

LAST SAVED = 3/13/2010
 PEN TABLE = V8.tbl
 PLOT DRIVER = TR-Xerox6284-To-File.plt

| | | | |
|---|----------------------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonehue | DESIGNED - | REVISED - |
| 1:\0906600\0906601\cad\plans\073.087\087-Sht-Grading-NWQuad.dgn | | DRAWN / | REVISED - |
| | PLOT SCALE = 50.0023' / IN. | CHECKED - | REVISED - |
| | PLOT DATE = 3/16/2010 4:43:16 PM | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INTERCHANGE GRADING DETAILS
 NORTHWEST QUADRANT

SCALE: 1" = 50' SHEET NO. 1 OF 2 SHEETS STA. 1352+00 TO STA. 1364+00

| | | | | |
|---------------------------|----------|---------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 101 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



LAST SAVED = 3/13/2010
 PEN TABLE = VB.tbl
 PLOT DRIVER = TR: Xerox6284-Te-File.plt

| | |
|-------------------------------------|----------------------------|
| FILE NAME = | USER NAME = sdonehue |
| \\0906600\0906601\cad\plans\074.D87 | DB7-Sht-Grading-SVQued.dgn |
| PLOT SCALE = 50.0023' / IN. | CHECKED - |
| PLOT DATE = 3/16/2010 4:43:19 PM | DATE - |

| | |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**HORNER &
 SHIFRIN, INC.**
 ENGINEERS

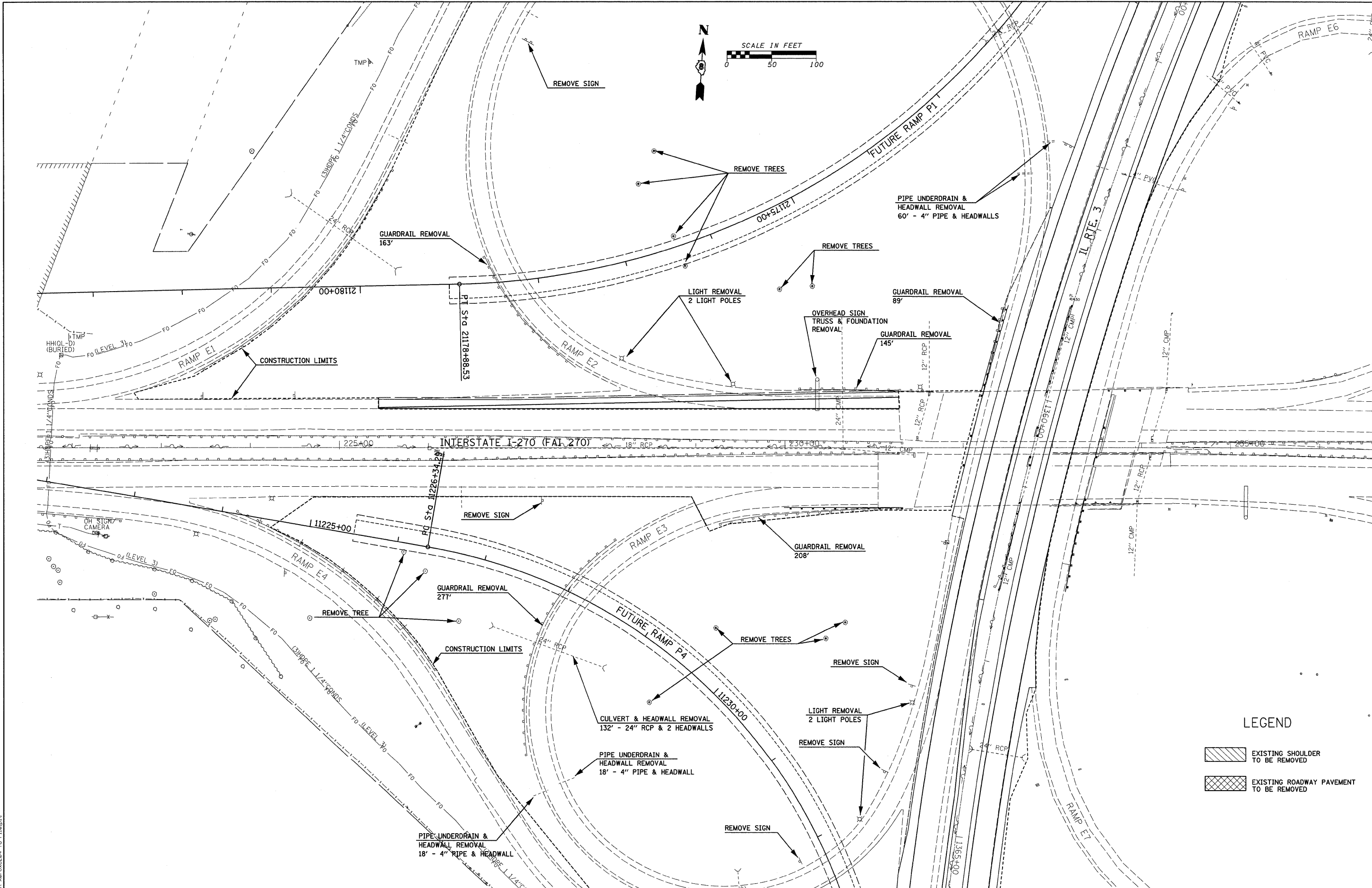
INTERCHANGE GRADING DETAILS
 SOUTHWEST QUADRANT

SCALE: 1" = 50' SHEET NO. 2 OF 2 SHEETS STA. 1352+00 TO STA. 1364+00



| | | | | |
|---------------------------|---------------------|-------------------|---------------------|------------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 102 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



SCALE IN FEET
0 50 100



LEGEND

-  EXISTING SHOULDER TO BE REMOVED
-  EXISTING ROADWAY PAVEMENT TO BE REMOVED

LAST SAVED = 3/13/2010
PEN TABLE = vb.tbl
PLOT DRIVER = TR: Xerox6284-1c-File.plt

FILE NAME =
USER NAME = sdonehue
D87-Sht-Ram-01.dgn

DESIGNED -
DRAWN -
CHECKED -
DATE = 3/16/2010 4:43:21 PM

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

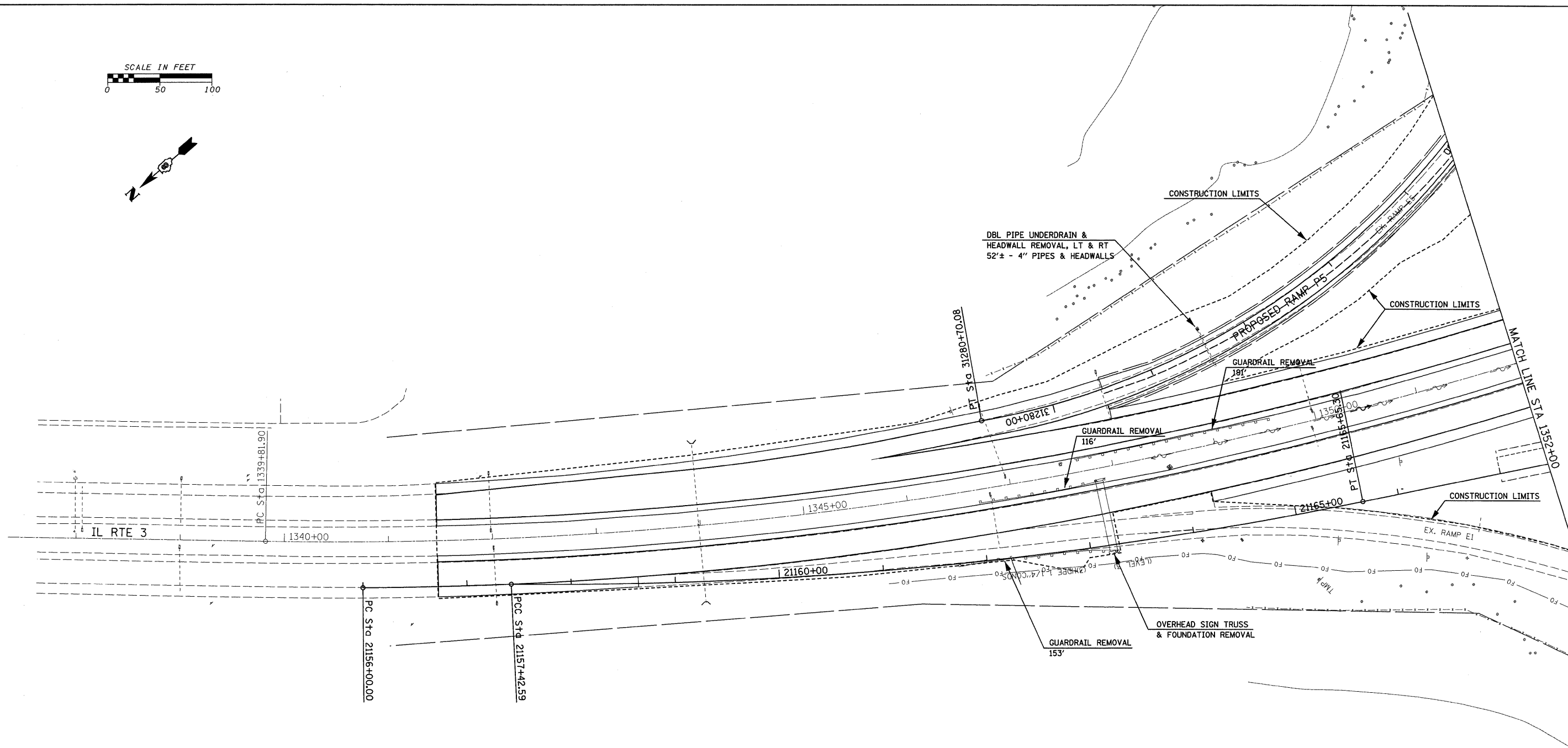
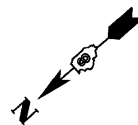
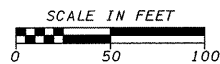


SCALE: 1" = 50'

SHEET NO. 1 OF 10 SHEETS
STA. TO STA.

REMOVAL DETAILS
INTERSTATE 270

| | | | | |
|---------------------------|---------------------|-------------------|---------------------|------------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 103 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



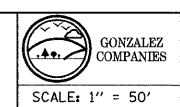
LEGEND

- EXISTING SHOULDER TO BE REMOVED
- EXISTING ROADWAY PAVEMENT TO BE REMOVED

LAST SAVED = 3/13/2010
 PEN TABLE = V8.tbl
 PLOT DRIVER = TR-Xerox6284-TeFile.plt

| | | | |
|--|----------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonehue | DESIGNED - | REVISED - |
| \\0906600\0906601\ced\plans\076_087\087-Shr-Rem-02.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 50.0023' / IN. | | CHECKED - | REVISED - |
| PLOT DATE = 3/16/2010 4:43:23 PM | | DATE - | REVISED - |

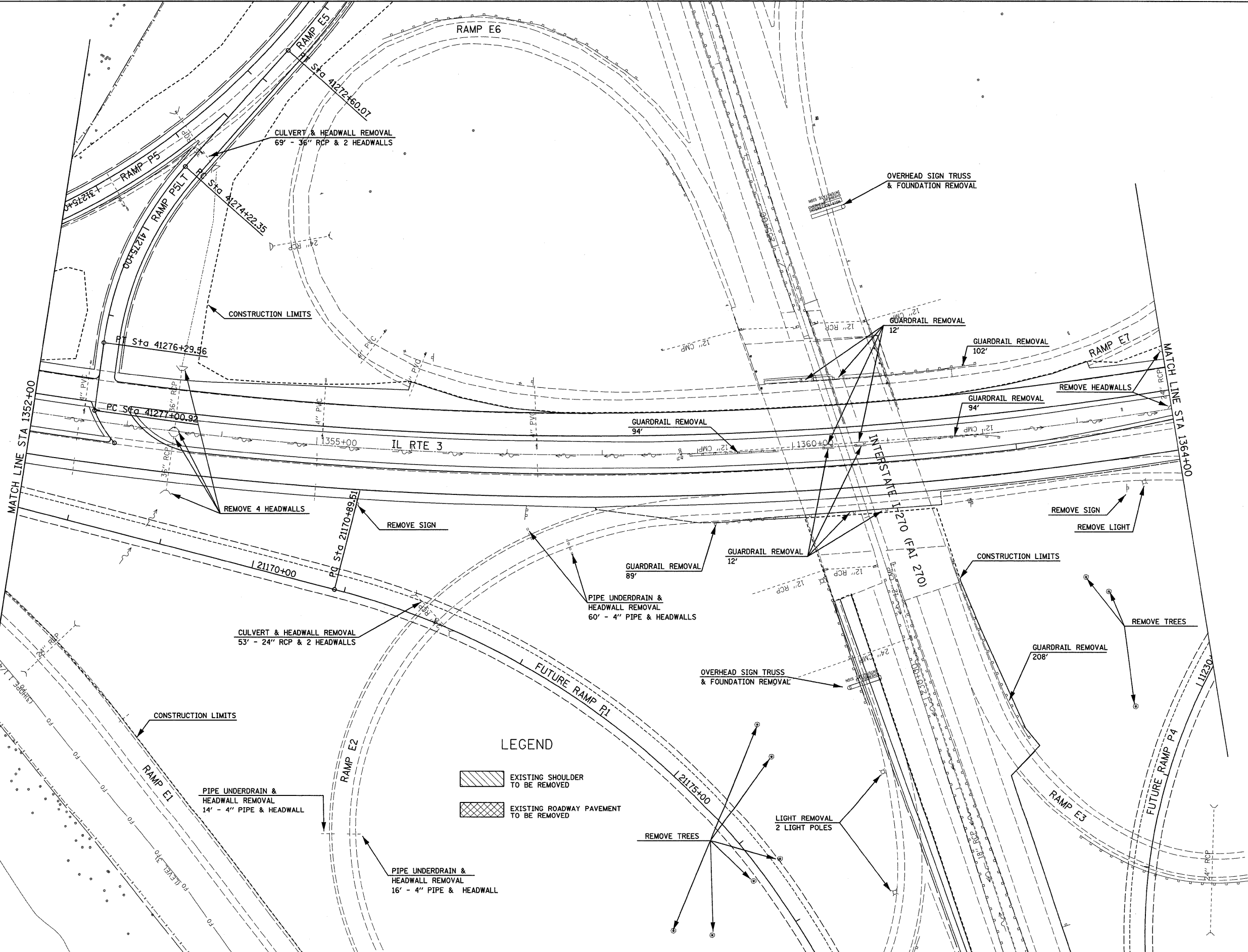
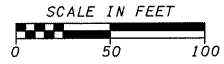
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



**REMOVAL DETAILS
ILLINOIS ROUTE 3**

SCALE: 1" = 50' SHEET NO. 2 OF 10 SHEETS STA. 1341+46.86 TO STA. 1352+00

| | | | | |
|---------------------------|----------|---------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 104 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



LEGEND

- EXISTING SHOULDER TO BE REMOVED
- EXISTING ROADWAY PAVEMENT TO BE REMOVED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



SCALE: 1" = 50'

**REMOVAL DETAILS
ILLINOIS ROUTE 3**

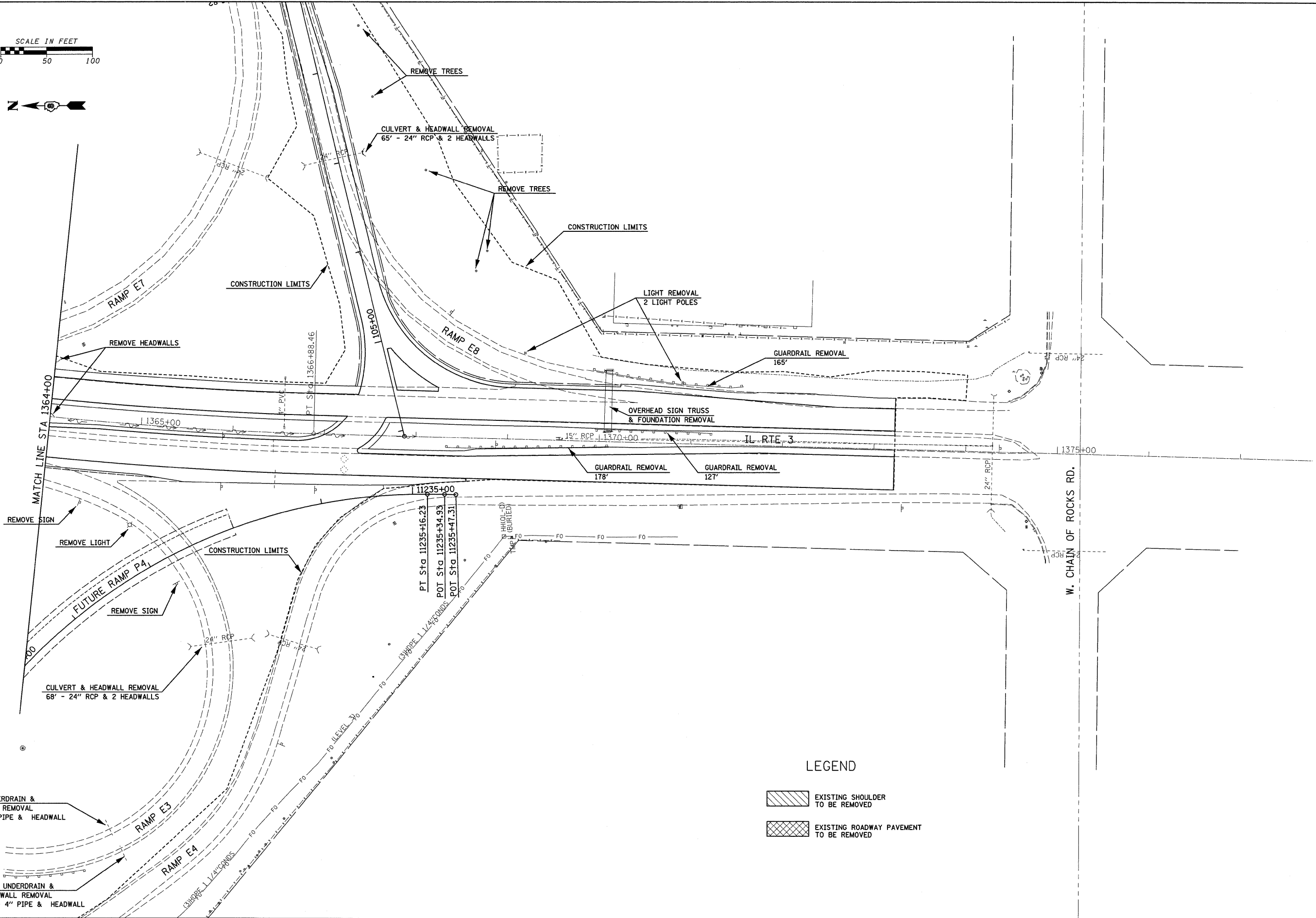
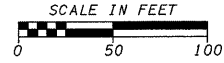
SHEET NO. 3 OF 10 SHEETS STA. 1352+00 TO STA. 1364+00

| | | | | |
|---------------------------|---------------------|-------------------|---------------------|------------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 105 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

FILE NAME =
USER NAME = sdonahue
DESIGNED -
DRAWN -
PLOT SCALE = 50.0023' / IN.
PLOT DATE = 3/16/2010 4:43:26 PM

| | |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - | REVISED - |

LAST SAVED = 3/13/2010
PEN TABLE = VB.tbl
PLOT DRIVER = TR-Xerox6284-To-File.plt



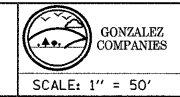
LEGEND

- EXISTING SHOULDER TO BE REMOVED
- EXISTING ROADWAY PAVEMENT TO BE REMOVED

LAST SAVED = 3/13/2010
 PEN TABLE = VB.tbl
 PLOT DRIVER = TR-Xerox6284-TopFile.plt

| | | | |
|--|----------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - |
| 1:\0906600\0906601\oad\vt\plans\078.D87\087-Shr-Rem-04.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 50.0023 ' / IN. | | CHECKED - | REVISED - |
| PLOT DATE = 3/16/2010 4:43:28 PM | | DATE - | REVISED - |

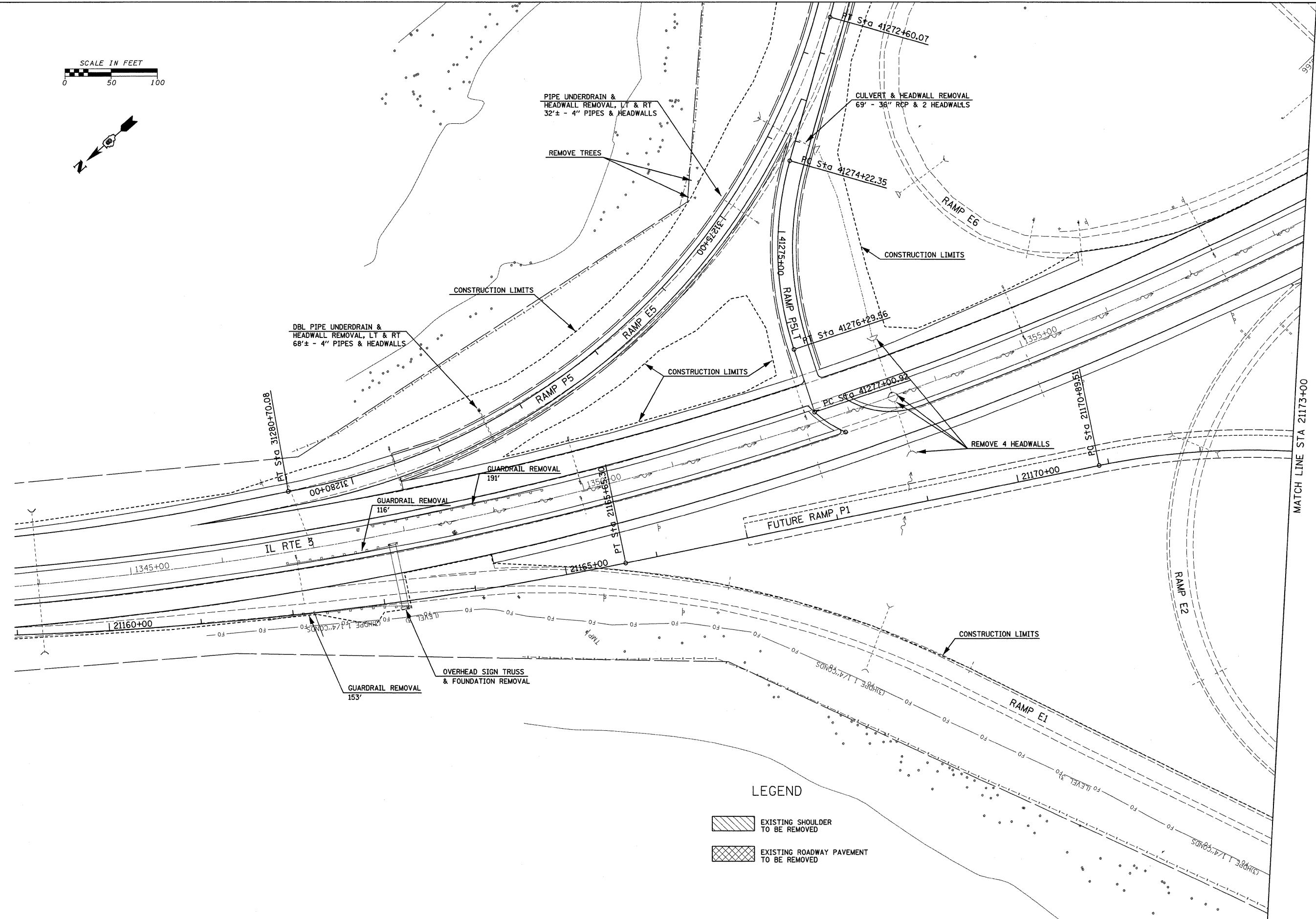
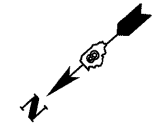
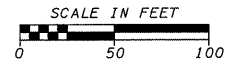
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



REMOVAL DETAILS
ILLINOIS ROUTE 3

SCALE: 1" = 50' SHEET NO. 4 OF 10 SHEETS STA. 1364+00 TO STA.

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 106 |
| CONTRACT NO. T6D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

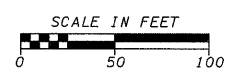
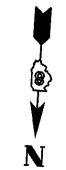
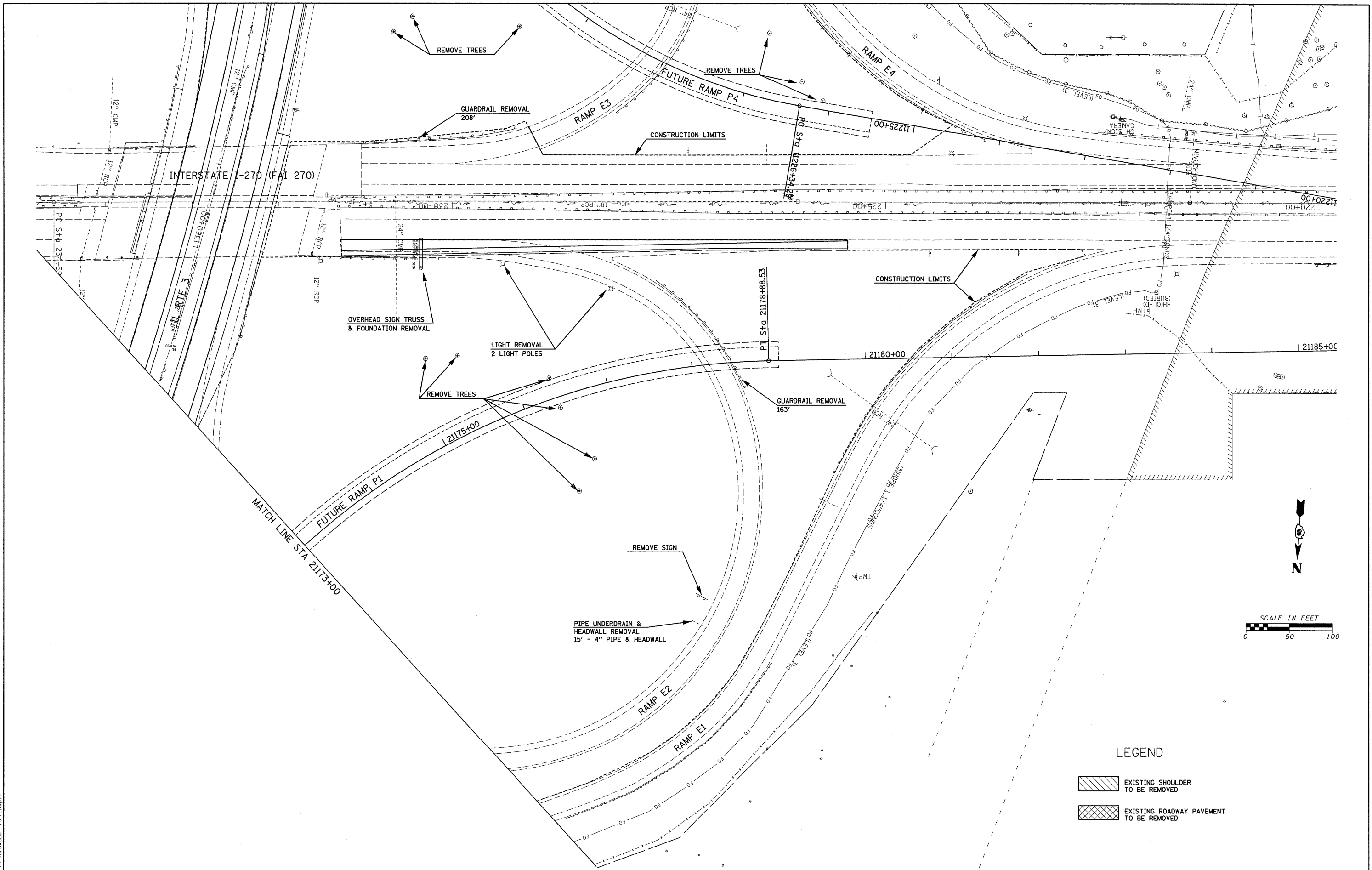


LEGEND

- EXISTING SHOULDER TO BE REMOVED
- EXISTING ROADWAY PAVEMENT TO BE REMOVED

LAST SAVED = 3/13/2010
 PEN TABLE = VB.tbl
 PLOT DRIVER = TR-Xerox6284-Te-File.plt

| | | | | | | | | | | | | |
|---|----------------------|------------|-----------|---|---------------------|--------------------------|-----------------------------------|--------------------|---------|---------------------------|-----------|--|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SCALE: 1" = 50' | REMOVAL DETAILS | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| 1:\0906600\0906601\oad\plans\079.087\087-Shr-Rem-05.dgn | | DRAWN - | REVISED - | | | PROPOSED RAMP P1 | 270 | 60-2RS-3 | MADISON | 231 | 107 | |
| PLOT SCALE = 50.0023' / IN. | | CHECKED - | REVISED - | | | SHEET NO. 5 OF 10 SHEETS | STA. 21159+65.85 TO STA. 21173+00 | CONTRACT NO. 76D87 | | ILLINOIS FED. AID PROJECT | | |
| PLOT DATE = 3/16/2010 4:43:30 PM | | DATE - | REVISED - | | | | | | | | | |



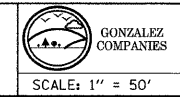
LEGEND

- EXISTING SHOULDER TO BE REMOVED
- EXISTING ROADWAY PAVEMENT TO BE REMOVED

LAST SAVED = 3/13/2010
 PEN TABLE = VB.tbl
 PLOT DRIVER = TR-Xerox6204-Te-File.plt

| | | | |
|--------------------------------------|----------------------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - |
| \\0906600\0906601\ced\plans\080.L087 | 087-Sht-Rem-06.dgn | DRAWN - | REVISED - |
| | PLOT SCALE = 50.0023 ' / IN. | CHECKED - | REVISED - |
| | PLOT DATE = 3/16/2010 4:43:33 PM | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

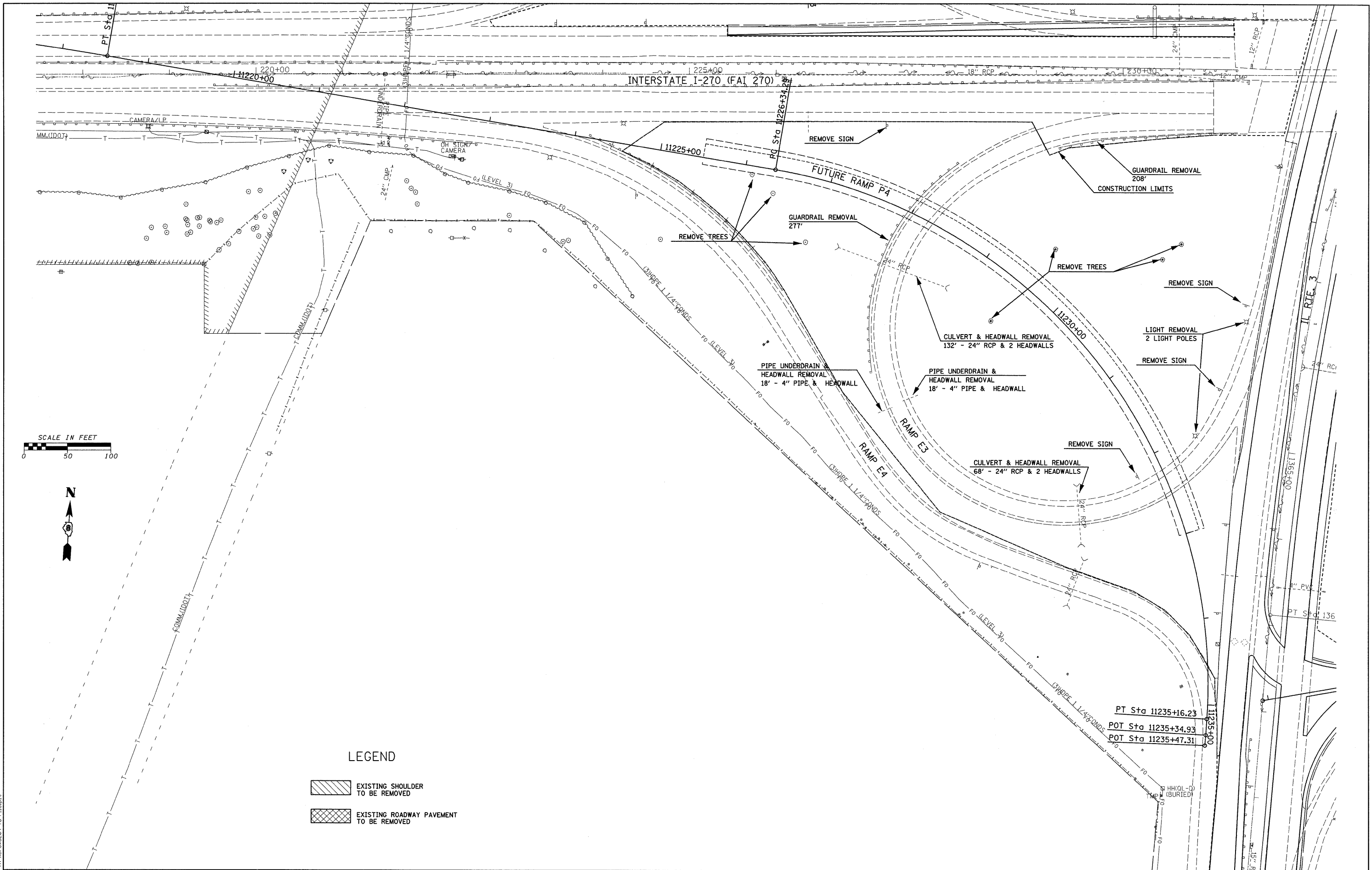


REMOVAL DETAILS
PROPOSED RAMP P1



SCALE: 1" = 50'

SHEET NO. 6 OF 10 SHEETS STA. 21173+00 TO STA. 21184+25.91


| | | | | |
|---------------------------|---------------------|-------------------|---------------------|------------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 108 |
| CONTRACT NO. T6D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

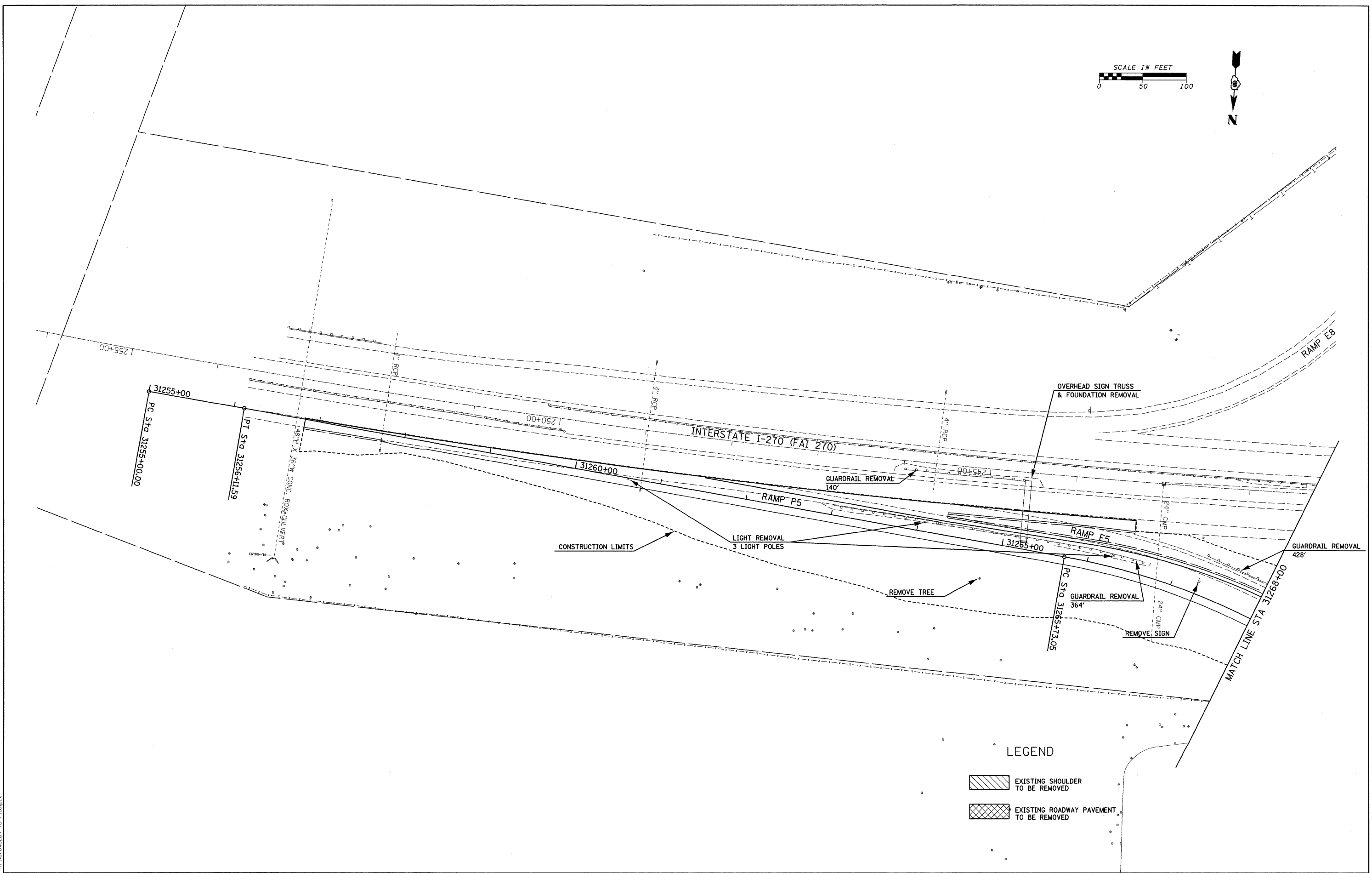
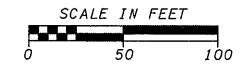


LEGEND

-  EXISTING SHOULDER TO BE REMOVED
-  EXISTING ROADWAY PAVEMENT TO BE REMOVED

LAST SAVED = 3/13/2010
 PEN TABLE = VB.tbl
 PLOT DRIVER = TR-Xerox6284-TeFile.plt

| | | | | | | | |
|---|--|--|--|---|--|---|---|
| FILE NAME = s:\2906600\0906601\oad\plans\281.D876\087-Sht-Rem-07.dgn | USER NAME = sdonehue DRAWN - CHECKED - DATE - | DESIGNED - DRAWN - CHECKED - DATE - | REVISED - REVISED - REVISED - REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION |  GONZALEZ COMPANIES SCALE: 1" = 50' | REMOVAL DETAILS PROPOSED RAMP P4 SHEET NO. 7 OF 10 SHEETS STA. 11221+00 TO STA. 11235+47.31 | F.A.I. RTE. 270 SECTION 60-2RS-3 COUNTY MADISON TOTAL SHEETS 231 SHEET NO. 109 CONTRACT NO. 76087 ILLINOIS FED. AID PROJECT |
|---|--|--|--|---|--|---|---|



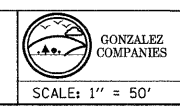
LEGEND

- EXISTING SHOULDER TO BE REMOVED
- EXISTING ROADWAY PAVEMENT TO BE REMOVED

LAST SAVED = 3/13/2010
 PEN TABLE = VB.tbl
 PLOT DRIVER = TR-Xerox6224-Te-File.plt

| | | | |
|--|----------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - |
| :\0906600\0906601\ced\plans\082.087\087-Shr-Rem-08.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 50.0023' / IN. | | CHECKED - | REVISED - |
| PLOT DATE = 3/16/2010 4:43:37 PM | | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

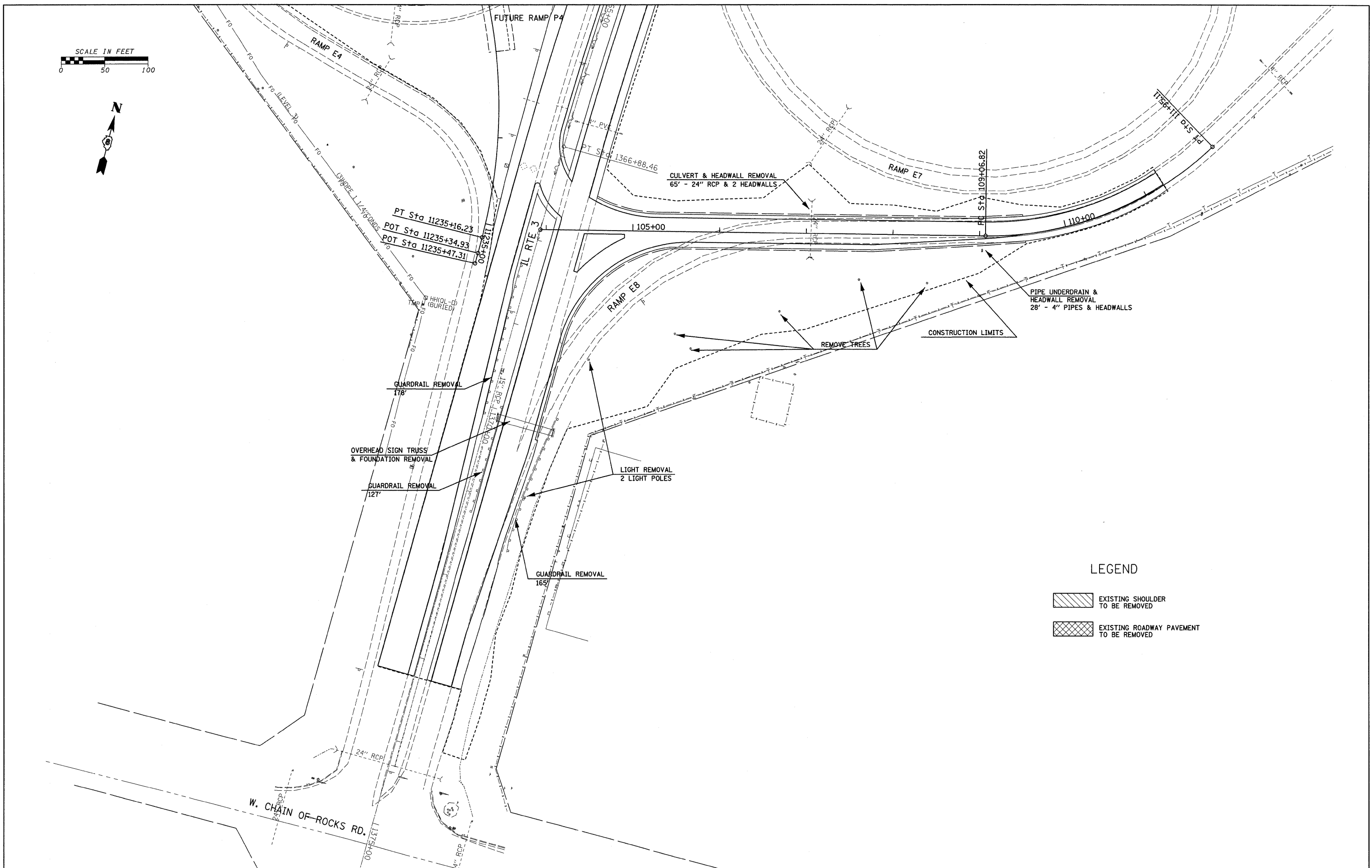
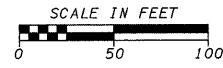


REMOVAL DETAILS
PROPOSED RAMP P5

SCALE: 1" = 50'

SHEET NO. 8 OF 10 SHEETS STA. 31256+83.49 TO STA. 31268+00

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 110 |
| CONTRACT NO. T6D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



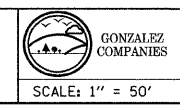
LEGEND

- EXISTING SHOULDER TO BE REMOVED
- EXISTING ROADWAY PAVEMENT TO BE REMOVED

LAST SAVED = 3/13/2010
 PEN TABLE = 00.001
 PLOT DRIVER = TR:keroc6204-To-File.plt

| | | | |
|---|----------------------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - |
| \\09066600\09066601\oad\c\plans\084.D07 | 087-Shr-Rem-10.dgn | DRAWN - | REVISED - |
| | PLOT SCALE = 50.0023' / IN. | CHECKED - | REVISED - |
| | PLOT DATE = 3/16/2010 4:43:42 PM | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



**REMOVAL DETAILS
PROPOSED RAMP P8**

SCALE: 1" = 50' SHEET NO. 10 OF 10 SHEETS STA. 31268+00 TO STA. 31280+70.08

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 112 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

DESIGN: AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals. ("AASHTO Specifications")

CONSTRUCTION: Current (at time of letting) Illinois Department of Transportation Standard Specifications for Road and Bridge Construction, Supplemental Specifications and Special Provisions. ("Standard Specifications")

LOADING: 90 M.P.H. WIND VELOCITY

WALKWAY LOADING: Dead load plus 500 lbs. concentrated live load.

DESIGN STRESSES:
Field Units
 $f'_c = 3,500$ p.s.i.
 $f_y = 60,000$ p.s.i. (reinforcement)

WELDING: All welds to be continuous unless otherwise shown. All welding to be done in accordance with current AWS D1.1 and D1.2 Structural Welding Codes (Steel and Aluminum) and the Standard Specifications.

MATERIALS: Aluminum Alloys as shown throughout plans. All Structural Steel Pipe shall be ASTM A53 Grade B or A500 Grade B or C. If A500 pipe is substituted for A53, then the outside diameter shall be as detailed and wall thickness greater than or equal to A53.

All Structural Steel Plates and Shapes shall conform to AASHTO M270 Gr. 36, Gr. 50 or Gr. 50W*. Stainless steel for shims, sleeves and handhole covers shall be ASTM A240, Type 302 or 304, or another alloy suitable for exterior exposure and acceptable to the Engineer.

The steel pipe and stiffening ribs for the column shall have a minimum longitudinal Charpy V-Notch (CVN) energy of 15 lb.-ft. at 40° F. (Zone 2) before galvanizing.

FASTENERS FOR ALUMINUM TRUSSES: All bolts noted as "high strength" must satisfy the requirements of AASHTO M164 (ASTM A325), or approved alternate, and must have matching lock nuts. Threaded studs for splices (if Members interfere) must satisfy the requirements of ASTM A449, ASTM A193, Grade B7, or approved alternate, and must have matching lock nuts. Bolts and lock nuts not required to be high strength must satisfy the requirements of ASTM A307. All bolts and lock nuts must be hot dip galvanized per AASHTO M232. The lock nuts must have nylon or steel inserts. A stainless steel flat washer conforming to ASTM A240 Type 302 or 304, is required under both head and nut or under both nuts where threaded studs are used. High strength bolt installation shall conform to Article 505.04 (f) (2)d of the IDOT Standard Specifications for Road and Bridge Construction. Rotational capacity ("ROCAP") testing of bolts will not be required.

U-BOLTS AND EYEBOLTS: U-Bolts and Eyebolts must be produced from ASTM A276 Type 304, 304L, 316 or 316L, Condition A, cold finished stainless steel, or an equivalent material acceptable to the Engineer. All nuts for U-Bolts and Eyebolts must be lock nuts equivalent to ASTM A307 with nylon or steel inserts and hot dip galvanized per AASHTO M232. A stainless steel flat washer conforming to ASTM A240, Type 302 or 304, is required under each U-Bolt and Eyebolt lock nut.

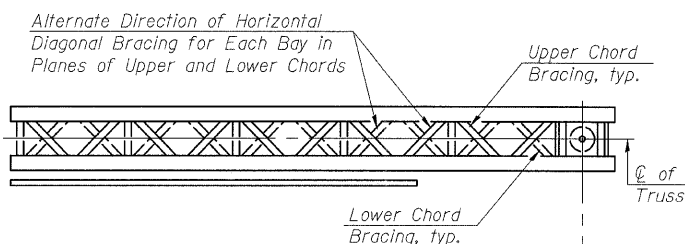
GALVANIZING: All Steel Grating, Plates, Shapes and Pipe shall be Hot Dip Galvanized after fabrication in accordance with AASHTO M111. Painting is not permitted.

ANCHOR RODS: Shall conform to AASHTO M314 Gr. 105 with a minimum Charpy V-Notch (CVN) energy of 15 lb.-ft. at 10° F.

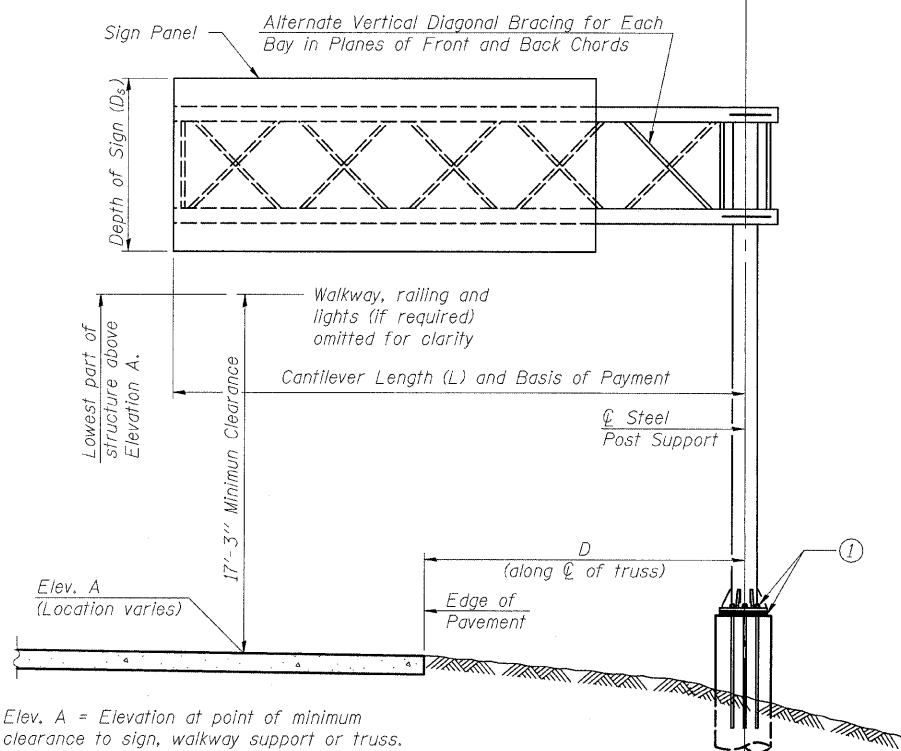
CONCRETE SURFACES: All concrete surfaces above an elevation 6" below the lowest final ground line at each foundation shall be cleaned and coated with Bridge Seat Sealer in accordance with the Standard Specifications.

REINFORCEMENT BARS: Reinforcement Bars designated (E) shall be epoxy coated in accordance with the Standard Specifications.

FOUNDATIONS: The contract unit price for Drilled Shaft Concrete Foundations shall include reinforcement bars complete in place.



TYPICAL PLAN
(Walkway not shown)

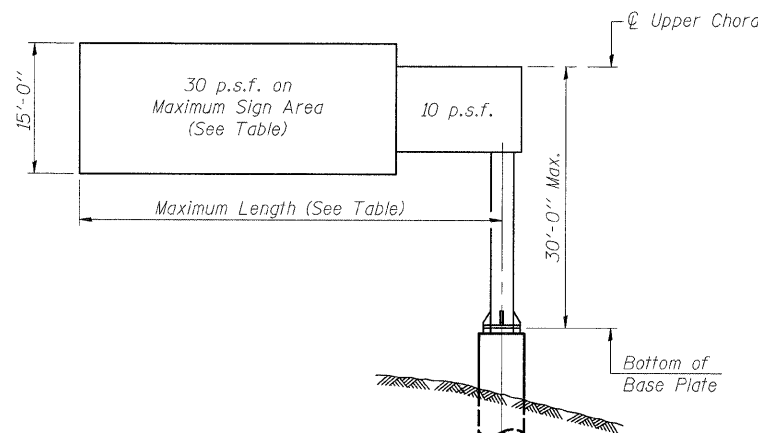


TYPICAL ELEVATION
Looking in Direction of Traffic

Sign support structures may be subject to damaging vibrations and oscillations when sign panels are not in place during erection or maintenance of the structure. To avoid these vibrations and oscillations, consideration should be given to attaching temporary blank sign panels to the structure.

| Structure Number | Station | Design Truss Type | Cantilever Length (L) | Elev. A | Dim. D | D _s | Total Sign Area |
|------------------|-----------|-------------------|-----------------------|---------|--------|----------------|-----------------|
| 8C0601270R002.7 | 235+35.00 | III-C-A | 35'-0" | 436.25 | 16'-0" | 9'-6" | 152 sf |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |

| Truss Type | Maximum Sign Area | Maximum Length |
|------------|-------------------|----------------|
| I-C-A | 170 Sq. Ft. | 25 Ft. |
| II-C-A | 340 Sq. Ft. | 30 Ft. |
| III-C-A | 400 Sq. Ft. | 40 Ft. |



DESIGN WIND LOADING DIAGRAM

Parameters shown are basis for I.D.O.T. Standards. Installations not within dimensional limits shown require special analysis for all components.

① After adjustments to level truss and insure adequate vertical clearance, all top and leveling nuts shall be tightened against the base plate with a minimum torque of 200 lb.-ft. Stainless steel mesh shall then be placed around the perimeter of the base plate. Secure to base plate with stainless steel banding.

Note:
Trusses shall be shipped individually with adequate provision to prevent detrimental motion during transport. This may require ropes between horizontals and diagonals or energy dissipating (elastic) ties to the vehicle. The contractor is responsible for maintaining the configuration and protection of the trusses.

* If M270 Gr. 50W (M222) steel is proposed, chemistry for plate to be used shall first be approved by the Engineer as suitable for galvanizing and welding.

TOTAL BILL OF MATERIAL

| ITEM | UNIT | TOTAL |
|---|----------|-------|
| OVERHEAD SIGN STRUCTURE CANTILEVER TYPE I-C-A | Foot | 0 |
| OVERHEAD SIGN STRUCTURE CANTILEVER TYPE II-C-A | Foot | 0 |
| OVERHEAD SIGN STRUCTURE CANTILEVER TYPE III-C-A | Foot | 35 |
| OVERHEAD SIGN STRUCTURE WALKWAY, TYPE A | Foot | 23 |
| DRILLED SHAFT CONCRETE FOUNDATIONS | Cu. Yds. | 10.7 |

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

OSC-A-1

12-1-08

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |
| | | |

CANTILEVER SIGN STRUCTURES
GENERAL PLAN & ELEVATION
ALUMINUM TRUSS & STEEL POST

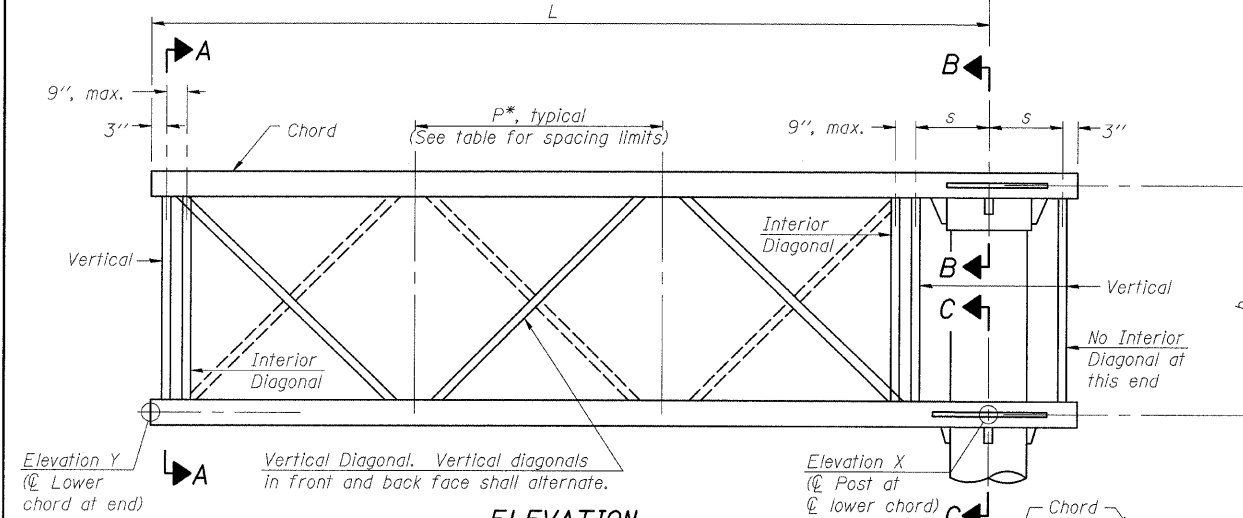
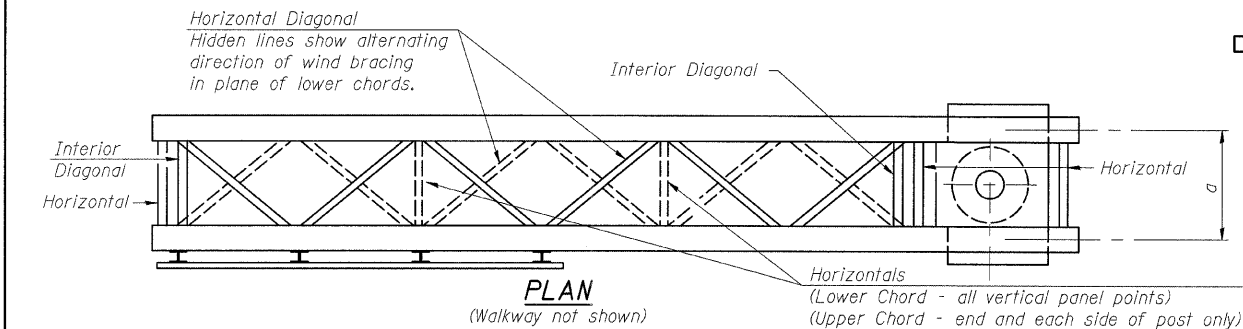
F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3
MADISON COUNTY

HORNER & SHIFRIN, INC.
ENGINEERS

SHEET NO. 1
9 SHEETS

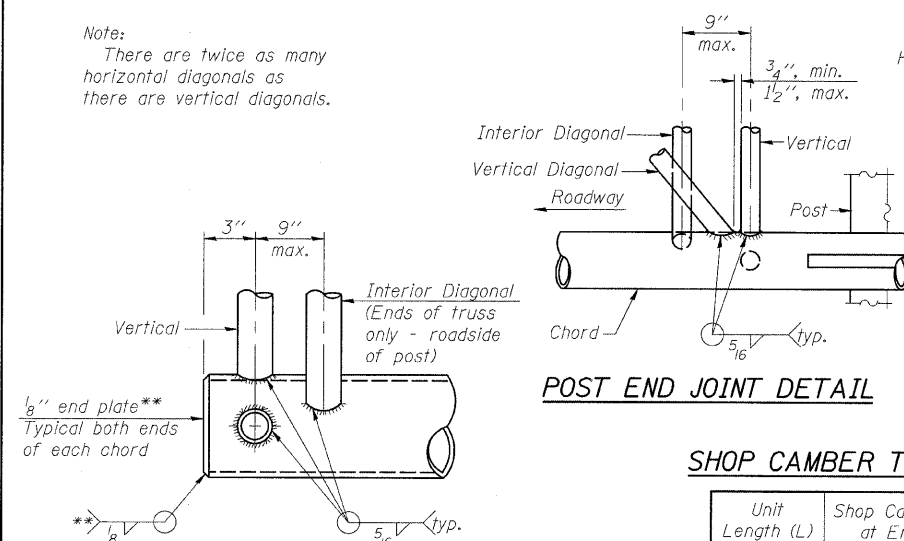
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 113 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



TYPICAL TRUSS UNIT
For Section B-B and Section C-C, see Base Sheet OSC-A-3.

Note:
There are twice as many horizontal diagonals as there are vertical diagonals.



CANTILEVER END JOINT DETAIL

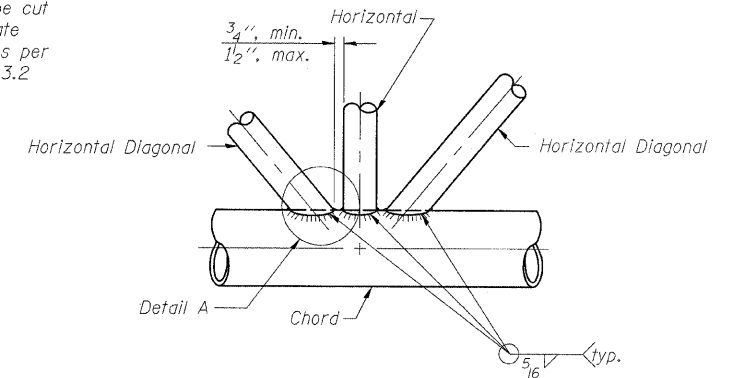
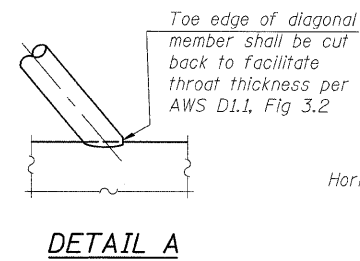
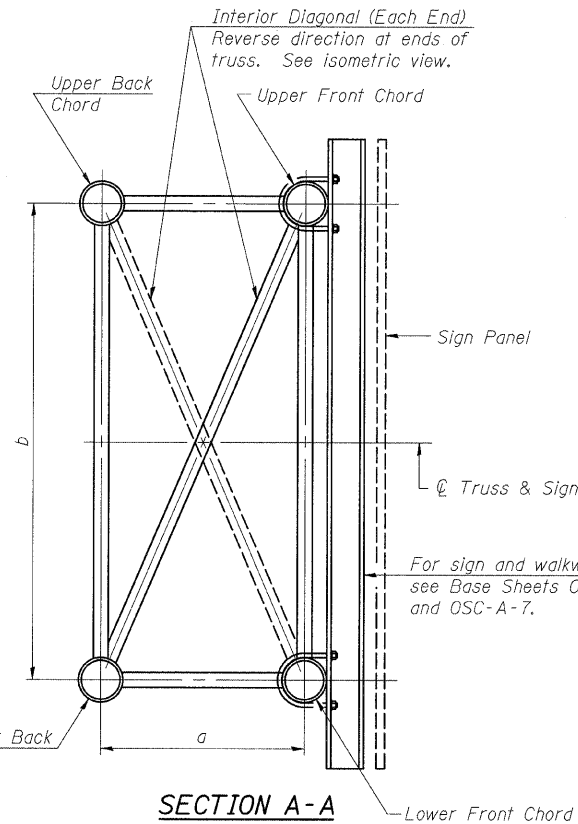
** Contractor may alternatively use standard aluminum drive-fit cap to close ends.

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

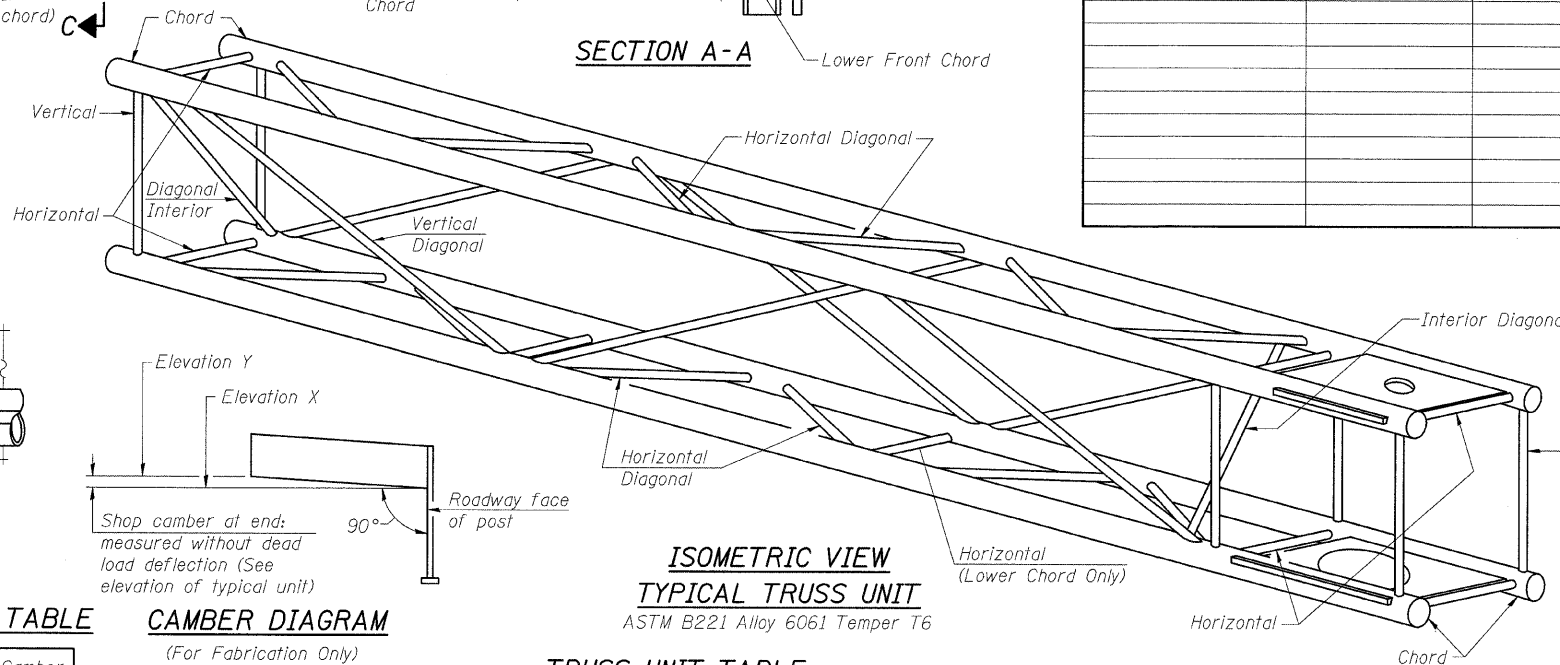
OSC-A-2

12-1-08

HORNER & SHIFRIN, INC.
ENGINEERS



| Structure Number | Station | Truss Type | Design Length (L) | Number of Panels Per Unit | Panel Length (P)* |
|------------------|-----------|------------|-------------------|---------------------------|-------------------|
| 8C0601270R002.7 | 235+35.00 | III-C-A | 35'-0" | 6 | 66" |
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |



SHOP CAMBER TABLE

| Unit Length (L) | Shop Camber at End |
|-----------------|--------------------|
| 15' | 1 1/2" |
| 16'-17' | 1 3/4" |
| 18'-20' | 2" |
| 21'-22' | 2 1/4" |
| 23'-25' | 2 1/2" |
| 26'-27' | 2 3/4" |
| 28'-30' | 3" |
| 31'-32' | 3 1/4" |
| 33'-35' | 3 1/2" |
| 36'-37' | 4" |
| 38'-40' | 4 1/2" |

CAMBER DIAGRAM
(For Fabrication Only)

TRUSS UNIT TABLE

| Truss Type | Dimension "a" | Dimension "b" | Dimension "s" | Limits for Panel Spacing (P)* | Up. & Low. Chord | | Verticals; Horizontals; Vertical, Horizontal, and Interior Diagonals | |
|-----------------------|---------------|---------------|---------------|-------------------------------|------------------|-------|--|-------|
| | | | | | O.D. | Wall | O.D. | Wall |
| I-C-A | 24" | 54" | 16" | 36" min. to 48" max. | 5" | 5/16" | 2 1/2" | 5/16" |
| II-C-A | 36" | 66" | 21" | 42" min. to 54" max. | 6 1/2" | 5/16" | 3 1/4" | 5/16" |
| III-C-A (35' Max.) | 36" | 84" | 21" | 48" min. to 66" max. | 7" | 3/8" | 3 1/2" | 3/8" |
| III-C-A (>35' to 40') | 36" | 84" | 21" | 48" min. to 66" max. | 8" | 3/8" | 3 1/2" | 3/8" |

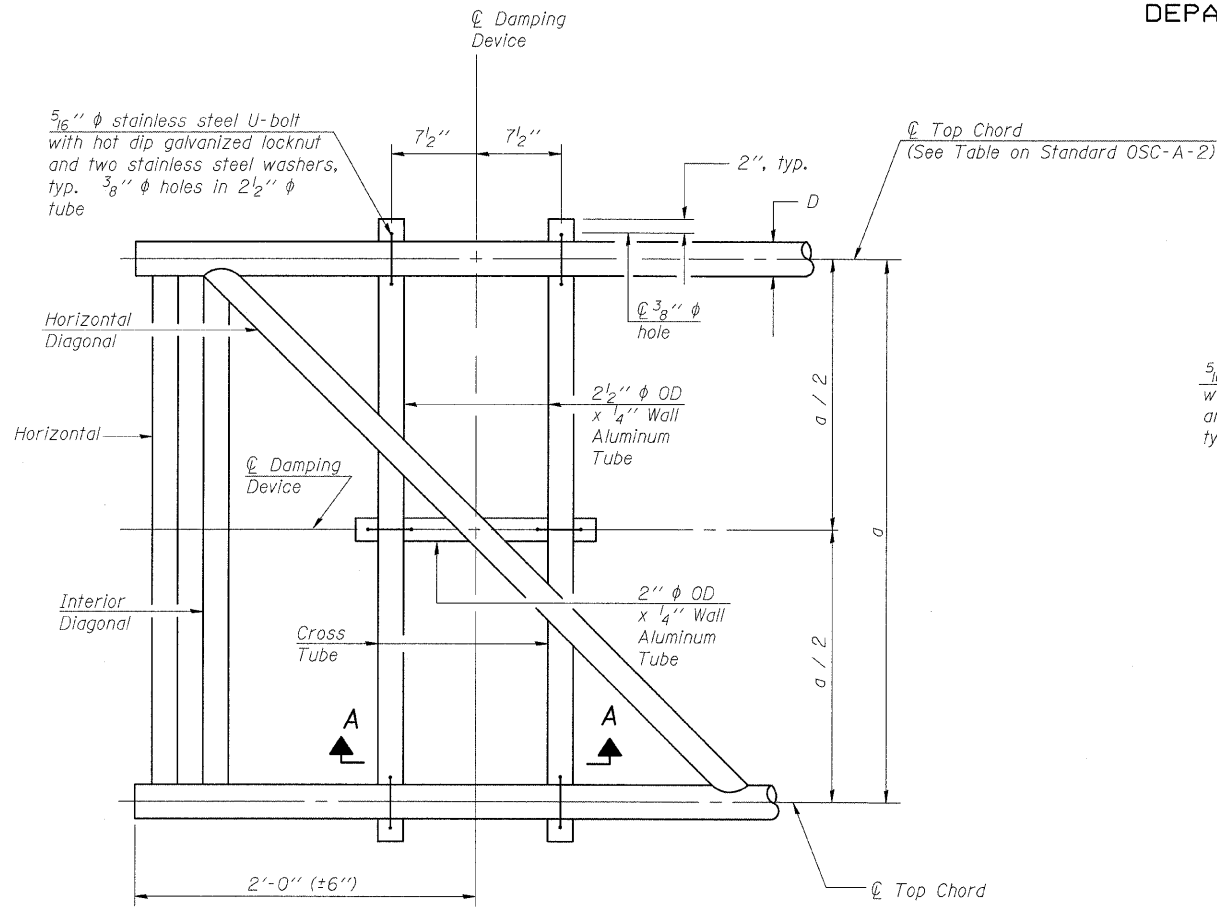
*P = $\frac{L-s-3"}{\# \text{ Panels}}$

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |

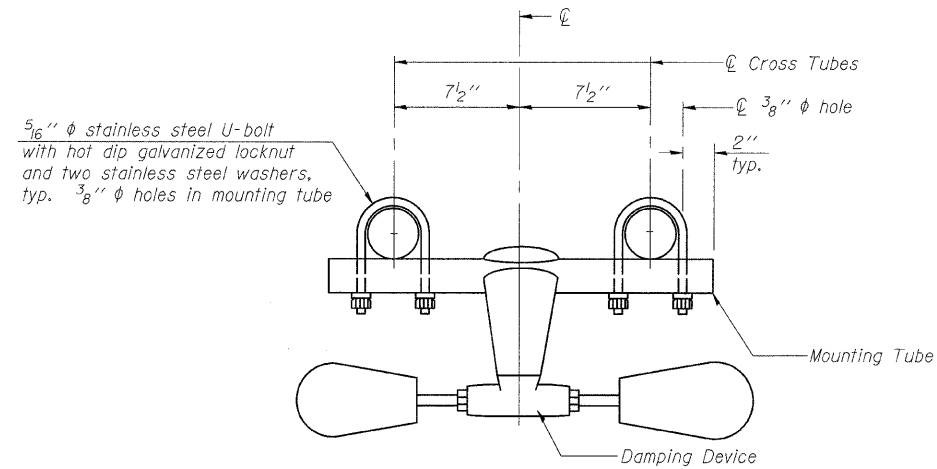
CANTILEVER SIGN STRUCTURES TRUSS DETAILS ALUMINUM TRUSS & STEEL POST
F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3 MADISON COUNTY

| | | | | | |
|-------------|--------------------|------------------|----------------|---------------------------|---------------|
| SHEET NO. 2 | F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 114 |
| | CONTRACT NO. 76D87 | | | ILLINOIS FED. AID PROJECT | |

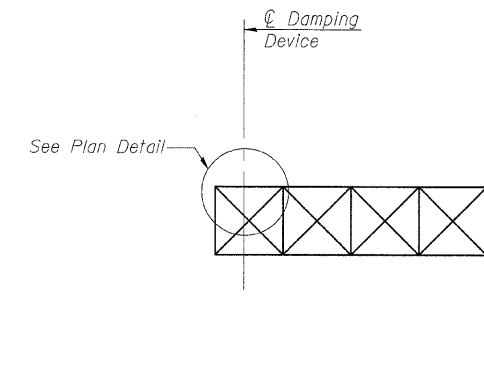
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



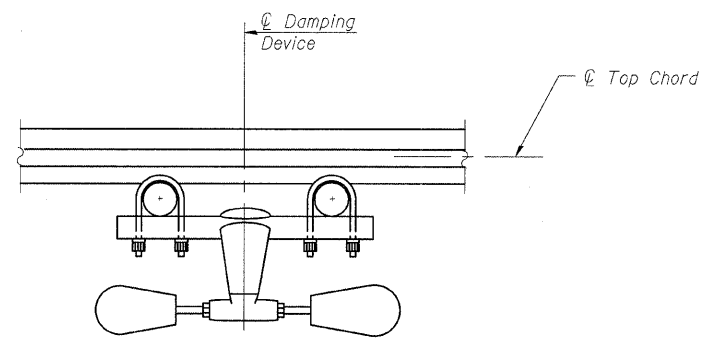
PLAN DETAIL



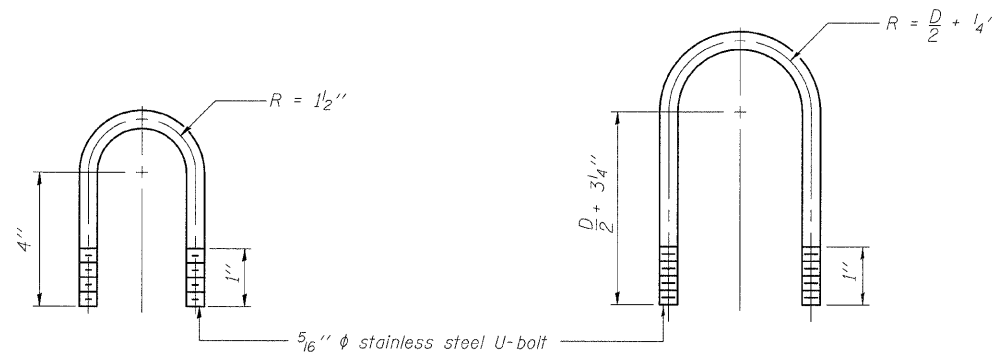
TRUSS DAMPING
DEVICE CONNECTION DETAIL



ELEVATION
Aluminum Cantilever
Sign Structure



SECTION A-A



DAMPING DEVICE MOUNTING
TUBE U-BOLT DETAIL
(Typical)

TOP CHORD TO CROSS TUBE
U-BOLT DETAIL
(Typical)

- GENERAL NOTES**
- Damper: One damper per truss. (31 lbs. Stockbridge-Type Aluminum-29" minimum between ends of weights)
- Materials: Aluminum tubes shall be ASTM B221 alloy 6061 temper T6

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

OSC-A-D 12-1-08

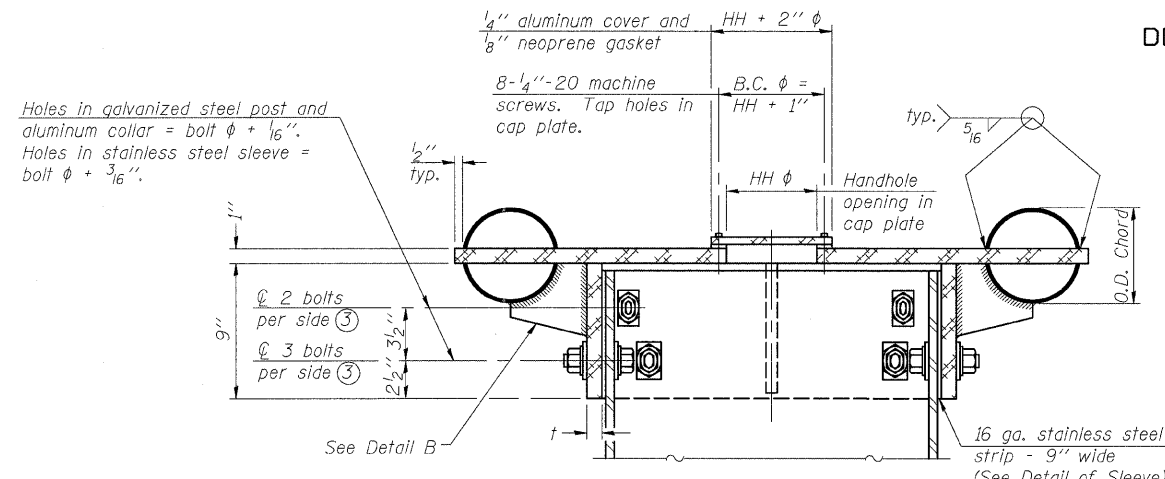
CANTILEVER SIGN STRUCTURE
DAMPING DEVICE

F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3
MADISON COUNTY

| | | | | | |
|---------------------------|-------------|----------|--------------------|--------------|-----------|
| SHEET NO. 3 9 SHEETS | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 270 | 60-2RS-3 | MADISON | 231 | 115 |
| | | | CONTRACT NO. 76D87 | | |
| ILLINOIS FED. AID PROJECT | | | | | |

HORNER & SHIFRIN, INC.
ENGINEERS

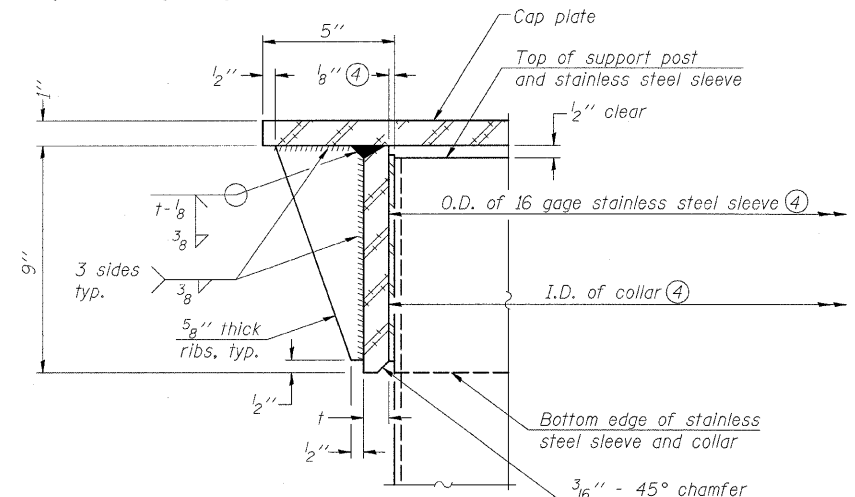
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



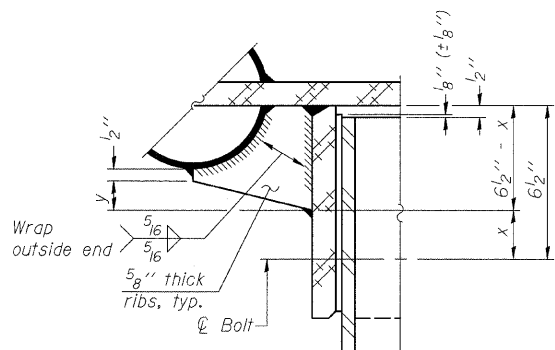
SECTION B-B

Bolts, washers (including contoured washers), and locknuts shall be stainless steel.

④ Collar I.D. shall be manufactured to correspond to O.D. of actual galvanized post and stainless steel sleeve plus 1/8 inch (±1/16 inch). Maximum gap between post and collar at any location equals 1/8 inch before tightening bolts.

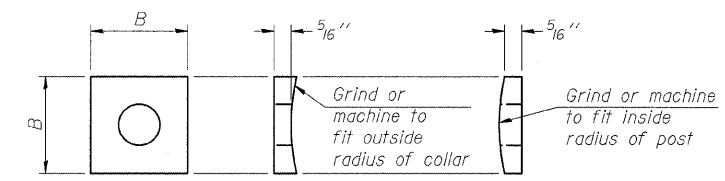


DETAIL A
(Two locations)



DETAIL B

Two locations (For details not shown, see Detail C)



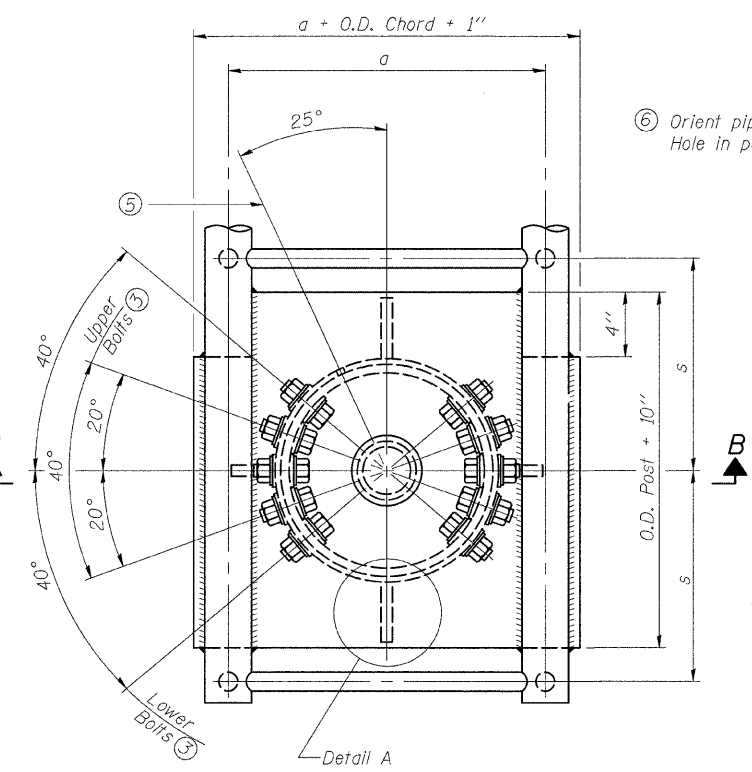
CONTOURED WASHERS

| Bolt Size | Contoured Washers | |
|-----------|-------------------|--------|
| | Hole Dia. | B |
| 7/8" | 1" | 2 1/2" |
| 1" | 1 1/8" | 3" |
| 1 1/4" | 1 3/8" | 3 1/4" |

DETAIL OF STAINLESS STEEL SLEEVE

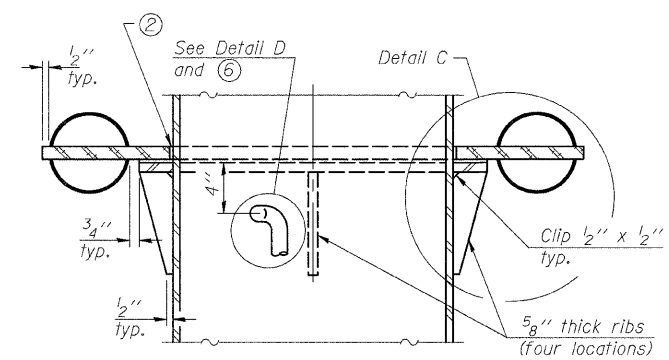
Weld to post after galvanizing. (Prepare post surface to insure tight, uniform fit and allow welding.) Welds to be 1/2 inch long at 6 inch cts. along top edge and at 1/4 inch opening.

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |

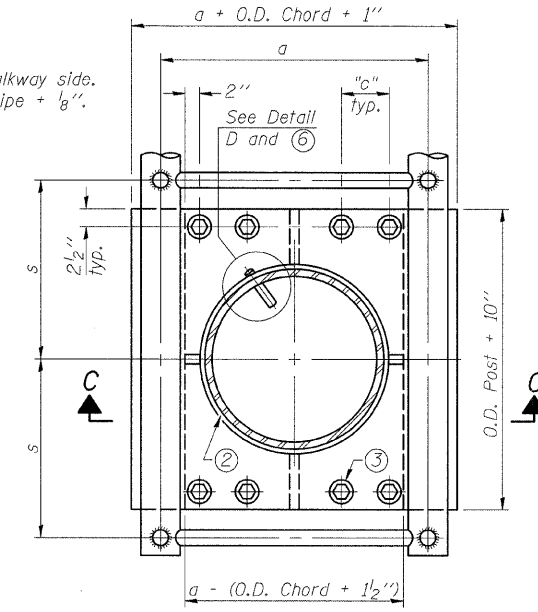


PLAN VIEW - TOP OF COLUMN

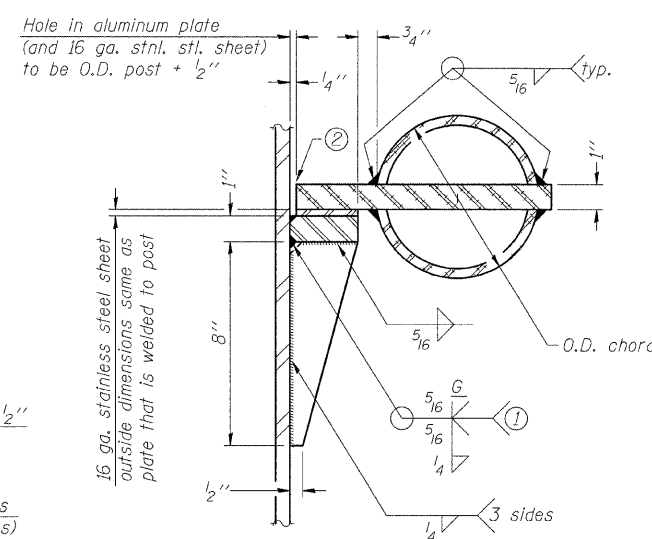
⑤ Optional full penetration weld in collar. (Two locations maximum....(180° apart)....X-ray or UT 100%)



SECTION C-C



SECTION THRU POST ABOVE LOWER CHORDS



DETAIL C

① Grind top if required to fully seat aluminum plate and stainless steel sheet.

② After tightening lower connection bolts, fill gap with non-hardening, silicone caulk suitable for exterior exposure and acceptable to the Engineer. Cost is included in Overhead Sign Structure Cantilever.

| Truss Type | Post Size | Upper & Lower Connection Bolt Diameter ③ | Lower Juncture Bolt Spacing Dimension "c" ③ | Opening in Cap Plate "HH" | Collar Thickness (t) | Side Ribs | |
|-----------------------|-----------------|--|---|---------------------------|----------------------|-----------|--------|
| | | | | | | x | y |
| I-C-A | 16" φ (83#/'') | 7/8" | 3 1/4" | 8" | 5/8" | 1 3/4" | 2 1/4" |
| II-C-A | 24" φ (125#/'') | 1" | 3 1/2" | 12" | 7/8" | 2" | 1 1/4" |
| III-C-A (35' max.) | 24" φ (125#/'') | 1 1/4" | 3 1/2" | 12" | 7/8" | 2" | 1" |
| III-C-A (>35' to 40') | 24" φ (171#/'') | 1 1/4" | 3 1/2" | 12" | 7/8" | 2" | 1" |

③ Upper and lower connection bolts in collar and bolts at lower chord connection shall be high strength with matching locknuts. Connection bolts shall have 2 stainless steel flat washers each.

HORNER & SHIFRIN, INC. ENGINEERS

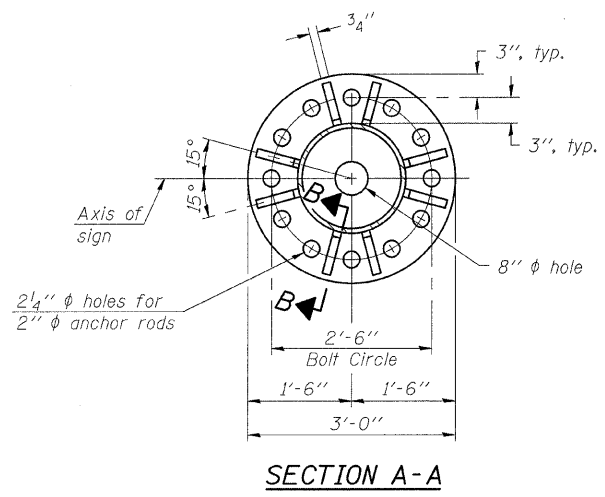
DESIGNED KLH
CHECKED JJD
DRAWN KLH
CHECKED JJD

OSC-A-3 12-1-08

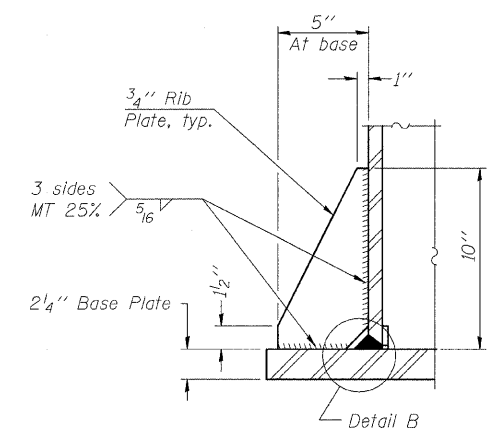
SHEET NO. 4
9 SHEETS

| CANTILEVER SIGN STRUCTURES JUNCTURE DETAILS ALUMINUM TRUSS & STEEL POST | | | | |
|---|----------|---------|--------------|-----------|
| F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3 MADISON COUNTY | | | | |
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 116 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

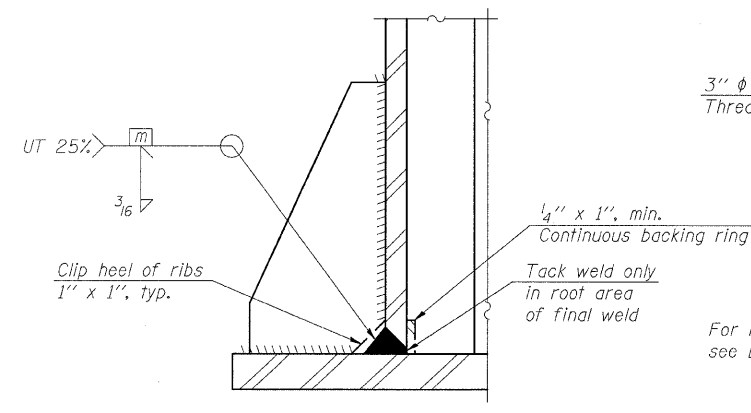
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



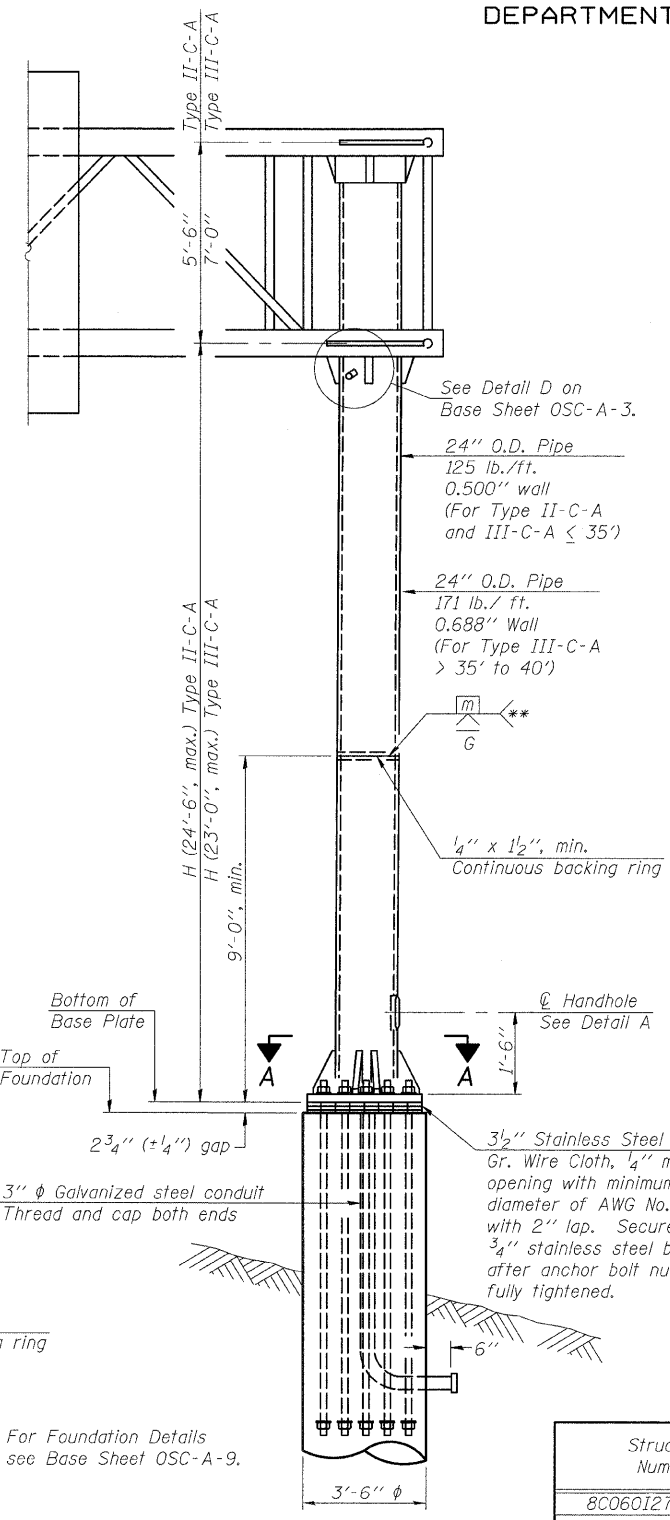
SECTION A-A



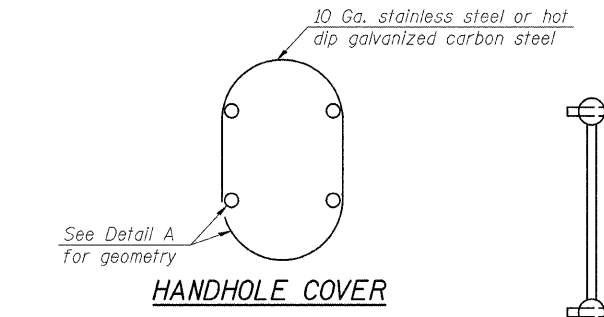
SECTION B-B



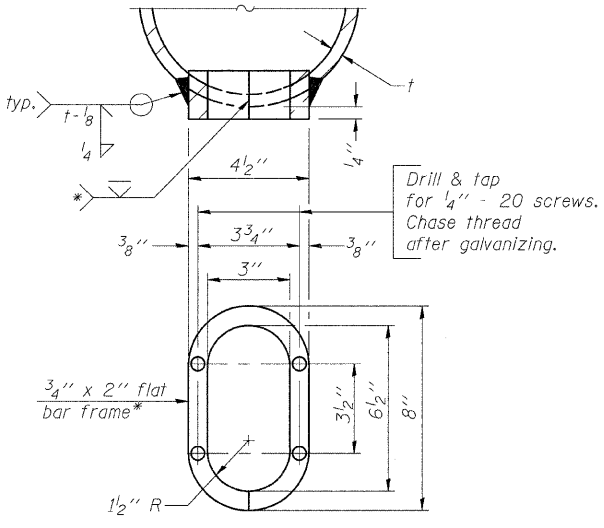
DETAIL B
(Typical rib)



FRONT ELEVATION



HANDHOLE COVER

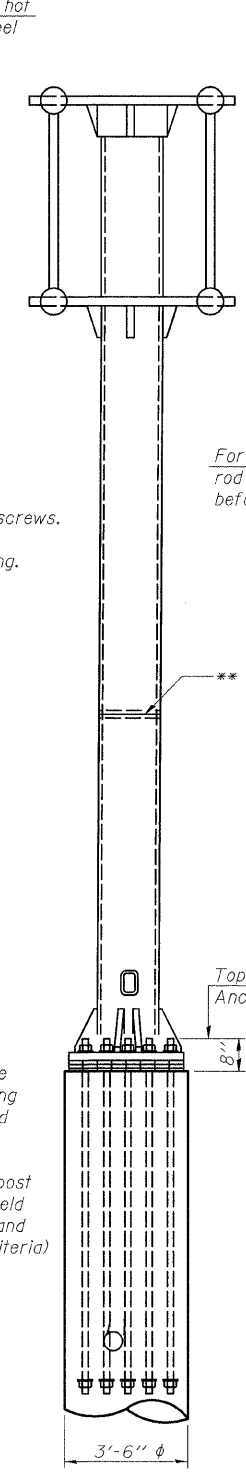


DETAIL A

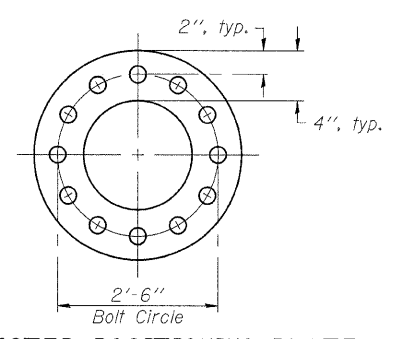
Provide 8" x 4 1/2" cover. Outside corners = 2 1/4" radius. Provide 4 - 5/16" phi holes in cover for 1/4" - 20 round head hot dip galvanized or stainless steel machine screws. (See cover details.)

* Bent bars may be butt welded top and bottom or bottom only. In lieu of fabricated handhole frame as shown, may cut from 2" plate (rolling direction vertical). All cut faces to be ground to ANSI Roughness of 500 min or less.

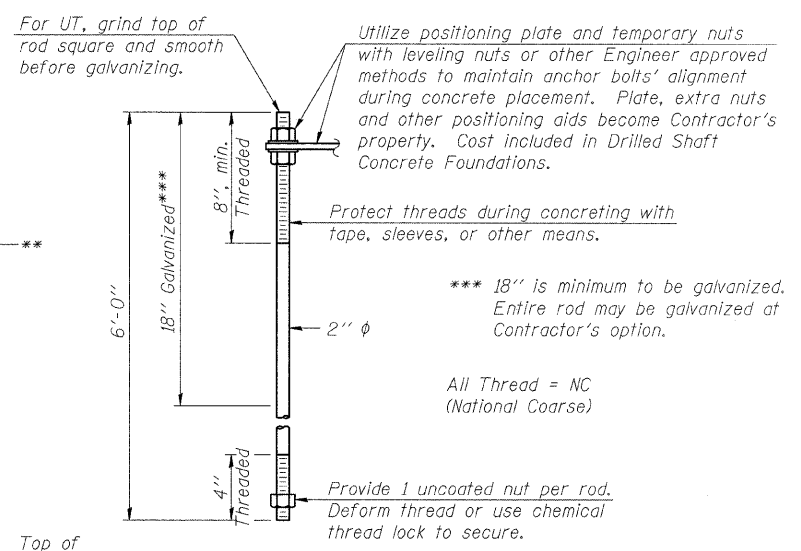
** Butt welded joint in post is only allowed for post heights (H) over 20 ft. in length. If used, weld procedure must be preapproved by Engineer and joint shall receive 100% RT or UT (tension criteria) at Contractor's expense.



SIDE ELEVATION



SUGGESTED POSITIONING PLATE



ANCHOR ROD DETAIL

Anchor rods shall conform to AASHTO M314 Grade 105 and meet Charpy V-Notch (CVN) energy of 15 lb.-ft. at 10° F. before galvanizing. Galvanize the upper 18" (minimum***) and associated M291, Grade A, C or DH heavy hex nuts and hardened washers per AASHTO M232. No welding shall be permitted on rods. Provide an unfinished nut at bottom, a hexagon locknut and washer above base plate and a leveling nut and washer below base plate. Nuts shall each be tightened with 200 lb.-ft. minimum torque against base plate. Before or after threading, but before galvanizing, each anchor rod shall be ultrasonically tested (UT) by a Level II or III inspector, qualified in accord with ANSI guidelines, using a straight beam, 1/2" phi 3.5 mhz. transducer, to insure no rejectable flaws exist in the upper 18" (tension criteria). Cost of testing included in Drilled Shaft Concrete Foundations.

| Structure Number | Station | H |
|------------------|-----------|--------|
| 8C0601270R002.7 | 235+35.00 | 22'-1" |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |

Note: "H" based on 15'-0" or actual sign height, whichever is greater.

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |
| | | |
| | | |

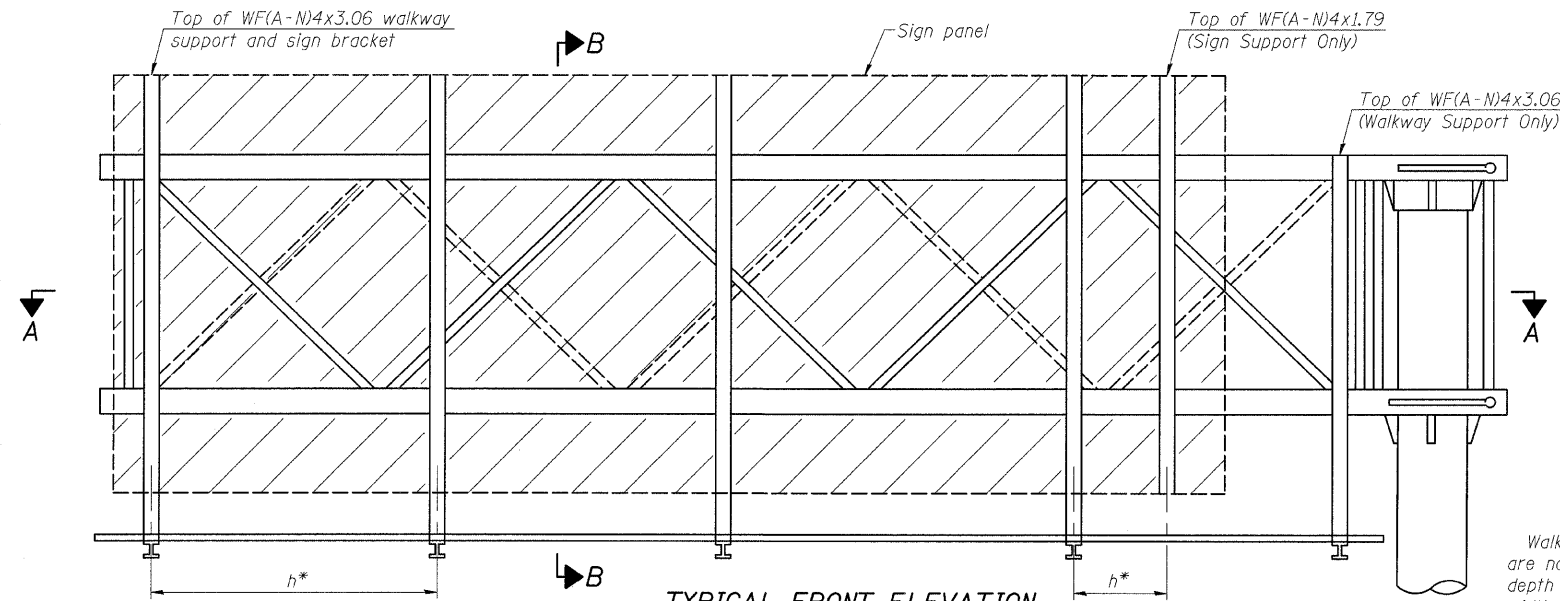
OSC-A-5 12-1-08

HORNER & SHIFRIN, INC.
ENGINEERS

SHEET NO. 5
9 SHEETS

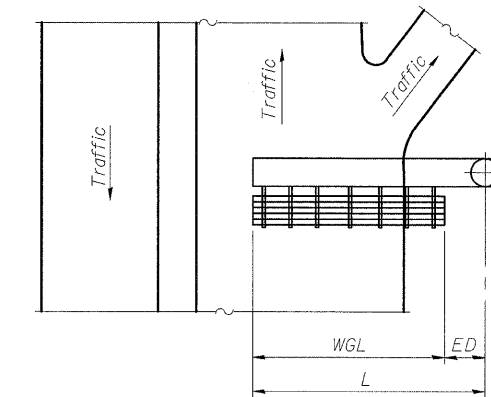
| CANTILEVER SIGN STRUCTURES TYPE II-C-A & III-C-A TRUSS SUPPORT POST ALUMINUM TRUSS & STEEL POST | | | | |
|---|----------|---------|--------------|-----------|
| F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3 MADISON COUNTY | | | | |
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 117 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

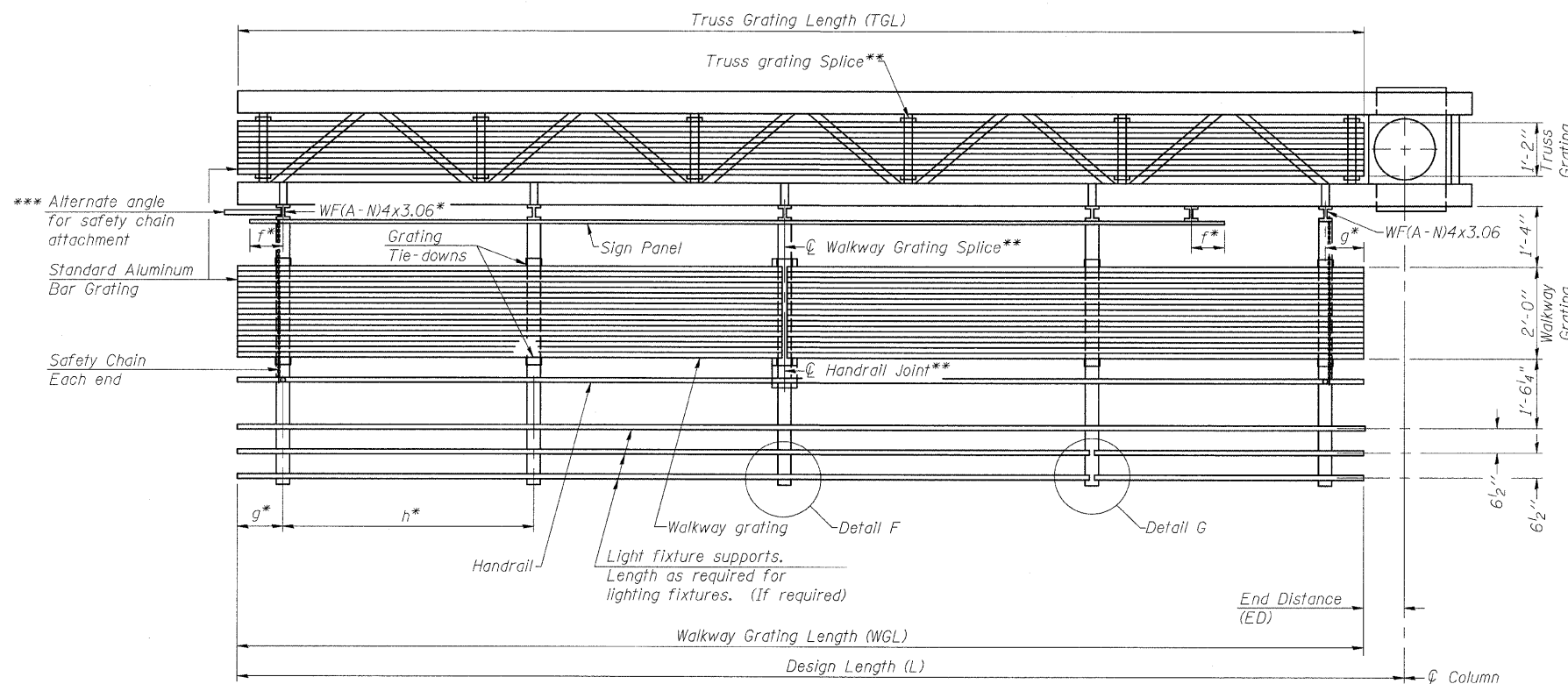


TYPICAL FRONT ELEVATION
With lights and handrail omitted for clarity.

Walkway and truss grating dimensions are nominal and may vary (width ±1/2", depth ±1/2") based on available standard widths.



PLAN WALKWAY AND HANDRAIL SKETCH
(Road plan beneath truss varies)



SECTION A-A

Truss grating to facilitate inspection shall run full length of cantilevers. Cost of truss grating is included in Overhead Sign Structure Cantilever.

Handrail and walkway grating shall span a minimum of three brackets between splices.
** Use and location of handrail joints or grating splices are optional, based on lengths needed and material availability.

$$TGL = L - \left(\frac{\text{Post O.D.}}{2} + 6'' \right)$$

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

OSC-A-6 12-1-08

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |
| | | |

HORNER & SHIFRIN, INC.
ENGINEERS

BRACKET TABLE

| Sign Width | | Number Brackets Required |
|--------------|-----------------------|--------------------------|
| Greater Than | Less Than or Equal To | |
| 8'-0" | 14'-0" | 2 |
| 14'-0" | 20'-0" | 3 |
| 20'-0" | 26'-0" | 4 |
| 26'-0" | 32'-0" | 5 |
| | | 6 |

| Structure Number | Station | WGL | ED | TGL |
|------------------|-----------|--------|--------|--------|
| 8C0601270R002.7 | 235+35.00 | 23'-0" | 12'-0" | 33'-6" |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |

Notes:
 * Space walkway brackets WF(A-N)4x3.06 and sign brackets WF(A-N)4x1.79 for efficiency and within limits shown:
 f = 12" maximum, 4" minimum (End of sign to center of nearest bracket)
 g = 12" maximum, 4" minimum (End of walkway to center of nearest bracket)
 h = 6'-0" maximum (center to center sign and/or walkway support brackets, WF(A-N)4x1.79 or WF(A-N)4x3.06)
 *** If walkway bracket at safety chain location is behind sign, add angle to bracket. See alternate safety chain attachment on base sheet OSC-A-8.
 For details of sign placement, sign/walkway brackets, truss and walkway gratings, grating splices and Section B-B, see Base Sheet OSC-A-7.
 For details of handrail, handrail joint, safety chain and Details F and G, see Base Sheet OSC-A-8.

| CANTILEVER SIGN STRUCTURES ALUMINUM WALKWAY DETAILS ALUMINUM TRUSS & STEEL POST | | | | |
|---|-------------|----------|---------|---------------|
| F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3 MADISON COUNTY | | | | |
| SHEET NO. 6 | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS |
| 9 SHEETS | 270 | 60-2RS-3 | MADISON | 231 |
| CONTRACT NO. 76D87 | | | | SHEET NO. 118 |
| ILLINOIS FED. AID PROJECT | | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SPECIFICATIONS FOR STANDARD ALUMINUM GRATING

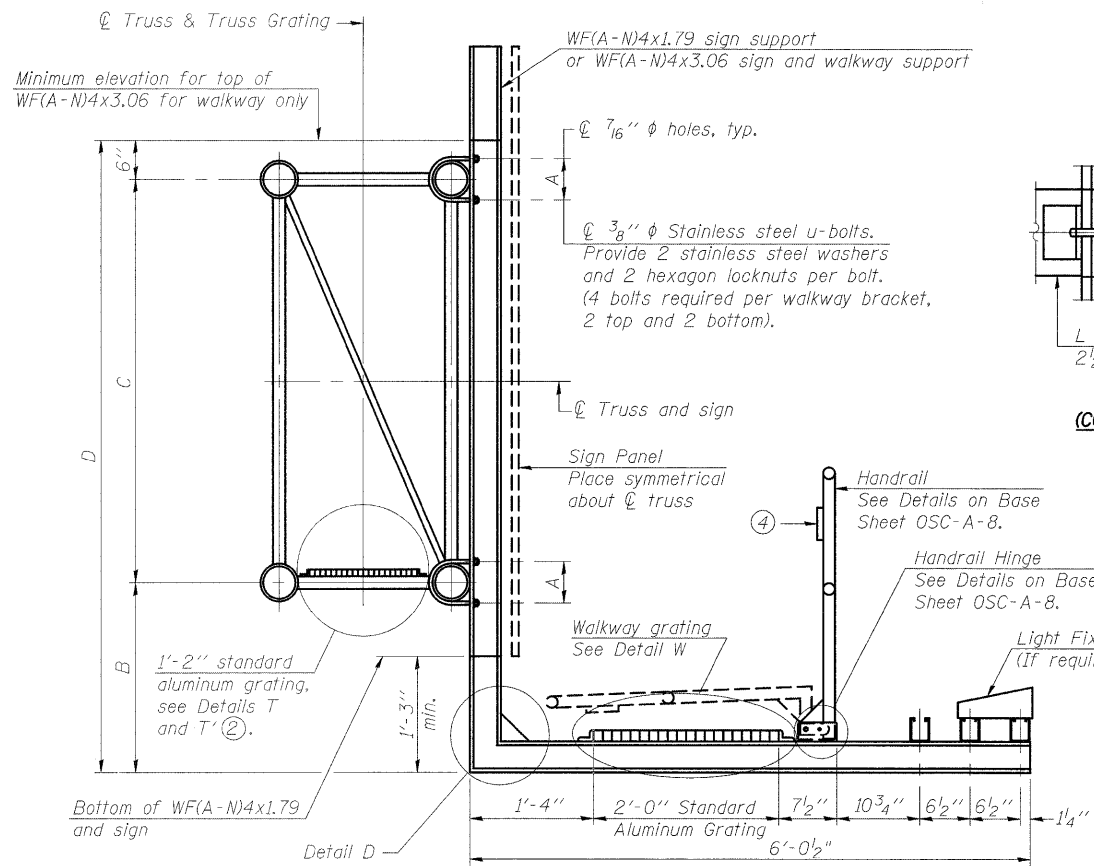
Main Bearing Bars (MBB) shall be $\frac{3}{16}$ " x $1\frac{1}{2}$ " on $1\frac{3}{16}$ " centers and conform to ASTM B211 Alloy 6061-T6.
Cross bars (CB) shall be $\frac{3}{16}$ " x $1\frac{1}{2}$ " on 4" centers and conform to ASTM B221 Alloy 6063-T5 or 6061-T6.

OR

Aluminum Grating with modified "T" sections for main bearing bars shall meet the following requirements:

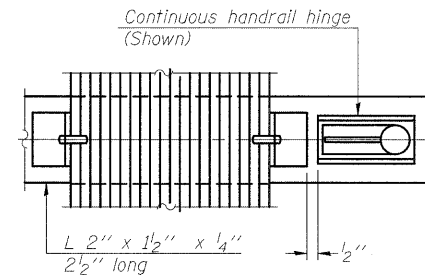
Main bars shall conform to ASTM B221 Alloy 6061-T6 and have a minimum section modulus equal to 0.0705 in.³ per bar, a depth of $1\frac{1}{2}$ ", spaced on $1\frac{3}{16}$ " centers.

Cross bars shall conform to ASTM B221 Alloy 6063-T5 or T-42 and spaced on 4" centers.



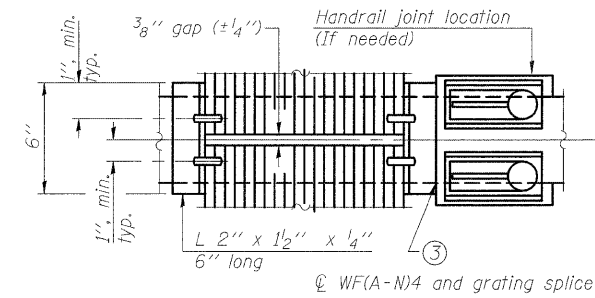
SECTION B-B

Sign shall be even with the top of the bracket, but it may extend no more than 6" above the top of the bracket for field adjustments.

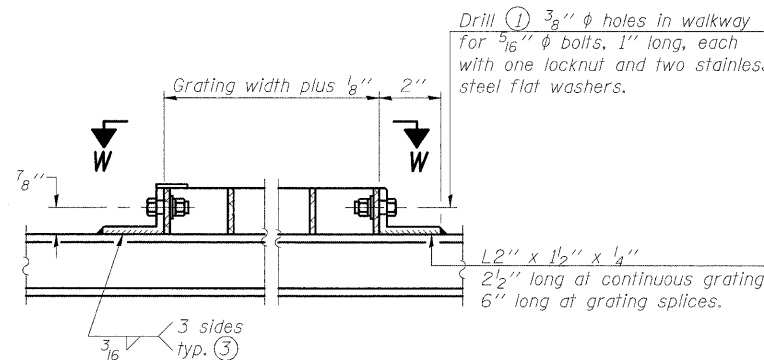


(CONTINUOUS WALKWAY GRATING)

SECTION W-W

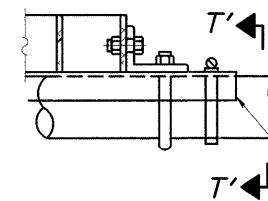


(AT WALKWAY GRATING SPLICE)



DETAIL W

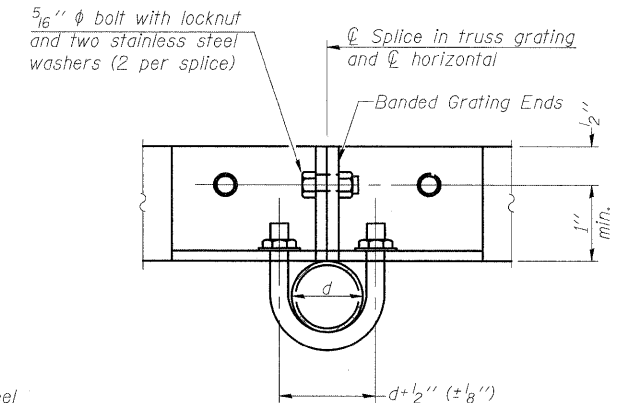
(Walkway grating)



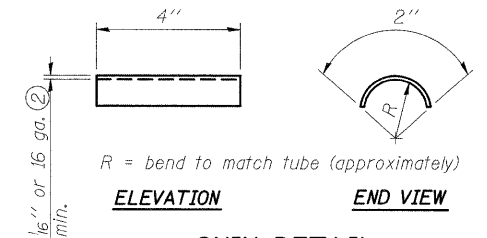
DETAIL T'

(Truss grating splice)

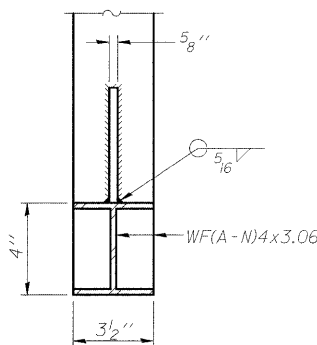
Details not shown same as Detail T. Alternate materials may be used subject to the Engineer's review and approval.



SECTION T'-T'

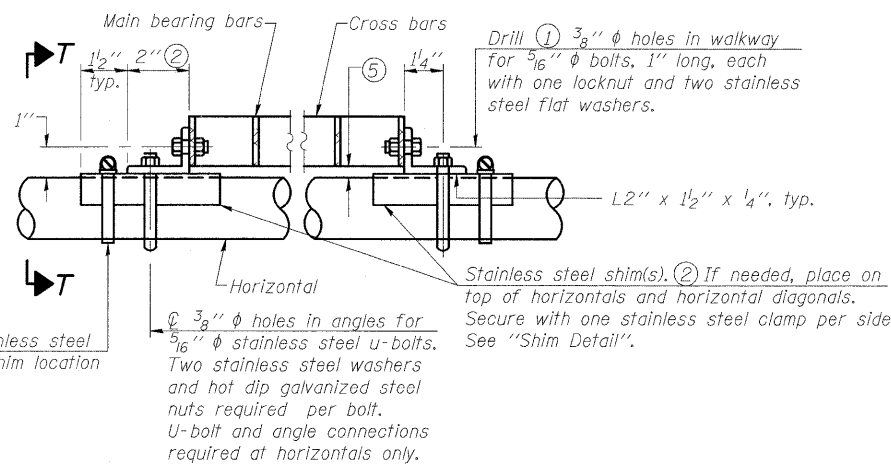


SHIM DETAIL



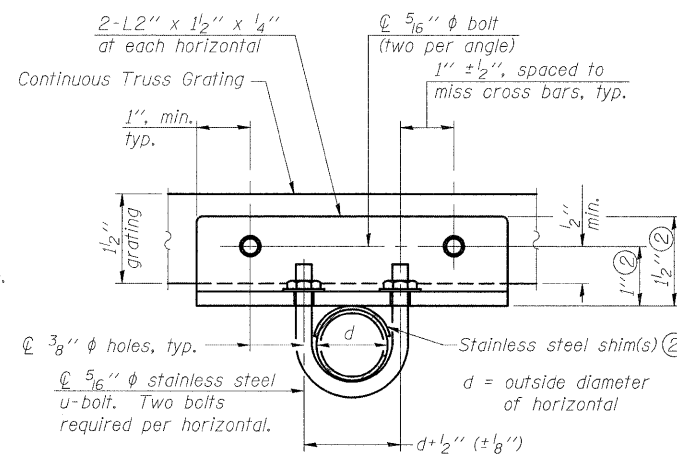
SECTION D-D

Screw type stainless steel tube clamp at shim location



DETAIL T

(Continuous Truss grating)



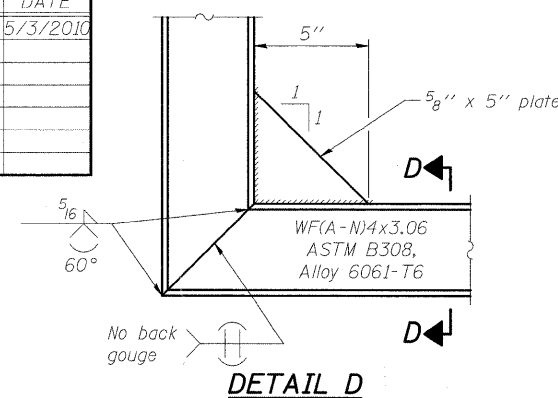
SECTION T-T

| NUMBER | REVISION | DATE |
|--------|--------------------|----------|
| 1 | Revised Dimensions | 5/3/2010 |

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

OSC-A-7

6-1-09



DETAIL D

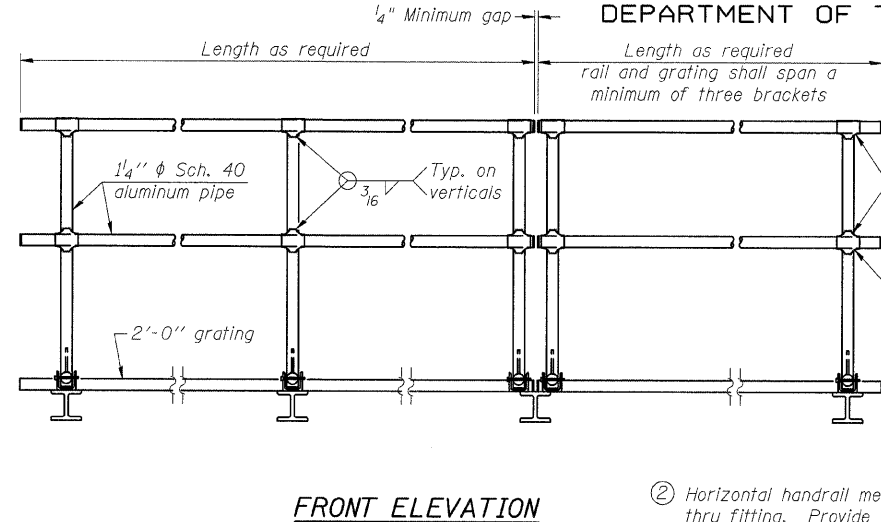
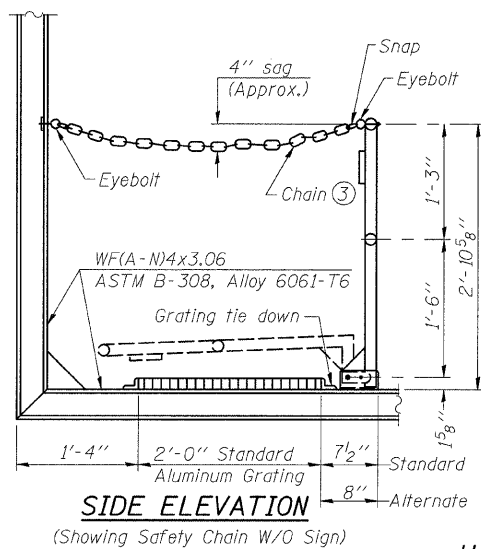
| Structure Number | Station | A | ⑥ B | C | ⑥ D |
|------------------|-----------|--------|-------|-------|--------|
| 8C0601270R002.7 | 235+35.00 | 7 1/2" | 2'-6" | 7'-0" | 10'-0" |

HORNER & SHIFRIN, INC.
ENGINEERS

SHEET NO. 7
9 SHEETS

| CANTILEVER SIGN STRUCTURES WALKWAY DETAILS ALUMINUM TRUSS & STEEL POST | | | | |
|--|----------|---------|--------------|-----------|
| F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3 MADISON COUNTY | | | | |
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 119 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



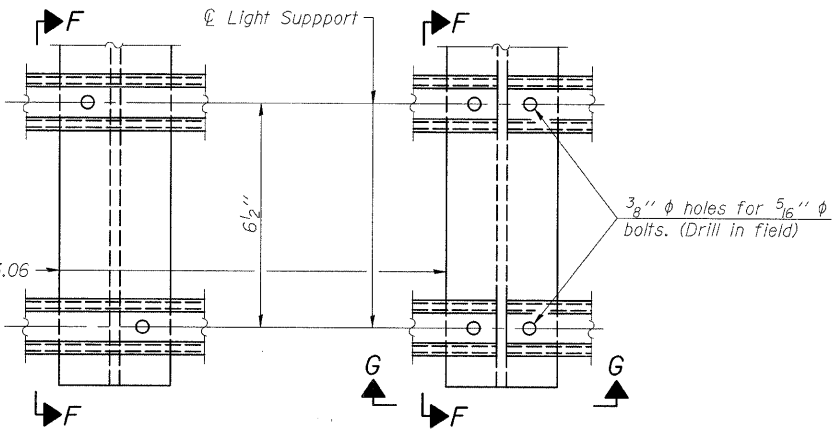
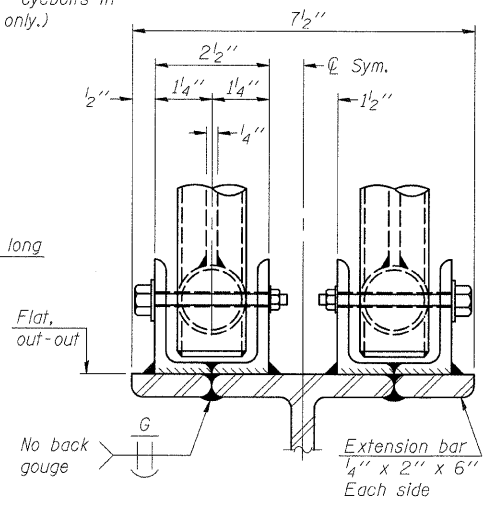
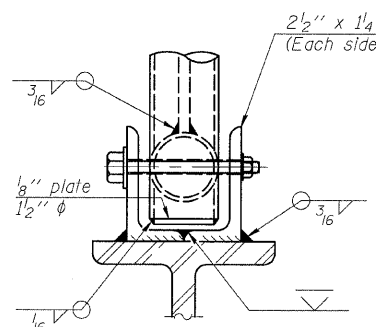
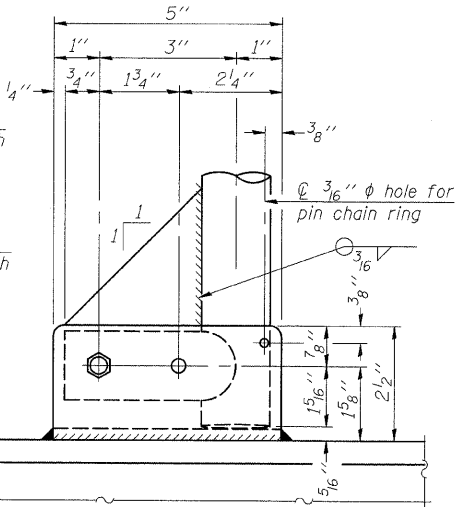
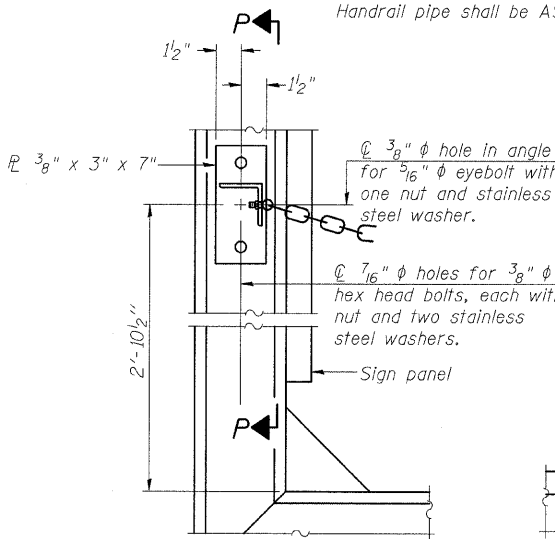
① Install standard force-fit end caps or weld 1/8" end plates with 1/8" c.f.w. and grind smooth. (All rail ends)

Fittings-ASTM B26, Alloy 356-T7 or 1 1/2" aluminum pipe

② Horizontal handrail member shall be continuous thru fitting. Provide 7/16" hole in fitting for 3/8" bolt. Field drill 7/16" hole in horizontal rail member. Provide locknut and two stainless steel washers for bolt. (Use 5/16" eyebolts in 7/16" holes on top rail at ends only.)

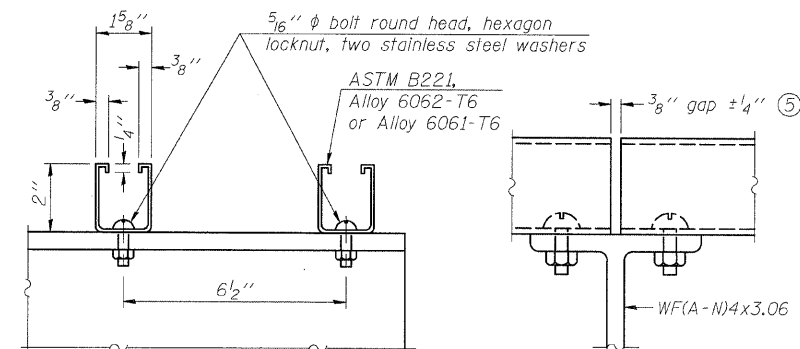
HANDRAIL DETAILS

Handrail pipe shall be ASTM B241 or B429, Alloy 6063-T6 or Alloy 6061-T6.



DETAIL F

DETAIL G



SECTION F-F

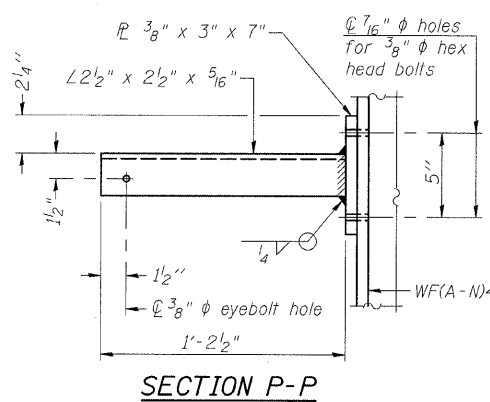
SECTION G-G

LIGHTING FIXTURE MOUNTS (IF REQUIRED)

⑤ Field cut ends of light support channels shall be free of burrs or hazardous projections and coated with zinc-rich primer or equivalent.

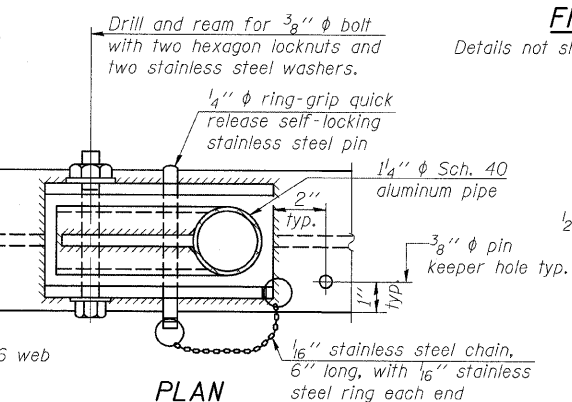
ALTERNATE SAFETY CHAIN ATTACHMENT (With Sign Present)

Items not shown same as "Side Elevation" of "Handrail Details"



SECTION P-P

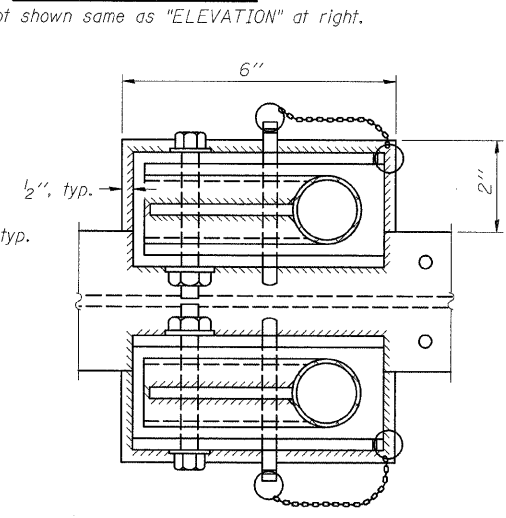
SIDE ELEVATION (Handrail Detail)



PLAN
DETAIL E HANDRAIL HINGE

Details not shown same as "ELEVATION" at right.

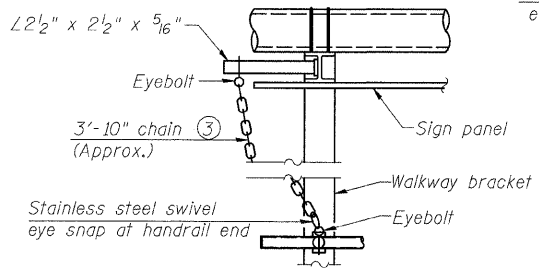
FRONT ELEVATION (Handrail Detail)



PLAN AT HANDRAIL JOINT

ELEVATION AT HANDRAIL JOINT

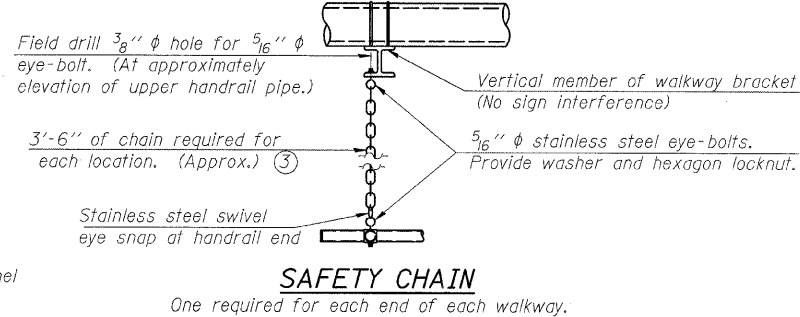
Details not shown same as "FRONT ELEVATION"



ALTERNATE SAFETY CHAIN ATTACHMENT

Details not shown similar to "Safety Chain" Details (Walkway omitted for clarity)

- ③ 3/16" Type 304L stainless steel chain, approximately 12 links per foot.
- ④ Extrusions may be used in lieu of the details shown, with approval of the Engineer.



SAFETY CHAIN

One required for each end of each walkway.

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

OSC-A-8 12-1-08

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |
| | | |

Details not shown same as "PLAN"

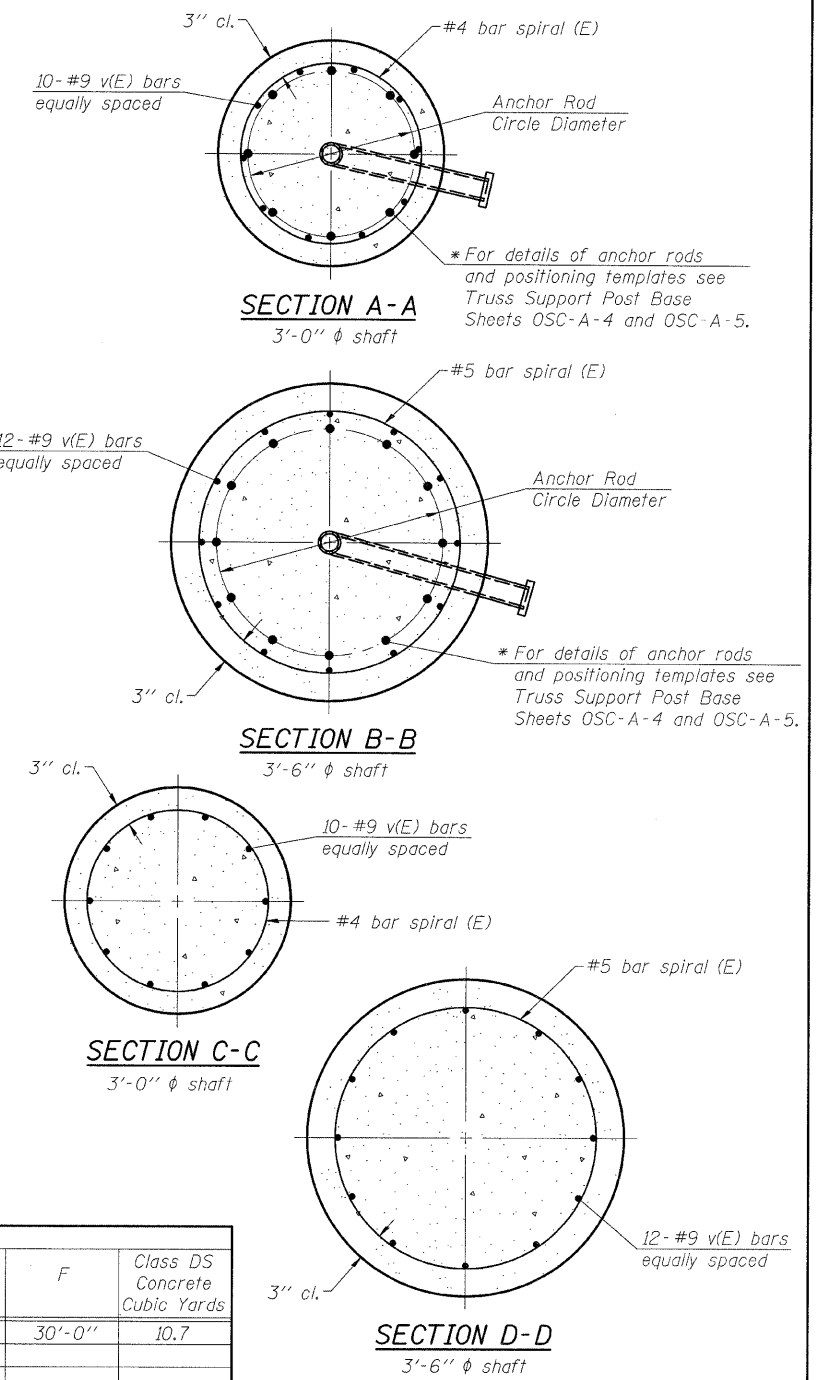
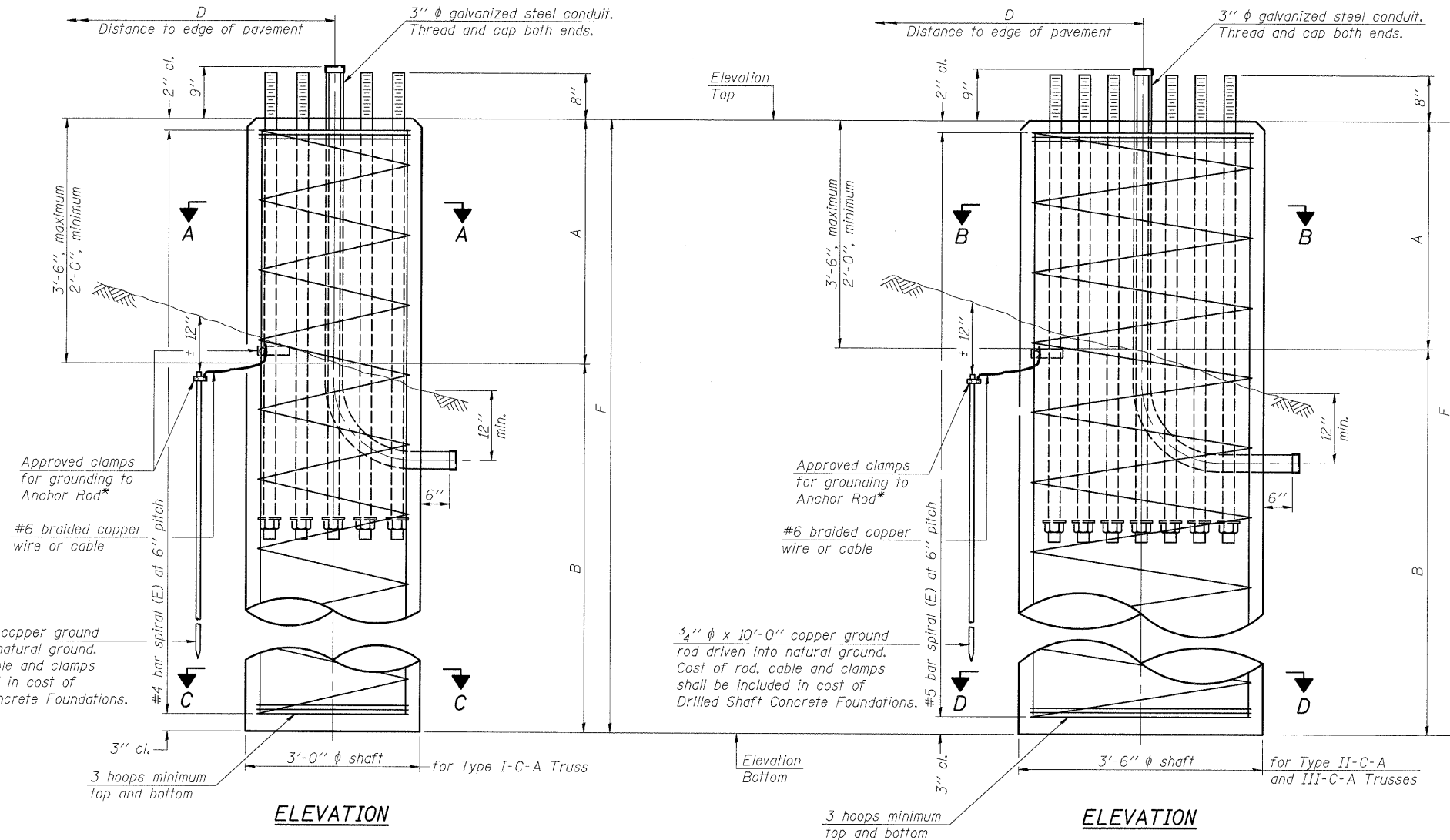


SHEET NO. 8
9 SHEETS

| CANTILEVER SIGN STRUCTURES HANDRAIL DETAILS ALUMINUM TRUSS & STEEL POST | | | | |
|---|----------|---------|---------------------------|-----------|
| F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3 MADISON COUNTY | | | | |
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 120 |
| CONTRACT NO. 76D87 | | | ILLINOIS FED. AID PROJECT | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

* Grind anchor rod to bright finish at ground clamp location before installing clamp.



NOTES:
The foundation dimensions shown in the Foundation Design Table are based on the presence of mostly cohesive soils with an average Unconfined Compressive Strength (Qu) of at least 1.25 tsf, which must be determined by previous soil investigations at the jobsite. When other conditions are indicated, the boring data will be included in the plans and the foundation dimensions shown in the Foundation Data Table will be the result of site specific designs.
If the conditions encountered are different than those indicated, the Contractor shall notify the Engineer to determine if the foundation dimensions need to be modified. If dimensions "B" or "F" are revised by more than 12" by the Contractor, "as-built" plans shall be prepared and submitted to the District Bureau of Operations for future reference.
No sonotubes or decomposable forms shall be used below the lower conduit entrance. Permanent metal forms or other shielding may not be left in place below that elevation without the Engineer's written permission.
Concrete shall be placed monolithically, without construction joints.
Backfill shall be placed per Article 502 of Standard Specification and prior to erection of support column.
A normal surface finish followed by a Bridge Seat Sealer application will be required on concrete surfaces above the lowest elevation 6" below finished ground line. Cost included in "Drilled Shaft Concrete Foundation".

| Structure Number | Station | Truss Type | Shaft Diameter | Elevation Top | Elevation Bottom | Qu | A | B | F | Class DS Concrete Cubic Yards |
|------------------|-----------|------------|----------------|---------------|------------------|---------|-------|--------|--------|-------------------------------|
| 8C0601270R002.7 | 235+35.00 | III-C-A | 3'-6" | 436.94 | 406.94 | 1.4 tsf | 3'-6" | 26'-6" | 30'-0" | 10.7 |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |

| Truss Type | Post Base Sheet | Maximum Cantilever Length (ft) | Maximum Total Sign Area (sq ft) | Shaft Diameter (in) | "B" Depth (ft) | Anchor Rods | | Anchor Rod Circle Diameter (in) |
|------------|-----------------|--------------------------------|---------------------------------|---------------------|----------------|-------------|---------------|---------------------------------|
| | | | | | | No. | Diameter (in) | |
| I-C-A | OSC-A-4 | 25 | 170 | 3.0 | 16.0 | 8 | 2 | 22 |
| II-C-A | OSC-A-5 | 30 | 170 | 3.5 | 17.0 | 12 | 2 | 30 |
| II-C-A | OSC-A-5 | 30 | 340 | 3.5 | 21.5 | 12 | 2 | 30 |
| III-C-A | OSC-A-5 | 35 | 170 | 3.5 | 19.0 | 12 | 2 | 30 |
| III-C-A | OSC-A-5 | 35 | 250 | 3.5 | 22.5 | 12 | 2 | 30 |
| III-C-A | OSC-A-5 | 35 | 400 | 3.5 | 26.5 | 12 | 2 | 30 |
| III-C-A | OSC-A-5 | 40 | 400 | 3.5 | 32.0 | 12 | 2 | 30 |

DESIGNED KLH
CHECKED JJD
DRAWN KLH
CHECKED JJD

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |

OSC-A-9 12-1-08

HORNER & SHIFRIN, INC.
ENGINEERS

SHEET NO. 9
9 SHEETS

**EXISTING CANTILEVER SIGN STRUCTURES
DRILLED SHAFT
ALUMINUM TRUSS & STEEL POST**

F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3
MADISON COUNTY

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 121 |

CONTRACT NO. 76D87
ILLINOIS FED. AID PROJECT

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

DESIGN: AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals. ("AASHTO Specifications")

CONSTRUCTION: Current (at time of letting) Illinois Department of Transportation Standard Specifications for Road and Bridge Construction, Supplemental Specifications and Special Provisions. ("Standard Specifications")

LOADING: 90 M.P.H. WIND VELOCITY

WALKWAY LOADING: Dead load plus 500 lbs. concentrated live load.

DESIGN STRESSES:
Field Units
 $f'_c = 3,500$ p.s.i.
 $f_y = 60,000$ p.s.i. (reinforcement)

WELDING: All welds to be continuous unless otherwise shown. All welding to be done in accordance with current AWS D1.1 and D1.2 Structural Welding Codes (Steel and Aluminum) and the Standard Specifications.

MATERIALS: Aluminum Alloys as shown throughout plans. All Structural Steel Pipe shall be ASTM A53 Grade B or A500 Grade B or C. If A500 pipe is substituted for A53, then the outside diameter shall be as detailed and wall thickness greater than or equal to A53. All Structural Steel Plates and Shapes shall conform to AASHTO M270 Gr. 36, Gr. 50 or Gr. 50W*. Stainless steel for shims, sleeves and handhole covers shall be ASTM A240, Type 302 or 304, or another alloy suitable for exterior exposure and acceptable to the Engineer. The steel pipe and stiffening ribs at the base plate for the column shall have a minimum longitudinal Charpy V-Notch (CVN) energy of 15 lb.-ft. at 40° F. (Zone 2) before galvanizing.

FASTENERS FOR ALUMINUM TRUSSES: All bolts noted as "high strength" must satisfy the requirements of AASHTO M164 (ASTM A325), or approved alternate, and must have matching lock nuts. Threaded studs for splices (if Members interfere) must satisfy the requirements of ASTM A449, ASTM A193, Grade B7, or approved alternate, and must have matching lock nuts. Bolts and lock nuts not required to be high strength must satisfy the requirements of ASTM A307. All bolts and lock nuts must be hot dip galvanized per AASHTO M232. The lock nuts must have nylon or steel inserts. A stainless steel flat washer conforming to ASTM A240 Type 302 or 304, is required under both head and nut or under both nuts where threaded studs are used. High strength bolt installation shall conform to Article 505.04 (f) (2)d of the IDOT Standard Specifications for Road and Bridge Construction. Rotational capacity ("ROCAP") testing of bolts will not be required.

U-BOLTS AND EYEBOLTS: U-Bolts and Eyebolts must be produced from ASTM A276 Type 304, 304L, 316 or 316L, Condition A, cold finished stainless steel, or an equivalent material acceptable to the Engineer. All nuts for U-Bolts and Eyebolts must be lock nuts equivalent to ASTM A307 with nylon or steel inserts and hot dip galvanized per AASHTO M232. A stainless steel flat washer conforming to ASTM A240, Type 302 or 304, is required under each U-Bolt and Eyebolt lock nut.

GALVANIZING: All Steel Grating, Plates, Shapes and Pipe shall be Hot Dip Galvanized after fabrication in accordance with AASHTO M111. Painting is not permitted.

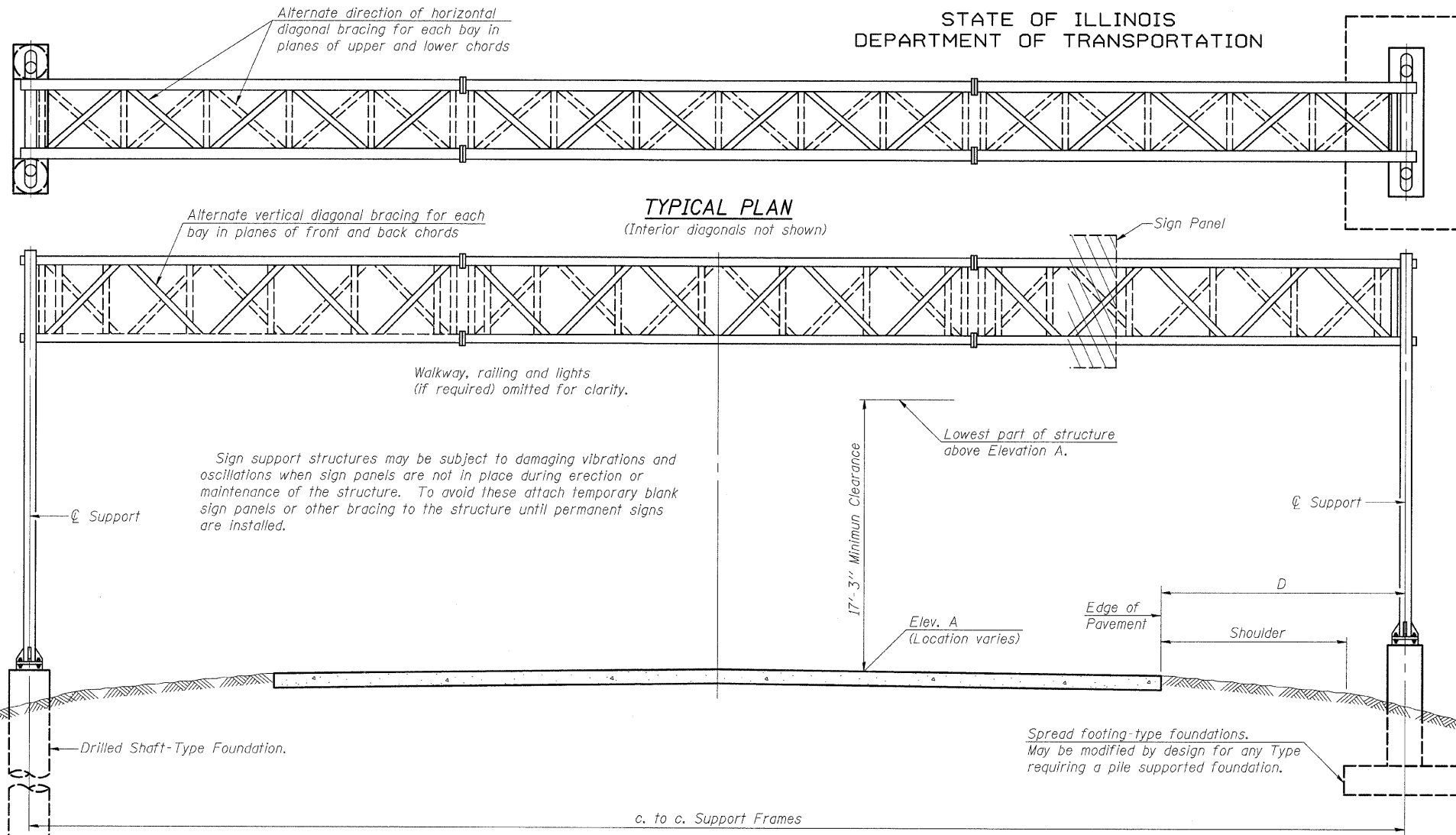
ANCHOR RODS: Shall conform to AASHTO M314 Gr. 36, 55 or 105 with a minimum Charpy V-Notch (CVN) energy of 15 lb.-ft. at 40° F.

CONCRETE SURFACES: All concrete surfaces above an elevation 6" below the lowest final ground line at each foundation shall be cleaned and coated with Bridge Seat Sealer in accordance with the Standard Specifications.

REINFORCEMENT BARS: Reinforcement Bars designated (E) shall be epoxy coated in accordance with the Standard Specifications.

FOUNDATIONS: The contract unit price for Concrete Foundations and Drilled Shaft Concrete Foundations shall include reinforcement bars complete in place.

* If M270 Gr. 50W (M222) steel is proposed, chemistry for plate to be used shall first be approved by the Engineer as suitable for galvanizing and welding.



TYPICAL ELEVATION
(Looking at Face of Signs)**

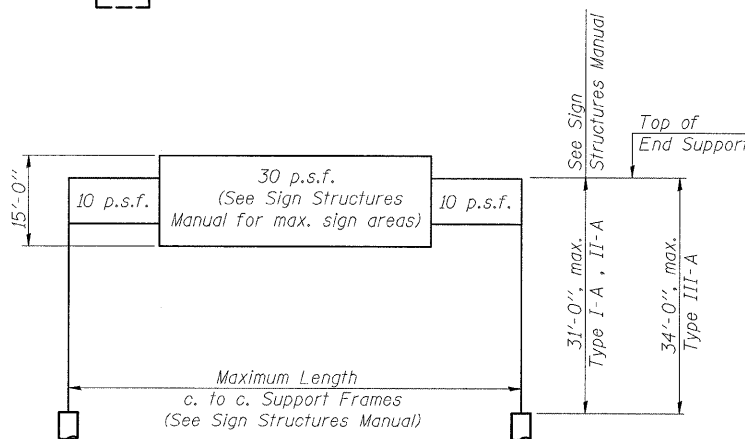
Elev. A = Elevation at point of minimum clearance to sign, walkway support or truss.

| Structure Number | Station | Design Truss Type | c. to c. Supports | Elev. A | Dim. D | Height of Tallest Sign | Total Sign Area |
|------------------|-------------|-------------------|-------------------|---------|--------|------------------------|-----------------|
| 8S060I270L003.0 | 249+50.00 | I-A | 69' | 446.54 | 16'-9" | 11'-6" | 382 sf |
| 8S060S003L009.3 | 1347+50.00 | I-A | 86' | 420.90 | 17'-8" | 9'-0" | 261 sf |
| 8S060I270L002.8 | 31271+50.00 | I-A | 71' | 425.13 | 22'-0" | 7'-6" | 270 sf |
| 8S060S003R008.6 | 1370+32.00 | I-A | 60' | 416.20 | 11'-0" | 9'-0" | 262 sf |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |

**Looking upstation for structures with signs both sides.

TOTAL BILL OF MATERIAL

| ITEM | UNIT | TOTAL |
|---|----------|-------|
| OVERHEAD SIGN STRUCTURE SPAN TYPE I-A | Foot | 286 |
| OVERHEAD SIGN STRUCTURE SPAN TYPE II-A | Foot | 0 |
| OVERHEAD SIGN STRUCTURE SPAN TYPE III-A | Foot | 0 |
| OVERHEAD SIGN STRUCTURE WALKWAY TYPE A | Foot | 165 |
| CONCRETE FOUNDATIONS | Cu. Yds. | 100.0 |
| DRILLED SHAFT CONCRETE FOUNDATIONS | Cu. Yds. | 20.9 |



DESIGN WIND LOADING DIAGRAM

Parameters shown are basis for I.D.O.T. Standards and Sign Manual Tables. Installations not within dimensional limits shown require special analysis for all components.

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |

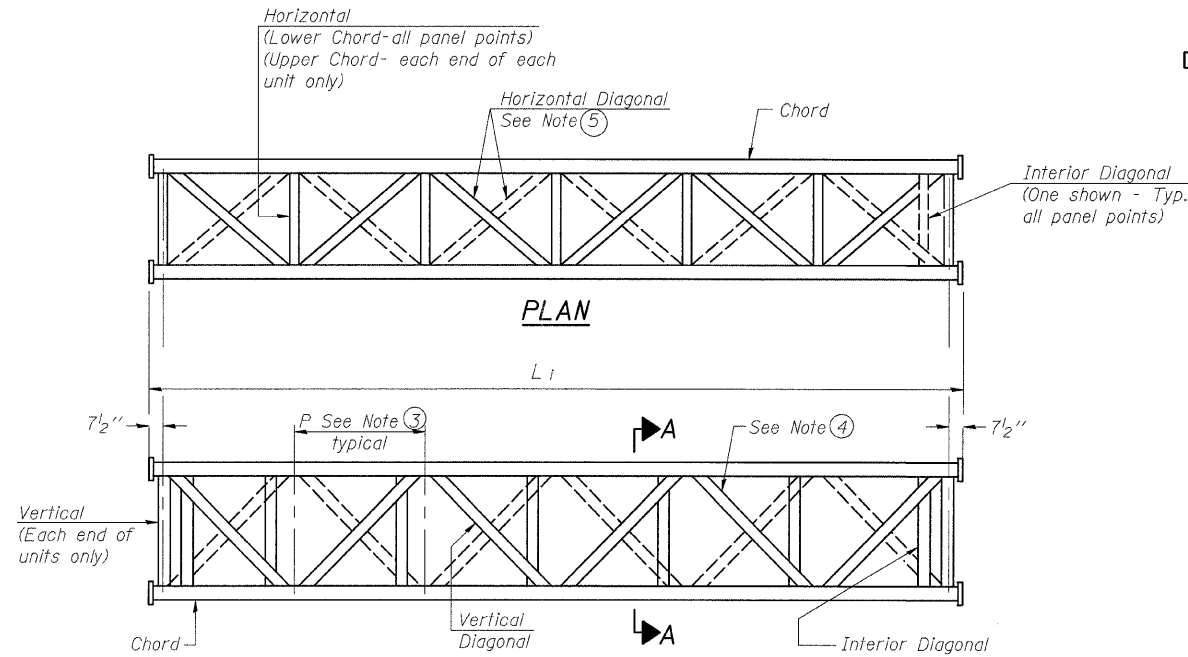
OS-A-1 12-1-08

OVERHEAD SIGN STRUCTURES
GENERAL PLAN & ELEVATION
ALUMINUM TRUSS & STEEL SUPPORTS
F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3
MADISON COUNTY

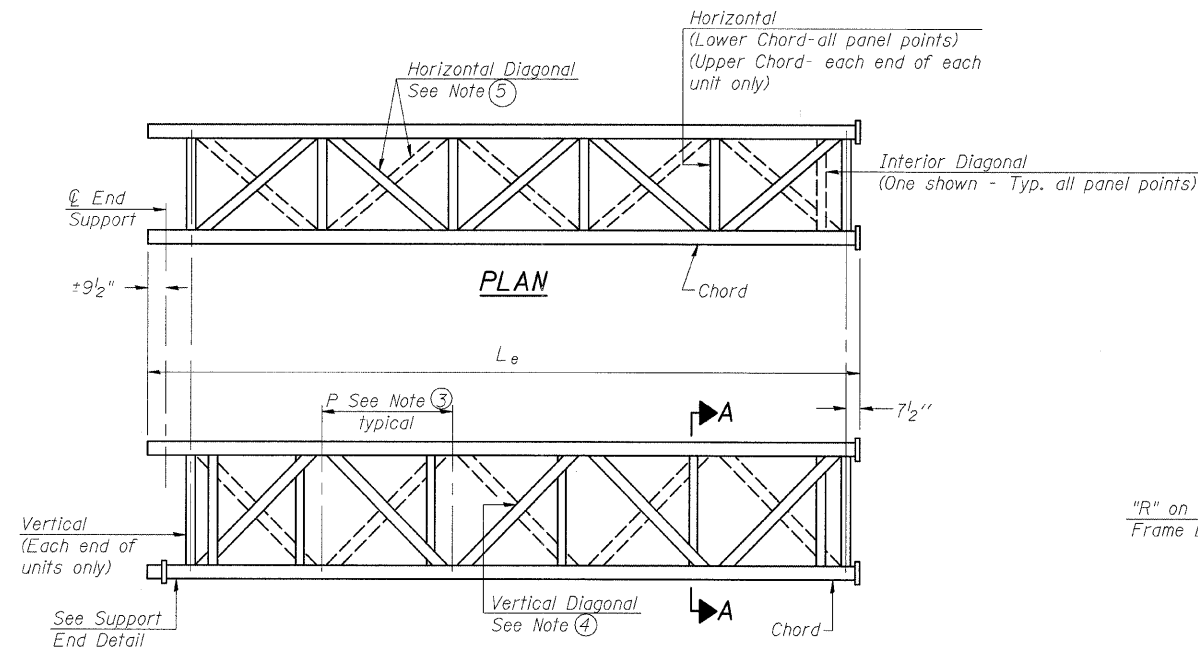
| | | | | | |
|--------------------------|-------------|----------|---------|--------------|-----------|
| SHEET NO. 1 14 SHEETS | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | 270 | 60-2RS-3 | MADISON | 231 | 122 |
| CONTRACT NO. 76D87 | | | | | |



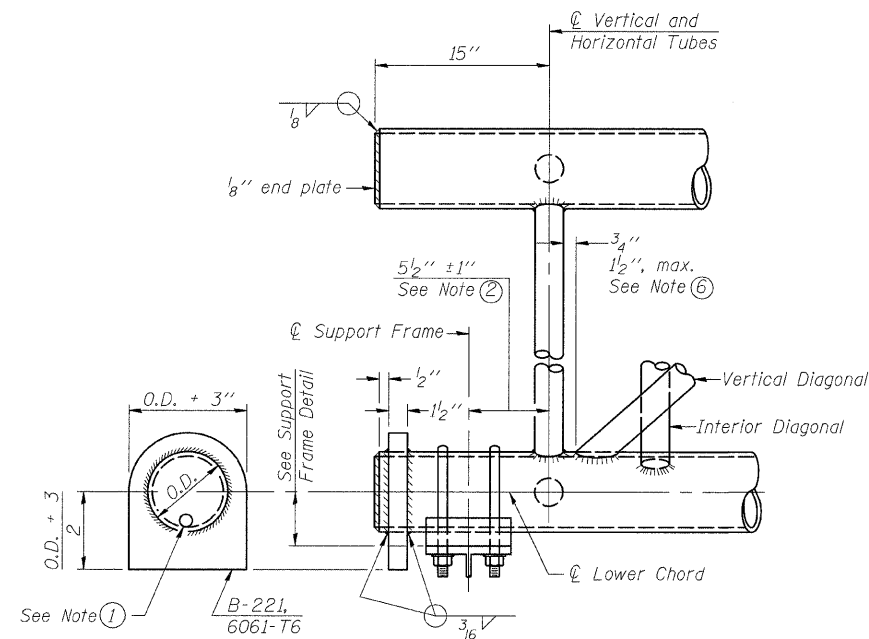
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



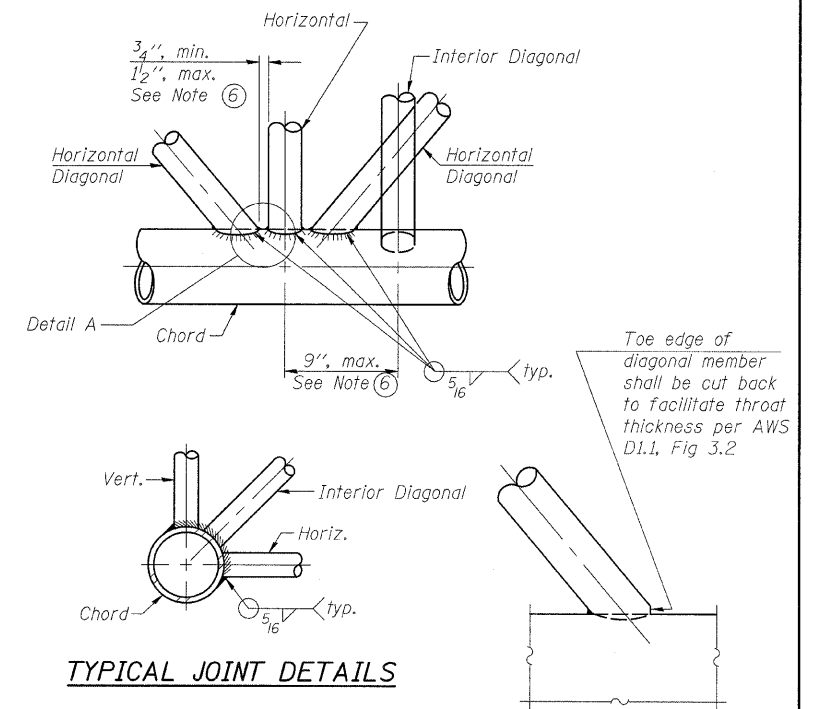
ELEVATION
TYPICAL INTERIOR UNIT
Even number of panels/interior unit required.



ELEVATION
TYPICAL EXTERIOR UNIT
Even or odd number of panels/interior units allowed.

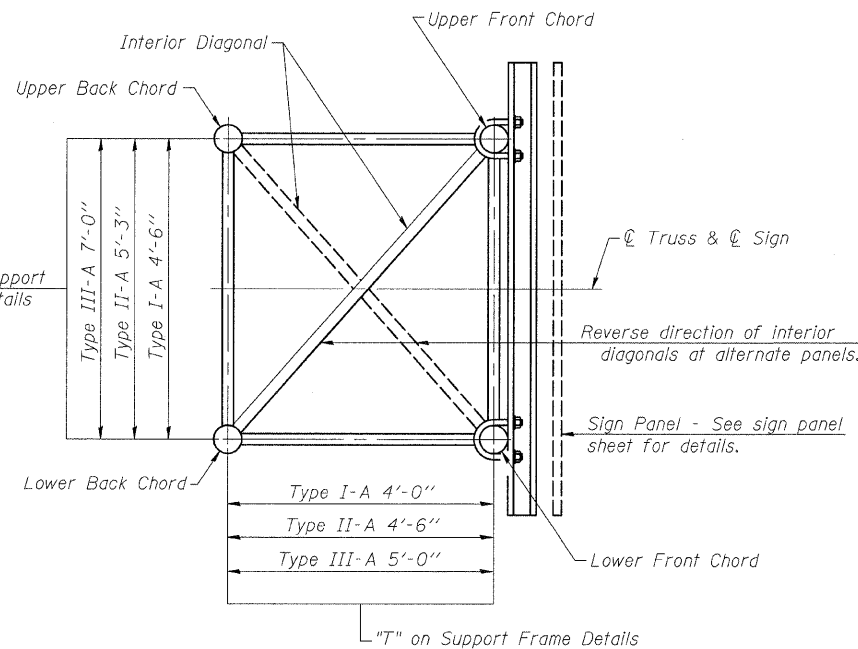


SUPPORT END DETAIL FOR EXTERIOR UNIT



TYPICAL JOINT DETAILS

DETAIL A



SECTION A-A

- ① Contractor may alternatively use standard aluminum drive-fit cap to close end. 1/2" ϕ drain hole in end plate/drive-fit cap. (Typ. at ends of all chords)
- ② 5 1/2" end dimension may vary by \pm 1" to provide uniform panel spacing (P).
- ③ Panel spacing (P) shall be uniform for entire truss and between 4'-0" and 5'-0" for Type I-A.
- ④ Vertical Diagonals in front and back face shall alternate.
- ⑤ Hidden lines show wind bracing alternates direction between planes of top and bottom chords.
- ⑥ All diagonals shall be detailed for minimum offset from the panel point based on the following: Offset shall be such as to provide a 3/4" minimum to 1 1/2" maximum clearance between any diagonal and any horizontal or vertical member, and to provide clearance for U-bolt connections of signs or walkway brackets.

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

OS-A-2

12-1-08

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |
| | | |
| | | |

HORNER & SHIFRIN, INC.
ENGINEERS

SHEET NO. 2
14 SHEETS

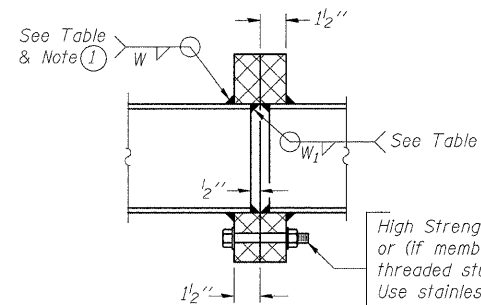
OVERHEAD SIGN STRUCTURES
ALUMINUM TRUSS DETAILS
FOR TRUSS TYPES I-A, II-A AND III-A
F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3
MADISON COUNTY

| | | | | |
|--------------------|------------------|----------------|---------------------------|---------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 123 |
| CONTRACT NO. 76D87 | | | ILLINOIS FED. AID PROJECT | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRUSS UNIT TABLE

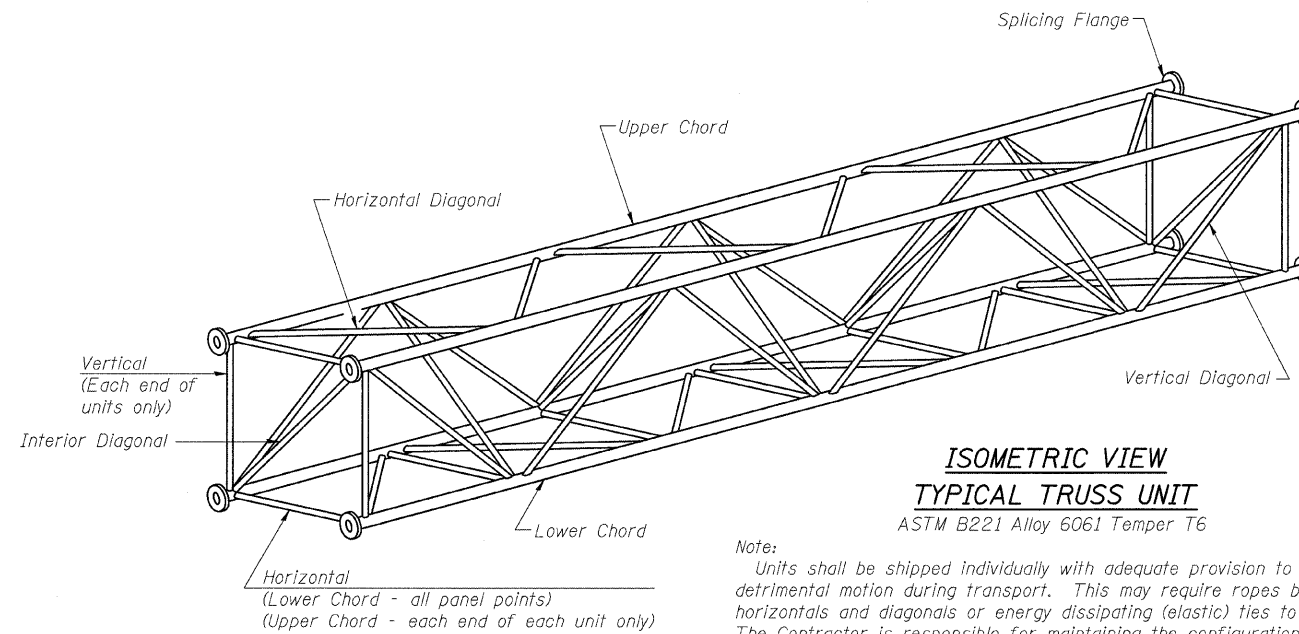
| Structure Number | Station | Design Truss Type | Exterior Units (2) | | Interior Unit | | Upper & Lower Chord | | Verticals; Horizontals; Vertical, Horizontal, and Interior Diagonals | | Camber at Midspan | Splicing Flange | | | | | | | | |
|------------------|-------------|-------------------|---------------------|-----------------------------|----------------|------------|---------------------|-----------------------------|--|------|-------------------|-----------------|-------|--------|------------|------|------------|----------------|--------|---------|
| | | | No. Panels per Unit | Unit Lgth.(L _u) | Panel Lgth.(P) | No. Req'd. | No. Panels per Unit | Unit Lgth.(L _i) | Panel Lgth.(P) | O.D. | | Wall | O.D. | Wall | Bolts | | Weld Sizes | | | |
| | | | | | | | | | | | | | | | No./Splice | Dia. | W | W ₁ | A | B |
| 8S0601270L003.0 | 249+50.00 | I-A | 7 | 35'-3 1/4" | 4'-9 1/4" | - | - | - | - | 5" | 5/16" | 2 1/2" | 5/16" | 1 3/4" | 6 | 7/8" | 5/16" | 1/4" | 8 3/4" | 11 3/4" |
| 8S060S003L009.3 | 1347+50.00 | I-A | 6 | 29'-4 1/2" | 4'-7" | 1 | 6 | 28'-9" | 4'-7" | 5" | 5/16" | 2 1/2" | 5/16" | 2 1/2" | 6 | 7/8" | 5/16" | 1/4" | 8 3/4" | 11 3/4" |
| 8S0601270L002.8 | 31271+50.00 | I-A | 7 | 36'-3 1/2" | 4'-11" | - | - | - | - | 5" | 5/16" | 2 1/2" | 5/16" | 1 3/4" | 6 | 7/8" | 5/16" | 1/4" | 8 3/4" | 11 3/4" |
| 8S060S003R008.6 | 1370+32.00 | I-A | 6 | 30'-10 1/2" | 4'-10" | - | - | - | - | 5" | 1/4" | 2 1/2" | 1/4" | 1 1/4" | 6 | 7/8" | 5/16" | 1/4" | 8 3/4" | 11 3/4" |



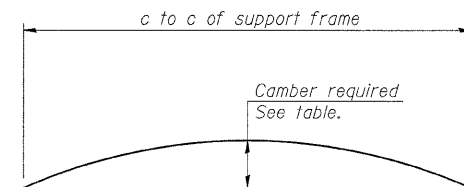
SECTION B-B

High Strength bolts with locknuts or (if members interfere) threaded studs with 2 locknuts. Use stainless steel washers under head and nut. See table.

① Splicing Flanges shall be attached to each truss unit with the truss shop assembled to camber shown. Truss units shall be in proper alignment and flange surfaces shall be shop bolted into full contact before welding. Sufficient external welds or tacks shall be made to secure flanges until remaining welds are made after disassembly. Adjacent flanges shall be "match marked" to insure proper field assembly.



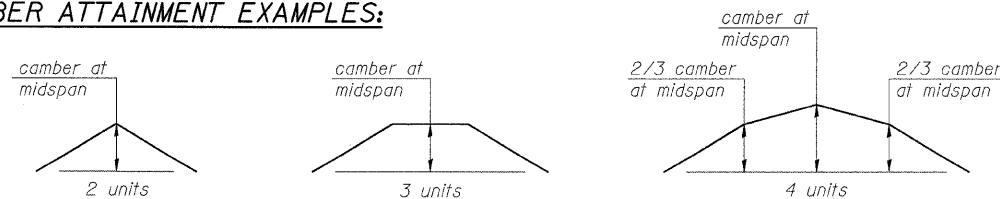
Note: Units shall be shipped individually with adequate provision to prevent detrimental motion during transport. This may require ropes between horizontals and diagonals or energy dissipating (elastic) ties to the vehicle. The Contractor is responsible for maintaining the configuration and protection of the units.



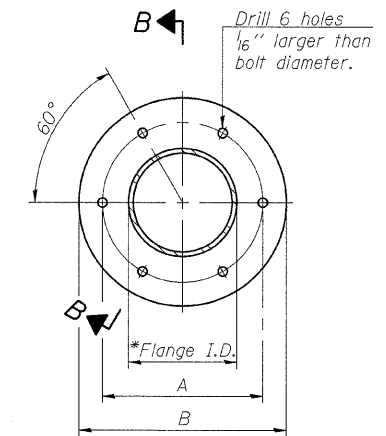
CAMBER DIAGRAM

Camber curve shown is theoretical. Actual camber attained by slope changes at splices between units.

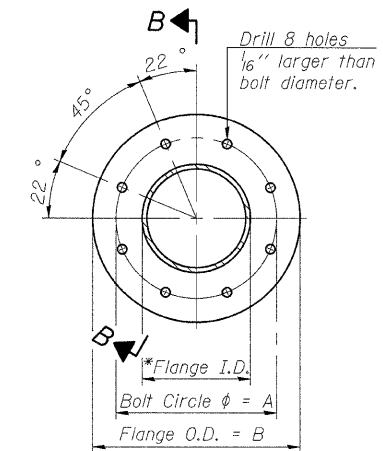
CAMBER ATTAINMENT EXAMPLES:



Camber shown is for fabrication only, measured with truss fully supported. (No-load condition)



TRUSS TYPES I-A, II-A, & III-A



TRUSS TYPES II-A & III-A

SPlicing FLANGES

ASTM B221, Alloy 6061-T6 or ASTM B209, Alloy 6061-T651

*To fit O.D. of Chord with maximum gap of 1/16".

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

OS4-A-2

12-1-08

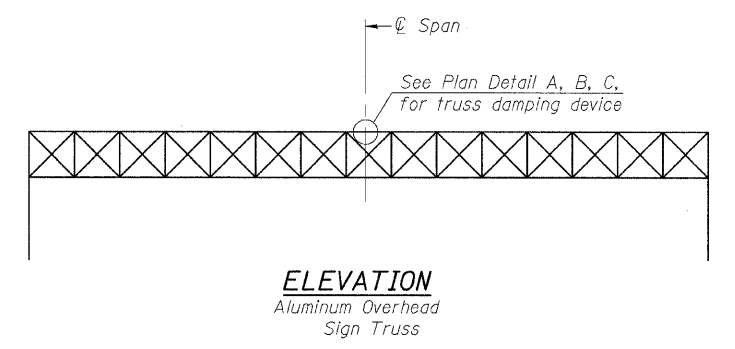
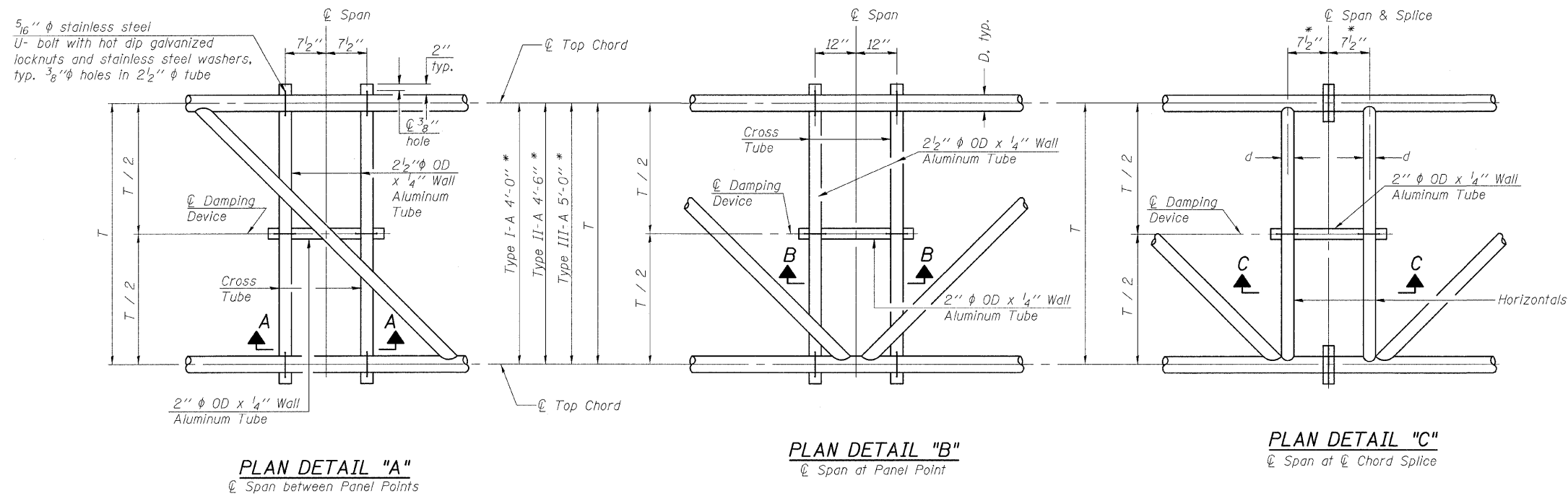
HORNER & SHIFRIN, INC.
ENGINEERS

SHEET NO. 3
14 SHEETS

| OVERHEAD SIGN STRUCTURES ALUMINUM TRUSS DETAILS FOR TRUSS TYPES I-A, II-A AND III-A | | | | |
|---|----------|---------|---------------------------|-----------|
| F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3 MADISON COUNTY | | | | |
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 124 |
| CONTRACT NO. 76D87 | | | ILLINOIS FED. AID PROJECT | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

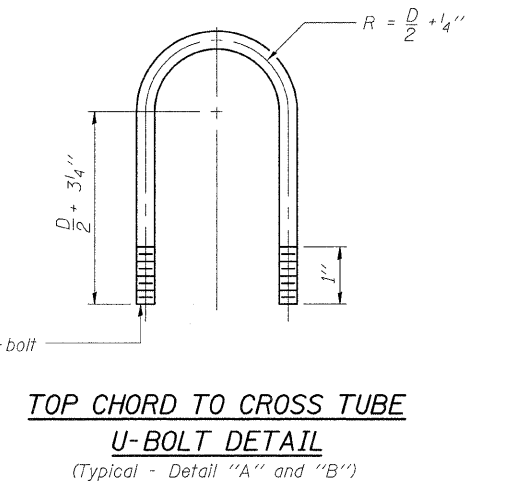
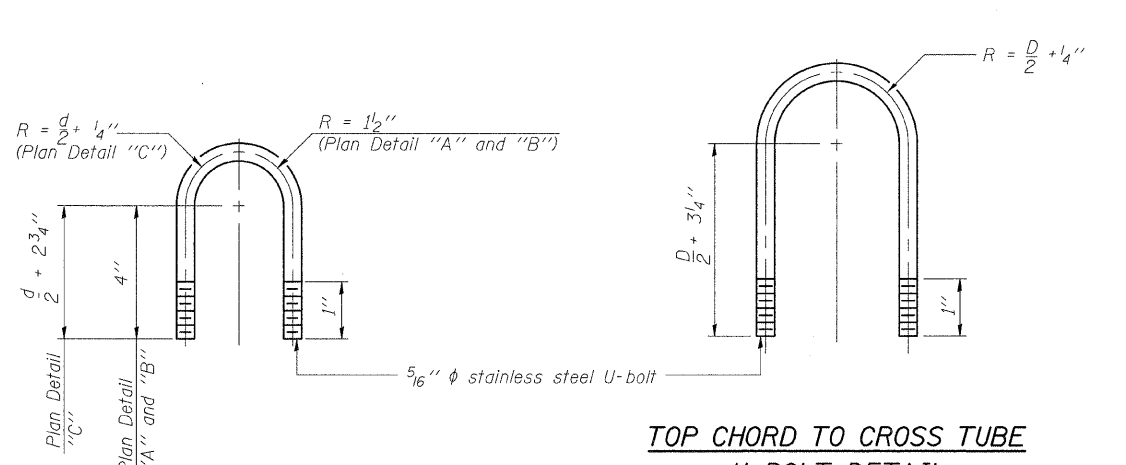
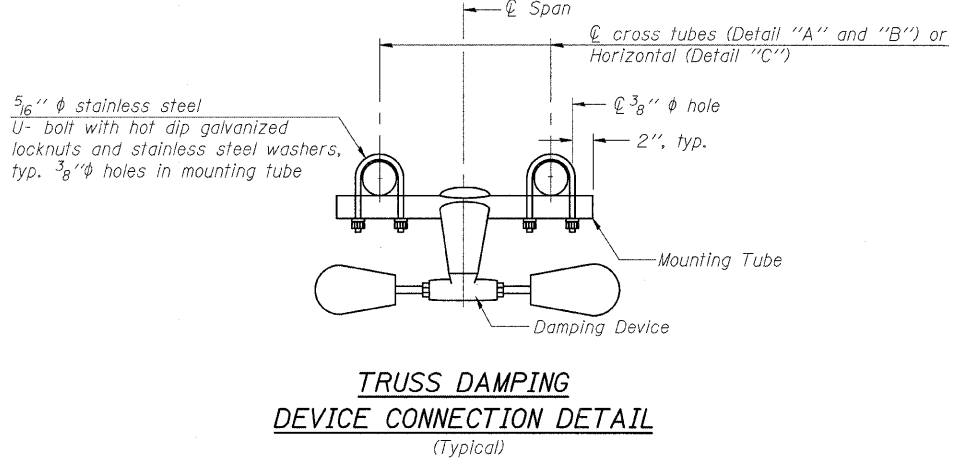
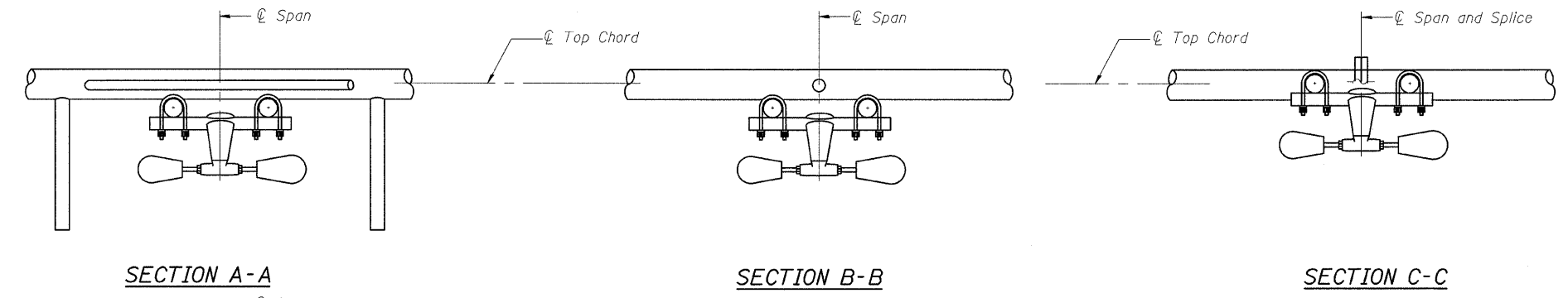
* Center of horizontal to center of splice dimension may vary. Verify before drilling holes in mounting tube.



NOTES

Damper: One damper per truss. (31 lbs. minimum Stockbridge-Type Aluminum - 29" minimum between ends of weights) Cost included in Overhead Sign Structure...

Materials: Materials: Aluminum tubes shall be ASTM B221 alloy 6061 temper T6. Cost included in Overhead Sign Structure...



| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

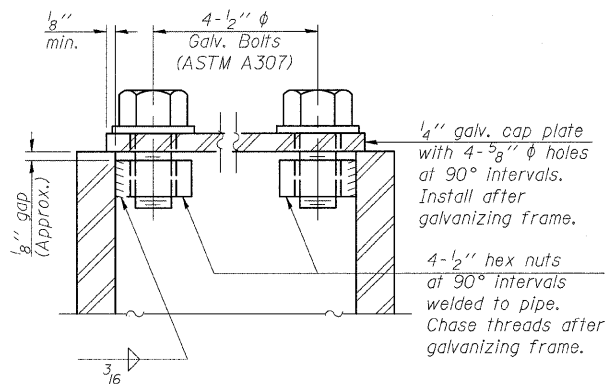
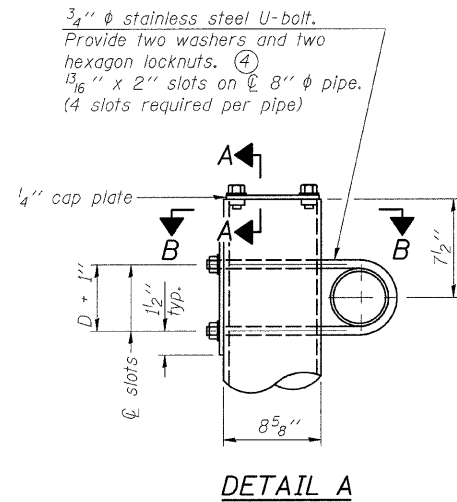
OS-A-D 12-1-08

HORNER & SHIFRIN, INC.
ENGINEERS

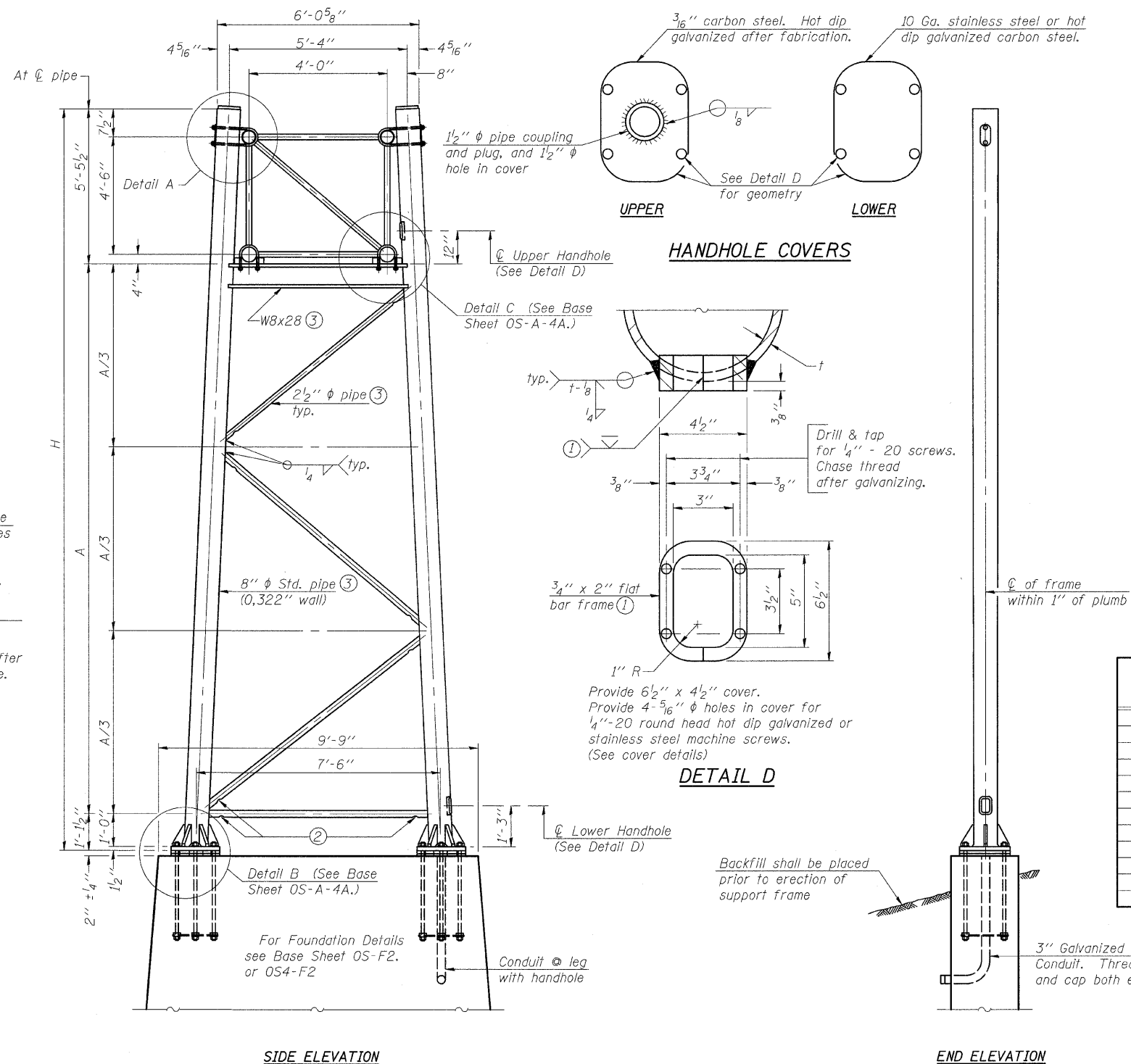
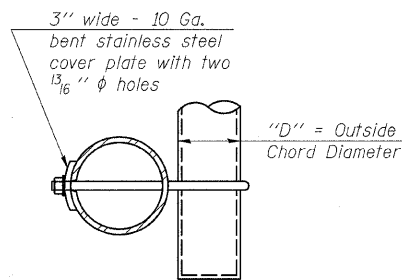
SHEET NO. 4
14 SHEETS

| OVERHEAD SIGN STRUCTURE DAMPING DEVICE | | | | |
|--|----------|---------|--------------|-----------|
| F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3 MADISON COUNTY | | | | |
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 125 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



As an alternate to bolts, may use galvanized drive-fit caps installed after galvanizing frame.



8" ϕ PIPE TRUSS SUPPORT FRAME

Support Design Loads: See Base Sheet OS-A-1 for design and loading criteria.
Load combinations checked include deadload plus:
a) 100% wind normal to sign, 20% parallel to sign
b) 60% wind normal to sign, 30% parallel to sign

- ① In lieu of fabricated handhole frame as shown, may cut from 2" plate (rolling direction vertical). All cut faces to be ground to ANSI Roughness of 500 min or less.
- ② Galvanizing vent holes of adequate size shall be provided on underside at each end of bracing pipes. Alternately, holes may be provided in wall of pipe column. All vent holes shall be drilled and de-burred, typ.
- ③ Steel pipe, plate, carbon steel handhole covers and rolled sections shall be hot dip galvanized after fabrication. Painting is not permitted. See Base Sheet OS-A-1.
- ④ See General Notes for fasteners.
- ⑤ Dimensions shown are based on selection criteria in the Sign Structures Manual. Nonstandard applications must have dimensions verified or amended as appropriate.
- ⑥ "H" based on 15'-0" or actual sign height, whichever is greater.

| Structure Number | Station | Support | | H (6) | A |
|------------------|------------|---------|-------|--------|---------|
| | | Left | Right | | |
| BS060S003R008.6 | 1370+32.00 | X | X | 26'-5" | 19'-10" |
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |

OVERHEAD SIGN STRUCTURES
SUPPORT FRAME FOR TYPE I-A ALUMINUM TRUSS

F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3
MADISON COUNTY

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |
| | | |

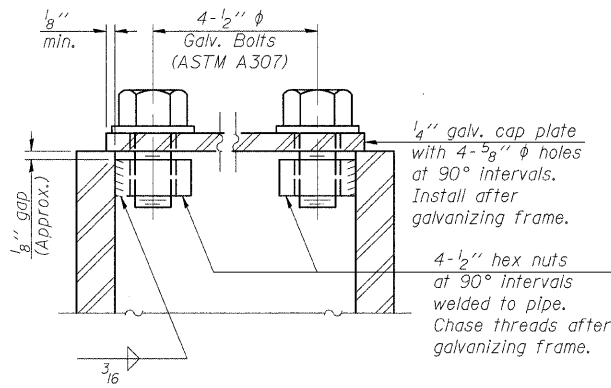
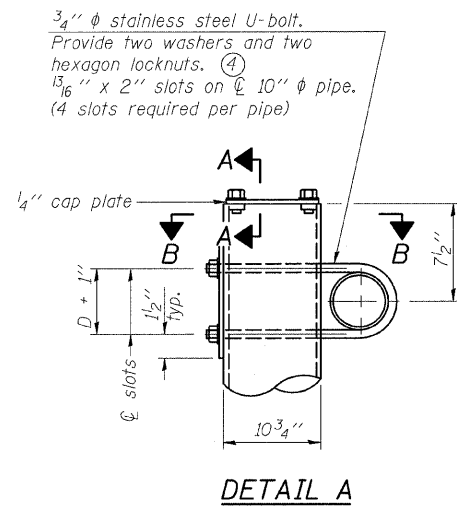
OS-A-4 12-1-08



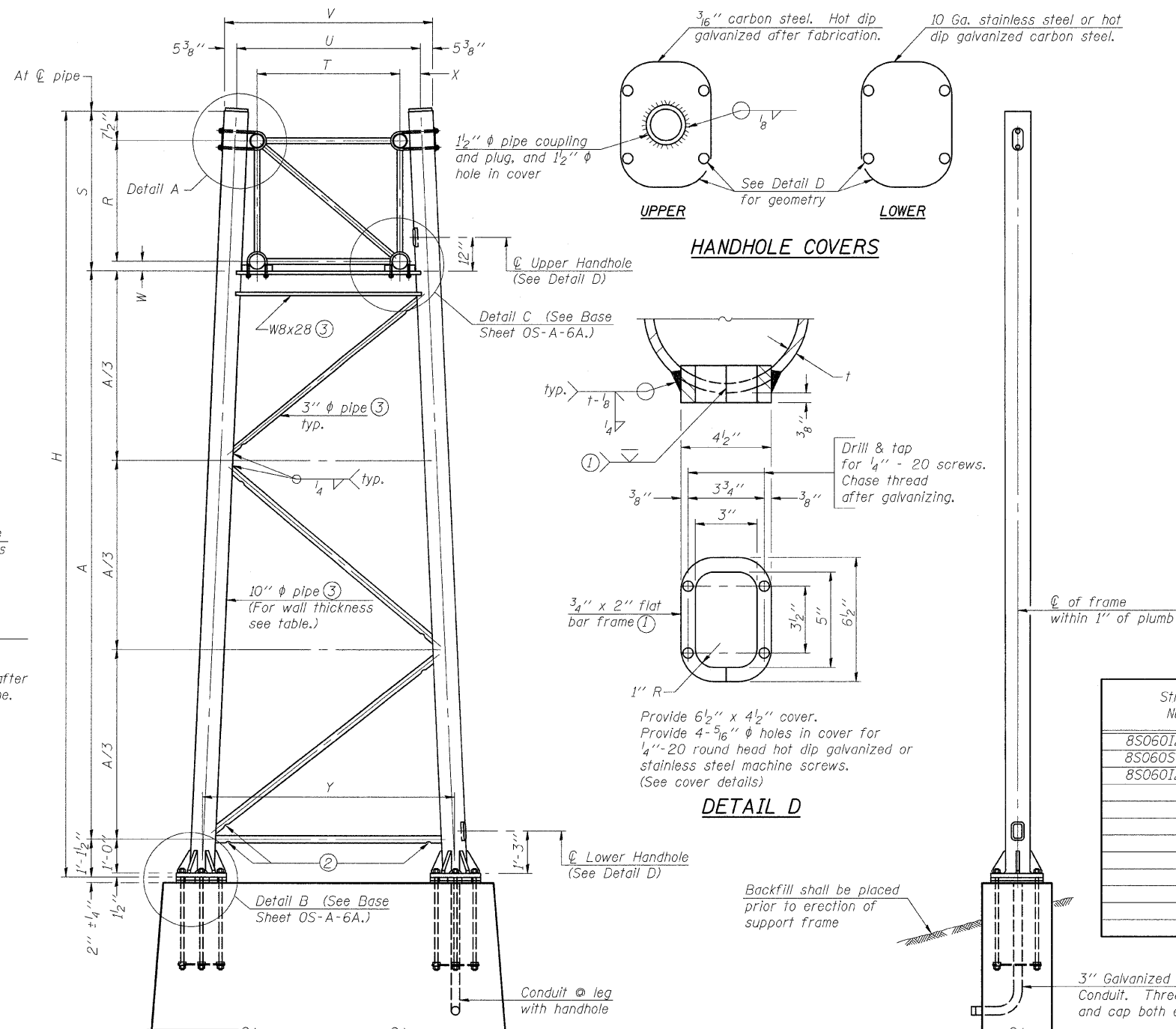
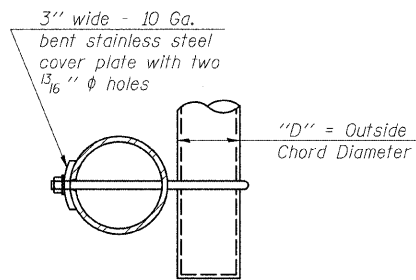
SHEET NO. 5
14 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 126 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



As an alternate to bolts, may use galvanized drive-fit caps installed after galvanizing frame.



For Foundation Details, see base sheet OS-F3 (Spread Footing) or OS4-F3 (Drilled Shaft).

SIDE ELEVATION

END ELEVATION

10" ϕ PIPE TRUSS SUPPORT FRAME

| Truss Type | Dimensions | | | | | | | |
|------------|------------|-----------|-------|-------|------------|--------|--------|-------|
| | R | S | T | U | V | W | X | Y |
| I-A | 4'-6" | 5'-5 1/2" | 4'-0" | 5'-6" | 6'-4 3/4" | 4" | 9" | 8'-3" |
| II-A (5) | 5'-3" | 6'-3 1/4" | 4'-6" | 6'-1" | 6'-11 3/4" | 4 3/4" | 9 1/2" | 8'-3" |

Support Design Loads: See Base Sheet OS-A-1 for design and loading criteria.
Load combinations checked include deadload plus:
a) 100% wind normal to sign, 20% parallel to sign
b) 60% wind normal to sign, 30% parallel to sign

- In lieu of fabricated handhole frame as shown, may cut from 2" plate (rolling direction vertical). All cut faces to be ground to ANSI Roughness of 500 min or less.
- Galvanizing vent holes of adequate size shall be provided on underside at each end of bracing pipes. Alternately, holes may be provided in wall of pipe column. All vent holes shall be drilled and de-burred, typ.
- Steel pipe, plate, carbon steel handhole covers and rolled sections shall be hot dip galvanized after fabrication. Painting is not permitted. See Base Sheet OS-A-1.
- See General Notes for fasteners.
- Dimensions shown are based on selection criteria in the Sign Structures Manual. Nonstandard applications must have dimensions verified or amended as appropriate.
- "H" based on 15'-0" or actual sign height, whichever is greater.

| Structure Number | Station | Support | | Truss Type | Pipe Wall Thickness | H (6) | A |
|------------------|-------------|---------|-------|------------|---------------------|---------|---------|
| | | Left | Right | | | | |
| 8S0601270L003.0 | 249+50.00 | X | X | I-A | 0.279 | 28'-8" | 22'-1" |
| 8S0605003L009.3 | 1347+50.00 | X | X | I-A | 0.279 | 29'-5" | 22'-10" |
| 8S0601270L002.8 | 31271+50.00 | X | X | I-A | 0.279 | 30'-10" | 24'-3" |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |
| | | | | | | | |

| |
|--------------|
| DESIGNED KLH |
| CHECKED JJD |
| DRAWN KLH |
| CHECKED JJD |

OS-A-6

12-1-08

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |

SHEET NO. 6
14 SHEETS

HORNER & SHIFRIN, INC.
ENGINEERS

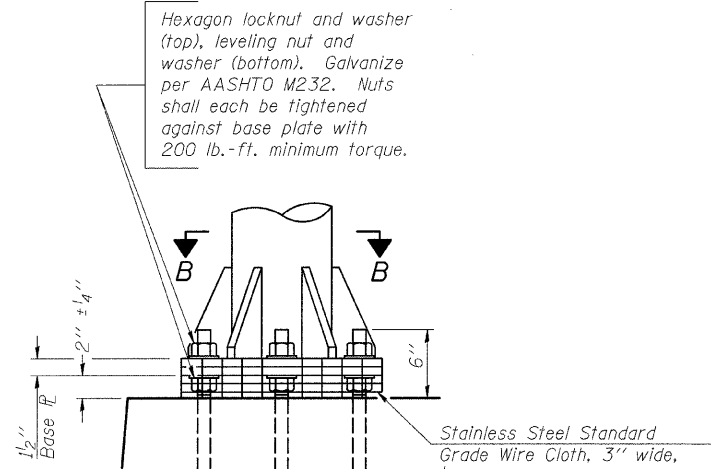
**OVERHEAD SIGN STRUCTURES
SUPPORT FRAME FOR ALUMINUM TRUSS**

F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3
MADISON COUNTY

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 127 |
| CONTRACT NO. 76D87 | | | | |

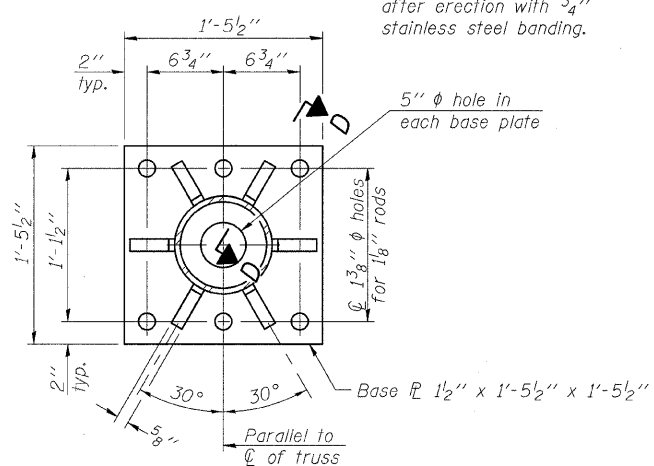
ILLINOIS FED. AID PROJECT

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

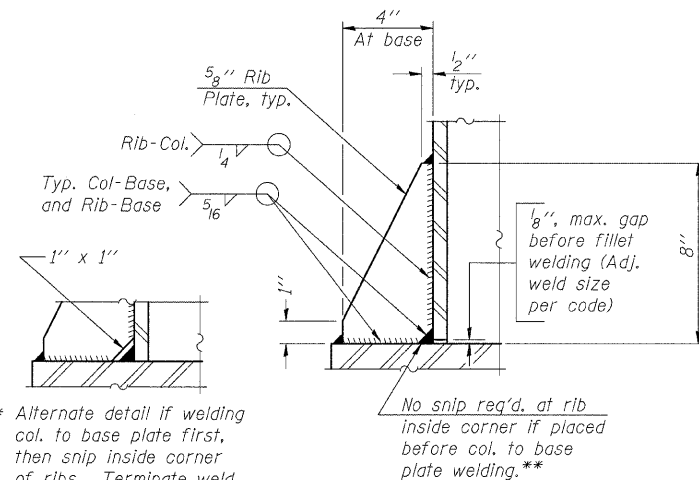


DETAIL B

Ribs shall be cut to fit slope of pipe.

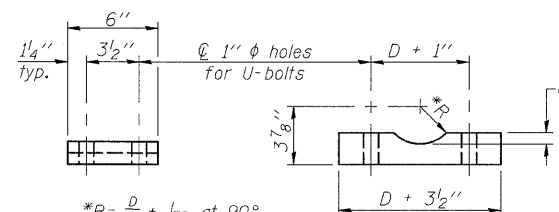


SECTION B-B



SECTION D-D

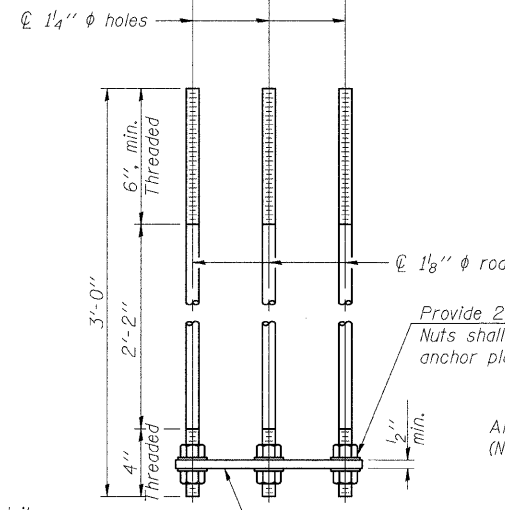
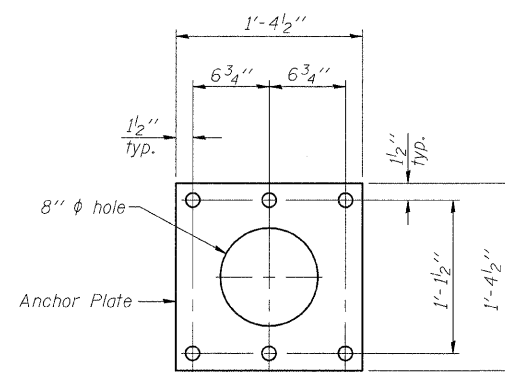
** Alternate detail if welding col. to base plate first, then snip inside corner of ribs. Terminate weld on rib 1/4" from snip.



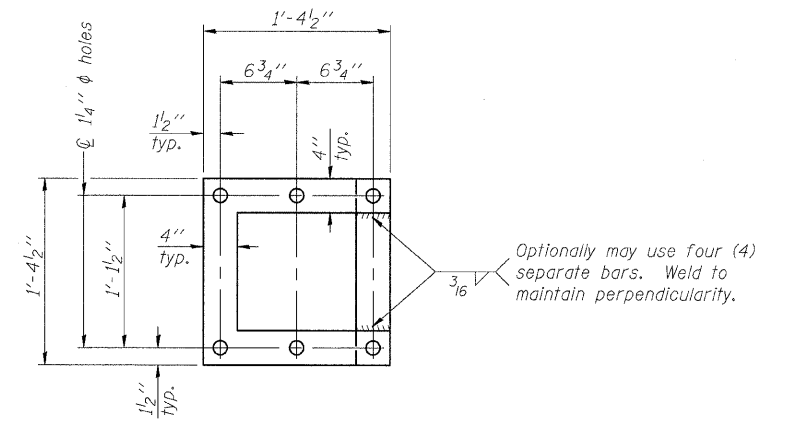
SADDLE SHIM DETAIL

ASTM B26 Alloy 356-F
or
ASTM B209 Alloy 6061-T651
(4 required per sign truss)

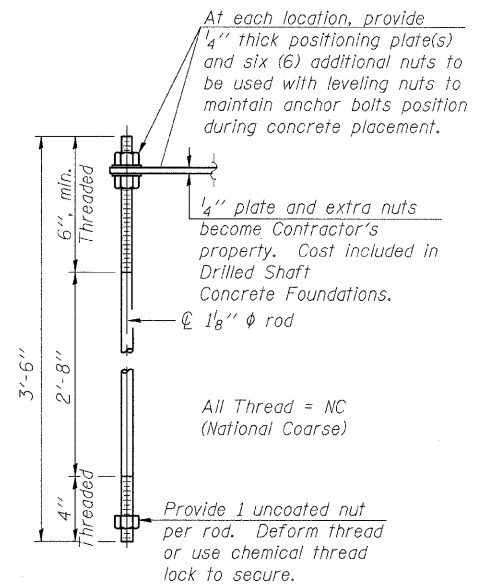
| Truss Chord Nominal Dia. | a |
|--------------------------|--------|
| 5" | 3/4" |
| 5 1/2" | 13/16" |
| 6" | 7/8" |
| 6 1/2" | 15/16" |



ANCHOR ROD DETAIL
Spread Footing Foundation



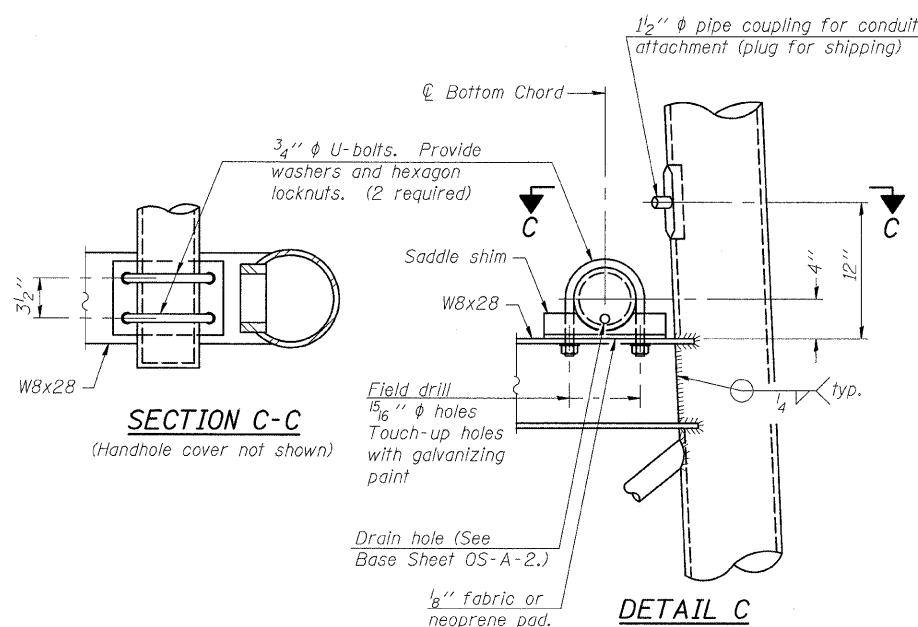
POSITIONING PLATE(S)



ANCHOR ROD DETAIL
Drilled Shaft Foundation

Anchor rods shall conform to AASHTO M314 Grade 36 or 55 and meet Charpy V-Notch (CVN) energy of 15 lb.-ft. at 40° F. Galvanize upper 12" per AASHTO M232. No welding shall be permitted on rods.

TYPE I-A TRUSS
8" ϕ PIPE SUPPORT FRAME DETAILS



SECTION C-C
(Handhole cover not shown)

DETAIL C

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |
| | | |

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

OS-A-4A

12-1-08

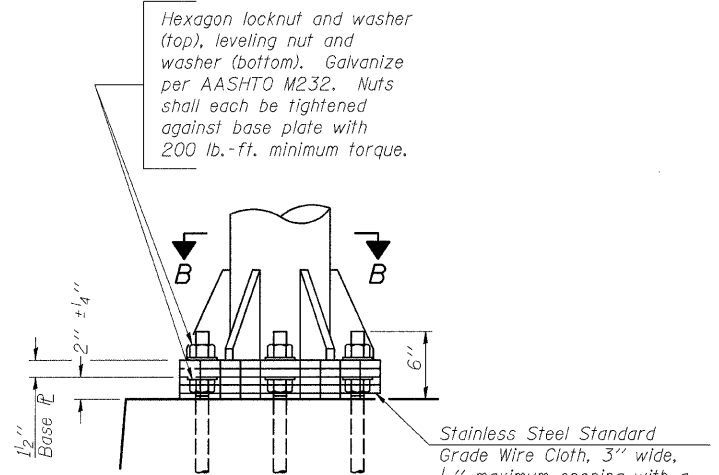
OVERHEAD SIGN STRUCTURES
SUPPORT FRAME DETAILS ALUMINUM TRUSS

F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3
MADISON COUNTY

| SHEET NO. 7 | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|----------|---------|--------------|-----------|
| 14 SHEETS | 270 | 60-2RS-3 | MADISON | 231 | 128 |
| CONTRACT NO. 76D87 | | | | | |
| ILLINOIS FED. AID PROJECT | | | | | |

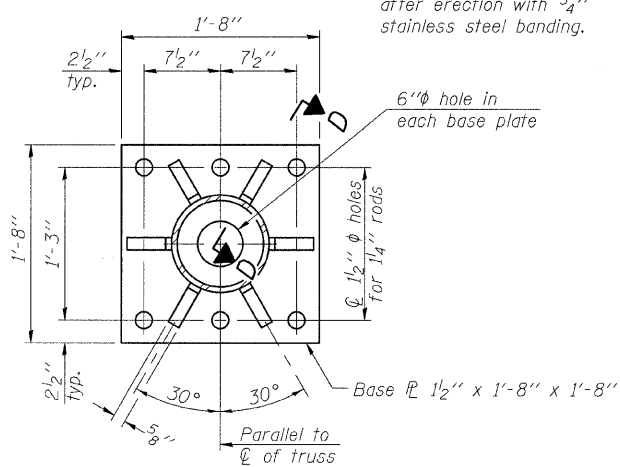
HORNER & SHIFRIN, INC.
ENGINEERS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

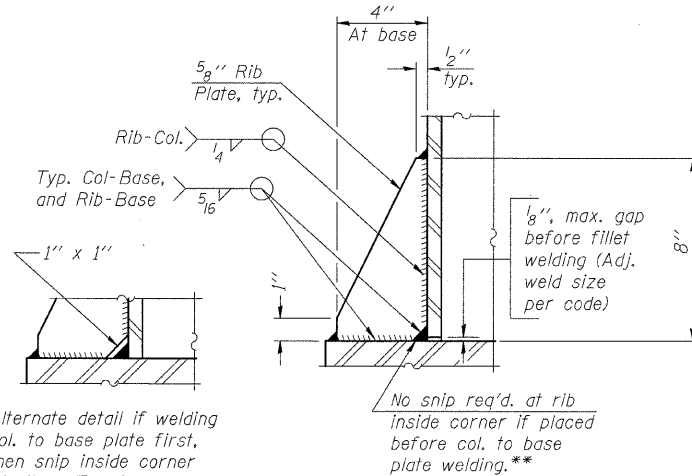


DETAIL B

Ribs shall be cut to fit slope of pipe.
Stainless Steel Standard Grade Wire Cloth, 3" wide, 1/4" maximum opening with a minimum wire diameter of AWG. No. 16 with a minimum 2" lap. Secure to base plate after erection with 3/4" stainless steel banding.

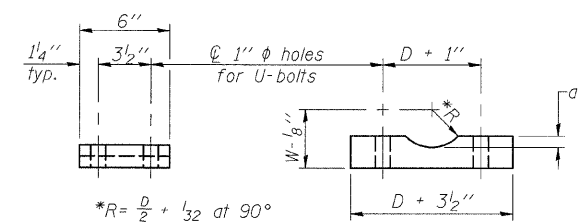


SECTION B-B



SECTION D-D

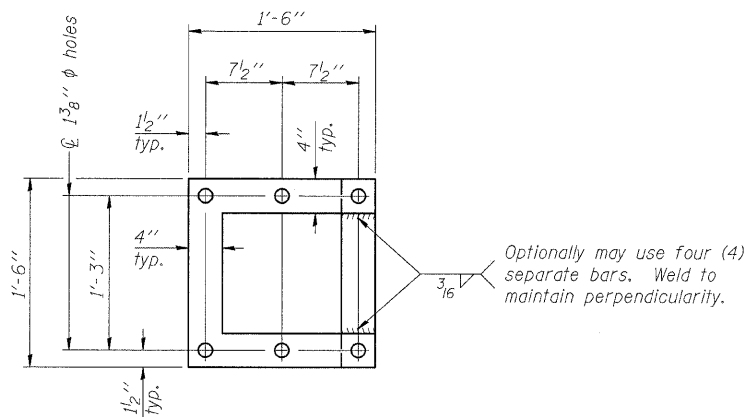
** Alternate detail if welding col. to base plate first, then snip inside corner of ribs. Terminate weld on rib 1/4" from snip.



SADDLE SHIM DETAIL

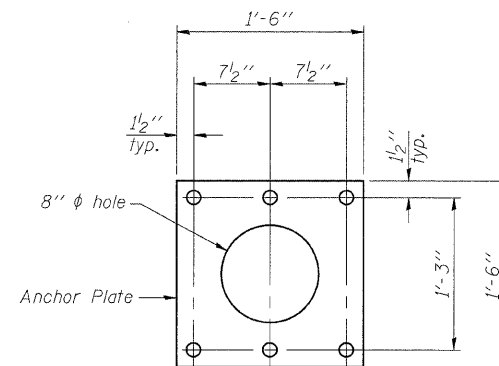
ASTM B26 Alloy 356-F
or
ASTM B209 Alloy 6061-T651
(4 required per sign truss)

| Truss Chord Nominal Dia. | a |
|--------------------------|--------|
| 5" | 3/4" |
| 5 1/2" | 13/16" |
| 6" | 7/8" |
| 6 1/2" | 15/16" |
| 7" | 1" |



POSITIONING PLATE(S)

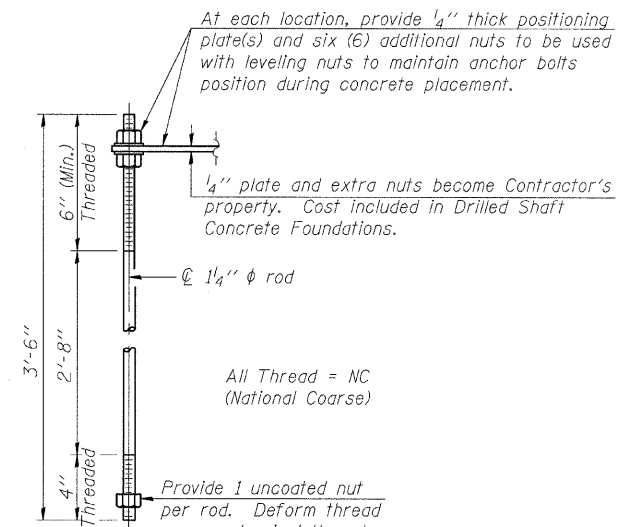
Optionally may use four (4) separate bars. Weld to maintain perpendicularity.



ANCHOR ROD DETAIL
Spread Footing Foundation

Provide 2 uncoated nuts per rod. Nuts shall be "snug tight" against anchor plate.

All Thread = NC (National Coarse)



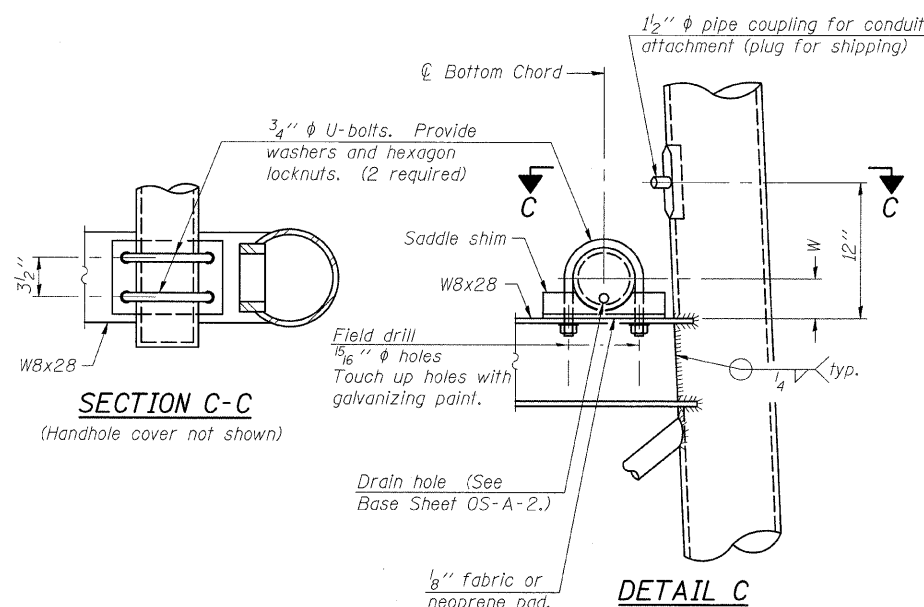
ANCHOR ROD DETAIL
Drilled Shaft Foundation

All Thread = NC (National Coarse)

Provide 1 uncoated nut per rod. Deform thread or use chemical thread lock to secure.

Anchor rods shall conform to AASHTO M314 Grade 36 or 50 and meet Charpy V-Notch (CVN) energy of 15 lb.-ft. at 40° F. Galvanize upper 12" per AASHTO M232. No welding shall be permitted on rods.

10" Ø PIPE SUPPORT FRAME DETAILS



SECTION C-C

DETAIL C

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |
| | | |

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

OS-A-6A 12-1-08

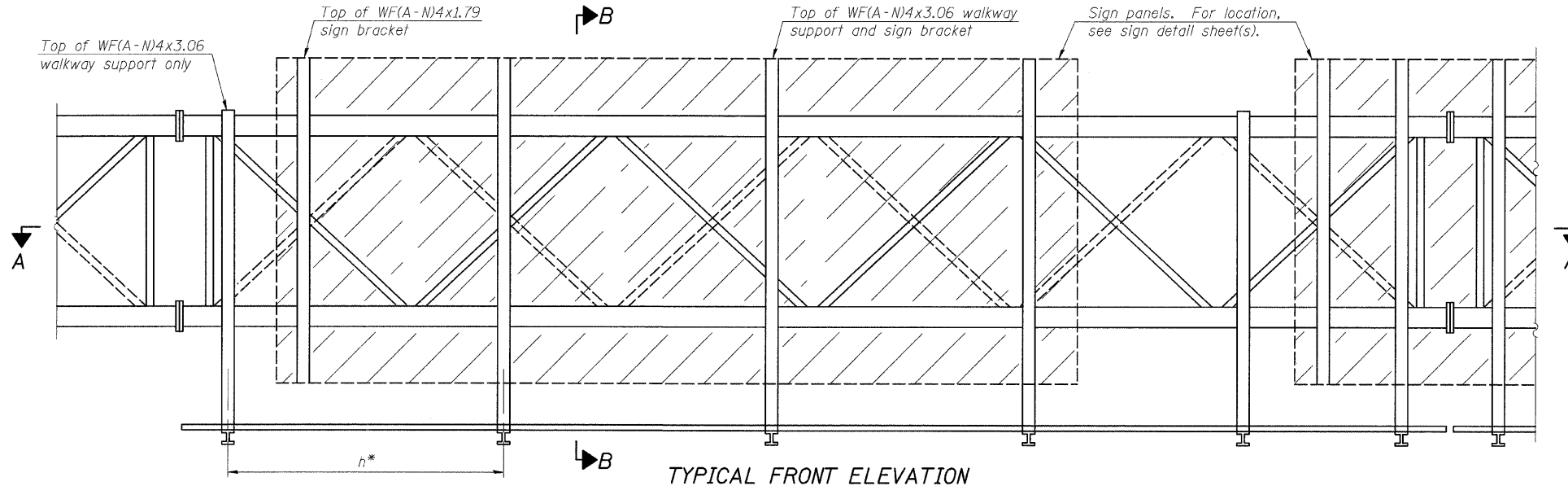
OVERHEAD SIGN STRUCTURES
SUPPORT FRAME DETAILS ALUMINUM TRUSS

F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3
MADISON COUNTY

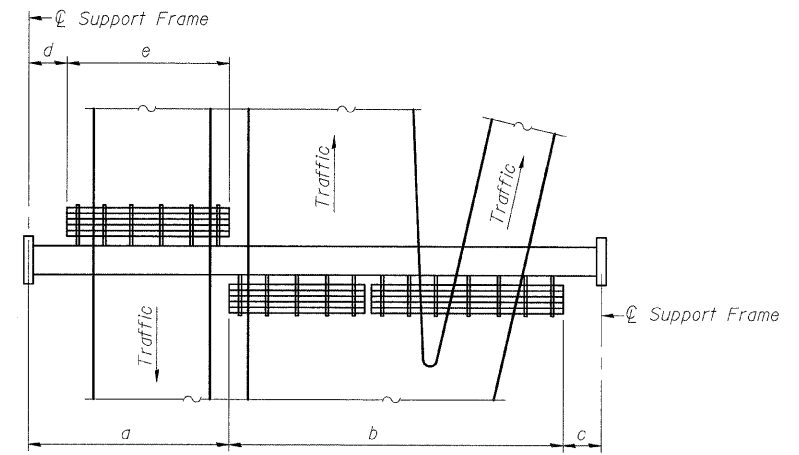
| | | | | | |
|-------------|--------------------|------------------|----------------|---------------------------|---------------|
| SHEET NO. 8 | F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 129 |
| 14 SHEETS | CONTRACT NO. 76D87 | | | ILLINOIS FED. AID PROJECT | |

HORNER & SHIFRIN, INC.
ENGINEERS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



TYPICAL FRONT ELEVATION
With lights and handrail omitted for clarity.
For Section B-B, see Base Sheet OS-A-10.



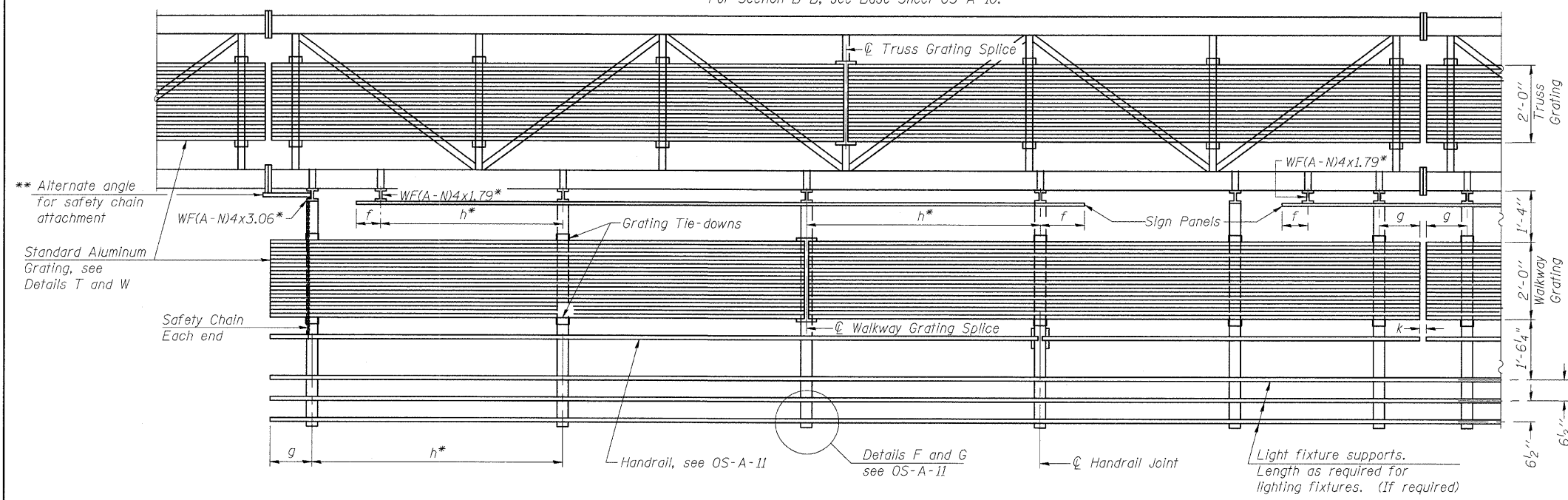
PLAN WALKWAY AND HANDRAIL SKETCH
(Road plan beneath truss varies)

BRACKET TABLE

| Sign Width | | Number Brackets Required |
|--------------|-----------------------|--------------------------|
| Greater Than | Less Than or Equal To | |
| | 8'-0" | 2 |
| 8'-0" | 14'-0" | 3 |
| 14'-0" | 20'-0" | 4 |
| 20'-0" | 26'-0" | 5 |
| 26'-0" | 32'-0" | 6 |

Notes:

- * Space walkway brackets WF(A-N)4x3.06 and sign brackets WF(A-N)4x1.79 for efficiency and within limits shown:
 $f = 12''$ maximum, $4''$ minimum (End of sign to ϕ of nearest bracket)
 $g = 12''$ maximum, $4''$ minimum (End of walkway grating to ϕ of nearest support bracket)
 $h = 6'-0''$ maximum (ϕ to ϕ sign and/or walkway support brackets, WF(A-N)4x1.79 or WF(A-N)4x3.06)
 $k = 2''$ maximum gap between adjacent walkway grating sections and handrail ends
 - ** If walkway bracket at safety chain location is behind sign, add angle to bracket, see Alternate Safety Chain Attachment on Base Sheet OS-A-11.
- For Details T and W, Section B-B and Grating Splice Details see Base Sheet OS-A-10.
For Handrail Details see Base Sheet OS-A-11.



SECTION A-A

Handrail and walkway shall span a minimum of three brackets between splices and/or gap joints. Place all sign and walkway brackets as close to panel points as practical. Handrail joints, grating, and light support splices placed as needed.

Truss grating to facilitate inspection shall run full length (center to center of support frames) $\pm 12''$ on overhead trusses. Cost of truss grating is included in "Overhead Sign Structure".

Walkway and Truss Grating width dimensions are nominal and may vary $\pm 1/2''$ based on available standard widths.

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |

| Structure Number | Station | a | b | c | d | e | Walkway Grating and Handrail Lengths |
|------------------|-------------|-----|-----|-----|---|---|--------------------------------------|
| 85060I270L003.0 | 249+50.00 | 18' | 41' | 10' | - | - | 41' |
| 85060S003L009.3 | 1347+50.00 | 17' | 46' | 23' | - | - | 46' |
| 85060I270L002.8 | 31271+50.00 | 18' | 40' | 13' | - | - | 40' |
| 85060S003R008.6 | 1370+32.00 | 12' | 38' | 10' | - | - | 38' |

DESIGNED KLH
CHECKED JJD
DRAWN KLH
CHECKED JJD

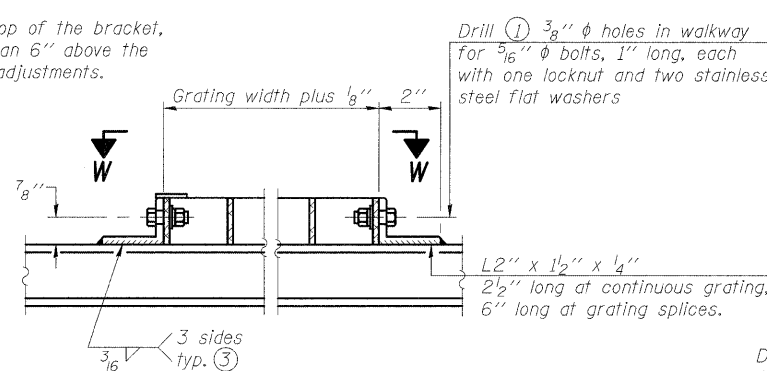
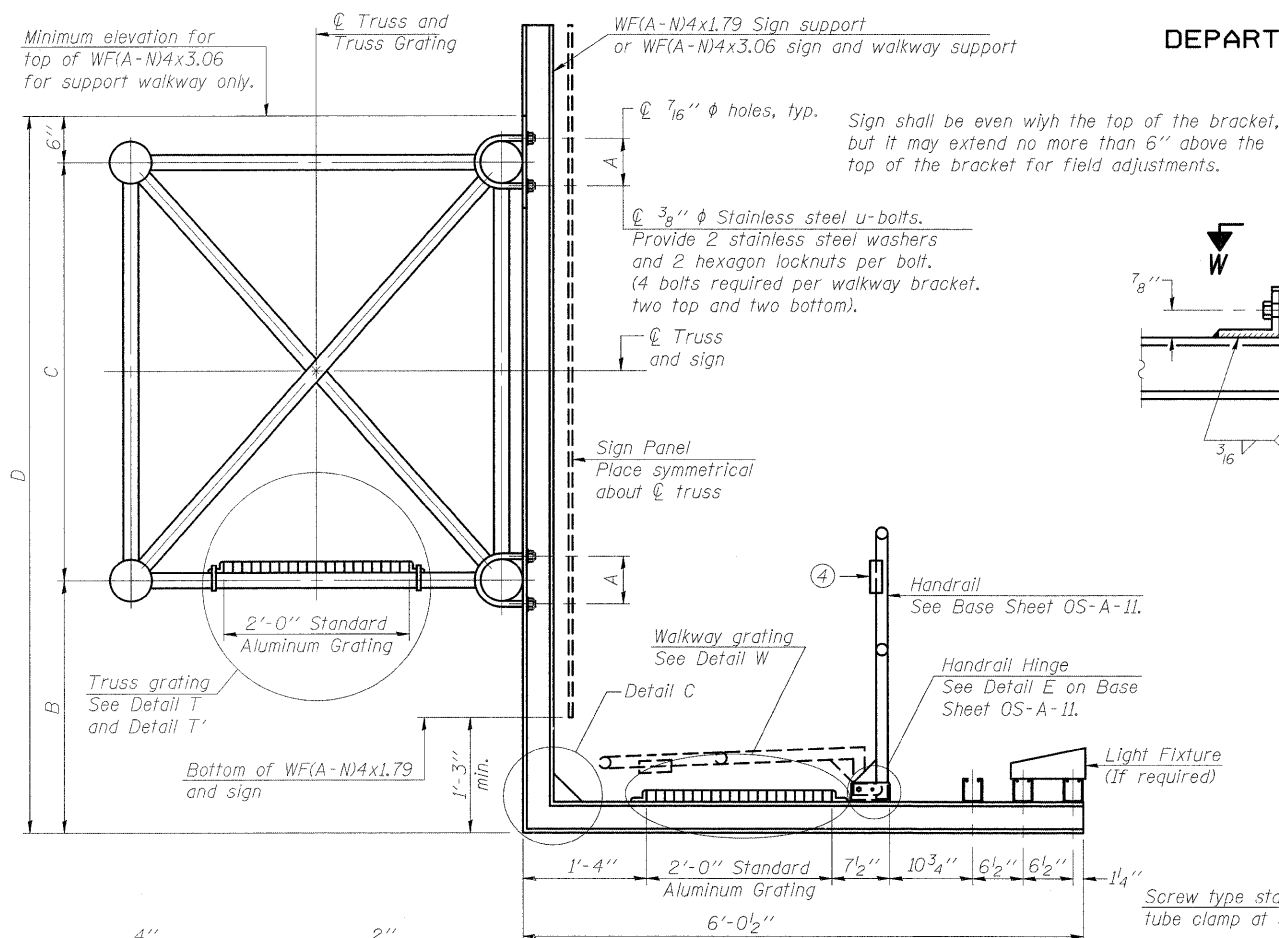
OS-A-9 12-1-08



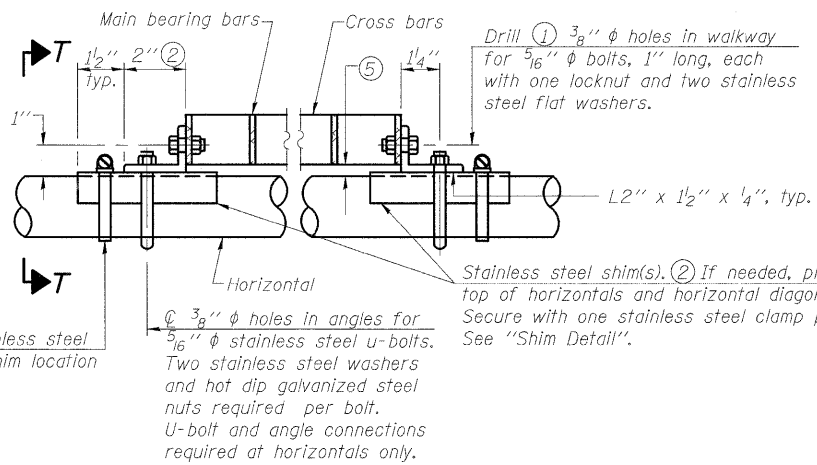
SHEET NO. 9
14 SHEETS

| OVERHEAD SIGN STRUCTURES ALUMINUM WALKWAY DETAILS | | | | |
|---|----------|---------|--------------|-----------|
| F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3 MADISON COUNTY | | | | |
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 130 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



DETAIL W
(Walkway grating)



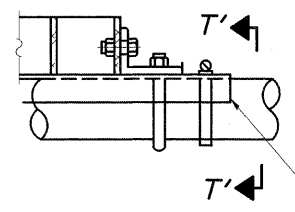
DETAIL T
(Continuous Truss grating)

SPECIFICATIONS FOR STANDARD ALUMINUM GRATING

Main Bearing Bars shall be 3/16" x 1 1/2" on 1 3/16" centers and conform to ASTM B221 Alloy 6061-T6.
Cross bars shall be 3/16" x 1 1/2" on 4" centers and conform to ASTM B221 Alloy 6063-T5 or 6061-T6.

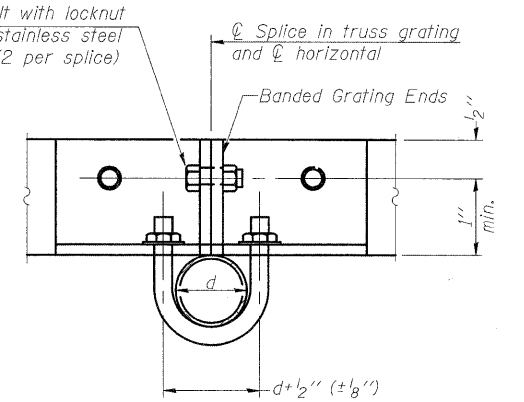
OR

Aluminum Grating with modified "T" sections for main bearing bars shall meet the following requirements:
Main bars shall conform to ASTM B221 Alloy 6061-T6 and have a minimum section modulus equal to 0.0705 in.³ per bar, a depth of 1 1/2", spaced on 1 3/16" centers.
Cross bars shall conform to ASTM B221 Alloy 6063-T5 or T-42 and spaced on 4" centers.

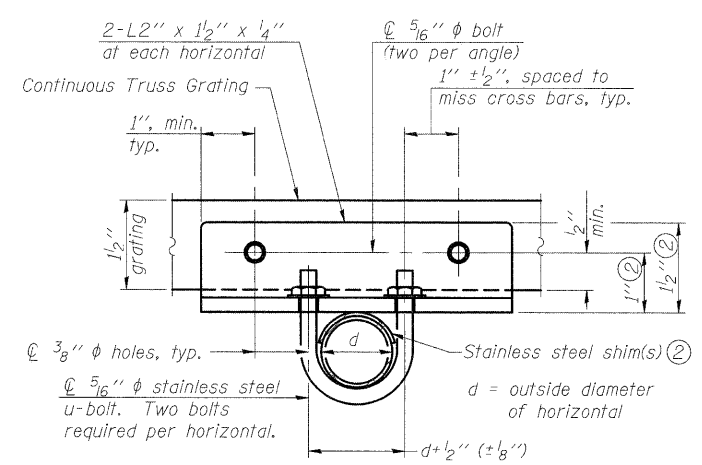


DETAIL T'

(Truss grating splice)
Details not shown same as Detail T. Alternate materials may be used subject to the Engineer's review and approval.

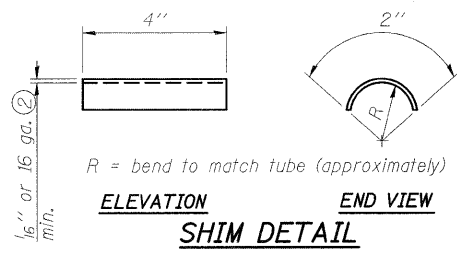


SECTION T'-T'

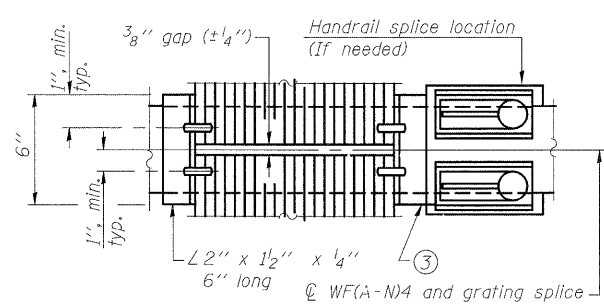


SECTION T-T

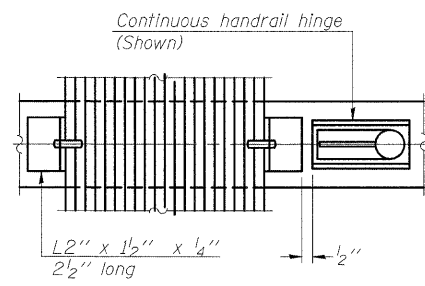
- ① Drilling holes in grating may be done in shop or field, based on Contractor's preference and subject to accurate alignment.
- ② Stainless steel shims shall be placed as shown in Detail T if needed to compensate for alignment variations between horizontal and diagonal pipes beyond adjustment provided by angles. Thicker shims may be used subject to shims performing properly.
- ③ If Handrail Joint present, weld angle to WF(A-N)4 and 1/4" extension bars. (See Base Sheet OS-A-11.)
- ④ 1/8" x 1/2" x 2" welded to handrail posts to protect locations that contact grating.
- ⑤ Tube to grating gap may vary from 0 to 1/2", max. to align walkway, allow for camber, etc.
- ⑥ Based on actual height of tallest sign given on OS-A-1.



SECTION B-B

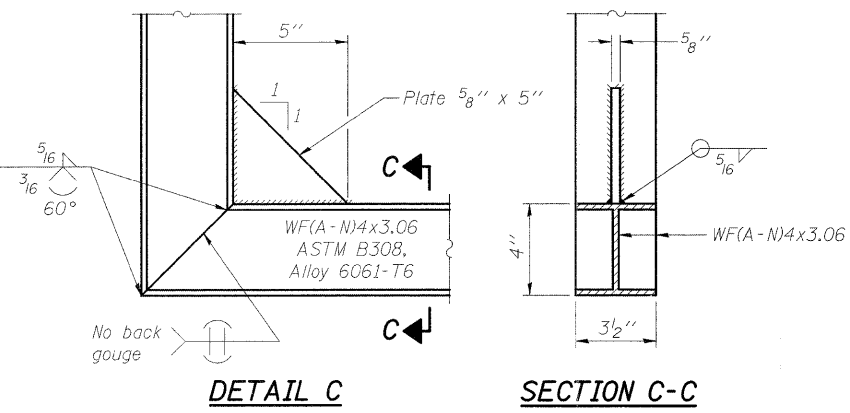


(AT WALKWAY GRATING SPLICE)



(CONTINUOUS WALKWAY GRATING)

SECTION C-C



DETAIL C

| NUMBER | REVISION | DATE |
|--------|--------------------|----------|
| 1 | Revised Dimensions | 5/3/2010 |

| Structure Number | Station | A | ⑥ B | C | ⑥ D |
|------------------|-------------|--------|-------|-------|-------|
| 8S060I270L003.0 | 249+50.00 | 5 1/2" | 4'-9" | 4'-6" | 9'-9" |
| 8S060S003L009.3 | 1347+50.00 | 5 1/2" | 3'-6" | 4'-6" | 8'-6" |
| 8S060I270L002.8 | 31271+50.00 | 5 1/2" | 2'-9" | 4'-6" | 7'-9" |
| 8S060S003R008.6 | 1370+32.00 | 5 1/2" | 3'-6" | 4'-6" | 8'-6" |

DESIGNED KLH
CHECKED JJD
DRAWN KLH
CHECKED JJD



OS-A-10 6-1-09

**OVERHEAD SIGN STRUCTURES
ALUMINUM WALKWAY DETAILS**

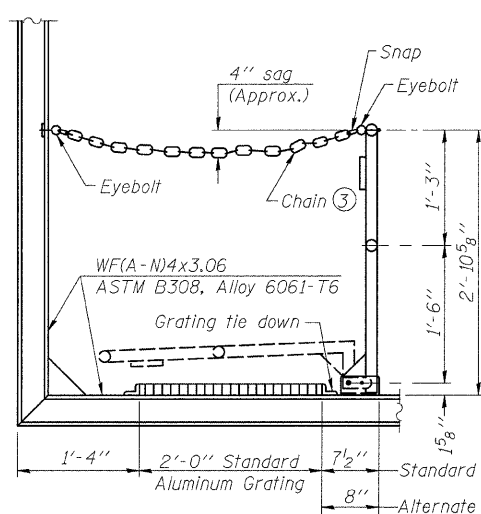
F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3
MADISON COUNTY

| | | | | | |
|--------------|-----------------|------------------|----------------|------------------|---------------|
| SHEET NO. 10 | F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 131 |
|--------------|-----------------|------------------|----------------|------------------|---------------|

CONTRACT NO. 76D87

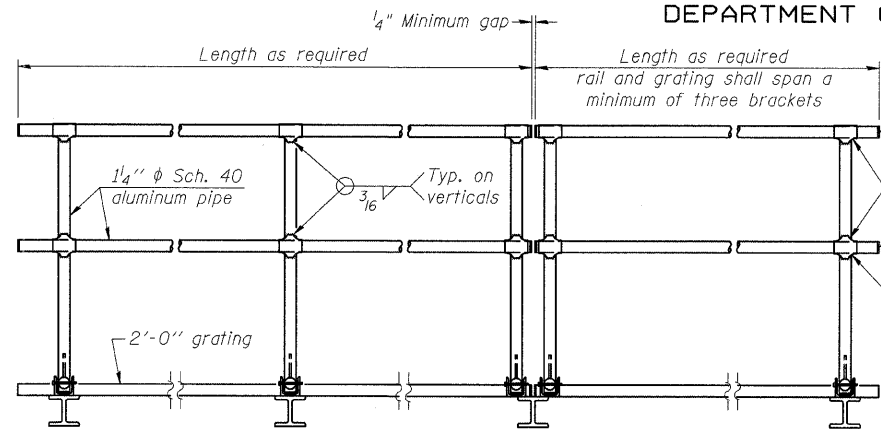
ILLINOIS FED. AID PROJECT

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



SIDE ELEVATION

(Showing safety chain w/o sign)



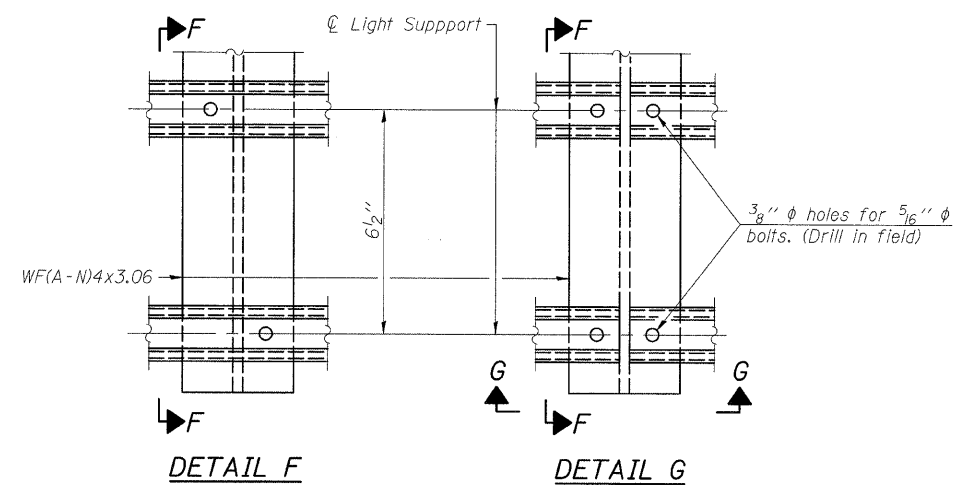
FRONT ELEVATION

HANDRAIL DETAILS

Handrail pipe shall be ASTM B241 or B429, Alloy 6063-T6 or Alloy 6061-T6.

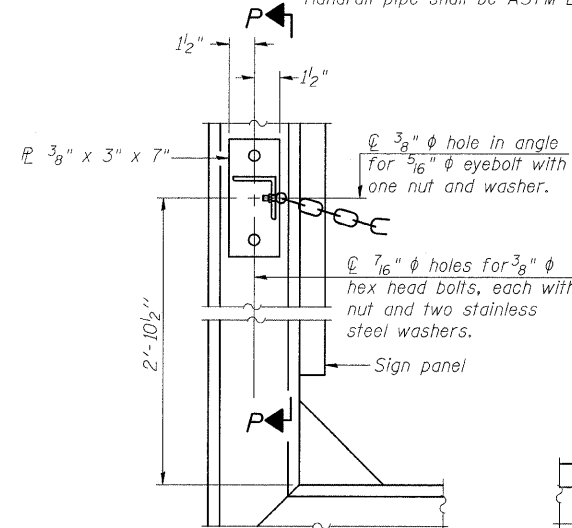
- ① Install standard force-fit end caps or weld 1/8" end plates with 1/8" c.f.w. and grind smooth. (All rail ends)
- ② Horizontal handrail member shall be continuous thru fitting. Provide 1/16" hole in fitting for 3/8" hole in horizontal rail member. Provide locknut and two stainless steel washers for bolt. (Use 5/16" eyebolts in 7/16" holes on top rail at ends only.)

Fittings-ASTM B26, Alloy 356-T7 or 1/2" aluminum pipe



DETAIL F

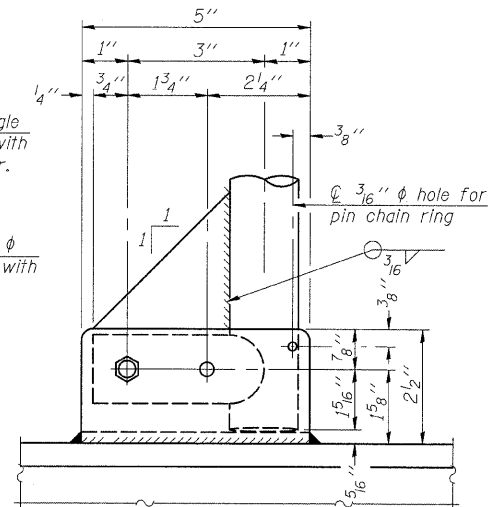
DETAIL G



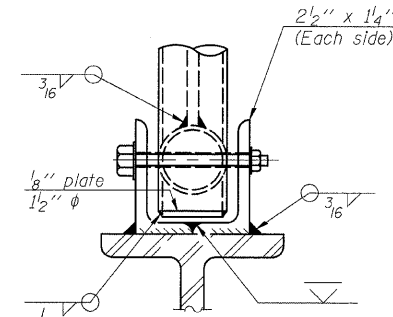
ALTERNATE SAFETY CHAIN ATTACHMENT

(With Sign Present)

Items not shown same as "Side Elevation" of "Handrail Details"

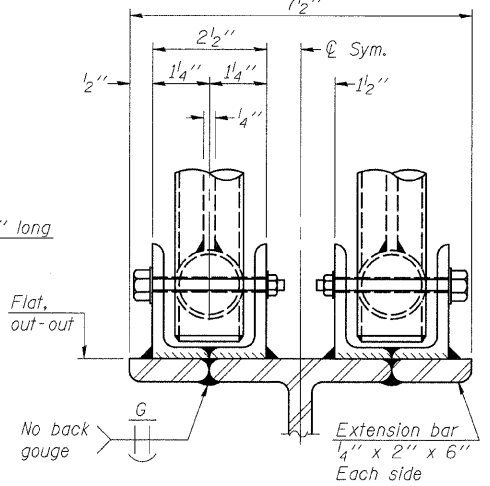


SIDE ELEVATION

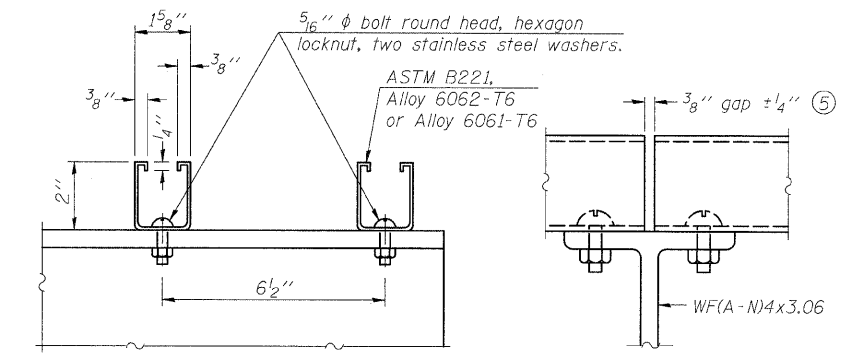


FRONT ELEVATION

See "Elevation" at right for dimensions.



ELEVATION AT HANDRAIL JOINT

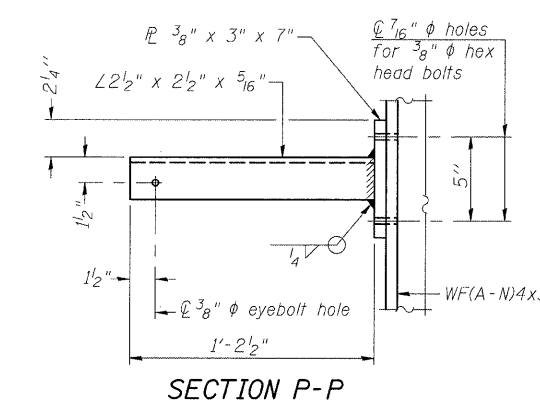


SECTION F-F

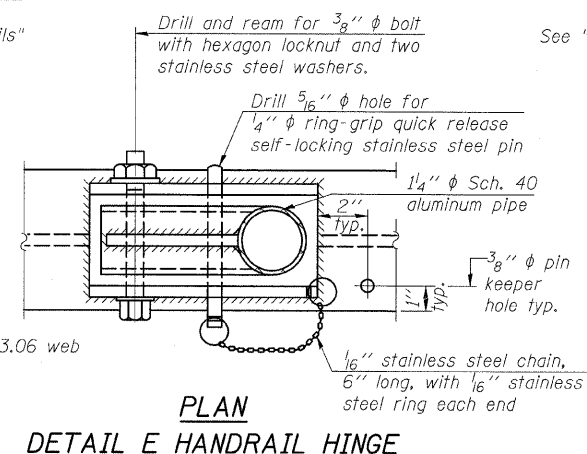
SECTION G-G

LIGHTING FIXTURE MOUNTS (IF REQUIRED)

⑤ Field cut ends of light support channels shall be free of burrs or hazardous projections and coated with zinc-rich primer or equivalent.

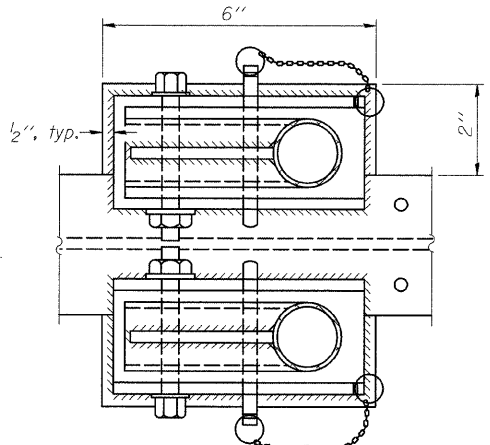


SECTION P-P



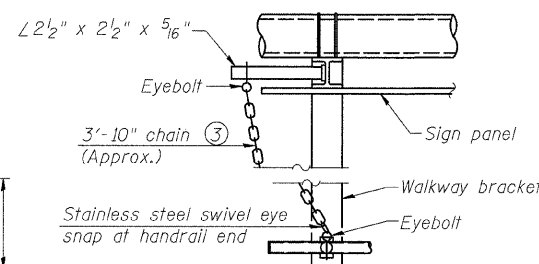
PLAN

DETAIL E HANDRAIL HINGE



PLAN AT HANDRAIL JOINT

Details not shown same as "PLAN"

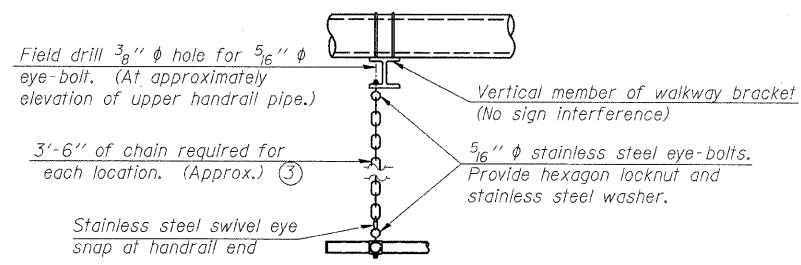


ALTERNATE SAFETY CHAIN ATTACHMENT

Details not shown similar to "Safety Chain" Details (Walkway omitted for clarity)

③ 3/16" Type 304L stainless steel chain, approximately 12 links per foot.

④ Extrusions may be used in lieu of the details shown, with approval of the Engineer.



SAFETY CHAIN

One required for each end of each walkway.

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

OS-A-11 12-1-08

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |



SHEET NO. 11
14 SHEETS

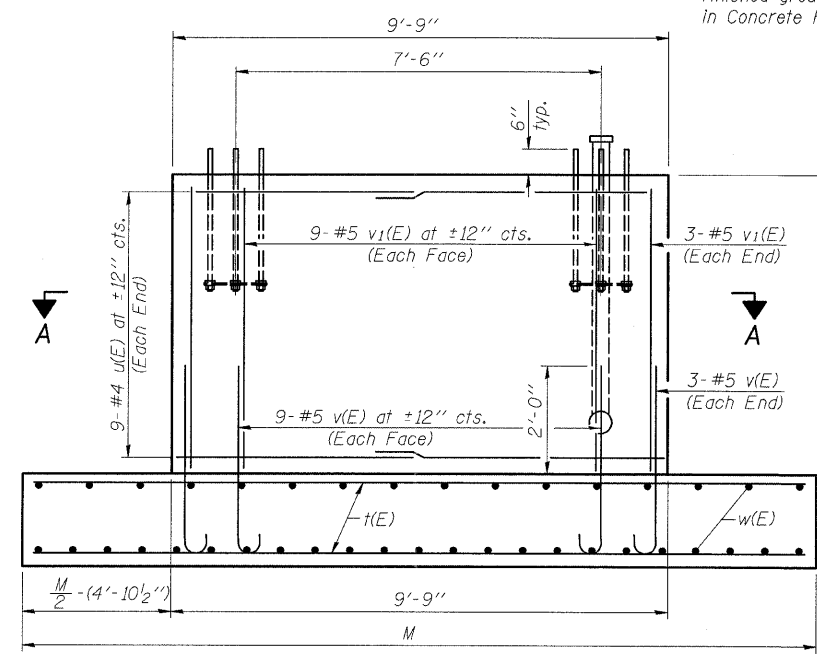
| OVERHEAD SIGN STRUCTURES ALUMINUM HANDRAIL DETAILS | | | | |
|---|----------|---------|---------------------------|-----------|
| F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3 MADISON COUNTY | | | | |
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 132 |
| CONTRACT NO. 76D87 | | | ILLINOIS FED. AID PROJECT | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

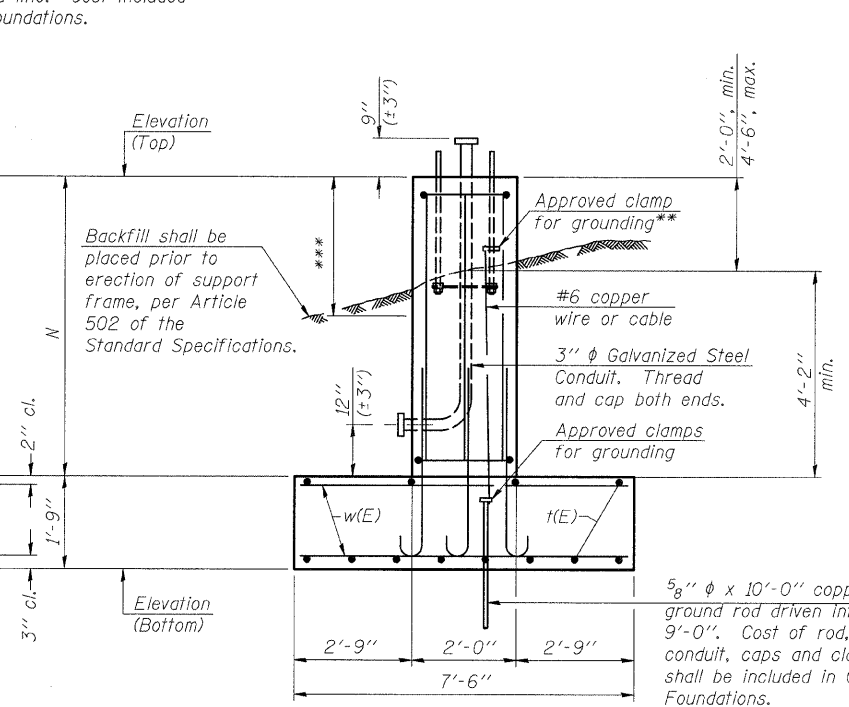
For anchor rod size and placement, see Support Frame Detail Sheet.

** Anchor rod shall be ground or filed to bright metal at clamp and cable connection location.

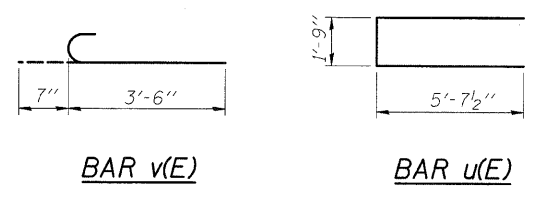
*** A normal surface finish followed by a Bridge Seat Sealer application will be required on concrete surfaces above the lowest elevation 6" below finished ground line. Cost included in Concrete Foundations.



SIDE ELEVATION



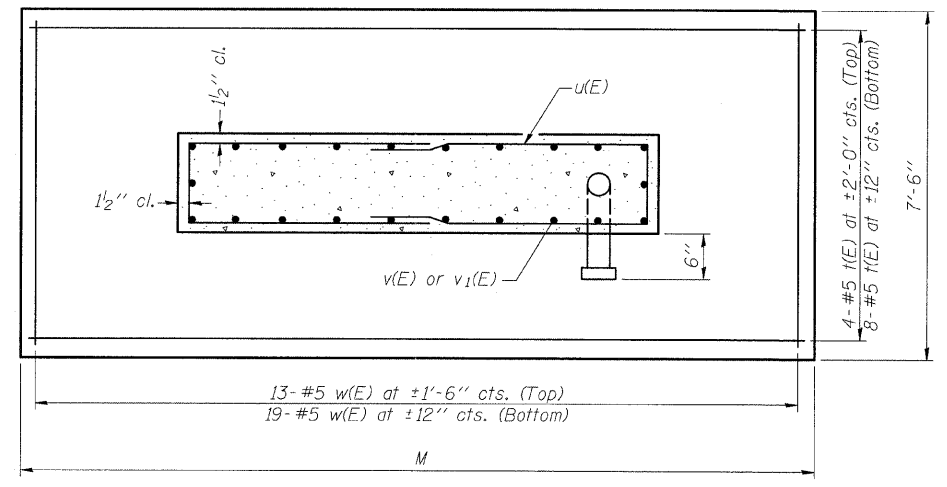
END ELEVATION



BAR LIST - EACH FOUNDATION

| Bar | Number | Size | Length | Shape |
|-------|--------|------|--------|-------|
| t(E) | 12 | #5 | * | — |
| u(E) | 18 | #4 | 13'-0" | U |
| v(E) | 24 | #5 | 4'-1" | C |
| v1(E) | 24 | #5 | * | — |
| w(E) | 32 | #5 | 7'-3" | — |

*Length of t(E) bar = (Dim. M) - 6"
v1(E) bar = (Dim. N) - 3"



SECTION A-A

| Structure Number | Station | Left Foundation | | | | Right Foundation | | | | Class SI Concrete (Cu. Yds.) |
|------------------|------------|-----------------|------------------|-------|--------|------------------|------------------|-------|--------|------------------------------|
| | | Elevation Top | Elevation Bottom | N | M | Elevation Top | Elevation Bottom | N | M | |
| 8S060S003R008.6 | 1370+32.00 | 419.03 | 409.82 | 7'-5" | 21'-6" | 419.03 | 408.61 | 8'-8" | 21'-6" | 32.7 |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |

Note: The foundation dimensions shown are based on the presence of mostly cohesive soils with an average Unconfined Compressive Strength (Qu) of at least 1.0 tsf, which must be determined by previous soil investigations at the jobsite. When other conditions are indicated, the boring data will be included in the plans and the foundation dimensions shown will be the result of site specific designs. During construction, if footing length or width or wall height change by more than 12", or if reinforcement is changed, "as-built" plans shall be prepared and submitted to the District Bureau of Operations for future reference.

DETAILS FOR 8" φ SUPPORT FRAME

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |
| | | |

OS-F2 12-1-08



SHEET NO. 12
14 SHEETS

**OVERHEAD SIGN STRUCTURES
SPREAD FOOTING DETAILS**

F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3
MADISON COUNTY

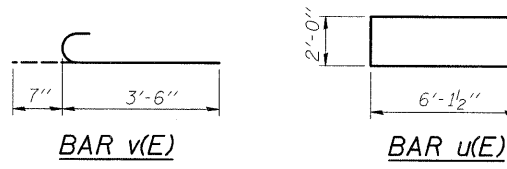
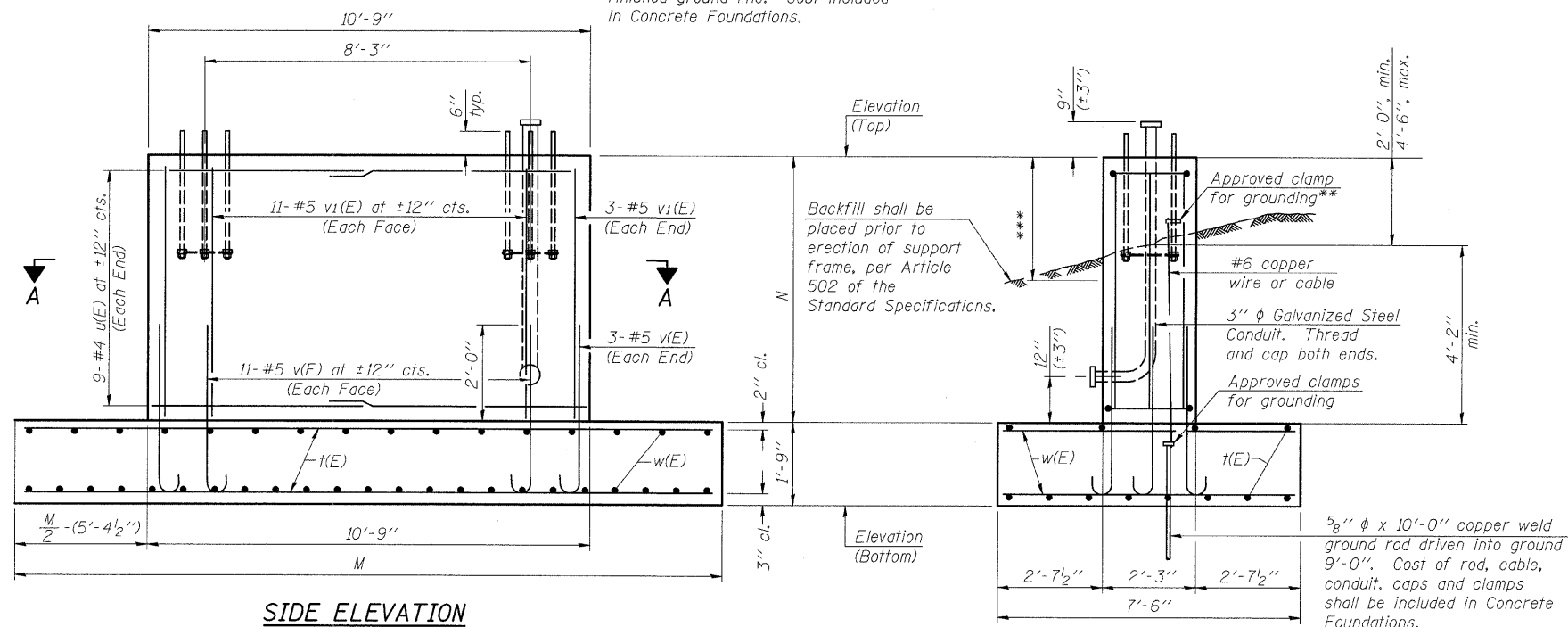
| | | | | |
|---------------------------|----------|---------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 133 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

For anchor rod size and placement, see Support Frame Detail Sheet.

** Anchor rod shall be ground or filed to bright metal at clamp and cable connection location.

*** A normal surface finish followed by a Bridge Seat Sealer application will be required on concrete surfaces above the lowest elevation 6" below finished ground line. Cost included in Concrete Foundations.



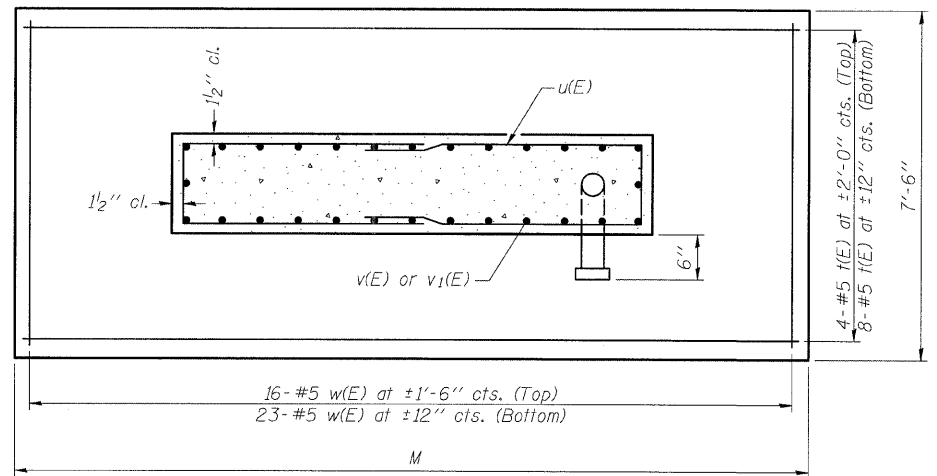
BAR LIST - EACH FOUNDATION

| Bar | Number | Size | Length | Shape |
|-------|--------|------|--------|-------|
| t(E) | 12 | #5 | * | — |
| u(E) | 18 | #4 | 14'-3" | — |
| v(E) | 28 | #5 | 4'-1" | U |
| v1(E) | 28 | #5 | * | — |
| w(E) | 39 | #5 | 7'-3" | — |

*Length of t(E) bar = (Dim. M) - 6"
v1(E) bar = (Dim. N) - 3"

SIDE ELEVATION

END ELEVATION



SECTION A-A

| Structure Number | Station | Left Foundation | | | | Right Foundation | | | | Class SI Concrete (Cu. Yds.) |
|------------------|-------------|-----------------|------------------|-------|--------|------------------|------------------|-----------|--------|------------------------------|
| | | Elevation Top | Elevation Bottom | N | M | Elevation Top | Elevation Bottom | N | M | |
| 8S060S003L009.3 | 1347+50.00 | 420.69 | 411.27 | 7'-8" | 21'-6" | 420.69 | 412.48 | 6'-5 1/2" | 21'-6" | 33.7 |
| 8S060I270L002.8 | 31271+50.00 | 423.50 | 415.33 | 6'-5" | 21'-6" | 423.50 | 414.17 | 7'-7" | 21'-6" | 33.6 |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |

Note:
The foundation dimensions shown are based on the presence of mostly cohesive soils with an average Unconfined Compressive Strength (Qu) of at least 1.0 tsf, which must be determined by previous soil investigations at the jobsite. When other conditions are indicated, the boring data will be included in the plans and the foundation dimensions shown will be the result of site specific designs.
During construction, if footing length or width or wall height change by more than 12", or if reinforcement is changed, "as-built" plans shall be prepared and submitted to the District Bureau of Operations for future reference.

DETAILS FOR 10" Ø SUPPORT FRAME

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |
| | | |
| | | |

OS-F3 12-1-08



SHEET NO. 13
14 SHEETS

**OVERHEAD SIGN STRUCTURES
SPREAD FOOTING DETAILS**

**F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3
MADISON COUNTY**

| | | | | |
|---------------------------|------------------|----------------|------------------|---------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 134 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

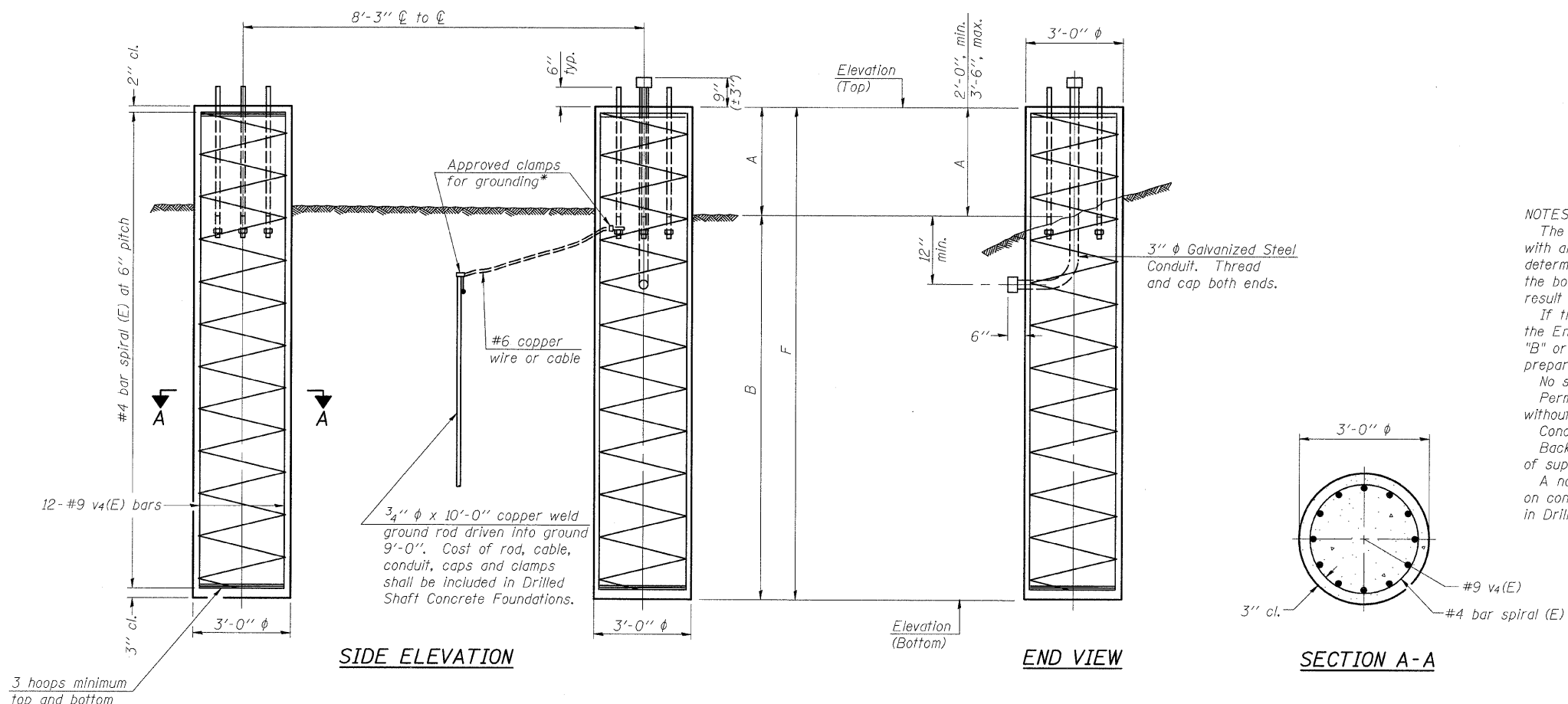
For anchor rod size and placement, see Support Frame Detail Sheet.

* Anchor rod shall be ground or filed to bright metal at clamp and cable connection location.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAR LIST - EACH FOUNDATION

| Bar | Number | Size | Length | Shape |
|--|--------|------|-----------|-------|
| v4(E) | 24 | #9 | F less 5" | — |
| #4 bar spiral (E) - see Side Elevation | | | | |



NOTES:

The foundation dimensions shown are based on the presence of mostly cohesive soils with an average Unconfined Compressive Strength (Qu) of at least 1.25 tsf, which must be determined by previous soil investigations at the jobsite. When other conditions are indicated, the boring data will be included in the plans and the foundation dimensions shown will be the result of site specific designs.

If the conditions encountered are different than those indicated, the Contractor shall notify the Engineer to determine if the foundation dimensions need to be modified. If dimensions "B" or "F" are revised by more than 12" by the Contractor, "as-built" plans shall be prepared and submitted to the District Bureau of Operations for future reference.

No sonotubes or decomposable forms shall be used below the lower conduit entrance. Permanent metal forms or other shielding may not be left in place below that elevation without the Engineer's written permission.

Concrete shall be placed monolithically, without construction joints.

Backfill shall be placed per Article 502 of Standard Specification and prior to erection of support column.

A normal surface finish followed by a Bridge Seat Sealer application will be required on concrete surfaces above the lowest elevation 6" below finished ground line. Cost included in Drilled Shaft Concrete Foundation.

| Structure Number | Station | Left Foundation | | | Right Foundation | | | Class DS Concrete (Cu. Yds.) | | | | |
|------------------|-----------|-----------------|------------------|-------|------------------|--------|---------------|------------------------------|------------------|--------|---------|------|
| | | Elevation Top | Elevation Bottom | A | B | F | Elevation Top | | Elevation Bottom | A | B | F |
| 8S0601270L003.0 | 249+50.00 | 447.10 | 427.10 | 3'-6" | 16'-6" | 20'-0" | 447.10 | 427.18 | 3'-5" | 16'-6" | 19'-11" | 20.9 |
| | | | | | | | | | | | | |
| | | | | | | | | | | | | |
| | | | | | | | | | | | | |
| | | | | | | | | | | | | |
| | | | | | | | | | | | | |
| | | | | | | | | | | | | |

DETAILS FOR 10" φ SUPPORT FRAME
TYPE I-A or II-A TRUSS

OVERHEAD SIGN STRUCTURES
DRILLED SHAFT DETAILS

F.A.I. ROUTE 270 (I-270), SEC. 60-2RS-3
MADISON COUNTY

| | |
|----------|-----|
| DESIGNED | KLH |
| CHECKED | JJD |
| DRAWN | KLH |
| CHECKED | JJD |

| NUMBER | REVISION | DATE |
|--------|----------|------|
| | | |
| | | |
| | | |
| | | |

SHEET NO. 14
14 SHEETS

HORNER & SHIFRIN, INC.
ENGINEERS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 135 |

CONTRACT NO. 76D87

ILLINOIS FED. AID PROJECT

OS4-F3

12-1-08



SOIL BORING LOG

Date 01/18/10

ROUTE FAI 270 / IL Rte 3 DESCRIPTION Overhead Sign Foundation LOGGED BY SCI/BCR

SECTION 60-2RS-3 LOCATION Granite City, S1/2, SEC. 29, TWP. 4N, RNG. 9W

COUNTY Madison DRILLING METHOD CME 75 w/HSA HAMMER TYPE Automatic

| STRUCT. NO. | DEPTH | BLOW | UCS | MOIST | Surface Water Elev. | DEPTH | BLOW | UCS | MOIST |
|--|-------|-------|-------|-------|--|-------|-------|-------|-------|
| Station | H | S | Qu | T | ft | H | S | Qu | T |
| BORING NO. B-SP1 | | | | | Groundwater Elev.: | | | | |
| Station 1347+50 | | | | | First Encounter 406.9 ft | | | | |
| Offset 65.0 ft R | | | | | Upon Completion Not observed | | | | |
| Ground Surface Elev. 419.9 ft | (ft) | (/6") | (tsf) | (%) | After - Hrs. - ft | (ft) | (/6") | (tsf) | (%) |
| ASPHALT - 22 inches | | | | | SAND: Brown, fine (A-3) (continued) | | | | |
| | 9 | | | | | 6 | | | |
| FILL: Gray, silty clay (A-6) 418.1 | 5 | 3.0 | 17 | | | 8 | | | |
| | 5 | P | | | | 7 | | | |
| CLAY: Brown (A-7) 416.9 | | | | | | | | | |
| | 3 | | | | | 4 | | | |
| | 7 | 4.2 | 24 | | | 8 | | | |
| | 9 | B | | | | 9 | | | |
| | -5 | | | | | -25 | | | |
| SILTY CLAY: Brown, some sand (A-6) 413.2 | 2 | 1.9 | 28 | | | 2 | | | |
| | 3 | B | | | | 3 | | | |
| | 4 | | | | | 9 | | | |
| SILT: Brown (A-4) 411.9 | | | | | SAND: Gray, fine to medium (A-3) 391.9 | | | | |
| | 1 | | | | | 2 | | | |
| | 2 | <0.25 | 30 | | | 9 | | | |
| | 2 | P | | | | 12 | | | |
| | -10 | | | | | -30 | | | |
| SAND: Brown, fine (A-3) 407.1 | | | | | | | | | |
| | WH | | | | | | | | |
| | 2 | <0.25 | 31 | | | | | | |
| | 2 | P | | | | | | | |
| | 3 | | | | | 3 | | | |
| | 3 | | | | | 3 | | | |
| | 3 | | | | | 8 | | | |
| | -15 | | | | | -35 | | | |
| | 1 | | | | | | | | |
| | 5 | | | | | | | | |
| | 6 | | | | | | | | |
| | 2 | | | | | 4 | | | |
| | 5 | | | | | 12 | | | |
| | 8 | | | | | 20 | | | |
| | -20 | | | | | -40 | | | |

Boring terminated at 40.0 ft.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)



SOIL BORING LOG

Date 01/20/10

ROUTE FAI 270 / IL Rte 3 DESCRIPTION Overhead Sign Foundation LOGGED BY SCI/BCR

SECTION 60-2RS-3 LOCATION Granite City, S1/2, SEC. 29, TWP. 4N, RNG. 9W

COUNTY Madison DRILLING METHOD CME 75 w/HSA HAMMER TYPE Automatic

| STRUCT. NO. | DEPTH | BLOW | UCS | MOIST | Surface Water Elev. | DEPTH | BLOW | UCS | MOIST |
|--------------------------------|-------|-------|-------|-------|-------------------------------------|-------|-------|-------|-------|
| Station | H | S | Qu | T | ft | H | S | Qu | T |
| BORING NO. B-SP2 | | | | | Groundwater Elev.: | | | | |
| Station 1347+50 | | | | | First Encounter 411.2 ft | | | | |
| Offset 5.0 ft R | | | | | Upon Completion - ft | | | | |
| Ground Surface Elev. 421.7 ft | (ft) | (/6") | (tsf) | (%) | After - Hrs. - ft | (ft) | (/6") | (tsf) | (%) |
| FILL: Brown, silt (A-4) 420.4 | | | | | SAND: Brown, fine (A-3) (continued) | | | | |
| | 3 | | | | | | | | |
| FILL: Brown, clay (A-7) 418.7 | 5 | 3.8 | 26 | | | 4 | | | |
| Gray | 6 | B | | | | 6 | | | |
| | | | | | | 11 | | | |
| CLAY: Brown (A-7) 416.9 | | | | | | | | | |
| | 1 | | | | | 2 | | | |
| | 4 | 1.8 | 34 | | | 3 | | | |
| | 4 | B | | | | 7 | | | |
| | -5 | | | | | -25 | | | |
| CLAYEY SILT: Brown (A-4) 416.9 | | | | | | | | | |
| | WH | | | | | 2 | | | |
| | 1 | 0.5 | 32 | | | 4 | | | |
| | 3 | P | | | | 9 | | | |
| | 2 | | | | | | | | |
| | WH | | | | | 5 | | | |
| | 1 | <0.25 | 39 | | | 8 | | | |
| | 2 | P | | | | 18 | | | |
| | -10 | | | | | -30 | | | |
| SAND: Brown, fine (A-3) 411.2 | | | | | | | | | |
| | 2 | | | | | | | | |
| | 3 | | | | | | | | |
| | 4 | | | | | | | | |
| | 2 | | | | | 1 | | | |
| | 3 | | | | | 2 | | | |
| | 6 | | | | | 3 | | | |
| | 8 | | | | | -35 | | | |
| | -15 | | | | | | | | |
| | 3 | | | | | | | | |
| | 5 | | | | | | | | |
| | 8 | | | | | | | | |
| | 4 | | | | | 4 | | | |
| | 9 | | | | | 7 | | | |
| | 10 | | | | | 15 | | | |
| | -20 | | | | | -40 | | | |

Boring terminated at 40.0 ft.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)

LAST SAVED = 3/16/2010
PEN TABLE = VB.tbl
PLOT DRIVER = TR-Xerox6204-To-File.plt

| | | | |
|---|----------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - |
| U:\0906600\0906601\cad\plans\085.087\087-Sht-OSS-9Log.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 50.0023' / IN. | | CHECKED - | REVISED - |
| PLOT DATE = 3/16/2010 11:42:00 PM | | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



SIGN STRUCTURE PLANS
BORING LOGS

SCALE: NONE SHEET NO. 1 OF 6 SHEETS STA. TO STA.

| | | | | |
|--------------------|----------|---------|---------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 136 |
| CONTRACT NO. 76D87 | | | ILLINOIS FED. AID PROJECT | |



SOIL BORING LOG

Date 01/15/10

ROUTE FAI 270 / IL Rte 3 DESCRIPTION Overhead Sign Foundation LOGGED BY SCI/BCR

SECTION 60-2RS-3 LOCATION Granite City, S1/2, SEC. 29, TWP. 4N, RNG. 9W

COUNTY Madison DRILLING METHOD CME 75 w/HSA HAMMER TYPE Automatic

| STRUCT. NO. Station | DEPTH (ft) | BLOW (/6") | UCS (tsf) | MOIST (%) | Surface Water Elev. ft | Stream Bed Elev. ft | GROUNDWATER ELEV.: First Encounter Upon Completion After Hrs. | DEPTH (ft) | BLOW (/6") | UCS (tsf) | MOIST (%) |
|------------------------|---------------|---------------|--------------|--------------|---------------------------|------------------------|--|---------------|---------------|--------------|--------------|
|------------------------|---------------|---------------|--------------|--------------|---------------------------|------------------------|--|---------------|---------------|--------------|--------------|

| | | | | | | | | | | | |
|--|-------|---|-------|----|--|-------|---|----|-----|----|--|
| CRUSHED ROCK - 12 inches | 422.5 | | | | | | SANDY SILT: Brown and gray (A-4) (continued) | | | | |
| FILL: Brown, clay (A-7) | | 2 | | | | 402.3 | SAND: Brown, fine sand (A-3) | 2 | | | |
| | | 2 | 1.9 | 37 | | | Hydrometer and sieve analysis conducted on sample from 21.25 to 22.5 feet | 1 | | | |
| | | 3 | B | | | | Some clay | 2 | | | |
| | | | | | | 400.5 | SANDY SILT: Gray (A-4) | 1 | | | |
| | | 3 | | | | | | 2 | 0.5 | 31 | |
| | | 4 | 2.8 | 30 | | | SAND: Gray, fine (A-3) | 3 | P | | |
| | | 5 | B | | | 398.0 | | | | | |
| | | | | | | | | 8 | | | |
| | | 2 | | | | | | 10 | | | |
| | | 4 | 2.5 | 40 | | | | 10 | | | |
| | | 5 | B | | | | | | | | |
| | | | | | | | | | | | |
| | | 2 | | | | | | 4 | | | |
| | | 3 | 2.1 | 33 | | | | 8 | | | |
| | | 5 | B | | | -10 | | 8 | | | |
| CLAY: Gray (A-7) | 413.0 | | | | | | | | | | |
| | | 2 | | | | | | | | | |
| Brown | | 4 | 2.2 | 33 | | | | | | | |
| | | 5 | B | | | | | | | | |
| | | | | | | | | | | | |
| Gray and brown | | 1 | | | | | | 10 | | | |
| | | 2 | 0.5 | 33 | | | | 8 | | | |
| | | 3 | B | | | -15 | | 7 | | | |
| | | | | | | | | | | | |
| | | 1 | | | | | | | | | |
| | | 2 | 1.2 | 35 | | | | | | | |
| | | 3 | B | | | | | | | | |
| SILTY SAND: Brown (A-2) | 406.0 | | | | | | | | | | |
| Atterberg Limits (18.5-20.0 ft): Liquid Limit = 29, Plastic Limit = 28, Plasticity Index = 1 | 404.8 | | | | | | | | | | |
| | | 1 | WH | | | | | 4 | | | |
| | | 1 | <0.25 | 33 | | | | 7 | | | |
| | | 1 | P | | | -20 | | 8 | | | |
| | | | | | | | | | | | |

Boring terminated at 40.0 ft.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)



SOIL BORING LOG

Date 01/14/10

ROUTE FAI 270 / IL Rte 3 DESCRIPTION Overhead Sign Foundation LOGGED BY SCI/BCR

SECTION 60-2RS-3 LOCATION Granite City, S1/2, SEC. 29, TWP. 4N, RNG. 9W

COUNTY Madison DRILLING METHOD CME 75 w/HSA HAMMER TYPE Automatic

| STRUCT. NO. Station | DEPTH (ft) | BLOW (/6") | UCS (tsf) | MOIST (%) | Surface Water Elev. ft | Stream Bed Elev. ft | GROUNDWATER ELEV.: First Encounter Upon Completion After Hrs. | DEPTH (ft) | BLOW (/6") | UCS (tsf) | MOIST (%) |
|------------------------|---------------|---------------|--------------|--------------|---------------------------|------------------------|--|---------------|---------------|--------------|--------------|
|------------------------|---------------|---------------|--------------|--------------|---------------------------|------------------------|--|---------------|---------------|--------------|--------------|

| | | | | | | | | | | | |
|--|-------|----|-------|----|--|-------|-------------------------------------|--|--|--|--|
| FILL: Gray, clay, some sand, some rock (A-7) | 416.8 | | | | | | SAND: Brown, fine (A-3) (continued) | | | | |
| | | 4 | | | | | | | | | |
| FILL: Brown, sandy silt (A-4) | 416.0 | | | | | | | | | | |
| | | 4 | 3.1 | 22 | | | | | | | |
| | | 5 | B | | | 396.0 | CLAYEY SILT: Gray, some sand (A-4) | | | | |
| FILL: Gray and brown, clay (A-7) | | | | | | | | | | | |
| | | | | | | 394.8 | SAND: Brown, fine (A-3) | | | | |
| | | | | | | | | | | | |
| | | 1 | | | | | | | | | |
| | | 3 | 1.9 | 30 | | | | | | | |
| | | 4 | B | | | -25 | | | | | |
| | | | | | | | | | | | |
| | | 2 | | | | | | | | | |
| | | 3 | 1.3 | 38 | | | | | | | |
| | | 4 | S/15 | | | | | | | | |
| | | | | | | 409.8 | | | | | |
| CLAY: Gray (A-7) | | | | | | | | | | | |
| | | 1 | | | | | | | | | |
| | | 3 | 2.3 | 32 | | | | | | | |
| | | 4 | B | | | -10 | | | | | |
| Brown | | | | | | | | | | | |
| | | 2 | | | | | | | | | |
| | | 3 | 2.1 | 34 | | | | | | | |
| | | 4 | B | | | | | | | | |
| | | | | | | | | | | | |
| | | | | | | | | | | | |
| | | 1 | | | | | | | | | |
| | | 3 | 0.5 | 34 | | | | | | | |
| | | 2 | B | | | -15 | | | | | |
| | | | | | | | | | | | |
| | | | | | | 402.3 | | | | | |
| SAND: Brown, fine (A-3) | | | | | | | | | | | |
| | | 1 | | | | | | | | | |
| CLAYEY SILT: Brown and gray, some sand (A-4) | 401.3 | | | | | | | | | | |
| | | 1 | <0.25 | 28 | | | | | | | |
| | | 1 | P | | | | | | | | |
| Hydrometer and sieve analysis conducted on sample from 16.5 to 17.5 feet | 399.8 | | | | | | | | | | |
| | | | | | | | | | | | |
| | | | | | | | | | | | |
| | | 3 | | | | | | | | | |
| | | 1 | WR | | | | | | | | |
| SAND: Brown, fine (A-3) | | | | | | | | | | | |
| | | | | | | | | | | | |
| | | 4 | | | | | | | | | |
| | | 8 | | | | | | | | | |
| | | | | | | | | | | | |
| | | 12 | | | | -40 | | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)

LAST SAVED = 3/16/2010
PEN TABLE = V8.tbl
PLOT DRIVER = TR-Xe-ep6204-To-File.plt

| FILE NAME | USER NAME | DESIGNED | REVISED |
|--|-----------|----------|---------|
| \\29266001\0905601\cad\l\plans\085_087 | sdonahue | - | - |
| 087-Str-OSS-BLog.dgn | | - | - |
| PLOT SCALE = 50.0023' / IN. | | - | - |
| PLOT DATE = 3/16/2010 11:42:01 PM | | - | - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



SIGN STRUCTURE PLANS
BORING LOGS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------|---------|--------------|--------------------|
| 270 | 60-2RS-3 | MADISON | 231 | 137 |
| | | | | CONTRACT NO. 76D87 |

SCALE: NONE SHEET NO. 2 OF 6 SHEETS STA. TO STA.



SOIL BORING LOG

Date 01/14/10

ROUTE FAI 270 / IL Rte 3 DESCRIPTION Overhead Sign Foundation LOGGED BY SCI/BCR

SECTION 60-2RS-3 LOCATION Granite City, S1/2, SEC. 29, TWP. 4N, RNG. 9W

COUNTY Madison DRILLING METHOD CME 75 w/HSA HAMMER TYPE Automatic

| STRUCT. NO. Station | DEPTH H | BLOW S | UCS Qu | MOIST T | Surface Water Elev. _____ ft | Stream Bed Elev. _____ ft |
|--|--------------|-----------|-----------|------------|--|---------------------------|
| BORING NO. <u>B-SP9</u> Station <u>31272+40</u> Offset <u>0.0 ft R</u> Ground Surface Elev. <u>417.8</u> ft | (ft) | (/6") | (tsf) | (%) | Groundwater Elev.: First Encounter <u>403.3</u> ft ▼ Upon Completion _____ ft After _____ Hrs. _____ ft | |
| SAND: Gray, fine to medium, trace coarse sand (A-3) (continued) | | | | | | |
| | <u>375.8</u> | | | | | |
| SAND: Gray, fine to coarse (A-1) | | | | | | |
| | <u>372.8</u> | <u>6</u> | | | | |
| | <u>45</u> | <u>11</u> | | | | |
| Boring terminated at 45.0 ft. | <u>45</u> | <u>14</u> | | | | |
| | <u>-50</u> | | | | | |
| | <u>-55</u> | | | | | |
| | <u>-60</u> | | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)



SOIL BORING LOG

Date 01/13/10

ROUTE FAI 270 / IL Rte 3 DESCRIPTION Overhead Sign Foundation LOGGED BY SCI/BCR

SECTION 60-2RS-3 LOCATION Granite City, S1/2, SEC. 29, TWP. 4N, RNG. 9W

COUNTY Madison DRILLING METHOD CME 75 w/HSA HAMMER TYPE Automatic

| STRUCT. NO. Station | DEPTH H | BLOW S | UCS Qu | MOIST T | Surface Water Elev. _____ ft | Stream Bed Elev. _____ ft |
|---|--------------|-----------|-------------|------------|--|---------------------------|
| BORING NO. <u>B-SP10</u> Station <u>31261+49</u> Offset <u>8.0 ft L</u> Ground Surface Elev. <u>443.6</u> ft | (ft) | (/6") | (tsf) | (%) | Groundwater Elev.: First Encounter <u>406.6</u> ft ▼ Upon Completion _____ ft After _____ Hrs. _____ ft | |
| ASPHALT - 12 inches | | | | | | |
| | <u>442.6</u> | | | | | |
| FILL: Gray, clay, some sand (A-7) | | <u>7</u> | | | FILL: Gray, clay (A-7) (continued) | |
| | <u>440.6</u> | <u>4</u> | <u>2.1</u> | <u>24</u> | Some sand | <u>2</u> |
| | <u>440.6</u> | <u>5</u> | <u>B</u> | | | <u>5</u> |
| FILL: Gray, sandy silt, some clay (A-4) | | | | | FILL: Gray and brown, clay (A-7) | <u>6</u> |
| | <u>438.1</u> | <u>2</u> | | | | <u>1.5</u> |
| | <u>436.9</u> | <u>5</u> | <u>1.9</u> | <u>22</u> | | <u>30</u> |
| | <u>436.9</u> | <u>6</u> | <u>S/15</u> | | | |
| FILL: Gray, sandy silt (A-4) | | | | | FILL: Gray, silty clay, some sand (A-6) | |
| | <u>435.6</u> | <u>2</u> | | | | |
| | <u>435.6</u> | <u>5</u> | <u>3.0</u> | <u>20</u> | | |
| FILL: Gray, clay (A-7) | | | | | | |
| | <u>435.6</u> | <u>6</u> | <u>B</u> | | | |
| FILL: Gray, silty clay, some sand (A-6) | | | | | | |
| | <u>432.4</u> | <u>1</u> | | | CLAY: Gray (A-7) | |
| | <u>431.6</u> | <u>2</u> | <u>1.4</u> | <u>23</u> | | |
| | <u>430.6</u> | <u>6</u> | <u>B</u> | | | |
| FILL: Gray, silty clay, some sand (A-6) | | | | | | |
| | <u>429.1</u> | <u>3</u> | | | | |
| | <u>429.1</u> | <u>6</u> | <u>3.0</u> | <u>30</u> | | |
| FILL: Gray, clay (A-7) | | | | | | |
| | <u>429.1</u> | <u>10</u> | <u>B</u> | | | |
| | <u>429.1</u> | <u>15</u> | <u>B</u> | | | |
| FILL: Gray, silty clay, some sand (A-6) | | | | | | |
| | <u>411.6</u> | <u>2</u> | | | CLAYEY SILT: Gray and brown (A-4) | |
| | <u>411.6</u> | <u>9</u> | <u>1.3</u> | <u>17</u> | | |
| FILL: Brown, sand fine (A-3) | | | | | | |
| | <u>409.1</u> | <u>10</u> | <u>B</u> | | | |
| FILL: Gray, silty clay, some sand (A-6) | | | | | | |
| | <u>409.1</u> | <u>2</u> | | | | |
| | <u>409.1</u> | <u>3</u> | <u>2.1</u> | <u>22</u> | | |
| FILL: Gray, clay (A-7) | | | | | | |
| | <u>406.6</u> | <u>6</u> | <u>B</u> | | | |
| | <u>406.6</u> | <u>7</u> | <u>S/15</u> | | | |
| FILL: Gray, clay (A-7) | | | | | | |
| | <u>406.6</u> | <u>2</u> | | | SANDY SILT: Gray and brown (A-4) | |
| | <u>406.6</u> | <u>5</u> | <u>2.8</u> | <u>21</u> | | |
| FILL: Gray, clay (A-7) | | | | | | |
| | <u>406.6</u> | <u>7</u> | <u>S/15</u> | | | |
| | <u>406.6</u> | <u>1</u> | | | SILTY SAND: Brown, fine (A-2) | |
| | <u>406.6</u> | <u>3</u> | <u>1.0</u> | <u>28</u> | | |
| | <u>406.6</u> | <u>6</u> | <u>S/10</u> | | | |
| | <u>406.6</u> | <u>1</u> | | | | |
| | <u>406.6</u> | <u>3</u> | | | | |
| | <u>406.6</u> | <u>4</u> | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)

LAST SAVED = 3/16/2010
PEN TABLE = V8.tbl
PLOT DRIVER = TR-Xerox6204-To-File.plt

| | | | |
|-------------------------------------|-----------------------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - |
| \\0906602\0906621\cad\plans\085.087 | 6087-Sht-OSS-BLog.dgn | DRAWN - | REVISED - |
| | PLOT SCALE = 50.0023 ' / IN. | CHECKED - | REVISED - |
| | PLOT DATE = 3/16/2010 11:42:02 PM | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



SIGN STRUCTURE PLANS
BORING LOGS

SCALE: NONE

SHEET NO. 3 OF 6 SHEETS STA. TO STA.

| | | | | |
|--------------------|------------------|----------------|---------------------------|---------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 138 |
| CONTRACT NO. 76D87 | | | ILLINOIS FED. AID PROJECT | |



Illinois Department of Transportation

Division of Highways
SCI Engineering

SOIL BORING LOG

Page 2 of 2

Date 01/13/10

ROUTE FAI 270 / IL Rte 3 DESCRIPTION Overhead Sign Foundation LOGGED BY SCI/BCR
SECTION 60-2RS-3 LOCATION Granite City, S1/2, SEC. 29, TWP. 4N, RNG. 9W
COUNTY Madison DRILLING METHOD CME 75 w/HSA HAMMER TYPE Automatic

| STRUCT. NO. Station | D E P T H | B L O W S | U C S Qu | M O I S T | Surface Water Elev. _____ ft | | Stream Bed Elev. _____ ft | |
|--|-----------------------|-----------------------|-------------------|-----------------------|------------------------------|--|---------------------------|--|
| | | | | | | | | |
| BORING NO. Station Offset | | | | | Groundwater Elev.: | | | |
| B-SP10 31261+49 8.0 ft L | | | | | First Encounter 406.6 ft | | Upon Completion - ft | |
| Ground Surface Elev. 443.6 ft | | | | | After - Hrs. - ft | | | |
| SILTY SAND: Brown, fine (A-2) (continued) | | | | | | | | |
| | | | | | | | | |
| SAND: Gray, fine (A-3) | | | | | | | | |
| Approximately 1 inch of blow-in at sampling depth. | | 1 3 4 | | | | | | |
| | | | | | | | | |
| Slightly coarser | | | | | | | | |
| Approximately 1 inch of blow-in at sampling depth. | | 4 4 7 | | | | | | |
| Boring terminated at 50.0 ft. | | | | | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)



Illinois Department of Transportation

Division of Highways
SCI Engineering

SOIL BORING LOG

Page 1 of 2

Date 01/14/10

ROUTE FAI 270 / IL Rte 3 DESCRIPTION Overhead Sign Foundation LOGGED BY SCI/BCR
SECTION 60-2RS-3 LOCATION Granite City, S1/2, SEC. 29, TWP. 4N, RNG. 9W
COUNTY Madison DRILLING METHOD CME 75 w/HSA HAMMER TYPE Automatic

| STRUCT. NO. Station | D E P T H | B L O W S | U C S Qu | M O I S T | Surface Water Elev. _____ ft | | Stream Bed Elev. _____ ft | |
|---|-----------------------|-----------------------|-------------------|-----------------------|------------------------------|--|---------------------------|--|
| | | | | | | | | |
| BORING NO. Station Offset | | | | | Groundwater Elev.: | | | |
| B-SP11 31261+45 52.0 ft L | | | | | First Encounter 405.1 ft | | Upon Completion - ft | |
| Ground Surface Elev. 442.1 ft | | | | | After - Hrs. - ft | | | |
| FILL: Brown, silty clay, some crushed rock (A-6) | | | | | | | | |
| | | | | | | | | |
| FILL: Gray and brown, clay (A-7) | | | 0.8 S/15 | 34 | | | | |
| | | | | | | | | |
| Gray | | 2 5 6 | 2.4 B | 28 | | | | |
| | | | | | | | | |
| FILL: Brown, sandy silt, some clay (A-4) | | | 0.9 S/10 | 19 | | | | |
| | | | | | | | | |
| FILL: Gray, clay (A-7) | | | 3.0 B | 27 | | | | |
| | | | | | | | | |
| FILL: Brown, silty clay, some sand, some silt (A-6) | | | 3.1 S/15 | 28 | | | | |
| | | | | | | | | |
| FILL: Gray, clay, some sand (A-7) | | | 3.5 B | 18 | | | | |
| | | | | | | | | |
| FILL: Gray, clay (A-7) | | | 1.5 B | 20 | | | | |
| | | | | | | | | |
| Brown, trace to some sand | | 2 4 6 | 1.4 S/15 | 32 | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)

LAST SAVED = 3/16/2010
PEN TABLE = VB.tbl
PLOT DRIVER = TR-Xerox6204-To-File.plt

| | | | |
|--------------------------------------|------------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED = | REVISED = |
| \\0906600\09066001\oad\plans\085.087 | sd087-Sht-OSS-BLog.dgn | DRAWN = | REVISED = |
| PLOT SCALE = 50.0023 / / IN. | CHECKED = | REVISOR = | REVISED = |
| PLOT DATE = 3/16/2010 11:42:03 PM | DATE = | REVISOR = | REVISED = |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



SIGN STRUCTURE PLANS
BORING LOGS

SCALE: NONE

SHEET NO. 4 OF 6 SHEETS STA. TO STA.

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 139 |
| CONTRACT NO. 76D87 | | | | |

ILLINOIS FED. AID PROJECT



Illinois Department of Transportation
Division of Highways
SCI Engineering

SOIL BORING LOG

Page 2 of 2

Date 01/14/10

ROUTE FAI 270 / IL Rte 3 DESCRIPTION Overhead Sign Foundation LOGGED BY SCI/BCR

SECTION 60-2RS-3 LOCATION Granite City, S1/2, SEC. 29, TWP. 4N, RNG. 9W

COUNTY Madison DRILLING METHOD CME 75 w/HSA HAMMER TYPE Automatic

Table with columns for DEPTH, BLOW COUNT, UCS, M.O.I.S.T., and Soil Descriptions (e.g., SILT, SAND, CLAY). Includes groundwater elevation data and hammer test results.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)



Illinois Department of Transportation
Division of Highways
SCI Engineering

SOIL BORING LOG

Page 1 of 1

Date 02/17/10

ROUTE FAI 270 / IL Rte 3 DESCRIPTION Overhead Sign Foundation LOGGED BY SCI/BCR

SECTION 60-2RS-3 LOCATION Granite City, S1/2, SEC. 29, TWP. 4N, RNG. 9W

COUNTY Madison DRILLING METHOD CME 45 w/HSA HAMMER TYPE Automatic

Table with columns for DEPTH, BLOW COUNT, UCS, M.O.I.S.T., and Soil Descriptions (e.g., ASPHALT, SAND, CLAY). Includes groundwater elevation data and hammer test results.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION



SIGN STRUCTURE PLANS BORING LOGS

Table with columns for F.A.I. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO., CONTRACT NO.

SCALE: NONE SHEET NO. 5 OF 6 SHEETS STA. TO STA. ILLINOIS FED. AID PROJECT

FILE NAME = ... USER NAME = sdonahue ... PLOT SCALE = 50.0023' / 1" ... PLOT DATE = 3/16/2010 11:42:04 PM

LAST SAVED = 3/16/2010 PEN TABLE = V8.tbl PLOT DRIVER = TR: Xerox6204- To:File.plt



Illinois Department of Transportation
Division of Highways
SCI Engineering

SOIL BORING LOG

Page 1 of 1

Date 02/17/10

ROUTE FAI 270 / IL Rte 3 DESCRIPTION Overhead Sign Foundation LOGGED BY SCI/BCR

SECTION 60-2RS-3 LOCATION Granite City, S1/2, SEC. 29, TWP. 4N, RNG. 9W

COUNTY Madison DRILLING METHOD CME 45 w/HSA HAMMER TYPE Automatic

| STRUCT. NO. Station | DESCRIPTION | D E P T H (ft) | B L O W S (/6") | U C S Qu (tsf) | M O I S T Q T (%) | Surface Water Elev. Stream Bed Elev. | D E P T H (ft) | B L O W S (/6") | U C S Qu (tsf) | M O I S T Q T (%) |
|---|---|-------------------------------|--------------------------------|----------------------------|--|---|-------------------------------|--------------------------------|----------------------------|--|
| Ex. 8S0605003R008.6 1370+15 | | | | | | | | | | |
| BORING NO. Station Offset Ground Surface Elev. | B-SP13 1370+24 5.0 ft L 415.3 ft | | | | | | | | | |

| Soil Description | D E P T H (ft) | B L O W S (/6") | U C S Qu (tsf) | M O I S T Q T (%) | Notes |
|---|-------------------------------|--------------------------------|----------------------------|--|-------|
| FILL: Brown, silt, trace rock (A-4) | | | | | |
| | 2 | | | | |
| | 14 | 0.8 | P | 21 | |
| Last N-value of SPT test not performed - concrete obstruction. Boring drilled directly east of storm sewer. | | | | | |
| | 412.3 | | | | |
| FILL: Brown and gray, clay (A-7) | | | | | |
| | 1 | | | | |
| | 2 | 1.6 | B | 33 | |
| | 3 | | | | |
| | -5 | | | | |
| | 409.8 | | | | |
| CLAY: Gray and brown (A-7) | | | | | |
| | 1 | | | | |
| | 1 | 1.0 | B | 46 | |
| | 2 | | | | |
| | 407.3 | | | | |
| SILT: Brown and gray (A-4) | | | | | |
| | 1 | | | | |
| | 1 | <0.25 | P | 30 | |
| | 2 | | | | |
| | -10 | | | | |
| | | | | | |
| | 1 | | | | |
| | 1 | 0.3 | P | 30 | |
| | 3 | | | | |
| | ▼ | | | | |
| Becomes gray | | | | | |
| | 1 | | | | |
| | 1 | <0.25 | P | 39 | |
| | 1 | | | | |
| | -15 | | | | |
| | 399.8 | | | | |
| SAND: Gray, fine (A-3) | | | | | |
| | 1 | | | | |
| | 1 | 0.4 | B | 64 | |
| | 1 | | | | |
| | 398.3 | | | | |
| CLAY: Gray (A-7) | | | | | |
| | 1 | | | | |
| | 397.3 | | | | |
| SAND: Gray, fine (A-2) | | | | | |
| | 1 | | | | |
| | 1 | 0.4 | B | 56 | |
| | 1 | | | | |
| | 4 | | | | |
| | -20 | | | | |
| | 395.8 | | | | |
| | 375.3 | | | | |
| | -40 | | | | |

Boring terminated at 40.0 ft.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrator)
AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)



Illinois Department of Transportation
Division of Highways
SCI Engineering

SOIL BORING LOG

Page 1 of 1

Date 02/18/10

ROUTE FAI 270 / IL Rte 3 DESCRIPTION Overhead Sign Foundation LOGGED BY SCI/BCR

SECTION 60-2RS-3 LOCATION Granite City, S1/2, SEC. 29, TWP. 4N, RNG. 9W

COUNTY Madison DRILLING METHOD CME 45 w/HSA HAMMER TYPE Automatic

| STRUCT. NO. Station | DESCRIPTION | D E P T H (ft) | B L O W S (/6") | U C S Qu (tsf) | M O I S T Q T (%) | Surface Water Elev. Stream Bed Elev. | D E P T H (ft) | B L O W S (/6") | U C S Qu (tsf) | M O I S T Q T (%) |
|---|---|-------------------------------|--------------------------------|----------------------------|--|---|-------------------------------|--------------------------------|----------------------------|--|
| Ex. 8C061270R002.7 235+25 | | | | | | | | | | |
| BORING NO. Station Offset Ground Surface Elev. | B-SP14 235+55 70.0 ft R 435.0 ft | | | | | | | | | |

| Soil Description | D E P T H (ft) | B L O W S (/6") | U C S Qu (tsf) | M O I S T Q T (%) | Notes |
|---|-------------------------------|--------------------------------|----------------------------|--|-------|
| FILL: Dark brown, sandy silt, some rock (A-4) | | | | | |
| | 1 | | | | |
| | 3 | 1.0 | P | 24 | |
| | 3 | | | | |
| | 414.5 | | | | |
| Becomes brown, no rock | | | | | |
| | 1 | | | | |
| | 3 | 1.0 | P | 24 | |
| | 3 | | | | |
| | 431.0 | | | | |
| FILL: Brown, silty clay (A-6) | | | | | |
| | 1 | | | | |
| | 2 | 1.0 | B | 30 | |
| | 3 | | | | |
| | 430.3 | | | | |
| FILL: Brown, sandy silt (A-4) | | | | | |
| | 1 | | | | |
| | 4 | 1.1 | S/10 | 19 | |
| | 5 | | | | |
| | 6 | | | | |
| | 427.0 | | | | |
| FILL: Brown and gray, clay (A-7) | | | | | |
| | 1 | | | | |
| | 3 | 1.2 | S/15 | 37 | |
| | 3 | | | | |
| | -10 | | | | |
| | | | | | |
| | 1 | | | | |
| | 3 | 1.3 | S/15 | 30 | |
| | 3 | | | | |
| | 404.0 | | | | |
| SAND: Brown, fine, some silt (A-2) | | | | | |
| | 1 | | | | |
| | 2 | | | | |
| | 3 | 1.4 | S/15 | 49 | |
| | 3 | | | | |
| | -30 | | | | |
| | | | | | |
| | 1 | | | | |
| | 2 | | | | |
| | 2 | | | | |
| | 6 | | | | |
| | -35 | | | | |
| | 398.0 | | | | |
| SAND: Brown, fine (A-3) | | | | | |
| | 2 | | | | |
| | 4 | 2.1 | B | 26 | |
| | 6 | | | | |
| | 417.0 | | | | |
| FILL: Gray, silty sand (A-2) | | | | | |
| | 4 | | | | |
| | 8 | | | | |
| | 6 | | | | |
| | -20 | | | | |
| | 395.0 | | | | |

Boring terminated at 40.0 ft.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrator)
AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)

LAST SAVED = 3/16/2010 11:42:05 AM
PEN TABLE = V8.tbl
PLOT DRIVER = TR: Xerox6204-To-File.plt

FILE NAME = \\09066001\09066001\oad\t\plans\085.087\ED07-Sht-OSS-BLog.dgn
USER NAME = sdonahue

DESIGNED -
DRAWN -
CHECKED -
DATE -

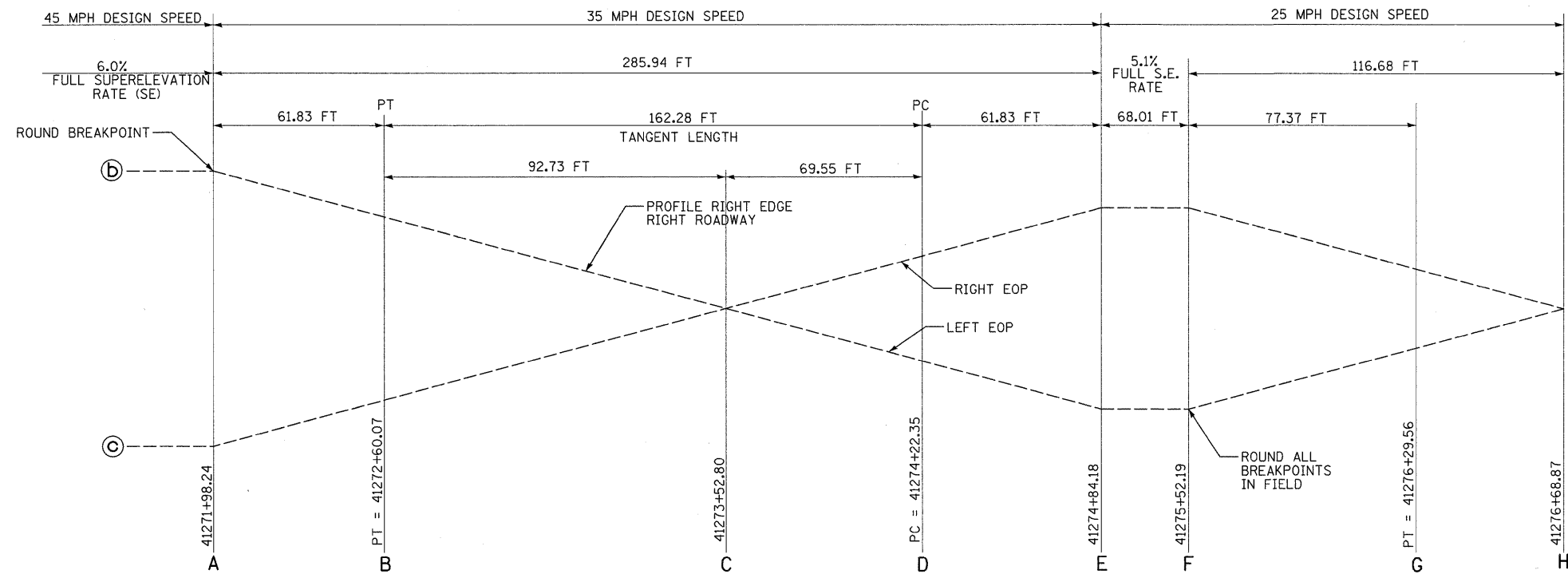
REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

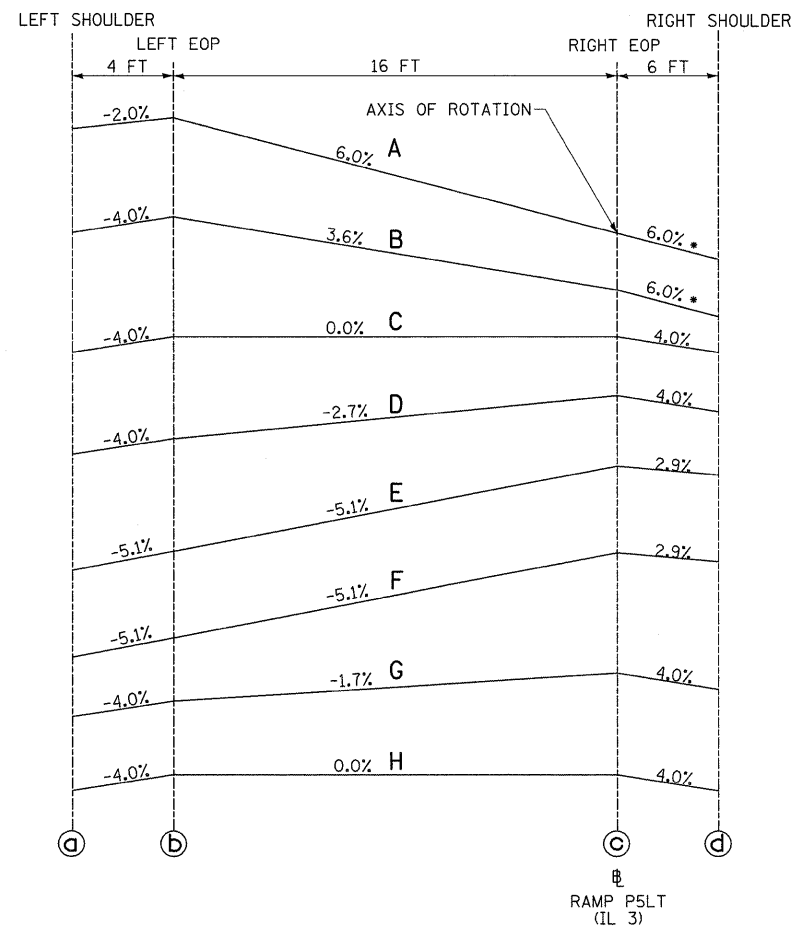
HORNER & SHIFRIN, INC.
ENGINEERS

SIGN STRUCTURE PLANS
BORING LOGS
SCALE: NONE SHEET NO. 6 OF 6 SHEETS STA. TO STA.

| | | | | |
|---------------------------|----------|---------|--------------|-----------|
| F.A.I. RTE: | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 141 |
| CONTRACT NO. | | | 76D87 | |
| ILLINOIS FED. AID PROJECT | | | | |



ELEVATION



SUPERELEVATION DETAILS

| | ELEVATIONS | | | |
|---|------------|--------|--------|--------|
| | a | b | c | d |
| A | 424.19 | 424.27 | 424.31 | 423.95 |
| B | 422.58 | 422.74 | 422.16 | 421.80 |
| C | 420.28 | 420.44 | 420.44 | 420.20 |
| D | 418.56 | 418.72 | 419.15 | 418.91 |
| E | 417.07 | 417.27 | 418.08 | 417.91 |
| F | 416.59 | 416.79 | 417.61 | 417.44 |
| G | 417.64 | 417.80 | 418.07 | 417.83 |
| H | 418.32 | 418.48 | 418.48 | 418.24 |

* RAMP P-5 TRAVELED WAY

CROSS SECTIONS

LAST SAVED = 3/13/2010
PEN TABLE = VB.tbl
PLOT DRIVER = TR-Xerox6204-To-File.plt

| | | | |
|---|---------------------|------------|-----------|
| FILE NAME = | USER NAME = sconeuh | DESIGNED - | REVISED - |
| 1:\0906680\09066801\cad\plans\006.D07\087-Sht-Details.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 50.0023' / IN. | | CHECKED - | REVISED - |
| PLOT DATE = 3/16/2010 4:44:02 PM | | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

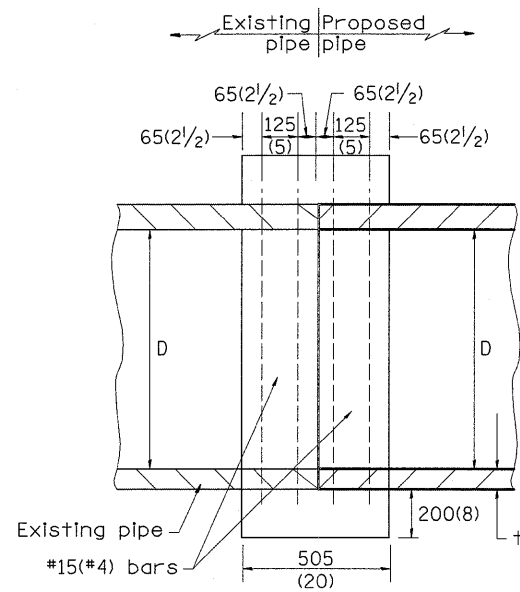
HORNER & SHIFRIN, INC.
ENGINEERS

SPECIAL DETAILS
RAMP P5LT SPECIAL SUPERELEVATION TRANSITION

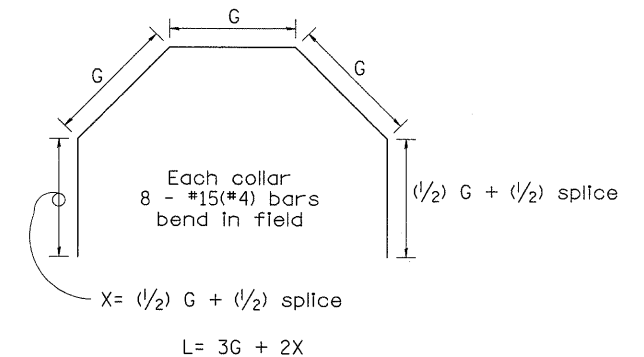
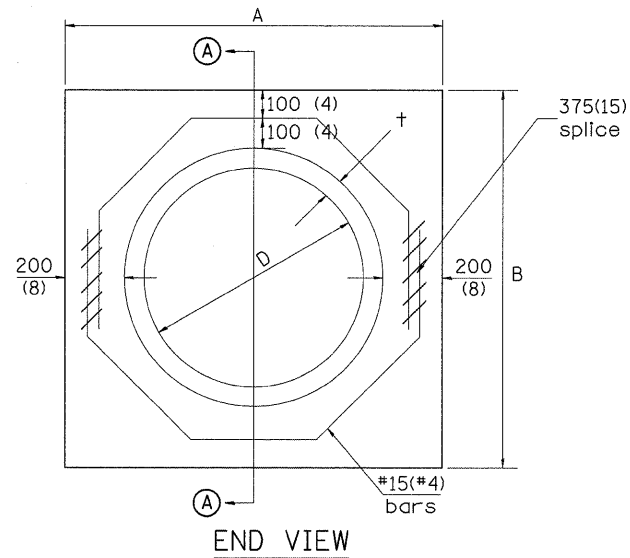
SCALE: NONE

SHEET NO. 1 OF 4 SHEETS STA. TO STA.

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 142 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



SECTION A - A



| Each Collar | | | | | | | | |
|--------------------|------------|--------------|--------------|-------------------------|----------------|---------------|---------------|---------|
| Reinforcement Bars | | | | | | | | |
| D | † | A | B | CL. SI CONC. | G | X | L | Weight |
| m (in) | m (in) | m (ft) | m (ft) | m ³ (cu. yd) | m (in) | m (in) | m (ft) | kg (lb) |
| 300 (12) | 51 (2.00) | 0.814 (2.67) | 0.814 (2.67) | 0.270 (0.4) | 253 (9 15/16) | 317 (12 7/16) | 1.393 (4.57) | 11 (24) |
| 375 (15) | 57 (2.25) | 0.902 (2.96) | 0.902 (2.96) | 0.315 (0.4) | 290 (11 3/8) | 335 (13 3/16) | 1.541 (5.05) | 12 (27) |
| 450 (18) | 64 (2.50) | 0.991 (3.25) | 0.991 (3.25) | 0.362 (0.5) | 327 (12 13/16) | 354 (13 7/8) | 1.689 (5.54) | 14 (30) |
| 525 (21) | 70 (2.75) | 1.079 (3.54) | 1.079 (3.54) | 0.411 (0.5) | 364 (14 1/4) | 372 (14 5/8) | 1.836 (6.02) | 15 (32) |
| 600 (24) | 76 (3.00) | 1.167 (3.83) | 1.167 (3.84) | 0.460 (0.6) | 401 (15 11/16) | 391 (15 5/16) | 1.984 (6.51) | 16 (35) |
| 675 (27) | 83 (3.25) | 1.259 (4.13) | 1.259 (4.13) | 0.516 (0.7) | 438 (17 1/4) | 409 (16 1/16) | 2.131 (6.99) | 17 (37) |
| 750 (30) | 89 (3.50) | 1.347 (4.42) | 1.347 (4.42) | 0.570 (0.7) | 475 (18 11/16) | 428 (16 3/4) | 2.279 (7.48) | 18 (40) |
| 825 (33) | 95 (3.75) | 1.436 (4.71) | 1.436 (4.71) | 0.624 (0.8) | 512 (20 1/8) | 446 (17 1/2) | 2.426 (7.96) | 19 (43) |
| 900 (36) | 102 (4.00) | 1.524 (5.00) | 1.524 (5.00) | 0.682 (0.9) | 549 (21 9/16) | 465 (18 3/16) | 2.574 (8.44) | 20 (45) |
| 1050 (42) | 114 (4.50) | 1.701 (5.58) | 1.701 (5.58) | 0.800 (1.0) | 622 (24 7/16) | 501 (19 3/4) | 2.869 (9.41) | 23 (50) |
| 1200 (48) | 127 (5.00) | 1.881 (6.17) | 1.881 (6.17) | 0.930 (1.2) | 696 (27 5/16) | 538 (21 3/16) | 3.164 (10.38) | 25 (55) |

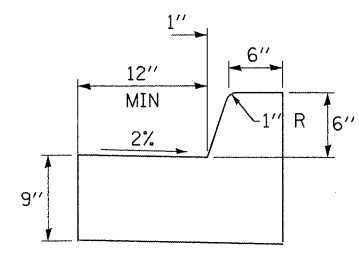
GENERAL NOTES

1. THE COLLAR SHALL BE CONSTRUCTED ENTIRELY OF CLASS SI CONCRETE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 503 OF THE STANDARD SPECIFICATIONS. REINFORCEMENT BARS SHALL CONFORM TO SECTION 508.

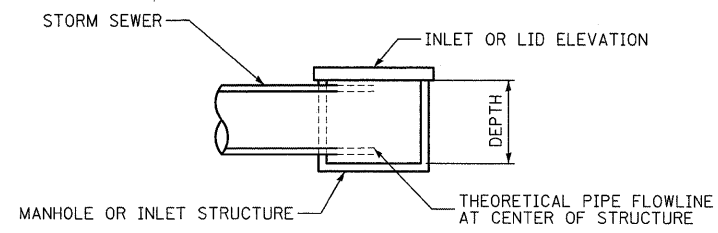
LAST SAVED = 3/13/2010
PEN TABLE = V8.tbl
PLOT DRIVER = TR-Xerox6204-Te-File.plt

| | | | | | | | | | | | | | | |
|-------------------------------------|----------------------------------|------------|-----------|---|--|-------------------------|-------------------------|---------------------------|---------|--------|-----------------|--------------|-----|-----|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | HORNER & SHIFRIN, INC. ENGINEERS | SPECIAL DETAILS | | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | | |
| \\0906600\0906601\cad\plans\086.D87 | 087-Sht-Details.dgn | DRAWN - | REVISED - | | | SCALE: NONE | SHEET NO. 2 OF 4 SHEETS | STA. | TO STA. | 270 | 60-2RS-3 | MADISON | 231 | 143 |
| | PLOT SCALE = 50.0023' / IN. | CHECKED - | REVISED - | | | CONCRETE COLLAR DETAILS | | CONTRACT NO. 76D87 | | | | | | |
| | PLOT DATE = 3/16/2010 4:44:02 PM | DATE - | REVISED - | | | FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | | | | |

GUTTER FLAG WIDTH VARIES THROUGHOUT THE SPLITTER ISLAND. SEE INTERSECTION DETAILS FOR FURTHER INFORMATION.

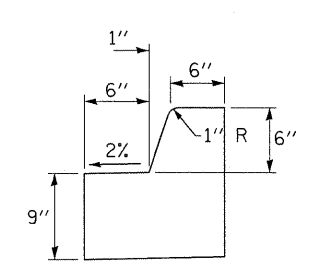


**COMBINATION CONCRETE CURB & GUTTER,
TYPE B (SPECIAL)**

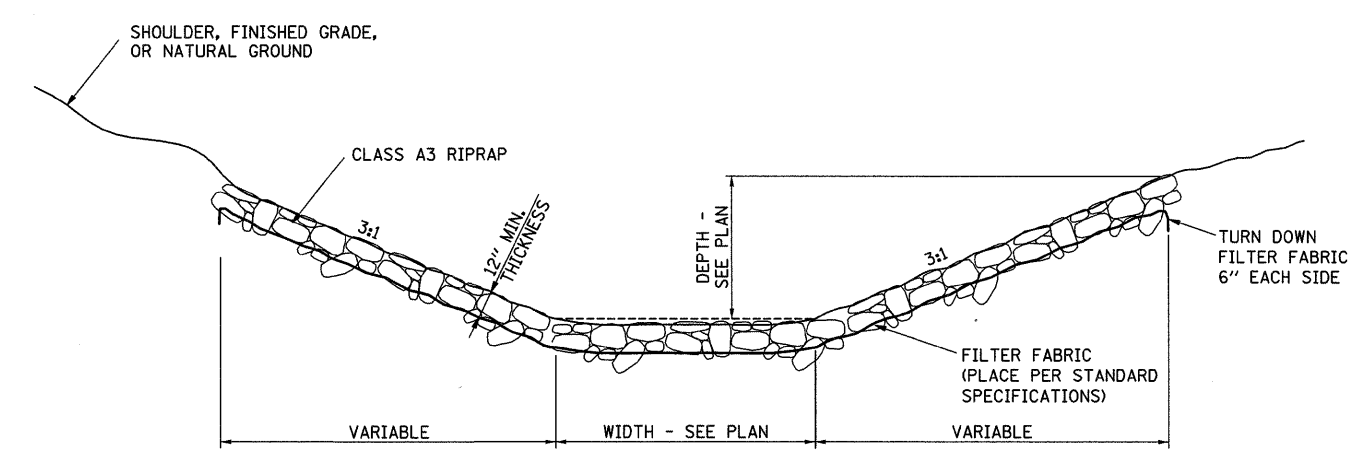


NOTE:
CONTRACTOR TO TAKE CARE TO ACCOUNT FOR PIPE THICKNESS WHEN CONSTRUCTING BOTTOM OF MANHOLE / INLET.

INLET STRUCTURE DETAIL



**COMBINATION CONCRETE CURB & GUTTER,
TYPE B-6.06 (SPECIAL)**



AGGREGATE DITCH

LAST SAVED = 3/13/2010
PEN TABLE = V6.tbl
PLOT DRIVER = TR-Xerox6204-Te-File.plt

| | | | |
|--|----------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - |
| \\09066600\09066601\oad\l_plans\086.L087\087-Sht-Details.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 50.0023 ' / IN. | | CHECKED - | REVISED - |
| PLOT DATE = 3/16/2010 4:44:03 PM | | DATE - | REVISED - |

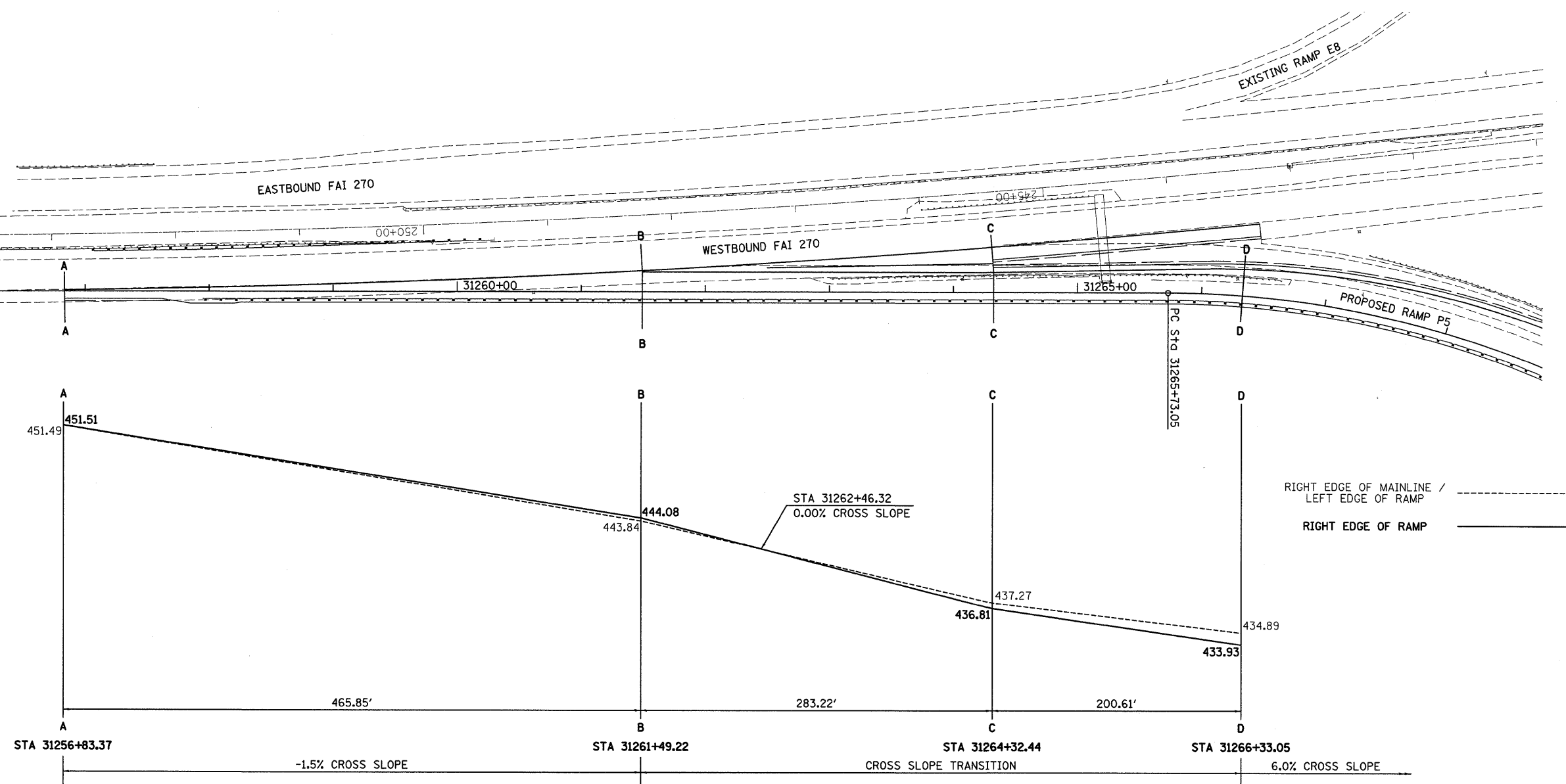
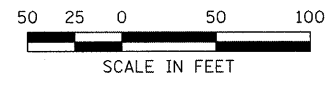
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**HORNER &
SHIFRIN, INC.
ENGINEERS**

SPECIAL DETAILS
CURB AND GUTTER, INLET STRUCTURES,
AND AGGREGATE DITCH

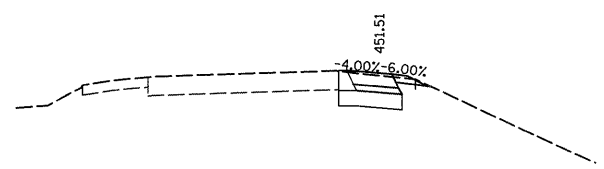
SCALE: NONE SHEET NO. 3 OF 4 SHEETS STA. TO STA.

| | | | | |
|---|----------|---------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 144 |
| CONTRACT NO. T6D87 | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

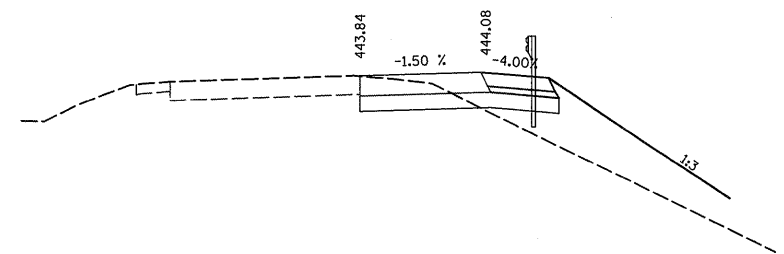


RIGHT EDGE OF MAINLINE /
LEFT EDGE OF RAMP -----
RIGHT EDGE OF RAMP _____

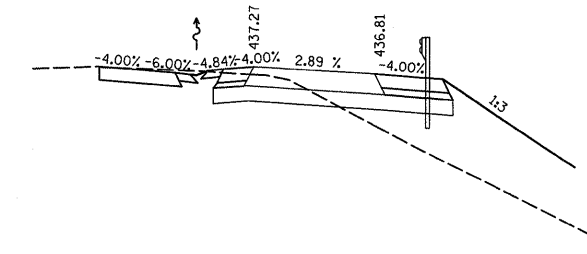
SECTION A - A
STA 31256+83.37



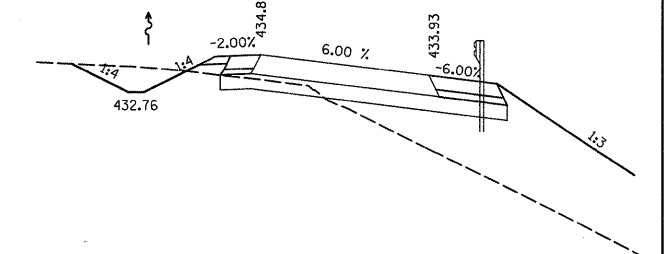
SECTION B - B
STA 31261+49.22



SECTION C - C
STA 31264+32.44



SECTION D - D
STA 31266+33.05



RAMP 5 TERMINAL DETAIL

LAST SAVED = 3/13/2010
PEN TABLE = VB.tbl
PLOT DRIVER = TR: Xerox6284-TopFile.plt

| | | | |
|---|----------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - |
| \\0906600\0906601\oad\plans\085.087\087-Sht-Details.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 50.0023' / IN. | | CHECKED - | REVISED - |
| PLOT DATE = 3/16/2010 4:44:03 PM | | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

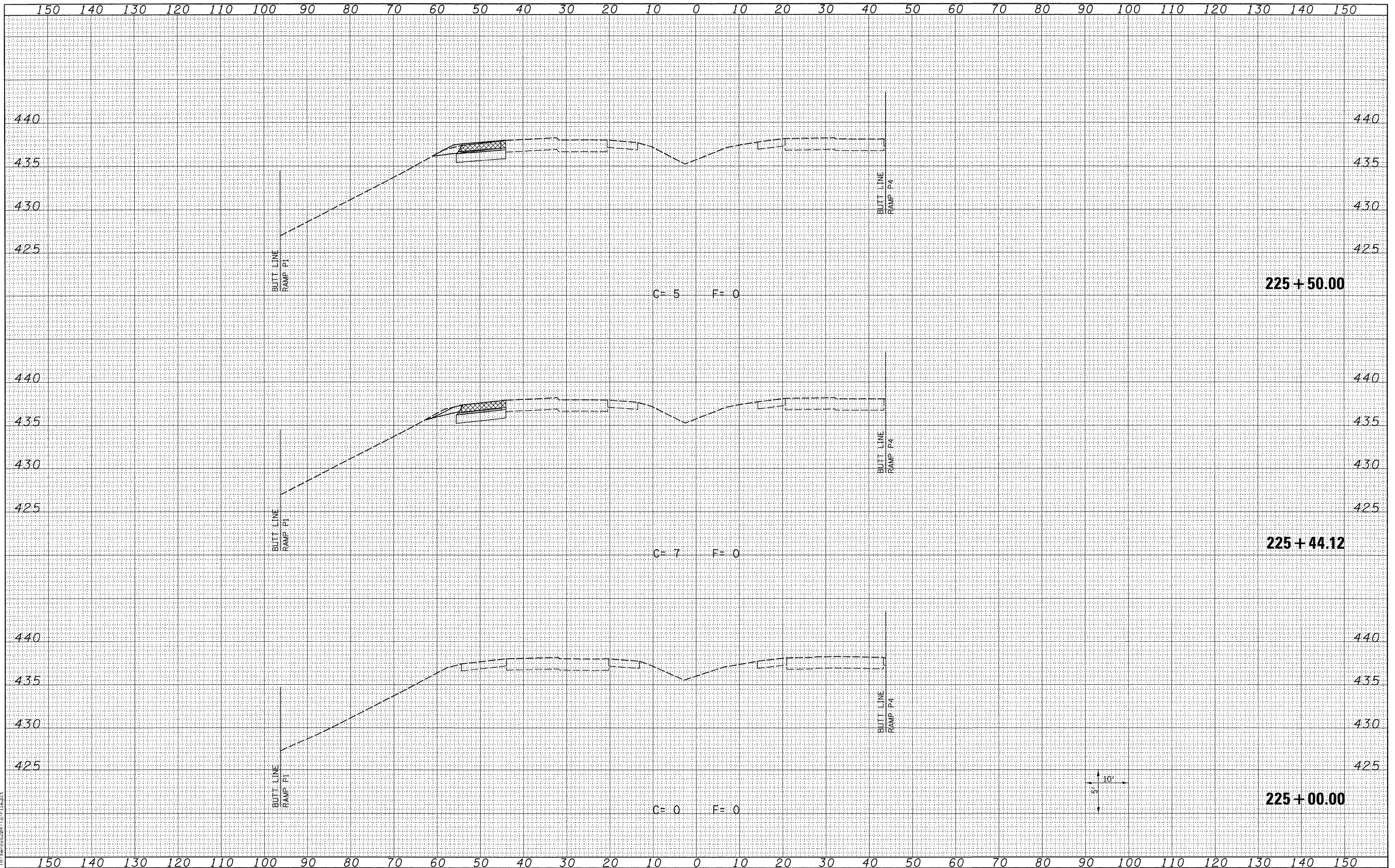


SPECIAL DETAILS
RAMP P5 EXIT TERMINAL
SCALE: NONE SHEET NO. 4 OF 4 SHEETS STA. TO STA.

| | | | | |
|---------------------------|----------|---------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 145 |
| CONTRACT NO. T6D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| | | |
|--------------|---------------|------|
| FINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | TEMP. DATE | |
| | AREAS CHECKED | |

| | | |
|-----------------|---------------|------|
| ORIGINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | TEMP. DATE | |
| | AREAS CHECKED | |



LAST SAVED = 3/13/2010
 PEN TABLE = V8.161
 PLOT DRIVER = TR:Veroc6204-10-F:\eaplts

FILE NAME =
 USER NAME = sdonahue
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISIED -
 REVISIED -
 REVISIED -
 REVISIED -

REVISIED -
 REVISIED -
 REVISIED -
 REVISIED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

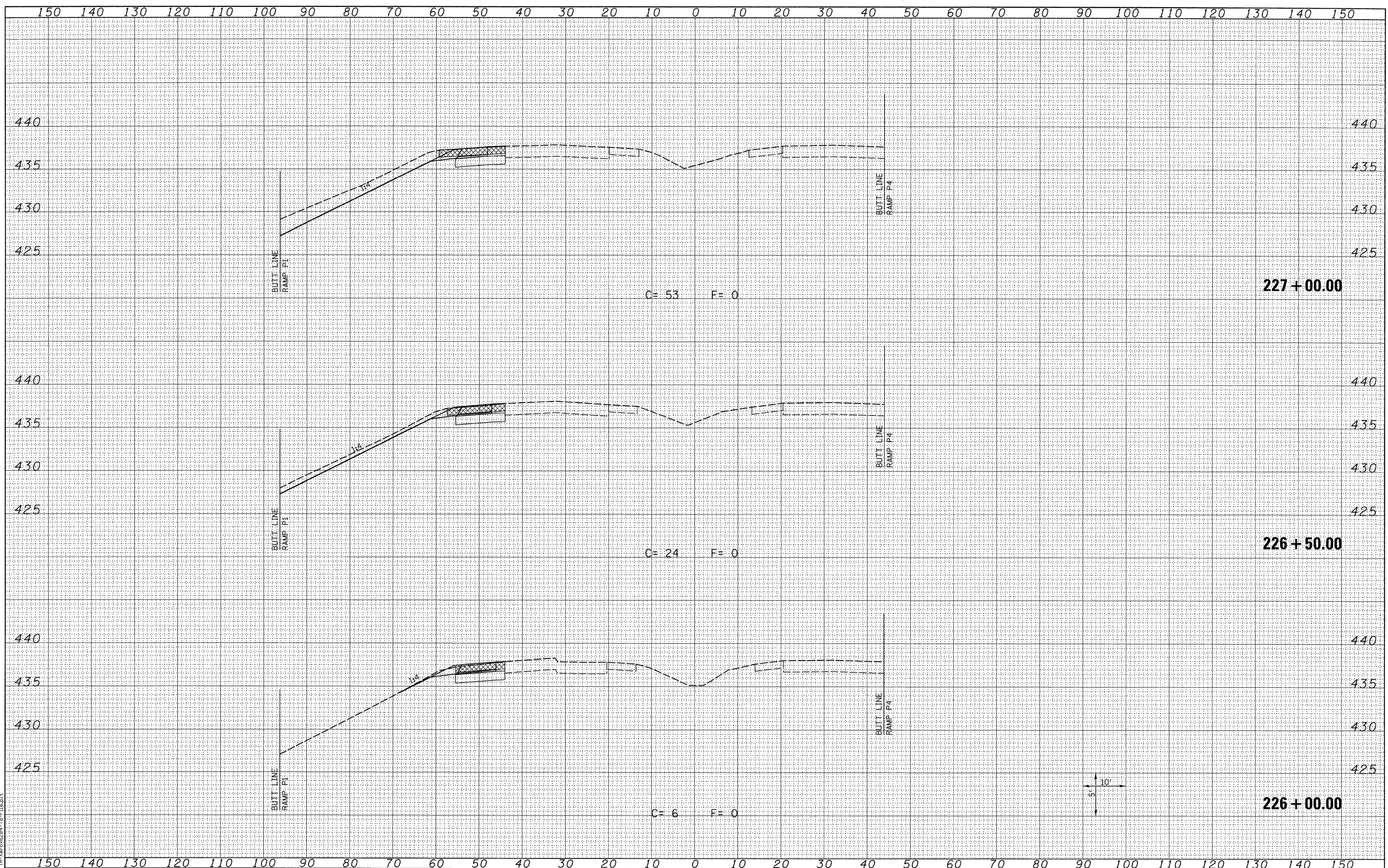
HORNER &
 SHIFRIN, INC.
 ENGINEERS

CROSS SECTIONS
 PROPOSED I-270
 SCALE: SEE SHT
 SHEET NO. 1 OF 6 SHEETS
 STA. 225+00.00 TO STA. 225+50.00

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|---------|--------------|--------------------|
| 270 | 60-2RS-3 | MADISON | 231 | 146 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |

| | | |
|--------------|---------------|------|
| FINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | TEMPLATE | |
| | AREAS CHECKED | |

| | | |
|-----------------|---------------|------|
| ORIGINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | TEMPLATE | |
| | AREAS CHECKED | |



LAST SAVED = 3/13/2010
 PEN TABLE = V8.141
 PLOT DRIVER = TR-Xerox204-10-File.plt

FILE NAME =
 USER NAME = sdonahue
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

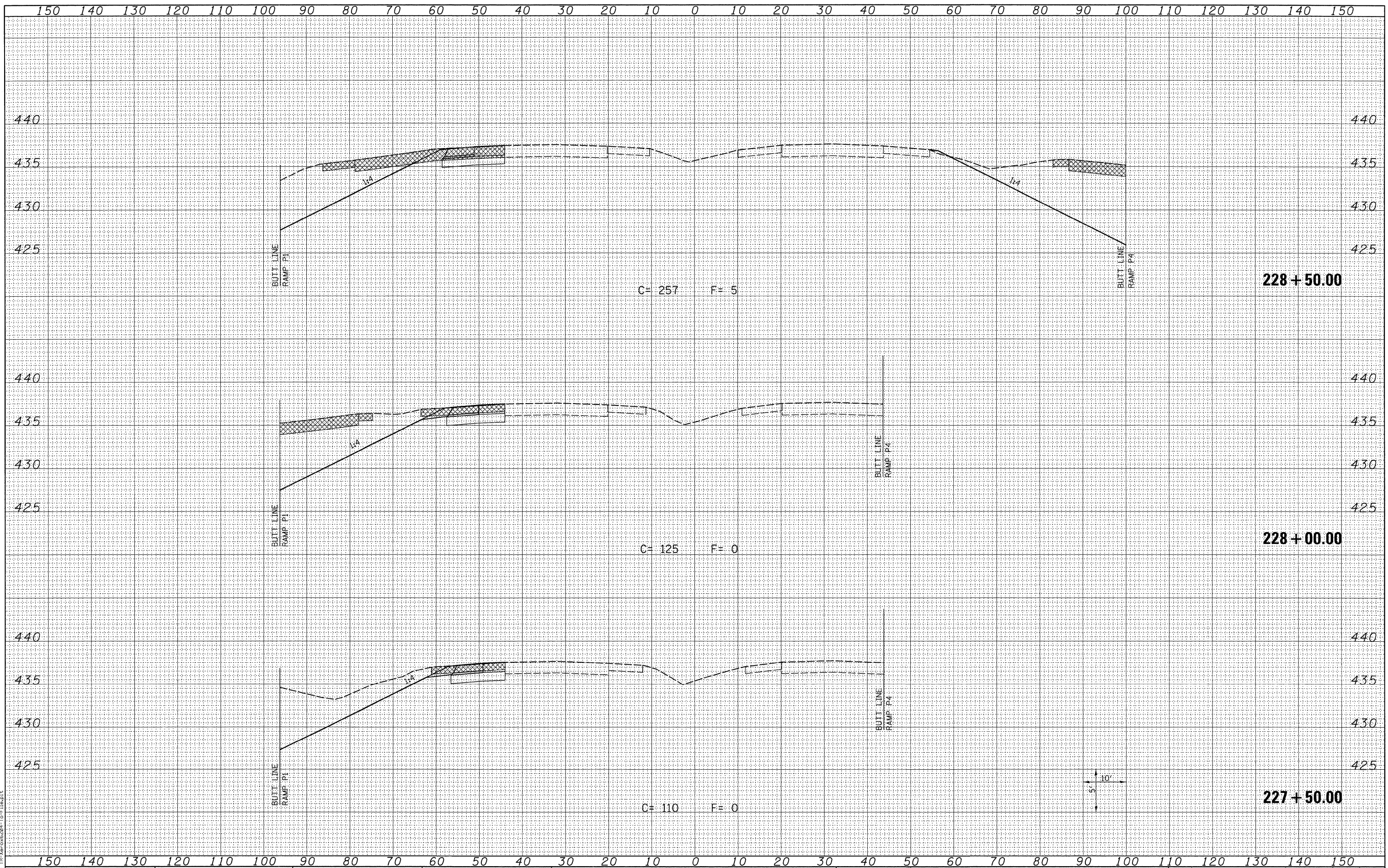
HORNER & SHIFRIN, INC.
 ENGINEERS

CROSS SECTIONS
 PROPOSED I-270
 SCALE: SEE SHT
 SHEET NO. 2 OF 6 SHEETS
 STA. 226+00.00 TO STA. 227+00.00

| | | | | |
|---------------------------|----------|---------|--------------|--------------------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 147 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



LAST SAVED = 3/13/2010
 PEN TABLE = VA.tbl
 PLOT DRIVER = TR:Arco6204-To-F:leg.plt

FILE NAME =
 USER NAME = sdonahue
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISIED -
 REVISIED -
 REVISIED -
 REVISIED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

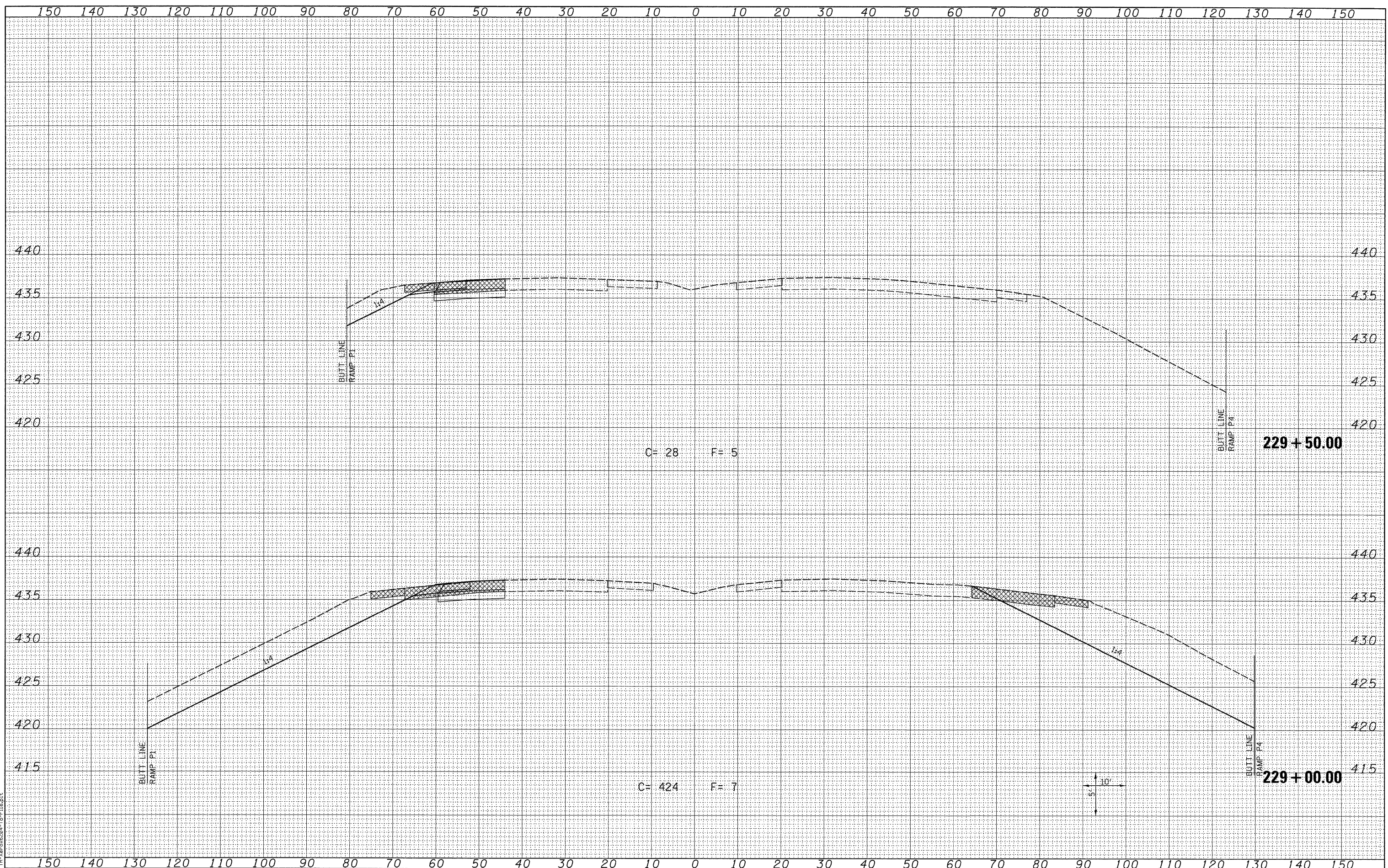


CROSS SECTIONS
 PROPOSED I-270
 SCALE: SEE SHT
 SHEET NO. 3 OF 6 SHEETS
 STA. 227+50.00 TO STA. 228+50.00

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 148 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|------|
| BY | DATE |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|---------------|------|
| BY | DATE |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



LAST SAVED = 3/13/2010
 PEN TABLE = V8.141
 PLOT DRIVER = TR:Veroc6204-To-File.plt

FILE NAME =
 USER NAME = sdonahue
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISIED -
 REVISIED -
 REVISIED -
 REVISIED -

REVISIED -
 REVISIED -
 REVISIED -
 REVISIED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

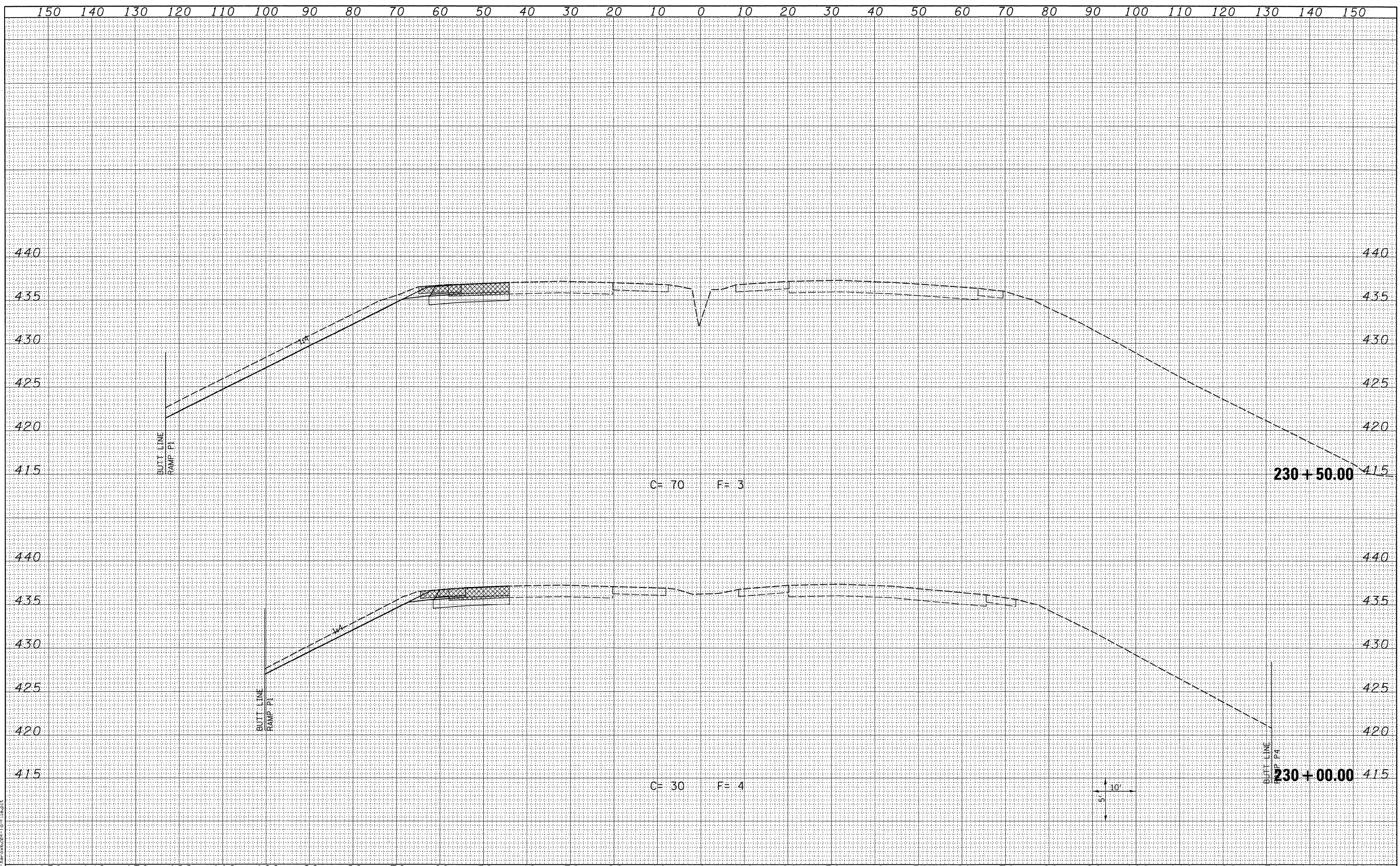
HORNER &
 SHIFRIN, INC
 ENGINEERS

CROSS SECTIONS
 PROPOSED I-270
 SCALE: SEE SHT
 SHEET NO. 4 OF 6 SHEETS
 STA. 229+00.00 TO STA. 229+50.00

| | | | | |
|---------------------------|----------|---------|--------------|--------------------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 149 |
| | | | | CONTRACT NO. 76087 |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|--------------|---------------|
| BY | DATE |
| | |
| FINAL SURVEY | SURVEYED |
| NOTE BOOK | PLOTTED |
| NO. | TEMPLATE |
| | AREAS CHECKED |

| | |
|-----------------|---------------|
| BY | DATE |
| | |
| ORIGINAL SURVEY | SURVEYED |
| NOTE BOOK | PLOTTED |
| NO. | TEMPLATE |
| | AREAS CHECKED |

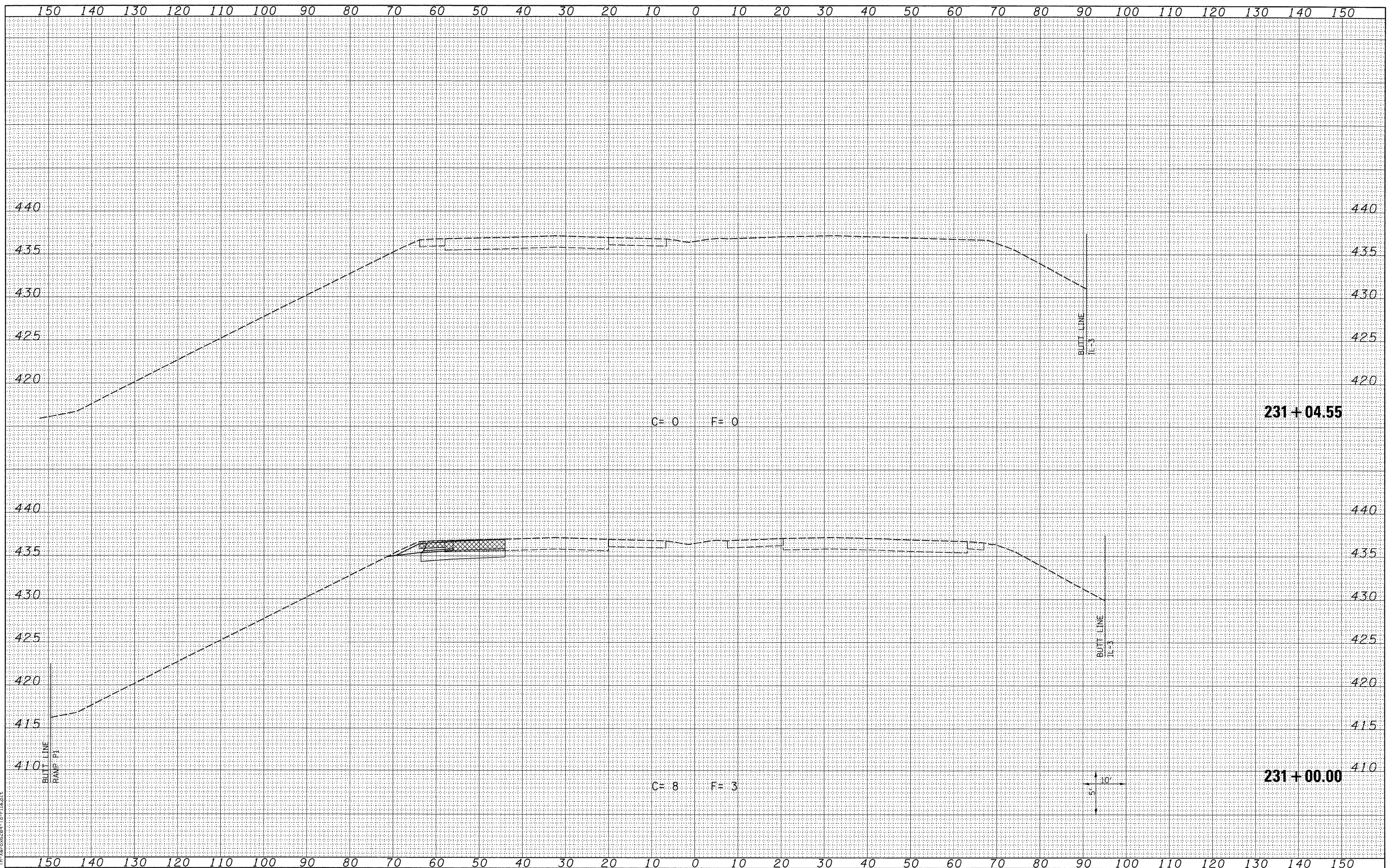


LAST SAVED = 3/13/2010
 PEN TABLE = V8.143
 PLOT DRIVER = TR-Xenc6284-To-File.plt

| | | | | | | | | | | | | | |
|--|----------------------|------------|---------------------------|---|--|---|--|--|--------------------|---------------------|-------------------|---------------------|------------------|
| FILE NAME = \\0906600\0906601\cad\plens\087_0876087-SH-XSht-1270.dgn | USER NAME = sdonahue | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | | CROSS SECTIONS PROPOSED I-270 | | | F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 150 |
| PLOT SCALE = 10.0005' / IN. | CHECKED - | REVISED - | CONTRACT NO. 76D87 | | | | | | | | | | |
| PLOT DATE = 3/16/2010 4:44:10 PM | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | | | | |
| | | | | | | | | | | | | | |

| | | | |
|--------------|---------------|----|------|
| FINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
| NO. | TEMPLATE | | |
| | AREAS CHECKED | | |

| | | | |
|-----------------|---------------|----|------|
| ORIGINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
| NO. | TEMPLATE | | |
| | AREAS CHECKED | | |



LAST SAVED = 3/13/2010
 PEN TABLE = 18.0005
 PLOT DRIVER = TR: Xerox6204; To: File.plt

| | |
|---|----------------------|
| FILE NAME = | USER NAME = sdonehue |
| 1:\09056600\09056601\cad\1\plans\087_0876087-Sht-X9Sht-1270.dgn | DESIGNED - |
| | DRAWN - |
| | CHECKED - |
| | DATE - |

| | |
|----------------------------------|------------|
| PLOT SCALE = 10.0005' / IN. | REVISIED - |
| PLOT DATE = 3/16/2010 4:44:10 PM | REVISIED - |
| | REVISIED - |
| | REVISIED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HORNER &
 SHIFRIN, INC.
 ENGINEERS

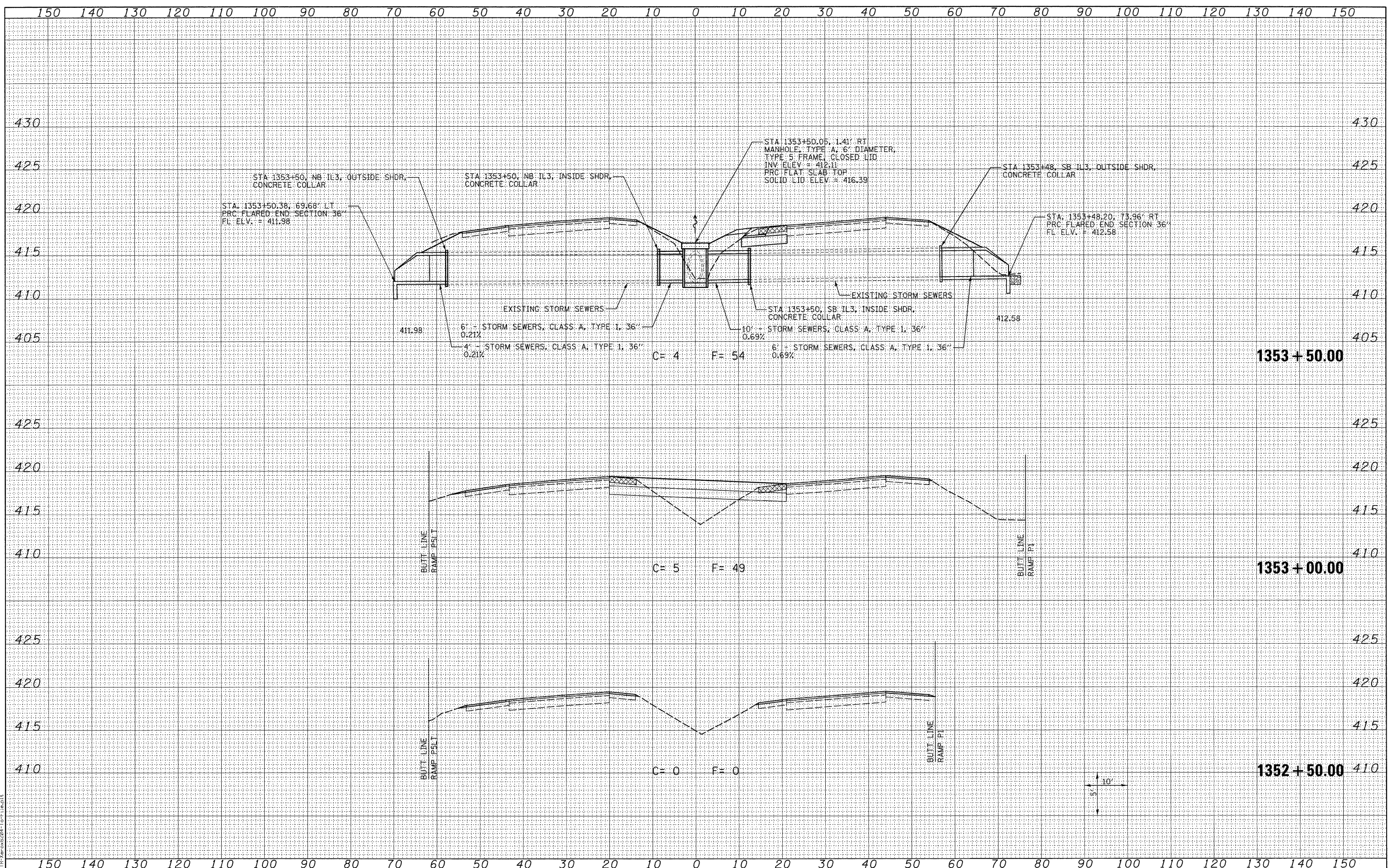
CROSS SECTIONS
 PROPOSED I-270

SCALE: SEE SHT SHEET NO. 6 OF 6 SHEETS STA. 231+00.00 TO STA. 231+04.55

| | | | | |
|---------------------------|------------------|----------------|------------------|--------------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 151 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|--|
| DATE | |
| BY | |
| NO. | |
| FINAL SURVEY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |

| | |
|-----------------|--|
| DATE | |
| BY | |
| NO. | |
| ORIGINAL SURVEY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |

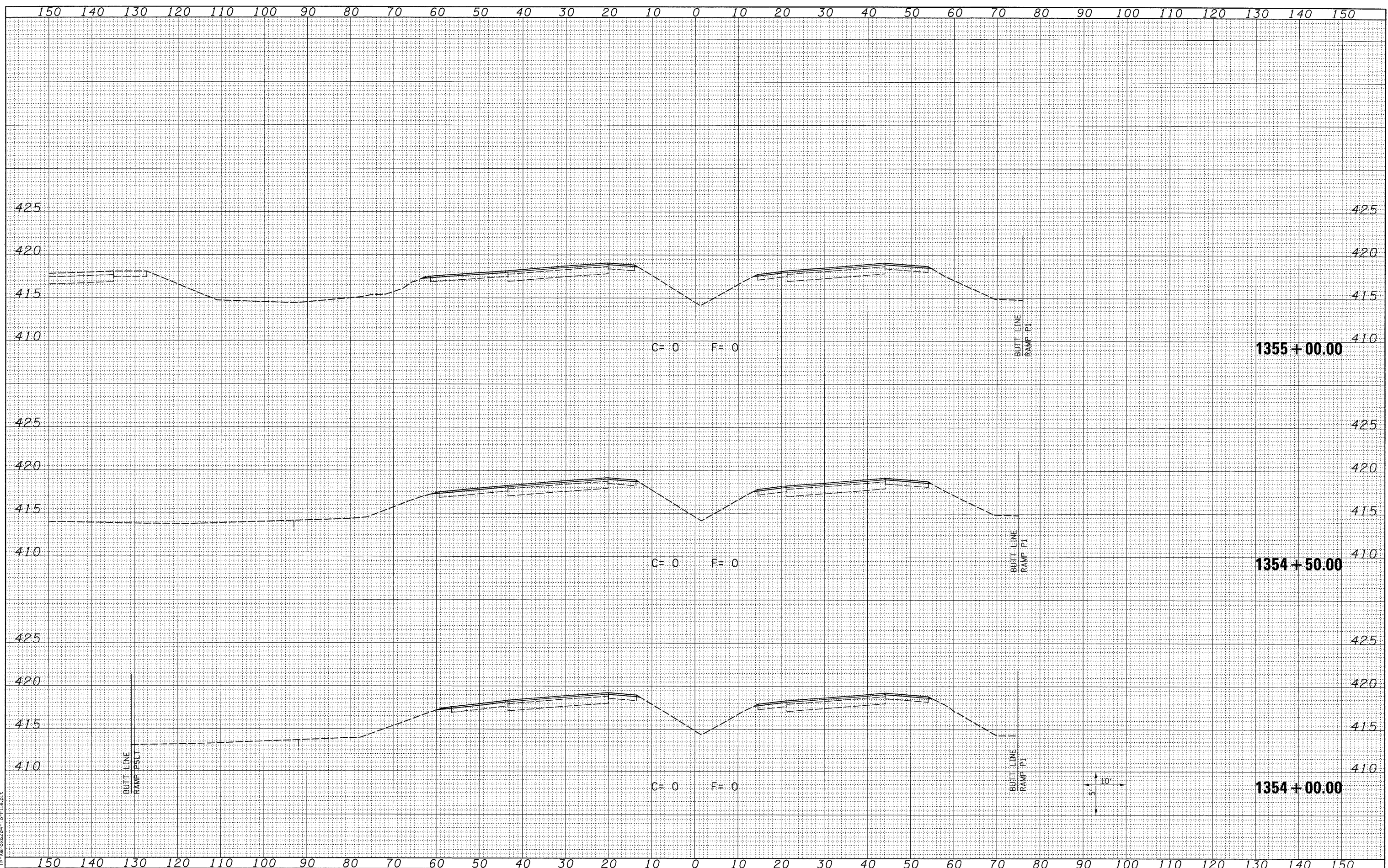


LAST SAVED = 3/16/2010
 PEN TABLE = V8.dwg
 PLOT DRIVER = HP-Verax6294-10-F11e.plt

| | | | | | | | | | | | | |
|--|----------------------|------------|-----------|---|--|--------------------------|--|---------------------------|---------------------|-------------------|---------------------|------------------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | | CROSS SECTIONS | | F.A.I. R.T. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 152 |
| DESIGNED - | REVISED - | DRAWN - | REVISED - | | | ILLINOIS ROUTE 3 | | | | | | |
| DRAWN - | REVISED - | CHECKED - | REVISED - | | | SCALE: SEE SHT | | | | | | |
| CHECKED - | REVISED - | DATE - | REVISED - | | | SHEET NO. 1 OF 15 SHEETS | | | | | | |
| PLOT SCALE = 10.0000' / IN. PLOT DATE = 3/16/2010 11:23:31 PM | | | | STA. 1352+50.00 TO STA. 1353+50.00 | | CONTRACT NO. 76D87 | | ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|------|
| BY | DATE |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|---------------|------|
| BY | DATE |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



LAST SAVED = 3/16/2010
 PEN TABLE = V8.161
 PLOT DRIVER = TR-Vercos204-10-File.plt

FILE NAME =
 USER NAME = sdonahue
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISIED -
 REVISIED -
 REVISIED -
 REVISIED -

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

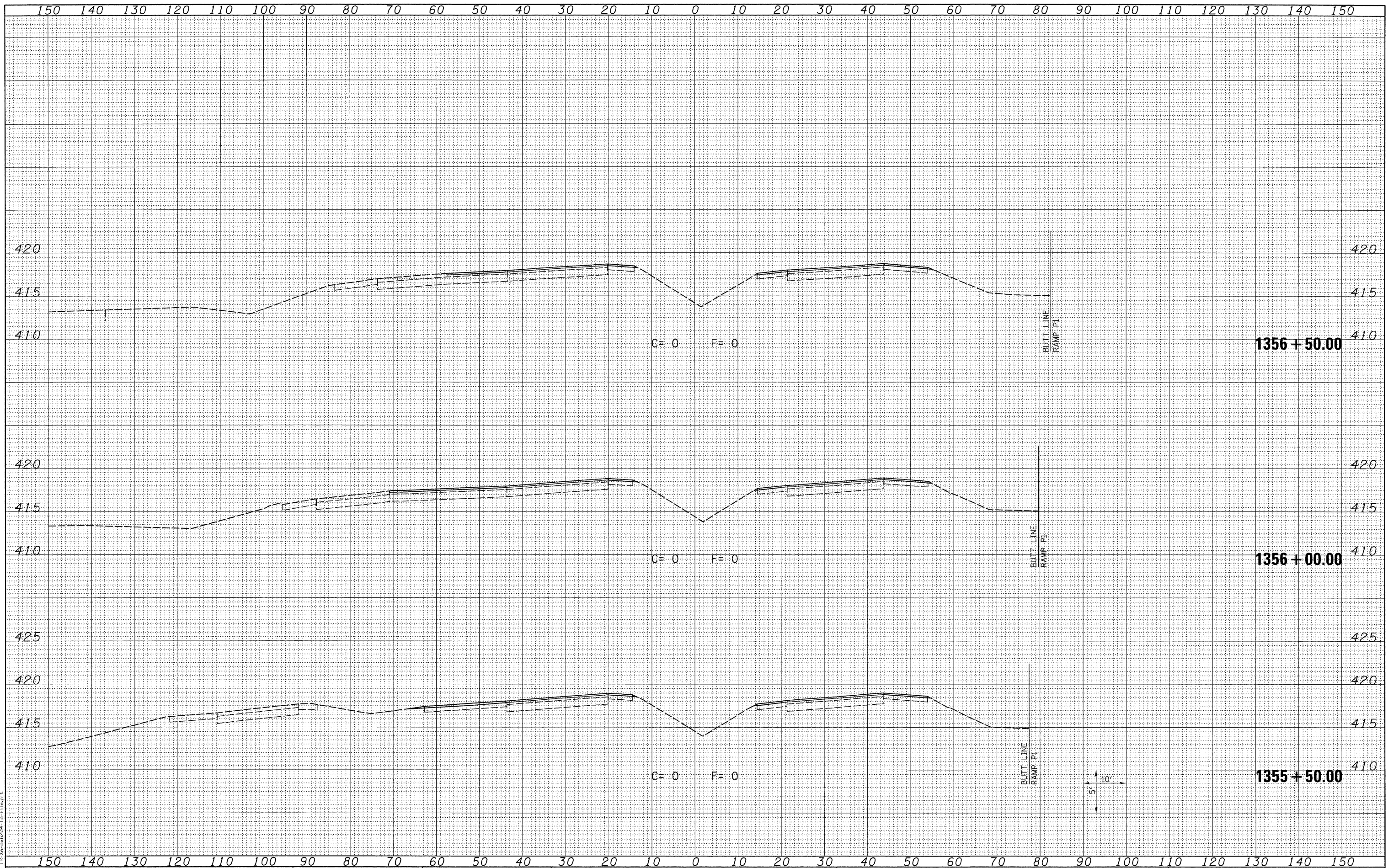
HORNER &
 SHIFRIN, INC
 ENGINEERS

CROSS SECTIONS
 ILLINOIS ROUTE 3
 SCALE: SEE SHT SHEET NO. 2 OF 15 SHEETS STA. 1354+00.00 TO STA. 1355+00.00

| | | | | |
|---------------------------|----------|---------|--------------|--------------------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 153 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|---------------|
| DATE | |
| BY | |
| FINAL SURVEY | SURVEYED |
| NOTE BOOK NO. | PLOTTED |
| | TEMPLATE |
| | AREAS CHECKED |

| | |
|-----------------|---------------|
| DATE | |
| BY | |
| ORIGINAL SURVEY | SURVEYED |
| NOTE BOOK NO. | PLOTTED |
| | TEMPLATE |
| | AREAS CHECKED |



LAST SAVED = 3/16/2018
 PEN TABLE = 1/8, 1/4
 PLOT DRIVER = TR-3e-06204-Te-Fileplot

| | |
|--|----------------------|
| FILE NAME = | USER NAME = sdonahue |
| h:\0906600\0906601\cad\1\plans\088_0876087-Sht-XSSht-IL3.dgn | DESIGNED - |
| | DRAWN - |
| | CHECKED - |
| | DATE - |
| PLOT SCALE = 10.0005 ' / IN. | REVISED - |
| PLOT DATE = 3/16/2018 4:44:16 PM | REVISED - |

| | |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

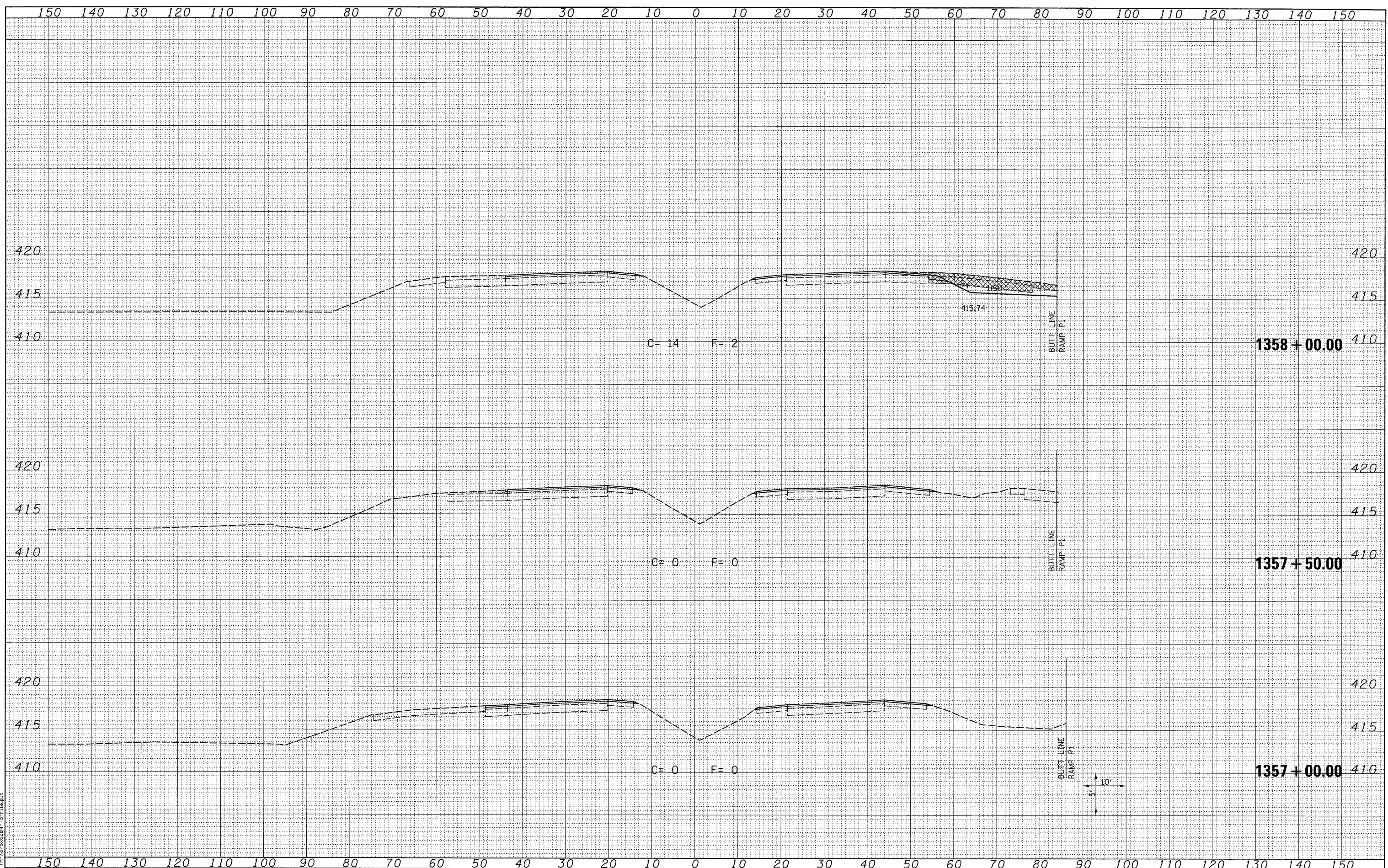


CROSS SECTIONS
 ILLINOIS ROUTE 3
 SCALE: SEE SHT SHEET NO. 3 OF 15 SHEETS STA. 1355+50.00 TO STA. 1356+50.00

| | | | | |
|---------------------------|----------|---------|--------------|--------------------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 154 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |

| | | | |
|---------------|----------|----|------|
| FINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK NO. | PLOTTED | | |
| AREAS CHECKED | TEMPLATE | | |

| | | | |
|-----------------|----------|----|------|
| ORIGINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK NO. | PLOTTED | | |
| AREAS CHECKED | TEMPLATE | | |



LAST SAVED = 3/16/2010
 PEN TABLE = V6.H4
 PLOT DRIVER = TR: Xerox6204- To: File.plt

FILE NAME =
 \\0906600\0906601\cad\pl\088_0876087-Sht-XSSht-IL3.dgn

USER NAME = sdonahue
 DESIGNED -
 DRAWN -
 CHECKED -
 PLOT DATE = 3/16/2010 4:44:17 PM

REVISIED -
 REVISIED -
 REVISIED -
 REVISIED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

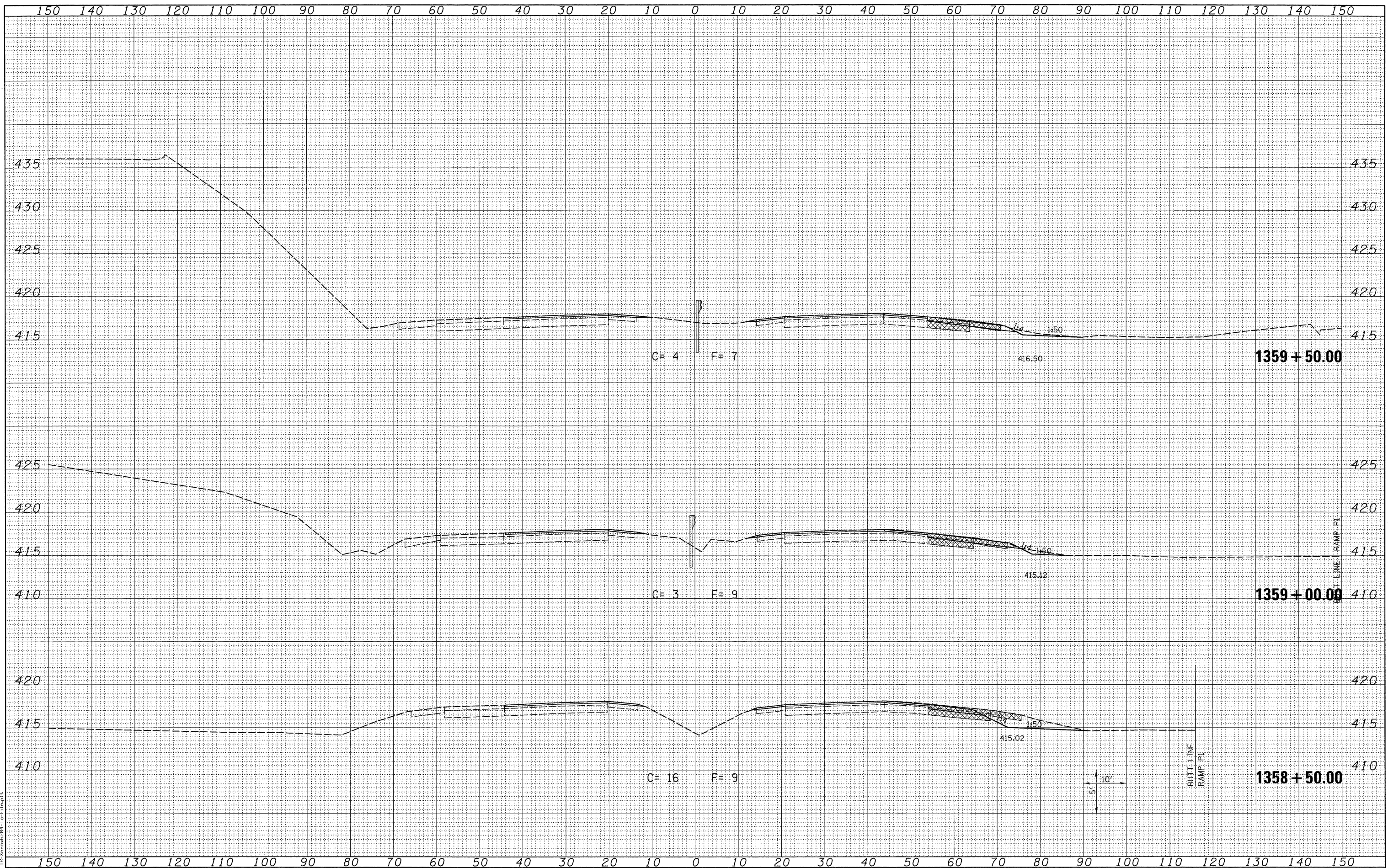
HORNER &
 SHIFRIN, INC.
 ENGINEERS

CROSS SECTIONS
 ILLINOIS ROUTE 3
 SCALE: SEE SHT SHEET NO. 4 OF 15 SHEETS STA. 1357+00.00 TO STA. 1358+00.00

| | | | | |
|---------------------------|----------|---------|--------------|--------------------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 155 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



LAST SAVED = 3/16/2010
 PEN TABLE = V8.B4
 PLOT DRIVER = TP-V806204-For-File.plt

FILE NAME =
 USER NAME = sdonahue
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISIED -
 REVISIED -
 REVISIED -
 REVISIED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

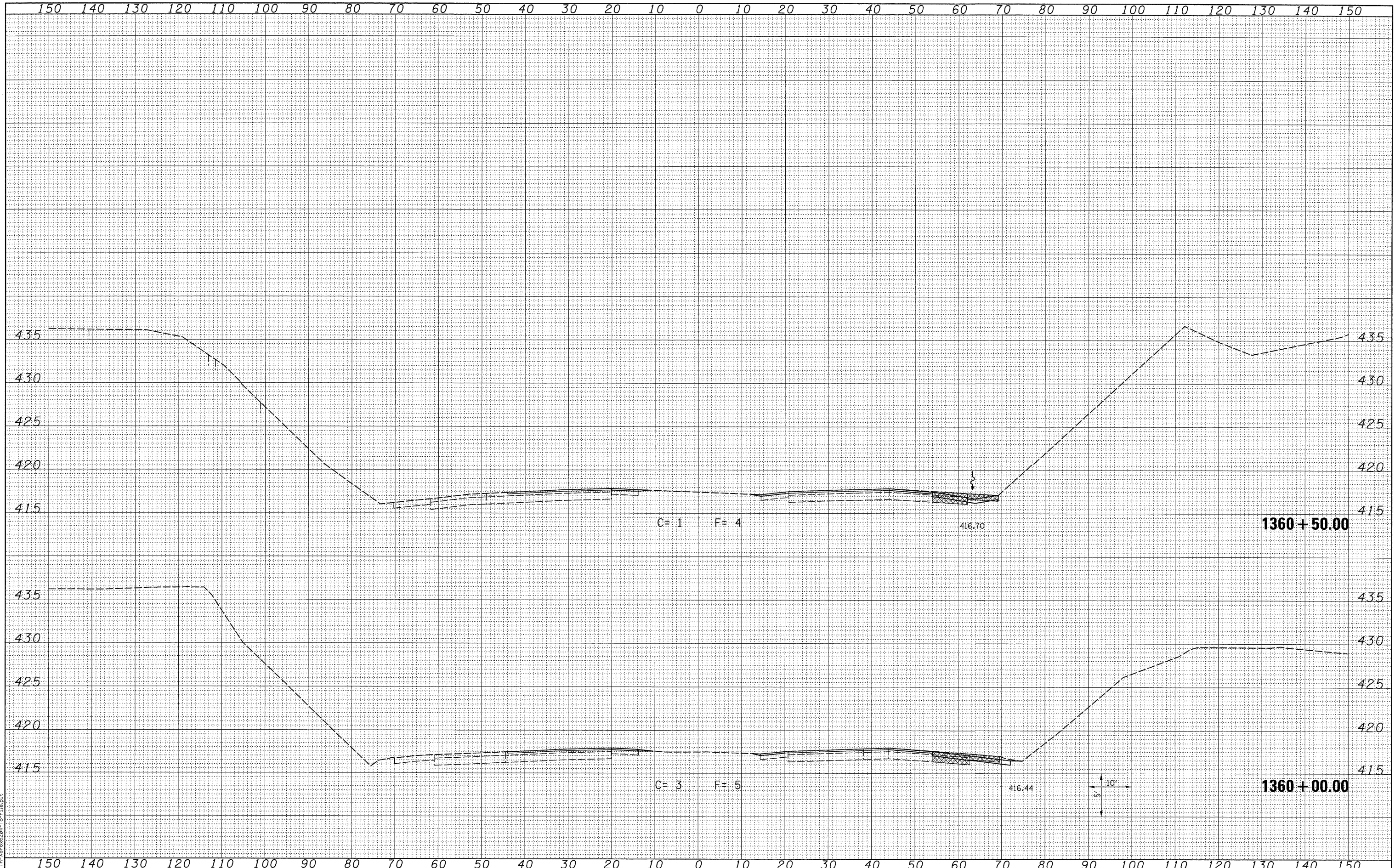
HORNER &
 SHIFRIN, INC.
 ENGINEERS

CROSS SECTIONS
 ILLINOIS ROUTE 3
 SCALE: SEE SHT
 SHEET NO. 5 OF 15 SHEETS
 STA. 1358+50.00 TO STA. 1359+50.00

| | | | | |
|--------------------|----------|---------|---------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 156 |
| CONTRACT NO. 76D87 | | | ILLINOIS FED. AID PROJECT | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



LAST SAVED = 3/16/2010
PEN TABLE = 08.041
PLOT DRIVER = TR: Xerox6204 To: File.plt

FILE NAME =
\\0906600\0906600\oad\c\plane\088_D876D87-Sht-XSht-IL3.dgn

USER NAME = sdonahue
DESIGNED -
DRAWN -
CHECKED -
DATE -

REVISIED -
REVISIED -
REVISIED -
REVISIED -

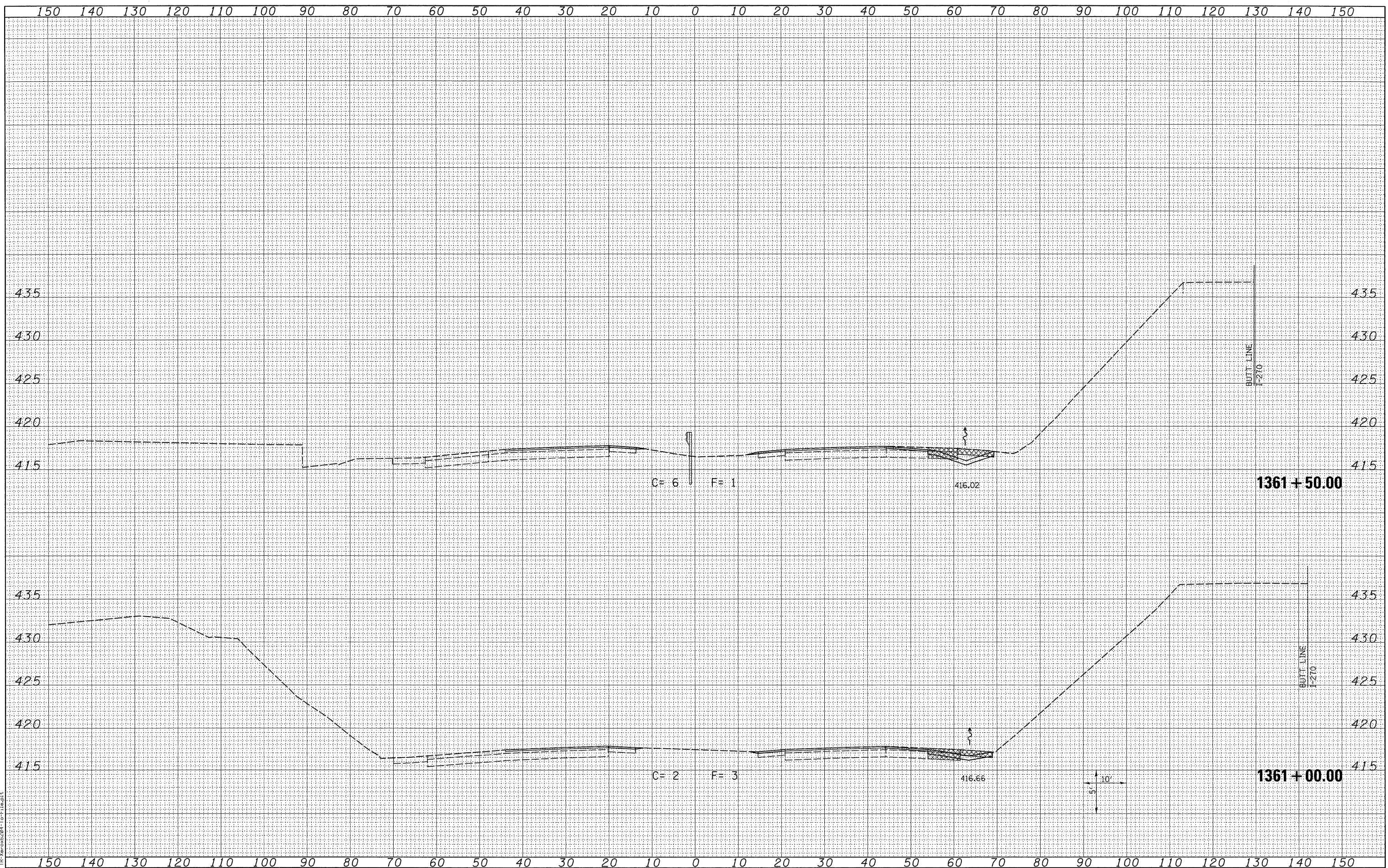
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HORNER &
SHIFRIN, INC
ENGINEERS

SCALE: SEE SHT
SHEET NO. 6 OF 15 SHEETS
STA. 1360+00.00 TO STA. 1360+50.00

CROSS SECTIONS
ILLINOIS ROUTE 3

| | | | | |
|---------------------------|---------------------|-------------------|---------------------|------------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 157 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



| | |
|-----------------|---------------|
| DATE | |
| BY | |
| ORIGINAL SURVEY | SURVEYED |
| NOTE BOOK | PLOTTED |
| NO. | TEMPLATE |
| | AREAS CHECKED |

| | |
|-----------------|---------------|
| DATE | |
| BY | |
| ORIGINAL SURVEY | SURVEYED |
| NOTE BOOK | PLOTTED |
| NO. | TEMPLATE |
| | AREAS CHECKED |

LAST SAVED = 2/16/2010
 PEN TABLE = V8.BA
 PLOT DRIVER = TR-Vax6204-For-Fileplot

FILE NAME =
 USER NAME = sdonahue
 DESIGNED -
 DRAWN -
 CHECKED -
 PLOT DATE = 3/16/2010 4:44:20 PM

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

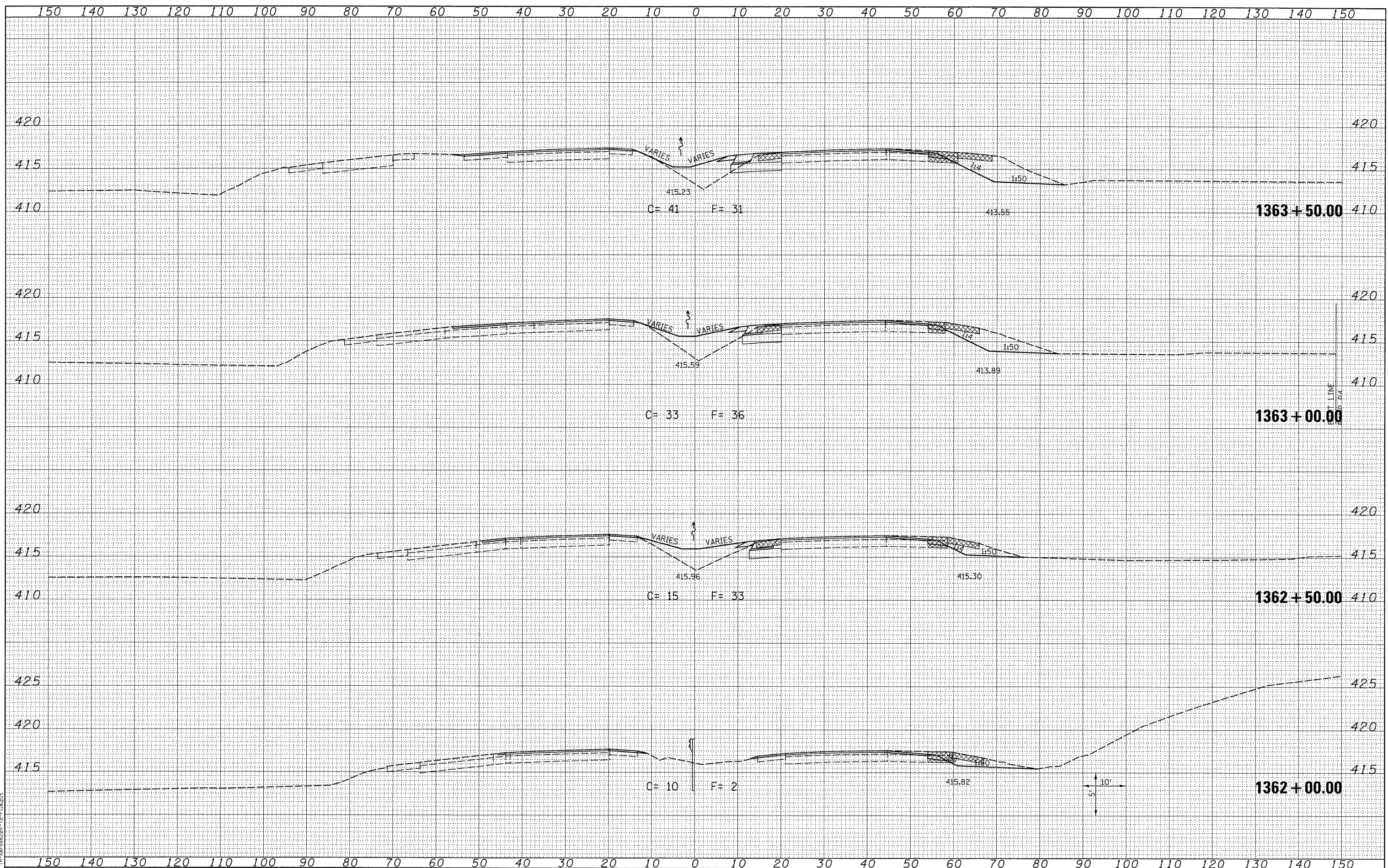


CROSS SECTIONS
 ILLINOIS ROUTE 3
 SCALE: SEE SHT SHEET NO. 7 OF 15 SHEETS STA. 1361+00.00 TO STA. 1361+50.00

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|---------|--------------|--------------------|
| 270 | 60-2RS-3 | MADISON | 231 | 158 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|------|
| BY | DATE |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|---------------|------|
| BY | DATE |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



LAST SAVED = 3/16/2010
 PEN TABLE = VB-HH
 PLOT DRIVER = TR-Xerox6204-To-File.plt

FILE NAME =
 USER NAME = sdonehue
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISOR -
 REVISION -
 REVISION -
 REVISION -

REVISOR -
 REVISION -
 REVISION -
 REVISION -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HORNER &
 SHIFRIN, INC.
 ENGINEERS

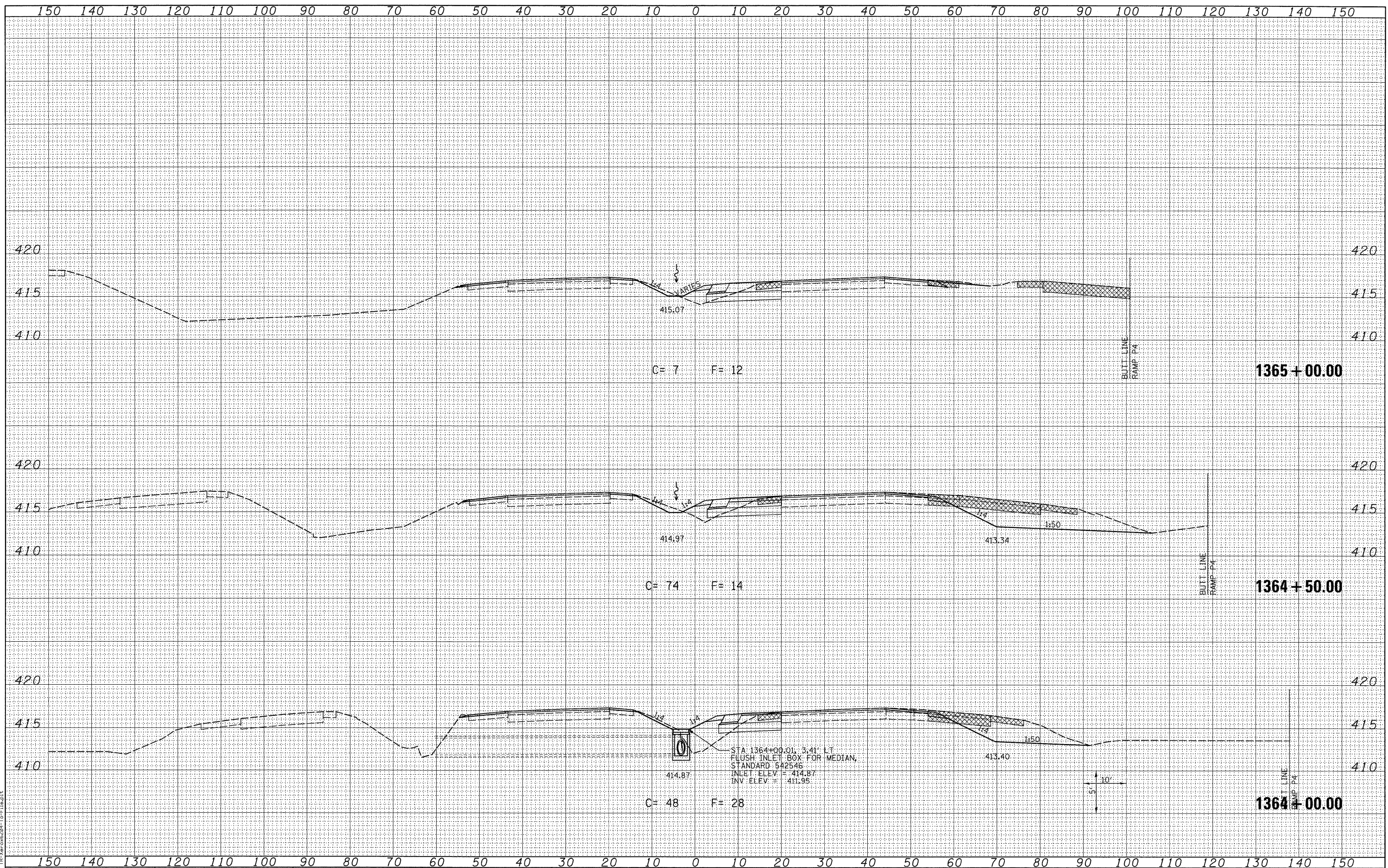
CROSS SECTIONS
 ILLINOIS ROUTE 3
 SCALE: SEE SHT
 SHEET NO. 8 OF 15 SHEETS
 STA. 1362+00.00 TO STA. 1363+50.00

| | | | | |
|---------------------------|----------|---------|--------------|--------------------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 159 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |

| | | |
|--------------|---------------|------|
| FINAL SURVEY | SURVEYED | DATE |
| NO. | PLOTTED | |
| | TEMPLATE | |
| | NOTE BOOK | |
| | AREAS CHECKED | |

| | | |
|-----------------|---------------|------|
| ORIGINAL SURVEY | SURVEYED | DATE |
| NO. | PLOTTED | |
| | TEMPLATE | |
| | NOTE BOOK | |
| | AREAS CHECKED | |

LAST SAVED = 3/16/2010
 PEN TABLE = V8.04
 PLOT DRIVER = TR:\xerox6204-TopFile.plt



| | | | |
|--|----------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - |
| 1:\0906600\0906601\cad\1_plans\088_0876087-Sht-XSSht-IL3.dgn | | DRAWN - | REVISED - |
| | | CHECKED - | REVISED - |
| | | DATE - | REVISED - |

| | |
|----------------------------------|--|
| PLOT SCALE = 10.0005' / IN. | |
| PLOT DATE = 3/16/2010 4:44:23 PM | |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HORNER &
 SHIFRIN, INC.
 ENGINEERS

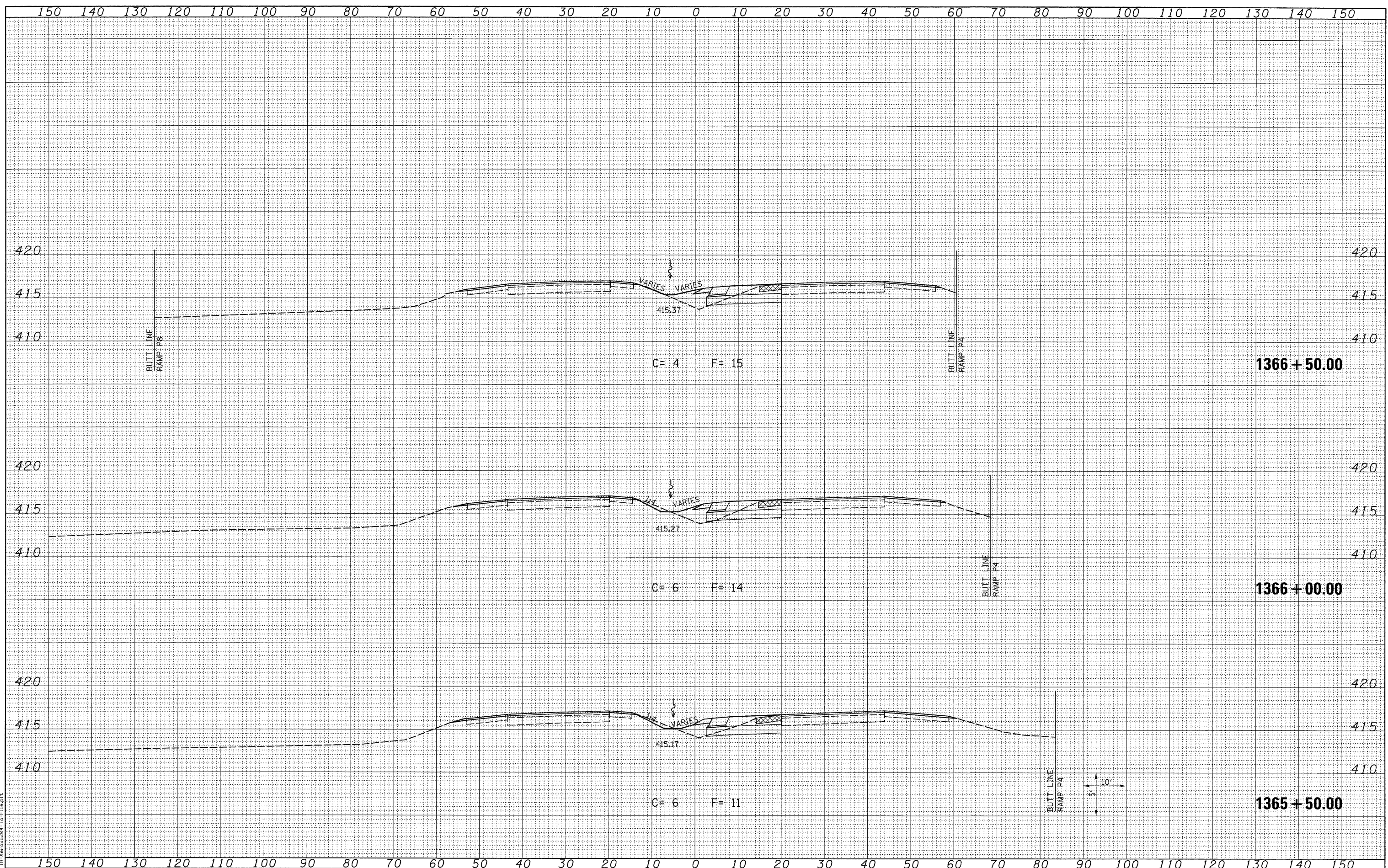
CROSS SECTIONS
 ILLINOIS ROUTE 3

SCALE: SEE SHT SHEET NO. 9 OF 15 SHEETS STA. 1364+00.00 TO STA. 1365+00.00

| | | | | |
|--------------------|----------|---------|---------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 160 |
| CONTRACT NO. 76D87 | | | ILLINOIS FED. AID PROJECT | |

| | |
|----|------|
| BY | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

| | |
|----|------|
| BY | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |



LAST SAVED = 3/16/2010
 PEN TABLE = 18.161
 PLOT DRIVER = TR:Ver6294-10-File.plt

| | |
|--|----------------------|
| FILE NAME = | USER NAME = sdonehue |
| 1:\0906600\0906601\cad\1-plans\088_0876087-Sht-XSSht-IL3.dgn | DESIGNED - |
| | DRAWN - |
| | CHECKED - |
| | DATE - |

| | |
|----------------------------------|------------|
| PLOT SCALE = 10.0005' / IN. | REVISIED - |
| PLOT DATE = 3/16/2010 4:44:24 PM | REVISIED - |
| | REVISIED - |

| | |
|------------|------------|
| DESIGNED - | REVISIED - |
| DRAWN - | REVISIED - |
| CHECKED - | REVISIED - |
| DATE - | REVISIED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**HORNER &
SHIFRIN, INC
ENGINEERS**

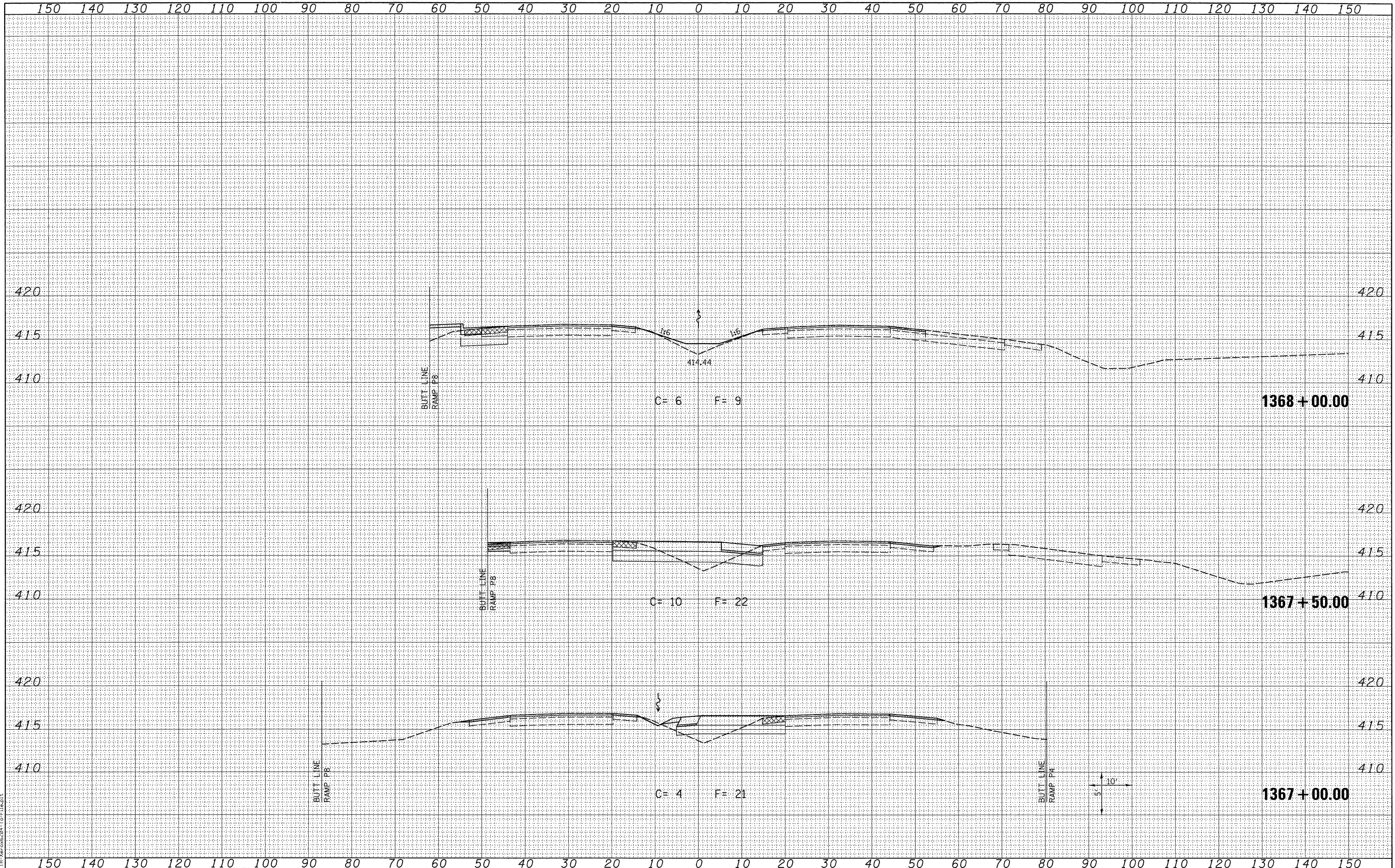
**CROSS SECTIONS
ILLINOIS ROUTE 3**

SCALE: SEE SHT SHEET NO. 10 OF 15 SHEETS STA. 1365+50.00 TO STA. 1366+50.00

| | | | | |
|---------------------------|------------------|----------------|------------------|--------------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 161 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|-----------------|--|
| DATE | |
| BY | |
| ORIGINAL SURVEY | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



LAST SAVED = 2/16/2010
 PEN TABLE = V8.tbl
 PLOT DRIVER = TR:Veroc6204-10-File.plt

FILE NAME = \\09066600\09066601\cad\l\plane\088_0876087-Sht-XSSht-IL3.dgn
 USER NAME = sdonahue

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

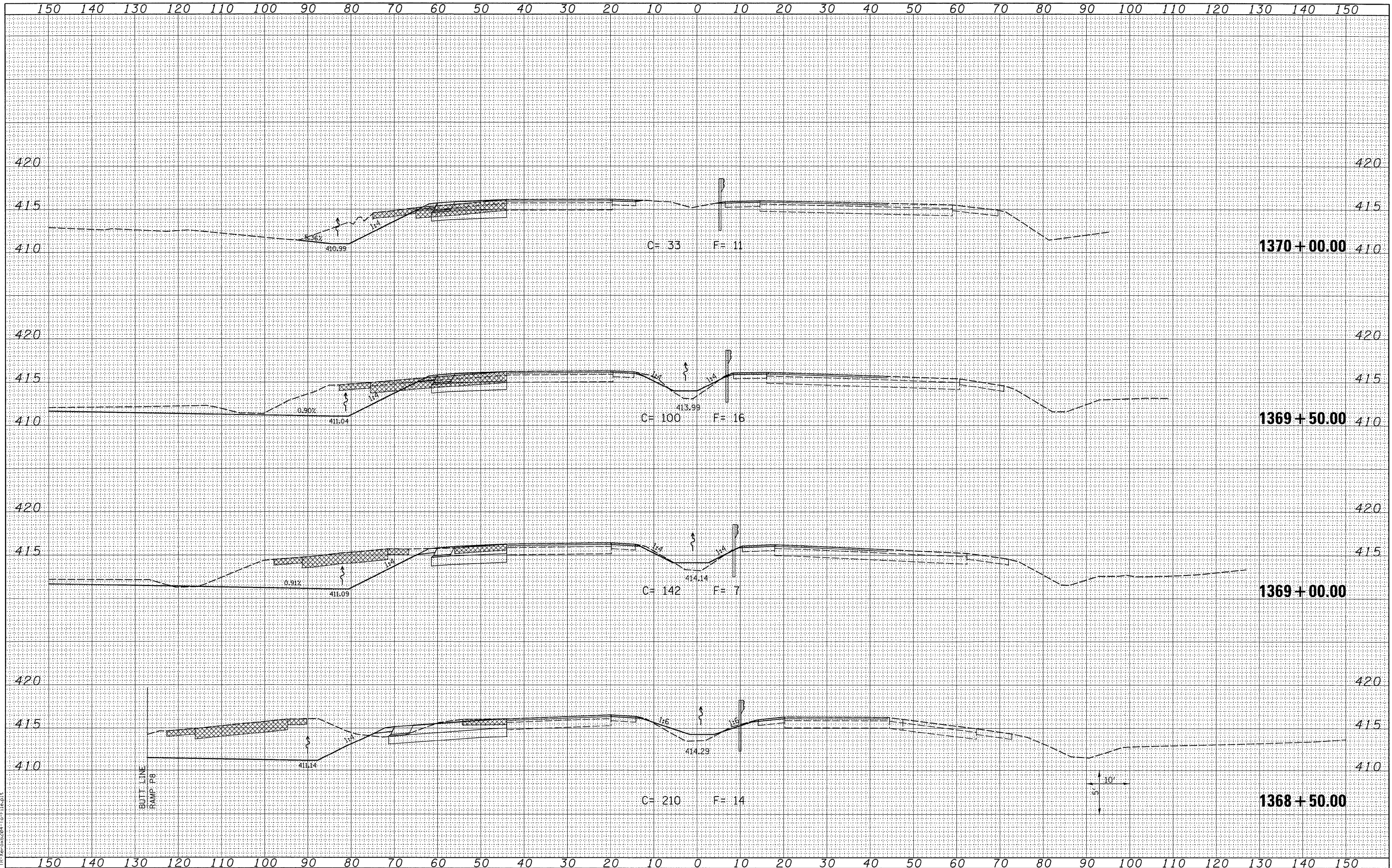
REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HORNER &
 SHIFRIN, INC
 ENGINEERS

CROSS SECTIONS
 ILLINOIS ROUTE 3
 SCALE: SEE SHT SHEET NO. 11 OF 15 SHEETS STA. 1367+00.00 TO STA. 1368+00.00

| | | | | |
|--------------------|----------|---------|---------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 162 |
| CONTRACT NO. 76D87 | | | ILLINOIS FED. AID PROJECT | |



| | |
|---------------|--|
| DATE | |
| BY | |
| FINAL SURVEY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|-----------------|--|
| DATE | |
| BY | |
| ORIGINAL SURVEY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

LAST SAVED = 3/16/2010
 PEN TABLE = V6.161
 PLOT DRIVER = TR-V606204-TorFile.plt

| | | | |
|--|----------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - |
| 1:\0906600\0906601\oad\1_plans\088.DB76D87-Sht-XSSht-IL3.dgn | | DRAWN - | REVISED - |
| | | CHECKED - | REVISED - |
| | | DATE - | REVISED - |

| | | |
|----------------------------------|-----------|-----------|
| PLOT SCALE = 10.0005' / IN. | CHECKED - | REVISED - |
| PLOT DATE = 3/16/2010 4:44:26 PM | DATE - | REVISED - |

| | |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

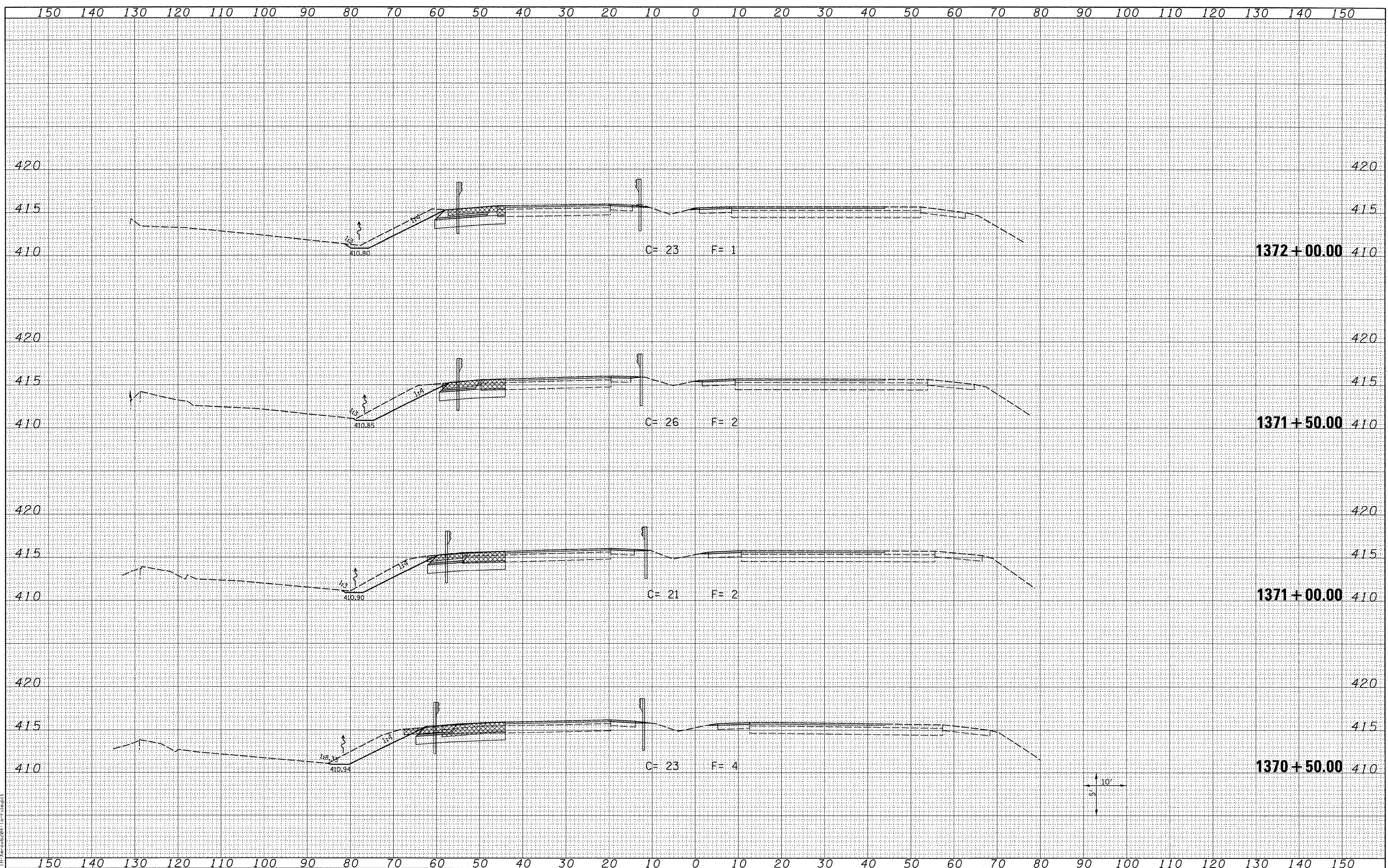
HORNER &
 SHIFRIN, INC.
 ENGINEERS

CROSS SECTIONS
 ILLINOIS ROUTE 3
 SCALE: SEE SHT SHEET NO. 12 OF 15 SHEETS STA. 1368+50.00 TO STA. 1370+00.00

| | | | | |
|---------------------------|----------|---------|--------------|--------------------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 163 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|--------------|---------------|
| DATE | |
| BY | |
| FINAL SURVEY | SURVEYED |
| NOTE BOOK | PLOTTED |
| NO. | TEMPLATE |
| | AREAS CHECKED |

| | |
|-----------------|---------------|
| DATE | |
| BY | |
| ORIGINAL SURVEY | SURVEYED |
| NOTE BOOK | PLOTTED |
| NO. | TEMPLATE |
| | AREAS CHECKED |



LAST SAVED = 3/16/2010
 PEN TABLE = W8.5x1
 PLOT DRIVER = IR-Vers6204-10-File.plt

FILE NAME =
 USER NAME = sdonahue
 DESIGNED -
 DRAWN -
 CHECKED -
 PLOT DATE = 3/16/2010 4:44:27 PM

| | |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

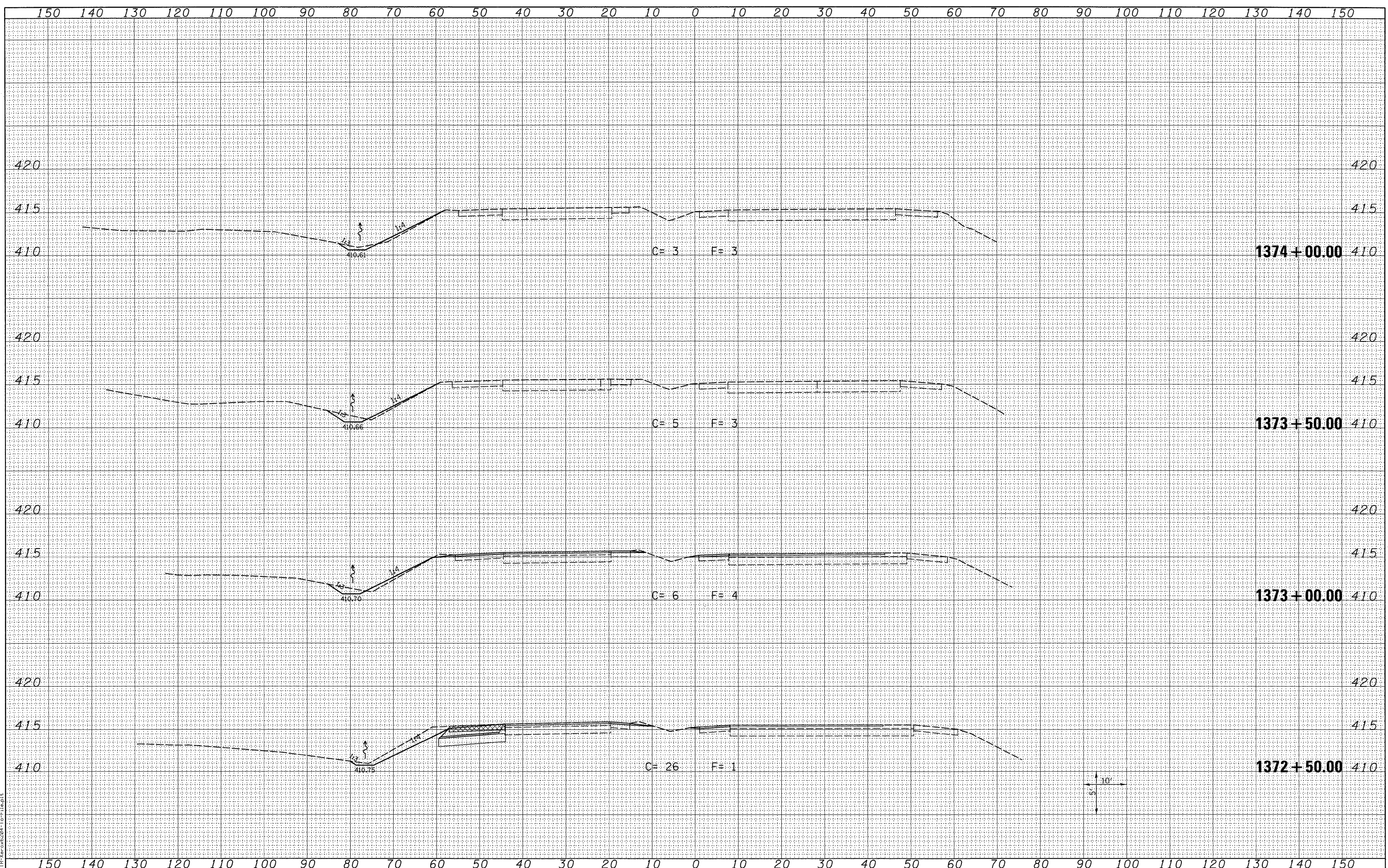
HORNER &
 SHIFRIN, INC
 ENGINEERS

CROSS SECTIONS
 ILLINOIS ROUTE 3
 SCALE: SEE SHT SHEET NO. 13 OF 15 SHEETS STA. 1370+50.00 TO STA. 1372+00.00

| | | | | |
|--------------------|----------|---------|---------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 164 |
| CONTRACT NO. 76D87 | | | ILLINOIS FED. AID PROJECT | |

| | |
|--------------|---------------|
| DATE | |
| BY | |
| FINAL SURVEY | SURVEYED |
| NOTE BOOK | PLOTTED |
| NO. | TEMPLATE |
| | AREAS CHECKED |

| | |
|-----------------|---------------|
| DATE | |
| BY | |
| ORIGINAL SURVEY | SURVEYED |
| NOTE BOOK | PLOTTED |
| NO. | TEMPLATE |
| | AREAS CHECKED |



LAST SAVED = 3/16/2010
 PEN TABLE = V8.dwg
 PLOT DRIVER = TR-Verod6204-1or-File.plt

| | |
|---|----------------------|
| FILE NAME = | USER NAME = sdonahue |
| \\0906600\0906601\ced\l_plans\088_D876D87-Sht-X5Sht-IL3.dgn | DESIGNED - |
| | DRAWN - |
| | CHECKED - |
| | DATE - |

| | |
|------------|------------|
| DESIGNED - | REVISIED - |
| DRAWN - | REVISIED - |
| CHECKED - | REVISIED - |
| DATE - | REVISIED - |

| | |
|------------|------------|
| DESIGNED - | REVISIED - |
| DRAWN - | REVISIED - |
| CHECKED - | REVISIED - |
| DATE - | REVISIED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION



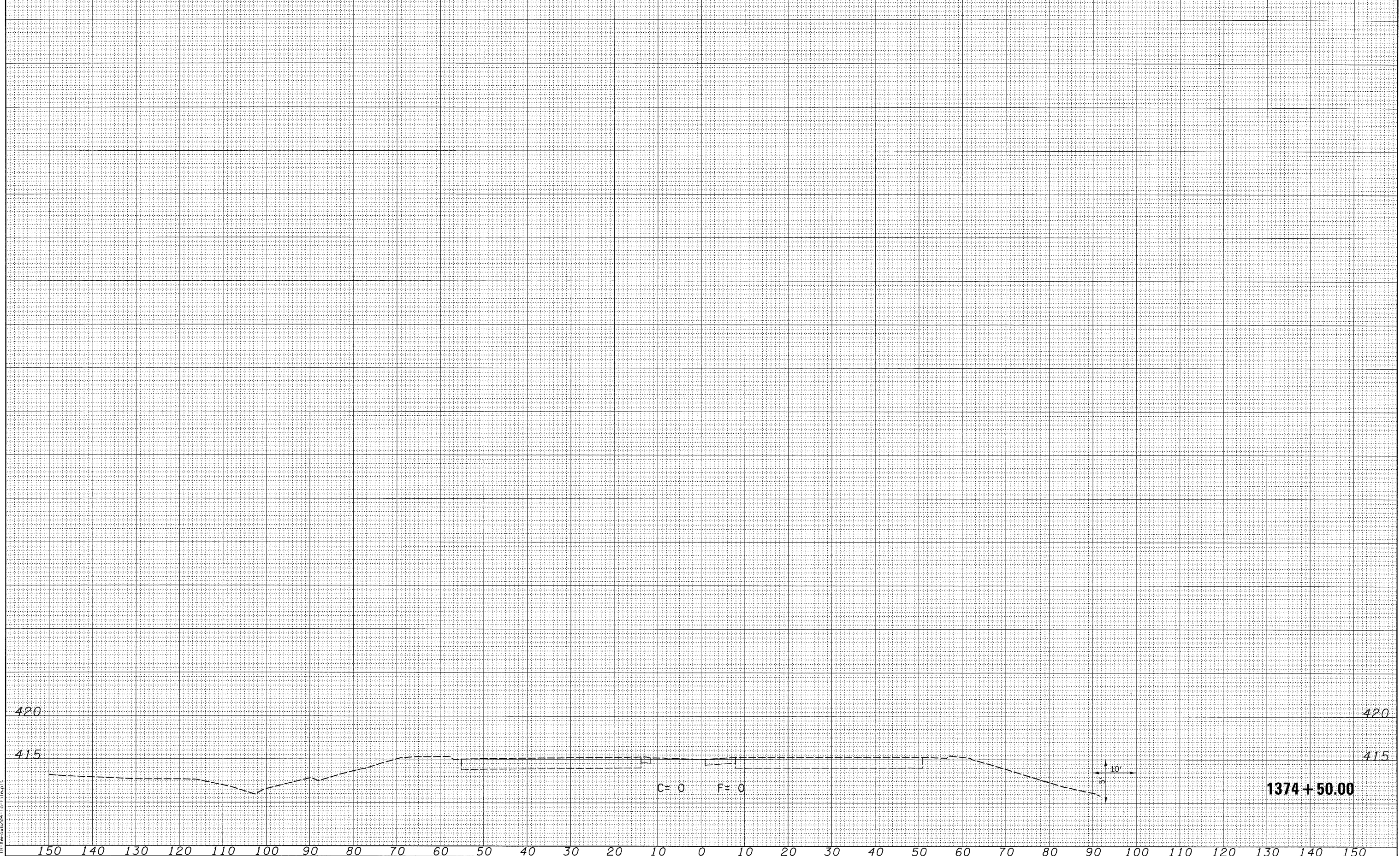
CROSS SECTIONS
 ILLINOIS ROUTE 3
 SCALE: SEE SHT SHEET NO. 14 OF 15 SHEETS STA. 1372+50.00 TO STA. 1374+00.00

| | | | | |
|--------------------|------------------|----------------|---------------------------|---------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 165 |
| CONTRACT NO. 76D87 | | | ILLINOIS FED. AID PROJECT | |

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

| | | |
|---------------|---------------|------|
| FINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK NO. | PLOTTED | |
| | TEMPLATE | |
| | AREAS CHECKED | |

| | | |
|-----------------|---------------|------|
| ORIGINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK NO. | PLOTTED | |
| | TEMPLATE | |
| | AREAS CHECKED | |



LAST SAVED = 3/16/2010
 PEN TABLE = V8.161
 PLOT DRIVER = TR:Veroc6204-1a-File.plt

FILE NAME =
 J:\0906600\0906601\cad\plots\088_0876087-Sht-XSht-IL3.dgn

USER NAME = sdonehue
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISOR -
 REVISIONS -
 REVISIONS -
 REVISIONS -

PLOT SCALE = 10.0005' / IN.
 PLOT DATE = 3/16/2010 4:44:29 PM

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HORNER &
 SHIFRIN, INC.
 ENGINEERS

CROSS SECTIONS
 ILLINOIS ROUTE 3
 SCALE: SEE SHT SHEET NO. 15 OF 15 SHEETS STA. 1374+50.00 TO STA. 1374+50.00

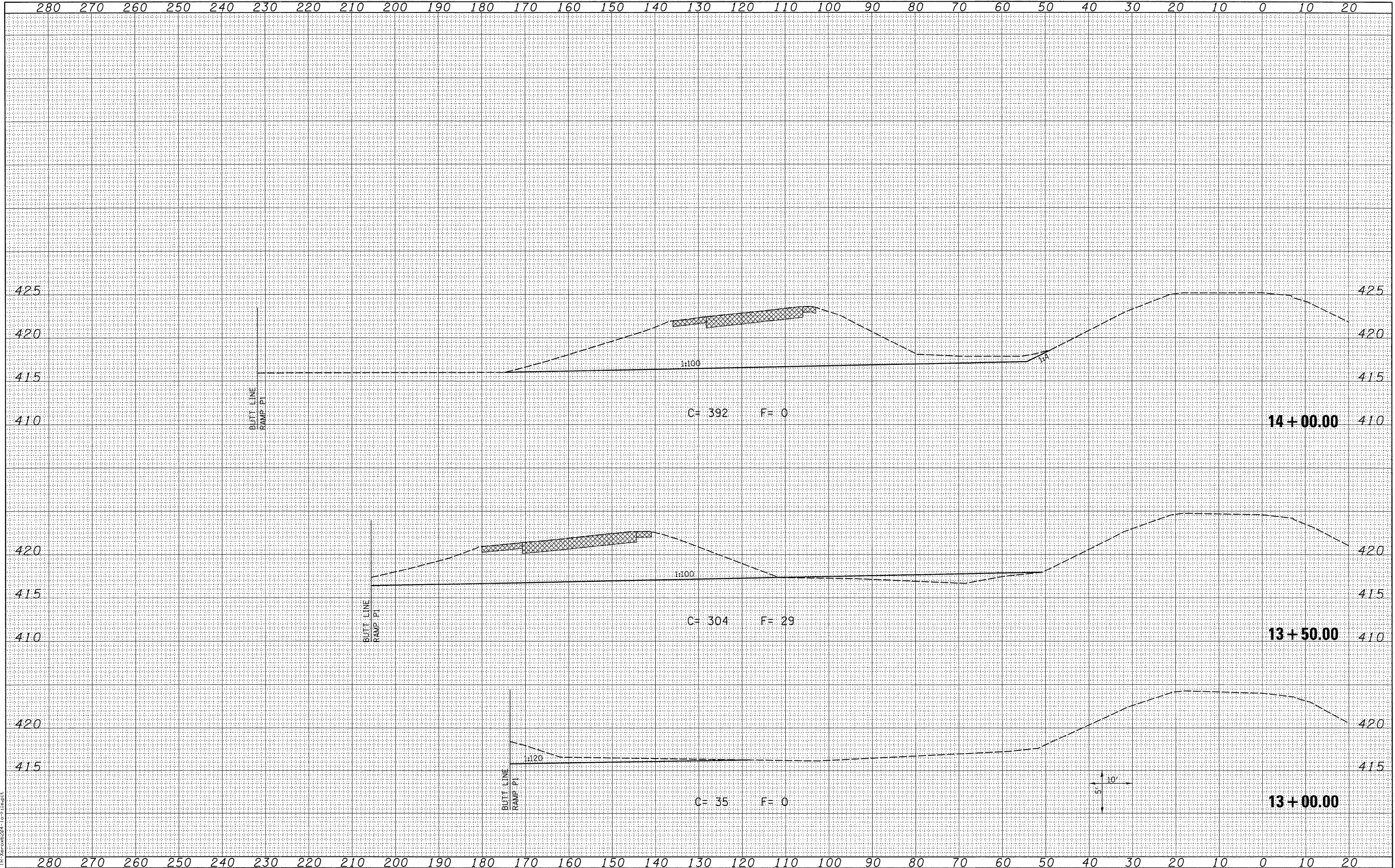
| | | | | |
|---------------------------|------------------|----------------|------------------|---------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 166 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

| | |
|----|------|
| BY | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

| | |
|----|------|
| BY | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

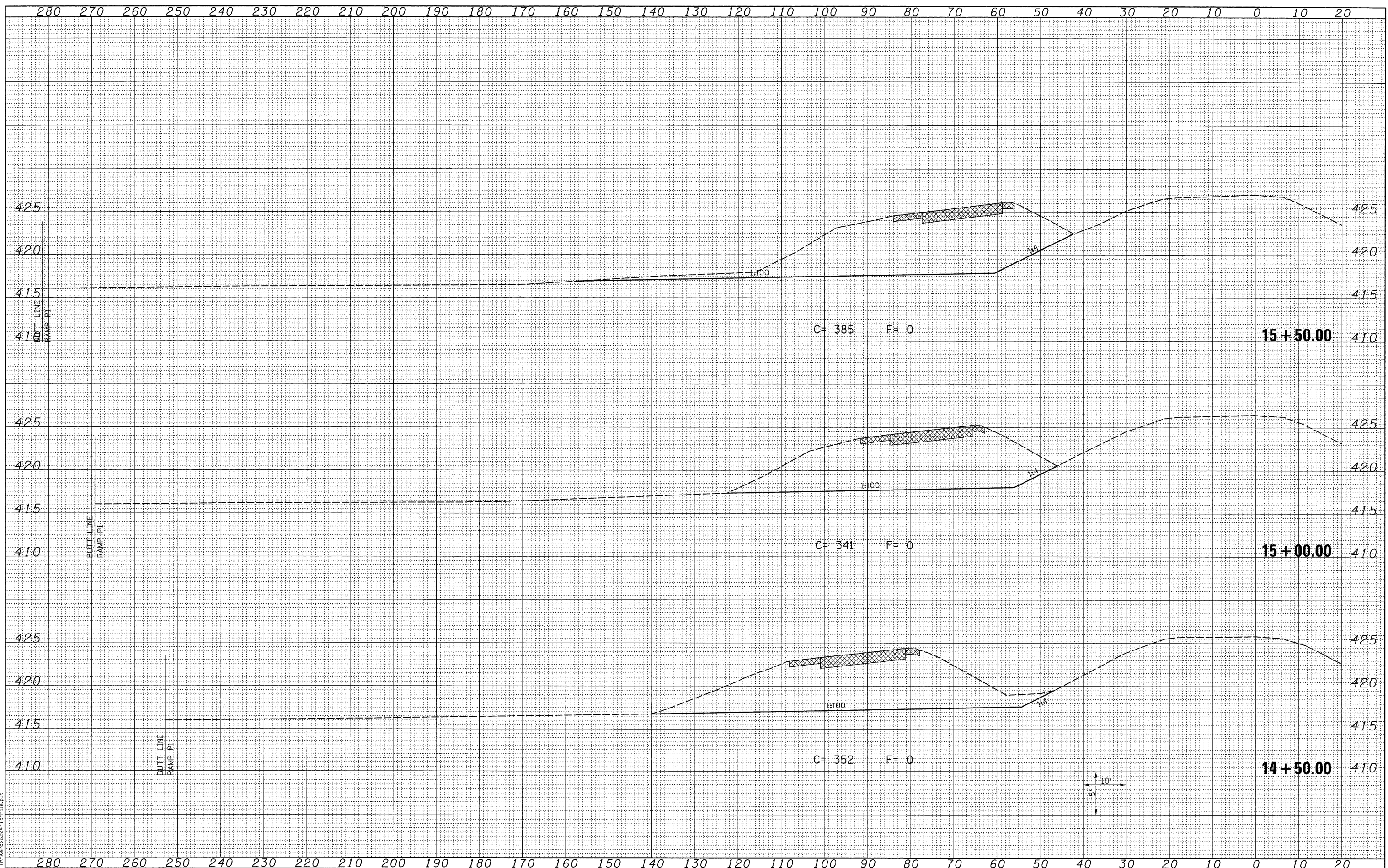
LAST SAVED = 3/13/2010
 PEN TABLE = V6.141
 PLOT DRIVER = TR-Vecor6204-For-File.plt



| | | | | | | | | | | | | |
|---|----------------------|------------|-----------|---|--|---|--|---------------------------|----------|---------|--------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | | CROSS SECTIONS EXISTING RAMP E1 | | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | DRAWN - | REVISED - | | | | | 270 | 60-2RS-3 | MADISON | 231 | 167 |
| | | CHECKED - | REVISED - | | | | | CONTRACT NO. 76D87 | | | | |
| | | DATE - | REVISED - | | | | | ILLINOIS FED. AID PROJECT | | | | |
| SCALE: SEE SHT SHEET NO. 1 OF 5 SHEETS STA. 13+00.00 TO STA. 14+00.00 | | | | | | | | | | | | |

| | |
|---------------|------|
| BY | DATE |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| NO. | |
| AREAS CHECKED | |

| | |
|---------------|------|
| BY | DATE |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| NO. | |
| AREAS CHECKED | |



LAST SAVED = 3/13/2010
 PEN TABLE = 1/8,1/4
 PLOT DRIVER = TR:Veroc6284-10-F:leapl1

FILE NAME =
 USER NAME = sdonahue
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -
 PLOT SCALE = 10.0025 ' / IN.
 PLOT DATE = 3/16/2010 4:44:32 PM

DESIGNED -
 REVISIONS -
 REVISIONS -
 REVISIONS -
 REVISIONS -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



**CROSS SECTIONS
 EXISTING RAMP E1**

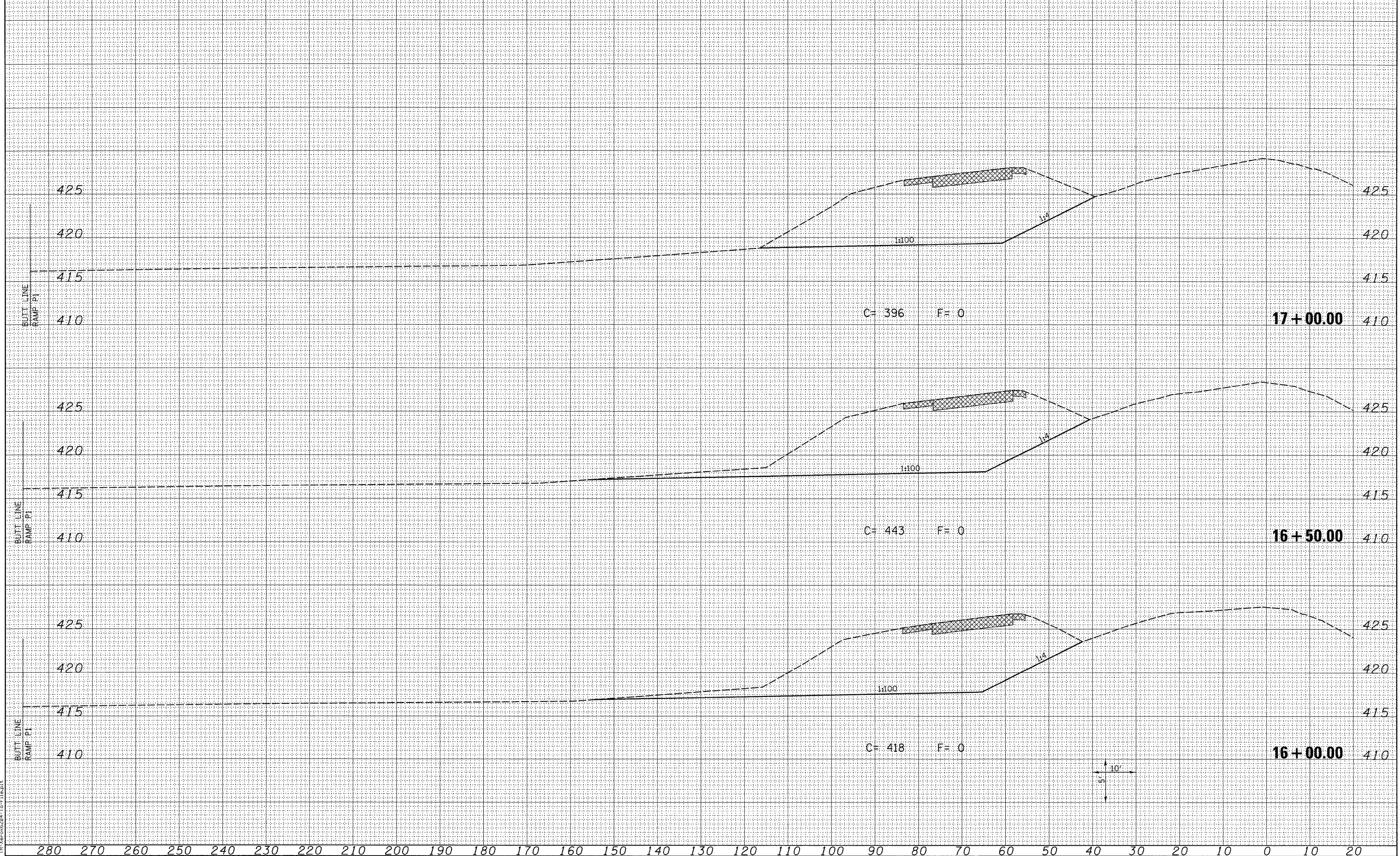
SCALE: SEE SHT SHEET NO. 2 OF 5 SHEETS STA. 14+50.00 TO STA. 15+50.00

| | | | | |
|---------------------------|------------------|----------------|------------------|--------------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 168 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |

280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

| | |
|---------------|------|
| BY | DATE |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|---------------|------|
| BY | DATE |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



280 270 260 250 240 230 220 210 200 190 180 170 160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20

LAST SAVED = 3/13/2010
 PEN TABLE = 10,141
 PLOT DRIVER = TR-Xerox6204-CorFile.plt

| | |
|---|----------------------|
| FILE NAME = | USER NAME = adonahue |
| 1:\0906600\0906601\cad\1\plans\089_0876087-Sht-XSSht-RampE1.dgn | DESIGNED - |
| | DRAWN - |
| | CHECKED - |
| | DATE - |

| | |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - | REVISED - |

| |
|----------------------------------|
| PLOT SCALE = 10.0005' / IN. |
| PLOT DATE = 3/16/2010 4:44:33 PM |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

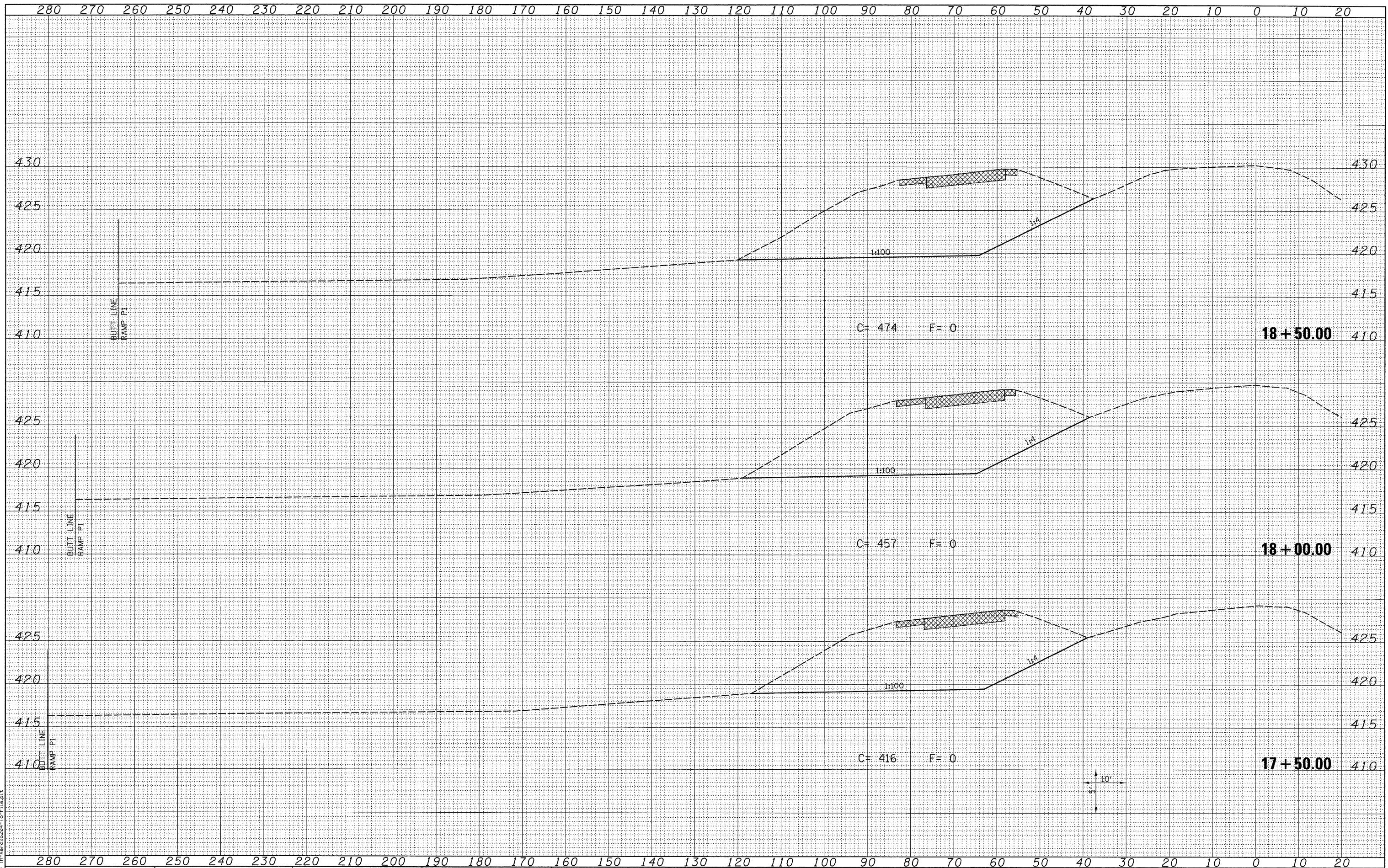
HORNER &
 SHIFRIN, INC.
 ENGINEERS

CROSS SECTIONS
 EXISTING RAMP E1
 SCALE: SEE SHT SHEET NO. 3 OF 5 SHEETS STA. 16+00.00 TO STA. 17+00.00

| | | | | |
|---------------------------|----------|---------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 169 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



LAST SAVED = 3/13/2010
 PEN TABLE = VA45
 PLOT DRIVER = IR-Micro6204-To-File.plt

| | | | |
|---|----------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - |
| 1:\09066600\09066601\cad\1\plans\089_0876087-Sht-XSSht-RampE1.dgn | | DRAWN - | REVISED - |
| | | CHECKED - | REVISED - |
| | | DATE - | REVISED - |
| | | | |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HORNER &
 SHIFRIN, INC.
 ENGINEERS

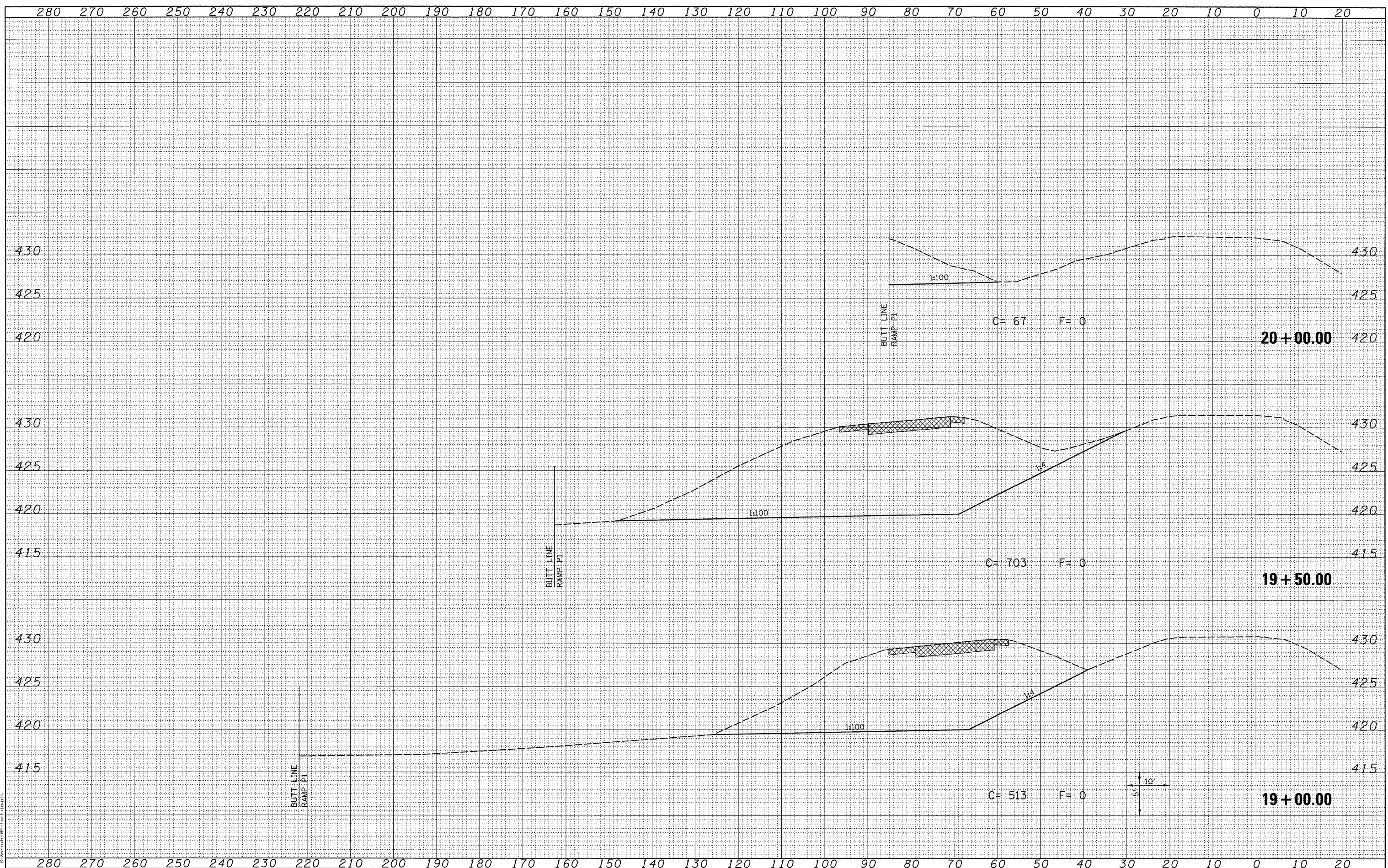
CROSS SECTIONS
 EXISTING RAMP E1

SCALE: SEE SHT SHEET NO. 4 OF 5 SHEETS STA. 17+50.00 TO STA. 18+50.00

| | | | | |
|---------------------------|----------|---------|--------------|--------------------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 170 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |

| | | |
|--------------|---------------|------|
| FINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | TEMPLATE | |
| | AREAS CHECKED | |

| | | |
|-----------------|---------------|------|
| ORIGINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | TEMPLATE | |
| | AREAS CHECKED | |



LAST SAVED = 2/13/2010
 PEN TABLE = 18.161
 PLOT DRIVER = IR:Verex6294-1-File.plt

| | |
|--|----------------------|
| FILE NAME = | USER NAME = sdonahue |
| \\09066620\09066601\cad\1\plans\089_0876087-Sht-XSSht-RampE1.dgn | DESIGNED - |
| | DRAWN - |
| | CHECKED - |
| | DATE - |

| | |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - | REVISED - |

| | |
|----------------------------------|-----------|
| PLLOT SCALE = 10.0005' / IN. | REVISIONS |
| PLOT DATE = 3/16/2010 4:44:35 PM | |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

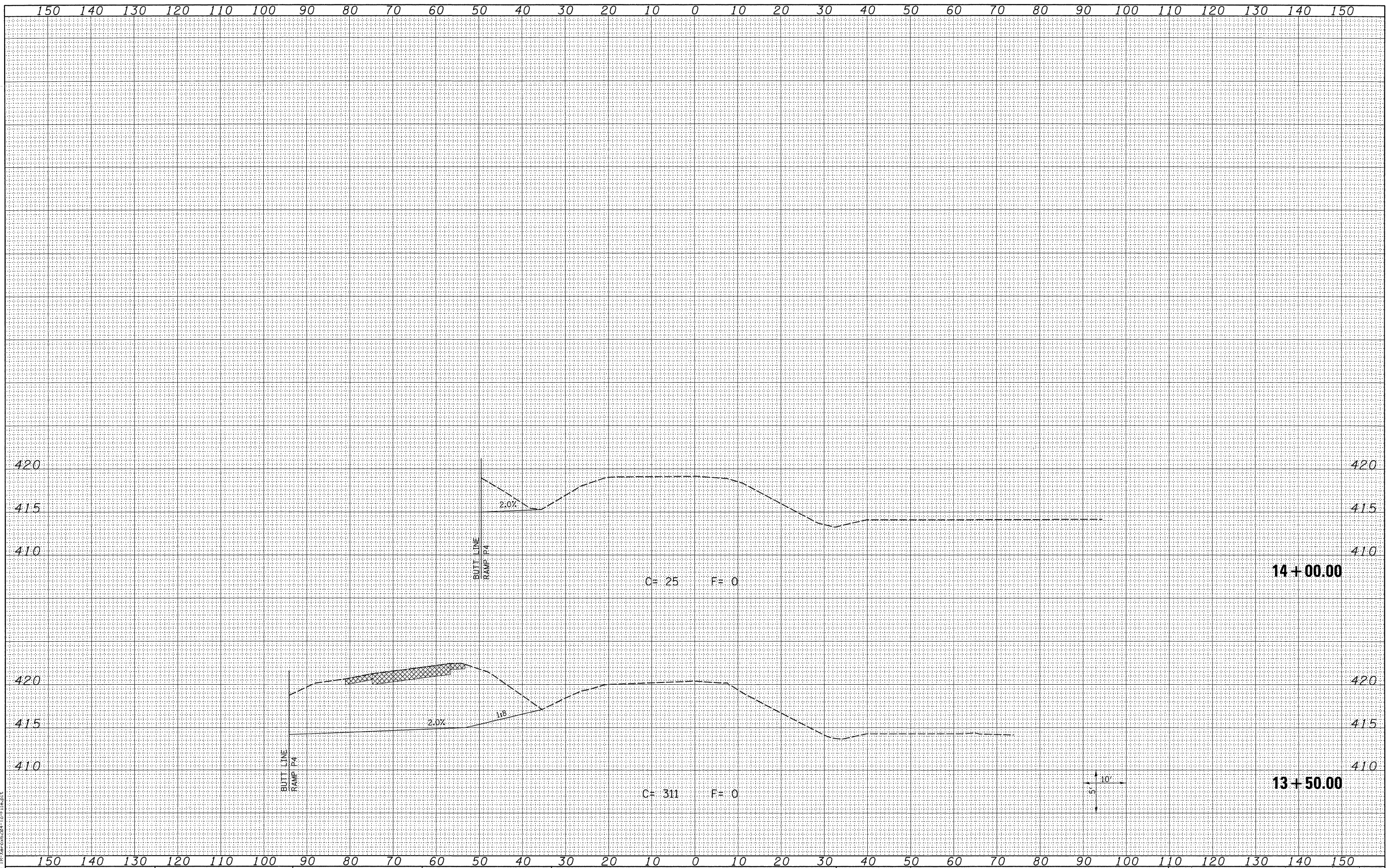
HORNER &
 SHIFRIN, INC.
 ENGINEERS

| | |
|------------------------------------|-------------------------|
| CROSS SECTIONS EXISTING RAMP E1 | |
| SCALE: SEE SHT | SHEET NO. 5 OF 5 SHEETS |
| STA. 19+00.00 TO STA. 20+00.00 | |

| | | | | |
|---------------------------|-------------------|-----------------|-------------------|----------------|
| F.A.I. RATE: 270 | SECTION: 60-2RS-3 | COUNTY: MADISON | TOTAL SHEETS: 231 | SHEET NO.: 171 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| | | |
|---------------|---------------|------|
| FINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK NO. | PLOTTED | |
| | TEMPLATE | |
| | AREAS CHECKED | |

| | | |
|-----------------|---------------|------|
| ORIGINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK NO. | PLOTTED | |
| | TEMPLATE | |
| | AREAS CHECKED | |



LAST SAVED = 3/13/2010
 PEN TABLE = V6.tbl
 PLOT DRIVER = JTP-V6065204-TP-File.plt

FILE NAME = \\0906600\0906601\cad\1_plans\090_D876087-Sht-XSSht-RampE4.dgn
 USER NAME = sdonehue

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

PLOT SCALE = 10.0005' / IN.
 PLOT DATE = 3/16/2010 4:44:39 PM

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

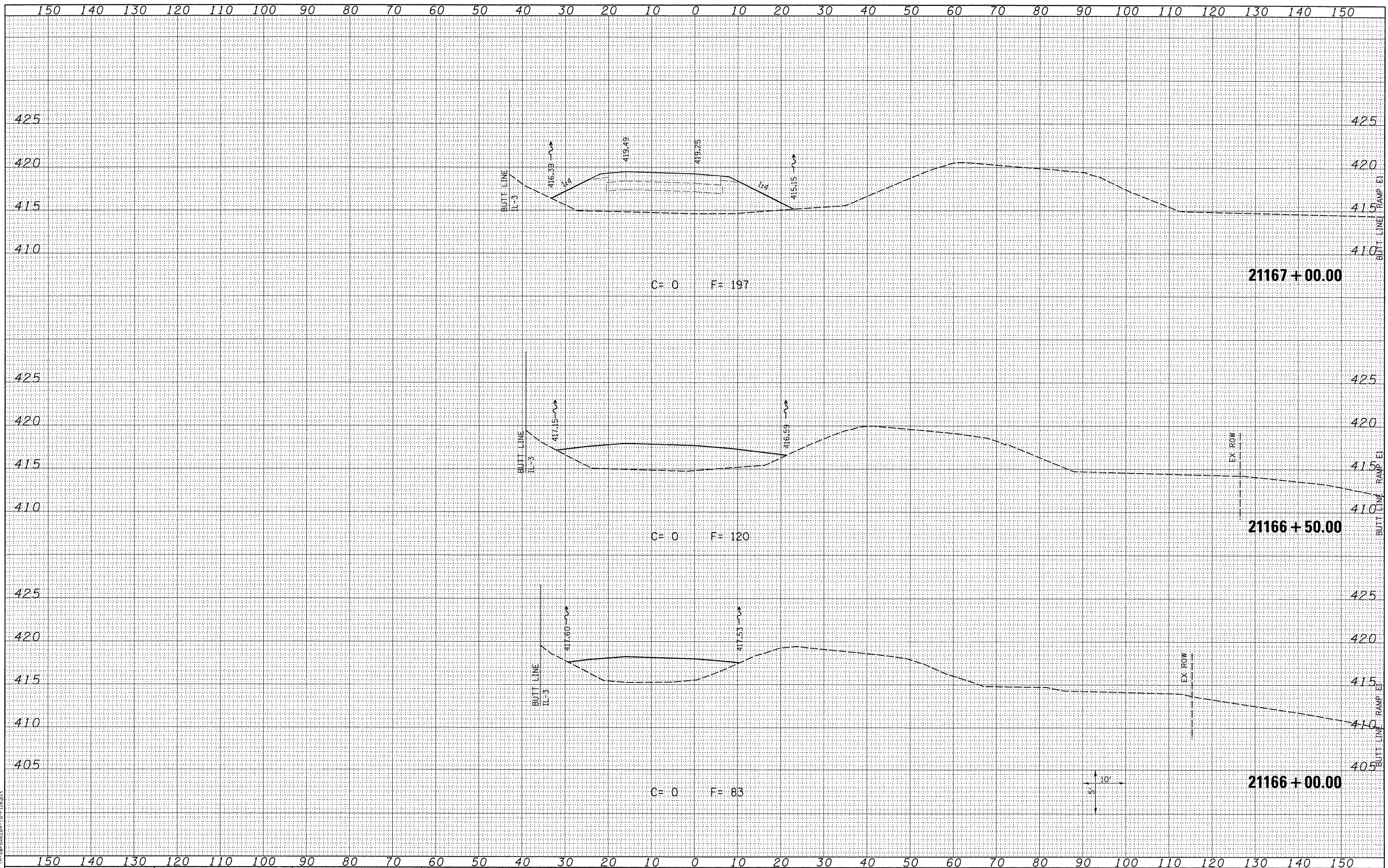
HORNER & SHIFRIN, INC.
 ENGINEERS

CROSS SECTIONS
 EXISTING RAMP E4
 SCALE: SEE SHT SHEET NO. 3 OF 3 SHEETS STA. 13+50.00 TO STA. 14+00.00

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 174 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



LAST SAVED = 3/16/2010
 PEN TABLE = VBA41
 PLOT DRIVER = TR-Xerox6204-ToFile.plt

| | |
|--|----------------------|
| FILE NAME = | USER NAME = sdonahue |
| 1:\09066800\09066801\cad\1\plans\091.LD876DB7-Sh | XSSht-RampP1.dgn |
| PLOT SCALE = 10.0005' / IN. | CHECKED - |
| PLOT DATE = 3/16/2010 4:44:42 PM | DATE - |

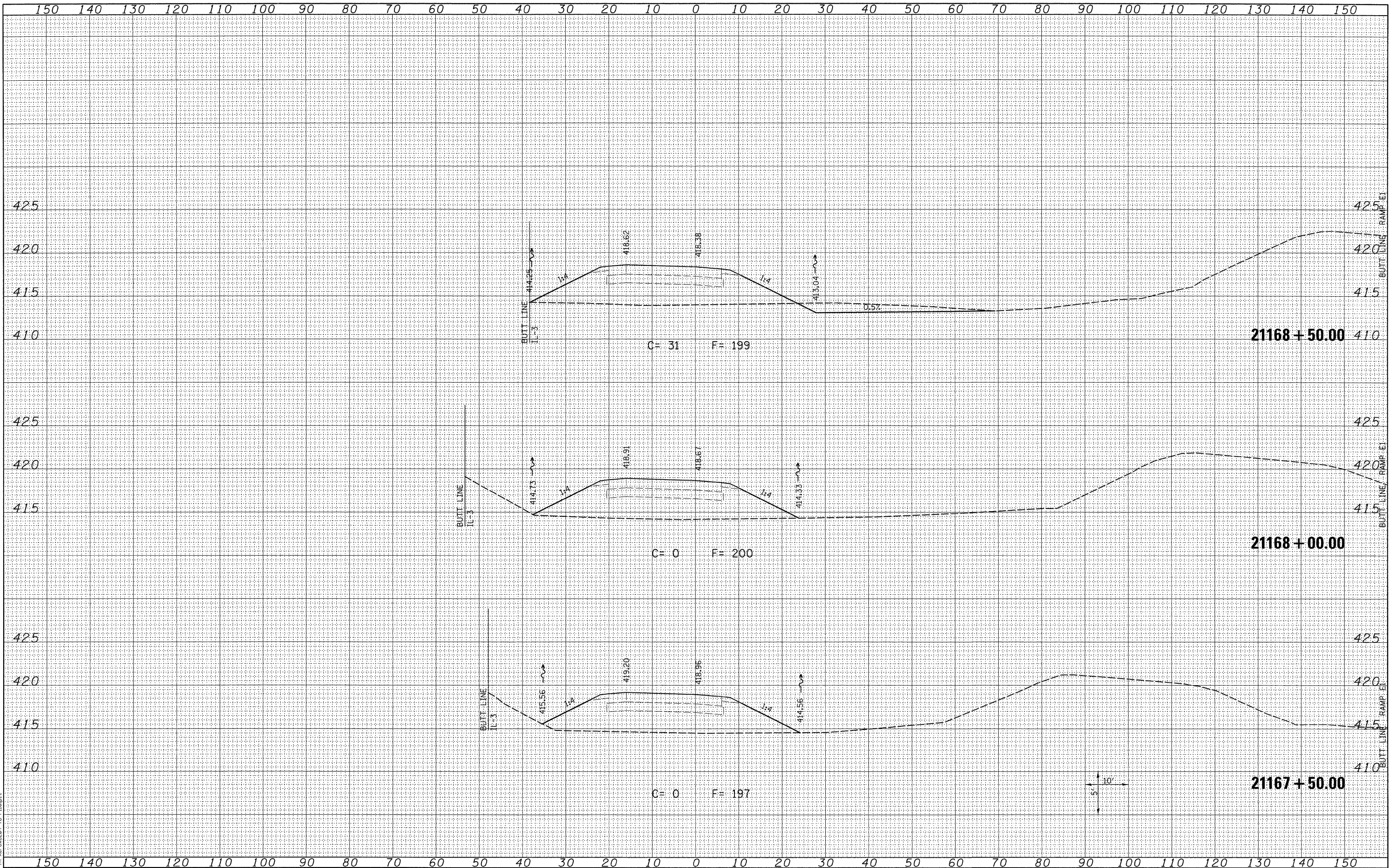
| | |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION



CROSS SECTIONS
 PROPOSED RAMP P1
 SCALE: SEE SHT
 SHEET NO. 1 OF 11 SHEETS
 STA. 21166+00.00 TO STA. 21167+00.00

| | | | | |
|---------------------------|----------|---------|--------------|--------------------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 175 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |



| BY | DATE |
|----|------|
| | |
| | |
| | |

| BY | DATE |
|----|------|
| | |
| | |
| | |

FILE NAME = 3/16/2010
 USER NAME = sdonahue
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -
 PLOT SCALE = 10.0005' / IN.
 PLOT DATE = 3/16/2010 4:44:43 PM

| | |
|------------|------------|
| DESIGNED - | REVISIED - |
| DRAWN - | REVISIED - |
| CHECKED - | REVISIED - |
| DATE - | REVISIED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HORNER & SHIFRIN, INC.
ENGINEERS

CROSS SECTIONS
 PROPOSED RAMP P1

SCALE: SEE SHT SHEET NO. 2 OF 11 SHEETS STA. 21167+50.00 TO STA. 21168+50.00

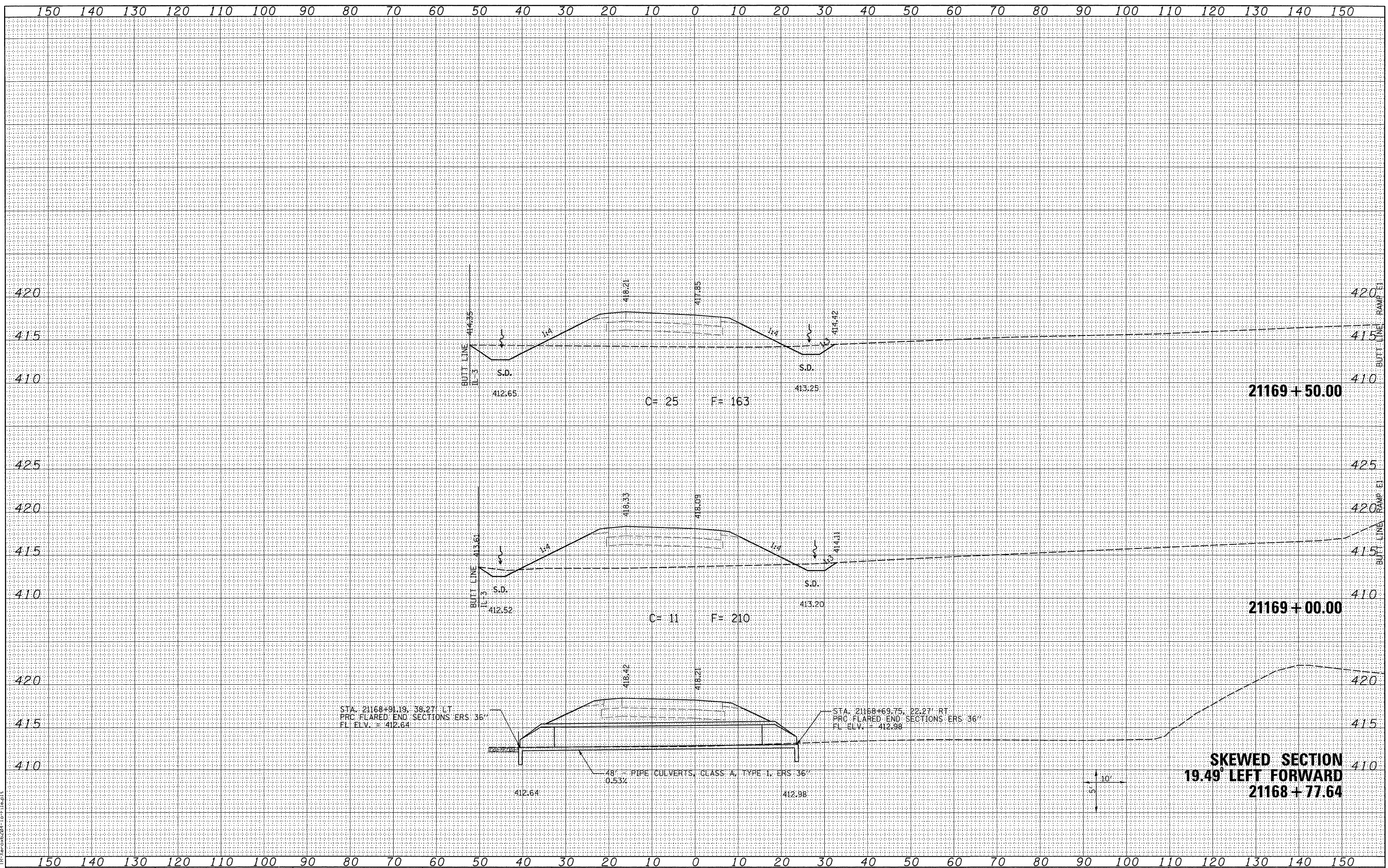
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 176 |

CONTRACT NO. 76D87

ILLINOIS FED. AID PROJECT

| | |
|---------------|------|
| BY | DATE |
| SURVEYED | |
| PLOTTED | |
| NOTE BOOK | |
| AREAS CHECKED | |

| | |
|-----------------|------|
| BY | DATE |
| ORIGINAL SURVEY | |
| NOTE BOOK | |
| AREAS CHECKED | |



STA. 21168+91.19, 38.27' LT
 PRC FLARED END SECTIONS ERS 36"
 FL ELV. = 412.64'

STA. 21168+69.75, 22.27' RT
 PRC FLARED END SECTIONS ERS 36"
 FL ELV. = 412.98'

48" PIPE CULVERTS, CLASS A, TYPE 1, ERS 36"
 0.53%

SKEWED SECTION
19.49° LEFT FORWARD
21168 + 77.64

LAST SAVED = 3/16/2010
 PEN TABLE = 18.64
 PLOT DRIVER = TP-Via-cad6204-To-File.plt

| | | | |
|--|----------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - |
| \\0906600\0906601\cad\l_plans\091.D876D87-Sh-X5Sh-RampP1.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 10.0005' / IN. | | CHECKED - | REVISED - |
| PLOT DATE = 3/16/2010 4:44:44 PM | | DATE - | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HORNER & SHIFRIN, INC.
 ENGINEERS

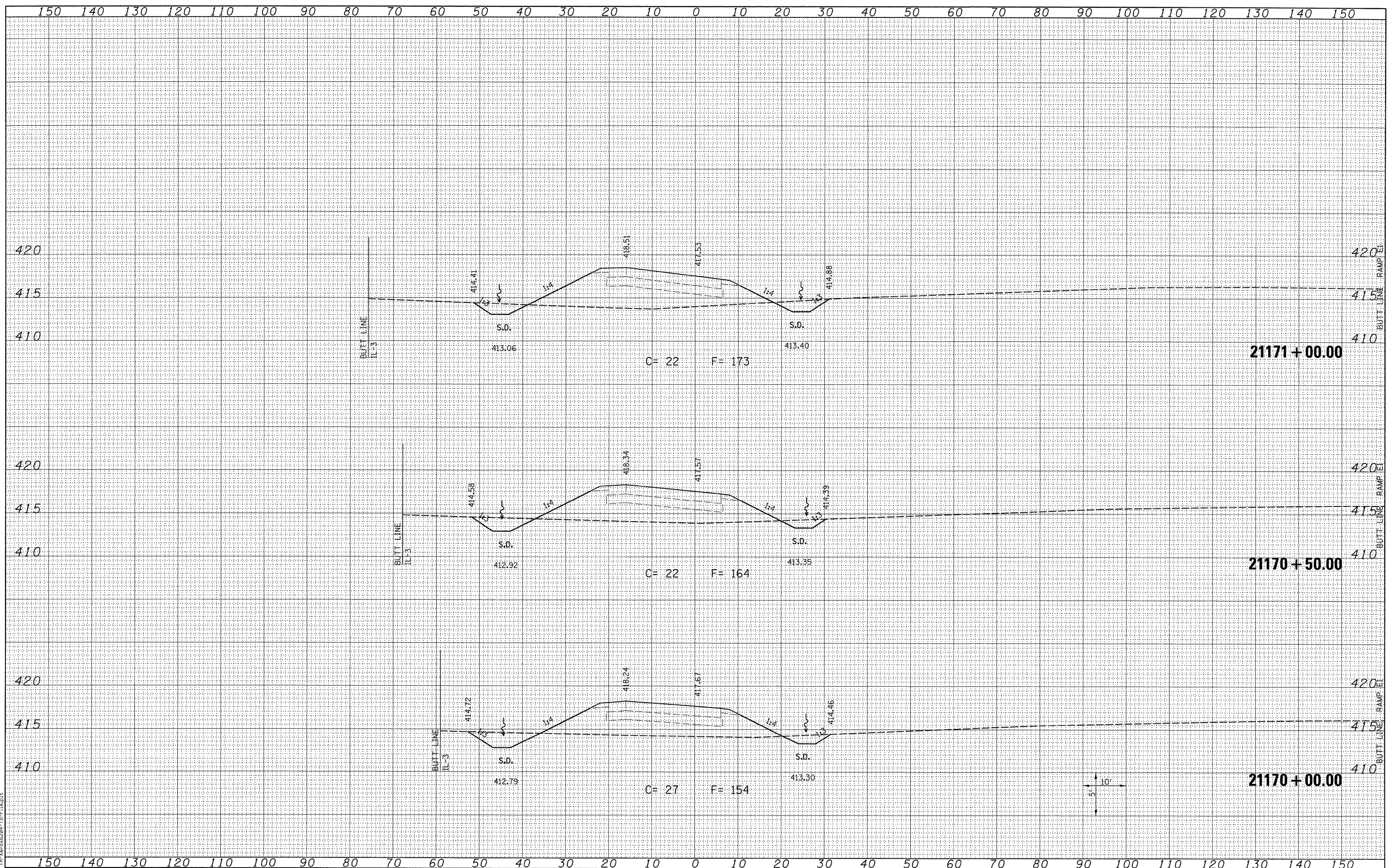
CROSS SECTIONS
 PROPOSED RAMP P1

SCALE: SEE SHT SHEET NO. 3 OF 11 SHEETS STA. 21168+77.64 TO STA. 21169+50.00

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|---------|--------------|--------------------|
| 270 | 60-2RS-3 | MADISON | 231 | 177 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|------|
| BY | DATE |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|---------------|------|
| BY | DATE |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



LAST SAVED = 3/16/2010
PEN TABLE = V8.H4
PLOT DRIVER = TR-Xerox6204-ForFile.plt

FILE NAME =
\\0906600\0906601\cad\1_plans\091.D876D87-Sh-XSht-RampP1.dgn

| | | |
|-----------------------------------|------------|-----------|
| USER NAME = sdonahue | DESIGNED - | REVISED - |
| DRAWN - | REVISOR - | REVISOR - |
| PLLOT SCALE = 18.0005' / IN. | CHECKED - | REVISOR - |
| PLLOT DATE = 3/16/2010 4:44:45 PM | DATE - | REVISOR - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



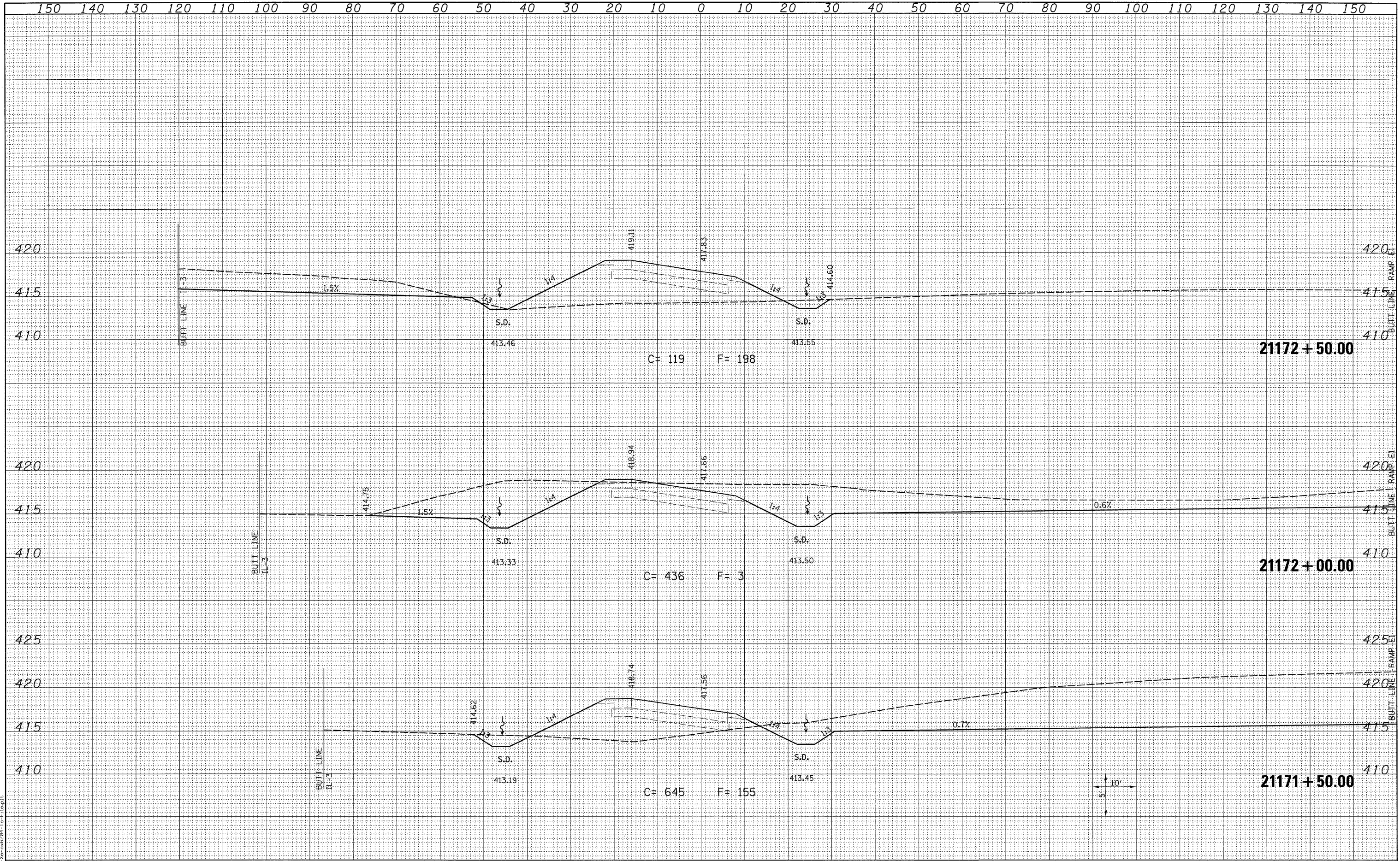
**CROSS SECTIONS
PROPOSED RAMP P1**

SCALE: SEE SHT SHEET NO. 4 OF 11 SHEETS STA. 21170+00.00 TO STA. 21171+00.00

| | | | | |
|--------------------|------------------|----------------|------------------|---------------------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 178 |
| CONTRACT NO. 76D87 | | | | ILLINOIS FED. AID PROJECT |

| | | |
|--------------|---------------|------|
| FINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | TEMPLATE | |
| | AREAS CHECKED | |

| | | |
|-----------------|---------------|------|
| ORIGINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | TEMPLATE | |
| | AREAS CHECKED | |

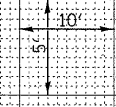
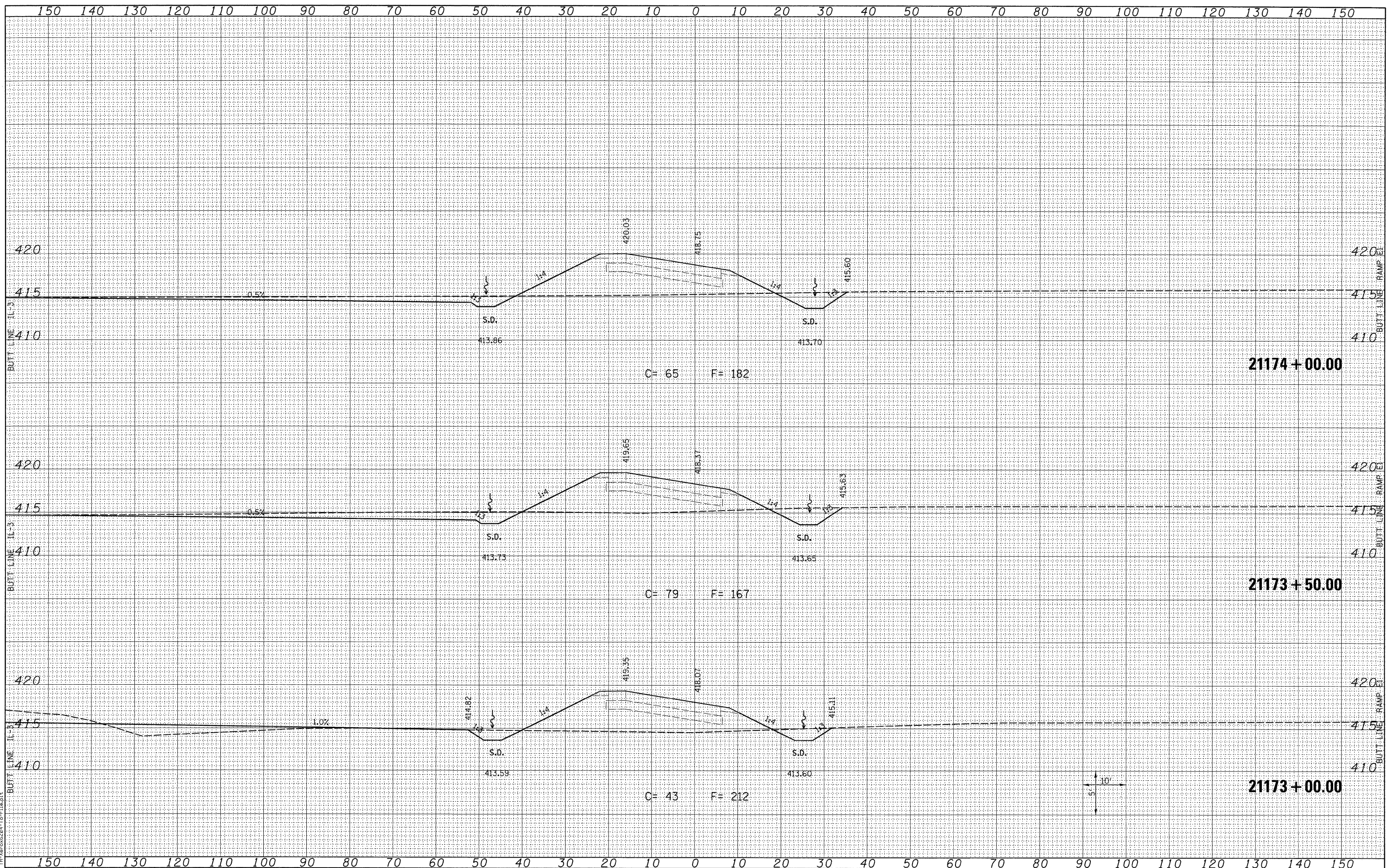


LAST SAVED = 2/16/2010
PEN TABLE = V6.S4
PLOT DRIVER = TP-Varex6204-1-File.plt

| | | | | | | | | | | | |
|---|----------------------|------------|-----------|---|--|---|-------------------|--------------------------------------|----------------|---------------------------|---------------|
| FILE NAME = | USER NAME = edonahue | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | | CROSS SECTIONS PROPOSED RAMP P1 | F.A.I. R.T.E. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 179 |
| H:\0906600\09066001\cad\1\plans\091.D076D07-Sh-XSSht-RampP1.dgn | DRAWN - | REVISED - | | | | | | | | | |
| PLOT SCALE = 10.0005 ' / IN. | CHECKED - | REVISED - | | | | | | | | | |
| PLOT DATE = 3/16/2010 4:44:46 PM | DATE - | REVISED - | | | | | | | | | |
| SCALE: SEE SHT | | | | | | SHEET NO. 5 OF 11 SHEETS | | STA. 21171+50.00 TO STA. 21172+50.00 | | ILLINOIS FED. AID PROJECT | |

| | | |
|---------------|----------|------|
| FINAL SURVEY | SURVEYED | DATE |
| PLOTTED | BY | |
| NOTE BOOK | | |
| AREAS CHECKED | | |
| NO. | | |

| | | |
|-----------------|----------|------|
| ORIGINAL SURVEY | SURVEYED | DATE |
| PLOTTED | BY | |
| NOTE BOOK | | |
| AREAS CHECKED | | |
| NO. | | |



LAST SAVED = 3/16/2010
 PEN TABLE = V8.161
 PLOT DRIVER = TR: Xerox6284-For-File.plt

FILE NAME =
 USER NAME = sdonahue
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISÉD -
 REVISÉD -
 REVISÉD -
 REVISÉD -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

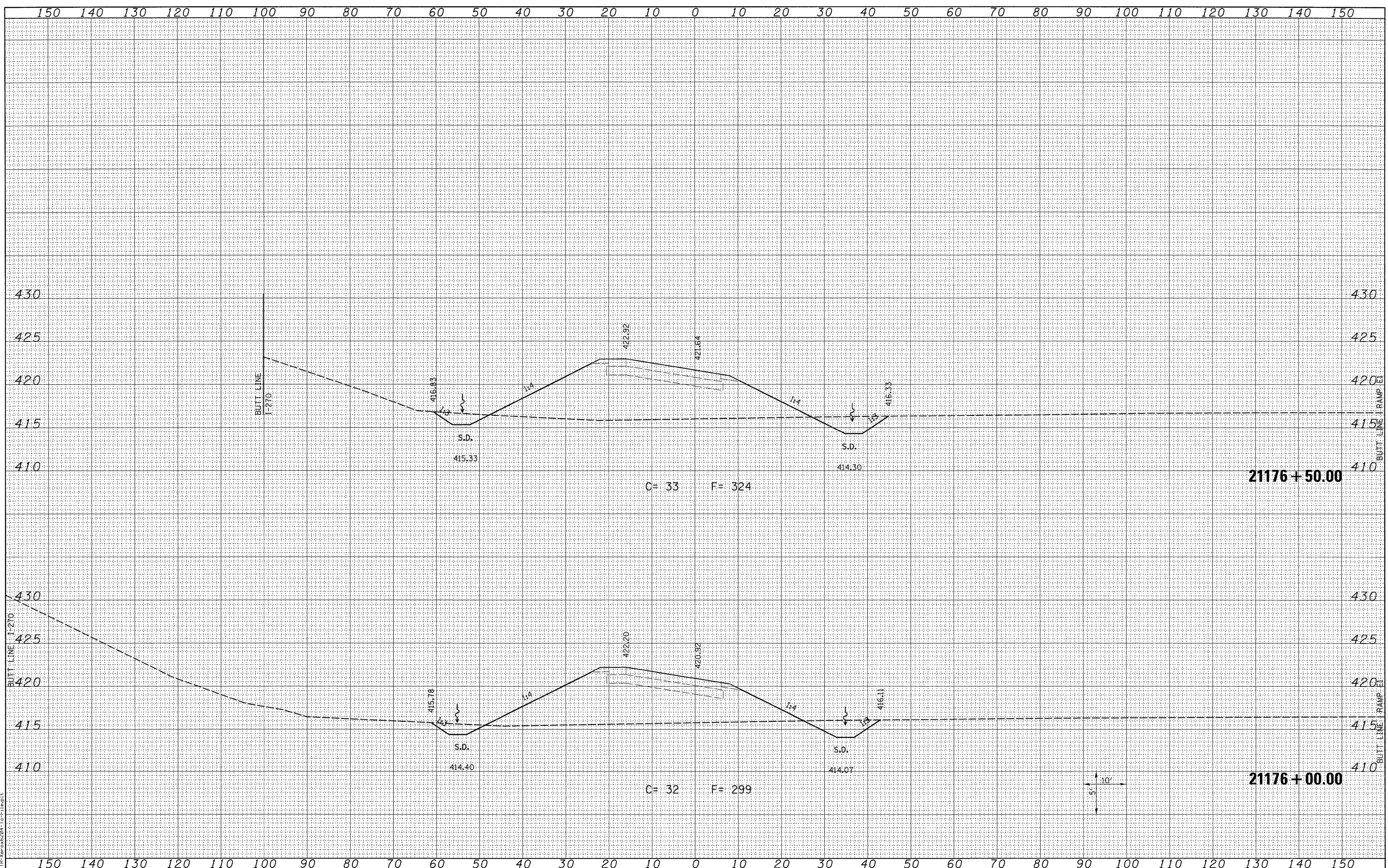


CROSS SECTIONS
 PROPOSED RAMP P1
 SCALE: SEE SHT SHEET NO. 6 OF 11 SHEETS STA. 21173+00.00 TO STA. 21174+00.00

| | | | | |
|---------------------------|----------|---------|--------------|--------------------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 180 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|--|
| DATE | |
| BY | |
| FINAL SURVEY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|-----------------|--|
| DATE | |
| BY | |
| ORIGINAL SURVEY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



LAST SAVED = 3/16/2010
 PEN TABLE = V8.tbl
 PLOT DRIVER = HP-Ver6294-1a-File.plt

FILE NAME = J:\0966600\0906601\cad\l_plans\09L_D876D87-Sh-XSht-RampP1.dgn
 USER NAME = sdonahue

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

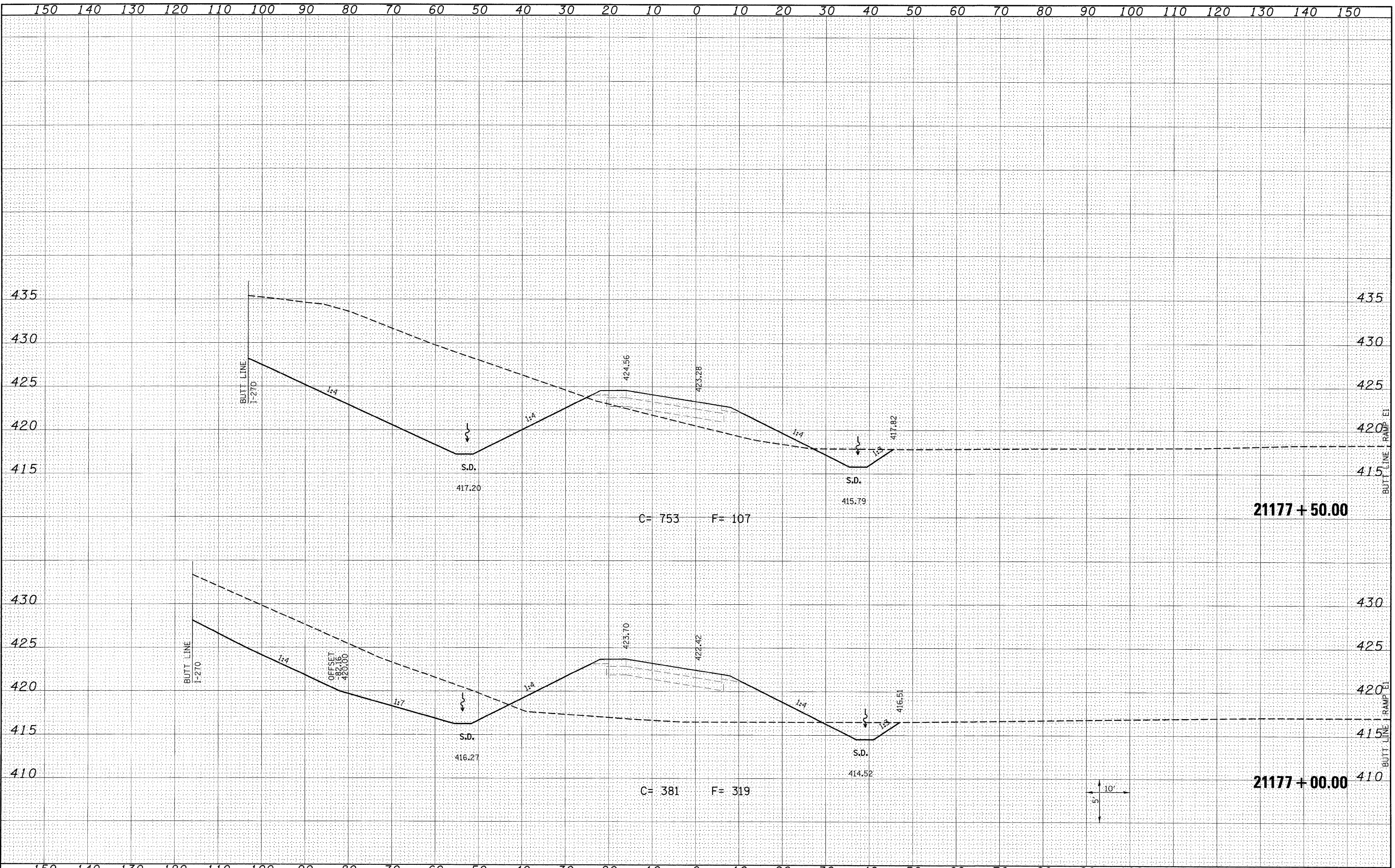


CROSS SECTIONS
 PROPOSED RAMP P1
 SCALE: SEE SHT
 SHEET NO. 8 OF 11 SHEETS
 STA. 21176+00.00 TO STA. 21176+50.00

| | | | | |
|---------------------------|-------------------|-----------------|-------------------|----------------|
| F.A.I. RATE: 270 | SECTION: 60-2RS-3 | COUNTY: MADISON | TOTAL SHEETS: 231 | SHEET NO.: 182 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|--------------|------|
| FINAL SURVEY | DATE |
| SURVEYED | BY |
| NOTED | |
| PLOTTED | |
| TEMPLATE | |
| AREAS | |
| CHECKED | |
| NO. | |

| | |
|-----------------|------|
| ORIGINAL SURVEY | DATE |
| SURVEYED | BY |
| NOTED | |
| PLOTTED | |
| TEMPLATE | |
| AREAS | |
| CHECKED | |
| NO. | |



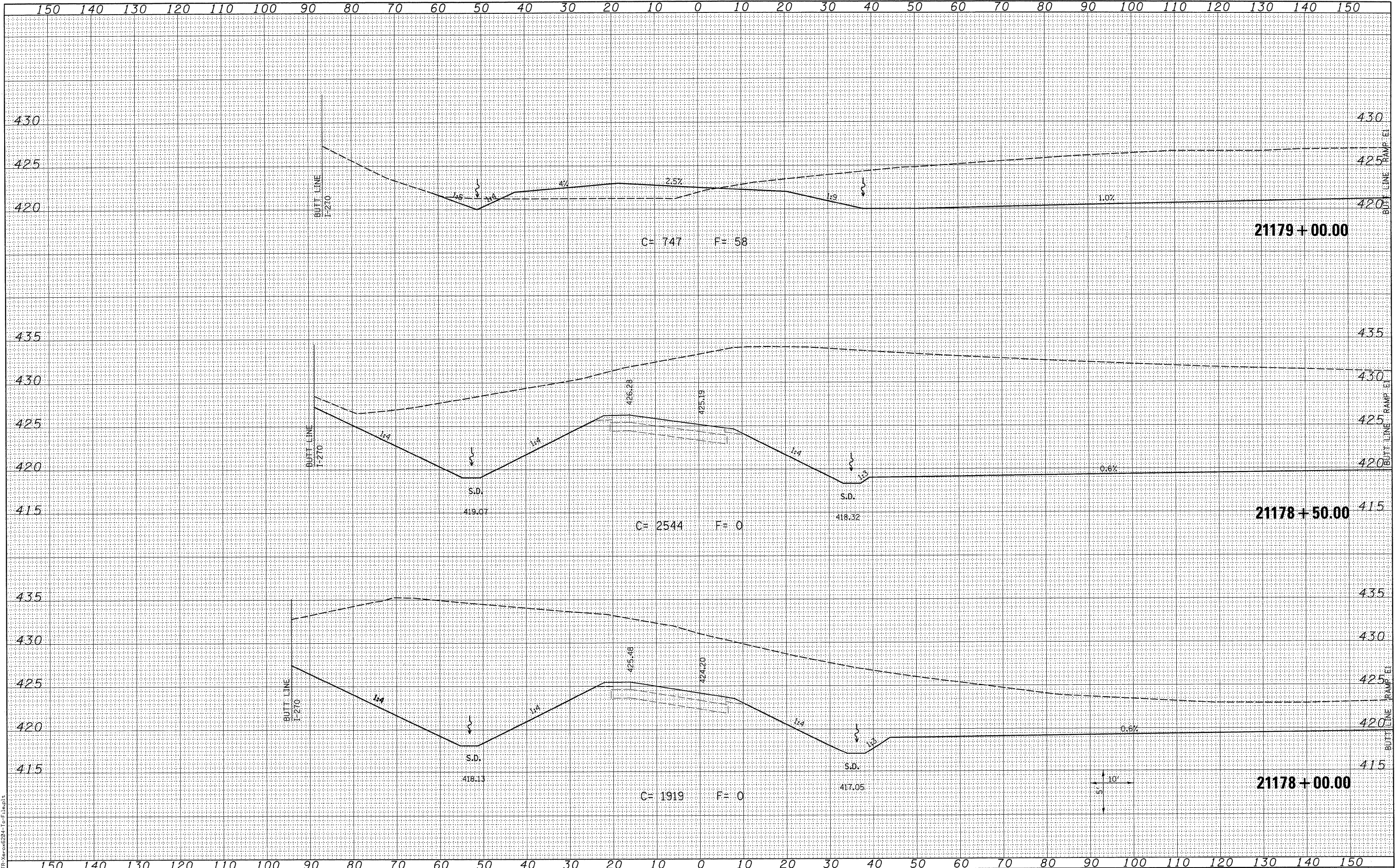
| | | | | | | | | | | | |
|------------------------------|------------------------------|------------|-----------|---|---|---|---------------------------|----------|---------|--------------|-----------|
| FILE NAME = | USER NAME = owerbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | HORNER & SHIFRIN, INC. ENGINEERS | CROSS SECTIONS PROPOSED RAMP P1 | F.A.T. RTE. = | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| LAST SAVED = #LASTSAVEDDATE* | XSSht-RampP1.dgn | DRAWN - | REVISED - | | | | 270 | 60-2RS-3 | MADISON | 231 | 183 |
| PEN TABLE = #PENLIST* | PLOT SCALE = 10.0000' / IN. | CHECKED - | REVISED - | | | | CONTRACT NO. 76D87 | | | | |
| PLOT DRIVER = #PLTDVSE* | PLOT DATE = 4/23/2010 *TIME* | DATE - | REVISED - | | | | ILLINOIS FED. AID PROJECT | | | | |

LAST SAVED = #LASTSAVEDDATE*
 PEN TABLE = #PENLIST*
 PLOT DRIVER = #PLTDVSE*

| | |
|---------------|---------------|
| DATE | |
| BY | |
| FINAL SURVEY | SURVEYED |
| NOTE BOOK NO. | PLOTTED |
| | TEMPLATE |
| | AREAS CHECKED |

| | |
|-----------------|---------------|
| DATE | |
| BY | |
| ORIGINAL SURVEY | SURVEYED |
| NOTE BOOK NO. | PLOTTED |
| | TEMPLATE |
| | AREAS CHECKED |

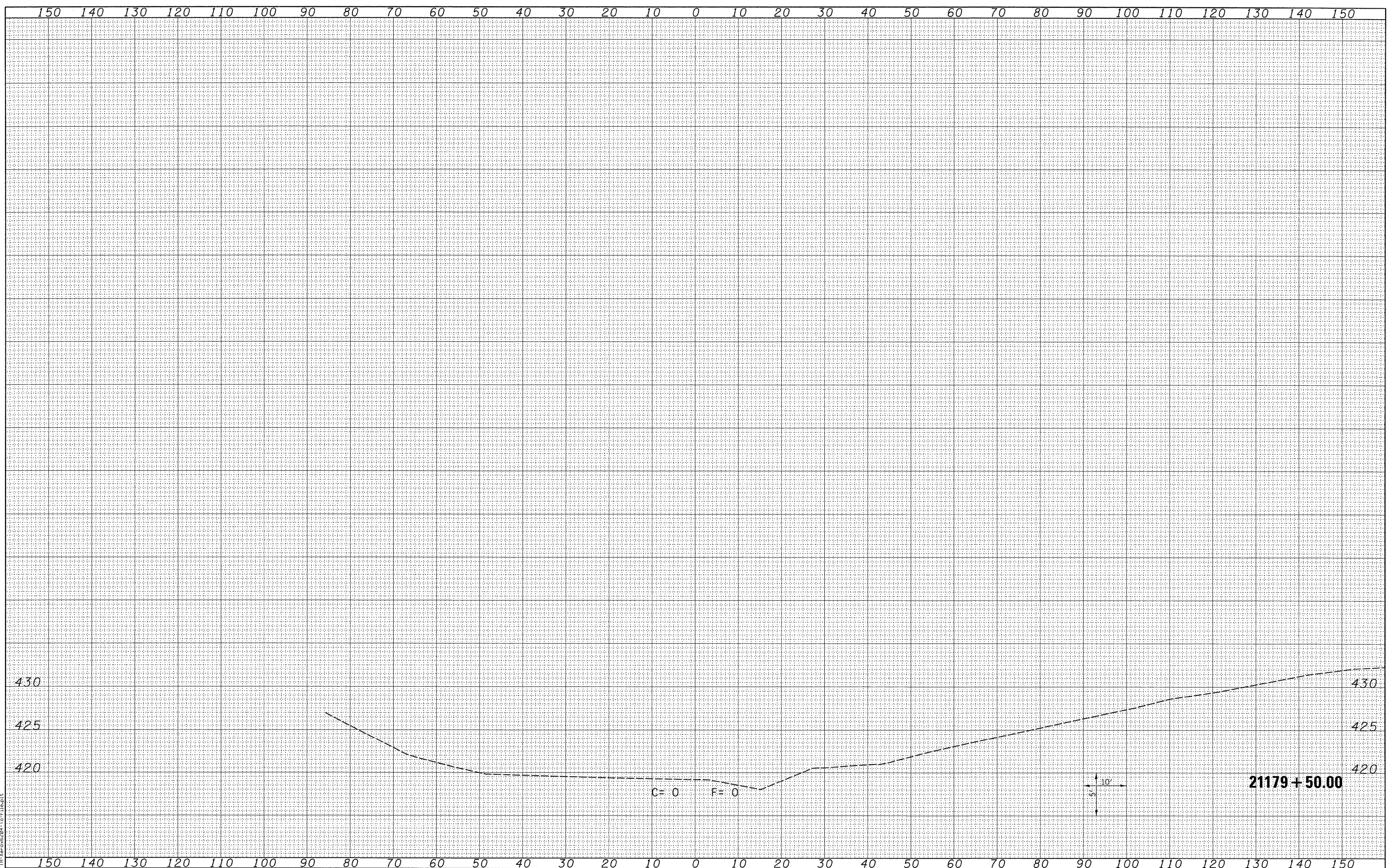
LAST SAVED = 3/16/2010 4:44:51 PM
 PEN TABLE = V8.tbl
 PLOT DRIVER = \\hp-cw6204-10-File.plt



| | | | | | | | | | |
|-------------------------------------|---------------------------------|------------|-----------|---|--|-----------------------|---------------------------|---|--------------------------------------|
| FILE NAME = \\hp-cw6204-10-File.plt | USER NAME = sdonehue | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | | CROSS SECTIONS | | F.A.I. RTE. 270 SECTION 60-2RS-3 COUNTY MADISON TOTAL SHEETS 231 SHEET NO. 184 CONTRACT NO. 76D87 ILLINOIS FED. AID PROJECT | |
| | PLT SCALE = 10.0005' / IN. | DRAWN - | REVISED - | | | PROPOSED RAMP P1 | | | |
| | PLT DATE = 3/16/2010 4:44:51 PM | CHECKED - | REVISED - | | | SCALE: SEE SHT | SHEET NO. 10 OF 11 SHEETS | | STA. 21178+00.00 TO STA. 21179+00.00 |
| | | DATE - | REVISED - | | | | | | |

| | | |
|--------------|---------------|------|
| FINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | TEMPLATE | |
| | AREAS CHECKED | |

| | | |
|-----------------|---------------|------|
| ORIGINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | TEMPLATE | |
| | AREAS CHECKED | |



LAST SAVED = 2/16/2010
 PEN TABLE = V8.161
 PLOT DRIVER = TR:V806204>To:File.plt

FILE NAME =
 J:\0906600\0906601\cad\plns\091_D876D87-Sh

USER NAME = sdonehue
 -X5Sh-RampP1.dgn
 PLOT SCALE = 10.0005' / IN.
 PLOT DATE = 3/16/2010 4:44:51 PM

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

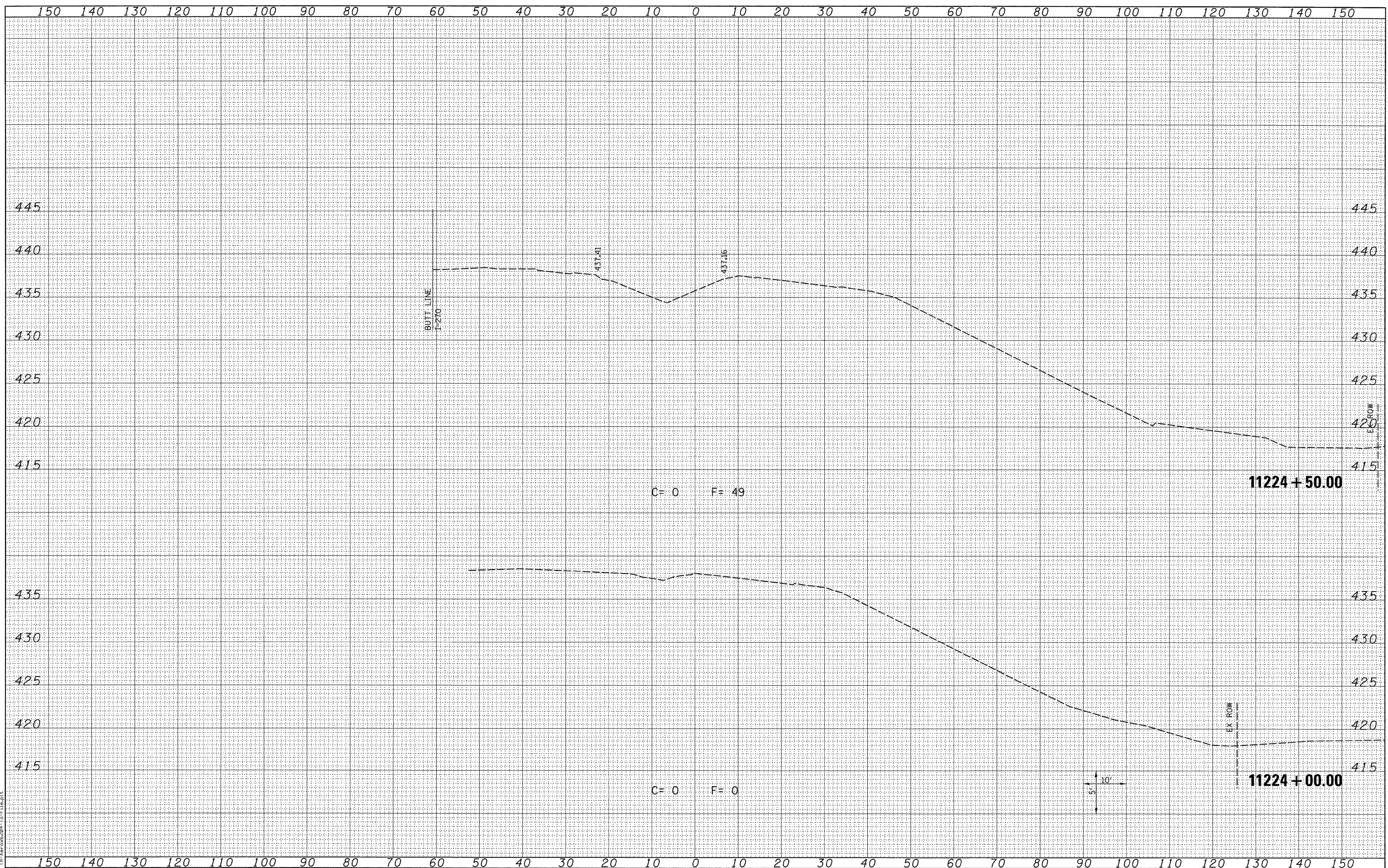
HORNER &
 SHIFRIN, INC.
 ENGINEERS

CROSS SECTIONS
 PROPOSED RAMP P1
 SCALE: SEE SHT SHEET NO. 11 OF 11 SHEETS STA. 21179+50.00 TO STA. 21179+50.00

| | | | | |
|---------------------------|---------------------|-------------------|---------------------|------------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 185 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| | | |
|--------------|---------------|------|
| FINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | TEMPLATE | |
| | AREAS CHECKED | |

| | | |
|-----------------|---------------|------|
| ORIGINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | TEMPLATE | |
| | AREAS CHECKED | |



LAST SAVED = 3/16/2010
 PEN TABLE = V8.B4
 PLOT DRIVER = TR-V8r06204-To-File.plt

| | | | |
|---|----------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonehue | DESIGNED - | REVISED - |
| \\0906600\0906601\cad\1\plans\092_0876087-Sht-XSht-RampP4.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 10.0005' / IN. | | CHECKED - | REVISED - |
| PLOT DATE = 3/16/2010 4:44:54 PM | | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



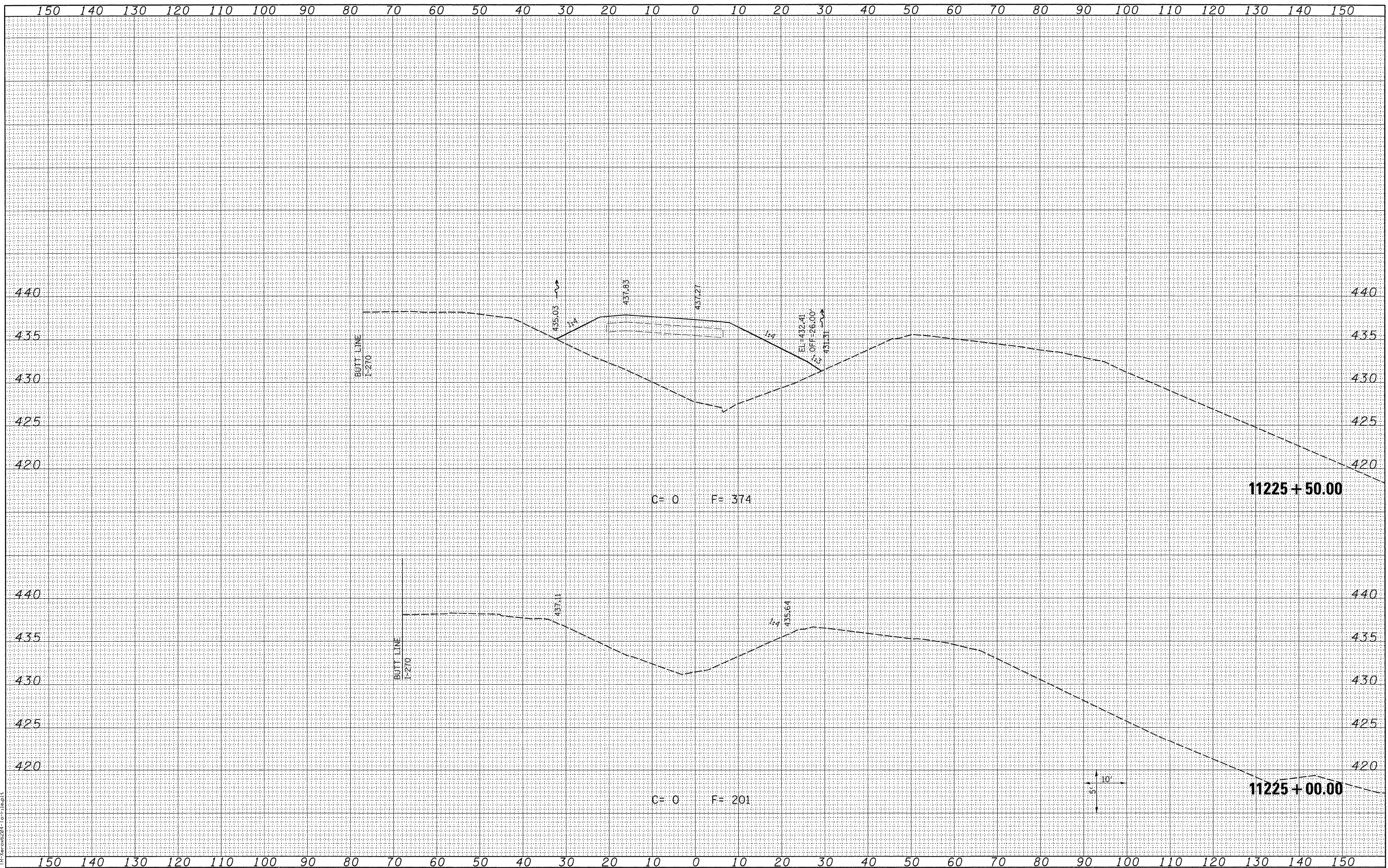
**CROSS SECTIONS
 PROPOSED RAMP P4**

SCALE: SEE SHT SHEET NO. 1 OF 8 SHEETS STA. 11224+00.00 TO STA. 11224+50.00

| | | | | |
|---------------------------|------------------|----------------|------------------|---------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 186 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



LAST SAVED = 3/16/2010
 PEN TABLE = V8.DWG
 PLOT DRIVER = TP-V806204-TP-File.plt

| | | | |
|--|----------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - |
| \\0906600\0906601\cad\plens\092.D876087-Sht-XSSht-RampP4.dgn | | DRAWN - | REVISED - |
| | | CHECKED - | REVISED - |
| | | DATE - | REVISED - |
| | | | |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HORNER &
 SHIFRIN, INC.
 ENGINEERS

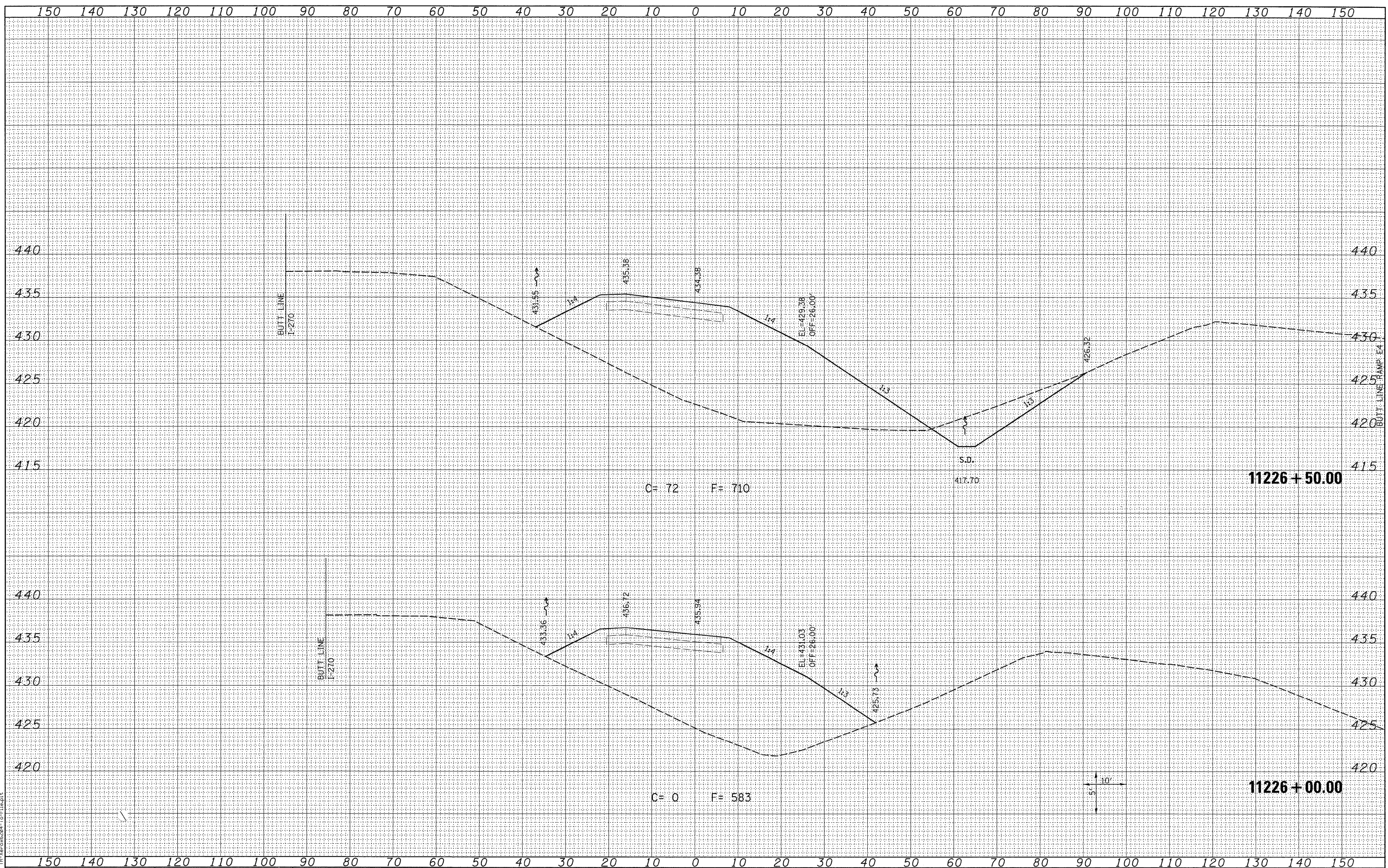
CROSS SECTIONS
 PROPOSED RAMP P4

SCALE: SEE SHT SHEET NO. 2 OF 8 SHEETS STA. 11225+00.00 TO STA. 11225+50.00

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 187 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| NO. | |
| AREAS CHECKED | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| NO. | |
| AREAS CHECKED | |



LAST SAVED = 3/16/2010
 PEN TABLE = 16.161
 PLOT DRIVER = T:\Xero6284-TopFiles\p

FILE NAME =
 USER NAME = sdonahue
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

DESIGNED -
 REVISIONS:
 REVISIONS:
 REVISIONS:
 REVISIONS:

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HORNER &
 SHIFRIN, INC.
 ENGINEERS

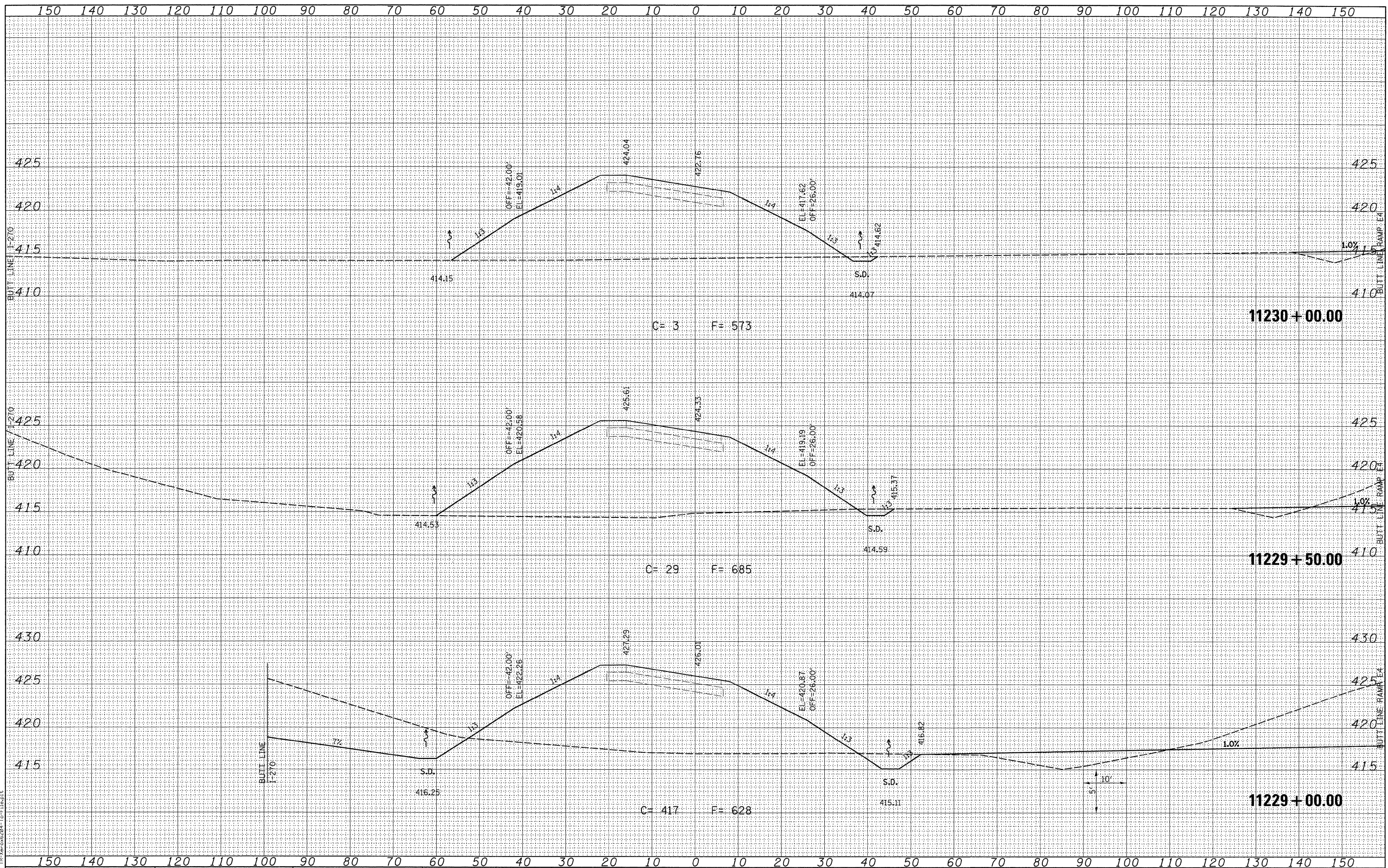
CROSS SECTIONS
 PROPOSED RAMP P4

SCALE: SEE SHT SHEET NO. 3 OF 8 SHEETS STA. 11226+00.00 TO STA. 11226+50.00

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 188 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



LAST SAVED = 3/16/2010
 PEN TABLE = V6.dwg
 PLOT DRIVER = IR-V6.dwg

FILE NAME =
 USER NAME = scondahue
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISÉD -
 REVISÉD -
 REVISÉD -
 REVISÉD -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

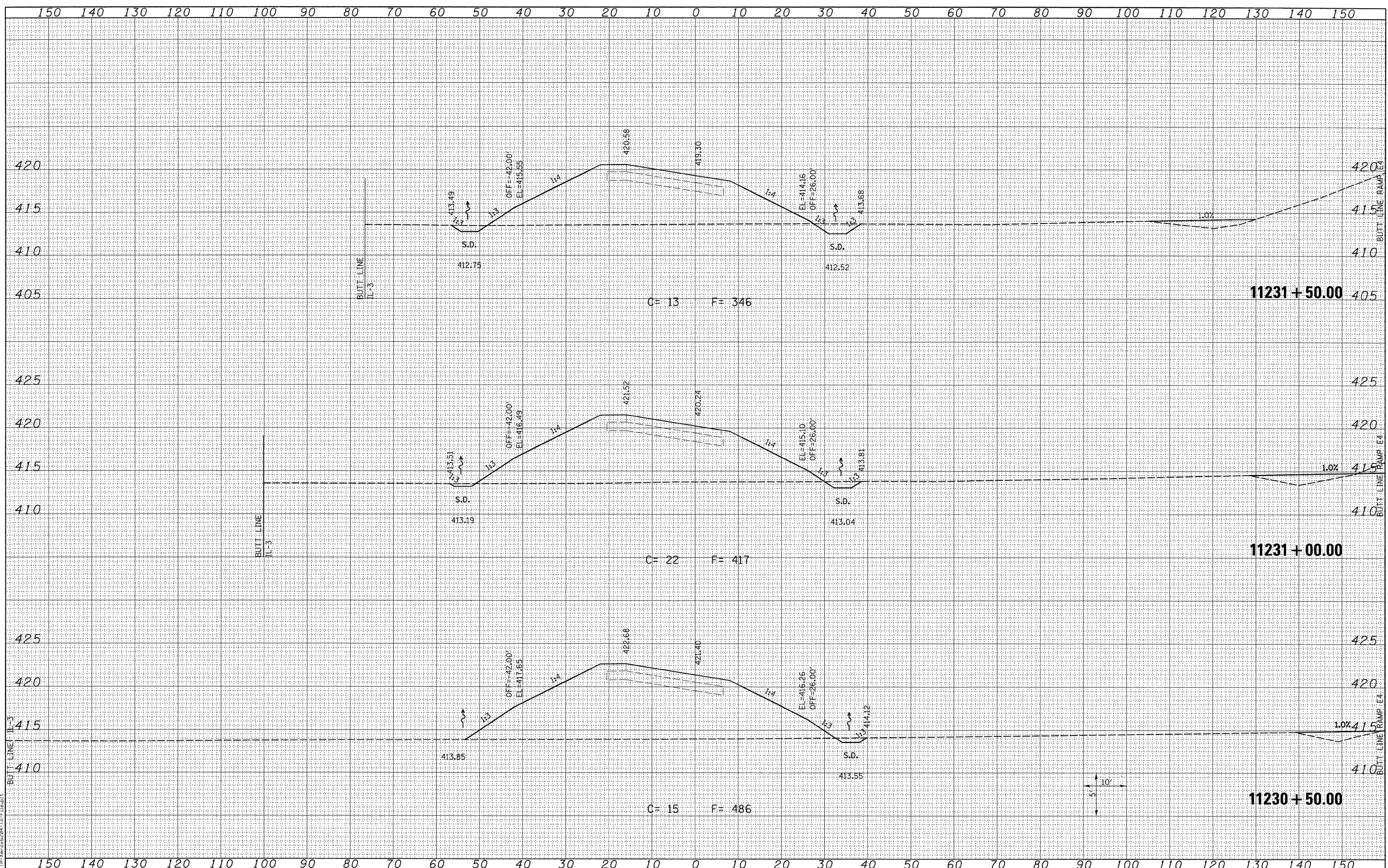


CROSS SECTIONS
 PROPOSED RAMP P4
 SCALE: SEE SHT
 SHEET NO. 6 OF 8 SHEETS
 STA. 11229+00.00 TO STA. 11230+00.00

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 191 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|--------------|--|
| DATE | |
| BY | |
| FINAL SURVEY | |
| NOTED | |
| NO. | |

| | |
|-----------------|--|
| DATE | |
| BY | |
| ORIGINAL SURVEY | |
| NOTED | |
| NO. | |



LAST SAVED = 3/16/2010
 PEN TABLE = V8 (6)
 PLOT DRIVER = TR-V80x6204-10-File.plt

| | |
|---|----------------------|
| FILE NAME = | USER NAME = sdonahue |
| \\s0906600\0906600\cad\plans\02_0876087-Sht-XSht-RampP4.dgn | DESIGNED - |
| PLOT SCALE = 10.0005' / IN. | DRAWN - |
| PLOT DATE = 3/16/2010 4:45:00 PM | CHECKED - |
| | DATE - |
| | REVISED - |
| | REVISED - |
| | REVISED - |
| | REVISED - |

| | |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

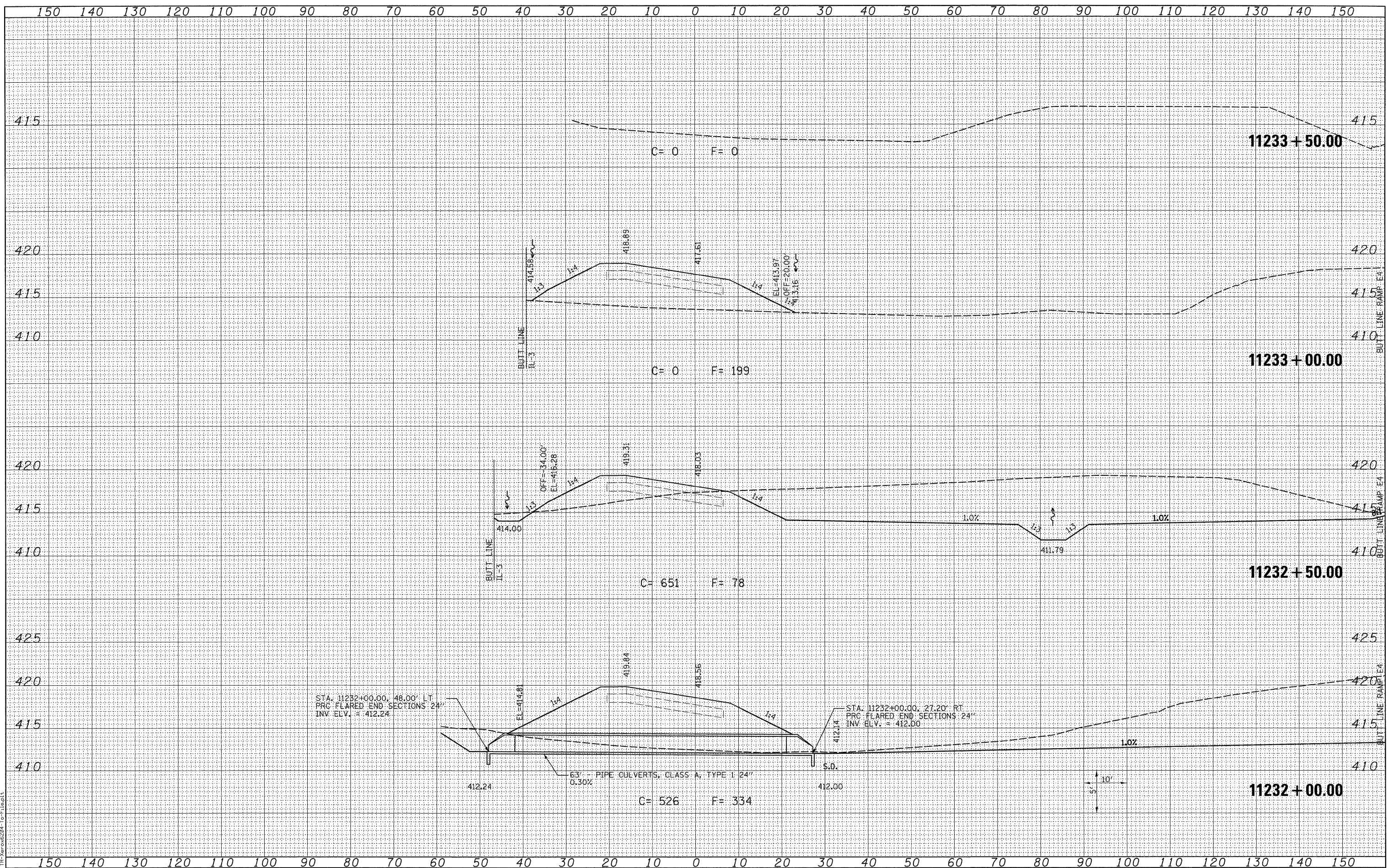


| | |
|--------------------------------------|-------------------------|
| CROSS SECTIONS | |
| PROPOSED RAMP P4 | |
| SCALE: SEE SHT | SHEET NO. 7 OF 8 SHEETS |
| STA. 11230+50.00 TO STA. 11231+50.00 | |

| | | | | |
|---------------------------|------------------|----------------|--------------------|---------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 192 |
| | | | CONTRACT NO. 76D87 | |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|---------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



LAST SAVED = 3/16/2010
 PEN TABLE = V:\60606060
 PLOT DRIVER = TR:\xoc6204-To-File.plt

| | |
|------------------------------|----------------------|
| FILE NAME = | USER NAME = sdonahue |
| DESIGNED - | REVISOR - |
| DRAWN - | REVISOR - |
| PLLOT SCALE = 18.0005' / IN. | CHECKED - |
| DATE = 3/16/2010 4:45:01 PM | REVISOR - |

| | |
|-----------------------------|-----------|
| DESIGNED - | REVISOR - |
| DRAWN - | REVISOR - |
| CHECKED - | REVISOR - |
| DATE = 3/16/2010 4:45:01 PM | REVISOR - |

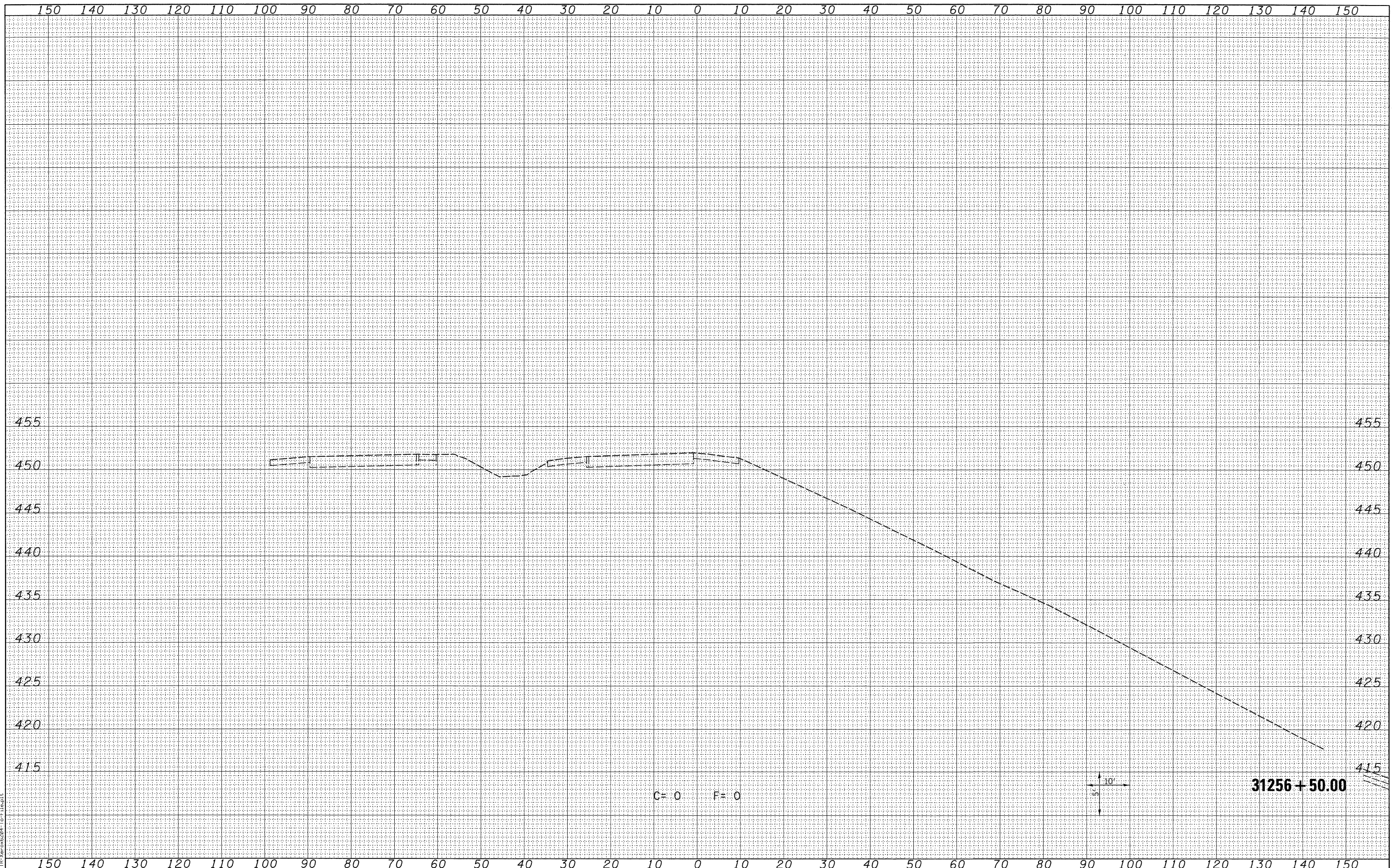
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**HORNER &
 SHIFRIN, INC.
 ENGINEERS**

**CROSS SECTIONS
 PROPOSED RAMP P4**

SCALE: SEE SHT SHEET NO. 8 OF 8 SHEETS STA. 11232+00.00 TO STA. 11233+50.00

| | | | | |
|---------------------------|----------|---------|--------------|--------------------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 193 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |



| | | |
|---------------|---------------|------|
| FINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK NO. | PLOTTED | |
| | TEMPLATE | |
| | AREAS CHECKED | |

| | | |
|-----------------|---------------|------|
| ORIGINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK NO. | PLOTTED | |
| | TEMPLATE | |
| | AREAS CHECKED | |

LAST SAVED = 3/16/2010
 PEN TABLE = V8.tbl
 PLOT DRIVER = TR:Vericut204-To-File.plt

FILE NAME = \\0966600\0905601\oad\l_plans\093_0876087-Sht-XSht-RampP5.dgn
 USER NAME = sdonahue
 PLOT SCALE = 10.0000' / IN.
 PLOT DATE = 3/16/2010 4:45:05 PM

| | |
|------------|------------|
| DESIGNED - | REVISIED - |
| DRAWN - | REVISIED - |
| CHECKED - | REVISIED - |
| DATE - | REVISIED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HORNER &
 SHIFRIN, INC
 ENGINEERS
 SCALE: SEE SHT

| | | |
|-------------|--------------|--------------------------------------|
| SHEET NO. 1 | OF 30 SHEETS | STA. 31256+50.00 TO STA. 31256+50.00 |
|-------------|--------------|--------------------------------------|

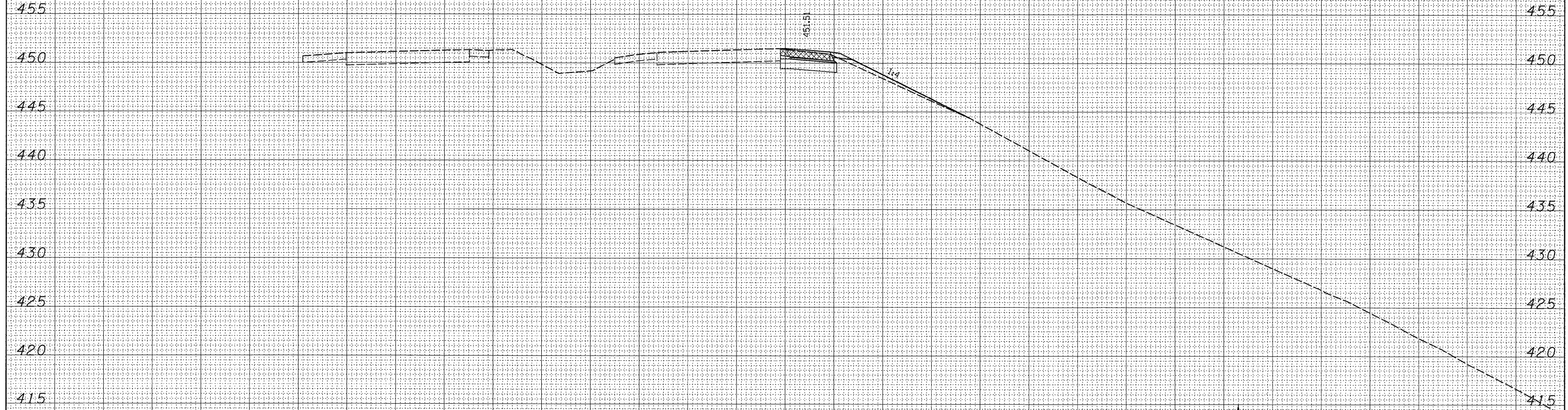
CROSS SECTIONS
 PROPOSED RAMP P5

| | | | | |
|--------------------|------------------|----------------|------------------|---------------------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 194 |
| CONTRACT NO. 76D87 | | | | ILLINOIS FED. AID PROJECT |

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

| | | |
|--------------|---------------|------|
| FINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | TEMPLATE | |
| | AREAS CHECKED | |

| | | |
|-----------------|---------------|------|
| ORIGINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | TEMPLATE | |
| | AREAS CHECKED | |



31256 + 83.37

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

LAST SAVED = 3/16/2010
 PEN TABLE = V8.tbl
 PLOT DRIVER = TR:Kern6284-To-File.plt

FILE NAME =
 \\0906600\0906601\cad\1\plans\093_0876087-Sht-XSht-RampP5.dgn

USER NAME = sdonahue
 DESIGNED -
 DRAWN -
 PLOT SCALE = 10.0005' / IN.
 CHECKED -
 PLOT DATE = 3/16/2010 4:45:06 PM
 DATE -

REVISIED -
 REVISIED -
 REVISIED -
 REVISIED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HORNER &
 SHIFRIN, INC.
 ENGINEERS

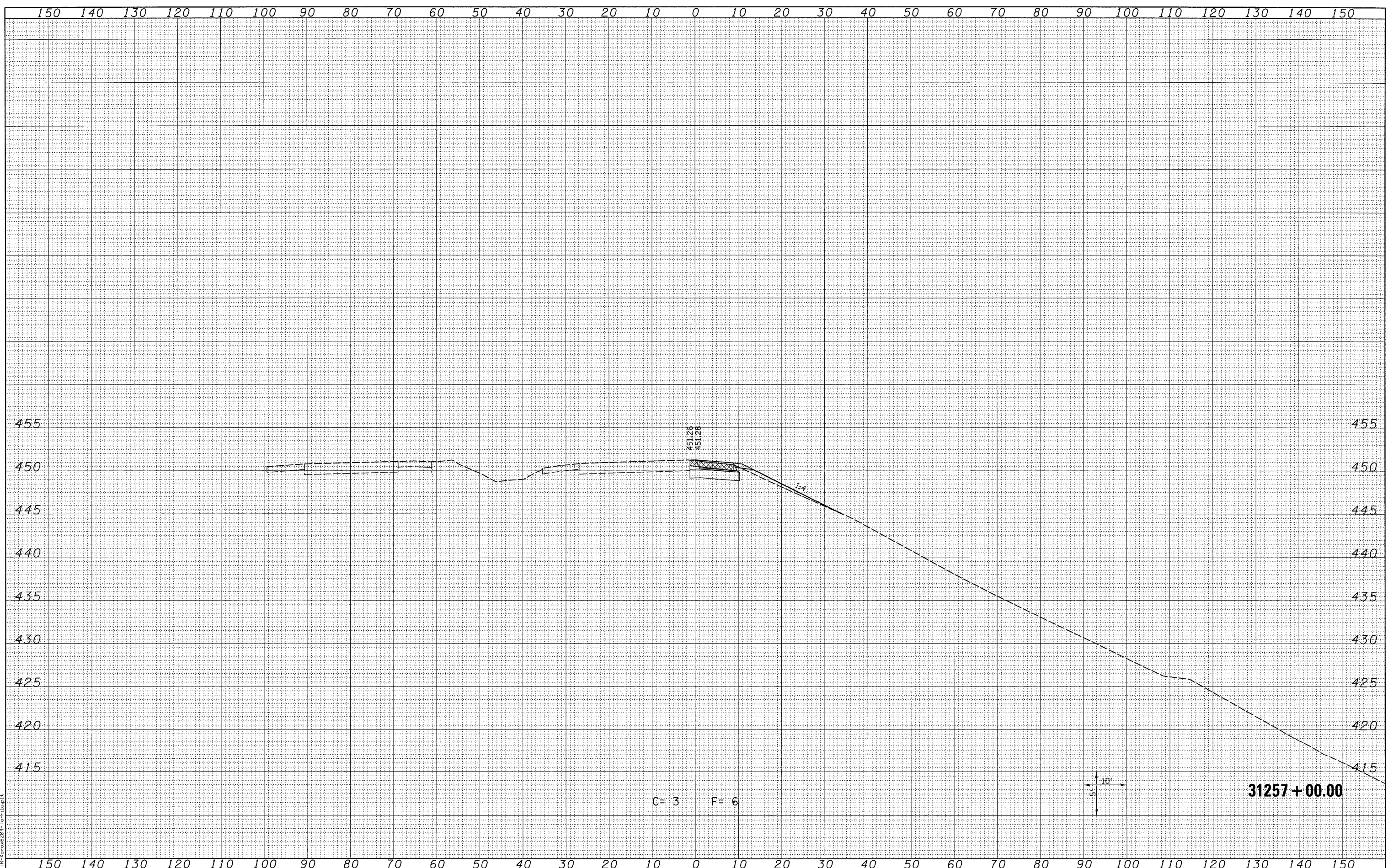
CROSS SECTIONS
 PROPOSED RAMP P5

| | | | | |
|--------------------|----------|---------|---------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 195 |
| CONTRACT NO. 76D87 | | | ILLINOIS FED. AID PROJECT | |

SCALE: SEE SHT SHEET NO. 2 OF 30 SHEETS STA. 31256+83.37 TO STA. 31256+83.37

| | | |
|---------------|---------------|------|
| FINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK NO. | PLOTTED | |
| | TEMPLATE | |
| | AREAS CHECKED | |

| | | |
|-----------------|---------------|------|
| ORIGINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK NO. | PLOTTED | |
| | TEMPLATE | |
| | AREAS CHECKED | |



LAST SAVED = 3/16/2010
 PEN TABLE = V8.dwg
 PLOT DRIVER = TR-Verex6204-To-File.plt

| | |
|--|----------------------|
| FILE NAME = | USER NAME = sdonehue |
| \\0906600\0906601\cad\pl\plans\093_D876D87-Sht-XSht-RampP5.dgn | DESIGNED - |
| | DRAWN - |
| | CHECKED - |
| | DATE - |

| | |
|----------------------------------|------------|
| PLOT SCALE = 10.0005' / IN. | REVISIED - |
| PLOT DATE = 3/16/2010 4:45:07 PM | REVISIED - |
| | REVISIED - |
| | REVISIED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HORNER &
 SHIFFRIN, INC
 ENGINEERS

CROSS SECTIONS
 PROPOSED RAMP P5

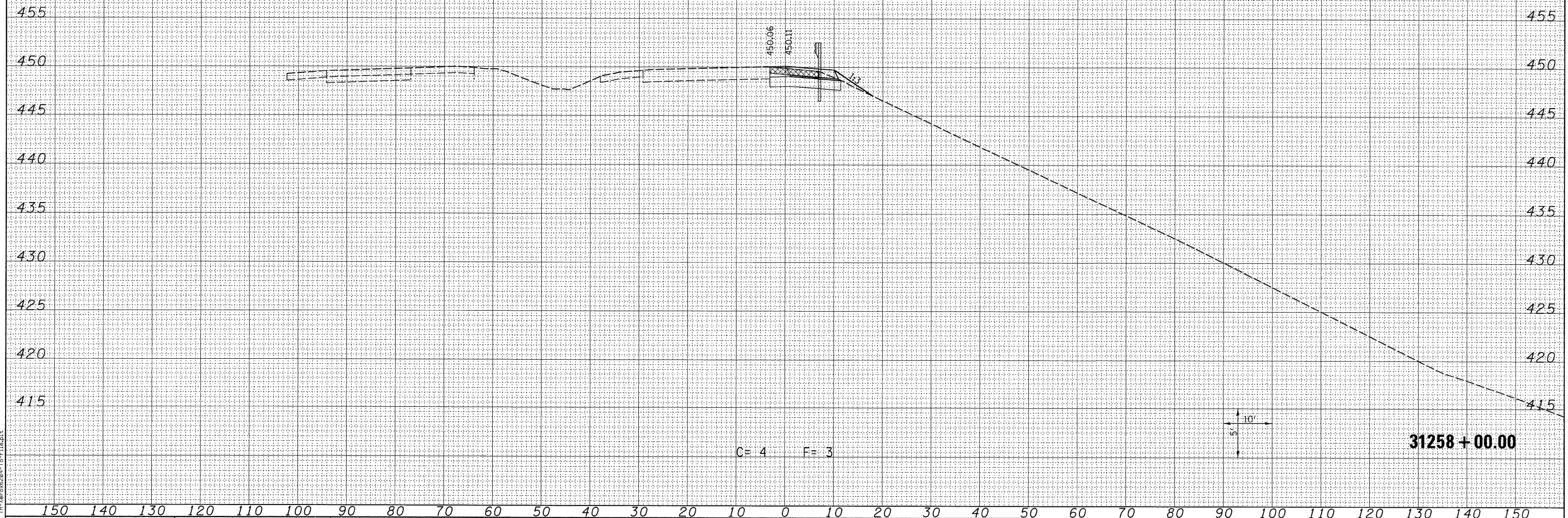
| | | | | |
|---------------------------|------------------|----------------|------------------|--------------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 196 |
| | | | | CONTRACT NO. 76D87 |
| ILLINOIS FED. AID PROJECT | | | | |

SCALE: SEE SHT SHEET NO. 3 OF 30 SHEETS STA. 31257+00.00 TO STA. 31257+00.00

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

| | | | |
|---------------|---------------|----|------|
| FINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK NO. | PLOTTED | | |
| | TEMPLATE | | |
| | AREAS CHECKED | | |

| | | | |
|-----------------|---------------|----|------|
| ORIGINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK NO. | PLOTTED | | |
| | TEMPLATE | | |
| | AREAS CHECKED | | |



LAST SAVED = 3/16/2010
 PEN TABLE = VBA1
 PLOT DRIVER = TR>Xerox6204-TP-F1.job1

| | | | |
|---|----------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - |
| \\0906600\0906601\cad\1\plans\093_D076087-Sht-XSht-RampP5.dgn | | DRAWN - | REVISED - |
| | | CHECKED - | REVISED - |
| | | DATE - | REVISED - |
| | | | |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION



CROSS SECTIONS
 PROPOSED RAMP P5
 SCALE: SEE SHT
 SHEET NO. 5 OF 30 SHEETS
 STA. 31258+00.00 TO STA. 31258+00.00

| | | | | |
|---------------------------|----------|---------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 270 | 60-2RS-3 | MADISON | 231 | 198 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

31258 + 00.00



| | | |
|---------------|---------------|------|
| FINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK NO. | PLOTTED | |
| | TEMPLATE | |
| | AREAS CHECKED | |

| | | |
|-----------------|---------------|------|
| ORIGINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK NO. | PLOTTED | |
| | TEMPLATE | |
| | AREAS CHECKED | |

LAST SAVED = 3/16/2010
 PEN TABLE = V8.161
 PLOT DRIVER = TR-V80x6204-10-File.plt

FILE NAME =
 USER NAME = sdonahue
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISSED -
 REVISSED -
 REVISSED -
 REVISSED -

REVISSED -
 REVISSED -
 REVISSED -
 REVISSED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

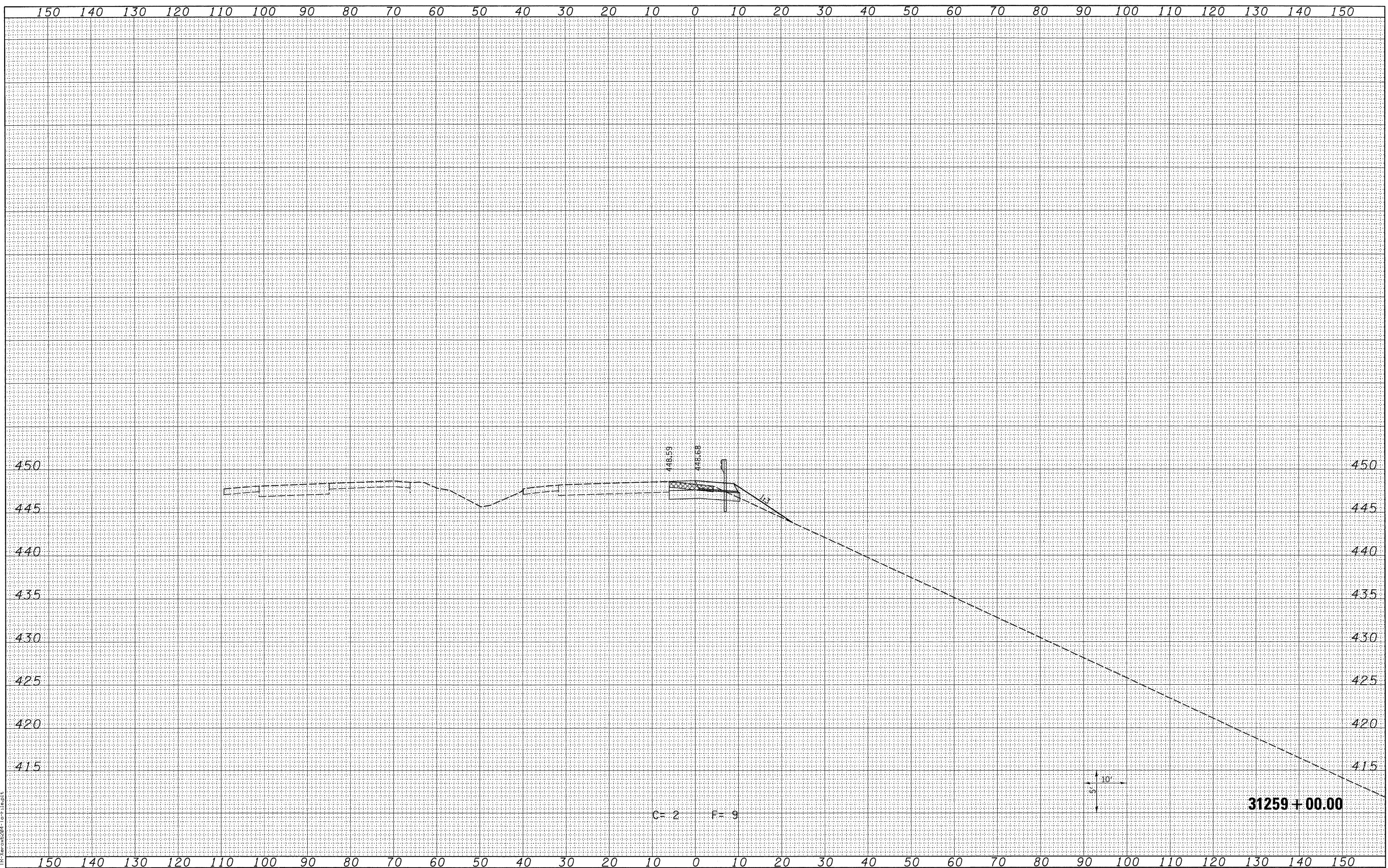
HORNER &
 SHIFRIN, INC
 ENGINEERS

CROSS SECTIONS
 PROPOSED RAMP P5
 SCALE: SEE SHT SHEET NO. 6 OF 30 SHEETS STA. 31258+50.00 TO STA. 31258+50.00

| | | | | |
|---------------------------|------------------|----------------|------------------|---------------|
| F.A.I. RTE. 270 | SECTION 60-2RS-3 | COUNTY MADISON | TOTAL SHEETS 231 | SHEET NO. 199 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| | |
|---------------|------|
| BY | DATE |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |

| | |
|---------------|------|
| BY | DATE |
| SURVEYED | |
| PLOTTED | |
| TEMPLATE | |
| NOTE BOOK | |
| AREAS CHECKED | |
| NO. | |



LAST SAVED = 3/16/2010
 PEN TABLE = V8.064
 PLOT DRIVER = TR-V8062004-Top-File.plt

| | | | |
|--|----------------------|------------|-----------|
| FILE NAME = | USER NAME = sdonahue | DESIGNED - | REVISED - |
| J:\0906600\0906601\cod\1\plans\093_0876087-Sht-XSht-RampP5.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 10.0005' / IN. | | CHECKED - | REVISED - |
| PLOT DATE = 3/16/2010 4:45:10 PM | | DATE - | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION



CROSS SECTIONS
 PROPOSED RAMP P5
 SCALE: SEE SHT SHEET NO. 7 OF 30 SHEETS STA. 31259+00.00 TO STA. 31259+00.00

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|---------|--------------|-----------|
| 270 | 60-2RS-3 | MADISON | 231 | 200 |
| CONTRACT NO. 76D87 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |