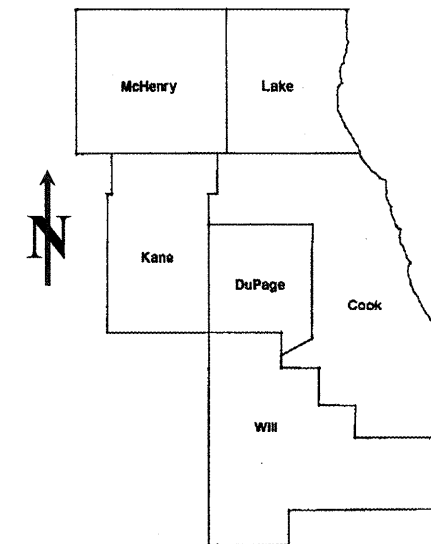


ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NUMBER
VARIOUS	2009-028 PP	DUPAGE	27	1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
DISTRICT ONE
PROPOSED HIGHWAY PLANS

CONTRACT NO. 60G27

D-91-358-09



LOCATION OF IMPROVEMENT INDICATED THUS:

FOR INDEX OF SHEETS SEE SHEET 2

VARIOUS ROUTES
 SECTION: 2009-028 PP
 VARIOUS INTERSTATE RAMPS AND MAINLINE
 PAVEMENT PATCHING
 DUPAGE COUNTY
 C-91-358-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
 SUBMITTED: MARCH 24, 2009
Diane M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
May 8, 2009
Charles J. Ingersoll
ENGINEER OF DESIGN AND ENVIRONMENT
May 8, 2009
Christine M. Reed
DIRECTOR, DIVISION OF HIGHWAYS

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

J.U.L.I.E.: JOINT UTILITY LOCATION
INFORMATION FOR EXCAVATION
(312) 744-7000

CONTRACT NO. 60G27

DISTRICT ONE - DESIGN - PLAN PREPARATION ENGINEER:
 KEN ENG / (847) 705-4247

INDEX OF SHEETS

<u>SHEET NO.</u>	<u>DESCRIPTION</u>
1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	GENERAL LOCATION MAP
5	SUMMARY OF PATCHING SCHEDULE
6-19	PATCHING SCHEDULE
20	BUTT JOINT AND HMA TAPER DETAILS
21	FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS
22	FREEWAY SINGLE & MULTI-LANE WEAVE
23-24	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS
25	FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES
26	STANDARD TRAFFIC SIGNAL DESIGN DETAILS
27	DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING

STATE STANDARDS

<u>STANDARD NO.</u>	<u>DESCRIPTION</u>
000001-05	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
701101-02	OFF-RD. OPERATIONS, MULTILANE
701400-03	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-05	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-05	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP
701426-03	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION
701446-01	TWO LANE CLOSURE, FREEWAY/EXPRESSWAY
701901-01	TRAFFIC CONTROL DEVICES
780001-02	

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

	<u>MIXTURE TYPE</u>	<u>AC TYPE</u>	<u>AIR VOIDS (%)</u>
FOR USE ON HMA SURFACES	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 (IL 9.5 MM), 2"	PG 64-22	4% @ 90 GYR
FOR USE ON PCC SURFACES	CLASS D PATCHES, (HMA BINDER IL-25.0 MM)	PG 64-22	4% @ 105 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "C.U.A.N." (CHICAGO UTILITY ALERT NETWORK) AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES (48 HOUR NOTIFICATION IS REQUIRED).

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE (OR TOLLWAY) PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (OR ISTHA).

WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISABILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4155 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT MR. DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER AT (847) 741-9857 MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THE EXISTING ROADWAY TYPICAL SECTION FOR HOT-MIX ASPHALT SURFACES IS ASSUMED TO HAVE A 3 INCH HOT-MIX ASPHALT OVERLAY ON TOP OF A TEN INCH CONCRETE BASE. THE EXISTING ROADWAY TYPICAL SECTION FOR CONCRETE SURFACES IS ASSUMED TO BE 10 INCHES OF FULL DEPTH PORTLAND CEMENT CONCRETE PAVEMENT.

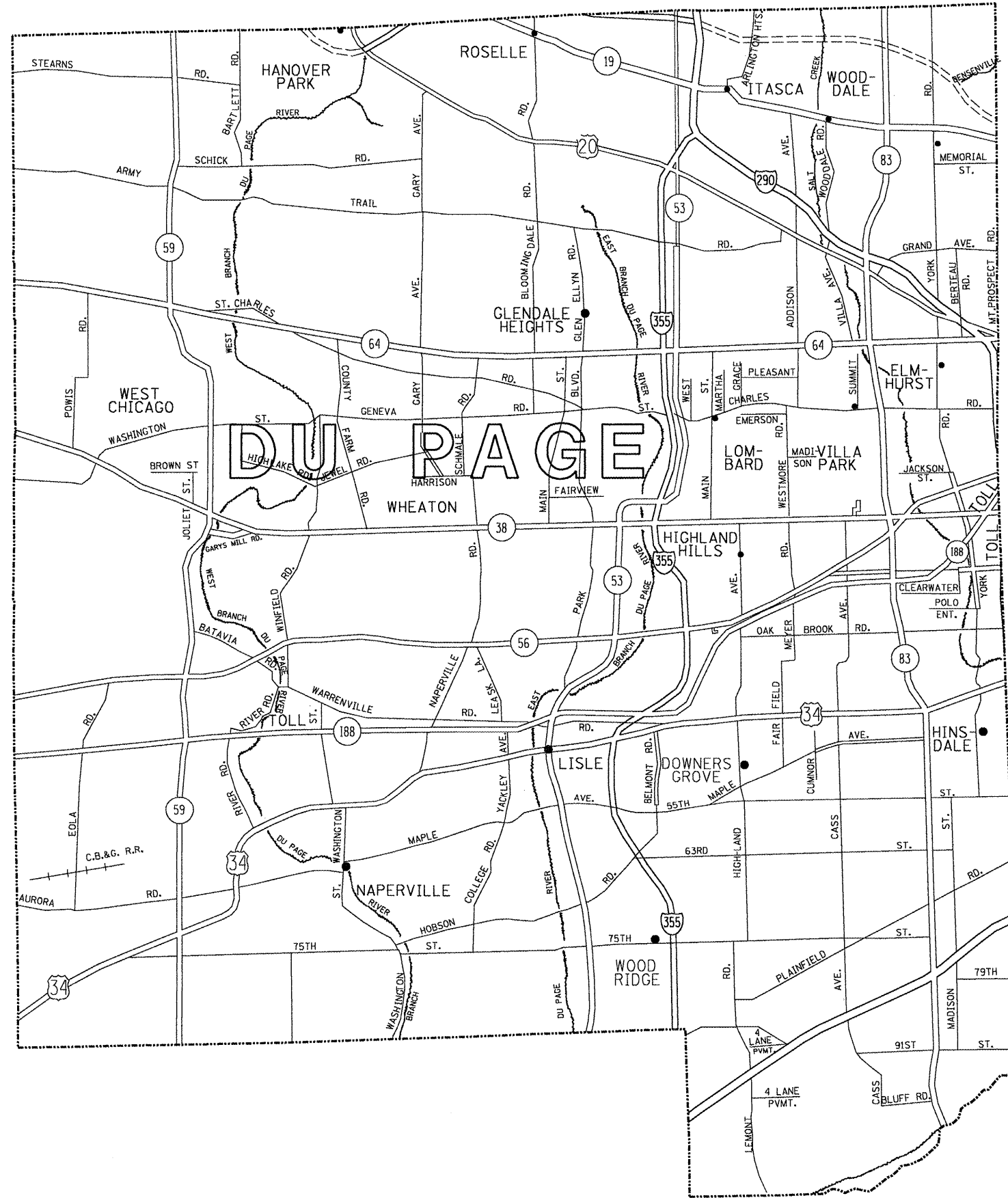
ALL PAVEMENT PATCHES ON HOT-MIX ASPHALT SURFACES SHOWN IN THE PLANS ARE TO BE TWO (2) INCH MILL AND RESURFACE ONLY. THE MINIMUM WIDTH FOR MILLING AND PATCHING SHALL BE TWO (2) FEET. ALL PAVEMENT PATCHES ON CONCRETE SURFACES ARE TO BE 10 INCH CLASS D PATCHES.

THE COST OF ANY PARTIAL OR FULL DEPTH PATCHING REQUIRED AFTER THE REMOVAL OF THE EXISTING 2 INCH HOT-MIX ASPHALT SURFACE SHALL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwork\pwork\wilgreendp\0125109\Design.dgn	DRAWN -	REVISED -	VAR.			2009-028 PP	DUPAGE	27	2	
PLOT SCALE = 100.0000 ' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60G27							
PLOT DATE = 3/24/2009	DATE -	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
					SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.

Rev.

SUMMARY OF QUANTITIES			URBAN						CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE										
CODE NO	ITEM	UNIT	TOTAL QUANTITIES													CODE NO	ITEM	UNIT	TOTAL QUANTITIES									
				1000																								
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	6	6																								
40600300	AGGREGATE (PRIME COAT)	TON	27	27																								
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	40	40																								
40600895	CONSTRUCTING TEST STRIP	EACH	1	1																								
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	659	659																								
40603345	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90	TON	1475	1475																								
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SO YD	13167	13167																								
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SO YD	115	115																								
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	406	406																								
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	106	106																								
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	178	178																								
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6																								
67100100	MOBILIZATION	L SUM	1	1																								
70300530	PAVEMENT MARKING TAPE, TYPE III 5"	FOOT	3198	3198																								
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	1333	1333																								
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	220	220																								
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	22928	22928																								
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1350	1350																								
*78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	165	165																								
*78004220	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	FOOT	3198	3198																								
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	308	308																								
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	308	308																								
*88600600	DETECTOR LOOP REPLACEMENT	FOOT	600	600																								
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1																								
	* SPECIALTY ITEM																											



FILE NAME = c:\p\work\p\dot\w\jgreendp\0125109\Design.dgn	USER NAME = wjgreendp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL LOCATION MAP - DUPAGE COUNTY			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					VAR.	2009-028 PP	DUPAGE	27	4
		PLOT SCALE = 100.0000' / IN.	CHECKED -		REVISED -	CONTRACT NO. 60G27						
		PLOT DATE = 3/24/2009	DATE -		REVISED -	SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

SUMMARY - CENTRAL INTERSTATE RAMPS & MAINLINE	HMA 2" MILL & RESURFACE (SY)	CLASS D PATCH, 10" (SY)
NB GARY AVE. (TRAVIS TO ELGIN-O'HARE ENTRANCE RAMP)	771	0
SB GARY AVE. (TRAVIS TO ELGIN-O'HARE EXIT RAMP)	226	0
EB THORNDALE AVE. (PARK BLVD. TO ROHLWING RD.)	982	0
WB THORNDALE AVE. (PARK BLVD. TO ROHLWING RD.)	854	0
NB IL 53 (ARMY TRAIL RD. TO THORNDALE AVE./PCC SECTION)	1554	0
SB I-290/IL 53 (THORNDALE AVE. TO CHICAGO/JOLIET SPLIT)	600	0
SB I-355/IL 53 (CHICAGO/JOLIET SPLIT TO ARMY TRAIL RD.)	2882	0
EXIT RAMP (SB I-355 TO ARMY TRAIL RD.)	0	410
SB TO EB I-290/I-355 (CHICAGO/JOLIET SPLIT TO NB TO EB MERGE)	1232	0
ENTRANCE RAMP (EB US 20 TO NB I-290/I-355)	0	88
EXIT RAMP (SB I-355 TO US 20)	0	218
ENTRANCE RAMP - WB US 20 TO NB I-290/I-355)	120	89
ENTRANCE RAMP - EB US 20 TO SB I-355)	501	0
EB I-290 (IL 83 TO NB MERGE, N/O FLYOVER)	2498	0
WB I-290 (IL 83 TO NB MERGE, N/O FLYOVER)	947	0
SUMMARY TOTALS:	13167	805
	SY	SY

NOTE:

DUE TO ROUNDING, SUMMARY TOTALS MAY VARY SLIGHTLY FROM THE TOTALS ON THE PATCHING SCHEDULES.

FILE NAME = c:\p\work\p\dot\wilgreendp\0125109\Design.dgn	USER NAME = wilgreendp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF PATCHING SCHEDULE CENTRAL INTERSTATES				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
		DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	VAR.	2009-02B PP	DUPAGE	27	5
		CHECKED -	REVISED -		CONTRACT NO. 60C27										
		DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT										

ROUTE: NB Gary Ave. (Travis to Elgin-O'Hare entrance ramp) - HMA SURFACE

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Travis	Elgin-O'Hare Entrance Ramp	NB	1	12	6	72	8
			2	12	6	72	8
			1	12	6	72	8
			2	12	6	72	8
			1	12	6	72	8
			1	12	6	72	8
			2	12	6	72	8
			1	12	6	72	8
			2	12	6	72	8
			1	12	6	72	8
			2	12	6	72	8
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			1	12	6	72	8
			2	12	6	72	8
			1	12	6	72	8
			2	12	6	72	8
			1	12	6	72	8
			2	12	6	72	8
			1	12	6	72	8
			2	12	6	72	8
		Joint Repair		2	200	400	44
			1	20	6	120	13
			1	20	6	120	13
			1	20	6	120	13
			1	12	30	360	40
			1	12	6	72	8
			1	12	30	360	40
			1	20	6	120	13
			1	20	6	120	13
			1	20	6	120	13
			1	20	6	120	13
			1	20	6	120	13
			1	20	6	120	13
			1	20	6	120	13
			1	20	6	120	13
			1	20	6	120	13
		Joint Repair		2	200	400	44
TOTALS:			1	20	80	1600	178

TOTALS: 822 FT 771 SY

ROUTE: SB Gary Ave. (Travis to Elgin-O'Hare Exit Ramp) - HMA SURFACE

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Travis	Elgin-O'Hare Exit Ramp	SB	1	13	6	78	9
			1	13	6	78	9
			1	13	6	78	9
			1	13	6	78	9
			1	13	6	78	9
			1	13	6	78	9
			1	13	6	78	9
			1	13	6	78	9
			1	13	6	78	9
			1	13	6	78	9
			1	13	6	78	9
			1	13	6	78	9
		Joint Repair		2	200	400	44
			1	13	6	78	9
			1	13	6	78	9
			1	13	6	78	9
		Joint Repair		2	20	40	4
			1	13	6	78	9
			1	13	6	78	9
			1	12	6	72	8
			2	12	6	72	8
			1	12	6	72	8
			2	12	6	72	8
			1	12	6	72	8
			2	12	6	72	8
			1	12	6	72	8
			2	12	6	72	8
			1	12	6	72	8
			2	12	6	72	8
			1	12	6	72	8
			2	12	6	72	8
			1	12	6	72	8
			2	12	6	72	8
			1	12	6	72	8
			2	12	6	72	8
			1	12	6	72	8
			2	12	6	72	8
TOTALS:						346 FT	226 SY

TOTALS: 346 FT 226 SY

FILE NAME *	USER NAME = wilgreendp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		PATCHING SCHEDULE GARY AVE.		F.A. RTE. VAR.	SECTION 2009-028 PP	COUNTY DUPAGE	TOTAL SHEETS 27	SHEET NO. 6
c:\pr_work\pwt\wilgreendp\d0125109\Design.dgn	PLOT SCALE = 1/8" = 100.0000' / IN.	DRAWN -	REVISED -			SCALE: _____ SHEET NO. _____ OF _____ SHEETS STA. _____ TO STA. _____		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
PLOT DATE = 3/24/2009	DATE -	CHECKED -	REVISED -			CONTRACT NO. 60G27						

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ROUTE: WB Thorndale Ave. - Park Blvd. to Rohlwing Rd. - HMA SURFACE

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Park Blvd.	Rohlwing Rd.	WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	5	5	25	3
		WB	1	2	150	300	33
		WB	1	12	6	72	8
		WB	1	12	30	360	40
		WB	1	5	10	50	6
		WB	1	12	10	120	13
		WB	1	12	25	300	33
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	4	20	80	9
		WB	2	3	200	600	67
		WB	2	2	150	300	33
		WB	2	12	4	48	5
		WB	2	12	10	120	13
		WB	2	2	125	250	28
		WB	2	2	75	150	17

ROUTE: WB Thorndale Ave. - Park Blvd. to Rohlwing Rd. - HMA SURFACE

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Park Blvd.	Rohlwing Rd.	WB	2	2	450	900	100
		WB	2	12	5	60	7
		WB	2	2	250	500	56
		WB	2	12	10	120	13
		WB	3	12	2	24	3
		WB	3	12	2	24	3
		WB	3	12	2	24	3
		WB	3	12	2	24	3
		WB	3	12	2	24	3
		WB	3	12	2	24	3
		WB	3	12	2	24	3
		WB	3	12	2	24	3
		WB	3	12	2	24	3
		WB	3	12	2	24	3
		WB	3	12	2	24	3
		WB	3	12	2	24	3
		WB	3	5	40	200	22
		WB	3	3	150	450	50
		WB	3	5	20	100	11
		WB	3	2	150	300	33
		WB	3	2	125	250	28
		WB	3	12	15	180	20
		WB	3	2	100	200	22
		WB	3	2	250	500	56
		WB	3	12	10	120	13
		WB	3	4	4	16	2
		WB	3	5	30	150	17

TOTALS: 2501 FT 854 SY

FILE NAME *	USER NAME = wjgreendp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PATCHING SCHEDULE THORNDALE AVE.	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca\pr_work\p\dot\w\jgreendp\0125109\Design.dgn	DRAWN -	REVISED -	VAR.			2009-028 PP	DUPAGE	27	8	
PLOT SCALE = 100.0000 ' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60G27							
PLOT DATE = 3/24/2009	DATE -	REVISED -	SCALE:			SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

ROUTE: NB IL 53 (Army Trail Rd. to Thorndale Ave./Concrete Pvmnt. Section) - HMA SURFACE

CROSS STREETS		DIRECTION (EB/NB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
ARMY TRAIL RD. BRIDGE	LAKE ST. EXIT (CONC PAVT)	EB/NB	1-5,	60	5	300	33
"	"	EB/NB	2,3,	12	4	48	5
"	"	EB/NB	2,3,	12	4	48	5
"	"	EB/NB	2,3,	12	4	48	5
"	"	EB/NB	2,3,	12	4	48	5
"	"	EB/NB	2,3,	12	4	48	5
"	"	EB/NB	2,3,	12	4	48	5
"	"	EB/NB	2,3,	12	4	48	5
"	"	EB/NB	4,5,	24	40	960	107
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	3-5,	36	4	144	16
"	"	EB/NB	3-4,	12	4	48	5
"	"	EB/NB	3-4,	12	4	48	5
"	"	EB/NB	3-4,	12	4	48	5
"	"	EB/NB	3-4,	12	4	48	5
"	"	EB/NB	3-4,	12	4	48	5
"	"	EB/NB	4	12	40	480	53
"	"	EB/NB	3,4,	4	55	220	24
"	"	EB/NB	1-4,	56	7	392	44
"	"	EB/NB	2-4,	36	4	144	16
"	"	EB/NB	1,2,	24	4	96	11
"	"	EB/NB	1,2,	24	4	96	11
"	"	EB/NB	1,2,	24	4	96	11
"	"	EB/NB	1,2,	24	4	96	11
"	"	EB/NB	1,2,	24	4	96	11
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	1,2,	24	4	96	11
"	LAKE ST. EXIT (conc)	EB/NB	4, BJ	16	4	64	7
LAKE ST. EXIT (conc)	"	EB/NB	1	12	4	48	5
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	1,2	24	4	96	11
"	"	EB/NB	1-3,	36	4	144	16
"	"	EB/NB	2,3,	24	4	96	11
"	"	EB/NB	2	12	6	72	8
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	2,3	44	4	176	20
"	LAKE ST. BRIDGE N.	EB/NB	1-4,	56	4	224	25

ROUTE: NB IL 53 (Army Trail Rd. to Thorndale Ave./Concrete Pvmnt. Section) - HMA SURFACE

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
LAKE ST. BRIDGE S.	"	EB/NB	1-4,	50	4	200	22
"	"	EB/NB	4, SHLDR	4	25	100	11
"	"	EB/NB	3,4,	24	4	96	11
"	"	EB/NB	3,4,	24	4	96	11
"	"	EB/NB	2,3	24	4	96	11
"	"	EB/NB	2,3	24	4	96	11
"	"	EB/NB	3	12	4	48	5
"	"	EB/NB	3	12	4	48	5
"	LAKE ST. BRIDGE (N. EXIT)	EB/NB	3	12	4	48	5
LAKE ST. BRIDGE (CONC.)	"	EB/NB	3-5, BJ	36	6	216	24
LAKE ST. BRIDGE (CONC.)	"	EB/NB	1-2, BJ	24	4	96	11
"	"	EB/NB	1-5,	60	4	240	27
"	"	EB/NB	1-5,	60	4	240	27
"	"	EB/NB	3,4,	24	4	96	11
"	"	EB/NB	3,4,	24	4	96	11
"	"	EB/NB	3,4,	24	4	96	11
"	"	EB/NB	3,4,	24	4	96	11
"	"	EB/NB	3,4,	24	4	96	11
"	"	EB/NB	3,4,	24	4	96	11
"	"	EB/NB	2	12	4	48	5
"	"	EB/NB	2	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	5	12	4	48	5
"	"	EB/NB	5	12	4	48	5
"	"	EB/NB	1,2,	12	4	48	5
"	"	EB/NB	3	12	4	48	5
"	"	EB/NB	4	12	20	240	27
"	"	EB/NB	2,3,	24	8	192	21
"	"	EB/NB	2,3,	24	8	192	21
"	"	EB/NB	4,5,	24	8	192	21
"	"	EB/NB	4,5,	24	8	192	21
"	"	EB/NB	4	12	5	60	7
"	"	EB/NB	4	12	5	60	7
"	"	EB/NB	4	12	5	60	7
"	"	EB/NB	4	12	5	60	7
"	"	EB/NB	5	12	5	60	7
"	"	EB/NB	5	12	5	60	7
"	"	EB/NB	5	12	5	60	7
"	"	EB/NB	5	12	5	60	7
"	"	EB/NB	5	12	5	60	7
"	"	EB/NB	5	12	5	60	7
"	"	EB/NB	3	12	4	48	5
"	"	EB/NB	5	24	4	96	11
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	2	12	4	48	5
"	"	EB/NB	2	12	4	48	5
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	1	12	4	48	5

CONTINUED ON THE NEXT SHEET

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ROUTE: NB IL 53 (Army Trail Rd. to Thorndale Ave./Concrete Pvmnt. Section) - HMA SURFACE

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
N/O FLYOVER	"	EB/NB	1	12	4	48	5
"	"	EB/NB	LL	4	90	360	40
"	"	EB/NB	3-5,	36	4	144	16
"	"	EB/NB	4,5	24	4	96	11
"	"	EB/NB	5	12	4	48	5
"	"	EB/NB	5	12	6	72	8
"	"	EB/NB	5	12	20	240	27
"	"	EB/NB	3	12	4	48	5
"	"	EB/NB	3	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	1-4,	48	4	192	21
"	"	EB/NB	5	12	4	48	5
"	"	EB/NB	5	12	4	48	5
"	"	EB/NB	5	12	4	48	5
"	"	EB/NB	5	12	4	48	5
"	"	EB/NB	5	12	4	48	5
"	"	EB/NB	5	12	4	48	5
"	"	EB/NB	5	12	4	48	5
"	"	EB/NB	5	12	4	48	5
"	"	EB/NB	5	12	4	48	5
"	"	EB/NB	3,4	24	4	96	11
"	"	EB/NB	5	12	8	96	11
"	THORNDALE EXIT	EB/NB	shldr	4	50	200	22
THORNDALE EXIT	"	EB/NB	5-7,	36	4	144	16
"	"	EB/NB	6	12	5	60	7
"	"	EB/NB	4	12	8	96	11
"	"	EB/NB	4	12	8	96	11
"	"	EB/NB	4	12	8	96	11
"	"	EB/NB	4	12	8	96	11
"	"	EB/NB	4	12	8	96	11
"	"	EB/NB	4	12	8	96	11
"	"	EB/NB	4	12	8	96	11
"	"	EB/NB	4	12	8	96	11
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	4	12	4	48	5
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	1	12	4	48	5
"	"	EB/NB	3	12	4	48	5
"	"	EB/NB	3	12	4	48	5
"	"	EB/NB	3	12	4	48	5
"	"	EB/NB	3	12	4	48	5
"	"	EB/NB	3	12	4	48	5
"	THORNDALE CONC.	EB/NB	3	12	4	48	5

TOTALS: 971 FT 1554 SY

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PATCHING SCHEDULE IL 53			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pwork\pwork\wilgreendp\08125109\Design.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT
		CHECKED -	REVISED -									
		DATE -	REVISED -									
												CONTRACT NO. 60G27

ROUTE: Exit Ramp (SB I-355 to Army Trail Rd.) - PCC SURFACE

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Off Ramp	Off Ramp	SB	1	16	10	160	18
		SB	1	20	2	40	4
		SB	1	16	2	32	4
		SB	1	12	2	24	3
		SB	1	12	2	24	3
		SB	1	12	2	24	3
		SB	1	12	2	24	3
		SB	1	12	2	24	3
		SB	1	12	2	24	3
		SB	1	12	2	24	3
		SB	1	12	8	96	11
		SB	1	12	2	24	3
		SB	1	12	6	72	8
		SB	1	12	2	24	3
		SB	1	12	2	24	3
		SB	1	12	2	24	3
		SB	1	12	2	24	3
		SB	1	12	2	24	3
		SB	1	2	300	600	67
		SB	2	12	15	180	20
		SB	2	12	5	60	7
		SB	2	12	2	24	3
		SB	2	12	2	24	3
		SB	2	12	2	24	3
		SB	2	12	5	60	7
		SB	2	12	2	24	3
		SB	2	12	2	24	3
		SB	2	12	2	24	3
		SB	2	12	2	24	3
		SB	2	12	2	24	3
		SB	2	12	2	24	3
		SB	2	12	4	48	5
		SB	2	12	2	24	3
		SB	2	12	2	24	3
		SB	2	12	2	24	3
		SB	2	12	2	24	3
		SB	2	12	2	24	3
		SB	2	12	2	24	3
		SB	2	2	500	1000	111
		SB	3	12	15	180	20
		SB	3	12	5	60	7
		SB	3	12	2	24	3
		SB	3	12	2	24	3
		SB	3	12	2	24	3
		SB	3	12	5	60	7
		SB	3	12	2	24	3
		SB	3	12	2	24	3
		SB	3	12	2	24	3
		SB	3	12	2	24	3
		SB	3	12	2	24	3
		SB	3	12	2	24	3
		SB	3	12	2	24	3
		SB	3	12	2	24	3
		SB	3	12	2	24	3
		SB	3	12	2	24	3
		SB	3	12	5	60	7
		SB	3	12	2	24	3
		SB	3	12	2	24	3
		SB	3	12	2	24	3

TOTALS: 969 FT 410 SY

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		CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PATCHING SCHEDULE			
I-355 EXIT RAMP			
SCALE:	SHEET NO. OF	SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2009-028 PP	DUPAGE	27	13
CONTRACT NO. 60G27				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

ROUTE: SB to EB I-290/I-355 (Chicago-Joliet Split to NB to EB Merge) - HMA SURFACE

CROSS FROM	STREETS TO	DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
I 355 / I(53) -I355 Split-flyover before flyover on I-290-EB		EB	1,2	24	20	480	53
			1,2	24	40	960	107
			1,2	24	35	840	93
			1,2	24	25	600	67
			1,2	24	40	960	107
			1,2	24	50	1200	133
			1,2	24	15	360	40
			1,2	24	40	960	107
	I-290 -Flyover Approach	EB	1,2	24	30	720	80
After flyover on I-290 EB		EB	1,2	24	30	720	80
			1,2	24	20	480	53
			1,2	24	15	360	40
			1,2	24	20	480	53
			1,2	24	35	840	93
			1,2	24	10	240	27
			1,2	24	15	360	40
			1,2	24	10	240	27
	1000 feet east of Flyover	EB	1,2	24	12	288	32

TOTALS: 462 1232
FT SY

ROUTE: Entrance Ramp - EB US 20 to NB I-290/I-355 - PCC SURFACE

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
EB RT 20 to NB-I-290	Concrete Pavement			12	4	48	5
Ramp				14	4	56	6
				14	4	56	6
				16	4	64	7
				16	4	64	7
				16	4	64	7
				16	4	64	7
				16	4	64	7
				14	4	56	6
				16	4	64	7
				16	4	64	7
				16	4	64	7
	Concrete Pavement			16	4	64	7

TOTALS: 52 FT 88 SY

ROUTE: Exit Ramp - SB I-355 to US 20 - PCC SURFACE

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
SB I-355 to Rt 20 / Ramp	Concrete Pavement			14	8	112	12
				14	8	112	12
				14	6	84	9
				14	8	112	12
				14	8	112	12
				14	4	56	6
				14	4	56	6
				14	4	56	6
				14	4	56	6
				14	4	56	6
				14	4	56	6
				14	4	56	6
				14	4	56	6
				14	4	56	6
				14	4	56	6
				24	4	96	11
				24	4	96	11
				24	4	96	11
				24	4	96	11
				36	6	216	24
	Concrete Pavement			36	6	216	24

TOTAL 214 FT 218 SY

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		CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PATCHING SCHEDULE
I-355 AND US 20 RAMPS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2009-028 PP	DUPAGE	27	15
CONTRACT NO. 60G27			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

ROUTE: Entrance Ramp - WB US 20 to NB I-290/I-355 - PCC SURFACE

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Entrance Ramp WB Rt 20	Concrete			12	6	72	8
To NB I-290				12	6	72	8
				12	4	48	5
				12	4	48	5
				12	4	48	5
				12	5	60	7
				12	6	72	8
				12	4	48	5
				12	4	48	5
				12	4	48	5
				12	4	48	5
				12	4	48	5
				12	6	72	8
	Concrete			12	6	72	8

TOTALS: 67 FT 89 SY

ROUTE: Entrance Ramp - WB US 20 to NB I-290/I-355 - HMA SURFACE

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Entrance Ramp WB Rt 20	Asphalt			12	50	600	67
To NB I-290	Asphalt			12	40	480	53

TOTALS: 90 FT 120 SY

ROUTE: Entrance Ramp (EB US 20 to SB I-355) - HMA SURFACE

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Ent. Ramp	Ent. Ramp	SB	1	18	20	360	40
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	6	96	11
		SB	1	16	4	64	7
		SB	1	16	2	32	4
		SB	1	16	2	32	4
		SB	1	16	175	2800	311
		SB	1	6	15	90	10
		SB	1	6	50	300	33
		SB	1	16	6	96	11
		SB	1	16	10	160	18

TOTALS: 320 FT 501 SY

ROUTE: EB I-290 (IL 83 to NB Merge, N/O Flyover) - HMA SURFACE

CROSS STREETS		DIRECTION (EB/WB (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
SB/EB MERGE (gore)	ADDISON BRIDGE N.	EB/SB	3	12	5	60	7
"	"	EB/SB	3	12	6	72	8
"	"	EB/SB	3	12	6	72	8
"	"	EB/SB	3	12	8	96	11
"	"	EB/SB	1-3,	36	6	216	24
"	"	EB/SB	2,3,	24	4	96	11
"	"	EB/SB	2,3,	24	4	96	11
"	"	EB/SB	2,3,	24	4	96	11
"	"	EB/SB	2,3,	24	4	96	11
"	"	EB/SB	2,3,	24	8	192	21
"	"	EB/SB	2	12	10	120	13
"	"	EB/SB	3	12	12	144	16
"	ADDISON BRIDGE (conc)	EB/SB	1-3, BJ	36	6	216	24
ADDISON BRIDGE (conc)	"	EB/SB	1-3, BJ	36	6	216	24
"	"	EB/SB	2,3,	24	8	192	21
"	"	EB/SB	2,3,	24	8	192	21
"	"	EB/SB	2,3,	24	8	192	21
"	"	EB/SB	2,3,	24	8	192	21
"	"	EB/SB	3	12	8	96	11
"	"	EB/SB	1	12	4	48	5
"	"	EB/SB	1	12	4	48	5
"	"	EB/SB	1	12	4	48	5
"	"	EB/SB	1	12	4	48	5
"	"	EB/SB	1	12	4	48	5
"	"	EB/SB	2	12	4	48	5
"	"	EB/SB	2	12	4	48	5
"	"	EB/SB	2	12	4	48	5
"	"	EB/SB	2	12	4	48	5
"	"	EB/SB	2	12	4	48	5
"	"	EB/SB	2	12	4	48	5
"	"	EB/SB	3	12	4	48	5
"	"	EB/SB	3	12	4	48	5
"	"	EB/SB	3	12	4	48	5
"	"	EB/SB	3	12	4	48	5
"	"	EB/SB	3	12	4	48	5
"	"	EB/SB	3	12	14	168	19
"	"	EB/SB	2,3,	24	8	192	21
"	"	EB/SB	3	12	30	360	40
"	"	EB/SB	2	12	30	360	40
"	"	EB/SB	1	12	30	360	40
"	"	EB/SB	2	12	8	96	11
"	"	EB/SB	1,2,	24	4	96	11
"	"	EB/SB	2,3,	24	5	120	13
"	WOOD-DALE BRIDGE N.	EB/SB	1-3, BJ	36	6	216	24
WOOD-DALE BRIDGE S.	"	EB/SB	1-3, BJ	36	6	216	24
"	"	EB/SB	LL	4	300	1200	133
"	"	EB/SB	3	12	6	72	8
"	IL 83S EXIT	EB/SB	3,4,LL	4	150	600	67
IL 83S EXIT	IL 83S ENT RAMP	EB/SB	1-4,	54	5	270	30
"	IL 83 BRIDGE	EB/SB	1-4,	48	5	240	27

TOTALS: 1660 FT 2498 SY

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PATCHING SCHEDULE I-290				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
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		CHECKED -	REVISED -						CONTRACT NO. 60G27						
		DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT										

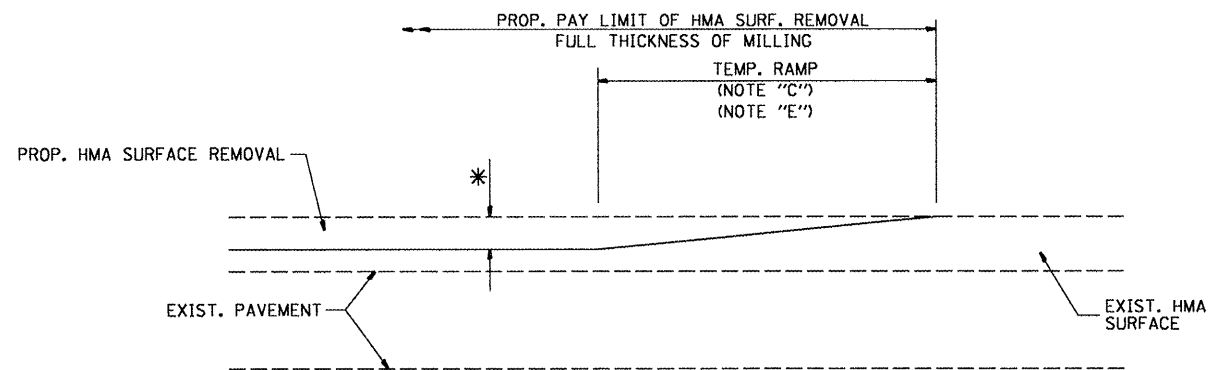
ROUTE: WB I-290 (IL 83 to NB Merge, N/O Flyover) - HMA SURFACE

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
83N exit	"	WB/NB	EXIT RAMP	12	4	48	5
"	"	WB/NB	EXIT RAMP	12	4	48	5
"	CONC. PAV'T BRIDGE	WB/NB	1,2,	24	8	192	21
83S entrance / conc.	"	WB/NB	1-4,	48	4	192	21
"	"	WB/NB	4,ramp	4	60	240	27
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	3	12	25	300	33
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	2	12	8	96	11
"	"	WB/NB	2,3,	4	20	80	9
"	"	WB/NB	1,2,	4	40	160	18
"	"	WB/NB	1,2,	4	20	80	9
"	"	WB/NB	1-3,	36	5	180	20
"	SB 83 WB 290 ENT RAMP	WB/NB	ENT RAMP	4	16	64	7
WB 290 ENT RAMP	"	WB/NB	3	16	5	80	9
"	"	WB/NB	3	4	20	80	9
"	"	WB/NB	2	12	4	48	5
"	"	WB/NB	1,2,	4	35	140	16
"	"	WB/NB	2,3,	4	20	80	9
"	"	WB/NB	3	12	8	96	11
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	3	4	20	80	9
"	"	WB/NB	3	12	20	240	27
"	"	WB/NB	3	4	20	80	9
"	WOOD-DALE BRIDGE	WB/NB	2,3,	24	4	96	11
WOOD-DALE BRIDGE	"	WB/NB	3	12	4	48	5
"	"	WB/NB	2,3,	24	5	120	13
"	"	WB/NB	3	12	5	60	7
"	"	WB/NB	2,3,	24	4	96	11
"	"	WB/NB	2	12	4	48	5
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	2,3,	24	4	96	11
"	"	WB/NB	1-3,	36	4	144	16
"	"	WB/NB	2	24	6	144	16
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	2	24	6	144	16
"	"	WB/NB	3	24	4	96	11
"	"	WB/NB	2	36	4	144	16
"	"	WB/NB	3	24	7	168	19
"	"	WB/NB	2,3,	12	4	48	5
"	"	WB/NB	2	24	4	96	11
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	2	12	4	48	5
"	"	WB/NB	2,3,	12	4	48	5
"	"	WB/NB	3	12	6	72	8
"	"	WB/NB	2,3,	24	5	120	13
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	1-3,	36	4	144	16
"	"	WB/NB	2,3,	24	4	96	11
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	2,3,	24	5	120	13
"	"	WB/NB	2,3,	24	4	96	11
"	ADDISON RD.	WB/NB	2,3	24	4	96	11

ROUTE: WB I-290 (IL 83 to NB Merge, N/O Flyover) - HMA SURFACE

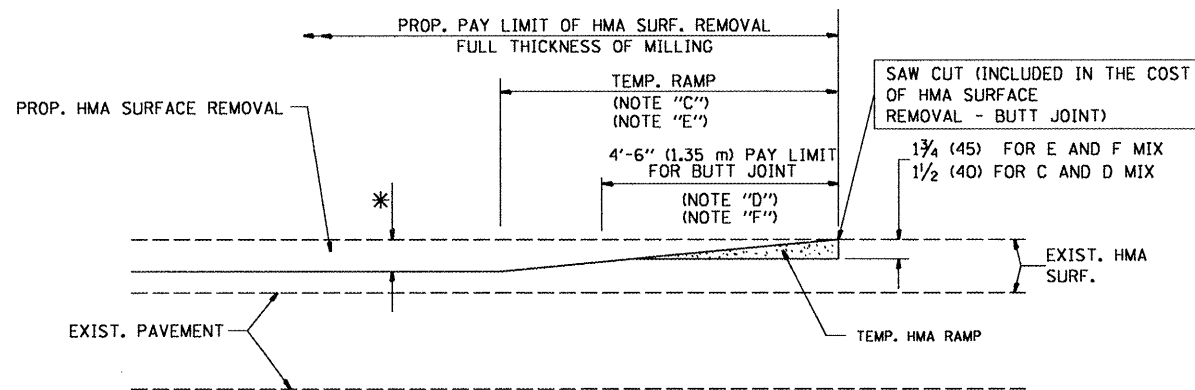
CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
ADDISON RD.	"	WB/NB	1-3,	36	5	180	20
"	"	WB/NB	2,3,	12	4	48	5
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	2,3,	24	4	96	11
"	"	WB/NB	2	12	4	48	5
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	3	12	5	60	7
"	"	WB/NB	2	12	4	48	5
"	"	WB/NB	2	12	4	48	5
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	2	12	4	48	5
"	"	WB/NB	2	12	4	48	5
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	2,3,	4	25	100	11
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	1-3,	36	4	144	16
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	2,3,	24	4	96	11
"	"	WB/NB	2,3,	24	4	96	11
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	2	12	4	48	5
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	2,3,	36	4	144	16
"	"	WB/NB	1-3,	12	4	48	5
"	"	WB/NB	3	36	4	144	16
"	"	WB/NB	2	24	4	96	11
"	"	WB/NB	3	24	4	96	11
"	"	WB/NB	1-3,	12	4	48	5
"	"	WB/NB	2,3,	24	4	96	11
"	"	WB/NB	2,3,	4	25	100	11
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	3	12	4	48	5
"	"	WB/NB	2,3 LL	4	20	80	9
"	"	WB/NB	1,2,	24	4	96	11
"	"	WB/NB	1-3,	36	4	144	16
"	"	WB/NB	2,3,	32	4	128	14
"	I-355 SPLIT	WB/NB	1,2,	24	4	96	11
I-355 SPLIT	"	WB/NB	2	12	4	48	5
"	N/O FLYOVER	WB/NB	1,2,	24	4	96	11

TOTALS: 731 FT 947 SY



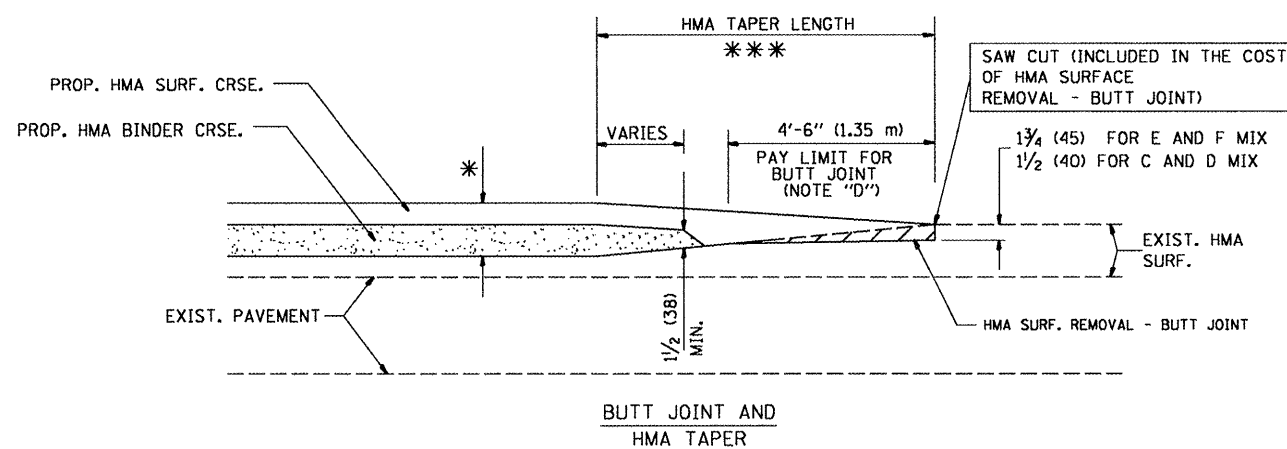
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

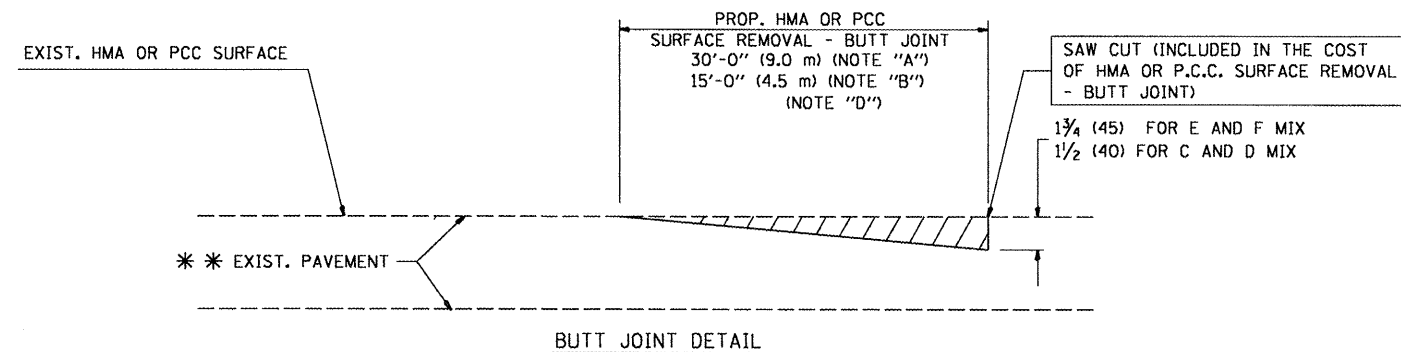


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

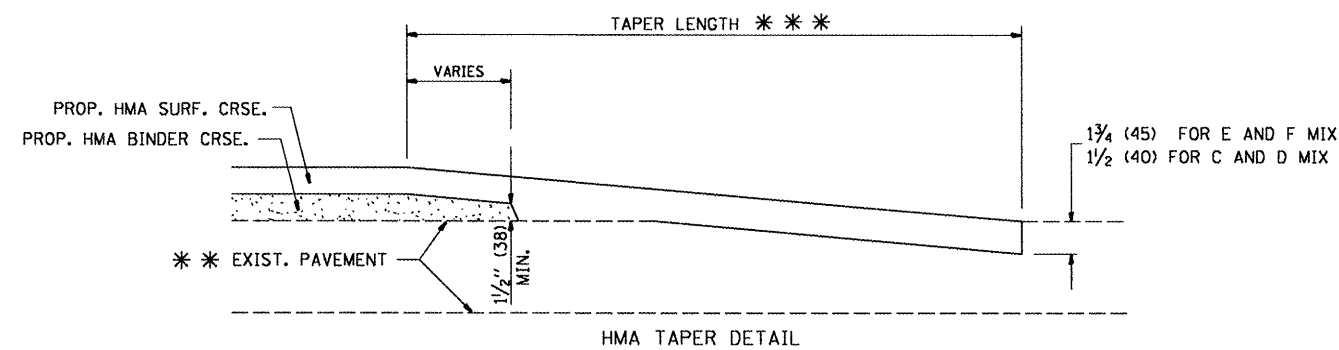
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

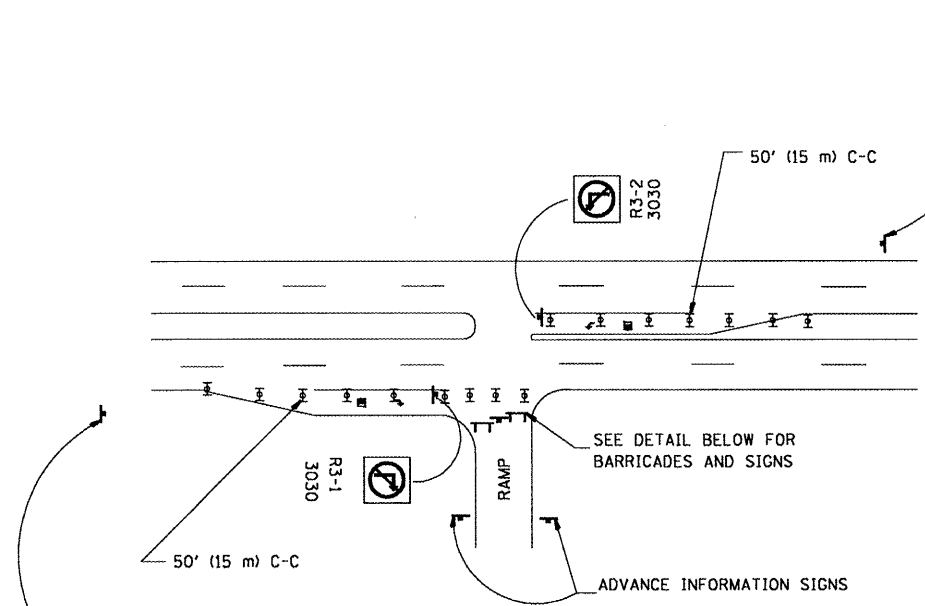
FILE NAME =	USER NAME = wilgreendp	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
ct:\pw\work\p\idot\wilgreendp\0125109\01-15.tsd.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 3/24/2009		DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

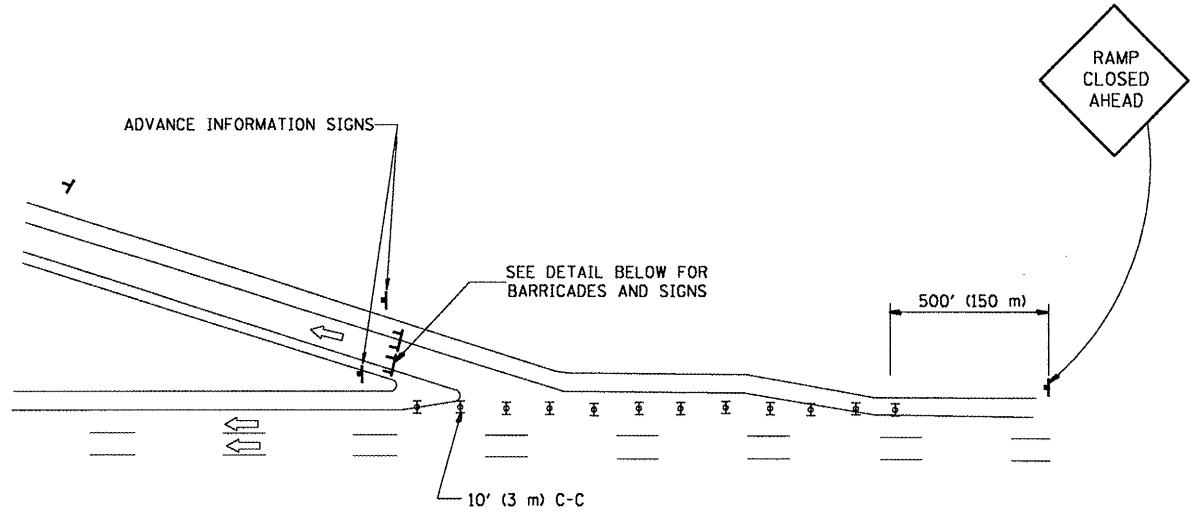
BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAK	2009-028 PP	DUPAGE	27	20
BD400-05 BD32		CONTRACT NO. 60627		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

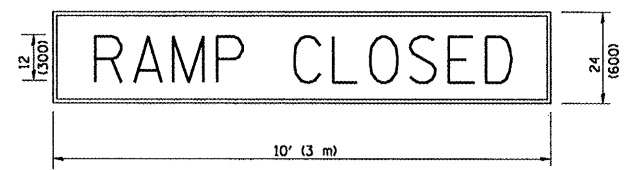


ENTRANCE RAMP CLOSURE



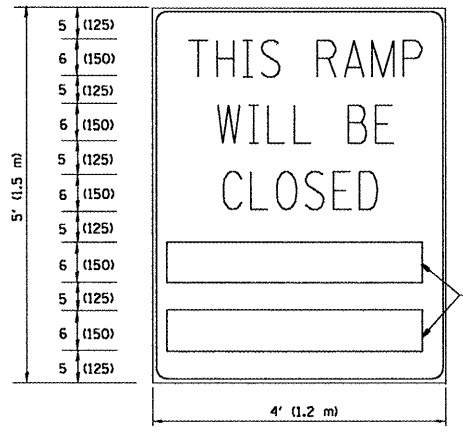
EXIT RAMP CLOSURE

RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE REFLECTORIZED BACKGROUND
1 (25) BORDER
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR THE CLOSED EXIT RAMPS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE REFLECTORIZED BACKGROUND

1/2 (12) BORDER

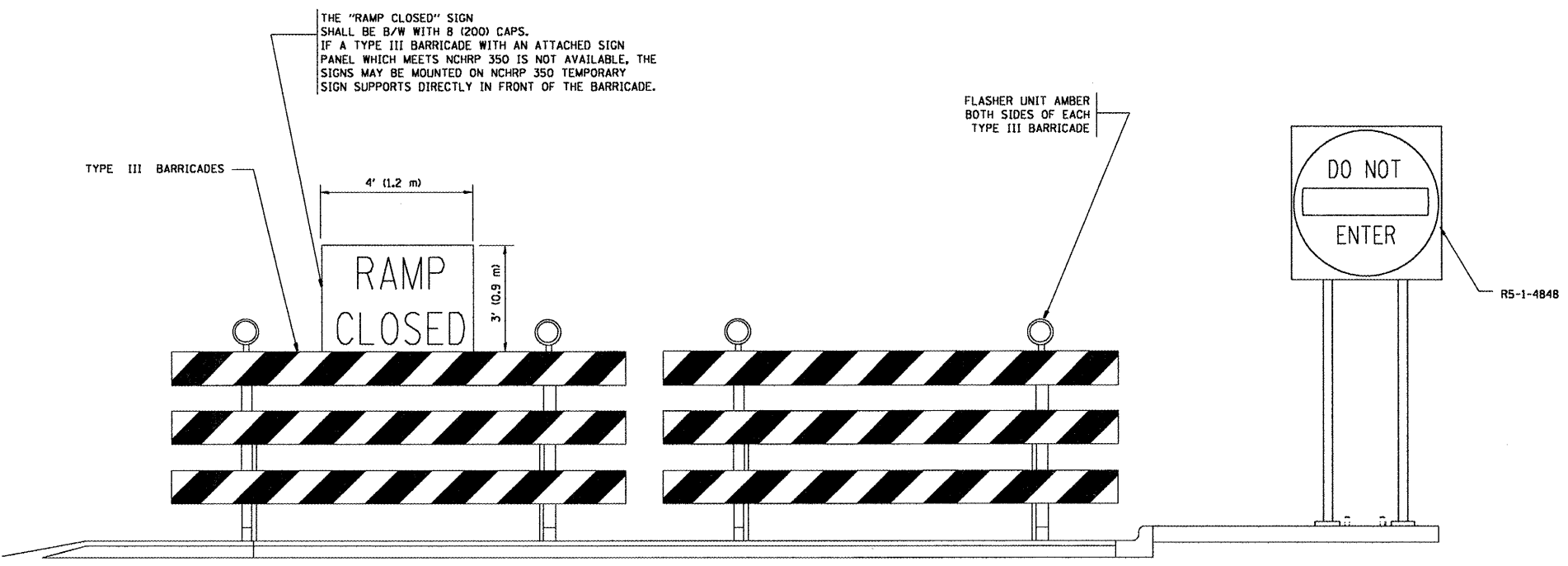
THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

GENERAL NOTES:

1. CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
2. STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
3. A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
4. ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
5. THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
7. THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY FOUR (24) HOURS IN LENGTH.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



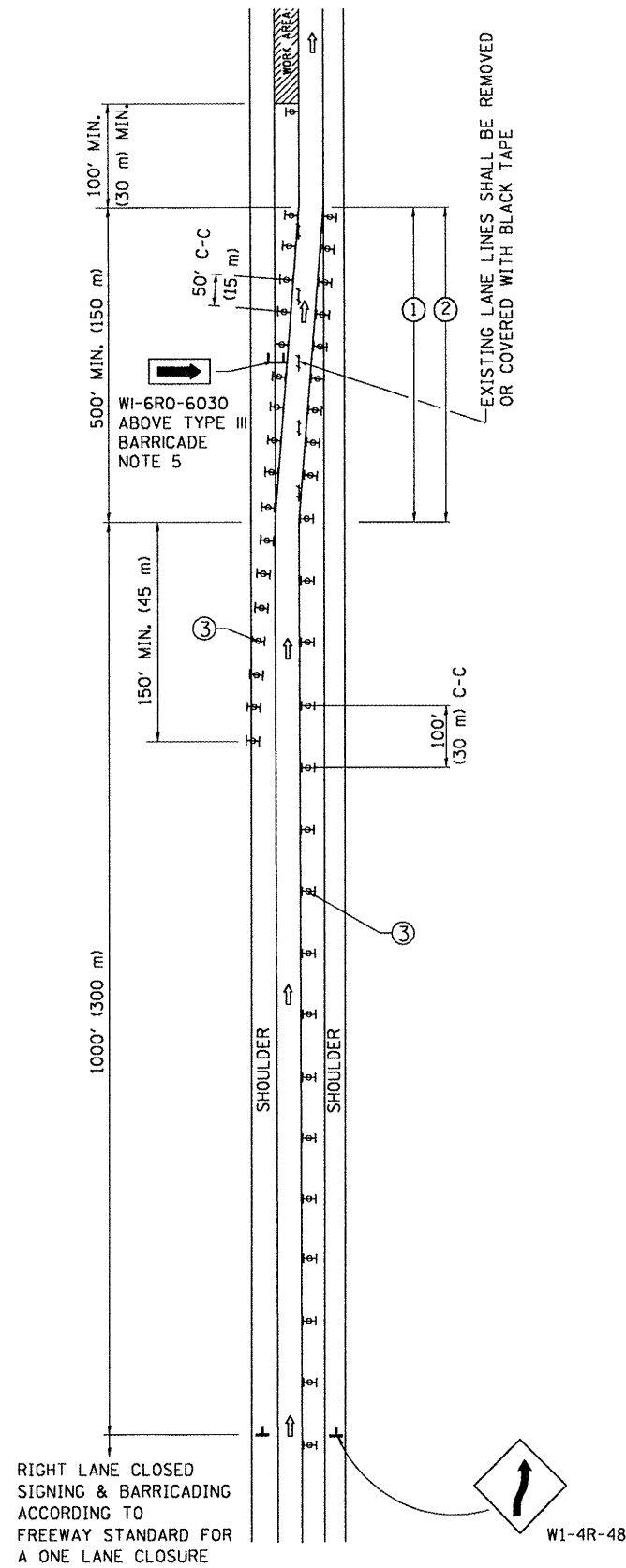
DETAIL FOR REQUIRED BARRICADES & SIGNS

SYMBOLS

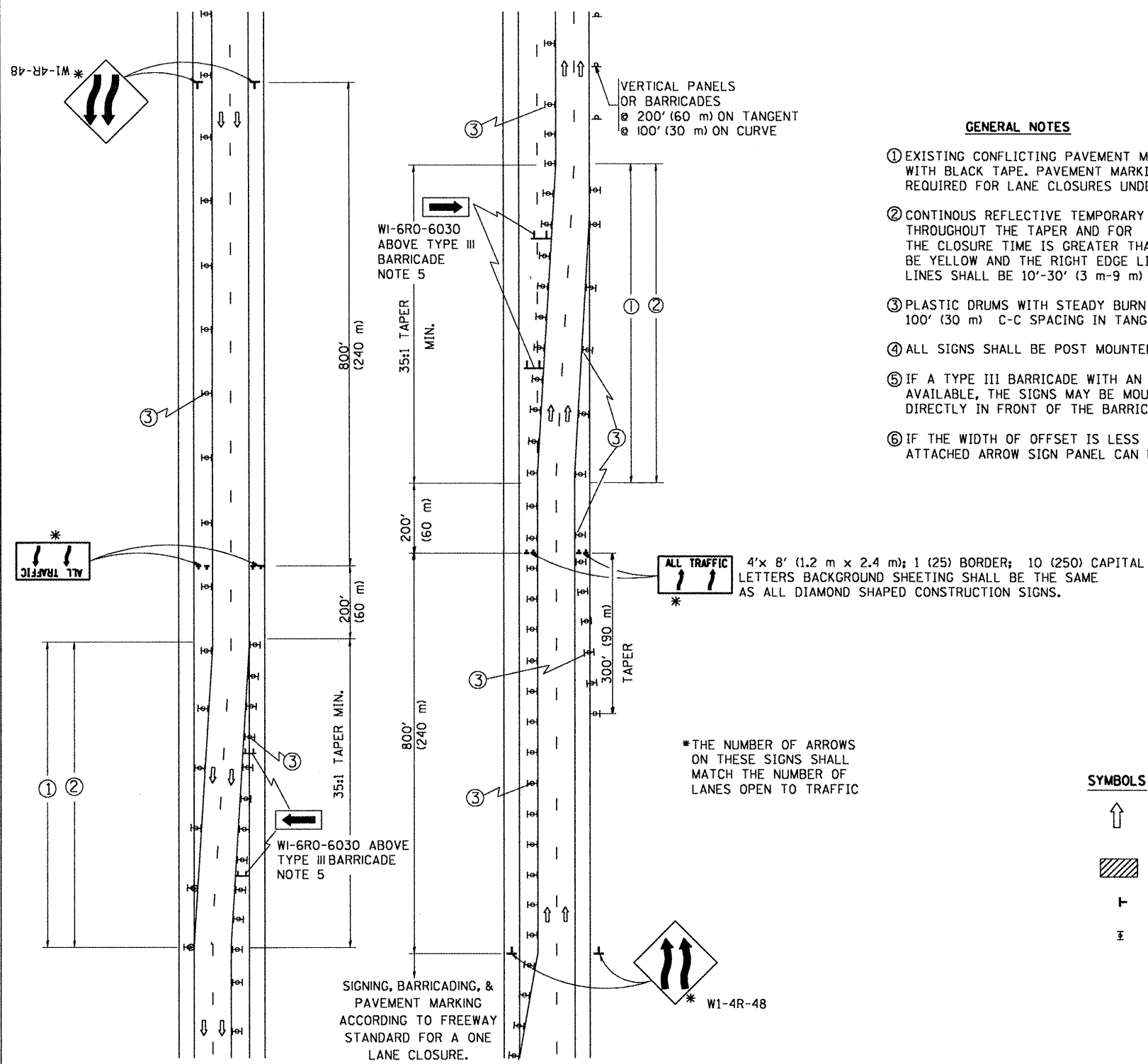
- ⊞ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- ⊞ TYPE III BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = wilgreendp	DESIGNED - DWS	REVISED - DWS 12-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FREEWAY ENTRANCE AND EXIST RAMP CLOSURE DETAILS		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
at:\pwork\pwork\wilgreendp\0125109\01std.dgn		DRAWN -	REVISED - DWS/JAF 12-02		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	VAR. 2009-028 PP	DUPAGE	27	21
		CHECKED -	REVISED - JAF 02-06					TC-08			
		DATE - 02-83	REVISED - SPB 01-07					CONTRACT NO. 60627			

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED OR COVERED WITH BLACK TAPE. PAVEMENT MARKING REMOVAL OR BLACK TAPE SHALL NOT BE REQUIRED FOR LANE CLOSURES UNDER 24 HOURS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVE LANE LINES SHALL BE 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS DIRECTLY IN FRONT OF THE BARRICADE.
- ⑥ IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.

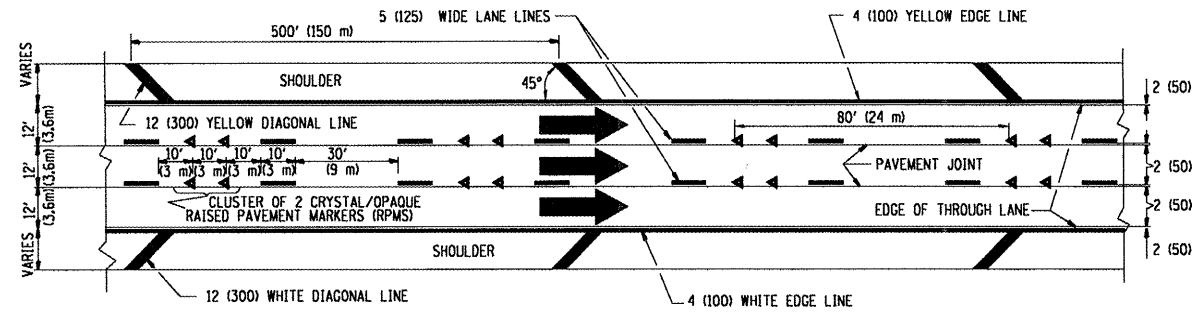
SYMBOLS

- ↑ DIRECTION OF TRAFFIC
- ▨ WORK AREA
- ┌ SIGN ON PORTABLE OR PERMANENT SUPPORT
- ⊞ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH MONO-DIRECTIONAL STEADY BURNING LIGHT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = wilgreendp	DESIGNED - DWS	REVISED - DWS 11-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE		F.A. RTE. VAW. 2009-028 PP	SECTION DJ-PAGE 27 22	COUNTY	TOTAL SHEETS 27	SHEET NO. 22	
CONTRACT NO. 60627	PLOT SCALE = 1/8" = 1'-0"	CHECKED -	REVISED - JAF 01-03		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT			
	PLOT DATE = 3/24/2009	DATE - 02-87	REVISED - JAF 02-06									
			REVISED - SPB 01-07									

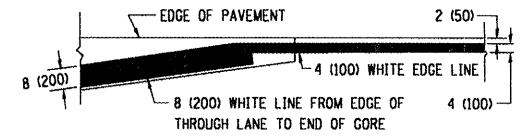
THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH
THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH



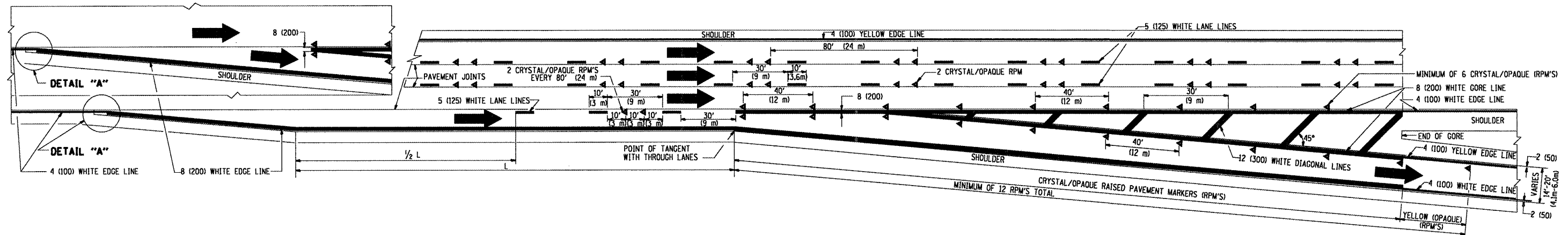
TYPICAL EDGE LINES & LANE LINES

NOTES:

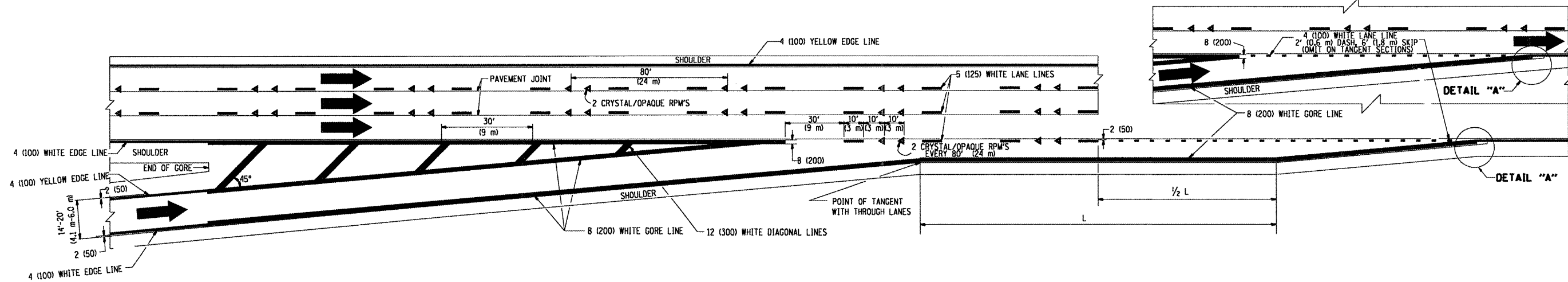
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE SHALL BE USED FOR ALL LANE LINES ON BITUMINOUS PAVEMENT
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC



DETAIL "A"

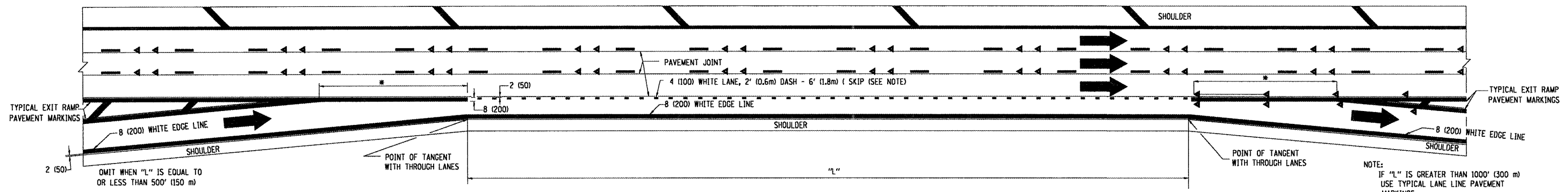


TYPICAL EXIT RAMP PAVEMENT MARKINGS

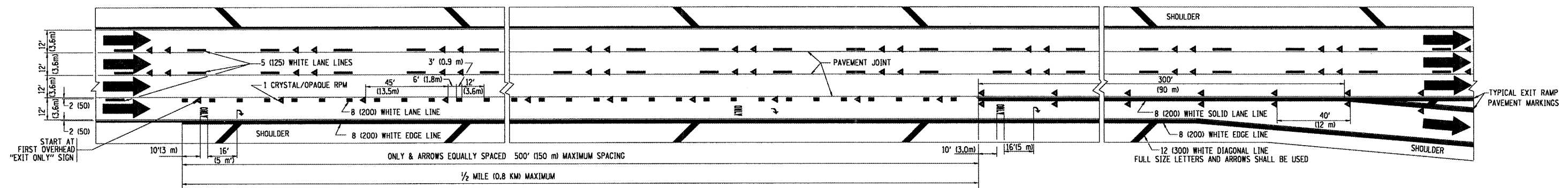


TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS

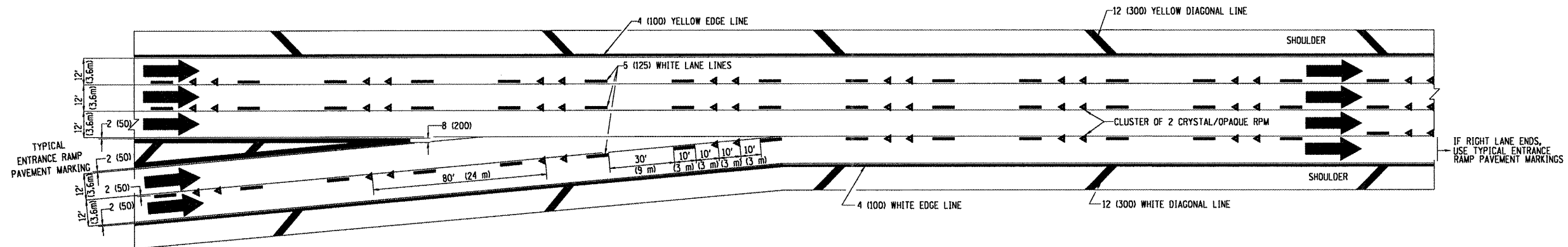
FILE NAME =	USER NAME = wilgreendp	DESIGNED - D.W.S.	REVISED - A.H. 03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS		F.A. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cs:\pw\work\pav\dot\wilgreendp\01251099.Dwg		DRAWN -	REVISED - D.W.S. 07-96		SCALE: NONE	SHEET NO. 1 OF 2 SHEETS	STA.	VAR. 2009-028 PP	DJ PAGE	27	23
		CHECKED -	REVISED - J.A.F. 02-06				TO STA.	TC-12			
		PLOT DATE = 3/24/2009	DATE - 01-90		REVISED - S.P.B. 01-07			CONTRACT NO. 60627			
							FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TYPICAL ENTRANCE/EXIT RAMP COMBINATION PAVEMENT MARKINGS



TYPICAL EXIT ONLY LANE PAVEMENT MARKINGS

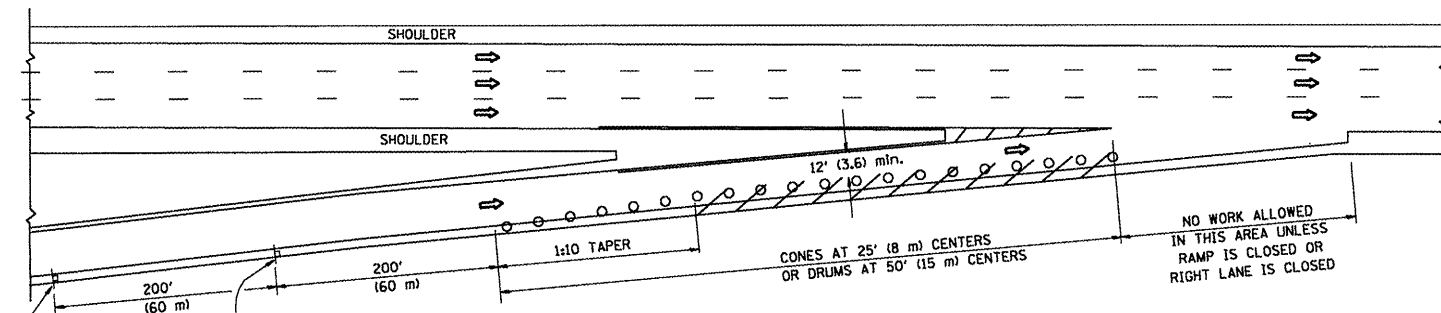


TYPICAL TWO LANE ENTRANCE RAMP PAVEMENT MARKINGS

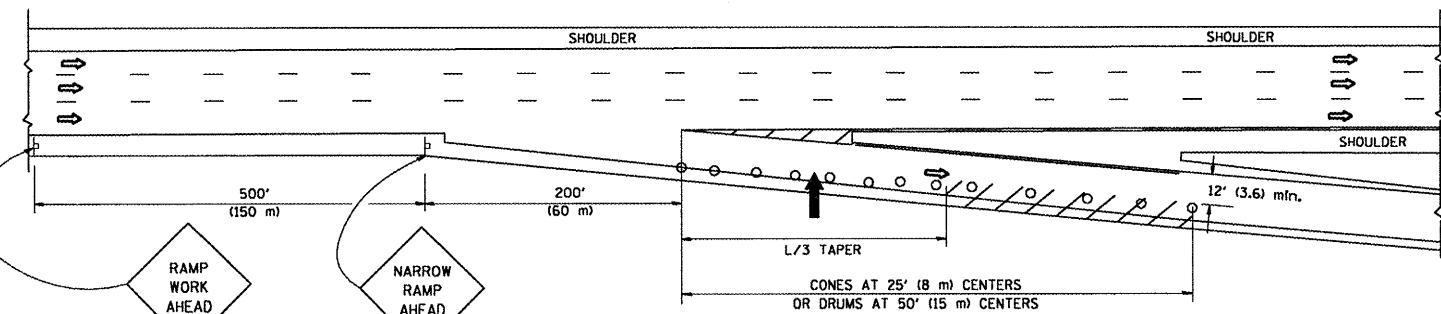
FILE NAME =	USER NAME = wlgreendp	DESIGNED - D.W.S.	REVISED - A.H. 03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\wlgreendp\0125109\Di	sstd.dgn	DRAWN -	REVISED - D.W.S. 07-96		SCALE: NONE	SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.	VAR. 2009-028 PP	DUDAGE 27	24
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED - J.A.F. 02-06						TC-12	CONTRACT NO. 60627	
	PLOT DATE = 3/24/2009	DATE - 01-90	REVISED - S.P.B. 01-07						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

PARTIAL RAMP CLOSURE DETAILS

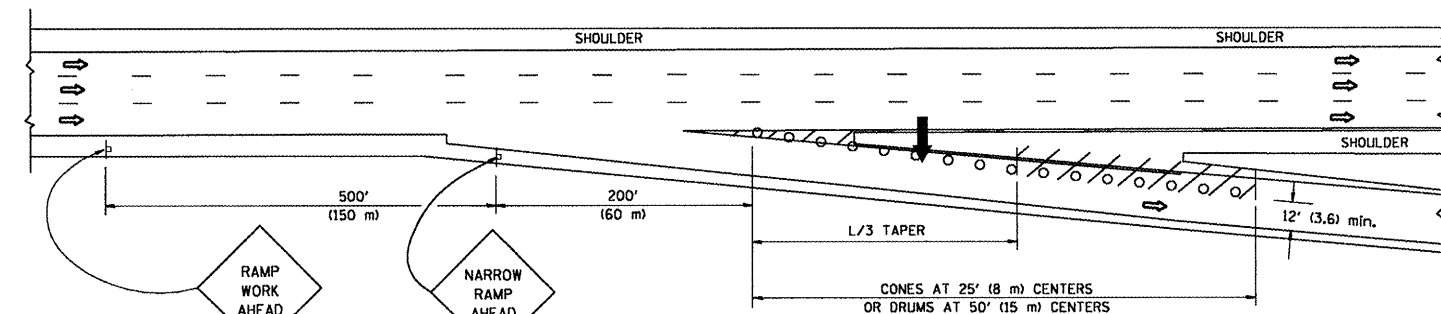
SHOULDER CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

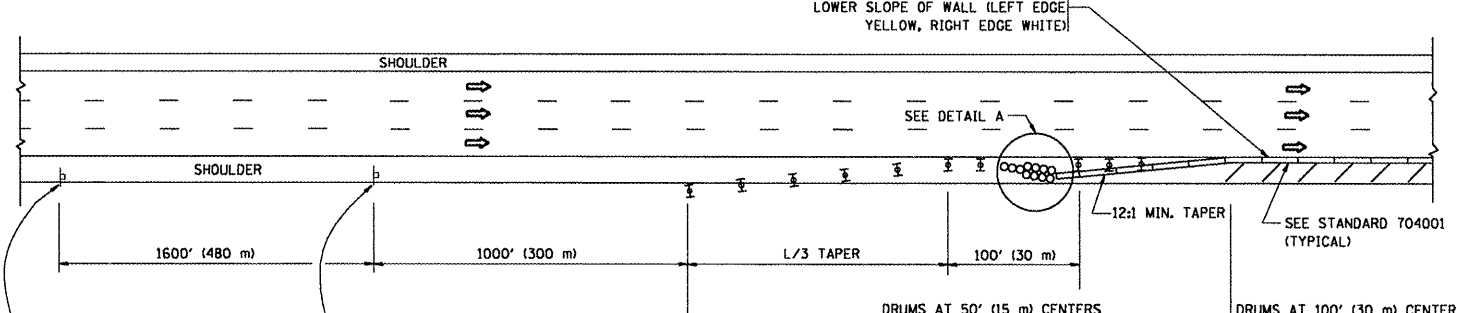
- ARROWBOARD
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

GENERAL NOTES

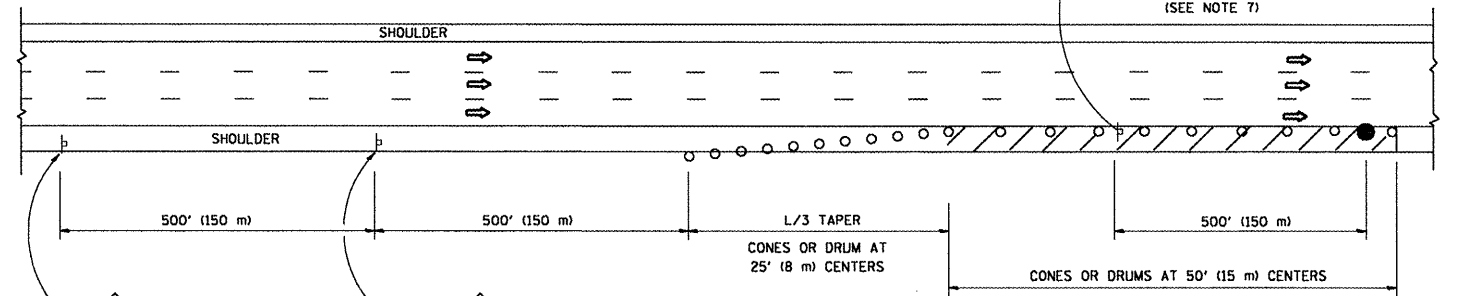
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH $L=0.65(W)(S)$ $L=(W)(S)$
	W = WIDTH OF OFFSET IN FEET (METERS) S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

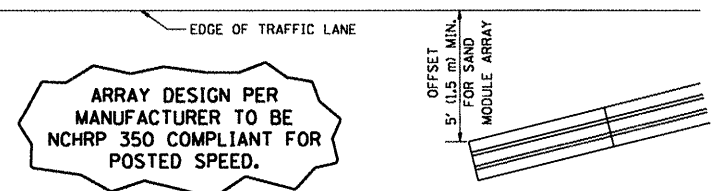


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



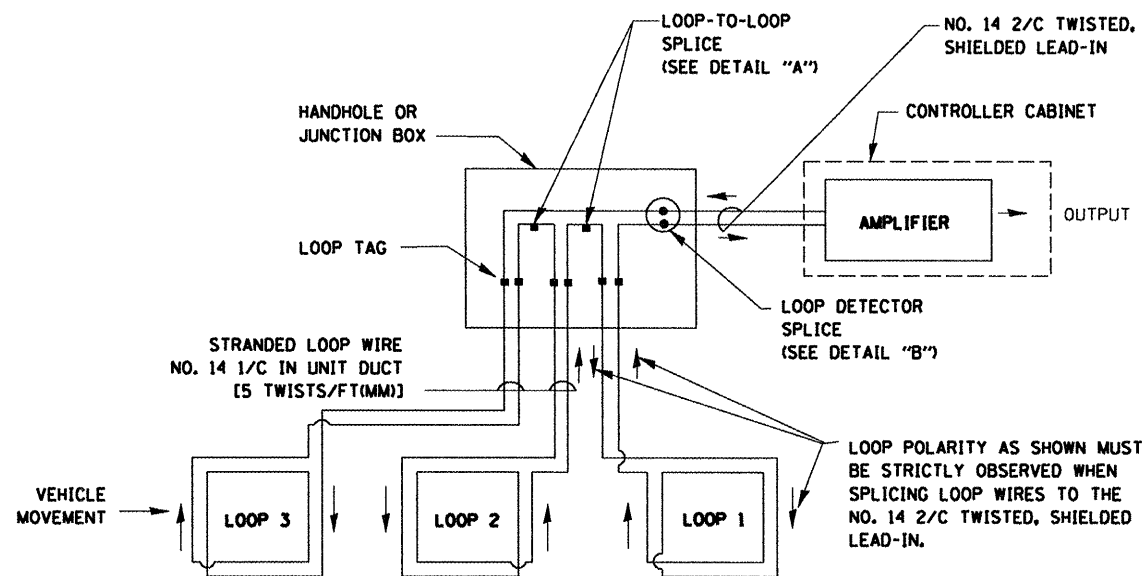
DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED - J.A.F. 12-02	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca\pw_work\pilot\wilgreendp\02125109\Di	sstd.dgn	DRAWN - D.W.S.	REVISED - 04-03		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	VAR. 2009-028 PP	DW PAGE 27	25		
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED - J.A.F. 12-06					TC-17			CONTRACT NO. 60627	
	PLOT DATE = 3/24/2009	DATE - 11-96	REVISED - S.P.B. 01-07					FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

LOOP DETECTOR NOTES

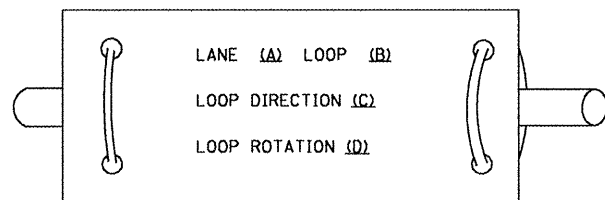
1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.



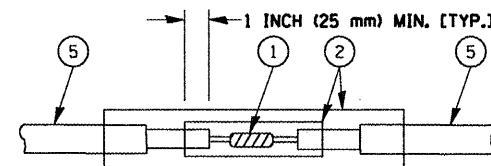
DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

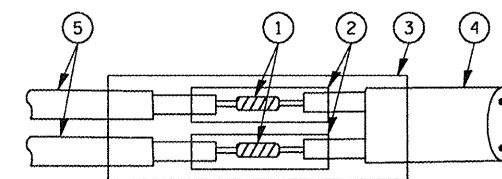
LOOP LEAD-IN CABLE TAG



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

LOOP DETECTOR SPLICE

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

FILE NAME =	USER NAME = wilgreendp	DESIGNED - D.A.D.	REVISED - 11-12-01
ca:\pvc\work\p1dot\wilgreendp\0125109\01-01-02.dgn		DRAWN - R.W.P.	REVISED - BUR. TRAFFIC 01-01-02
PLOT SCALE = 100.0000' / IN.		CHECKED - D.A.Z.	REVISED -
PLOT DATE = 3/24/2009		DATE - 05-30-00	REVISED -

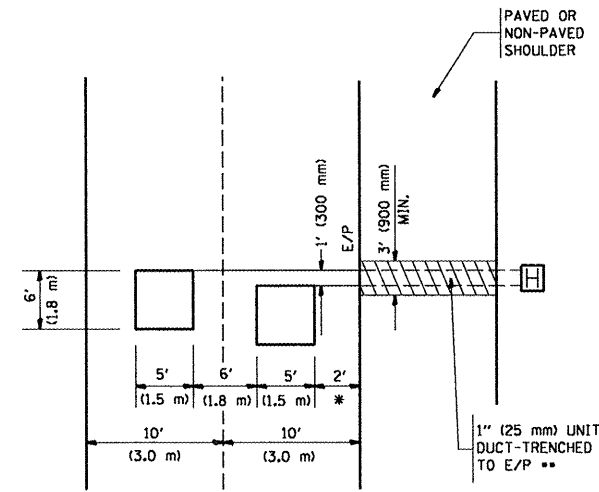
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
STANDARD TRAFFIC SIGNAL DESIGN DETAILS			
SCALE: NONE	SHEET NO. 1 OF 4 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2009-02B PP	DUPAGE	27	26
TS-05			CONTRACT NO. 60627	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

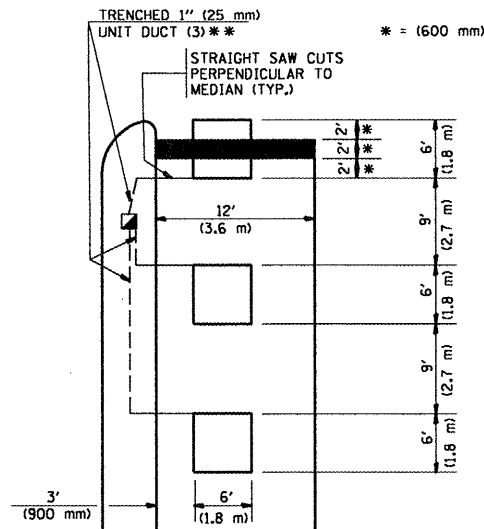


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

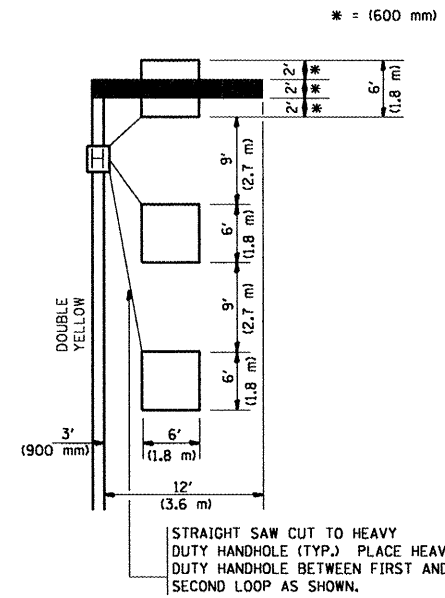
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

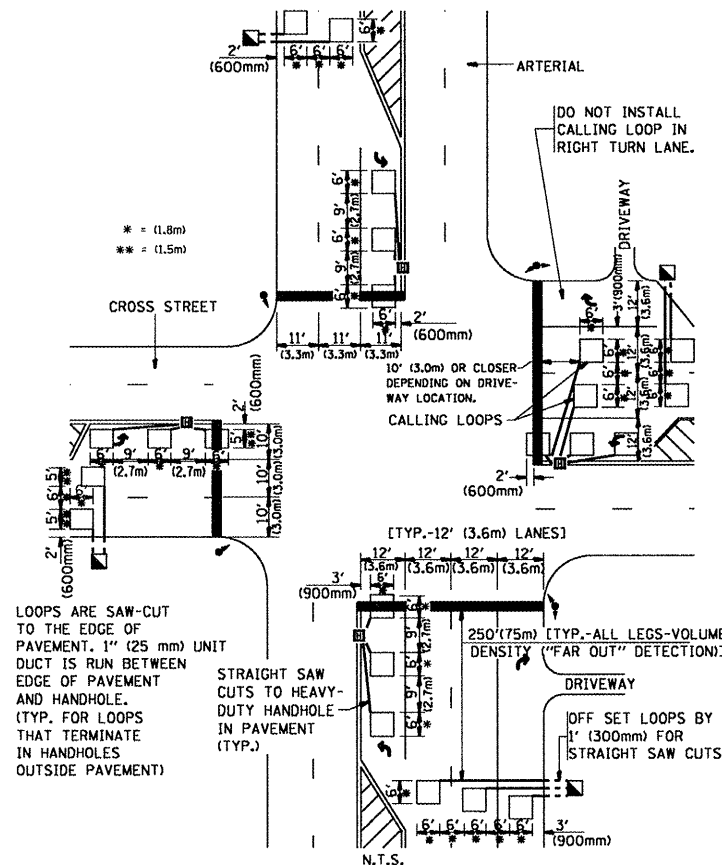
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



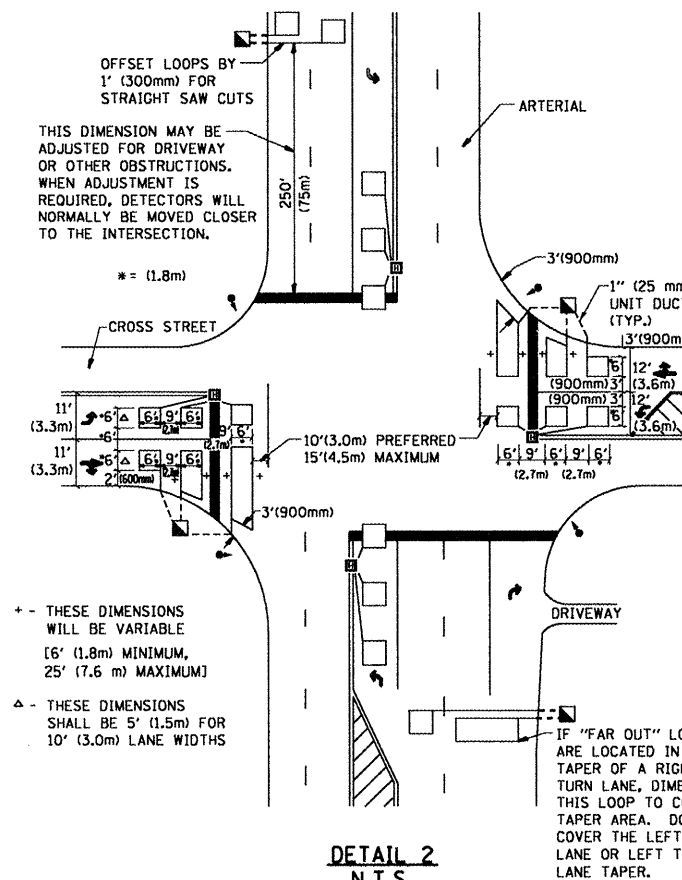
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwork\pwork\wilgreendp\0125109\Dist1Std.dgn		DRAWN -	REVISED -			VAR. 2009-028 PP	DUPAGE	27	27	
PLOT SCALE = 100.0000' / IN.		CHECKED - R.K.F.	REVISED -			TS-07	CONTRACT NO. 60627			
PLOT DATE = 3/24/2009		DATE -	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
SCALE: NONE					SHEET NO. 1 OF 1 SHEETS		STA. TO STA.			