

FOR INDEX OF SHEETS SEE SHEET NUMBER 2

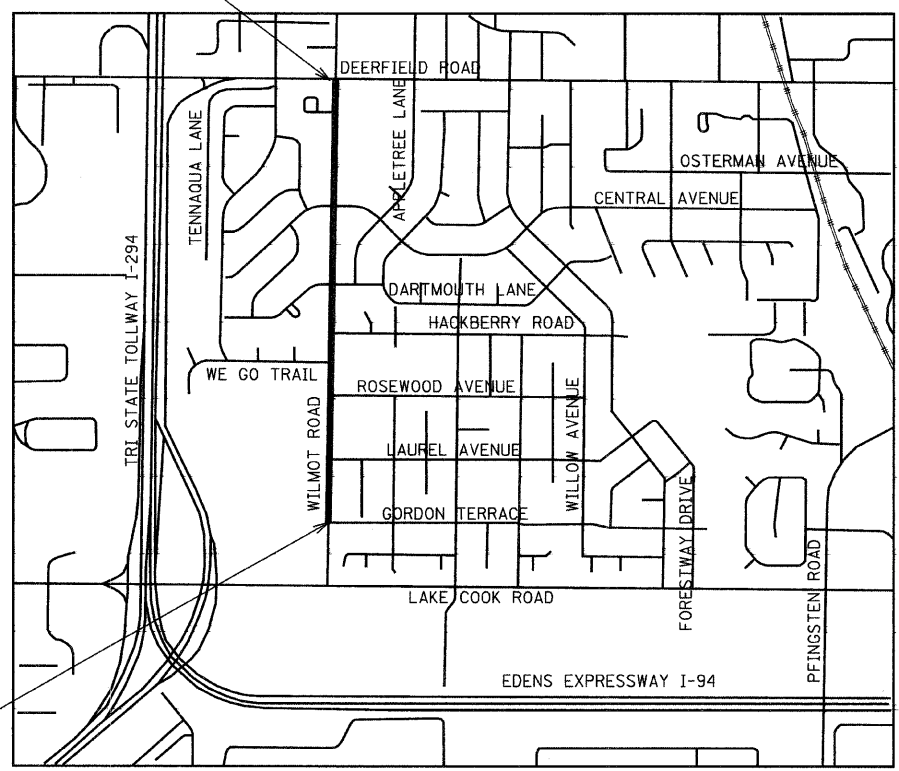
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PROPOSED
HIGHWAY PLANS**
RESURFACING AND WIDENING
VILLAGE OF DEERFIELD
F.A.U. ROUTE 2718 (WILMOT ROAD)
GORDON TERRACE TO F.A.U. ROUTE 1257 (DEERFIELD ROAD)
SECTION 07-00080-00-WR
PROJECT: ARA -8003(870)
LAKE COUNTY
C-91-031-08

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2718	07-00080-00-WR	LAKE	1	1
FED. ROAD DIST. NO.	ILLINOIS CONTRACT NO.		63124	
			55+2=57	

ASSOCIATE FIELD ENGINEER: KEVIN STALLWORTH (847) 705-4169
CONSULTANT: CIORBA GROUP, INC. (773) 775-4009



END IMPROVEMENT
STA. 95 + 69



BEGIN IMPROVEMENT
STA. 50 + 34

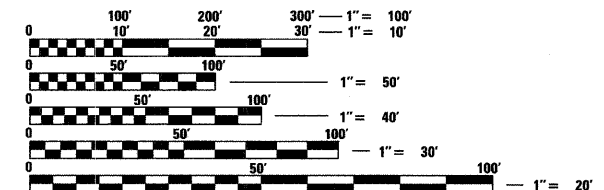


DESIGN DESIGNATION

WILMOT ROAD
FUNCTIONAL CLASSIFICATION: LOCAL COLLECTOR
DESIGN SPEED = 30 MPH
POSTED SPEED = 30 MPH

TRAFFIC DATA

EXISTING ADT - 7905 (2008)
DESIGN ADT - 7905



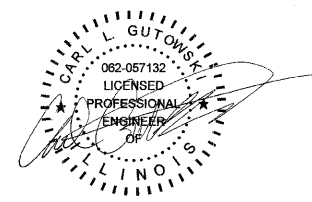
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CONTRACT NO. 63124

LOCATION MAP
1" = 1000'

GROSS AND NET LENGTH OF PROJECT = 4,535 FT = 0.86 MI



DATE: 1/30/2009
SEAL EXPIRES: 11/30/2009

APPROVED: *[Signature]*
VILLAGE OF DEERFIELD, VILLAGE ENGINEER

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PASSED: FEBRUARY 9, 2009
[Signature]
DISTRICT ENGINEER OF LOCAL ROAD & STREETS

RELEASING FOR BID
BASED ON LIMITED REVIEW: FEBRUARY 10, 2009
[Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

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OF THE STATE OF ILLINOIS**

Ciorba Group, Inc.
DESIGN FIRM
REGISTRATION NUMBER
184-001016
CONSULTING ENGINEERS
SUITE 402, 5507 NORTH CUMBERLAND AVE
CHICAGO, ILLINOIS 60656 :: (773) 775-4009

PLANS PREPARED BY: CIORBA GROUP

INDEX OF SHEETS

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STATE STANDARDS

- 000001-05 STANDARD SYMBS, ABBREVIATIONS AND PATTERNS
- 424001-04 CURB RAMPS FOR SIDEWALKS
- 442201-02 CLASS C AND D PATCHES
- 602001-01 CATCH BASIN TYPE A
- 602011-01 CATCH BASIN TYPE C
- 602301-02 INLET - TYPE A
- 602401-02 MANHOLE TYPE A
- 602406-02 MANHOLE TYPE A 6' DIAMETER
- 602601-01 PRECAST REINFORCED CONCRETE FLAT SLAB TOP
- 604001-03 FRAME AND LIDS TYPE 1
- 606001-03 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
- 701301-03 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701501-04 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
- 701502-03 URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
- 701701-06 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701801-04 SIDEWALK CLOSURE
- 701901-01 TRAFFIC CONTROL DEVICES
- 720001-01 SIGN PANEL MOUNTING DETAILS
- 720006-02 SIGN PANEL ERECTION DETAILS
- 720011-01 METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
- 780001-02 TYPICAL PAVEMENT MARKINGS
- 814001-02 HANDHOLES
- 862001-01 UNINTERRUPTIBLE POWER SUPPLY (UPS)
- 873001-02 TRAFFIC SIGNAL GROUNDING & BONDING
- 876001-01 PEDESTRIAN PUSH BUTTON POST
- 877001-04 STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
- 878001-07 CONCRETE FOUNDATION DETAILS
- 880006-01 TRAFFIC SIGNAL MOUNTING DETAILS

LEGEND (IN ADDITION TO STATE STD. 000001-05)

EXISTING	PROPOSED	DESCRIPTION
○	⊙	VALVE VAULT
○	●	VALVE BOX
□	—	INLET
○	●	CATCH BASIN
○	⊙	MANHOLE
○	●	B-BOX

664.54 +85
 664.18 16.5' LT
 TOC ELEV STATION
 EOP ELEV OFFSET

GENERAL NOTES

1. ALL UNDERGROUND UTILITY LOCATIONS, INCLUDING SANITARY SEWERS, STORM SEWERS, WATER MAINS, AND THEIR SERVICE LINES, SHOWN ON THE PLANS ARE APPROXIMATE ONLY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO HAVE THE RESPECTIVE UTILITY COMPANIES FIELD-LOCATE ALL UTILITIES, ASCERTAIN THEIR STATUS AND ADJUST OR RELOCATE THESE UTILITIES, AS NECESSARY, PRIOR TO STARTING CONSTRUCTION. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL NOTIFY ALL PUBLIC AND PRIVATE UTILITIES BEFORE STARTING CONSTRUCTION, INCLUDING, BUT NOT LIMITED TO:
 - A. PEOPLES ENERGY
3001 GRAND AVENUE
WAUKEGAN, IL 60085
CONTACT PERSON: MR. TED SILARSKI
CONTACT PHONE NUMBER:
 - B. COMMONWEALTH EDISON COMPANY
1500 FRANKLIN BOULEVARD
LIBERTYVILLE, IL 60048
CONTACT PERSON: MR. TOM STUTZMAN
CONTACT PHONE NUMBER: (630) 437-2236
 - C. MCI
2400 NORTH GLENVILLE
RICHARDSON, TX 75082
CONTACT PERSON: MR. DEAN BOYERS
CONTACT PHONE NUMBER: (972) 729-6322
 - D. AT&T
866 ROCK CREEK ROAD
PLANO, IL 60545
CONTACT PERSON: MR. CARL DONAHUE
CONTACT PHONE NUMBER: (630) 552-4677
 - E. COMCAST CABLE
688 INDUSTRIAL DRIVE
ELMHURST, IL 60126
CONTACT PERSON: MS. MARY STEFAN
CONTACT PHONE NUMBER: (630) 600-6346
 - F. VILLAGE OF DEERFIELD
465 ELM STREET
DEERFIELD, IL 60015
CONTACT PERSON: MS. BARBARA LITTLE
CONTACT PHONE NUMBER: (847) 317-2490
 - G. ADESTA, LLC
1428 SHERMAN ROAD
ROMEOVILLE, IL 60446
CONTACT PERSON: MR. LOU URIDIL
CONTACT PHONE NUMBER: (630) 739-0546
 - H. LAKE COUNTY PUBLIC WORKS
650 WINCHESTER ROAD
LIBERTYVILLE, IL 60048
CONTACT PERSON: MR. DENNIS PRICE
CONTACT PHONE NUMBER: (847) 377-7500
 - I. ROGERS TELECOM INC
4804 NORTH 40TH STREET
SHEBOYGAN, WI 53083
CONTACT PERSON: MR. MATTHEW GABRIELSE
CONTACT PHONE NUMBER: (920) 459-2600
 - J. SBC
1200 NORTH ARLINGTON HEIGHTS
ARLINGTON HEIGHTS, IL 60004
CONTACT PERSON: MS. MANESSA CHAMBERLIN
CONTACT PHONE NUMBER: (847) 481-6900
2. PROCUREMENT OF ALL NECESSARY PERMITS, AND PAYMENT THEREOF, SHALL BE THE CONTRACTOR'S RESPONSIBILITY.
3. DURING THE CONSTRUCTION OPERATION WHEN ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS, OR DRAINAGE STRUCTURES SO THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS ALL DRAINAGE STRUCTURES SHALL BE FREE FROM ALL DIRT AND DEBRIS. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THIS PROJECT. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR FAILURE TO VERIFY EXISTING DIMENSIONS OR CONDITIONS.
5. THE CONTRACTOR SHALL LIMIT HIS CONSTRUCTION ACTIVITIES TO THE WORK AREAS DESIGNATED ON THE PLANS. ANY DAMAGE TO AREAS OUTSIDE OF THESE LIMITS SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER.
6. THE PROPOSED COMBINATION CONCRETE CURB AND GUTTER SHALL BE DEPRESSED ACROSS ALL DRIVEWAYS, HANDICAPPED RAMPS AND/OR AS DIRECTED BY THE ENGINEER. HANDICAPPED RAMPS SHALL BE PROVIDED AT ALL CROSS WALK LOCATIONS, AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH THE IDOT STANDARD SPECIFICATIONS.
7. PROPER DRAINAGE SHALL BE MAINTAINED IN THE IMPROVEMENT AREA DURING CONSTRUCTION. THE COST OF THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. FRAMES ON ALL NEW STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE STRUCTURE COST AND LOCKED IN CONCRETE IN THE ROADWAY.
9. THE COST OF THE REMOVAL OF ANY EXISTING POLES, POSTS, TREE STUMPS OR OTHER OBSTRUCTIONS WHICH INTERFERE WITH THE PROPOSED IMPROVEMENTS AND WHICH ARE NOT TO BE REMOVED AS A SEPARATE PAY ITEM SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

10. THE CONTRACTOR AT HIS OWN EXPENSE SHALL BE REQUIRED TO RELOCATE ALL ROAD SIGNS WHICH INTERFERE WITH HIS CONSTRUCTION OPERATIONS AND TO TEMPORARILY RESET SUCH SIGNS DURING HIS CONSTRUCTION OPERATIONS. ALL WORK INVOLVING SIGNS SHALL BE COVERED BY THE FOLLOWING REQUIREMENTS:
 - A. SIGNS SHALL NOT BE REMOVED UNTIL PROGRESS OF WORK NECESSITATES REMOVAL.
 - B. EVERY SIGN REMOVED MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER VISIBLE TO TRAFFIC ON THE HIGHWAY. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND NEAT APPEARING FOR THE DURATION OF THE TEMPORARY SETTING.
 - C. ALL SIGNS MUST BE RE-ERECTED IN THEIR PERMANENT LOCATIONS AS DESIGNATED BY THE ENGINEER AS THE ROADWAY IS COMPLETED.
 - D. ALL SIGNS NOT REQUIRED FOR REUSE AFTER CONSTRUCTION IS COMPLETED SHALL REMAIN THE PROPERTY OF THE VILLAGE. THE CONTRACTOR SHALL BE REQUIRED TO STORE THEM AT THE JOB SITE FOR PICKUP BY THE VILLAGE.
 - E. ANY SIGN OR SIGN POST DAMAGED BY THE CONTRACTOR SHALL BE REPLACED AT HIS OWN EXPENSE. THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A LIST OF ALL EXISTING DAMAGED SIGNS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.
 - F. THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER AN INVENTORY OF ALL SIGNS MOVED DURING CONSTRUCTION.
11. THE CONTRACTOR SHALL PLACE WARNING SIGNS PRIOR TO PLACEMENT OF PRIME COAT AND COMMENCEMENT OF PAVING OPERATIONS OR AS DIRECTED BY THE ENGINEER. THIS WORK SHALL NOT BE A SEPARATE PAY ITEM, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
12. OFFSETS TO STRUCTURES ARE FROM THE CENTERLINE TO THE EDGE OF PAVEMENT. OFFSETS TO STRUCTURES NOT LOCATED ALONG THE EDGE OF PAVEMENT ARE FROM THE SURVEY BASELINE TO CENTER OF STRUCTURE.
13. THE CONTRACTOR SHALL NOT REMOVE THE EXISTING SURFACE COURSE FOR BUTT JOINTS MORE THAN 24 HOURS PRIOR TO PLACING THE NEW SURFACE COURSE.
14. THE CONTRACTOR SHALL OBTAIN AUTHORIZATION FROM THE ENGINEER BEFORE BEGINNING WORK ON ANY STREET.
15. WHEN PAVING IN THE VICINITY OF SCHOOLS OR PUBLIC BUILDINGS, THE CONTRACTOR SHALL SCHEDULE THE APPLICATION OF PRIME COAT TO BE DONE NOT LESS THAN 30 MINUTES NOR MORE THAN TWO HOURS BEFORE THE PLACEMENT OF THE BITUMINOUS CONCRETE MAT.
16. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL PAVEMENT OPENINGS, OPEN HOLES, EQUIPMENT AND RUBBLE LEFT IN THE PUBLIC RIGHT-OF-WAY. THE CONTRACTOR SHALL MAINTAIN HIGH VISIBILITY OF ALL TEMPORARY HAZARDS TO PEDESTRIANS AND MOTORISTS.
17. THE THICKNESS OF THE HMA COURSES SHOWN ON STREETS TO BE RESURFACED IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE ON WHICH THE BITUMINOUS COURSE IS PLACED.
18. ALL DRIVEWAYS, CARRIAGE WALKS, AND SERVICE WALKS ADJACENT TO THE NEW CURB AND GUTTER AND MARKED FOR REMOVAL WILL BE RECONSTRUCTED TO PROVIDE GENTLE SLOPES AND PROFILES AS DIRECTED BY THE ENGINEER.
19. ALL PARKWAYS DISTURBED BY CONSTRUCTION OPERATIONS SHALL BE PROPERLY GRADED AND RECEIVE FOUR INCHES OF TOPSOIL AND SODDING (SEE SOD SPECIFICATIONS).
20. THE CONTRACTOR MAY BE REQUIRED TO PUT A GRADE BREAK IN THE SCREED AND ADJUST THE CONTROLS OF HIS PAVING MACHINE TO ALLOW THE REQUIRED THICKNESS OF ASPHALT OVER THE FULL PAVEMENT CROSS SECTION.
21. THE CONTRACTOR SHALL BE REQUIRED TO CLEAN EACH SUCCESSIVE NEW HMA COURSE PRIOR TO PLACEMENT OF THE NEXT HMA COURSE AS DIRECTED BY THE ENGINEER. THE COST OF THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
22. ALL CURB AND GUTTER REMOVAL AND REPLACEMENT SHALL BE COMPLETED PRIOR TO PLACING ANY BITUMINOUS MATERIAL ON THAT STREET.
23. THE WORD "WATER", "SANITARY", OR "STORM" SHALL BE CAST INTO THE LID OF EACH RESPECTIVE VALVE VAULT OR MANHOLE REQUIRING A NEW FRAME AND LID.
24. FRAMES, LIDS AND GRATES OF EXISTING CATCH BASINS, INLETS, MANHOLES AND VALVE VAULTS WHICH ARE TO BE ABANDONED OR REPLACED IN THIS PROJECT SHALL BE SALVAGED AND REMAIN THE PROPERTY OF THE VILLAGE OF DEERFIELD. THE CONTRACTOR SHALL DELIVER THESE CASTINGS TO A FACILITY DESIGNATED BY THE VILLAGE.
25. AS DIRECTED BY THE ENGINEER, SIDEWALK RAMPS SHALL BE PLACED AT EACH INTERSECTION WHERE THE CURB AND GUTTER IS BEING REPAIRED. NO ADDITIONAL COMPENSATION WILL BE ALLOWED DUE TO NECESSARY MODIFICATIONS IN REGULAR SIDEWALK AND CURB AND GUTTER ITEMS.
26. PLACEMENT OF TOPSOIL SHALL BE COMPLETED WITHIN 10-15 DAYS AFTER THE COMPLETION OF CURB AND GUTTER, PAVING AND/OR DRIVEWAY REPLACEMENT OPERATIONS, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
27. GENERAL LOCATIONS OF MOST CURB AND GUTTER, DRIVEWAY AND SIDEWALK REMOVAL AND REPLACEMENT FOR THE STREETS TO BE REHABILITATED OR REPAIRED HAVE BEEN SHOWN ON THE PLANS. HOWEVER, ADDITIONAL AREAS REQUIRING REMOVAL AND REPLACEMENT MAY BE DETERMINED BY THE ENGINEER IN THE FIELD. THE CONTRACT QUANTITIES HAVE BEEN ADJUSTED TO INCLUDE ALLOWANCES FOR WORK DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION.

28. ON THE STREETS TO BE REHABILITATED OR REPAIRED, ANY WORK WITHIN THE CURB AND GUTTER RETURN SHALL MATCH THE EXISTING RETURN RADIUS UNLESS OTHERWISE NOTED ON THE PLANS.
29. THE CONTRACTOR SHALL BE REQUIRED TO MOVE ANY DECORATIVE ROCKS OR PAVEMENT BRICKS THAT INTERFERE WITH CONSTRUCTION TO A DESIGNATED LOCATION INDICATED BY THE ENGINEER. (THIS WORK WILL BE INCIDENTAL TO THE CONTRACT.)
30. ALL DIMENSIONS SHOWN ARE FROM THE BASELINE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE SHOWN. ALL RADI SHOWN ARE FROM THE EDGE OF PAVEMENT.
31. EXPENSE INVOLVED IN CONNECTING PROPOSED SEWERS TO EXISTING SEWERS OR PROPOSED SEWERS TO EXISTING SEWER STRUCTURES OR PROPOSED SEWER STRUCTURES TO EXISTING SEWERS WITH A CONCRETE COLLAR OR AS SPECIFIED BY THE ENGINEER SHALL BE CONSIDERED INCIDENTAL TO THE ITEM BEING CONNECTED.
32. FRAME ELEVATIONS ARE GIVEN ONLY TO ASSIST IN DETERMINING THE APPROXIMATE OVERALL HEIGHT AT THE STRUCTURE. FRAMES ON ALL NEW STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE STRUCTURE COST.
33. ALL PIPE SLOPES ARE FROM INSIDE WALL OF STRUCTURE TO INSIDE WALL OF STRUCTURE. THE PIPE WILL NOT BE PAID FOR THROUGH STRUCTURES
34. ALL FRAMES AND GRATES DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION WILL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
35. WHERE REQUIRED BY RESTRICTIVE DEPTHS, PRECAST REINFORCED CONCRETE FLAT SLAB TOPS SHALL BE USED FOR MANHOLES AND CATCH BASINS, WITH THE USE OF TONGUE AND GROOVE WHERE DEPTHS ALLOW.
36. HORIZONTAL AND/OR VERTICAL SEPARATION BETWEEN STORM SEWERS AND WATER MAINS SHALL BE IN CONFORMANCE WITH SECTION 41 OF THE STANDARD SPECIFICATIONS FOR WATER & SEWER MAIN CONSTRUCTION IN ILLINOIS.
37. VALVES ON EXISTING WATER MAIN LINES SHALL BE OPERATED BY VILLAGE PERSONNEL ONLY. THE ENGINEER AND THE VILLAGE WATER DEPARTMENT SHALL BE NOTIFIED IN ADVANCE WHEN WATER MAIN ADJUSTMENTS WILL BE MADE SO THAT LOCAL RESIDENTS MAY BE NOTIFIED. THE VILLAGE WATER DEPARTMENT SHALL BE PRESENT FOR INSPECTION OF WATER MAIN ADJUSTMENTS
38. EXISTING SEWERS OR WATER MAINS TO BE ABANDONED THAT INTERFERE WITH THE PROPOSED IMPROVEMENTS SHALL BE REMOVED, BACKFILLED AND DISPOSED OF OFF SITE AS INCIDENTAL TO THE CONTRACT.
39. ONLY HDPE RECYCLED PLASTIC ADJUSTMENT RINGS WILL BE ALLOWED FOR THE ADJUSTMENTS OF CATCH BASINS, MANHOLES, VALVE VAULTS, AND INLETS. NO MORE THAN 2 RINGS FOR TOTAL ADJUSTMENT OF 8 INCHES WILL BE ALLOWED
40. THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION. IN PARTICULAR, THE CONTRACTOR WILL TAKE ADEQUATE MEASURES TO PREVENT THE UNDERMINING OF UTILITIES AND SEWERS WHICH ARE STILL IN SERVICE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROTECT EXCAVATION TRENCHES DURING THE INSTALLATION OF STORM SEWER, WATER MAIN, AND SANITARY SEWER TO INCLUDE ANY SHORING OR DEWATERING EQUIPMENT NECESSARY. THE COST OF THIS WORK SHALL BE CONSIDERED INCLUDED IN THE CONTRACT.
41. REQUIREMENTS FOR THE PROTECTION OF WATERMAIN AND WATER SERVICE LINES FROM SANITARY SEWERS, STORM SEWERS, AND HOUSE SEWERS SHALL COMPLY WITH IEPA SEPARATION REQUIREMENTS.
42. ALL TRENCHES FOR WATER MAIN SHALL BE CONSTRUCTED IN ACCORDANCE WITH DIVISION II SECTION 20-2.20B OF THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS, ESPECIALLY CONCERNING BACKFILLING AS SOON AS PRACTICALLY POSSIBLE FOR PROTECTION.
43. ALL UTILITIES FOR THIS CONTRACT SHALL BE INSTALLED PER THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS, CURRENT EDITION.
44. BACKFILLING OF STRUCTURES SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF ARTICLE 550.07 OF THE STANDARD SPECIFICATIONS AND AS HEREIN SPECIFIED. STRUCTURES UNDER PAVEMENT SHALL BE BACKFILLED WITH COURSE AGGREGATE CA-7 (VIRGIN CRUSHED LIMESTONE). STRUCTURES IN PARKWAY SHALL BE BACKFILLED WITH FINE AGGREGATE FA-6 (CLEAN BROWN BEACH SAND).
45. THE CONTRACTOR SHALL ASSUME FULL MAINTENANCE OF ALL EXISTING VILLAGE OF DEERFIELD OWNED UTILITIES AS SOON AS THE CONTRACTOR BEGINS ANY PHYSICAL WORK ON THE CONTRACT OR ANY PORTION THEREOF. MAINTENANCE SHALL INCLUDE ANY PART WITHIN THE RIGHT-OF-WAY THAT FALLS WITHIN THE PROJECT LIMITS AND UTILITIES OUTSIDE OF THE PROJECT LIMITS UP TO THE NEAREST STRUCTURE. THE CONTRACTOR SHALL PROVIDE IMMEDIATE CORRECTIVE ACTION WHEN ANY PART OF THE SYSTEM FAILS TO FUNCTION PROPERLY. THE CONTRACTOR SHALL PROVIDE THE DIRECTOR OF PUBLIC WORKS WITH A 24 HOUR TELEPHONE NUMBER FOR EMERGENCY CALLS. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT WITH NO EXTRA COMPENSATION FOR THE WORK DESCRIBED HEREIN TO BE CHARGED TO THE CONTRACT.
46. MAILBOX REMOVAL AND REPLACEMENT TO BE DONE IN ACCORDANCE WITH ARTICLE 107.20 OF THE STANDARD SPECIFICATIONS AND SHALL BE INCIDENTAL TO THE CONTRACT.

FILE NAME = N:\PROJ\3328\Phase1\Design\MiscSheets\3328-GEN-NOTES.dgn	USER NAME = cgotovaks	DESIGNED - EPS	REVISED -	VILLAGE OF DEERFIELD WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)		INDEX OF SHEETS, STATE STANDARDS, & GENERAL NOTES		F.A.U. RTE. = 2718	SECTION = 07-00080-00-WR	COUNTY = LAKE	TOTAL SHEETS = 57	SHEET NO. = 2
	PLOT SCALE = 2.0004' / IN.	DRAWN - AL	REVISED -					SCALE: 1" = 40'	SHEET NO. 2 OF 57 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	CONTRACT NO. 63124

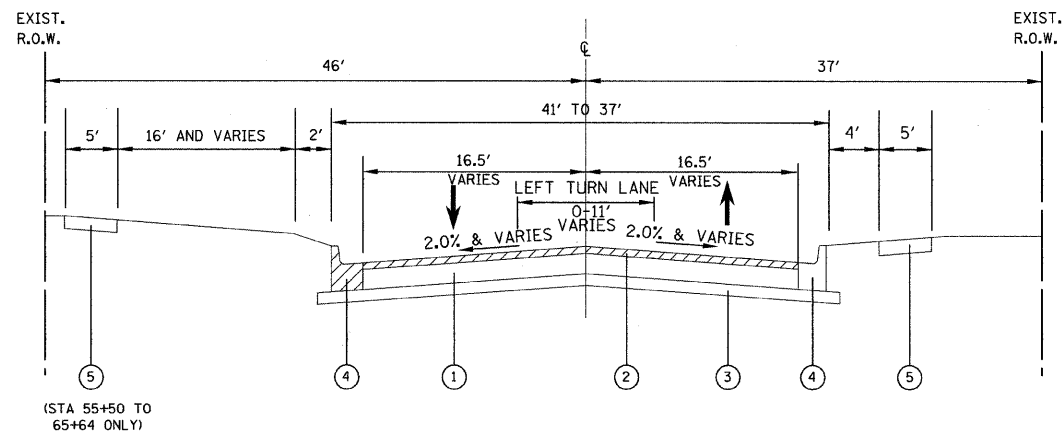
CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	1000-2A	Y031-IF	100% DEERFIELD
20101000	TEMPORARY FENCE	FOOT	1120	1120		
20101200	TREE ROOT PRUNING (SPECIAL)	EACH	15	15		
20101300	TREE PRUNING 1-10	EACH	7	7		
20101350	TREE PRUNING OVER 10	EACH	5	5		
20101400	NITROGEN FERTILIZER NUTRIENT	POUND	14	14		
20101500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	14	14		
20101600	POTASSIUM FERTILIZER NUTRIENT	POUND	14	14		
20200100	EARTH EXCAVATION	CU YD	1400	1400		
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	130	130		
20700420	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	130	130		
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	390	390		
21101615	TOPSOIL FURISH AND PLACE, 4"	SQ YD	1080	1080		
25200110	SODDING, SALT TOLERANT	SQ YD	1080	1080		
25200200	SUPPLEMENTAL WATERING	UNIT	6	6		
28000510	INLET FILTERS	EACH	55	55		
31101200	SUB-BASE GRANULAR MATERIAL, TYPE B, 4"	SQ YD	5320	5197		123
35400500	PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 10"	SQ YD	300	300		
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	1910	1910		
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	3180	3180		
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GAL	3910	3910		
40600300	AGGREGATE (PRIME COAT)	TON	80	80		
40600825	POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50	TON	1100	1100		
40600895	CONSTRUCTING TEST STRIP	EACH	2	2		
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	245	245		
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1750	1750		
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5"	SQ FT	8000	7220		780
42400300	PORTLAND CEMENT CONCRETE SIDEWALK 6"	SQ FT	130	130		
42400800	DETECTABLE WARNINGS	SQ FT	500	500		
44000100	PAVEMENT REMOVAL	SQ YD	140	140		
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	17300	17300		
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	825	825		
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	4190	4075		115
44000600	SIDEWALK REMOVAL	SQ FT	8130	6295		1835
60603800	COMBINATION CURB AND GUTTER TYPE B-6.12	FOOT	5310	5194		116
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6		
67100100	MOBILIZATION	L SUM	1	1		
X7011005	TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR	L SUM	1	1		
70101700	TRAFFIC CONTROL AND PROTECTION	L SUM	1	1		
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	12	12		
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	4370	4370		
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	9500	9500		
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	30	30		
72000100	SIGN PANEL - TYPE 1	SQ FT	104	104		
72400710	RELOCATE SIGN PANEL - TYPE 1	SQ FT	18	18		
72900200	METAL POST - TYPE B	FOOT	236	236		
78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS	SQ FT	801	801		

* SPECIALTY ITEM

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	1000-2A	Y031-IF	100% DEERFIELD
78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	6360	6360		
78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3430	3430		
78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	205	205		
78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	255	255		
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	10	10		
78300100	PAVEMENT MARKING REMOVAL	SQ FT	60	60		
X8050015	SERVICE INSTALLATION - POLE MOUNTED	EACH	1	1		
81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	25	25		
81000700	CONDUIT TRENCHED, 2 1/2" DIA., GALVANIZED STEEL	FOOT	80	80		
81001000	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	14	14		
81018900	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	35	35		
81400100	HANDHOLE	EACH	2	2		
81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	102	102		
84500120	REMOVAL OF ELECTRIC SERVICE INSTALLATION	EACH	1	1		
85700100	FULL-ACTUATED CONTROLLER AND TYPE III CABINET	EACH	1	1		
86200200	UNINTERRUPTIBLE POWER SUPPLY, STANDARD	EACH	1	1		
87301215	ELECTRIC CABLE IN CONDUIT SIGNAL 14 2C	FOOT	140	140		
87301225	ELECTRIC CABLE IN CONDUIT SIGNAL 14 3C	FOOT	160	160		
87301245	ELECTRIC CABLE IN CONDUIT SIGNAL 14 5C	FOOT	480	480		
87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO.6 2C	FOOT	30	30		
X8730027	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO.6 1C	FOOT	170	170		
87502460	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 12 FT	EACH	2	2		
87700160	STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.	EACH	2	2		
87800100	CONCRETE FOUNDATION, TYPE A	FOOT	8	8		
87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4	4		
87800400	CONCRETE FOUNDATION, TYPE E, 30" DIAMETER	FOOT	30	30		
88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4	4		
88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2	2		
88200110	TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	4	4		
88800100	PEDESTRIAN PUSH BUTTON	EACH	2	2		
89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1	1		
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1	1		
89502380	REMOVE EXISTING HANDHOLE	EACH	2	2		
89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	5	5		
XX003338	TEST HOLE	EACH	10	5		5
XX007490	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 8"	SQ YD	265	265		
XX004904	BITUMINOUS DRIVEWAY PAVEMENT 10"	SQ YD	430	430		
20013798	CONSTRUCTION LAYOUT	L SUM	1	1		
X0301766	DRILL AND GROUT #6 TIE BARS	EACH	633	633		
XX002258	STRUCTURES TO BE ADJUSTED	EACH	20	20		
Z0076600	TRAINERS	HOUR	500	500		
XX004689	SANITARY SERVICE TO BE ADJUSTED	EACH	25			25
20800150	TRENCH BACKFILL	CU YD	4048	320		3728
X0712400	TEMPORARY PAVEMENT	SQ YD	380			380
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	110	110		
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	230	230		

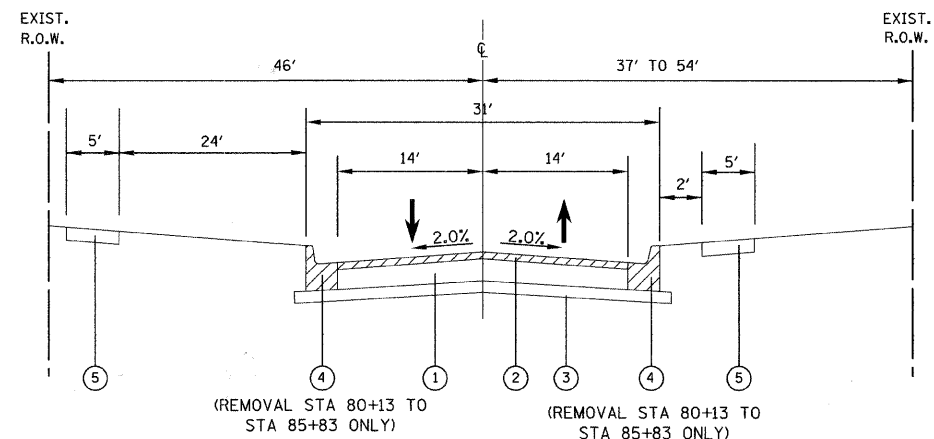
CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	1000-2A	Y031-IF	100% DEERFIELD
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	620	620		
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	3530	3530		
55019500	STORM SEWERS, TYPE 1, REINFORCED CONCRETE CULVERT, STORM DRAIN, AND SEWER PIPE, CLASS IV 12"	FOOT	70	70		
55021600	STORM SEWERS, TYPE 2, REINFORCED CONCRETE CULVERT, STORM DRAIN, AND SEWER PIPE, CLASS III 12"	FOOT	120	120		
55021800	STORM SEWERS, TYPE 2, REINFORCED CONCRETE CULVERT, STORM DRAIN, AND SEWER PIPE, CLASS III 18"	FOOT	70	70		
55100500	STORM SEWER REMOVAL, 12"	FOOT	645	645		
55100900	STORM SEWER REMOVAL, 18"	FOOT	85	85		
55101200	STORM SEWER REMOVAL, 24"	FOOT	75	75		
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER TYPE 1 FRAME, OPEN LID	EACH	24	24		
60218300	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	1		
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	6	6		
60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2	2		
60223800	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	8	8		
60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	17	17		
60500040	REMOVING MANHOLES	EACH	11	11		
60500050	REMOVING CATCH BASINS	EACH	27	27		
60500060	REMOVING INLETS	EACH	24	24		
60500070	REMOVING MANHOLES TO MAINTAIN FLOW	EACH	1	1		
X0321905	STORM SEWERS, TYPE 1, WATER MAIN QUALITY PIPE, 12"	FOOT	320	320		
X0321907	STORM SEWERS, TYPE 2, WATER MAIN QUALITY PIPE, 12"	FOOT	225	225		
X0321909	STORM SEWERS, TYPE 2, WATER MAIN QUALITY PIPE, 24"	FOOT	75	75		
60206905	CATCH BASINS, TYPE C, 2'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	12	12		
X0321914	STORM SEWERS, TYPE 2, WATER MAIN QUALITY PIPE, 18"	FOOT	35	35		
Z0041700	PLUG EXISTING STORM SEWERS	EACH	2	2		
XX002136	PRESSURE CONNECTION	EACH	1			1
56103000	DUCTILE IRON WATER MAIN, 6"	FOOT	165			165
56103100	DUCTILE IRON WATER MAIN, 8"	FOOT	560			560
56103200	DUCTILE IRON WATER MAIN, 10"	FOOT	4460			4460
56104900	WATER VALVES, 6"	EACH	3			3
56105000	WATER VALVES, 8"	EACH	9			9
56105100	WATER VALVES, 10"	EACH	8			8
56400500	FIRE HYDRANTS TO BE REMOVED	EACH	12			12
60249120	VALVE VAULTS, 5'-DIAMETER	EACH	18			18
60249400	VALVE BOXES, 6"	EACH	2			2
60500370	FILLING VALVE BOXES	EACH	1			1
60500405	FILLING VALVE VAULTS	EACH	12			12
60500505	FILLING METER VAULTS	EACH	1			1
X0300020	WATER SERVICE CONNECTION	EACH	38			38
X0325012	CONNECTIONS TO EXISTING WATER MAINS (NON-PRESSURE) - 10"	EACH	1			1
X5640150	FIRE HYDRANT ASSEMBLY COMPLETE	EACH	12			12
XX003516	CONNECTIONS TO EXISTING WATER MAINS (NON-PRESSURE) - 8"	EACH	9			9
XX003517	CONNECTIONS TO EXISTING WATER MAINS (NON-PRESSURE) - 6"	EACH	1			1
XX005107	CONNECTIONS TO EXISTING WATER MAINS (NON-PRESSURE) - 12"	EACH	1			1
X0321914	Cut and Cap Existing Water Main	EACH	9			9
XX007871	STORM SEWER SERVICE, 6" (SHORT) (TYP.)	EACH	25	25		
XX007874	STORM SEWER SERVICE, 6" (LONG) (TYP.)	EACH	12	12		

FILE NAME = N:\PROJ\3328\PhaseII\Design\MiscSheets\3328-500.dgn	USER NAME = cgurowaki	DESIGNED - EPS	REVISED -	VILLAGE OF DEERFIELD WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)	SUMMARY OF QUANTITIES			F.A.U. RTE. 2718	SECTION 07-00080-00-WR	COUNTY LAKE	TOTAL SHEETS 55	SHEET NO. 3
	PLOT SCALE = 2.0004' / IN.	DRAWN - AL	REVISED -		SCALE:	SHEET NO. 3 OF 55 SHEETS	STA. TO STA.	CONTRACT NO. 63124				
	PLOT DATE = 5/13/2009	CHECKED - CLG	REVISED -									
		DATE - 4-7-2009	REVISED -									



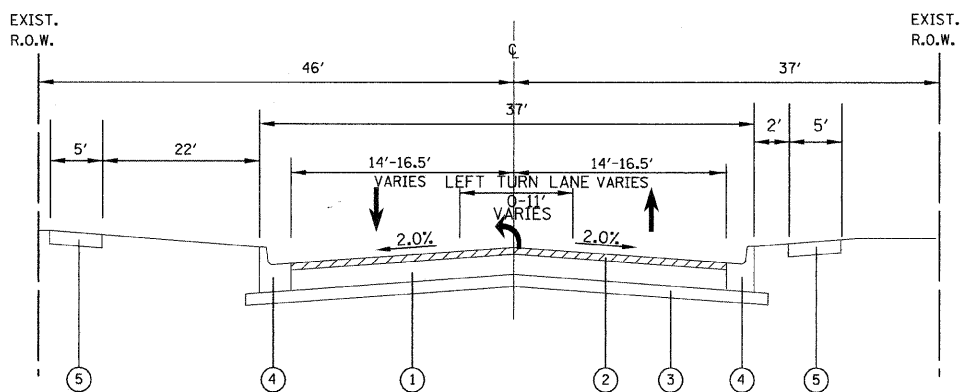
EXISTING TYPICAL SECTION

STA 50+34 - STA 65+64



EXISTING TYPICAL SECTION

STA 65+64 - STA 89+32



EXISTING TYPICAL SECTION

STA 89+32 - STA 95+69

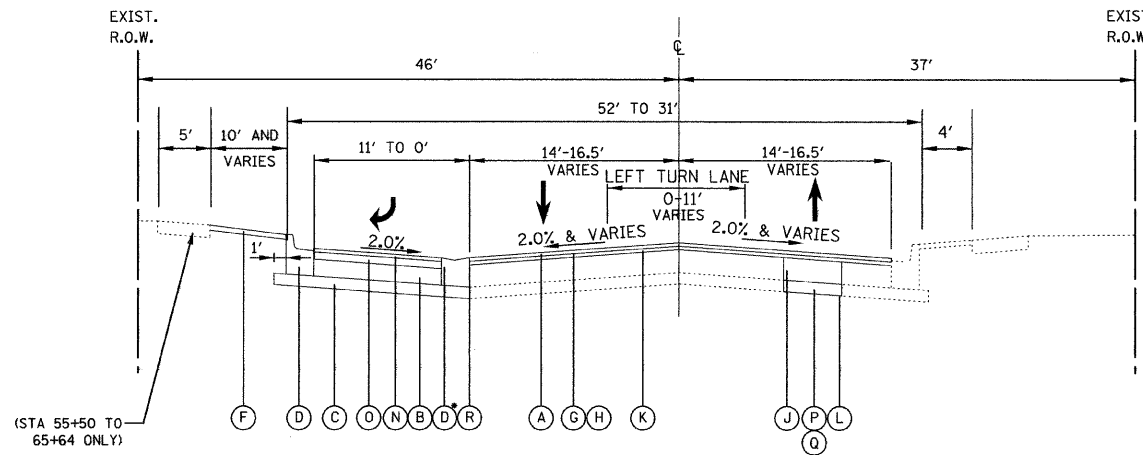
EXISTING LEGEND

- ① HOT-MIX ASPHALT PAVEMENT, 12"
- ② HOT MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ③ AGGREGATE SUB-BASE (VARIES 7")
- ④ COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12 (REMOVAL AS DIRECTED BY ENGINEER)
- ⑤ PCC SIDEWALK (REMOVAL AS DIRECTED BY THE ENGINEER)

REMOVAL

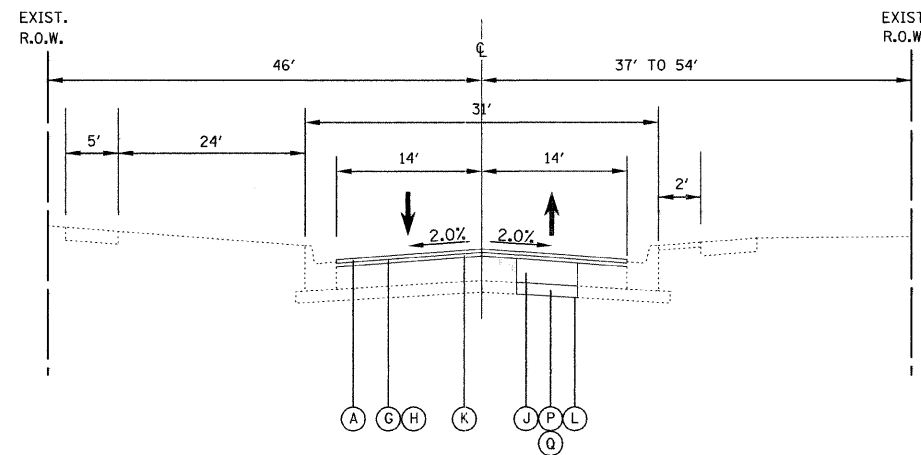
FILE NAME =	USER NAME = cgotowski	DESIGNED - EPS	REVISED -	VILLAGE OF DEERFIELD WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)	EXISTING TYPICAL SECTIONS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 1/8" = 1'-0"	DRAWN - AL	REVISED -			2718	07-00080-00-WR	LAKE	55	4
	PLOT DATE = 1/30/2009	CHECKED - CLG	REVISED -			CONTRACT NO. 63124				
		DATE - 1-30-2009	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

SCALE: N/A SHEET NO. 4 OF 55 SHEETS STA. TO STA.



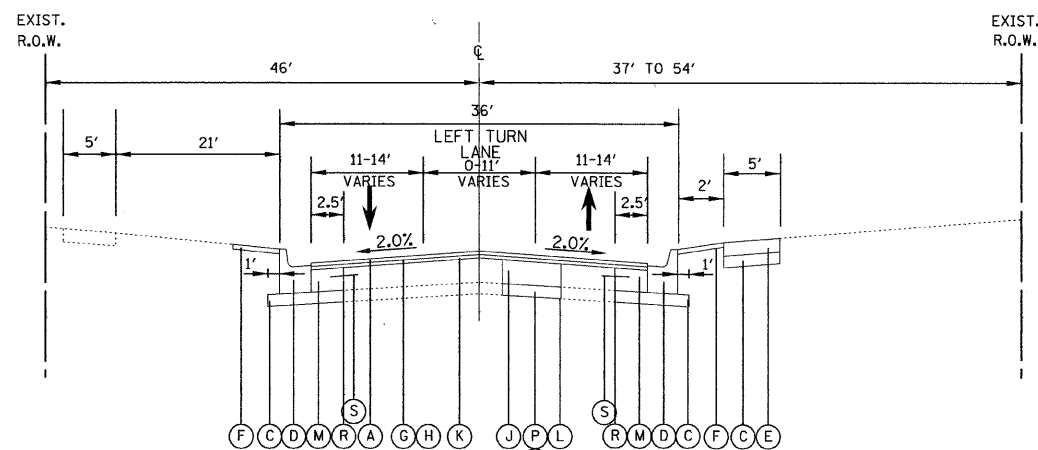
PROPOSED TYPICAL SECTION

STA 50+34 - STA 66+00
• (DEPRESSED)



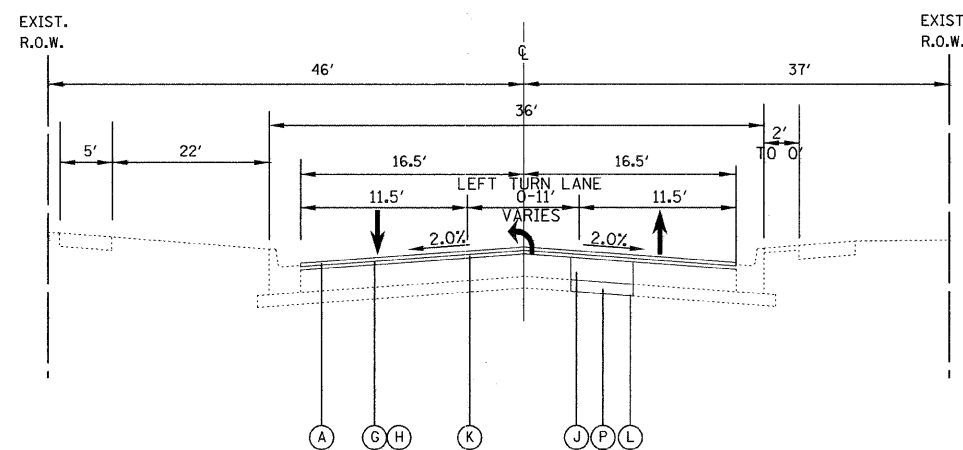
PROPOSED TYPICAL SECTION

STA 66+00 - STA 79+92
STA 86+03 - STA 89+32



PROPOSED TYPICAL SECTION

STA 79+92 - STA 86+03



PROPOSED TYPICAL SECTION

STA 89+32 - STA 95+69

PROPOSED LEGEND

- (A) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
- (B) HOT-MIX ASPHALT BASE COURSE, 8" (IN 3 LIFTS)
- (C) SUB-BASE GRANULAR MATERIAL, TYPE B, 4"
- (D) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- (E) PCC SIDEWALK, 5" (6" AT DRIVEWAYS)
- (F) SODDING SPECIAL, 4"
- (G) BITUMINOUS MATERIALS (PRIME COAT)
- (H) AGGREGATE (PRIME COAT)
- (J) CLASS D PATCHES, 10" (IN 4 LIFTS)
- (K) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- (L) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION (AS DIRECTED BY ENGINEER)
- (M) PORTLAND CEMENT CONCRETE BASE COURSE WIDENING, 10"
- (N) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
- (O) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/4"
- (P) PORUS GRANULAR EMBANKMENT (AS DIRECTED BY THE ENGINEER)
- (Q) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (AS DIRECTED BY ENGINEER)
- (R) FULL DEPTH SAWCUT (INCIDENTAL)
- (S) EPOXY COATED NO. 6 TIE BARS @ 24" C-C

HOT MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AC TYPE	AIR VOIDS
ROADWAY		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5mm, MIX "D", N50	PG 64 -22	4% @ 50 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	SBS/SBR PG 76-28/-22	4% @ 50 GYR.
LEVELING BINDER (HAND METHOD), N50	PG 64 -22*	4% @ 50 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	PG 64 -22*	4% @ 50 GYR.
HOT-MIX ASPHALT BASE COURSE, 8" (HMA BINDER IL-19.0)	PG 64 -22*	4% @ 50 GYR.
TEMPORARY PAVEMENT		
TEMP PVMT HOT-MIX ASPHALT SURFACE COURSE, IL-9.5mm, MIX "D", N50, 1 1/2"	PG 64 -22	4% @ 50 GYR.
TEMP PVMT HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 1 1/2"	PG 64 -22*	4% @ 50 GYR.
PATCHING		
CLASS D PATCHES, 10" TYPE I TO IV (HMA BINDER IL-19mm)	PG 64 -22*	4% @ 70 GYR.
DRIVEWAYS		
HMA SURFACE COURSE, MIX C, N 50 (IL 9.5mm); 2"	PG 64 -22	4% @ 50 GYR.
HMA BASE COURSE (HMA BINDER IL-19 mm); PE - 6", CE - 8"	PG 64 -22*	4% @ 50 GYR.
PAVEMENT WIDENING		
HOT-MIX ASPHALT BASE COURSE WIDENING, 10" (HMA BINDER IL-19.0)	PG 64 -22*	4% @ 50 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL SURFACE MIXTURE QUANTITIES IS 112 LB/SQ YD/ IN.
• WHEN THE RAP EXCEEDS 20% THE NEW BINDER IN THE MIX SHALL BE PG 58-22

FILE NAME = n:\proj\3328\phases1\design\MiscSheets\TypicalSections\3328-TypSec2.dgn	USER NAME = ogutowski	DESIGNED - EPS	REVISED -	VILLAGE OF DEERFIELD WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)	PROPOSED TYPICAL SECTIONS	F.A.U. RTE. 2718	SECTION 07-00080-00-WR	COUNTY LAKE	TOTAL SHEETS 55	SHEET NO. 5
	PLOT SCALE = 1/8" = 1'-0"	DRAWN - AL	REVISED -			SCALE: N/A	SHEET NO. 5 OF 55 SHEETS	STA. TO STA.	CONTRACT NO. 63124	
	PLOT DATE = 1/30/2009	CHECKED - CLG	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
	DATE = 1-30-2009		REVISED -							

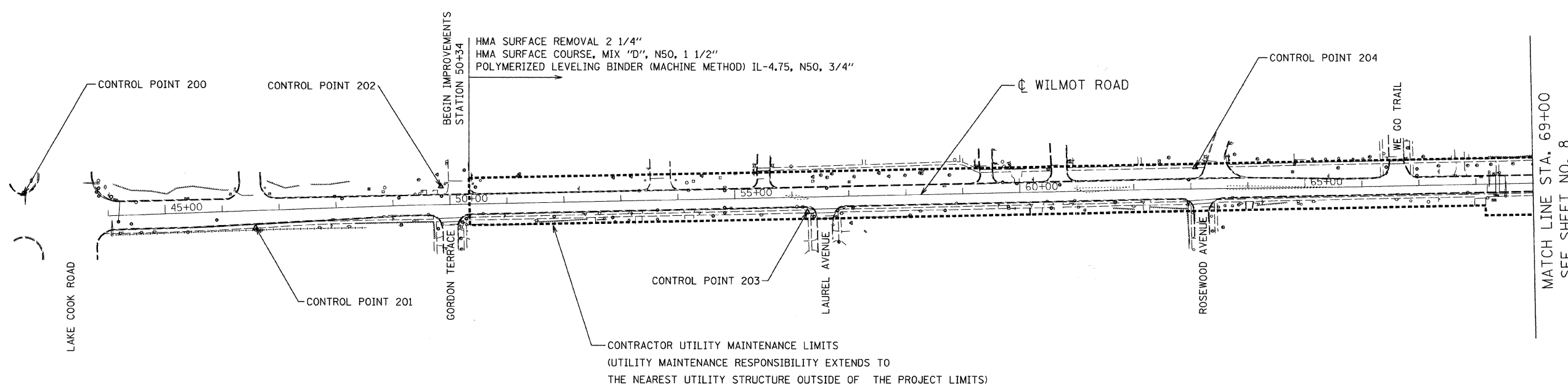
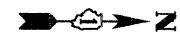
EARTHWORK SCHEDULE						
STATION		20200100		20201200		EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
		EARTH EXCAVATION	EARTH EXCAVATION VOLUME USED (15% SHRINKAGE)	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	EMBANKMENT	
LINE "WILMOT"		(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)
50+34	TO	50+50	14.6	12	0	12
50+50	TO	51+00	44.2	38	0	38
51+00	TO	51+50	39.7	34	0	34
51+50	TO	52+00	37.0	31	0	31
52+00	TO	52+50	36.7	31	0	31
52+50	TO	53+00	34.2	29	0	29
53+00	TO	53+50	42.6	36	0	36
53+50	TO	54+00	45.6	39	0	39
54+00	TO	54+50	33.7	29	0	29
54+50	TO	55+00	33.1	28	0	28
55+00	TO	55+50	37.2	32	0	32
55+50	TO	56+00	43.6	37	0	37
56+00	TO	56+50	50.7	43	0	43
56+50	TO	57+00	51.5	44	0	44
57+00	TO	57+50	52.9	45	0	45
57+50	TO	58+00	54.5	46	0	46
58+00	TO	58+50	51.2	44	0	44
58+50	TO	59+00	46.2	39	0	39
59+00	TO	59+50	36.9	31	0	31
59+50	TO	60+00	31.7	27	0	27
60+00	TO	60+50	44.5	38	0	38
60+50	TO	61+00	44.6	38	0	38
61+00	TO	61+50	36.1	31	0	31
61+50	TO	62+00	36.9	31	0	31
62+00	TO	62+50	34.3	29	0	29
62+50	TO	63+00	32.3	27	0	27
63+00	TO	63+50	28.1	24	0	24
63+50	TO	64+00	31.7	27	0	27
64+00	TO	64+50	39.8	34	0	34
64+50	TO	65+00	43.2	37	0	37
65+00	TO	65+50	36.4	31	0	31
65+50	TO	66+00	13.5	11	0	11
TOTALS			1239	1053	0	1053
79+92	TO	80+00	0.0	0	0	0
80+00	TO	80+50	3.1	3	0	2
80+50	TO	81+00	8.5	7	0	6
81+00	TO	81+50	13.6	12	0	10
81+50	TO	82+00	16.4	14	0	13
82+00	TO	82+50	16.7	14	0	12
82+50	TO	83+00	16.9	14	0	10
83+00	TO	83+50	17.2	15	0	12
83+50	TO	84+00	16.9	14	0	13
84+00	TO	84+50	16.3	14	0	14
84+50	TO	85+00	14.8	13	0	12
85+00	TO	85+50	13.3	11	0	11
85+50	TO	86+00	7.4	6	0	6
86+00	TO	86+03	0.1	0	0	0
TOTALS			161	137	0	123

ESTIMATED QUANTITIES FOR THE FOLLOWING ITEMS HAVE BEEN INCLUDED IN THE PLANS AND SHALL BE UTILIZED ONLY AT THE DISCRETION OF THE RESIDENT ENGINEER

- 300 SQ FT - SIDEWALK REMOVAL
- 300 SQ FT - PCC SIDEWALK 5"
- 50 SQ FT - DETECTABLE WARNINGS
- 200 FEET - CURB REMOVAL
- 200 FEET - COMBINATION CONCRETE CURB AND GUTTER B.6-12
- 100 SQ YD - DRIVEWAY PAVEMENT REMOVAL
- 100 SQ YD - HMA DRIVEWAY PAVEMENT, 8"
- 130 CU YD - POROUS GRANULAR EMBANKMENT, SUBGRADE
- 130 CU YD - REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS
- 390 SQ YD - GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- 1 EACH - PRESSURE CONNECTION
- 750 SQ YD - CLASS D PATCHES 10 INCHES
- 15 TONS - LEVELING BINDER (HAND METHOD)
- 25 EACH - SANITARY SERVICE TO BE ADJUSTED

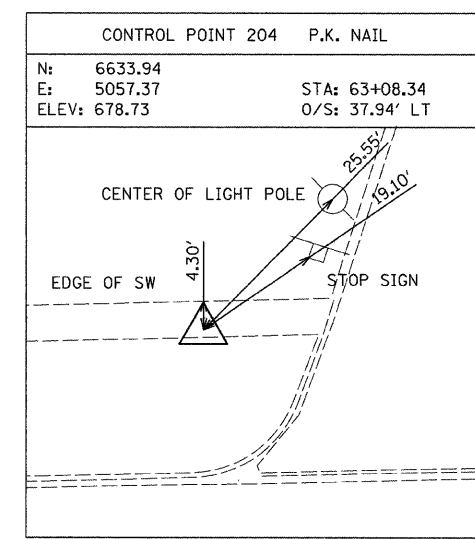
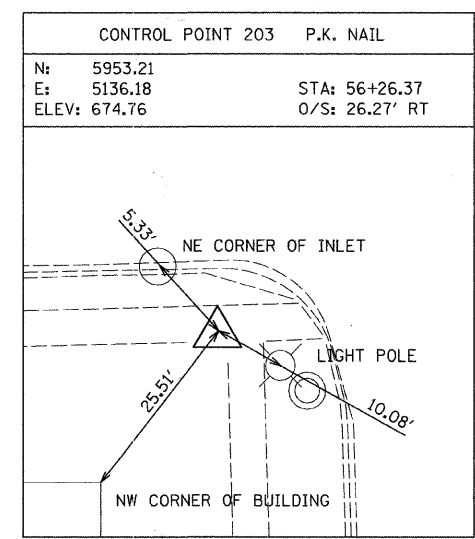
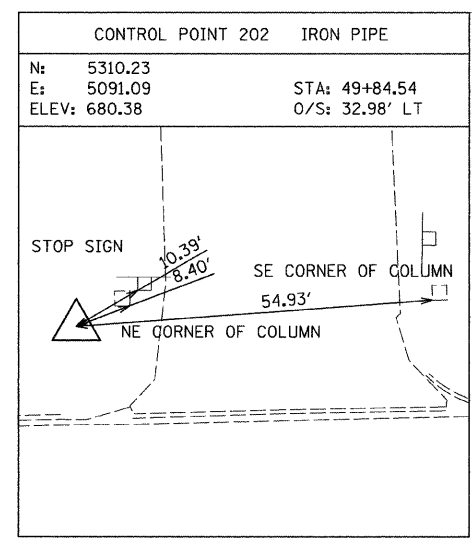
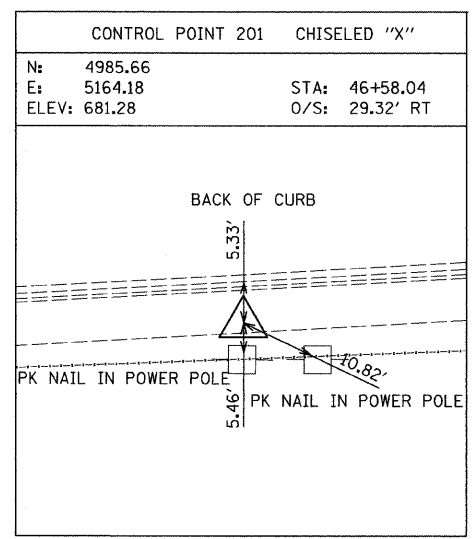
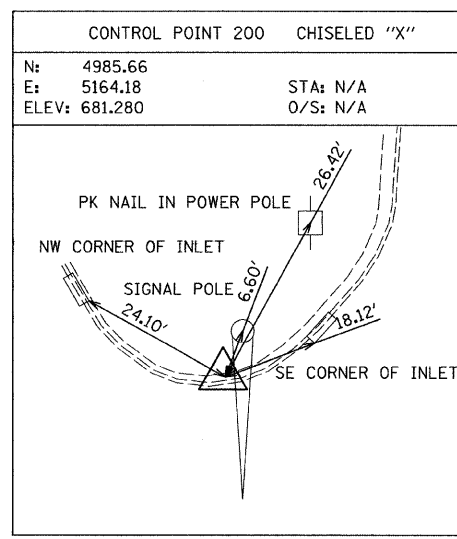
SUMMARY	
20200100	
EARTH EXCAVATION	
(CU YD)	
1,400	

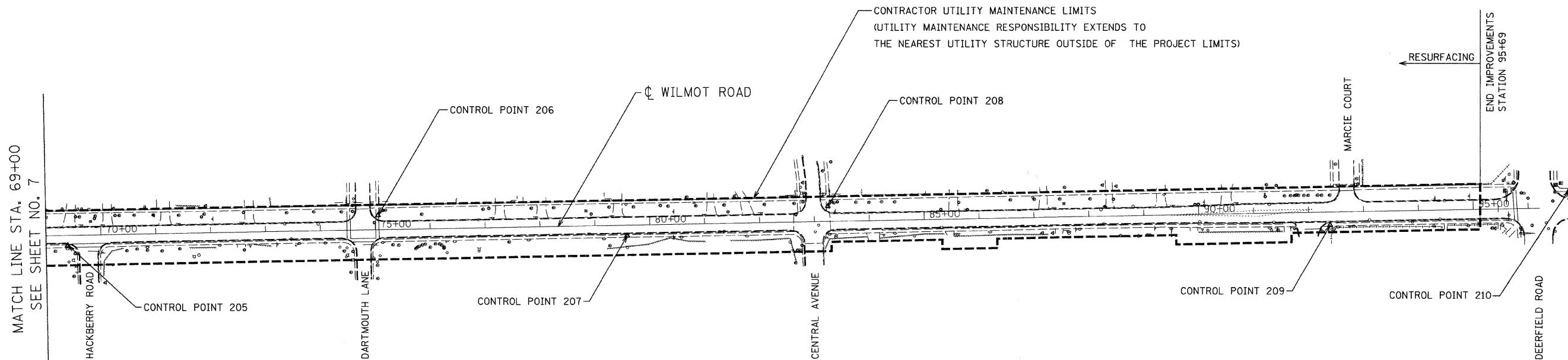
NOTE:
 POROUS GRANULAR EMBANKMENT, SUBGRADE (PGES) AND GEOTECHNICAL FABRIC FOR GROUND STABILIZATION HAS BEEN PROVIDED FOR USE IN THE EVENT THAT UNSTABLE OR UNSUITABLE SUBGRADE CONDITIONS ARE ENCOUNTERED. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY A QUALIFIED SOILS INSPECTOR. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. ANY PGES AND GEOTECHNICAL FABRIC NOT NEEDED AT THE TIME OF CONSTRUCTION SHOULD BE DELETED FROM THE CONTRACT.



WILMOT ROAD CENTERLINE		
DESCRIPTION	NORTHING	EASTING
P.O.T. 44+00.00	4,726.7559	5,143.8599
P.O.T. 49+42.38	5,268.8105	5,124.9892
P.O.T. 62+61.34	6,587.4494	5,095.9245
P.O.T. 65+97.78	6,923.8587	5,091.5005
P.O.T. 68+99.05	7,225.0543	5,084.8567

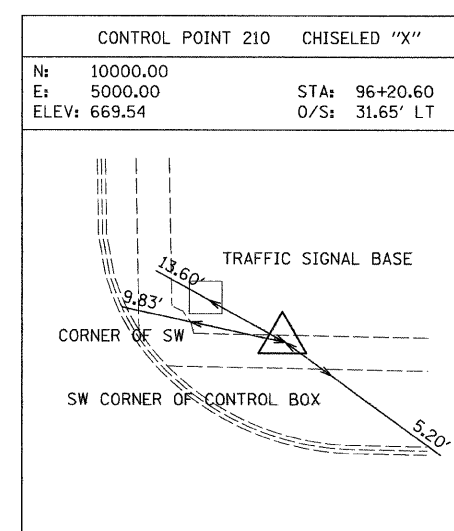
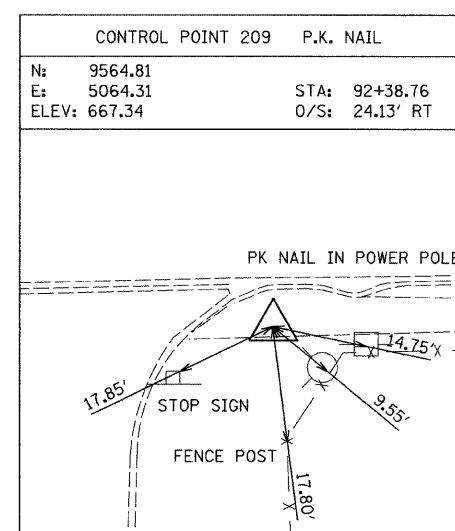
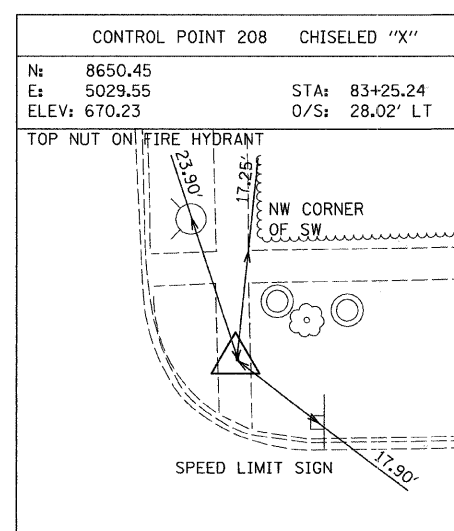
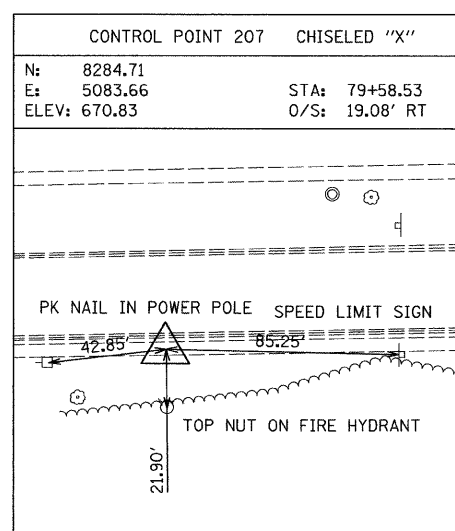
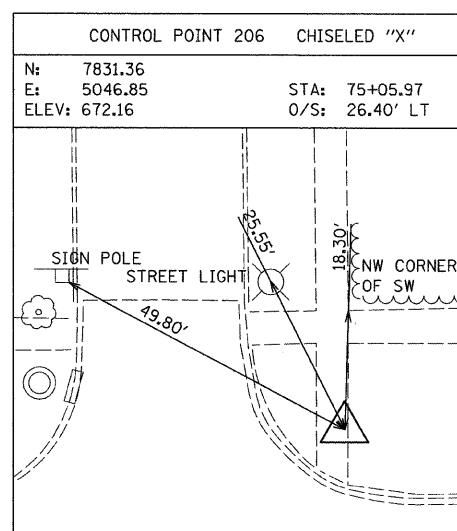
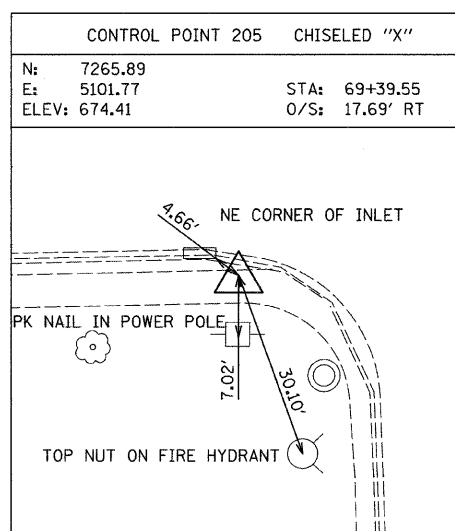
BENCHMARK:
 SURVEY MONUMENT 4.4' RT OF WILMOT ROAD CL AT HACKBERRY ROAD
 STA: 69+77.31
 ELEV = 673.979





WILMOT ROAD CENTERLINE		
DESCRIPTION	NORTHING	EASTING
P.O.T. 83+43.28	8,669.0178	5,057.2203
P.O.T. 91+55.90	9,481.4988	5,041.8082
P.O.T. 96+20.60	9,946.1030	5,032.7149

BENCHMARK:
 SURVEY MONUMENT 4.4' RT OF WILMOT ROAD $\text{\textcircled{C}}$ AT HACKBERRY ROAD
 STA: 69+77.31
 ELEV = 673.979

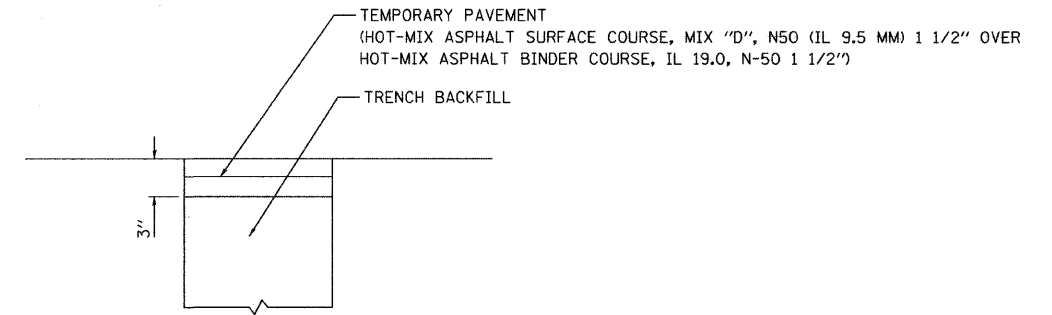


MAINTENANCE OF TRAFFIC GENERAL NOTES

1. THE CONTRACTOR SHALL MAINTAIN ACCESS TO PUBLIC AND PRIVATE PROPERTIES FOR EMERGENCY VEHICLES AT ALL TIMES.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL BARRICADES, LIGHTS AND OTHER DEVICES INSTALLED BY HIM ARE IN PLACE AND OPERATING 24 HOURS EACH DAY INCLUDING SUNDAYS AND HOLIDAYS DURING THE TIME THIS CONSTRUCTION IS IN EFFECT. THIS SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION.
3. TEMPORARY TRAFFIC CONTROL SIGNS MUST BE INSTALLED AT THE DIRECTION AND UNDER THE SUPERVISION OF THE ENGINEER. 48 HOURS NOTICE MUST VE GIVEN TO THE ENGINEER AND THE VILLAGE OF DEERFIELD.
4. ALL TRAFFIC SIGNS SHALL MEET WITH THE APPROVAL OF THE MANUAL ON THE UNIFORM TRAFFIC CONTROL DEVICES AND SHALL MEET WITH THE APPROVAL OF THE ENGINEER REGARDING LOCATION, TYPE, SIZE, AND DURATION.
5. FOR TYPICAL SIGN INSTALLATION, SEE IDOT HIGHWAY STANDARDS.
6. APPROPRIATE IDOT TRAFFIC CONTROL STANDARDS SHALL BE USED TO INSTALL AND REMOVE TRAFFIC CONTROL AND PROTECTION DEVICES.

IF FOR ANY REASON WORK MUST BE PERFORMED AT NIGHT, TYPE 1 OR TYPE 2 BARRICADES WITH STEADY BURNING LIGHTS ARE TO BE USED.
7. AS A MINIMUM, ALL AMBER FLASHING LIGHTS THAT ARE REQUIRED SHALL MEET THE REQUIREMENTS FOR TYPE-A LOW INTENSITY FLASHING LIGHTS IN ARTICLE 1084.01 OF THE STANDARD SPECIFICATIONS. ALL LIGHTS SHALL OPERATE DURING HOURS OF DARKNESS. ONLY LIGHTS THAT HAVE BEEN APPROVED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION SHALL BE USED.
8. FLAGS ON "WARNING" SIGNS ARE OPTIONAL.
9. SIDEWALK ACCESS SHALL BE MAINTAINED ON ONE SIDE OF THE STREET AT ALL TIMES. CLOSED SIDEWALKS SHALL BE APPROPRIATELY BARRICADED. USE STANDARD 701801-04.
10. TEMPORARY PAVEMENT MARKINGS AND SHORT-TERM PAVEMENT MARKINGS SHALL BE USED AS DIRECTED BY THE ENGINEER.

TEMPORARY ACCESS SHALL BE CONSTRUCTED AT ALL CROSS STREETS AND DRIVEWAYS TO MAINTAIN TRAFFIC. IT SHALL BE INSTALLED DURING THE SAME DAY THAT THE PAVEMENT IS REMOVED AND REMAIN IN PLACE UNTIL DIRECTED BY THE ENGINEER TO REMOVE IT. THIS WORK SHALL BE PAID FOR AS AGGREGATE FOR TEMPORARY ACCESS.
11. CONSTRUCTION OPERATIONS THROUGH COMMERCIAL DRIVEWAYS SHALL BE STAGED IN A MANNER THAT WILL ELIMINATE ONLY ONE ACCESS POINT AT A TIME. THE REMAINING ACCESS POINTS SHALL BE MAINTAINED WITH A TEMPORARY SURFACE, AS DIRECTED BY THE ENGINEER, TO PROVIDE SAFE PASSAGE TO DELIVERY TRUCKS, CUSTOMERS AND EMPLOYEES. THIS WORK SHALL BE INCLUDED IN THE COST OF THE DRIVEWAY PAVEMENT PAY ITEM.
12. THE COST OF REMOVING TEMPORARY PAVEMENT (FROM TOP OF SURFACE TO SUBGRADE) SHALL BE INCLUDED IN THE COST OF CLASS D PATCHES.
13. USE STANDARD DRAWING FOR "TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS" WHERE APPROPRIATE.
14. OPEN TRENCHES SHALL BE PLATED OR BACKFILLED TO MATCH THE EXISTING SURROUNDING PAVEMENT AT THE END OF EACH WORK DAY. THE COST OF PLATING A TRENCH SHALL BE CONSIDERED INCIDENTAL TO THE INSTALLATION OF THE UTILITY. ANY TRENCHES THAT ARE BACKFILLED WITH TRENCH BACKFILL TO MATCH THE SURROUNDING PAVEMENT SHALL BE CAPPED WITH THREE INCHES OF BINDER COURSE (AS SHOWN ON THE TEMPORARY PAVEMENT DETAIL) WITHIN 24 HOURS OF THE PLACEMENT OF THE TRENCH BACKFILL. REMOVAL OF THE TOP THREE INCHES OF TRENCH BACKFILL TO ACCOMMODATE THE BINDER WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO TRENCH BACKFILL.
15. THE CONTRACTOR SHALL MAINTAIN TEMPORARY PATCHES AT ALL TIMES. A NOMINAL QUANTITY OF 15 TONS OF LEVELING BINDER, HAND METHOD HAS BEEN INCLUDED IN THE PLANS FOR PATCHING OF THE TEMPORARY PAVEMENT.
16. OPEN TRENCHES SHALL NOT BE LEFT UNATTENDED AT ANY TIME.
17. ALL EQUIPMENT AND MATERIALS SHALL BE REMOVED FROM THE DRIVING SURFACE OF WILMOT ROAD AT THE END OF EACH WORKING DAY.
18. THE COST OF INSTALLING, MAINTAINING, AND REMOVING THE DETOUR SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR.
19. ONE WAY NORTHBOUND TRAFFIC SHALL BE PROVIDED ON WILMOT ROAD THROUGH THE WORK ZONE.
20. TWO NEW SERVICES WILL BE ADDED FOR THE WALGREEN PROPERTIES AND A NEW 4" SERVICE IS EXPECTED TO BE INSTALLED FOR WILMOT SCHOOL. THE CONTRACTOR SHALL COORDINATE WITH THE PROPERTY OWNER CONCERNING THE INSTALLATION OF THESE SERVICES AND WORK MAY HAVE TO BE DONE DURING OFF-PEAK HOURS OR ON WEEKENDS. NO ADDITIONAL COMPENSATION WILL BE AWARDED TO THE CONTRACTOR FOR OVERTIME OR ADDITIONAL TIME SPENT TO PREPARE AND INSTALL THESE CONNECTIONS.



TEMPORARY PAVEMENT DETAIL

FILE NAME = N:\PROJ\3328\Phase1\Design\MOT\3328-MOT-NOTES.dgn	USER NAME = ogutowski	DESIGNED - EPS	REVISED -	VILLAGE OF DEERFIELD WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)	MAINTENANCE OF TRAFFIC GENERAL NOTES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 2.0004" / IN.	DRAWN - AL	REVISED -			2718	07-00080-00-WR	LAKE	57	17
	PLOT DATE = 4/7/2009	CHECKED - CLG	REVISED -			CONTRACT NO. 63124				
		DATE - 4-7-2009	REVISED -	SCALE:	SHEET NO. 17 OF 57 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		

DETOUR PLAN WILMOT ROAD: SUGGESTED STAGES OF CONSTRUCTION

STAGE 1

A. CONSTRUCTION

1. REMOVE EXISTING NORTHBOUND CURB AND GUTTERS AND DRIVEWAYS
2. INSTALL PROPOSED DRAINAGE STRUCTURES.
3. PLACE STORM SEWER, WATERMAIN, PUSH WATERMAIN SERVICES AND 10" PATCHING WHEN COMPLETE.
4. PLACE CURB AND GUTTER, DRIVEWAYS, AND PAVEMENT WIDENING WHERE INDICATED ON PLANS.

B. TRAFFIC CONTROL

1. DETOUR SIGNS PLACED.
2. TRAFFIC MAINTAINED AS SHOWN IN TYPICAL SECTION FOR STAGE 1.
3. ROADWAY CLOSED TO SOUTHBOUND TRAFFIC WILMOT ROAD, FROM GORDON TERRACE TO DEERFIELD ROAD.
4. ROADWAY OPENED TO NORTHBOUND TRAFFIC TRAVELED IN WEST LANE.

STAGE 2

A. CONSTRUCTION

1. REMOVE EXISTING SOUTHBOUND CURB AND GUTTERS AND DRIVEWAYS
2. INSTALL PROPOSED DRAINAGE STRUCTURES.
3. PLACE STORM SEWER, WATERMAIN, PUSH WATERMAIN SERVICES, CONNECT TO FIRE HYDRANTS, AND 10" PATCHING WHEN COMPLETE.
4. PLACE CURB AND GUTTER, DRIVEWAYS, AND PAVEMENT WIDENING WHERE INDICATED ON PLANS.

B. TRAFFIC CONTROL

1. DETOUR SIGNS TO REMAIN IN PLACE.
2. TRAFFIC MAINTAINED AS SHOWN IN TYPICALS SECTION FOR STAGE 2.
3. ROADWAY CLOSED TO SOUTHBOUND TRAFFIC WILMOT ROAD, FROM GORDON TERRACE TO DEERFIELD ROAD.
4. ROADWAY OPENED TO NORTHBOUND TRAFFIC TRAVELED IN EAST LANE.

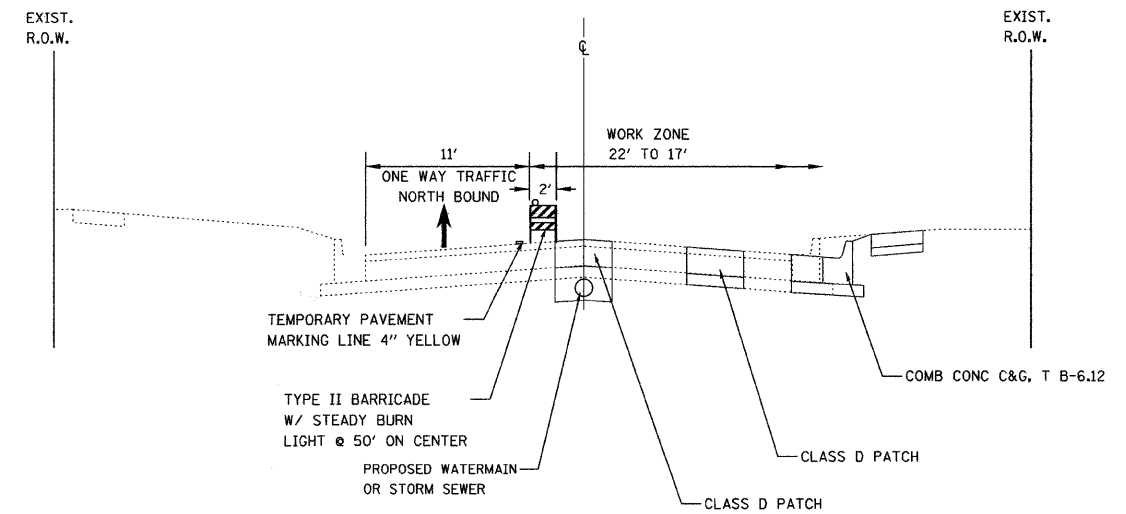
STAGE 3/POST-STAGE 3

A. CONSTRUCTION

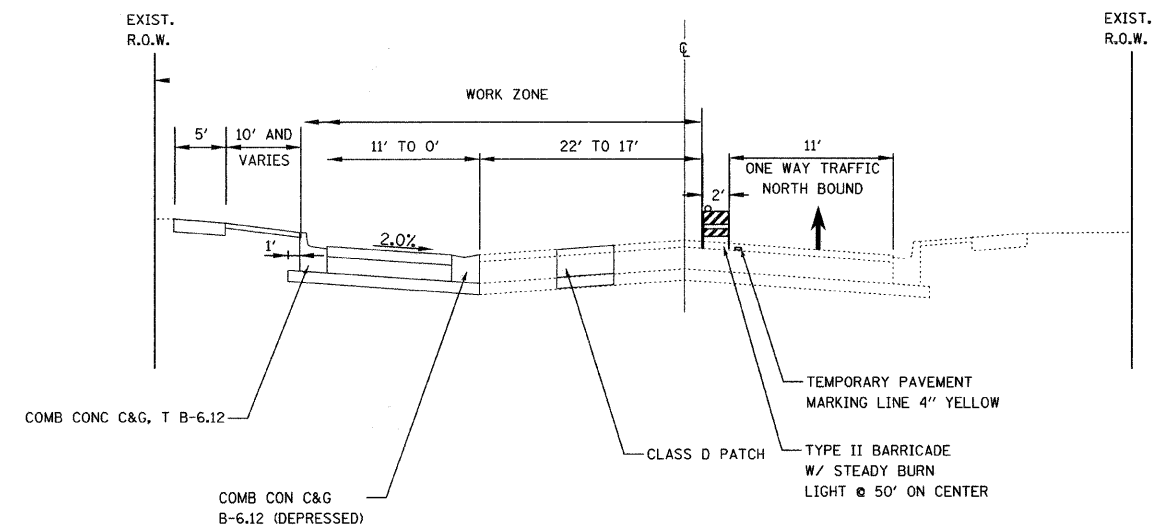
1. MILLING OF EXISTING SURFACE.
2. LEVELING BINDER AND SURFACE COURSE PLACED ALONG WILMOT ROAD, FROM GORDON TERRACE TO DEERFIELD ROAD.
3. BINDER COURSE AND SURFACE COURSE PLACED AT RIGHT TURN LANE.

B. TRAFFIC CONTROL

1. ROADWAY OPEN TO BOTH NORTHBOUND AND SOUTHBOUND TRAFFIC WITH FLAGGERS ON SITE TO DIRECT TRAFFIC.
2. ROAD CONSTRUCTION AHEAD SIGNS TO REMIAN ERECT.
3. DETOUR SIGNS REMOVED.

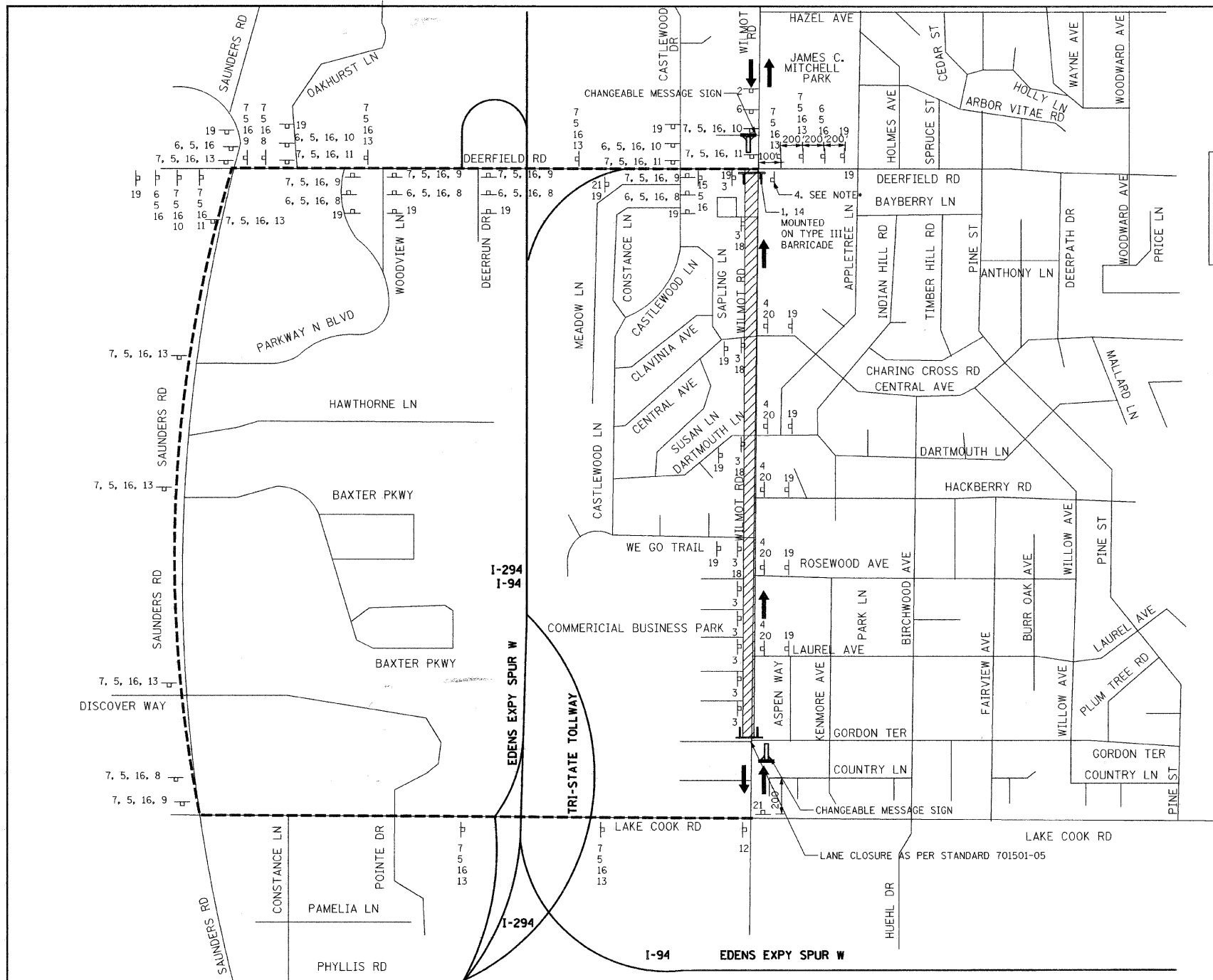


MAINTENANCE OF TRAFFIC
WILMOT ROAD
GORDON TERRACE TO DEERFIELD ROAD
STAGE 1

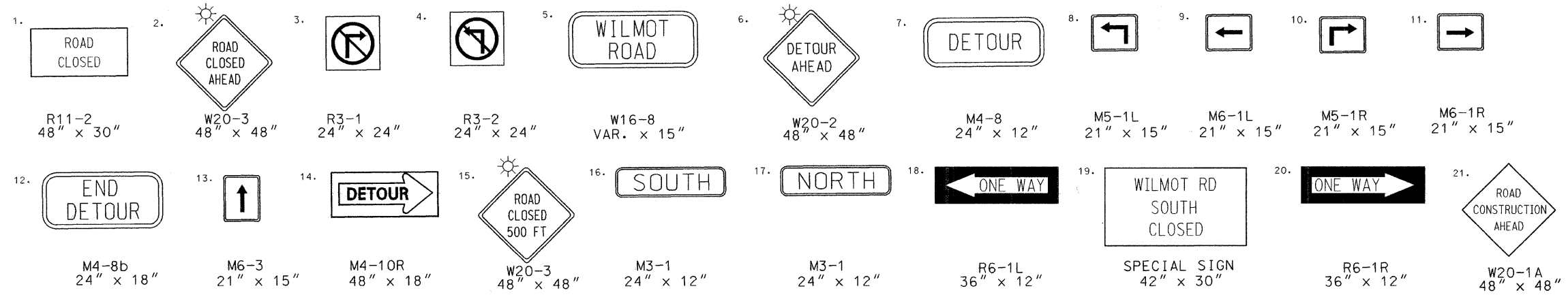
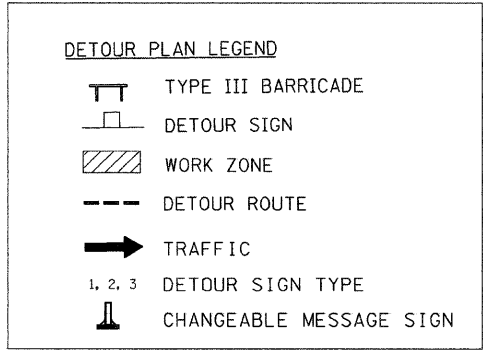
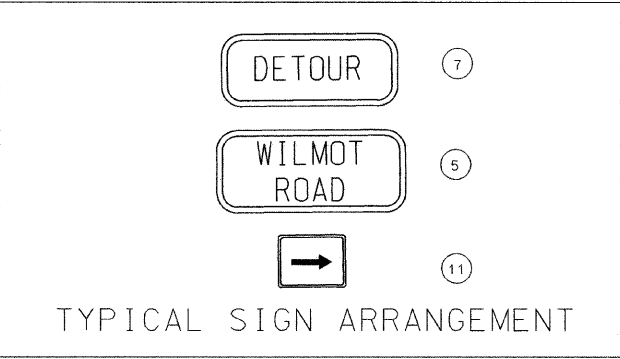


MAINTENANCE OF TRAFFIC
WILMOT ROAD
GORDON TERRACE TO DEERFIELD ROAD
STAGE 2

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	PLOT SCALE = 2.0004" / IN.	DRAWN - AL	REVISED -			2718	07-00080-00-WR	LAKE	57	17A
	PLOT DATE = 4/7/2009	CHECKED - CLG	REVISED -			CONTRACT NO. 63124				
	DATE - 4-7-2009	REVISED -	SCALE:			SHEET NO. 17 OF 57 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT



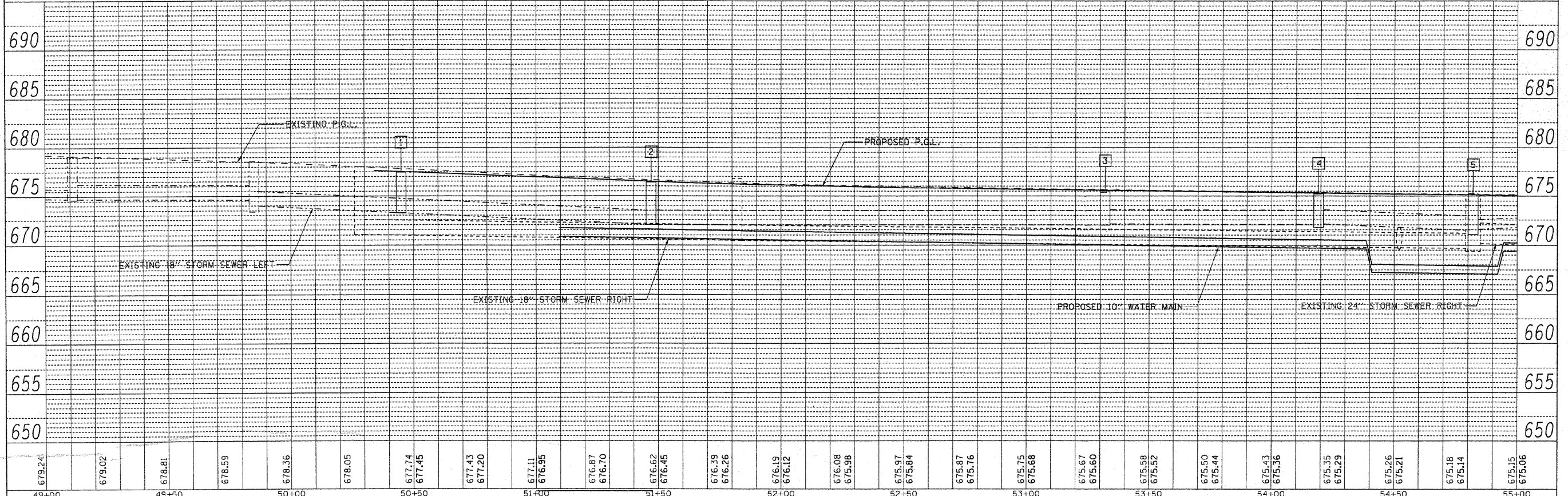
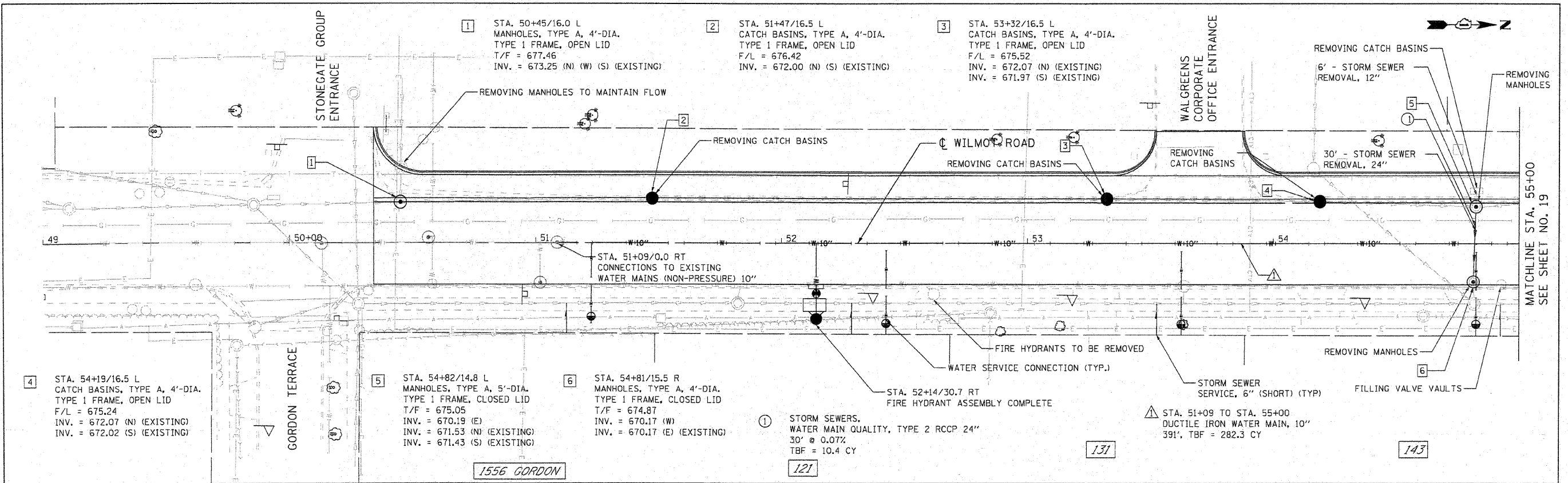
*** NOTE:**
 WESTBOUND LEFT TURN
 LANE ON DEERFIELD ROAD
 AT WILMOT ROAD SHALL BE
 CLOSED AS PER STANDARD
 701701-06.



FILE NAME =	USER NAME = ogutowski	DESIGNED - EPS	REVISIONS	VILLAGE OF DEERFIELD WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)	MAINTENANCE OF TRAFFIC - DETOUR PLAN			F.A.U. R.T.E. = 2718	SECTION = 07-00080-00-WR	COUNTY = LAKE	TOTAL SHEETS = 57	SHEET NO. = 17B
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	PLOT DATE = 4/7/2009	CHECKED - CLG	REVISIONS		FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT							
		DATE - 4-7-2009	REVISIONS									

DATE	
BY	
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NO. OF SHEETS	
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NO.	

DATE	
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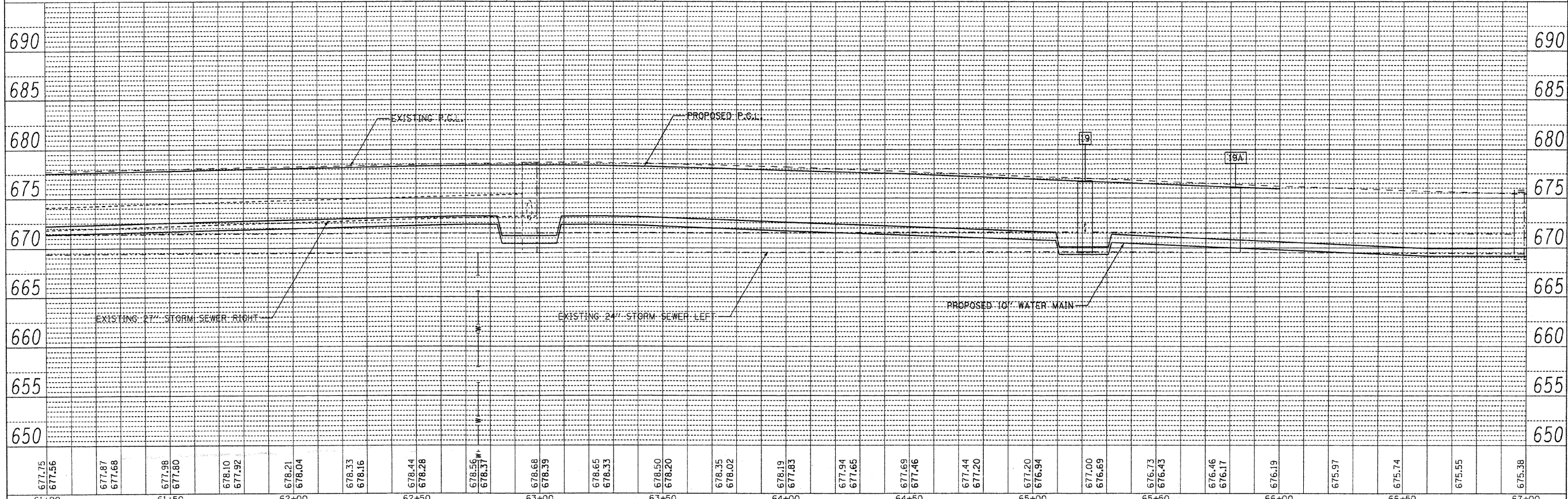
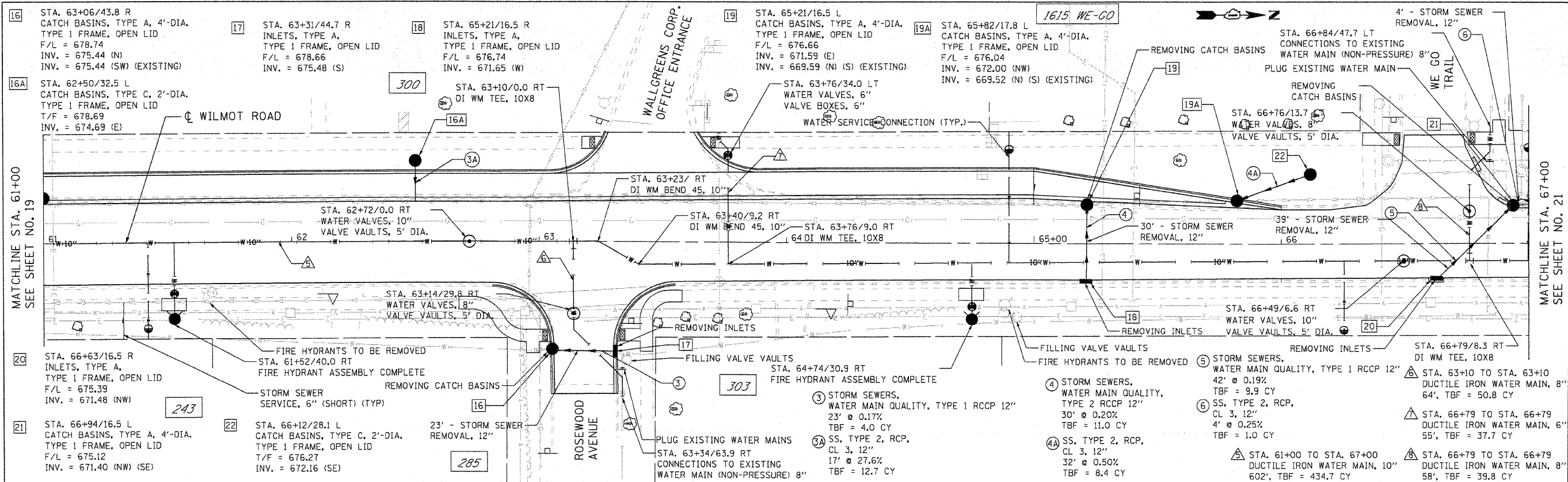


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49+00	49+10	49+20	49+30	49+40	49+50	49+60	49+70	49+80	49+90	50+00	50+10	50+20	50+30	50+40	50+50	50+60	50+70	50+80	50+90	51+00	51+10	51+20	51+30	51+40	51+50	51+60	51+70	51+80	51+90	52+00	52+10	52+20	52+30	52+40	52+50	52+60	52+70	52+80	52+90	53+00	53+10	53+20	53+30	53+40	53+50	53+60	53+70	53+80	53+90	54+00	54+10	54+20	54+30	54+40	54+50	54+60	54+70	54+80	54+90	55+00

USER NAME = agutowski	DESIGNED - EPS	REVISED -	VILLAGE OF DEERFIELD WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)	PROPOSED DRAINAGE PLAN	F.A.U. SECTION COUNTY TOTAL SHEET	
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PLOT DATE = 5/13/2009	CHECKED - CLG	REVISED -			CONTRACT NO. 63124	
DATE - 4-7-2009	REVISED -	SCALE: 1" = 20' SHEET NO. 18 OF 55 SHEETS STA. 49+00 TO STA. 55+00				

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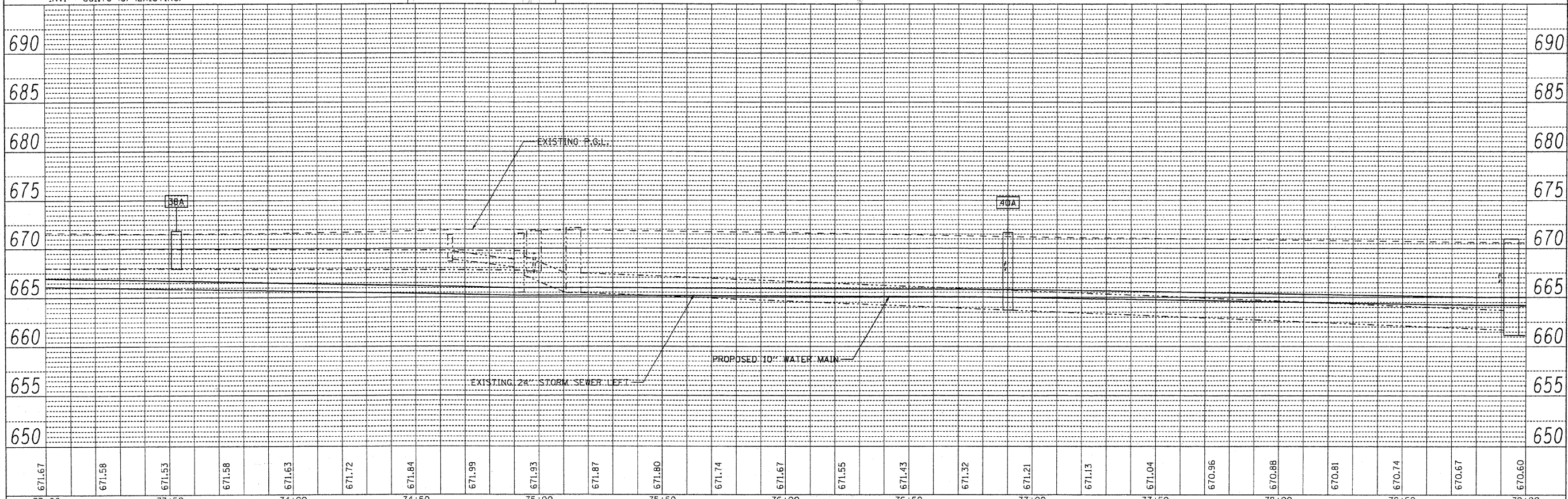
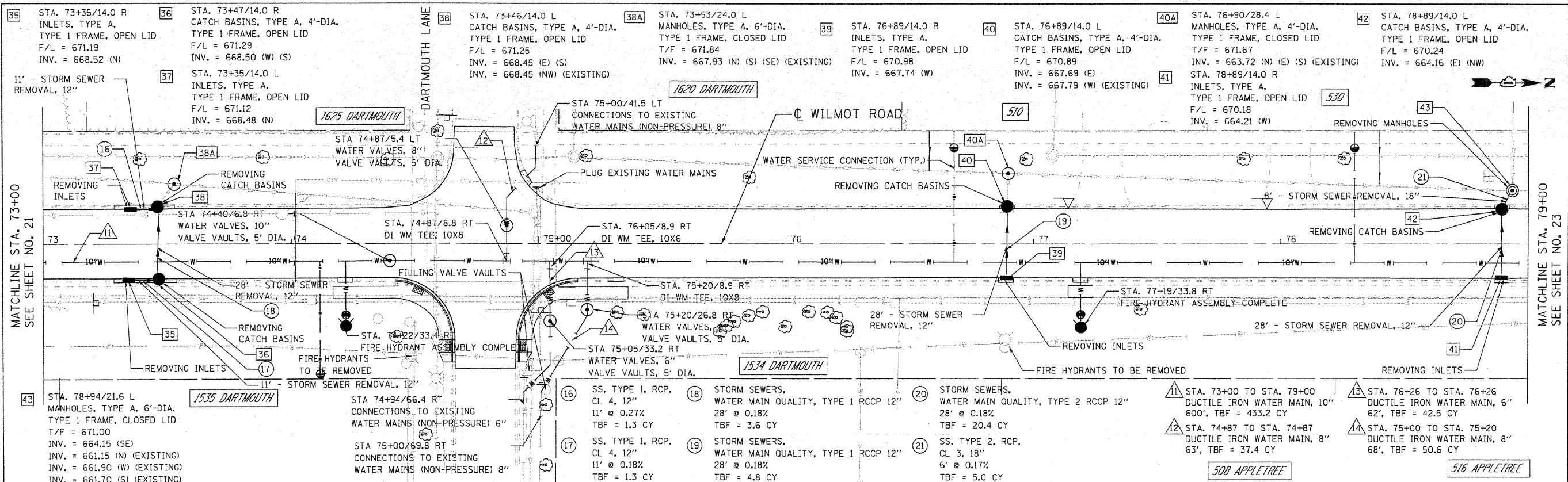
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



677.75	677.56	677.87	677.68	677.98	677.80	678.10	677.92	678.21	678.04	678.33	678.16	678.44	678.28	678.56	678.37	678.68	678.50	678.82	678.63	678.94	678.75	679.06	678.87	679.18	679.00	679.31	679.12	679.43	679.24	679.55	679.36	679.67	679.48	679.79	679.60	679.91	679.72	680.03	679.84	680.15	679.96	680.27	680.08	680.39	680.20	680.51	680.32	680.63	680.44	680.75	680.56	680.87	680.68	680.99	680.80	681.11	680.92	681.23	681.04	681.35	681.16	681.47	681.28	681.59	681.40	681.71	681.52	681.83	681.64	681.95	681.76	682.07	681.88	682.19	682.00	682.31	682.12	682.43	682.24	682.55	682.36	682.67	682.48	682.79	682.60	682.91	682.72	683.03	682.84	683.15	682.96	683.27	683.08	683.39	683.20	683.51	683.32	683.63	683.44	683.75	683.56	683.87	683.68	683.99	683.80	684.11	683.92	684.23	684.04	684.35	684.16	684.47	684.28	684.59	684.40	684.71	684.52	684.83	684.64	684.95	684.76	685.07	684.88	685.19	685.00	685.31	685.12	685.43	685.24	685.55	685.36	685.67	685.48	685.79	685.60	685.91	685.72	686.03	685.84	686.15	685.96	686.27	686.08	686.39	686.20	686.51	686.32	686.63	686.44	686.75	686.56	686.87	686.68	686.99	686.80	687.11	686.92	687.23	687.04	687.35	687.16	687.47	687.28	687.59	687.40	687.71	687.52	687.83	687.64	687.95	687.76	688.07	687.88	688.19	688.00	688.31	688.12	688.43	688.24	688.55	688.36	688.67	688.48	688.79	688.60	688.91	688.72	689.03	688.84	689.15	688.96	689.27	689.08	689.39	689.20	689.51	689.32	689.63	689.44	689.75	689.56	689.87	689.68	690.00	689.81	690.11	689.92	690.23	690.04	690.35	690.16	690.47	690.28	690.59	690.40	690.71	690.52	690.83	690.64	690.95	690.76	691.07	690.88	691.19	691.00	691.31	691.12	691.43	691.24	691.55	691.36	691.67	691.48	691.79	691.60	691.91	691.72	692.03	691.84	692.15	691.96	692.27	692.08	692.39	692.20	692.51	692.32	692.63	692.44	692.75	692.56	692.87	692.68	692.99	692.80	693.11	692.92	693.23	693.04	693.35	693.16	693.47	693.28	693.59	693.40	693.71	693.52	693.83	693.64	693.95	693.76	694.07	693.88	694.19	694.00	694.31	694.12	694.43	694.24	694.55	694.36	694.67	694.48	694.79	694.60	694.91	694.72	695.03	694.84	695.15	694.96	695.27	695.08	695.39	695.20	695.51	695.32	695.63	695.44	695.75	695.56	695.87	695.68	695.99	695.80	696.11	695.92	696.23	696.04	696.35	696.16	696.47	696.28	696.59	696.40	696.71	696.52	696.83	696.64	696.95	696.76	697.07	696.88	697.19	697.00	697.31	697.12	697.43	697.24	697.55	697.36	697.67	697.48	697.79	697.60	697.91	697.72	698.03	697.84	698.15	697.96	698.27	698.08	698.39	698.20	698.51	698.32	698.63	698.44	698.75	698.56	698.87	698.68	698.99	698.80	699.11	698.92	699.23	699.04	699.35	699.16	699.47	699.28	699.59	699.40	699.71	699.52	699.83	699.64	699.95	699.76	700.07	699.88	700.19	699.96	700.31	700.04	700.43	700.12	700.55	700.24	700.67	700.36	700.79	700.48	700.91	700.60	701.03	700.72	701.15	700.84	701.27	700.96	701.39	701.08	701.51	701.20	701.63	701.32	701.75	701.44	701.87	701.56	701.99	701.68	702.11	701.78	702.23	701.88	702.35	701.98	702.47	702.08	702.59	702.18	702.71	702.28	702.83	702.38	702.95	702.48	703.07	702.58	703.19	702.68	703.31	702.78	703.43	702.88	703.55	702.98	703.67	703.08	703.79	703.18	703.91	703.28	704.03	703.38	704.15	703.48	704.27	703.58	704.39	703.68	704.51	703.78	704.63	703.88	704.75	703.98	704.87	704.08	704.99	704.18	705.11	704.28	705.23	704.38	705.35	704.48	705.47	704.58	705.59	704.68	705.71	704.78	705.83	704.88	705.95	704.98	706.07	705.08	706.19	705.18	706.31	705.28	706.43	705.38	706.55	705.48	706.67	705.58	706.79	705.68	706.91	705.78	707.03	705.88	707.15	705.98	707.27	706.08	707.39	706.18	707.51	706.28	707.63	706.38	707.75	706.48	707.87	706.58	707.99	706.68	708.11	706.78	708.23	706.88	708.35	706.98	708.47	707.08	708.59	707.18	708.71	707.28	708.83	707.38	708.95	707.48	709.07	707.58	709.19	707.68	709.31	707.78	709.43	707.88	709.55	707.98	709.67	708.08	709.79	708.18	709.91	708.28	710.03	708.38	710.15	708.48	710.27	708.58	710.39	708.68	710.51	708.78	710.63	708.88	710.75	708.98	710.87	709.08	710.99	709.18	711.11	709.28	711.23	709.38	711.35	709.48	711.47	709.58	711.59	709.68	711.71	709.78	711.83	709.88	711.95	709.98	712.07	710.08	712.19	710.18	712.31	710.28	712.43	710.38	712.55	710.48	712.67	710.58	712.79	710.68	712.91	710.78	713.03	710.88	713.15	710.98	713.27	711.08	713.39	711.18	713.51	711.28	713.63	711.38	713.75	711.48	713.87	711.58	713.99	711.68	714.11	711.78	714.23	711.88	714.35	711.98	714.47	712.08	714.59	712.18	714.71	712.28	714.83	712.38	714.95	712.48	715.07	712.58	715.19	712.68	715.31	712.78	715.43	712.88	715.55	712.98	715.67	713.08	715.79	713.18	715.91	713.28	716.03	713.38	716.15	713.48	716.27	713.58	716.39	713.68	716.51	713.78	716.63	713.88	716.75	713.98	716.87	714.08	716.99	714.18	717.11	714.28	717.23	714.38	717.35	714.48	717.47	714.58	717.59	714.68	717.71	714.78	717.83	714.88	717.95	714.98	718.07	715.08	718.19	715.18	718.31	715.28	718.43	715.38	718.55	715.48	718.67	715.58	718.79	715.68	718.91	715.78	719.03	715.88	719.15	715.98	719.27	716.08	719.39	716.18	719.51	716.28	719.63	716.38	719.75	716.48	719.87	716.58	719.99	716.68	720.11	716.78	720.23	716.88	720.35	716.98	720.47	717.08	72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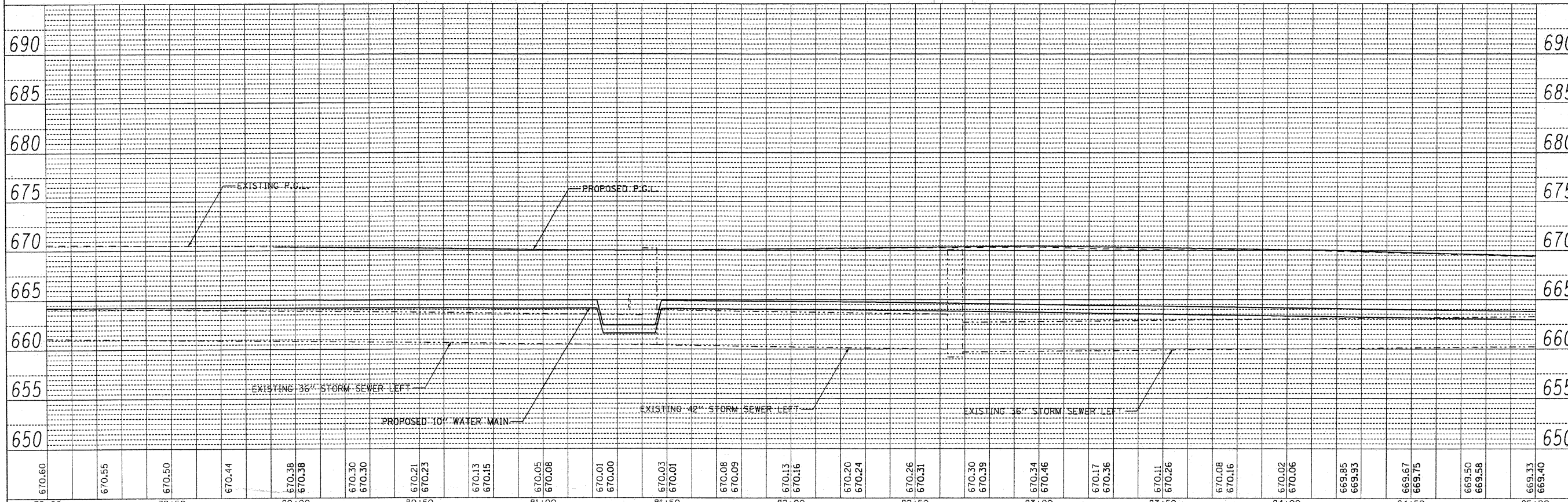
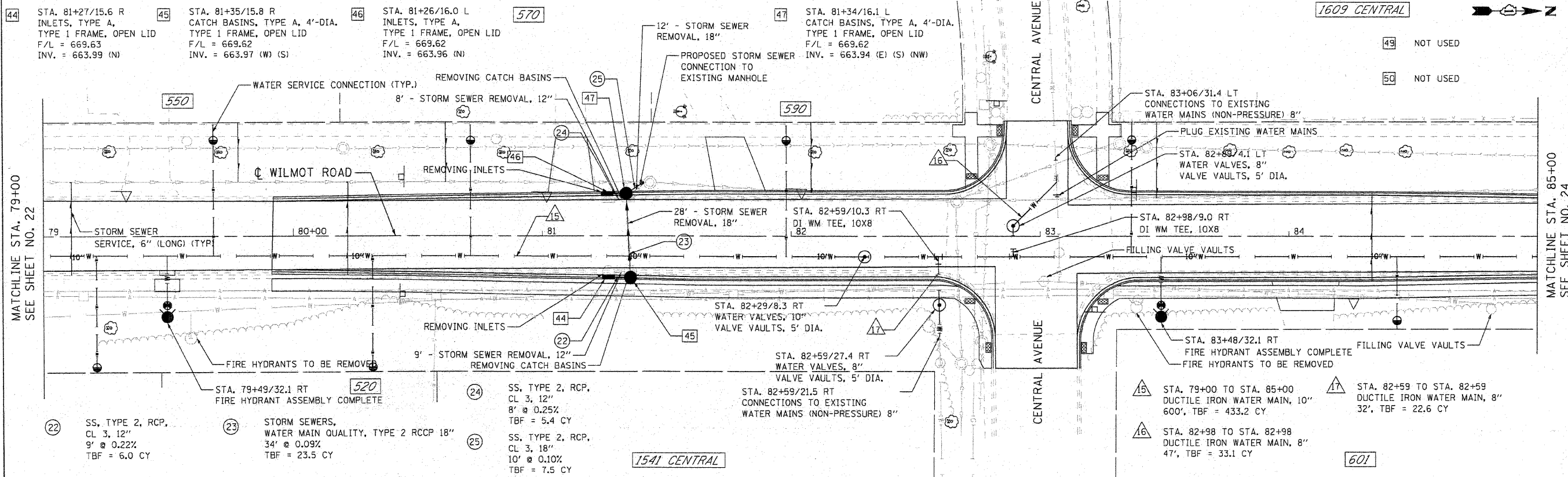
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671.67	671.58	671.53	671.58	671.63	671.72	671.84	671.99	671.93	671.87	671.80	671.74	671.67	671.55	671.43	671.32	671.21	671.13	671.04	670.96	670.88	670.81	670.74	670.67	670.60
73+00		73+50		74+00		74+50		75+00		75+50		76+00		76+50		77+00		77+50		78+00		78+50		79+00

USER NAME = agutowski DESIGNED - EPS REVISIONS - DRAWN - AL REVISIONS - CHECKED - CLG REVISIONS - DATE - 4-7-2009 REVISIONS -	VILLAGE OF DEERFIELD WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)	PROPOSED DRAINAGE PLAN SCALE: 1" = 20' SHEET NO. 22 OF 55 SHEETS STA. 73+00 TO STA. 79+00	F.A.U. SECTION COUNTY TOTAL SHEETS SHEET NO. 2718 07-00080-00-WR LAKE 55 22 CONTRACT NO. 63124 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
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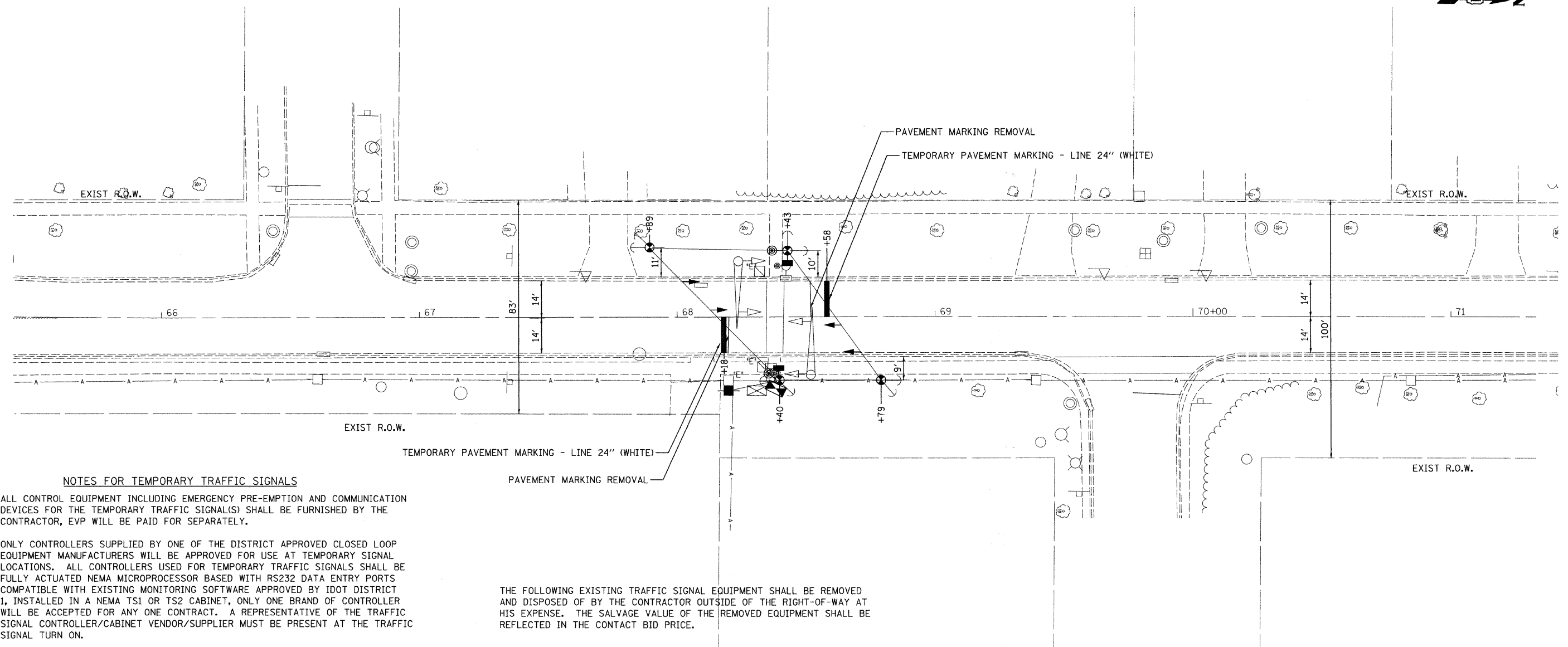
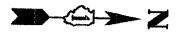


79+00	670.60	670.55	670.50	670.44	670.38	670.38	670.30	670.30	670.21	670.23	670.13	670.15	670.05	670.08	670.01	670.00	670.03	670.01	670.08	670.09	670.13	670.16	670.20	670.24	670.26	670.31	670.30	670.39	670.34	670.46	670.17	670.36	670.11	670.26	670.08	670.16	670.02	670.06	669.85	669.93	669.67	669.75	669.50	669.58	669.33	669.40		
79+00	79+50	80+00	80+50	81+00	81+50	82+00	82+50	83+00	83+50	84+00	84+50	85+00																																				

USER NAME = agutowski DESIGNED - EPS REVISIONS: DRAWN - AL CHECKED - CLG DATE - 4-7-2009	VILLAGE OF DEERFIELD WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)	PROPOSED DRAINAGE PLAN SCALE: 1" = 20' SHEET NO. 23 OF 55 SHEETS STA. 79+00 TO STA. 85+00	F.A.U. RTE. 2718 SECTION 07-00080-00-WR COUNTY LAKE TOTAL SHEETS 55 SHEET NO. 23 CONTRACT NO. 63124 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
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NOTES FOR TEMPORARY TRAFFIC SIGNALS

- ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR, EVP WILL BE PAID FOR SEPARATELY.
- ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS1 OR TS2 CABINET, ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT. A REPRESENTATIVE OF THE TRAFFIC SIGNAL CONTROLLER/CABINET VENDOR/SUPPLIER MUST BE PRESENT AT THE TRAFFIC SIGNAL TURN ON.
- ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12" (300mm), HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATED HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS, EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL AT THE TIME OF THE TURN ON. IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- THE STOP LINES ARE TO BE RELOCATED AT THE TIME OF THE TEMPORARY SIGNAL TURN ON. A QUANTITY OF 30 FEET OF TEMPORARY PAVEMENT MARKING - LINE 24" AND 60 SOFT OF PAVEMENT MARKING REMOVAL HAVE BEEN INCLUDED IN THE CONTRACT QUANTITIES FOR THIS WORK.

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR OUTSIDE OF THE RIGHT-OF-WAY AT HIS EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTACT BID PRICE.

- 1 EACH CONTROLLER AND CABINET (COMPLETE)
- 4 EACH SIGNAL HEAD, 1-FACE, 3-SECTION
- 2 EACH ALUMINUM MAST ARM POLE
- 2 EACH SIGNAL POST
- 2 EACH PEDESTRIAN SIGNAL HEAD, 1-FACE
- 2 EACH PEDESTRIAN PUSHBUTTON
- 1 EACH SERVICE INSTALLATION

RESTORATION OF THE WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, AND TRENCH AND BACKFILL, ETC.. SURFACES SUCH AS SHOULDERS, MEDIANS SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

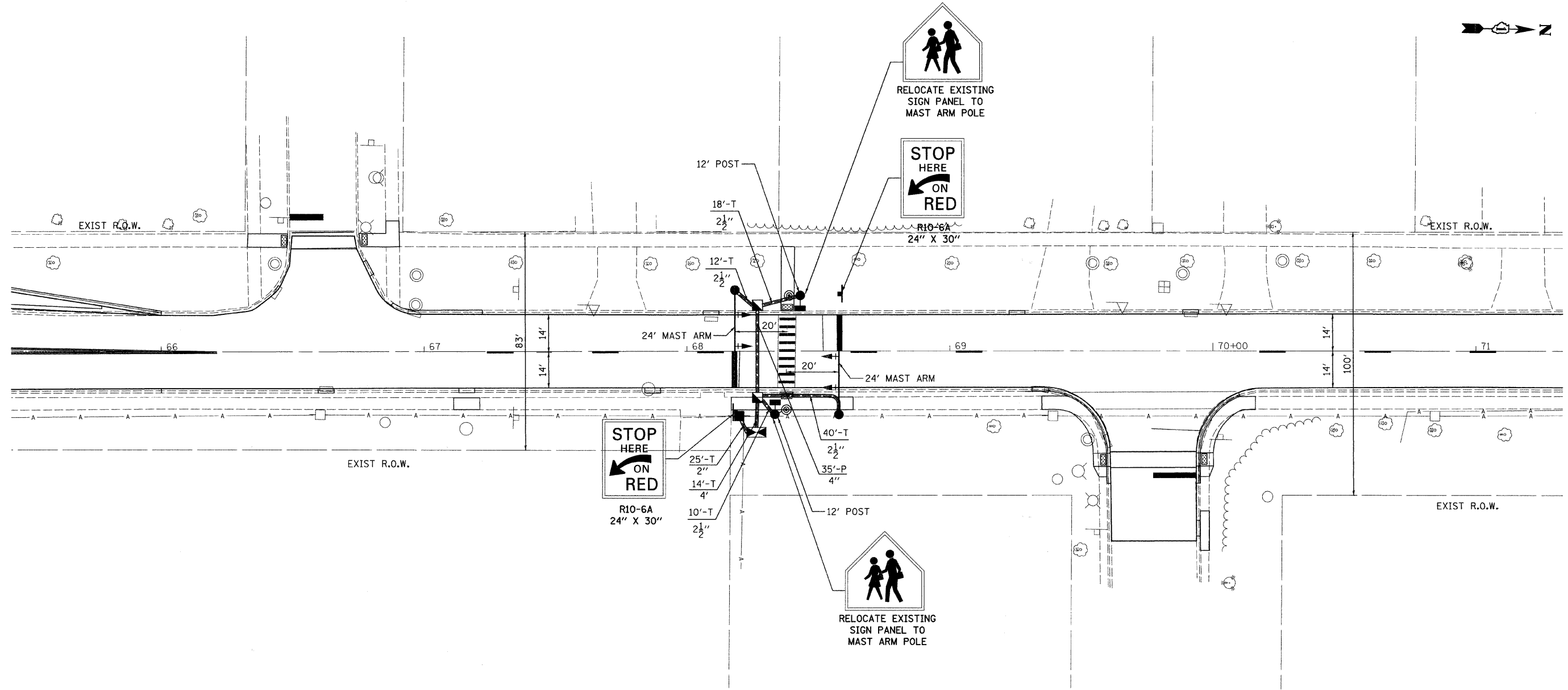
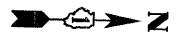
TEMPORARY CABLE DIAGRAM LEGEND

- ▶ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORINGAL LOCATION
- ◀ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- ⊙ TEMPORARY WOOD POLE
- ▣ TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE AND CABLE
- TEMPORARY SERVICE INSTALLATION
- ▬ TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- ⊙ PEDESTRIAN PUSHBUTTON DETECTOR

EXISTING EQUIPMENT TO BE REMOVED LEGEND

- ◀ EXISTING SIGNAL HEAD TO BE REMOVED
- ⊙ EXISTING SERVICE INSTALLATION TO BE REMOVED
- ⊙ EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED
- ⊙ EXISTING CONTROLLER AND FOUNDATION TO BE REMOVED
- ⊙ EXISTING HANDHOLE TO BE REMOVED
- ⊙ EXISTING PEDESTRIAN SIGNAL TO BE REMOVED
- ⊙ EXISTING PEDESTRIAN PUSHBUTTON TO BE REMOVED
- ⊙ EXISTING MAST ARM POLE AND FOUNDATION TO BE REMOVED

FILE NAME = n:\proj\3328\phase1\design\Traffic Analysis\Ped Crossing\3328-TEMPSIG1.dgn	USER NAME = egutowski	DESIGNED - JCC	REVISED -	VILLAGE OF DEERFIELD WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)	TEMPORARY TRAFFIC SIGNAL AND REMOVAL PLAN	F.A.U. RTE. 2718	SECTION 07-00080-00-WR	COUNTY LAKE	TOTAL SHEETS 55	SHEET NO. 26
	PLOT SCALE = 1/30/2009	DRAWN - JCC	REVISED -			CONTRACT NO. 63124				
	PLOT DATE = 1/30/2009	CHECKED - CLG	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
	DATE - 1-30-2009	REVISED -		SCALE: 1" = 20'	SHEET NO. OF 55 SHEETS	STA. TO STA.				



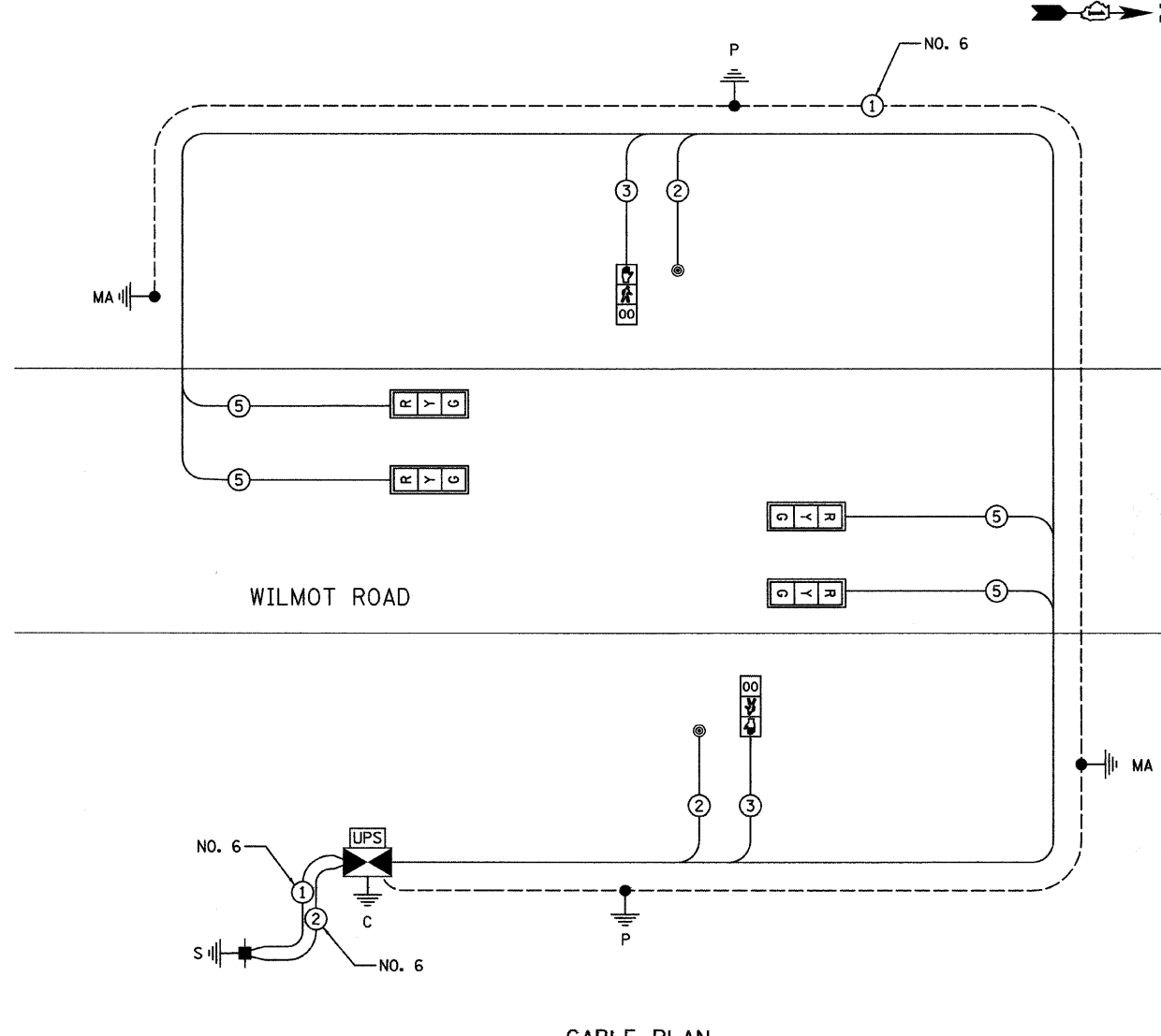
TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING		PROPOSED	EXISTING
CONTROLLER			EMERGENCY VEHICLE SYSTEM DETECTOR		
SERVICE INSTALLATION			CONFIRMATION BEACON		
SIGNAL HEAD			SIGNAL HEAD OPTICALLY PROGRAMMED		
SIGNAL HEAD WITH BACKPLATE			CONDUIT SPLICE		
SIGNAL HEAD, PEDESTRIAN WITH TIMER			WOOD POLE		
SIGNAL POST			RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		
MAST ARM ASSEMBLY AND POLE, STEEL			VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
MAST ARM ASSEMBLY AND POLE, ALUMINUM			RAILROAD CONTROLLER		
COMMON TRENCH	CT		ILLUMINATED SIGN, FIBER OPTIC 'NO LEFT TURN'		
UNIT DUCT	UD		ILLUMINATED SIGN, FIBER OPTIC 'NO RIGHT TURN'		
HANDHOLE			TELEPHONE CONNECTION		
HEAVY DUTY HANDHOLE			LUMINAIRE		
DOUBLE HANDHOLE					
G.S. CONDUIT IN TRENCH OR PUSHED					
PEDESTRIAN PUSHBUTTON DETECTOR					
DETECTOR LOOP					
CAST IRON JUNCTION BOX					

RESTORATION OF THE WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, AND TRENCH AND BACKFILL, ETC.. SURFACES SUCH AS SHOULDERS, MEDIANS SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

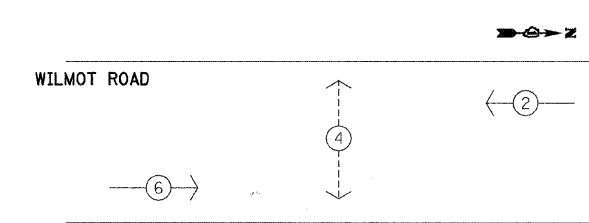
CABLE PLAN LEGEND

EXISTING	PROPOSED	
		8" (200mm) TRAFFIC SIGNAL SECTION
		12" (300mm) TRAFFIC SIGNAL SECTION
		12" (300mm) PEDESTRIAN SIGNAL SECTION
		12" (300mm) PEDESTRIAN SIGNAL SECTION WITH COUNTDOWN TIMER
		CONTROLLER CABINET
		SERVICE INSTALLATION
		TELEPHONE CONNECTION
		MAGNETIC DETECTOR
		EMERGENCY VEHICLE LIGHT DETECTOR
		CONFIRMATION BEACON
		PUSHBUTTON DETECTOR
		VEHICLE DETECTOR, INDUCTION LOOP
		DENOTES NUMBER OF CONDUCTORS, ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED.
		MICROWAVE VEHICLE SENSOR
		SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD
		RAILROAD CONTROL CABINET
		ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
		ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"
		GROUND ROD AT HANDHOLE (H), DOUBLE HANDHOLE (H), OR CONTROLLER (C).
		GROUND ROD AT POST (P), OR MAST ARM POLE (MA).
		GROUND ROD AT ELECTRIC SERVICE INSTALLATION
		GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)
		FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 MM12F SM12F



CABLE PLAN

CONTROLLER SEQUENCE



PHASE DESIGNATION DIAGRAM

LEGEND

- VEHICULAR MOVEMENT
- PEDESTRIAN MOVEMENT
- NUMBER REFERS TO ASSOCIATED PHASE

SUMMARY OF QUANTITIES

ITEM CODE	DESCRIPTION	UNIT	TOTAL
72000100	SIGN PANEL - TYPE 1	SQ FT	10.0
72400710	RELOCATE SIGN PANEL - TYPE 1	SQ FT	18.0
72900200	METAL POST - TYPE B	FOOT	27.0
X8050015	SERVICE INSTALLATION - POLE MOUNTED	EACH	1.0
81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	25.0
81000700	CONDUIT TRENCHED, 2 1/2" DIA, GALVANIZED STEEL	FOOT	80.0
81001000	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	14.0
81018900	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	35.0
81400100	HANDHOLE	EACH	2.0
81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	102.0
84500120	REMOVAL OF ELECTRIC SERVICE INSTALLATION	EACH	1.0
85700100	FULL-ACTUATED CONTROLLER AND TYPE III CABINET	EACH	1.0
86200200	UNINTERRUPTIBLE POWER SUPPLY (STANDARD)	EACH	1.0
87301215	ELECTRIC CABLE IN CONDUIT SIGNAL 14 2C	FOOT	140.0
87301225	ELECTRIC CABLE IN CONDUIT SIGNAL 14 3C	FOOT	160.0
87301245	ELECTRIC CABLE IN CONDUIT SIGNAL 14 5C	FOOT	480.0
87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO.6 2C	FOOT	30.0
X8730027	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO.6 1C	FOOT	170.0
87502460	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 12 FT	EACH	2.0
87700160	STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.	EACH	2.0
87800100	CONCRETE FOUNDATION, TYPE A	FOOT	8.0
87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4.0
87800400	CONCRETE FOUNDATION, TYPE E, 30" DIAMETER	FOOT	30.0
88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4.0
88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2.0
88200110	TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	4.0
88800100	PEDESTRIAN PUSH BUTTON	EACH	2.0
89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1.0
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1.0
89502380	REMOVE EXISTING HANDHOLE	EACH	2.0
89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	5.0

NOTE:
EQUIPMENT GROUND CONDUCTOR (GREEN COLOR CODED) SPLICE TO FRAME AND COVER IS REQUIRED FOR ALL HANDHOLES OR DOUBLE HANDHOLES THAT CARRY SIGNAL CABLES AND SERVICE CABLES.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	%OPERATION	
SIGNAL (RED)	4	135	17	0.50	34.00
(YELLOW)	4	135	25	0.25	25.00
(GREEN)	4	135	15	0.25	15.00
ARROW		135	12	0.10	
PED. SIGNAL	2	135	25	1.00	50
CONTROLLER	1	100	100	1.00	100
TOTAL =					224.00

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	ALL FOUNDATIONS	FT. (m)
TYPE A - POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS	3.5 (1.0)
D - CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	20'±L-2'
E - M. ARM POLE		SIGNAL POST	2 (1.0)		6m±L-0.6m±
24" (600mm)	10 (3.0)	CONTROLLER CAB.	1 (0.5)	BRACKET MOUNTED	13 (4.0)
30" (750mm)	15 (4.6)	FIBER OPTIC	13 (4.0)	PED. PUSHBUTTON	4 (1.2)
		ELECTRIC SERVICE	1 (0.5)	ELECTRIC SERVICE	13.5 (4.1)
		GROUND CABLE	1 (0.5)	SERVICE TO GROUND	13.5 (4.1)
				POST MOUNTED	6 (1.8)

ENERGY COSTS TO:
VILLAGE OF DEERFIELD
850 WALKEGAN ROAD
DEERFIELD IL 60015
ENERGY SUPPLY CONTACT: JOHN GLYNN
PHONE: (866) 639-3532
COMPANY: EXCELON BSC IT

USER NAME = ogutowski
DESIGNED - JCC
DRAWN - JCC
CHECKED - CLG
DATE - 1-30-2009
PLOT SCALE = 1/30/2009
PLOT DATE = 1/30/2009
FILE NAME = n:\proj\3328\phase1\design\Traffic Analysis\Ped Crossing\3328-PEDSIG2.dgn

REVISIONS:
REVISED -
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REVISED -

**VILLAGE OF DEERFIELD
WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)**

SCHEDULE OF QUANTITIES, CABLE PLAN AND PHASE DESIGNATION DIAGRAM

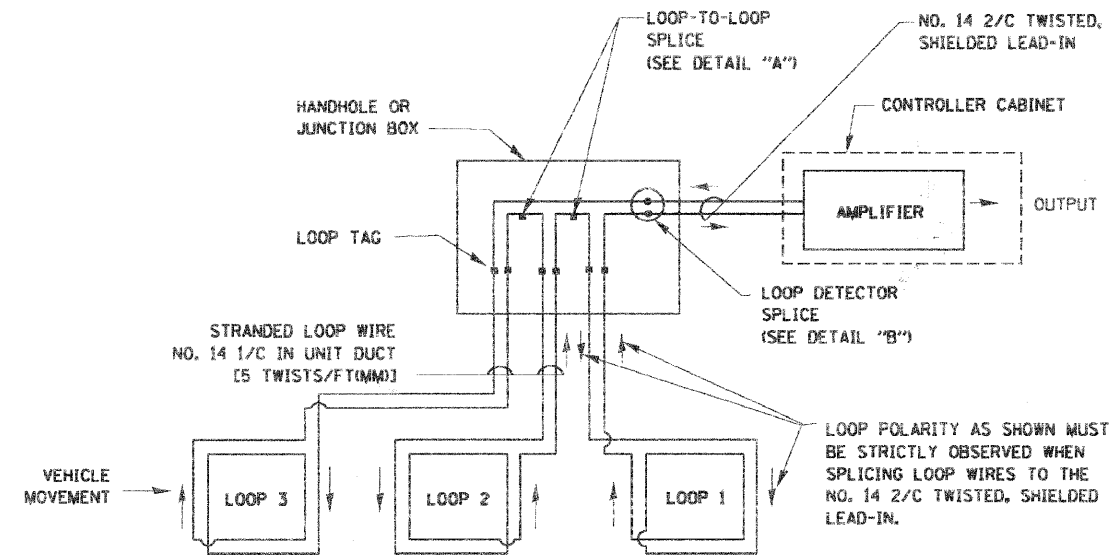
SCALE: SHEET NO. OF 55 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2718	07-00080-00-WR	LAKE	55	29

CONTRACT NO. 63124
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

LOOP DETECTOR NOTES

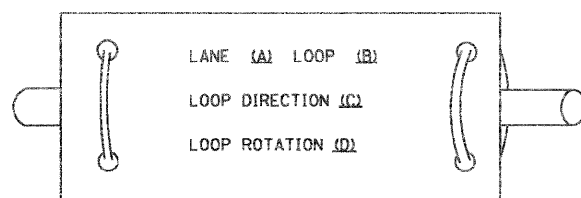
1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.



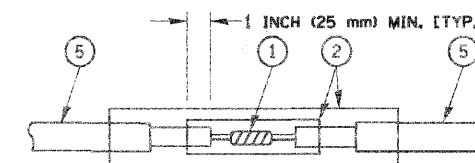
DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

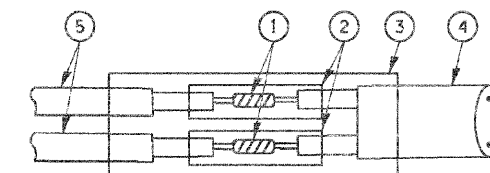
LOOP LEAD-IN CABLE TAG



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

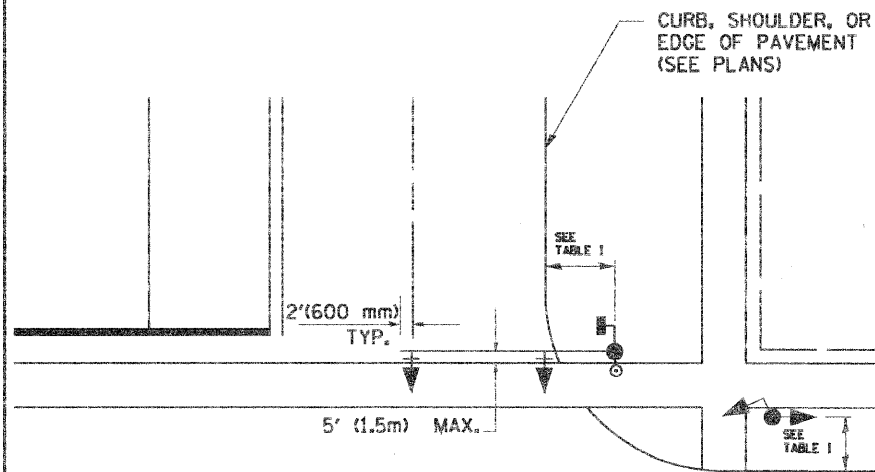
LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

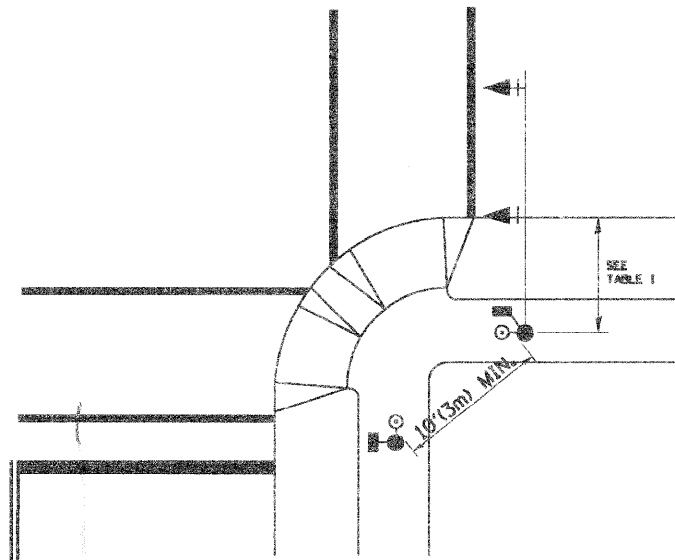
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	PLOT SCALE = 5/8,0000 "/> <td>CHECKED - D.A.Z.</td> <td>REVISED -</td> <td>SCALE: NONE</td> <td>SHEET NO. 1 OF 4 SHEETS</td> <td>STA. TO STA.</td> <td colspan="4">FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</td>	CHECKED - D.A.Z.	REVISED -			SCALE: NONE	SHEET NO. 1 OF 4 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	
	PLOT DATE = 1/4/2008	DATE = 05-30-00	REVISED -			CONTRACT NO. 63124				

TRAFFIC SIGNAL MAST ARM AND POST

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA, INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



PEDESTRIAN SIGNAL PUSHBUTTON



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

NOTES:

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.
 AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.
 PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:
 A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL-WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.
 B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.
 C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
 D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).
 E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK.
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

PEDESTRIAN SIGNAL POST

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

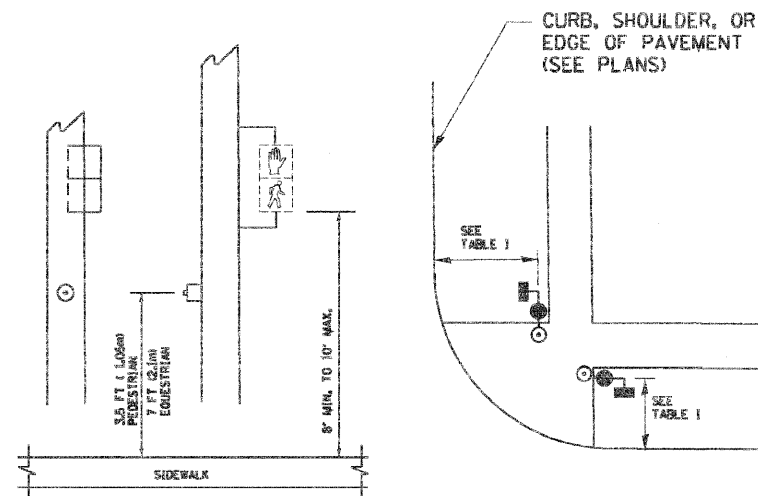


TABLE I

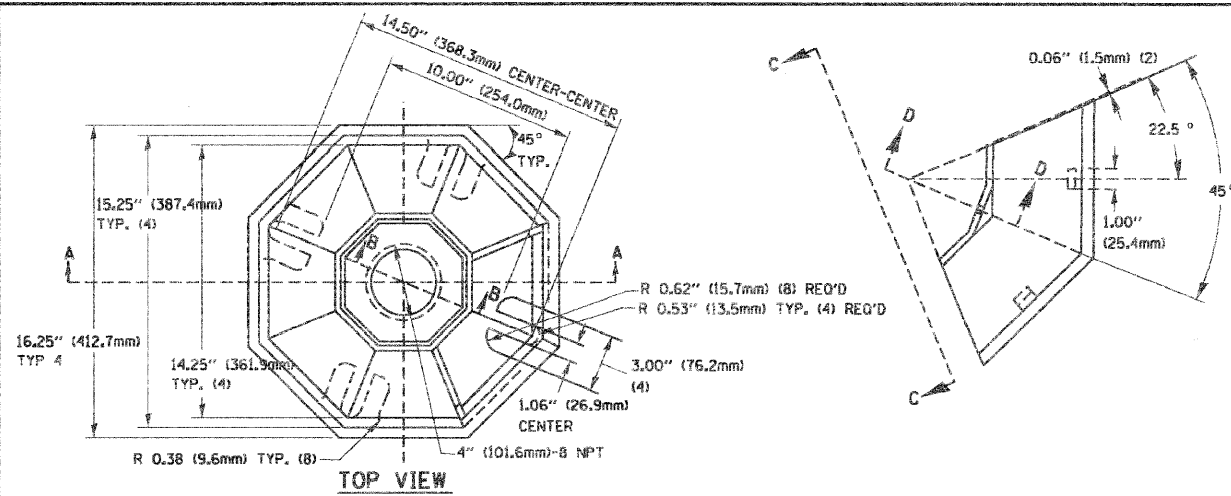
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB)	SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN PUSHBUTTON	SEE NOTE 1	SEE NOTE 1

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		DATE -	REVISED -

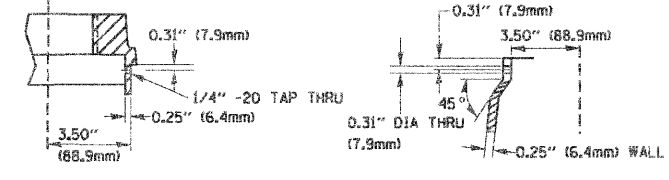
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**
SCALE: NONE SHEET NO. 2 OF 4 SHEETS STA. TO STA.

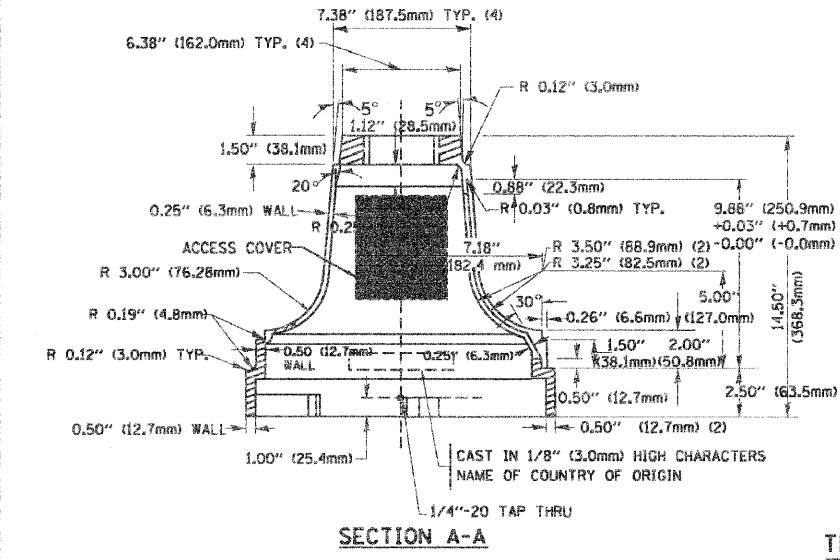
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2718	07-0080-00-WR	LAKE	55	31
TS-05		CONTRACT NO. 63124		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



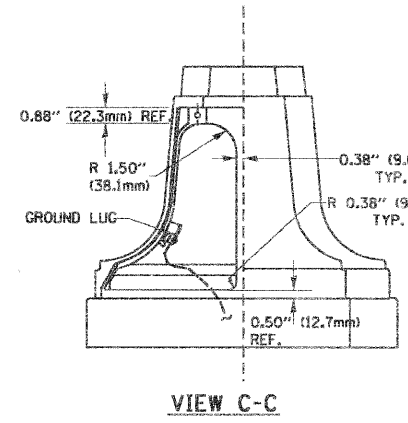
SECTION B-B



SECTION D-D

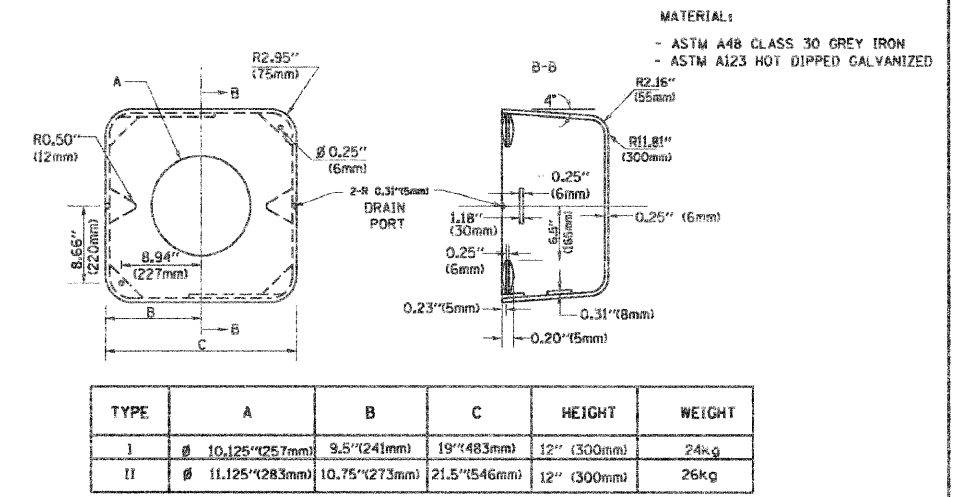


SECTION A-A



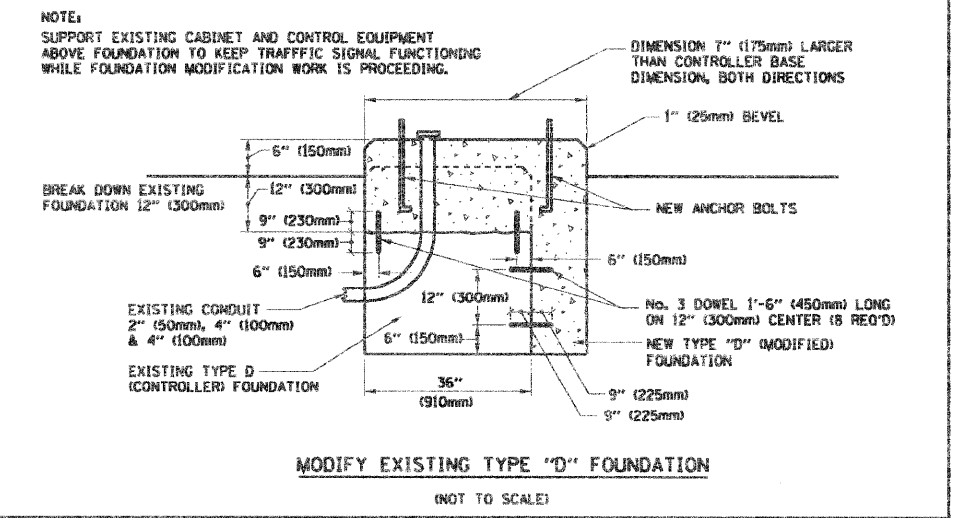
VIEW C-C

TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



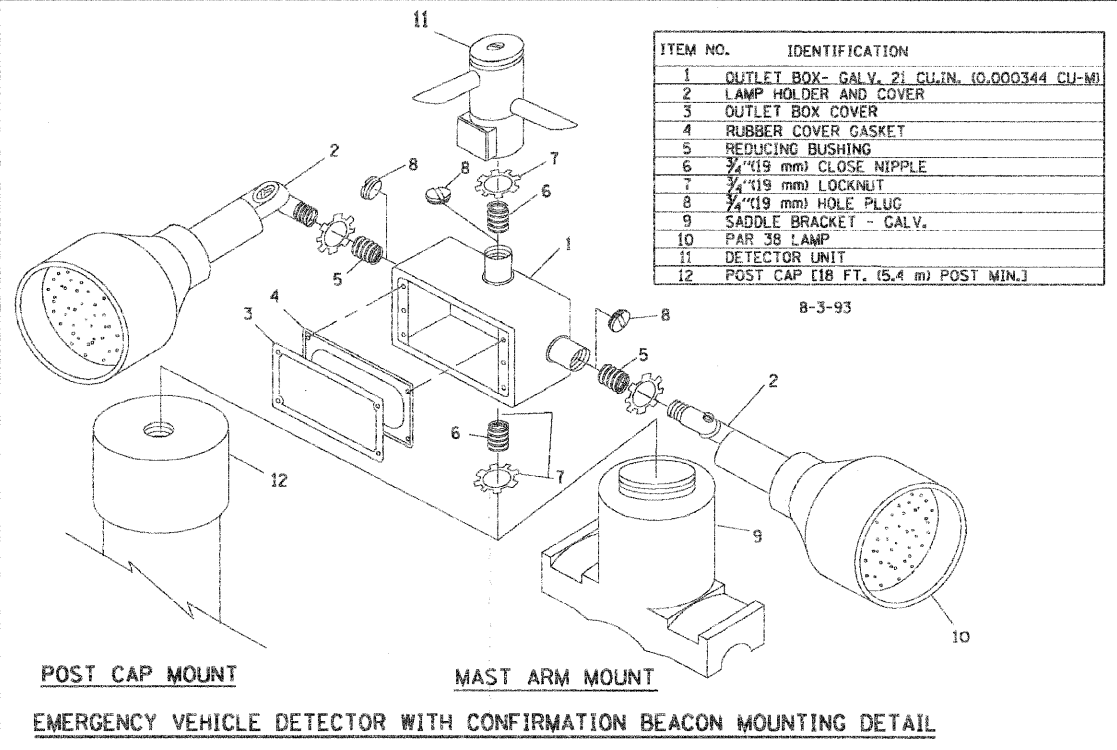
TYPE	A	B	C	HEIGHT	WEIGHT
I	Ø 10.125\" (257.7mm)	9.5\" (241mm)	19\" (483mm)	12\" (300mm)	24kg
II	Ø 11.125\" (283.3mm)	10.75\" (273.3mm)	21.5\" (546mm)	12\" (300mm)	26kg

SHROUD DETAIL



MODIFY EXISTING TYPE "D" FOUNDATION

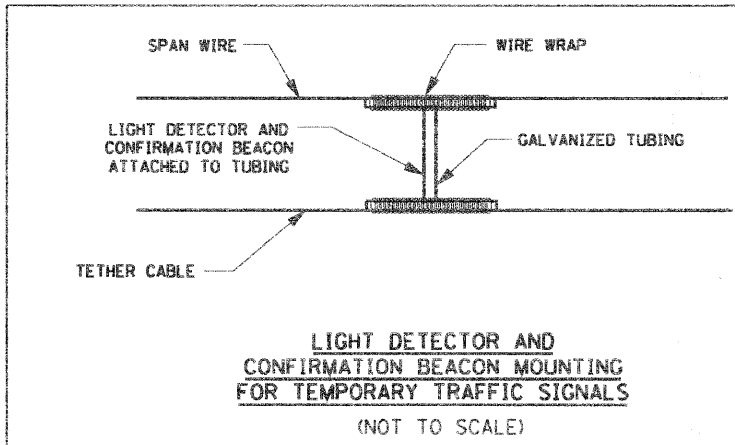
(NOT TO SCALE)



ITEM NO.	IDENTIFICATION
1	OUTLET BOX - GALV. 21 CU. IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\" (19 mm) CLOSE NIPPLE
7	3/4\" (19 mm) LOCKNUT
8	3/4\" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	PAR 38 LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

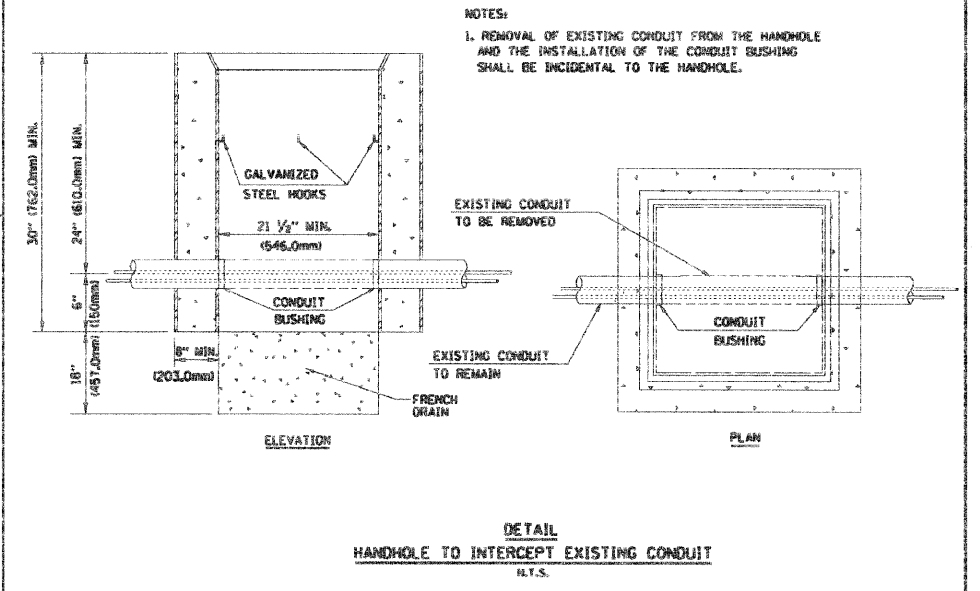
NOTES:

- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



LIGHT DETECTOR AND CONFIRMATION BEACON MOUNTING FOR TEMPORARY TRAFFIC SIGNALS

(NOT TO SCALE)



DETAIL HANDHOLE TO INTERCEPT EXISTING CONDUIT

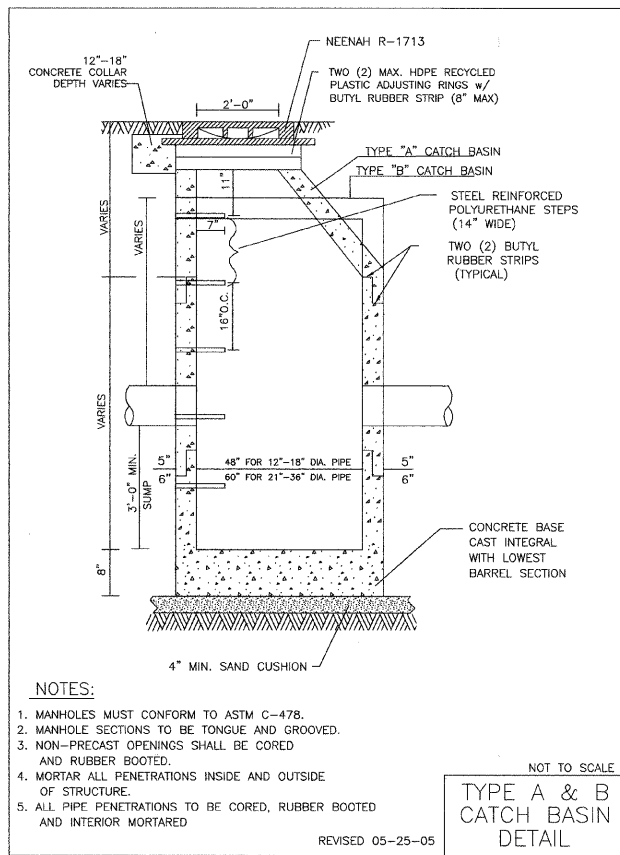
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		DRAWN - R.W.P.	REVISED - BUR,TRAFFIC 11-12-01
		CHECKED - D.A.Z.	REVISED - BUR,TRAFFIC 01-01-02
		DATE - 05-30-00	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS
SCALE: NONE SHEET NO. 4 OF 4 SHEETS STA. TO STA.

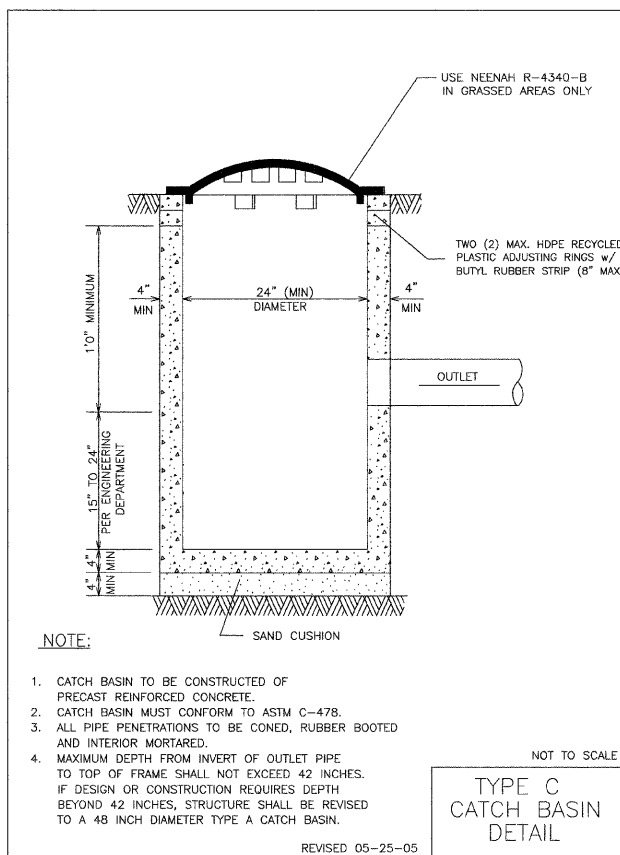
F.A. RTE. 2718	SECTION 07-0080-00-WR	COUNTY LAKE	TOTAL SHEETS 55	SHEET NO. 33
TS-05		CONTRACT NO. 63124		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- NOTES:**
1. MANHOLES MUST CONFORM TO ASTM C-478.
 2. MANHOLE SECTIONS TO BE TONGUE AND GROOVED.
 3. NON-PRECAST OPENINGS SHALL BE CORED AND RUBBER BOOTED.
 4. MORTAR ALL PENETRATIONS INSIDE AND OUTSIDE OF STRUCTURE.
 5. ALL PIPE PENETRATIONS TO BE CORED, RUBBER BOOTED AND INTERIOR MORTARED.

NOT TO SCALE
**TYPE A & B
CATCH BASIN
DETAIL**

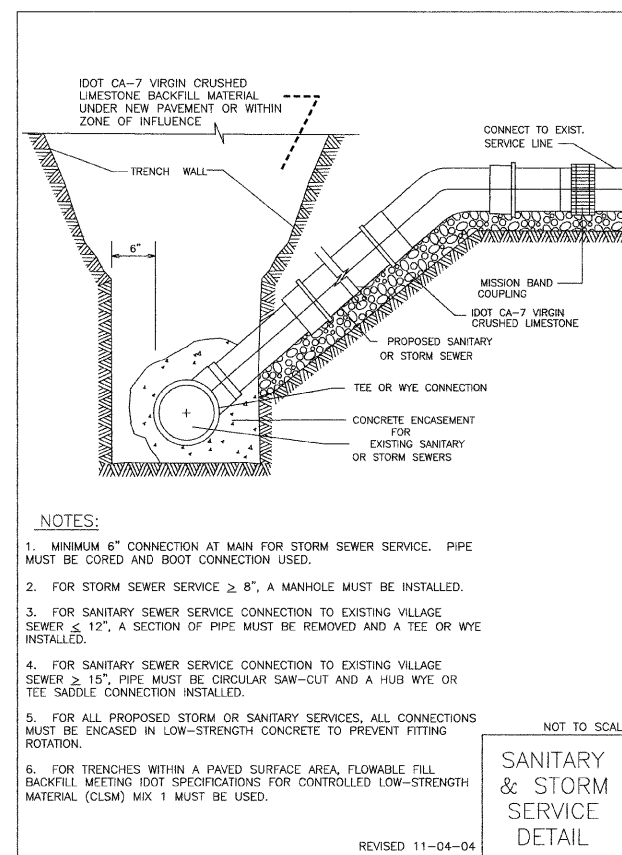
REVISED 05-25-05



- NOTE:**
1. CATCH BASIN TO BE CONSTRUCTED OF PRECAST REINFORCED CONCRETE.
 2. CATCH BASIN MUST CONFORM TO ASTM C-478.
 3. ALL PIPE PENETRATIONS TO BE CONED, RUBBER BOOTED AND INTERIOR MORTARED.
 4. MAXIMUM DEPTH FROM INVERT OF OUTLET PIPE TO TOP OF FRAME SHALL NOT EXCEED 42 INCHES. IF DESIGN OR CONSTRUCTION REQUIRES DEPTH BEYOND 42 INCHES, STRUCTURE SHALL BE REVISED TO A 48 INCH DIAMETER TYPE A CATCH BASIN.

NOT TO SCALE
**TYPE C
CATCH BASIN
DETAIL**

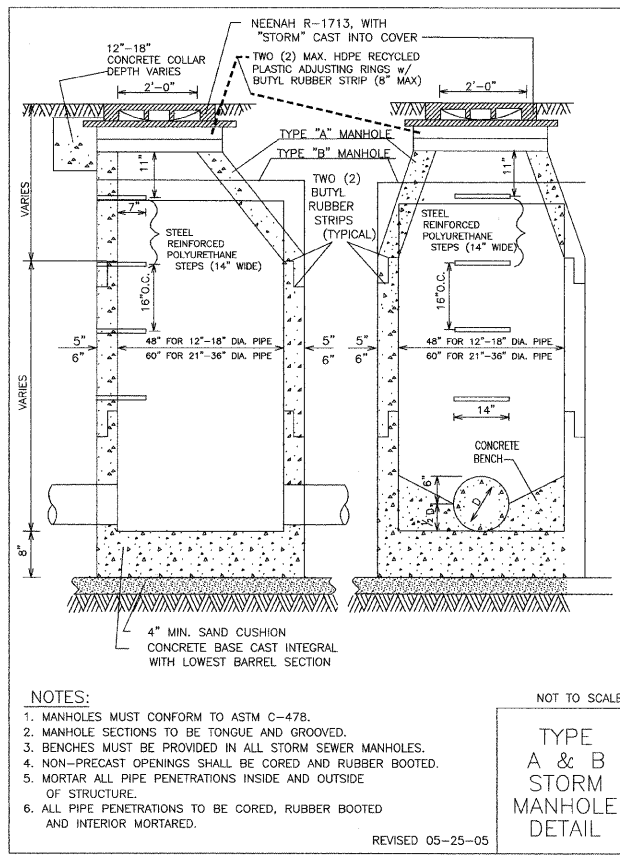
REVISED 05-25-05



- NOTES:**
1. MINIMUM 6" CONNECTION AT MAIN FOR STORM SEWER SERVICE. PIPE MUST BE CORED AND BOOT CONNECTION USED.
 2. FOR STORM SEWER SERVICE $\geq 8"$, A MANHOLE MUST BE INSTALLED.
 3. FOR SANITARY SEWER SERVICE CONNECTION TO EXISTING VILLAGE SEWER $\leq 12"$, A SECTION OF PIPE MUST BE REMOVED AND A TEE OR WYE INSTALLED.
 4. FOR SANITARY SEWER SERVICE CONNECTION TO EXISTING VILLAGE SEWER $\geq 15"$, PIPE MUST BE CIRCULAR SAW-CUT AND A HUB WYE OR TEE SADDLE CONNECTION INSTALLED.
 5. FOR ALL PROPOSED STORM OR SANITARY SERVICES, ALL CONNECTIONS MUST BE ENCASED IN LOW-STRENGTH CONCRETE TO PREVENT FITTING ROTATION.
 6. FOR TRENCHES WITHIN A PAVED SURFACE AREA, FLOWABLE FILL BACKFILL MEETING IDOT SPECIFICATIONS FOR CONTROLLED LOW-STRENGTH MATERIAL (CLSM) MIX 1 MUST BE USED.

NOT TO SCALE
**SANITARY
& STORM
SERVICE
DETAIL**

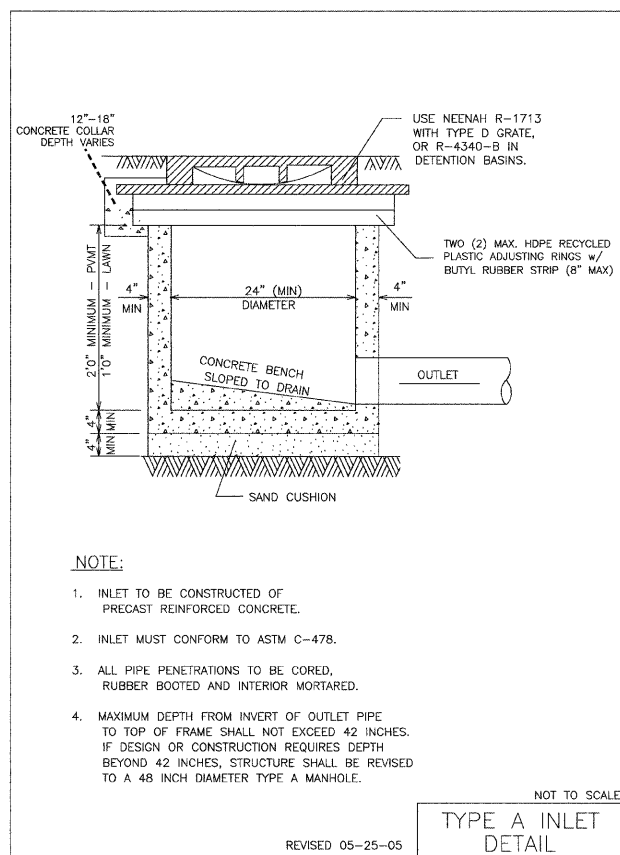
REVISED 11-04-04



- NOTES:**
1. MANHOLES MUST CONFORM TO ASTM C-478.
 2. MANHOLE SECTIONS TO BE TONGUE AND GROOVED.
 3. BENCHES MUST BE PROVIDED IN ALL STORM SEWER MANHOLES.
 4. NON-PRECAST OPENINGS SHALL BE CORED AND RUBBER BOOTED.
 5. MORTAR ALL PIPE PENETRATIONS INSIDE AND OUTSIDE OF STRUCTURE.
 6. ALL PIPE PENETRATIONS TO BE CORED, RUBBER BOOTED AND INTERIOR MORTARED.

NOT TO SCALE
**TYPE
A & B
STORM
MANHOLE
DETAIL**

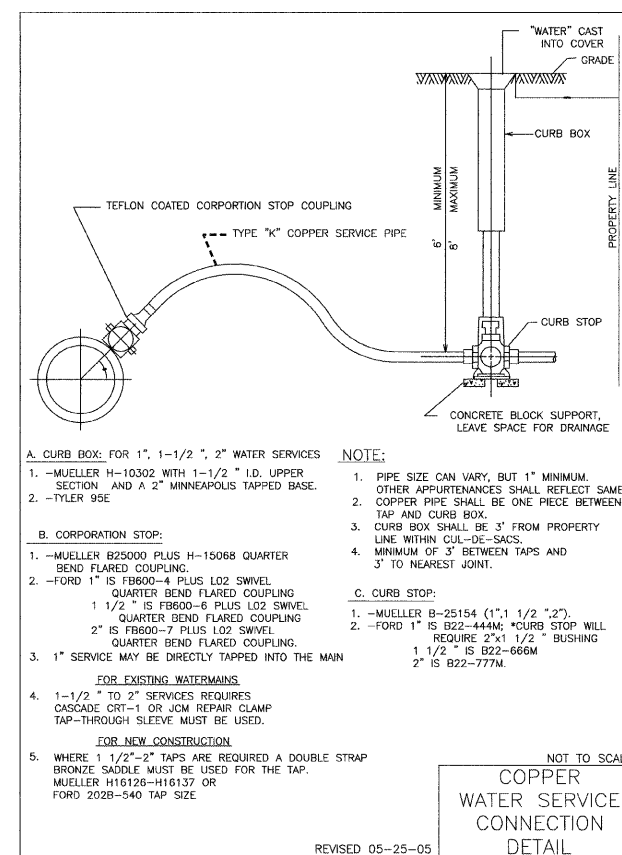
REVISED 05-25-05



- NOTE:**
1. INLET TO BE CONSTRUCTED OF PRECAST REINFORCED CONCRETE.
 2. INLET MUST CONFORM TO ASTM C-478.
 3. ALL PIPE PENETRATIONS TO BE CORED, RUBBER BOOTED AND INTERIOR MORTARED.
 4. MAXIMUM DEPTH FROM INVERT OF OUTLET PIPE TO TOP OF FRAME SHALL NOT EXCEED 42 INCHES. IF DESIGN OR CONSTRUCTION REQUIRES DEPTH BEYOND 42 INCHES, STRUCTURE SHALL BE REVISED TO A 48 INCH DIAMETER TYPE A MANHOLE.

NOT TO SCALE
**TYPE A INLET
DETAIL**

REVISED 05-25-05

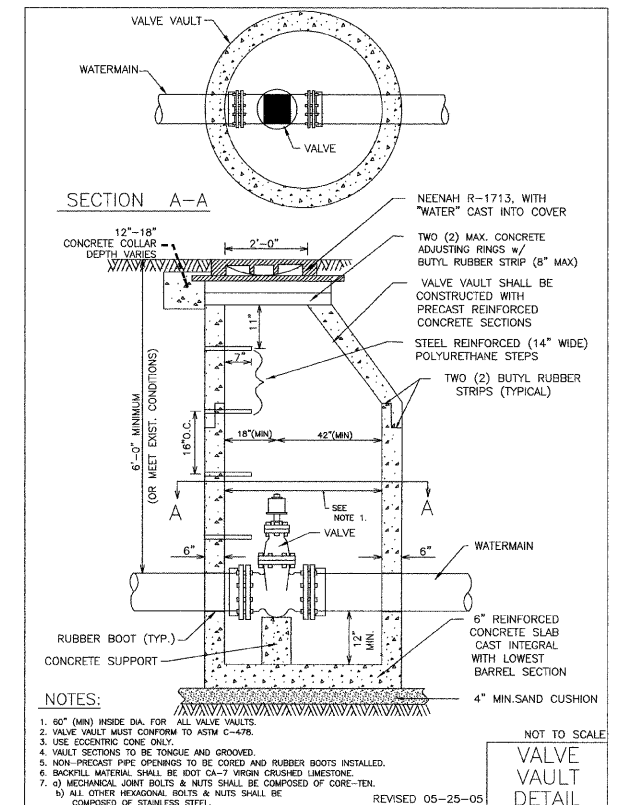
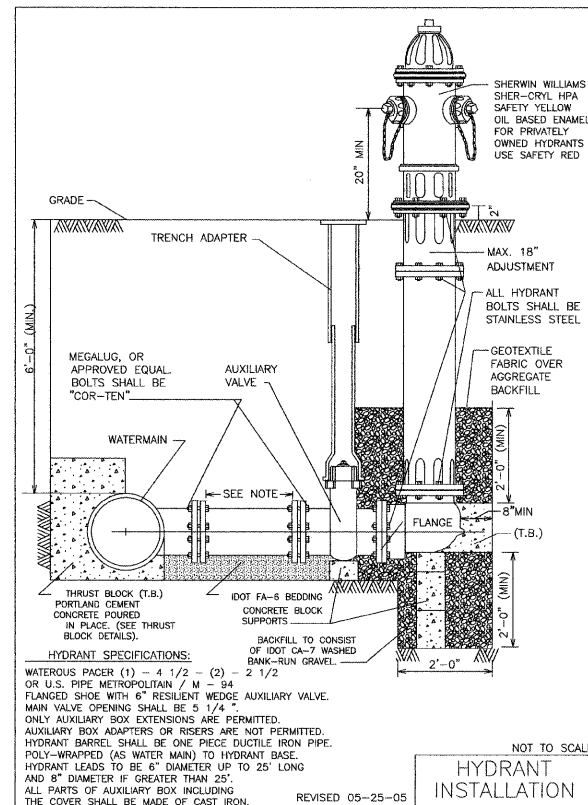
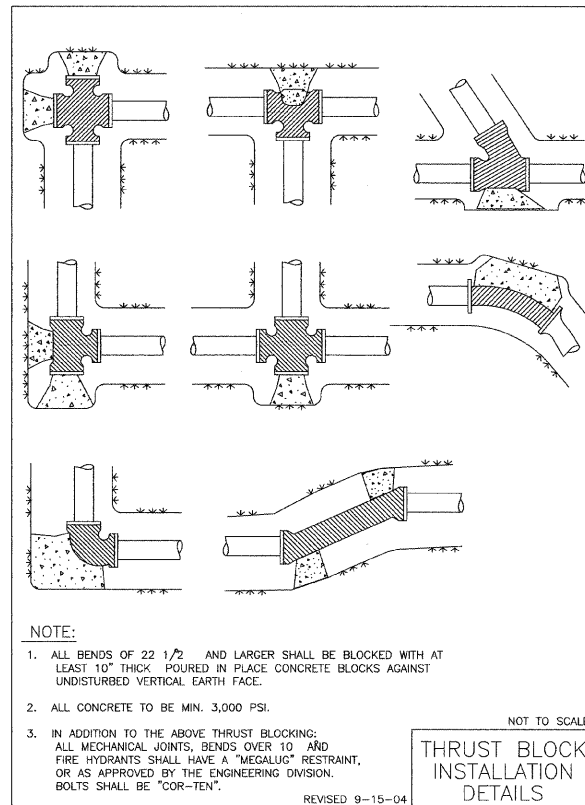
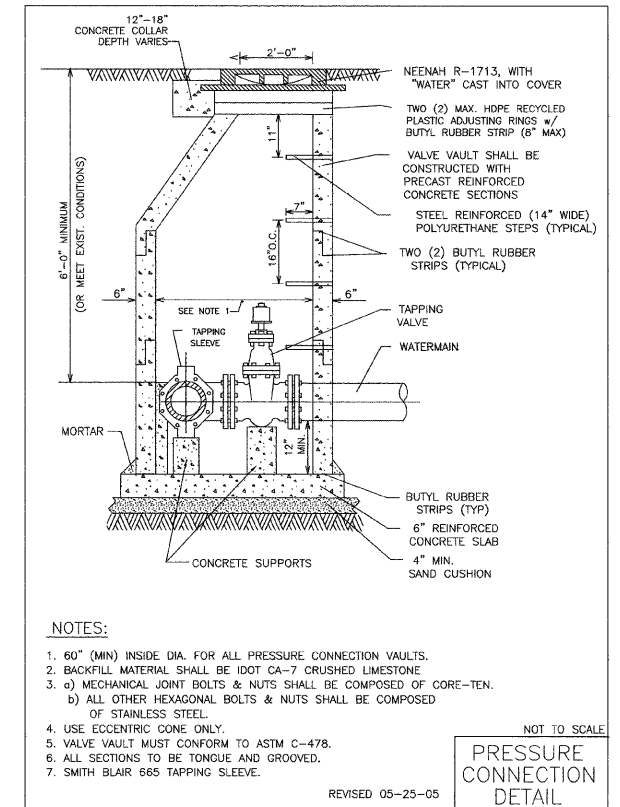
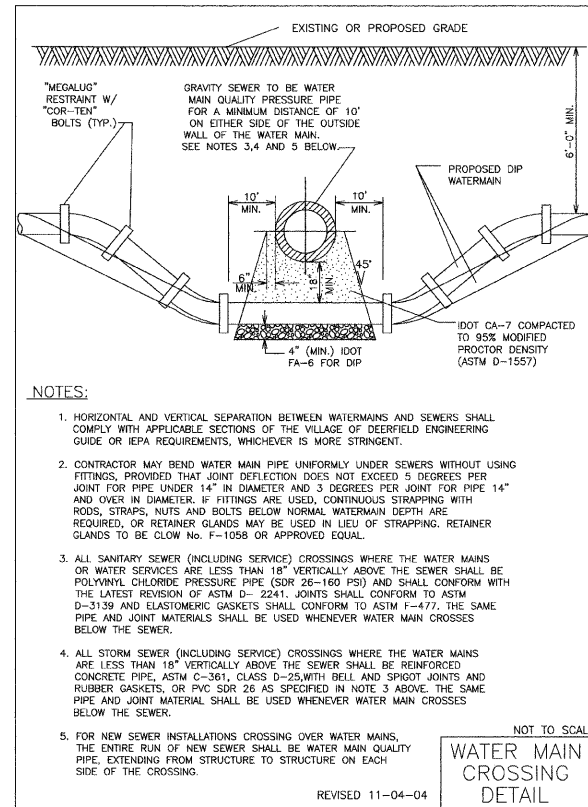


- A. CURB BOX:** FOR 1", 1-1/2", 2" WATER SERVICES
1. -MUELLER H-10302 WITH 1-1/2" I.D. UPPER SECTION AND A 2" MINNEAPOLIS TAPPED BASE.
 2. -TYLER 95E
- B. CORPORATION STOP:**
1. -MUELLER B25000 PLUS H-15068 QUARTER BEND FLARED COUPLING.
 2. -FORD 1" IS FB600-4 PLUS L02 SWIVEL QUARTER BEND FLARED COUPLING
 - 1 1/2" IS FB600-6 PLUS L02 SWIVEL QUARTER BEND FLARED COUPLING
 - 2" IS FB600-7 PLUS L02 SWIVEL QUARTER BEND FLARED COUPLING.
- C. CURB STOP:**
1. -MUELLER B-25154 (1" 1/2" 2").
 2. -FORD 1" IS B22-444M; CURB STOP WILL REQUIRE 2"x1 1/2" BUSHING
 - 1 1/2" IS B22-666M
 - 2" IS B22-777M
- FOR EXISTING WATERMANS**
1. 1-1/2" TO 2" SERVICES REQUIRES CASCADE CRT-1 OR ICM REPAIR CLAMP TAP-THROUGH SLEEVE MUST BE USED.
- FOR NEW CONSTRUCTION**
5. WHERE 1 1/2"-2" TAPS ARE REQUIRED A DOUBLE STRAP BRONZE SADDLE MUST BE USED FOR THE TAP. MUELLER H16128-H16137 OR FORD 2028-540 TAP SIZE

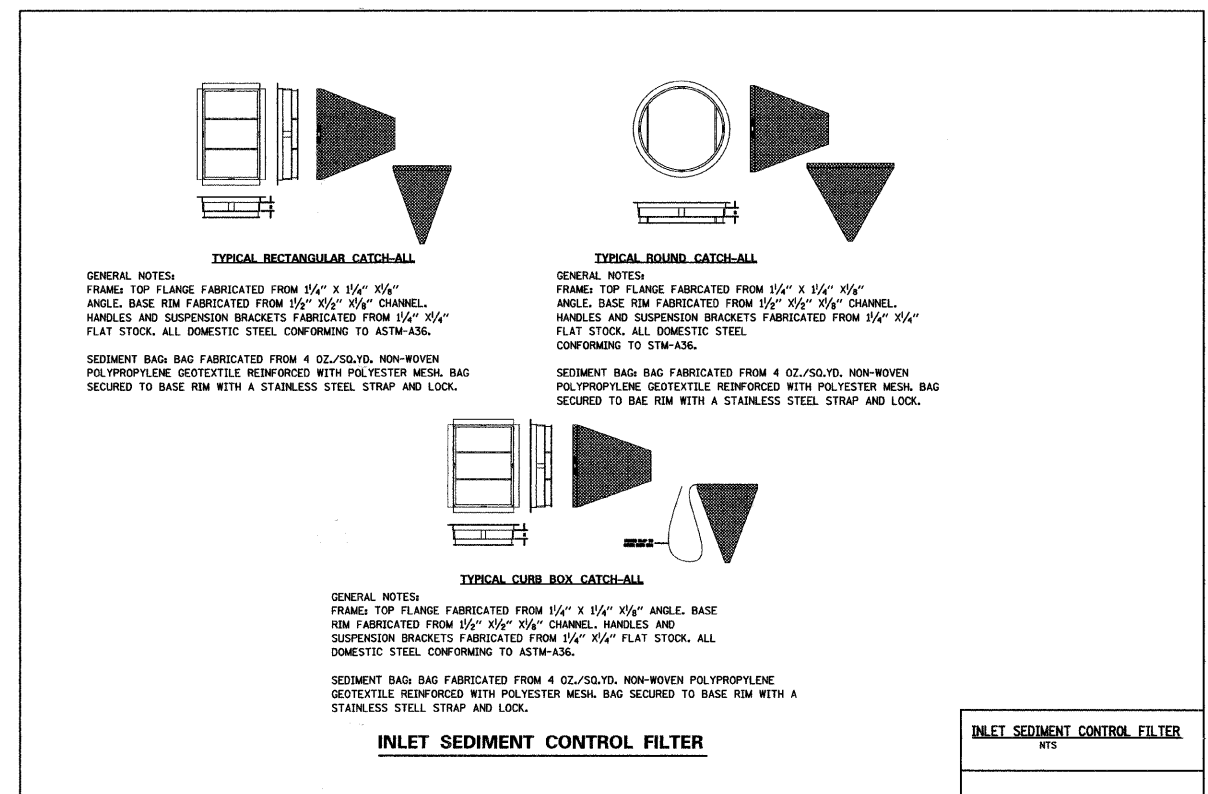
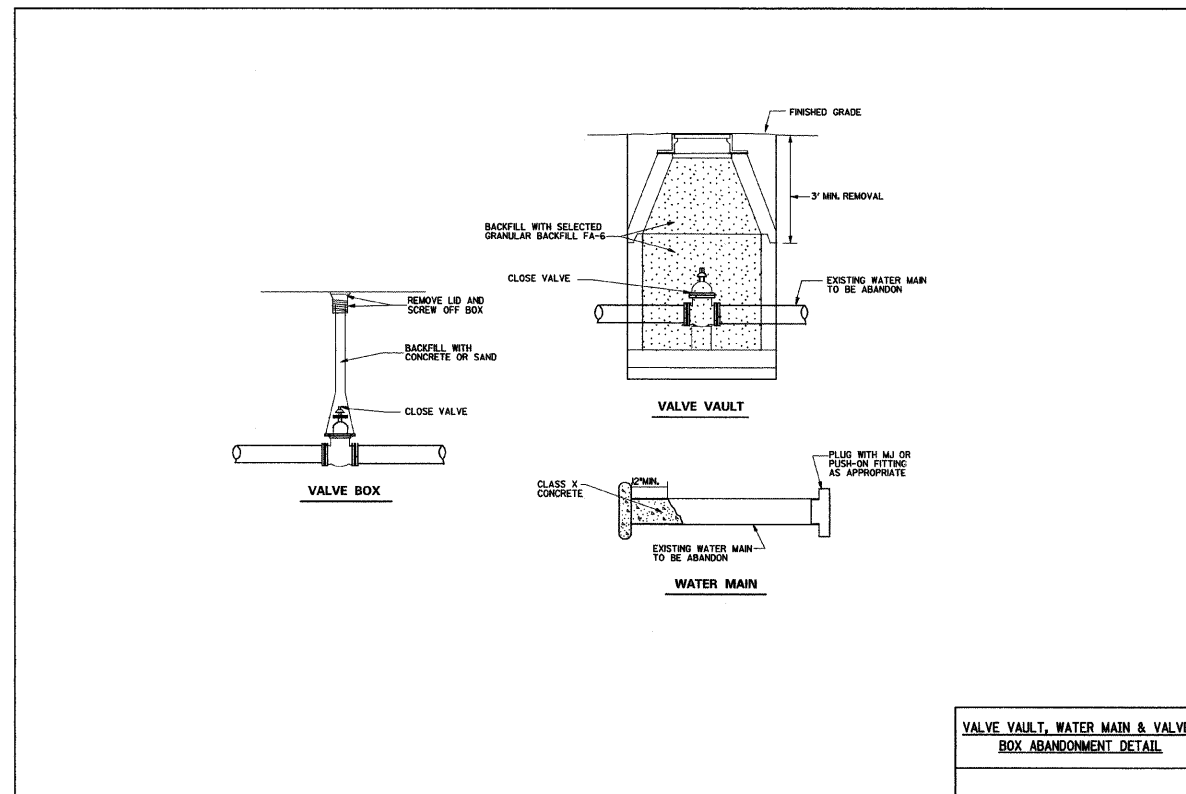
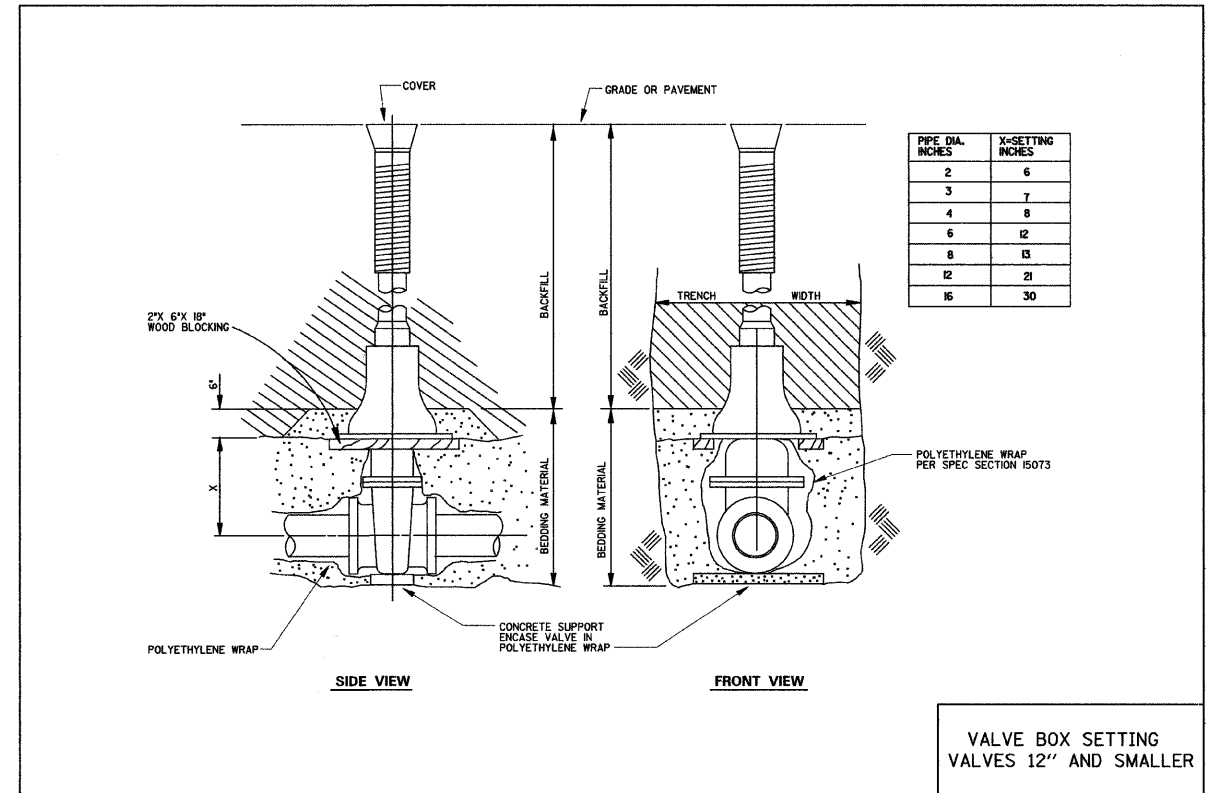
NOT TO SCALE
**COPPER
WATER SERVICE
CONNECTION
DETAIL**

REVISED 05-25-05

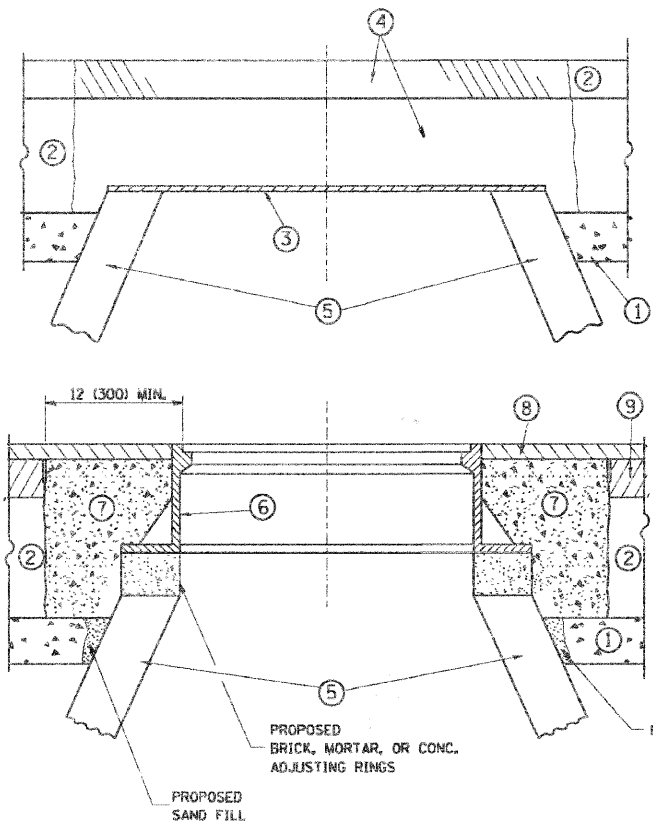
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	PLOT SCALE = 0.9800 / IN.	DRAWN - AL	REVISED -			2718	07-00080-00-WR	LAKE	55	34
	PLOT DATE = 1/30/2009	CHECKED - CLG	REVISED -			CONTRACT NO. 63124				
	DATE - 1-30-2009	REVISED -		SCALE:	SHEET NO. 35 OF 55 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		



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	PLOT DATE = 1/30/2009	CHECKED - CLG	REVISED -	WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)	SCALE:	SHEET NO. 39 OF 55 SHEETS STA. TO STA.		CONTRACT NO. 63124		
		DATE - 1-30-2009	REVISED -			FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



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	PLOT DATE = 1/30/2009	CHECKED - CLG	REVISED -			CONTRACT NO. 63124				
	DATE - 1-30-2009	REVISED -		SCALE:	SHEET NO. 40 OF 55 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS S1 CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

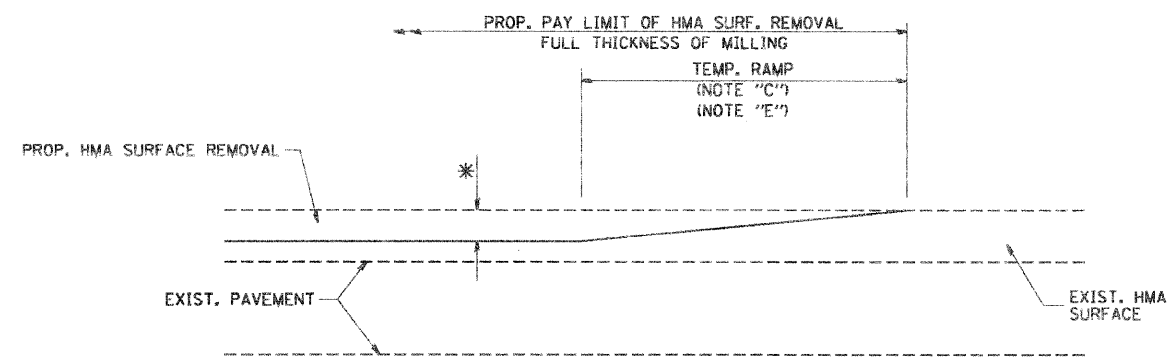
THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

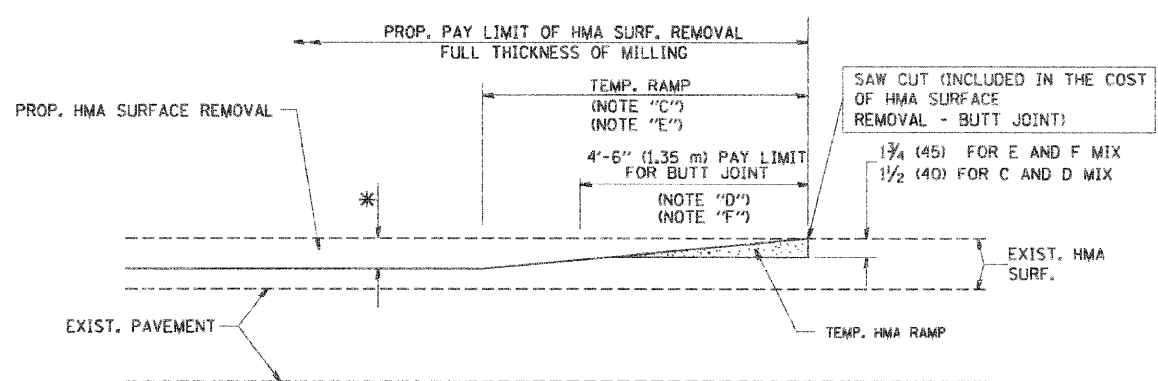
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\dvs\test\22x34\bd88.dgn	USER NAME = goglianobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	2718	07-00080-00-WR	LAKE	55 37
		PLOT SCALE = 50.0000' / IN.	REVISED - R. WIEDEMAN 05-14-04							90600-03 (BD-8)		CONTRACT NO. 63124
		PLOT DATE = 1/4/2000	REVISED - R. BORO 01-01-07						FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT			



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

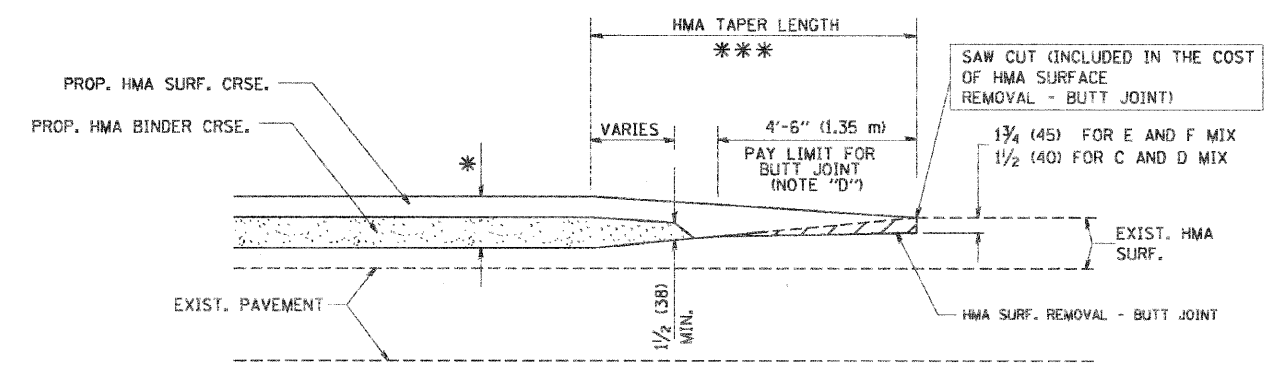
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

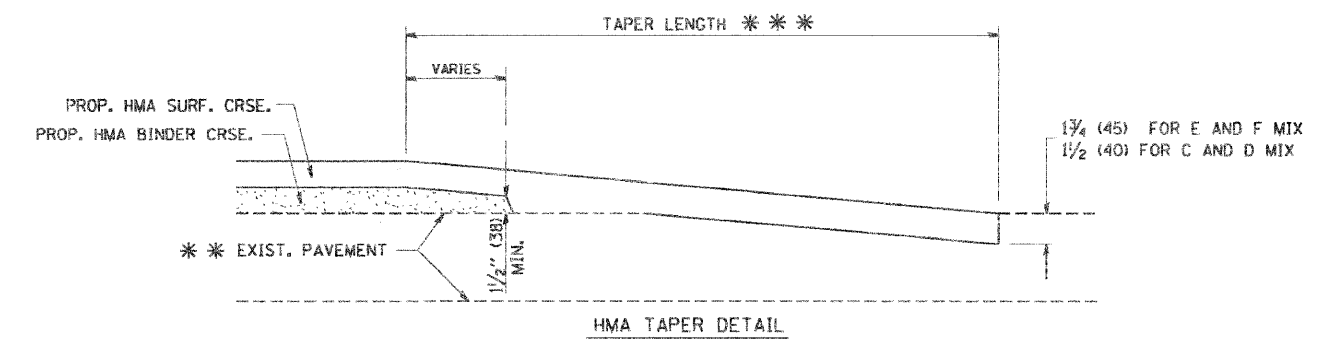
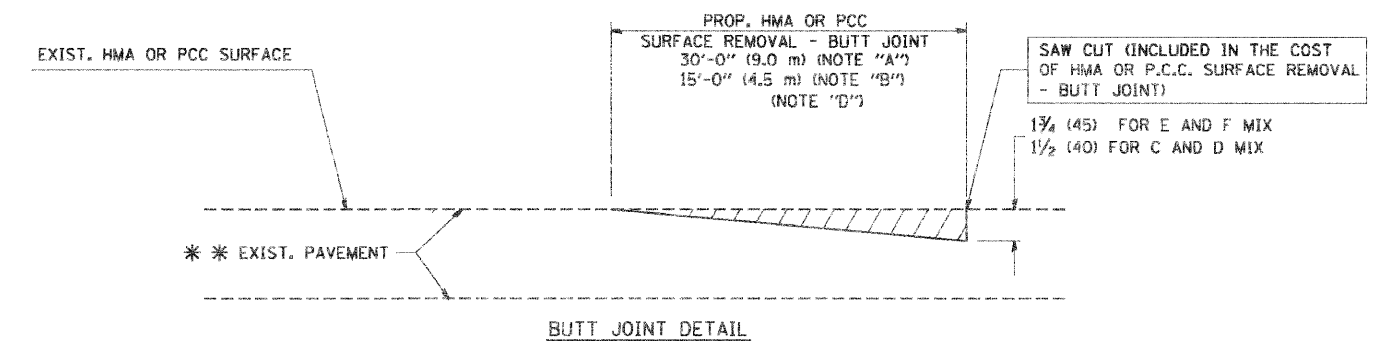
OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

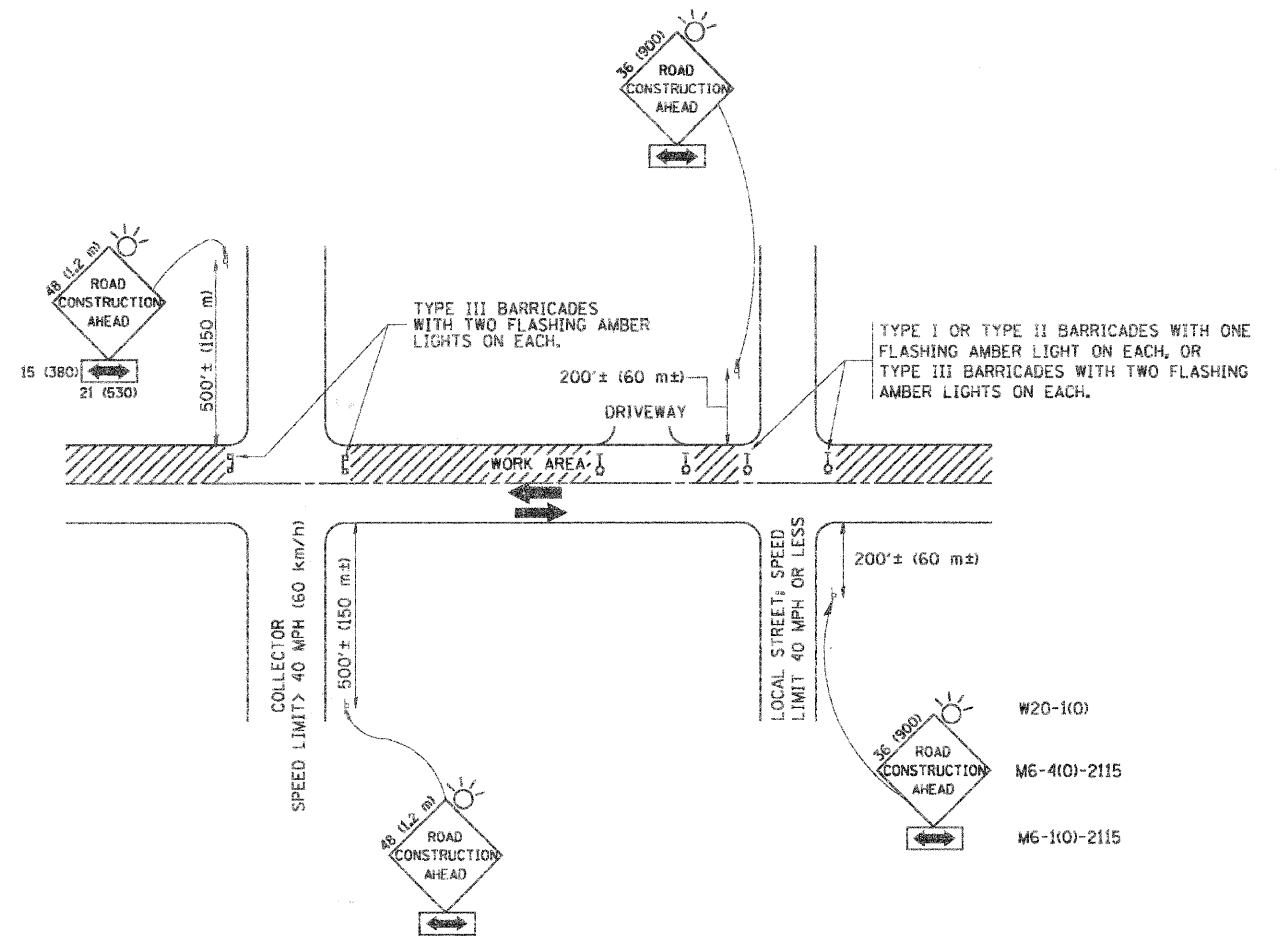
- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PLOT SCALE = 5/8" = 1' IN.	CHECKED -	DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	BD400-05 BD32		CONTRACT NO. 63124		
PLOT DATE = 1/4/2008	DATE = 06-13-90	REVISOR - M. GOMEZ 04-06-01	REVISED - R. BORO 01-01-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

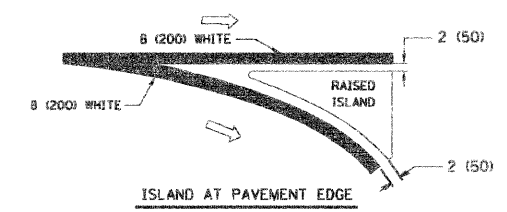
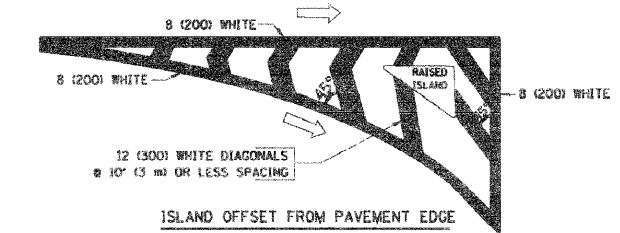
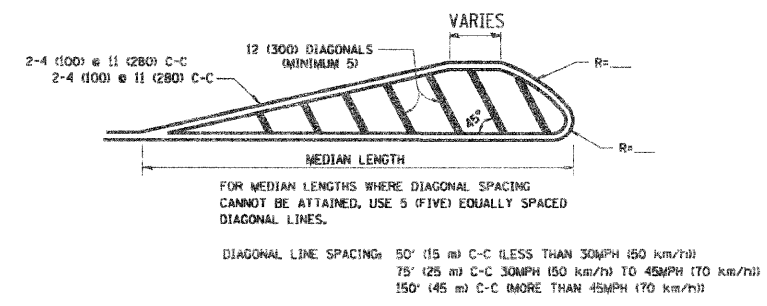
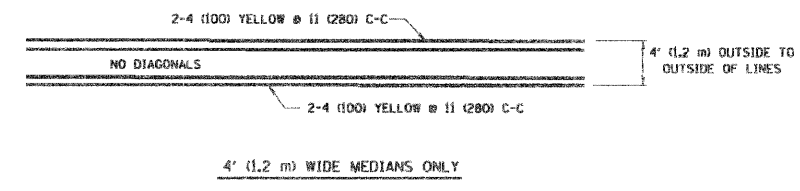
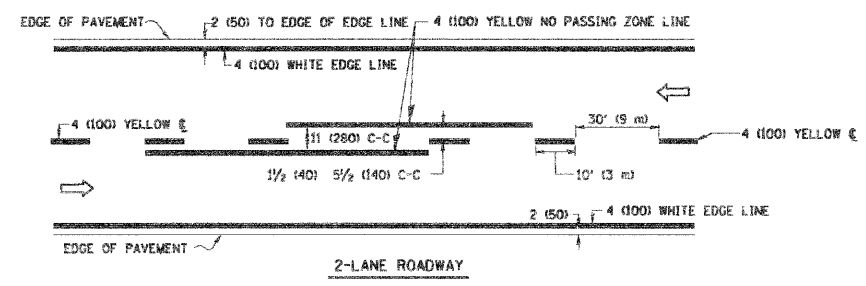
NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

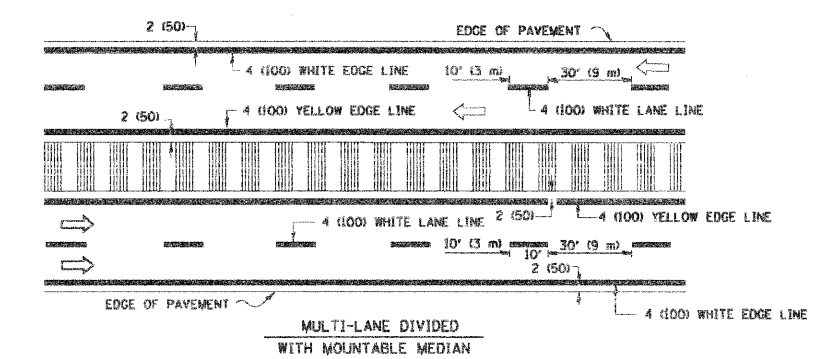
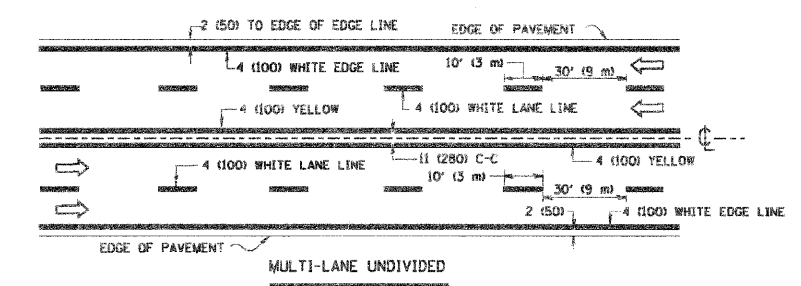
USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
 - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
 - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

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	PLOT SCALE = 50.000' / IN.	DRAWN - -	REVISOR - A. HOUSEH 03-06-96		2718	07-00080-00-WR	LAKE	55	39		
	PLOT DATE = 1/4/2008	CHECKED - -	REVISOR - A. HOUSEH 10-15-96		TC-10		CONTRACT NO. 63124				
		DATE - 06-89	REVISOR - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

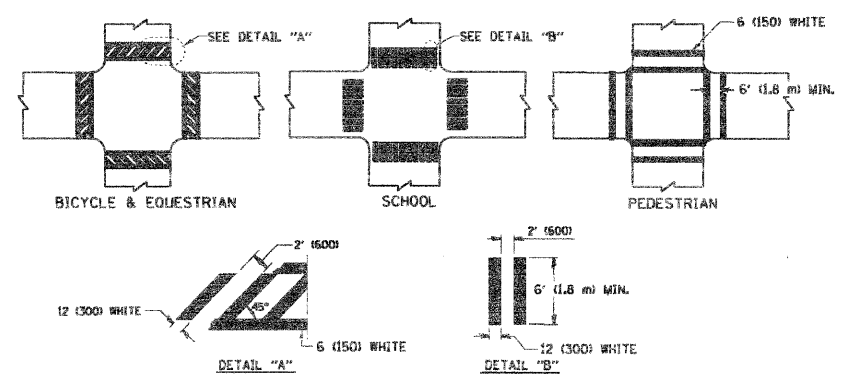


TYPICAL ISLAND MARKING

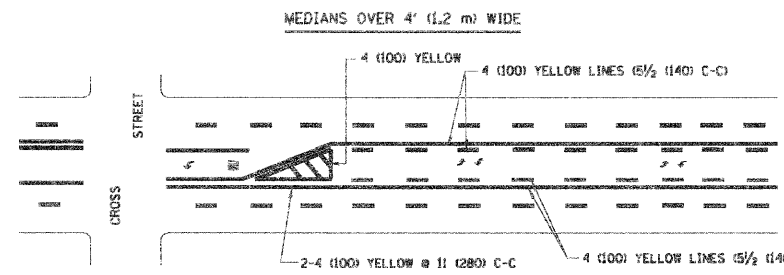


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

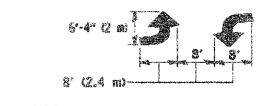
TYPICAL LANE AND EDGE LINE MARKING



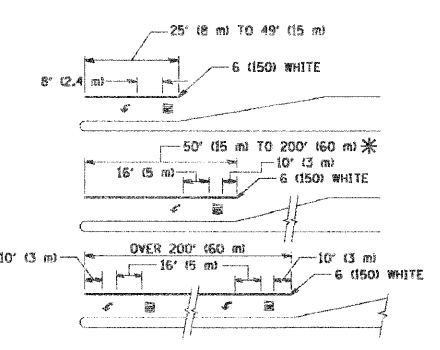
TYPICAL CROSSWALK MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
* AREA = 15.6 SQ. FT. (1.5 m²) | AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

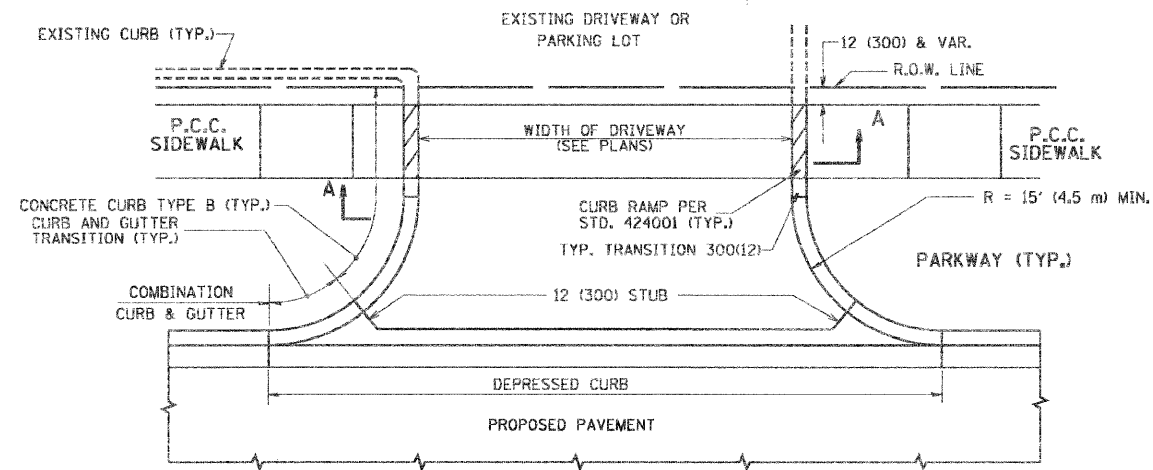
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

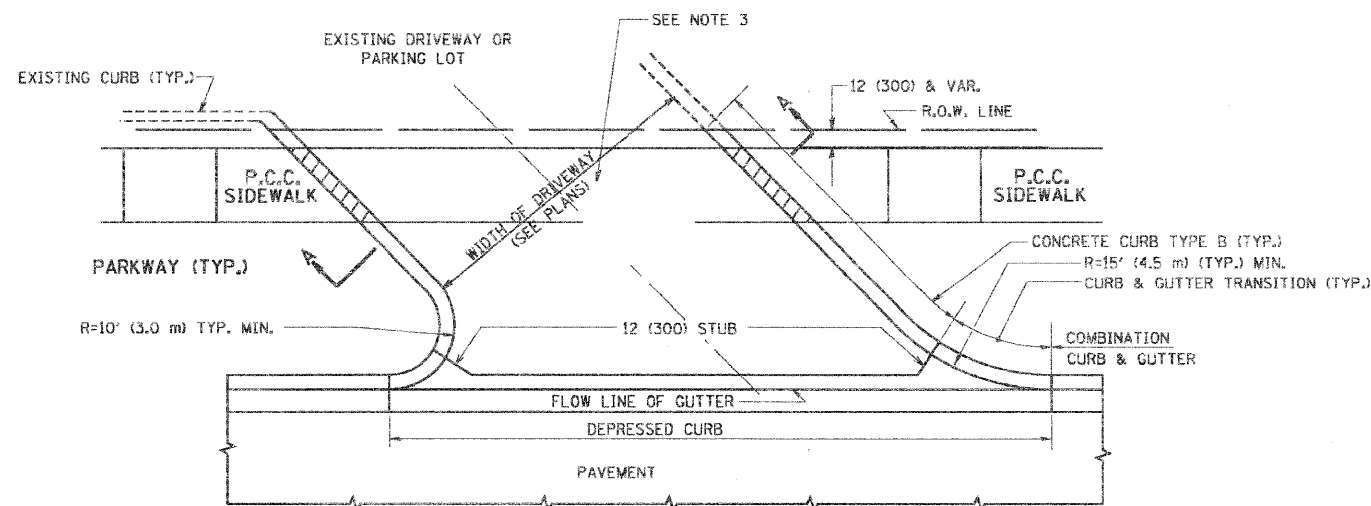
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 5' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINES; FULL SIZE LETTERS & SYMBOLS 18" (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT; OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD T80001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD T80001.

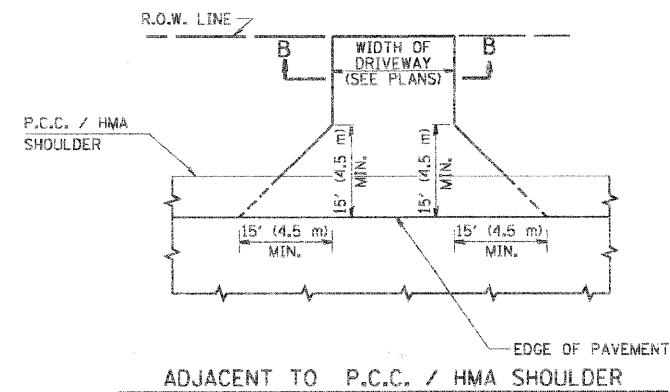
All dimensions are in inches (millimeters) unless otherwise shown.



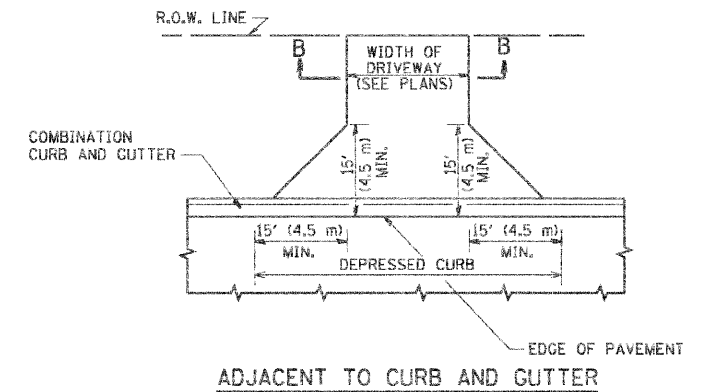
WITH CONCRETE CURB, TYPE B



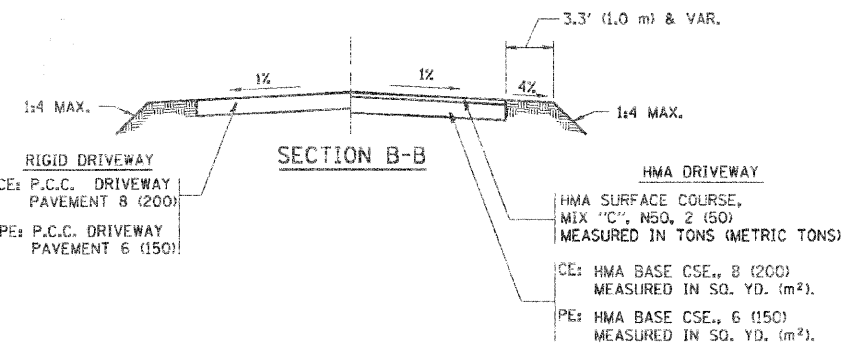
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE,
MIX "C", N50, 2 (50)
MEASURED IN TONS (METRIC TONS)

CE: HMA BASE CSE., 8 (200)
MEASURED IN SQ. YD. (m²).
PE: HMA BASE CSE., 6 (150)
MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

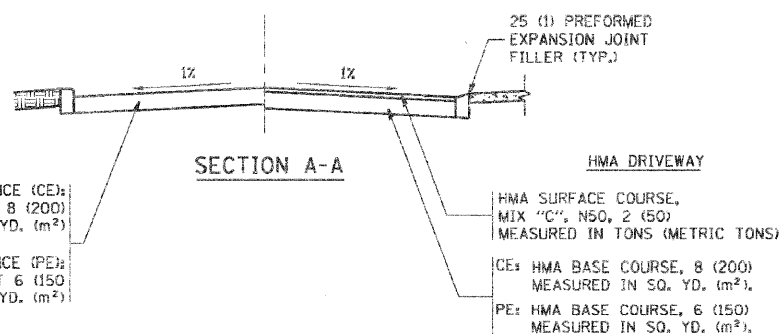
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

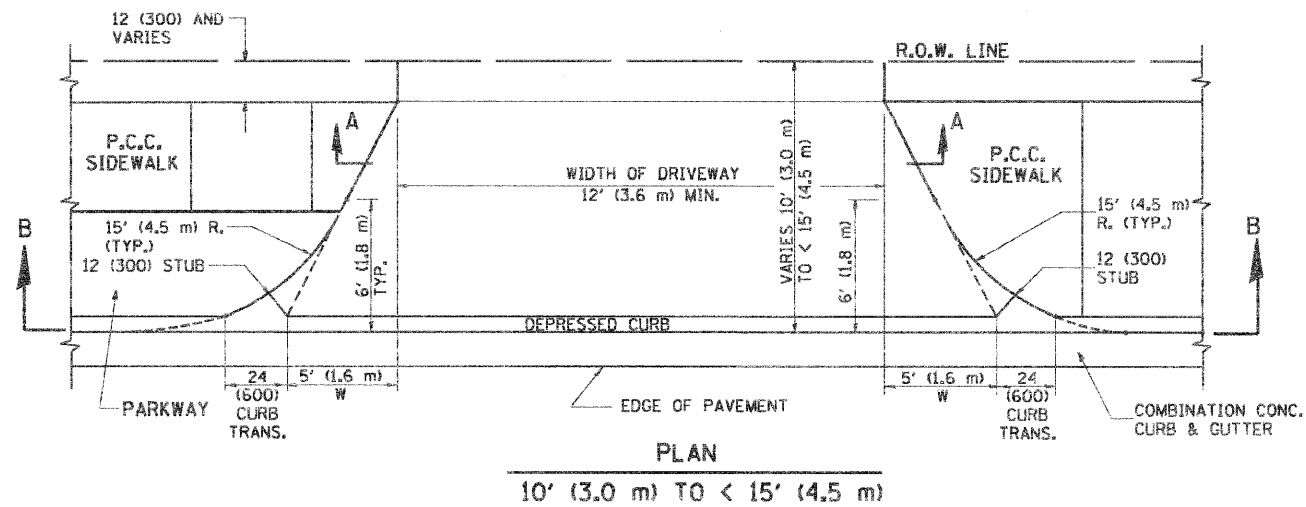
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



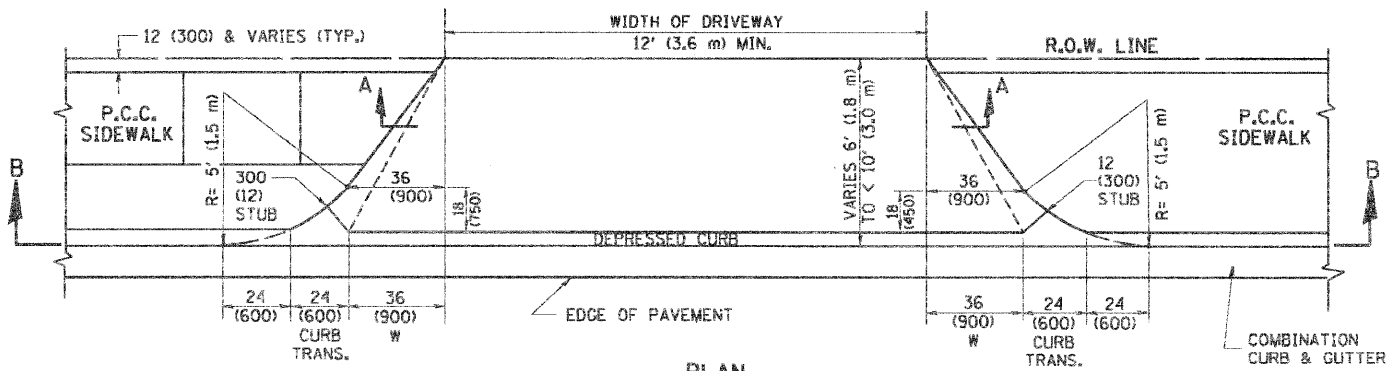
RIGID DRIVEWAY
COMMERCIAL ENTRANCE (CE):
P.C.C. DRIVEWAY PAVEMENT 8 (200)
MEASURED IN SQ. YD. (m²)
NON-COMMERCIAL ENTRANCE (PE):
P.C.C. DRIVEWAY PAVEMENT 6 (150)
MEASURED IN SQ. YD. (m²)

HMA DRIVEWAY
HMA SURFACE COURSE,
MIX "C", N50, 2 (50)
MEASURED IN TONS (METRIC TONS)
CE: HMA BASE COURSE, 8 (200)
MEASURED IN SQ. YD. (m²).
PE: HMA BASE COURSE, 6 (150)
MEASURED IN SQ. YD. (m²).

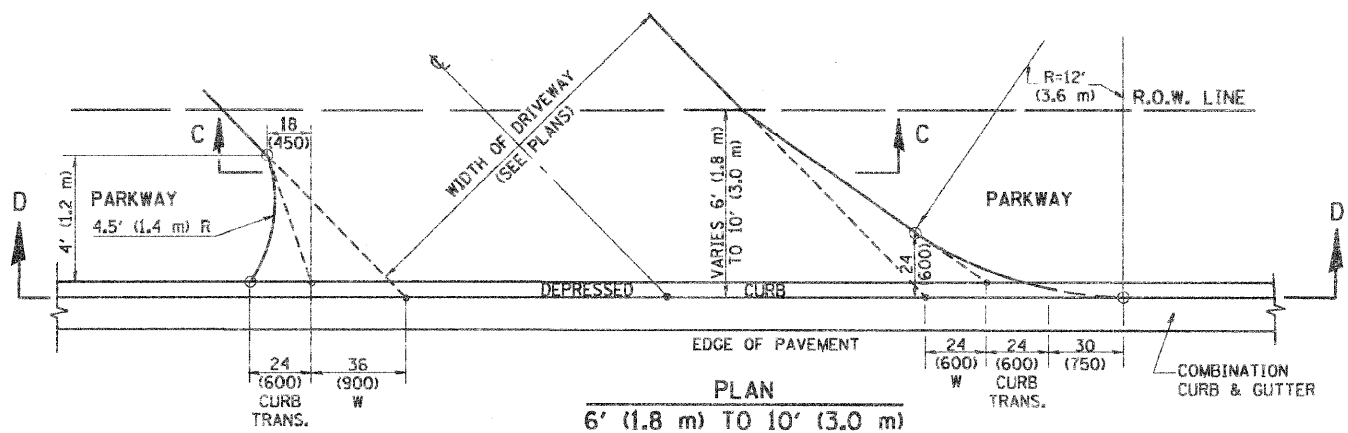
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PLOT SCALE = 48,9999' / IN.	CHECKED -	DRAWN -	REVISED - P. LOFLUER 04-15-03		2718 07-00080-00-WR LAKE 55 41		
PLOT DATE = 6/12/2008	DATE - 11-04-95	REVISOR - R. BORO 01-01-07	REVISED - R. BORO 06-11-08		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.		BD0156-07 (BD-01) CONTRACT NO. 63124
							FED. ROAD DIST. NO. 1 ILLINOIS/FED. AID PROJECT



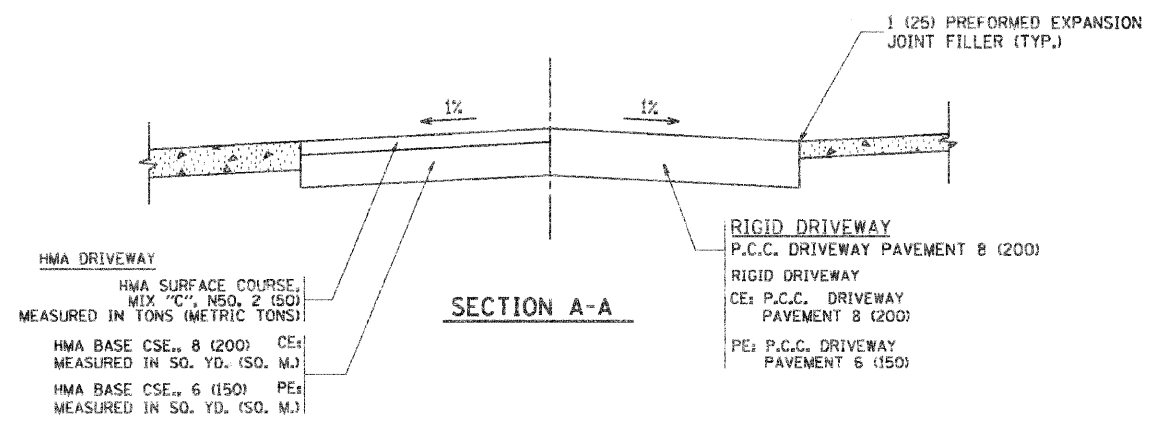
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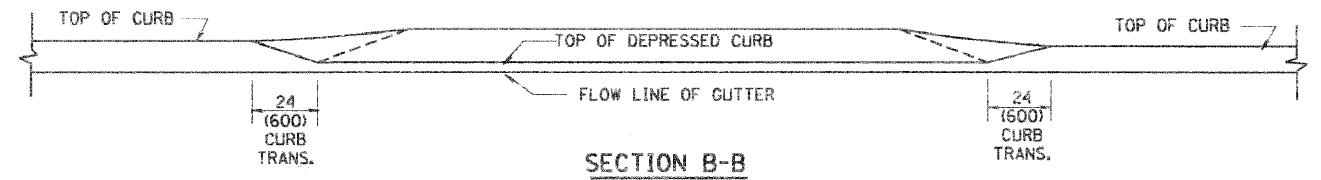
PLAN
6' (1.8 m) TO < 10' (3.0 m)



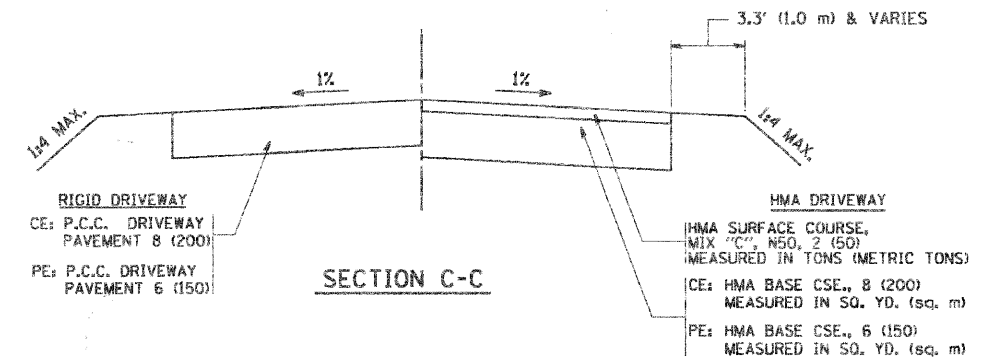
PLAN
6' (1.8 m) TO 10' (3.0 m)



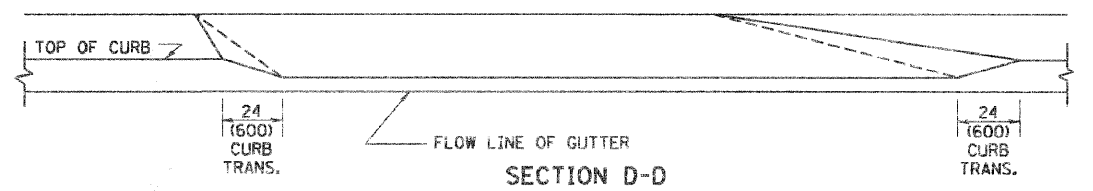
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

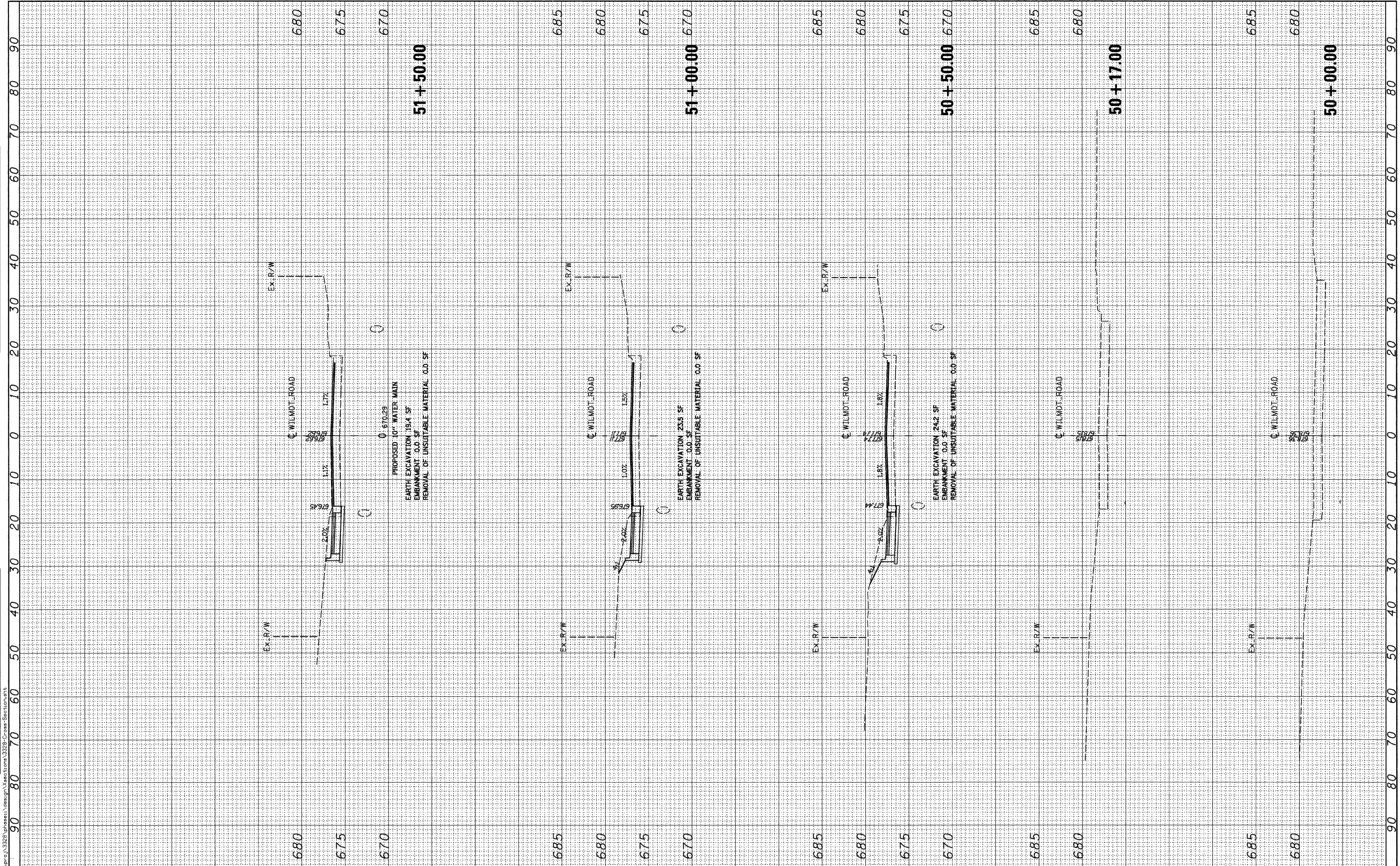
"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME = W:\drivetail\22x34\ba02.dgn	USER NAME = geglernobts	DESIGNED - R. SHAH	REVISED - T. HOLTZ 04-08-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY DETAILS		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - M. GOMEZ 04-06-01		DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)		2718	07-00080-00-WR	LAKE	55	42
		CHECKED -	REVISED - P. LOFLEUR 04-15-03		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD400-02 (BD-02) CONTRACT NO. 63124		
		DATE - 11-06-95	REVISED - R. BORO 01-01-07		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT						

FINAL SURVEY	BY	DATE
SURVEYED		
NOTE BOOK		
TEMPLATE		
AREAS CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
NOTE BOOK		
TEMPLATE		
AREAS CHECKED		
NO.		



FILE NAME = n:\p\c\1328\phase1\design\sections\3228-Cross-Section.sht

Ciorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60656
 Tel. 773.775.4009 Fax 773.775.4014

USER NAME = equrawaki	DESIGNED - WBL	REVISED -
PLOT SCALE = 3/8" = 1' IN.	DRAWN - WBL	REVISED -
PLOT DATE = 1/30/2009	CHECKED - CLG	REVISED -
	DATE - 1-30-2009	REVISED -

VILLAGE OF DEERFIELD
WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)

CROSS SECTIONS

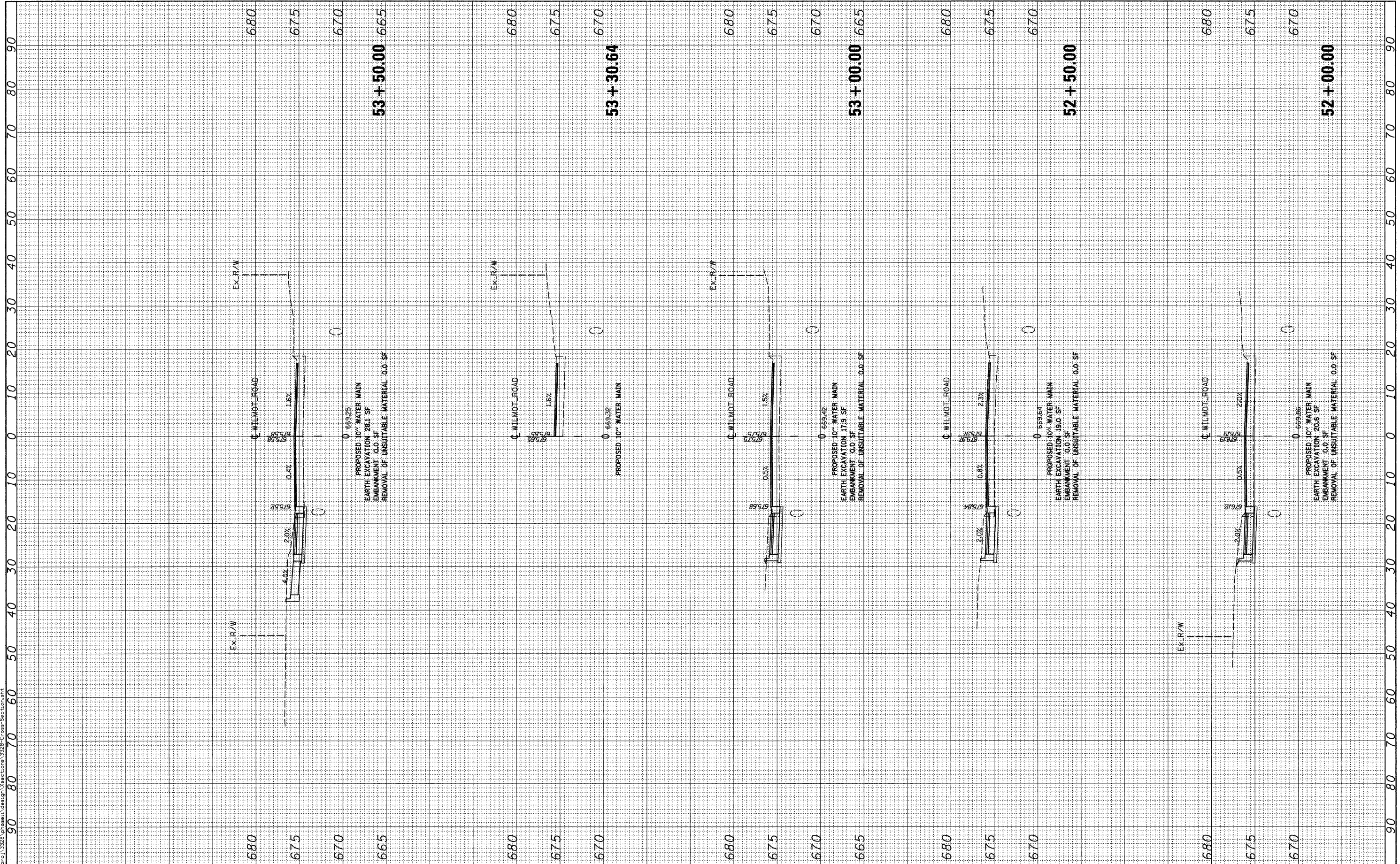
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F.A. RTE. 2718	SECTION 07-00080-00-WR	COUNTY LAKE	TOTAL SHEETS 55	SHEET NO. 43
CONTRACT NO. 63124				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

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Ciorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60656
 Tel. 773.775.4009 Fax 773.775.4014

USER NAME = egutowski	DESIGNED - WBL	REVISED -
PLOT SCALE = 1/4" = 1'-0"	DRAWN - WBL	REVISED -
PLOT DATE = 1/30/2009	CHECKED - CLG	REVISED -
	DATE - 1-30-2009	REVISED -

VILLAGE OF DEERFIELD
WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)

CROSS SECTIONS

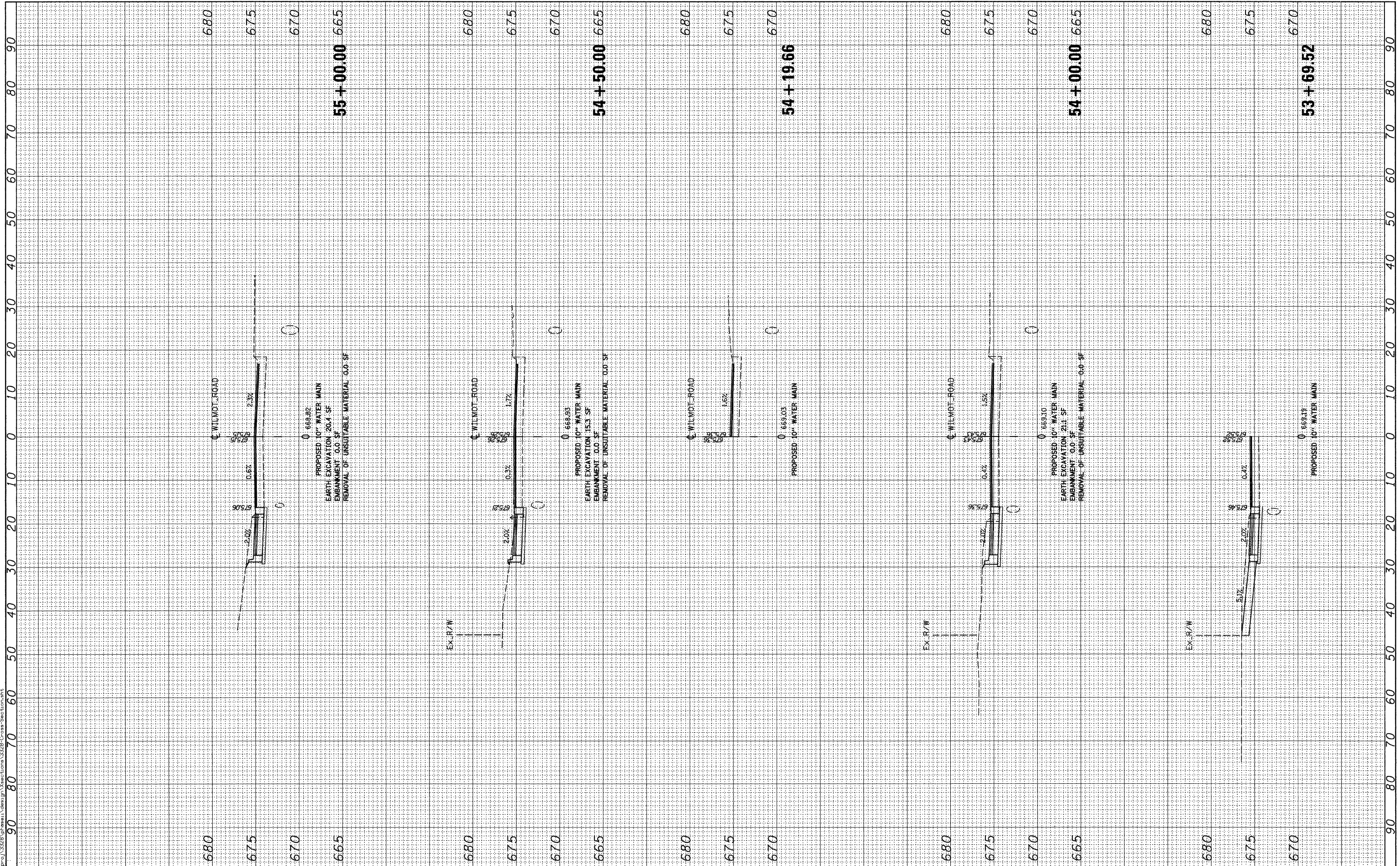
SCALE:	SHEET NO.	OF	SHEETS	STA. 52+00.00	TO STA. 53+50.00
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F.A. RTE. 2718	SECTION 07-00080-00-WR	COUNTY LAKE	TOTAL SHEETS 55	SHEET NO. 44
CONTRACT NO. 63124				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED BY	DATE

ORIGINAL SURVEY NO.	SURVEYED BY	DATE

FILE NAME = c:\pcc\13220\shwan\design\33220\cross-sections



Ciorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60656
 Tel. 773.775.4009 Fax 773.775.4014

USER NAME = agutowski	DESIGNED - WBL	REVISED -
PLOT SCALE = 9.7988' / IN.	DRAWN - WBL	REVISED -
PLOT DATE = 1/30/2009	CHECKED - CLG	REVISED -
	DATE - 1-30-2009	REVISED -

VILLAGE OF DEERFIELD
WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)

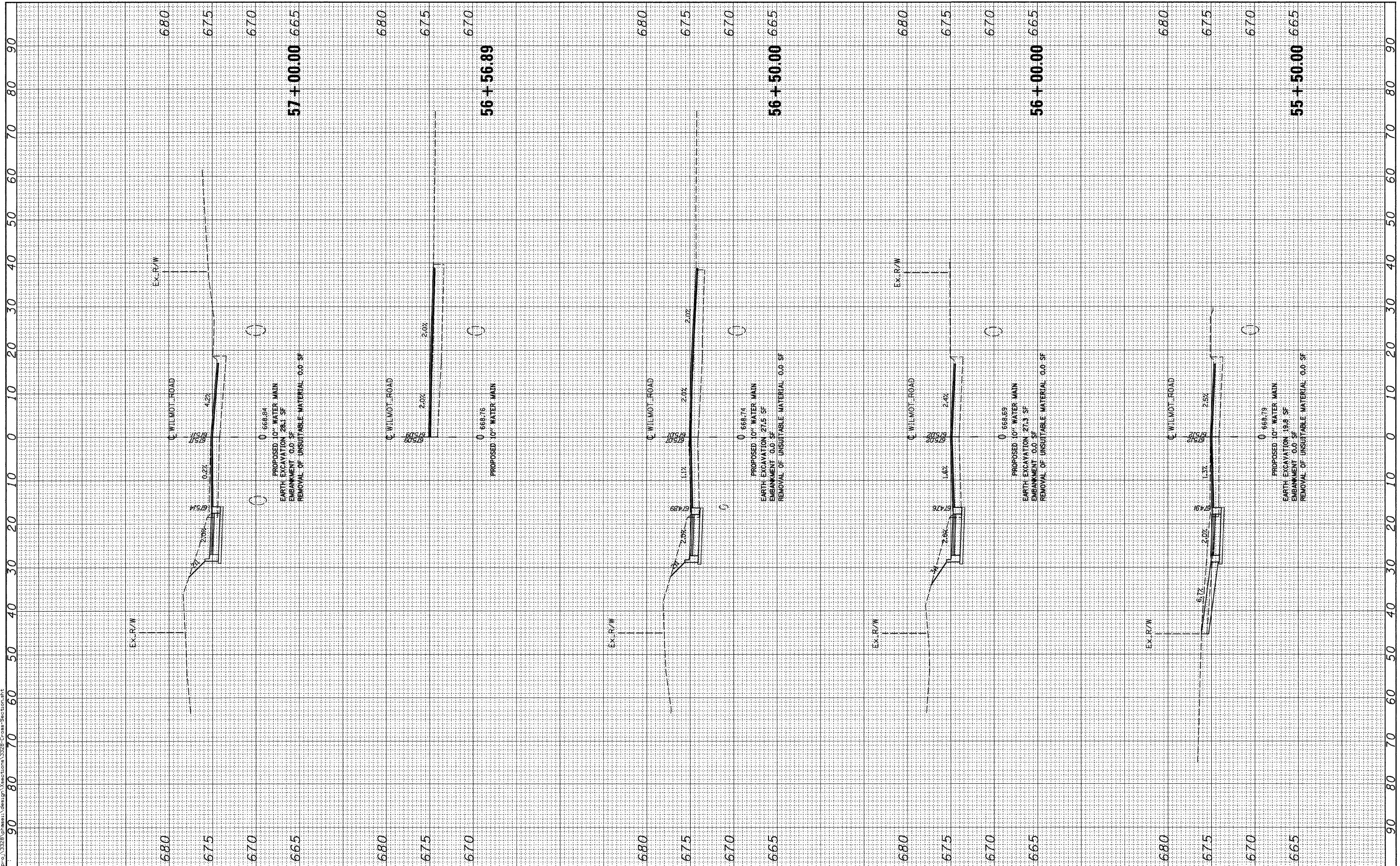
SCALE:	SHEET NO. OF SHEETS	STA. 53+69.52 TO STA. 55+00.00
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F.A. RTE. 2718	SECTION 07-00080-00-WR	COUNTY LAKE	TOTAL SHEETS 55	SHEET NO. 45
CONTRACT NO. 63124				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

FINAL	SURVEY	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEY	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

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USER NAME =	cgutowski
DESIGNED -	WBL
DRAWN -	WBL
CHECKED -	CLG
DATE -	1-30-2009
REVISIONS	
REVISED -	
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REVISED -	

VILLAGE OF DEERFIELD
WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)

CROSS SECTIONS

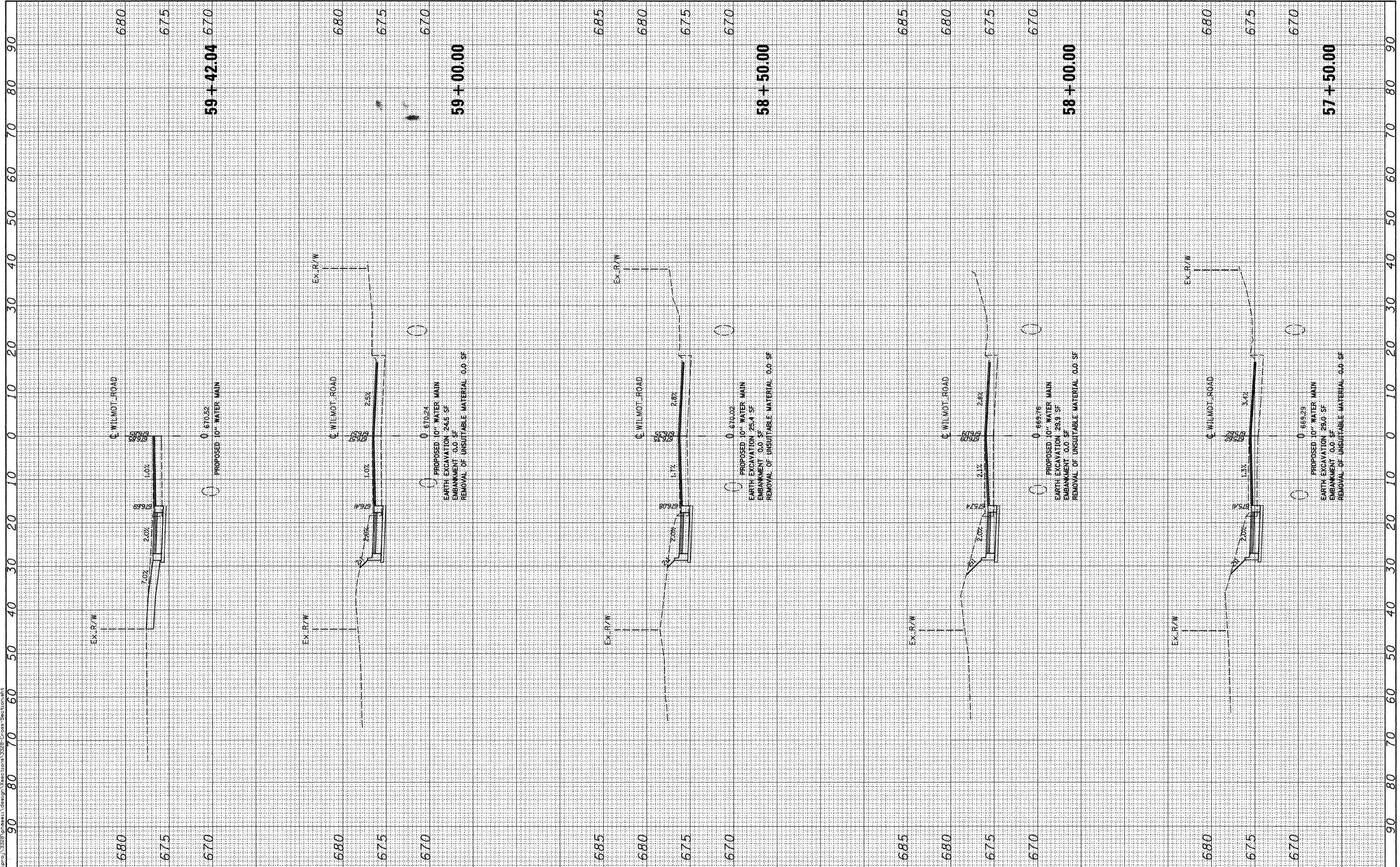
SCALE:	SHEET NO.	OF	SHEETS	STA. 55+50.00	TO STA. 57+00.00
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2718	07-00080-00-WR	LAKE	55	46
CONTRACT NO. 63124				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

FINAL SURVEY NO.	DATE
REVISIONS	BY
PLOTTED	
TEMPLATE	
AREAS CHECKED	

ORIGINAL SURVEY NO.	DATE
REVISIONS	BY
PLOTTED	
TEMPLATE	
AREAS CHECKED	

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USER NAME = agutowski	DESIGNED - WBL	REVISED -
PLOT SCALE = 1/4" = 1' IN.	DRAWN - WBL	REVISED -
PLOT DATE = 1/30/2009	CHECKED - CLG	REVISED -
	DATE - 1-30-2009	REVISED -

**VILLAGE OF DEERFIELD
 WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)**

SCALE:	SHEET NO.	OF SHEETS	STA. 57+50.00 TO STA. 59+42.04
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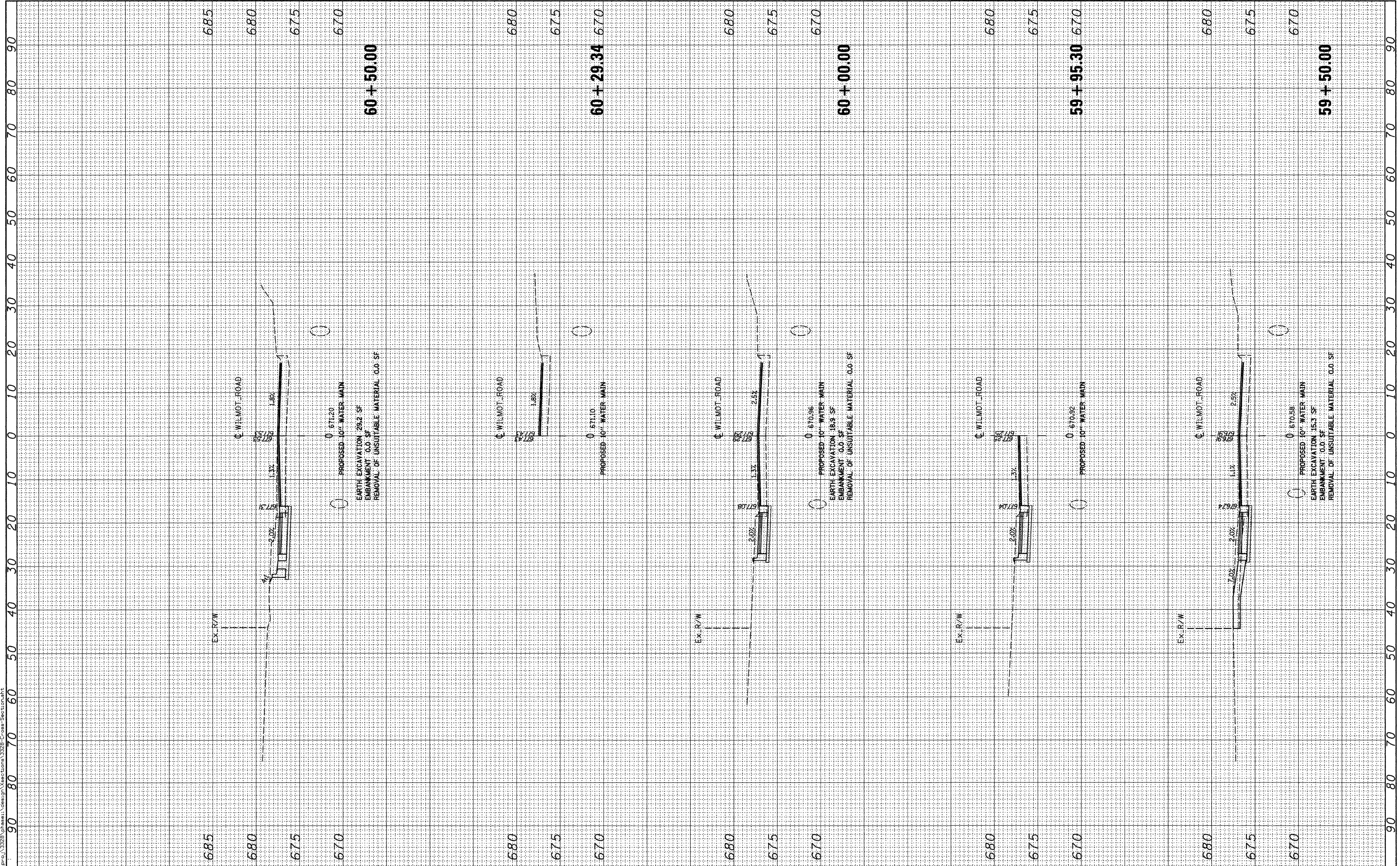
CROSS SECTIONS

F.A. RTE. 2718	SECTION 07-00080-00-WR	COUNTY LAKE	TOTAL SHEETS 55	SHEET NO. 47
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		CONTRACT NO. 63124

FINAL SURVEY	CHECKED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	CHECKED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

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USER NAME = cgotowski	DESIGNED - WBL	REVISED -
PLOT SCALE = 1/4" = 1'-0"	DRAWN - WBL	REVISED -
PLOT DATE = 1/30/2009	CHECKED - CLG	REVISED -
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VILLAGE OF DEERFIELD
WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)

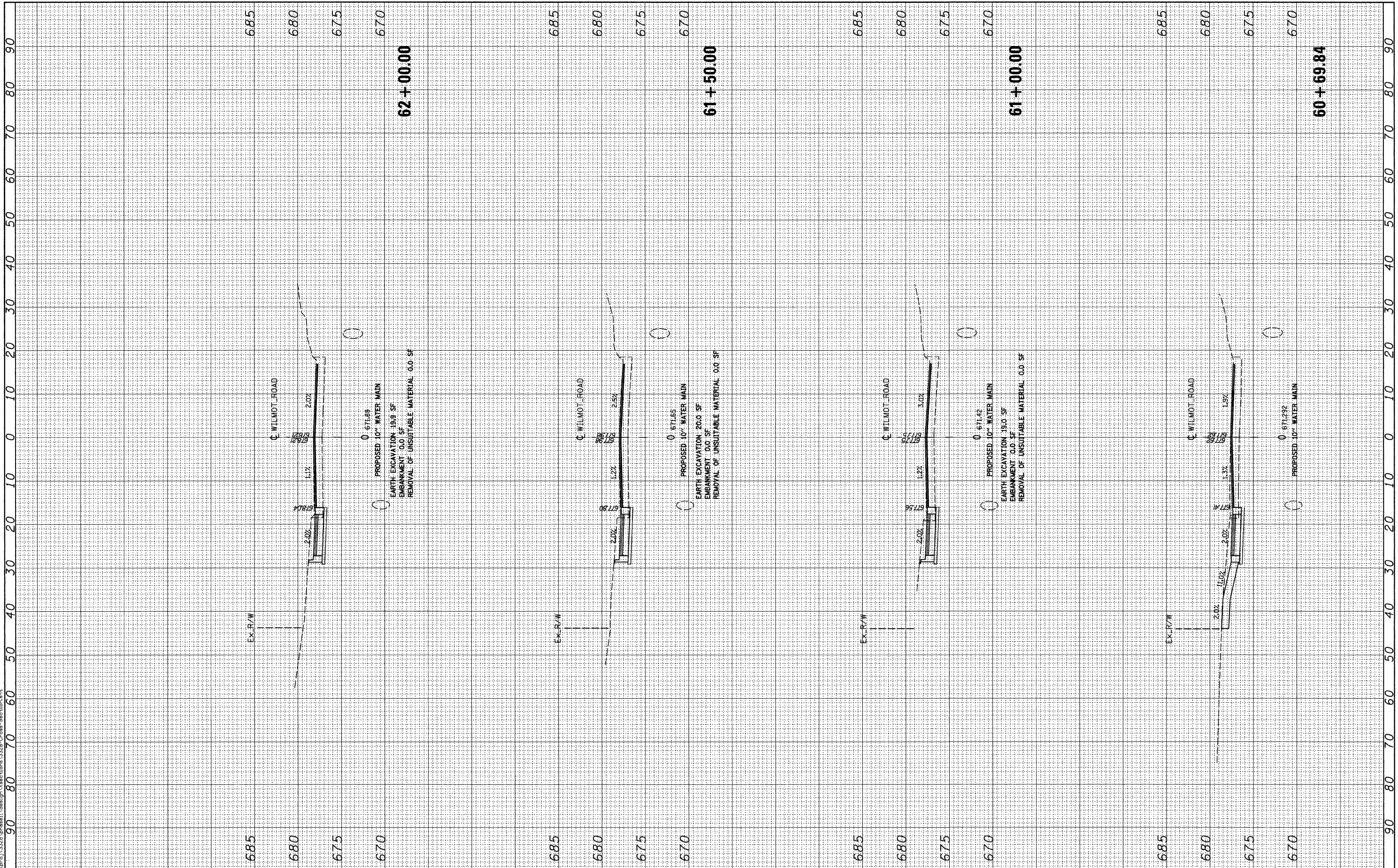
CROSS SECTIONS			
SCALE:	SHEET NO.	OF SHEETS	STA. 59+50.00 TO STA. 60+50.00

F.A. RTE. 2718	SECTION 07-0080-00-WR	COUNTY LAKE	TOTAL SHEETS 55	SHEET NO. 48
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 63124	

FINAL SURVEY NOTE BOOK NO.	BY	DATE
REVISIONS		
PLOTTED		
TEMPLATE		
AREAS CHECKED		

ORIGINAL SURVEY NOTE BOOK NO.	BY	DATE
REVISIONS		
PLOTTED		
TEMPLATE		
AREAS CHECKED		

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USER NAME = cgotowski	DESIGNED - WBL	REVISED -
PLOT SCALE = 1/4" = 1'-0"	DRAWN - WBL	REVISED -
PLOT DATE = 1/30/2009	CHECKED - CLG	REVISED -
	DATE - 1-30-2009	REVISED -

VILLAGE OF DEERFIELD
WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)

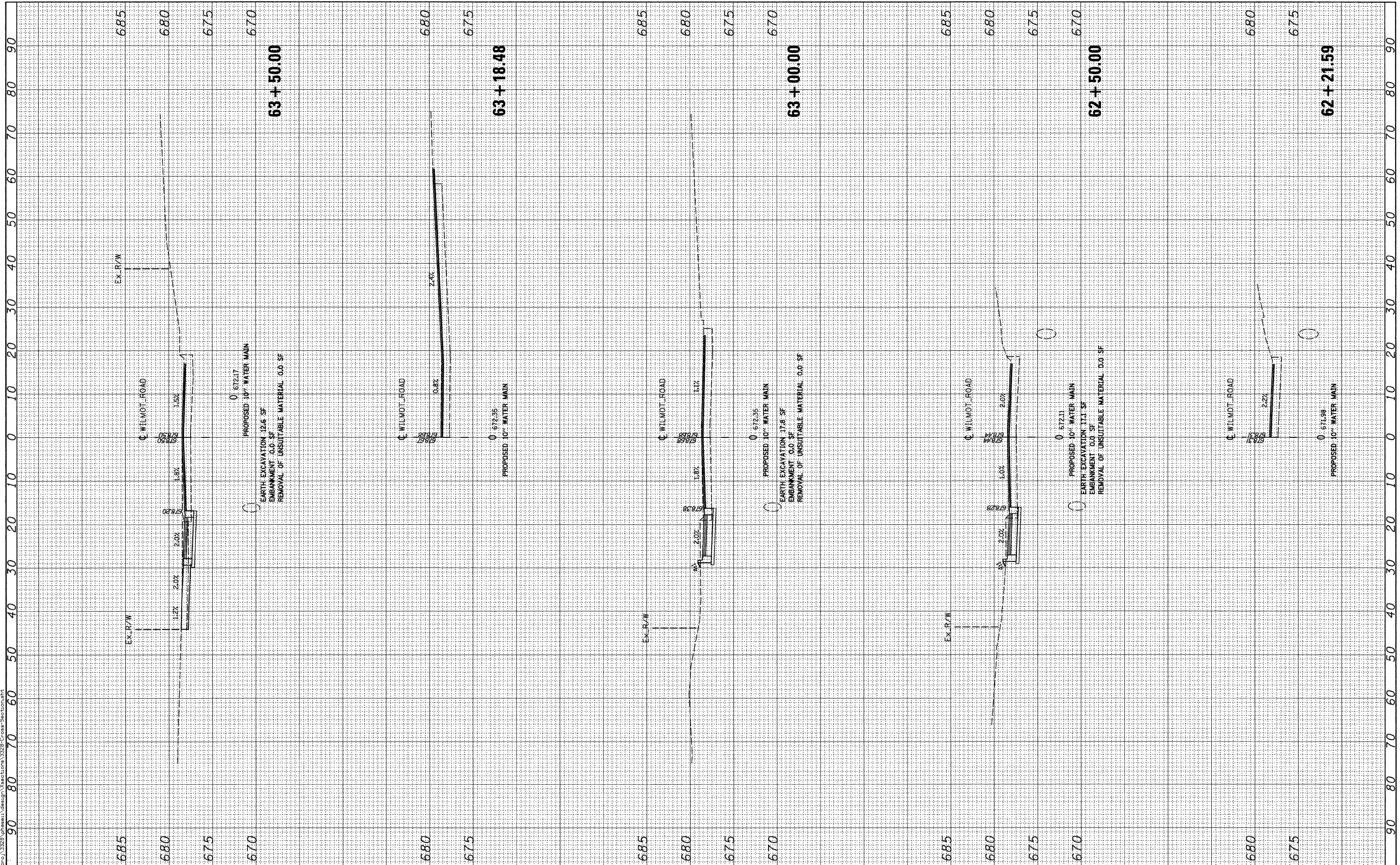
CROSS SECTIONS			
SCALE:	SHEET NO.	OF SHEETS	STA. 60+69.84 TO STA. 62+00.00

F.A. RTE. 271B	SECTION 07-00080-00-WR	COUNTY LAKE	TOTAL SHEETS 55	SHEET NO. 49
CONTRACT NO. 63124			FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT	

FINAL SURVEY NO.	SURVEY PLOTTED	BY	DATE
NO.	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEY PLOTTED	BY	DATE
NO.	AREAS CHECKED		

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 Tel. 773.775.4009 Fax 773.775.4014

USER NAME = egutowski	DESIGNED - WBL	REVISED -
PLOT SCALE = 1/4" = 1'-0"	DRAWN - WBL	REVISED -
PLOT DATE = 1/30/2009	CHECKED - CLG	REVISED -
	DATE - 1-30-2009	REVISED -

VILLAGE OF DEERFIELD
WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)

SCALE:	SHEET NO.	OF	SHEETS	STA. 62+21.59	TO STA. 63+50.00
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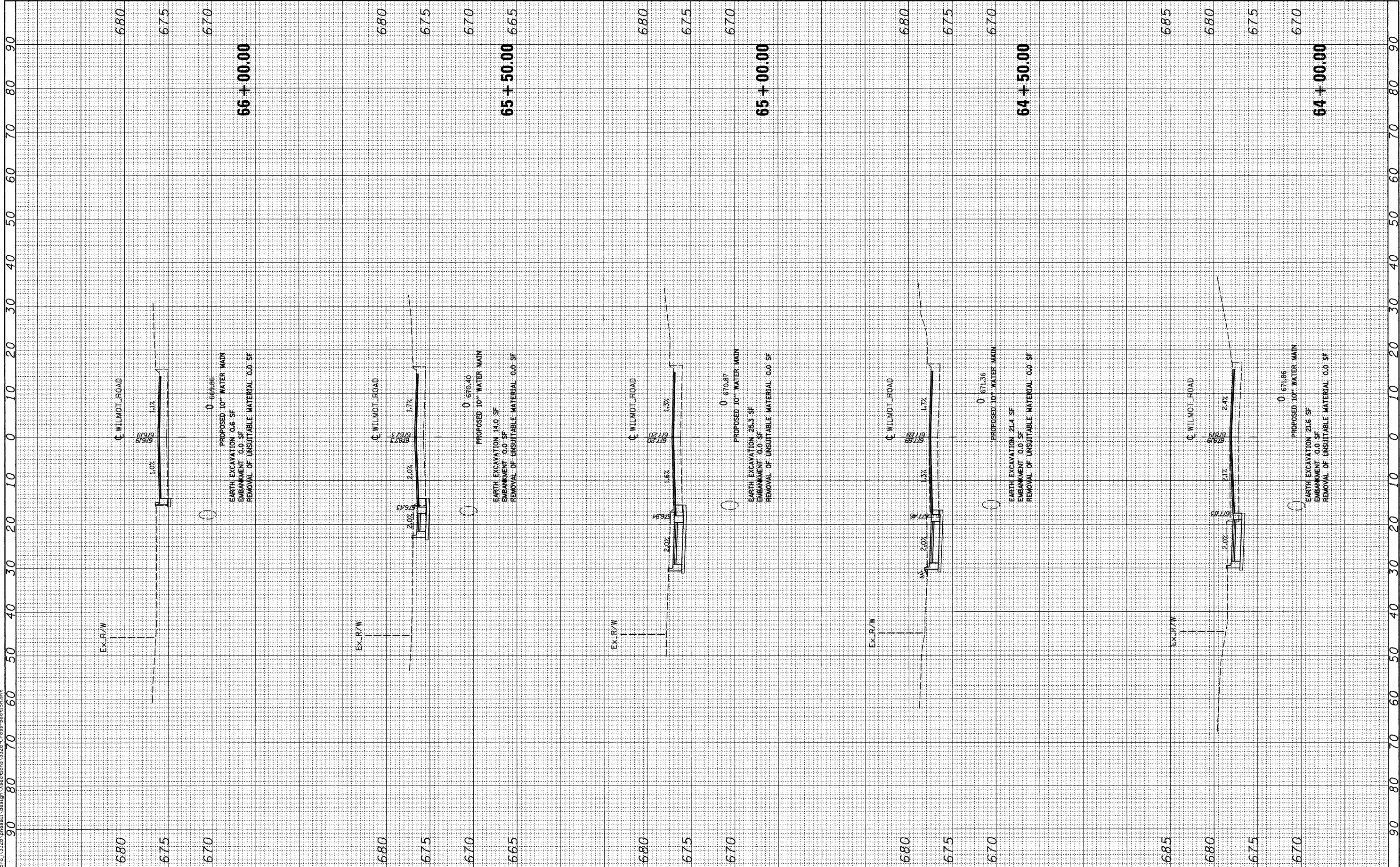
CROSS SECTIONS

F.A. RTE. 2718	SECTION 07-00080-00-WR	COUNTY LAKE	TOTAL SHEETS 55	SHEET NO. 50
CONTRACT NO. 63124				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

FINAL SURVEY	SURVISED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVISED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
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USER NAME = cgutowski	DESIGNED - WBL	REVISED -
PLOT SCALE = 9.7988' / IN.	DRAWN - WBL	REVISED -
PLOT DATE = 1/30/2009	CHECKED - CLG	REVISED -
	DATE - 1-30-2009	REVISED -

VILLAGE OF DEERFIELD
WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)

CROSS SECTIONS

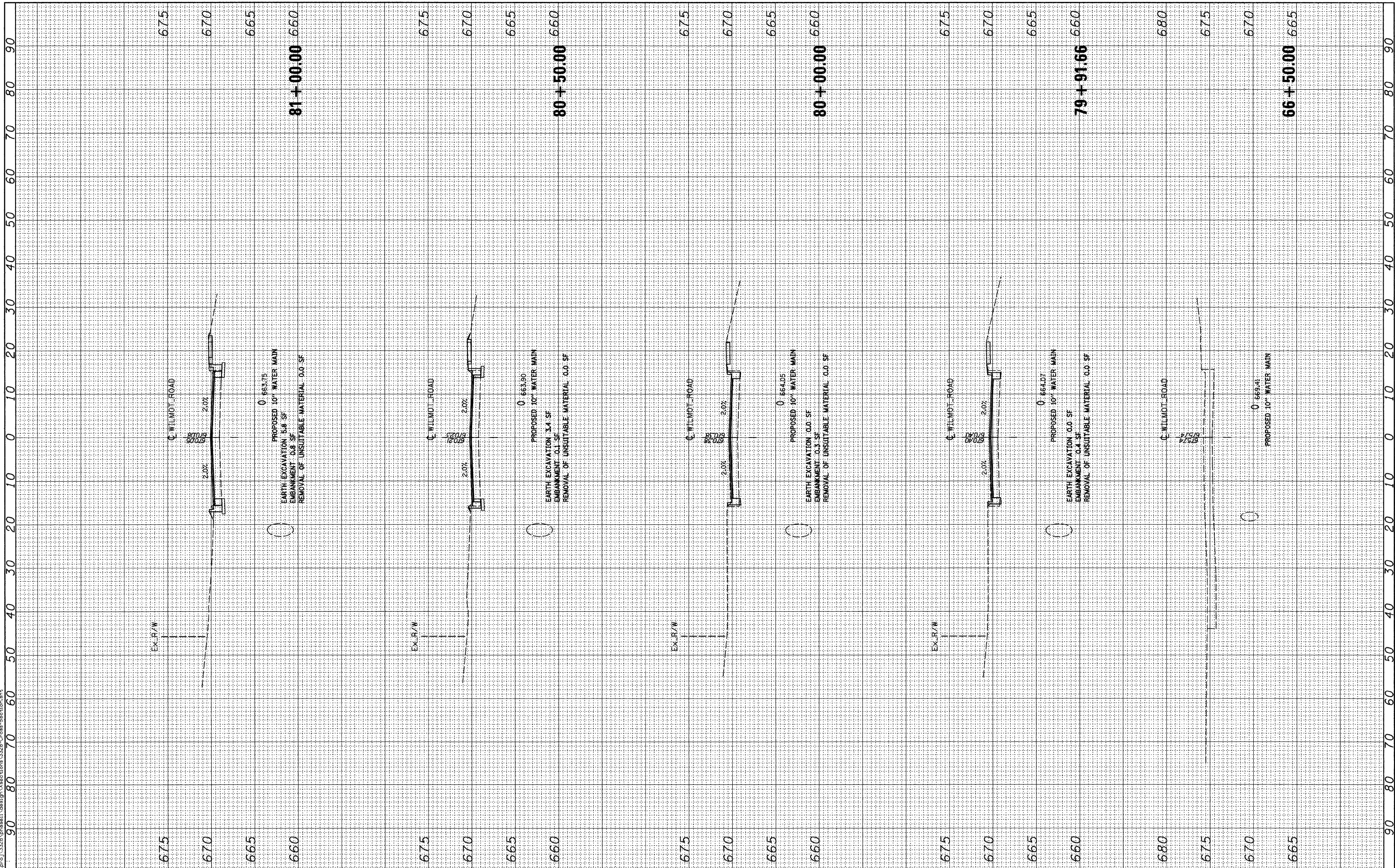
SCALE:	SHEET NO.	OF SHEETS	STA. 64+00.00 TO STA. 66+00.00
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F.A. RTE. 2718	SECTION 07-00080-00-WR	COUNTY LAKE	TOTAL SHEETS 55	SHEET NO. 51
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63124	

FINAL SURVEY	BY	DATE
REVIEWED		
NOTE BOOK		
NO.		
TEMPLATE		
AREAS CHECKED		
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ORIGINAL SURVEY	BY	DATE
REVIEWED		
NOTE BOOK		
NO.		
TEMPLATE		
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AREAS CHECKED		

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USER NAME =	cgutowski
DESIGNED -	WBL
REVISIONS	
DRAWN -	WBL
CHECKED -	CLG
DATE -	1-30-2009
PLOT SCALE =	1/32" = 1' / IN.
PLOT DATE =	1/30/2009

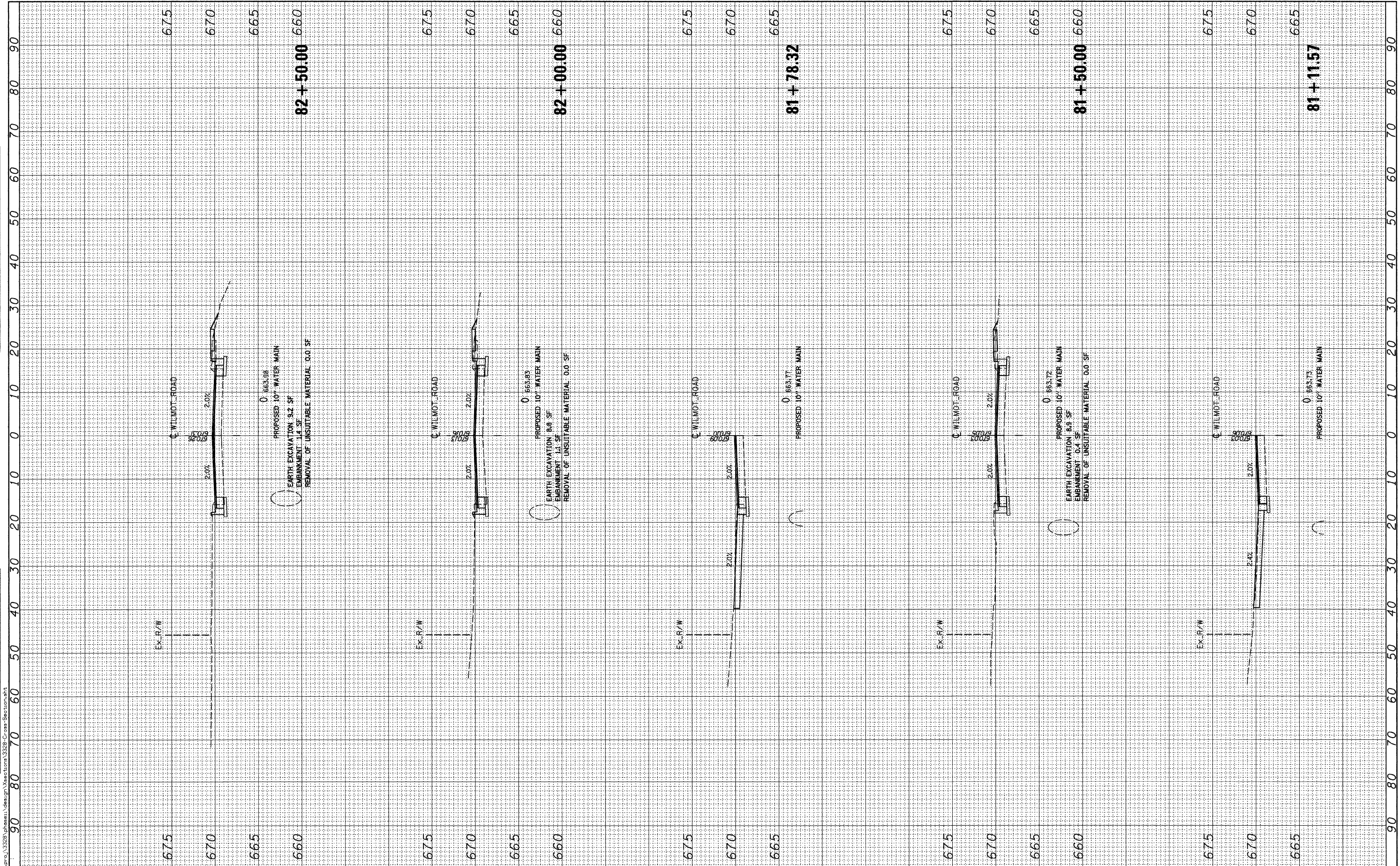
VILLAGE OF DEERFIELD
WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)

CROSS SECTIONS			
SCALE:	SHEET NO.	OF SHEETS	STA. 66+50.00 TO STA. 81+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2718	07-00080-00-WR	LAKE	55	52
CONTRACT NO. 63124				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		



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USER NAME =	cgutowaki	DESIGNED -	WBL	REVISED -	
PLOT SCALE =	3/8" = 1' / IN.	DRAWN -	WBL	REVISED -	
PLOT DATE =	1/30/2009	CHECKED -	CLG	REVISED -	
		DATE -	1-30-2009	REVISED -	

VILLAGE OF DEERFIELD
WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)

CROSS SECTIONS

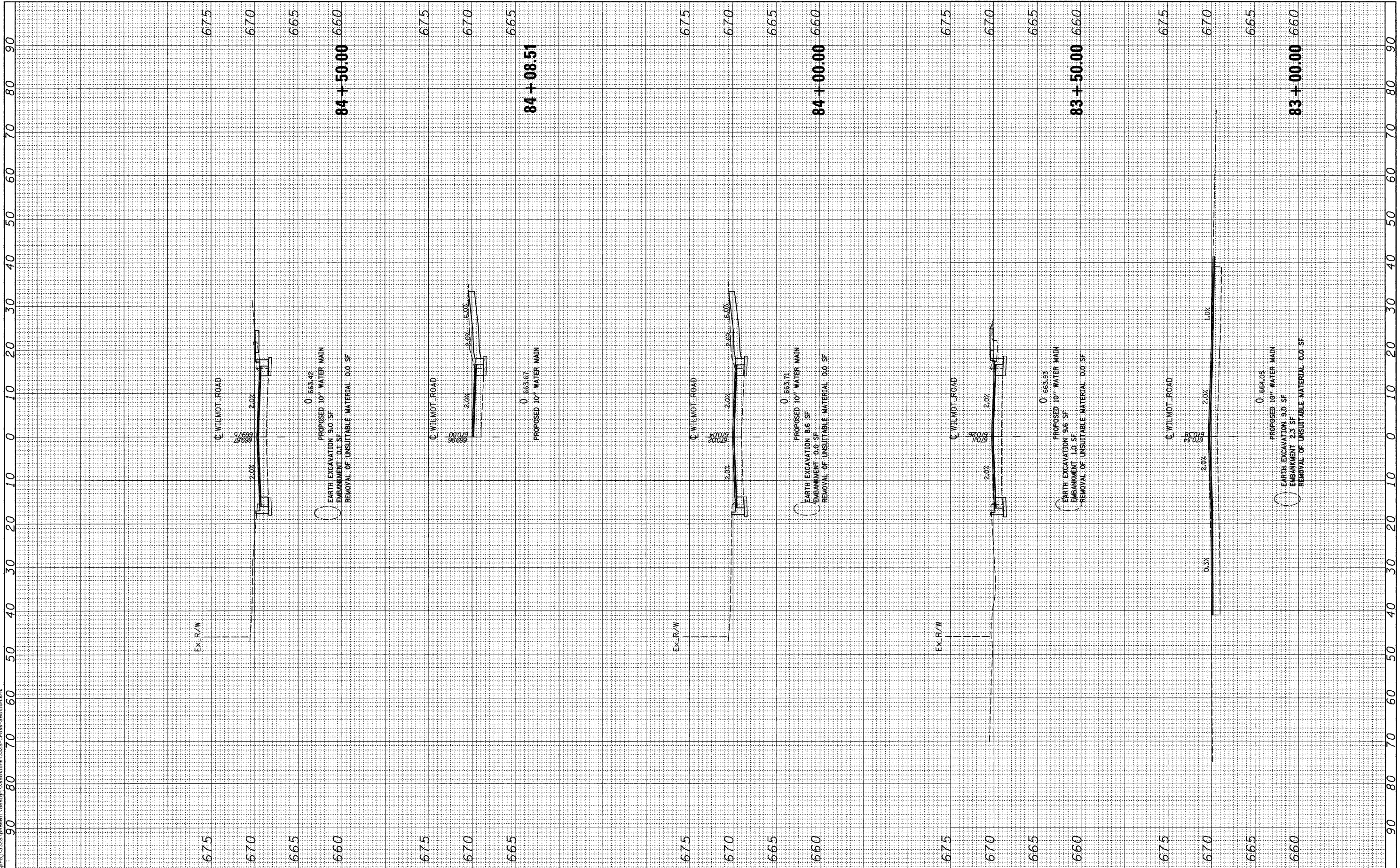
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2718	07-0080-00-WR	LAKE	55	53
CONTRACT NO. 63124				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

FINISH SURVEY	BY	DATE
EXAMINED		
PLOTTED		
NOTE BOOK		
AREAS CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
EXAMINED		
PLOTTED		
NOTE BOOK		
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NO.		

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USER NAME =	cgutowski	DESIGNED -	WBL	REVISED -	
PLOT SCALE =	9.7988" / IN.	DRAWN -	WBL	REVISED -	
PLOT DATE =	1/30/2009	CHECKED -	CLG	REVISED -	
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VILLAGE OF DEERFIELD
WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)

CROSS SECTIONS

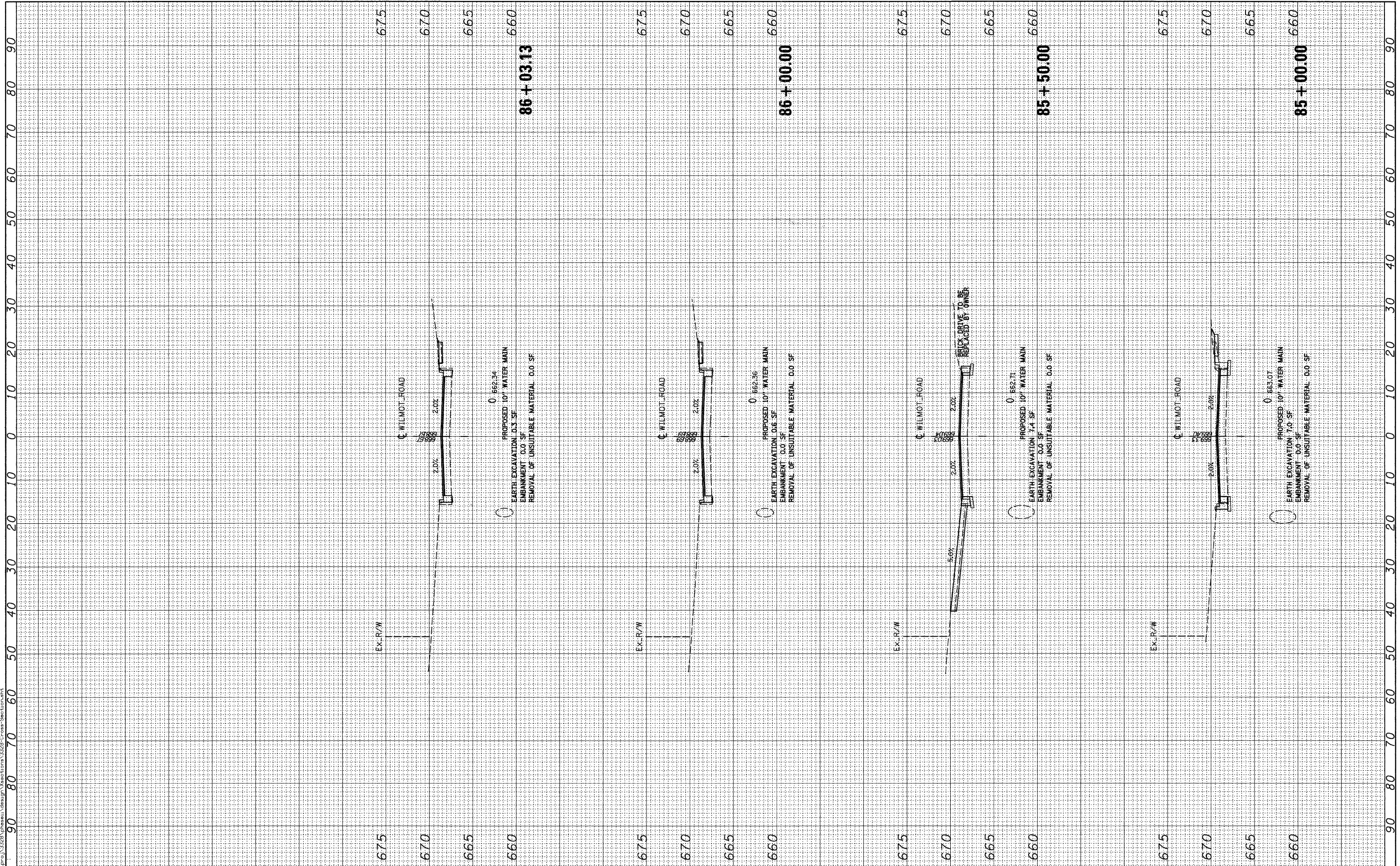
SCALE:	SHEET NO.	OF	SHEETS	STA. 83+00.00	TO STA. 84+50.00
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2718	07-00080-00-WR	LAKE	55	54
CONTRACT NO. 63124				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEY PLOTTED	BY	DATE
	NOTE BOOK		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEY PLOTTED	BY	DATE
	NOTE BOOK		
	AREAS CHECKED		

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USER NAME = ogustowski	DESIGNED - WBL	REVISED -
PLOT SCALE = 1/32" = 1' / IN.	DRAWN - WBL	REVISED -
PLOT DATE = 1/30/2009	CHECKED - CLG	REVISED -
	DATE - 1-30-2009	REVISED -

VILLAGE OF DEERFIELD
WILMOT ROAD (GORDON TERRACE TO DEERFIELD ROAD)

SCALE:	SHEET NO.	OF	SHEETS	STA. 85+00.00	TO STA. 86+03.13
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F.A. RTE. 2718	SECTION 07-00080-00-WR	COUNTY LAKE	TOTAL SHEETS 55	SHEET NO. 55
CONTRACT NO. 63124				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				