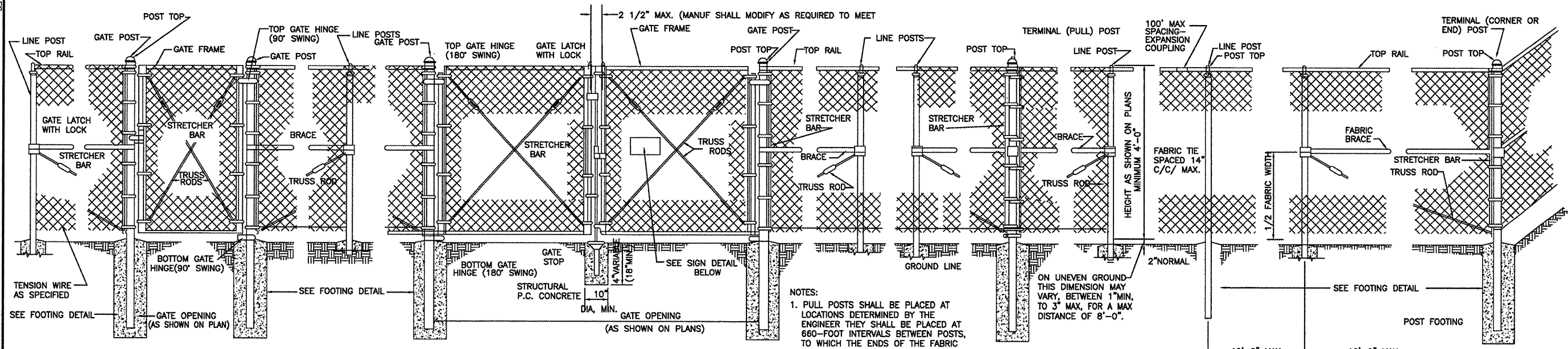


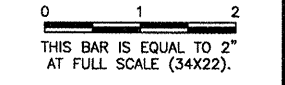
IMAGE FILES

DU071

K:\Dupage\0425704-Apron_Ph\Draw\Sheets\
 FILE: 27_apron-fendtl.dwg
 LAYOUT: Layout1
 UPDATE BY: jlinke
 SURVEY BOOK #
 DATE: Fri 1/23/09 10:19am
 XREF DWG: tbcont.dwg
 tb.dwg



REVISIONS		
NUMBER	BY	DATE



- NOTES:
- PULL POSTS SHALL BE PLACED AT LOCATIONS DETERMINED BY THE ENGINEER THEY SHALL BE PLACED AT 660-FOOT INTERVALS BETWEEN POSTS, TO WHICH THE ENDS OF THE FABRIC ARE CLAMPED OR MIDWAY BETWEEN SUCH POSTS WHEN THE DISTANCE IS LESS THAN 1320' AND GREATER THAN 660'
 - WHERE FENCE HAS A CHANGE IN DIRECTION OF 15° OR MORE, A TERMINAL POST SHALL BE PLACED AS SHOWN ABOVE.

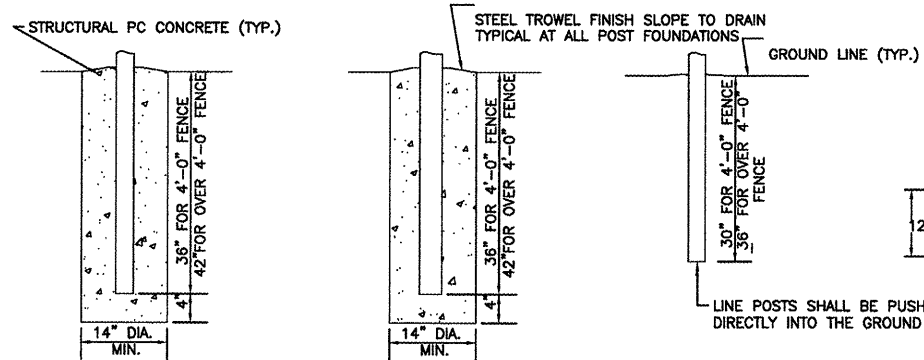
PEDESTRIAN GATE ARRANGEMENT

VEHICLE GATE ARRANGEMENT

PULL POST ARRANGEMENT

LINE POST ARRANGEMENT

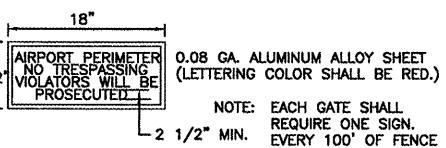
CORNER OF END POST ARRANGEMENT



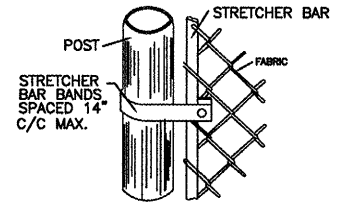
FOOTING FOR TERMINAL POST

FOOTING FOR GATE POST

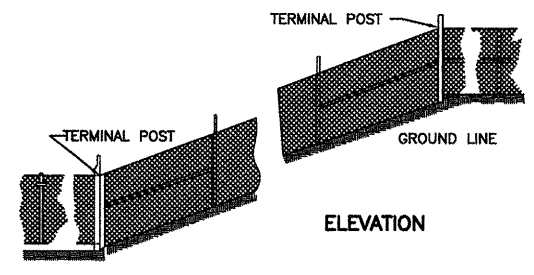
FOOTING FOR LINE POST



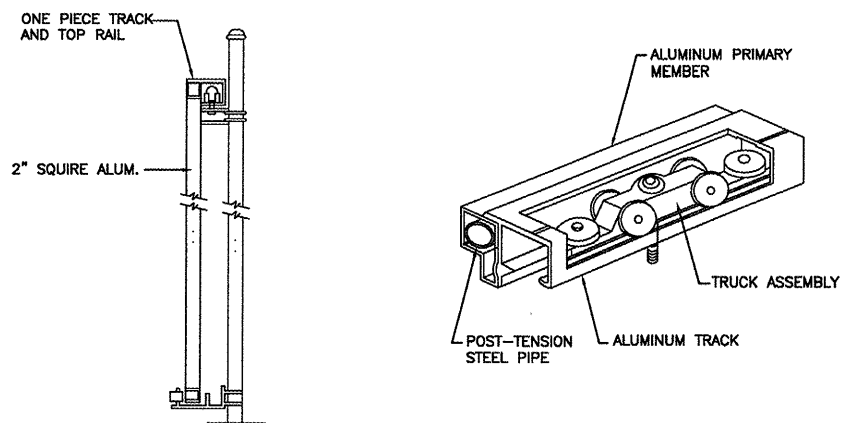
SIGN DETAIL



METHOD OF FASTENING STRETCHER BAR TO POST



FENCE INSTALLATION ON SLOPES



ROLLER ASSEMBLY FOR SLIDING GATE

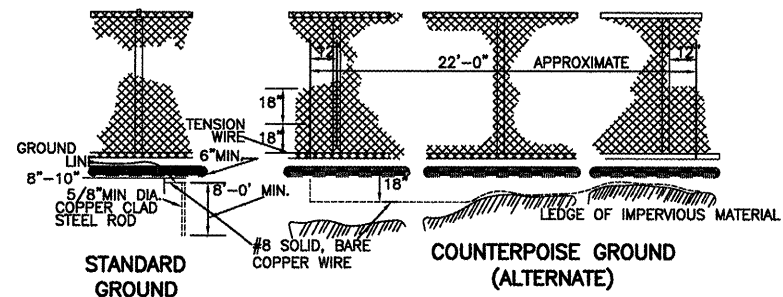
NOTE: GATE AND ROLLERS SHALL BE MOUNTED INBOARD
 CONTRACTOR SHALL SUBMIT SHOP DRAWINGS ENCLOSED
 TRUCK ROLLER ASSEMBLY FOR SLIDING DRIVEWAY GATES.
 (PAGE-FORTRESS GATE OR EQUAL)



METHOD OF TYING FABRIC TO TENSION WIRE



METHOD OF TYING FABRIC TO PIPE



PROTECTIVE ELECTRICAL GROUND

- NOTES:
- CONTINUOUS FENCE SHALL BE GROUNDED AT INTERVALS NOT EXCEEDING 1000' EXCEPT THERE SHALL BE A GROUND NOT EXCEEDING 100 FT. FROM A GATE IN EACH SECTION OF THE FENCE ADJACENT TO THE GATE.
 - FENCE UNDER POWER LINE SHALL BE GROUNDED BY THREE GROUNDS, ONE DIRECTLY UNDER THE CROSSING AND ONE ON EACH SIDE 25 TO 50 FT. AWAY. A SINGLE GROUND SHALL BE LOCATED DIRECTLY UNDER EACH TELEPHONE WIRE OR CABLE CROSSING.
 - THE COUNTERPOISE SHALL BE USED ONLY WHERE IT IS IMPOSSIBLE TO DRIVE A GROUND ROD BECAUSE OF AN IMPERVIOUS EARTH STRUCTURES.
 - THE GROUND WIRE SHALL BE CONNECTED TO FABRIC, TENSION WIRE, AND THE GROUND ROD BY A MECHANICAL CLAMP OF CAST BRONZE BODY AND BRONZE OR STAINLESS STEEL BOLTS AND WASHERS.

DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 SOUTH FLIGHT CENTER APRON - PHASE 4

FENCING DETAILS

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DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	MJS / DKP
APPROVED BY:	MJS
DATE:	01/30/09
JOB No:	04257-04-00-00
A.I.P. PROJECT:	3-17-0017-B18
ILLINOIS PROJECT:	DPA-3391
SHEET	22 OF 31 SHEETS