

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

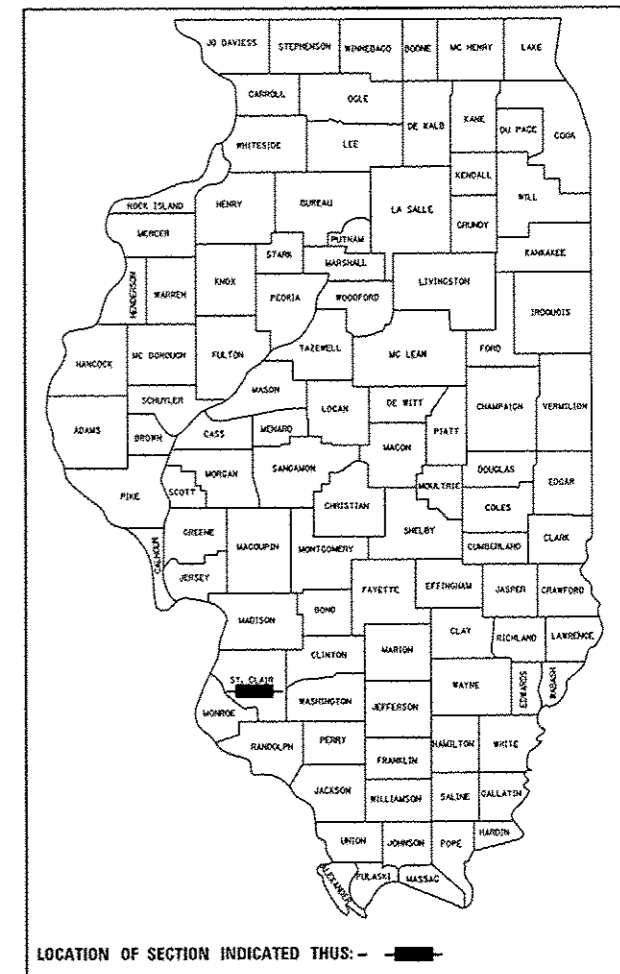
FAI ROUTE 64 (I-64)
SECTION 82-(4,5,6,7)RS-1

RESURFACING, MICROSURFACING, & PATCHING
ST. CLAIR COUNTY

C-98-023-15

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	1
		ILLINOIS	CONTRACT NO. 76H71	

D-98-028-15



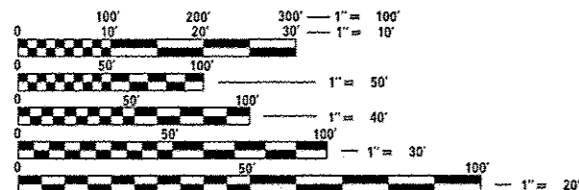
FOR INDEX OF SHEETS, SEE SHEET NO. 2

ADT

IL 157 to IL 159
2013 ADT = 77,700 (ACTUAL)
2015 ADT = 79,300 (ESTIMATED)
2035 ADT = 96,800 (ESTIMATED)
SU = 2.2%
MU = 8.8%

IL 159 to US 50
2013 ADT = 67,900 (ACTUAL)
2015 ADT = 69,300 (ESTIMATED)
2035 ADT = 84,600 (ESTIMATED)
SU = 2.1%
MU = 9.9%

US 50 to GREENMOUNT ROAD
2013 ADT = 56,600 (ACTUAL)
2015 ADT = 57,700 (ESTIMATED)
2035 ADT = 70,400 (ESTIMATED)
SU = 2.2%
MU = 9.9%

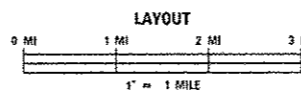
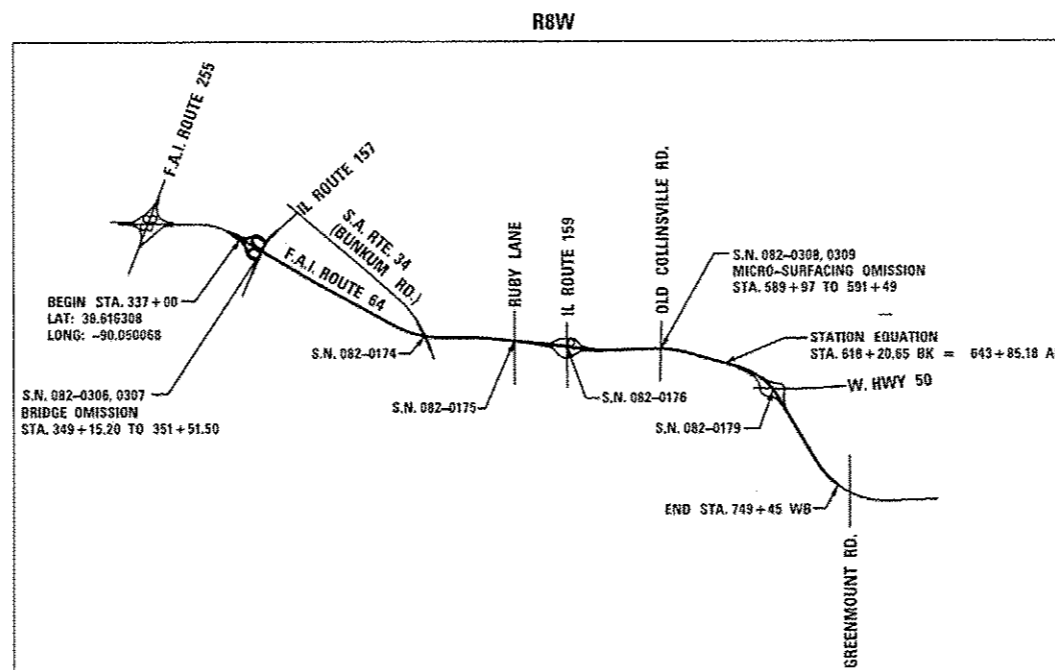


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: TIM PADGETT 618-346-3325
PROJECT MANAGER: JOHNSON DUROSIER 618-346-3208

CONTRACT NO. 76H71



GROSS LENGTH = 41245 FT. = 7.812 MILE
NET LENGTH = 40856.7 FT. = 7.738 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED *May 26 2015*
Jeffrey T. K...
DEPUTY DIRECTOR OF HIGHWAYS, REGION 5/ENGINEER

May 8 2015
Edward D. Baranowski, PE
ENGINEER OF DESIGN AND ENVIRONMENT

May 8 2015
Omer Osman, PE
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

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61-63	DETAIL SHEET

LIST OF STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442001-04	CLASS A PATCHES
701400-08	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-09	LANE CLOSURE, FREEWAY/EXPRESSWAY
701406-09	LANE CLOSURE FREEWAY/EXPRESSWAY, DAY OPERATIONS ONLY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCES OR EXIT RAMP, FOR SPEEDS > 45 MPH
701426-07	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS FOR SPEEDS ≥ 45 MPH
701428	TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY/EXPRESSWAY
701446-06	TWO LANE CLOSURE FREEWAY/EXPRESSWAY
701901-04	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

GENERAL NOTES

① ILLINOIS STATE LAW REQUIRES A 48-HOUR NOTICE BE GIVEN TO ALL UTILITIES WITHIN THE PROJECT AREA BEFORE DIGGING. FIELD MARKING OF FACILITIES MAY BE OBTAINED BY CONTACTING J.U.L.I.E. OR FOR NON-MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:

UTILITY	ABOVE GROUND	BELOW GROUND
•AMEREN ILLINOIS	X	X
•AT&T	X	X
•BUCKEYE PARTNERS L.P.-WOOD RIVER PIPELINE		X
•CASEYVILLE TOWNSHIP ADVANCED WASTEWATER TREATMENT SYSTEM		X
•VILLAGE OF CASEYVILLE		X
•CHARTER COMMUNICATIONS	X	X
•CLEARWAVE COMMUNICATIONS	X	X
•CITY OF FAIRVIEW HEIGHTS		X
•ILLINOIS AMERICAN WATER COMPANY		X
•MEDIACOM	X	X
•CITY OF O'FALLON		X
•MCLEOD TELECOMMUNICATIONS SERVICES, LLC	X	X
•VILLAGE OF SHILOH	X	X

MEMBERS OF J.U.L.I.E. CALL TOLL FREE (800) 892-0123 OR 811 AND ARE INDICATED BY *. NON- J.U.L.I.E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.

② THE SURFACE REMOVAL THICKNESS OF THE EXISTING HMA OVERLAY MAY VARY FROM THAT SHOWN ON THE PLANS. NO ADDITIONAL PAYMENT WILL BE MADE FOR ACCOMMODATING THE VARIATION IN EXISTING HMA THICKNESS.

③ THE THICKNESS OF THE BITUMINOUS MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.

④ ALL AREAS DISTURBED FOR ANY REASON SHALL BE PERMANENTLY SEEDDED AS DIRECTED BY THE ENGINEER. ALL AREAS DISTURBED BY THE CONTRACTOR OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE SEEDDED AT THE CONTRACTOR'S EXPENSE.

⑤ THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

⑥ "ROAD CONSTRUCTION AHEAD" SIGNS SHALL BE PLACED AT THE BEGINNING AND END OF THE PROJECT PLUS THE INTERSECTING SIDE ROADS AND RAMP AND WILL BE INCLUDED IN THE TRAFFIC CONTROL PAY ITEMS. ALL CONSTRUCTION SIGNS SHALL BE 48" FLUORESCENT ORANGE.

⑦ THE ENGINEER SHALL NOTIFY DISTRICT 8 OPERATIONS TO DISPLAY APPLICABLE TRAFFIC CONTROL MESSAGES ON THE IDOT MESSAGE BOARDS IN ADVANCE.

⑧ TWO CHANGEABLE MESSAGE SIGNS SHALL BE PLACED AT EACH INTERCHANGE TWO WEEKS IN ADVANCE OF THE START OF THE PROJECT AS WELL AS THE DURATION OF THE PROJECT AS WELL AS ONE CHANGEABLE MESSAGE SIGN AT EACH END OF THE PROJECT LIMITS.

⑨ SINCE THE ADT>5,000, THE AGGREGATE FOR THE SURFACE LIFT OF MICRO-SURFACING SHALL MEET THE FRICTION AGGREGATE REQUIREMENTS FOR MIXTURE "E" IN ARTICLE 1004.03(A).

⑩ ALL EXISTING PAVEMENT MARKING WILL NEED TO BE REMOVED PRIOR TO THE PLACING OF THE MICRO-SURFACE OVERLAY.

⑪ THE PAVEMENT MARKING SHOWN IN THE PLANS IS APPROXIMATE. THE LOCATIONS OF THE PROPOSED PAVEMENT MARKING SHOULD MATCH EXISTING FIELD CONDITIONS.

⑫ MICRO-SURFACING SHALL NOT COVER EXISTING RUMBLE STRIP IN THE SHOULDER.

⑬ THE INTENT OF THE PROPOSED MILLING IS TO LEAVE THE EXISTING RUMBLE STRIPS. THE WIDTH OF THE PROPOSED MILLING AT THE EDGE OF PAVEMENT SHALL VARY AS DIRECTED BY THE ENGINEER TO AVOID DAMAGE TO THE EXISTING RUMBLE STRIPS ON THIS PROJECT.

⑭ WHEN THE VERTICAL MILLED FACE BETWEEN TRAVELED LANES OR AT THE EDGE OF PAVEMENT CREATES A DROP-OFF GREATER THAN 1.5 INCHES AND LESS THAN/EQUAL TO 4 INCHES FOR MORE THAN 0.5 MILES IN LENGTH OF DROP-OFF EXPOSURE IN A WORK ZONE OR MORE THAN 48 HOURS, THE CONTRACTOR SHALL CONSTRUCT A TEMPORARY WEDGE OR MILLED 1:3 (MINIMUM) SLOPED EDGE ALONG THE DROP-OFF. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST PER SQUARE YARD FOR HOT-MIX ASPHALT SURFACE REMOVAL, 2 INCH.

⑮ THE CONTRACTOR SHALL COMPLETE THE FULL DEPTH PATCHING PRIOR TO THE RESURFACING AND MICRO-SURFACING. IN THE PAVEMENT AREA TO BE RESURFACED, THE CONTRACTOR SHALL COMPLETE THE LONGITUDINAL PARTIAL DEPTH REMOVAL/PATCHING AFTER THE HMA SURFACE REMOVAL. SEQUENCED MILLING, PATCHING, AND RESURFACING OF THE TRAFFIC LANES WILL BE ALLOWED.

⑯ THE DETAIL FOR THE THE BUTT JOINT ANTICIPATES RESURFACING PRIOR TO THE PLACEMENT OF MICRO-SURFACING.

⑰ THE CONTRACTOR SHALL CLEAN OUT MILLING/DEBRIS FROM THE RUMBLE STRIPS. THIS OPERATION WILL BE INCLUDED IN THE HMA REMOVAL PAY ITEM.

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE TO THIS PROJECT:

ROUTE	64 (I-64)
SECTION	82-(4,5,6,7)RS-1
COUNTY	ST. CLAIR
CONTRACT	76H71

DESCRIPTION	Resurfacing, patching, and micro-surfacing I-64 from West of IL 157 to West of Greenmount Road
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ADT (Construction Yr):	69300
MUZ	9.9
SUZ	2.1
20 YR. ESAL'S	43.62

MIXTURE USE	SURFACE/SMA	PARTIAL DEPTH	MICROSURFACING, 2 PASSES
AC/PG	SBS PG 76-22	SBS PG 76-22	
RAP % (MAX)	SEE SPECIAL PROVISION	SEE SPECIAL PROVISION	
DESIGN AIR VOIDS	4.0% @ Nges=80	4.0% @ Nges=90	
MIX COMPOSITION (gradation)	IL 12.5	IL 9.5	
FRICTION AGG	SMA	"E"	"E"
QUALITY MGMT PROGRAM	PPF	OC/OA	

STATION EQUATIONS

STA. 618+20.85 BK = STA. 643.85.18 AH

COMMITMENTS

NONE

FILE NAME : c:\pwork\p104\duroaer\10419849\0871\shtr-gennote.dgn	USER NAME : duroaerj	DESIGNED -	REVISIONS -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, HIGHWAY STANDARDS, GENERAL NOTES, COMMITMENTS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE : 100.0000 ' / in.	DRAWN -	REVISIONS -	64			82-(4,5,6,7)RS-1	ST CLAIR	63	2	
PLOT DATE : 3/27/2015	CHECKED -	REVISIONS -	CONTRACT NO. 76H71							
	DATE -	REVISIONS -	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							

MINIMUM NUMBER OF LANES TO BE LEFT OPEN SUNDAY THRU SATURDAY, SEE NOTE *

	12:00 AM	1:00 AM	2:00 AM	3:00 AM	4:00 AM	5:00 AM	6:00 AM	7:00 AM	8:00 AM	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	6:00 PM	7:00 PM	8:00 PM	9:00 PM	10:00 PM	11:00 PM
64 255-159																								
WB	ONE LN OPEN**					ALL LN OPEN					TWO LN OPEN				ALL LN OPEN				TWO LN OPEN		ONE LN OPEN**			
EB	ONE LN OPEN**					TWO LN OPEN					ALL LN OPEN				TWO LN OPEN		ONE LN OPEN**							
64 159-GREENMOUNT																								
WB	ONE LN OPEN**					ALL LN OPEN		TWO LN OPEN								ONE LN OPEN**								
EB	ONE LN OPEN**					TWO LN OPEN								ALL LN OPEN		ONE LN OPEN**								

*ALL LANES TO REMAIN OPEN FROM FRIDAY 12 PM TO SATURDAY 12 AM
SEE THE SPECIAL PROVISIONS FOR PEAK HOUR RESTRICTIONS

**THE CONTRACTOR SHALL UTILIZE TRAFFIC CONTROL AND PROTECTION,
(SPECIAL) WHEN TWO LANES ARE CLOSED TO TRAFFIC.

100% STATE

CONSTR. CODE

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY
				0005 URBAN
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	108350	108350
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	1603	1603
40600990	TEMPORARY RAMP	SO YD	305	305
40603153	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	TON	21192	21192
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SO YD	189214	189214
44200610	CLASS A PATCHES, TYPE II, 13 INCH	SO YD	125	125
44200612	CLASS A PATCHES, TYPE III, 13 INCH	SO YD	19	19
44200614	CLASS A PATCHES, TYPE IV, 13 INCH	SO YD	272	272
44213000	PATCHING REINFORCEMENT	SO YD	416	416
44213200	SAW CUTS	FOOT	1661	1661
44213204	TIE BARS 3/4"	EACH	97	97
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	4	4
67100100	MOBILIZATION	L SUM	1	1
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	12	12

FILE NAME =	USER NAME = durosarj	DESIGNED -	REVISED -
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/27/2015	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82(4,5,6,7)RS-1	ST. CLAIR	63	4
			CONTRACT NO. 76H71	
ILLINOIS FED. AID PROJECT				

100% STATE

CONSTR. CODE

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	
				0005	URBAN
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1	1	
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1	
70100815	TRAFFIC CONTROL AND PROTECTION, STANDARD 701446	L SUM	1	1	
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	40	40	
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	18646	18646	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	191568	191568	
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	12909	12909	
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	13047	13047	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	90530	90530	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	86518	86518	
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	8280	8280	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	13047	13047	
* 78004210	PERFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 4"	FOOT	105050	105050	

* SPECIALTY ITEM

FILE NAME =	USER NAME = durasier.j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
or\p-work\pedit\durasier.j\0419049\007	71-ht-500.dgn	DRAWN -	REVISED -		64	82(4,5,6,7)RS-1	ST. CLAIR	63	5				
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	PLOT DATE = 3/27/2015	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

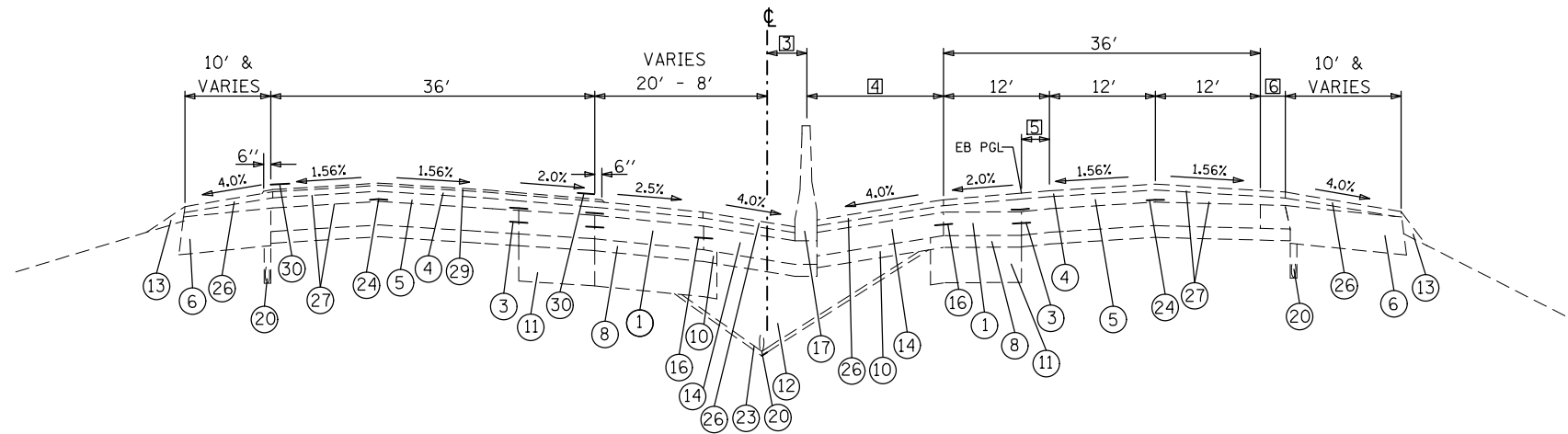
100% STATE
CONSTR. CODE

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	100% STATE	
				ROADWAY 0005	URBAN
* 78004240	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	FOOT	4629	4629	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	4438	4438	
78300100	PAVEMENT MARKING REMOVAL	50 FT	62643	62643	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	4438	4438	
X4405020	LONGITUDINAL PARTIAL DEPTH REMOVAL 2"	FOOT	35591	35591	
X4405030	LONGITUDINAL PARTIAL DEPTH REMOVAL 3"	FOOT	23755	23755	
X7D10410	SPEED DISPLAY TRAILER	CAL MO	8	8	
X4420900	LONGITUDINAL PARTIAL DEPTH PATCHING	TON	1773	1773	
Z0034105	MATERIAL TRANSFER DEVICE	TON	21192	21192	
X4480200	MICRO-SURFACING, 2 PASSES, TYPE II	50 YD	171831	171831	
φ Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	Hour	2000	2000	
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1	

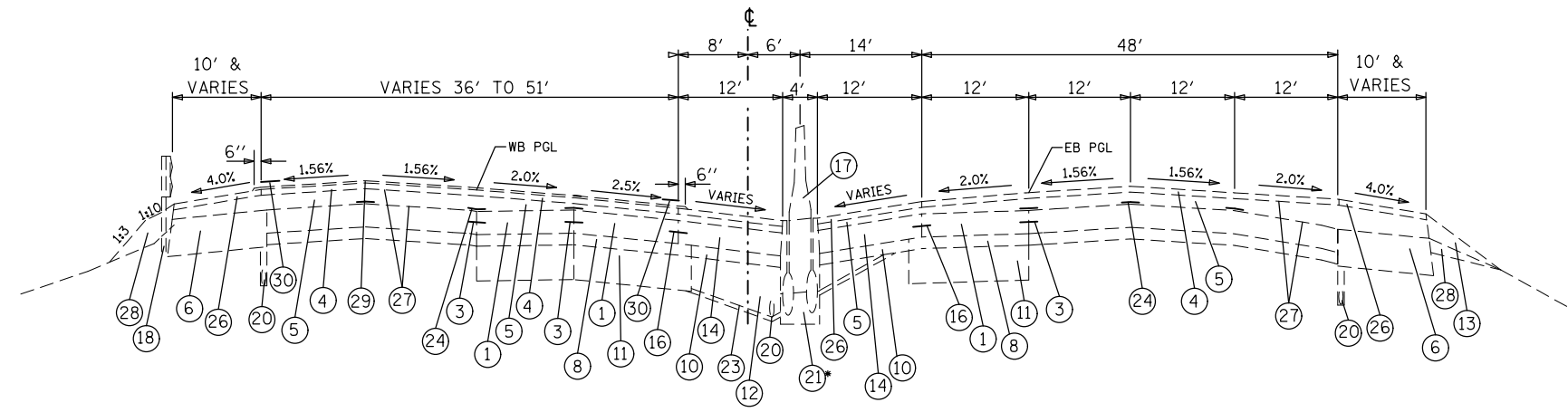
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* SPECIALTY ITEM

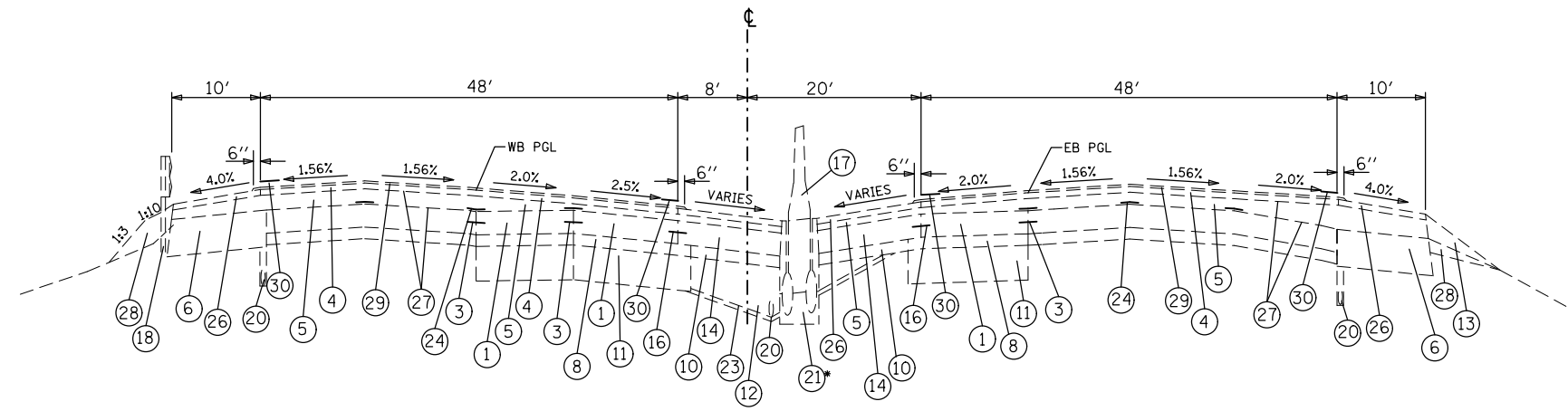
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STA. 337+00 TO STA. 347+89.14 EB
 STA. 337+00 TO STA. 346+66.94 WB
 3 VARIES 0' (STA. 337+00) TO 6' (STA. 345+51.58)
 4 VARIES 8' (STA. 337+00) TO 14' (STA. 346+01.58)
 5 VARIES 12' (STA. 337+00) TO 0' (STA. 346+01.58)
 6 VARIES



STA. 347+89.14 TO STA. 351+51.50 EB
 STA. 346+66.94 TO STA. 351+51.50 WB
 BRIDGE OMISSION WB STA. 349+15.20 TO 351+51.50

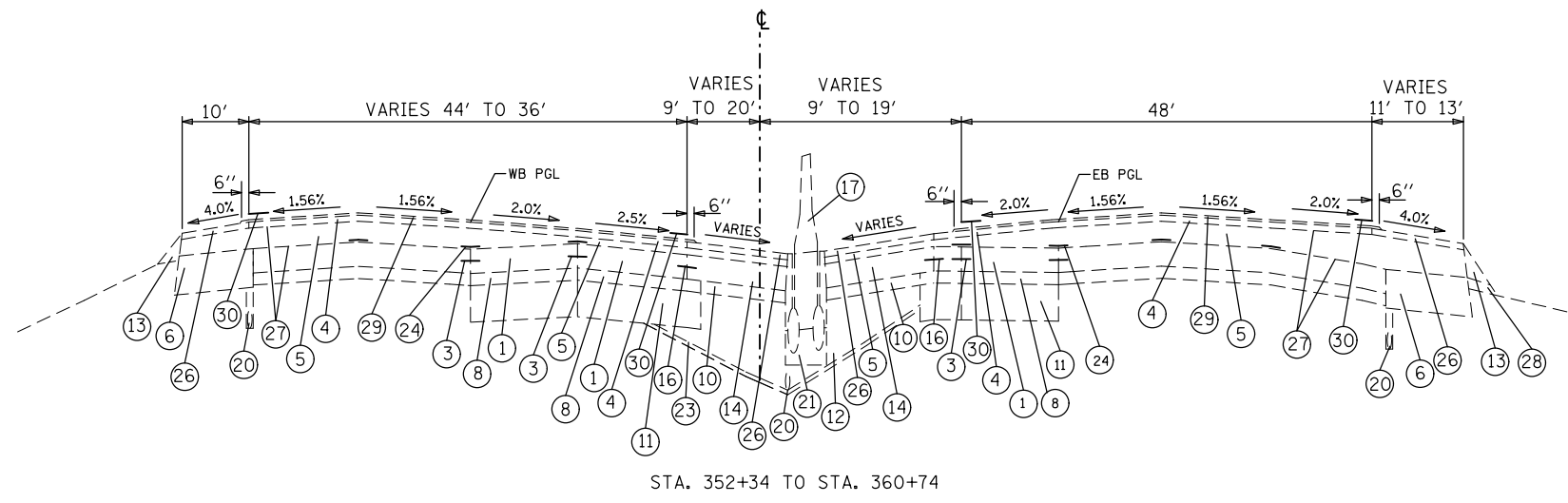


STA. 351+51.50 TO STA. 352+34

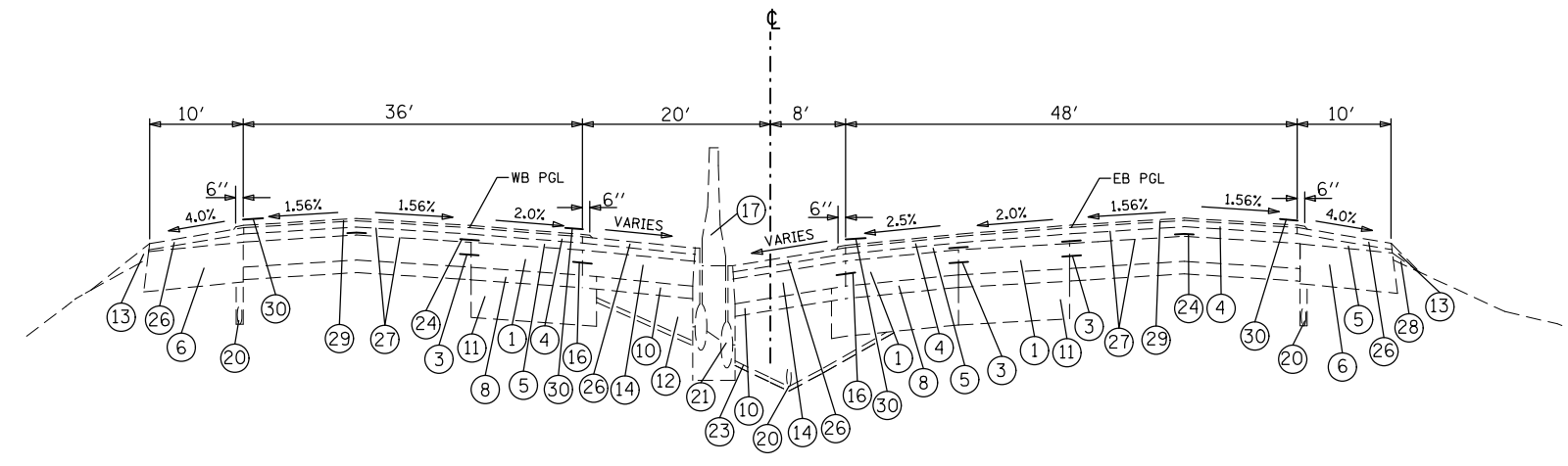
LEGEND

- 1 EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
- 2 EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
- 3 EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
- 4 EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS I (2")
- 5 EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
- 6 EXISTING 13" BITUMINOUS SHOULDER
- 7 EXISTING 8" BITUMINOUS SHOULDER
- 8 EXISTING STABILIZED SUB-BASE 4"
- 9 EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- 10 EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A 4"
- 11 EXISTING AGGREGATE SUBGRADE 12"
- 12 EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
- 13 EXISTING AGGREGATE SHOULDER, TYPE B
- 14 EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
- 15 EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 9"
- 16 EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE 3/4 INCH EPOXY COATED SMOOTH DOWEL BARS AT 24" C-C AND 24" LONG
- 17 EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
- 18 EXISTING GUARDRAIL
- 19 EXISTING BITUMINOUS SHOULDER CURB
- 20 EXISTING PIPE UNDERDRAINS 6"
- 21 EXISTING SLOTTED DRAIN STRUCTURE
- 22 EXISTING STORM SEWER
- 23 EXISTING FILTER FABRIC
- 24 EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- 25 EXISTING NOISE ABATEMENT WALL
- 26 EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
- 27 EXISTING BITUMINOUS MATERIALS (PRIME COAT)
- 28 EXISTING EMBANKMENT
- 29 EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
- 30 EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE

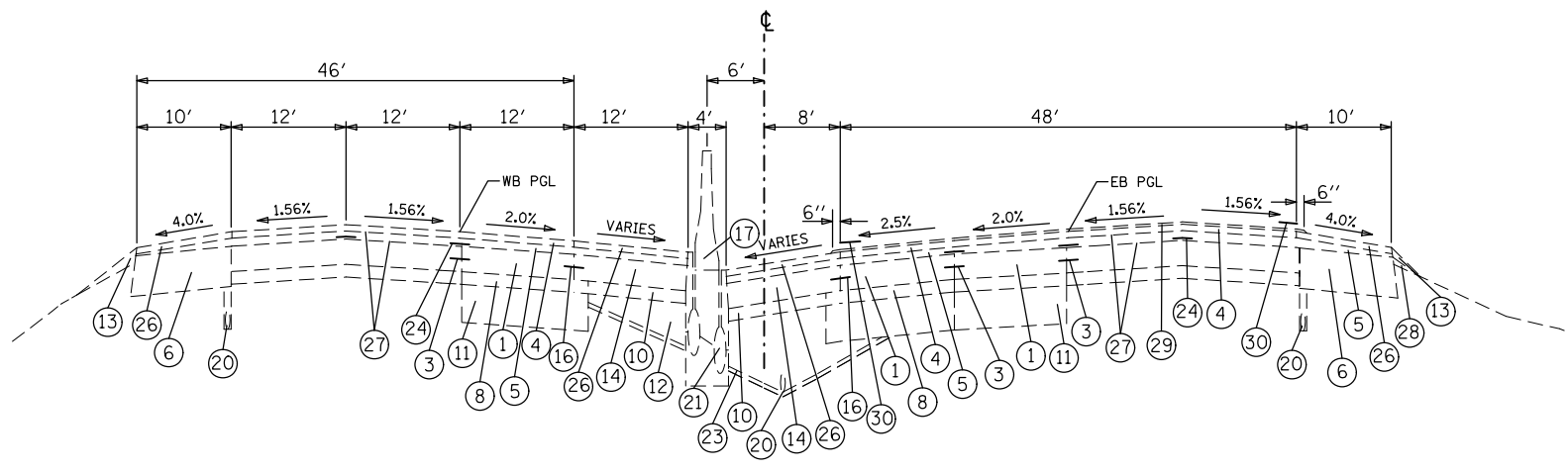
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PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -	CONTRACT NO. 76H71								
PLOT DATE = 3/26/2015	DATE -	REVISED -	SCALE:		SHEET NO. 1 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		



STA. 352+34 TO STA. 360+74



STA. 360+74 TO STA. 367+00

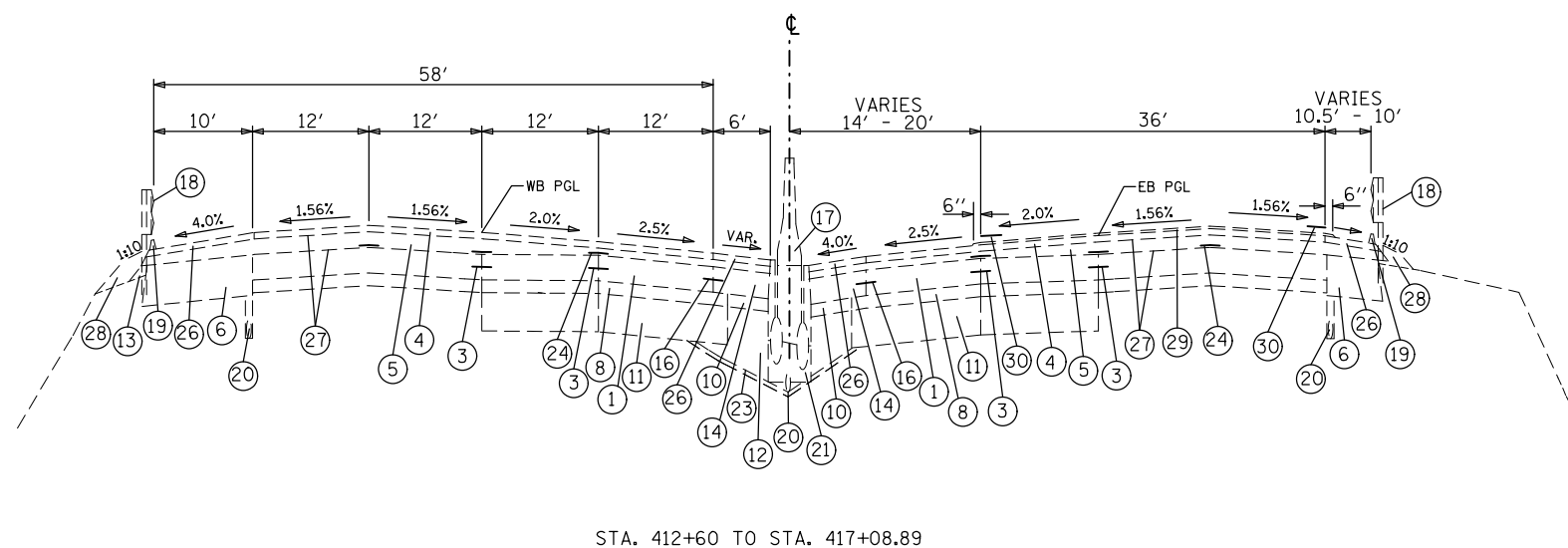
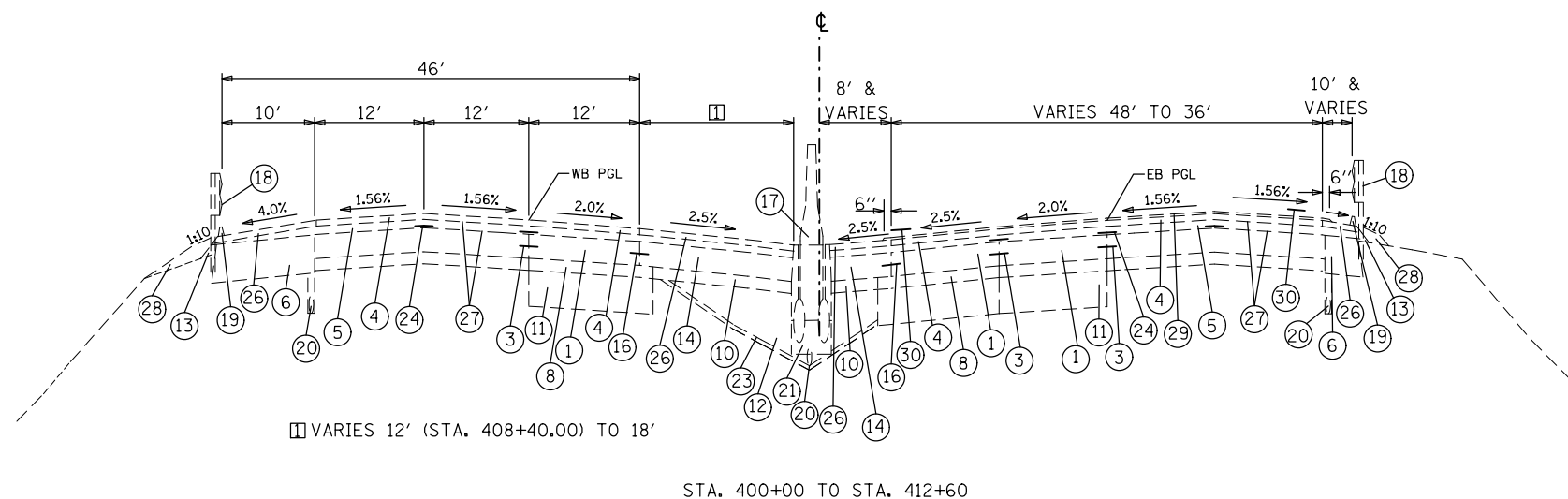


STA. 367+00 TO STA. 400+00

LEGEND

- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
- ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
- ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
- ④ EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS 1 (2")
- ⑤ EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
- ⑥ EXISTING 13" BITUMINOUS SHOULDER
- ⑦ EXISTING 8" BITUMINOUS SHOULDER
- ⑧ EXISTING STABILIZED SUB-BASE 4"
- ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- ⑩ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A 4"
- ⑪ EXISTING AGGREGATE SUBGRADE 12"
- ⑫ EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
- ⑬ EXISTING AGGREGATE SHOULDER, TYPE B
- ⑭ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
- ⑮ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 9"
- ⑯ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE 3/4 INCH EPOXY COATED SMOOTH DOWEL BARS AT 24" C-C AND 24" LONG
- ⑰ EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
- ⑱ EXISTING GUARDRAIL
- ⑲ EXISTING BITUMINOUS SHOULDER CURB
- ⑳ EXISTING PIPE UNDERDRAINS 6"
- ㉑ EXISTING SLOTTED DRAIN STRUCTURE
- ㉒ EXISTING STORM SEWER
- ㉓ EXISTING FILTER FABRIC
- ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ㉕ EXISTING NOISE ABATEMENT WALL
- ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
- ㉗ EXISTING BITUMINOUS MATERIALS (PRIME COAT)
- ㉘ EXISTING EMBANKMENT
- ㉙ EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
- ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE

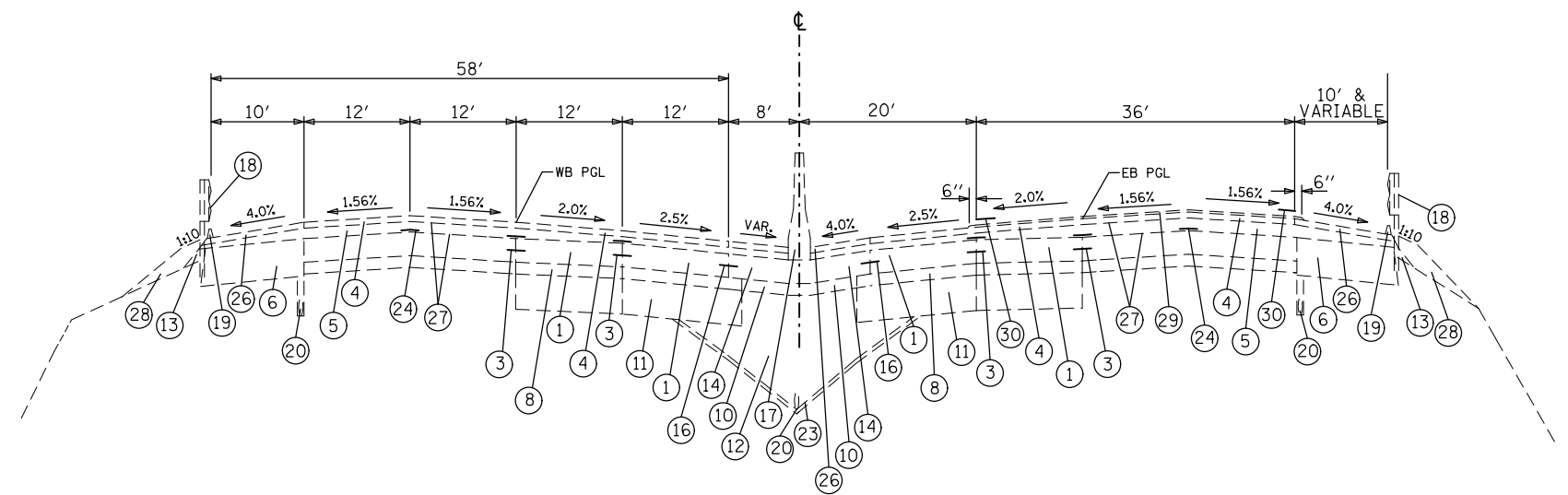
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et:\pw\work\p\midot\durosier_j\0419849\0876h71-sht-typical.dgn	DRAWN -	REVISED -	64				82-(4,5,6,7)-RS-1	ST. CLAIR	63	8	
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -	CONTRACT NO. 76H71								
PLOT DATE = 3/26/2015	DATE -	REVISED -	SCALE:		SHEET NO. 2 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		



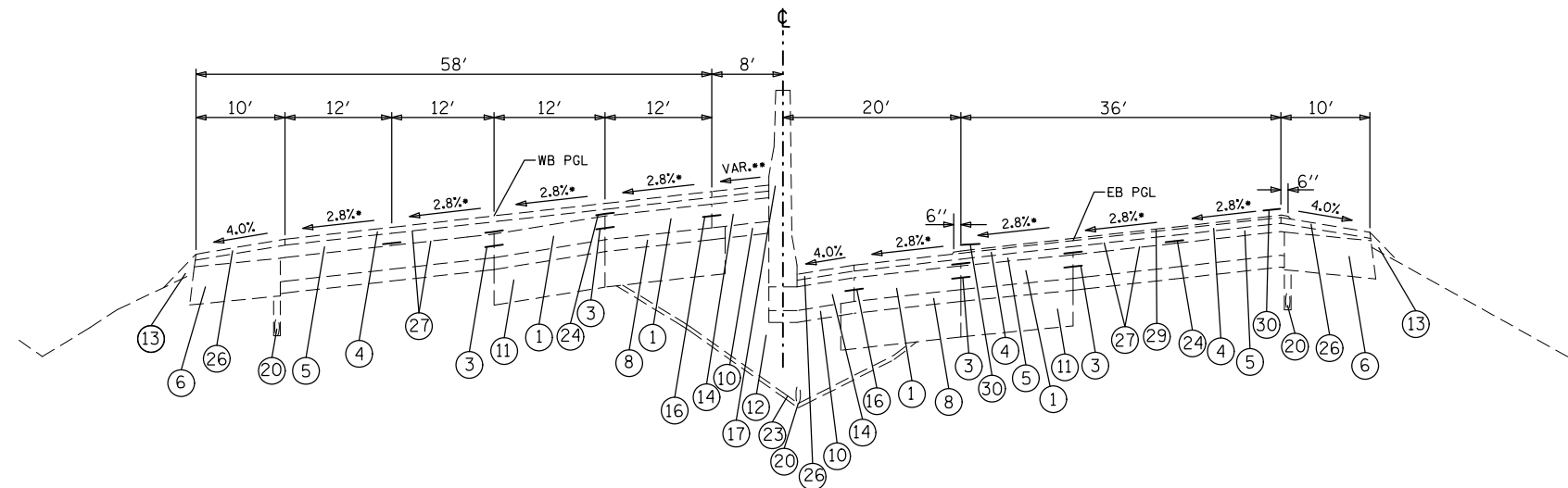
LEGEND

- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
- ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
- ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
- ④ EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS I (2")
- ⑤ EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
- ⑥ EXISTING 13" BITUMINOUS SHOULDER
- ⑦ EXISTING 8" BITUMINOUS SHOULDER
- ⑧ EXISTING STABILIZED SUB-BASE 4"
- ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- ⑩ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A 4"
- ⑪ EXISTING AGGREGATE SUBGRADE 12"
- ⑫ EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
- ⑬ EXISTING AGGREGATE SHOULDER, TYPE B
- ⑭ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
- ⑮ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 9"
- ⑯ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE 3/4 INCH EPOXY COATED SMOOTH DOWEL BARS AT 24" C-C AND 24" LONG
- ⑰ EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
- ⑱ EXISTING GUARDRAIL
- ⑲ EXISTING BITUMINOUS SHOULDER CURB
- ⑳ EXISTING PIPE UNDERDRAINS 6"
- ㉑ EXISTING SLOTTED DRAIN STRUCTURE
- ㉒ EXISTING STORM SEWER
- ㉓ EXISTING FILTER FABRIC
- ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ㉕ EXISTING NOISE ABATEMENT WALL
- ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
- ㉗ EXISTING BITUMINOUS MATERIALS (PRIME COAT)
- ㉘ EXISTING EMBANKMENT
- ㉙ EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
- ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE

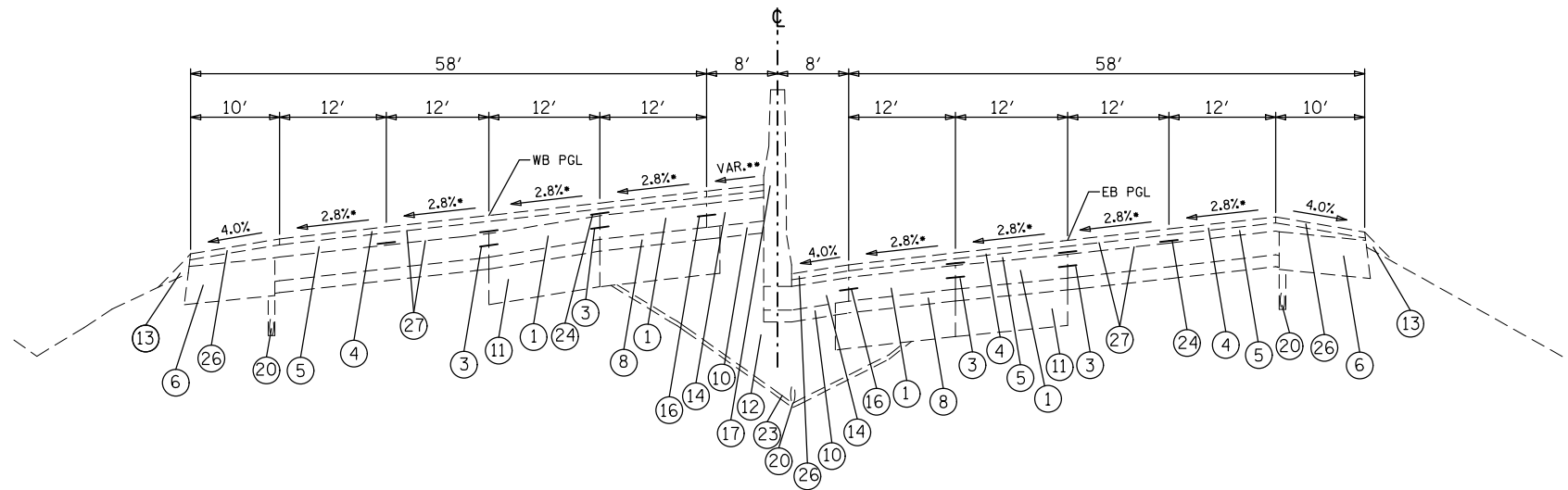
FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING TYPICAL SECTIONS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw_work\p\dot\durosier_j\0419849\0876h71-sht-typical.dgn		DRAWN -	REVISED -		64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	9			
PLOT SCALE = 100.0000' / 1in.		CHECKED -	REVISED -		CONTRACT NO. 76H71							
PLOT DATE = 3/26/2015		DATE -	REVISED -		SCALE:	SHEET NO. 3 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		



STA. 417+08.89 TO STA. 435+43.93 EB
 STA. 434+54.13 WB



STA. 435+43.93 EB TO STA. 443+00 EB
 STA. 434+54.13 WB TO STA. 443+00 WB

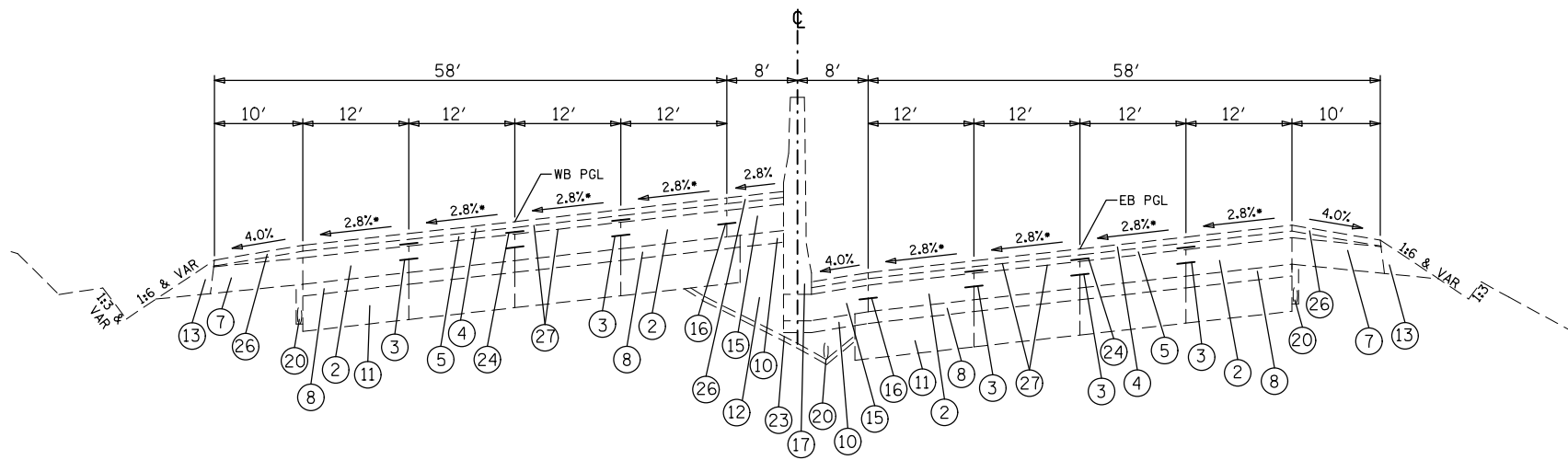


STA. 443+00 EB TO STA. 452+41.86 EB
 STA. 443+00 WB TO STA. 451+93.21 WB
 STA. 456+82.38 EB TO STA. 466+57.16 EB
 STA. 458+93.21 WB TO STA. 467+46.96 WB

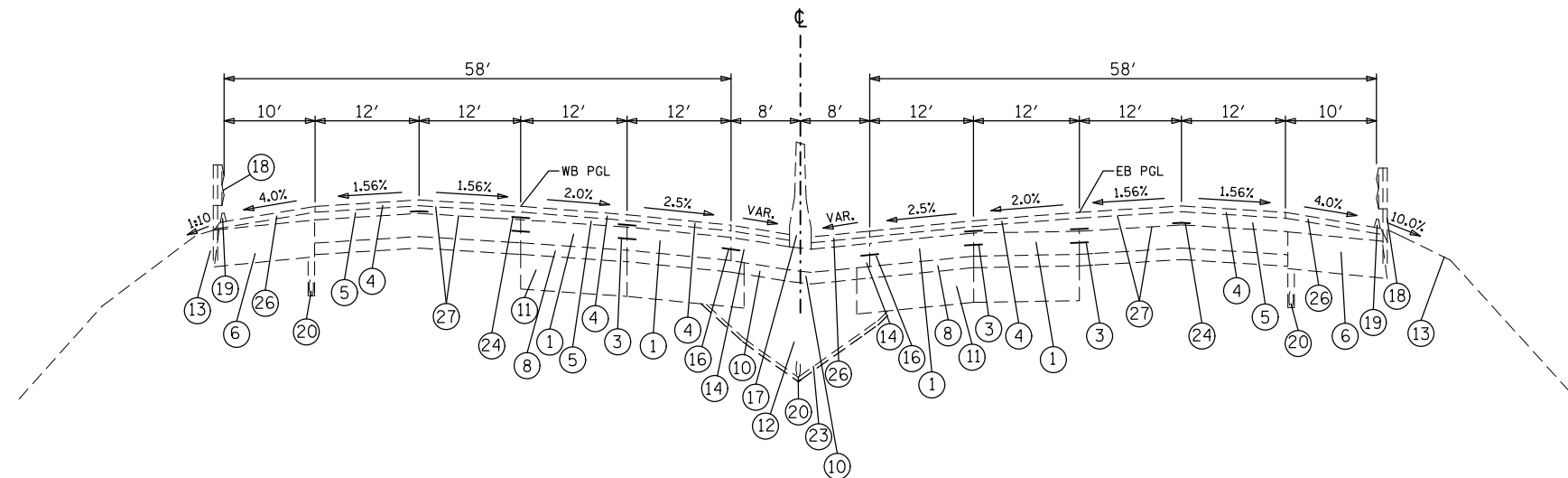
STA. 548+60.20 EB TO STA. 559+48.32 EB
 STA. 547+70.40 WB TO STA. 560+38.12 WB

- LEGEND
- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
 - ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
 - ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
 - ④ EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS I (2")
 - ⑤ EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
 - ⑥ EXISTING 13" BITUMINOUS SHOULDER
 - ⑦ EXISTING 8" BITUMINOUS SHOULDER
 - ⑧ EXISTING STABILIZED SUB-BASE 4"
 - ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
 - ⑩ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A 4"
 - ⑪ EXISTING AGGREGATE SUBGRADE 12"
 - ⑫ EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
 - ⑬ EXISTING AGGREGATE SHOULDER, TYPE B
 - ⑭ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
 - ⑮ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - ⑯ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE 3/4 INCH EPOXY COATED SMOOTH DOWEL BARS AT 24" C-C AND 24" LONG
 - ⑰ EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
 - ⑱ EXISTING GUARDRAIL
 - ⑲ EXISTING BITUMINOUS SHOULDER CURB
 - ⑳ EXISTING PIPE UNDERDRAINS 6"
 - ㉑ EXISTING SLOTTED DRAIN STRUCTURE
 - ㉒ EXISTING STORM SEWER
 - ㉓ EXISTING FILTER FABRIC
 - ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
 - ㉕ EXISTING NOISE ABATEMENT WALL
 - ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
 - ㉗ EXISTING BITUMINOUS MATERIALS (PRIME COAT)
 - ㉘ EXISTING EMBANKMENT
 - ㉙ EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
 - ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE

FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING TYPICAL SECTIONS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\durosier_j\d0419849\d876h71-sht-typical.dgn	DRAWN -	REVISED -	64			82-(4,5,6,7)-RS-1	ST. CLAIR	63	10	
PLOT SCALE = 100.0000' / 1".	CHECKED -	REVISED -	CONTRACT NO. 76H71							
PLOT DATE = 3/26/2015	DATE -	REVISED -	SCALE:			SHEET NO. 4 OF 11 SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	



STA. 452+41.86 EB TO STA. 456+82.38 EB
 STA. 451+93.21 WB TO STA. 458+93.21 WB

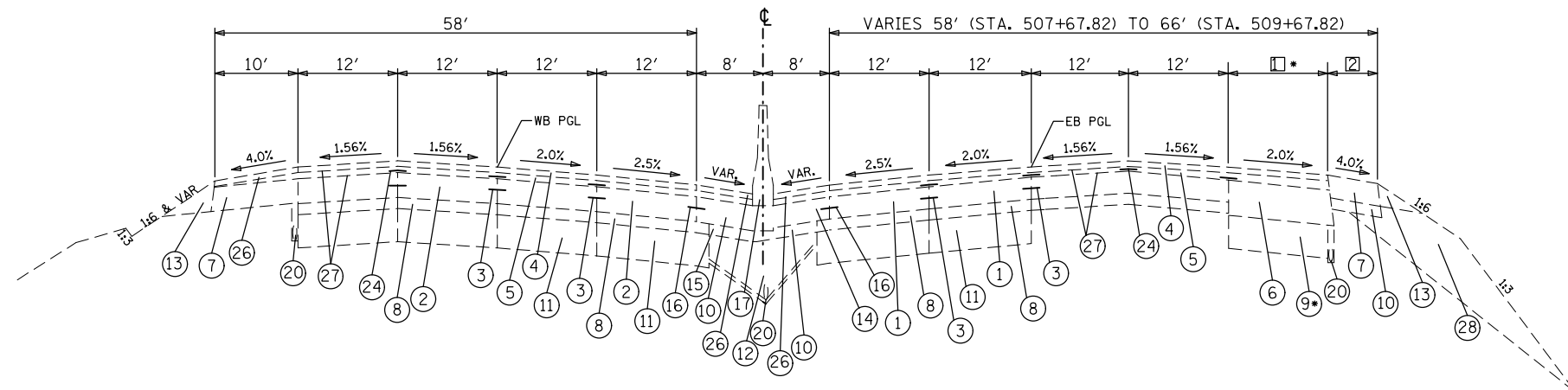


STA. 466+57.16 EB TO STA. 506+92.82 EB
 STA. 467+46.96 WB TO STA. 504+07.00 WB
 STA. 521+38.92 EB TO STA. 548+60.20 EB
 STA. 510+34.00 WB TO STA. 547+70.40 WB
 STA. 607+83.78 EB TO STA. 613+80.02
 STA. 606+93.98 WB
 STA. 693+92.51 EB TO STA. 725+00 EB
 STA. 692+18.71 WB TO STA. 705+00 WB

LEGEND

- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
- ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
- ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
- ④ EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS I (2")
- ⑤ EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
- ⑥ EXISTING 13" BITUMINOUS SHOULDER
- ⑦ EXISTING 8" BITUMINOUS SHOULDER
- ⑧ EXISTING STABILIZED SUB-BASE 4"
- ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- ⑩ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A 4"
- ⑪ EXISTING AGGREGATE SUBGRADE 12"
- ⑫ EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
- ⑬ EXISTING AGGREGATE SHOULDER, TYPE B
- ⑭ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
- ⑮ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 9"
- ⑯ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE 3/4 INCH EPOXY COATED SMOOTH DOWEL BARS AT 24" C-C AND 24" LONG
- ⑰ EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
- ⑱ EXISTING GUARDRAIL
- ⑲ EXISTING BITUMINOUS SHOULDER CURB
- ⑳ EXISTING PIPE UNDERDRAINS 6"
- ㉑ EXISTING SLOTTED DRAIN STRUCTURE
- ㉒ EXISTING STORM SEWER
- ㉓ EXISTING FILTER FABRIC
- ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ㉕ EXISTING NOISE ABATEMENT WALL
- ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
- ㉗ EXISTING BITUMINOUS MATERIALS (PRIME COAT)
- ㉘ EXISTING EMBANKMENT
- ㉙ EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
- ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGE LINE

FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING TYPICAL SECTIONS		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\pwork\durosier_j\d0419849\d876h71-sht-typical.dgn	DRAWN -	REVISED -	64				82-(4,5,6,7)-RS-1	ST. CLAIR	63	11	
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -	CONTRACT NO. 76H71								
PLOT DATE = 3/26/2015	DATE -	REVISED -	SCALE:		SHEET NO. 5 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		

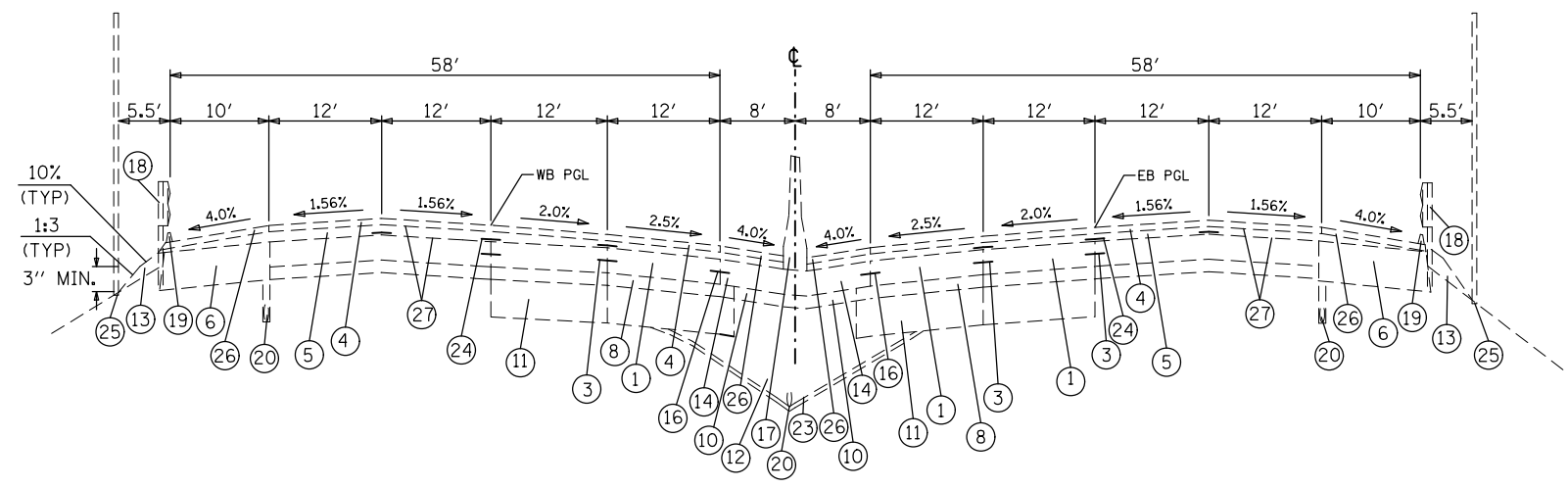


STA. 506+92.82 EB TO STA. 521+38.92 EB
 STA. 504+07.00 WB TO STA. 510+34.00 WB

* □ SHOULDER PAVEMENT & SUB-BASE WIDTH VARY
 10' (STA. 509+17.82) TO 12' (STA. 509+67.82)
 □ VARIES 0' (STA. 507+67.82) TO 6' (STA. 509+17.82)

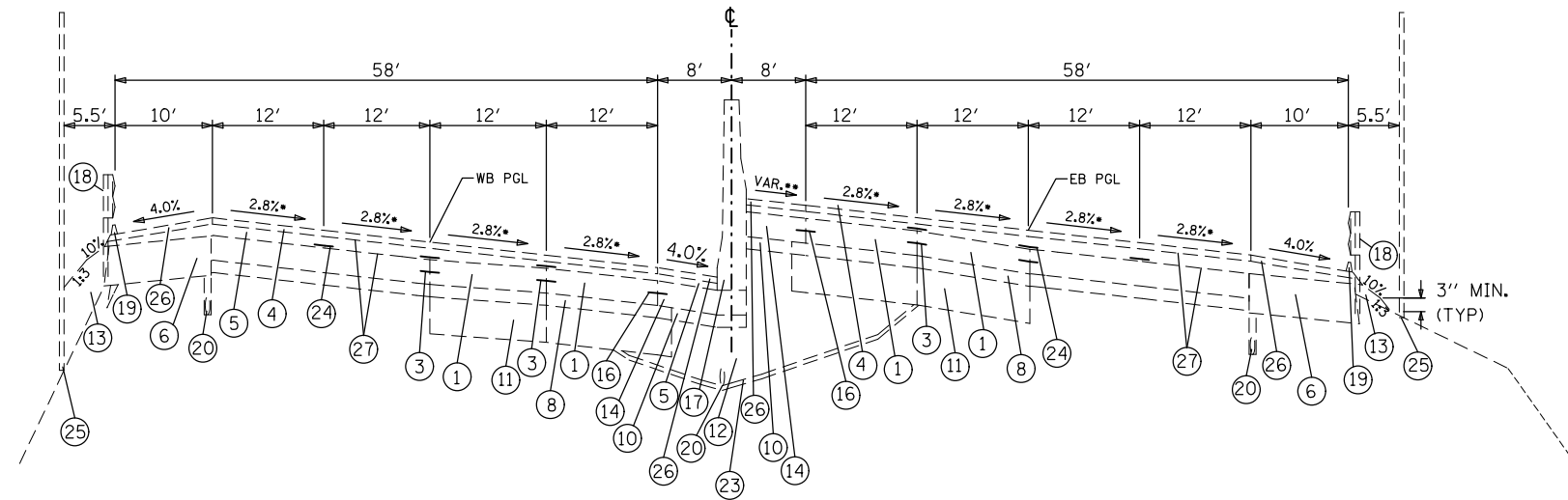
LEGEND

- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
- ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
- ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
- ④ EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS I (2")
- ⑤ EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
- ⑥ EXISTING 13" BITUMINOUS SHOULDER
- ⑦ EXISTING 8" BITUMINOUS SHOULDER
- ⑧ EXISTING STABILIZED SUB-BASE 4"
- ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- ⑩ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A 4"
- ⑪ EXISTING AGGREGATE SUBGRADE 12"
- ⑫ EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
- ⑬ EXISTING AGGREGATE SHOULDER, TYPE B
- ⑭ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
- ⑮ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 9"
- ⑯ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE 3/4 INCH EPOXY COATED SMOOTH DOWEL BARS AT 24" C-C AND 24" LONG
- ⑰ EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
- ⑱ EXISTING GUARDRAIL
- ⑲ EXISTING BITUMINOUS SHOULDER CURB
- ⑳ EXISTING PIPE UNDERDRAINS 6"
- ㉑ EXISTING SLOTTED DRAIN STRUCTURE
- ㉒ EXISTING STORM SEWER
- ㉓ EXISTING FILTER FABRIC
- ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ㉕ EXISTING NOISE ABATEMENT WALL
- ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
- ㉗ EXISTING BITUMINOUS MATERIALS (PRIME COAT)
- ㉘ EXISTING EMBANKMENT
- ㉙ EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
- ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE

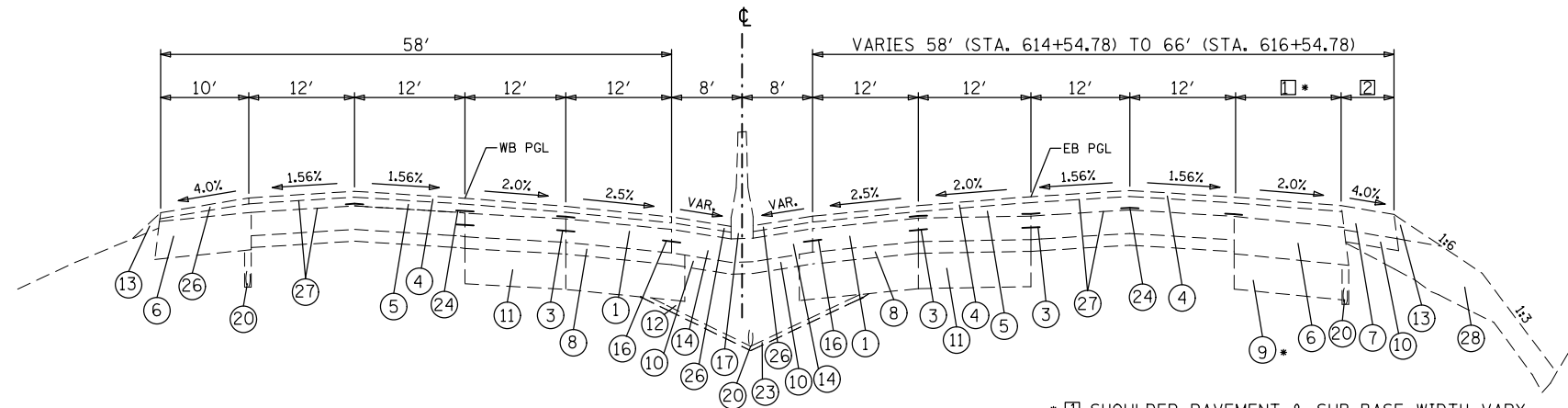


STA. 559+48.32 EB TO STA. 585+31.38 EB
 STA. 560+38.12 WB TO STA. 586+21.18 WB

FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING TYPICAL SECTIONS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\dot\durosier_j\d0419849\d0876h71-sht-typical.dgn	DRAWN -	REVISED -	64					82-(4,5,6,7)-RS-1	ST. CLAIR	63	12	
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -	CONTRACT NO. 76H71									
PLOT DATE = 3/26/2015	DATE -	REVISED -	SCALE:		SHEET NO. 6 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



STA. 585+31.38 EB TO STA. 607+83.78 EB
 STA. 586+21.18 WB TO STA. 606+93.98 WB



STA. 613+80.02 TO STA. 657+24.79 EB
 STA. 614+04.78 WB TO STA. 652+43.94 WB

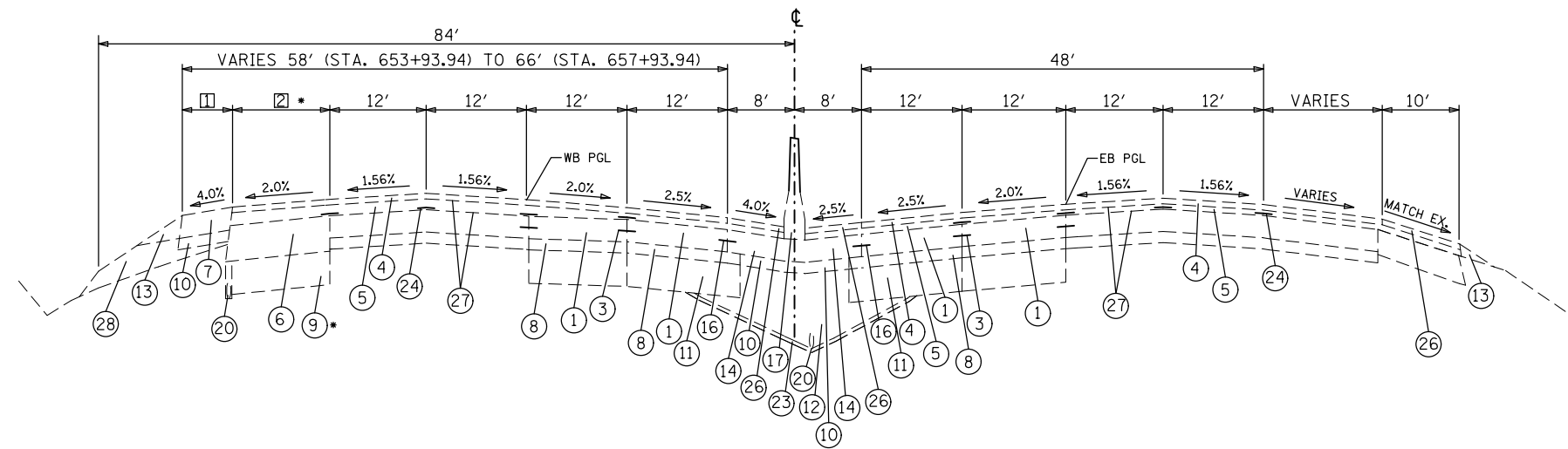
STATION EQUATION
 STA. 618+20.65 BK =
 STA. 643+85.18 AH

* □ SHOULDER PAVEMENT & SUB-BASE WIDTH VARY
 10' (STA. 616+04.78) TO 12' (STA. 616+54.78)
 □ VARIES 0' (STA. 614+54.78) TO 6' (STA. 616+04.78)

LEGEND

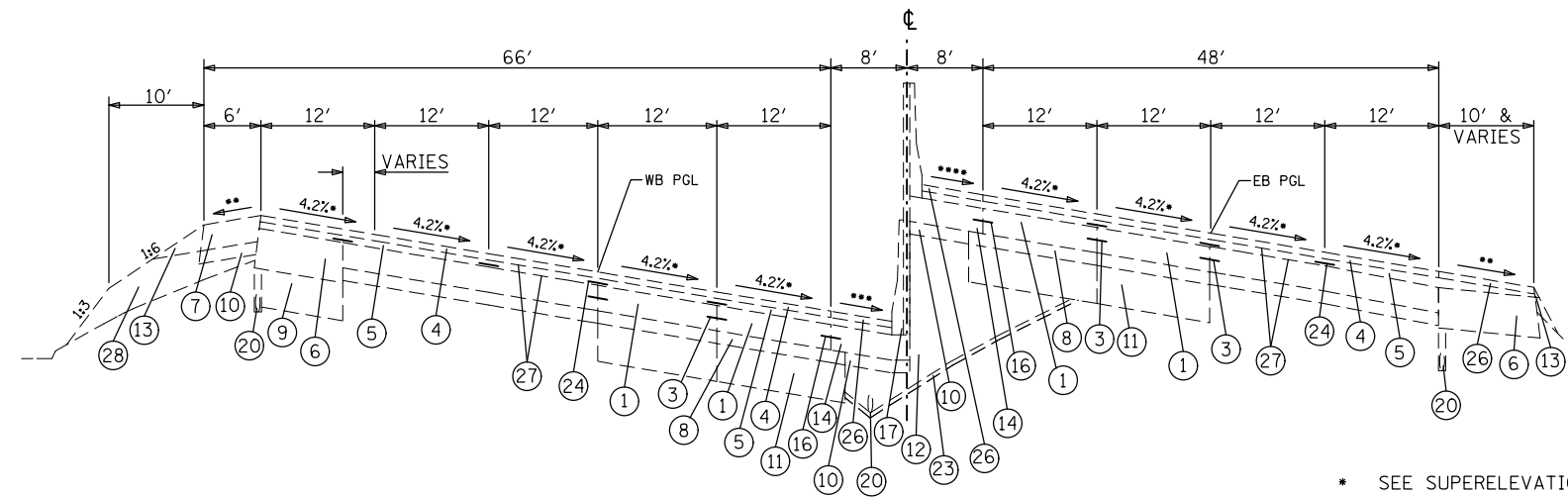
- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
- ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
- ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
- ④ EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS I (2")
- ⑤ EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
- ⑥ EXISTING 13" BITUMINOUS SHOULDER
- ⑦ EXISTING 8" BITUMINOUS SHOULDER
- ⑧ EXISTING STABILIZED SUB-BASE 4"
- ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- ⑩ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A 4"
- ⑪ EXISTING AGGREGATE SUBGRADE 12"
- ⑫ EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
- ⑬ EXISTING AGGREGATE SHOULDER, TYPE B
- ⑭ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
- ⑮ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 9"
- ⑯ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE 3/4 INCH EPOXY COATED SMOOTH DOWEL BARS AT 24" C-C AND 24" LONG
- ⑰ EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
- ⑱ EXISTING GUARDRAIL
- ⑲ EXISTING BITUMINOUS SHOULDER CURB
- ⑳ EXISTING PIPE UNDERDRAINS 6"
- ㉑ EXISTING SLOTTED DRAIN STRUCTURE
- ㉒ EXISTING STORM SEWER
- ㉓ EXISTING FILTER FABRIC
- ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ㉕ EXISTING NOISE ABATEMENT WALL
- ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
- ㉗ EXISTING BITUMINOUS MATERIALS (PRIME COAT)
- ㉘ EXISTING EMBANKMENT
- ㉙ EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
- ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE

FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING TYPICAL SECTIONS		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\durosier_j\0419849\d876h71-sht-typical.dgn	DRAWN -	REVISED -	64				82-(4,5,6,7)-RS-1	ST. CLAIR	63	13	
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -	CONTRACT NO. 76H71								
PLOT DATE = 3/26/2015	DATE -	REVISED -	SCALE:		SHEET NO. 7 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		



□ VARIES 0' (STA. 653+93.94) TO 6' (STA. 656+93.94)
 * ▣ SHOULDER PAVEMENT & SUB-BASE WIDTH VARY
 10' (STA. 656+93.94) TO 12' (STA. 657+93.94)

STA. 657+24.79 EB TO STA. 658+69.57 EB
 STA. 652+43.94 WB TO STA. 658+54.90 WB



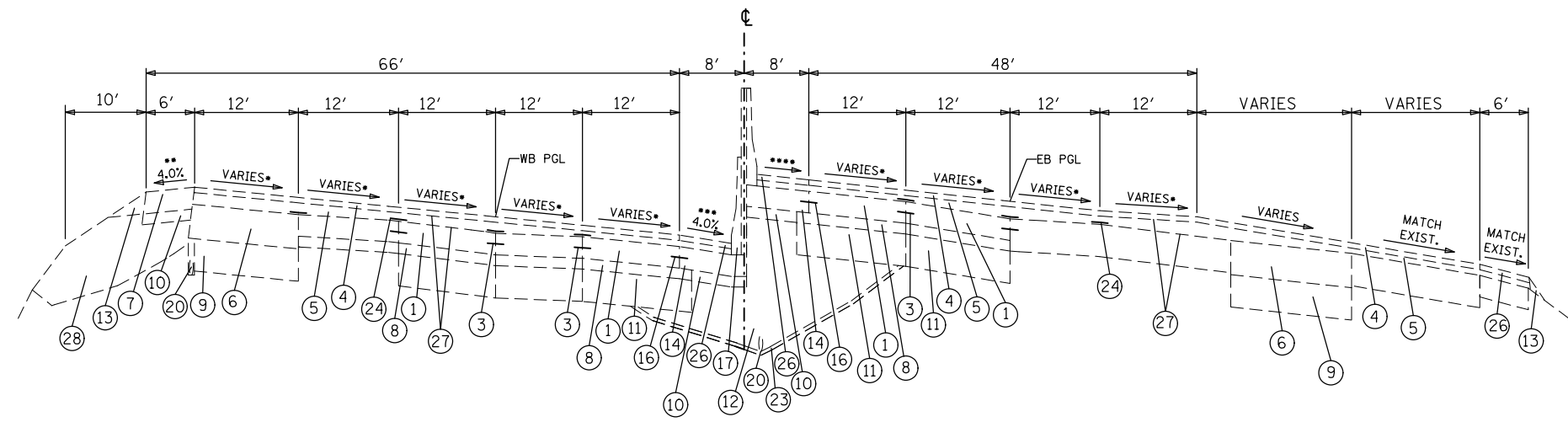
STA. 658+69.57 EB TO STA. 661+10.04
 STA. 658+54.90 WB TO STA. 662+84.20
 STA. 662+84.20 TO STA. 672+93.94

- * SEE SUPERELEVATION TRANSITION DETAILS FOR PAVEMENT SUPERELEVATION.
- ** WHEN THE SUPERELEVATION RATE IS 4% OR LESS, THE SHOULDER SLOPE IS 4%. WHEN PAVEMENT SUPERELEVATION RATE EXCEEDS 4%, THE MAXIMUM ALGEBRAIC DIFFERENCE IN CROSS SLOPE IS 8%.
- *** WHEN THE SUPERELEVATION RATE IS 4% OR LESS, THE SHOULDER SLOPE IS 4%. WHEN PAVEMENT SUPERELEVATION RATE EXCEEDS 4%, THE SHOULDER SLOPE SHOULD EQUAL ADJACENT PAVEMENT SUPERELEVATION RATE.
- **** SHOULDER SLOPE SHOULD EQUAL ADJACENT PAVEMENT SUPERELEVATION RATE.

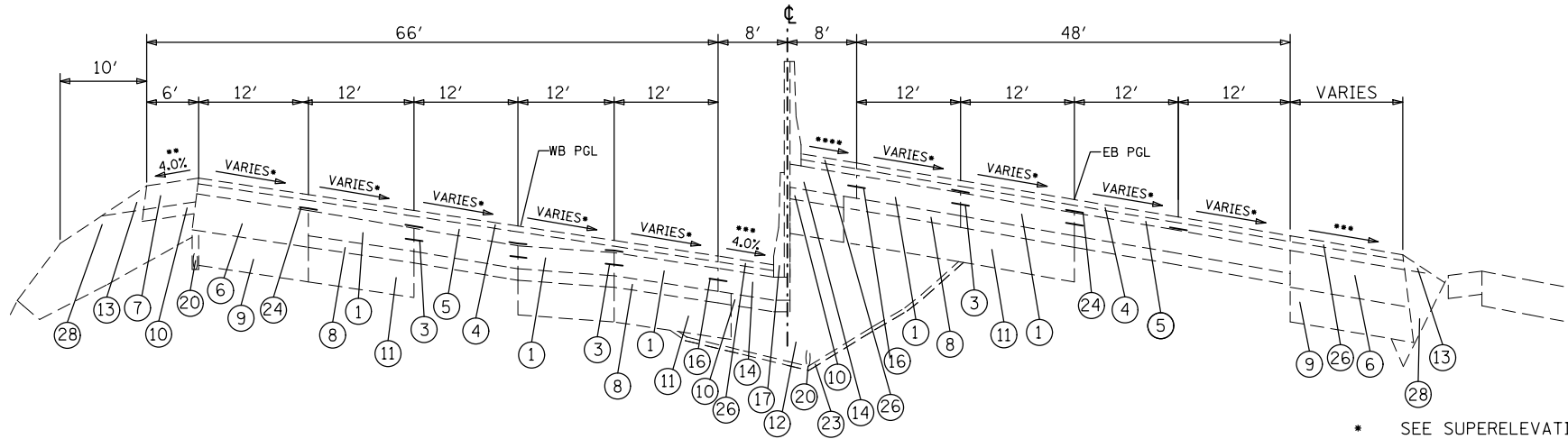
LEGEND

- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
- ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
- ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
- ④ EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS I (2")
- ⑤ EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
- ⑥ EXISTING 13" BITUMINOUS SHOULDER
- ⑦ EXISTING 8" BITUMINOUS SHOULDER
- ⑧ EXISTING STABILIZED SUB-BASE 4"
- ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- ⑩ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A 4"
- ⑪ EXISTING AGGREGATE SUBGRADE 12"
- ⑫ EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
- ⑬ EXISTING AGGREGATE SHOULDER, TYPE B
- ⑭ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
- ⑮ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 9"
- ⑯ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE 3/4 INCH EPOXY COATED SMOOTH DOWEL BARS AT 24" C-C AND 24" LONG
- ⑰ EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
- ⑱ EXISTING GUARDRAIL
- ⑲ EXISTING BITUMINOUS SHOULDER CURB
- ⑳ EXISTING PIPE UNDERDRAINS 6"
- ㉑ EXISTING SLOTTED DRAIN STRUCTURE
- ㉒ EXISTING STORM SEWER
- ㉓ EXISTING FILTER FABRIC
- ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ㉕ EXISTING NOISE ABATEMENT WALL
- ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
- ㉗ EXISTING BITUMINOUS MATERIALS (PRIME COAT)
- ㉘ EXISTING EMBANKMENT
- ㉙ EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
- ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE

FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING TYPICAL SECTIONS		F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -	CONTRACT NO. 76H71								
PLOT DATE = 3/26/2015	DATE -	REVISED -	SCALE:		SHEET NO. 8 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		



STA. 661+10.04 TO STA. 662+23.36



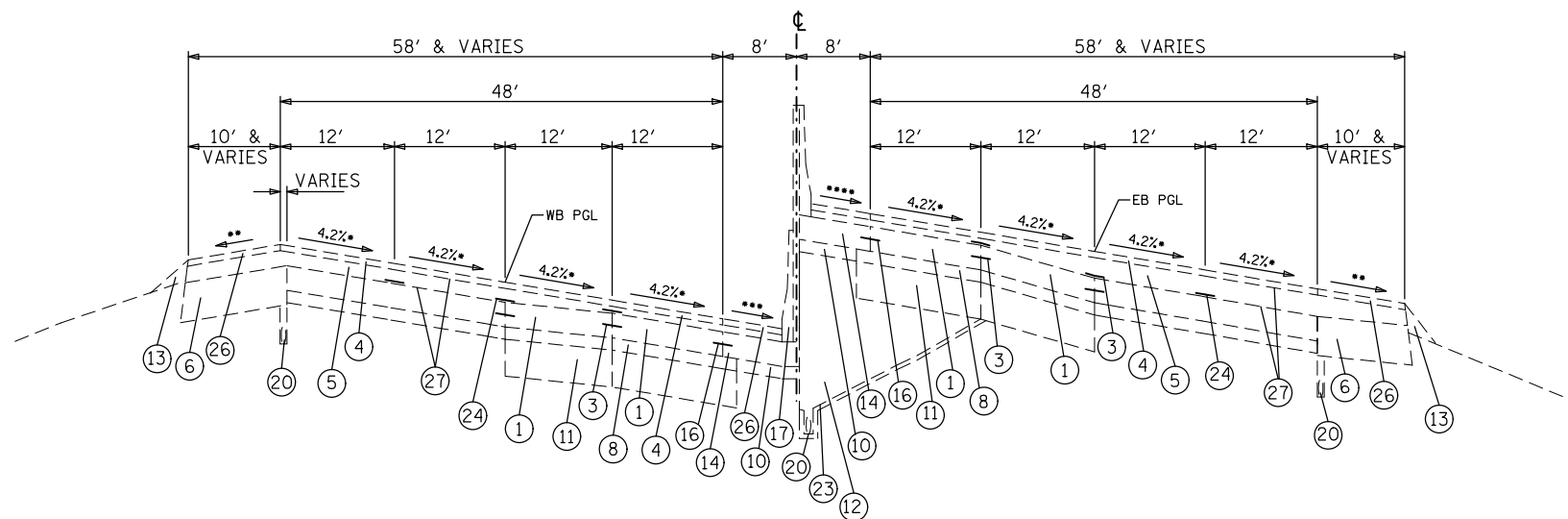
STA. 662+23.36 TO STA. 662+84.20

LEGEND

- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
- ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
- ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
- ④ EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS I (2")
- ⑤ EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
- ⑥ EXISTING 13" BITUMINOUS SHOULDER
- ⑦ EXISTING 8" BITUMINOUS SHOULDER
- ⑧ EXISTING STABILIZED SUB-BASE 4"
- ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
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- ⑪ EXISTING AGGREGATE SUBGRADE 12"
- ⑫ EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
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- ⑳ EXISTING PIPE UNDERDRAINS 6"
- ㉑ EXISTING SLOTTED DRAIN STRUCTURE
- ㉒ EXISTING STORM SEWER
- ㉓ EXISTING FILTER FABRIC
- ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ㉕ EXISTING NOISE ABATEMENT WALL
- ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
- ㉗ EXISTING BITUMINOUS MATERIALS (PRIME COAT)
- ㉘ EXISTING EMBANKMENT
- ㉙ EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
- ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE

- * SEE SUPERELEVATION TRANSITION DETAILS FOR PAVEMENT SUPERELEVATION.
- ** WHEN THE SUPERELEVATION RATE IS 4% OR LESS, THE SHOULDER SLOPE IS 4%. WHEN PAVEMENT SUPERELEVATION RATE EXCEEDS 4%, THE MAXIMUM ALGEBRAIC DIFFERENCE IN CROSS SLOPE IS 8%.
- *** WHEN THE SUPERELEVATION RATE IS 4% OR LESS, THE SHOULDER SLOPE IS 4%. WHEN PAVEMENT SUPERELEVATION RATE EXCEEDS 4%, THE SHOULDER SLOPE SHOULD EQUAL ADJACENT PAVEMENT SUPERELEVATION RATE.
- **** SHOULDER SLOPE SHOULD EQUAL ADJACENT PAVEMENT SUPERELEVATION RATE.

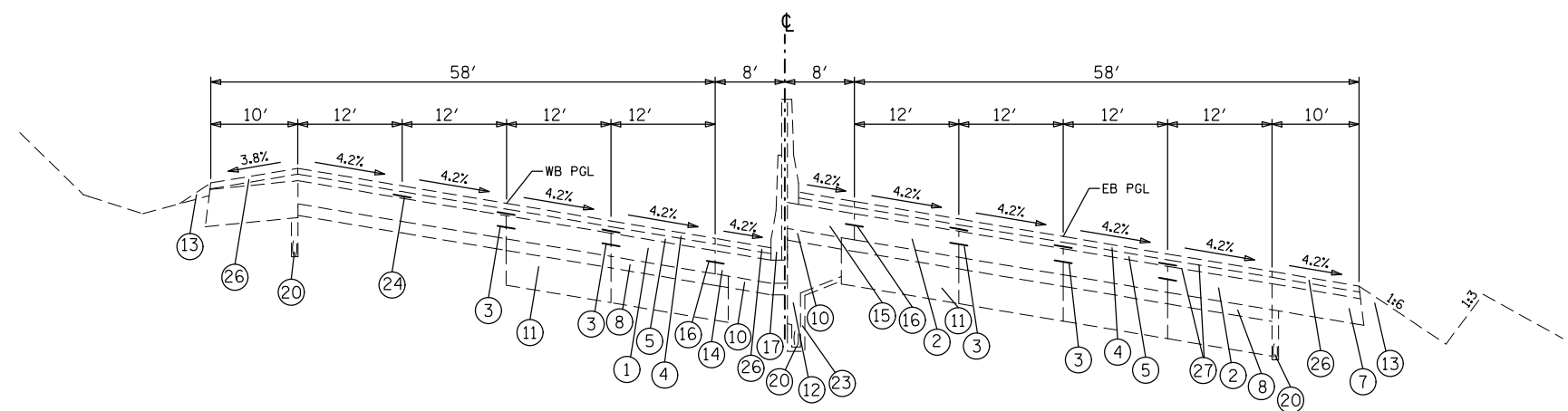
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PLOT SCALE = 100.0000' / 1" .	CHECKED -	REVISED -	CONTRACT NO. 76H71									
PLOT DATE = 3/26/2015	DATE -	REVISED -	SCALE:		SHEET NO. 9 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



- * SEE SUPERELEVATION TRANSITION DETAILS FOR PAVEMENT SUPERELEVATION.
- ** WHEN THE SUPERELEVATION RATE IS 4% OR LESS, THE SHOULDER SLOPE IS 4%. WHEN PAVEMENT SUPERELEVATION RATE EXCEEDS 4%, THE MAXIMUM ALGEBRAIC DIFFERENCE IN CROSS SLOPE IS 8%.
- *** WHEN THE SUPERELEVATION RATE IS 4% OR LESS, THE SHOULDER SLOPE IS 4%. WHEN PAVEMENT SUPERELEVATION RATE EXCEEDS 4%, THE SHOULDER SLOPE SHOULD EQUAL ADJACENT PAVEMENT SUPERELEVATION RATE.
- **** SHOULDER SLOPE SHOULD EQUAL ADJACENT PAVEMENT SUPERELEVATION RATE.

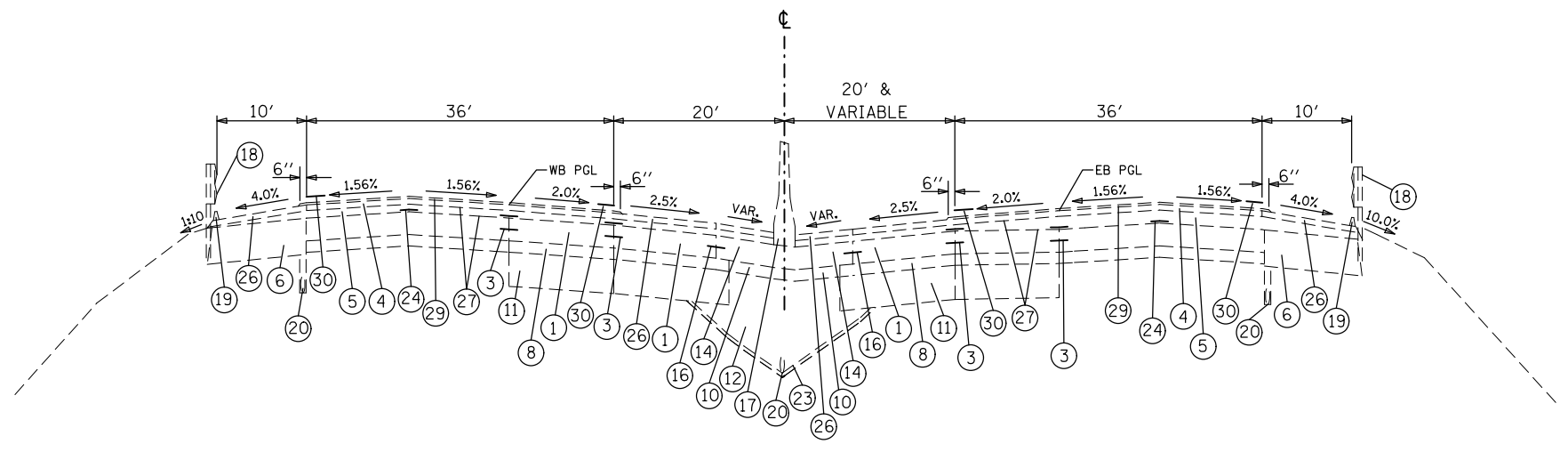
STA. 672+93.94 TO STA. 681+00
 STA. 689+15 TO STA. 693+92.51 EB
 STA. 692+18.71 WB

- LEGEND
- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
 - ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
 - ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
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 - ⑦ EXISTING 8" BITUMINOUS SHOULDER
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 - ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
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 - ⑪ EXISTING AGGREGATE SUBGRADE 12"
 - ⑫ EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
 - ⑬ EXISTING AGGREGATE SHOULDER, TYPE B
 - ⑭ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
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 - ⑱ EXISTING GUARDRAIL
 - ⑲ EXISTING BITUMINOUS SHOULDER CURB
 - ⑳ EXISTING PIPE UNDERDRAINS 6"
 - ㉑ EXISTING SLOTTED DRAIN STRUCTURE
 - ㉒ EXISTING STORM SEWER
 - ㉓ EXISTING FILTER FABRIC
 - ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
 - ㉕ EXISTING NOISE ABATEMENT WALL
 - ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
 - ㉗ EXISTING BITUMINOUS MATERIALS (PRIME COAT)
 - ㉘ EXISTING EMBANKMENT
 - ㉙ EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
 - ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE

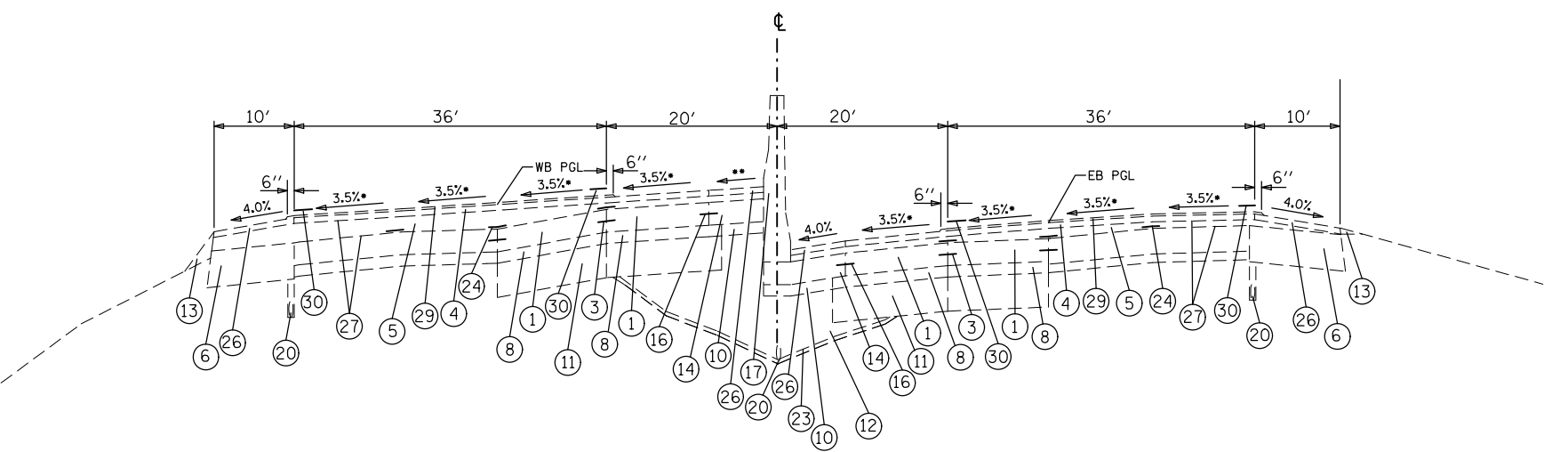


STA. 681+00 TO STA. 689+15

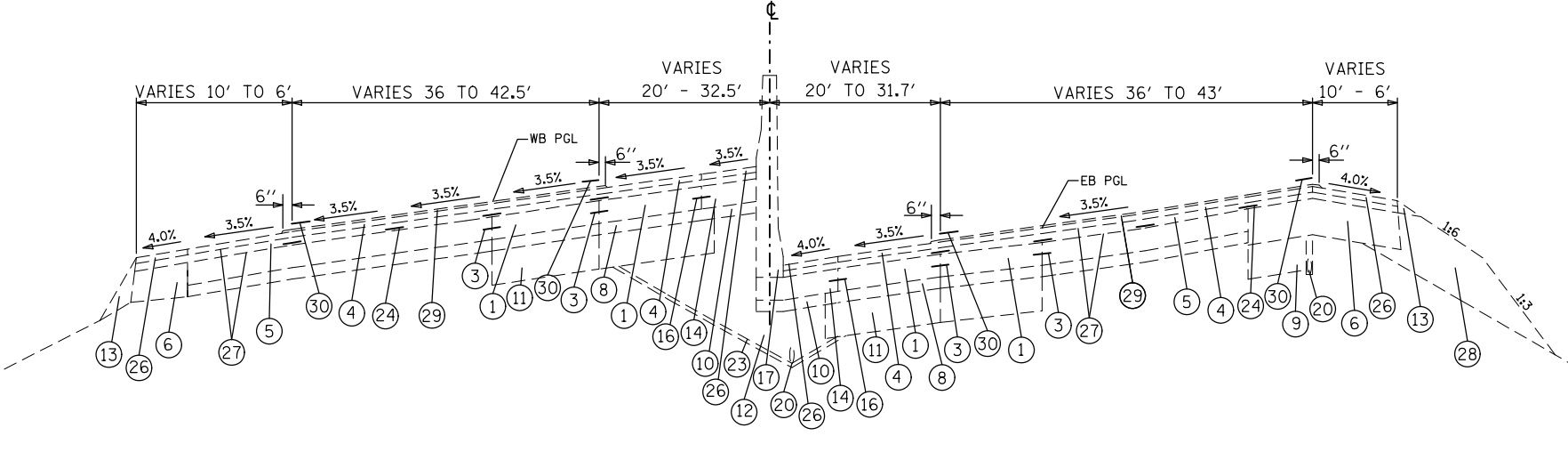
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PLOT DATE = 3/26/2015		DATE -	REVISED -		SCALE:	SHEET NO. 10 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		



STA. 725+00 EB TO STA. 732+09.69 EB
 STA. 705+00 WB TO STA. 730+77.89 WB



STA. 732+09.69 EB TO STA. 735+74.76
 STA. 730+77.89 WB

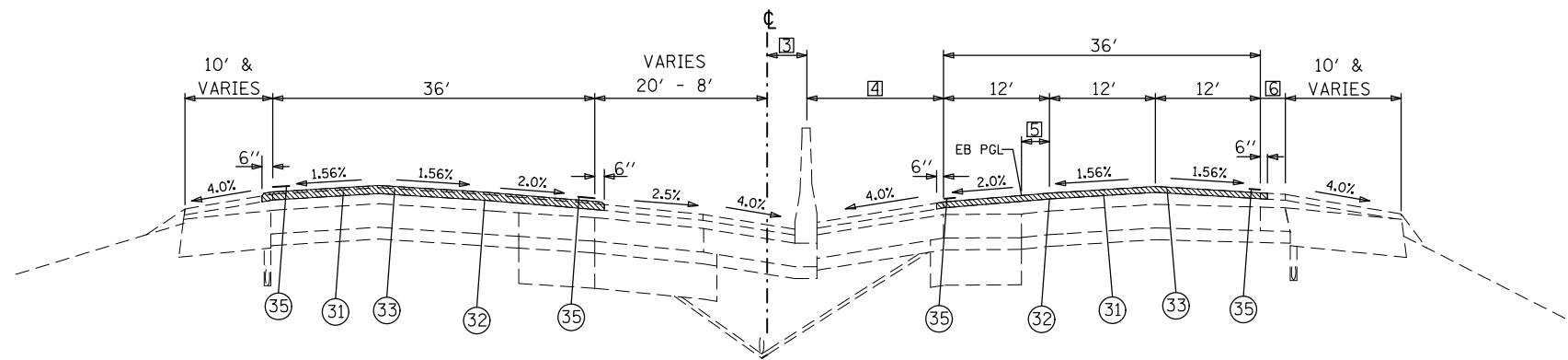


STA. 735+74.76 TO STA. 744+15.00

LEGEND

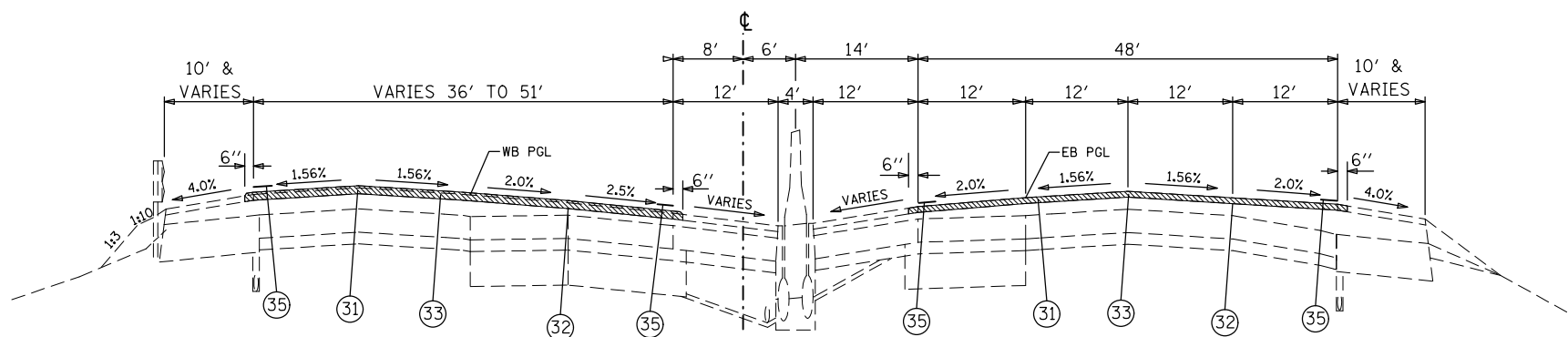
- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
- ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
- ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
- ④ EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS I (2")
- ⑤ EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
- ⑥ EXISTING 13" BITUMINOUS SHOULDER
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- ⑧ EXISTING STABILIZED SUB-BASE 4"
- ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
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- ⑫ EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
- ⑬ EXISTING AGGREGATE SHOULDER, TYPE B
- ⑭ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
- ⑮ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 9"
- ⑯ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE 3/4 INCH EPOXY COATED SMOOTH DOWEL BARS AT 24" C-C AND 24" LONG
- ⑰ EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
- ⑱ EXISTING GUARDRAIL
- ⑲ EXISTING BITUMINOUS SHOULDER CURB
- ⑳ EXISTING PIPE UNDERDRAINS 6"
- ㉑ EXISTING SLOTTED DRAIN STRUCTURE
- ㉒ EXISTING STORM SEWER
- ㉓ EXISTING FILTER FABRIC
- ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ㉕ EXISTING NOISE ABATEMENT WALL
- ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
- ㉗ EXISTING BITUMINOUS MATERIALS (PRIME COAT)
- ㉘ EXISTING EMBANKMENT
- ㉙ EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
- ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE

FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING TYPICAL SECTIONS		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = 3/26/2015	DATE -	REVISED -	SCALE:		SHEET NO. 11 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		

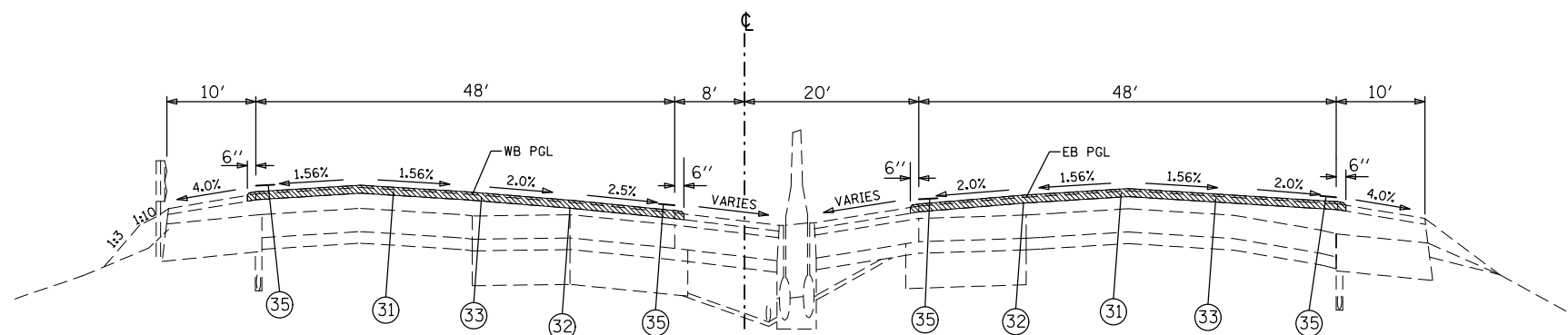


STA. 337+00 TO STA. 347+89.14 EB
 STA. 337+00 TO STA. 346+66.94 WB


3 VARIES 0' (STA. 337+00) TO 6' (STA. 345+51.58)
 4 VARIES 8' (STA. 337+00) TO 14' (STA. 346+01.58)
 5 VARIES 12' (STA. 337+00) TO 0' (STA. 346+01.58)
 6 VARIES



STA. 347+89.14 TO STA. 351+51.50 EB
 STA. 346+66.94 TO STA. 351+51.50 WB
 BRIDGE OMISSION WB STA. 349+15.20 TO 351+51.50



STA. 351+51.50 TO STA. 352+34

 HMA SURFACE REMOVAL

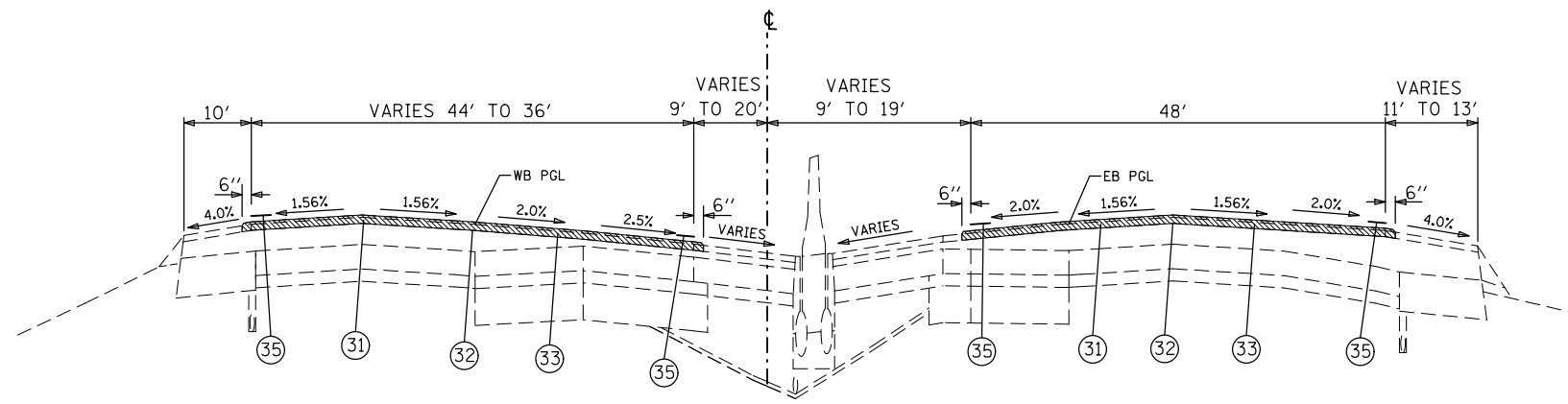
LEGEND

- 1 EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
- 2 EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
- 3 EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
- 4 EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS I (2")
- 5 EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
- 6 EXISTING 13" BITUMINOUS SHOULDER
- 7 EXISTING 8" BITUMINOUS SHOULDER
- 8 EXISTING STABILIZED SUB-BASE 4"
- 9 EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- 10 EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A 4"
- 11 EXISTING AGGREGATE SUBGRADE 12"
- 12 EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
- 13 EXISTING AGGREGATE SHOULDER, TYPE B
- 14 EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
- 15 EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 9"
- 16 EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE 3/4 INCH EPOXY COATED SMOOTH DOWEL BARS AT 24" C-C AND 24" LONG
- 17 EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
- 18 EXISTING GUARDRAIL
- 19 EXISTING BITUMINOUS SHOULDER CURB
- 20 EXISTING PIPE UNDERDRAINS 6"
- 21 EXISTING SLOTTED DRAIN STRUCTURE
- 22 EXISTING STORM SEWER
- 23 EXISTING FILTER FABRIC
- 24 EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- 25 EXISTING NOISE ABATEMENT WALL
- 26 EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
- 27 EXISTING BITUMINOUS MATERIALS (PRIME COAT)
- 28 EXISTING EMBANKMENT
- 29 EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
- 30 EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE
- 31 PROPOSED HMA SURFACE REMOVAL, 2" (SEE NOTE)
- 32 PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
- 33 PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT N80, 2"
- 34 PROPOSED MICRO-SURFACING, 2 PASSES
- 35 PROPOSED PREFORMED PLASTIC PAVEMENT MARKING TAPE, TYPE B INLAID EDGELINE
- 36 PROPOSED THERMOPLASTIC PAVEMENT MARKING EDGELINE

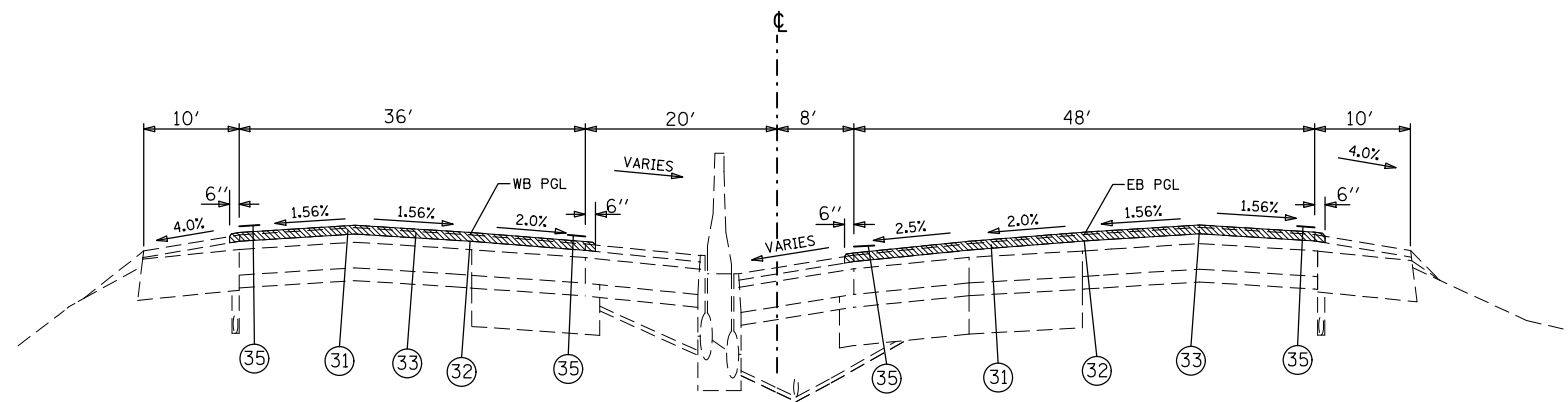
*NOTE:

THE INTENT OF THE HMA SURFACE REMOVAL IS TO REMOVE THE EXISTING MICROSURFACING AND THE EXISTING 2" HMA SURFACE.

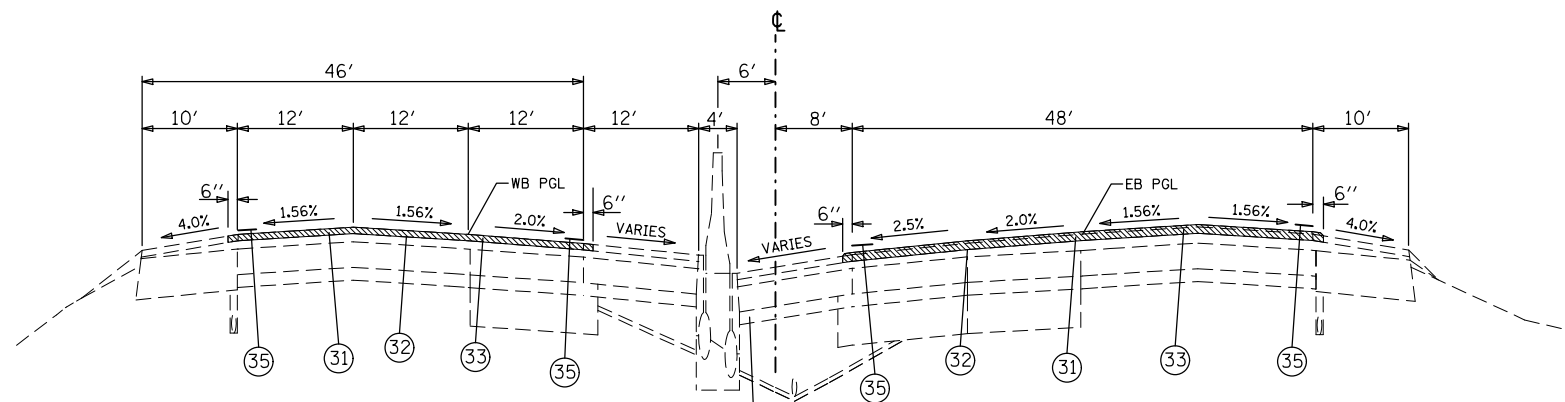
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PLOT SCALE = 100.0000' / 1".		CHECKED -	REVISED -		CONTRACT NO. 76H71							
PLOT DATE = 3/26/2015		DATE -	REVISED -		SCALE:	SHEET NO. 1 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		



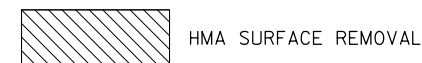
STA. 352+34 TO STA. 360+74



STA. 360+74 TO STA. 367+00



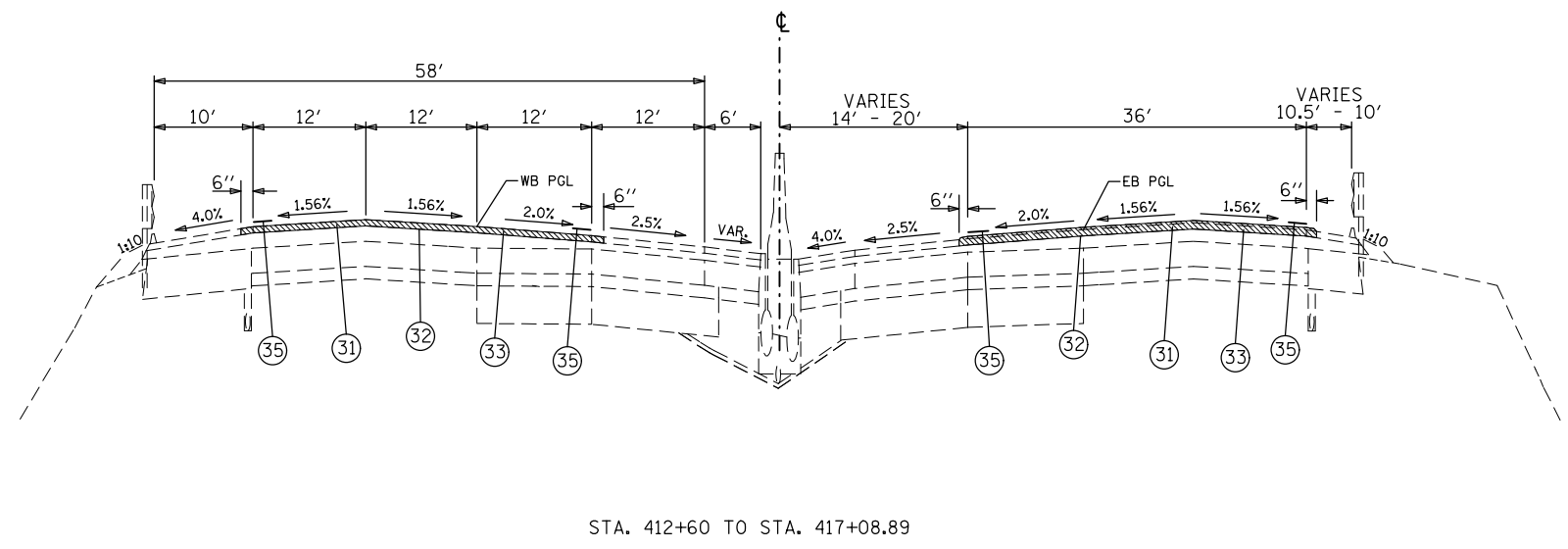
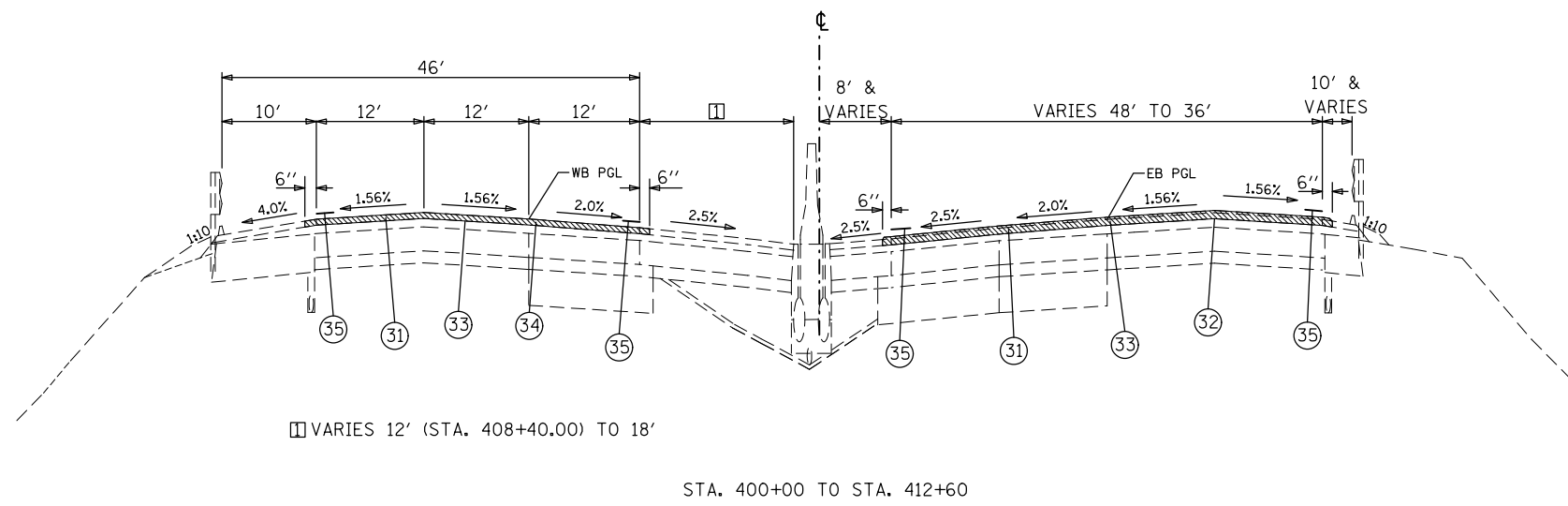
STA. 367+00 TO STA. 400+00



- LEGEND
- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
 - ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
 - ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
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 - ⑦ EXISTING 8" BITUMINOUS SHOULDER
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 - ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
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 - ⑲ EXISTING BITUMINOUS SHOULDER CURB
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 - ㉑ EXISTING SLOTTED DRAIN STRUCTURE
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 - ㉓ EXISTING FILTER FABRIC
 - ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
 - ㉕ EXISTING NOISE ABATEMENT WALL
 - ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
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 - ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE
 - ㉛ PROPOSED HMA SURFACE REMOVAL, 2" (SEE NOTE)
 - ㉜ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
 - ㉝ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT N80, 2"
 - ㉞ PROPOSED MICRO-SURFACING, 2 PASSES
 - ㉟ PROPOSED PREFORMED PLASTIC PAVEMENT MARKING TAPE, TYPE B INLAID EDGELINE
 - ㊱ PROPOSED THERMOPLASTIC PAVEMENT MARKING EDGELINE

*NOTE:
THE INTENT OF THE HMA SURFACE REMOVAL IS TO REMOVE THE EXISTING MICROSURFACING AND THE EXISTING 2" HMA SURFACE.

FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TYPICAL SECTIONS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\durosier_j\0419849\d876h71-sht-typical.dgn		DRAWN -	REVISED -		64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	19			
PLOT SCALE = 100.0000' / 1".		CHECKED -	REVISED -		CONTRACT NO. 76H71							
PLOT DATE = 3/26/2015		DATE -	REVISED -		SCALE:	SHEET NO. 2 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		

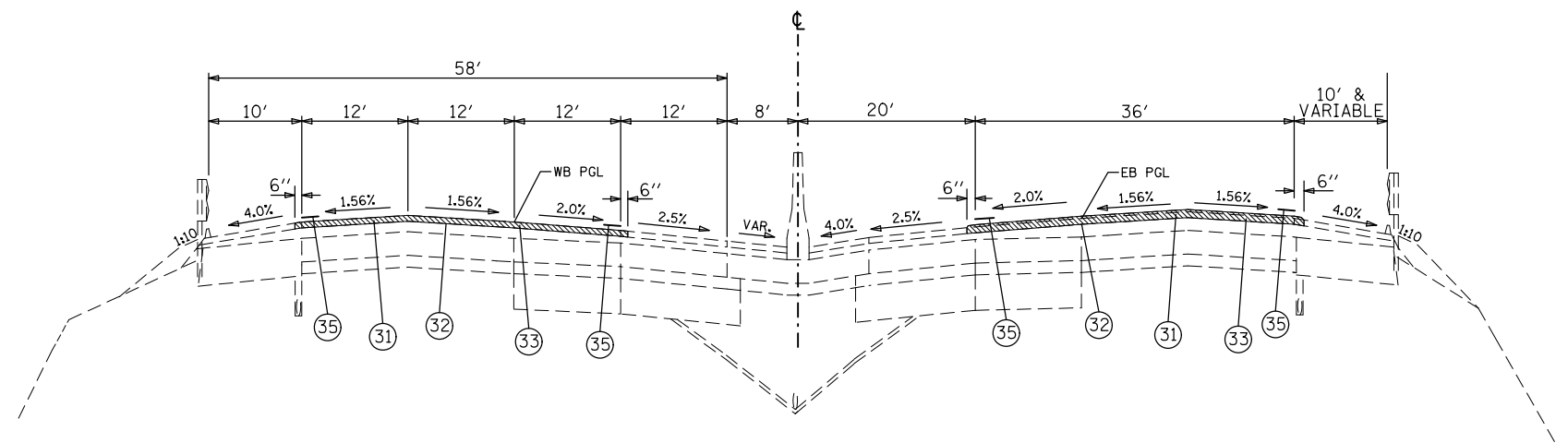


- LEGEND
- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
 - ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
 - ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
 - ④ EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS 1 (2")
 - ⑤ EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
 - ⑥ EXISTING 13" BITUMINOUS SHOULDER
 - ⑦ EXISTING 8" BITUMINOUS SHOULDER
 - ⑧ EXISTING STABILIZED SUB-BASE 4"
 - ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
 - ⑩ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A 4"
 - ⑪ EXISTING AGGREGATE SUBGRADE 12"
 - ⑫ EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
 - ⑬ EXISTING AGGREGATE SHOULDER, TYPE B
 - ⑭ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
 - ⑮ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - ⑯ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE 3/4 INCH EPOXY COATED SMOOTH DOWEL BARS AT 24" C-C AND 24" LONG
 - ⑰ EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
 - ⑱ EXISTING GUARDRAIL
 - ⑲ EXISTING BITUMINOUS SHOULDER CURB
 - ⑳ EXISTING PIPE UNDERDRAINS 6"
 - ㉑ EXISTING SLOTTED DRAIN STRUCTURE
 - ㉒ EXISTING STORM SEWER
 - ㉓ EXISTING FILTER FABRIC
 - ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
 - ㉕ EXISTING NOISE ABATEMENT WALL
 - ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
 - ㉗ EXISTING BITUMINOUS MATERIALS (PRIME COAT)
 - ㉘ EXISTING EMBANKMENT
 - ㉙ EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
 - ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE
 - ㉛ PROPOSED HMA SURFACE REMOVAL, 2" (SEE NOTE)
 - ㉜ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
 - ㉝ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT N80, 2"
 - ㉞ PROPOSED MICRO-SURFACING, 2 PASSES
 - ㉟ PROPOSED PREFORMED PLASTIC PAVEMENT MARKING TAPE, TYPE B INLAID EDGELINE
 - ㊱ PROPOSED THERMOPLASTIC PAVEMENT MARKING EDGELINE

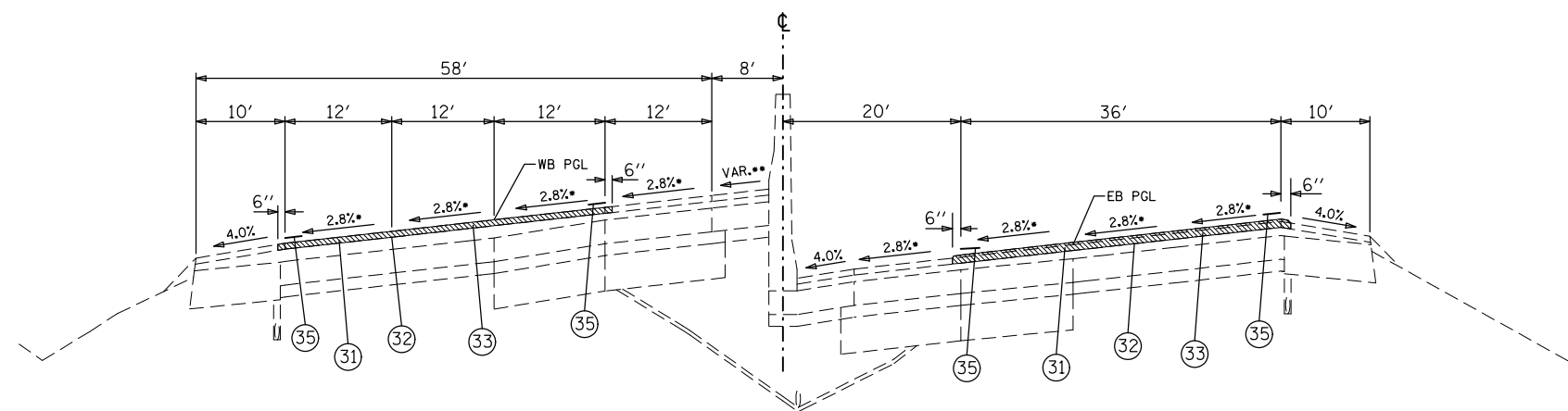
*NOTE:
THE INTENT OF THE HMA SURFACE REMOVAL IS TO REMOVE THE EXISTING MICROSURFACING AND THE EXISTING 2" HMA SURFACE.



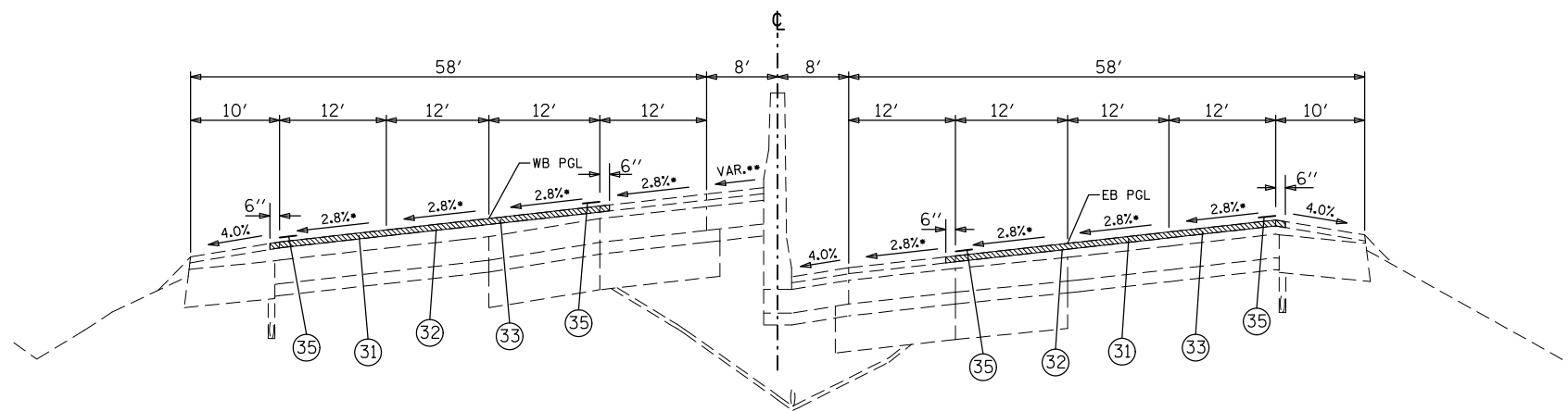
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et:\pw_work\p\dot\durosier_j\d0419849\d876h71-sht-typical.dgn		DRAWN -	REVISED -		64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	20			
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED -		CONTRACT NO. 76H71							
PLOT DATE = 3/26/2015		DATE -	REVISED -		SCALE:	SHEET NO. 3 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		



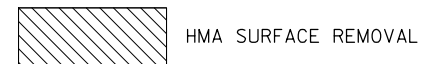
STA. 417+08.89 TO STA. 435+43.93 EB
 STA. 434+54.13 WB TO STA. 434+54.13 WB



STA. 435+43.93 EB TO STA. 443+00 EB
 STA. 434+54.13 WB TO STA. 443+00 WB



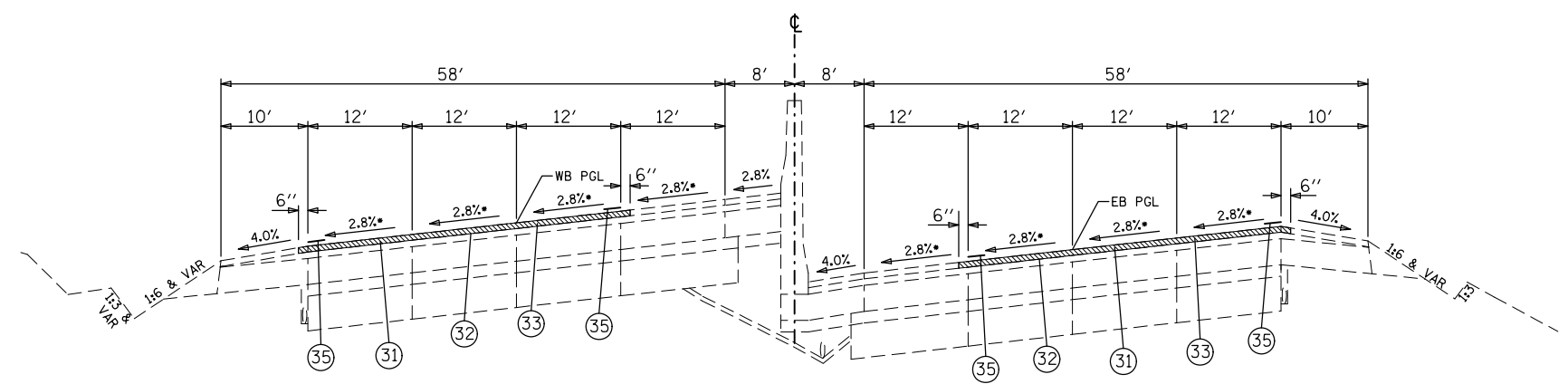
STA. 443+00 EB TO STA. 452+41.86 EB
 STA. 443+00 WB TO STA. 451+93.21 WB
 STA. 456+82.38 EB TO STA. 466+57.16 EB
 STA. 458+93.21 WB TO STA. 467+46.96 WB



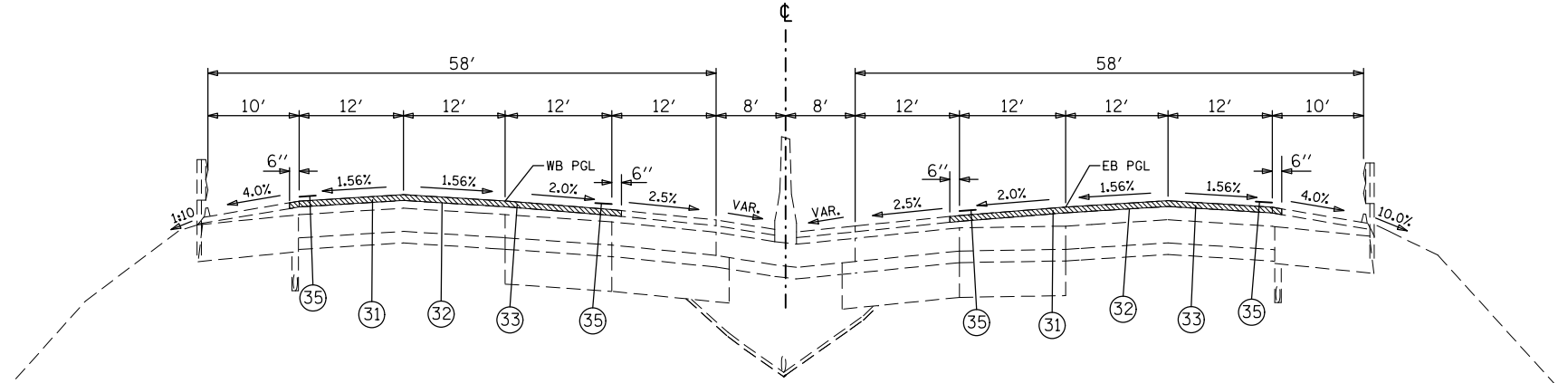
- LEGEND
- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
 - ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
 - ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
 - ④ EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS I (2")
 - ⑤ EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
 - ⑥ EXISTING 13" BITUMINOUS SHOULDER
 - ⑦ EXISTING 8" BITUMINOUS SHOULDER
 - ⑧ EXISTING STABILIZED SUB-BASE 4"
 - ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
 - ⑩ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A 4"
 - ⑪ EXISTING AGGREGATE SUBGRADE 12"
 - ⑫ EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
 - ⑬ EXISTING AGGREGATE SHOULDER, TYPE B
 - ⑭ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
 - ⑮ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - ⑯ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE 3/4 INCH EPOXY COATED SMOOTH DOWEL BARS AT 24" C-C AND 24" LONG
 - ⑰ EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
 - ⑱ EXISTING GUARDRAIL
 - ⑲ EXISTING BITUMINOUS SHOULDER CURB
 - ⑳ EXISTING PIPE UNDERDRAINS 6"
 - ㉑ EXISTING SLOTTED DRAIN STRUCTURE
 - ㉒ EXISTING STORM SEWER
 - ㉓ EXISTING FILTER FABRIC
 - ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
 - ㉕ EXISTING NOISE ABATEMENT WALL
 - ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
 - ㉗ EXISTING BITUMINOUS MATERIALS (PRIME COAT)
 - ㉘ EXISTING EMBANKMENT
 - ㉙ EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
 - ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE
 - ㉛ PROPOSED HMA SURFACE REMOVAL, 2" (SEE NOTE)
 - ㉜ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
 - ㉝ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT N80, 2"
 - ㉞ PROPOSED MICRO-SURFACING, 2 PASSES
 - ㉟ PROPOSED PREFORMED PLASTIC PAVEMENT MARKING TAPE, TYPE B INLAID EDGELINE
 - ㊱ PROPOSED THERMOPLASTIC PAVEMENT MARKING EDGELINE

*NOTE:
 THE INTENT OF THE HMA SURFACE REMOVAL IS TO REMOVE THE EXISTING MICROSURFACING AND THE EXISTING 2" HMA SURFACE.

FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TYPICAL SECTIONS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\durosier_j\d0419849\d876h71-sht-typical.dgn	DRAWN -	REVISED -	64					82-(4,5,6,7)-RS-1	ST. CLAIR	63	21	
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -	CONTRACT NO. 76H71									
PLOT DATE = 3/26/2015	DATE -	REVISED -	SCALE:		SHEET NO. 4 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

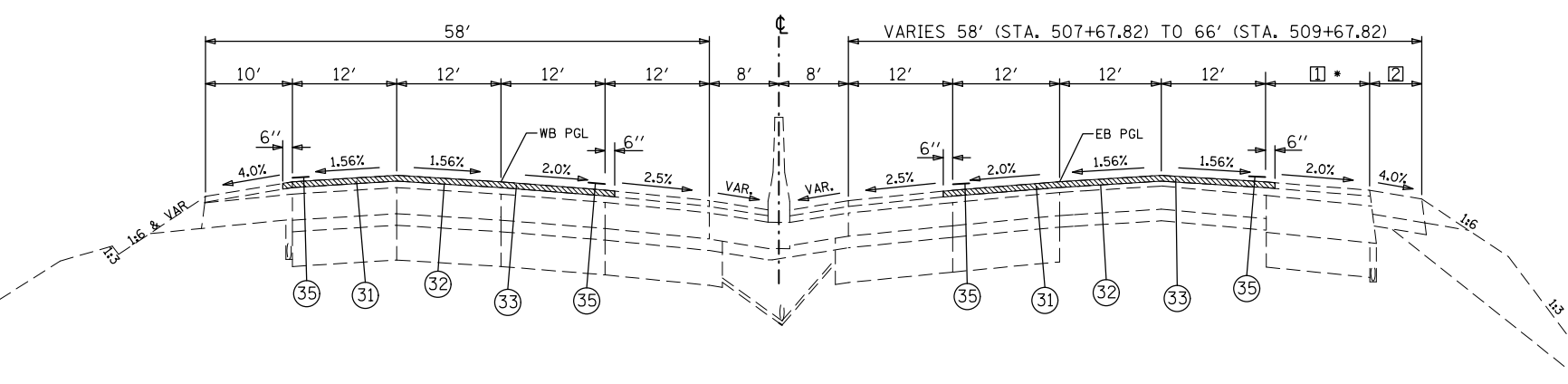


STA. 452+41.86 EB TO STA. 456+82.38 EB
 STA. 451+93.21 WB TO STA. 458+93.21 WB



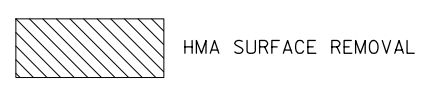
STA. 466+57.16 EB TO STA. 506+92.82 EB
 STA. 467+46.96 WB TO STA. 504+07.00 WB

STA. 521+38.92 EB TO STA. 547+36.24 EB
 STA. 510+34.00 WB TO STA. 547+36.24 WB



STA. 506+92.82 EB TO STA. 521+38.92 EB
 STA. 504+07.00 WB TO STA. 510+34.00 WB

* □ SHOULDER PAVEMENT & SUB-BASE WIDTH VARY
 10' (STA. 509+17.82) TO 12' (STA. 509+67.82)
 □ VARIES 0' (STA. 507+67.82) TO 6' (STA. 509+17.82)



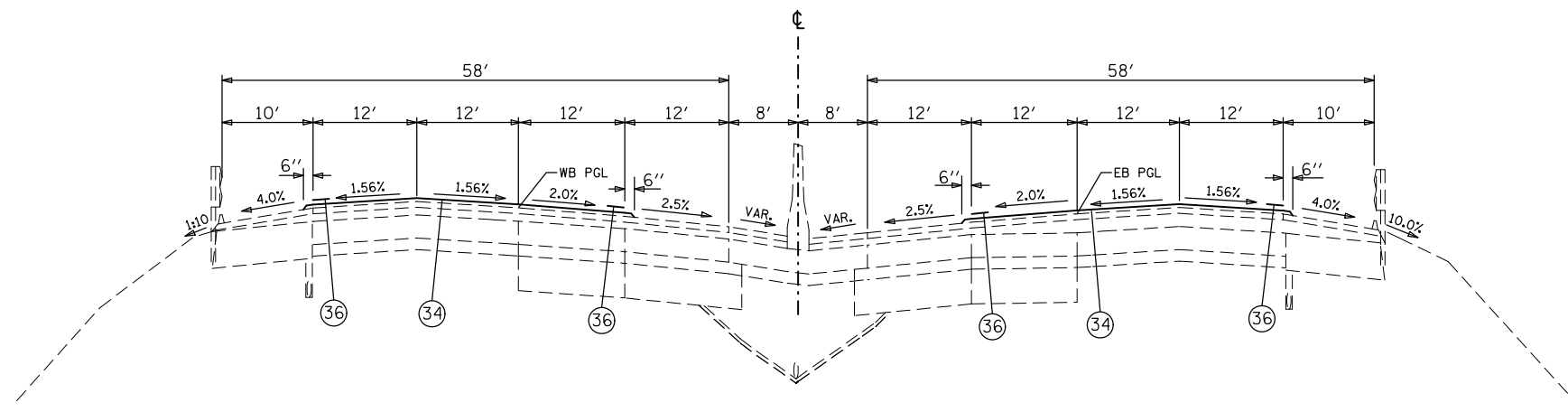
LEGEND

- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
- ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
- ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
- ④ EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS I (2")
- ⑤ EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
- ⑥ EXISTING 13" BITUMINOUS SHOULDER
- ⑦ EXISTING 8" BITUMINOUS SHOULDER
- ⑧ EXISTING STABILIZED SUB-BASE 4"
- ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- ⑩ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A 4"
- ⑪ EXISTING AGGREGATE SUBGRADE 12"
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- ⑬ EXISTING AGGREGATE SHOULDER, TYPE B
- ⑭ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
- ⑮ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 9"
- ⑯ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE 3/4 INCH EPOXY COATED SMOOTH DOWEL BARS AT 24" C-C AND 24" LONG
- ⑰ EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
- ⑱ EXISTING GUARDRAIL
- ⑲ EXISTING BITUMINOUS SHOULDER CURB
- ⑳ EXISTING PIPE UNDERDRAINS 6"
- ㉑ EXISTING SLOTTED DRAIN STRUCTURE
- ㉒ EXISTING STORM SEWER
- ㉓ EXISTING FILTER FABRIC
- ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ㉕ EXISTING NOISE ABATEMENT WALL
- ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
- ㉗ EXISTING BITUMINOUS MATERIALS (PRIME COAT)
- ㉘ EXISTING EMBANKMENT
- ㉙ EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
- ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE
- ㉛ PROPOSED HMA SURFACE REMOVAL, 2" (SEE NOTE)
- ㉜ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
- ㉝ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT N80, 2"
- ㉞ PROPOSED MICRO-SURFACING, 2 PASSES
- ㉟ PROPOSED PREFORMED PLASTIC PAVEMENT MARKING TAPE, TYPE B INLAID EDGELINE
- ㊱ PROPOSED THERMOPLASTIC PAVEMENT MARKING EDGELINE

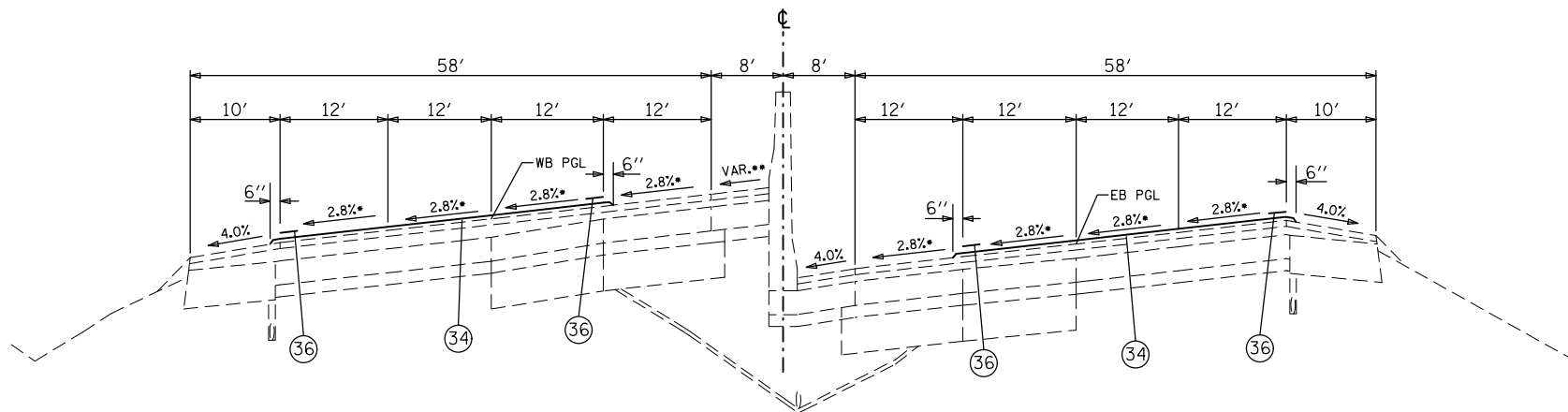
*NOTE:

THE INTENT OF THE HMA SURFACE REMOVAL IS TO REMOVE THE EXISTING MICROSURFACING AND THE EXISTING 2" HMA SURFACE.

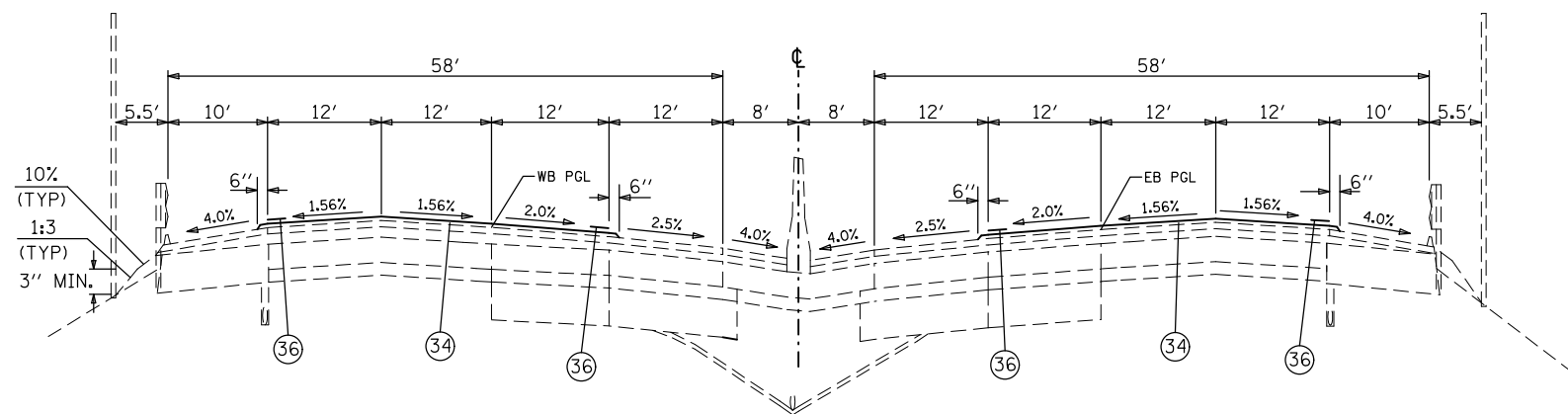
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et:\pw\work\p\dot\durosier_j\0419849\d876h71-sht-typical.dgn		DRAWN -	REVISED -		64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	22			
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED -		CONTRACT NO. 76H71							
PLOT DATE = 3/26/2015		DATE -	REVISED -		SCALE:	SHEET NO. 5 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		



STA. 547+36.24 EB TO STA. 548+60.20 EB
 STA. 547+36.24 WB TO STA. 547+70.40 WB
 STA. 607+83.78 EB TO STA. 613+80.02
 STA. 606+93.98 WB TO STA. 613+80.02
 STA. 693+92.51 EB TO STA. 725+00 EB
 STA. 692+18.71 WB TO STA. 705+00 WB



STA. 548+60.20 EB TO STA. 559+48.32 EB
 STA. 547+70.40 WB TO STA. 560+38.12 WB



STA. 559+48.32 EB TO STA. 585+31.38 EB
 STA. 560+38.12 WB TO STA. 586+21.18 WB

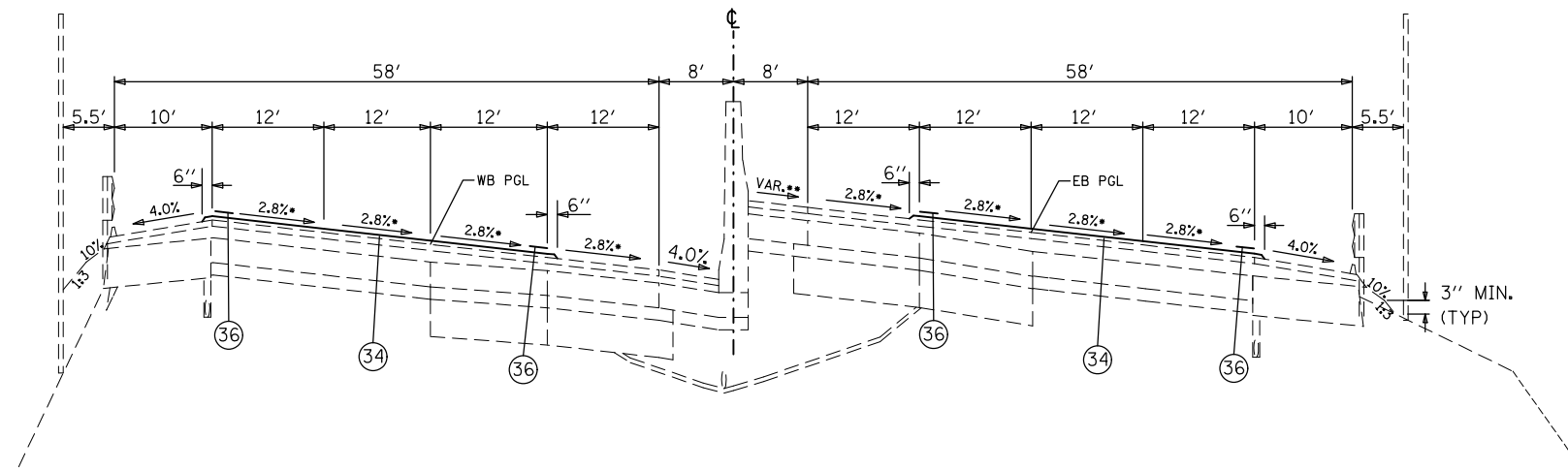
LEGEND

- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
- ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
- ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
- ④ EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS I (2")
- ⑤ EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
- ⑥ EXISTING 13" BITUMINOUS SHOULDER
- ⑦ EXISTING 8" BITUMINOUS SHOULDER
- ⑧ EXISTING STABILIZED SUB-BASE 4"
- ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- ⑩ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A 4"
- ⑪ EXISTING AGGREGATE SUBGRADE 12"
- ⑫ EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
- ⑬ EXISTING AGGREGATE SHOULDER, TYPE B
- ⑭ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
- ⑮ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 9"
- ⑯ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE 3/4 INCH EPOXY COATED SMOOTH DOWEL BARS AT 24" C-C AND 24" LONG
- ⑰ EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
- ⑱ EXISTING GUARDRAIL
- ⑲ EXISTING BITUMINOUS SHOULDER CURB
- ⑳ EXISTING PIPE UNDERDRAINS 6"
- ㉑ EXISTING SLOTTED DRAIN STRUCTURE
- ㉒ EXISTING STORM SEWER
- ㉓ EXISTING FILTER FABRIC
- ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ㉕ EXISTING NOISE ABATEMENT WALL
- ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
- ㉗ EXISTING BITUMINOUS MATERIALS (PRIME COAT)
- ㉘ EXISTING EMBANKMENT
- ㉙ EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
- ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE
- ㉛ PROPOSED HMA SURFACE REMOVAL, 2" (SEE NOTE)
- ㉜ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
- ㉝ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT N80, 2"
- ㉞ PROPOSED MICRO-SURFACING, 2 PASSES
- ㉟ PROPOSED PREFORMED PLASTIC PAVEMENT MARKING TAPE, TYPE B INLAID EDGELINE
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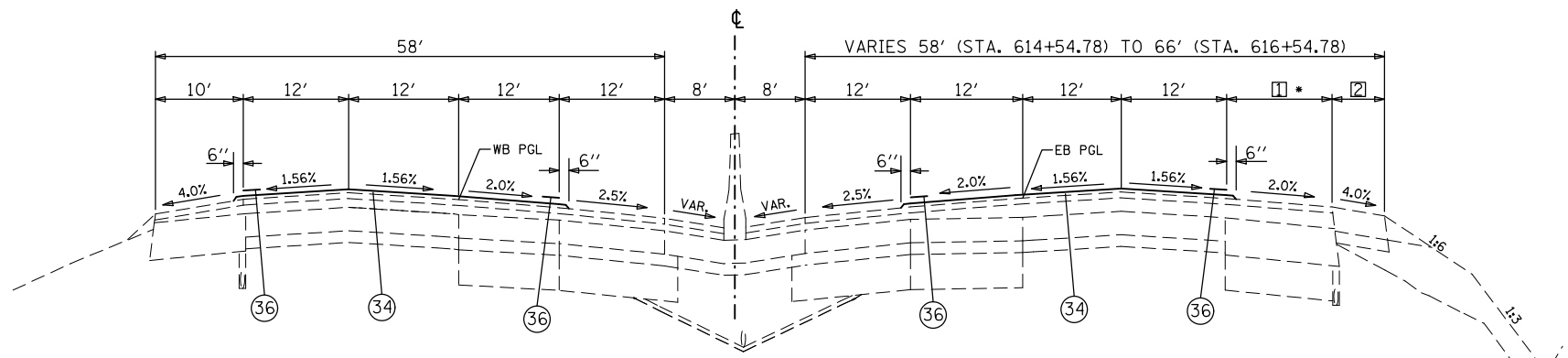
*NOTE:

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et:\pwork\pwork\dot\durosier_j\d0419849\d876h71-sht-typical.dgn	DRAWN -	REVISED -	64					82-(4,5,6,7)-RS-1	ST. CLAIR	63	23	
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -	CONTRACT NO. 76H71									
PLOT DATE = 3/26/2015	DATE -	REVISED -	SCALE:		SHEET NO. 6 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



STA. 585+31.38 EB TO STA. 607+83.78 EB
 STA. 586+21.18 WB TO STA. 606+93.98 WB
 (BRIDGE OMISSION STA. 589+97 TO STA. 591+49)



STA. 613+80.02 TO STA. 657+24.79 EB
 STA. 652+43.94 WB

* □ SHOULDER PAVEMENT & SUB-BASE WIDTH VARY
 10' (STA. 616+04.78) TO 12' (STA. 616+54.78)
 □ VARIES 0' (STA. 614+54.78) TO 6' (STA. 616+04.78)

STATION EQUATION
 STA. 618+20.65 BK =
 STA. 643+85.18 AH

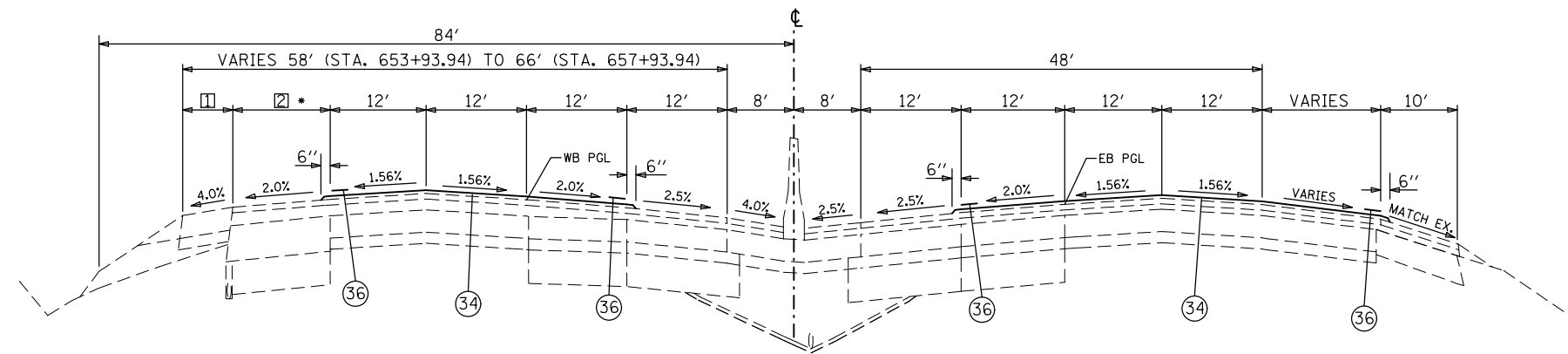
LEGEND

- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
- ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
- ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
- ④ EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS I (2")
- ⑤ EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
- ⑥ EXISTING 13" BITUMINOUS SHOULDER
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- ⑰ EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
- ⑱ EXISTING GUARDRAIL
- ⑲ EXISTING BITUMINOUS SHOULDER CURB
- ⑳ EXISTING PIPE UNDERDRAINS 6"
- ㉑ EXISTING SLOTTED DRAIN STRUCTURE
- ㉒ EXISTING STORM SEWER
- ㉓ EXISTING FILTER FABRIC
- ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ㉕ EXISTING NOISE ABATEMENT WALL
- ㉖ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
- ㉗ EXISTING BITUMINOUS MATERIALS (PRIME COAT)
- ㉘ EXISTING EMBANKMENT
- ㉙ EXISTING MICRO-SURFACING SINGLE PASS, TYPE II, 1/2" (WITH RUT FILLING)
- ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE
- ㉛ PROPOSED HMA SURFACE REMOVAL, 2" (SEE NOTE)
- ㉜ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
- ㉝ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT N80, 2"
- ㉞ PROPOSED MICRO-SURFACING, 2 PASSES
- ㉟ PROPOSED PREFORMED PLASTIC PAVEMENT MARKING TAPE, TYPE B INLAID EDGELINE
- ㊱ PROPOSED THERMOPLASTIC PAVEMENT MARKING EDGELINE

*NOTE:

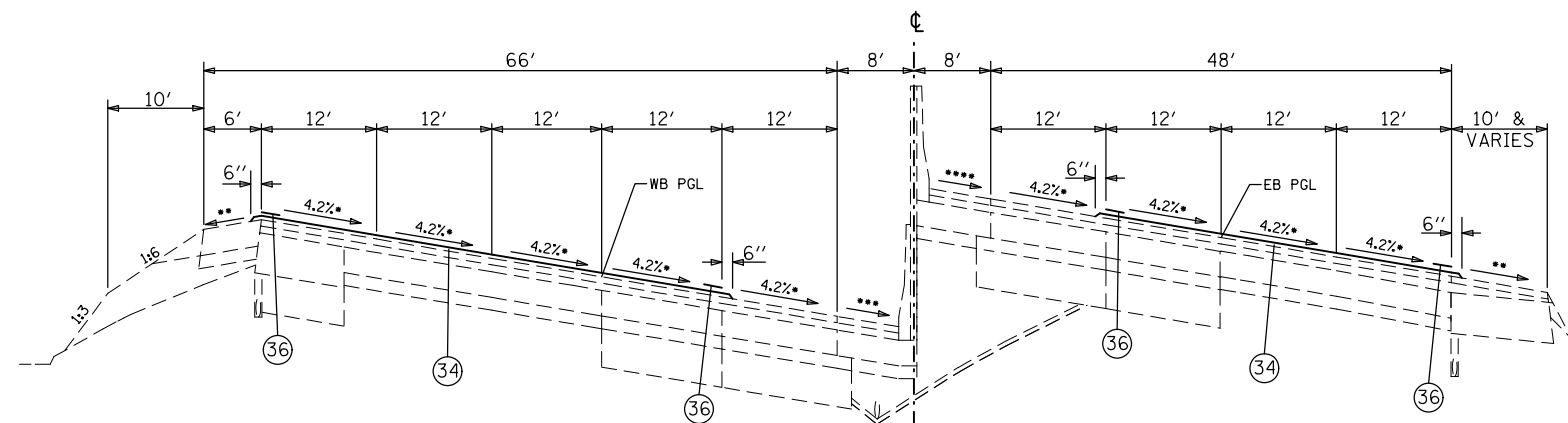
THE INTENT OF THE HMA SURFACE REMOVAL IS TO REMOVE THE EXISTING MICROSURFACING AND THE EXISTING 2" HMA SURFACE.

FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TYPICAL SECTIONS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\durosier_j\d0419849\d876h71-sht-typical.dgn		DRAWN -	REVISED -		64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	24			
PLOT SCALE = 100.0000' / 1in.		CHECKED -	REVISED -		CONTRACT NO. 76H71							
PLOT DATE = 3/26/2015		DATE -	REVISED -		SCALE:	SHEET NO. 7 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		



□ VARIES 0' (STA. 653+93.94) TO 6' (STA. 656+93.94)
 * SHOULDERS PAVEMENT & SUB-BASE WIDTH VARY
 10' (STA. 656+93.94) TO 12' (STA. 657+93.94)

STA. 657+24.79 EB TO STA. 658+69.57 EB
 STA. 652+43.94 WB TO STA. 658+54.90 WB



STA. 658+69.57 EB TO STA. 661+10.04
 STA. 658+54.90 WB
 STA. 662+84.20 TO STA. 672+93.94

- * SEE SUPERELEVATION TRANSITION DETAILS FOR PAVEMENT SUPERELEVATION.
- ** WHEN THE SUPERELEVATION RATE IS 4% OR LESS, THE SHOULDER SLOPE IS 4%. WHEN PAVEMENT SUPERELEVATION RATE EXCEEDS 4%, THE MAXIMUM ALGEBRAIC DIFFERENCE IN CROSS SLOPE IS 8%.
- *** WHEN THE SUPERELEVATION RATE IS 4% OR LESS, THE SHOULDER SLOPE IS 4%. WHEN PAVEMENT SUPERELEVATION RATE EXCEEDS 4%, THE SHOULDER SLOPE SHOULD EQUAL ADJACENT PAVEMENT SUPERELEVATION RATE.
- **** SHOULDER SLOPE SHOULD EQUAL ADJACENT PAVEMENT SUPERELEVATION RATE.

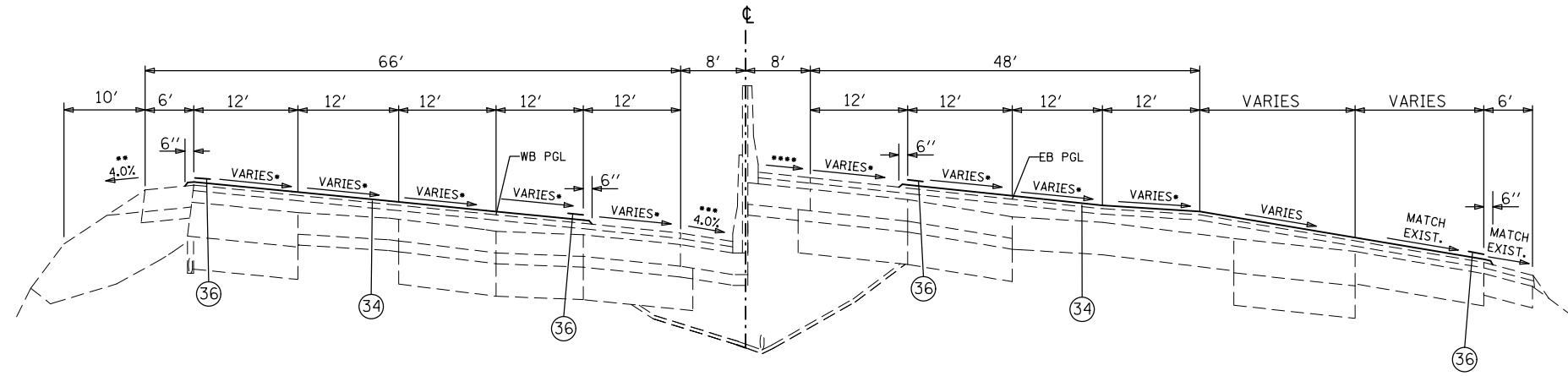
LEGEND

- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
- ② EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
- ③ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE NO. 6 EPOXY COATED DEFORMED TIE BARS AT 24" C-C AND 24" LONG, DRILLED AND GROUTED
- ④ EXISTING POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, CLASS I (2")
- ⑤ EXISTING POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N105 (2-1/4" & VARIES)
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- ⑦ EXISTING 8" BITUMINOUS SHOULDER
- ⑧ EXISTING STABILIZED SUB-BASE 4"
- ⑨ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B 12"
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- ⑪ EXISTING AGGREGATE SUBGRADE 12"
- ⑫ EXISTING POROUS GRANULAR EMBANKMENT, SUBGRADE
- ⑬ EXISTING AGGREGATE SHOULDER, TYPE B
- ⑭ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 8"
- ⑮ EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 9"
- ⑯ EXISTING LONGITUDINAL CONSTRUCTION JOINT - USE 3/4 INCH EPOXY COATED SMOOTH DOWEL BARS AT 24" C-C AND 24" LONG
- ⑰ EXISTING CONCRETE BARRIER, CONCRETE BARRIER (MODIFIED), CONCRETE BARRIER (SPECIAL), OR CONCRETE BARRIER TRANSITION
- ⑱ EXISTING GUARDRAIL
- ⑲ EXISTING BITUMINOUS SHOULDER CURB
- ⑳ EXISTING PIPE UNDERDRAINS 6"
- ㉑ EXISTING SLOTTED DRAIN STRUCTURE
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- ㉚ EXISTING THERMOPLASTIC PAVEMENT MARKING, EDGELINE
- ㉛ PROPOSED HMA SURFACE REMOVAL, 2" (SEE NOTE)
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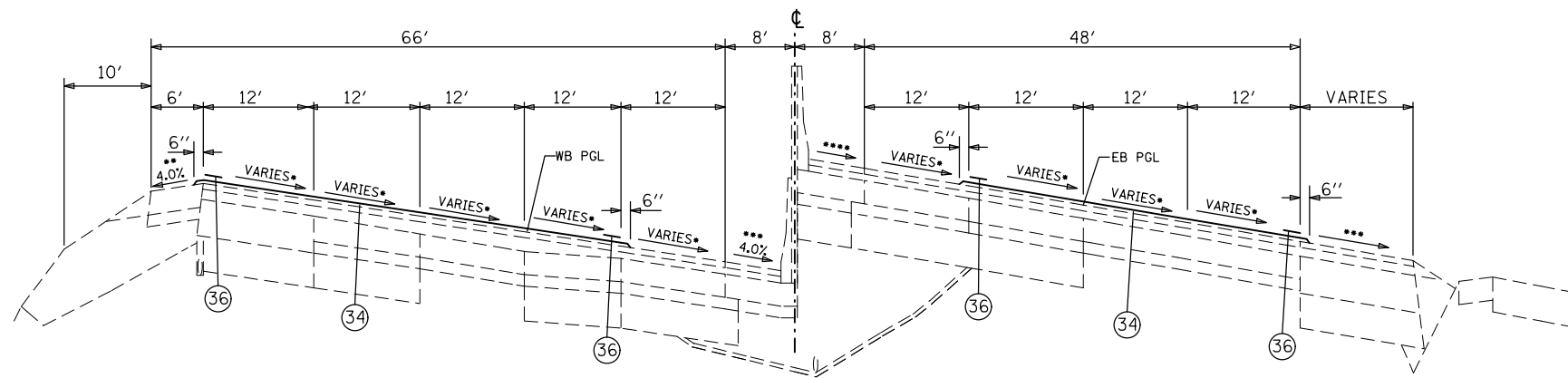
*NOTE:

THE INTENT OF THE HMA SURFACE REMOVAL IS TO REMOVE THE EXISTING MICROSURFACING AND THE EXISTING 2" HMA SURFACE.

FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TYPICAL SECTIONS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\durosier_j\d0419849\d876h71-sht-typical.dgn		DRAWN -	REVISED -		64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	25			
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED -		CONTRACT NO. 76H71							
PLOT DATE = 3/26/2015		DATE -	REVISED -		SCALE:	SHEET NO. 8 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		



STA. 661+10.04 TO STA. 662+23.36



STA. 662+23.36 TO STA. 662+84.20

LEGEND

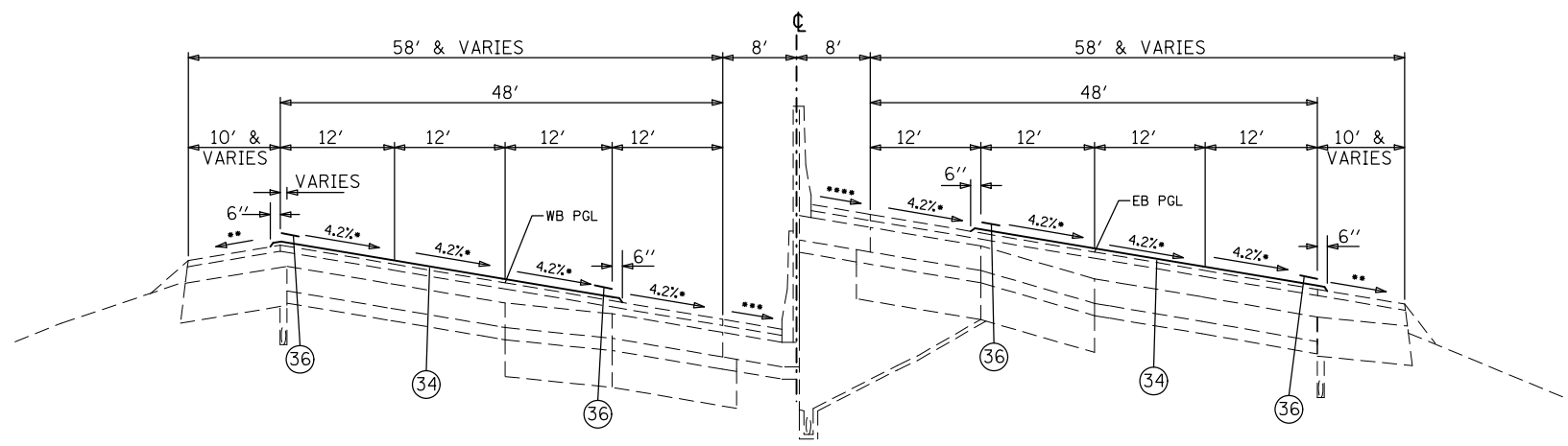
- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
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- ㉑ EXISTING SLOTTED DRAIN STRUCTURE
- ㉒ EXISTING STORM SEWER
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- ㉔ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
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- ㊱ PROPOSED THERMOPLASTIC PAVEMENT MARKING EDGELINE

*NOTE:

THE INTENT OF THE HMA SURFACE REMOVAL IS TO REMOVE THE EXISTING MICROSURFACING AND THE EXISTING 2" HMA SURFACE.

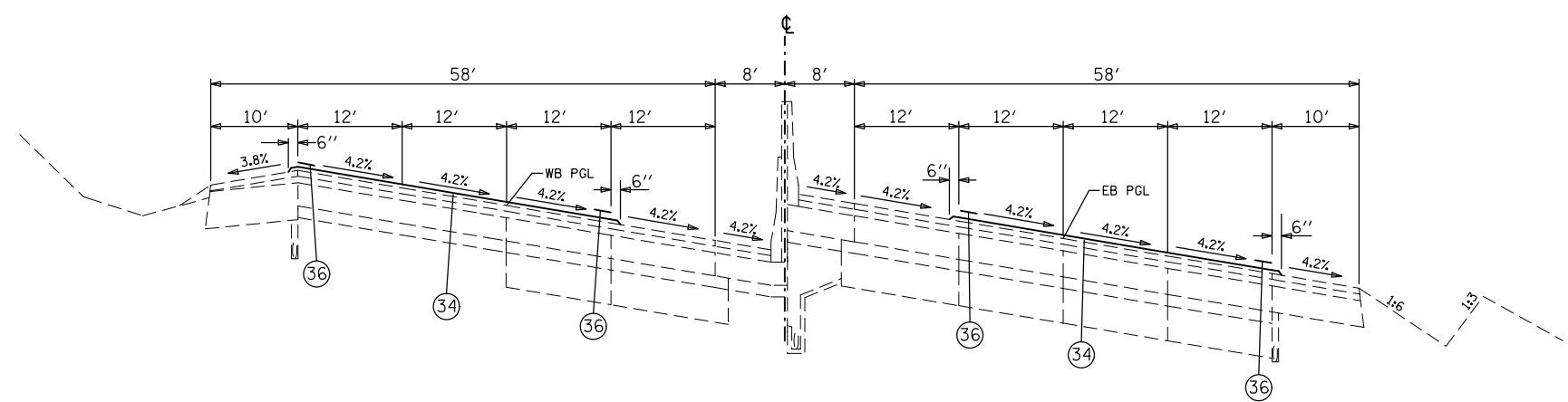
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FILE NAME =	USER NAME = durosier.j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TYPICAL SECTIONS			F.A.I. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p1dot\durosier.j\d0419849\d876h71-sht-typical.dgn	PLOT SCALE = 100.0000' / 1in.	DRAWN -	REVISED -					64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	26
	PLOT DATE = 3/26/2015	CHECKED -	REVISED -		CONTRACT NO. 76H71							
		DATE -	REVISED -		SCALE:	SHEET NO. 9 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		



- * SEE SUPERELEVATION TRANSITION DETAILS FOR PAVEMENT SUPERELEVATION.
- ** WHEN THE SUPERELEVATION RATE IS 4% OR LESS, THE SHOULDER SLOPE IS 4%. WHEN PAVEMENT SUPERELEVATION RATE EXCEEDS 4%, THE MAXIMUM ALGEBRAIC DIFFERENCE IN CROSS SLOPE IS 8%.
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- **** SHOULDER SLOPE SHOULD EQUAL ADJACENT PAVEMENT SUPERELEVATION RATE.

STA. 672+93.94 TO STA. 681+00
 STA. 689+15 TO STA. 693+92.51 EB
 STA. 692+18.71 WB



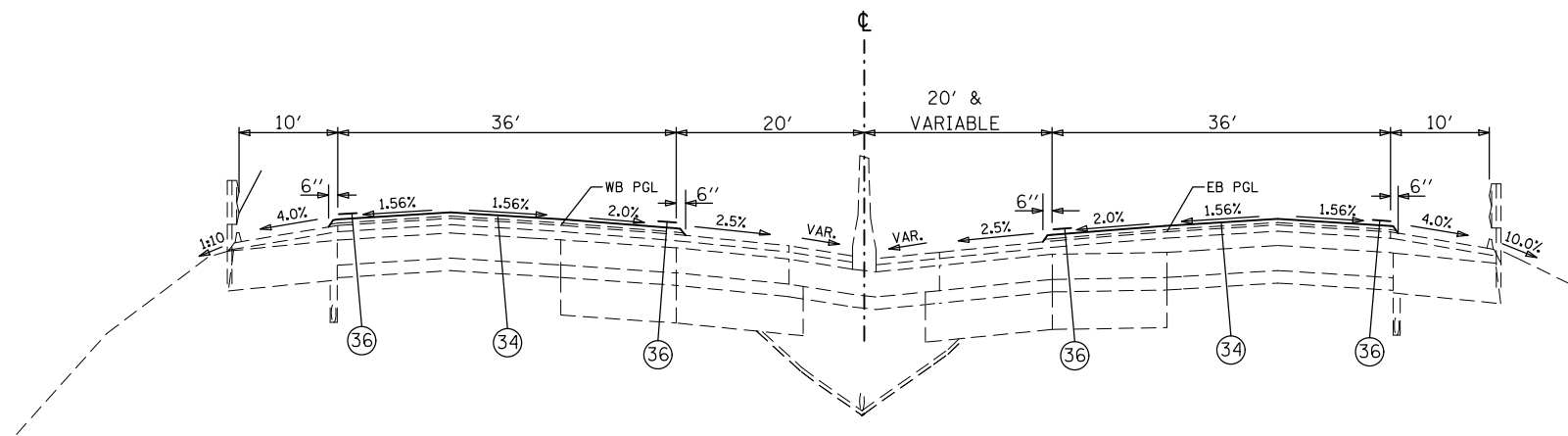
STA. 681+00 TO STA. 689+15

LEGEND

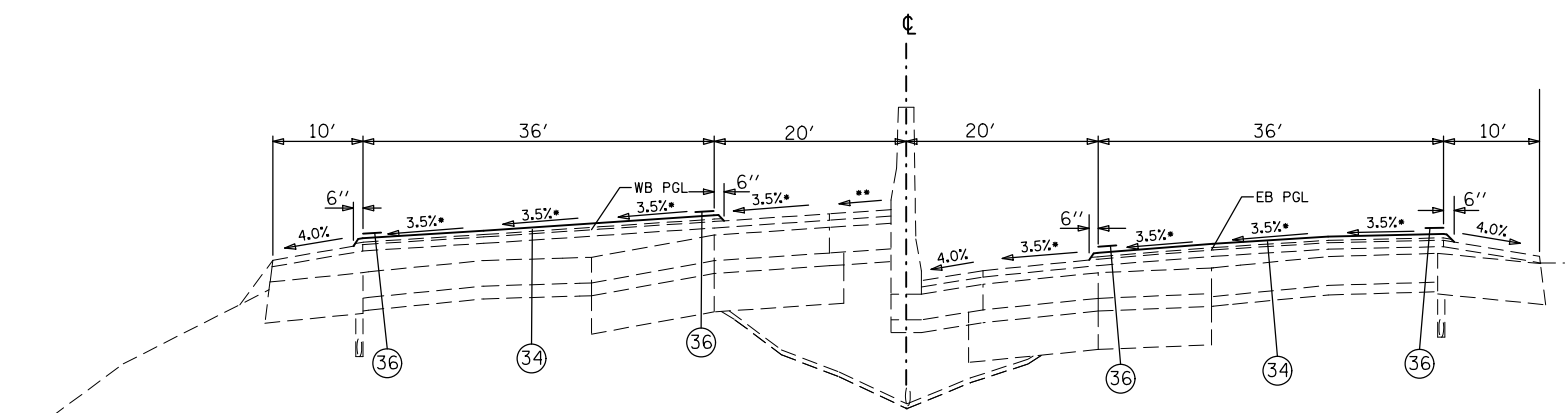
- 1 EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
- 2 EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"
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- 18 EXISTING GUARDRAIL
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- 21 EXISTING SLOTTED DRAIN STRUCTURE
- 22 EXISTING STORM SEWER
- 23 EXISTING FILTER FABRIC
- 24 EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- 25 EXISTING NOISE ABATEMENT WALL
- 26 EXISTING BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (2")
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- 33 PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT N80, 2"
- 34 PROPOSED MICRO-SURFACING, 2 PASSES
- 35 PROPOSED PREFORMED PLASTIC PAVEMENT MARKING TAPE, TYPE B INLAID EDGELINE
- 36 PROPOSED THERMOPLASTIC PAVEMENT MARKING EDGELINE

*NOTE:
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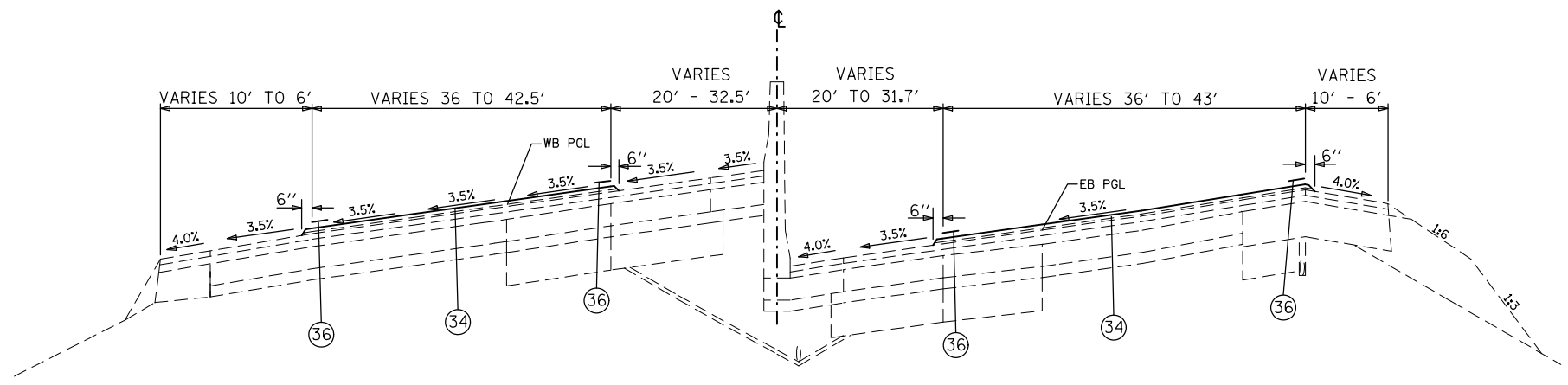
FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TYPICAL SECTIONS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\durosier_j\d0419849\d876h71-sht-typical.dgn	PLOT SCALE = 100.0000' / 1in.	DRAWN -	REVISED -					64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	27
	PLOT DATE = 3/26/2015	CHECKED -	REVISED -		SCALE: SHEET NO. 10 OF 11 SHEETS STA. TO STA.			CONTRACT NO. 76H71				
		DATE -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							



STA. 725+00 EB TO STA. 732+09.69 EB
 STA. 705+00 WB TO STA. 730+77.89 WB



STA. 732+09.69 EB TO STA. 735+74.76
 STA. 730+77.89 WB



STA. 735+74.76 TO STA. 747+46 EB
 STA. 749+45 WB

LEGEND

- ① EXISTING CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 8"
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*NOTE:

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FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TYPICAL SECTIONS	F.A.I. RTE. =	SECTION =	COUNTY =	TOTAL SHEETS =	SHEET NO. =	
		DRAWN -	REVISED -			64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	28	
		CHECKED -	REVISED -			CONTRACT NO. 76H71					
		DATE -	REVISED -			SCALE:	SHEET NO. 11 OF 11 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

LOCATION			PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID								THERMOPLASTIC PAVEMENT MARKING										PAVEMENT MARKING REMOVAL**		RAISED REFLECTIVE PAVEMENT MARKER									
			LINE - 4"				LINE - 8"				LINE - 4"					LINE - 8"							REMOVAL		NEW							
			SKIP-DASH (WHITE)		SOLID (WHITE)		SOLID (YELLOW)		SKIP-DASH (WHITE)		SOLID (WHITE)		SKIP-DASH (WHITE)		SOLID (WHITE)		SOLID (YELLOW)		SKIP-DASH (WHITE)				SOLID (WHITE)		SQ FT		1-WAY CRYSTAL		1-WAY CRYSTAL			
STA TO STA			FOOT		FOOT		FOOT		FOOT		FOOT		FOOT		FOOT		FOOT		FOOT		SQ FT		EACH		EACH							
			EB	WB	EB	WB	EB	WB	EB	WB	EB	WB	EB	WB	EB	WB	EB	WB	EB	WB	EB	WB	EB	WB	EB	WB						
337+00	TO	348+00	550	550	810	1047	1100	1100			551	756									367	367	55	90	55	90						
348+00	TO	349+15	58	58	115	115	115	115				230									38	38	9	17	9	17						
BRIDGE OMISSION																																
351+52	TO	400+00	3636	2445	4848	54849	4849	4849		56											1616	1616	364	260	364	260						
400+00	TO	408+00	400	400	800	800	800	800	200												267	267	40	40	40	40						
408+00	TO	423+00	750	750	1500	1500	1500	1500	10												500	500	75	75	75	75						
423+00	TO	438+00	750	750	1500	1500	1500	1500													500	500	75	75	75	75						
438+00	TO	452+00	700	700	1400	1400	1400	1400													467	467	70	70	70	70						
452+00	TO	467+00	750	750	1500	1500	1500	1500													500	500	75	75	75	75						
467+00	TO	482+00	750	750	1500	1500	1500	1500													500	500	75	75	75	75						
482+00	TO	497+00	750	750	1500	1500	1500	1500													500	500	75	75	75	75						
497+00	TO	512+00	750	750	1500	1500	1500	1500	127												500	500	100	75	100	75						
512+00	TO	527+00	915	824	1500	1502	1500	1500	88		860	512									500	500	126	82	126	82						
527+00	TO	547+36	1068	1091	1936	1983	1986	2036			850	389	13			50			50		679	679	109	109	109	109						
547+36	TO	557+00											552	482	964	853	964	964		76	570	745	1207	1313	55	99	55	99				
557+00	TO	572+00											750	750	1500	1500	1500	1500					1250	1250	75	75	75	75				
572+00	TO	587+00											750	750	1500	1500	1500	1500					1250	1250	75	75	75	75				
587+00	TO	589+97											149	149	297	297	297	297					248	248	15	15	15	15				
BRIDGE OMISSION																																
591+49	TO	602+00											526	526	1051	1051	1051	1051					876	876	53	53	53	53				
602+00		617+00											750	750	1500	1500	1500	1500	85				1307	1250	77	75	77	75				
617+00	TO	657+00											899	718	1436	1436	1436	1436	177				1375	1197	162	200	162	200				
657+00	TO	671+00											722	831	1400	1400	1400	1400			640		1601	1210	107	83	107	83				
671+00	TO	686+00											750	799	1500	1500	1500	1500				1298	1250	2132	75	80	75	80				
686+00	TO	701+00											821	750	1500	1500	1500	1500			1062	734	1982	1739	82	112	82	112				
701+00	TO	716+00											750	750	1500	1500	1500	1500		71		48	1250	1329	75	92	75	92				
716+00	TO	731+00											375	750	1500	1500	1500	1500	375				1375	1250	38	75	38	75				
731+00	TO	749+45											412	785	1646	1845	1646	1845	285		1010	1054	2098	2194	71	78	71	78				
SUB-TOTALS			11827	10568	20410	20696	20750	20800	425	56	2261	1887	8217	8788	17294	17382	17344	17493	922	146	3332	3879	24000	24171	2208	2230	2208	2230				
TOTALS			105050								4629				86518								8280				48171		4438		4438	

*STATION EQUATION 618+20.65 BK = 643+85.18 AH

**Pavement Marking Removal within resurfacing limits reflects 4" yellow edge line removal for Traffic Control and Protection Special

*THERMOPLASTIC PAVEMENT MARKING LINE, 12"						
LOCATION			PAVEMENT MARKING REMOVAL		SOLID (YELLOW)	
STA TO STA			SQ FT		FOOT	
			EB	WB	EB	WB
337+00	TO	348+00	198	198	198	198
348+00	TO	363+00	182	182	182	182
363+00	TO	378+00	182	182	182	182
378+00	TO	393+00	182	182	182	182
393+00	TO	408+00	173	192	173	192
408+00	TO	423+00	215	234	215	234
423+00	TO	438+00	267	267	267	267
438+00	TO	452+00	268	268	268	268
452+00	TO	467+00	269	269	269	269
467+00	TO	482+00	269	269	269	269
482+00	TO	497+00	269	269	269	269
497+00	TO	512+00	269	269	269	269
512+00	TO	527+00	269	269	269	269
527+00	TO	542+00	269	269	269	269
542+00	TO	557+00	269	269	269	269
557+00	TO	572+00	269	269	269	269
572+00	TO	587+00	269	269	269	269
587+00	TO	602+00	269	269	269	269
602+00	TO	617+00	269	269	269	269
643+00	TO	657+00	269	269	269	269
657+00	TO	671+00	269	269	269	269
671+00	TO	686+00	269	269	269	269
686+00	TO	701+00	269	269	269	269
701+00	TO	716+00	269	269	269	269
716+00	TO	731+00	269	269	269	269
731+00	TO	745+00	269	269	269	269
SUB-TOTAL			6505	6543	6505	6543
TOTAL			13047		13047	

* FOR REMOVAL AND REPLACEMENT OF DIAGONAL YELLOW PAVEMENT MARKINGS IN MEDIAN FOR ENTIRE LENGTH OF PROJECT

**THERMOPLASTIC PAVEMENT MARKING LINE, 12"			
LOCATION		PAVEMENT MARKING REMOVAL (SQ FT)	
		EB	WB
IL 157 INTERCHANGE		150	225
IL 159 INTERCHANGE		225	200
US 50 INTERCHANGE		150	225
GREENMOUNT INTERCHANGE		250	
SUB-TOTAL		775	650
TOTAL		1425	

** FOR THE REMOVAL OF EXISTING DIAGONAL WHITE PAVEMENT MARKINGS LOCATED IN THE GORES AND ON THE SHOULDERS

LOCATION			HMA SURFACE REMOVAL 2"	BITUMINOUS MATERIALS (PRIME COAT)	POLY HMA SURFACE COURSE, STONE MATRIX ASPHALT N80
			SD YD	POUNDS	TONS
EASTBOUND					
337+00	TO	346+39	3860	1737	432
346+39	TO	400+00	30507	13728	3417
400+00	TO	408+41	3995	1798	447
408+41	TO	506+91	40494	18223	4535
506+91	TO	509+66	1306	588	146
509+66	TO	522+11	7055	3175	790
522+11	TO	526+00	2641	1188	296
526+00	TO	535+00	3700	1665	414
535+00	TO	547+36	6595	2968	739
WESTBOUND					
337+00	TO	344+75	3186	1434	357
344+75	TO	348+60	2828	1272	317
348+60	TO	352+35	2125	956	238
352+35	TO	354+57	1055	475	118
354+57	TO	513+62	65387	29424	7323
513+62	TO	521+00	3670	1651	411
521+00	TO	524+44	2026	912	227
524+44	TO	526+67	1539	692	172
526+67	TO	527+00	194	87	22
527+00	TO	527+25	101	46	11
527+25	TO	537+79	5241	2358	587
537+79	TO	539+45	978	440	109
539+45	TO	547+36	3252	1463	364
SUB-TOTALS			189214	85146	21192
TOTALS			189214	85146	21192

*SUB-TOTALS INCLUDE BRIDGE OMISSION FROM STA. 349+15.20 TO STA. 351+51.50

LOCATION			MICRO-SURFACING 2 PASSES, TYPE II MIXTURE E		BITUMINOUS MATERIALS (PRIME COAT)	
			EB	WB	EB	WB
STA TO STA			SQ YD	SQ YD	POUNDS	POUNDS
EASTBOUND						
547+06	TO	550+21	2000		270	
550+21	TO	553+00	1581		213	
553+00	TO	559+15	2921		394	
559+15	TO	613+60	22385		3022	
613+60	TO	616+52	1387		187	
616+52	TO	618+21	920		124	
618+21	TO	643+85	6980		942	
643+85	TO	657+90	7664		1035	
657+90	TO	661+10	2025		273	
661+10	TO	693+00	13114		1770	
693+00	TO	697+56	3053		412	
697+56	TO	700+41	1615		218	
700+41	TO	706+32	2807		379	
706+32	TO	742+41	14837		2003	
742+41	TO	747+46	2749		371	
WESTBOUND						
547+06	TO	549+00		798		108
549+00	TO	552+30		2250		304
552+30	TO	555+32		1502		203
555+32	TO	618+21		25855		3490
618+21	TO	643+85		6980		942
643+85	TO	652+44		3531		477
652+44	TO	657+94		2613		353
657+94	TO	672+93		8494		1147
672+93	TO	678+00		3155		426
678+00	TO	679+17		670		90
679+17	TO	697+33		7466		1008
697+33	TO	701+24		2737		369
701+24	TO	704+07		1407		190
704+07	TO	735+00		12716		1717
735+00	TO	743+93		3870		522
743+93	TO	749+45		2969		401
SUB-TOTALS			85430	86401	11533	11664
TOTALS			171831		23197	

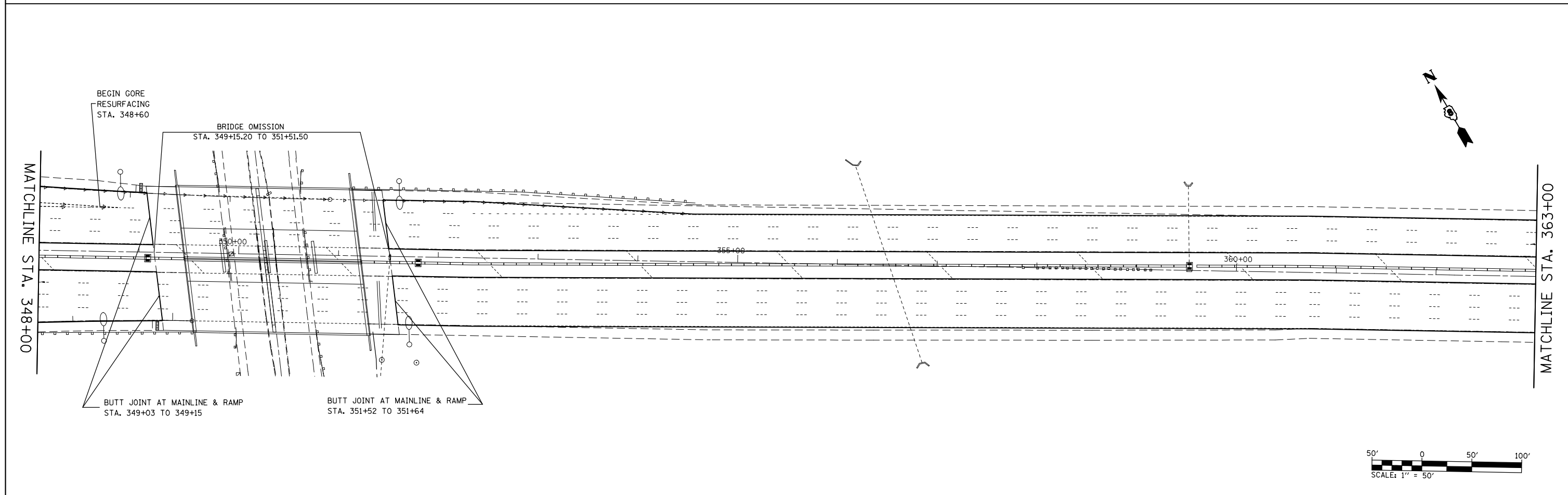
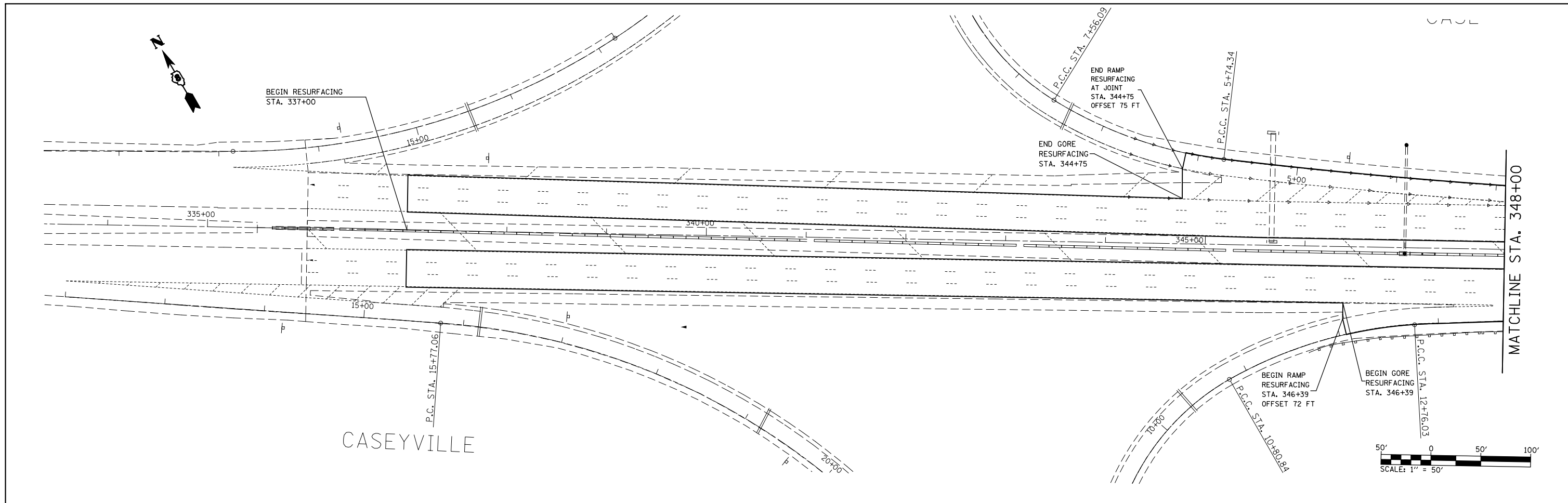
*SUB-TOTALS INCLUDE BRIDGE OMISSION STA. 589+97 TO STA. 591+49

CLASS A PAVEMENT PATCHING 13 INCH SCHEDULE										
LOCATION	LANE	LENGTH	LANE WIDTH	CLASS A PATCH TYPE I	CLASS A PATCH TYPE II	CLASS A PATCH TYPE III	CLASS A PATCH TYPE IV	PATCHING REINF.	TIE BARS NO. 6	SAWCUT
		(FT)	(FT)	(SQ YD)	(SQ YD)	(SQ YD)	(SQ YD)	(SQ YD)	(EACH)	(FT)
EASTBOUND										
355+00	MIDDLE	6.0	12.0		8.0			8.0		60.0
397+24	MIDDLE	6.0	12.0		8.0			8.0		60.0
591+49	INSIDE	6.0	12.0		8.0			8.0		60.0
591+49	MIDDLE	6.0	12.0		8.0			8.0		60.0
591+49	OUTSIDE	6.0	12.0		8.0			8.0		60.0
WESTBOUND										
340+50	MIDDLE	80.0	12.0				106.7	106.7	39	208.0
347+00	MIDDLE	20.0	12.0				26.7	26.7	9	88.0
349+09	OUTSIDE	6.0	12.0		8.0			8.0		60.0
349+09	MIDDLE	6.0	12.0		8.0			8.0		60.0
349+09	MIDDLE	6.0	12.0		8.0			8.0		60.0
349+09	INSIDE	6.0	12.0		8.0			8.0		60.0
370+12	MIDDLE	40.0	12.0				53.3	53.3	19	128.0
385+96	INSIDE	12.0	12.0			16.0		16.0		72.0
438+76	INSIDE	30.0	12.0				40.0	40.0	14	108.0
454+60	MIDDLE	6.0	12.0		8.0			8.0		60.0
589+97	INSIDE	6.0	12.0		8.0			8.0		60.0
589+97	MIDDLE	6.0	12.0		8.0			8.0		60.0
589+97	OUTSIDE	6.0	12.0		8.0			8.0		60.0
SUBTOTAL:					104.0	16.0	226.7	346.7	81.0	1384.0
ANTICIPATED FAILURES (20%):					20.8	3.2	45.3	69.3	16.2	276.8
TOTAL:					124.8	19.2	272.0	416.0	97	1660.8

LOCATION (see note)	LONGITUDINAL PARTIAL DEPTH REMOVAL, 2"	LONGITUDINAL PARTIAL DEPTH REMOVAL, 3"	LONGITUDINAL PARTIAL DEPTH PATCHING
	FEET	FEET	TON
EASTBOUND			
EOP	1675	700	67.8
LANE 1-2			
LANE 2-3			
EOP			
WESTBOUND			
EOP	27984	19096	1409.4
LANE 1-2			
LANE 2-3			
EOP			
SUBTOTAL:	29659	19796	1477.2
ANTICIPATED FAILURES (20%):	5931.8	3959.2	295.4
TOTAL:	35590.8	23755.2	1772.7

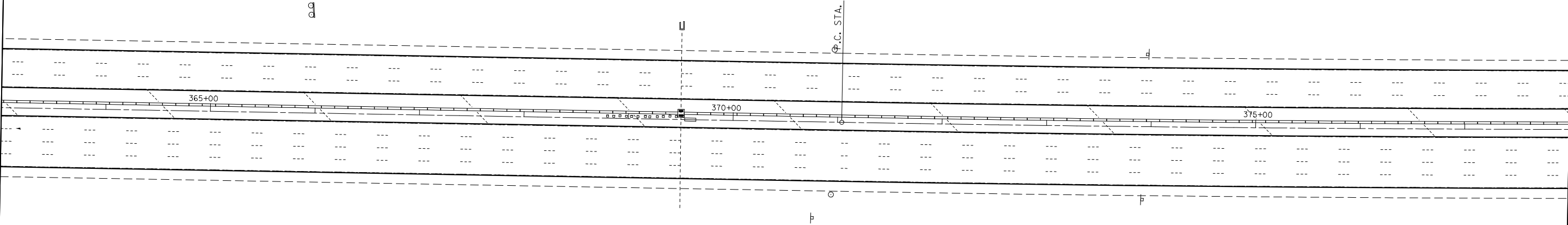
NOTE: THE LOCATION OF ALL LONGITUDINAL PARTIAL DEPTH REMOVAL/PATCHING SHALL BE DETERMINED BY THE ENGINEER.

TEMPORARY RAMPS				
STATION	DIRECTION	LENGTH	WIDTH	AREA
		FT	FT	SQ YD
MAINLINE				
337+00	EB	6.7	37	27.5
337+00	WB	6.7	37	27.5
349+15	EB	6.7	49	36.5
349+15	WB	6.7	49	36.5
351+52	EB	6.7	49	36.5
351+52	WB	6.7	49	36.5
547+36	EB	6.7	37	27.5
547+36	WB	6.7	37	27.5
RAMPS				
526+00	EB	6.7	16.5	12.3
526+67	WB	6.7	16.5	12.3
535+00	EB	6.7	16.5	12.3
539+45	WB	6.7	16.5	12.3
TOTAL				305.2

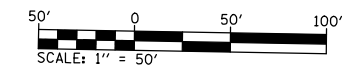


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	PLOT DATE = 3/26/2015	DATE -	REVISED -		FED. ROAD DIST. NO.	ILLINOIS						

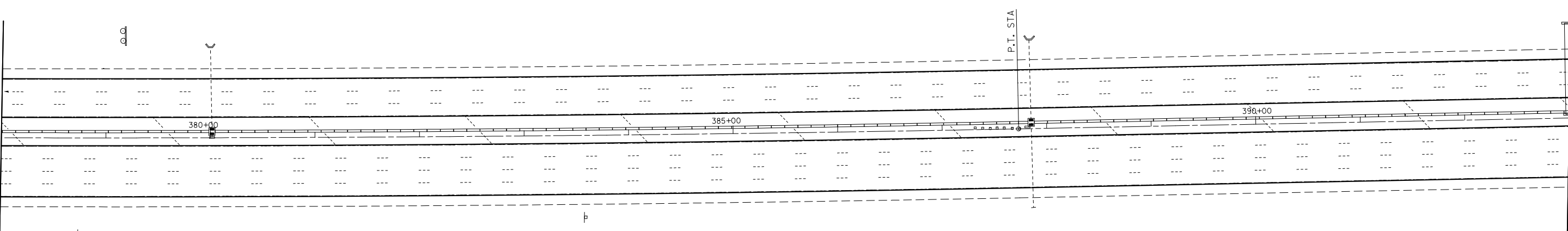
MATCHLINE STA. 363+00



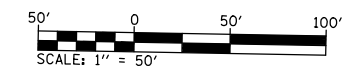
MATCHLINE STA. 378+00



MATCHLINE STA. 378+00



MATCHLINE STA. 393+00



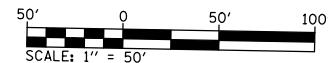
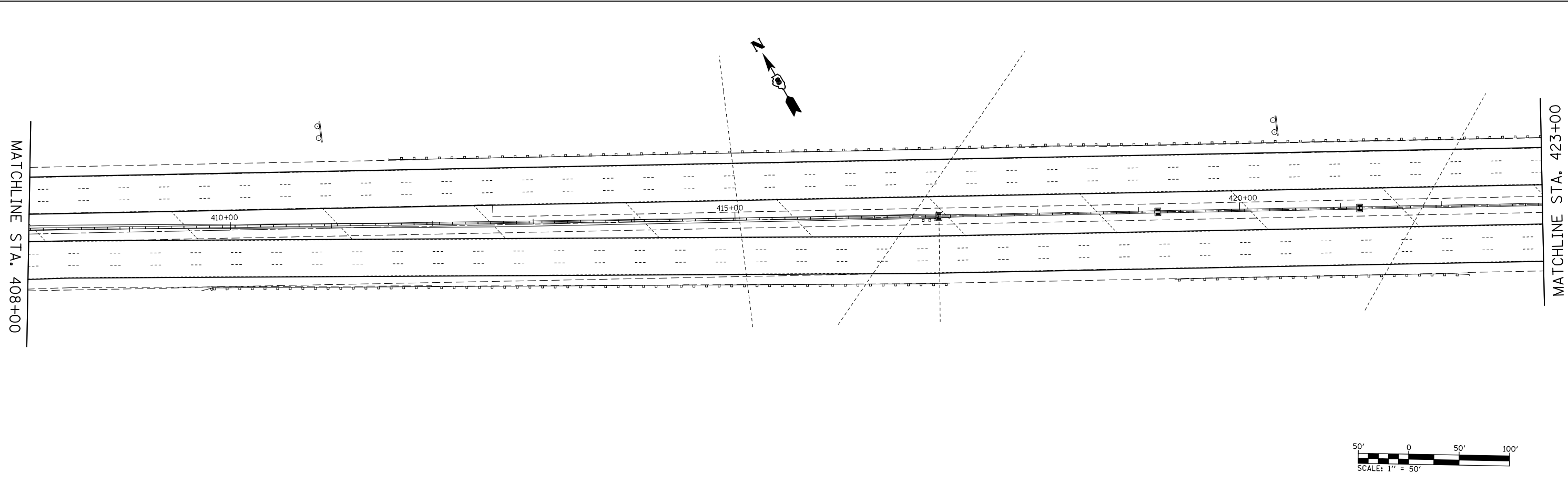
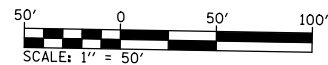
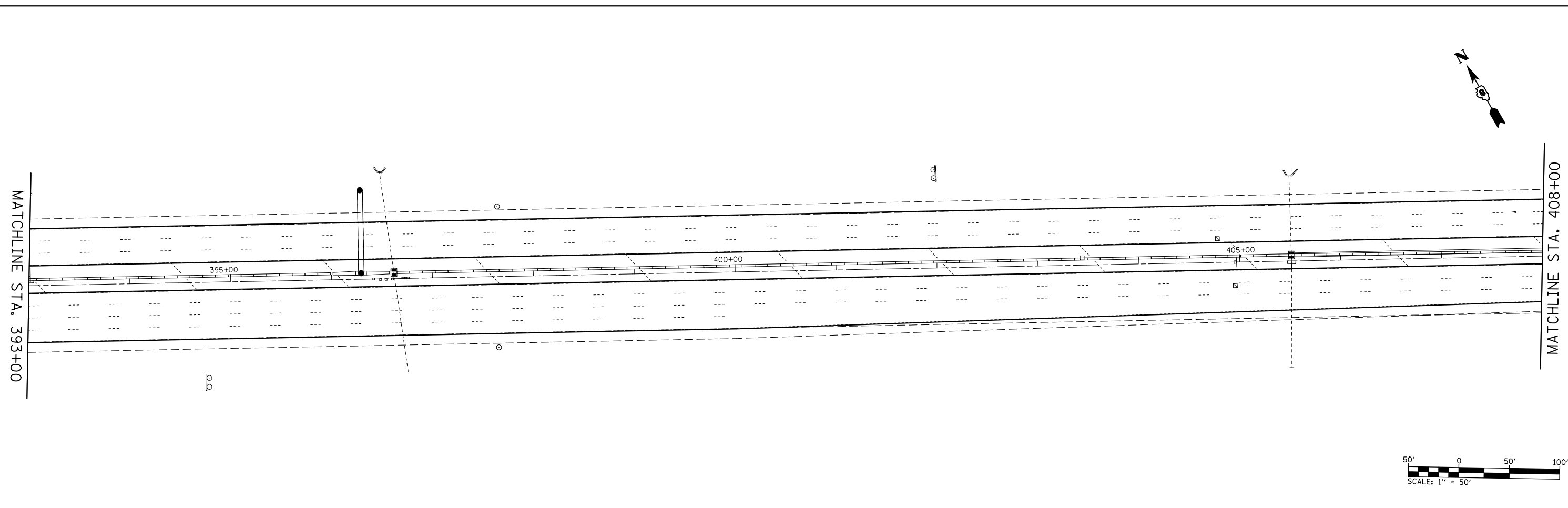
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	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: 1" = 50' SHEET NO. 2 OF 14 SHEETS STA. 363+00 TO STA. 393+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	(82-(4,5,6,7)-RS-1	ST. CLAIR	63	34
FED. ROAD DIST. NO. ILLINOIS			CONTRACT NO. 76H71	



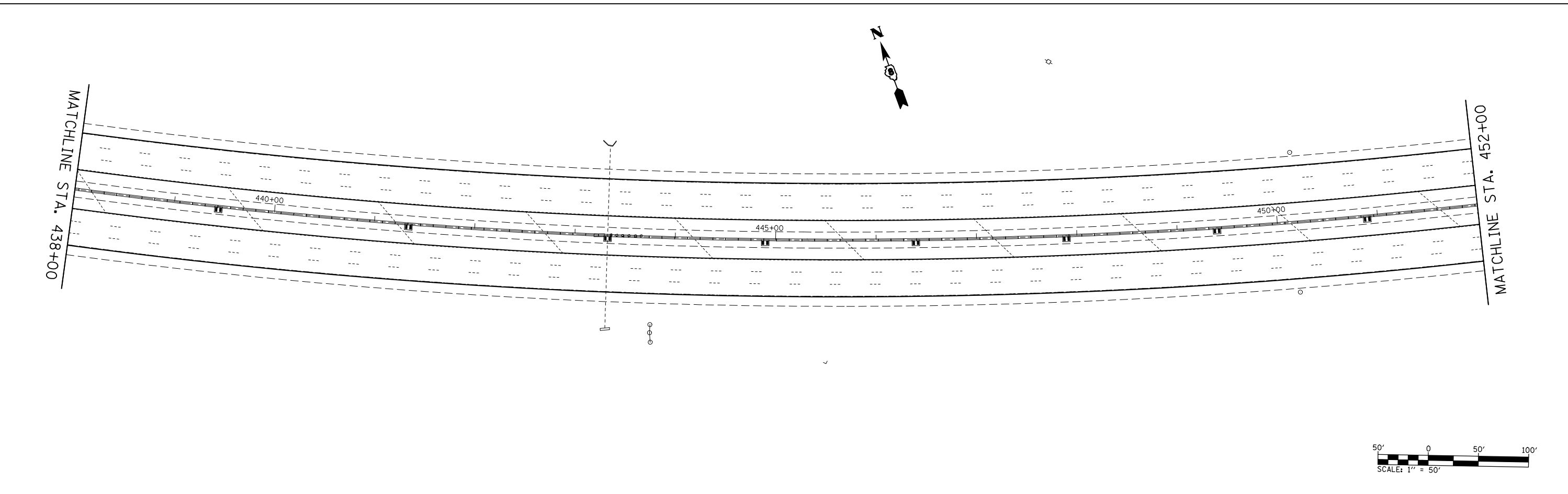
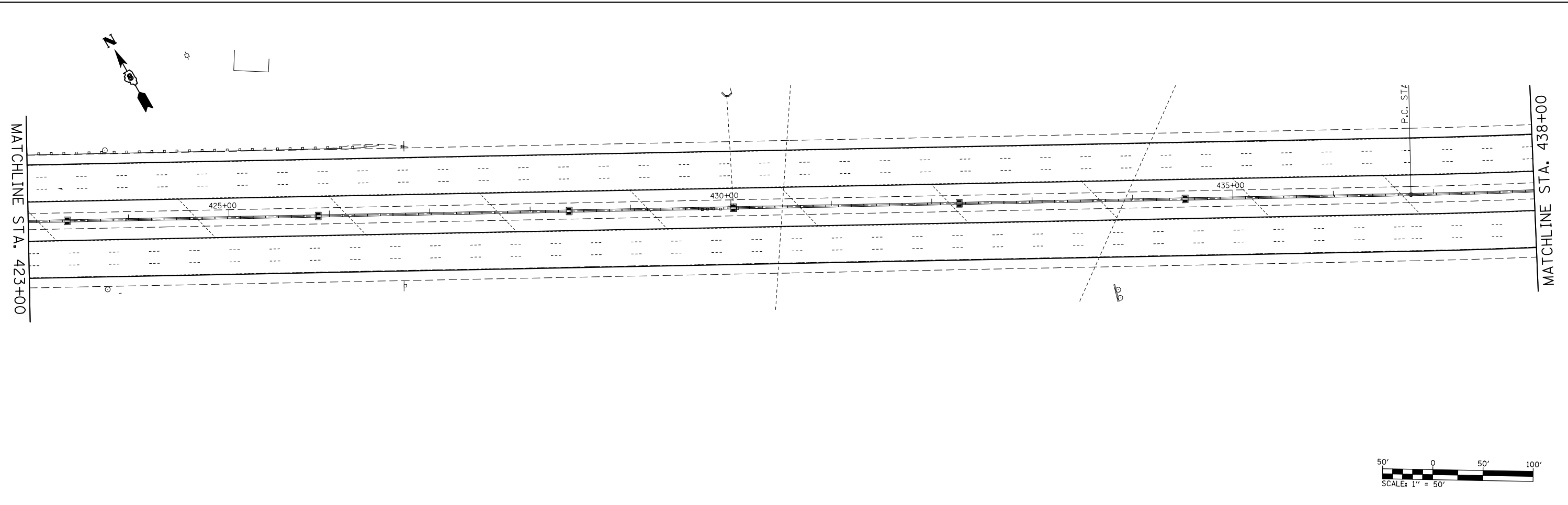
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	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: 1" = 50' SHEET NO. 3 OF 14 SHEETS STA. 393+00 TO STA. 423+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	35
FED. ROAD DIST. NO. ILLINOIS			CONTRACT NO. 76H71	



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	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: 1" = 50' SHEET NO. 4 OF 14 SHEETS STA. 423+00 TO STA. 452+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	36
FED. ROAD DIST. NO. ILLINOIS			CONTRACT NO. 76H71	

MATCHLINE STA. 452+00



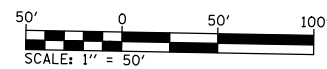
455+00

460+00

465+00

P.T. STA. 465+23.83

MATCHLINE STA. 467+00



MATCHLINE STA. 467+00



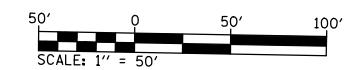
470+00

475+00

480+00

P.C. STA. 478+70.52

MATCHLINE STA. 482+00



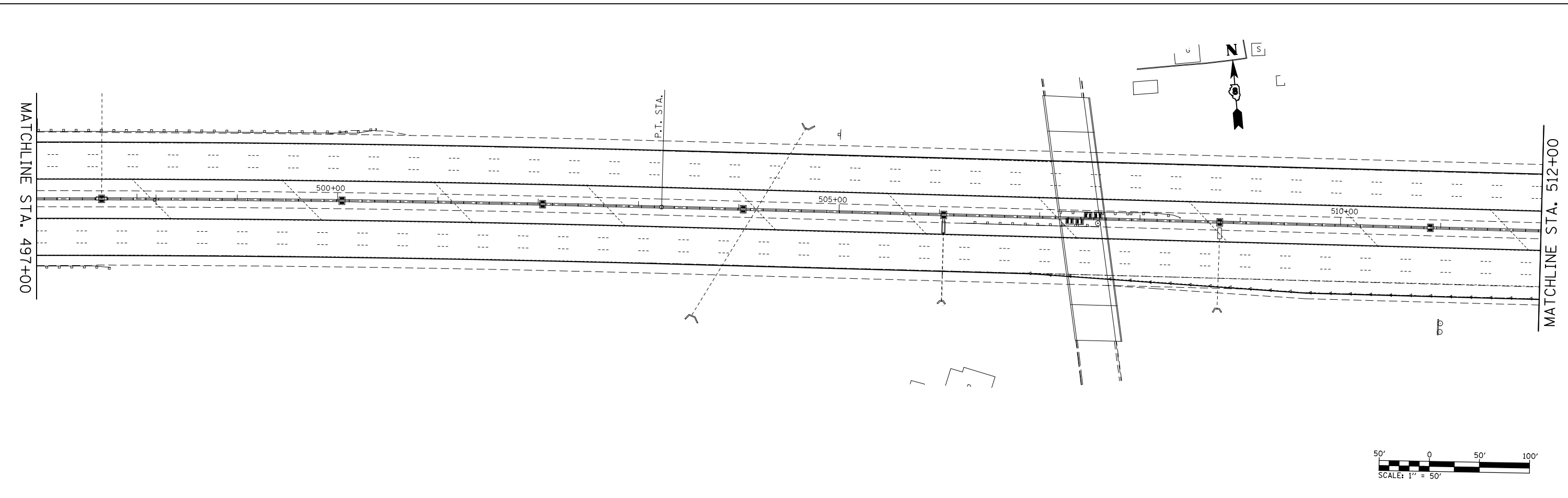
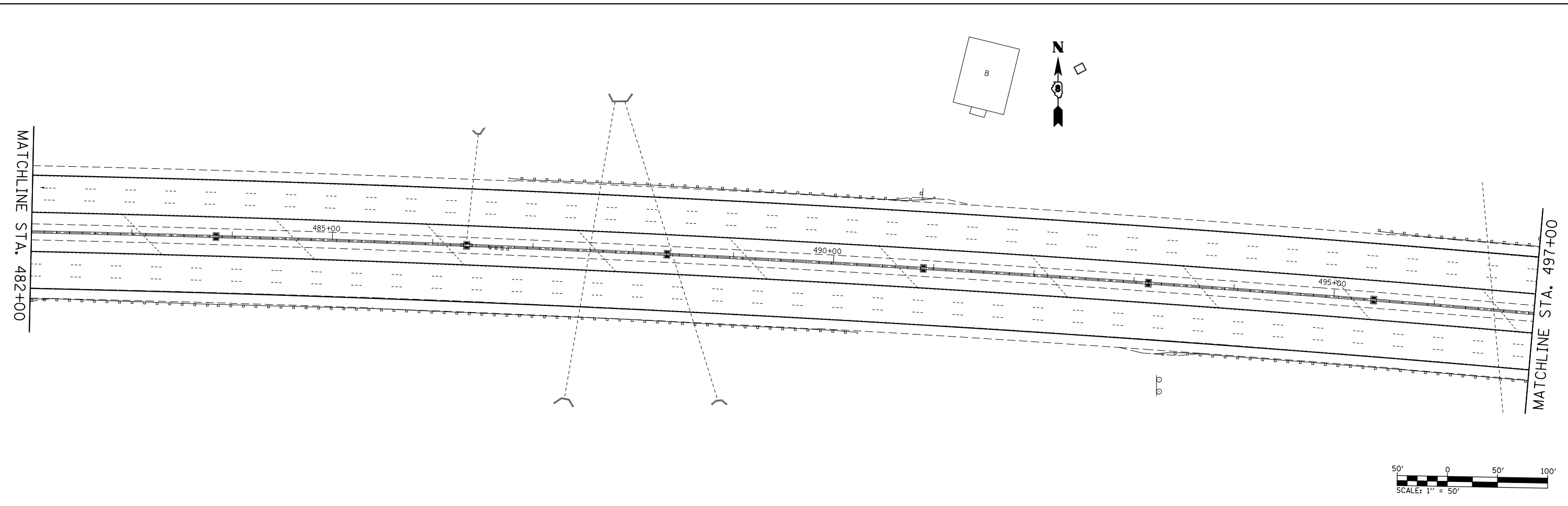
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	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: 1" = 50' SHEET NO. 5 OF 14 SHEETS STA. 452+00 TO STA. 482+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	37
CONTRACT NO. 76H71				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



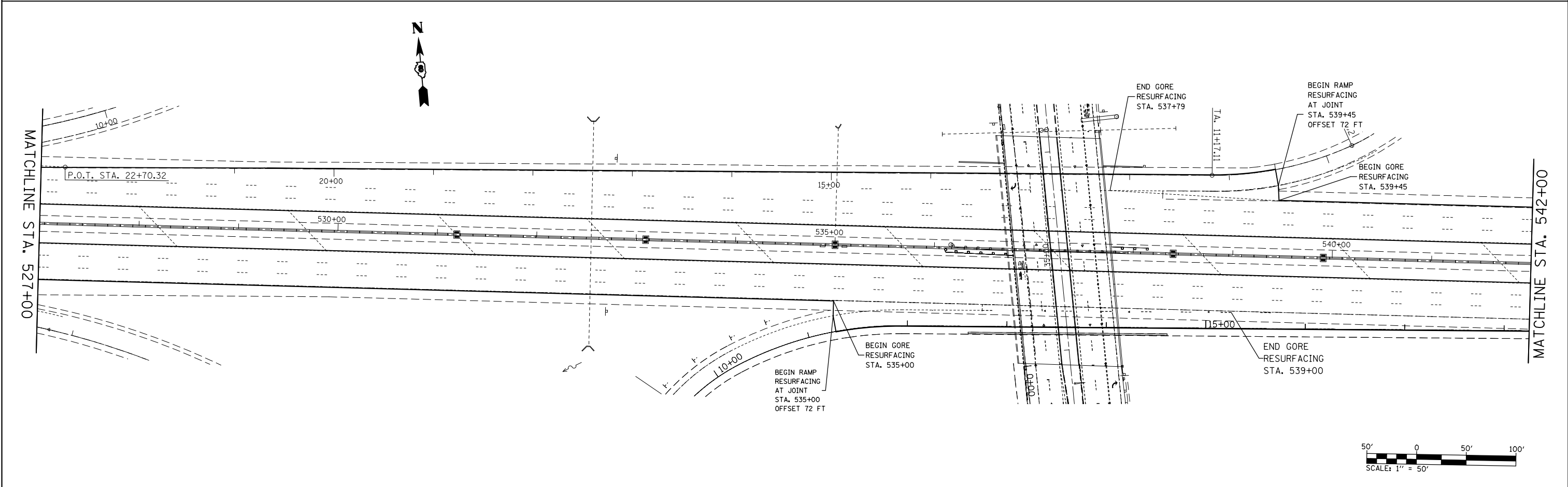
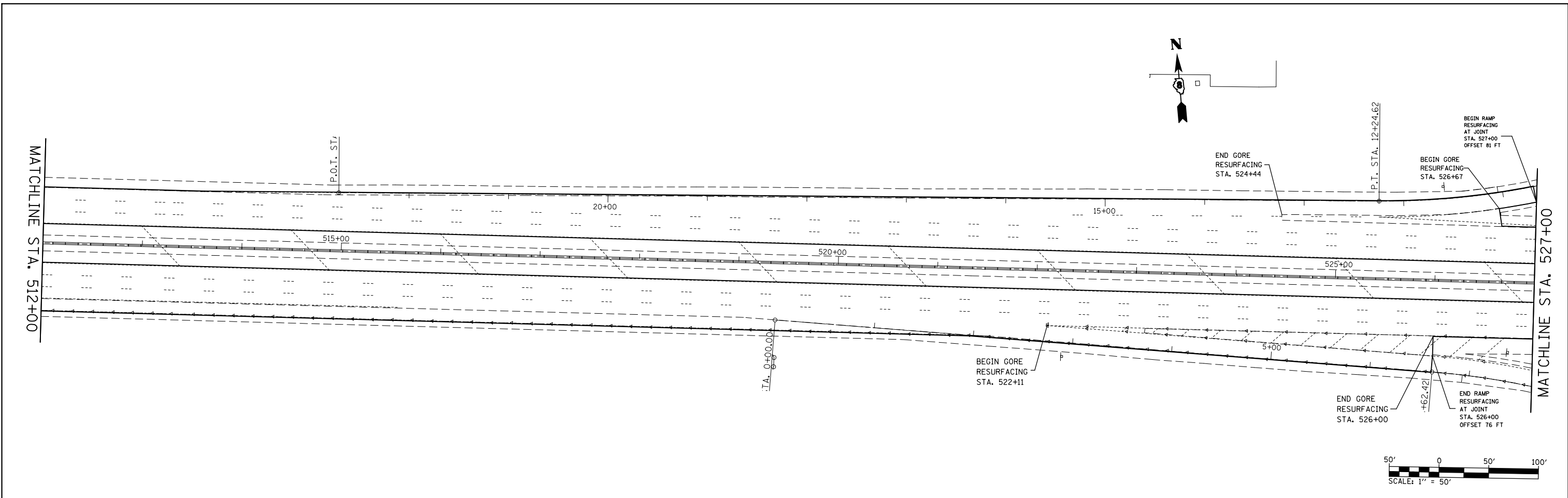
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: 1" = 50' SHEET NO. 6 OF 14 SHEETS STA. 482+00 TO STA. 512+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	38
CONTRACT NO. 76H71				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



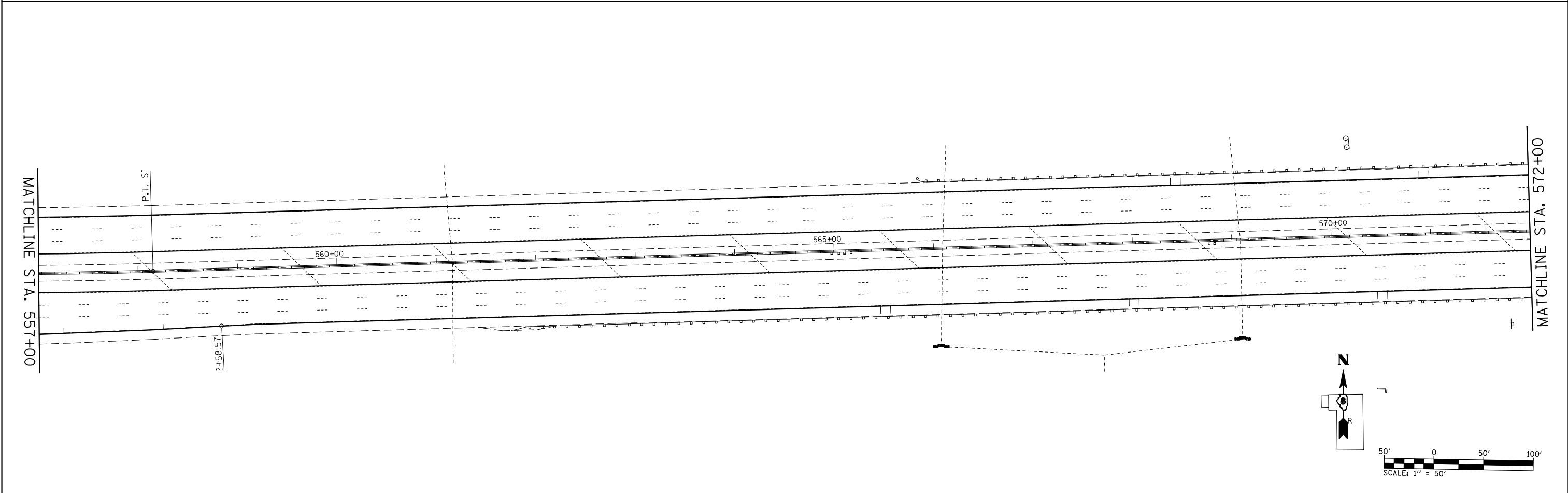
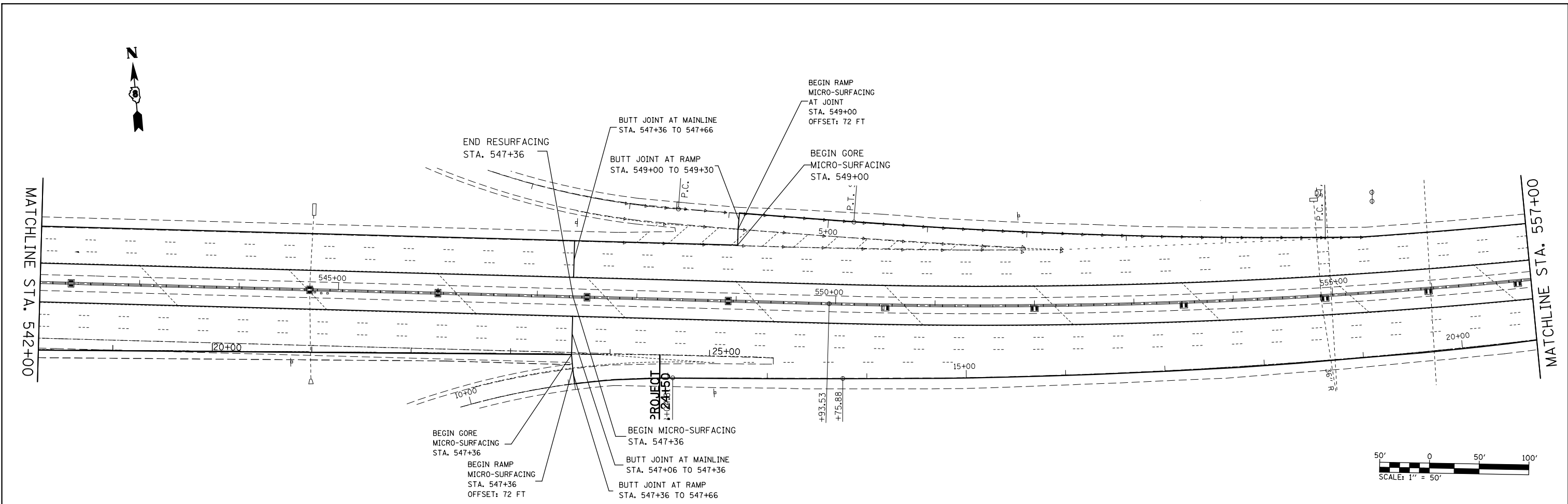
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

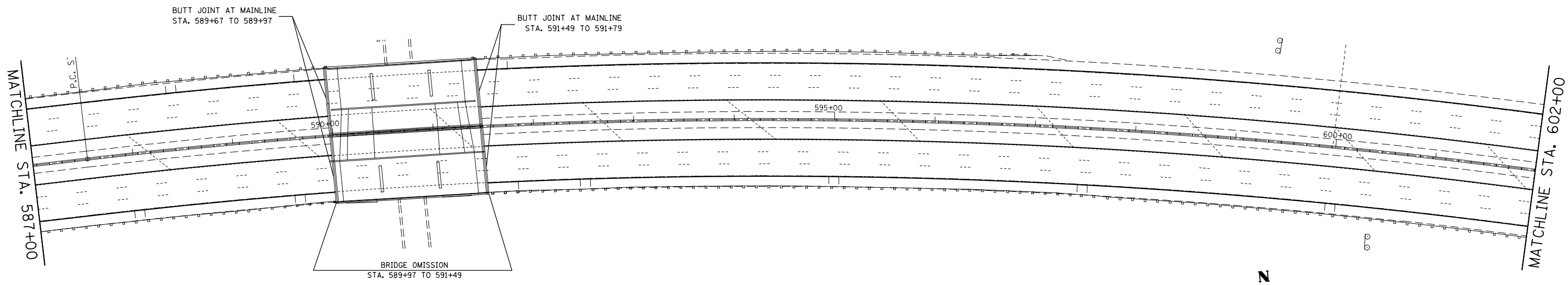
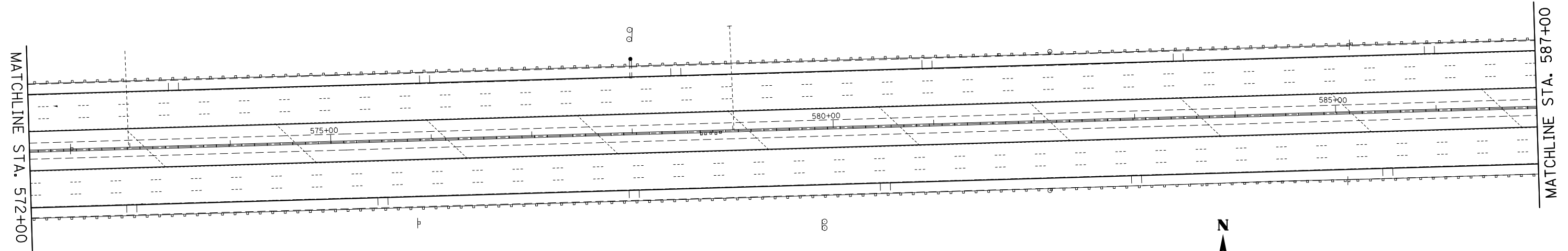
PLAN SHEET

SCALE: 1" = 50' SHEET NO. 7 OF 14 SHEETS STA. 512+00 TO STA. 542+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	39
CONTRACT NO. 76H71				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = durosier.j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN SHEET			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\pwork\durosier.j\d0419849\d876h71-sht-plan.dgn		DRAWN -	REVISED -		64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	40			
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -		CONTRACT NO. 76H71							
PLOT DATE = 3/26/2015		DATE -	REVISED -		FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT						
				SCALE: 1" = 50'	SHEET NO. 8 OF 14 SHEETS	STA. 542+00 TO STA. 572+00						



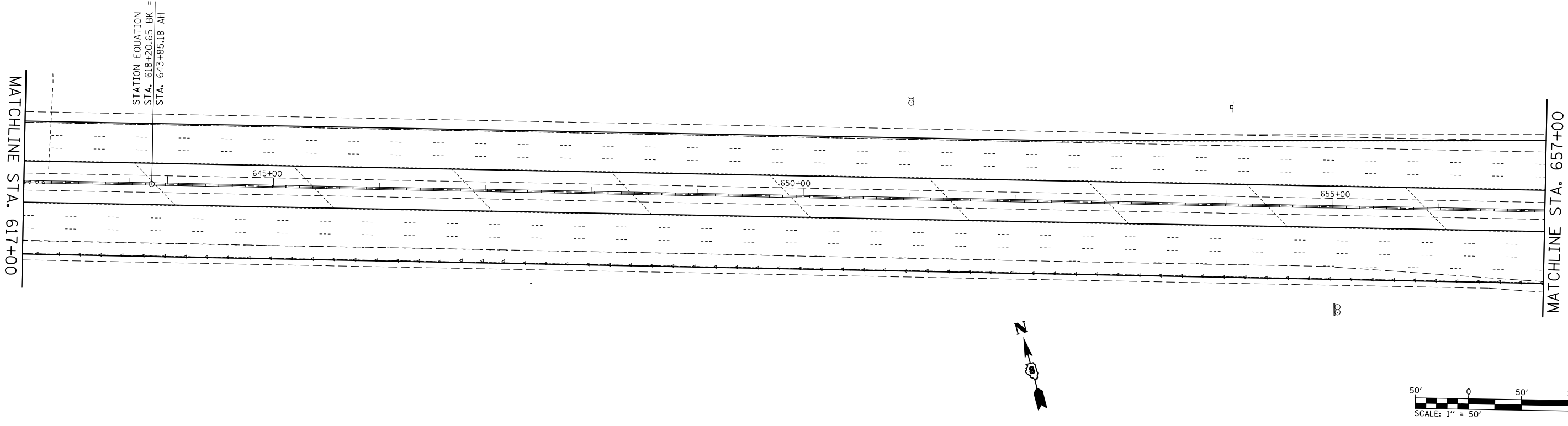
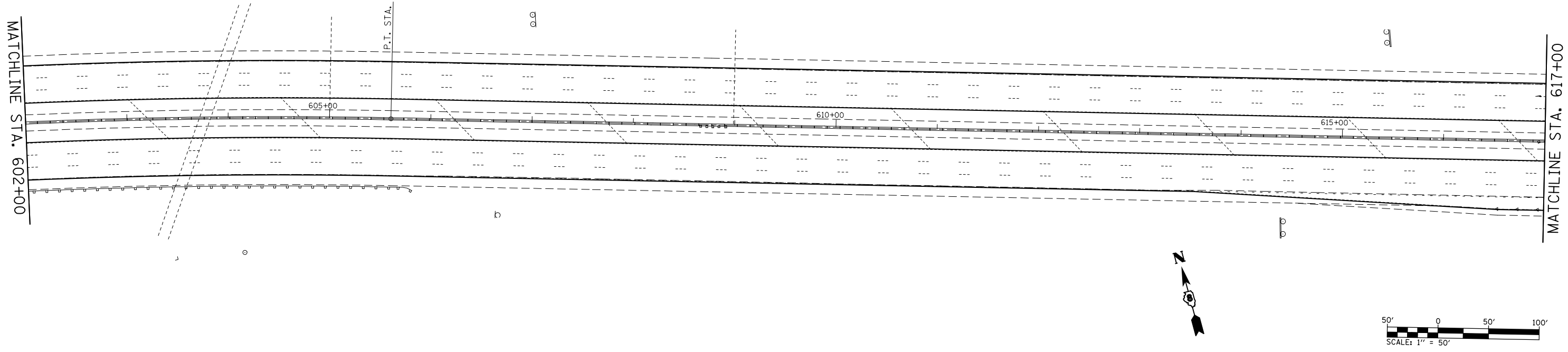
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et:\pw\work\p\dot\durosier.j\d0419849\d876h71-sht-plan.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: 1" = 50' SHEET NO. 9 OF 14 SHEETS STA. 572+00 TO STA. 602+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	41
CONTRACT NO. 76H71				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



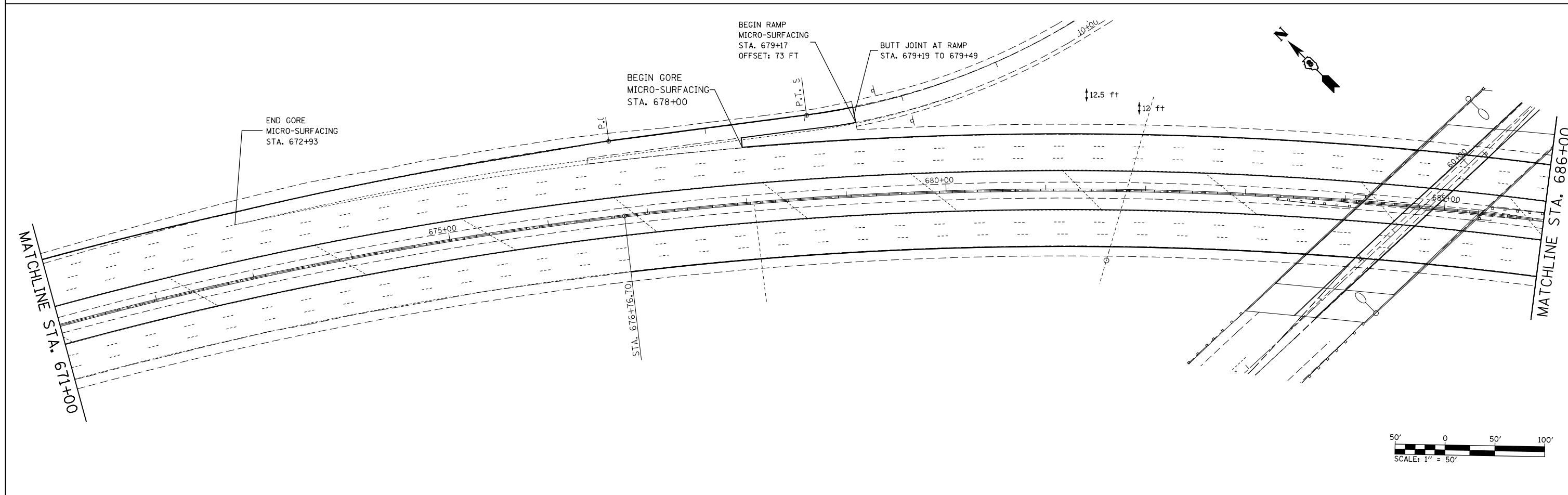
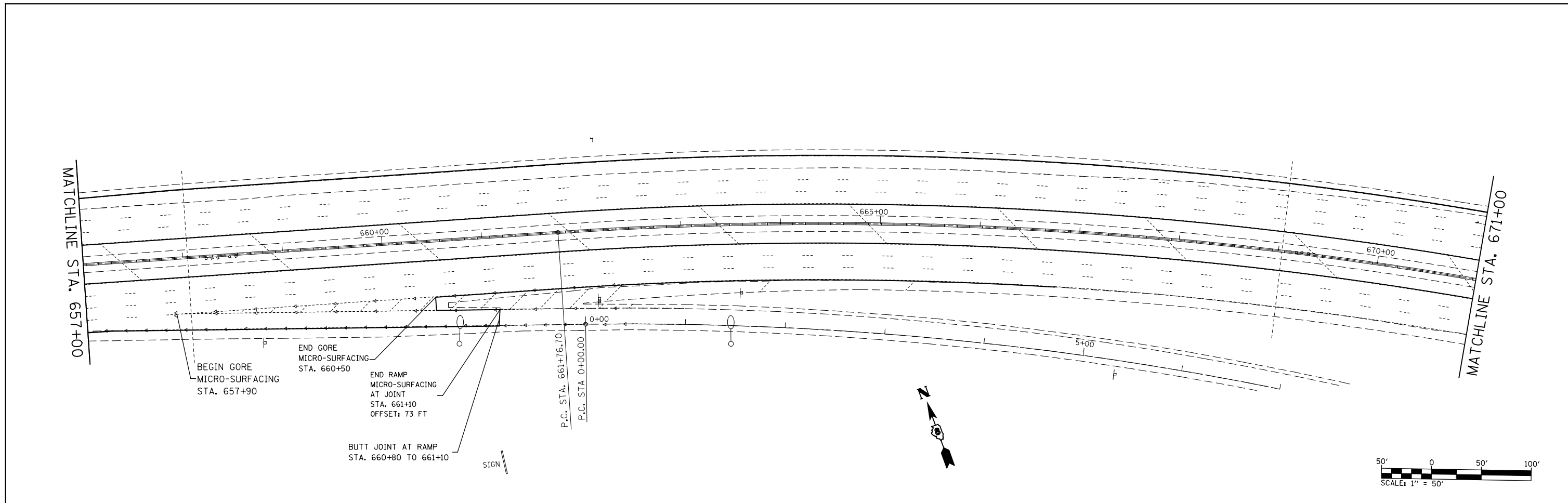
FILE NAME =	USER NAME = durosier.j	DESIGNED -	REVISED -
et:\pw\work\p\dot\durosier.j\d0419849\d876h71-sht-plan.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

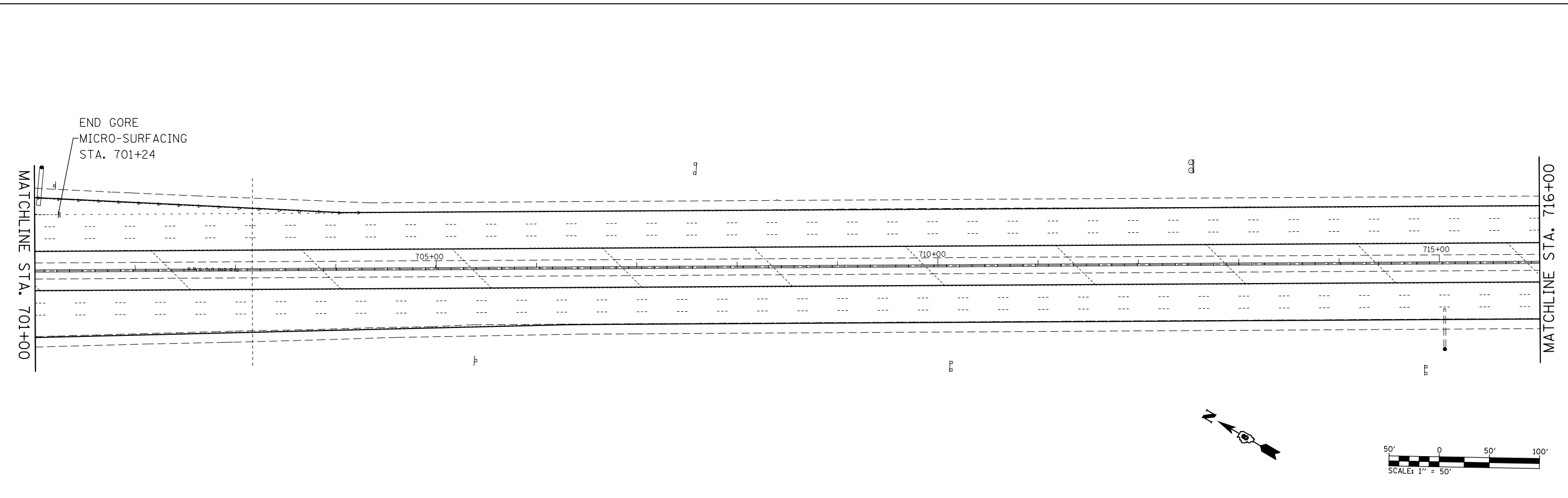
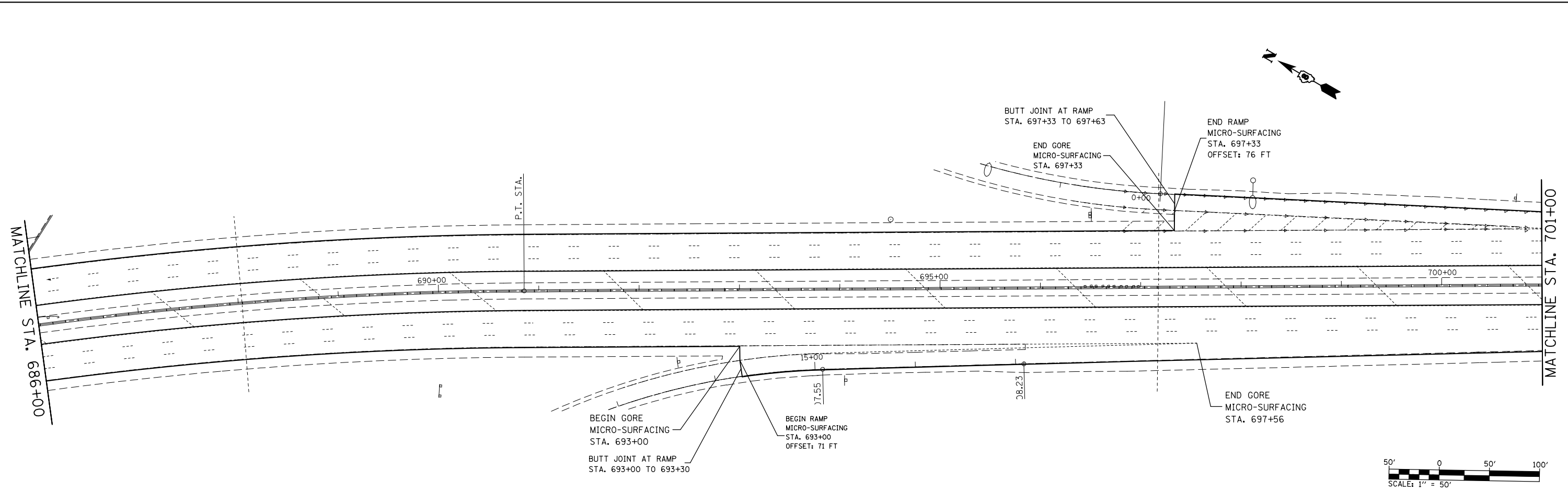
PLAN SHEET

SCALE: 1" = 50' SHEET NO. 10 OF 14 SHEETS STA. 602+00 TO STA. 657+00

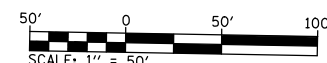
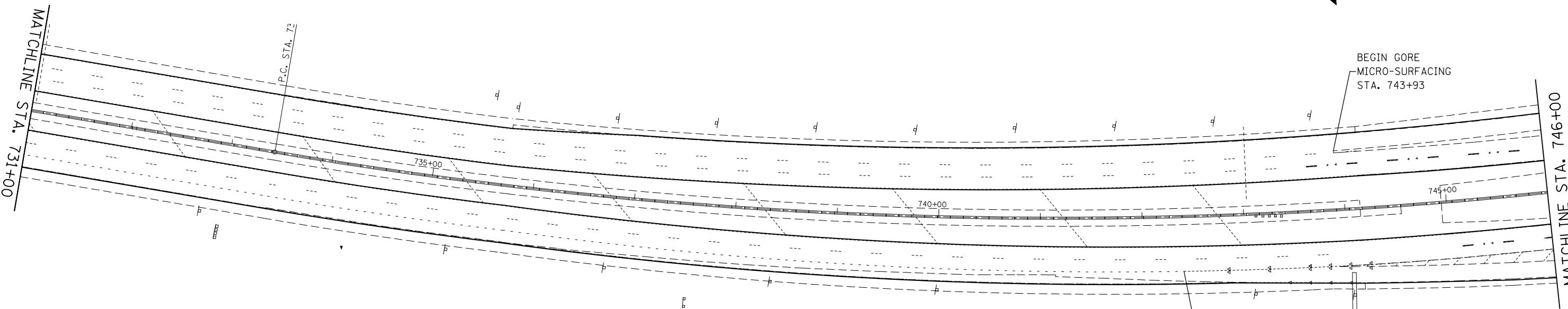
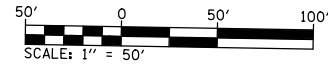
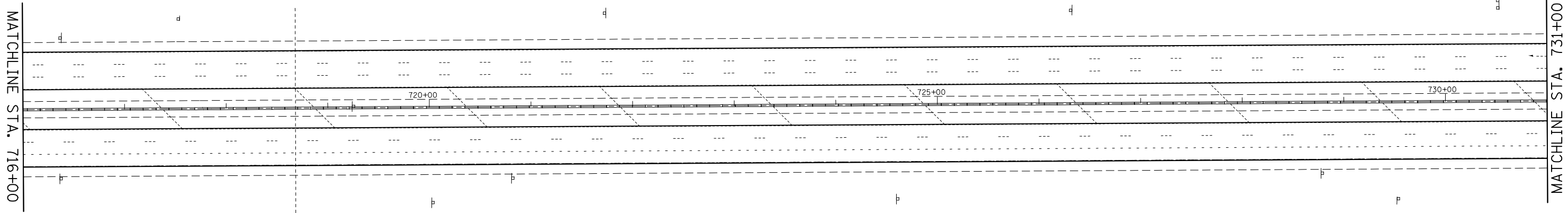
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	42
CONTRACT NO. 76H71				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = durosier-j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN SHEET			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\midot\durosier-j\d0419849\d876h71-sht-plan.dgn		DRAWN -	REVISED -		64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	43			
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -		CONTRACT NO. 76H71							
	PLOT DATE = 3/26/2015	DATE -	REVISED -		FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT						
				SCALE: 1" = 50'	SHEET NO. 11 OF 14 SHEETS	STA. 657+00 TO STA. 686+00						



FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN SHEET			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\durosier_j\0419849\d876h71-sht-plan.dgn		DRAWN -	REVISED -		64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	44			
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -		CONTRACT NO. 76H71							
PLOT DATE = 3/26/2015		DATE -	REVISED -		FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT					
				SCALE: 1" = 50'	SHEET NO. 12 OF 14 SHEETS	STA. 686+00 TO STA. 716+00						



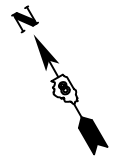
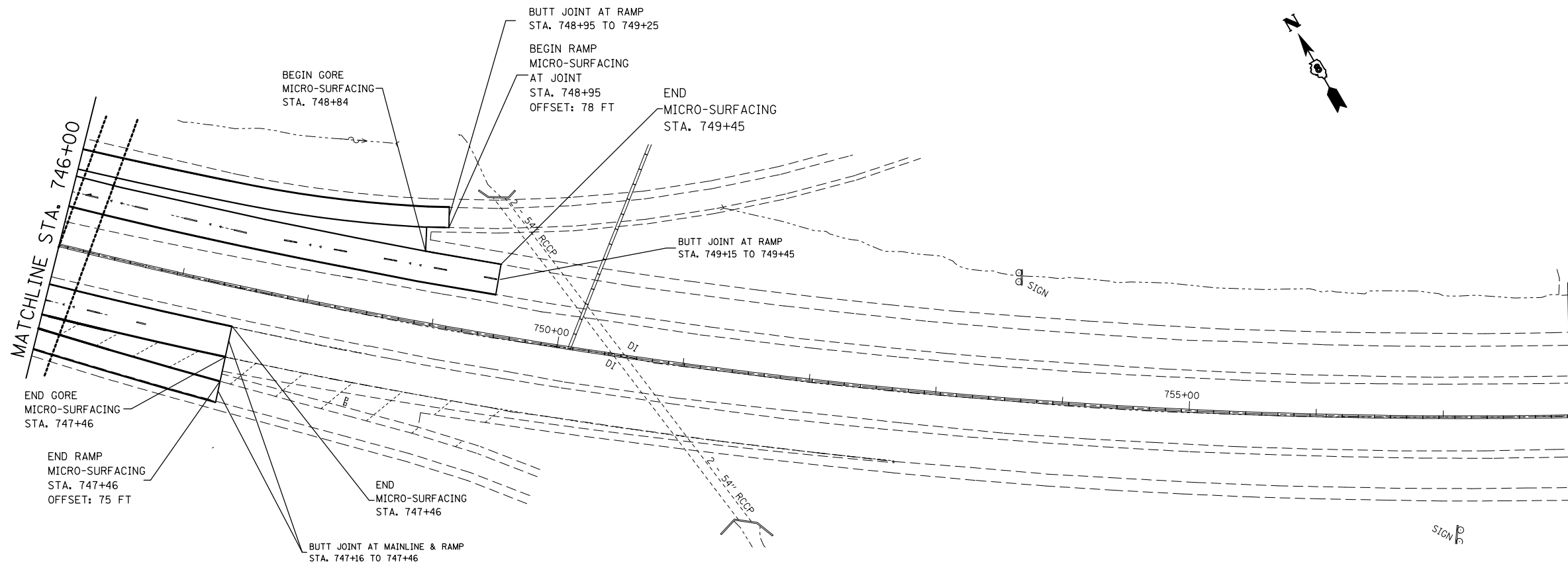
FILE NAME =	USER NAME = durosier.j	DESIGNED -	REVISED -
c:\pwork\pwork\dot\durosier.j\d0419849\d876h71-sht-plan.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: 1" = 50' SHEET NO. 13 OF 14 SHEETS STA. 716+00 TO STA. 746+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	45
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76H71	



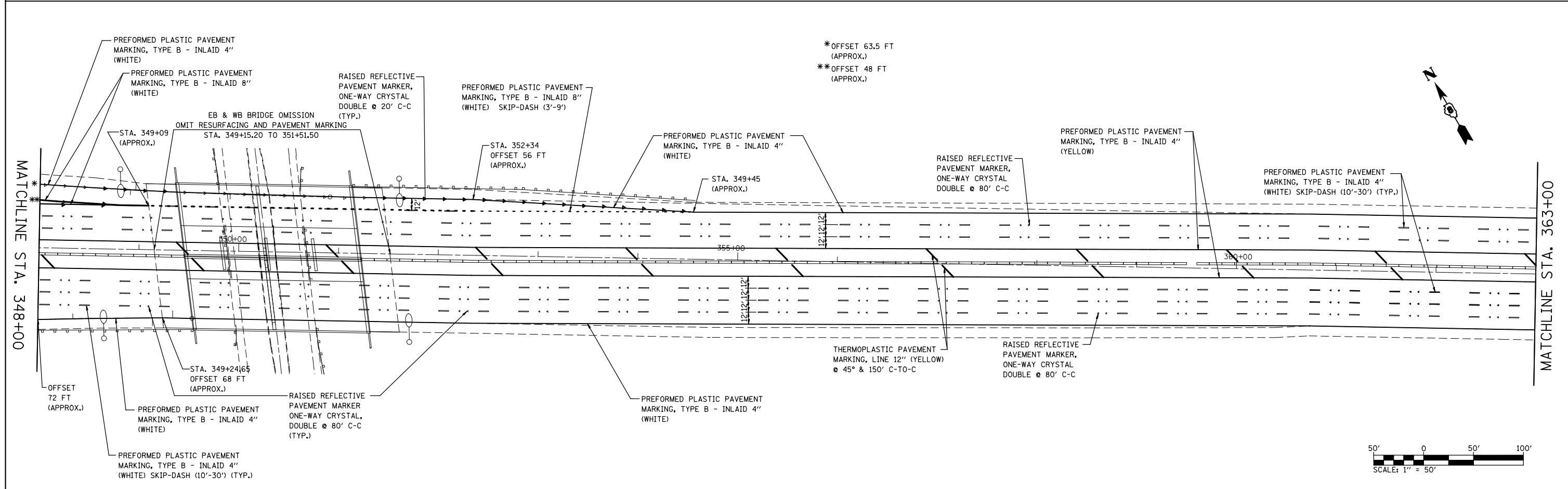
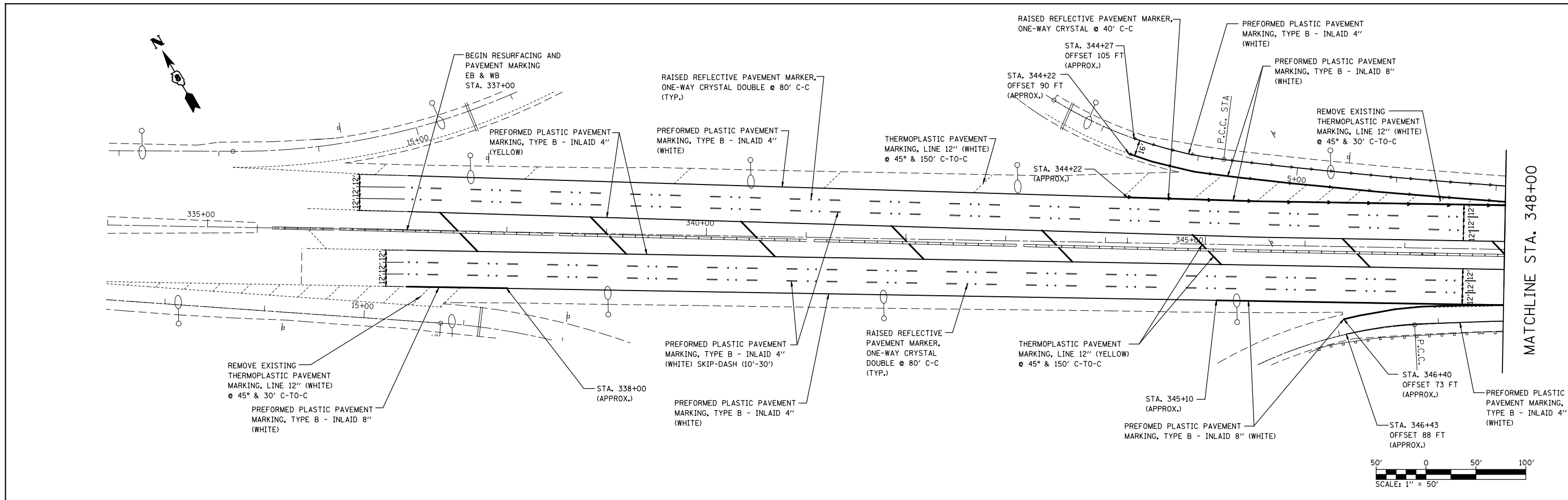
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c:\pw\work\p1dot\durosier.j\d0419849\d876h71-sht-plan.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: 1" = 50' SHEET NO. 13 OF 14 SHEETS STA. 746+00 TO STA. 749+45

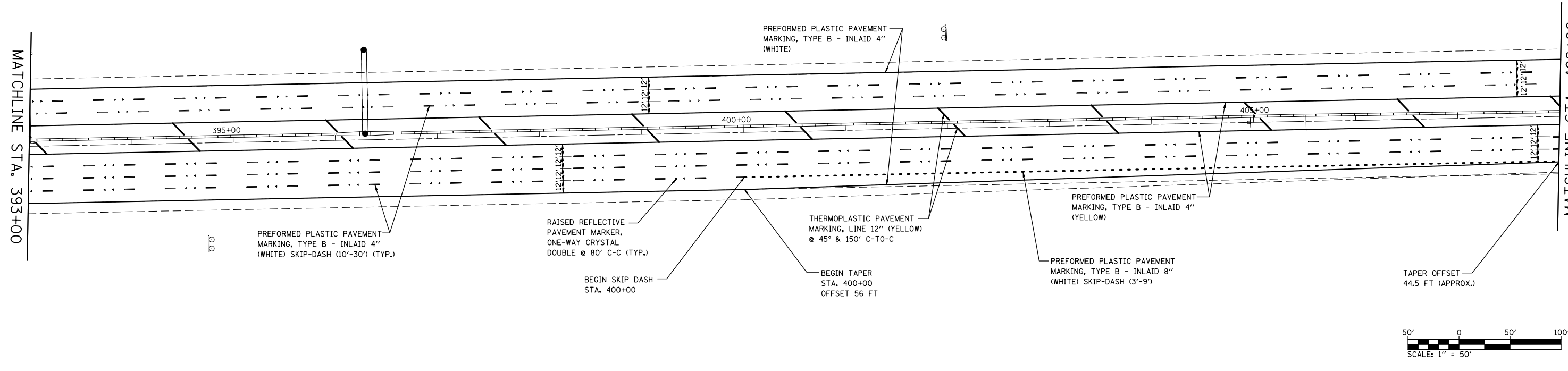
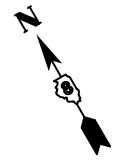
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	46
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76H71	



FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING SHEET			F.A.I. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\midot\durosier_j\d0419849\d876h71-sht-pmk.dgn		DRAWN -	REVISED -		64	82-(4,5,6,7)RS-1	ST. CLAIR	63	47			
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -		CONTRACT NO. 76H71							
	PLOT DATE = 3/26/2015	DATE -	REVISED -		FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT					

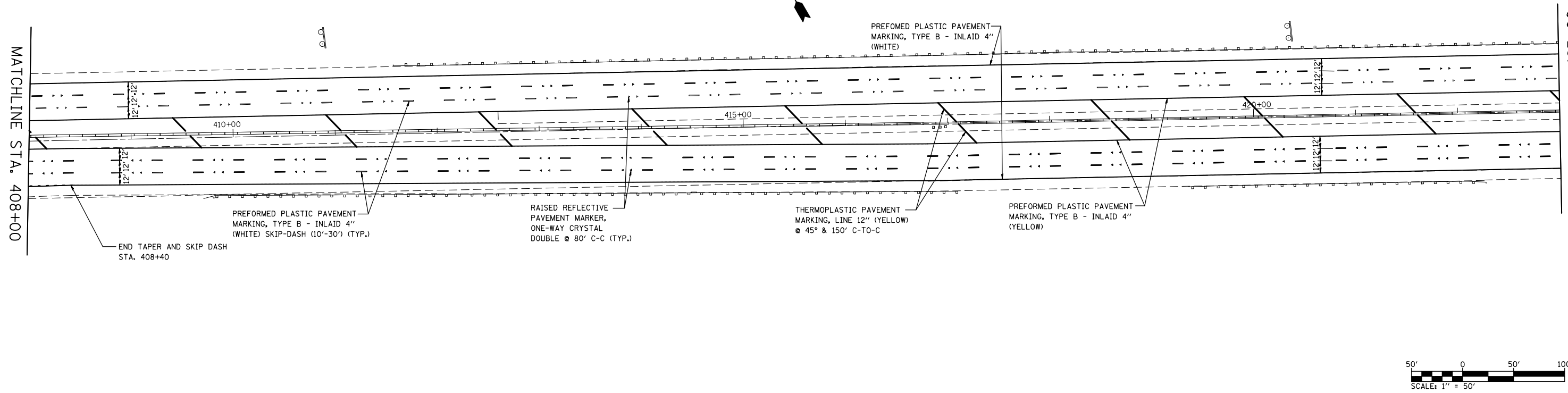
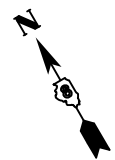
MATCHLINE STA. 393+00

MATCHLINE STA. 408+00



MATCHLINE STA. 408+00

MATCHLINE STA. 423+00



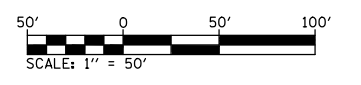
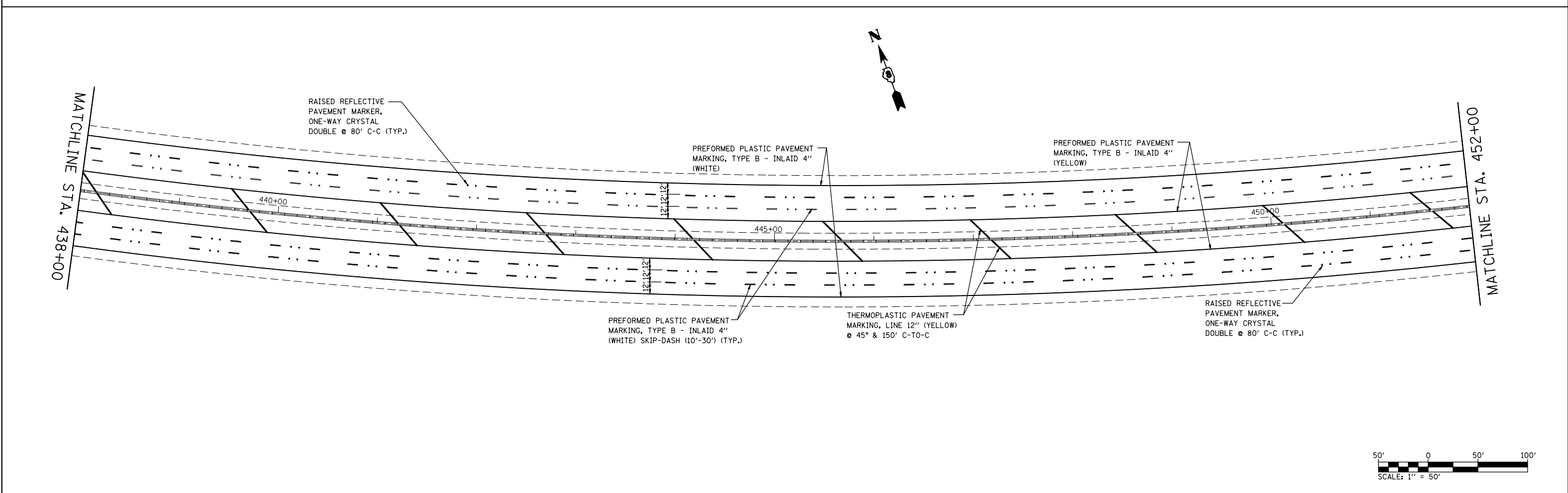
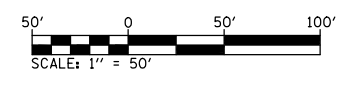
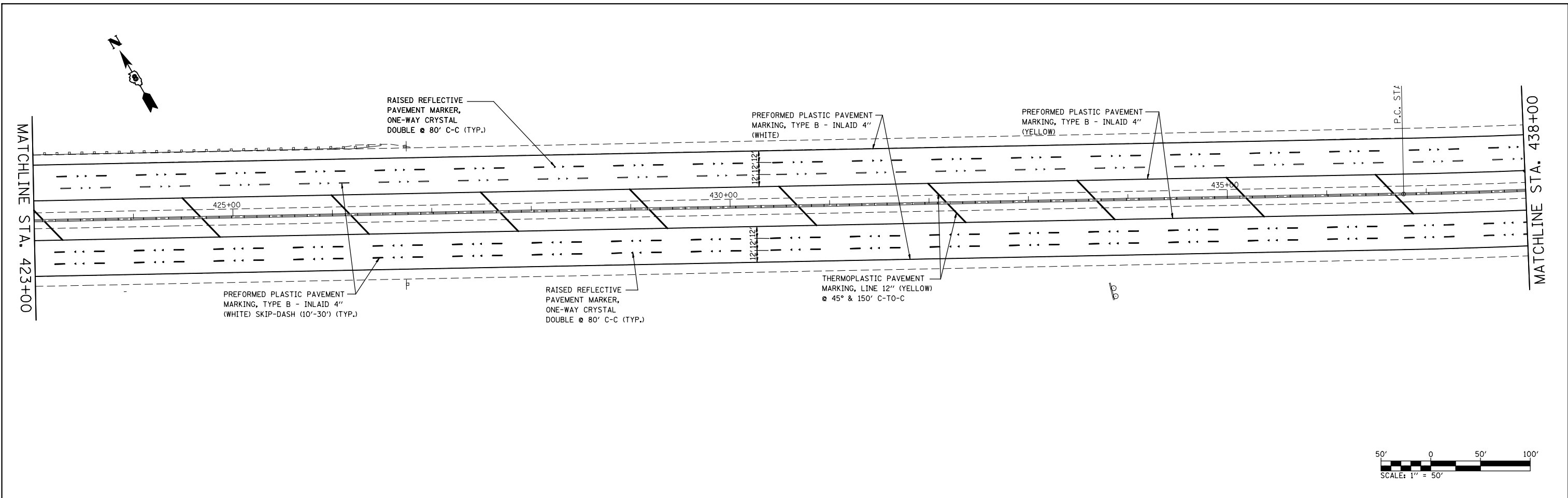
FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -
et:\pw\work\p\dot\durosier_j\d0419849\d876h71-sht-pmk.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

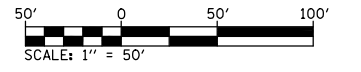
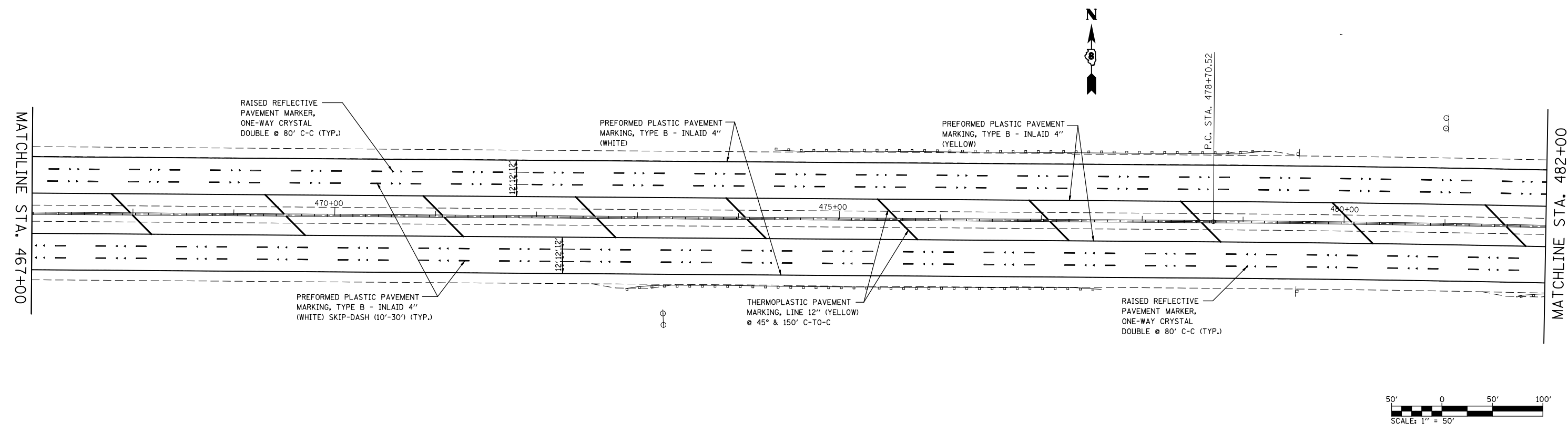
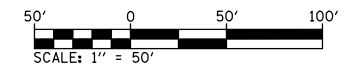
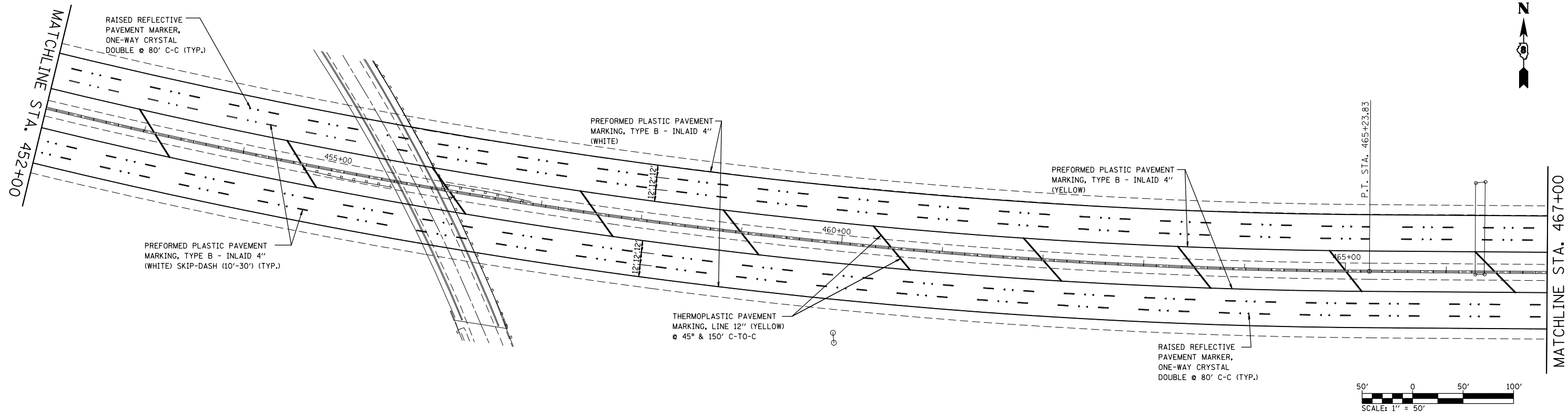
PAVEMENT MARKING SHEET

SCALE: 1" = 50' SHEET NO. 3 OF 14 SHEETS STA. 393+00 TO STA. 423+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)RS-1	ST. CLAIR	63	49
CONTRACT NO. 76H71				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING SHEET			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\dot\durosier_j\d0419849\d876h71-sht-pmk.dgn		DRAWN -	REVISED -		64	82-(4,5,6,7)RS-1	ST. CLAIR	63	50			
PLOT SCALE = 100.0000' / 1in.		CHECKED -	REVISED -		CONTRACT NO. 76H71							
PLOT DATE = 3/26/2015		DATE -	REVISED -		FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT						
				SCALE: 1" = 50'	SHEET NO. 4 OF 14 SHEETS	STA. 423+00 TO STA. 452+00						



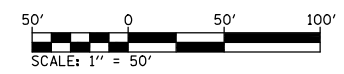
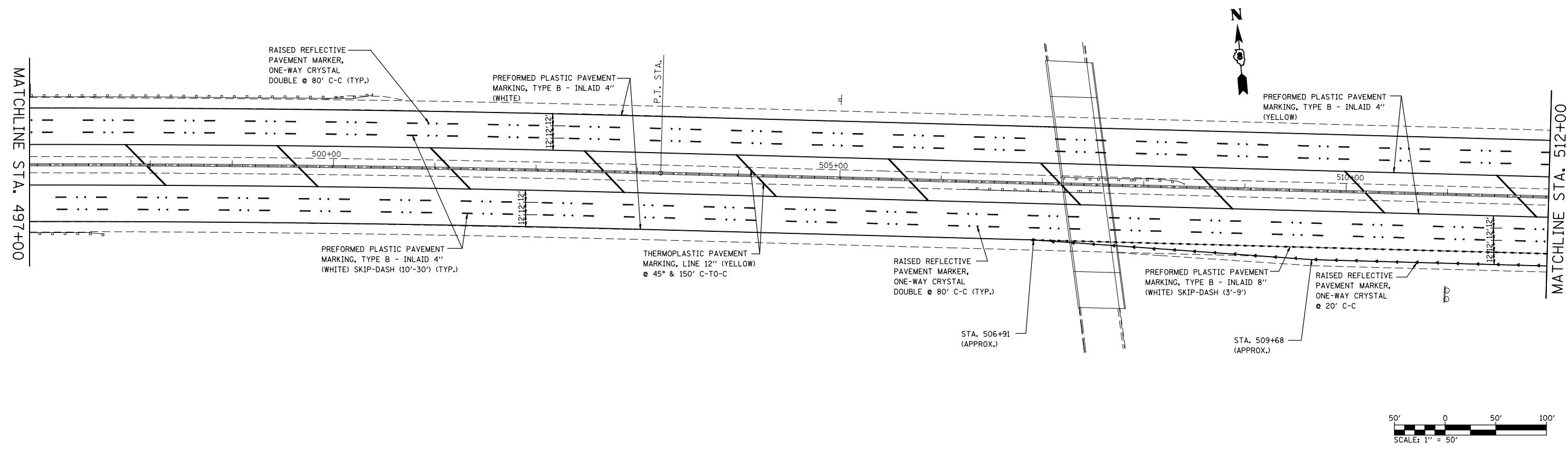
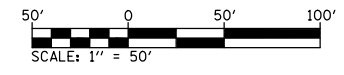
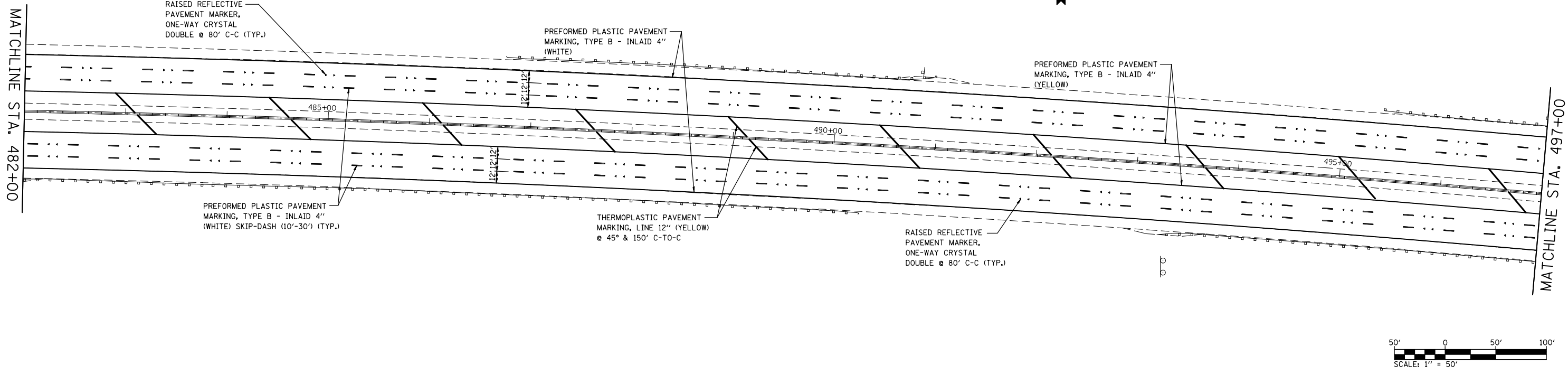
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et:\pwork\pwork\dot\durosier.j\d0419849\d876h71-sht-pmk.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING SHEET

SCALE: 1" = 50' SHEET NO. 5 OF 14 SHEETS STA. 452+00 TO STA. 482+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)RS-1	ST. CLAIR	63	51
CONTRACT NO. 76H71				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



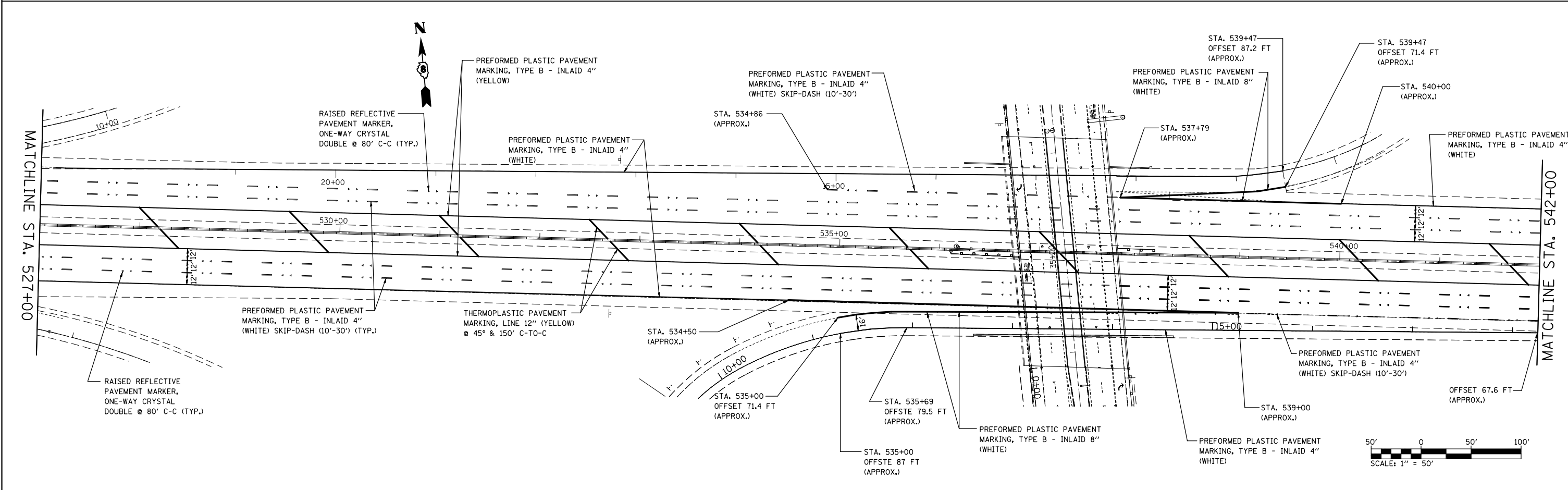
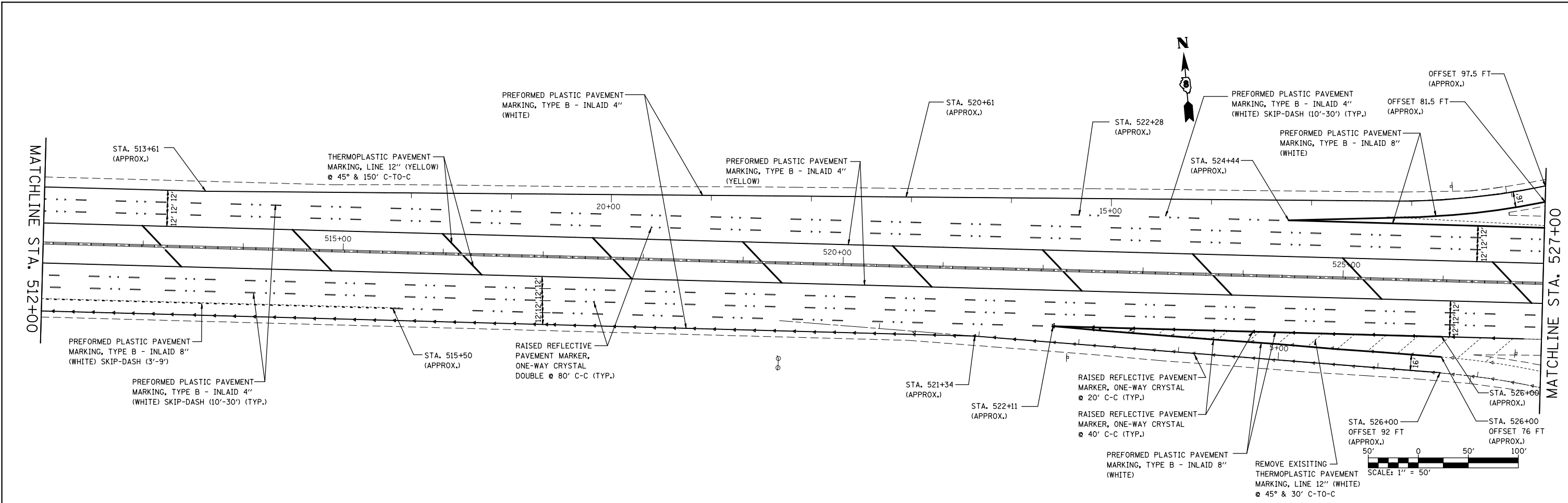
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

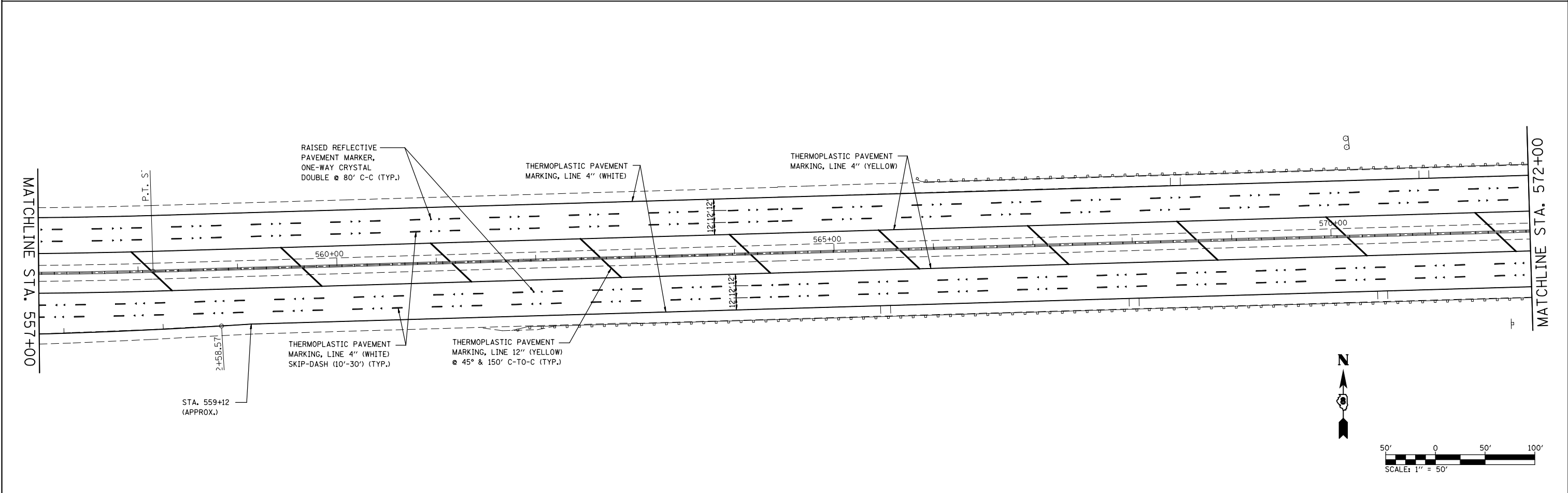
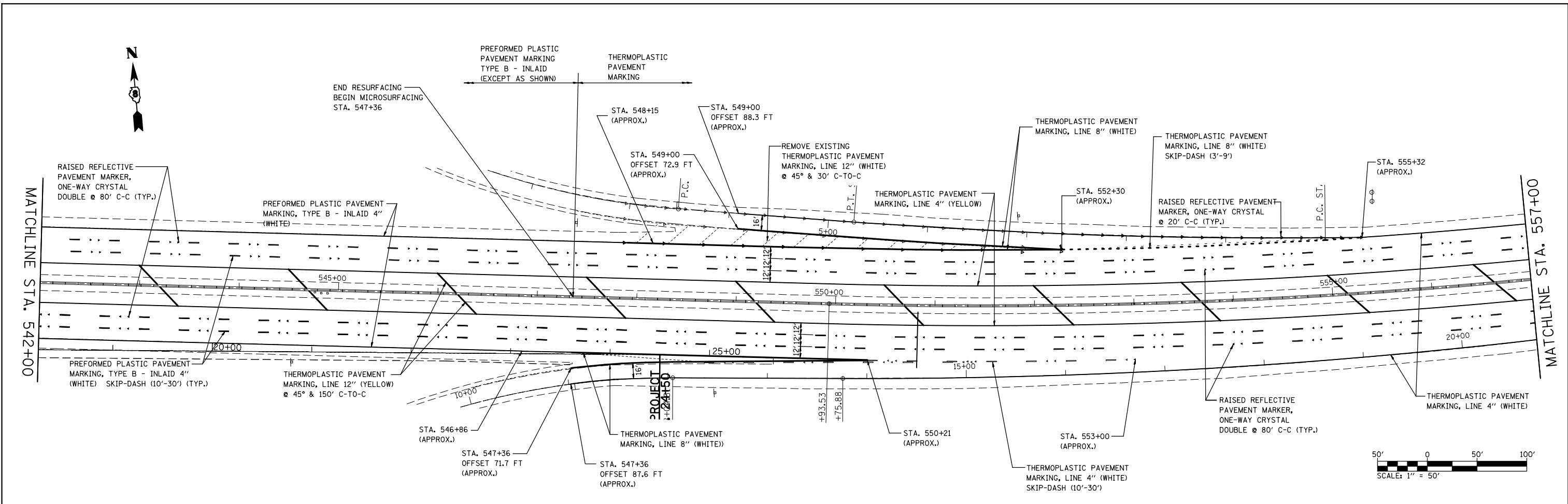
PAVEMENT MARKING SHEET

SCALE: 1" = 50' SHEET NO. 6 OF 14 SHEETS STA. 482+00 TO STA. 512+00

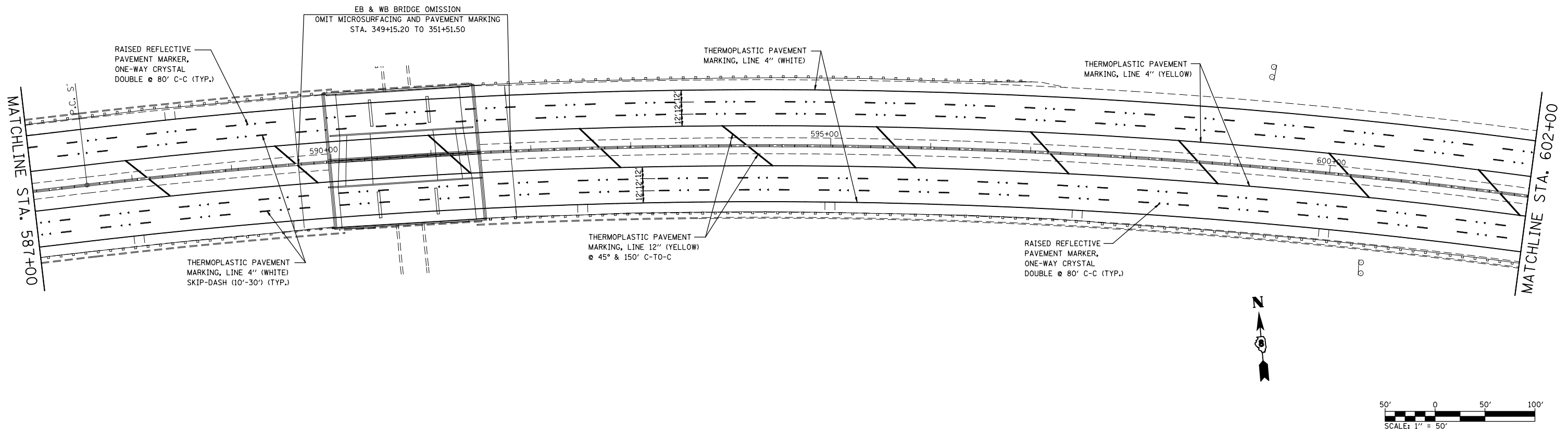
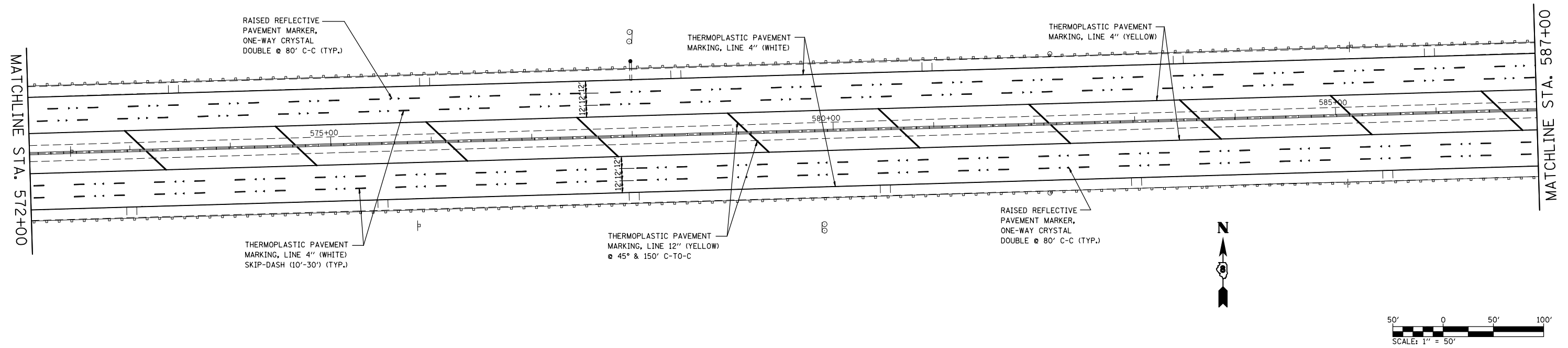
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)RS-1	ST. CLAIR	63	52
CONTRACT NO. 76H71				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



FILE NAME =	USER NAME = durossier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING SHEET	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\work\pwork\dot\durossier_j\d0419849\nd876h71-sht-pmk.dgn	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -			64	82-(4,5,6,7)RS-1	ST. CLAIR	63	53	
	PLOT DATE = 3/26/2015	CHECKED -	REVISED -			CONTRACT NO. 76H71					
		DATE -	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					



FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING SHEET			F.A.I. RTE. = 64	SECTION = 82-(4,5,6,7)RS-1	COUNTY = ST. CLAIR	TOTAL SHEETS = 63	SHEET NO. = 54
et:\pwork\pwork\dot\durosier_j\d0419849\d876h71-sht-pmk.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE: 1" = 50'	SHEET NO. 8 OF 14 SHEETS	STA. 542+00 TO STA. 572+00	CONTRACT NO. 76H71				
PLOT DATE = 3/26/2015	DATE -	DATE -	DATE -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							



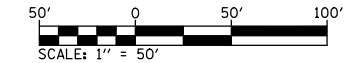
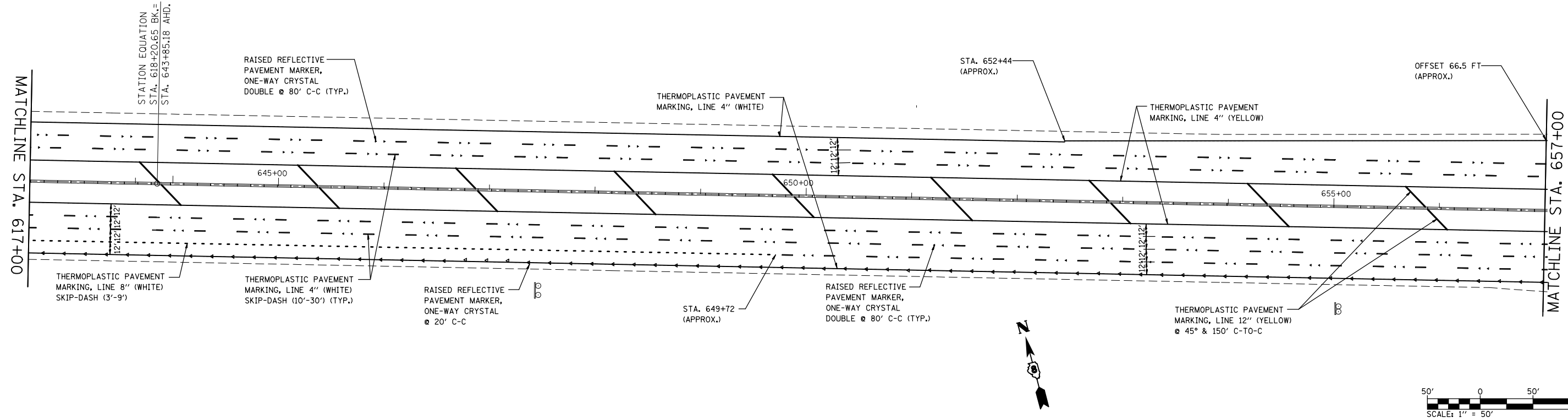
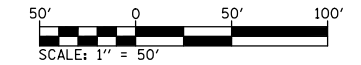
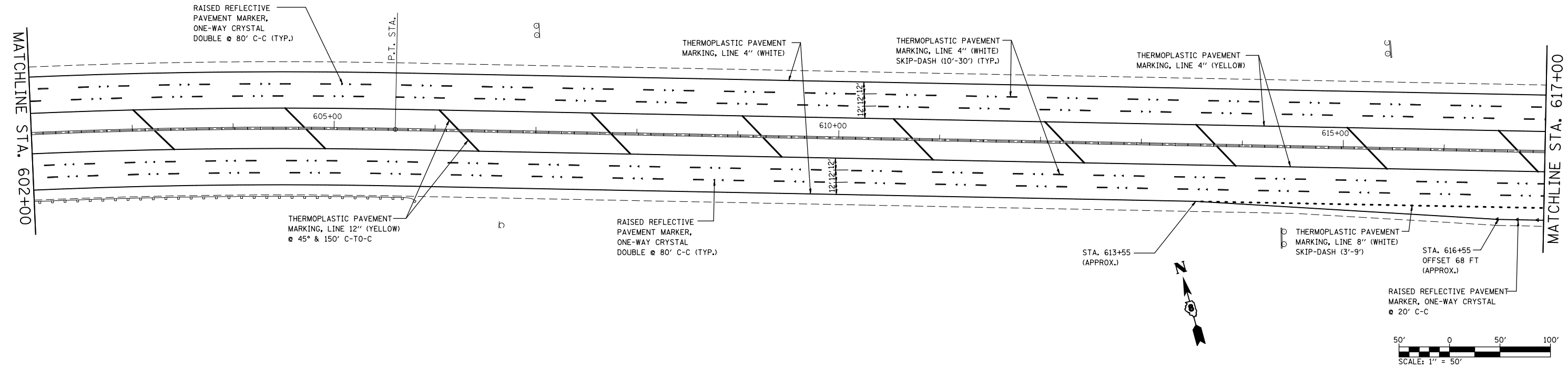
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et:\pwork\pwork\dot\durosier.j\d0419849\d876h71-sht-pmk.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING SHEET

SCALE: 1" = 50' SHEET NO. 9 OF 14 SHEETS STA. 572+00 TO STA. 602+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)RS-1	ST. CLAIR	63	55
CONTRACT NO. 76H71				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



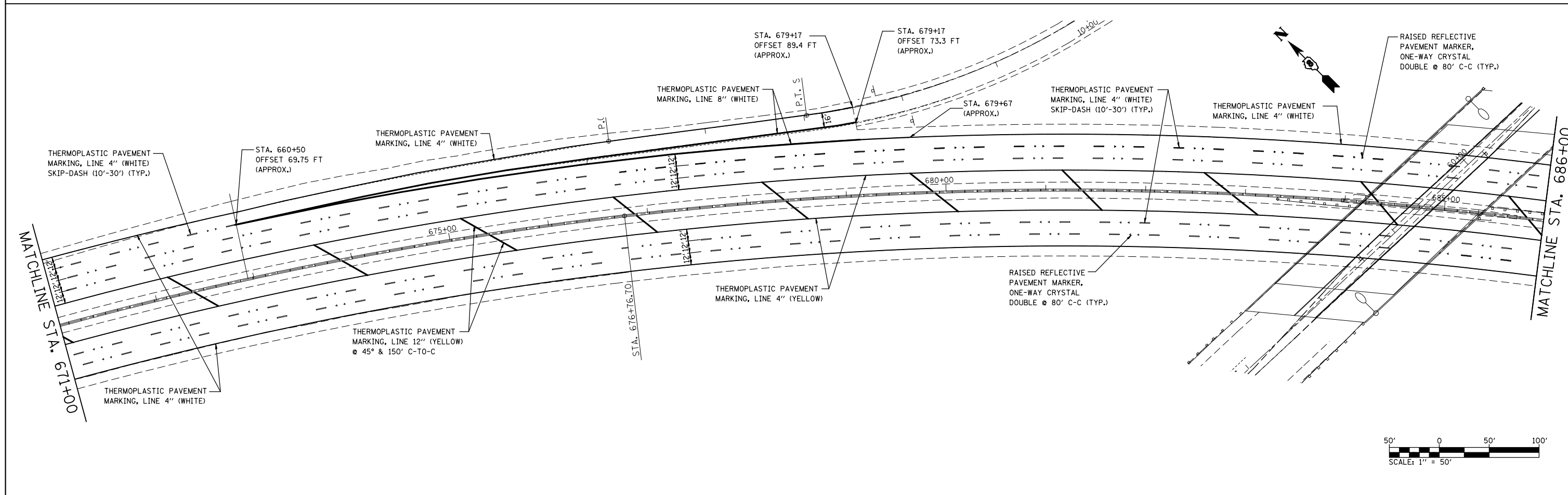
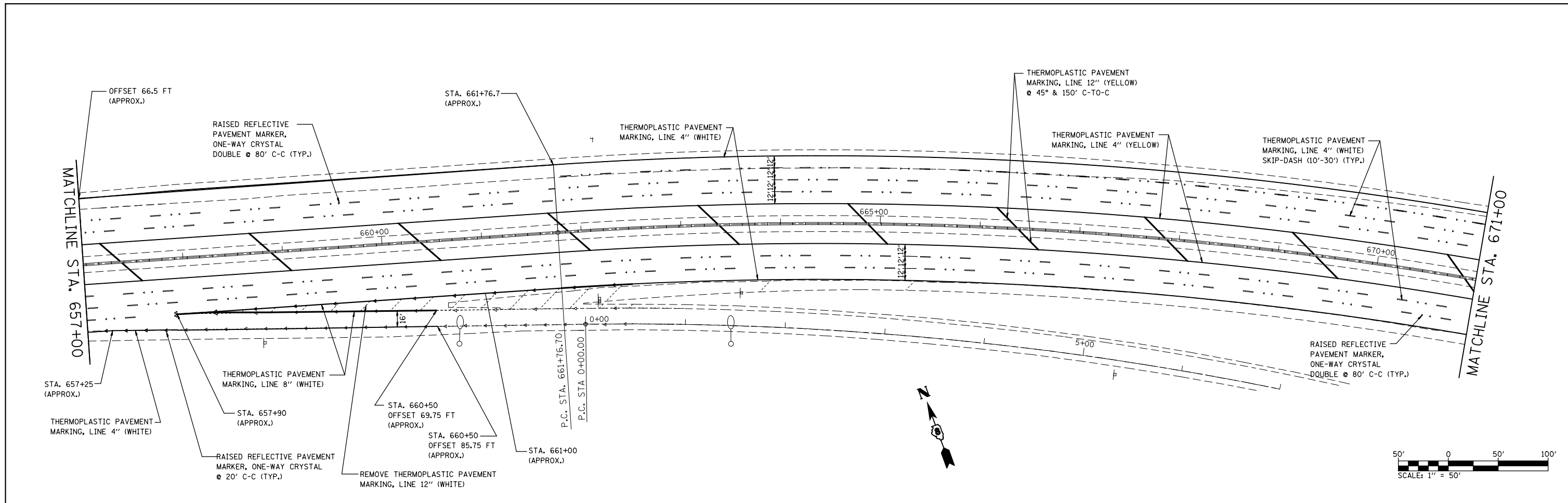
FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -
et:\pwork\work\pwork\durosier_j\0419849\d876h71-sht-pmk.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

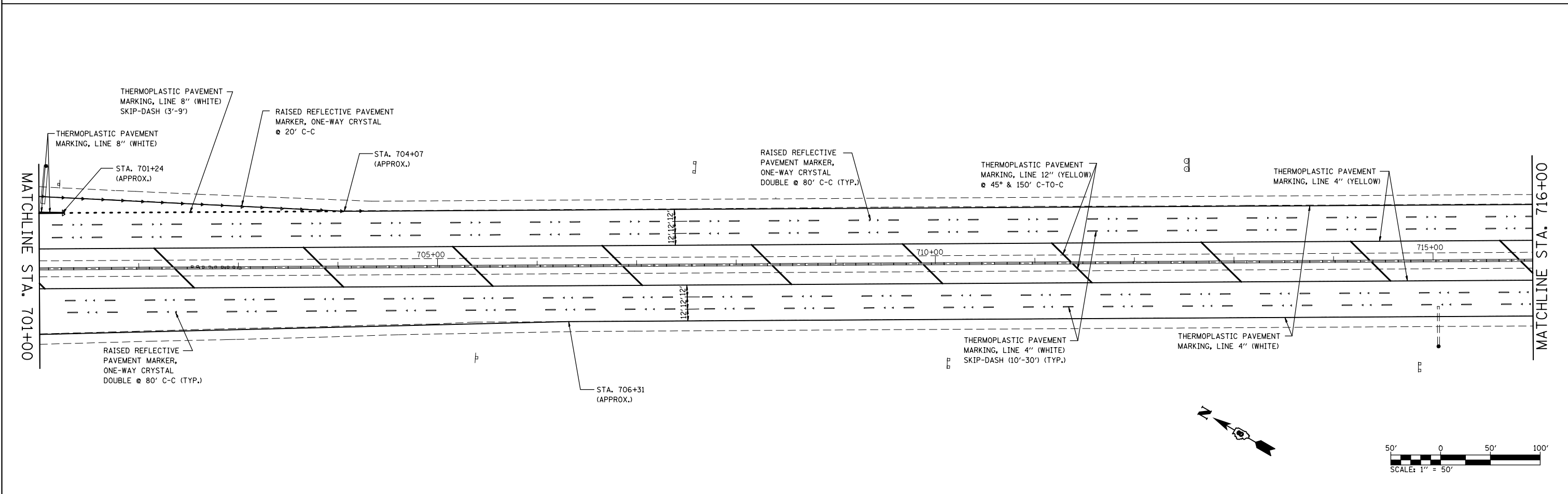
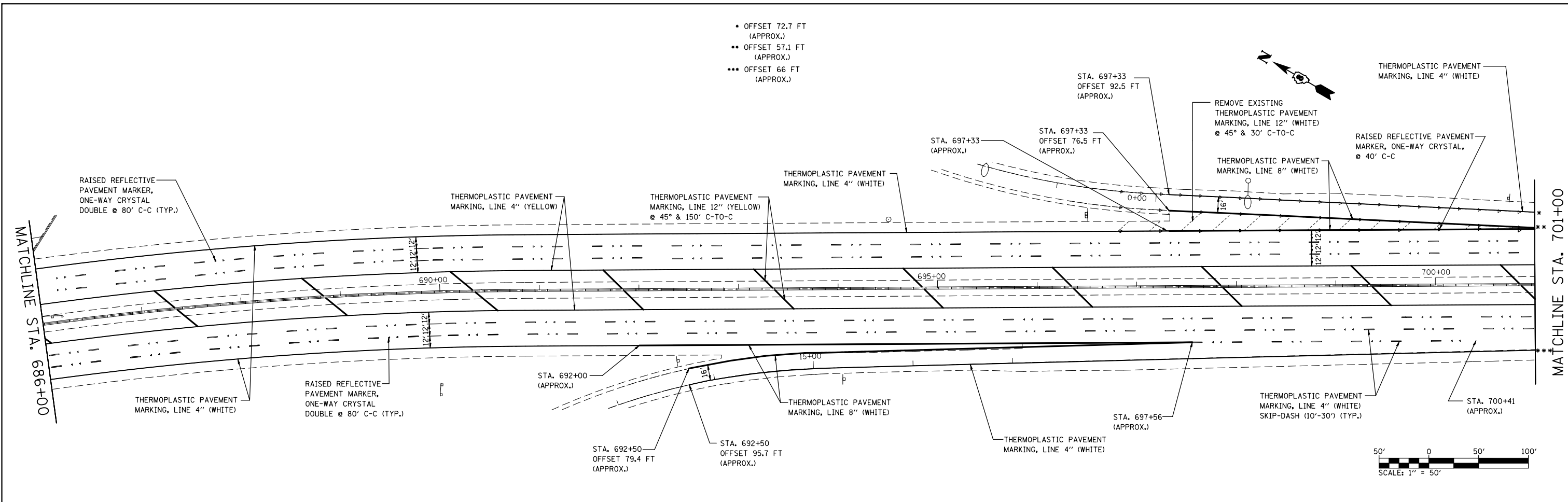
PAVEMENT MARKING SHEET

SCALE: 1" = 50' SHEET NO. 10 OF 14 SHEETS STA. 602+00 TO STA. 657+00

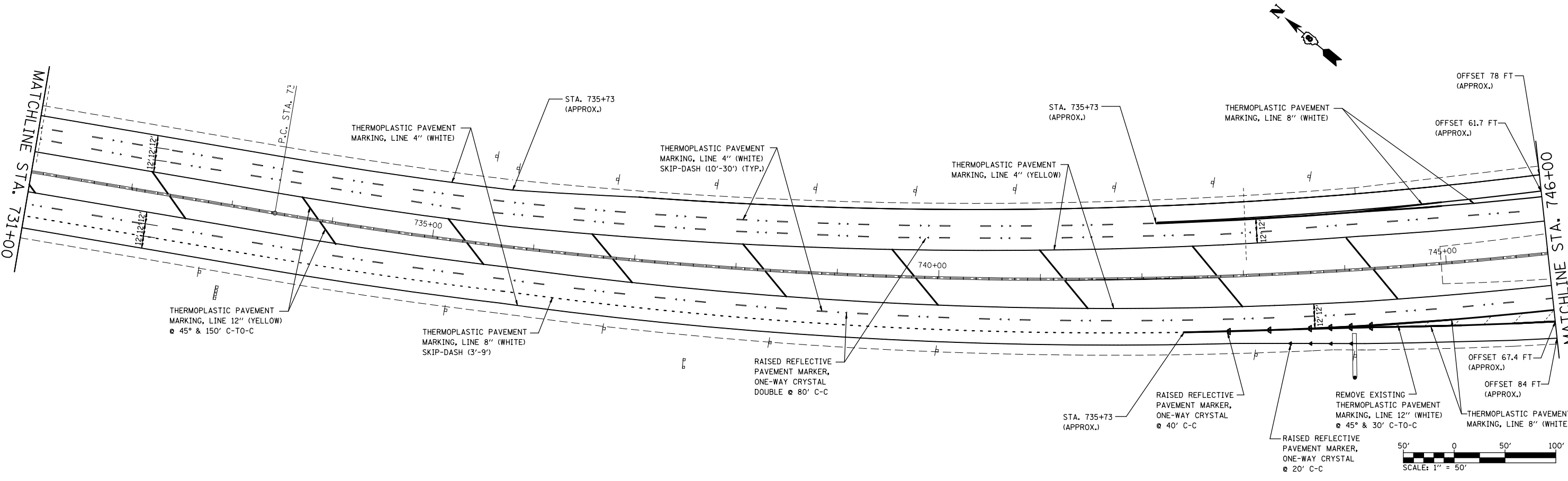
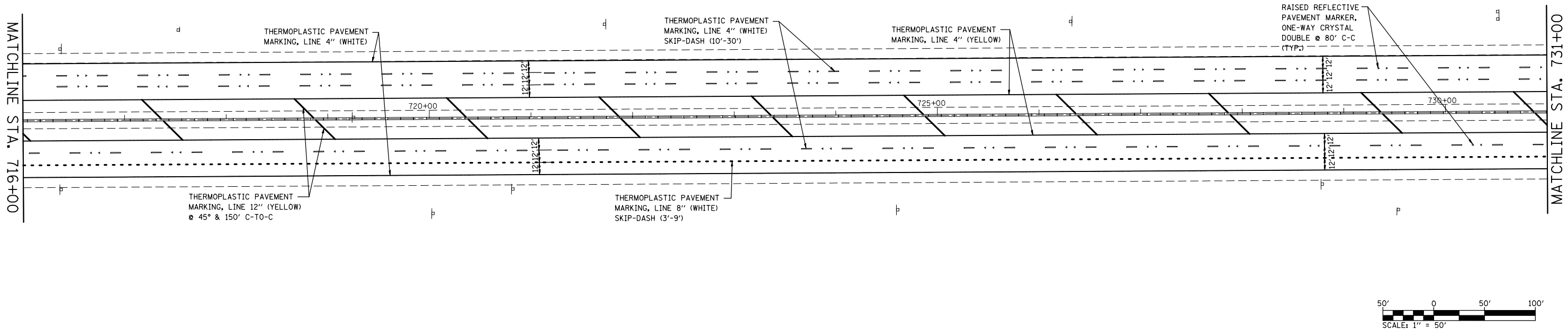
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)RS-1	ST. CLAIR	63	56
CONTRACT NO. 76H71				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = durosier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING SHEET			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwwork\pwwork\durosier_j\d0419849\d876h71-sht-pmk.dgn		DRAWN -	REVISED -		64	82-(4,5,6,7)RS-1	ST. CLAIR	63	57	CONTRACT NO. 76H71		
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -		SCALE: 1" = 50'	SHEET NO. 11 OF 14 SHEETS	STA. 657+00 TO STA. 686+00	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
	PLOT DATE = 3/26/2015	DATE -	REVISED -									



FILE NAME =	USER NAME = dursoier_j	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING SHEET			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -		CONTRACT NO. 76H71							
	PLOT DATE = 3/26/2015	DATE -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							
				SCALE: 1" = 50'	SHEET NO. 12 OF 14 SHEETS	STA. 686+00 TO STA. 716+00						



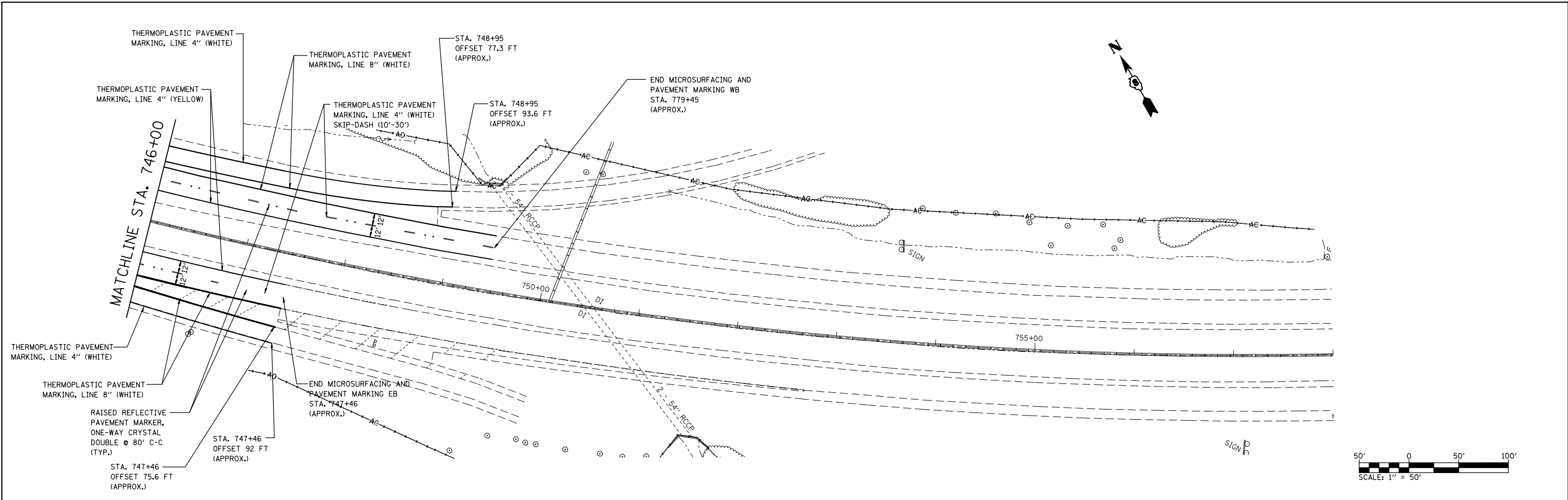
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PLOT DATE = 3/26/2015		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING SHEET

SCALE: 1" = 50' SHEET NO. 13 OF 14 SHEETS STA. 716+00 TO STA. 745+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)RS-1	ST. CLAIR	63	59
CONTRACT NO. 76H71				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

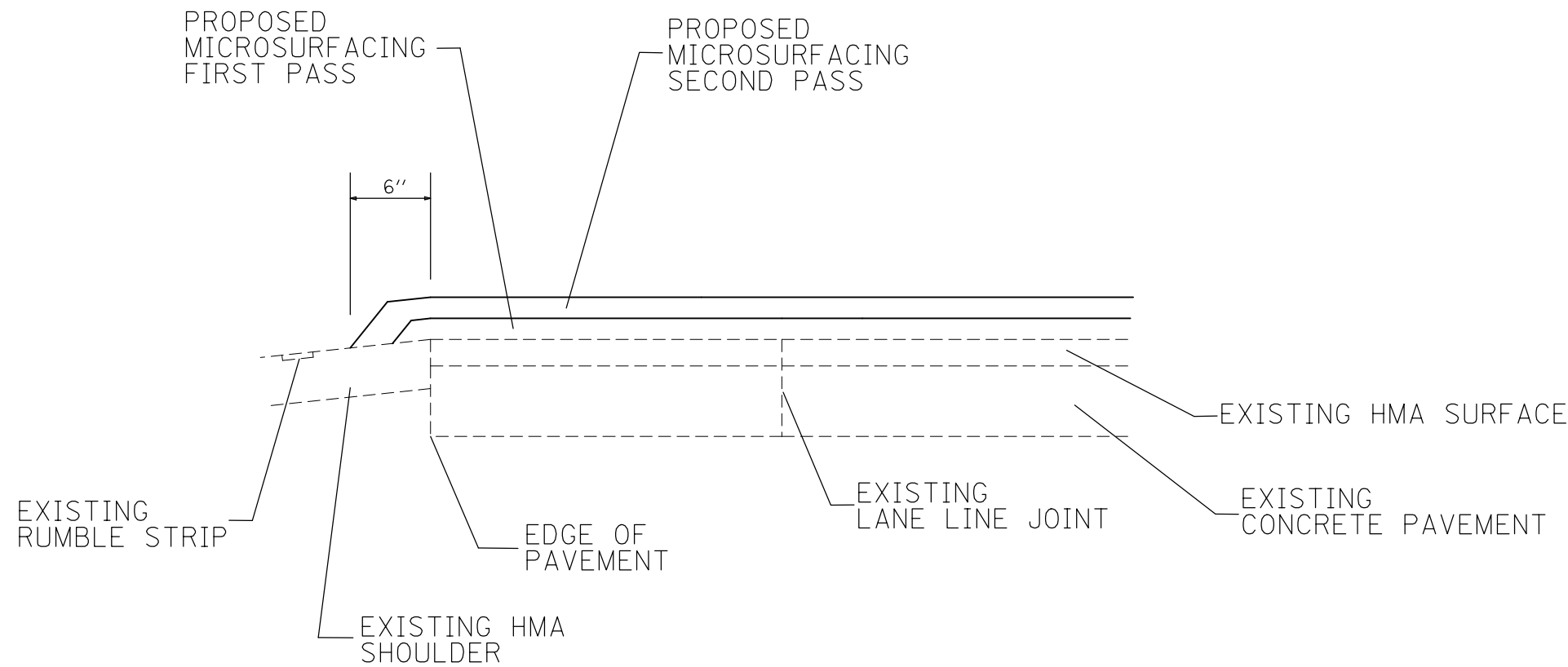


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	PLOT DATE = 3/26/2015	DATE -	REVISED -

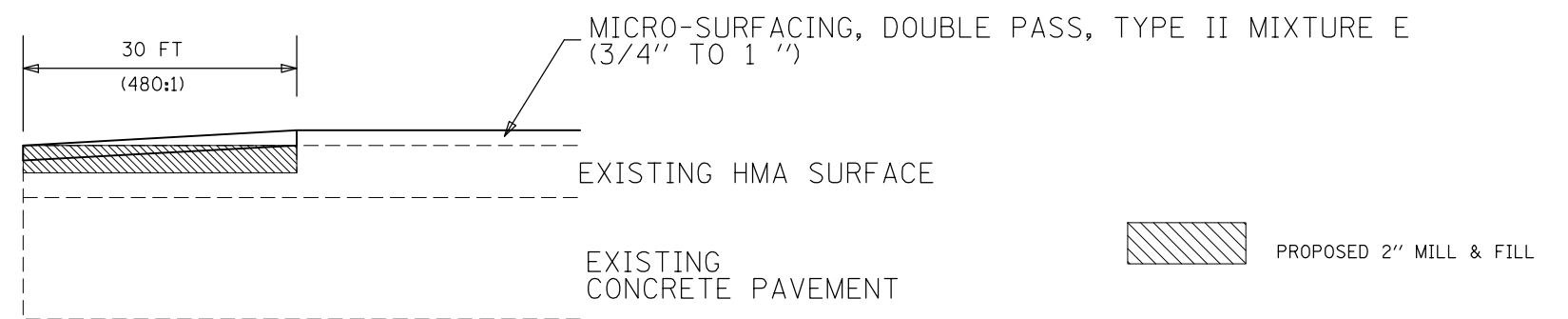
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING SHEET		
SCALE: 1" = 50'	SHEET NO. 14 OF 14 SHEETS	STA. 746+00 TO STA. 749+45

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)RS-1	ST. CLAIR	63	60
CONTRACT NO. 76H71				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



TYPICAL MICROSURFACING DETAIL
(N.T.S.)



BUTT JOINT DETAIL (locations shown on plan sheets)
(N.T.S.)

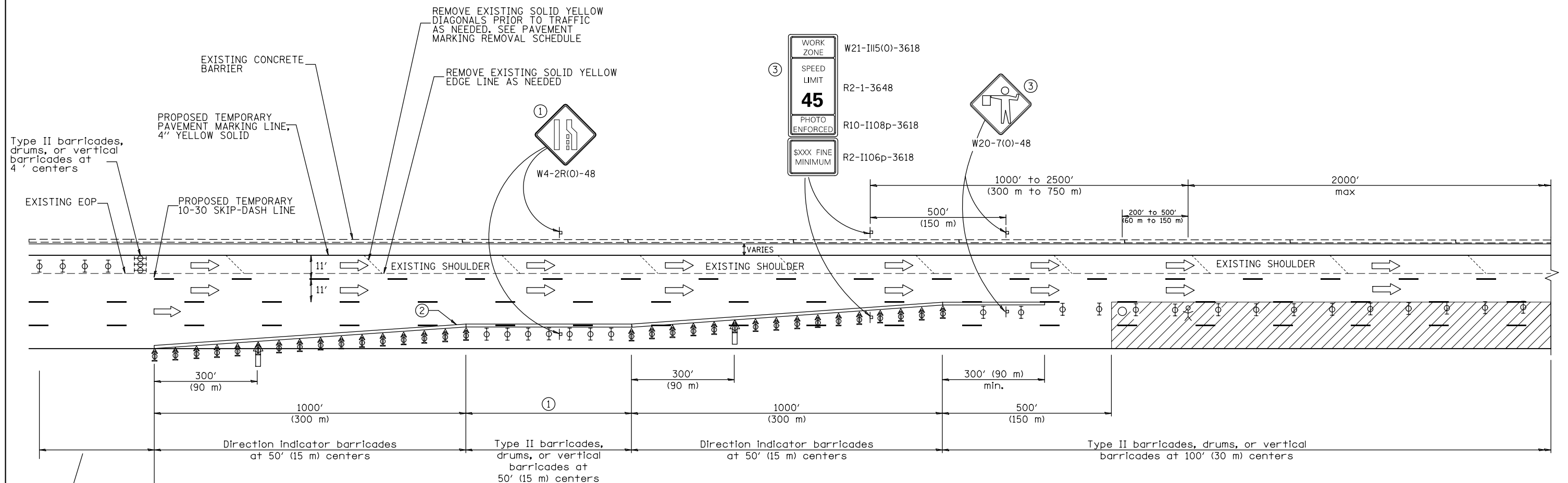
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	PLOT DATE = 3/26/2015	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAIL SHEET

SCALE: SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	61
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76H71	



Type II barricades, drums, or vertical barricades at 100' (30 m) centers on median side only*

See Standard 701400 for approach Start of lane closure taper

*Type II barricades required in locations where the EOP line has been removed and where the shoulder is not being utilized as a driving lane in accordance with Traffic Control and Protection, (Special).

SYMBOLS

- ↑ Arrow board
- ▨ Work area
- ⊗ Worker
- ⊥ Sign
- ⦿ Direction Indicator barricade with steady burn monodirectional light
- ⊕ Type II barricade, drum, or vertical barricade with steady burn monodirectional light
- Flagger with traffic control sign

① The length of the tangent section shall be:

Duration of Closure	Length of Tangent Section
< 14 Days	1000' (300 m)
≥ 14 Days	2000' (600 m)

For off peak closures less than 24 hours, the tangent section and the lane ends sign may be omitted if approved by the Engineer.

② ReflectORIZED temporary pavement marking tape shall be placed throughout the tapers and for 300' (90 m) along-side the work area when the closure time is greater than four-teen days. The edge line shall be white for right lane closures and yellow for left lane closures.

③ Work zone speed limits signs and FLAGGER signs shall be moved as necessary to maintain the required spacing between the signs and the workers in each separate work activity. Work Zone Speed Limit 55 Photo Enforced sign shall be omitted when the work area dictates that placement of the sign array within 500' (150 m) of the End Work Zone Speed Limit sign.

GENERAL NOTES

Unless specified otherwise, all temporary pavement marking, blackout tape, and workzone pavement marking removal shall be included in the cost of Traffic Control and Protection, (Special).

In lieu of removing pavement marking on the structures, blackout tape shall be used.

This Standard is used where at any time any vehicle, equipment, workers or their activities will encroach on two lanes of a freeway/expressway and utilize the left shoulder as a driving lane.

This Standard must always be used in combination with Standard 701400.

Check barricades shall be placed in the middle of the closed lanes at 1000' (300 m) centers.

All dimensions are in inches (millimeters) unless otherwise shown.

A minimum of 6 feet is required between all signs and traffic. Therefore, it is anticipated that in locations where the shoulder lane is dropped, the 18 foot shoulder is present and not being used for traffic on the opposite side of the concrete barrier.

TRAFFIC CONTROL AND PROTECTION, (SPECIAL)

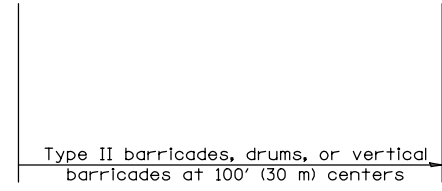
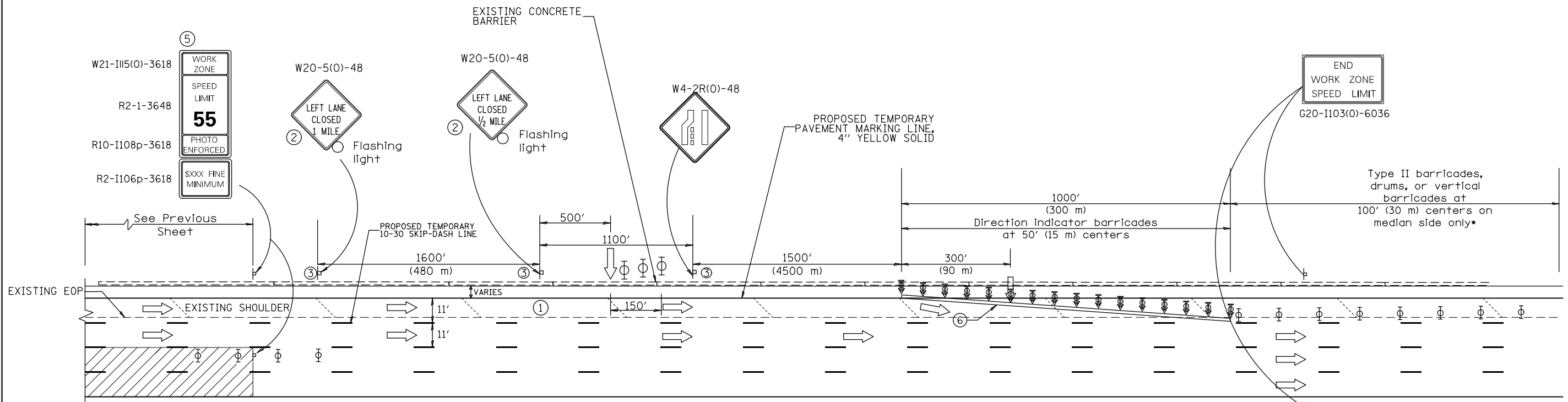
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	PLOT DATE = 3/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL DETAIL

SCALE: SHEET NO. 2 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-(4,5,6,7)-RS-1	ST. CLAIR	63	62
CONTRACT NO. 76H71				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



SYMBOLS

- Arrow board
- Work area
- Worker
- Sign
- Direction indicator barricade with steady burn monodirectional light
- Type II barricade, drum, or vertical barricade with steady burn monodirectional light
- Flagger with traffic control sign

- ① Three, Type II barricades, drums, or vertical barricades at 50' (15 m) centers.
- ② This sign shall be used when 2 lanes are closed.
- ③ This sign shall be omitted when median width is less than 10' (3 m).
- ④ This sign shall only be used if the existing speed limit is greater than 65 mph.
- ⑤ Work zone speed limits signs and FLAGGER signs shall be moved as necessary to maintain the required spacing between the signs and the workers in each separate work activity. Work Zone Speed Limit 55 Photo Enforced sign shall be omitted when the work area dictates that placement of the sign array within 500' (150 m) of the End Work Zone Speed Limit sign.
- ⑥ ReflectORIZED temporary pavement marking tape shall be placed throughout the tapers and for 300' (90 m) along-side the work area when the closure time is greater than fourteen days. The edge line shall be white for right lane closures and yellow for left lane closures.

GENERAL NOTES

In lieu of removing pavement marking on the structures, blackout tape shall be used.

Unless specified otherwise, all temporary pavement marking, blackout tape, and workzone pavement marking removal shall be included in the cost of Traffic Control and Protection, (Special).

This Standard is used where at any time any vehicle, equipment, workers or their activities will encroach on two lanes of a freeway/expressway and utilize the left shoulder as a driving lane.

This Standard must always be used in combination with Standard 701400.

Check barricades shall be placed in the middle of the closed lanes at 1000' (300 m) centers.

All dimensions are in inches (millimeters) unless otherwise shown.

A minimum of 6 feet is required between all signs and traffic. Therefore, it is anticipated that in locations where the shoulder lane is dropped, the 18 foot shoulder is present and not being used for traffic on the opposite side of the concrete barrier.

*Type II barricades required in locations where the EOP line has been removed and where the shoulder is not being utilized as a driving lane in accordance with Traffic Control and Protection, (Special).

TRAFFIC CONTROL AND PROTECTION SPECIAL SCHEDULE (APPROX.) ****										
LOCATION	TEMPORARY PAVEMENT MARKING LINE - 4"						PAVEMENT MARKING BLACKOUT TAPE, 4"****		WORK ZONE PAVEMENT MARKING REMOVAL	
	SKIP-DASH (WHITE)		SOLID (WHITE)**		SOLID (YELLOW)					
STA TO STA	FOOT		FOOT		FOOT		FOOT		SQ FT	
	EB	WB	EB	WB	EB	WB	EB	WB	EB	WB
337+00 TO 749+45	10311	10311	6600	6600	41245	41245	389	389	19515	19515
SUB-TOTALS	10311	10311	6600	6600	41245	41245	389	389	19515	19515
TOTALS	116313						778		39030	

**Solid white tape used along tapers and 300' alongside the work area within the closure

***Blackout Tape used on bridges with project limits

****Unless noted otherwise, cost for these items is included in the cost for Traffic Control and Protection, (Special)

TRAFFIC CONTROL AND PROTECTION, (SPECIAL)