

CONSTRUCTION PLANS FOR MACOMB MUNICIPAL AIRPORT MACOMB, MCDONOUGH COUNTY, ILLINOIS RECONSTRUCT RAMP

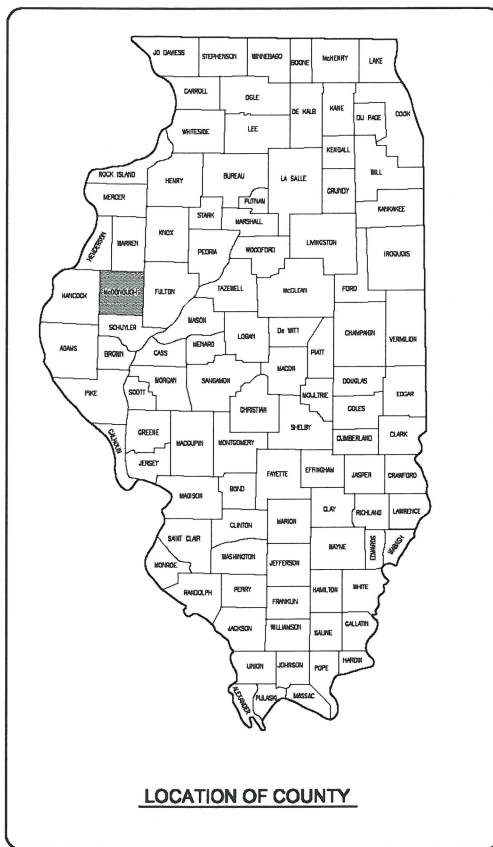
SCOPE OF WORK

THIS PROJECT CONSISTS OF RECONSTRUCTION OF THE WEST SECTION OF THE GENERAL AVIATION RAMP. THIS WORK INCLUDES BITUMINOUS PAVEMENT MILLING, 6" PORTLAND CEMENT CONCRETE (PCC) PAVING, TIE DOWN REMOVAL, TIE DOWN CONSTRUCTION, MARKING, INLET ADJUSTMENT, AND INCIDENTAL ITEMS.

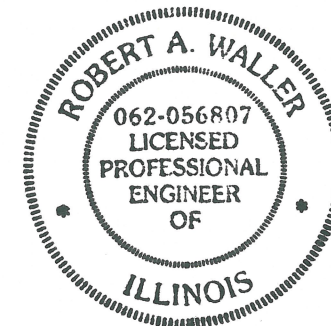


LOCATION

ILL. PROJ.: MQB-4010
A.I.P. PROJ.: 3-17-0064-B19
LATITUDE: 40° 31' 22"
LONGITUDE: 90° 39' 15"
ELEVATION: 705.0' M.S.L.
DATE: APRIL 16, 2010



LOCATION OF COUNTY



Submitted by: *[Signature]* ENG'R
Date Submitted: APRIL 16, 2010
Lic. Exp. Date: NOVEMBER 30, 2010

DATE	REVISION

MACOMB MUNICIPAL AIRPORT
MACOMB, ILLINOIS
A.I.P. PROJ.: 3-17-0064-B19
ILL. PROJ.: MQB-4010

LAYOUT	DPE	01/25/10
DRAWN	DAK/DPE	01/25/10
REVIEWED	JDW	01/25/10



RECONSTRUCT RAMP
COVER SHEET

APR 20, 2010 8:02 AM HARR01115
I:\AIRPORTS\MACOMB\09A0150\CADD\AIRPORT\SHEET\R-001.CVR.DWG

SCOPE OF WORK

THIS PROJECT CONSISTS OF RECONSTRUCTION OF THE WEST SECTION OF THE GENERAL AVIATION RAMP. THIS WORK INCLUDES BITUMINOUS PAVEMENT MILLING, 6" PORTLAND CEMENT CONCRETE (PCC) PAVING, TIE-DOWN REMOVAL, TIE-DOWN CONSTRUCTION, MARKING, INLET ADJUSTMENT, AND INCIDENTAL ITEMS.

UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. **CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123.** CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

HEIGHT OF CONSTRUCTION EQUIPMENT

THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 25 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A CONCRETE TRUCK.

HAUL ROUTE AND VEHICLE PARKING

THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND PARKING AREA AS SHOWN ON THIS SHEET. THE PROPOSED PARKING AREA WILL BE 125' X 250' (APPROXIMATE). THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE (INCLUDING THE ENTRANCE) AND PARKING AREA THROUGHOUT THE COURSE OF THE PROJECT. ANY AREAS DAMAGED OUTSIDE OF THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AND AT THE CONTRACTOR'S OWN EXPENSE. THE CONTRACTOR SHALL SUPPLY, PLACE, AND REMOVE ANY TEMPORARY PIPE CULVERTS NECESSARY TO CONSTRUCT THE HAUL ROUTE AT NO ADDITIONAL COST TO THE PROJECT. AT THE CONCLUSION OF THE PROJECT THE CONTRACTOR WILL GRADE, FERTILIZE, SEED AND MULCH ALL AREAS CURRENTLY TURFED WITHIN THE HAUL ROUTE AND PARKING AREA LIMITS, AS NEEDED, TO RESTORE IT TO ITS ORIGINAL STATE. ALL AREAS OF THE HAUL ROUTE THAT ARE WITHIN EXISTING CROP LINES SHALL BE TILLED TO ELIMINATE THE COMPACTION OF THE SOIL THAT OCCURS OVER THE COURSE OF THE PROJECT, TO THE SATISFACTION OF THE RESIDENT ENGINEER. IN ALL CASES ONLY ORGANIC MATERIAL (SOIL OR TURF) SHALL EXIST IN THE AREA OF THE HAUL ROUTE FOLLOWING ITS RESTORATION. RESTORATION OF THE HAUL ROUTE AND PARKING AREA WILL BE PAID FOR IN ACCORDANCE WITH ITEM AR150540 HAUL ROUTE, PER LUMP SUM, AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED.

AIRPORT SECURITY NOTE

AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. ONLY CONTRACTOR EMPLOYEES OR MATERIAL SUPPLIERS SHALL BE ALLOWED WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL COMPLY WITH THE AIRPORT MANAGER, OR HIS AUTHORIZED REPRESENTATIVE, IN REGARD TO ALL SECURITY MATTERS.

CONTRACTOR RESPONSIBILITIES

THE CONTRACTOR'S EQUIPMENT PARKING AND STORAGE AREA WILL BE AS SHOWN ON THIS SHEET. THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR VEHICLES IN THIS AREA. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE THIS AREA.

THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREA AND ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.

THE CONTRACTOR SHALL MAINTAIN CONTINUOUS TAXIWAY ACCESS TO ALL HANGARS AND ADMINISTRATIVE AREAS.

METHODS TO REDUCE DUST IN THE PROJECT AREA TO THE SATISFACTION OF THE AIRPORT MANAGER IS REQUIRED AND SHALL BE CONSIDERED INCIDENTAL TO THE APRON PROJECT AND NO EXTRA COMPENSATION IS ALLOWED.

ALL WORK PERFORMED SHALL BE DONE IN A ORDERLY AND EFFECTIVE MANNER TO MINIMIZE IMPACT TO AIRCRAFT OPERATIONS.

TRENCHES OR HOLES MAY REMAIN OPEN OVERNIGHT BUT SHALL BE ENTIRELY BARRICADED.

NO RUNWAY SHALL BE CLOSED OVERNIGHT.

CONTROL POINT DATA

NO.	DESCRIPTION	NORTHING	EASTING	ELEVATION
1	NGS MON., STEEL ROD IN SLEEVE	1,403,476.08	2,161,141.29	684.61
2	IRON PIN	1,402,486.40	2,160,694.36	N/A
3	IRON PIN	1,402,927.42	2,160,214.70	N/A
4	CHSLD BOX ON W. SIDE PCC FOUNDATION OF AIRPORT BEACON			686.12
5	CHSLD BOX ON N. SIDE PCC FOUNDATION OF FLAGPOLE			688.94

150-ENGINEER'S FIELD OFFICE NOTES

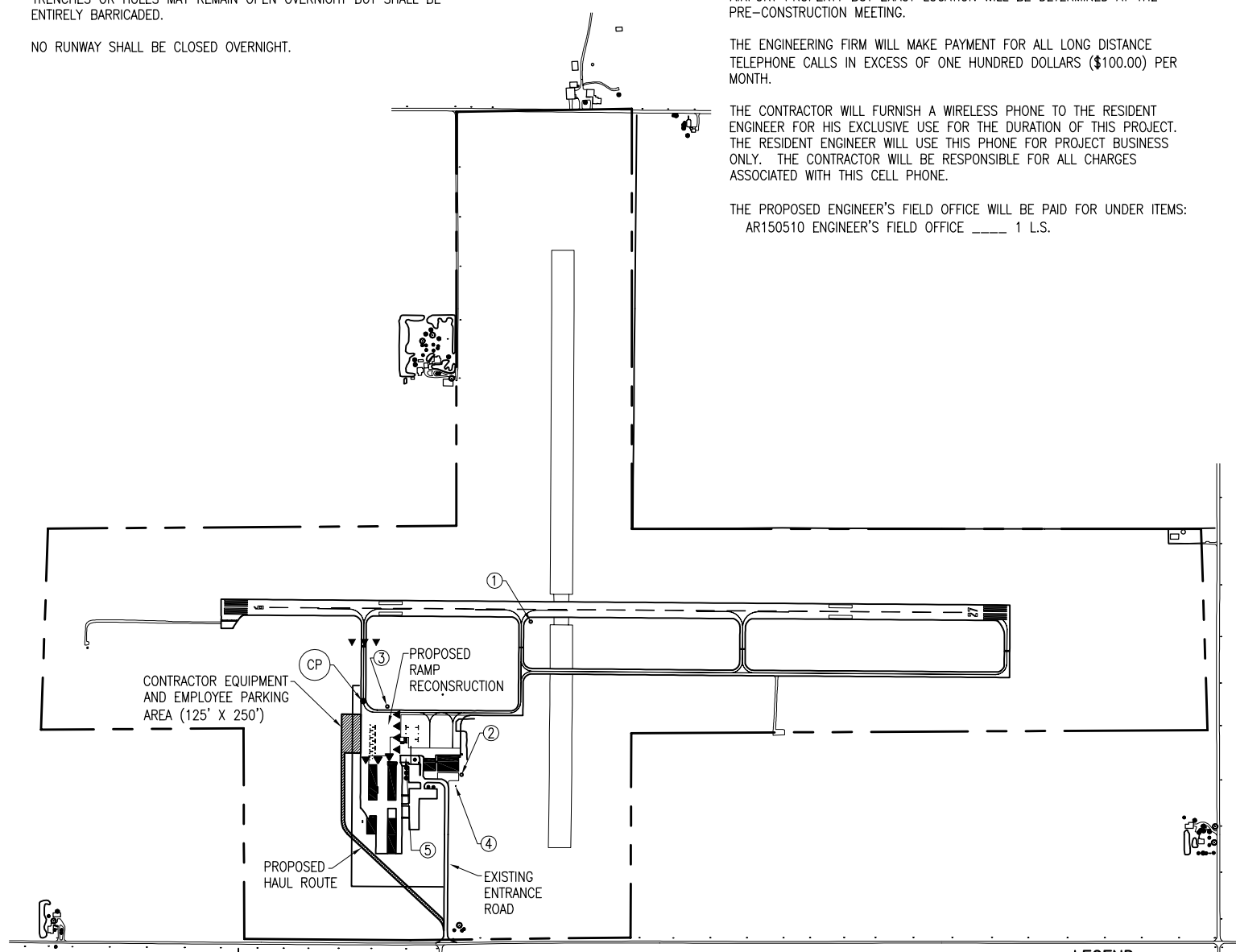
THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE FURNISHED, MAINTAINED, AND REMOVED IN ACCORDANCE WITH ITEM AR150510 "ENGINEER'S FIELD OFFICE" AS STATED ON PAGE 49 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS NOV. 2, 2009.

THE LOCATION OF THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE ON AIRPORT PROPERTY BUT EXACT LOCATION WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.

THE ENGINEERING FIRM WILL MAKE PAYMENT FOR ALL LONG DISTANCE TELEPHONE CALLS IN EXCESS OF ONE HUNDRED DOLLARS (\$100.00) PER MONTH.

THE CONTRACTOR WILL FURNISH A WIRELESS PHONE TO THE RESIDENT ENGINEER FOR HIS EXCLUSIVE USE FOR THE DURATION OF THIS PROJECT. THE RESIDENT ENGINEER WILL USE THIS PHONE FOR PROJECT BUSINESS ONLY. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL CHARGES ASSOCIATED WITH THIS CELL PHONE.

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE PAID FOR UNDER ITEMS: AR150510 ENGINEER'S FIELD OFFICE ____ 1 L.S.



BARRICADES AND TRAFFIC CONES

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND TRAFFIC CONES AS DIRECTED BY THE AIRPORT MANAGER, OR HIS AUTHORIZED REPRESENTATIVE. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING OR RED STEADY-BURN LIGHTS AND 20" SQUARE ORANGE FLAGS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE PAID FOR IN ACCORDANCE WITH AR150530 TRAFFIC MAINTENANCE, PER LUMP SUM, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE AIRPORT MANAGER, OR HIS AUTHORIZED REPRESENTATIVE SHALL ISSUE THE NOTAM ASSOCIATED WITH ANY TAXIWAY CLOSURE.

LEGEND

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED HAUL ROUTE AND EQUIPMENT PARKING AREA
- PROPOSED BENCHMARK
- PROPOSED BARRICADES
- CRITICAL POINT

J.U.L.I.E. INFORMATION

COUNTY _____ McDONOUGH
 CITY _____ MACOMB
 TOWNSHIP _____ MACOMB T-6-N, R-2-W
 SECTION NO. _____ 7 & 8
 ADDRESS _____ MACOMB MUNICIPAL AIRPORT
 RR #1
 MACOMB, ILLINOIS 61455

MB027

CERTIFIED PAYROLLS

THE RESIDENT ENGINEER CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THE PERIOD HAVE BEEN RECEIVED.

MATERIAL CERTIFICATION

COMPLETED WORK CANNOT BE PLACED ON A CONSTRUCTION REPORT UNTIL ALL MATERIAL CERTIFICATIONS FOR THAT PAY ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER.

PROPOSED SAFETY PLAN

GENERAL - THE MACOMB MUNICIPAL AIRPORT IS COMPRISED OF TWO RUNWAYS. THE PROPOSED CONSTRUCTION DOES NOT ANTICIPATE REQUIRING RUNWAY CLOSURES.

THE TAXIWAY ADJACENT TO THE RAMP TO THE NORTH AND THE EAST WILL BE CLOSED ANY TIME WORK IS BEING ACCOMPLISHED WITHIN THE PROJECT LIMITS AND WITHIN 80' OF THE TAXIWAY CENTERLINE. ANY TIME THE GRADE OFF THE EDGE OF PAVEMENT EXCEEDS A DEPTH OF 3", BARRICADES WILL BE PLACED AS SHOWN IN ACCORDANCE WITH ALL OTHER NOTES ON THIS SHEET. ACCESS TO THE TWO HANGARS SOUTH OF THE PROJECT LIMIT WILL BE CONTINUOUS. HOWEVER TAXING OF AIRCRAFT IN THIS AREA IS PROHIBITED, WITH TRANSPORT OF ALL AIRCRAFT IN THIS AREA BEING UNDER TOW.

IDENTIFICATION - WHEN THE CONTRACTORS VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3') FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE). THE CONTRACTOR WILL ALSO PROVIDE WORKERS WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THE PERSON AS BEING PART OF THE CONSTRUCTION CREW.

RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (122.80 MHz.) WITH THE AIRPORT UNICOM. THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE MACOMB MUNICIPAL AIRPORT AND ENABLE THE AIRPORT TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

EROSION CONTROL

THIS PROJECT WILL DISTURB LESS THAN 1 ACRE OF LAND, THEREFORE A N.P.D.E.S. PERMIT WILL NOT BE REQUIRED.

CRITICAL POINT DATA

CRITICAL POINT (CP)
 LATITUDE: 40° 31' 05.00"
 LONGITUDE: 90° 39' 27.84"
 ELEVATION: 692.35' M.S.L.

DATE	REVISION

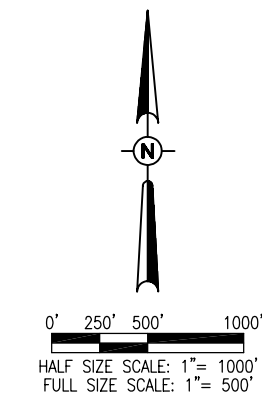
MACOMB MUNICIPAL AIRPORT
 MACOMB, ILLINOIS
 A.I.P. PROJ.: 3-17-0064-B19
 I.L. PROJ.: MQB-4010

Hanson Project No. 09A0150D_0001	03/10/10
Filename R-0035FY.DWG	DAK
Scale 1" = 500'	DPE
Date 04/16/10	JDW
LAYOUT	REVIEWED
DRAWN	DATE



RECONSTRUCT RAMP
 PROPOSED SAFETY PLAN

APR 14, 2010 11:47 AM HARR01115
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BITUMINOUS PAVEMENT MILLING (AR401650)

THE DESIGNATED MILLING AREAS ON THIS SHEET WILL BE CUT OR TRIMMED. THE TOP 6" (INCHES) OF THE EXISTING BITUMINOUS SURFACE/BASE COURSE MATERIAL WILL BE REMOVED BY ROTO-MILLING. THE TOLERANCE OF THE MILLING WILL BE AS STATED IN THE SPECIAL PROVISIONS FOR BITUMINOUS SURFACE COURSE, 401-4.14 SURFACE TEST OF STANDARD SPECIFICATIONS.

WHERE THE BITUMINOUS SURFACING MATCHES THE EXISTING BITUMINOUS PAVEMENT TO REMAIN, THE EXISTING PAVEMENT WILL BE SAWED IF A VERTICAL FACE IS NOT ACHIEVED BY THE MILLING OPERATIONS. SAWING WILL BE PAID FOR UNDER ITEM AR401665 - BITUMINOUS PAVEMENT SAWING - PER LINEAR FOOT.

AFTER MILLING OPERATION IS COMPLETE, THE PAVEMENT SHALL BE INSPECTED AND ACCEPTED BY THE RESIDENT ENGINEER PRIOR TO PROCEEDING WITH THE PROPOSED PCC PAVEMENT.

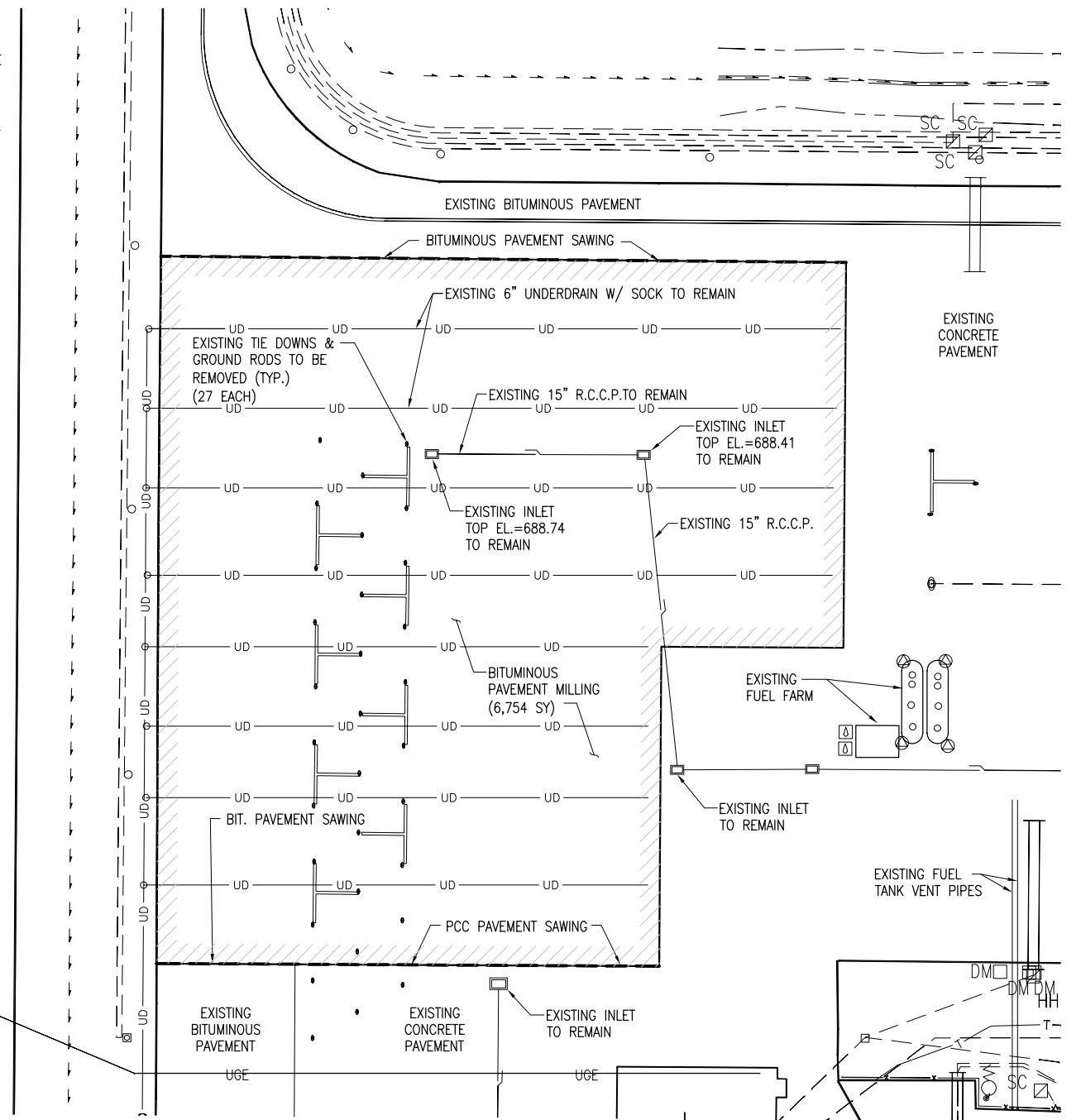
PRIOR TO PAVING THE APRON, ALL MILLED MATERIAL WILL BE BROOMED AND BLOWN CLEAN TO THE ACCEPTANCE OF THE RESIDENT ENGINEER.

ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER.

EXISTING DRAINAGE SYSTEM

THE EXISTING UNDER GROUND DRAINAGE SYSTEM CONSISTING OF UNDERDRAINS, INLETS, AND RCCP SHALL REMAIN. THE CONTRACTOR SHALL TAKE PRECAUTION TO NOT DISTURB OR DAMAGE THE EXISTING SYSTEM. ANY DISTURBANCE OR DAMAGE SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT HIS OWN EXPENSE.



LEGEND

[Symbol]	EXISTING PAVEMENT
[Symbol]	PROPOSED CONSTRUCTION LIMITS
[Symbol]	EXISTING BUILDING
[Symbol]	PROPOSED PAVEMENT SAWING
[Symbol]	EXISTING ELECTRICAL DUCT
[Symbol]	EXISTING TOP OF DITCH
[Symbol]	EXISTING BOTTOM OF DITCH
[Symbol]	EXISTING ELECTRICAL CABLES
[Symbol]	EXISTING 6" UNDERDRAIN
[Symbol]	EXISTING STORM SEWER
[Symbol]	EXISTING SANITARY SEWER
[Symbol]	EXISTING UNDERGROUND ELECTRIC
[Symbol]	EXISTING WATER LINE
[Symbol]	EXISTING TELEPHONE LINE
[Symbol]	EXISTING UNDERGROUND ELECTRIC
[Symbol]	EXISTING FENCE
[Symbol]	EXISTING UNDERGROUND FUEL TANK
[Symbol]	EXISTING STAKE MOUNTED TAXIWAY LIGHT
[Symbol]	EXISTING BASE MOUNTED TAXIWAY LIGHT
[Symbol]	EXISTING ELECTRICAL HANDHOLE
[Symbol]	EXISTING INLET
[Symbol]	EXISTING ELECTRICAL TRANSFORMER
[Symbol]	EXISTING BEACON
[Symbol]	EXISTING JUNCTION BOX
[Symbol]	EXISTING ELECTRICAL SERVICE METER
[Symbol]	EXISTING WATER WELL
[Symbol]	EXISTING ENTRANCE ROAD/PARKING LOT LIGHT
[Symbol]	EXISTING DUCT MARKER
[Symbol]	EXISTING FUEL PUMP
[Symbol]	EXISTING FUEL MONITORING WELL
[Symbol]	EXISTING INSPECTION HOLE

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TIE DOWN REMOVAL NOTES

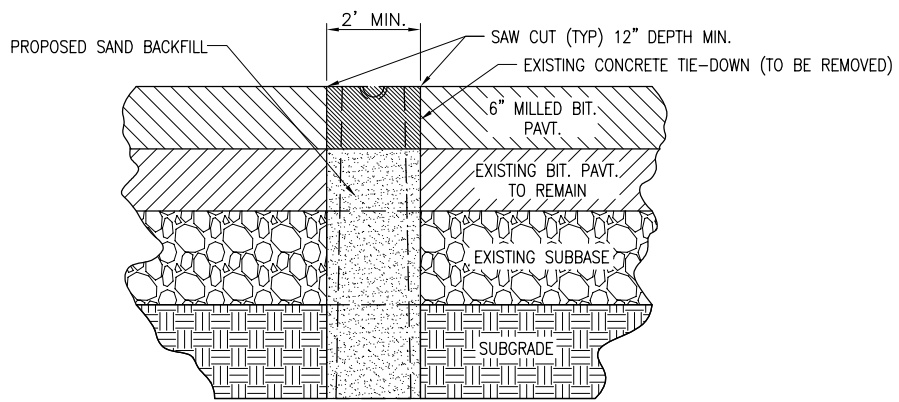
THE EXISTING TIE DOWNS AND GROUND RODS IDENTIFIED SHALL BE REMOVED AND DISPOSED OF IN A LEGAL MANNER OFF AIRPORT PROPERTY IN ACCORDANCE WITH THE DETAILS ON THIS SHEET.

THE CONTRACTOR WILL SEPARATE THE TIE DOWNS TO BE REMOVED FROM THE SURROUNDING PAVEMENT PRIOR TO PULLING THE TIE DOWN OUT. REMOVAL OF THE TIE DOWN SHALL NOT CAUSE ANY DISPLACEMENT OF THE SURROUNDING PAVEMENT. ANY DISPLACEMENT WILL BE CORRECTED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER.

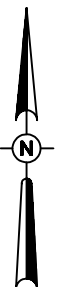
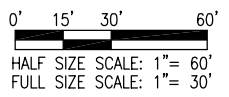
THE CONTRACTOR SHALL FILL THE HOLE WITH SAND.

REMOVAL OF GROUND RODS, BACKFILLING THE HOLES WITH SAND, AND COMPACTION TO THE SATISFACTION OF THE RESIDENT ENGINEER SHALL BE CONSIDERED INCIDENTAL TO AND PAID FOR UNDER ITEM:

AR510900 "REMOVE TIE DOWN" _____ 27 EA.



TIE DOWN REMOVAL DETAIL
"NOT TO SCALE"



APR 14, 2010 11:47 AM HARR01115 I:\AIRPORTS\MACOMB\09A0150\CADD\AIRPORT\SHEET\R-101SIT.DWG

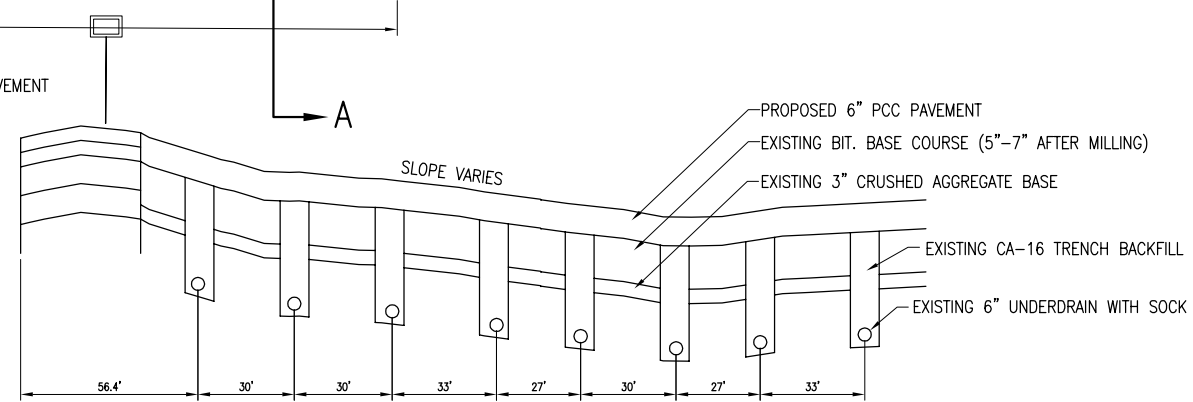
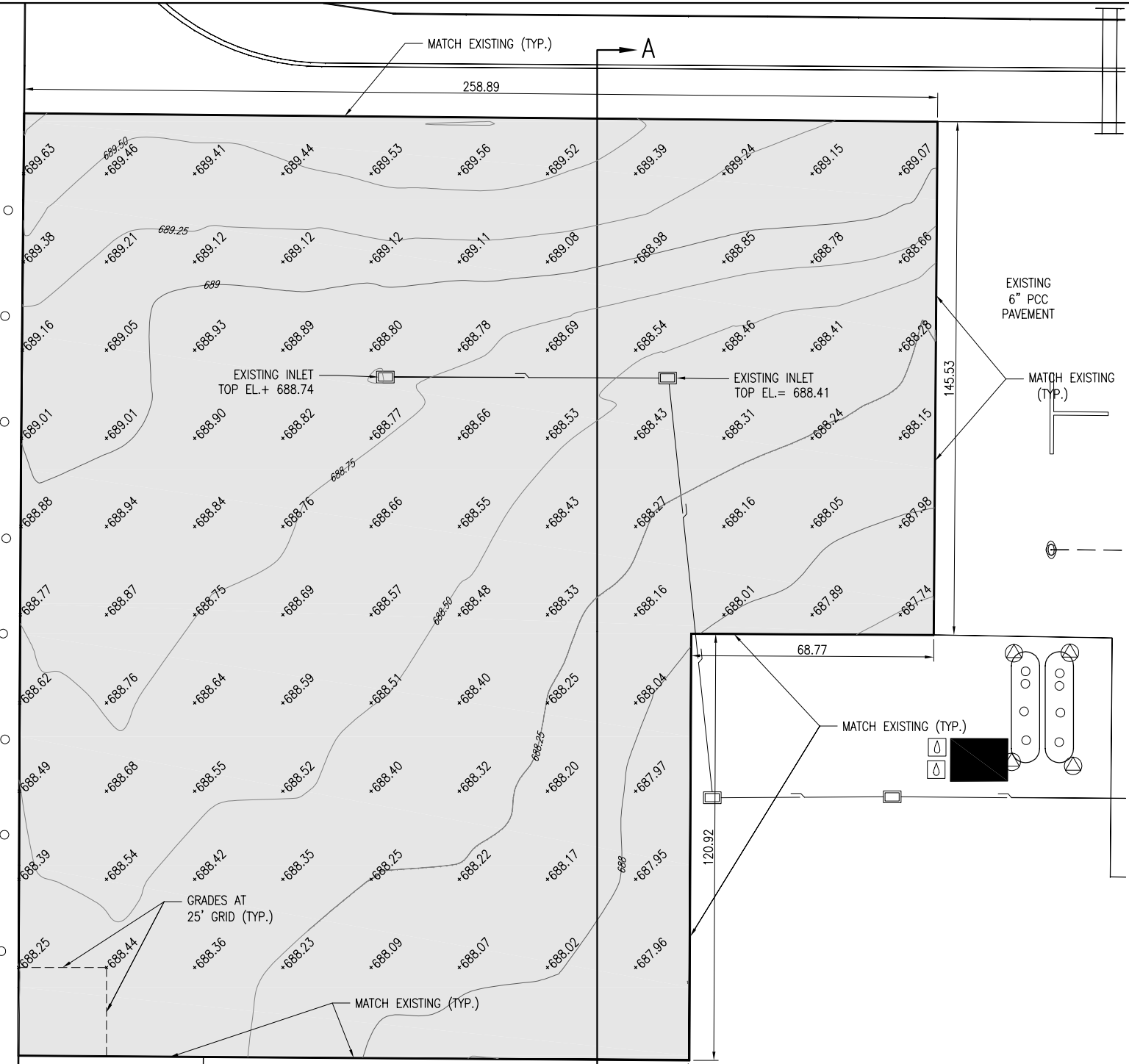
MB027

DATE		REVISION							
MACOMB MUNICIPAL AIRPORT MACOMB, ILLINOIS									
A.I.P. PROJ.: 3-17-0064-B19 IL PROJ.: MQB-4010									
Hanson Project No. 09A0150D_0001 Filename R-101SIT.DWG Scale 1" = 30' Date 04/16/10									
LAYOUT	DAK	03/10/10							
DRAWN	DPE	03/10/10							
REVIEWED	JDW	03/18/10							
HANSON Hanson Professional Services Inc. 1626 South State Street Springfield, IL 62702-2866 Offices Nationwide									
RECONSTRUCT RAMP	EXISTING SITE PLAN								
4									
4 of 8 sheets									

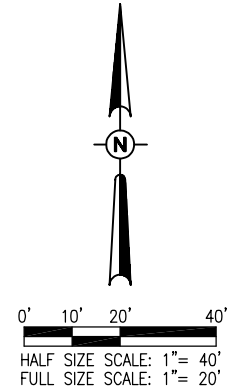
NOTE: EXISTING INLETS SHALL REMAIN AT THE EXISTING ELEVATION. THE CONTRACTOR SHALL TAKE PRECAUTION TO NOT DISTURB OR DAMAGE THE EXISTING DRAINAGE STRUCTURES. ANY DISTURBANCE OR DAMAGE SHALL BE CORRECTED OR REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.

MB027

- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED IMPROVEMENTS
 - EXISTING BUILDING
 - EXISTING ELECTRICAL DUCT
 - EXISTING TOP OF DITCH
 - EXISTING BOTTOM OF DITCH
 - EXISTING ELECTRICAL CABLES
 - EXISTING UNDERDRAIN
 - EXISTING STORM SEWER
 - EXISTING UNDERGROUND ELECTRIC
 - EXISTING STAKE MOUNTED TAXIWAY LIGHT
 - EXISTING BASE MOUNTED TAXIWAY LIGHT
 - EXISTING INLET
 - EXISTING INSPECTION HOLE



PROPOSED TYPICAL SECTION "A-A"
"NOT TO SCALE"



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APR 14, 2010 11:47 AM HARR01115 I:\AIRPORTS\MACOMB\09A0150\CADD\AIRPORT\SHEET\R-121CON-A.DWG

BY																													
REVISION																													
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MACOMB MUNICIPAL AIRPORT MACOMB, ILLINOIS																													
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<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td colspan="2">Hanson Project No. 09A0150D_0001</td> <td colspan="2">Date: 04/16/10</td> </tr> <tr> <td colspan="2">Filename: R-121CON-A.DWG</td> <td colspan="2">Scale: 1" = 20'</td> </tr> <tr> <td>LAYOUT</td> <td>DAK</td> <td>03/10/10</td> <td></td> </tr> <tr> <td>DRAWN</td> <td>DPE</td> <td>03/10/10</td> <td></td> </tr> <tr> <td>REVIEWED</td> <td>JDW</td> <td>03/18/10</td> <td></td> </tr> </table>										Hanson Project No. 09A0150D_0001		Date: 04/16/10		Filename: R-121CON-A.DWG		Scale: 1" = 20'		LAYOUT	DAK	03/10/10		DRAWN	DPE	03/10/10		REVIEWED	JDW	03/18/10	
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REVIEWED	JDW	03/18/10																											
<p>Hanson Professional Services Inc. 1626 South State Street Springfield, IL 62702-2866 Offices Nationwide</p>																													
RECONSTRUCT RAMP	PROPOSED CONSTRUCTION PLAN																												
5																													
5 of 8 sheets																													

JOINTING NOTES

ALL EXPOSED JOINT EDGES SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/4" OR STONED TO PRODUCE THE 1/8" CHAMFER.

ALL LONGITUDINAL AND TRANSVERSE CONTRACTION AND CONSTRUCTION JOINTS SHALL BE SAWED.

CERTIFICATION FOR ALL REINFORCING STEEL, DOWELS AND REBAR SHALL BE PROVIDED **BEFORE** PAVING BEGINS WITH **NO EXCEPTION**.

ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE SURFACE OF THE PAVEMENT AND TO THE CENTERLINES OF THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION. DOWELS FOR THE 6" PAVEMENT SHALL BE 3/4" DIA., 18" LENGTH AND 12" SPACING.

ALL TIE-BARS SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT. SUPPORT PINS SHALL BE OF SUFFICIENT LENGTH TO PENETRATE AT LEAST 6" INTO THE SUBGRADE.

ALL TIE-BARS SHALL BE PLACED AT A POINT NOT EXCEEDING 15" OR CLOSER THAN 6" FROM A TRANSVERSE, CONTRACTION, EXPANSION, OR CONSTRUCTION JOINT AND SPACED 36" ON CENTERS AND SHALL BE **NO. 4 DEFORMED BARS, 20" IN LENGTH.**

DOWELS IN TRANSVERSE, CONTRACTION, AND CONSTRUCTION JOINTS SHALL BE EPOXY COATED AND HALF THE LENGTH GREASED WITH A HEAVY GREASE.

ALLOWABLE TOLERANCES FOR GROOVE DEPTH WILL BE +1/8" FOR CONSTRUCTION JOINTS AND +1/4" FOR CONTRACTION JOINTS.

DOWELS LOCATED IN THE CONSTRUCTION JOINTS WILL BE DRILLED AND EPOXIED IN PLACE. THE EPOXY MATERIAL MUST BE APPROVED BY THE ILLINOIS DIVISION OF AERONAUTICS

THE COST OF ALL DOWEL BARS, TIE-BARS, SAWING AND SEALING SHALL BE INCLUDED IN THE COST OF THE P.C.C. PAVEMENT.

PRIOR TO PLACING ADJACENT PAVEMENT SECTIONS, THE VERTICAL EDGE SHALL BE CHECKED FOR TRUENESS IF THE FACE IS BURRED OR IRREGULAR, THE CONTRACTOR SHALL GRIND, STONE OR SAW THE FACE A MINIMUM OF 2" IN DEPTH TO PRODUCE A SMOOTH AND STRAIGHT EDGE.

WHEN CONSTRUCTING "FILL-IN" PAVEMENT LANES THE CONTRACTOR SHALL USE BELTING OR OTHER PROTECTIVE MATERIAL FOR THE PAVING MACHINE TO TRAVEL ON AND WILL ROPE THE TRANSVERSE JOINTS.

JOINT SEALANT SHALL BE AS REQUIRED IN THE SPECIFICATIONS ITEM 501-2.5.

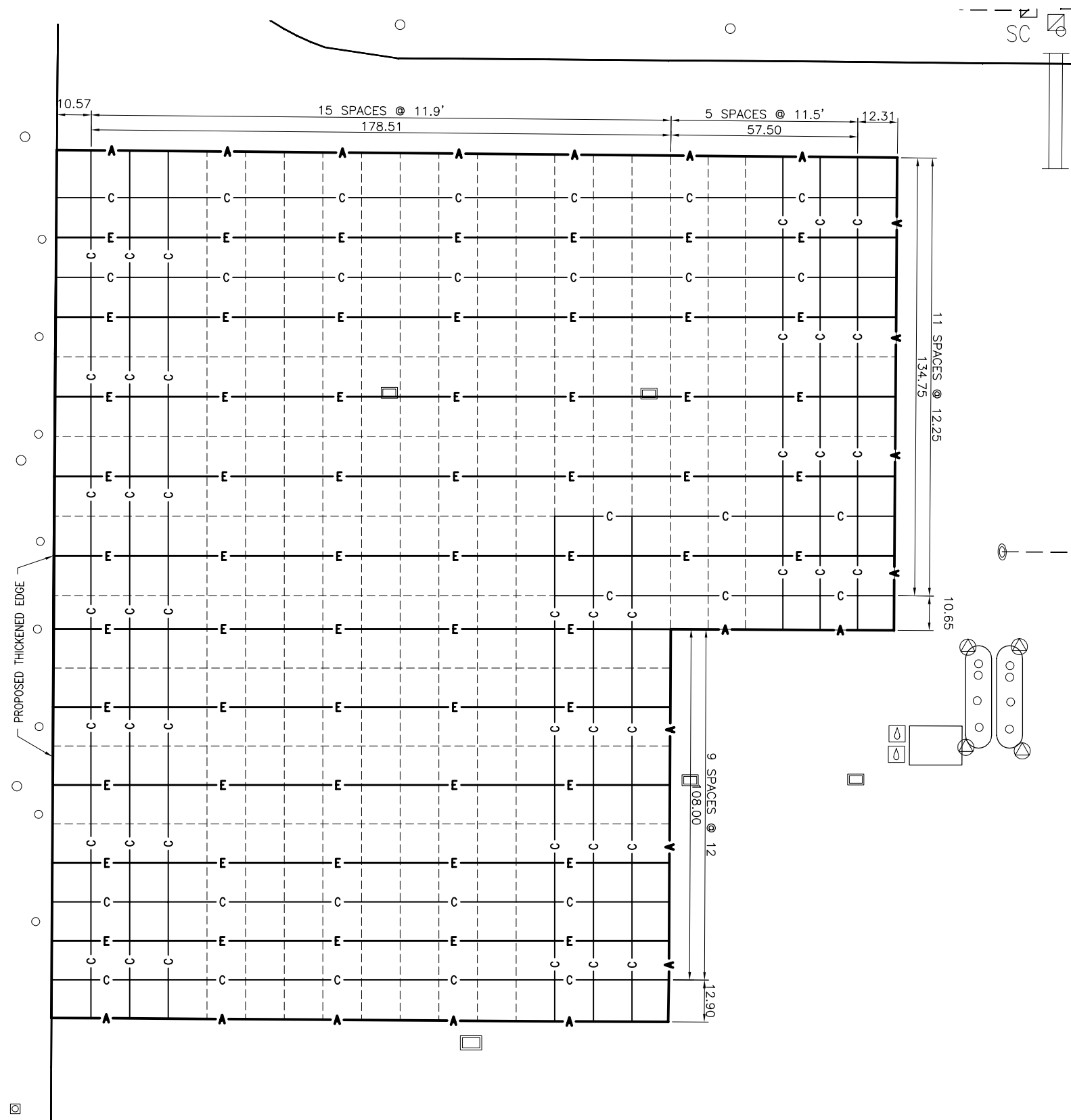
CURING COMPOUND WILL BE AS REQUIRED IN THE SPECIFICATIONS, ITEM 501-3.17 AND SHALL BE APPROVED PRIOR TO THE PAVING OPERATION BY THE ENGINEER.

ALL NON-ALIGNED EDGES WILL BE SAWED FULL DEPTH.

ALL REFERENCES TO METHOD 1 PAVING IN THE SPECIFICATIONS SHALL BE APPLIED TO THIS PROJECT.

INLET NOTE

ISOLATE EXISTING INLETS AT GRADE, SEE DETAIL.



LEGEND

- [Solid Line] EXISTING PAVEMENT
- [Dashed Line] PROPOSED IMPROVEMENTS
- [Thick Solid Line] EXISTING BUILDING
- [Line with 'A'] PROPOSED TYPE A THICKENED EDGE EXPANSION JOINT
- [Line with 'C'] PROPOSED TYPE C DOWELED CONTRACTION JOINT
- [Dashed Line with 'D'] PROPOSED TYPE D DUMMY CONTRACTION JOINT
- [Line with 'E'] PROPOSED E DOWELED CONSTRUCTION JOINT
- [Thick Solid Line] PROPOSED THICKENED EDGE
- [Double Line] EXISTING ELECTRICAL DUCT
- [Dashed Line] EXISTING THICKENED EDGE
- [Dashed Line] EXISTING JOINTS
- [Circle with 'X'] EXISTING UNDERGROUND FUEL TANK
- [Circle with 'S'] EXISTING STAKE MOUNTED TAXIWAY LIGHT
- [Circle with 'B'] EXISTING BASE MOUNTED TAXIWAY LIGHT
- [Square] EXISTING INLET
- [Square with 'P'] EXISTING FUEL PUMP
- [Circle with 'M'] EXISTING FUEL MONITORING WELL
- [Circle] EXISTING INSPECTION HOLE

MB027

DATE	REVISION	BY

**MACOMB MUNICIPAL AIRPORT
MACOMB, ILLINOIS**

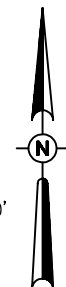
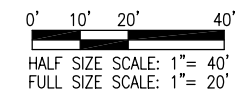
IL PROJ.: MQB-4010 A.I.P. PROJ.: 3-17-0064-B19

Hanson Project No. 09A0150D_0001	DAK	03/10/10
Filename R-161JNT.DWG	DAK/DPE	03/10/10
Scale 1" = 20'	JDW	03/18/10
Date 04/16/10		

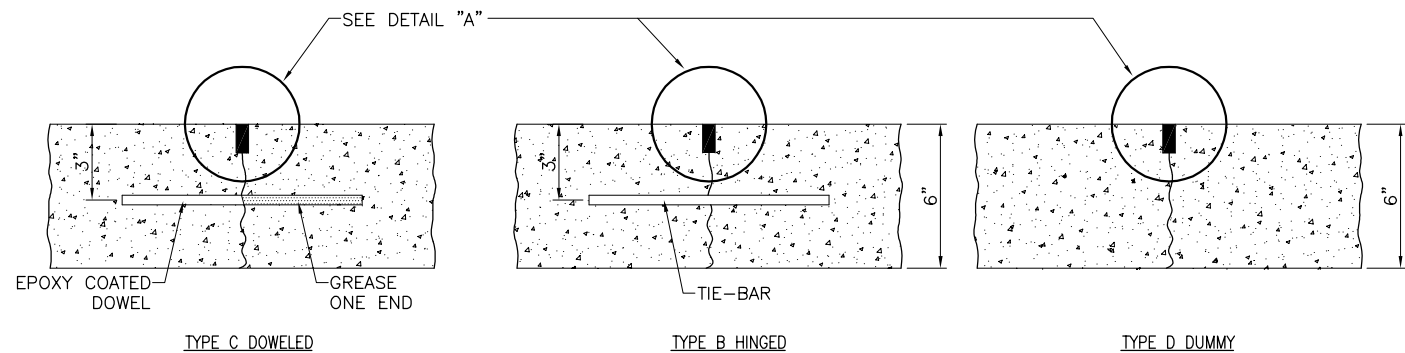
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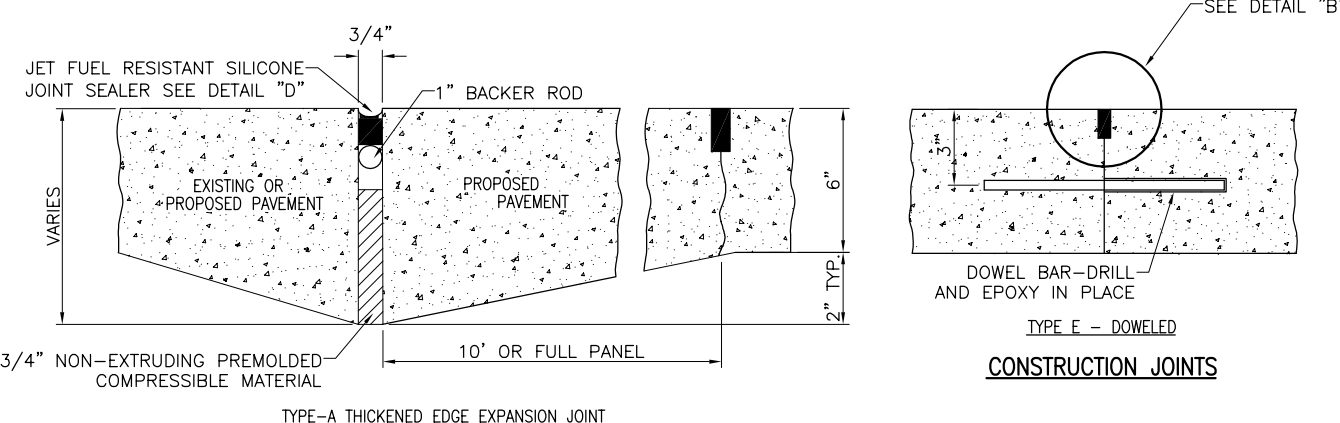
**RECONSTRUCT
RAMP
PROPOSED
JOINTING
PLAN**



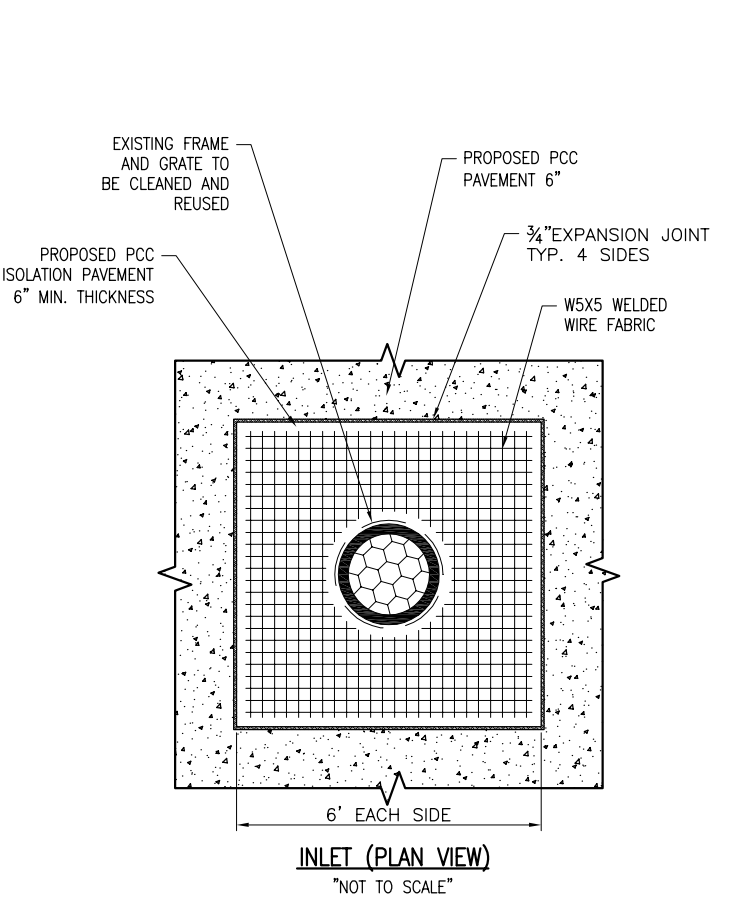
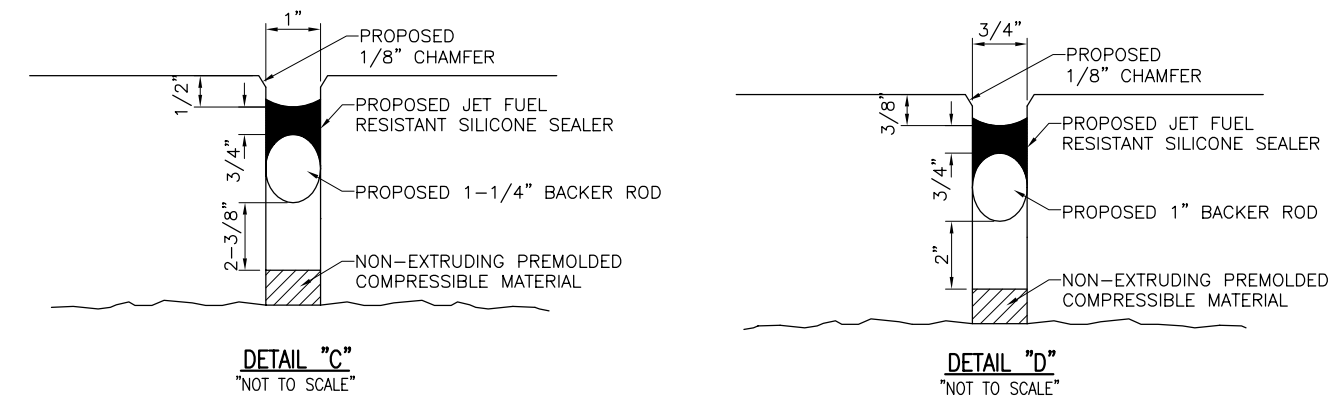
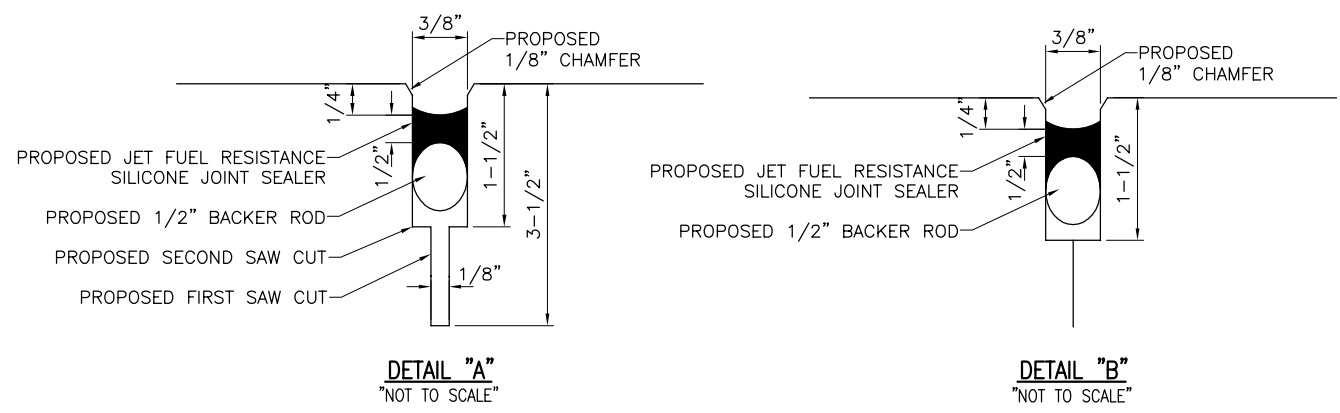
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CONTRACTION JOINTS



EXPANSION JOINTS



JOINTING NOTES

ALL EXPOSED JOINT EDGES SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/4" OR STONED TO PRODUCE THE 1/8" CHAMFER.

ALL LONGITUDINAL AND TRANSVERSE CONTRACTION AND CONSTRUCTION JOINTS SHALL BE SAWS.

ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE SURFACE OF THE PAVEMENT AND TO THE CENTERLINES OF THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION. DOWELS FOR THE 6" PAVEMENT SHALL BE 3/4" DIA., 18" LENGTH AND 12" SPACING.

ALL TIE-BARS SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT. SUPPORT PINS SHALL BE OF SUFFICIENT LENGTH TO PENETRATE AT LEAST 6" INTO THE SUBGRADE.

ALL TIE-BARS SHALL BE PLACED AT A POINT NOT EXCEEDING 15" OR CLOSER THAN 6" FROM A TRANSVERSE, CONTRACTION, EXPANSION, OR CONSTRUCTION JOINT AND SPACED 30" ON CENTERS AND SHALL BE NO. 4 DEFORMED BARS, 30" IN LENGTH.

DOWELS IN TRANSVERSE, CONTRACTION, AND CONSTRUCTION JOINTS SHALL BE EPOXY COATED AND HALF THE LENGTH GREASED WITH A HEAVY GREASE.

ALLOWABLE TOLERANCES FOR GROOVE DEPTH WILL BE +1/8" FOR CONSTRUCTION JOINTS AND +1/4" FOR CONTRACTION JOINTS.

DOWELS LOCATED IN THE CONSTRUCTION JOINTS WILL BE DRILLED AND EPOXY IN PLACE. THE EPOXY MATERIAL MUST BE APPROVED BY THE ILLINOIS DIVISION OF AERONAUTICS

THE COST OF ALL DOWEL BARS, TIE-BARS, CURING COMPOUND, SAWING AND SEALING SHALL BE INCLUDED IN THE COST OF THE P.C.C. PAVEMENT.

PRIOR TO PLACING ADJACENT PAVEMENT SECTIONS, THE VERTICAL EDGE SHALL BE CHECKED FOR TRUENESS IF THE FACE IS BURRED OR IRREGULAR, THE CONTRACTOR SHALL GRIND, STONE OR SAW THE FACE A MINIMUM OF 2" IN DEPTH TO PRODUCE A SMOOTH AND STRAIGHT EDGE.

WHEN CONSTRUCTING "FILL-IN" PAVEMENT LANES THE CONTRACTOR SHALL USE BELTING OR OTHER PROTECTIVE MATERIAL FOR THE PAVING MACHINE TO TRAVEL ON AND WILL ROPE THE TRANSVERSE JOINTS.

JOINT SEALANT SHALL BE AS REQUIRED IN THE SPECIFICATIONS ITEM 501-2.5.

CURING COMPOUND WILL BE AS REQUIRED IN THE SPECIFICATIONS, ITEM 501-3.17 AND SHALL BE APPROVED PRIOR TO THE PAVING OPERATION BY THE ENGINEER.

ALL NON-ALIGNED EDGES WILL BE SAWS FULL DEPTH.

DATE	REVISION

**MACOMB MUNICIPAL AIRPORT
MACOMB, ILLINOIS**

IL PROJ.: MQB-4010 A.I.P. PROJ.: 3-17-0064-B19

Hanson Project No. 09A0150D_0001	File Name R-561JNT.DWG	Date 04/16/10	LAYOUT	DAK	03/10/10
Scale NOT TO SCALE	Date 04/16/10	REVIEWED	JDW	03/18/10	03/10/10

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Springfield, IL 62702-2886
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RECONSTRUCT RAMP

PROPOSED JOINTING DETAILS

APR 14, 2010 11:48 AM HARR01115
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PAVEMENT MARKING QUANTITIES		
WORK ITEM	YELLOW AREA	BLACK AREA
TAXIWAY CENTERLINE STRIPE	430	430
TIEDOWN MARKING	156	156
GROUND ROD MARKING	20	8
TOTAL	606	594

620-PAVEMENT MARKING-WATERBORNE NOTES

THE PAVEMENT MARKING-WATERBORNE (620) SHALL BE PLACED IN ACCORDANCE WITH ITEM 620 "PAVEMENT MARKING" AS STATED ON PAGE 277 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOV. 2, 2009.

THIS ITEM SHALL CONSIST OF TAXIWAY CENTERLINE, AND TIEDOWN MARKING IN ACCORDANCE WITH THESE SPECIFICATIONS AND AT THE LOCATIONS SHOWN ON THE CONSTRUCTION PLANS. ALL MARKING WILL BE YELLOW IN COLOR WITH A 6-IN BLACK BORDER. THE PROPOSED PAVEMENT MARKING WILL BE APPLIED IN TWO APPLICATIONS.

ANY MATERIAL DELIVERED THAT FAILS TO MEET THE SPECIFICATIONS SHALL BE DISPOSED OF BY THE VENDOR AND IMMEDIATELY REPLACED WITH ACCEPTABLE MATERIAL ENTIRELY AT THE VENDOR'S EXPENSE, INCLUDING HANDLING AND TRANSPORTATION CHARGES.

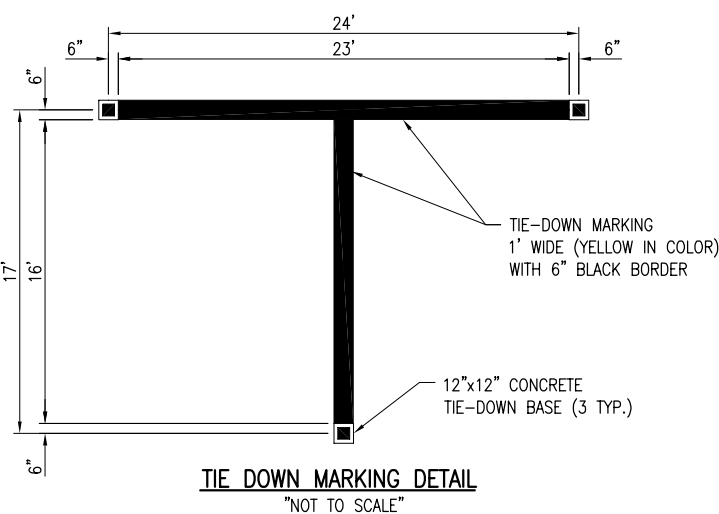
ALL CURING COMPOUND WILL BE CLEANED FROM CONCRETE PAVEMENT PRIOR TO APPLYING PAINT. NO EXCEPTIONS.

ALL PROPOSED MARKING WILL BE COMPLETED IN ACCORDANCE WITH THE DETAILS SHOWN ON THE CONSTRUCTION PLANS.

GLASS BEADS SHALL BE REQUIRED ONLY ON THE SECOND APPLICATION OF YELLOW MARKING.

CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

THE PROPOSED MARKING WILL BE PAID FOR UNDER ITEM:
AR620520 PAVEMENT MARKING-WATERBORNE _____ PER S.F.
AR620525 PAVEMENT MARKING-BLACK BORDER _____ PER S.F.



TIE DOWN MARKING DETAIL
"NOT TO SCALE"

TIE DOWN NOTES

NEW TIE DOWNS WILL BE INSTALLED IN ACCORDANCE WITH DETAIL "C" AT THE LOCATIONS SHOWN ON THIS SHEET.

THE PERIMETER OF EACH PROPOSED TIE DOWN SHALL BE SAWED.

TIE DOWN CASTINGS SHALL BE NEENAH R-3490A OR EQUAL.

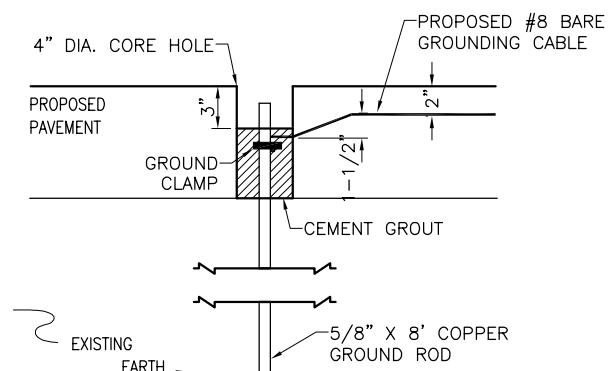
CONCRETE SHALL MEET THE REQUIREMENTS OF 610 - STRUCTURAL CONCRETE.

THE PROPOSED TIE DOWN MARKING WILL BE IN ACCORDANCE WITH "TIE DOWN MARKING DETAIL" ON THIS SHEET AND THE SPECIFICATIONS.

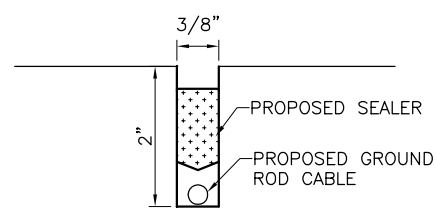
THE TIE DOWNS WILL BE PAID FOR UNDER ITEM:
AR510510 "TIE DOWN" _____ PER EA.

THE GROUND RODS WILL BE PAID FOR UNDER ITEM:
AR510515 "GROUND ROD" _____ PER EA.

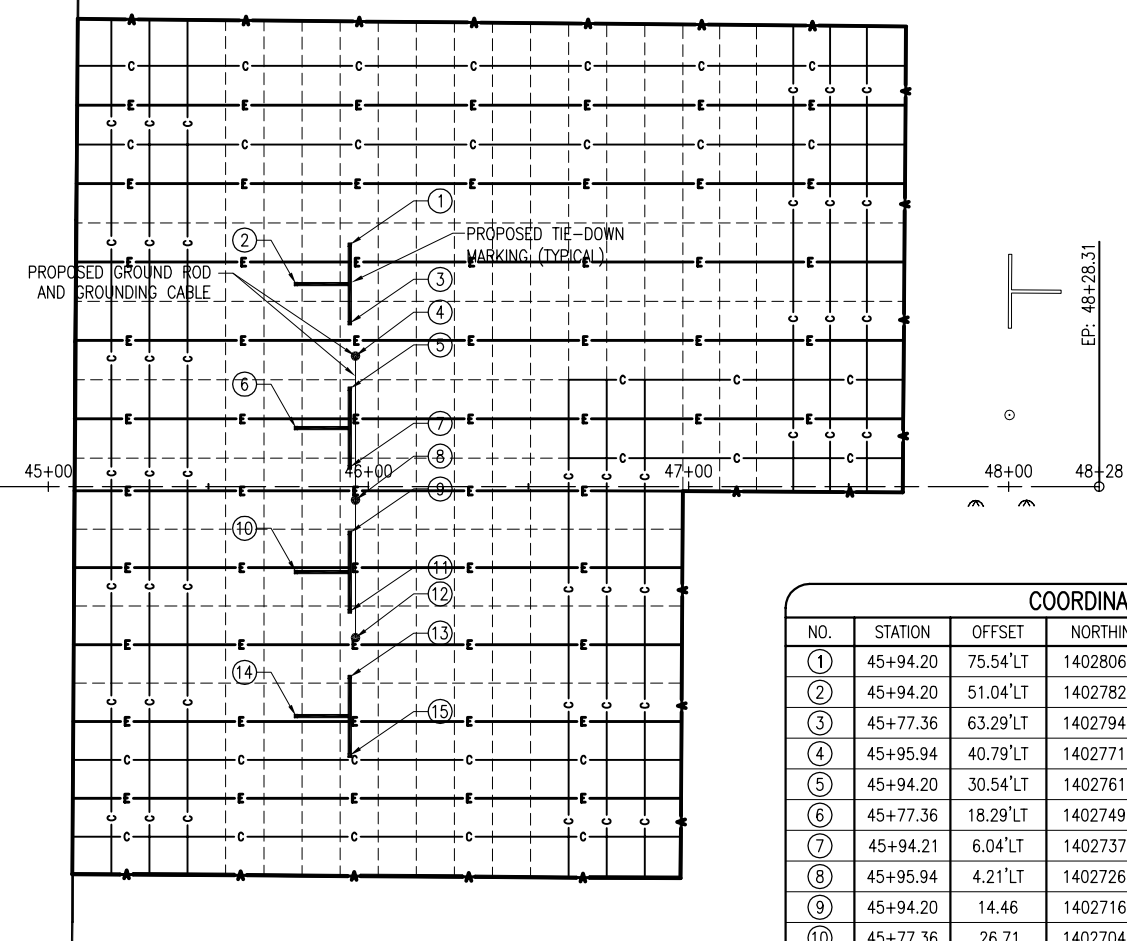
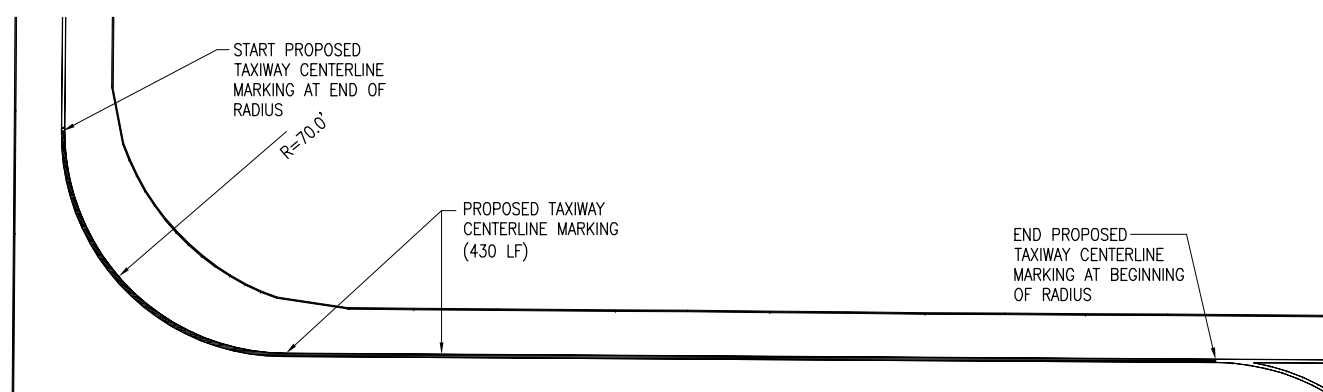
THE PROPOSED #8 BARE GROUNDING CABLE WILL BE CONSIDERED INCIDENTAL TO ITEM AR510515 GROUND ROD AND NO ADDITIONAL COMPENSATION ALLOWED.



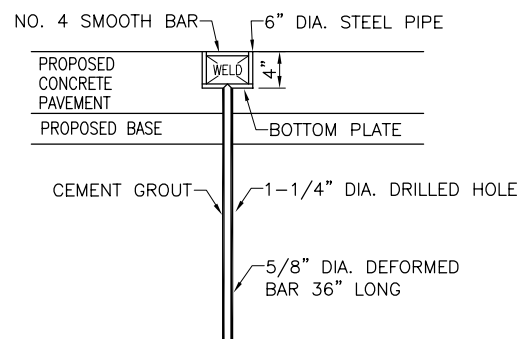
GROUND ROD DETAIL
"NOT TO SCALE"



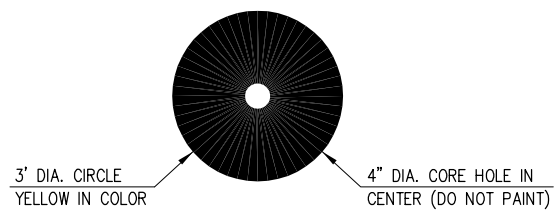
GROUND CABLE DETAIL
"NOT TO SCALE"



COORDINATE DATA TABLE					
NO.	STATION	OFFSET	NORTHING	EASTING	DESCRIPTION
①	45+94.20	75.54'LT	1402806.59	2160128.26	TIEDOWN
②	45+94.20	51.04'LT	1402782.09	2160128.26	TIEDOWN
③	45+77.36	63.29'LT	1402794.34	2160111.41	TIEDOWN
④	45+95.94	40.79'LT	1402771.84	2160130.00	GROUND ROD
⑤	45+94.20	30.54'LT	1402761.59	2160128.26	TIEDOWN
⑥	45+77.36	18.29'LT	1402749.34	2160111.41	TIEDOWN
⑦	45+94.21	6.04'LT	1402737.09	2160128.27	TIEDOWN
⑧	45+95.94	4.21'LT	1402726.84	2160130.00	GROUND ROD
⑨	45+94.20	14.46	1402716.59	2160128.26	TIEDOWN
⑩	45+77.36	26.71	1402704.34	2160111.41	TIEDOWN
⑪	45+94.20	38.96	1402692.09	2160128.26	TIEDOWN
⑫	45+95.94	47.21	1402683.84	2160130.00	GROUND ROD
⑬	45+94.20	59.46	1402671.59	2160128.26	TIEDOWN
⑭	45+77.36	71.71	1402659.34	2160111.41	TIEDOWN
⑮	45+94.20	83.96	1402647.09	2160128.26	TIEDOWN



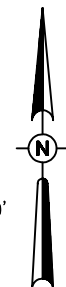
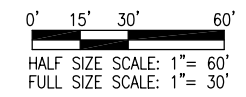
TIEDOWN DETAIL "C"
"NOT TO SCALE"



GROUND ROD MARKING DETAIL
"NOT TO SCALE"

LEGEND

- EXISTING PAVEMENT
- PROPOSED IMPROVEMENTS
- EXISTING BUILDING
- EXISTING UNDERGROUND FUEL TANK
- EXISTING MARKING
- PROPOSED MARKING
- EXISTING BEACON
- EXISTING FUEL PUMP
- EXISTING FUEL MONITORING WELL
- PROPOSED TIE-DOWN



DATE	REVISION

MACOMB MUNICIPAL AIRPORT
MACOMB, ILLINOIS

Hanson Project No. 09A0150D_0001	LAYOUT	DAK	03/10/10
Filename: R-1511MRK.DWG	DRAWN	DAK/DPE	03/10/10
Scale: 1" = 30'	REVIEWED	JDW	03/18/10
Date: 04/16/10			



RECONSTRUCT RAMP
PROPOSED MARKING PLAN

APR 14, 2010 11:48 AM HARR01115
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