

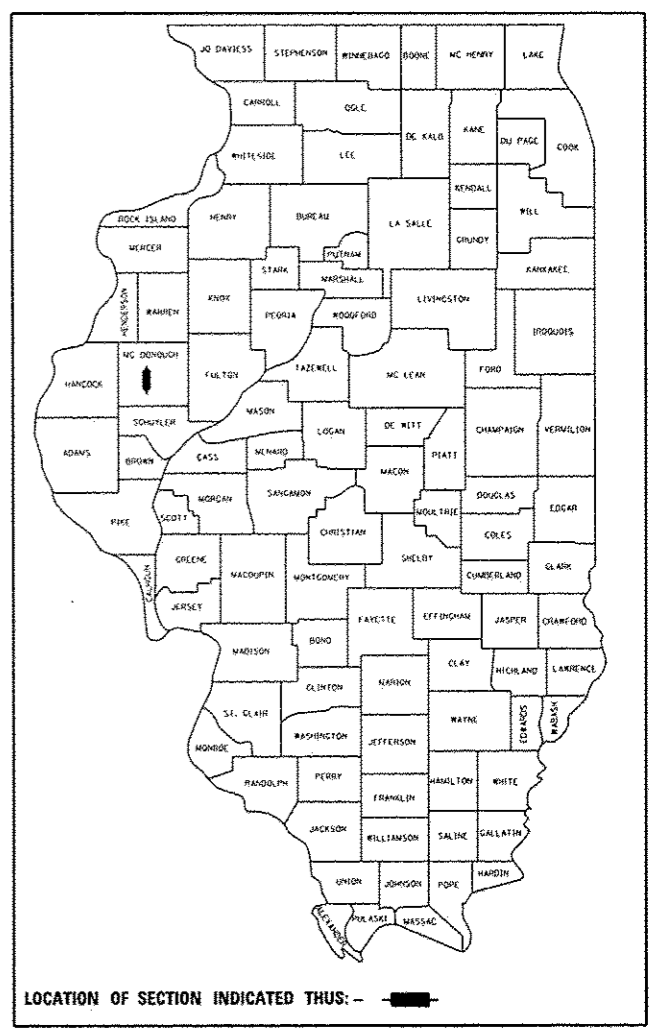
FOR INDEX OF SHEETS, SEE SHEET NO. 2
AND
LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED HIGHWAY PAVING PLANS

FAP ROUTE 407 (IL 336 /US 67 /IL 110)
SECTION 55[3(PV; HB(2-6); B, B-1, B-2)]
PROJECT ACNHPP-0407 (005)
NEW CONSTRUCTION 2 LANE HIGHWAY
McDONOUGH COUNTY
C-94-050-13

06-12-2015 LETTING ITEM 078



STA 5010+72.23
SN 055-0077
PROPOSED TWO SPAN 54" WEB PLATE GIRDER
STRUCTURE, 289'-11 1/2" BK TO BK ABUTMENTS, WITH
A REINFORCED CONCRETE PIER, 33'-2" O TO O
CONCRETE DECK, SKEWED 43°45'17" LT AHD

STA 779+70.00
SN 055-0075
PROPOSED SINGLE SPAN 42" PPC I-BEAM
STRUCTURE, 61'-6" BK TO BK ABUTMENTS,
43'-2" O TO O CONCRETE DECK,
SKEWED 30° LT AHD

STA 3012+00.00
SN 055-0074
PROPOSED TWO SPAN 42" WEB PLATE GIRDER
STRUCTURE, 245'-10 1/4" BK TO BK ABUTMENTS, WITH
A REINFORCED CONCRETE PIER, 33'-2" O TO O
CONCRETE DECK, SKEWED 30° RT AHD

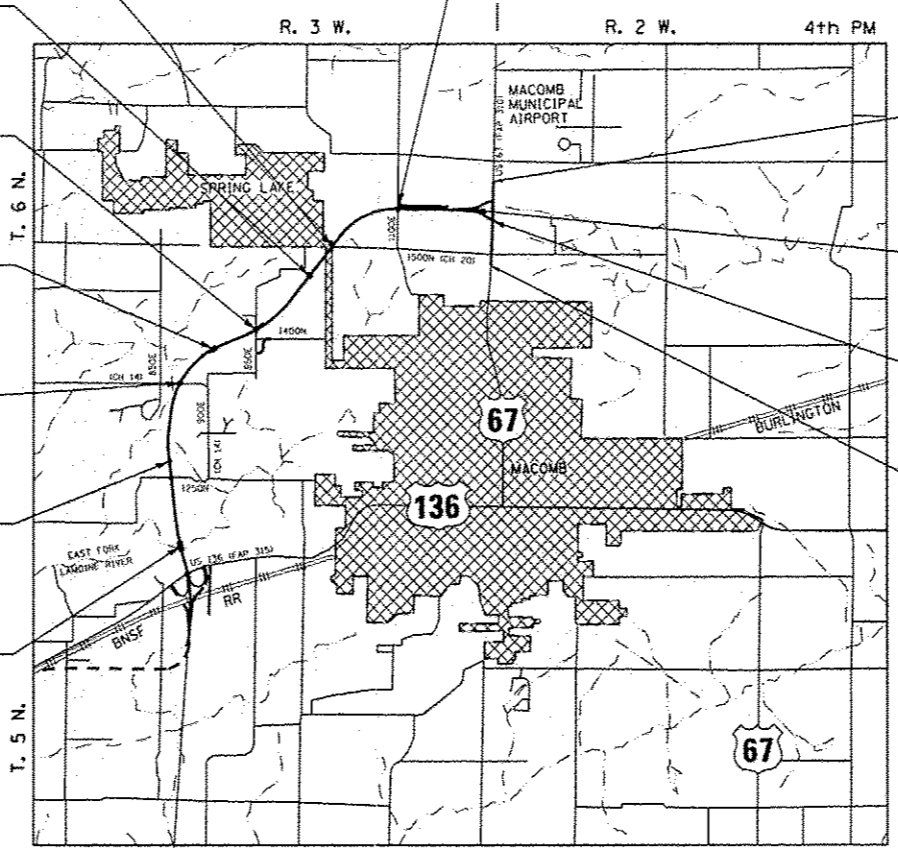
STA 702+70.00
SN 055-0072
PROPOSED SINGLE SPAN 36" PPC I-BEAM
STRUCTURE, 57'-6" BK TO BK ABUTMENTS,
43'-4" O TO O CONCRETE DECK,
SKEWED 0° TO LOCAL TANGENT

STA 681+23.20
SN 055-0070
PROPOSED THREE SPAN 36" PPC I-BEAM
STRUCTURE, 153'-0" BK TO BK ABUTMENTS, WITH
REINFORCED CONCRETE PIERS, 44'-7" O TO O
CONCRETE DECK, SKEWED 33°32'40" RT AHD

STA 634+11.73
SN 055-0068
PROPOSED SINGLE SPAN 36" PPC I-BEAM
STRUCTURE, 51'-6" BK TO BK ABUTMENTS,
43'-2" O TO O CONCRETE DECK,
SKEWED 13° LT AHD

STA 583+30.75
SN 055-0046
PROPOSED FOUR SPAN 72" WEB PLATE GIRDER
STRUCTURE, 654'-0" BK TO BK ABUTMENTS, WITH
REINFORCED CONCRETE PIERS, 43'-2" O TO O
CONCRETE DECK, ON CONTINUOUS CURVE

STA 6019+01.81
SN 055-0078
PROPOSED TWO SPAN 42" WEB PLATE GIRDER
STRUCTURE, 212'-0" BK TO BK ABUTMENTS, WITH
A REINFORCED CONCRETE PIER, 33'-2" O TO O
CONCRETE DECK



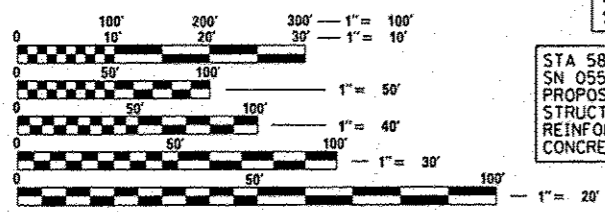
US 67 IMPROVEMENTS
END
STA 461+50.00

SECTION
ENDS
STA. 886+50.00

STA 444+22.28
SN 055-2508
PROPOSED 7'-5 1/8" EXTENSION TO
QUAD 4'x2' PRECAST BOX CULVERT

US 67 IMPROVEMENTS
BEGIN
STA 429+64.16

SECTION
BEGINS
STA. 541+57.14

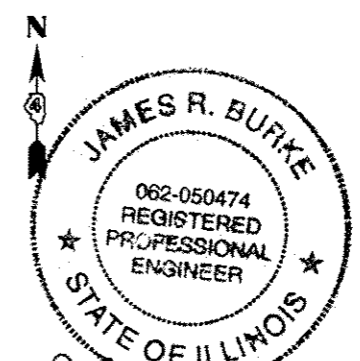


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER : MIKE LEWIS PHONE: (309) 671-3454
PROJECT MANAGER : ELIAS ELDERZI PHONE: (309) 671-3459
CONTRACT NO. 68B44

GROSS LENGTH = 34,492.86 FT. = 6.533 MILES
NET LENGTH = 34,492.86 FT. = 6.533 MILES



J.R. Burke
1/23/15
Exp 11/30/15

Hutchison Engineering, Inc.
SINCE 1945
1801 West Lafayette
PO Box 820
Jacksonville, Illinois 62651
PHONE : (217)245-7164 FAX (217)243-0468

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED *Jan 30* 20 *15*
Kevin A. Garretts
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 8 20 *15*
John D. Baranzelli P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

May 8 20 *15*
Omar Osman P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

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* INCLUDES SHEETS 24A, 396A, 421A, 444A, 470A, 490A, 514A, 549A AND 574A

LIST OF STANDARDS

(UNLESS OTHERWISE NOTED, STANDARDS SHALL APPLY TO ALTERNATE A & B)

| | | |
|--------------------|-----------|-----------|
| 000001-06 | 606001-06 | 701901-04 |
| 280001-07 | 606006-02 | 704001-07 |
| 406001-06 (ALT. B) | 606301-04 | 720001-01 |
| 406101-05 (ALT. B) | 630001-10 | 720006-04 |
| 420001-08 (ALT. A) | 630301-06 | 720011-01 |
| 420101-05 (ALT. A) | 631031-13 | 720021-02 |
| 420201-09 (ALT. A) | 635001-01 | 728001-01 |
| 420301-06 (ALT. A) | 635006-03 | 780001-05 |
| 420401-11 | 635011-02 | 781001-03 |
| 442201-03 | 642001-02 | 782001 |
| 482001-02 (ALT. B) | 643001-02 | 821101-01 |
| 483001-04 (ALT. A) | 664001-02 | 825006-02 |
| 515001-03 | 665001-02 | 830021-02 |
| 542301-03 | 666001-01 | 830026 |
| 542401-01 | 701001-02 | 836001-02 |
| 601001-04 | 701006-05 | 838001 |
| 601101-01 | 701011-04 | BLR 21-9 |
| 602301-04 | 701101-04 | BLR 22-7 |
| 602306-03 | 701106-02 | BLR 24-2 |
| 602401-03 | 701400-08 | |
| 602601-03 | 701401-09 | |
| 602701-02 | 701402-10 | |
| 604086-03 | 701406-09 | |
| 604101-01 | 701428 | |

SOIL REPORT AVAILABILITY

THE SOILS REPORT AND ALL SOILS DATA COLLECTED AND PROCESSED FOR THE SOILS REPORT MADE IN CONJUNCTION WITH THE DESIGN OF THIS IMPROVEMENT IS ON FILE AT THE DISTRICT OFFICE WHERE IT IS AVAILABLE FOR INSPECTION BY CONTRACTORS OR PROSPECTIVE BIDDERS. BY SUBMITTING A BID, THE CONTRACTOR ACKNOWLEDGES THAT THE SOILS REPORT HAS BEEN MADE AVAILABLE AND IS AWARE OF THE REPORT CONTENTS AND APPENDICES.

AVAILABILITY OF ELECTRONIC FILES

MICROSTATION AND GEOPAK FILES OF THIS PROJECT WILL BE MADE AVAILABLE TO THE CONTRACTOR. IF THERE IS A CONFLICT BETWEEN THE ELECTRONIC FILES AND THE PRINTED CONTRACT PLANS AND DOCUMENTS, THE PRINTED CONTRACT PLANS AND DOCUMENTS SHALL TAKE PRECEDENCE OVER THE ELECTRONIC FILES. THE CONTRACTOR SHALL ACCEPT ALL RISK ASSOCIATED WITH USING THE ELECTRONIC FILES AND SHALL HOLD THE DEPARTMENT HARMLESS FOR ANY ERRORS OR OMISSIONS IN THE ELECTRONIC FILES AND THE DATA CONTAINED THEREIN. ERRORS OR DELAYS RESULTING FROM THE USE OF THE ELECTRONIC FILES BY THE CONTRACTOR SHALL NOT RESULT IN AN EXTENSION OF TIME FOR ANY INTERIM OR FINAL COMPLETION DATE OR SHALL NOT BE CONSIDERED CAUSE FOR ADDITIONAL COMPENSATION. THE CONTRACTOR SHALL NOT USE, SHARE, OR DISTRIBUTE THESE ELECTRONIC FILES EXCEPT FOR THE PURPOSE OF CONSTRUCTING THIS CONTRACT. ANY CLAIMS BY THIRD PARTIES DUE TO USE OR ERRORS SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL INCLUDE THIS DISCLAIMER WITH THE TRANSFER OF THESE ELECTRONIC FILES TO ANY OTHER PARTIES AND SHALL INCLUDE APPROPRIATE LANGUAGE BINDING THEM TO SIMILAR RESPONSIBILITIES.

UTILITIES - LOCATION/INFORMATION ON PLANS

THE LOCATIONS OF EXISTING WATER MAINS, GAS MAINS, SEWERS, ELECTRIC POWER LINES, TELEPHONE LINES AND OTHER UTILITIES AS SHOWN ON THE PLANS ARE BASED ON CAREFUL FIELD INVESTIGATION AND THE BEST INFORMATION AVAILABLE, BUT THEY ARE NOT GUARANTEED. UNLESS ELEVATIONS ARE SHOWN ALL UTILITY LOCATIONS SHOWN ON THE CROSS SECTIONS ARE BASED ON THE APPROXIMATE DEPTH SUPPLIED BY THE UTILITY COMPANY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN THEIR EXACT LOCATION FROM THE UTILITY COMPANIES AND BY FIELD INSPECTION.

TREE REMOVAL - UTILITY RELOCATION

TREE REMOVAL MAY BE NECESSARY PRIOR TO UTILITY COMPANIES BEING ABLE TO RELOCATE THEIR FACILITIES OUTSIDE THE CONSTRUCTION LIMITS. THE CONTRACTOR SHOULD COORDINATE ANY CONTRACT TREE REMOVAL ACTIVITIES WITH THE UTILITY COMPANIES TO ELIMINATE CONFLICTS AND POTENTIAL DELAYS CAUSED BY UTILITY TREE REMOVAL ACTIVITIES OR INCOMPLETE UTILITY RELOCATIONS.

PLAN ELEVATIONS - U.S.G.S. MEAN SEA LEVEL DATUM

ALL ELEVATIONS SHOWN ON THE PLANS ARE ESTABLISHED FROM U.S.G.S. MEAN SEA LEVEL DATUM.

TREE REMOVAL

THE DISTRICT FOUR TREE COMMITTEE SHOULD BE CONTACTED AND PRIOR APPROVAL OBTAINED FOR ANY TREE REMOVAL BEYOND THE LIMITS/LOCATIONS INCLUDED IN THE PLANS.

RIGHT-OF-WAY

UNLESS OTHERWISE INDICATED ON THE ROW PLAN SHEETS, ALL RIGHT-OF-WAY SHOWN IN THE PLANS AS PROPOSED ROW WAS PREVIOUSLY ACQUIRED FOR CONSTRUCTION OF THE PRE-GRADED SECTION (55-3) AND IS CONSIDERED EXISTING ROW.

PROPERTY OWNER ACCESS REQUIREMENT

ACCESS MUST BE MAINTAINED TO ALL EXISTING PROPERTIES DURING CONSTRUCTION PER ARTICLE 107.09 UNLESS ARRANGEMENTS ARE MADE IN WRITING BY THE CONTRACTOR WITH THE PROPERTY OWNERS WITH A COPY TO THE ENGINEER FOR SHORT-TERM CLOSURES.

SEEDING - SIDESLOPE RIPPING

ALL SLOPES STEEPER THAN 3 TO 1 AND OVER 15 FT. (4.5 m) IN HEIGHT SHALL BE RIPPED. THIS SHALL CONSIST OF RIPPING BETWEEN 18 INCHES TO 24 INCHES (450 mm TO 600 mm) DEEP NORMAL TO THE SLOPE. THE INTERVAL OF RIPPING ALONG THE SLOPE SHALL BE 12 FT. (3.6 m). THIS WORK SHALL BE DONE AFTER THE SEED BED HAS BEEN PREPARED BUT BEFORE ANY FERTILIZER OR SEED HAS BEEN APPLIED. THE FERTILIZER AND SEED SHALL BE APPLIED WITHIN A 24-HOUR PERIOD AFTER THE RIPPING HAS BEEN DONE. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE VARIOUS ITEMS OF SEEDING INVOLVED.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

| MIXTURE USE(S): | FULL DEPTH HMA POLYMER SURFACE COURSE (2") | FULL DEPTH HMA POLYMER BINDER COURSE (2 1/4") | FULL DEPTH HMA LOWER BINDER LIFTS | HMA SHOULDER (SURFACE LIFT) | HMA SHOULDER (LOWER LIFTS) | HMA SURFACE COURSE (SIDE ROADS) | HMA BASE COURSE (SIDE ROADS) | INCIDENTAL SURFACE COURSE |
|--|--|---|-----------------------------------|-----------------------------|----------------------------|---------------------------------|------------------------------|---------------------------|
| AC/PG: | SBS OR SBR 76-28 | SBS OR SBR 76-28 | PG 64-22 | PG 64-22 | PG 64-22 | PG 64-22 | PG 64-22 | PG 64-22 |
| DESIGN AIR VOIDS: | 4.0% @ N=70 | 4.0% @ N=70 | 4.0% @ N=70 | 4.0% @ N=50 | 4.0% @ N=50 | 4.0% @ N=50 | 4.0% @ N=50 | 4.0% @ N=50 |
| MIXTURE COMPOSITION: (MIXTURE GRADATION) | IL 9.5 | IL 9.5 | IL 19.0 | IL 9.5 | IL 19.0 | IL 9.5 | IL 19.0 | IL 9.5 |
| FRICTION AGGREGATE: | MIX D (DOLOMITE ONLY) | N.A. | N.A. | MIX C | N.A. | MIX C | N.A. | MIX C |
| QUALITY MANAGEMENT PROGRAM: | PPF | PPF | PPF | QCQA | QCQA | QCQA | QCQA | QCQA |

NOTE:

- INDIVIDUAL LIFT THICKNESS OF EACH MIX TYPE WILL BE NO LESS THAN 3 TIMES NOMINAL MAXIMUM AGGREGATE SIZE AND NO MORE THAN 6 TIMES NOMINAL MAXIMUM AGGREGATE SIZE, UNLESS OTHERWISE APPROVED BY THE ENGINEER.
- FOR DESIGN PURPOSES, MIXTURE WEIGHT FOR ALL MIXES IS DETERMINED TO BE 112.0 LB/S.Y./IN., UNLESS OTHERWISE NOTED.
- SUBLOT SIZES FOR PPF AND QCP MIXES WILL BE 1000 TONS, UNLESS OTHERWISE AGREED TO BY THE ENGINEER AND THE PAVING CONTRACTOR.

GENERAL NOTES

ENVIRONMENTAL REVIEWS

PRIOR TO THE USE OF ANY PROPOSED BORROW AREAS, USE AREAS (TEMPORARY ACCESS ROADS, DETOURS, RUN-AROUNDS, ETC.) AND/OR WASTE AREAS, THE CONTRACTOR SHALL FILE THE REQUIRED ENVIRONMENTAL RESOURCE REQUEST SURVEYS ACCORDING TO SECTION 107.22 OF THE STANDARD SPECIFICATIONS. THESE SURVEYS ARE REQUIRED IN ORDER FOR THE DEPARTMENT TO CONDUCT CULTURAL AND BIOLOGICAL RESOURCE SURVEYS FOR THE PROPOSED SITE.

PRIOR TO ANY WASTE MATERIALS BEING REMOVED FROM THE CONSTRUCTION SITE THE REQUIRED ENVIRONMENTAL RESOURCE SURVEYS WILL NEED TO BE OBTAINED AND FILED BY THE CONTRACTOR. EXCESS WASTE PRODUCTS REMOVED FROM THE CONSTRUCTION SITE SHALL BE DISPOSED OF AS REQUIRED IN SECTION 202.03 OF THE STANDARD SPECIFICATIONS.

ANY PROTRUDING METAL BARS SHALL BE REMOVED PRIOR TO THE DISPOSAL OF BROKEN CONCRETE AT APPROVED DISPOSAL SITES.

THE REQUIRED ENVIRONMENTAL RESOURCE DOCUMENTATION SHALL INCLUDE THE FOLLOWING:

- BDE FORM 2289 (CULTURAL AND NATURAL RESOURCES REVIEW OF BORROW AREAS)
- BDE FORM 2290 (WASTE/USE AREA REVIEW)
- A LOCATION MAP SHOWING THE SIZE LIMITS AND LOCATION OF THE USE AREA
- COLOR PHOTOGRAPHS DEPICTING THE USE AREA
- BORROW AREA ENTRY AGREEMENT FORM - D4 P10101

PLEASE NOTE THAT A MINIMUM OF FOUR WEEKS SHALL BE ALLOWED FOR THE DISTRICT TO OBTAIN THE REQUIRED WASTE SITE ENVIRONMENTAL CLEARANCES AND FIVE WEEKS FOR THE REQUIRED BORROW SITE ENVIRONMENTAL CLEARANCES.

LEGEND OF NON-STANDARD SYMBOLS

| | |
|-----|---------------------------------------|
| | PROPOSED DITCH |
| | EXISTING DITCH |
| TOG | TOP OF GRATE |
| TOC | TOP OF CURB |
| EOP | EDGE OF PAVEMENT |
| | SPECIAL DITCH LEFT (SP DT LT) |
| | SPECIAL DITCH RIGHT (SP DT RT) |
| | SPECIAL MEDIAN DITCH (SP MED DT) |
| TBR | ITEM TO BE REMOVED |
| | HOT-MIX ASPHALT SURFACE REMOVAL |
| | FENCE REMOVAL |
| | PAVEMENT/SHOULDER REMOVAL |
| | PAVEMENT PATCHING |
| FST | FLAT SLAB TOP (SEE HWY STD OR DETAIL) |
| | TRENCH BACKFILL, CU. YDS. |

| | | | | | | | | | | | | | | | | |
|---|--------------------|-----------------|----------|---|--|--|--|---|--|--|--|---------------------------|-----------------------------------|-------------------|-------------------|--------------------|
| FILE NAME: V:\Transportation\2894\2-Lane Paving Pl... | USER NAME: JDeon | DESIGNED: JRB | REVISED: | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | | | | FAP ROUTE 407 (IL 336 /IL 110) | | | | F.A.P. RTE: 407 | SECTION: 55C3(PV)HB(2-6)B,B-1,B-2 | COUNTY: McDONOUGH | TOTAL SHEETS: 874 | SHEET NO.: 2 |
| PLT SCALE: 2,000 FT / IN. | PLT DATE: 2/2/2015 | DRAWN: TJD | REVISED: | | | | | INDEX OF SHEETS, LEGEND & GENERAL NOTES | | | | SCALE: N/A | SHEET NO. 1 OF 2 SHEETS | STA. N/A | TO STA. N/A | CONTRACT NO. 68B44 |
| | | CHECKED: JRB | REVISED: | | | | | | | | | ILLINOIS FED. AID PROJECT | | | | |
| | | DATE: 1-30-2015 | REVISED: | | | | | | | | | | | | | |

GENERAL NOTES (CONTINUED)

HOT-MIX ASPHALT PAVING (ALTERNATE B)

ALL LIFTS OF HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N70; POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N70 AND POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX D, N70 SHALL BE PLACED ACROSS BOTH LANES OF IL 336 SIMULTANEOUSLY. THIS SHALL BE ACCOMPLISHED BY USING ONE HMA PAVER CAPABLE OF PAVING THE ENTIRE WIDTH OF BOTH LANES OR USING TWO HMA PAVERS IN AN ECHELON FORMATION.

PAVEMENT STATION NUMBERS & PLACEMENT

THE CONTRACTOR SHALL PROVIDE LABOR AND MATERIALS REQUIRED TO IMPRINT PAVEMENT STATION NUMBERS IN THE FINISHED SURFACE OF THE PAVEMENT AND/OR OVERLAY. THE NUMBERS SHALL BE APPROXIMATELY 3/4 INCH (20 mm) WIDE, 5 INCHES (125 mm) HIGH AND 3/8 INCH (15 mm) DEEP.

THE PAVEMENT STATION NUMBERS SHALL BE INSTALLED AS SPECIFIED HEREIN:

INTERVAL - 200 FEET (ENGLISH STATIONING) OR 100 METERS (METRIC STATIONING)

BOTTOM OF NUMBERS - 6 INCHES (150 mm) FROM THE INSIDE EDGE OF THE PAVEMENT MARKING

LOCATION:

- 2, 3, & 5 LANE PAVEMENTS - RIGHT EDGE OF PAVEMENT IN DIRECTION OF INCREASING STATIONS
- MULTI-LANE DIVIDED ROADWAYS - OUTSIDE EDGE OF PAVEMENT IN BOTH DIRECTIONS
- RAMPS - ALONG BASELINE EDGE OF PAVEMENT

POSITION - STATIONS SHALL BE PLACED SO THEY CAN BE READ FROM THE ADJACENT SHOULDER

FORMAT - ENGLISH (METRIC) PAVEMENT STATIONS SHALL USE THIS FORMAT "XXX (XX+X00)", WHERE X REPRESENTS THE PAVEMENT STATION

THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE COST OF THE ASSOCIATED PAVEMENT AND/OR OVERLAY PAY ITEMS.

AGGREGATE FOR DRIVEWAY REPLACEMENT

THE MATERIAL USED FOR CONSTRUCTION OF PERMANENT AGGREGATE DRIVEWAYS SHALL BE GRAVEL OR CRUSHED STONE, AS DIRECTED BY THE ENGINEER, TO REPLACE IN KIND THE EXISTING AGGREGATE DRIVEWAYS.

NO ADDITIONAL COMPENSATION SHALL BE PROVIDED FOR THIS REQUIREMENT BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE PAY ITEM FOR THE AGGREGATE AS SPECIFIED ON THE PLANS.

PAVING SURFACE COURSE

CONTINUOUS PAVING OPERATIONS ON THE MAIN ROADWAY SHALL BE MAINTAINED AT ALL TIMES DURING THE CONSTRUCTION OF THE HOT-MIX ASPHALT SURFACE. NO INTERRUPTIONS FOR SIDE ROADS, ENTRANCES, TURN LANES, ETC. WILL BE ALLOWED.

ORDERING LENGTH CONFIRMATION - DRAINAGE ITEMS

THE CONTRACTOR SHALL CONSULT WITH THE ENGINEER IN REGARD TO THE EXACT LENGTH OF THE BOX/PIPE CULVERTS, STORM SEWERS, AND/OR PIPE DRAINS REQUIRED PRIOR TO ORDERING THESE ITEMS.

EXISTING DRAINAGE PIPES CONNECTED TO NEW STRUCTURES

IN ACCORDANCE WITH SECTION 602 OF THE STANDARD SPECIFICATIONS, THE CONNECTING OF EXISTING DRAIN TILES, PIPE CULVERTS, OR STORM SEWERS TO THE PROPOSED DRAINAGE SYSTEM STRUCTURES WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCLUDED IN THE PAY ITEMS PROVIDED.

MEDIAN AND ISLAND NOSES

WHEN CONSTRUCTING MEDIAN AND ISLAND NOSES THE FOLLOWING CRITERIA SHOULD BE FOLLOWED:

- BARRIER CURB SHALL BE USED TO CONSTRUCT NOSES WHEN THE MEDIAN OR ISLAND SURROUNDS A MAST ARM OR OTHER NON-BREAKAWAY FOUNDATION.
- RAMPED NOSES SHALL BE USED ON MEDIANS OR ISLANDS WITH BREAKAWAY POSTS.

BUTT JOINT CUTTING TIME RESTRICTION

BUTT JOINTS SHALL NOT BE MILLED MORE THAN THREE (3) DAYS PRIOR TO PLACEMENT OF THE HMA SURFACE COURSE.

SIGN POST HOLES

VERTICAL HOLES SHALL BE CONSTRUCTED IN THE ISLAND PAVEMENT AND/OR CONCRETE MEDIAN OF THE TYPE SPECIFIED OR CONCRETE MEDIAN SURFACE 4 INCHES (100 mm). THE HOLES SHALL BE 24 INCHES (600mm) IN DIAMETER OR 24 INCHES (600 mm) SQUARE AND THEY SHALL BE FREE OF ANY OBSTRUCTION, EXCEPT EARTH, FOR A DEPTH OF 5 FEET (1.5 m) AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. ANY HOLES NOT USED FOR THE PLACEMENT OF SIGNS SHALL BE FILLED AND COMPACTED FLUSH WITH THE TOP OF THE ISLAND PAVEMENT, CONCRETE MEDIAN OF THE TYPES SPECIFIED, OR CONCRETE MEDIAN SURFACE 4 INCHES (100 mm). THE TOP 3 INCHES (75 mm) OF SAID COMPACTED FILL SHALL CONSIST OF A HOT-MIX ASPHALT MIXTURE. ALL HOLES IN WHICH THE SIGN POSTS ARE INSTALLED AT THE TIME OF THIS CONTRACT SHALL BE SIMILARLY FILLED.

THIS WORK, INCLUDING ANY REQUIRED PAVEMENT REMOVAL NECESSARY TO CONSTRUCT THE SIGN POST HOLES, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE FOOT (SQUARE METER) FOR ISLAND PAVEMENT AND/OR CONCRETE MEDIAN OF THE TYPE SPECIFIED, OR CONCRETE MEDIAN SURFACE, 4 INCHES (100 mm).

RIGHT-OF-WAY MARKERS

WHEN INSTALLING RIGHT-OF-WAY MAKERS, CARE SHALL BE TAKEN TO NOT DISTURB ANY EXISTING PROPERTY/RIGHT-OF-WAY PINS. IF A PROPERTY/RIGHT-OF-WAY PIN IS FOUND AT THE LOCATION OF A PROPOSED RIGHT-OF-WAY MARKER, THE MARKER SHALL BE PLACED ONE (1) FOOT IN FRONT OF THE PIN.

ENGINEERS FIELD OFFICE

ADD THE FOLLOWING SENTENCE TO THE END OF PARAGRAPH 670.02 (I) AND 670.04 (e):

ALL OF THE TELEPHONE LINES PROVIDED SHALL HAVE UNPUBLISHED NUMBERS.

SETTING OF SECTION CORNER MONUMENTATION

ALL SECTION CORNER LOCATION ON THIS PROJECT SHALL BE LOCATED AND VERIFIED BY A LICENSED LAND SURVEYOR PRIOR TO ANY REMOVAL WORK BEING PERFORMED. THE LAND SURVEYOR SHALL LOCATE THE EXISTING SECTION CORNERS THROUGH COURTHOUSE RESEARCH, PERSONAL KNOWLEDGE OR THROUGH THE ASSISTANCE OF LOCAL FIRMS PERFORMING LAND SURVEYING IN THE AREA. IF THE SECTION CORNER DOES NOT EXIST THROUGH EITHER ITS PHYSICAL LOCATION OR THROUGH TIES IN THE FIELD IT SHALL NOT BE RESET, THERE SHALL BE NO CALCULATING OF SECTION CORNERS ONTO A PROJECT REQUIRED.

ONCE THE PAVING AND STRIPING OPERATIONS HAVE BEEN PERFORMED THE SECTION CORNER SHALL BE RESET AT THE DIRECTION OF A LICENSED LAND SURVEYOR. IF ANY DIMENSIONS HAVE BEEN CHANGED, IT SHALL BE THE RESPONSIBILITY OF THE SURVEYOR TO FILE A NEW MONUMENT RECORD IN THE APPROPRIATE COURTHOUSE.

A COPY OF ALL DRAWINGS OR MONUMENT RECORDS PRODUCED FROM THIS PROJECT SHALL BE SENT TO THE CHIEF OF SURVEYS, ILLINOIS DEPARTMENT OF TRANSPORTATION, REGION THREE/DISTRICT FOUR, PEORIA, ILLINOIS.

THE SUPPLYING, DRILLING, SETTING OF DISKS, PROFESSIONAL SERVICES, LABOR AND ANY OTHER ADDITIONAL WORK REQUIRED TO PERFORM THIS WORK SHALL BE PAID FOR UNDER PAY ITEM FOR PERMANENT SURVEY MARKERS, TYPE I.

REFER TO DISTRICT FOUR CADD STANDARD 667101 FOR DETAILS.

SIGNING

SIGN LOCATIONS MAY VARY FROM THE STATIONS SHOWN ON THE PLANS IN ACCORDANCE WITH DIRECTIONS FROM THE ENGINEER AT THE TIME OF CONSTRUCTION. SIGN LOCATIONS MAY BE ADJUSTED IN THE FIELD TO AVOID ANY FOUND UTILITIES.

ALL WOOD POST LOCATIONS SHALL BE VERIFIED WITH THE BUREAU OF OPERATIONS, TRAFFIC SECTION, BEFORE INSTALLATION.

JOB SPECIFIC NOTES

MANHOLES AND INLETS

FOR INLETS AND MANHOLES CONSTRUCTED IN CONJUNCTION WITH THE CURB AND GUTTER, THE DISTANCE SHOWN ON THE PLANS IS TO THE FACE OF CURB. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONSTRUCT EACH INLET OR MANHOLE, AT THE PROPER LOCATION, SO THAT THE FRAME OR GRATE MATCHES THE CURB LINE. FOR ALL MANHOLES OR INLETS NOT CONSTRUCTED IN CONJUNCTION WITH THE CURB AND GUTTER OR CURB, THE DISTANCE SHOWN ON THE PLAN IS TO THE CENTER OF THE MANHOLE OR INLET.

TOP SOIL REMOVAL

MATERIAL ACCEPTABLE FOR USE AS TOPSOIL IS AVAILABLE THROUGHOUT THE LIMITS OF THE SIDEROAD IMPROVEMENT PORTIONS OF THE PROJECT WITHIN THE ROW LIMITS IN BOTH CUT AND FILL SECTIONS. TOPSOIL SHOULD BE REMOVED TO THE DEPTH ENCOUNTERED OR TO AN AVERAGE DEPTH OF 4". THE TOPSOIL SHALL BE STOCKPILED FOR LATER USE, AS APPROPRIATE.

ACTUAL TOPSOIL THICKNESSES ENCOUNTERED VARY FROM 4" TO 12" WITHIN THE LIMITS OF THE IMPROVEMENTS. BELOW IS A LIST OF ANTICIPATED TOPSOIL THICKNESSES AS IDENTIFIED IN THE ROADWAY GEOTECHNICAL REPORT (RGR). ACTUAL TOPSOIL THICKNESSES WILL LIKELY VARY.

| LOCATION | ESTIMATED THICKNESS |
|------------|---------------------|
| TR 950E | 4" |
| TOWER ROAD | 12" |
| CH 20 | 4" |
| TR 1100E | 4" |
| US 67 | 4" |

EXISTING EROSION CONTROL

ANY EXISTING EROSION CONTROL ITEM FROM PREVIOUS OR ADJACENT CONTRACTS SHALL BE MAINTAINED AS DIRECTED BY THE ENGINEER AT NO ADDITIONAL COST.

SETTLEMENT

TO FACILITATE PROPER SETTLEMENT OF UNDERLYING SOILS IN AREAS OF PROPOSED EMBANKMENTS, THERE ARE AREAS THAT HAVE BEEN IDENTIFIED IN THE ROADWAY GEOTECHNICAL REPORT (RGR) AND SHOWN IN THE PLANS THAT REQUIRE THE USE OF SETTLEMENT PLATFORMS.

THE FOLLOWING ARE THE LOCATIONS HAVING SOILS THAT WILL REQUIRE SETTLEMENT MEASURES:

| LOCATION |
|-----------------------------------|
| TR 950E : STA 3017+00 TO 3020+00 |
| CH 20 : STA 5008+00 TO 5013+00 |
| TR 1100E : STA 6017+00 TO 6021+00 |

LOCAL DETOUR

PAVEMENT PATCHING OF LOCAL DETOUR ROADWAYS SHALL BE COMPLETED PRIOR TO THE CLOSURE ANY SIDEROAD.

JOB SPECIFIC NOTES (CONTINUED)

CONSECUTIVE SIDE STREET (ROAD) CLOSURE

SIDE STREETS (ROADS) MAY BE CLOSED AT THE SAME TIME DURING CONSTRUCTION. ONLY AS INDICATED IN THE TRAFFIC CONTROL PLANS AND AS APPROVED BY THE ENGINEER.

BLR STANDARD 21 SHALL BE USED FOR ALL LOCAL ROAD CLOSURES WITHOUT ANY ENTRANCES WITHIN THE CLOSED AREA. BLR STANDARD 22 CAN BE USED WHERE IT IS NECESSARY TO ALLOW LOCAL TRAFFIC ACCESS.

COMMITMENTS

THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER (JAMES OR SHELLEY STANDLEY) PRIOR TO ANY WORK BEING PERFORMED ON THEIR DRIVEWAY ON THE SOUTH SIDE OF COUNTY HIGHWAY 20 WHICH ACCESSES THE PASTURE, WEST OF THEIR HOUSE. ALL WORK NECESSARY TO PREPARE THE LOCATION AND PLACE THE AGGREGATE DRIVE SHALL NOT DISTURB THE NEWLY INSTALLED FENCE AND SHALL BE CONFINED TO THE AREA DIRECTLY IN FRONT OF AND BEHIND THE GATE.

COMMITMENTS ARE NOT TO BE ALTERED WITHOUT THE WRITTEN APPROVAL OF ALL PARTIES TO WHICH THE COMMITMENT WAS MADE.

ANY ENTITIES REALIZED TO BE AFFECTED BY CONSTRUCTION SHALL BE CONTACTED PRIOR TO THE CLOSING OF THE ROADWAY. THESE INCLUDE BUT ARE NOT LIMITED TO:

McDONOUGH COUNTY ENGINEER:
MR. TOM HICKMAN
(309) 833-4196 (W)
(309) 333-9213 (C)

EMMET TOWNSHIP ROAD COMMISSIONER:
MR. JEFF LEE
(309) 333-7333 (C)

McDONOUGH COUNTY ESDA: (309) 837-2686

McDONOUGH COUNTY SHERIFF'S DEPT.: (309) 833-2323 (NON-EMERGENCY/ADMIN.)

FIRE DEPARTMENT: (309) 836-7800 (NON-EMERGENCY)

AMBULANCE SERVICE: (309) 837-4500 (NON-EMERGENCY)

US POST OFFICE IN MACOMB: (309) 833-5594

BOARD OF EDUCATION: (309) 833-4161

SCHOOL DISTRICT 185 TRANSPORTATION DEPT: (309) 836-6806

McDONOUGH COUNTY PUBLIC TRANSPORTATION: (309) 837-3941

PROCESSING LIME MODIFIED SOIL

PROCESSING LIME MODIFIED SOIL SHALL BE 12" MINIMUM, EXCEPT IN THE AREAS AND DEPTHS SHOWN BELOW:

LOCATIONS

| | | |
|--------------------------|-----|--|
| IL 336/JL 110: | | |
| STA 598+00 TO STA 603+50 | 16" | |
| STA 607+00 TO STA 610+50 | 16" | |
| STA 615+00 TO STA 618+00 | 16" | |
| STA 712+00 TO STA 722+00 | 18" | |
| STA 723+00 TO STA 730+00 | 24" | |
| STA 745+00 TO STA 750+50 | 20" | |
| STA 750+50 TO STA 756+50 | 18" | |
| STA 757+50 TO STA 760+00 | 16" | |
| STA 775+00 TO STA 778+50 | 24" | |
| STA 790+00 TO STA 794+00 | 14" | |
| STA 795+50 TO STA 806+00 | 14" | |
| STA 847+00 TO STA 863+00 | 18" | |
| RAMP L: | | |
| STA 66+50 TO STA 73+50 | 16" | |
| RAMP K: | | |
| STA 100+00 TO STA 107+00 | 16" | |
| RAMP A: | | |
| STA 321+00 TO STA 326+50 | 18" | |

VARYING FIELD CONDITIONS

THESE PLANS WERE PREPARED BASED UPON THE GRADING SECTION (55-31A), CONTRACT NO. 68A42, HAVING BEEN BUILT AS SHOWN IN THOSE PLANS. BECAUSE CONSTRUCTION WAS NOT COMPLETE PRIOR TO THE COMPLETION OF THESE PLANS, ACTUAL AS-BUILT FIELD CONDITIONS MAY VARY FROM THOSE SHOWN WITHIN.

ALTERNATE BID NOTES:

1. THE PLANS CONTAIN PROVISIONS FOR A BASE BID OF PORTLAND CEMENT CONCRETE PAVEMENT (PCC), ALTERNATE A, AND AN ALTERNATE BID OF HOT-MIX ASPHALT PAVEMENT (HMA), ALTERNATE B. THE ALTERNATE BID PROVISION SHALL APPLY TO THE IL 336 MAINLINE, RAMPS K AND L AT THE US 136 INTERCHANGE, AND RAMPS A AND D AT THE US 67 INTERCHANGE. ALL OTHER SIDEROAD PAVEMENTS SHALL BE AS SHOWN IN THE PLANS. THE CONTRACTOR SHALL NOTE THE FOLLOWING:
2. IF THE BASE BID (ALTERNATE A-PCC) IS AWARDED, THE FOLLOWING SHEETS AND STANDARDS SHALL BE DELETED FROM THE PLAN SET: SHEETS 33-38, 48-55 AND 62-63; STANDARDS 406001-06, 406101-05 AND 482001-02.
3. IF THE ALTERNATE BID (ALTERNATE B-HMA) IS AWARDED, THE FOLLOWING SHEETS AND STANDARDS SHALL BE DELETED FROM THE PLAN SET: SHEETS 27-32, 40-47, 60-61 AND 284-298; STANDARDS 420001-08, 420101-05, 420201-09, 420301-06 AND 483001-04.

| FILE NAME | USER NAME | DESIGNED | REVISED | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | | FAP ROUTE 407 (IL 336 /IL 110) GENERAL NOTES, COMMITMENTS & PHONE NUMBERS | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|-----------|-----------|---------|---|--|--|--|---|----------------------------|-----------|-----------------|--------------|
| \\tr\transportation\2891\2-Lane Paving P | JDean | JRB | - | DRAWN - TJD | | SCALE: N/A | | 407 | 55C3(PV,HB(2-6);B,8-1,8-2) | McDONOUGH | 874 | 3 |
| | | JRB | - | CHECKED - JRB | | SHEET NO. 2 OF 2 SHEETS | | | | | | |
| | | 1-30-2015 | - | DATE - 1-30-2015 | | STA. N/A TO STA. N/A | | CONTRACT NO. 68B44 ILLINOIS FED. AID PROJECT | | | | |

| ROUTE/STREET | OFFSET | LOCATION | OWNER | TYPE OF UTILITY | TYPE OF CONFLICT | DISPOSITION | REMARKS |
|-----------------|-----------|-------------|-----------------------------|-----------------|------------------|--------------------|---------|
| IL 336/IL 110 | CROSSROAD | STA 639+30 | PRIVATE | SANITARY SEWER | NEW CONSTRUCTION | PENDING RELOCATION | |
| 950E (EMORY RD) | 45' RT | STA 3013+40 | MCDONOUGH POWER COOPERATIVE | POWER POLE | NEW CONSTRUCTION | RELOCATE | |
| | | | | | | | |
| | | | | | | | |

| | | | | | | | | | | | |
|--|-------------------|----------------|-----------|---|--|-----------------------------|--------------------|---------|---------------------------|-----------------|--------------|
| FILE NAME : | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) STATUS OF UTILITIES TO BE ADJUSTED | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-Lane Paving Plans\CROD Sheets\0468844-shr-utility.dgn | DRAWN - TJD | REVISED - | | | 407 | 55(3(PV,HB(2-6);B,B-1,B-2)) | MoDONOUGH | 874 | 4 | | |
| PLOT SCALE = 24.00' / IN. | CHECKED - JRB | REVISED - | | | | | CONTRACT NO. 68B44 | | | | |
| PLOT DATE = 1/28/2015 | DATE = 1-30-2015 | REVISED - | | | SCALE: N/A | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT | | |

| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | | | | | | | | | |
|----------|---------------------------------------|-------|---|--------------------------|----------------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|-------------------|--|--|--|--|--|--|--|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | | | | | | | |
| | | | | ROADWAY 0001 RURAL | HIGHWAY LIGHTING 0021 NONE | BRIDGE 0008 055-0046 | BRIDGE 0008 055-0068 | BRIDGE 0008 055-0070 | BRIDGE 0008 055-0072 | BRIDGE 0008 055-0074 | BRIDGE 0008 055-0075 | BRIDGE 0008 055-0077 | BRIDGE 0008 055-0078 | | | | | | | | |
| 20100110 | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | UNIT | 18 | 18 | | | | | | | | | | | | | | | | | |
| 20200100 | EARTH EXCAVATION | CU YD | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | | | | | | | | |
| 20400800 | FURNISHED EXCAVATION | CU YD | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | | | | | | | | |
| 20800150 | TRENCH BACKFILL | CU YD | 693 | 693 | | | | | | | | | | | | | | | | | |
| 21101615 | TOPSOIL FURNISH AND PLACE, 4" | SQ YD | 118647 | 118647 | | | | | | | | | | | | | | | | | |
| 25000210 | SEEDING, CLASS 2A | ACRE | 43.50 | 43.50 | | | | | | | | | | | | | | | | | |
| 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 3915 | 3915 | | | | | | | | | | | | | | | | | |
| 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 3915 | 3915 | | | | | | | | | | | | | | | | | |
| 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 3915 | 3915 | | | | | | | | | | | | | | | | | |
| 25000750 | MOWING | ACRE | 90.00 | 90.00 | | | | | | | | | | | | | | | | | |
| 25100115 | MULCH, METHOD 2 | ACRE | 35.75 | 35.75 | | | | | | | | | | | | | | | | | |
| 25100635 | HEAVY DUTY EROSION CONTROL BLANKET | SQ YD | 37618 | 37618 | | | | | | | | | | | | | | | | | |
| 28000250 | TEMPORARY EROSION CONTROL SEEDING | POUND | 13908 | 13908 | | | | | | | | | | | | | | | | | |
| 28000305 | TEMPORARY DITCH CHECKS | FOOT | 1561 | 1561 | | | | | | | | | | | | | | | | | |

NON-PART. (100% STATE)

| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | | |
|----------|------------------------------|-------|---|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|----------|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | |
| | | | | ROADWAY | HIGHWAY LIGHTING | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE | |
| | | | | 0001 | 0021 | 0008 | 0008 | 0008 | 0008 | 0008 | 0008 | 0008 | 0008 | 0008 |
| | | | | RURAL | NONE | 055-0046 | 055-0068 | 055-0070 | 055-0072 | 055-0074 | 055-0075 | 055-0077 | 055-0078 | 055-0078 |
| 28000315 | AGGREGATE DITCH CHECKS | TON | 38 | 38 | | | | | | | | | | |
| 28000400 | PERIMETER EROSION BARRIER | FOOT | 11568 | 11568 | | | | | | | | | | |
| 28000500 | INLET AND PIPE PROTECTION | EACH | 20 | 20 | | | | | | | | | | |
| 28000510 | INLET FILTERS | EACH | 22 | 22 | | | | | | | | | | |
| 28100105 | STONE RIPRAP, CLASS A3 | SQ YD | 173 | 173 | | | | | | | | | | |
| 28100107 | STONE RIPRAP, CLASS A4 | SQ YD | 949 | 800 | | | 149 | | | | | | | |
| 28100109 | STONE RIPRAP, CLASS A5 | SQ YD | 932 | 932 | | | | | | | | | | |
| 28100125 | STONE RIPRAP, CLASS B3 | SQ YD | 4216 | 580 | | | 1318 | | 1041 | | 1277 | | | |
| 28100207 | STONE RIPRAP, CLASS A4 | TON | 10473 | | | 10473 | | | | | | | | |
| 28200200 | FILTER FABRIC | SQ YD | 17784 | 1926 | | 15709 | 149 | | | | | | | |
| 30103000 | SHAPING AND GRADING ROADWAY | UNIT | 484.95 | 484.95 | | | | | | | | | | |
| 30200650 | PROCESSING MODIFIED SOIL 12" | SQ YD | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | |
| 30200750 | PROCESSING MODIFIED SOIL 14" | SQ YD | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | |
| 30200850 | PROCESSING MODIFIED SOIL 16" | SQ YD | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | |

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|----------|--|-------|---|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|------|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | |
| | | | | ROADWAY | HIGHWAY LIGHTING | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE | |
| | | | | 0001 | 0021 | 0008 | 0008 | 0008 | 0008 | 0008 | 0008 | 0008 | 0008 | 0008 |
| | | | | RURAL | NONE | 055-0046 | 055-0068 | 055-0070 | 055-0072 | 055-0074 | 055-0075 | 055-0077 | 055-0078 | |
| 40300400 | BITUMINOUS MATERIALS (COVER AND SEAL COATS) | TON | 97.4 | 97.4 | | | | | | | | | | |
| 40300500 | COVER COAT AGGREGATE | TON | 407.3 | 407.3 | | | | | | | | | | |
| 40300600 | SEAL COAT AGGREGATE | TON | 203.7 | 203.7 | | | | | | | | | | |
| 40600285 | POLYMERIZED BITUMINOUS MATERIALS (PRIME COAT) | POUND | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | |
| 40600990 | TEMPORARY RAMP | SQ YD | 116 | 116 | | | | | | | | | | |
| 40603085 | HOT-MIX ASPHALT BINDER COURSE, IL-19, 0, N70 | TON | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | |
| 40603235 | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19, 0, N70 | TON | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | |
| 40603310 | HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 | TON | 1178 | 1178 | | | | | | | | | | |
| 40603540 | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | TON | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | |
| 40800050 | INCIDENTAL HOT-MIX ASPHALT SURFACING | TON | 233 | 233 | | | | | | | | | | |
| 42000401 | PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED) | SQ YD | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | |
| 42000406 | PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED) | SQ YD | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | |
| 42001200 | PAVEMENT FABRIC | SQ YD | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | |
| 42001300 | PROTECTIVE COAT | SQ YD | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | |

| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | | | | | | | | |
|----------|---|-------|---|--------------------------|----------------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|--|--|--|--|--|--|--|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | | | | | | | |
| | | | | ROADWAY 0001 RURAL | HIGHWAY LIGHTING 0021 NONE | BRIDGE 0008 055-0046 | BRIDGE 0008 055-0068 | BRIDGE 0008 055-0070 | BRIDGE 0008 055-0072 | BRIDGE 0008 055-0074 | BRIDGE 0008 055-0075 | BRIDGE 0008 055-0077 | BRIDGE 0008 055-0078 | | | | | | | |
| 42001420 | BRIDGE APPROACH PAVEMENT CONNECTOR (PCC) | SQ YD | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | | | | | | | |
| 42001430 | BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE) | SQ YD | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | | | | | | | |
| 44000100 | PAVEMENT REMOVAL | SQ YD | 24668 | 24668 | | | | | | | | | | | | | | | | |
| 44000157 | HOT-MIX ASPHALT SURFACE REMOVAL, 2" | SQ YD | 11603 | 11603 | | | | | | | | | | | | | | | | |
| 44201741 | CLASS D PATCHES, TYPE II, 8 INCH | SQ YD | 22 | 22 | | | | | | | | | | | | | | | | |
| 48101200 | AGGREGATE SHOULDERS, TYPE B | TON | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | | | | | | | |
| 48203029 | HOT-MIX ASPHALT SHOULDERS, 8" | SQ YD | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | | | | | | | |
| 48300400 | PORTLAND CEMENT CONCRETE SHOULDERS 9" | SQ YD | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | | | | | | | |
| 48300405 | PORTLAND CEMENT CONCRETE SHOULDERS 9 1/4" | SQ YD | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | | | | | | | |
| 50105220 | PIPE CULVERT REMOVAL | FOOT | 160 | 160 | | | | | | | | | | | | | | | | |
| 50200100 | STRUCTURE EXCAVATION | CU YD | 2570 | | | 192 | 232 | 849 | 196 | 121 | 214 | 406 | 360 | | | | | | | |
| 50300225 | CONCRETE STRUCTURES | CU YD | 1750.7 | | | 661.3 | 65.6 | 378.8 | 85.4 | 135.7 | 89.7 | 228.2 | 106.0 | | | | | | | |
| 50300255 | CONCRETE SUPERSTRUCTURE | CU YD | 3326.5 | | | 1029.3 | 225 | 408.5 | 234.8 | 405.3 | 250.5 | 419.1 | 354.0 | | | | | | | |
| 50300260 | BRIDGE DECK GROOVING | SQ YD | 8256 | | | 2989 | 479 | 923 | 490 | 955 | 504 | 1070 | 846 | | | | | | | |

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|----------|--|-------|-------------------------------------|--------------------------|----------------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% |
| | | | | ROADWAY 0001 RURAL | HIGHWAY LIGHTING 0021 NONE | BRIDGE 0008 055-0046 | BRIDGE 0008 055-0068 | BRIDGE 0008 055-0070 | BRIDGE 0008 055-0072 | BRIDGE 0008 055-0074 | BRIDGE 0008 055-0075 | BRIDGE 0008 055-0077 | BRIDGE 0008 055-0078 |
| 50300280 | CONCRETE ENCASEMENT | CU YD | 33.2 | | | 9.2 | | | | 6.6 | | 13.4 | 4.0 |
| 50300300 | PROTECTIVE COAT | SQ YD | 10414 | | | 3724 | 564 | 1129 | 593 | 1259 | 613 | 1416 | 1116 |
| 50400805 | FURNISHING AND ERECTING PRECAST PRESTRESSED CONCRETE I-BEAMS, 36 IN. | FOOT | 1517.5 | | | | 301.5 | 887 | 329 | | | | |
| 50400905 | FURNISHING AND ERECTING PRECAST PRESTRESSED CONCRETE I-BEAMS, 42 IN. | FOOT | 350 | | | | | | | | 350.0 | | |
| 50500105 | FURNISHING AND ERECTING STRUCTURAL STEEL | L SUM | 1 | | | 0.6 | | | | 0.13 | | 0.17 | 0.10 |
| 50500505 | STUD SHEAR CONNECTORS | EACH | 14814 | | | 7128 | | | | 2538 | | 2664 | 2484 |
| 50800105 | REINFORCEMENT BARS | POUND | 109230 | | | 109230 | | | | | | | |
| 50800205 | REINFORCEMENT BARS, EPOXY COATED | POUND | 1071910 | | | 374380 | 61020 | 154270 | 68350 | 114000 | 68960 | 129580 | 101350 |
| 50800515 | BAR SPLICERS | EACH | 506 | | | 194 | 88 | | | 68 | | 84 | 72 |
| 50800530 | MECHANICAL SPLICERS | EACH | 216 | | | 216 | | | | | | | |
| 51100100 | SLOPE WALL 4 INCH | SQ YD | 1925 | | | | | 633 | | 424 | | 484 | 384 |
| 51200958 | FURNISHING METAL SHELL PILES 14" X 0.250" | FOOT | 630 | | | | | | 630 | | | | |
| 51200959 | FURNISHING METAL SHELL PILES 14" X 0.312" | FOOT | 256 | | | | 256 | | | | | | |
| 51201600 | FURNISHING STEEL PILES HP12X53 | FOOT | 5419 | | | 1440 | | | | 1054 | | 1040 | 1885 |

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| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | | |
|------------|--------------------------------|-------|-------------------------------------|--------------------------|----------------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|---|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | |
| | | | | ROADWAY 0001 RURAL | HIGHWAY LIGHTING 0021 NONE | BRIDGE 0008 055-0046 | BRIDGE 0008 055-0068 | BRIDGE 0008 055-0070 | BRIDGE 0008 055-0072 | BRIDGE 0008 055-0074 | BRIDGE 0008 055-0075 | BRIDGE 0008 055-0077 | BRIDGE 0008 055-0078 | |
| 51201610 | FURNISHING STEEL PILES HP12X63 | FOOT | 3540 | | | | | | | | 732 | 2808 | | |
| 51201800 | FURNISHING STEEL PILES HP14X73 | FOOT | 585 | | | | | | | | 585 | | | |
| 51201900 | FURNISHING STEEL PILES HP14X89 | FOOT | 3358 | | | | | 3358 | | | | | | |
| 51202305 | DRIVING PILES | FOOT | 13788 | | | 1440 | 256 | 3358 | 630 | 1639 | 732 | 3848 | 1885 | |
| 51203200 | TEST PILE METAL SHELLS | EACH | 3 | | | | 1 | | 2 | | | | | |
| 51203600 | TEST PILE STEEL HP12X53 | EACH | 7 | | | 2 | | | | 1 | | 1 | 3 | |
| 51203610 | TEST PILE STEEL HP12X63 | EACH | 4 | | | | | | | | 2 | 2 | | |
| 51203800 | TEST PILE STEEL HP14X73 | EACH | 2 | | | | | | | | 2 | | | |
| 51203900 | TEST PILE STEEL HP14X89 | EACH | 4 | | | | | 4 | | | | | | |
| 51204650 | PILE SHOES | EACH | 50 | | | 26 | 12 | | 12 | | | | | |
| 51500100 | NAME PLATES | EACH | 9 | 1 | | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 |
| 51602000 | PERMANENT CASING | FOOT | 171 | | | 171 | | | | | | | | |
| * 51603000 | DRILLED SHAFT IN SOIL | CU YD | 179.5 | | | 179.5 | | | | | | | | |
| * 51604000 | DRILLED SHAFT IN ROCK | CU YD | 237.6 | | | 237.6 | | | | | | | | |

* SPECIALTY ITEM

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|---|--------------------|---------------------------|--------------------|---|---|-----------------------------|-----------|--------|-----------------|--------------|
| FILE NAME v:\transportation\289\2-lane paving plan | USER NAME JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) SUMMARY OF QUANTITIES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| PLT SCALE = 1/8" = 1'-0" | DRAWN - TJD | REVISIONS | 407 | | | 55C3(PV;HB(2-6);B,B-1,B-2)) | McDONOUGH | 874 | 11 | |
| PLT DATE = 1/29/2015 | CHECKED - JRB | REVISIONS | CONTRACT NO. 68B44 | | | | | | | |
| DATE - 1-30-2015 | REVISIONS | ILLINOIS FED. AID PROJECT | | | | | | | | |
| SCALE: NONE | | | | SHEET 7 OF 21 SHEETS STA. TO STA. | | | | | | |

| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% | CONSTRUCTION CODE | | | | | | | | | |
|-------------------|---|--------------|----------------------|----------------------|----------------------|----------------------|----------------------|----------------------|----------------------|----------------------|----------------------|----------------------|----------------------|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% |
| | | | | ROADWAY | HIGHWAY LIGHTING | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE |
| TOTAL QUANTITY | 0001 RURAL | 0021 NONE | 0008 | 0008 | 0008 | 0008 | 0008 | 0008 | 0008 | 0008 | 0008 | | |
| | | | | 055-0046 | 055-0068 | 055-0070 | 055-0072 | 055-0074 | 055-0075 | 055-0077 | 055-0078 | | |
| 52000110 | PREFORMED JOINT STRIP SEAL | FOOT | 88 | | | | | | | | 88 | | |
| 52000212 | FINGER PLATE EXPANSION JOINT, 4" | FOOT | 80 | | | 80 | | | | | | | |
| 52100020 | ELASTOMERIC BEARING ASSEMBLY, TYPE II | EACH | 12 | | | | | | | | 12 | | |
| 52100030 | ELASTOMERIC BEARING ASSEMBLY, TYPE III | EACH | 12 | | | 12 | | | | | | | |
| 52100505 | ANCHOR BOLTS, 5/8" | EACH | 24 | | | | | | | | 24 | | |
| 52100510 | ANCHOR BOLTS, 3/4" | EACH | 24 | | | 24 | | | | | | | |
| 52100520 | ANCHOR BOLTS, 1" | EACH | 60 | | | | | | 24 | | | 36 | |
| 52100530 | ANCHOR BOLTS, 1 1/4" | EACH | 24 | | | | | | 12 | | 12 | | |
| 52100540 | ANCHOR BOLTS, 1 1/2" | EACH | 36 | | | 36 | | | | | | | |
| 54001001 | BOX CULVERT END SECTIONS, CULVERT NO. 1 | EACH | 1 | 1 | | | | | | | | | |
| 542A0229 | PIPE CULVERTS, CLASS A, TYPE 1 24" | FOOT | 180 | 180 | | | | | | | | | |
| 542A1063 | PIPE CULVERTS, CLASS A, TYPE 2 18" | FOOT | 40 | 40 | | | | | | | | | |
| 542A1069 | PIPE CULVERTS, CLASS A, TYPE 2 24" | FOOT | 196 | 196 | | | | | | | | | |
| 542A1081 | PIPE CULVERTS, CLASS A, TYPE 2 36" | FOOT | 40 | 40 | | | | | | | | | |

FILE NAME: v:\transportation\2891\2-lane paving plots\cadd sheets\0460944-sh1-50008.dgn
 USER NAME: JDaen
 DESIGNED - JRB
 DRAWN - TJD
 CHECKED - JRB
 DATE - 1-30-2015
 PLOT SCALE: 1/8" = 1'-0"
 PLOT DATE: 1/29/2015

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP ROUTE 407 (IL 336 /IL 110)
SUMMARY OF QUANTITIES**

SCALE: NONE SHEET 8 OF 21 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|---------------------------|-----------|--------------------|--------------|
| 407 | 55[3PV;HB(2-6);8,8-1,8-2] | MCDONOUGH | 874 | 12 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 68844 | |

| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | |
|----------|---|------|-------------------------------------|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% |
| | | | | ROADWAY | HIGHWAY LIGHTING | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE | BRIDGE |
| | | | | 0001 | 0021 | 0008 | 0008 | 0008 | 0008 | 0008 | 0008 | 0008 | 0008 |
| | | | | RURAL | NONE | 055-0046 | 055-0068 | 055-0070 | 055-0072 | 055-0074 | 055-0075 | 055-0077 | 055-0078 |
| 542A4009 | PIPE CULVERTS, CLASS A, TYPE 6 24" | FOOT | 180 | 180 | | | | | | | | | |
| 542D0220 | PIPE CULVERTS, CLASS D, TYPE 1 15" | FOOT | 84 | 84 | | | | | | | | | |
| 542D1060 | PIPE CULVERTS, CLASS D, TYPE 2 15" | FOOT | 160 | 160 | | | | | | | | | |
| 542D1063 | PIPE CULVERTS, CLASS D, TYPE 2 18" | FOOT | 68 | 68 | | | | | | | | | |
| 542D1069 | PIPE CULVERTS, CLASS D, TYPE 2 24" | FOOT | 98 | 98 | | | | | | | | | |
| 54213657 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12" | EACH | 1 | 1 | | | | | | | | | |
| 54213663 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18" | EACH | 1 | 1 | | | | | | | | | |
| 54213669 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24" | EACH | 10 | 10 | | | | | | | | | |
| 54213675 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 30" | EACH | 1 | 1 | | | | | | | | | |
| 54213681 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 36" | EACH | 2 | 2 | | | | | | | | | |
| 54215550 | METAL END SECTIONS 15" | EACH | 6 | 6 | | | | | | | | | |
| 54215553 | METAL END SECTIONS 18" | EACH | 2 | 2 | | | | | | | | | |
| 54215559 | METAL END SECTIONS 24" | EACH | 2 | 2 | | | | | | | | | |
| 550A0050 | STORM SEWERS, CLASS A, TYPE 112" | FOOT | 136 | 136 | | | | | | | | | |

| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | | | | | | | | |
|----------|---|-------|-------------------------------------|--------------------------|----------------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|--|--|--|--|-----|--|--|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | | | | | | | |
| | | | | ROADWAY 0001 RURAL | HIGHWAY LIGHTING 0021 NONE | BRIDGE 0008 055-0046 | BRIDGE 0008 055-0068 | BRIDGE 0008 055-0070 | BRIDGE 0008 055-0072 | BRIDGE 0008 055-0074 | BRIDGE 0008 055-0075 | BRIDGE 0008 055-0077 | BRIDGE 0008 055-0078 | | | | | | | |
| 550A0070 | STORM SEWERS, CLASS A, TYPE 1 15" | FOOT | 494 | 494 | | | | | | | | | | | | | | | | |
| 550A0090 | STORM SEWERS, CLASS A, TYPE 1 18" | FOOT | 110 | 110 | | | | | | | | | | | | | | | | |
| 550A0360 | STORM SEWERS, CLASS A, TYPE 2 15" | FOOT | 336 | 336 | | | | | | | | | | | | | | | | |
| 550A0380 | STORM SEWERS, CLASS A, TYPE 2 18" | FOOT | 146 | 146 | | | | | | | | | | | | | | | | |
| 550A0430 | STORM SEWERS, CLASS A, TYPE 2 30" | FOOT | 104 | 104 | | | | | | | | | | | | | | | | |
| 58700300 | CONCRETE SEALER | SQ FT | 3728 | | | 1143 | | | 1596 | | | | | | | | | 989 | | |
| 59100100 | GEOCOMPOSITE WALL DRAIN | SQ YD | 832 | | | 161 | 90 | 98 | 91 | 81 | 100 | 134 | 77 | | | | | | | |
| 60100060 | CONCRETE HEADWALLS FOR PIPE DRAINS | EACH | 195 | 195 | | | | | | | | | | | | | | | | |
| 60107600 | PIPE UNDERDRAINS 4" | FOOT | 99064 | 99064 | | | | | | | | | | | | | | | | |
| 60108100 | PIPE UNDERDRAINS 4" (SPECIAL) | FOOT | 3753 | 3753 | | | | | | | | | | | | | | | | |
| 60219530 | MANHOLES, TYPE A, 4' -DIAMETER, TYPE 23 FRAME AND GRATE | EACH | 1 | 1 | | | | | | | | | | | | | | | | |
| 60222230 | MANHOLES, TYPE A, 5' -DIAMETER, TYPE 23 FRAME AND GRATE | EACH | 2 | 2 | | | | | | | | | | | | | | | | |
| 60237460 | INLETS, TYPE A, TYPE 23 FRAME AND GRATE | EACH | 11 | 11 | | | | | | | | | | | | | | | | |
| 60240327 | INLETS, TYPE B, TYPE 23 FRAME AND GRATE | EACH | 8 | 8 | | | | | | | | | | | | | | | | |

| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | | | | | | | | |
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| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | | | | | | | |
| | | | | ROADWAY 0001 RURAL | HIGHWAY LIGHTING 0021 NONE | BRIDGE 0008 055-0046 | BRIDGE 0008 055-0068 | BRIDGE 0008 055-0070 | BRIDGE 0008 055-0072 | BRIDGE 0008 055-0074 | BRIDGE 0008 055-0075 | BRIDGE 0008 055-0077 | BRIDGE 0008 055-0078 | | | | | | | |
| 60608552 | COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4, 06 | FOOT | 658.5 | 658.5 | | | | | | | | | | | | | | | | |
| 60608582 | COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4, 24 | FOOT | 3187.0 | 3187.0 | | | | | | | | | | | | | | | | |
| 60610400 | COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6, 24 | FOOT | 61.5 | 61.5 | | | | | | | | | | | | | | | | |
| 60618300 | CONCRETE MEDIAN SURFACE, 4 INCH | SQ FT | 162 | 162 | | | | | | | | | | | | | | | | |
| 60618320 | CONCRETE MEDIAN SURFACE, 6 INCH | SQ FT | 4446 | 4446 | | | | | | | | | | | | | | | | |
| * 63000001 | STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS | FOOT | 9888 | 9888 | | | | | | | | | | | | | | | | |
| * 63100085 | TRAFFIC BARRIER TERMINAL, TYPE 6 | EACH | 32 | 32 | | | | | | | | | | | | | | | | |
| * 63100167 | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT | EACH | 44 | 44 | | | | | | | | | | | | | | | | |
| 63200310 | GUARDRAIL REMOVAL | FOOT | 150 | 150 | | | | | | | | | | | | | | | | |
| 63500105 | DELINEATORS | EACH | 279 | 279 | | | | | | | | | | | | | | | | |
| 64200116 | SHOULDER RUMBLE STRIPS, 16 INCH | FOOT | 77610 | 77610 | | | | | | | | | | | | | | | | |
| 64300260 | IMPACT ATTENUATORS (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3 | EACH | 2 | 2 | | | | | | | | | | | | | | | | |
| 64300450 | IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3 | EACH | 4 | 4 | | | | | | | | | | | | | | | | |
| 64301090 | ATTENUATOR BASE | SQ YD | 110 | 110 | | | | | | | | | | | | | | | | |

* SPECIALTY ITEM

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|---|-----------------------|------------------|--------------------|---|---|----------------------------|-----------------------|--------------|---------------------------|-----------|
| FILE NAME * | USER NAME * Jdwen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) SUMMARY OF QUANTITIES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| \\transportation\289\2-one paving plan\add sheets\0468844-shs-50011.dgn | DRAWN - TJD | REVISED - | 407 | | | 55C31PV;HB12-6);B,B-1,B-2) | McDONOUGH | 874 | 15 | |
| PLOT SCALE = 1/8"=1'-0" IN. | CHECKED - JRB | REVISED - | CONTRACT NO. 68B44 | | | | | | | |
| Default | PLOT DATE = 1/29/2015 | DATE = 1-30-2015 | REVISED - | | | SCALE: NONE | SHEET 11 OF 21 SHEETS | STA. TO STA. | ILLINOIS FED. AID PROJECT | |

| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | | | | | | | | |
|----------|---|--------|-------------------------------------|--------------------------|----------------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|--|--|--|--|--|--|--|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | | | | | | | |
| | | | | ROADWAY 0001 RURAL | HIGHWAY LIGHTING 0021 NONE | BRIDGE 0008 055-0046 | BRIDGE 0008 055-0068 | BRIDGE 0008 055-0070 | BRIDGE 0008 055-0072 | BRIDGE 0008 055-0074 | BRIDGE 0008 055-0075 | BRIDGE 0008 055-0077 | BRIDGE 0008 055-0078 | | | | | | | |
| 66400510 | CHAIN LINK FENCE, 10' | FOOT | 5985 | 5985 | | | | | | | | | | | | | | | | |
| 66500105 | WOVEN WIRE FENCE, 4' | FOOT | 1685 | 1685 | | | | | | | | | | | | | | | | |
| 66600105 | FURNISHING AND ERECTING RIGHT OF WAY MARKERS | EACH | 20 | 20 | | | | | | | | | | | | | | | | |
| 66700205 | PERMANENT SURVEY MARKERS, TYPE I | EACH | 10 | 10 | | | | | | | | | | | | | | | | |
| 66700305 | PERMANENT SURVEY MARKERS, TYPE II | EACH | 8 | 8 | | | | | | | | | | | | | | | | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 25 | 25 | | | | | | | | | | | | | | | | |
| 67000600 | ENGINEER'S FIELD LABORATORY | CAL MO | 25 | 25 | | | | | | | | | | | | | | | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | | | | | | | | | | | | | | | | |
| 70100700 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701406 | L SUM | 1 | 1 | | | | | | | | | | | | | | | | |
| 70101830 | TRAFFIC CONTROL AND PROTECTION, STANDARD BLR 21 | L SUM | 1 | 1 | | | | | | | | | | | | | | | | |
| 70101835 | TRAFFIC CONTROL AND PROTECTION, STANDARD BLR 22 | L SUM | 1 | 1 | | | | | | | | | | | | | | | | |
| 70103815 | TRAFFIC CONTROL SURVEILLANCE | CAL DA | 90 | 90 | | | | | | | | | | | | | | | | |
| 70106800 | CHANGEABLE MESSAGE SIGN | CAL MO | 6 | 6 | | | | | | | | | | | | | | | | |
| 70300210 | TEMPORARY PAVEMENT MARKING LETTERS & SYMBOLS | SQ FT | 563 | 563 | | | | | | | | | | | | | | | | |

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| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | | | | | | | | |
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| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | | | | | | | |
| | | | | ROADWAY 0001 RURAL | HIGHWAY LIGHTING 0021 NONE | BRIDGE 0008 055-0046 | BRIDGE 0008 055-0068 | BRIDGE 0008 055-0070 | BRIDGE 0008 055-0072 | BRIDGE 0008 055-0074 | BRIDGE 0008 055-0075 | BRIDGE 0008 055-0077 | BRIDGE 0008 055-0078 | | | | | | | |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 46868 | 46868 | | | | | | | | | | | | | | | | |
| 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6" | FOOT | 1573 | 1573 | | | | | | | | | | | | | | | | |
| 70300250 | TEMPORARY PAVEMENT MARKING - LINE 8" | FOOT | 2894 | 2894 | | | | | | | | | | | | | | | | |
| 70300260 | TEMPORARY PAVEMENT MARKING - LINE 12" | FOOT | 1626 | 1626 | | | | | | | | | | | | | | | | |
| 70300280 | TEMPORARY PAVEMENT MARKING - LINE 24" | FOOT | 104 | 104 | | | | | | | | | | | | | | | | |
| 70300520 | PAVEMENT MARKING TAPE, TYPE III 4" | FOOT | 12910 | 12910 | | | | | | | | | | | | | | | | |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 25037 | 25037 | | | | | | | | | | | | | | | | |
| 70400100 | TEMPORARY CONCRETE BARRIER | FOOT | 5137.5 | 5137.5 | | | | | | | | | | | | | | | | |
| 70400200 | RELOCATE TEMPORARY CONCRETE BARRIER | FOOT | 4962.5 | 4962.5 | | | | | | | | | | | | | | | | |
| 70600241 | IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE, NARROW), TEST LEVEL 2 | EACH | 2 | 2 | | | | | | | | | | | | | | | | |
| 70600341 | IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE, NARROW), TEST LEVEL 2 | EACH | 2 | 2 | | | | | | | | | | | | | | | | |
| * 72000100 | SIGN PANEL - TYPE 1 | SQ FT | 958 | 958 | | | | | | | | | | | | | | | | |
| * 72000200 | SIGN PANEL - TYPE 2 | SQ FT | 1205.5 | 1205.5 | | | | | | | | | | | | | | | | |
| * 72000300 | SIGN PANEL - TYPE 3 | SQ FT | 1232.5 | 1232.5 | | | | | | | | | | | | | | | | |

* SPECIALTY ITEM

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|---|-------------------|----------------|---------------------------|---|---|--|-----------------------|----------------------------|-----------|--------------|--------------|-----------|
| FILE NAME = | USER NAME = JDeem | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) SUMMARY OF QUANTITIES | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\289112-lane paving plan\cadd sheets\0462844\sh1-50013.dgn | DRAWN - TJD | REVISED - | 407 | | | | | 55C3(PV,HB(2-6);B,B-1,B-2) | McDONOUGH | 874 | 17 | |
| PLOT SCALE = 100.0000' / IN. | CHECKED - JRB | REVISED - | CONTRACT NO. 68844 | | | | | | | | | |
| Default | DATE - 1-30-2015 | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | | | |
| | | | | SCALE: NONE | | | SHEET 13 OF 21 SHEETS | | | STA. TO STA. | | |

| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | | | | | | | | |
|------------|--|-------|-------------------------------------|--------------------------|----------------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|--|--|--|--|--|--|--|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | | | | | | | |
| | | | | ROADWAY 0001 RURAL | HIGHWAY LIGHTING 0021 NONE | BRIDGE 0008 055-0046 | BRIDGE 0008 055-0068 | BRIDGE 0008 055-0070 | BRIDGE 0008 055-0072 | BRIDGE 0008 055-0074 | BRIDGE 0008 055-0075 | BRIDGE 0008 055-0077 | BRIDGE 0008 055-0078 | | | | | | | |
| * 72400100 | REMOVE SIGN PANEL ASSEMBLY - TYPE A | EACH | 30 | 30 | | | | | | | | | | | | | | | | |
| * 72400200 | REMOVE SIGN PANEL ASSEMBLY - TYPE B | EACH | 40 | 40 | | | | | | | | | | | | | | | | |
| * 72400310 | REMOVE SIGN PANEL - TYPE 1 | SQ FT | 2.2 | 2.2 | | | | | | | | | | | | | | | | |
| * 72400320 | REMOVE SIGN PANEL - TYPE 2 | SQ FT | 13.8 | 13.8 | | | | | | | | | | | | | | | | |
| * 72700100 | STRUCTURAL STEEL SIGN SUPPORT - BREAKAWAY | POUND | 5011.2 | 5011.2 | | | | | | | | | | | | | | | | |
| * 72800100 | TELESCOPING STEEL SIGN SUPPORT | FOOT | 138 | 138 | | | | | | | | | | | | | | | | |
| * 73000100 | WOOD SIGN SUPPORT | FOOT | 2736 | 2736 | | | | | | | | | | | | | | | | |
| * 73100100 | BASE FOR TELESCOPING STEEL SIGN SUPPORT | EACH | 1 | 1 | | | | | | | | | | | | | | | | |
| * 73400100 | CONCRETE FOUNDATIONS | CU YD | 11.3 | 11.3 | | | | | | | | | | | | | | | | |
| * 78009000 | MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 449 | 449 | | | | | | | | | | | | | | | | |
| * 78009004 | MODIFIED URETHANE PAVEMENT MARKING - LINE 4" | FOOT | 134058 | 134058 | | | | | | | | | | | | | | | | |
| * 78009006 | MODIFIED URETHANE PAVEMENT MARKING - LINE 6" | FOOT | 2764 | 2764 | | | | | | | | | | | | | | | | |
| * 78009008 | MODIFIED URETHANE PAVEMENT MARKING - LINE 8" | FOOT | 8459 | 8459 | | | | | | | | | | | | | | | | |
| * 78009012 | MODIFIED URETHANE PAVEMENT MARKING - LINE 12" | FOOT | 1881 | 1881 | | | | | | | | | | | | | | | | |

* SPECIALTY ITEM

| | | | | | | | | | | |
|--|-------------------|----------------|---------------------------|---|---|---------------------------|-----------|--------|--------------|-----------|
| FILE NAME * | USER NAME * JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) SUMMARY OF QUANTITIES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| \\transportation\289\lane paving files\add sheets\0468044-sh-50214.dgn | DRAWN - TJD | REVISED - | 407 | | | 55C31PV:HB(2-6)8.8-1.B-27 | McDONOUGH | 874 | 18 | |
| PLOT SCALE = 1/8"=1'-0" / IN. | CHECKED - JRB | REVISED - | CONTRACT NO. 68B44 | | | | | | | |
| Default | DATE - 1-30-2015 | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |

| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | | | | | | | | |
|------------|---|-------|-------------------------------------|--------------------------|----------------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|--|--|--|--|--|--|--|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | | | | | | | |
| | | | | ROADWAY 0001 RURAL | HIGHWAY LIGHTING 0021 NONE | BRIDGE 0008 055-0046 | BRIDGE 0008 055-0068 | BRIDGE 0008 055-0070 | BRIDGE 0008 055-0072 | BRIDGE 0008 055-0074 | BRIDGE 0008 055-0075 | BRIDGE 0008 055-0077 | BRIDGE 0008 055-0078 | | | | | | | |
| * 78009024 | MODIFIED URETHANE PAVEMENT MARKING - LINE 24" | FOOT | 168 | 168 | | | | | | | | | | | | | | | | |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 840 | 840 | | | | | | | | | | | | | | | | |
| * 78100105 | RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) | EACH | 40 | 40 | | | | | | | | | | | | | | | | |
| * 78200300 | PRISMATIC CURB REFLECTOR | EACH | 210 | 210 | | | | | | | | | | | | | | | | |
| * 78200410 | GUARDRAIL MARKERS, TYPE A | EACH | 92 | 92 | | | | | | | | | | | | | | | | |
| * 78200530 | BARRIER WALL MARKERS, TYPE C | EACH | 569 | 569 | | | | | | | | | | | | | | | | |
| * 78201000 | TERMINAL MARKER - DIRECT APPLIED | EACH | 40 | 40 | | | | | | | | | | | | | | | | |
| 78300100 | PAVEMENT MARKING REMOVAL | SQ FT | 3674 | 3674 | | | | | | | | | | | | | | | | |
| 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 70 | 70 | | | | | | | | | | | | | | | | |
| * 80400100 | ELECTRIC SERVICE INSTALLATION | EACH | 2 | | | 2 | | | | | | | | | | | | | | |
| * 81028770 | UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 3" DIA. | FOOT | 1032 | | | 1032 | | | | | | | | | | | | | | |
| * 81603000 | UNIT DUCT, 600V, 2-1C NO. 8, 1/C NO. 8 GROUND, (XLP-TYPE USE), 3/4" DIA. POLYETHYLENE | FOOT | 14485 | | | 14485 | | | | | | | | | | | | | | |
| * 81702420 | ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/C NO. 8 | FOOT | 540 | | | 540 | | | | | | | | | | | | | | |
| * 82103900 | LUMINAIRE, SODIUM VAPOR, MULTI-MOUNT, 250 WATT | EACH | 58 | | | 58 | | | | | | | | | | | | | | |

* SPECIALTY ITEM

| | | | | | | | | | | |
|--|-------------------------------------|------------------|-----------|---|---|---------------------------|-----------------------------|-----------|--------------|--------------------|
| FILE NAME * | USER NAME * JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) SUMMARY OF QUANTITIES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| \\transportation\2891\2-lane paving plan | \\cadd\sheet\10468844-shs-50015.dgn | DRAWN - TJD | REVISED - | | | 407 | 55C3(PV)HB(2-6);B;B-1,B-2)) | McDONOUGH | 874 | 19 |
| Default | PLDT SCALE = 1/8"=1'-0" IN. | CHECKED - JRB | REVISED - | | | SCALE: NONE | SHEET 15 OF 21 SHEETS | STA. | TO STA. | CONTRACT NO. 68844 |
| | PLDT DATE = 1/29/2015 | DATE - 1-30-2015 | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | |

| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | | | | | | | | |
|------------|--|-------|---|--------------------------|----------------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|--|--|--|--|--|--|--|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | | | | | | | |
| | | | | ROADWAY 0001 RURAL | HIGHWAY LIGHTING 0021 NONE | BRIDGE 0008 055-0046 | BRIDGE 0008 055-0068 | BRIDGE 0008 055-0070 | BRIDGE 0008 055-0072 | BRIDGE 0008 055-0074 | BRIDGE 0008 055-0075 | BRIDGE 0008 055-0077 | BRIDGE 0008 055-0078 | | | | | | | |
| * 82500320 | LIGHTING CONTROLLER, POLE MOUNTED, 480VOLT, 60AMP | EACH | 2 | | 2 | | | | | | | | | | | | | | | |
| * 83060820 | LIGHT POLE, GALVANIZED STEEL, 40 FT. M. H., TENON MOUNT | EACH | 3 | | 3 | | | | | | | | | | | | | | | |
| * 83060830 | LIGHT POLE, GALVANIZED STEEL, 45 FT. M. H., TENON MOUNT | EACH | 51 | | 51 | | | | | | | | | | | | | | | |
| * 83060835 | LIGHT POLE, GALVANIZED STEEL, 45 FT. M. H., TENON MOUNT-TWIN | EACH | 2 | | 2 | | | | | | | | | | | | | | | |
| * 83600300 | LIGHT POLE FOUNDATION, 30" DIAMETER | FOOT | 358 | | 358 | | | | | | | | | | | | | | | |
| * 83800205 | BREAKAWAY DEVICE, TRANSFORMER BASE, 15 INCH BOLT CIRCLE | EACH | 53 | | 53 | | | | | | | | | | | | | | | |
| * 84200804 | REMOVAL OF POLE FOUNDATION | EACH | 2 | | 2 | | | | | | | | | | | | | | | |
| * 84400105 | RELOCATE EXISTING LIGHTING UNIT | EACH | 2 | | 2 | | | | | | | | | | | | | | | |
| * 84500110 | REMOVAL OF LIGHTING CONTROLLER | EACH | 2 | | 2 | | | | | | | | | | | | | | | |
| * 84500120 | REMOVAL OF ELECTRIC SERVICE INSTALLATION | EACH | 2 | | 2 | | | | | | | | | | | | | | | |
| X0324159 | WHITEWASHING FOR CONCRETE PAVEMENT | SQ YD | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | | | | | | | |
| X0325349 | TEMPORARY CONCRETE BARRIER (TO REMAIN PERMANENTLY) | FOOT | 1725 | 1725 | | | | | | | | | | | | | | | | |
| X0327547 | ABANDON EXISTING UTILITIES | L SUM | 1 | 1 | | | | | | | | | | | | | | | | |
| X4020400 | AGGREGATE SURFACE COURSE, TYPE B 4" | SQ YD | 900 | 900 | | | | | | | | | | | | | | | | |

* SPECIALTY ITEM

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|---|-------------------|----------------|-------------|---|---|-----------------------|---------------------------------------|---------------------------|---------------------|-----------------|--|--|
| FILE NAME * v:\transportation\2594\2-lane paving plots\loads sheets\468844-shr-50015.dgn | USER NAME * JDenn | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) SUMMARY OF QUANTITIES | F.A.P. RTE. 407 | SECTION 55C3(PV,H812-5);8.8-1,8-2) | COUNTY McDONOUGH | TOTAL SHEETS 874 | SHEET NO. 20 | | |
| PLOT SCALE * 1/8"=1'-0" / IN. | CHECKED - JRB | REVISOR - | SCALE: NONE | | | SHEET 16 OF 21 SHEETS | STA. TO STA. | ILLINOIS FED. AID PROJECT | | | | |
| PLOT DATE * 1/29/2015 | DATE - 1-30-2015 | REVISOR - | | | | | | | | | | |
| Default | | | | | | | | | | | | |

| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | | | | | | | | |
|----------|---|-------|-------------------------------------|--------------------------|----------------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|--|--|--|--|--|--|--|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | | | | | | | |
| | | | | ROADWAY 0001 RURAL | HIGHWAY LIGHTING 0021 NONE | BRIDGE 0008 055-0046 | BRIDGE 0008 055-0068 | BRIDGE 0008 055-0070 | BRIDGE 0008 055-0072 | BRIDGE 0008 055-0074 | BRIDGE 0008 055-0075 | BRIDGE 0008 055-0077 | BRIDGE 0008 055-0078 | | | | | | | |
| X5010520 | REMOVE CONCRETE BOX CULVERT END SECTION | EACH | 4 | 4 | | | | | | | | | | | | | | | | |
| X5860110 | GRANULAR BACKFILL FOR STRUCTURES | CU YD | 1405 | | | 218 | 146 | 194 | 189 | 153 | 202 | 169 | 134 | | | | | | | |
| X6023610 | MANHOLES, TYPE A, 4'-DIAMETER, WITH MEDIAN INLET, SPECIAL | EACH | 1 | 1 | | | | | | | | | | | | | | | | |
| X6023615 | MANHOLES, TYPE A, 5'-DIAMETER, WITH MEDIAN INLET, SPECIAL | EACH | 3 | 3 | | | | | | | | | | | | | | | | |
| X6050700 | REMOVE INLET BOX | EACH | 2 | 2 | | | | | | | | | | | | | | | | |
| X6060097 | CLASS SI CONCRETE (OUTLET), SPECIAL | CU YD | 20.0 | 20.0 | | | | | | | | | | | | | | | | |
| X6061902 | CONCRETE MEDIAN, TYPE SM (SPECIAL) | SQ FT | 2272 | 2272 | | | | | | | | | | | | | | | | |
| X6062400 | CONCRETE GUTTER (SPECIAL) | FOOT | 792.0 | 792.0 | | | | | | | | | | | | | | | | |
| X6062700 | CONCRETE GUTTER, TYPE A (SPECIAL) | FOOT | 730.0 | 730.0 | | | | | | | | | | | | | | | | |
| X6350110 | DELINEATORS (SPECIAL) | EACH | 44 | 44 | | | | | | | | | | | | | | | | |
| X6660410 | REMOVE RIGHT-OF-WAY MARKERS | EACH | 14 | 14 | | | | | | | | | | | | | | | | |
| X6670105 | PERMANENT SURVEY MARKERS (SPECIAL) | EACH | 3 | 3 | | | | | | | | | | | | | | | | |
| X6670109 | PERMANENT SURVEY TIES | EACH | 52 | 52 | | | | | | | | | | | | | | | | |
| XT010206 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701401 (SPECIAL) | L SUM | 1 | 1 | | | | | | | | | | | | | | | | |

| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | | | | | | | | |
|------------|---|--------|---|--------------------------|----------------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|--|--|--|--|--|--|--|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | | | | | | | |
| | | | | ROADWAY 0001 RURAL | HIGHWAY LIGHTING 0021 NONE | BRIDGE 0008 055-0046 | BRIDGE 0008 055-0068 | BRIDGE 0008 055-0070 | BRIDGE 0008 055-0072 | BRIDGE 0008 055-0074 | BRIDGE 0008 055-0075 | BRIDGE 0008 055-0077 | BRIDGE 0008 055-0078 | | | | | | | |
| X7010410 | SPEED DISPLAY TRAILER | CAL MO | 8 | 8 | | | | | | | | | | | | | | | | |
| * X7330064 | SIGN SUPPORT SPECIAL | EACH | 4 | 4 | | | | | | | | | | | | | | | | |
| * X7830070 | GROOVING FOR RECESSED PAVEMENT MARKING 5" | FOOT | 97753 | 97753 | | | | | | | | | | | | | | | | |
| * X7830074 | GROOVING FOR RECESSED PAVEMENT MARKING 7" | FOOT | 2764 | 2764 | | | | | | | | | | | | | | | | |
| * X8110458 | CONDUIT ATTACHED TO STRUCTURE, 2" DIA., STAINLESS STEEL | FOOT | 40 | | 40 | | | | | | | | | | | | | | | |
| * X8410102 | TEMPORARY LIGHTING SYSTEM | L SUM | 1 | | 1 | | | | | | | | | | | | | | | |
| Z0001002 | GUARDRAIL AGGREGATE EROSION CONTROL | TON | 4173 | 4173 | | | | | | | | | | | | | | | | |
| Z0013798 | CONSTRUCTION LAYOUT | L SUM | 1 | 1 | | | | | | | | | | | | | | | | |
| Z0018002 | DRAINAGE SCUPPERS, DS-11 | EACH | 3 | | | 2 | | 1 | | | | | | | | | | | | |
| Z0022800 | FENCE REMOVAL | FOOT | 1030 | 1030 | | | | | | | | | | | | | | | | |
| Z0024478 | FLEXIBLE DELINEATORS | EACH | 16 | 16 | | | | | | | | | | | | | | | | |
| Z0034105 | MATERIAL TRANSFER DEVICE | TON | SEE TABLE 1 - ALTERNATE PAVEMENT QUANTITIES | | | | | | | | | | | | | | | | | |
| Z0046304 | PIPE UNDERDRAINS FOR STRUCTURES 4" | FOOT | 1353 | | | 324 | 170 | 150 | 135 | 141 | 152 | 147 | 134 | | | | | | | |
| Z0048665 | RAILROAD PROTECTIVE LIABILITY INSURANCE | L SUM | 1 | 1 | | | | | | | | | | | | | | | | |

* SPECIALTY ITEM

| CODE NO. | ITEM | UNIT | FED/STATE 80%/20% TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | | | | | | | | |
|------------|------------------------------------|-------|-------------------------------------|--------------------------|----------------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|----------------------------|--|--|--|--|--|--|--|
| | | | | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | FED/STATE 80%/20% | | | | | | | |
| | | | | ROADWAY 0001 RURAL | HIGHWAY LIGHTING 0021 NONE | BRIDGE 0008 055-0046 | BRIDGE 0008 055-0068 | BRIDGE 0008 055-0070 | BRIDGE 0008 055-0072 | BRIDGE 0008 055-0074 | BRIDGE 0008 055-0075 | BRIDGE 0008 055-0077 | BRIDGE 0008 055-0078 | | | | | | | |
| Z0062456 | TEMPORARY PAVEMENT | 50 YD | 2108 | 2108 | | | | | | | | | | | | | | | | |
| Z0065100 | SETTLEMENT PLATFORMS | EACH | 6 | 6 | | | | | | | | | | | | | | | | |
| φ Z0076600 | TRAINEES | HOUR | 1500 | 1500 | | | | | | | | | | | | | | | | |
| φ Z0076604 | TRAINEES TRAINING PROGRAM GRADUATE | HOUR | 1500 | 1500 | | | | | | | | | | | | | | | | |

φ 0042

TABLE 1 - ALTERNATE PAVEMENT QUANTITIES

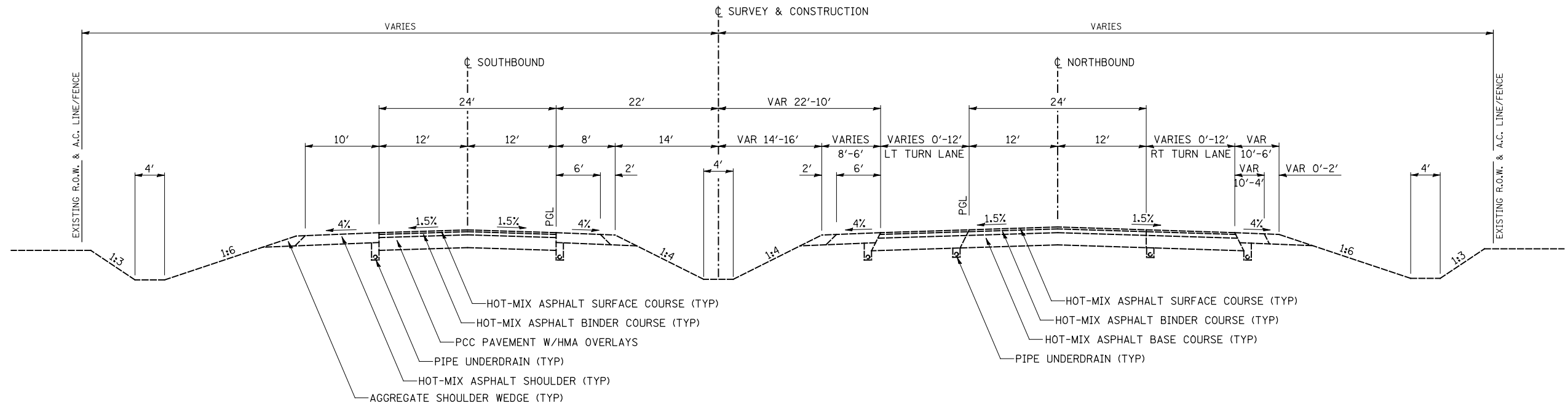
| CODE NO. | ITEM | UNIT | TOTAL QUANTITY (ALT. A - PCC) | TOTAL QUANTITY (ALT. B - HMA) |
|----------|--|-------|-------------------------------|-------------------------------|
| 20200100 | EARTH EXCAVATION | CU YD | 73,600 | 72,790 |
| 20400800 | FURNISHED EXCAVATION | CU YD | 126,715 | 127,850 |
| 30200650 | PROCESSING MODIFIED SOIL 12" | SQ YD | 194,727 | 200,829 |
| 30200750 | PROCESSING MODIFIED SOIL 14" | SQ YD | 6,606 | 6,820 |
| 30200850 | PROCESSING MODIFIED SOIL 16" | SQ YD | 9,667 | 10,029 |
| 30200950 | PROCESSING MODIFIED SOIL 18" | SQ YD | 16,207 | 16,760 |
| 30201050 | PROCESSING MODIFIED SOIL 20" | SQ YD | 2,506 | 2,587 |
| 30201250 | PROCESSING MODIFIED SOIL 24" | SQ YD | 4,784 | 4,939 |
| 30201500 | LIME | TON | 5,016.7 | 5,177.4 |
| 31200100 | STABILIZED SUBBASE 4" | SQ YD | 153,037 | |
| 40600285 | POLYMERIZED BITUMINOUS MATERIALS (PRIME COAT) | POUND | 23,457.4 | 263,624.8 |
| 40603085 | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 | TON | 1,017 | 52,048 |
| 40603235 | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 | TON | 339 | 17,341 |
| 40603540 | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | TON | 1,591 | 16,485 |

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TABLE 1 - ALTERNATE PAVEMENT QUANTITIES

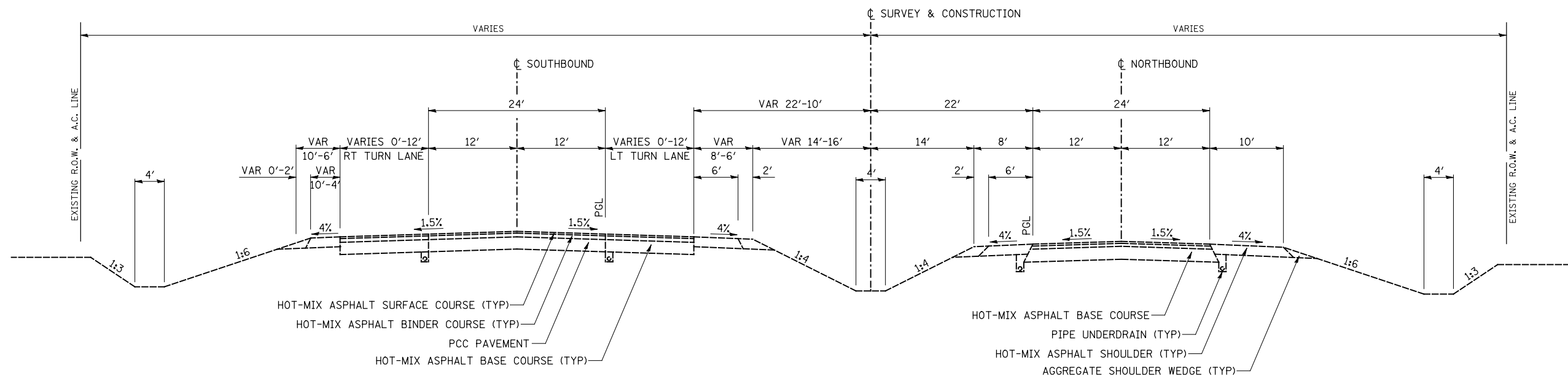
| CODE NO. | ITEM | UNIT | TOTAL QUANTITY (ALT. A - PCC) | TOTAL QUANTITY (ALT. B - HMA) |
|----------|--|-------|-------------------------------|-------------------------------|
| 42000401 | PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED) | SQ YD | 116,449 | |
| 42000406 | PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED) | SQ YD | 15,502 | |
| 42001200 | PAVEMENT FABRIC | SQ YD | 4,607 | |
| 42001300 | PROTECTIVE COAT | SQ YD | 214,362 | 2,725 |
| 42001420 | BRIDGE APPROACH PAVEMENT CONNECTOR (PCC) | SQ YD | 4,607 | |
| 42001430 | BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE) | SQ YD | | 4,607 |
| 48101200 | AGGREGATE SHOULDERS, TYPE B | TON | 23,778 | 18,552 |
| 48203029 | HOT-MIX ASPHALT SHOULDERS, 8" | SQ YD | | 75,081 |
| 48300400 | PORTLAND CEMENT CONCRETE SHOULDERS 9" | SQ YD | 68,081 | |
| 48300405 | PORTLAND CEMENT CONCRETE SHOULDERS 9 1/4" | SQ YD | 7,001 | |
| X0324159 | WHITEWASHING FOR CONCRETE PAVEMENTS | SQ YD | 153,037 | |
| Z0034105 | MATERIAL TRANSFER DEVICE | TON | 2,474 | 85,401 |

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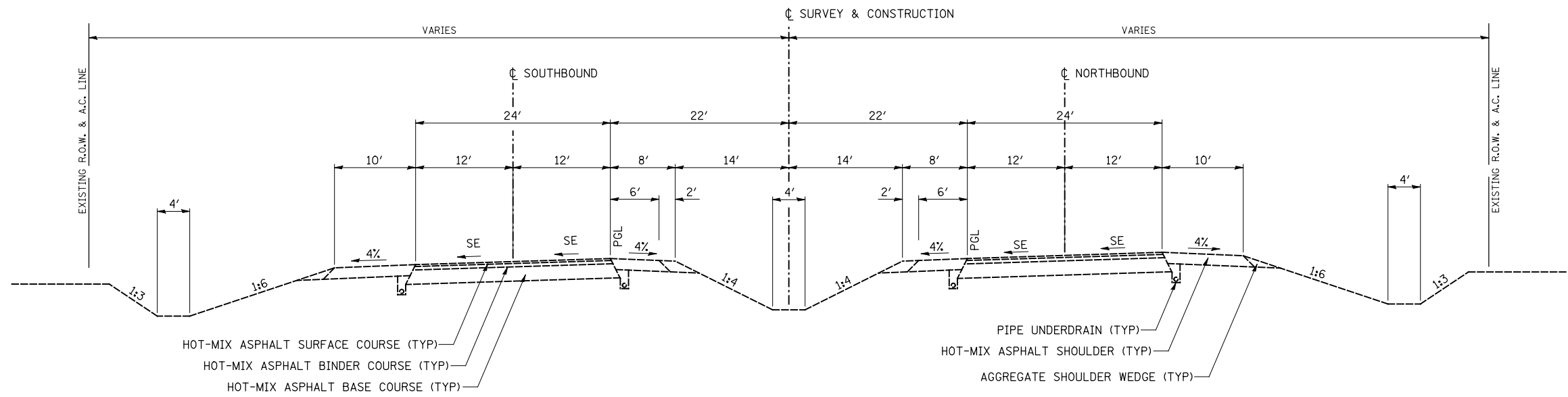
US 67 TANGENT SECTION
 STA 429+64.16 TO STA 434+78.91

NOTE:
 EXISTING PAVEMENT IS 13" HOT-MIX ASPHALT.



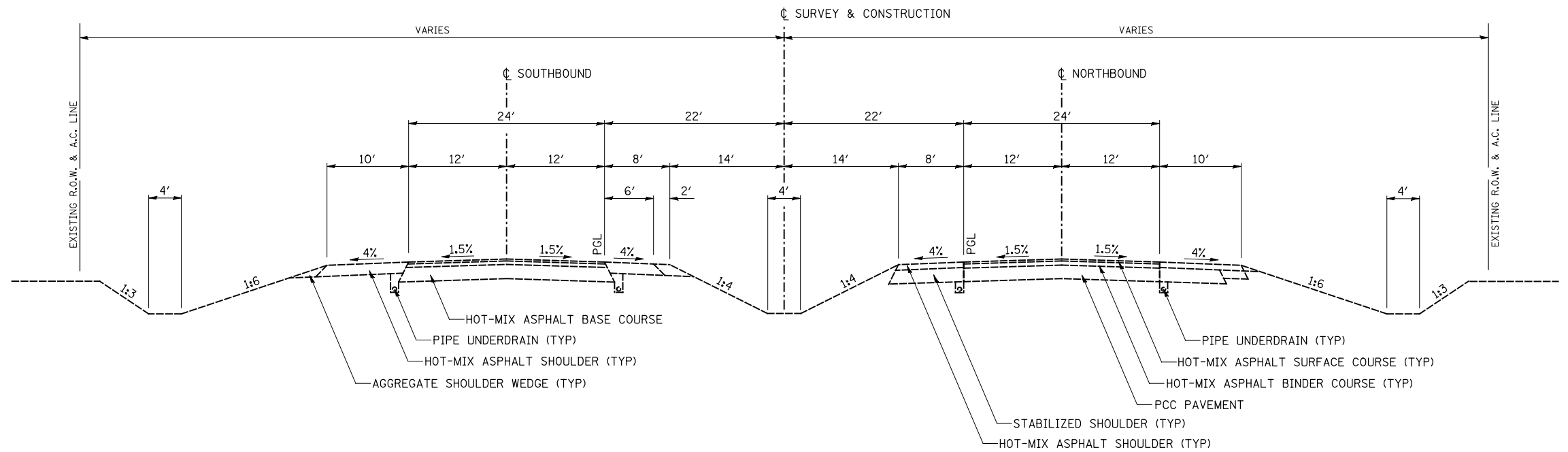
US 67 TANGENT SECTION
 STA 434+78.91 TO STA 440+00.00

| | | | | | | | | | | |
|--|-------------------|----------------|---------------------------|---|---|-----------------------------|-----------|-------------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) EXISTING TYPICAL SECTIONS - FAP ROUTE 310 (US 67) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-US67typical01.dgn | DRAWN - TJD | REVISED - | 407 | | | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 25 | |
| PLOT SCALE = 16.000' / IN. | CHECKED - JRB | REVISED - | CONTRACT NO. 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | SCALE: N/A | SHEET NO. 1 OF 2 SHEETS | STA. N/A | TO STA. N/A | | |



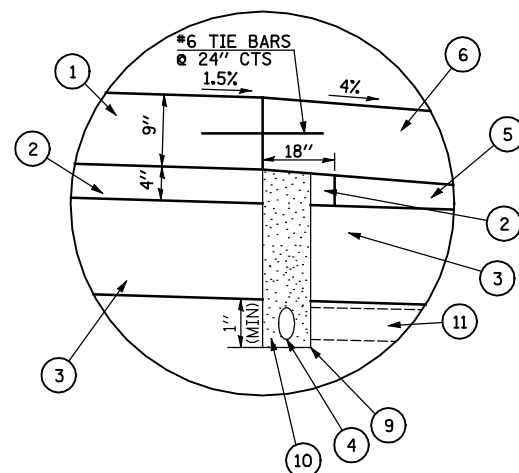
US 67 SUPERELEVATED SECTION
 STA 440+00.00 TO STA 455+11.00

NOTE:
 EXISTING PAVEMENT IS 13" HOT-MIX ASPHALT.



US 67 TANGENT SECTION
 STA 455+11.00 TO STA 461+50.00

| | | | | | | | | | | |
|--|-------------------|----------------|---------------------------|---|---|-----------------------------|-----------|-------------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) EXISTING TYPICAL SECTIONS - FAP ROUTE 310 (US 67) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-US67typical02.dgn | DRAWN - TJD | REVISED - | 407 | | | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 26 | |
| PLOT SCALE = 16.000' / IN. | CHECKED - JRB | REVISED - | CONTRACT NO. 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | SCALE: N/A | SHEET NO. 2 OF 2 SHEETS | STA. N/A | TO STA. N/A | | |

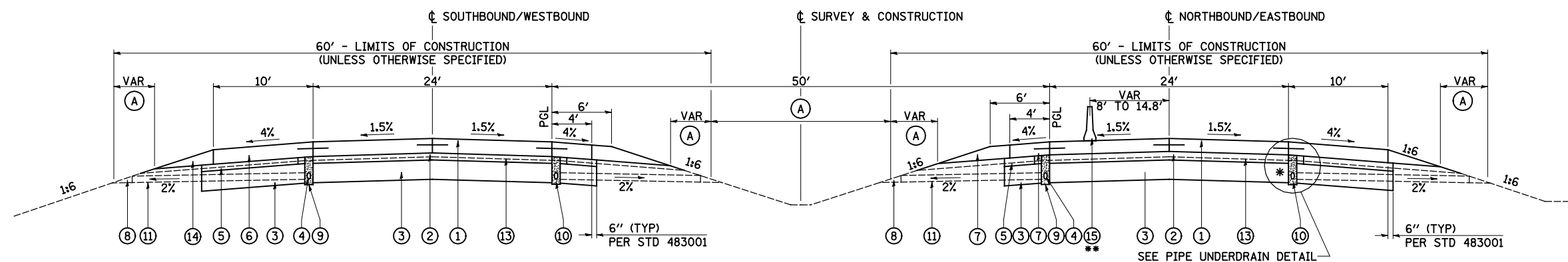


PIPE UNDERDRAIN DETAIL

* CONSTRUCT (2) TO 18" BEYOND EDGE OF PAVEMENT
 CONSTRUCT (1) WITHOUT TIE BARS
 CONSTRUCT (9) AND COMPLETE CONSTRUCTION OF (4) AND (10)
 INSTALL TIE BARS AND COMPLETE (5) AND (6).

| RIGID PAVEMENT DESIGN INFORMATION | | |
|--|-------------------|----------|
| STRUCTURAL DESIGN TRAFFIC: 3,607 | YEAR: 2032 | |
| PV = 3,246 | SU = 180 | MU = 180 |
| ROAD/STREET CLASSIFICATION: RURAL | CLASS: 1 | |
| PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE: | | |
| PV = 32% | SU = 45% | MU = 45% |
| RIGID TRAFFIC FACTOR: | ACTUAL TF = 1.37 | |
| | MINIMUM TF = 5.02 | |
| SUBGRADE SUPPORTING RATING: | | |
| SSR = POOR | | |

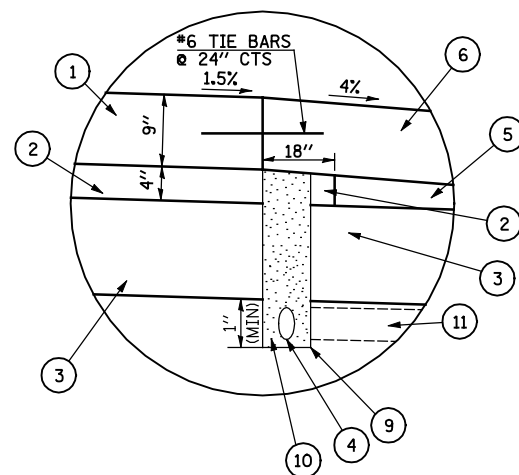
- LEGEND**
- (1) PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
 - (2) STABILIZED SUBBASE 4"
 - (3) LIME MODIFIED SOIL (12" MIN)
 - (4) PIPE UNDERDRAINS 4"
 - (5) COMPACTED SUBGRADE (EMBANKMENT)
 - (6) PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - (7) AGGREGATE SHOULDERS, TYPE B (9")
 - (8) CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
 - (9) UNDERDRAIN TRENCH (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
 - (10) FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
 - (11) PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
 - (12) GUARDRAIL AGGREGATE EROSION CONTROL
 - (13) EXISTING PRE-GRADE GROUND LINE (11 1/4" BELOW PGL)
 - (14) AGGREGATE WEDGE SHOULDER, TYPE B (9") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B
 - (15) TEMPORARY CONCRETE BARRIER (TO REMAIN PERMANENTLY)
 - (A) SEEDING, CLASS 2A



** STA 559+00.0 TO STA 576+13.41 (RIGHT SIDE ONLY)
 SEE PLAN & PROFILE SHEET AND SCHEDULE

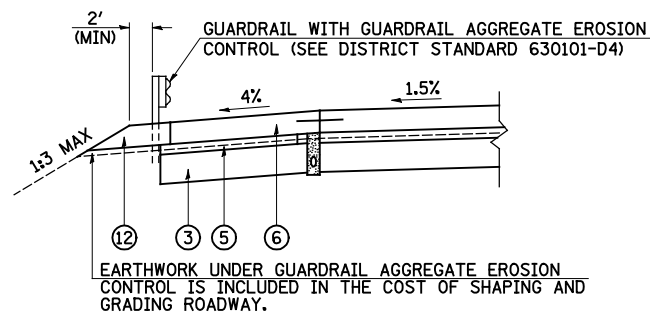
IL 336 /IL 110 TANGENT SECTION
 STA 541+57.14 TO STA 546+24.47 - SB ONLY
 STA 546+24.47 TO STA 548+25.25
 STA 575+11.16 TO STA 577+00.00

| | | | | | | | | | | |
|--|-------------------|----------------|---------------------------|---|--|-----------------------------|-----------|--------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - IL 336 /IL 110 TANGENT SECTION (ALT. A) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-lane paving plans\cadd sheets\0468844-sht-typical12.dgn | DRAWN - TJD | REVISIED - | 407 | | | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 27 | |
| PLOT SCALE = 20.00' / IN. | CHECKED - JRB | REVISIED - | CONTRACT NO. 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISIED - | ILLINOIS FED. AID PROJECT | | | | | | | |



PIPE UNDERDRAIN DETAIL

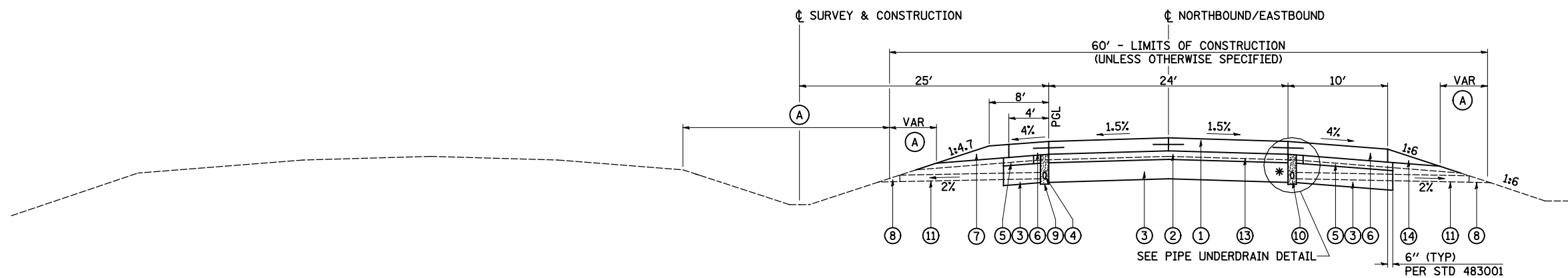
* CONSTRUCT (2) TO 18" BEYOND EDGE OF PAVEMENT
 CONSTRUCT (1) WITHOUT TIE BARS
 CONSTRUCT (9) AND COMPLETE CONSTRUCTION OF (4) AND (10)
 INSTALL TIE BARS AND COMPLETE (5) AND (6).



GUARDRAIL SECTION

LEGEND

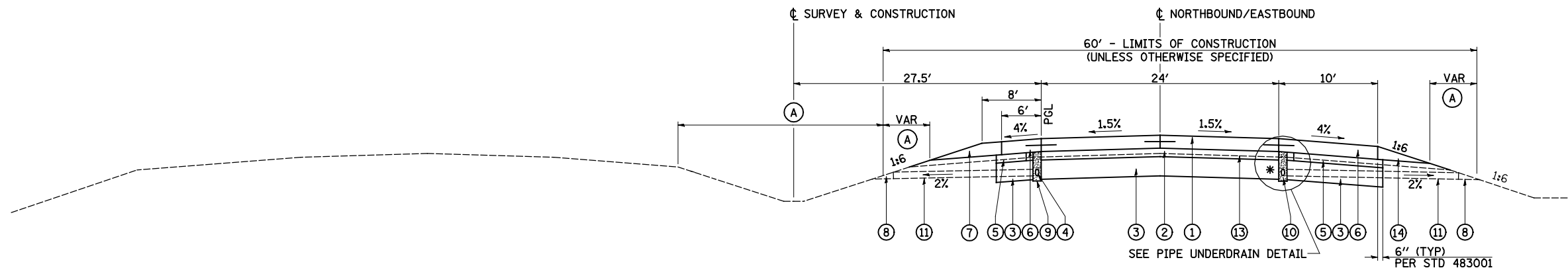
- (1) PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
- (2) STABILIZED SUBBASE 4"
- (3) LIME MODIFIED SOIL (12" MIN)
- (4) PIPE UNDERDRAINS 4"
- (5) COMPACTED SUBGRADE (EMBANKMENT)
- (6) PORTLAND CEMENT CONCRETE SHOULDERS 9"
- (7) AGGREGATE SHOULDERS, TYPE B (9")
- (8) CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- (9) UNDERDRAIN TRENCH (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- (10) FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- (11) PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- (12) GUARDRAIL AGGREGATE EROSION CONTROL
- (13) EXISTING PRE-GRADE GROUND LINE (1 1/4" BELOW PGL)
- (14) AGGREGATE WEDGE SHOULDER, TYPE B (9") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B
- (A) SEEDING, CLASS 2A



IL 336 /IL 110 TANGENT SECTION

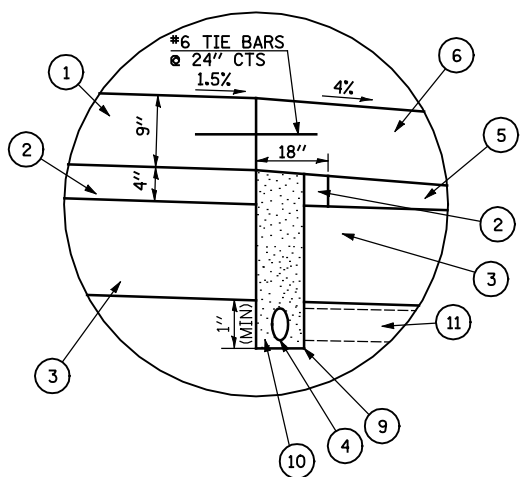
STA 577+00.00 TO STA 578+67.56
 STA 578+67.56 TO STA 579+74.26 - CURVE #2 RT
 STA 579+74.26 TO STA 586+87.33 - BRIDGE & APPROACH PAVEMENT OMISSION
 STA 586+87.33 TO STA 602+01.15 - CURVE #2 RT
 STA 602+01.15 TO STA 633+39.48
 STA 633+39.48 TO STA 634+86.98 - BRIDGE & APPROACH PAVEMENT OMISSION
 STA 634+86.98 TO STA 635+34.06
 STA 648+71.14 TO STA 657+80.17
 STA 708+30.50 TO STA 708+50.00
 STA 708+50.00 TO STA 710+25.00 - TRANSITION MEDIAN WIDTH FROM 50' TO 55'

| | | | | | | | | | | | | | |
|--|-------------------|------------------|-----------|---|--|-------------------------|------|--------------------|---------|-----------------------------|--------------|-----------|----|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - IL 336 /IL 110 TANGENT SECTION (ALT. A) | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-sht-typical07.dgn | | DRAWN - TJD | REVISED - | | SCALE: N/A | SHEET NO. 2 OF 6 SHEETS | STA. | TO STA. | 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 28 |
| PLOT SCALE = 20.00' / IN. | | CHECKED - JRB | REVISED - | | | | | CONTRACT NO. 68B44 | | | | | |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | | |

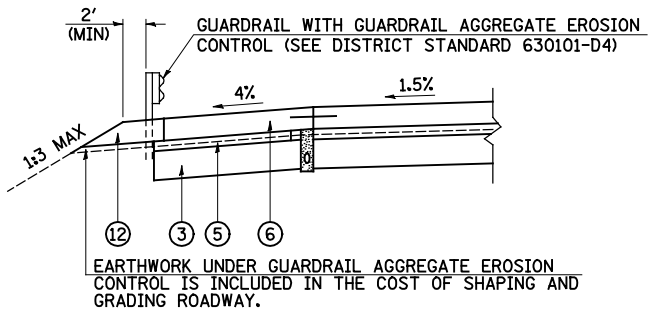


IL 336 /IL 110 TANGENT SECTION

STA 710+25.00 TO STA 718+25.64
 STA 754+54.14 TO STA 779+10.00
 STA 779+10.00 TO STA 780+30.00 - BRIDGE & APPROACH PAVEMENT OMISSION
 STA 780+30.00 TO STA 796+76.36
 STA 835+53.62 TO STA 864+50.00



PIPE UNDERDRAIN DETAIL

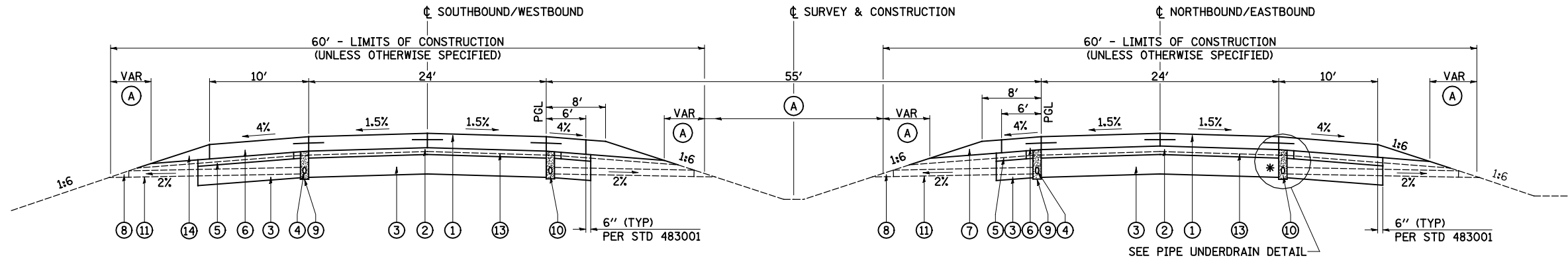


GUARDRAIL SECTION

LEGEND

- ① PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
- ② STABILIZED SUBBASE 4"
- ③ LIME MODIFIED SOIL (12" MIN)
- ④ PIPE UNDERDRAINS 4"
- ⑤ COMPACTED SUBGRADE (EMBANKMENT)
- ⑥ PORTLAND CEMENT CONCRETE SHOULDERS 9"
- ⑦ AGGREGATE SHOULDERS, TYPE B (9')
- ⑧ CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- ⑨ UNDERDRAIN TRENCH (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- ⑩ FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑪ PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- ⑫ GUARDRAIL AGGREGATE EROSION CONTROL
- ⑬ EXISTING PRE-GRADE GROUND LINE (1 1/4" BELOW PGL)
- ⑭ AGGREGATE WEDGE SHOULDER, TYPE B (9') - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B
- Ⓐ SEEDING, CLASS 2A

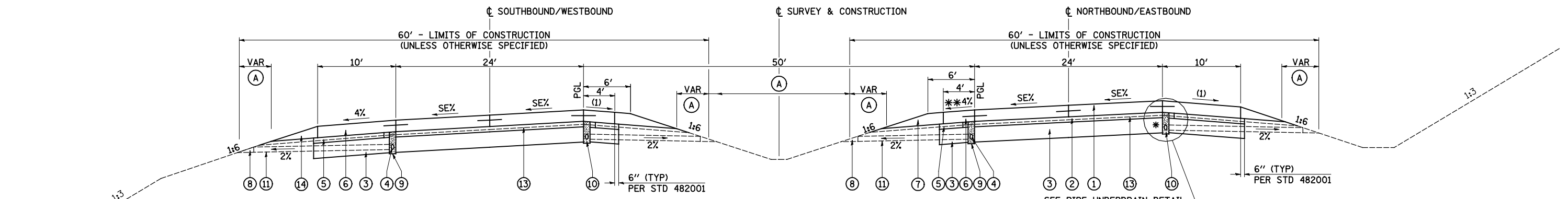
* CONSTRUCT ② TO 18" BEYOND EDGE OF PAVEMENT
 CONSTRUCT ① WITHOUT TIE BARS
 CONSTRUCT ⑨ AND COMPLETE CONSTRUCTION OF ④ AND ⑩
 INSTALL TIE BARS AND COMPLETE ⑤ AND ⑥.



IL 336 /IL 110 TANGENT SECTION

STA 864+50.00 TO STA 886+50

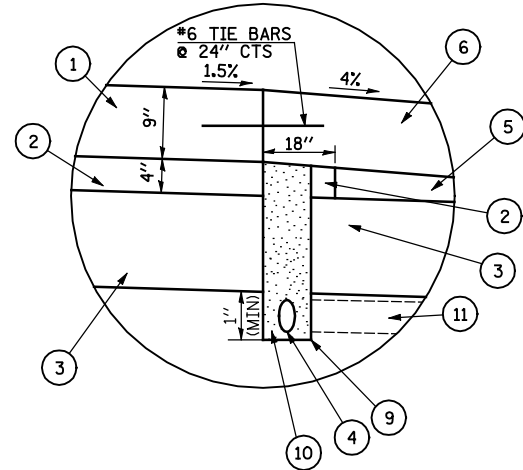
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|--|-------------------|----------------|-----------|---|--|-------------------------|------|--------------------|---------|-----------------------------|--------------|-----------|----|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - IL 336 /IL 110 TANGENT SECTION (ALT. A) | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-sht-typical08.dgn | DRAWN - TJD | REVISOR - | REVISOR - | | SCALE: N/A | SHEET NO. 3 OF 6 SHEETS | STA. | TO STA. | 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 29 |
| PLOT SCALE = 20.00' / IN. | CHECKED - JRB | REVISOR - | REVISOR - | | | | | CONTRACT NO. 68B44 | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISOR - | REVISOR - | | ILLINOIS FED. AID PROJECT | | | | | | | | |



(1) MAXIMUM ROLLOVER = 8%
 ** 4% OR MATCH SE IF >4%

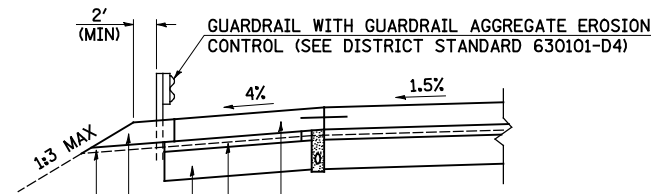
IL 336 /IL 110 SUPERELEVATED SECTION

STA 548+25.25 TO STA 575+11.16 - CURVE #1 LT



PIPE UNDERDRAIN DETAIL

* CONSTRUCT (2) TO 18" BEYOND EDGE OF PAVEMENT
 CONSTRUCT (1) WITHOUT TIE BARS
 CONSTRUCT (9) AND COMPLETE CONSTRUCTION OF (4) AND (10)
 INSTALL TIE BARS AND COMPLETE (5) AND (6).

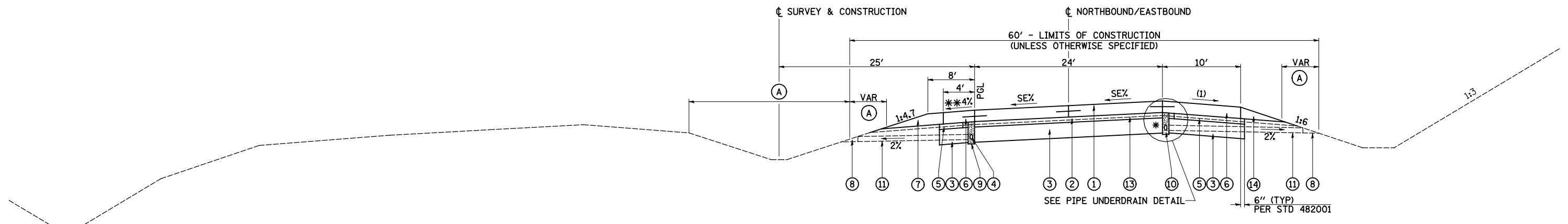


EARTHWORK UNDER GUARDRAIL AGGREGATE EROSION CONTROL IS INCLUDED IN THE COST OF SHAPING AND GRADING ROADWAY.

GUARDRAIL SECTION

LEGEND

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
- (2) STABILIZED SUBBASE 4"
- (3) LIME MODIFIED SOIL (12" MIN)
- (4) PIPE UNDERDRAINS 4"
- (5) COMPACTED SUBGRADE (EMBANKMENT)
- (6) PORTLAND CEMENT CONCRETE SHOULDERS 9"
- (7) AGGREGATE SHOULDERS, TYPE B (9")
- (8) CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- (9) UNDERDRAIN TRENCH (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- (10) FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- (11) PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- (12) GUARDRAIL AGGREGATE EROSION CONTROL
- (13) EXISTING PRE-GRADE GROUND LINE (11/4" BELOW PGL)
- (14) AGGREGATE WEDGE SHOULDER, TYPE B (9") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B
- (A) SEEDING, CLASS 2A



(1) MAXIMUM ROLLOVER = 8%
 ** 4% OR MATCH SE IF >4%

IL 336 /IL 110 SUPERELEVATED SECTION

STA 635+34.06 TO STA 648+71.14 - CURVE #3 RT
 STA 657+80.17 TO STA 680+16.85 - CURVE #4 RT
 STA 680+16.85 TO STA 682+27.34 - BRIDGE & APPROACH PAVEMENT OMISSION
 STA 682+27.34 TO STA 702+13.27 - CURVE #4 RT
 STA 702+13.27 TO STA 703+26.73 - BRIDGE & APPROACH PAVEMENT OMISSION
 STA 703+26.73 TO STA 708+30.50 - CURVE #4 RT

SEE SUPERELEVATION TRANSITION DETAILS FOR ATTAINMENT AND REMOVAL INFORMATION.

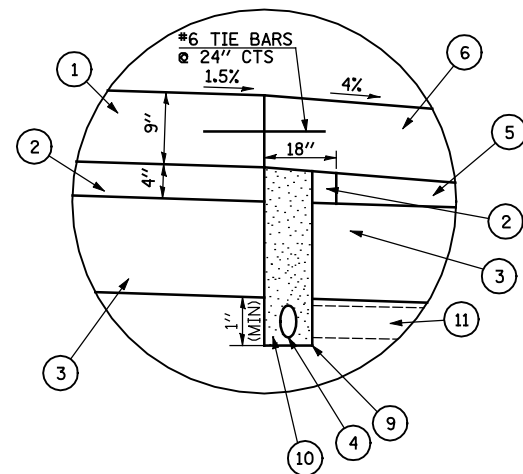
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|--|-------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-sht-typical09.dgn | | DRAWN - TJD | REVISED - |
| PLOT SCALE = 20.00' / IN. | | CHECKED - JRB | REVISED - |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAP ROUTE 407 (IL 336 /IL 110)
 TYPICAL SECTIONS - IL 336 /IL 110 SUPERELEVATED SECTION (ALT. A)**

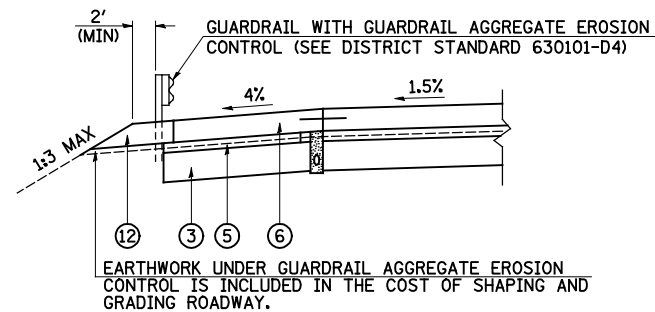
SCALE: N/A SHEET NO. 4 OF 6 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------------------------|-----------|---------------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 30 |
| | | | CONTRACT NO. 68B44 | |
| ILLINOIS FED. AID PROJECT | | | | |



PIPE UNDERDRAIN DETAIL

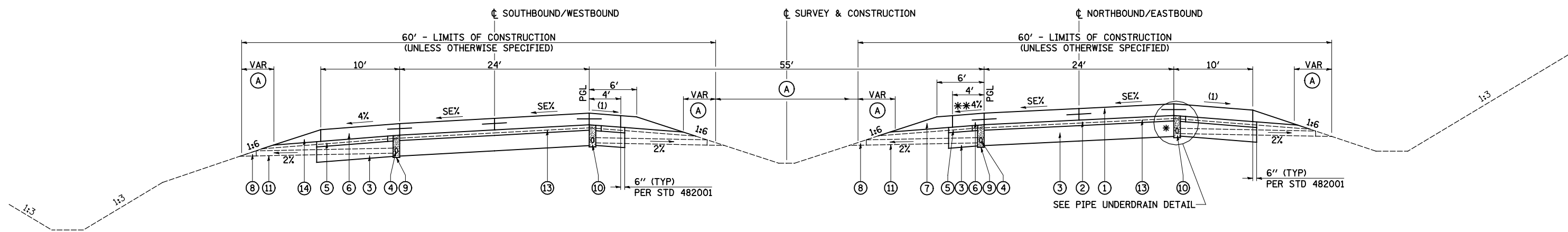
- * CONSTRUCT (2) TO 18" BEYOND EDGE OF PAVEMENT
- CONSTRUCT (1) WITHOUT TIE BARS
- CONSTRUCT (9) AND COMPLETE CONSTRUCTION OF (4) AND (10)
- INSTALL TIE BARS AND COMPLETE (5) AND (6).



GUARDRAIL SECTION

LEGEND

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
- (2) STABILIZED SUBBASE 4"
- (3) LIME MODIFIED SOIL (12" MIN)
- (4) PIPE UNDERDRAINS 4"
- (5) COMPACTED SUBGRADE (EMBANKMENT)
- (6) PORTLAND CEMENT CONCRETE SHOULDERS 9"
- (7) AGGREGATE SHOULDERS, TYPE B (9")
- (8) CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- (9) UNDERDRAIN TRENCH (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- (10) FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- (11) PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- (12) GUARDRAIL AGGREGATE EROSION CONTROL
- (13) EXISTING PRE-GRADE GROUND LINE (1 1/4" BELOW PGL)
- (14) AGGREGATE WEDGE SHOULDER, TYPE B (9") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B
- (A) SEEDING, CLASS 2A



(1) MAXIMUM ROLLOVER = 8%
 ** 4% OR MATCH SE IF >4%

IL 336 /IL 110 SUPERELEVATED SECTION
 STA 718+25.64 TO STA 754+54.14 - CURVE #5 LT
 STA 796+76.36 TO STA 835+53.62 - CURVE #6 RT

SEE SUPERELEVATION TRANSITION DETAILS FOR ATTAINMENT AND REMOVAL INFORMATION.

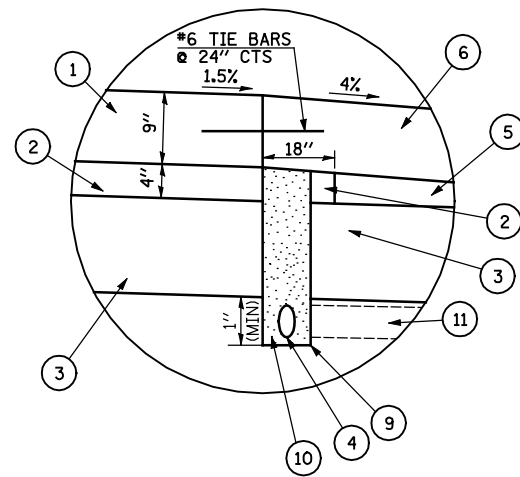
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| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-sht-typical10.dgn | | DRAWN - TJD | REVISED - |
| PLOT SCALE = 20.00' / IN. | | CHECKED - JRB | REVISED - |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAP ROUTE 407 (IL 336 /IL 110)
 TYPICAL SECTION - IL 336 /IL 110 SUPERELEVATED SECTION (ALT. A)**

SCALE: N/A SHEET NO. 5 OF 6 SHEETS STA. TO STA.

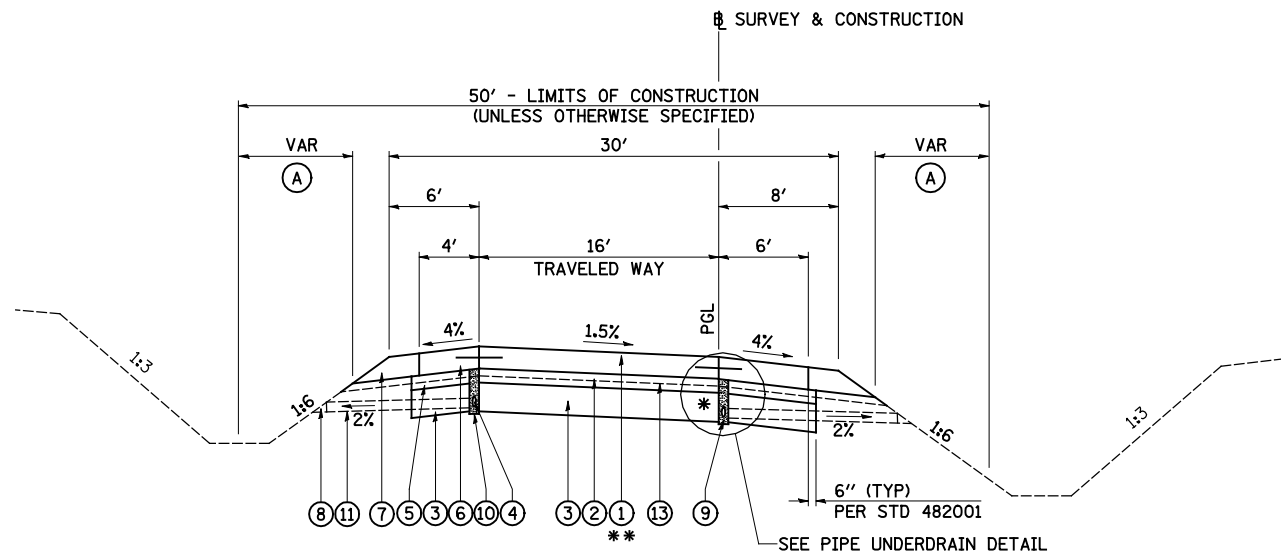
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------------------------|-----------|---------------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 31 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |



PIPE UNDERDRAIN DETAIL

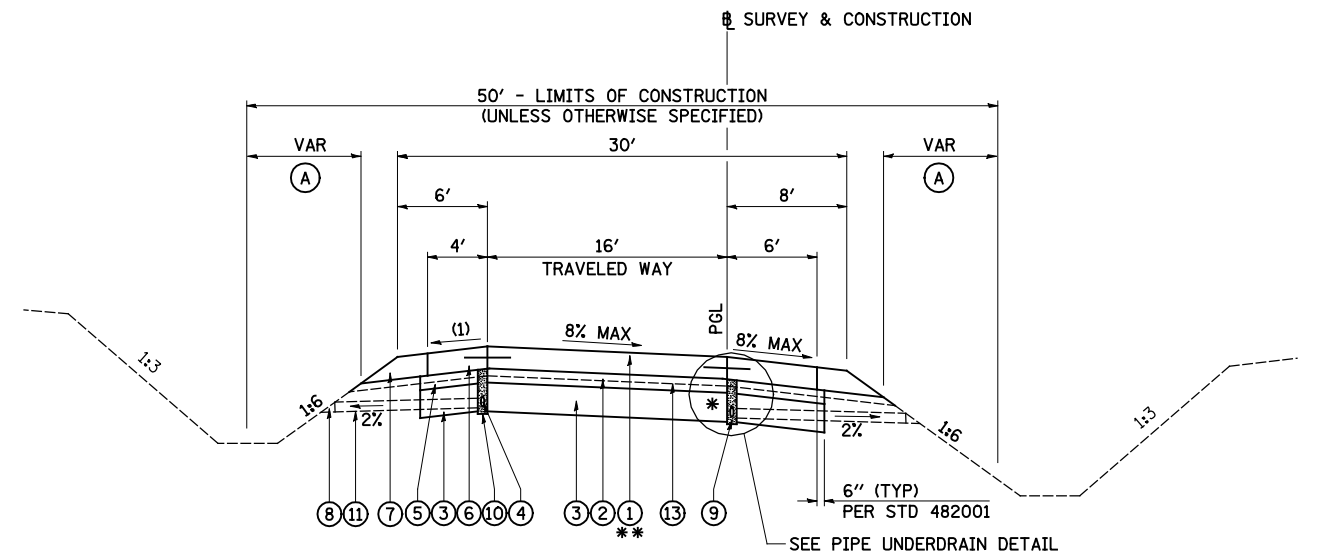
- * CONSTRUCT (2) TO 18" BEYOND EDGE OF PAVEMENT
- CONSTRUCT (1) WITHOUT TIE BARS
- CONSTRUCT (9) AND COMPLETE CONSTRUCTION OF (4) AND (10)
- INSTALL TIE BARS AND COMPLETE (5) AND (6).

- LEGEND**
- (1) PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
 - (2) STABILIZED SUBBASE 4"
 - (3) LIME MODIFIED SOIL (12" MIN)
 - (4) PIPE UNDERDRAINS 4"
 - (5) COMPACTED SUBGRADE (EMBANKMENT)
 - (6) PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - (7) AGGREGATE SHOULDERS, TYPE B (9")
 - (8) CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
 - (9) UNDERDRAIN TRENCH (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
 - (10) FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
 - (11) PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
 - (12) GUARDRAIL AGGREGATE EROSION CONTROL
 - (13) EXISTING PRE-GRADE GROUND LINE (1 1/4" BELOW PGL)
 - (14) AGGREGATE WEDGE SHOULDER, TYPE B (9") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B
 - (A) SEEDING, CLASS 2A



TYPICAL RAMP TANGENT SECTION

RAMP K (US 136) - STA 101+45.00 TO STA 103+23.25
 RAMP L (US 136) - STA 68+31.07 TO STA 72+04.23



(1) MAXIMUM ROLLOVER = 8%

TYPICAL RAMP SUPERELEVATED SECTION

RAMP K (US 136) - STA 103+23.25 TO STA 119+61.56
 RAMP L (US 136) - STA 56+58.52 TO STA 68+31.07
 RAMP A (US 67) - STA 322+39.93 TO STA 332+74.54
 RAMP D (US 67) - STA 206+60.09 TO STA 219+04.55

** RAMP A (US 67) - STA 321+07.18 TO STA 322+39.93 - PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
 RAMP D (US 67) - STA 219+04.55 TO STA 220+14.88 - PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)

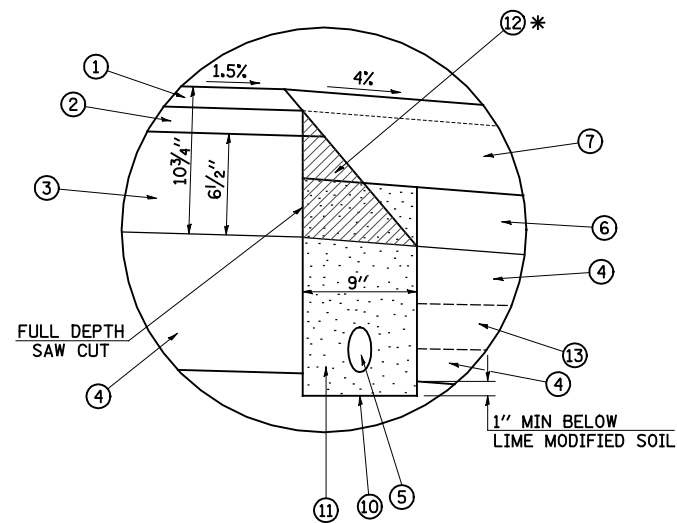
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|--|-------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-typical11.dgn | | DRAWN - TJD | REVISED - |
| PLOT SCALE = 20.00 FT / IN. | | CHECKED - JRB | REVISED - |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAP ROUTE 407 (IL 336 /IL 110)
 TYPICAL SECTIONS - INTERCHANGE RAMP K, L, A & D (ALT. A)**

SCALE: N/A SHEET NO. 6 OF 6 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------------------------|-----------|--------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 32 |
| | | | CONTRACT NO. 68B44 | |
| ILLINOIS FED. AID PROJECT | | | | |

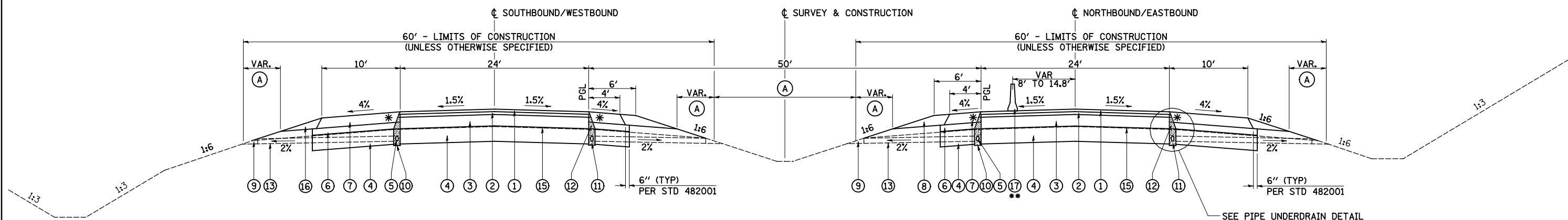


PIPE UNDERDRAIN DETAIL

* CONSTRUCT (2) AND (3). SAWCUT AS SHOWN IN DETAIL. THEN CONSTRUCT (6).
 CONSTRUCT (5), (10), AND (11). CONSTRUCT (7) TO TOP OF (2) PRIOR TO PLACING (1).
 CONSTRUCT (1) AND COMPLETE CONSTRUCTION OF (7).

| FLEXIBLE PAVEMENT DESIGN INFORMATION | | |
|--|-------------------|-----------------|
| STRUCTURAL DESIGN TRAFFIC: 3,607 | YEAR: 2032 | |
| PV = 3,246 | SU = 180 | MU = 180 |
| ROAD/STREET CLASSIFICATION: RURAL | CLASS: 1 | |
| PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE: | | |
| PV = 32% | SU = 45% | MU = 45% |
| FLEXIBLE TRAFFIC FACTOR: ACTUAL TF = 1.00 | AC TYPE = 20 | |
| | MINIMUM TF = 3.56 | |
| PG GRADE: | BINDER = 64-22 | SURFACE = 64-22 |
| SUBGRADE SUPPORTING RATING: | | |
| SSR = POOR | | |

- LEGEND
- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
 - (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 2 1/4"
 - (3) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 6 1/2"
 - (4) LIME MODIFIED SOIL (12" MIN)
 - (5) PIPE UNDERDRAINS 4"
 - (6) COMPACTED SUBGRADE (EMBANKMENT)
 - (7) HOT-MIX ASPHALT SHOULDERS, 8"
 - (8) AGGREGATE SHOULDERS, TYPE B (8")
 - (9) CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
 - (10) 9" TRENCH FOR PIPE UNDERDRAINS (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
 - (11) FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
 - (12) HOT-MIX ASPHALT WEDGE TO BE REMOVED PRIOR TO TRENCHING (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
 - (13) PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
 - (14) GUARDRAIL AGGREGATE EROSION CONTROL
 - (15) EXISTING PRE-GRADE GROUND LINE (1 1/4" BELOW PGL)
 - (16) AGGREGATE WEDGE SHOULDER, TYPE B (8") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B
 - (17) TEMPORARY CONCRETE BARRIER (TO REMAIN PERMANENTLY)
 - (A) SEEDING, CLASS 2A

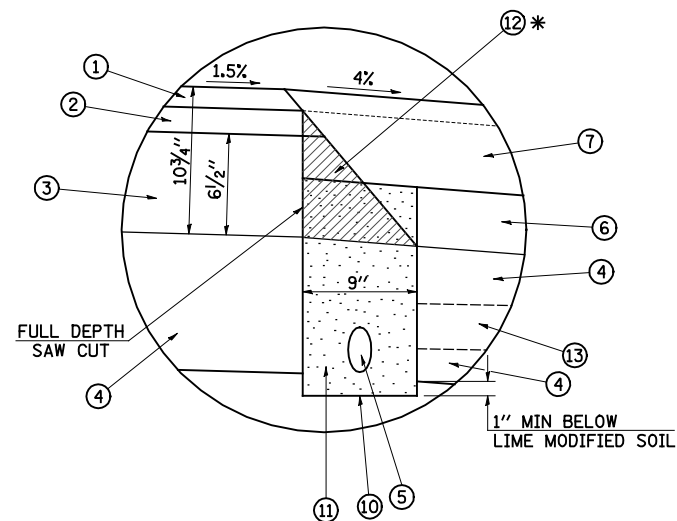


IL 336 /IL 110 TANGENT SECTION

STA 541+57.14 TO STA 546+24.47 - SB ONLY
 STA 546+24.47 TO STA 548+25.25
 STA 575+11.16 TO STA 577+00.00

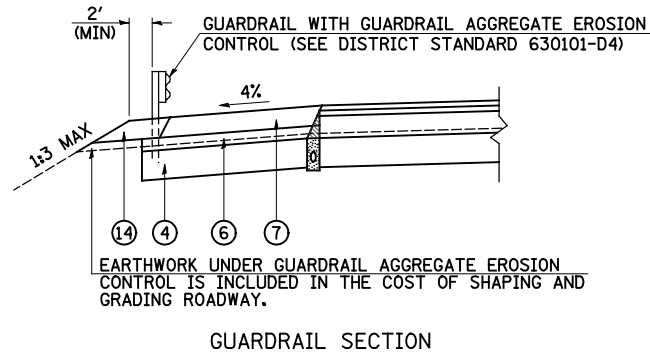
** STA 559+00.0 TO STA 576+13.41 (RIGHT SIDE ONLY)
 SEE PLAN & PROFILE SHEET AND SCHEDULE

| | | | | | | | | | | |
|--|-------------------|----------------|---------------------------|---|--|-----------------------------|-----------|--------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - IL 336 /IL 110 TANGENT SECTION (ALT. B) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-lane paving plans\cadd sheets\0468844-sht-typical00.dgn | DRAWN - TJD | REVISED - | 407 | | | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 33 | |
| PLOT SCALE = 20.00' / IN. | CHECKED - JRB | REVISED - | CONTRACT NO. 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |



PIPE UNDERDRAIN DETAIL

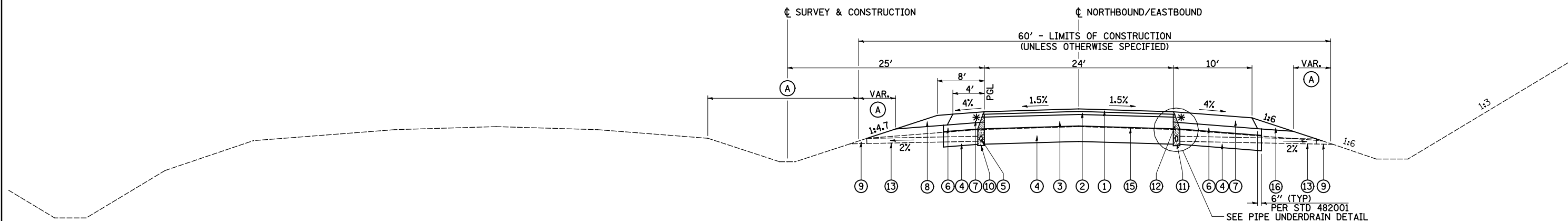
* CONSTRUCT (2) AND (3). SAWCUT AS SHOWN IN DETAIL. THEN CONSTRUCT (6).
 CONSTRUCT (5), (10), AND (11). CONSTRUCT (7) TO TOP OF (2) PRIOR TO PLACING (1).
 CONSTRUCT (1) AND COMPLETE CONSTRUCTION OF (7).



GUARDRAIL SECTION

LEGEND

- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 2 1/4"
- (3) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 6 1/2"
- (4) LIME MODIFIED SOIL (12" MIN)
- (5) PIPE UNDERDRAINS 4"
- (6) COMPACTED SUBGRADE (EMBANKMENT)
- (7) HOT-MIX ASPHALT SHOULDERS, 8"
- (8) AGGREGATE SHOULDERS, TYPE B (8')
- (9) CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- (10) 9" TRENCH FOR PIPE UNDERDRAINS (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- (11) FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- (12) HOT-MIX ASPHALT WEDGE TO BE REMOVED PRIOR TO TRENCHING (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- (13) PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- (14) GUARDRAIL AGGREGATE EROSION CONTROL
- (15) EXISTING PRE-GRADE GROUND LINE (1 1/4" BELOW PGL)
- (16) AGGREGATE WEDGE SHOULDER, TYPE B (8') - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B
- (A) SEEDING, CLASS 2A



IL 336 /IL 110 TANGENT SECTION

STA 577+00.00 TO STA 578+67.56
 STA 578+67.56 TO STA 579+74.26 - CURVE #2 RT
 STA 579+74.26 TO STA 586+87.33 - BRIDGE & APPROACH PAVEMENT OMISSION
 STA 586+87.33 TO STA 602+01.15 - CURVE #2 RT
 STA 602+01.15 TO STA 633+39.48
 STA 633+39.48 TO STA 634+86.98 - BRIDGE & APPROACH PAVEMENT OMISSION
 STA 634+86.98 TO STA 635+34.06
 STA 648+71.14 TO STA 657+80.17
 STA 708+30.50 TO STA 708+50.00
 STA 708+50.00 TO STA 710+25.00 - TRANSITION MEDIAN WIDTH FROM 50' TO 55'

| | | | |
|--|-------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-typical01.dgn | | DRAWN - TJD | REVISED - |
| PLOT SCALE = 20.00' / IN. | | CHECKED - JRB | REVISED - |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

FAP ROUTE 407 (IL 336 /IL 110)
 TYPICAL SECTIONS - IL 336 /IL 110 TANGENT SECTION (ALT. B)

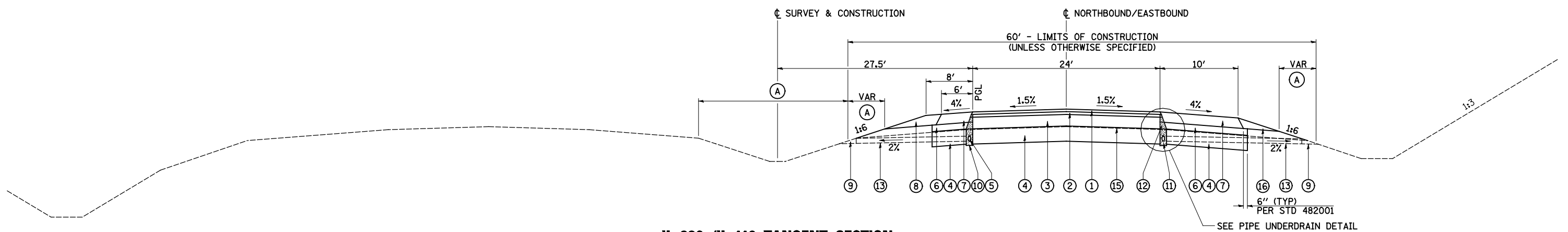
SCALE: N/A SHEET NO. 2 OF 6 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------------------------|-----------|--------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 34 |
| | | | CONTRACT NO. 68B44 | |
| ILLINOIS FED. AID PROJECT | | | | |

CL SURVEY & CONSTRUCTION

CL NORTHBOUND/EASTBOUND

60' - LIMITS OF CONSTRUCTION (UNLESS OTHERWISE SPECIFIED)

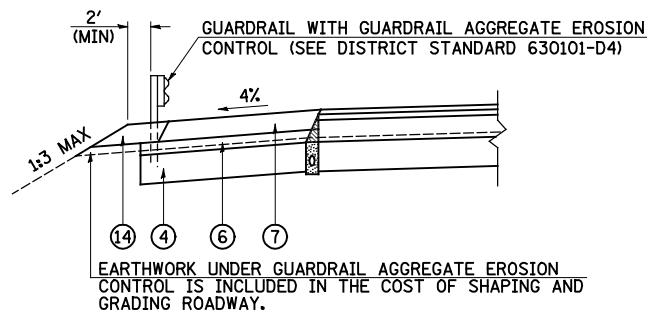


IL 336 /IL 110 TANGENT SECTION

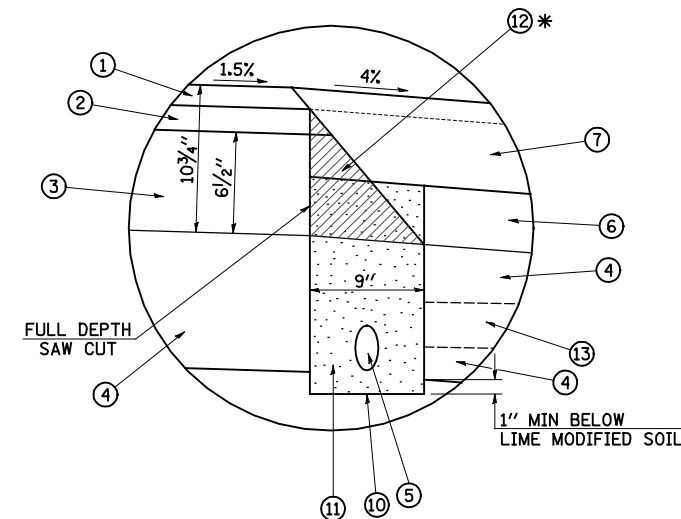
STA 710+25.00 TO STA 718+25.64
STA 754+54.14 TO STA 779+10.00
STA 779+10.00 TO STA 780+30.00 - BRIDGE & APPROACH PAVEMENT OMISSION
STA 780+30.00 TO STA 796+76.36
STA 835+53.62 TO STA 864+50.00

LEGEND

- ① POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
- ② POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 2 1/4"
- ③ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 6 1/2"
- ④ LIME MODIFIED SOIL (12" MIN)
- ⑤ PIPE UNDERDRAINS 4"
- ⑥ COMPACTED SUBGRADE (EMBANKMENT)
- ⑦ HOT-MIX ASPHALT SHOULDERS, 8"
- ⑧ AGGREGATE SHOULDERS, TYPE B (8")
- ⑨ CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- ⑩ 9" TRENCH FOR PIPE UNDERDRAINS (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- ⑪ FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑫ HOT-MIX ASPHALT WEDGE TO BE REMOVED PRIOR TO TRENCHING (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑬ PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- ⑭ GUARDRAIL AGGREGATE EROSION CONTROL
- ⑮ EXISTING PRE-GRADE GROUND LINE (11 1/4" BELOW PGL)
- ⑯ AGGREGATE WEDGE SHOULDER, TYPE B (8") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B
- (A) SEEDING, CLASS 2A



GUARDRAIL SECTION



PIPE UNDERDRAIN DETAIL

* CONSTRUCT ② AND ③. SAWCUT AS SHOWN IN DETAIL. THEN CONSTRUCT ⑥. CONSTRUCT ⑤, ⑩, AND ⑪. CONSTRUCT ⑦ TO TOP OF ② PRIOR TO PLACING ①. CONSTRUCT ① AND COMPLETE CONSTRUCTION OF ⑦.

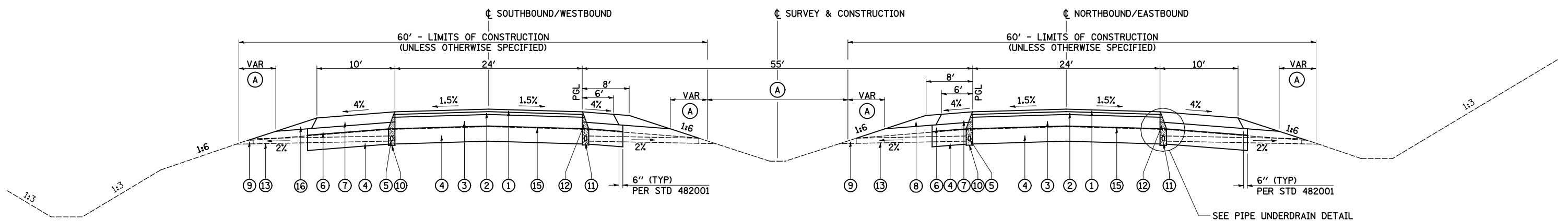
CL SOUTHBOUND/WESTBOUND

CL SURVEY & CONSTRUCTION

CL NORTHBOUND/EASTBOUND

60' - LIMITS OF CONSTRUCTION (UNLESS OTHERWISE SPECIFIED)

60' - LIMITS OF CONSTRUCTION (UNLESS OTHERWISE SPECIFIED)



IL 336/IL 110 TANGENT SECTION

STA 864+50.00 TO STA 886+50.00

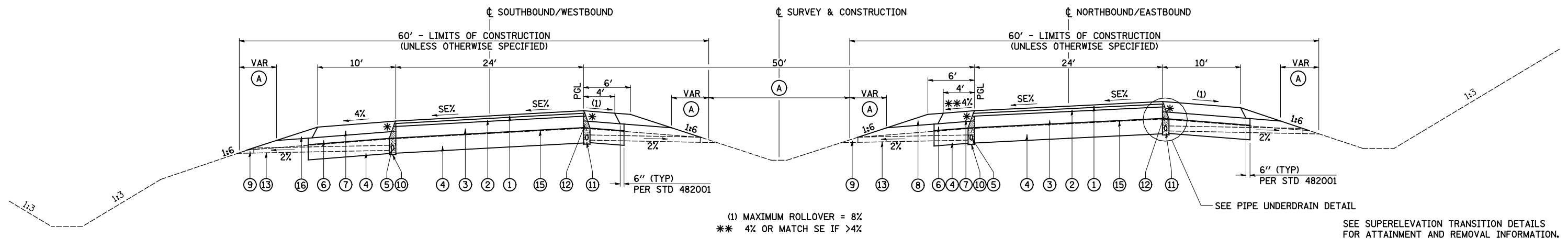
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|--|-------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-shr-typical02.dgn | | DRAWN - TJD | REVISED - |
| PLOT SCALE = 20.00' / IN. | | CHECKED - JRB | REVISED - |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 407 (IL 336 /IL 110)
TYPICAL SECTIONS - IL 336 /IL 110 TANGENT SECTION (ALT. B)

SCALE: N/A SHEET NO. 3 OF 6 SHEETS STA. TO STA.

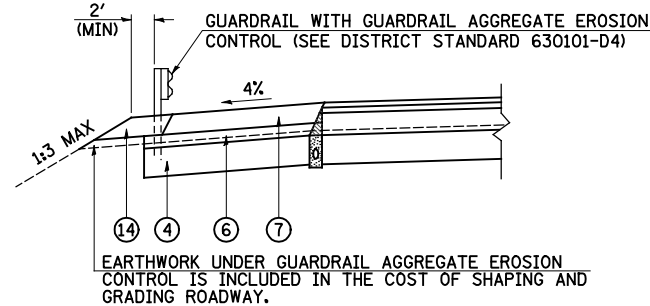
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------------------------|-----------|---------------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 35 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |



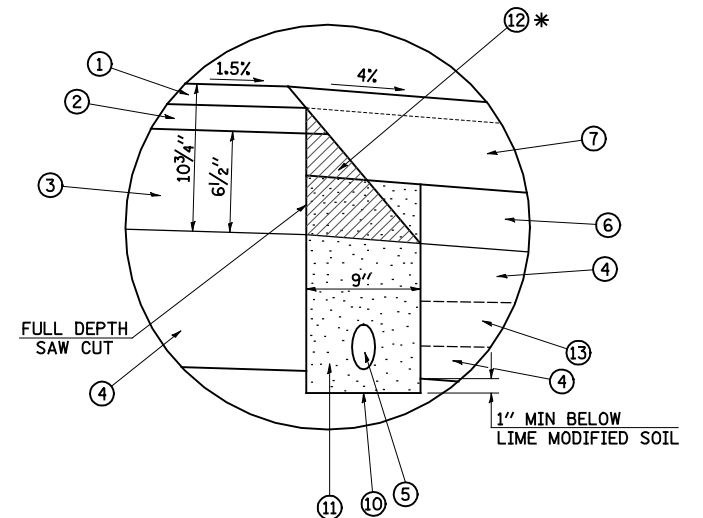
IL 336 / IL 110 SUPERELEVATED SECTION
 STA 548+25.25 TO STA 575+11.16 - CURVE #1 LT

LEGEND

- ① POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
- ② POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 2 1/4"
- ③ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 6 1/2"
- ④ LIME MODIFIED SOIL (12" MIN)
- ⑤ PIPE UNDERDRAINS 4"
- ⑥ COMPACTED SUBGRADE (EMBANKMENT)
- ⑦ HOT-MIX ASPHALT SHOULDERS, 8"
- ⑧ AGGREGATE SHOULDERS, TYPE B (8")
- ⑨ CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- ⑩ 9" TRENCH FOR PIPE UNDERDRAINS (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- ⑪ FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑫ HOT-MIX ASPHALT WEDGE TO BE REMOVED PRIOR TO TRENCHING (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑬ PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- ⑭ GUARDRAIL AGGREGATE EROSION CONTROL
- ⑮ EXISTING PRE-GRADE GROUND LINE (11 1/4" BELOW PGL)
- ⑯ AGGREGATE WEDGE SHOULDER, TYPE B (8") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B
- (A) SEEDING, CLASS 2A

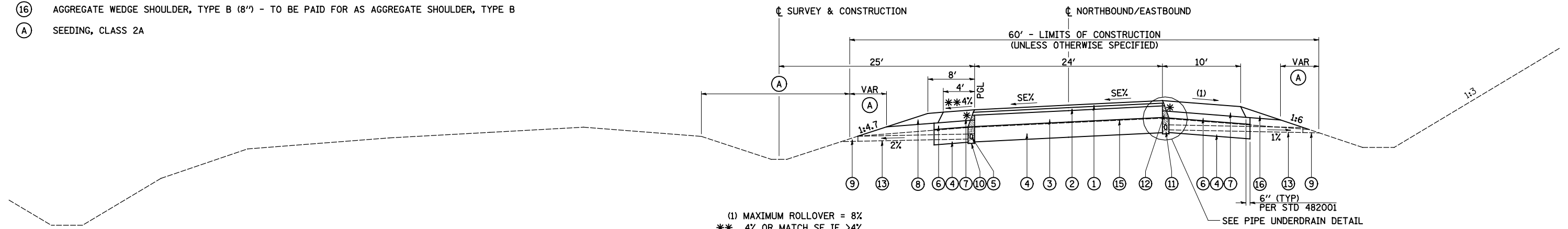


GUARDRAIL SECTION



PIPE UNDERDRAIN DETAIL

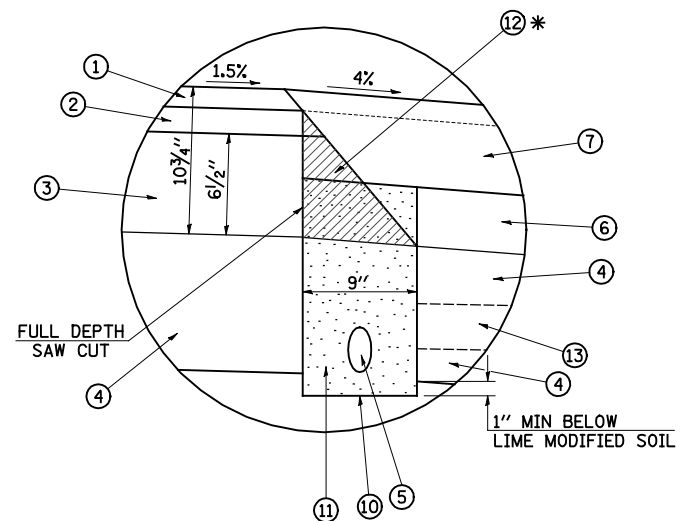
* CONSTRUCT ② AND ③. SAWCUT AS SHOWN IN DETAIL. THEN CONSTRUCT ⑥. CONSTRUCT ⑤, ⑩, AND ⑪. CONSTRUCT ⑦ TO TOP OF ② PRIOR TO PLACING ①. CONSTRUCT ① AND COMPLETE CONSTRUCTION OF ⑦.



IL 336 / IL 110 SUPERELEVATED SECTION
 STA 635+34.06 TO STA 648+71.14 - CURVE #3 RT
 STA 657+80.17 TO STA 680+16.85 - CURVE #4 RT
 STA 680+16.85 TO STA 682+27.34 - BRIDGE & APPROACH PAVEMENT OMISSION
 STA 682+27.34 TO STA 702+13.27 - CURVE #4 RT
 STA 702+13.27 TO STA 703+26.73 - BRIDGE & APPROACH PAVEMENT OMISSION
 STA 703+26.73 TO STA 708+30.50 - CURVE #4 RT

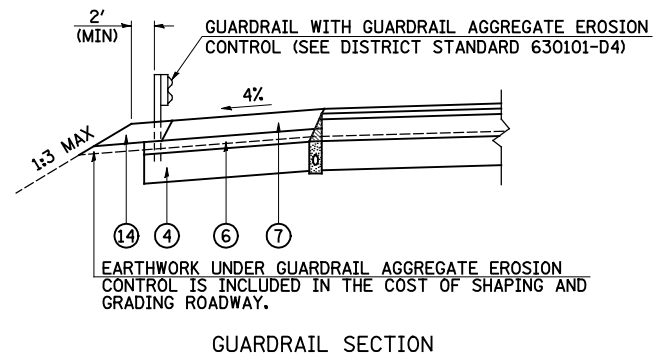
SEE SUPERELEVATION TRANSITION DETAILS FOR ATTAINMENT AND REMOVAL INFORMATION.

| | | | | | | | | | | | | |
|--|-------------------|----------------|-----------|---|--|-----------------------------|-----------|-------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 / IL 110) TYPICAL SECTIONS - IL 336 / IL 110 SUPERELEVATED SECTION (ALT. B) | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-typical03.dgn | DRAWN - TJD | REVISED - | | | 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 36 | | | |
| PLOT SCALE = 20.00' / IN. | CHECKED - JRB | REVISED - | | | CONTRACT NO. 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | | | |



PIPE UNDERDRAIN DETAIL

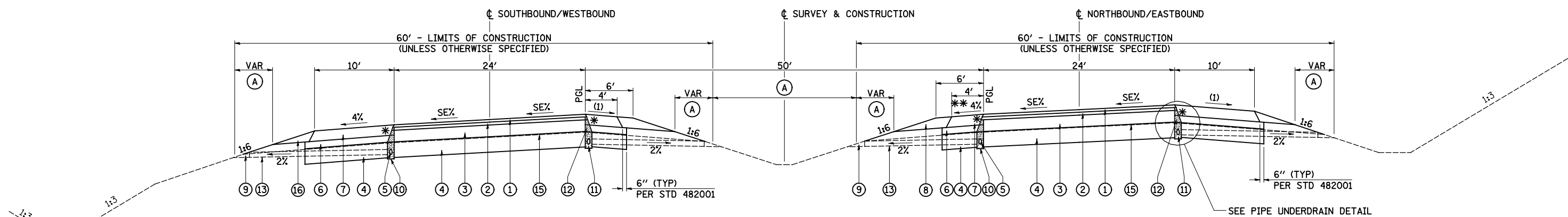
* CONSTRUCT (2) AND (3). SAWCUT AS SHOWN IN DETAIL. THEN CONSTRUCT (6).
 CONSTRUCT (5), (10), AND (11). CONSTRUCT (7) TO TOP OF (2) PRIOR TO PLACING (1).
 CONSTRUCT (1) AND COMPLETE CONSTRUCTION OF (7).



GUARDRAIL SECTION

LEGEND

- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 2 1/4"
- (3) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 6 1/2"
- (4) LIME MODIFIED SOIL (12" MIN)
- (5) PIPE UNDERDRAINS 4"
- (6) COMPACTED SUBGRADE (EMBANKMENT)
- (7) HOT-MIX ASPHALT SHOULDERS, 8"
- (8) AGGREGATE SHOULDERS, TYPE B (8")
- (9) CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- (10) 9" TRENCH FOR PIPE UNDERDRAINS (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- (11) FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- (12) HOT-MIX ASPHALT WEDGE TO BE REMOVED PRIOR TO TRENCHING (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- (13) PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- (14) GUARDRAIL AGGREGATE EROSION CONTROL
- (15) EXISTING PRE-GRADE GROUND LINE (11 1/4" BELOW PGL)
- (16) AGGREGATE WEDGE SHOULDER, TYPE B (8") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B
- (A) SEEDING, CLASS 2A



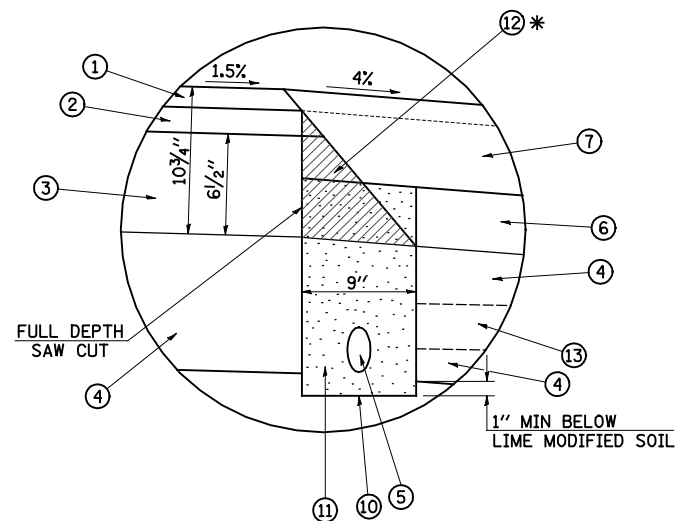
(1) MAXIMUM ROLLOVER = 8%
 ** 4% OR MATCH SE IF >4%

IL 336 /IL 110 SUPERELEVATED SECTION

STA 718+25.64 TO STA 754+54.14 - CURVE #5 LT
 STA 796+76.36 TO STA 835+53.62 - CURVE #6 RT

SEE SUPERELEVATION TRANSITION DETAILS FOR ATTAINMENT AND REMOVAL INFORMATION.

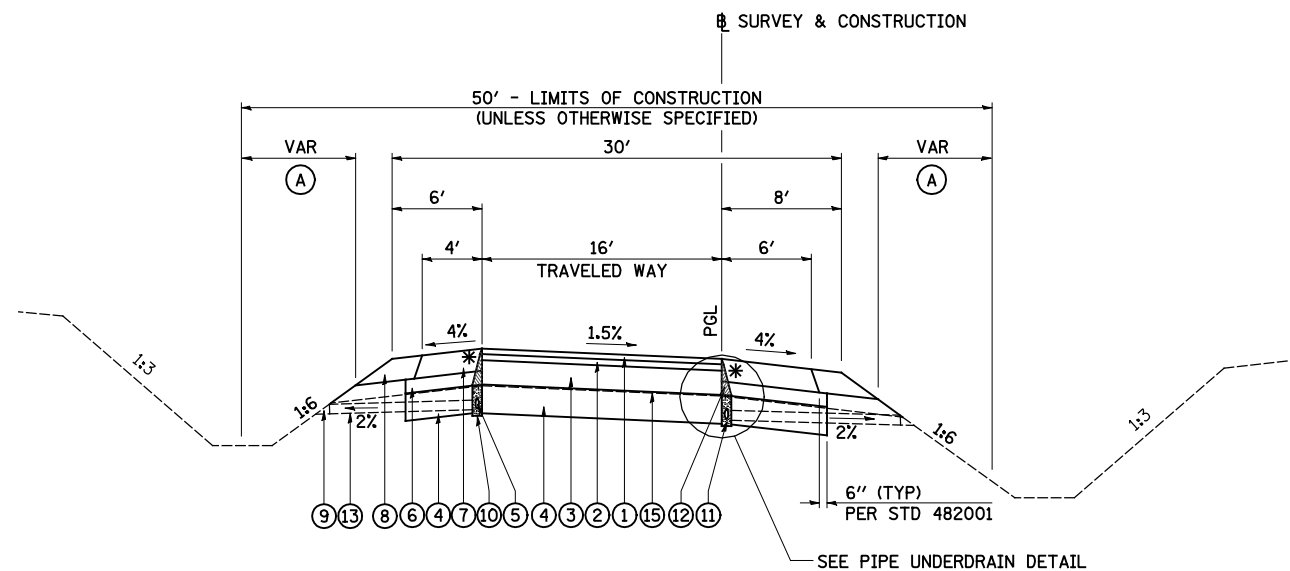
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|--|-------------------|----------------|---------------------------|---|--|-----------------------------|-----------|--------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - IL 336 /IL 110 SUPERELEVATED SECTION (ALT. B) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-typical04.dgn | DRAWN - TJD | REVISED - | 407 | | | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 37 | |
| PLOT SCALE = 20.00' / IN. | CHECKED - JRB | REVISED - | CONTRACT NO. 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |



PIPE UNDERDRAIN DETAIL

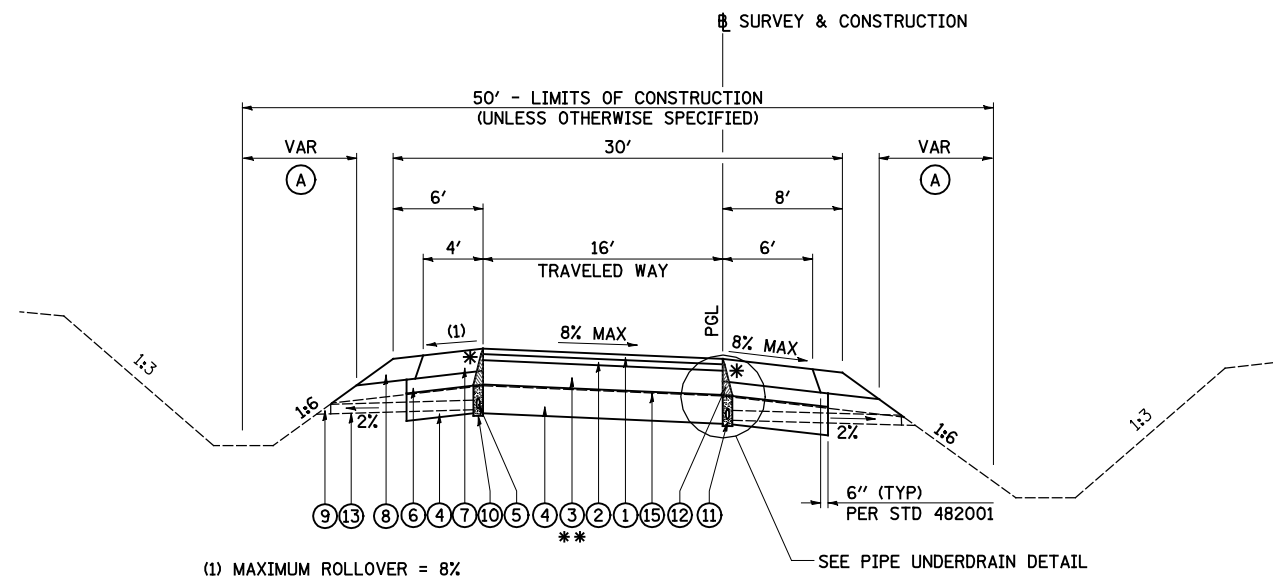
* CONSTRUCT (2) AND (3). SAWCUT AS SHOWN IN DETAIL. THEN CONSTRUCT (6). CONSTRUCT (5), (10), AND (11). CONSTRUCT (7) TO TOP OF (2) PRIOR TO PLACING (1). CONSTRUCT (1) AND COMPLETE CONSTRUCTION OF (7).

- LEGEND**
- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
 - (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 2 1/4"
 - (3) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 6 1/2"
 - (4) LIME MODIFIED SOIL (12" MIN)
 - (5) PIPE UNDERDRAINS 4"
 - (6) COMPACTED SUBGRADE (EMBANKMENT)
 - (7) HOT-MIX ASPHALT SHOULDERS, 8"
 - (8) AGGREGATE SHOULDERS, TYPE B (8")
 - (9) CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
 - (10) 9" TRENCH FOR PIPE UNDERDRAINS (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
 - (11) FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
 - (12) HOT-MIX ASPHALT WEDGE TO BE REMOVED PRIOR TO TRENCHING (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
 - (13) PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
 - (14) GUARDRAIL AGGREGATE EROSION CONTROL
 - (15) EXISTING PRE-GRADE GROUND LINE (11 1/4" BELOW PGL)
 - (16) AGGREGATE WEDGE SHOULDER, TYPE B (8") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B
 - (A) SEEDING, CLASS 2A



TYPICAL RAMP TANGENT SECTION

RAMP K (US 136) - STA 101+45.00 TO STA 103+23.25
 RAMP L (US 136) - STA 68+31.07 TO STA 72+04.23



TYPICAL RAMP SUPERELEVATED SECTION

RAMP K (US 136) - STA 103+23.25 TO STA 119+61.56
 RAMP L (US 136) - STA 56+58.52 TO STA 68+31.07
 RAMP A (US 67) - STA 322+39.93 TO STA 332+74.54
 RAMP D (US 67) - STA 206+60.09 TO STA 219+04.55

** RAMP A (US 67) - STA 321+07.18 TO STA 322+39.93 - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 7"
 RAMP D (US 67) - STA 219+04.55 TO STA 220+14.88 - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 7"

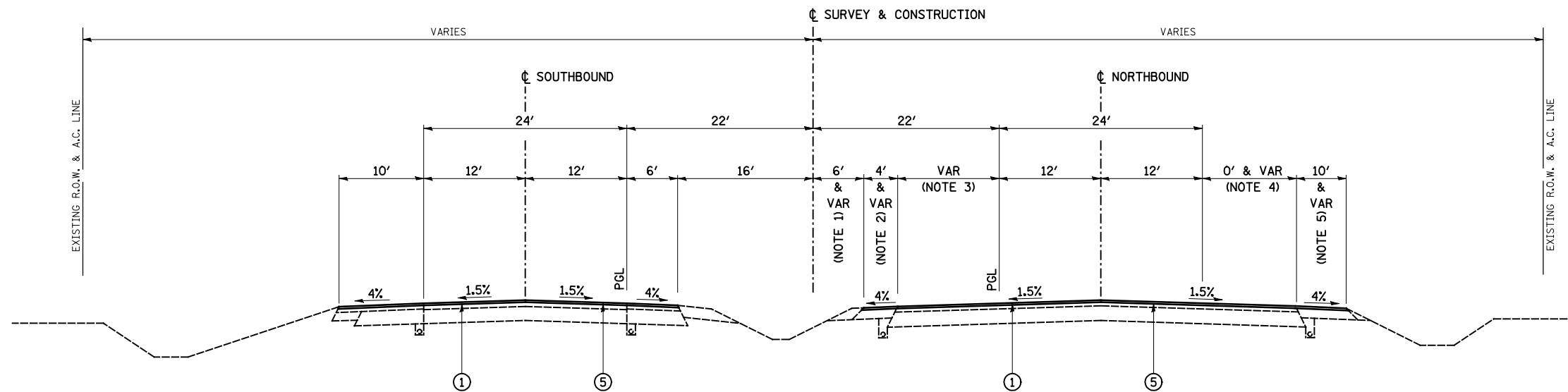
| | | | |
|--|-------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-typical05.dgn | | DRAWN - TJD | REVISED - |
| PLOT SCALE = 20.00 FT / IN. | | CHECKED - JRB | REVISED - |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAP ROUTE 407 (IL 336 /IL 110)
 TYPICAL SECTIONS - INTERCHANGE RAMP K, L, A & D (ALT. B)**

SCALE: N/A SHEET NO. 6 OF 6 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------------------------|-----------|---------------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 38 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |



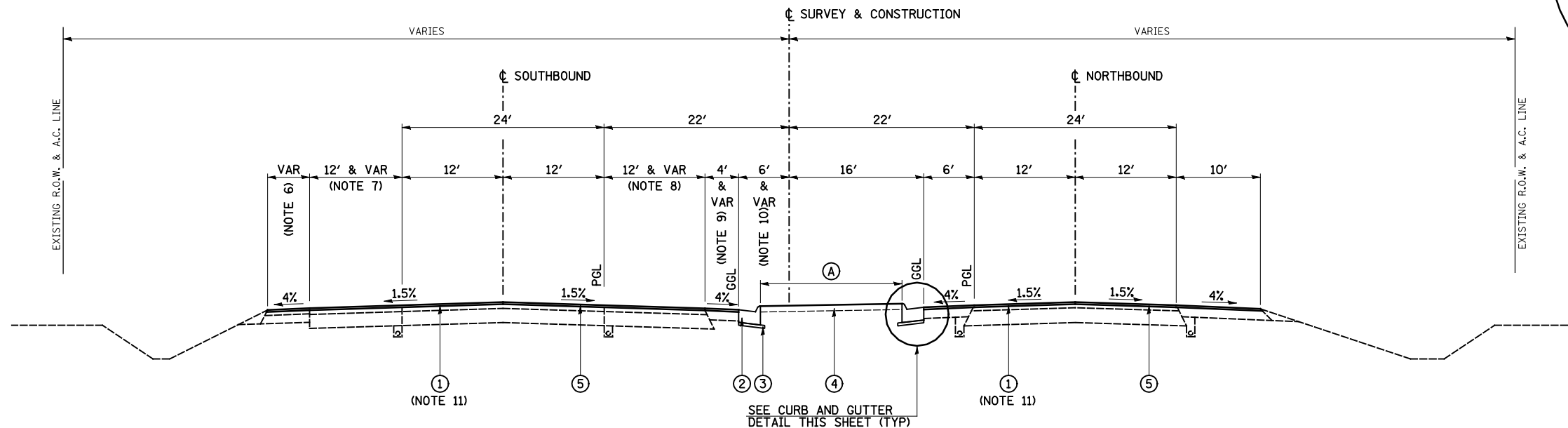
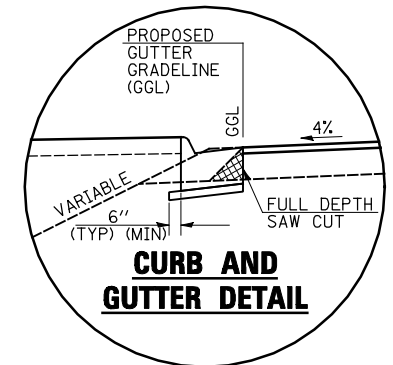
US 67 SECTION NO. 1
 STA 429+64.16 TO STA 435+24.79

NOTES:

1. VARIES FROM 16' @ STA 429+64.16 TO 6' @ STA 431+84.79
2. VARIES FROM 6' @ STA 429+64.16 TO 4' @ STA 431+84.79
3. VARIES FROM 0' @ STA 429+64.16 TO 12' @ STA 431+84.79
4. VARIES FROM 0' @ STA 430+32.62 TO 12' @ STA 431+44.65
5. VARIES FROM 10' @ STA 430+32.62 TO 5' @ STA 431+44.67 AND VARIES FROM 5' @ STA 431+44.67 TO 7' @ STA 433+92.98
6. VARIES FROM 5' @ STA 435+92.13 TO 6' @ STA 438+21.87 AND VARIES FROM 6' @ STA 438+21.87 TO 8' @ STA 438+46.31
7. VARIES FROM 12' @ STA 438+21.87 TO 10.1' @ STA 438+46.31
8. VARIES FROM 12' @ STA 437+51.55 TO 7.2' @ STA 438+46.31
9. VARIES FROM 4' @ STA 437+51.55 TO 5.5' @ STA 438+46.31
10. VARIES FROM 6' @ STA 437+51.55 TO 9.7' @ STA 438+46.31
11. FROM STA 437+50.00 TO STA 438+46.31 HMA SURFACE SHOULD BE LAID TO THE PGL SHOWN IN THE ROADWAY PLAN & PROFILE

LEGEND

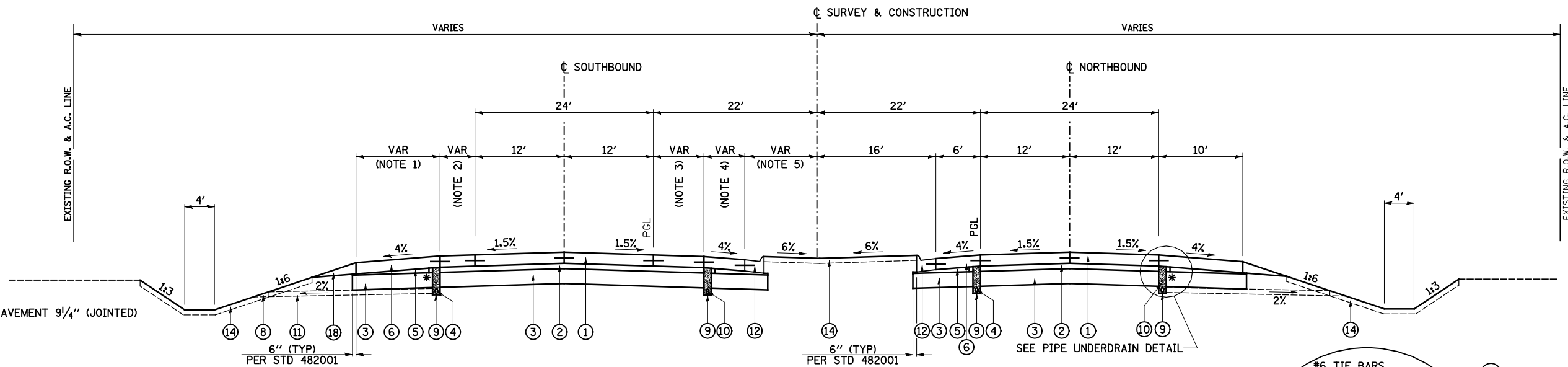
- ① POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
- ② COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- ③ SUBBASE GRANULAR MATERIAL, TYPE C 4"
- ④ TOPSOIL FURNISH AND PLACE, 4"
- ⑤ HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- (A) SEEDING, CLASS 2A



US 67 SECTION NO. 2
 STA 435+24.79 TO STA 438+46.31

SEE CURB AND GUTTER DETAIL THIS SHEET (TYP)

| | | | | | | | | | | | | |
|--|-------------------|----------------|-----------|---|--|-----------------------------|-----------|-------------|---------------------------|--------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - FAP ROUTE 310 (US 67) | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-US67typical03.dgn | DRAWN - TJD | REVISOR - | REVISOR - | | 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 39 | | | |
| PLOT SCALE = 16.000' / IN. | CHECKED - JRB | REVISOR - | REVISOR - | | CONTRACT NO. 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISOR - | REVISOR - | | SCALE: N/A | SHEET NO. 1 OF 1 SHEETS | STA. N/A | TO STA. N/A | ILLINOIS FED. AID PROJECT | | | |



LEGEND

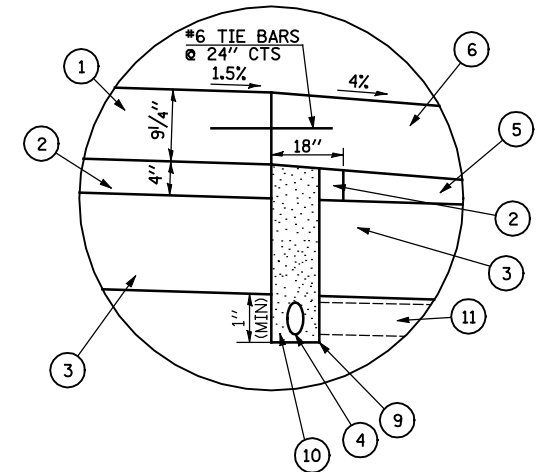
- ① PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
- ② STABILIZED SUBBASE 4"
- ③ LIME MODIFIED SOIL (12" MIN)
- ④ PIPE UNDERDRAINS 4"
- ⑤ COMPACTED SUBGRADE (EMBANKMENT)
- ⑥ PORTLAND CEMENT CONCRETE SHOULDERS, 9 1/4"
- ⑦ AGGREGATE SHOULDERS, TYPE B (9 1/4")
- ⑧ CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- ⑨ UNDERDRAIN TRENCH (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- ⑩ FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑪ PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- ⑫ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- ⑬ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- ⑭ TOPSOIL FURNISH AND PLACE, 4"
- ⑮ CONCRETE MEDIAN SURFACE, 6 INCH
- ⑯ CONCRETE GUTTER, TYPE A (SPECIAL)
- ⑰ AGGREGATE WEDGE SHOULDER, TYPE B (9 1/4") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B

US 67 SECTION NO. 3

STA 438+46.31 TO STA 439+90.70

RIGID PAVEMENT DESIGN INFORMATION

| | | |
|--|-------------------|----------|
| STRUCTURAL DESIGN TRAFFIC: 10,475 | YEAR: 2032 | |
| PV = 9,218 | SU = 314 | MU = 943 |
| ROAD/STREET CLASSIFICATION: RURAL | CLASS: 1 | |
| PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE: | | |
| PV = 32% | SU = 45% | MU = 45% |
| RIGID TRAFFIC FACTOR: ACTUAL TF = 6.32 | MINIMUM TF = 5.02 | |
| SUBGRADE SUPPORTING RATING: | | |
| SSR = POOR | | |

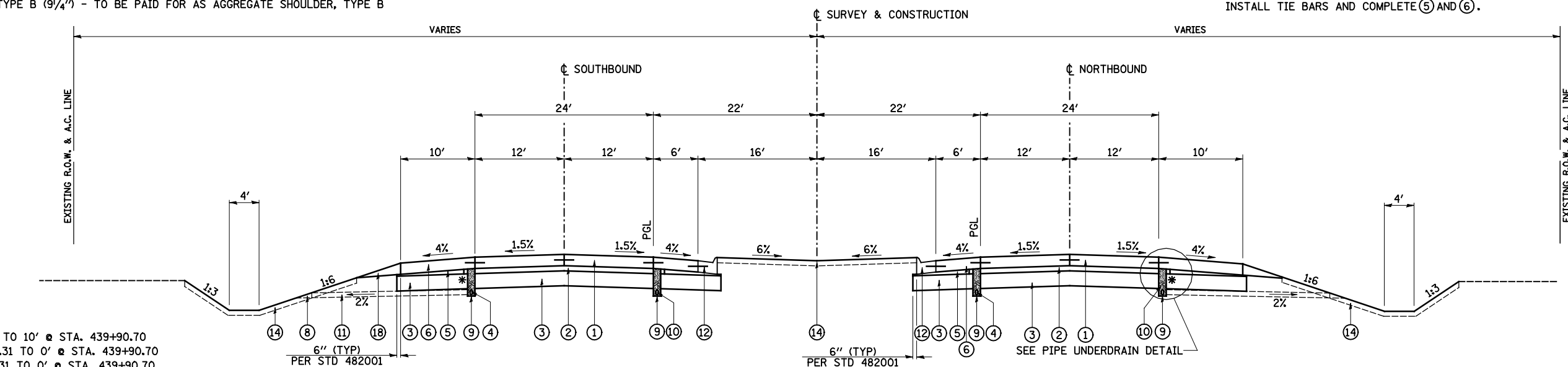


PIPE UNDERDRAIN DETAIL

- * CONSTRUCT ② TO 18" BEYOND EDGE OF PAVEMENT
- CONSTRUCT ① WITHOUT TIE BARS
- CONSTRUCT ⑨ AND COMPLETE CONSTRUCTION OF ④ AND ⑩
- INSTALL TIE BARS AND COMPLETE ⑤ AND ⑥.

NOTES:

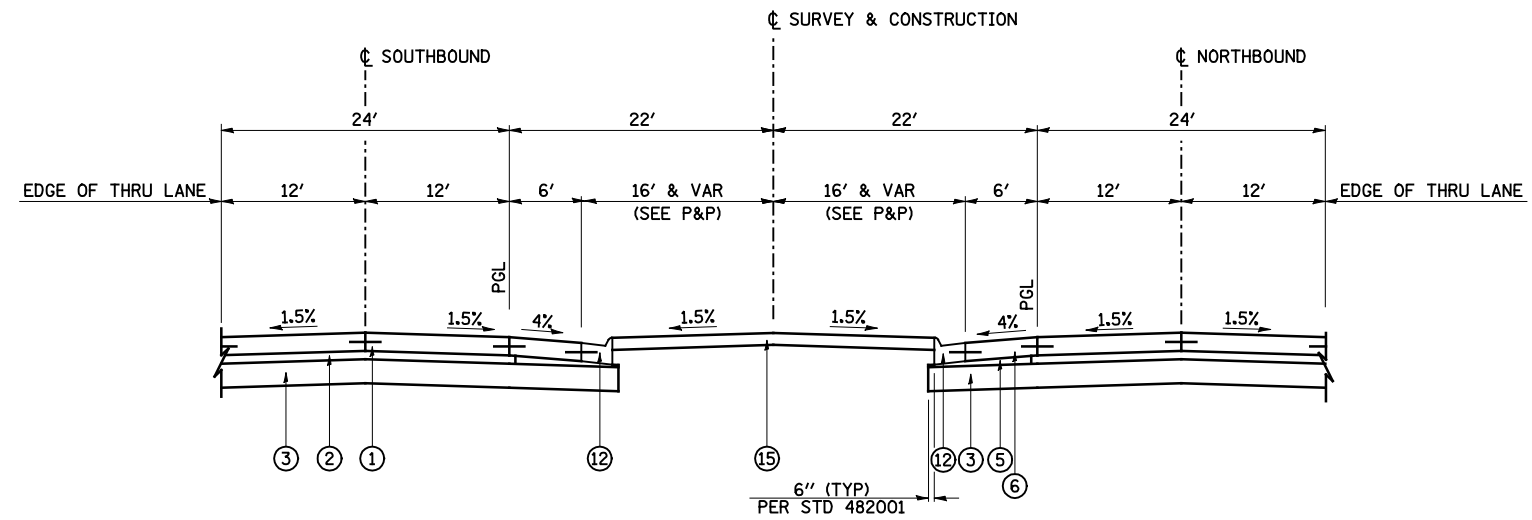
- 1. VARIES FROM 8' @ STA 438+46.31 TO 10' @ STA. 439+90.70
- 2. VARIES FROM 10.1' @ STA. 438+46.31 TO 0' @ STA. 439+90.70
- 3. VARIES FROM 7.2' @ STA. 438+46.31 TO 0' @ STA. 439+90.70
- 4. VARIES FROM 5.5' @ STA. 438+46.31 TO 6' @ STA. 439+90.70
- 5. VARIES FROM 9.7' @ STA. 438+46.31 TO 16' @ STA. 439+90.70



US 67 SECTION NO. 4

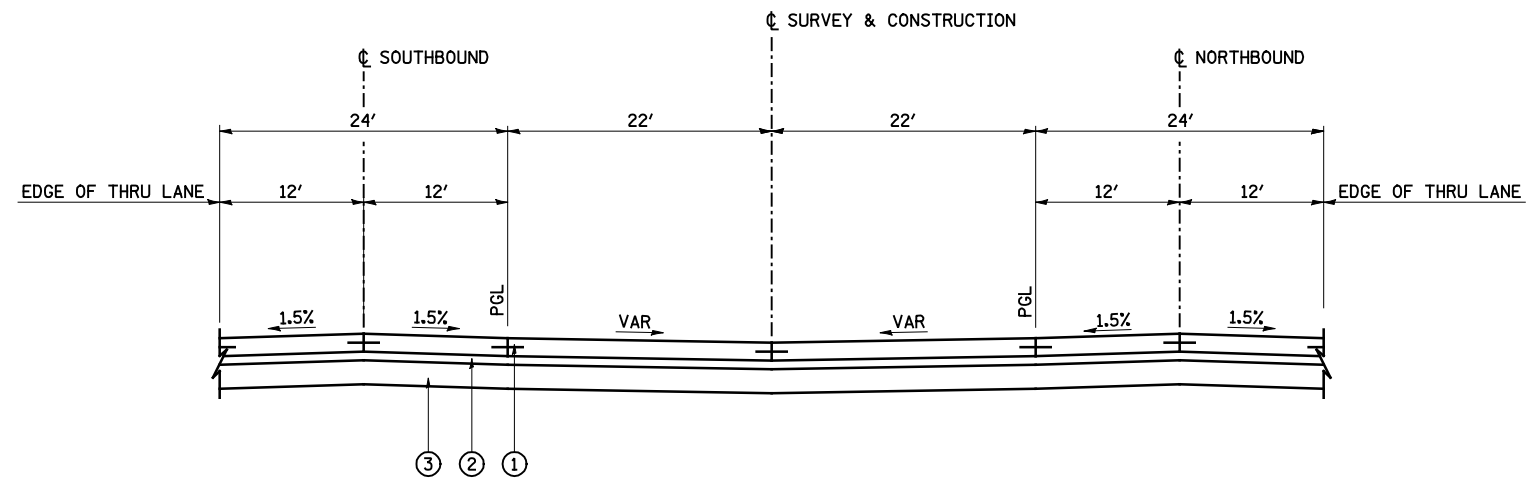
STA 439+90.70 TO STA 444+54.79

| | | | | | | | | | | |
|--|-------------------|----------------|---------------------------|---|---|-----------------------------|-----------|--------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - FAP ROUTE 310 (US 67) (ALT. A) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-US67typical12.dgn | DRAWN - TJD | REVISED - | 407 | | | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 40 | |
| PLOT SCALE = 16.000' / IN. | CHECKED - JRB | REVISED - | CONTRACT NO. 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |



US 67 SECTION NO. 5

STA 444+54.79 TO STA 445+04.26



US 67 SECTION NO. 6

STA 445+04.26 TO STA 445+35.93

LEGEND

- ① PORTLAND CEMENT CONCRETE PAVEMENT 9/4" (JOINTED)
- ② STABILIZED SUBBASE 4"
- ③ LIME MODIFIED SOIL (12" MIN)
- ④ PIPE UNDERDRAINS 4"
- ⑤ COMPACTED SUBGRADE (EMBANKMENT)
- ⑥ PORTLAND CEMENT CONCRETE SHOULDERS, 9/4"
- ⑦ AGGREGATE SHOULDERS, TYPE B (9/4")
- ⑧ CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- ⑨ UNDERDRAIN TRENCH (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- ⑩ FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑪ PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- ⑫ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- ⑬ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- ⑭ TOPSOIL FURNISH AND PLACE, 4"
- ⑮ CONCRETE MEDIAN SURFACE, 6 INCH
- ⑯ CONCRETE MEDIAN, TYPE SM (SPECIAL)
- ⑰ CONCRETE GUTTER, TYPE A (SPECIAL)
- ⑱ AGGREGATE WEDGE SHOULDER, TYPE B (9/4") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B

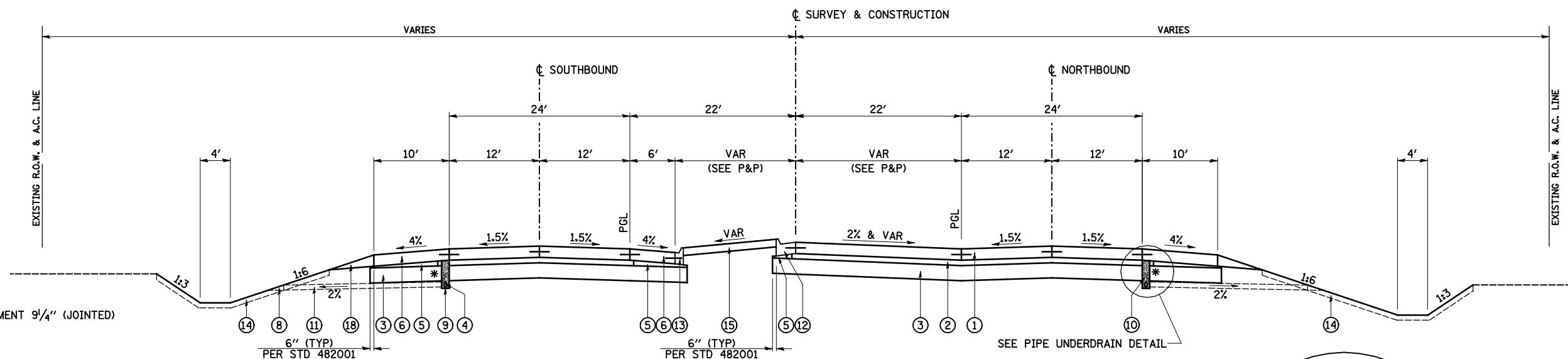
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|--|----------------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-US67typical13.dgn | | DRAWN - TJD | REVISED - |
| | PLOT SCALE = 16.000' / IN. | CHECKED - JRB | REVISED - |
| | PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

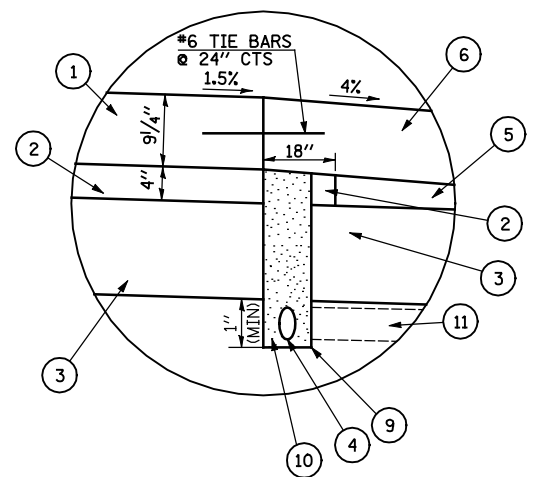
**FAP ROUTE 407 (IL 336 /IL 110)
TYPICAL SECTIONS - FAP ROUTE 310 (US 67) (ALT. A)**

SCALE: N/A SHEET NO. 2 OF 8 SHEETS STA. N/A TO STA. N/A

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------------------------|-----------|--------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 41 |
| | | | CONTRACT NO. 68B44 | |
| ILLINOIS FED. AID PROJECT | | | | |



US 67 SECTION NO. 7
 STA 445+35.93 TO STA 445+72.65

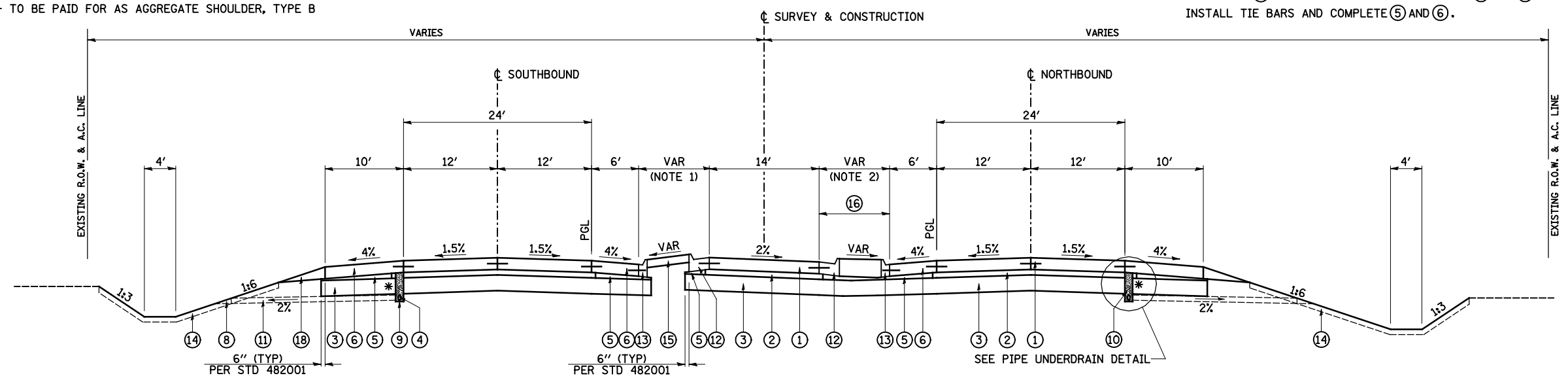


PIPE UNDERDRAIN DETAIL

* CONSTRUCT ② TO 18" BEYOND EDGE OF PAVEMENT
 CONSTRUCT ① WITHOUT TIE BARS
 CONSTRUCT ⑨ AND COMPLETE CONSTRUCTION OF ④ AND ⑩
 INSTALL TIE BARS AND COMPLETE ⑤ AND ⑥.

LEGEND

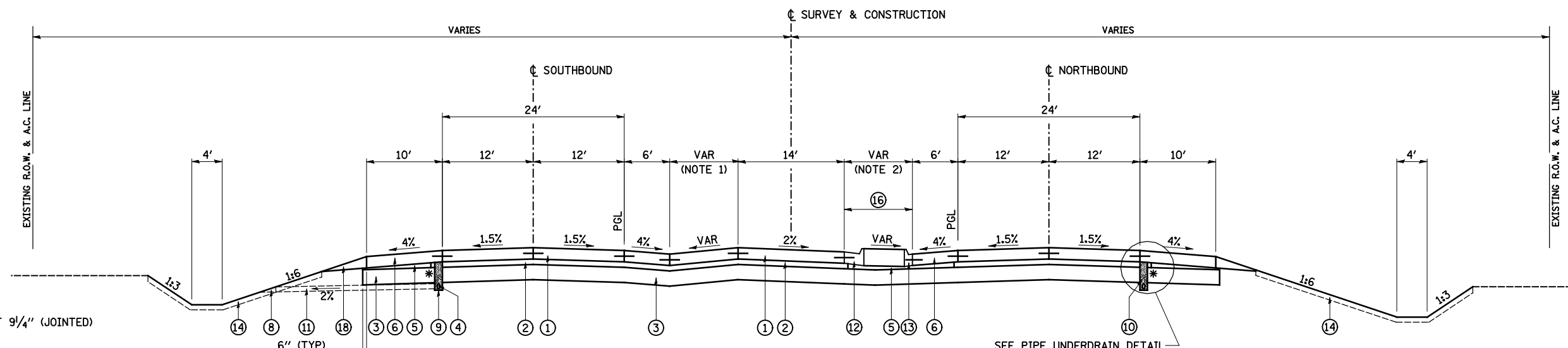
- ① PORTLAND CEMENT CONCRETE PAVEMENT 9/4" (JOINTED)
- ② STABILIZED SUBBASE 4"
- ③ LIME MODIFIED SOIL (12" MIN)
- ④ PIPE UNDERDRAINS 4"
- ⑤ COMPACTED SUBGRADE (EMBANKMENT)
- ⑥ PORTLAND CEMENT CONCRETE SHOULDERS, 9/4"
- ⑦ AGGREGATE SHOULDERS, TYPE B (9/4")
- ⑧ CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- ⑨ UNDERDRAIN TRENCH (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- ⑩ FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑪ PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- ⑫ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- ⑬ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- ⑭ TOPSOIL FURNISH AND PLACE, 4"
- ⑮ CONCRETE MEDIAN SURFACE, 6 INCH
- ⑯ CONCRETE MEDIAN, TYPE SM (SPECIAL)
- ⑰ CONCRETE GUTTER, TYPE A (SPECIAL)
- ⑱ AGGREGATE WEDGE SHOULDER, TYPE B (9/4") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B



US 67 SECTION NO. 8
 STA 445+72.65 TO STA 446+78.12

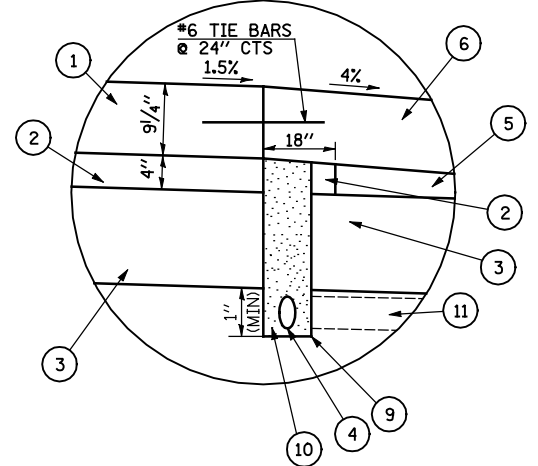
- NOTES:**
- VARIES FROM 14' @ STA 445+72.65 TO 8' @ STA 446+78.20
 - VARIES FROM 4' @ STA 445+72.65 TO 10' @ STA 446+78.20

| | | | | | | | | | |
|---|----------------------------|------------------|-----------|---|---------------------|--|---------------------|---------------------|-----------------|
| FILE NAME = v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-US67typcoa114.dgn | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | F.A.P. ROUTE 407 | SECTION 55C3(PV,HB(2-6);B,B-1,B-2)] | COUNTY McDONOUGH | TOTAL SHEETS 874 | SHEET NO. 42 |
| | PLOT SCALE = 16.000' / IN. | DRAWN - TJD | REVISED - | | | | | | |
| PLOT DATE = 1/26/2015 | CHECKED - JRB | DATE - 1-30-2015 | REVISED - | | | | | | |

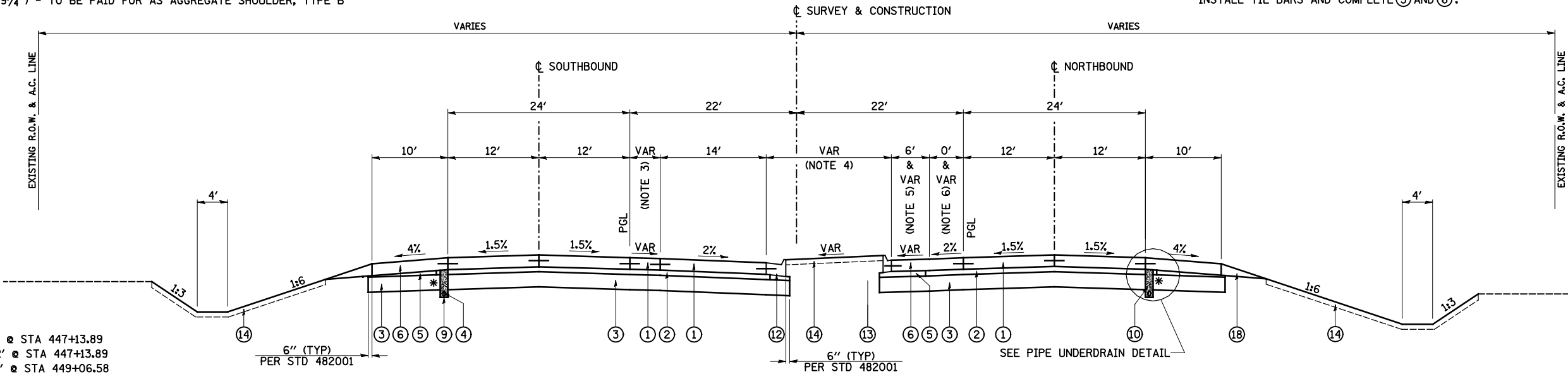


- LEGEND**
- ① PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
 - ② STABILIZED SUBBASE 4"
 - ③ LIME MODIFIED SOIL (12" MIN)
 - ④ PIPE UNDERDRAINS 4"
 - ⑤ COMPACTED SUBGRADE (EMBANKMENT)
 - ⑥ PORTLAND CEMENT CONCRETE SHOULDERS, 9 1/4"
 - ⑦ AGGREGATE SHOULDERS, TYPE B (9 1/4")
 - ⑧ CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
 - ⑨ UNDERDRAIN TRENCH (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
 - ⑩ FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
 - ⑪ PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
 - ⑫ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
 - ⑬ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
 - ⑭ TOPSOIL FURNISH AND PLACE, 4"
 - ⑮ CONCRETE MEDIAN SURFACE, 6 INCH
 - ⑯ CONCRETE MEDIAN, TYPE SM (SPECIAL)
 - ⑰ CONCRETE GUTTER, TYPE A (SPECIAL)
 - ⑱ AGGREGATE WEDGE SHOULDER, TYPE B (9 1/4") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B

US 67 SECTION NO. 9
 STA 446+78.12 TO STA 447+13.89



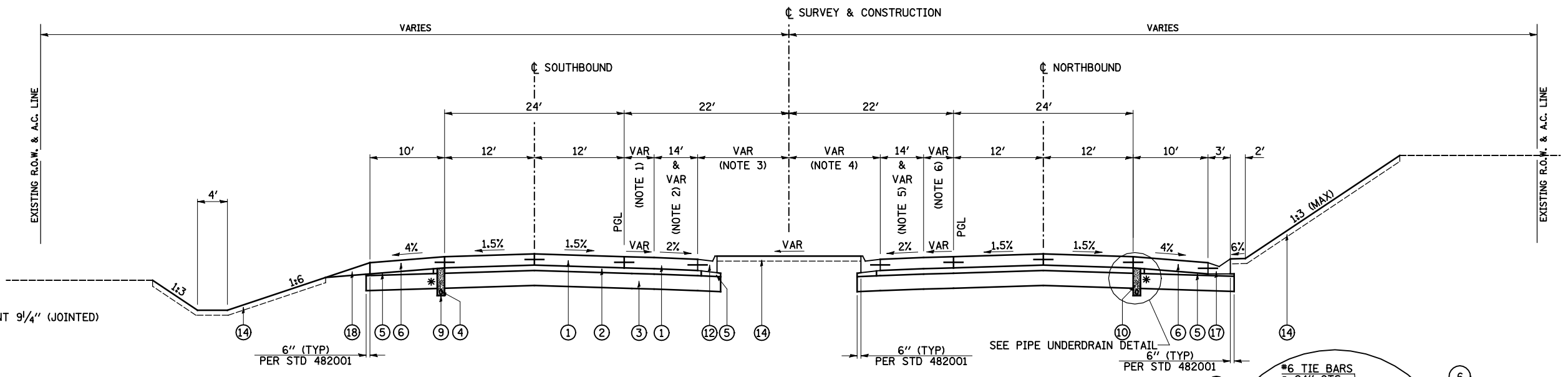
- * CONSTRUCT ② TO 18" BEYOND EDGE OF PAVEMENT
 CONSTRUCT ① WITHOUT TIE BARS
 CONSTRUCT ⑨ AND COMPLETE CONSTRUCTION OF ④ AND ⑩
 INSTALL TIE BARS AND COMPLETE ⑤ AND ⑥.



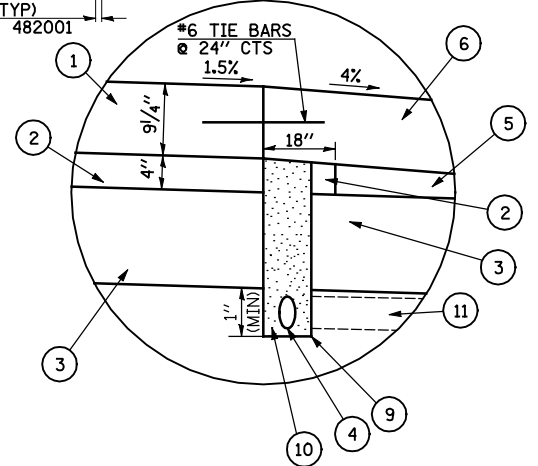
- NOTES:**
1. VARIES FROM 8' @ STA 446+78.20 TO 12' @ STA 447+13.89
 2. VARIES FROM 10' @ STA 446+78.20 TO 12' @ STA 447+13.89
 3. VARIES FROM 12' @ STA 447+13.89 TO 1.1' @ STA 449+06.58
 4. VARIES FROM 12' @ STA 447+13.89 TO 22.9' @ STA 449+06.58
 5. VARIES FROM 5' @ STA 448+44.88 TO 1.5' @ STA 449+06.58
 6. VARIES FROM 1' @ STA 448+44.88 TO 4.5' @ STA 449+06.58

US 67 SECTION NO. 10
 STA 447+13.89 TO STA 449+06.58

| | | | | | | | | | | |
|---|-------------------|----------------|---------------------------|---|---|-----------------------------|-----------|-------------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - FAP ROUTE 310 (US 67) (ALT. A) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-US67typical115.dgn | DRAWN - TJD | REVISED - | 407 | | | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 43 | |
| PLOT SCALE = 16.000' / IN. | CHECKED - JRB | REVISED - | CONTRACT NO. FAP 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | SCALE: N/A | SHEET NO. 4 OF 8 SHEETS | STA. N/A | TO STA. N/A | | |



US 67 SECTION NO. 11
STA 449+06.58 TO STA 450+93.40



PIPE UNDERDRAIN DETAIL

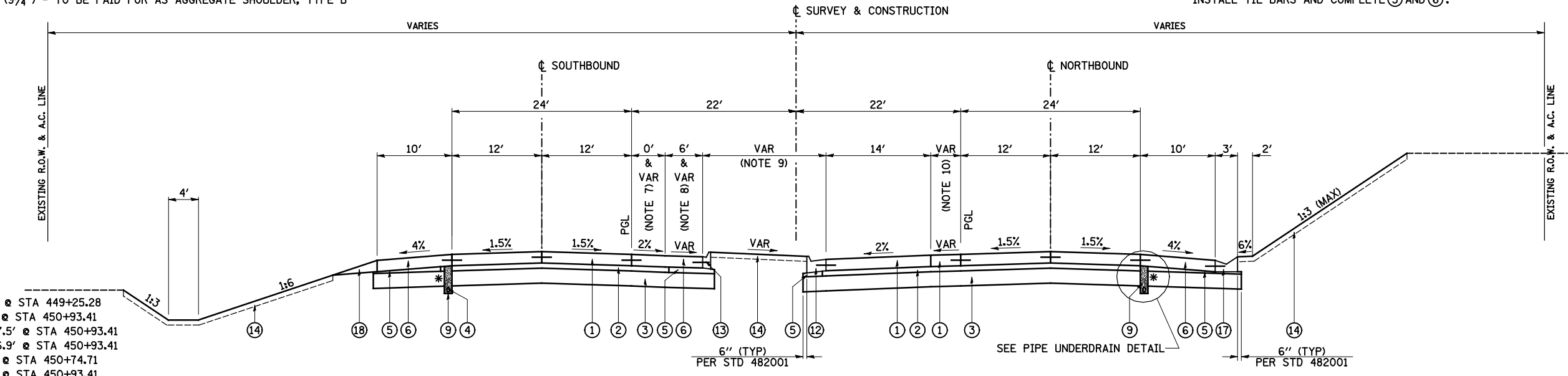
* CONSTRUCT (2) TO 18" BEYOND EDGE OF PAVEMENT
CONSTRUCT (1) WITHOUT TIE BARS
CONSTRUCT (9) AND COMPLETE CONSTRUCTION OF (4) AND (10)
INSTALL TIE BARS AND COMPLETE (5) AND (6).

LEGEND

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
- (2) STABILIZED SUBBASE 4"
- (3) LIME MODIFIED SOIL (12" MIN)
- (4) PIPE UNDERDRAINS 4"
- (5) COMPACTED SUBGRADE (EMBANKMENT)
- (6) PORTLAND CEMENT CONCRETE SHOULDERS, 9 1/4"
- (7) AGGREGATE SHOULDERS, TYPE B (9 1/4")
- (8) CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- (9) UNDERDRAIN TRENCH (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- (10) FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- (11) PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- (12) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- (13) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- (14) TOPSOIL FURNISH AND PLACE, 4"
- (15) CONCRETE MEDIAN SURFACE, 6 INCH
- (16) CONCRETE MEDIAN, TYPE SM (SPECIAL)
- (17) CONCRETE GUTTER, TYPE A (SPECIAL)
- (18) AGGREGATE WEDGE SHOULDER, TYPE B (9 1/4") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B

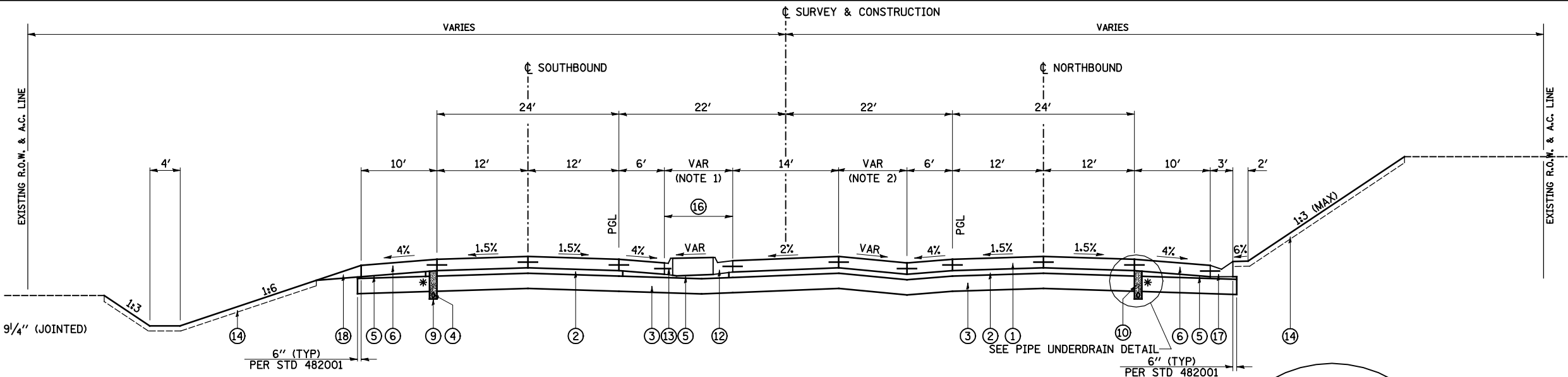
NOTES:

- 1. VARIES FROM 1.1' @ STA 449+06.58 TO 0' @ STA 449+25.28
- 2. VARIES FROM 14' @ STA 449+25.28 TO 5' @ STA 450+93.41
- 3. VARIES FROM 6.9' @ STA 449+06.58 TO 17.5' @ STA 450+93.41
- 4. VARIES FROM 17.5' @ STA 449+06.58 TO 6.9' @ STA 450+93.41
- 5. VARIES FROM 5' @ STA 449+06.58 TO 14' @ STA 450+74.71
- 6. VARIES FROM 0' @ STA 450+74.71 TO 1.1' @ STA 450+93.41
- 7. VARIES FROM 5' @ STA 450+93.41 TO 0' @ STA 451+55.11
- 8. VARIES FROM 1.5' @ STA 450+93.41 TO 5' @ STA 451+55.11
- 9. VARIES FROM 24.4' @ STA 450+93.41 TO 12' @ STA 452+86.10
- 10. VARIES FROM 1.1' @ STA 450+93.41 TO 12' @ STA 452+86.10

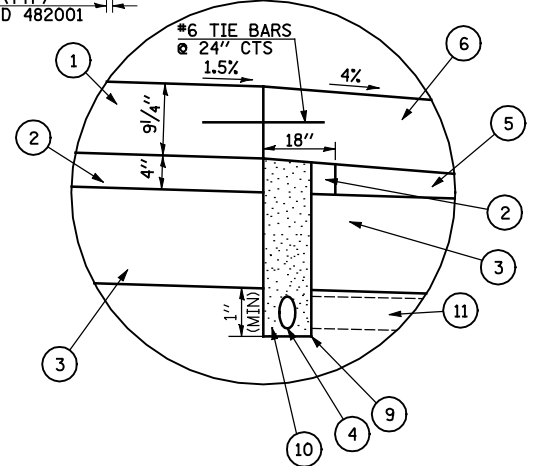


US 67 SECTION NO. 12
STA 450+93.40 TO STA 452+86.10

| | | | | | | | | | | | | |
|---|-------------------|----------------|-----------|---|---|-----------------------------|-----------|-------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - FAP ROUTE 310 (US 67) (ALT. A) | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-US67\typroa116.dgn | DRAWN - TJD | REVISED - | | | 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 44 | | | |
| PLOT SCALE = 16.000' / IN. | CHECKED - JRB | REVISED - | | | CONTRACT NO. 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | | | |



US 67 SECTION NO. 13
STA 452+86.10 TO STA 453+21.79

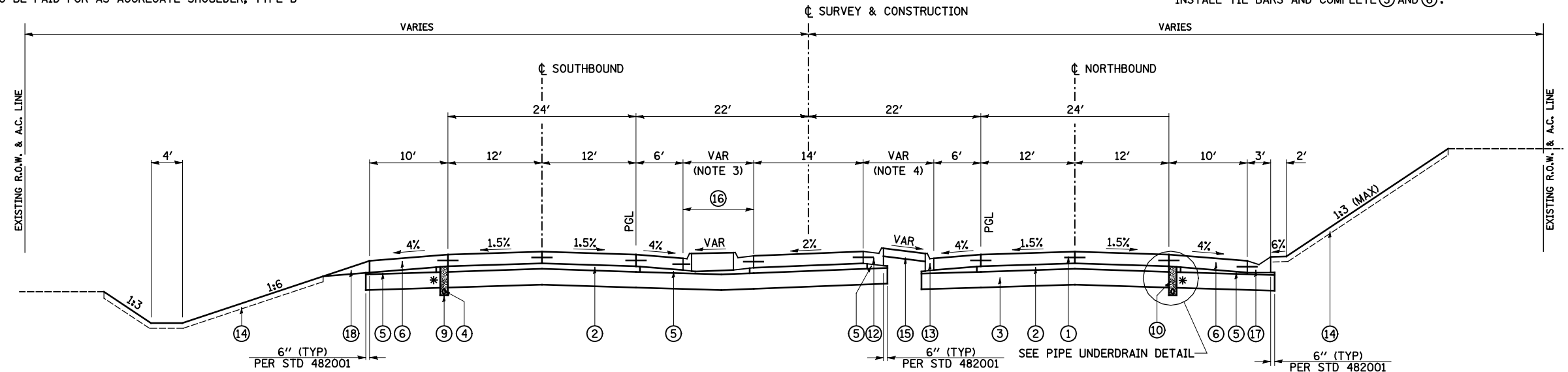


PIPE UNDERDRAIN DETAIL

* CONSTRUCT (2) TO 18" BEYOND EDGE OF PAVEMENT
 CONSTRUCT (1) WITHOUT TIE BARS
 CONSTRUCT (9) AND COMPLETE CONSTRUCTION OF (4) AND (10)
 INSTALL TIE BARS AND COMPLETE (5) AND (6).

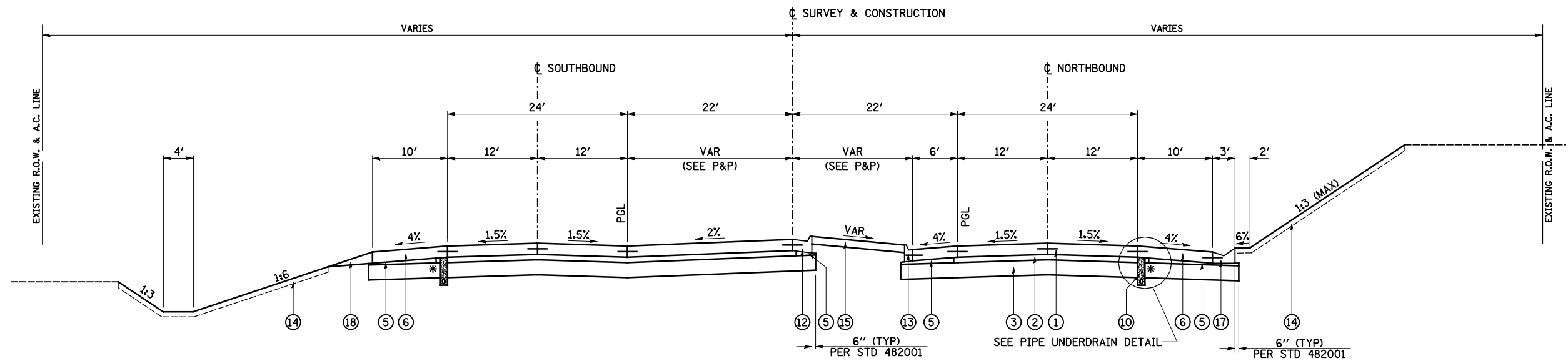
LEGEND

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 9/4" (JOINTED)
- (2) STABILIZED SUBBASE 4"
- (3) LIME MODIFIED SOIL (12" MIN)
- (4) PIPE UNDERDRAINS 4"
- (5) COMPACTED SUBGRADE (EMBANKMENT)
- (6) PORTLAND CEMENT CONCRETE SHOULDERS, 9/4"
- (7) AGGREGATE SHOULDERS, TYPE B (9/4")
- (8) CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- (9) UNDERDRAIN TRENCH (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- (10) FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- (11) PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- (12) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- (13) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- (14) TOPSOIL FURNISH AND PLACE, 4"
- (15) CONCRETE MEDIAN SURFACE, 6 INCH
- (16) CONCRETE MEDIAN, TYPE SM (SPECIAL)
- (17) CONCRETE GUTTER, TYPE A (SPECIAL)
- (18) AGGREGATE WEDGE SHOULDER, TYPE B (9/4") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B

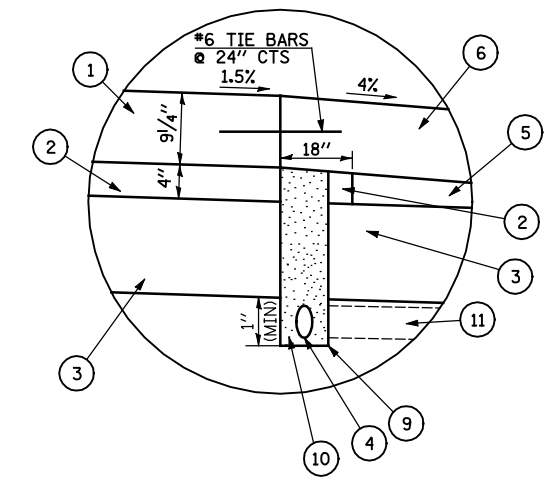


US 67 SECTION NO. 14
STA 453+21.79 TO STA 454+27.23

- NOTES:**
- VARIABLES FROM 12' @ STA 452+86.10 TO 10' @ STA 453+21.79
 - VARIABLES FROM 12' @ STA 452+86.10 TO 8' @ STA 453+21.79
 - VARIABLES FROM 10' @ STA 453+21.79 TO 4' @ STA 454+27.23
 - VARIABLES FROM 8' @ STA 453+21.79 TO 14' @ STA 454+27.23



US 67 SECTION NO. 15
 STA 454+27.23 TO STA 454+64.18

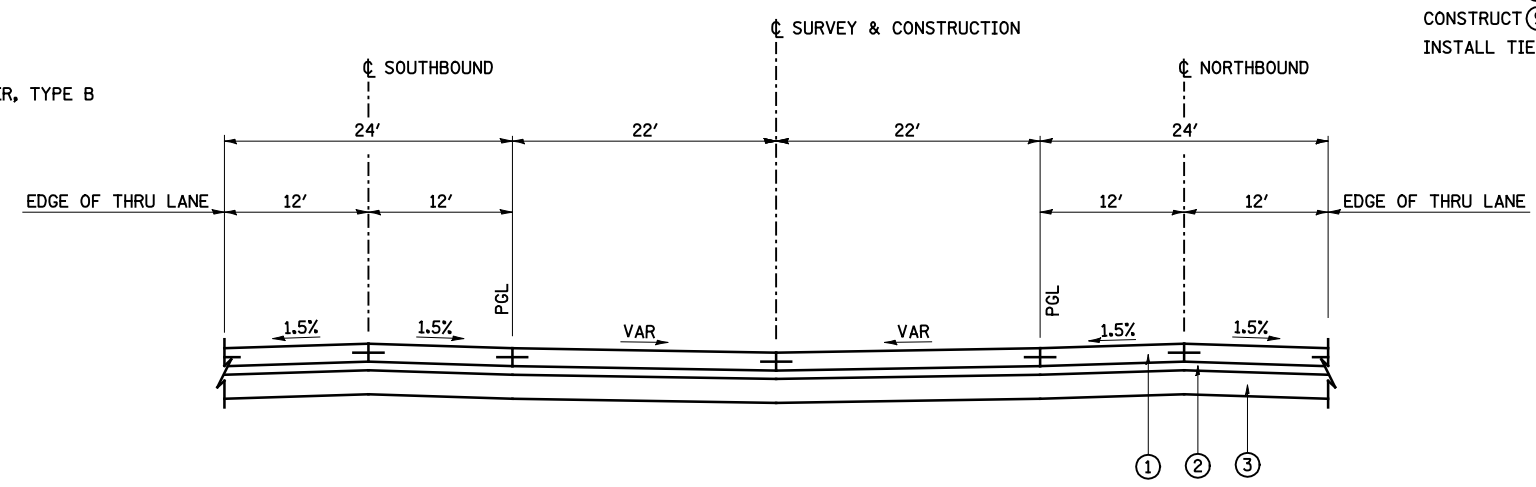


PIPE UNDERDRAIN DETAIL

* CONSTRUCT ② TO 18" BEYOND EDGE OF PAVEMENT
 CONSTRUCT ① WITHOUT TIE BARS
 CONSTRUCT ⑨ AND COMPLETE CONSTRUCTION OF ④ AND ⑩
 INSTALL TIE BARS AND COMPLETE ⑤ AND ⑥.

LEGEND

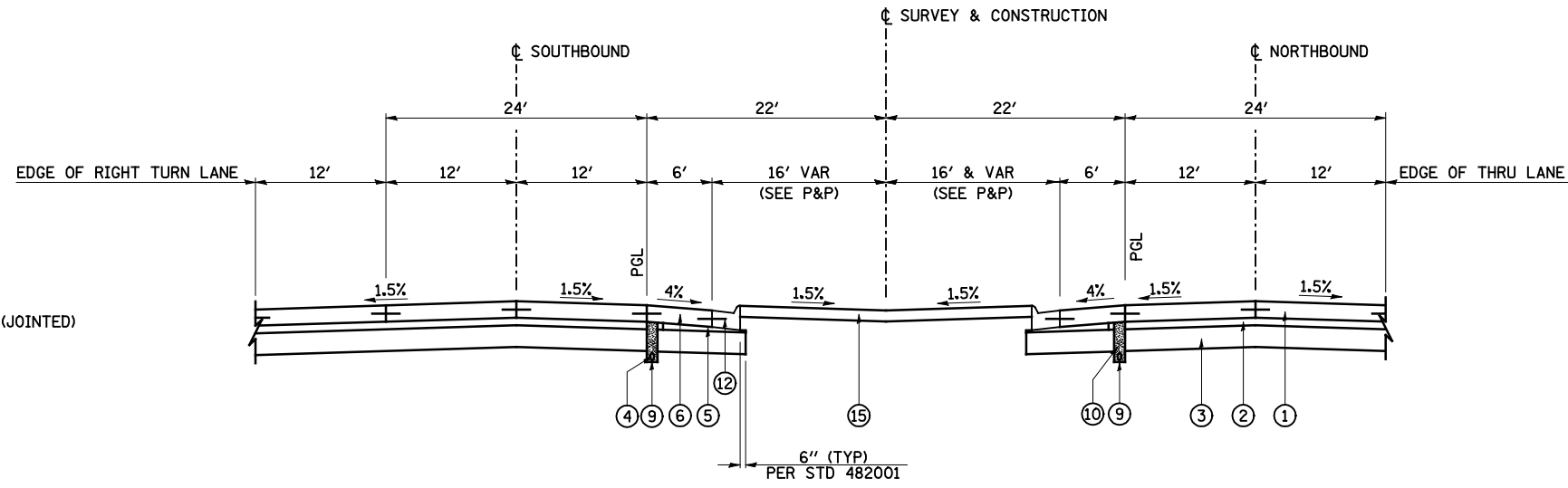
- ① PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
- ② STABILIZED SUBBASE 4"
- ③ LIME MODIFIED SOIL (12" MIN)
- ④ PIPE UNDERDRAINS 4"
- ⑤ COMPACTED SUBGRADE (EMBANKMENT)
- ⑥ PORTLAND CEMENT CONCRETE SHOULDERS, 9 1/4"
- ⑦ AGGREGATE SHOULDERS, TYPE B (9 1/4")
- ⑧ CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- ⑨ UNDERDRAIN TRENCH (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- ⑩ FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑪ PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- ⑫ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- ⑬ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- ⑭ TOPSOIL FURNISH AND PLACE, 4"
- ⑮ CONCRETE MEDIAN SURFACE, 6 INCH
- ⑯ CONCRETE MEDIAN, TYPE SM (SPECIAL)
- ⑰ CONCRETE GUTTER, TYPE A (SPECIAL)
- ⑱ AGGREGATE WEDGE SHOULDER, TYPE B (9 1/4") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B



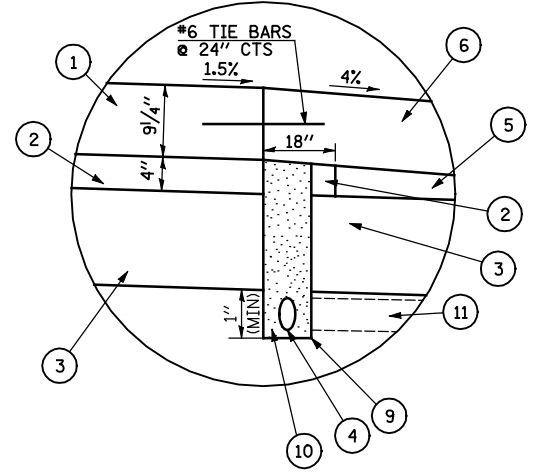
US 67 SECTION NO. 16
 STA 454+64.18 TO STA 454+95.74

- NOTES:**
1. VARIES FROM 14' @ STA 445+72.65 TO 8' @ STA 446+78.20
 2. VARIES FROM 4' @ STA 445+72.65 TO 10' @ STA 446+78.20

| | | | | | | | | | | |
|---|-------------------|----------------|---------------------------|---|---|-----------------------------|-----------|----------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - FAP ROUTE 310 (US 67) (ALT. A) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| vt:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-US67typical18.dgn | DRAWN - TJD | REVISIED - | 407 | | | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 46 | |
| PLOT SCALE = 16.000' / IN. | CHECKED - JRB | REVISIED - | CONTRACT NO. 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISIED - | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | SCALE: N/A | SHEET NO. 7 OF 8 SHEETS | | STA. N/A | TO STA. N/A | |



US 77 SECTION NO. 17
 STA 454+95.74 TO STA 455+45.16

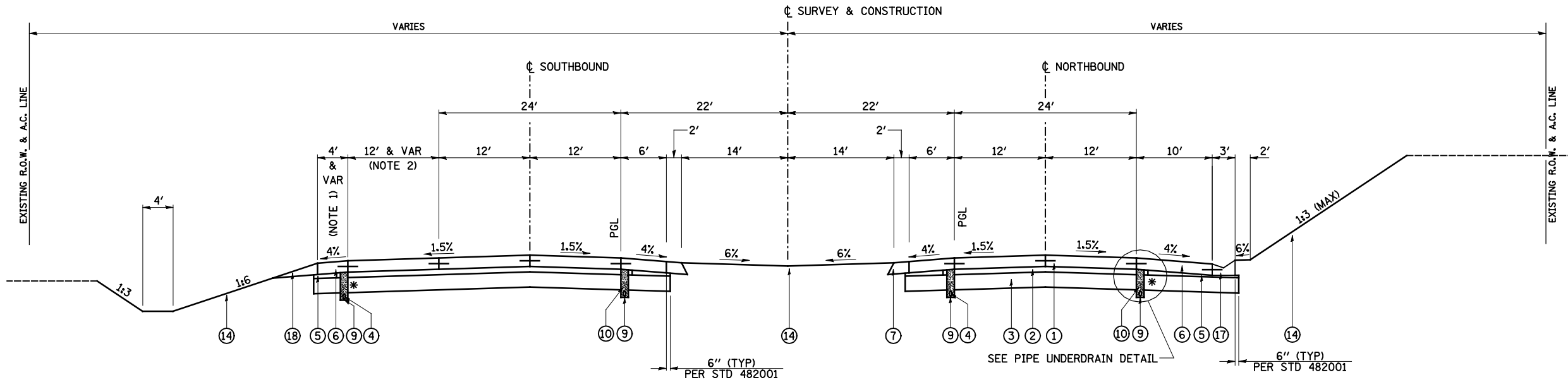


PIPE UNDERDRAIN DETAIL

* CONSTRUCT (2) TO 18" BEYOND EDGE OF PAVEMENT
 CONSTRUCT (1) WITHOUT TIE BARS
 CONSTRUCT (9) AND COMPLETE CONSTRUCTION OF (4) AND (10)
 INSTALL TIE BARS AND COMPLETE (5) AND (6).

LEGEND

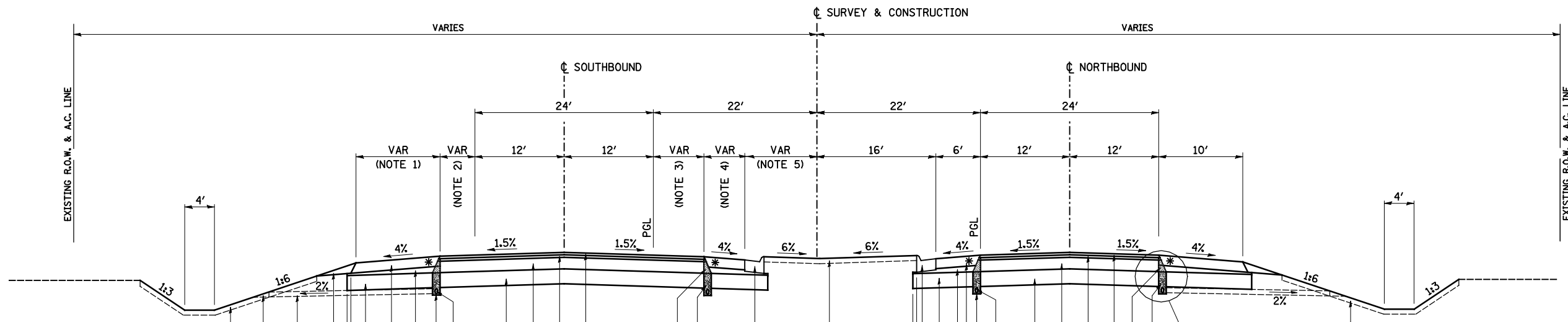
- (1) PORTLAND CEMENT CONCRETE PAVEMENT 9/4" (JOINTED)
- (2) STABILIZED SUBBASE 4"
- (3) LIME MODIFIED SOIL (12" MIN)
- (4) PIPE UNDERDRAINS 4"
- (5) COMPACTED SUBGRADE (EMBANKMENT)
- (6) PORTLAND CEMENT CONCRETE SHOULDERS, 9/4"
- (7) AGGREGATE SHOULDERS, TYPE B (9/4")
- (8) CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- (9) UNDERDRAIN TRENCH (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- (10) FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- (11) PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- (12) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- (13) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- (14) TOPSOIL FURNISH AND PLACE, 4"
- (15) CONCRETE MEDIAN SURFACE, 6 INCH
- (16) CONCRETE MEDIAN, TYPE SM (SPECIAL)
- (17) CONCRETE GUTTER, TYPE A (SPECIAL)
- (18) AGGREGATE WEDGE SHOULDER, TYPE B (9/4") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B



US 77 SECTION NO. 18
 STA 455+45.16 TO STA 461+50.00

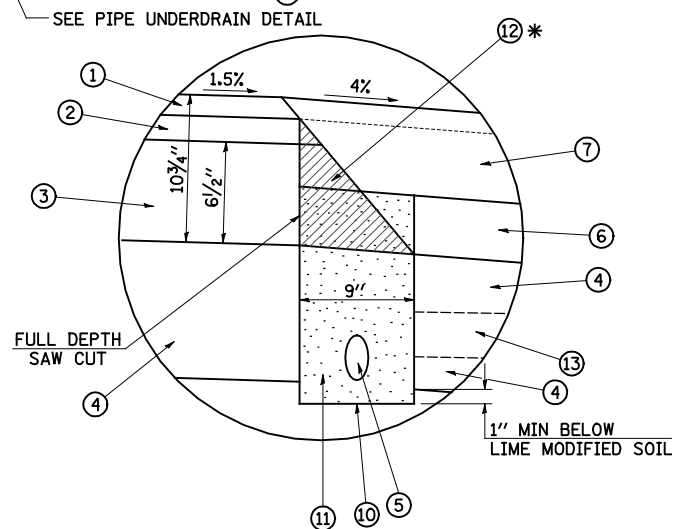
- NOTES:**
- VARIES FROM 4' @ STA 459+77.99 TO 10' @ STA 461+50
 - VARIES FROM 12' @ STA 458+43.86 TO 0' @ STA 461+50

| | | | | | | | | | | |
|--|-------------------|----------------|---------------------------|---|---|-----------------------------|-----------|--------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - FAP ROUTE 310 (US 77) (ALT. A) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-US67typical19.dgn | DRAWN - TJD | REVISED - | 407 | | | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 47 | |
| PLOT SCALE = 16.000' / IN. | CHECKED - JRB | REVISED - | CONTRACT NO. 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |



US 67 SECTION NO. 3
STA 438+46.31 TO STA 439+90.70

| FLEXIBLE PAVEMENT DESIGN INFORMATION | | |
|--|-------------------|----------|
| STRUCTURAL DESIGN TRAFFIC: 10,475 | YEAR: 2032 | |
| PV = 9,218 | SU = 314 | MU = 943 |
| ROAD/STREET CLASSIFICATION: RURAL | CLASS: 1 | |
| PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE: | | |
| PV = 32% | SU = 45% | MU = 45% |
| FLEXIBLE TRAFFIC FACTOR: ACTUAL TF = 4.48 | AC TYPE = 20 | |
| | MINIMUM TF = 3.56 | |
| PG GRADE: BINDER = 64-22 | SURFACE = 64-22 | |
| SUBGRADE SUPPORTING RATING: | | |
| SSR = POOR | | |



PIPE UNDERDRAIN DETAIL

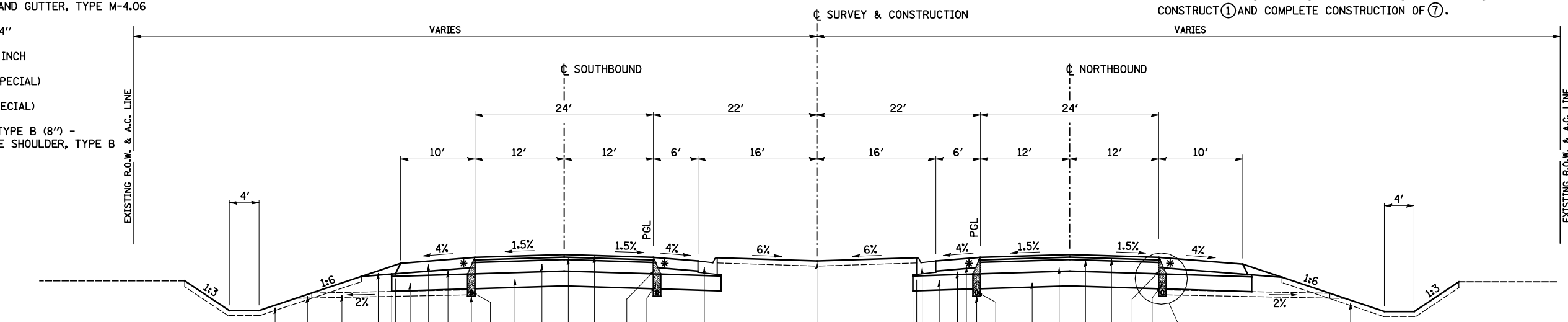
* CONSTRUCT (2) AND (3). SAWCUT AS SHOWN IN DETAIL. THEN CONSTRUCT (6). CONSTRUCT (5), (10), AND (11). CONSTRUCT (7) TO TOP OF (2) PRIOR TO PLACING (1). CONSTRUCT (1) AND COMPLETE CONSTRUCTION OF (7).

LEGEND

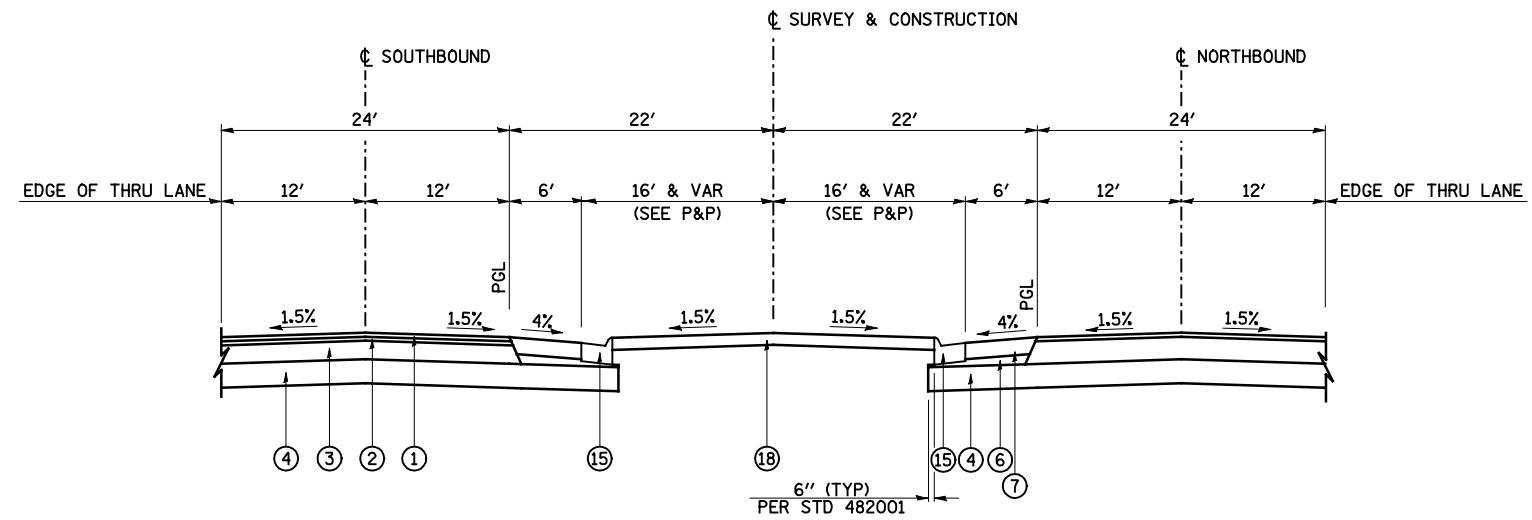
- ① POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
- ② POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 2 1/4"
- ③ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 7"
- ④ LIME MODIFIED SOIL (12" MIN)
- ⑤ PIPE UNDERDRAINS 4"
- ⑥ COMPACTED SUBGRADE (EMBANKMENT)
- ⑦ HOT-MIX ASPHALT SHOULDERS, 8"
- ⑧ AGGREGATE SHOULDERS, TYPE B (8")
- ⑨ CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- ⑩ 9" TRENCH FOR PIPE UNDERDRAINS (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- ⑪ FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑫ HOT-MIX ASPHALT WEDGE TO BE REMOVED PRIOR TO TRENCHING (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑬ PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- ⑭ GUARDRAIL AGGREGATE EROSION CONTROL
- ⑮ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- ⑯ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- ⑰ TOPSOIL FURNISH AND PLACE, 4"
- ⑱ CONCRETE MEDIAN SURFACE, 6 INCH
- ⑲ CONCRETE MEDIAN, TYPE SM (SPECIAL)
- ⑳ CONCRETE GUTTER, TYPE A (SPECIAL)
- ㉑ AGGREGATE WEDGE SHOULDER, TYPE B (8") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B

NOTES:

- 1. VARIES FROM 8' @ STA 438+46.31 TO 10' @ STA. 439+90.70
- 2. VARIES FROM 10.1' @ STA. 438+46.31 TO 0' @ STA. 439+90.70
- 3. VARIES FROM 7.2' @ STA. 438+46.31 TO 0' @ STA. 439+90.70
- 4. VARIES FROM 5.5' @ STA. 438+46.31 TO 6' @ STA. 439+90.70
- 5. VARIES FROM 9.7' @ STA. 438+46.31 TO 16' @ STA. 439+90.70

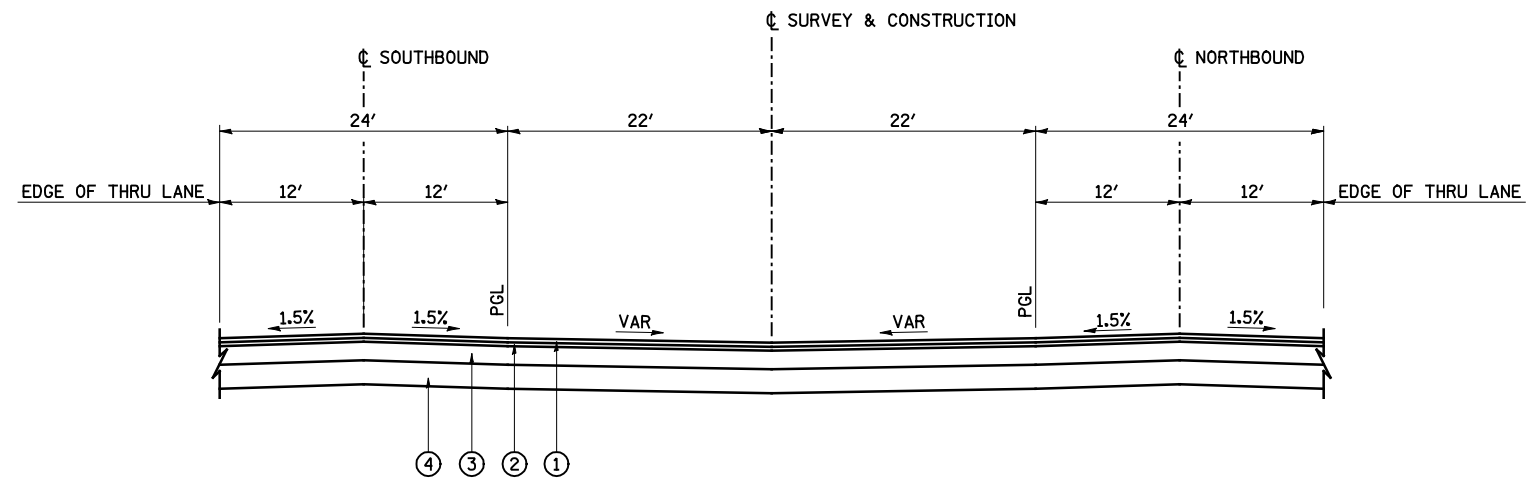


US 67 SECTION NO. 4
STA 439+90.70 TO STA 444+54.79



US 67 SECTION NO. 5

STA 444+54.79 TO STA 445+04.26



US 67 SECTION NO. 6

STA 445+04.26 TO STA 445+35.93

LEGEND

- ① POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
- ② POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 2 1/4"
- ③ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 7"
- ④ LIME MODIFIED SOIL (12" MIN)
- ⑤ PIPE UNDERDRAINS 4"
- ⑥ COMPACTED SUBGRADE (EMBANKMENT)
- ⑦ HOT-MIX ASPHALT SHOULDERS, 8"
- ⑧ AGGREGATE SHOULDERS, TYPE B (8")
- ⑨ CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- ⑩ 9" TRENCH FOR PIPE UNDERDRAINS (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- ⑪ FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑫ HOT-MIX ASPHALT WEDGE TO BE REMOVED PRIOR TO TRENCHING (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑬ PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- ⑭ GUARDRAIL AGGREGATE EROSION CONTROL
- ⑮ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- ⑯ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- ⑰ TOPSOIL FURNISH AND PLACE, 4"
- ⑱ CONCRETE MEDIAN SURFACE, 6 INCH
- ⑲ CONCRETE MEDIAN, TYPE SM (SPECIAL)
- ⑳ CONCRETE GUTTER, TYPE A (SPECIAL)
- ㉑ AGGREGATE WEDGE SHOULDER, TYPE B (8") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B

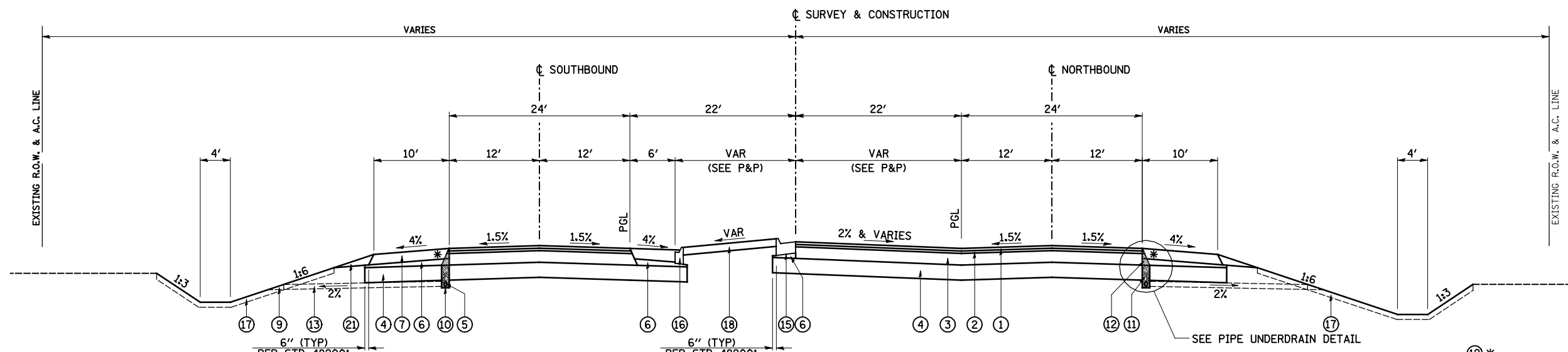
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|--|-------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-sht-US67typical05.dgn | | DRAWN - TJD | REVISED - |
| | | CHECKED - JRB | REVISED - |
| | | DATE - 1-30-2015 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

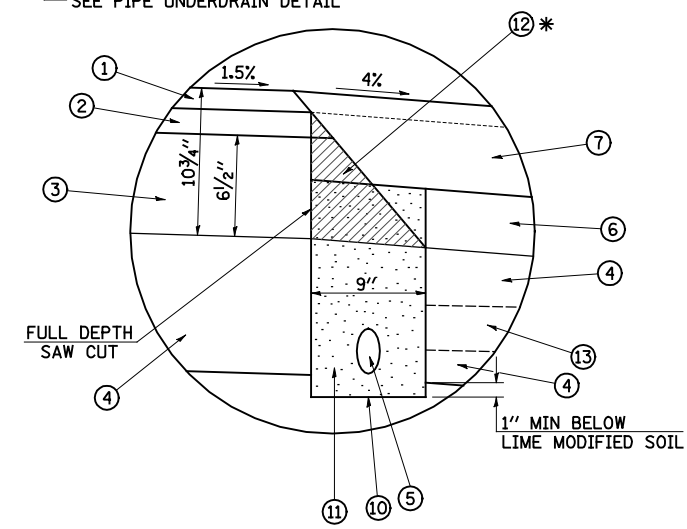
**FAP ROUTE 407 (IL 336 /IL 110)
TYPICAL SECTIONS - FAP ROUTE 310 (US 67) (ALT. B)**

SCALE: N/A SHEET NO. 2 OF 8 SHEETS STA. N/A TO STA. N/A

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------------------------|-----------|--------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 49 |
| CONTRACT NO. 68B44 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

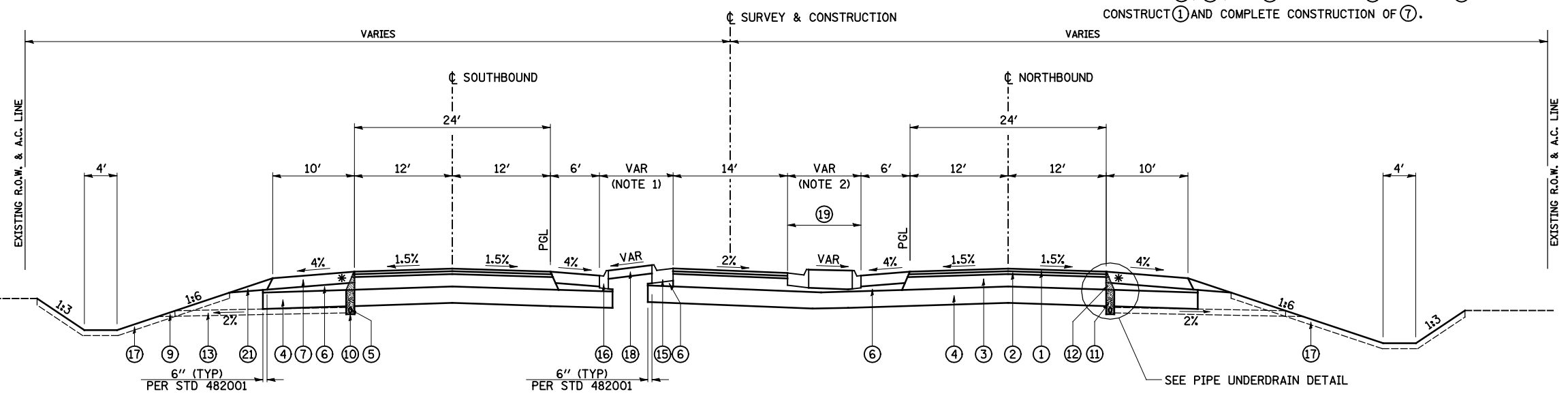


US 67 SECTION NO. 7
 STA 445+35.93 TO STA 445+72.65



PIPE UNDERDRAIN DETAIL

* CONSTRUCT (2) AND (3). SAWCUT AS SHOWN IN DETAIL. THEN CONSTRUCT (6). CONSTRUCT (5), (10), AND (11). CONSTRUCT (7) TO TOP OF (2) PRIOR TO PLACING (1). CONSTRUCT (1) AND COMPLETE CONSTRUCTION OF (7).



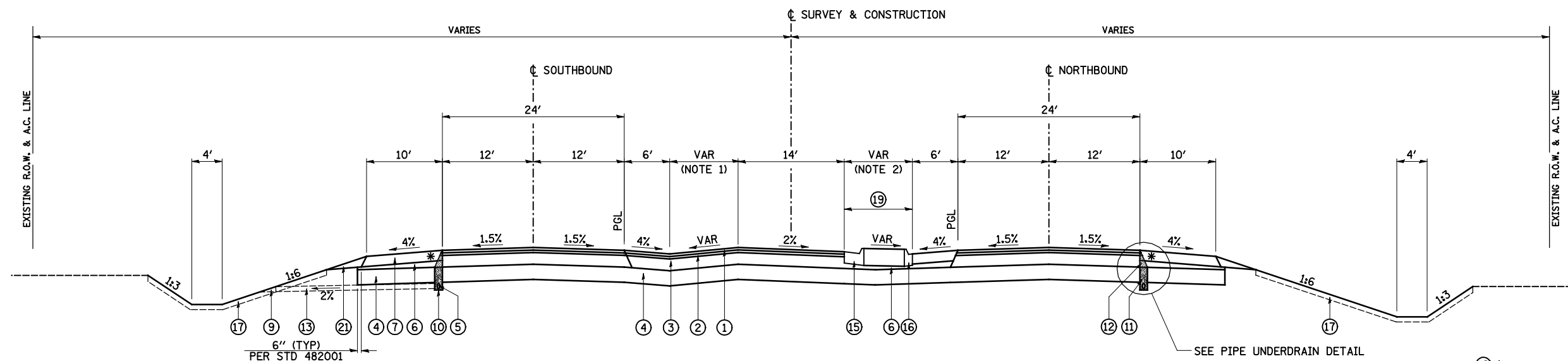
US 67 SECTION NO. 8
 STA 445+72.65 TO STA 446+78.12

LEGEND

- ① POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
- ② POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 2 1/4"
- ③ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 7"
- ④ LIME MODIFIED SOIL (12" MIN)
- ⑤ PIPE UNDERDRAINS 4"
- ⑥ COMPACTED SUBGRADE (EMBANKMENT)
- ⑦ HOT-MIX ASPHALT SHOULDERS, 8"
- ⑧ AGGREGATE SHOULDERS, TYPE B (8")
- ⑨ CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- ⑩ 9" TRENCH FOR PIPE UNDERDRAINS (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- ⑪ FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑫ HOT-MIX ASPHALT WEDGE TO BE REMOVED PRIOR TO TRENCHING (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑬ PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- ⑭ GUARDRAIL AGGREGATE EROSION CONTROL
- ⑮ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- ⑯ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- ⑰ TOPSOIL FURNISH AND PLACE, 4"
- ⑱ CONCRETE MEDIAN SURFACE, 6 INCH
- ⑲ CONCRETE MEDIAN, TYPE SM (SPECIAL)
- ⑳ CONCRETE GUTTER, TYPE A (SPECIAL)
- ㉑ AGGREGATE WEDGE SHOULDER, TYPE B (8") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B

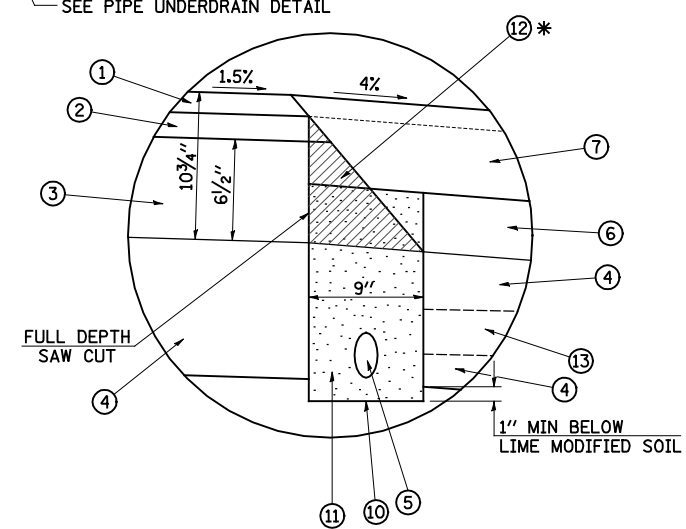
- NOTES:**
- VARIES FROM 14' @ STA 445+72.65 TO 8' @ STA 446+78.20
 - VARIES FROM 4' @ STA 445+72.65 TO 10' @ STA 446+78.20

| | | | | | | | | | | |
|---|----------------------------|----------------|-----------|---|---|-------------|-----------------------------|--------------------|--------------|-----------|
| FILE NAME = v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-sht-US67typical06.dgn | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - FAP ROUTE 310 (US 67) (ALT. B) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 16.000' / IN. | CHECKED - JRB | REVISED - | | | 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 50 |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - | REVISED - | SCALE: N/A | SHEET NO. 3 OF 8 SHEETS | STA. N/A | TO STA. N/A | CONTRACT NO. 68B44 | | |
| ILLINOIS FED. AID PROJECT | | | | | | | | | | |



US 67 SECTION NO. 9

STA 446+78.12 TO STA 447+13.89



PIPE UNDERDRAIN DETAIL

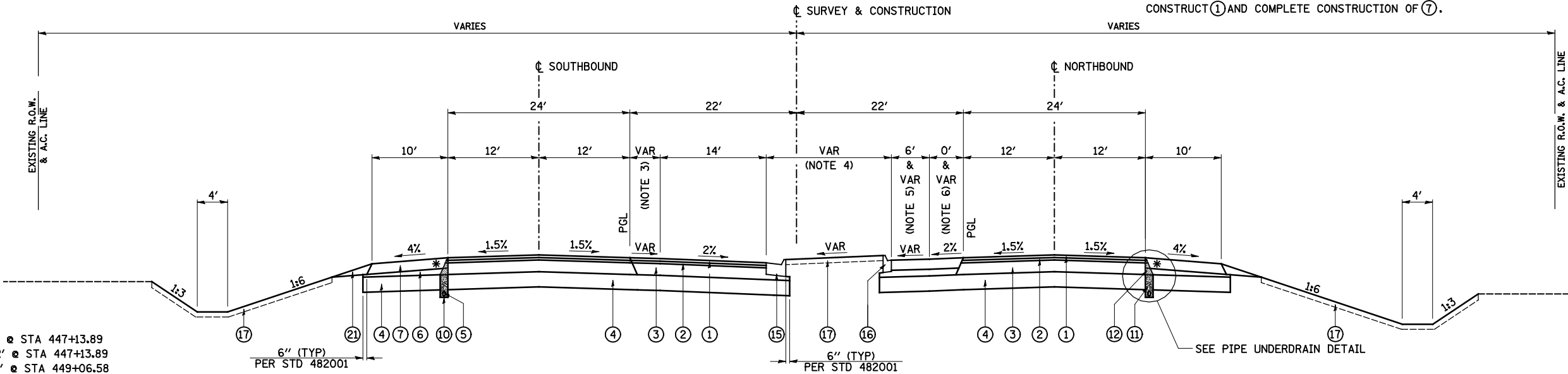
* CONSTRUCT (2) AND (3). SAWCUT AS SHOWN IN DETAIL. THEN CONSTRUCT (6). CONSTRUCT (5), (10), AND (11). CONSTRUCT (7) TO TOP OF (2) PRIOR TO PLACING (1). CONSTRUCT (1) AND COMPLETE CONSTRUCTION OF (7).

LEGEND

- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 2 1/4"
- (3) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 7"
- (4) LIME MODIFIED SOIL (12" MIN)
- (5) PIPE UNDERDRAINS 4"
- (6) COMPACTED SUBGRADE (EMBANKMENT)
- (7) HOT-MIX ASPHALT SHOULDERS, 8"
- (8) AGGREGATE SHOULDERS, TYPE B (8")
- (9) CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- (10) 9" TRENCH FOR PIPE UNDERDRAINS (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- (11) FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- (12) HOT-MIX ASPHALT WEDGE TO BE REMOVED PRIOR TO TRENCHING (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- (13) PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- (14) GUARDRAIL AGGREGATE EROSION CONTROL
- (15) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- (16) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- (17) TOPSOIL FURNISH AND PLACE, 4"
- (18) CONCRETE MEDIAN SURFACE, 6 INCH
- (19) CONCRETE MEDIAN, TYPE SM (SPECIAL)
- (20) CONCRETE GUTTER, TYPE A (SPECIAL)
- (21) AGGREGATE WEDGE SHOULDER, TYPE B (8") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B

NOTES:

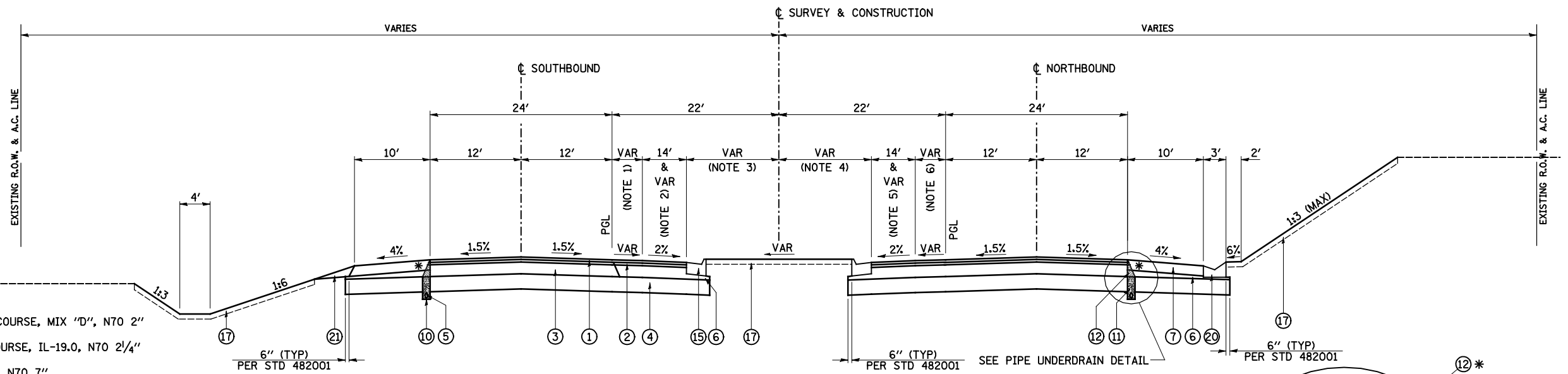
- 1. VARIES FROM 8' @ STA 446+78.20 TO 12' @ STA 447+13.89
- 2. VARIES FROM 10' @ STA 446+78.20 TO 12' @ STA 447+13.89
- 3. VARIES FROM 12' @ STA 447+13.89 TO 1.1' @ STA 449+06.58
- 4. VARIES FROM 12' @ STA 447+13.89 TO 22.9' @ STA 449+06.58
- 5. VARIES FROM 5' @ STA 448+44.88 TO 1.5' @ STA 449+06.58
- 6. VARIES FROM 1' @ STA 448+44.88 TO 4.5' @ STA 449+06.58



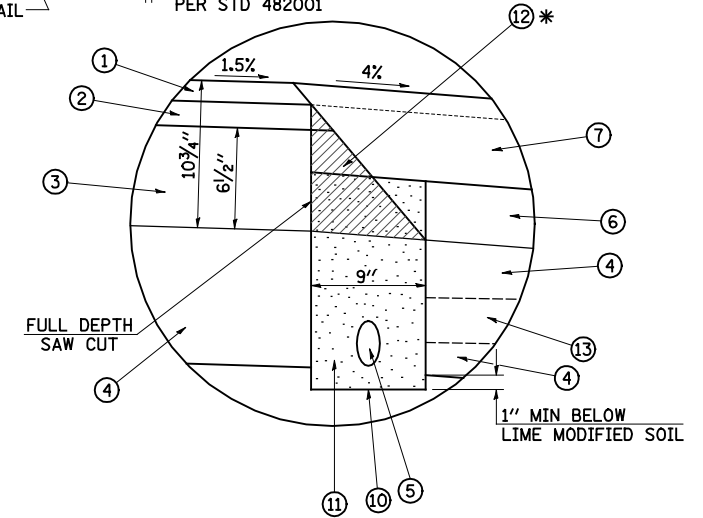
US 67 SECTION NO. 10

STA 447+13.89 TO STA 449+06.58

| | | | | | | | | | | |
|--|-------------------|----------------|---------------------------|---|---|-----------------------------|-----------|--------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - FAP ROUTE 310 (US 67) (ALT. B) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-US67typical07.dgn | DRAWN - TJD | REVISED - | 407 | | | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 51 | |
| PLOT SCALE = 16.000' / IN. | CHECKED - JRB | REVISED - | CONTRACT NO. 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |



US 67 SECTION NO. 11
 STA 449+06.58 TO STA 450+93.40



PIPE UNDERDRAIN DETAIL

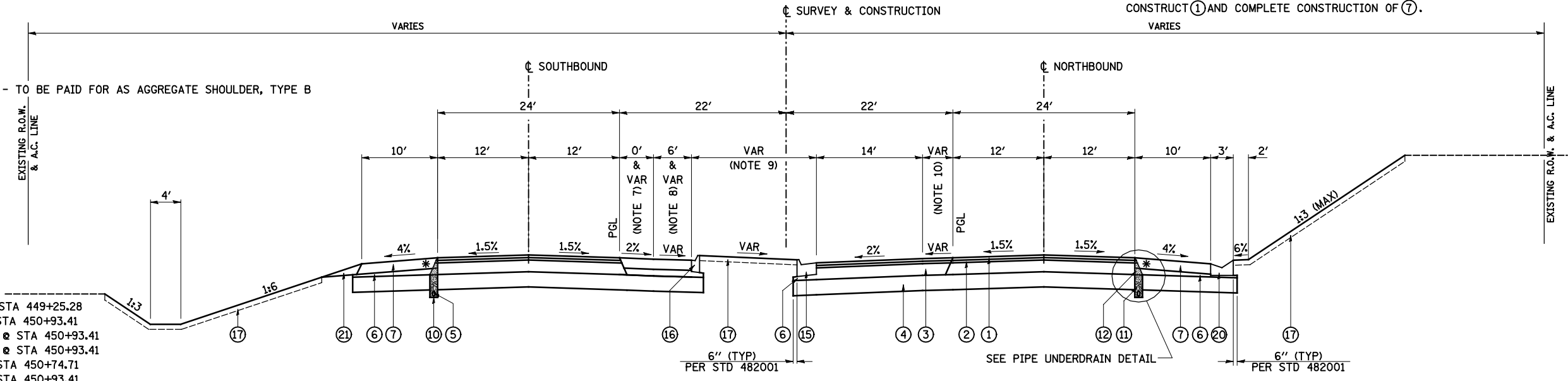
* CONSTRUCT (2) AND (3). SAWCUT AS SHOWN IN DETAIL. THEN CONSTRUCT (6). CONSTRUCT (5), (10), AND (11). CONSTRUCT (7) TO TOP OF (2) PRIOR TO PLACING (1). CONSTRUCT (1) AND COMPLETE CONSTRUCTION OF (7).

LEGEND

- ① POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
- ② POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 2 1/4"
- ③ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 7"
- ④ LIME MODIFIED SOIL (12" MIN)
- ⑤ PIPE UNDERDRAINS 4"
- ⑥ COMPACTED SUBGRADE (EMBANKMENT)
- ⑦ HOT-MIX ASPHALT SHOULDERS, 8"
- ⑧ AGGREGATE SHOULDERS, TYPE B (8")
- ⑨ CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- ⑩ 9" TRENCH FOR PIPE UNDERDRAINS (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- ⑪ FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑫ HOT-MIX ASPHALT WEDGE TO BE REMOVED PRIOR TO TRENCHING (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑬ PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- ⑭ GUARDRAIL AGGREGATE EROSION CONTROL
- ⑮ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- ⑯ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- ⑰ TOPSOIL FURNISH AND PLACE, 4"
- ⑱ CONCRETE MEDIAN SURFACE, 6 INCH
- ⑲ CONCRETE MEDIAN, TYPE SM (SPECIAL)
- ⑳ CONCRETE GUTTER, TYPE A (SPECIAL)
- ㉑ AGGREGATE WEDGE SHOULDER, TYPE B (8") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B

NOTES:

- 1. VARIES FROM 1.1' @ STA 449+06.58 TO 0' @ STA 449+25.28
- 2. VARIES FROM 14' @ STA 449+25.28 TO 5' @ STA 450+93.41
- 3. VARIES FROM 6.9' @ STA 449+06.58 TO 17.5' @ STA 450+93.41
- 4. VARIES FROM 17.5' @ STA 449+06.58 TO 6.9' @ STA 450+93.41
- 5. VARIES FROM 5' @ STA 449+06.58 TO 14' @ STA 450+74.71
- 6. VARIES FROM 0' @ STA 450+74.71 TO 1.1' @ STA 450+93.41
- 7. VARIES FROM 5' @ STA 450+93.41 TO 0' @ STA 451+55.11
- 8. VARIES FROM 1.5' @ STA 450+93.41 TO 5' @ STA 451+55.11
- 9. VARIES FROM 24.4' @ STA 450+93.41 TO 12' @ STA 452+86.10
- 10. VARIES FROM 1.1' @ STA 450+93.41 TO 12' @ STA 452+86.10



US 67 SECTION NO. 12
 STA 450+93.40 TO STA 452+86.10

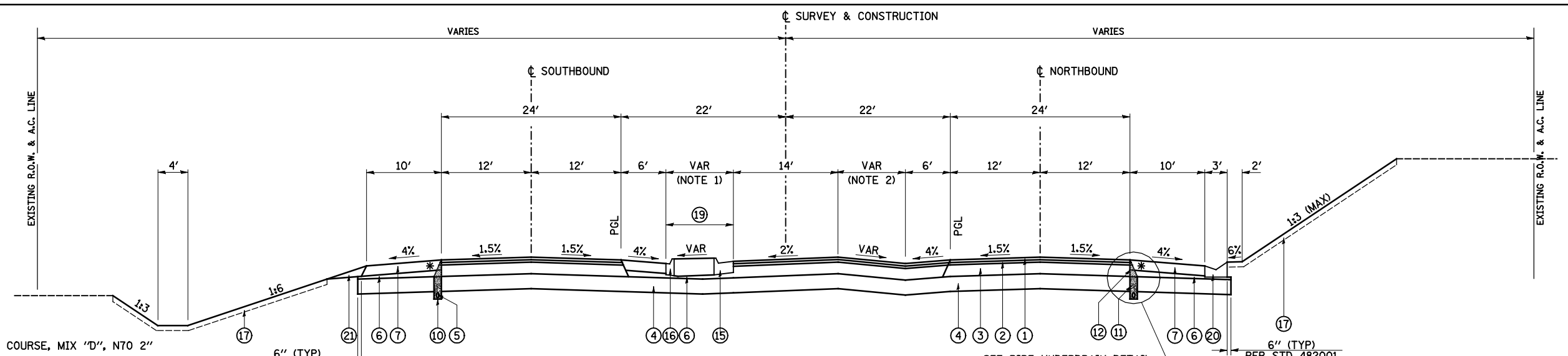
| | | | |
|--|-------------------|------------------|-----------|
| FILE NAME = v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-US67typcoa108.dgn | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| | | DRAWN - TJD | REVISED - |
| | | CHECKED - JRB | REVISED - |
| | | DATE - 1-30-2015 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

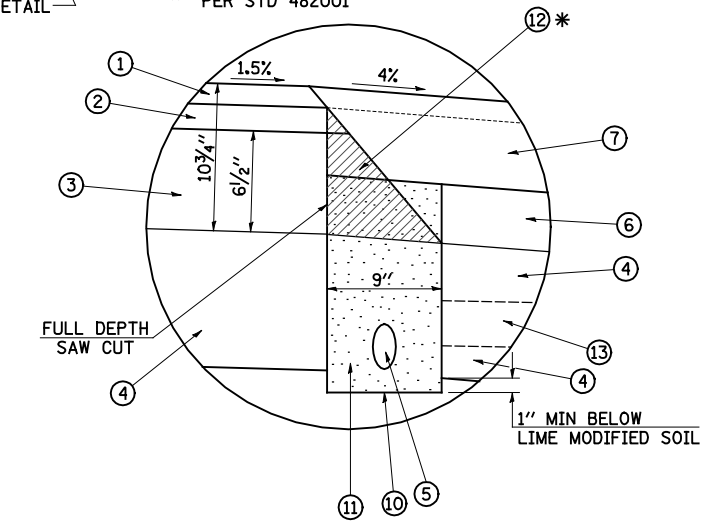
FAP ROUTE 407 (IL 336 /IL 110)
TYPICAL SECTIONS - FAP ROUTE 310 (US 67) (ALT. B)

SCALE: N/A SHEET NO. 5 OF 8 SHEETS STA. N/A TO STA. N/A

| | | | | |
|--------------------|-------------------------------------|------------------|---------------------------|--------------|
| F.A.P. RTE. 407 | SECTION 55C3(PV,HB(2-6);B,B-1,B-2)] | COUNTY McDONOUGH | TOTAL SHEETS 874 | SHEET NO. 52 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |



US 67 SECTION NO. 13
 STA 452+86.10 TO STA 453+21.79

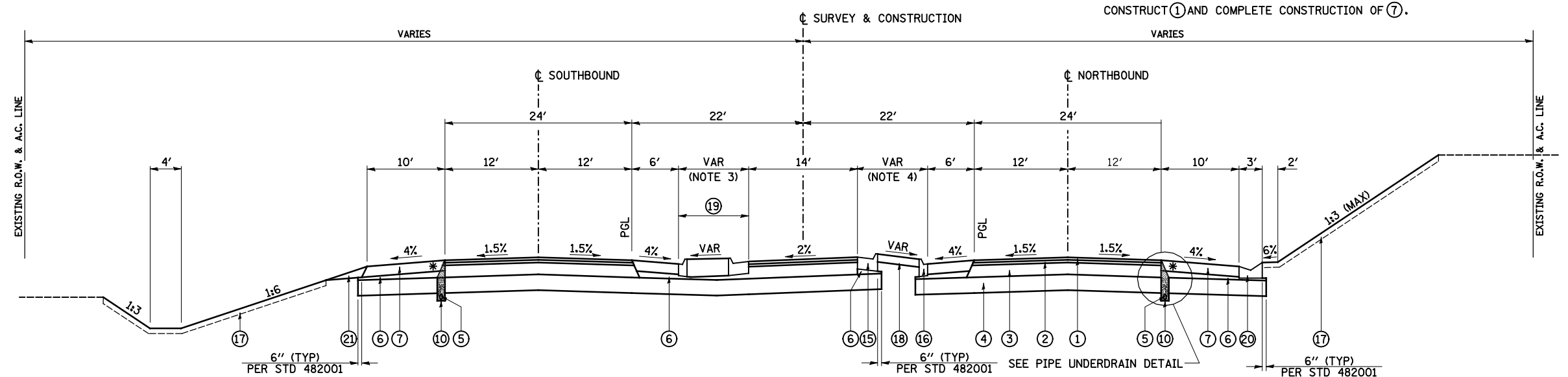


PIPE UNDERDRAIN DETAIL

* CONSTRUCT (2) AND (3). SAWCUT AS SHOWN IN DETAIL. THEN CONSTRUCT (6). CONSTRUCT (5), (10), AND (11). CONSTRUCT (7) TO TOP OF (2) PRIOR TO PLACING (1). CONSTRUCT (1) AND COMPLETE CONSTRUCTION OF (7).

LEGEND

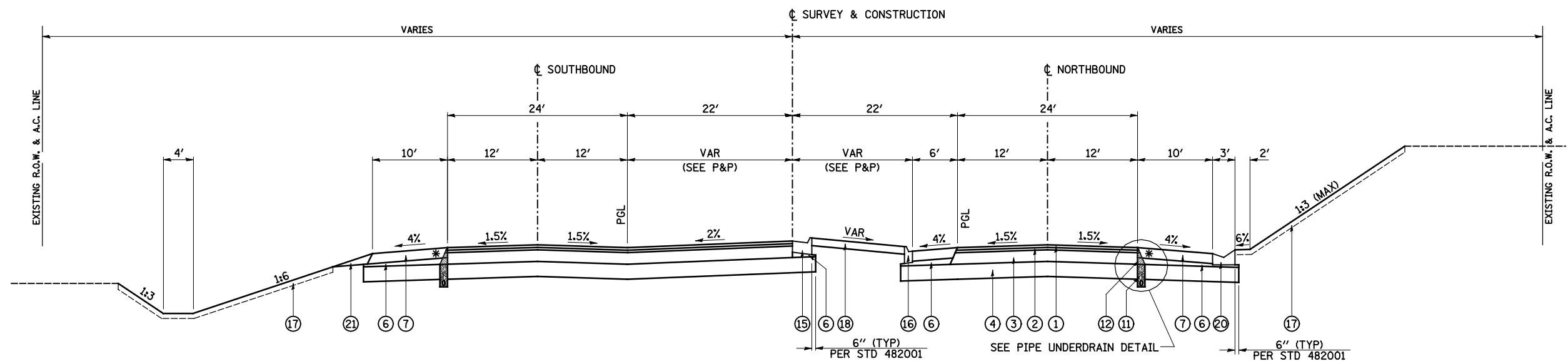
- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 2 1/4"
- (3) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 7"
- (4) LIME MODIFIED SOIL (12" MIN)
- (5) PIPE UNDERDRAINS 4"
- (6) COMPACTED SUBGRADE (EMBANKMENT)
- (7) HOT-MIX ASPHALT SHOULDERS, 8"
- (8) AGGREGATE SHOULDERS, TYPE B (8")
- (9) CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- (10) 9" TRENCH FOR PIPE UNDERDRAINS (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- (11) FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- (12) HOT-MIX ASPHALT WEDGE TO BE REMOVED PRIOR TO TRENCHING (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- (13) PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- (14) GUARDRAIL AGGREGATE EROSION CONTROL
- (15) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- (16) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- (17) TOPSOIL FURNISH AND PLACE, 4"
- (18) CONCRETE MEDIAN SURFACE, 6 INCH
- (19) CONCRETE MEDIAN, TYPE SM (SPECIAL)
- (20) CONCRETE GUTTER, TYPE A (SPECIAL)
- (21) AGGREGATE WEDGE SHOULDER, TYPE B (8") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B



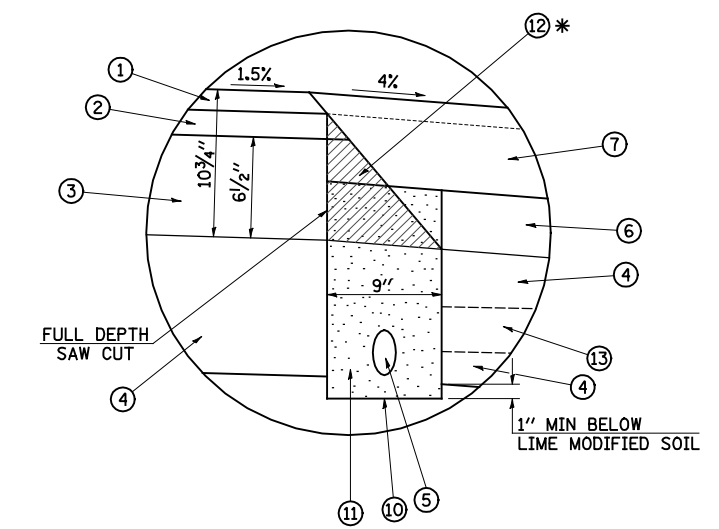
US 67 SECTION NO. 14
 STA 453+21.79 TO STA 454+27.23

- NOTES:**
- VARIABLES FROM 12' @ STA 452+86.10 TO 10' @ STA 453+21.79
 - VARIABLES FROM 12' @ STA 452+86.10 TO 8' @ STA 453+21.79
 - VARIABLES FROM 10' @ STA 453+21.79 TO 4' @ STA 454+27.23
 - VARIABLES FROM 8' @ STA 453+21.79 TO 14' @ STA 454+27.23

| | | | | | | | | | | |
|--|-------------------|----------------|---------------------------|---|---|-----------------------------|-----------|--------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - FAP ROUTE 310 (US 67) (ALT. B) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-US67typical10.dgn | DRAWN - TJD | REVISED - | 407 | | | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 53 | |
| PLOT SCALE = 16.000' / IN. | CHECKED - JRB | REVISED - | CONTRACT NO. 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |



US 67 SECTION NO. 15
STA 454+27.23 TO STA 454+64.18

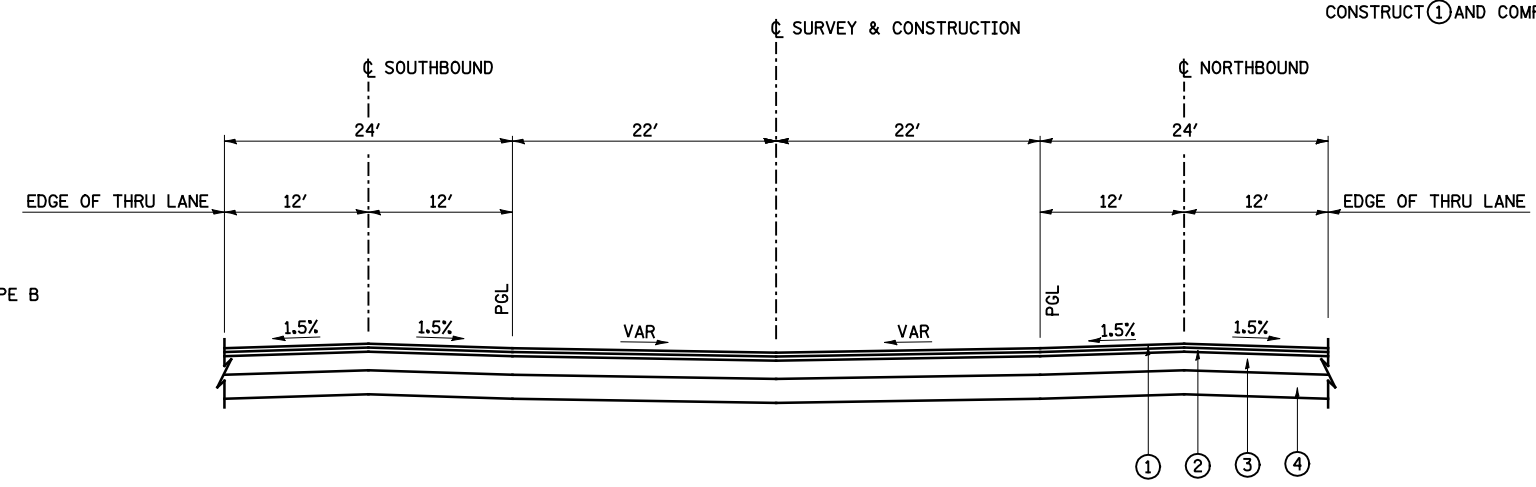


PIPE UNDERDRAIN DETAIL

* CONSTRUCT (2) AND (3). SAWCUT AS SHOWN IN DETAIL. THEN CONSTRUCT (6). CONSTRUCT (5), (10), AND (11). CONSTRUCT (7) TO TOP OF (2) PRIOR TO PLACING (1). CONSTRUCT (1) AND COMPLETE CONSTRUCTION OF (7).

LEGEND

- ① POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
- ② POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 2 1/4"
- ③ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 7"
- ④ LIME MODIFIED SOIL (12" MIN)
- ⑤ PIPE UNDERDRAINS 4"
- ⑥ COMPACTED SUBGRADE (EMBANKMENT)
- ⑦ HOT-MIX ASPHALT SHOULDERS, 8"
- ⑧ AGGREGATE SHOULDERS, TYPE B (8")
- ⑨ CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
- ⑩ 9" TRENCH FOR PIPE UNDERDRAINS (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
- ⑪ FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑫ HOT-MIX ASPHALT WEDGE TO BE REMOVED PRIOR TO TRENCHING (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
- ⑬ PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
- ⑭ GUARDRAIL AGGREGATE EROSION CONTROL
- ⑮ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- ⑯ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- ⑰ TOPSOIL FURNISH AND PLACE, 4"
- ⑱ CONCRETE MEDIAN SURFACE, 6 INCH
- ⑲ CONCRETE MEDIAN, TYPE SM (SPECIAL)
- ⑳ CONCRETE GUTTER, TYPE A (SPECIAL)
- ㉑ AGGREGATE WEDGE SHOULDER, TYPE B (8") - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B

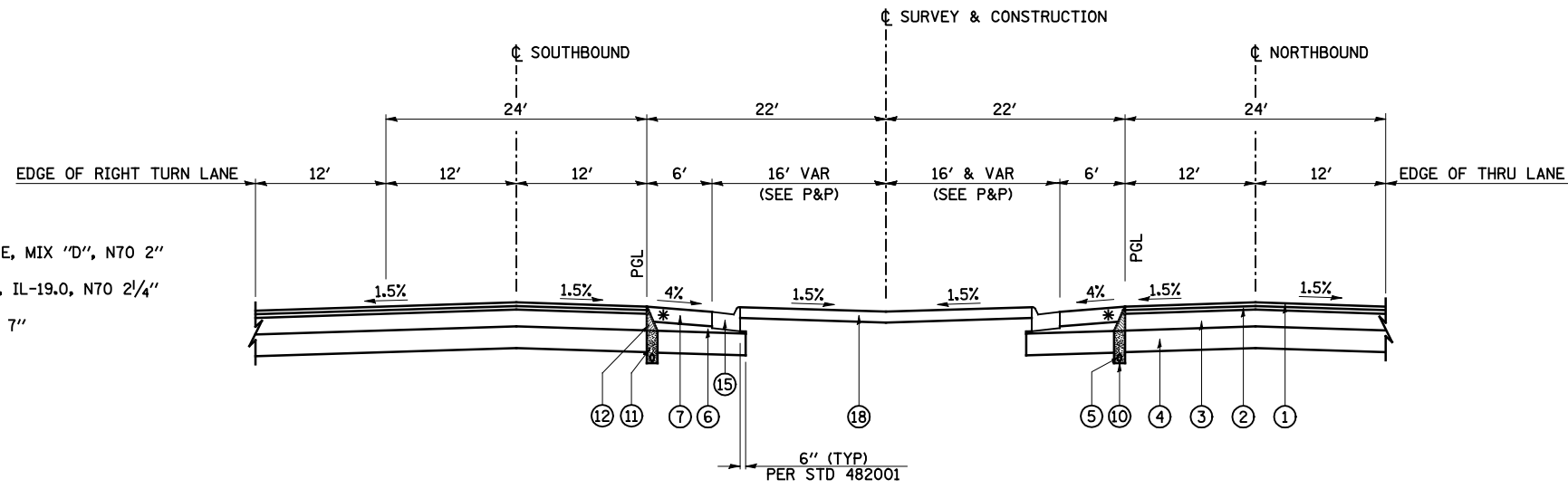


US 67 SECTION NO. 16
STA 454+64.18 TO STA 454+95.74

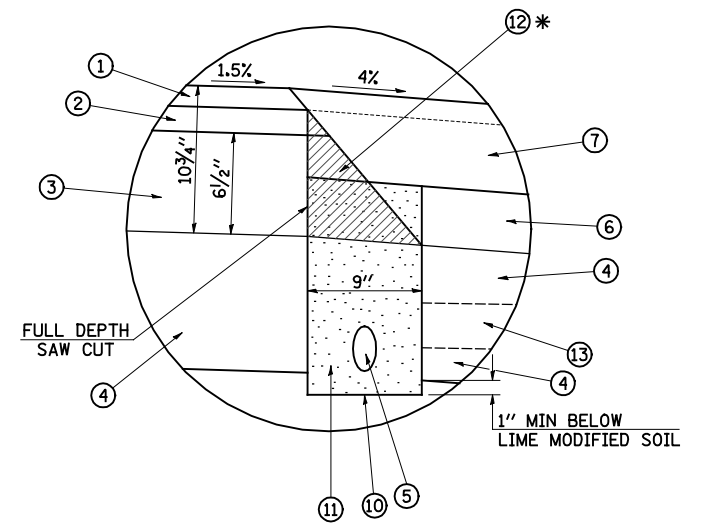
- NOTES:
1. VARIES FROM 14' @ STA 445+72.65 TO 8' @ STA 446+78.20
 2. VARIES FROM 4' @ STA 445+72.65 TO 10' @ STA 446+78.20

| | | | | | | | | | | |
|---|-------------------|----------------|---------------------------|---|---|-----------------------------|-----------|--------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - FAP ROUTE 310 (US 67) (ALT. B) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| vt:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-US67typ10a110.dgn | DRAWN - TJD | REVISIONS - | 407 | | | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 54 | |
| PLOT SCALE = 16.000' / IN. | CHECKED - JRB | REVISIONS - | CONTRACT NO. 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISIONS - | ILLINOIS FED. AID PROJECT | | | | | | | |

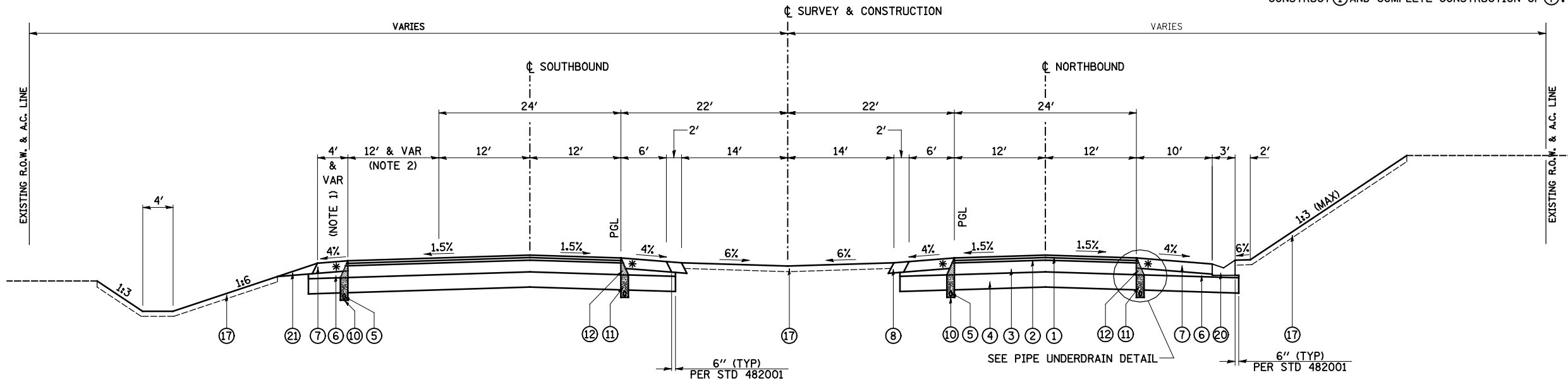
- LEGEND**
- ① POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
 - ② POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 2 1/4"
 - ③ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 7"
 - ④ LIME MODIFIED SOIL (12" MIN)
 - ⑤ PIPE UNDERDRAINS 4"
 - ⑥ COMPACTED SUBGRADE (EMBANKMENT)
 - ⑦ HOT-MIX ASPHALT SHOULDERS, 8"
 - ⑧ AGGREGATE SHOULDERS, TYPE B (8')
 - ⑨ CONCRETE HEADWALL FOR PIPE DRAINS (PER STD 601101)
 - ⑩ 9" TRENCH FOR PIPE UNDERDRAINS (PER STD 601001) TO BE INSTALLED WITH LASER-GUIDED TRENCHER
 - ⑪ FA-4 OR FM-4 SAND (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
 - ⑫ HOT-MIX ASPHALT WEDGE TO BE REMOVED PRIOR TO TRENCHING (INCLUDED IN COST OF PIPE UNDERDRAINS 4")
 - ⑬ PIPE UNDERDRAINS 4" (SPECIAL) (PER ARTICLE 601.04) AT 2% SLOPE
 - ⑭ GUARDRAIL AGGREGATE EROSION CONTROL
 - ⑮ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
 - ⑯ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
 - ⑰ TOPSOIL FURNISH AND PLACE, 4"
 - ⑱ CONCRETE MEDIAN SURFACE, 6 INCH
 - ⑲ CONCRETE MEDIAN, TYPE SM (SPECIAL)
 - ⑳ CONCRETE GUTTER, TYPE A (SPECIAL)
 - ㉑ AGGREGATE WEDGE SHOULDER, TYPE B (8') - TO BE PAID FOR AS AGGREGATE SHOULDER, TYPE B



US 67 SECTION NO. 17
STA 454+95.74 TO STA 455+45.16



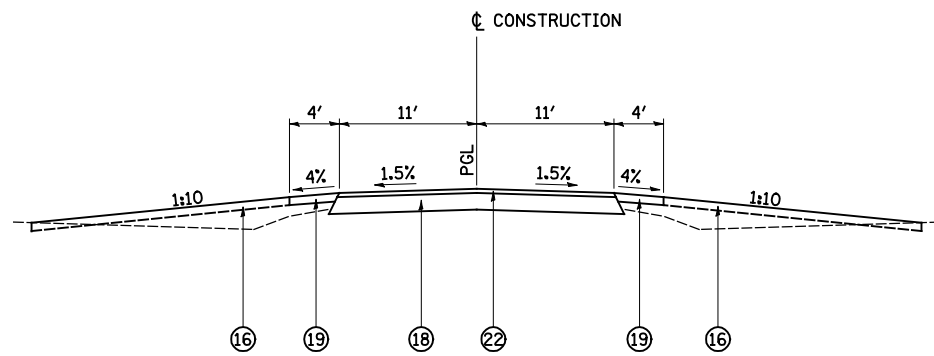
PIPE UNDERDRAIN DETAIL
* CONSTRUCT ② AND ③. SAWCUT AS SHOWN IN DETAIL. THEN CONSTRUCT ⑥. CONSTRUCT ⑤, ⑩, AND ⑪. CONSTRUCT ⑦ TO TOP OF ② PRIOR TO PLACING ①. CONSTRUCT ① AND COMPLETE CONSTRUCTION OF ⑦.



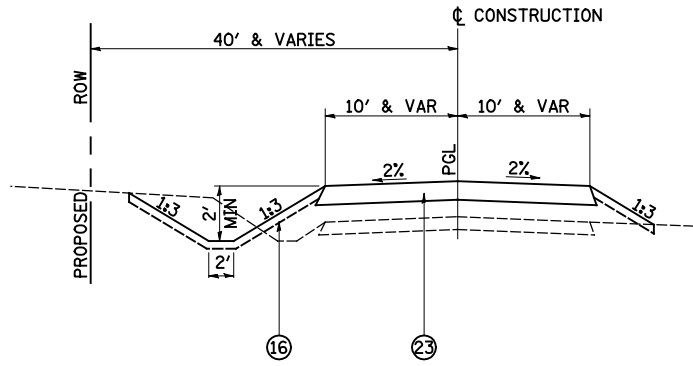
US 67 SECTION NO. 18
STA 455+45.16 TO STA 461+50.00

- NOTES:**
- VARIES FROM 4' @ STA 459+77.99 TO 9.9' @ STA 461+50
 - VARIES FROM 12' @ STA 458+43.86 TO 0' @ STA 461+50

| | | | | | | | | | | |
|--|-------------------|----------------|---------------------------|---|---|-----------------------------|-----------|--------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TYPICAL SECTIONS - FAP ROUTE 310 (US 67) (ALT. B) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-US67typical11.dgn | DRAWN - TJD | REVISED - | 407 | | | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 55 | |
| PLOT SCALE = 16.000' / IN. | CHECKED - JRB | REVISED - | CONTRACT NO. 68B44 | | | | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |

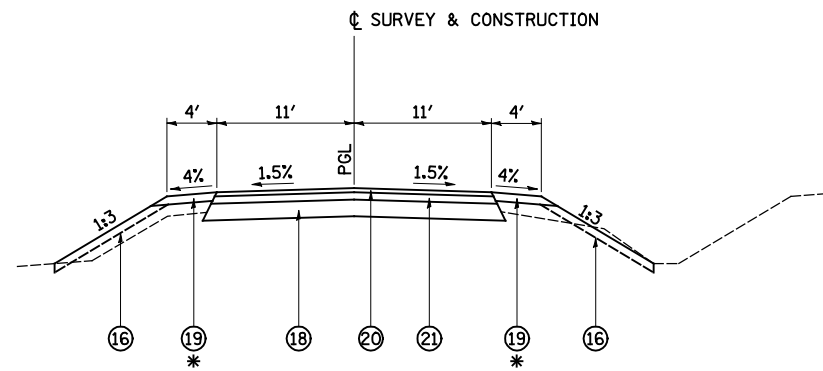


US 67 FRONTAGE ROAD CONNECTOR
STA 461+00.00 RT

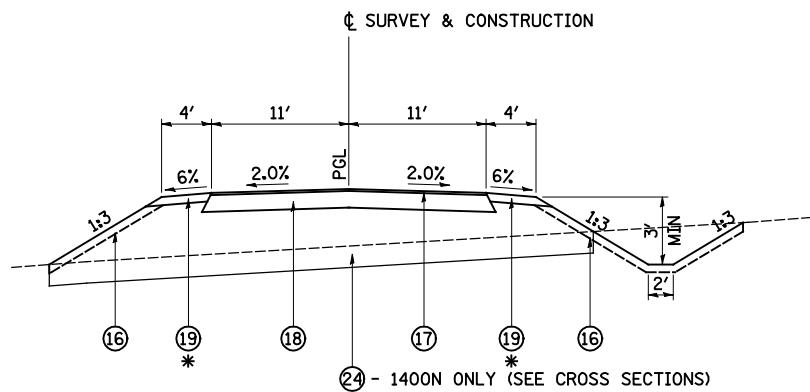


SERVICE DRIVE TYPICAL SECTION
STA 10837+85.26 TO STA 10839+80.00

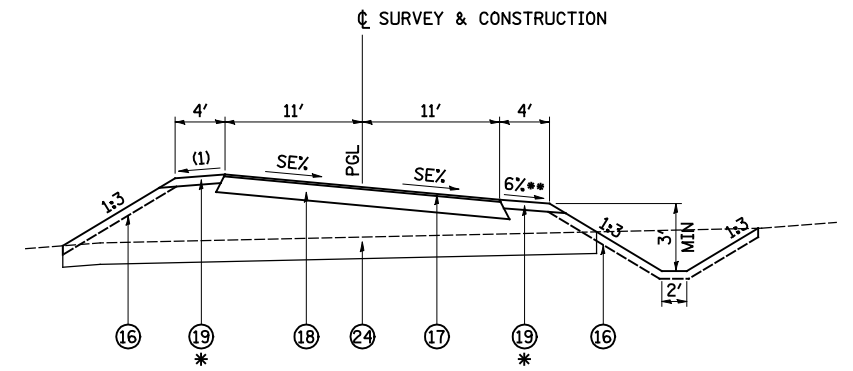
- LEGEND**
- ①6 TOPSOIL FURNISH AND PLACE, 4"
 - ①7 BITUMINOUS SURFACE TREATMENT, TYPE A-3
 - ①8 AGGREGATE BASE COURSE, TYPE A 8"
 - ①9 AGGREGATE SHOULDER, TYPE B
 - ②0 HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 2"
 - ②1 HOT-MIX ASPHALT BASE COURSE, 4"
 - ②2 INCIDENTAL HOT-MIX ASPHALT SURFACING, 2"
 - ②3 AGGREGATE SURFACE COURSE, TYPE A 8"
 - ②4 LIME MODIFIED SOIL, 12"



COUNTY HIGHWAY TANGENT SECTION
COUNTY HIGHWAY 20 (1500N)



TOWNSHIP ROAD TANGENT SECTION
TOWNSHIP ROAD 950E
TOWNSHIP ROAD 1400N (RELOCATED)
TOWNSHIP ROAD 1100E



TOWNSHIP ROAD SUPERELEVATED SECTION
TOWNSHIP ROAD 1400N (RELOCATED)

(1) MAXIMUM ROLLOVER = 8%
** 6% OR MATCH SE IF > 4%

* AGGREGATE SHOULDER, TYPE B SHALL BE 4" IN THICKNESS EXCEPT IN LOCATION OF GUARDRAIL. THE THICKNESS SHALL BE INCREASED TO 8" IN THESE LOCATIONS.

| | | | |
|--|-------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-typical06.dgn | | DRAWN - TJD | REVISED - |
| PLOT SCALE = 20.00 FT / IN. | | CHECKED - JRB | REVISED - |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

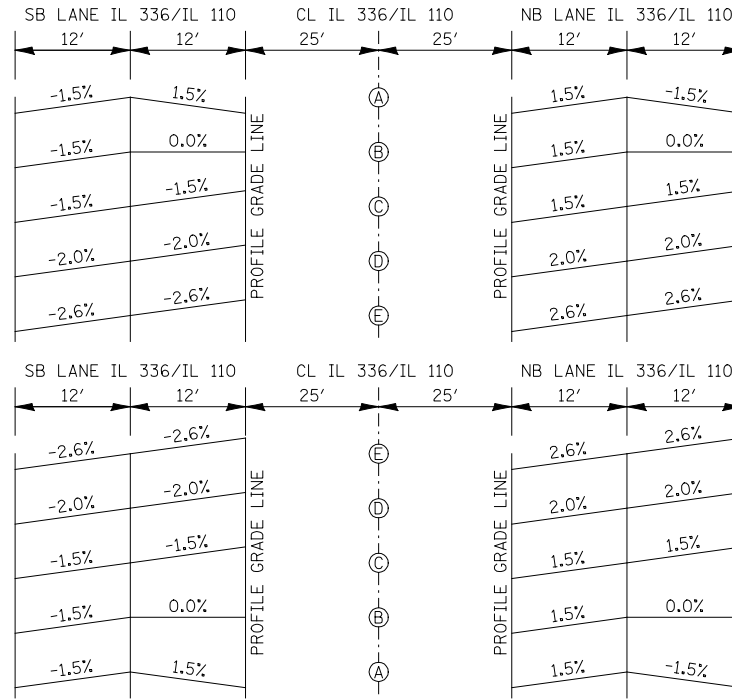
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | | | |
|---|-------------------------|------|---------|
| FAP ROUTE 407 (IL 336 /IL 110) | | | |
| TYPICAL SECTIONS - COUNTY HIGHWAY & TOWNSHIP ROADS | | | |
| SCALE: N/A | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------------------------|-----------|---------------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 56 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |

FAP 407 (IL 336/IL 110)- Curve No. 1 SOUTHBOUND

| SECTION | STATION | LEFT OUTSIDE EOP | | X-SLOPE | LEFT LANE CL | | X-SLOPE | LEFT INSIDE EOP | |
|---------|------------|------------------|--------|---------|--------------|--------|---------|-----------------|--------|
| | | OFFSET | ELEV | | OFFSET | ELEV | | OFFSET | ELEV |
| A | 548+25.250 | 49 | 662.96 | -1.5% | 37 | 663.14 | 1.5% | 25 | 662.96 |
| | 548+50.000 | 49 | 662.88 | -1.5% | 37 | 663.06 | 0.7% | 25 | 662.98 |
| B | 548+72.506 | 49 | 662.83 | -1.5% | 37 | 663.01 | 0.0% | 25 | 663.01 |
| | 548+75.000 | 49 | 662.82 | -1.5% | 37 | 663.00 | -0.1% | 25 | 663.01 |
| | 549+00.000 | 49 | 662.78 | -1.5% | 37 | 662.96 | -0.9% | 25 | 663.06 |
| C | 549+19.761 | 49 | 662.75 | -1.5% | 37 | 662.93 | -1.5% | 25 | 663.11 |
| | 549+25.000 | 49 | 662.74 | -1.6% | 37 | 662.93 | -1.6% | 25 | 663.12 |
| | 549+50.000 | 49 | 662.72 | -2.0% | 37 | 662.96 | -2.0% | 25 | 663.20 |
| D/PC | 549+51.233 | 49 | 662.72 | -2.0% | 37 | 662.96 | -2.0% | 25 | 663.20 |
| | 549+75.000 | 49 | 662.71 | -2.4% | 37 | 663.00 | -2.4% | 25 | 663.29 |
| E | 549+90.597 | 49 | 662.71 | -2.6% | 37 | 663.03 | -2.6% | 25 | 663.34 |



FAP 407 (IL 336/IL 110)- Curve No. 1 NORTHBOUND

| SECTION | STATION | RIGHT INSIDE EOP | | X-SLOPE | RIGHT LANE CL | | X-SLOPE | RIGHT OUTSIDE EOP | |
|---------|------------|------------------|--------|---------|---------------|--------|---------|-------------------|--------|
| | | OFFSET | ELEV | | OFFSET | ELEV | | OFFSET | ELEV |
| A | 548+25.250 | 25 | 662.96 | 1.5% | 37 | 663.14 | -1.5% | 49 | 662.96 |
| | 548+50.000 | 25 | 662.98 | 1.5% | 37 | 663.16 | -0.7% | 49 | 663.07 |
| B | 548+72.506 | 25 | 663.01 | 1.5% | 37 | 663.19 | 0.0% | 49 | 663.19 |
| | 548+75.000 | 25 | 663.01 | 1.5% | 37 | 663.19 | 0.1% | 49 | 663.20 |
| | 549+00.000 | 25 | 663.06 | 1.5% | 37 | 663.24 | 0.9% | 49 | 663.35 |
| C | 549+19.761 | 25 | 663.11 | 1.5% | 37 | 663.29 | 1.5% | 49 | 663.47 |
| | 549+25.000 | 25 | 663.12 | 1.6% | 37 | 663.31 | 1.6% | 49 | 663.50 |
| | 549+50.000 | 25 | 663.20 | 2.0% | 37 | 663.43 | 2.0% | 49 | 663.67 |
| D/PC | 549+51.233 | 25 | 663.20 | 2.0% | 37 | 663.44 | 2.0% | 49 | 663.68 |
| | 549+75.000 | 25 | 663.29 | 2.4% | 37 | 663.57 | 2.4% | 49 | 663.86 |
| E | 549+90.597 | 25 | 663.34 | 2.6% | 37 | 663.66 | 2.6% | 49 | 663.97 |

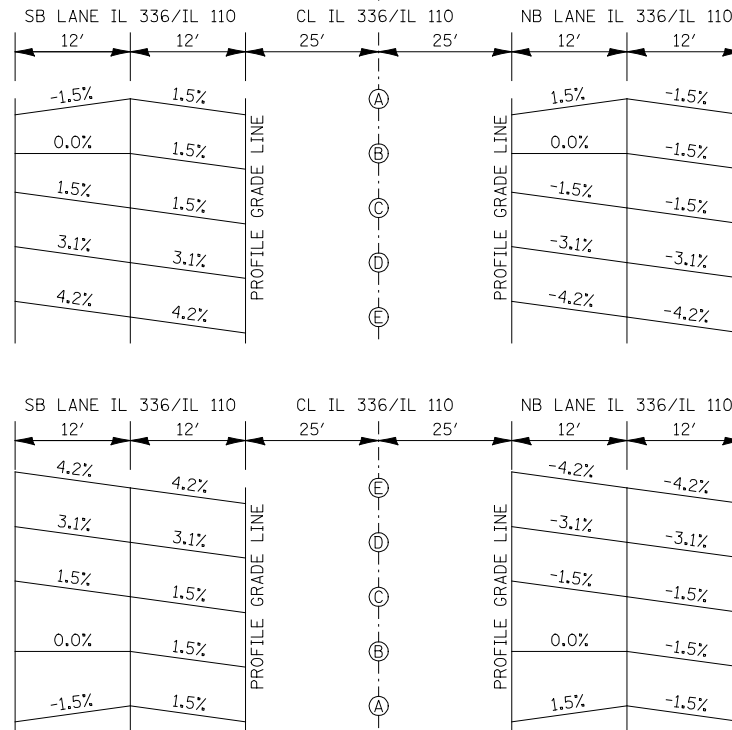
| SECTION | STATION | LEFT OUTSIDE EOP | | X-SLOPE | LEFT LANE CL | | X-SLOPE | LEFT INSIDE EOP | |
|---------|------------|------------------|--------|---------|--------------|--------|---------|-----------------|--------|
| | | OFFSET | ELEV | | OFFSET | ELEV | | OFFSET | ELEV |
| E | 573+45.814 | 49 | 661.40 | -2.6% | 37 | 661.71 | -2.6% | 25 | 662.03 |
| | 573+50.000 | 49 | 661.34 | -2.6% | 37 | 661.65 | -2.6% | 25 | 661.96 |
| | 573+75.000 | 49 | 661.03 | -2.2% | 37 | 661.29 | -2.2% | 25 | 661.55 |
| D/PT | 573+85.177 | 49 | 660.91 | -2.0% | 37 | 661.15 | -2.0% | 25 | 661.39 |
| | 574+00.000 | 49 | 660.72 | -1.8% | 37 | 660.93 | -1.8% | 25 | 661.14 |
| C | 574+16.650 | 49 | 660.50 | -1.5% | 37 | 660.68 | -1.5% | 25 | 660.86 |
| | 574+25.000 | 49 | 660.38 | -1.5% | 37 | 660.56 | -1.2% | 25 | 660.71 |
| | 574+50.000 | 49 | 660.04 | -1.5% | 37 | 660.22 | -0.4% | 25 | 660.27 |
| B | 574+63.905 | 49 | 659.84 | -1.5% | 37 | 660.02 | 0.0% | 25 | 660.02 |
| | 574+75.000 | 49 | 659.68 | -1.5% | 37 | 659.86 | 0.4% | 25 | 659.82 |
| | 575+00.000 | 49 | 659.32 | -1.5% | 37 | 659.50 | 1.1% | 25 | 659.36 |
| A | 575+11.160 | 49 | 659.15 | -1.5% | 37 | 659.33 | 1.5% | 25 | 659.15 |

| SECTION | STATION | RIGHT INSIDE EOP | | X-SLOPE | RIGHT LANE CL | | X-SLOPE | RIGHT OUTSIDE EOP | |
|---------|------------|------------------|--------|---------|---------------|--------|---------|-------------------|--------|
| | | OFFSET | ELEV | | OFFSET | ELEV | | OFFSET | ELEV |
| E | 573+45.814 | 25 | 662.03 | 2.6% | 37 | 662.34 | 2.6% | 49 | 662.66 |
| | 573+50.000 | 25 | 661.96 | 2.6% | 37 | 662.27 | 2.6% | 49 | 662.57 |
| | 573+75.000 | 25 | 661.55 | 2.2% | 37 | 661.81 | 2.2% | 49 | 662.07 |
| D/PT | 573+85.177 | 25 | 661.39 | 2.0% | 37 | 661.63 | 2.0% | 49 | 661.87 |
| | 574+00.000 | 25 | 661.14 | 1.8% | 37 | 661.35 | 1.8% | 49 | 661.56 |
| C | 574+16.650 | 25 | 660.86 | 1.5% | 37 | 661.04 | 1.5% | 49 | 661.22 |
| | 574+25.000 | 25 | 660.71 | 1.5% | 37 | 660.89 | 1.2% | 49 | 661.04 |
| | 574+50.000 | 25 | 660.27 | 1.5% | 37 | 660.45 | 0.4% | 49 | 660.51 |
| B | 574+63.905 | 25 | 660.02 | 1.5% | 37 | 660.20 | 0.0% | 49 | 660.20 |
| | 574+75.000 | 25 | 659.82 | 1.5% | 37 | 660.00 | -0.4% | 49 | 659.96 |
| | 575+00.000 | 25 | 659.36 | 1.5% | 37 | 659.54 | -1.1% | 49 | 659.40 |
| A | 575+11.160 | 25 | 659.15 | 1.5% | 37 | 659.33 | -1.5% | 49 | 659.15 |

BY OTHERS

FAP 407 (IL 336/IL 110)- Curve No. 3 SOUTHBOUND

| SECTION | STATION | LEFT OUTSIDE EOP | | X-SLOPE | LEFT LANE CL | | X-SLOPE | LEFT INSIDE EOP | |
|---------|------------|------------------|--------|---------|--------------|--------|---------|-----------------|--------|
| | | OFFSET | ELEV | | OFFSET | ELEV | | OFFSET | ELEV |
| A | 635+34.060 | 49 | 688.56 | -1.5% | 37 | 688.74 | 1.5% | 25 | 688.56 |
| | 635+50.000 | 49 | 688.87 | -0.9% | 37 | 688.98 | 1.5% | 25 | 688.80 |
| | 635+75.000 | 49 | 689.33 | 0.0% | 37 | 689.33 | 1.5% | 25 | 689.15 |
| B | 635+75.116 | 49 | 689.33 | 0.0% | 37 | 689.33 | 1.5% | 25 | 689.15 |
| | 636+00.000 | 49 | 689.78 | 0.9% | 37 | 689.67 | 1.5% | 25 | 689.49 |
| C | 636+16.173 | 49 | 690.06 | 1.5% | 37 | 689.88 | 1.5% | 25 | 689.70 |
| | 636+25.000 | 49 | 690.22 | 1.7% | 37 | 690.02 | 1.7% | 25 | 689.82 |
| | 636+50.000 | 49 | 690.64 | 2.1% | 37 | 690.38 | 2.1% | 25 | 690.13 |
| | 637+00.000 | 49 | 691.44 | 3.0% | 37 | 691.08 | 3.0% | 25 | 690.71 |
| D/PC | 637+01.551 | 49 | 691.46 | 3.1% | 37 | 691.10 | 3.1% | 25 | 690.73 |
| | 637+25.000 | 49 | 691.82 | 3.5% | 37 | 691.40 | 3.5% | 25 | 690.98 |
| | 637+50.000 | 49 | 692.19 | 3.9% | 37 | 691.71 | 3.9% | 25 | 691.24 |
| E | 637+64.769 | 49 | 692.40 | 4.2% | 37 | 691.89 | 4.2% | 25 | 691.39 |



FAP 407 (IL 336/IL 110)- Curve No. 3 NORTHBOUND

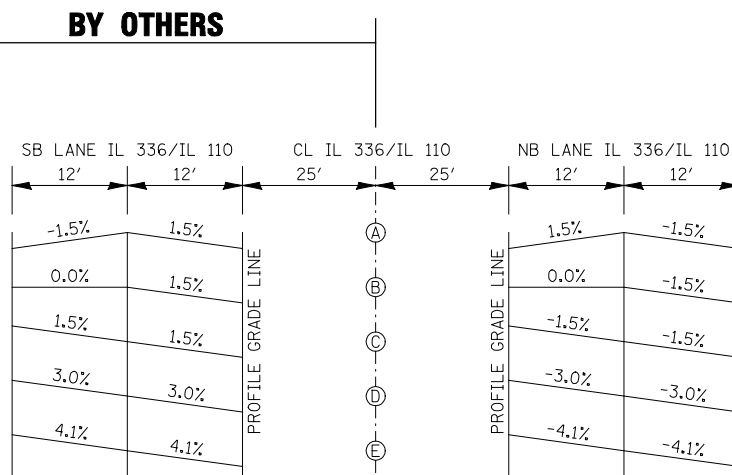
| SECTION | STATION | RIGHT INSIDE EOP | | X-SLOPE | RIGHT LANE CL | | X-SLOPE | RIGHT OUTSIDE EOP | |
|---------|------------|------------------|--------|---------|---------------|--------|---------|-------------------|--------|
| | | OFFSET | ELEV | | OFFSET | ELEV | | OFFSET | ELEV |
| A | 635+34.060 | 25 | 688.56 | 1.5% | 37 | 688.74 | -1.5% | 49 | 688.56 |
| | 635+50.000 | 25 | 688.80 | 0.9% | 37 | 688.91 | -1.5% | 49 | 688.73 |
| | 635+75.000 | 25 | 689.15 | 0.0% | 37 | 689.15 | -1.5% | 49 | 688.97 |
| B | 635+75.116 | 25 | 689.15 | 0.0% | 37 | 689.15 | -1.5% | 49 | 688.97 |
| | 636+00.000 | 25 | 689.49 | -0.9% | 37 | 689.38 | -1.5% | 49 | 689.20 |
| C | 636+16.173 | 25 | 689.70 | -1.5% | 37 | 689.52 | -1.5% | 49 | 689.34 |
| | 636+25.000 | 25 | 689.82 | -1.7% | 37 | 689.62 | -1.7% | 49 | 689.42 |
| | 636+50.000 | 25 | 690.13 | -2.1% | 37 | 689.88 | -2.1% | 49 | 689.62 |
| | 637+00.000 | 25 | 690.71 | -3.0% | 37 | 690.35 | -3.0% | 49 | 689.98 |
| D/PC | 637+01.551 | 25 | 690.73 | -3.1% | 37 | 690.36 | -3.1% | 49 | 690.00 |
| | 637+25.000 | 25 | 690.98 | -3.5% | 37 | 690.56 | -3.5% | 49 | 690.15 |
| | 637+50.000 | 25 | 691.24 | -3.9% | 37 | 690.77 | -3.9% | 49 | 690.29 |
| E | 637+64.769 | 25 | 691.39 | -4.2% | 37 | 690.88 | -4.2% | 49 | 690.37 |

| SECTION | STATION | LEFT OUTSIDE EOP | | X-SLOPE | LEFT LANE CL | | X-SLOPE | LEFT INSIDE EOP | |
|---------|------------|------------------|--------|---------|--------------|--------|---------|-----------------|--------|
| | | OFFSET | ELEV | | OFFSET | ELEV | | OFFSET | ELEV |
| E | 646+40.435 | 49 | 692.35 | 4.2% | 37 | 691.84 | 4.2% | 25 | 691.33 |
| | 646+50.000 | 49 | 692.21 | 4.0% | 37 | 691.72 | 4.0% | 25 | 691.24 |
| | 646+75.000 | 49 | 691.84 | 3.6% | 37 | 691.41 | 3.6% | 25 | 690.98 |
| | 647+00.000 | 49 | 691.46 | 3.1% | 37 | 691.09 | 3.1% | 25 | 690.71 |
| D/PT | 647+03.653 | 49 | 691.41 | 3.1% | 37 | 691.04 | 3.1% | 25 | 690.67 |
| | 647+25.000 | 49 | 691.07 | 2.7% | 37 | 690.75 | 2.7% | 25 | 690.43 |
| | 647+50.000 | 49 | 690.66 | 2.2% | 37 | 690.39 | 2.2% | 25 | 690.13 |
| | 647+75.000 | 49 | 690.24 | 1.8% | 37 | 690.03 | 1.8% | 25 | 689.82 |
| C | 647+89.032 | 49 | 689.99 | 1.5% | 37 | 689.81 | 1.5% | 25 | 689.63 |
| | 648+00.000 | 49 | 689.80 | 1.1% | 37 | 689.67 | 1.5% | 25 | 689.49 |
| | 648+25.000 | 49 | 689.35 | 0.2% | 37 | 689.33 | 1.5% | 25 | 689.15 |
| B | 648+30.088 | 49 | 689.26 | 0.0% | 37 | 689.26 | 1.5% | 25 | 689.08 |
| | 648+50.000 | 49 | 688.89 | -0.7% | 37 | 688.98 | 1.5% | 25 | 688.80 |
| A | 648+71.144 | 49 | 688.49 | -1.5% | 37 | 688.67 | 1.5% | 25 | 688.49 |

BY OTHERS

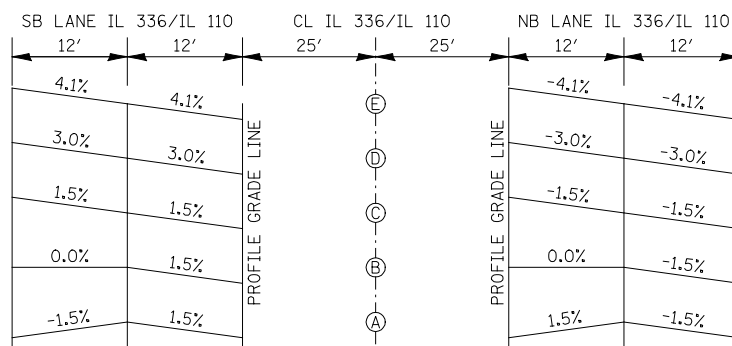
FAP 407 (IL 336/IL 110)- Curve No. 4 SOUTHBOUND

| SECTION | STATION | LEFT OUTSIDE EOP | | | X-SLOPE | LEFT LANE CL | | | X-SLOPE | LEFT INSIDE EOP | | |
|---------|------------|------------------|--------|--|---------|--------------|--------|--|---------|-----------------|--------|--|
| | | OFFSET | ELEV | | | OFFSET | ELEV | | | OFFSET | ELEV | |
| A | 657+80.172 | 49 | 666.33 | | -1.5% | 37 | 666.51 | | 1.5% | 25 | 666.33 | |
| | 658+00.000 | 49 | 665.86 | | -0.5% | 37 | 665.92 | | 1.5% | 25 | 665.74 | |
| B | 658+21.593 | 49 | 665.27 | | 0.0% | 37 | 665.27 | | 1.5% | 25 | 665.09 | |
| | 658+25.000 | 49 | 665.18 | | 0.1% | 37 | 665.17 | | 1.5% | 25 | 664.99 | |
| | 658+50.000 | 49 | 664.54 | | 1.0% | 37 | 664.42 | | 1.5% | 25 | 664.24 | |
| C | 658+63.013 | 49 | 664.21 | | 1.5% | 37 | 664.03 | | 1.5% | 25 | 663.85 | |
| | 658+75.000 | 49 | 663.90 | | 1.7% | 37 | 663.70 | | 1.7% | 25 | 663.49 | |
| | 659+00.000 | 49 | 663.26 | | 2.2% | 37 | 663.00 | | 2.2% | 25 | 662.74 | |
| | 659+25.000 | 49 | 662.62 | | 2.6% | 37 | 662.30 | | 2.6% | 25 | 661.99 | |
| D/PC | 659+43.093 | 49 | 662.16 | | 3.0% | 37 | 661.80 | | 3.0% | 25 | 661.45 | |
| | 659+50.000 | 49 | 661.98 | | 3.1% | 37 | 661.61 | | 3.1% | 25 | 661.24 | |
| | 659+75.000 | 49 | 661.34 | | 3.5% | 37 | 660.91 | | 3.5% | 25 | 660.49 | |
| | 660+00.000 | 49 | 660.70 | | 4.0% | 37 | 660.22 | | 4.0% | 25 | 659.74 | |
| E | 660+03.843 | 49 | 660.60 | | 4.1% | 37 | 660.11 | | 4.1% | 25 | 659.62 | |



FAP 407 (IL 336/IL 110)- Curve No. 4 NORTHBOUND

| SECTION | STATION | RIGHT INSIDE EOP | | | X-SLOPE | RIGHT LANE CL | | | X-SLOPE | RIGHT OUTSIDE EOP | | |
|---------|------------|------------------|--------|--|---------|---------------|--------|--|---------|-------------------|--------|--|
| | | OFFSET | ELEV | | | OFFSET | ELEV | | | OFFSET | ELEV | |
| A | 657+80.172 | 25 | 666.33 | | 1.5% | 37 | 666.51 | | -1.5% | 49 | 666.33 | |
| | 658+00.000 | 25 | 665.74 | | 0.5% | 37 | 665.80 | | -1.5% | 49 | 665.62 | |
| B | 658+21.593 | 25 | 665.09 | | 0.0% | 37 | 665.09 | | -1.5% | 49 | 664.91 | |
| | 658+25.000 | 25 | 664.99 | | -0.1% | 37 | 664.98 | | -1.5% | 49 | 664.80 | |
| | 658+50.000 | 25 | 664.24 | | -1.0% | 37 | 664.12 | | -1.5% | 49 | 663.94 | |
| C | 658+63.013 | 25 | 663.85 | | -1.5% | 37 | 663.67 | | -1.5% | 49 | 663.49 | |
| | 658+75.000 | 25 | 663.49 | | -1.7% | 37 | 663.28 | | -1.7% | 49 | 663.08 | |
| | 659+00.000 | 25 | 662.74 | | -2.2% | 37 | 662.48 | | -2.2% | 49 | 662.22 | |
| | 659+25.000 | 25 | 661.99 | | -2.6% | 37 | 661.68 | | -2.6% | 49 | 661.36 | |
| D/PC | 659+43.093 | 25 | 661.45 | | -3.0% | 37 | 661.09 | | -3.0% | 49 | 660.74 | |
| | 659+50.000 | 25 | 661.24 | | -3.1% | 37 | 660.87 | | -3.1% | 49 | 660.50 | |
| | 659+75.000 | 25 | 660.49 | | -3.5% | 37 | 660.07 | | -3.5% | 49 | 659.64 | |
| | 660+00.000 | 25 | 659.74 | | -4.0% | 37 | 659.26 | | -4.0% | 49 | 658.78 | |
| E | 660+03.843 | 25 | 659.62 | | -4.1% | 37 | 659.14 | | -4.1% | 49 | 658.65 | |

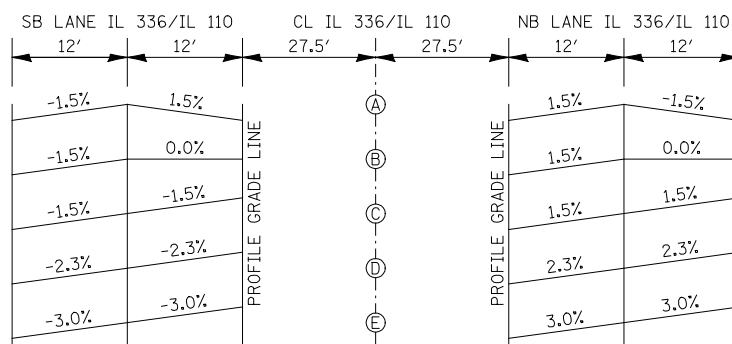


| SECTION | STATION | LEFT OUTSIDE EOP | | | X-SLOPE | LEFT LANE CL | | | X-SLOPE | LEFT INSIDE EOP | | |
|---------|------------|------------------|--------|--|---------|--------------|--------|--|---------|-----------------|--------|--|
| | | OFFSET | ELEV | | | OFFSET | ELEV | | | OFFSET | ELEV | |
| E | 706+06.829 | 49 | 703.68 | | 4.1% | 37 | 703.20 | | 4.1% | 25 | 702.71 | |
| | 706+25.000 | 49 | 703.74 | | 3.7% | 37 | 703.29 | | 3.7% | 25 | 702.84 | |
| | 706+50.000 | 49 | 703.80 | | 3.3% | 37 | 703.40 | | 3.3% | 25 | 703.01 | |
| D/PT | 706+67.579 | 49 | 703.83 | | 3.0% | 37 | 703.48 | | 3.0% | 25 | 703.12 | |
| | 706+75.000 | 49 | 703.84 | | 2.8% | 37 | 703.50 | | 2.8% | 25 | 703.17 | |
| | 707+00.000 | 49 | 703.87 | | 2.4% | 37 | 703.59 | | 2.4% | 25 | 703.30 | |
| | 707+25.000 | 49 | 703.88 | | 1.9% | 37 | 703.65 | | 1.9% | 25 | 703.43 | |
| C | 707+47.659 | 49 | 703.88 | | 1.5% | 37 | 703.70 | | 1.5% | 25 | 703.52 | |
| | 707+50.000 | 49 | 703.88 | | 1.4% | 37 | 703.71 | | 1.5% | 25 | 703.53 | |
| | 707+75.000 | 49 | 703.86 | | 0.5% | 37 | 703.80 | | 1.5% | 25 | 703.62 | |
| B | 707+89.079 | 49 | 703.84 | | 0.0% | 37 | 703.84 | | 1.5% | 25 | 703.66 | |
| | 708+00.000 | 49 | 703.83 | | -0.4% | 37 | 703.87 | | 1.5% | 25 | 703.69 | |
| | 708+25.000 | 49 | 703.78 | | -1.3% | 37 | 703.93 | | 1.5% | 25 | 703.75 | |
| A | 708+30.500 | 49 | 703.76 | | -1.5% | 37 | 703.94 | | 1.5% | 25 | 703.76 | |

| SECTION | STATION | RIGHT INSIDE EOP | | | X-SLOPE | RIGHT LANE CL | | | X-SLOPE | RIGHT OUTSIDE EOP | | |
|---------|------------|------------------|--------|--|---------|---------------|--------|--|---------|-------------------|--------|--|
| | | OFFSET | ELEV | | | OFFSET | ELEV | | | OFFSET | ELEV | |
| E | 706+06.829 | 25 | 702.71 | | -4.1% | 37 | 702.22 | | -4.1% | 49 | 701.74 | |
| | 706+25.000 | 25 | 702.84 | | -3.7% | 37 | 702.40 | | -3.7% | 49 | 701.95 | |
| | 706+50.000 | 25 | 703.01 | | -3.3% | 37 | 702.62 | | -3.3% | 49 | 702.23 | |
| D/PT | 706+67.579 | 25 | 703.12 | | -3.0% | 37 | 702.77 | | -3.0% | 49 | 702.41 | |
| | 706+75.000 | 25 | 703.17 | | -2.8% | 37 | 702.83 | | -2.8% | 49 | 702.49 | |
| | 707+00.000 | 25 | 703.30 | | -2.4% | 37 | 703.02 | | -2.4% | 49 | 702.74 | |
| | 707+25.000 | 25 | 703.43 | | -1.9% | 37 | 703.20 | | -1.9% | 49 | 702.97 | |
| C | 707+47.659 | 25 | 703.52 | | -1.5% | 37 | 703.34 | | -1.5% | 49 | 703.16 | |
| | 707+50.000 | 25 | 703.53 | | -1.4% | 37 | 703.36 | | -1.5% | 49 | 703.18 | |
| | 707+75.000 | 25 | 703.62 | | -0.5% | 37 | 703.56 | | -1.5% | 49 | 703.38 | |
| B | 707+89.079 | 25 | 703.66 | | 0.0% | 37 | 703.66 | | -1.5% | 49 | 703.48 | |
| | 708+00.000 | 25 | 703.69 | | 0.4% | 37 | 703.74 | | -1.5% | 49 | 703.56 | |
| | 708+25.000 | 25 | 703.75 | | 1.3% | 37 | 703.91 | | -1.5% | 49 | 703.73 | |
| A | 708+30.500 | 25 | 703.76 | | 1.5% | 37 | 703.94 | | -1.5% | 49 | 703.76 | |

FAP 407 (IL 336/IL 110)- Curve No. 5 SOUTHBOUND

| SECTION | STATION | LEFT OUTSIDE EOP | | | X-SLOPE | LEFT LANE CL | | | X-SLOPE | LEFT INSIDE EOP | | |
|---------|------------|------------------|--------|--|---------|--------------|--------|--|---------|-----------------|--------|--|
| | | OFFSET | ELEV | | | OFFSET | ELEV | | | OFFSET | ELEV | |
| A | 718+25.637 | 51.5 | 699.71 | | -1.5% | 39.5 | 699.89 | | 1.5% | 27.5 | 699.71 | |
| | 718+50.000 | 51.5 | 699.49 | | -1.5% | 39.5 | 699.67 | | 0.7% | 27.5 | 699.59 | |
| B | 718+70.618 | 51.5 | 699.31 | | -1.5% | 39.5 | 699.49 | | 0.0% | 27.5 | 699.49 | |
| | 718+75.000 | 51.5 | 699.27 | | -1.5% | 39.5 | 699.45 | | -0.1% | 27.5 | 699.47 | |
| | 719+00.000 | 51.5 | 699.04 | | -1.5% | 39.5 | 699.22 | | -1.0% | 27.5 | 699.34 | |
| C | 719+15.598 | 51.5 | 698.90 | | -1.5% | 39.5 | 699.08 | | -1.5% | 27.5 | 699.26 | |
| | 719+25.000 | 51.5 | 698.82 | | -1.7% | 39.5 | 699.02 | | -1.7% | 27.5 | 699.22 | |
| | 719+50.000 | 51.5 | 698.59 | | -2.1% | 39.5 | 698.84 | | -2.1% | 27.5 | 699.09 | |
| D/PC | 719+60.738 | 51.5 | 698.50 | | -2.3% | 39.5 | 698.77 | | -2.3% | 27.5 | 699.04 | |
| | 719+75.000 | 51.5 | 698.37 | | -2.5% | 39.5 | 698.67 | | -2.5% | 27.5 | 698.97 | |
| | 720+00.000 | 51.5 | 698.14 | | -2.9% | 39.5 | 698.49 | | -2.9% | 27.5 | 698.84 | |
| E | 720+05.798 | 51.5 | 698.09 | | -3.0% | 39.5 | 698.45 | | -3.0% | 27.5 | 698.81 | |



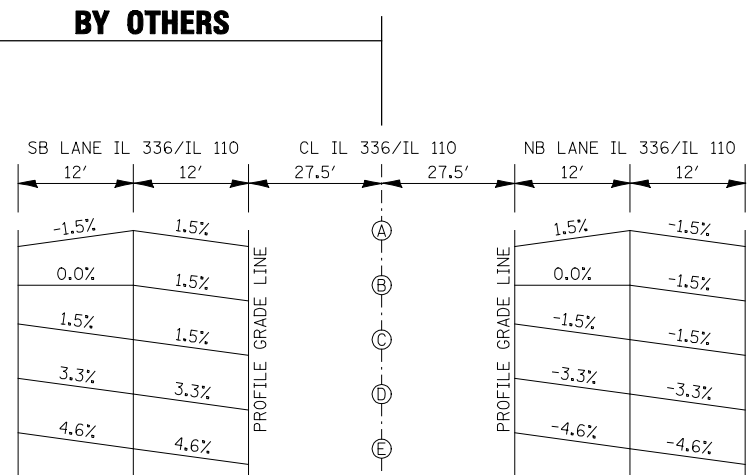
FAP 407 (IL 336/IL 110)- Curve No. 5 NORTHBOUND

| SECTION | STATION | RIGHT INSIDE EOP | | | X-SLOPE | RIGHT LANE CL | | | X-SLOPE | RIGHT OUTSIDE EOP | | |
|---------|------------|------------------|--------|--|---------|---------------|--------|--|---------|-------------------|--------|--|
| | | OFFSET | ELEV | | | OFFSET | ELEV | | | OFFSET | ELEV | |
| A | 718+25.637 | 27.5 | 699.71 | | 1.5% | 39.5 | 699.89 | | -1.5% | 51.5 | 699.71 | |
| | 718+50.000 | 27.5 | 699.59 | | 1.5% | 39.5 | 699.77 | | -0.7% | 51.5 | 699.69 | |
| B | 718+70.618 | 27.5 | 699.49 | | 1.5% | 39.5 | 699.67 | | 0.0% | 51.5 | 699.67 | |
| | 718+75.000 | 27.5 | 699.47 | | 1.5% | 39.5 | 699.65 | | 0.1% | 51.5 | 699.66 | |
| | 719+00.000 | 27.5 | 699.34 | | 1.5% | 39.5 | 699.52 | | 1.0% | 51.5 | 699.64 | |
| C | 719+15.598 | 27.5 | 699.26 | | 1.5% | 39.5 | 699.44 | | 1.5% | 51.5 | 699.62 | |
| | 719+25.000 | 27.5 | 699.22 | | 1.7% | 39.5 | 699.41 | | 1.7% | 51.5 | 699.61 | |
| | 719+50.000 | 27.5 | 699.09 | | 2.1% | 39.5 | 699.34 | | 2.1% | 51.5 | 699.59 | |
| D/PC | 719+60.738 | 27.5 | 699.04 | | 2.3% | 39.5 | 699.31 | | 2.3% | 51.5 | 699.58 | |
| | 719+75.000 | 27.5 | 698.97 | | 2.5% | 39.5 | 699.26 | | 2.5% | 51.5 | 699.56 | |
| | 720+00.000 | 27.5 | 698.84 | | 2.9% | 39.5 | 699.19 | | 2.9% | 51.5 | 699.54 | |
| E | 720+05.798 | 27.5 | 698.81 | | 3.0% | 39.5 | 699.17 | | 3.0% | 51.5 | 699.53 | |

| SECTION | STATION | LEFT OUTSIDE EOP | | | X-SLOPE | LEFT LANE CL | | | X-SLOPE | LEFT INSIDE EOP | | |
|---------|------------|------------------|--------|--|---------|--------------|--------|--|---------|-----------------|--------|--|
| | | OFFSET | ELEV | | | OFFSET | ELEV | | | OFFSET | ELEV | |
| E | 752+73.983 | 51.5 | 698.49 | | -3.0% | 39.5 | 698.85 | | -3.0% | 27.5 | 699.21 | |
| | 752+75.000 | 51.5 | 698.50 | | -3.0% | 39.5 | 698.86 | | -3.0% | 27.5 | 699.22 | |
| | 753+00.000 | 51.5 | 698.72 | | -2.6% | 39.5 | 699.03 | | -2.6% | 27.5 | 699.34 | |
| D/PT | 753+19.043 | 51.5 | 698.89 | | -2.3% | 39.5 | 699.16 | | -2.3% | 27.5 | 699.44 | |
| | 753+25.000 | 51.5 | 698.95 | | -2.2% | 39.5 | 699.21 | | -2.2% | 27.5 | 699.47 | |
| | 753+50.000 | 51.5 | 699.17 | | -1.7% | 39.5 | 699.38 | | -1.7% | 27.5 | 699.59 | |
| C | 753+64.183 | 51.5 | 699.30 | | -1.5% | 39.5 | 699.48 | | -1.5% | 27.5 | 699.66 | |
| | 753+75.000 | 51. | | | | | | | | | | |

FAP 407 (IL 336/IL 110)- Curve No. 6 SOUTHBOUND

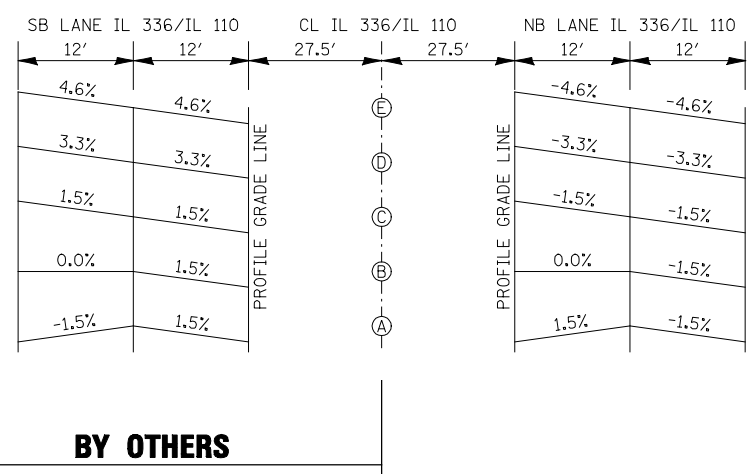
| SECTION | STATION | LEFT OUTSIDE EOP | | X-SLOPE | LEFT LANE CL | | X-SLOPE | LEFT INSIDE EOP | |
|---------|------------|------------------|--------|---------|--------------|--------|---------|-----------------|--------|
| | | OFFSET | ELEV | | OFFSET | ELEV | | OFFSET | ELEV |
| A | 796+76.363 | 51.5 | 711.81 | -1.5% | 39.5 | 711.99 | 1.5% | 27.5 | 711.81 |
| | 797+00.000 | 51.5 | 711.85 | -0.6% | 39.5 | 711.92 | 1.5% | 27.5 | 711.74 |
| B | 797+16.722 | 51.5 | 711.87 | 0.0% | 39.5 | 711.87 | 1.5% | 27.5 | 711.69 |
| | 797+25.000 | 51.5 | 711.88 | 0.3% | 39.5 | 711.85 | 1.5% | 27.5 | 711.67 |
| C | 797+50.000 | 51.5 | 711.92 | 1.2% | 39.5 | 711.77 | 1.5% | 27.5 | 711.59 |
| | 797+57.081 | 51.5 | 711.93 | 1.5% | 39.5 | 711.75 | 1.5% | 27.5 | 711.57 |
| D/PC | 797+75.000 | 51.5 | 711.95 | 1.8% | 39.5 | 711.73 | 1.8% | 27.5 | 711.52 |
| | 798+00.000 | 51.5 | 711.99 | 2.3% | 39.5 | 711.72 | 2.3% | 27.5 | 711.44 |
| E | 798+25.000 | 51.5 | 712.03 | 2.8% | 39.5 | 711.70 | 2.8% | 27.5 | 711.37 |
| | 798+50.000 | 51.5 | 712.06 | 3.2% | 39.5 | 711.68 | 3.2% | 27.5 | 711.29 |
| E | 798+54.122 | 51.5 | 712.07 | 3.3% | 39.5 | 711.67 | 3.3% | 27.5 | 711.28 |
| | 798+75.000 | 51.5 | 712.10 | 3.7% | 39.5 | 711.66 | 3.7% | 27.5 | 711.22 |
| E | 799+00.000 | 51.5 | 712.14 | 4.2% | 39.5 | 711.64 | 4.2% | 27.5 | 711.14 |
| | 799+22.822 | 51.5 | 712.17 | 4.6% | 39.5 | 711.62 | 4.6% | 27.5 | 711.07 |



FAP 407 (IL 336/IL 110)- Curve No. 6 NORTHBOUND

| SECTION | STATION | RIGHT INSIDE EOP | | X-SLOPE | RIGHT LANE CL | | X-SLOPE | RIGHT OUTSIDE EOP | |
|---------|------------|------------------|--------|---------|---------------|--------|---------|-------------------|--------|
| | | OFFSET | ELEV | | OFFSET | ELEV | | OFFSET | ELEV |
| A | 796+76.363 | 27.5 | 711.81 | 1.5% | 39.5 | 711.99 | -1.5% | 51.5 | 711.81 |
| | 797+00.000 | 27.5 | 711.74 | 0.6% | 39.5 | 711.81 | -1.5% | 51.5 | 711.63 |
| B | 797+16.722 | 27.5 | 711.69 | 0.0% | 39.5 | 711.69 | -1.5% | 51.5 | 711.51 |
| | 797+25.000 | 27.5 | 711.67 | -0.3% | 39.5 | 711.63 | -1.5% | 51.5 | 711.45 |
| C | 797+50.000 | 27.5 | 711.59 | -1.2% | 39.5 | 711.44 | -1.5% | 51.5 | 711.26 |
| | 797+57.081 | 27.5 | 711.57 | -1.5% | 39.5 | 711.39 | -1.5% | 51.5 | 711.21 |
| D/PC | 797+75.000 | 27.5 | 711.52 | -1.8% | 39.5 | 711.30 | -1.8% | 51.5 | 711.08 |
| | 798+00.000 | 27.5 | 711.44 | -2.3% | 39.5 | 711.16 | -2.3% | 51.5 | 710.89 |
| E | 798+25.000 | 27.5 | 711.37 | -2.8% | 39.5 | 711.03 | -2.8% | 51.5 | 710.70 |
| | 798+50.000 | 27.5 | 711.29 | -3.2% | 39.5 | 710.90 | -3.2% | 51.5 | 710.52 |
| E | 798+54.122 | 27.5 | 711.28 | -3.3% | 39.5 | 710.88 | -3.3% | 51.5 | 710.48 |
| | 798+75.000 | 27.5 | 711.22 | -3.7% | 39.5 | 710.77 | -3.7% | 51.5 | 710.33 |
| E | 799+00.000 | 27.5 | 711.14 | -4.2% | 39.5 | 710.64 | -4.2% | 51.5 | 710.14 |
| | 799+22.822 | 27.5 | 711.07 | -4.6% | 39.5 | 710.52 | -4.6% | 51.5 | 709.97 |

| SECTION | STATION | LEFT OUTSIDE EOP | | X-SLOPE | LEFT LANE CL | | X-SLOPE | LEFT INSIDE EOP | |
|---------|------------|------------------|--------|---------|--------------|--------|---------|-----------------|--------|
| | | OFFSET | ELEV | | OFFSET | ELEV | | OFFSET | ELEV |
| E | 833+07.163 | 51.5 | 710.49 | 4.6% | 39.5 | 709.94 | 4.6% | 27.5 | 709.39 |
| | 833+25.000 | 51.5 | 710.35 | 4.2% | 39.5 | 709.84 | 4.2% | 27.5 | 709.33 |
| D/PT | 833+50.000 | 51.5 | 710.15 | 3.8% | 39.5 | 709.69 | 3.8% | 27.5 | 709.24 |
| | 833+75.000 | 51.5 | 709.95 | 3.3% | 39.5 | 709.55 | 3.3% | 27.5 | 709.15 |
| C | 833+75.863 | 51.5 | 709.94 | 3.3% | 39.5 | 709.55 | 3.3% | 27.5 | 709.15 |
| | 834+00.000 | 51.5 | 709.75 | 2.9% | 39.5 | 709.41 | 2.9% | 27.5 | 709.07 |
| B | 834+25.000 | 51.5 | 709.55 | 2.4% | 39.5 | 709.26 | 2.4% | 27.5 | 708.98 |
| | 834+50.000 | 51.5 | 709.35 | 1.9% | 39.5 | 709.12 | 1.9% | 27.5 | 708.89 |
| A | 834+72.904 | 51.5 | 709.17 | 1.5% | 39.5 | 708.99 | 1.5% | 27.5 | 708.81 |
| | 834+75.000 | 51.5 | 709.15 | 1.4% | 39.5 | 708.98 | 1.5% | 27.5 | 708.80 |
| A | 835+00.000 | 51.5 | 708.95 | 0.5% | 39.5 | 708.90 | 1.5% | 27.5 | 708.72 |
| | 835+13.263 | 51.5 | 708.85 | 0.0% | 39.5 | 708.85 | 1.5% | 27.5 | 708.67 |
| A | 835+25.000 | 51.5 | 708.76 | -0.4% | 39.5 | 708.81 | 1.5% | 27.5 | 708.63 |
| | 835+50.000 | 51.5 | 708.56 | -1.4% | 39.5 | 708.72 | 1.5% | 27.5 | 708.54 |
| A | 835+53.622 | 51.5 | 708.53 | -1.5% | 39.5 | 708.71 | 1.5% | 27.5 | 708.53 |



| SECTION | STATION | RIGHT INSIDE EOP | | X-SLOPE | RIGHT LANE CL | | X-SLOPE | RIGHT OUTSIDE EOP | |
|---------|------------|------------------|--------|---------|---------------|--------|---------|-------------------|--------|
| | | OFFSET | ELEV | | OFFSET | ELEV | | OFFSET | ELEV |
| E | 833+07.163 | 27.5 | 709.39 | -4.6% | 39.5 | 708.84 | -4.6% | 51.5 | 708.29 |
| | 833+25.000 | 27.5 | 709.33 | -4.2% | 39.5 | 708.82 | -4.2% | 51.5 | 708.31 |
| D/PT | 833+50.000 | 27.5 | 709.24 | -3.8% | 39.5 | 708.79 | -3.8% | 51.5 | 708.33 |
| | 833+75.000 | 27.5 | 709.15 | -3.3% | 39.5 | 708.75 | -3.3% | 51.5 | 708.36 |
| C | 833+75.863 | 27.5 | 709.15 | -3.3% | 39.5 | 708.75 | -3.3% | 51.5 | 708.36 |
| | 834+00.000 | 27.5 | 709.07 | -2.9% | 39.5 | 708.72 | -2.9% | 51.5 | 708.38 |
| B | 834+25.000 | 27.5 | 708.98 | -2.4% | 39.5 | 708.69 | -2.4% | 51.5 | 708.40 |
| | 834+50.000 | 27.5 | 708.89 | -1.9% | 39.5 | 708.66 | -1.9% | 51.5 | 708.43 |
| A | 834+72.904 | 27.5 | 708.81 | -1.5% | 39.5 | 708.63 | -1.5% | 51.5 | 708.45 |
| | 834+75.000 | 27.5 | 708.80 | -1.4% | 39.5 | 708.63 | -1.5% | 51.5 | 708.45 |
| A | 835+00.000 | 27.5 | 708.72 | -0.5% | 39.5 | 708.66 | -1.5% | 51.5 | 708.48 |
| | 835+13.263 | 27.5 | 708.67 | 0.0% | 39.5 | 708.67 | -1.5% | 51.5 | 708.49 |
| A | 835+25.000 | 27.5 | 708.63 | 0.4% | 39.5 | 708.68 | -1.5% | 51.5 | 708.50 |
| | 835+50.000 | 27.5 | 708.54 | 1.4% | 39.5 | 708.70 | -1.5% | 51.5 | 708.52 |
| A | 835+53.622 | 27.5 | 708.53 | 1.5% | 39.5 | 708.71 | -1.5% | 51.5 | 708.53 |

EARTHWORK (ALTERNATE A)

| LOCATION | 20200100 | EXCAVATION TO BE USED IN EMBANKMENT ADJUSTED FOR SHRINKAGE * | EMBANKMENT | 20400800 | 21101615 |
|---|------------------|--|------------------|-----------------------------------|----------------------------|
| | EARTH EXCAVATION | | | BALANCE WASTE (+) OR SHORTAGE (-) | TOPSOIL FURNISH & PLACE 4" |
| | CU YD | | | SQ YD | |
| FAP ROUTE 407 (IL 336/IL 110) | | | | | |
| STA 541+57.14 TO STA 577+00.00 | | | | | 2,590.5 |
| STA 577+00.00 TO STA 580+03.80 | | | | | 142.1 |
| STA 586+57.70 TO STA 633+80.96 | | | | | 2,309.1 |
| STA 634+32.45 TO STA 680+63.18 | | | | | 2,263.9 |
| STA 682+14.84 TO STA 702+42.09 | | | | | 991.1 |
| STA 702+98.91 TO STA 729+00.00 | | | | | 1,271.6 |
| STA 729+00.00 TO STA 734+00.00 | 49,345.6 | 39,476.5 | 183.8 | 39,292.7 | 6,477.8 |
| STA 734+00.00 TO STA 779+23.37 | | | | | 2,211.4 |
| STA 779+84.87 TO STA 801+00.00 | | | | | 1,034.1 |
| STA 801+00.00 TO STA 805+00.00 | 1,241.9 | 993.5 | 2,782.9 | (1,789.4) | 2,006.6 |
| STA 805+00.00 TO STA 840+00.00 | | | | | 1,711.1 |
| STA 840+00.00 TO STA 842+00.00 | 852.9 | 682.3 | 2,878.7 | (2,196.4) | 865.8 |
| STA 842+00.00 TO STA 864+50.00 | | | | | 1,100.0 |
| STA 864+50.00 TO STA 886+50.00 | | | | | 1,679.1 |
| RAMP L | | | | | |
| STA 50+00.00 TO STA 72+04.23 | | | | | 916.7 |
| RAMP K | | | | | |
| STA 101+45.00 TO STA 131+93.42 | | | | | 1,490.3 |
| RAMP A | | | | | |
| STA 322+00.00 TO STA 323+50.00 | 0.0 | 0.0 | 1,073.5 | (1,073.5) | 1,274.7 |
| STA 323+50.00 TO STA 345+42.00 | | | | | 761.8 |
| RAMP D | | | | | |
| STA 200+00.00 TO STA 219+04.55 | | | | | 769.8 |
| STA 219+04.55 TO STA 219+50.00 | 0.0 | 0.0 | 342.6 | (342.6) | 184.8 |
| EMORY ROAD (950E) | | | | | |
| STA 3001+00.00 TO STA 3010+75.89 (BACK OF ABUTMENT) | 1,223.7 | 979.0 | 15,371.0 | (14,392.0) | 6,707.7 |
| STA 3013+21.78 (BACK OF ABUTMENT) TO STA 3027+00.00 | 2,924.3 | 2,339.4 | 18,658.2 | (16,318.8) | 8,356.8 |
| TOWER ROAD (1400N) | | | | | |
| RE-GRADE EXISTING ROADBED | 390.0 | 312.0 | 867.0 | (555.0) | 3,900.0 |
| STA 11+00.00 TO STA 28+00.00 | 3,985.6 | 3,188.5 | 7,648.7 | (4,460.2) | 12,163.4 |
| COUNTY HIGHWAY 20 (1500N) | | | | | |
| STA 5000+50.00 TO STA 5009+33.81 (BACK OF ABUTMENT) | 48.6 | 38.9 | 33,086.8 | (33,047.9) | 7,065.0 |
| STA 5012+19.25 (BACK OF ABUTMENT) TO STA 5031+00.00 | 320.4 | 256.3 | 40,165.0 | (39,908.7) | 9,443.2 |
| STA 5042+00.00 TO STA 5051+00.00 | 39.5 | 31.6 | 753.4 | (721.8) | 769.4 |
| 1100E | | | | | |
| STA 6005+00.00 TO STA 6017+97.81 (BACK OF ABUTMENT) | 57.0 | 45.6 | 28,638.4 | (28,592.8) | 7,195.7 |
| STA 6020+09.81 (BACK OF ABUTMENT) TO STA 6031+78.00 | 107.6 | 86.1 | 24,534.8 | (24,448.7) | 5,436.3 |
| SERVICE DRIVE | | | | | |
| STA 10838+00.00 TO STA 10839+80.00 | 100.5 | 80.4 | 998.8 | (918.4) | 753.2 |
| FAP ROUTE 310 (US 67) | | | | | |
| RESURFACING (STA 435+24.79 TO STA 438+50.00) | 56.6 | 45.3 | 137.9 | (92.6) | 609.9 |
| PRE-STAGE 1 (STA 439+25.00 TO STA 446+00.00) | 40.8 | 32.6 | 29.9 | 2.7 | |
| STAGE 1 (STA 438+50.00 TO STA 461+50.00) | 3,776.0 | 3,020.8 | 962.6 | 2,058.2 | 7,410.1 |
| STAGE 2 (STA 438+50.00 TO STA 461+50.00) | 3,919.0 | 3,135.2 | 1,865.4 | 1,269.8 | 4,654.1 |
| STAGE 3 (STA 438+50.00 TO STA 461+50.00) | 5,169.3 | 4,135.4 | 4,616.6 | (481.2) | 12,129.7 |
| TOTAL | 73,599.3 | 58,879.4 | 185,596.0 | (126,716.6) | 118,646.8 |
| USE | 73,600 | 58,880 | 185,596 | 126,715 | 118,647 |

* SHRINKAGE FACTOR = 20%

PAVEMENT (ALTERNATE A)

| STATION TO STATION | 30200650 | 30200750 | 30200850 | 30200950 | 30201050 | 30201250 | 30201500 | 31200100 | X0324159 | 40600285 | 40603540 | 42000401 | 42000406 | 42001200 | 42001300 | 42001420 | 48101200 | 48300400 | 48300405 | Z0034105 |
|--------------------------------------|--------------------------|----------------|----------------|-----------------|----------------|----------------|----------------|------------------------|-------------------------------------|---|--|---|---|-----------------|------------------|--|-----------------------------|--|--|--------------------------|
| | PROCESSING MODIFIED SOIL | | | | | | LIME | STABILIZED SUBBASE, 4" | WHITEWASHING FOR CONCRETE PAVEMENTS | POLYMERIZED BITUMINOUS MATERIALS (PRIME COAT) | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | PORTLAND CEMENT CONCRETE PAVEMENT, 9" (JOINTED) | PORTLAND CEMENT CONCRETE PAVEMENT, 9 1/4" (JOINTED) | PAVEMENT FABRIC | PROTECTIVE COAT | BRIDGE APPROACH PAVEMENT CONNECTOR (PCC) | AGGREGATE SHOULDERS, TYPE B | PORTLAND CEMENT CONCRETE SHOULDERS, 9" | PORTLAND CEMENT CONCRETE SHOULDERS, 9 1/4" | MATERIAL TRANSFER DEVICE |
| | 12" | 14" | 16" | 18" | 20" | 24" | | | | | | | | | | | | | | |
| FAP ROUTE 407 (IL 336/IL 110) | | | | | | | | | | | | | | | | | | | | |
| 541+57.14 TO 579+74.19 | 29,867.9 | | | | | | 591.4 | 20,929.3 | 20,929.3 | | | 18,181.4 | | 450.3 | 26,905.1 | 450.3 | 2,399.3 | 8,273.4 | | |
| 586+87.25 TO 633+50.29 | 15,006.8 | 5,200.0 | | | | | 434.3 | 14,356.4 | 14,356.4 | | | 11,883.5 | | 918.6 | 19,820.0 | 918.6 | 2,323.9 | 7,017.9 | | |
| 634+63.20 TO 680+33.20 | 19,803.3 | | | | | | 392.1 | 14,070.2 | 14,070.2 | | | 11,609.5 | | 937.4 | 19,394.5 | 937.4 | 2,264.7 | 6,847.6 | | |
| 682+45.53 TO 702+11.93 | 8,521.1 | | | | | | 168.7 | 6,222.5 | 6,222.5 | | | 4,647.2 | | 919.8 | 8,343.1 | 919.8 | 823.8 | 2,776.1 | | |
| 703+28.07 TO 778+94.53 | 18,617.1 | | 1,138.9 | 7,288.9 | 2,505.6 | 4,783.3 | 887.3 | 23,110.0 | 23,110.0 | | | 19,671.5 | | 916.3 | 33,655.6 | 916.3 | 3,211.9 | 13,067.8 | | |
| 780+13.72 TO 886+50.00 | 44,582.0 | 6,605.6 | | 7,288.9 | | | 1,251.7 | 38,627.7 | 38,627.7 | | | 33,884.8 | | 464.2 | 54,731.9 | 464.2 | 5,224.5 | 20,382.9 | | |
| RAMP L | | | | | | | | | | | | | | | | | | | | |
| 50+00.00 TO 72+04.23 | 4,950.0 | | 1,662.6 | | | | 141.9 | 4,367.4 | 4,367.4 | | | 3,765.7 | | | 6,286.4 | | 1,023.4 | 2,520.7 | | |
| RAMP K | | | | | | | | | | | | | | | | | | | | |
| 101+45.00 TO 131+93.42 | 7,480.2 | | 1,665.0 | | | | 192.1 | 5,650.2 | 5,650.2 | | | 5,019.1 | | | 7,862.2 | | 1,282.4 | 2,843.1 | | |
| RAMP A | | | | | | | | | | | | | | | | | | | | |
| 321+07.18 TO 345+42.02 | 5,676.0 | | | 1,628.4 | | | 160.8 | 4,494.9 | 4,494.9 | | | 4,066.5 | | | 6,201.5 | | 940.8 | 2,135.0 | | |
| RAMP D | | | | | | | | | | | | | | | | | | | | |
| 200+00.00 TO 220+14.88 | 6,044.7 | | | | | | 119.7 | 4,170.6 | 4,170.6 | | | 3,718.9 | | | 5,934.5 | | 905.6 | 2,215.6 | | |
| FAP ROUTE 310 (US 67) | | | | | | | | | | | | | | | | | | | | |
| 429+64.16 TO 438+46.31 | | | | | | | | | | 9,251.2 | 1,295.2 | | | | | | | | | 1,295.2 |
| 438+46.31 TO 461+50.00 | 25,888.2 | | | | | | 512.6 | 17,036.9 | 17,036.9 | | | | 15,501.1 | | 22,501.7 | | 676.7 | | 7,000.6 | |
| TOTAL | 186,437.3 | 6,605.6 | 9,666.5 | 16,206.2 | 2,505.6 | 4,783.3 | 4,852.6 | 153,036.1 | 153,036.1 | 9,251.2 | 1,295.2 | 116,448.1 | 15,501.1 | 4,606.6 | 211,636.5 | 4,606.6 | 21,077.0 | 68,080.1 | 7,000.6 | 1,295.2 |
| USE | 186,438 | 6,606 | 9,667 | 16,207 | 2,506 | 4,784 | 4,852.6 | 153,037 | 153,037 | 9,251.2 | 1,296 | 116,449 | 15,502 | 4,607 | 211,637 | 4,607 | 21,077 | 68,081 | 7,001 | 1,296 |

EARTHWORK (ALTERNATE B)

| LOCATION | 20200100 | EXCAVATION TO BE USED IN EMBANKMENT ADJUSTED FOR SHRINKAGE * | EMBANKMENT | 20400800 | 21101615 |
|---|------------------|--|------------------|-----------------------------------|----------------------------|
| | EARTH EXCAVATION | | | BALANCE WASTE (+) OR SHORTAGE (-) | TOPSOIL FURNISH & PLACE 4" |
| | CU YD | | | SQ YD | |
| FAP ROUTE 407 (IL 336/IL 110) | | | | | |
| STA 541+57.14 TO STA 577+00.00 | | | | | 2,590.5 |
| STA 577+00.00 TO STA 580+03.80 | | | | | 142.1 |
| STA 586+57.70 TO STA 633+80.96 | | | | | 2,309.1 |
| STA 634+32.45 TO STA 680+63.18 | | | | | 2,263.9 |
| STA 682+14.84 TO STA 702+42.09 | | | | | 991.1 |
| STA 702+98.91 TO STA 729+00.00 | | | | | 1,271.6 |
| STA 729+00.00 TO STA 734+00.00 | 49,345.6 | 39,476.5 | 183.8 | 39,292.7 | 6,477.8 |
| STA 734+00.00 TO STA 779+23.37 | | | | | 2,211.4 |
| STA 779+84.87 TO STA 801+00.00 | | | | | 1,034.1 |
| STA 801+00.00 TO STA 805+00.00 | 1,241.9 | 993.5 | 2,782.9 | (1,789.4) | 2,006.6 |
| STA 805+00.00 TO STA 840+00.00 | | | | | 1,711.1 |
| STA 840+00.00 TO STA 842+00.00 | 852.9 | 682.3 | 2,878.7 | (2,196.4) | 865.8 |
| STA 842+00.00 TO STA 864+50.00 | | | | | 1,100.0 |
| STA 864+50.00 TO STA 886+50.00 | | | | | 1,679.1 |
| RAMP L | | | | | |
| STA 50+00.00 TO STA 72+04.23 | | | | | 916.7 |
| RAMP K | | | | | |
| STA 101+45.00 TO STA 131+93.42 | | | | | 1,490.3 |
| RAMP A | | | | | |
| STA 322+00.00 TO STA 323+50.00 | 0.0 | 0.0 | 1,073.5 | (1,073.5) | 1,274.7 |
| STA 323+50.00 TO STA 345+42.00 | | | | | 761.8 |
| RAMP D | | | | | |
| STA 200+00.00 TO STA 219+04.55 | | | | | 769.8 |
| STA 219+04.55 TO STA 219+50.00 | 0.0 | 0.0 | 342.6 | (342.6) | 184.8 |
| EMORY ROAD (950E) | | | | | |
| STA 3001+00.00 TO STA 3010+75.89 (BACK OF ABUTMENT) | 1,223.7 | 979.0 | 15,371.0 | (14,392.0) | 6,707.7 |
| STA 3013+21.78 (BACK OF ABUTMENT) TO STA 3027+00.00 | 2,924.3 | 2,339.4 | 18,658.2 | (16,318.8) | 8,356.8 |
| TOWER ROAD (1400N) | | | | | |
| RE-GRADE EXISTING ROADBED | 390.0 | 312.0 | 867.0 | (555.0) | 3,900.0 |
| STA 11+00.00 TO STA 28+00.00 | 3,985.6 | 3,188.5 | 7,648.7 | (4,460.2) | 12,163.4 |
| COUNTY HIGHWAY 20 (1500N) | | | | | |
| STA 5000+50.00 TO STA 5009+33.81 (BACK OF ABUTMENT) | 48.6 | 38.9 | 33,086.8 | (33,047.9) | 7,065.0 |
| STA 5012+19.25 (BACK OF ABUTMENT) TO STA 5031+00.00 | 320.4 | 256.3 | 40,165.0 | (39,908.7) | 9,443.2 |
| STA 5042+00.00 TO STA 5051+00.00 | 39.5 | 31.6 | 753.4 | (721.8) | 769.4 |
| 1100E | | | | | |
| STA 6005+00.00 TO STA 6017+97.81 (BACK OF ABUTMENT) | 57.0 | 45.6 | 28,638.4 | (28,592.8) | 7,195.7 |
| STA 6020+09.81 (BACK OF ABUTMENT) TO STA 6031+78.00 | 107.6 | 86.1 | 24,534.8 | (24,448.7) | 5,436.3 |
| SERVICE DRIVE | | | | | |
| STA 10838+00.00 TO STA 10839+80.00 | 100.5 | 80.4 | 998.8 | (918.4) | 753.2 |
| FAP ROUTE 310 (US 67) | | | | | |
| RESURFACING (STA 435+24.79 TO STA 438+50.00) | 56.6 | 45.3 | 137.9 | (92.6) | 609.9 |
| PRE-STAGE 1 (STA 439+25.00 TO STA 446+00.00) | 40.8 | 32.6 | 29.9 | 2.7 | |
| STAGE 1 (STA 438+50.00 TO STA 461+50.00) | 3,530.9 | 2,824.7 | 1,058.6 | 1,766.1 | 7,410.1 |
| STAGE 2 (STA 438+50.00 TO STA 461+50.00) | 3,465.7 | 2,772.6 | 2,117.5 | 655.1 | 4,654.1 |
| STAGE 3 (STA 438+50.00 TO STA 461+50.00) | 5,059.3 | 4,047.4 | 4,756.5 | (709.1) | 12,129.7 |
| TOTAL | 72,790.9 | 58,232.7 | 186,084.0 | (127,851.3) | 118,646.8 |
| USE | 72,790 | 58,233 | 186,084 | 127,850 | 118,647 |

* SHRINKAGE FACTOR = 20%

PAVEMENT (ALTERNATE B)

| STATION TO STATION | 30200650 | 30200750 | 30200850 | 30200950 | 30201050 | 30201250 | 30201500 | 40600285 | 40603085 | 40603235 | 40603540 | 42001430 | 48101200 | 48203029 | 20034105 |
|-----------------------------|--------------------------|----------------|-----------------|-----------------|----------------|----------------|----------------|---|---|---|--|---|-----------------------------|-------------------------------|--------------------------|
| | PROCESSING MODIFIED SOIL | | | | | | LIME | POLYMERIZED BITUMINOUS MATERIALS (PRIME COAT) | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, "MIX D", N70 | BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE) | AGGREGATE SHOULDERS, TYPE B | HOT-MIX ASPHALT SHOULDERS, 8" | MATERIAL TRANSFER DEVICE |
| | 12" | 14" | 16" | 18" | 20" | 24" | | | | | | | | | |
| | SQ YD | | | | | | TON | POUND | TON | | | SQ YD | TON | SQ YD | TON |
| FAP 407 (IL RTE 336) | | | | | | | | | | | | | | | |
| 541+57.14 TO 579+74.19 | 30,886.5 | | | | | | 611.5 | 33,057.8 | 6,961.4 | 2,340.4 | 2,050.4 | 450.3 | 1,794.3 | 8,273.4 | 11,352.2 |
| 586+87.25 TO 633+50.29 | 15,518.5 | | 5,377.3 | | | | 449.3 | 21,609.3 | 4,550.9 | 1,529.8 | 1,340.2 | 918.6 | 1,790.8 | 7,017.9 | 7,420.9 |
| 634+63.20 TO 680+33.20 | 20,478.7 | | | | | | 405.5 | 21,111.1 | 4,446.0 | 1,494.5 | 1,309.3 | 937.4 | 1,746.3 | 6,847.6 | 7,249.8 |
| 682+45.53 TO 702+11.93 | 8,811.7 | | | | | | 174.5 | 8,453.6 | 1,780.7 | | 598.3 | 524.1 | 919.8 | 638.7 | 2,776.1 |
| 703+28.07 TO 778+94.53 | 19,225.4 | | 1,175.8 | 7,525.3 | 2,586.8 | 4,938.5 | 916.1 | 35,771.4 | 7,533.4 | 2,532.4 | 2,218.5 | 916.3 | 2,380.2 | 13,067.8 | 12,284.3 |
| 780+13.72 TO 886+50.00 | 46,028.2 | 6,819.8 | | 7,525.3 | | | 1,292.5 | 61,617.2 | 12,976.5 | 4,362.1 | 3,821.4 | 464.2 | 3,867.5 | 20,382.9 | 21,160.0 |
| RAMP L | | | | | | | | | | | | | | | |
| 50+00.00 TO 72+04.23 | 5,193.8 | | 1,744.5 | | | | 148.9 | 6,963.8 | 1,475.7 | 489.6 | 429.4 | | 787.5 | 2,520.7 | 2,394.7 |
| RAMP K | | | | | | | | | | | | | | | |
| 101+45.00 TO 131+93.42 | 7,848.7 | | 1,731.3 | | | | 201.1 | 9,246.1 | 1,960.8 | 651.7 | 567.6 | | 986.8 | 2,843.1 | 3,180.1 |
| RAMP A | | | | | | | | | | | | | | | |
| 321+07.18 TO 345+42.02 | 5,955.6 | | | 1,708.6 | | | 168.6 | 7,472.3 | 1,582.5 | 527.1 | 459.7 | | 724.0 | 2,135.0 | 2,569.3 |
| RAMP D | | | | | | | | | | | | | | | |
| 200+00.00 TO 220+14.88 | 6,342.5 | | | | | | 125.6 | 6,844.4 | 1,450.7 | 482.5 | 420.5 | | 696.9 | 2,215.6 | 2,353.7 |
| FAP 310 (US RTE 67) | | | | | | | | | | | | | | | |
| 429+64.16 TO 438+46.31 | | | | | | | | 9,251.2 | | | 1,295.2 | | | | 1,295.2 |
| 438+46.31 TO 461+50.00 | 26,249.7 | | | | | | 519.7 | 28,020.4 | 6,312.0 | 1,993.6 | 1,753.5 | | 437.2 | 7,000.6 | 10,059.1 |
| TOTAL | 192,539.3 | 6,819.8 | 10,028.9 | 16,759.2 | 2,586.8 | 4,938.5 | 5,013.3 | 249,418.6 | 51,030.6 | 17,002.0 | 16,189.8 | 4,606.6 | 15,850.2 | 75,080.7 | 84,222.4 |
| USE | 192,540 | 6,820 | 10,029 | 16,760 | 2,587 | 4,939 | 5,013.3 | 249,418.6 | 51,031 | 17,002 | 16,190 | 4,607 | 15,851 | 75,081 | 84,223 |

| TREE REMOVAL | | | |
|-----------------------|-----------|--|-----------|
| STATION | OFFSET | 20100110 | |
| | | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | |
| | | | UNIT |
| FAP ROUTE 310 (US 67) | | | |
| 460+59.9 | 102.7' RT | | 6 |
| 460+90.8 | 102.9' RT | | 6 |
| 461+06.0 | 103.1' RT | | 6 |
| TOTAL | | | 18 |
| USE | | | 18 |

| AGGREGATE DITCH CHECKS | | | |
|------------------------|-----------|------------------------|-----------|
| STATION | SIDE | 28000315 | |
| | | AGGREGATE DITCH CHECKS | |
| | | | TON |
| 950E (EMORY RD) | | | |
| 3018+92.1 | 135.0' LT | | 19 |
| 1400N (TOWER RD) | | | |
| 17+48.8 | 47.2' RT | | 19 |
| TOTAL | | | 38 |
| USE | | | 38 |

| LOCATION | PERMANENT SEEDING SCHEDULE | | | | | |
|--------------------------------|----------------------------|------------------------------------|--------------------------------------|-------------------------------------|--------------|--------------------|
| | 25000210 | 25000400 | 25000500 | 25000600 | 25000750 | 25100115 |
| | SEEDING, CLASS 2A | NITROGEN FERTILIZER NUTRIENT | PHOSPHORUS FERTILIZER NUTRIENT | POTASSIUM FERTILIZER NUTRIENT | MOWING | MULCH, METHOD 2 |
| ACRE | | POUND | | | ACRE | |
| FAP 407 (IL 336) | | | | | | |
| STA 541+57.14 TO STA 577+00 | 2.49 | 224.1 | 224.1 | 224.1 | 2.49 | 2.49 |
| STA 577+00 TO STA 580+03.80 | 0.20 | 18.0 | 18.0 | 18.0 | 0.20 | 0.20 |
| STA 586+57.70 TO STA 633+80.96 | 3.12 | 280.8 | 280.8 | 280.8 | 3.12 | 3.12 |
| STA 634+32.45 TO STA 680+63.18 | 3.08 | 277.2 | 277.2 | 277.2 | 3.08 | 3.08 |
| STA 682+14.84 TO STA 702+42.09 | 1.35 | 121.5 | 121.5 | 121.5 | 1.35 | 1.35 |
| STA 702+98.91 TO STA 729+00 | 1.82 | 163.8 | 163.8 | 163.8 | 1.82 | 1.82 |
| STA 729+00 TO STA 734+00 | 1.34 | 120.6 | 120.6 | 120.6 | 1.34 | 0.54 |
| STA 734+00 TO STA 779+23.37 | 3.21 | 288.9 | 288.9 | 288.9 | 3.21 | 3.21 |
| STA 779+84.87 TO STA 801+00 | 1.51 | 135.9 | 135.9 | 135.9 | 1.51 | 1.51 |
| STA 801+00 TO STA 805+00 | 0.42 | 37.8 | 37.8 | 37.8 | 0.42 | 0.22 |
| STA 805+00 TO STA 840+00 | 2.48 | 223.2 | 223.2 | 223.2 | 2.48 | 2.48 |
| STA 840+00 TO STA 842+00 | 0.18 | 16.2 | 16.2 | 16.2 | 0.18 | 0.04 |
| STA 842+00 TO STA 864+50 | 1.60 | 144.0 | 144.0 | 144.0 | 1.60 | 1.60 |
| STA 864+50 TO STA 886+50 | 1.67 | 150.3 | 150.3 | 150.3 | 1.67 | 1.67 |
| RAMPS | | | | | | |
| RAMP L | 0.19 | 17.1 | 17.1 | 17.1 | 0.19 | 0.19 |
| RAMP K | 0.31 | 27.9 | 27.9 | 27.9 | 0.31 | 0.31 |
| RAMP A | 0.42 | 37.8 | 37.8 | 37.8 | 0.42 | 0.42 |
| RAMP D | 0.20 | 18.0 | 18.0 | 18.0 | 0.20 | 0.20 |
| 950E (EMORY RD) | | | | | | |
| STA 3001+00 TO STA 3010+75.89 | 1.39 | 125.1 | 125.1 | 125.1 | 1.39 | 0.56 |
| STA 3013+21.78 TO STA 3027+00 | 1.73 | 155.7 | 155.7 | 155.7 | 1.73 | 1.13 |
| 1400N (TOWER RD) | | | | | | |
| RE-GRADE EXISTING ROADWAY | 0.81 | 72.9 | 72.9 | 72.9 | 0.81 | 0.81 |
| STA 11+00 TO STA 28+00 | 2.51 | 225.9 | 225.9 | 225.9 | 2.51 | 2.51 |
| COUNTY HIGHWAY 20 | | | | | | |
| STA 5000+50 TO STA 5009+33.81 | 1.46 | 131.4 | 131.4 | 131.4 | 1.46 | 0.20 |
| STA 5012+19.25 TO STA 5031+00 | 1.95 | 175.5 | 175.5 | 175.5 | 1.95 | 0.32 |
| STA 5042+00 TO STA 5051+00 | 0.16 | 14.4 | 14.4 | 14.4 | 0.16 | 0.16 |
| 1100E | | | | | | |
| STA 6005+00 TO STA 6017+97.81 | 1.49 | 134.1 | 134.1 | 134.1 | 1.49 | 0.17 |
| STA 6020+09.81 TO STA 6031+78 | 1.12 | 100.8 | 100.8 | 100.8 | 1.12 | 0.20 |
| SERVICE DRIVE | | | | | | |
| STA 10838+00 TO STA 10839+80 | 0.16 | 14.4 | 14.4 | 14.4 | 0.16 | 0.16 |
| FAP ROUTE 310 (US 67) | | | | | | |
| STA 435+24.79 TO STA 438+50 | 0.13 | 11.7 | 11.7 | 11.7 | 0.13 | 0.13 |
| STA 438+50 TO STA 461+50 | 5.00 | 450.0 | 450.0 | 450.0 | 5.00 | 4.92 |
| TOTAL | 43.50 | 3,915.0 | 3,915.0 | 3,915.0 | 43.50 | 35.72 |
| USE | 43.50 | 3,915 | 3,915 | 3,915 | 43.50 | 35.75 |

| TEMPORARY DITCH CHECKS | | | |
|--------------------------------------|------|------------------------|------|
| STATION | SIDE | 28000305 | |
| | | TEMPORARY DITCH CHECKS | |
| | | | FOOT |
| FAP ROUTE 407 (IL 336/IL 110) | | | |
| 734+00 | LT | | 18 |
| 734+00 | MED | | 22 |
| 734+00 | RT | | 18 |
| 800+76 | LT | | 18 |
| 802+00 | MED | | 22 |
| 802+54 | RT | | 18 |
| 803+28 | LT | | 18 |
| 805+72 | RT | | 18 |
| 840+00 | LT | | 18 |
| 840+12 | RT | | 18 |
| 840+50 | MED | | 22 |
| 841+50 | MED | | 22 |
| 842+00 | LT | | 18 |
| 842+00 | RT | | 18 |
| 950E (EMORY RD) | | | |
| 3000+50 | LT | | 12 |
| 3002+00 | RT | | 12 |
| 3002+50 | LT | | 12 |
| 3004+00 | RT | | 12 |
| 3004+50 | LT | | 12 |
| 3006+50 | LT | | 12 |
| 3006+58 | RT | | 12 |
| 3007+46 | RT | | 12 |
| 3007+62 | LT | | 12 |
| 3008+34 | RT | | 12 |
| 3008+37 | LT | | 12 |
| 3009+13 | LT | | 12 |
| 3009+22 | RT | | 12 |
| 3009+88 | LT | | 12 |
| 3010+10 | RT | | 12 |
| 3013+53 | LT | | 12 |
| 3013+88 | RT | | 12 |
| 3014+18 | RT | | 12 |
| 3014+48 | RT | | 12 |
| 3014+77 | RT | | 12 |
| 3015+07 | RT | | 12 |
| 3015+37 | RT | | 12 |
| 3015+67 | RT | | 12 |
| 3015+97 | RT | | 12 |
| 3016+27 | RT | | 12 |
| 3016+57 | RT | | 12 |
| 3016+87 | RT | | 12 |
| 3022+04 | LT | | 12 |
| 3022+90 | LT | | 12 |
| 3023+06 | RT | | 12 |
| 3023+51 | LT | | 12 |
| 3023+59 | RT | | 12 |
| 3024+01 | LT | | 12 |
| 3024+07 | RT | | 12 |
| 3024+43 | LT | | 12 |
| 3024+49 | RT | | 12 |
| 3024+83 | LT | | 12 |
| 3024+89 | RT | | 12 |
| 3025+24 | LT | | 12 |
| 3025+30 | RT | | 12 |
| 3025+70 | RT | | 12 |
| 1400N (TOWER RD) | | | |
| 11+42 | LT | | 12 |
| 11+42 | RT | | 12 |
| 13+42 | LT | | 12 |
| 13+42 | RT | | 12 |
| 15+42 | LT | | 12 |
| 15+42 | RT | | 12 |
| 17+38 | LT | | 12 |
| 17+38 | RT | | 12 |
| 17+59 | LT | | 12 |
| 17+59 | RT | | 12 |
| 19+55 | LT | | 12 |
| 19+55 | RT | | 12 |
| 21+20 | RT | | 12 |
| 22+60 | LT | | 12 |
| 22+70 | RT | | 12 |
| 24+05 | LT | | 12 |
| 24+09 | RT | | 12 |
| 24+80 | LT | | 12 |
| 25+55 | LT | | 12 |

| TEMPORARY DITCH CHECKS | | | |
|------------------------------|------|------------------------|--------------|
| STATION | SIDE | 28000305 | |
| | | TEMPORARY DITCH CHECKS | |
| | | | FOOT |
| COUNTY HIGHWAY 20 | | | |
| 5008+00 | RT | | 12 |
| 5010+00 | LT | | 12 |
| 5018+14 | RT | | 12 |
| 5019+28 | LT | | 12 |
| 5021+03 | RT | | 12 |
| 5025+00 | LT | | 12 |
| 5025+41 | RT | | 12 |
| 1100E | | | |
| 6005+00 | LT | | 12 |
| 6005+00 | RT | | 12 |
| 6009+00 | LT | | 12 |
| 6010+30 | RT | | 12 |
| 6020+02 | LT | | 12 |
| 6020+10 | RT | | 12 |
| 6031+78 | LT | | 12 |
| 6031+78 | RT | | 12 |
| SERVICE DRIVE | | | |
| 10838+60 | LT | | 12 |
| 10839+80 | LT | | 12 |
| FAP ROUTE 310 (US 67) | | | |
| 438+10 | RT | | 21 |
| 440+10 | RT | | 24 |
| 442+10 | RT | | 24 |
| 444+08 | RT | | 24 |
| 444+29 | RT | | 24 |
| 445+35 | LT | | 18 |
| 446+25 | RT | | 18 |
| 447+35 | LT | | 18 |
| 447+98 | RT | | 18 |
| 449+35 | LT | | 18 |
| 450+50 | LT | | 18 |
| 452+50 | LT | | 18 |
| 454+50 | LT | | 18 |
| 456+11 | LT | | 18 |
| 456+36 | LT | | 18 |
| 456+61 | LT | | 18 |
| 457+53 | RT | | 18 |
| 459+50 | RT | | 18 |
| 461+50 | RT | | 18 |
| TOTAL | | | 1,561 |
| USE | | | 1,561 |

| TEMPORARY SEEDING SCHEDULE | | | |
|--------------------------------------|--------|--------------|-----------------------------------|
| LOCATION | MOWING | 25000750 | 28000250 |
| | | ACRE | TEMPORARY EROSION CONTROL SEEDING |
| | | | 3 APPLICATIONS POUND |
| FAP ROUTE 407 (IL 336/IL 110) | | | |
| STA 541+57.14 TO STA 577+00 | | 2.49 | 747.0 |
| STA 577+00 TO STA 580+03.80 | | 0.20 | 60.0 |
| STA 586+57.70 TO STA 633+80.96 | | 3.12 | 936.0 |
| STA 634+32.45 TO STA 680+63.18 | | 3.08 | 924.0 |
| STA 682+14.84 TO STA 702+42.09 | | 1.35 | 405.0 |
| STA 702+98.91 TO STA 729+00 | | 1.82 | 546.0 |
| STA 729+00 TO STA 734+00 | | 2.64 | 792.0 |
| STA 734+00 TO STA 779+23.37 | | 3.21 | 963.0 |
| STA 779+84.87 TO STA 801+00 | | 1.51 | 453.0 |
| STA 801+00 TO STA 805+00 | | 1.46 | 438.0 |
| STA 805+00 TO STA 840+00 | | 2.48 | 744.0 |
| STA 840+00 TO STA 842+00 | | 0.70 | 210.0 |
| STA 842+00 TO STA 864+50 | | 1.60 | 480.0 |
| STA 864+50 TO STA 886+50 | | 1.67 | 501.0 |
| RAMPS | | | |
| RAMP L | | 0.19 | 57.0 |
| RAMP K | | 0.31 | 93.0 |
| RAMP A | | 0.42 | 126.0 |
| RAMP D | | 0.20 | 60.0 |
| 950E (EMORY RD) | | | |
| STA 3001+00 TO STA 3010+75.89 | | 1.39 | 417.0 |
| STA 3013+21.78 TO STA 3027+00 | | 1.73 | 519.0 |
| 1400N (TOWER RD) | | | |
| RE-GRADE EXISTING ROADWAY | | 0.81 | 243.0 |
| STA 11+00 TO STA 28+00 | | 2.51 | 753.0 |
| COUNTY HIGHWAY 20 | | | |
| STA 5000+50 TO STA 5009+33.81 | | 1.46 | 438.0 |
| STA 5012+19.25 TO STA 5031+00 | | 1.95 | 585.0 |
| STA 5042+00 TO STA 5051+00 | | 0.16 | 48.0 |
| 1100E | | | |
| STA 6005+00 TO STA 6017+97.81 | | 1.49 | 447.0 |
| STA 6020+09.81 TO STA 6031+78 | | 1.12 | 336.0 |
| SERVICE DRIVE | | | |
| STA 10838+00 TO STA 10839+80 | | 0.16 | 48.0 |
| FAP ROUTE 310 (US 67) | | | |
| STA 435+24.79 TO STA 438+50 | | 0.13 | 39.0 |
| STA 438+50 TO STA 461+50 | | 5.00 | 1,500.0 |
| TOTAL | | 46.36 | 13,908.0 |
| USE | | 46.50 | 13,908 |

| HEAVY DUTY EROSION CONTROL BLANKET | | | |
|--------------------------------------|---------|------------------------------------|---------|
| STATION TO STATION | SIDE | 25100635 | |
| | | HEAVY DUTY EROSION CONTROL BLANKET | |
| | | | SQ YD |
| FAP ROUTE 407 (IL 336/IL 110) | | | |
| 729+38 | 734+00 | RT | 2,436.9 |
| 730+45 | 734+00 | LT | 1,411.5 |
| 801+00 | 803+00 | LT | 372.5 |
| 802+86 | 805+00 | RT | 584.0 |
| 840+24 | 841+86 | LT | 347.4 |
| 840+21 | 841+86 | RT | 322.5 |
| 950E (EMORY RD) | | | |
| 3006+00 | 3010+76 | LT | 1,968.0 |
| 3006+00 | 3010+76 | RT | 2,050.5 |
| 3018+00 | 3021+00 | LT | 1,476.1 |
| 3018+00 | 3021+00 | RT | 1,436.7 |
| COUNTY HIGHWAY 20 | | | |
| 5004+00 | 5009+34 | RT | 3,391.1 |
| 5005+00 | 5009+34 | LT | 2,723.7 |
| 5012+19 | 5019+00 | | |

| PERIMETER EROSION BARRIER | | | |
|--------------------------------------|---------|---------------------------|---------------|
| STATION TO STATION | SIDE | 28000400 | |
| | | PERIMETER EROSION BARRIER | |
| | | FOOT | |
| FAP ROUTE 407 (IL 336/IL 110) | | | |
| 729+34 | 734+05 | RT | 480 |
| 730+70 | 734+05 | LT | 466 |
| 800+86 | 802+52 | LT | 342 |
| 802+28 | 805+95 | RT | 370 |
| 840+02 | 842+09 | LT | 228 |
| 840+09 | 842+01 | RT | 222 |
| 950E (EMORY RD) | | | |
| 3013+46 | 3016+34 | LT | 289 |
| 3017+00 | 3018+80 | LT | 198 |
| 3019+05 | 3021+00 | LT | 205 |
| COUNTY HIGHWAY 20 | | | |
| 5003+00 | 5009+12 | LT | 615 |
| 5003+00 | 5008+78 | RT | 581 |
| 5011+00 | 5018+33 | RT | 734 |
| 5013+61 | 5019+36 | LT | 597 |
| 5019+13 | 5019+91 | RT | 85 |
| 5019+61 | 5023+00 | LT | 344 |
| 5020+10 | 5023+00 | RT | 295 |
| 5042+00 | 5045+00 | RT | 300 |
| 5042+00 | 5045+17 | LT | 319 |
| 5045+93 | 5049+58 | LT | 368 |
| 5046+22 | 5049+00 | RT | 278 |
| 5049+39 | 5050+99 | RT | 161 |
| 5049+98 | 5050+99 | LT | 103 |
| 1100E | | | |
| 6005+38 | 6006+76 | RT | 144 |
| 6007+82 | 6009+47 | RT | 166 |
| 6007+83 | 6009+00 | LT | 117 |
| 6010+20 | 6018+00 | LT | 782 |
| 6011+19 | 6017+90 | RT | 673 |
| 6020+02 | 6024+17 | RT | 420 |
| 6020+02 | 6025+08 | LT | 512 |
| FAP ROUTE 310 (US 67) | | | |
| 438+46 | 444+16 | LT | 569 |
| 456+00 | 462+00 | LT | 605 |
| TOTAL | | | 11,568 |
| USE | | | 11,568 |

| INLET EROSION CONTROL ITEMS | | | |
|--------------------------------------|----------|-------------------------|---------------|
| STATION | OFFSET | 28000500 | |
| | | INLET & PIPE PROTECTION | INLET FILTERS |
| | | EACH | |
| FAP ROUTE 407 (IL 336/IL 110) | | | |
| 572+50.0 | 0.0' RT | 1 | |
| 729+37.1 | 81.4' RT | 1 | |
| 801+28.3 | 0.0' RT | 1 | |
| 842+35.0 | 76.1' LT | 1 | |
| 863+50.0 | 0.0' RT | 1 | |
| 950E (EMORY RD) | | | |
| 3016+34.6 | 63.9' LT | 1 | |
| 3017+17.1 | 42.4' RT | 1 | |
| 3019+24.3 | 82.3' RT | 1 | |
| 3026+37.0 | 22.3' RT | 1 | |
| 1400N (TOWER RD) | | | |
| 11+66.5 | 34.8' LT | 1 | |
| 17+48.5 | 29.2' LT | 1 | |
| 24+69.9 | 28.1' RT | 1 | |
| 26+10.0 | 27.7' LT | 1 | |
| COUNTY HIGHWAY 20 | | | |
| 5025+55.0 | 23.5' RT | 1 | |
| 1100E | | | |
| 6010+17.0 | 44.0' RT | 1 | |
| RAMP D | | | |
| 219+65.1 | 35.1' LT | 1 | |
| 219+70.7 | 35.1' LT | 1 | |
| FAP ROUTE 310 (US 67) | | | |
| 440+43.5 | 15.0' LT | | 1 |
| 440+43.5 | 15.0' RT | | 1 |
| 442+83.5 | 15.0' LT | | 1 |
| 442+83.5 | 15.0' RT | | 1 |
| 442+93.5 | 15.0' LT | | 1 |
| 442+93.5 | 0.0' RT | 1 | |
| 442+93.5 | 15.0' RT | | 1 |
| 444+94.4 | 0.1' RT | | 1 |
| 445+36.9 | 0.2' RT | | 1 |
| 445+73.6 | 15.1' LT | | 1 |
| 445+73.7 | 12.9' RT | | 1 |
| 446+25.1 | 15.1' RT | | 1 |
| 447+25.1 | 4.3' RT | | 1 |
| 448+35.1 | 1.9' LT | | 1 |
| 448+35.0 | 15.5' RT | | 1 |
| 449+25.1 | 7.0' LT | | 1 |
| 451+65.1 | 1.8' RT | | 1 |
| 452+80.1 | 4.7' LT | | 1 |
| 453+75.1 | 15.2' LT | | 1 |
| 454+26.4 | 13.0' LT | | 1 |
| 454+26.5 | 15.1' RT | | 1 |
| 454+63.2 | 0.0' RT | | 1 |
| 454+66.0 | 88.8' LT | 1 | |
| 455+05.8 | 0.0' RT | | 1 |
| 460+77.8 | 75.1' RT | 1 | |
| TOTAL | | 20 | 22 |
| USE | | 20 | 22 |

| STATION TO STATION | SIDE | DESCRIPTION | WIDTH | LENGTH | RIPRAP | | | | | |
|---------------------------------------|-----------|-----------------|-----------------|--------|--------------|--------------|--------------|-------------|----------------|---------------|
| | | | | | 28100105 | 28100107 | 28100109 | 28100125 | 28200200 | |
| | | | | | STONE RIPRAP | | | | | FILTER FABRIC |
| | | | | | CLASS A3 | CLASS A4 | CLASS A5 | CLASS B3 | SQ YD | |
| FAP ROUTE 407 (IL 336/ IL 110) | | | | | | | | | | |
| 572+50 | LT | CULVERT & SLOPE | 6.0 | 13.0 | | | | 8.6 | 8.6 | |
| RAMP D | | | | | | | | | | |
| 219+44.8 | 219+56.8 | RT | CULVERT | 12.0 | 20.0 | | 26.7 | | 26.7 | |
| RAMP A | | | | | | | | | | |
| 321+68.6 | 321+80.3 | RT | CULVERT | 12.0 | 17.7 | | 23.6 | | 23.6 | |
| 321+76.0 | 322+12.9 | RT | DITCH | 12.4 | 57.3 | | 79.2 | | 79.2 | |
| 950E (EMORY RD) | | | | | | | | | | |
| 3013+42.6 | LT | OUTLET | 6.0 | 81.9 | 54.6 | | | | 54.6 | |
| 3013+61.0 | RT | OUTLET | 6.0 | 20.8 | 13.9 | | | | 13.9 | |
| 3018+00.0 | 3019+10.7 | RT | DITCH | 14.6 | 110.7 | | 179.6 | | 179.6 | |
| 3019+36.6 | 3023+00.0 | RT | DITCH | 14.6 | 363.4 | | 589.5 | | 589.5 | |
| 3021+00.0 | 3022+00.0 | LT | DITCH | 14.6 | 100.0 | | 162.2 | | 162.2 | |
| 1400N (TOWER RD) | | | | | | | | | | |
| 17+42.3 | 17+55.3 | LT | CULVERT & SLOPE | 13.0 | 23.6 | | 34.1 | | 34.1 | |
| 17+42.3 | 17+55.3 | RT | CULVERT | 13.0 | 20.0 | | 28.9 | | 28.9 | |
| 25+50.6 | 28+00.0 | RT | DITCH | 14.6 | 249.4 | | 404.6 | | 404.6 | |
| 27+07.3 | 28+00.0 | LT | DITCH | 14.6 | 92.7 | | 150.4 | | 150.4 | |
| 1100E | | | | | | | | | | |
| 6017+67.8 | LT | OUTLET | 6.0 | 78.0 | 52.0 | | | | 52.0 | |
| 6017+67.8 | RT | OUTLET | 6.0 | 78.0 | 52.0 | | | | 52.0 | |
| FAP ROUTE 310 (US 67) | | | | | | | | | | |
| 440+38.6 | 440+48.6 | RT | CULVERT & SLOPE | 10.0 | 23.6 | | 26.2 | | 26.2 | |
| 445+68.7 | 445+78.7 | RT | CULVERT & SLOPE | 10.0 | 23.6 | | 26.2 | | 26.2 | |
| 447+97.5 | 448+07.5 | RT | CULVERT | 6.0 | 10.0 | | | 6.7 | 6.7 | |
| 257+43.0 | 457+53.0 | RT | CULVERT | 6.0 | 10.0 | | | 6.7 | 6.7 | |
| TOTAL | | | | | 172.5 | 799.9 | 931.3 | 22.0 | 1,925.7 | |
| USE | | | | | 173 | 800 | 932 | 22 | 1,926 | |

| SHAPING AND GRADING ROADWAY | | |
|--------------------------------------|---|---------------|
| STATION TO STATION | 30103000 SHAPING AND GRADING ROADWAY UNIT | |
| FAP ROUTE 407 (IL 336/IL 110) | | |
| 541+57.14 TO 577+00.00 | 35.42 | |
| 864+50.00 TO 886+50.00 | 22.00 | |
| NORTHBOUND/ EASTBOUND LANES | | |
| 546+24.47 TO 580+05.80 | 33.81 | |
| 586+55.70 TO 633+77.39 | 47.22 | |
| 634+30.29 TO 680+69.86 | 46.40 | |
| 682+22.26 TO 702+41.01 | 20.19 | |
| 702+98.99 TO 779+16.44 | 76.17 | |
| 779+77.94 TO 886+50.00 | 106.72 | |
| RAMP L | | |
| 50+00.00 TO 72+04.23 | 22.04 | |
| RAMP K | | |
| 101+45.00 TO 131+93.42 | 30.48 | |
| RAMP A | | |
| 321+07.18 TO 345+42.02 | 24.35 | |
| RAMP D | | |
| 200+00.00 TO 220+14.88 | 20.15 | |
| TOTAL | | 484.95 |
| USE | | 484.95 |

| ENTRANCES | | | | | | |
|-------------------------|------|------------------|------|---|---|---|
| STATION | TYPE | THICKNESS INCHES | SIDE | 40200800 AGGREGATE SURFACE COURSE, TYPE B | 40800050 INCIDENTAL HOT-MIX ASPHALT SURFACING | 40201000 AGGREGATE FOR TEMPORARY ACCESS |
| 950E (EMORY RD) | | | | | | |
| 2999+10.0 | FE | 8 | LT | 74.6 | | 38.1 |
| 3016+75.0 | FE | 8 | LT | 109.6 | | 63.8 |
| 3017+50.0 | FE | 8 | RT | 130.9 | | 79.7 |
| 3025+91.5 | FE | 8 | LT | 53.2 | | 21.7 |
| 1400N (TOWER RD) | | | | | | |
| 12+00.0 | FE | 8 | LT | 102.1 | | 58.0 |
| 25+11.0 | CE | 8 | RT | 84.4 | | 26.7 |
| 26+54.0 | FE | 8 | LT | 105.3 | | 60.3 |
| 1500N (CH 20) | | | | | | |
| 5001+25.0 | FE | 8 | RT | 74.2 | | 37.1 |
| 5002+05.0 | FE | 8 | LT | 106.6 | | 61.5 |
| 5018+72.0 | PE | 6 | RT | | 115.6 | 296.4 |
| 5023+33.0 | FE | 8 | RT | 85.2 | | 45.3 |
| 5025+77.0 | PE | 6 | RT | | 37.3 | 19.5 |
| 5026+20.0 | FE | 8 | LT | 64.5 | | 29.9 |
| 1100E | | | | | | |
| 6009+82.0 | FE | 8 | RT | 108.1 | | 62.7 |
| TOTAL | | | | 1,098.7 | 152.9 | 900.7 |
| USE | | | | 1,099 | 153 | 901 |

| 35800100 PREPARATION OF BASE | | SQ YD |
|------------------------------|--|------------|
| LOCATION | | |
| SPRINGLAKE RD. (CH20) | | 160 |
| ABATTE AVE. | | 60 |
| EMORY RD (950 E) | | 130 |
| 1600N | | 150 |
| 1100E | | 120 |
| EMBER | | 60 |
| TOWER RD | | 90 |
| TOTAL | | 770 |

| 35800200 AGGREGATE BASE REPAIR | | | |
|--------------------------------|-------|------------|--|
| LOCATION | DEPTH | TON | |
| SPRINGLAKE RD. (CH20) | 6" | 55 | |
| ABATTE AVE. | 6" | 21 | |
| EMORY RD (950 E) | 6" | 45 | |
| 1600N | 6" | 51 | |
| 1100E | 6" | 41 | |
| EMBER | 6" | 21 | |
| TOWER RD | 6" | 31 | |
| TOTAL | | 265 | |

| 40300200 BIT. MATERIAL (PRIME COAT) | | TON |
|-------------------------------------|--|-------------|
| LOCATION | | |
| SPRINGLAKE RD. (CH20) | | 0.32 |
| ABATTE AVE. | | 0.12 |
| EMORY RD (950 E) | | 0.26 |
| 1600N | | 0.3 |
| 1100E | | 0.24 |
| EMBER | | 0.12 |
| TOWER RD | | 0.18 |
| TOTAL | | 1.54 |

| 40300500 COVER COAT AGGREGATE | | TON |
|-------------------------------|--|--------------|
| LOCATION | | |
| SPRINGLAKE RD. (CH20) | | 4 |
| ABATTE AVE. | | 1.5 |
| EMORY RD (950 E) | | 3.26 |
| 1600N | | 3.8 |
| 1100E | | 3 |
| EMBER | | 1.5 |
| TOWER RD | | 2.2 |
| TOTAL | | 19.26 |

| 40300600 SEAL COAT AGGREGATE | | TON |
|------------------------------|--|-------------|
| LOCATION | | |
| SPRINGLAKE RD. (CH20) | | 2 |
| ABATTE AVE. | | 0.8 |
| EMORY RD (950 E) | | 1.64 |
| 1600N | | 1.9 |
| 1100E | | 1.5 |
| EMBER | | 0.8 |
| TOWER RD | | 1.1 |
| TOTAL | | 9.74 |

| 40300400 BIT. MAT. (COVER AND SEAL COATS) | | TON |
|---|--|-------------|
| LOCATION | | |
| SPRINGLAKE RD. (CH20) | | 0.9 |
| ABATTE AVE. | | 0.36 |
| EMORY RD (950 E) | | 0.78 |
| 1600N | | 0.9 |
| 1100E | | 0.72 |
| EMBER | | 0.26 |
| TOWER RD | | 0.54 |
| TOTAL | | 4.46 |

Add a note that a Class A-3 Bituminous surface Treatment will be used.

1 APPLICATIONS

2 APPLICATIONS

1 APPLICATIONS

2 APPLICATIONS - COVER COAT
1 APPLICATION - SEAL COAT

| | | | | | | | | | | |
|--|-------------------|----------------|-----------|---|---|--------------------|------------------------|--------------|---------------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) SCHEDULES OF QUANTITIES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-lane paving plans\cadd sheets\468844-sht:schedule06.dgn | DRAWN - TJD | REVISI | REVISI | | | 407 | 55C3(PV)HB(2-6)B-1B-2J | McDONOUGH | 874 | 65 |
| PLOT SCALE = 100.0000' / IN. | CHECKED - JRB | REVISI | REVISI | | | CONTRACT NO. 68844 | | | | |
| Default | DATE - 1-30-2015 | REVISI | REVISI | | | SCALE: NONE | SHEET 6 OF 14 SHEETS | STA. TO STA. | ILLINOIS FED. AID PROJECT | |

| SIDEROADS | | | | | | | | | | | | | | | |
|-----------------------------------|-------------------------------|--------------|-----------------------------------|---------------------------------|--------------------------------------|--------------------------------|--|-------------|--------------|--------------|---|--|--------------------------------------|-----------------------------|--------------------------|
| STATION TO STATION | 30200650 | 30201500 | 35100700 | 35501300 | 40200700 | 40201000 | 40300200 | 40300400 | 40300500 | 40300600 | 40600285 | 40603310 | 40800050 | 48101200 | Z0034105 |
| | PROCESSING MODIFIED SOIL, 12" | LIME | AGGREGATE BASE COURSE, TYPE A, 8" | HOT-MIX ASPHALT BASE COURSE, 4" | AGGREGATE SURFACE COURSE, TYPE A, 8" | AGGREGATE FOR TEMPORARY ACCESS | BITUMINOUS SURFACE TREATMENT, TYPE A-3 | | | | POLYMERIZED BITUMINOUS MATERIALS (PRIME COAT) | HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 | INCIDENTAL HOT-MIX ASPHALT SURFACING | AGGREGATE SHOULDERS, TYPE B | MATERIAL TRANSFER DEVICE |
| | SQ YD | TON | SQ YD | | | TON | | | | POUND | TON | | | | |
| 950E (EMORY RD) | | | | | | | | | | | | | | | |
| 3001+50.00 TO 3010+45.89 | | | 2,178.9 | | | | 0.5 | 13.1 | 54.5 | 27.2 | | | | | 279.6 |
| 3013+51.78 TO 3027+00.00 | | | 3,275.9 | | | | 0.8 | 19.7 | 81.9 | 40.9 | | | | | 406.6 |
| 1400N (TOWER RD) | | | | | | | | | | | | | | | |
| 10+11.00 TO 28+00.00 | 8,289.0 | 164.1 | 4,471.8 | | | | 1.1 | 26.8 | 111.8 | 55.9 | | | | | 372.3 |
| 1500N (CH 20) | | | | | | | | | | | | | | | |
| 1020E (SPRING LAKE PARK ENTRANCE) | | | | | | | | | | | 135.5 | | 43.0 | | 11.1 |
| 5000+50.00 TO 5009+00.00 | | | 2,172.2 | 2,108.9 | | | | | | | 2,041.9 | 234.5 | | 263.1 | 234.5 |
| 5012+48.54 TO 5031+00.00 | | | 4,731.5 | 4,593.7 | | | | | | | 4,447.7 | 510.8 | | 506.7 | 510.8 |
| 5042+00.00 TO 5044+99.59 | | | 765.6 | 743.3 | | | | | | | 719.7 | 82.7 | | 66.0 | 82.7 |
| 5044+99.59 TO 5046-31.58 | | | 826.5 | 813.5 | | | | | | | 777.0 | 90.7 | | 38.6 | 90.7 |
| 5046-31.58 TO 5051+00.00 | | | 1,197.1 | 1,162.2 | | | | | | | 1,125.3 | 258.5 | | 51.6 | 258.5 |
| 1100E | | | | | | | | | | | | | | | |
| 6005+00.00 TO 6006+52.07 | | | 371.7 | | | | 0.1 | 2.2 | 9.3 | 4.6 | | | | | 16.7 |
| 6007+83.72 TO 6017+67.81 | | | 2,405.6 | | | | 0.6 | 14.4 | 60.1 | 30.1 | | | | | 351.5 |
| 6020+39.81 TO 6031+78.00 | | | 2,782.2 | | | | 0.7 | 16.7 | 69.6 | 34.8 | | | | | 316.3 |
| 1100E FRONTAGE ROAD | | | | | | | | | | | | | | | |
| 10837+85.26 TO 10839+80.00 | | | | | 825.6 | 282.1 | | | | | | | | | |
| FAP ROUTE 310 (US 67) | | | | | | | | | | | | | | | |
| 461+00.00, RT | | | 324.1 | | | 59.2 | 0.1 | | | | 162.1 | | 36.3 | | 20.5 |
| TOTAL | 8,289.0 | 164.1 | 25,503.1 | 9,421.6 | 825.6 | 341.3 | 3.9 | 92.9 | 387.2 | 193.5 | 9,409.2 | 1,177.2 | 79.3 | 2,700.6 | 1,177.2 |
| USE | 8,289 | 164.1 | 25,504 | 9,422 | 826 | 342 | 3.9 | 92.9 | 388 | 194 | 9,409.2 | 1,178 | 80 | 2,701 | 1,178 |

| TEMPORARY RAMP | | | | |
|------------------------------|------------|------|----------|----------------------|
| STATION TO STATION | TO STATION | SIDE | 40600990 | TEMPORARY RAMP SQ YD |
| FAP ROUTE 310 (US 67) | | | | |
| 429+64.16 TO 429+70.83 | RT | | | 17.8 |
| 429+64.16 TO 429+70.83 | LT | | | 17.8 |
| 434+72.83 TO 434+93.63 | RT | | | 16.3 |
| 434+76.85 TO 434+98.85 | LT | | | 16.3 |
| 438+39.64 TO 438+46.31 | RT | | | 17.8 |
| 438+39.64 TO 438+46.31 | LT | | | 29.6 |
| TOTAL | | | | 115.6 |
| USE | | | | 116 |

| CLASS D PATCH | | | |
|---------------------------------|-------|----------|--|
| LOCATION | WIDTH | 44201741 | CLASS D PATCHES, TYPE II, 8 INCH SQ YD |
| FAP ROUTE 310 (US 67) | | | |
| STA 454+31.0 TO STA 454+57.2 LT | 3.42' | | 21.5 |
| TOTAL | | | 21.5 |
| USE | | | 22 |

| CROSSOVER PAVEMENT | | | | |
|--------------------------------------|---|---|---|--|
| STATION TO STATION | 40600285 | 40603085 | 40603235 | 40603540 |
| | POLYMERIZED BITUMINOUS MATERIALS (PRIME COAT) | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, "MIX D", N70 |
| | POUND | TON | | |
| FAP ROUTE 407 (IL 336/IL 110) | | | | |
| 569+67.31 TO 578+73.91 | 2,179.6 | 461.8 | 153.7 | 134.0 |
| 858+88.02 TO 870+52.59 | 2,617.4 | 554.8 | 184.5 | 160.8 |
| TOTAL | 4,797.0 | 1,016.6 | 338.2 | 294.8 |
| USE | 4,797.0 | 1,017 | 339 | 295 |

| PAVEMENT REMOVAL | | | | | | |
|--------------------------------------|------------|------|------------------|-------------------------------------|-------|--|
| STATION TO STATION | TO STATION | SIDE | 44000100 | 44000157 | SQ YD | |
| | | | PAVEMENT REMOVAL | HOT-MIX ASPHALT SURFACE REMOVAL, 2" | | |
| FAP ROUTE 407 (IL 336/IL 110) | | | | | | |
| 541+57.14 TO 544+59.59 | LT | | 219.3 | | | |
| 546+24.47 TO 549+87.35 | RT | | 341.6 | | | |
| FAP ROUTE 310 (US 67) | | | | | | |
| 429+64.16 TO 438+46.31 | LT/RT | | | 11,564.0 | | |
| 435+76.00 TO 463+70.00 | RT | | 1,862.7 | | | |
| 438+46.31 TO 461+50.00 | LT/RT | | 22,244.0 | | | |
| 1500N (CH 20) | | | | | | |
| 1020E (SPRING LAKE ENTRANCE) 5001+24 | LT | | | 38.6 | | |
| TOTAL | | | 24,667.6 | 11,602.6 | | |
| USE | | | 24,668 | 11,603 | | |

| PIPE CULVERT REMOVAL | | | | |
|-------------------------|------|-------------|----------|---------------------------|
| STATION | SIDE | DESCRIPTION | 50105220 | PIPE CULVERT REMOVAL FOOT |
| | | | | |
| 950E (EMORY RD) | | | | |
| 3007+66.7 | LT | 12" CMP | | 21 |
| 3015+10.8 | RT | 15" CMP | | 26 |
| 1400N (TOWER RD) | | | | |
| 25+09.0 | RT | 12" CMP | | 51 |
| 26+55.8 | LT | 30" CMP | | 62 |
| TOTAL | | | | 160 |
| USE | | | | 160 |

| PIPE CULVERT SCHEDULE | | | | | | | | | | | | | | | | | | | | | | | | |
|--------------------------------------|------|------------------------------|----------|------------------------|--|------------|-----------|------------|------------------------|------------|-----------|------------|-----------|--|--|-----------|--------------------|----------|----------|----------|-----|-----|-----|--|
| STATION | SIDE | TYPE | STANDARD | 20800150 | 54001001 | 542A0229 | 542A1063 | 542A1069 | 542A1081 | 542A4009 | 542D0220 | 542D1060 | 542D1063 | 542D1069 | X6023615 | 54213669 | 54213681 | 54215550 | 54215553 | 54215559 | | | | |
| | | | | PIPE CULVERTS, CLASS A | | | | | PIPE CULVERTS, CLASS D | | | | | MANHOLES, TYPE A, 5' DIA. WITH MEDIAN INLET, SPECIAL | PRECAST REINFORCED CONCRETE FLARED END SECTION | | METAL END SECTIONS | | | | | | | |
| | | | | TRENCH BACKFILL | BOX CULVERT END SECTION, CULVERT NO. 1 | TYPE 1 | TYPE 2 | TYPE 6 | TYPE 1 | TYPE 2 | TYPE 2 | TYPE 2 | 24" | | 36" | 15" | 18" | 24" | 24" | 36" | 15" | 18" | 24" | |
| | | | | CU YD | EACH | FOOT | | | | | | | | | | | | | | EACH | | | | |
| FAP ROUTE 407 (IL 336/IL 110) | | | | | | | | | | | | | | | | | | | | | | | | |
| 572+50.0 | MED | INLET, CULVERT & END SECTION | 542301 | 107.1 | | | | | | 103 | | | | | | 1 | 1 | | | | | | | |
| 863+50.0 | MED | INLET, CULVERT & END SECTION | 542301 | | | | | | | 93 | | | | | | 1 | 1 | | | | | | | |
| RAMP D | | | | | | | | | | | | | | | | | | | | | | | | |
| 219+59.0 | | PIPE CULVERT & END SECTION | 542301 | 43.1 | | | | | | 136 | | | | | | | | | | 4 | | | | |
| 950E (EMORY RD) | | | | | | | | | | | | | | | | | | | | | | | | |
| 3011+03.9 | | PIPE CULVERT & END SECTION | 542301 | | | | | | | 180 | | | | | | | | | | 2 | | | | |
| 3017+50.0 | RT | PIPE CULVERT & END SECTION | 542401 | | | | | | | | | 82 | | | | | | | | 2 | | | | |
| 3025+91.5 | LT | PIPE CULVERT & END SECTION | 542401 | | | | | | | | 84 | | | | | | | | | 2 | | | | |
| 1400N (TOWER RD) | | | | | | | | | | | | | | | | | | | | | | | | |
| 12+00.0 | LT | PIPE CULVERT & END SECTION | 542401 | | | | | | | | | | 68 | | | | | | | 2 | | | | |
| 17+48.8 | | PIPE CULVERT & END SECTION | 542301 | 21.2 | | | 40 | | 40 | | | | | | | | 2 | | | | | | | |
| 25+11.0 | RT | PIPE CULVERT & END SECTION | 542401 | | | | | | | | | 78 | | | | | | | 2 | | | | | |
| 26+54.0 | LT | PIPE CULVERT & END SECTION | 542401 | | | | | | | | | | | 98 | | | | | | 2 | | | | |
| FAP ROUTE 310 (US 67) | | | | | | | | | | | | | | | | | | | | | | | | |
| 444+22.28 | LT | BOX CULVERT END SECTION | | | 1 | | | | | | | | | | | | | | | | | | | |
| 461+00.0 | RT | PIPE CULVERT & END SECTION | 542301 | 3.4 | | | 44 | | | | | | | | | | 2 | | | | | | | |
| TOTAL | | | | 174.8 | 1 | 180 | 40 | 196 | 40 | 180 | 84 | 160 | 68 | 98 | 2 | 10 | 2 | 6 | 2 | 2 | | | | |
| USE | | | | 175 | 1 | 180 | 40 | 196 | 40 | 180 | 84 | 160 | 68 | 98 | 2 | 10 | 2 | 6 | 2 | 2 | | | | |

| | | | |
|---|------------------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| W:\Transportation\2891\2-Lane Paving Plans\CADD Sheets\D468B44-sht-schedule07.dgn | | DRAWN - TJD | REVISED - |
| Default | PLOT SCALE = 100.0000' / IN. | CHECKED - JRB | REVISED - |
| | PLOT DATE = 2/3/2015 | DATE - 1-30-2015 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | | | | | | | | |
|---|---------|--------------|--------------|--------------------|---------------------------|---------------------------|--------------|-----------|
| FAP ROUTE 407 (IL 336 /IL 110) SCHEDULES OF QUANTITIES | | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| SCALE: NONE | SHEET 7 | OF 14 SHEETS | STA. TO STA. | 407 | 55C3(PV)HB(2-6)B,B-1,B-2J | McDONOUGH | 874 | 66 |
| | | | | CONTRACT NO. 68B44 | | ILLINOIS FED. AID PROJECT | | |

STORM SEWER

| STATION | OFFSET | TYPE | 20800150 | 54213657 | 54213663 | 54213675 | 550A0050 | 550A0070 | 550A0090 | 550A0360 | 550A0380 | 550A0430 | 60219530 | 60222230 | X6023610 | X6023615 | 60237460 | 60240327 | |
|------------------------------|------------|-------------|-----------------|---|----------|----------|-----------------------|------------|------------|------------|------------|------------|-------------------------|----------|----------------------------|----------|-------------------------|----------|---|
| | | | TRENCH BACKFILL | PRECAST REINFORCED CONCRETE FLARED END SECTIONS | | | STORM SEWERS, CLASS A | | | | | | MANHOLES, TYPE A | | | | INLETS | | |
| | | | | | | | TYPE 1 | | | TYPE 2 | | | 4' DIA. | 5' DIA. | 4' DIA. | 5' DIA. | TYPE A | TYPE B | |
| | | | | 12" | 18" | 30" | 12" | 15" | 18" | 15" | 18" | 30" | TYPE 23 FRAME AND GRATE | | WITH MEDIAN INLET, SPECIAL | | TYPE 23 FRAME AND GRATE | | |
| CU YD | EACH | | | FOOT | | | | | | EACH | | | | | | | | | |
| FAP ROUTE 310 (US 67) | | | | | | | | | | | | | | | | | | | |
| 440+43.60 | 14.00 LT. | INLET | | | | | | | | | | | | | | | | 1 | |
| | | SS | 2 | | | | | 28 | | | | | | | | | | | |
| 440+43.60 | 14.00 RT. | INLET | | | | | | | | | | | | | | | | | 1 |
| | | SS | 11 | | | | | 54 | | | | | | | | | | | |
| 440+43.60 | 69.20' RT | END SECTION | | 1 | | | | | | | | | | | | | | | |
| 442+83.60 | 14.00 LT. | INLET | | | | | | | | | | | | | | | | 1 | |
| | | SS | 2 | | | | | 8 | | | | | | | | | | | |
| 442+93.60 | 14.00 LT. | INLET | | | | | | | | | | | | | | | | | 1 |
| | | SS | 2 | | | | | 12 | | | | | | | | | | | |
| 442+93.60 | C.L. | MANHOLE | | | | | | | | | | | | | 1 | | | | |
| | | SS | 2 | | | | | 12 | | | | | | | | | | | |
| 442+93.60 | 14.00 RT. | INLET | | | | | | | | | | | | | | | | | 1 |
| | | SS | 2 | | | | | 8 | | | | | | | | | | | |
| 442+83.60 | 14.00 RT. | INLET | | | | | | | | | | | | | | | | | 1 |
| | | SS | | | | | | | 110 | | | | | | | | | | |
| 444+95.30 | C.L. | INLET | 2 | | | | | | 52 | | | | | | | | | | 1 |
| | | SS | | | | | | | | | | | | | | | | | |
| 445+36.90 | C.L. | INLET | | | | | | | | | | | | | | | | | 1 |
| | | SS | 9 | | | | | 38 | | | | | | | | | | | |
| 445+73.70 | 15.50 LT. | INLET | | | | | | | | | | | | | | | | | 1 |
| | | SS | 9 | | | | | 28 | | | | | | | | | | | |
| 445+73.70 | 13.90 RT. | MANHOLE | | | | | | | | | | | | 1 | | | | | |
| | | SS | 15 | | | | | | | | 62 | | | | | | | | |
| 445+73.70 | 76.80 RT. | END SECTION | | 1 | | | | | | | | | | | | | | | |
| 446+25.00 | 15.50 RT. | INLET | 18 | | | | | | 50 | | | | | | | | | | 1 |
| | | SS | 34 | | | | | 98 | | | | | | | | | | | |
| 447+25.00 | 5.40 RT. | INLET | | | | | | | | | | | | | | | | | 1 |
| | | SS | 26 | | | | | 110 | | | | | | | | | | | |
| 448+35.00 | 0.90 LT. | MANHOLE | | | | | | | | | | | 1 | | | | | | |
| | | SS | 18 | | | | | 90 | | | | | | | | | | | |
| 449+25.00 | 6.00 LT. | INLET | | | | | | | | | | | | | | | | | 1 |
| | | SS | 2 | | | | | 14 | | | | | | | | | | | |
| 448+35.00 | 15.50 RT | INLET | | | | | | | | | | | | | | | | | 1 |
| | | SS | | | | | | | | | | | | | | | | | |
| 451+65.00 | 0.90 LT. | INLET | | | | | | | | | | | | | | | | | 1 |
| | | SS | 33 | | | | | | | 114 | | | | | | | | | |
| 452+80.00 | 5.70 LT. | INLET | | | | | | | | | | | | | | | | | 1 |
| | | SS | 69 | | | | | | | 94 | | | | | | | | | |
| 453+75.00 | 15.50 LT. | INLET | | | | | | | | | | | | | | | | | 1 |
| | | SS | 43 | | | | | | | 50 | | | | | | | | | |
| 454+26.40 | 15.50 RT. | INLET | 8 | | | | | | 28 | | | | | | | | | | 1 |
| | | SS | 81 | | | | | | | | 84 | | | | | | | | |
| 454+26.40 | 14.00 LT. | MANHOLE | | | | | | | | | | | | 1 | | | | | |
| | | SS | | | | | | | | | | | | | | | | | |
| 454+66.00 | 88.82 LT. | MANHOLE | | | | | | | | | | | | | | | 1 | | |
| | | SS | 101 | | | | | | | | | 104 | | | | | | | |
| 455+66.40 | 111.80 LT. | END SECTION | | 1 | | | | | | | | | | | | | | | |
| | | SS | 16 | | | | | | | | 38 | | | | | | | | |
| 454+63.20 | C.L. | INLET | | | | | | | | | | | | | | | | | 1 |
| | | SS | 13 | | | | | | | | 40 | | | | | | | | |
| 455+05.70 | C.L. | INLET | | | | | | | | | | | | | | | | | 1 |
| | | SS | | | | | | | | | | | | | | | | | |
| TOTAL | | | 518 | 1 | 1 | 1 | 136 | 494 | 110 | 336 | 146 | 104 | 1 | 2 | 1 | 1 | 11 | 8 | |

PIPE UNDERDRAIN OUTLET SCHEDULE

| STATION | 28100125 | | 60100060 | | 60108100 | | | | | | | | | |
|--------------------------------------|-----------------------|-------|------------------------------------|------------------------------------|-------------------------------|-----------|--------------|------------|-------|----|-----------|----|--|--|
| | STONE RIPRAP CLASS B3 | SQ YD | CONCRETE HEADWALLS FOR PIPE DRAINS | | PIPE UNDERDRAINS 4" (SPECIAL) | | | | | | | | | |
| | | | SINGLE PIPE OPENING | DOUBLE PIPE OPENING (SAG LOCATION) | SB/ WB LANES | | NB/ EB LANES | | RAMPS | | US RTE 67 | | | |
| | | | | | LT OUTSIDE | LT MEDIAN | RT MEDIAN | RT OUTSIDE | LT | RT | LT | RT | | |
| | | EACH | | FOOT | | | | | | | | | | |
| FAP ROUTE 407 (IL 336/IL 110) | | | | | | | | | | | | | | |
| 547+90.00 | 3 | | | 2 | | | 28 | 28 | | | | | | |
| 548+00.00 | 3 | | | | | | | | | | 20 | | | |
| 549+75.00 | 3 | | | 1 | | 28 | | | | | | | | |
| 552+00.00 | 6 | | 3 | | | 18 | | 14 | 14 | | | | | |
| 553+00.00 | 3 | | 1 | | | | | | | | 18 | | | |
| 555+90.00 | 9 | | 4 | | | 18 | | 14 | 14 | | 18 | | | |
| 560+50.00 | 9 | | 4 | | | 18 | | 14 | 14 | | 18 | | | |
| 571+00.00 | 9 | | 4 | | | 18 | | 14 | 14 | | 32 | | | |
| 575+00.00 | 6 | | 2 | | | 18 | | | | | 23 | | | |
| 576+25.00 | 3 | | 2 | | | | | 14 | | 25 | | | | |
| 579+75.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 586+87.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 591+50.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 596+00.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 599+90.00 | 3 | | | 1 | | | | | | | 36 | | | |
| 600+00.00 | 3 | | | 1 | | | | 28 | | | | | | |
| 604+00.00 | 6 | | 2 | | | | | 14 | | | 18 | | | |
| 608+50.00 | 3 | | 1 | | | | | | 14 | | | | | |
| 608+60.00 | 3 | | 1 | | | | | | | | 18 | | | |
| 613+50.00 | 6 | | 2 | | | | | 14 | | | 18 | | | |
| 618+50.00 | 6 | | 2 | | | | | 14 | | | 18 | | | |
| 623+50.00 | 6 | | 2 | | | | | 14 | | | 18 | | | |
| 628+50.00 | 6 | | 2 | | | | | 14 | | | 18 | | | |
| 633+42.00 | 3 | | 1 | | | | | | | | 18 | | | |
| 633+52.00 | 3 | | 1 | | | | | | 14 | | | | | |
| 634+55.00 | 3 | | 1 | | | | | | | | 18 | | | |
| 634+65.00 | 3 | | 1 | | | | | | 14 | | | | | |
| 647+00.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 652+00.00 | 3 | | 1 | | | | | | 14 | | | | | |
| 652+10.00 | 3 | | 1 | | | | | | | | 18 | | | |
| 657+00.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 662+00.00 | 3 | | 1 | | | | | | 14 | | | | | |
| 662+10.00 | 3 | | 1 | | | | | | | | 18 | | | |
| 667+00.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 671+90.00 | 3 | | | 1 | | | | | | | 36 | | | |
| 672+00.00 | 3 | | | 1 | | | | 28 | | | | | | |
| 676+00.00 | 6 | | 2 | | | | | 14 | | | 18 | | | |
| 680+29.00 | 3 | | 1 | | | | | | 14 | | | | | |
| 680+53.00 | 3 | | 1 | | | | | | | | 18 | | | |
| 682+41.00 | 3 | | 1 | | | | | | 14 | | | | | |
| 682+66.00 | 3 | | 1 | | | | | | | | 18 | | | |
| 687+00.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 691+00.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 697+00.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 702+12.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 703+28.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 713+68.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 718+68.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 723+68.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 728+68.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 733+00.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 736+00.00 | 6 | | 2 | | | 2 | | | 28 | | 36 | | | |
| 740+00.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 744+00.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 748+00.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 753+00.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 758+00.00 | 3 | | 1 | | | | | | | | 14 | | | |
| 758+10.00 | 3 | | 1 | | | | | | | | 18 | | | |
| 762+00.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 766+00.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 770+00.00 | 3 | | 1 | | | | | | 14 | | | | | |
| 770+10.00 | 3 | | 1 | | | | | | | | 18 | | | |
| 774+50.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |
| 778+75.00 | 3 | | 1 | | | | | | | | 18 | | | |
| 778+98.00 | 3 | | 1 | | | | | | | | 18 | | | |
| 779+94.00 | 3 | | 1 | | | | | | | | 18 | | | |
| 780+18.00 | 3 | | 1 | | | | | | | | 14 | | | |
| 789+50.00 | 6 | | 2 | | | | | | 14 | | 18 | | | |

PIPE UNDERDRAIN OUTLET SCHEDULE

| STATION | 28100125 | | 60100060 | | 60108100 | | | | | | | | | | | | |
|--------------------------------------|-----------------------|-------|------------------------------------|------------------------------------|-------------------------------|-----------|--------------|------------|-----------|------------|------------|--------------|--------------|------------|------------|------------|-----------|
| | STONE RIPRAP CLASS B3 | SQ YD | CONCRETE HEADWALLS FOR PIPE DRAINS | | PIPE UNDERDRAINS 4" (SPECIAL) | | | | | | | | | | | | |
| | | | SINGLE PIPE OPENING | DOUBLE PIPE OPENING (SAG LOCATION) | SB/ WB LANES | | NB/ EB LANES | | RAMPS | | US RTE 67 | | | | | | |
| | | | | | LT OUTSIDE | LT MEDIAN | RT MEDIAN | RT OUTSIDE | LT | RT | LT | RT | | | | | |
| | | EACH | | FOOT | | | | | | | | | | | | | |
| FAP ROUTE 407 (IL 336/IL 110) | | | | | | | | | | | | | | | | | |
| 794+00.00 | 3 | | 1 | | | | | | | | | 14 | | | | | |
| 794+10.00 | 3 | | 1 | | | | | | | | | | 18 | | | | |
| 798+00.00 | 6 | | 2 | | | | | | | | 14 | | 18 | | | | |
| 801+50.00 | 6 | | | 2 | | | | | | | 28 | | 36 | | | | |
| 806+00.00 | 3 | | 1 | | | | | | | | 14 | | | | | | |
| 806+50.00 | 3 | | 1 | | | | | | | | | | 18 | | | | |
| 811+00.00 | 3 | | 1 | | | | | | | | 14 | | | | | | |
| 811+50.00 | 3 | | 1 | | | | | | | | | | 18 | | | | |
| 819+60.00 | 6 | | 2 | | | | | | | | 14 | | 18 | | | | |
| 823+60.00 | 6 | | 2 | | | | | | | | 14 | | 18 | | | | |
| 827+60.00 | 3 | | 1 | | | | | | | | 14 | | | | | | |
| 828+00.00 | 3 | | 1 | | | | | | | | | | 18 | | | | |
| 831+60.00 | 3 | | 1 | | | | | | | | 14 | | | | | | |
| 833+00.00 | 3 | | 1 | | | | | | | | | | 18 | | | | |
| 835+60.00 | 3 | | 1 | | | | | | | | 14 | | | | | | |
| 839+40.00 | 6 | | | 2 | | | | | | | 28 | | 36 | | | | |
| 843+00.00 | 6 | | 2 | | | | | | | | 14 | | 18 | | | | |
| 847+00.00 | 6 | | 2 | | | | | | | | 14 | | 18 | | | | |
| 851+00.00 | 6 | | 2 | | | | | | | | 14 | | 18 | | | | |
| 860+00.00 | 6 | | 2 | | | | | | | | 14 | | 18 | | | | |
| 864+50.00 | 6 | | 2 | | | | | | | | 25 | | 18 | | | | |
| 869+00.00 | 6 | | 2 | | | | | | | 14 | 14 | | | | | | |
| 869+10.00 | 3 | | 2 | | | | | | 18 | | | | 18 | | | | |
| 874+00.00 | 9 | | 4 | | | | | | 18 | | 14 | | 14 | | | | |
| 878+90.00 | 6 | | | 2 | | | | | 60 | | | | | 60 | | | |
| 879+00.00 | 3 | | 2 | | | | | | | | 14 | | 14 | | | | |
| 881+00.00 | 3 | | 1 | | | | | | | | | | | 34 | | | |
| 883+90.00 | 6 | | 2 | | | | | | 45 | | | | | | 55 | | |
| 885+00.00 | 3 | | 2 | | | | | | | | 14 | | 14 | | | | |
| RAMP L | | | | | | | | | | | | | | | | | |
| 57+50.00 | 3 | | 1 | | | | | | | | | | | 18 | | | |
| 60+00.00 | 6 | | | 2 | | | | | | | | 36 | 36 | | | | |
| 65+00.00 | 6 | | 2 | | | | | | | | | 18 | 18 | | | | |
| 70+00.00 | 6 | | 2 | | | | | | | | | | 18 | 18 | | | |
| RAMP K | | | | | | | | | | | | | | | | | |
| 103+50.00 | 3 | | 1 | | | | | | | | | 18 | | | | | |
| 104+00.00 | 3 | | 1 | | | | | | | | | | 18 | | | | |
| 108+50.00 | 3 | | 1 | | | | | | | | | 18 | | 18 | | | |
| 109+00.00 | 3 | | 1 | | | | | | | | | | 18 | | | | |
| 113+50.00 | 3 | | 1 | | | | | | | | | 18 | | | | | |
| 114+00.00 | 3 | | 1 | | | | | | | | | | 18 | | | | |
| 118+40.00 | 3 | | | 1 | | | | | | | | 36 | | | | | |
| 188+80.00 | 3 | | | 1 | | | | | | | | | | 36 | | | |
| RAMP A | | | | | | | | | | | | | | | | | |
| 322+40.00 | 3 | | 1 | | | | | | | | | | | 18 | | | |
| 322+50.00 | 3 | | 1 | | | | | | | | | 18 | | | | | |
| 325+00.00 | 3 | | 1 | | | | | | | | | | 18 | | | | |
| 327+50.00 | 3 | | 1 | | | | | | | | | 18 | | | | | |
| 330+00.00 | 3 | | 1 | | | | | | | | | | 18 | | | | |
| RAMP D | | | | | | | | | | | | | | | | | |
| 213+50.00 | 3 | | 1 | | | | | | | | | 18 | | | | | |
| 214+00.00 | 3 | | 1 | | | | | | | | | | 18 | | | | |
| 218+00.00 | 3 | | | 1 | | | | | | | | 36 | | | | | |
| 219+00.00 | 3 | | | 1 | | | | | | | | | | 36 | | | |
| FAP ROUTE 310 (US RTE 67) | | | | | | | | | | | | | | | | | |
| 439+00.00 | 6 | | 2 | | | | | | | | | | | 18 | 18 | | |
| 442+93.60 | 6 | | | 2 | | | | | | | | | | 36 | 36 | | |
| 445+50.00 | 3 | | 1 | | | | | | | | | | | 18 | | | |
| 450+00.00 | 6 | | 2 | | | | | | | | | | | 18 | 18 | | |
| 454+25.00 | 3 | | 1 | | | | | | | | | | | 18 | | | |
| 460+50.00 | 3 | | 1 | | | | | | | | | | | 18 | | | |
| 461+30.00 | 3 | | 1 | | | | | | | | | | | 18 | | | |
| TOTAL | 558 | | 172 | | | | | | 23 | 259 | 154 | 1,100 | 1,484 | 252 | 288 | 126 | 90 |
| USE | 558 | | 195 | | | | | | | | | 3,753 | | | | | |

PIPE UNDERDRAINS SCHEDULE

| STATION TO STATION | | 60107600 | | | | | | | | | | |
|--------------------------------------|----|---------------------|----------------|----------------|-----------------|-----------------|----------------|----------------|----------------|----------------|----------------|----------------|
| | | PIPE UNDERDRAINS 4" | | | | | | | | | | |
| | | NB/ EB LANES | | SB/ WB LANES | | RAMPS | | US RTE 67 | | | | |
| | | LT OUTSIDE | LT MEDIAN | RT MEDIAN | RT OUTSIDE | LT | RT | LT OUTSIDE | LT MEDIAN | RT MEDIAN | RT OUTSIDE | |
| | | FOOT | | | | | | | | | | |
| FAP ROUTE 407 (IL 336/IL 110) | | | | | | | | | | | | |
| 541+57.14 | TO | 577+00.00 | 3,542.9 | 3,542.9 | | | | | | | | |
| 546+24.47 | TO | 579+75.00 | | | 3,350.5 | 3,350.5 | | | | | | |
| 586+87.00 | TO | 633+42.00 | | | | 4,655.0 | | | | | | |
| 586+87.00 | TO | 633+52.00 | | | 4,665.0 | | | | | | | |
| 634+55.00 | TO | 680+53.00 | | | | 4,598.0 | | | | | | |
| 634+65.00 | TO | 680+29.00 | | | 4,564.0 | | | | | | | |
| 682+41.00 | TO | 702+12.00 | | | 1,971.0 | | | | | | | |
| 682+66.00 | TO | 702+12.00 | | | | 1,946.0 | | | | | | |
| 703+28.00 | TO | 778+75.00 | | | | 7,547.0 | | | | | | |
| 703+28.00 | TO | 778+98.00 | | | 7,570.0 | | | | | | | |
| 779+94.00 | TO | 886+50.00 | | | | 10,656.0 | | | | | | |
| 780+18.00 | TO | 886+50.00 | | | 10,632.0 | | | | | | | |
| 864+50.00 | TO | 886+50.00 | 2,200.0 | 2,200.0 | | | | | | | | |
| RAMP L | | | | | | | | | | | | |
| 50+00.00 | TO | 72+04.23 | | | | | 2,204.2 | | | | | |
| 56+58.52 | TO | 72+04.23 | | | | 1,545.7 | | | | | | |
| RAMP K | | | | | | | | | | | | |
| 101+45.00 | TO | 119+61.56 | | | | 1,816.6 | | | | | | |
| 101+45.00 | TO | 129+67.15 | | | | | 2,822.2 | | | | | |
| RAMP A | | | | | | | | | | | | |
| 322+40.00 | TO | 345+42.02 | | | | | 2,302.0 | | | | | |
| 322+50.00 | TO | 334+73.72 | | | | 1,223.7 | | | | | | |
| RAMP D | | | | | | | | | | | | |
| 200+00.00 | TO | 219+00.00 | | | | | 1,900.0 | | | | | |
| 206+06.82 | TO | 218+00.00 | | | | 1,193.2 | | | | | | |
| FAP ROUTE 310 (US 67) | | | | | | | | | | | | |
| 438+46.31 | TO | 460+50.00 | | | | | | 2,283.7 | | | 2,203.7 | |
| 438+46.31 | TO | 461+30.00 | | | | | | | 638.0 | | | |
| 438+46.31 | TO | 444+84.29 | | | | | | | | 654.5 | | |
| 438+46.31 | TO | 445+00.76 | | | | | | | | | 634.3 | |
| 454+99.24 | TO | 461+50.00 | | | | | | | 650.8 | | | |
| 455+15.70 | TO | 461+50.00 | | | | | | | | | | |
| TOTAL | | | 5,742.9 | 5,742.9 | 32,752.5 | 32,752.5 | 5,779.2 | 9,228.4 | 2,283.7 | 1,288.8 | 1,288.8 | 2,203.7 |
| USE | | | | | | | | | | | | 99,064 |

CONCRETE CURB, GUTTER, MEDIANS & ISLANDS

| STATION | TO | STATION | SIDE | 31102100 | 42001300 | 60608552 | 60608582 | 60610400 | 60618300 | 60618320 | X6060097 | X6061902 | X6062400 | X6062700 |
|--------------------------------------|----|-----------|-------|--------------------------------------|-----------------|--------------------------------------|----------------|-------------|-------------------------|----------------|-------------------------------------|------------------------------------|---------------------------|----------------------------------|
| | | | | SUBBASE GRANULAR MATERIAL, TYPE C 4" | PROTECTIVE COAT | COMBINATION CONCRETE CURB AND GUTTER | | | CONCRETE MEDIAN SURFACE | | CLASS SI CONCRETE (OUTLET), SPECIAL | CONCRETE MEDIAN, TYPE SM (SPECIAL) | CONCRETE GUTTER (SPECIAL) | CONCRETE GUTTER TYPE A (SPECIAL) |
| | | | | SQ YD | | TYPE M-4.06 | TYPE M-4.24 | TYPE M-6.24 | 4 INCH | 6 INCH | CU YD | SQ FT | FOOT | |
| | | | | FOOT | | | SQ FT | | | | FOOT | | | |
| FAP ROUTE 407 (IL 336/IL 110) | | | | | | | | | | | | | | |
| 553+00.00 | TO | 555+15.00 | LT | | 126.5 | | | | | | | | | 214.3 |
| 553+47.47 | TO | 555+50.00 | RT | | 122.3 | | | | | | | | | 206.6 |
| 568+06.77 | TO | 569+89.26 | LT | | 115.8 | | | | | | | | | 182.3 |
| 568+60.85 | TO | 570+46.48 | RT | | 120.1 | | | | | | | | | 188.6 |
| RAMP D | | | | | | | | | | | | | | |
| 219+81.75 | TO | 220+06.88 | RT | | 36.4 | | | 61.4 | 161.5 | | | | | |
| FAP ROUTE 310 (US 67) | | | | | | | | | | | | | | |
| 435+24.79 | TO | 445+04.26 | LT/RT | 221.0 | 726.3 | | 1,972.2 | | | 1,014.3 | | | | |
| 445+35.93 | TO | 446+82.20 | LT | | 209.1 | 136.3 | 172.3 | | | 1,222.1 | | | | |
| 445+70.76 | TO | 447+13.89 | RT | | 126.2 | | | | | | 1,135.6 | | | |
| 447+13.89 | TO | 449+06.59 | RT | | 27.8 | 192.7 | | | | | | | | |
| 447+13.89 | TO | 450+93.40 | LT/RT | | 118.3 | | 380.1 | | | | | | | |
| 449+06.59 | TO | 452+86.10 | LT/RT | | 118.3 | | 380.1 | | | | | | | |
| 450+93.40 | TO | 452+86.10 | LT | | 27.8 | 192.7 | | | | | | | | |
| 452+86.10 | TO | 454+29.23 | LT | | 126.2 | | | | | | 1,135.7 | | | |
| 453+17.79 | TO | 454+64.18 | RT | | 209.1 | 136.4 | 172.3 | | | 1,221.8 | | | | |
| 448+07.46 | TO | 449+10.00 | RT | | 47.0 | | | | | | 10.0 | | | |
| 449+10.00 | TO | 456+40.00 | RT | | 275.8 | | | | | | | | | 730.0 |
| 456+40.00 | TO | 457+43.03 | RT | | 47.1 | | | | | | 10.0 | | | |
| 454+95.74 | TO | 455+45.16 | LT/RT | | 144.0 | | 110.0 | | | 987.8 | | | | |
| TOTAL | | | | 221.0 | 2,724.1 | 658.1 | 3,187.0 | 61.4 | 161.5 | 4,446.0 | 20.0 | 2,271.3 | 791.8 | 730.0 |
| USE | | | | 221 | 2,725 | 658.5 | 3,187.0 | 61.5 | 162 | 4,446 | 20.0 | 2,272 | 792.0 | 730.0 |

GUARDRAIL

| LOCATION (STATION TO STATION) | SIDE | 63000001 | 63100085 | 63100167 | 78200410 | 78200530 | 78201000 | 78201002 | |
|--------------------------------------|------|--|---|--|---------------------------------|------------------------------------|---|--|-------------------------|
| | | STEEL PLATE BEAM GUARDRAIL, TY A, 6 FOOT POSTS | TRAFFIC BARRIER TERMINAL, TYPE 6 | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT | GUARDRAIL MARKERS, TYPE A | BARRIER WALL MARKERS, TYPE C | TERMINAL MARKER - DIRECT APPLIED | GUARDRAIL AGGREGATE EROSION CONTROL | |
| | | | | | | | | | MONODIRECTIONAL CRYSTAL |
| | | FOOT | EACH | | | | | TON | |
| FAP ROUTE 407 (IL 336/IL 110) | | | | | | | | | |
| 576+07.09 TO 579+90.24 | RT | 287.5 | 1 | 1 | 4 | | 1 | 129.1 | |
| 578+95.01 TO 579+90.78 | MED | 0.0 | 1 | 1 | | | 1 | 39.7 | |
| LAMOINE RIVER BRIDGE | RT | | | | | 1 | | | |
| LAMOINE RIVER BRIDGE | MED | | | | | 3 | | | |
| 586+70.72 TO 588+29.05 | MED | 62.5 | 1 | 1 | 2 | | 1 | 61.2 | |
| 586+70.76 TO 589+67.62 | RT | 200.0 | 1 | 1 | 1 | | 1 | 105.3 | |
| 625+15.53 TO 626+90.53 | MED | 75.0 | | 2 | 4 | | 2 | 77.3 | |
| 628+26.66 TO 633+59.81 | RT | 437.5 | 1 | 1 | 4 | | 1 | 180.4 | |
| 631+61.06 TO 633+69.20 | MED | 112.5 | 1 | 1 | 1 | | 1 | 77.0 | |
| WILDLIFE CROSSING #1 BRIDGE | MED | | | | | 1 | | | |
| 634+37.71 TO 637+59.15 | RT | 225.0 | 1 | 1 | 1 | | 1 | 113.2 | |
| 634+47.11 TO 636+05.25 | MED | 62.5 | 1 | 1 | 2 | | 1 | 61.1 | |
| 674+92.20 TO 680+70.43 | RT | 475.0 | 1 | 1 | 4 | | 1 | 187.4 | |
| 678+66.30 TO 680+46.92 | MED | 87.5 | 1 | 1 | | | 1 | 76.8 | |
| CH 14 BRIDGE | RT | | | | | 1 | | | |
| CH 14 BRIDGE | MED | | | | | 2 | | | |
| 682+23.31 TO 683+82.06 | MED | 62.5 | 1 | 1 | 2 | | 1 | 61.3 | |
| 682+48.48 TO 685+60.72 | RT | 212.5 | 1 | 1 | | | 1 | 110.3 | |
| 697+94.87 TO 702+31.24 | RT | 337.5 | 1 | 1 | 4 | | 1 | 149.7 | |
| 700+50.25 TO 702+31.59 | MED | 87.5 | 1 | 1 | 1 | | 1 | 68.5 | |
| WILDLIFE CROSSING #2 BRIDGE | MED | | | | | 1 | | | |
| 703+08.41 TO 704+64.65 | MED | 62.5 | 1 | 1 | 2 | | 1 | 60.5 | |
| 703+08.76 TO 705+67.81 | RT | 162.5 | 1 | 1 | 1 | | 1 | 93.3 | |
| 730+73.69 TO 732+48.17 | MED | 75.0 | | 2 | 4 | | 2 | 75.1 | |
| 774+08.85 TO 778+91.99 | RT | 387.5 | 1 | 1 | 4 | | 1 | 164.5 | |
| 776+81.94 TO 779+15.09 | MED | 137.5 | 1 | 1 | 1 | | 1 | 85.0 | |
| WILDLIFE CROSSING #3 BRIDGE | MED | | | | | 1 | | | |
| 779+76.99 TO 783+35.14 | RT | 262.5 | 1 | 1 | 1 | | 1 | 124.7 | |
| 780+00.09 TO 781+70.73 | MED | 75.0 | 1 | 1 | 2 | | 1 | 65.1 | |
| 802+78.26 TO 804+54.15 | MED | 75.0 | | 2 | 4 | | 2 | 77.6 | |
| 841+01.69 TO 842+76.69 | MED | 75.0 | | 2 | 4 | | 2 | 77.3 | |
| 950E (EMORY RD) | | | | | | | | | |
| 3004+98.48 TO 3010+54.13 | LT | 462.5 | 1 | 1 | 2 | | 1 | 142.8 | |
| 3006+03.30 TO 3010+71.45 | RT | 375.0 | 1 | 1 | 4 | | 1 | 119.1 | |
| 950E BRIDGE | RT | | | | | 1 | | | |
| 3013+23.51 TO 3016+31.66 | LT | 212.5 | 1 | 1 | 4 | | 1 | 77.5 | |
| 3013+42.94 TO 3017+01.09 | RT | 262.5 | 1 | 1 | 1 | | 1 | 90.7 | |
| 3017+15.72 TO 3021+28.22 | LT | 312.5 | | 2 | 4 | | | 110.6 | |
| 3018+00.00 TO 3021+25.00 | RT | 225.0 | | 2 | 4 | | | 91.2 | |
| 1500N (CH 20) | | | | | | | | | |
| 5003+95.42 TO 5009+03.57 | RT | 387.5 | 1 | 1 | 4 | | 1 | 131.2 | |
| 5004+01.47 TO 5009+34.62 | LT | 437.5 | 1 | 1 | 2 | | 1 | 137.3 | |
| 1500N (CH 20) BRIDGE | RT | | | | | 1 | | | |
| 5012+13.92 TO 5018+22.07 | RT | 512.5 | 1 | 1 | 1 | | 1 | 153.3 | |
| 5012+44.98 TO 5020+03.13 | LT | 662.5 | 1 | 1 | 1 | | 1 | 191.7 | |
| 1100E | | | | | | | | | |
| 6010+02.16 TO 6017+85.31 | LT | 687.5 | 1 | 1 | 2 | | 1 | 197.8 | |
| 6010+39.66 TO 6017+85.31 | RT | 650.0 | 1 | 1 | 5 | | 1 | 186.3 | |
| 1100E BRIDGE | LT | | | | | 1 | | | |
| 6020+22.31 TO 6023+92.96 | RT | 275.0 | 1 | 1 | 1 | | 1 | 96.4 | |
| 6020+22.31 TO 6025+05.46 | LT | 387.5 | 1 | 1 | 4 | | 1 | 125.2 | |
| TOTAL | | 9,887.5 | 32 | 44 | 92 | 13 | 40 | 4172.5 | |
| USE | | 9,888 | 32 | 44 | 92 | 13 | 40 | 4,173 | |

IMPACT ATTENUATORS - PIER PROTECTION

| STATION TO STATION | SIDE | 64300450 | 64301090 |
|--------------------------------------|------|---|--------------------|
| | | IMPACT ATTENUATOR (NON-REDIRECTIVE), TEST LEVEL 3 | ATTENUATOR BASE |
| | | | EACH |
| FAP ROUTE 407 (IL 336/IL 110) | | | |
| 553+99.30 TO 554+32.50 | MED | 1 | 27.3 |
| 554+49.29 TO 554+82.52 | MED | 1 | 27.3 |
| 568+33.86 TO 568+67.06 | MED | 1 | 27.3 |
| 569+64.86 TO 569+98.10 | MED | 1 | 27.3 |
| TOTAL | | 4 | 109.2 |
| USE | | 4 | 110 |

TEMPORARY CONCRETE BARRIER FOR LANE DROP

| STATION TO STATION | SIDE | 64300260 | 78200530 | X0325349 |
|--------------------------------------|------|--|---------------------------------|---|
| | | IMPACT ATTENUATOR (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3 | BARRIER WALL MARKERS, TYPE C | TEMPORARY CONCRETE BARRIER (TO REMAIN PERMANENTLY) |
| | | | EACH | |
| | | EACH | EACH | FOOT |
| FAP ROUTE 407 (IL 336/IL 110) | | | | |
| 558+83.17 TO 559+00.00 | RT | 1 | | |
| 559+00.00 TO 576+13.41 | RT | | 140 | 1719.0 |
| 576+13.41 TO 576+30.24 | RT | 1 | | |
| TOTAL | | 2 | 140 | 1719.0 |
| USE | | 2 | 140 | 1725 |

GUARDRAIL REMOVAL

| LOCATION (STATION TO STATION) | SIDE | 63200310 |
|----------------------------------|------|------------------------------|
| | | GUARDRAIL REMOVAL FOOT |
| 950E (EMORY RD) | | |
| 3004+98.48 TO 3010+54.13 | LT | 150.0 |
| TOTAL | | 150.0 |
| USE | | 150 |

DELINEATORS

| LOCATION | 63500105 |
|--|-------------|
| | DELINEATORS |
| STATION TO STATION | EACH |
| FAP ROUTE 407 (ILL 336/ IL 110) | |
| 542+00 TO 562+00 (SB) | 6 |
| 547+00 TO 565+00 (NB) | 5 |
| 571+00 TO 873+00 (SB) | 75 |
| 574+00 TO 877+00 (NB) | 76 |
| RAMP L | |
| 52+00 TO 58+00 RT | 7 |
| 57+00 TO 67+26 LT | 21 |
| RAMP K | |
| 104+00 TO 119+50 LT | 26 |
| 118+70 TO 129+00 RT | 11 |
| RAMP D | |
| 200+55 TO 207+55 RT | 15 |
| 207+55 TO 212+55 LT | 7 |
| 215+59 TO 218+33 RT | 6 |
| RAMP A | |
| 323+17 TO 326+42 RT | 6 |
| 329+08 TO 332+28 LT | 5 |
| 332+92 TO 344+92 RT | 13 |
| TOTAL | 279 |

SHOULDER RUMBLE STRIPS, 16 INCH

| LOCATION (STATION TO STATION) | LANES | SIDE | 64200116 |
|--------------------------------------|-------|---------|---------------------------------------|
| | | | SHOULDER RUMBLE STRIPS, 16 INCH |
| | | | FOOT |
| FAP ROUTE 407 (IL 336/IL 110) | | | |
| 541+57.14 TO 569+67.31 | SB/WB | RT | 2,810.20 |
| 542+59.71 TO 562+95.72 | SB/WB | LT | 2,036.00 |
| 569+60.02 TO 577+00.00 | SB/WB | LT | 740.00 |
| 573+42.83 TO 577+00.00 | SB/WB | RT | 357.20 |
| 546+24.47 TO 576+30.25 | NB/EB | LT | 3,005.80 |
| 547+27.26 TO 564+99.42 | NB/EB | RT | 1,772.20 |
| 577+26.23 TO 578+73.93 | NB/EB | LT | 147.70 |
| 587+87.54 TO 632+47.48 | NB/EB | LT & RT | 8,919.90 |
| 635+60.38 TO 679+39.04 | NB/EB | LT & RT | 8,757.40 |
| 683+53.79 TO 701+11.10 | NB/EB | LT & RT | 3,514.60 |
| 704+28.90 TO 777+87.60 | NB/EB | LT & RT | 14,717.40 |
| 781+06.79 TO 858+88.02 | NB/EB | LT & RT | 15,562.50 |
| 858+88.42 TO 877+82.29 | NB/EB | RT | 1,894.30 |
| 864+49.23 TO 886+50.00 | NB/EB | LT | 2,200.80 |
| 864+50.00 TO 867+52.39 | SB/WB | LT & RT | 604.80 |
| 867+52.39 TO 870+52.59 | SB/WB | LT | 300.20 |
| 870+52.59 TO 873+21.59 | SB/WB | ST & RT | 538.00 |
| 873+21.59 TO 886+50.00 | SB/WB | RT | 1,328.40 |
| 884+42.29 TO 886+50.00 | NB/EB | RT | 207.70 |
| 885+92.62 TO 886+50.00 | SB/WB | LT | 57.40 |
| FAP ROUTE 310 (US 67) | | | |
| 429+64.16 TO 430+32.62 | NB | RT | 68.5 |
| 429+64.16 TO 433+20.39 | SB | LT & RT | 712.5 |
| 433+20.39 TO 433+94.76 | SB | RT | 74.4 |
| 435+63.63 TO 435+97.71 | NB | LT | 34.1 |
| 435+97.71 TO 445+00.76 | NB | LT & RT | 1,806.1 |
| 439+90.70 TO 443+21.10 | SB | LT & RT | 660.8 |
| 443+21.10 TO 444+82.08 | SB | RT | 161.0 |
| 445+00.76 TO 460+47.83 | NB | RT | 1,547.1 |
| 445+04.49 TO 454+86.10 | SB | LT | 981.6 |
| 445+38.43 TO 446+74.71 | SB | RT | 136.3 |
| 445+78.37 TO 448+44.88 | NB | LT | 266.5 |
| 451+55.11 TO 454+21.65 | SB | RT | 266.5 |
| 453+25.28 TO 454+61.68 | NB | LT | 136.4 |
| 454+99.24 TO 461+50.00 | SB | RT | 650.8 |
| 455+15.70 TO 461+50.00 | NB | LT | 634.3 |
| TOTAL | | | 77,609.4 |
| USE | | | 77,610 |

FURNISHING AND ERECTING RIGHT OF WAY MARKERS

| STATION | OFFSET FOOT | SIDE | 66600105 |
|------------------------------|----------------|------|---|
| | | | FURNISHING AND ERECTING RIGHT OF WAY MARKERS |
| | | | EACH |
| 1400N (TOWER RD) | | | |
| 10+75.00 | 50.00 | RT | 1 |
| 10+75.00 | 50.00 | LT | 1 |
| 12+61.00 | 45.00 | RT | 1 |
| 12+61.00 | 45.00 | LT | 1 |
| 15+65.29 | 45.00 | LT | 1 |
| 15+65.29 | 45.00 | RT | 1 |
| 17+58.04 | 55.00 | LT | 1 |
| 17+58.04 | 50.00 | RT | 1 |
| 26+20.22 | 55.00 | LT | 1 |
| 26+20.22 | 50.00 | RT | 1 |
| 27+00.00 | 55.00 | LT | 1 |
| 27+00.00 | 50.00 | RT | 1 |
| 28+00.00 | 35.98 | LT | 1 |
| 28+00.00 | 35.72 | RT | 1 |
| FAP ROUTE 310 (US 67) | | | |
| 448+60.21 | 89.03 | RT | 1 |
| 453+00.00 | 89.93 | RT | 1 |
| 460+27.34 | 100.28 | RT | 1 |
| 460+42.64 | 129.37 | RT | 1 |
| 461+34.71 | 129.61 | RT | 1 |
| 461+50.02 | 100.63 | RT | 1 |
| TOTAL | | | 20 |
| USE | | | 20 |

| FENCE SCHEDULE | | | |
|--------------------------------------|-------|-----------------------|----------------------|
| LOCATION | SIDE | 66400510 | 66500105 |
| | | CHAIN LINK FENCE, 10' | WOVEN WIRE FENCE, 4' |
| | | FOOT | |
| FAP ROUTE 407 (IL 336/IL 110) | | | |
| STA 580+09.0 TO STA 580+09.0 | RT | | 78 |
| STA 586+82.2 TO STA 586+82.2 | LT/RT | | 79 |
| STA 633+26.4 TO STA 633+69.6 | RT | | 106 |
| STA 633+76.6 TO STA 633+84.7 | LT/RT | | 128 |
| STA 634+22.5 TO STA 634+80.8 | RT | | 106 |
| STA 634+37.3 TO STA 634+89.4 | RT/LT | | 134 |
| STA 676+00.0 TO STA 679+70.3 | LT | | 409 |
| STA 679+70.3 TO STA 680+59.5 | LT/RT | | 161 |
| STA 681+94.6 TO STA 682+70.7 | RT | | 136 |
| STA 680+83.1 TO STA 682+70.7 | RT | | 278 |
| STA 681+52.6 TO STA 682+09.8 | LT/RT | | 140 |
| STA 681+52.6 TO STA 682+99.6 | LT | | 163 |
| STA 682+35.9 TO STA 684+30.7 | RT | | 262 |
| STA 700+00.0 TO STA 702+42.2 | LT/RT | | 342 |
| STA 700+00.0 TO STA 702+41.8 | RT | | 257 |
| STA 702+97.8 TO STA 704+86.8 | RT/LT | | 303 |
| STA 702+98.2 TO STA 707+00.0 | RT | | 427 |
| STA 728+98.6 TO STA 729+79.4 | RT | | 91 |
| STA 729+69.9 TO STA 731+12.5 | LT | | 151 |
| STA 730+34.8 TO STA 730+65.9 | RT | | 55 |
| STA 731+69.6 TO STA 732+55.1 | LT | | 104 |
| STA 777+38.2 TO STA 778+99.0 | RT | | 189 |
| STA 779+00.0 TO STA 779+33.6 | LT/RT | | 188 |
| STA 779+59.3 TO STA 780+11.2 | RT | | 60 |
| STA 779+93.1 TO STA 781+70.0 | RT/LT | | 253 |
| STA 800+01.6 TO STA 801+69.1 | LT | | 176 |
| STA 801+79.9 TO STA 801+69.1 | RT | | 177 |
| STA 802+19.5 TO STA 803+61.8 | LT | | 157 |
| STA 804+12.4 TO STA 806+85.1 | RT | | 268 |
| STA 839+80.3 TO STA 840+79.4 | LT | | 102 |
| STA 839+69.5 TO STA 840+78.8 | RT | | 116 |
| STA 841+31.1 TO STA 842+38.0 | LT | | 113 |
| STA 841+30.4 TO STA 842+38.0 | RT | | 111 |
| COUNTY HIGHWAY 14 | | | |
| STA 2014+00.0 TO STA 2014+72. | RT | | 73 |
| STA 2014+72.7 TO STA 2017+02. | RT | | 230 |
| STA 2017+02.3 TO STA 2020+00. | RT | | 298 |
| STA 2020+00.0 TO STA 2020+16. | RT | | 41 |
| STA 2020+16.4 TO STA 2022+51. | RT | | 237 |
| STA 2022+51.6 TO STA 2024+04. | RT | | 153 |
| FAP ROUTE 310 (US 67) | | | |
| STA 435+47.8 TO STA 443+69.8 | LT | | 818 |
| TOTAL | | 5,985 | 1,685 |
| USE | | 5,985 | 1,685 |

| US 67 STAGING - TEMPORARY PAVEMENT MARKING | | | | | | | | | | |
|--|------|-------------------|----------------------------|--------------------|-----------------|----------------|-----------------|--------------------------------|-----------------|------------------------------------|
| STATION TO STATION | SIDE | 70300210 | 70300220 | | | | 70300260 | 70300520 | | 70301000 |
| | | LETTERS & SYMBOLS | TEMPORARY PAVEMENT MARKING | | | | WHITE SOLID 12" | PAVEMENT MARKING TAPE TYPE III | | WORK ZONE PAVEMENT MARKING REMOVAL |
| | | | WHITE SOLID 4" | WHITE SKIP DASH 4" | YELLOW SOLID 4" | WHITE SOLID 4" | | WHITE SOLID 4" | YELLOW SOLID 4" | |
| SQ FT | FOOT | | | | | FOOT | | SQ FT | | |
| FAP ROUTE 310 (US 67) STAGE 1 | | | | | | | | | | |
| 420+76.0 TO 431+97.4 | RT | | 1,122 | | | | | | 374.0 | |
| 428+65.5 TO 430+22.9 | RT | | | 40 | | | | | 13.3 | |
| 430+22.9 | RT | 36.4 | | | | | | | 36.4 | |
| 430+22.9 TO 434+38.8 | RT | | 832 | | | 85 | | | 362.3 | |
| 434+98.0 TO 435+99.4 | RT | | 201 | | | | | | 67.0 | |
| 435+99.4 TO 463+70.0 | RT | | 2,771 | | | | | | 923.7 | |
| 435+76.2 TO 463+70.0 | RT | | | | | 2,794 | | | 931.3 | |
| 452+07.0 TO 457+88.0 | LT | | | | | 581 | | | 193.7 | |
| FAP ROUTE 310 (US 67) STAGE 2 | | | | | | | | | | |
| 414+50.0 TO 418+67.5 | RT | | | | | 418 | | | 139.3 | |
| 418+67.5 TO 422+77.7 | RT | | | | | 411 | | | 137.0 | |
| 418+67.5 TO 419+42.7 | RT | | | 20 | | | | | 6.7 | |
| 419+42.7 | RT | 36.4 | | | | | | | 36.4 | |
| 419+42.7 TO 422+83.0 | RT | | 680 | | | 85 | | | 311.7 | |
| 423+21.2 TO 432+48.1 | RT | | | | | 948 | | | 316.0 | |
| 430+64.9 TO 431+56.1 | RT | | | 24 | | | | | 8.0 | |
| 431+51.1 TO 434+38.8 | RT | | 566 | | | 68 | | | 256.7 | |
| 431+56.1 | RT | 36.4 | | | | | | | 36.4 | |
| 434+97.7 TO 435+79.8 | RT | | | | | | 178 | | 59.3 | |
| 435+21.6 TO 435+50.7 | RT | | | | | | | 47 | 15.7 | |
| 435+22.7 TO 436+92.6 | LT | | 510 | | | 51 | | | 221.0 | |
| 435+50.7 TO 464+35.0 | RT | | | | | | | 2,885 | 961.7 | |
| 435+79.8 TO 464+35.0 | RT | | | | | | 2,855 | | 951.7 | |
| 436+10.6 TO 470+70.4 | LT | | | | | 3,460 | | | 1,153.3 | |
| 436+92.6 | LT | 36.4 | | | | | | | 36.4 | |
| 436+92.6 TO 438+21.2 | LT | | | 64 | | | | | 21.3 | |
| 438+21.7 TO 465+11.0 | LT | | 2,690 | | | | | | 896.7 | |
| 470+70.4 TO 471+00.0 | LT | | | | | 54 | | | 18.0 | |
| 471+75.5 TO 473+27.1 | LT | | 304 | | | 152 | 10 | | 162.0 | |
| 473+27.1 TO 474+24.0 | LT | | | 24 | | | | | 8.0 | |
| 473+27.1 TO 482+07.0 | LT | | | | | 880 | | | 293.3 | |
| 473+34.6 | LT | 36.4 | | | | | | | 36.4 | |
| FAP ROUTE 310 (US 67) STAGE 3 | | | | | | | | | | |
| 435+22.0 TO 437+15.2 | LT | | 388 | | | 51 | | | 180.3 | |
| 435+28.0 TO 437+15.2 | LT | | 188 | | | | | | 62.7 | |
| 436+44.6 TO 438+21.5 | LT | | 179 | | | | | | 59.7 | |
| 437+15.2 | LT | 36.4 | | | | | | | 36.4 | |
| 437+15.2 TO 438+21.5 | LT | | | 56 | | | | | 18.7 | |
| 438+21.5 TO 463+87.5 | LT | | | | | | | 2,566 | 855.3 | |
| 438+21.5 TO 482+00.0 | LT | | | | | | | 4,379 | 1,459.7 | |
| SUBTOTAL | | 218.4 | 10,431 | 228 | 9,698 | 350 | 7,412 | 5,498 | 11,657.4 | |
| TOTAL | | 219 | 20,357 | | | 350 | 12,910 | | 11,658 | |

| TEMPORARY PAVEMENT MARKING SCHEDULE | | | | | | | | | | | | | |
|---|---------|---------------------|----------------|---------------|------------------|-----------------|----------------|----------------|---------------|---------------|---------------|------------------------------------|-----------------|
| LOCATION | LENGTH | 70300210 | 70300220 | | | | 70300240 | 70300250 | 70300260 | | 70300280 | 70301000 | |
| | | LETTERS AND SYMBOLS | LINE 4" | | | | LINE 6" | LINE 8" | LINE 12" | | LINE 24" | WORK ZONE PAVEMENT MARKING REMOVAL | |
| | | | WHITE SOLID | YELLOW SOLID | YELLOW SKIP DASH | DOUBLE YELLOW | | | WHITE SOLID | WHITE SOLID | | | WHITE SOLID |
| STATION TO STATION | | SQ FT | FOOT | | | | | FOOT | | SQ FT | | | |
| FAP ROUTE 310 (US 67) NORTHBOUND | | | | | | | | | | | | | |
| STA. 429+64.16 TO STA. 462+76.74 | 3312.64 | 187.20 | 3215.53 | | | | 3709.72 | 783.91 | 1201.50 | 434.64 | 187.85 | 28.00 | 4,367.04 |
| FAP ROUTE 310 (US 67) SOUTHBOUND | | | | | | | | | | | | | |
| STA. 429+64.16 TO STA. 462+76.74 | 3312.64 | 156.00 | 3541.71 | | | | 3341.78 | 789.34 | 1691.62 | 407.71 | 244.86 | 28.00 | 4,681.47 |
| COUNTY HIGHWAY 20 | | | | | | | | | | | | | |
| STA. 5000+50.00 TO STA. 5031+00.00 | 3050.00 | | 1656.91 | 691.73 | 283.09 | 6100.00 | | | | | | | 2,910.55 |
| STA. 5042+00.00 TO STA. 5051+00.00 | 900.00 | | | 225.00 | | 1889.35 | | | | | 22.00 | | 748.78 |
| AT US 67 INTERSECTION | 442.50 | | | 80.00 | | | | | | | | | 26.67 |
| 1400N (TOWER RD) | | | | | | | | | | | | | |
| STA. 10+00.00 TO STA. 28+00.00 | 1800.00 | | | | | 1775.35 | | | | | | 26.00 | 643.78 |
| SUBTOTAL | | 343.20 | 8414.15 | 996.73 | 2058.44 | 15040.85 | 1573.25 | 2893.12 | 842.35 | 432.71 | 104.00 | | 13378.28 |
| TOTAL | | 343.2 | 26510.2 | | | 1573.2 | 2893.1 | 1275.1 | | 104.0 | | | 13378.3 |
| USE | | 344 | 26,511 | | | 1,573 | 2,894 | 1,276 | | 104 | | | 13,379 |

PAVEMENT MARKING REMOVAL

| STATION TO STATION | SIDE | DESCRIPTION | 78300100 |
|---|-------|--|--------------------------|
| | | | PAVEMENT MARKING REMOVAL |
| | | | SQ FT |
| FAP ROUTE 407 (IL 336/IL 110) | | | |
| 537+50.0 TO 549+87.0 | RT | REMOVE EXISTING NB LANE DROP EDGE LINE TO GORE END | 622.5 |
| 541+59.5 TO 545+68.4 | LT | REMOVE EXISTING SB EDGE LINE ALONG ENTRANCE RAMP | 136.4 |
| RAMP K | | | |
| | RT/LT | REMOVE EXISTING EB US 136 EDGE LINE | 56.5 |
| | LT | REMOVE EXISTING WB US 136 EDGE LINE ALONG TURN BAY ONTO RAMP K | 155.6 |
| RAMP L | | | |
| | RT/LT | REMOVE EXISTING EB US 136 EDGE LINE | 66.9 |
| FAP ROUTE 310 (US 67) PRIOR TO STAGE 1 | | | |
| 420+76.0 TO 430+76.0 | RT | REMOVE EXISTING SKIP-DASH | 125.0 |
| 435+76.2 TO 463+70.0 | RT | REMOVE EXISTING EDGE LINE | 931.3 |
| FAP ROUTE 310 (US 67) PRIOR TO STAGE 2 | | | |
| 414+50.0 TO 424+50.0 | RT | REMOVE EXISTING SKIP-DASH | 125.0 |
| 430+64.9 TO 431+56.1 | RT | REMOVE EXISTING SKIP-DASH | 15.0 |
| 434+32.8 TO 435+50.7 | RT | REMOVE EXISTING SKIP-DASH | 15.0 |
| 435+21.4 TO 437+27.6 | LT | REMOVE SOLID LINE OF LT TURN LANE | 103.1 |
| 435+21.4 TO 437+68.8 | LT | REMOVE SOLID LINE OF RT TURN LANE | 123.7 |
| 435+21.4 TO 438+46.31 | LT | REMOVE EXISTING SKIP-DASH | 45.0 |
| 437+06.0 | LT | REMOVE ARROW IN RT TURN LANE | 15.6 |
| 437+68.7 | LT | REMOVE ARROW IN RT TURN LANE | 15.6 |
| 438+21.2 TO 465+11.0 | LT | REMOVE EXISTING EDGE LINE | 896.6 |
| 461+50.0 TO 464+35.0 | RT | REMOVE EXISTING EDGE LINE | 95.0 |
| 471+76.7 TO 482+07.0 | LT | REMOVE EXISTING SKIP-DASH | 130.0 |
| TOTAL | | | 3,673.8 |
| USE | | | 3,674 |

PAVEMENT MARKING SCHEDULE

| LOCATION | LENGTH | 78009000 | 78009004 | | | | | 78009006 | 78009008 | | 78009012 | | 78009024 | 78100100 | | | 78100105 | | 78200300 | | X7830070 | X7830074 |
|---|-----------|-------------------|------------------------------------|-----------------|------------------|------------------|-----------------|-----------------|---------------|-----------------|---------------|---------------|-----------------------------------|------------------|----------------|--|----------------|--------------------------|------------------|---|---|----------------|
| | | LETTERS & SYMBOLS | MODIFIED URETHANE PAVEMENT MARKING | | | | | | | | | | RAISED REFLECTIVE PAVEMENT MARKER | | | RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) | | PRISMATIC CURB REFLECTOR | | GROOVING FOR RECESSED PAVEMENT MARKING 5" | GROOVING FOR RECESSED PAVEMENT MARKING 7" | |
| | | | WHITE SOLID | YELLOW SOLID | YELLOW SKIP DASH | DOUBLE YELLOW | WHITE SOLID | WHITE SKIP DASH | WHITE SOLID | WHITE SKIP DASH | YELLOW SOLID | WHITE SOLID | WHITE SOLID | CRYSTAL, ONE-WAY | AMBER, ONE-WAY | AMBER, TWO-WAY | AMBER, ONE-WAY | AMBER, TWO-WAY | CRYSTAL, ONE-WAY | | | AMBER, ONE-WAY |
| STATION TO STATION | FOOT | SQ FT | FOOT | | | | | | | | | | EACH | | | | | | FOOT | | | |
| FAP ROUTE 407 (IL 336/IL 110) NORTHBOUND | | | | | | | | | | | | | | | | | | | | | | |
| STA. 537+50.00 TO STA. 886+50.00 | 34,900.00 | 34.50 | 5,969.79 | 6,352.50 | 8,704.28 | 31,711.39 | 316.08 | 1,508.32 | | 323.04 | | | 32 | 18 | 367 | 4 | 32 | | | 33,703.99 | 316.08 | |
| FAP ROUTE 407 (IL 336/IL 110) SOUTHBOUND | | | | | | | | | | | | | | | | | | | | | | |
| STA. 540+40.40 TO STA. 886+50.00 | 34,609.60 | | 5,750.66 | | | 32,351.11 | 874.55 | 1,594.43 | | | | | 104 | 34 | | 4 | | | | 33,891.63 | 874.55 | |
| RAMP L | | | | | | | | | | | | | | | | | | | | | | |
| STA. 50+55.75 TO STA. 73+31.70 | 2,275.95 | 24.00 | 1,719.07 | | | 2,394.82 | | 579.26 | 74.96 | | 78.81 | 32.00 | 40 | 28 | | | | 8 | | 4,113.89 | | |
| RAMP K | | | | | | | | | | | | | | | | | | | | | | |
| STA. 100+32.40 TO STA. 130+17.42 | 2,985.02 | 46.80 | 2,501.55 | | | 3,069.06 | | 549.08 | 182.99 | | | | | | | | | | | 5,131.53 | | |
| RAMP D | | | | | | | | | | | | | | | | | | | | | | |
| STA. 200+55.72 TO STA. 220+14.88 | 1,959.16 | | 1,354.21 | | | 2,073.37 | | 495.28 | | 124.21 | 32.00 | 57 | 15 | | | | | 11 | | 3,427.58 | | |
| RAMP A | | | | | | | | | | | | | | | | | | | | | | |
| STA. 321+07.18 TO STA. 343+92.00 | 2,284.82 | | 1,287.63 | | | 2,308.06 | | 406.39 | 174.98 | | 79.43 | | | | | | | | | 3,675.12 | | |
| FAP ROUTE 310 (US 67) NORTHBOUND | | | | | | | | | | | | | | | | | | | | | | |
| STA. 429+64.16 TO STA. 461+50.00 | 3,312.64 | 187.20 | 3,215.53 | | | 3,709.72 | 783.91 | 1,201.50 | | 434.64 | 187.85 | 28.00 | 69 | | | | | | 95 | 6,925.25 | 783.91 | |
| FAP ROUTE 310 (US 67) SOUTHBOUND | | | | | | | | | | | | | | | | | | | | | | |
| STA. 429+64.16 TO STA. 461+50.00 | 3,312.64 | 156.00 | 3,541.71 | | | 3,341.78 | 789.34 | 1,691.62 | | 407.71 | 244.86 | 28.00 | 76 | | | | | | 96 | 6,883.49 | 789.34 | |
| COUNTY HIGHWAY 20 | | | | | | | | | | | | | | | | | | | | | | |
| STA. 5000+50.00 TO STA. 5031+00.00 | 3,050.00 | | 1,656.91 | 691.73 | 283.09 | 6,100.00 | | | | | | | | | | | | | | | | |
| STA. 5042+00.00 TO STA. 5051+00.00 | 900.00 | | | 225.00 | | 1,889.35 | | | | | 22.00 | | | | | | | | | | | |
| AT US 67 INTERSECTION | 442.50 | | | 80.00 | | | | | | | | | | | | | | | | | | |
| 1400N (TOWER RD) | | | | | | | | | | | | | | | | | | | | | | |
| STA. 10+00.00 TO STA. 28+00.00 | 1,800.00 | | | | 1,775.35 | | | | | | | 26.00 | | | | | | | | | | |
| SUBTOTAL | | 448.50 | 26,997.08 | 7,349.22 | 10,762.72 | 88,948.67 | 2,763.89 | 8,025.89 | 432.92 | 1,165.39 | 715.16 | 168.00 | 378 | 95 | 367 | 8 | 32 | 19 | 191 | 97,752.48 | 2,763.89 | |
| TOTAL | | 449 | | 134,058 | | | 2,764 | 8,459 | | 1,881 | 168 | | 840 | | 40 | | 210 | | 97,753 | 2,764 | | |

PERMANENT SURVEY MARKER SCHEDULE

| LOCATION | 66700205 | 66700305 | X6670105 | X6670109 |
|--|--------------------------|----------|-----------|-----------------------|
| | PERMANENT SURVEY MARKERS | | | |
| | TYPE I | TYPE II | (SPECIAL) | PERMANENT SURVEY TIES |
| EACH | | | | |
| FAP ROUTE 407 (IL 336/IL 110) | | | | |
| SOUTHEAST ABUTMENT OF LAMOINE RIVER BRIDGE (055-0046) | | 1 | | |
| SOUTHEAST WINGWALL OF WILDLIFE CROSSING #1 BRIDGE (055-0068) | | 1 | | |
| SOUTHEAST WINGWALL OF COUNTY HIGHWAY 14 BRIDGE (055-0070) | | 1 | | |
| SOUTHEAST WINGWALL OF WILDLIFE CROSSING #2 BRIDGE (055-0072) | | 1 | | |
| SOUTHEAST ABUTMENT OF 950E (EMORY RD) BRIDGE (055-0074) | | 1 | | |
| SOUTHEAST WINGWALL OF WILDLIFE CROSSING #3 BRIDGE (055-0075) | | 1 | | |
| SOUTHEAST ABUTMENT OF COUNTY HIGHWAY 20 BRIDGE (055-0077) | | 1 | | |
| SOUTHEAST WINGWALL OF 1100E BRIDGE (055-0078) | | 1 | | |
| COUNTY HIGHWAY 20 | | | | |
| STA 5018+89.40, 0.08' RT | | | | 4 |
| STA 5045+95.26, 0.10' LT | | | | 4 |
| FAP ROUTE 310 (US 67) | | | | |
| POT STA 419+73.76, 0' RT | | | 1 | 4 |
| POT STA 429+70.00, 0' RT | | | 1 | 4 |
| STA 434+78.58, 42.31' LT | | | | 4 |
| PC STA 437+88.75, 0' RT | | | 1 | 4 |
| PI STA 441+35.48, 4.26' RT | | | 1 | 4 |
| PT STA 444+82.08, 0' RT | 1 | | | 4 |
| PC STA 455+39.36, 0' RT | 1 | | | 4 |
| PI STA 458+23.46, 2.86' LT | | | 1 | 4 |
| PT STA 461+07.48, 0' RT | | | 1 | 4 |
| POT STA 471+00.00, 0' RT | | | 1 | 4 |
| POT STA 478+10.63, 0' RT | | | 1 | 4 |
| TOTAL | 10 | 8 | 3 | 52 |
| USE | 10 | 8 | 3 | 52 |

RAISED REFLECTIVE PAVEMENT MARKER REMOVAL

| STATION TO STATION | SIDE | 78300200 |
|--|------|---|
| | | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL |
| | | EACH |
| FAP ROUTE 407 (IL 336/IL 110) | | |
| (EXISTING MARKERS ON EXISTING EXIT RAMP J) | | |
| 546+02.00 TO 549+87.00 | RT | 19 |
| FAP ROUTE 310 (US 67) | | |
| 429+64.16 TO 438+46.31 | RT | 25 |
| 429+64.16 TO 438+46.31 | LT | 26 |
| TOTAL | | 70 |
| USE | | 70 |

RAILROAD PROTECTIVE LIABILITY INSURANCE

| LOCATION | Z0048665 |
|--------------------------------------|---|
| | RAILROAD PROTECTIVE LIABILITY INSURANCE |
| | L SUM |
| FAP ROUTE 407 (IL 336/IL 110) | |
| 554+42.1 | 1 |
| TOTAL | 1 |
| USE | 1 |

FENCE REMOVAL

| STATION TO STATION | SIDE | Z0022800 |
|------------------------------|------|---------------|
| | | FENCE REMOVAL |
| | | FOOT |
| 950E (EMORY RD) | | |
| 3022+00.0 TO 3025+76.7 | LT | 377 |
| 1400N (TOWER RD) | | |
| 24+76.1 TO 24+78.6 | RT | 17 |
| 25+12.2 TO 26+96.7 | RT | 196 |
| 26+50.6 TO 28+00.0 | LT | 174 |
| FAP ROUTE 310 (US 67) | | |
| 448+57.9 TO 451+20.5 | LT | 266 |
| TOTAL | | 1,030 |
| USE | | 1,030 |

SETTLEMENT PLATFORMS

| STATION | OFFSET | Z0065100 |
|--------------------------|----------|----------------------|
| | | SETTLEMENT PLATFORMS |
| | | EACH |
| 950E (EMORY RD) | | |
| 3018+08.0 | 16.0' LT | 1 |
| 3019+58.0 | 16.0' RT | 1 |
| COUNTY HIGHWAY 20 | | |
| 5009+32.0 | 15.0' LT | 1 |
| 5012+52.0 | 15.0' RT | 1 |
| 1100E | | |
| 6017+73.0 | 15.0' LT | 1 |
| 6020+23.0 | 15.0' RT | 1 |
| TOTAL | | 6 |
| USE | | 6 |

FAP ROUTE 310 (US 67) MAINTENANCE OF TRAFFIC

| LOCATION | OFFSET | SIDE | 70400100 | 70400200 | 78200530 | 70600241 | 70600341 | X7010206 | Z0062456 |
|--------------------------------------|--------|------|----------------------------|-------------------------------------|---|---|--|---|--------------------|
| | | | TEMPORARY CONCRETE BARRIER | RELOCATE TEMPORARY CONCRETE BARRIER | BARRIER WALL MARKERS, TYPE C MONODIRECTIONAL CRYSTAL | IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE, NARROW), TEST LEVEL 2 | IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE, NARROW), TEST LEVEL 2 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701401 (SPECIAL) | TEMPORARY PAVEMENT |
| | | | FT | | EACH | | | L SUM | SQ YD |
| FAP ROUTE 310 (US 67) | | | | | | | | | |
| TRAFFIC CONTROL | | | | | | | | | |
| PER TRAFFIC CONTROL AND STAGING PLAN | | | | | | | | | |
| PRE-STAGE 1 | | | | | | | | | |
| STA 435+76.0 | | | | | | | | | |
| STA 463+70.0 | | RT | | | | | | | 2,108.0 |
| STAGE 1 | | | | | | | | | |
| STA 436+19.7 | 47.9' | | | | | | 1 | | |
| STA 460+50.0 | | RT | 2,437.5 | | 196.0 | | | | |
| STAGE 2 | | | | | | | | | |
| STA 435+92.1 | 20.1' | | | | | | 1 | | |
| STA 461+50.0 | | RT | 125.0 | 2,437.5 | 12.0 | | | | |
| STA 438+46.0 | | | | | | | | | |
| STA 464+13.6 | 20.1' | LT | 2,575.0 | | 208.0 | | 1 | | |
| STAGE 3 | | | | | | | | | |
| STA 438+46.0 | | LT | | 2,525.0 | | | | | |
| STA 463+67.0 | 47.1' | | | | | | 1 | | |
| TOTAL | | | 5,137.5 | 4,962.5 | 416.0 | | 2 | 1 | 2,108.0 |
| USE | | | 5,137.5 | 4,962.5 | 416 | | 2 | 1 | 2,108 |

REMOVE RIGHT-OF-WAY MARKERS

| STATION | OFFSET | X6660410 |
|------------------------------|------------|-----------------------------|
| | | REMOVE RIGHT-OF-WAY MARKERS |
| | | EACH |
| 950E (EMORY RD) | | |
| 3012+03.27 | 12.66' RT | 1 |
| 3012+43.34 | 27.50' RT | 1 |
| 1400N (TOWER RD) | | |
| 24+83.44 | 12.65' RT | 1 |
| COUNTY HIGHWAY 20 | | |
| 5045+35.59 | 31.44' LT | 1 |
| 5045+51.15 | 34.19' RT | 1 |
| 5045+93.89 | 33.15' LT | 1 |
| 5046+16.26 | 33.51' RT | 1 |
| FAP ROUTE 310 (US 67) | | |
| 447+19.16 | 145.43' LT | 1 |
| 449+60.49 | 64.71' RT | 1 |
| 450+18.71 | 133.05' LT | 1 |
| 451+10.03 | 78.90' RT | 1 |
| 454+19.89 | 153.50' LT | 1 |
| 457+16.79 | 105.27' LT | 1 |
| 464+13.92 | 99.10' LT | 1 |
| TOTAL | | 14 |
| USE | | 14 |

DELINEATORS (SPECIAL)

| LOCATION | X6350110 |
|--------------------------------------|-----------------------|
| | DELINEATORS (SPECIAL) |
| | EACH |
| FAP ROUTE 407 (IL 336/IL 110) | |
| STA. 555+75 | 4 |
| STA. 567+25 | 4 |
| STA. 608+20 | 4 |
| STA. 615+20 | 4 |
| STA. 626+10 | 4 |
| STA. 691+20 | 4 |
| STA. 729+03 | 4 |
| STA. 738+00 | 4 |
| STA. 801+00 | 4 |
| STA. 839+85 | 4 |
| STA. 869+10 | 4 |
| TOTAL | 44 |
| USE | 44 |

FLEXIBLE DELINEATORS

| STATION | OFFSET | Z0024478 |
|------------------------------|--------|----------------------|
| | | FLEXIBLE DELINEATORS |
| | | EACH |
| FAP ROUTE 310 (US 67) | | |
| 446+87 | 16' LT | 1 |
| 447+12 | 16' LT | 1 |
| 447+37 | 16' LT | 1 |
| 447+62 | 16' LT | 1 |
| 447+87 | 16' LT | 1 |
| 448+12 | 16' LT | 1 |
| 448+37 | 16' LT | 1 |
| 448+62 | 16' LT | 1 |
| 448+87 | 16' LT | 1 |
| 449+12 | 16' LT | 1 |
| 449+37 | 16' LT | 1 |
| 449+62 | 16' LT | 1 |
| 449+87 | 16' LT | 1 |
| 450+12 | 16' LT | 1 |
| 450+37 | 16' LT | 1 |
| 450+62 | 16' LT | 1 |
| TOTAL | | 16 |
| USE | | 16 |

REMOVE CONCRETE BOX CULVERT END SECTION

| STATION | OFFSET | X5010520 |
|------------------------------|----------|---|
| | | REMOVE CONCRETE BOX CULVERT END SECTION |
| | | EACH |
| FAP ROUTE 310 (US 67) | | |
| 444+18.6 | 84.4' LT | 1 |
| 444+23.8 | 84.1' LT | 1 |
| 444+28.8 | 83.9' LT | 1 |
| 444+34.1 | 83.6' LT | 1 |
| TOTAL | | 4 |
| USE | | 4 |

REMOVE INLET BOX

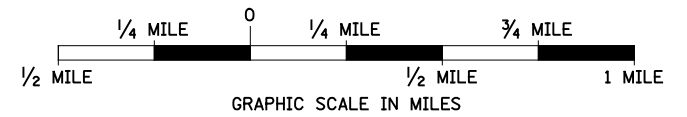
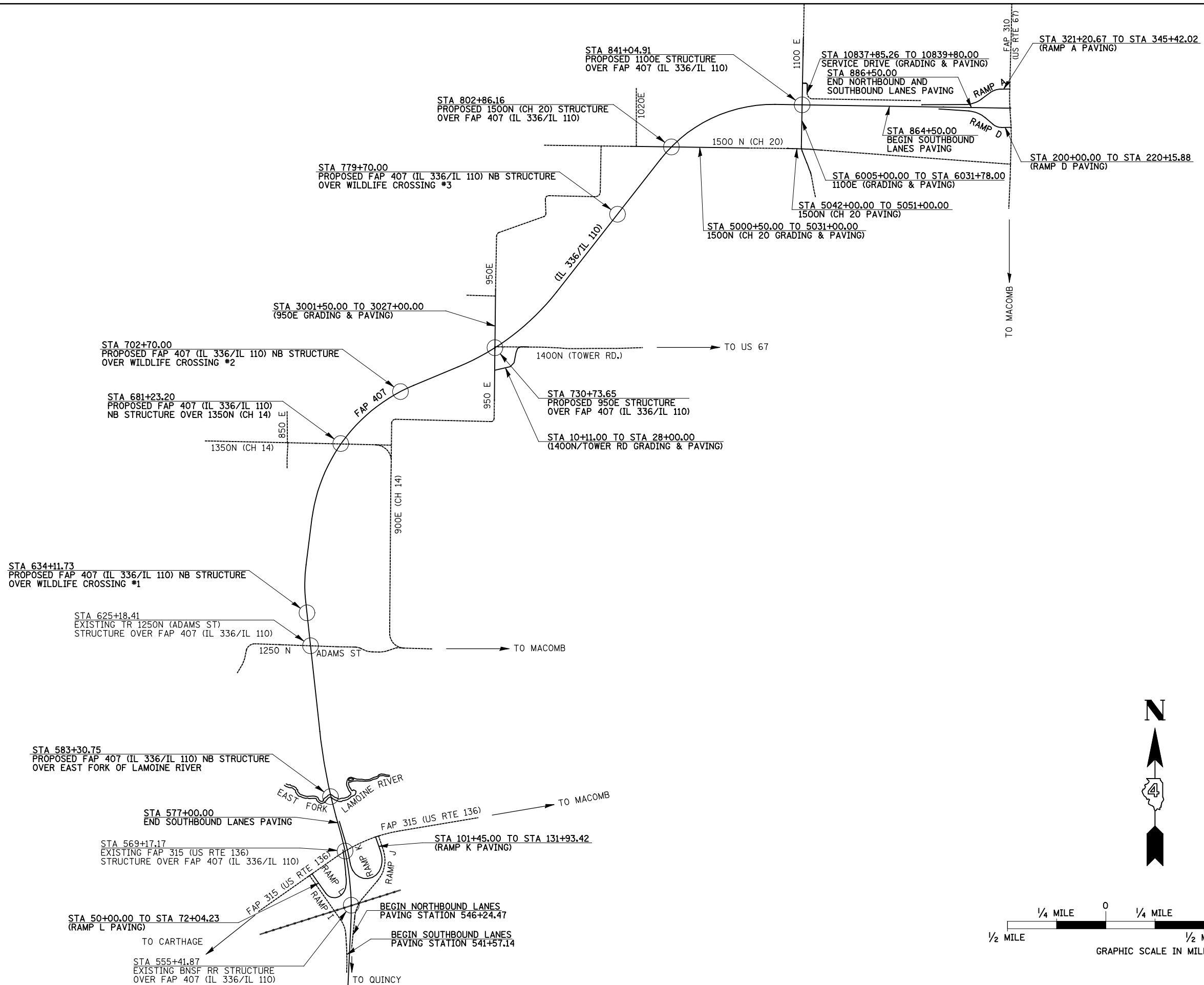
| STATION | OFFSET | X6050700 |
|------------------------------|---------|------------------|
| | | REMOVE INLET BOX |
| | | EACH |
| FAP ROUTE 310 (US 67) | | |
| 443+92.4 | 3.0' RT | 1 |
| 444+50.8 | 2.6' RT | 1 |
| TOTAL | | 2 |
| USE | | 2 |

ABANDON EXISTING UTILITIES

| LOCATION | X0327547 |
|--------------------------------|----------------------------|
| | ABANDON EXISTING UTILITIES |
| | L SUM |
| COUNTY HIGHWAY 20 | |
| STA 5001+14.0 TO STA 5030+71.2 | 1 |
| TOTAL | 1 |
| USE | 1 |

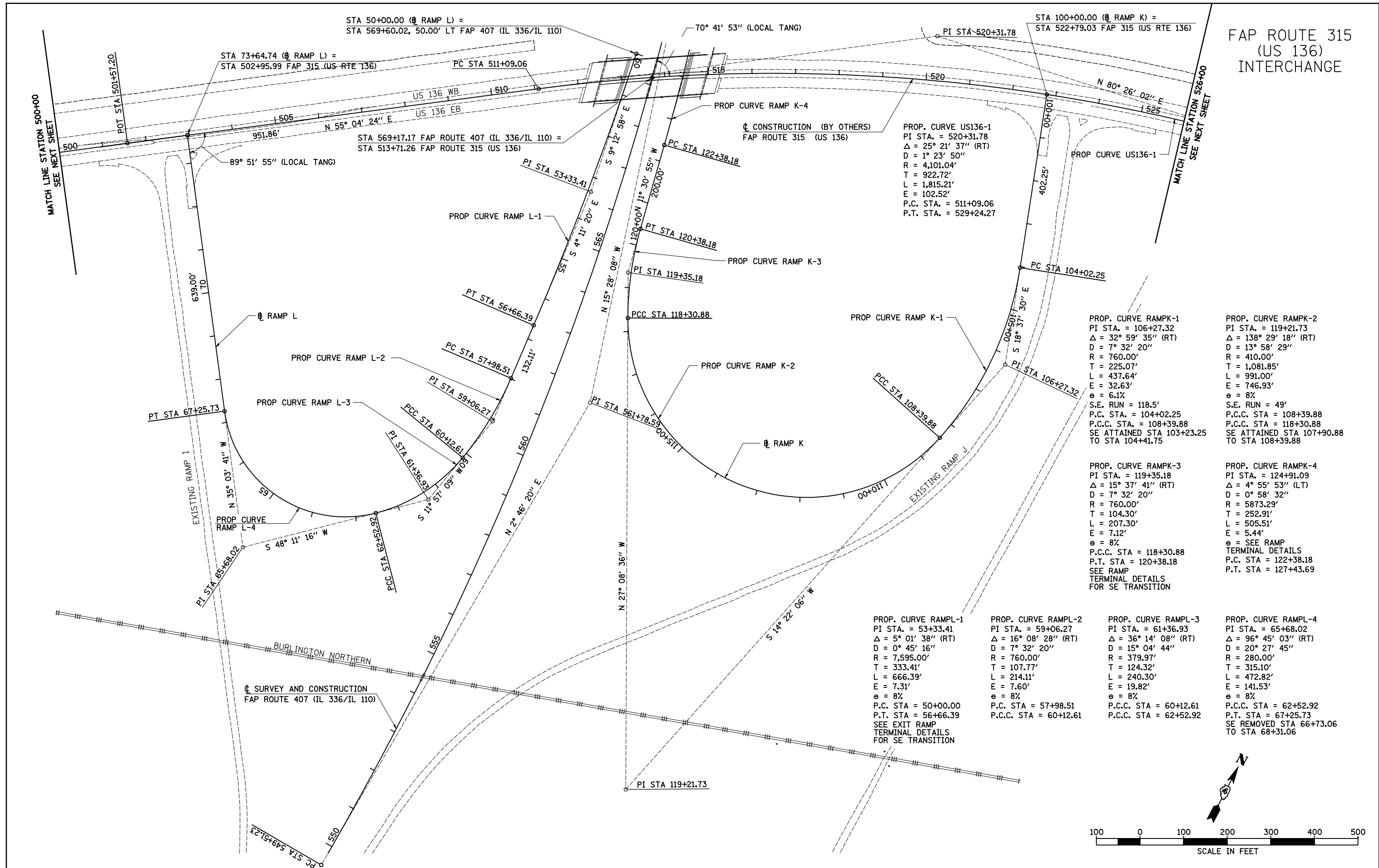
AGGREGATE SURFACE COURSE, TYPE B, 4"

| LOCATION | X4020400 |
|--------------------------------------|--------------------------------------|
| | AGGREGATE SURFACE COURSE, TYPE B, 4" |
| | SQ YD |
| FAP ROUTE 407 (IL 336/IL 110) | |
| WILDLIFE CROSSING #1 (STA 634+11.7) | 640.9 |
| WILDLIFE CROSSING #2 (STA 702+70.0) | 258.6 |
| TOTAL | 899.5 |
| USE | 900 |



| | | | | | | | | | | | | |
|---|-------------------|----------------|-----------|---|--|-------------------------|--------------|--------------------|-------------------------------------|------------------|------------------|--------------|
| FILE NAME = v:\transportation\2891\2-Lane Paving Plans\cadd sheets\046844-sht-ATB00.dgn | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) PROJECT LINE DIAGRAM | | | F.A.P. RTE. 407 | SECTION 55C3(PV,HB(2-6);B,B-1,B-2)] | COUNTY McDONOUGH | TOTAL SHEETS 874 | SHEET NO. 74 |
| PLOT SCALE = 1/320.00 "/> <td>CHECKED - AWM</td> <td>REVISED -</td> <td>REVISED -</td> <td>SCALE: 1"=1/4 MILE</td> <td>SHEET NO. 1 OF 1 SHEETS</td> <td>STA. TO STA.</td> <td colspan="5" style="text-align: center;">CONTRACT NO. 68B44</td> | CHECKED - AWM | REVISED - | REVISED - | | SCALE: 1"=1/4 MILE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | CONTRACT NO. 68B44 | | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | | | | | | | | |

FAP ROUTE 315
(US 136)
INTERCHANGE



PROP. CURVE US136-1
PI STA. = 520+31.78
Δ = 25° 21' 37" (RT)
D = 1° 23' 50"
R = 4,101.04'
T = 922.72'
L = 1,815.21'
E = 102.52'
P.C. STA. = 511+09.06
P.T. STA. = 529+24.27

PROP. CURVE RAMPK-1
PI STA. = 106+27.32
Δ = 32° 59' 35" (RT)
D = 7° 32' 20"
R = 760.00'
T = 225.07'
L = 437.64'
E = 32.63'
e = 6.1%
S.E. RUN = 118.5'
P.C. STA. = 104+02.25
P.C.C. STA. = 108+39.88
SE ATTAINED STA 103+23.25
TO STA 104+41.75

PROP. CURVE RAMPK-2
PI STA. = 119+21.73
Δ = 138° 29' 18" (RT)
D = 13° 58' 29"
R = 410.00'
T = 1,081.85'
L = 991.00'
E = 746.93'
e = 8%
S.E. RUN = 49'
P.C.C. STA = 108+39.88
P.C.C. STA = 118+30.88
SE ATTAINED STA 107+90.88
TO STA 108+39.88

PROP. CURVE RAMPK-3
PI STA. = 119+35.18
Δ = 15° 37' 41" (RT)
D = 7° 32' 20"
R = 760.00'
T = 104.30'
L = 207.30'
E = 7.12'
e = 8%
P.C.C. STA = 118+30.88
P.T. STA = 120+38.18
SEE RAMP
TERMINAL DETAILS
FOR SE TRANSITION

PROP. CURVE RAMPK-4
PI STA. = 124+91.09
Δ = 4° 55' 53" (LT)
D = 0° 58' 32"
R = 5873.29'
T = 252.91'
L = 505.51'
E = 5.44'
e = 8%
SEE RAMP
TERMINAL DETAILS
P.C. STA = 122+38.18
P.T. STA = 127+43.69

PROP. CURVE RAMPL-1
PI STA. = 53+33.41
Δ = 5° 01' 38" (RT)
D = 0° 45' 16"
R = 7,595.00'
T = 333.41'
L = 666.39'
E = 7.31'
e = 8%
P.C. STA = 50+00.00
P.T. STA = 56+66.39
SEE EXIT RAMP
TERMINAL DETAILS
FOR SE TRANSITION

PROP. CURVE RAMPL-2
PI STA. = 59+06.27
Δ = 16° 08' 28" (RT)
D = 7° 32' 20"
R = 760.00'
T = 107.77'
L = 214.11'
E = 7.60'
e = 8%
P.C. STA = 57+98.51
P.C.C. STA = 60+12.61

PROP. CURVE RAMPL-3
PI STA. = 61+36.93
Δ = 36° 14' 08" (RT)
D = 15° 04' 44"
R = 379.97'
T = 124.32'
L = 240.30'
E = 19.82'
e = 8%
P.C.C. STA = 60+12.61
P.C.C. STA = 62+52.92

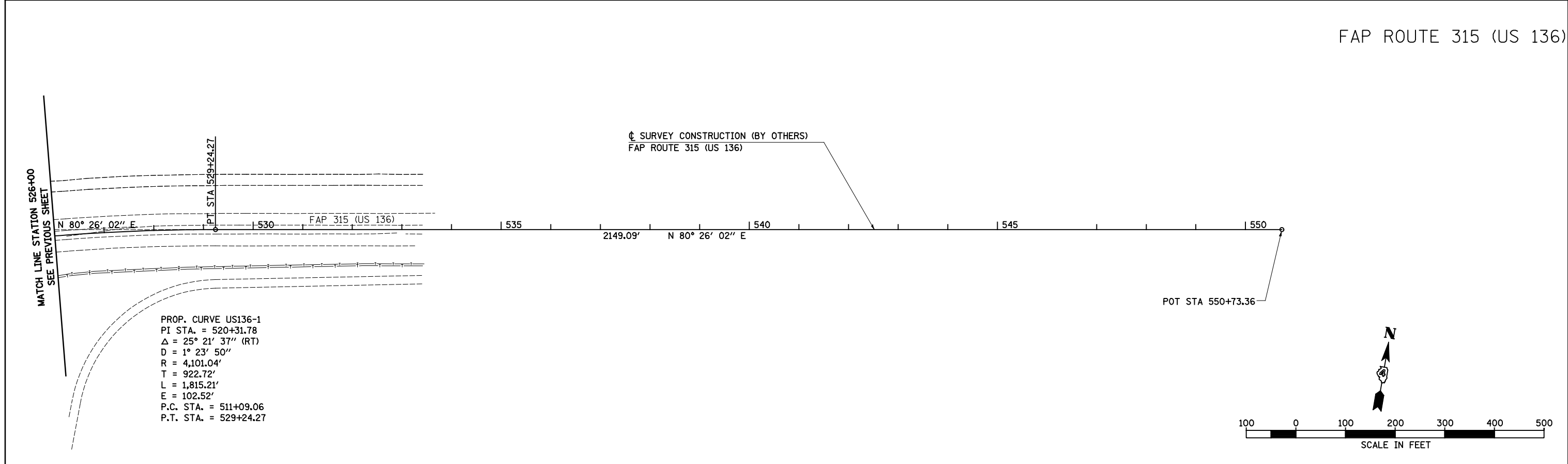
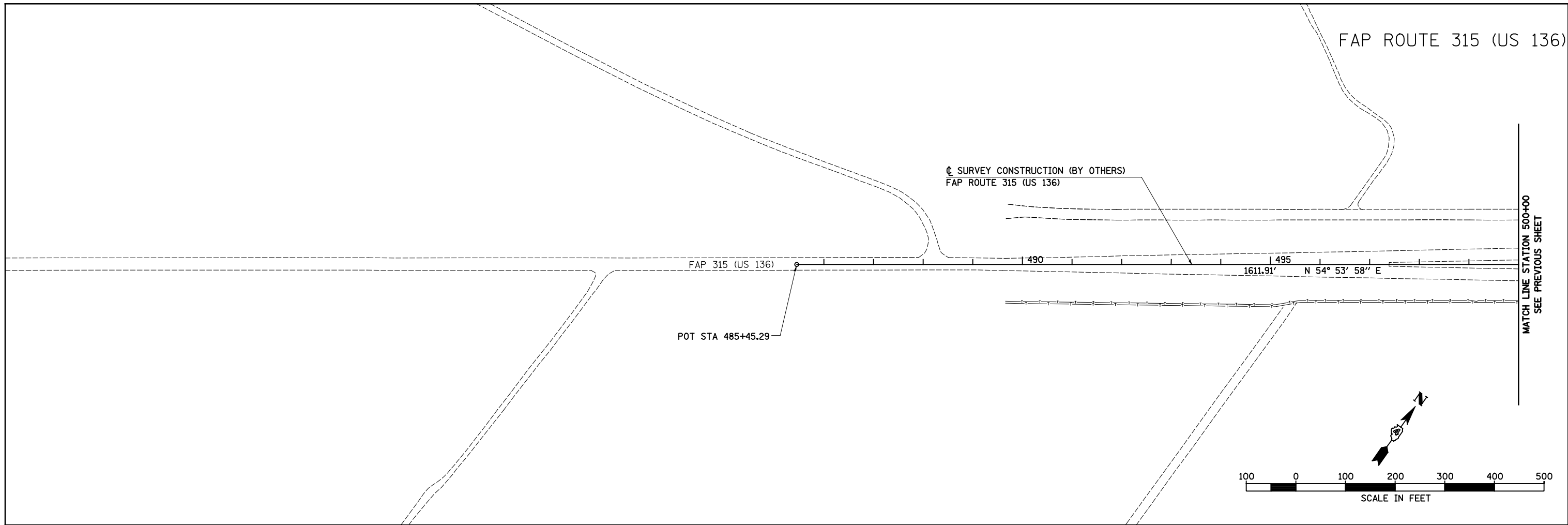
PROP. CURVE RAMPL-4
PI STA. = 65+68.02
Δ = 96° 45' 03" (RT)
D = 20° 27' 45"
R = 280.00'
T = 315.10'
L = 472.82'
E = 141.53'
e = 8%
P.C.C. STA = 62+52.92
P.T. STA = 67+25.73
SE REMOVED STA 66+73.06
TO STA 68+31.06

| | | | |
|--|-------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-sht-ATB01.dgn | | DRAWN - RLR | REVISED - |
| PLOT SCALE = 200.00' / IN. | | CHECKED - AWM | REVISED - |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

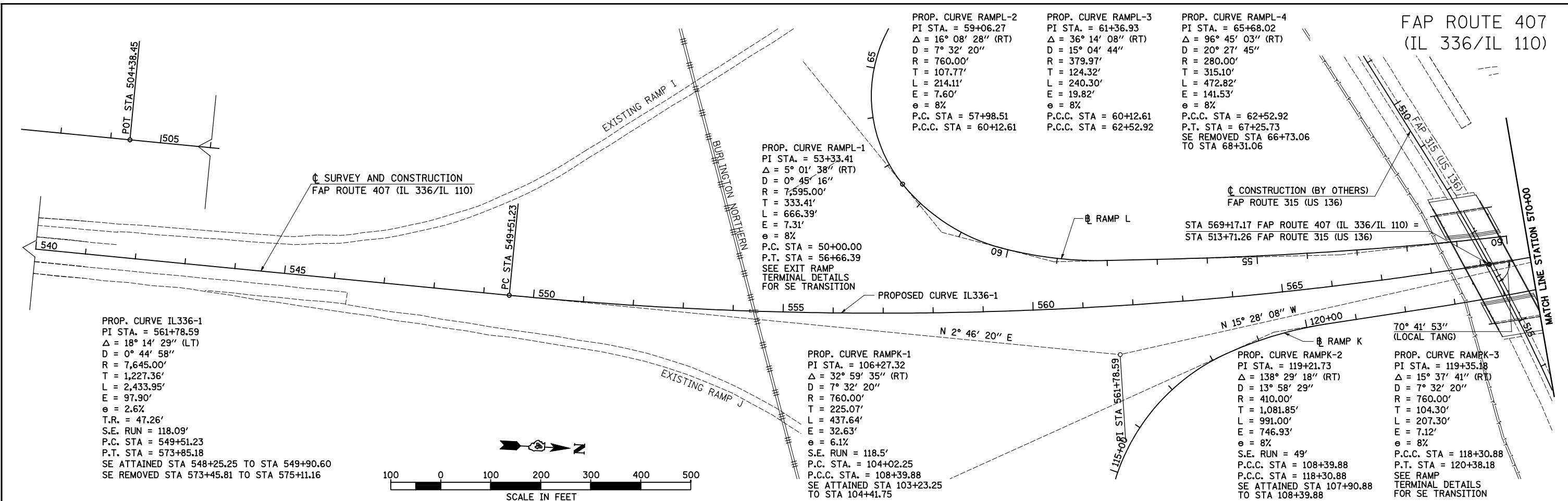
FAP ROUTE 407 (IL 336 /IL 110)
ALIGNMENT LAYOUT - FAP ROUTE 315 (US 136) INTERCHANGE
SCALE: 1"=100' SHEET NO. 1 OF 14 SHEETS STA. 549+50 TO STA. 570+00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------------------------|-----------|--------------|--------------------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 75 |
| | | | | CONTRACT NO. 68B44 |
| ILLINOIS FED. AID PROJECT | | | | |

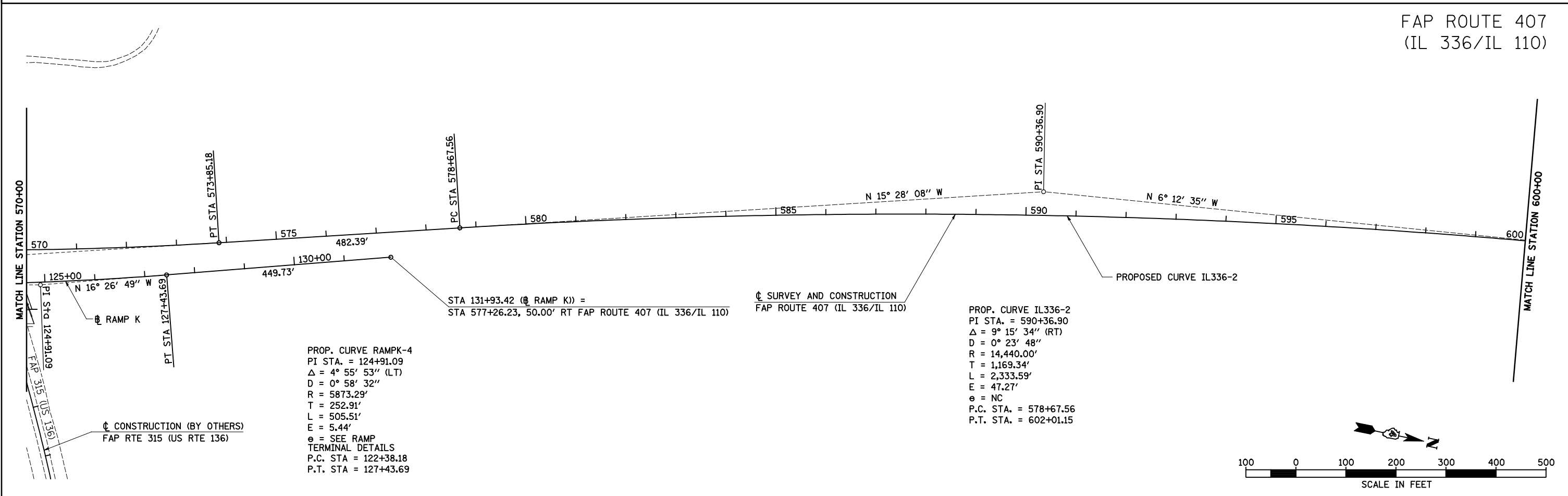


| | | | | | | | | | | | | |
|--|-------------------|------------------|-----------|---|---|-----------------------------|-------------|----------------|---------------------------|--------------------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) ALIGNMENT LAYOUT - FAP ROUTE 315 (US 136) | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-ATB02.dgn | | DRAWN - RLR | REVISED - | | 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 76 | CONTRACT NO. 68B44 | | |
| PLOT SCALE = 200.00' / IN. | | CHECKED - AWM | REVISED - | | SCALE: 1"=100' | SHEET NO. 2 OF 14 SHEETS | STA. 485+45 | TO STA. 550+75 | ILLINOIS FED. AID PROJECT | | | |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - | | | | | | | | | |

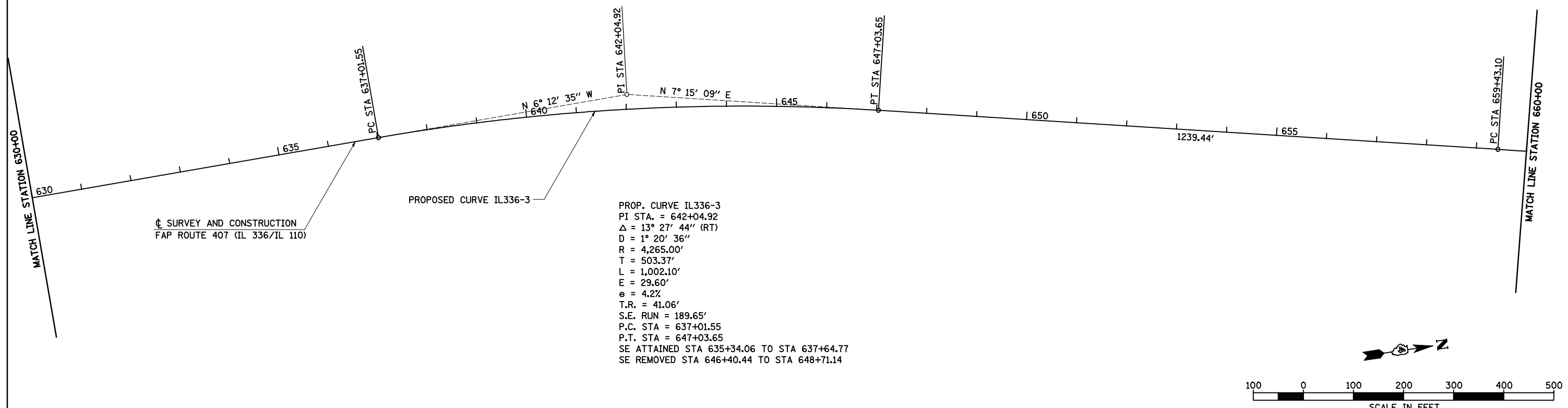
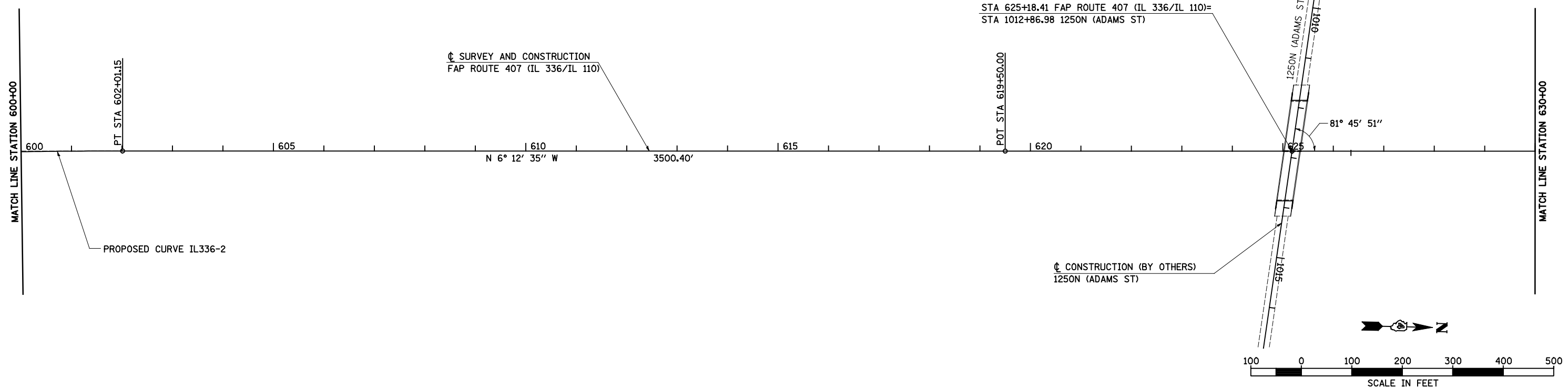
FAP ROUTE 407
(IL 336/IL 110)



FAP ROUTE 407
(IL 336/IL 110)



| | | | | | | | | | | | | |
|--|-------------------|------------------|-----------|---|--|--------------------------|----------------------------|-------------|-----------------------------|---------------------------|--------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) ALIGNMENT LAYOUT | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-sht-ATB03.dgn | | DRAWN - RLR | REVISED - | | SCALE: 1"=100' | SHEET NO. 3 OF 14 SHEETS | STA. 540+00 TO STA. 600+00 | 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 77 |
| PLOT SCALE = 200.00' / IN. | | CHECKED - AWM | REVISED - | | | | | | | CONTRACT NO. 68B44 | | |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - | | | | | | | ILLINOIS FED. AID PROJECT | | |



| | | | |
|--|-------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-ATB04.dgn | | DRAWN - RLR | REVISED - |
| PLOT SCALE = 200.00' / IN. | | CHECKED - AWM | REVISED - |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

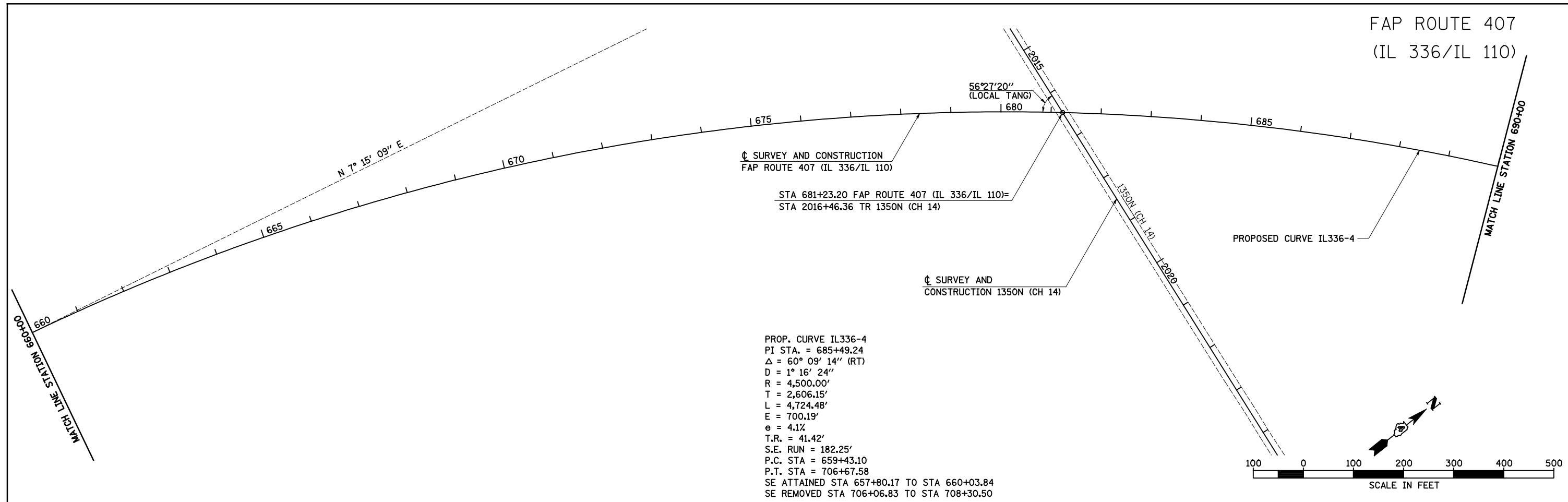
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP ROUTE 407 (IL 336 /IL 110)
ALIGNMENT LAYOUT**

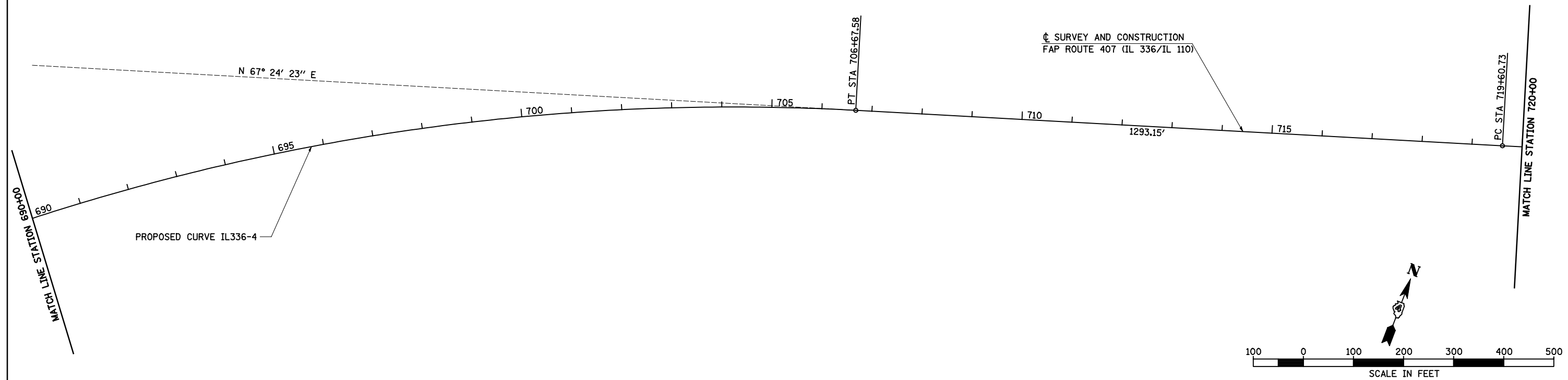
SCALE: 1"=100' SHEET NO. 4 OF 14 SHEETS STA. 600+00 TO STA. 660+00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------------------------|-----------|---------------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 78 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |

FAP ROUTE 407
(IL 336/IL 110)



FAP ROUTE 407
(IL 336/IL 110)



| | | | |
|--|-------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-ATB05.dgn | | DRAWN - RLR | REVISED - |
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| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

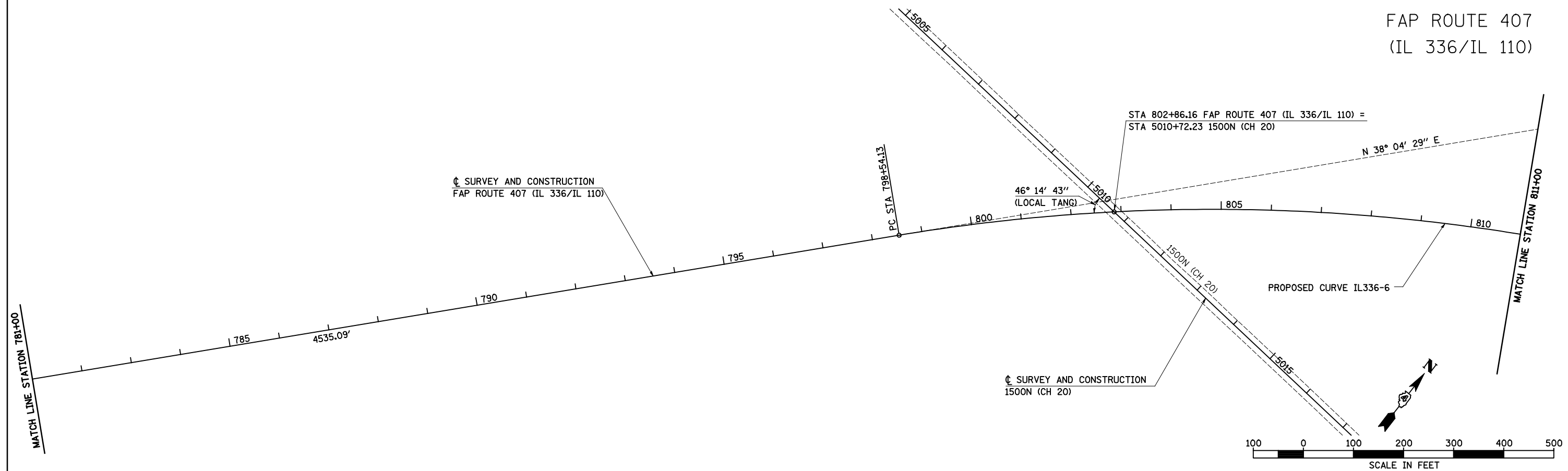
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 407 (IL 336 /IL 110)
ALIGNMENT LAYOUT

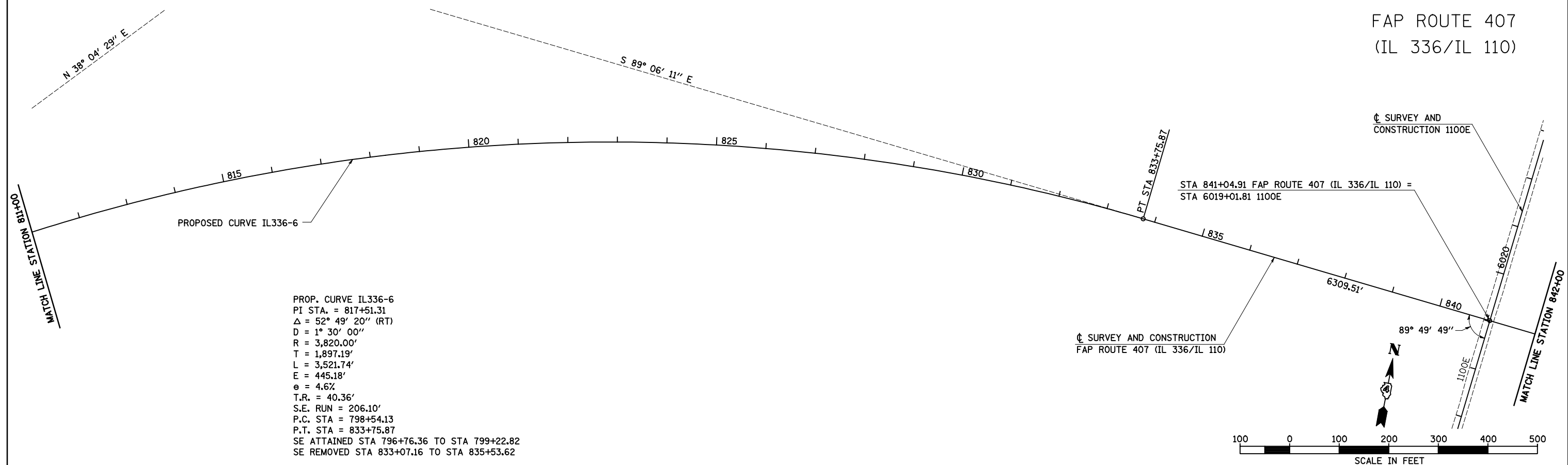
SCALE: 1"=100' SHEET NO. 5 OF 14 SHEETS STA. 660+00 TO STA. 720+00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------------------------|-----------|---------------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 79 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |

FAP ROUTE 407
(IL 336/IL 110)



FAP ROUTE 407
(IL 336/IL 110)



PROP. CURVE IL336-6
 PI STA. = 817+51.31
 Δ = 52° 49' 20" (RT)
 D = 1° 30' 00"
 R = 3,820.00'
 T = 1,897.19'
 L = 3,521.74'
 E = 445.18'
 e = 4.6%
 T.R. = 40.36'
 S.E. RUN = 206.10'
 P.C. STA = 798+54.13
 P.T. STA = 833+75.87
 SE ATTAINED STA 796+76.36 TO STA 799+22.82
 SE REMOVED STA 833+07.16 TO STA 835+53.62

| | | | |
|--|-------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-ATB07.dgn | | DRAWN - RLR | REVISED - |
| PLOT SCALE = 200.00' / IN. | | CHECKED - AWM | REVISED - |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

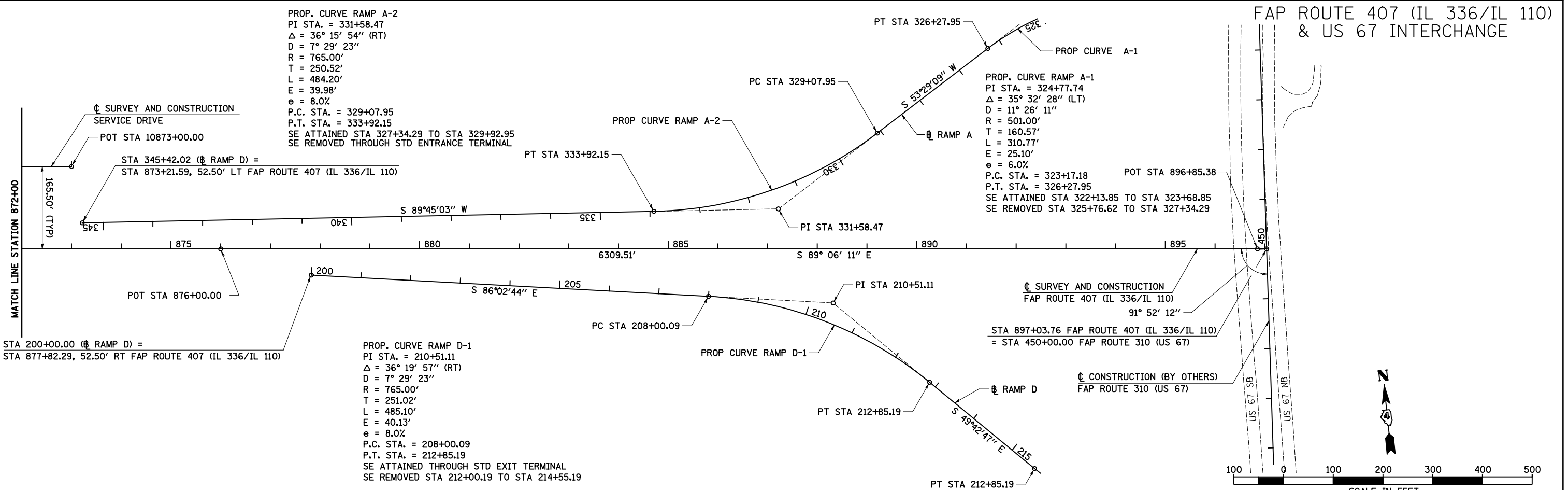
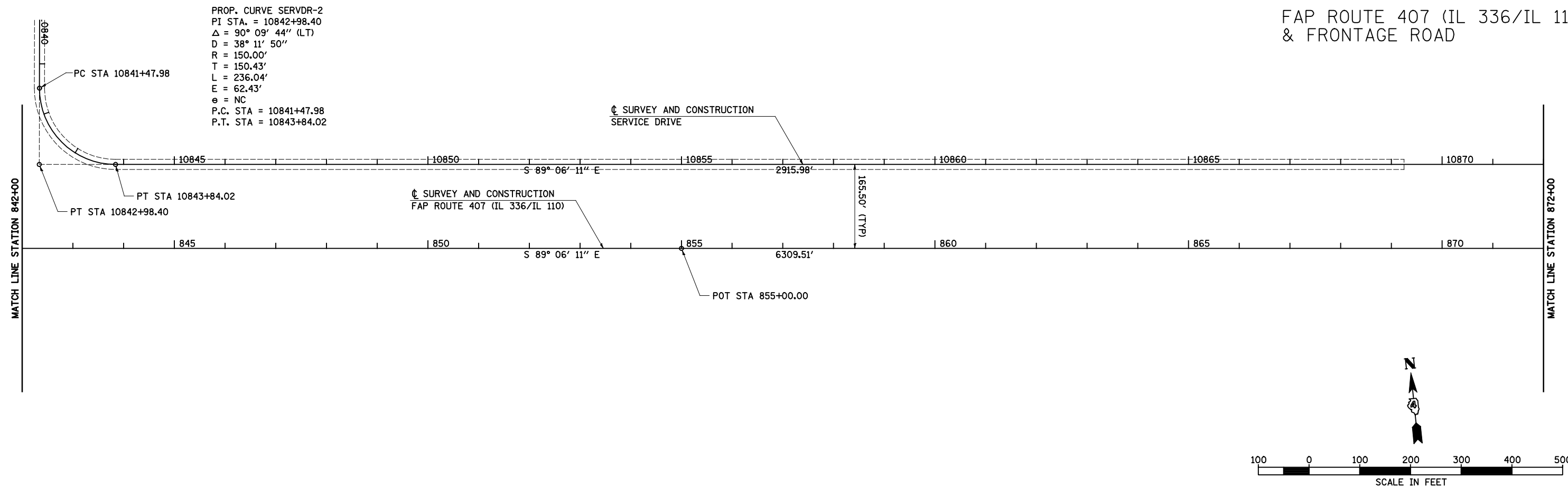
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 407 (IL 336 /IL 110)
ALIGNMENT LAYOUT

SCALE: 1"=100' SHEET NO. 7 OF 14 SHEETS STA. 781+00 TO STA. 842+00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------------------------|-----------|---------------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 81 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |

FAP ROUTE 407 (IL 336/IL 110)
& FRONTAGE ROAD



| | | | |
|--|-------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-sht-ATB08.dgn | | DRAWN - RLR | REVISED - |
| PLOT SCALE = 200.00' / IN. | | CHECKED - AWM | REVISED - |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | |
|--|--------------------------|-------------|-------------------|
| FAP ROUTE 407 (IL 336 /IL 110) ALIGNMENT LAYOUT | | | |
| SCALE: 1"=100' | SHEET NO. 8 OF 14 SHEETS | STA. 842+00 | TO STA. 897+03.76 |

| | | | | |
|--------------------|-----------------------------|-----------|---------------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 82 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |

US 67 INTERCHANGE

PROP. CURVE US 67-2
 PI STA. = 458+23.46
 $\Delta = 2^\circ 18' 31''$ (RT)
 $D = 0^\circ 24' 23''$
 $R = 14,100.00'$
 $T = 284.10'$
 $L = 568.12'$
 $E = 2.86'$
 $e = NC$
 P.C. STA. = 455+39.36
 P.T. STA. = 461+07.48

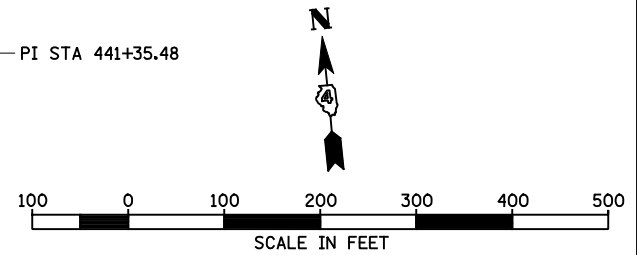
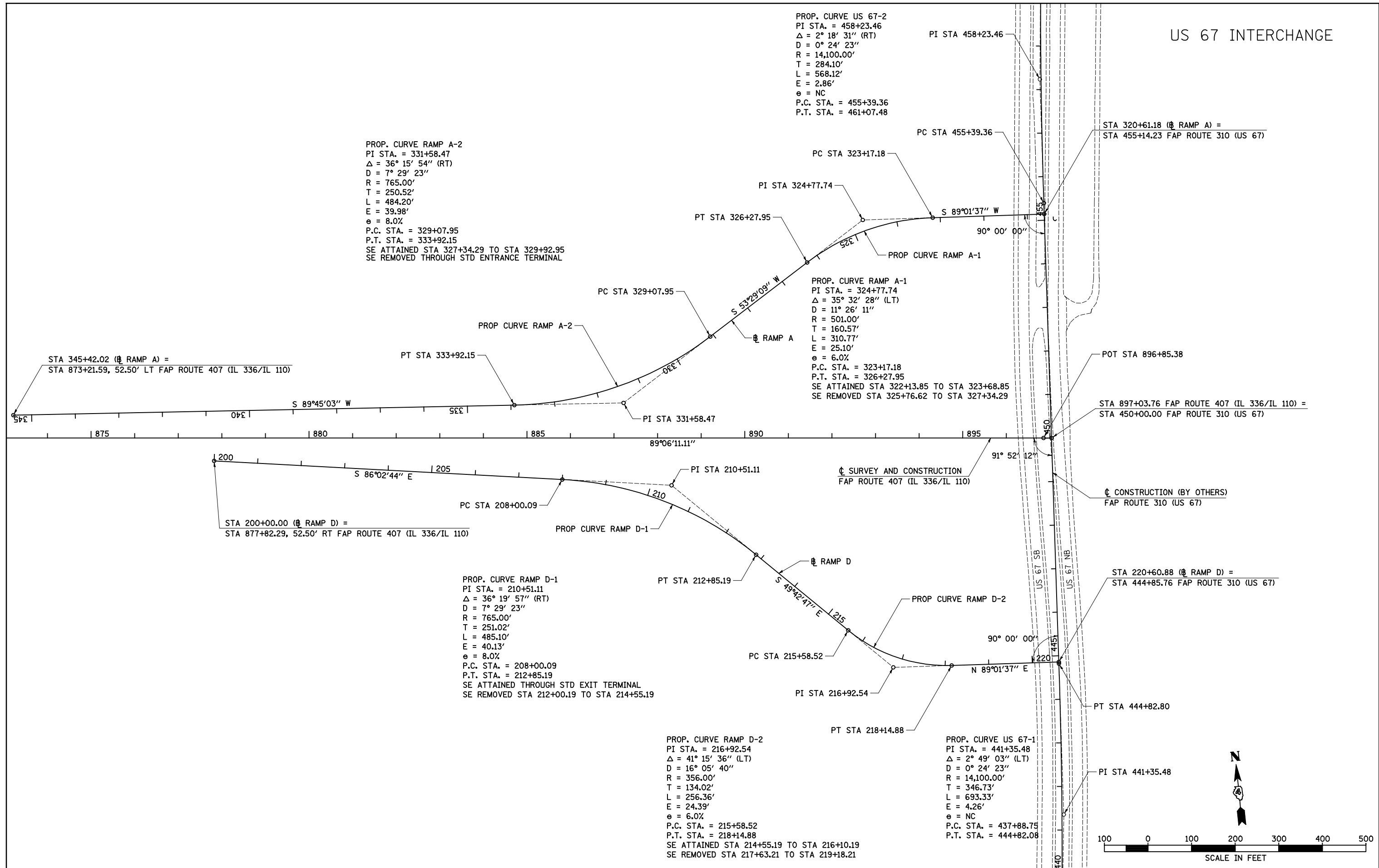
PROP. CURVE RAMP A-2
 PI STA. = 331+58.47
 $\Delta = 36^\circ 15' 54''$ (RT)
 $D = 7^\circ 29' 23''$
 $R = 765.00'$
 $T = 250.52'$
 $L = 484.20'$
 $E = 39.98'$
 $e = 8.0\%$
 P.C. STA. = 329+07.95
 P.T. STA. = 333+92.15
 SE ATTAINED STA 327+34.29 TO STA 329+92.95
 SE REMOVED THROUGH STD ENTRANCE TERMINAL

PROP. CURVE RAMP A-1
 PI STA. = 324+77.74
 $\Delta = 35^\circ 32' 28''$ (LT)
 $D = 11^\circ 26' 11''$
 $R = 501.00'$
 $T = 160.57'$
 $L = 310.77'$
 $E = 25.10'$
 $e = 6.0\%$
 P.C. STA. = 323+17.18
 P.T. STA. = 326+27.95
 SE ATTAINED STA 322+13.85 TO STA 323+68.85
 SE REMOVED STA 325+76.62 TO STA 327+34.29

PROP. CURVE RAMP D-1
 PI STA. = 210+51.11
 $\Delta = 36^\circ 19' 57''$ (RT)
 $D = 7^\circ 29' 23''$
 $R = 765.00'$
 $T = 251.02'$
 $L = 485.10'$
 $E = 40.13'$
 $e = 8.0\%$
 P.C. STA. = 208+00.09
 P.T. STA. = 212+85.19
 SE ATTAINED THROUGH STD EXIT TERMINAL
 SE REMOVED STA 212+00.19 TO STA 214+55.19

PROP. CURVE RAMP D-2
 PI STA. = 216+92.54
 $\Delta = 41^\circ 15' 36''$ (LT)
 $D = 16^\circ 05' 40''$
 $R = 356.00'$
 $T = 134.02'$
 $L = 256.36'$
 $E = 24.39'$
 $e = 6.0\%$
 P.C. STA. = 215+58.52
 P.T. STA. = 218+14.88
 SE ATTAINED STA 214+55.19 TO STA 216+10.19
 SE REMOVED STA 217+63.21 TO STA 219+18.21

PROP. CURVE US 67-1
 PI STA. = 441+35.48
 $\Delta = 2^\circ 49' 03''$ (LT)
 $D = 0^\circ 24' 23''$
 $R = 14,100.00'$
 $T = 346.73'$
 $L = 693.33'$
 $E = 4.26'$
 $e = NC$
 P.C. STA. = 437+88.75
 P.T. STA. = 444+82.08



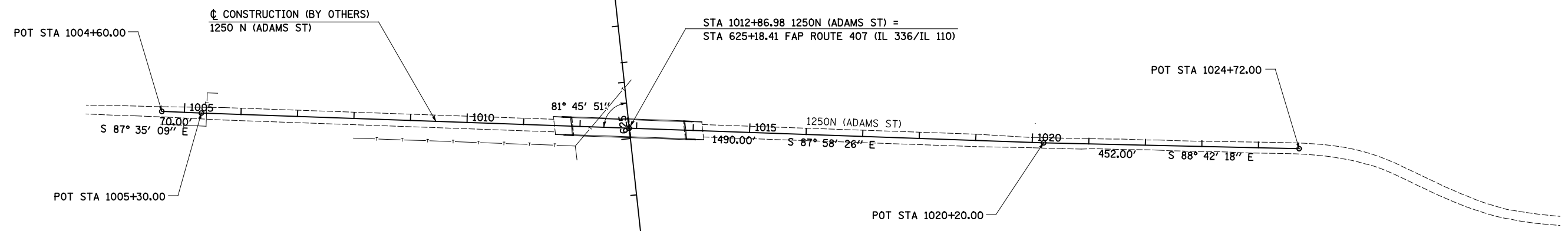
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| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-sht-ATB09.dgn | | DRAWN - RLR | REVISED - |
| PLOT SCALE = 200.00' / IN. | | CHECKED - JRB | REVISED - |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

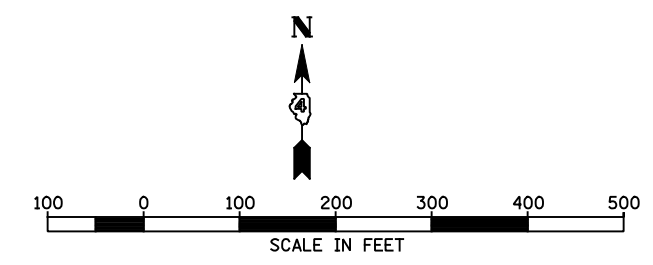
FAP ROUTE 407 (IL 336 /IL 110)
 ALIGNMENT LAYOUT - FAP ROUTE 310 (US 67) INTERCHANGE
 SCALE: 1"=100' SHEET NO. 9 OF 14 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------------------------|-----------|---------------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 83 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |

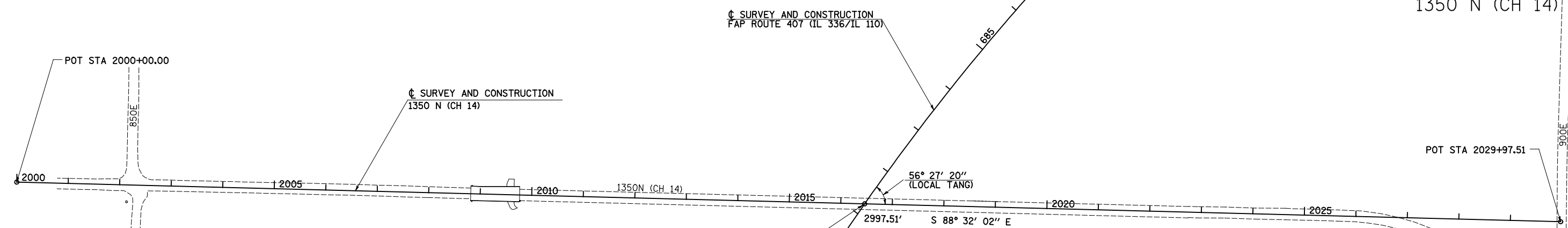
1250N (ADAMS STREET)



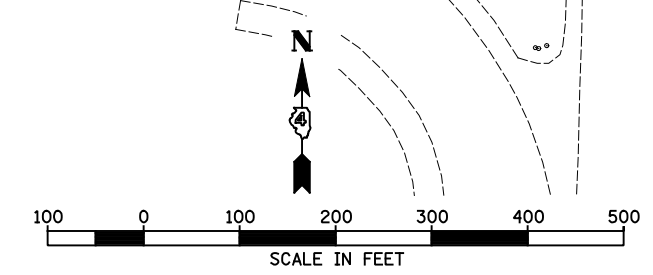
CL SURVEY AND CONSTRUCTION
FAP ROUTE 407 (IL 336/IL 110)



1350 N (CH 14)



STA 2016+46.36 1350N (CH 14) =
STA 681+23.20 FAP ROUTE 407 (IL 336/IL 110)



| | | | |
|---|-------------------|----------------|-----------|
| FILE NAME = v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-sht-ATB11.dgn | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| PLOT SCALE = 200.00' / IN. | CHECKED - AWM | DRAWN - RLR | REVISED - |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | | REVISED - |

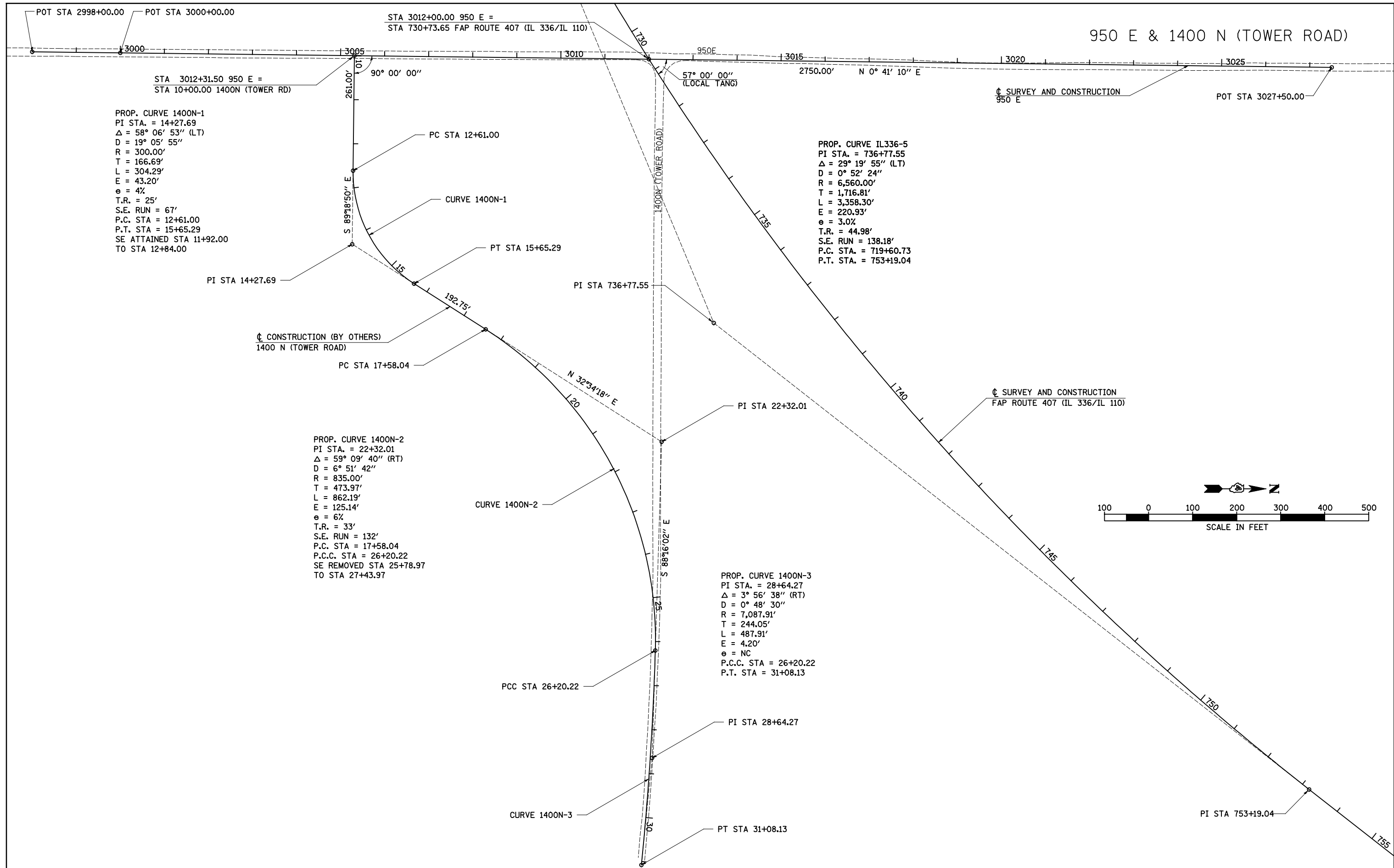
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP ROUTE 407 (IL 336 /IL 110)
ALIGNMENT LAYOUT - 1250N (ADAMS ST) & 1500N (CH 14)**

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------------------------|-----------|---------------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 84 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |

SCALE: 1"=100' SHEET NO. 10 OF 14 SHEETS STA. TO STA.

950 E & 1400 N (TOWER ROAD)

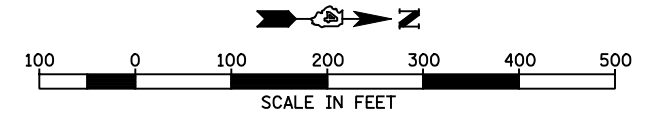


PROP. CURVE 1400N-1
 PI STA. = 14+27.69
 $\Delta = 58^\circ 06' 53''$ (LT)
 $D = 19^\circ 05' 55''$
 $R = 300.00'$
 $T = 166.69'$
 $L = 304.29'$
 $E = 43.20'$
 $e = 4\%$
 $T.R. = 25'$
 $S.E. RUN = 67'$
 $P.C. STA = 12+61.00$
 $P.T. STA = 15+65.29$
 $SE ATTAINED STA 11+92.00$
 $TO STA 12+84.00$

PROP. CURVE IL336-5
 PI STA. = 736+77.55
 $\Delta = 29^\circ 19' 55''$ (LT)
 $D = 0^\circ 52' 24''$
 $R = 6,560.00'$
 $T = 1,716.81'$
 $L = 3,358.30'$
 $E = 220.93'$
 $e = 3.0\%$
 $T.R. = 44.98'$
 $S.E. RUN = 138.18'$
 $P.C. STA = 719+60.73$
 $P.T. STA = 753+19.04$

PROP. CURVE 1400N-2
 PI STA. = 22+32.01
 $\Delta = 59^\circ 09' 40''$ (RT)
 $D = 6^\circ 51' 42''$
 $R = 835.00'$
 $T = 473.97'$
 $L = 862.19'$
 $E = 125.14'$
 $e = 6\%$
 $T.R. = 33'$
 $S.E. RUN = 132'$
 $P.C. STA = 17+58.04$
 $P.C.C. STA = 26+20.22$
 $SE REMOVED STA 25+78.97$
 $TO STA 27+43.97$

PROP. CURVE 1400N-3
 PI STA. = 28+64.27
 $\Delta = 3^\circ 56' 38''$ (RT)
 $D = 0^\circ 48' 30''$
 $R = 7,087.91'$
 $T = 244.05'$
 $L = 487.91'$
 $E = 4.20'$
 $e = NC$
 $P.C.C. STA = 26+20.22$
 $P.T. STA = 31+08.13$



| | | | |
|--|-------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-sht-ATB12.dgn | | DRAWN - RLR | REVISED - |
| PLOT SCALE = 200.00' / IN. | | CHECKED - AWM | REVISED - |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAP ROUTE 407 (IL 336 /IL 110)
 ALIGNMENT LAYOUT - 950 E & 1400 N (TOWER RD)**

SCALE: 1"=100' SHEET NO. 11 OF 14 SHEETS STA. TO STA.

| | | | | |
|--------------------|-----------------------------|-----------|---------------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 85 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |

PROP. CURVE CH20-1
 PI STA. = 5045+71.38
 $\Delta = 3^\circ 27' 01''$ (RT)
 $D = 0^\circ 42' 33''$
 $R = 8,080.00'$
 $T = 243.36'$
 $L = 486.57'$
 $E = 3.66'$
 $e = NC$
 P.C. STA = 5043+28.02
 P.T. STA = 5048+14.59

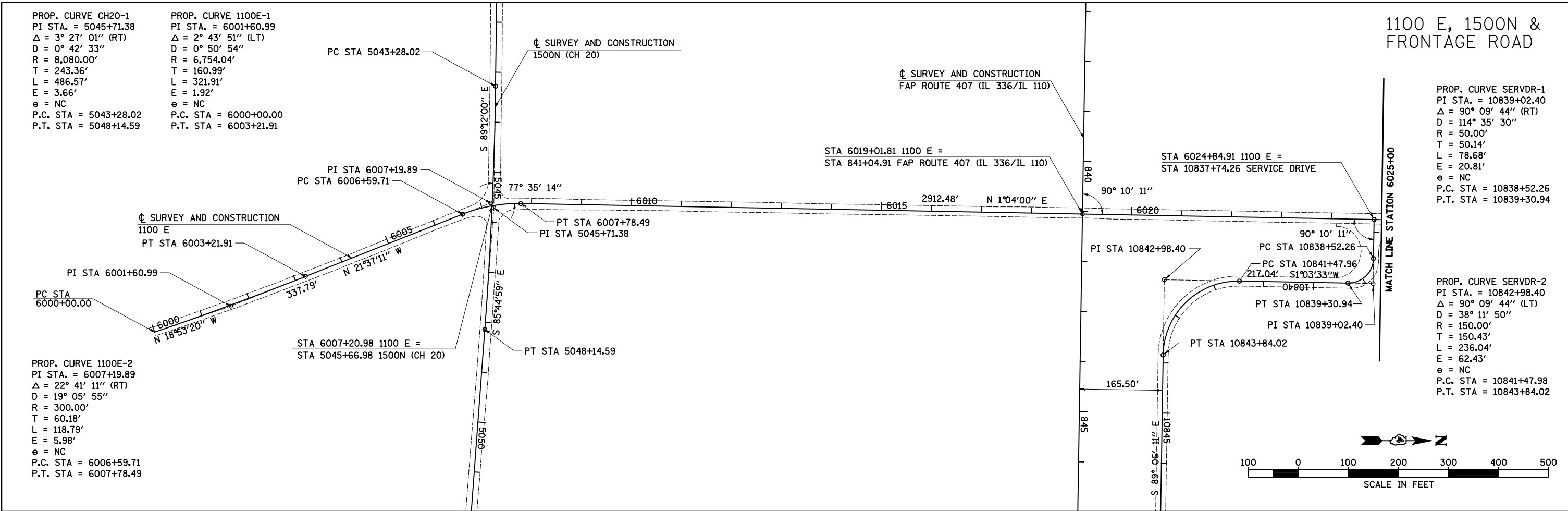
PROP. CURVE 1100E-1
 PI STA. = 6001+60.99
 $\Delta = 2^\circ 43' 51''$ (LT)
 $D = 0^\circ 50' 54''$
 $R = 6,754.04'$
 $T = 160.99'$
 $L = 321.91'$
 $E = 1.92'$
 $e = NC$
 P.C. STA = 6000+00.00
 P.T. STA = 6003+21.91

PROP. CURVE 1100E-2
 PI STA. = 6007+19.89
 $\Delta = 22^\circ 41' 11''$ (RT)
 $D = 19^\circ 05' 55''$
 $R = 300.00'$
 $T = 60.18'$
 $L = 118.79'$
 $E = 5.98'$
 $e = NC$
 P.C. STA = 6006+59.71
 P.T. STA = 6007+78.49

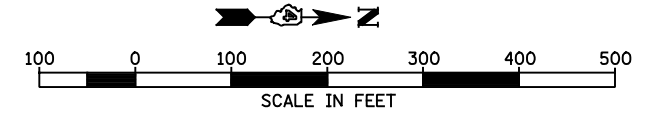
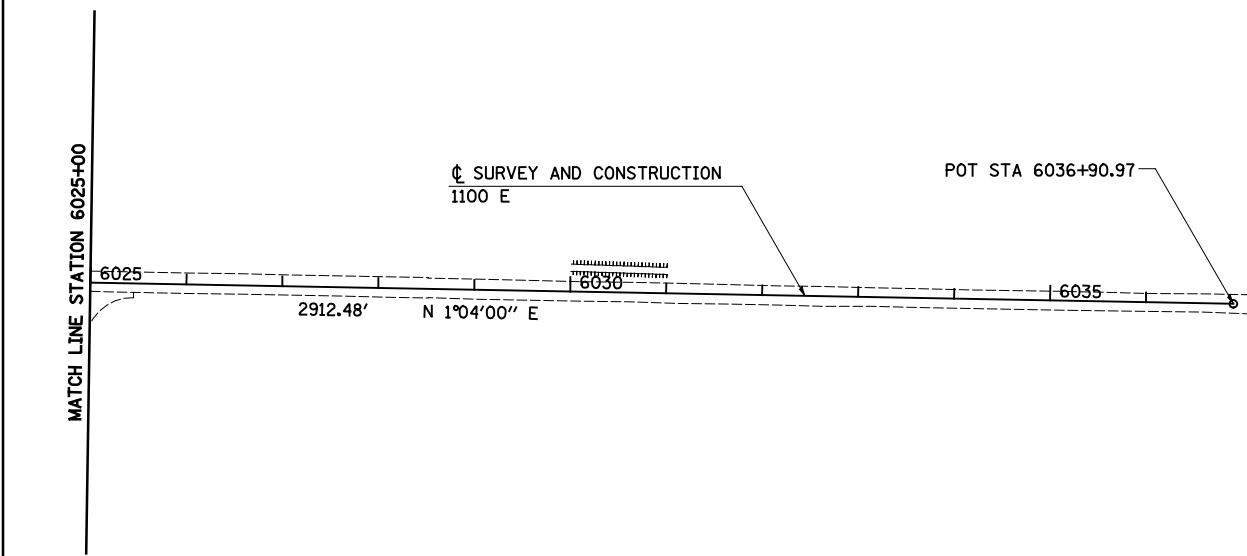
1100 E, 1500N & FRONTAGE ROAD

PROP. CURVE SERVDR-1
 PI STA. = 10839+02.40
 $\Delta = 90^\circ 09' 44''$ (RT)
 $D = 114^\circ 35' 30''$
 $R = 50.00'$
 $T = 50.14'$
 $L = 78.68'$
 $E = 20.81'$
 $e = NC$
 P.C. STA = 10838+52.26
 P.T. STA = 10839+30.94

PROP. CURVE SERVDR-2
 PI STA. = 10842+98.40
 $\Delta = 90^\circ 09' 44''$ (LT)
 $D = 38^\circ 11' 50''$
 $R = 150.00'$
 $T = 150.43'$
 $L = 236.04'$
 $E = 62.43'$
 $e = NC$
 P.C. STA = 10841+47.98
 P.T. STA = 10843+84.02



1100 E



| | | | |
|--|-------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-sht-ATB13.dgn | | DRAWN - RLR | REVISED - |
| PLOT SCALE = 200.00' / IN. | | CHECKED - AWM | REVISED - |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

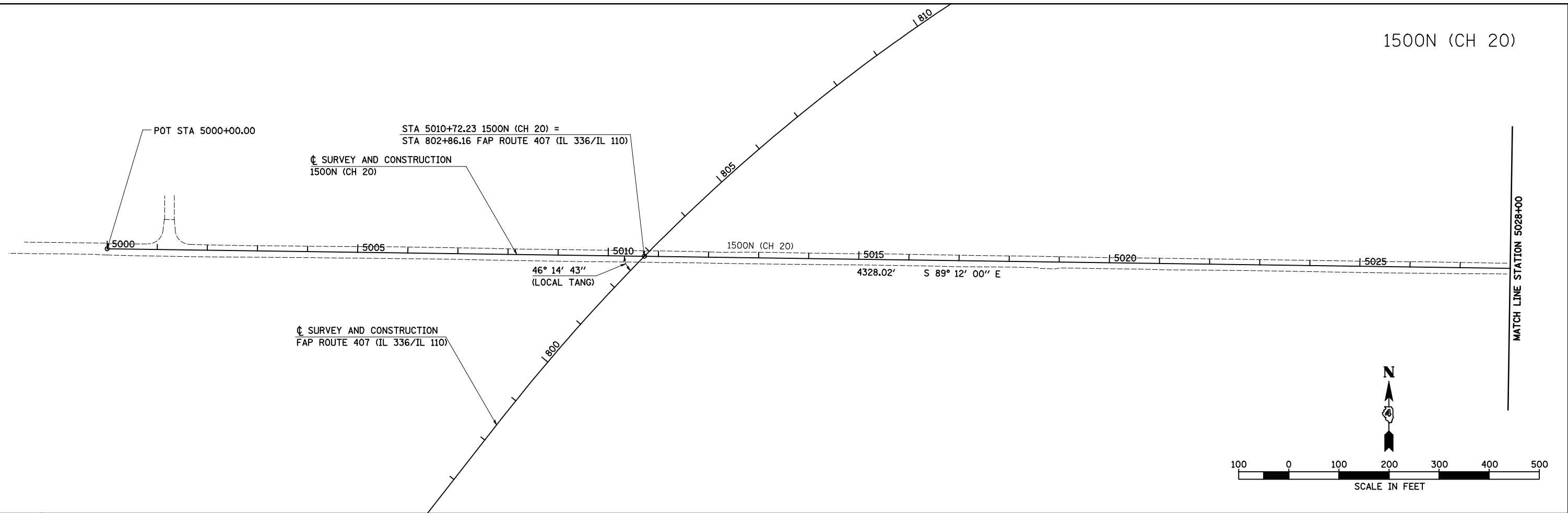
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAP ROUTE 407 (IL 336 /IL 110)
 ALIGNMENT LAYOUT - 1100E, 1500N (CH 20) & SERVICE DRIVE**

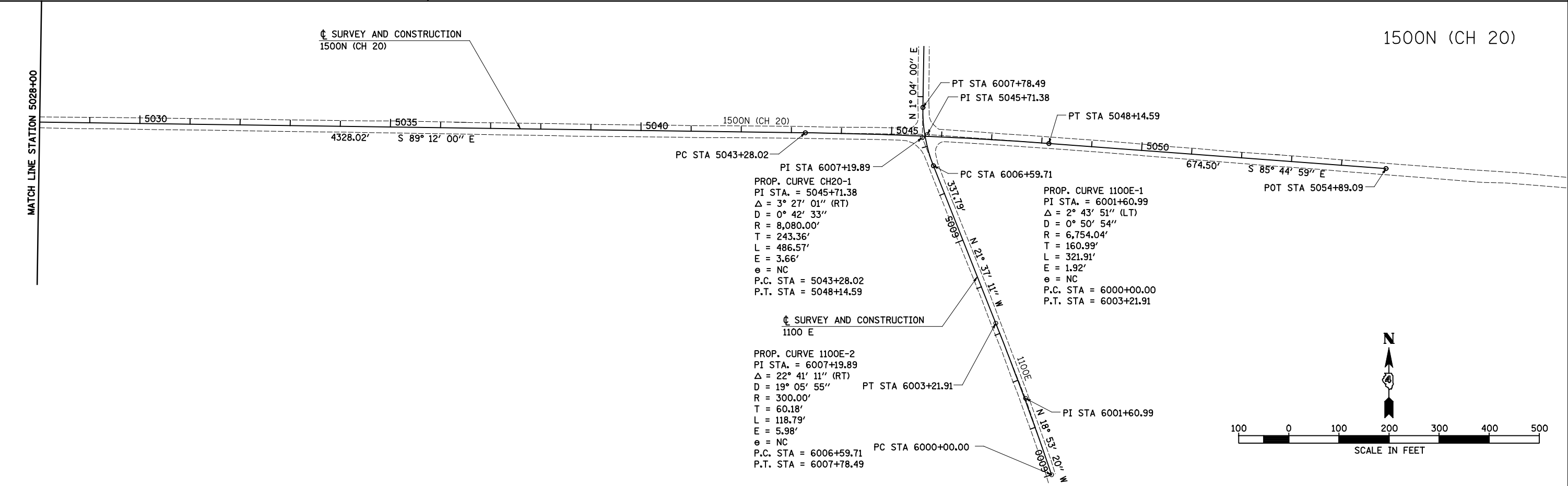
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|--------------------|-----------------------------|-----------|---------------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 86 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |

SCALE: 1"=100' SHEET NO. 12 OF 14 SHEETS STA. 6000+00 TO STA. 6036+90.97

1500N (CH 20)



1500N (CH 20)



PROP. CURVE CH20-1
 PI STA. = 5045+71.38
 $\Delta = 3^\circ 27' 01''$ (RT)
 $D = 0^\circ 42' 33''$
 $R = 8,080.00'$
 $T = 243.36'$
 $L = 486.57'$
 $E = 3.66'$
 $e = NC$
 P.C. STA = 5043+28.02
 P.T. STA = 5048+14.59

PROP. CURVE 1100E-1
 PI STA. = 6001+60.99
 $\Delta = 2^\circ 43' 51''$ (LT)
 $D = 0^\circ 50' 54''$
 $R = 6,754.04'$
 $T = 160.99'$
 $L = 321.91'$
 $E = 1.92'$
 $e = NC$
 P.C. STA = 6000+00.00
 P.T. STA = 6003+21.91

PROP. CURVE 1100E-2
 PI STA. = 6007+19.89
 $\Delta = 22^\circ 41' 11''$ (RT)
 $D = 19^\circ 05' 55''$
 $R = 300.00'$
 $T = 60.18'$
 $L = 118.79'$
 $E = 5.98'$
 $e = NC$
 P.C. STA = 6006+59.71
 P.T. STA = 6007+78.49

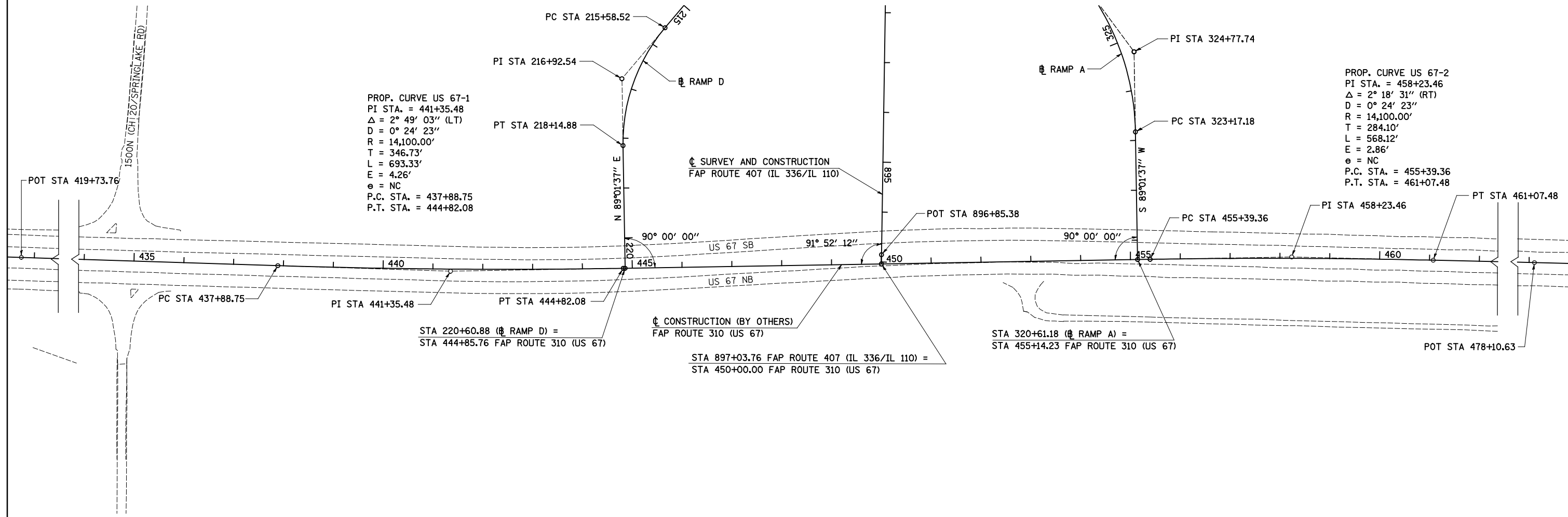
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| PLOT SCALE = 200.00' / IN. | | CHECKED - AWM | REVISED - |
| PLOT DATE = 1/26/2015 | | DATE - 1-30-2015 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP ROUTE 407 (IL 336 /IL 110)
ALIGNMENT LAYOUT - 1500N (CH 20)**

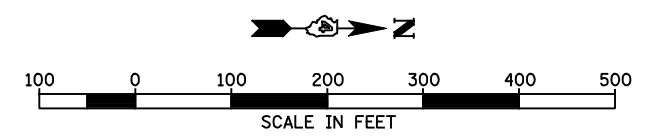
SCALE: 1"=100' SHEET NO. 13 OF 14 SHEETS STA. 5000+00.00 TO STA. 5054+89.09

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------------------------|-----------|---------------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 87 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |

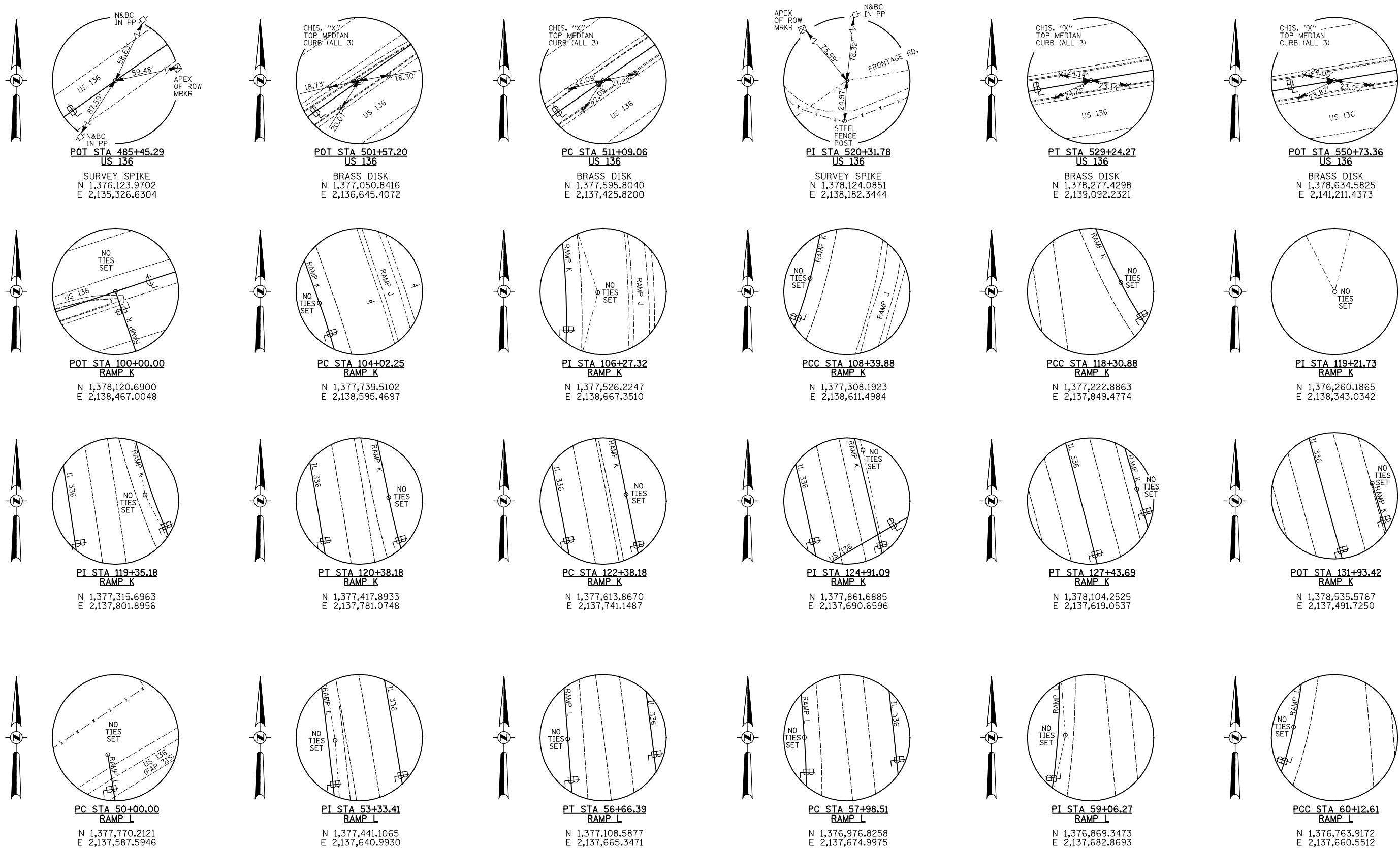


PROP. CURVE US 67-1
 PI STA. = 441+35.48
 $\Delta = 2^\circ 49' 03''$ (LT)
 $D = 0^\circ 24' 23''$
 $R = 14,100.00'$
 $T = 346.73'$
 $L = 693.33'$
 $E = 4.26'$
 $e = NC$
 P.C. STA. = 437+88.75
 P.T. STA. = 444+82.08

PROP. CURVE US 67-2
 PI STA. = 458+23.46
 $\Delta = 2^\circ 18' 31''$ (RT)
 $D = 0^\circ 24' 23''$
 $R = 14,100.00'$
 $T = 284.10'$
 $L = 568.12'$
 $E = 2.86'$
 $e = NC$
 P.C. STA. = 455+39.36
 P.T. STA. = 461+07.48



| | | | | | | | | | | | | |
|---|-------------------|----------------|------------|---|--|---------------------------|--|----------------------------------|---------------------|-----------------|--|--|
| FILE NAME = v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-ATB15.dgn | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) ALIGNMENT LAYOUT - FAP ROUTE 310 (US 67) | F.A.P. RTE. 407 | SECTION 55C3(PV,HB(2-6);B,B-1,B-2)] | COUNTY McDONOUGH | TOTAL SHEETS 874 | SHEET NO. 88 | | |
| PLOT SCALE = 200.00' / IN. | CHECKED - AWM | REVISIED - | REVISIED - | | | SCALE: 1"=100' | SHEET NO. 14 OF 14 SHEETS | STA. 419+73.76 TO STA. 478+10.63 | CONTRACT NO. 68B44 | | | |
| PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISIED - | REVISIED - | | | ILLINOIS FED. AID PROJECT | | | | | | |
| | | | | | | | | | | | | |



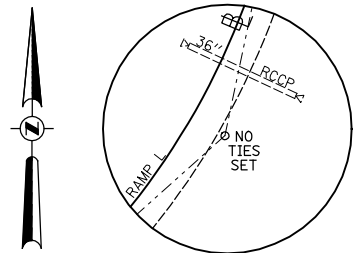
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|--|-------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-sht-ATB.dgn | | DRAWN - JW | REVISED - |
| | | CHECKED - TJD | REVISED - |
| | | DATE - 1-30-2015 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

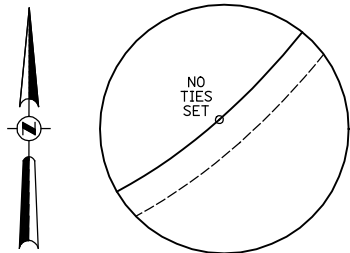
**FAP ROUTE 407 (IL 336 /IL 110)
ALIGNMENT TIES**

SCALE: N/A SHEET NO. 1 OF 6 SHEETS STA. N/A TO STA. N/A

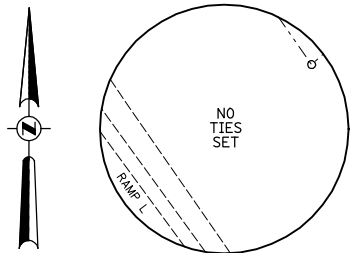
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------------------------|-----------|--------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 89 |
| CONTRACT NO. 68B44 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



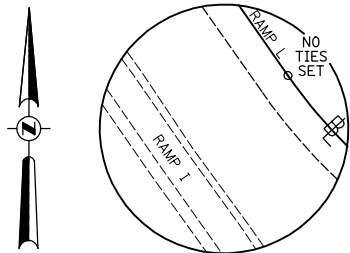
PI STA 61+36.93
RAMP L
N 1,376,642.2890
E 2,137,634.8041



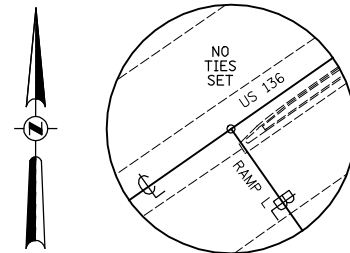
PCC STA 62+52.92
RAMP L
N 1,376,559.4035
E 2,137,542.1416



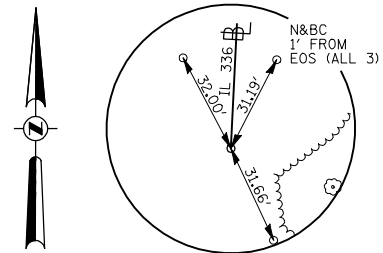
PI STA 65+68.02
RAMP L
N 1,376,349.3270
E 2,137,307.2849



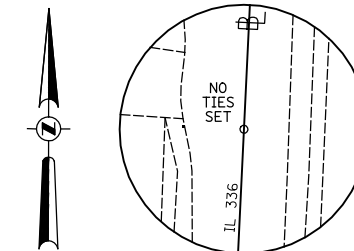
PT STA 67+25.73
RAMP L
N 1,376,607.2505
E 2,137,126.2731



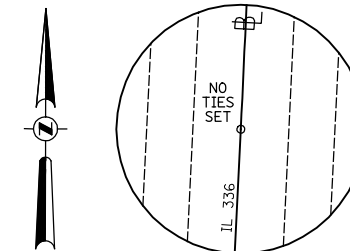
POT STA 73+64.64
RAMP L
N 1,377,130.2998
E 2,136,759.1952



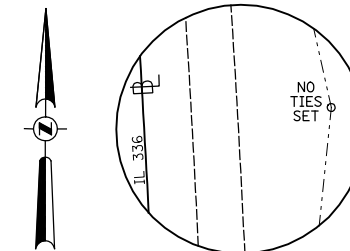
POT STA 504+38.45
FAP 407 (IL 336/IL 110)
BRASS DISK
N 1,371,277.2256
E 2,137,584.2219



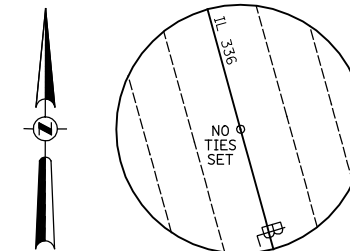
POT STA 541+57.14
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 541+57.14"
N 1,374,991.5633
E 2,137,764.0860



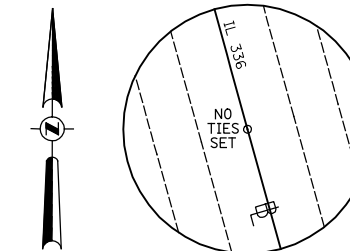
PC STA 549+51.23
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 549+51.23"
N 1,375,784.7210
E 2,137,802.4941



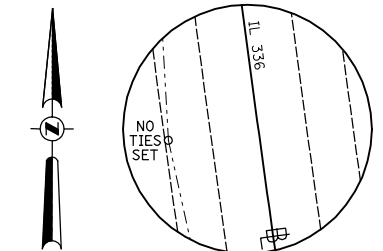
PI STA 561+78.59
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 561+78.59"
N 1,377,010.6433
E 2,137,861.8585



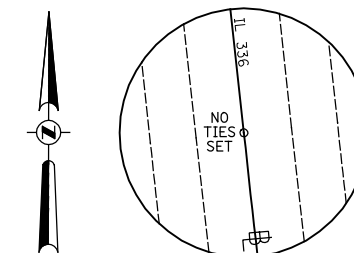
PT STA 573+85.18
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 573+85.18"
N 1,378,193.5410
E 2,137,534.5013



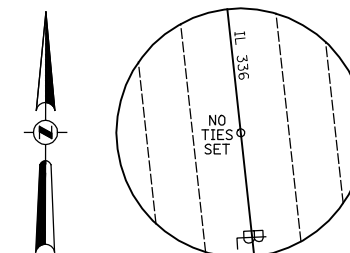
PC STA 578+67.56
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 578+67.56"
N 1,378,658.4519
E 2,137,405.8410



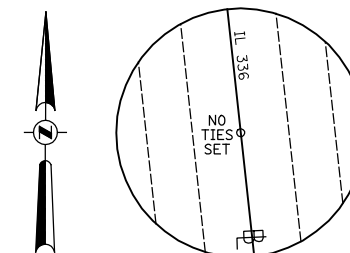
PI STA 590+36.90
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 590+36.90"
N 1,379,785.4337
E 2,137,093.9581



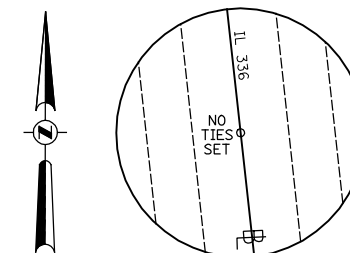
PT STA 602+01.15
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 602+01.15"
N 1,380,947.9141
E 2,136,967.4742



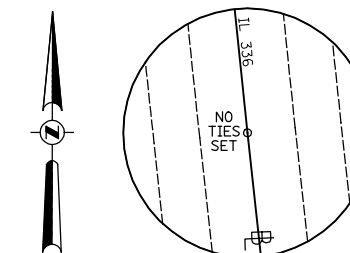
POT STA 610+75.00
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 610+75.00"
N 1,381,816.6359
E 2,136,872.9528



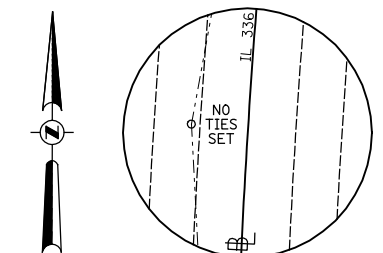
POT STA 619+50.00
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 619+50.00"
N 1,382,686.5021
E 2,136,778.3069



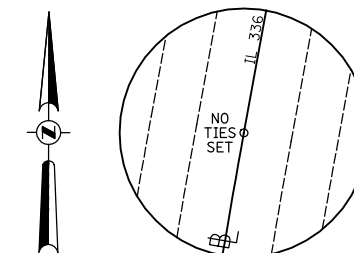
POT STA 628+25.00
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 628+25.00"
N 1,383,556.3682
E 2,136,683.6610



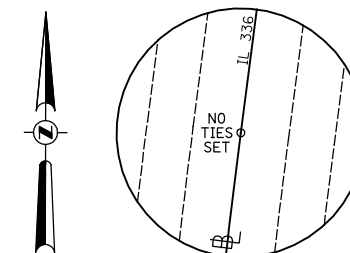
PC STA 637+01.55
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 637+01.55"
N 1,384,427.7770
E 2,136,588.8472



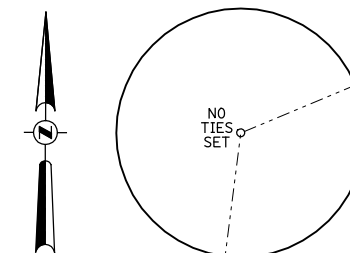
PI STA 642+04.92
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 642+04.92"
N 1,384,928.1921
E 2,136,534.3995



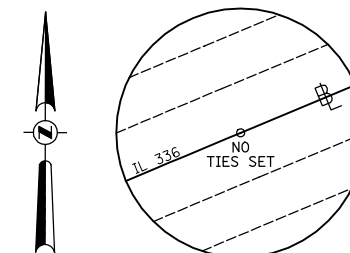
PT STA 647+03.65
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 647+03.65"
N 1,385,427.5333
E 2,136,597.9461



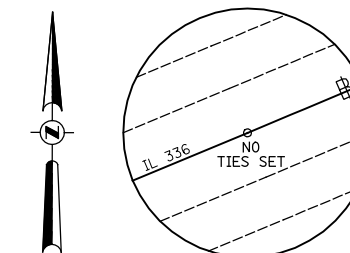
PC STA 659+43.10
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 659+43.10"
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E 2,136,754.4168



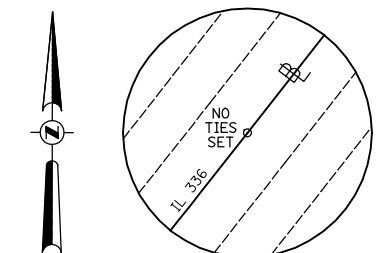
PI STA 685+49.24
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 685+49.24"
N 1,389,242.3557
E 2,137,083.4237



PT STA 706+67.58
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 706+67.58"
N 1,390,243.6114
E 2,139,489.5581



PC STA 719+60.73
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 719+60.73"
N 1,390,740.4281
E 2,140,683.4666



PT STA 753+19.04
FAP 407 (IL 336/IL 110)
BRASS DISK "STA 753+19.04"
N 1,392,751.4975
E 2,143,327.2586

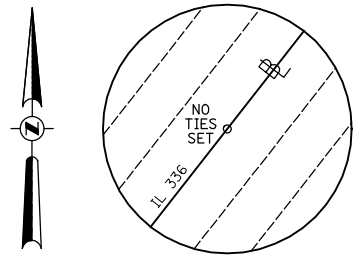
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| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
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| | | CHECKED - TJD | REVISED - |
| | | DATE - 1-30-2015 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

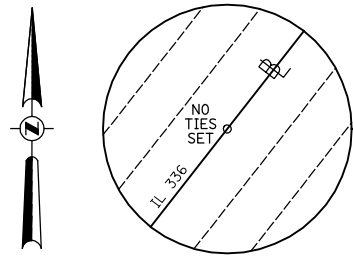
**FAP ROUTE 407 (IL 336 /IL 110)
ALIGNMENT TIES**

SCALE: N/A SHEET NO. 2 OF 6 SHEETS STA. N/A TO STA. N/A

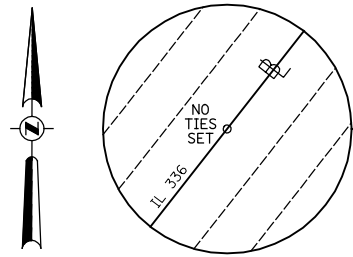
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|--------------------|-----------------------------|-----------|---------------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 90 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |



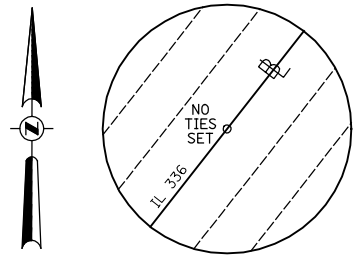
POT STA 762+25.00
FAP 407 (IL 336/IL 110)
 BRASS DISK "STA 762+25.00"
 N 1,393,464.6774
 E 2,143,885.9549



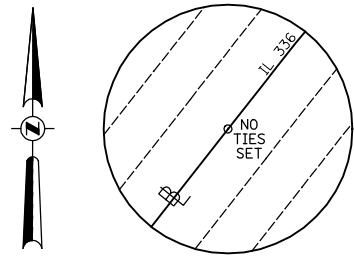
POT STA 771+25.00
FAP 407 (IL 336/IL 110)
 BRASS DISK "STA 771+25.00"
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 E 2,144,440.9746



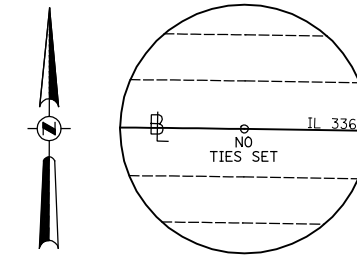
POT STA 780+25.00
FAP 407 (IL 336/IL 110)
 BRASS DISK "STA 780+25.00"
 N 1,394,881.6505
 E 2,144,995.9942



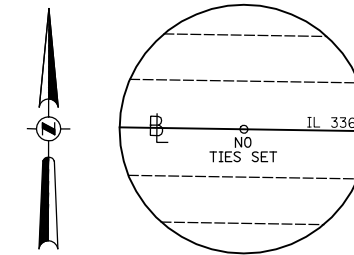
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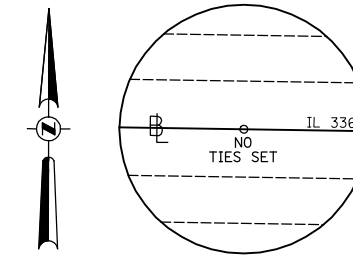
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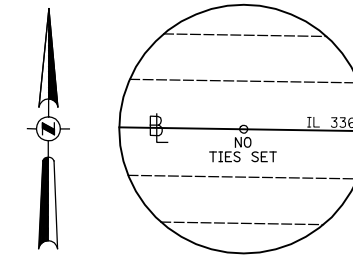
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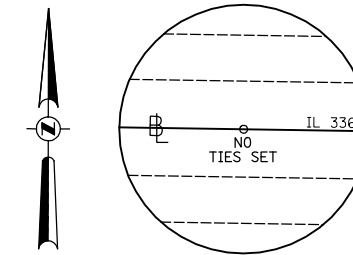
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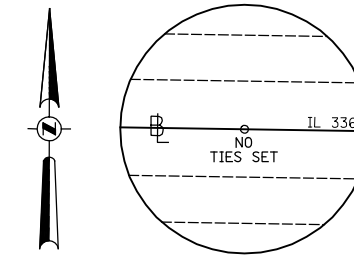
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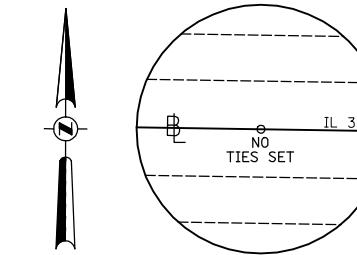
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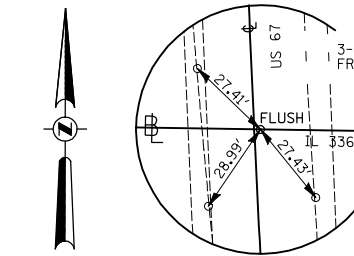
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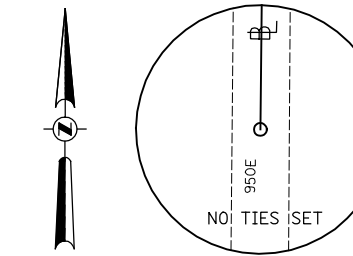
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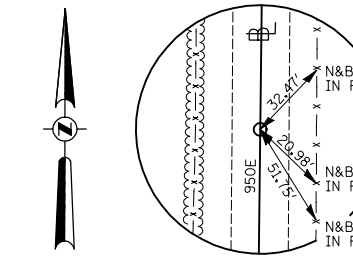
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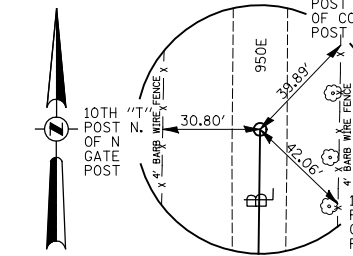
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 5/8" I-ROD W/ CAP
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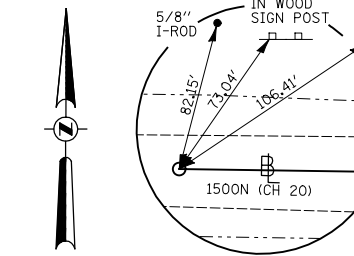
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950E (EMORY RD)
 SURVEY POINT
 N 1,398,853.0013
 E 2,141,653.0998



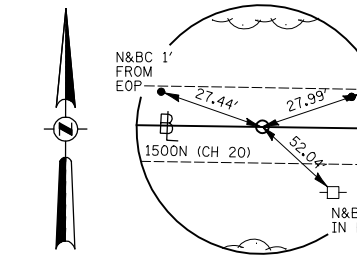
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950E (EMORY RD)
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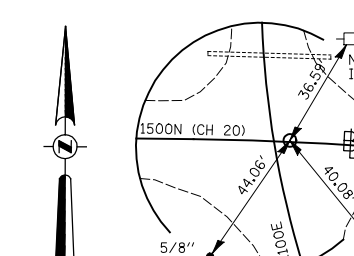
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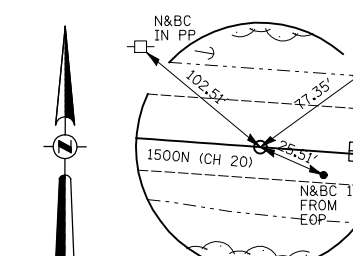
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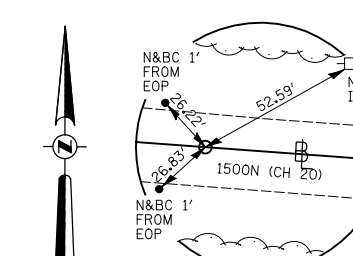
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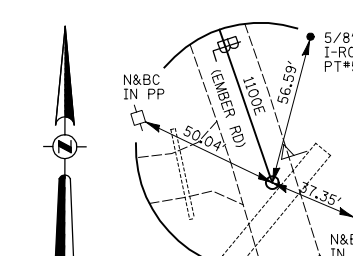
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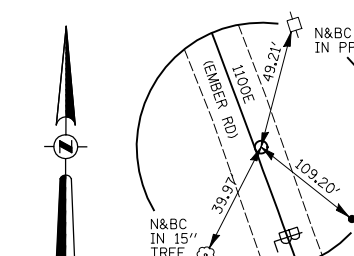
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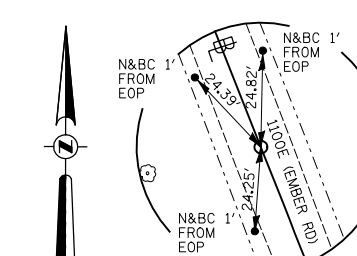
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1500N (CH 20)
 SURVEY SPIKE
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 E 2,150,823.2162



PC STA 6000+00.00
1100E (EMBER RD)
 SURVEY SPIKE
 N 1,395,918.6751
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PT STA 6001+60.99
1100E (EMBER RD)
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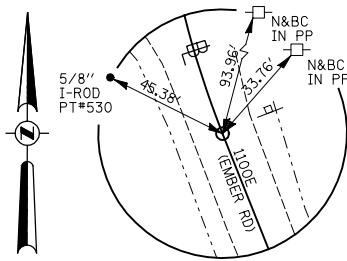
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1100E (EMBER RD)
 SURVEY SPIKE
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 E 2,150,044.4818

| | | | |
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| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
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| | | CHECKED - TJD | REVISED - |
| | | DATE - 1-30-2015 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

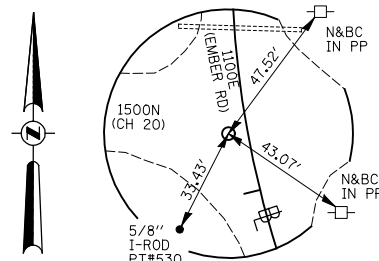
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| FAP ROUTE 407 (IL 336 /IL 110) ALIGNMENT TIES | | | |
| SCALE: N/A | SHEET NO. 3 OF 6 SHEETS | STA. N/A | TO STA. N/A |

| | | | | |
|---------------------------|-----------------------------|-----------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 91 |
| CONTRACT NO. 68B44 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



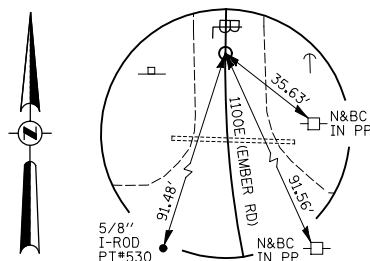
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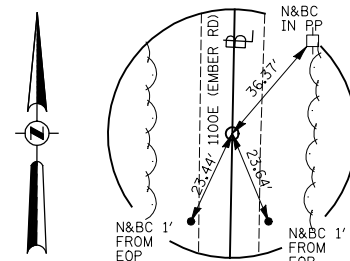
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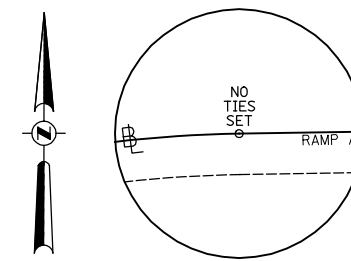
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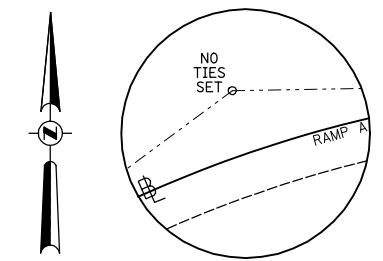
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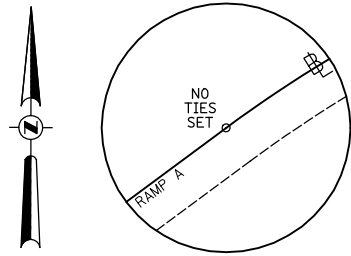
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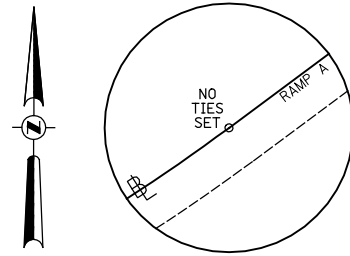
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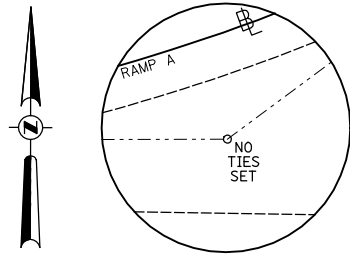
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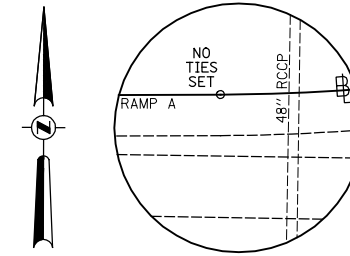
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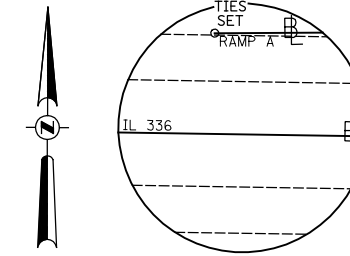
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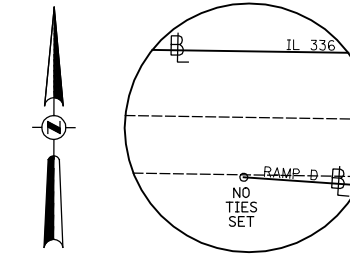
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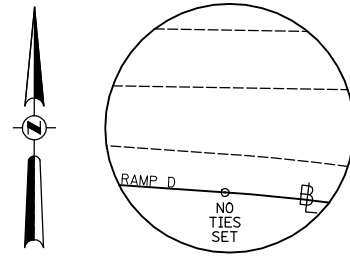
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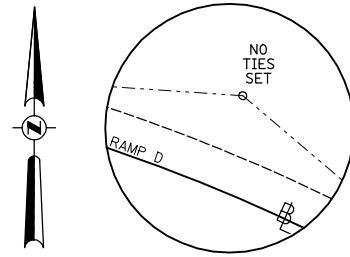
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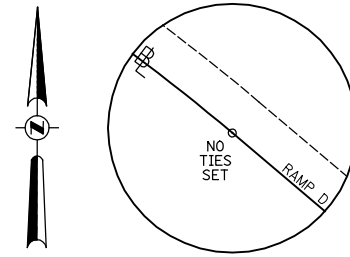
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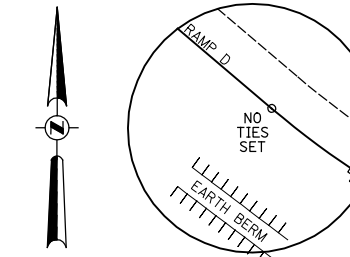
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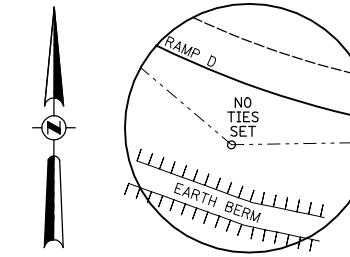
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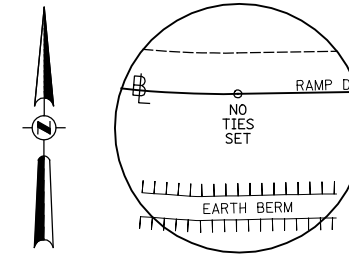
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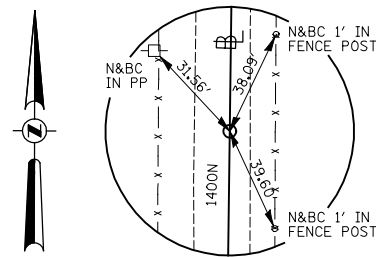
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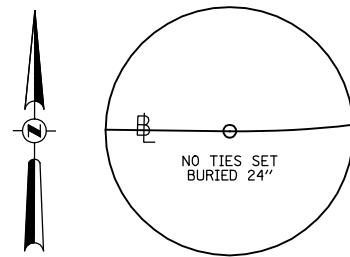
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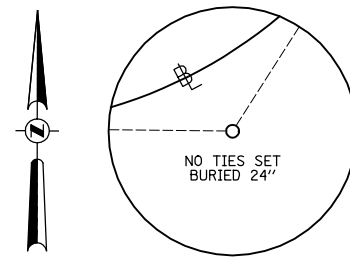
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SURVEY SPIKE
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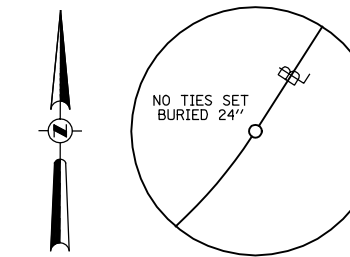
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5/8" I-ROD W/ CAP (BURIED 24")
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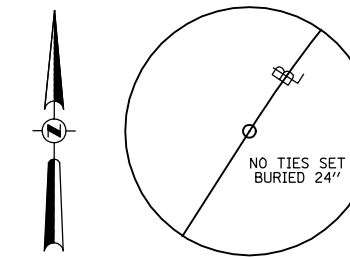
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5/8" I-ROD W/ CAP (BURIED 24")
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E 2,142,089.5145



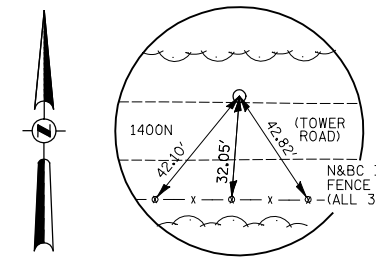
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1400N (TOWER ROAD)

5/8" I-ROD W/ CAP (BURIED 24")
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PC STA 17+58.04
1400N (TOWER ROAD)

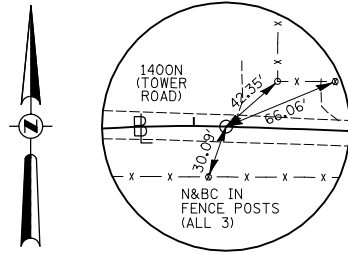
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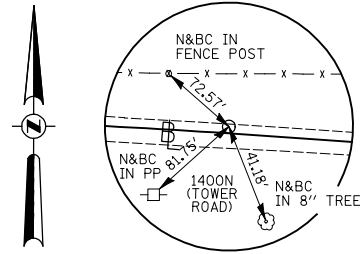
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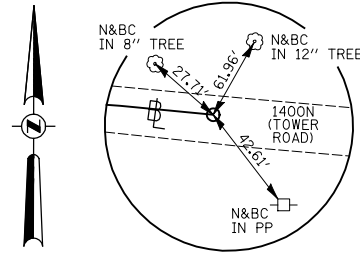
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|--|---------------------------|------------------|-----------|---|--|-------------------------|----------|-------------|---------------------------|-----------------------------|--------------|-----------|----|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) ALIGNMENT TIES | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\0468B44-sht-ATB.dgn | PLOT SCALE = 60.00' / IN. | DRAWN - JW | REVISED - | | SCALE: N/A | SHEET NO. 4 OF 6 SHEETS | STA. N/A | TO STA. N/A | 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 92 |
| | PLOT DATE = 1/26/2015 | CHECKED - TJD | REVISED - | | | | | | CONTRACT NO. 68B44 | | | | |
| | | DATE - 1-30-2015 | REVISED - | | | | | | ILLINOIS FED. AID PROJECT | | | | |



PCC STA 26+20.22
1400N (TOWER ROAD)
 SURVEY SPIKE
 N 1,391,267.3245
 E 2,143,011.9363



PI STA 28+64.27
1400N (TOWER ROAD)
 SURVEY SPIKE
 N 1,391,259.9452
 E 2,143,255.8736



PT STA 31+08.13
1400N (TOWER ROAD)
 SURVEY SPIKE
 N 1,391,235.8049
 E 2,143,498.7257

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|--|---------------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
| v:\transportation\2891\2-Lane Paving Plans\cadd sheets\D468B44-sht-ATB.dgn | | DRAWN - JW | REVISED - |
| | PLOT SCALE = 60.00' / IN. | CHECKED - TJD | REVISED - |
| | PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - |

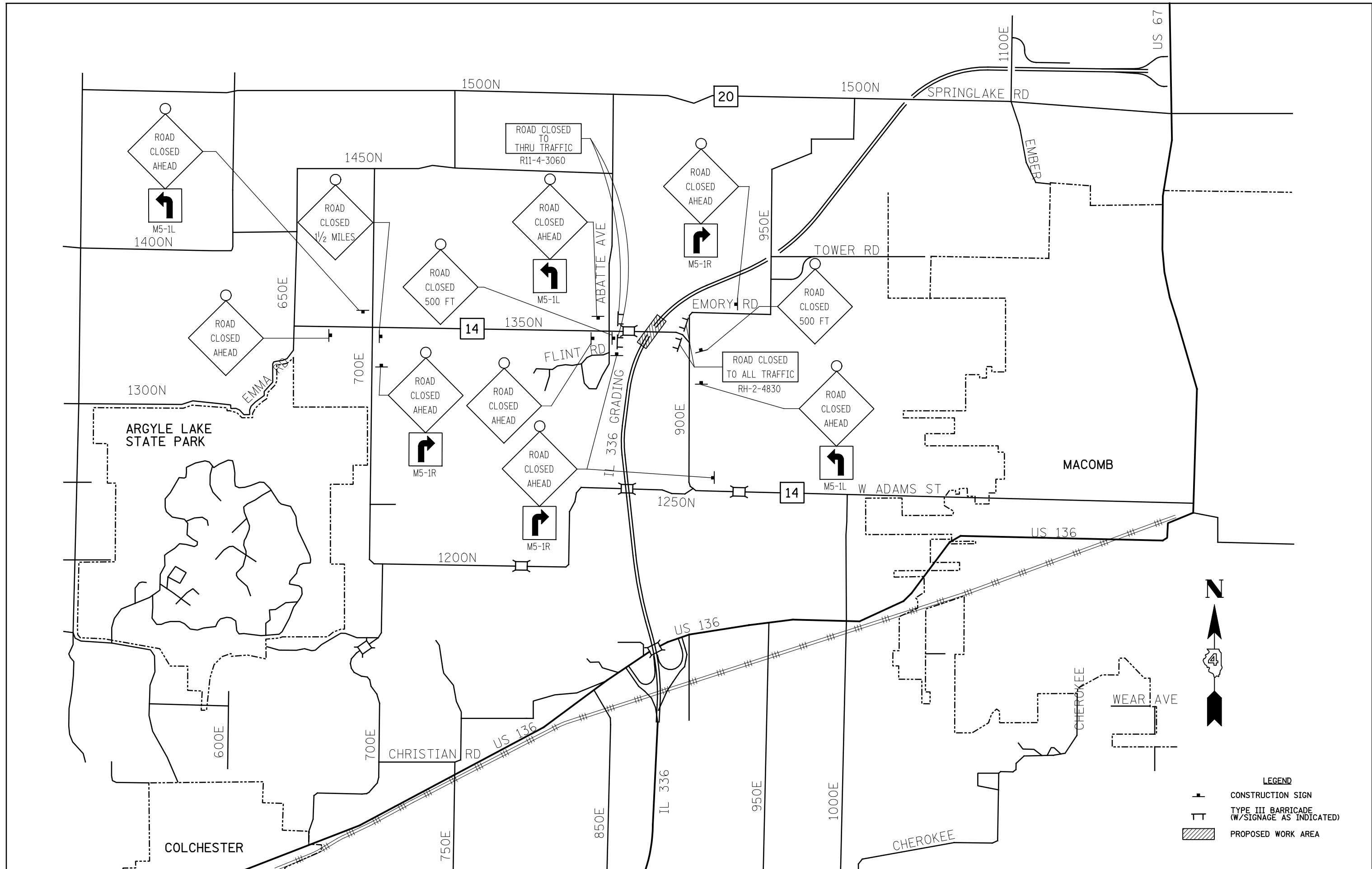
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | |
|---------------------------------------|-------------------------|----------|-------------|
| FAP ROUTE 407 (IL 336 /IL 110) | | | |
| ALIGNMENT TIES | | | |
| SCALE: N/A | SHEET NO. 5 OF 6 SHEETS | STA. N/A | TO STA. N/A |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------------------------|-----------|--------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 93 |
| CONTRACT NO. 68B44 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

BENCHMARKS

| BM # | ELEVATION | LOCATION | OFFSET | DESCRIPTION |
|-----------|-----------|-----------------------------------|------------|---|
| BM HEI 5 | 692.71 | IL RTE 336 STA ± 555+00 | ±245' RT | IDOT DISK ON S.E. ABUT. OF R.R. TRACK OVERPASS OVER EXISTING NB EXIT RAMP J |
| BM HEI 6 | 692.69 | IL RTE336 554+41.66 | 101' RT | IDOT DISK ON S.E. ABUT. OF R.R. TRACK OVERPASS OVER IL RTE 336 |
| BM HEI 7 | 692.56 | IL RTE 336 STA ± 552+75 | ±475' LT | IDOT DISK ON S.W. ABUT. OF R.R. TRACK OVERPASS OVER EXISTING SB ENTRANCE I |
| BM HEI 8 | 693.37 | US RTE 136 501+57.20 | 0' | IDOT DISK, STAMPED STA 15+287.998, U.S. 136 ☺ |
| BM HEI 9 | 693.84 | US RTE 136 511+09.06 | 0' | IDOT DISK, STAMPED STA 15+578.123, U.S. 136 ☺ |
| BM HEI 10 | 690.74 | US RTE 136 520+01.05 | 0' | IDOT DISK, STAMPED STA 15+850.000, U.S. 136 ☺ |
| BM HEI 11 | 658.91 | IL RTE 336 543+49.08 | 111' LT | CHIS. "X" ON 3'x3' BOX CULV. WEST SIDE OF EXISTING RAMP I @ STA STAMP 32+00 |
| BM HEI 12 | 662.55 | ADAMS ST (1250N) STA ± 1020+53 | 28' RT | R.R. SPIKE IN P.P. S. SIDE OF 1250N RD, W. SIDE OF DRV. HO. #8781 |
| BM HEI 14 | 693.68 | ADAMS ST (1250N) STA ±1005+48 | 30' LT | R.R. SPIKE IN P.P. N. SIDE OF 1250N RD, ±50' W. OF DRV. TO RESIDENCE LABELLED "BLACK" ON M.B. |
| BM HEI 31 | 706.83 | IL RTE 336 780+21.66 | 736' LT | RE-SET BM W-11 ON N.W. COR. OF DROP BOX OF SAME N. HDWL, X-RD CULV. @ HO. #1810 EMORY RD |
| BM HEI 35 | 695.54 | IL RTE 336 STA ± 643+00 | ±300' RT | CHIS. "D" ON TOP OF N.E. COR. CONC. PATIO/BOAT HOUSE, N. SIDE OF LAKE |
| BM HEI 37 | 619.41 | CH 14 (1350N) STA ± 2009+76 | ±12' RT | CHIS. "D" ON S.E. HDWL OF BRIDGE |
| BM HEI 38 | 699.04 | IL RTE 336 700+00.00 | 130' RT | TOP OF ROW MARKER, S. ROW LINE |
| BM HEI 40 | 710.68 | IL RTE 336 ± 731+01 | ±18.5' RT | SURVEY SPIKE IN S.E. SIDE OF PP @ S.E. QUAD OF 950E & 1400N |
| BM HEI 41 | 688.53 | IL RTE 336 ± 737+89 | ±2' LT | CHIS. "D" ON N. COR. OF S.W. INLET BOX |
| BM HEI 44 | 719.50 | IL RTE 336 781+20.00 | 0' | CHIS. "X" ON S.S.E. FLANGE BOLT OF F. HYD. @ 1500N & SPRINGLAKE PARK RD |
| BM HEI 47 | 705.56 | IL RTE 336 ± 842+35 | ±78.5' RT | CHIS. "D" ON HDWL @ S.E. QUAD OF IL 336 & 1100E |
| BM HEI 48 | 710.36 | US 67 ± 435+50 | ±109.7' LT | RR SPIKE IN PP AT THE N.W. QUAD OF US 67 & 1500N (CH 20) |
| BM HEI 49 | 698.18 | US 67 ± 444+18 | ±90.5' RT | CHIS. "D" ON CENTER OF E. END OF MULTI BOX CULVERT ±0.2 MI N. 1500N |
| BM HEI 50 | 702.97 | US 67 | | TOP OF ROW MRKR, E. SIDE OF US 67, ±300' N. OF FRONTAGE RD., ±8' S.W. OF GAS REG., ±0.4 MI. N. OF 1500N |
| BM 100 | 717.66 | 1500N | | CHIS. "X" ON S.W. CAP BOLT F. HYD., ±275M W. OF EMBER RD, N. SIDE OF 1500N RD |
| BM W-15 | 700.20 | CH 14 (900E) | 348' LT | 60d NAIL IN E. FACE OF P.P. S.W. QUAD. OF 1350N & 900E ROADS |
| BM W-17 | 621.59 | 1250N | | POLE SPIKE IN N.E. SIDE OF P.P., ±50M W. OF INT. OF W. ADAMS (900E) & 1250N., N. SIDE OF 1250N. |



LEGEND

| | |
|--|---|
| | CONSTRUCTION SIGN |
| | TYPE III BARRICADE (W/SIGNAGE AS INDICATED) |
| | PROPOSED WORK AREA |

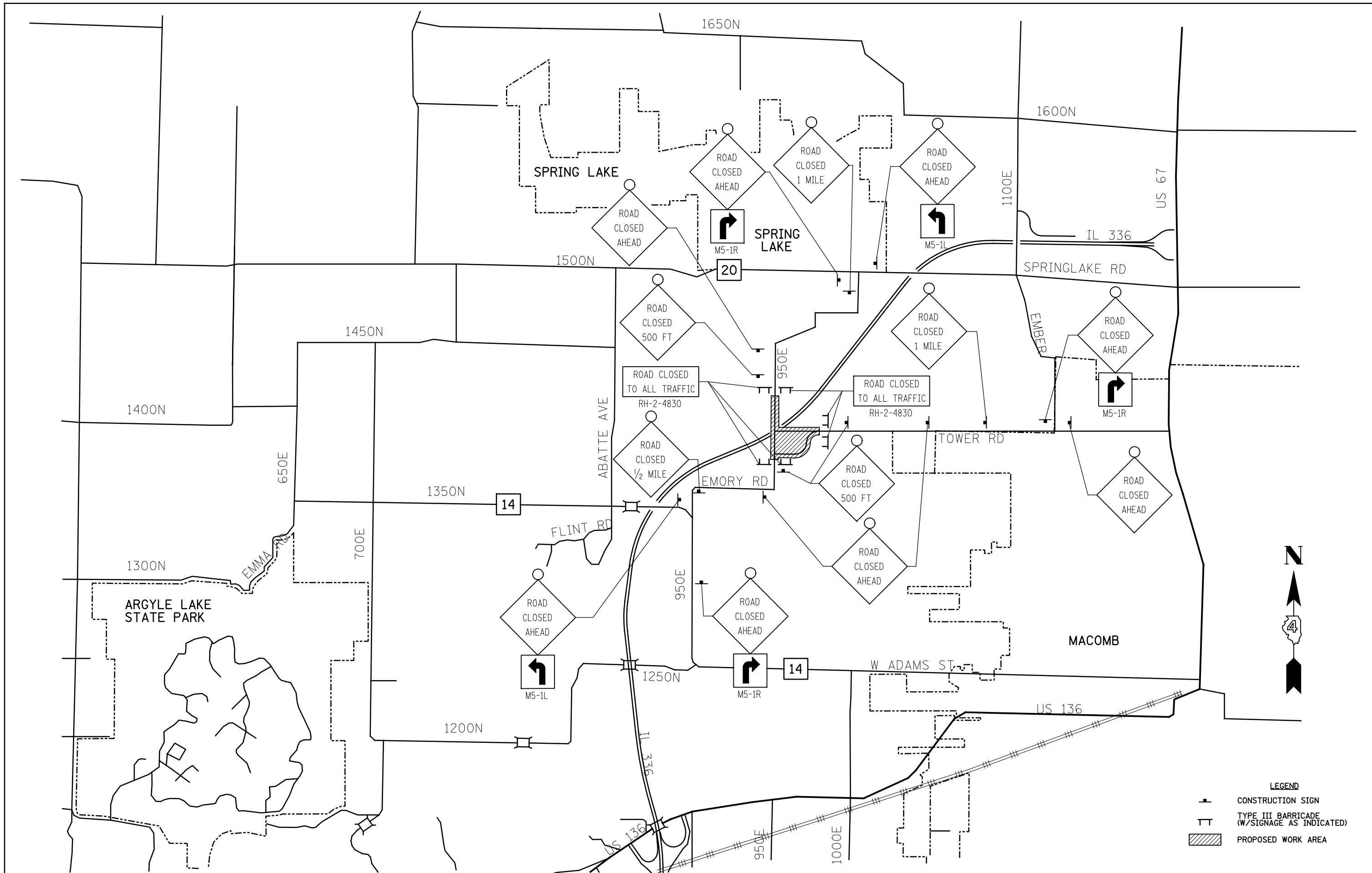
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| | PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - |




**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP ROUTE 407 (IL 336 /IL 110)
COUNTY HIGHWAY 14 MAINTENANCE OF TRAFFIC SCHEMATIC**

SCALE: NONE SHEET 1 OF 4 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------------------------|-----------|--------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | MCDONOUGH | 874 | 95 |
| | | | CONTRACT NO. 68B44 | |
| ILLINOIS FED. AID PROJECT | | | | |



- LEGEND**
-  CONSTRUCTION SIGN
 -  TYPE III BARRICADE (W/SIGNAGE AS INDICATED)
 -  PROPOSED WORK AREA

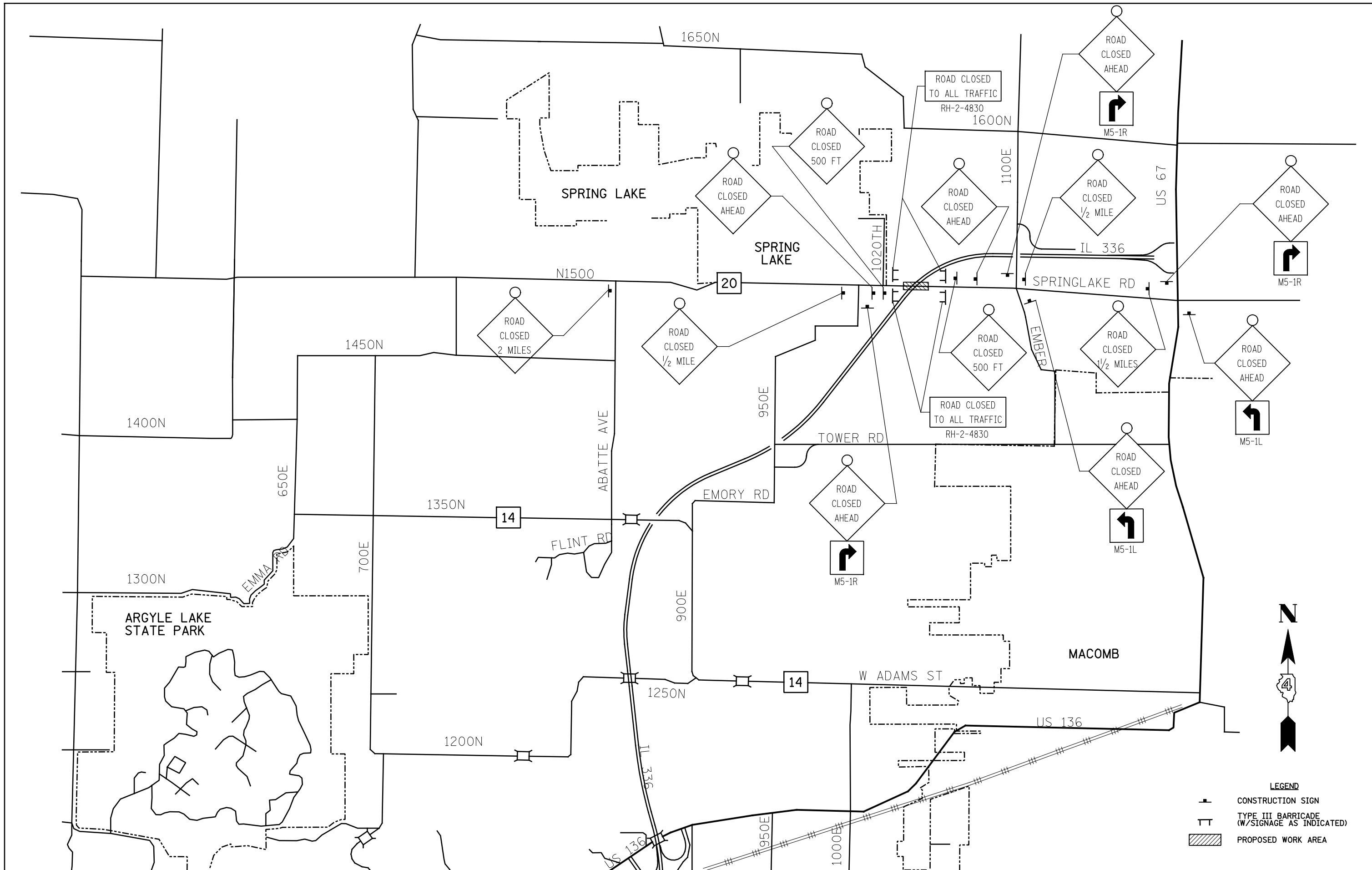
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| Default | | CHECKED - JRB | REVISED - |
| | PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP ROUTE 407 (IL 336 / IL 110)
950E & TOWER RD MAINTENANCE OF TRAFFIC SCHEMATIC**

SCALE: NONE SHEET 2 OF 4 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------------------------|-----------|--------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | MCDONOUGH | 874 | 96 |
| | | | CONTRACT NO. 68B44 | |
| ILLINOIS FED. AID PROJECT | | | | |



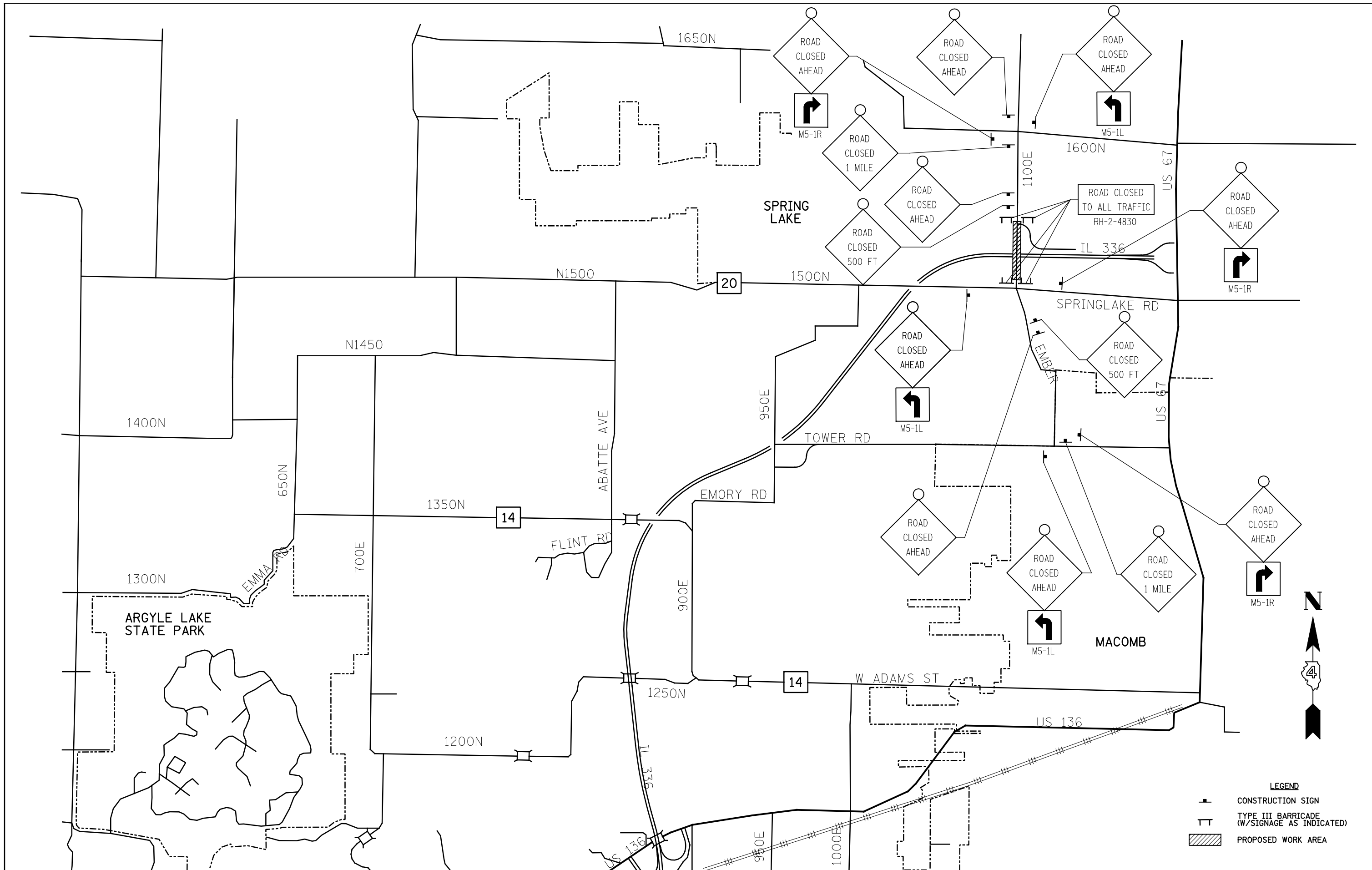
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| Default | | CHECKED - JRB | REVISED - |
| | PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP ROUTE 407 (IL 336 /IL 110)
COUNTY HIGHWAY 20 MAINTENANCE OF TRAFFIC SCHEMATIC**

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------------------------|-----------|--------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | MCDONOUGH | 874 | 97 |
| | | | CONTRACT NO. 68B44 | |
| ILLINOIS FED. AID PROJECT | | | | |

SCALE: NONE SHEET 3 OF 4 SHEETS STA. TO STA.



| | | | |
|--|-----------------------|------------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
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| | PLOT DATE = 1/26/2015 | DATE - 1-30-2015 | REVISED - |

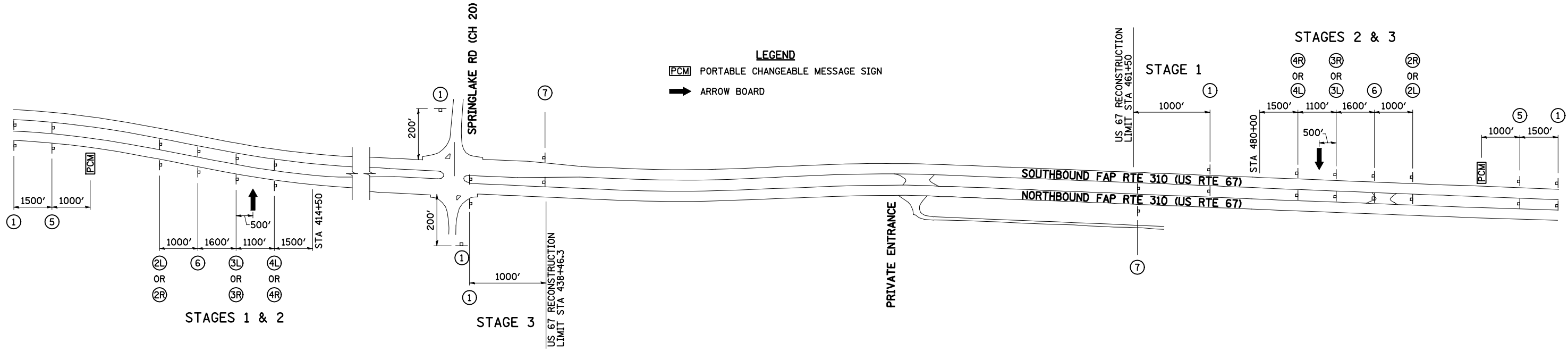
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP ROUTE 407 (IL 336 / IL 110)
1100E MAINTENANCE OF TRAFFIC SCHEMATIC**

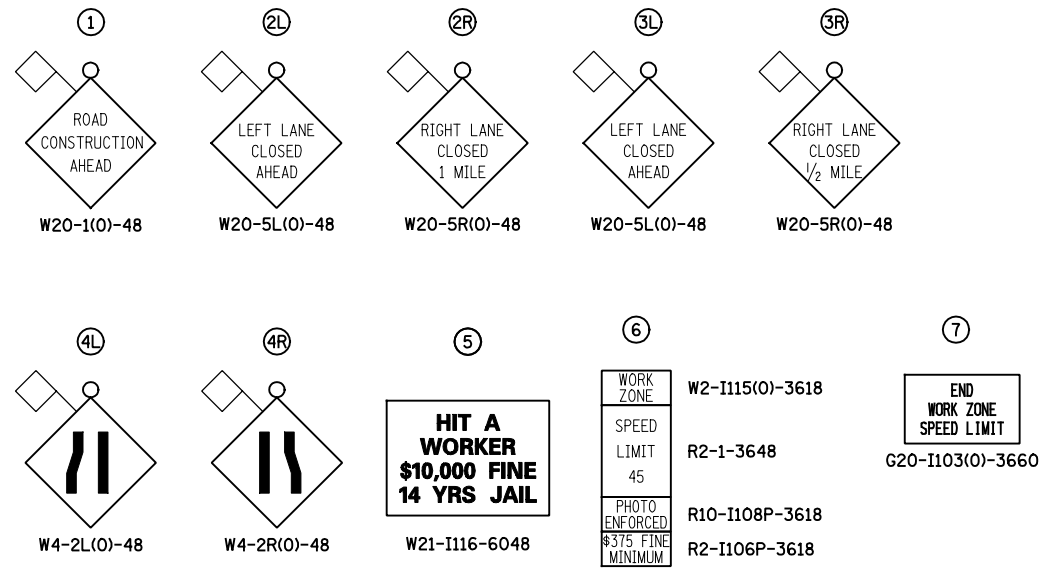
SCALE: NONE SHEET 4 OF 4 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------------------------|-----------|---------------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | MCDONOUGH | 874 | 98 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |

NOTE
SIGN IN ACCORDANCE WITH STD 701402
(POSTED SPEED 45 MPH)



LEGEND
 PORTABLE CHANGEABLE MESSAGE SIGN
 ARROW BOARD



TRAFFIC CONTROL AND PROTECTION

TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH THE TRAFFIC CONTROL PLAN, SECTION 701 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS.

EXISTING CONFLICTING TRAFFIC CONTROL SIGNS AND DEVICES WILL BE REMOVED OR COVERED BY THE CONTRACTOR AFTER THE PROPOSED TRAFFIC CONTROL REQUIREMENTS ARE MET, OR AS AUTHORIZED BY THE ENGINEER. SIGNS OR DEVICES LEFT IN PLACE AT THIS TIME ARE TO BE PROTECTED FROM DAMAGE BY THE CONTRACTOR.

DRUMS WITH WORKING LIGHTS SHALL BE REQUIRED TO DELINEATE THE TRAVELED WAY. THE MAXIMUM SPACING SHALL BE 100 FEET, CENTER TO CENTER ALONG TANGENTS; AND 25 OR 50 FEET, CENTER TO CENTER ALONG RADII AND TRANSITIONS. SEE STAGE CONSTRUCTION PLAN SHEETS.

SUPPLEMENTAL CONSTRUCTION SPEED LIMIT SIGNING IS TO BE INSTALLED AT THE LOCATIONS AS DIRECTED BY THE ENGINEER FOR MOTORISTS ENTERING FROM SIDEROADS.

THE FOLLOWING TRAFFIC CONTROL STANDARDS ARE THE MINIMUM REQUIREMENTS FOR THE TRAFFIC CONTROL FOR THIS WORK: 701400, 701401, 701402, 701406, 701901 AND 704001.

PRE-STAGE 1 CONSTRUCTION

CLOSE INSIDE NORTHBOUND LANE ON US 67 USING STANDARD 701401 AND CONSTRUCT TEMPORARY PAVEMENT FROM STA 435+76.0 TO STA 463+70.0.

CLOSE SOUTHBOUND LANES, ONE LANE AT A TIME, USING STANDARD 701401.

CONSTRUCT MANHOLES, STORM SEWER AND CLASS D PATCH ACROSS SOUTHBOUND LANES FROM STA 454+26.4, 12.9' LT TO STA 454+66.0, 88.8' LT AND CONSTRUCT STORM SEWER FROM STA 454+66.0, 88.8' LT TO STA 455+66.4, 111.8' LT.

CONSTRUCT PROPOSED FRONTAGE ROAD CONNECTOR TO US 67 NORTHBOUND LANE AT STA 461+00.0 RT.

STAGE 1 MAINTENANCE OF TRAFFIC

MAINTAIN BOTH SOUTHBOUND LANES OF TRAFFIC.

CLOSE OUTSIDE NORTHBOUND LANE ON US 67 AND MAINTAIN ONE LANE OF TRAFFIC ON INSIDE LANE AND TEMPORARY PAVEMENT.

STAGE 1 CONSTRUCTION

REMOVE EXISTING PAVEMENT FROM NORTHBOUND LANES OF US 67 UP TO STAGE REMOVAL LINE AS SHOWN IN STAGE 1 PLANS AND CONSTRUCT SUB-BASE LAYER.

CONSTRUCT PROPOSED PAVEMENT, GUTTER AND DITCHES FOR OUTSIDE NORTHBOUND LANE OF US 67 FROM STA 438+46.3 TO STA 461+50.0.

INSTALL STORM SEWER UNDER THE OUTSIDE NORTHBOUND LANE OF US 67.

STAGE 2 MAINTENANCE OF TRAFFIC

CLOSE INSIDE SOUTHBOUND LANE OF US 67 AND MAINTAIN ONE LANE OF TRAFFIC ON OUTSIDE LANE.

CLOSE INSIDE NORTHBOUND LANE OF US 67 AND MAINTAIN ONE LANE OF TRAFFIC ON NEW PAVEMENT FROM STAGE 1.

STAGE 2 CONSTRUCTION

REMOVE EXISTING PAVEMENT FROM SOUTHBOUND LANES OF US 67 UP TO STAGE REMOVAL LINE AS SHOWN IN STAGE 2 PLANS AND CONSTRUCT SUB-BASE LAYER.

INSTALL STORM SEWER ALONG INSIDE NORTHBOUND AND SOUTHBOUND LANES OF US 67.

CONSTRUCT PROPOSED CURB AND GUTTER AND PAVEMENT FOR INSIDE NORTHBOUND AND SOUTHBOUND LANES OF US 67 FROM STA 438+46.3 TO STA 461+50.0.

STAGE 3 MAINTENANCE OF TRAFFIC

CLOSE OUTSIDE SOUTHBOUND LANE OF US 67 AND MAINTAIN ONE LANE OF TRAFFIC ON NEW PAVEMENT FROM STAGE 2.

MAINTAIN BOTH NORTHBOUND LANES OF TRAFFIC ON NEW PAVEMENT.

STAGE 3 CONSTRUCTION

CONSTRUCT PROPOSED PAVEMENT AND DITCHES FOR OUTSIDE SOUTHBOUND LANE OF US 67 FROM STA 438+46.3 TO STA 461+50.0.

POST-STAGE 3 CONSTRUCTION

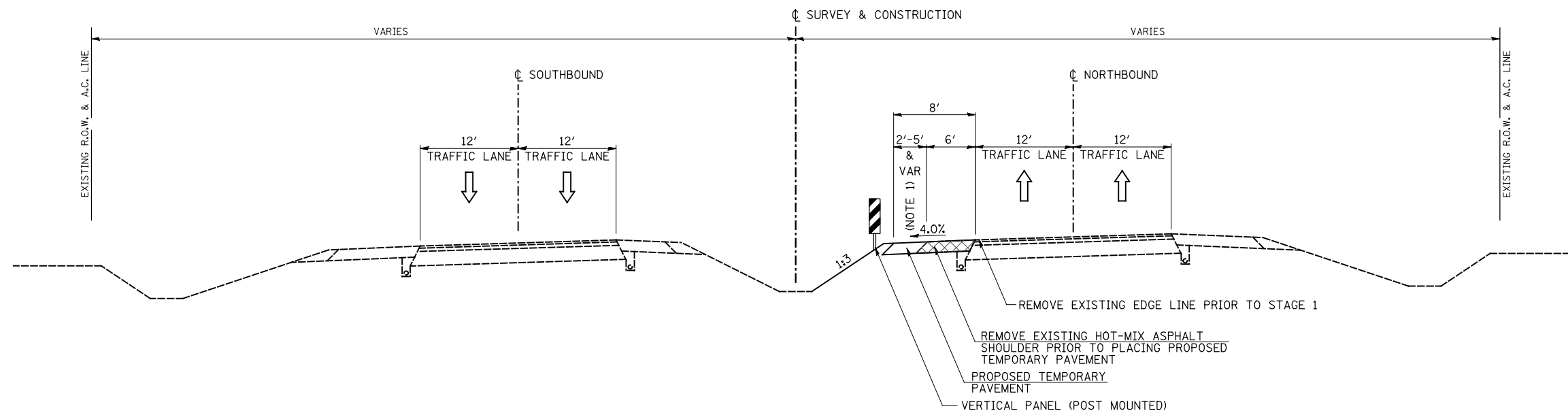
CLOSE INSIDE NORTHBOUND AND SOUTHBOUND LANES ON US 67 USING STANDARD 701401 AND CONSTRUCT PROPOSED EARTH MEDIAN, AND CURB & GUTTER FROM STA 435+24.8 TO STA 438+46.3.

CONSTRUCT MILLING AND RESURFACING FROM STA 429+50 TO STA 438+46.3 UTILIZING STANDARD 701406.

INSTALL FINAL PAVEMENT MARKING AND PERMANENT SEEDING.

| | | | | | | | | | | | |
|--|-----------------------------|----------------|-----------|---|--|---------------------------|---------------------------|-----------|--------------|-----------|--|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FAP ROUTE 407 (IL 336 /IL 110) TRAFFIC CONTROL AND STAGING PLAN - FAP ROUTE 310 (US 67) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| W:\Transportation\2891\2-Lane Paving Plans\CADD Sheets\D468B44-sht-staging-US67-01.dwg | PLOT SCALE = 40.0008' / IN. | CHECKED - JRB | REVISED - | | | 407 | 55(3(PV,HB(2-6)B,-1,B-2)) | McDONOUGH | 874 | 99 | |
| PLOT DATE = 2/4/2015 | DATE - 1-30-2015 | REVISI | REVISED - | | | CONTRACT NO. 68B44 | | | | | |
| | | | | | | ILLINOIS FED. AID PROJECT | | | | | |

SCALE: 1"=200' SHEET 1 OF 18 SHEETS STA. N/A TO STA. N/A

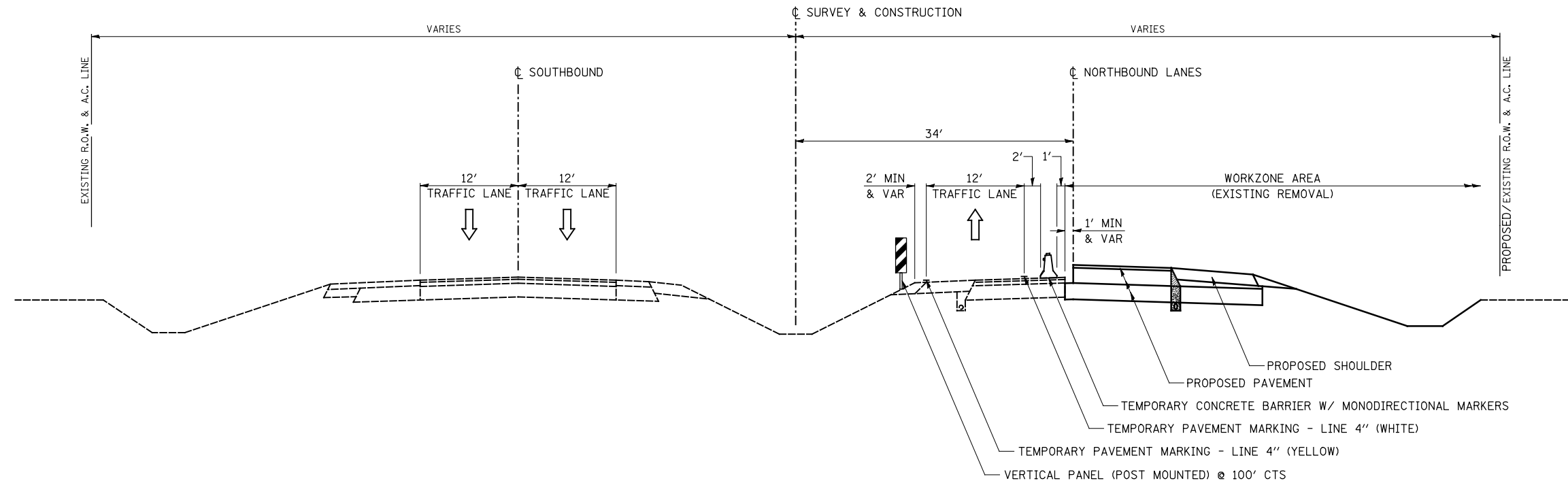


PRE-STAGE 1 TYPICAL SECTION
FAP ROUTE 310 (US 67)

STA 435+76.00 TO STA 463+70.00

NOTES:

- EXTRA 2' TO 5' WIDTH PROVIDED FROM STA 439+25 TO STA 446+00.
 VARIES FROM 2.0' @ STA 440+92.00 TO 5.0' @ STA 442+70.00.
 VARIES FROM 5.0' @ STA 444+10.00 TO 2.0' @ STA 445+22.00.



STAGE 1 TYPICAL SECTION
FAP ROUTE 310 (US 67)

STA 438+46.31 TO STA 461+50.00

| | | | |
|---|-------------------|----------------|-----------|
| FILE NAME = | USER NAME = JDeen | DESIGNED - JRB | REVISED - |
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| PLOT SCALE = 16.000' / IN. | DATE - 1-30-2015 | REVISIONS | |
| PLOT DATE = 2/4/2015 | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 407 (IL 336 /IL 110)
PRE-STAGE 1 & STAGE 1 TYPICAL SECTIONS - FAP ROUTE 310 (US 67)

SCALE: N/A SHEET NO. 2 OF 18 SHEETS STA. N/A TO STA. N/A

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------------------------|-----------|---------------------------|-----------|
| 407 | 55C3(PV,HB(2-6);B,B-1,B-2)] | McDONOUGH | 874 | 100 |
| CONTRACT NO. 68B44 | | | ILLINOIS FED. AID PROJECT | |