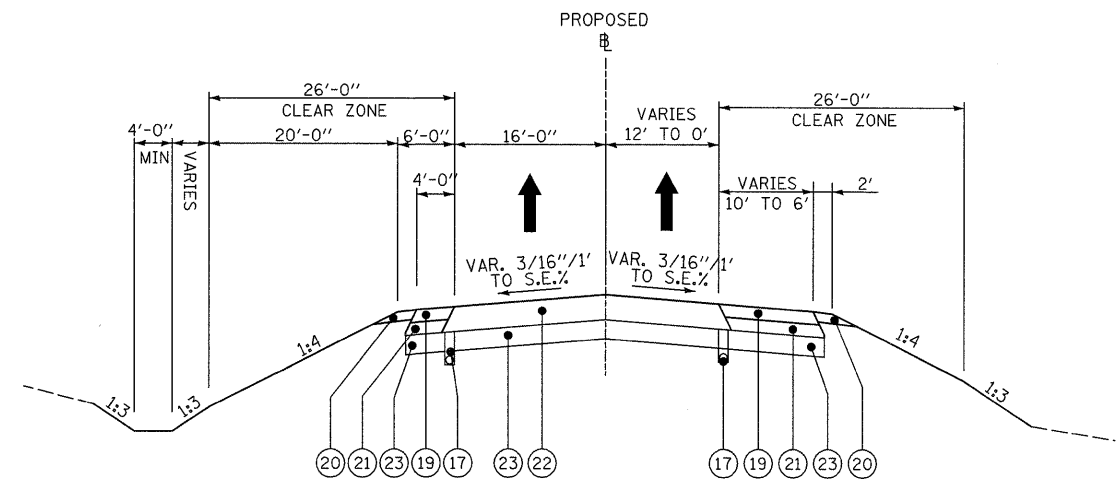
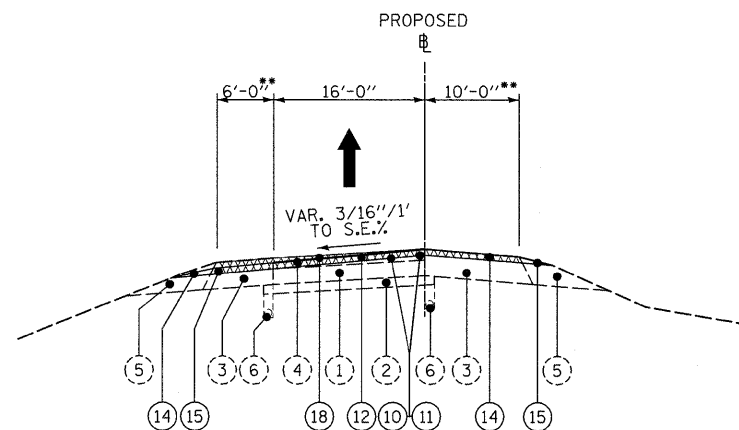


RAMP TYPICAL SECTION  
RAMP P5LT  
STA 41273+55 TO STA 41276+58



RAMP TYPICAL SECTION  
RAMP P8  
STA 104+38.42 TO STA 109+52.10

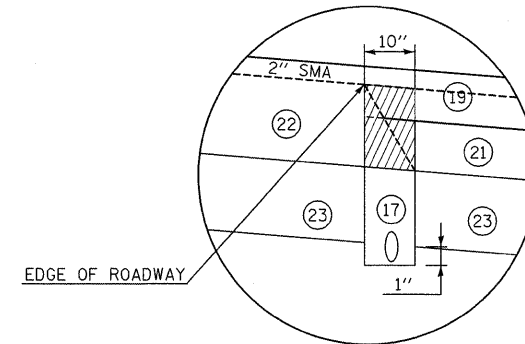


RAMP TYPICAL SECTION  
RAMP P8  
STA 109+52.10 TO STA 111+23.40

\*\* SHOULDER WIDTHS SHOWN ON THE TYPICALS ARE THEORETICAL SHOULDER WIDTHS. ACTUAL SHOULDER WIDTHS SHALL MATCH EXISTING CONDITIONS UNLESS OTHERWISE SPECIFIED.

NOTES:

- SHOULDER SLOPE - LOW SIDE OF S.E. : SLOPE SHALL BE THE SAME AS THE S.E. BUT NOT LESS THAN 4%.
- SHOULDER SLOPE - HIGH SIDE OF S.E. : WHEN THE S.E. RATE OF THE PAVEMENT IS BETWEEN 0% AND 4%, THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE S.E. RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER WILL NOT BE GREATER THAN 8%.
- MAXIMUM 8% BREAKOVER



NOTES:

WHEN BACKFILLING THE PIPE UNDERDRAIN TRENCH, FILL MATERIAL SHALL BE COMPACTED TO THE SATISFACTION OF THE ENGINEER

SEQUENCE OF CONSTRUCTION FOR PIPE UNDERDRAINS

- CONSTRUCT THE PROPOSED FULL DEPTH ASPHALT PAVEMENT TO THE BOTTOM OF THE TOP LIFT
- CONSTRUCT THE PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE C
- CONSTRUCT THE PROPOSED HOT-MIX ASPHALT SHOULDERS TO THE BOTTOM OF THE TOP LIFT
- REMOVE A PORTION OF THE NEWLY CONSTRUCTED SHOULDER AND PLACE THE UNDERDRAINS AND SAND BACKFILL
- PLACE SUB-BASE GRANULAR MATERIAL, TYPE C
- PLACE HOT-MIX ASPHALT SHOULDERS TO THE BOTTOM OF THE TOP LIFT
- PLACE THE FINAL LIFT OF HMA SHOULDER

LEGEND

- |  |   |  |
|--|---|--|
| ① EXISTING P.C.C. CONCRETE                   | ⑩ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)  | ⑱ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH |
| ② EXISTING SUB-BASE GRANULAR MATERIAL TYPE A | ⑪ PROPOSED AGGREGATE (PRIME COAT)   | ⑲ PROPOSED HOT-MIX ASPHALT SHOULDERS 10 1/2"               |
| ③ EXISTING HMA SHOULDERS                     | ⑫ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 2" | ⑳ PROPOSED AGGREGATE SHOULDERS, TYPE B, 6"                 |
| ④ EXISTING HMA SURFACE                       | ⑬ PROPOSED PAVEMENT REMOVAL   | ㉑ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE C              |
| ⑤ EXISTING AGGREGATE SHOULDERS               | ⑭ PROPOSED AGGREGATE SHOULDERS, TYPE B  | ㉒ PROPOSED HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 13"      |
| ⑥ EXISTING PIPE UNDERDRAIN                   | ⑮ PROPOSED HOT-MIX ASPHALT SHOULDERS, 2"  | ㉓ PROPOSED LIME MODIFIED SOIL - 12"                        |
| ⑦ EXISTING GUARDRAIL                         | ⑯ PROPOSED HOT-MIX SURFACE REMOVAL, 2"  | ㉔ PROPOSED GUARDRAIL (SEE PLANS FOR LOCATIONS)             |
|  | ⑰ PROPOSED PIPE UNDERDRAINS, 4"   | ㉕ PROPOSED PAVED SHOULDER REMOVAL                          |
|  |   | ㉖ STRIP REFLECTIVE CRACK CONTROL TREATMENT                 |

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11\0906600\0906601\oad\plans\004.087	EDB7-Sht-Typicel.dgn	DRAWN -	REVISED -			270	60-2RS-3	MADISON	231	13		
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