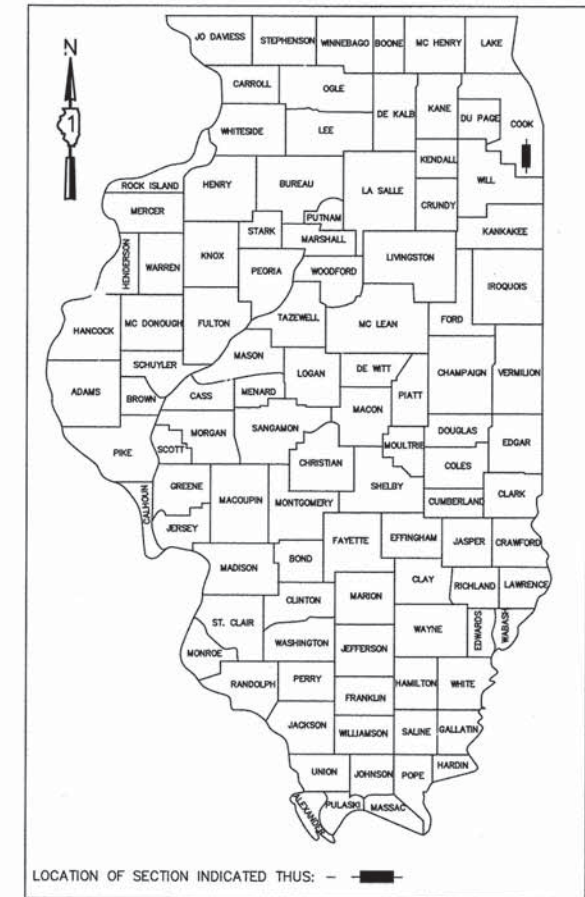


06-12-2015 LETTING ITEM 029

# STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS PLANS FOR PROPOSED FEDERAL AID HIGHWAY

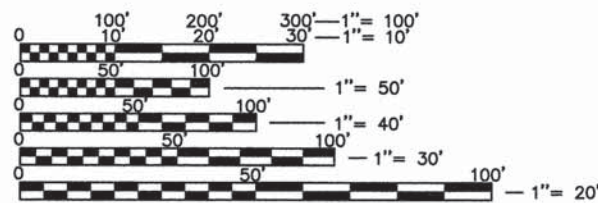
FAU 1713 (CALIFORNIA AVENUE)  
167TH STREET TO SUSSEX COURT  
RESURFACING  
SECTION No. 14-00045-00-RS  
PROJECT No. M-4003(486)  
CITY OF MARKHAM  
COOK COUNTY  
C-91-217-15

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1713	14-00045-00-RS	COOK	14	1
FEDERAL ROAD DISTRICT NO. 1 ILLINOIS		CONTRACT NO. 61B65		



FOR INDEX OF HIGHWAY STANDARDS  
AND INDEX OF SHEETS  
SEE SHEET NO. 2

CALIFORNIA AVENUE URBAN MAJOR COLLECTOR  
ADT= 2,560 (2014)  
POSTED SPEED LIMIT=20 M.P.H.



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

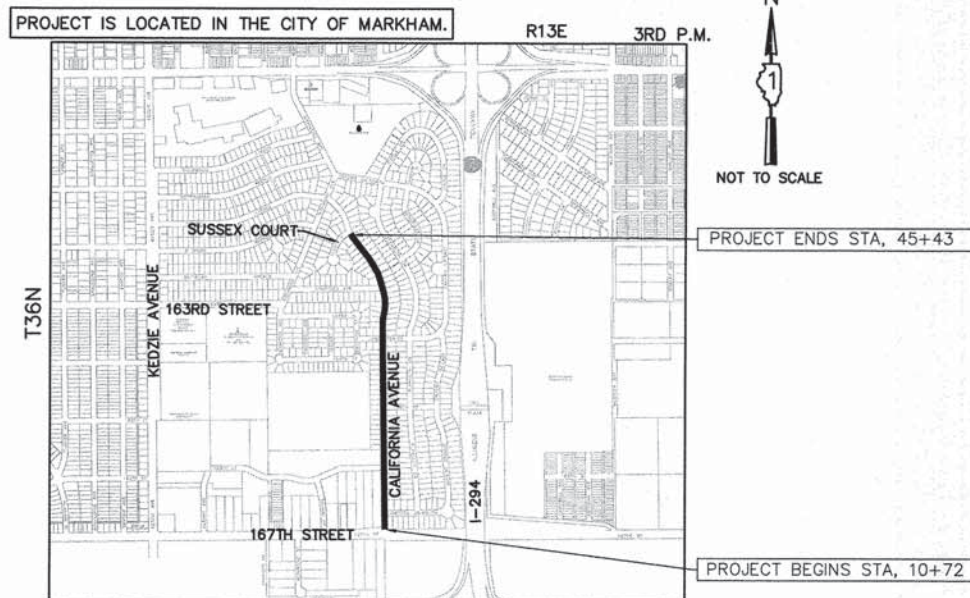
J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123 OR 811

PLANS PREPARED BY:



PROFESSIONAL DESIGN FIRM NO.: 184-005786  
EXPIRATION DATE: 04/30/2015

CONTRACT NO. 61B65



LOCATION MAP  
N.T.S.

BREMEN TOWNSHIP  
GROSS LENGTH OF PROJECT = 3,471 FEET (0.66 MI)  
NET LENGTH OF PROJECT = 3,471 FEET (0.66 MI)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED [Signature] 3-20-2015  
MAYOR, CITY OF MARKHAM

PASSED [Signature] APRIL 8 2015  
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR  
BID BASED ON  
LIMITED REVIEW [Signature] APRIL 8 2015  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER



[Signature] 3/13/2015  
ENGINEER DATE  
WILLIAM J ZALEWSKI  
ILLINOIS REGISTRATION NO. 062046121  
EXPIRATION DATE: 11/30/15

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

PROGRAM AND OFFICE ENGINEER: FAWAD AQUEEL, P.E., PTOE 847-705-4120, SCHAMBURG, IL  
CONSULTANTS: VANTAGEPOINT ENGINEERING, LLC (708) 478-4004

SPECIFICATIONS AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2012; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2015; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD); THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", (SSTCI), "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY 1996 FIFTH EDITION, THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE LAWS AND GOVERNMENT AGENCY REGULATIONS AND RULES; AUTHORITIES HAVING JURISDICTION; OSHA REGULATIONS AND RULES; AND ANY APPLICABLE RULES AND REGULATIONS OF THE STATE OF ILLINOIS OR COOK COUNTY AGENCIES. FURTHERMORE, AND AS RELATED TO THE WORK, THE CONTRACTOR SHALL GIVE NOTICES AND COMPLY WITH APPLICABLE LAWS, ORDINANCES, RULES, REGULATIONS, AND LAWFUL ORDERS OF ALL PUBLIC AUTHORITIES BEARING ON THE SAFETY OF PERSONS OR PROPERTY OR THEIR PROTECTION FROM DAMAGE, INJURY OR LOSS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT STANDARD.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

UTILITIES

ALL TRAFFIC CONTROL SHALL COOPERATE WITH THE VILLAGE IN ANY UNDERGROUND UTILITY CONSTRUCTION WHICH THE VILLAGE MAY WANT TO PLACE DURING THE CONTRACTOR'S OPERATIONS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNER OF ALL EXISTING FACILITIES THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED. IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATION.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR AT HIS OWN EXPENSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING THE CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

COORDINATION OF ALL UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GASS AND CABLE TELEVISION FACILITIES (48 HOUR NOTIFICATION IS REQUIRED).

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE ENGINEER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED. AND SHALL BE AS INDICATED ON THE PLANS, ELEVATIONS SHOWN AT POINT OF CURVE, ETC. IS EDGE OF PAVEMENT OTHERWISE NOTED.

ALL OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS FOR STRUCTURES, ETC. ARE FROM THE PROPOSED BASE LINE OF CONSTRUCTION.

PRIOR TO CONSTRUCTION, THE ENGINEER SHALL MARK THE LIMITS OF PAVING OPERATIONS, INCLUDING THE REMOVAL AND RESURFACING LIMITS, START AND END OF THE PROJECT AND ALL SIDE STREET RADII, ETC.

MISCELLANEOUS

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSION AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE INCLUDED IN THE UNIT COST OF REMOVAL ITEMS AND SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.  
PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT OF SURFACE COURSE, UNLESS OTHERWISE INDICATED.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS". THIS WORK SHALL BE INCLUDED IN THE COST FOR PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH, SPECIAL.

ANY EXISTING PAVEMENT DAMAGED BY THE CONTRACTOR DURING THE CONSTRUCTION SHALL BE REPLACED/REPAIRED BY THE CONTRACTOR AT HIS/HERS OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER WITH NO ADDITIONAL COMPENSATION TO THE CONTRACTOR.

ALL TRANSITIONS IN CURB HEIGHT SHALL OCCUR OVER 3 FEET ALONG CURB LINE. AT LOCATIONS WHERE CURB TERMINATES, THE LAST 1 FOOT SHALL BE DEPRESSED. DEPRESSED CURB SHALL ALSO BE AT LOCATIONS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER.

GENERAL NOTES

THE OWNER, THE VILLAGE OF MARKHAM SHALL BE NOTIFIED IN WRITING AT LEAST (3) FULL WORKING DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION.

THE CONTRACTOR SHALL PROVIDE ALL NECESSARY PROTECTION FOR EXISTING UTILITIES IN CONFORMANCE WITH THE AFFECTED UTILITY COMPANIES REQUIREMENTS AS MAY BE REQUIRED TO PERFORM THE WORK OF THIS CONTRACT.

BEFORE BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THE LINE AND GRADES SHOWN ON THE CONTRACT DRAWINGS. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONTRACT DRAWINGS, THE CONTRACTOR SHALL IMMEDIATELY REPORT SAME TO THE OWNER PRIOR TO PERFORMING WORK. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF WORK AS REQUIRED.

THE WORK PERFORMED UNDER THIS CONTRACT SHALL IN NO WAY INTERFERE WITH THE NORMAL OPERATION OF ANY EXISTING UTILITY SERVICE. THE CONTRACTOR SHALL FURNISH ALL NECESSARY ITEMS OF EQUIPMENT REQUIRED TO MAINTAIN SUCH NORMAL OPERATION AT NO ADDITIONAL COST TO THE OWNER. THE COST ASSOCIATED FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

SOIL EROSION PROTECTION SHALL BE IN ACCORDANCE WITH IEPA STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL. ALL DISTURBED AREAS (NOT IMPERVIOUS IN NATURE) SHALL BE FINE GRADED, TOP SOIL RESTORED (MIN 4 INCHES) AND SEED/MULCH APPLIED UNLESS OTHERWISE SPECIFIED ON THE PLANS.

DETECTABLE WARNINGS, SIDEWALK, COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT SHALL BE AS DIRECTED BY ENGINEER.

THE REMOVAL AND/OR REPLACEMENT OF ANY DRIVEWAYS, PAVEMENT, CURB, SIDEWALK, ETC. SHALL BE ACCOMPLISHED BY MEANS OF A SAW CUT JOINT, AT THE DIRECTION OF THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE VARIOUS ITEMS OF ADJACENT WORK.

PRIOR TO SUBMITTING A BID, THE PROSPECTIVE BIDDER SHALL THOROUGHLY EXAMINE THE PROVISIONS OF THE CONTRACT, EXAMINE THE SITE OF THE PROPOSED WORK, AND ACQUAINT THEMSELVES WITH ALL THE LOCAL CONDITIONS AFFECTING THE CONTRACT. THE PROSPECTIVE BIDDER SHALL ALSO BE FAMILIAR WITH THE REQUIREMENTS OF THE CONSTRUCTION.

STORM SEWER

WHENEVER, DURING CONSTRUCTION, OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST FOR CURB AND GUTTER.

ANY EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST.

FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES OF ALL NEW, ADJUSTED OR RECONSTRUCTED STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE STRUCTURE, ADJUSTMENT, OR RECONSTRUCTION COST. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE STRUCTURE SIZE.

WHEN EXISTING DRAINAGE OR SEWERAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PUBLIC OR PRIVATE DRAINS, SEWERS, OR CATCH BASINS, HE SHALL PROVIDE FACILITIES TO TAKE ALL STORM WATER WHICH WOULD BE RECEIVED BY THESE FACILITIES AND DISCHARGE SAME. HE SHALL ALSO PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET, AND BE PREPARED AT ALL TIMES TO DISPOSED OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL SUCH TIME THAT PERMANENT CONNECTIONS WITH SEWERS ARE CONSTRUCTED AND IN SERVICE, THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF COMBINATION CURB AND GUTTER, TYPE M6.12.

DRAINAGE STRUCTURES CONSTRUCTED OVER THE EXISTING STORM SEWER SHALL INCLUDE THE COST TO REMOVE THE NECESSARY PORTION OF THE STORM SEWER.

ALL REMOVAL OR EXCAVATION ITEMS BEING DISPOSED OF AT AN UNCONTAMINATED SOIL FILL OPERATION OR CLEAN CONSTRUCTION AND DEMOLITION DEBRIS (CCDD) FILL SITE SHALL MEET THE REQUIREMENTS OF PUBLIC ACT 96-1416. ALL COSTS ASSOCIATED WITH MEETING THESE REQUIREMENTS SHALL BE INCLUDED IN THE UNIT PRICE COST FOR THE ASSOCIATED REMOVAL OR EXCAVATION ITEMS IN THE CONTRACT. THESE COSTS SHALL INCLUDE BUT ARE NOT LIMITED TO ALL REQUIRED TESTING, LAB ANALYSIS, CERTIFICATION BY A LICENSED PROFESSIONAL ENGINEER, AND STATE AND LOCAL TIPPING FEES.

IDOT STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
606001-06	CONCRETE CURB TYPE B AND COMBINATION CURB AND GUTTER
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES

INDEX OF SHEETS

1	COVER SHEET
2	GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	TYPICAL SECTIONS
5-8	RESURFACING PLAN
9-14	DETAILS

FILE NAME =	USER NAME =	DESIGNED -- TPP	REVISED --	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CALIFORNIA AVENUE FROM 167TH ST. TO SUSSEX COURT</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -- JSS	REVISED --		<b>STP/LAFO</b>			1713	14-00045-00-RS	COOK	14	2
		CHECKED -- WJZ	REVISED --		<b>GENERAL NOTES</b>			<b>CONTRACT NO. 61B65</b>				
		DATE -- 01/26/15	REVISED --		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003-(486)</small>		

SUMMARY OF QUANTITIES					PAVEMENT	
S.I.	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE	
					0005	
	20101400	NITROGEN FERTILIZER NUTRIENT	POUND	15	15	
	25200100	SODDING	SQ YD	286	286	
	40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	6,180	6,180	
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	4	4	
	40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	503	503	
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	430	430	
	40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1,630	1,630	
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	938	938	
	42400800	DETECTABLE WARNINGS	SQ FT	120	120	
	<del>4400400</del>	PAVEMENT REMOVAL, SPECIAL	SQ YD	771	771	
	44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	11,645	11,645	
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	22	22	
	44000600	SIDEWALK REMOVAL	SQ FT	1,131	1,131	
	44201692	CLASS D PATCHES, TYPE II, 4 INCH	SQ YD	17	17	
	44201694	CLASS D PATCHES, TYPE III, 4 INCH	SQ YD	47	47	
	44201696	CLASS D PATCHES, TYPE IV, 4 INCH	SQ YD	163	163	
	60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	14	14	
	67100100	MOBILIZATION	L SUM	1	1	
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
	Z0004510	HOT MIX ASPHALT DRIVEWAY, 3"	SQ YD	22	22	
	Z0004562	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT, TYPE M-6.12	FOOT	1,040	1,040	

\* - INDICATES SPECIALTY ITEMS

FILE NAME =	USER NAME =	DESIGNED - TPP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	CALIFORNIA AVENUE FROM 167TH ST. TO SUSSEX COURT STP/LAFO			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JSS	REVISED -		SUMMARY OF QUANTITIES			1713	14-00045-00-RS	COOK	14	3
		CHECKED - WJZ	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 61B65		
		DATE - 01/26/15	REVISED -		FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT M-4003-(486)							

- ① HOT MIX ASPHALT SURFACE REMOVAL, 2.5", S.Y.
- ② EXISTING HOT-MIX ASPHALT PAVEMENT TO REMAIN, 2" MINIMUM AFTER MILLING
- ③ EXISTING AGGREGATE BASE TO REMAIN
- ④ CLASS D PATCHES, TYPE I, TYPE II, TYPE III OR TYPE IV, 4" (LOCATIONS AS DIRECTED BY RE), S.Y.
- ⑤ EXISTING CURB & GUTTER TO BE REMOVED, FT. (LOCATIONS AS DIRECTED BY R.E.)
- ⑥ EXISTING DRIVEWAY PAVEMENT TO BE REMOVED, S.Y.
- ⑦ HOT MIX ASPHALT SURFACE COURSE, MIX D, N50 - 2 1/2", TON
- ⑧ COMBINATION CONCRETE CURB & GUTTER, M-6.12, FT. (LOCATIONS AS DIRECTED BY R.E.)
- ⑨ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 0" - 3/4", TON
- ⑩ HOT MIX ASPHALT SURFACE COURSE, MIX D, N50, 3"
- ⑪ PAVEMENT REMOVAL, SPECIAL (LOCATION AS EXISTING)

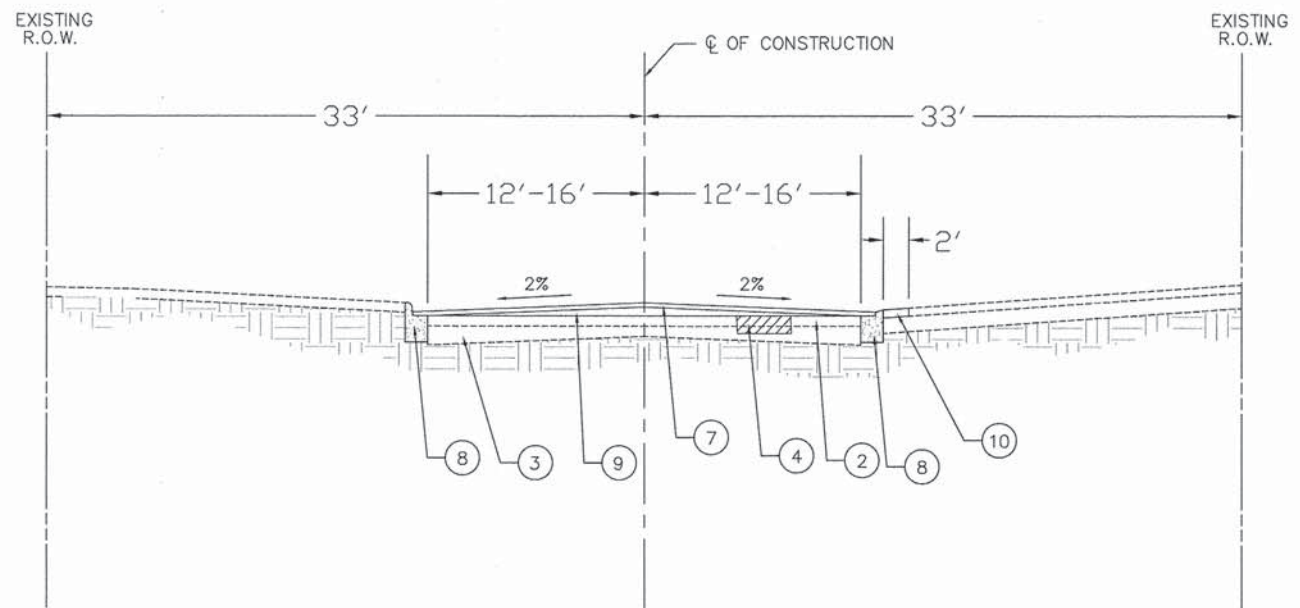
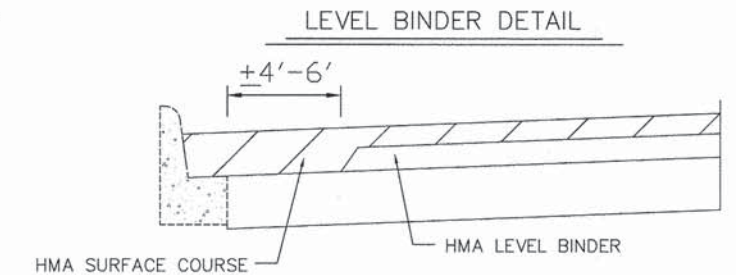
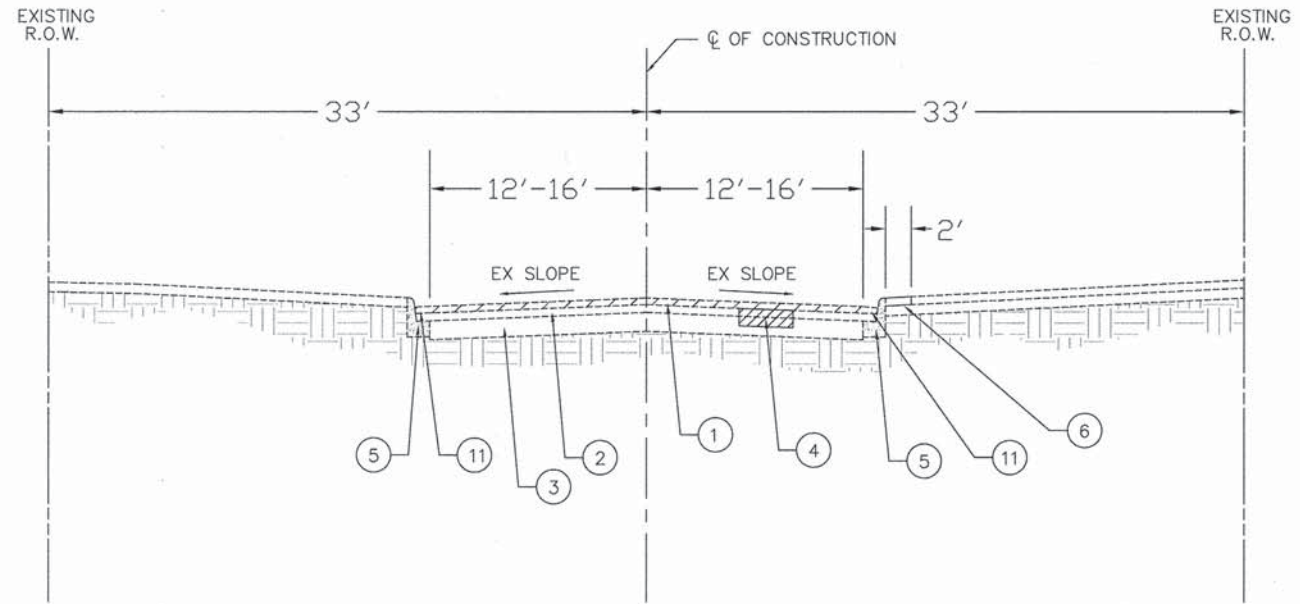
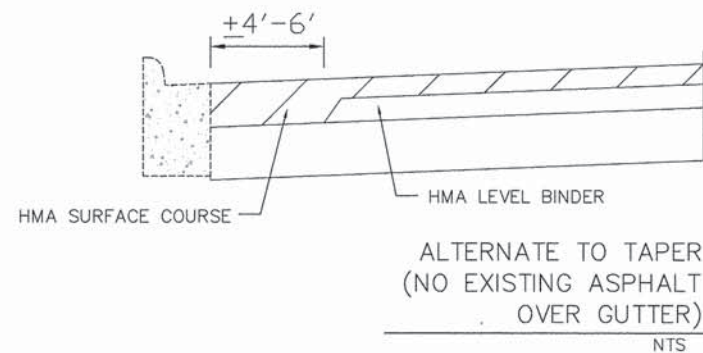
NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING.

MIXTURE TYPE	AIR VOIDS @ N <sub>DES</sub>
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5mm) 2 1/2"	4% @ 50 Gyr.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	3.5% @ 50 Gyr.
DRIVEWAYS	
HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5mm) 3"	4% @ 50 Gyr.
PATCHING	
CLASS D PATCHES, TYPE I, II, III OR IV, 4" (HMA BINDER IL-19.0mm)	4% @ 70 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.



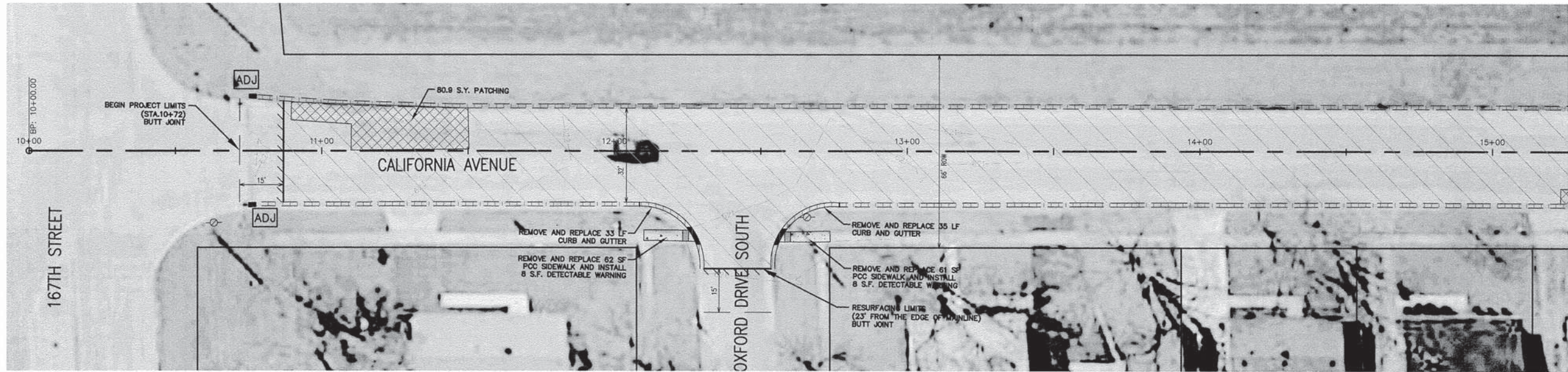
NOTE:  
THE EXISTING PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE REMOVED BEHIND THE CURB AND REPLACED WITH PORTLAND CEMENT CONCRETE SIDEWALK. THE LIMITS SHALL BE AS SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER.

FILE NAME =	USER NAME =	DESIGNED - TPP	REVISED -
		DRAWN - JSS	REVISED -
	PLOT SCALE = 1" = 20'	CHECKED - WJZ	REVISED -
	PLOT DATE =	DATE - 01/26/15	REVISED -

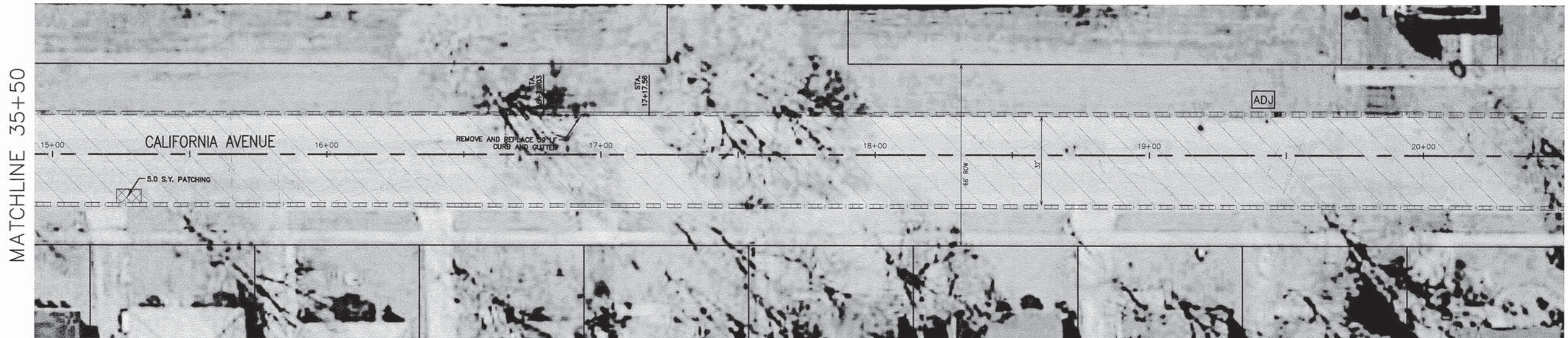
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

CALIFORNIA AVENUE FROM 167TH ST. TO SUSSEX COURT STP/LAFO TYPICAL CROSS SECTIONS			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1713	14-00045-00-RS	COOK	14	4
CONTRACT NO. 61B65				
FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT M-4003-(486)		



MATCHLINE 15+00

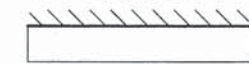


MATCHLINE 35+50

MATCHLINE 20+50



**LEGEND**



BUTT JOINT



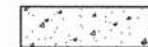
DETECTABLE WARNING



COMBINATION CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT



DEPRESSED CURB



SIDEWALK REMOVAL, PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH.



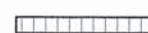
HMA SURFACE REMOVAL, 2 1/2"



HMA DRIVEWAY REMOVAL & REPLACEMENT



ROADWAY PATCHING



PAVEMENT REMOVAL, SPECIAL ADJUSTMENT

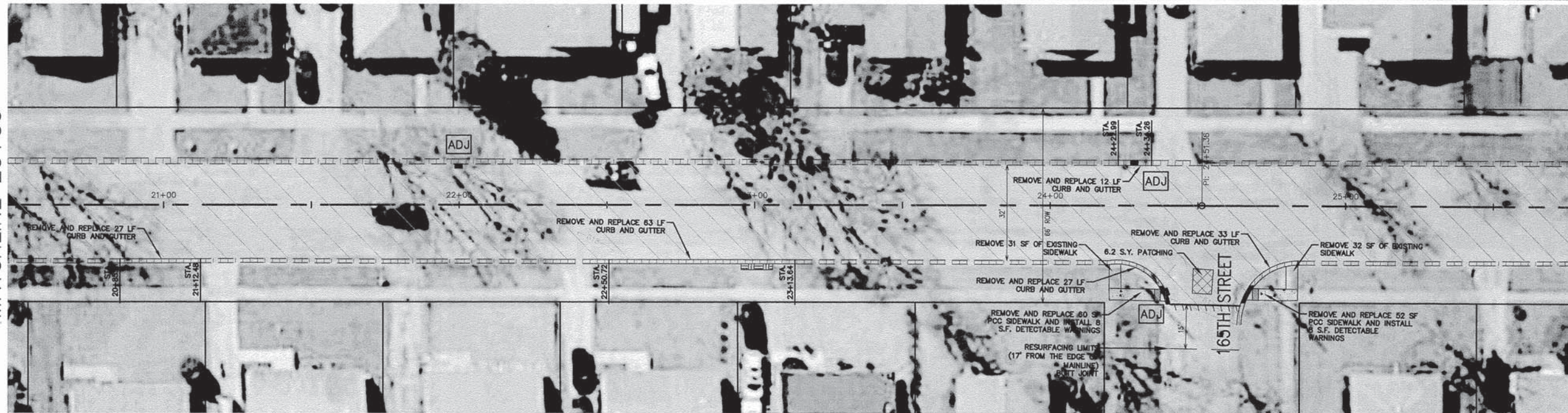


ADJ

NOTE: REMOVAL & REPLACEMENT LIMITS SHALL BE MARKED OUT IN THE FIELD BY THE ENGINEER. THE TYPE, SIZE, AND LOCATION OF THESE ITEMS WILL BE DETERMINED IN THE FIELD DURING CONSTRUCTION.

FILE NAME =	USER NAME =	DESIGNED - TPP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	CALIFORNIA AVENUE FROM 167TH ST. TO SUSSEX COURT STP/LAFO RESURFACING PLAN				F.A.U. RTE. 1713	SECTION 14-00045-00-RS	COUNTY COOK	TOTAL SHEETS 14	SHEET NO. 5
	PLOT SCALE = 1" = 20'	DRAWN - JSS	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 61B65			
	PLOT DATE =	CHECKED - WJZ	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003-(486)								
		DATE - 01/26/15	REVISED -										

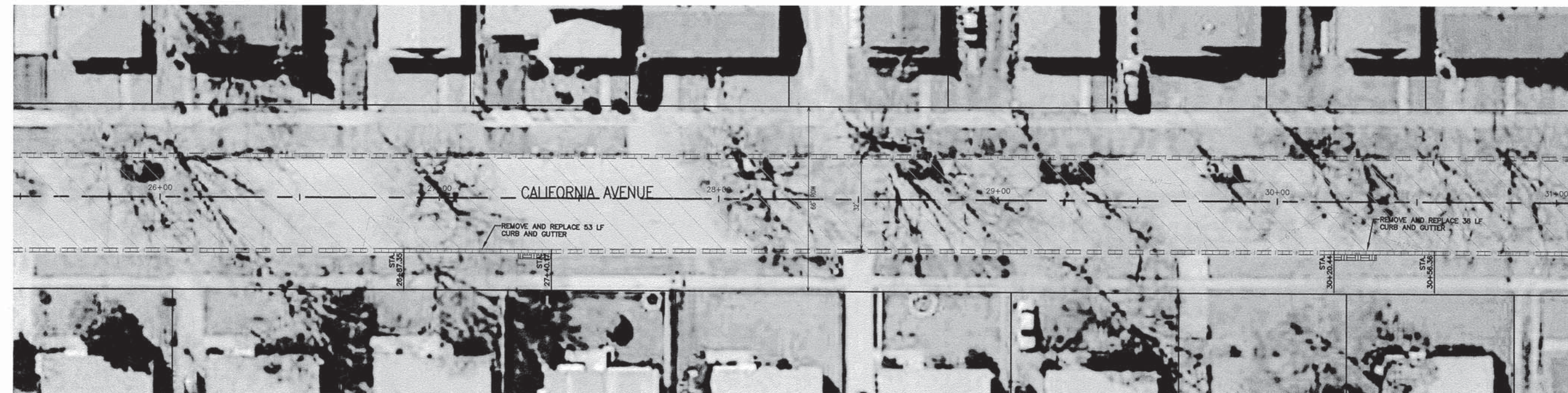
MATCHLINE 20+50



MATCHLINE 25+50



MATCHLINE 25+50



MATCHLINE 31+00



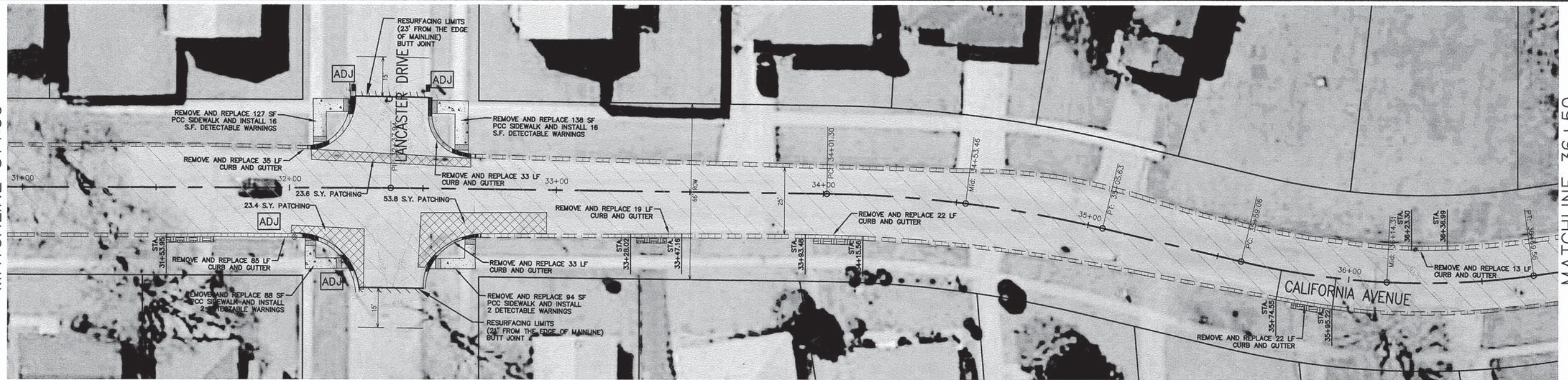
**LEGEND**

- BUTT JOINT
- DETECTABLE WARNING
- COMBINATION CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT
- DEPRESSED CURB
- SIDEWALK REMOVAL, PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH.
- HMA SURFACE REMOVAL, 2 1/2"
- HMA DRIVEWAY REMOVAL & REPLACEMENT
- ROADWAY PATCHING
- PAVEMENT REMOVAL, SPECIAL ADJUSTMENT

NOTE: REMOVAL & REPLACEMENT LIMITS SHALL BE MARKED OUT IN THE FIELD BY THE ENGINEER. THE TYPE, SIZE, AND LOCATION OF THESE ITEMS WILL BE DETERMINED IN THE FIELD DURING CONSTRUCTION.

FILE NAME =	USER NAME =	DESIGNED - TPP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	CALIFORNIA AVENUE FROM 167TH ST. TO SUSSEX COURT STP/LAFO RESURFACING PLAN				F.A.U. RTE. 1713	SECTION 14-00045-00-RS	COUNTY COOK	TOTAL SHEETS 14	SHEET NO. 6
	PLOT SCALE = 1" = 20'	DRAWN - JSS	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 61B65			
	PLOT DATE =	CHECKED - WJZ	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003-(486)								
		DATE - 01/26/15	REVISED -										

MATCHLINE 31+00



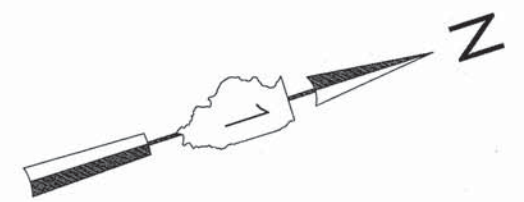
MATCHLINE 36+50



MATCHLINE 36+50



MATCHLINE 41+50



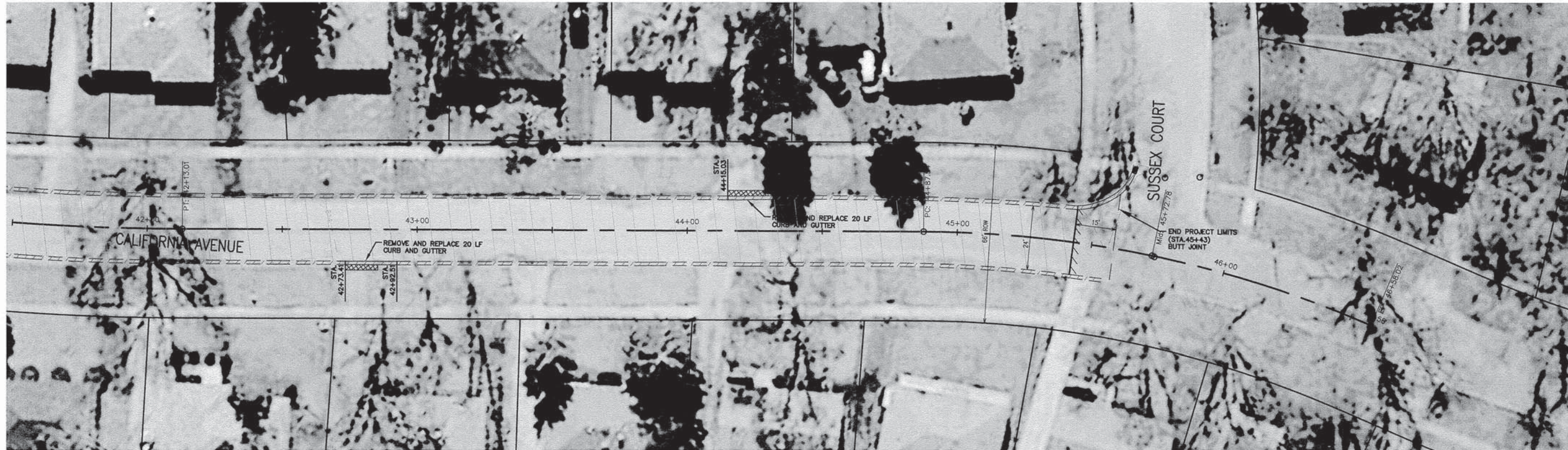
**LEGEND**

- BUTT JOINT
- DETECTABLE WARNING
- COMBINATION CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT
- DEPRESSED CURB
- SIDEWALK REMOVAL, PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH.
- HMA SURFACE REMOVAL, 2 1/2"
- HMA DRIVEWAY REMOVAL & REPLACEMENT
- ROADWAY PATCHING
- PAVEMENT REMOVAL, SPECIAL ADJUSTMENT
- ADJ

NOTE: REMOVAL & REPLACEMENT LIMITS SHALL BE MARKED OUT IN THE FIELD BY THE ENGINEER. THE TYPE, SIZE, AND LOCATION OF THESE ITEMS WILL BE DETERMINED IN THE FIELD DURING CONSTRUCTION.

FILE NAME =	USER NAME =	DESIGNED -- TPP	REVISED --	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	CALIFORNIA AVENUE FROM 167TH ST. TO SUSSEX COURT ST/PLAFO RESURFACING PLAN				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 1" = 20'	DRAWN -- JSS	REVISED --		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	1713	14-00045-00-RS	COOK	14	7
	PLOT DATE =	CHECKED -- WJZ	REVISED --		CONTRACT NO. 61B65									
		DATE -- 01/26/15	REVISED --		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003-(486)									

MATCHLINE 41+50



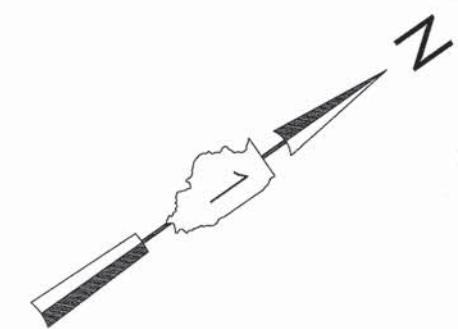
**LEGEND**

- BUTT JOINT
- DETECTABLE WARNING
- COMBINATION CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT
- DEPRESSED CURB

- SIDEWALK REMOVAL, PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH.
- HMA SURFACE REMOVAL, 2 1/2"
- HMA DRIVEWAY REMOVAL & REPLACEMENT
- ROADWAY PATCHING
- PAVEMENT REMOVAL, SPECIAL ADJUSTMENT

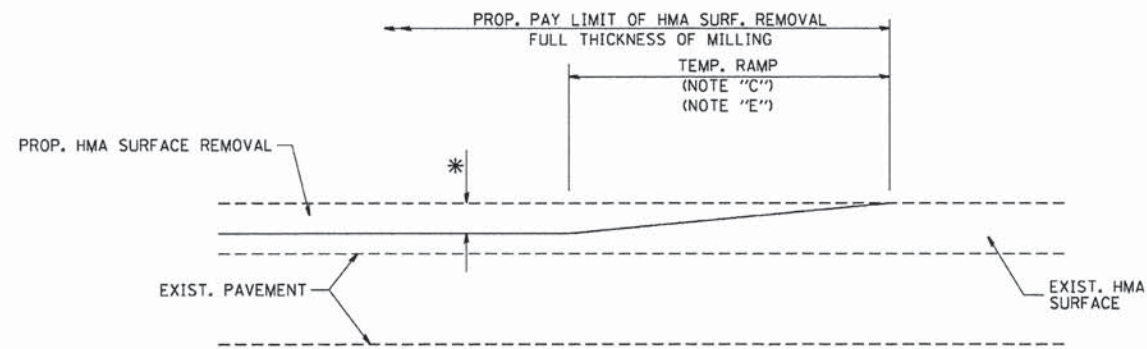
ADJ

NOTE: REMOVAL & REPLACEMENT LIMITS SHALL BE MARKED OUT IN THE FIELD BY THE ENGINEER. THE TYPE, SIZE, AND LOCATION OF THESE ITEMS WILL BE DETERMINED IN THE FIELD DURING CONSTRUCTION.



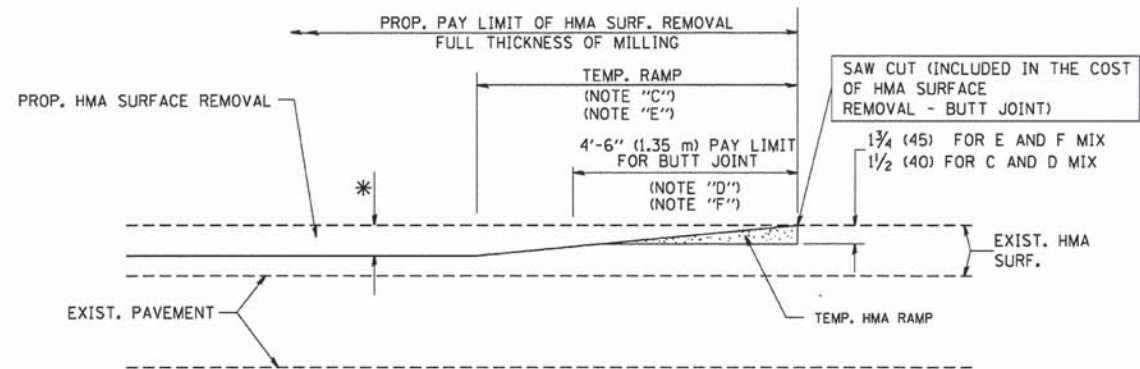
FILE NAME =	USER NAME =	DESIGNED - TPP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CALIFORNIA AVENUE FROM 167TH ST. TO SUSSEX COURT ST/PLAFO RESURFACING PLAN</b>				F.A.U. RTE. 1713	SECTION 14-00045-00-RS	COUNTY COOK	TOTAL SHEETS 14	SHEET NO. 8
	PLOT SCALE = 1" = 20'	DRAWN - JSS	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 61B65			
	PLOT DATE =	CHECKED - WJZ	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003-(486)								
		DATE - 01/26/15	REVISED -										





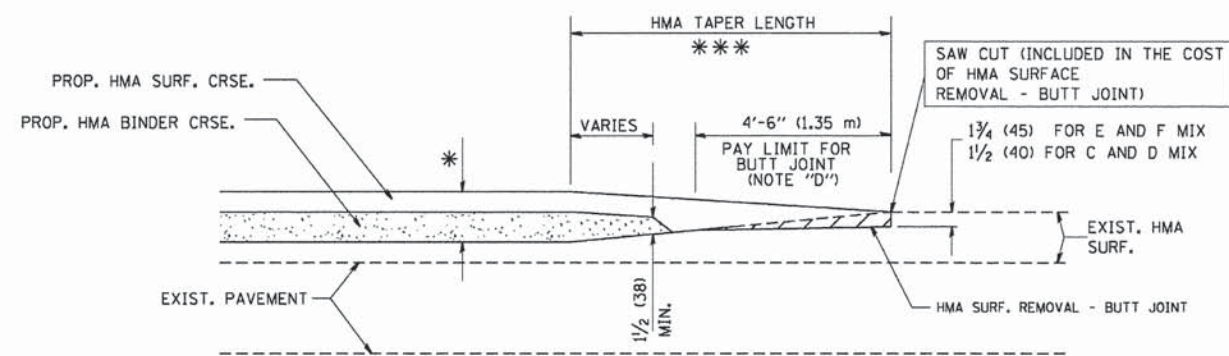
MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

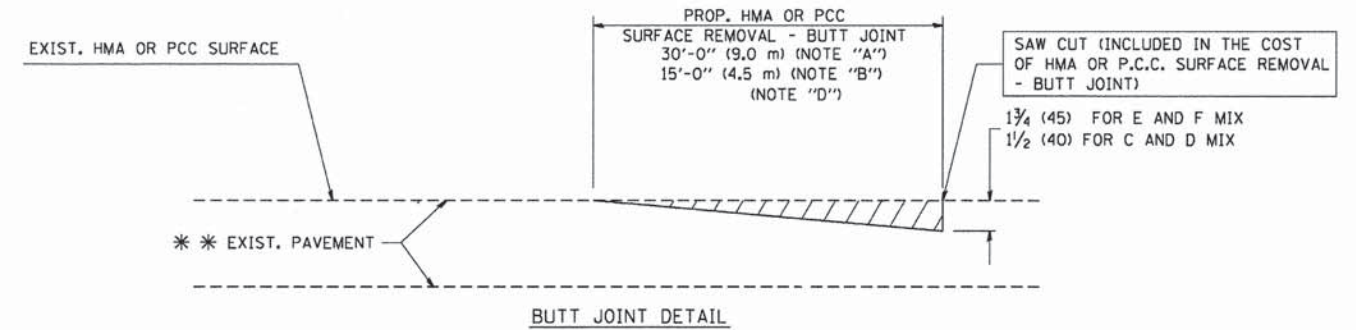


HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

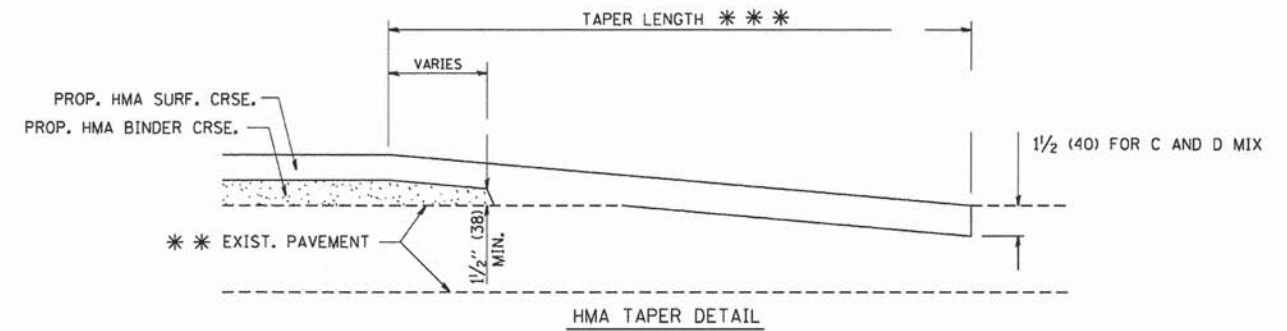
OPTION 2  
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER  
TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL  
TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

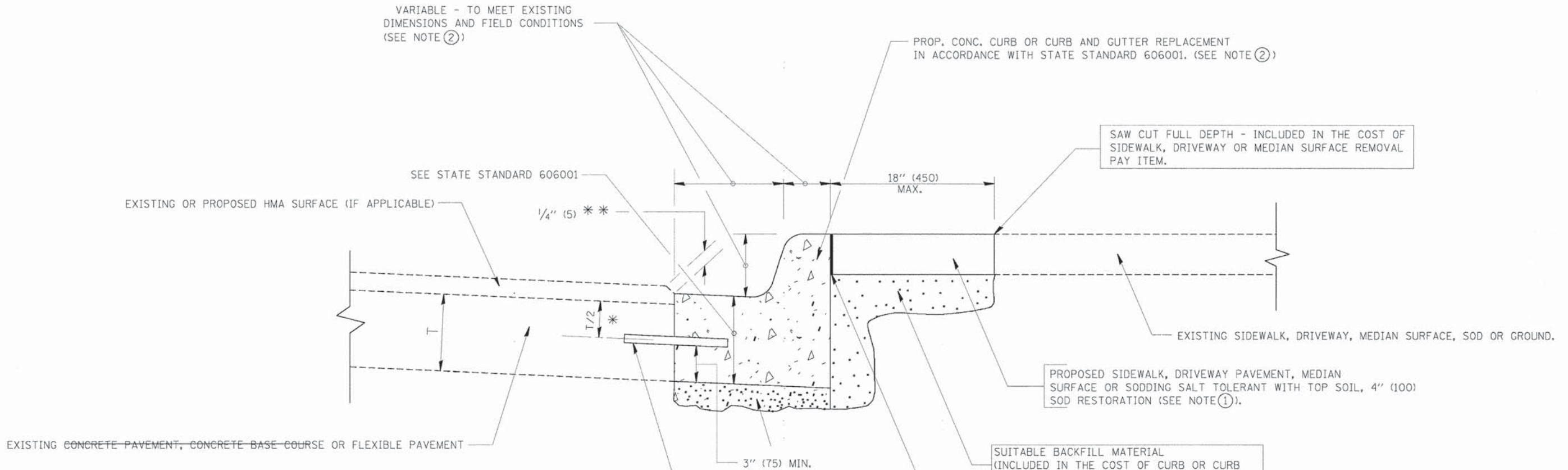
FILE NAME =	USER NAME =	DESIGNED -- TPP	REVISED --
		DRAWN -- JSS	REVISED --
	PLOT SCALE = 1" = 20'	CHECKED -- WJZ	REVISED --
	PLOT DATE =	DATE -- 01/26/15	REVISED --

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CALIFORNIA AVENUE FROM 167TH ST. TO SUSSEX COURT  
STP/LAFO  
DETAILS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1713	14-00045-00-RS	COOK	14	9
CONTRACT NO. 61B65				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003-(486)				



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
  - \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY. SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

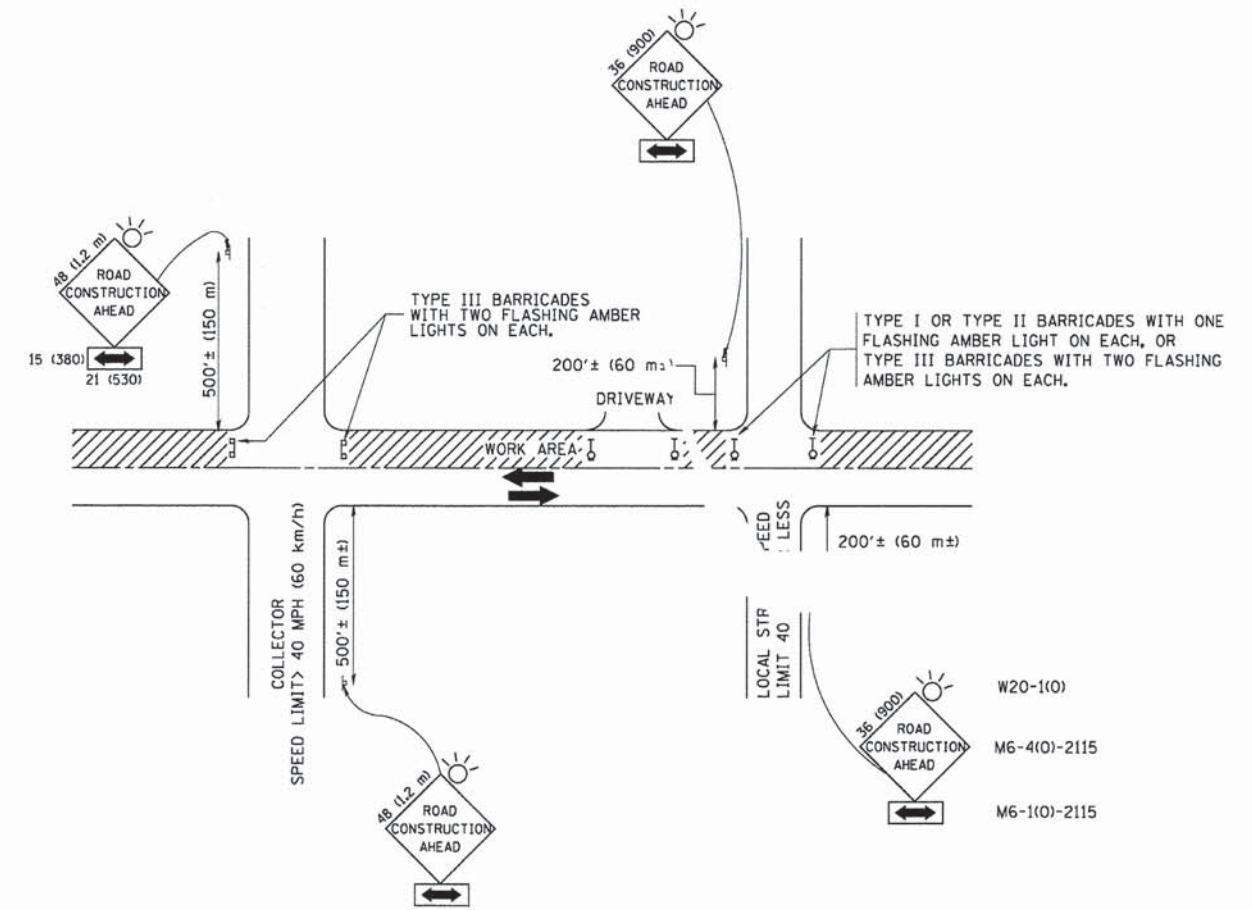
- SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

## CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME =	DESIGNED - TPP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CALIFORNIA AVENUE FROM 167TH ST. TO SUSSEX COURT ST/PLAFO DETAILS</b>	F.A.U. RTE. 1713	SECTION 14-00045-00-RS	COUNTY COOK	TOTAL SHEETS 14	SHEET NO. 10
PLOT SCALE = 1" = 20'	CHECKED - WJZ	DATE - 01/26/15	REVISED -	SCALE: SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 61B65 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003-(486)				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

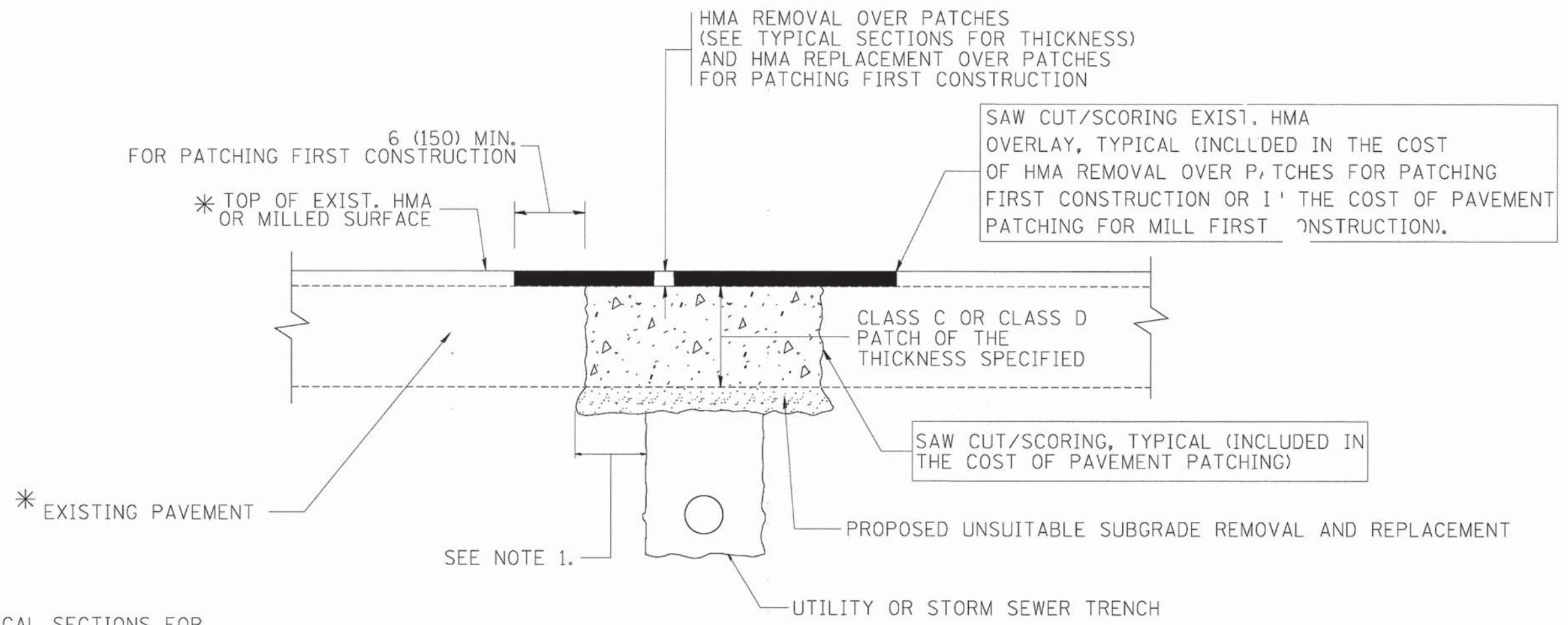
NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME =	DESIGNED - TPP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CALIFORNIA AVENUE FROM 167TH ST. TO SUSSEX COURT STP/LAFO DETAILS</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
	PLOT SCALE = 1" = 20'	DRAWN - JSS	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	1713	14-00045-00-RS	COOK	14	11
	PLOT DATE =	CHECKED - WJZ	REVISED -		<b>CONTRACT NO. 61B65</b>									
		DATE - 01/26/15	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003-(486)									



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

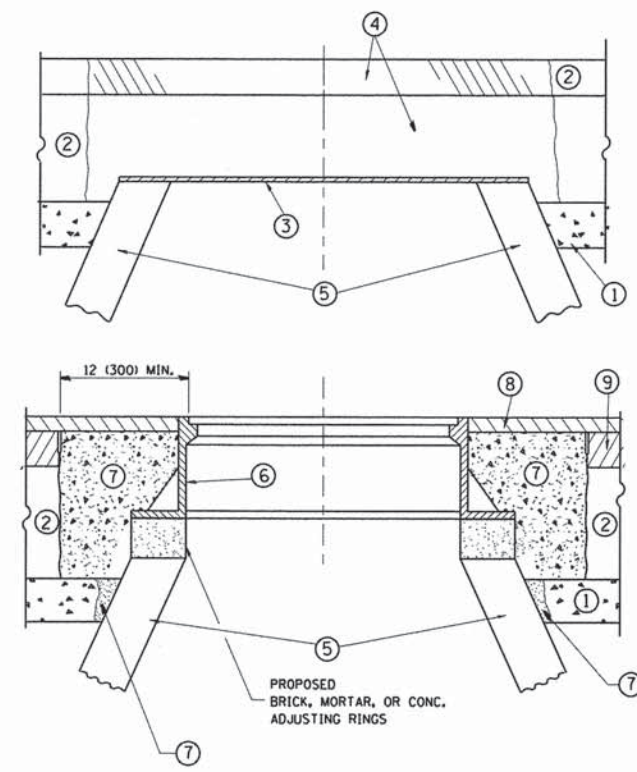
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME =	DESIGNED - TPP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	CALIFORNIA AVENUE FROM 167TH ST. TO SUSSEX COURT STP/LAFO DETAILS				F.A.U. RTE. 1713	SECTION 14-00045-00-RS	COUNTY COOK	TOTAL SHEETS 14	SHEET NO. 12
	PLOT SCALE = 1" = 20'	DRAWN - JSS	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 61B65			
	PLOT DATE =	CHECKED - WJZ	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003-(486)								
		DATE - 01/26/15	REVISED -										



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.

3S PP-1#  
AS PER THE EXISTING

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1# CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

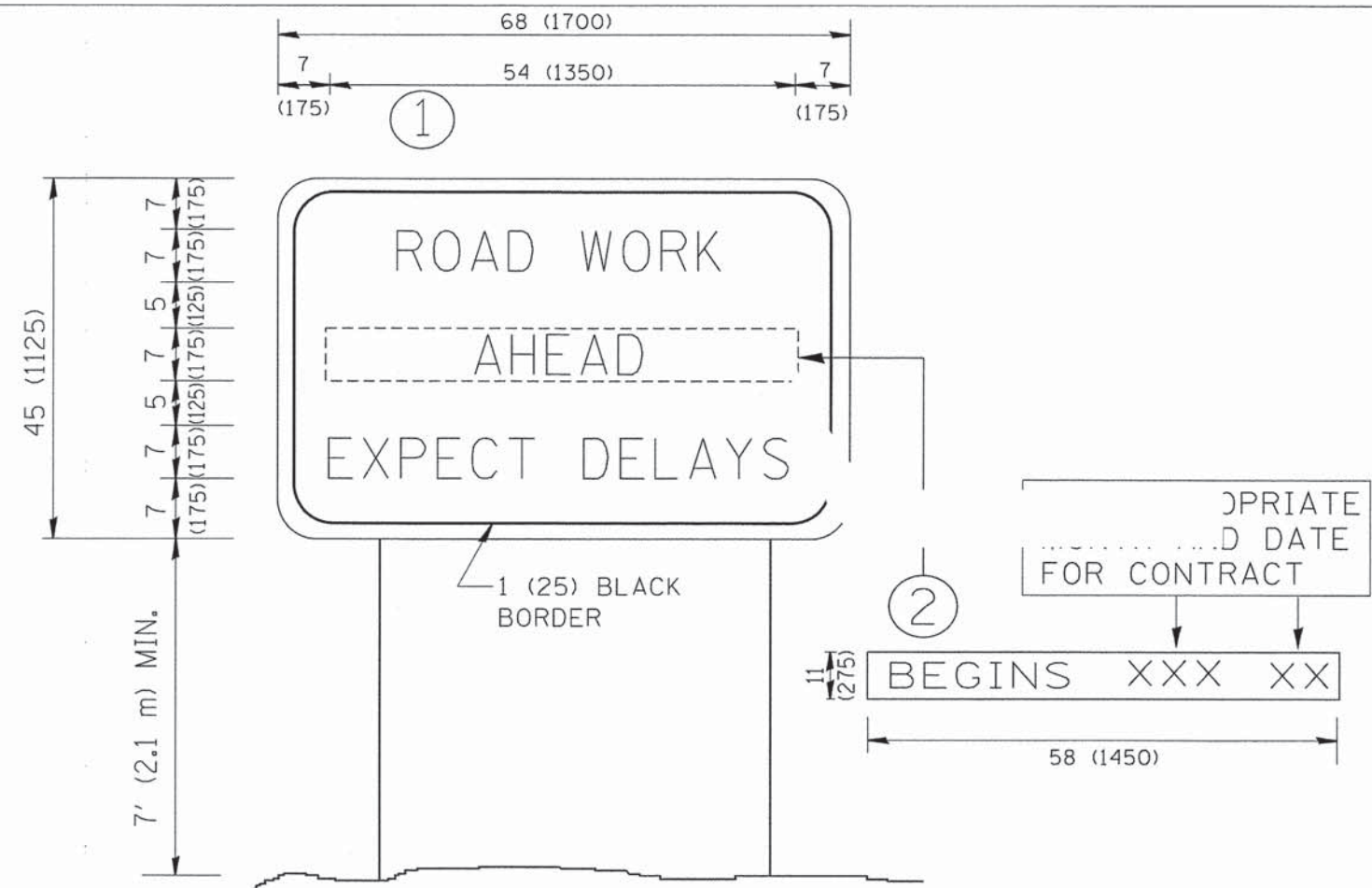
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME =	DESIGNED -- TPP	REVISED --	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CALIFORNIA AVENUE FROM 167TH ST. TO SUSSEX COURT STP/LAFO DETAILS</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 1" = 20'	DRAWN -- JSS	REVISED --			1713	14-00045-00-RS	COOK	14	13	
	PLOT DATE =	CHECKED -- WJZ	REVISED --			<b>CONTRACT NO. 61B65</b>					
		DATE -- 01/26/15	REVISED --			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003-(486)					
				SCALE:		SHEET NO. OF SHEETS		STA. TO STA.			



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME =	DESIGNED - TPP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CALIFORNIA AVENUE FROM 167TH ST. TO SUSSEX COURT STP/LAFO DETAILS</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 1" = 20'	DRAWN - JSS	REVISED -			1713	14-00045-00-RS	COOK	14	14	
	PLOT DATE =	CHECKED - WJZ	REVISED -			<b>CONTRACT NO. 61B65</b>					
		DATE - 01/26/15	REVISED -			<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003-(486)</small>					
					SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.		