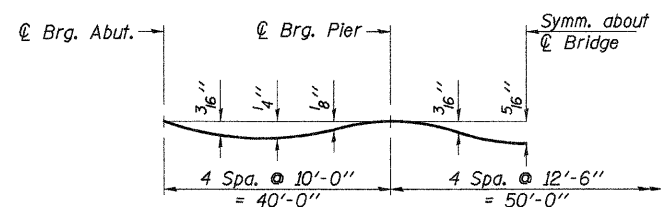


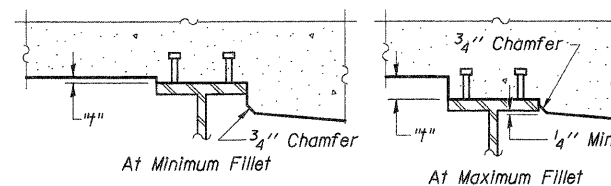
PLAN



DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)

Note:  
The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on Sheet 4 of 16.



To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown above. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on sheet 4 of 16, minus slab thickness, equals the fillet heights "t" above top flange of beams.

FILLET HEIGHTS

DESIGNED	A.L.S.
CHECKED	A.R.K.
DRAWN	S.A.P.
CHECKED	A.R.K. & A.L.S.

TOP OF SLAB ELEVATIONS  
SECTION 04-16101-02-BR  
MARTINTON ROAD DISTRICT  
IROQUOIS COUNTY  
STATION 38+50

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FERRIS, H. HOFFMAN, R. ACCIARIELLO, M. MORROW, W. SPRINGFIELD, IL

JOB NO.: 48341  
FILE: SLAB.DGN  
DATE: 12/04/09