

SCOPE OF WORK

THIS PROJECT CONSISTS OF THE FOLLOWING:

RECONSTRUCTION OF 960 FEET X 150 FEET OF BITUMINOUS PAVEMENT ON RUNWAY 6-24. ASSOCIATED WORK INCLUDES PAVEMENT MILLING, PAVING, RUNWAY PAVEMENT GROOVING AND MARKING.

PROPOSED SAFETY PLAN

ANY TIME THE CONTRACTOR IS WORKING WITHIN 200' OF THE RUNWAY CENTERLINE, THE RUNWAY WILL BE CLOSED. THE CONTRACTOR WILL SMOOTH GRADE ALL AREAS WITHIN THE SAFETY AREA TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT PRIOR TO REOPENING THE RUNWAY. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE DECATUR AIRPORT IS TOWER CONTROLLED FROM 6 A.M. TO 10 P.M. AND IS COMPRISED OF THREE (3) RUNWAYS. THE PROPOSED CONSTRUCTION WILL AFFECT RUNWAYS 6-24 AND 12-30. RUNWAY 18-36 WILL NOT REQUIRE CLOSURE. THE SAFETY PLAN AS OUTLINED HERE AND IN THE SPECIAL PROVISIONS WILL MAXIMIZE SAFETY AND ALLOW MINIMUM RUNWAY CLOSURE TIME.

THE CLOSURE OF A RUNWAY WILL REQUIRE THE FOLLOWING:

- 1. PROVIDING THE SPECIFIED ADVANCE NOTIFICATION TO THE AIRPORT DIRECTOR.
2. CONFIRMING THE ISSUANCE OF THE NOTAM WITH AIRPORT DIRECTOR AND CONTROL TOWER PRIOR TO STARTING THE ACTUAL CLOSURE.
3. PLACING THE 10 FT. X 60 FT. CROSSES OR PORTABLE RUNWAY CLOSURE MARKERS (SEE MARKER NOTE THIS SHEET) AT THE RUNWAY END OR ON THE NUMBERS AS THE SITUATION DICTATES.
4. PLACEMENT OF BARRICADES.
5. MAINTENANCE OF THE CROSSES AND BARRICADES DURING THE CLOSING PERIOD.

OPENING A RUNWAY WILL REQUIRE THE FOLLOWING:

- 1. THE PAVEMENT WILL BE SWEEPED BY A POWER BROOM TO REMOVE ANY AND ALL DEBRIS FROM THE PAVEMENT.
2. THE AREA WITHIN 200 FEET OF A RUNWAY CENTERLINE MUST BE SMOOTH GRADED WITH A 1% - 5% SLOPE AWAY FROM THE PAVEMENT EDGE. THE PAVEMENT EDGE DROP-OFF CAN NOT EXCEED 3 INCHES. NO HOLES OR MOUNDS WILL BE PERMITTED WITHIN THE AREA.
3. ALL PERSONNEL AND EQUIPMENT WILL BE OUT OF THE 200 FEET LIMITS.
4. ALL RUNWAY AND TAXIWAY LIGHTING CIRCUITS WILL BE OPERATIONAL.
5. RUNWAY MARKING, IF OBLITERATED, WILL BE REMARKED.
6. THE CLOSED PAVEMENTS WILL BE VISUALLY INSPECTED BY AIRPORT PERSONNEL PRIOR TO OPENING. THE CONTRACTOR WILL MAKE ANY CORRECTIONS REQUIRED AS A RESULT OF THE INSPECTION.

RADIO CONTROL:

THE CONTRACTOR WILL BE REQUIRED TO HAVE A TWO-WAY RADIO, CAPABLE OF BEING IN CONTACT WITH THE CONTROL TOWER, GROUND CONTROL CHANNEL (121.75 MHZ). THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE CONTROL TOWER, THUS KEEPING THE CONTROL TOWER INFORMED ON HIS CONSTRUCTION ACTIVITIES AND ENABLE THE CONTROL TOWER TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE SOME ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

BARRICADES

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND BARRIERS AT THE LOCATIONS SHOWN ON THIS SHEET AND AS DESCRIBED IN THE SPECIAL PROVISIONS.

BARRICADES WILL BE USED IN AREAS OUTSIDE OF THE RUNWAY, AND TRAFFIC CONES WILL BE USED ON THE RUNWAY. THE BARRICADES WILL BE I.D.O.T. TYPE 1, EQUIPPED WITH RED CONTINUOUS OR FLASHING LIGHTS AND EXTENDED 18 INCH SQUARE ORANGE FLAG.

PAYMENT FOR BARRICADES, THEIR PLACEMENT AND REMOVALS AS RUNWAYS AND TAXIWAYS ARE OPENED AND CLOSED, MAINTENANCE, AND REPAIRS WILL BE MADE BY ITEM: AR150530 "TRAFFIC MAINTENANCE" - PER LUMP SUM.

HAUL ROUTE, VEHICLE PARKING AND MATERIAL STORAGE

THE PROPOSED HAUL ROUTE AND EQUIPMENT PARKING AREA WILL BE AT THE LOCATION SHOWN ON THIS DRAWING. THE CONTRACTOR WILL MAINTAIN THE AREA FOR HAULING MATERIALS TO AND FROM THE SITE, FOR VEHICLE PARKING, AND MATERIAL STORAGE. ANY AREAS DAMAGED OUTSIDE THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

AT THE COMPLETION OF THE PROJECT, THE HAUL ROUTE, ASSOCIATED VEHICLE PARKING, AND MATERIAL STORAGE AREA WILL BE COMPLETELY REMOVED AND THE AREA RESTORED TO THE PRE-CONSTRUCTION CONDITION AND TO THE SATISFACTION OF THE RESIDENT ENGINEER.

THE HAUL ROUTE, INCLUDING PARKING AND STORAGE AREAS SHALL BE CONSIDERED AS ONE UNIT FOR PAYMENT PURPOSES. PAYMENT WILL BE MADE FOR MAINTAINING THE HAUL ROUTES, VEHICLE AND MATERIAL STORAGE AREAS, AND FOR RESTORATION OF THE AREAS AT THE COMPLETION OF THE PROJECT UNDER ITEM: AR150540 "HAUL ROUTE" - PER LUMP SUM.

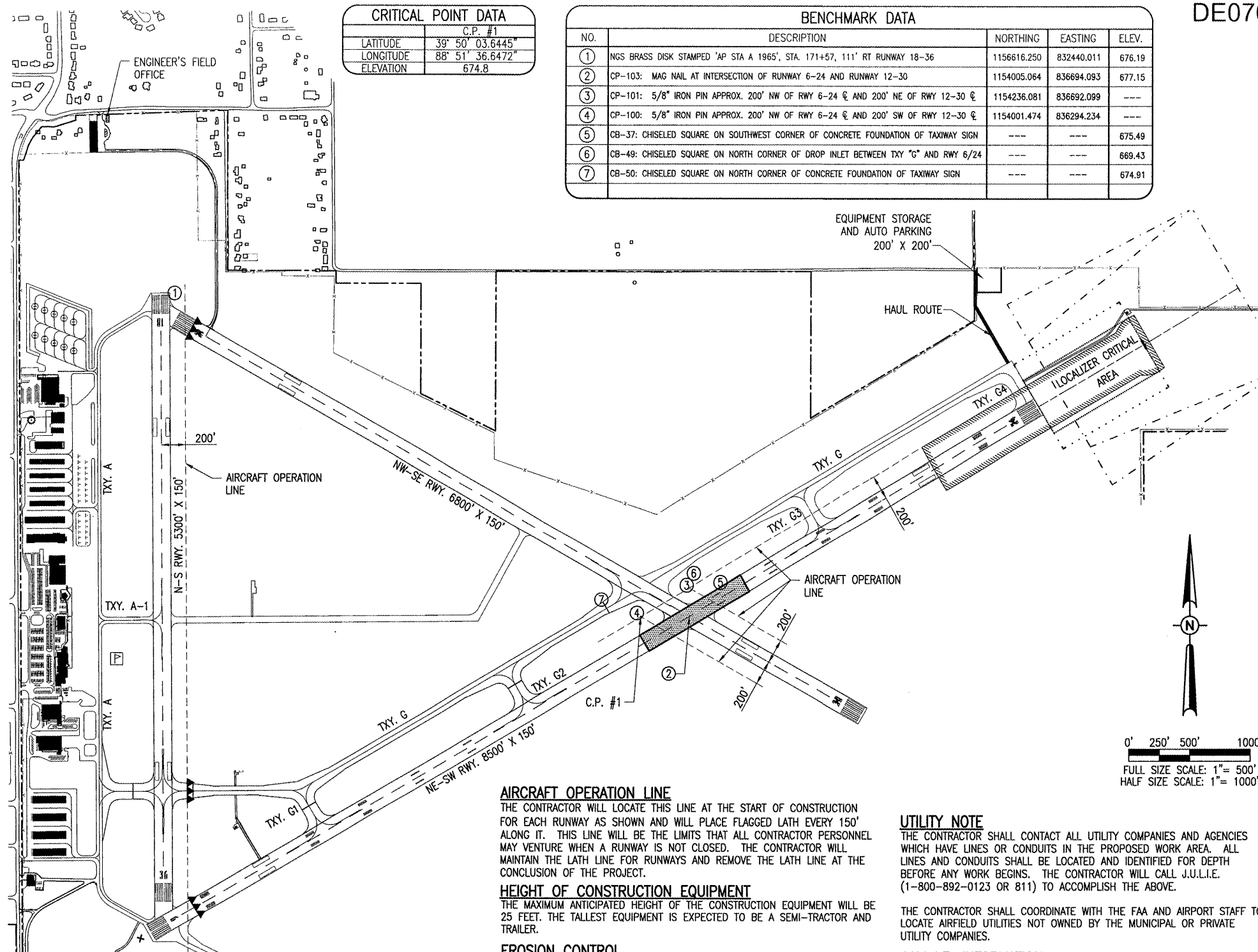
CONTRACTOR RESPONSIBILITIES

THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR VEHICLES IN THE CONTRACTOR'S EQUIPMENT PARKING AND STORAGE AREA AS SHOWN ON THIS SHEET. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE THIS AREA.

AIRPORT SECURITY SHALL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREAS. ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM. THE CONTRACTOR WILL MONITOR ACCESS POINTS USED BY CONSTRUCTION VEHICLES AND PERSONNEL WHEN IN USE AND WILL CLOSE AND LOCK THE ACCESS POINTS AT THE END OF EACH DAY.

THE CONTRACTOR WILL FURNISH ALL OF HIS EMPLOYEES WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THEM AS BEING PART OF THE CONSTRUCTION CREW.

WHEN THE CONTRACTOR'S VEHICLES ARE ON THE AIRPORT SITE THEY SHALL BE PROPERLY MARKED. THE MARKING SHALL CONSIST OF A THREE (3') FOOT SQUARE FLAG CONSISTING OF A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES OF NOT LESS THAN ONE (1') FOOT ON EACH SIDE DISPLAYED IN FULL VIEW ABOVE THE VEHICLE.



CRITICAL POINT DATA table with columns: C.P. #1, LATITUDE, LONGITUDE, ELEVATION.

BENCHMARK DATA table with columns: NO., DESCRIPTION, NORTHING, EASTING, ELEV.

DE070

AIRCRAFT OPERATION LINE

THE CONTRACTOR WILL LOCATE THIS LINE AT THE START OF CONSTRUCTION FOR EACH RUNWAY AS SHOWN AND WILL PLACE FLAGGED LATH EVERY 150' ALONG IT. THIS LINE WILL BE THE LIMITS THAT ALL CONTRACTOR PERSONNEL MAY VENTURE WHEN A RUNWAY IS NOT CLOSED. THE CONTRACTOR WILL MAINTAIN THE LATH LINE FOR RUNWAYS AND REMOVE THE LATH LINE AT THE CONCLUSION OF THE PROJECT.

HEIGHT OF CONSTRUCTION EQUIPMENT

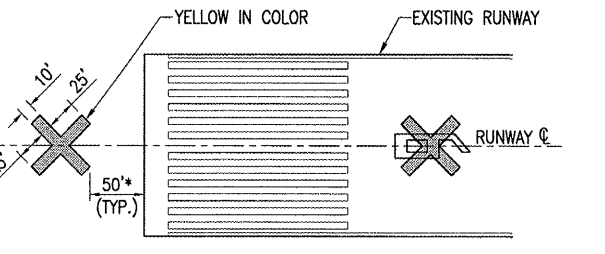
THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 25 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A SEMI-TRACTOR AND TRAILER.

EROSION CONTROL

THE PROPOSED CONSTRUCTION WILL NOT DISTURB MORE THAN 1 ACRES OF LAND, THEREFORE AN EROSION CONTROL PLAN WILL NOT BE NECESSARY.

ENGINEER'S FIELD OFFICE

THE LOCATION OF THE PROPOSED CONSTRUCTION TRAILER WILL BE AS SHOWN.



* UNLESS OTHERWISE NOTED
DETAIL OF RUNWAY CLOSURE MARKER
NOT TO SCALE

UTILITY NOTE

THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES AND AGENCIES WHICH HAVE LINES OR CONDUITS IN THE PROPOSED WORK AREA. ALL LINES AND CONDUITS SHALL BE LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY WORK BEGINS. THE CONTRACTOR WILL CALL J.U.L.I.E. (1-800-892-0123 OR 811) TO ACCOMPLISH THE ABOVE.

THE CONTRACTOR SHALL COORDINATE WITH THE FAA AND AIRPORT STAFF TO LOCATE AIRFIELD UTILITIES NOT OWNED BY THE MUNICIPAL OR PRIVATE UTILITY COMPANIES.

J.U.L.I.E. INFORMATION

COUNTY.....MACON
CITY.....DECATUR
TOWNSHIP.....LONG CREEK
SECTION NO.....20 & 21
ADDRESS.....DECATUR AIRPORT
AIRPORT ROAD
DECATUR, ILLINOIS 62524

LEGEND

- EXISTING IMPROVEMENTS
EXISTING BUILDINGS
PROPOSED CONSTRUCTION IMPROVEMENTS
PROPOSED EQUIPMENT/VEHICLE PARKING AND HAUL ROUTE
EXISTING AIRPORT PROPERTY LINE
BENCHMARK/CONTROL POINT
BARRICADES (PLACED WHEN REQUIRED)

Project information including: DECATUR AIRPORT, DECATUR, ILLINOIS, PROJECT NO. 3-17-0033-2, HANSON PROFESSIONAL SERVICES INC., PROJECT: RECONSTRUCT 960 FEET OF RUNWAY 6-24, PROPOSED SAFETY PLAN, SHEET 3 OF 15.

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