06-12-2015 LETTING ITEM 042

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN: THE VILLAGE OF ADDISON

THE VILLAGE OF DOWNERS GROVE

THE VILLAGE OF HANOVER PARK THE VILLAGE OF HINSDALE

THE CITY OF AURORA THE VILLAGE OF BENSENVILLE THE VILLAGE OF CLARENDON HILLS

THE CITY OF ELMHURST

THE VILLAGE OF ITASCA THE VILLAGE OF LOMBARD

THE VILLAGE OF OAK BROOK THE CITY OF OAKBROOK TERRACE THE VILLAGE OF ROSELLE

THE VILLAGE OF SCHAUMBURG THE VILLAGE OF VILLA PARK THE VILLAGE OF WESTMONT THE CITY OF WOOD DALE

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STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

PROPOSED HIGHWAY PLANS

VARIOUS ROUTES SECTION: 2015-030RS VARIOUS LOCATIONS IN DUPAGE COUNTY INTERMITTENT RESURFACING **DUPAGE COUNTY**

C-91-330-15

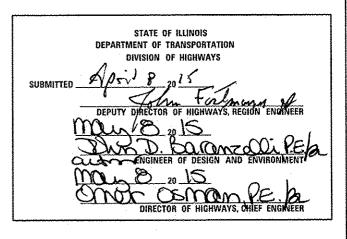
FOR GENERAL LOCATION MAP, SEE SHEET NO. 4



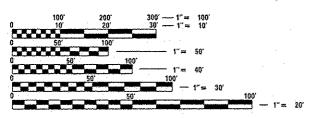
2015-030RS

DUPAGE 44 1

ILLINOIS CONTRACT NO. 62484



PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS



FILL SIZE PLANS HAVE BEEN PREPARED LISING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION 1-800-892-0123

PROJECT ENGINEER: DANIEL WILGREEN (847) 705-4240 PROJECT MANAGER: KEN ENG

CONTRACT NO. 62A84

INDEX OF SHEETS

STATE STANDARDS

| SHEET NO. | DESCRIPTION | STANDARD NO. | DESCRIPTION | | | |
|-----------|--|-------------------|--|--|--|--|
| | | | | | | |
| . 1 | COVER SHEET | 000001 - 06 | TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS | | | |
| 2 | INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES | 701011 - 04 | OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY | | | |
| 3 | SUMMARY OF QUANTITIES | 701301 - 04 | LANE CLOSURE, 2L. 2W, SHORT TIME OPERATIONS | | | |
| 4 | GENERAL LOCATION MAP | 701306 -03 | LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS - DAY ONLY | | | |
| 5 | ROUTE INFORMATION | 701311 - 03 | LANE CLOSURE 2L. 2W MOVING OPERATIONS - DAY ONLY | | | |
| 6 | SUMMARY OF INTERMITTENT RESURFACING SCHEDULE | 701336 - 06 | LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES | | | |
| 7-35 | INTERMITTENT RESURFACING SCHEDULE | 701421 - 07 | LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR | | | |
| 36 | BUTT JOINT AND HMA TAPER DETAILS (BD-32) | | SPEEDS > 45 MPH TO 55 MPH | | | |
| 37 | TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS. INTERSECTIONS AND DRIVEWAYS (TC-10) | 701426-07 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS \geq 45 MPH | | | |
| 38 | TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11) | 701427 - 03 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS & 40 MPH | | | |
| 39 | DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13) | 701502-06 | URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL | | | |
| 40 | TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (IC-14) | | LEFT TURN LANE | | | |
| 41 | PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC | 701601-09 | URBAN LANE CLOSURE, MULTILANE, IW OR 2W WITH NONTRAVERSABLE MEDIAN | | | |
| .41 | STAGING (TC-16) | 701602 - 07 | URBAN LANE CLOSURE. MULTILANE, 2W WITH BIDIRECTIONAL | | | |
| 42 | ARTERIAL ROAD INFORMATION SIGN (TC-22) | - , | LEFT TURN LANE | | | |
| 43 | STANDARD TRAFFIC SIGNAL DESIGN DETAILS (TS-05, SHEET 2 OF 7) | 701606 <i>-10</i> | URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN | | | |
| 44 | DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING. | 701701-09 | URBAN LANE CLOSURE, MULTILANE INTERSECTION | | | |
| | 415-011 | 701901-04 | TRAFFIC CONTROL DEVICES | | | |

| HOT-MIX ASPHALT MIXTURE RI | EQUIREMENTS | QUALITY MANAGEMENT | | |
|--|-----------------------------------|--------------------|--|--|
| MIXTURE TYPE | AJR VOIDS (%) @ N _{DES.} | PROGRAM (QMP) | | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5MM), 2" | 4% Q 70 GYR | QC/ QA | | |
| OMP DESIGNATIONS OHALITY CONTROL (OHALITY | ASSLIDANCE (OC/OA) | | | |

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 L8S/SO YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PC TO-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS. QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE (OR TOLLWAY) PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (OR ISTHA)

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

BEFORE BEGINNING ANY WORK. THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL INTERMITTENT RESURFACING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER AT (847) 741-9857 MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN

THE EXISTING ROADWAY TYPICAL SECTION IS ASSUMED TO HAVE A 3 INCH HOT-MIX ASPHALT OVERLAY ON TOP OF A TEN INCH CONCRETE BASE.

ALL INTERMITTENT RESURFACING LOCATIONS SHOWN IN THE PLANS ARE TWO (2) INCH MILL AND RESURFACE ONLY. THE MINIMUM WIDTH FOR INTERMITTENT RESURFACING SHALL BE

NO PATCHING OR RESURFACING IS TO BE DONE WITHIN FIFTY (50) FEET OF ANY RAILROAD CROSSING.

THE COST OF ANY PARTIAL OR FULL DEPTH PATCHING REQUIRED AFTER THE REMOVAL OF THE EXISTING 2 INCH HOT-MIX ASPHALT SURFACE SHALL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109,04 OF THE STANDARD SPECIFICATIONS.

ANY DETECTOR LOOPS DAMAGED BY MILLING SHALL BE REPLACED IN KIND. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO QUANTIFY LOOP REPLACEMENTS NEEDED AND PROVIDE THE RESIDENT ENGINEER THIS INFORMATION PRIOR TO GRINDING OR REMOVAL.

ALL LOOP DETECTOR LOCATIONS SHALL BE CURB MARKED BY THE CONTRACTOR PRIOR TO MILLING FOR THE PURPOSE OF REESTABLISHING DETECTOR LOOP LAYOUT AFTER THE RESURFACING IS

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC. THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).

OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS. ANY MILLED PAVEMENT IS TO BE RESURFACED BY THE END OF EACH DAY AND OPEN TO TRAFFIC.

SHEETS NO.

44 2 CONTRACT NO. 62A84

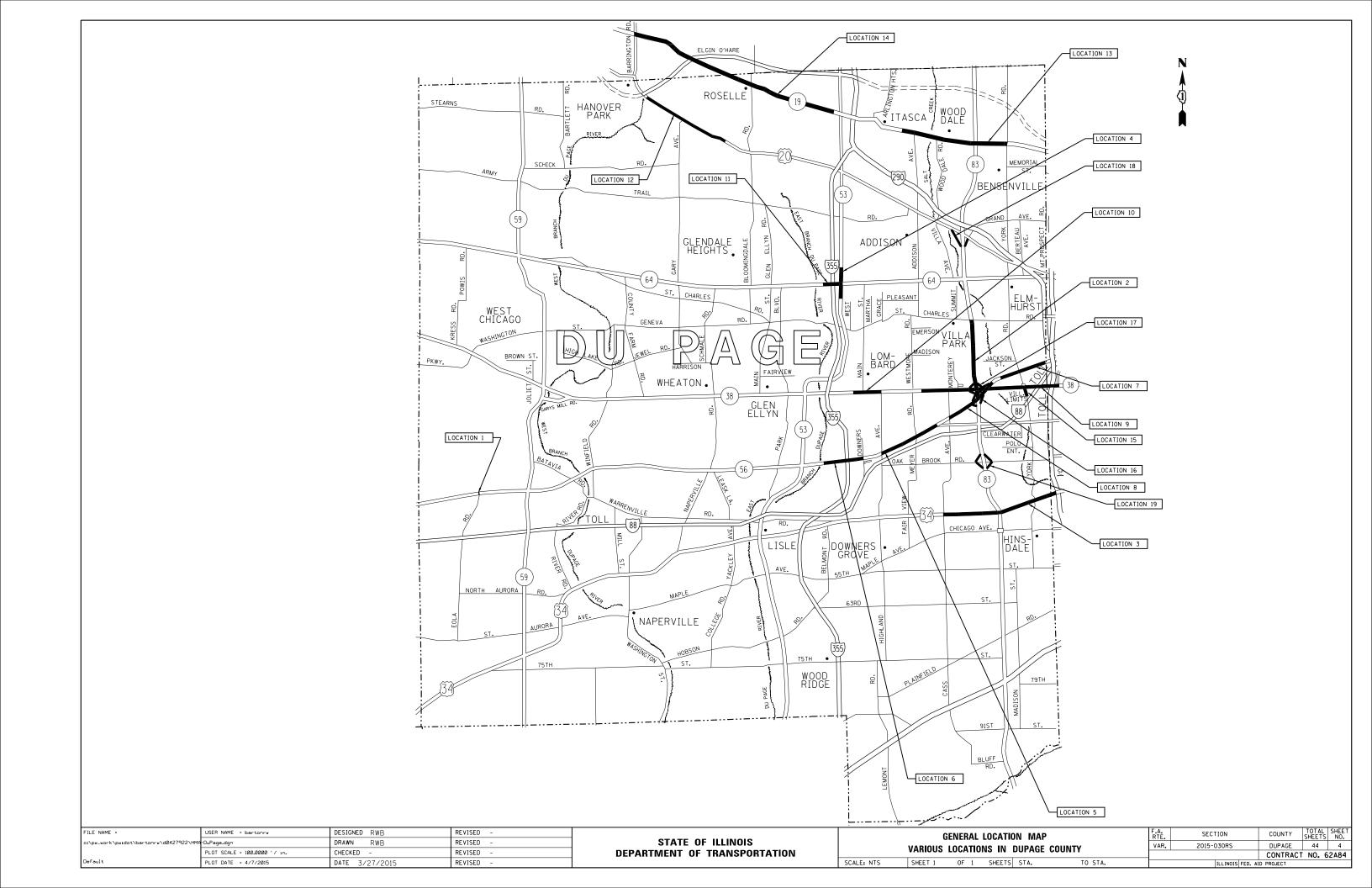
COUNTY DUPAGE

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| De Fault | PLOT DATE # 4/7/2015 | DATE - | REVISED - | |

| STATI | E OF | ILLINOIS |
|------------|------|----------------|
| DEPARTMENT | 0F | TRANSPORTATION |

| | OF SHEETS, STATE STANDARDS, AND GENERAL NOTES | | | | | RTE. | SECTION | Ĺ |
|---------|---|-------|--------|----------|---------------|------|---------------------|---|
| INDEX (| OF SHEETS, | SIAIL | SIANUA | IUS, ANU | GENERAL NOTES | YAR, | 2015-030RS | |
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| | CUECT 1 | OF. | SHEETS | STA | TO STA | - | In a provide con At | ~ |

| | | | | URBAN | , | | A.16 % A | A., a., a. | 5057 | | | | | ···· | URBAN | | | | ~ <u>~</u> | | |
|--|--------------------|---|--|--|---------------|--|--|--|--|--|--|--------------|---------------------------------------|------------|------------|---------------|---|--|---|--|-------------------------|
| | | SUMMARY OF QUANTITIES | | | | C | ONSTRUCTI | ON TYPE | CODE | - | | SUMMA | ARY OF QUANTITIES | | | | C: | ONSTRUCTI | ON TYPE CO | 30E | |
| (j | 2005 110 | | | TOTAL | 100% STATE | Andrews Andrews | | | | edwerter der der der der der der der der der d | 6005 110 | | | | TOTAL | 100% STATE | | *************************************** | | | |
| | CODE NO | ITEM | UNIT | QUANTITIES | 0005 | | anananananan angangan | | | | CODE NO | | ITEM | UNIT | QUANTITIES | 0005 | | | | İ | |
| | 40600275 | BITUMINOUS MATERIALS (PRIME COAT) | POUND | 15937 | 15937 | | and the same of th | | | | * 78000500 | THERMOPLAST | IC PAVEMENT MARKING - LINE 8 11 | FOOT | 159 | 159 | | | | | |
| | | | | | | derine ver Andreas de San de S | | | | | | | | | | | | Amagan and a second | | | |
| | 40600400 | MIXTURE FOR CRACKS, JOINTS, AND | TON | 54 | 54 | | | | | | | | | | | | | | | | |
| | | FLANCEWAYS | | | - | | Anna Anna Anna Anna Anna Anna Anna Anna | ~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~ | | | * 78000600 | THERMOPLAST | IC PAVEMENT MARKING - LINE 12" | FOOT | 88 | 88 | | | | | |
| | 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT | SQ YD | 1063 | 1063 | And the state of t | and the second s | | menders who makes the control of the Control | | | nouth- | | | | | | | | | |
| and the same of th | | JOINT | | | | | | | | | * 78000650 | THERMOPLAST | IC PAVEMENT MARKING - LINE 24" | FOOT | 130 | 130 | | | | | |
| and the state of t | 40603340 | HOT-MIX ASPHALT SURFACE COURSE. MIX | TON | 3967 | 3967 | | THE PROPERTY OF THE PROPERTY O | | and the state of t | at the state of th | | | | | | | | - | | | |
| | 408003340 | "O", N70 | I UN | | 3301 | | | | delete that we denote the state of the state | 41.414 | * 78100100 | RAISED REFL | ECTIVE PAVEMENT MARKER | EACH | 1112 | 1112 | | | | | |
| | | | | | | | | | | | | | | | | | | | | | |
| | 44000157 | HOT-MIX ASPHALT SURFACE REMOVAL, 2" | SO YD | 35415 | 35415 | | 4444 | | | *************************************** | 78300200 | RAISED REFLI | ECTIVE PAVEMENT MARKER CEMOVAL | EACH | 1112 | 1112 | | | | dimmi in mare the section of | |
| | | | *************************************** | | | | | | | 1 | | | | | <u></u> | | | | | | |
| | 60300305 | FRAMES AND LIDS TO BE ADJUSTED | EACH | 10 | 10 | | | | | | * 88600600 | DETECTOR LO | OP REPLACEMENT | FQOT | 1405 | 1405 | | - | | is a series of the series of t | |
| | 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 6 | 6 | | de frances de la constante de | | the desired and the second sec | and the state of t | e-britanni branch gerichten | | | | | | | | | and the state of t | |
| | | | | | | · | | | | A. | 20030850 | TEMPORARY II | NFORMATION SIGNING | SQ FT | 1208 | 1208 | | | | | |
| | 67100100 | MOBILIZATION | LSUM | 1 | | | | | | or a series of the series of t | Carolina sanarana | | | | | | | | | | |
| | | • | | | | | | | | <u> </u> | | | | | | | | | | | |
| | 70300520 | PAVEMENT MARKING TAPE, TYPE III 4" | FOOT | 2676 | 2676 | | | | | | | | · | | | | *************************************** | | | The state of the s | |
| | 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 892 | 892 | | | | | the service state of the servi | | | | | | | | | | | |
| | - | | manus militaris del | | | | | ······································ | | and the second s | | | | | | | | | | | |
| * | 78000100 | THERMOPLASTIC PAVEMENT MARKING - | SO FT | 1038 | 1038 | | | | | | **** | | | | | | | | | | |
| | | LETTERS AND SYMBOLS | Anna de constante | 4 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 | | | | · · · · · · | Anna Anna and Anna an | And a contract of the contract | Grand Control of Contr | | | | | | | multura tupaa (dagamayana) | | | |
| * | 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4 | FOOT | 26112 | 26112 | | - | | were transference of the second secon | And the second s | Per de la constante de la cons | | | | | | | | | | |
| | | | Province and the second | The state of the s | | | | | | | | | | | | | | 1 | | | |
| ्. | 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE (6) | FOOT | 3322 | 3322 | | | | | | | | · | | - | | | and the state of t | | | |
| * | | | anvertina and a second | and the same of th | | | - 1 | * SP | ECIALTY I | тем | | | | | | | | The state of the s | *************************************** | and the same of th | |
| | FILE HAME : | <u> </u> | I SIGNED - R | | REVISED | | | | | | <u></u> | <u>L</u> | | | <u></u> | l.: | F.A. RTE. | SECTI | ION | COUNTY SH | OTAL SHEET HEETS NO. |
| | es/pauwork/paudoti | | AWN - R | W8 | REVISED - | | | | | STATE OF | ILLINOIS RANSPORTA | TION | SUMMARY | OF QUANT | ITIES | | VAR. | 2015-0 | | DUPAGE | 44 3 |
| | Default | | E - 4 | /3/2015 | REVISED | | | | CHIII | UI I | ut unim | | SCALE: SHEET 1 OF 1 | SHEETS STA | . 1 | TO STA. | | [1 | LLINOIS FED. AID F | CONTRACT I | 4V. 0ZA84 |



| | | 1 | | 1 | |
|--------|---|---|--|-------------|---------------------|
| | SUMMARY - DUPAGE COUNTY ARTERIAL ROUTES | CITIES/VILLAGES | TOWNSHIPS | SPEED LIMIT | EXISTING ADT (YEAR) |
| LOC.1 | WB IL 56 (500' WEST OF EOLA RD.) | AURORA | WINFIELD | 55 MPH | 17,500 (2013) |
| LOC.2 | IL 83 (IL 56 TO ST. CHARLES RD.) | ELMHURST, OAKBROOK TERRACE, VILLA PARK | YORK | 45-50 MPH | 71,800 (2013) |
| LOC.3 | US 34 (CASS AVE. TO I-294) | CLARENDON HILLS, HINSDALE, WESTMONT DOWNERS GROVE, LYONS | | 35 MPH | 35,600 (2013) |
| LOC.4 | IL 53 (PLEASANT LN. TO SIDNEY AVE.) | ADDISON, LOMBARD | ADDISON, BLOOMINGDALE, MILTON, YORK | 40 MPH | 21,300 (2013) |
| LOC.5 | IL 56 (22ND ST. TO HIGHLAND AVE.) | LOMBARD, OAK BROOK | YORK | 45 MPH | 46,800 (2013) |
| LOC.6 | IL 56 (DOWNERS DR. TO GRAY AVE.) | DOWNERS GROVE | MILTON, YORK | 45-50 MPH | 54,900 (2013) |
| LOC.7 | IL 56 (PROSPECT AVE. TO CALDWELL AVE.) | ELMHURST | YORK | 35 MPH | 17,500 (2013) |
| LOC.8 | IL 56 (SUMMIT AVE. TO COMMONWEALTH LN.) | ELMHURST, OAKBROOK TERRACE | YORK | 35-45 MPH | 22,600 (2013) |
| LOC.9 | IL 38 (WISCONSIN AVE. TO HARRISON ST.) | ELMHURST, OAK BROOK, OAKBROOK TERRACE, VILLA PARK | PROVISO, YORK | 35-55 MPH | 54,700 (2013) |
| LOC.10 | IL 38 (HIGHLAND AVE. TO FINLEY RD.) | LOMBARD | YORK | 35 MPH | 41,600 (2013) |
| LOC.11 | IL 64 (IL 53 TO SWIFT RD.) | LOMBARD | BLOOMINGDALE, MILTON | 45 MPH | 51,600 (2013) |
| LOC.12 | US 20 (SUMMERFIELD DR. TO GREENBROOK BLVD.) | HANOVER PARK, ROSELLE | BLOOMINGDALE | 40 MPH | 34,200 (2013) |
| LOC.13 | IL 19 (PROSPECT AVE. TO YORK RD.) | BENSENVILLE, WOOD DALE | ADDISON | 30-35 MPH | 26,200 (2013) |
| LOC.14 | IL 19 (IL 53 TO BARRINGTON RD.) | HANOVER PARK, ITASCA, ROSELLE, SCHAUMBURG | BLOOMINGDALE, SCHAUMBURG | 30-45 MPH | 34,600 (2013) |
| LOC.15 | YORK RD. (IL 38 TO NORTH OF I-88) | ELMHURST | YORK | 45 MPH | 14,300 (2008) |
| LOC.16 | IL 56 (RAMPS AT IL 83) | OAK BROOK, OAKBROOK TERRACE | YORK | 25-30 MPH | 3,400 (2012) |
| LOC.17 | IL 38 (RAMPS AT IL 83 AND WB IL 56) | ELMHURST, OAKBROOK TERRACE YORK | | 25-30 MPH | 2,850 (2012) |
| LOC.18 | US 20 (RAMPS AT IL 83) | ADDISON, ELMHURST | ADDISON | 25-45 MPH | 24,300 (2005) |
| LOC.19 | 31ST ST. (RAMPS AT IL 83) | OAK BROOK | YORK | N/A | 3,450 (2012) |

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| Default | PLOT DATE = 4/7/2015 | DATE - | - | 4/6/2015 | REVISED | - |

| STATI | E OF | ILLINOIS |
|------------|------|----------------|
| DEPARTMENT | 0F | TRANSPORTATION |

| ROUTE INFORMATION | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | | |
|--------------------------------|---------|---------------------------|---------------------------|--------|-----------------|--------------|--|--|
| VARIOUS LOCATIONS IN DUPAGE C | VAR. | VAR. 2015-030RS DUPAGE 44 | | | | | | |
| VAIII003 LOCATIONS IN DOLAGE C | UUNII | | CONTRACT NO. 62A84 | | | | | |
| SHEET 1 OF 1 SHEETS STA. | TO STA. | | ILLINOIS FED. AID PROJECT | | | | | |
| | | | | | | | | |

| | | HMA 2" MILL |
|--------|---|--------------|
| | SUMMARY - DUPAGE COUNTY ARTERIAL ROUTES | & RESURFACE |
| | | (SY) |
| | | |
| LOC.1 | WB IL 56 (500' WEST OF EOLA RD.) | 67 |
| 1003 | II 02 /II FC TO CT CHARLEC RD \ | 107 |
| LOC.2 | IL 83 (IL 56 TO ST. CHARLES RD.) | 107 |
| LOC.3 | US 34 (CASS AVE. TO I-294) | 1,293 |
| | (0.007.001.101.201) | |
| LOC.4 | IL 53 (PLEASANT LN. TO SIDNEY AVE.) | 60 |
| | | |
| LOC.5 | IL 56 (22ND ST. TO HIGHLAND AVE.) | 1,160 |
| | | |
| LOC.6 | IL 56 (DOWNERS DR. TO GRAY AVE.) | 723 |
| | | |
| LOC.7 | IL 56 (PROSPECT AVE. TO CALDWELL AVE.) | 957 |
| LOC.8 | IL 56 (SUMMIT AVE. TO COMMONWEALTH LN.) | 561 |
| 100.8 | IL 36 (30 MINITI AVE. TO COMMONWEALTH LIV.) | 201 |
| LOC.9 | IL 38 (WISCONSIN AVE. TO HARRISON ST.) | 5,334 |
| 200.0 | | 5,55 . |
| LOC.10 | IL 38 (HIGHLAND AVE. TO FINLEY RD.) | 413 |
| | | |
| LOC.11 | IL 64 (IL 53 TO SWIFT RD.) | 176 |
| | | |
| LOC.12 | US 20 (SUMMERFIELD DR. TO GREENBROOK BLVD.) | 3,893 |
| 10040 | H 40 (PROCEST AVE TO VORKER) | 0.744 |
| LOC.13 | IL 19 (PROSPECT AVE. TO YORK RD.) | 8,714 |
| LOC 14 | IL 19 (IL 53 TO BARRINGTON RD.) | 7,873 |
| 100.14 | IL 19 (IL 33 TO DARRING TON RD.) | 7,873 |
| LOC.15 | YORK RD. (IL 38 TO NORTH OF I-88) | 293 |
| | (2007) | |
| LOC.16 | IL 56 (RAMPS AT IL 83) | 1,171 |
| | | |
| LOC.17 | IL 38 (RAMPS AT IL 83 AND WB IL 56) | 1,849 |
| | | |
| LOC.18 | US 20 (RAMPS AT IL 83) | 435 |
| | | |
| LOC.19 | 31ST ST. (RAMPS AT IL 83) | 336 |
| | DUDACE COUNTY ARTERIAL TOTAL | 25 445 |
| | DUPAGE COUNTY ARTERIAL TOTAL = | 35,415 SY |
| | | 31 |

| ILE NAME = USER NAME = bartonrw | | DESIGNED - | | RWB | REVISED | = |
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| Default | PLOT DATE = 4/7/2015 | DATE - | | 4/6/2015 | REVISED | = |

| SUMMARY OF INTERMITTENT RESURFACING SCHEDULE | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|---------------------------|------------|----------|-----------------|--------------|
| VARIOUS LOCATIONS IN DUPAGE COUNTY | VAR. | 2015-030RS | DUPAGE | 44 | 6 |
| VAIII003 EUCATIONS IN DOLAGE COUNT | | | CONTRACT | NO. (| 2A84 |
| SHEET 1 OF 1 SHEETS STA. TO STA. | ILLINOIS FED. AID PROJECT | | | | |

ROUTE: IL 56 (500' West of Eola Road)

| CROSS S | CROSS STREET | | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-----------|------------------------|---------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| Eola Road | 500' West of Eola Road | WB | 1 | 12 | 50 | 600 | 67 |
| | | | | | | | |

TOTALS: 50 67 FT SY

ROUTE: IL 83 (IL 56 to St. Charles Road)

| CROSS S | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|------------------|------------------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| IL 56 | | NB | 1 | 12 | 5 | 60 | 7 |
| | | NB | 1 | 12 | 5 | 60 | 7 |
| | | NB | 1 | 12 | 3 | 36 | 4 |
| | | NB | 2 | 12 | 5 | 60 | 7 |
| | | NB | 2 | 12 | 5 | 60 | 7 |
| | | NB | 2 | 12 | 3 | 36 | 4 |
| | | NB | 3 | 12 | 5 | 60 | 7 |
| | | NB | 3 | 12 | 5 | 60 | 7 |
| | St. Charles Road | NB | 3 | 12 | 3 | 36 | 4 |
| St. Charles Road | | SB | 1 | 12 | 3 | 36 | 4 |
| | | SB | 1 | 12 | 3 | 36 | 4 |
| | | SB | 1 | 12 | 3 | 36 | 4 |
| | | SB | 1 | 12 | 3 | 36 | 4 |
| | | SB | 2 | 12 | 3 | 36 | 4 |
| | | SB | 2 | 12 | 3 | 36 | 4 |
| | | SB | 2 | 12 | 3 | 36 | 4 |
| | | SB | 2 | 12 | 3 | 36 | 4 |
| | | SB | 2 | 6 | 5 | 30 | 3 |
| | | SB | 2 | 6 | 5 | 30 | 3 |
| | | SB | 3 | 12 | 3 | 36 | 4 |
| | | SB | 3 | 12 | 3 | 36 | 4 |
| | | SB | 3 | 12 | 3 | 36 | 4 |
| | IL 56 | SB | 3 | 12 | 3 | 36 | 4 |
| | | | | | | | |

TOTALS: 85 107 FT SY

ROUTE: US 34 (Cass Avenue to I-294)

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------|--------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| I-294 | | WB | LT | 3 | 20 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 6 | 10 | 60 | 7 |
| | | WB | LT | 3 | 200 | 600 | 67 |

ROUTE: US 34 (Cass Avenue to I-294) (Continued)

| STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------------|-------------|--|---|--|--|--|
| | | NO. | PATCH | PATCH | AREA | AREA |
| | | l | | | | (SQ YD) |
| | | | | | | 7 |
| + | | | | | | 33 |
| | | | | | | 3 |
| | | | | | | 7 |
| + | | | | | | 10 |
| | | | | | | 7 |
| | | | | | | |
| | | L | | | | 7 |
| | | | | | | 7 |
| | | | | | | 7 |
| | | | | | | 7 |
| | | | | | | 3 |
| | | 1 | | | | 7 |
| | | 1 | | | | 17 |
| | | 1 | | | 60 | 7 |
| | WB | 1 | 3 | 100 | 300 | 33 |
| | WB | 1 | 12 | 10 | 120 | 13 |
| | WB | 1 | 12 | 5 | 60 | 7 |
| | WB | CL | 3 | 50 | 150 | 17 |
| 1 | | 1 | | | | 33 |
| 1 | | | | | | 33 |
| | | | | | | 20 |
| + | | | | | | 33 |
| + | | | | | | 20 |
| + | | | | | | 17 |
| + | | | | | | 13 |
| | | | <u> </u> | | | |
| | | | | | | 13 |
| | | | | | | 13 |
| | | | | | | 27 |
| | | | | | | 7 |
| | | | | | | 7 |
| Cass Avenue | | | | | | 7 |
| | | | | | | 3 |
| | WB | | | 20 | 120 | 13 |
| | WB | | 6 | 20 | 120 | 13 |
| | WB | 2 | 3 | 10 | 30 | 3 |
| | WB | 2 | 3 | 20 | 60 | 7 |
| | WB | | | 10 | 60 | 7 |
| | | | | | | 3 |
| 1 | | | | | | 7 |
| 1 | | | | | | 3 |
| + | | | | | | 7 |
| + | | | | | | 7 |
| + | | | | | | 7 |
| + | | | | | | 7 |
| + | | | | | | 7 |
| + | | | | | | 4 |
| | | | | | | 10 |
| | | | | | | 3 |
| | | | | | | 7 |
| | | | | | | 7 |
| | | | | | | 3 |
| | | | | | | 7 |
| | WB | 2 | 12 | 10 | 120 | 13 |
| Cass Avenue | WB | 2 | 3 | 10 | 30 | 3 |
| | | | | 50 | 150 | 17 |
| | EB | LT | 3 | 00 | 150 | 1 17 |
| | EB EB | LT LT | 3 | 50 | 150 | 17 |
| | Cass Avenue | TO (EB/WB) (NB/SB) WB | TO (EB/WB) (NO. (NB/SB) (1,2,3) WB 1 WB CL WB 1 WB 2 TO (EB/MB) NO. PATCH (MB/SB) (1,2,3) WIDTH WB 1 1 3 WB 1 3 3 WB 1 6 WB 1 1 3 WB 1 1 12 WB 1 1 3 WB 1 1 12 WB 1 3 WB 1 1 12 WB 1 3 WB 1 6 WB 1 3 WB 1 6 WB 1 6 WB 1 3 WB 1 6 WB 2 3 WB 2 6 WB 2 6 WB 2 3 WB 2 6 WB 2 6 WB 2 3 WB 2 6 TO (EB/WB) NO. (PATCH (NE/SB) (1,2,3) WDTH LENGTH (NE/SB) (1,2,3) WDTH LENGTH (SWB 1 122 5 SWB 1 1 1 | TO (BEAMS) NO, PATCH (NO FATCH (NO F |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMI | ITTENT RES | HRFAC | ING SCHEDI | III F | F.A. RTF | SECTION | COUNTY | TOTAL SHEET |
|---|-------------------------------|-----------------|-----------|------------------------------|--------|---------|------------|--------|------------|---------|-------------|---------------|-------------|-------------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | | IL 56 / IL | | | OLL. | VAR. | 2015-030RS | DUPAGE | 44 7 |
| | PLOT SCALE = 100.0000 ' / in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | | IL 30 / IL | 03 / U | 3 34 | | | | CONTRACT | T NO. 62A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - | | SCALE: | SHEET 1 | OF 29 | SHEETS | STA. | TO STA. | | ILLINOIS FED. | AID PROJECT | |

ROUTE: US 34 (Cass Avenue to I-294) (Continued)

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------------|--------|-----------|-----------|----------|----------|------------|----------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | LT | 3 | 200 | 600 | 67 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 6 | 10 | 60 | 7 |
| | | EB | 1 | 6 | 5 | 30 | 3 |
| | | EB | LT | 3 | 100 | 300 | 33 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 6 | 10 | 60 | 7 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | | EB | 1 | 6 | 20 | 120 | 13 |
| | | EB | 1 | 3 | 200 | 600 | 67 |
| | | EB | 1 | 6 | 40 | 240 | 27 |
| | | EB | 1 | 6 | 10 | 60 | 7 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 6 | 10 | 60 | 7 |
| | | EB | 1 | 12 | 20 | 240 | 27 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | I-294 | EB | 1 | 3 | 20 | 60 | 7 |
| Cass Avenue | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 30 | 90 | 10 |
| | | EB | 2 | 3 | 30 | 90 | 10 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 6 | 10 | 60 | 7 |
| | | EB | 2 | 6 | 10 | 60 | 7 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 6 | 10 | 60 | 7 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB EB | 2 | 12 | 10 20 | 120 60 | 13 7 |
| | | | 2 | 3 | | | |
| | | EB | 2 | 6 | 10 | 90 90 | 7 |
| | | EB EB | 2 | 3 | 30 | | 10 |
| | | | 2 | 12 | 10 | 120 | 13 13 |
| | | EB EB | 2 | 6 6 | 20 | 120 120 | 13 |
| | | EB | 2 | 3 | 20 30 | 90 | 10 |
| | | EB | 2 | 6 | 10 | 60 | 7 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 10 | 30 | 3 |
| | | EB | 2 | 6 | 20 | 120 | 13 |
| | | EB | 2 | 3 | 30 | 90 | 10 |
| | I-294 | EB | 2 | 6 | 20 | 120 | 13 |
| | 1-234 | ED | | 0 | 20 | 120 | 13 |
| 1 | I | I | 1 | ı | 1 | | 1 |

TOTALS: 2880 1293 FT SY

ROUTE: IL 53 (Pleasant Lane to Sidney Avenue)

| CROSS S | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|---------------|---------------|-----------|-----------|----------|----------|---------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| Sidney Avenue | | SB | 1 | 3 | 30 | 90 | 10 |
| | | SB | 1 | 3 | 30 | 90 | 10 |
| | Pleasant Lane | SB | 1 | 3 | 10 | 30 | 3 |
| Pleasant Lane | | NB | 1 | 3 | 20 | 60 | 7 |
| | | NB | 1 | 6 | 20 | 120 | 13 |
| | | NB | RT | 12 | 5 | 60 | 7 |
| | | NB | RT | 12 | 5 | 60 | 7 |
| | Sidney Avenue | NB | 1 | 3 | 10 | 30 | 3 |
| | | | | | | | |

TOTALS: 130 60 FT SY

ROUTE: IL 56 (22nd Street to Highland Avenue)

| CROSS S | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------------|-----------------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| 22nd Street | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | LT | 3 | 40 | 120 | 13 |
| | | WB | LT | 3 | 20 | 60 | 7 |
| | | WB | 1 | 3 | 30 | 90 | 10 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | LT | 3 | 30 | 90 | 10 |
| | | WB | 1 | 3 | 100 | 300 | 33 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | LT | 3 | 100 | 300 | 33 |
| | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 3 | 100 | 300 | 33 |
| | | WB | LT | 3 | 200 | 600 | 67 |
| | | WB | 1 | 3 | 30 | 90 | 10 |
| | | WB | 1 | 6 | 10 | 60 | 7 |
| | Highland Avenue | WB | 1 | 6 | 10 | 60 | 7 |
| 22nd Street | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 3 | 100 | 300 | 33 |
| | | WB | 2 | 3 | 100 | 300 | 33 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 3 | 100 | 300 | 33 |
| | | WB | 2 | 3 | 100 | 300 | 33 |
| | | WB | 2 | 3 | 100 | 300 | 33 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 3 | 30 | 90 | 10 |
| | | WB | 2 | 3 | 100 | 300 | 33 |
| | Highland Avenue | WB | 2 | 3 | 20 | 60 | 7 |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMITTENT RESURFACING SCHEDULE | F., | A. | SECTION | COUNTY | TOTAL | SHEET NO. |
|---|------------------------------|-----------------|-----------|------------------------------|--------|-----------------------------------|-----|-----|------------------|----------|---------|--------------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | US 34 / IL 53 / IL 56 | V. | AR. | 2015-030RS | DUPAGE | 44 | 8 |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | US 34 / IL 33 / IL 30 | | | | CONTRACT | T NO. F | 2A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - | | SCALE: | SHEET 2 OF 29 SHEETS STA. TO STA. | | | ILLINOIS FED. AI | | | |

ROUTE: IL 56 (22nd Street to Highland Avenue) (Continued)

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-----------------|-----------------|-----------|-----------|----------|----------|---------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| 22nd Street | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | Highland Avenue | WB | 3 | 12 | 5 | 60 | 7 |
| Highland Avenue | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 6 | 10 | 60 | 7 |
| | | EB | 1 | 3 | 200 | 600 | 67 |
| | | EB | 1 | 3 | 100 | 300 | 33 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | | EB | 1 | 3 | 250 | 750 | 83 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | 22nd Street | EB | 1 | 3 | 300 | 900 | 100 |
| Highland Avenue | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 3 | 30 | 90 | 10 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 3 | 40 | 120 | 13 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 30 | 90 | 10 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 40 | 120 | 13 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 3 | 30 | 90 | 10 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 40 | 120 | 13 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 100 | 300 | 33 |
| | 22nd Street | EB | 2 | 3 | 40 | 120 | 13 |
| Highland Avenue | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | 22nd Street | EB | 3 | 12 | 5 | 60 | 7 |
| | | | | | | | |

TOTALS: 3045 1160 FT SY

ROUTE: IL 56 (Downers Drive to Gray Avenue)

| CROSS S | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------------|--------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| Gray Avenue | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 5 | 60 | 7 |

ROUTE: IL 56 (Downers Drive to Gray Avenue) (Continued)

| CROSS | STREET | DIRECTION | LANE | PAV/EMENIT | PAVEMENT | REPAIR | REPAIR |
|---------------|----------------|-----------|-----------|------------|----------|---------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| FROIVI | | (NB/SB) | (1, 2, 3) | WDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 6 | 30 | 180 | 20 |
| | | EB | 1 | 12 | 20 | 240 | 27 |
| | | EB | 1 | 6 | 100 | 600 | 67 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | LT1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | Dournors Drive | EB | | 3 | | 60 | 7 |
| Croy Avanua | Downers Drive | EB | 1 | 12 | 20 5 | 60 | 7 |
| Gray Avenue | | EB | 2 2 | 12 | 10 | 120 | 13 |
| | | EB | | | 5 | 60 | 7 |
| | | EB | 2 | 12 | 20 | 120 | 13 |
| | | | 2 | 6 | | | |
| | | EB | 2 | | 100 | 600 | 67 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | Downers Drive | EB | 2 | 12 | 5 | 60 | 7 |
| Gray Avenue | | EB | 3 | 3 | 30 | 90 | 10 |
| | | EB | RT | 12 | 10 | 120 | 13 |
| | | EB | 3 | 3 | 30 | 90 | 10 |
| | | EB | RT | 12 | 20 | 240 | 27 |
| | | EB | 3 | 3 | 20 | 60 | 7 |
| | | EB | 3 | 3 | 10 | 30 | 3 |
| | Downers Drive | EB | 3 | 12 | 20 | 240 | 27 |
| Downers Drive | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 3 | 100 | 300 | 33 |
| | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | LT1 | 12 | 20 | 240 | 27 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 3 | 20 | 60 | 7 |
| _ | Gray Avenue | WB | 1 | 3 | 10 | 30 | 3 |
| Downers Drive | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 3 | 100 | 300 | 33 |
| | Gray Avenue | WB | 2 | 12 | 5 | 60 | 7 |
| Downers Drive | | WB | 3 | 3 | 20 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | Gray Avenue | WB | 3 | 12 | 5 | 60 | 7 |
| | | | | | | | |
| | | | | | | | |

| TOTALS: | 1065 | 72 |
|---------|------|----|
| | FT | S |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMITTENT RESURFACING SCHEDULE | F.A. RTE. | SECTION | COUNTY TOTAL SHEET SHEETS NO. |
|--|------------------------------|-----------------|-----------|------------------------------|--------|-----------------------------------|--------------|---------------|-------------------------------|
| c:\pw_work\pwidot\bartonrw\d0427922\HM | A-DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | U FE | VAR. | 2015-030RS | DUPAGE 44 9 |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | IL 30 | | | CONTRACT NO. 62A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - | | SCALE: | SHEET 3 OF 29 SHEETS STA. TO STA. | | ILLINOIS FED. | AID PROJECT |

ROUTE: IL 56 (Prospect Avenue to Caldwell Avenue)

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|----------------------|---------------------|-----------|-----------|----------|----------|---------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| Caldwell Avenue | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | 1 | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 3 | 40 | 120 | 13 |
| | | WB | 1 | 3 | 100 | 300 | 33 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 3 | 100 | 300 | 33 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | LT1 | 3 | 50 | 150 | 17 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 3 | 40 | 120 | 13 |
| | 1 | WB | 1 | 3 | 200 | 600 | 67 |
| | 1 | WB | 1 | 3 | 20 | 60 | 7 |
| | + | WB | CL | 12 | 20 | 240 | 27 |
| | 1 | WB | 1 | 3 | 30 | 90 | 10 |
| | | WB | 1 | 3 | 40 | 120 | 13 |
| | Prospect Avenue | WB | 1 | 6 | 30 | 180 | 20 |
| Caldwell Avenue | 1 100poot / ttollao | WB | 2 | 3 | 30 | 90 | 10 |
| Cala Woll 7 Worldo | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 20 | 240 | 27 |
| | | WB | RT | 3 | 40 | 120 | 13 |
| | | WB | 2 | 12 | 30 | 360 | 40 |
| | | WB | 2 | 12 | 30 | 360 | 40 |
| | Prospect Avenue | WB | 2 | 12 | 20 | 240 | 27 |
| Prospect Avenue | T TOOPEST 7 WOTTER | EB | 1 | 3 | 100 | 300 | 33 |
| 1 100poot 7 (tollido | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 3 | 50 | 150 | 17 |
| | | EB | LT1 | 12 | 10 | 120 | 13 |
| | + | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | + | EB | 1 | 3 | 100 | 300 | 33 |
| | + | EB | 1 | 3 | 100 | 300 | 33 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | | EB | 1 | 12 | 20 | 240 | 27 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | + | EB | 1 | 3 | 50 | 150 | 17 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | + | EB | 1 | 3 | 20 | 60 | 7 |
| | + | EB | 1 | 12 | 30 | 360 | 40 |
| | + | EB | 1 | 12 | 5 | 60 | 7 |
| | Caldwell Avenue | EB | 1 | 3 | 30 | 90 | 10 |
| Prospect Avenue | Calawell Averlue | EB | 2 | 12 | 20 | 240 | 27 |
| 1 103 pect Avenue | + | EB | 2 | 3 | 30 | 90 | 10 |
| | + | EB | 2 | 6 | 20 | 120 | 13 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | 1 | EB | RT | 3 | 50 | 150 | 17 |
| | | | | 3 | 50 | 150 | |
| | 1 | EB | 2 | | | | 17 |
| | | EB | 2 | 3 | 20 | 60 | 7 |

ROUTE: IL 56 (Prospect Avenue to Caldwell Avenue)

| CROSS S | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|---------|-----------------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 2 | 6 | 20 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | Caldwell Avenue | EB | 2 | 3 | 30 | 90 | 10 |
| | | | | | | | |

TOTALS: 1810 957 FT SY

(Continued)

ROUTE: IL 56 (Summit Avenue to Commonwealth Lane)

| CROSS : | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------------------|---------------|-----------|-----------|----------|----------|---------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| Commonwealth Lane | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 12 | 144 | 16 |
| | | WB | 2 | 12 | 12 | 144 | 16 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 40 | 480 | 53 |
| | | WB | 2 | 12 | 40 | 480 | 53 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 2 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | Summit Avenue | WB | 3 | 12 | 6 | 72 | 8 |
| Summit Avenue | | EB | 2 | 6 | 6 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMITTENT RESURFACING SCHEDULE | F.A. | SECTION | COUNTY | TOTAL | SHEET NO. |
|--|------------------------------|-----------------|-----------|------------------------------|--------|-----------------------------------|------|------------------|-----------|---------|--------------|
| c:\pw_work\pwidot\bartonrw\d0427922\HM | A-DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | II EE | VAR. | 2015-030RS | DUPAGE | 44 | 10 |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | IL 30 | | | CONTRACT | T NO. 6 | 2A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - | | SCALE: | SHEET 4 OF 29 SHEETS STA. TO STA. | | ILLINOIS FED. AI | D PROJECT | | |

ROUTE: IL 56 (Summit Avenue to Commonwealth Lane) (Continued)

| CROSS : | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|---------|-------------------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 20 | 240 | 27 |
| | | EB | 2 | 12 | 20 | 240 | 27 |
| | | EB | 3 | 12 | 20 | 240 | 27 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 24 | 288 | 32 |
| | Commonwealth Lane | EB | 1 | 12 | 4 | 48 | 5 |
| | | | | | | | |

TOTALS:

ROUTE: IL 38 (Wisconsin Avenue to Harrison Street)

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-----------------|--------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| Harrison Street | | WB | 1 | 24 | 100 | 2400 | 267 |
| | | WB | 2 | 24 | 100 | 2400 | 267 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | Ramp | 18 | 6 | 108 | 12 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | Ramp | 12 | 6 | 72 | 8 |
| | | WB | Ramp | 18 | 5 | 90 | 10 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | I-294 | WB | 2 | 12 | 6 | 72 | 8 |
| I-294 | | WB | 1 | 12 | 8 | 96 | 11 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 3 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 8 | 96 | 11 |
| | | WB | 2 | 12 | 8 | 96 | 11 |
| | | WB | 3 | 12 | 8 | 96 | 11 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 12 | 144 | 16 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 12 | 144 | 16 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 6 | 72 | 8 |

ROUTE: IL 38 (Wisconsin Avenue to Harrison Street) (Continued)

| CROSS STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|--------------|-----------|-----------|----------|----------|---------|---------|
| FROM TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| 11.6.11 | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | WB | 3 | 12 | 5 | 60 | 7 |
| | WB | 1 | 12 | 5 | 60 | 7 |
| | WB | 2 | 12 | 5 | 60 | 7 |
| | WB | 3 | 12 | 8 | 96 | 11 |
| | | | | | | |
| | WB | 1 | 12 | 5 | 60 | 7 |
| | WB | 2 | 12 | 12 | 144 | 16 |
| | WB | 3 | 12 | 12 | 144 | 16 |
| | WB | 1 | 12 | 5 | 60 | 7 |
| | WB | 2 | 12 | 5 | 60 | 7 |
| | WB | 3 | 12 | 5 | 60 | 7 |
| | WB | Ramp | 12 | 5 | 60 | 7 |
| | WB | 1 | 12 | 5 | 60 | 7 |
| | WB | 2 | 12 | 5 | 60 | 7 |
| | WB | 3 | 12 | 5 | 60 | 7 |
| | WB | 1 | 12 | 5 | 60 | 7 |
| | WB | 2 | 12 | 5 | 60 | 7 |
| | WB | 3 | 12 | 12 | 144 | 16 |
| | WB | 1 | 12 | 5 | 60 | 7 |
| | WB | 2 | 12 | 12 | 144 | 16 |
| | WB | 3 | 12 | | 60 | 7 |
| | | | | 5 | | |
| | WB | 1 | 12 | 5 | 60 | 7 |
| | WB | 2 | 12 | 6 | 72 | 8 |
| | WB | 3 | 12 | 5 | 60 | 7 |
| | WB | 1 | 12 | 5 | 60 | 7 |
| | WB | 2 | 12 | 5 | 60 | 7 |
| | WB | 3 | 12 | 6 | 72 | 8 |
| | WB | 1 | 12 | 10 | 120 | 13 |
| | WB | 2 | 12 | 10 | 120 | 13 |
| | WB | Ramp | 18 | 10 | 180 | 20 |
| | WB | Ramp | 18 | 10 | 180 | 20 |
| | WB | 3 | 12 | 12 | 144 | 16 |
| | WB | Ramp | 18 | 6 | 108 | 12 |
| | WB | 1 | 12 | 5 | 60 | 7 |
| | WB | 2 | 12 | 5 | 60 | 7 |
| + | WB | 3 | 12 | 5 | 60 | 7 |
| | WB | | | | 90 | 10 |
| | | Ramp | 18 | 5 | | |
| | WB | 1 | 12 | 5 | 60 | 7 |
| | WB | 2 | 12 | 10 | 120 | 13 |
| | WB | 3 | 12 | 6 | 72 | 8 |
| | WB | Ramp | 18 | 5 | 90 | 10 |
| | WB | 1 | 12 | 5 | 60 | 7 |
| | WB | 2 | 12 | 5 | 60 | 7 |
| | WB | 3 | 12 | 5 | 60 | 7 |
| | WB | Ramp | 18 | 5 | 90 | 10 |
| | WB | 1 | 12 | 5 | 60 | 7 |
| | WB | 2 | 12 | 6 | 72 | 8 |
| | WB | 3 | 12 | 6 | 72 | 8 |
| | WB | 1 | 12 | 5 | 60 | 7 |
| | WB | 2 | 12 | 6 | 72 | 8 |
| | WB | 2 | 12 | 5 | 60 | 7 |
| | WB | 3 | 12 | 5 | 60 | 7 |
| | | | | | | |
| | WB | 1 | 12 | 5 | 60 | 7 |
| | WB | 2 | 12 | 5 | 60 | 7 |
| | WB | 3 | 12 | 5 | 60 | 7 |
| | WB | 3 | 12 | 5 | 60 | 7 |
| | WB | 1 | 12 | 5 | 60 | 7 |

CONTINUED ON NEXT SHEET

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMI | TTENT RESUF | FACING SCH | FDUI F | F.A. RTF. | SECTION | COUNTY | TOTAL SHEET |
|---|------------------------------|-----------------|-----------|------------------------------|--------|---------|-------------|------------|---------|--------------|---------------|-------------|-------------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | | IL 56 / | | | VAR. | 2015-030RS | DUPAGE | 44 11 |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | | IL 30 / | IL JO | | | | CONTRACT | T NO. 62A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - | | SCALE: | SHEET 5 | OF 29 SHE | ETS STA. | TO STA. | | ILLINOIS FED. | AID PROJECT | |

561 SY

| FROM TO (EBAVB) (NO) PATCH PATCH AREA AREA (NBSB) (I, 2, 3) WIDTH LENGTH (SC FT) (SQ YD) (NBSB) (I, 2, 3) WIDTH LENGTH (SC FT) (SQ YD) (NBSB) (I, 2, 3) WIDTH (SC FT) (SQ YD) (SQ YD) (NBSB) (I, 2, 3) WIDTH (SC FT) (SQ YD) (SQ YD) (SQ YD) (NBSB) (I, 2, 3) WIDTH (SC FT) (SQ YD) (S | CROSS | STREET | DIRECTION | LANE | PAVEMENT. | PAVEMENT | REPAIR | REPAIR |
|--|-------------------|-------------------|-----------|------|-----------|----------|--------|--------|
| (NB/SB) | | | | 1 | | | | |
| WB | TROW | | | l | | | | 1 |
| WB | | | | | | | | |
| WB 2 12 5 60 7 | | | | | | | | |
| WB 3 12 5 60 7 | | | | | | | | |
| WB | | | | | | | | |
| WB | | | | | | | | |
| WB 3 112 5 60 7 WB 1 112 5 60 7 WB 2 112 5 60 7 WB 2 112 5 60 7 WB 2 112 5 60 7 WB 3 112 5 60 7 WB 1 112 5 60 7 | | | | | | | | |
| WB | | | | | | | | |
| WB 2 112 5 60 7 WB 3 112 5 60 7 WB 1 112 5 60 7 WB 2 112 5 60 7 WB 3 12 5 60 7 WB 4 12 5 60 7 WB 5 1 12 5 60 7 WB 6 7 WB 7 12 5 60 7 WB 8 1 12 5 60 7 WB 9 1 12 5 60 7 WB 1 12 5 60 7 WB 1 12 5 60 7 WB 2 12 5 60 7 WB 3 12 5 60 7 WB 4 12 5 60 7 WB 5 12 5 60 7 WB 6 7 WB 7 12 5 60 7 WB 8 1 12 5 60 7 WB 9 1 12 5 60 7 WB 1 12 5 60 7 WB 1 12 5 60 7 WB 2 12 5 60 7 WB 3 12 5 60 7 WB 4 12 5 60 7 WB 5 12 5 60 7 WB 6 7 WB 7 12 5 60 7 WB 8 1 12 5 60 7 WB 9 1 12 5 60 7 WB 3 12 5 60 7 WB 4 12 5 60 7 WB 5 12 12 144 16 WB 6 1 12 12 144 16 WB 1 12 5 60 7 WB 3 12 12 144 16 WB 4 12 5 60 7 WB 5 12 12 144 16 WB 6 7 8 WB 7 12 12 144 16 WB 8 8 12 12 144 16 WB 9 1 12 5 60 7 WB 9 1 12 5 60 7 WB 9 1 12 5 60 | | | | | | | | |
| WB 3 12 5 60 7 | | | | | | | | |
| WB | | | WB | 2 | | 5 | 60 | 7 |
| WB | | | WB | 3 | | 5 | 60 | 7 |
| WB | | | WB | 1 | 12 | 5 | 60 | 7 |
| WB 2 12 5 60 7 | | | WB | 2 | | 5 | 60 | 7 |
| WB 3 12 5 60 7 | | | WB | 1 | 12 | 5 | 60 | 7 |
| WB 3 12 5 60 7 | | | WB | 2 | 12 | 5 | 60 | 7 |
| WB | | | | | | 5 | | 7 |
| WB 2 12 5 60 7 | | | | | | | | |
| WB 3 12 5 60 7 | | | | | | | | |
| WB | | | | | | | | |
| WB | | | | | | | | |
| WB | | | | | | | | |
| WB 2 12 5 60 7 | | | | | | | | |
| WB 3 12 5 60 7 | | | | | | | | |
| WB | | | | | | | | |
| WB 2 12 5 60 7 | | | | | | | | |
| WB | | | | | | | | |
| WB 2 12 5 60 7 | | | | | | | | |
| WB 3 12 5 60 7 | | | | | | | | |
| WB | | | | | | | | |
| WB 2 12 5 60 7 WB 3 12 5 60 7 WB 1 12 5 60 7 WB 2 12 5 60 7 WB 3 12 5 60 7 WB 1 12 5 60 7 WB 3 12 5 60 7 WB 3 12 10 120 13 WB 3 12 12 144 16 WB 1 12 5 60 7 WB 1 12 5 60 7 WB 1 12 12 144 16 </td <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> | | | | | | | | |
| WB 3 12 5 60 7 | | | | | | | | |
| WB | | | | | | | | |
| WB 2 12 5 60 7 WB 3 12 5 60 7 WB 1 12 5 60 7 WB 2 12 5 60 7 WB 3 12 10 120 13 WB 3 12 12 144 16 WB 1 12 5 60 7 WB 1 12 5 60 7 WB 3 12 5 60 7 WB 4 12 12 144 16 Salt Creek Bridge WB 2 12 12 144 16 Salt Creek Bridge WB Ramp 12 5 60 7 WB 3 12 12 144 16 WB 3 12 5 60 7 | | | | 3 | | 5 | | 7 |
| WB 3 12 5 60 7 WB 1 12 5 60 7 WB 2 12 5 60 7 WB 3 12 5 60 7 WB 3 12 10 120 13 WB 3 12 10 120 13 WB 3 12 12 144 16 WB 1 12 5 60 7 WB 1 12 5 60 7 WB 1 12 5 60 7 WB 3 12 5 60 7 WB 1 12 12 144 16 Salt Creek Bridge WB 2 12 12 144 16 Salt Creek Bridge WB Ramp 12 5 60 7 WB 3 | | | WB | 1 | 12 | | 60 | 7 |
| WB 1 12 5 60 7 WB 2 12 5 60 7 WB 3 12 5 60 7 WB 3 12 10 120 13 WB 3 12 12 144 16 WB 1 12 5 60 7 WB 1 12 5 60 7 WB 1 12 5 60 7 WB 3 12 5 60 7 WB 3 12 12 144 16 Salt Creek Bridge WB 2 12 12 144 16 Salt Creek Bridge WB Ramp 12 5 60 7 WB 3 12 12 144 16 WB 3 12 12 144 16 WB 3 | | | WB | 2 | | | 60 | 7 |
| WB 2 12 5 60 7 WB 3 12 5 60 7 WB 3 12 10 120 13 WB 3 12 12 144 16 WB 1 12 5 60 7 WB 1 12 5 60 7 WB 1 12 5 60 7 WB 3 12 5 60 7 WB 1 12 5 60 7 WB 3 12 12 144 16 Salt Creek Bridge WB 2 12 12 144 16 Salt Creek Bridge WB Ramp 12 5 60 7 WB 3 12 12 144 16 WB 3 12 12 144 16 WB 3 | | | WB | 3 | 12 | 5 | 60 | 7 |
| WB 3 12 5 60 7 WB 3 12 10 120 13 WB 3 12 12 144 16 WB 1 12 5 60 7 WB 1 12 5 60 7 WB 1 12 5 60 7 WB 3 12 5 60 7 WB 1 12 12 144 16 Salt Creek Bridge WB 2 12 12 144 16 Salt Creek Bridge WB Ramp 12 5 60 7 WB Ramp 12 5 60 7 WB 3 12 12 144 16 WB 3 12 12 144 16 WB 3 12 12 144 16 WB 3< | | | WB | 1 | 12 | 5 | 60 | 7 |
| WB 3 12 5 60 7 WB 3 12 10 120 13 WB 3 12 12 144 16 WB 1 12 5 60 7 WB 1 12 5 60 7 WB 1 12 5 60 7 WB 3 12 5 60 7 WB 1 12 12 144 16 Salt Creek Bridge WB 2 12 12 144 16 Salt Creek Bridge WB Ramp 12 5 60 7 WB Ramp 12 5 60 7 WB 3 12 12 144 16 WB 3 12 12 144 16 WB 3 12 12 144 16 WB 3< | | | WB | 2 | 12 | 5 | 60 | 7 |
| WB 3 12 10 120 13 WB 3 12 12 144 16 WB 1 12 5 60 7 WB 2 12 5 60 7 WB 1 12 5 60 7 WB 3 12 5 60 7 WB 1 12 12 144 16 Salt Creek Bridge WB 2 12 12 144 16 Salt Creek Bridge WB Ramp 12 5 60 7 WB Ramp 12 5 60 7 WB Ramp 12 5 60 7 WB 3 12 12 144 16 WB 3 12 12 144 16 WB 3 12 12 144 16 WB <td< td=""><td></td><td></td><td>WB</td><td>3</td><td>12</td><td>5</td><td>60</td><td>7</td></td<> | | | WB | 3 | 12 | 5 | 60 | 7 |
| WB 3 12 12 144 16 WB 1 12 5 60 7 WB 2 12 5 60 7 WB 1 12 5 60 7 WB 3 12 5 60 7 WB 1 12 12 144 16 Salt Creek Bridge WB 2 12 12 144 16 Salt Creek Bridge WB Ramp 12 5 60 7 WB Ramp 12 5 60 7 WB 3 12 12 144 16 WB <td< td=""><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></td<> | | | | | | | | |
| WB 1 12 5 60 7 WB 2 12 5 60 7 WB 1 12 5 60 7 WB 3 12 5 60 7 WB 1 12 12 144 16 Salt Creek Bridge WB 2 12 12 144 16 Salt Creek Bridge WB Ramp 12 5 60 7 WB Ramp 12 5 60 7 WB Ramp 12 5 60 7 WB 3 12 12 144 16 WB 3 12 5 60 7 WB 3< | | | | | | | | |
| WB 2 12 5 60 7 WB 1 12 5 60 7 WB 3 12 5 60 7 WB 1 12 12 144 16 Salt Creek Bridge WB 2 12 12 144 16 Salt Creek Bridge WB Ramp 12 5 60 7 WB Ramp 12 5 60 7 WB 3 12 12 144 16 WB 3 12 5 60 7 WB 3 12 5 60 72 8 WB | | | | | | | | |
| WB 1 12 5 60 7 WB 3 12 5 60 7 WB 1 12 12 144 16 Salt Creek Bridge WB 2 12 12 144 16 Salt Creek Bridge WB Ramp 12 5 60 7 WB Ramp 12 5 60 7 WB 3 12 12 144 16 WB 2 12 5 60 7 WB 3 12 6 72 8 WB 3 12 5 60 7 WB 3< | | | | | | | | |
| WB 3 12 5 60 7 WB 1 12 12 144 16 Salt Creek Bridge WB 2 12 12 144 16 Salt Creek Bridge WB Ramp 12 5 60 7 WB Ramp 12 5 60 7 WB 3 12 12 144 16 WB 3 12 5 60 7 WB 3 12 5 60 7 WB 3 12 6 72 8 WB 3 12 5 60 7 WB 3< | | | | | | | | |
| WB 1 12 12 144 16 Salt Creek Bridge WB 2 12 12 144 16 Salt Creek Bridge WB Ramp 12 5 60 7 WB Ramp 12 5 60 7 WB 3 12 12 144 16 WB 3 12 12 5 60 7 WB 3 12 6 72 8 WB 3 12 5 60 7 WB 3 12 5 60 7 <th< td=""><td></td><td></td><td></td><td></td><td>12</td><td>5</td><td></td><td></td></th<> | | | | | 12 | 5 | | |
| Salt Creek Bridge WB 2 12 12 144 16 Salt Creek Bridge WB Ramp 12 5 60 7 WB Ramp 12 5 60 7 WB 3 12 12 144 16 WB 3 12 12 144 16 WB 3 12 12 144 16 WB 2 12 5 60 7 WB 3 12 6 72 8 WB 3 12 6 72 8 WB 3 12 5 60 7 WB 1 | | | | | | | | |
| Salt Creek Bridge WB Ramp 12 5 60 7 WB Ramp 12 5 60 7 WB 3 12 12 144 16 WB 3 12 12 144 16 WB 3 12 12 144 16 WB 2 12 5 60 7 WB 3 12 6 72 8 WB 3 12 6 72 8 WB 3 12 5 60 7 WB 3 12 5 60 7 WB 1 12 8 96 11 | | Salt Crook Bridge | | | | | | |
| WB Ramp 12 5 60 7 WB 3 12 12 144 16 WB 3 12 12 144 16 WB 3 12 12 144 16 WB 2 12 5 60 7 WB 3 12 6 72 8 WB 2 12 6 72 8 WB 3 12 5 60 7 WB 3 12 5 60 7 WB 1 12 8 96 11 | Salt Croak Bridge | Jail Greek Bridge | | | | | | |
| WB 3 12 12 144 16 WB 3 12 12 144 16 WB 3 12 12 144 16 WB 2 12 5 60 7 WB 3 12 6 72 8 WB 2 12 6 72 8 WB 3 12 5 60 7 WB 3 12 5 60 7 WB 1 12 8 96 11 | Sait Greek Bridge | | | | | | | |
| WB 3 12 12 144 16 WB 3 12 12 144 16 WB 2 12 5 60 7 WB 3 12 6 72 8 WB 2 12 6 72 8 WB 3 12 5 60 7 WB 3 12 5 60 7 WB 1 12 8 96 11 | | | | | 12 | | | |
| WB 3 12 12 144 16 WB 2 12 5 60 7 WB 3 12 6 72 8 WB 2 12 6 72 8 WB 3 12 5 60 7 WB 3 12 5 60 7 WB 1 12 8 96 11 | | | | | | | | |
| WB 2 12 5 60 7 WB 3 12 6 72 8 WB 2 12 6 72 8 WB 3 12 5 60 7 WB 1 12 8 96 11 | | | | | | | | |
| WB 3 12 6 72 8 WB 2 12 6 72 8 WB 3 12 5 60 7 WB 1 12 8 96 11 | | | | | | | | |
| WB 2 12 6 72 8 WB 3 12 5 60 7 WB 1 12 8 96 11 | | | | 2 | 12 | | 60 | |
| WB 3 12 5 60 7 WB 1 12 8 96 11 | | | | | | | | |
| WB 1 12 8 96 11 | | | | | | | | |
| | | | | 3 | | | | |
| WB 2 12 144 16 | | | | | 12 | | | |
| | | | WB | 2 | 12 | 12 | 144 | 16 |

ROUTE: IL 38 (Wisconsin Avenue to Harrison Street) (Continued)

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|------------------|------------------|-----------|-----------|----------|----------|---------|----------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | WB | 3 | 12 | 12 | 144 | 16 |
| | | WB | 2 | 12 | 12 | 144 | 16 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 3 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 12 | 144 | 16 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | Wisconsin Avenue | WB | 2 | 12 | 5 | 60 | 7 |
| Wisconsin Avenue | | EB | Joint | 3 | 100 | 300 | 33 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 20 | 240 | 27 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 40 | 480 | 53 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | Joint | 3 | 100 | 300 | 33 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 8 | 96 | 11 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | 2 | 12 12 | 5 5 | 60 | 7 |
| | | EB | | | | 60 | |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | |
| | | EB | 1 | 12 | 12 | 144 | 16 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 67 |
| | | EB | Joint | 3 | 200 | 600 | 67 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | Joint | 3 | 200 | 600 | 67 |
| | | EB | 1 | 18 | 5 | 90 | 10 |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMITTENT RESURFACING SCHEDULE | F.A. | SECTION | COUNTY | TOTAL | SHEET NO. |
|---|-------------------------------|-----------------|-----------|------------------------------|--------|-----------------------------------|------|------------------|----------|---------|--------------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | II 20 | VAR. | 2015-030RS | DUPAGE | 44 | 12 |
| | PLOT SCALE = 100.0000 ' / 10. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | IL 38 | | | CONTRACT | T NO. F | 2A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - | | SCALE: | SHEET 6 OF 29 SHEETS STA. TO STA. | | ILLINOIS FED. AI | | | |

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------|--------|-----------|-----------|----------|----------|---------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 2 | 18 | 5 | 90 | 10 |
| | | EB | 1 | 18 | 40 | 720 | 80 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | Joint | 3 | 200 | 600 | 67 |
| | | EB | Ramp | 18 | 5 | 90 | 10 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 20 | 240 | 27 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 20 | 240 | 27 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | Joint | 3 | 50 | 150 | 17 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | Joint | 3 | 30 | 90 | 10 |
| | | EB | Joint | 3 | 200 | 600 | 67 |
| | | EB | 1 | 12 | 15 | 180 | 20 |
| | | EB | 2 | 12 | 20 | 240 | 27 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | Joint | 3 | 100 | 300 | 33 |
| | | EB | Joint | 3 | 30 | 90 | 10 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | Joint | 3 | 20 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | Ramp | 18 | 10 | 180 | 20 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 5 | 60 | 7 |

EB

12

5

60

ROUTE: IL 38 (Wisconsin Avenue to Harrison Street) (Continued)

| CROSS STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|--------------|-----------|-----------|----------|----------|------------|----------|
| FROM TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | EB | 3 | 12 | 5 | 60 | 7 |
| | EB | 2 | 12 | 5 | 60 | 7 |
| | EB | 1 | 12 | 5 | 60 | 7 |
| | EB | 2 | 12 | 5 | 60 | 7 |
| | EB | 1 | 12 | 5 | 60 | 7 |
| | EB | 2 | 12 | 5 | 60 | 7 |
| | EB | 3 | 12 | 10 | 120 | 13 |
| | EB | 2 | 12 | 10 | 120 | 13 |
| | EB | 1 | 12 | 10 | 120 | 13 |
| | EB | 2 | 3 | 20 | 60 | 7 |
| | EB | 3 | 12 | 5 | 60 | 7 |
| | EB | 1 | 12 | 10 | 120 | 13 |
| | EB | 2 | 12 | 12 | 144 | 16 |
| | EB | 3 | 12 | 12 | 144 | 16 |
| | EB | 1 | 12 | 10 | 120 | 13 |
| | EB | 2 | 12 | 10 | 120 | 13 |
| | EB | 3 | 12 | 5 | 60 | 7 |
| | EB | 1 | 12 | 5 | 60 | 7 |
| | EB | 2 | 12 | 5 | 60 | 7 |
| | EB | 3 | 12 | 5 | 60 | 7 |
| | EB | 1 | 12 | 12 | 144 | 16 |
| | EB | 2 | 12 | 5 | 60 | 7 |
| | EB | 3 | 12 | 5 | 60 | 7 |
| | EB | 1 | 12 | 5 | 60 | 7 |
| | EB | 2 | 12 | 5 | 60 | 7 |
| | EB | 3 | 12 | 12 | 144 | 16 |
| | EB | 1 | 12 | 5 | 60 | 7 |
| | EB | | 12 | 5 | | 7 |
| | EB | 3 | 12 | 5 | 60 60 | 7 |
| | EB | 3 | 12 | 5 | 60 | 7 |
| | EB | 3 | 12 | 5 | 60 | 7 |
| | | | | | | 33 |
| | EB | Joint | 3 12 | 100 | 300 | |
| | EB EB | 2 | 12 | 12 12 | 144 144 | 16 16 |
| | | | | | | |
| | EB | 3 | 12 | 12 | 144 | 16 |
| | EB | 1 | 12 | 12 | 144 | 16 |
| | EB | 2 | 12 | 5 | 60 | 7 |
| | EB | 3 | 12 | 5 | 60 | 7 |
| | EB | 1 | 12 | 10 | 120 | 13 |
| | EB | 2 | 12 | 10 | 120 | 13 |
| | EB | 3 | 12 | 10 | 120 | 13 |
| | EB | 1 | 12 | 10 | 120 | 13 |
| | EB | 2 | 12 | 10 | 120 | 13 |
| | EB | 3 | 12 | 10 | 120 | 13 |
| | EB | 3 | 12 | 5 | 60 | 7 |
| | EB | 1 | 12 | 5 | 60 | 7 |
| | EB | 2 | 12 | 5 | 60 | 7 |
| | EB | 3 | 12 | 5 | 60 | 7 |
| | EB | 1 | 12 | 10 | 120 | 13 |
| | EB | 2 | 12 | 5 | 60 | 7 |
| | EB | 3 | 12 | 5 | 60 | 7 |
| | EB | 3 | 12 | 5 | 60 | 7 |
| | EB | 1 | 12 | 10 | 120 | 13 |
| | EB | 3 | 12 | 10 | 120 | 13 |
| | FD | I Dama | 18 | 5 | 90 | 10 |
| | EB EB | Ramp 3 | 12 | 10 | 120 | 13 |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMITTENT RESURFACING SCHEDULE | F.A. | SECTION | COUNTY | TOTAL | SHEET | 1 |
|---|-------------------------------|-----------------|-----------|------------------------------|--------|-----------------------------------|------|------------------|---------|---------|-------|---|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | II 20 | VAR. | 2015-030RS | DUPAGE | 44 | 13 | 1 |
| | PLOT SCALE = 100.0000 ' / 10. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | IL 38 | | | CONTRAC | T NO. F | 62A84 | 1 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - | | SCALE: | SHEET 7 OF 29 SHEETS STA. TO STA. | | ILLINOIS FED. AI | | | | 1 |

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------|--------|-----------|---------------|----------|----------|---------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| I ROW | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | | 18 | 10 | 180 | 20 |
| | | EB | Ramp Joint | 3 | 200 | 600 | 67 |
| | | EB | | 18 | 8 | 144 | 16 |
| | | | Ramp | | | | |
| | | EB | Ramp | 18 | 10 | 180 | 20 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | Joint | 3 | 20 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | Ramp | 18 | 10 | 180 | 20 |
| | | EB | Ramp | 18 | 10 | 180 | 20 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | Ramp | 18 | 10 | 180 | 20 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | Ramp | 18 | 12 | 216 | 24 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | | | | | | |

ROUTE: IL 38 (Wisconsin Avenue to Harrison Street) (Continued)

| CROSS S | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------------|-------------|-----------|-----------|----------|----------|----------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | Joint | 3 | 100 | 300 | 33 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 3 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 15 | 180 | 20 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | Ramp | 20 | 5 | 100 | 11 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | | 20 | 5 | 100 | 11 |
| | | EB | Ramp 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | | | 24 | 5 | 120 | 13 |
| | | | Ramp & 1 | 12 | 5 | | 7 |
| | | EB | 2 | | | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 12 | 5 | 60 | 7 |
| | 1204 D | EB | 2 | | 5 | 60 | 7 |
| 1 204 D | I-294 Ramps | EB | 3 | 12 | 12 | 144 | 16 |
| I-294 Ramps | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | İ | EB | 3 | 18 12 | 5 | 90 | 10 |
| | | | | 1 17 | 5 | 60 | 7 |
| | | EB | 1 | | | | |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB EB | 2 | 12 12 | 5 5 | 60 60 | 7 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMITTENT RESURFACING SCHEDULE | F.A. | SECTION | COUNTY | TOTAL | SHEET | |
|---|-------------------------------|-----------------|-----------|------------------------------|--------|-----------------------------------|------|------------------|---------|---------|-------|---|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | II OO | VAR. | 2015-030RS | DUPAGE | 44 | 14 | + |
| | PLOT SCALE = 100.0000 ' / in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | IL 38 | | | | CONTRAC | T NO. 6 | 2A84 | 1 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - | | SCALE: | SHEET 8 OF 29 SHEETS STA. TO STA. | | ILLINOIS FED. AI | | | | 1 |

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------|-----------------|-----------|-----------|----------|----------|---------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | Ramp | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | LT1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 3 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | Harrison Street | EB | 3 | 12 | 10 | 120 | 13 |
| | | | | | | | |

TOTALS: 4943 5334 FT SY

| FILE NAME = | USER NAME = bartonrw | DESIGNED | - | RWB | REVISED - |
|---|------------------------------|----------|---|----------|-----------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN | - | RWB | REVISED - |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED | - | | REVISED - |
| Default | PLOT DATE = 4/7/2015 | DATE | - | 4/6/2015 | REVISED - |

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

| | INTERMIT | TEN | T RI | SURFA | CING SCH | EDULE | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEE |
|-----------------------------------|---------------|-----|------|---------|----------|-----------------|--------------|---------|--------|-----------------|------|
| | IL 38 / IL 64 | | | | VAR. | 2015-030RS | DUPAGE | 44 | 15 | | |
| IL 30 / IL 04 | | | | | | CONTRACT | NO. 6 | 52A8 | | | |
| SHEET 9 OF 29 SHEETS STA. TO STA. | | | | TO STA. | | ILLINOIS FED. A | ID PROJECT | | | | |

| ROUTE: IL 3 | ام ما ما ما ما | A | Ciplos | Deed | |
|--------------|----------------|-----------|--------|--------|--|
| KOUTE. JIL 3 | o (Highland | Avenue to | rilley | r(uau) | |

| CROSS S | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-----------------|-----------------|-----------|-----------|----------|----------|---------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| Highland Avenue | Highland Avenue | | Joint | 3 | 100 | 300 | 33 |
| | | | Joint | 3 | 200 | 600 | 67 |
| | | | 1 | 12 | 60 | 720 | 80 |
| | | | 2 | 12 | 60 | 720 | 80 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | Finley Road | WB | 2 | 12 | 5 | 60 | 7 |
| Finley Road | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | Highland Avenue | EB | RT | 12 | 60 | 720 | 80 |
| | | | | | | | |

TOTALS: 535 413 FT SY

ROUTE: IL 64 (IL 53 to Swift Road)

SCALE:

| CROSS S | STDEET | DIRECTION | LANE | DAVEMENT. | PAVEMENT | REPAIR | REPAIR |
|------------|------------|-----------|-----------|-----------|----------|---------|---------|
| | | | | | | | |
| FROM | то | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| IL 53 | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 3 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 12 | 144 | 16 |
| | | WB | 2 | 12 | 12 | 144 | 16 |
| | Swift Road | WB | 3 | 12 | 12 | 144 | 16 |
| Swift Road | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 3 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 3 | 12 | 6 | 72 | 8 |
| | | EB | 4 | 12 | 6 | 72 | 8 |
| | | EB | LT1 | 12 | 24 | 288 | 32 |
| | IL 53 | EB | LT2 | 12 | 24 | 288 | 32 |
| | | | | | | · | |

TOTALS: 132 176 FT SY

ROUTE: US 20 (Summerfield Drive to Greenbrook Boulevard)

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|---------------------------------------|-----------------|----------------------------|-----------------------------|--------------------------|-------------------------------|------------------------------------|------------------------------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| Greenbrook Boulevard | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 6 | 12 | 72 | 8 |
| | | EB | 2 | 12 | 13 | 156 | 17 |
| | | EB | 1 | 16 | 14 | 224 | 25 |
| | | | | 20 | 6 | | 13 |
| | | EB | 1 | | | 120 | |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | LT | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 35 | 420 | 47 |
| | | EB | RT | 12 | 10 | 120 | 13 |
| | | EB | 2, RT | 3 | 40 | 120 | 13 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | RT | 12 | 3 | 36 | 4 |
| | Bartels Road | EB | LT | 12 | 3 | 36 | 4 |
| Bartels Road | | EB | INT | 4 | 20 | 80 | 9 |
| Duitelo Hoda | | EB | EOP | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | | | | _ | | |
| | | EB | 1 | 12 | 50 | 600 | 67 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 35 | 420 | 47 |
| | | EB | 1 | 10 | 100 | 1000 | 111 |
| | | EB | 2 | 10 | 6 | 60 | 7 |
| | | EB | 2 | 10 | 3 | 30 | 3 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 3 | 13 | 39 | 4 |
| | | EB | 1 | 12 | 70 | 840 | 93 |
| | | EB | 2 | 12 | 70 | 840 | 93 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | | | | | | |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 90 | 1080 | 120 |
| | | EB | 1 | 12 | 90 | 1080 | 120 |
| | | EB | 1 | 10 | 60 | 600 | 67 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 20 | 3 | 60 | 7 |
| · · · · · · · · · · · · · · · · · · · | | EB | LT | 12 | 10 | 120 | 13 |
| | | EB | 2 | 3 | 40 | 120 | 13 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 200 | 2400 | 267 |
| | | | 1 | 6 | 10 | 60 | 7 |
| | | <u>F</u> B | | | , | | |
| | | EB FB | | 12 | 3 | | |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB EB | 1 | 3 | 20 | 36 60 | 7 |
| | | EB EB EB | 1 1 2 | 3 6 | 20 30 | 36 60 180 | 4 7 20 |
| | | EB EB EB | 1 1 2 LT | 3 6 12 | 20 30 3 | 36 60 180 36 | 4 7 20 4 |
| | Cloverdale Road | EB EB EB EB | 1 1 2 LT 1 | 3 6 12 12 | 20 30 3 3 | 36 60 180 36 36 | 4 7 20 4 4 |
| Cloverdale Road | Cloverdale Road | EB EB EB EB EB | 1 1 2 LT 1 2 | 3 6 12 12 12 | 20 30 3 3 3 20 | 36 60 180 36 36 240 | 4 7 20 4 4 27 |
| Cloverdale Road | Cloverdale Road | EB EB EB EB | 1 1 2 LT 1 | 3 6 12 12 | 20 30 3 3 | 36 60 180 36 36 | 4 7 20 4 4 |

ROUTE: US 20 (Summerfield Drive to Greenbrook Boulevard) (Continued)

| ROUTE | : US 20 (Summerfield Driv | e to Greenbrook | Boulevard | d) | (Continued) | | |
|---------------|---------------------------|-----------------|-----------|----------|-------------|---------|---------|
| CPOSS | STREET | DIRECTION | LANE | DAVEMENT | PAVEMENT | REPAIR | REPAIR |
| | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| FROM | 10 | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | + | EB | 1 | 6 | 20 | 120 | 13 |
| | + | EB | 1 | 6 | 3 | 18 | 2 |
| | + | EB | 1 | 12 | 6 | 72 | 8 |
| | + | EB | 2 | 12 | 60 | 720 | 80 |
| | + | EB | 1 | 12 | 10 | 120 | 13 |
| | + | EB | 1 | 12 | 10 | 120 | 13 |
| | | | | | | | |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 3 | 36 | 4 50 |
| | | EB | 2 | 12 | 40 | 480 | 53 |
| | | EB | 2 | 12 | 60 | 720 | 80 |
| | Thorn Road | EB | 2 | 12 | 6 | 72 | 8 |
| Thorn Road | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | EOP | 3 | 80 | 240 | 27 |
| | | EB | 2 | 3 | 10 | 30 | 3 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 3 | 10 | 30 | 3 |
| | + | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | + | EB | LT | 12 | 3 | 36 | 4 |
| | | | | | | | |
| | <u> </u> | EB | 1 | 12 | 3 | 36 | 4 |
| | 1 | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | RT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 6 | 6 | 36 | 4 |
| | | EB | 1 | 6 | 6 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | RT | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | Gary Avenue | EB | 2 | 12 | 10 | 120 | 13 |
| Gary Avenue | | EB | EOP | 3 | 60 | 180 | 20 |
| • | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 8 | 96 | 11 |
| | Virginia Road | EB | 2 | 12 | 8 | 96 | 11 |
| Virginia Dood | Viigiilla Roau | | | 12 | 35 | 420 | 47 |
| Virginia Road | | EB | 1 | 12 | | | |
| | | EB | 1 | | 14 | 168 | 19 |
| | 1/ 5 1 | EB | 2 | 12 | 3 | 36 | 4 |
| | Keeney Road | EB | Median | 12 | 6 | 72 | 8 |
| Keeney Road | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 20 | 3 | 60 | 7 |
| | | EB | 2 | 12 | 13 | 156 | 17 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 20 | 6 | 120 | 13 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | 1 | - FD | 1 | 20 | 2 | 60 | 7 |

CONTINUED ON NEXT SHEET

60

60

60

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | |
|---|-------------------------------|-----------------|-----------|------------------------------|------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | 1 |
| | PLOT SCALE = 100.0000 ' / in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | 1 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - | | SCAL |

| | INTERMIT | TENT | ΓR | ESURFAC | CING SC | HEDULE |
|--------|----------|------|----|---------|---------|---------|
| | | | | US 20 | | |
| SCALE: | SHEET 10 | OF | 29 | SHEETS | STA. | TO STA. |
| | | | | | | |

EB

EB

EB

20

12

12

5

5

LT

| | | THE INDIC FED. A | ID DDO IECT | | |
|---|--------------|------------------|-------------|--------|--------------|
| _ | | | CONTRACT | NO. 6 | 2A84 |
| | VAR. | 2015-030RS | DUPAGE | 44 | 16 |
| | F.A. RTE. | SECTION | COUNTY | SHEETS | SHEET NO. |

ROUTE: US 20 (Summerfield Drive to Greenbrook Boulevard) (Continued)

| CROSS | CROSS STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|------------------|--|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| THOM | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | | 12 | | 36 | |
| | | | 2 | | 3 | | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | Bryn Mawr Avenue | EB | 2 | 12 | 3 | 36 | 4 |
| Bryn Mawr Avenue | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | EOP | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | 2 | 12 | 9 | 108 | 12 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 20 | 3 | 60 | 7 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 30 | 360 | 40 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 6 | 65 | 390 | 43 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Danworth Street | | EOP | | | | |
| D (1.0) | Papworth Street | EB | | 3 | 80 | 240 | 27 |
| Papworth Street | | EB | EOP | 3 | 90 | 270 | 30 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 8 | 96 | 11 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 15 | 180 | 20 |
| | | EB | 2 | 2 | 3 | 6 | 1 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Wheaton Road | EB | 1 | 6 | 30 | 180 | 20 |
| Wheaton Road | | EB | 1 | 3 | 10 | 30 | 3 |
| TTIOULOIT I LOUG | | EB | 1 | 12 | 25 | 300 | 33 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | | | | | | |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 35 | 105 | 12 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 3 | 10 | 30 | 3 |
| | | EB | 1, 2 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 10 | 30 | 3 |
| | | EB | 1 1 | 12 | 3 | 36 | 4 |
| | | | _ ' | 14 | | | |

ROUTE: US 20 (Summerfield Drive to Greenbrook Boulevard) (Continued)

| CROSS S | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------------------|----------------|----------------|-----------|----------|----------|-----------|----------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 52 | 156 | 17 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | EOP | 3 | 27 | 81 | 9 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 20 | 3 | 60 | 7 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | Rodenburg Road | EB | LT | 20 | 3 | 60 | 7 |
| Rodenburg Road | rodenburg road | EB | 1, 2 | 3 | 80 | 240 | 27 |
| Troderiburg Trodu | | EB | 2 | 6 | 12 | 72 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| - | | EB | 1, 2 | 3 | 70 | 210 | 23 |
| | | EB | EOP | 3 | 70 | 210 | 23 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 20 | 240 | 27 |
| | | | EOP | | 65 | 390 | |
| | | EB EB | EOP | 6 3 | 60 | 180 | 43 20 |
| | | | | | | | |
| | | EB | 1, 2 | 3 | 80 | 240 | 27 |
| | | EB | 1, 2 | 3 | 35 | 105 | 12 |
| | | EB | 1, 2 | 3 | 30 | 90 | 10 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | Garden Avenue | EB | 2 | 12 | 6 | 72 | 8 |
| Garden Avenue | | EB | EOP | 3 | 70 | 210 | 23 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 40 | 120 | 13 |
| | | EB | 1, Median | 3 | 40 | 120 | 13 |
| | | EB | 2 | 6 | 20 | 120 | 13 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | | | | | | |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | | 1 EOP | 12 3 | 3 140 | 36 420 | 4 47 |
| | | EB | | | | | |
| | | EB EB EB | EOP | 3 | 140 | 420 96 | 47 |
| | | EB EB | EOP 1 | 3 12 | 140 8 | 420 | 47 11 |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - KWB | KENIZED - | | |
|---|------------------------------|-----------------|-----------|------------------------------|--------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - | | SCALE: |

| | INTERMITTENT RESURFACING SCHEDULE | | | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEE NO |
|--|-----------------------------------|------|---|--------|------|---------|--------------|----------------|------------|-----------------|------------|
| | | | | | | | VAR. | 2015-030RS | DUPAGE | 44 | 17 |
| | | | | 3 20 | | | | | CONTRACT | NO. | 62A8 |
| | SHEET 11 | OF 2 | 9 | SHEETS | STA. | TO STA. | | TILINOIS EED A | ID PROJECT | | |

ROUTE: US 20 (Summerfield Drive to Greenbrook Boulevard) (Continued)

| CROSS | CROSS STREET | | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------------------|----------------------|---------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 1, 2 | 3 | 17 | 51 | 6 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | EOP | 3 | 80 | 240 | 27 |
| | | EB | 2 | 6 | 65 | 390 | 43 |
| | | EB | 2 | 10 | 6 | 60 | 7 |
| | | EB | Median | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Summerfield Drive | EB | 2 | 12 | 3 | 36 | 4 |
| Summerfield Drive | | WB | 1, 2 | 3 | 130 | 390 | 43 |
| | | WB | 1 | 12 | 40 | 480 | 53 |
| | | WB | 2 | 12 | 50 | 600 | 67 |
| | | WB | 1 | 15 | 110 | 1650 | 183 |
| | | WB | 1, 2 | 3 | 30 | 90 | 10 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1, 2 | 3 | 15 | 45 | 5 |
| | | WB | 1, 2 | 3 | 130 | 390 | 43 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 20 | 240 | 27 |
| | | WB | 1 | 6 | 3 | 18 | 2 |
| | | WB | 1 | 6 | 3 | 18 | 2 |
| | | WB | 1, 2 | 3 | 35 | 105 | 12 |
| | | WB | 2 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | Garden Avenue | WB | 1, 2 | 3 | 25 | 75 | 8 |
| Garden Avenue | | WB | 1, 2 | 3 | 120 | 360 | 40 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | Greenbrook Boulevard | WB | 1, 2 | 3 | 50 | 150 | 17 |
| | | | | | | | |
| | | | | | | | |

TOTALS: 4584 3893 FT SY

ROUTE: IL 19 (Prospect Avenue to York Road)

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-----------|--------|-----------|-----------|----------|----------|---------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| York Road | | WB | 2 | 22 | 15 | 330 | 37 |
| | | WB | 2 | 5 | 5 | 25 | 3 |
| | | WB | 1, 2 | 3 | 12 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |

ROUTE: IL 19 (Prospect Avenue to York Road) (Continued)

| CROSS STF | REET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-----------|------|-----------|-----------|----------|----------|-----------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | WB | 2 | 3 | 15 | 45 | 5 |
| | | WB | 1, 2 | 3 | 5 | 15 | 2 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 57 | 171 | 19 |
| | | WB | 2 | 3 | 27 | 81 | 9 |
| | | WB | 1,Median | 3 | 90 | 270 | 30 |
| | | WB | 2 | 3 | 8 | 24 | 3 |
| | | WB | 1, 2 | 3 | 110 | 330 | 37 |
| | | WB | 1, Median | 3 | 25 | 75 | 8 |
| | | WB | 1 | 15 | 3 | 45 | 5 |
| | | WB | 1, Median | 15 | 3 | 45 | 5 |
| | | WB | 1, 2 | 3 | 35 | 105 | 12 |
| | | WB | EOP | 3 | 25 | 75 | 8 |
| | | WB | EOP | 3 | 10 | 30 | 3 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | EOP | 3 | 30 | 90 | 10 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1, 2 | 3 | 10 | 30 | 3 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 10 | 28 | 280 | 31 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 2 | 5 | 5 | 25 | 3 |
| | | WB | EOP | 3 | 15 | 45 | 5 |
| | | WB | 1, 2 | 3 | 26 | 78 | 9 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 6 | 5 | 30 | 3 |
| | | WB | 1, 2 | 3 | 75 75 | 225 | 25 |
| | | WB WB | 1, 2 1 | 3 12 | 75 3 | 225 36 | 25 4 |
| | | WB | 1, 2 | 3 | 89 | 267 | 30 |
| | | WB | EOP | 3 | 20 | 60 | 7 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 12 | 7 | 84 | 9 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 10 | 3 | 30 | 3 |
| | | WB | 2 | 10 | 3 | 30 | 3 |
| | | WB | 2 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 1, 2 | 3 | 130 | 390 | 43 |
| | | WB | 2 | 12 | 8 | 96 | 11 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | | | | | | |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMIT | TENT RE | SURFAC | ING SCHED | ULF | F.A. RTF | SECTION | COUNTY | TOTAL S | SHEET NO. |
|---|-------------------------------|-----------------|-----------|------------------------------|--------|-------------|---------|-----------|-----------|---------|-------------|---------------|-------------|----------|--------------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | 1141 - 1111 | | | | OLL | VAR. | 2015-030RS | DUPAGE | 44 | 18 |
| | PLOT SCALE = 100.0000 ' / in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | | U3 A | 20 / IL 1 | 9 | | | | CONTRACT | T NO. 62 | A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - | | SCALE: | SHEET 12 | OF 29 | SHEETS | STA. | TO STA. | | ILLINOIS FED. | AID PROJECT | | - |

| CROSS S | TREET | DIRECTION | LANE | PA\/EMENT | PAVEMENT | REPAIR | REPAIR |
|-------------|-------------|--|--|--|---|--|--|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| FROW | 10 | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | EOP | 3 | 65 | 195 | 22 |
| | | WB | 1 | 12 | 8 | 96 | 11 |
| | | WB | 2 | 6 | 12 | 72 | 8 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 8 | 20 | 160 | 18 |
| | | WB | 1, 2 | 3 | 10 | 30 | |
| | | WB | 2 | | | 36 | 3 4 |
| | | WB | | 6 | 6 | | |
| | | | 1, 2 | 3 | 50 | 150 | 17 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 12 | 7 | 84 | 9 |
| | | WB | 2 | 12 3 | 13 | 156 | 17 |
| | | WB | 1, 2 | | 16 | 48 | 5 |
| | | WB | 1 | 12 | 15 | 180 | 20 |
| | | WB | 1, 2 | 3 | 22 | 66 | 7 |
| | | WB | 2 | 12 | 14 | 168 | 19 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 10 | 120 | 13 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | EOP | 4 | 30 | 120 | 13 |
| | | WB | 1 | 6 | 10 | 60 | 7 |
| | | \A/D | 1 4 2 | 1 2 | 20 | | |
| | Church Dood | WB | 1, 2 | 3 | 30 | 90 | 10 |
| Church Bood | Church Road | WB | EOP | 6 | 10 | 60 | 7 |
| Church Road | Church Road | WB WB | EOP INT | 6 3 | 10 80 | 60 240 | 7 27 |
| Church Road | Church Road | WB WB WB | EOP INT INT | 6 3 3 | 10 80 40 | 60 240 120 | 7 27 13 |
| Church Road | Church Road | WB WB WB | EOP INT INT INT | 6 3 3 12 | 10 80 40 12 | 60 240 120 144 | 7 27 13 16 |
| Church Road | Church Road | WB WB WB WB | EOP INT INT INT | 6 3 3 12 12 | 10 80 40 12 12 | 60 240 120 144 144 | 7 27 13 16 16 |
| Church Road | Church Road | WB WB WB WB WB WB | EOP INT INT INT INT | 6 3 3 12 12 12 | 10 80 40 12 12 12 | 60 240 120 144 144 144 | 7 27 13 16 16 |
| Church Road | Church Road | WB WB WB WB WB WB WB | EOP INT INT INT INT INT INT 2 | 6 3 3 12 12 12 12 | 10 80 40 12 12 12 4 | 60 240 120 144 144 144 48 | 7 27 13 16 16 16 5 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB | EOP INT INT INT INT INT Z CL | 6 3 3 12 12 12 12 12 3 | 10 80 40 12 12 12 4 12 | 60 240 120 144 144 144 48 36 | 7 27 13 16 16 16 5 4 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB | EOP INT INT INT INT INT CL CL | 6 3 3 12 12 12 12 12 3 3 | 10 80 40 12 12 12 4 12 90 | 60 240 120 144 144 144 48 36 270 | 7 27 13 16 16 16 5 4 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB | EOP INT INT INT INT INT CL CL 1, 2 | 6 3 3 12 12 12 12 12 3 3 3 | 10 80 40 12 12 12 4 12 90 | 60 240 120 144 144 144 48 36 270 270 | 7 27 13 16 16 16 5 4 30 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB | EOP INT INT INT INT INT CL CL 1, 2 | 6 3 3 12 12 12 12 12 3 3 3 | 10 80 40 12 12 12 4 12 90 90 3 | 60 240 120 144 144 144 48 36 270 270 36 | 7 27 13 16 16 16 5 4 30 30 4 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB | EOP INT INT INT INT 2 CL CL 1, 2 1 | 6 3 3 12 12 12 12 12 3 3 3 12 | 10 80 40 12 12 12 12 4 12 90 90 3 | 60 240 120 144 144 144 48 36 270 270 270 36 216 | 7 27 13 16 16 16 5 4 30 30 4 24 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB WB W | EOP INT INT INT INT 2 CL CL 1, 2 1 Median | 6 3 3 12 12 12 12 12 3 3 3 12 12 12 | 10 80 40 12 12 12 12 4 12 90 90 3 18 38 | 60 240 120 144 144 144 48 36 270 270 270 36 216 | 7 27 13 16 16 16 16 5 4 30 30 4 24 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB WB W | EOP INT INT INT INT 2 CL CL 1, 2 1 Median 2 | 6 3 3 12 12 12 12 12 3 3 3 12 12 12 12 12 12 12 12 12 12 | 10 80 40 12 12 12 12 4 12 90 90 3 18 38 8 | 60 240 120 144 144 144 48 36 270 270 270 36 216 114 | 7 27 13 16 16 16 16 5 4 30 30 4 24 13 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB WB W | EOP INT INT INT INT 2 CL CL 1, 2 1 Median | 6 3 3 12 12 12 12 12 3 3 3 12 12 3 12 12 3 | 10 80 40 12 12 12 12 4 12 90 90 3 18 38 8 | 60 240 120 144 144 144 48 36 270 270 270 36 216 114 96 180 | 7 27 13 16 16 16 5 4 30 30 4 24 13 11 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB WB W | EOP INT INT INT INT 2 CL CL 1, 2 1 Median 2 1, 2 1 | 6 3 3 12 12 12 12 12 3 3 3 12 12 3 12 12 3 12 | 10 80 40 12 12 12 12 4 12 90 90 3 18 38 8 60 3 | 60 240 120 144 144 144 48 36 270 270 270 36 216 114 96 180 36 | 7 27 13 16 16 16 5 4 30 30 4 24 13 11 20 4 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB WB W | EOP INT INT INT INT 2 CL CL 1, 2 1 Median 2 1, 2 1 2 | 6 3 3 12 12 12 12 12 3 3 3 12 12 3 12 12 3 | 10 80 40 12 12 12 12 4 12 90 90 3 18 38 8 | 60 240 120 144 144 144 48 36 270 270 270 36 216 114 96 180 | 7 27 13 16 16 16 5 4 30 30 4 24 13 11 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB WB W | EOP INT INT INT INT 2 CL CL 1, 2 1 Median 2 1, 2 2 2 | 6 3 3 12 12 12 12 12 3 3 12 12 3 12 3 12 3 12 12 3 12 12 3 12 12 12 12 12 12 12 12 12 12 | 10 80 40 12 12 12 12 4 12 90 90 3 18 38 8 60 3 13 3 | 60 240 120 144 144 144 48 36 270 270 36 216 114 96 180 36 39 | 7 27 13 16 16 16 16 5 4 30 30 4 24 13 11 20 4 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB WB W | EOP INT INT INT INT 2 CL CL 1, 2 1 Median 2 1, 2 1 2 | 6 3 3 12 12 12 12 12 3 3 3 12 12 3 12 3 12 3 12 3 12 3 3 12 12 3 3 3 12 12 3 3 3 12 12 3 3 3 4 4 5 5 6 7 8 8 8 8 8 8 8 8 8 8 8 8 8 | 10 80 40 12 12 12 12 4 12 90 90 3 18 38 8 60 3 13 | 60 240 120 144 144 144 48 36 270 270 36 216 114 96 180 36 39 | 7 27 13 16 16 16 5 4 30 30 4 24 13 11 20 4 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB WB W | EOP INT INT INT INT 2 CL CL 1, 2 1 Median 2 1, 2 2 2 | 6 3 3 12 12 12 12 12 3 3 12 12 3 12 3 12 3 12 12 3 10 10 10 10 10 10 10 10 10 10 | 10 80 40 12 12 12 12 4 12 90 90 3 18 38 8 60 3 13 3 | 60 240 120 144 144 144 48 36 270 270 36 216 114 96 180 36 39 30 | 7 27 13 16 16 16 5 4 30 30 4 24 13 11 20 4 4 3 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB WB W | EOP INT INT INT INT INT 2 CL CL 1, 2 1 Median 2 1, 2 2 2 1 | 6 3 3 12 12 12 12 12 12 3 3 12 12 3 12 3 | 10 80 40 12 12 12 12 4 12 90 90 3 18 38 8 60 3 13 3 3 | 60 240 120 144 144 144 48 36 270 270 36 216 114 96 180 36 39 30 | 7 27 13 16 16 16 16 5 4 30 30 4 24 13 11 20 4 4 3 3 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB WB W | EOP INT INT INT INT 2 CL CL 1, 2 1 Median 2 1, 2 2 2 1 | 6 3 3 12 12 12 12 12 3 3 12 12 3 12 3 10 10 10 12 12 6 6 | 10 80 40 12 12 12 12 4 12 90 90 3 18 38 8 60 3 13 3 3 10 10 | 60 240 120 144 144 144 48 36 270 270 270 36 216 114 96 180 36 39 30 30 120 120 180 36 | 7 27 13 16 16 16 16 5 4 30 30 4 24 13 11 20 4 4 3 3 13 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB WB W | EOP INT INT INT INT INT 2 CL CL 1, 2 1 Median 2 1, 2 2 2 1 2 2 | 6 3 3 12 12 12 12 12 3 3 12 12 3 12 12 3 12 3 12 3 12 3 12 12 6 6 6 12 | 10 80 40 12 12 12 14 12 90 90 3 18 38 8 60 3 13 3 3 10 10 30 6 3 | 60 240 120 144 144 144 48 36 270 270 270 36 216 114 96 180 36 39 30 30 120 120 180 36 36 36 36 37 36 36 37 36 37 36 37 38 38 38 38 38 38 38 38 38 38 | 7 27 13 16 16 16 16 5 4 30 30 4 24 13 11 20 4 4 3 3 13 13 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB WB W | EOP INT INT INT INT INT 2 CL CL 1, 2 1 Median 2 1, 2 2 2 1 2 EOP | 6 3 3 12 12 12 12 12 3 3 12 12 3 10 10 10 12 12 6 6 12 12 | 10 80 40 12 12 12 14 12 90 90 3 18 38 8 60 3 13 3 3 10 10 30 6 | 60 240 120 144 144 144 148 36 270 270 36 216 114 96 180 36 39 30 120 120 180 36 36 36 | 7 27 13 16 16 16 16 5 4 30 30 4 24 13 11 20 4 3 3 13 13 20 4 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB WB W | EOP INT INT INT INT INT 2 CL CL 1, 2 1 Median 2 1, 2 2 2 1 2 EOP 1 | 6 3 3 12 12 12 12 12 3 3 3 12 12 3 10 10 10 12 12 6 6 12 12 12 12 | 10 80 40 12 12 12 12 4 12 90 90 3 18 38 8 60 3 13 3 3 10 10 30 6 3 3 3 | 60 240 120 144 144 144 148 36 270 270 36 216 114 96 180 36 39 30 120 120 180 36 36 36 36 | 7 27 13 16 16 16 16 5 4 30 30 4 24 13 11 20 4 3 3 13 13 20 4 4 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB WB W | EOP INT INT INT INT INT 2 CL CL 1, 2 1 Median 2 1, 2 2 2 1 1 2 EOP 1 1 1 1 | 6 3 3 12 12 12 12 12 12 3 3 12 12 3 10 10 10 12 12 6 6 12 12 12 12 12 12 | 10 80 40 12 12 12 12 4 12 90 90 3 18 38 8 60 3 13 3 3 10 10 30 6 3 3 3 3 | 60 240 120 144 144 144 148 36 270 270 36 216 114 96 180 36 39 30 120 120 180 36 36 36 36 | 7 27 13 16 16 16 16 5 4 30 30 4 24 13 11 20 4 4 3 3 13 13 20 4 4 4 |
| Church Road | Church Road | WB WB WB WB WB WB WB WB WB WB WB WB WB W | EOP INT INT INT INT INT 2 CL CL 1, 2 1 Median 2 1, 2 2 2 1 2 EOP 1 1 1 | 6 3 3 12 12 12 12 12 3 3 3 12 12 3 10 10 10 12 12 6 6 12 12 12 12 | 10 80 40 12 12 12 12 4 12 90 90 3 18 38 8 60 3 13 3 3 10 10 30 6 3 3 3 | 60 240 120 144 144 144 148 36 270 270 36 216 114 96 180 36 39 30 120 120 180 36 36 36 36 | 7 27 13 16 16 16 16 5 4 30 30 4 24 13 11 20 4 4 3 3 13 13 20 4 4 4 4 |

| ROUTE: | II 19 | (Prospect Avenue to York Road |) | (Continued) | |
|--------|-----------|------------------------------------|---|-------------|--|
| | I - I - I | i reopeot / tveride to r ont rtedd | , | Continuaca | |

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-----------------|-----------------|-----------|-------------------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | WB | 2 | 12 | 10 | 120 | 13 |
| | | WB | Median | 3 | 12 | 36 | 4 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | 1 | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | 1 | | | | | | |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 60 | 180 | 20 |
| | | WB | 1 | 12 | 8 | 96 | 11 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | EOP | 3 | 18 | 54 | 6 |
| | | WB | 2 | 12 | 9 | 108 | 12 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | | 12 | | 72 | |
| | | | 2 | | 6 | | 8 |
| | Eastview Avenue | WB | 2 | 12 | 40 | 480 | 53 |
| Eastview Avenue | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 30 | 90 | 10 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | 1 | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | 1 | WB | 2 | 12 | 3 | 36 | 4 |
| | + | | | | | | |
| | | WB | 1, 2 | 3 | 60 | 180 | 20 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 25 | 75 | 8 |
| | | WB | EOP | 6 | 23 | 138 | 15 |
| | | WB | 1, 2 | 3 | 100 | 300 | 33 |
| | | WB | EOP | 8 | 8 | 64 | 7 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | Franzen Street | WB | EOP | 3 | 30 | 90 | 10 |
| Franzen Street | | WB | 1, 2 | 3 | 40 | 120 | 13 |
| . ranzon otroct | | WB | EOP | 3 | 13 | 39 | 4 |
| | | WB | 1, 2 | 3 | 35 | 105 | 12 |
| | | WB | 1, 2 | 12 | 3 | 36 | 4 |
| | | | | | | | |
| | | WB | 1, 2 | 3 | 8 | 24 | 3 |
| | | WB | 1 | 12 | 8 | 96 | 11 |
| | 1 | WB | 2 | 12 | 3 | 36 | 4 |
| | Marshall Road | WB | 1 | 12 | 3 | 36 | 4 |
| Marshall Road | | WB | INT | 6 | 20 | 120 | 13 |
| | | WB | INT | 3 | 50 | 150 | 17 |
| | | WB | 1, 2 | 3 | 90 | 270 | 30 |
| | | WB | 2 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 3 | 30 | 90 | 10 |
| | | | | | | 120 | 13 |
| | | W/R | l1 Median | 1 3 | 1 40 | 170 | |
| | | WB WB | 1, Median 1, 2 | 3 | 40 60 | 180 | 20 |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMITTENT RESURFACING SCHEDULE | F.A. RTF | SECTION | COUNTY | TOTAL | SHEET NO. |
|---|------------------------------|-----------------|-----------|------------------------------|--------|------------------------------------|-------------|------------------|----------|---------|--------------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | II 40 | VAR. | 2015-030RS | DUPAGE | 44 | 19 |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | IL 19 | | | CONTRACT | T NO. F | 2A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - | | SCALE: | SHEET 13 OF 29 SHEETS STA. TO STA. | | ILLINOIS FED. AI | | | |

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-----------------|-------------------|-----------|--------------|----------|-----------|---------|----------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | WB | 1, Median | 3 | 20 | 60 | 7 |
| | | WB | 1, 2 | 3 | 35 | 105 | 12 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 6 | 3 | 18 | 2 |
| | | WB | 2 | 6 | 3 | 18 | 2 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 6 | 3 | 18 | 2 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | LT | 15 | 3 | 45 | 5 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | Spruce Avenue | WB | 1 | 12 | 3 | 36 | 4 |
| Spruce Avenue | ' | WB | INT | 6 | 6 | 36 | 4 |
| | | WB | INT | 6 | 110 | 660 | 73 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | | WB | EOP | 3 | 15 | 45 | 5 |
| | | WB | 2 | 12 | 8 | 96 | 11 |
| | | WB | EOP | 3 | 12 | 36 | 4 |
| | | WB | 1, 2 | 3 | 100 | 300 | 33 |
| | Pine Lane | WB | 1, Median | 3 | 60 | 180 | 20 |
| Pine Lane | T IIIC Edito | WB | 1, 2 | 3 | 120 | 360 | 40 |
| T IIIC LUTIC | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | | WB | EOP | 3 | 20 | 60 | 7 |
| | Edgewood Avenue | WB | 1, 2 | 3 | 25 | 75 | 8 |
| Edgewood Avenue | Lagewood / Werlac | WB | 1, 2 | 3 | 130 | 390 | 43 |
| Lugewood Avenue | | WB | EOP | 4 | 6 | 24 | 3 |
| | | WB | 2 | 6 | 3 | 18 | 2 |
| | | WB | 1, 2 | 3 | 105 | 315 | 35 |
| | | WB | 1, 2 | 3 | 50 | 150 | 17 |
| | Ash Avenue | WB | 1, 2 | 6 | 50 | 300 | 33 |
| Ash Avenue | Asii Aveilue | WB | INT | 6 | 6 | 36 | 4 |
| ASII Aveilue | | WB | EOP | 3 | 70 | 210 | 23 |
| | | WB | 1, 2 | 3 | 90 | 270 | 30 |
| | | WB | 1, 2 | 12 | 17 | 204 | 23 |
| | | WB | 1, 2 | 3 | 22 | 66 | 7 |
| | Hemlock Avenue | WB | 1, 2 | 3 | 90 | 270 | 30 |
| Hemlock Avenue | Heimook Avenue | WB | EOP | 3 | 60 | 180 | 20 |
| Henniock Avenue | | WB | | 3 | 30 | 90 | 10 |
| | | WB | 1, 2 1, 2 | 3 | 70 | 210 | 23 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | Control Avenue | WB | 1, 2 | 3 | 87 | 261 | 29 |
| Control Assess | Central Avenue | | I, ∠ EOP | | | | |
| Central Avenue | | WB | | 3 | 87 100 | 261 | 29 33 |
| | | WB | 1, 2 | 3 12 | 100 | 300 | 4 |
| | | WB | LT | | 3 | 36 | |
| | Cotolo - A | WB | 1, 2 | 3 | 30 | 90 | 10 |
| Ostalia - A | Catalpa Avenue | WB | LT | 12 | 3 | 36 | 4 |
| Catalpa Avenue | | WB | 1, 2 | 3 | 40 | 120 | 13 |
| | | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | Maple Avenue | WB | 1, 2 | 3 | 25 | 75 | 8 |
| Maple Avenue | | WB | 1, Median | 6 | 20 | 120 | 13 |
| | <u> </u> | WB | 1, 2 | 3 | 20 | 60 | 7 |
| | Cedar Avenue | WB | 1 | 12 | 35 | 420 | 47 |
| | | | | | | | |

| ROUTE: | IL 19 (Prospect Avenue to York Road) | (Continued) |
|--------|--------------------------------------|-------------|
| | | |

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|--------------------|--------------------|-----------|-----------|----------|----------|----------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| Cedar Avenue | | WB | 1, 2 | 3 | 30 | 90 | 10 |
| Ocaal / Wellac | | WB | 1, 2 | 3 | 27 | 81 | 9 |
| | | WB | 2 | 12 | 20 | 240 | 27 |
| | Oak Avenue | WB | 1, 2 | 3 | 40 | 120 | 13 |
| Oak Avenue | Cuit / (Veride | WB | Median | 4 | 15 | 60 | 7 |
| Oak Avenue | Elmwood Avenue | WB | Median | 15 | 3 | 45 | 5 |
| Elmwood Avenue | Lilliwood Aveilde | WB | 1, 2 | 3 | 15 | 45 | 5 |
| Elliwood Avenue | | WB | EOP | 3 | 58 | 174 | 19 |
| | | WB | EOP | | 30 | 180 | |
| | | | | 6 | | | 20 |
| | | WB | EOP | 3 | 50 | 150 | 17 |
| | 1 10 10 1 | WB | LT | 12 | 8 | 96 | 11 |
| | Wood Dale Road | WB | 1 | 12 | 25 | 300 | 33 |
| Wood Dale Road | | WB | EOP | 3 | 80 | 240 | 27 |
| | | WB | 2 | 8 | 8 | 64 | 7 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 1, 2 | 3 | 10 | 30 | 3 |
| | | WB | 1, 2 | 3 | 70 | 210 | 23 |
| | | WB | EOP | 3 | 40 | 120 | 13 |
| | | WB | 1, Median | 4 | 4 | 16 | 2 |
| | | WB | Median | 14 | 10 | 140 | 16 |
| | | WB | 2 | 12 | 35 | 420 | 47 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 15 | 45 | 5 |
| | | WB | 1,Median | 6 | 65 | 390 | 43 |
| | | WB | 2 | 6 | 4 | 24 | 3 |
| | | WB | 1, 2 | 6 | 20 | 120 | 13 |
| | | WB | 1, 2 | 3 | 65 | 195 | 22 |
| | | WB | 1, Median | 3 | 30 | 90 | 10 |
| | Crave Avenue | WB | 1, Median | 3 | 25 | 90 75 | 8 |
| O A | Grove Avenue | WB | EOP | 3 | 70 | 210 | 23 |
| Grove Avenue | | | | | | | |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | <u> </u> | WB | 1 | 12 | 3 | 36 | 4 |
| | Bridge Omission | WB | 2 | 12 | 3 | 36 | 4 |
| Bridge Omission | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | Forest View Avenue | WB | EOP | 3 | 10 | 30 | 3 |
| Forest View Avenue | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 1, 2 | 3 | 14 | 42 | 5 |
| | | WB | 1 | 4 | 30 | 120 | 13 |
| | | WB | 1, Median | | 30 | 90 | 10 |
| | | WB | 2 | 12 | 24 | 288 | 32 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | Harvey Avenue | WB | 2 | 12 | 25 | 300 | 33 |
| Harvey Avenue | Tidivey Aveilue | WB | EOP | 3 | 15 | 45 | 5 |
| naivey Avenue | | WB | 1, Median | 3 | 50 | 150 | 17 |
| | | | 1, Median | | | | 7 |
| | | WB | | 12 | 5 | 60 | |
| | | WB | 1, Median | 6 | 18 | 108 | 12 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 3 | 45 | 135 | 15 |
| | | WB | 1 | 15 | 4 | 60 | 7 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 15 | 3 | 45 | 5 |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMIT | TENT RE | SHRFA | CING SCH | IEDIJI E | F.A. | SECTION | COUNTY | TOTAL SHEETS | SHEET |
|---|------------------------------|-----------------|-----------|------------------------------|--------|----------|---------|--------|------------|----------|------|---------------|-------------|-----------------|-------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | | | II 40 | 01110 0011 | LEGGEE | VAR. | 2015-030RS | DUPAGE | 44 | 20 |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | | | IL 19 | | | | | CONTRAC | T NO. 6 | 2A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/6/2015 | REVISED - | | SCALE: | SHEET 14 | OF 29 | SHEETS | STA. | TO STA. | | ILLINOIS FED. | AID PROJECT | | |

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------------------|-------------------|--|---|--|--|---|--|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| TROW | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | WB | 1 | 15 | 3 | 45 | 5 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | LT | 12 | 16 | 192 | 21 |
| | | WB | 2 | 6 | 17 | 102 | 11 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | + | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | Addison Road | WB | 2 | 12 | 3 | 36 | 4 |
| Addison Dood | Addison Road | WB | | 6 | | | 53 |
| Addison Road | | | EOP | | 80 | 480 | |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 2 | 25 | 8 | 200 | 22 |
| | | WB | 2 | 5 | 30 | 150 | 17 |
| | | WB | 2 | 5 | 30 | 150 | 17 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 11 | 132 | 15 |
| | | WB | 2 | 12 | 35 | 420 | 47 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 6 | 18 | 108 | 12 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2, RT | 3 | 120 | 360 | 40 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 15 | 3 | 45 | 5 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | Dalewood Avenue | WB | 2 | 12 | 3 | 36 | 4 |
| Dalewood Avenue | Balewood / Wellac | WB | INT | 8 | 8 | 64 | 7 |
| Daiewood / Weride | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 19 | 228 | 25 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | 000 | | 12 | 1 3 1 | | |
| | | \A/D | 1 2 | | | 10 | 5 |
| | | WB | 1, 2 | 3 | 16 | 48 75 | 5 |
| | | WB | 1, Median | 3 15 | 16 5 | 75 | 8 |
| | | WB WB | 1, Median 1 | 3 15 16 | 16 5 3 | 75 48 | 8 5 |
| | | WB WB WB | 1, Median 1 2 | 3 15 16 12 | 16 5 3 6 | 75 48 72 | 8 5 8 |
| | | WB WB WB | 1, Median 1 2 1, 2 | 3 15 16 12 3 | 16 5 3 6 15 | 75 48 72 45 | 8 5 8 5 |
| | | WB WB WB WB | 1, Median 1 2 1, 2 LT | 3 15 16 12 3 12 | 16 5 3 6 15 3 | 75 48 72 45 36 | 8 5 8 5 4 |
| | | WB WB WB WB WB | 1, Median 1 2 1, 2 LT 1 | 3 15 16 12 3 12 | 16 5 3 6 15 3 | 75 48 72 45 36 36 | 8 5 8 5 4 4 |
| | | WB WB WB WB WB WB | 1, Median 1 2 1, 2 LT 1 2 | 3 15 16 12 3 12 12 12 | 16 5 3 6 15 3 3 | 75 48 72 45 36 36 | 8 5 8 5 4 4 21 |
| | | WB WB WB WB WB WB WB WB | 1, Median 1 2 1, 2 LT 1 2 LT 1 LT | 3 15 16 12 3 12 12 12 12 | 16 5 3 6 15 3 3 16 6 | 75 48 72 45 36 36 192 72 | 8 5 8 5 4 4 21 8 |
| | | WB WB WB WB WB WB WB WB WB | 1, Median 1 2 1, 2 LT 1 2 LT 1 1 | 3 15 16 12 3 12 12 12 12 12 | 16 5 3 6 15 3 3 16 6 | 75 48 72 45 36 36 192 72 | 8 5 8 5 4 4 21 8 8 |
| | | WB WB WB WB WB WB WB WB WB WB | 1, Median 1 2 1, 2 LT 1 2 LT 1 2 LT 1 2 LT 1 2 | 3 15 16 12 3 12 12 12 12 12 12 | 16 5 3 6 15 3 3 16 6 6 | 75 48 72 45 36 36 192 72 72 | 8 5 8 5 4 4 4 21 8 8 8 8 |
| | | WB WB WB WB WB WB WB WB WB WB WB | 1, Median 1 2 1, 2 LT 1 2 LT 1 2 LT 1 LT 1 LT 1 LT 1 LT 1 | 3 15 16 12 3 12 12 12 12 12 12 12 12 | 16 5 3 6 15 3 3 16 6 6 | 75 48 72 45 36 36 192 72 72 72 72 36 | 8 5 8 5 4 4 21 8 8 8 |
| | | WB WB WB WB WB WB WB WB WB WB WB WB | 1, Median 1 2 1, 2 LT 1 2 LT 1 2 LT 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 | 3 15 16 12 3 12 12 12 12 12 12 12 12 12 | 16 5 3 6 15 3 3 16 6 6 6 | 75 48 72 45 36 36 192 72 72 72 36 36 | 8 5 8 5 4 4 21 8 8 8 8 |
| | | WB WB WB WB WB WB WB WB WB WB WB WB WB | 1, Median 1 2 1, 2 LT 1 2 LT 1 2 LT 1 2 LT 1 2 LT 1 2 | 3 15 16 12 3 12 12 12 12 12 12 12 12 12 12 | 16 5 3 6 15 3 3 16 6 6 6 | 75 48 72 45 36 36 192 72 72 72 72 36 36 36 144 | 8 5 8 5 4 4 21 8 8 8 4 4 |
| | | WB WB WB WB WB WB WB WB WB WB WB WB WB W | 1, Median 1 2 1, 2 LT 1 2 LT 1 2 LT 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 | 3 15 16 12 3 12 12 12 12 12 12 12 12 12 12 | 16 5 3 6 15 3 3 16 6 6 6 6 3 3 3 | 75 48 72 45 36 36 192 72 72 72 72 36 36 36 144 | 8 5 8 5 4 4 21 8 8 8 4 4 4 |
| | | WB WB WB WB WB WB WB WB WB WB WB WB WB | 1, Median 1 2 1, 2 LT 1 2 LT 1 2 LT 1 2 LT 1 2 LT 1 2 | 3 15 16 12 3 12 12 12 12 12 12 12 12 12 12 | 16 5 3 6 15 3 3 16 6 6 6 | 75 48 72 45 36 36 192 72 72 72 72 36 36 36 144 | 8 5 8 5 4 4 21 8 8 8 4 4 |
| | | WB WB WB WB WB WB WB WB WB WB WB WB WB W | 1, Median 1 2 1, 2 LT 1 2 LT 1 2 LT 1 2 LT 1 1 2 LT 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 | 3 15 16 12 3 12 12 12 12 12 12 12 12 12 12 | 16 5 3 6 15 3 3 16 6 6 6 6 3 3 3 | 75 48 72 45 36 36 192 72 72 72 72 36 36 36 144 | 8 5 8 5 4 4 21 8 8 8 4 4 4 |

| ROUTE: | IL 19 (Prospect Avenue to York Road) | (Continued) |
|--------|--------------------------------------|-------------|

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|------------------|-----------------|-----------|-----------|----------|----------|-----------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | LT | 15 | 5 | 75 | 8 |
| | | WB | 1, LT | 6 | 40 | 240 | 27 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 16 | 48 | 5 |
| | | WB | 1, Median | | 22 | 66 | 7 |
| | | WB | 1 | 6 | 15 | 90 | 10 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1, 2 | 3 | 50 | 150 | 17 |
| | | WB | EOP | 3 | 25 | 75 475 | 8 |
| | | WB | EOP | 5 | 35 | 175 | 19 |
| | | WB | 1, 2 | 3 | 260 | 780 | 87 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB WB | 2 | 12 12 | 3 | 36 36 | 4 |
| | | WB | EOP | 3 | 10 | 30 | 3 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | Prospect Avenue | WB | 1 | 12 | 3 | 36 | 4 |
| Prospect Avenue | Flospect Avenue | EB | EOP | 6 | 23 | 138 | 15 |
| 1 103pect Avenue | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | EOP | 3 | 26 | 78 | 9 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 15 | 45 | 5 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1,Median | | 10 | 30 | 3 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | 0 | EB | 1 | 12 | 3 | 36 | 4 |
| 0 5 . | Station Drive | EB | 2 | 12 | 3 | 36 | 4 |
| Station Drive | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | | | | | | |
| | Miller Lane | EB EB | 1 2 | 12 12 | 6 6 | 72 72 | 8 |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMITTENT RESURFACING SCHEDULE | F.A. | SECTION | COUNTY | TOTAL | SHEET NO. |
|--------------------------------|------------------------------|-----------------|-----------|------------------------------|--------|------------------------------------|------|-----------------|-----------|---------|--------------|
| c:\pw_work\pwidot\bartonrw\d04 | 27922\HMA-DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | II 10 | VAR. | 2015-030RS | DUPAGE | 44 | 21 |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | IL 19 | | | CONTRAC | T NO. F | 2A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/7/2015 | REVISED - | | SCALE: | SHEET 15 OF 29 SHEETS STA. TO STA. | | ILLINOIS FED. A | D PROJECT | | |

| CPOSS | STREET | DIRECTION | LANE | IDAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-----------------|--------------------|-----------|-----------|-----------|----------|---------|---------|
| | | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| FROM | ТО | 1 ' | I | | | | 1 |
| N.C.II | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| Miller Lane | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | EOP | 3 | 50 | 150 | 17 |
| | | EB | 2 | 12 | 9 | 108 | 12 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 3 | 25 | 75 | 8 |
| | | EB | 2 | 6 | 6 | 36 | 4 |
| | | EB | 1 | 16 | 4 | 64 | 7 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 8 | 12 | 96 | 11 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | | | | | 30 | |
| | Delevered Assessed | EB | 1 | 10 | 3 | | 3 |
| D-1- 1 * | Dalewood Avenue | EB | 2 | 6 | 20 | 120 | 13 |
| Dalewood Avenue | | EB | EOP | 3 | 90 | 270 | 30 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 22 | 66 | 7 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | RT | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | 1 | 6 | 6 | 36 | 4 |
| | | EB | RT | 12 | 6 | 72 | 8 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | | | | | |
| | | | 2 | 12 | 6 | 72 | 8 |
| | 1 | EB | RT | 12 | 6 | 72 | 8 |
| | Addison Road | EB | EOP | 3 | 23 | 69 | 8 |
| Addison Road | | EB | INT | 14 | 20 | 280 | 31 |
| | | EB | EOP | 3 | 60 | 180 | 20 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 6 | 40 | 240 | 27 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | EOP | 3 | 25 | 75 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 8 | 96 | 11 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 20 | 240 | 27 |
| | | EB | 2 | 12 | 3 | 36 | |
| | | | | | | | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 15 | 180 | 20 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 40 | 480 | 53 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | 1 | 12 | 35 | 420 | 47 |
| | 1 | | | | 1 | | |

| ROUTE: | IL 19 (Prospect Avenue to York Road) | (Continued) |
|--------|--------------------------------------|-------------|
| | | |

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|--------------------|--------------------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | + | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | + | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | + | EB | 1 | 12 | 5 | 60 | 7 |
| | | | | 12 | 5 | 60 | 7 |
| | | EB | 1 | | | | |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | LT | 6 | 40 | 240 | 27 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1, 2 | 3 | 40 | 120 | 13 |
| | | EB | LT | 12 | 25 | 300 | 33 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Harvey Avenue | EB | 2 | 12 | 6 | 72 | 8 |
| Harvey Avenue | , | EB | EOP | 6 | 50 | 300 | 33 |
| , | | EB | 2 | 12 | 26 | 312 | 35 |
| | | EB | 1 | 12 | 35 | 420 | 47 |
| | + | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | + | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | | | | 360 | 40 |
| | | | 1, Median | | 60 | | |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | 1 | 12 | 15 | 180 | 20 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Forest View Avenue | EB | 2 | 12 | 3 | 36 | 4 |
| Forest View Avenue | | EB | 1, 2 | 3 | 35 | 105 | 12 |
| | | EB | EOP | 3 | 60 | 180 | 20 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | Bridge Omission | EB | 2 | 12 | 3 | 36 | 4 |
| Bridge Omission | Diluge Offission | EB | LT | 12 | 10 | 120 | 13 |
| Bridge Omission | | | | | | | |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1, 2 | 3 | 17 | 51 | 6 |
| | | EB | 1, 2 | 3 | 12 | 36 | 4 |
| | | EB | 1, Median | 3 | 17 | 51 | 6 |
| | Edgebrook Road | EB | 2 | 12 | 40 | 480 | 53 |
| Edgebrook Road | | EB | INT | 6 | 60 | 360 | 40 |
| | | EB | INT | 12 | 10 | 120 | 13 |
| | | EB | 1, 2 | 3 | 35 | 105 | 12 |
| | | | | | | | |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMITTENT RESURFACING SCHEDULE | F.A. | SECTION | COUNTY | TOTAL | SHEET |
|---|------------------------------|-----------------|-----------|------------------------------|--------|------------------------------------|------|------------------|----------|---------|-------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | II 40 | VAR. | 2015-030RS | DUPAGE | 44 | 22 |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | IL 19 | | | CONTRACT | T NO. f | 52A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/7/2015 | REVISED - | | SCALE: | SHEET 16 OF 29 SHEETS STA. TO STA. | | ILLINOIS FED. AI | | | |

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------------------|---------------------|-----------|--------------|----------|----------|------------|----------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| TROW | 10 | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 1, 2 | 6 | 20 | 120 | 13 |
| | | EB | 1, 2 | 3 | 20 | 60 | 7 |
| | | EB | 1, 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, Median | | 50 | 300 | 33 |
| | | EB | 1, Median | 6 | 30 | 180 | 20 |
| | Only your and Drive | | | 3 | | | 7 |
| Oakwood Drive | Oakwood Drive | EB | 1, 2 | | 22 | 66 | |
| Oakwood Drive | | EB | EOP | 6 | 60 | 360 | 40 |
| | | EB | 1, 2 | 3 | 90 | 270 | 30 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1, 2 | 3 | 40 | 120 | 13 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | 1, LT | 3 | 45 | 135 | 15 |
| | | EB | 1, 2 | 3 | 60 | 180 | 20 |
| | | EB | 1 | 12 | 12 | 144 | 16 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1, 2 | 3 | 40 | 120 | 13 |
| | Wood Dale Road | EB | 1 | 12 | 3 | 36 | 4 |
| Wood Dale Road | | EB | INT | 12 | 20 | 240 | 27 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1, 2 | 6 | 33 | 198 | 22 |
| | | EB | 1 | 3 | 6 | 18 | 2 |
| | Railroad Omission | EB | 1, 2 | 6 | 30 | 180 | 20 |
| Railroad Omission | | EB | 1, 2 | 3 | 30 | 90 | 10 |
| | | EB | 2 | 12 | 25 | 300 | 33 |
| | | EB | 1, 2 | 3 | 30 | 90 | 10 |
| | | EB | 1, Median | 15 | 15 | 225 | 25 |
| | | EB | 1, 2 | 3 | 10 | 30 | 3 |
| | | EB | 1, 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 6 | 25 | 150 | 17 |
| | | EB | 2 | 12 | 15 | 180 | 20 |
| | | EB | 1, 2 | 3 | 23 | 69 | 8 |
| | | EB | 1, 2 | 3 | 30 | 90 | 10 |
| | | EB | LT | 8 | 10 | 80 | 9 |
| | | EB | LT | 3 | 16 | 48 | 5 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | LT | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 1, 2 | 3 | 130 | 390 | 43 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | EOP | 3 | 10 | 30 | 3 |
| | | EB | 1, 2 | 3 | 30 | 90 | 10 |
| | | EB | 2 | 12 | 20 | 240 | 27 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | 1, 2 | 3 | 50 | 150 | 17 |
| | i | | ۱, ∠ | ر ا | 1 50 | 130 | 1 17 |
| | | | 1 2 | 2 | 60 | 190 | 20 |
| | | EB EB | 1, 2 1, 2 | 3 | 60 60 | 180 180 | 20 20 |

| | , , , , , , , , , , , , , , , , , , , |
|--|---------------------------------------|
| ROUTE: IL 19 (Prospect Avenue to York Road |) (Continued) |
| NOOTE. HE TO IT TO SPECIAL AVEILUE TO IT ON TOOL | (Continued) |

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------------------|--------------------|-----------|-----------|----------|----------|------------|----------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 12 | 36 | 4 |
| | | EB | 1 | 20 | 6 | 120 | 13 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | <u> </u> | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1, LT | 3 | 15 | 45 | 5 |
| | | EB | LT | 12 | 12 | 144 | 16 |
| | | EB | 2 | 12 | 12 | 144 | 16 |
| | + | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | | | | 125 | 375 | 42 |
| | | EB EB | 1, 2 1 | 3 12 | 3 | 36 | 42 |
| | | | 1 | | | | |
| | | EB | | 20 | 6 | 120 | 13 |
| | | EB | LT | 20 | 5 | 100 | 11 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | LT | 12 | 15 | 180 | 20 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 80 | 960 | 107 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | LT | 12 | 15 | 180 | 20 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | Hemlock Avenue | EB | 2 | 12 | 10 | 120 | 13 |
| Hemlock Avenue | | EB | EOP | 3 | 60 | 180 | 20 |
| | | EB | 1 | 12 | 25 | 300 | 33 |
| | | EB | 1, 2 | 3 | 23 | 69 | 8 |
| | | EB | 1 | 12 | 65 | 780 | 87 |
| | Ash Avenue | EB | 1, 2 | 3 | 80 | 240 | 27 |
| Ash Avenue | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | EOP | 3 | 50 | 150 | 17 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 12 | 8 | 96 | 11 |
| | | EB | Median | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1, 2 | 3 | 18 | 54 | 6 |
| | | EB | Median | 12 | 6 | 72 | 8 |
| | | EB | Median | 12 | 20 | 240 | 27 |
| | 1 | EB | Median | 12 | 4 | 48 | 5 |
| | | EB | 1, 2 | 3 | 18 | 54 | 6 |
| | Edgewood Avenue | EB | 1, 2 | 3 | 16 | 48 | 5 |
| Edgewood Avenue | _agovoda / (veride | EB | EOP | 3 | 60 | 180 | 20 |
| _agewood / Wellac | | EB | 1,2 | 3 | 40 | 120 | 13 |
| | | EB | 1,2 | 12 | 25 | 300 | 33 |
| | + | EB | Median | 12 | 3 | 36 | 4 |
| | + | EB | Median | 12 | 23 | 276 | 31 |
| | | | | | | | |
| | | EB | 1, 2 | 3 | 34 | 102 | 11 |
| | | EB | 1, Median | | 30 | 90 | 10 |
| | | | | | | | |
| | | EB EB | 1, 2 | 12 3 | 14 50 | 168 150 | 19 17 |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMITTENT RESURFACING SCHEDULE | F.A. | SECTION | COUNTY | TOTAL | SHEET NO. |
|-----------------------|--------------------------------|-----------------|-----------|------------------------------|--------|------------------------------------|------|-----------------|------------|---------|--------------|
| c:\pw_work\pwidot\bar | rtonrw\d0427922\HMA-DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | II 10 | VAR. | 2015-030RS | DUPAGE | 44 | 23 |
| | PLOT SCALE = 100.0000 ' / 10. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | IL 19 | | | CONTRAC | T NO. f | 2A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/7/2015 | REVISED - | | SCALE: | SHEET 17 OF 29 SHEETS STA. TO STA. | | ILLINOIS FED. A | ID PROJECT | | |

| CDOSS | STREET | DIRECTION | LANE | I DAVÆMENIT | PAVEMENT | REPAIR | REPAIR |
|---------------|---------------|-----------|------------|-------------|----------|-----------|---------|
| FROM | | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| FROM | ТО | | l | WIDTH | LENGTH | | 1 |
| | | (NB/SB) | (1, 2, 3) | | | (SQ FT) | (SQ YD) |
| | | EB | 1, Median | 3 | 86 | 258 | 29 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | Median | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 33 | 99 | 11 |
| | | EB | 1, Median | | 33 | 99 | 11 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | ļ <u></u> | EB | 1 | 12 | 4 | 48 | 5 |
| 5: 1 | Pine Lane | EB | 1 | 12 | 3 | 36 | 4 |
| Pine Lane | | EB | EOP | 3 | 30 | 90 | 10 |
| | | EB | 2 | 3 | 5 | 15 | 2 |
| | | EB | 1, LT | 3 | 40 | 120 | 13 |
| | | EB | 1, 2 | 3 | 15 | 45 | 5 |
| | | EB | 1 | 20 | 6 | 120 | 13 |
| | | EB | LT | 12 | 5 | 60 | 7 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 20 | 240 | 27 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 55 | 165 | 18 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | LT | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1, LT | 3 | 10 | 30 | 3 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | 1 0 1 | EB | 1 | 12 | 3 | 36 | 4 |
| 0 | Spruce Avenue | EB | 2 | 12 | 3 | 36 | 4 |
| Spruce Avenue | | EB | 1 | 12 | 15 | 180 | 20 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | EOP | 3 | 26 | 78 10 | 9 |
| | | EB | 1 1 | 6 12 | 3 | 18 | 2 |
| | | EB | - | | 45 | 540 | 60 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 6 12 | 3 | 18 36 | 2 4 |
| | | EB | 2 | 12 | 3 | 36 | |
| | | EB | 1 | | 3 | | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 7 |
| | | EB | 2 | 5 3 | 12 | 60 45 | 5 |
| | | EB EB | EOP EOP | 3 | 15 75 | 45 225 | 25 |
| | | | | 3 | | 150 | |
| | | EB | EOP | | 50 | | 17 |
| | | EB | EOP | 3 | 20 | 60 | 7 |
| | | EB | 1, 2 | 12 | 8 | 96 180 | 11 |
| | | EB | 1, 2 | 6 | 30 | 180 | 20 |
| | | EB | LT 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | 1 | EB | 2 | 12 | 3 | 36 | 4 |

| ROUTE: | IL 19 (Prospect Avenue to York Road) | (Continued) |
|--------|--------------------------------------|-------------|
| | | |

| CROSS ST FROM | | DIRECTION | LANE | 1 / \V L V L \ | PAVEMENT | REPAIR | REPAIR |
|--------------------|------------------|-----------|-----------|----------------------|----------|---------|---------|
| | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Marshall Road | EB | 2 | 12 | 3 | 36 | 4 |
| Marshall Road | Maishaii read | EB | 1 | 12 | 6 | 72 | 8 |
| Iviai silali 1 Cau | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | EOP | 3 | 15 | 45 | 5 |
| | | EB | | 12 | 3 | 36 | 4 |
| | | | 2 | | | | |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | EOP | 3 | 25 | 75 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | EOP | 3 | 20 | 60 | 7 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 6 | 90 | 540 | 60 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 130 | 390 | 43 |
| | | EB | EOP | 3 | 40 | 120 | 13 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | 2 | 12 | 55 | 660 | 73 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 1, 2 | 3 | 18 | 54 | 6 |
| | | EB | 1, 2 | 3 | 35 | 105 | 12 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 26 | 312 | 35 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | | | 3 | | | |
| | | EB | 1, 2 | | 5 | 15 | 2 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 7 | 5 | 35 | 4 |
| | | EB | 1, 2 | 3 | 65 | 195 | 22 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | EOP | 3 | 35 | 105 | 12 |
| | | EB | 1, 2 | 3 | 40 | 120 | 13 |
| | | EB | 1, 2 | 3 | 40 | 120 | 13 |
| | | EB | 2 | 12 | 10 | 120 | 13 |
| | | EB | 1 | 12 | 10 | 120 | 13 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1, 2 | 3 | 25 | 75 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 30 | 360 | 40 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | Eastview Avenue | EB | 2 | 12 | 20 | 240 | 27 |
| Eastview Avenue | Lastriew Avellue | EB | 1, 2 | 3 | 45 | 135 | 15 |
| Eastview Averlue | | | | | | | |
| | | EB | EOP | 3 | 45 | 135 | 15 |
| | | EB | 2 | 12 | 40 | 480 | 53 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 45 | 135 | 15 |
| 1 | | EB | 1, 2 | 3 | 50 | 150 | 17 |
| | | EB | 2 | 12 | 3 | 36 | 4 |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMIT | TENT RE | SIIRFA | CING SCH | FNIJI F | F.A. | SECTION | COUNTY | TOTAL S SHEETS | SHEET NO. |
|---|------------------------------|-----------------|-----------|------------------------------|--------|----------|---------|--------|------------|---------|------|---------------|-------------|-------------------|--------------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | | | II 40 | onita ooni | EDOLL | VAR. | 2015-030RS | DUPAGE | 44 | 24 |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | | | IL 19 | | | | | CONTRAC | T NO. 6 | 2A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/7/2015 | REVISED - | | SCALE: | SHEET 18 | OF 29 | SHEETS | STA. | TO STA. | | ILLINOIS FED. | AID PROJECT | | |

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|---------------|---------------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 110 | 330 | 37 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 10 | 3 | 30 | 3 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 22 | 66 | 7 |
| | | EB | 2 | 12 | 15 | 180 | 20 |
| | | EB | 2 | 12 | 50 | 600 | 67 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 12 | 20 | 240 | 27 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 6 | 25 | 150 | 17 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1, 2 | 3 | 60 | 180 | 20 |
| | Parkside Lane | EB | 2 | 12 | 3 | 36 | 4 |
| Parkside Lane | | EB | 2 | 12 | 50 | 600 | 67 |
| | | EB | 2 | 12 | 35 | 420 | 47 |
| | | EB | EOP | 3 | 12 | 36 | 4 |
| | | EB | 1, 2 | 3 | 25 | 75 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 20 | 5 | 100 | 11 |
| | | EB | RT | 12 | 22 | 264 | 29 |
| | | EB | 2 | 10 | 3 | 30 | 3 |
| | | EB | 1 | 10 | 3 | 30 | 3 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 35 | 420 | 47 |
| | Church Road | EB | EOP | 6 | 20 | 120 | 13 |

TOTALS: 13920 8714 FT SY

ROUTE: IL 19 (IL 53 to Barrington Road)

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|------------------|------------------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| Barrington Road | | EB | 2 | 12 | 40 | 480 | 53 |
| | | EB | 2 | 3 | 90 | 270 | 30 |
| | | EB | 1,2 | 3 | 20 | 60 | 7 |
| | | EB | 1,2 | 3 | 5 | 15 | 2 |
| | 1 | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1,2 | 3 | 60 | 180 | 20 |
| | | EB | 2 | 3 | 45 | 135 | 15 |
| | | | | 3 | 35 | | 12 |
| | | EB | 2 | | | 105 | |
| | | EB | 1,2 | 3 | 15 | 45 | 5 |
| | | EB | 2 | 4 | 12 | 48 | 5 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 3 | 35 | 105 | 12 |
| | | EB | 2 | 3 | 45 | 135 | 15 |
| | | EB | 12 | 12 | 5 | 60 | 7 |
| | | EB | 2 | 3 | 20 | 60 | 7 |
| | | EB | 2 | 3 | 45 | 135 | 15 |
| | | EB | 2 | 3 | 100 | 300 | 33 |
| | | EB | 2 | 9 | 3 | 27 | 3 |
| | Kingsbury Drive | EB | 2 | 9 | 3 | 27 | 3 |
| Kingsbury Drive | Tringsbury Drive | EB | 2 | 5 | 12 | 60 | 7 |
| Kingsbury Drive | | EB | 2 | 3 | 15 | 45 | 5 |
| | | | | | | | |
| | | EB | 2 | 3 | 30 | 90 | 10 |
| | | EB | 1,2 | 3 | 5 | 15 | 2 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Cumberland Drive | EB | LT,1 | 3 | 65 | 195 | 22 |
| Cumberland Drive | Longmeadow Lane | EB | CL | 6 | 10 | 60 | 7 |
| Longmeadow Lane | Edgebrook Lane | EB | 1 | 12 | 3 | 36 | 4 |
| Edgebrook Lane | | EB | 1,2 | 3 | 55 | 165 | 18 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | LT | 10 | 3 | 30 | 3 |
| | <u> </u> | EB | 1 | 12 | 3 | 36 | 4 |
| | Northway Drive | EB | 2 | 12 | 3 | 36 | 4 |
| Na atlanca Daina | Northway Drive | EB | 1 | 12 | 6 | 72 | |
| Northway Drive | | | | | | | 8 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | Olde Salem Road | EB | LT | 12 | 3 | 36 | 4 |
| Olde Salem Road | | EB | CL,1 | 3 | 55 | 165 | 18 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | LT,1 | 3 | 40 | 120 | 13 |
| | | EB | 2 | 6 | 4 | 24 | 3 |
| | | EB | LT,1 | 3 | 65 | 195 | 22 |
| | | EB | 2 | 3 | 45 | 135 | 15 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | | 2 | 12 | 3 | | |
| | | EB | | | | 36 | 4 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | Orchard Lane | EB | 2 | 13 | 3 | 39 | 4 |
| Orchard Lane | | EB | CL | 3 | 6 | 18 | 2 |
| | | EB | 2 | 14 | 4 | 56 | 6 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | | | | | 70 | |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMIT | TENT RES | SURFAC | ING SCHED | IJI F | F.A. RTF. | SECTION | COUNTY | TOTAL S | SHEET NO. |
|---|------------------------------|-----------------|-----------|------------------------------|--------|----------|----------|--------|------------|---------|--------------|---------------|-------------|----------|--------------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | | | 11 40 | into Coned | | VAR. | 2015-030RS | DUPAGE | 44 | 25 |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | | | IL 19 | | | | | CONTRACT | T NO. 67 | 2A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/7/2015 | REVISED - | | SCALE: | SHEET 19 | OF 29 | SHEETS | STA. | TO STA. | | ILLINOIS FED. | AID PROJECT | | |

ROUTE: IL 19 (IL 53 to Barrington Road) (Continued)

| CROSS STREET DIRECTION (EB/WB) (NO. (NB/SB)) LANE (NB/SB) (1,2,3) PAVEMENT PAVEMENT PAVEMENT PATCH AR (SQ WDTH LENGTH (SQ WDTH LENGTH)) REF EB 2 12 6 7 EB 1,2 3 160 48 EB 2 12 4 4 EB 1 3 15 4 EB 1 9 3 2 EB 2 9 3 2 EB 1 10 3 3 EB 1 12 3 3 EB 2 9 3 2 EB 1 12 3 3 EB 1 12 3 3 EB 2 9 3 2 Olde Salem Circle EB RT 9 3 2 Olde Salem Circle EB RT 9 3 2 | EA AREA (SQ YD) 2 8 8 8 5 5 5 7 3 7 3 0 3 |
|---|---|
| (NB/SB) (1,2,3) WDTH LENGTH (SQ EB 2 12 6 7 EB 1,2 3 160 48 EB 1,2 3 160 48 EB 2 12 4 4 EB 1 3 15 4 EB 1 9 3 2 EB 2 9 3 2 EB LT 10 3 3 EB 1 12 3 3 EB 2 9 3 2 Olde Salem Circle EB RT 9 3 2 Olde Salem Circle EB 2 12 4 4 | FT) (SQ YD) 2 8 30 53 8 5 5 5 7 3 7 3 0 3 |
| EB 2 12 6 7 EB 1,2 3 160 48 EB 2 12 4 4 EB 1 3 15 4 EB 1 9 3 2 EB 2 9 3 2 EB LT 10 3 3 EB 1 12 3 3 EB 1 12 3 3 EB 2 9 3 2 Olde Salem Circle EB RT 9 3 2 Olde Salem Circle EB 2 12 4 4 | 2 8 30 53 8 5 5 5 7 3 7 3 0 3 |
| EB 1,2 3 160 48 EB 2 12 4 4 EB 1 3 15 4 EB 1 9 3 2 EB 2 9 3 2 EB LT 10 3 3 EB 1 12 3 3 EB 1 12 3 3 EB 2 9 3 2 Olde Salem Circle EB RT 9 3 2 Olde Salem Circle EB 2 12 4 4 | 30 53 8 5 5 5 7 3 0 3 |
| EB 2 12 4 4 EB 1 3 15 4 EB 1 9 3 2 EB 2 9 3 2 EB LT 10 3 3 EB 1 12 3 3 EB 1 12 3 3 EB 2 9 3 2 Olde Salem Circle EB RT 9 3 2 Olde Salem Circle EB 2 12 4 4 | 8 5 5 5 7 3 7 3 0 3 |
| EB 1 3 15 4 EB 1 9 3 2 EB 2 9 3 2 EB LT 10 3 3 EB 1 12 3 3 EB 2 9 3 2 Olde Salem Circle EB RT 9 3 2 Olde Salem Circle EB 2 12 4 4 | 5 5 7 3 7 3 0 3 |
| EB 1 9 3 2 EB 2 9 3 2 EB LT 10 3 3 EB 1 12 3 3 EB 2 9 3 2 Olde Salem Circle EB RT 9 3 2 Olde Salem Circle EB 2 12 4 4 | 7 3 7 3 0 3 |
| EB 2 9 3 2 EB LT 10 3 3 EB 1 12 3 3 EB 2 9 3 2 Olde Salem Circle EB RT 9 3 2 Olde Salem Circle EB 2 12 4 4 | 7 3 0 3 |
| EB LT 10 3 3 EB 1 12 3 3 EB 2 9 3 2 Olde Salem Circle EB RT 9 3 2 Olde Salem Circle EB 2 12 4 4 | 0 3 |
| EB 1 12 3 3 EB 2 9 3 2 Olde Salem Circle EB RT 9 3 2 Olde Salem Circle EB 2 12 4 4 | |
| EB 2 9 3 2 Olde Salem Circle EB RT 9 3 2 Olde Salem Circle EB 2 12 4 4 | 6 4 |
| Olde Salem Circle EB RT 9 3 2 Olde Salem Circle EB 2 12 4 4 | |
| Olde Salem Circle EB 2 12 4 4 | |
| | |
| EB 2,RT 3 15 4 | |
| EB 2 12 3 3 | |
| EB RT 12 3 3 | |
| EB 2,RT 3 10 3 | |
| EB 2 12 3 3 | |
| EB 2,RT 3 100 30 | |
| Wise Road EB RT 9 3 2 | |
| Wise Road EB 2 12 4 4 | |
| EB 2 12 4 4 4 EB 2 12 4 4 | |
| EB 2 4 10 4 | |
| EB 2 4 10 4 EB 2 3 75 22 | |
| EB 2 3 75 22 EB 2 3 50 15 | |
| EB 2 3 50 15 | |
| | |
| | |
| EB 2 3 40 12 EB 2 12 3 3 | |
| | |
| | |
| EB 2 12 4 4 | |
| EB 2 12 3 3 | |
| EB 2 12 3 3 | |
| EB LT 6 3 1 | |
| EB 1 12 3 3 | |
| EB 2 12 3 3 3 | |
| Mercury Drive EB LT,1 3 100 30 | |
| Wright Boulevard EB 1 14 3 4 | I |
| EB 1 14 3 4 | |
| EB 1 14 3 4 | |
| EB 1 14 3 4 | |
| EB 1 4 30 12 | |
| EB 1 14 3 4 | |
| Dupage County Line EB 1 14 3 4 | |
| Dupage County Line EB 1 14 3 4 | |
| EB 1 14 3 4 | |
| EB 1 14 12 16 | |
| EB 1 14 3 4 | |
| EB 1 14 5 7 | |
| EB 1 14 3 4 | |
| EB 1 14 3 4 | |
| EB LT,1 14 3 4 | |
| Mitchell Boulevard EB LT 12 3 3 | |
| | 8 5 |
| Mitchell Boulevard EB 1 16 3 4 RR Tracks EB 1 14 3 4 | 2 5 |

| DOLITE: II 40 (II 52 to Dominaton Dood) | (Continued) |
|---|-------------|
| ROUTE: IL 19 (IL 53 to Barrington Road) | (Continued) |

| CROSS S | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|------------------|-----------------|----------------|-----------|----------|----------|-----------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| RR Tracks | | EB | 1 | 14 | 4 | 56 | 6 |
| | | EB | 1 | 14 | 6 | 84 | 9 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 1 | 14 | 4 | 56 | 6 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | Williams Street | EB | 1 | 14 | 3 | 42 | 5 |
| Williams Street | Williams Street | EB | 1 | 14 | 3 | 42 | 5 |
| Williams Street | | EB | | | | | 5 |
| | | _ | 1 | 14 | 3 | 42 | |
| | | EB EB | 1 | 8 14 | 3 12 | 24 168 | 3 19 |
| | | | | | | | |
| | | EB | 1 | 14 | 4 | 56 | 6 |
| | | EB | 1 | 6 | 3 | 18 | 2 |
| | | EB | 1 | 6 | 4 | 24 | 3 |
| | | EB | 1 | 6 | 55 | 330 | 37 |
| | | EB | 1 | 3 | 85 | 255 | 28 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 6 | 20 | 120 | 13 |
| | | EB | 1 | 3 | 30 | 90 | 10 |
| | Lincoln Street | EB | 1 | 16 | 3 | 48 | 5 |
| Lincoln Street | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 13 | 5 | 65 | 7 |
| | | EB | 1 | 10 | 3 | 30 | 3 |
| | | EB | 1 | 9 | 5 | 45 | 5 |
| | | EB | 1 | 19 | 3 | 57 | 6 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 9 | 20 | 180 | 20 |
| | | EB | 1 | 19 | 4 | 76 | 8 |
| | | EB | 1 | 9 | 9 | 81 | 9 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | Marion Street | EB | 1 | 19 | 6 | 114 | 13 |
| Marion Street | | EB | 1 | 19 | 4 | 76 | 8 |
| | | EB | 1 | 9 | 6 | 54 | 6 |
| | | EB | 1 | 9 | 35 | 315 | 35 |
| | | EB | 1 | 10 | 3 | 30 | 3 |
| | | EB | 1 | 10 | 3 | 30 | 3 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | Roselle Road | EB | LT | 12 | 3 | 36 | 4 |
| Roselle Road | 1 toothe I toda | EB | 2 | 12 | 4 | 48 | 5 |
| 1 TOSCILE I TORU | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | | 12 | | 48 | 5 |
| | | EB | 2 | 6 | 4 | 24 | 3 |
| | | EB | | 8 | | 32 | |
| | | | 1 | | 4 | | 4 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | LT | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | | | . 40 | 1 / | 10 | 5 |
| | | EB | 2 | 12 | 4 | 48 | |
| | | EB EB EB | 2 1 | 6 12 | 10 | 60 48 | 7 5 |

| FILE NAM | ME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMITTENT RESURFACING SCHEDULE | F.A. | SECTION | COUNTY | TOTAL | SHEET |
|-----------|---------------------------------|------------------------------|-----------------|-----------|------------------------------|--------|------------------------------------|------|-----------------|-----------|---------|-------|
| c:\pw_wor | rk\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | II 10 | VAR. | 2015-030RS | DUPAGE | 44 | 26 |
| | | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | IL 19 | | | CONTRAC | T NO. f | 52A84 |
| Default | | PLOT DATE = 4/7/2015 | DATE - 4/7/2015 | REVISED - | | SCALE: | SHEET 20 OF 29 SHEETS STA. TO STA. | | ILLINOIS FED. A | D PROJECT | | |

| ROUTE: | IL 19 (IL 53 to Barrington Road) | (Continued) |
|--------|----------------------------------|-------------|
|--------|----------------------------------|-------------|

| FROM | CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|---|-----------------|-----------------|-----------|------|----------|----------|--------|---------|
| (NB/SB) (1,2,3) WOTH LENGTH (SC FT) (SO EB) | | | | NO. | ı | l I | | AREA |
| EB | | | | | | l I | | (SQ YD) |
| EB | | | | | | | | 2 |
| Fig. Fig. | | | | | | | | 5 |
| Prospect Street | | | | | | | | 5 |
| Prospect Street | | Dreamant Street | | | | | | |
| BB | B 1.01 | Prospect Street | | | | | | |
| EB | Prospect Street | | | | | | | 6 |
| EB | | | | | | | | 5 |
| EB | | | | | | | | 5 |
| EB | | | | 2 | | | 48 | 5 |
| EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 13 10 130 1 EB 1 12 4 48 5 EB 1 12 4 48 5 Park Street EB 2 12 4 48 5 EB 1 11 4 44 5 5 5 EB 1 11 4 44 5 6 6 7 3 35 445 5 5 5 6 6 6 7 2 8 6 6 6 7 2 8 6 6 6 7 2 8 8 1 1 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 | | | EB | 1 | 12 | 4 | 48 | 5 |
| EB | | | EB | 2 | 12 | 4 | 48 | 5 |
| EB | | | EB | 1 | 12 | 4 | 48 | 5 |
| EB | | | | 2 | | | | 5 |
| Park Street | | | | | | | | 14 |
| Park Street EB 2 12 4 48 5 BB 1,2 6 20 120 1 BB 2 13 35 455 5 BB 1 11 4 44 5 BB 1 11 4 44 5 BB 2 13 250 3250 36 BB 2 12 4 48 5 BB 2 12 4 48 5 BB 2 12 6 72 8 BB 2 13 60 780 8 BB 1,2 4 45 180 2 BB 1,2 4 45 180 2 BB 1 12 6 72 8 BB 1 12 6 72 8 BB 2 4 20 | | | | | | | | 5 |
| Park Street EB 1,2 6 20 120 1 EB 2 13 35 455 5 EB 1 11 4 44 4 4 4 4 4 4 4 4 5 5 5 5 5 5 5 5 5 5 5 5 5 5 5 5 5 5 6 EB 2 11 4 44 4 4 4 8 5 6 6 7 2 6 72 8 6 6 72 8 8 1 11 4 44 44 44 5 6 6 72 8 8 1 12 6 72 8 8 1 12 6 72 8 8 1 12 6 72 8 8 1 12 6 72 8 8 <td></td> <td>Park Street</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td>5</td> | | Park Street | | | | | | 5 |
| EB 2 13 35 455 5 EB 1 11 4 44 4 4 4 4 5 3 EB 2 13 250 3250 35 44 45 36 44 44 44 44 44 44 44 44 44 44 5 36 36 36 38 36 36 38 36 36 38 36 36 38 36 36 38 36 38 36 38 36 38 36 38 36 38 36 | Davis Chroat | Park Street | | | | | | |
| EB 1 11 4 44 5 EB 2 13 250 325 | Park Street | | | | | | | 13 |
| EB | | | | | | | | 51 |
| EB | | | | | | | | 5 |
| EB 2 12 4 48 5 EB 2 12 6 72 8 EB 1 11 4 44 5 EB 2 13 60 780 8 EB 2 13 60 780 8 EB 2 13 65 845 9 EB 2 13 65 845 9 EB 2 13 65 845 9 EB 2 4 20 80 9 EB 2 4 20 80 9 EB 2 4 70 280 3 EB 2 12 4 48 5 | | | | 2 | | 250 | | 361 |
| EB | | | | | | 6 | 66 | 7 |
| EB 1 11 4 44 5 EB 2 13 60 780 8 EB 1,2 4 45 180 2 EB 2 13 65 845 9 EB 1 12 6 72 8 EB 1 12 6 72 8 EB 2 4 20 80 9 EB 1 12 6 72 8 EB 2 12 6 72 8 EB 2 12 6 72 8 EB 2 12 4 48 5 EB 2 12 4 40 160 1 EB 1,2 4 40 160 1 EB 1,2 4 40 160 1 EB 2 12 4 48 | | | EB | 2 | 12 | 4 | 48 | 5 |
| EB 1 11 4 44 5 EB 2 13 60 780 8 EB 1,2 4 45 180 2 EB 2 13 65 845 9 EB 1 12 6 72 8 EB 1 12 6 72 8 EB 2 4 20 80 9 EB 1 12 6 72 8 EB 2 12 6 72 8 EB 2 12 6 72 8 EB 2 12 4 48 5 EB 2 12 4 40 160 1 EB 1,2 4 40 160 1 EB 1,2 4 40 160 1 EB 2 12 4 48 | | | EB | | | 6 | 72 | 8 |
| EB 2 13 60 780 8 EB 1,2 4 45 180 2 EB 2 13 65 845 9 EB 1 12 6 72 8 EB 2 4 20 80 9 EB 2 4 20 80 9 EB 2 12 6 72 8 EB 2 12 6 72 8 EB 2 12 4 48 5 EB 2 4 20 80 9 EB 2 4 20 80 9 EB 2 4 20 80 9 EB 1,2 4 40 160 1 EB 1 12 6 72 8 EB 2 12 4 48 5 | | | | | | | | 5 |
| EB 1,2 4 45 180 2 EB 2 13 65 845 9 EB 1 12 6 72 8 EB 2 4 20 80 9 EB 1 12 6 72 8 EB 2 12 6 72 8 EB 2 12 4 48 5 EB 2 4 70 280 3 EB 2 4 70 280 3 EB 2 4 20 80 9 EB 2 12 4 48 5 EB 2 12 4 40 160 1 EB 1 12 6 72 8 EB 2 12 6 72 8 EB 2 12 4 48 | | | | | | | | 87 |
| EB 2 13 65 845 9 EB 1 12 6 72 8 EB 2 4 20 80 9 EB 1 12 6 72 8 EB 2 12 6 72 8 EB 2 12 6 72 8 EB 2 4 70 280 3 EB 2 12 4 48 5 EB 2 4 20 80 9 EB 2 4 20 80 9 EB 1,2 4 40 160 16 EB 1 12 6 72 8 EB 2 12 6 72 8 EB 2 12 4 48 5 EB 2 12 4 48 5 <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td>20</td> | | | | | | | | 20 |
| EB 1 12 6 72 8 EB 2 4 20 80 9 EB 1 12 6 72 8 EB 2 12 6 72 8 EB 2 12 6 72 8 EB 2 4 70 280 3 EB 2 4 20 80 9 EB 2 4 20 80 9 EB 1,2 4 40 160 1 EB 2 12 4 48 5 EB 2 12 4 48 5 <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> | | | | | | | | |
| EB 2 4 20 80 5 EB 1 12 6 72 8 EB 2 12 6 72 8 EB 2 12 6 72 8 EB 2 4 70 280 3 EB 2 12 4 48 5 EB 2 4 20 80 9 EB 2 4 20 80 9 EB 1,2 4 40 160 1 EB 1,2 4 40 160 1 EB 1,2 4 40 160 1 EB 2 12 4 48 5 EB 2 12 4 48 5 EB 2 12 4 48 5 EB LT 13 4 52 6 | | | | | | | | |
| EB | | | | | | | | |
| EB 2 12 6 72 8 EB 2 4 70 280 3 EB 2 4 70 280 3 EB 2 12 4 48 5 EB 2 4 20 80 9 EB 1,2 4 40 160 1 EB 1 12 6 72 8 EB 2 12 6 72 8 EB 2 12 4 48 5 EB 2 12 4 48 5 EB LT 13 4 52 6 EB LT 13 4 52 6 EB LT 13 4 52 6 EB LT 13 4 48 5 EB 1 12 4 48 5 EB 1 12 4 48 5 EB 2 <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td>9</td> | | | | | | | | 9 |
| EB 2 4 70 280 3 EB 2 12 4 48 5 EB 2 4 20 80 5 EB 1,2 4 40 160 1 EB 2 12 6 72 8 EB 2 12 4 48 5 EB 2 12 4 48 5 EB LT 13 4 52 6 EB LT 13 4 52 6 EB LT 13 4 48 5 EB LT 12 4 48 5 EB 1 12 4 48 5 <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td>8</td> | | | | | | | | 8 |
| EB 2 12 4 48 5 EB 2 4 20 80 9 EB 1,2 4 40 160 1 EB 1,2 4 40 160 1 EB 1 12 6 72 8 EB 2 12 6 72 8 EB 2 12 4 48 5 EB 2 12 4 48 5 EB LT 13 4 52 6 EB LT 12 4 48 5 EB LT 12 4 48 5 EB 1 12 4 48 5 | | | | | 12 | | | 8 |
| EB 2 4 20 80 55 EB 1,2 4 40 160 1 EB 1,2 4 40 160 1 EB 1 12 6 72 8 EB 2 12 6 72 8 EB 2 12 4 48 5 EB 2 4 30 120 1 EB LT 13 4 52 6 EB LT 13 4 52 6 EB LT 12 4 48 5 Lawrence Avenue EB 1 12 4 48 5 EB 2 4 6 24 3 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 <t< td=""><td></td><td></td><td>EB</td><td>2</td><td>4</td><td>70</td><td>280</td><td>31</td></t<> | | | EB | 2 | 4 | 70 | 280 | 31 |
| EB 1,2 4 40 160 1 EB 1 12 6 72 8 EB 2 12 6 72 8 EB 2 12 4 48 5 EB 2 4 30 120 1 EB LT 13 4 52 6 EB LT 12 4 48 5 EB LT 12 4 48 5 Lawrence Avenue EB 1 12 4 48 5 EB 2 4 6 24 3 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 | | | EB | 2 | 12 | 4 | 48 | 5 |
| EB 1,2 4 40 160 1 EB 1 12 6 72 8 EB 2 12 6 72 8 EB 2 12 4 48 5 EB 2 4 30 120 1 EB LT 13 4 52 6 EB LT 12 4 48 5 EB LT 12 4 48 5 Lawrence Avenue EB 1 12 4 48 5 EB 2 4 6 24 3 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 | | | EB | 2 | 4 | 20 | 80 | 9 |
| EB 1 12 6 72 8 EB 2 12 6 72 8 EB 2 12 4 48 5 EB 2 4 30 120 1 EB LT 13 4 52 6 EB LT 12 4 48 5 EB LT 12 4 48 5 Lawrence Avenue EB 1 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 </td <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td>18</td> | | | | | | | | 18 |
| EB 2 12 6 72 8 EB 2 12 4 48 5 EB 2 4 30 120 1 EB LT 13 4 52 6 EB LT 12 4 48 5 EB LT 12 4 48 5 Lawrence Avenue EB 1 12 4 48 5 EB 1 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 1 12 4 48 5 | | | | | | | | 8 |
| EB 2 12 4 48 5 EB 2 4 30 120 1 EB LT 13 4 52 6 EB LT 12 4 48 5 EB Lawrence Avenue EB 1 12 4 48 5 Lawrence Avenue EB 2 4 6 24 3 EB 1 12 4 48 5 EB 2 4 6 24 3 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 1 | | | | | | | | 8 |
| EB 2 4 30 120 1 EB LT 13 4 52 6 EB LT 12 4 48 5 EB Lawrence Avenue EB 1 12 4 48 5 Lawrence Avenue EB 2 4 6 24 3 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 1 12 4 48 5 EB 1 | | | | | | | | 5 |
| EB LT 13 4 52 6 EB LT 12 4 48 5 Lawrence Avenue EB 1 12 4 48 5 Lawrence Avenue EB 2 4 6 24 3 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 | | | | | | | | 13 |
| EB LT 12 4 48 5 Lawrence Avenue EB 1 12 4 48 5 Lawrence Avenue EB 2 4 6 24 3 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 | | | | | | | | |
| Lawrence Avenue EB 1 12 4 48 5 Lawrence Avenue EB 2 4 6 24 3 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 12 4 48 5 EB 2 4 55 220 2 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 | | | | | | | | 6 |
| Lawrence Avenue EB 2 4 6 24 3 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 4 55 220 2 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 | | | | | | | | 5 |
| EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 12 4 48 5 EB 2 4 55 220 2 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 8 96 1 | | Lawrence Avenue | | | | | | 5 |
| EB 2 12 4 48 5 EB 2 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 4 55 220 2 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 8 96 1 | Lawrence Avenue | | | | | | | 3 |
| EB 2 12 4 48 5 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 4 55 220 2 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 8 96 1 | | | | | | | | 5 |
| EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 4 55 220 2 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 8 96 1 | | | EB | | | 4 | 48 | 5 |
| EB 1 12 4 48 5 EB 2 12 4 48 5 EB 2 4 55 220 2 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 8 96 1 | | | EB | 2 | 12 | 4 | 48 | 5 |
| EB 2 12 4 48 5 EB 2 4 55 220 2 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 8 96 1 | | | | | | | | 5 |
| EB 2 4 55 220 2 EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 8 96 1 | | | | | | | | 5 |
| EB 1 12 4 48 5 EB 2 12 4 48 5 EB 1 12 8 96 1 | | | | | | | | 24 |
| EB 2 12 4 48 5 EB 1 12 8 96 1 | | | | | | | | 5 |
| EB 1 12 8 96 1 | | | | | | | | 5 |
| | | | | | | | | |
| EB 2 12 8 96 1 | | | | | | | | 11 |
| | | | | | | | | 11 |
| | | | | | | | | 5 |
| | | | | | | | | 5 |
| EB 1,2 4 100 400 4 | | | EB | 1,2 | 4 | 100 | 400 | 44 |
| | | | EB | | 12 | 4 | 48 | 5 |

| ROUTE: | IL 19 (IL 53 to Barrington Road) | (Continued) |
|--------|----------------------------------|--------------|
| | in to the Barrington ready | (5511111454) |

| CROSS S | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-----------------|------------------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | LT | 12 | 4 | 48 | 5 |
| | | EB | LT | 12 | 4 | 48 | 5 |
| | Maple Avenue | EB | 1 | 12 | 4 | 48 | 5 |
| Maple Avenue | ' | EB | LT | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | Maple Avenue | EB | 2 | 4 | 45 | 180 | 20 |
| Maple Avenue | Ardmore Street | EB | 1 | 12 | 6 | 72 | 8 |
| Ardmore Street | Alumore Street | EB | 2 | 4 | 15 | 60 | 7 |
| Alumore Street | | EB | 1 | 12 | 4 | 48 | 5 |
| | | | | | | | |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 1,2 | 4 | 40 | 160 | 18 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 4 | 70 | 280 | 31 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 13 | 60 | 780 | 87 |
| | | EB | 2 | 4 | 20 | 80 | 9 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 6 | 10 | 60 | 7 |
| | | EB | 1 | 11 | 4 | 44 | 5 |
| | | EB | 2 | 13 | 55 | 715 | 79 |
| | | EB | 1 | 11 | 4 | 44 | 5 |
| | | EB | 1,2 | 4 | 15 | 60 | 7 |
| | | EB | 2 | 13 | 10 | 130 | 14 |
| | | | | + | 15 | | 7 |
| | | EB | 1,2 | 4 | | 60 | |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 45 | 540 | 60 |
| | | EB | 1,2 | 4 | 15 | 60 | 7 |
| | | EB | 1 | 12 | 15 | 180 | 20 |
| | Pinecroft Drive | EB | 2 | 12 | 30 | 360 | 40 |
| Pinecroft Drive | | EB | 1 | 12 | 4 | 48 | 5 |
| | Pinecroft Drive | EB | 2 | 4 | 12 | 48 | 5 |
| Pinecroft Drive | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 6 | 72 | 8 |
| | | EB | 2 | 4 | 50 | 200 | 22 |
| | | EB | 1,2 | 4 | 30 | 120 | 13 |
| | | EB | 2 | 12 | 40 | 480 | 53 |
| | | EB | 2 | 4 | 30 | 120 | 13 |
| | | EB | 2 | 4 | 10 | 40 | 4 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | | | | 44 | |
| | | | 2 | 11 | 4 | | 5 |
| | | EB | 2 | 4 | 80 | 320 | 36 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 13 | 60 | 780 | 87 |
| | | EB | 1 | 11 | 4 | 44 | 5 |
| <u> </u> | | EB | 1,RT | 4 | 30 | 120 | 13 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | RT | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | Springhill Drive | EB | RT | 12 | 4 | 48 | 5 |
| | Springhill Drive | l FR | l KI | 12 | 4 | 48 | ; |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMITTENT RESURFACING SCHEDULE | F.A. | SECTION | COUNTY | TOTAL | SHEET |
|----------------------------------|------------------------------|-----------------|-----------|------------------------------|--------|------------------------------------|------|-----------------|-----------|---------|-------|
| c:\pw_work\pwidot\bartonrw\d0427 | 922\HMA-DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | II 40 | VAR. | 2015-030RS | DUPAGE | 44 | 27 |
| | PLOT SCALE = 100.0000 '/ 10. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | IL 19 | | | CONTRAC | T NO. F | 2A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/7/2015 | REVISED - | | SCALE: | SHEET 21 OF 29 SHEETS STA. TO STA. | | ILLINOIS FED. A | D PROJECT | | |

ROUTE: IL 19 (IL 53 to Barrington Road) (Continued)

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|------------------|------------------|-----------|-----------|----------|----------|-----------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| Springhill Drive | | EB | RT | 12 | 4 | 48 | 5 |
| | Springhill Drive | EB | 1,2 | 4 | 100 | 400 | 44 |
| Springhill Drive | 1 0 | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 6 | 72 | 8 |
| | | EB | 1 | 13 | 4 | 52 | 6 |
| | | EB | 1 | 13 | 4 | 52 | 6 |
| | | EB | 1 | 13 | 25 | 325 | 36 |
| | | EB | 1 | 13 | 4 | 52 | 6 |
| | | EB | 1 | 4 | 6 | 24 | 3 |
| | Sycamore Avenue | EB | 1 | 4 | 60 | 240 | 27 |
| Sycamore Avenue | Gydaniole Avenue | EB | 1 | 3 | 35 | 105 | 12 |
| Sycamore Avenue | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | | | 35 | 48 105 | 12 |
| | | | 1 | 3 12 | | | |
| | | EB | 1 | | 4 | 48 | 5 |
| | | EB | 1 | 3 | 50 | 150 | 17 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 3 | 25 | 75 | 8 |
| | | EB | 1 | 9 | 3 | 27 | 3 |
| | | EB | 1 | 9 | 3 | 27 | 3 |
| | | EB | 1 | 9 | 3 | 27 | 3 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 9 | 3 | 27 | 3 |
| | | EB | 1 | 9 | 3 | 27 | 3 |
| | | EB | 1 | 12 | 20 | 240 | 27 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | Harvey Road | EB | 1 | 12 | 3 | 36 | 4 |
| Harvey Road | | EB | 1 | 9 | 3 | 27 | 3 |
| | | EB | 1 | 9 | 3 | 27 | 3 |
| | | EB | 1 | 9 | 3 | 27 | 3 |
| | | EB | 1 | 3 | 5 | 15 | 2 |
| | | EB | 1 | 3 | 6 | 18 | 2 |
| | | EB | 1 | 14 | 3 | 42 | 5 |
| | | EB | 2 | 8 | 4 | 32 | 4 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1,2 | 3 | 125 | 375 | 42 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 15 | 3 | 45 | 5 |
| | | EB | 2 | 15 | 3 | 45 | 5 |
| | | EB | 2 | 15 | 4 | 60 | 7 |
| | | EB | 2 | 15 | 4 | 60 | 7 |
| | | EB | 2 | 15 | 4 | 60 | 7 |
| | | EB | 1 | 6 | 3 | 18 | 2 |
| | | | | | | 10 | |
| | | EB | 2 | 13 | 4 | 52 | 6 |

| ROUTE: I | IL 19 (IL 53 to Barrington Road) | (Continued) |
|----------|----------------------------------|-------------|
|----------|----------------------------------|-------------|

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------------------|------------------|-----------|-----------|----------|----------|-----------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | LT | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | | | | 3 | | |
| | | EB | 2 | 12 | | 36 | 4 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 1 | 6 | 4 | 24 | 3 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | LT,1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 6 | 4 | 24 | 3 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 14 | 4 | 56 | 6 |
| | | EB | 2 | 14 | 4 | 56 | 6 |
| | | EB | 1 | 6 | 4 | 24 | 3 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 6 | 6 | 36 | 4 |
| | Medinah Road | EB | 1 | 6 | 3 | 18 | 2 |
| Medinah Road | Wednamica | EB | 2 | 12 | 3 | 36 | 4 |
| Meditati i toda | | EB | 2 | 3 | 50 | 150 | 17 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | | | 12 | 3 | | |
| | | EB | 2 | | | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 13 | 3 | 39 | 4 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 12 | 4 | 48 | 5 |
| | | EB | 2 | 14 | 4 | 56 | 6 |
| | | EB | 2 | 12 | 8 | 96 | 11 |
| | | EB | 2 | 12 | 3 | 36 | 4 |
| | | EB | 2 | 11 | 3 | 33 | 4 |
| | | EB | 2 | 11 | 3 | 33 | 4 |
| | Valley Lane | EB | 1,2 | 3 | 125 | 375 | 42 |
| Valley Lane | Valley Larie | EB | 1 | 12 | 3 | 36 | 4 |
| valley Latte | Manor Road | EB | 1 | 12 | 3 | 36 | 4 |
| Manor Road | IVIALIOI ROAU | EB | 1 | 12 | 3 | 36 | 4 |
| ויומווטו ולטמט | | | | | | | |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | N 1 151 | EB | 1 | 12 | 3 | 36 | 4 |
| | Newland Place | EB | 1 | 12 | 3 | 36 | 4 |
| Newland Place | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Gates Avenue | EB | 1 | 12 | 4 | 48 | 5 |
| Gates Avenue | | EB | 1 | 8 | 4 | 32 | 4 |
| | | EB | 1 | 8 | 4 | 32 | 4 |
| | | EB | CL | 3 | 80 | 240 | 27 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 3 | 20 | 60 | 7 |
| | | EB | 1 | 3 | 90 | 270 | 30 |
| | Hillcrest Avenue | EB | 1 | 9 | 4 | 36 | 4 |
| Hillcrest Avenue | Timorest Avenue | EB | 1 | 3 | 55 | 165 | 18 |
| | I | =0 | | | | | |
| Tilliorest Avenue | | ED | l 4 | 2 | 0.5 | 75 | |
| Timorest Avenue | | EB EB | 1 | 3 | 25 45 | 75 135 | 8 15 |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMITTENT RESURFACING SCHEDULE | F.A. | SECTION | COUNTY | TOTAL | SHEE | Г |
|---|------------------------------|-----------------|-----------|------------------------------|--------|------------------------------------|------|------------------|---------|-------|------|-----|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | II 40 | VAR. | 2015-030RS | DUPAGE | 44 | 28 | _ |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | IL 19 | | | CONTRAC | T NO. | 62A8 | , - |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/7/2015 | REVISED - | | SCALE: | SHEET 22 OF 29 SHEETS STA. TO STA. | | ILLINOIS FED. AI | | | | Τ |

| ROUTE: IL | L 19 (IL 53 to Barrington Road) | (Continued) |
|-----------|---------------------------------|-------------|
|-----------|---------------------------------|-------------|

| CROSS S | STDEET | DIRECTION | LANE | TDAV/EMENIT | PAVEMENT | REPAIR | REPAIR |
|------------------|------------------|-----------|-----------|-------------|----------|---------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| FROIVI | 10 | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | | EB | 1 | 12 | 3 | 36 | 4 |
| | Baker Drive | EB | 1 | 12 | 3 | 36 | 4 |
| Baker Drive | 261(6) 211(6 | EB | 1 | 10 | 3 | 30 | 3 |
| | | EB | 1,2 | 3 | 30 | 90 | 10 |
| | | EB | 2 | 8 | 3 | 24 | 3 |
| | Andrene Lane | EB | 2 | 8 | 30 | 240 | 27 |
| Andrene Lane | | WB | 1 | 6 | 20 | 120 | 13 |
| | | WB | 2 | 4 | 15 | 60 | 7 |
| | | WB | 2 | 6 | 30 | 180 | 20 |
| | | WB | 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | Baker Drive | WB | 1 | 12 | 3 | 36 | 4 |
| Baker Drive | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 6 | 20 | 120 | 13 |
| | Hillcrest Avenue | WB | 1 | 12 | 3 | 36 | 4 |
| Hillcrest Avenue | | WB | 1 | 12 | 7 | 84 | 9 |
| | | WB | 1 | 3 | 70 | 210 | 23 |
| | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 3 | 15 | 45 | 5 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 4 | 30 | 120 | 13 |
| | | WB | 1 | 8 | 4 | 32 | 4 |
| | Gates Avenue | WB | 1 | 3 | 40 | 120 | 13 |
| Gates Avenue | | WB | 1 | 6 | 20 | 120 | 13 |
| | | WB | 1 | 8 | 3 | 24 | 3 |
| | | WB | 1 | 8 | 3 | 24 | 3 |
| | | WB | 1 | 8 | 10 | 80 | 9 |
| | | WB | 1 | 8 | 3 | 24 | 3 |
| | | WB | 1 | 8 | 3 | 24 | 3 |
| | | WB | 1 | 3 | 25 | 75 | 8 |
| | Newland Place | WB | 1 | 9 | 3 | 27 | 3 |
| Newland Place | | WB | 1 | 4 | 30 | 120 | 13 |
| | | WB | 1 | 13 | 6 | 78 | 9 |
| | | WB | 1 | 13 | 3 | 39 | 4 |
| | | WB | 1 | 13 | 5 | 65 | 7 |
| | | WB | 1 | 13 | 3 | 39 | 4 |
| | | WB | 1 | 13 | 3 | 39 | 4 |
| | | WB | 1 | 3 | 100 | 300 | 33 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | Manor Road | WB | 1 | 10 | 3 | 30 | 3 |
| Manor Road | | WB | 1,2 | 3 | 55 | 165 | 18 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | Valley Lane | WB | 1 | 12 | 3 | 36 | 4 |

| ROUTE: IL 19 | (IL 53 to Barrington Road) | (Continued) |
|--------------|----------------------------|-------------|
| | | |

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|---------------|--------------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| Valley Lane | | WB | 1 | 12 | 3 | 36 | 4 |
| valley Larie | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 13 | 4 | 52 | 6 |
| | | WB | 1 | 3 | 45 | 135 | 15 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | | | | | | |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 6 | 3 | 18 | 2 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 10 | 120 | 13 |
| | | WB | 1,2 | 3 | 12 | 36 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | | WB | 1 | 6 | 3 | 18 | 2 |
| | | WB | 1 | 6 | 3 | 18 | 2 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 14 | 3 | 42 | 5 |
| | Medinah Road | WB | 2 | 13 | 3 | 39 | 4 |
| Medinah Road | Wiedman road | WB | 2 | 16 | 3 | 48 | 5 |
| Medinani Noau | | WB | 1 | 7 | 3 | 21 | 2 |
| | | | | | | | |
| | | WB | 2 | 13 | 3 | 39 | 4 |
| | | WB | 2 | 13 | 3 | 39 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 10 | 3 | 30 | 3 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | | | |
| | | | | | 4 | 48 | 5 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 8 | 4 | 32 | 4 |
| | | WB | 1,2 | 3 | 12 | 36 | 4 |
| | | WB | 2 | 7 | 4 | 28 | 3 |
| | | WB | 1,2 | 3 | 40 | 120 | 13 |
| | | WB | 2 | 6 | 4 | 24 | 3 |
| | | WB | 2 | 5 | 3 | 15 | 2 |
| | | WB | 1,2 | 3 | 50 | 150 | 17 |
| | | WB | 1 | 13 | 3 | 39 | 4 |
| | | WB | 1 | 14 | 6 | 84 | 9 |
| | Harvey Road | WB | 1 | 13 | 3 | 39 | 4 |
| Harvey Road | 1.2 | WB | 1 | 12 | 3 | 36 | 4 |
| riarvey riodu | | WB | 1 | 6 | 10 | 60 | 7 |
| | + | | | 3 | | 60 | 7 |
| | | WB | 1 | | 20 | | |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 3 | 15 | 45 | 5 |
| | | WB | 1 | 4 | 40 | 160 | 18 |
| | | WB | 1 | 12 | 5 | 60 | 7 |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMITTENT RESURFACING SCHEDULE | F.A. | SECTION | COUNTY | TOTAL | SHEET |
|---|-------------------------------|-----------------|-----------|------------------------------|--------|------------------------------------|------|------------------|---------|---------|-------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | II 10 | VAR. | 2015-030RS | DUPAGE | 44 | 29 |
| | PLOT SCALE = 100.0000 ' / in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | IL 19 | | | CONTRAC | T NO. F | 62A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/7/2015 | REVISED - | | SCALE: | SHEET 23 OF 29 SHEETS STA. TO STA. | | ILLINOIS FED. AI | | | |

| CROSS S | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|------------------|------------------|-----------|-----------|----------|----------|-----------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| 1110111 | 10 | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | WB | 1 | 3 | 35 | 105 | 12 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 3 | 20 | 60 | 7 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | Sycamore Avenue | WB | 1 | 12 | 3 | 36 | 4 |
| Sycamore Avenue | Sycamore Avenue | WB | 1 | 12 | 10 | 120 | 13 |
| Sycamore Avenue | | WB | 1 | 4 | 270 | 1080 | 120 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1,2 | 4 | 15 | 60 | 7 |
| | Springhill Drive | WB | 1,2 | 4 | 100 | 400 | 44 |
| Springhill Drive | Springhill Drive | WB | 1,2 | 4 | 30 | 120 | 13 |
| Spirigniii Drive | | WB | 2 | 12 | 30 4 | 48 | 5 |
| | | WB | | | | | |
| | | WB | 2 | 4 12 | 40 | 160 96 | 18 |
| | | | | | 8 | | 11 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 13 | 35 | 455 | 51 |
| | | WB | 1,2 | 4 | 15 | 60 | 7 |
| | | WB | 2 | 12 | 200 | 2400 | 267 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 30 | 120 | 13 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 15 | 60 | 7 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 50 | 200 | 22 |
| | <u></u> | WB | 2 | 4 | 50 | 200 | 22 |
| D | Pinecroft Drive | WB | 1,2 | 4 | 45 | 180 | 20 |
| Pinecroft Drive | | WB | 1,2 | 4 | 95 | 380 | 42 |
| | | WB | 2 | 4 | 50 | 200 | 22 |
| | | WB | 2 | 12 | 45 | 540 | 60 |
| | | WB | 2 | 4 | 25 | 100 | 11 |
| | | WB | 2 | 12 | 30 | 360 | 40 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 13 | 70 | 910 | 101 |
| | | WB | 2 | 13 | 20 | 260 | 29 |
| | | WB | 1 | 11 | 4 | 44 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 1,2 | 4 | 10 | 40 | 4 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1,2 | 4 | 15 | 60 | 7 |
| | | WB | 2 | 10 | 4 | 40 | 4 |
| | | WB | 2 | 4 | 6 | 24 | 3 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1,2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 12 | 4 | 48 | 5 |

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-----------------|------------------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | WB | 2 | 4 | 15 | 60 | 7 |
| | | WB | LT | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | LT | 12 | 4 | 48 | 5 |
| | | | | | | | |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 75 | 300 | 33 |
| | | WB | 2 | 4 | 55 | 220 | 24 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | Maple Avenue | WB | 2 | 12 | 4 | 48 | 5 |
| Maple Avenue | Maple Avenue | WB | 2 | 4 | 20 | 80 | 9 |
| Maple Avenue | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 140 | 560 | 62 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 75 | 300 | 33 |
| | | WB | 1 | 12 | 40 | 480 | 53 |
| | | WB | 2 | 12 | 40 | 480 | 53 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 13 | 25 | 325 | 36 |
| | | WB | 1,2 | 4 | 10 | 40 | 4 |
| | | WB | 1,2 | 12 | 4 | 48 | 5 |
| | | | | | | | |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 20 | 240 | 27 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 45 | 180 | 20 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | Lawrence Avenue | WB | 2 | 12 | 4 | 48 | 5 |
| Lawrence Avenue | Lawrence / Wende | WB | 2 | 4 | 10 | 40 | 4 |
| Lawrence Avenue | | WB | 2 | 8 | 6 | 48 | 5 |
| | | | | | | | |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 8 | 32 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 65 | 260 | 29 |
| | | WB | 1 | 12 | 6 | 72 | 8 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 25 | 100 | 11 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 4 | 10 | 40 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 8 | 96 | 11 |
| | | | | | | | |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 4 | 10 | 40 | 4 |
| | i | WB | 2 | 13 | 6 | 78 | 9 |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMITTENT RESURFACING SCHEDULE | F.A. RTF. | SECTION | COUNTY | TOTAL SHEET |
|---|-------------------------------|-----------------|-----------|------------------------------|--------|------------------------------------|---------------------------|------------|--------|-------------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | II 40 | VAR. | 2015-030RS | DUPAGE | 44 30 |
| | PLOT SCALE = 100.0000 ' / in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | IL 19 | | | | | T NO. 62A84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/7/2015 | REVISED - | | SCALE: | SHEET 24 OF 29 SHEETS STA. TO STA. | ILLINOIS FED. AID PROJECT | | | |

| ROUTE: IL | L 19 (IL 53 to Barrington Road) | (Continued) |
|-----------|---------------------------------|-------------|
|-----------|---------------------------------|-------------|

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-----------------|-----------------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| 11.0111 | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | WB | 1,2 | 6 | 30 | 180 | 20 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1,2 | 4 | 8 | 32 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 4 | 15 | 60 | 7 |
| | | WB | 2 | 4 | 10 | 40 | 4 |
| | | WB | 1,2 | 4 | 10 | 40 | 4 |
| | Park Street | WB | 1,2 | 4 | 25 | 100 | 11 |
| Park Street | | WB | 2 | 12 | 15 | 180 | 20 |
| | Park Street | WB | 2 | 12 | 4 | 48 | 5 |
| Park Street | | WB | 1 | 14 | 6 | 84 | 9 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | Prospect Street | WB | 2 | 12 | 4 | 48 | 5 |
| Prospect Street | | WB | 1 | 12 | 4 | 48 | 5 |
| | Prospect Street | WB | 2 | 12 | 4 | 48 | 5 |
| Prospect Street | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | LT | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | Bokelman Street | WB | 2 | 12 | 4 | 48 | 5 |
| Bokelman Street | Bokelman Street | WB | 1 | 13 | 4 | 52 | 6 |
| Bokelman Street | | WB | 1 | 11 | 4 | 44 | 5 |
| | | WB | 2 | 13 | 4 | 52 | 6 |
| | | WB | 2 | 4 | 10 | 40 | 4 |
| | | WB | LT,1 | 18 | 4 | 72 | 8 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | LT | 12 | 4 | 48 | 5 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | Roselle Road | WB | 2 | 12 | 4 | 48 | 5 |
| Roselle Road | Roselle Road | WB | 1 | 12 | 12 | 144 | 16 |
| Roselle Road | | WB | 1 | 6 | 3 | 18 | 2 |
| | | WB | 1 | 19 | 3 | 57 | 6 |
| | | WB | 1 | 3 | 25 | 75 | 8 |
| | | WB | 1 | 19 | 3 | 57 | 6 |
| | | WB | 1 | 19 | 3 | 57 | 6 |
| | | WB | 1 | 3 | 8 | 24 | 3 |
| | | WB | 1 | 6 | 3 | 18 | 2 |
| | Marion Street | WB | 1 | 3 | 20 | 60 | 7 |
| Marion Street | | WB | 1 | 7 | 3 | 21 | 2 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 1 | 3 | 6 | 18 | 2 |
| | 1 | WB | 1 | 10 | 3 | 30 | 3 |
| 11. 7. 2 | Lincoln Street | WB | 1 | 6 | 6 | 36 | 4 |
| Lincoln Street | Lincoln Street | NB | 1 | 15 | 3 | 45 | 5 |
| Lincoln Street | | WB | 1 | 12 | 3 | 36 | 4 |
| | VACUE OF C | WB | 1 | 14 | 3 | 42 | 5 |
| 14.500 | Williams Street | WB | 1 | 14 | 3 | 42 | 5 |
| Williams Street | | WB | 1 | 14 | 3 | 42 | 5 |
| | | WB | 1 | 14 | 3 | 42 | 5 |

| ROUTE. [IL 19 (IL 33 to Bailington Road) (Continued) | ROUTE: IL 19 | (IL 53 to Barrington Road) | (Continued) |
|--|--------------|----------------------------|-------------|
|--|--------------|----------------------------|-------------|

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|----------------------|---------------------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | WB | 1 | 14 | 4 | 56 | 6 |
| | | WB | 1 | 14 | 3 | 42 | 5 |
| | | WB | 1 | 14 | 3 | 42 | 5 |
| | RR Tracks | WB | 1 | 14 | 10 | 140 | 16 |
| RR Tracks | Mitchell Boulevard | WB | 1 | 12 | 3 | 36 | 4 |
| Mitchell Boulevard | Witterien Bodievard | WB | 1 | 14 | 3 | 42 | 5 |
| Willonell Doulevalu | <u> </u> | WB | 1 | 14 | 3 | 42 | 5 |
| | - | WB | 1 | 14 | 3 | 42 | 5 |
| | Cook County Line | | | | 3 | | |
| On all On water Live | Cook County Line | WB | 1 | 14 | | 42 | 5 |
| Cook County Line | | WB | 1 | 14 | 4 | 56 | 6 |
| | | WB | 1 | 3 | 40 | 120 | 13 |
| | | WB | 1 | 3 | 25 | 75 | 8 |
| | | WB | 1 | 8 | 3 | 24 | 3 |
| | | WB | 1 | 14 | 3 | 42 | 5 |
| | | WB | 1 | 14 | 3 | 42 | 5 |
| | | WB | 1 | 6 | 10 | 60 | 7 |
| | Wright Boulevard | WB | 1 | 6 | 30 | 180 | 20 |
| Mercury Drive | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | LT,1 | 3 | 65 | 195 | 22 |
| | | WB | LT | 12 | 3 | 36 | 4 |
| | Wise Road | WB | 1,2 | 3 | 35 | 105 | 12 |
| Wise Road | 111121111111 | WB | RAD | 9 | 3 | 27 | 3 |
| | | WB | 2 | 3 | 10 | 30 | 3 |
| | | WB | 1,2 | 3 | 10 | 30 | 3 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | Olde Salem Circle | WB | 2 | 12 | 3 | 36 | 4 |
| Olde Salem Circle | Olde Salem Circle | WB | 1,2 | 3 | 15 | 45 | 5 |
| Olde Salem Circle | | WB | | 8 | | | |
| | - | | LT | | 3 | 24 | 3 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | LT | 10 | 3 | 30 | 3 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | LT,1 | 3 | 80 | 240 | 27 |
| | | WB | 2 | 4 | 20 | 80 | 9 |
| | | WB | 2 | 9 | 3 | 27 | 3 |
| | Orchard Lane | WB | 2 | 9 | 3 | 27 | 3 |
| Orchard Lane | | WB | 2 | 6 | 4 | 24 | 3 |
| | | WB | 2 | 9 | 3 | 27 | 3 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 6 | 3 | 18 | 2 |
| | | WB | 2 | 6 | 4 | 24 | 3 |
| | <u> </u> | WB | 1 | 12 | 7 | 84 | 9 |
| | 1 | WB | 2 | 12 | 7 | 84 | 9 |
| | + | WB | 1 | 9 | 3 | 27 | 3 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | - | WB | 2 | 12 | 3 | 36 | 4 |
| | + | | | | | | |
| | Oldo Calarra Danii | WB | 2 | 3 | 55 | 165 | 18 |
| Objective D. J. | Olde Salem Road | WB | 2 | 12 | 3 | 36 | 4 |
| Olde Salem Road | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 2 | 13 | 4 | 52 | 6 |
| | | WB | 2 | 12 | 7 | 84 | 9 |
| | | WB | 2 | 3 | 40 | 120 | 13 |
| | | WB | 12 | 12 | 4 | 48 | 5 |
| | Northway Drive | WB | 2 | 3 | 10 | 30 | 3 |
| N. () D.: | , | NB | 1 | 15 | 50 | 750 | 83 |
| Northway Drive | | IND | | 13 | J 50 I | 750 | 1 00 |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMIT | TENT RE | SURFAC | ING SCHEDU | IJI F | F.A. RTF. | SECTION | COUNTY | TOTAL SHEE | Ŧ |
|---|------------------------------|-----------------|-----------|------------------------------|--------|----------|---------|--------|------------|---------|--------------|---------------|-------------|-------------|---|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | | | 11 40 | ind comes | | VAR. | 2015-030RS | DUPAGE | 44 31 | ┨ |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | | ı | IL 19 | | | | | CONTRACT | T NO. 62A84 | 4 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/7/2015 | REVISED - | | SCALE: | SHEET 25 | OF 29 | SHEETS | STA. | TO STA. | | ILLINOIS FED. | AID PROJECT | | |

ROUTE: IL 19 (IL 53 to Barrington Road) (Continued)

| CROSS | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|------------------|------------------|-----------|-----------|----------|----------|---------|---------|
| FROM | ТО | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WDTH | LENGTH | (SQ FT) | (SQ YD) |
| Northway Drive | | WB | 2 | 12 | 4 | 48 | 5 |
| , | | WB | 1 | 12 | 3 | 36 | 4 |
| | Edgebrook Lane | WB | 1 | 6 | 6 | 36 | 4 |
| Edgebrook Lane | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | LT,1 | 3 | 50 | 150 | 17 |
| | | WB | LT,1 | 3 | 15 | 45 | 5 |
| | Longmeadow Lane | WB | LT,1 | 3 | 15 | 45 | 5 |
| Longmeadow Lane | Longineacow Lane | WB | 1 | 6 | 4 | 24 | 3 |
| Longineadow Lane | | WB | 2 | 6 | 3 | 18 | 2 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | Cumberland Drive | WB | 2 | 3 | 55 | 165 | 18 |
| Cumberland Drive | Cumberiand Drive | WB | 2 | 12 | 3 | 36 | 4 |
| Cumbenatio Diffe | | WB | 2 | 6 | 6 | 36 | 4 |
| | | WB | 2 | 3 | 30 | 90 | 10 |
| | | WB | 2 | 12 | 30 | 36 | 4 |
| | | WB | | 3 | 60 | 180 | 20 |
| | | WB | 2 | 12 | 3 | 36 | |
| | | | 1 | | | | 4 |
| | _ | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 3 | 12 | 36 | 4 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 3 | 25 | 75 | 8 |
| | | WB | 2 | 3 | 25 | 75 | 8 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | Kingsbury Drive | WB | 2 | 3 | 85 | 255 | 28 |
| Kingsbury Drive | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 3 | 70 | 210 | 23 |
| | | WB | 1 | 6 | 4 | 24 | 3 |
| | | WB | 2 | 3 | 12 | 36 | 4 |
| | | WB | 1 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 12 | 5 | 60 | 7 |
| | | WB | 2 | 3 | 12 | 36 | 4 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 3 | 100 | 300 | 33 |
| | | WB | 1 | 12 | 3 | 36 | 4 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 14 | 5 | 70 | 8 |
| | | WB | 2 | 3 | 12 | 36 | 4 |
| | | WB | 2 | 13 | 4 | 52 | 6 |
| | | WB | 2 | 3 | 10 | 30 | 3 |
| | | WB | 2 | 12 | 3 | 36 | 4 |
| | | WB | 2 | 3 | 12 | 36 | 4 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 12 | 4 | 48 | 5 |
| | | WB | 2 | 3 | 20 | 60 | 7 |
| | | WB | 2 | 3 | 55 | 165 | 18 |
| | | WB | 1 | 12 | 4 | 48 | 5 |
| | Barrington Road | WB | 2 | 9 | 4 | 36 | 4 |
| | | | | | | | |
| | | | | | | | |

TOTALS: 11321 7873 FT SY

ROUTE: York Road (IL 38 to North of I-88)

| CROSS S | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|---------------|---------------|-----------|-----------|----------|----------|---------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| North of I-88 | | NB | 1 | 12 | 8 | 96 | 11 |
| | | NB | 2 | 12 | 8 | 96 | 11 |
| | | NB | 1 | 12 | 24 | 288 | 32 |
| | | NB | 2 | 12 | 24 | 288 | 32 |
| | | NB | 1 | 12 | 4 | 48 | 5 |
| | | NB | 2 | 12 | 4 | 48 | 5 |
| | | NB | 1 | 12 | 4 | 48 | 5 |
| | | NB | 2 | 12 | 4 | 48 | 5 |
| | | NB | 1 | 12 | 4 | 48 | 5 |
| | | NB | 2 | 12 | 4 | 48 | 5 |
| | | NB | 1 | 12 | 4 | 48 | 5 |
| | | NB | 2 | 12 | 4 | 48 | 5 |
| | | NB | 1 | 12 | 6 | 72 | 8 |
| | | NB | 2 | 12 | 6 | 72 | 8 |
| | | NB | 1 | 12 | 6 | 72 | 8 |
| | | NB | 2 | 12 | 6 | 72 | 8 |
| | | NB | 2 | 12 | 4 | 48 | 5 |
| | | NB | 1 | 12 | 4 | 48 | 5 |
| | | NB | 2 | 12 | 4 | 48 | 5 |
| | | NB | 1 | 12 | 4 | 48 | 5 |
| | IL 38 | NB | 2 | 12 | 4 | 48 | 5 |
| IL 38 | | SB | 1 | 12 | 4 | 48 | 5 |
| | | SB | 1 | 12 | 4 | 48 | 5 |
| | | SB | 2 | 12 | 4 | 48 | 5 |
| | | SB | 1 | 12 | 4 | 48 | 5 |
| | | SB | 2 | 12 | 4 | 48 | 5 |
| | | SB | 1 | 12 | 4 | 48 | 5 |
| | | SB | 2 | 12 | 4 | 48 | 5 |
| | | SB | 2 | 12 | 4 | 48 | 5 |
| | | SB | 1 | 12 | 4 | 48 | 5 |
| | | SB | 2 | 12 | 4 | 48 | 5 |
| | | SB | 1 | 12 | 4 | 48 | 5 |
| | | SB | 2 | 12 | 4 | 48 | 5 |
| | | SB | 1 | 12 | 4 | 48 | 5 |
| | | SB | 2 | 12 | 4 | 48 | 5 |
| | | SB | 1 | 12 | 4 | 48 | 5 |
| | | SB | 2 | 12 | 4 | 48 | 5 |
| | | SB | 1 | 12 | 8 | 96 | 11 |
| | | SB | 1 | 12 | 4 | 48 | 5 |
| | North of I-88 | SB | 2 | 12 | 4 | 48 | 5 |
| | | | | | | | |

TOTALS: 220 293 FT SY

| FILE NAME = | USER NAME = bartonrw | DESIGNED | - | RWB | REVISED - |
|---|------------------------------|----------|---|----------|-----------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN | - | RWB | REVISED - |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED | - | | REVISED - |
| Default | PLOT DATE = 4/7/2015 | DATE | - | 4/7/2015 | REVISED - |

| STATI | E 01 | F ILLINOIS |
|------------|------|----------------|
| DEPARTMENT | OF | TRANSPORTATION |

| | INTERMIT | TENT R | ESURFA | CING SCH | HEDULE | F.A. RTE. | SECT | ION | | COUNTY | TOTAL SHEETS | SHEE NO. |
|--------|-------------------|---------|--------|----------|---------|--------------|------|----------|---------|-----------|-----------------|-------------|
| | IL 19 / YORK ROAD | | | | VAR. | 2015-030RS | | | DUPAGE | 44 | 32 | |
| | | IL 13 / | | | | | | | | CONTRACT | NO. | 62A84 |
| SCALE: | SHEET 26 | OF 29 | SHEETS | STA. | TO STA. | | | ILLINOIS | FED. AI | D PROJECT | | |

ROUTE: IL 56 (Ramps at IL 83)

| CROSS | STREET | DIRECTION | LANE | DAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-----------|------------|-----------|-------------|----------|----------|------------|----------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| FROW | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| SB IL 83 | | EB | Ramp | 18 | 4 | 72 | 8 |
| 3B IE 03 | | EB | Ramp | 18 | 4 | 72 | 8 |
| | | EB | Ramp | 18 | 4 | 72 | 8 |
| | | EB | Ramp | 18 | 4 | 72 | 8 |
| | | EB | Ramp | 18 | 4 | 72 | 8 |
| | | EB | Ramp | 18 | 4 | 72 | 8 |
| | | EB | Ramp | 18 | 4 | 72 | 8 |
| | | EB | Ramp | 18 | 4 | 72 | 8 |
| | | EB | Ramp | 18 | 4 | 72 | 8 |
| | | EB | Ramp | 18 | 6 | 108 | 12 |
| | | EB | | 3 | 460 | 1380 | 153 |
| | | EB | Ramp | 18 | 15 | 270 | 30 |
| | | EB | Ramp | 18 | 4 | 72 | 8 |
| | | | Ramp | | | | |
| | | EB EB | Ramp | 3 18 | 260 6 | 780 108 | 87 12 |
| | | EB | Ramp | 18 | 4 | 72 | 8 |
| | | | Ramp | | | | |
| | | EB | Ramp | 18 | 4 | 72 | 8 |
| | | EB | Ramp | 18 | 8 | 144 | 16 |
| | | EB | Ramp | 31 | 64 | 1984 | 220 |
| | | EB | Ramp | 31 | 24 | 744 | 83 |
| | | EB | Ramp | 31 | 4 | 124 | 14 |
| | | EB | Ramp | 18 | 30 | 540 | 60 |
| 0.5 !! 00 | EB IL 56 | EB | Ramp | 18 | 4 | 72 | 8 |
| SB IL 83 | | WB | Ramp | 19 | 20 | 380 | 42 |
| | | WB | Ramp | 19 | 4 | 76 | 8 |
| | | WB | Ramp | 19 | 4 | 76 | 8 |
| | | WB | Ramp | 19 | 4 | 76 | 8 |
| | | WB | Ramp | 19 | 4 | 76 | 8 |
| | | WB | Ramp | 19 | 4 | 76 | 8 |
| | | WB | Ramp | 19 | 10 | 190 | 21 |
| | 1115 11 50 | WB | Ramp | 19 | 6 | 114 | 13 |
| | WB IL 56 | WB | Ramp | 19 | 6 | 114 | 13 |
| EB IL 56 | | NB | Ramp | 19 | 10 | 190 | 21 |
| | | NB | Ramp | 19 | 6 | 114 | 13 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 6 | 114 | 13 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 6 | 114 | 13 |
| | | NB | Ramp | 19 | 10 | 190 | 21 |
| | | NB | Ramp | 19 | 6 | 114 | 13 |
| | | NB | Ramp | 19 | 10 | 190 | 21 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 6 | 114 | 13 |
| | | NB | Ramp | 19 | 30 | 570 | 63 |
| | NB IL 83 | NB | Ramp | 19 | 4 | 76 | 8 |
| | | | | | | | |

TOTALS: 1109 1171 FT SY

ROUTE: IL 38 (Ramps at IL 83 and WB IL 56)

| CROSS S | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-----------|----------|-----------|-----------|----------|----------|-----------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| T TOWN | 10 | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| EB IL 38 | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | | 19 | 4 | 76 | 8 |
| | | | Ramp | | | 76 | |
| | | NB NB | Ramp | 19 19 | 4 | 76 76 | 8 |
| - | | | Ramp | 19 | 4 | 76 76 | 8 |
| | | NB | Ramp | | 4 | | 8 |
| | | NB | Ramp | 19 | 4 | 76 76 | 8 |
| | ND " 00 | NB | Ramp | 19 | 4 | 76 450 | 8 |
| 0.5 11 00 | NB IL 83 | NB | Ramp | 19 | 8 | 152 | 17 |
| SB IL 83 | | WB | Ramp | 19 | 4 | 76 | 8 |
| | | WB | Ramp | 19 | 4 | 76 | 8 |
| | | WB | Ramp | 19 | 15 | 285 | 32 |
| | | WB | Ramp | 19 | 4 | 76 | 8 |
| | | WB | Ramp | 19 | 6 | 114 | 13 |
| | | WB | Ramp | 19 | 6 | 114 | 13 |
| | | WB | Ramp | 19 | 6 | 114 | 13 |
| | | WB | Ramp | 19 | 6 | 114 | 13 |
| | | WB | Ramp | 19 | 6 | 114 | 13 |
| | | WB | Ramp | 19 | 6 | 114 | 13 |
| | | WB | Ramp | 19 | 10 | 190 | 21 |
| | | WB | Ramp | 19 | 6 | 114 | 13 |
| | | WB | Ramp | 19 | 6 | 114 | 13 |
| | | WB | Ramp | 19 | 6 | 114 | 13 |
| | | WB | Ramp | 19 | 8 | 152 | 17 |
| | | WB | Ramp | 19 | 8 | 152 | 17 |
| | | WB | Ramp | 19 | 8 | 152 | 17 |
| | | WB | Ramp | 19 | 6 | 114 | 13 |
| | | WB | Ramp | 19 | 6 | 114 | 13 |
| | | WB | Ramp | 19 | 4 | 76 | 8 |
| | | WB | Ramp | 19 | 4 | 76 | 8 |
| | | WB | Ramp | 19 | 8 | 152 | 17 |
| | | WB | Ramp | 19 | 4 | 76 | 8 |
| | | WB | Ramp | 19 | 4 | 76 | 8 |
| | | WB | Ramp | 19 | 12 | 228 | 25 |
| | | WB | Ramp | 19 | 15 | 285 | 32 |
| | WB IL 38 | WB | Ramp | 3 | 100 | 300 | 33 |
| EB IL 38 | | SB | Ramp | 19 | 20 | 380 | 42 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 6 | 114 | 13 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | | | | | | |

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - | | | INTERMIT | TENT RESU | RFACING S | SCHEDULE | F.A. RTF. | SECTION | COUNTY | TOTAL SH | IEET |
|---|------------------------------|-----------------|-----------|------------------------------|--------|----------|-----------|-----------|-----------|--------------|---------------|-------------|-----------|--------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN - RWB | REVISED - | STATE OF ILLINOIS | | | | IL 38 | 301123022 | VAR. | 2015-030RS | DUPAGE | 44 | 33 |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | | | IL 30 / | IL 30 | | | | CONTRAC | T NO. 62A | .84 |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/7/2015 | REVISED - | | SCALE: | SHEET 27 | 0F 29 SH | EETS STA. | TO STA. | | ILLINOIS FED. | AID PROJECT | | \neg |

ROUTE: IL 38 (Ramps at IL 83 and WB IL 56) (Continued)

| CROSS STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|--------------|-----------|--------------|----------|----------|-----------|---------|
| FROM TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | SB | Ramp | 19 | 4 | 76 | 8 |
| | SB | Ramp | 19 | 4 | 76 | 8 |
| | SB | Ramp | 19 | 4 | 76 | 8 |
| SB IL 83 | SB | Ramp | 19 | 4 | 76 | 8 |
| NB IL 83 | WB | Ramp | 19 | 30 | 570 | 63 |
| 140 12 00 | WB | Ramp | 19 | 24 | 456 | 51 |
| | WB | Ramp | 19 | 4 | 76 | 8 |
| | WB | Ramp | 19 | 6 | 114 | 13 |
| | WB | Ramp | 19 | 4 | 76 | 8 |
| | WB | Ramp | 19 | 6 | 114 | 13 |
| | WB | Ramp | 19 | 8 | 152 | 17 |
| | WB | Ramp | 19 | 4 | 76 | 8 |
| | WB | Ramp | 19 | 8 | 152 | 17 |
| | WB | Ramp | 19 | 8 | 152 | 17 |
| | WB | | 19 | 6 | 114 | 13 |
| | | Ramp | | 4 | 76 | |
| | WB WB | Ramp | 19 19 | 4 | 76 76 | 8 |
| | WB | Ramp | 19 | 4 | 76 76 | 8 |
| | | Ramp | | | | |
| | WB | Ramp | 19 | 8 | 152 | 17 |
| 1M/D II 00 | WB | Ramp | 19 | 14 | 266 | 30 |
| WB IL 38 | WB | Ramp | 19 | 8 | 152 | 17 |
| WB IL 38 | SB | Ramp | 19 | 4 | 76 | 8 |
| | SB | Ramp | 19 | 6 | 114 | 13 |
| | SB | Ramp | 19 | 6 | 114 | 13 |
| | SB | Ramp | 19 | 4 | 76 | 8 |
| | SB | Ramp | 19 | 6 | 114 | 13 |
| | SB | Ramp | 19 | 4 | 76 | 8 |
| | SB | Ramp | 19 | 4 | 76 | 8 |
| | SB | Ramp | 19 | 4 | 76 | 8 |
| | SB | Ramp | 19 | 4 | 76 | 8 |
| | SB | Ramp | 19 | 10 | 190 | 21 |
| | SB | Ramp | 19 | 4 | 76 | 8 |
| | SB | Ramp | 19 | 8 | 152 | 17 |
| | SB | Ramp | 19 | 4 | 76 | 8 |
| | SB | Ramp | 19 | 8 | 152 | 17 |
| | SB | Ramp | 19 | 8 | 152 | 17 |
| | SB | Ramp | 19 | 12 | 228 | 25 |
| | SB | Ramp | 19 | 10 | 190 | 21 |
| | SB | Ramp | 19 | 24 | 456 | 51 |
| | SB | Ramp | 19 | 4 | 76 | 8 |
| SB IL 83 | SB | Ramp | 19 | 4 | 76 | 8 |
| WB IL 56 | WB | Ramp | 19 | 4 | 76 | 8 |
| | WB | Ramp | 19 | 20 | 380 | 42 |
| | WB | Ramp | 19 | 10 | 190 | 21 |
| | WB | Ramp | 19 | 40 | 760 | 84 |
| | WB | Ramp | 19 | 30 | 570 | 63 |
| | WB | Ramp | 19 | 4 | 76 | 8 |
| | WB | Ramp | 19 | 4 | 76 | 8 |
| WB IL 38 | WB | Ramp | 19 | 10 | 190 | 21 |
| WB IL 38 | NB | Ramp | 19 | 6 | 114 | 13 |
| | NB | Ramp | 19 | 4 | 76 | 8 |
| | NB | Ramp | 19 | 4 | 76 | 8 |
| | NB | Ramp | 19 | 20 | 380 | 42 |
| | NB | Ramp | 19 | 6 | 114 | 13 |
| | | | | | | |
| | NB NB | Ramp Ramp | 19 19 | 4 | 76 190 | 8 21 |

| ROUTE: IL 38 (Ramps at IL 83 and WB IL 56 | (Continued) |
|---|-------------|
| | |

| CROSS S | CROSS STREET | | | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|---------|--------------|---------|-----------|----------|----------|---------|---------|
| FROM | FROM TO | | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| | | NB | Ramp | 19 | 10 | 190 | 21 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | | NB | Ramp | 19 | 20 | 380 | 42 |
| | NB IL 83 | NB | Ramp | 19 | 30 | 570 | 63 |
| | | | | | | | |

TOTALS: 960 1849 FT SY

ROUTE: US 20 (Ramps at IL 83)

| CROSS S | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|----------|----------|-----------|-----------|----------|----------|---------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| NB IL 83 | | NB | Ramp | 19 | 6 | 114 | 13 |
| | | NB | Ramp | 19 | 6 | 114 | 13 |
| | | NB | Ramp | 19 | 4 | 76 | 8 |
| | US 20 | NB | Ramp | 19 | 24 | 456 | 51 |
| US 20 | | NB | Ramp | 19 | 24 | 456 | 51 |
| | NB IL 83 | NB | Ramp | 19 | 4 | 76 | 8 |
| SB IL 83 | | SB | Ramp | 19 | 18 | 342 | 38 |
| | | SB | Ramp | 19 | 12 | 228 | 25 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | US 20 | SB | Ramp | 19 | 8 | 152 | 17 |
| US 20 | | SB | Ramp | 19 | 8 | 152 | 17 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 4 | 76 | 8 |
| | | SB | Ramp | 19 | 6 | 114 | 13 |
| | SB IL 83 | SB | Ramp | 19 | 6 | 114 | 13 |
| | | | | | | | |

TOTALS: 206 435 FT SY

| FILE NAME = | USER NAME = bartonrw | DESIGNED - RWB | REVISED - |
|--|-------------------------------|-----------------|-----------|
| c:\pw_work\pwidot\bartonrw\d0427922\HM | -DuPage.dgn | DRAWN - RWB | REVISED - |
| | PLOT SCALE = 100.00000 '/ in. | CHECKED - | REVISED - |
| Default | PLOT DATE = 4/7/2015 | DATE - 4/7/2015 | REVISED - |

| STAT | E OF | ILLINOIS | |
|------------|------|-----------|-------|
| DEPARTMENT | OF | TRANSPORT | ATION |

| | INTERMIT | TENT R | ESURFACI | ING SCH | EDULE | F | R. |
|-------|----------|--------|----------|---------|---------|---|----|
| | | 11 3 | 8 / US 2 | n | | | ٧. |
| | | 11. | 0 / 00 2 | | | | |
| CALE: | SHEET 28 | OF 29 | SHEETS | STA. | TO STA. | | _ |

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------|------------------|------------|-----------------|--------------|
| VAR. | 2015-030RS | DUPAGE | 44 | 34 |
| | | CONTRACT | NO. 6 | 2A84 |
| | ILL INDIS FED. A | ID PROJECT | | |

ROUTE: 31st Street (Ramps at IL 83)

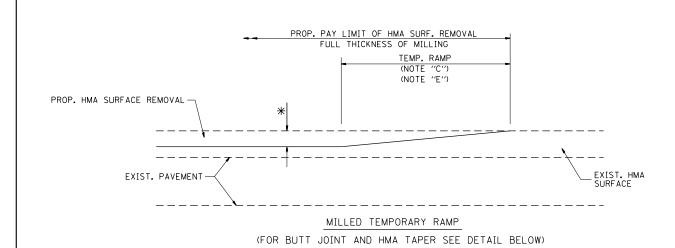
| CROSS S | STREET | DIRECTION | LANE | PAVEMENT | PAVEMENT | REPAIR | REPAIR |
|-------------|-------------|-----------|-----------|----------|----------|---------|---------|
| FROM | TO | (EB/WB) | NO. | PATCH | PATCH | AREA | AREA |
| | | (NB/SB) | (1, 2, 3) | WIDTH | LENGTH | (SQ FT) | (SQ YD) |
| SB IL 83 | | SB | Ramps | 18 | 6 | 108 | 12 |
| | | SB | Ramps | 30 | 6 | 180 | 20 |
| | | SB | Ramps | 18 | 6 | 108 | 12 |
| | | SB | Ramps | 30 | 6 | 180 | 20 |
| | | SB | Ramps | 18 | 6 | 108 | 12 |
| | | SB | Ramps | 30 | 6 | 180 | 20 |
| | | SB | Ramps | 18 | 6 | 108 | 12 |
| | | SB | Ramps | 18 | 6 | 108 | 12 |
| | | SB | Ramps | 18 | 6 | 108 | 12 |
| | | SB | Ramps | 18 | 6 | 108 | 12 |
| | | SB | Ramps | 18 | 6 | 108 | 12 |
| | | SB | Ramps | 18 | 6 | 108 | 12 |
| | 31st Street | SB | Ramps | 18 | 6 | 108 | 12 |
| 31st Street | | SB | Ramps | 18 | 6 | 108 | 12 |
| | SB IL 83 | SB | Ramps | 18 | 6 | 108 | 12 |
| 31st Street | | NB | Ramps | 18 | 6 | 108 | 12 |
| | | NB | Ramps | 18 | 6 | 108 | 12 |
| | | NB | Ramps | 18 | 6 | 108 | 12 |
| | | NB | Ramps | 18 | 6 | 108 | 12 |
| | | NB | Ramps | 18 | 6 | 108 | 12 |
| | | NB | Ramps | 18 | 6 | 108 | 12 |
| | | NB | Ramps | 18 | 6 | 108 | 12 |
| | NB IL 83 | NB | Ramps | 18 | 6 | 108 | 12 |
| NB IL 83 | | NB | Ramps | 18 | 6 | 108 | 12 |
| | | NB | Ramps | 18 | 6 | 108 | 12 |
| | 31st Street | NB | Ramps | 18 | 6 | 108 | 12 |
| | | | | | | | |

TOTALS: 156 336 FT SY

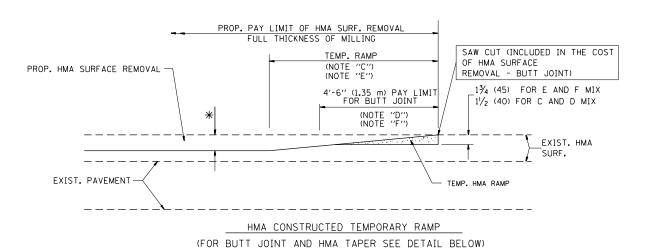
| FILE NAME = | USER NAME = bartonrw | DESIGNED | - | RWB | REVISED - |
|---|-------------------------------|----------|---|----------|-----------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -DuPage.dgn | DRAWN | - | RWB | REVISED - |
| | PLOT SCALE = 100.0000 ' / in. | CHECKED | - | | REVISED - |
| Default | PLOT DATE = 4/7/2015 | DATE | - | 4/7/2015 | REVISED - |

| STATE OF ILLINOIS | |
|------------------------------|--|
| DEPARTMENT OF TRANSPORTATION | |

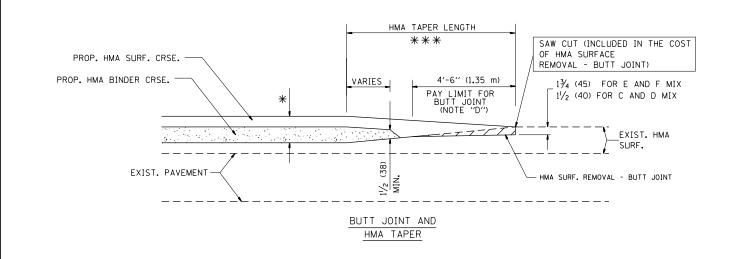
| | INTERMITTENT RESURFACING SCHEDULE | | | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|-----------------------------------|----|----|--------|------|------------|--------------|-------------------|------------|-----------------|--------------|
| 31ST STREET | | | | | VAR. | 2015-030RS | DUPAGE | 44 | 35 | | |
| | JIJI JIIILLI | | | | | | | | CONTRAC | T NO. 6 | 2A84 |
| | SHEET 29 | ΩF | 29 | SHEETS | STA. | TO STA | | THE INOIS FED. AT | n ppn iect | | |



OPTION 1



OPTION 2 TYPICAL TEMPORARY RAMP

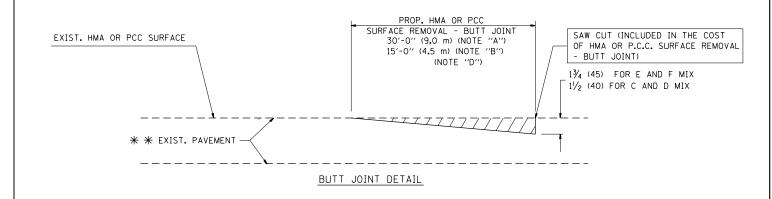


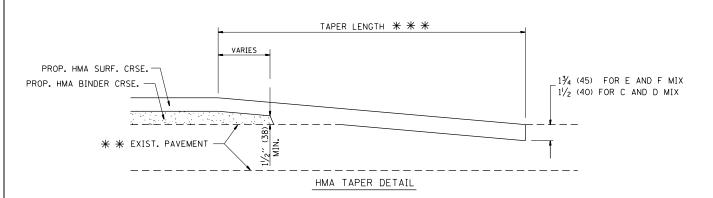
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS

OTHERWISE SHOWN.





TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

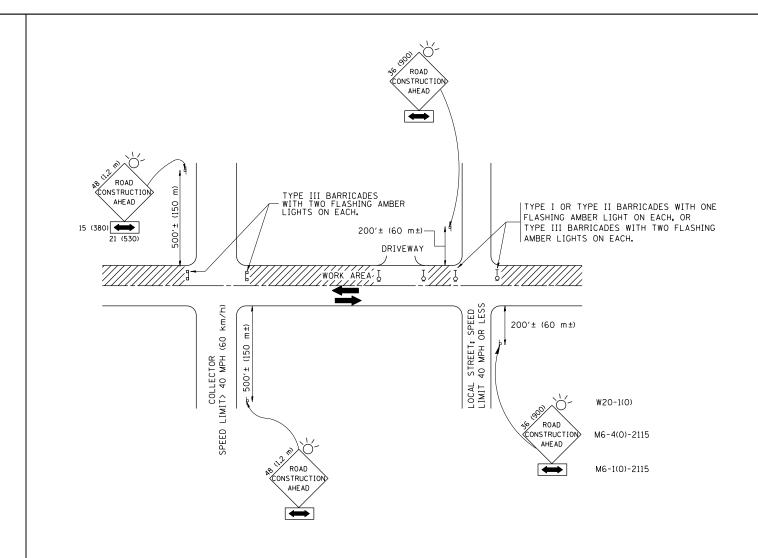
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER)
FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

SCALE: NONE



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- 0) ONE ROAD CONSTRUCTION AHEAD SIGN $36 \times 36 \ (900 \times 900)$ WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- g) ONE ROAD CONSTRUCTION AHEAD SIGN 48 \times 48 (1.2 m \times 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (MG-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (MG-4).

SCALE: NONE

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

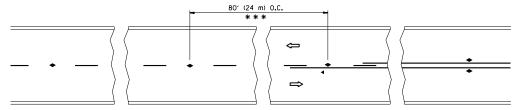
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

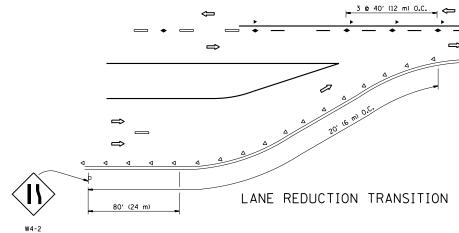
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

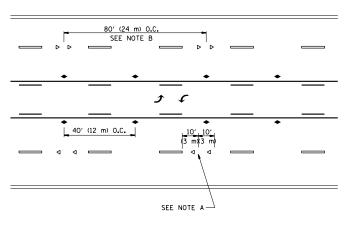
SHEET NO. 1 OF 1 SHEETS STA. TO STA.



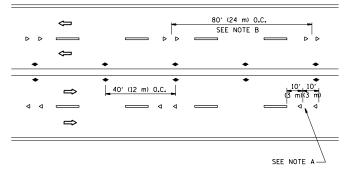
*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO-LANE/TWO-WAY

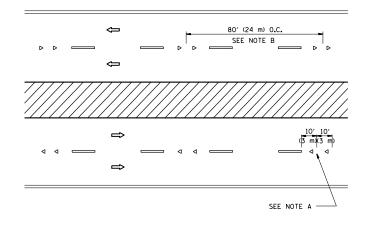




TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- 2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- 3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

SYMBOLS

---- YELLOW STRIPE

WHITE STRIPE

- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (₩/O)
- ◆ TWO-WAY AMBER MARKER

DESIGN NOTES

- 1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- 2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- 3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- 4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

MINIMUM OF 3 W
EQUALLY SPACED 3 @ 80' (24 m) O.C. — ___ 3 @ 80' (24 m) O.C. 3 @ 40' (12 m) 3 @ 40' (12 m) 40' (12 m) 0.C. 40' (12 m) ⇔ \Rightarrow ◆ 40′ (12 m) 0.C. 40' (12 m) 0.C. * SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

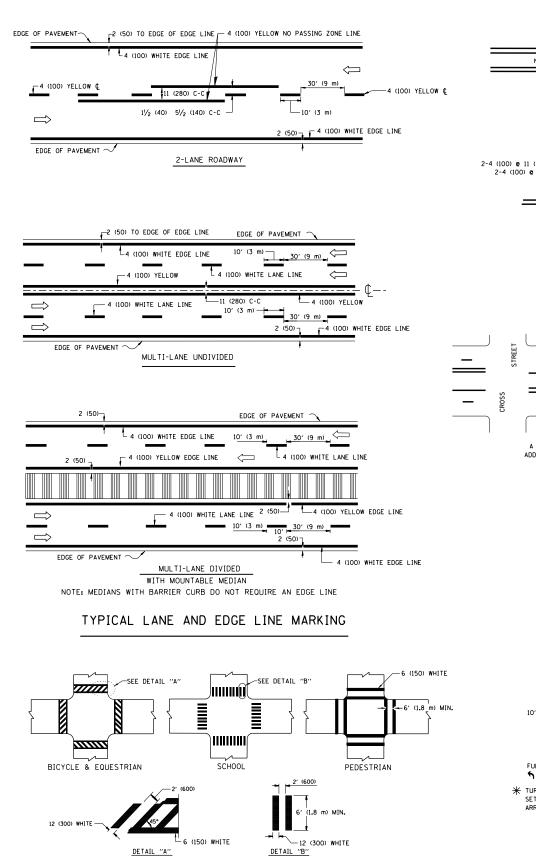
All dimensions are in inches (millimeters) unless otherwise shown.

| FILE NAME = | USER NAME = bartonrw | DESIGNED - | KENIZED | - I. RAMMACHER | 09-19-94 | |
|---|-------------------------------|------------|---------|----------------|----------|---|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -Dupage-DistStd.dgn | DRAWN - | REVISED | -T. RAMMACHER | 03-12-99 | |
| | PLOT SCALE = 100.0000 ' / in. | CHECKED - | REVISED | -T. RAMMACHER | 01-06-00 | D |
| | PLOT DATE = 4/7/2015 | DATE - | REVISED | - C. JUCIUS | 09-09-09 | |

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

| | TYPICAL AP | PLICATIONS | | F.A RTE. | SECTION | COUNTY |
|-------------|-------------------------|---------------------|-------------|-------------|---------------------------------|-------------|
| DAIGED D | EFLECTIVE PAVEMENT MA | DVEDS (SNOW) DLOW | DECICTANT\ | VAR. | 2015-030RS | DUAPGE |
| NAISLU N | ELECTIVE PAVEINIENT INA | nkina (alvove-riovi | nL3I3 [ANT] | | TC-11 | CONTRA |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEE | TS STA. | TO STA. | FED. R | OAD DIST. NO. 1 ILLINOIS FED. | AID PROJECT |

SECTION COUNTY DUAPGE 44 38 /AR. 2015-030RS CONTRACT NO. 62A84



TYPICAL CROSSWALK MARKING

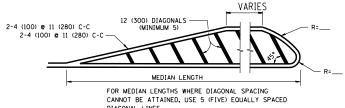
2-4 (100) YELLOW • 11 (280) C-C

NO DIAGONALS

4' (1.2 m) OUTSIDE TO OUTSIDE OF LINES

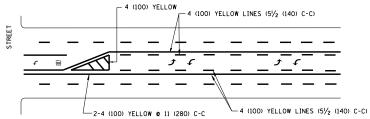
2-4 (100) YELLOW • 11 (280) C-C

4' (1.2 m) WIDE MEDIANS ONLY

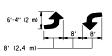


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

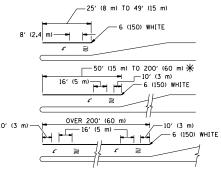


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

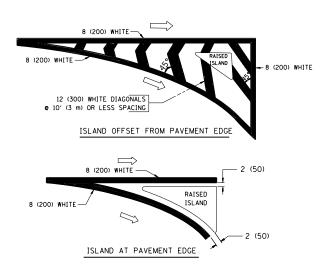


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. \P AREA = 15.6 SO. FT. (1.5 m²) \P AREA = 20.8 SO. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

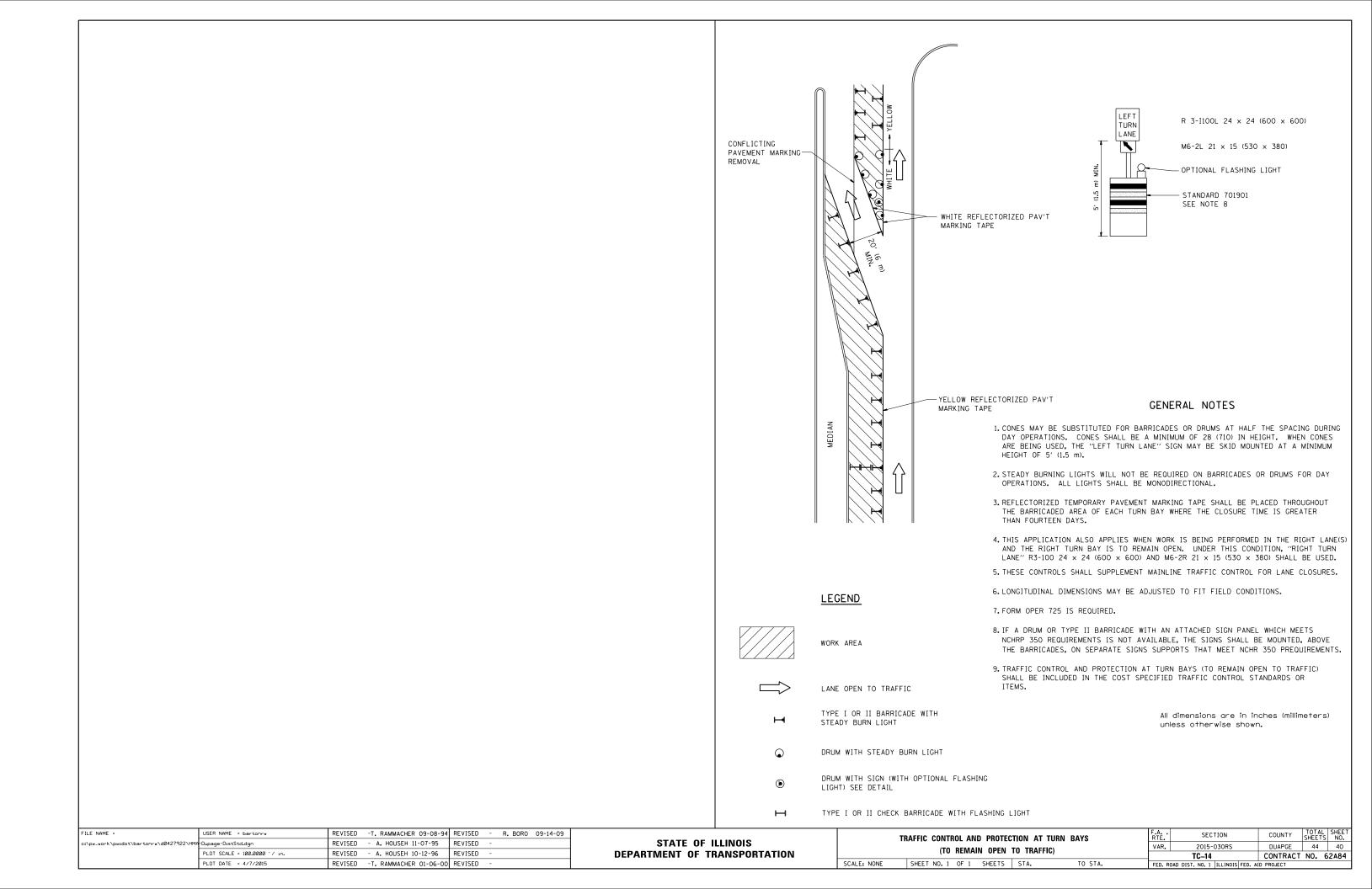
| TURE OF MIRWING | | | | DELENIE A DELUBYS |
|---|---|------------------------------------|---|--|
| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS | 4 (100) 2 @ 4 (100) | SOLID SOLID | YELLOW YELLOW | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 1280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW: EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART 5EE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1,2 m) WIDE MEDIANS | SOLID | YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SO. FT. (0.33 m²) EACH "X"=54.0 SO. FT. (5.0 m²) |
| SHOULDER DIAGONALS | 12 (300) © 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (0VER 45MPH (70 km/h)) |

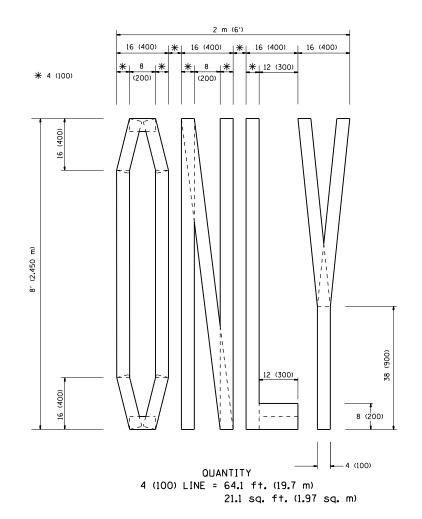
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

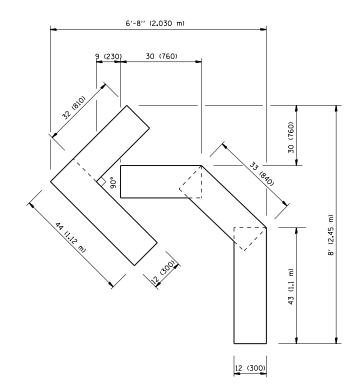
All dimensions are in inches (millimeters) unless otherwise shown.

| TYPICAL | LUKN | LANE | MARKING |
|---------|------|------|---------|
| | | | |

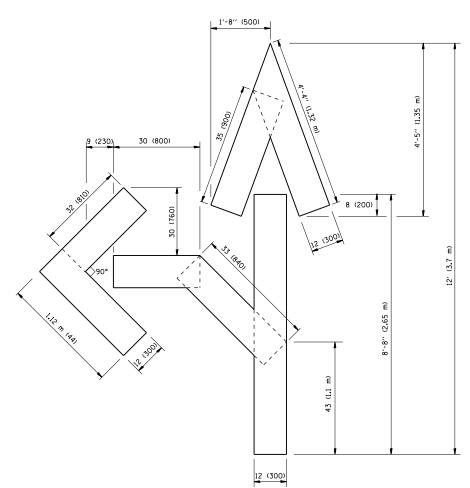
| FILE NAME = | USER NAME = bartonrw | DESIGNED - EVERS | REVISED -T. RAMMACHER 10-27-94 | | DISTRICT ONE | F.A. · SECTION | COUNTY TOTAL SHEET |
|---|-------------------------------|------------------|--------------------------------|------------------------------|--|---------------------------------------|--------------------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -Dupage-DistStd.dgn | DRAWN - | REVISED -C. JUCIUS 09-09-09 | STATE OF ILLINOIS | | VAR. 2015-030RS | DUAPGE 44 39 |
| | PLOT SCALE = 100.0000 ' / in. | CHECKED - | REVISED - | DEPARTMENT OF TRANSPORTATION | TYPICAL PAVEMENT MARKINGS | TC-13 | CONTRACT NO. 62A84 |
| | PLOT DATE = 4/7/2015 | DATE - 03-19-90 | REVISED - | | SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA. | FED. ROAD DIST, NO. 1 ILLINOIS FED. A | |







OUANTITY 4 (100) LINE = 45.5 ft. (13.9 m) 15.2 sq. ft. (1.39 sq. m)



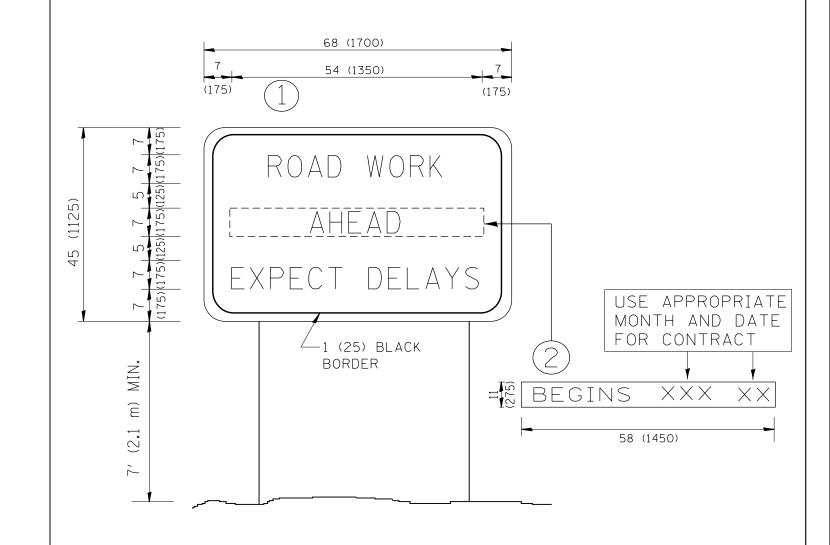
OUANTITY 4 (100) LINE = 82.5 ft. (25.3 m) 27.5 sq. ft. (2.53 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

| ILE NAME = USER NAME = bartonrw | | DESIGNED | - | | REVISED -T. RAMMACHER 06-05- | | | |
|---|----------------------|----------|---|----------|------------------------------|------------------------|--|--|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | DRAWN | - | | REVISED | -T. RAMMACHER 11-04-97 | | | |
| PLOT SCALE = 100.0000 '/ in. | | CHECKED | - | | REVISED | -T. RAMMACHER 03-02-98 | | |
| | PLOT DATE = 4/7/2015 | DATE | - | 09-18-94 | REVISED | -E. GOMEZ 08-28-00 | | |

| STATI | STATE OF ILLINOIS ARTMENT OF TRANSPORTATION | |
|------------|--|----------------|
| DEPARTMENT | 0F | TRANSPORTATION |

| PAVEMENT MARKING LETTERS AND SYMBOLS | | | | | | RTE. | SECTION | COUNTY | SHEETS | NO. |
|--|---------------------|--|--|--|--|------|-----------------------------------|-----------|--------|-------|
| | FOR TRAFFIC STAGING | | | | | | 2015-030RS | DUAPGE | 44 | 41 |
| FUN INAFFIC STAUING | | | | | | | TC-16 | CONTRACT | NO. 6 | 52A84 |
| SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA. | | | | | | | DAD DIST. NO. 1 ILLINOIS FED. A | D PROJECT | | |



NOTES:

- 1. USE BLACK LETTERING ON ORANGE BACKGROUND.
- 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- 4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
- 7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

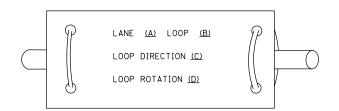
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| FILE NAME = | USER NAME = bartonrw | DESIGNED - | REVISED - R. MIRS 09-15-97 | | ARTERIAL ROAD | | | F.A | SECTION | COUNTY | TOTAL SHEET |
|--|------------------------------|------------|--------------------------------|------------------------------|--|--|-------------|-----------------------------|-------------|----------|-------------|
| c:\pw_work\pwidot\bartonrw\d0427922\HM | -Dupage-DistStd.dgn | DRAWN - | REVISED - R. MIRS 12-11-97 | STATE OF ILLINOIS | | | VAR. | 2015-030RS | DUAPGE | 44 42 | |
| | PLOT SCALE = 100.0000 '/ in. | CHECKED - | REVISED -T. RAMMACHER 02-02-99 | DEPARTMENT OF TRANSPORTATION | INFORMATION SIGN | | | | TC-22 | CONTRACT | NO. 62A84 |
| | PLOT DATE = 4/7/2015 | DATE - | REVISED - C. JUCIUS 01-31-07 | | SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA. | | FED. ROAD [| DIST. NO. 1 ILLINOIS FED. | AID PROJECT | | |

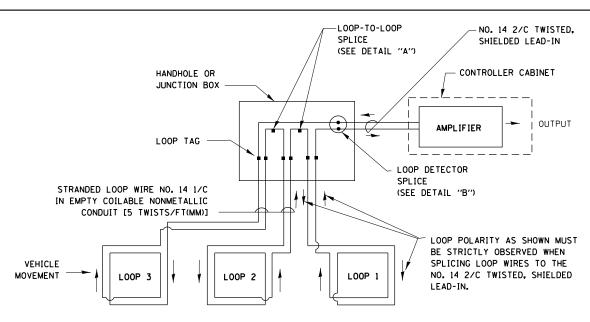
LOOP DETECTOR NOTES

- 1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- 2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- 3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- 4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- 5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- 6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- 7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

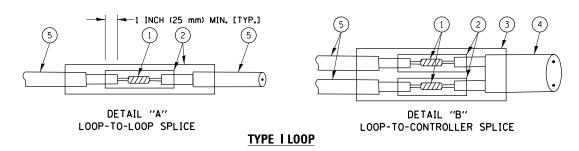


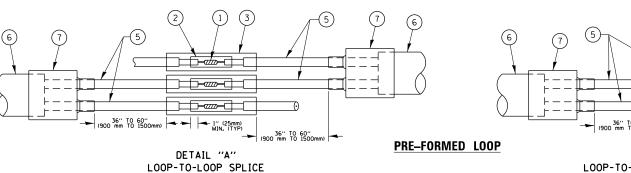
- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP *1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
 THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



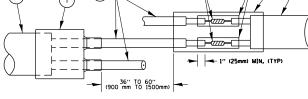


LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- (2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- (3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

SCALE: NONE

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

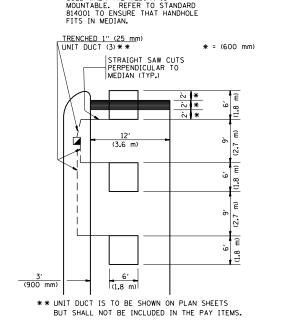
| FILE NAME = | USER NAME = bartonrw | DESIGNED | - | DAD | REVISED | - | DAG 1-1-14 |
|---|-------------------------------|----------|---|----------|---------|---|------------|
| c:\pw_work\pwidot\bartonrw\d0427922\HMA | -Dupage-DistStd.dgn | DRAWN | - | BCK | REVISED | - | |
| | PLOT SCALE = 100.0000 ' / in. | CHECKED | - | DAD | REVISED | - | |
| | PLOT DATE = 4/7/2015 | DATE | - | 10-28-09 | REVISED | - | |

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

| | DISTRICT ONE | | | | | | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|--|------|--------|------|---------|--------|-------------------------------|-------------|-----------------|--------------|
| | STANDARD TRAFFIC SIGNAL DESIGN DETAILS | | | | | VAR. | 2015-030RS | DUAPGE | 44 | 43 |
| | | | | | | | TS-05 | CONTRACT | NO. (| 62A84 |
| | SHEET NO. 2 | OF 7 | SHEETS | STA. | TO STA. | FED. R | DAD DIST. NO. 1 ILLINOIS FED. | AID PROJECT | | |

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER. PAVED SHOULDER. PAVED OR NON-PAVED SHOULDER 1'' (25 mm) UNIT DUCT TRENCHED TO E/P ** * = (600 mm) * * UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING) HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD BI4001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN. TRENCHED 1" (25 mm) UNIT DUCT (3) ** * = (600 mm) STRAIGHT SAW CUTS PERPENDICULAR TO MEDIAN (TYP.)

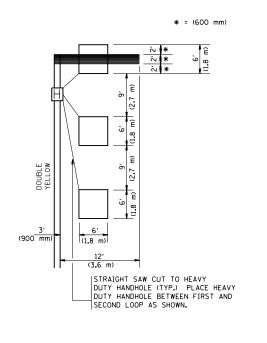


NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO

PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

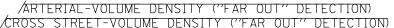
LE<u>FT TURN LANES WITHOUT MEDIANS</u> VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

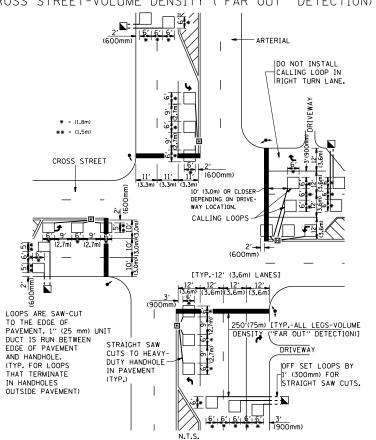


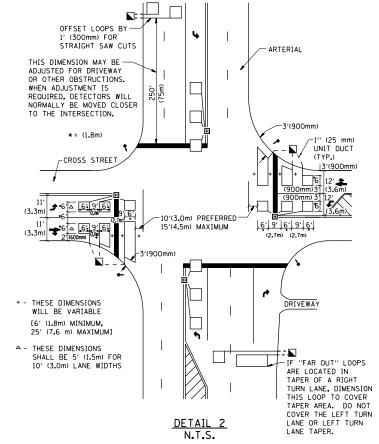
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

SCALE: NONE



ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)





NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIFLDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX, EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF <u>ALL</u> DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

JOTE.

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

| FILE NAME = | USER NAME = bartonrw | DESIGNED - | REVISED - |
|---------------------------------------|-------------------------------|------------------|-----------|
| c:\pw_work\pwidot\bartonrw\d0427922\H | 1A-Dupage-DistStd.dgn | DRAWN - | REVISED - |
| | PLOT SCALE = 100.0000 ' / in. | CHECKED - R.K.F. | REVISED - |
| | PLOT DATE = 4/7/2015 | DATE - | REVISED - |

DETAIL

N.T.S.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

| DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING | | | | | | SECT | rion | COUNTY | TOTAL SHEETS | | |
|---|------------------|--------|-------|---------|----------|-----------------|------------------|-----------|-----------------|--|--|
| | | | | | | 2015-0 | DUAPGE | 44 | 44 | | |
| | DETAILS TON | | TS-07 | ' | CONTRACT | NO. 6 | 2A84 | | | | |
| | SHEET NO. 1 OF 1 | SHEETS | STA. | TO STA. | FED. R | DAD DIST. NO. 1 | ILLINOIS FED. AI | D PROJECT | | | |