

1. COVER SHEET, INDEX OF SHEETS & STATE STANDARDS
2. SUMMARY OF QUANTITIES & GENERAL NOTES
3. TYPICAL SECTIONS
- 4.-7. PAVEMENT PLAN
- 8.-11. PAVEMENT MARKING PLAN
- 12.-23. IDOT DISTRICT 1 STANDARD DETAILS

DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

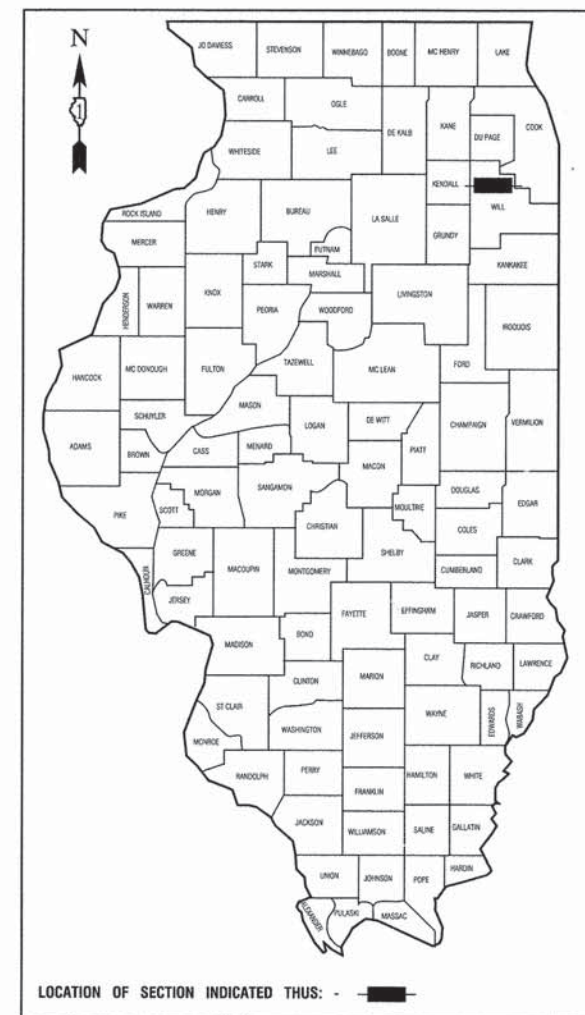
HIGHWAY STANDARDS

- 000001-06 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 424001-08 PERPENDICULAR CURB RAMPS
- 442201-03 CLASS C AND D PATCHES
- 606001-06 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
- 701006-05 OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
- 701301-04 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701311-03 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
- 701427-03 LANE CLOSURE, MULTILANE INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
- 701502-06 URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
- 701601-09 URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
- 701602-07 URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
- 701701-09 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701801-05 SIDEWALK, CORNER OR CROSSWALK CLOSURE
- 701901-04 TRAFFIC CONTROL DEVICES
- 780001-05 TYPICAL PAVEMENT MARKINGS
- 886001-01 DETECTOR LOOP INSTALLATIONS
- 886006-01 TYPICAL LAYOUTS FOR DETECTOR LOOPS

**FAU281 (CROSSROADS PARKWAY)
NORTH CENTER BOULEVARD TO FAU 351 (VETERANS PARKWAY)
ROADWAY RESURFACING
SECTION NO.: 15-00062-00-RS
PROJECT NO.: M-4003 (487)
VILLAGE of ROMEOVILLE
WILL COUNTY
JOB NO.: C-91-219-15**

F. A. ID. ETC.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
429	15-00062-00-RS	WILL.	23	1
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003 (487)		

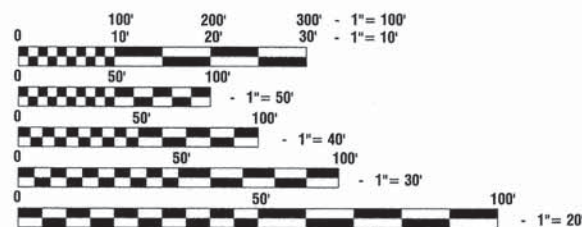
CONTRACT #61B63



LOCATION OF SECTION INDICATED THIS: - [black rectangle] -

CROSSROADS PARKWAY	
2012 ADT -	10,800
2040 ADT -	13,000
POSTED SPEED LIMIT -	30 mph
DESIGN PERIOD -	20 YEARS
DESIGN SPEED LIMIT -	30 mph
STREET CLASSIFICATION -	MAJOR COLLECTOR

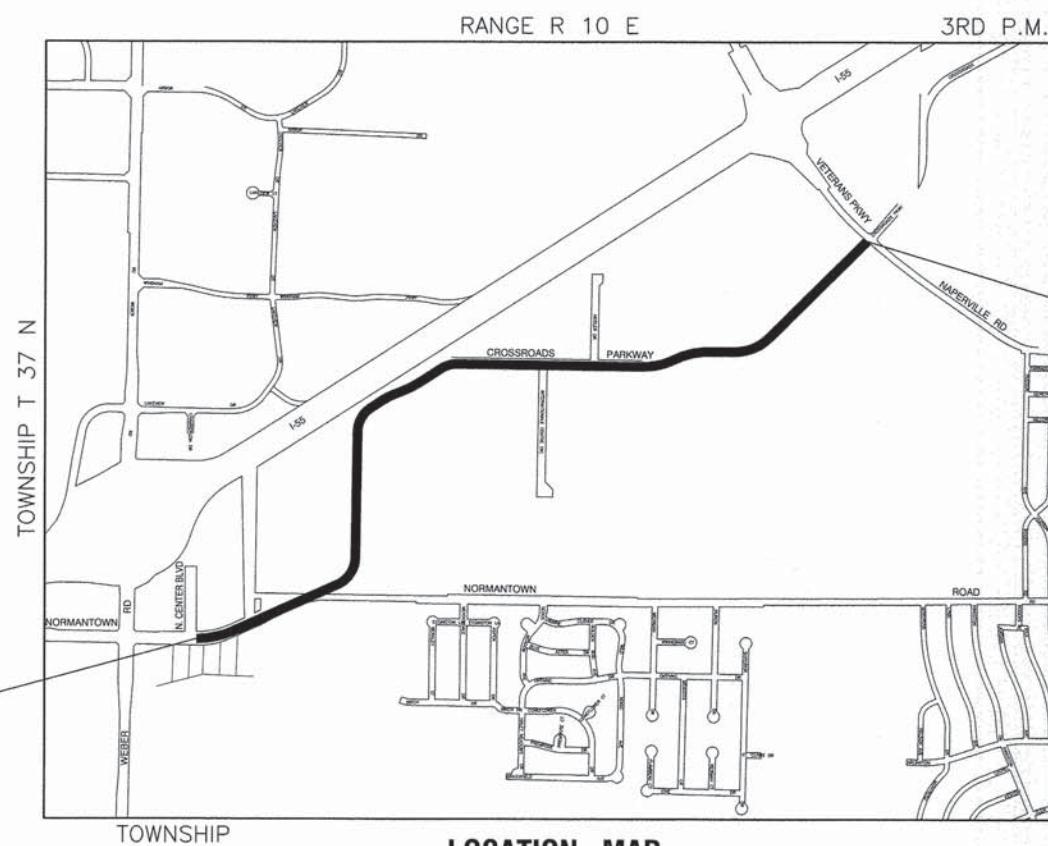
SCALES
 PLAN - 1"=50'
 PROFILE HORIZ. - 1"=50'
 PROFILE VERT. - 1"=5'
 CROSS SECTIONS - 1"=10'



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

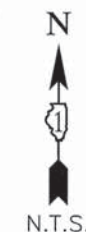
J. U. L. I. E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123 or 811

CONTRACT NO. 61B63



LOCATION MAP

GROSS LENGTH= 9,106 FEET= 1.725 MILES
 NET LENGTH= 9,106 FEET= 1.725 MILES



N.T.S.

END IMPROVEMENTS
 CROSSROADS PARKWAY
 STA 103+10.87

BEGIN IMPROVEMENTS
 CROSSROADS PARKWAY
 STA 12+05.30

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

Approved: *John D. Noah* 3/10/15
 Village of Romeoville

Passed: APRIL 8, 2015
C. Holt CHRISTOPHER HOLT
 District 1 Engineer of Local Roads & Streets

Released for Bid Based on Limited Review: *John F. ...* April 10, 2015
 Deputy Director of Highways, Region 1 Engineer

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PREPARED BY OR UNDER THE DIRECT SUPERVISION OF:

Nancy L. ...
 2/24/15



SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE
S.I.	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 0005
*	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	400	400
	40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUNDS	31470	31470
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	10	10
	40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1980	1980
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	400	400
	40603153	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	TON	5376	5376
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	895	895
	42400800	DETECTABLE WARNINGS	SQ FT	216	216
	44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	46625	46625
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	79	79
	44000600	SIDEWALK REMOVAL	SQ FT	635	635
	44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	20	20
	44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	31	31
	44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	45	45
	44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	160	160
	60250200	CATCH BASINS TO BE ADJUSTED	EACH	70	70
	60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1
	60255500	MANHOLES TO BE ADJUSTED	EACH	1	1
	60266600	VALVE BOXES TO BE ADJUSTED	EACH	1	1
	67100100	MOBILIZATION	L SUM	1	1
	70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1
	70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1
	70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1

* - INDICATES SPECIALTY ITEMS

SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE
S.I.	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 0005
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	6000	6000
	70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1000	1000
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	970	970
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	26320	26320
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3045	3045
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1725	1725
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	365	365
*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	2000	2000
	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1460	1460
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	75	75
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	6	6
	Z0004538	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 10"	SQ YD	62	62
	XX006343	SEEDING (COMPLETE)	SQ YD	400	400

GENERAL NOTES

* - INDICATES SPECIALTY ITEMS

1. THE ROBINSON ENGINEERING, LTD. FIELD OFFICE (708-331-6700), ERIC BJORK AT THE VILLAGE OF ROMEVILLE (815-886-1870). AND TOM PAWLOWICZ AT THE VILLAGE OF BOLINGBROOK (630-226-8850) ALL SHALL BE NOTIFIED TWO (2) WORKING DAYS BEFORE CONSTRUCTION BEGINS.
2. BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 AND (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION REQUIRED)
3. UTILITIES INDICATED ON THE PLANS ARE PROVIDED FOR THE CONTRACTOR'S USE AND ARE BASED UPON INFORMATION AVAILABLE AT THE TIME OF THE ADVERTISEMENT FOR BIDS. THE OWNER AND ENGINEER DO NOT GUARANTEE THE ACCURACY OF UTILITY INFORMATION.
4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
5. THE THICKNESS OF HMA MIXTURE STATED IN THE SPECIFICATIONS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA SURFACE IS PLACED.
6. ACCESS TO DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES BY LIMITING CURB AND GUTTER REPAIR TO ONE-HALF THE DRIVEWAY WIDTH AT ONE TIME AS WELL AS TEMPORARY AGGREGATE. ANY TEMPORARY AGGREGATE REQUIRED SHALL BE CONSIDERED INCLUDED IN THE COST OF THE RELATED PAY ITEM IT IS NEEDED FOR WHEN DIRECTED BY THE ENGINEER.
7. THE REMOVAL AND/OR REPLACEMENT OF ANY DRIVEWAYS, PAVEMENT, CURB, SIDEWALK, ETC. SHALL BE ACCOMPLISHED BY MEANS OF A SAW CUT JOINT, AT THE DIRECTION OF THE ENGINEER. SAW CUTTING WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE FOR THE VARIOUS REMOVAL ITEMS.
8. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR OTHER DRAINAGE STRUCTURES SHALL BE REMOVED BY THE END OF EACH DAY BY THE CONTRACTOR AT THEIR EXPENSE.
9. THE CONTRACTOR SHALL LEAVE ANY CLEAN EXCESS ORGANIC FILL EXCAVATED DURING THE CURB AND GUTTER AND SIDEWALK REMOVAL AND REPLACEMENT OPERATIONS ON SITE. ANY EXCESS MATERIAL SHALL BE SPREAD OR PLACED AT LOCATIONS DETERMINED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE FOR THE VARIOUS REMOVAL AND REPLACEMENT ITEMS. RESTORATION OF AREAS WHERE EXCESS MATERIALS IS PLACED SHALL BE PAID FOR AS SEEDING (COMPLETE).
10. CLASS D PATCHING QUANTITIES FOR THIS CONTRACT SHALL BE PERFORMED AT THE DIRECTION OF THE ENGINEER AFTER PAVEMENT MILLING.

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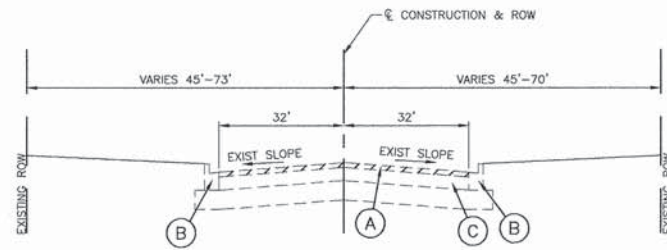
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PLOT DATE = 02-24-15	CHECKED -- AG	REVISION --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAU 429 (CROSSROADS PARKWAY)
ROADWAY RESURFACING
SUMMARY OF QUANTITIES & GENERAL NOTES

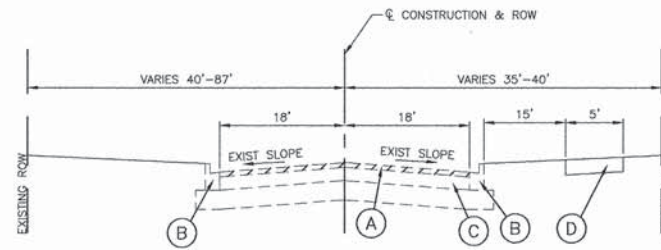
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
429	15-00062-00-RS	WILL	23	2
CONTRACT NO. 61B63				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (487)				



EXISTING TYPICAL SECTION

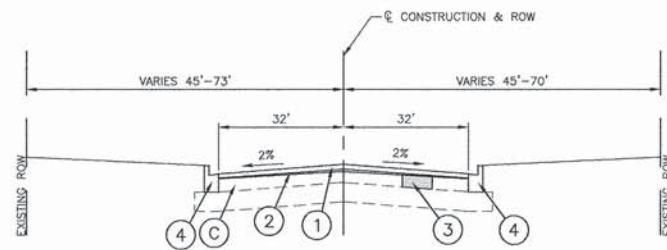
CROSSROADS PARKWAY/NORMANTOWN ROAD
NORTH CENTER BOULEVARD TO NORMANTOWN ROAD
STA 12+05.30 TO STA 27+00.00



EXISTING TYPICAL SECTION

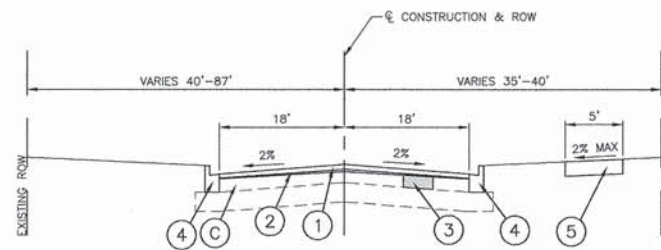
CROSSROADS PARKWAY
NORMANTOWN ROAD TO VETERANS PARKWAY
STA 27+00 TO STA 103+10.87

- EXISTING LEGEND**
- (A) HOT MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 - (B) EXISTING CURB & GUTTER TO BE REMOVED AT LOCATIONS SHOWN ON PLANS OR DIRECTED BY ENGINEER
 - (C) EXISTING HOT-MIX ASPHALT PAVEMENT (VARIES 10" TO 15")
 - (D) EXISTING PCC SIDEWALK TO BE REMOVED AT LOCATIONS SHOWN ON PLANS OR DIRECTED BY ENGINEER



PROPOSED TYPICAL SECTION

CROSSROADS PARKWAY/NORMANTOWN ROAD
NORTH CENTER BOULEVARD TO NORMANTOWN ROAD
NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING
STA 12+05.30 TO STA 27+00.00



PROPOSED TYPICAL SECTION

CROSSROADS PARKWAY
NORMANTOWN ROAD TO VETERANS PARKWAY
NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING
STA 27+00 TO STA 103+10.87

- PROPOSED LEGEND**
- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, IL-9.5mm 2"
 - (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 - (3) CLASS D PATCH, 10" AT LOCATIONS SHOWN ON PLANS OR DIRECTED BY ENGINEER
 - (4) PROPOSED CURB AND GUTTER TO BE INSTALLED AT LOCATIONS SHOWN ON PLAN OR DIRECTED BY ENGINEER (IN KIND)
 - (5) PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK 5" (REPLACEMENT AT LOCATIONS DIRECTED BY THE ENGINEER)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS
(CONTRACTOR SHALL MILL BEFORE PATCHING)

MIXTURE TYPE	AIR VOIDS @ Ndes
RESURFACING	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, IL-9.5MM, 2"	3.5% @ 80 Gyr.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	3.5% @ 50 Gyr.
PATCHING	
CLASS D PATCHES, TYPE I, II, III, IV, (HMA BINDER IL-19.0mm); 10" (IN 3 LIFTS)	4% @ 70 Gyr.
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2" (IL 9.5 MM)	4% @ 50 Gyr.
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19mm); CE-8"	4% @ 50 Gyr.

NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN. FOR "AC TYPE" AND "PERCENT RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.
 2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.
- NOTE:
CLASS D PATCHES, TYPE I, II, III & IV AT APPROXIMATE STATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

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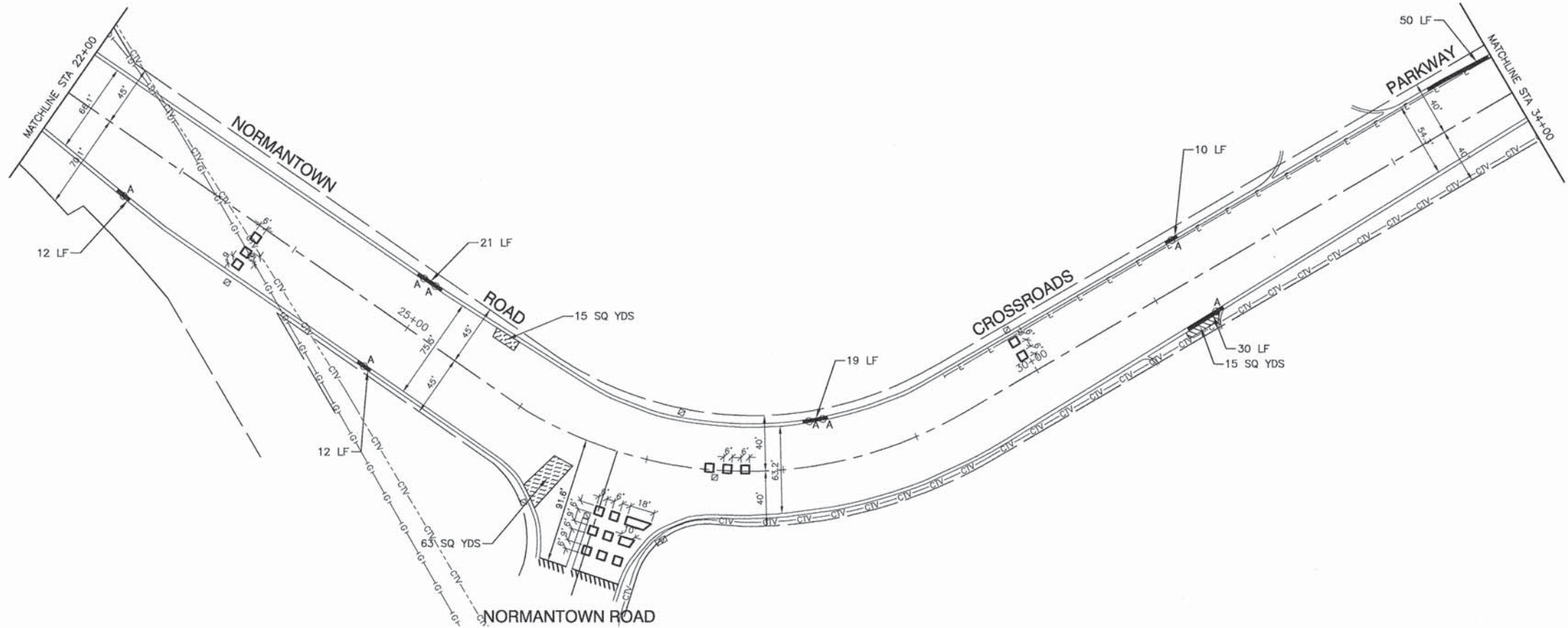
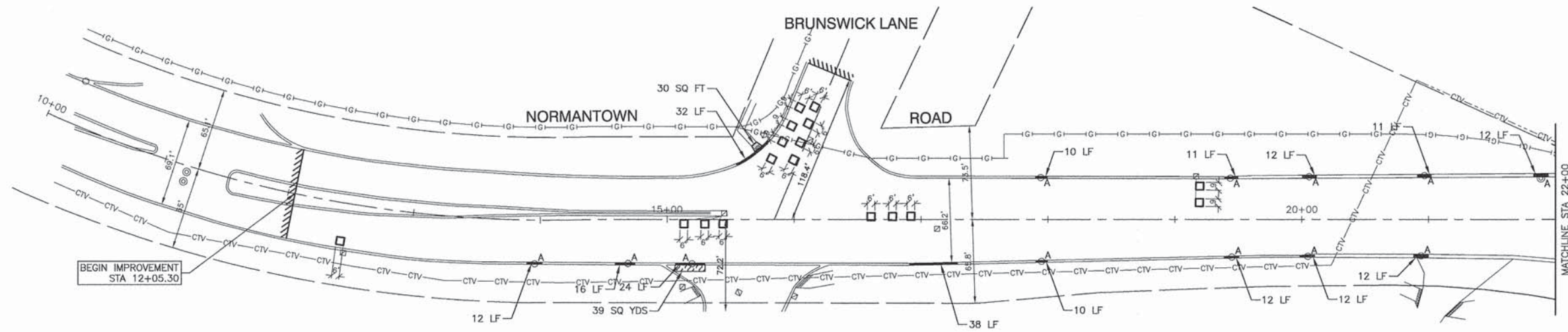
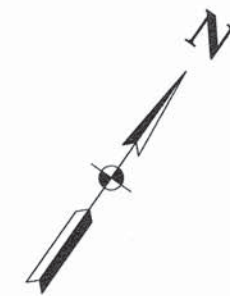
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAU 429 (CROSSROADS PARKWAY)
ROADWAY RESURFACING
TYPICAL SECTIONS

SCALE: NONE SHEET NO. 3 OF 23 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
429	15-00062-00-RS	WILL	23	3
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (487)			CONTRACT NO. 61B63	



LEGEND

- CLASS D PATCHES, 10"
- HMA DRIVEWAY REMOVAL AND REPLACEMENT
- CONCRETE SIDEWALK REMOVAL AND REPLACEMENT
- BUTT JOINTS
- CURB AND GUTTER REMOVAL AND REPLACEMENT
- "A" STRUCTURE TO BE ADJUSTED
- DETECTOR LOOP TO BE REPLACED
- DETECTABLE WARNING

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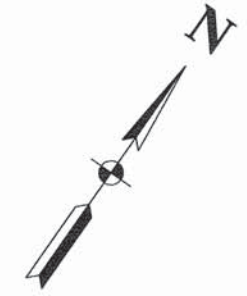
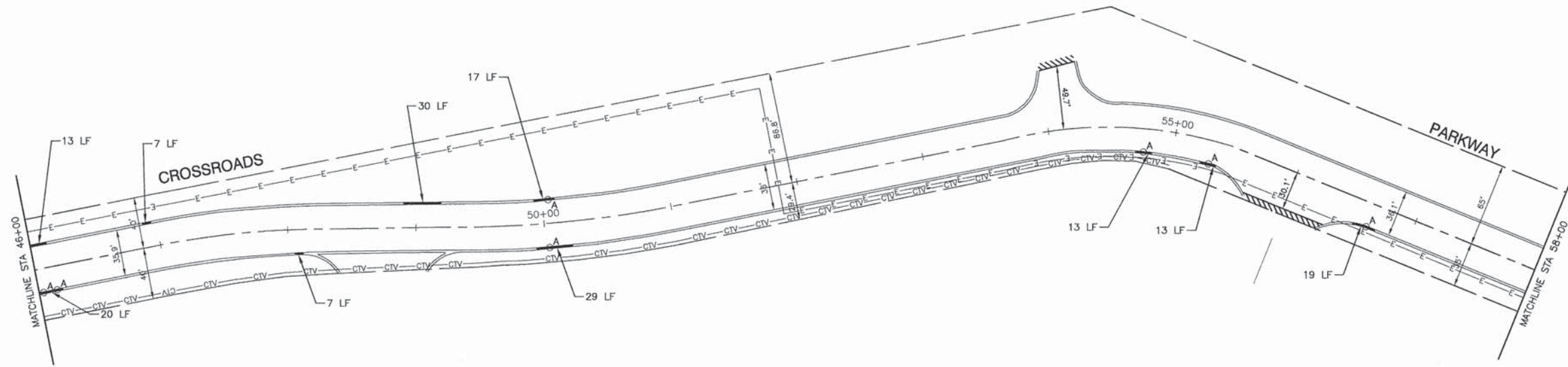
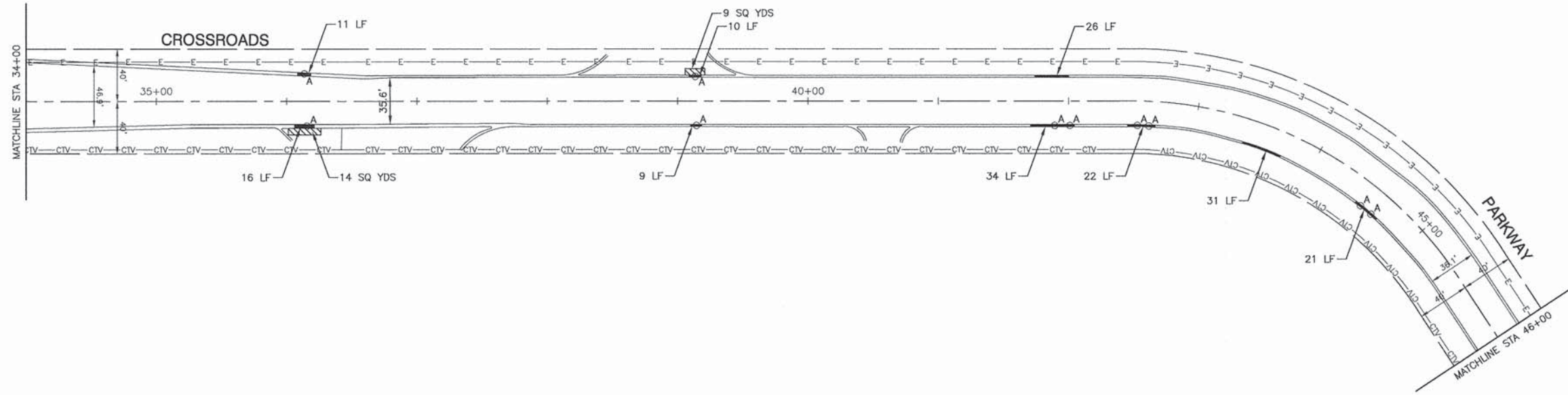
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAU 429 (CROSSROADS PARKWAY)
 ROADWAY RESURFACING
 PROPOSED PLAN**

SCALE: 1"=50' SHEET NO. 4 OF 23 SHEETS STA. 12+05.30 TO STA. 34+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
429	15-00062-00-RS	WILL	23	4
CONTRACT NO. 61B63				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (487)				



LEGEND

	CLASS D PATCHES, 10"
	HMA DRIVEWAY REMOVAL AND REPLACEMENT
	CONCRETE SIDEWALK REMOVAL AND REPLACEMENT
	BUTT JOINTS
	CURB AND GUTTER REMOVAL AND REPLACEMENT
	"A" STRUCTURE TO BE ADJUSTED
	DETECTOR LOOP TO BE REPLACED
	DETECTABLE WARNING

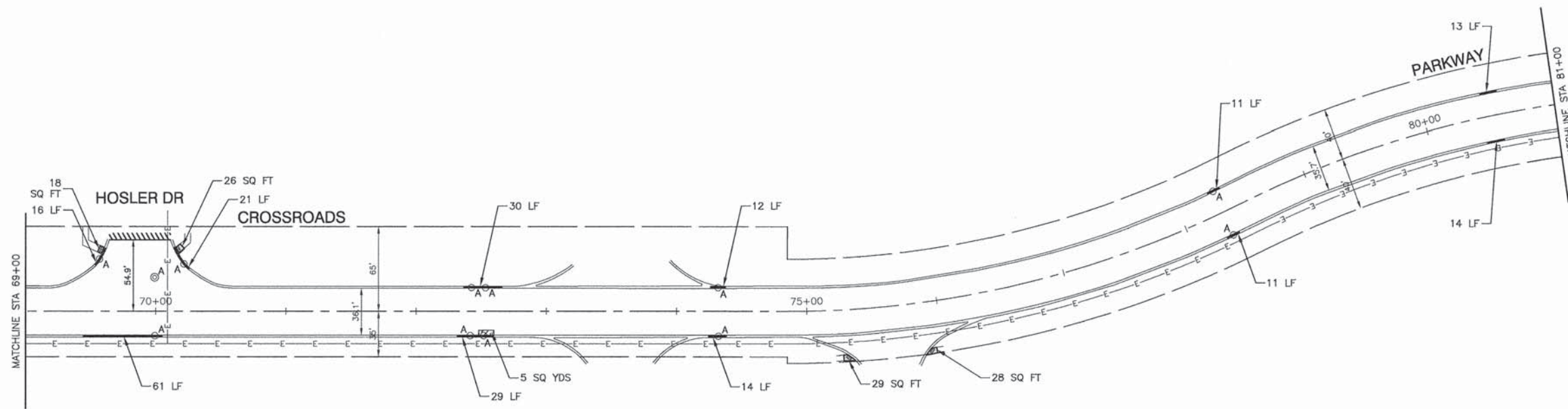
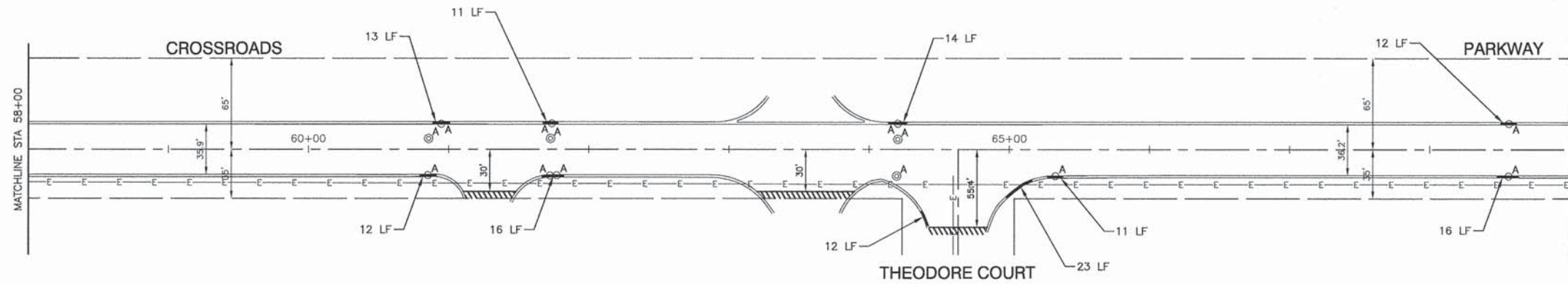
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CHECKED — AG	REVISD —

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

FAU 429 (CROSSROADS PARKWAY)
 ROADWAY RESURFACING
 PROPOSED PLAN
 SCALE: 1"=50' SHEET NO. 5 OF 23 SHEETS STA. 34+00 TO STA. 58+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
429	15-00062-00-RS	WILL	23	5
CONTRACT NO. 61B63				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (487)				



LEGEND

- CLASS D PATCHES, 10"
- HMA DRIVEWAY REMOVAL AND REPLACEMENT
- CONCRETE SIDEWALK REMOVAL AND REPLACEMENT
- BUTT JOINTS
- CURB AND GUTTER REMOVAL AND REPLACEMENT
- "A" STRUCTURE TO BE ADJUSTED
- DETECTOR LOOP TO BE REPLACED
- DETECTABLE WARNING

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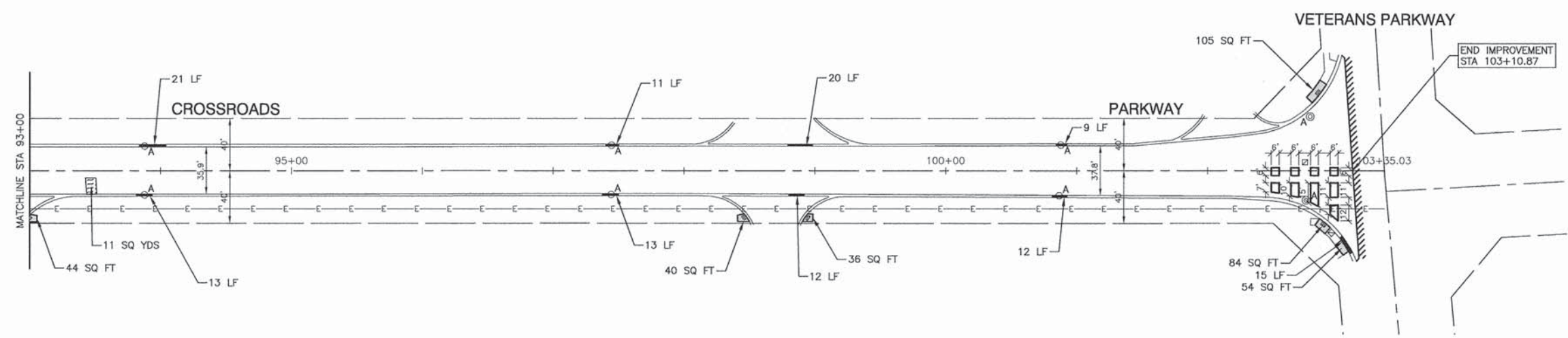
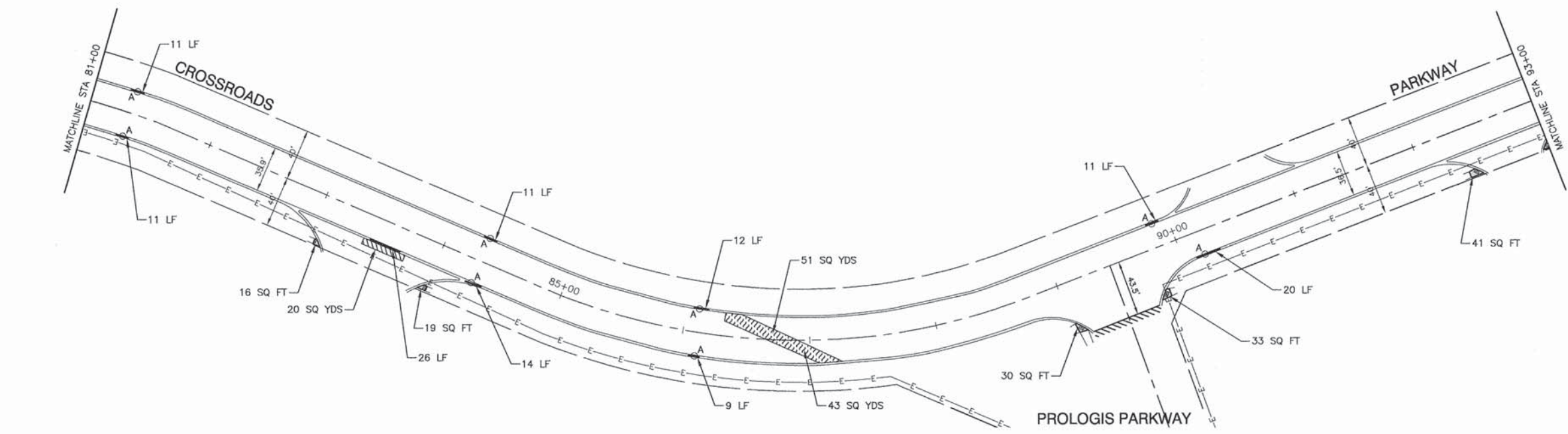
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

FAU 429 (CROSSROADS PARKWAY)
ROADWAY RESURFACING
PROPOSED PLAN

SCALE: 1"=50' SHEET NO. 6 OF 23 SHEETS STA. 58+00 TO STA. 81+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
429	15-00062-00-RS	WILL	23	6
CONTRACT NO. 61B63				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (487)				



LEGEND

- CLASS D PATCHES, 10"
- HMA DRIVEWAY REMOVAL AND REPLACEMENT
- CONCRETE SIDEWALK REMOVAL AND REPLACEMENT
- BUTT JOINTS
- CURB AND GUTTER REMOVAL AND REPLACEMENT
- "A" STRUCTURE TO BE ADJUSTED
- DETECTOR LOOP TO BE REPLACED
- DETECTABLE WARNING

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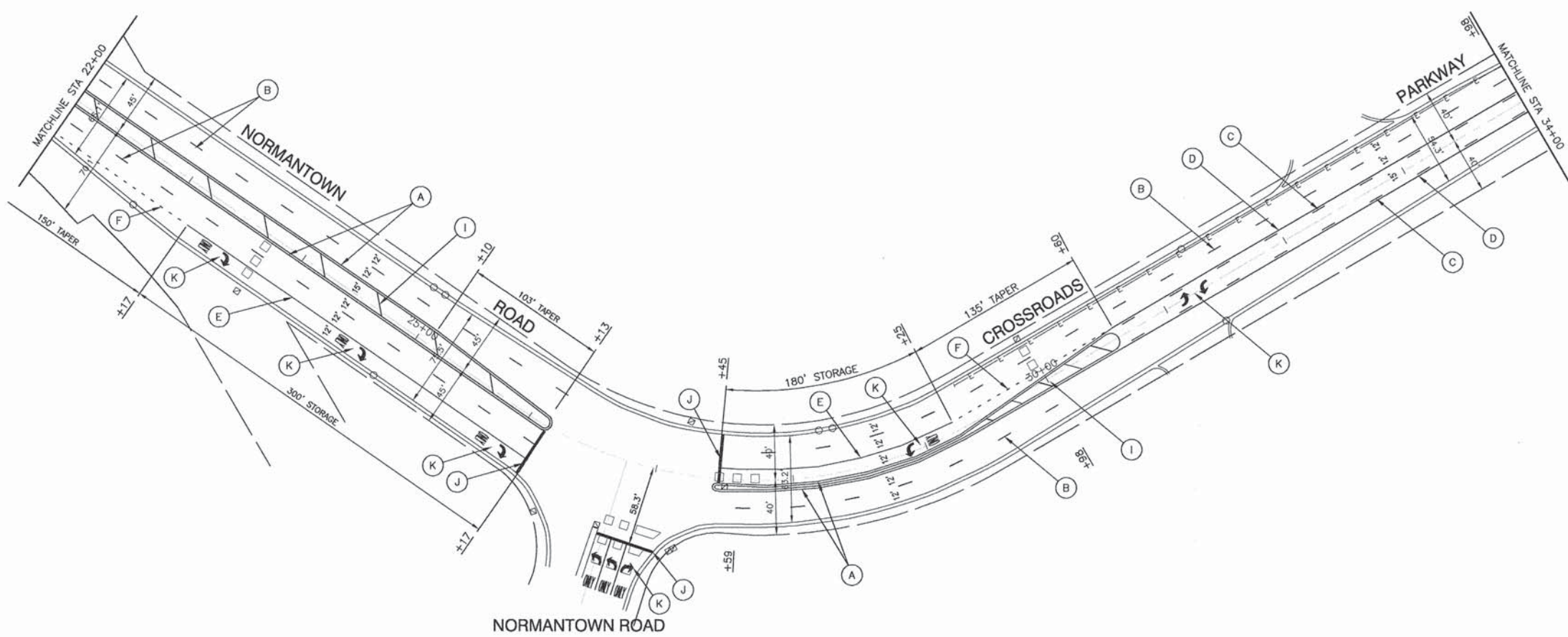
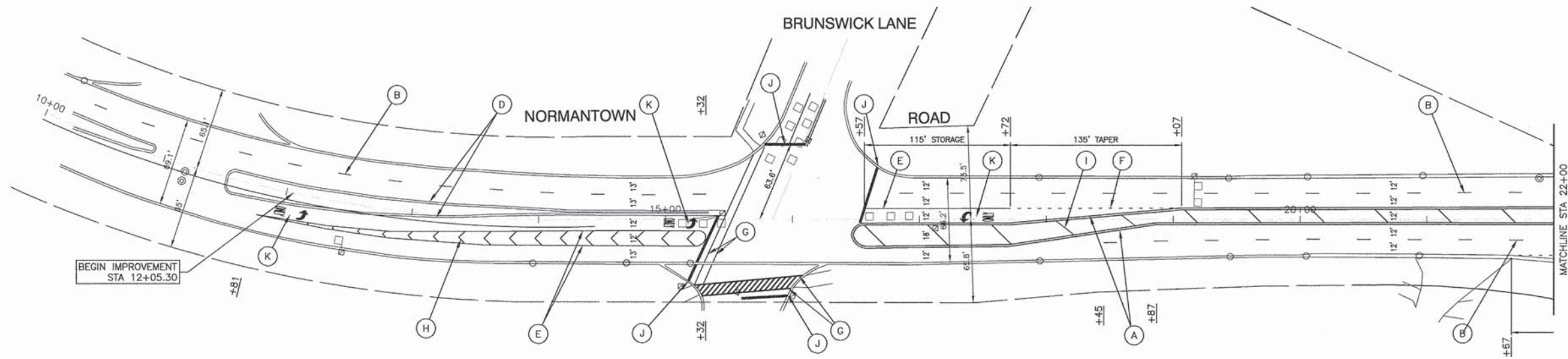
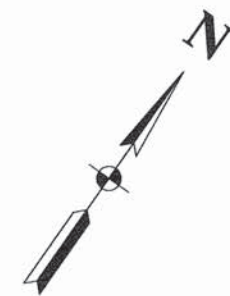
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAU 429 (CROSSROADS PARKWAY)
ROADWAY RESURFACING
PROPOSED PLAN**

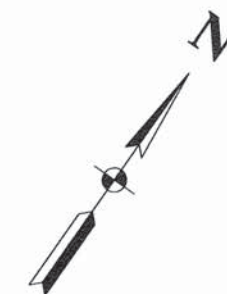
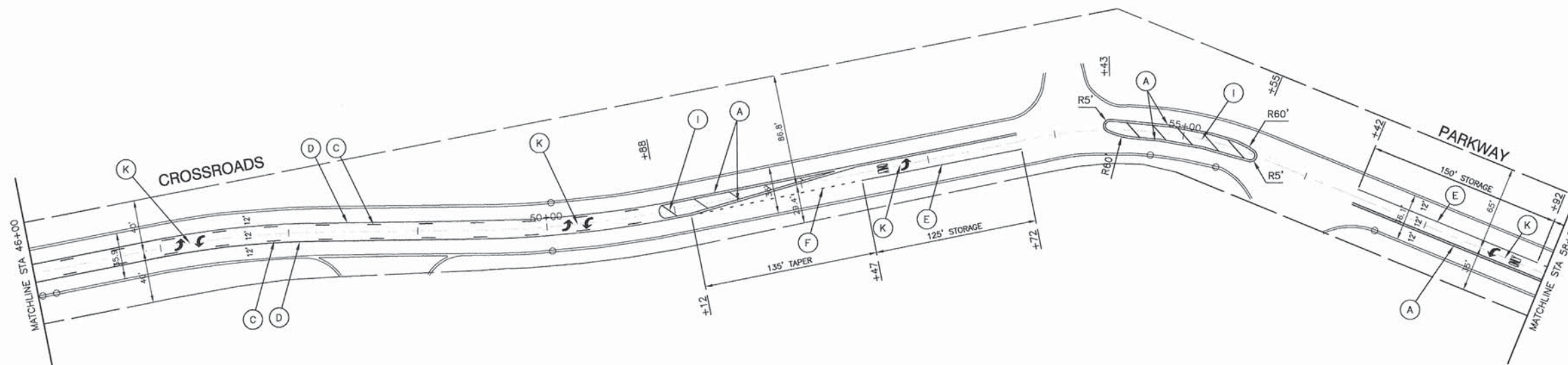
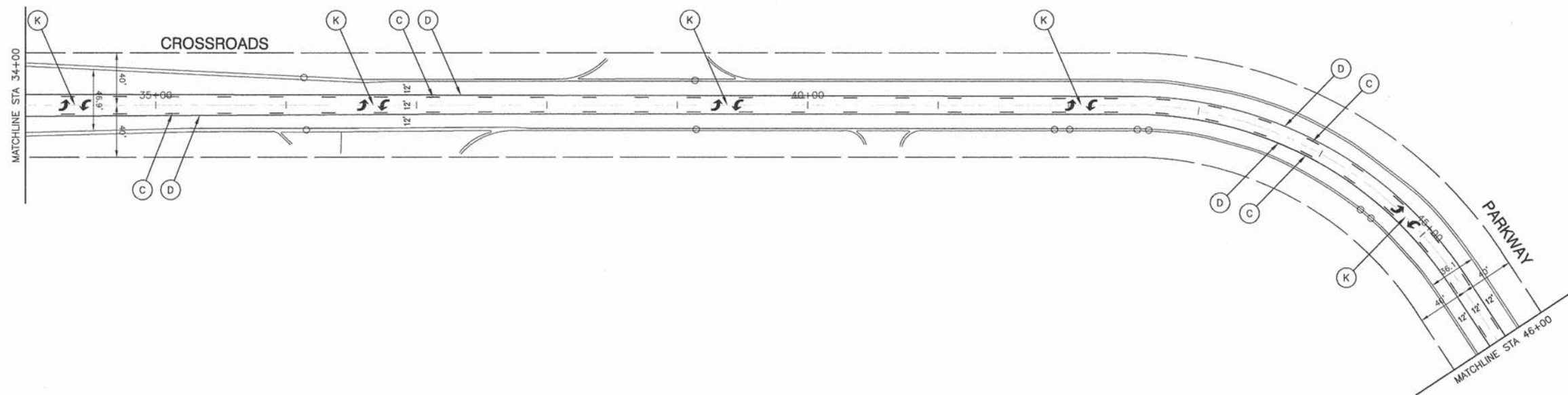
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F.A.U. RTE. 429	SECTION 15-00062-00-RS	COUNTY WILL	TOTAL SHEETS 23	SHEET NO. 7
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT M-4003 (487)	



- LEGEND**
- (A) 4" DOUBLE YELLOW LINE (11" OC)
 - (B) 4" WHITE SKIP DASH (10' LINE-30' SPACE)
 - (C) 4" YELLOW SKIP DASH (10' LINE-30' SPACE)
 - (D) 4" YELLOW LINE
 - (E) 6" WHITE LINE
 - (F) 6" WHITE SKIP DASH (2' LINE-6' SPACE)
 - (G) 6" WHITE CROSSWALK LINE
 - (H) 12" WHITE DIAGONAL LINE (20' C-C)
 - (I) 12" YELLOW DIAGONAL LINE (50' C-C)
 - (J) 24" WHITE STOP BAR
 - (K) LETTERS AND SYMBOLS - WHITE

FILE NAME = 14811-PLAN-01 - PVMK P01	USER NAME =	DESIGNED -- TAG	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAU 429 (CROSSROADS PARKWAY) ROADWAY RESURFACING PAVEMENT MARKING PLAN		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = 02-24-15	DRAWN -- FB	REVISOR --	REVISED --	SCALE: 1"=50'		SHEET NO. 8 OF 23 SHEETS		STA. 12+05.30 TO STA. 34+00		CONTRACT NO. 61B63	
	CHECKED -- AG	REVISOR --	REVISED --	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (487)							



LEGEND

- (A) 4" DOUBLE YELLOW LINE (11" OC)
- (B) 4" WHITE SKIP DASH (10' LINE-30' SPACE)
- (C) 4" YELLOW SKIP DASH (10' LINE-30' SPACE)
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- (H) 12" WHITE DIAGONAL LINE (20' C-C)
- (I) 12" YELLOW DIAGONAL LINE (50' C-C)
- (J) 24" WHITE STOP BAR
- (K) LETTERS AND SYMBOLS - WHITE

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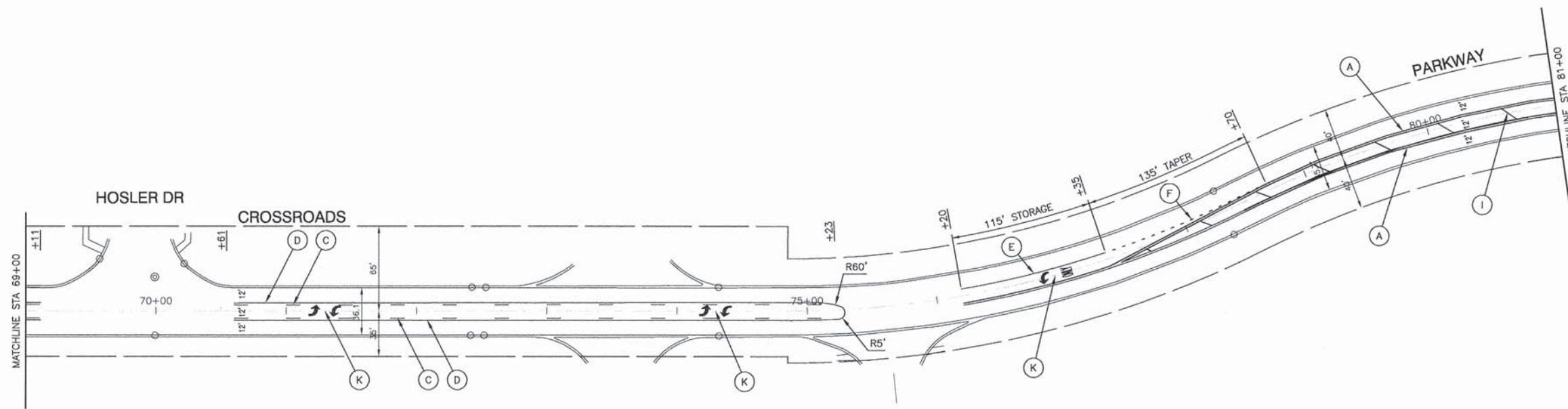
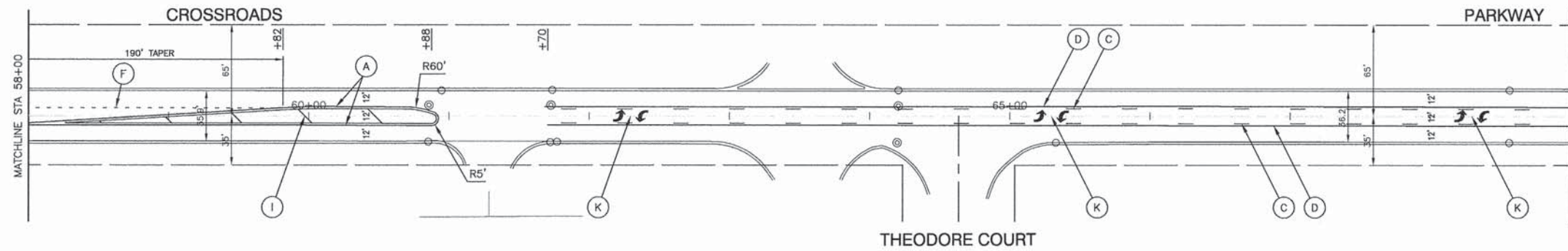
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PLOT SCALE =	DRAWN - FB	REVISD -
PLOT DATE = 02-24-15	CHECKED - AG	REVISD -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAU 429 (CROSSROADS PARKWAY)
ROADWAY RESURFACING
PAVEMENT MARKING PLAN

SCALE: 1"=50' SHEET NO. 9 OF 23 SHEETS STA. 34+00 TO STA. 58+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
429	15-00062-00-RS	WILL	23	9
CONTRACT NO. 61B63				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (487)				



LEGEND

- (A) 4" DOUBLE YELLOW LINE (11" OC)
- (B) 4" WHITE SKIP DASH (10' LINE-30' SPACE)
- (C) 4" YELLOW SKIP DASH (10' LINE-30' SPACE)
- (D) 4" YELLOW LINE
- (E) 6" WHITE LINE
- (F) 6" WHITE SKIP DASH (2' LINE-6' SPACE)
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- (H) 12" WHITE DIAGONAL LINE (20' C-C)
- (I) 12" YELLOW DIAGONAL LINE (50' C-C)
- (J) 24" WHITE STOP BAR
- (K) LETTERS AND SYMBOLS - WHITE

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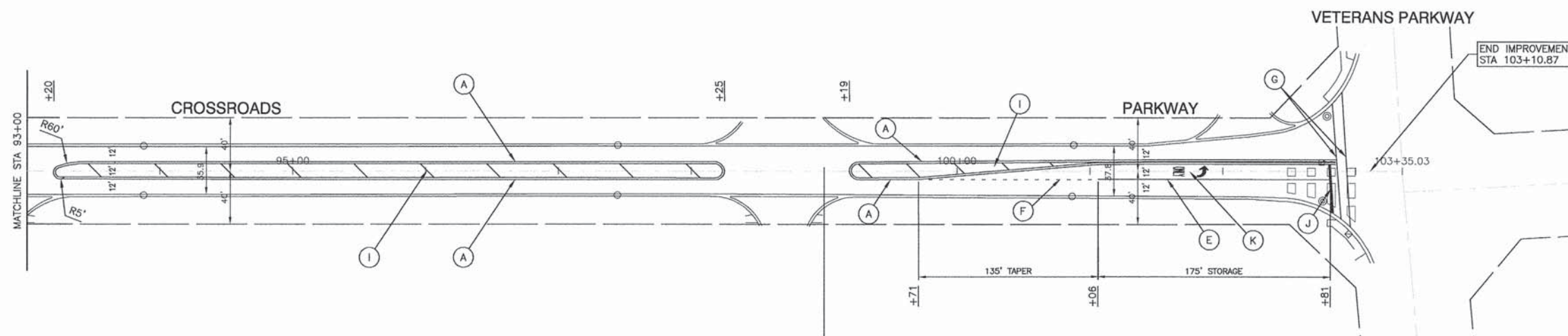
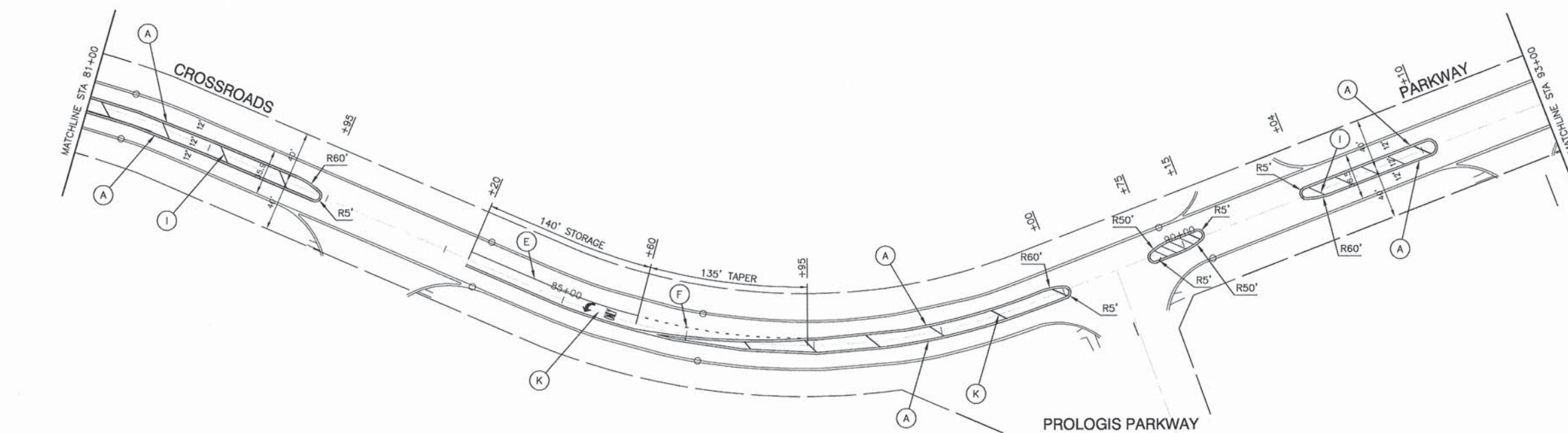
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PLOT DATE = 02-24-15	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAU 429 (CROSSROADS PARKWAY)
 ROADWAY RESURFACING
 PAVEMENT MARKING PLAN**

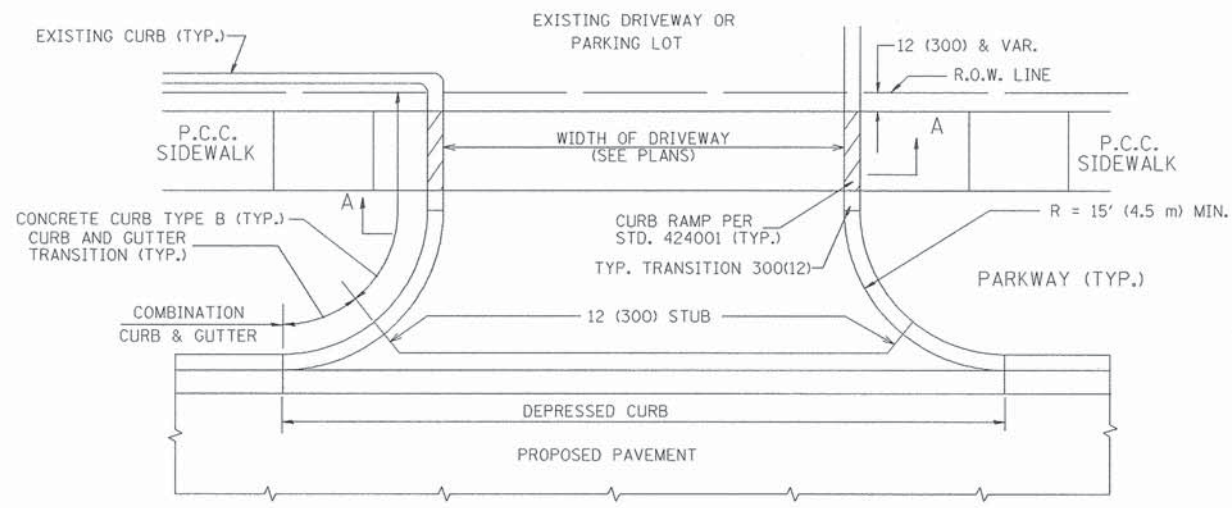
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F.A.U. RTE. 429	SECTION 15-00062-00-RS	COUNTY WILL	TOTAL SHEETS 23	SHEET NO. 10
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT M-4003 (487)	

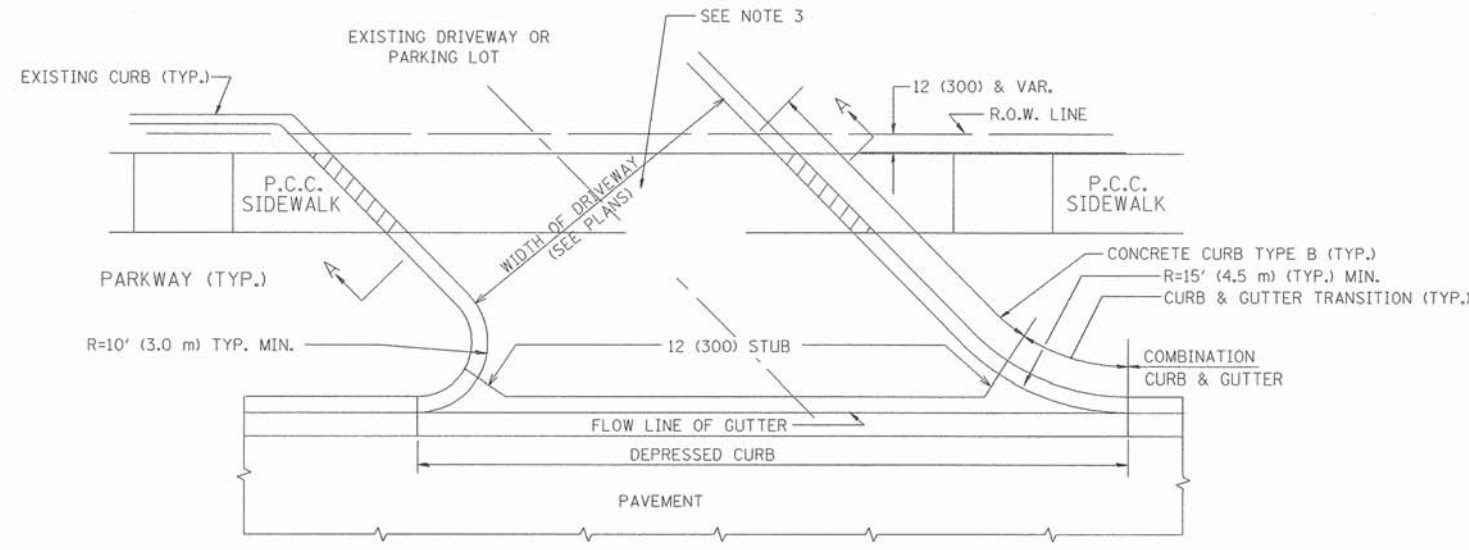


- LEGEND**
- (A) 4" DOUBLE YELLOW LINE (11" OC)
 - (B) 4" WHITE SKIP DASH (10' LINE-30' SPACE)
 - (C) 4" YELLOW SKIP DASH (10' LINE-30' SPACE)
 - (D) 4" YELLOW LINE
 - (E) 6" WHITE LINE
 - (F) 6" WHITE SKIP DASH (2' LINE-6' SPACE)
 - (G) 6" WHITE CROSSWALK LINE
 - (H) 12" WHITE DIAGONAL LINE (20' C-C)
 - (I) 12" YELLOW DIAGONAL LINE (50' C-C)
 - (J) 24" WHITE STOP BAR
 - (K) LETTERS AND SYMBOLS - WHITE

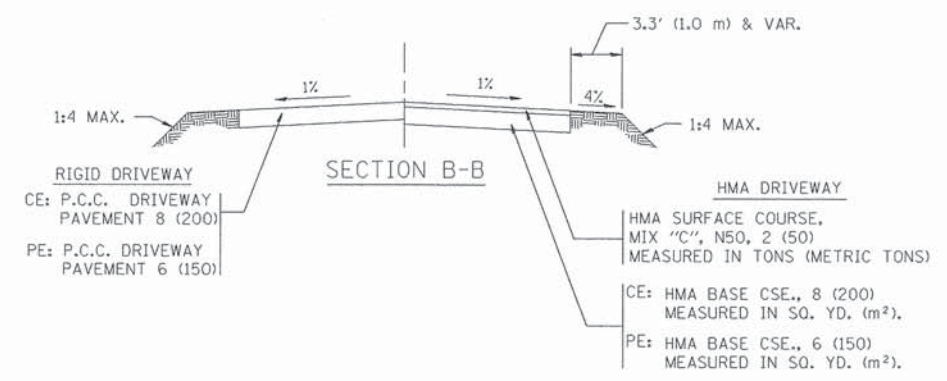
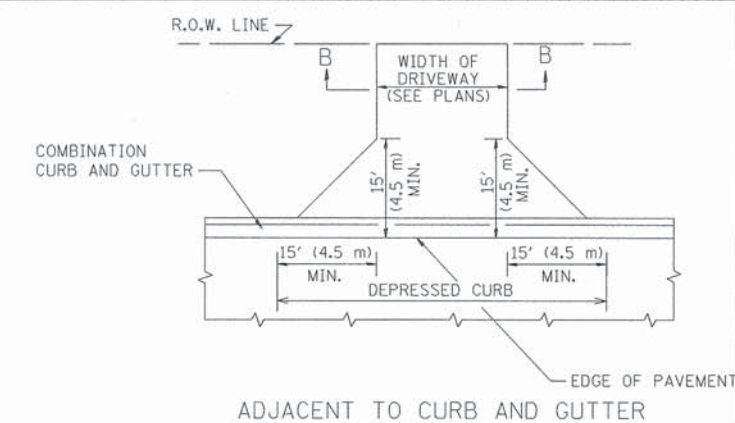
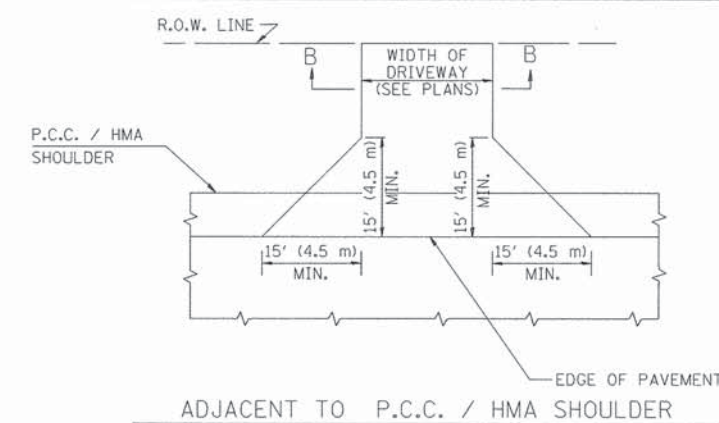
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	PLOT SCALE =	DRAWN - FB	REVISED -					429	15-00062-00-RS	WILL	23	11
PLOT DATE = 02-24-15	CHECKED - AG	REVISED -		SCALE: 1"=50'	SHEET NO. 11 OF 23 SHEETS	STA. 81+00 TO STA. 103+10.87	CONTRACT NO. 61B63					
							FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (487)					



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



RURAL FIELD ENTRANCE (FE)
 HMA SURFACE COURSE,
 MIX "C", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 AGGREGATE BASE CSE., TYPE B, 8 (200)
 MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

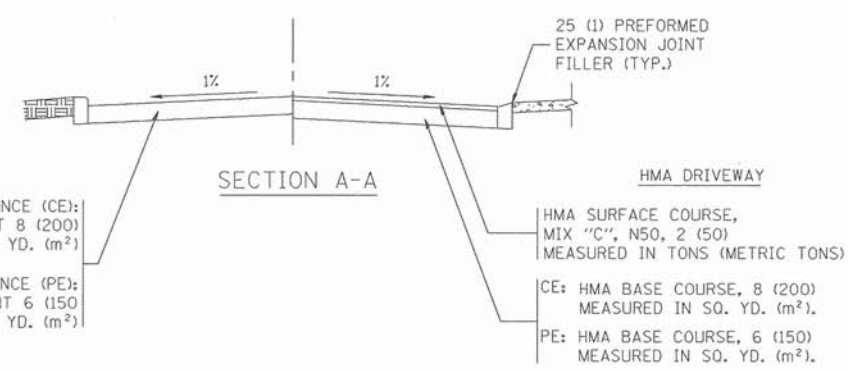
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



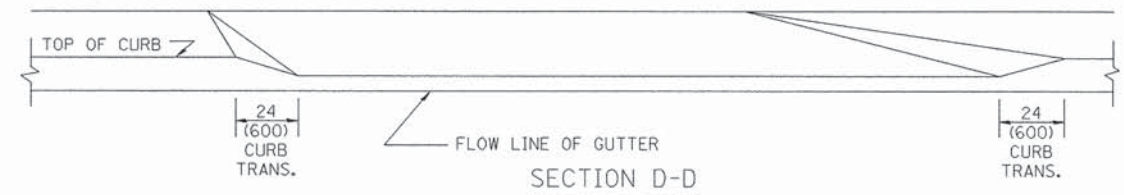
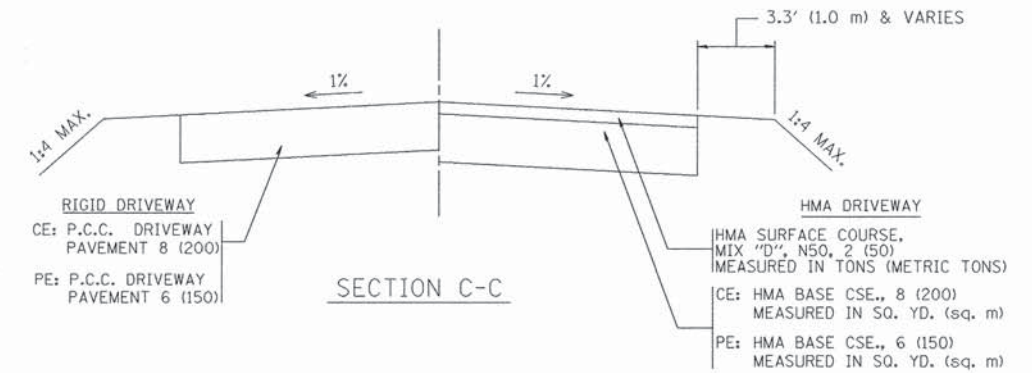
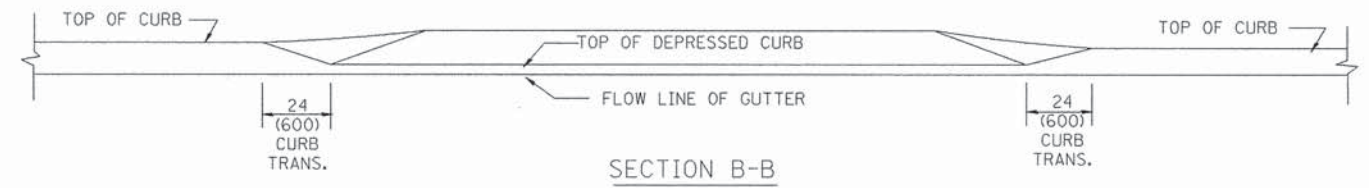
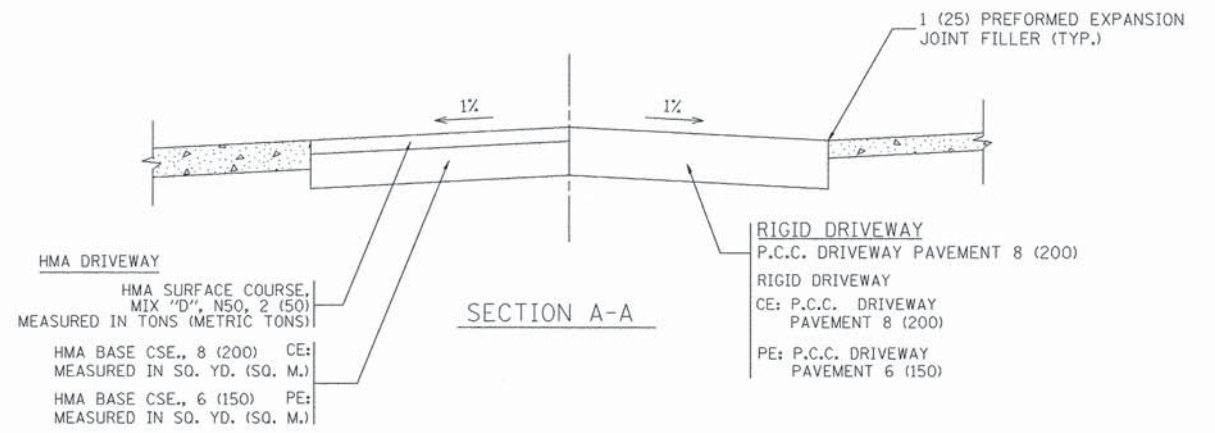
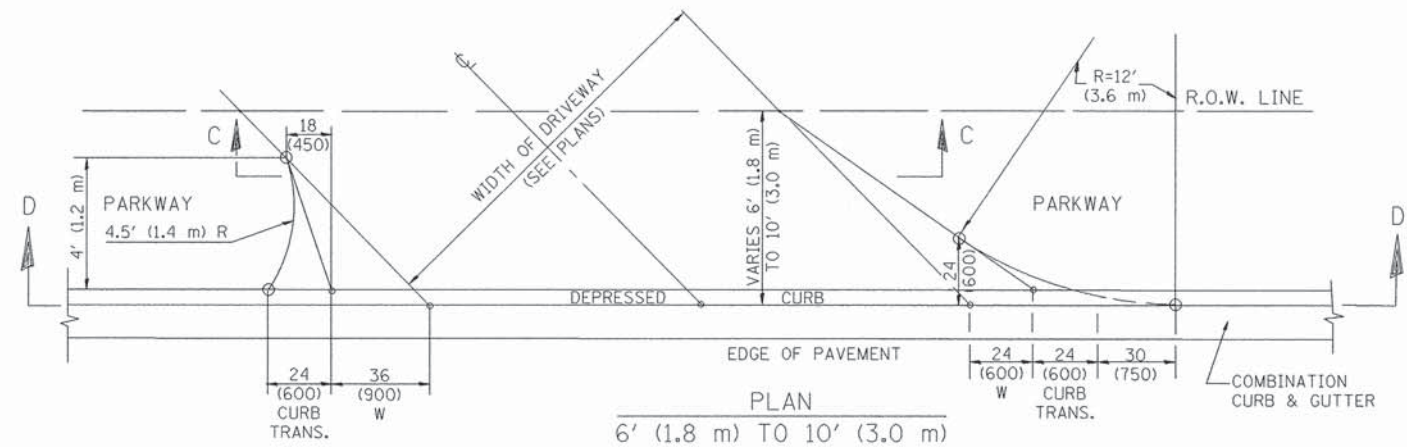
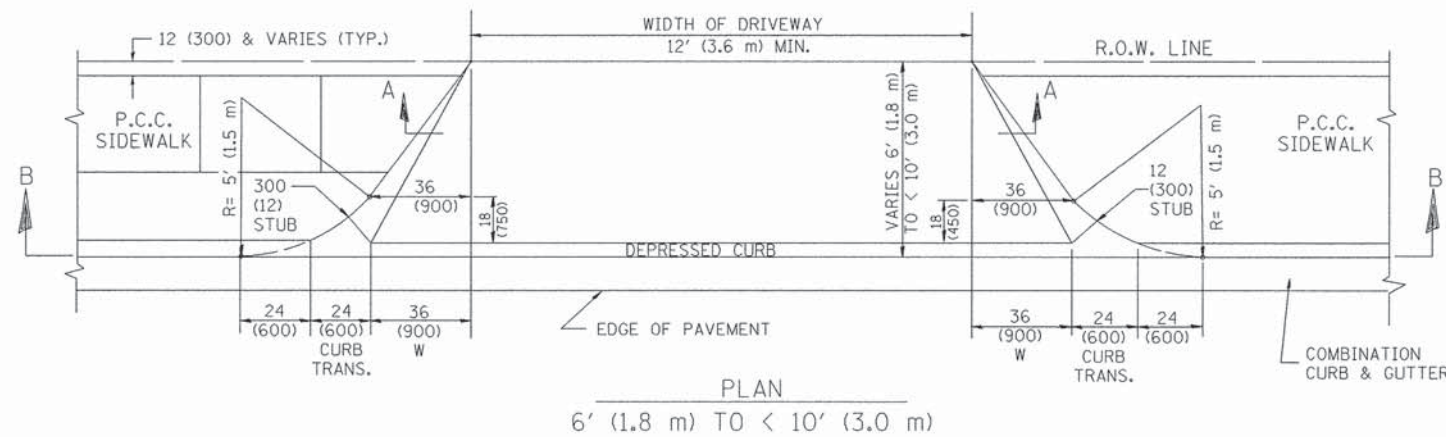
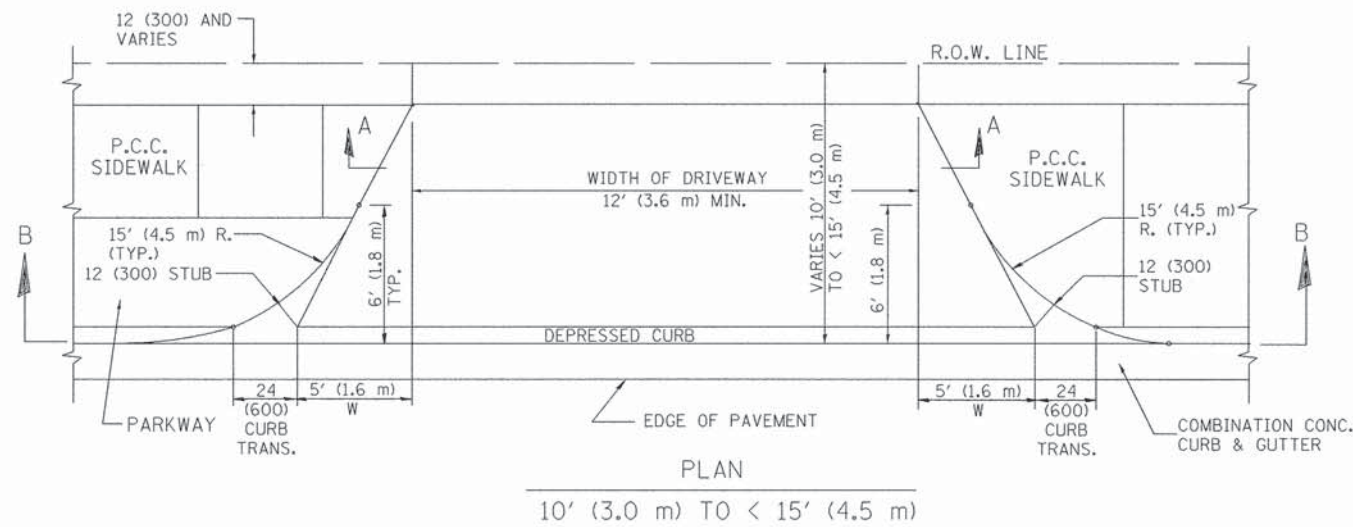
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	PLOT DATE = 6/12/2008	DATE - 11-04-95	REVISED - R. BORO 06-11-08

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
 FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE SHEET NO. 12 OF 23 SHEETS STA. TO STA.

F.A.U. RTE. 429	SECTION 15-00062-00-RS	COUNTY WILL	TOTAL SHEETS 23	SHEET NO. 12
BD-1		CONTRACT NO. 61B63		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (487)				



GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

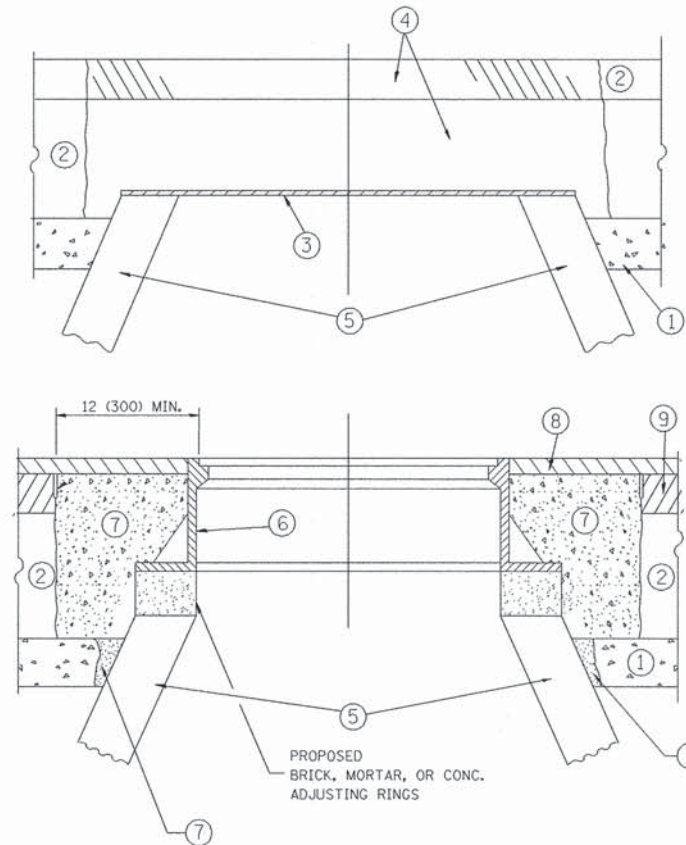
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 13 OF 23 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
429	15-00062-00-RS	WILL	23	13
BD-2			CONTRACT NO. 61B63	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (487)				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

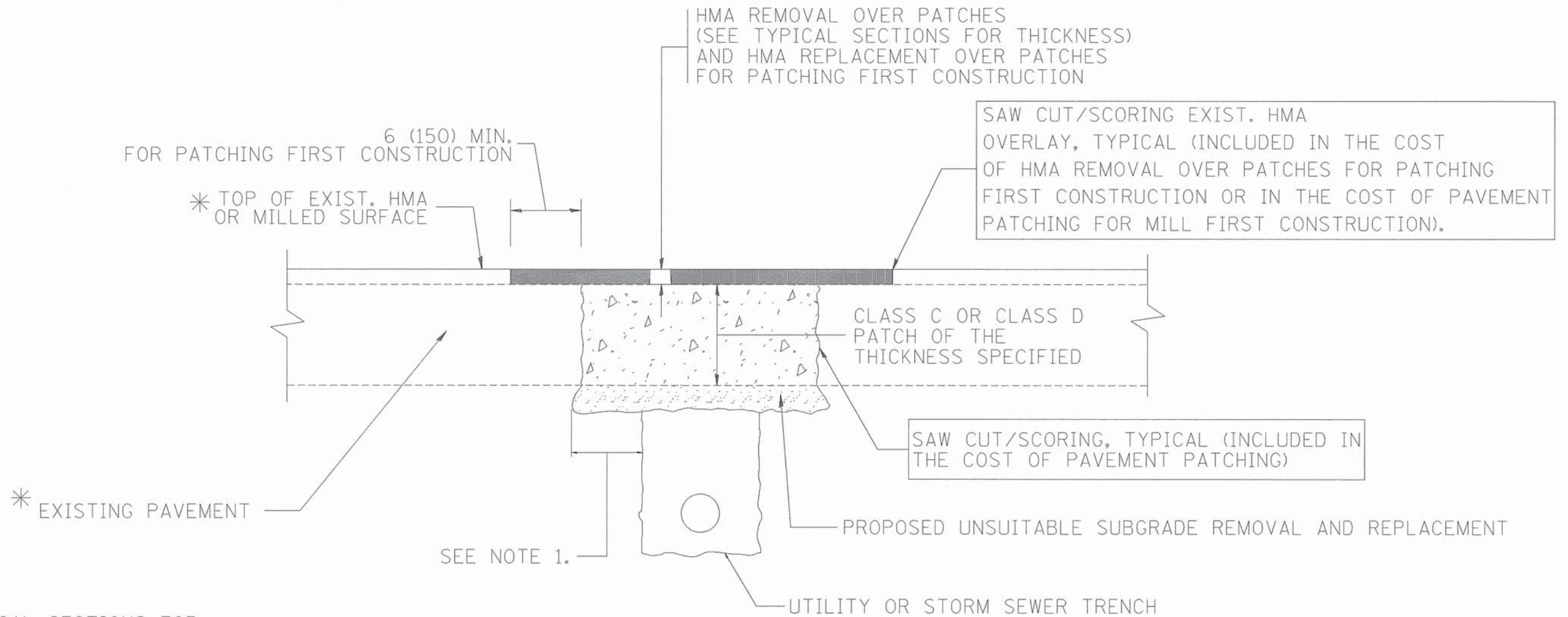
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	PLOT SCALE = 1/648.5000' / m	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE SHEET NO. 14 OF 23 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
429	15-00062-00-RS	WILL	23	14
BD-8			CONTRACT NO. 61B63	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (487)				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = board1	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. BORO 01-01-07		429	15-00062-00-RS	WILL	23	15			
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 61B63				
	PLOT DATE = 10/27/2006	DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 15 OF 23 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003 (487)		

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SPECIAL AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

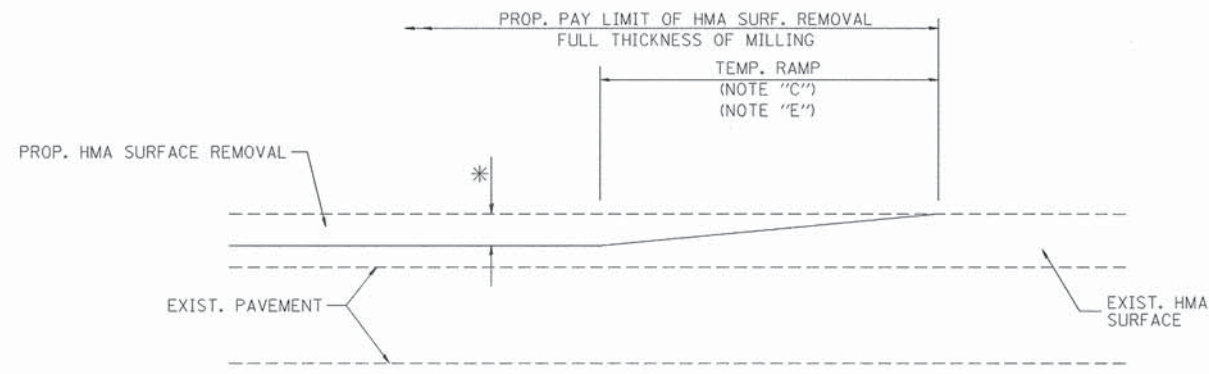
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

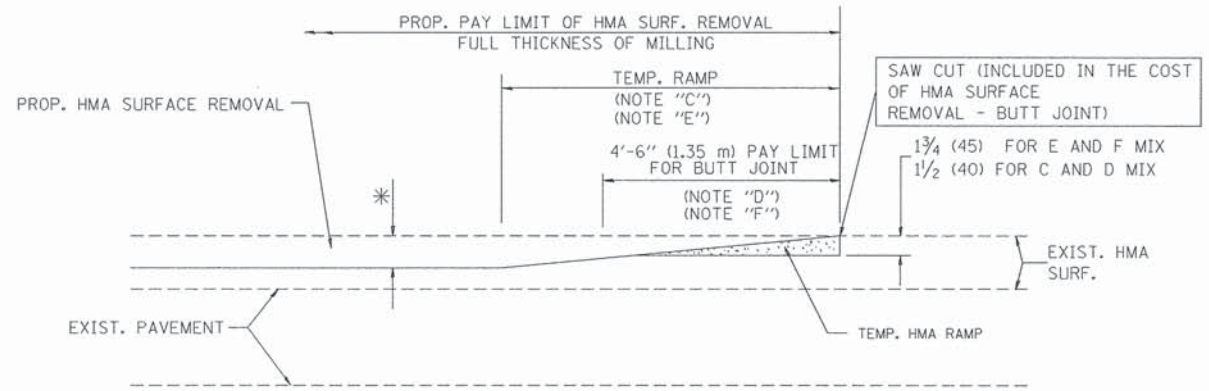
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cc:\pwork\pwork\drivakosgn\d0108315\bd24.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01			429	15-00062-00-RS	WILL	23	16
PLOT SCALE = 58.000 "/ IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01	REVISED - R. BORO 12-15-09			BD600-06 (BD-24)		CONTRACT NO. 61B63		
PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09	SCALE: NONE			SHEET NO. 16	OF 23 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1



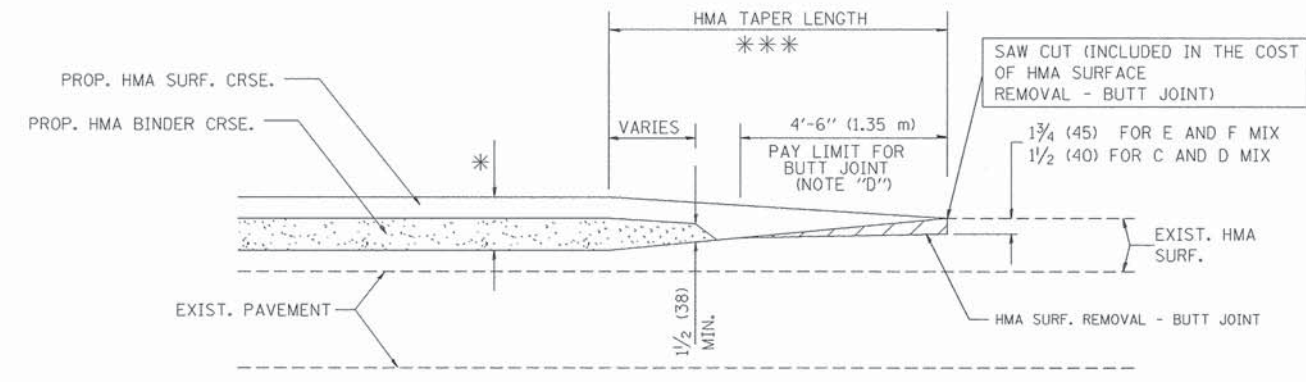
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

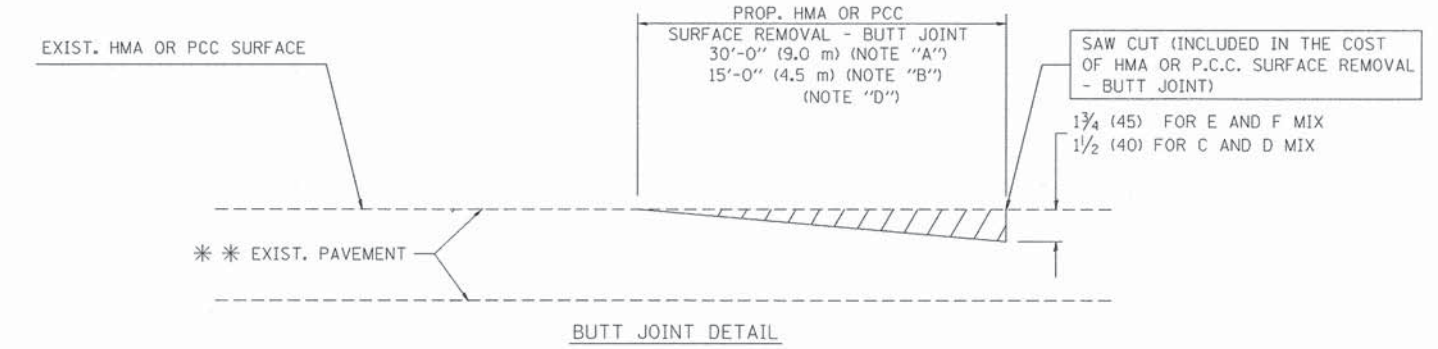


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

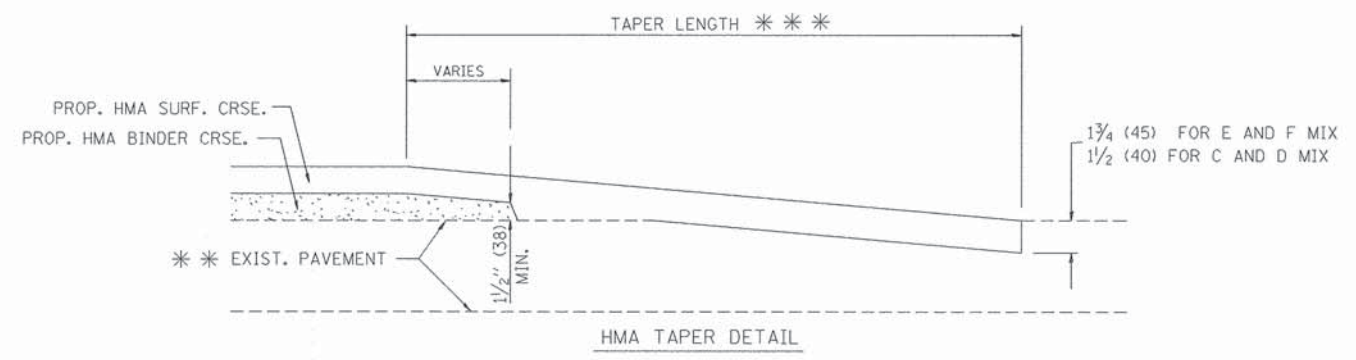
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

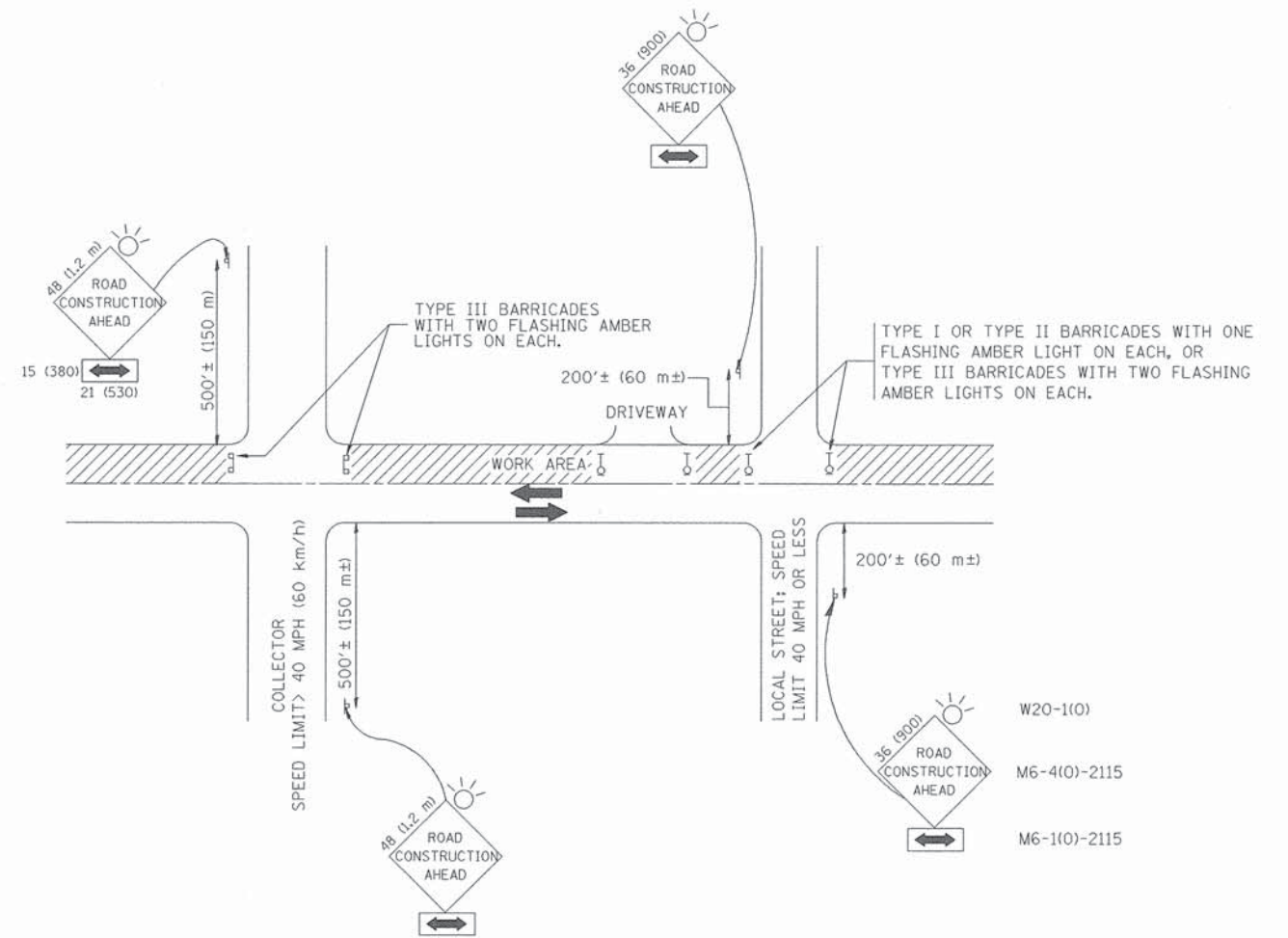
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dststd\22x34\bd32.dgn	USER NAME = goglionobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
BUTT JOINT AND HMA TAPER DETAILS		429	15-00062-00-RS	WILL	23	17
SCALE: NONE		SHEET NO. 17 OF 23 SHEETS		STA.	TO STA.	

BD400-05 BD32		CONTRACT NO. 61B63	
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT M-4003 (487)	



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
 - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
 - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

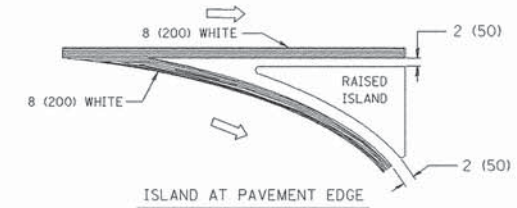
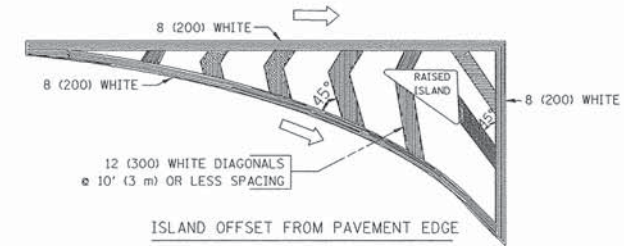
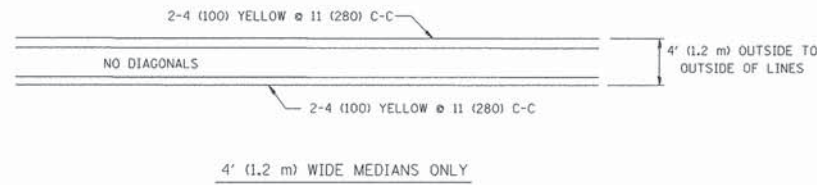
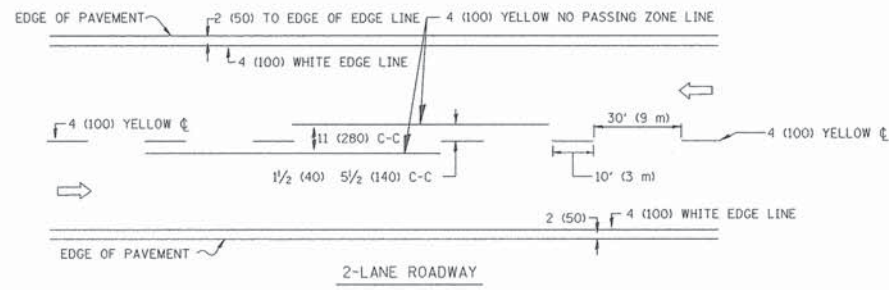
All dimensions are in millimeters (Inches) unless otherwise shown.

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		DRAWN -	REVISED - A. HOUSEH 03-06-96
		PLOT SCALE = 58,0000 / IN.	REVISED - A. HOUSEH 10-15-96
		PLOT DATE = 1/4/2008	REVISED - T. RAMMACHER 01-06-00

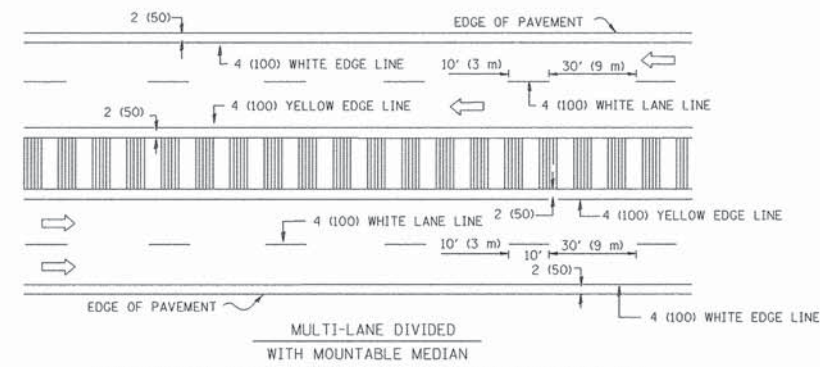
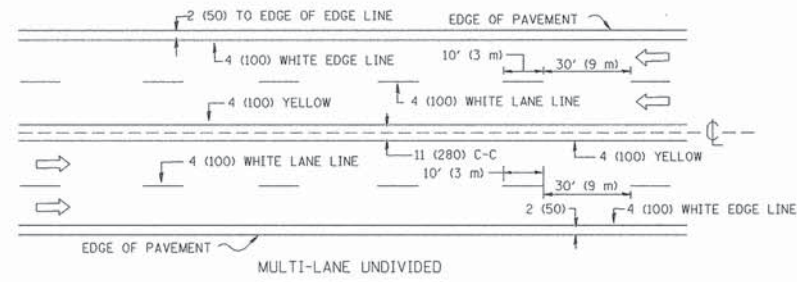
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
TRAFFIC CONTROL AND PROTECTION FOR			
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 18 OF 23 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
429	15-00062-00-RS	WILL	23	18
TC-10			CONTRACT NO. 61B63	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (487)				

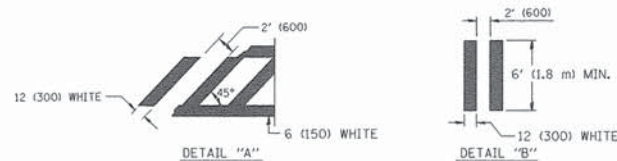
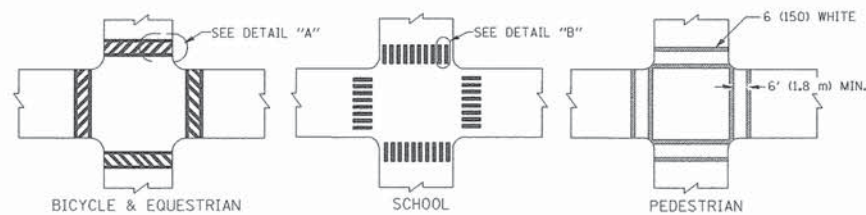


TYPICAL ISLAND MARKING

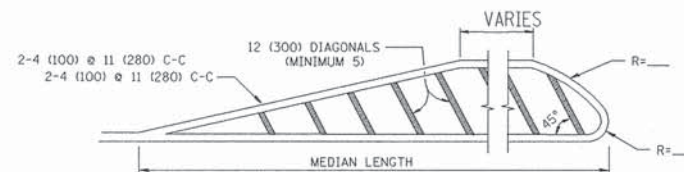


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



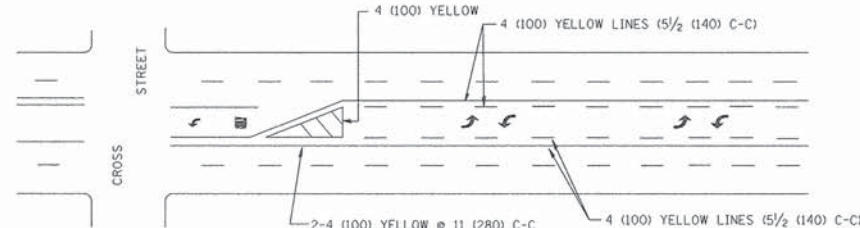
TYPICAL CROSSWALK MARKING



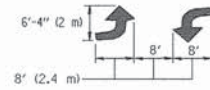
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

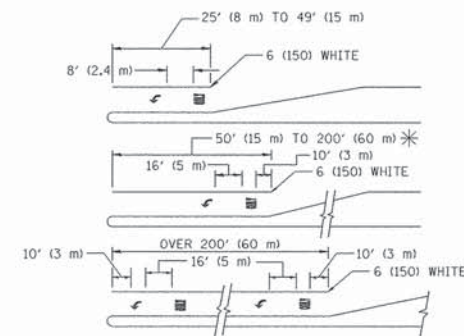


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100)	SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
LANE LINES	5 (125) ON FREEWAYS	SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
TWO WAY LEFT TURN MARKING	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN)	2 @ 6 (150)	SOLID	WHITE	NOT LESS THAN 6' (1.8 m) APART
DIAGONALS (BIKE & EQUESTRIAN)	12 (300) @ 45°	SOLID	WHITE	2' (600) APART
LONGITUDINAL BARS (SCHOOL)	12 (300) @ 90°	SOLID	WHITE	2' (600) APART
STOP LINES	24 (600)	SOLID	WHITE	SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
PAINTED MEDIANS	NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

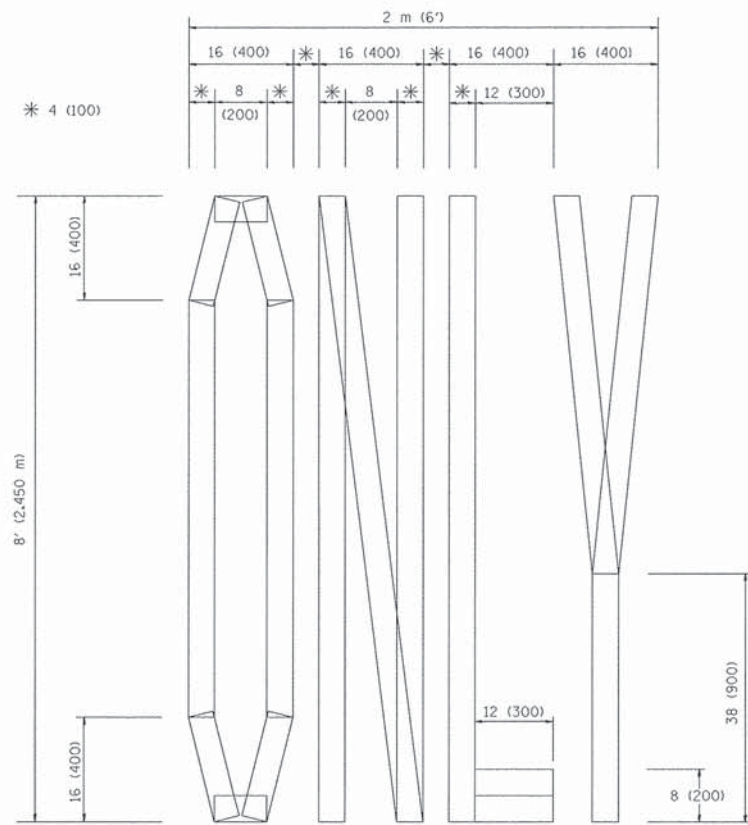
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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

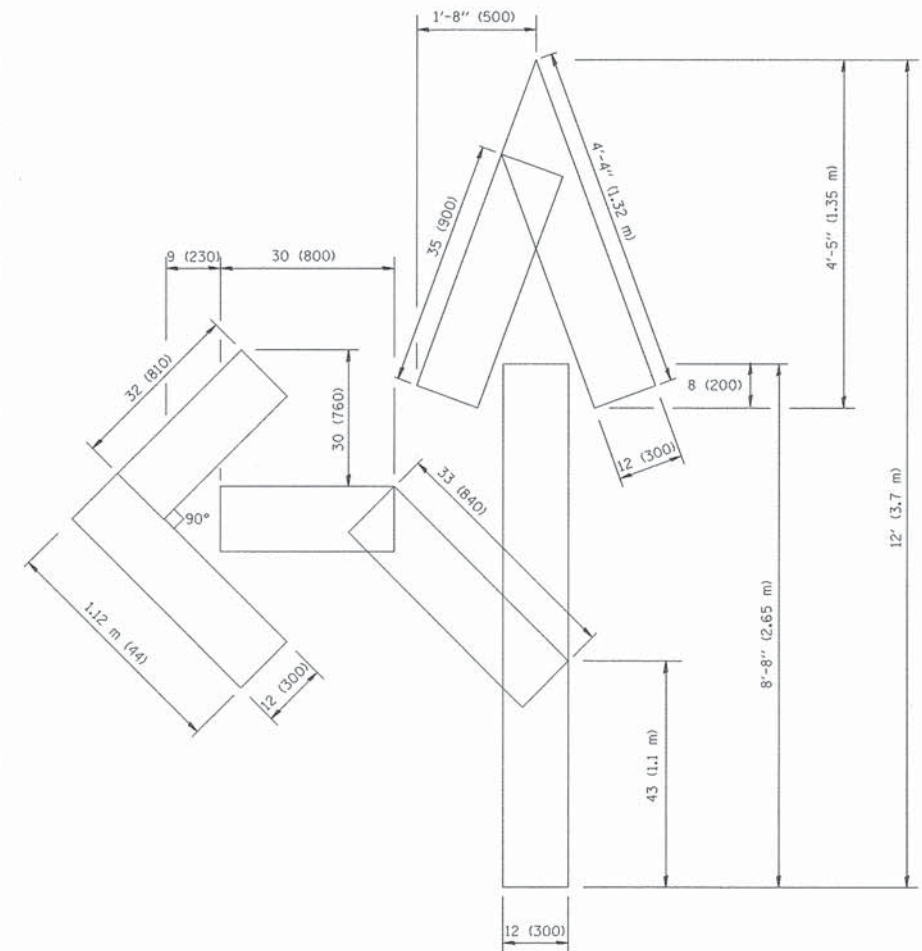
DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 19 OF 23 SHEETS STA. TO STA.

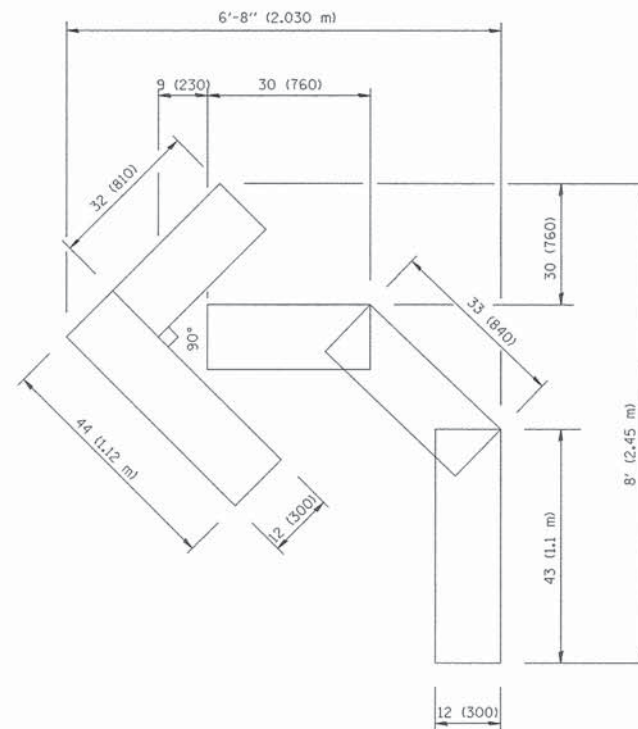
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
429	15-00062-00-RS	WILL	23	19
TC-13		CONTRACT NO. 61B63		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003 (487)		



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

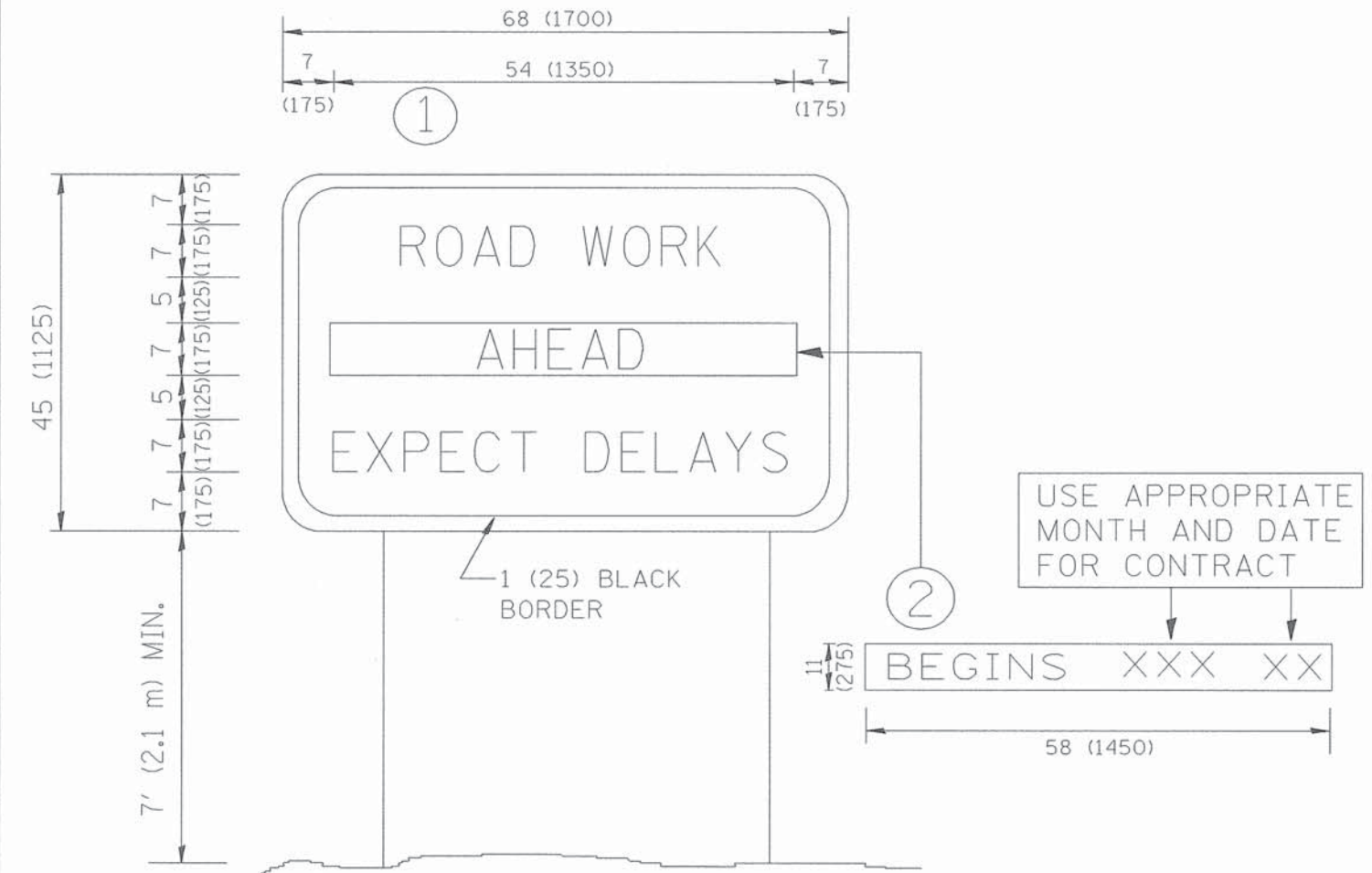
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	PLOT SCALE = 50.0000 "/ IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - PAVEMENT MARKING LETTERS AND
 SYMBOLS FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 20 OF 23 SHEETS STA. TO STA.

F.A.U. RTE. 429	SECTION 15-00062-00-RS	COUNTY WILL	TOTAL SHEETS 23	SHEET NO. 20
TC-16			CONTRACT NO. 61B63	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (487)				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
ci:\pw_work\pwidot\drivakosgn\d0100315\to	3.dgn	DRAWN -	REVISED - C. JUCIUS 09-09-09
PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -	
PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

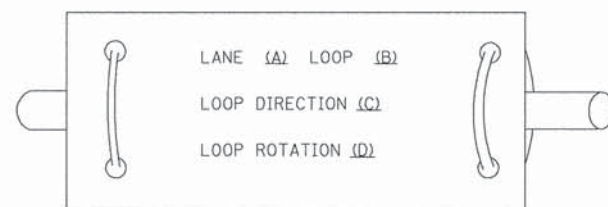
DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 21	OF 23 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
429	15-00062-00-RS	WILL	23	21
TC-22			CONTRACT NO. 61B63	
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003 (487)		

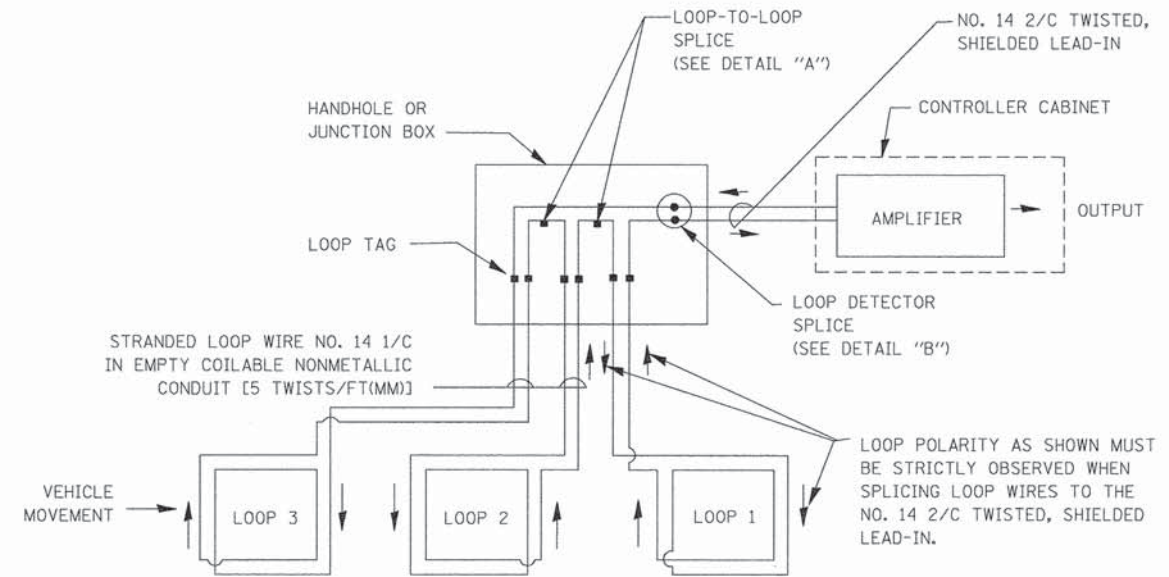
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

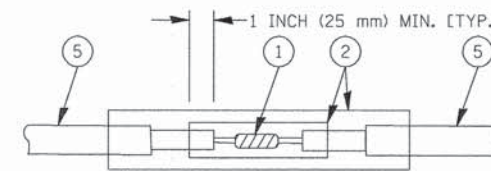


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

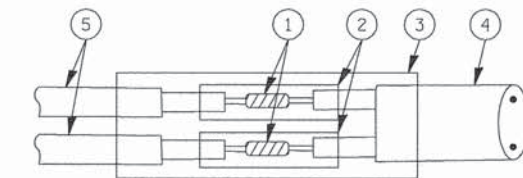


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

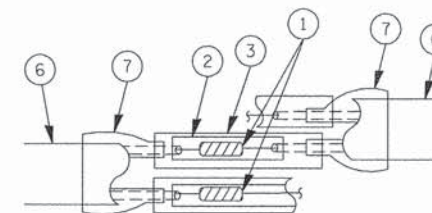


DETAIL "A" LOOP-TO-LOOP SPLICE



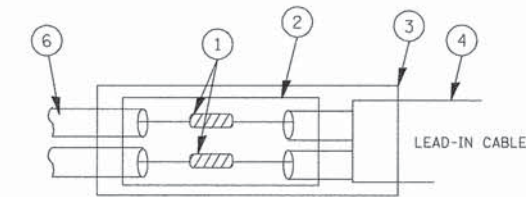
DETAIL "B" LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A" LOOP-TO-LOOP SPLICE

PREFORMED LOOP



DETAIL "B" LOOP-TO-CONTROLLER SPLICE

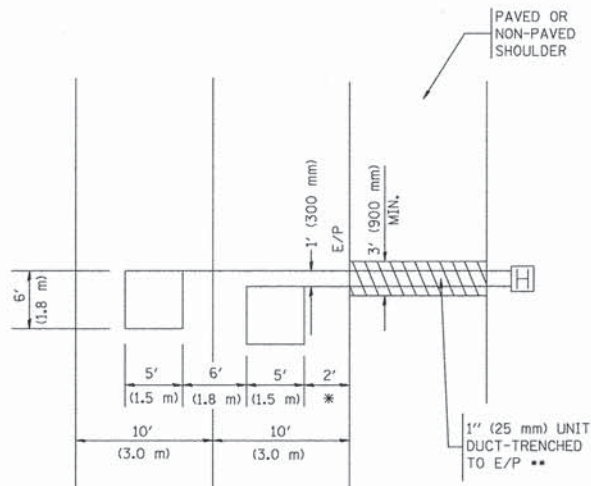
LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PREFORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = bauerd	DESIGNED - DAD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
01\p\work\PW1007\BAUEROL\02108315\ts05.dgn	DRAWN - BCK	REVISD -	429			15-00062-00-RS	WILL	23	22	
PLOT SCALE = 50.0000' / IN.	CHECKED - DAD	REVISD -	TS-05			CONTRACT NO. 61B63				
PLOT DATE = 11/4/2009	DATE - 10-28-09	REVISD -	SCALE: NONE			SHEET NO. 22 OF 23 SHEETS				
				STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (487)				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



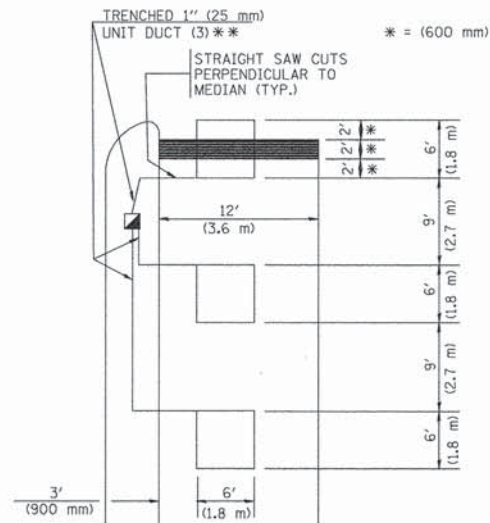
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

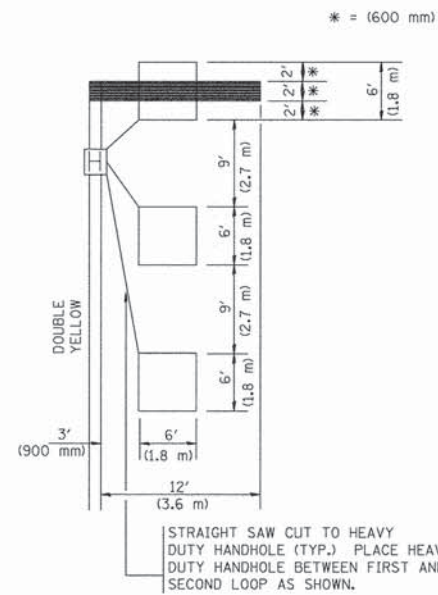


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

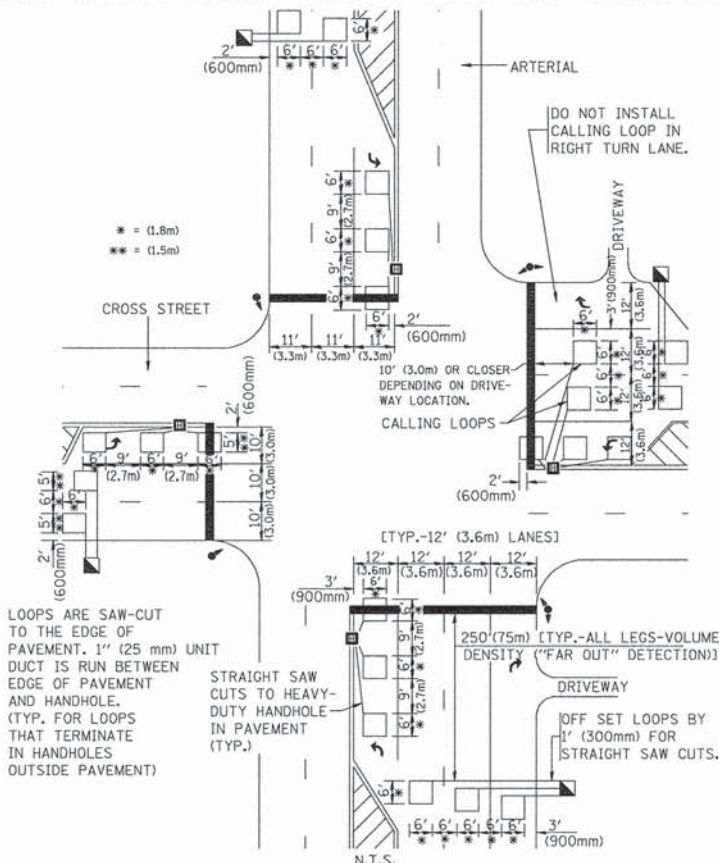
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



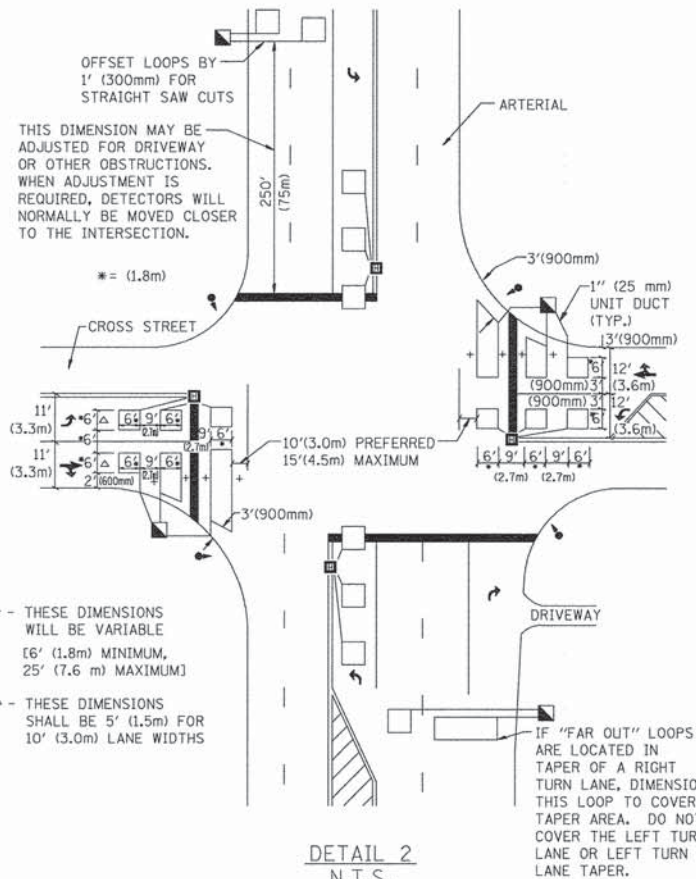
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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USER NAME = gaglianob
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 1/4/2009

DESIGNED -
DRAWN -
CHECKED - R.K.F.
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET NO. 23 OF 23 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
429	15-00062-00-RS	WILL	23	23
TS-07			CONTRACT NO. 61B63	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (487)				