAMP B, RAMP E, AND I-64, THE RECONSTRUCTION OF 15TH STREET, BAUGH AVENUE, ST. CLAIR AVENUE, AND RAMPS. PROPOSED RIGHT-OF-WAY AND EASEMENTS WILL BE REQUIRED TO ACCOMMODATE RECONSTRUCTION OF THE BRIDGE, RAMPS, AND LOCALS. THERE ARE NO SCHEDULED NEIGHBORING ACTIVITIES THAT WILL AFFECT THE SOIL EROSION AND SEDIMENT CONTROL PLANS AND NO OFF-SITE LAND DISTURBING ACTIVITIES.

I. SEE THE EROSION CONTROL PLANS AND/OR DRAINAGE PLANS FOR THIS CONTRACT FOR INFORMATION REGARDING DRAINAGE PATTERNS, APPROXIMATE SLOPES ANTICIPATED BEFORE AND AFTER MAJOR GRADING ACTIVITIES, LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE AND CONTROLS TO PREVENT OFF SITE SEDIMENT TRACKING (TO BE ADDED AFTER CONTRACTOR IDENTIFIES LOCATIONS), AREAS OF SOIL DISTURBANCE, THE LOCATION OF MAJOR STRUCTURAL AND NON-STRUCTURAL CONTROLS IDENTIFIED IN THE PLAN, THE LOCATION OF AREAS WHERE STABILIZATION PRACTICES ARE EXPECTED TO OCCUR, SURFACE WATERS (INCLUDING WETLANDS) AND LOCATIONS WHERE STORM WATER IS DISCHARGED TO SURFACE WATER INCLUDING WETLANDS.

J. THE FOLLOWING IS A LIST OF RECEIVING WATER(S) AND THE ULTIMATE RECEIVING WATER(S), AND AERIAL EXTENT OF WETLAND ACREAGE AT THE SITE. THE LOCATION OF THE RECEIVING WATERS CAN BE FOUND ON THE EROSION AND SEDIMENT CONTROL PLANS:

N/A

K. THE FOLLOWING POLLUTANTS OF CONCERN WILL BE ASSOCIATED WITH THIS CONSTRUCTION PROJECT: (CHECK ALL THAT APPLY)

- SOIL SEDIMENT
- CONCRETE
- CONCRETE TRUCK WASTE
- CONCRETE CURING COMPOUNDS
- SOLID WASTE DEBRIS
- PAINTS
- SOLVENTS
- FERTILIZERS / PESTICIDES
- PETROLEUM (GAS, DIESEL, OIL, KEROSENE, HYDRAULIC OIL/FLUIDS)
- ANTIFREEZE / COOLANTS
- WASTE WATER FROM CLEANING CONSTRUCTION EQUIPMENT
- OTHER (SPECIFY).....
- OTHER (SPECIFY).....
- OTHER (SPECIFY).....
- OTHER (SPECIFY).....
- OTHER (SPECIFY).....

II. CONTROLS

THIS SECTION OF THE PLAN ADDRESSES THE CONTROLS THAT WILL BE IMPLEMENTED FOR EACH OF THE MAJOR CONSTRUCTION ACTIVITIES DESCRIBED IN I.C. ABOVE AND FOR ALL USE AREAS, BORROW SITES, AND WASTE SITES. FOR EACH MEASURE DISCUSSED, THE CONTRACTOR WILL BE RESPONSIBLE FOR ITS IMPLEMENTATION AS INDICATED. THE CONTRACTOR SHALL PROVIDE TO THE RESIDENT ENGINEER A PLAN FOR THE IMPLEMENTATION OF THE MEASURES INDICATED. THE CONTRACTOR, AND SUBCONTRACTORS, WILL NOTIFY THE RESIDENT ENGINEER OF ANY PROPOSED CHANGES, MAINTENANCE, OR MODIFICATIONS TO KEEP CONSTRUCTION ACTIVITIES COMPLIANT WITH THE PERMIT. EACH SUCH CONTRACTOR HAS SIGNED THE REQUIRED CERTIFICATION ON FORMS WHICH WILL BE PROVIDED AT THE PRE-CONSTRUCTION CONFERENCE, AND ARE A PART OF, THIS PLAN:

A. EROSION AND SEDIMENT CONTROL

1. STABILIZED PRACTICES: PROVIDED BELOW IS A DESCRIPTION OF INTERIM AND PERMANENT STABILIZATION PRACTICES, INCLUDING SITE SPECIFIC SCHEDULING OF THE IMPLEMENTATION OF THE PRACTICES. SITE PLANS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES MAY INCLUDE BUT ARE NOT LIMITED TO: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, GEOTEXTILES, SODDING, VEGETATIVE BUFFER STRIPS, PROTECTION OF TREES, PRESERVATION OF MATURE VEGETATION, AND OTHER APPROPRIATE MEASURES. EXCEPT AS PROVIDED BELOW IN II(A)(1)(c) AND II(A)(3), STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 14 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASES ON ALL DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION WILL NOT OCCUR FOR A PERIOD OF 21 OR MORE CALENDAR DAYS.

g. WHERE THE INITIATION OF STABILIZATION MEASURES BY THE 14TH DAY AFTER CONSTRUCTION ACTIVITY TEMPORARILY OR PERMANENTLY CEASES IS PRECLUDED BY SNOW COVER, STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE THEREAFTER.

THE FOLLOWING STABILIZATION PRACTICES WILL BE USED FOR THIS PROJECT: (CHECK ALL THAT APPLY)

- PRESERVATION OF MATURE VEGETATION
- VEGETATED BUFFER STRIPS
- PROTECTION OF TREES
- TEMPORARY EROSION CONTROL SEEDING
- TEMPORARY TURF (SEEDING, CLASS 7)
- TEMPORARY MULCHING
- PERMANENT SEEDING
- EROSION CONTROL BLANKET / MULCHING
- SODDING
- GEOTEXTILES
- OTHER (SPECIFY).....
- OTHER (SPECIFY).....
- OTHER (SPECIFY).....
- OTHER (SPECIFY).....

DESCRIBE HOW THE STABILIZATION PRACTICES LISTED ABOVE WILL BE UTILIZED:

1. TEMPORARY EROSION CONTROL SEEDING - THIS ITEM WILL BE APPLIED TO ALL BARE AREAS EVERY SEVEN DAYS TO MINIMIZE THE AMOUNT OF EXPOSED SURFACE AREAS.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED IF THEY ARE TO REMAIN UNUSED FOR MORE THAN 14 DAYS.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

BARE AND SPARSELY VEGETATED GROUND IN HIGHLY ERODIBLE AREAS AS DETERMINED BY THE ENGINEER SHALL BE TEMPORARILY SEEDED AT THE BEGINNING OF CONSTRUCTION WHERE NO CONSTRUCTION ACTIVITIES ARE EXPECTED WITHIN 7 DAYS.

2. PERMANENT SEEDING - SEEDING, CLASS 2A WILL BE INSTALLED PER IDOT SPECIFICATIONS.

3. EROSION CONTROL BLANKETS/MULCHING - EROSION CONTROL BLANKETS WILL BE INSTALLED OVER FILL SLOPES AND IN HIGH VELOCITY AREAS (I.E. DITCHES) THAT HAVE BEEN BROUGHT TO FINAL GRADE AND SEEDED TO PROTECT SLOPES FROM EROSION AND ALLOW SEEDS TO GERMINATE. MULCH, METHOD 2 WILL BE APPLIED IN RELATIVELY FLAT AREAS TO PROTECT THE DISTURBED AREAS AND PREVENT FURTHER EROSION.

MULCH AS APPLIED TO TEMPORARY EROSION CONTROL SEEDING SHALL BE BY THE METHOD SPECIFIED IN THE CONTRACT AND AT THE DIRECTION OF THE ENGINEER. MULCH WILL BE PAID SEPARATELY AND SHALL CONFORM TO SECTION 251 OF THE STANDARD SPECIFICATIONS.

4. PROTECTION OF TREES/TEMPORARY TREE PROTECTION - THIS ITEM SHALL CONSIST OF ITEMS "TEMPORARY FENCING" AND "TREE TRUCK PROTECTION" AS SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER AND IN ACCORDANCE WITH ARTICLE 201.05 OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

PERMANENT STABILIZATION - ALL AREAS DISTURBED BY CONSTRUCTION WILL BE STABILIZED WITH PERMANENT SEEDING IMMEDIATELY FOLLOWING THE FINISHED GRADING. EROSION CONTROL BLANKETS WILL BE INSTALLED OVER FILL SLOPES WHICH HAVE BEEN BROUGHT TO FINAL GRADE AND HAVE BEEN SEEDED TO PROTECT THE SLOPES FROM RILL AND GULLY EROSION AND ALLOW SEED TO GERMINATE PROPERLY. MULCH, METHOD 2 WILL BE USED ON RELATIVELY FLAT AREAS.

2. STRUCTURAL PRACTICES: PROVIDED BELOW IS A DESCRIPTION OF STRUCTURAL PRACTICES THAT WILL BE IMPLEMENTED, TO THE DEGREE ATTAINABLE, TO DIVERT FLOWS FROM EXPOSED SOILS, STORE FLOWS OR OTHERWISE LIMIT RUNOFF AND THE DISCHARGE OF POLLUTANTS FROM EXPOSED AREAS OF THE SITE. SUCH PRACTICES MAY INCLUDE BUT ARE NOT LIMITED TO: PERIMETER EROSION BARRIER, EARTH DIKES, DRAINAGE SWALES, SEDIMENT TRAPS, DITCH CHECKS, SUBSURFACE DRAINS, PIPE SLOPE DRAINS, LEVEL SPREADERS, GABIONS, AND TEMPORARY OR PERMANENT SEDIMENT BASINS. THE INSTALLATION OF THESE DEVICES MAY BE SUBJECT TO SECTION 404 OF THE CLEAN WATER ACT.

THE FOLLOWING STRUCTURAL PRACTICES WILL BE USED FOR THIS PROJECT:(CHECK ALL THAT APPLY)

- PERIMETER EROSION BARRIER
- TEMPORARY DITCH CHECK
- STORM DRAIN INLET PROTECTION
- SEDIMENT TRAP
- TEMPORARY PIPE SLOPE DRAIN
- TEMPORARY SEDIMENT BASIN
- TEMPORARY STREAM CROSSING
- STABILIZED CONSTRUCTION EXITS
- TURF REINFORCEMENT MATS
- PERMANENT CHECK DAMS
- PERMANENT SEDIMENT BASIN
- AGGREGATE DITCH
- PAVED DITCH
- ROCK OUTLET PROTECTION
- RIPRAP
- GABIONS
- SLOPE MATTRESS
- RETAINING WALLS
- SLOPE WALLS
- CONCRETE REVETMENT MATS
- LEVEL SPREADERS
- OTHER (SPECIFY).....
- OTHER (SPECIFY).....
- OTHER (SPECIFY).....
- OTHER (SPECIFY).....
- OTHER (SPECIFY).....

DESCRIBE HOW THE STRUCTURAL PRACTICES LISTED ABOVE WILL BE UTILIZED:

1. PERIMETER EROSION BARRIER - SILT FENCES WILL BE PLACED ALONG THE CONSTRUCTION LIMIT TO PREVENT SILT AND RUNOFF FROM LEAVING THE SITE.

CONSTRUCT AT BEGINNING OF CONSTRUCTION. REMOVE AT END OF CONSTRUCTION.

2. STORM DRAIN INLET PROTECTION - INLET AND PIPE PROTECTION WILL BE PROVIDED FOR STORM SEWERS, CULVERTS, IN ALL INLETS, CATCH BASINS AND MANHOLES DURING CONSTRUCTION AND WILL BE CLEANED ON A REGULAR BASIS.

3. TEMPORARY DITCH CHECKS - DITCH CHECKS WILL BE PLACED IN SWALES WHERE RUNOFF VELOCITY IS HIGH. ALL STRUCTURAL PRACTICES ARE SHOWN IN DETAIL ON THE EROSION CONTROL PLANS.

TEMPORARY DITCH CHECKS SHALL BE LOCATED AT EVERY 1.5 FT. FALL/RISE IN DITCH GRADE.

TEMPORARY DITCH CHECKS, AGGREGATE USES GRADING NO. 3- REMOVE AT END OF CONSTRUCTION.

STRAW BALES, HAY BALES, PERIMETER EROSION BARRIER AND SILT FENCE WILL NOT BE PERMITTED FOR TEMPORARY OR PERMANENT DITCH CHECKS. DITCH CHECKS SHALL BE COMPOSED OF AGGREGATE (IF SPECIFIED), ENVIROBERM, TRIANGULAR SILT DIKES, GEORIDGE AND ROLLED EXCELSIOR.

4. RIPRAP - STONE RIPRAP WITH FILTER FABRIC WILL BE USED AS PROTECTION AT THE DISCHARGE END OF ALL CULVERT END SECTIONS AND AS INLET/OUTLET PROTECTION TO PREVENT SCOURING AT THE END OF PIPES AND PREVENT DOWNSTREAM EROSION.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, TEMPORARY DITCH CHECKS, INLET AND PIPE PROTECTION, AND PERIMETER EROSION BARRIER SHALL BE INSTALLED AS CALLED OUT IN THIS PLAN AND DIRECTED BY THE ENGINEER.

ALL EROSION CONTROL PRODUCTS FURNISHED SHALL BE SPECIFICALLY RECOMMENDED BY THE MANUFACTURER FOR THE USE SPECIFIED IN THE EROSION CONTROL PLAN. PRIOR TO THE APPROVAL AND USE OF THE PRODUCT, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A NOTARIZED CERTIFICATION BY THE PRODUCER STATING THE INTENDED USE OF THE PRODUCT AND THAT THE PHYSICAL PROPERTIES REQUIRED FOR THIS APPLICATION ARE MET OR EXCEEDED. THE CONTRACTOR SHALL PROVIDE MANUFACTURER INSTALLATION PROCEDURES TO FACILITATE THE ENGINEER IN CONSTRUCTION INSPECTION.

FILE NAME =	USER NAME = IDOT	DESIGNED - GDO	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STORM WATER POLLUTION PREVENTION PLAN</b>	F.A.I. SHEETS	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
#FILE#		DRAWN - GDO	REVISED -			64	82-1-2HB	ST. CLAIR	345	96	
	PLOT SCALE = 50.0000' / IN.	CHECKED - TTB	REVISED -			CONTRACT NO. 76C49					
	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -			SCALE: NONE	SHEET NO. 1 OF 2 SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT

3. STORM WATER MANAGEMENT: PROVIDED BELOW IS A DESCRIPTION OF MEASURES THAT WILL BE INSTALLED DURING THE CONSTRUCTION PROCESS TO CONTROL POLLUTANTS IN STORM WATER DISCHARGES THAT WILL OCCUR AFTER CONSTRUCTION OPERATIONS HAVE BEEN COMPLETED. THE INSTALLATION OF THESE DEVICES MAY BE SUBJECT TO SECTION 404 OF THE CLEAN WATER ACT.

- a. SUCH PRACTICES MAY INCLUDE BUT ARE NOT LIMITED TO: STORM WATER DETENTION STRUCTURES (INCLUDING WET PONDS), STORM WATER RETENTION STRUCTURES, FLOW ATTENUATION BY USE OF OPEN VEGETATED SWALES AND NATURAL DEPRESSIONS, INFILTRATION OF RUNOFF ON SITE, AND SEQUENTIAL SYSTEMS (WHICH COMBINE SEVERAL PRACTICES). THE PRACTICES SELECTED FOR IMPLEMENTATION WERE DETERMINED ON THE BASIS OF THE TECHNICAL GUIDANCE IN SECTION 59-8 (EROSION AND SEDIMENT CONTROL) IN CHAPTER 59 (LANDSCAPE DESIGN AND EROSION CONTROL) OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION BUREAU OF DESIGN AND ENVIRONMENT MANUAL. IF PRACTICES OTHER THAN THOSE DISCUSSED IN SECTION 59-8 ARE SELECTED FOR IMPLEMENTATION OR IF PRACTICES ARE APPLIED TO SITUATIONS DIFFERENT FROM THOSE COVERED IN SECTION 59-8, THE TECHNICAL BASIS FOR SUCH DECISIONS WILL BE EXPLAINED BELOW.
- b. VELOCITY DISSIPATION DEVICES WILL BE PLACED AT DISCHARGE LOCATIONS AND ALONG THE LENGTH OF ANY OUTFALL CHANNEL AS NECESSARY TO PROVIDE A NON-EROSIVE VELOCITY FLOW FROM THE STRUCTURE TO A WATER COURSE SO THAT THE NATURAL PHYSICAL AND BIOLOGICAL CHARACTERISTICS AND FUNCTIONS ARE MAINTAINED AND PROTECTED (E.G. MAINTENANCE OF HYDROLOGIC CONDITIONS SUCH AS THE HYDROPERIOD AND HYDRODYNAMICS PRESENT PRIOR TO THE INITIATION OF CONSTRUCTION ACTIVITIES).

DESCRIPTION OF STORM WATER MANAGEMENT CONTROLS:

PERMANENT AGGREGATE DITCH CHECKS TO BE PROVIDED IN CONJUNCTION WITH STRUCTURES 4-01, 4-02, 6-11, 6-12, 7-06, AND 7-07 AS DESIGNATED IN THE DRAINAGE AND UTILITY PLAN. 5" RESTRICTOR PIPE TO BE PROVIDED AT FLOW LINE OF PERMANENT DITCH CHECK.

4. OTHER CONTROLS:

- a. VEHICLE ENTRANCES AND EXITS - STABILIZED CONSTRUCTION ENTRANCES AND EXITS MUST BE CONSTRUCTED TO PREVENT TRACKING OF SEDIMENTS ONTO ROADWAYS.

THE CONTRACTOR WILL PROVIDE THE RESIDENT ENGINEER WITH A WRITTEN PLAN IDENTIFYING THE LOCATION OF STABILIZED ENTRANCES AND EXITS AND THE PROCEDURES (S)HE WILL USE TO CONSTRUCT AND MAINTAIN THEM.

- b. MATERIAL DELIVERY, STORAGE, AND USE - THE FOLLOWING BMPs SHALL BE IMPLEMENTED TO HELP PREVENT DISCHARGES OF CONSTRUCTION MATERIALS DURING DELIVERY, STORAGE, AND USE:

- ALL PRODUCTS DELIVERED TO THE PROJECT SITE MUST BE PROPERLY LABELED.
- WATER TIGHT SHIPPING CONTAINERS AND/OR SEMI TRAILERS SHALL BE USED TO STORE HAND TOOLS, SMALL PARTS, AND MOST CONSTRUCTION MATERIALS THAT CAN BE CARRIED BY HAND, SUCH AS PAINT CANS, SOLVENTS, AND GREASE.
- A STORAGE/CONTAINMENT FACILITY SHOULD BE CHOSEN FOR LARGER ITEMS SUCH AS DRUMS AND ITEMS SHIPPED OR STORED ON PALLETS. SUCH MATERIAL IS TO BE COVERED BY A TIN ROOF OR LARGE SHEETS OF PLASTIC TO PREVENT PRECIPITATION FROM COMING IN CONTACT WITH THE PRODUCTS BEING STORED.
- LARGE ITEMS SUCH AS LIGHT STANDS, FRAMING MATERIALS AND LUMBER SHALL BE STORED IN THE OPEN IN A GENERAL STORAGE AREA. SUCH MATERIAL SHALL BE ELEVATED WITH WOOD BLOCKS TO MINIMIZE CONTACT WITH STORM WATER RUNOFF.
- SPILL CLEAN-UP MATERIALS, MATERIAL SAFETY DATA SHEETS, AN INVENTORY OF MATERIALS, AND EMERGENCY CONTACT NUMBERS SHALL BE MAINTAINED AND STORED IN ONE DESIGNATED AREA AND EACH CONTRACTOR IS TO INFORM HIS/HER EMPLOYEES AND THE RESIDENT ENGINEER OF THIS LOCATION.

- c. STOCKPILE MANAGEMENT - BMPs SHALL BE IMPLEMENTED TO REDUCE OR ELIMINATE POLLUTION OF STORM WATER FROM STOCKPILES OF SOIL AND PAVING MATERIALS SUCH AS BUT NOT LIMITED TO PORTLAND CEMENT CONCRETE RUBBLE, ASPHALT CONCRETE, ASPHALT CONCRETE RUBBLE, AGGREGATE BASE, AGGREGATE SUB BASE, AND PRE-MIXED AGGREGATE. THE FOLLOWING BMPs MAY BE CONSIDERED:

- PERIMETER EROSION BARRIER
- TEMPORARY SEEDING
- TEMPORARY MULCH
- PLASTIC COVERS
- SOIL BINDERS
- STORM DRAIN INLET PROTECTION

THE CONTRACTOR WILL PROVIDE THE RESIDENT ENGINEER WITH A WRITTEN PLAN OF THE PROCEDURES (S)HE WILL USE ON THE PROJECT AND HOW THEY WILL BE MAINTAINED.

- d. WASTE DISPOSAL. NO MATERIALS, INCLUDING BUILDING MATERIALS, SHALL BE DISCHARGED INTO WATERS OF THE STATE, EXCEPT AS AUTHORIZED BY A SECTION 404 PERMIT.
- e. THE PROVISIONS OF THIS PLAN SHALL ENSURE AND DEMONSTRATE COMPLIANCE WITH APPLICABLE STATE AND/OR LOCAL WASTE DISPOSAL, SANITARY SEWER OR SEPTIC SYSTEM REGULATIONS.
- f. THE CONTRACTOR SHALL PROVIDE A WRITTEN AND GRAPHIC PLAN TO THE RESIDENT ENGINEER IDENTIFYING WHERE EACH OF THE ABOVE AREAS WILL BE LOCATED AND HOW THEY ARE TO BE MANAGED.

5. APPROVED STATE OR LOCAL LAWS

THE MANAGEMENT PRACTICES, CONTROLS AND PROVISIONS CONTAINED IN THIS PLAN WILL BE IN ACCORDANCE WITH IDOT SPECIFICATIONS, WHICH ARE AT LEAST AS PROTECTIVE AS THE REQUIREMENTS CONTAINED IN THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY'S ILLINOIS URBAN MANUAL, 1995. PROCEDURES AND REQUIREMENTS SPECIFIED IN APPLICABLE SEDIMENT AND EROSION SITE PLANS OR STORM WATER MANAGEMENT PLANS APPROVED BY LOCAL OFFICIALS SHALL BE DESCRIBED OR INCORPORATED BY REFERENCE IN THE SPACE PROVIDED BELOW. REQUIREMENTS SPECIFIED IN SEDIMENT AND EROSION SITE PLANS, SITE PERMITS, STORM WATER MANAGEMENT SITE PLANS OR SITE PERMITS APPROVED BY LOCAL OFFICIALS THAT ARE APPLICABLE TO PROTECTING SURFACE WATER RESOURCES ARE, UPON SUBMITTAL OF AN NOI, TO BE AUTHORIZED TO DISCHARGE UNDER PERMIT ILR10 INCORPORATED BY REFERENCE AND ARE ENFORCEABLE UNDER THIS PERMIT EVEN IF THEY ARE NOT SPECIFICALLY INCLUDED IN THE PLAN.

DESCRIPTION OF PROCEDURES AND REQUIREMENTS SPECIFIED IN APPLICABLE SEDIMENT AND EROSION SITE PLANS OR STORM WATER MANAGEMENT PLANS APPROVED BY LOCAL OFFICIALS:

ALL MANAGEMENT PRACTICES, CONTROLS, AND OTHER PROVISIONS PROVIDED IN THIS PLAN ARE IN ACCORDANCE WITH "IDOT STANDARD SPECIFICATION FOR ROAD AND BRIDGE CONSTRUCTION AND THE ILLINOIS URBAN MANUAL".

III. MAINTENANCE:

THE FOLLOWING IS A DESCRIPTION OF PROCEDURES THAT WILL BE USED TO MAINTAIN, IN GOOD AND EFFECTIVE OPERATING CONDITIONS, THE VEGETATION, EROSION AND SEDIMENT CONTROL MEASURES AND OTHER PROTECTIVE MEASURES IDENTIFIED IN THIS PLAN.

1. SEEDING - ALL ERODIBLE BARE EARTH WILL BE TEMPORARILY SEEDED ON A WEEKLY BASIS TO MINIMIZE THE AMOUNT OF ERODIBLE SURFACE WITHIN THE CONTRACT LIMITS.
2. PERIMETER EROSION BARRIER - SEDIMENT WILL BE REMOVED IF THE INTEGRITY OF THE FENCING IS IN JEOPARDY AND ANY FENCING KNOCKED DOWN WILL BE REPAIRED IMMEDIATELY.
3. EROSION CONTROL BLANKET/MULCHING - ANY AREAS THAT FAIL WILL BE REPAIRED IMMEDIATELY.
4. PROTECTION OF TREES/TEMPORARY TREE PROTECTION - ANY PROTECTIVE MEASURES WHICH ARE KNOCKED DOWN WILL BE REPAIRED IMMEDIATELY.
5. DITCH CHECKS - SEDIMENT WILL BE REMOVED IF THE INTEGRITY OF THE DITCH CHECK IS IN JEOPARDY. ANY DITCH CHECKS WHICH FAIL WILL BE REPAIRED OR REPLACED IMMEDIATELY.

THE RESIDENT ENGINEER WILL PROVIDE MAINTENANCE GUIDES TO THE CONTRACTOR FOR THESE PRACTICES. ALL MAINTENANCE OF EROSION CONTROL SYSTEMS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR UNTIL CONSTRUCTION IS COMPLETE AND ACCEPTED BY IDOT AFTER FINAL INSPECTION. ALL LOCATIONS WHERE VEHICLES ENTER AND EXIT THE CONSTRUCTION SITE AND ALL OTHER AREAS SUBJECT TO EROSION SHOULD ALSO BE INSPECTED PERIODICALLY.

INSPECTION OF THESE AREAS SHALL BE MADE AT LEAST ONCE EVERY SEVEN DAYS AND WITHIN 24 HOURS OF THE END OF EACH 0.5 INCHES OR GREATER RAINFALL, OR AN EQUIVALENT SNOWFALL. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE CONSTRUCTION FIELD ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT EROSION CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR THE TEMPORARY EROSION CONTROL SYSTEM.

IV. INSPECTIONS

QUALIFIED PERSONNEL SHALL INSPECT DISTURBED AREAS OF THE CONSTRUCTION SITE WHICH HAVE NOT YET BEEN FINALLY STABILIZED, STRUCTURAL CONTROL MEASURES, AND LOCATIONS WHERE VEHICLES AND EQUIPMENT ENTER AND EXIT THE SITE. SUCH INSPECTIONS SHALL BE CONDUCTED AT LEAST ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A STORM THAT IS 0.5 INCHES OR GREATER OR EQUIVALENT SNOWFALL.

- A. DISTURBED AREAS, USE AREAS (STORAGE OF MATERIALS, STOCKPILES, MACHINE MAINTENANCE FUELING, ETC.), BORROW SITES, AND WASTE SITES SHALL BE INSPECTED FOR EVIDENCE OF, OR THE POTENTIAL FOR, POLLUTANTS ENTERING THE DRAINAGE SYSTEM. EROSION AND SEDIMENT CONTROL MEASURES IDENTIFIED IN THE PLAN SHALL BE OBSERVED TO ENSURE THAT THEY ARE OPERATING CORRECTLY. DISCHARGE LOCATIONS OR POINTS THAT ARE ACCESSIBLE, SHALL BE INSPECTED TO ASCERTAIN WHETHER EROSION CONTROL MEASURES ARE EFFECTIVE IN PREVENTING SIGNIFICANT IMPACTS TO RECEIVING WATERS. LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFF SITE SEDIMENT TRACKING.
- B. BASED ON THE RESULTS OF THE INSPECTION, THE DESCRIPTION OF POTENTIAL POLLUTANT SOURCES IDENTIFIED IN SECTION I ABOVE AND POLLUTION PREVENTION MEASURES IDENTIFIED IN SECTION II ABOVE SHALL BE REVISED AS APPROPRIATE AS SOON AS PRACTICABLE AFTER SUCH INSPECTION. ANY CHANGES TO THIS PLAN RESULTING FROM THE REQUIRED INSPECTIONS SHALL BE IMPLEMENTED WITHIN 1/2 HOUR TO 1 WEEK BASED ON THE URGENCY OF THE SITUATION. THE RESIDENT ENGINEER WILL NOTIFY THE CONTRACTOR OF THE TIME REQUIRED TO IMPLEMENT SUCH ACTIONS THROUGH THE WEEKLY INSPECTION REPORT.
- C. A REPORT SUMMARIZING THE SCOPE OF THE INSPECTION, NAME(S) AND QUALIFICATIONS OF PERSONNEL MAKING THE INSPECTION, THE DATE(S) OF THE INSPECTION, MAJOR OBSERVATIONS RELATING TO THE IMPLEMENTATION OF THIS STORM WATER POLLUTION PREVENTION PLAN, AND ACTIONS TAKEN IN ACCORDANCE WITH SECTION IV(B) SHALL BE MADE AND RETAINED AS PART OF THE PLAN FOR AT LEAST THREE (3) YEARS AFTER THE DATE OF THE INSPECTION. THE REPORT SHALL BE SIGNED IN ACCORDANCE WITH PART VI. G OF THE GENERAL PERMIT.
- D. IF ANY VIOLATION OF THE PROVISIONS OF THIS PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION WORK COVERED BY THIS PLAN, THE RESIDENT ENGINEER SHALL COMPLETE AND FILE AN "INCIDENCE OF NONCOMPLIANCE" (ION) REPORT FOR THE IDENTIFIED VIOLATION. THE RESIDENT ENGINEER SHALL USE FORMS PROVIDED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY AND SHALL INCLUDE SPECIFIC INFORMATION ON THE CAUSE OF NONCOMPLIANCE, ACTIONS WHICH WERE TAKEN TO PREVENT ANY FURTHER CAUSES OF NONCOMPLIANCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NONCOMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI. G OF THE GENERAL PERMIT. THE INCIDENCE OF NONCOMPLIANCE SHALL BE MAILED TO THE FOLLOWING ADDRESS:

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY  
DIVISION OF WATER POLLUTION CONTROL  
ATTN: COMPLIANCE ASSURANCE SECTION  
1021 NORTH GRAND EAST  
POST OFFICE BOX 19276  
SPRINGFIELD, ILLINOIS 62794-9276

V. NON-STORM WATER DISCHARGES:

EXCEPT FOR FLOWS FROM FIRE FIGHTING ACTIVITIES, SOURCES OF NON-STORM WATER THAT IS COMBINED WITH STORM WATER DISCHARGES ASSOCIATED WITH THE INDUSTRIAL ACTIVITY ADDRESSED IN THIS PLAN MUST BE DESCRIBED BELOW. APPROPRIATE POLLUTION PREVENTION MEASURES, AS DESCRIBED BELOW, WILL BE IMPLEMENTED FOR THE NON-STORM WATER COMPONENT(S) OF THE DISCHARGE.

- A. SPILL PREVENTION AND CONTROL - BMPs SHALL BE IMPLEMENTED TO CONTAIN AND CLEAN-UP SPILLS AND PREVENT MATERIAL DISCHARGES TO THE STORM DRAIN SYSTEM. THE CONTRACTOR SHALL PRODUCE A WRITTEN PLAN STATING HOW HIS/HER COMPANY WILL PREVENT, REPORT, AND CLEAN UP SPILLS AND PROVIDE A COPY TO ALL OF HIS/HER EMPLOYEES AND THE RESIDENT ENGINEER. THE CONTRACTOR SHALL NOTIFY ALL OF HIS/HER EMPLOYEES ON THE PROPER PROTOCOL FOR REPORTING SPILLS. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER OF ANY SPILLS IMMEDIATELY.

- B. CONCRETE RESIDUALS AND WASHOUT WASTES - THE FOLLOWING BMPs SHALL BE IMPLEMENTED TO CONTROL RESIDUAL CONCRETE, CONCRETE SEDIMENTS, AND RINSE WATER:

1. TEMPORARY CONCRETE WASHOUT FACILITIES SHALL BE CONSTRUCTED FOR RINSING OUT CONCRETE TRUCKS. SIGNS SHALL BE INSTALLED DIRECTING CONCRETE TRUCK DRIVERS WHERE DESIGNATED WASHOUT FACILITIES ARE LOCATED.
2. THE CONTRACTOR SHALL HAVE THE LOCATION OF TEMPORARY CONCRETE WASHOUT FACILITIES APPROVED BY THE RESIDENT ENGINEER.
3. ALL TEMPORARY CONCRETE WASHOUT FACILITIES ARE TO BE INSPECTED BY THE CONTRACTOR AFTER EACH USE AND ALL SPILLS MUST BE REPORTED TO THE RESIDENT ENGINEER AND CLEANED UP IMMEDIATELY.
4. CONCRETE WASTE SOLIDS/LIQUIDS SHALL BE DISPOSED OF PROPERLY.

- C. LITTER MANAGEMENT - A PROPER NUMBER OF DUMPSTERS SHALL BE PROVIDED ON SITE TO HANDLE DEBRIS AND LITTER ASSOCIATED WITH THE PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING HIS/HER EMPLOYEES PLACE ALL LITTER INCLUDING MARKING PAINT CANS, SODA CANS, FOOD WRAPPERS, WOOD LATHE, MARKING RIBBON, CONSTRUCTION STRING, AND ALL OTHER CONSTRUCTION RELATED LITTER IN THE PROPER DUMPSTERS.

- D. VEHICLE AND EQUIPMENT CLEANING - VEHICLES AND EQUIPMENT ARE TO BE CLEANED IN DESIGNATED AREAS ONLY, PREFERABLY OFF SITE.

- E. VEHICLE AND EQUIPMENT FUELING - A VARIETY OF BMPs CAN BE IMPLEMENTED DURING FUELING OF VEHICLES AND EQUIPMENT TO PREVENT POLLUTION. THE CONTRACTOR SHALL INFORM THE RESIDENT ENGINEER AS TO WHICH BMPs WILL BE USED ON THE PROJECT. THE CONTRACTOR SHALL INFORM THE RESIDENT ENGINEER HOW (S)HE WILL BE INFORMING HIS/HER EMPLOYEES OF THESE BMPs (I.E. SIGNS, TRAINING, ETC.). BELOW ARE A FEW EXAMPLES OF THESE BMPs:






1. CONTAINMENT
2. SPILL PREVENTION AND CONTROL
3. USE OF DRIP PANS AND ABSORBENTS
4. AUTOMATIC SHUT-OFF NOZZLES
5. TOPPING OFF RESTRICTIONS
6. LEAK INSPECTION AND REPAIR

- F. VEHICLE AND EQUIPMENT MAINTENANCE - ON SITE MAINTENANCE MUST BE PERFORMED IN ACCORDANCE WITH ALL ENVIRONMENTAL LAWS SUCH AS PROPER STORAGE AND NO DUMPING OF OLD ENGINE OIL OR OTHER FLUIDS ON SITE.

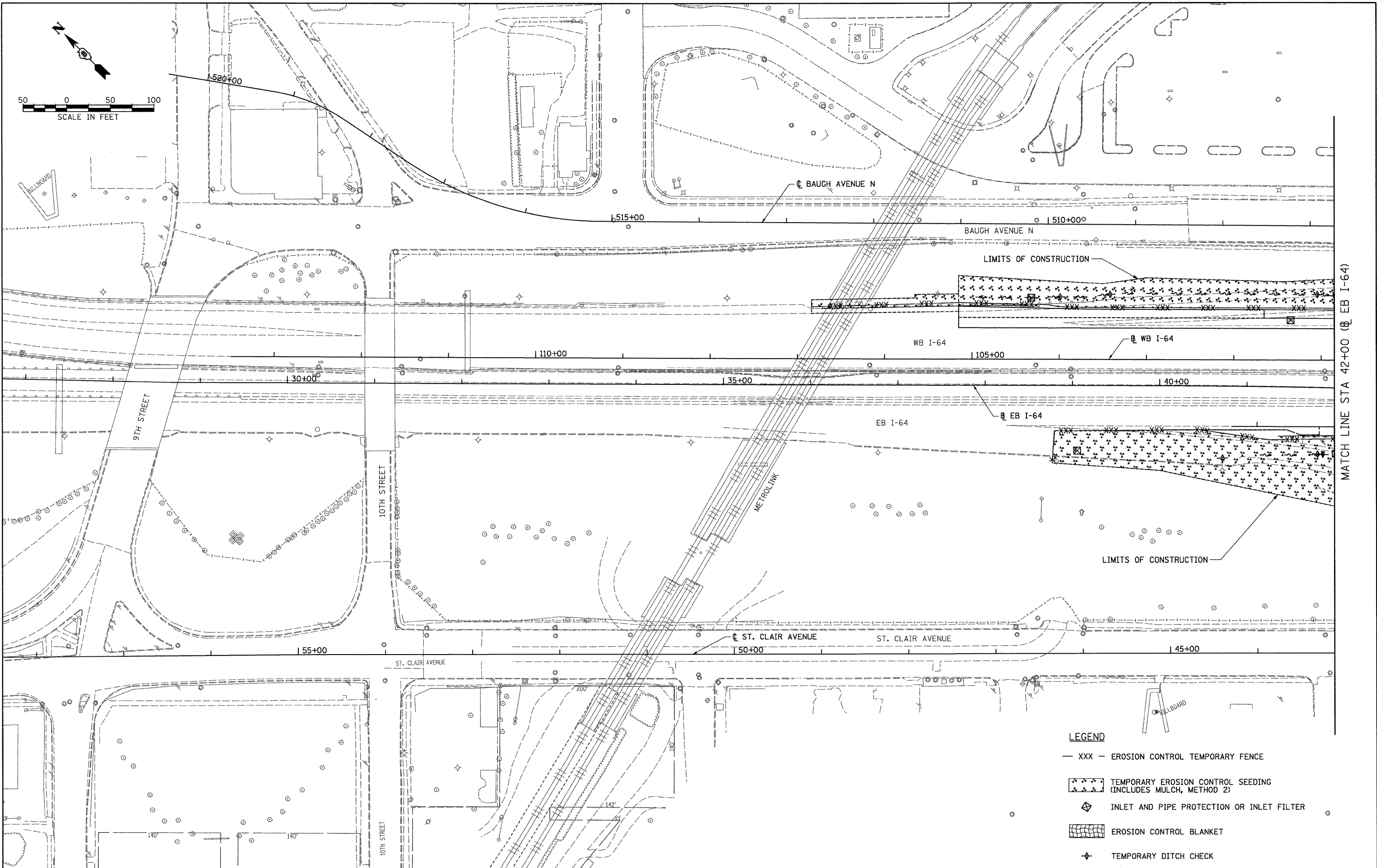
VI. FAILURE TO COMPLY:

FAILURE TO COMPLY WITH ANY PROVISIONS OF THIS STORM WATER POLLUTION PREVENTION PLAN WILL RESULT IN THE IMPLEMENTATION OF AN EROSION AND SEDIMENT CONTROL DEFICIENCY DEDUCTION AGAINST THE CONTRACTOR AND/OR PENALTIES UNDER THE NPDES PERMIT WHICH COULD BE PASSED ONTO THE CONTRACTOR.

LEGEND

-  TEMPORARY DITCH CHECK- ROLLED EXCELSIOR, SILT WEDGES
-  TEMPORARY DITCH CHECK- AGGREGATE
-  EROSION CONTROL BLANKET
-  PERIMETER EROSION BARRIER- SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
-  INLET AND PIPE PROTECTION- STRAW BALES, FILTER FABRIC, AGGREGATES

FILE NAME =	USER NAME = IDOT	DESIGNED - GDO	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STORM WATER POLLUTION PREVENTION PLAN</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN - GDO	REVISED -					64	82-1-2HB	ST. CLAIR	345	97
	PLOT SCALE = 50.0000' / IN.	CHECKED - TTB	REVISED -		SCALE: NONE			SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.	CONTRACT NO. 76C49	
	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -					FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



MATCH LINE STA 42+00 (EB I-64)

- LEGEND**
- XXX — EROSION CONTROL TEMPORARY FENCE
  - [Hatched Box] TEMPORARY EROSION CONTROL SEEDING (INCLUDES MULCH, METHOD 2)
  - [Diamond with X] INLET AND PIPE PROTECTION OR INLET FILTER
  - [Grid Pattern Box] EROSION CONTROL BLANKET
  - [Diamond with Circle] TEMPORARY DITCH CHECK

FILE NAME =	USER NAME = IDOT	DESIGNED - TTB	REVISED -
#FILE#		DRAWN - TTB	REVISED -
	PLOT SCALE = 50.0000' / IN.	CHECKED - JAH	REVISED -
	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -

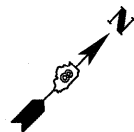
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EROSION CONTROL PLAN AND QUANTITIES**

SCALE: 1"=50'    SHEET NO. 1 OF 4 SHEETS    STA.    TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	98
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	

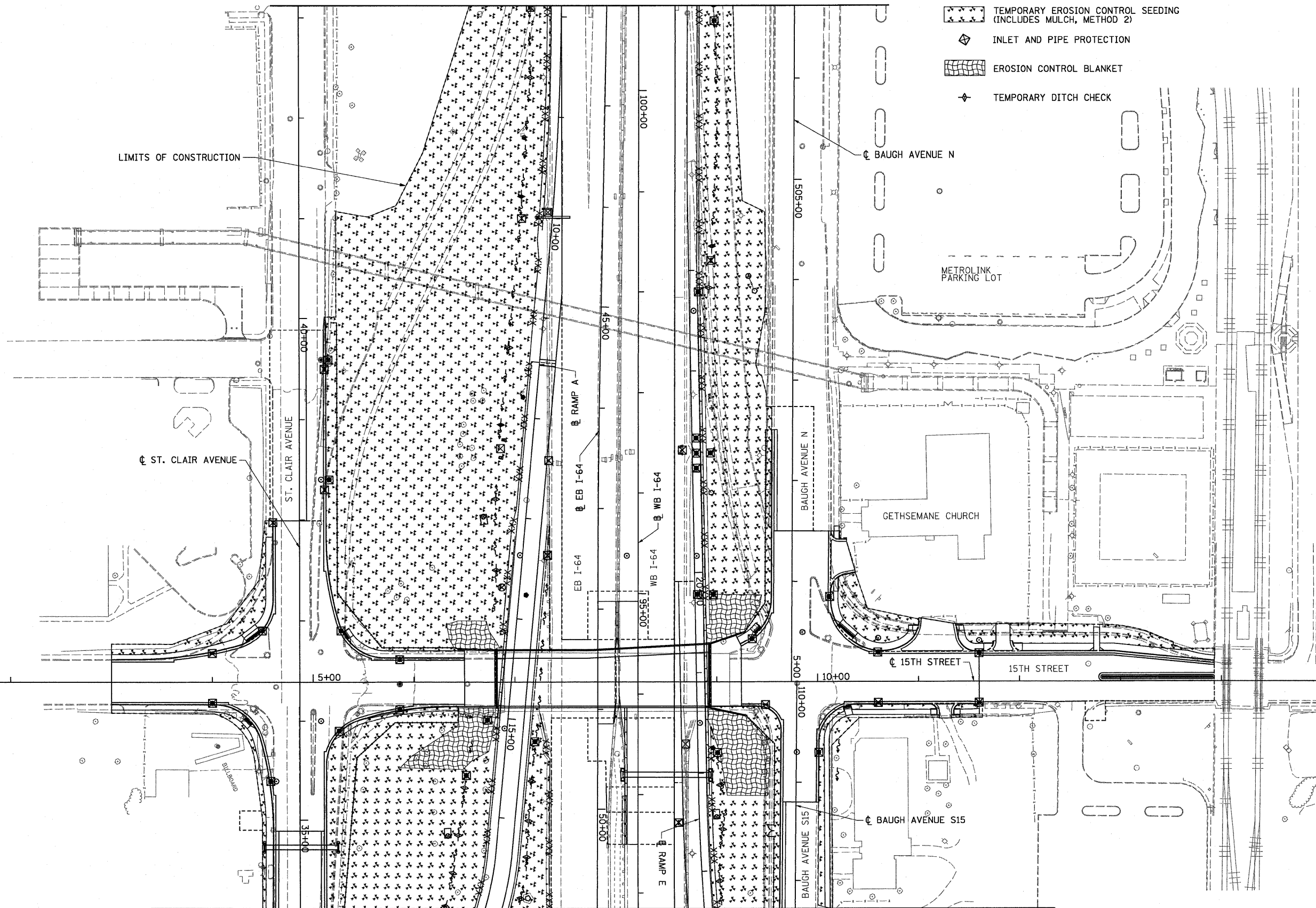




MATCH LINE STA 42+00 (B EB I-64)

**LEGEND**

- XXX - EROSION CONTROL TEMPORARY FENCE
- [Cross-hatch pattern] TEMPORARY EROSION CONTROL SEEDING (INCLUDES MULCH, METHOD 2)
- [Diamond symbol] INLET AND PIPE PROTECTION
- [Grid pattern] EROSION CONTROL BLANKET
- [Circle with cross symbol] TEMPORARY DITCH CHECK

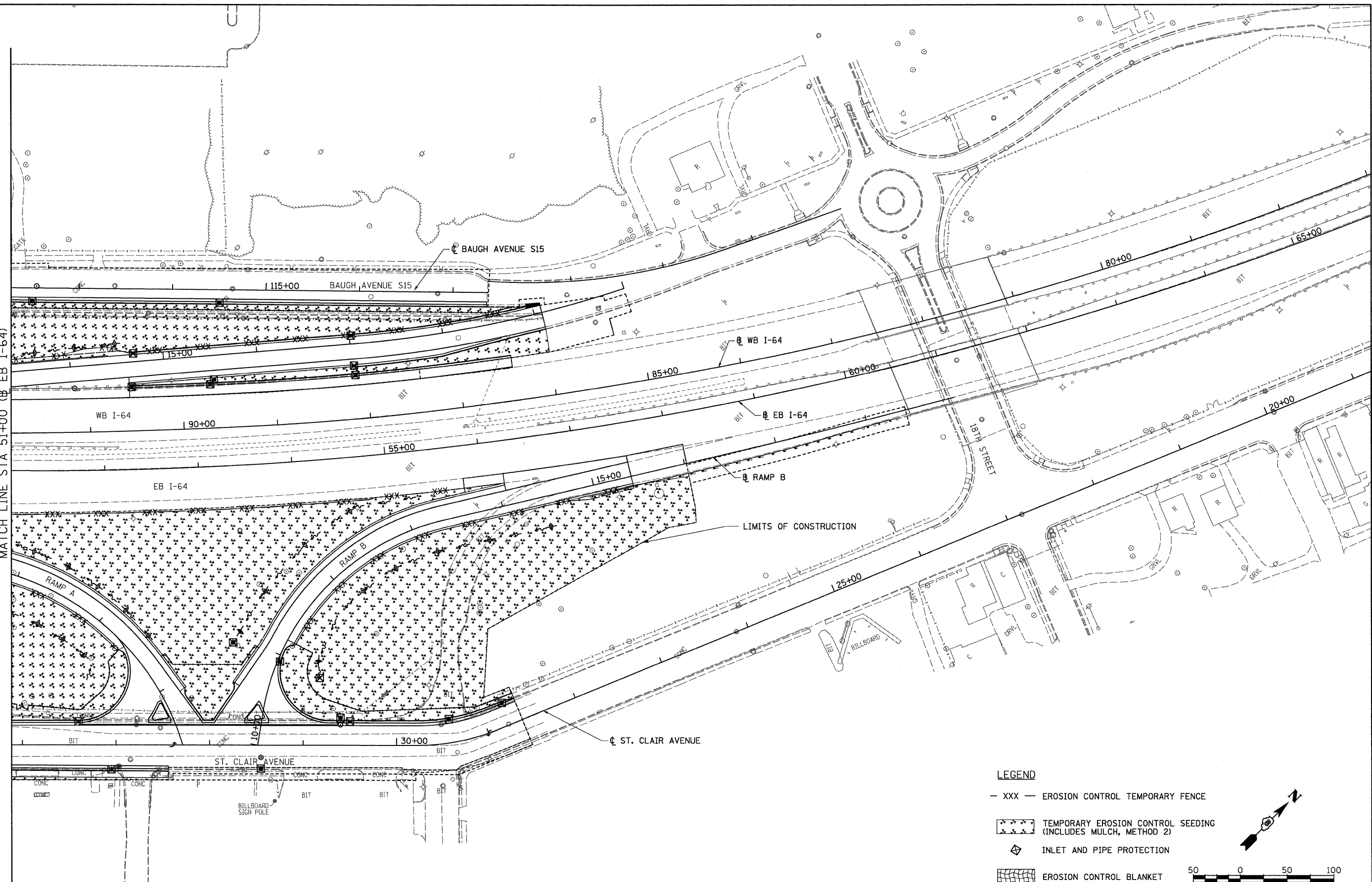


MATCH LINE STA 51+00 (B EB I-64)

FILE NAME = #FILE#	USER NAME = IDOT	DESIGNED - TTB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EROSION CONTROL PLAN AND QUANTITIES</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.0000' / IN.	DRAWN - TTB	REVISED -					64	82-1-2HB	ST. CLAIR	345	99
PLOT DATE = 3/17/2010	DATE - 3/19/2010	CHECKED - JAH	REVISED -	SCALE: 1"=50'    SHEET NO. 2 OF 4 SHEETS    STA.    TO STA.			FED. ROAD DIST. NO.    [ILLINOIS] FED. AID PROJECT					
		DATE - 3/19/2010	REVISED -				CONTRACT NO. 76C49					

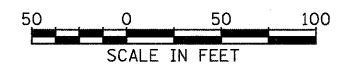


MATCH LINE STA 51+00 (B EB I-64)



**LEGEND**

- XXX - EROSION CONTROL TEMPORARY FENCE
- [XXXXXX] TEMPORARY EROSION CONTROL SEEDING (INCLUDES MULCH, METHOD 2)
- [Symbol] INLET AND PIPE PROTECTION
- [Symbol] EROSION CONTROL BLANKET
- [Symbol] TEMPORARY DITCH CHECK



FILE NAME = #FILEA*	USER NAME = IDOT	DESIGNED - TTB	REVISED -
		DRAWN - TTB	REVISED -
	PLOT SCALE = 50.0000' / IN.	CHECKED - JAH	REVISED -
	PLOT DATE = 3/17/2010	DATE - 3/19/2010	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EROSION CONTROL PLAN AND QUANTITIES**

SCALE: 1"=50' SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-1-2HB	ST. CLAIR	345	100
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76C49	