

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	9 RS-7	KANE	25A	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 60H26		

* 25+1=26

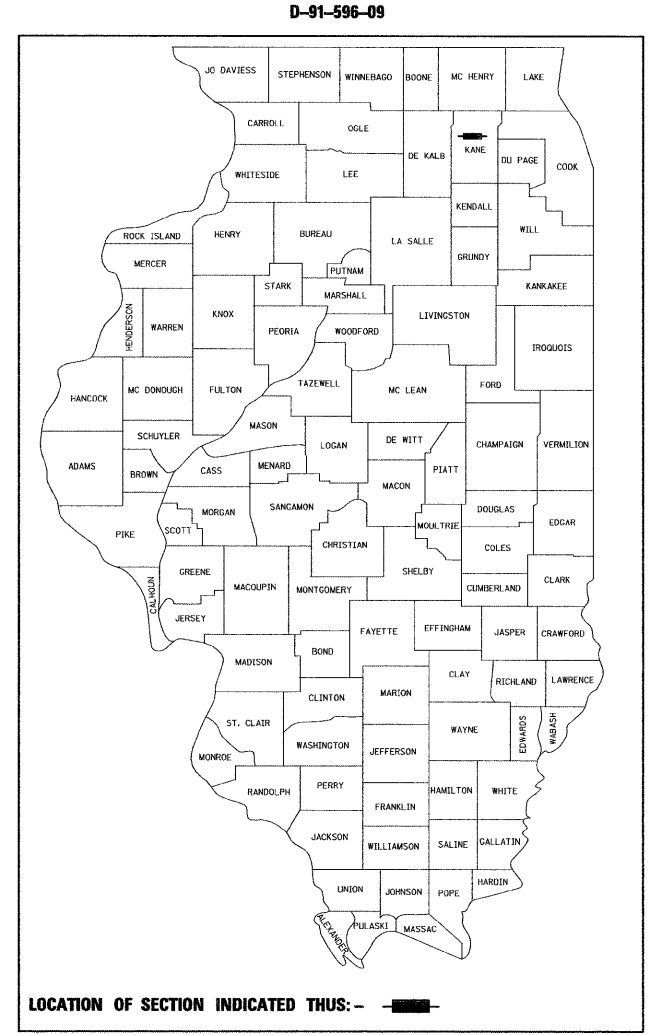
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAP 345 / US 20 (LAKE ST)
ILL 47 TO W OF PLANK RD.
SECTION: 9 RS-7
RESURFACING IMPROVEMENT
KANE COUNTY
C-91-596-09

FOR INDEX OF SHEETS, SEE SHEET NO. 2

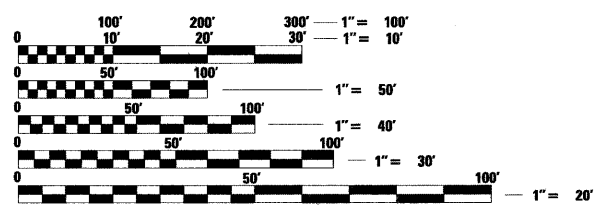
THE IMPROVEMENT IS LOCATED
IN THE TOWNSHIPS OF RUTLAND
AND PLATO AND THE VILLAGE OF
PINGREE GROVE



LOCATION OF SECTION INDICATED THUS: —



TRAFFIC DATA:
2006 ADT = 10100
POSTED SPEED LIMIT = 55 MPH
40 MPH (PINGREE GROVE)

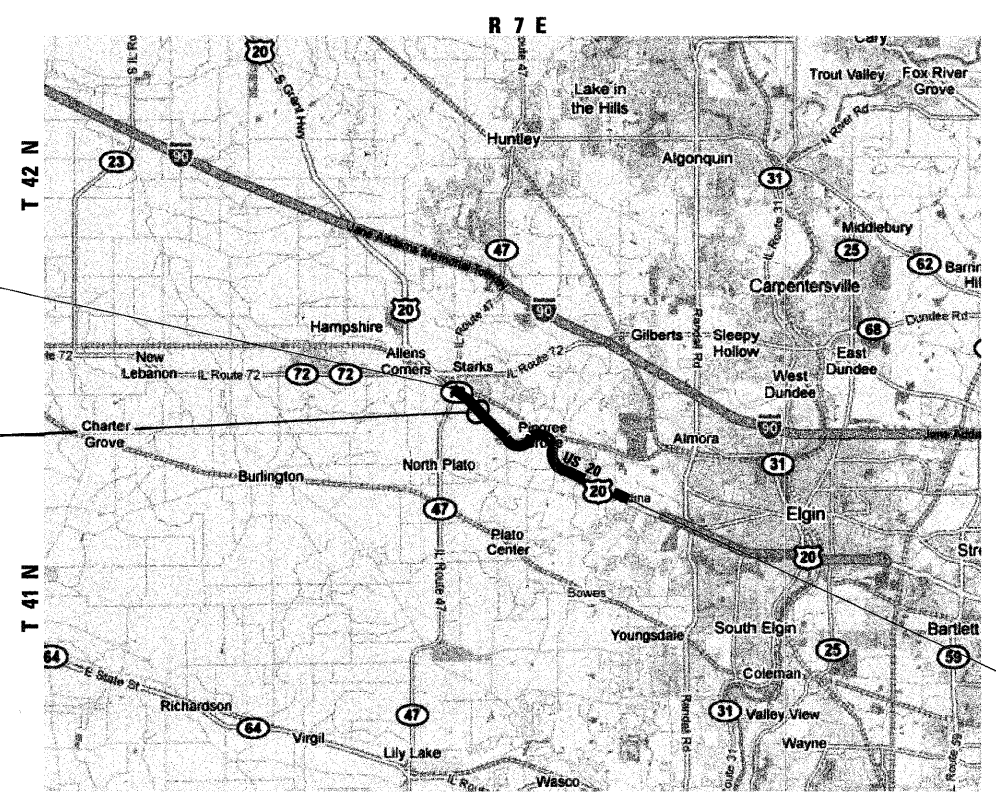


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT BEGINS
STA. 40 + 30

OMMISIONS
STA. 64 + 68 TO 77 + 70



RUTLAND AND PLATO TOWNSHIPS

PROJECT ENDS
STA. 314 + 00

JOSEPH P. NEAL
ALFRED BENESCH & CO.



EXPIRATION DATE 11-30-09
DATE 4-21-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED APRIL 21 20 09

Diana M. O'Keefe DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
May 1, 20 09

Charles G. Ingersoll ENGINEER OF DESIGN AND ENVIRONMENT
May 1, 20 09

Christine M. Reed DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

CONSULTANT PROJECT ENGINEER: ISSAM RAYAN (847) 705-4556

benesch
alfred benesch & company
Engineers • Surveyors • Planners
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

CONTRACT NO. 60H26

GROSS LENGTH OF IMPROVEMENT = 27,370 LINEAL FEET = 5.18 MILES
NET LENGTH OF IMPROVEMENT = 26,068 LINEAL FEET = 4.94 MILES

INDEX OF SHEETS

GENERAL NOTES

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	EXISTING AND PROPOSED TYPICAL SECTIONS
5-14	ROADWAY AND PAVEMENT MARKING PLANS
14A	DETECTOR LOOP REPLACEMENT
15	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
16	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
17	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS INTERSECTIONS AND DRIVEWAYS, (TC-10) ARTERIAL ROAD INFORMATION SIGN (TC-22)
18	TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) (TC-11)
19	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
20	TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
21	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)
22	HMA SHOULDER WIDENING AT CULVERT HEADWALLS
23	DRIVEWAY ENTRANCE SIGNING (TC-26)
24	DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)
25	DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER STABILIZATION AT TBT TY 1 SPL (BD-34)

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOUR NOTIFICATION IS REQUIRED).
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND CITY ARLINGTON HEIGHTS.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.
- THE RESIDENT ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER AT (847) 741-9857 AT LEAST (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- ALL PAVEMENT MARKING SHALL BE PLACED THROUGHOUT THE PROJECT ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING.
- ALL HMA PAVEMENT PATCHING SHALL BE CLASS D.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD (FOR FUTURE REFERENCES), ALL EXISTING PAVEMENT MARKING LINES IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- DRAINAGE ADJUSTMENT, CLEANING OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS. STATIONS ARE SHOWN FOR REFERENCE ONLY AND APPROXIMATE.

- WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- SAW CUTTING PRIOR TO ANY REMOVAL ITEMS NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS BEING REMOVED.
- THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- UNLESS OTHER CONDITIONS WARRANT, EXTENDED LANE CLOSURES SHALL BE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS. OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKINGS.
- TEMPORARY RAMPS AT BUTT JOINTS SHALL BE INCLUDED IN THE COST OF THE BUTT JOINT AS SHOWN ON THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET.
- MATCH EXISTING PAVEMENT MARKINGS AT THE PROJECT AND OMISSION LIMITS.
- PAY ITEM 55039700 - STORM SEWERS TO BE CLEANED - SHALL ONLY BE UTILIZED TO CLEAN STORM SEWER LATERAL PIPES BETWEEN INLETS OR CATCH BASINS AND MAIN SEWERS.
- STEEL PLATE BEAM GUARDRAIL TO BE REPLACED AS INDICATED ON PLANS AND AS DIRECTED BY THE ENGINEER.

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATION AND PATTERNS
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHLD. STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
630001-08	STEEL PLATE BEAM GUARDRAIL
630101-08	GUARDRAIL MOUNTED ON EXISTING CULVERTS
630201-06	PCC/HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
701201-03	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS >= 45 MPH
701306-02	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >= 45 MPH
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS ONLY
701336-05	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS >= 45 MPH
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

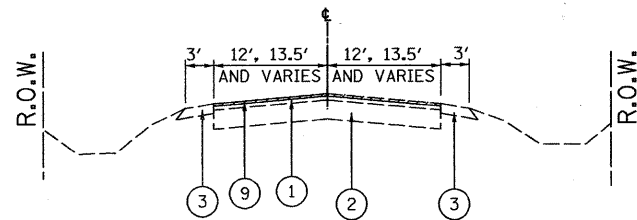
URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 100% STATE	
CODE NO.	ITEM DESCRIPTION	UNIT		ROADWAY 1000	GUARDRAIL SFTY - 3J
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	50	50	
20201006	GRADING AND SHAPING SHOULDERS	UNIT	525	525	
21400100	GRADING AND SHAPING DITCHES	FOOT	1575	1,575	
25000210	SEEDING, CLASS 2A	ACRE	0.14	0.14	
25200100	SODDING	SQ YD	44	44	
25200110	SODDING, SALT TOLERANT	SQ YD	219	219	
28101500	RIPRAP, SPECIAL	SQ YD	36	36	
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	10,101	10,101	
40600300	AGGREGATE (PRIME COAT)	TON	105	105	
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	28	28	
40600995	CONSTRUCTING TEST STRIP	EACH	2	2	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	425	425	
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	328	328	
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	7,650	7,650	
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	90,505	90,505	
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SQ YD	1948	1948	
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	45	45	
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	181	181	
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	634	634	
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	950	950	
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	1,387	1,387	
48203021	HOT-MIX ASPHALT SHOULDERS, 6"	SQ YD	221		221
* 63000001	STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS	FOOT	550		550
* 63000003	STEEL PLATE BEAM GUARD RAIL, TYPE A, 9 FOOT POSTS	FOOT	425		425
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	9		9
63200310	GUARDRAIL REMOVAL	FOOT	975		975
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	8	8	
67100100	MOBILIZATION	L SUM	1	1	
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1	
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1	

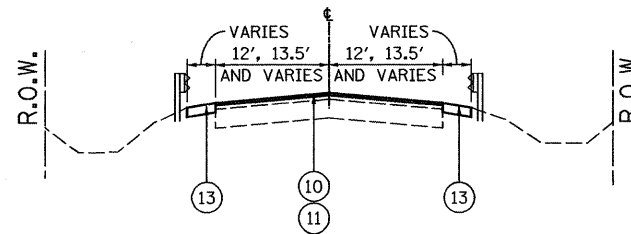
* SPECIALTY ITEM

URBAN

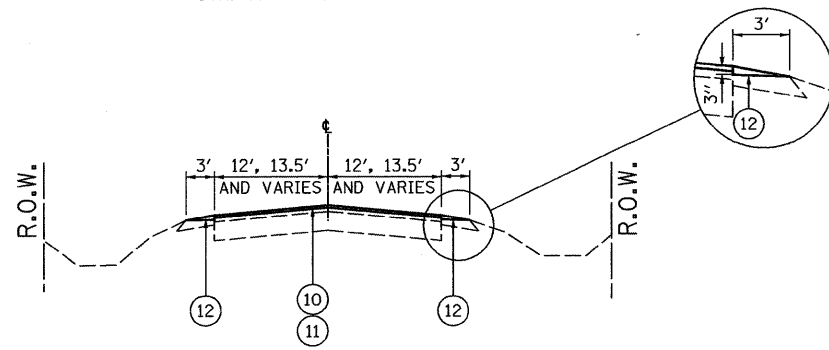
SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 100% STATE	
CODE NO.	ITEM DESCRIPTION	UNIT		ROADWAY 1000	GUARDRAIL SFTY - 3J
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1	
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	44,792	44,792	
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	103	103	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	89,583	89,583	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	813	813	
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	231	231	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	75	75	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	103	103	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	89,583	89,583	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	813	813	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	231	231	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	75	75	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	667	667	
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	22		22
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	667	667	
81300200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	5	5	
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	76	76	
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	103	103	
X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	3,810	3,810	
Z0014800	CULVERT TO BE CLEANED	FOOT	60	60	
* 63000005	STEEL PLATE BEAM GUARD RAIL, TYPE B	FOOT	125		125
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	37	37	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	45,234	45,234	



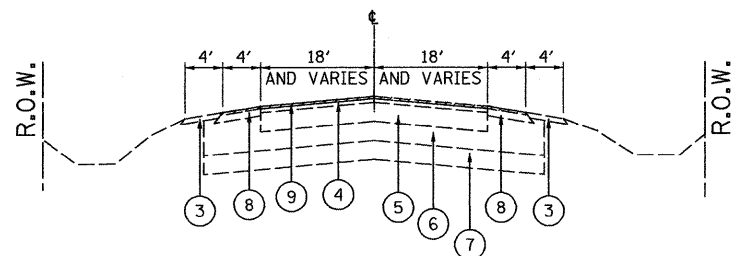
**U.S. ROUTE 20
EXIST. TYPICAL SECTIONS**
STA. 40+30 TO STA. 64+68
STA. 77+70 TO STA. 307+96



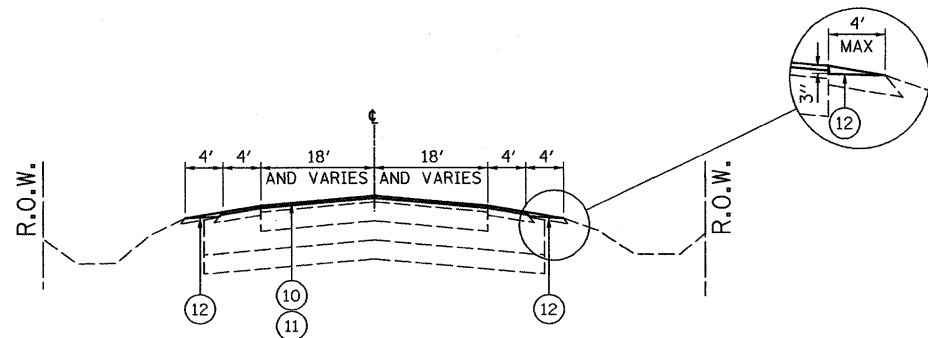
**U.S. ROUTE 20
PROP. TYPICAL SECTIONS**
⑬ USED IN FRONT OF GUARDRAIL
WHERE SHOULDER IS FAILING
STA. 43+37 TO STA. 44+87 RT
STA. 132+16 TO STA. 133+54 RT
STA. 133+16 TO STA. 134+16 LT
STA. 268+20 TO STA. 269+58 RT
STA. 268+65 TO STA. 270+03 LT



**U.S. ROUTE 20
PROP. TYPICAL SECTIONS**
STA. 40+30 TO STA. 64+68
STA. 77+70 TO STA. 307+96



**U.S. ROUTE 20
EXIST. TYPICAL SECTIONS**
STA. 307+96 TO STA. 314+00



**U.S. ROUTE 20
PROP. TYPICAL SECTIONS**
STA. 307+96 TO STA. 314+00

LEGEND

- ① EXIST. HOT-MIX ASPHALT SURFACE, 3"±
- ② EXIST. P.C. CONCRETE PAVEMENT, 10"±
- ③ EXIST. AGGREGATE SHOULDER
- ④ EXIST. HOT-MIX ASPHALT SURFACE, 2"
- ⑤ EXIST. HOT-MIX ASPHALT BINDER, 12 3/4"
- ⑥ EXIST. AGGREGATE SUBGRADE, 12"
- ⑦ EXIST. POROUS GRANULAR EMBANKMENT SUBGRADE, 12"
- ⑧ EXIST. HOT-MIX ASPHALT SHOULDER
- ⑨ HOT-MIX ASPHALT SURFACE REMOVAL, 1.5"
- ⑩ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX D, N70 ILL 9.5 mm, 1.5"
- ⑪ PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑫ PROP. AGGREGATE WEDGE SHOULDERS, TYPE B
- ⑬ PROP. HOT-MIX ASPHALT SHOULDERS, 6"

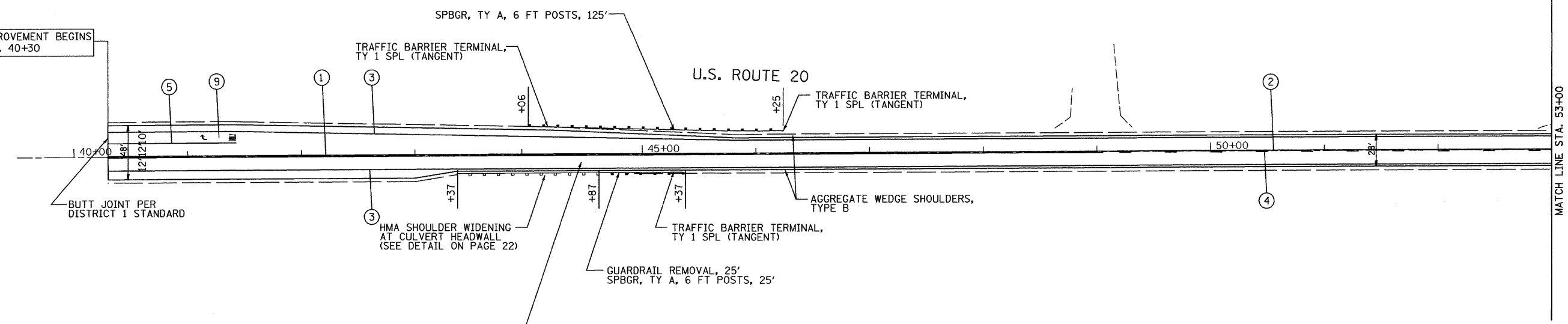
NOTES:

- 1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN.
- 2. CONTRACTOR SHALL PATCH BEFORE MILLING
- WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AC TYPE	AIR VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (ILL 9.5 mm)	PG 64-22	4% @ 70 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	SBS/SBR PG 76-28/-22	4% @ 50 GYR
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19mm)	PG 64-22*	4% @ 70 GYR
CLASS D PATCH (HMA BINDER IL-19mm)	PG 64-22*	4% @ 70 GYR
HMA SHOULDER	PG 64-22*	2% @ 30 GYR



IMPROVEMENT BEGINS STA. 40+30



LEGEND

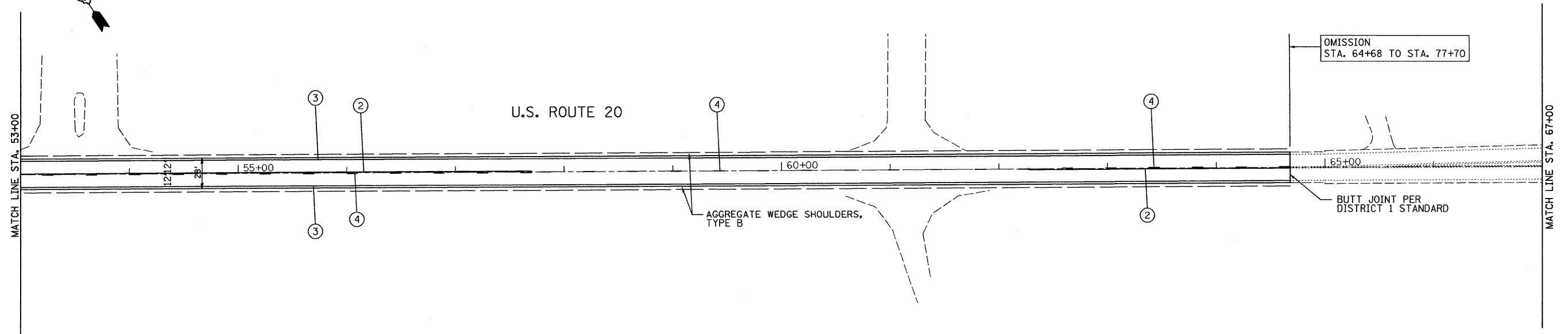
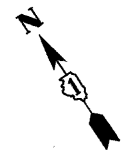
- ① 2-(4") YELLOW @ 11" C-C
- ② (4") YELLOW LINE
- ③ (4") WHITE EDGE LINE
- ④ (4") YELLOW LINE 10' DASH 30' SKIP
- ⑤ (6") WHITE TURN LANE
- ⑥ (24") STOP LINES
- ⑦ (12") YELLOW DIAGONALS
- ⑧ (6") WHITE CROSSWALK LINE (PEDESTRIAN)
- ⑨ LETTERS AND SYMBOLS
- ⑩ 6" WHITE DOTTED LINES (2' LINE WITH 6' SPACE)

PAVEMENT IMPROVEMENTS:

HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

NOTES:

PERMANENT PAVEMENT MARKING SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKING DETAILS"
 RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ON "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS"



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USER NAME = gthiesse
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 PLOT DATE = 5/14/2009

DESIGNED - BJW
 DRAWN - DMS
 CHECKED - JAS
 DATE - 4/21/09

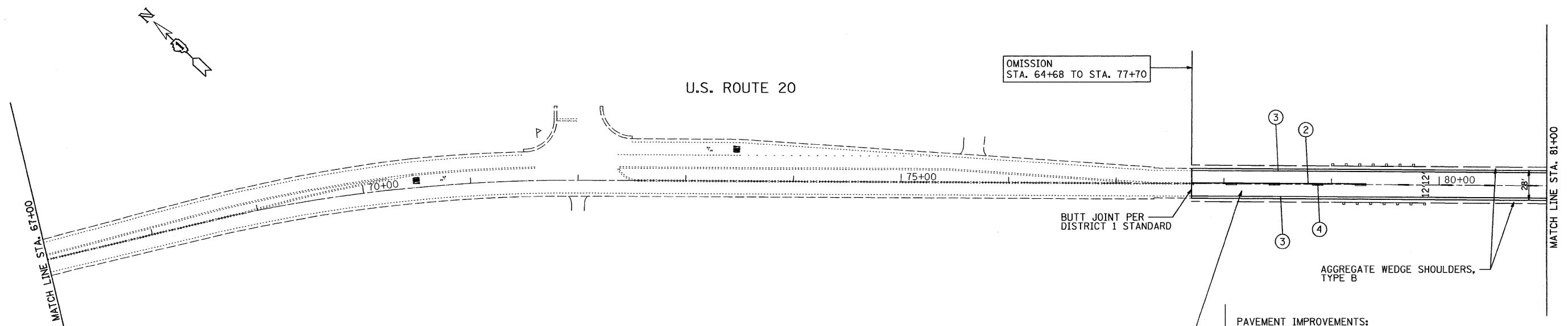
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING
 US 20 - ILL 47 TO W OF PLANK RD**

SCALE: SHEET NO. OF SHEETS STA. 39+50 TO STA. 67+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	9 RS-7	KANE	25	5
CONTRACT NO. 60H26				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



LEGEND

- ① 2-(4") YELLOW @ 11" C-C
- ② (4") YELLOW LINE
- ③ (4") WHITE EDGE LINE
- ④ (4") YELLOW LINE 10' DASH 30' SKIP
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OMISSION
STA. 64+68 TO STA. 77+70

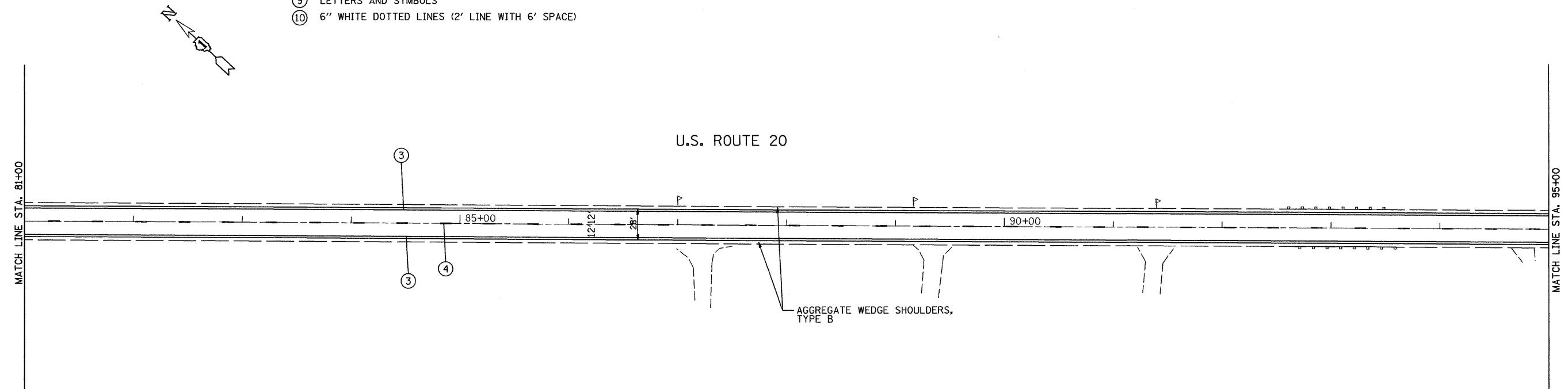
BUTT JOINT PER
DISTRICT 1 STANDARD

AGGREGATE WEDGE SHOULDERS,
TYPE B

PAVEMENT IMPROVEMENTS:
HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

NOTES:

PERMANENT PAVEMENT MARKING SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKING DETAILS"
RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ON "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS"



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PLOT DATE = 5/14/2009

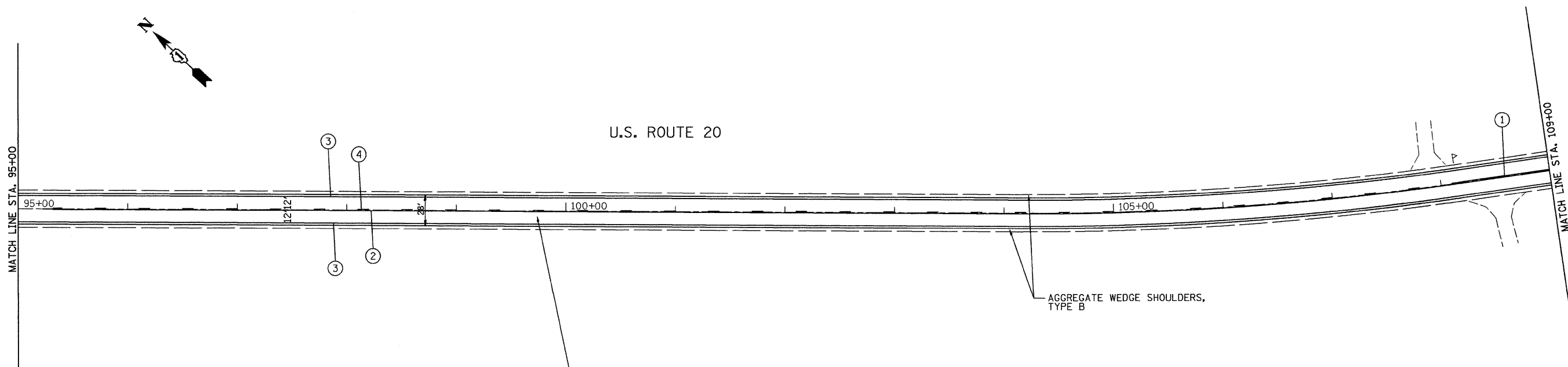
DESIGNED - BJW
DRAWN - DMS
CHECKED - JAS
DATE - 4/21/09

REVISED - 5/14/09
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING
US 20 - ILL 47 TO W OF PLANK RD
SCALE: SHEET NO. OF SHEETS STA. 67+00 TO STA. 95+00

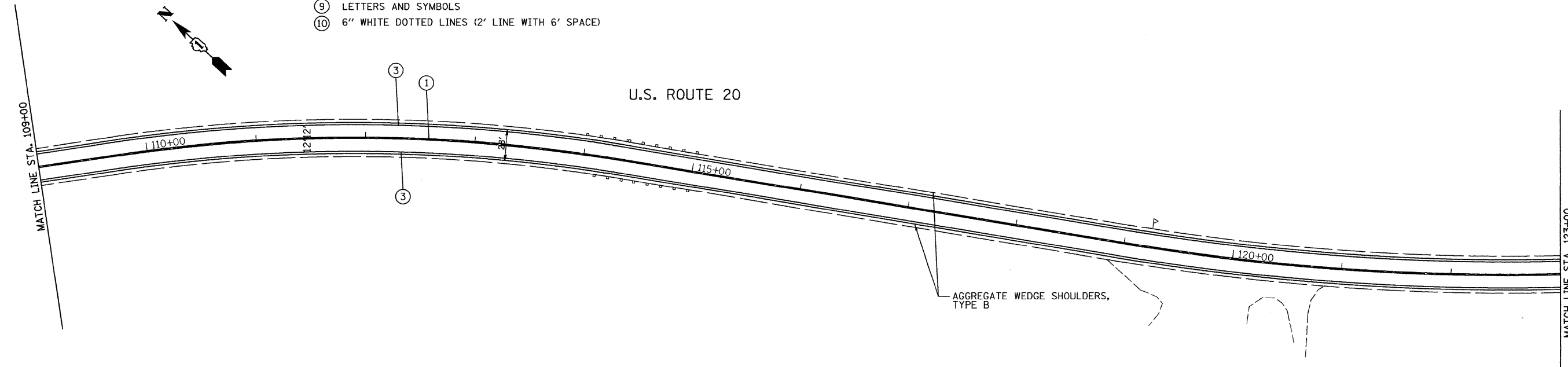
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345	9 RS-7	KANE	25	6
CONTRACT NO. 60H26				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



- LEGEND**
- ① 2-(4") YELLOW @ 11" C-C
 - ② (4") YELLOW LINE
 - ③ (4") WHITE EDGE LINE
 - ④ (4") YELLOW LINE 10' DASH 30' SKIP
 - ⑤ (6") WHITE TURN LANE
 - ⑥ (24") STOP LINES
 - ⑦ (12") YELLOW DIAGONALS
 - ⑧ (6") WHITE CROSSWALK LINE (PEDESTRIAN)
 - ⑨ LETTERS AND SYMBOLS
 - ⑩ 6" WHITE DOTTED LINES (2' LINE WITH 6' SPACE)

PAVEMENT IMPROVEMENTS:
 HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

NOTES:
 PERMANENT PAVEMENT MARKING SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKING DETAILS"
 RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ON "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS"



FILE NAME =
 ...\\plons\p1n5960\pp&mirk03.dgn

USER NAME = gthiesse
 PLOT SCALE = 1" = 50'
 PLOT DATE = 5/14/2009

DESIGNED - BJW
 DRAWN - DMS
 CHECKED - JAS
 DATE - 4/21/09

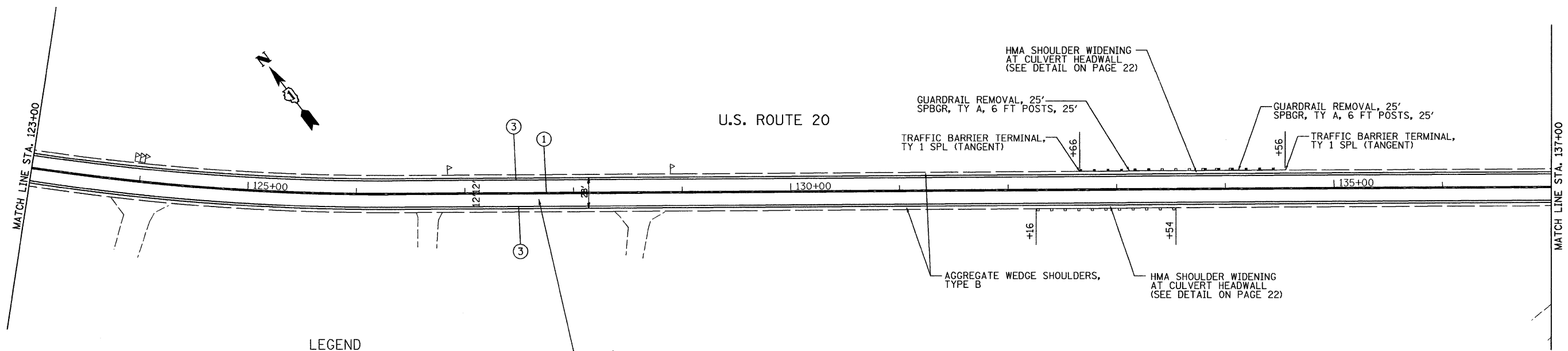
REVISED - 5/14/09
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING
 US 20 - ILL 47 TO W OF PLANK RD**

SCALE: SHEET NO. OF SHEETS STA. 95+00 TO STA. 123+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	9 RS-7	KANE	25	7
CONTRACT NO. 60H26				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



LEGEND

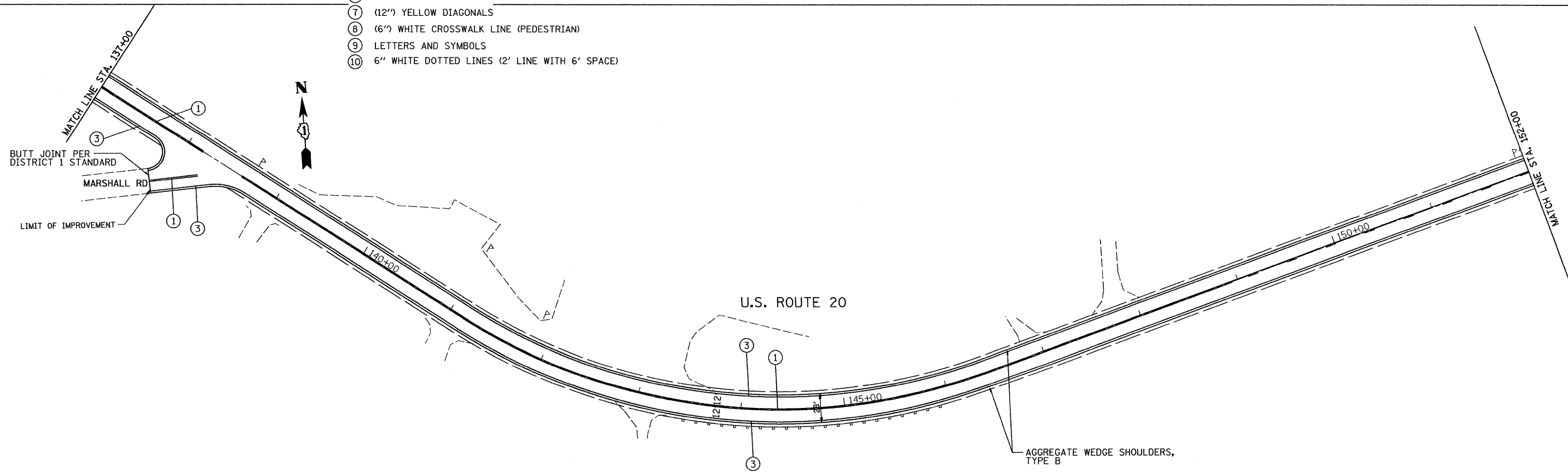
- ① 2-(4") YELLOW @ 11" C-C
- ② (4") YELLOW LINE
- ③ (4") WHITE EDGE LINE
- ④ (4") YELLOW LINE 10' DASH 30' SKIP
- ⑤ (6") WHITE TURN LANE
- ⑥ (24") STOP LINES
- ⑦ (12") YELLOW DIAGONALS
- ⑧ (6") WHITE CROSSWALK LINE (PEDESTRIAN)
- ⑨ LETTERS AND SYMBOLS
- ⑩ 6" WHITE DOTTED LINES (2' LINE WITH 6' SPACE)

PAVEMENT IMPROVEMENTS:

HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

NOTES:

PERMANENT PAVEMENT MARKING SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKING DETAILS"
 RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ON "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS"



FILE NAME =
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USER NAME = gthiesse
 PLOT SCALE = 1" = 50'
 PLOT DATE = 5/14/2009

DESIGNED - BJW
 DRAWN - DMS
 CHECKED - JAS
 DATE - 4/21/09

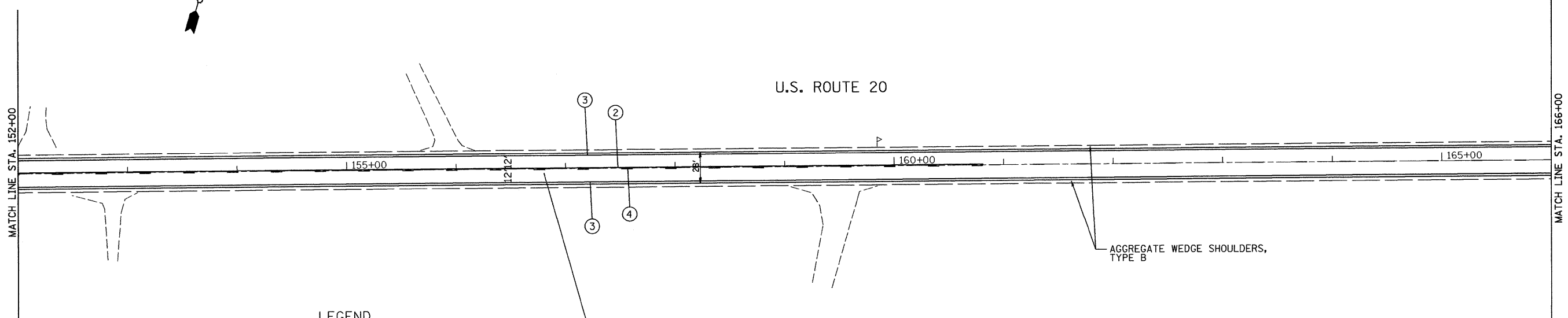
REVISED - 5/14/09
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING
 US 20 - ILL 47 TO W OF PLANK RD**

SCALE: SHEET NO. OF SHEETS STA. 123+00 TO STA. 152+00

F.A.P. RTE. 345	SECTION 9 RS-7	COUNTY KANE	TOTAL SHEETS 25	SHEET NO. 8
CONTRACT NO. 60H26				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



LEGEND

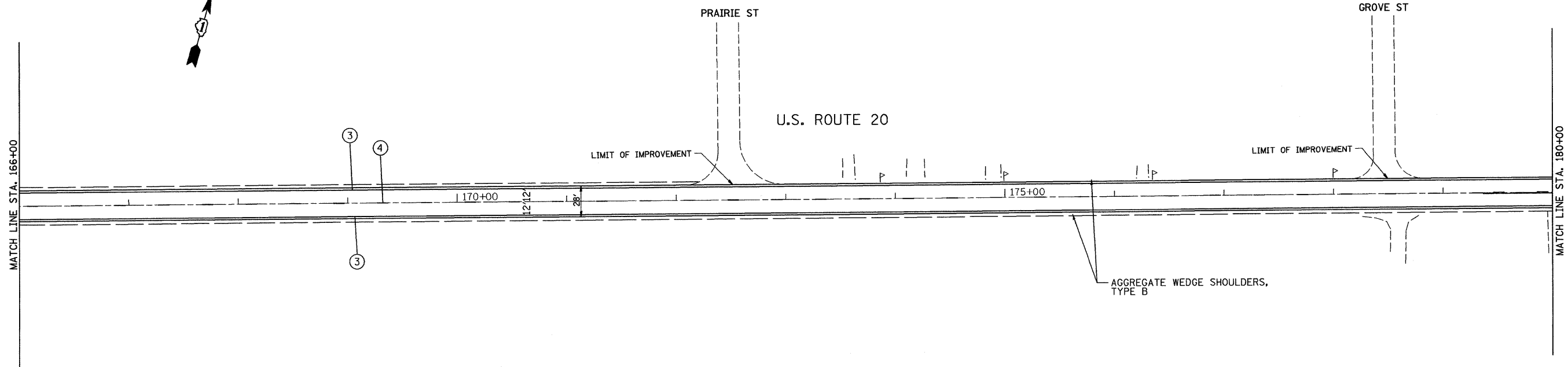
- ① 2-(4") YELLOW @ 11" C-C
- ② (4") YELLOW LINE
- ③ (4") WHITE EDGE LINE
- ④ (4") YELLOW LINE 10' DASH 30' SKIP
- ⑤ (6") WHITE TURN LANE
- ⑥ (24") STOP LINES
- ⑦ (12") YELLOW DIAGONALS
- ⑧ (6") WHITE CROSSWALK LINE (PEDESTRIAN)
- ⑨ LETTERS AND SYMBOLS
- ⑩ 6" WHITE DOTTED LINES (2' LINE WITH 6' SPACE)

PAVEMENT IMPROVEMENTS:

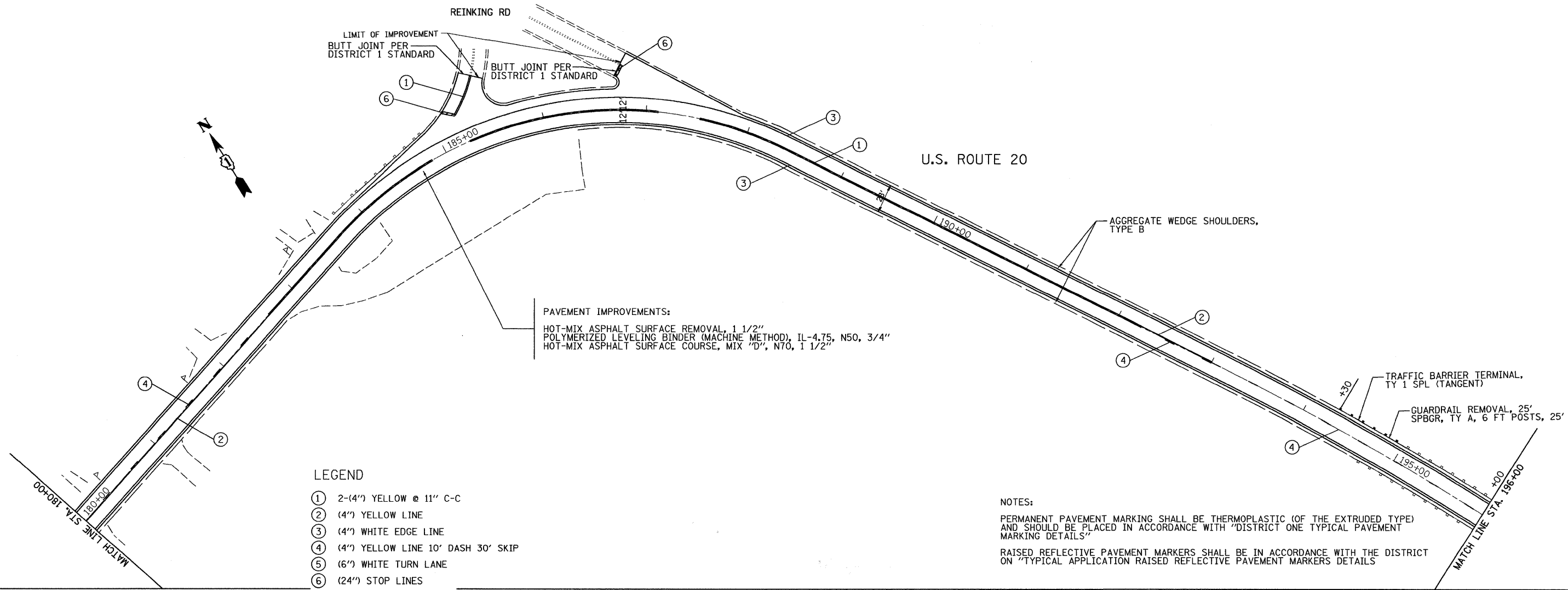
HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

NOTES:

PERMANENT PAVEMENT MARKING SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKING DETAILS"
 RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ON "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS"



FILE NAME = ...\\p1ans\p1n5960\pp&mrk\05.dgn	USER NAME = gthiesse	DESIGNED - BJW	REVISED - 5/14/09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING US 20 - ILL 47 TO W OF PLANK RD			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 1" = 50'	CHECKED - JAS	REVISED -					345	9 RS-7	KANE	25	9
PLOT DATE = 5/14/2009	DATE - 4/21/09	REVISED -	SCALE:		SHEET NO.	OF SHEETS	STA. 152+00	TO STA. 180+00	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			
					CONTRACT NO. 60H26							



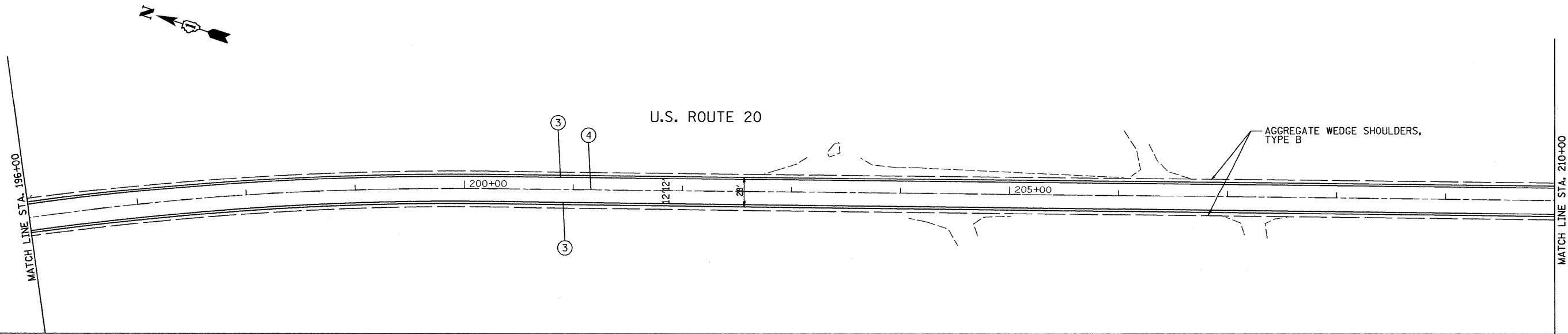
PAVEMENT IMPROVEMENTS:
 HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

LEGEND

- ① 2-(4") YELLOW @ 11" C-C
- ② (4") YELLOW LINE
- ③ (4") WHITE EDGE LINE
- ④ (4") YELLOW LINE 10' DASH 30' SKIP
- ⑤ (6") WHITE TURN LANE
- ⑥ (24") STOP LINES
- ⑦ (12") YELLOW DIAGONALS
- ⑧ (6") WHITE CROSSWALK LINE (PEDESTRIAN)
- ⑨ LETTERS AND SYMBOLS
- ⑩ 6" WHITE DOTTED LINES (2' LINE WITH 6' SPACE)

NOTES:

PERMANENT PAVEMENT MARKING SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKING DETAILS"
 RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ON "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS"



FILE NAME =
 ...Plans\p1n59609pp&mrk06.dgn

USER NAME = gthaeese
 PLOT SCALE = 1" = 50'
 PLOT DATE = 5/14/2009

DESIGNED - BJW
 DRAWN - DMS
 CHECKED - JAS
 DATE - 4/21/09

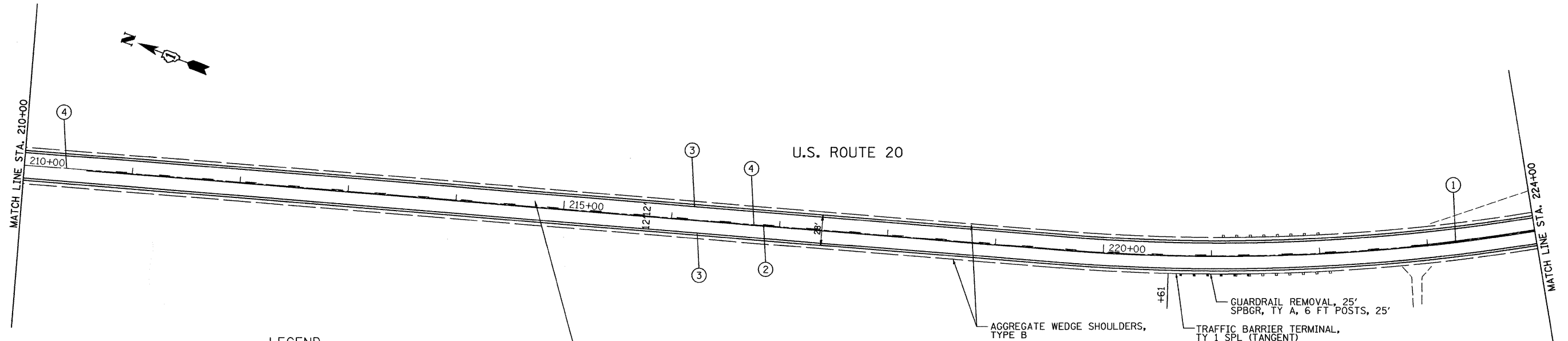
REVISED - 5/14/09
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING
 US 20 - ILL 47 TO W OF PLANK RD

SCALE: SHEET NO. OF SHEETS STA. 180+00 TO STA. 210+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	9 RS-7	KANE	25	10
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60H26	

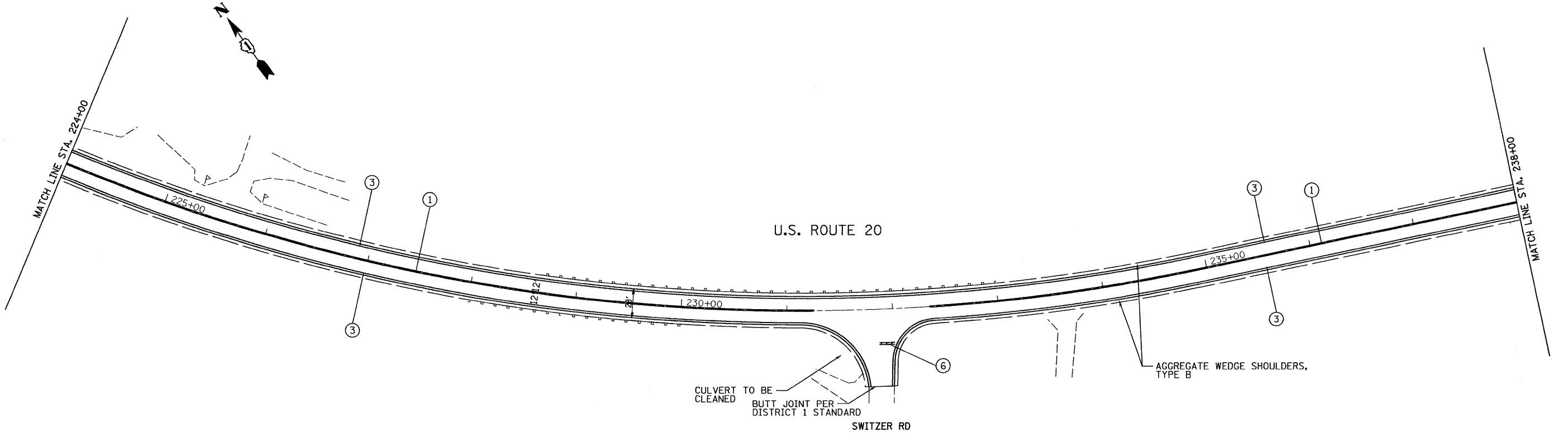


LEGEND

- ① 2-(4") YELLOW @ 11" C-C
- ② (4") YELLOW LINE
- ③ (4") WHITE EDGE LINE
- ④ (4") YELLOW LINE 10' DASH 30' SKIP
- ⑤ (6") WHITE TURN LANE
- ⑥ (24") STOP LINES
- ⑦ (12") YELLOW DIAGONALS
- ⑧ (6") WHITE CROSSWALK LINE (PEDESTRIAN)
- ⑨ LETTERS AND SYMBOLS
- ⑩ 6" WHITE DOTTED LINES (2' LINE WITH 6' SPACE)

PAVEMENT IMPROVEMENTS:
 HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

NOTES:
 PERMANENT PAVEMENT MARKING SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKING DETAILS"
 RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ON "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS"



FILE NAME =
 ...\P1ans\p1n59603pp&mk07.dgn

USER NAME = gthiesse
 PLOT SCALE = 1" = 50'
 PLOT DATE = 5/14/2009

DESIGNED - BJW
 DRAWN - DMS
 CHECKED - JAS
 DATE - 4/21/09

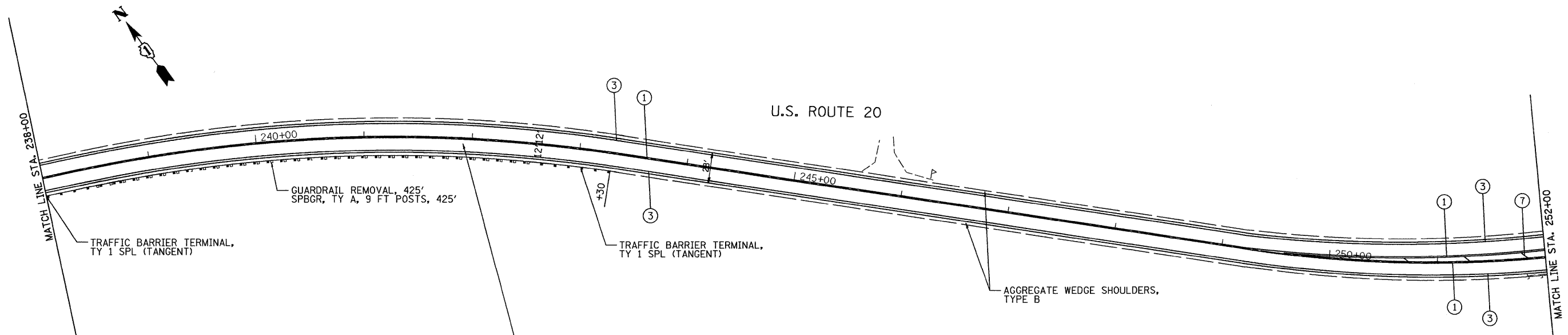
REVISED - 5/14/09
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING
 US 20 - ILL 47 TO W OF PLANK RD**

SCALE: SHEET NO. OF SHEETS STA. 210+00 TO STA. 238+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	9 RS-7	KANE	25	11
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60H26	



LEGEND

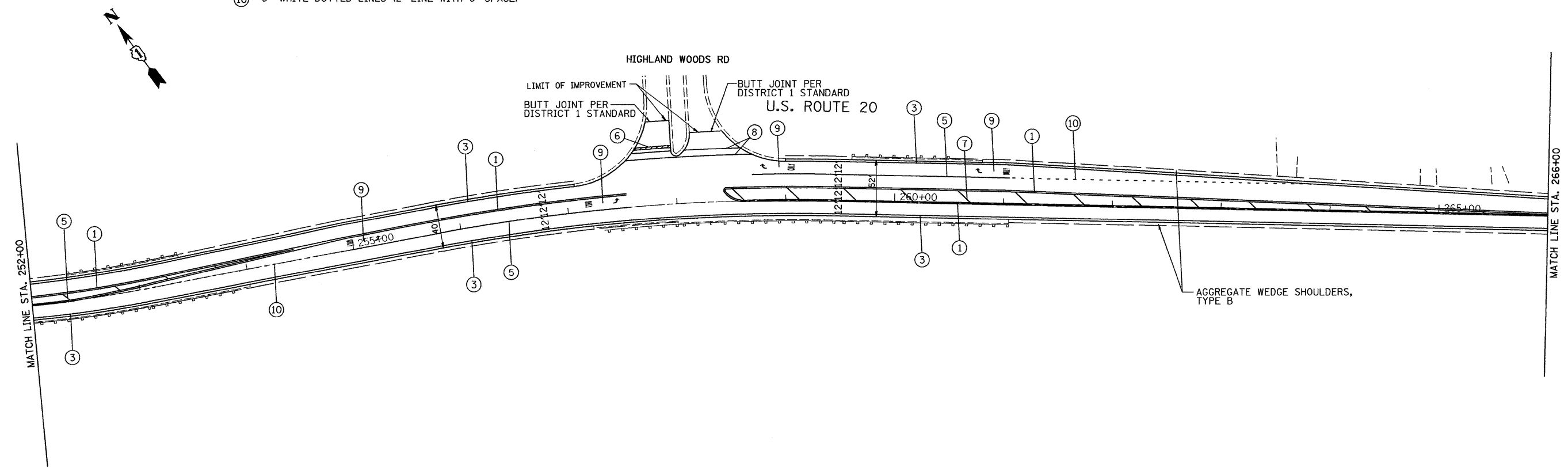
- ① 2-(4") YELLOW @ 11" C-C
- ② (4") YELLOW LINE
- ③ (4") WHITE EDGE LINE
- ④ (4") YELLOW LINE 10' DASH 30' SKIP
- ⑤ (6") WHITE TURN LANE
- ⑥ (24") STOP LINES
- ⑦ (12") YELLOW DIAGONALS
- ⑧ (6") WHITE CROSSWALK LINE (PEDESTRIAN)
- ⑨ LETTERS AND SYMBOLS
- ⑩ 6" WHITE DOTTED LINES (2' LINE WITH 6' SPACE)

PAVEMENT IMPROVEMENTS:

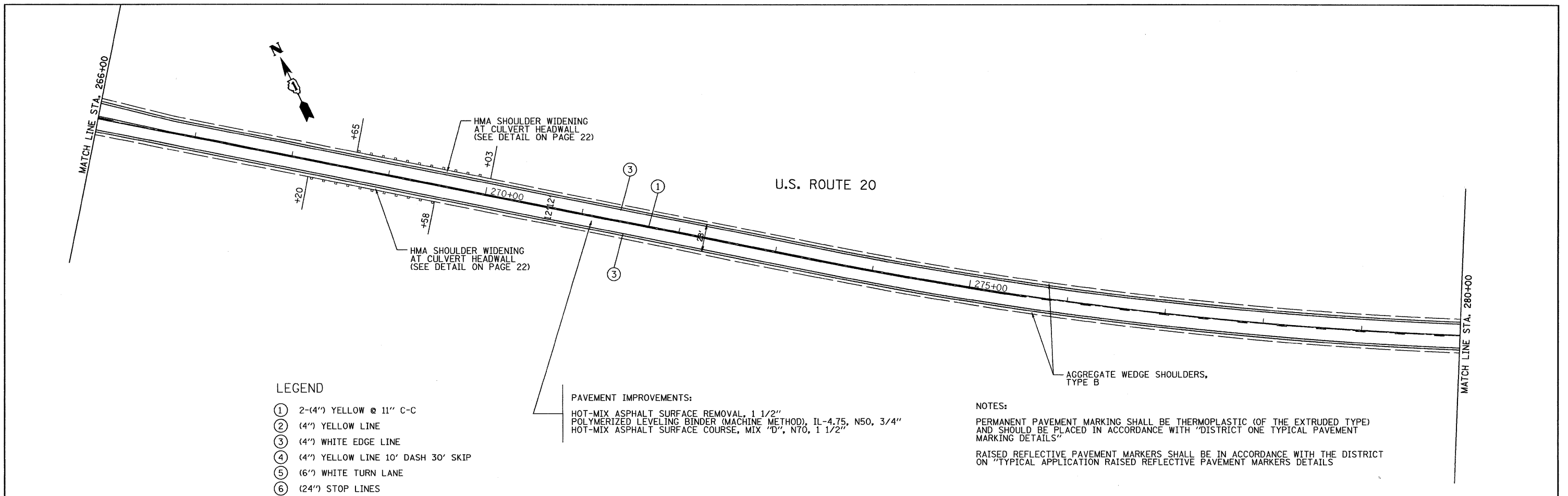
HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

NOTES:

PERMANENT PAVEMENT MARKING SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKING DETAILS"
 RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ON "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS"



FILE NAME = ...\\p1ans\p1n5960\pp&mrk08.dgn	USER NAME = gthuesse	DESIGNED - BJW	REVISED - 5/14/09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING US 20 - ILL 47 TO W OF PLANK RD			F.A.P. RTE. 345	SECTION 9 RS-7	COUNTY KANE	TOTAL SHEETS 25	SHEET NO. 12
PLOT SCALE = 1" = 50'	CHECKED - JAS	REVISED -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA. 238+00	TO STA. 266+00	CONTRACT NO. 60H26	
PLOT DATE = 5/14/2009	DATE - 4/21/09	REVISED -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							



LEGEND

- ① 2-(4") YELLOW @ 11" C-C
- ② (4") YELLOW LINE
- ③ (4") WHITE EDGE LINE
- ④ (4") YELLOW LINE 10' DASH 30' SKIP
- ⑤ (6") WHITE TURN LANE
- ⑥ (24") STOP LINES
- ⑦ (12") YELLOW DIAGONALS
- ⑧ (6") WHITE CROSSWALK LINE (PEDESTRIAN)
- ⑨ LETTERS AND SYMBOLS
- ⑩ 6" WHITE DOTTED LINES (2' LINE WITH 6' SPACE)

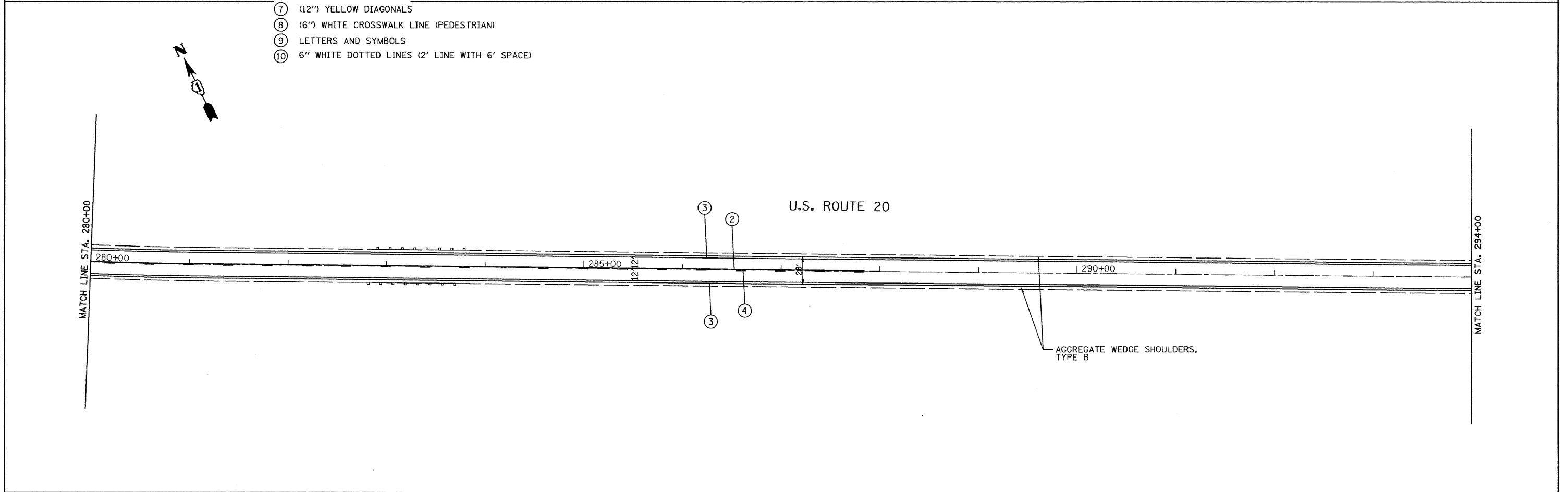
PAVEMENT IMPROVEMENTS:

HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

NOTES:

PERMANENT PAVEMENT MARKING SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKING DETAILS"

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ON "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS"



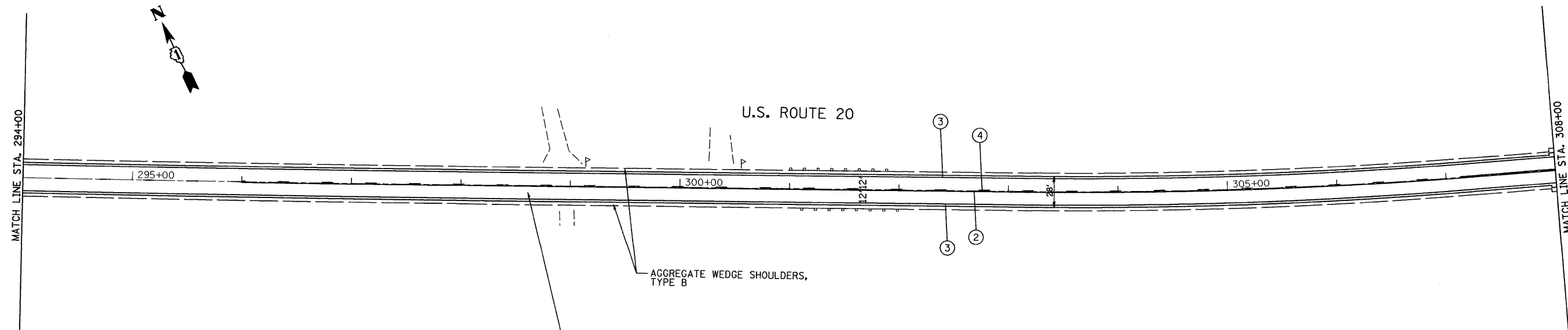
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PLOT SCALE = 1" = 50'	
PLOT DATE = 5/14/2009	

DESIGNED - BJW	REVISED - 5/14/09
DRAWN - DMS	REVISED -
CHECKED - JAS	REVISED -
DATE - 4/21/09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY AND PAVEMENT MARKING US 20 - ILL 47 TO W OF PLANK RD			
SCALE:	SHEET NO. OF SHEETS	STA. 266+00 TO STA. 294+00	

F.A.P. RTE. 345	SECTION 9 RS-7	COUNTY KANE	TOTAL SHEETS 25	SHEET NO. 13
CONTRACT NO. 60H26				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



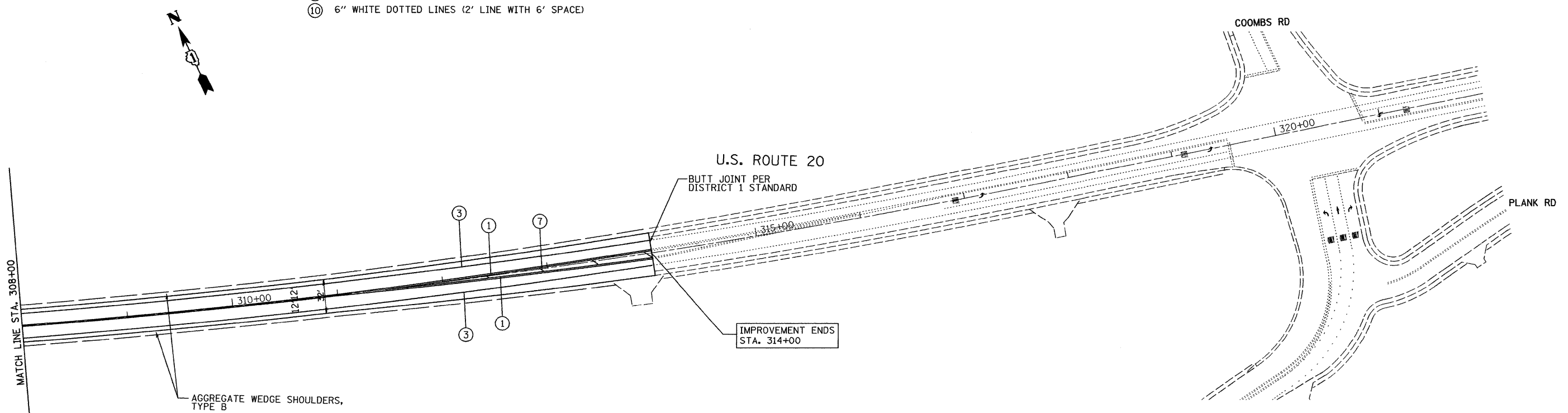
LEGEND

- ① 2-(4") YELLOW @ 11" C-C
- ② (4") YELLOW LINE
- ③ (4") WHITE EDGE LINE
- ④ (4") YELLOW LINE 10' DASH 30' SKIP
- ⑤ (6") WHITE TURN LANE
- ⑥ (24") STOP LINES
- ⑦ (12") YELLOW DIAGONALS
- ⑧ (6") WHITE CROSSWALK LINE (PEDESTRIAN)
- ⑨ LETTERS AND SYMBOLS
- ⑩ 6" WHITE DOTTED LINES (2' LINE WITH 6' SPACE)

PAVEMENT IMPROVEMENTS:
 HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

NOTES:

PERMANENT PAVEMENT MARKING SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKING DETAILS"
 RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ON "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS"



FILE NAME =
 ...\\plans\p1r5960\pp&mrk18.dgn

USER NAME = gthieso
 PLOT SCALE = 1" = 50'
 PLOT DATE = 5/14/2009

DESIGNED - BJW
 DRAWN - DMS
 CHECKED - JAS
 DATE - 4/21/09

REVISED - 5/14/09
 REVISED -
 REVISED -
 REVISED -

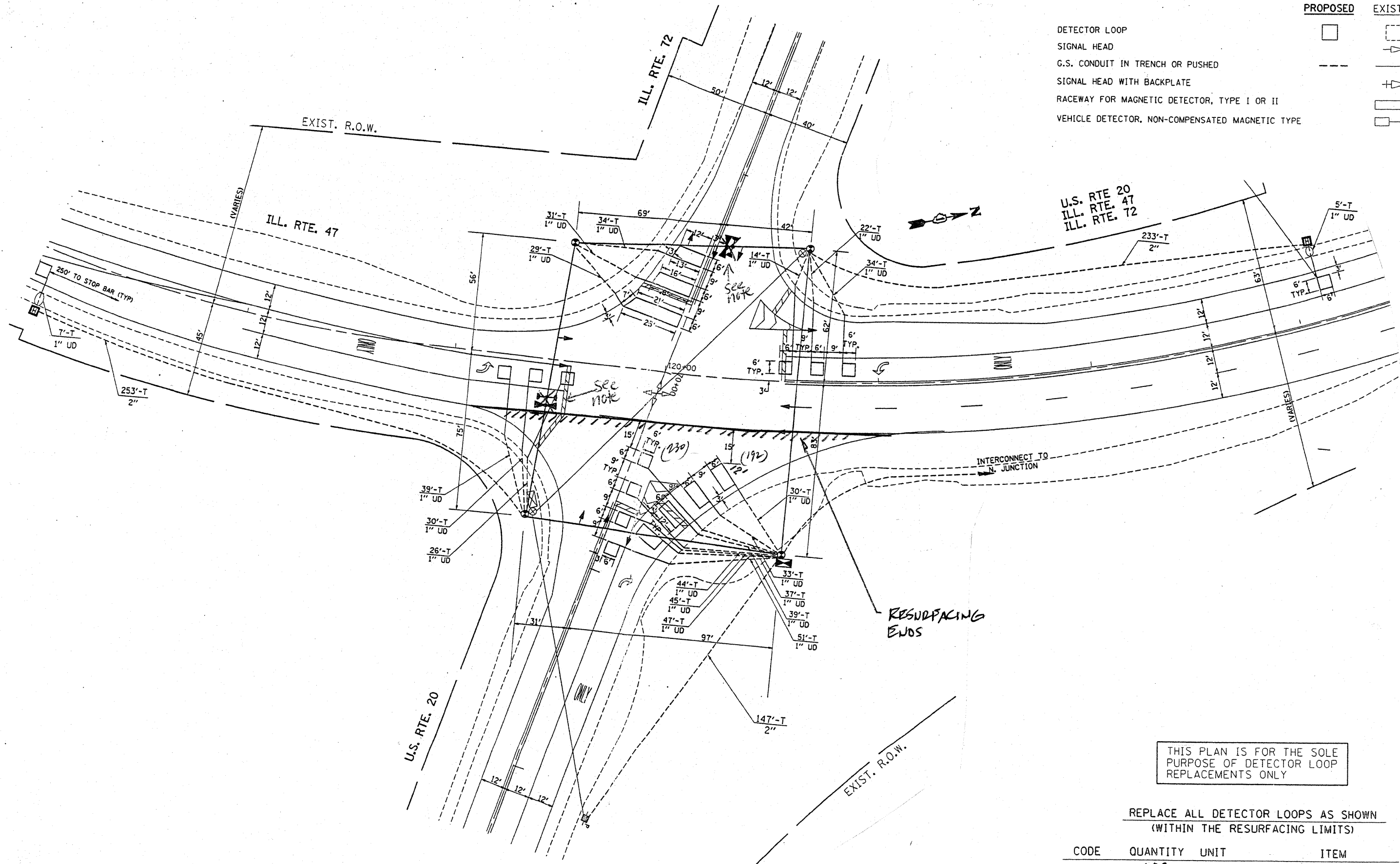
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING
 US 20 - ILL 47 TO W OF PLANK RD**
 SCALE: SHEET NO. OF SHEETS STA. 294+00 TO STA. 322+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	9 RS-7	KANE	25	14
CONTRACT NO. 60H26				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		

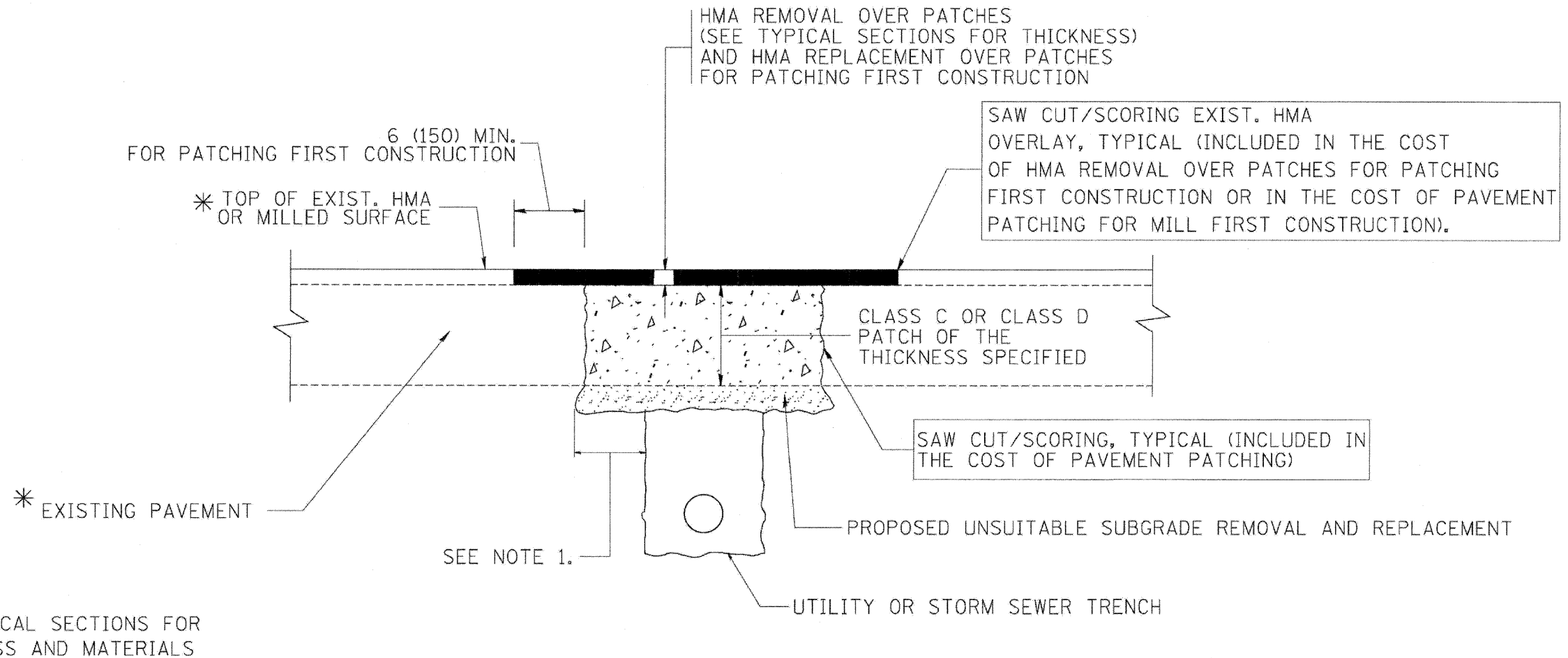


THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	422	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = konthaphixaybc	DESIGNED - BCK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT U.S. 20 @ ILL. RTE. 47 S. JUNCT.	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pawork\FMIDOT\KANTHAPHIXAYBC\d81126	1\traffsig.legend_v7.dgn	DRAWN - BCK	REVISED -								
	PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -			SCALE: NONE	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO.
	PLOT DATE = 4/3/2009	DATE	REVISED -								



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

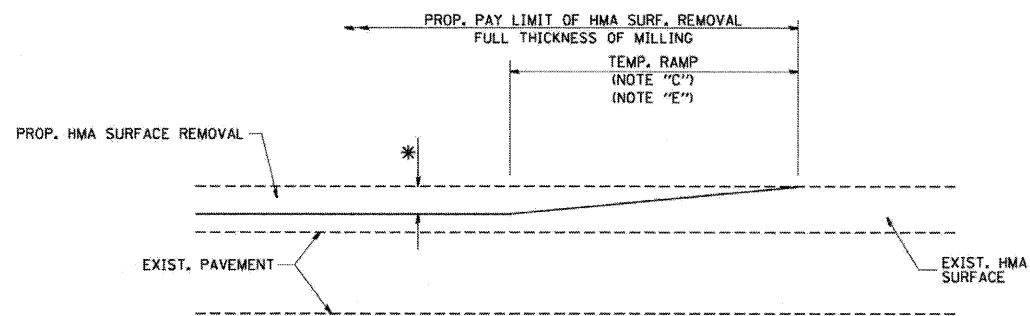
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

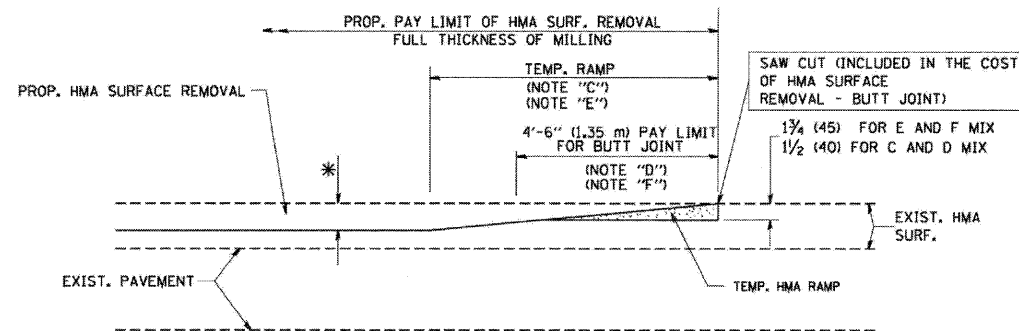
FILE NAME = c:\p\proj\dist\22x34\bd22.dgn	USER NAME = bweigd	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	345	9 R5-7	KANE	25	15
	PLOT DATE = 10/27/2008	CHECKED -	REVISED - R. BORO 09-04-07		BD400-04 (BD-22) CONTRACT NO.							
		DATE - 10-25-94	REVISED - K. ENG 10-27-08		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

FILE NAME = ...p\in59609_Dist1_Det1.dgn	USER NAME = bweigand	DESIGNED - BJW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 5.00' / IN.	DRAWN - DMS	REVISED -		SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	345	9 R5-7	KANE	25	15
	PLOT DATE = 4/21/2009	CHECKED - JAS	REVISED -		CONTRACT NO. 60H26							
		DATE - 4/21/09	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

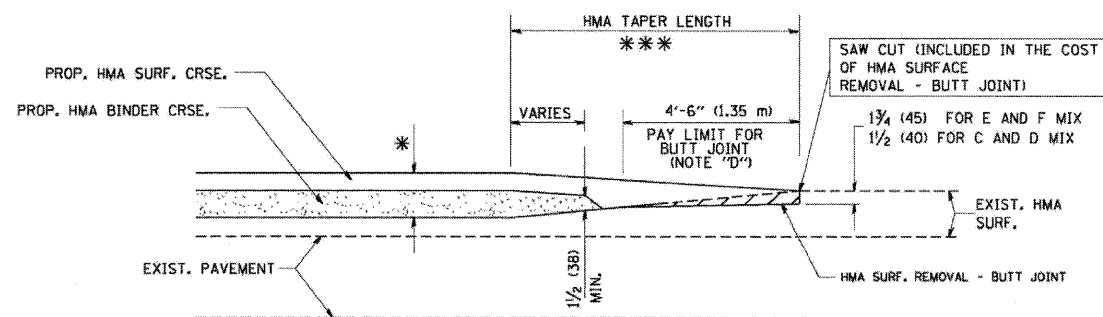
OPTION 1



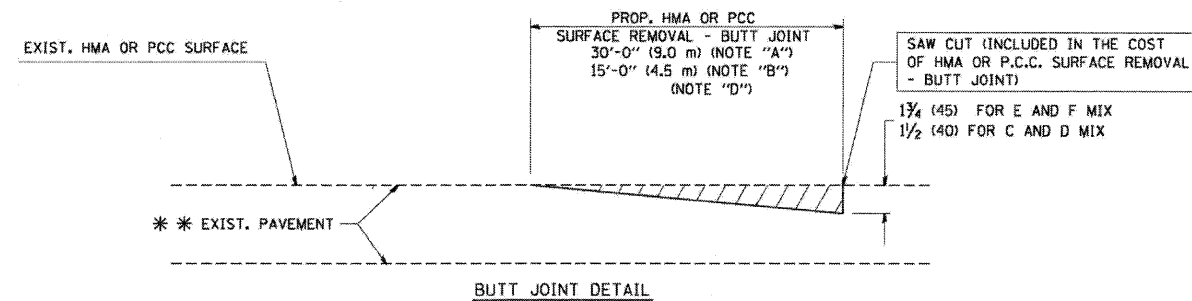
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

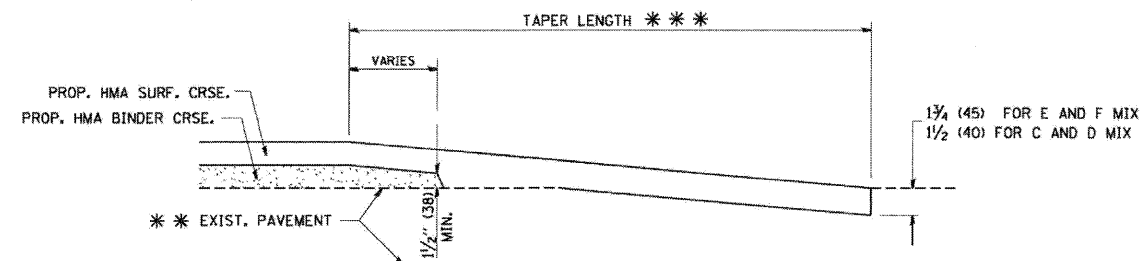
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

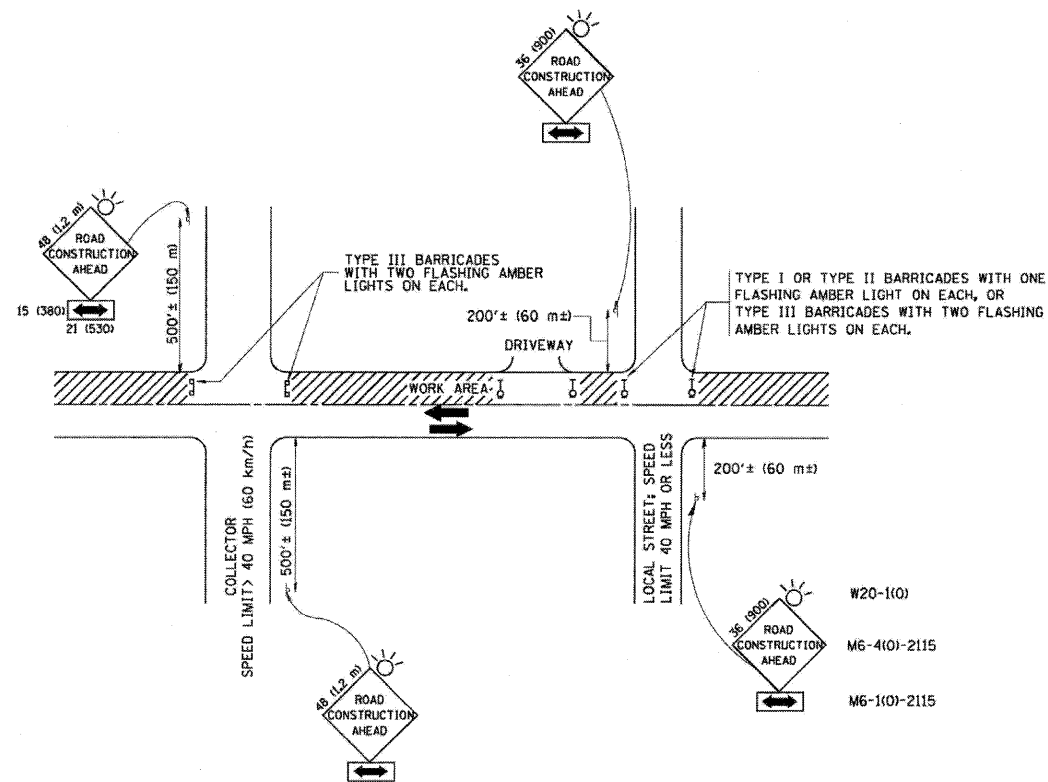
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	CHECKED - JAS	REVISED -
	DATE - 4/21/09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	9 RS-7	KANE	25	16
		CONTRACT NO. 60H26		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



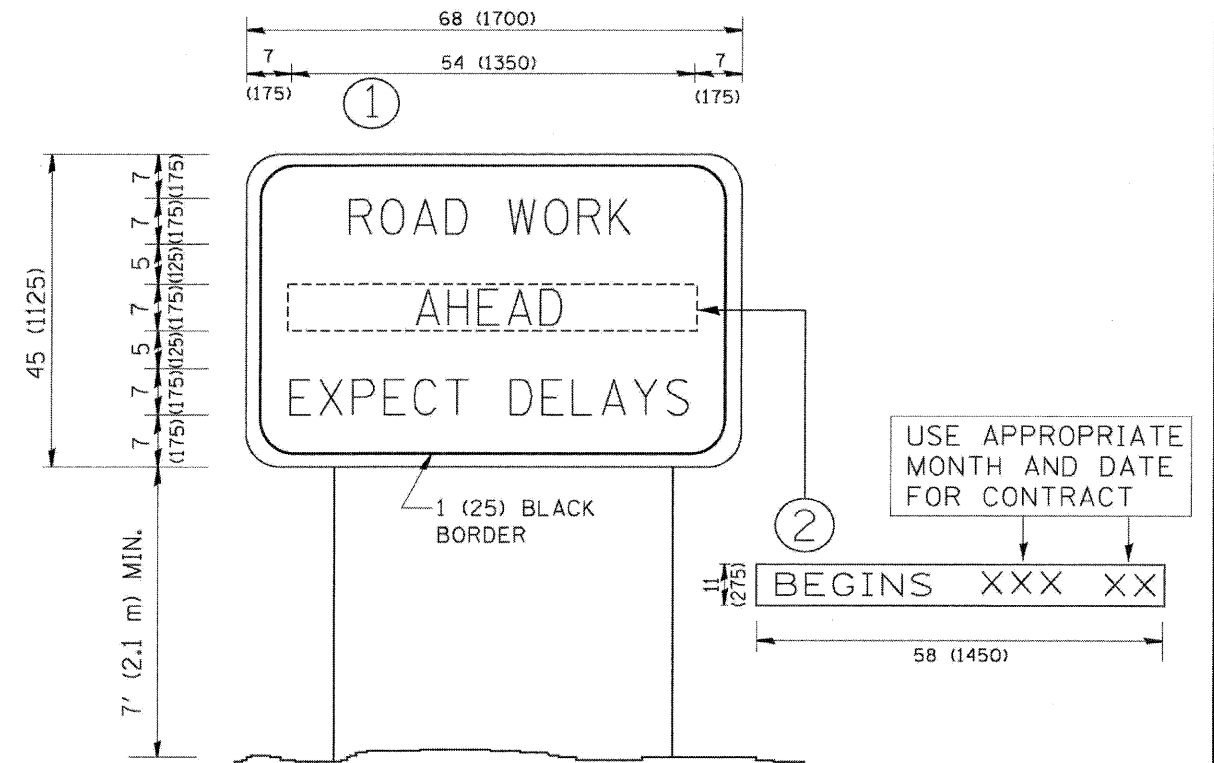
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1,2 m x 1,2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).**
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
 USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

All dimensions are in millimeters (inches) unless otherwise shown.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-10	CONTRACT NO.	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT	



NOTES:

- USE BLACK LETTERING ON ORANGE BACKGROUND.
- ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
- SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
- SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-22	CONTRACT NO.	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT	

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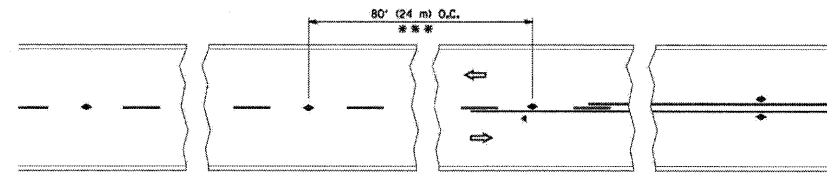
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS,
AND DRIVEWAYS AND ARTERIAL ROAD INFORMATION SIGN

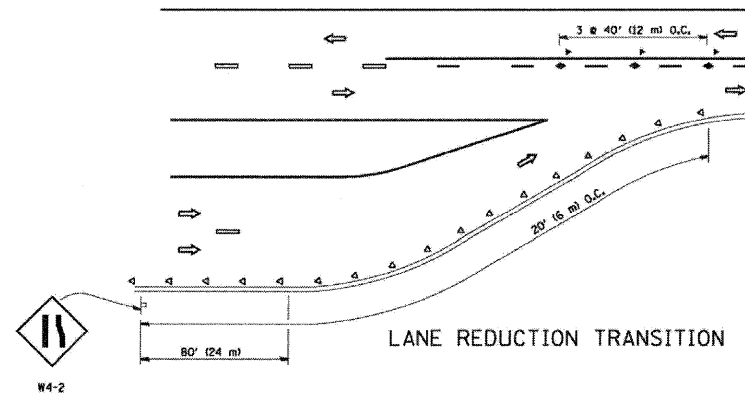
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60H26				
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

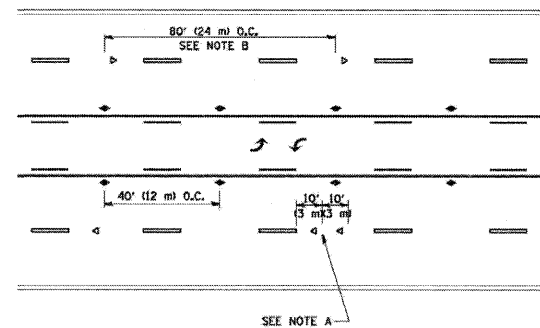


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

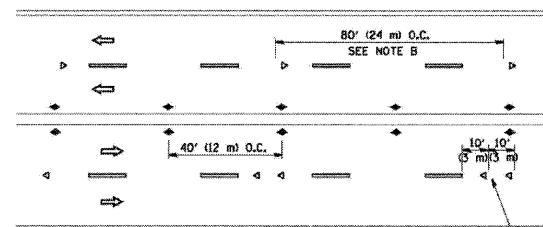
TWO-LANE/TWO-WAY



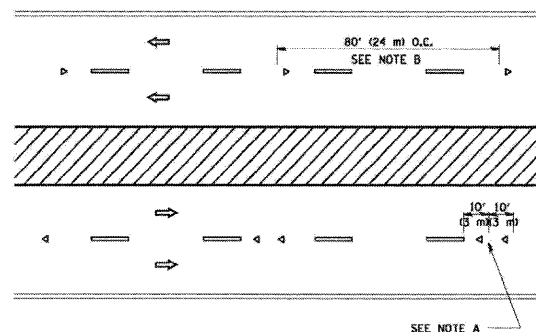
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

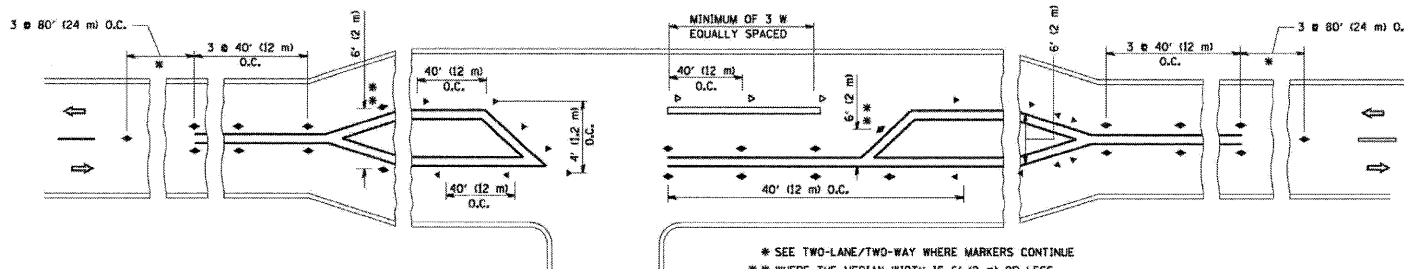
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT DATE = 1/4/2008	CHECKED - JAS	REVISED - T. RAMMACHER 01-06-00
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

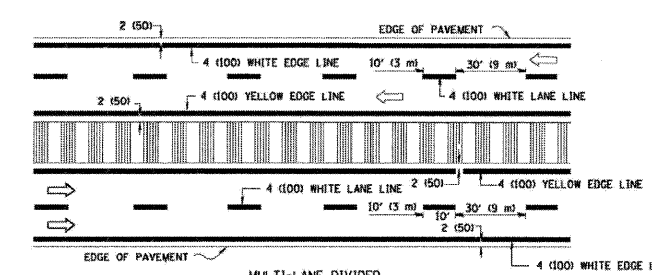
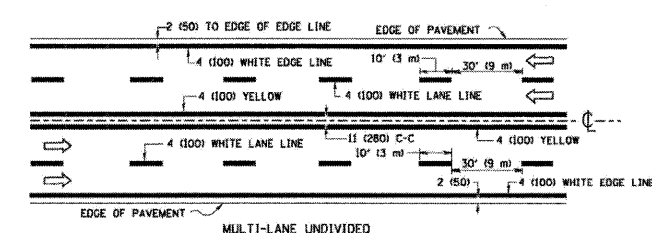
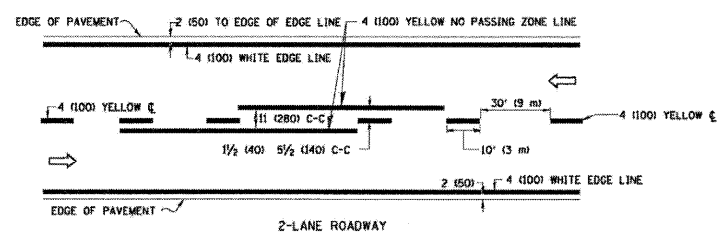
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RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			TC-11			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	CONTRACT NO.			
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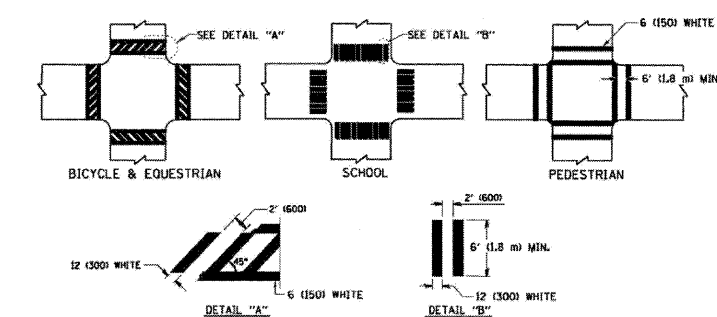
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

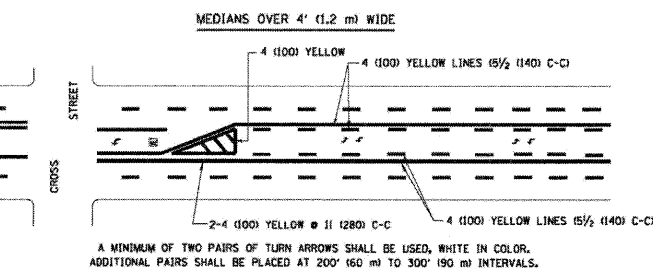
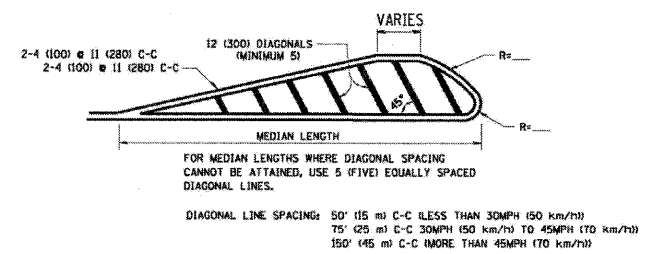
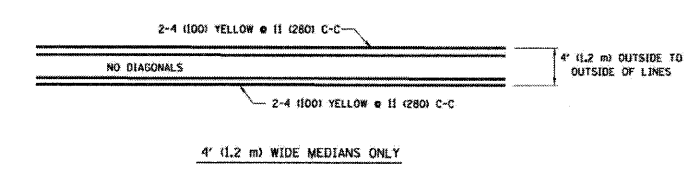
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RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)		345	9 RS-7	KANE	25	18
SCALE:	SHEET NO. OF SHEETS	STA.	CONTRACT NO. 60H26			
		FED. ROAD DIST. NO. ILLINOIS		FED. AID PROJECT		



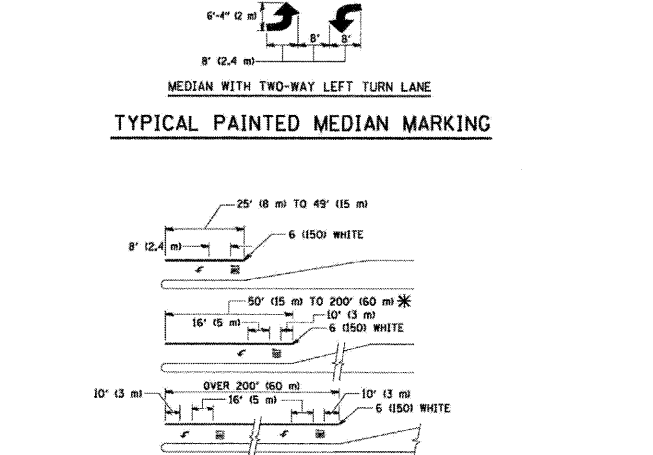
TYPICAL LANE AND EDGE LINE MARKING



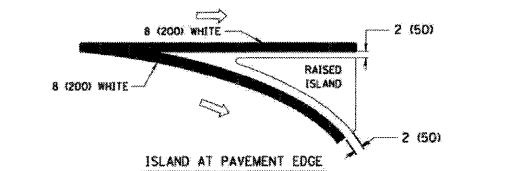
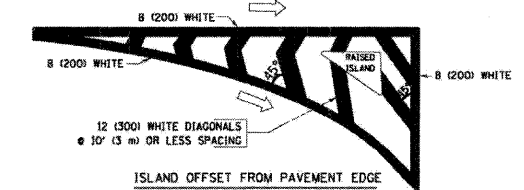
TYPICAL CROSSWALK MARKING



TYPICAL PAINTED MEDIAN MARKING



TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE, FULL SIZE LETTERS & SYMBOLS 18' (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH, 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL))	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW WHITE WHITE	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 78000 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 78000.

All dimensions are in inches (millimeters) unless otherwise shown.

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		DRAWN -	REVISED - A. HOUSEH 10-09-96
	PLOT SCALE = 58,000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-17-96
	PLOT DATE = 1/4/2008	DATE - 03-19-90	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-13 CONTRACT NO.		

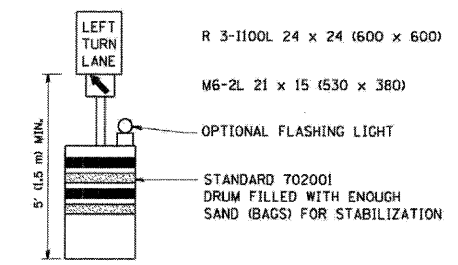
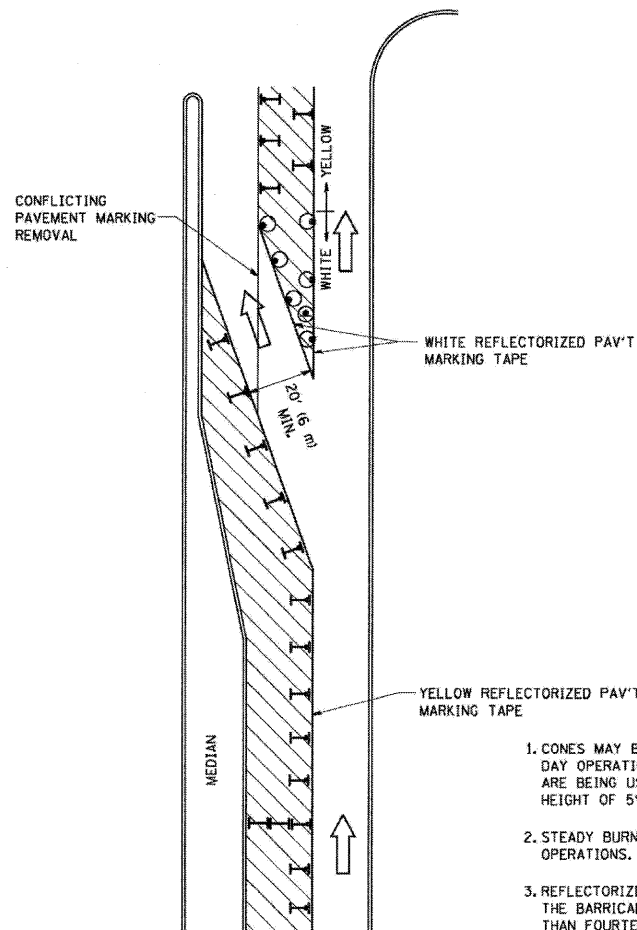
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FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE:	SHEET NO. OF SHEETS	STA.	TO STA.	9 RS-7 CONTRACT NO. 60H26		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	9 RS-7	KANE	25	19
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

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		DESIGNED - CHECKED -	REVISED - REVISED -
		DESIGNED - DATE -	REVISED - REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

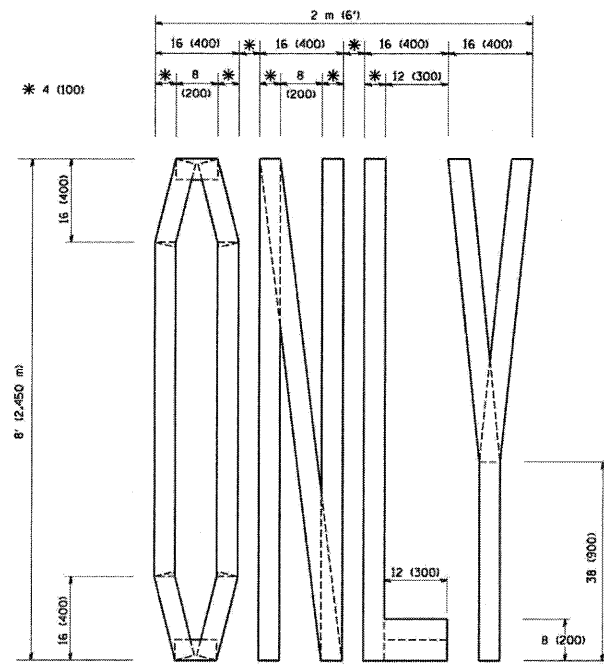
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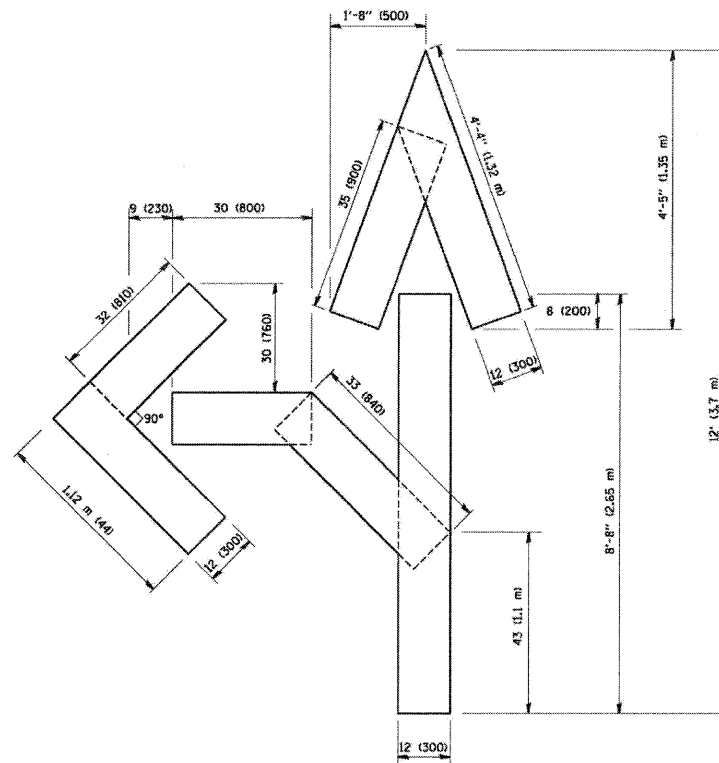
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	
SCALE:	SHEET NO. OF SHEETS STA. TO STA.

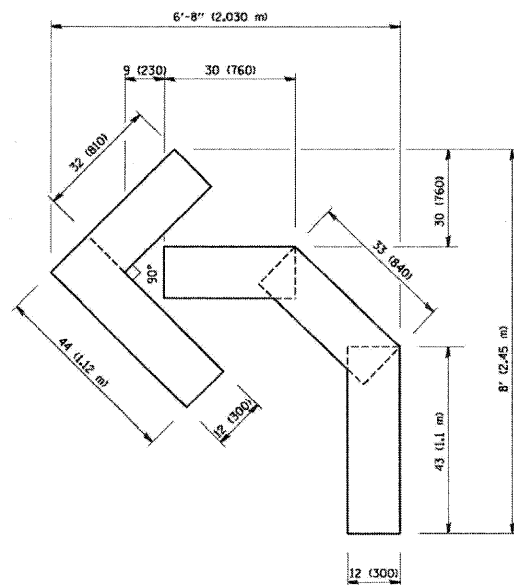
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	9 RS-7	KANE	25	20
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters)
unless otherwise shown.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
FOR TRAFFIC STAGING

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-16			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	CONTRACT NO.	
			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

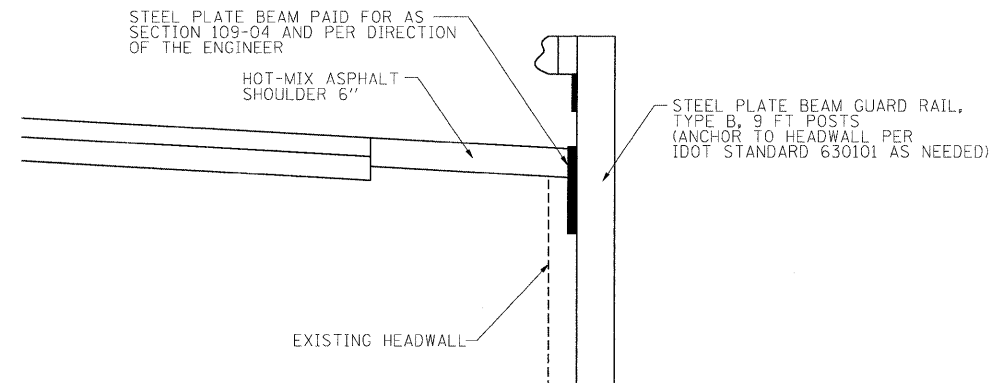
FILE NAME =
...pin59609_Dist1_Det07.dgn

USER NAME = bweizand	DESIGNED - DRAWN - CHECKED - DATE - 4/21/09	REVISOR - BJW - DMS - JAS - REVISED -
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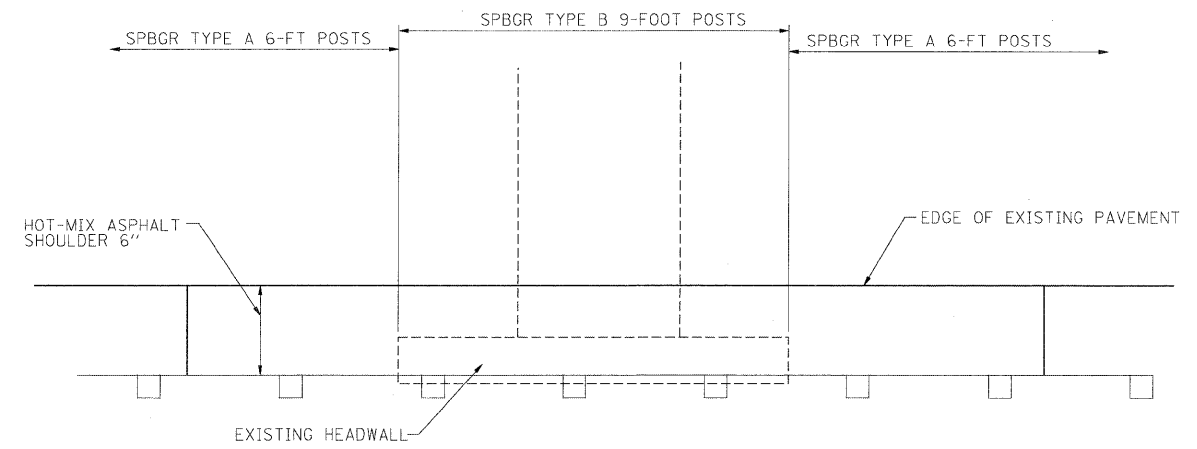
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
FOR TRAFFIC STAGING

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	9 R5-7	KANE	25	21
SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	CONTRACT NO. 60H26	
			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	



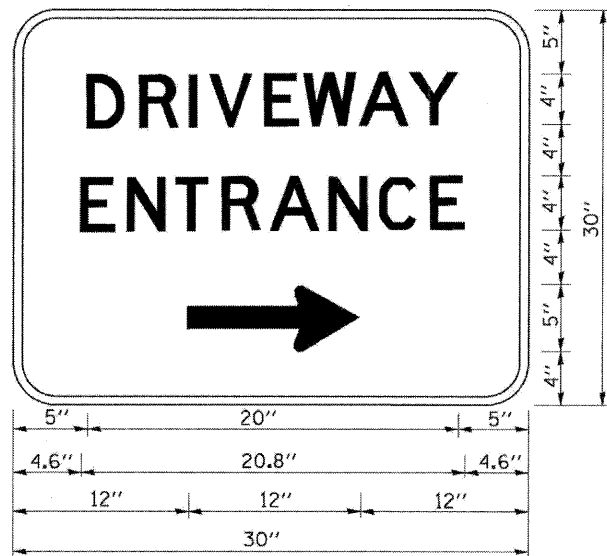
CROSS SECTION



NOTE: LENGTH OF SHOULDER WIDENING TO BE DETERMINED BY ENGINEER

PLAN VIEW

FILE NAME = ...p\In59609_Dist1_Det08.dgn	USER NAME = bweisgond	DESIGNED - BJW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HMA SHOULDER WIDENING AT CULVERT HEADWALL			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 5.00' / IN.	CHECKED - JAS	REVISED -					345	9 RS-7	KANE	25	22	
	PLOT DATE = 4/21/2009	DATE - 4/21/09	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 60H26		
								FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT			



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = W:\diststd\22x34\to26.dgn	USER NAME = gaglionob	DESIGNED - DRAWN -	REVISOR - REVISOR -	DESIGNED - DRAWN -	REVISOR - REVISOR -
		CHECKED - DATE -	REVISOR - REVISOR -	CHECKED - DATE -	REVISOR - REVISOR -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

TC-26 CONTRACT NO.
 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

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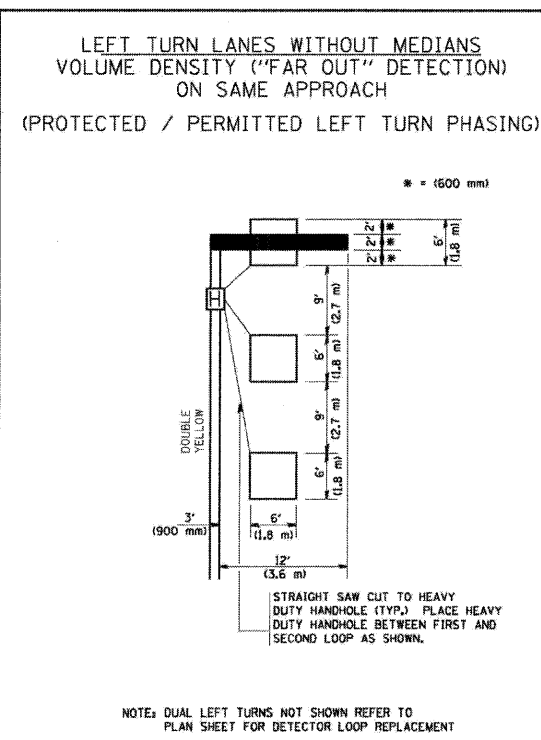
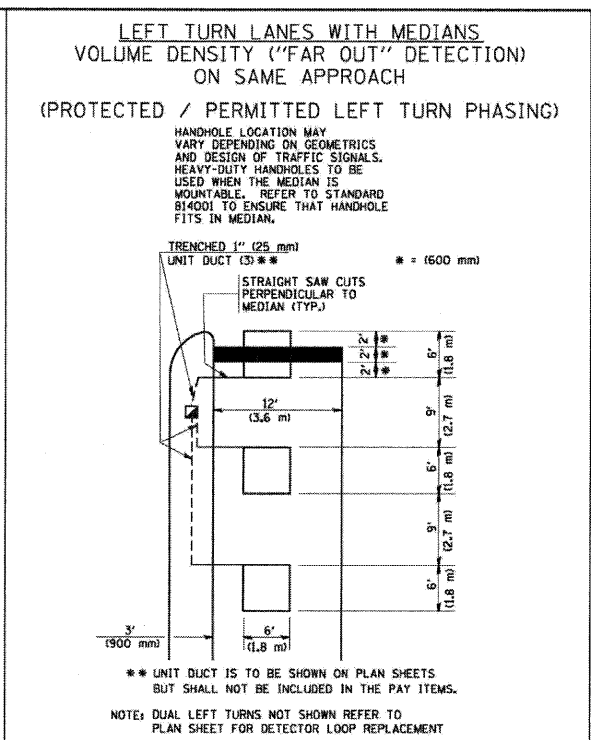
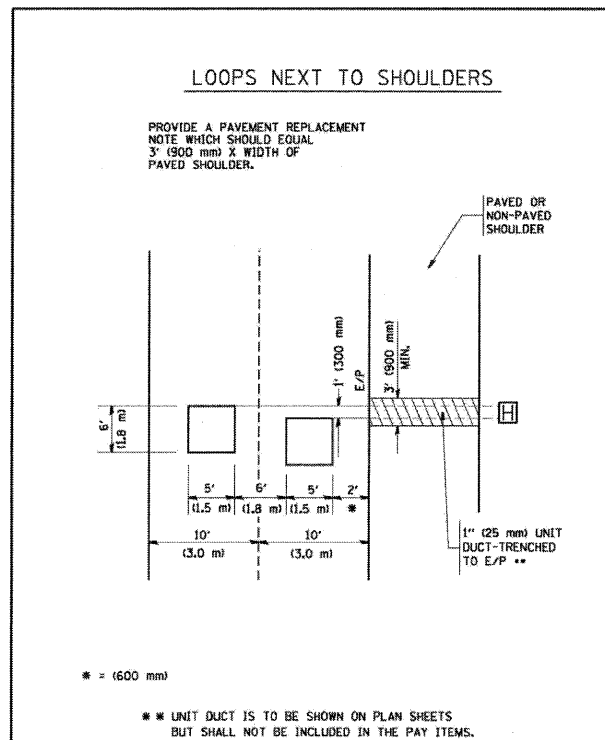
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY ENTRANCE
 SIGNING**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	9 RS-7	KANE	25	23

SCALE: SHEET NO. OF SHEETS STA. TO STA.

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT CONTRACT NO. 60H26



NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

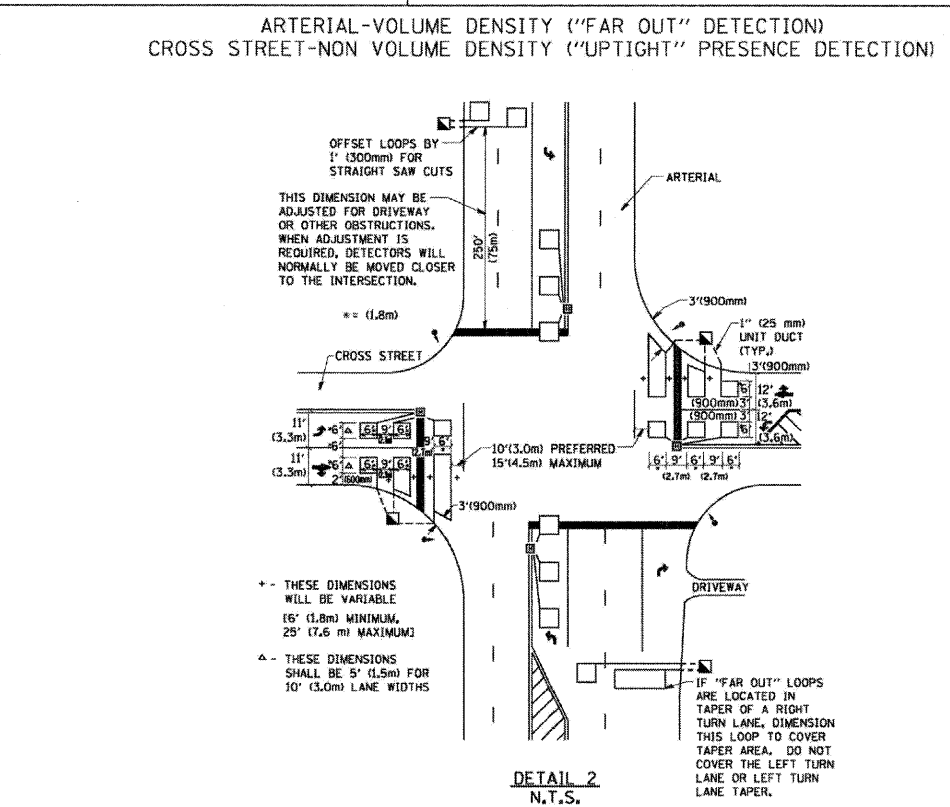
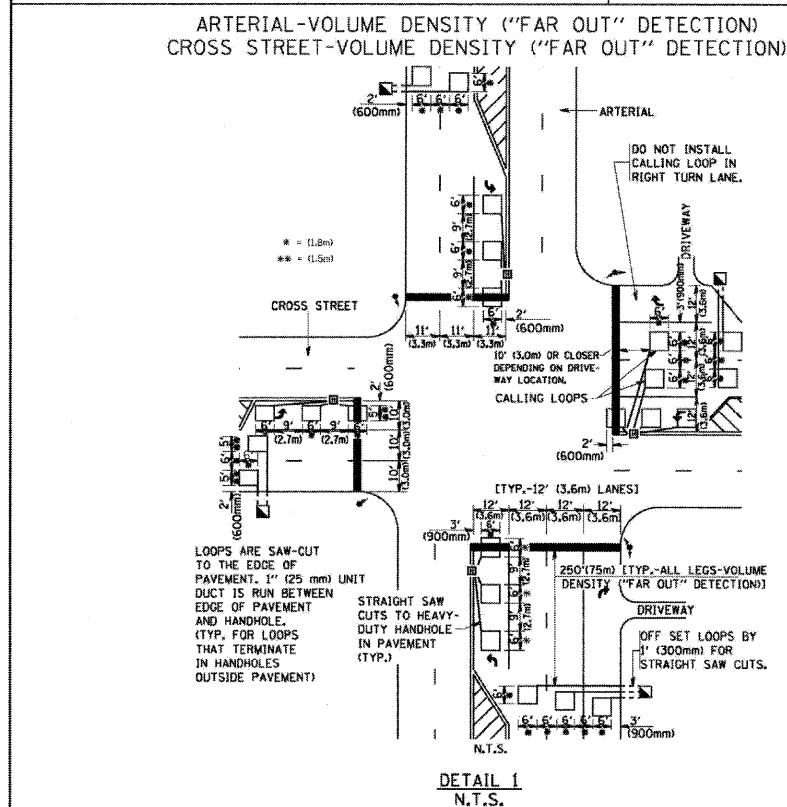
LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.



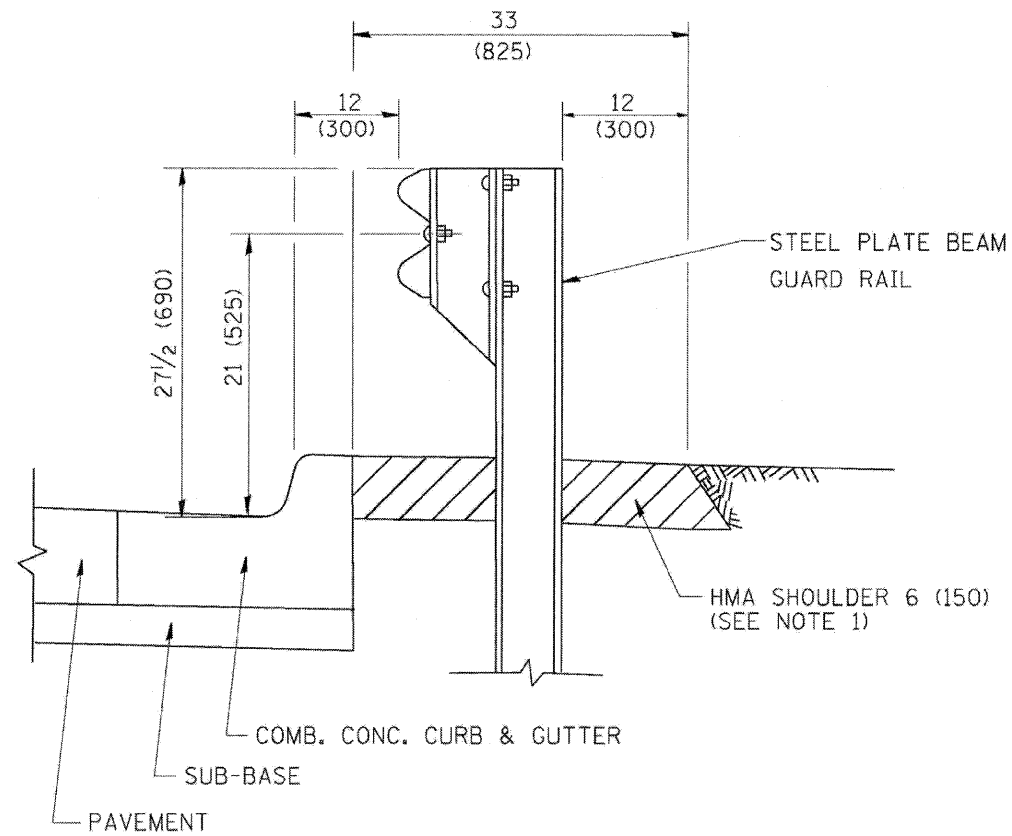
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		DRAWN -	REVISED -
		CHECKED - R.K.F.	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	F.A.P. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
			TS-07 CONTRACT NO.
			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

FILE NAME = ...vpln59689.Dist1.Det1A.dgn	USER NAME = bwsgend	DESIGNED - BJW	REVISED -
		DRAWN - DMS	REVISED -
		CHECKED - JAS	REVISED -
		DATE - 4/21/09	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	
SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	F.A.P. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
			345 9 RS-7 KANE 25 24
			CONTRACT NO. 60H26
			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	
SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	F.A.P. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
			345 9 RS-7 KANE 25 24
			CONTRACT NO. 60H26
			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

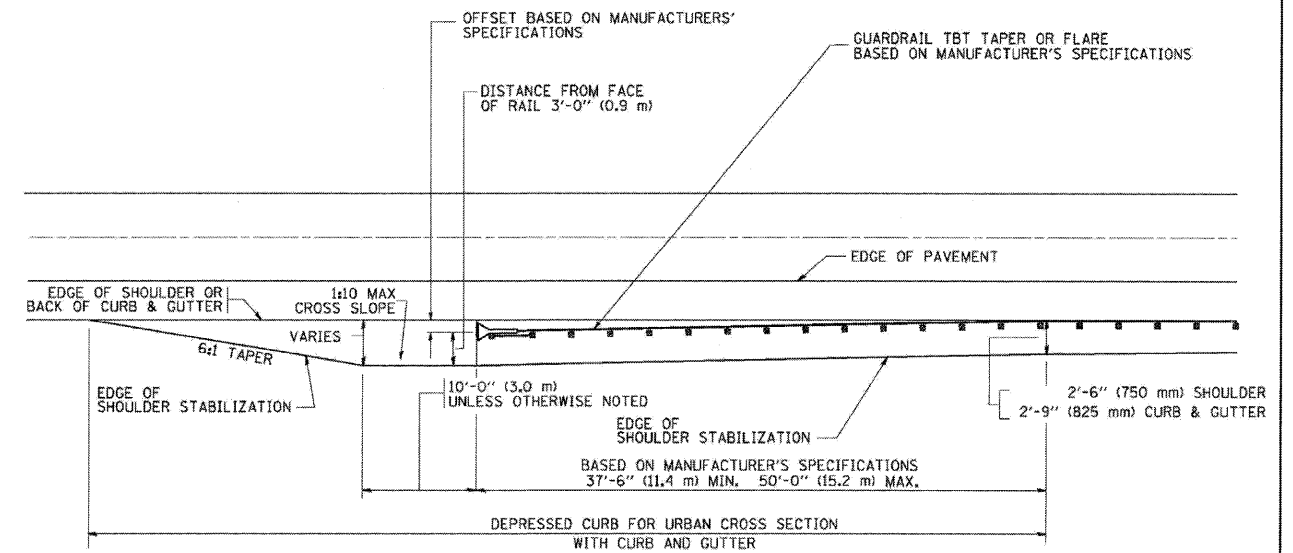


- NOTES: 1. THE HMA SHOULDER SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL
2. GUARD RAIL MAY BE PLACED AT THE BACK OF CURB WHEN DIRECTED BY THE ENGINEER.

BASIS OF PAYMENT: HMA SHOULDER 6 (150) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDER 6" (150 mm)".

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER
 [FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]



STABILIZATION AT TBT TY. 1 SPL.

TBT = TRAFFIC BARRIER TERMINAL
 ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = C:\Projects\22x34\Original\bd34.dgn	USER NAME = geg1enobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 02-23-95
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 5/8" = 1' IN.	CHECKED -	REVISED - E. GOMEZ 08-28-00
	PLOT DATE = 12/17/2009	DATE - 09-22-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER STABILIZATION AT TBT TY 1 SPL.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
BD600-10 (BD 34)			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

FILE NAME =
...p\159689_Dist1.Det11.dgn

USER NAME = bweigend	DESIGNED - BJW	REVISED -
	DRAWN - DMS	REVISED -
PLOT SCALE = 5/8" = 1' IN.	CHECKED - JAS	REVISED -
PLOT DATE = 4/21/2009	DATE - 4/21/09	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER STABILIZATION AT TBT TY 1 SPL.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	9 RS-7	KANE	25	25
CONTRACT NO. 60H26			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	

SCALE: SHEET NO. OF SHEETS STA. TO STA.