

CONSTRUCTION PLANS FOR VERMILION REGIONAL AIRPORT AUTHORITY

VERMILION REGIONAL AIRPORT DANVILLE, ILLINOIS

IL. PROJ. NO. DNV-4305
SBG PROJ. NO. 3-17-SBGP-111

REHABILITATE THE EASTERN PORTION OF THE AIRCRAFT PARKING APRON; REHABILITATE TAXIWAY C AND C1

APRIL 17, 2015

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SUMMARY OF QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	LS	1
AR150520	MOBILIZATION	LS	1
AR152410	UNCLASSIFIED EXCAVATION	CY	300
AR152480	SHOULDER ADJUSTMENT	SY	2654
AR201660	BITUMINOUS CRACK REPAIR	LF	320
AR201661	CLEAN AND SEAL CRACKS	LF	11840
AR201670	CRACK CONTROL FABRIC	SY	6500
AR209612	CRUSHED AGG. BASE COURSE - 12"	SY	1300
AR401610	BITUMINOUS SURFACE COURSE	TON	3420
AR401630	BITUMINOUS SURFACE TEST SECTION	EA	1
AR401650	BITUMINOUS PAVEMENT MILLING	SY	17825
AR401655	BUTT JOINT CONSTRUCTION	SY	1980
AR403610	BITUMINOUS BASE COURSE	TON	290
AR501905	REMOVE PAVEMENT	SY	1230
AR510510	TIE DOWN	EA	10
AR510900	REMOVE TIE DOWN	EA	10
AR602510	BITUMINOUS PRIME COAT	GAL	615
AR603510	BITUMINOUS TACK COAT	GAL	6780
AR620510	PAVEMENT MARKING	SF	12100
AR901510	SEEDING	ACRE	0.5
AR908525	KNITTED STRAW MAT	SY	2175



J.U.L.I.E.
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS
www.illinois811.com

THE LOCATION AND TYPE OF ALL UTILITIES SHOWN ON THESE PLANS INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATIONS OF ALL SUCH UTILITIES INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE OPERATIONAL SCHEDULE BY THE TIME THE INTERFERENCE IS NOTICED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

DESIGN INFORMATION

GEOMETRIC CRITERIA

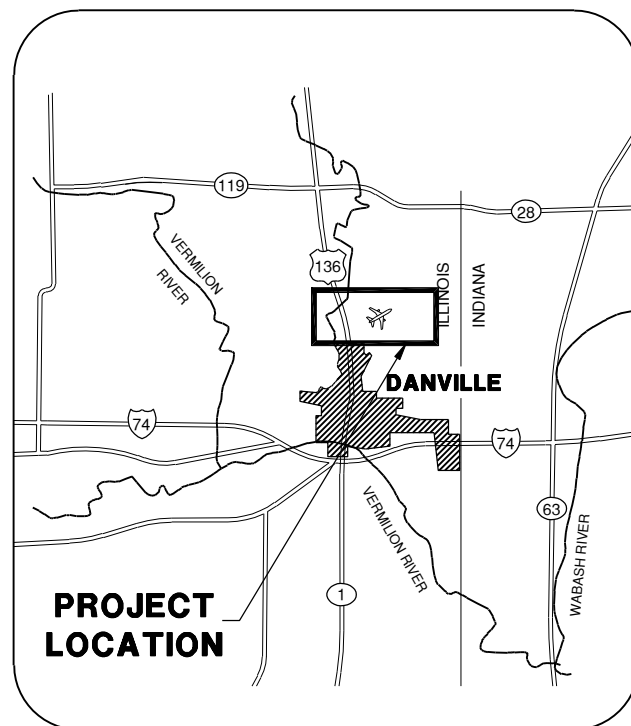
TAXIWAY DESIGN GROUP III
AIRCRAFT APPROACH CATEGORY C
WHEELBASE OF 42' 10"
MAIN GEAR WIDTH OF 15' 4"

PAVEMENT DESIGN CRITERIA

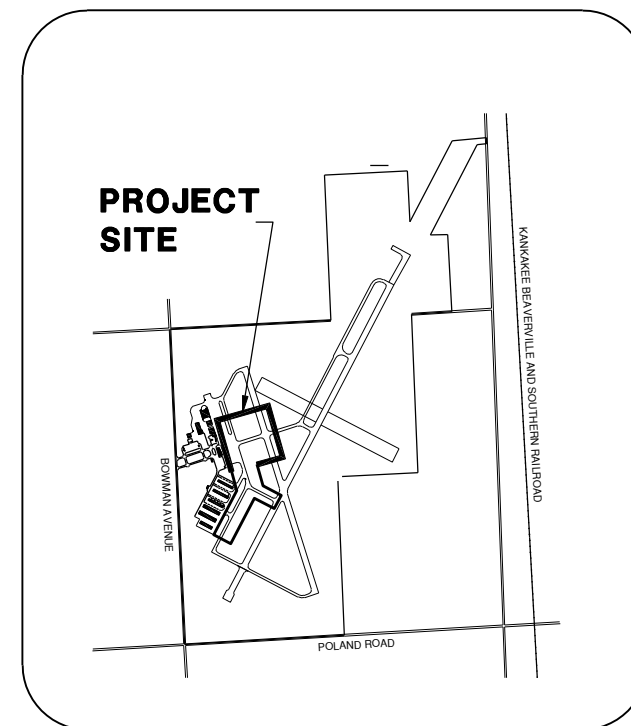
DEPARTURE WEIGHT = 99,500 LBS.
DUAL GEAR

CALL J.U.L.I.E.
BEFORE EXCAVATING
1-800-892-0123

TOWNSHIP: 20 NORTH
RANGE: 11 WEST
SECTION: 15
COUNTY: VERMILION
CIVIL TOWNSHIP:



LOCATION MAP



SITE PLAN



MAXIMUM EQUIPMENT HEIGHT - 25'
UNICOM FREQUENCY - 122.70

VERMILION REGIONAL AIRPORT AUTHORITY
VERMILION REGIONAL AIRPORT
DANVILLE, ILLINOIS

APPROVED *Robert H. Groth*
EXECUTIVE DIRECTOR OF AVIATION

DATE 4-9-15



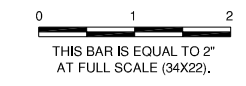
License No. 184-000613

SUBMITTED BY *Christopher B. Groth*
DATE 4-21-2015

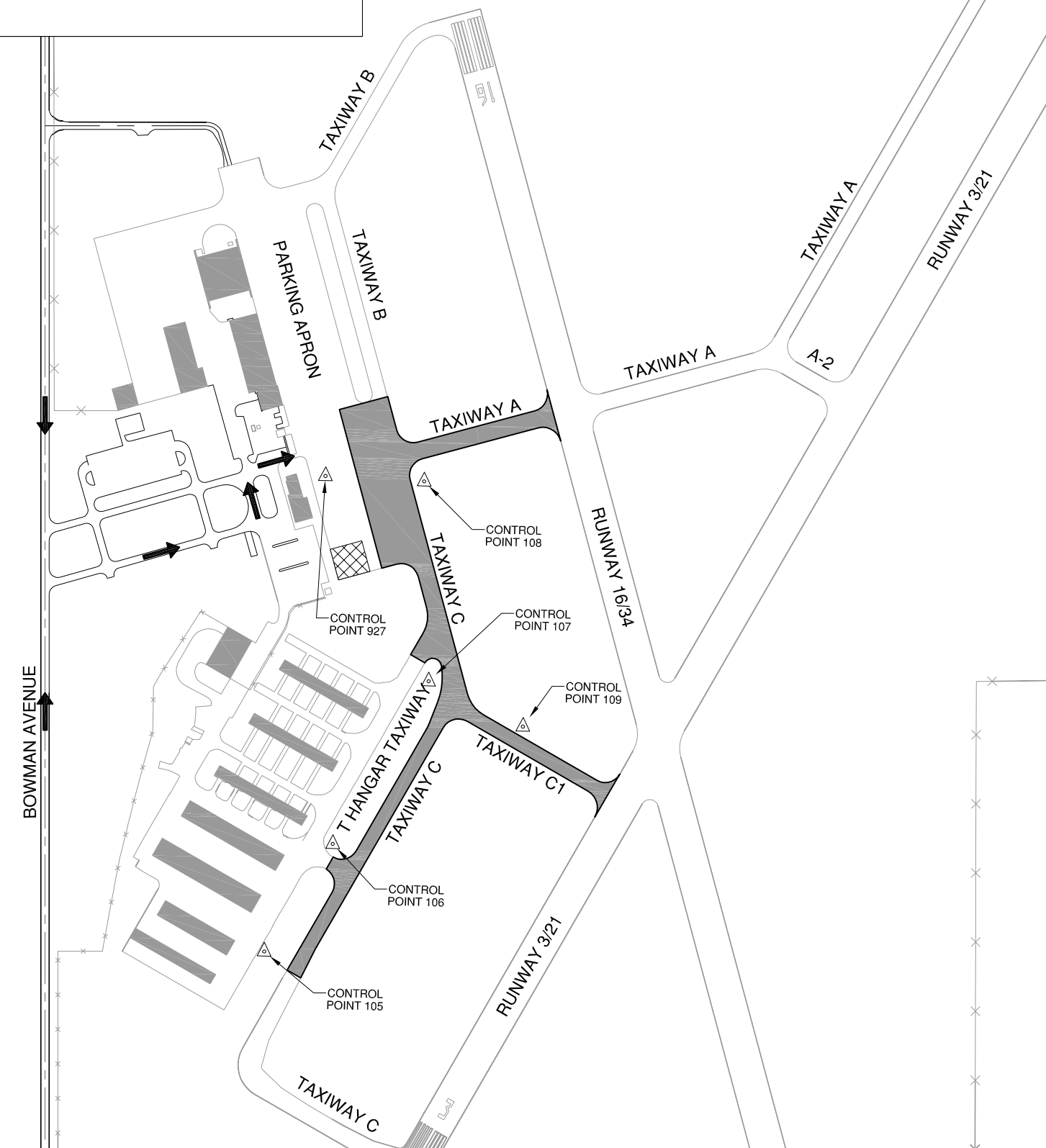
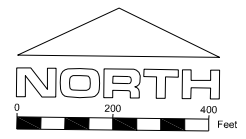
CMT JOB NUMBER: 14042-02

VE051

REVISIONS		
NUMBER	BY	DATE



CONTROL POINT TABLE			
POINT	NORTHING	EASTING	ELEVATION
105	1286002.322	1188801.052	655.74
106	1286318.041	1188992.355	656.61
107	1286794.748	1189259.157	659.64
108	1287372.992	1189233.464	660.15
109	1286669.782	1189534.962	656.88
927	1287382.221	1188947.680	662.81



LEGEND	
	AREA OF CONSTRUCTION
	CONTRACTOR'S STAGING AND STORAGE
	EXISTING
	CONTRACTOR'S ACCESS
	CONTROL POINT

- GENERAL NOTES**
- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
 - WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
 - THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION. AT NO POINT SHALL EQUIPMENT OR MATERIALS BE ALLOWED TO BE STORED ON RUNWAYS.
 - MEN, VEHICLES AND EQUIPMENT WILL NOT BE ALLOWED WITHIN THE FOLLOWING AREAS:
 - 250' FROM THE CENTERLINE OF AN ACTIVE RUNWAY.
 - 93' FROM THE CENTERLINE OF AN ACTIVE TAXIWAY.
 - BROKEN ASPHALT, ASPHALT MILLINGS, PCC AND OTHER CONSTRUCTION DEBRIS SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
 - ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
 - EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTINUOUSLY CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.
 - IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
 - CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - THE CONTRACTOR SHALL MAINTAIN A COMPLETE LIST OF PERSONNEL AUTHORIZED BY THE CONTRACTOR TO WORK ON THE AIRFIELD. THIS LIST SHALL BE PROVIDED TO THE AIRPORT AND RESIDENT ENGINEER BY THE CONTRACTOR UPON REQUEST.
 - THE CONTRACTOR SHALL USE AN EXISTING GATE(S) FOR ACCESS TO THE AIRFIELD.
 - CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS.
 - THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.

VERMILION REGIONAL AIRPORT
 VERMILION REGIONAL AIRPORT AUTHORITY
 DANVILLE, ILLINOIS

REHABILITATE EASTERN APRON; TAXIWAY C & C1
 PAVEMENTS
 AIRPORT SITE PLAN



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DESIGN BY:	CBG
DRAWN BY:	CMT
CHECKED BY:	CET
APPROVED BY:	CET
DATE:	APRIL 17, 2015
JOB No:	14042-02
IL. PROJ. NO. DNV-4305 SBG PROJ. NO. 3-17-SBGP-111	
SHEET	02 OF 25 SHEETS

GENERAL

- 1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- 1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

- 1. TOTAL CONTRACT TIME SHALL BE 44 CALENDAR DAYS.
2. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEET.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- 1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

5. CONTRACTOR ACCESS

- 1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
3. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE VILLAGE, COUNTY, TOWNSHIP, OR I.D.O.T.
4. CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE MARKED AND FLAGGED PER SECTION 70-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
5. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, ASPHALT, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR PERSONNEL THAT HAS OBTAINED PROPER DRIVING PRIVILEGES.
6. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE COMMON TRAFFIC ADVISORY FREQUENCY (CTAF, 122.7) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED CMI/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
7. THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN
8. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
9. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
10. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
11. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
12. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE COMMON TRAFFIC ADVISORY FREQUENCY. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
13. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
14. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
15. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
16. THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

- 1. THE CONTRACTOR SHALL NOTIFY PUBLIC SAFETY OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- 1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.

8. INSPECTION REQUIREMENTS

- 1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL REQUEST OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. THE AIRPORT WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.

9. UNDERGROUND UTILITIES

- 1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

10. PENALTIES

- 1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

11. SPECIAL CONDITIONS

- 1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

12. RUNWAY AND TAXIWAY VISUAL AIDS

- 1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
2. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2F.

13. MARKING AND SIGNS FOR ACCESS ROUTES

- 1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THE CONSTRUCTION ACTIVITY PLAN SHEETS.

14. HAZARD MARKING AND LIGHTING

- 1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE ENGINEER.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
5. THE AIRPORT WILL PROVIDE TWO PORTABLE CLOSED RUNWAY MARKERS FOR USE DURING THE PROJECT. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTENANCE OF THE RUNWAY CLOSURE MARKERS INCLUDING FUEL, OIL CHANGES AND REPLACEMENT OF THE LIGHTS. UPON COMPLETION OF THE PROJECT, THE PORTABLE CLOSED RUNWAY MARKERS SHALL BE TURNED OVER TO THE AIRPORT.

15. PROTECTION

- 1. ALL WORK REQUIRED INSIDE OF THE RUNWAY 3/21 OR 16/34 SAFETY AREAS, WHICH EXTENDS 250' AND 75' RESPECTIVELY FROM THE RUNWAY CENTERLINE, WILL REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 48 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 93' FROM THE TAXIWAY CENTERLINE OF TAXIWAYS, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
3. ALL WORK REQUIRED ON AN ACTIVE APRON OR INSIDE OF AN ACTIVE SAFETY AREA, WHICH EXTENDS 70' FROM THE APRON'S EDGE OF PAVEMENT, WILL REQUIRE A PORTION OF THAT APRON TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

16. OTHER LIMITATIONS ON CONSTRUCTION

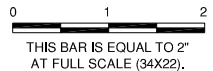
- 1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THE AIRSPACE FOR THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THAT SPECIFIED ON THE PLANS WITH THE FAA. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO COMPLETE.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE CONTRACT DOCUMENTS.

FILE: DNV-4305-1404202-GC004.dwg
UPDATE BY: ChrIs Groth
PLOT DATE: 4/21/2015 7:56 PM

VE051

REVISIONS

Table with 3 columns: NUMBER, BY, DATE



VERMILION REGIONAL AIRPORT
VERMILION REGIONAL AIRPORT AUTHORITY
DANVILLE, ILLINOIS

REHABILITATE EASTERN APRON; TAXIWAY C & C1 PAVEMENTS
CONSTRUCTION SAFETY PHASING NOTES



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Table with 2 columns: Field (DESIGN BY, DRAWN BY, CHECKED BY, APPROVED BY, DATE, JOB No), Value (CBG, ADD, CET, CET, APRIL 17, 2015, 14042-02)

IL PROJ. NO. DNV-4305
SBG PROJ. NO. 3-17-SBG-111

SEQUENCE OF CONSTRUCTION NOTES

THE GENERAL PROGRESSION OF THE WORK SHALL BE AS FOLLOWS:

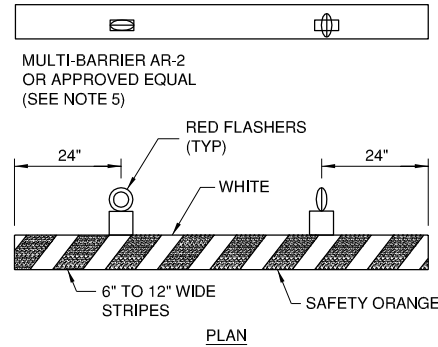
- SUBMIT MATERIAL AND EQUIPMENT SHOP, PLAN AND WORKING DRAWINGS FOR REVIEW. INCLUDE WITH THE SUBMITTALS ALL BUY AMERICAN CERTIFICATIONS FOR ALL MATERIALS.
- SUBMIT NOTICE OF OBSTRUCTION EVALUATION- AIRPORT AIRSPACE ANALYSIS (OE/AAA) INFORMATION FOR ANTICIPATED EQUIPMENT HEIGHTS IF IN EXCESS OF 25'. NOTE THAT THIS PROCESS MAY REQUIRE UP TO 90 DAYS FOR FAA APPROVAL. EQUIPMENT ABOVE 25' HEIGHT SHALL NOT BE UTILIZED UNTIL FAA APPROVAL HAS BEEN PROVIDED.
- SUBMIT PROJECT SCHEDULE SHOWING RELATIONSHIP BETWEEN CONSTRUCTION DURATION FOR PAY ITEMS IN RELATION TO THE PHASES OF WORK WHERE THEY ARE BEING PERFORMED. CLEARLY IDENTIFY DATES OF RUNWAY CLOSURES AND WHAT PHASES WILL BE WORKED IN DURING THAT CLOSURE.
- SUBMIT PRELIMINARY MATERIALS CERTIFICATIONS INCLUDING BUY AMERICAN CERTIFICATIONS AND WAIVER REQUEST FOR MATERIALS THAT DO NOT MEET THE CONTRACT REQUIREMENTS.
- INITIATE CONSTRUCTION OF TAXIWAY/APRON REHABILITATION. INSTALL BARRICADES AS OUTLINED ON THE CONSTRUCTION ACTIVITY PLANS. FIELD VERIFY LOCATION OF EXISTING UTILITIES.
- INITIATE CONSTRUCTION WITHIN THE VARIOUS PHASES OF THE PROJECT. TAXIWAY C/C1 AND APRON REHABILITATION PHASES SHALL BE CONSIDERED THE PRIMARY WORK AREAS. WORK IN THESE AREAS SHALL INCLUDE REMOVAL OF PAVEMENT, PAVEMENT MILLING, BITUMINOUS PAVING, PAVEMENT MARKING AND TURFING.
- UPON COMPLETION OF ALL PHASES, THE CONTRACTOR SHALL REQUEST A FINAL INSPECTION OF THE PROJECT.

RUNWAY SAFETY AREAS

- WORK IN THE RUNWAY 3/21 AND 16/34 SAFETY AREA SHALL BE LIMITED TO THAT WORK NECESSARY TO REHABILITATE TAXIWAY C1 AND TAXIWAY A PAVEMENTS. NO PAVEMENT IMPROVEMENTS WILL BE MADE TO THE SURFACE OF RUNWAY 3/21 AND 16/34.
- WORK WITHIN THE RUNWAY 3/21 AND 16/34 SHALL CONSIST OF PAVEMENT MILLING, PAVEMENT CONSTRUCTION AND PAVEMENT MARKING. THE MILLED PAVEMENT SURFACE WILL RESULT IN A DROP OFF WITHIN THE RSA OF LESS THAN 3". CONSTRUCTION WITHIN THE RSA SHALL REQUIRE THE CLOSURE OF RUNWAY 3/21 AND 16/34. RUNWAY SHALL BE REOPENED UPON COMPLETION OF CONSTRUCTION ACTIVITIES.
- WORK WITHIN THE RUNWAY SAFETY AREAS FOR PHASE 2A AND 3A SHALL OCCUR DURING DAY TIME OPERATIONS ONLY. RUNWAYS SHALL BE REOPENED AT THE END OF EACH WORK DAY. BARRICADES AND RUNWAY CLOSURE X SHALL BE PLACED AT THE BEGINNING OF EACH WORK DAY CLOSURE AND REMOVED AT THE END OF THE WORK DAY.
- EQUIPMENT OR PERSONNEL SHALL REMAIN CLEAR OF THE RUNWAY PAVEMENTS AT ALL TIMES.
- NO EQUIPMENT, STOCKPILES OR EXCAVATIONS SHALL REMAIN INSIDE THE RUNWAY SAFETY AREAS AFTER WORKING HOURS.

APRON / TAXIWAY OBJECT FREE AREAS

- WORK IN THE TAXIWAY OBJECT FREE AREAS SHALL REQUIRE THAT TAXIWAY TO BE CLOSED.
- NO EQUIPMENT, OPEN TRENCHES OR EXCAVATIONS SHALL REMAIN INSIDE THE TAXIWAY OBJECT FREE AREAS AFTER WORKING HOURS.
- THE TAXIWAYS OR EDGES OF APRONS SHALL BE CLOSED WITH BARRICADES AT 15' MAXIMUM SPACING PRIOR TO WORKING IN THE CRITICAL WORK AREAS.

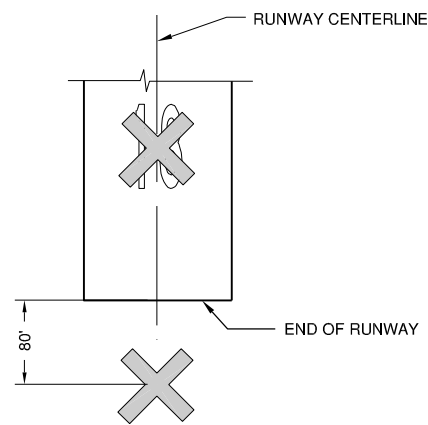


BARRICADE NOTES:

- FLASHERS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
- FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
- BARRICADES TO BE PLACED WITH A MAXIMUM OF 15' SPACING BETWEEN ENDS OF BARRICADES ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION OR AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
- FLASHERS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
- BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR LOW AS POSSIBLE, BUT NOT TO EXCEED 3 INCHES ABOVE THE GROUND.

LOW PROFILE LIGHTED BARRICADE

N.T.S.

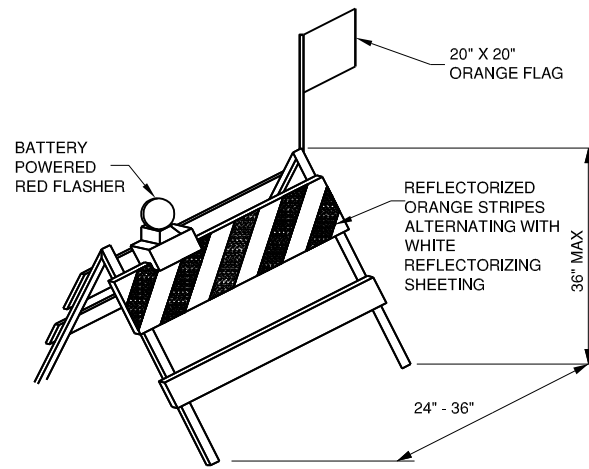


CLOSED RUNWAY MARKER DETAIL

N.T.S.

NOTES

- MARKERS SHALL BE SOLID YELLOW.
- MARKERS SHALL BE SELF CONTAINED MOBILE MARKERS PROVIDED BY THE CONTRACTOR THROUGH THE CONTRACT.
- CONTRACTOR SHALL MAINTAIN MARKERS.
- COST OF INSTALLING, MAINTAINING, RELOCATING AND REMOVING MARKERS SHALL BE INCIDENTAL TO THE CONTRACT.
- MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS.



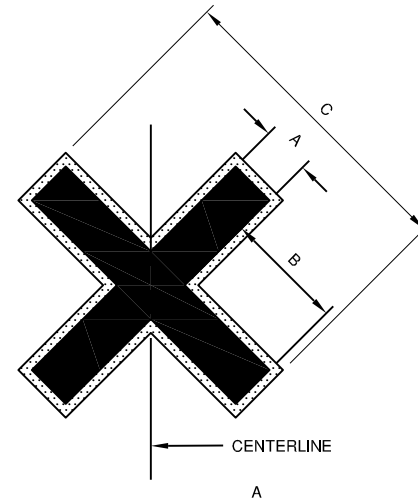
NOTE:

BARRICADES SHALL BE PLACED AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS 15' ON CENTER AT DESIGNATED LOCATIONS. BARRICADE SHALL BE WEIGHTED WITH A MINIMUM OF 6 SAND BAGS TO PREVENT THEM FROM BEING BLOWN OVER.

IDOT TYPE 1 BARRICADE DETAIL

NOTE:

WHERE NOT SPECIFIED, THE CONTRACTOR SHALL HAVE THE OPTION AS TO WHICH TYPE OF BARRICADE IS USED.



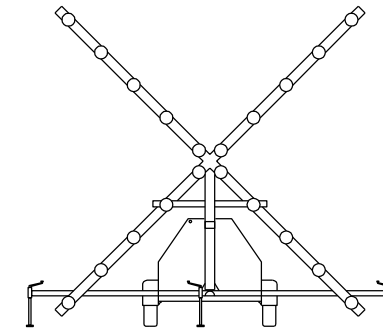
CLOSED RUNWAY/ TAXIWAY MARKER DETAIL

N.T.S.

SYMBOL TYPE	DIMENSION	A	B	C
CLOSED RUNWAY		10'-0"	25'-0"	60'-0"
CLOSED TAXIWAY		5'-0"	12'-6"	30'-0"

NOTES

- CLOSURE MARKERS SHALL BE SOLID YELLOW.
- MARKERS SHALL BE PLACED ON TAXIWAYS AT THE RUNWAY INTERSECTIONS INSIDE THE RUNWAY SAFETY AREA.
- MARKERS MAY BE PAINTED ON THE TAXIWAY USING TEMPORARY PAINT OR CONSTRUCTED OF FABRIC, COLORED PLASTIC, PAINTED SHEETS OF PLYWOOD OR SIMILAR MATERIALS.
- NON PAINTED MARKERS SHALL BE SECURED TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS. METHODS OF SECURING THE MARKERS SHALL NOT PROTRUDE MORE THAN 3" ABOVE THE PAVEMENT STRUCTURE

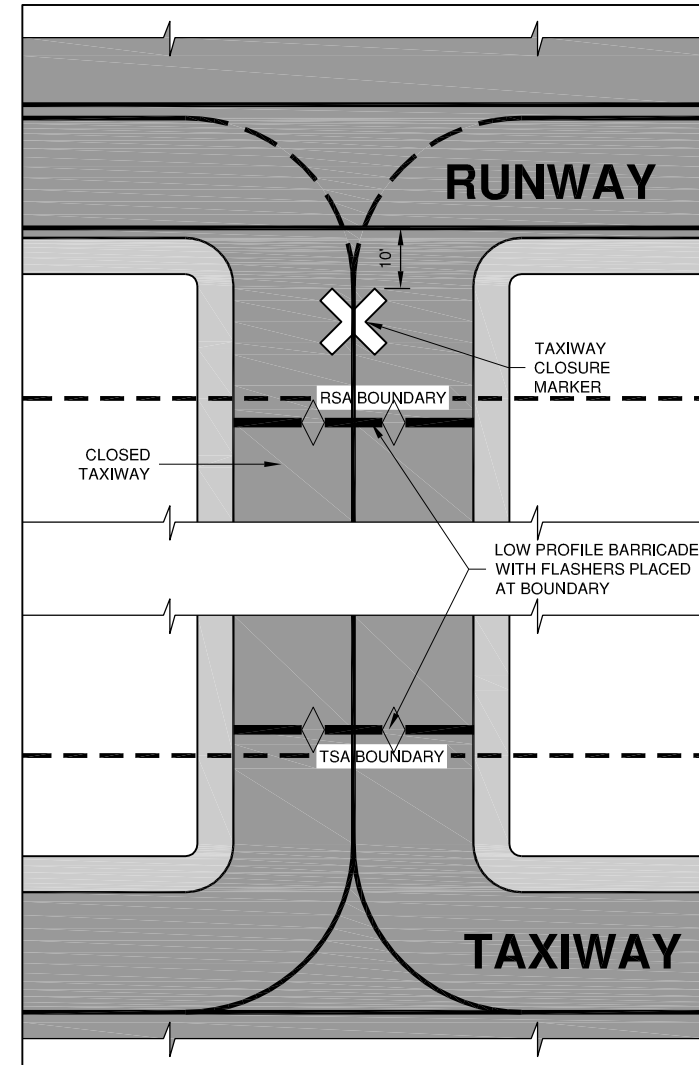


LIGHTED RUNWAY CLOSURE MARKER

N.T.S.

NOTES

- TO BE PLACED ON PAVEMENT AT THE RUNWAY NUMERALS.
- PAIR OF LIGHTED 'X'S TO BE PROVIDED BY THE AIRPORT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE (FUEL, OIL, LIGHT BULBS) WHEN USED DURING CONSTRUCTION CLOSURES.



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0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).

VERMILION REGIONAL AIRPORT
VERMILION REGIONAL AIRPORT AUTHORITY
DANVILLE, ILLINOIS

REHABILITATE EASTERN APRON; TAXIWAY C
& C1 PAVEMENTS
CONSTRUCTION ACTIVITY PLAN AND DETAILS











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JOB No:	14042-02

IL PROJ. NO. DNV-4305
SBG PROJ. NO. 3-17-SBGP-111

CRITICAL POINT TABLE					
POINT	LATITUDE	LONGITUDE	ELEVATION	HEIGHT ABOVE	TOP ELEVATION
1	N40° 11' 54.88"	W87° 36' 00.95"	669	25'	694
5	N40° 11' 55.75"	W87° 36' 01.28"	669	25'	694

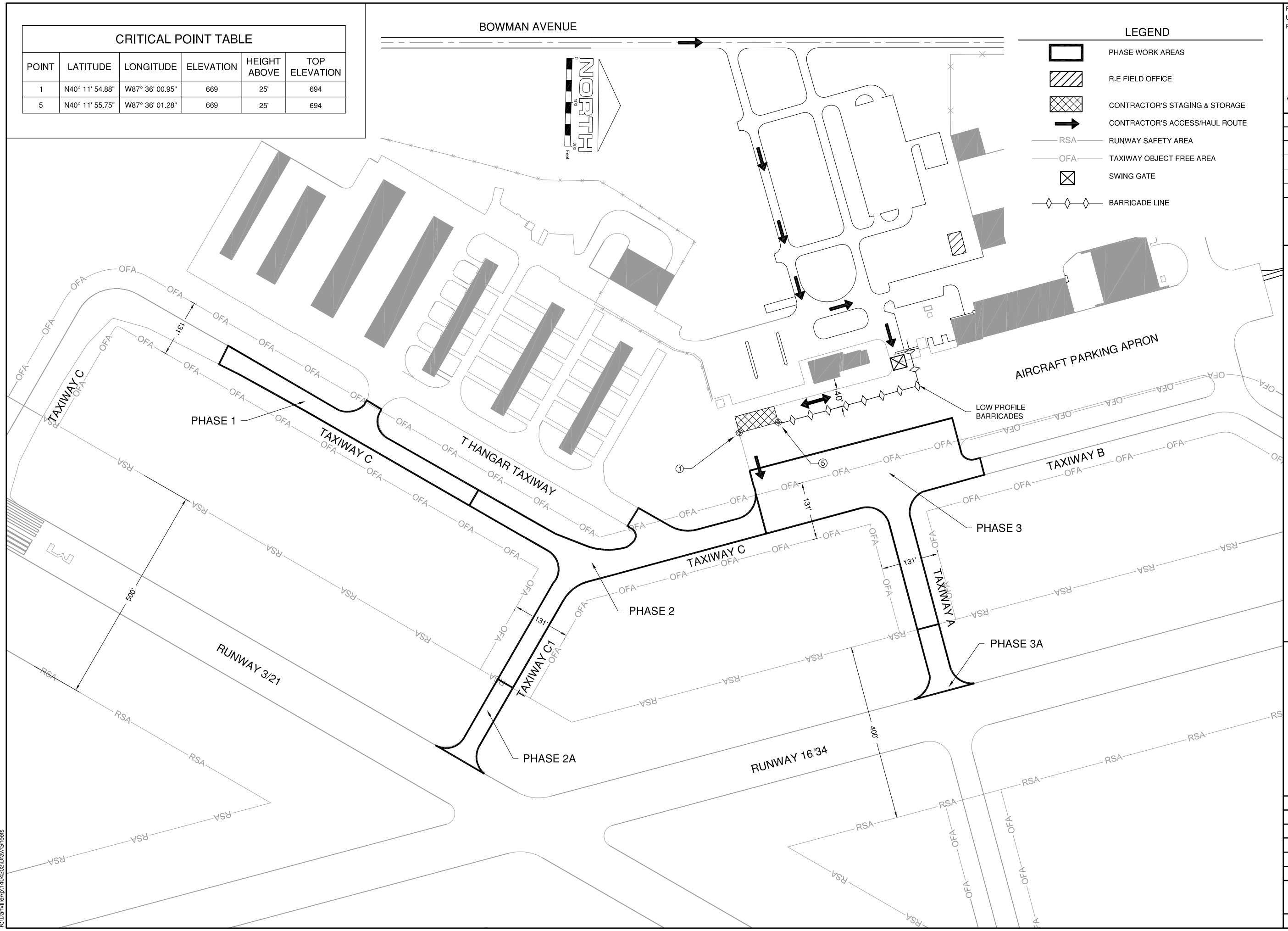
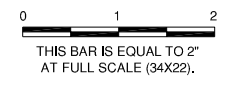
LEGEND

-  PHASE WORK AREAS
-  R.E. FIELD OFFICE
-  CONTRACTOR'S STAGING & STORAGE
-  CONTRACTOR'S ACCESS/HAUL ROUTE
-  RSA — RUNWAY SAFETY AREA
-  OFA — TAXIWAY OBJECT FREE AREA
-  SWING GATE
-  BARRICADE LINE

VE051

REVISIONS

NUMBER	BY	DATE



VERMILION REGIONAL AIRPORT
VERMILION REGIONAL AIRPORT AUTHORITY
DANVILLE, ILLINOIS

REHABILITATE EASTERN APRON; TAXIWAY
C & C1 PAVEMENTS
CONSTRUCTION ACTIVITY PLAN INDEX



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SHEET	05 OF 25 SHEETS

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CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	ELEVATION	HEIGHT ABOVE	TOP ELEVATION
1	N40° 11' 42.79"	W87° 36' 02.45"	658	25'	683
2	N40° 11' 43.42"	W87° 36' 03.92"	661	25'	686
3	N40° 11' 51.06"	W87° 35' 58.38"	659	25'	684
4	N40° 11' 50.41"	W87° 35' 56.90"	659	25'	684
5	N40° 11' 39.03"	W87° 36' 01.30"	656	25'	681

OPEN /CLOSED

TAXIWAY C - TAXIWAY C1 TO RUNWAY 3 - CLOSED
SOUTH T-HANGAR ENTRANCE - CLOSED
RUNWAY 3/21 AND 16/34 - OPEN
TAXIWAY C1 - OPEN
TAXIWAY A - OPEN
NORTH T-HANGAR ENTRANCE - OPEN

LEGEND

	BARRICADE LINE
	CONTRACTOR'S ACCESS
	RUNWAY SAFETY AREA
	OBJECT FREE AREA
	SWING GATE
	RUNWAY CLOSURE
	TAXIWAY CLOSURE

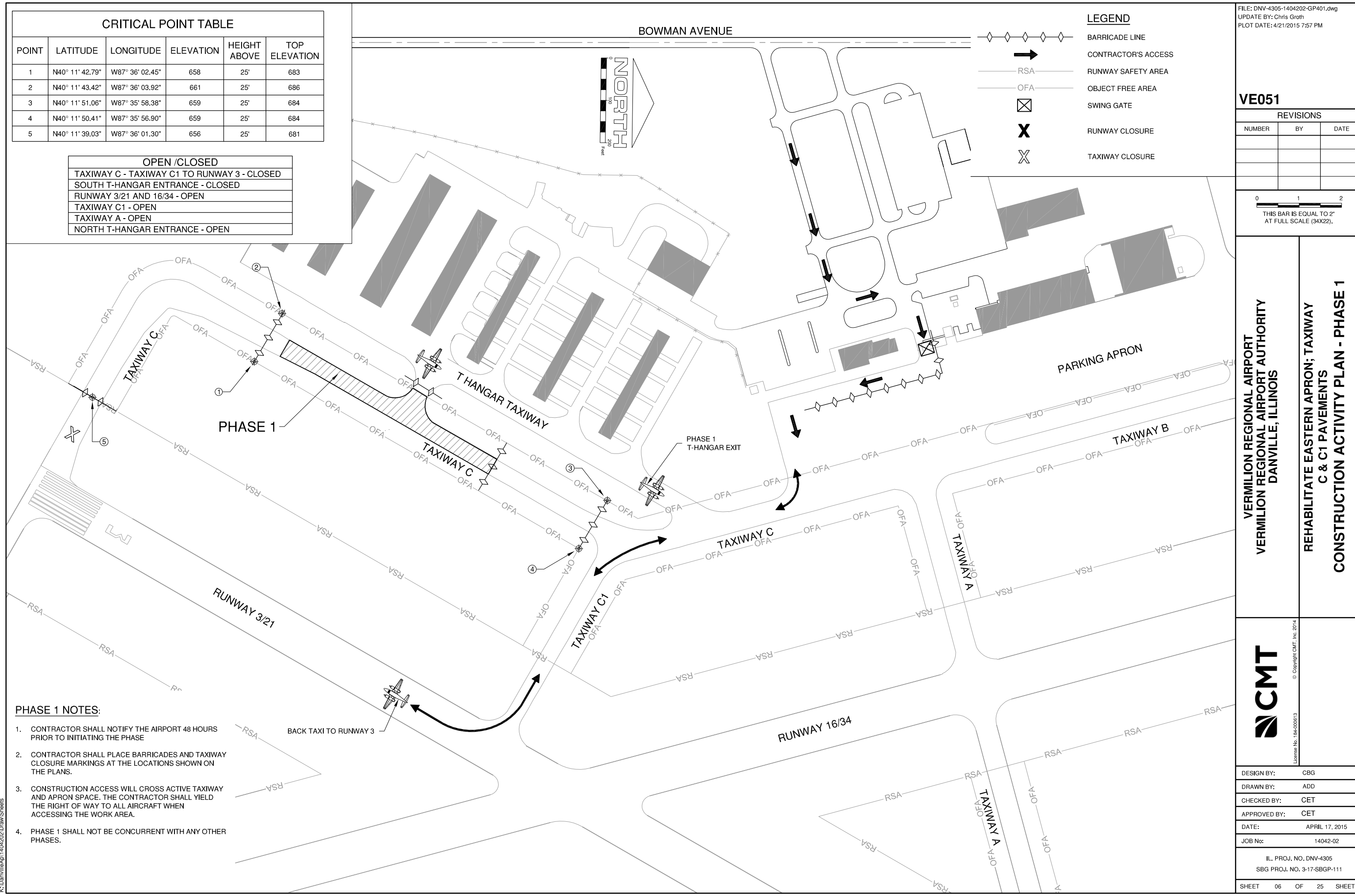
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 UPDATE BY: Chris Groth
 PLOT DATE: 4/21/2015 7:57 PM

VE051

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



PHASE 1 NOTES:

1. CONTRACTOR SHALL NOTIFY THE AIRPORT 48 HOURS PRIOR TO INITIATING THE PHASE
2. CONTRACTOR SHALL PLACE BARRICADES AND TAXIWAY CLOSURE MARKINGS AT THE LOCATIONS SHOWN ON THE PLANS.
3. CONSTRUCTION ACCESS WILL CROSS ACTIVE TAXIWAY AND APRON SPACE. THE CONTRACTOR SHALL YIELD THE RIGHT OF WAY TO ALL AIRCRAFT WHEN ACCESSING THE WORK AREA.
4. PHASE 1 SHALL NOT BE CONCURRENT WITH ANY OTHER PHASES.

VERMILION REGIONAL AIRPORT
 VERMILION REGIONAL AIRPORT AUTHORITY
 DANVILLE, ILLINOIS

REHABILITATE EASTERN APRON; TAXIWAY
 C & C1 PAVEMENTS
 CONSTRUCTION ACTIVITY PLAN - PHASE 1



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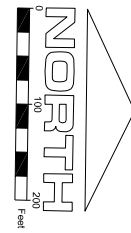
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JOB No:	14042-02
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SHEET	06 OF 25 SHEETS

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CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	ELEVATION	HEIGHT ABOVE	TOP ELEVATION
1	N40° 11' 50.27"	W87° 35' 53.11"	665	25'	690
2	N40° 11' 49.25"	W87° 35' 50.89"	665	25'	690
3	N40° 11' 48.15"	W87° 35' 51.70"	665	25'	690
4	N40° 11' 49.14"	W87° 35' 53.93"	663	25'	688
5	N40° 11' 50.41"	W87° 35' 56.90"	665	25'	690
6	N40° 11' 48.06"	W87° 35' 58.62"	663	25'	688
7	N40° 11' 48.69"	W87° 36' 00.09"	663	25'	688
8	N40° 11' 51.78"	W87° 35' 57.84"	666	25'	691
9	N40° 11' 52.54"	W87° 35' 59.26"	666	25'	691
10	N40° 11' 54.89"	W87° 36' 00.88"	668	25'	693
11	N40° 11' 55.27"	W87° 36' 01.02"	669	25'	694
12	N40° 11' 56.00"	W87° 35' 57.65"	667	25'	692
13	N40° 11' 51.50"	W87° 35' 55.99"	665	25'	690

BOWMAN AVENUE



LEGEND

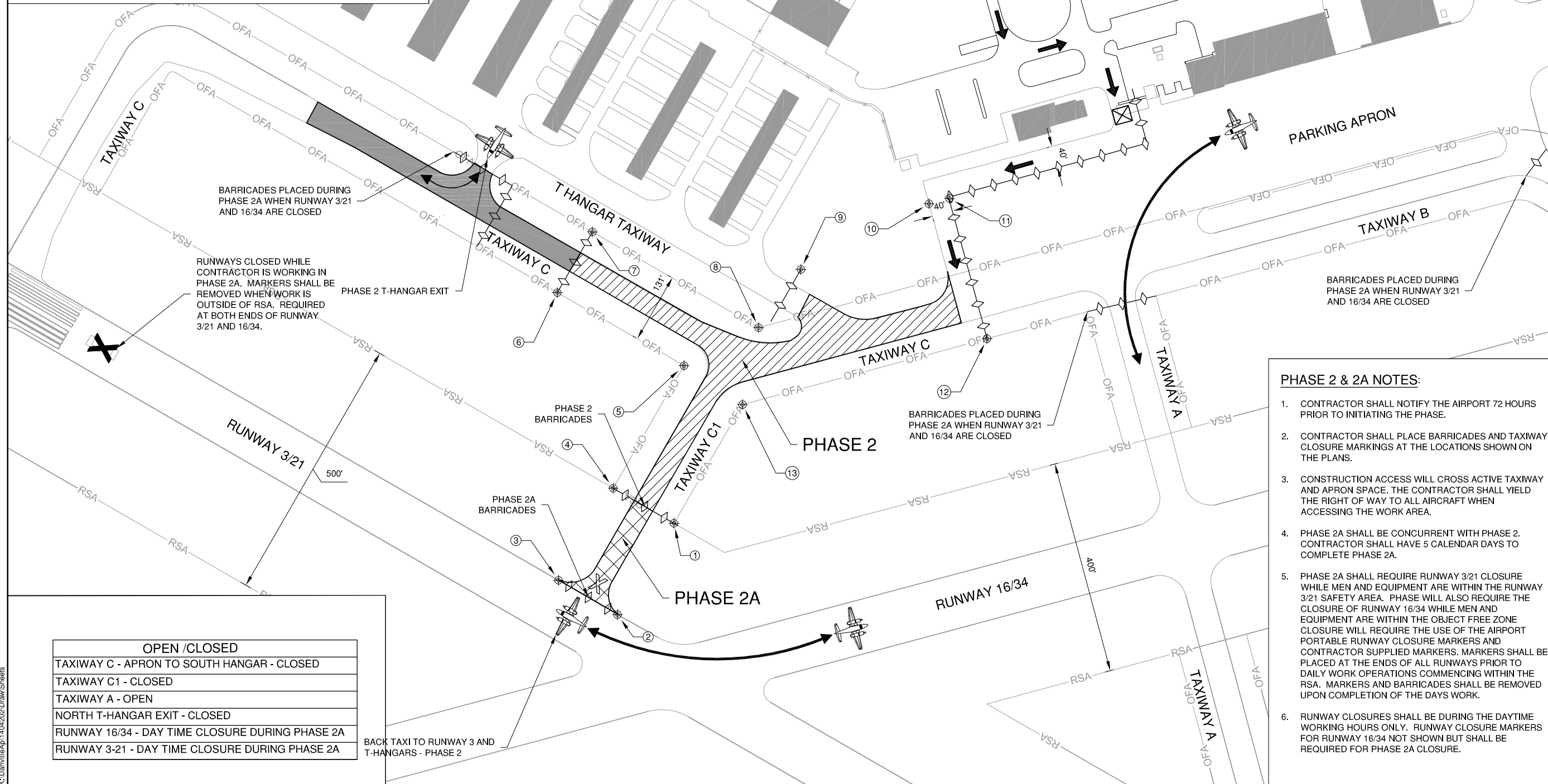
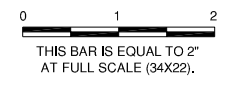
- BARRICADE LINE
- CONTRACTOR'S ACCESS
- RUNWAY SAFETY AREA
- TAXIWAY OBJECT FREE AREA
- SWING GATE
- RUNWAY CLOSURE
- TAXIWAY CLOSURE
- COMPLETED CONSTRUCTION

FILE: DNV-4305-1404202-GP402.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 4/21/2015 7:58 PM

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BARRICADES PLACED DURING PHASE 2A WHEN RUNWAY 3/21 AND 16/34 ARE CLOSED

RUNWAYS CLOSED WHILE CONTRACTOR IS WORKING IN PHASE 2A. MARKERS SHALL BE REMOVED WHEN WORK IS OUTSIDE OF RSA. REQUIRED AT BOTH ENDS OF RUNWAY 3/21 AND 16/34.

PHASE 2 T-HANGAR EXIT

PHASE 2 BARRICADES

BARRICADES PLACED DURING PHASE 2A WHEN RUNWAY 3/21 AND 16/34 ARE CLOSED

BARRICADES PLACED DURING PHASE 2A WHEN RUNWAY 3/21 AND 16/34 ARE CLOSED

PHASE 2 & 2A NOTES:

- CONTRACTOR SHALL NOTIFY THE AIRPORT 72 HOURS PRIOR TO INITIATING THE PHASE.
- CONTRACTOR SHALL PLACE BARRICADES AND TAXIWAY CLOSURE MARKINGS AT THE LOCATIONS SHOWN ON THE PLANS.
- CONSTRUCTION ACCESS WILL CROSS ACTIVE TAXIWAY AND APRON SPACE. THE CONTRACTOR SHALL YIELD THE RIGHT OF WAY TO ALL AIRCRAFT WHEN ACCESSING THE WORK AREA.
- PHASE 2A SHALL BE CONCURRENT WITH PHASE 2. CONTRACTOR SHALL HAVE 5 CALENDAR DAYS TO COMPLETE PHASE 2A.
- PHASE 2A SHALL REQUIRE RUNWAY 3/21 CLOSURE WHILE MEN AND EQUIPMENT ARE WITHIN THE RUNWAY 3/21 SAFETY AREA. PHASE WILL ALSO REQUIRE THE CLOSURE OF RUNWAY 16/34 WHILE MEN AND EQUIPMENT ARE WITHIN THE OBJECT FREE ZONE CLOSURE WILL REQUIRE THE USE OF THE AIRPORT PORTABLE RUNWAY CLOSURE MARKERS AND CONTRACTOR SUPPLIED MARKERS. MARKERS SHALL BE PLACED AT THE ENDS OF ALL RUNWAYS PRIOR TO DAILY WORK OPERATIONS COMMENCING WITHIN THE RSA. MARKERS AND BARRICADES SHALL BE REMOVED UPON COMPLETION OF THE DAYS WORK.
- RUNWAY CLOSURES SHALL BE DURING THE DAYTIME WORKING HOURS ONLY. RUNWAY CLOSURE MARKERS FOR RUNWAY 16/34 NOT SHOWN BUT SHALL BE REQUIRED FOR PHASE 2A CLOSURE.

OPEN /CLOSED

TAXIWAY C - APRON TO SOUTH HANGAR - CLOSED
TAXIWAY C1 - CLOSED
TAXIWAY A - OPEN
NORTH T-HANGAR EXIT - CLOSED
RUNWAY 16/34 - DAY TIME CLOSURE DURING PHASE 2A
RUNWAY 3-21 - DAY TIME CLOSURE DURING PHASE 2A

BACK TAXI TO RUNWAY 3 AND T-HANGARS - PHASE 2

VERMILION REGIONAL AIRPORT
 VERMILION REGIONAL AIRPORT AUTHORITY
 DANVILLE, ILLINOIS

REHABILITATE EASTERN APRON; TAXIWAY
 C & C1 PAVEMENTS
 CONSTRUCTION ACTIVITY PLAN - PHASE 2



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CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	ELEVATION	HEIGHT ABOVE	TOP ELEVATION
1	N40° 11' 59.18"	W87° 35' 55.36"	662	25'	687
2	N40° 12' 01.24"	W87° 35' 45.78"	673	25'	698
3	N40° 11' 46.92"	W87° 35' 48.16"	666	25'	681
4	N40° 11' 53.65"	W87° 35' 50.64"	667	25'	682
5	N40° 11' 57.84"	W87° 35' 58.34"	660	25'	685
6	N40° 11' 55.63"	W87° 35' 57.52"	660	25'	685
7	N40° 11' 55.10"	W87° 35' 59.94"	662	25'	687
8	N40° 11' 59.70"	W87° 36' 01.65"	662	25'	687
9	N40° 12' 00.24"	W87° 35' 59.16"	660	25'	685
10	N40° 11' 59.10"	W87° 35' 58.74"	660	25'	685
11	N40° 11' 36.57"	W87° 35' 55.70"	655	25'	680

LEGEND

- BARRICADE LINE
- CONTRACTOR'S ACCESS
- RSA
- OFA
- SWING GATE
- RUNWAY CLOSURE
- TAXIWAY CLOSURE
- COMPLETED CONSTRUCTION

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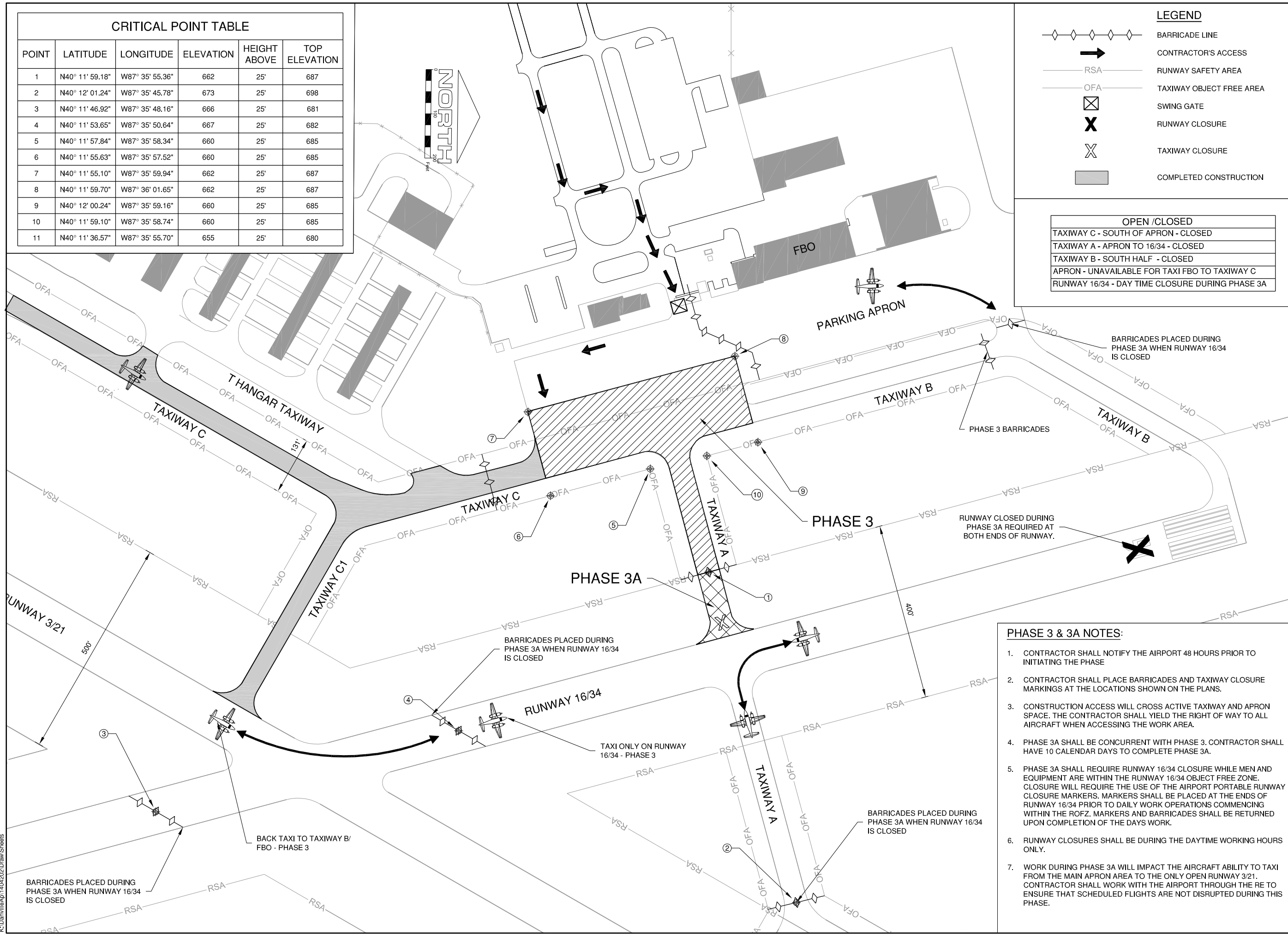
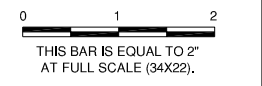
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OPEN /CLOSED

TAXIWAY C - SOUTH OF APRON - CLOSED
TAXIWAY A - APRON TO 16/34 - CLOSED
TAXIWAY B - SOUTH HALF - CLOSED
APRON - UNAVAILABLE FOR TAXI FBO TO TAXIWAY C
RUNWAY 16/34 - DAY TIME CLOSURE DURING PHASE 3A



**VERMILION REGIONAL AIRPORT
 VERMILION REGIONAL AIRPORT AUTHORITY
 DANVILLE, ILLINOIS**

**REHABILITATE EASTERN APRON, TAXIWAY
 C & C1 PAVEMENTS
 CONSTRUCTION ACTIVITY PLAN - PHASE 3**

- PHASE 3 & 3A NOTES:**
- CONTRACTOR SHALL NOTIFY THE AIRPORT 48 HOURS PRIOR TO INITIATING THE PHASE
 - CONTRACTOR SHALL PLACE BARRICADES AND TAXIWAY CLOSURE MARKINGS AT THE LOCATIONS SHOWN ON THE PLANS.
 - CONSTRUCTION ACCESS WILL CROSS ACTIVE TAXIWAY AND APRON SPACE. THE CONTRACTOR SHALL YIELD THE RIGHT OF WAY TO ALL AIRCRAFT WHEN ACCESSING THE WORK AREA.
 - PHASE 3A SHALL BE CONCURRENT WITH PHASE 3. CONTRACTOR SHALL HAVE 10 CALENDAR DAYS TO COMPLETE PHASE 3A.
 - PHASE 3A SHALL REQUIRE RUNWAY 16/34 CLOSURE WHILE MEN AND EQUIPMENT ARE WITHIN THE RUNWAY 16/34 OBJECT FREE ZONE. CLOSURE WILL REQUIRE THE USE OF THE AIRPORT PORTABLE RUNWAY CLOSURE MARKERS. MARKERS SHALL BE PLACED AT THE ENDS OF RUNWAY 16/34 PRIOR TO DAILY WORK OPERATIONS COMMENCING WITHIN THE ROFZ. MARKERS AND BARRICADES SHALL BE RETURNED UPON COMPLETION OF THE DAYS WORK.
 - RUNWAY CLOSURES SHALL BE DURING THE DAYTIME WORKING HOURS ONLY.
 - WORK DURING PHASE 3A WILL IMPACT THE AIRCRAFT ABILITY TO TAXI FROM THE MAIN APRON AREA TO THE ONLY OPEN RUNWAY 3/21. CONTRACTOR SHALL WORK WITH THE AIRPORT THROUGH THE RE TO ENSURE THAT SCHEDULED FLIGHTS ARE NOT DISRUPTED DURING THIS PHASE.

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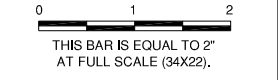
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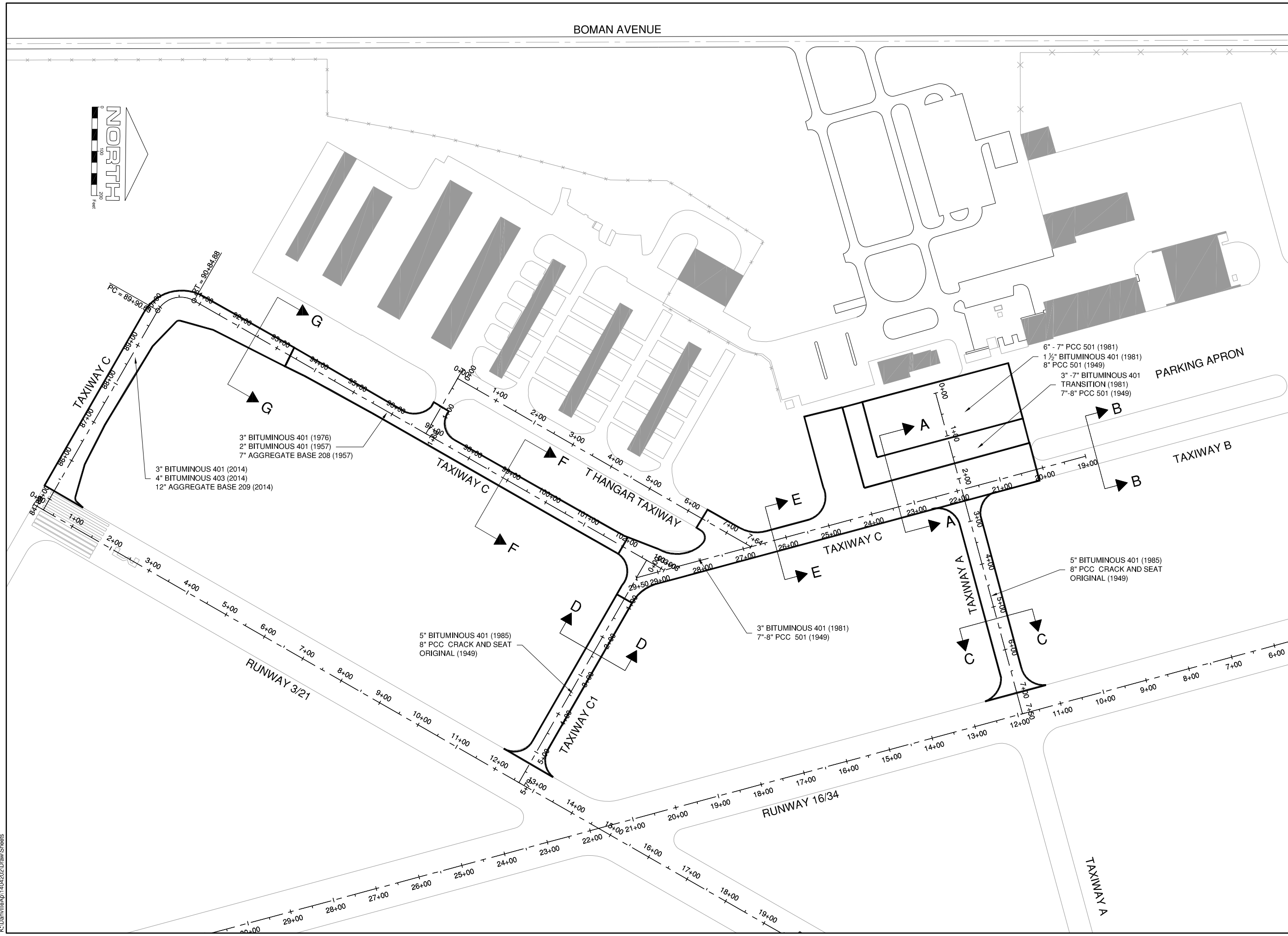


VERMILION REGIONAL AIRPORT
 VERMILION REGIONAL AIRPORT AUTHORITY
 DANVILLE, ILLINOIS

REHABILITATE EASTERN APRON; TAXIWAY
 C & C1 PAVEMENTS
 EXISTING PAVEMENT STRUCTURES



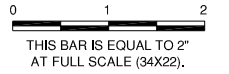
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SHEET	09 OF 25 SHEETS



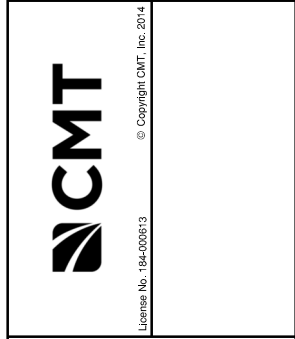
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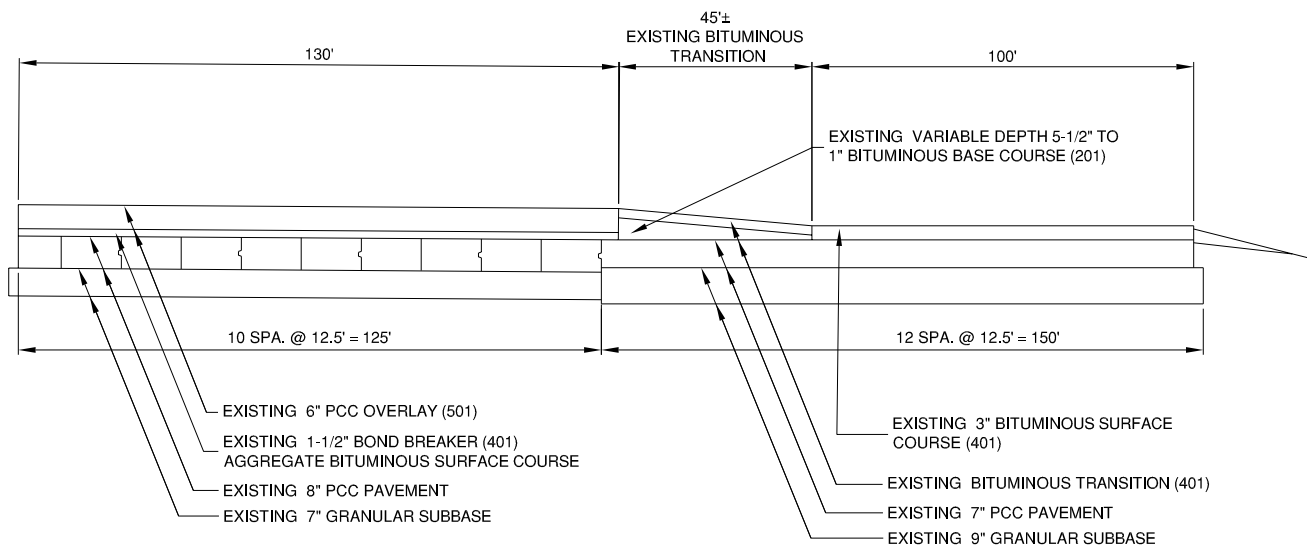
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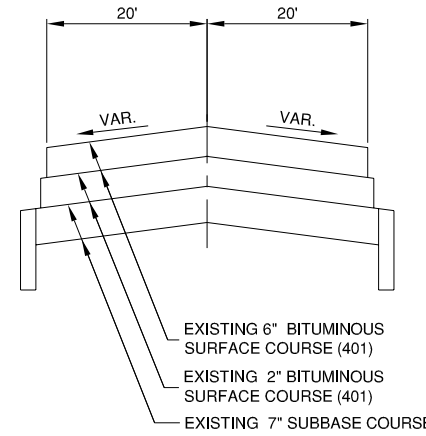
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DANVILLE, ILLINOIS
REHABILITATE EASTERN APRON; TAXIWAY
C & C1 PAVEMENTS
EXISTING TYPICAL SECTIONS



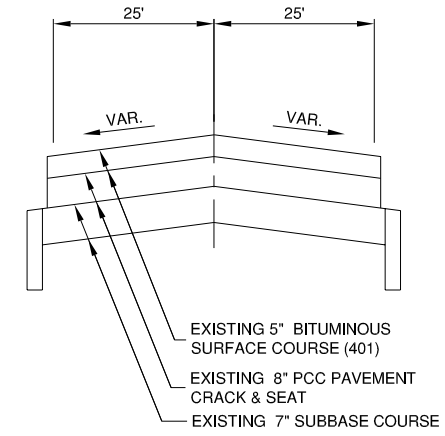
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SHEET	10 OF 25 SHEETS



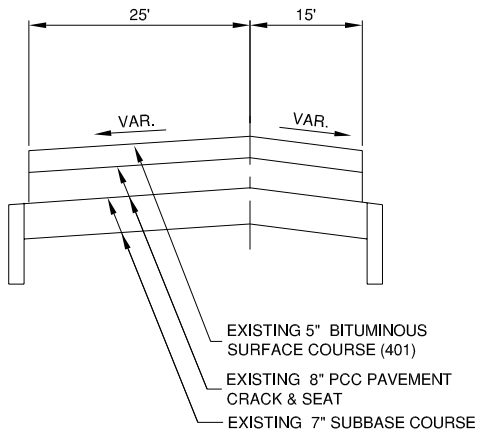
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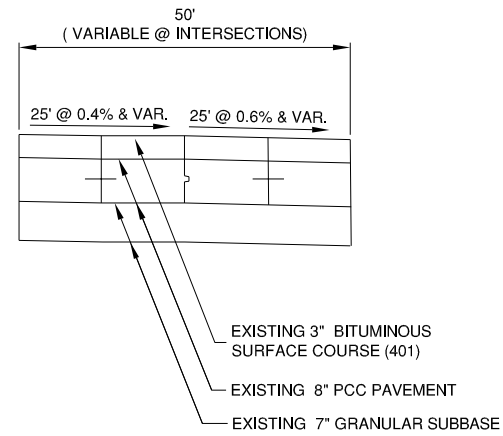
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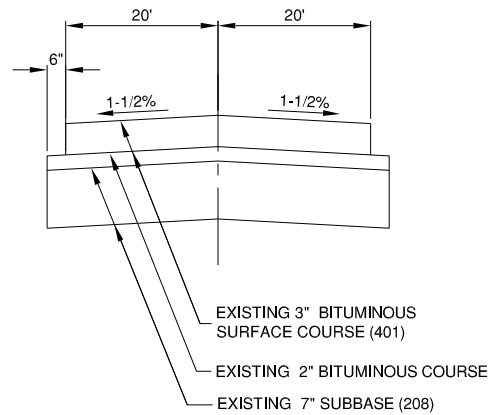
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SECTION C-C
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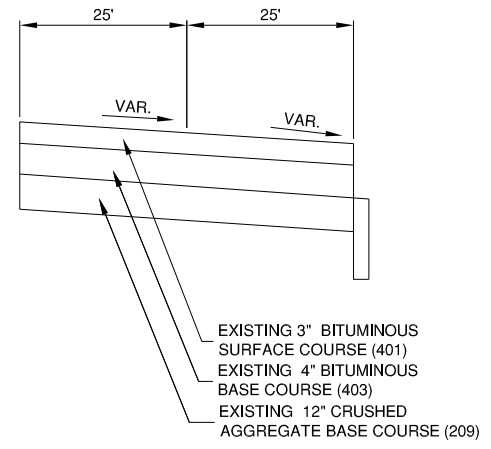
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EXISTING TAXIWAY C NORTH / TAXIWAY C1
SECTION E-E
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EXISTING TAXIWAY C SOUTH
SECTION F-F
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EXISTING TAXIWAY C
SECTION G-G
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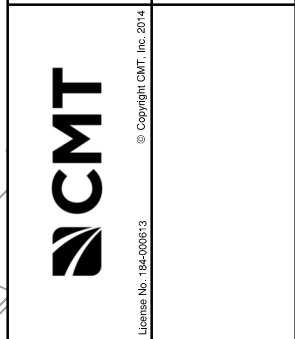
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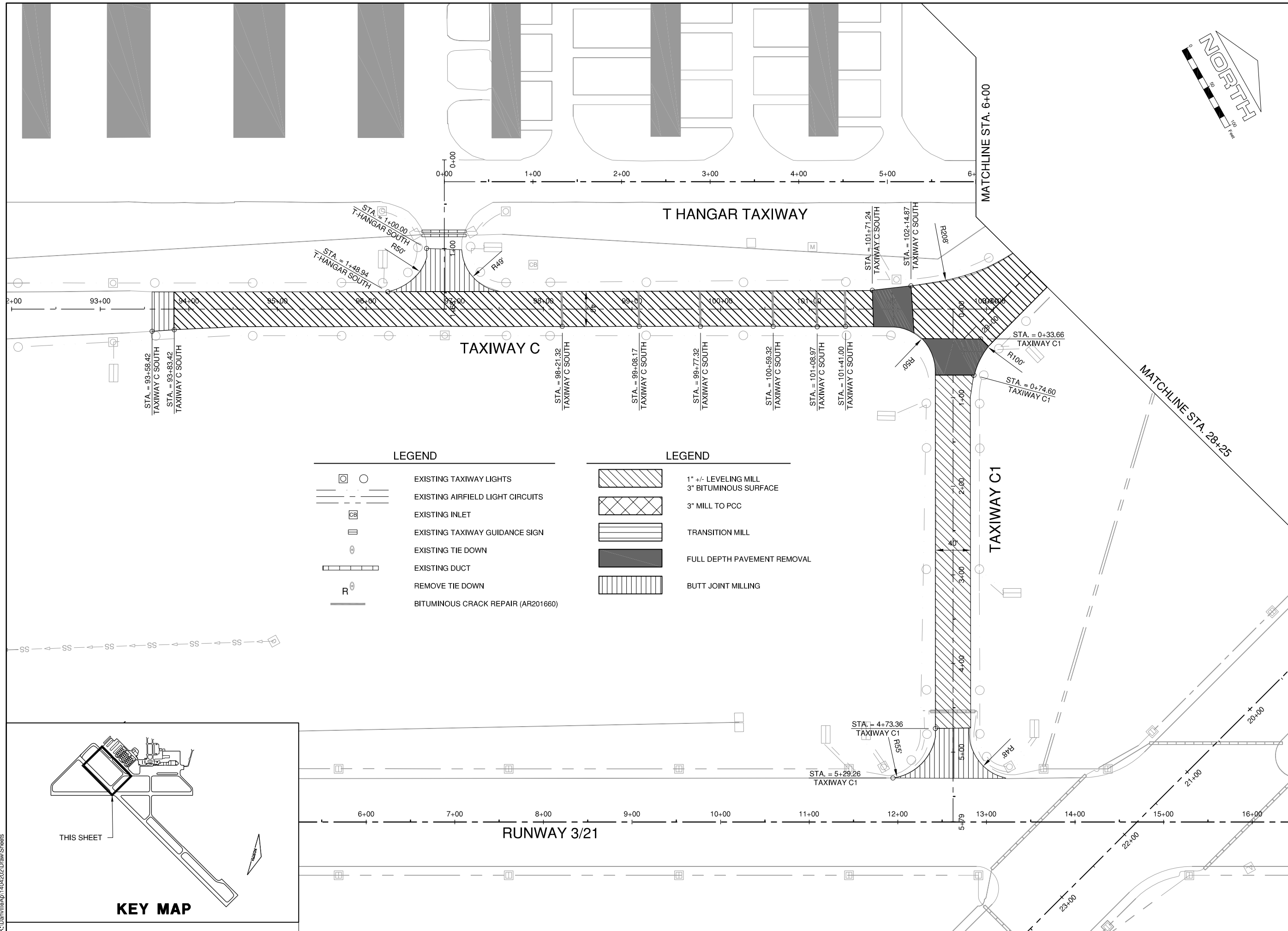
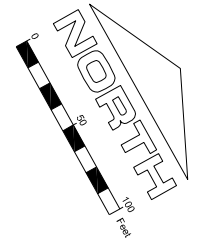
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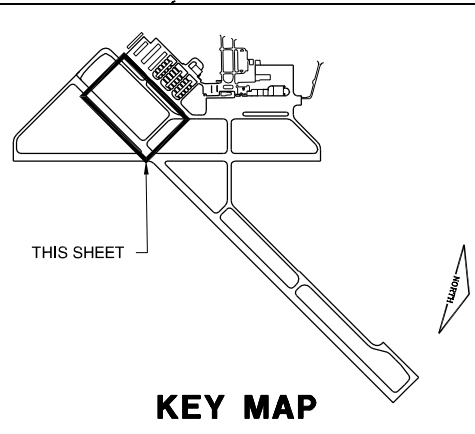
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 C & C1 PAVEMENTS
 EXISTING CONDITIONS AND REMOVALS SHEET 1**



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SHEET	11 OF 25 SHEETS



LEGEND		LEGEND	
	EXISTING TAXIWAY LIGHTS		1" +/- LEVELING MILL
	EXISTING AIRFIELD LIGHT CIRCUITS		3" BITUMINOUS SURFACE
	EXISTING INLET		3" MILL TO PCC
	EXISTING TAXIWAY GUIDANCE SIGN		TRANSITION MILL
	EXISTING TIE DOWN		FULL DEPTH PAVEMENT REMOVAL
	EXISTING DUCT		BUTT JOINT MILLING
	REMOVE TIE DOWN		
	BITUMINOUS CRACK REPAIR (AR201660)		



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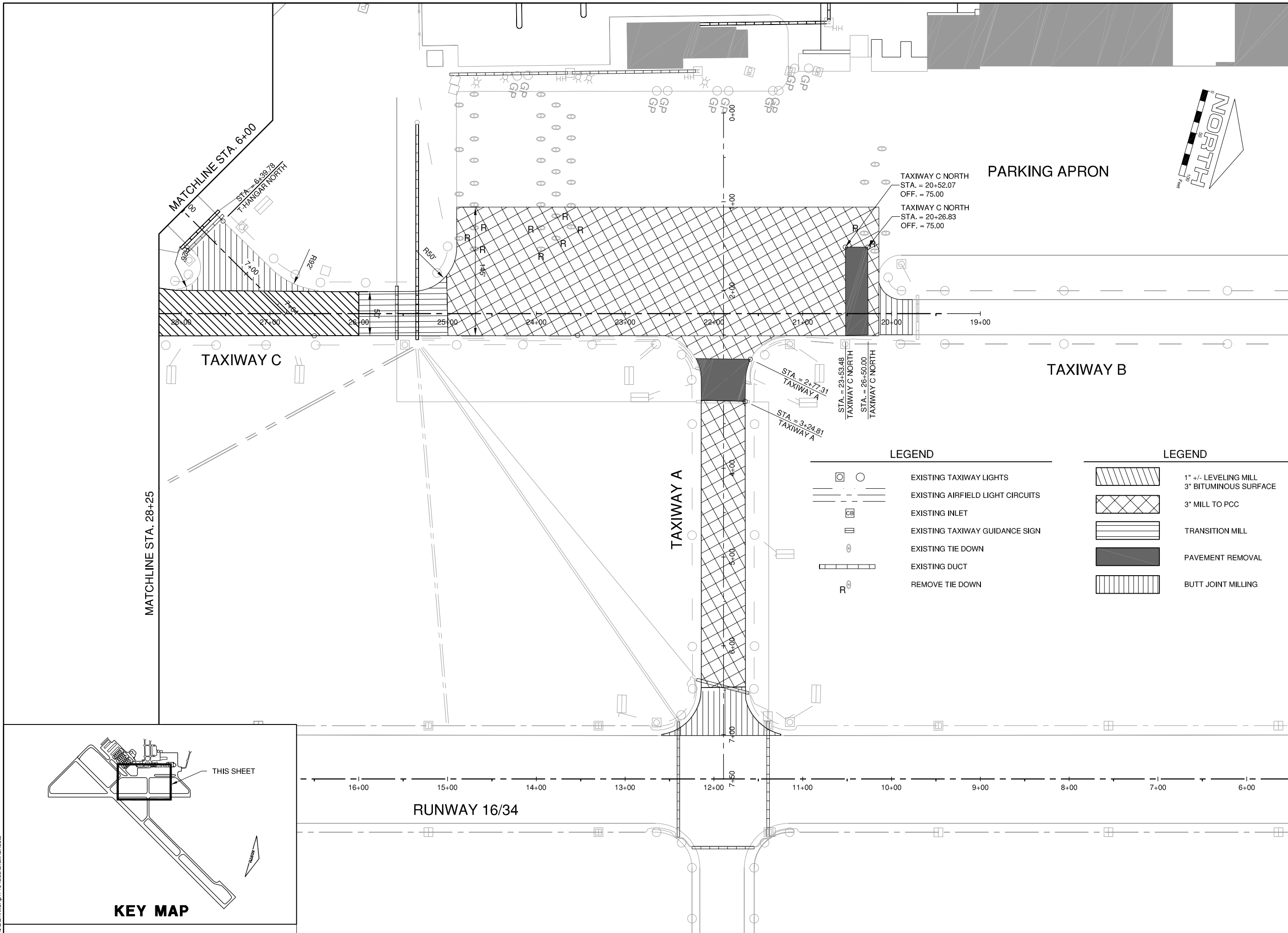
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**REHABILITATE EASTERN APRON; TAXIWAY
 C & C1 PAVEMENTS
 EXISTING CONDITIONS AND REMOVALS SHEET 2**



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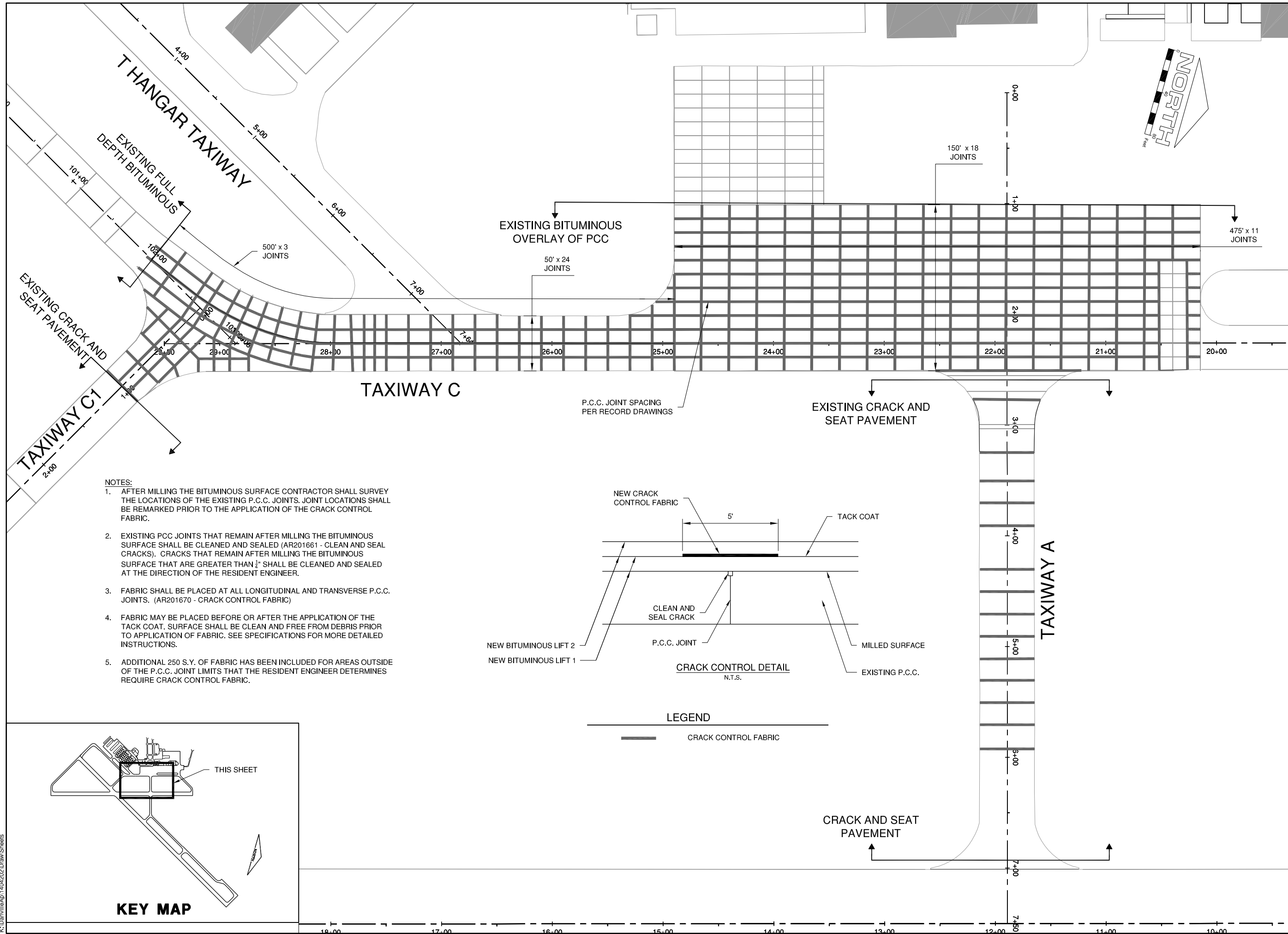
REHABILITATE EASTERN APRON; TAXIWAY
 C & C1 PAVEMENTS
 EXISTING PCC JOINTS



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NOTES:

1. AFTER MILLING THE BITUMINOUS SURFACE CONTRACTOR SHALL SURVEY THE LOCATIONS OF THE EXISTING P.C.C. JOINTS. JOINT LOCATIONS SHALL BE REMARKED PRIOR TO THE APPLICATION OF THE CRACK CONTROL FABRIC.
2. EXISTING PCC JOINTS THAT REMAIN AFTER MILLING THE BITUMINOUS SURFACE SHALL BE CLEANED AND SEALED (AR201661 - CLEAN AND SEAL CRACKS). CRACKS THAT REMAIN AFTER MILLING THE BITUMINOUS SURFACE THAT ARE GREATER THAN 1/4" SHALL BE CLEANED AND SEALED AT THE DIRECTION OF THE RESIDENT ENGINEER.
3. FABRIC SHALL BE PLACED AT ALL LONGITUDINAL AND TRANSVERSE P.C.C. JOINTS. (AR201670 - CRACK CONTROL FABRIC)
4. FABRIC MAY BE PLACED BEFORE OR AFTER THE APPLICATION OF THE TACK COAT. SURFACE SHALL BE CLEAN AND FREE FROM DEBRIS PRIOR TO APPLICATION OF FABRIC. SEE SPECIFICATIONS FOR MORE DETAILED INSTRUCTIONS.
5. ADDITIONAL 250 S.Y. OF FABRIC HAS BEEN INCLUDED FOR AREAS OUTSIDE OF THE P.C.C. JOINT LIMITS THAT THE RESIDENT ENGINEER DETERMINES REQUIRE CRACK CONTROL FABRIC.

LEGEND

— CRACK CONTROL FABRIC

KEY MAP

VE051

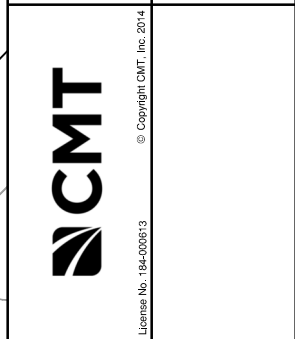
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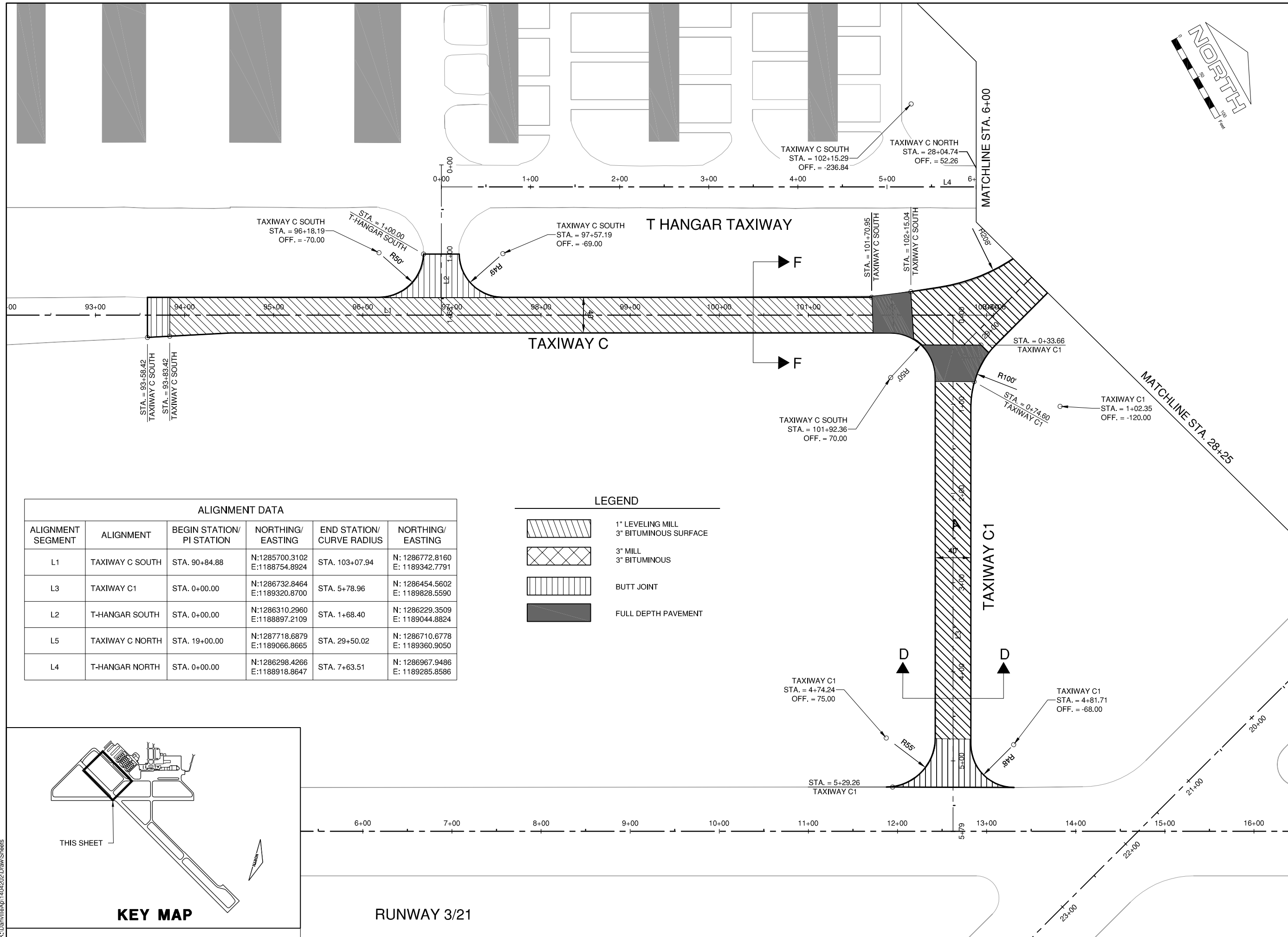
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 DANVILLE, ILLINOIS**

**REHABILITATE EASTERN APRON; TAXIWAY
 C & C1 PAVEMENTS**

PROPOSED IMPROVEMENTS SHEET 1

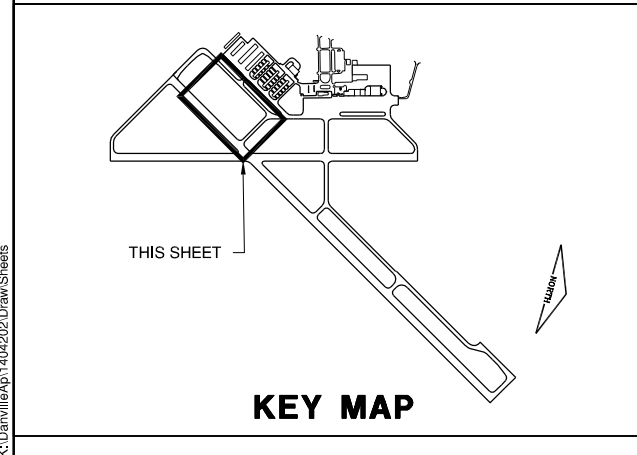


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SBG PROJ. NO. 3-17-SBG-111	
SHEET 14 OF 25 SHEETS	



ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L1	TAXIWAY C SOUTH	STA. 90+84.88	N:1285700.3102 E:1188754.8924	STA. 103+07.94	N: 1286772.8160 E: 1189342.7791
L3	TAXIWAY C1	STA. 0+00.00	N:1286732.8464 E:1189320.8700	STA. 5+78.96	N: 1286454.5602 E: 1189828.5590
L2	T-HANGAR SOUTH	STA. 0+00.00	N:1286310.2960 E:1188897.2109	STA. 1+68.40	N: 1286229.3509 E: 1189044.8824
L5	TAXIWAY C NORTH	STA. 19+00.00	N:1287718.6879 E:1189066.8665	STA. 29+50.02	N: 1286710.6778 E: 1189360.9050
L4	T-HANGAR NORTH	STA. 0+00.00	N:1286298.4266 E:1188918.8647	STA. 7+63.51	N: 1286967.9486 E: 1189285.8586

LEGEND	
	1" LEVELING MILL 3" BITUMINOUS SURFACE
	3" MILL 3" BITUMINOUS
	BUTT JOINT
	FULL DEPTH PAVEMENT



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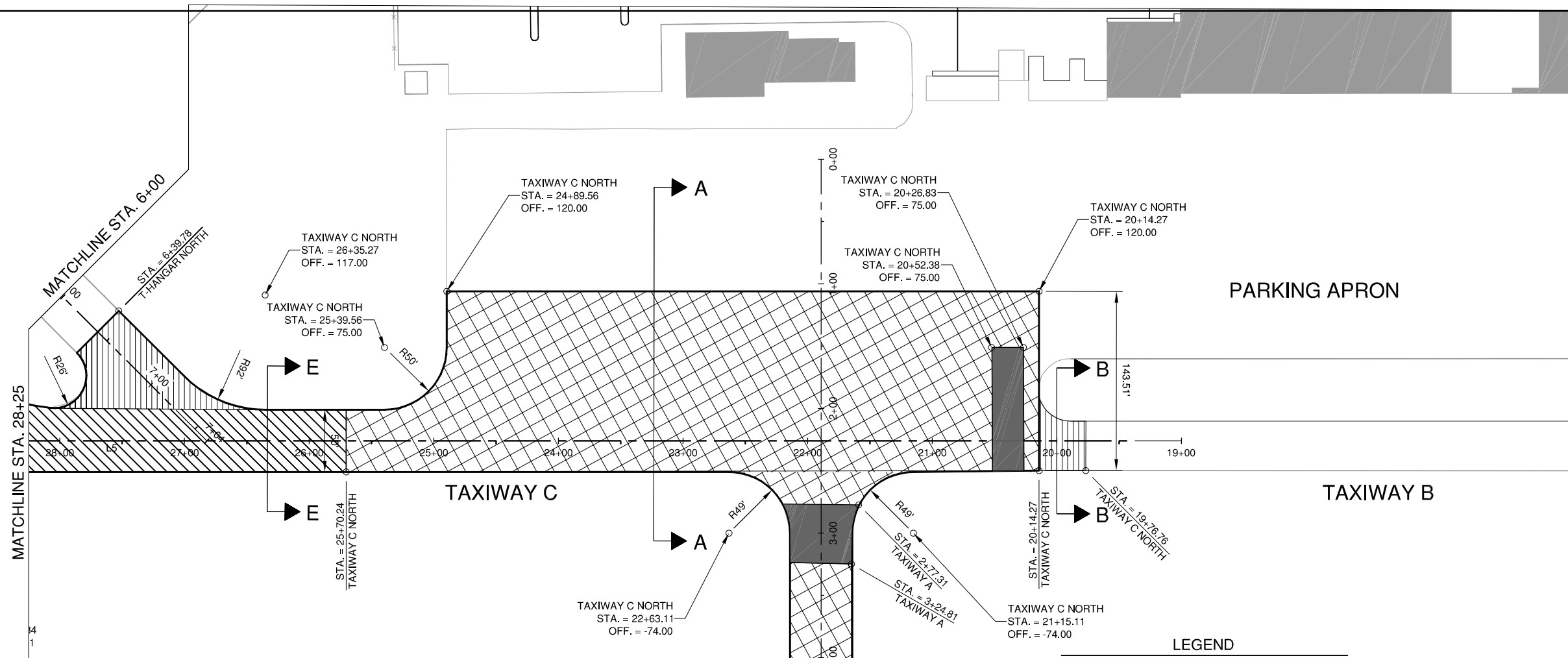
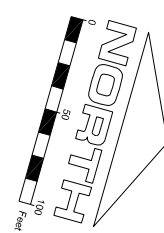
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REHABILITATE EASTERN APRON; TAXIWAY
 C & C1 PAVEMENTS
 PROPOSED IMPROVEMENTS SHEET 2



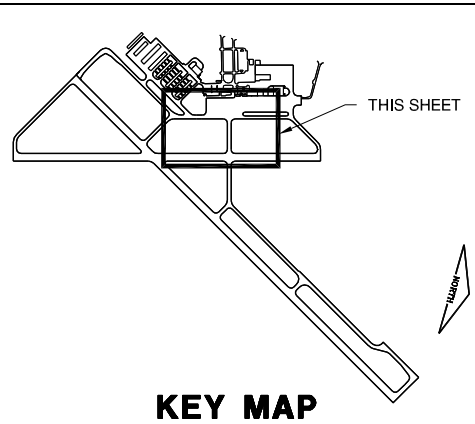
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SHEET	15 OF 25 SHEETS



LEGEND

- 1" LEVELING MILL
3" BITUMINOUS SURFACE
- 3" MILL
3" BITUMINOUS
- BUTT JOINT
- FULL DEPTH PAVEMENT

ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L1	TAXIWAY C SOUTH	STA. 90+84.88	N:1285700.3102 E:1188754.8924	STA. 103+07.94	N: 1286772.8160 E: 1189342.7791
L3	TAXIWAY C1	STA. 0+00.00	N:1286732.8464 E:1189320.8700	STA. 5+78.96	N: 1286454.5602 E: 1189828.5590
L2	T-HANGAR SOUTH	STA. 0+00.00	N:1286310.2960 E:1188897.2109	STA. 1+68.40	N: 1286229.3509 E: 1189044.8824
L5	TAXIWAY C NORTH	STA. 19+00.00	N:1287718.6879 E:1189066.8665	STA. 29+50.02	N: 1286710.6778 E: 1189360.9050
L4	T-HANGAR NORTH	STA. 0+00.00	N:1286298.4266 E:1188918.8647	STA. 7+63.51	N: 1286967.9486 E: 1189285.8586



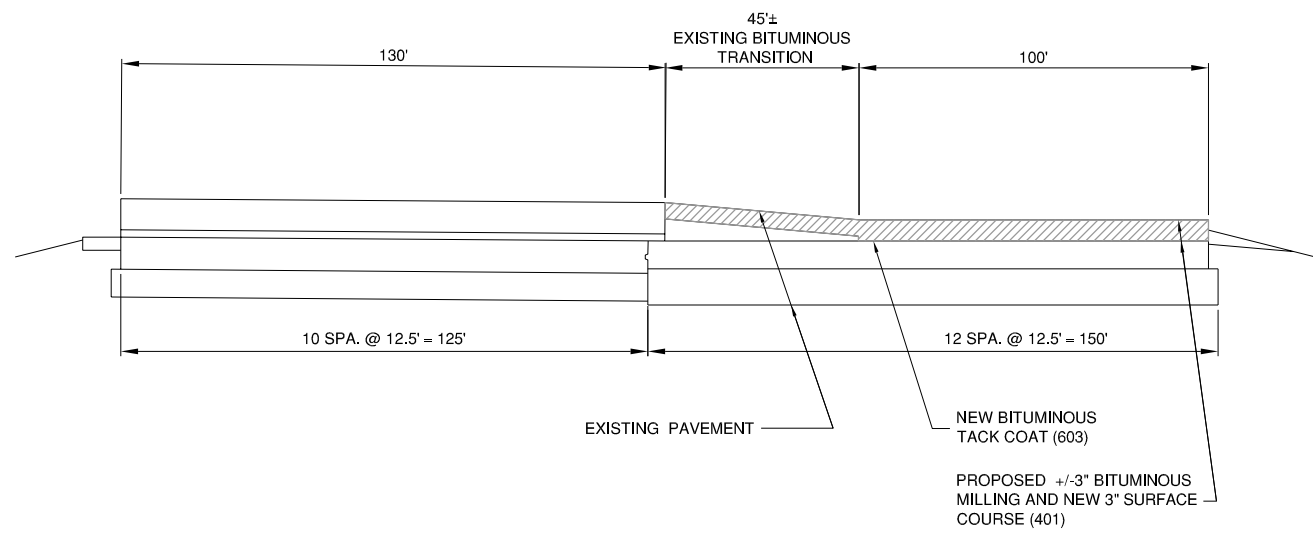
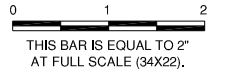
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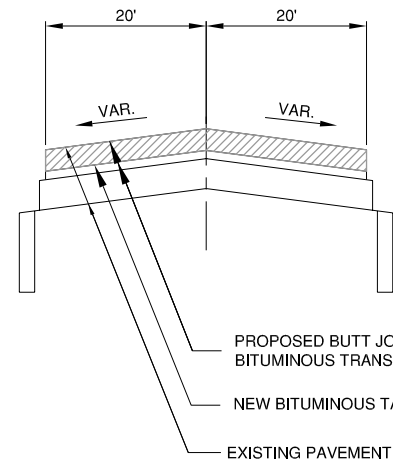
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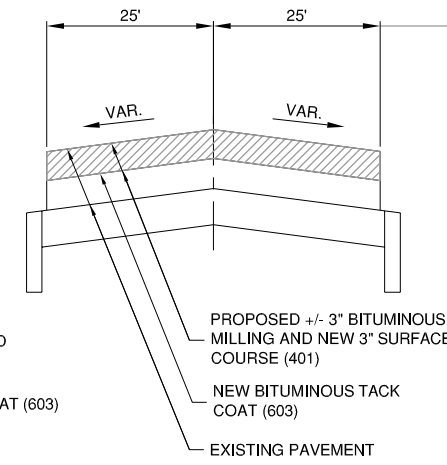
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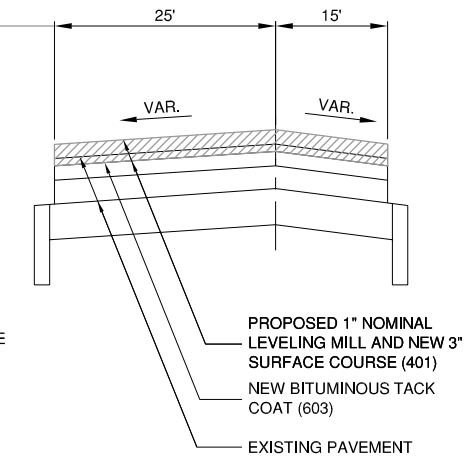
NEW TAXIWAY B SECTION B-B

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NEW TAXIWAY A SECTION C-C

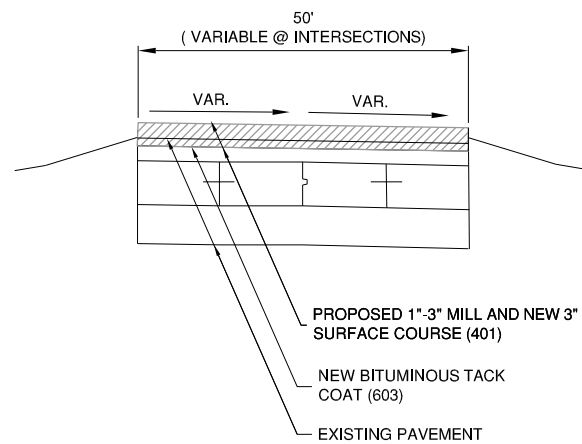
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NEW TAXIWAY C1 SECTION D-D

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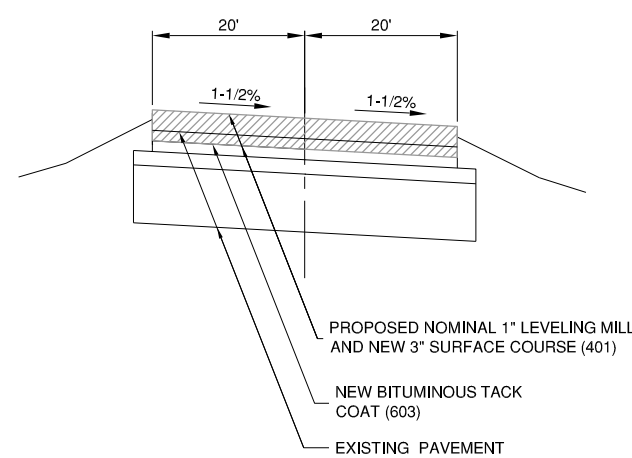
NOTE:
 1. TACK COAT BETWEEN LIFTS OF SURFACE COURSE NOT SHOWN.
 2. BITUMINOUS SURFACE COURSE SHALL BE PAVED IN 2 SEPARATE LIFTS.



NEW TAXIWAY C NORTH / TAXIWAY C1

SECTION E-E

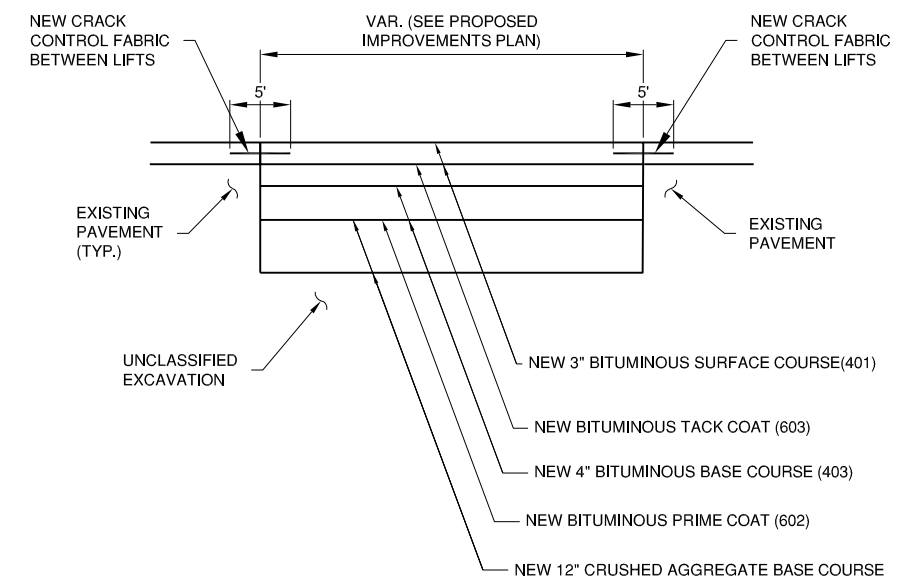
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NEW TAXIWAY C SOUTH / TAXIWAY C1

SECTION F-F

NTS



FULL DEPTH PAVEMENT

NTS

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**REHABILITATE EASTERN APRON; TAXIWAY
 C & C1 PAVEMENTS
 TYPICAL SECTIONS**



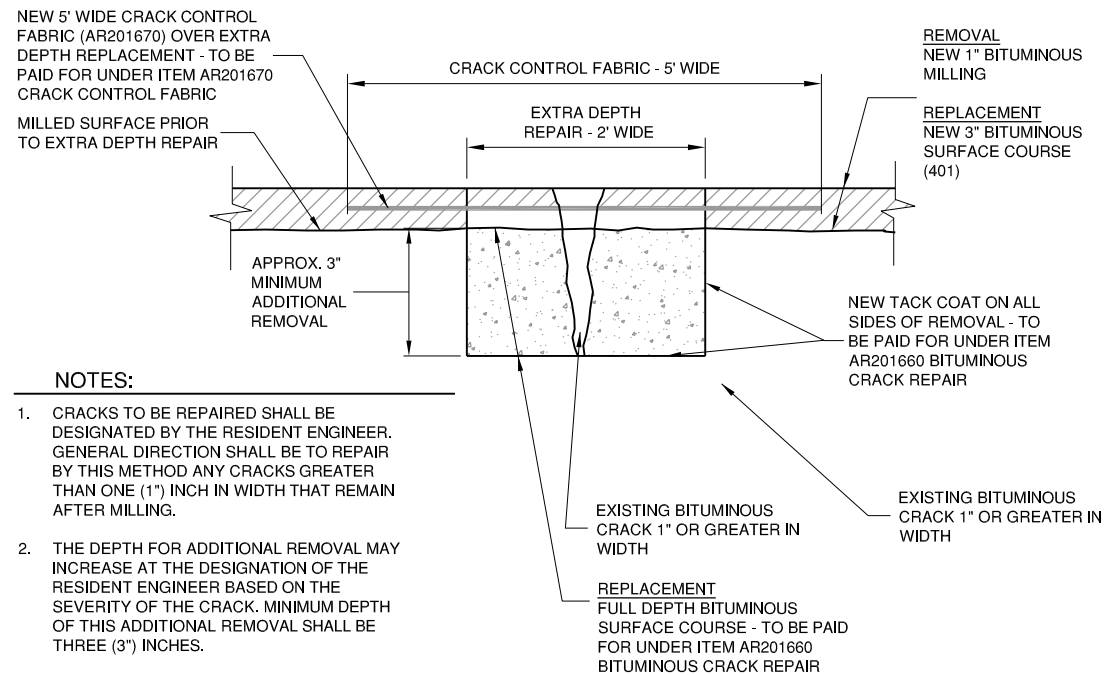
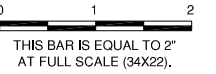
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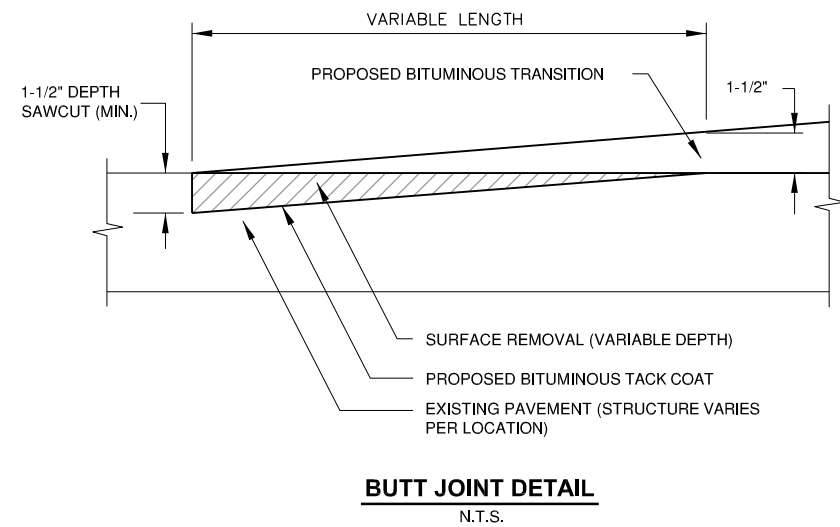
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- NOTES:**
1. CRACKS TO BE REPAIRED SHALL BE DESIGNATED BY THE RESIDENT ENGINEER. GENERAL DIRECTION SHALL BE TO REPAIR BY THIS METHOD ANY CRACKS GREATER THAN ONE (1") INCH IN WIDTH THAT REMAIN AFTER MILLING.
 2. THE DEPTH FOR ADDITIONAL REMOVAL MAY INCREASE AT THE DESIGNATION OF THE RESIDENT ENGINEER BASED ON THE SEVERITY OF THE CRACK. MINIMUM DEPTH OF THIS ADDITIONAL REMOVAL SHALL BE THREE (3") INCHES.
 3. THE CONTRACTOR SHALL REMOVE ALL LOOSE DEBRIS FROM THE EXTRA DEPTH REMOVAL TO THE SATISFACTION OF THE RESIDENT ENGINEER PRIOR TO APPLYING TACK COAT.
 4. PARALLEL CRACKS LOCATED LESS THAN THREE (3) FEET APART SHALL BE CONSIDERED ONE (1) CRACK FOR MEASUREMENT.

BITUMINOUS CRACK REPAIR (AR201660)
 N.T.S.



BUTT JOINT DETAIL
 N.T.S.

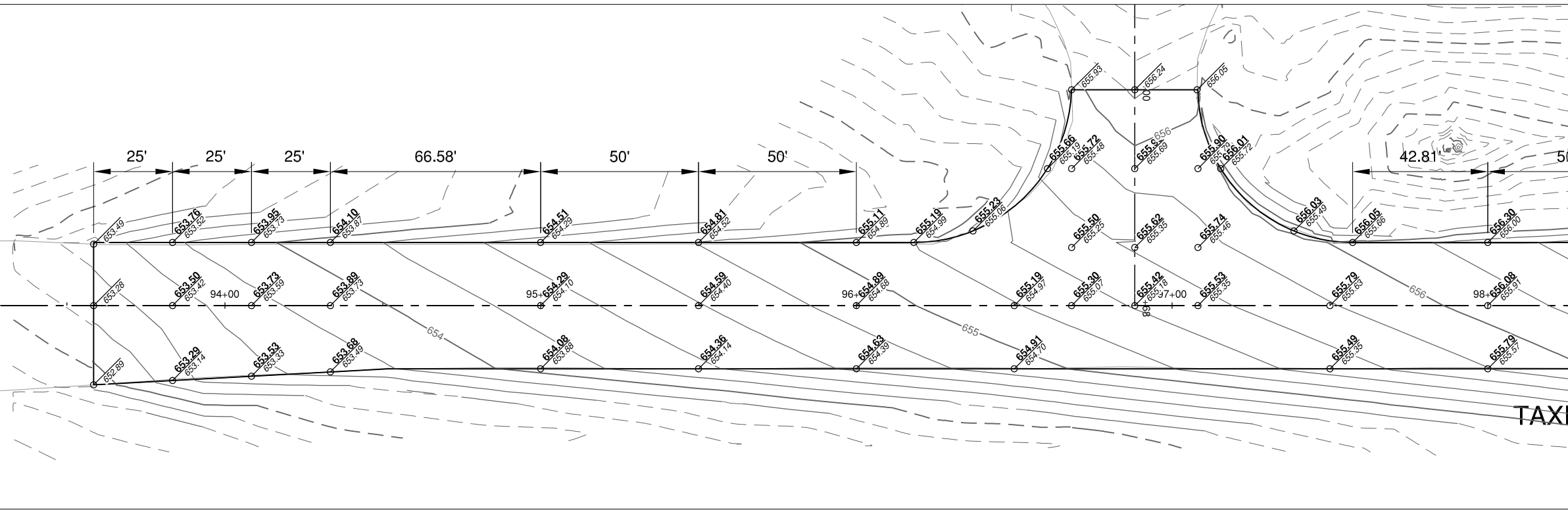
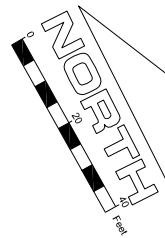
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C & C1 PAVEMENTS
CRACK REPAIR DETAIL



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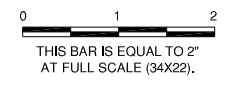
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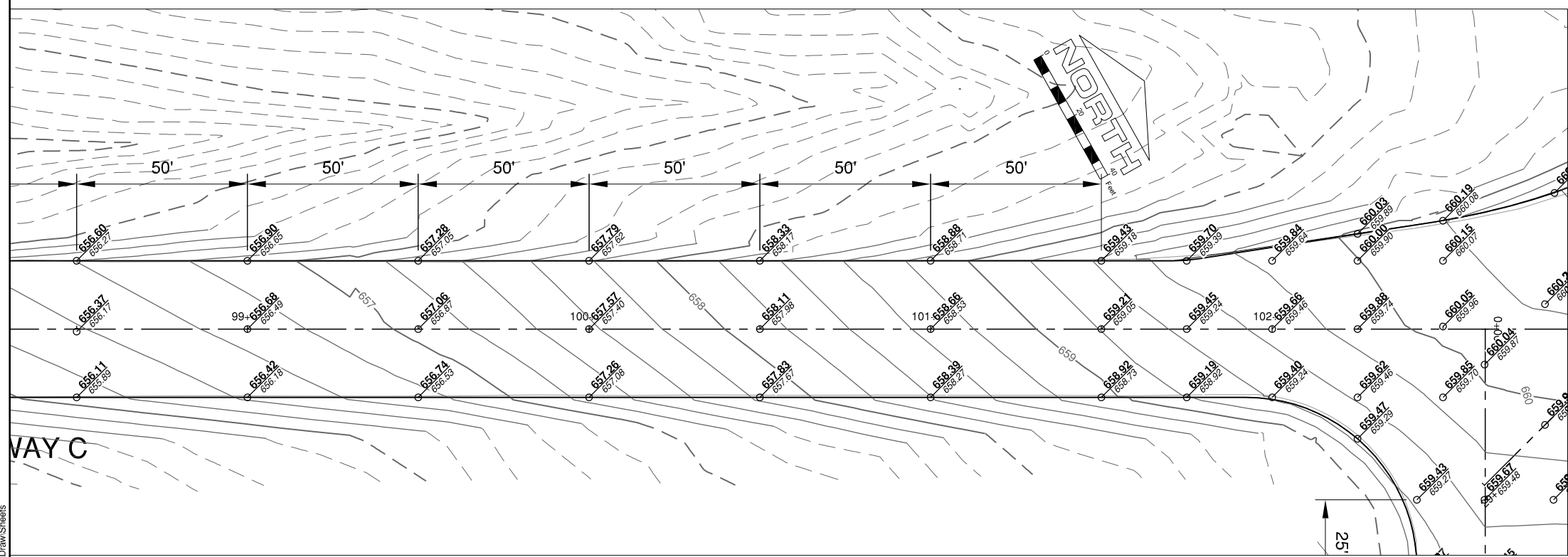
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 STAKING PLAN 1



LEGEND

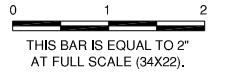
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	PROPOSED CONTOURS
	EXISTING CONTOURS



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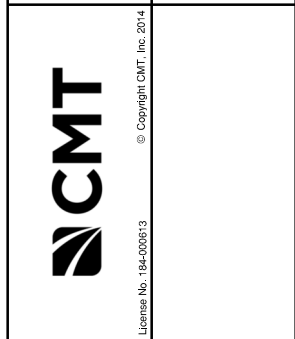
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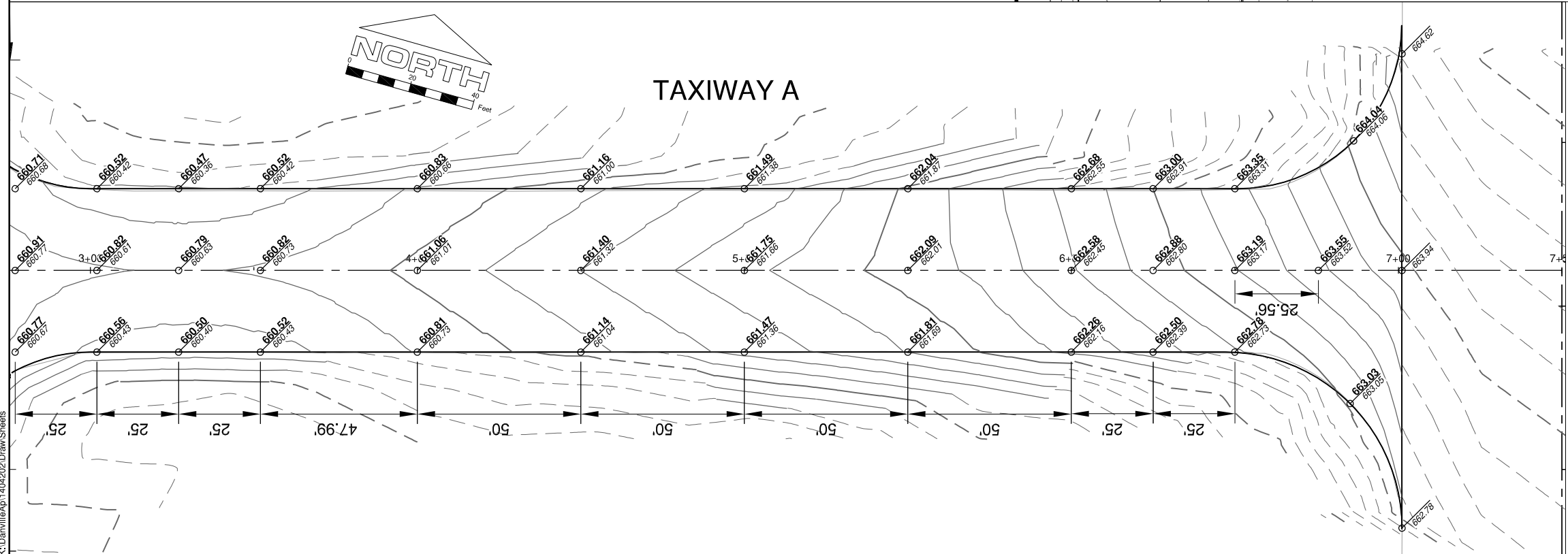
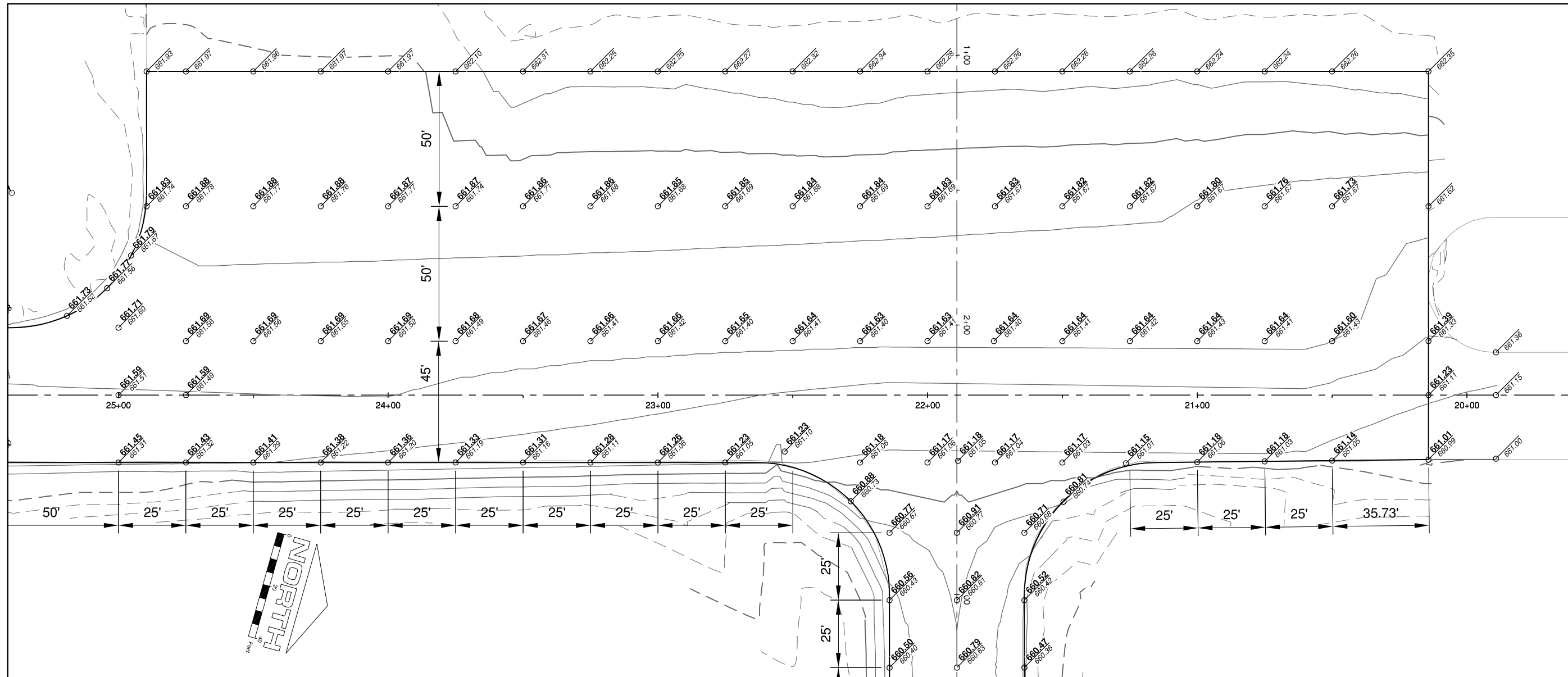


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**REHABILITATE EASTERN APRON; TAXIWAY
 C & C1 PAVEMENTS
 STAKING PLAN 3**



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SHEET	20 OF 25 SHEETS



LEGEND

	PROPOSED ELEVATION
	EXISTING ELEVATION
	PROPOSED CONTOURS
	EXISTING CONTOURS

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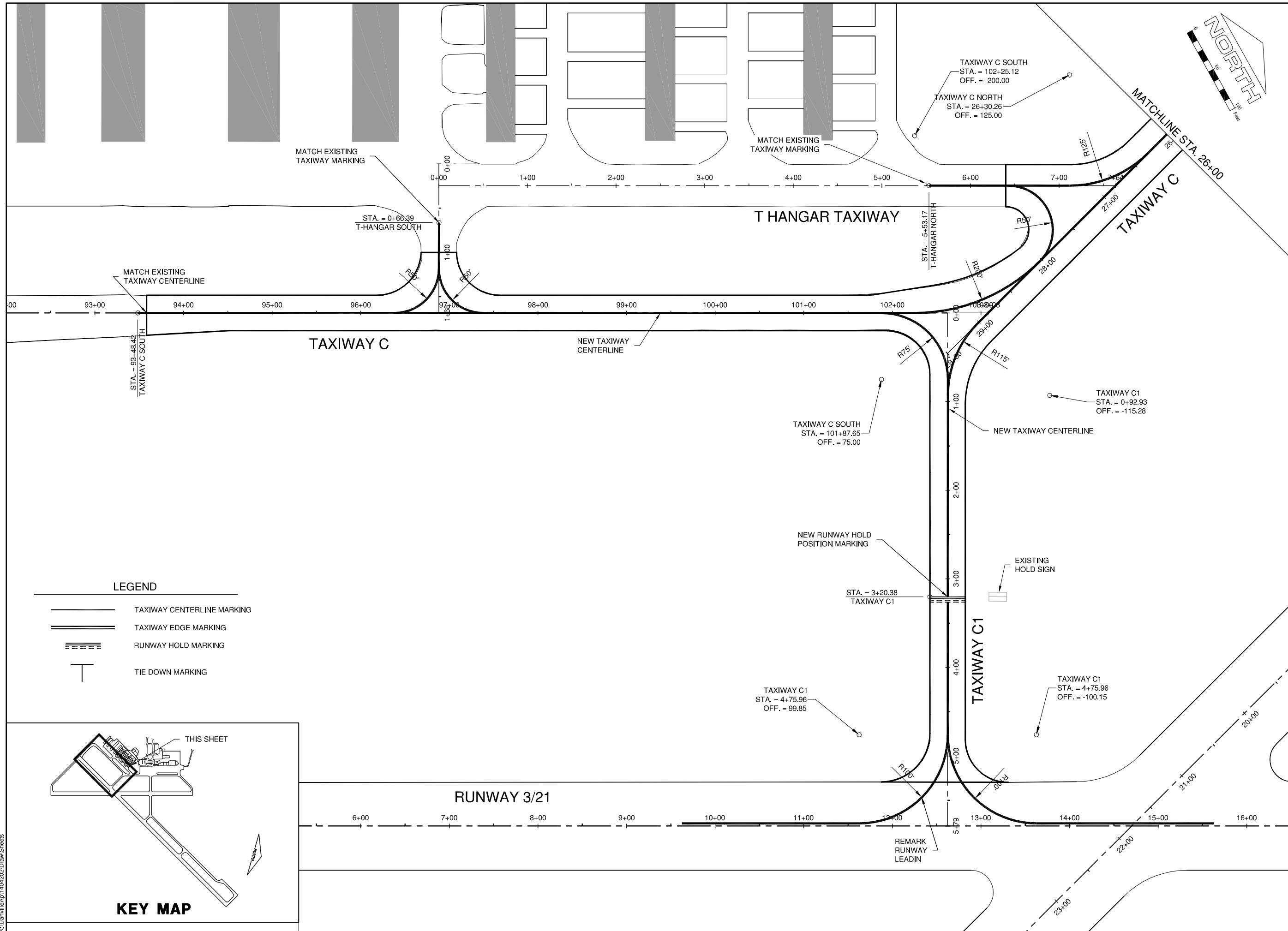
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**REHABILITATE EASTERN APRON; TAXIWAY C & C1 PAVEMENTS
 PAVEMENT MARKING PLAN SHEET 1**



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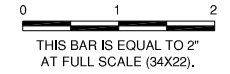
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SHEET	21 OF 25 SHEETS



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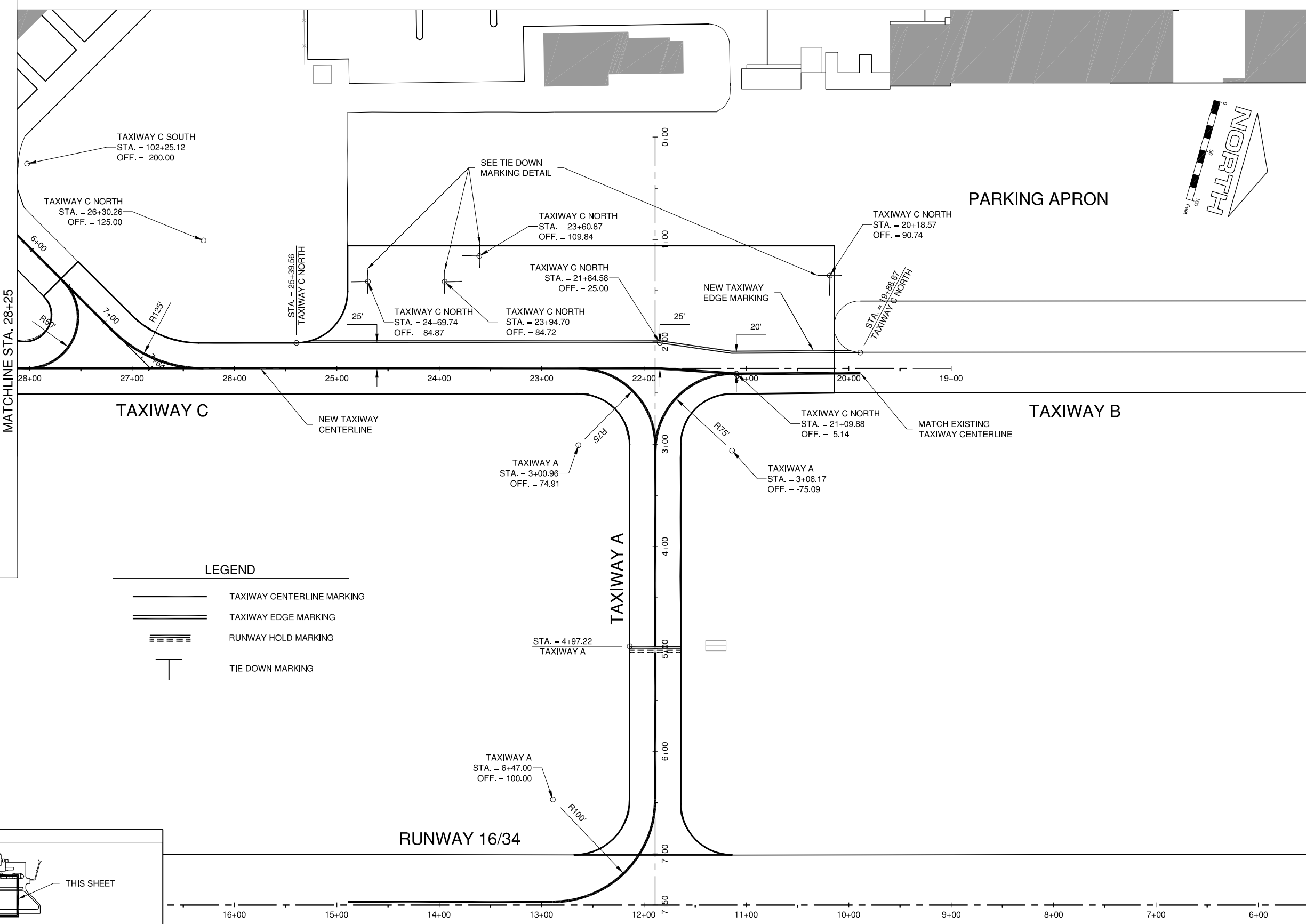
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**REHABILITATE EASTERN APRON; TAXIWAY
 C & C1 PAVEMENTS
 PAVEMENT MARKING PLAN SHEET 2**




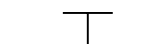


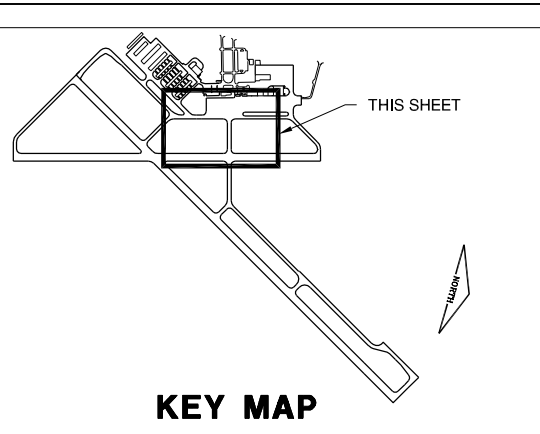
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JOB No:	14042-02
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SHEET	22 OF 25 SHEETS



LEGEND

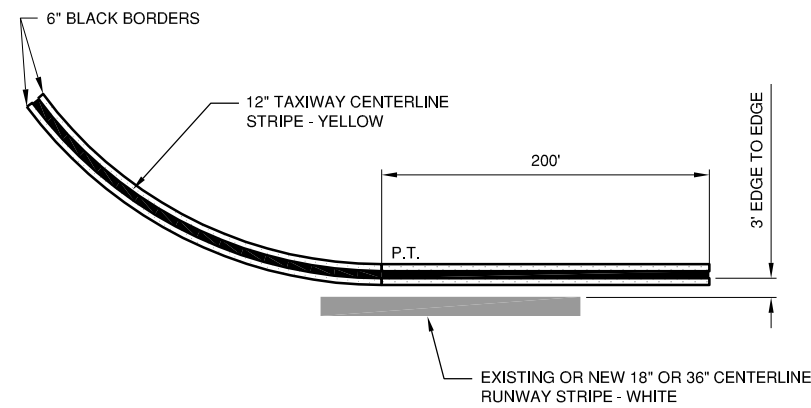
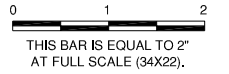
	TAXIWAY CENTERLINE MARKING
	TAXIWAY EDGE MARKING
	RUNWAY HOLD MARKING
	TIE DOWN MARKING



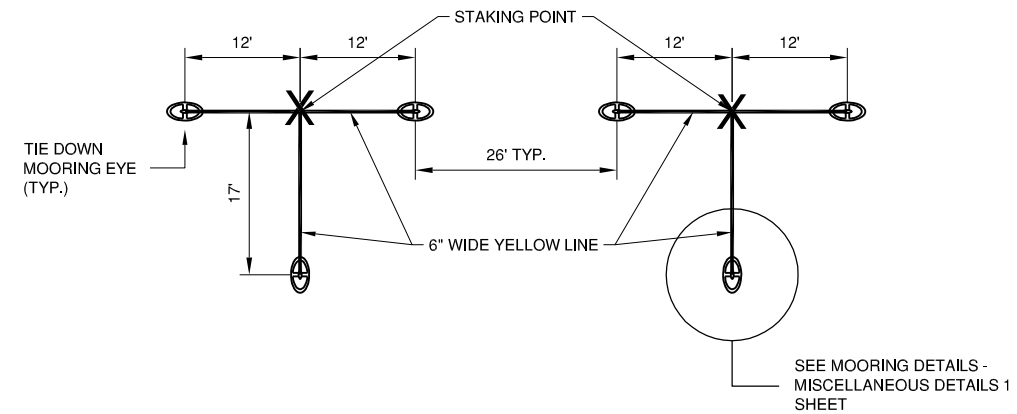
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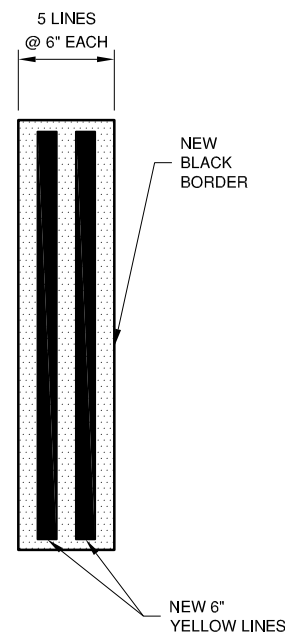
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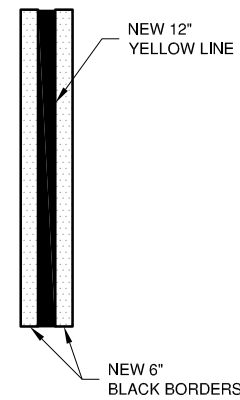
**TAXIWAY CENTERLINE MARKING
 DETAIL (YELLOW)**
 N.T.S.



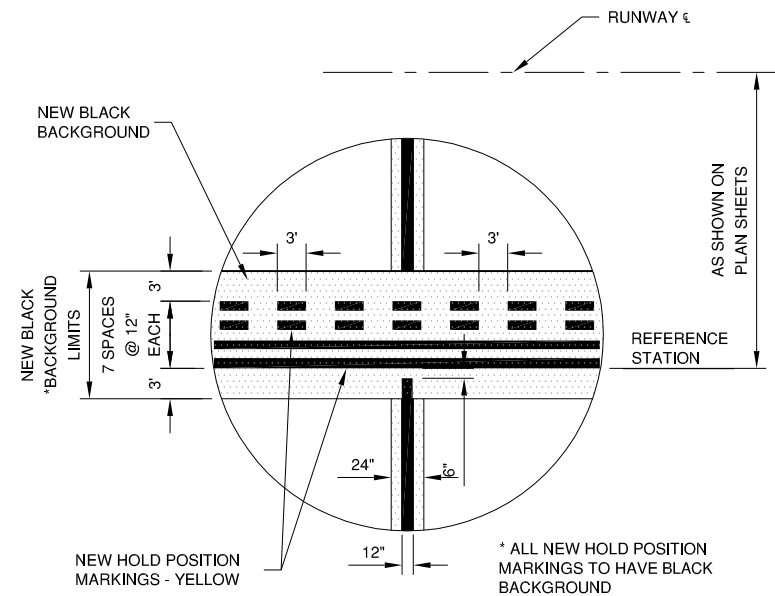
TIE DOWN DETAIL
 N.T.S.



**TAXIWAY EDGE MARKING
 CONTINUOUS**
 N.T.S.



**TAXIWAY CENTERLINE
 CONTINUOUS**
 N.T.S.



HOLD POSITION MARKING DETAIL
 N.T.S.

MARKING NOTE
 ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE BEADS & 6" BLACK BORDER. BLACK BORDER DOES NOT RECEIVE REFLECTIVE BEADS.

**VERMILION REGIONAL AIRPORT
 VERMILION REGIONAL AIRPORT AUTHORITY
 DANVILLE, ILLINOIS**
**REHABILITATE EASTERN APRON; TAXIWAY
 C & C1 PAVEMENTS
 PAVEMENT MARKING DETAIL**

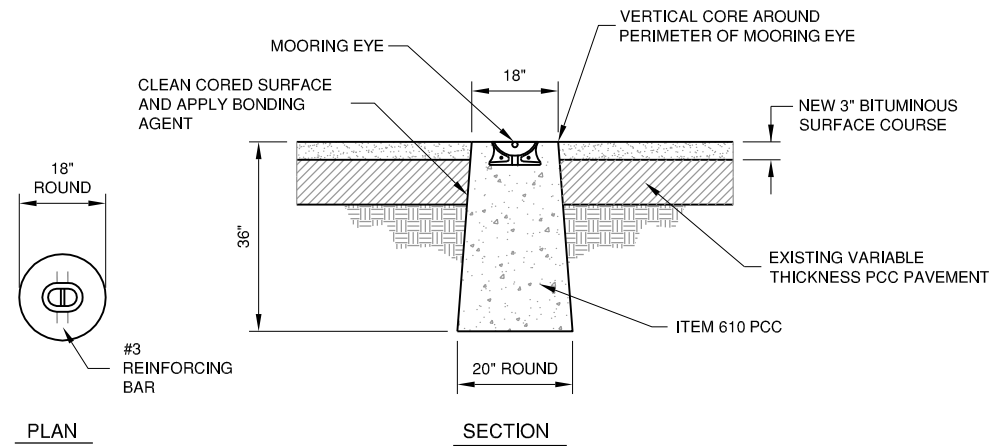
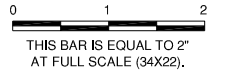


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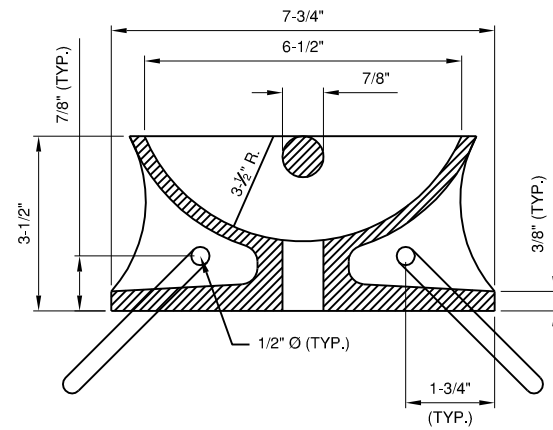
NOTE: THE MOORING EYE SHALL BE INSTALLED AFTER PAVING OPERATIONS ARE COMPLETE.

MOORING EYE IN PAVEMENT DETAIL

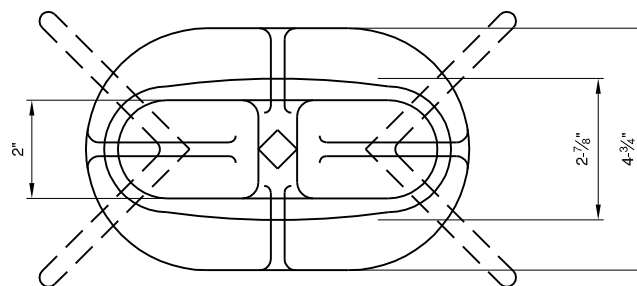
N.T.S.

MOORING NOTES

1. MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. R3490 OR APPROVED EQUIVALENT.
2. ANCHOR RODS SHALL BE #3 DEFORMED REBAR, 15" LONG AND SHALL BE BENT DOWNWARD AT 45 DEGREES.
3. THE MOORING EYE SHALL BE INSTALLED AFTER PAVING OPERATIONS ARE COMPLETE BY CORING THROUGH NEW PAVEMENT.

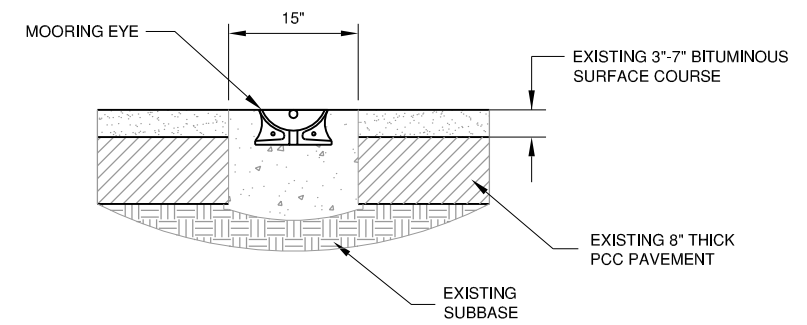


SECTION B-B



MOORING EYE DETAILS

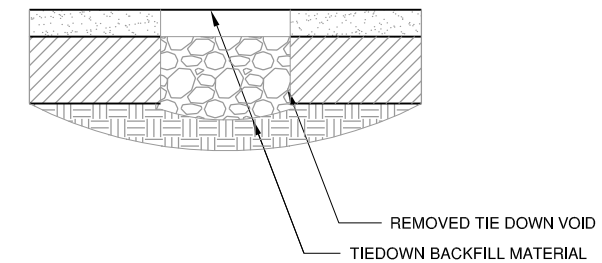
N.T.S.



SECTION

NOTES:

1. CONTRACTOR MAY BACKFILL VOID FROM TIEDOWN REMOVAL WITH SAND OR OTHER MATERIALS APPROVED BY THE RESIDENT ENGINEER. NEW TIEDOWNS WILL BE IN THE SAME LOCATION AS THE EXISTING TIEDOWNS.



TIE DOWN BACKFILL DETAIL

N.T.S.

**VERMILION REGIONAL AIRPORT
 VERMILION REGIONAL AIRPORT AUTHORITY
 DANVILLE, ILLINOIS**

**REHABILITATE EASTERN APRON, TAXIWAY
 C & C1 PAVEMENTS
 TIEDOWN DETAIL**



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LEGEND

- 5' SHOULDER ADJUSTMENT
- 4' KNITTED STRAW MATT
- SEEDING

VE051

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34x22).

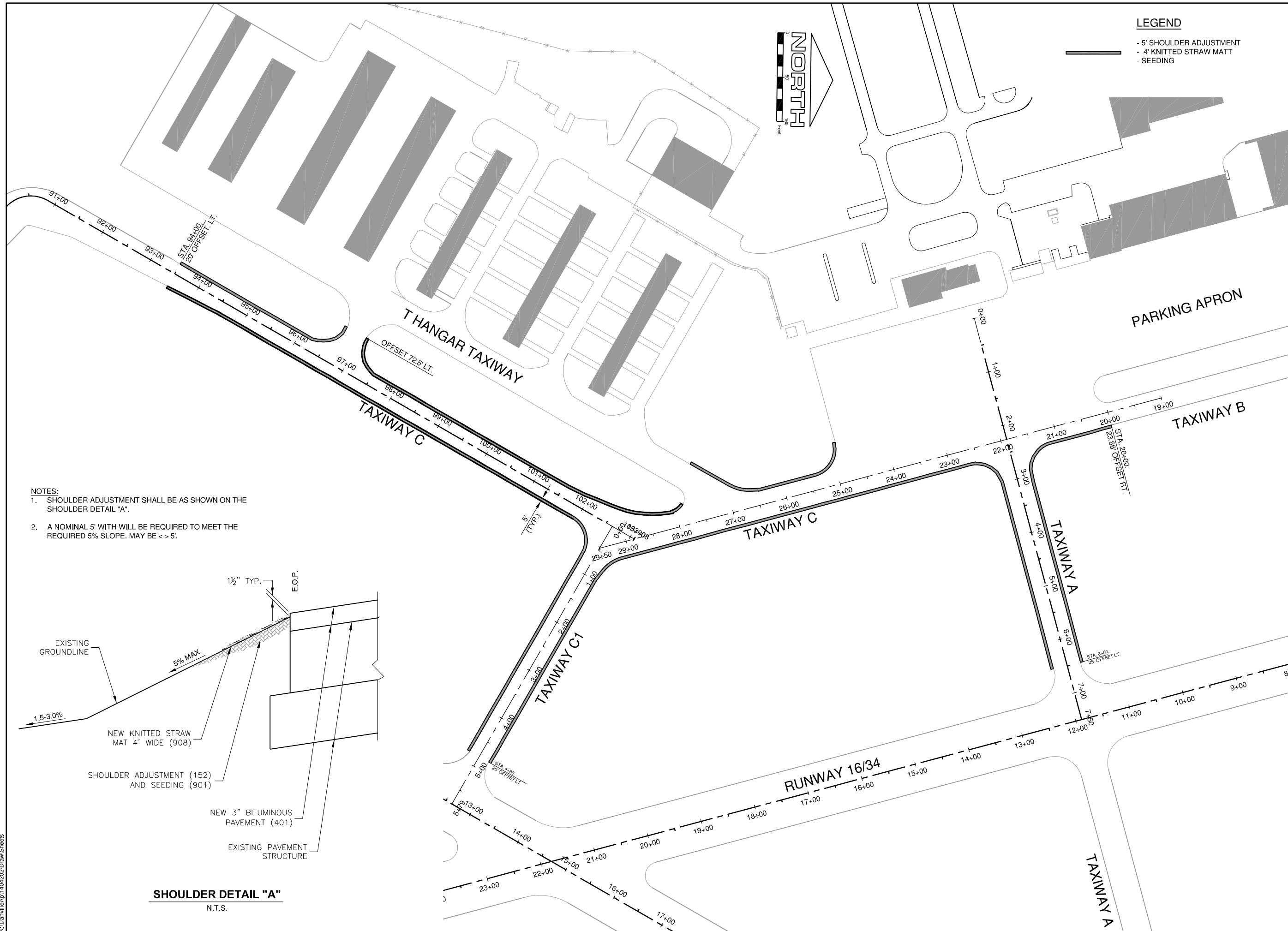
**VERMILION REGIONAL AIRPORT
 VERMILION REGIONAL AIRPORT AUTHORITY
 DANVILLE, ILLINOIS**

**REHABILITATE EASTERN APRON; TAXIWAY
 C & C1 PAVEMENTS
 TURFING AND SEEDING PLAN**



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SHEET	25 OF 25 SHEETS



- NOTES:**
- SHOULDER ADJUSTMENT SHALL BE AS SHOWN ON THE SHOULDER DETAIL "A".
 - A NOMINAL 5' WITH WILL BE REQUIRED TO MEET THE REQUIRED 5% SLOPE. MAY BE <> 5'.

SHOULDER DETAIL "A"
 N.T.S.

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