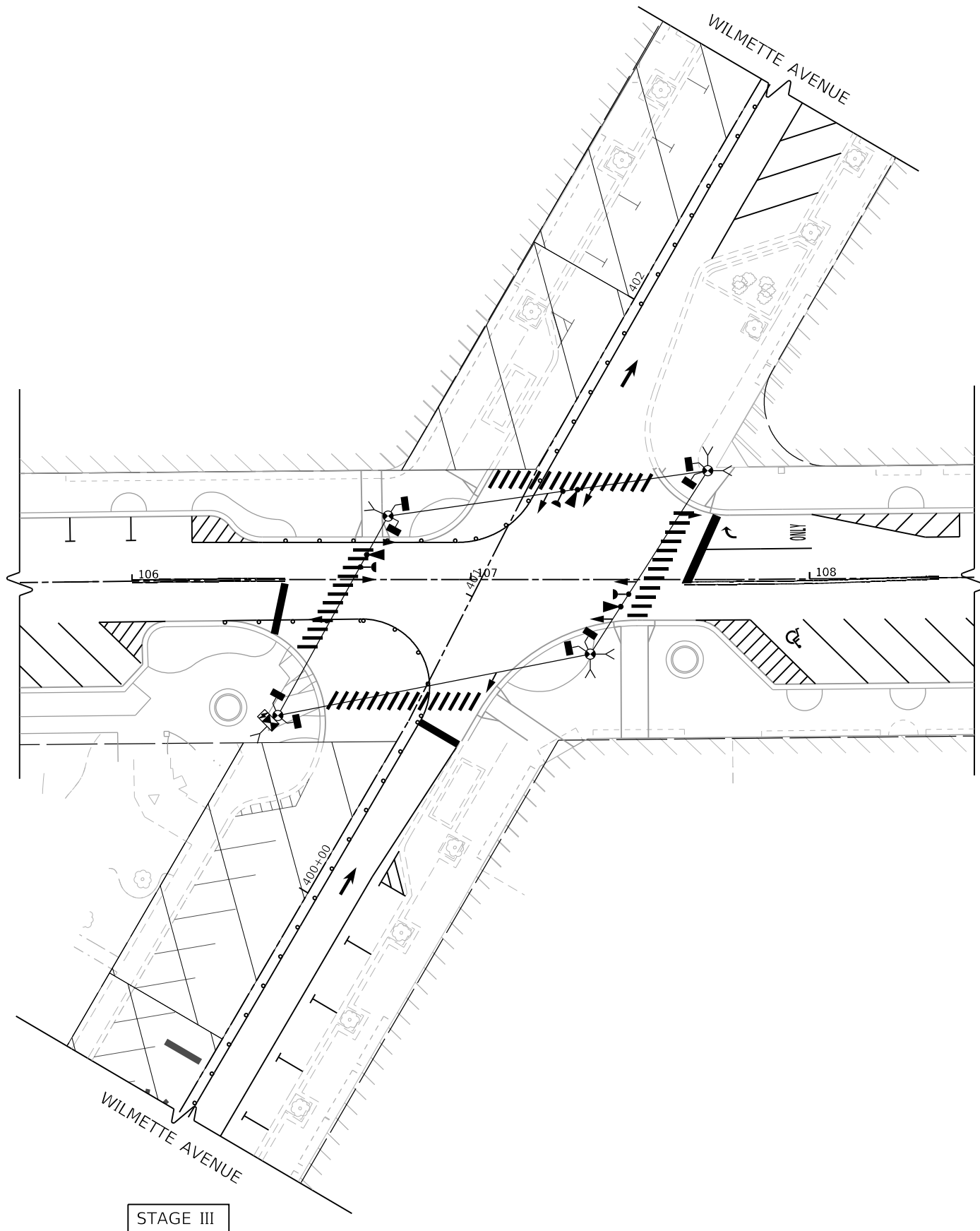


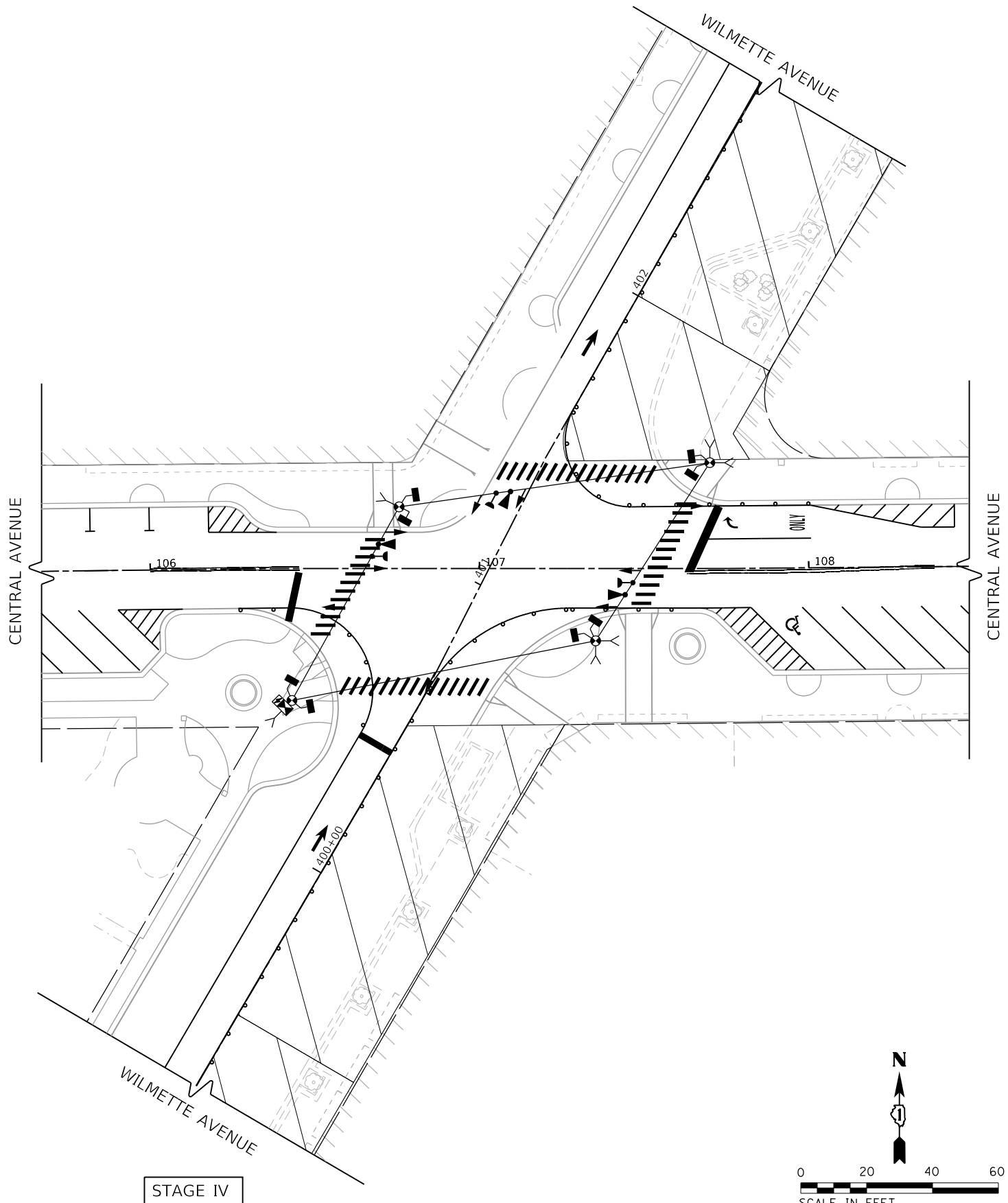
PLAN	SURVEYED	DATE
	PLOTTED	
	ALIGNED	
	CHECKED	
	BY	
	NO.	
	FILE NAME	

PROFILE	SURVEYED	DATE
	GRADES CHECKED	
	STRUCTURE	
	NOT AT THIS OFFICE	
	NO.	

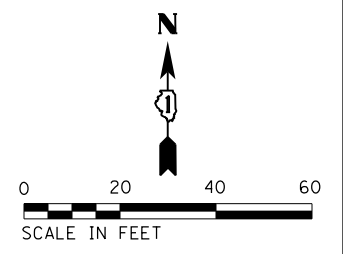
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 (847) 605-9600



STAGE III



STAGE IV



FILE NAME =	USER NAME = Mibeening	DESIGNED - GR	REVISED -
TS0114-MOT Stage		DRAWN - BTW	REVISED -
Default		CHECKED - DWB	REVISED -
	PLOT DATE = 2/13/2020	DATE - 02/13/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE AND WILMETTE AVENUE
 TEMPORARY TRAFFIC SIGNAL STAGING PLAN AND (SHEET 2 OF 2)**

SCALE: 1"=20' SHEET 14 OF 33 SHEETS STA. TO STA.

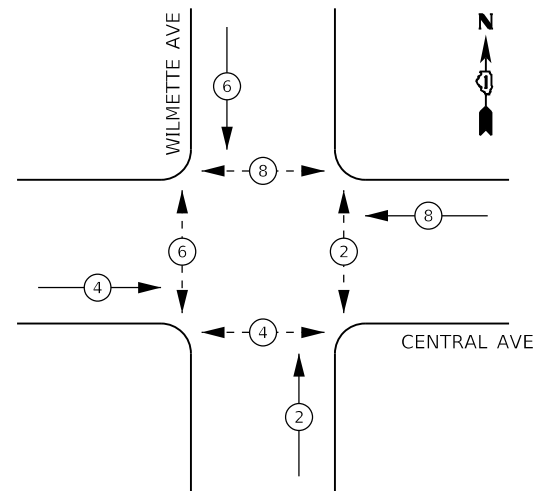
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	201
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	ALIGNED	
	CHECKED	
	FILED	
	DATE	

PROFILE	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	ALIGNED	
	CHECKED	
	FILED	
	DATE	

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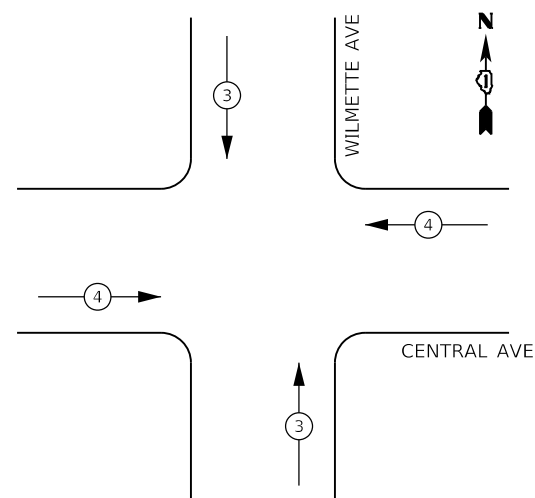
TEMPORARY CONTROLLER SEQUENCE



LEGEND:

- ← (⊙) ← PROTECTED PHASE
- ← - (⊙) - ← PROTECTED/PERMITTED PHASE
- ← (⊙) → PEDESTRIAN PHASE
- ← (⊙) OL OVERLAP

TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE



TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	12	11	50	66.0
(YELLOW)	12	20	5	12.0
(GREEN)	12	12	45	64.8
PERMISSIVE ARROW	0	10	10	
PED. SIGNAL	8	20	100	160.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
RADAR DETECTOR	0	20	100	
BLANK-OUT SIGN	0	25	5	
FLASHER	0	-	50	
STREET NAME SIGN	0	120	50	
LUMINAIRE	0	285	50	
TOTAL =				427.8

ENERGY COSTS TO:

CITY OF WILMETTE

ENERGY SUPPLY: CONTACT: _____
PHONE: _____
COMPANY: COMED
ACCOUNT NUMBER: _____

FILE NAME = TS0121-Temp Cabl	USER NAME = Mibeening	DESIGNED - GR	REVISED -
Default	PLOT SCALE = 40.0000' / in.	DRAWN - BTW	REVISED -
	PLOT DATE = 2/13/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

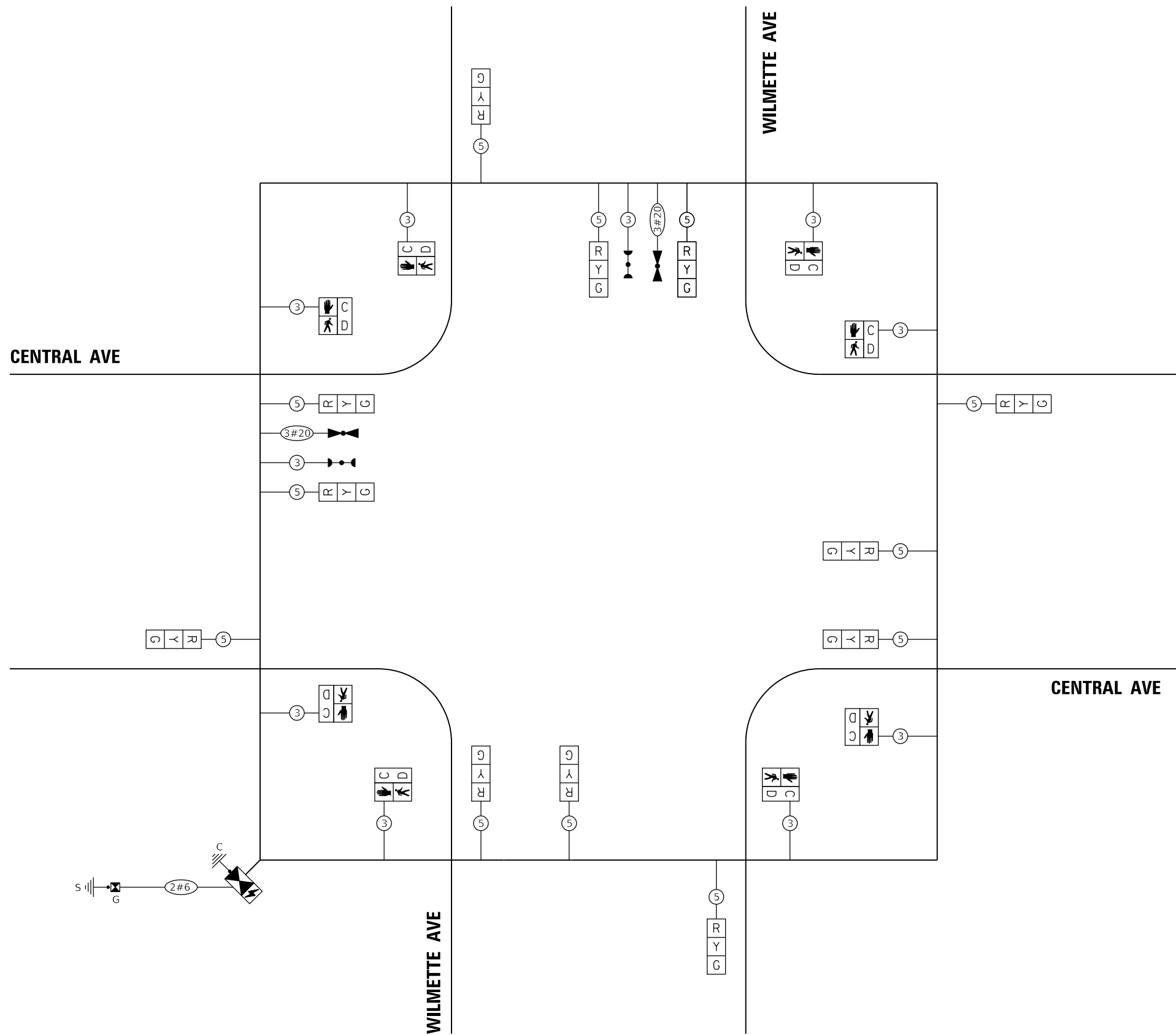
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN, TEMPORARY PHASE DESIGNATION DIAGRAM, AND TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE CENTRAL AVENUE AND WILMETTE AVENUE

SCALE: NONE SHEET 15 OF 33 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	202
				CONTRACT NO. 61G42

ILLINOIS FED. AID PROJECT



CABLE PLAN PRE-STAGE (NOT TO SCALE)

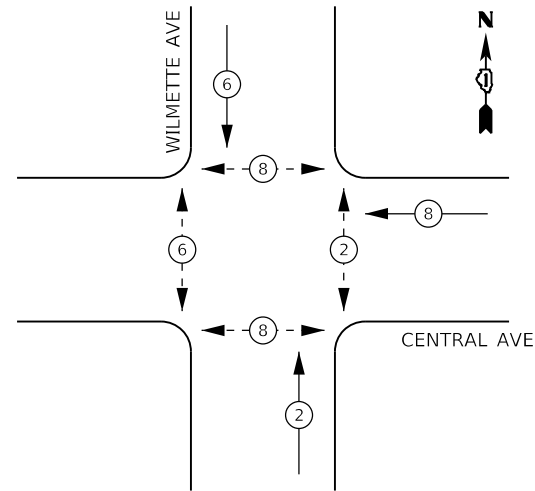


PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	GRADES CHECKED	
	ALIGNMENT CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	CADD FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK	GRADES CHECKED	BY
NO.	ALIGNMENT CHECKED	
	STRUCTURE NOTATIONS CHECKED	

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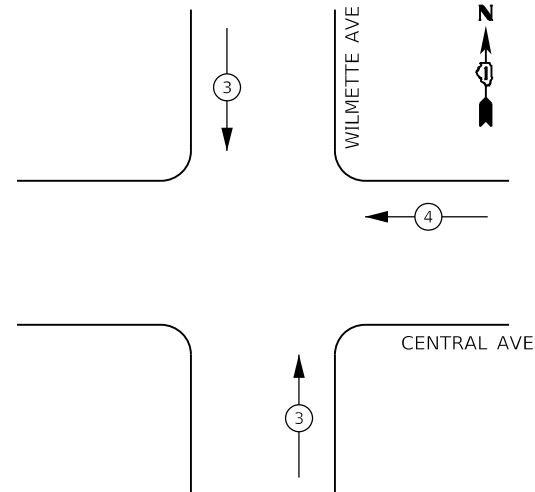
TEMPORARY CONTROLLER SEQUENCE



LEGEND:

- ← (⊙) ← PROTECTED PHASE
- ← (⊙) - - PROTECTED/PERMITTED PHASE
- ← (⊙) → PEDESTRIAN PHASE
- ← (⊙) OL OVERLAP

TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE



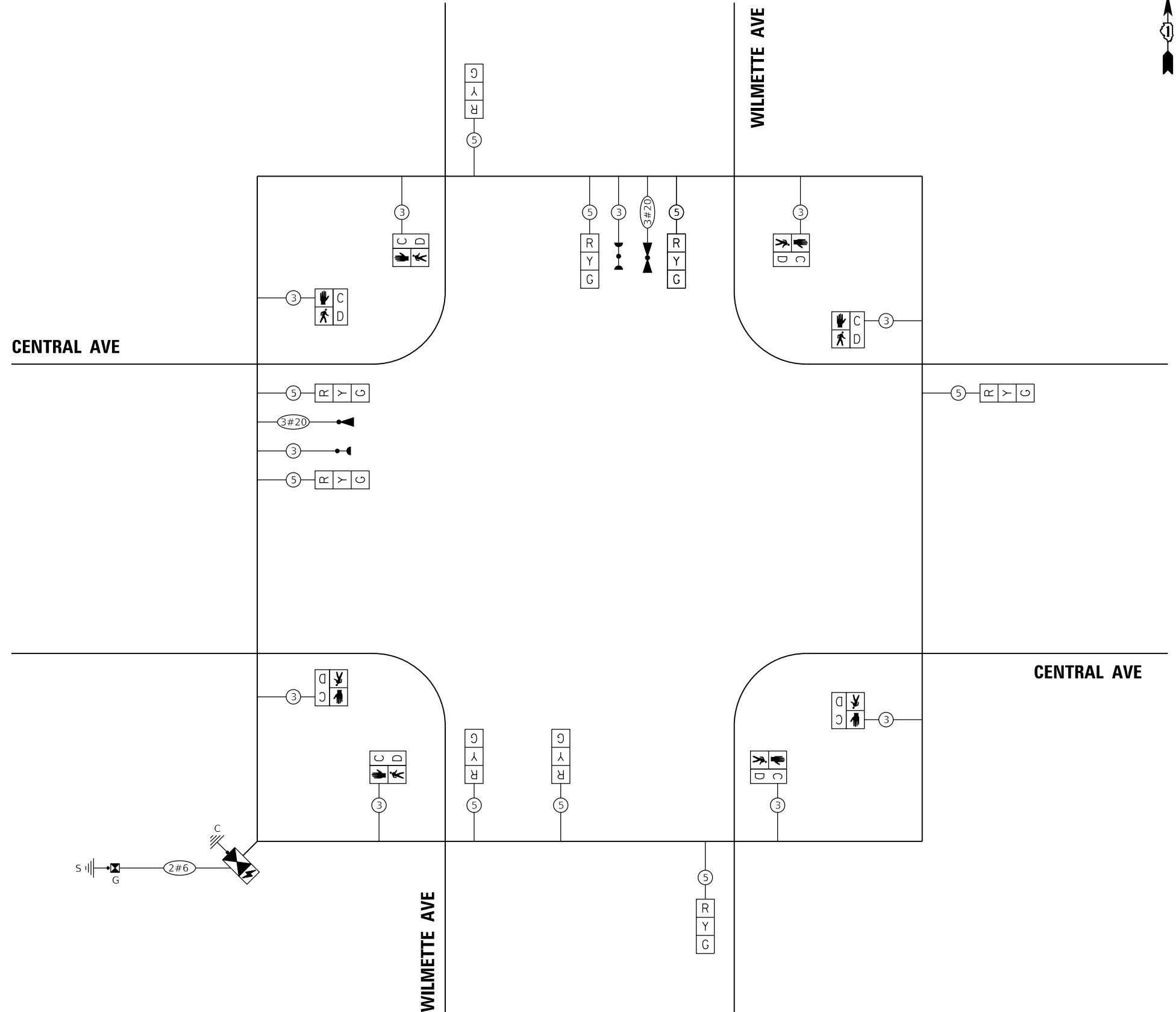
TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	8	11	50	44.0
(YELLOW)	8	20	5	8.0
(GREEN)	8	12	45	43.2
PERMISSIVE ARROW	0	10	10	
PED. SIGNAL	8	20	100	160.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
RADAR DETECTOR	0	20	100	
BLANK-OUT SIGN	0	25	5	
FLASHER	0	-	50	
STREET NAME SIGN	0	120	50	
LUMINAIRE	0	285	50	
TOTAL =				380.2

ENERGY COSTS TO:

CITY OF WILMETTE

ENERGY SUPPLY: CONTACT: _____
PHONE: _____
COMPANY: COMED
ACCOUNT NUMBER: _____



CABLE PLAN STAGES I AND II (NOT TO SCALE)

FILE NAME = TS0122-Temp Cabl	USER NAME = Mibeening	DESIGNED - GR	REVISED -
Default	PLOT SCALE = 40.0000' / in.	DRAWN - BTW	REVISED -
	PLOT DATE = 2/13/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN, TEMPORARY PHASE DESIGNATION DIAGRAM,
AND TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE
CENTRAL AVENUE AND WILMETTE AVENUE

SCALE: NONE SHEET 16 OF 33 SHEETS STA. TO STA.

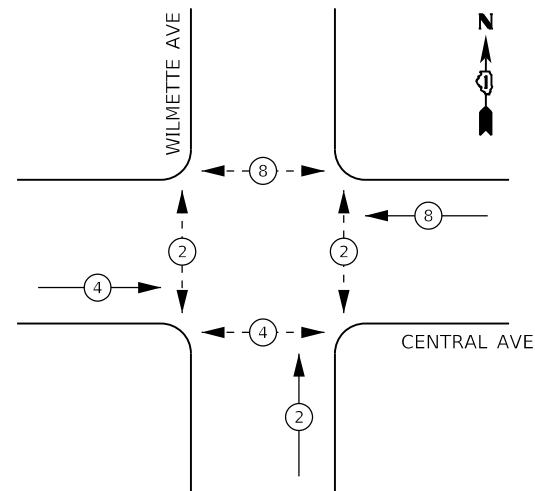
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	203
CONTRACT NO. 61G42				
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	
NO.	ALIGNED	
	CHECKED	
	BY	
	FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK	GRADES CHECKED	
NO.	STRUCTURE	
	NOT AT THIS OFFICE	

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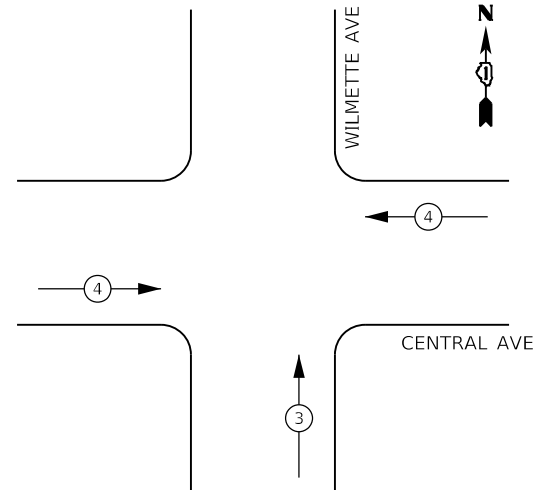
TEMPORARY CONTROLLER SEQUENCE



LEGEND:

- ← (⊛) → PROTECTED PHASE
- ← (⊖) → PROTECTED/PERMITTED PHASE
- ← (⊙) → PEDESTRIAN PHASE
- ← (OL) → OVERLAP

TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE



TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	8	11	50	44.0
(YELLOW)	8	20	5	8.0
(GREEN)	8	12	45	43.2
PERMISSIVE ARROW	0	10	10	
PED. SIGNAL	8	20	100	160.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
RADAR DETECTOR	0	20	100	
BLANK-OUT SIGN	0	25	5	
FLASHER	0	-	50	
STREET NAME SIGN	0	120	50	
LUMINAIRE	0	285	50	
TOTAL =				380.2

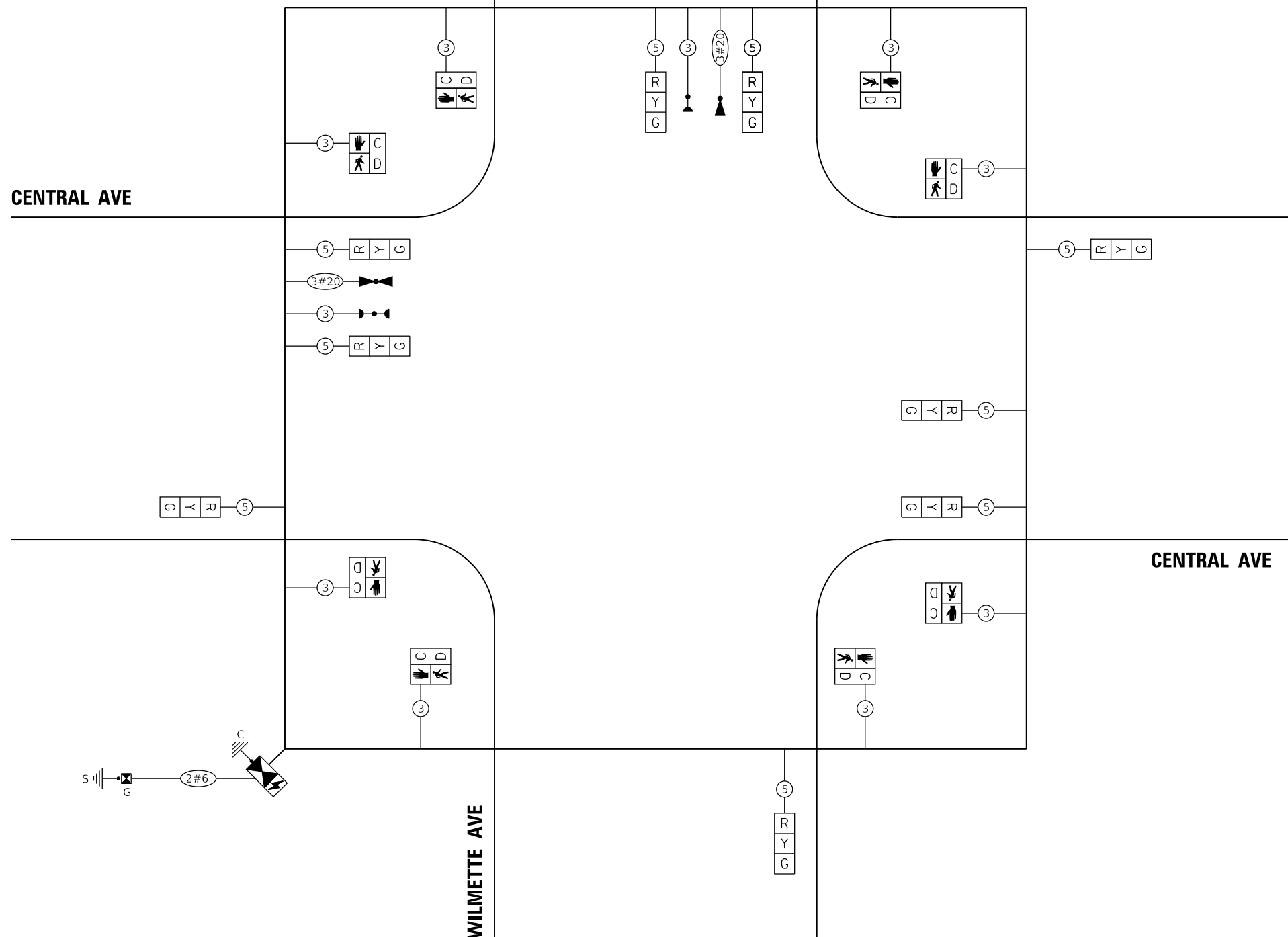
ENERGY COSTS TO:

CITY OF WILMETTE

ENERGY SUPPLY: CONTACT: _____
PHONE: _____
COMPANY: COMED
ACCOUNT NUMBER: _____

CENTRAL AVE

WILMETTE AVE



CABLE PLAN STAGES III AND IV (NOT TO SCALE)

FILE NAME = TS0123-Temp Cabl	USER NAME = Mibeening	DESIGNED - GR	REVISED -
Default	PLOT SCALE = 40.0000' / in.	DRAWN - BTW	REVISED -
	PLOT DATE = 2/13/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

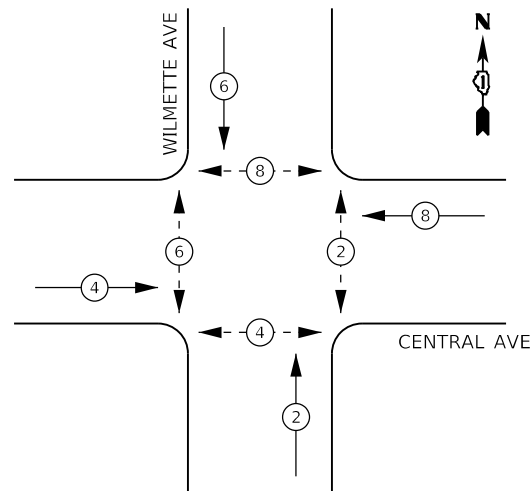
TEMPORARY CABLE PLAN, TEMPORARY PHASE DESIGNATION DIAGRAM,
AND TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE
CENTRAL AVENUE AND WILMETTE AVENUE

SCALE: NONE SHEET 17 OF 33 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	204
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				



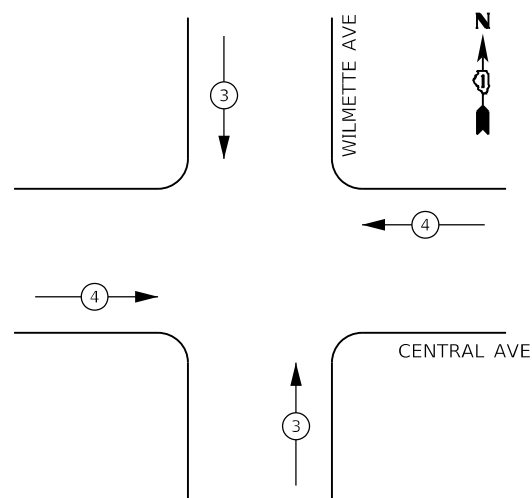
PROPOSED CONTROLLER SEQUENCE



LEGEND:

- ← ⊙ → PROTECTED PHASE
- ← ⊙ - - PROTECTED/PERMITTED PHASE
- ← ⊙ ⊙ → PEDESTRIAN PHASE
- ← ⊙ OL → OVERLAP

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	12	11	50	66.0
(YELLOW)	12	20	5	12.0
(GREEN)	12	12	45	64.8
ARROW	0	10	10	
PED. SIGNAL	8	20	100	160.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
RADAR DETECTOR	0	20	100	
BLANK-OUT SIGN	0	25	5	
FLASHER	0	-	50	
STREET NAME SIGN	0	120	50	
LUMINAIRE	0	285	50	
TOTAL =				427.8

ENERGY COSTS TO:

CITY OF WILMETTE

ENERGY SUPPLY: CONTACT: _____
 PHONE: _____
 COMPANY: COMED
 ACCOUNT NUMBER: _____

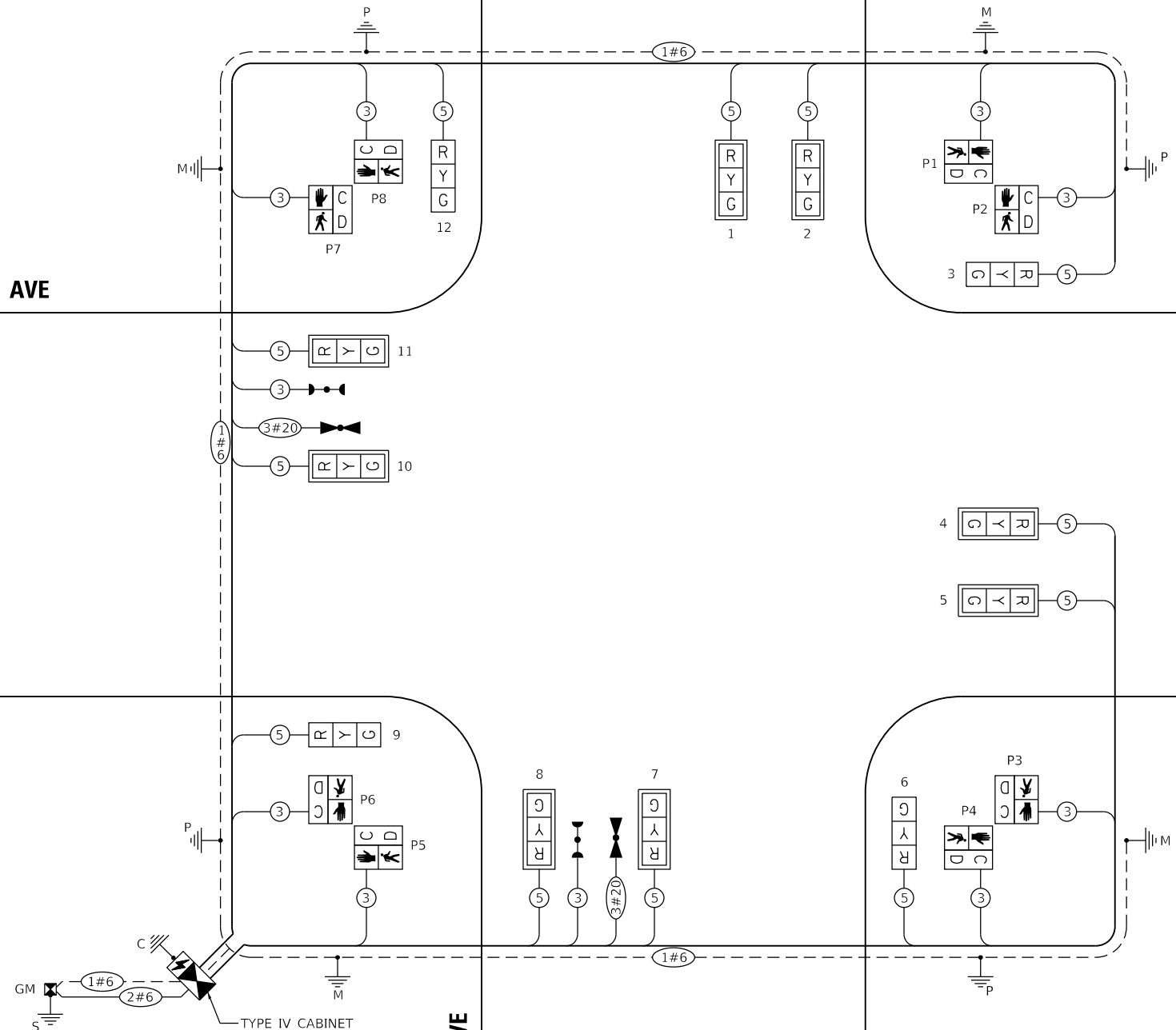
CENTRAL AVE

CENTRAL AVE

CENTRAL AVE

WILMETTE AVE

WILMETTE AVE



CABLE PLAN
(NOT TO SCALE)

PLAN	SURVEYED	BY	DATE
NOTE BOOK	GRADES CHECKED		
NO.	STRUCTURE NOTATIONS CHECKED		
	FILE NAME		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK	GRADES CHECKED		
NO.	STRUCTURE NOTATIONS CHECKED		
	FILE NAME		

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FILE NAME = TS0161-PR Cable	USER NAME = Mibeering	DESIGNED - GR	REVISED -
Default	PLOT SCALE = 40.0000' / in.	DRAWN - BTW	REVISED -
	PLOT DATE = 2/13/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

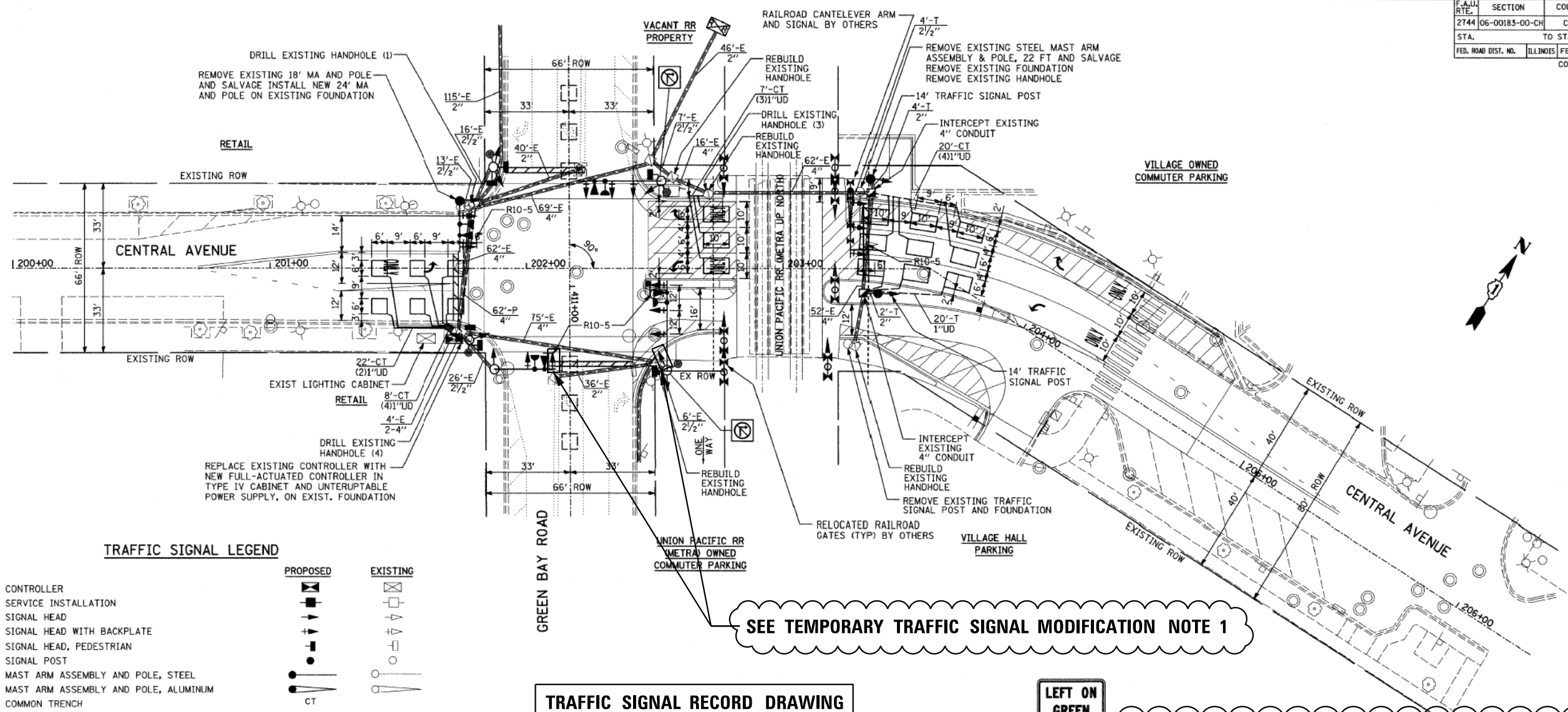
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

**CABLE PLAN, PHASE DESIGNATION DIAGRAM,
 AND EMERGENCY VEHICLE PREEMPTION SEQUENCE
 CENTRAL AVENUE AND WILMETTE AVENUE**

SCALE: NONE SHEET 19 OF 33 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	206
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	41
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83999				



TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
CONTROLLER	[Symbol]	[Symbol]
SERVICE INSTALLATION	[Symbol]	[Symbol]
SIGNAL HEAD	[Symbol]	[Symbol]
SIGNAL HEAD WITH BACKPLATE	[Symbol]	[Symbol]
SIGNAL HEAD, PEDESTRIAN	[Symbol]	[Symbol]
SIGNAL POST	[Symbol]	[Symbol]
MAST ARM ASSEMBLY AND POLE, STEEL	[Symbol]	[Symbol]
MAST ARM ASSEMBLY AND POLE, ALUMINUM	[Symbol]	[Symbol]
COMMON TRENCH	[Symbol]	[Symbol]
UNIT DUCT	[Symbol]	[Symbol]
HANDHOLE	[Symbol]	[Symbol]
HEAVY DUTY HANDHOLE	[Symbol]	[Symbol]
DOUBLE HANDHOLE	[Symbol]	[Symbol]
G.S. CONDUIT IN TRENCH OR PUSHED	[Symbol]	[Symbol]
PEDESTRIAN PUSHBUTTON DETECTOR	[Symbol]	[Symbol]
DETECTOR LOOP	[Symbol]	[Symbol]
CAST IRON JUNCTION BOX	[Symbol]	[Symbol]
EMERGENCY VEHICLE SYSTEM DETECTOR	[Symbol]	[Symbol]
CONFIRMATION BEACON	[Symbol]	[Symbol]
SIGNAL HEAD OPTICALLY PROGRAMMED	[Symbol]	[Symbol]
CONDUIT SPLICE	[Symbol]	[Symbol]
WOOD POLE	[Symbol]	[Symbol]
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	[Symbol]	[Symbol]
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	[Symbol]	[Symbol]
RAILROAD CONTROL CABINET	[Symbol]	[Symbol]
ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"	[Symbol]	[Symbol]
ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"	[Symbol]	[Symbol]
TELEPHONE	[Symbol]	[Symbol]

TRAFFIC SIGNAL RECORD DRAWING FOR INFORMATION ONLY

- THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR. THE PROPERTY OF THE VILLAGE OF WILMETTE AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE VILLAGE'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.
- 1 EACH STEEL MAST ARM ASSEMBLY POLE 18'
 - 1 EACH STEEL MAST ARM ASSEMBLY POLE 22'
- THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.
- 4 EACH PEDESTRIAN PUSH BUTTON
 - 1 EACH SIGNAL POST
 - 7 EACH SIGNAL HEAD, 1-FACE, 3-SECTION
 - 4 EACH SIGNAL HEAD, 1-FACE, 4-SECTION
 - 4 EACH SIGNAL HEAD, 1-FACE, 5-SECTION
 - 2 EACH PEDESTRIAN HEAD, 1-FACE, 2-SECTION
 - 2 EACH PEDESTRIAN HEAD, 2-FACE, 2-SECTION
 - 5484 FOOT EXISTING CABLE FROM CONDUIT

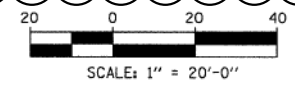
SEE TEMPORARY TRAFFIC SIGNAL MODIFICATION NOTE 1

LEFT ON GREEN ARROW ONLY

R10-5
24"x30"
SIGN DETAIL
N.T.S
4 REQUIRED

TEMPORARY TRAFFIC SIGNAL MODIFICATION NOTES (STAGE I AND II):

1. DE-ENERGIZE AND COVER EXISTING SOUTHBOUND LEFT-TURN SIGNAL HEADS FOR CLOSURE OF EASTBOUND CENTRAL AVENUE DURING STAGES I AND II.
2. IMPLEMENT TEMPORARY TRAFFIC SIGNAL TIMING AT CENTRAL AVENUE AND GREEN BAY ROAD TO OMIT PROTECTED SOUTHBOUND LEFT-TURN PHASE DURING STAGES I AND II. ALL SIGNAL PHASING AND TIMING MODIFICATIONS SHALL PROVIDE SAFE OPERATIONS DURING RAILROAD PREEMPTION, EMERGENCY VEHICLE PREEMPTION, AND NORMAL OPERATIONS.
3. RE-INSTATE EXISTING SIGNAL EQUIPMENT AND OPERATIONS UPON COMPLETION OF STAGE II AND RE-OPENING OF EASTBOUND TRAFFIC ON CENTRAL AVENUE.
4. ALL WORK ASSOCIATED WITH TEMPORARY MODIFICATION OF THIS EXISTING SIGNAL INSTALLATION SHALL BE INCLUDED THE COST FOR "MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION" AND "TEMPORARY TRAFFIC SIGNAL TIMING".



REVISIONS NAME DATE		TRAFFIC SIGNAL PLAN CENTRAL AVENUE AT GREEN BAY ROAD		VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
CONSULTANT TYLIN INTERNATIONAL		DRAWN: BK CHECKED: DAJ APPROVED: DAJ		SHEET NO.	
		DATE: 02-14-2008 SCALE: 1"=20'-0"		PROJECT NO.	

PLAN	SURVEYED	DATE
	PLOTTED	
	ALIGNED	
	CHECKED	
	FILED	
	NO.	

PLAN	SURVEYED	DATE
	PLOTTED	
	ALIGNED	
	CHECKED	
	FILED	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES	
	CHECKED	
	STRUCTURE	
	NOTATIONS	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES	
	CHECKED	
	STRUCTURE	
	NOTATIONS	
	NO.	

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FILE NAME = TS0181-Ex TS Gre	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
Default		DRAWN - MLB	REVISED -
		CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SCALE: NONE	SHEET 21	OF 33 SHEETS	STA. TO STA.
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CENTRAL AVENUE AND WILMETTE AVENUE
EXISTING TRAFFIC SIGNAL PLANS
GREEN BAY ROAD AND CENTRAL AVENUE

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	208
CONTRACT NO. 61G42				
ILLINOIS FED. AID PROJECT				

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	42
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83999				

CABLE PLAN LEGEND

EXISTING	PROPOSED	EXISTING	PROPOSED

TEMPORARY TRAFFIC SIGNAL MODIFICATION NOTES (STAGE I AND II):

- DE-ENERGIZE AND COVER EXISTING SOUTHBOUND LEFT-TURN SIGNAL HEADS FOR CLOSURE OF EASTBOUND CENTRAL AVENUE DURING STAGES I AND II.
- IMPLEMENT TEMPORARY TRAFFIC SIGNAL TIMING AT CENTRAL AVENUE AND GREEN BAY ROAD TO OMIT PROTECTED SOUTHBOUND LEFT-TURN PHASE DURING STAGES I AND II. ALL SIGNAL PHASING AND TIMING MODIFICATIONS SHALL PROVIDE SAFE OPERATIONS DURING RAILROAD PREEMPTION, EMERGENCY VEHICLE PREEMPTION, AND NORMAL OPERATIONS.
- RE-INSTATE EXISTING SIGNAL EQUIPMENT AND OPERATIONS UPON COMPLETION OF STAGE II AND RE-OPENING OF EASTBOUND TRAFFIC ON CENTRAL AVENUE.
- ALL WORK ASSOCIATED WITH TEMPORARY MODIFICATION OF THIS EXISTING SIGNAL INSTALLATION SHALL BE INCLUDED THE COST FOR "MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION" AND "TEMPORARY TRAFFIC SIGNAL TIMING".

SCHEDULE OF QUANTITIES

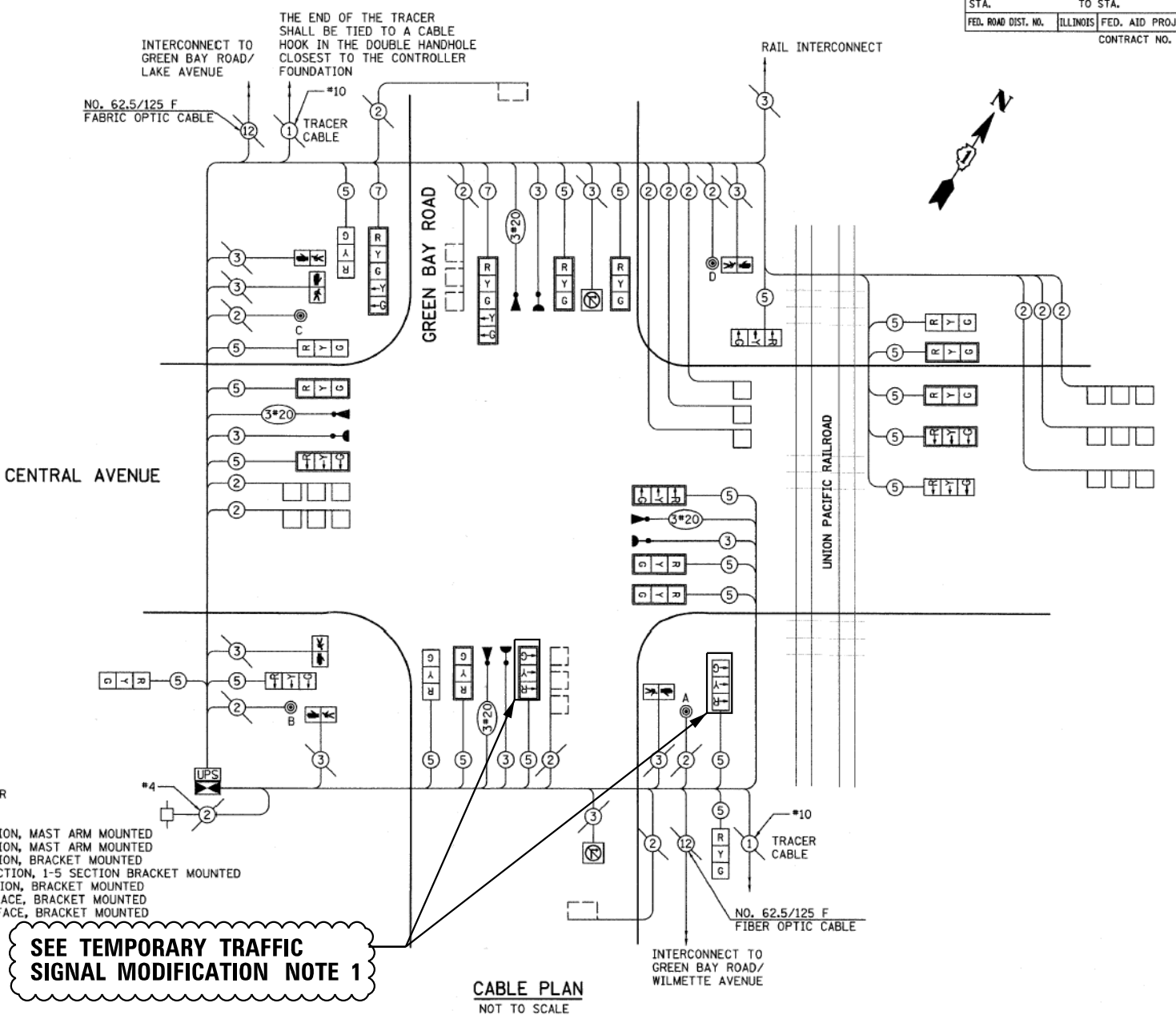
QTY	UNIT	ITEM DESCRIPTION
4	EACH	PEDESTRIAN PUSH BUTTON
6	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
4	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL
62	FOOT	CONDUIT, PUSH-4" DIA., GALVANIZED STEEL
2	EACH	HANDHOLE
4	EACH	REBUILD EXISTING HANDHOLE
6	EACH	DRILL EXISTING HANDHOLE
645	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
4053	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
362	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
1510	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.
8	FOOT	CONCRETE FOUNDATION, TYPE A
12	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
1	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
8	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
1	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, 1-3-SECTION, 1-5 SECTION BRACKET MOUNTED
2	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, BRACKET MOUNTED
13	EACH	TRAFFIC SIGNAL BACKPLATE
8	EACH	INDUCTIVE LOOP DETECTOR
663	FOOT	DETECTOR LOOP, TYPE I
10	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
1	EACH	REMOVE EXISTING HANDHOLE
2	EACH	REMOVE EXISTING CONCRETE FOUNDATION
645	FOOT	REMOVE EXISTING CABLE FROM CONDUIT
645	FOOT	ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED, SHIELDED
4	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 14 FT.
4	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
1	EACH	FULL - ACTUATED CONTROLLER, IN TYPE IV CABINET
1	EACH	UNINTERUPTABLE POWER SUPPLY, STANDARD
1	EACH	MAINTAIN EXISTING TRAFFIC SIGNAL INSTALLATION
1	EACH	TRANSCIVER, FIBER OPTIC
2	EACH	ILLUMINATED SIGN, LED
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				
TYPE	NO. LAMPS	WATTAGE LED	% OPERATIONS	TOTAL WATTAGE
SIGNAL (RED)	24	17	0.50	204
(YELLOW)	24	25	0.25	150
(GREEN)	24	15	0.25	90
ARROW	4	12	0.10	4.8
PED. SIGNAL	6	25	1.00	150
CONTROLLER	1	100	1.00	100
TL SIGN	2	25	0.05	2.5
TOTAL =				701.3

ENERGY COSTS- BILLED TO: VILLAGE OF WILMETTE
 (ADDRESS) 1200 WILMETTE AVENUE
 WILMETTE, IL

ENERGY SUPPLY - CONTACT _____
 PHONE _____
 COMPANY COMED

FOUNDATION DEPTH	DEPTH (FT.)	CABLE SLACK (FT.)	VERTICAL (FT.)
TYPE A - POST	4	HANDHOLE	6.5
D - CONTOLLER	4	DOUBLE HANDHOLE	13
E - MAST ARM POLE		SIGNAL POST	2
	10	CONTOLLER CAB.	1
	15	FIBER OPTIC	13
		ELECTRICAL SERVICE	1
		GROUND CABLE	1



SEE TEMPORARY TRAFFIC SIGNAL MODIFICATION NOTE 1

TRAFFIC SIGNAL RECORD DRAWING FOR INFORMATION ONLY

- NOTES:**
- PEDESTRIAN PUSH BUTTON "A" TO CALL PHASE 3.
 - PEDESTRIAN PUSH BUTTON "B" TO CALL PHASE 3 AND 6.
 - PEDESTRIAN PUSH BUTTON "C" TO CALL PHASE 4 AND 6.
 - PEDESTRIAN PUSH BUTTON "D" TO CALL PHASE 4.

REVISIONS	
NAME	DATE

CABLE PLAN, SCHEDULE OF QUANTITIES AND PHASE DESIGNATION DIAGRAM
 CENTRAL AVENUE AT GREEN BAY ROAD

CONSULTANT
TYLIN INTERNATIONAL

VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
DRAWN: BK	SHEET NO.
CHECKED: DAJ	
APPROVED: DAJ	
DATE: 02-14-2008	
SCALE: NONE	
JOB NO.:	PROJECT NO.:

PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	
NO.	ALIGNED	
	CHECKED	
	BY	
	FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK	GRADES CHECKED	
NO.	STRUCTURE	
	NOTATIONS	
	CHKD	

PROFILE	SURVEYED	DATE
NOTE BOOK	GRADES CHECKED	
NO.	STRUCTURE	
	NOTATIONS	
	CHKD	

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 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBERG, ILLINOIS 60173
 (847) 605-9600

FILE NAME = TS0182-Ex Cable	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 40.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/13/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CENTRAL AVENUE AND WILMETTE AVENUE EXISTING TRAFFIC SIGNAL CABLE PLANS GREEN BAY ROAD AND CENTRAL AVENUE	F.A.U. RTE. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 209
SCALE: NONE	SHEET 22	OF 33 SHEETS	STA. TO STA.	CONTRACT NO. 61G42	
			ILLINOIS	FED. AID PROJECT	

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS

																PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 5	PREEMPTOR NUMBER 6	
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1	1	1	5	5	9	9	12	12											
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	1T	1U	
CHANGE TO EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	3	1F	4 OR 5	1H	1J	2, 4 OR 5	3	2	1N	3, 4 OR 5	1Q	1R	2	1T	1U	3, 4 OR 5	
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	G	Y	R	
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	G	Y	R	
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS	R	R	R	R	R	R	G	Y	R	G	R	R	R	G	Y	R	G	Y	R	
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS	←Y	←R	←G	←G	←Y	←R	←G	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	
CENTRAL AVENUE FAR RIGHT AND CENTER MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
CENTRAL AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	
CENTRAL AVENUE (EAST OF TRACKS) LEFT CANTILEVER AND FAR LEFT MEDIAN SIGNALS	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	
CENTRAL AVENUE (EAST OF TRACKS) FAR RIGHT AND CENTER CANTILEVER SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
CENTRAL AVENUE NEAR RIGHT CENTER AND FAR RIGHT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
CENTRAL AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON SOUTH SIDE OF CENTRAL AVENUE (S. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON NORTH SIDE OF CENTRAL AVENUE (N. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	
PEDESTRIAN SIGNALS CROSSING CENTRAL AVENUE ON WEST SIDE OF GREEN BAY ROAD (W. LEG)	H	H	H	H	H	H	FH	H	H	FH	H	H	H	FH	H	H	FH	H	H	

P = ILLUMINATED PERSON = WALK
 FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
 H = ILLUMINATED SOLID HAND = DON'T WALK
 ◆ = EMERGENCY VEHICLE SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY VEHICLE INTERVAL AFTER EMERGENCY VEHICLE INTERVAL 2 OR 3 IS TERMINATED.

TRAFFIC SIGNAL RECORD DRAWING FOR INFORMATION ONLY

REVISIONS NAME DATE		EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS SHEET 1 OF 2 CENTRAL AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
CONSULTANT TYLIN INTERNATIONAL			DRAWN: _____ CHECKED: _____ APPROVED: _____ DATE: 02-14-2008 SCALE: NONE JOB NO.: _____	SHEET NO.: _____ PROJECT NO.: _____

PLAN SURVEYED PLOTTED ALIGNED CHECKED
 NOTE BOOK NO. _____ DATE _____
 BY _____

PROFILE SURVEYED PLOTTED GRADES CHECKED STRUCTURE NOTATIONS CHECKED
 NOTE BOOK NO. _____ DATE _____
 BY _____

PLAN SURVEYED PLOTTED ALIGNED CHECKED
 NOTE BOOK NO. _____ DATE _____
 BY _____

PROFILE SURVEYED PLOTTED GRADES CHECKED STRUCTURE NOTATIONS CHECKED
 NOTE BOOK NO. _____ DATE _____
 BY _____

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 SCHAMBERG, ILLINOIS 60173
 (847) 605-9600

FILE NAME = TS0184-Ex Emerge	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 40.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/13/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CENTRAL AVENUE AND WILMETTE AVENUE
 EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS
 GREEN BAY ROAD AND CENTRAL AVENUE
 SCALE: NONE SHEET 24 OF 33 SHEETS STA. TO STA.

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS

CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	OPERATION INTERVAL NUMBER																				PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 5	PREEMPTOR NUMBER 6	CLEAR TO NORMAL SEQUENCE													
	15	15	15	15	15	15	15	15	15	15	15	15	15	15	15	15	15	15	15	15	2	3	4	5														
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1V	1W	1X	1Y	1Z	1AA	1BB	1CC	1DD	1EE	1FF	1GG	1HH	1JJ	1KK	1LL	1MM	1NN	1PP	1QQ	1RR	1SS	1TT	1UU	1VV	1WW	1XX	1YY	1ZZ	1AAA	1BBB	1CCC	1DDD					
CHANGE TO EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1W	1X	1Y	2 OR 3	1AA	4	1CC	1DD	1EE	5	1GG	1HH	1JJ	1KK	2, 3 OR 5	4	1NN	1PP	2, 3 OR 4	5	1SS	1TT	1UU	1VV	2 OR 3	1XX	1YY	4	1AAA	1BBB	1CCC	1DDD	5					
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	R	R
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	R	R
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	R
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	←R	
CENTRAL AVENUE FAR RIGHT AND CENTER MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	G	G	G	Y	R	G	R	R	R	R	G	G	G	Y	R	G	G	G	G	G	G	Y	R	R	R	R	G	R
CENTRAL AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS	←G	←G	←Y	←R	←G	←G	←G	←G	←Y	←R	←G	←G	←G	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	
CENTRAL AVENUE (EAST OF TRACKS) LEFT CANTILEVER AND FAR LEFT MEDIAN SIGNALS	←Y	←R	←R	←R	←G	←G	←Y	←R	←R	←R	←G	←Y	←R	←R	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	
CENTRAL AVENUE (EAST OF TRACKS) FAR RIGHT AND CENTER CANTILEVER SIGNALS	R	R	R	R	R	R	R	R	R	R	G	Y	R	R	R	G	R	R	R	R	G	Y	R	R	G	G	G	G	Y	R	R	R	R	R	R	G	R	
CENTRAL AVENUE NEAR RIGHT CENTER AND FAR RIGHT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	Y	R	G	Y	R	G	G	G	G	G	G	R	R	R	G	
CENTRAL AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS	←G	←G	←Y	←R	←Y	←R	←G	←G	←G	←G	←R	←R	←R	←R	←R	←G	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON SOUTH SIDE OF CENTRAL AVENUE (S. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	FH	H	H	FH	FH	H	H	H	FH	H	H	FH	H	H	H	H	H	H	H	H		
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON NORTH SIDE OF CENTRAL AVENUE (N. LEG)	H	H	H	H	H	H	H	H	H	H	FH	H	H	H	H	FH	H	H	H	FH	H	H	H	FH	H	H	FH	H	H	H	H	H	H	H	H	H		
PEDESTRIAN SIGNALS CROSSING CENTRAL AVENUE ON WEST SIDE OF GREEN BAY ROAD (W. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	

P = ILLUMINATED PERSON = WALK
 FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
 H = ILLUMINATED SOLID HAND = DON'T WALK
 ◆ = EMERGENCY VEHICLE SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY VEHICLE INTERVAL AFTER EMERGENCY VEHICLE INTERVAL 2 OR 3 IS TERMINATED.

TRAFFIC SIGNAL RECORD DRAWING FOR INFORMATION ONLY

REVISIONS NAME DATE		EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS SHEET 2 OF 2 CENTRAL AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
CONSULTANT TYLIN INTERNATIONAL			DRAWN: _____ CHECKED: _____ APPROVED: _____ DATE: 02-14-2008 SCALE: NONE JOB NO.: _____	SHEET NO.: _____ PROJECT NO.: _____

PLAN SURVEYED _____ PLOTTED _____ ALIGNED _____ CHECKED _____ DATE FILE NAME _____ NO. _____	BY _____ DATE _____
PROFILE SURVEYED _____ PLOTTED _____ GRADES CHECKED _____ STRUCTURE NOTATIONS CHECKED _____ NO. _____	BY _____ DATE _____

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 SCHAMBURG, ILLINOIS 60173
 (847) 605-9600

PROPOSED RAILROAD PREEMPTION SEQUENCE OF OPERATIONS

																PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 5	PREEMPTOR NUMBER 2									
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1	5	9	12	15	19	22	25																					
CHANGE FROM EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER																2	3	4	5										
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	1T	1U	1V	1W	1X	1Y	1Z	2	3	4	5	
CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	2	1F	2	1H	2	2	2	1M	2	1P	2	1R	2	1T	2	1V	2	1X	2	1Z	2	3	4	5	▲	
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	G	▲
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	G	▲
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS	R	R	Y	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	G	▲
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	▲	
CENTRAL AVENUE FAR RIGHT AND CENTER MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	G	G	R	R	G	G	R	R	R	G	G	G	R	R	G	Y	R	R	▲
CENTRAL AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS	←R	←R	←R	←R	←R	←R	←R	←R	←G	←G	←G	←G	←R	←R	←R	←R	←R	←R	←G	←G	←R	←R	←G	←Y	←R	←R	←R	▲	
CENTRAL AVENUE (EAST OF TRACKS) LEFT CANTILEVER AND FAR LEFT MEDIAN SIGNALS	←R	←R	←R	←R	←R	←R	←R	←Y	←R	←Y	←R	←R	←R	←R	←R	←R	←R	←R	←Y	←R	←R	←R	←R	←R	←R	←R	←R	▲	
CENTRAL AVENUE (EAST OF TRACKS) FAR RIGHT AND CENTER CANTILEVER SIGNALS	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	Y	R	R	R	R	Y	R	R	R	R	R	R	R	▲	
CENTRAL AVENUE NEAR RIGHT CENTER AND FAR RIGHT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	Y	R	R	R	R	▲
CENTRAL AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS	←R	←R	←R	←R	←R	←R	←R	←Y	←R	←R	←Y	←R	←R	←R	←R	←R	←R	←R	←Y	←R	←R	←Y	←R	←R	←R	←R	←R	▲	
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON SOUTH SIDE OF CENTRAL AVENUE (S. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	FH	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	▲	
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON NORTH SIDE OF CENTRAL AVENUE (N. LEG)	H	H	H	H	H	H	H	H	H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	▲	
PEDESTRIAN SIGNALS CROSSING CENTRAL AVENUE ON WEST SIDE OF GREEN BAY ROAD (W. LEG)	H	H	FH	H	H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	▲	
INTERNALLY ILLUMINATED NRT SIGNS - GREEN BAY ROAD	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	▲	

HOLD

▲ RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL 2, 3 OR 4 (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.

P = ILLUMINATED PERSON = WALK
 FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
 H = ILLUMINATED SOLID HAND = DON'T WALK

TRAFFIC SIGNAL RECORD DRAWING FOR INFORMATION ONLY

REVISIONS NAME DATE		RAILROAD PREEMPTION SEQUENCE OF OPERATIONS CENTRAL AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
CONSULTANT TYLIN INTERNATIONAL			DRAWN: _____ CHECKED: _____ APPROVED: _____ DATE: 02-14-2008 SCALE: NONE JOB NO.: _____	SHEET NO. _____ PROJECT NO.: _____

PLAN SURVEYED PLOTTED ALIGNED CHECKED
 NOTE BOOK NO. _____ DATE _____
 BY _____

PROFILE SURVEYED PLOTTED GRADES CHECKED STRUCTURE NOTATIONS CHECKED
 NOTE BOOK NO. _____ DATE _____
 BY _____

PROFILE SURVEYED PLOTTED GRADES CHECKED STRUCTURE NOTATIONS CHECKED
 NOTE BOOK NO. _____ DATE _____
 BY _____

PROFILE SURVEYED PLOTTED GRADES CHECKED STRUCTURE NOTATIONS CHECKED
 NOTE BOOK NO. _____ DATE _____
 BY _____

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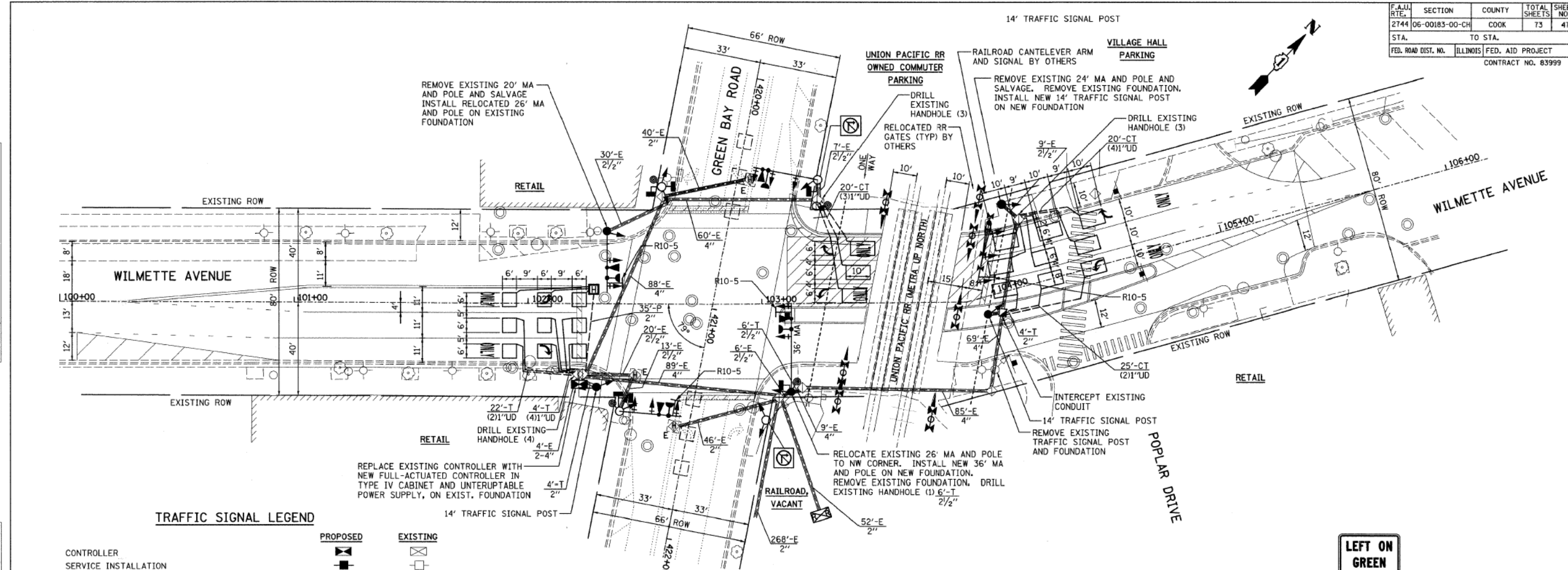
FILE NAME = TS0186-Ex Railro	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 40.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/13/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE AND WILMETTE AVENUE
 EXISTING RAILROAD PREEMPTION SEQUENCE OF OPERATIONS
 GREEN BAY ROAD AND CENTRAL AVENUE**

SCALE: NONE SHEET 26 OF 33 SHEETS STA. TO STA.

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	47
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83999				



TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
CONTROLLER	[Symbol]	[Symbol]
SERVICE INSTALLATION	[Symbol]	[Symbol]
SIGNAL HEAD	[Symbol]	[Symbol]
SIGNAL HEAD WITH BACKPLATE	[Symbol]	[Symbol]
SIGNAL HEAD, PEDESTRIAN	[Symbol]	[Symbol]
SIGNAL POST	[Symbol]	[Symbol]
MAST ARM ASSEMBLY AND POLE, STEEL	[Symbol]	[Symbol]
MAST ARM ASSEMBLY AND POLE, ALUMINUM	[Symbol]	[Symbol]
COMMON TRENCH	[Symbol]	[Symbol]
UNIT DUCT	[Symbol]	[Symbol]
HANDHOLE	[Symbol]	[Symbol]
HEAVY DUTY HANDHOLE	[Symbol]	[Symbol]
DOUBLE HANDHOLE	[Symbol]	[Symbol]
G.S. CONDUIT IN TRENCH OR PUSHED	[Symbol]	[Symbol]
PEDESTRIAN PUSHBUTTON DETECTOR	[Symbol]	[Symbol]
DETECTOR LOOP	[Symbol]	[Symbol]
CAST IRON JUNCTION BOX	[Symbol]	[Symbol]
EMERGENCY VEHICLE SYSTEM DETECTOR	[Symbol]	[Symbol]
CONFIRMATION BEACON	[Symbol]	[Symbol]
SIGNAL HEAD OPTICALLY PROGRAMMED	[Symbol]	[Symbol]
CONDUIT SPLICE	[Symbol]	[Symbol]
WOOD POLE	[Symbol]	[Symbol]
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	[Symbol]	[Symbol]
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	[Symbol]	[Symbol]
RAILROAD CONTROL CABINET	[Symbol]	[Symbol]
ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"	[Symbol]	[Symbol]
ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"	[Symbol]	[Symbol]
TELEPHONE	[Symbol]	[Symbol]

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE VILLAGE OF WILMETTE AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE VILLAGE'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- 1 EACH STEEL MAST ARM ASSEMBLY POLE 24'
- 1 EACH STEEL MAST ARM ASSEMBLY POLE 20'

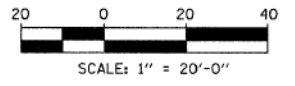
THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 4 EACH PEDESTRIAN PUSH BUTTONS
- 1 EACH SIGNAL POST
- 7 EACH SIGNAL HEAD, 1-FACE, 3-SECTION
- 4 EACH SIGNAL HEAD, 1-FACE, 4-SECTION
- 4 EACH SIGNAL HEAD, 1-FACE, 5-SECTION
- 2 EACH PEDESTRIAN HEAD, 1-FACE, 2-SECTION
- 2 EACH PEDESTRIAN HEAD, 2-FACE, 2-SECTION
- 6485 FOOT EXISTING CABLE FROM CONDUIT

LEFT ON GREEN ARROW ONLY

R10-5
24"x30"
SIGN DETAIL
N.T.S
4 REQUIRED

TRAFFIC SIGNAL RECORD DRAWING FOR INFORMATION ONLY



REVISIONS NAME DATE		TRAFFIC SIGNAL PLAN WILMETTE AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
CONSULTANT TYLIN INTERNATIONAL			DRAWN: BK CHECKED: DAJ APPROVED: DAJ DATE: 02-14-2008 SCALE: 1"=20'-0" JOB NO.: PROJECT NO.:	SHEET NO.:

PLAN	SURVEYED	DATE
	BY	
	PLOTTED	
	ALIGNED	
	CHECKED	
	FILED	
	NO.	

PROFILE	SURVEYED	DATE
	BY	
	GRADES	
	CHECKED	
	STRUCTURE	
	NOTATIONS	
	CHKD	
	NO.	

PROFILE	SURVEYED	DATE
	BY	
	GRADES	
	CHECKED	
	STRUCTURE	
	NOTATIONS	
	CHKD	
	NO.	

TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBERG, ILLINOIS 60173
 (847) 605-9600

FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
TS0187-Ex TS Gr-e		DRAWN - MLB	REVISED -
Default		CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CENTRAL AVENUE AND WILMETTE AVENUE EXISTING TRAFFIC SIGNAL PLANS GREEN BAY ROAD AND WILMETTE AVENUE		F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE		1296	16-00199-00-RS	COOK	318	214
SHEET 27 OF 33 SHEETS		STA. TO STA.		CONTRACT NO. 61G42		
		ILLINOIS		FED. AID PROJECT		

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	48
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83999				

CABLE PLAN LEGEND

EXISTING	PROPOSED	DESCRIPTION
		8" (200mm) TRAFFIC SIGNAL SECTION
		12" (300mm) TRAFFIC SIGNAL SECTION
		12" (300mm) PEDESTRIAN SIGNAL SECTION
		12" (300mm) PEDESTRIAN SIGNAL SECTION
		CONTROLLER CABINET
		SERVICE INSTALLATION
		TELEPHONE CONNECTION
		MAGNETIC DETECTOR
		EMERGENCY VEHICLE LIGHT DETECTOR
		CONFIRMATION BEACON
		PUSHBUTTON DETECTOR
		VEHICLE DETECTOR, INDUCTION LOOP
		DENOTES NUMBER OF CONDUCTORS, ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED.
		SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD

EXISTING	PROPOSED	DESCRIPTION
		RAILROAD CONTROL CABINET
		ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
		ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"
		GROUND ROD AT CONTROLLER
		GROUND ROD AT POST OR MAST ARM POLE
		GROUND ROD AT ELECTRIC SERVICE INSTALLATION
		GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)
		FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125 2-MM12F & SM12F
		HANDHOLE/DOUBLE HANDHOLE WITH GROUNDED FRAME AND COVER
		UNINTERRUPTIBLE POWER SUPPLY

SCHEDULE OF QUANTITIES

QTY	UNIT	ITEM DESCRIPTION
4	EACH	PEDESTRIAN PUSH BUTTON
12	FOOT	CONCRETE FOUNDATION, TYPE A
13.5	FOOT	CONCRETE FOUNDATION, TYPE E, 30 INCH DIAMETER
8	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
35	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL
1	EACH	HANDHOLE
10	EACH	HEAVY DUTY HANDHOLE
718	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
4262	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
378	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
1627	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 36 FT.
1	EACH	RELOCATE EXISTING MAST ARM ASSEMBLY AND POLE
12	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
1	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
5	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
1	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, 1-3 SECTION, BRACKET MOUNTED
2	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, BRACKET MOUNTED
13	EACH	TRAFFIC SIGNAL BACKPLATE
9	EACH	INDUCTIVE LOOP DETECTOR
803	FOOT	DETECTOR LOOP, TYPE I
4	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR, AMPLIFIER
8	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
718	FOOT	ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED, SHIELDED
3	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 14 FT.
6485	FOOT	REMOVE EXISTING CABLE FROM CONDUIT
3	EACH	REMOVE EXISTING CONCRETE FOUNDATION
1	EACH	FULL - ACTUATED CONTROLLER, IN TYPE IV CABINET
1	EACH	UNINTERRUPTIBLE POWER SUPPLY, STANDARD
1	EACH	TRANSCEIVER, FIBER OPTIC
2	EACH	ILLUMINATED SIGN, LED
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING
1	EACH	MAINTAIN EXISTING TRAFFIC SIGNAL INSTALLATION

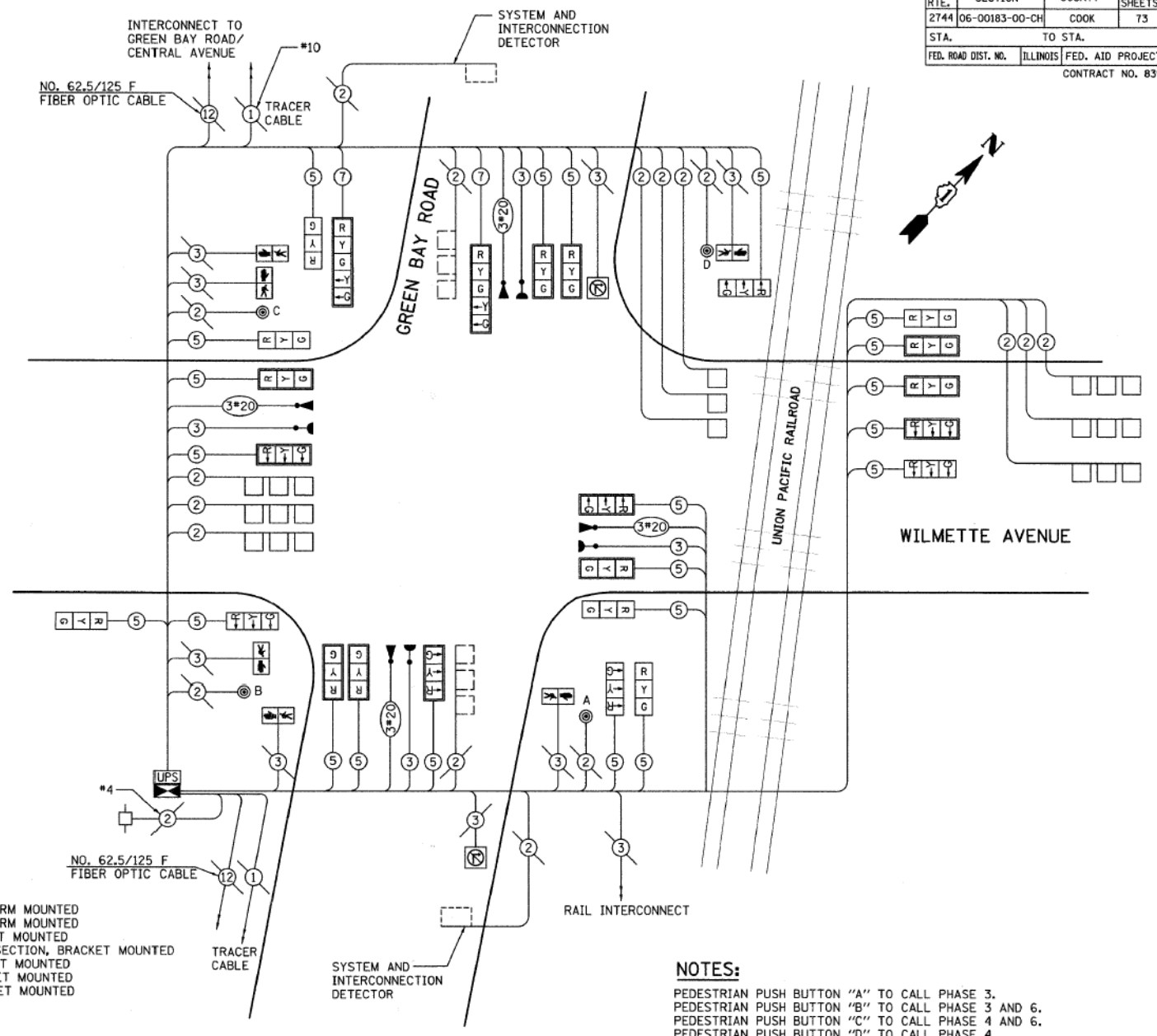
I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. LAMPS	WATTAGE LED	% OPERATIONS	TOTAL WATTAGE
SIGNAL (RED)	24	17	0.50	204
(YELLOW)	24	25	0.25	150
(GREEN)	24	15	0.25	90
ARROW	4	12	0.10	4.8
PED. SIGNAL	6	25	1.00	150
CONTROLLER	1	100	1.00	100
TL SIGN	2	25	0.05	2.5
FLASHER LED			0.05	0
TOTAL =				701.3

ENERGY COSTS- BILLED TO: VILLAGE OF WILMETTE
 (ADDRESS) 1200 WILMETTE AVENUE
 WILMETTE, IL

ENERGY SUPPLY - CONTACT _____
 PHONE _____
 COMPANY COMED

FOUNDATION DEPTH	DEPTH (FT.)	CABLE SLACK (FT.)	VERTICAL (FT.)
TYPE A - POST	4	HANDHOLE	6.5
D - CONTROLLER	4	DOUBLE HANDHOLE	13
E - MAST ARM POLE		SIGNAL POST	2
	10	CONTROLLER CAB.	1
	15	FIBER OPTIC	13
		ELECTRICAL SERVICE	1
		GROUND CABLE	1



CABLE PLAN
NOT TO SCALE

NOTES:
 PEDESTRIAN PUSH BUTTON "A" TO CALL PHASE 3.
 PEDESTRIAN PUSH BUTTON "B" TO CALL PHASE 3 AND 6.
 PEDESTRIAN PUSH BUTTON "C" TO CALL PHASE 4 AND 6.
 PEDESTRIAN PUSH BUTTON "D" TO CALL PHASE 4.

TRAFFIC SIGNAL RECORD DRAWING FOR INFORMATION ONLY

REVISIONS	
NAME	DATE

CABLE PLAN, SCHEDULE OF QUANTITIES AND PHASE DESIGNATION DIAGRAM
 WILMETTE AVENUE AT GREEN BAY ROAD
 CONSULTANT
TYLIN INTERNATIONAL

VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
DRAWN: BK	SHEET NO.
CHECKED: DAJ	
APPROVED: DAJ	
DATE: 02-14-2008	
SCALE: NONE	
JOB NO.:	PROJECT NO.:

PLAN	SURVEYED	DATE
NOTE BOOK NO.	BY	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	BY	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	BY	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	BY	

TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBERG, ILLINOIS 60173
 (847) 605-9600

FILE NAME = TS0188-Ex Cable	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 40.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/13/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CENTRAL AVENUE AND WILMETTE AVENUE EXISTING TRAFFIC SIGNAL CABLE PLANS GREEN BAY ROAD AND WILMETTE AVENUE	F.A.U. R.T.E. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 215
SCALE: NONE	SHEET 28	OF 33 SHEETS	STA. TO STA.	CONTRACT NO. 61G42	
ILLINOIS FED. AID PROJECT					

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS

																PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 5	PREEMPTOR NUMBER 6						
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1	1	1	5	5	9	9	12	12										CLEAR TO NORMAL SEQUENCE						
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	1T	1U	2	3	4	5		
CHANGE TO EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	3	1F	4 OR 5	1H	1J	2, 4 OR 5	3	2	1N	3, 4 OR 5	1Q	1R	2	1T	1U	3, 4 OR 5						◆
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS N/B	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	G	Y	R	G	R	R	R	◆	
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS N/B	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	G	Y	R	G	R	R	R	◆	
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS S/B	R	R	R	R	R	R	G	Y	R	G	R	R	R	G	Y	R	G	Y	R	R	G	R	R	◆	
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS S/B	←Y	←R	←G	←G	←Y	←R	←G	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	←R	◆	
WILMETTE AVENUE FAR RIGHT AND CENTER MAST ARM SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	◆	
WILMETTE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS W/B	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	◆	
WILMETTE AVENUE (EAST OF TRACKS) LEFT CANTILEVER AND FAR LEFT MEDIAN SIGNALS W/B	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	◆	
WILMETTE AVENUE (EAST OF TRACKS) FAR RIGHT AND CENTER CANTILEVER SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	◆	
WILMETTE AVENUE NEAR RIGHT CENTER AND FAR RIGHT MAST ARM SIGNALS E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	◆	
WILMETTE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS E/B	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	◆	
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON SOUTH SIDE OF WILMETTE AVENUE (S. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	◆	
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON NORTH SIDE OF WILMETTE AVENUE (N. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	◆	
PEDESTRIAN SIGNALS CROSSING WILMETTE AVENUE ON WEST SIDE OF GREEN BAY ROAD (W. LEG)	H	H	H	H	H	H	FH	H	H	FH	H	H	H	FH	H	H	FH	H	H	H	H	H	H	◆	

P = ILLUMINATED PERSON = WALK
 FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
 H = ILLUMINATED SOLID HAND = DON'T WALK
 ◆ = EMERGENCY VEHICLE SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY VEHICLE INTERVAL AFTER EMERGENCY VEHICLE INTERVAL 2 OR 3 IS TERMINATED.

TRAFFIC SIGNAL RECORD DRAWING FOR INFORMATION ONLY

REVISIONS NAME DATE		EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS SHEET 1 OF 2 WILMETTE AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
CONSULTANT TYLIN INTERNATIONAL			DRAWN: _____ CHECKED: _____ APPROVED: _____ DATE: 02-14-2008 SCALE: NONE JOB NO.: _____	SHEET NO.: _____ PROJECT NO.: _____

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	ALIGNMENT CHECKED	
	STRUCTURE NOTATIONS CHECKED	
NOTE BOOK NO.	CADD FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	ALIGNMENT CHECKED	
	STRUCTURE NOTATIONS CHECKED	
NOTE BOOK NO.	CADD FILE NAME	

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	ALIGNMENT CHECKED	
	STRUCTURE NOTATIONS CHECKED	
NOTE BOOK NO.	CADD FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	ALIGNMENT CHECKED	
	STRUCTURE NOTATIONS CHECKED	
NOTE BOOK NO.	CADD FILE NAME	

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 (847) 605-9600

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS

CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	15		15		15		19		19		22		22		25		25		25		PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 5	PREEMPTOR NUMBER 6	CLEAR TO NORMAL SEQUENCE													
	1V	1W	1X	1Y	1Z	1AA	1BB	1CC	1DD	1EE	1FF	1GG	1HH	1JJ	1KK	1LL	1MM	1NN	1PP	1QQ	1RR	1SS	1TT	1UU		1VV	1WW	1XX	1YY	1ZZ	1AAA	1BBB	1CCC	1DDD	1	2	3	4
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1V	1W	1X	1Y	1Z	1AA	1BB	1CC	1DD	1EE	1FF	1GG	1HH	1JJ	1KK	1LL	1MM	1NN	1PP	1QQ	1RR	1SS	1TT	1UU	1VV	1WW	1XX	1YY	1ZZ	1AAA	1BBB	1CCC	1DDD	1	2	3	4	5
CHANGE TO EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1W	1X	1Y	2 OR 3	1AA	4	1CC	1DD	1EE	5	1GG	1HH	1JJ	1KK	2, 3 OR 5	4	1NN	1PP	2, 3 OR 4	5	1SS	1TT	1UU	1VV	2 OR 3	1XX	1YY	4	1AAA	1BBB	1CCC	1DDD	5					
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	
WILMETTE AVENUE FAR RIGHT AND CENTER MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	G	G	G	Y	R	G	R	R	R	R	G	G	G	Y	R	G	G	G	G	G	G	Y	R	R	R	R	R	
WILMETTE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS	←G	←G	←Y	←R	←G	←G	←G	←G	←Y	←R	←G	←G	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	
WILMETTE AVENUE (EAST OF TRACKS) LEFT CANTILEVER AND FAR LEFT MEDIAN SIGNALS	←Y	←R	←R	←R	←G	←G	←Y	←R	←R	←R	←G	←Y	←R	←R	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	
WILMETTE AVENUE (EAST OF TRACKS) FAR RIGHT AND CENTER CANTILEVER SIGNALS	R	R	R	R	R	R	R	R	R	R	G	Y	R	R	R	G	R	R	R	R	G	Y	R	R	G	G	G	G	Y	R	R	R	R	R	R	R	R	
WILMETTE AVENUE NEAR RIGHT CENTER AND FAR RIGHT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	Y	R	G	Y	R	G	G	G	G	G	G	G	G	R	R	
WILMETTE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS	←G	←G	←Y	←R	←Y	←R	←G	←G	←G	←G	←R	←R	←R	←R	←R	←G	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON SOUTH SIDE OF WILMETTE AVENUE (S. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	FH	H	H	FH	FH	H	H	H	H	FH	H	H	FH	H	H	H	H	H	H	H	H	H	
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON NORTH SIDE OF WILMETTE AVENUE (N. LEG)	H	H	H	H	H	H	H	H	H	H	FH	H	H	H	H	FH	H	H	H	FH	H	H	H	FH	H	H	FH	H	H	H	H	H	H	H	H	H	H	
PEDESTRIAN SIGNALS CROSSING WILMETTE AVENUE ON WEST SIDE OF GREEN BAY ROAD (W. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	

P = ILLUMINATED PERSON = WALK
 FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
 H = ILLUMINATED SOLID HAND = DON'T WALK
 ◆ = EMERGENCY VEHICLE SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY VEHICLE INTERVAL AFTER EMERGENCY VEHICLE INTERVAL 2 OR 3 IS TERMINATED.

TRAFFIC SIGNAL RECORD DRAWING FOR INFORMATION ONLY

REVISIONS NAME DATE		EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS SHEET 2 OF 2 WILMETTE AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
CONSULTANT TYLIN INTERNATIONAL			DRAWN: _____ CHECKED: _____ APPROVED: _____ DATE: 02-14-2008 SCALE: NONE JOB NO.: _____	SHEET NO.: _____ PROJECT NO.: _____

PLAN SURVEYED PLOTTED CHECKED
 NOTE BOOK NO. _____
 DATE _____

PROFILE SURVEYED PLOTTED CHECKED
 NOTE BOOK NO. _____
 DATE _____

PROFILE SURVEYED PLOTTED CHECKED
 NOTE BOOK NO. _____
 DATE _____

PROFILE SURVEYED PLOTTED CHECKED
 NOTE BOOK NO. _____
 DATE _____



FILE NAME = TS0191-Ex Emerge	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 40.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/13/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE AND WILMETTE AVENUE
EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS
GREEN BAY ROAD AND WILMETTE AVENUE**

PROPOSED RAILROAD PREEMPTION SEQUENCE OF OPERATIONS

																PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 5	PREEMPTOR NUMBER 2												
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1	5	9	12	15	19	22	25																2	3	4	5					
CHANGE FROM EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER																																
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	1T	1U	1V	1W	1X	1Y	1Z	2	3	4	5				
CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	2	1F	2	1H	2	2	2	1M	2	1P	2	1R	2	1T	2	1V	2	1X	2	1Z	2	3	4	5	▲				
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	G	▲			
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	G	▲			
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS	R	R	Y	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	G	▲			
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	←Y	←R	▲			
WILMETTE AVENUE FAR RIGHT AND CENTER MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	G	G	R	R	G	G	R	R	G	G	G	R	R	G	Y	R	R	▲				
WILMETTE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS	←R	←R	←R	←R	←R	←R	←R	←R	←G	←G	←G	←G	←R	←R	←R	←R	←R	←R	←G	←G	←R	←R	←G	←Y	←R	←R	▲					
WILMETTE AVENUE (EAST OF TRACKS) LEFT CANTILEVER AND FAR LEFT MEDIAN SIGNALS	←R	←R	←R	←R	←R	←R	←R	←Y	←R	←Y	←R	←R	←R	←R	←R	←R	←R	←R	←Y	←R	←R	←R	←R	←R	←R	←R	▲					
WILMETTE AVENUE (EAST OF TRACKS) FAR RIGHT AND CENTER CANTILEVER SIGNALS	R	R	R	R	R	R	R	R	R	R	Y	R	R	Y	R	R	R	R	Y	R	R	R	R	R	R	R	R	▲				
WILMETTE AVENUE NEAR RIGHT CENTER AND FAR RIGHT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R	▲				
WILMETTE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS	←R	←R	←R	←R	←R	←R	←R	←Y	←R	←R	←R	←Y	←R	←R	←R	←R	←R	←R	←R	←Y	←R	←R	←Y	←R	←R	←R	←R	▲				
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON SOUTH SIDE OF WILMETTE AVENUE (S. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	FH	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	▲				
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON NORTH SIDE OF WILMETTE AVENUE (N. LEG)	H	H	H	H	H	H	H	H	H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	▲				
PEDESTRIAN SIGNALS CROSSING WILMETTE AVENUE ON WEST SIDE OF GREEN BAY ROAD (W. LEG)	H	H	FH	H	H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	▲				
INTERNALLY ILLUMINATED NRT SIGNS - GREEN BAY ROAD	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	▲				

HOLD

▲ RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL 2, 3 OR 4 (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.

P = ILLUMINATED PERSON = WALK
 FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
 H = ILLUMINATED SOLID HAND = DON'T WALK

TRAFFIC SIGNAL RECORD DRAWING FOR INFORMATION ONLY

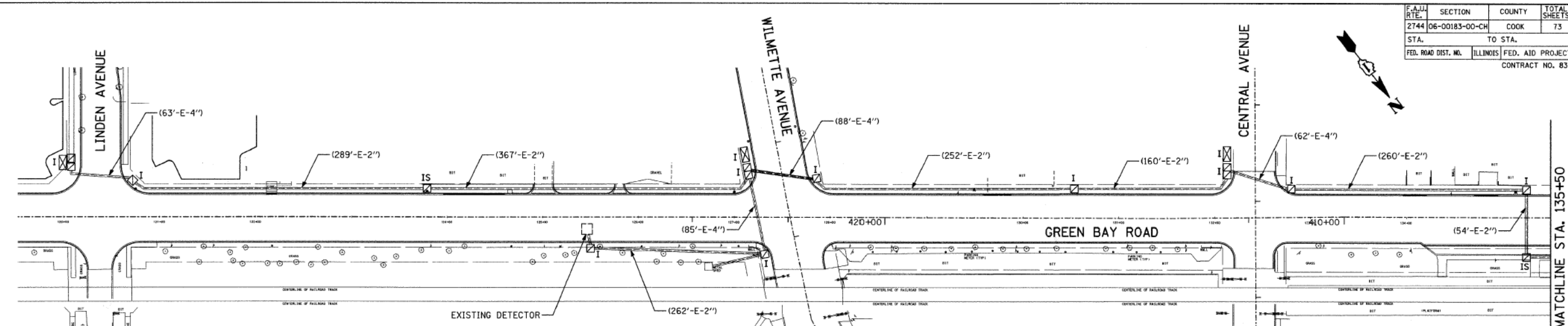
REVISIONS NAME DATE 		RAILROAD PREEMPTION SEQUENCE OF OPERATIONS WILMETTE AVENUE AT GREEN BAY ROAD CONSULTANT TYLIN INTERNATIONAL	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS DRAWN: _____ SHEET NO. _____ CHECKED: _____ APPROVED: _____ DATE: 02-14-2008 SCALE: NONE JOB NO.: _____ PROJECT NO.: _____
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PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	
	GRADES CHECKED	
	ALIGNMENT CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	CADD FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	
	GRADES CHECKED	
	ALIGNMENT CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	CADD FILE NAME	

TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBURG, ILLINOIS 60173
 (847) 605-9600

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	58
STA.	TO STA.			
	ILLINOIS		FED. AID PROJECT	
FED. ROAD DIST. NO.		CONTRACT NO. 83999		



INTERCONNECT SCHEMATIC LEGEND

- EXISTING INTERSECTION CONTROLLER
- PROPOSED INTERSECTION CONTROLLER
- PROPOSED MASTER CONTROLLER
- EXISTING INTERSECTION & SAMPLING (SYSTEM) DETECTORS
- EXISTING SAMPLING (SYSTEM) PREFORMED DETECTORS
- EXISTING INTERCONNECT CABLE - NO. 62.5/125 12F FIBER OPTIC CABLE
- PROPOSED INTERCONNECT CABLE - NO. 62.5/125 12F FIBER OPTIC CABLE

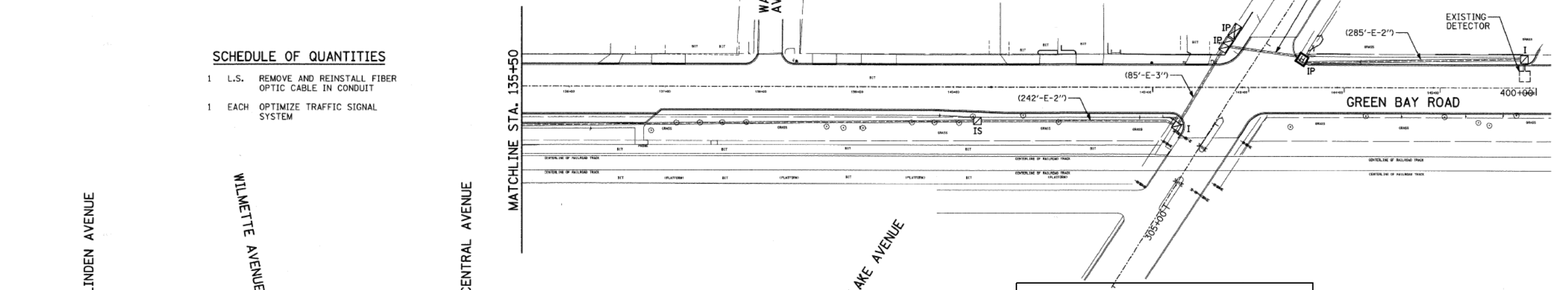
- EXISTING LOOP DETECTOR CABLE 2/C TWISTED, SHIELDED
- PROPOSED LOOP DETECTOR CABLE 2/C TWISTED, SHIELDED
- EXISTING ELECTRIC CABLE, 1/C (AS SPECIFIED)
- PROPOSED ELECTRIC CABLE, 1/C (AS SPECIFIED)
- EXISTING TELEPHONE CONNECTION
- PROPOSED TELEPHONE CONNECTION

INTERCONNECT PLAN LEGEND

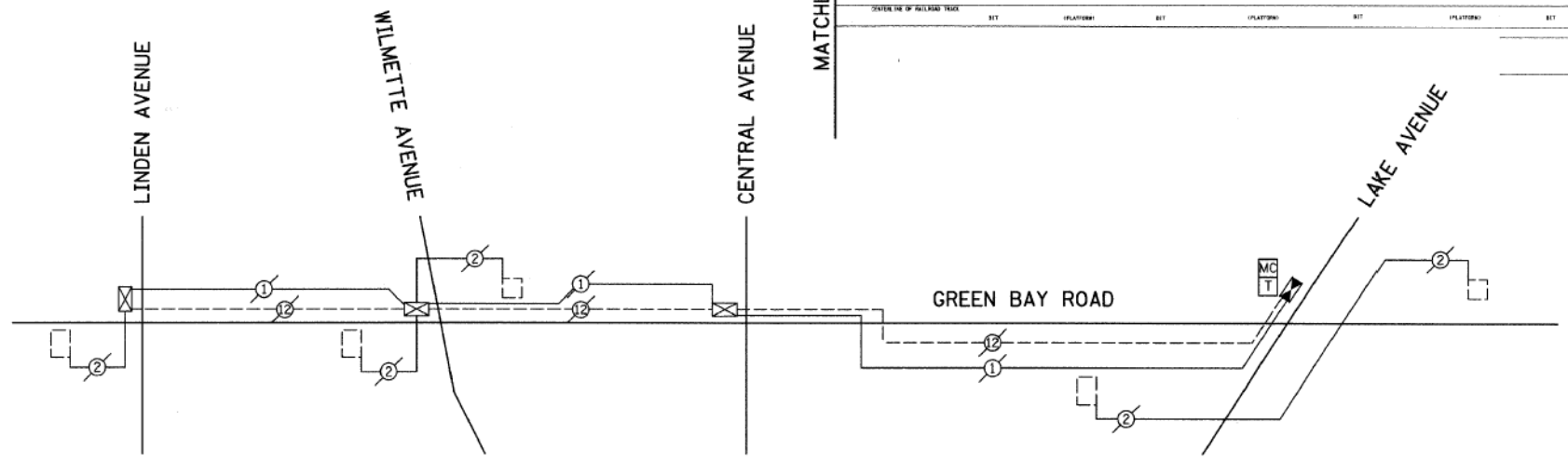
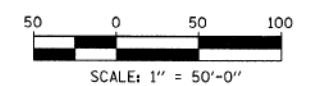
- | PROPOSED | EXISTING | |
|----------|----------|---------------------------------------|
| [Symbol] | [Symbol] | CONTROLLER |
| [Symbol] | [Symbol] | HANDHOLE |
| [Symbol] | [Symbol] | DOUBLE HANDHOLE |
| [Symbol] | [Symbol] | HEAVY DUTY HANDHOLE |
| [Symbol] | [Symbol] | G.S. CONDUIT IN TRENCH(OR PUSHED (P)) |
| [Symbol] | [Symbol] | DETECTOR LOOP |
| [Symbol] | [Symbol] | UNIT DUCT |
| [Symbol] | [Symbol] | SYSTEM |
| [Symbol] | [Symbol] | INTERSECTION |

SCHEDULE OF QUANTITIES

- 1 L.S. REMOVE AND REINSTALL FIBER OPTIC CABLE IN CONDUIT
- 1 EACH OPTIMIZE TRAFFIC SIGNAL SYSTEM



TRAFFIC SIGNAL RECORD DRAWING FOR INFORMATION ONLY



INTERCONNECT SCHEMATIC DIAGRAM
NOT TO SCALE

REVISIONS	
NAME	DATE

INTERCONNECT PLAN INTERCONNECT SCHEMATIC DIAGRAM

CONSULTANT
TYLIN INTERNATIONAL

VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
DRAWN: BK	SHEET NO.
CHECKED: DAJ	
APPROVED: DAJ	
DATE: 02-14-2008	
SCALE: 1"=50'-0"	
JOB NO.:	PROJECT NO.:

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	ALIGNED		
	CHECKED		
	NOTED		
	FILED		
	CAD FILE NAME		
	NO.		

PROFILE	SURVEYED	BY	DATE
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	NO.		

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	ALIGNED		
	CHECKED		
	NOTED		
	FILED		
	CAD FILE NAME		
	NO.		

PROFILE	SURVEYED	BY	DATE
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	NO.		

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SCHAMBURG, ILLINOIS 60173
(847) 605-9600

FILE NAME = TS0193-Ex Interconnect	USER NAME = Mibeening	DESIGNED - MLB	REVISED -
Default		DRAWN - MLB	REVISED -
		CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CENTRAL AVENUE AND WILMETTE AVENUE EXISTING INTERCONNECT PLANS GREEN BAY ROAD			
SCALE: NONE	SHEET 33	OF 33 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	220
ILLINOIS		FED. AID PROJECT		
CONTRACT NO. 61G42				

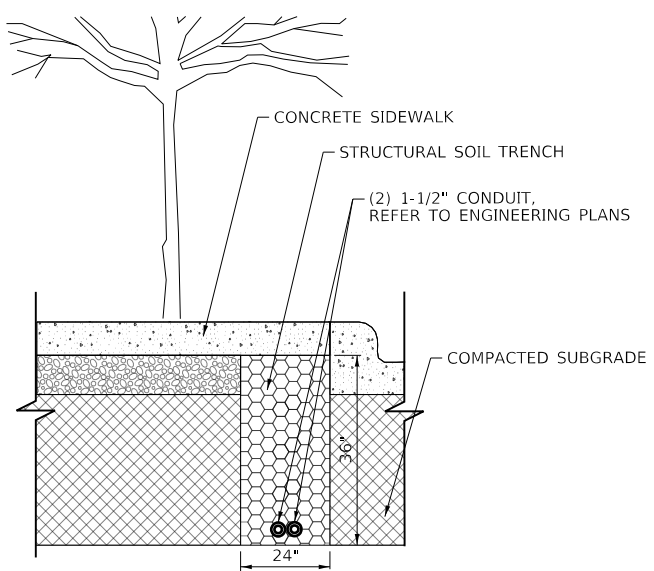
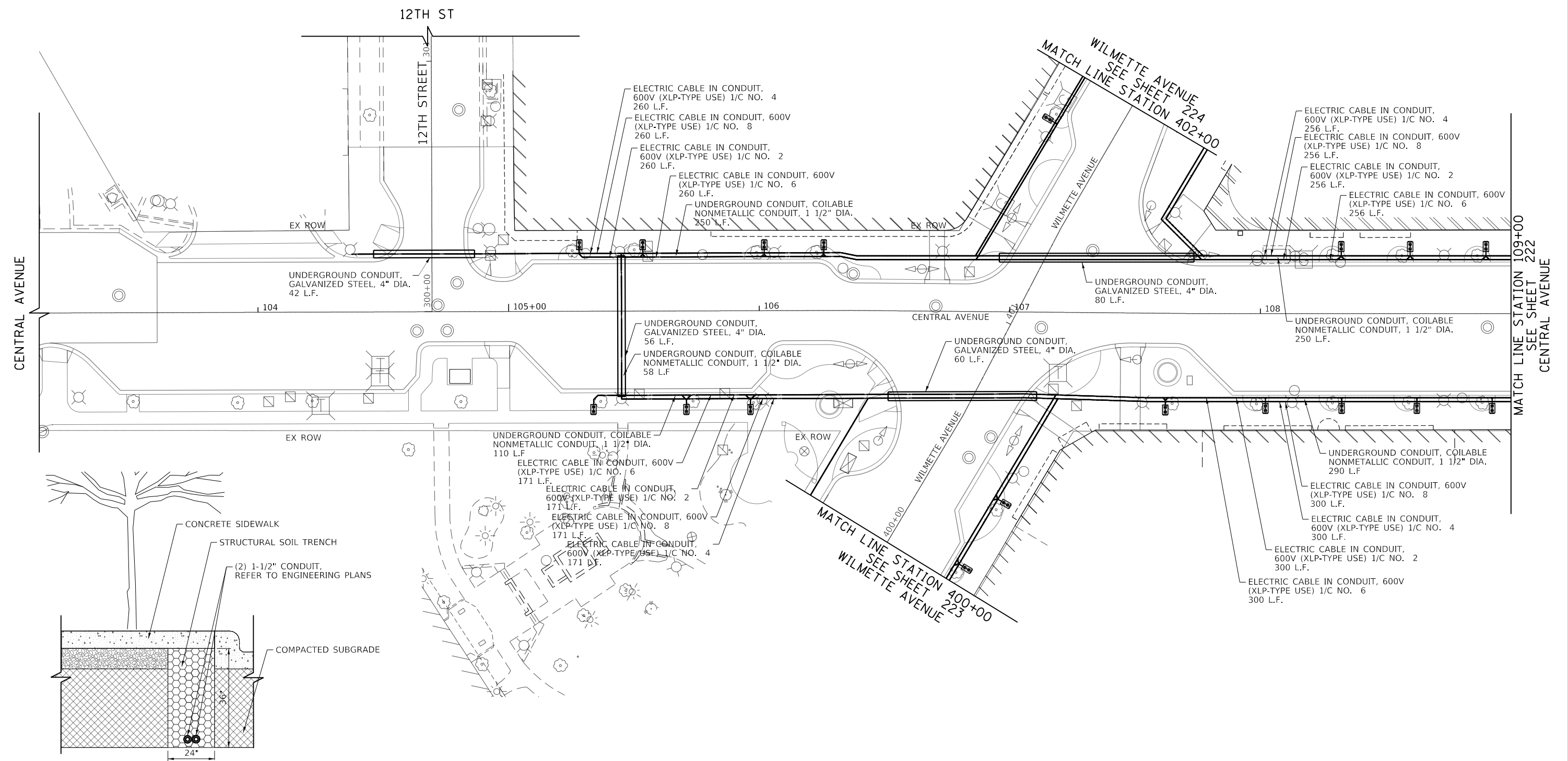


NOTES

1. THERE SHALL BE 2 SEPARATE 1-1/2" CONDUITS, ONE FOR STREET LIGHTING CIRCUITS AND ONE FOR FESTOON OUTLET CIRCUITS. THE 2 CONDUIT SHALL BE PLACED IN A COMMON TRENCH OR BORED TOGETHER.
2. THE NUMBER AND SIZE OF THE CONDUCTORS IS BASED UPON RECORD DRAWINGS. THE CONTRACTOR SHALL VERIFY THE NUMBER AND SIZE OF CONDUCTORS, AS WELL AS THE COLOR OF THE INSULATION PRIOR TO ORDERING MATERIALS. QUANTITIES NEEDED SHALL ALSO BE VERIFIED.
3. WHERE CROSSING UNDER A ROADWAY OR ALLEY, THE COILABLE NONMETALLIC CONDUIT SHALL BE SLEEVED WITH A GALVANIZED STEEL CONDUIT.
4. THE FESTOON OUTLETS SHALL HAVE A COVER THAT IS WEATHERPROOF WITH A PLUG INSERTED IN THE OUTLET.

PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
	CHECKED	
	ALIGNMENT	
	CONDUIT	
	CARD FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
	CHECKED	
	GRADES	
	STRUCTURE	
	NOTATIS CHFD	



COILABLE NON-METALLIC IN TRENCH, 1-1/2" DETAIL



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FILE NAME = 180000-ELEC	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 40.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE AND WILMETTE AVENUE
 LIGHTING AND ELECTRICAL PLAN**

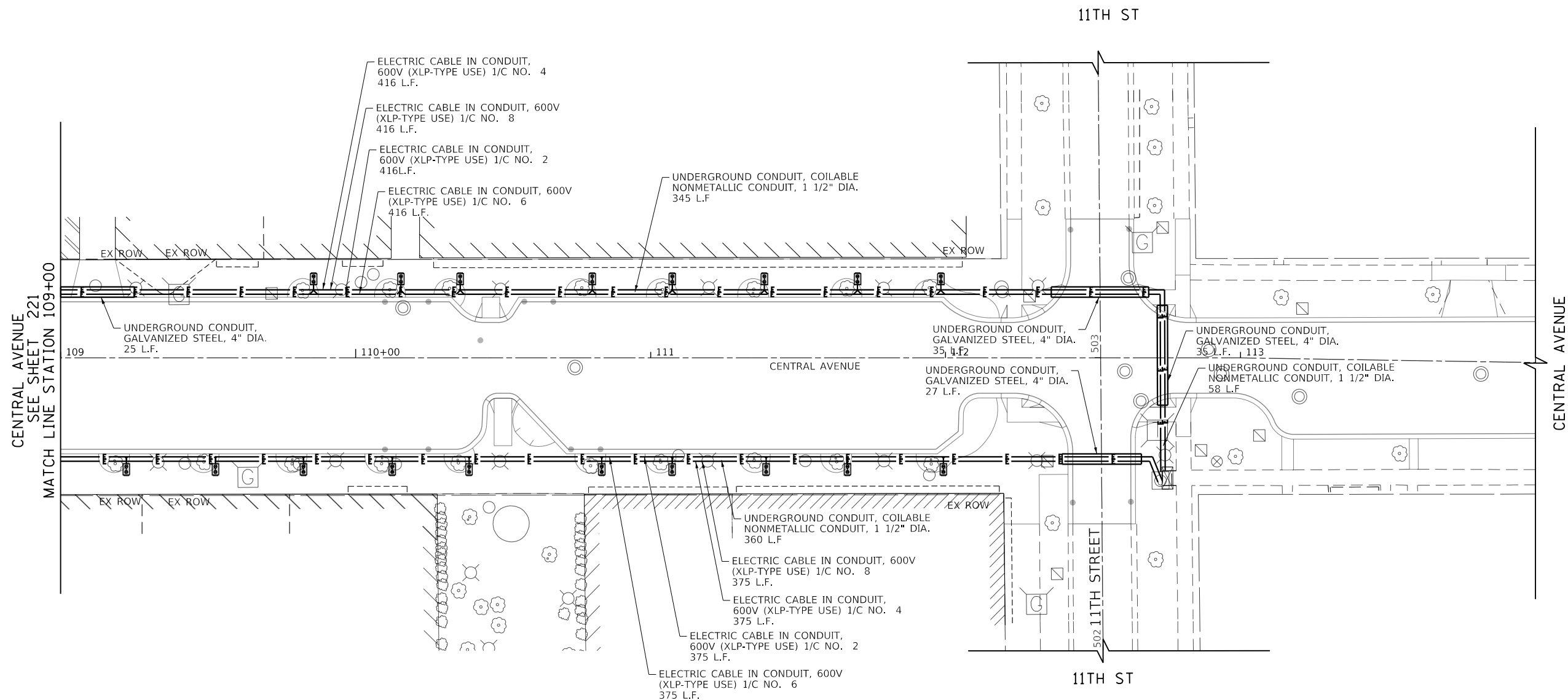
SCALE: 1"=20' SHEET 1 OF 15 SHEETS STA. 103+00.00 TO STA. 109+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	221
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61G42	



PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	CHECKED		
	ALIGNED		
	CAD FILE NAME		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO.	GRADES CHECKED		
	STRUCTURE NOTATIONS CHKD		



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 SCHAMBURG, ILLINOIS 60173
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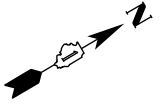
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	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE AND WILMETTE AVENUE
 LIGHTING AND ELECTRICAL PLAN**

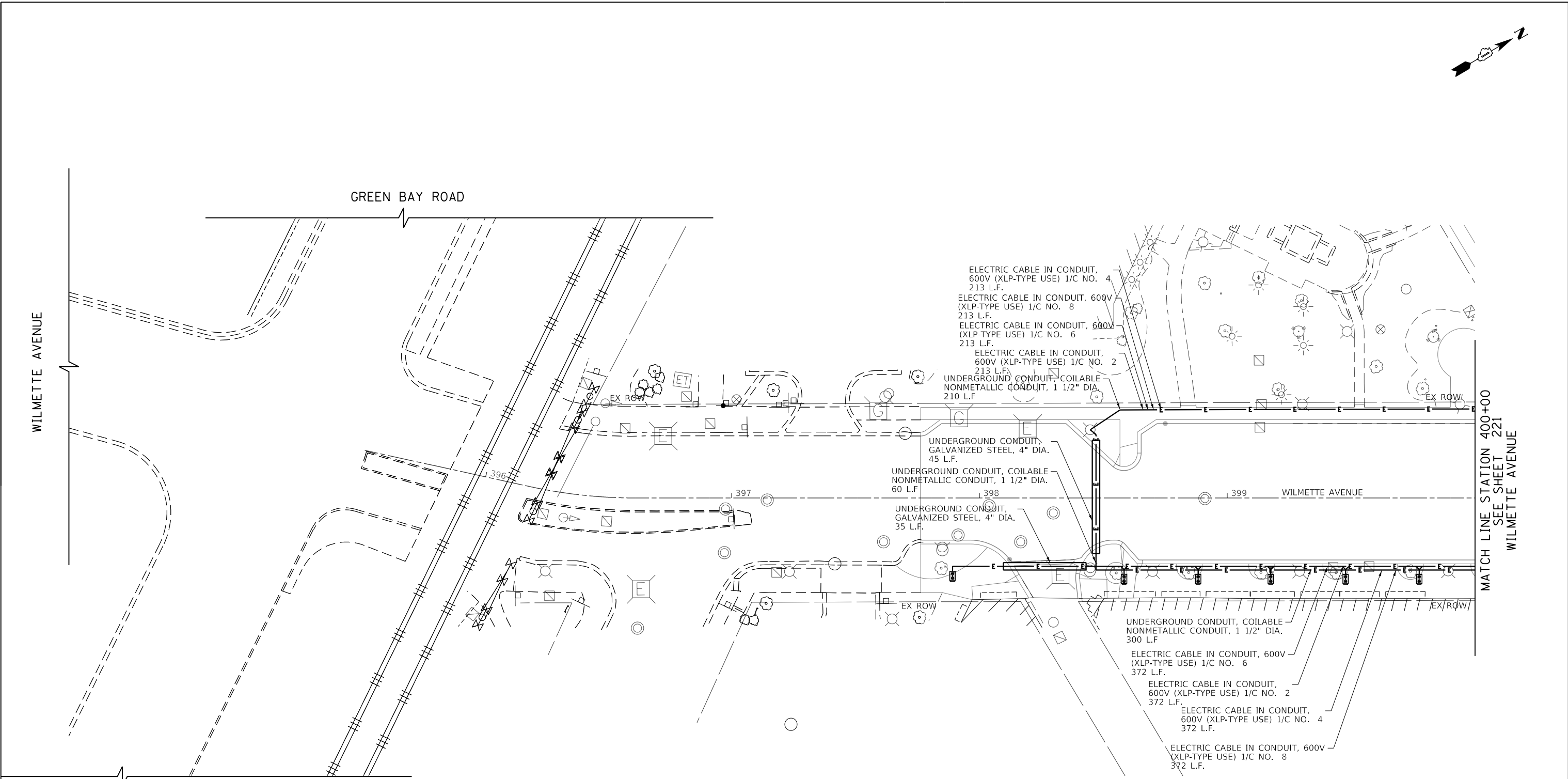
SCALE: 1"=20' SHEET 2 OF 15 SHEETS STA. 110+00.00 TO STA. 115+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	222
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				



PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
	CHECKED	
	ALIGNMENT	
	STRUCTURE	
	NOTATIS CHFD	
	FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	GRADES CHECKED	BY
	STRUCTURE	
	NOTATIS CHFD	



WILMETTE AVENUE

GREEN BAY ROAD

GREEN BAY ROAD

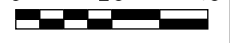
MATCH LINE STATION 400+00
SEE SHEET 221
WILMETTE AVENUE

ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 4
213 L.F.
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 8
213 L.F.
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6
213 L.F.
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 2
213 L.F.
UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 1 1/2" DIA.
210 L.F.

UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
45 L.F.
UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 1 1/2" DIA.
60 L.F.
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
35 L.F.

UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 1 1/2" DIA.
300 L.F.
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6
372 L.F.
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 2
372 L.F.
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 4
372 L.F.
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 8
372 L.F.

HORIZ. 0 20 40



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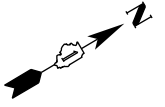
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Default	PLOT SCALE = 40.0004' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE AND WILMETTE AVENUE
LIGHTING AND ELECTRICAL PLAN**

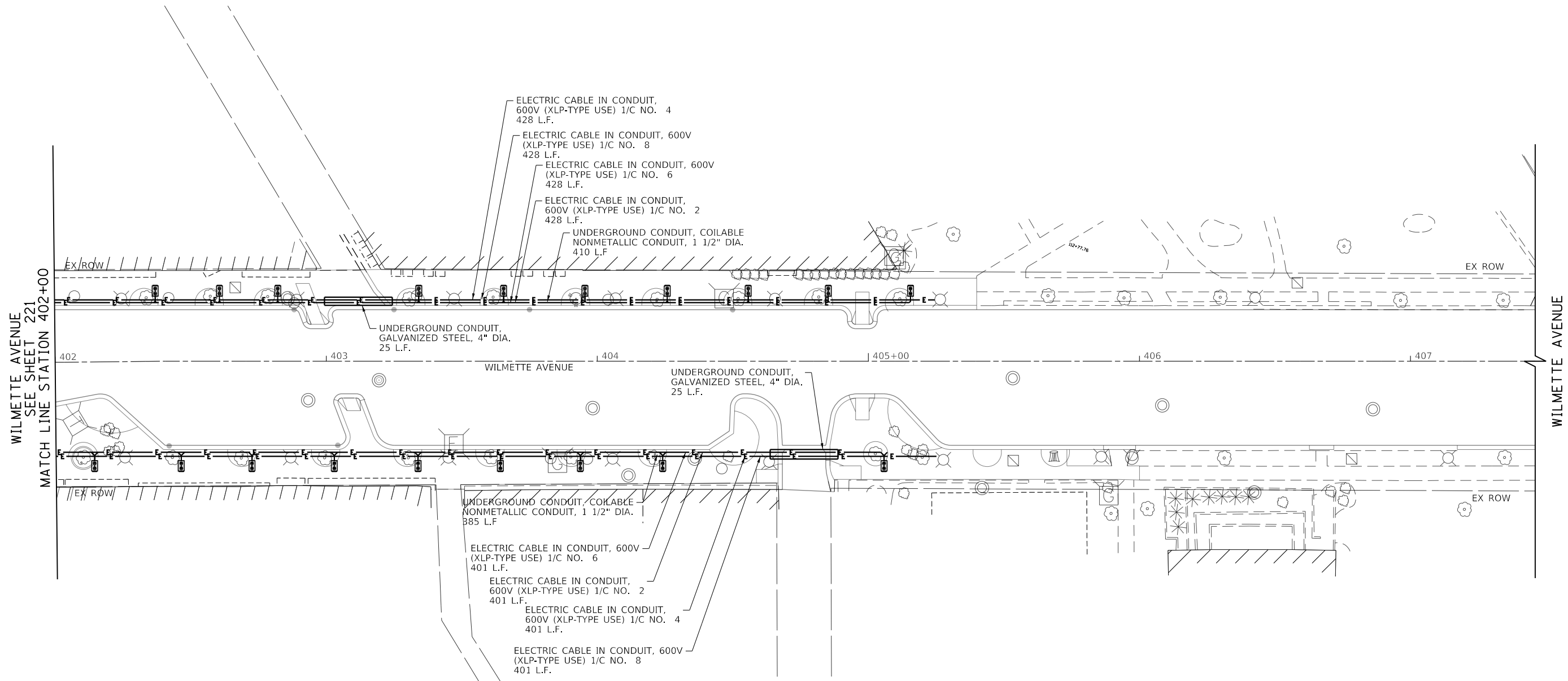
SCALE: 1"=20' SHEET 3 OF 15 SHEETS STA. 397+00.00 TO STA. 400+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	223
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				



PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
	CHECKED	
	ALIGNED	
	CAD FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	GRADES CHECKED	BY
	STRUCTURE NOTATIONS CHKD	



WILMETTE AVENUE
MATCH LINE STATION 402+00

WILMETTE AVENUE



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1475 EAST WOODFIELD ROAD, SUITE 600
SCHAUMBURG, ILLINOIS 60173
(847) 605-9600

FILE NAME = 180080-ELEC	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 40.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

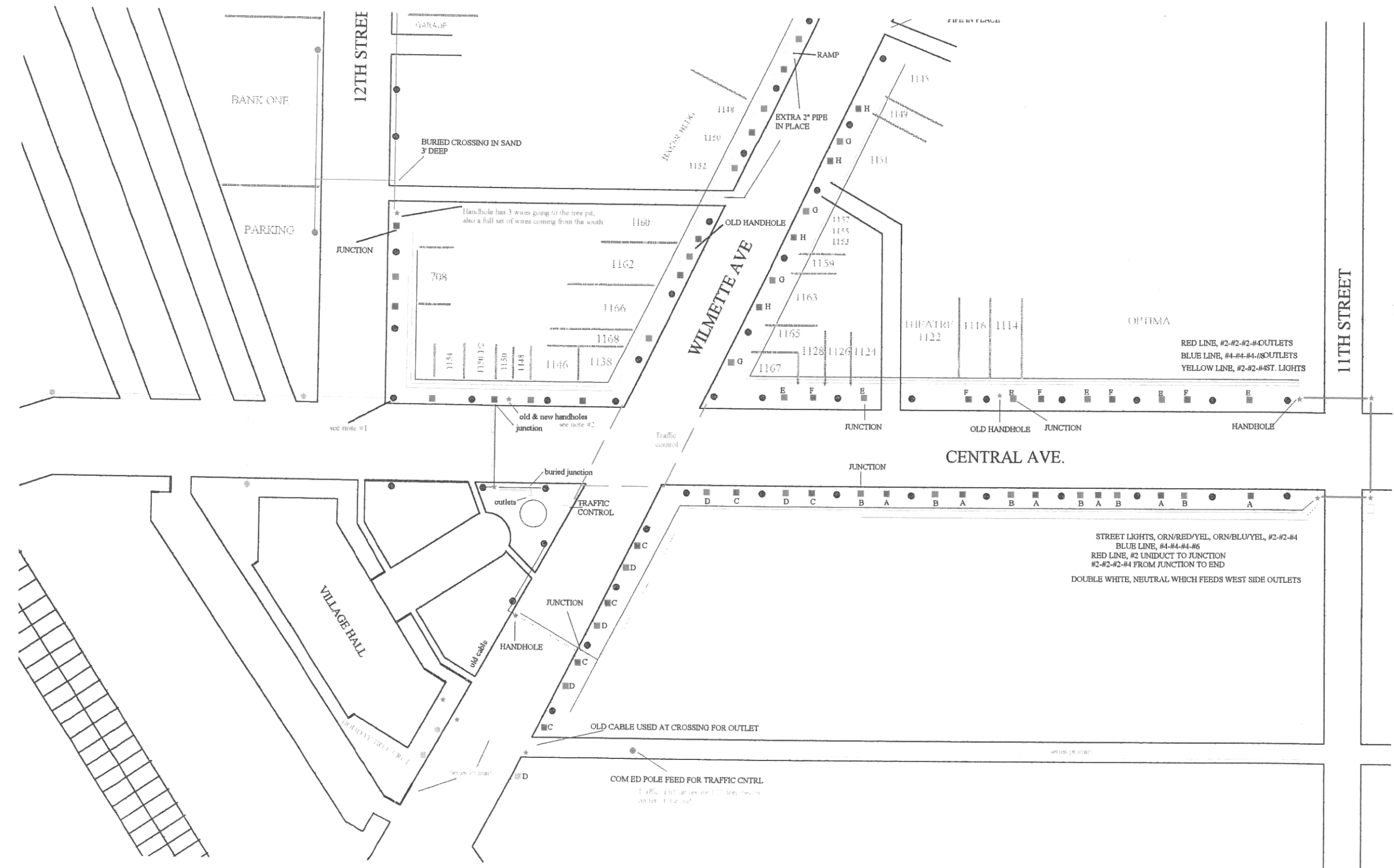
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE AND WILMETTE AVENUE
LIGHTING AND ELECTRICAL PLAN**

SCALE: 1"=20' SHEET 4 OF 15 SHEETS STA. 402+00.00 TO STA. 407+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	224
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				

FOR REFERENCE ONLY



PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
	CHECKED	
	ALIGNED	
	CAD FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	GRADES CHECKED	BY
	STRUCTURE NOTATIONS CHECKED	

TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBURG, ILLINOIS 60173
 (847) 605-9600

FILE NAME = 180080-ELEC	USER NAME = Mibeening	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 20.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE AND WILMETTE AVENUE
 EXISTING LIGHTING AND ELECTRICAL SYSTEMS**

SCALE: NONE SHEET 5 OF 15 SHEETS STA. TO STA.

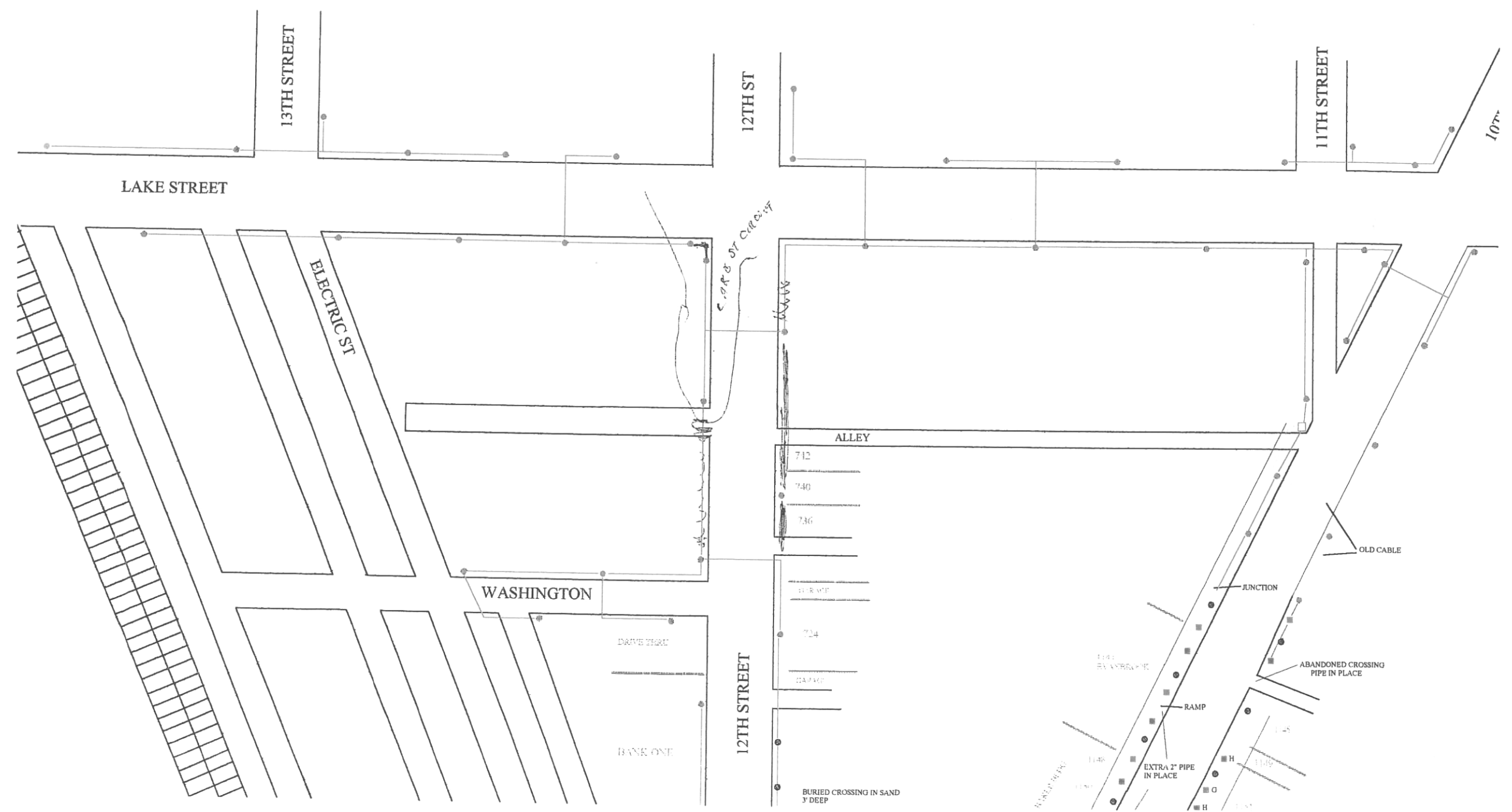
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	225
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				

FOR REFERENCE ONLY

PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	CHECKED		
	ALIGNMENT		
	FILE NAME		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO.	GRADES CHECKED		
	STRUCTURE		
	NOTATIONS		

TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBURG, ILLINOIS 60173
 (847) 605-9600



FILE NAME = 180080-ELEC	USER NAME = Mibeening	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 20.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

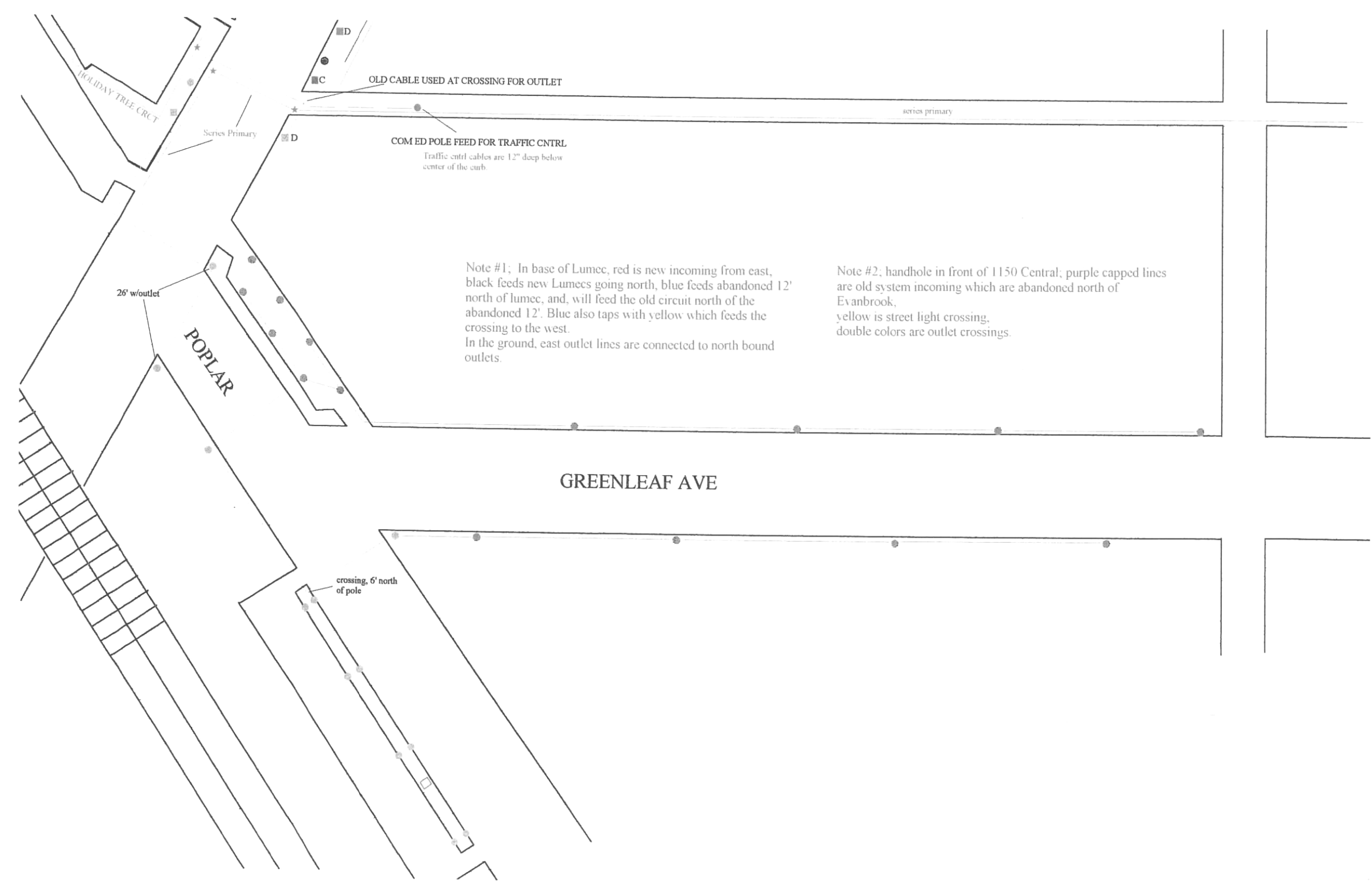
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE AND WILMETTE AVENUE
 EXISTING LIGHTING AND ELECTRICAL SYSTEMS**

SCALE: NONE SHEET 6 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	226
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				

FOR REFERENCE ONLY



PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	ALIGNED		
	CAD FILE NAME		
NOTE BOOK NO.			

PROFILE	SURVEYED	BY	DATE
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
NOTE BOOK NO.			

TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBURG, ILLINOIS 60173
 (847) 605-9600

FILE NAME = 180080-ELEC	USER NAME = Mibeening	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 20.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

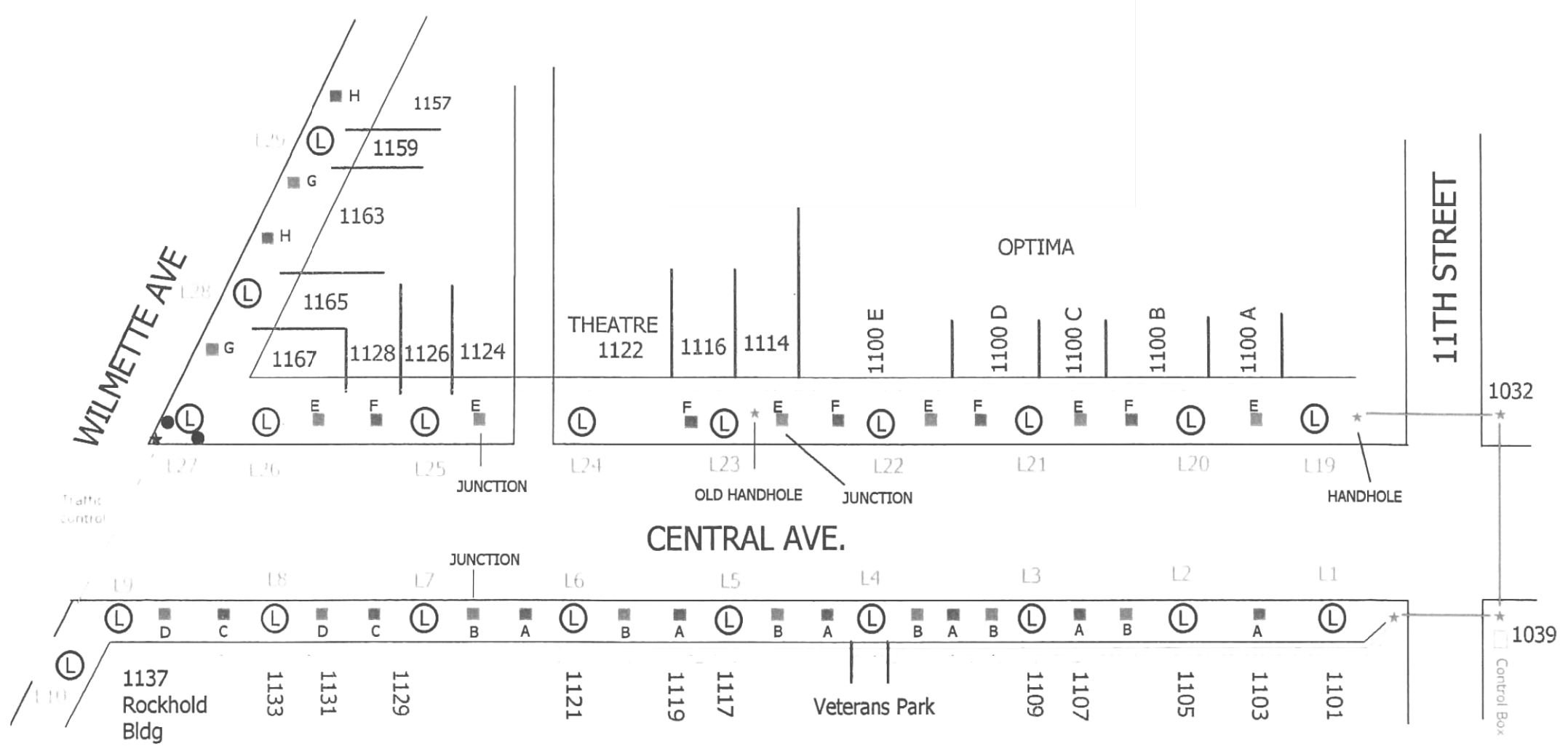
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CENTRAL AVENUE AND WILMETTE AVENUE EXISTING LIGHTING AND ELECTRICAL SYSTEMS	
SCALE: NONE	SHEET 7 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	227
CONTRACT NO. 61G42				
ILLINOIS FED. AID PROJECT				

FOR REFERENCE ONLY

Central Business District



PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	
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	BY	
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PROFILE	SURVEYED	DATE
NOTE BOOK NO.	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	BY	
	DATE	

TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBURG, ILLINOIS 60173
 (847) 605-9600

FILE NAME = 180080-ELEC	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 20.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

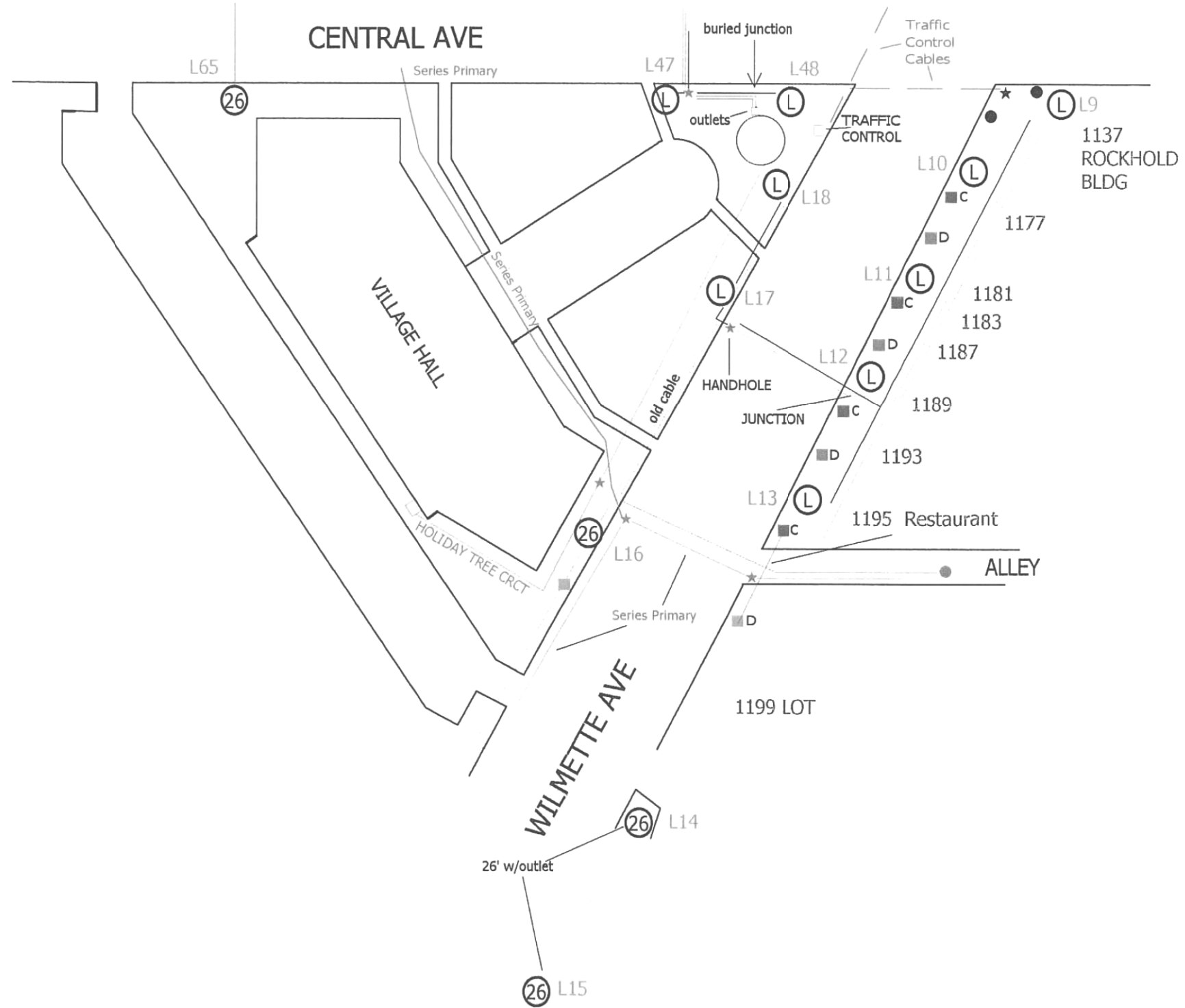
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CENTRAL AVENUE AND WILMETTE AVENUE EXISTING LIGHTING AND ELECTRICAL SYSTEMS	
SCALE: NONE	SHEET 8 OF 15 SHEETS STA. TO STA.

F.A.U. RTE. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 228
CONTRACT NO. 61G42				ILLINOIS FED. AID PROJECT

FOR REFERENCE ONLY

Central Business District



PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	
	CHECKED	
	ALIGNMENT	
	FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	GRADES CHECKED	
	STRUCTURE	
	NOTATIS CHFD	

TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBURG, ILLINOIS 60173
 (847) 605-9600

FILE NAME = 180080-ELEC	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 20.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

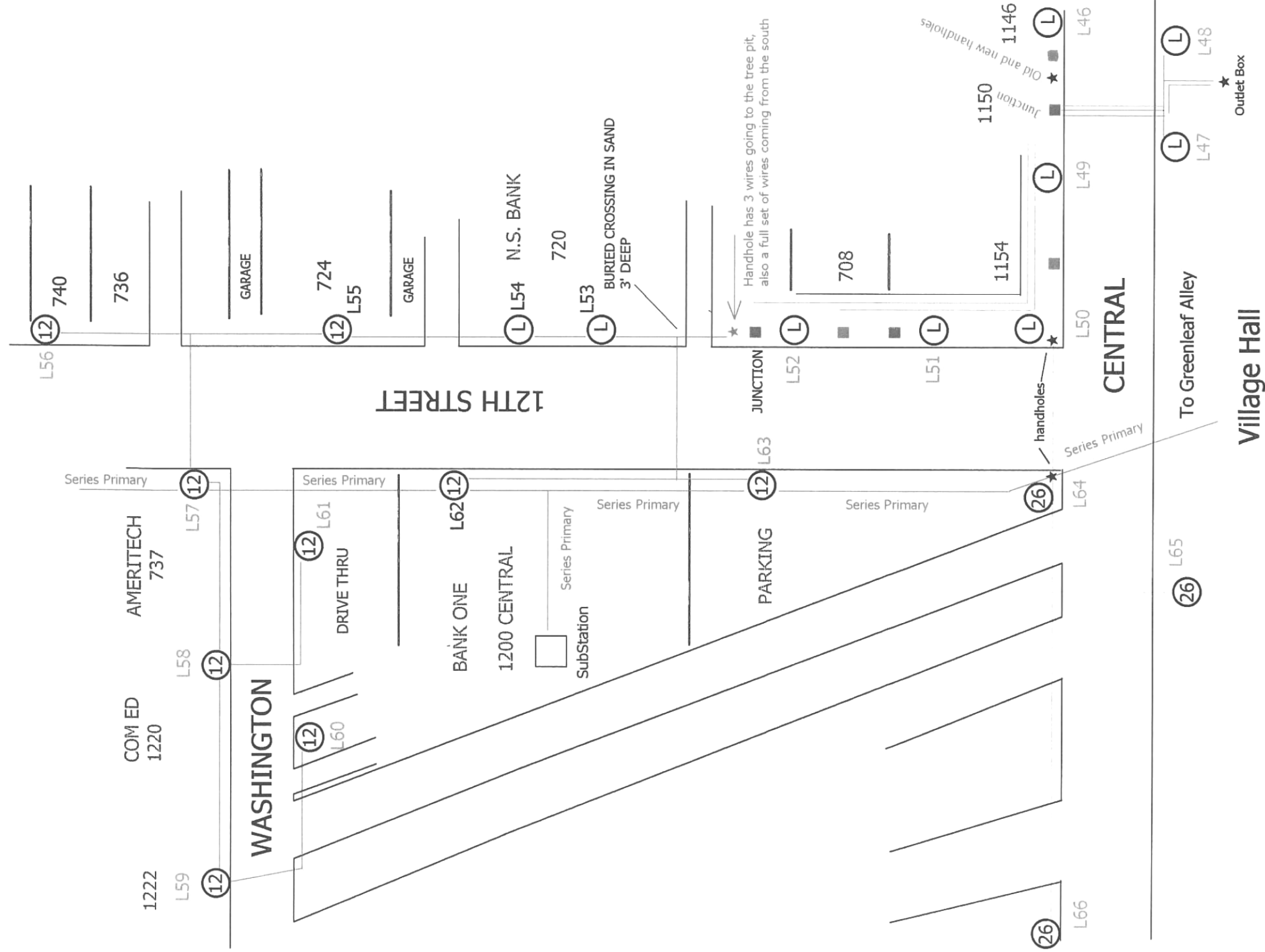
CENTRAL AVENUE AND WILMETTE AVENUE
 EXISTING LIGHTING AND ELECTRICAL SYSTEMS

SCALE: NONE SHEET 9 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	229
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

FOR REFERENCE ONLY

Central Business District



PLAN	SURVEYED	BY	DATE
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	ALIGNMENT		
	FILE NAME		

PROFILE	SURVEYED	BY	DATE
NO.	GRADES CHECKED		
	STRUCTURE		
	NOTATRS CHKD		



1475 EAST WOODFIELD ROAD, SUITE 600
SCHAUMBURG, ILLINOIS 60173
(847) 605-9600

FILE NAME = 180080-ELEC	USER NAME = Mibeening	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 20.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CENTRAL AVENUE AND WILMETTE AVENUE
EXISTING LIGHTING AND ELECTRICAL SYSTEMS

SCALE: NONE SHEET 10 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	230
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

FOR REFERENCE ONLY

Central Business District

LAKE STREET

See Page 20 Multi



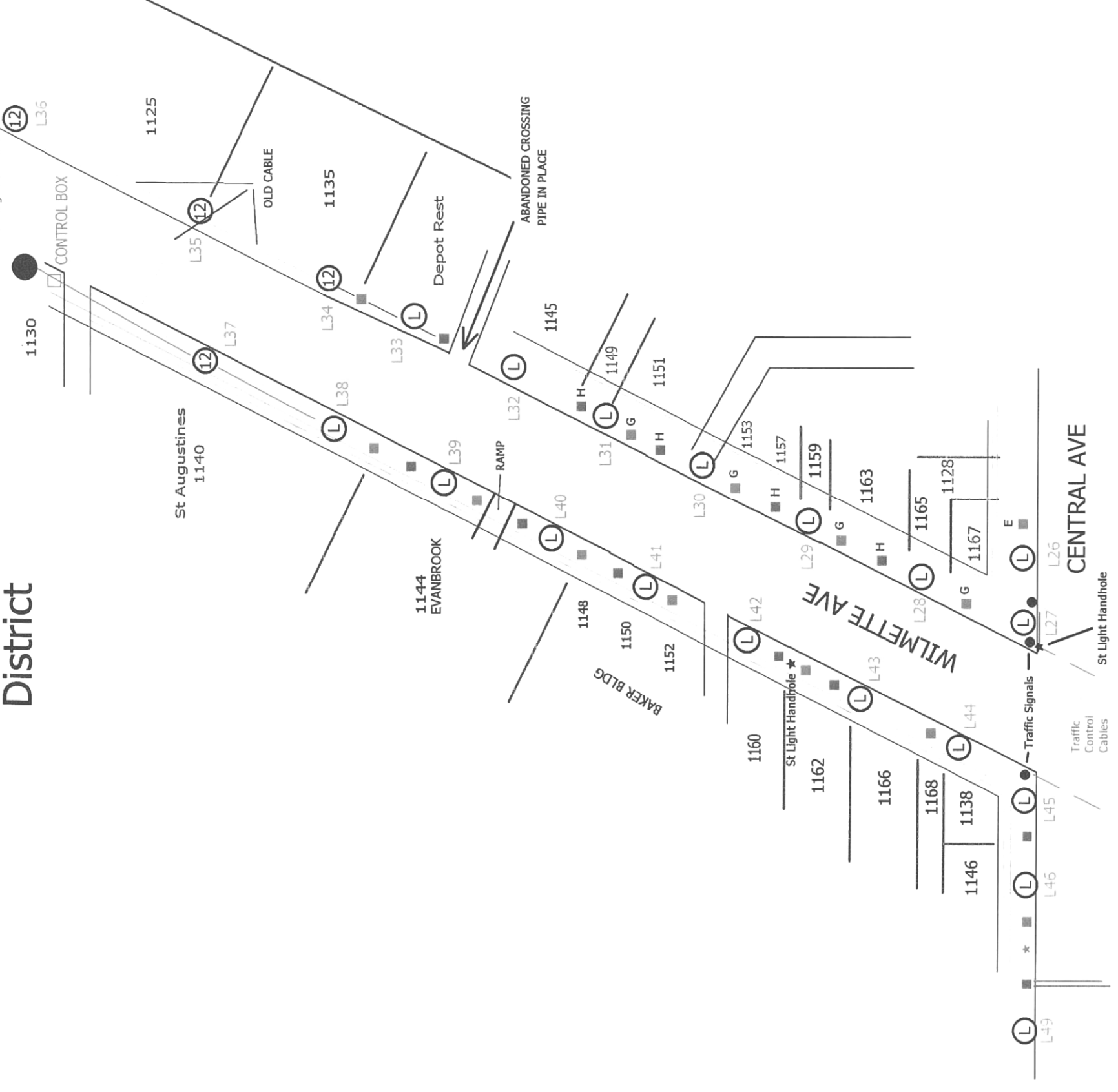
St Augustines 1140

1144 EVANBROOK

BAKER BLDG

WILMETTE AVE

CENTRAL AVE



PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	CHECKED		
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PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO.	GRADES CHECKED		
	STRUCTURE		
	NOTATIONS		



1475 EAST WOODFIELD ROAD, SUITE 600
SCHAUMBURG, ILLINOIS 60173
(847) 605-9600

FILE NAME = 180080-ELEC
Default

USER NAME = Mibeening
PLOT SCALE = 20.0000' / in.
PLOT DATE = 2/14/2020

DESIGNED - MLB
DRAWN - MLB
CHECKED - DWB
DATE - 02/13/2020

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CENTRAL AVENUE AND WILMETTE AVENUE
EXISTING LIGHTING AND ELECTRICAL SYSTEMS

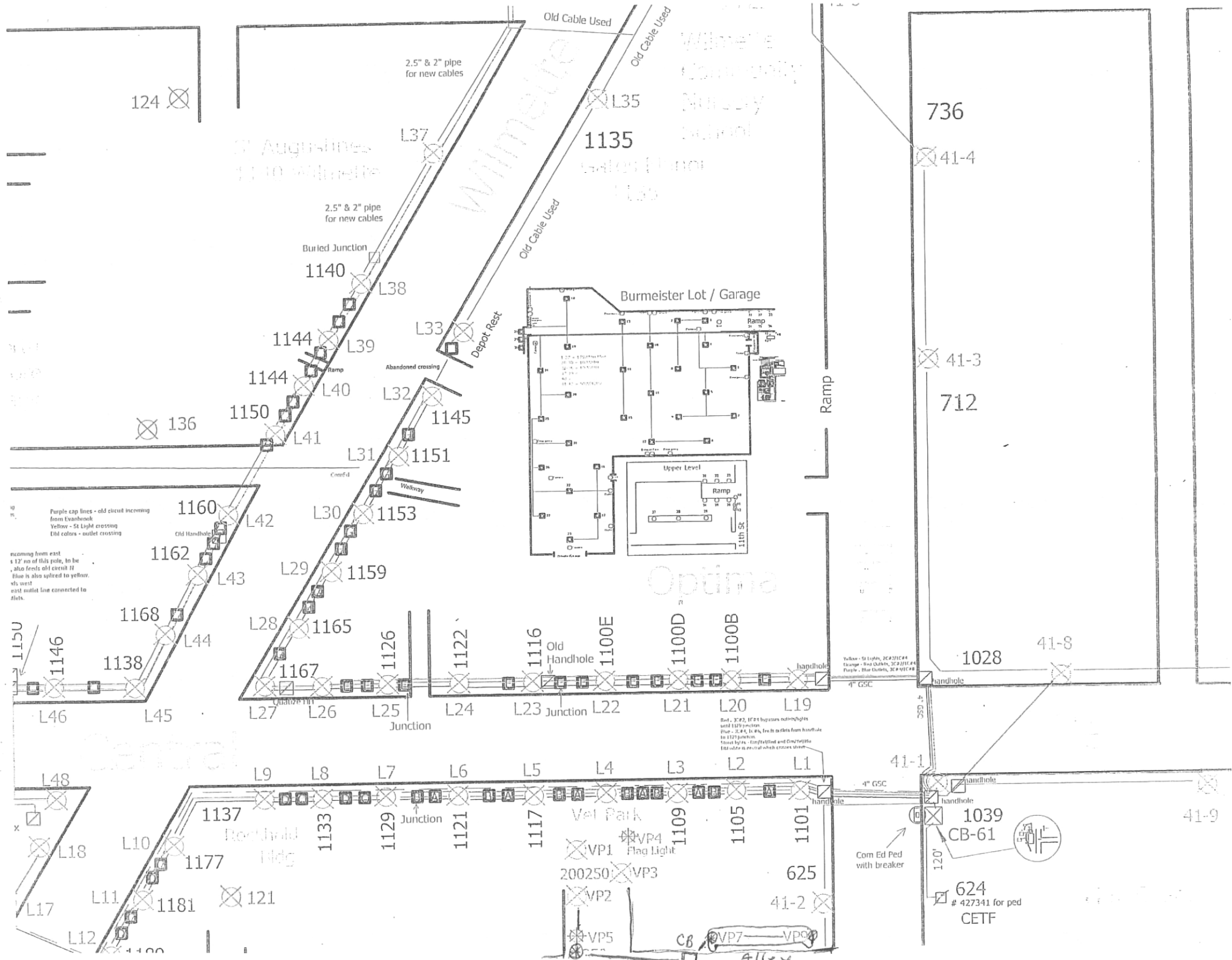
SCALE: NONE SHEET 11 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	231
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				

FOR REFERENCE ONLY

PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	CHECKED	
	ALIGNED	
	CAD FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK	GRADES CHECKED	BY
NO.	STRUCTURE	
	NOTATIONS CHECKED	



TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBURG, ILLINOIS 60173
 (847) 605-9600

FILE NAME = 180080-ELEC	USER NAME = Mibeening	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 20.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

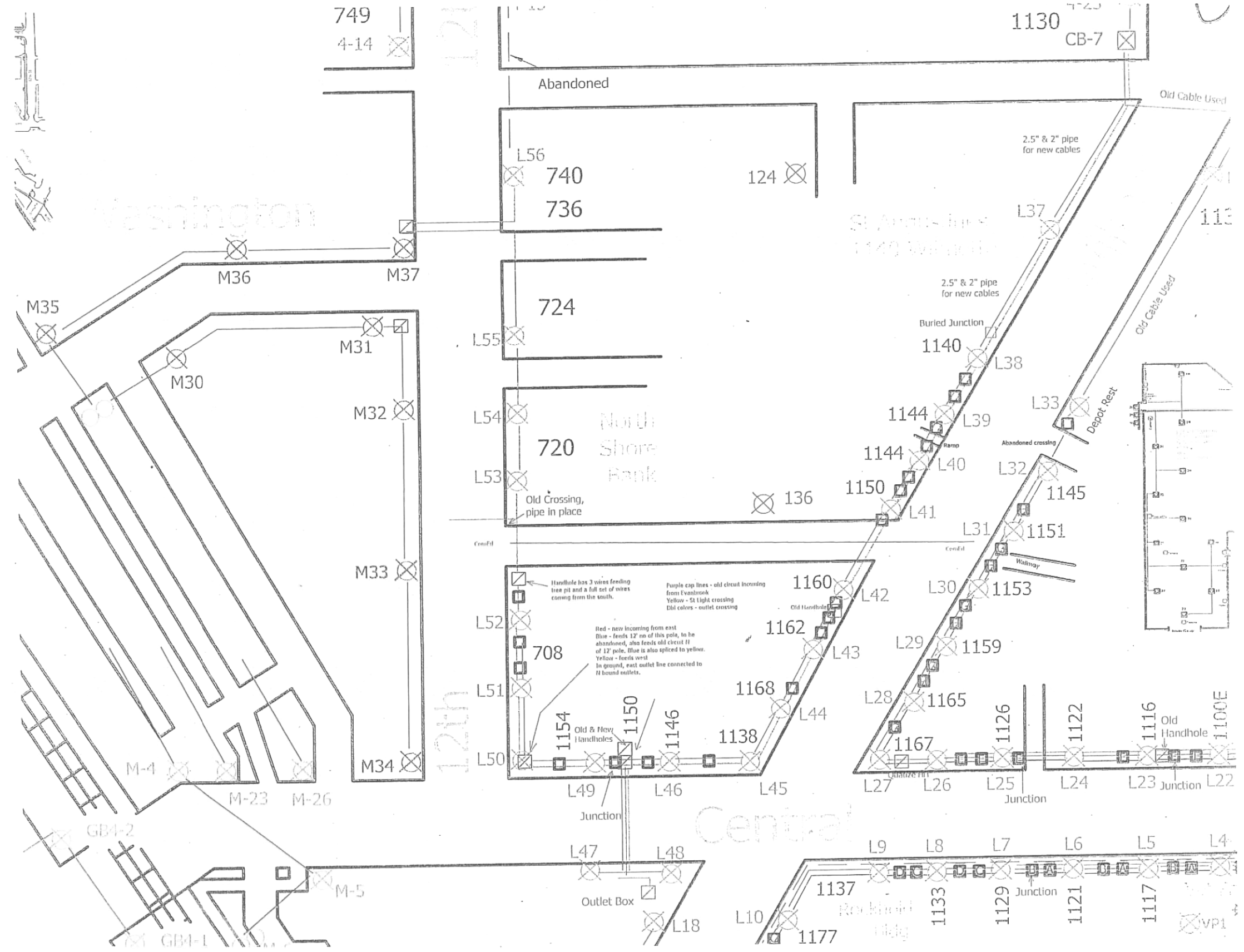
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CENTRAL AVENUE AND WILMETTE AVENUE
 EXISTING LIGHTING AND ELECTRICAL SYSTEMS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	232
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET 12 OF 15 SHEETS STA. TO STA.

FOR REFERENCE ONLY



PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	
NO.	CHECKED	
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	CAD FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK	GRADES CHECKED	
NO.	STRUCTURE NOTATIONS CHECKED	

TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBURG, ILLINOIS 60173
 (847) 605-9600

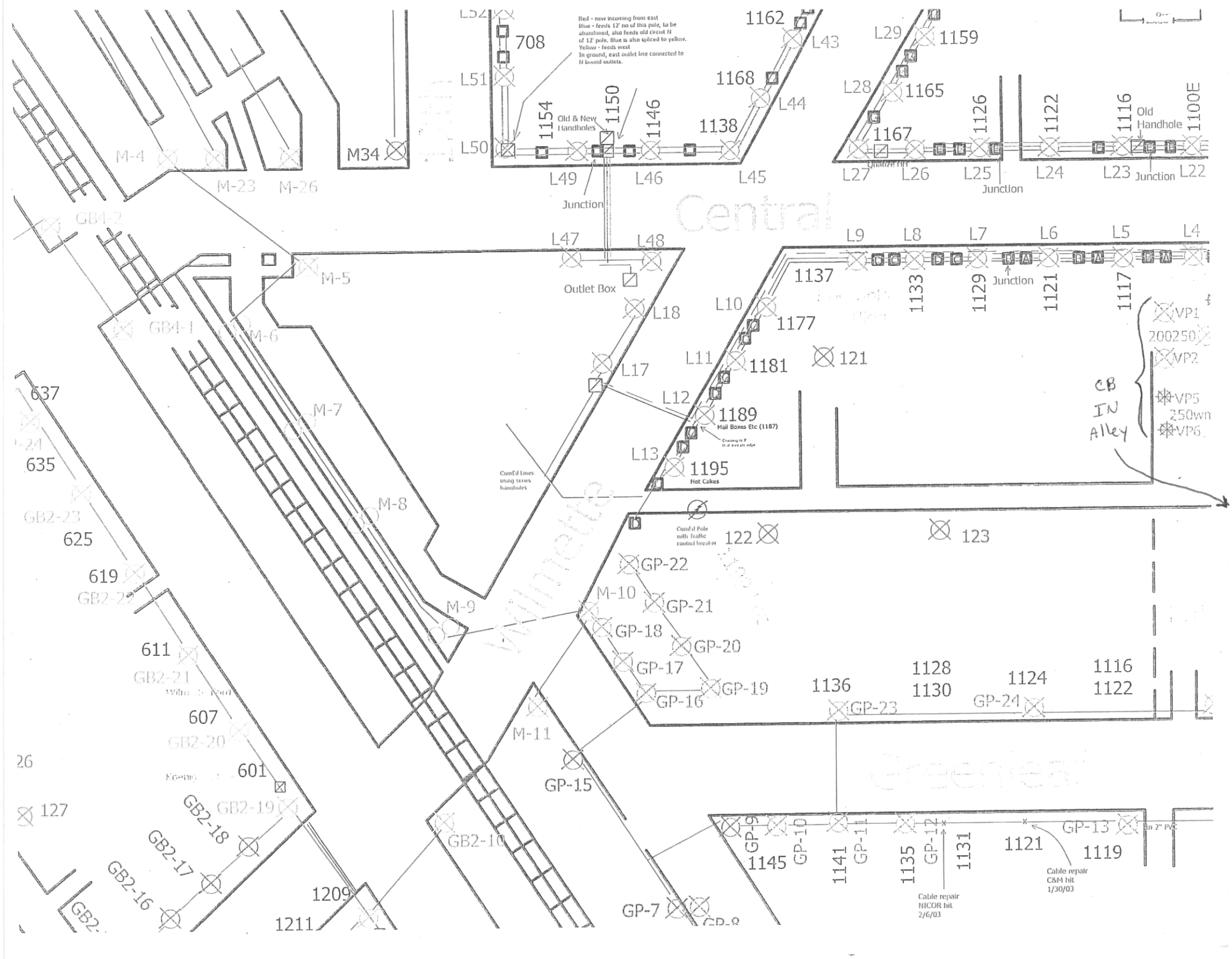
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Default	PLOT SCALE = 20.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CENTRAL AVENUE AND WILMETTE AVENUE EXISTING LIGHTING AND ELECTRICAL SYSTEMS	
SCALE: NONE	SHEET 13 OF 15 SHEETS STA. TO STA.

F.A.U. RTE. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 233
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				

FOR REFERENCE ONLY



PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
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	ALIGNED		
	CAD FILE NAME		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO.	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		

TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBERG, ILLINOIS 60173
 (847) 605-9600

FILE NAME = 180080-ELEC	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 20.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

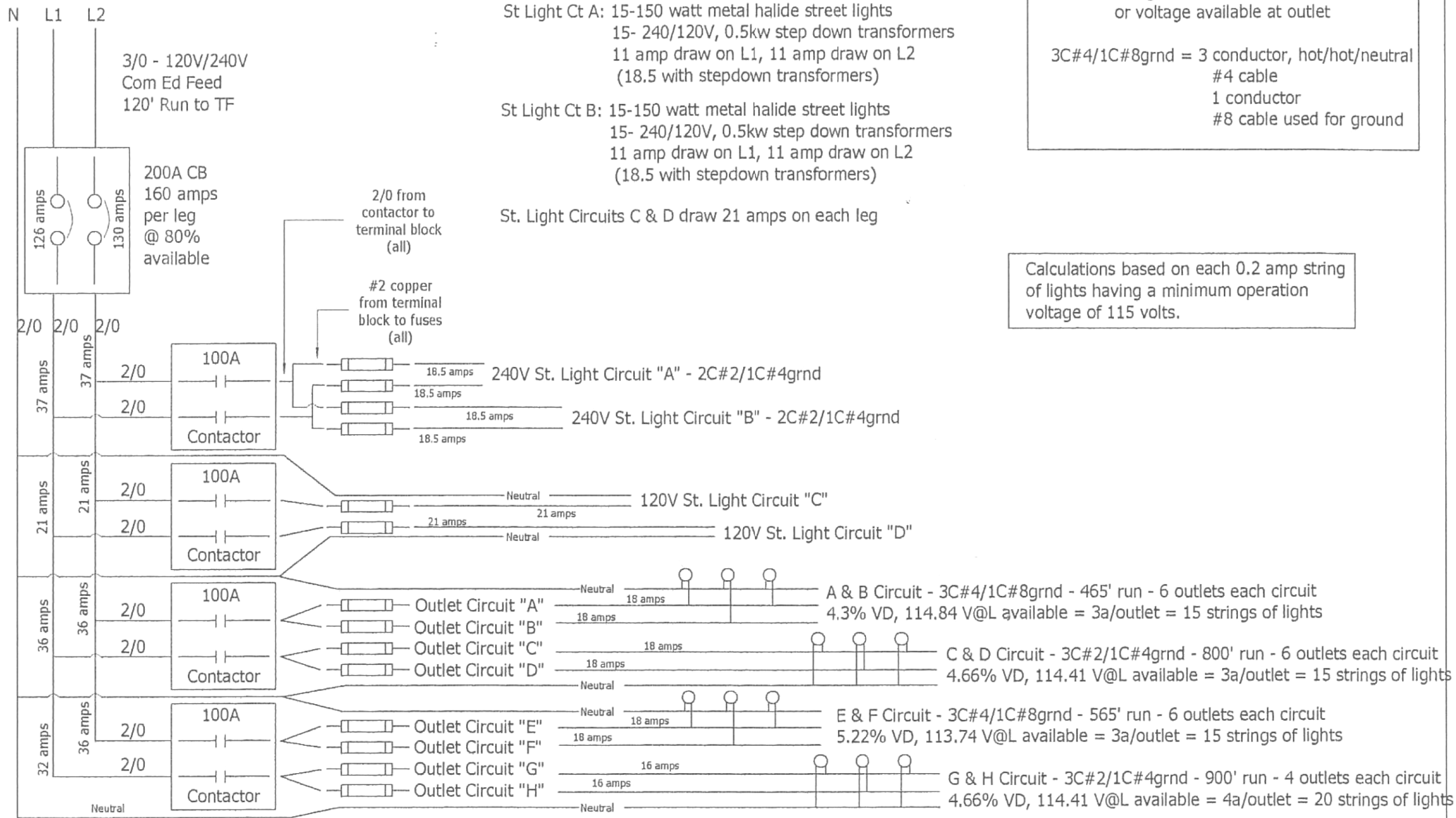
CENTRAL AVENUE AND WILMETTE AVENUE
 EXISTING LIGHTING AND ELECTRICAL SYSTEMS

SCALE: NONE SHEET 14 OF 15 SHEETS STA. TO STA.

F.A.U. RTE. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 234
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

FOR REFERENCE ONLY

Central Business District - 11th and Central Controller - 02/2004



VD = Voltage Drop
V@L = Voltage at Load
or voltage available at outlet

3C#4/1C#8grnd = 3 conductor, hot/hot/neutral
#4 cable
1 conductor
#8 cable used for ground

Calculations based on each 0.2 amp string
of lights having a minimum operation
voltage of 115 volts.

PLAN	SURVEYED	BY	DATE
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NOTE BOOK	NO.		
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	DATE		
	FILE NAME		

TranSystems
1475 EAST WOODFIELD ROAD, SUITE 600
SCHAMBURG, ILLINOIS 60173
(847) 605-9600

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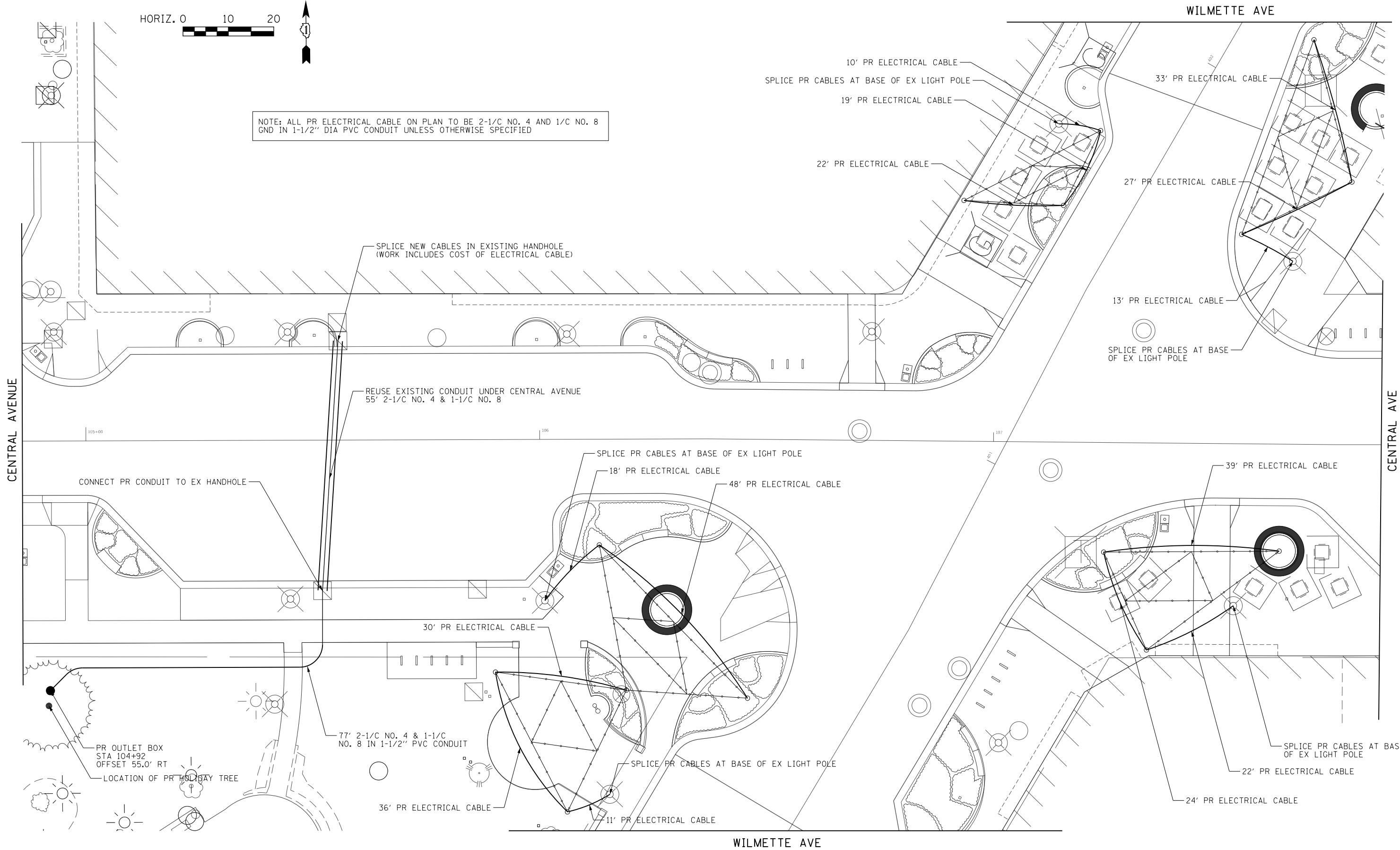


NOTE: ALL PR ELECTRICAL CABLE ON PLAN TO BE 2-1/C NO. 4 AND 1/C NO. 8 GND IN 1-1/2" DIA PVC CONDUIT UNLESS OTHERWISE SPECIFIED

PLAN	SURVEYED	DATE
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TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBURG, ILLINOIS 60173
 (847) 605-9600



FILE NAME = 180080-STSC
 Default

USER NAME = Mibeering
 PLOT SCALE = 20.0000' / in.
 PLOT DATE = 2/14/2020

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 DATE - 02/13/2020

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE AND WILMETTE AVENUE
 STREETScape LIGHTING DETAIL**

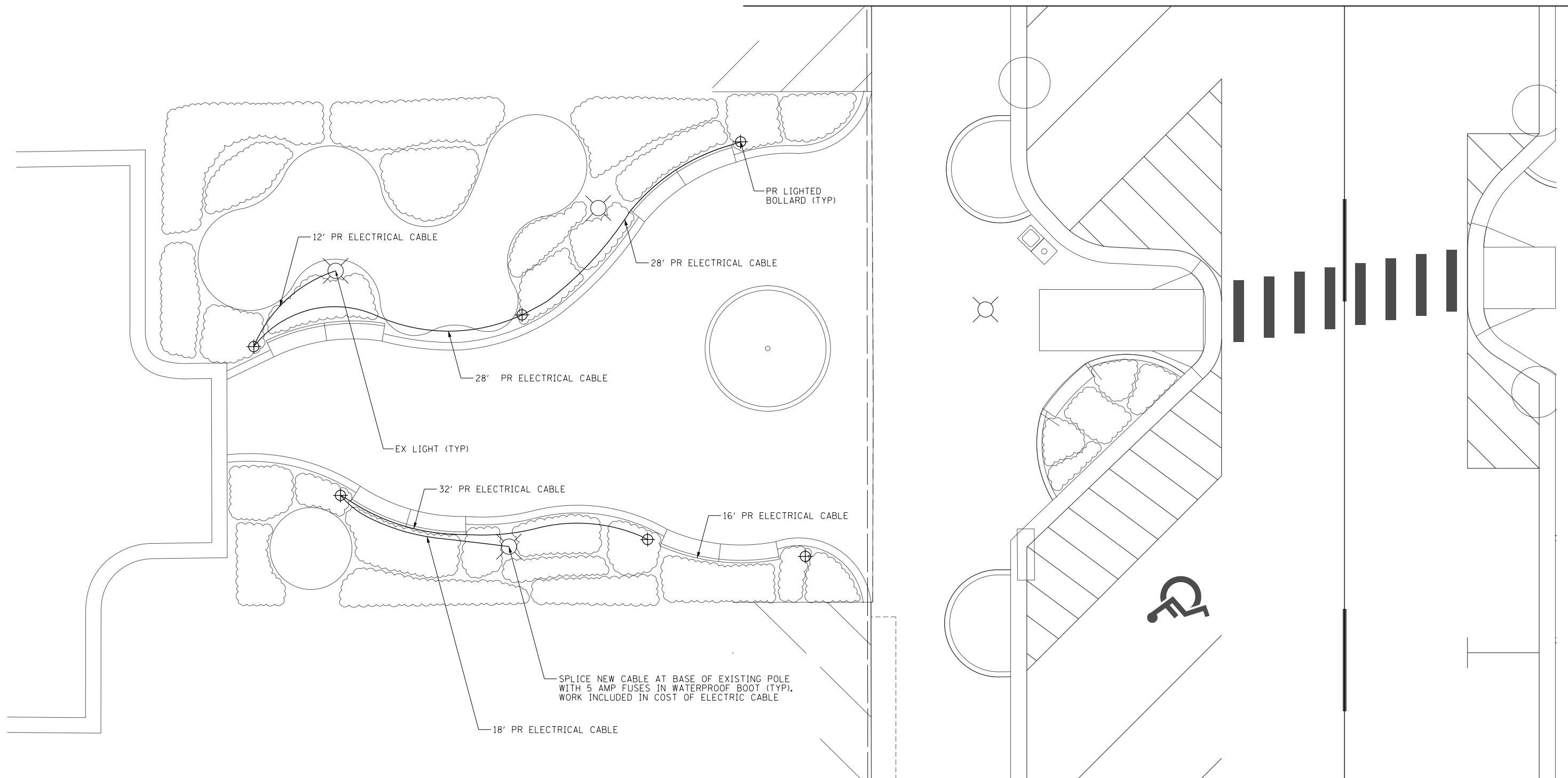
SCALE: 1"=10' SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	236
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

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CENTRAL AVE



NOTE: ALL PR ELECTRICAL CABLE ON SHEET TO BE 2-1/2" NO. 10 & 1/2" NO. 10 GROUND IN 1" CONDUIT

CENTRAL AVE

PLAN	SURVEYED	BY	DATE
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	ALIGNMENT		
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	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	GRADES CHECKED		
	STRUCTURE		
	NOT AT THIS OFFICE		
	NO. 1		

TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBURG, ILLINOIS 60173
 (847) 605-9600

FILE NAME = 180080-STSC	USER NAME = Mibeening	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 10.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE AND WILMETTE AVENUE
 STREETSCAPE LIGHTING DETAIL**

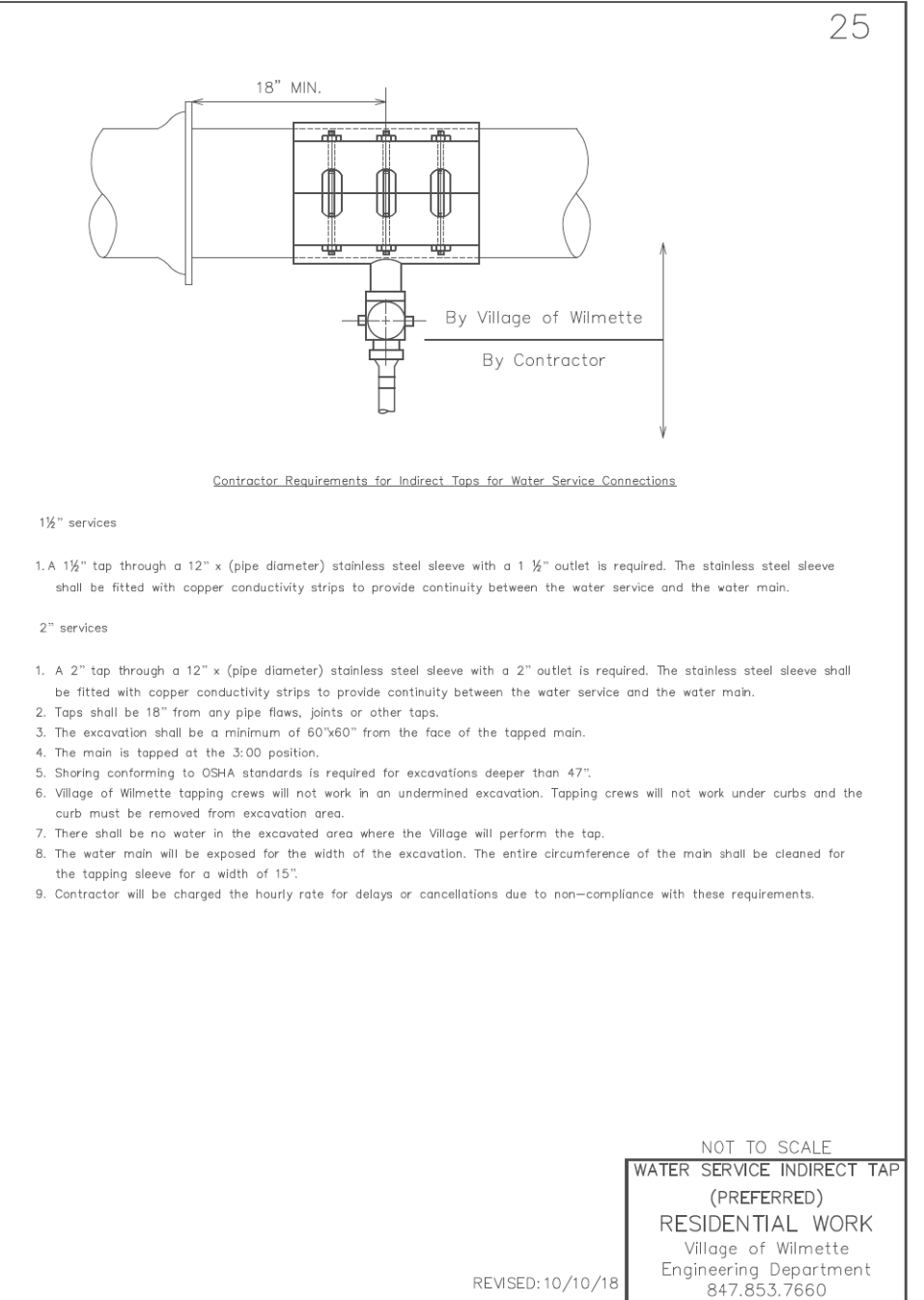
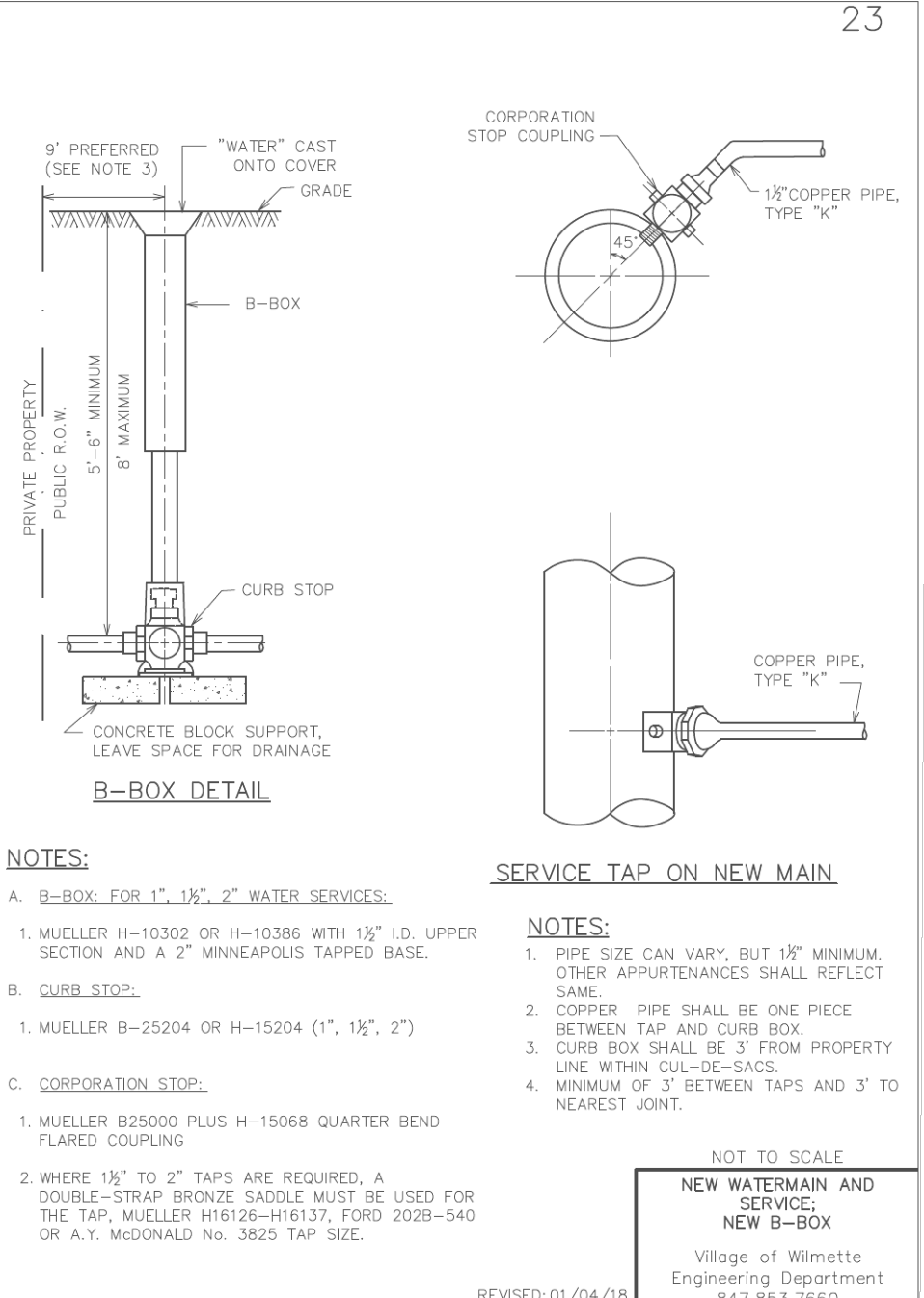
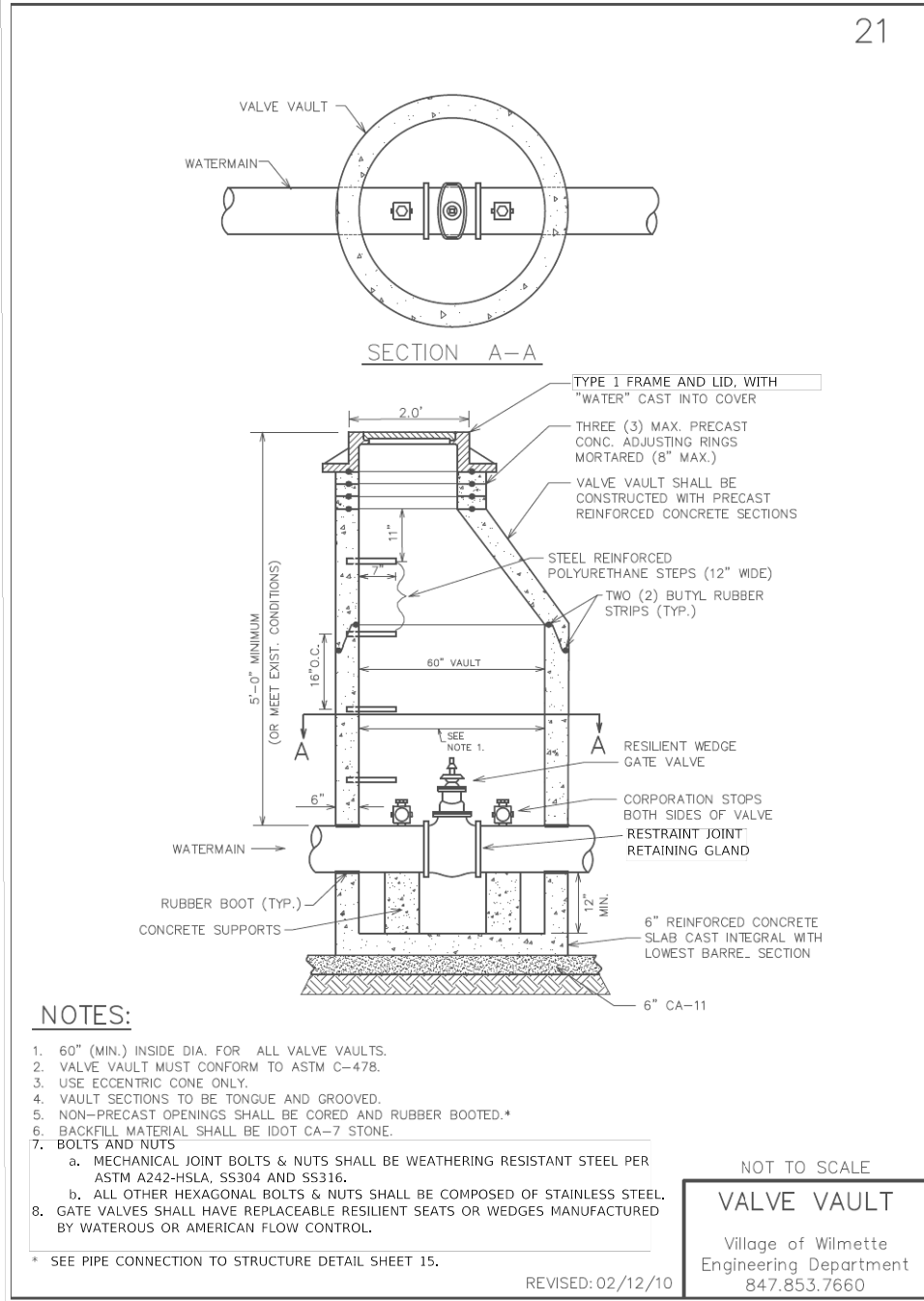
SCALE: 1"=5' SHEET 2 OF 2 TO STA.

F.A.U. RTE. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 237
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				

DATE	
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TranSystems
1475 EAST WOODFIELD ROAD, SUITE 600
SCHAUMBURG, ILLINOIS 60173
(847) 605-9600



FILE NAME = 180080-WILMETTE DETAIL	USER NAME = Mibeening	DESIGNED - MLB	REVISED -
Default		DRAWN - MLB	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED - DWB	REVISED -
	PLOT DATE = 2/14/2020	DATE - 02/13/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE AND WILMETTE AVENUE
VILLAGE OF WILMETTE DETAILS**

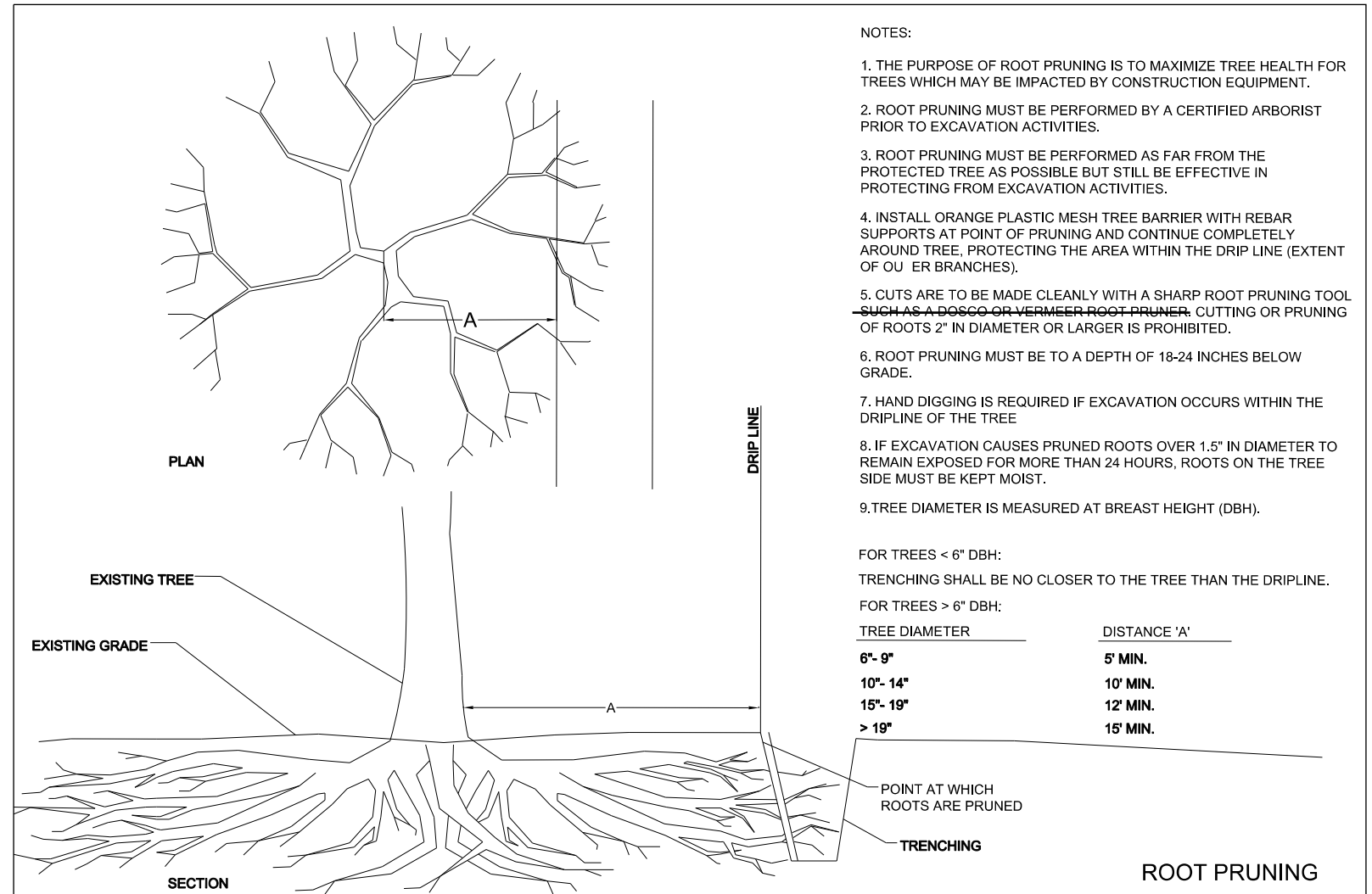
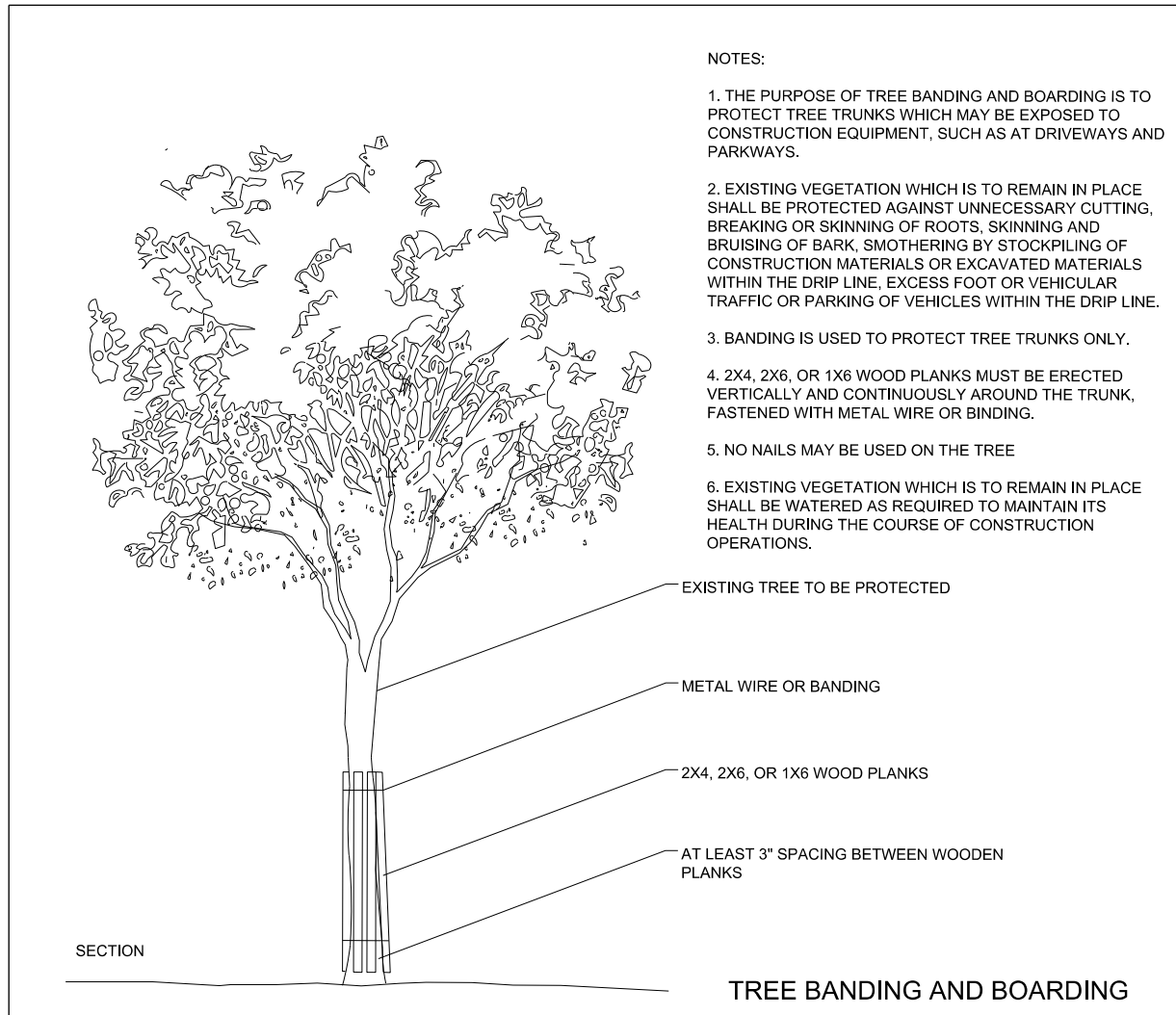
SCALE: NONE SHEET 1 OF 7 SHEETS STA. TO STA.

F.A.U. RTE. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 238
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
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	ALIGNED	
	FILED	
NOTE BOOK NO.	CADD FILE NAME	

PROFILE	SURVEYED	DATE
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	GRADES	
	STRUCTURE	
NOTE BOOK NO.	NOTATIS CHFD	

TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBURG, ILLINOIS 60173
 (847) 605-9600



FILE NAME = 180080-WILMETTE DETAIL	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 1/8" = 1' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE AND WILMETTE AVENUE
 VILLAGE OF WILMETTE DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

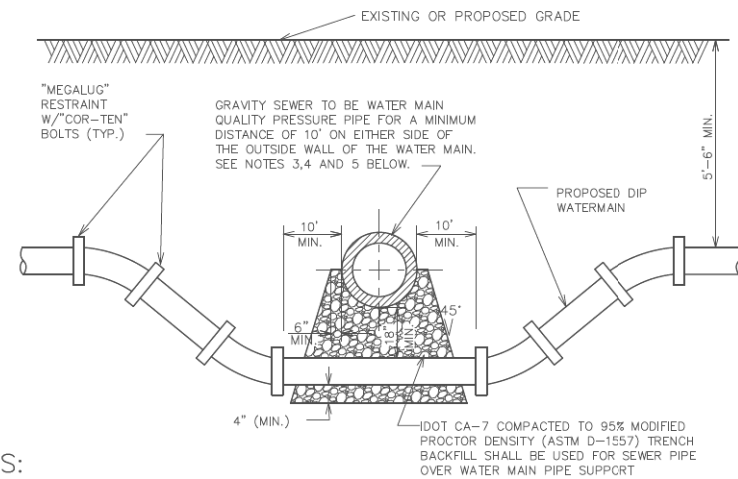
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	239
CONTRACT NO. 61G42				
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
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NOTE BOOK NO.		

PROFILE	SURVEYED	DATE
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	STRUCTURE	
	NOT THIS OFFICE	
NOTE BOOK NO.		

TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAUMBURG, ILLINOIS 60173
 (847) 605-9600

26



NOTES:

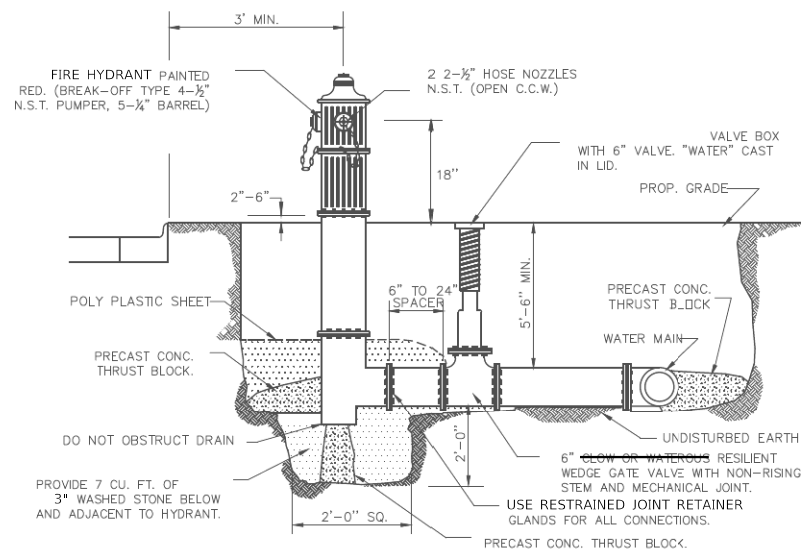
- HORIZONTAL AND VERTICAL SEPARATION BETWEEN WATERMANS AND SEWERS SHALL COMPLY WITH VILLAGE ENGINEERING STANDARDS MANUAL OR IEPA REQUIREMENTS, WHICHEVER IS MORE STRINGENT.
- CONTRACTOR MAY BEND WATER MAIN PIPE UNIFORMLY UNDER SEWERS WITHOUT USING FITTINGS, PROVIDED THAT JOINT DEFLECTION DOES NOT EXCEED 5 DEGREES PER JOINT FOR PIPE UNDER 14" IN DIAMETER AND 3 DEGREES PER JOINT FOR PIPE 14" AND OVER IN DIAMETER. IF FITTINGS ARE USED, CONTINUOUS STRAPPING WITH RODS, STRAPS, NUTS AND BOLTS BELOW NORMAL WATERMAIN DEPTH ARE REQUIRED, OR RETAINER GLANDS MAY BE USED IN LIEU OF STRAPPING. RETAINER GLANDS TO BE RESTRAINED JOINT RETAINER GLAND WITH WEATHER RESISTANT BOLTS MEETING THE REQUIREMENT OF ASTM A242-HSLA, SS304 AND SS316.
- ALL SANITARY SEWER (INCLUDING SERVICE) CROSSINGS WHERE THE WATER MAINS OR WATER SERVICES ARE LESS THAN 18" VERTICALLY ABOVE THE SEWER SHALL BE POLYVINYL CHLORIDE PRESSURE PIPE (SDR 26-160 PSI) AND SHALL CONFORM WITH THE LATEST REVISION OF ASTM D-2241. JOINTS SHALL CONFORM TO ASTM D-3139 AND ELASTOMERIC GASKETS SHALL CONFORM TO ASTM F-477. THE SAME PIPE AND JOINT MATERIALS SHALL BE USED WHENEVER WATER MAIN CROSSES BELOW THE SEWER.
- ALL STORM SEWER (INCLUDING SERVICE) CROSSINGS WHERE THE WATER MAINS ARE LESS THAN 18" VERTICALLY ABOVE THE SEWER SHALL BE REINFORCED CONCRETE PIPE, ASTM C-361, CLASS D-25, WITH BELL AND SPIGOT JOINTS AND RUBBER GASKETS, OR PVC SDR 26 AS SPECIFIED IN NOTE 3 ABOVE. THE SAME PIPE AND JOINT MATERIAL SHALL BE USED WHENEVER WATER MAIN CROSSES BELOW THE SEWER.
- FOR NEW SEWER INSTALLATIONS CROSSING OVER WATER MAINS, THE ENTIRE RUN OF NEW SEWER SHALL BE WATER MAIN QUALITY PIPE, EXTENDING FROM STRUCTURE TO STRUCTURE ON EACH SIDE OF THE CROSSING.

NOT TO SCALE

WATER MAIN CROSSING
 Village of Wilmette
 Engineering Department
 847.853.7660

REVISED: 02/28/07

27



NOTES:

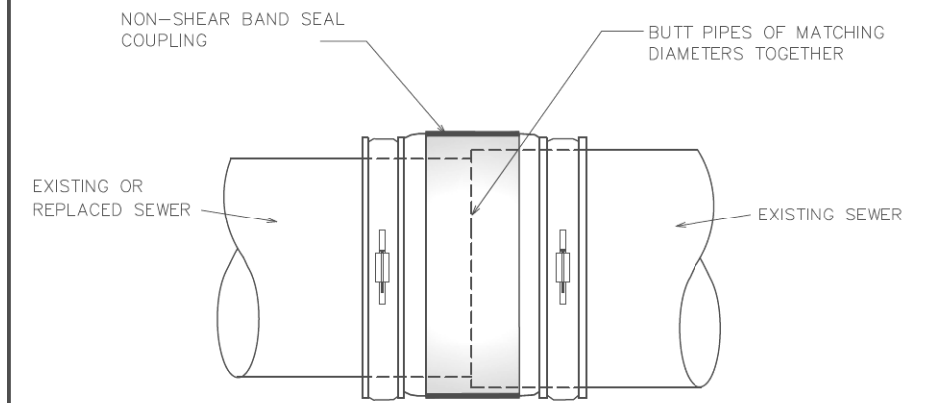
- ALL MAINS TO HAVE A MINIMUM OF 5'-6" OF COVER
- ALL HYDRANTS TO FACE STREET AND TO BE LOCATED A MINIMUM OF 3'-0" FROM THE BACK OF CURB TO THE C OF HYDRANT.

NOT TO SCALE

HYDRANT DETAIL
 Village of Wilmette
 Engineering Department
 847.853.7660

REVISED: 01/09/08

28



NOT TO SCALE

PIPE COUPLING
 Village of Wilmette
 Engineering Department
 847.853.7660

REVISED: 01/09/08

FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
180080-WILMETTE DETAIL		DRAWN - MLB	REVISED -
Default		CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CENTRAL AVENUE
 UNION PACIFIC RAILROAD TO SHERIDAN ROAD
 VILLAGE OF WILMETTE DETAILS

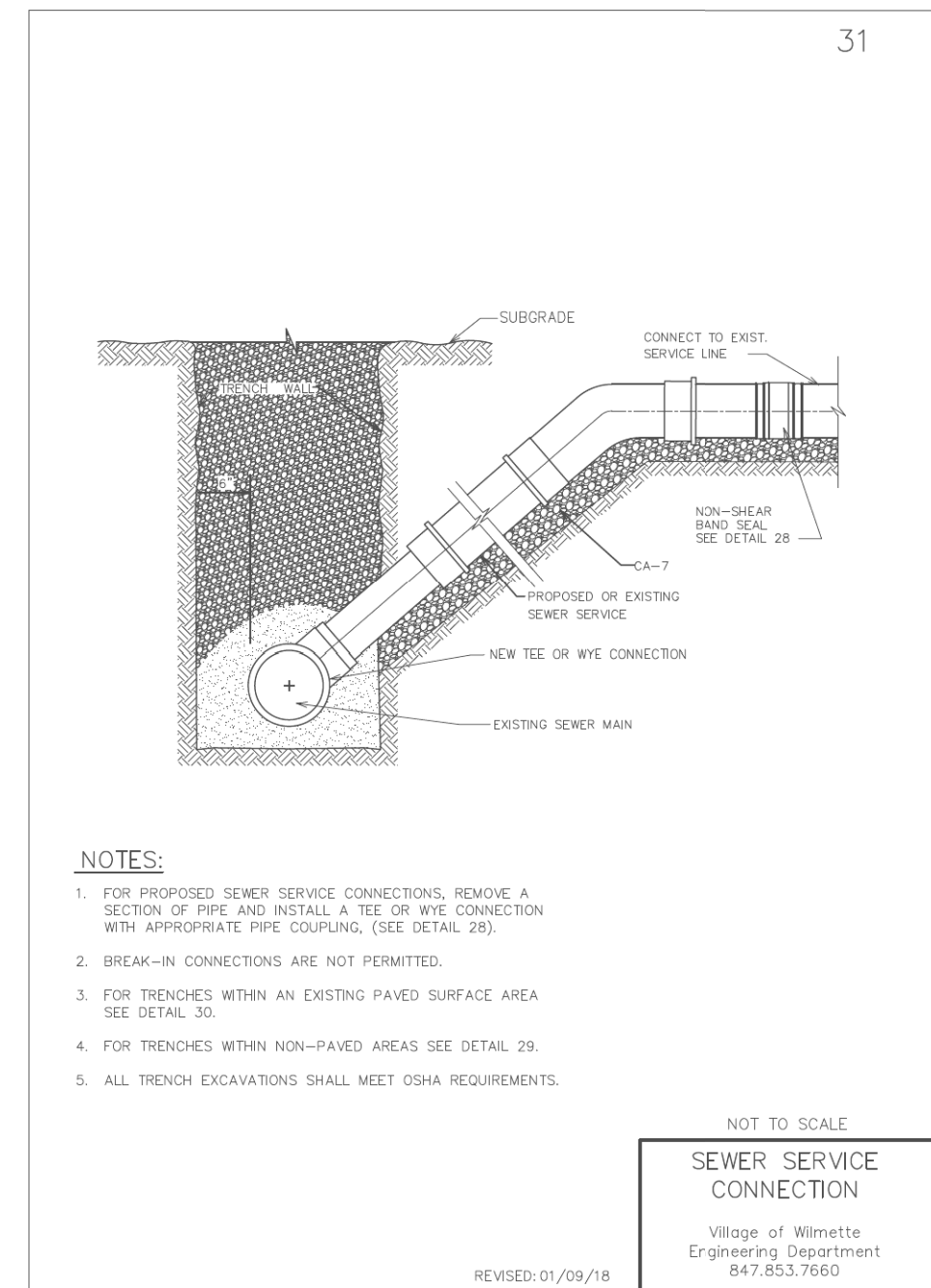
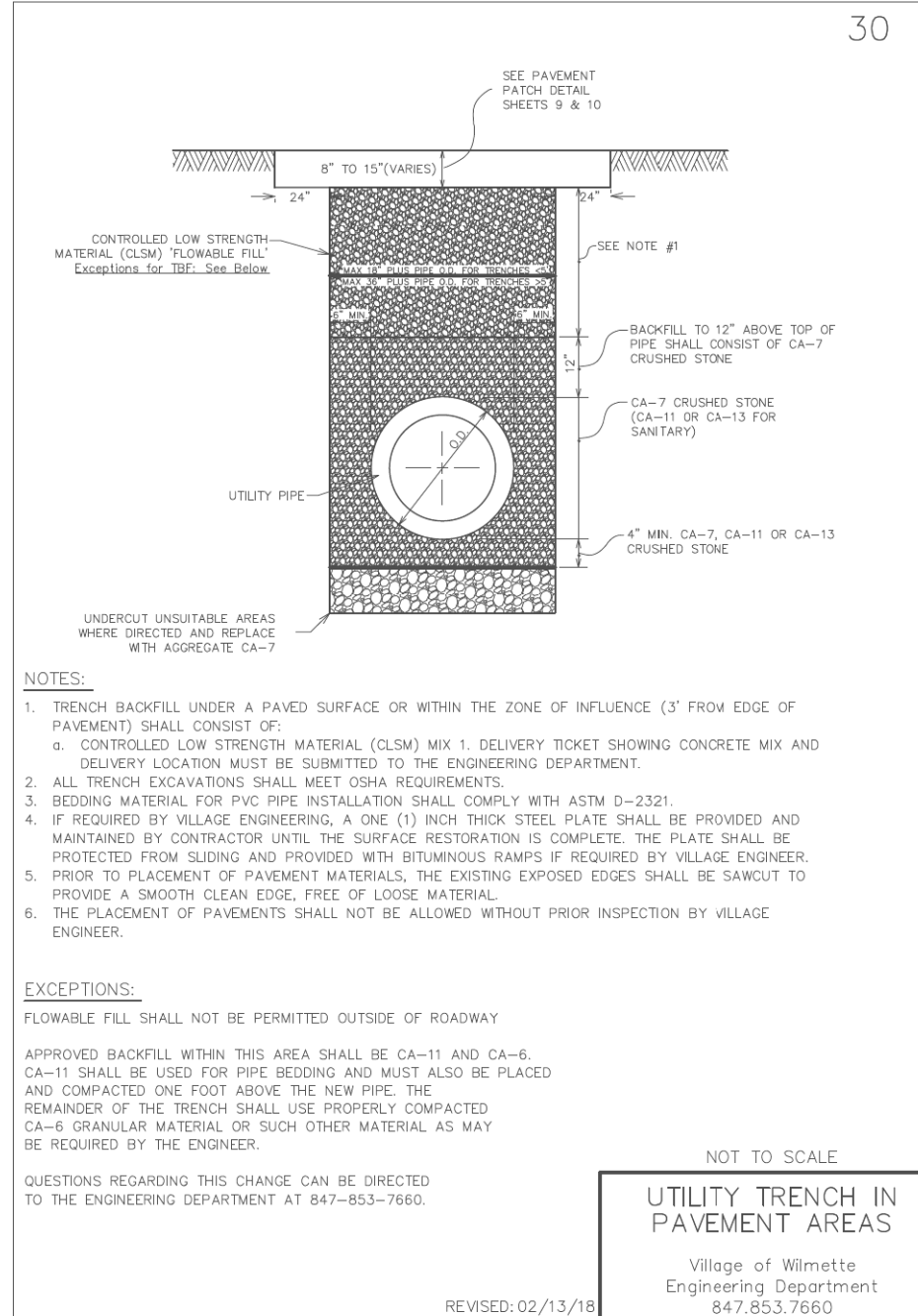
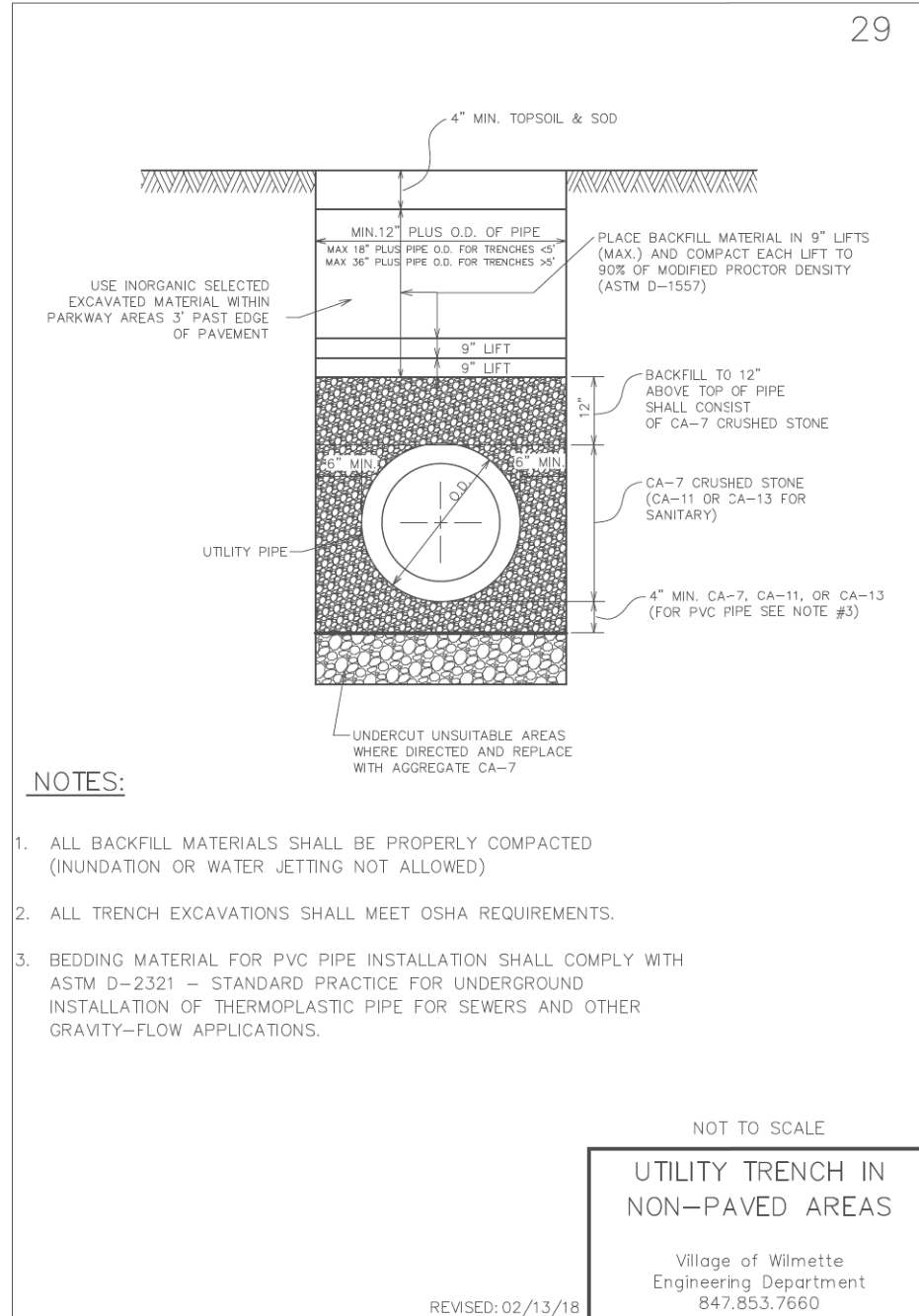
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	241
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO.	

TranSystems
1475 EAST WOODFIELD ROAD, SUITE 600
SCHAUMBURG, ILLINOIS 60173
(847) 605-9600



FILE NAME =	USER NAME = Mibeering	DESIGNED -	MLB	REVISED -	
180080-WILMETTE DETAIL		DRAWN -	MLB	REVISED -	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	DWB	REVISED -	
	PLOT DATE = 2/14/2020	DATE -	02/13/2020	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE
UNION PACIFIC RAILROAD TO SHERIDAN ROAD
VILLAGE OF WILMETTE DETAILS**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	242
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET 5 OF 7 SHEETS STA. TO STA.

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	ALIGNED		
	CAD FILE NAME		
NOTE BOOK NO.			

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	GRADES		
	STRUCTURE		
NOTE BOOK NO.			
	NOT AT THIS OFFICE		

TranSystems
1475 EAST WOODFIELD ROAD, SUITE 600
SCHAMBURG, ILLINOIS 60173
(847) 605-9600

32

EXISTING BACK OF CURB
EXISTING PAVEMENT
SURFACE GRADE (YP.)
10' MIN.
10' MIN.
8" MIN.
MIN 3/8" THICK BITUMINOUS COATED STEEL CASING PIPE, AUGERED & JACKED
SANITARY SEWER, STORM SEWER OR WATER MAIN (CARRIER PIPE)
INSTALL ALL STAINLESS STEEL CASING SPACERS FOR EACH PIPE LENGTH ON 6' CENTERS, OR AS RECOMMENDED BY THE MANUFACTURER
BRICK AND MORTAR BULKHEADS (BOTH ENDS). APPROVAL IS NEEDED FROM VILLAGE ENGINEER, PRIOR TO BACKFILLING.

WATERMAIN SIZE	CASING SIZE
16"	24"
14"	24"
12"	20"
10"	20"
8"	16"
6"	12"
4"	10"

NOTES:

- CASING PIPE IS REQUIRED UNDER ALL EXISTING ROADWAYS, OR AS OTHERWISE DIRECTED BY VILLAGE ENGINEER WHERE OPEN CUTS ARE NOT PERMITTED, EXCEPT FOR WATER SERVICE LINES UP TO 2" IN DIAMETER.
- WATER MAIN CASING SPACERS SHALL BE RESTRAINED IN POSITION.
- THE INSIDE DIAMETER OF THE CASING PIPE SHALL BE DETERMINED BY CONTRACTOR BUT IN NO CASE SHALL IT BE LESS THAN 8" LARGER THAN THE DIAMETER OF THE CARRIER PIPE TO ALLOW AMPLE SPACE FOR BELLS, AND CARRIER PIPE SLOPE (FOR GRAVITY PIPE).
- ALL AUGER PITS TO BE BACKFILLED WITH IDOT CA-7 (CRUSHED) AGGREGATE MATERIAL.

NOT TO SCALE
CASING PIPE
Village of Wilmette
Engineering Department
847.853.7660
REVISED: 01/09/08

36

TREE
DRIP LINE
SNOW FENCE ALONG PERIMETER OF DRIP LINE.

NOTES:

- EXISTING VEGETATION WHICH IS TO REMAIN IN PLACE SHALL BE PROTECTED AGAINST UNNECESSARY CUTTING, BREAKING OR SKINNING OF ROOTS, SKINNING AND BRUISING OF BARK, SMOTHERING BY STOCKPILING OF CONSTRUCTION MATERIALS OR EXCAVATED MATERIALS WITHIN THE DRIP LINE OF THE EXISTING VEGETATION CANOPY, EXCESS FOOT OR VEHICULAR TRAFFIC OR PARKING OF VEHICLES WITHIN THE DRIP LINE OF THE VEGETATION.
- ALL TREES TO REMAIN SHALL BE PROTECTED WITH A SNOW FENCE INSTALLED AROUND THE PERIMETER OF THE TREES DRIP LINE, WITH STURDY STEEL POLES SET 3' ON CENTER ALONG THE FENCE. ANY CONSTRUCTION INSIDE THE DRIP LINE OF THE TREE REQUIRES INSPECTION FOR POSSIBLE ROOT PRUNING PRIOR TO THE EXCAVATION. ROOT PRUNING MAY BE NECESSARY IN ORDER TO LESSEN THE IMPACT OF DAMAGE TO THE VEGETATION'S ROOT SYSTEM.
- EXISTING VEGETATION WHICH IS TO REMAIN IN PLACE SHALL BE WATERED A MINIMUM OF 1" OF WATER PER WEEK. WATERING SHALL COVER THE ENTIRE ROOT SYSTEM WITHIN THE TREE'S DRIPLINE EXTENDING OUT TO ITS DRIPLINE. WATERING IS REQUIRED AS TO HELP MAINTAIN ITS HEALTH DURING THE COURSE OF CONSTRUCTION OPERATIONS.
- PROTECTION SHALL BE PROVIDED FOR ROOTS OVER 1-1/2" WHICH ARE CUT DURING CONSTRUCTION OPERATIONS. WHENEVER SUCH A ROOT IS CUT, THE CUT FACES SHALL BE COATED WITH A PRUNE WOOD DRESSING OR OTHER ACCEPTABLE COATING SPECIALLY FORMULATED FOR HORTICULTURAL USE ON DAMAGED OR CUT PLANT TISSUES. ALL EXPOSED ROOTS SHALL BE TEMPORARILY COVERED WITH WET BURLAP TO PREVENT THE ROOTS FROM DRYING OUT. EXPOSED ROOTS SHALL BE PROVIDED WITH SOIL COVER AS SOON AS POSSIBLE.
- VEGETATION THAT BECOMES DAMAGED BY CONSTRUCTION OPERATIONS SHALL BE REPAIRED OR REPLACED IN A MANNER ACCEPTABLE TO THE VILLAGE.
- DAMAGED TREES SHALL BE REPAIRED BY AN ISA CERTIFIED ARBORIST. DAMAGED TREES WHICH CANNOT BE REPAIRED AND RESTORED TO FULL GROWTH STATUS, AS DETERMINED BY THE VILLAGE AND/OR THE CERTIFIED ARBORIST, SHALL BE REPLACED WITHIN A TIME PERIOD STATED BY THE VILLAGE.

NOT TO SCALE
TREE PROTECTION DETAIL
Village of Wilmette
Engineering Department
847.853.7660
REVISED: 01/25/10

13

TYPE 1 FRAME AND LID WITH "SANITARY" CAST INTO COVER
2.0'
THREE (3) MAX. PRECAST CONC. ADJUSTING RINGS MORTARED (8" MAX.)
TYPE A (CONE)
PROVIDE 3" VERTICAL LIP ON CONE SECTION
TYPE B (FLAT TOP)
STEEL REINFORCED POLYURETHANE STEPS (12" WIDE)
TWO (2) BUTYL RUBBER STRIPS (TYPICAL)
RUBBER BOOT ASTM - C923 (TYP.)
CONCRETE BENCH
CONNECT TO EXIST. SANITARY SEWER WITH NON-SHEAR U-23 COUPLING, SEE DETAIL U-23
6" CA-11
FLEXIBLE MANHOLE PIPE BOOT WITH 2 STAINLESS BANDS (EACH PIPE PENETRATION)
3.0' MIN.
3.0' MIN.
48" (MIN.) FOR 12"-18" DIA. PIPE
60" (MIN.) FOR 21"-36" DIA. PIPE
72" (MIN.) FOR 39"-60" DIA. PIPE
5"
6"
5"
6"
2" MIN. FROM BOTTOM OF LOWEST PIPE
CONCRETE BASE CAST INTEGRAL WITH LOWEST BARREL SECTION
6" CA-11

NOTES:

- MANHOLES MUST CONFORM TO ASTM C-478.
- MANHOLE SECTIONS TO BE TONGUE AND GROOVED.
- BENCHES MUST BE PROVIDED IN ALL SANITARY SEWER MANHOLES
- USE EXTERNAL LIFTING "HOLES" ONLY, BUT NOT FULL PENETRATION.
- ALL PIPE PENETRATIONS AND ALL NON-PRECAST OPENINGS SHALL BE CORED, RUBBER BOOTED AND INTERIOR MORTARED AROUND PIPE AND CONFORM TO ASTM C-923.*
- USE ECCENTRIC CONE ONLY.

* SEE PIPE CONNECTION TO STRUCTURE DETAIL SHEET 15.

Not To Scale
SANITARY MANHOLE
Village of Wilmette
Engineering Department
847.853.7660
REVISED: 01/09/08

FILE NAME = 180080-WILMETTE DETAIL	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
Default	PLOT SCALE = 100.0000' / in.	DRAWN - MLB	REVISED -
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -

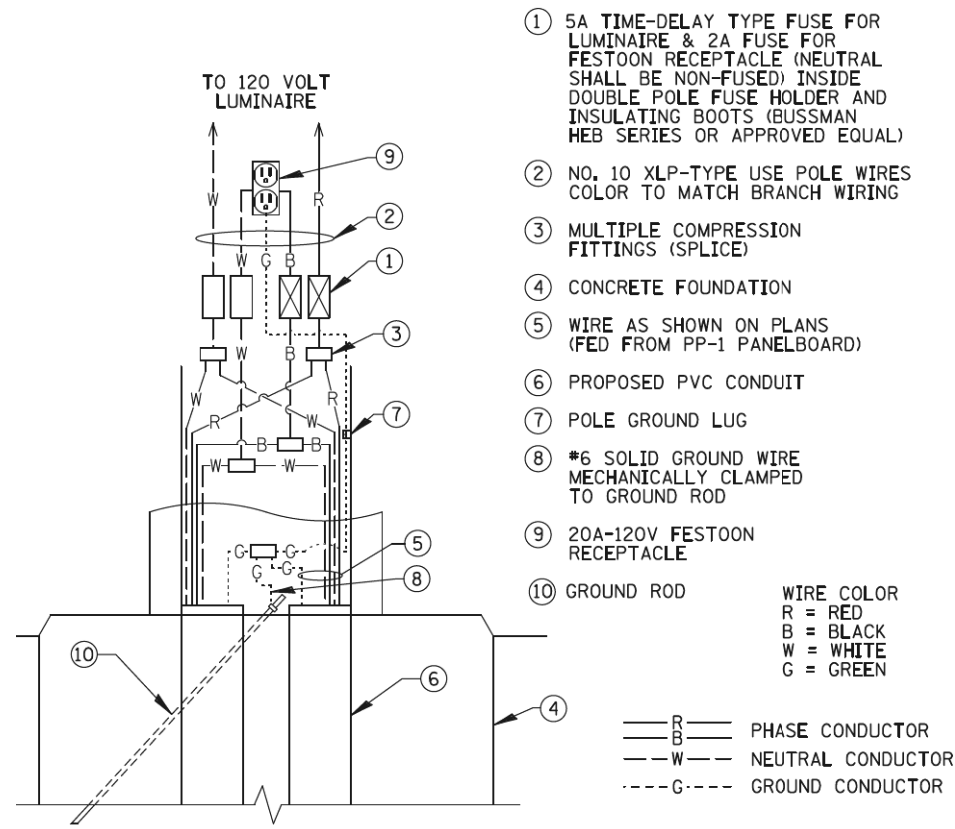
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CENTRAL AVENUE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
UNION PACIFIC RAILROAD TO SHERIDAN ROAD		1296	16-00199-00-RS	COOK	318	243
VILLAGE OF WILMETTE DETAILS		SCALE: NONE		SHEET 6 OF 7 SHEETS		STA. TO STA.
ILLINOIS FED. AID PROJECT						

CONTRACT NO. 61G42	
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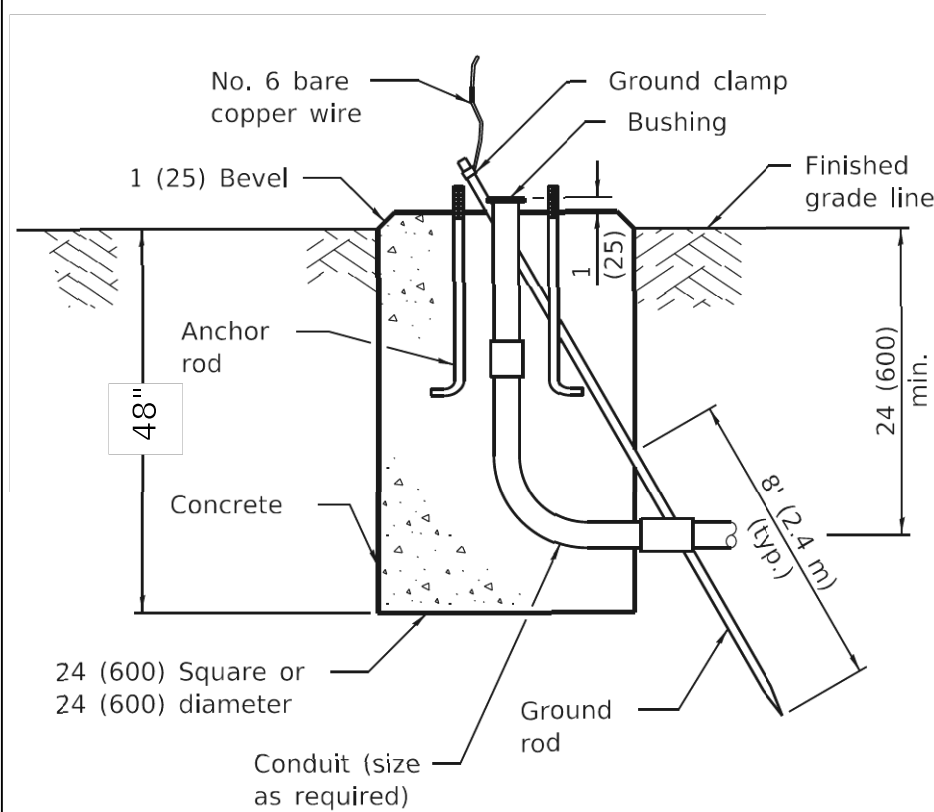
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	PLOTTED	
	CHECKED	
	BY	
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	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO.	
	FILE NAME	



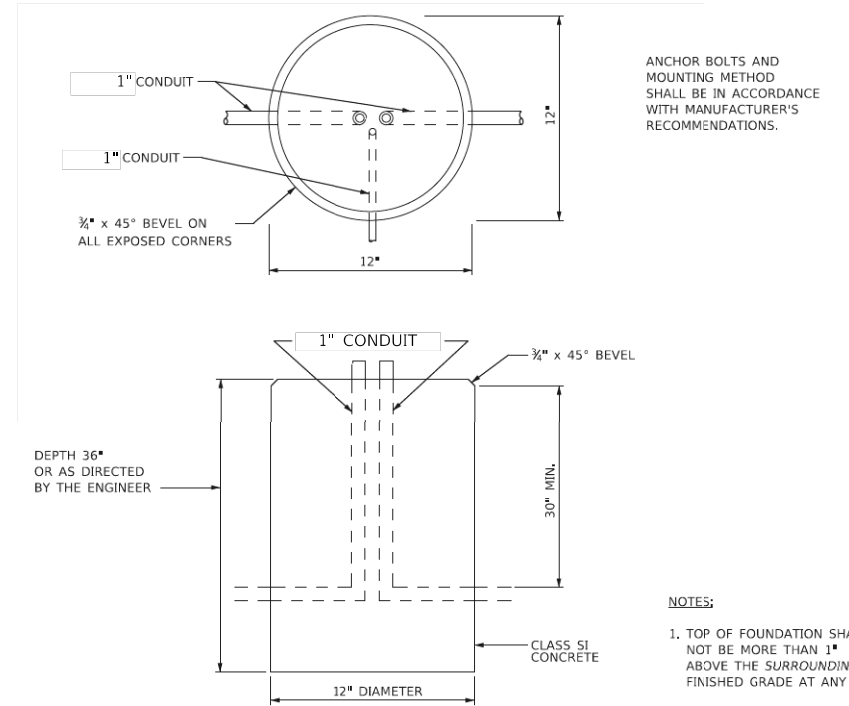
DECORATIVE FESTOON LIGHT POLE
HANDHOLE WIRING DIAGRAM

NOTE:
ALL MATERIALS SHALL BE INCLUDED IN THE COST OF LIGHT POLE, SPECIAL



TYPE A FOUNDATION FOR DECORATIVE
FESTOON LIGHT POLE

NOTE:
FOUNDATIONS FOR LIGHT POLE, SPECIAL SHALL BE PAID FOR AS CONCRETE FOUNDATION, TYPE A.



CONCRETE FOUNDATION (SPECIAL)

TranSystems
1475 EAST WOODFIELD ROAD, SUITE 600
SCHAUMBURG, ILLINOIS 60173
(847) 605-9600

FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
180080-WILMETTE DETAIL		DRAWN - MLB	REVISED -
Default		CHECKED - DWB	REVISED -
	PLOT DATE = 2/14/2020	DATE - 02/13/2020	REVISED -

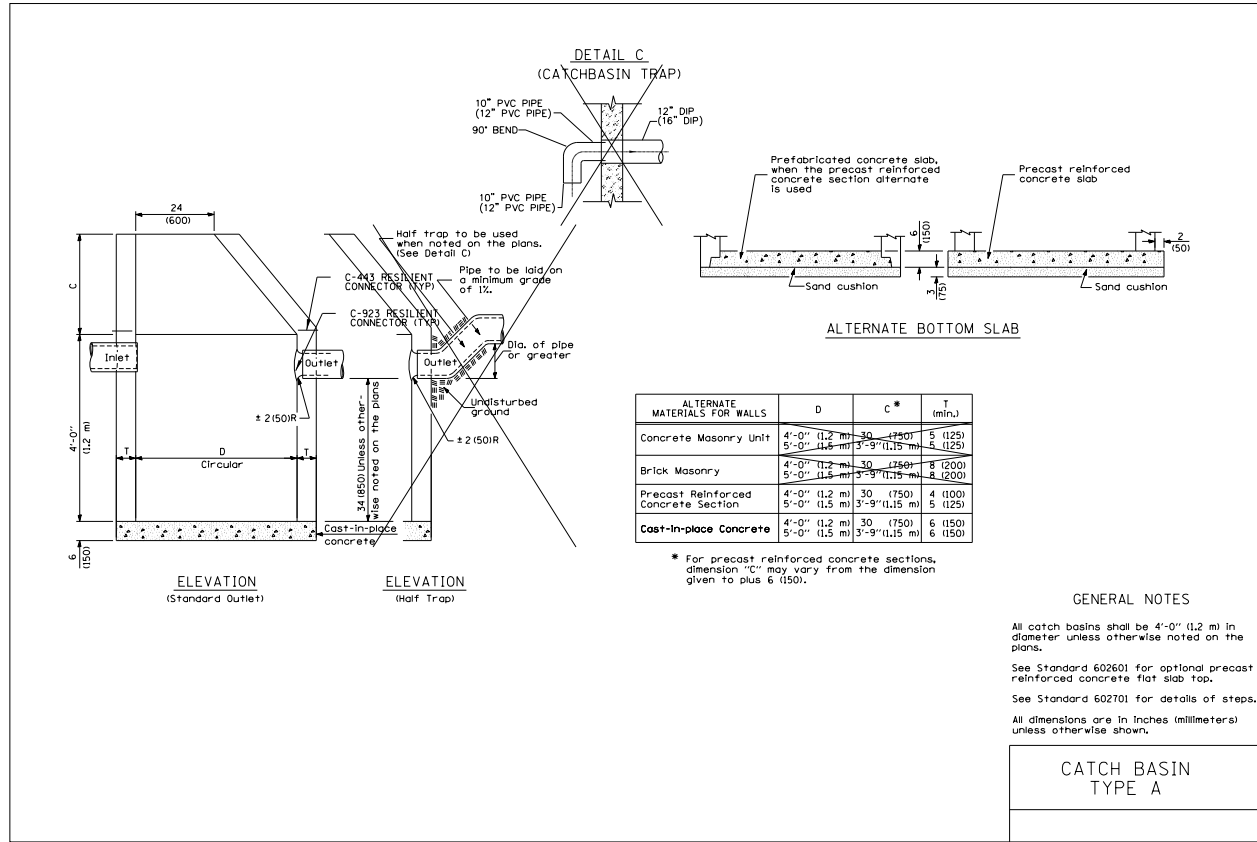
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CENTRAL AVENUE
UNION PACIFIC RAILROAD TO SHERIDAN ROAD
VILLAGE OF WILMETTE DETAILS

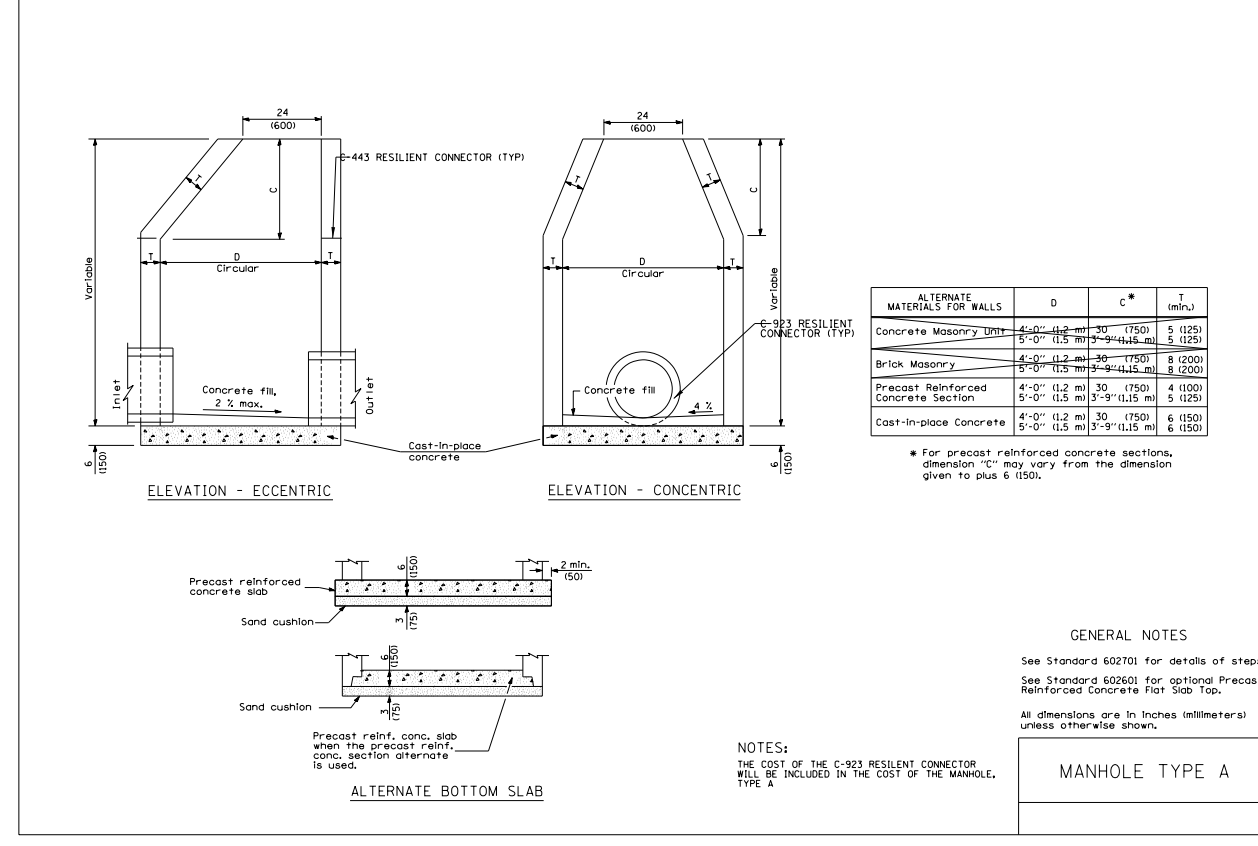
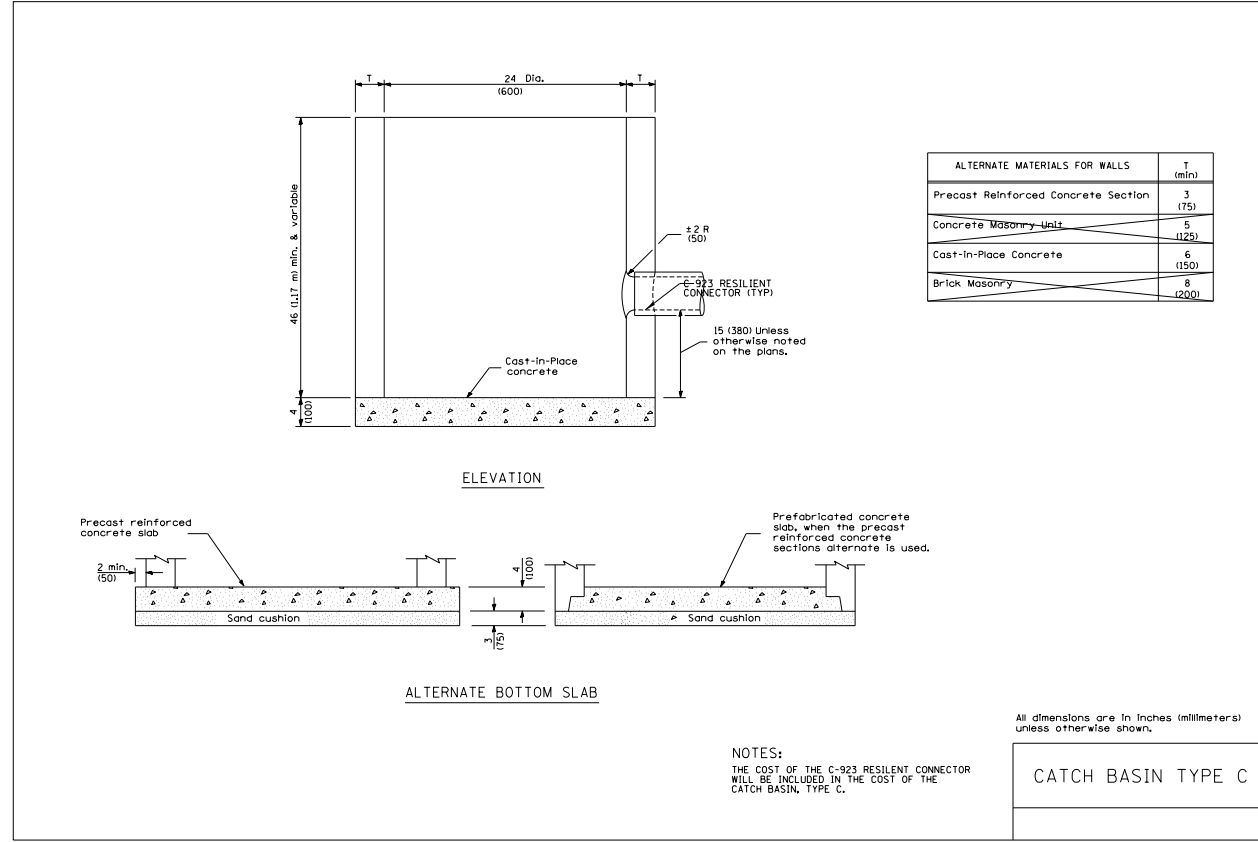
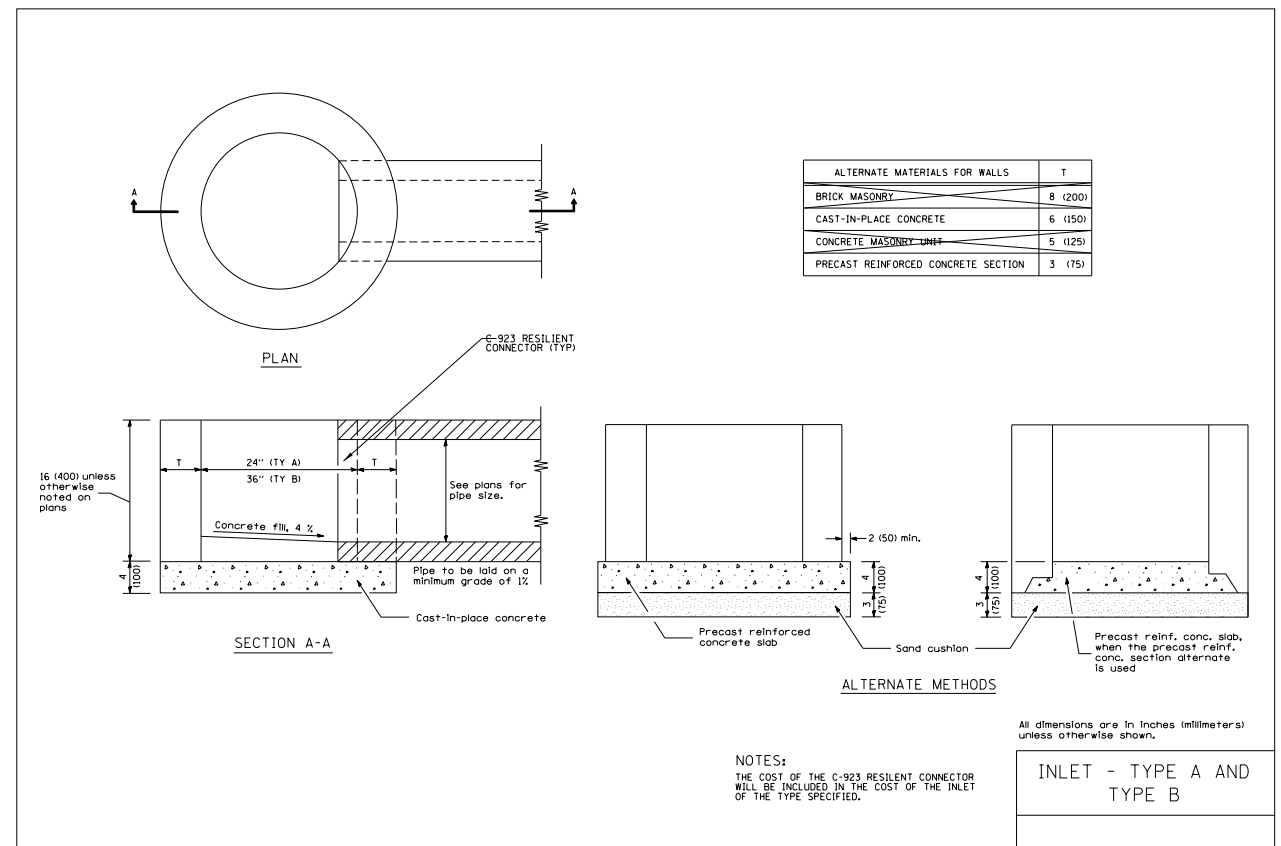
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	244
				CONTRACT NO. 61G42
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	ALIGNED		
	FILED		
NOTE BOOK NO.	CADD FILE NAME		



PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	ALIGNED		
	FILED		
NOTE BOOK NO.	CADD FILE NAME		



TranSystems
 1475 EAST WOODFIELD ROAD, SUITE 600
 SCHAMBURG, ILLINOIS 60173
 (847) 605-9600

FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -
180080-MWRDGC-DETAILS-03		DRAWN - MLB	REVISED -
Default		CHECKED - DWB	REVISED -
		DATE - 02/13/2020	REVISED -
		PLOT SCALE = 100.0005' / in.	
		PLOT DATE = 2/14/2020	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

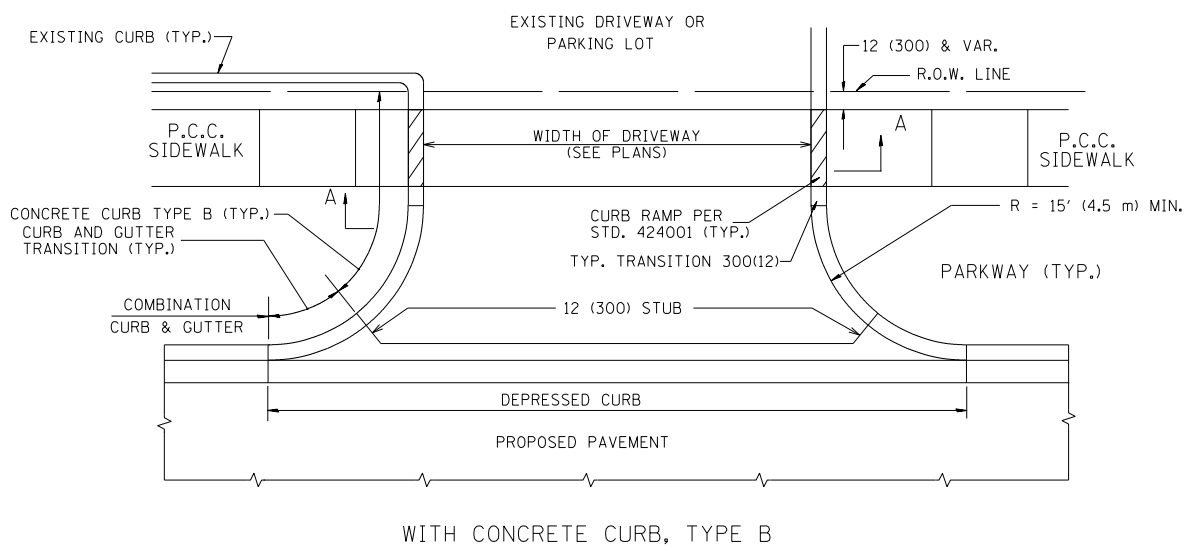
**CENTRAL AVENUE AND WILMETTE AVENUE
 MWRDGC DETAILS**

SCALE: 1"=50' SHEET 2 OF 2 SHEETS STA. TO STA.

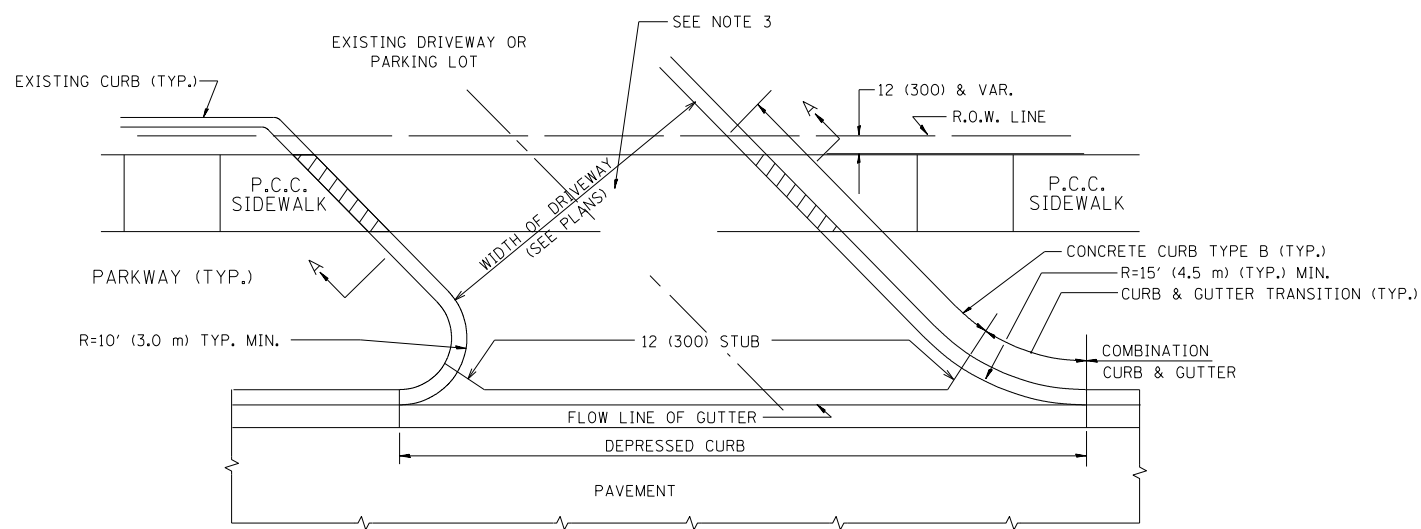
F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	246
				CONTRACT NO. 61G42
				ILLINOIS FED. AID PROJECT

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
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	NOTE BOOK NO.	

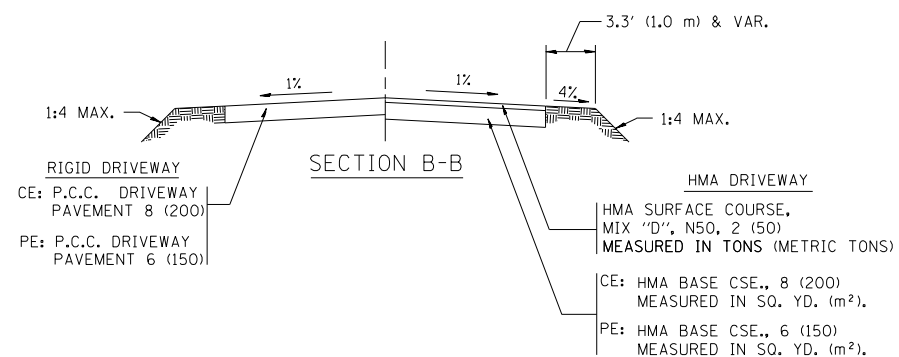
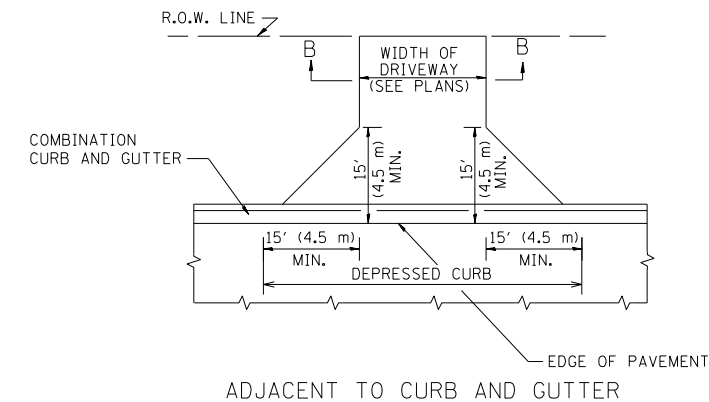
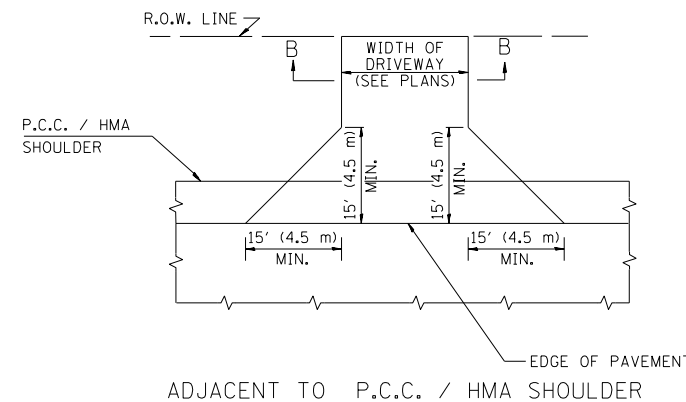
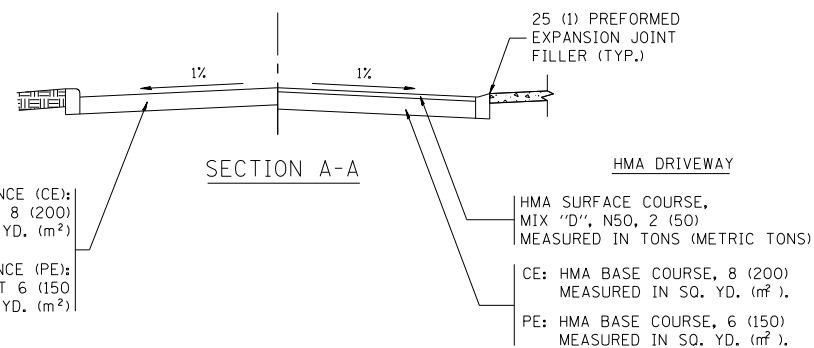
PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



RURAL FIELD ENTRANCE (FE)
HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)
AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

TranSystems
1475 EAST WOODFIELD ROAD, SUITE 600
SCHAMBURG, ILLINOIS 60173
(847) 605-9600

FILE NAME =	USER NAME = lryso	DESIGNED - R. SHAH	REVISED - P. LofLUER 04-15-03
c:\pw\work\p\dot\lryso\d0108315\bd01.dgn		DRAWN -	REVISED - R. BORO 01-01-07
180080-DISTDET-BD01		CHECKED -	REVISED - R. BORO 06-11-08
Default		DATE - 11-04-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

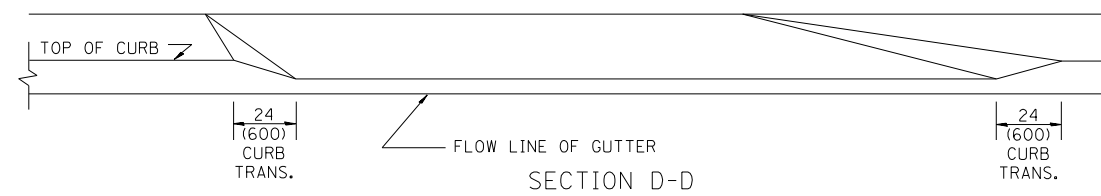
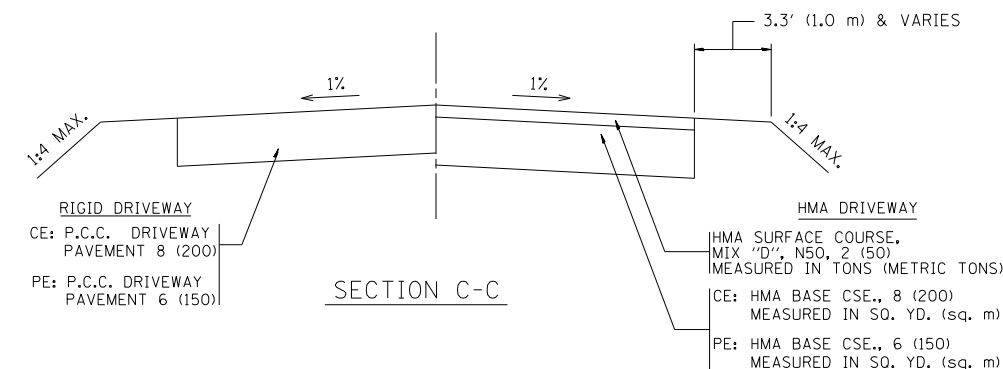
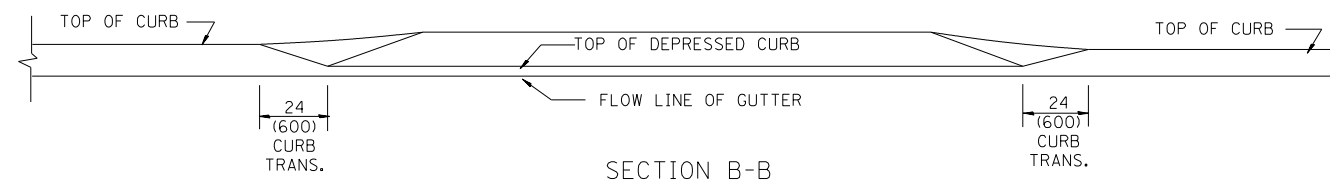
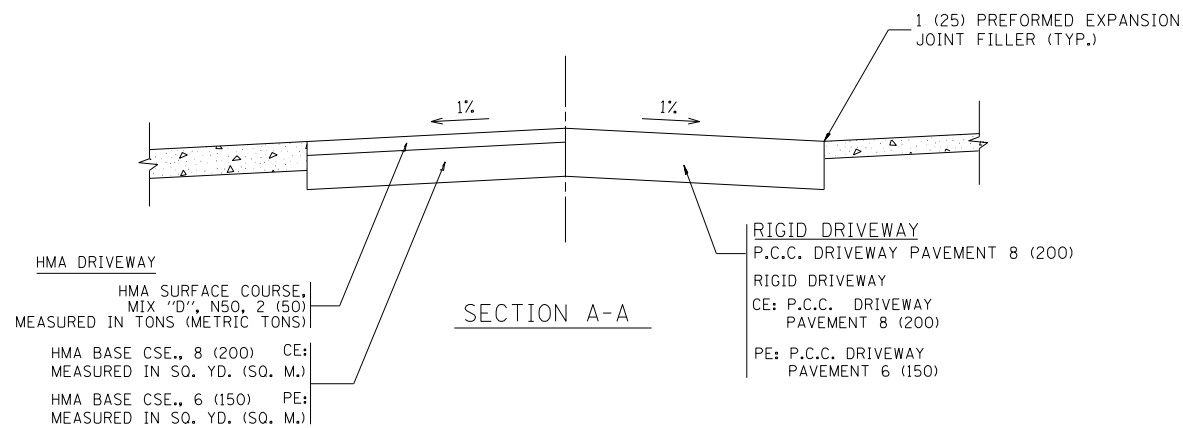
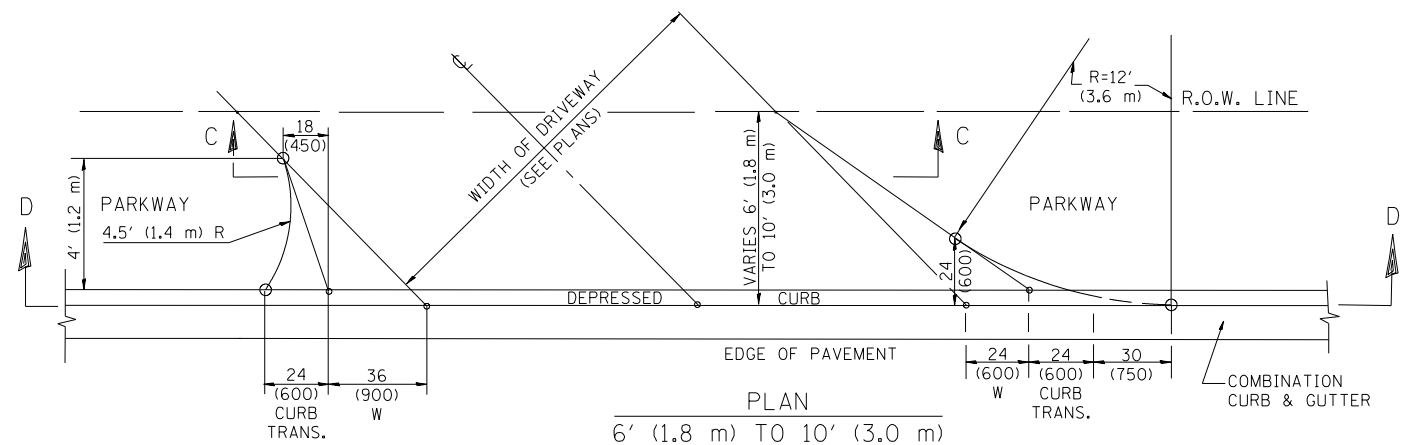
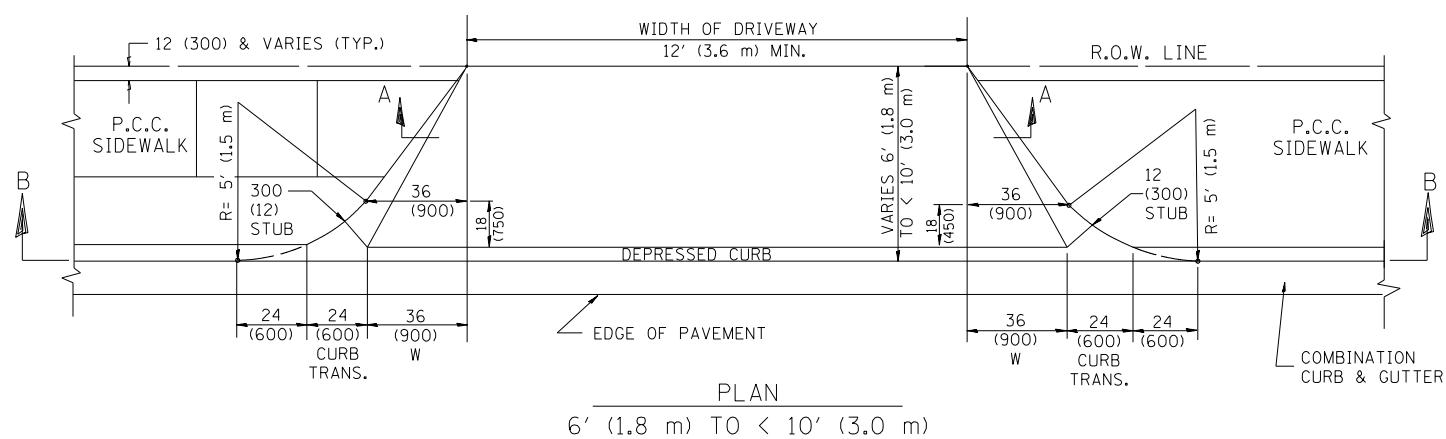
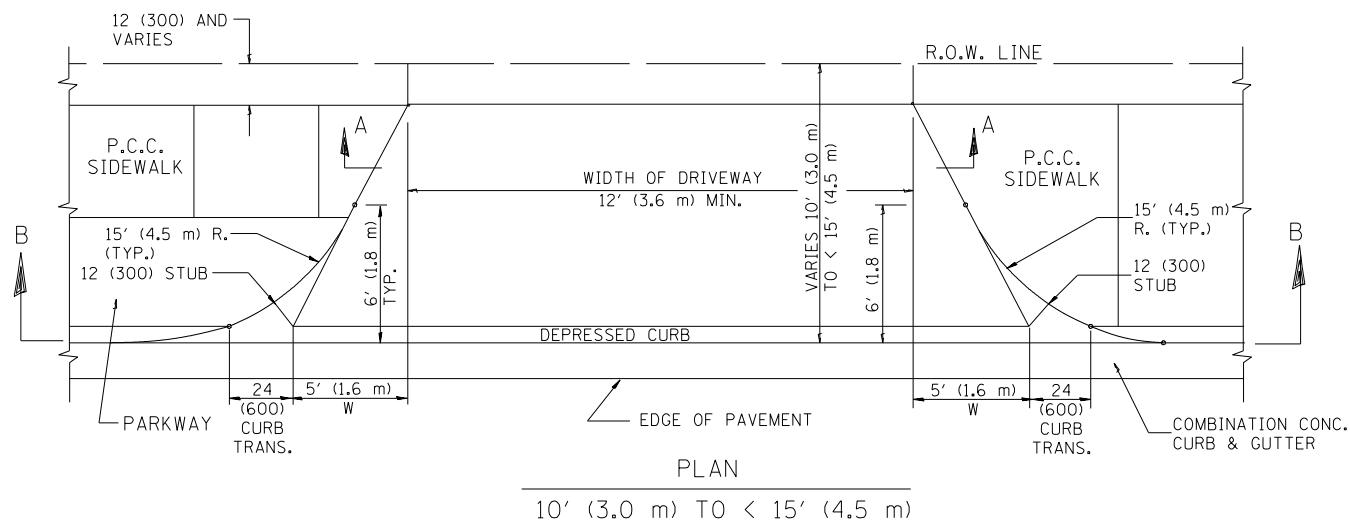
DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.
AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)

SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	247
BD0156-07 (BD-01)			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

DATE	
BY	
SURVEYED	
PLOTTED	
CHECKED	
ALIGNED	
FILE NAME	
PLAN	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
CHECKED	
ALIGNED	
FILE NAME	
PROFILE	
NOTE BOOK	
NO.	



GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = lleyso	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
ca:\pwork\pwork\lleyso\d0108315\bd02.dgn		DRAWN -	REVISED - P. LOFLEUR 04-15-03
180080-DISTDET-BD02	PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - R. BORO 01-01-07
Default	PLOT DATE = 10/28/2011	DATE - 11-06-95	REVISED - R. BORO 09-06-11

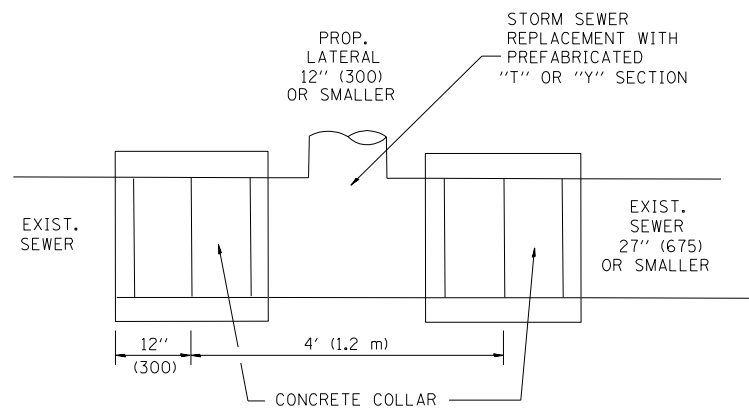
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS			
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS SHEET NO.
1296	16-00199-00-RS	COOK	318 248
BD400-02 (BD-02)		CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

DATE	
BY	
SURVEYED	
PLOTTED	
CHECKED	
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NOTE BOOK	
NO.	

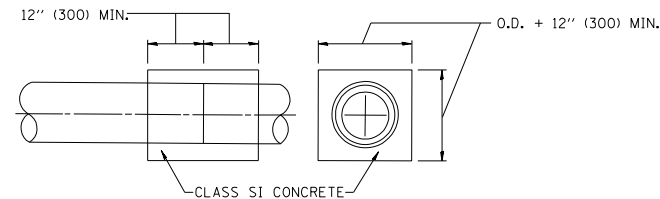
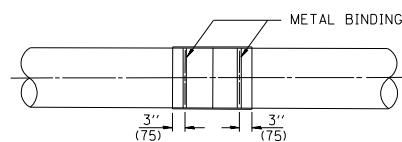
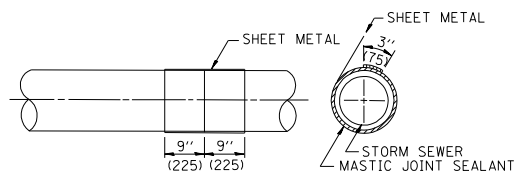
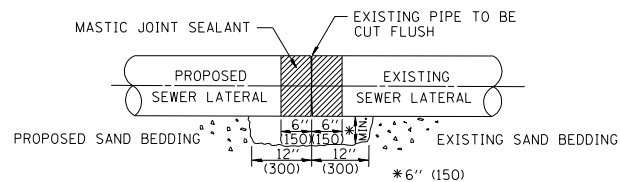
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DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

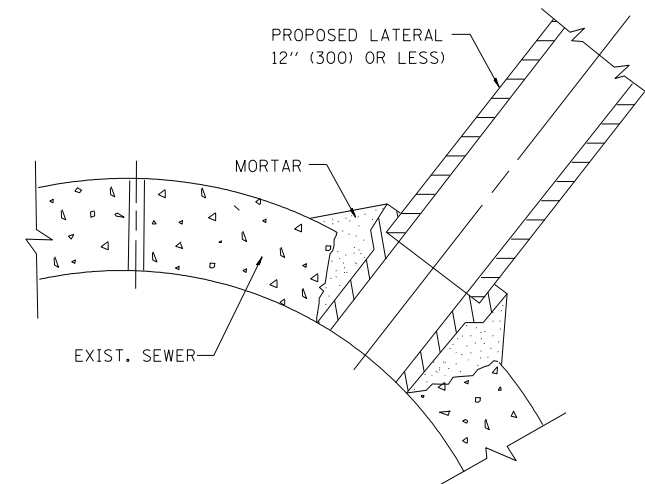


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gajlonobt	DESIGNED - M. DE YONG	REVISED - M. DE YONG 05-08-92
W:\diststd\22x34\bd07.dgn		DRAWN -	REVISED - R. SHAH 09-09-94
180080-DISTDET-BD07		CHECKED -	REVISED - R. SHAH 10-25-94
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**DETAIL OF STORM SEWER
 CONNECTION TO EXISTING SEWER**

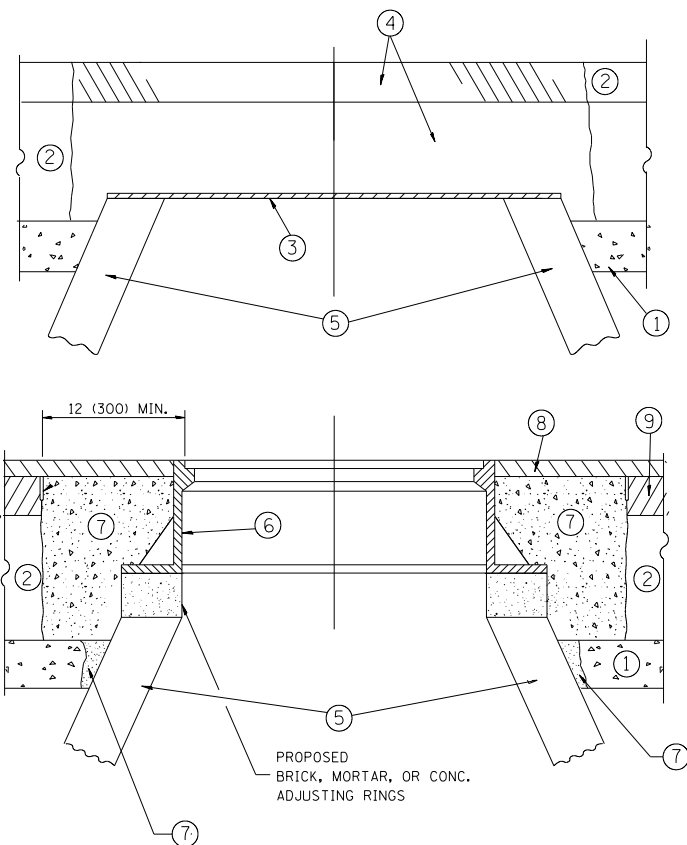
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD500-01 (BD-7)		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
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NOTE BOOK NO.	GRADES CHECKED	BY
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NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
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180080-DISTDET-BD08	PLOT SCALE = 1/68.5000 "/ m	CHECKED -	REVISED - R. BORO 03-09-11
Default	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	250
BD600-03 (BD-3)		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

3" (75) MIN.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96
ca:\pw\work\p\dot\drivakosgn\0108315\bd24.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
180080-DISTDET-BD24	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01
Default	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

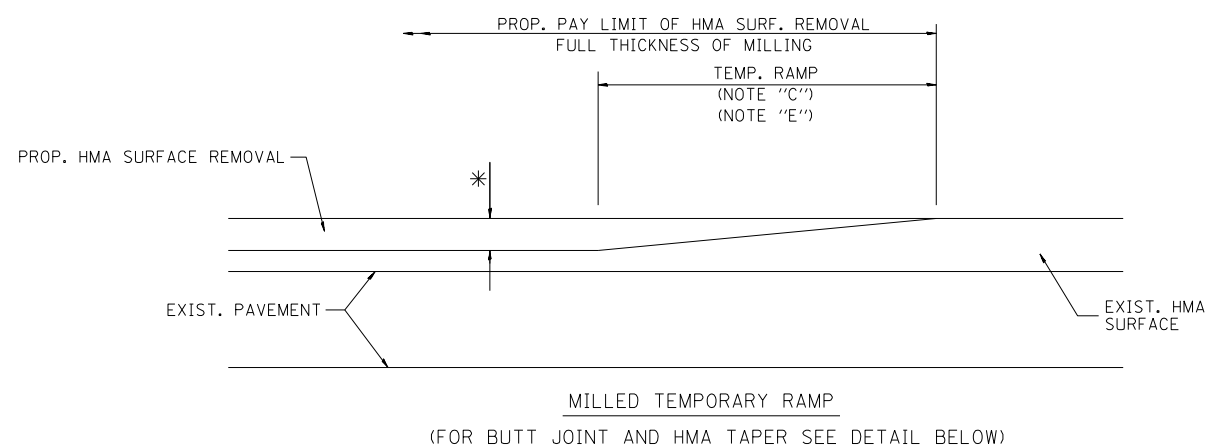
**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

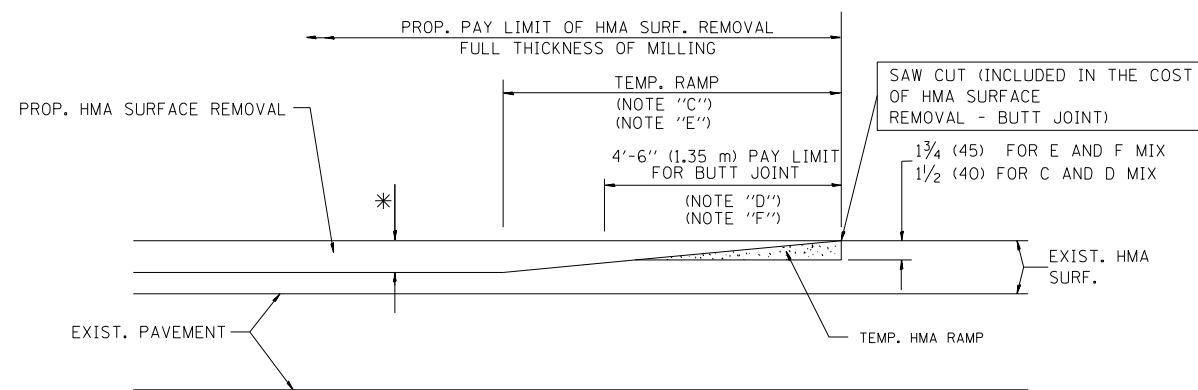
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BD600-06 (BD-24)		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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	DATE _____	

PROFILE	SURVEYED	DATE
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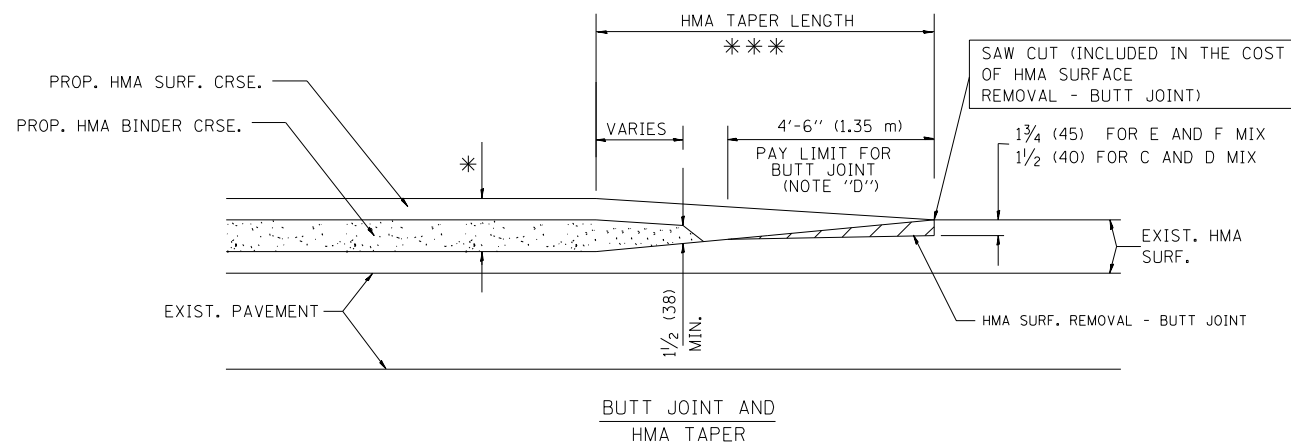


OPTION 1

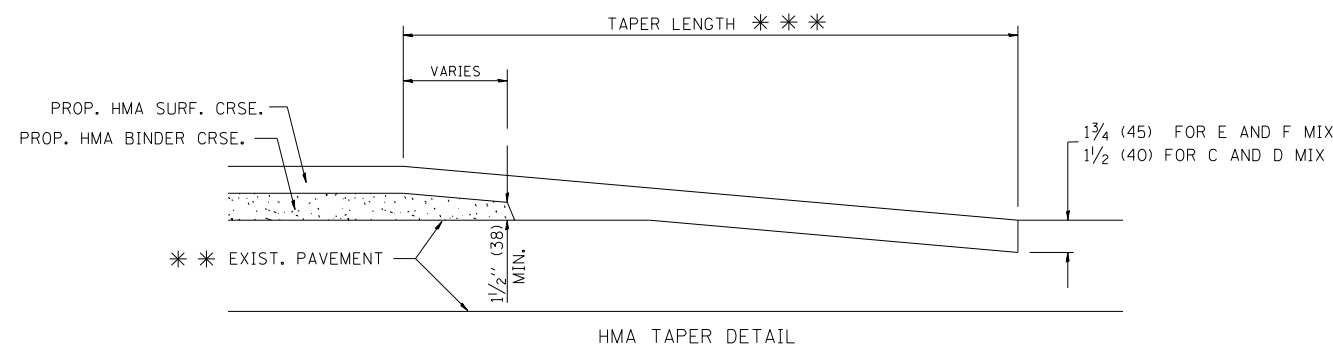
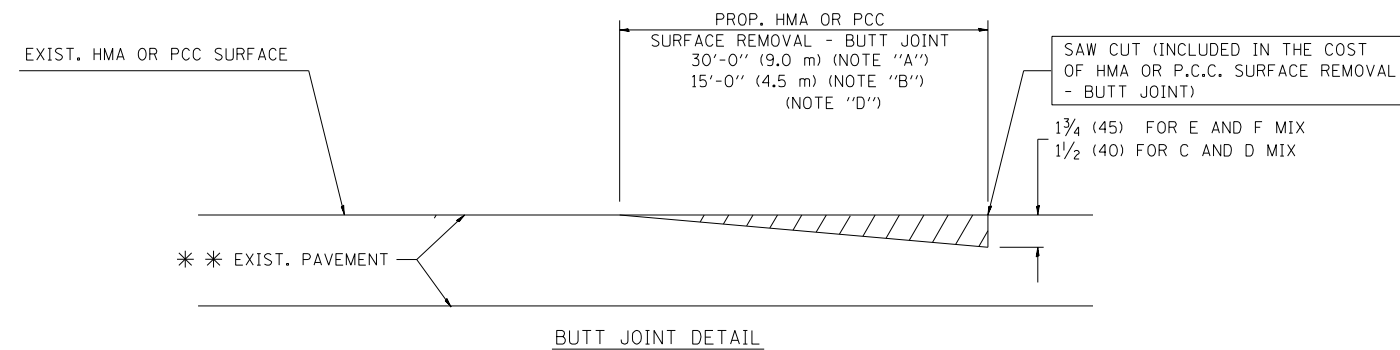


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gegl@nobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
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180080-DISTDET-BD32		CHECKED -	REVISED - M. GOMEZ 04-06-01
Default		DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	252
BD400-05 BD32		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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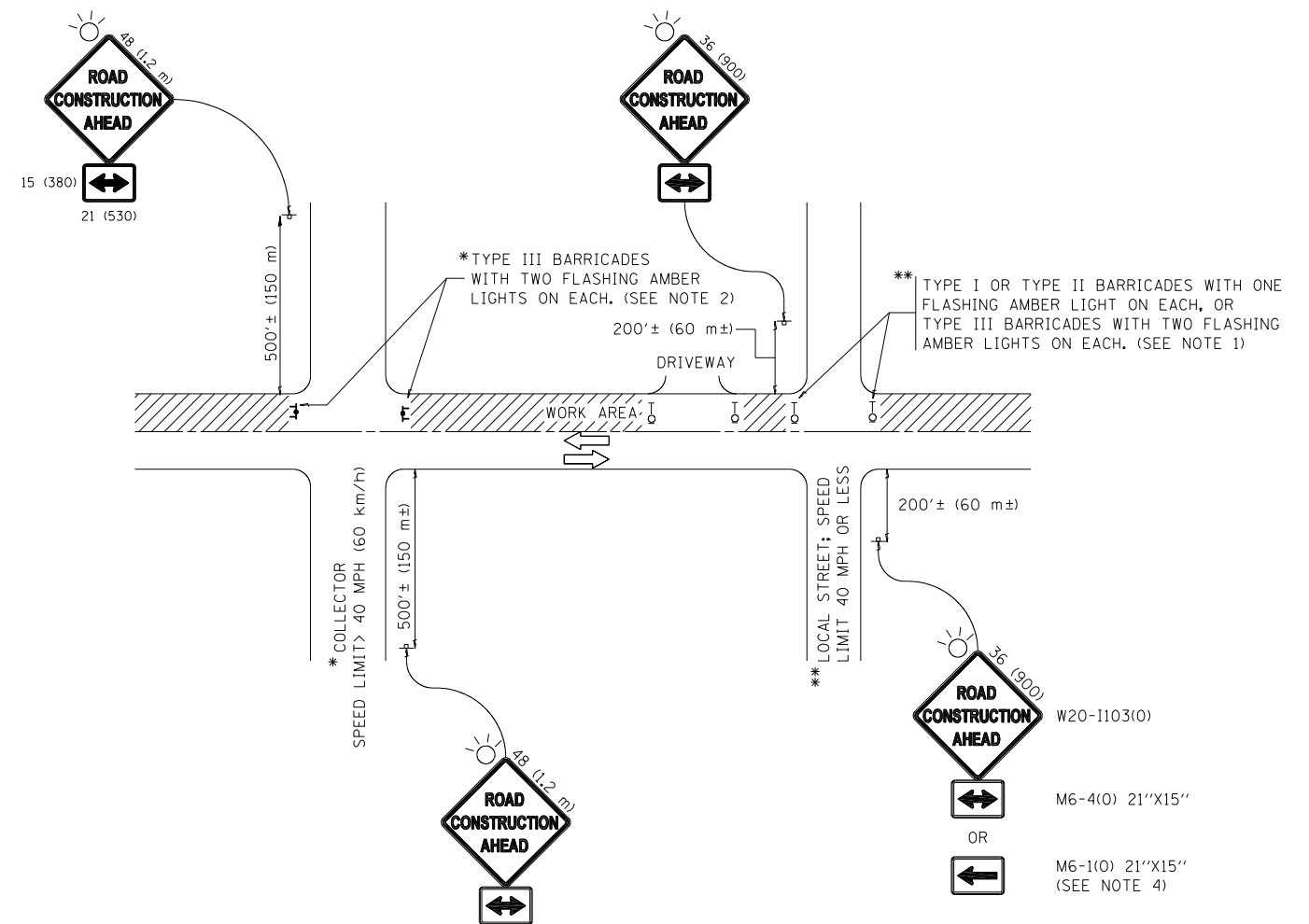
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180080-DISTDET-TC10	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
Default	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
 SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	253
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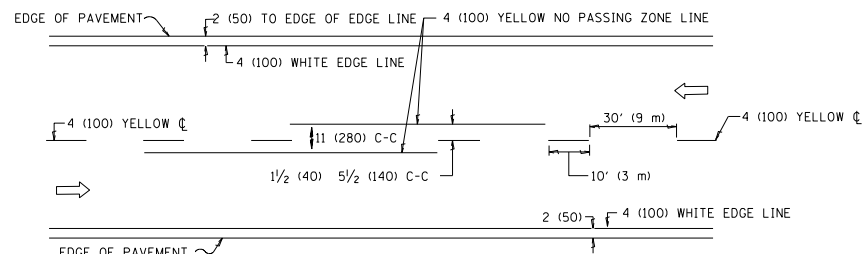
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

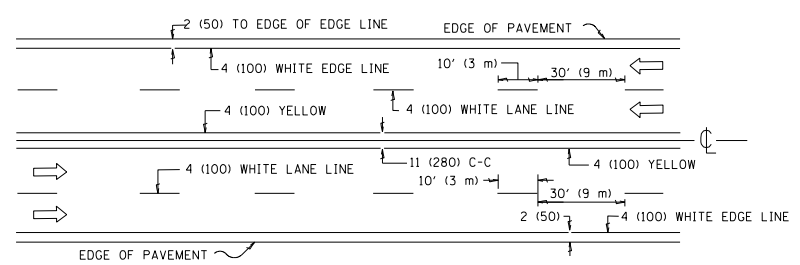
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NOTE BOOK	
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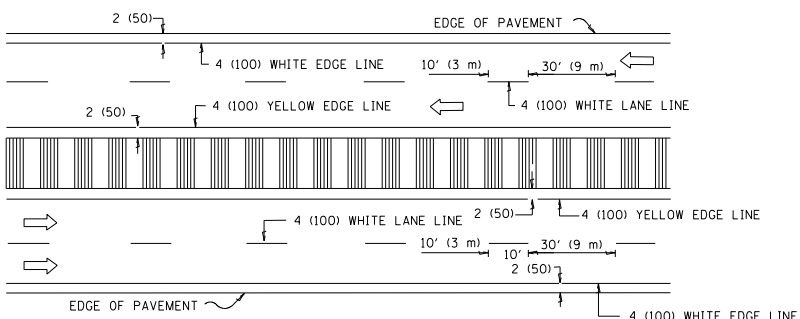
TranSystems
1475 EAST WOODFIELD ROAD, SUITE 600
SCHAMBURG, ILLINOIS 60173
(847) 605-9600



2-LANE ROADWAY

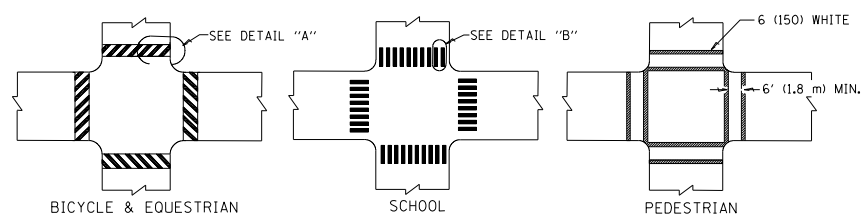


MULTI-LANE UNDIVIDED



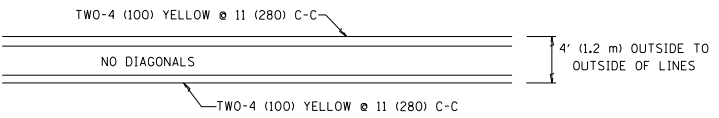
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

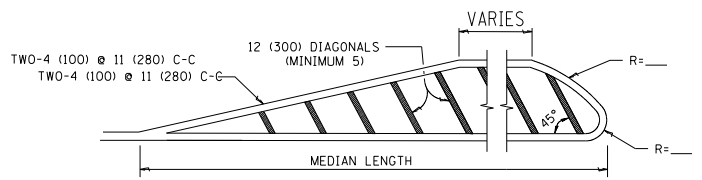


DETAIL "A"
DETAIL "B"
TYPICAL CROSSWALK MARKING

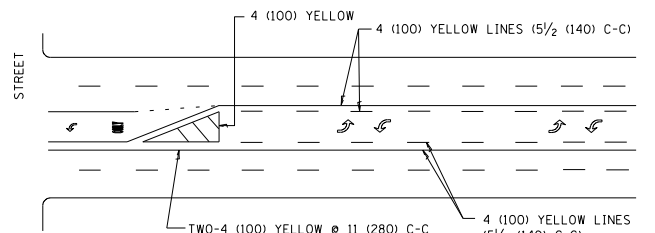
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



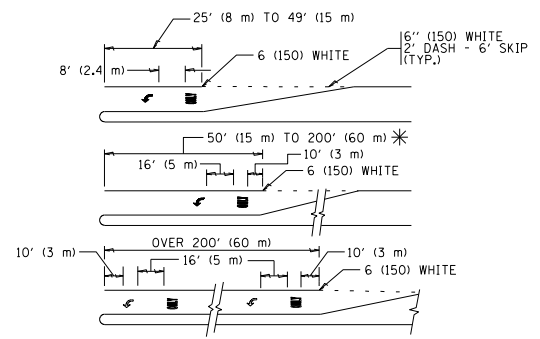
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

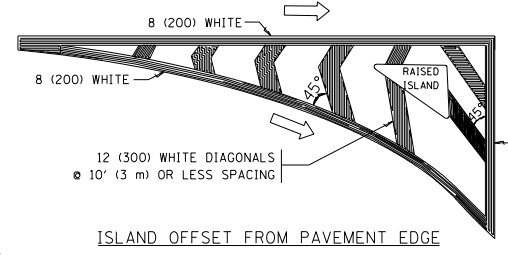


MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING

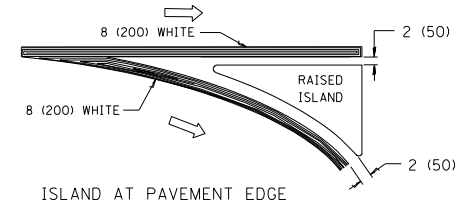


TYPICAL LEFT (OR RIGHT) TURN LANE
TYPICAL TURN LANE MARKING

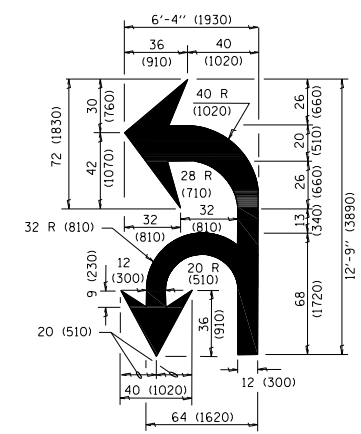
FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



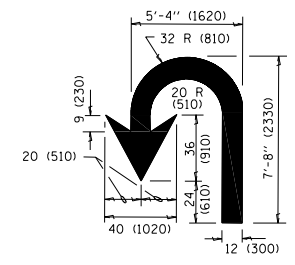
ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

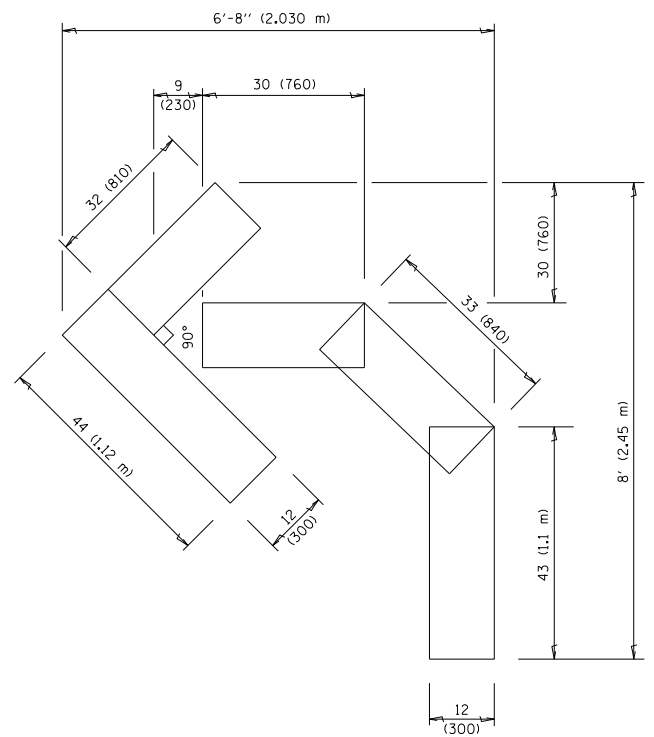
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W:\diststd\22x34\1c13.dgn		DRAWN -	REVISED - C. JUCIUS 07-01-13
180080-DISTDET-1C13		CHECKED -	REVISED - C. JUCIUS 12-21-15
Default		DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: NONE		SHEET 1 OF 1 SHEETS		STA.	TO STA.
DISTRICT ONE TYPICAL PAVEMENT MARKINGS					
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
1296	16-00199-00-RS	COOK	318	254	
TC-13			CONTRACT NO.		
ILLINOIS FED. AID PROJECT					

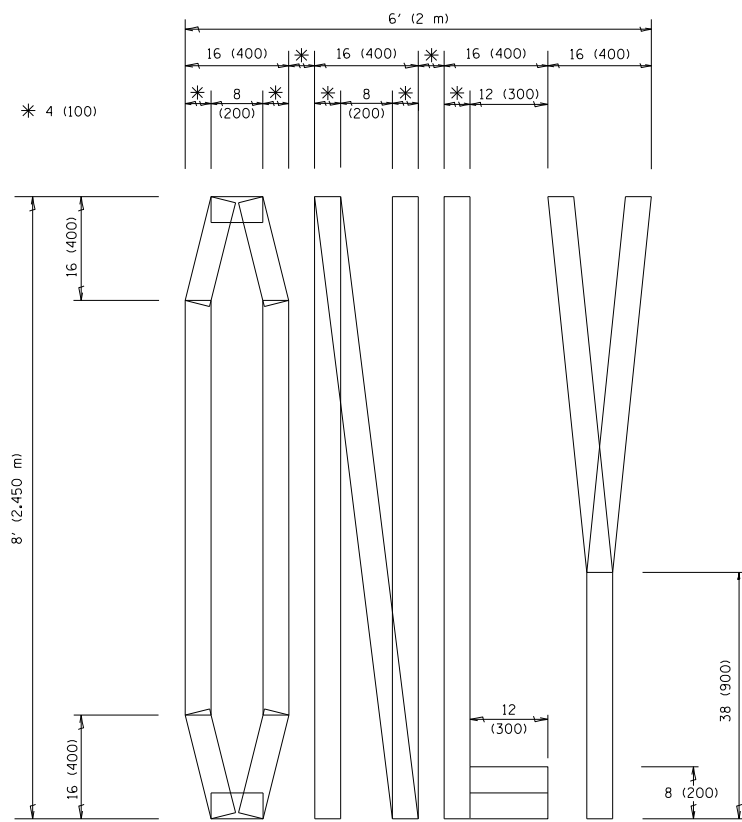
PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	CHECKED	
	ALIGNED	
	CADD FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK	GRADES CHECKED	BY
NO.	STRUCTURE NOTATIONS CHECKED	



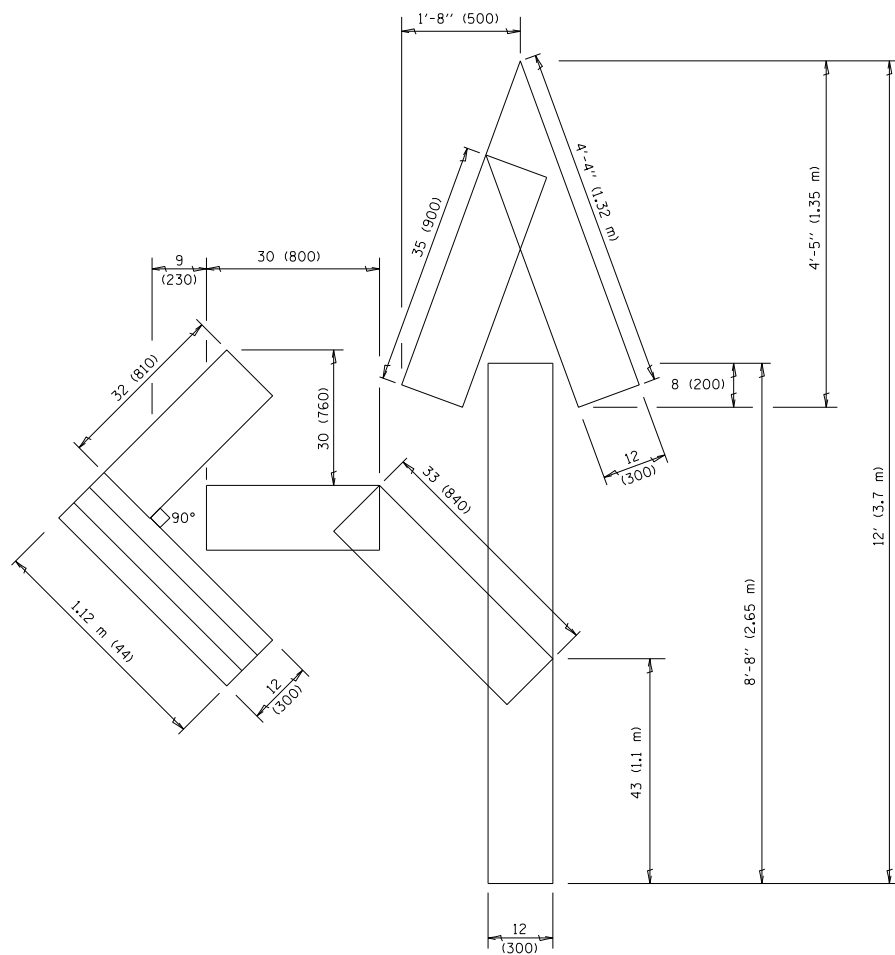
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

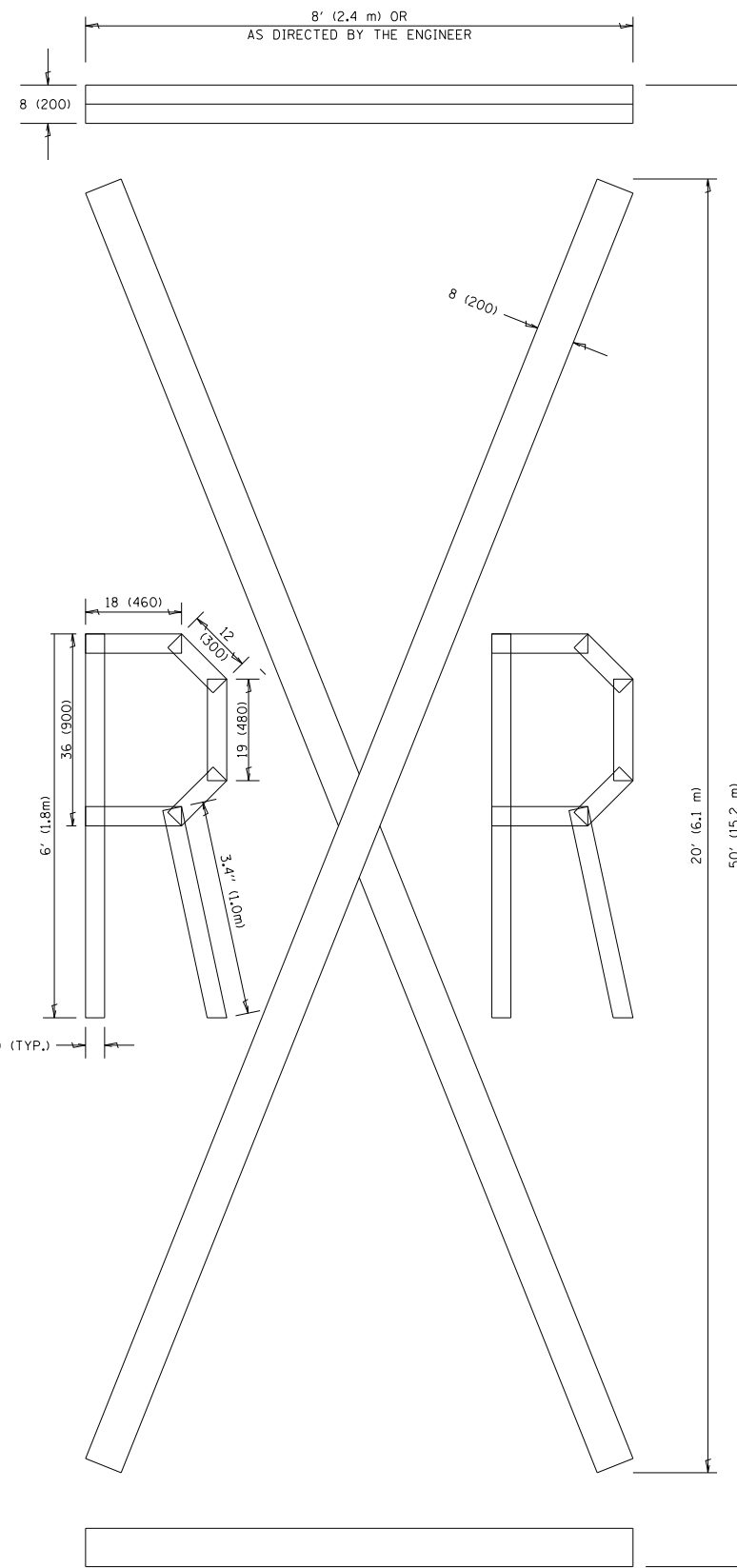


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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FILE NAME =	USER NAME = footemj	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
p:\1\084EBIDINTEG.illinois.gov\PIWDD\Documents\DOT Offices\District 1\Projects\Dist 1\084EBIDINTEG\CADD\Drawings\CAD\Drawings\TC16.dgn		CHECKED -	REVISED -E. GOMEZ 08-28-00
180080-DISTDET-TC16		DATE -	REVISED -E. GOMEZ 08-28-00
Default		PLOT DATE = 9/15/2016	REVISED -A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

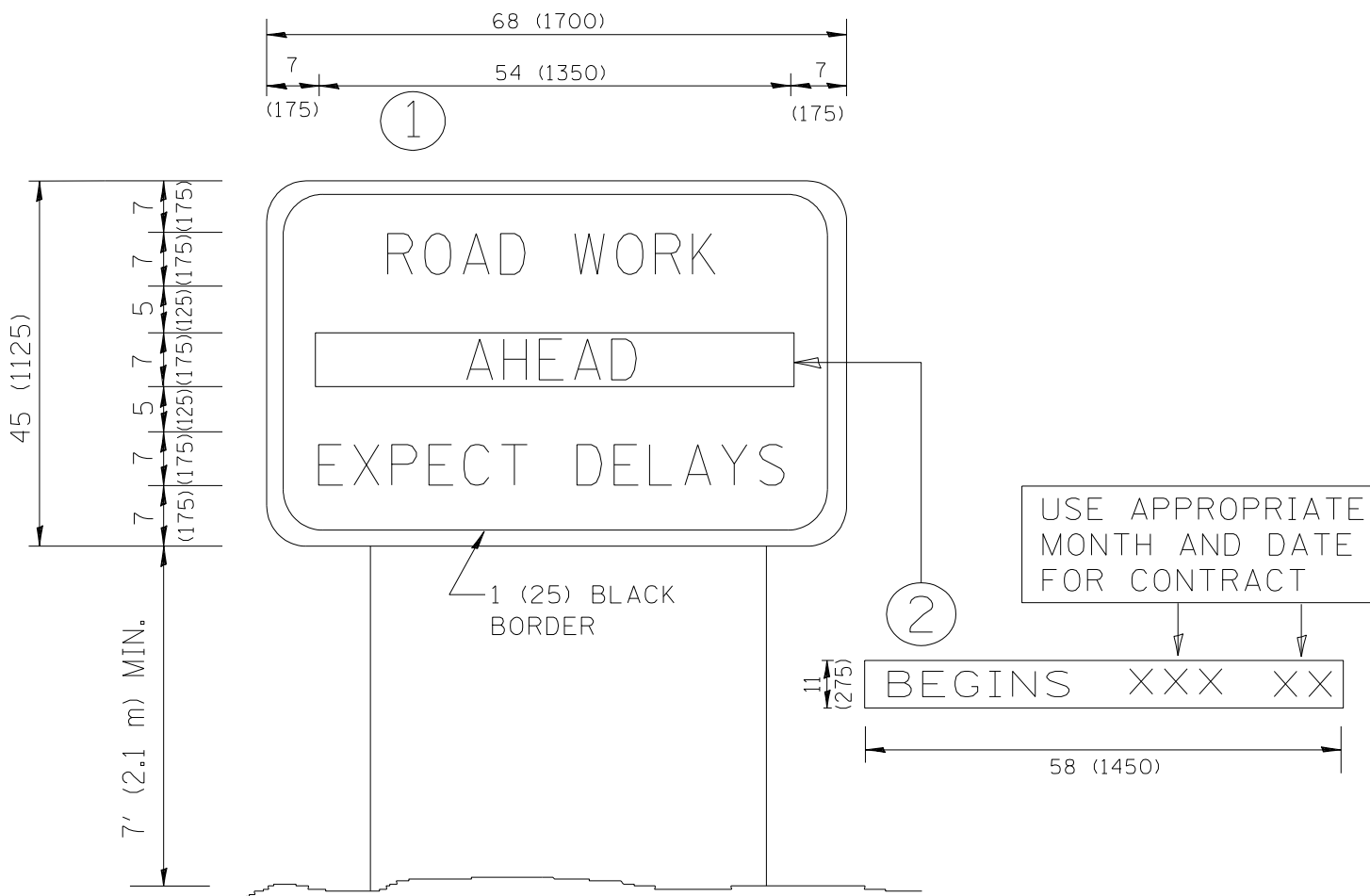
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	255
TC-16			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	NOTE BOOK NO.		
	CHECKED		
	DATE		
	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	NOTE BOOK NO.		
	CHECKED		
	DATE		

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NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gegl@onbt	DESIGNED -	REVISED - R. MIRS 09-15-97
W:\diststd\22x34\tc22.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
180080-DISTDET-TC22		CHECKED -	REVISED - T. RAMMACHER 02-02-99
Default		DATE -	REVISED - C. JUCIUS 01-31-07
	PLOT SCALE = 50.000' / IN.		
	PLOT DATE = 1/4/2008		

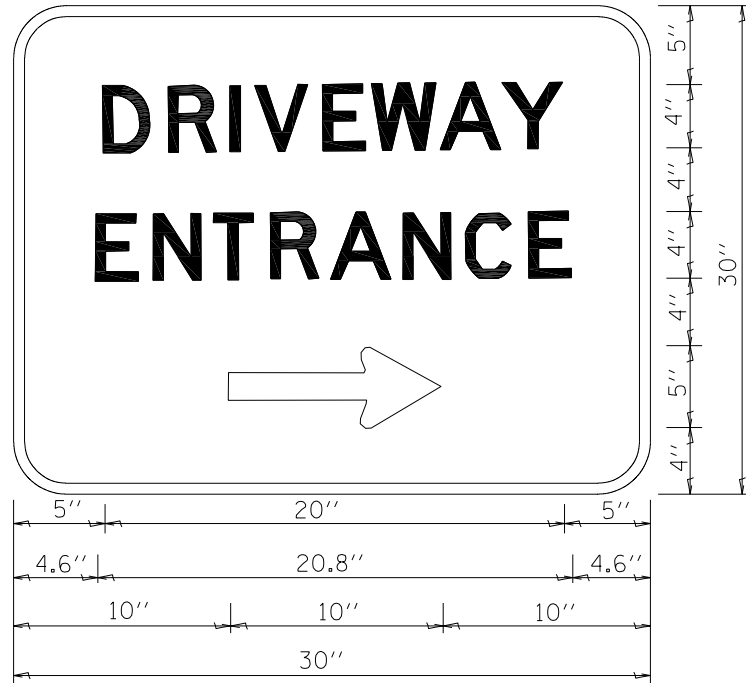
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ARTERIAL ROAD INFORMATION SIGN	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	256
TC-22		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	CHECKED		
	ALIGNED		
	CAD FILE NAME		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO.	GRADES CHECKED		
	STRUCTURE		
	NOT AT THIS OFFICE		



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".



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FILE NAME =	USER NAME = gegl1enobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
ct:\pw\work\pwidot\gagl1enobt\d0108315\tc26.dgn		DRAWN -	REVISED -
180080-DISTDET-TC26		CHECKED -	REVISED -
Default		DATE -	REVISED -
	PLOT SCALE = 50.000' / in.		
	PLOT DATE = 12/13/2012		

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

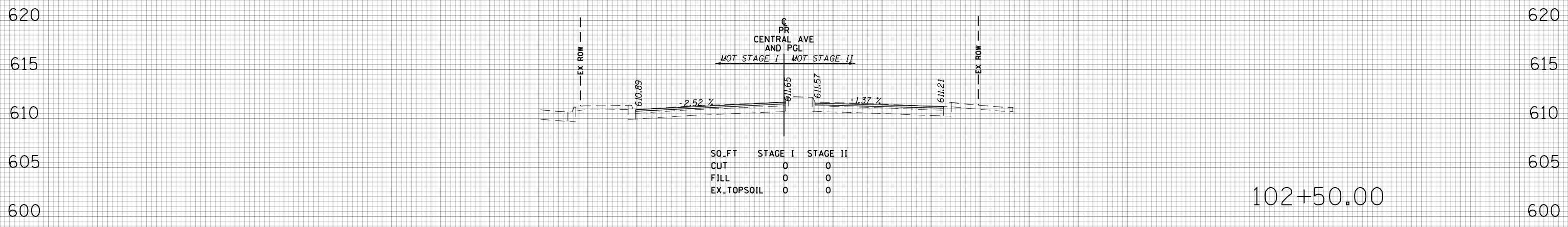
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

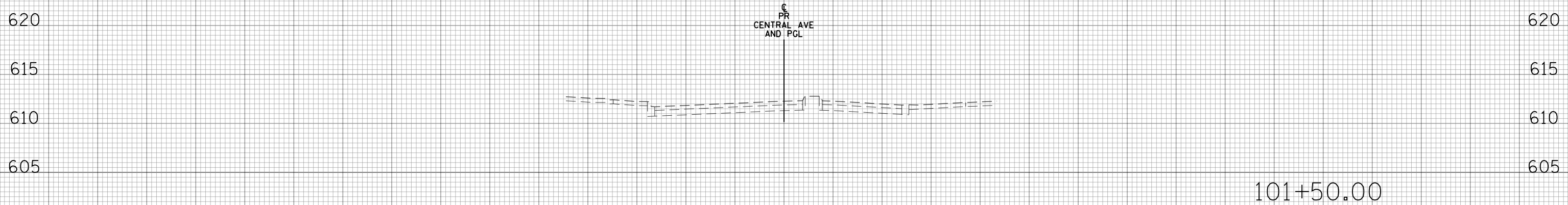
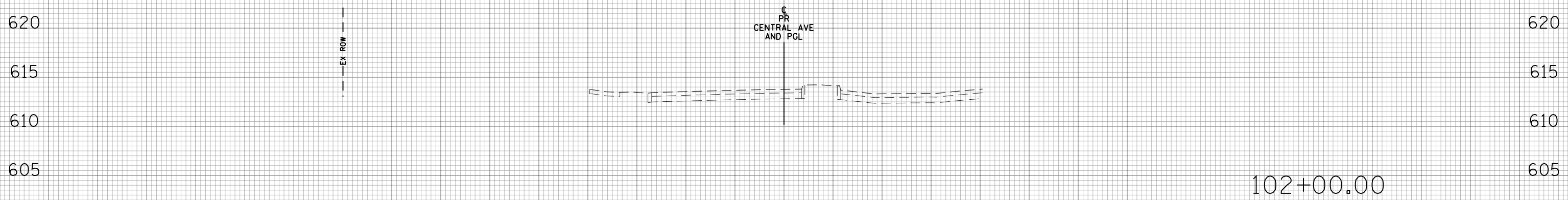
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TC-26			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	



DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	

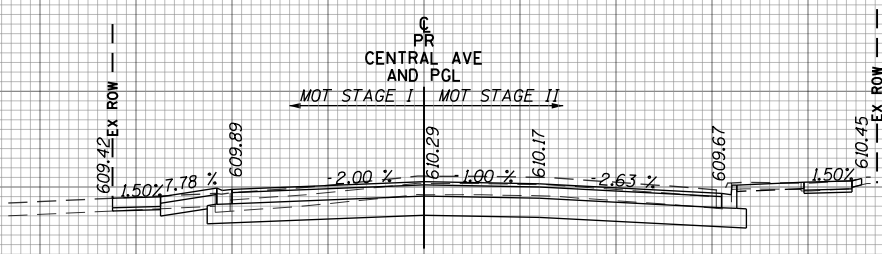


160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISIED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 258
	PLOT SCALE = 20.0000' / in.	CHECKED - DWB	REVISIED -		SCALE: 10H:5V	SHEET NO.258 OF 53 SHEETS	STA. 101+50.00 TO STA. 102+50.00	CONTRACT NO. 61G42		ILLINOIS FED. AID PROJECT		
	PLOT DATE = 2/14/2020	DATE 02/13/2020	REVISIED -									

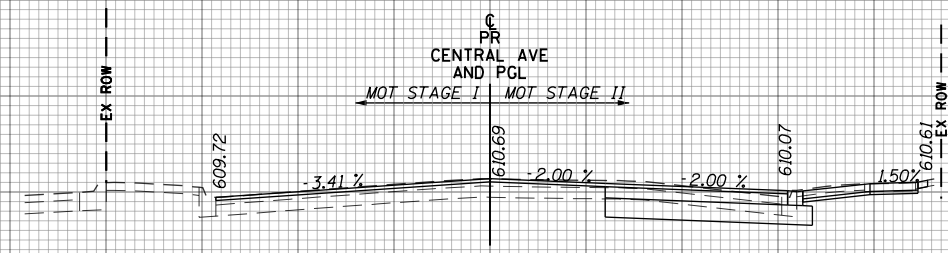
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620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600



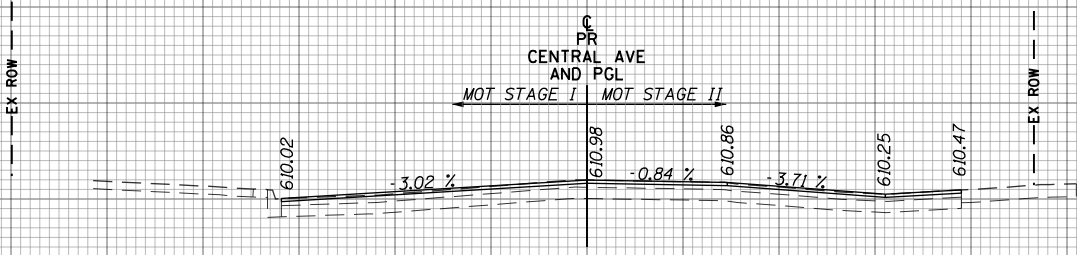
SO.FT	STAGE I	STAGE II
CUT	20	38
FILL	0	0
EX.TOPSOIL	0	3

104+00.00



SO.FT	STAGE I	STAGE II
CUT	0	20
FILL	0	9
EX.TOPSOIL	0	3

103+50.00



SO.FT	STAGE I	STAGE II
CUT	0	0
FILL	0	0
EX.TOPSOIL	0	0

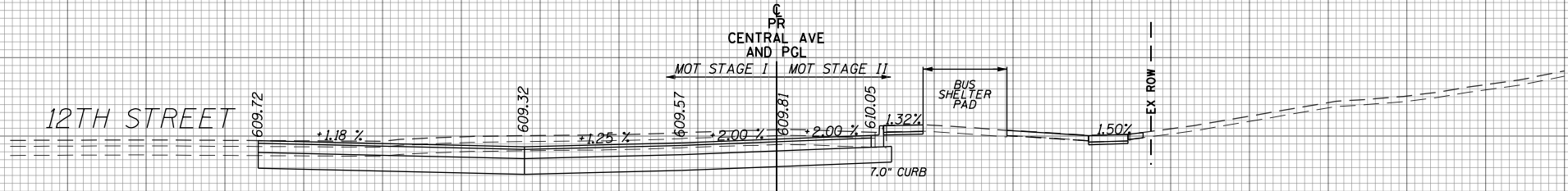
103+00.00

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY NOTE BOOK NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY NOTE BOOK NO.	

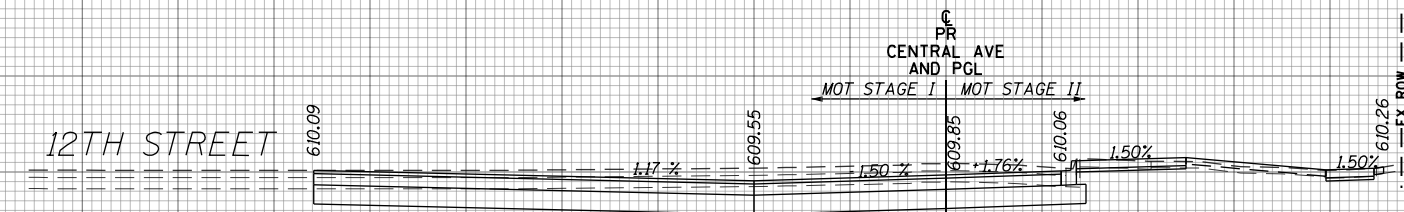
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FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 259
	PLOT SCALE = 20.0000' / in.	CHECKED - DWB	REVISED -		SCALE: 10H:5V	SHEET NO.259 OF 53 SHEETS	STA. 103+00.00 TO STA. 104+00.00	CONTRACT NO. 61G42		ILLINOIS FED. AID PROJECT		
	PLOT DATE = 2/14/2020	DATE 02/13/2020	REVISED -									



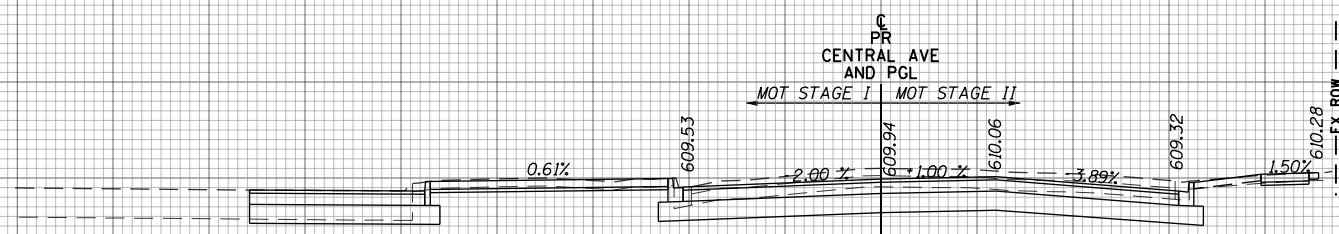
SO.FT	STAGE I	STAGE II
CUT	83	19
FILL	0	4
EX.TOPSOIL	0	4

104+80.50



SO.FT	STAGE I	STAGE II
CUT	78	19
FILL	0	2
EX.TOPSOIL	0	6

104+69.46

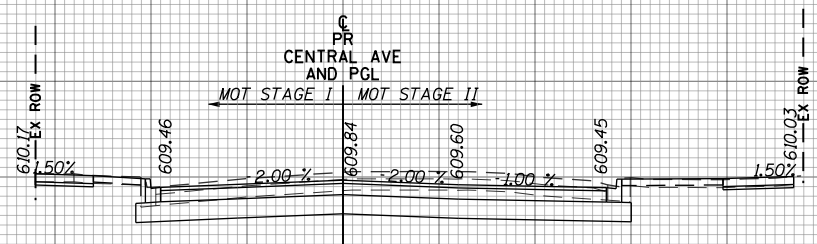


SO.FT	STAGE I	STAGE II
CUT	30	42
FILL	0	1
EX.TOPSOIL	0	0

104+50.00

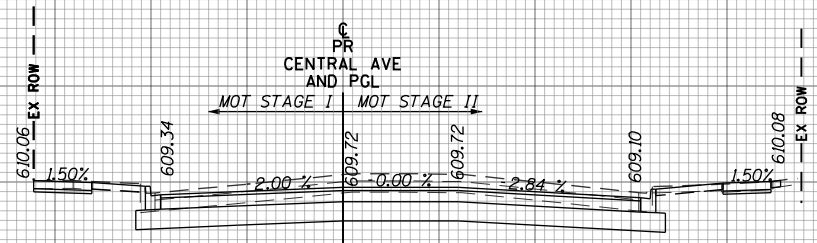
DATE _____ BY _____
 ORIGINAL SURVEY PLOTTED TEMPLATE AREAS CHECKED

DATE _____ BY _____
 FINAL SURVEY PLOTTED TEMPLATE AREAS CHECKED



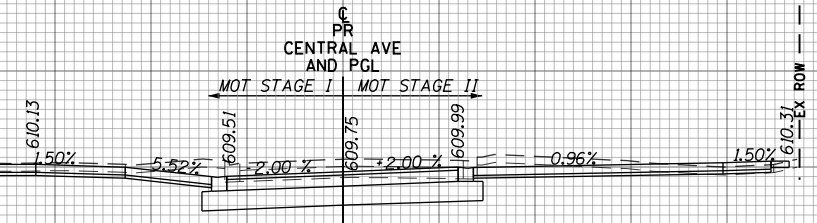
SO.FT	STAGE I	STAGE II
CUT	23	40
FILL	0	1
EX.TOPSOIL	0	0

106+00.00



SO.FT	STAGE I	STAGE II
CUT	26	46
FILL	0	0
EX.TOPSOIL	0	3

105+50.00



SO.FT	STAGE I	STAGE II
CUT	35	29
FILL	0	1
EX.TOPSOIL	0	0

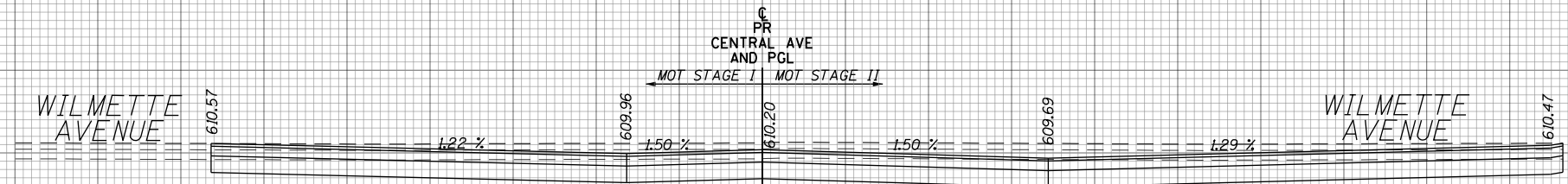
105+00.00

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

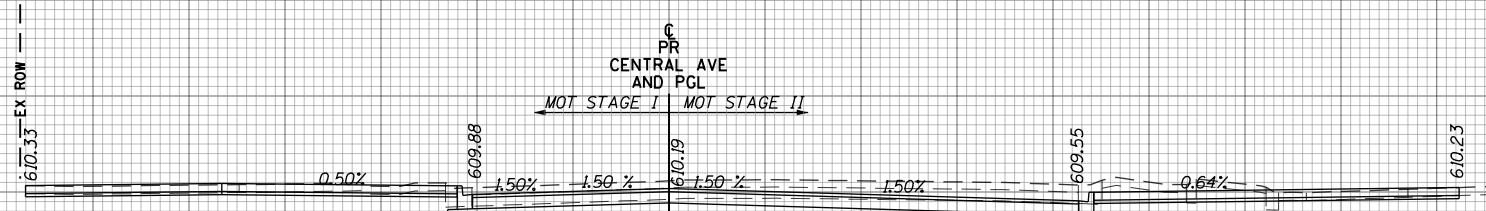
620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	75	127
FILL	0	0
EX.TOPSOIL	0	0

107+05.07
(SECTION ALONG
WILMETTE AVE. (C))

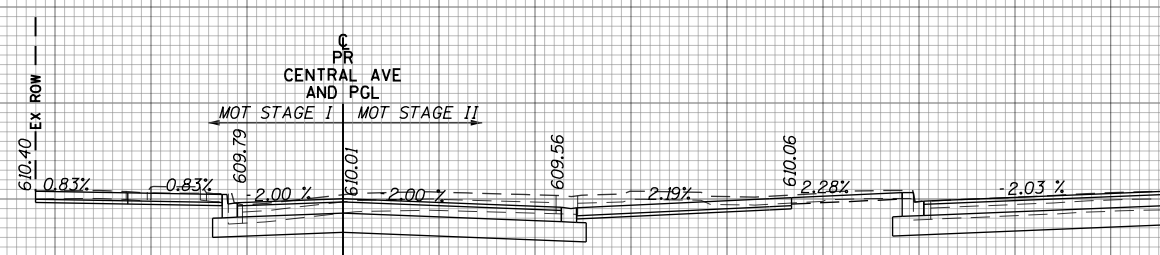
620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	33	79
FILL	0	1
EX.TOPSOIL	0	2

107+00.00

620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	18	47
FILL	0	0
EX.TOPSOIL	2	0

106+50.00

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

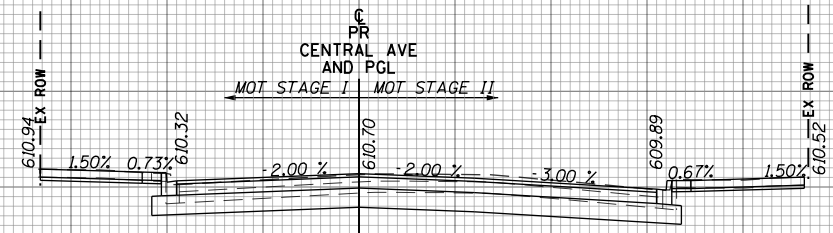
DATE	
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SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	

FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - MLB	REVISED -		1296	16-00199-00-RS	COOK	318	262			
		CHECKED - DWB	REVISED -		CONTRACT NO. 61G42							
		DATE 02/13/2020	REVISED -		SCALE: 10H:5V	SHEET NO262 OF 53 SHEETS	STA. 106+50.00 TO STA. 107+05.07	ILLINOIS FED. AID PROJECT				

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

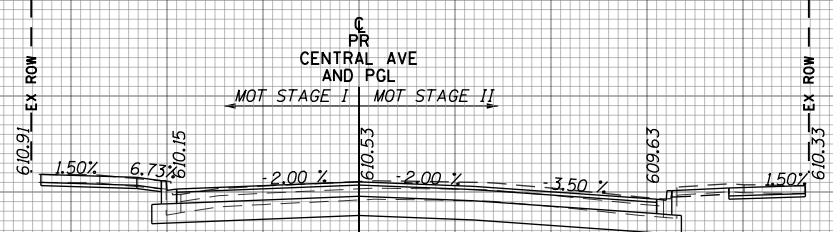
620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	17	32
FILL	0	0
EX.TOPSOIL	0	0

108+50.00

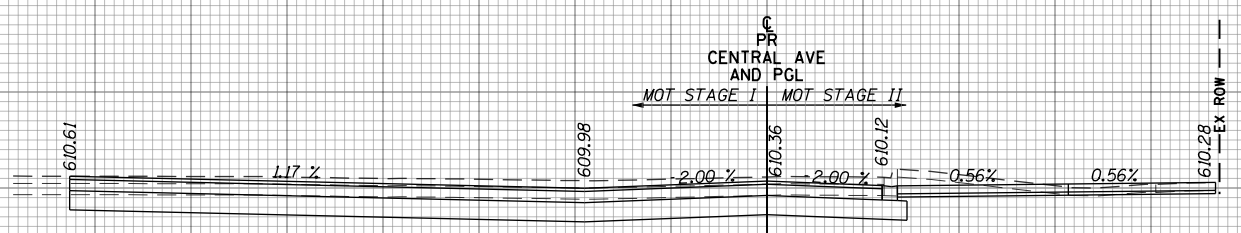
620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	19	34
FILL	0	0
EX.TOPSOIL	0	0

108+00.00

620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	75	28
FILL	0	0
EX.TOPSOIL	0	0

107+50.00

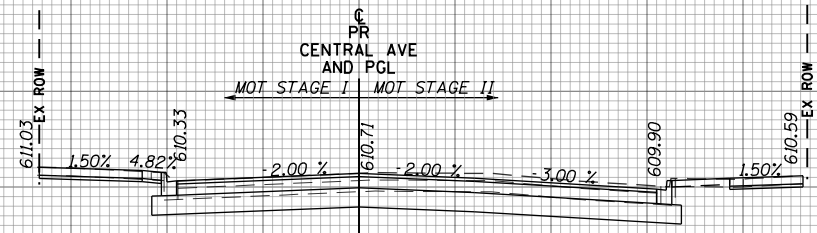
160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

DATE	
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SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
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DATE	
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SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

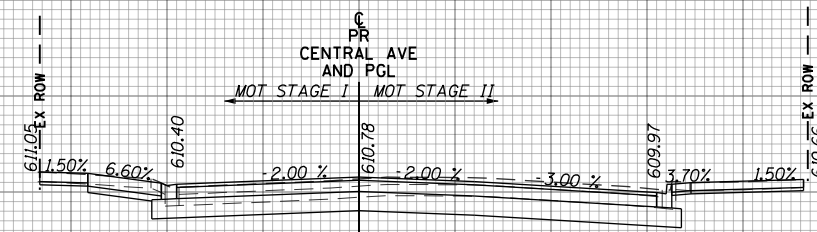
620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	19	34
FILL	0	0
EX.TOPSOIL	0	0

109+50.00

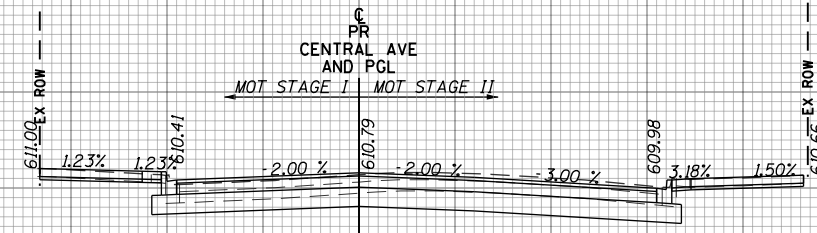
620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	19	34
FILL	0	0
EX.TOPSOIL	0	0

109+12.50

620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	15	34
FILL	0	0
EX.TOPSOIL	0	0

109+00.00

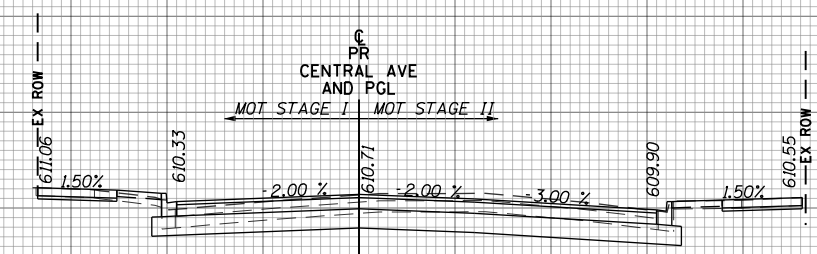
DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - MLB	REVISED -		1296	16-00199-00-RS	COOK	318	264			
		CHECKED - DWB	REVISED -		CONTRACT NO. 61G42							
		DATE 02/13/2020	REVISED -		ILLINOIS FED. AID PROJECT							
		PLOT SCALE = 20.0000' / in.			SCALE: 10H:5V	SHEET NO.264 OF 53 SHEETS	STA. 109+00.00 TO STA. 109+50.00					

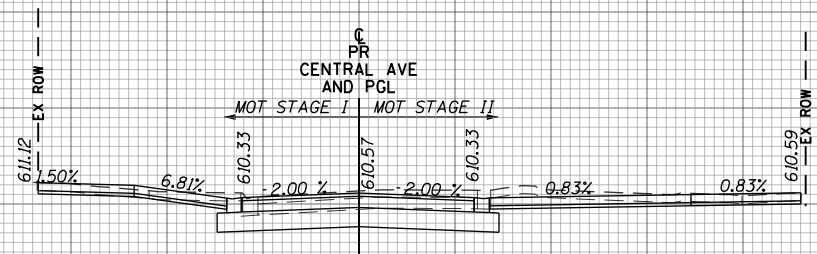
620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	14	33
FILL	1	0
EX.TOPSOIL	0	0

111+00.00

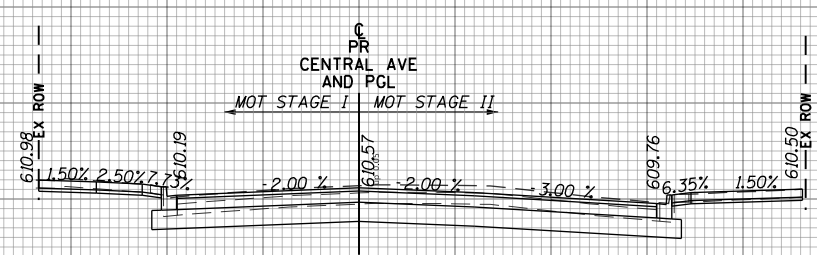
620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	16	26
FILL	0	0
EX.TOPSOIL	0	0

110+50.00

620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	21	36
FILL	0	0
EX.TOPSOIL	0	0

110+00.00

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
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DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
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FILE NAME =

USER NAME = Mibeering
 DESIGNED - MLB
 DRAWN - MLB
 CHECKED - DWB
 DATE 02/13/2020

REVISIED -
 REVISED -
 REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

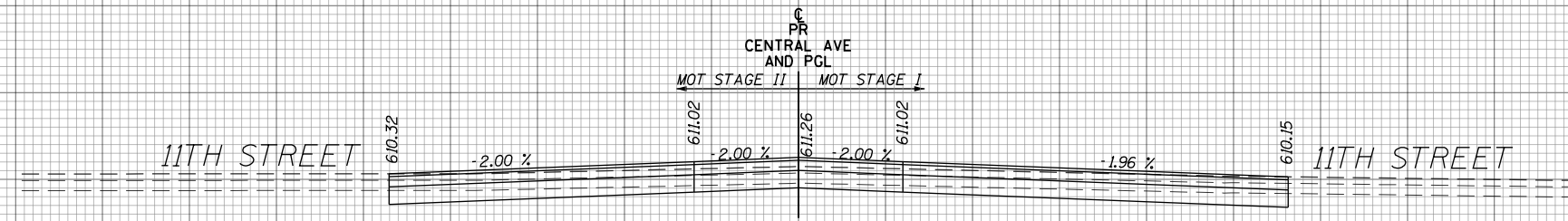
**CENTRAL AVENUE AND WILMETTE AVENUE
 CROSS SECTIONS
 CENTRAL AVENUE**

SCALE: 10H:5V SHEET NO.265 OF 53 SHEETS STA. 110+00.00 TO STA. 111+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	265
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

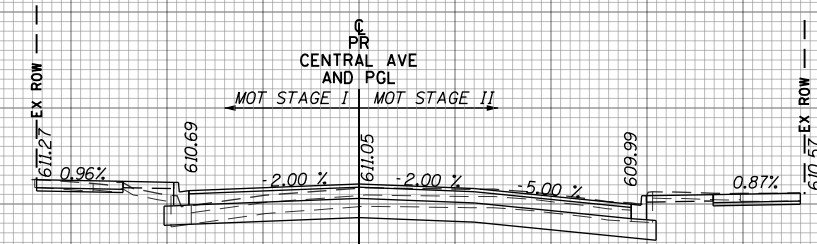
620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	29	23
FILL	0	1
EX.TOPSOIL	0	0

112+52.55

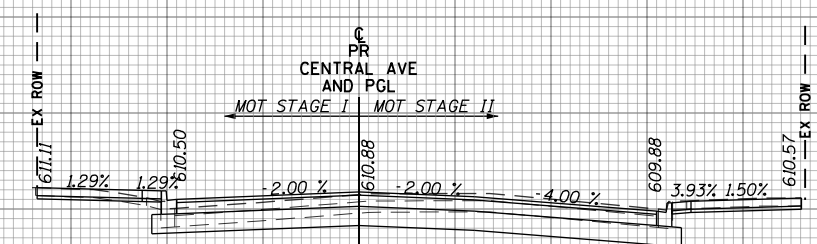
620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	8	27
FILL	3	0
EX.TOPSOIL	3	3

112+00.00

620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	12	33
FILL	0	0
EX.TOPSOIL	0	0

111+50.00

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

DATE	
BY	
SURVEYED	
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TEMPLATE	
AREAS	
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FINAL SURVEY	
NOTE BOOK	
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DATE	
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AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	

FILE NAME =

USER NAME = Mibeering
 DESIGNED - MLB
 DRAWN - MLB
 CHECKED - DWB
 DATE 02/13/2020

REVISIED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

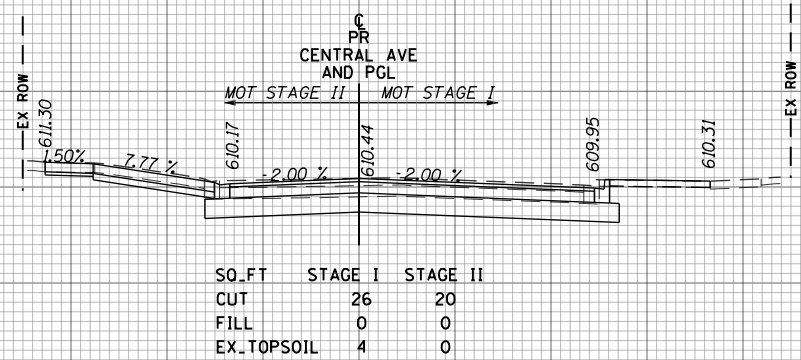
CENTRAL AVENUE AND WILMETTE AVENUE
 CROSS SECTIONS
 CENTRAL AVENUE

SCALE: 10H:5V SHEET NO266 OF 53 SHEETS STA. 111+50.00 TO STA. 112+52.55

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	266
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

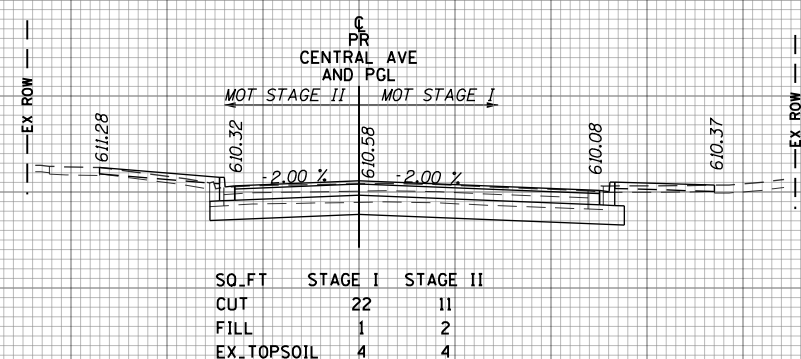
160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

620 615 610 605 600



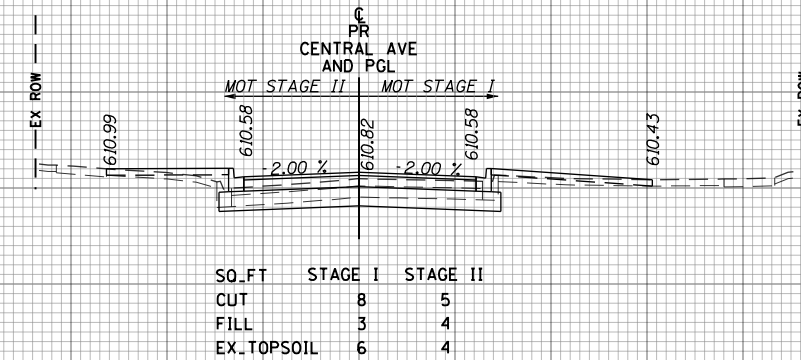
113+77.42

620 615 610 605 600



113+50.00

620 615 610 605 600



113+00.00

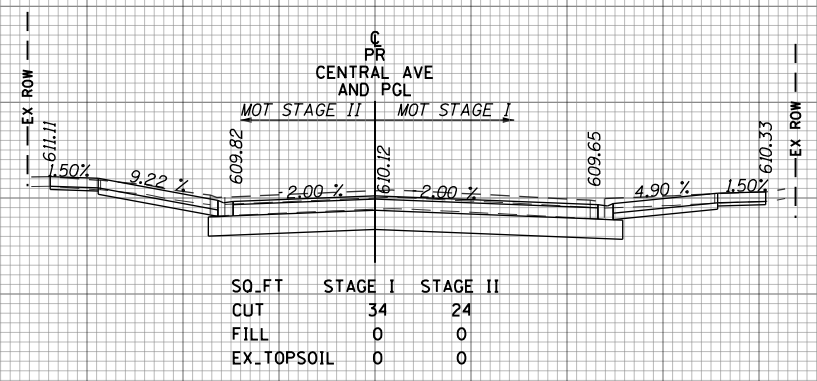
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DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
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FINAL SURVEY	
NOTE BOOK	
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DATE	
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SURVEYED	
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AREAS	
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ORIGINAL SURVEY	
NOTE BOOK	
NO.	

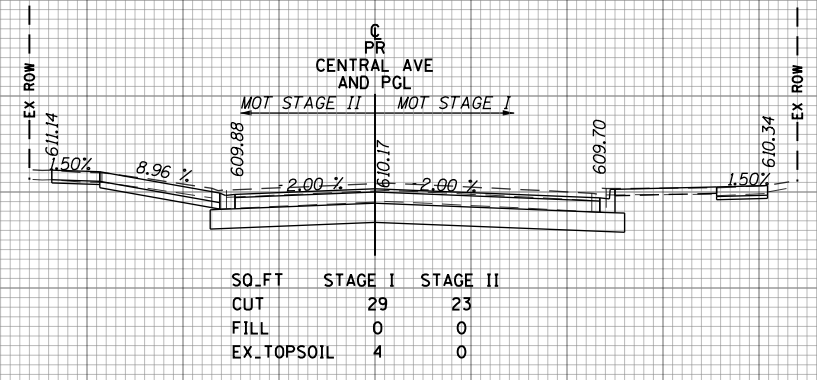
160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

620 615 610 605 600



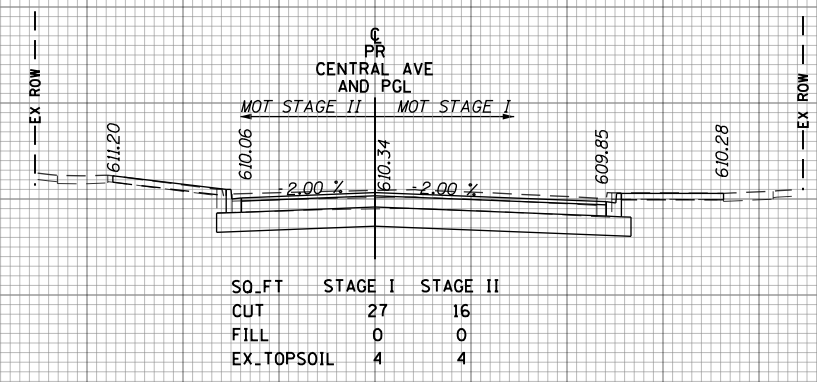
114+45.19

620 615 610 605 600



114+34.87

620 615 610 605 600



114+00.00

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
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DATE	
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SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	

FILE NAME =

USER NAME = Mibeering
DESIGNED - MLB
DRAWN - MLB
CHECKED - DWB
DATE 02/13/2020

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

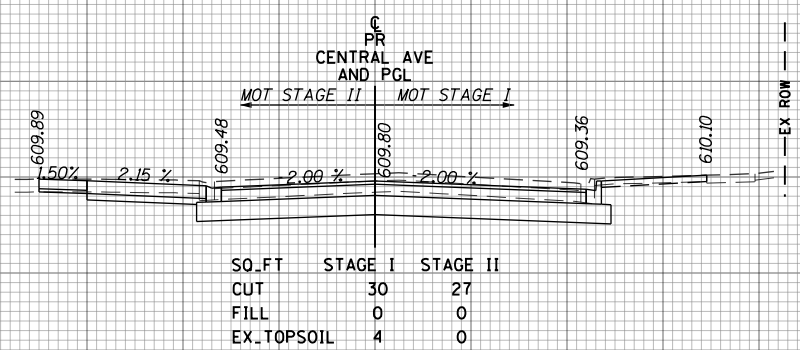
CENTRAL AVENUE AND WILMETTE AVENUE
CROSS SECTIONS
CENTRAL AVENUE

SCALE: 10H:5V SHEET NO.268 OF 53 SHEETS STA. 114+00.00 TO STA. 114+45.19

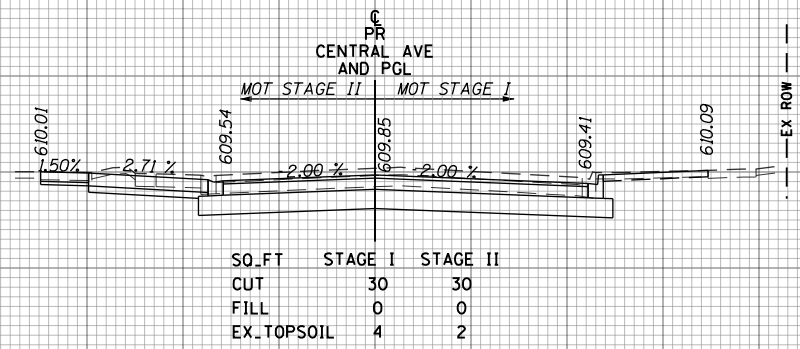
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	268
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

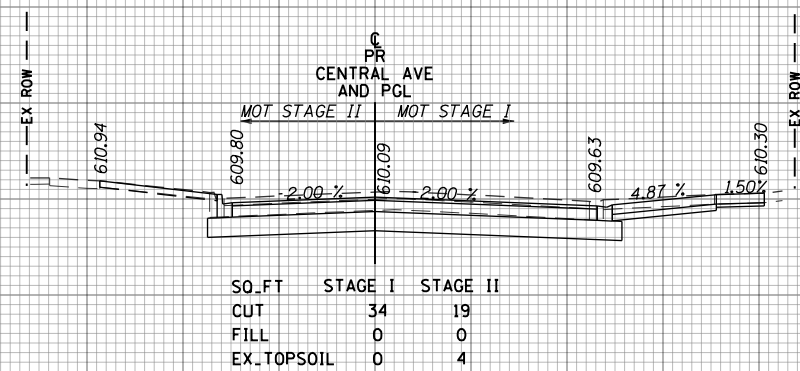
620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600



115+10.07



115+00.00



114+50.00

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
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DATE	
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SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

FILE NAME =

USER NAME = Mibeering	DESIGNED - MLB	REVISED -
	DRAWN - MLB	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED - DWB	REVISED -
PLOT DATE = 2/14/2020	DATE 02/13/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

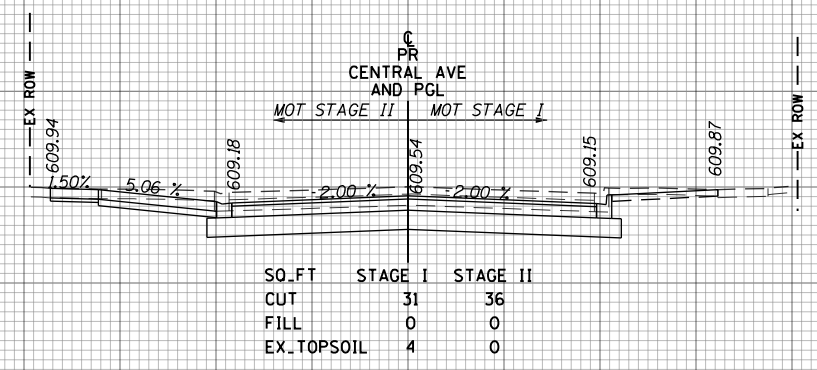
CENTRAL AVENUE AND WILMETTE AVENUE
CROSS SECTIONS
CENTRAL AVENUE

SCALE: 10H:5V SHEET NO.269 OF 53 SHEETS STA. 114+50.00 TO STA. 115+10.07

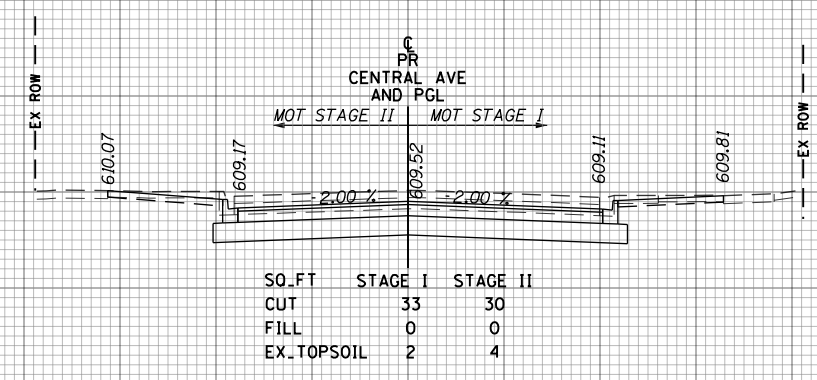
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	269
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

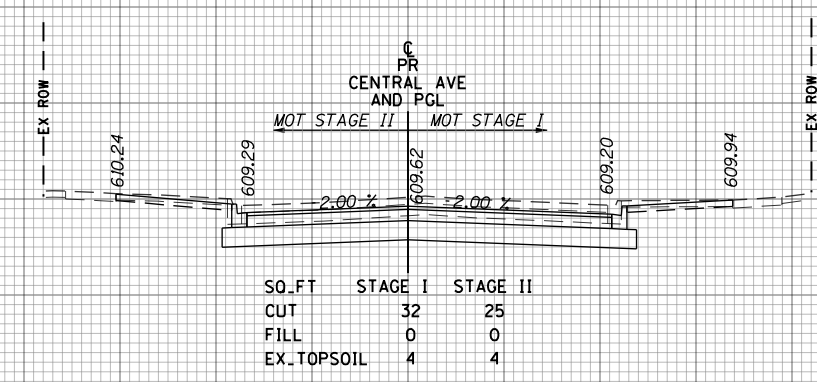
620 615 610 605 600



116+33.25



116+00.00



115+50.00

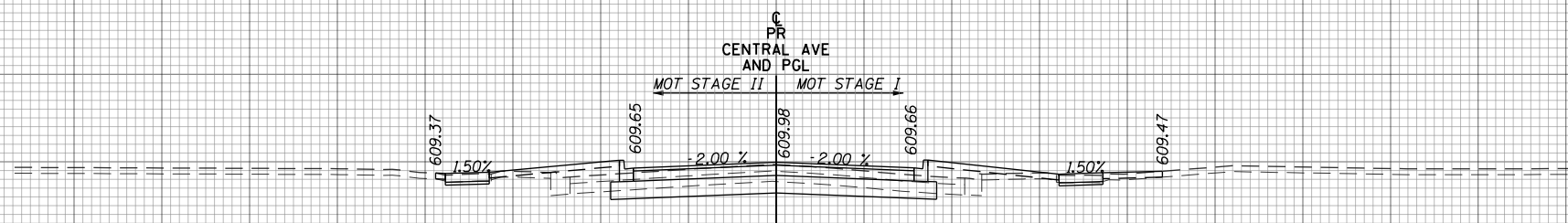
160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

DATE	
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SURVEYED	
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DATE	
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TEMPLATE	
AREAS	
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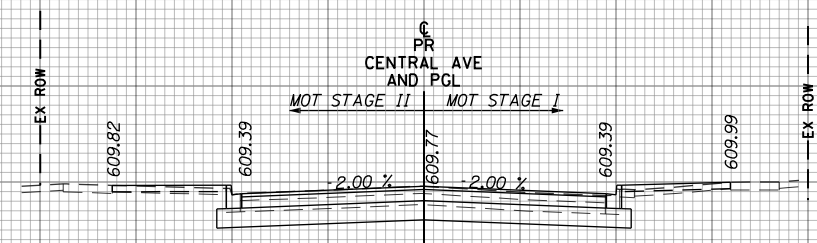
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620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600



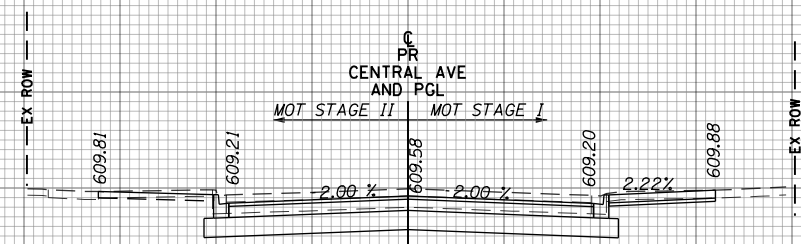
SO.FT	STAGE I	STAGE II
CUT	10	12
FILL	11	11
EX.TOPSOIL	5	3

117+50.00



SO.FT	STAGE I	STAGE II
CUT	16	17
FILL	2	1
EX.TOPSOIL	3	4

117+00.00



SO.FT	STAGE I	STAGE II
CUT	29	26
FILL	0	0
EX.TOPSOIL	0	4

116+50.00

DATE	
BY	
SURVEYED	
PLOTTED	
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FINAL SURVEY	
NOTE BOOK	
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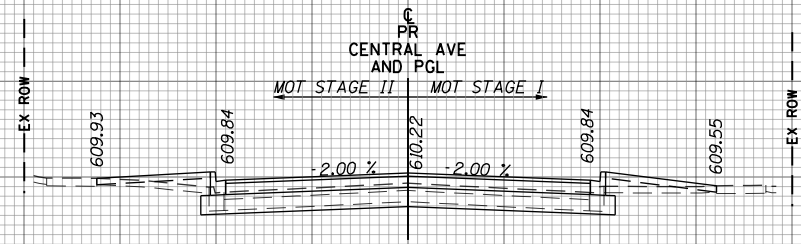
DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 20.0000' / in.	DRAWN - MLB	REVISED -		1296	16-00199-00-RS	COOK	318	271			
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -		CONTRACT NO. 61G42							
		DATE 02/13/2020	REVISED -	SCALE: 10H:5V			SHEET NO.271 OF 53 SHEETS			STA. 116+50.00 TO STA. 117+50.00		
ILLINOIS FED. AID PROJECT												

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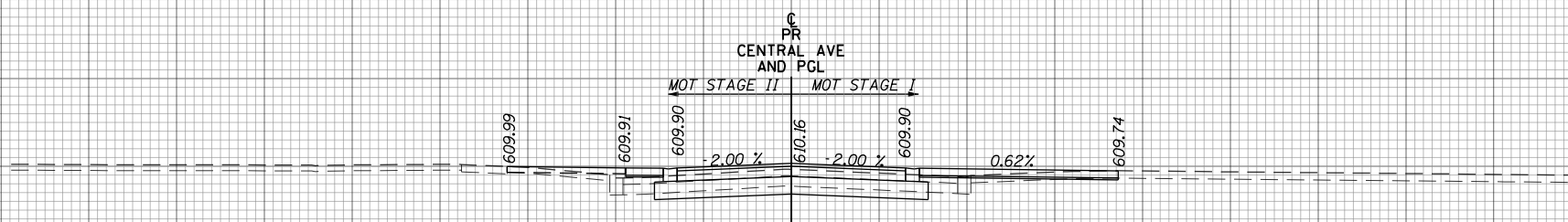
620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	5	5
FILL	5	4
EX.TOPSOIL	4	4

118+50.00

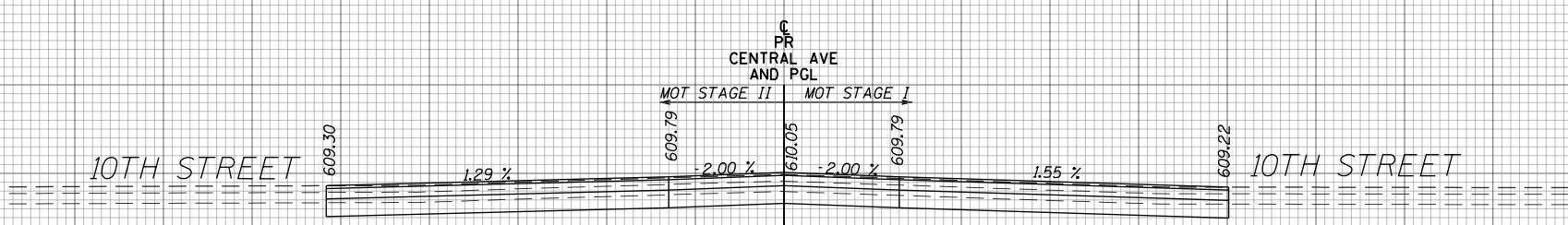
620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	7	6
FILL	6	8
EX.TOPSOIL	0	4

118+00.00

620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	35	37
FILL	0	0
EX.TOPSOIL	0	0

117+69.46

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

DATE	BY
SURVEYED	PLOTTED
TEMPLATE	AREAS CHECKED
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DATE	BY
SURVEYED	PLOTTED
TEMPLATE	AREAS CHECKED
NO.	

FILE NAME =

USER NAME = Mibeering

DESIGNED - MLB
DRAWN - MLB
CHECKED - DWB
DATE 02/13/2020

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REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

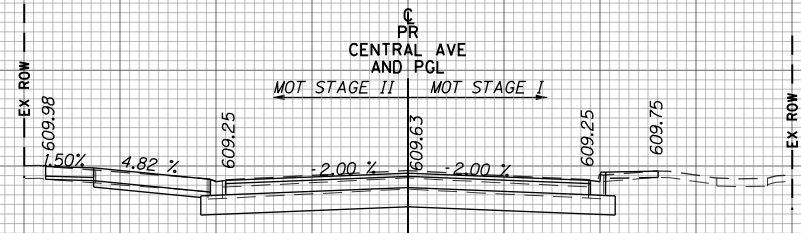
CENTRAL AVENUE AND WILMETTE AVENUE
CROSS SECTIONS
CENTRAL AVENUE

SCALE: 10H:5V SHEET NO.272 OF 53 SHEETS STA. 117+69.46 TO STA. 118+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	272
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

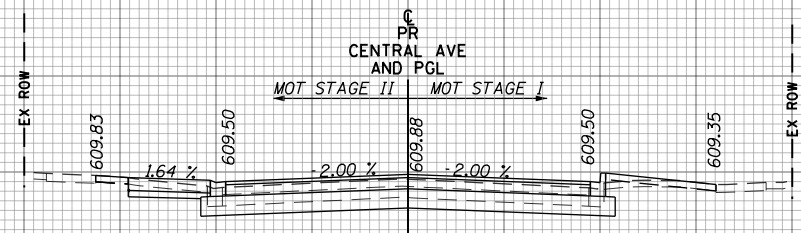
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620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600



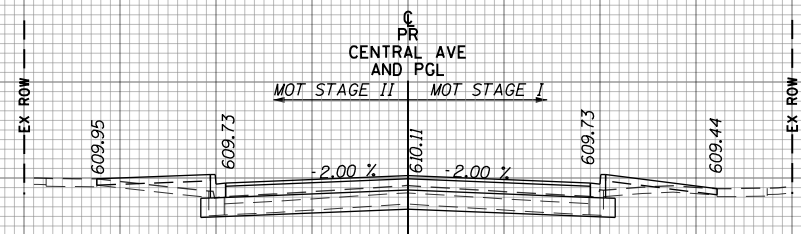
SO.FT	STAGE I	STAGE II
CUT	19	22
FILL	0	0
EX.TOPSOIL	2	0

120+00.00



SO.FT	STAGE I	STAGE II
CUT	11	12
FILL	1	0
EX.TOPSOIL	4	4

119+50.00



SO.FT	STAGE I	STAGE II
CUT	5	4
FILL	4	5
EX.TOPSOIL	4	4

119+00.00

DATE	BY
SURVEYED	PLOTTED
TEMPLATE	AREAS CHECKED
NOTE BOOK	AREAS CHECKED
NO.	

DATE	BY
SURVEYED	PLOTTED
TEMPLATE	AREAS CHECKED
NOTE BOOK	AREAS CHECKED
NO.	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

FILE NAME =

USER NAME = Mibeering

DESIGNED - MLB
DRAWN - MLB
CHECKED - DWB
DATE 02/13/2020

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE AND WILMETTE AVENUE
CROSS SECTIONS
CENTRAL AVENUE**

SCALE: 10H:5V

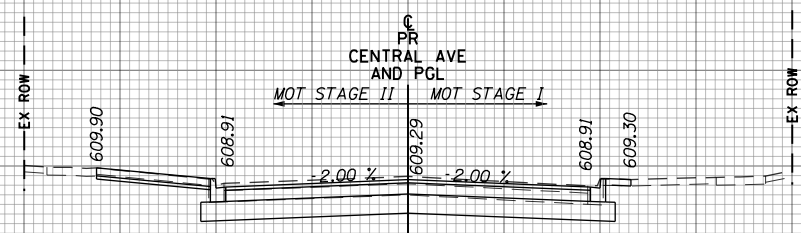
SHEET NO.273 OF 53 SHEETS

STA. 119+00.00 TO STA. 120+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	273
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

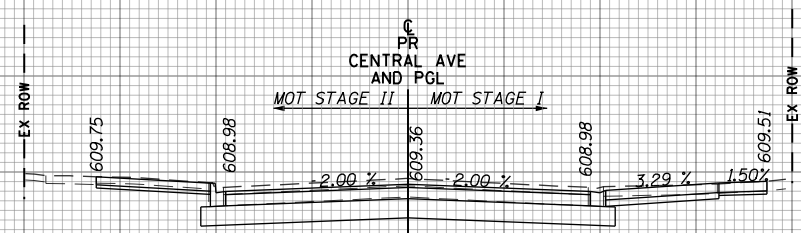
160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600



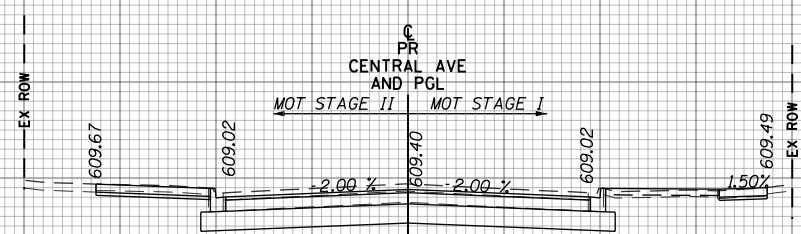
SO.FT	STAGE I	STAGE II
CUT	20	24
FILL	0	0
EX.TOPSOIL	1	4

121+00.00



SO.FT	STAGE I	STAGE II
CUT	28	26
FILL	0	0
EX.TOPSOIL	0	4

120+59.80



SO.FT	STAGE I	STAGE II
CUT	23	26
FILL	1	0
EX.TOPSOIL	4	4

120+50.00

DATE	BY
SURVEYED	PLOTTED
TEMPLATE	AREAS CHECKED
NOTE BOOK	AREAS CHECKED
NO.	

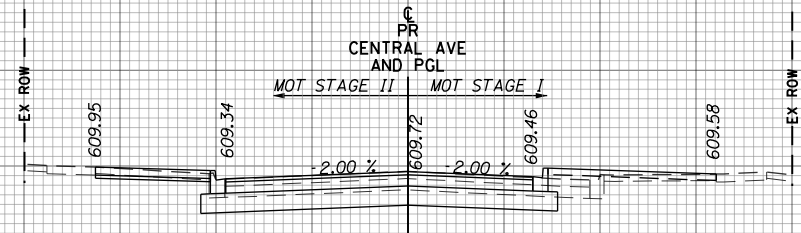
DATE	BY
SURVEYED	PLOTTED
TEMPLATE	AREAS CHECKED
NOTE BOOK	AREAS CHECKED
NO.	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE	F.A.U. RTE. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 274	
	PLOT SCALE = 20.0000' / in.	CHECKED - DWB	REVISED -		SCALE: 10H:5V	SHEET NO274 OF 53 SHEETS	STA. 120+50.00	TO STA. 121+00.00	ILLINOIS FED. AID PROJECT		
	PLOT DATE = 2/14/2020	DATE 02/13/2020	REVISED -								

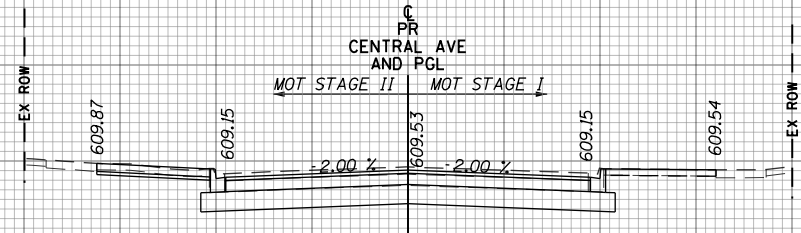
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620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600



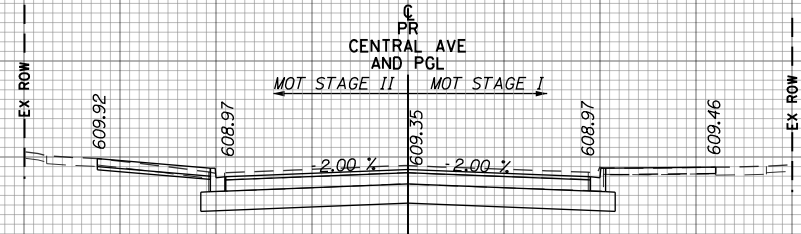
SO.FT	STAGE I	STAGE II
CUT	12	19
FILL	8	0
EX.TOPSOIL	4	4

122+50.00



SO.FT	STAGE I	STAGE II
CUT	21	24
FILL	0	0
EX.TOPSOIL	4	4

122+00.00



SO.FT	STAGE I	STAGE II
CUT	22	23
FILL	0	0
EX.TOPSOIL	4	4

121+50.00

DATE	BY
SURVEYED	PLOTTED
TEMPLATE	AREAS CHECKED
NOTE BOOK	AREAS CHECKED
NO.	

DATE	BY
SURVEYED	PLOTTED
TEMPLATE	AREAS CHECKED
NOTE BOOK	AREAS CHECKED
NO.	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

FILE NAME =
USER NAME = Mibeering
DESIGNED - MLB
DRAWN - MLB
PLOT SCALE = 20.0000' / in.
PLOT DATE = 2/14/2020

REVISIED -
REVISIED -
REVISIED -
REVISIED -
DATE 02/13/2020

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

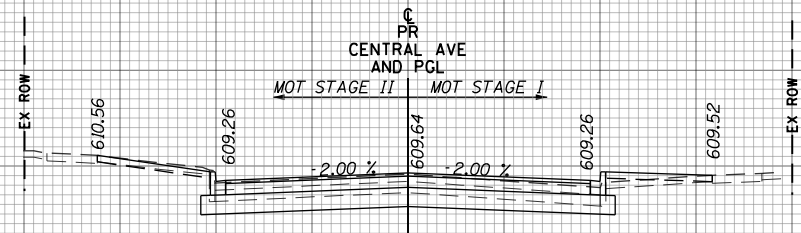
CENTRAL AVENUE AND WILMETTE AVENUE
CROSS SECTIONS
CENTRAL AVENUE

SCALE: 10H:5V SHEET NO.275 OF 53 SHEETS STA. 121+50.00 TO STA. 122+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	275
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

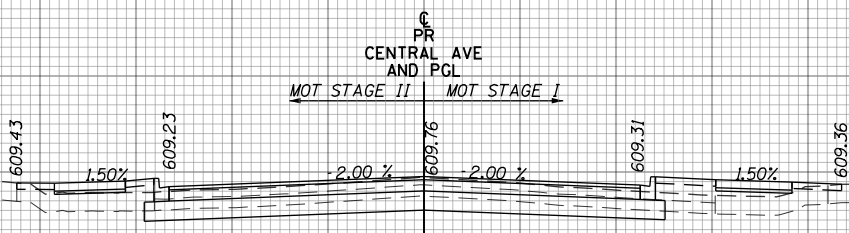
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620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600



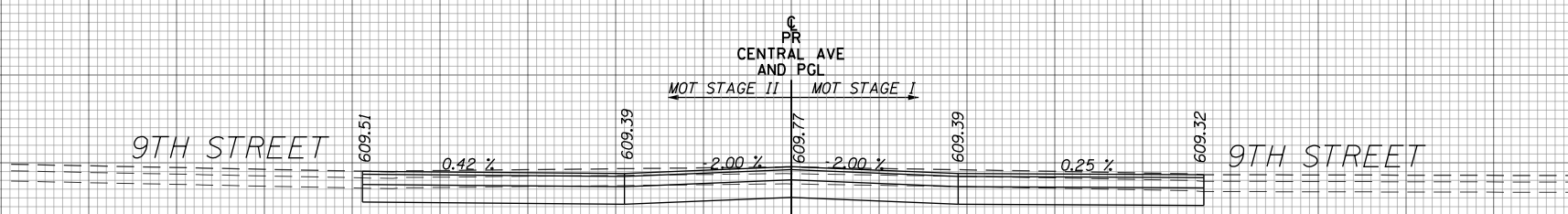
SO.FT	STAGE I	STAGE II
CUT	13	16
FILL	3	0
EX.TOPSOIL	4	4

123+50.00



SO.FT	STAGE I	STAGE II
CUT	15	17
FILL	22	14
EX.TOPSOIL	0	0

123+00.00



SO.FT	STAGE I	STAGE II
CUT	42	45
FILL	1	0
EX.TOPSOIL	0	0

122+86.21

DATE	BY
SURVEYED	PLOTTED
TEMPLATE	AREAS CHECKED
NOTE BOOK	AREAS CHECKED
NO.	

DATE	BY
SURVEYED	PLOTTED
TEMPLATE	AREAS CHECKED
NOTE BOOK	AREAS CHECKED
NO.	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

FILE NAME =
 USER NAME = Mibeering
 PLOT SCALE = 20.0000' / in.
 PLOT DATE = 2/14/2020

DESIGNED - MLB
 DRAWN - MLB
 CHECKED - DWB
 DATE 02/13/2020

REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

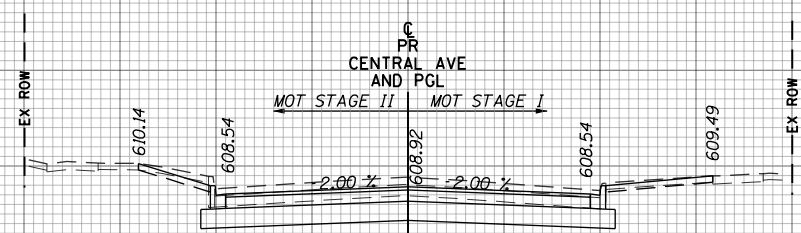
CENTRAL AVENUE AND WILMETTE AVENUE
 CROSS SECTIONS
 CENTRAL AVENUE

SCALE: 10H:5V SHEET NO276 OF 53 SHEETS STA. 122+86.21 TO STA. 123+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	276
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

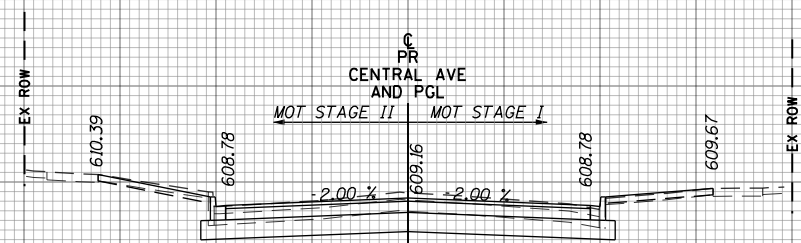
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620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600



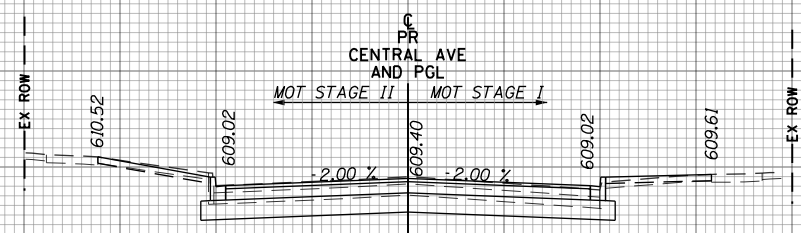
SO.FT	STAGE I	STAGE II
CUT	26	28
FILL	0	0
EX.TOPSOIL	4	2

125+00.00



SO.FT	STAGE I	STAGE II
CUT	20	20
FILL	1	0
EX.TOPSOIL	4	4

124+50.00



SO.FT	STAGE I	STAGE II
CUT	16	19
FILL	1	0
EX.TOPSOIL	4	4

124+00.00

DATE	BY

DATE	BY

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

FILE NAME =

USER NAME = Mibeering

DESIGNED - MLB
DRAWN - MLB
CHECKED - DWB
DATE 02/13/2020

REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

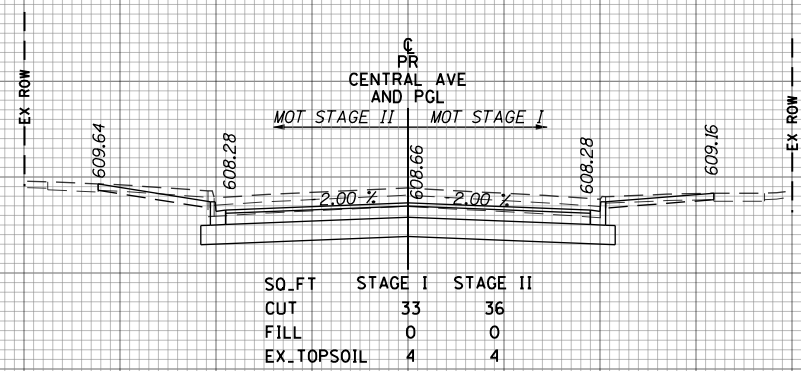
**CENTRAL AVENUE AND WILMETTE AVENUE
CROSS SECTIONS
CENTRAL AVENUE**

SCALE: 10H:5V SHEET NO.277 OF 53 SHEETS STA. 124+00.00 TO STA. 125+00.00

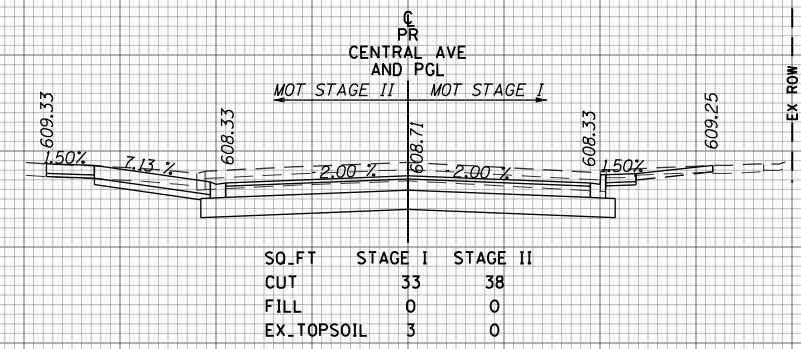
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	277
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

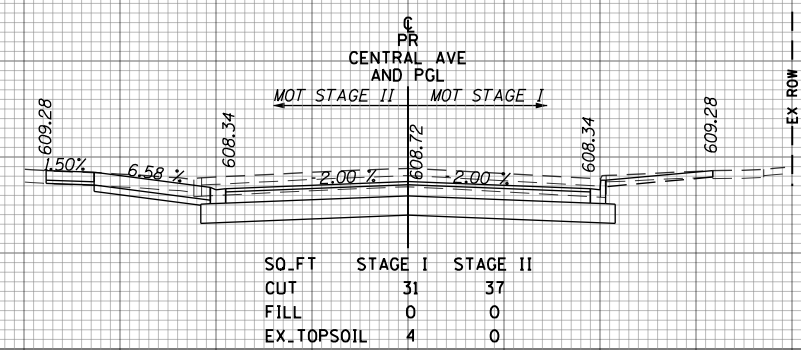
620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600



126+00.00



125+50.00



125+44.66

DATE	
BY	
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PLOTTED	
TEMPLATE	
AREAS	
CHECKED	

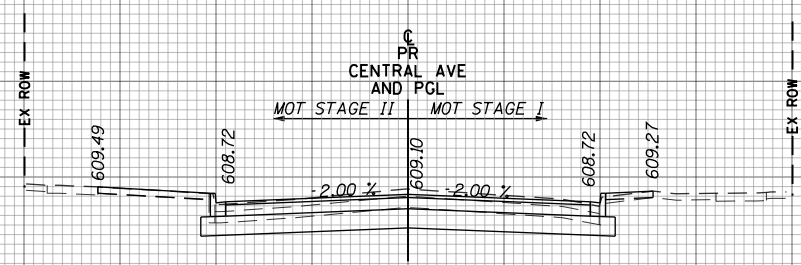
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BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

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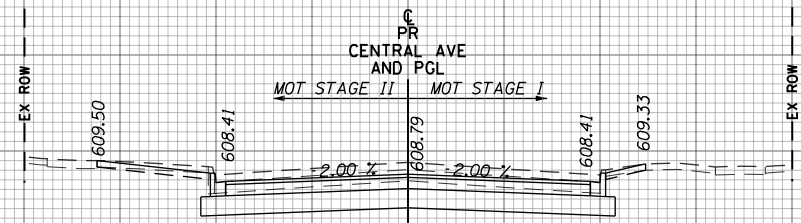
615
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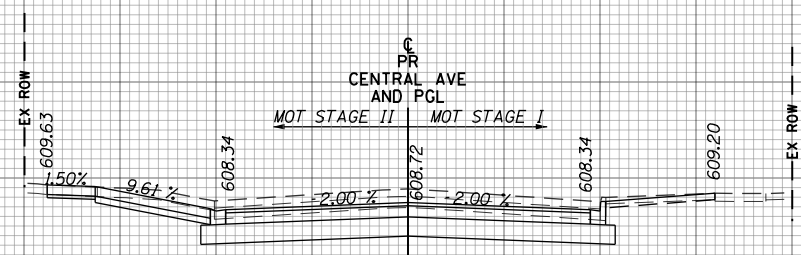
SO.FT	STAGE I	STAGE II
CUT	20	18
FILL	1	0
EX.TOPSOIL	2	4

127+00.00



SO.FT	STAGE I	STAGE II
CUT	27	29
FILL	0	0
EX.TOPSOIL	1	4

126+50.00



SO.FT	STAGE I	STAGE II
CUT	30	39
FILL	0	0
EX.TOPSOIL	4	0

126+30.32

DATE	BY	SURVEYED	PLOTTED	TEMPLATE	AREAS	CHECKED
FINAL SURVEY NO.		NOTE BOOK				

DATE	BY	SURVEYED	PLOTTED	TEMPLATE	AREAS	CHECKED
ORIGINAL SURVEY NO.		NOTE BOOK				

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

FILE NAME =

USER NAME = Mibeering

DESIGNED - MLB
DRAWN - MLB
CHECKED - DWB
DATE 02/13/2020

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CENTRAL AVENUE AND WILMETTE AVENUE
CROSS SECTIONS
CENTRAL AVENUE**

SCALE: 10H:5V

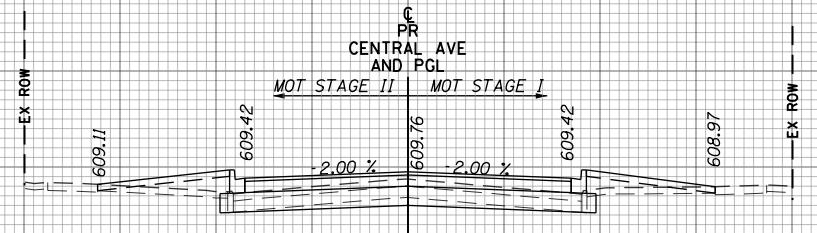
SHEET NO.279 OF 53 SHEETS

STA. 126+30.32 TO STA. 127+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	279
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

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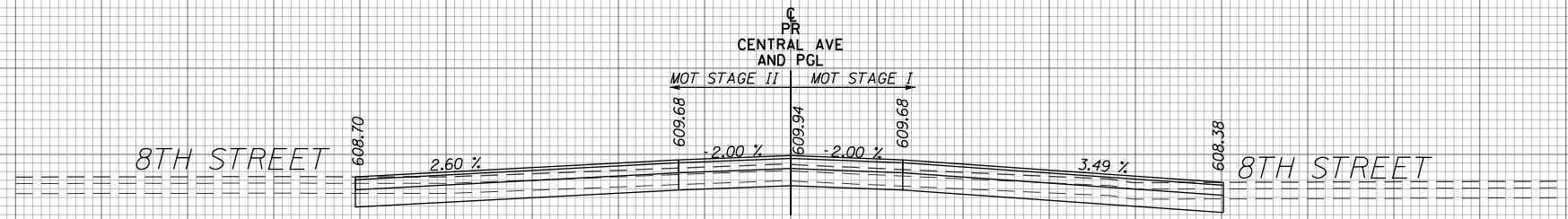
620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	4	5
FILL	6	8
EX.TOPSOIL	4	5

128+50.00

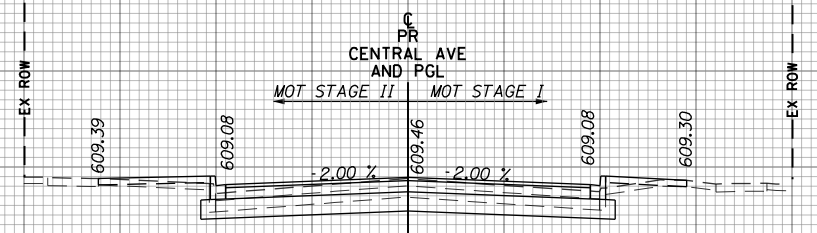
615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	20	22
FILL	0	0
EX.TOPSOIL	0	0

128+03.05

620 615 610 605 600



SO.FT	STAGE I	STAGE II
CUT	13	12
FILL	2	2
EX.TOPSOIL	3	4

127+50.00

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
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DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

FILE NAME =

USER NAME = Mibeering

DESIGNED - MLB
DRAWN - MLB
CHECKED - DWB
DATE 02/13/2020

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

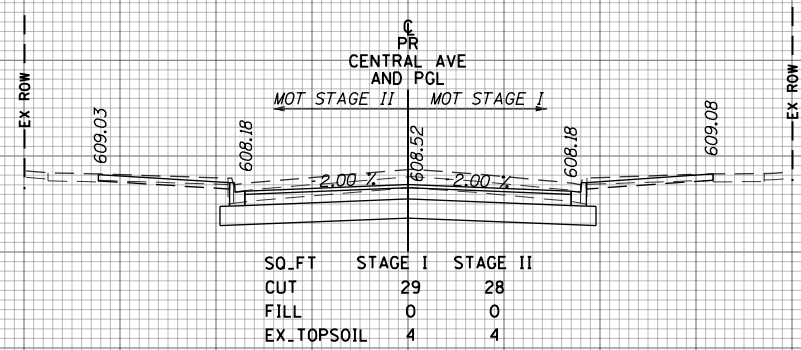
**CENTRAL AVENUE AND WILMETTE AVENUE
CROSS SECTIONS
CENTRAL AVENUE**

SCALE: 10H:5V SHEET NO280 OF 53 SHEETS STA. 127+50.00 TO STA. 128+50.00

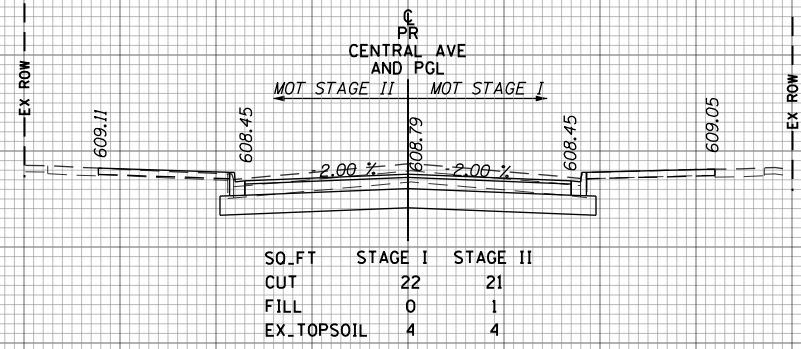
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	280
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

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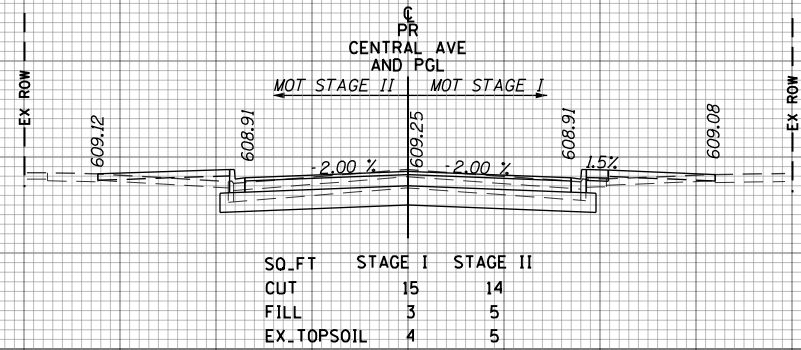
620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600



130+00.00



129+50.00



129+00.00

DATE	BY
SURVEYED	PLOTTED
TEMPLATE	AREAS CHECKED
NOTE BOOK	AREAS CHECKED
NO.	

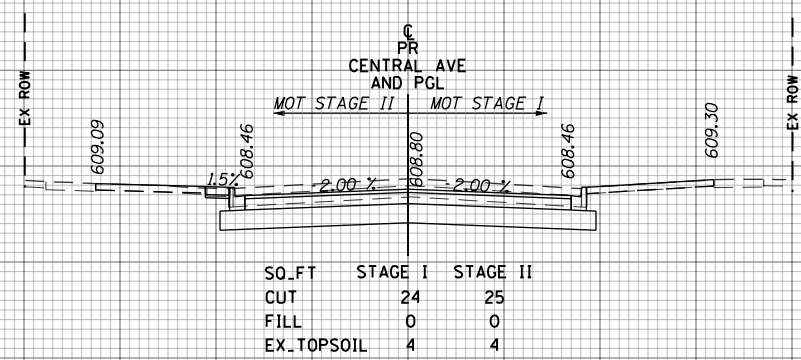
DATE	BY
SURVEYED	PLOTTED
TEMPLATE	AREAS CHECKED
NOTE BOOK	AREAS CHECKED
NO.	

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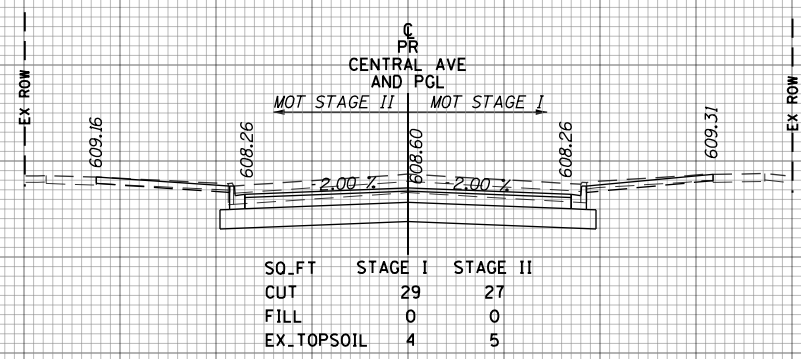
FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE	F.A.U. RTE. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 281	
	PLOT SCALE = 20.0000' / in.	CHECKED - DWB	REVISED -		SCALE: 10H:5V	SHEET NO.281 OF 53 SHEETS	STA. 129+00.00 TO STA. 130+00.00		CONTRACT NO. 61G42		
	PLOT DATE = 2/14/2020	DATE 02/13/2020	REVISED -		ILLINOIS FED. AID PROJECT						

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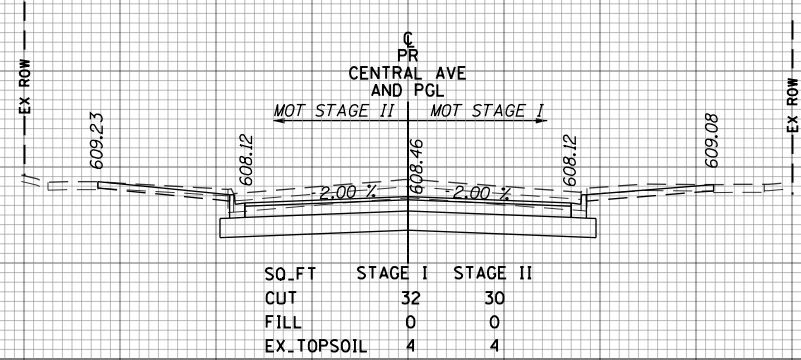
620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600



131+50.00



131+00.00



130+50.00

DATE	BY
SURVEYED	PLOTTED
TEMPLATE	AREAS CHECKED
NOTE BOOK	AREAS CHECKED
NO.	

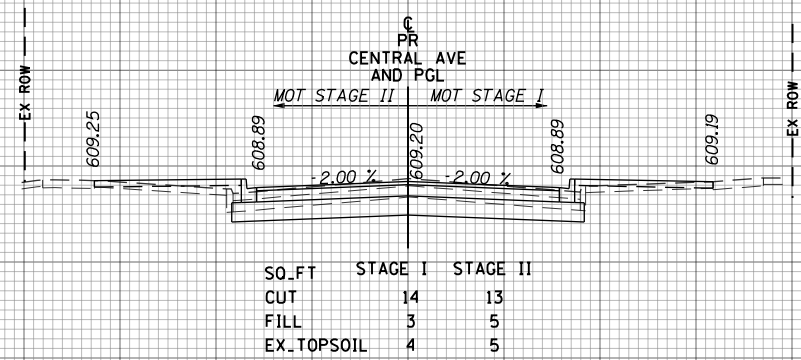
DATE	BY
SURVEYED	PLOTTED
TEMPLATE	AREAS CHECKED
NOTE BOOK	AREAS CHECKED
NO.	

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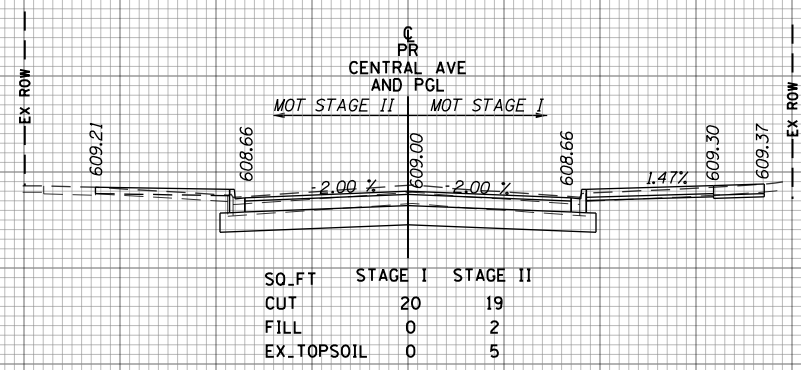
FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE		F.A.U. RTE. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 282
	PLOT SCALE = 20.0000' / in.	CHECKED - DWB	REVISED -		SCALE: 10H:5V	SHEET NO. 282 OF 53 SHEETS	STA. 130+50.00 TO STA. 131+50.00		CONTRACT NO. 61G42		
	PLOT DATE = 2/14/2020	DATE 02/13/2020	REVISED -		ILLINOIS FED. AID PROJECT						

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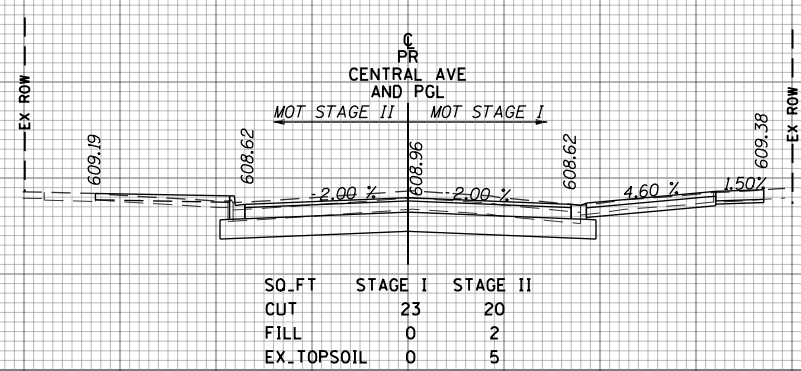
620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600



132+50.00



132+00.00



131+90.50

DATE	
BY	
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FINAL SURVEY	
NOTE BOOK	
NO.	

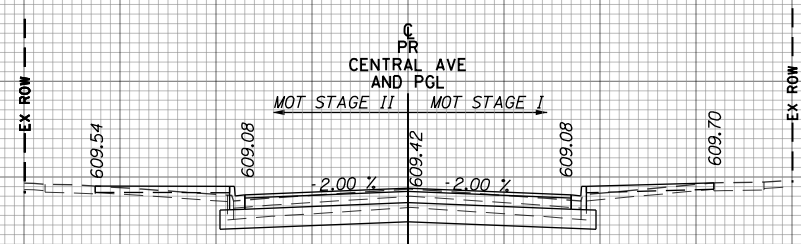
DATE	
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TEMPLATE	
AREAS	
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ORIGINAL SURVEY	
NOTE BOOK	
NO.	

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FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - MLB	REVISED -		1296	16-00199-00-RS	COOK	318	283			
		CHECKED - DWB	REVISED -		CONTRACT NO. 61G42							
		DATE 02/13/2020	REVISED -		ILLINOIS FED. AID PROJECT							
		PLOT SCALE = 20.0000' / in.			SCALE: 10H:5V	SHEET NO.283 OF 53 SHEETS	STA. 131+90.50 TO STA. 132+50.00					

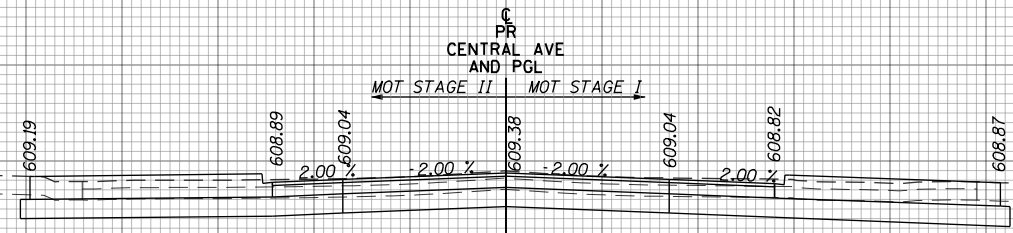
160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600



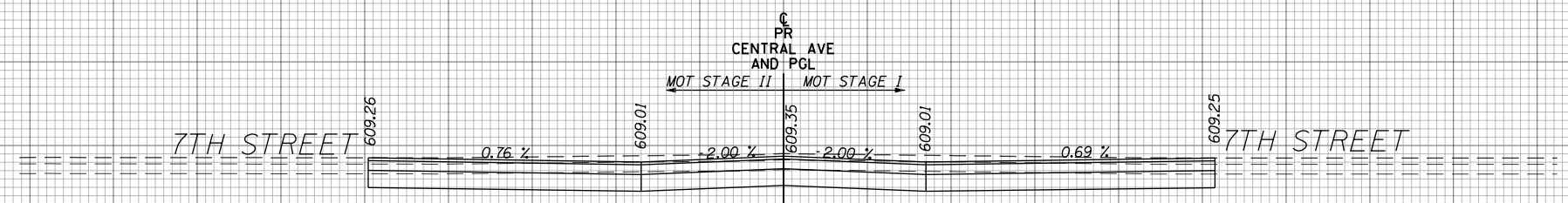
SO.FT	STAGE I	STAGE II
CUT	13	12
FILL	3	3
EX.TOPSOIL	4	5

133+50.00



SO.FT	STAGE I	STAGE II
CUT	52	46
FILL	0	0
EX.TOPSOIL	0	0

133+00.00



SO.FT	STAGE I	STAGE II
CUT	52	49
FILL	1	0
EX.TOPSOIL	0	0

132+89.18

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
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ORIGINAL SURVEY	
NOTE BOOK	
NO.	

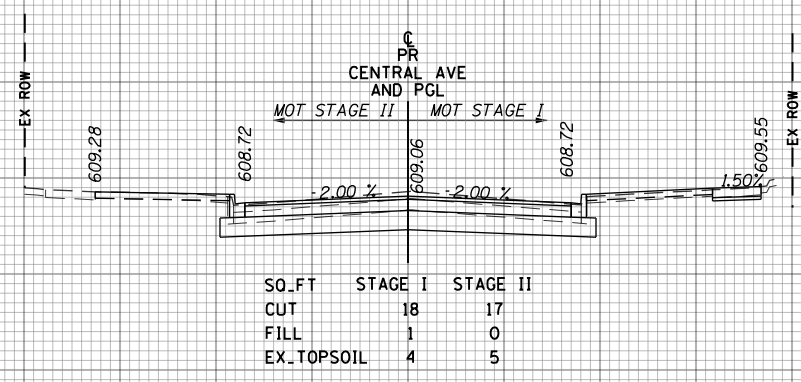
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FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 20.0000' / in.	DRAWN - MLB	REVISED -		1296	16-00199-00-RS	COOK	318	284			
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -		CONTRACT NO. 61G42				ILLINOIS FED. AID PROJECT			
		DATE 02/13/2020	REVISED -	SCALE: 10H:5V		SHEET NO284 OF 53 SHEETS		STA. 132+89.18 TO STA. 133+50.00				

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

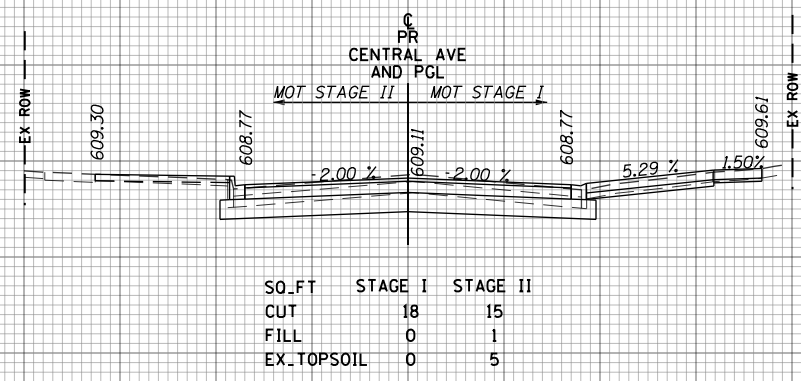
620 615 610 605 600

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FINAL SURVEY	
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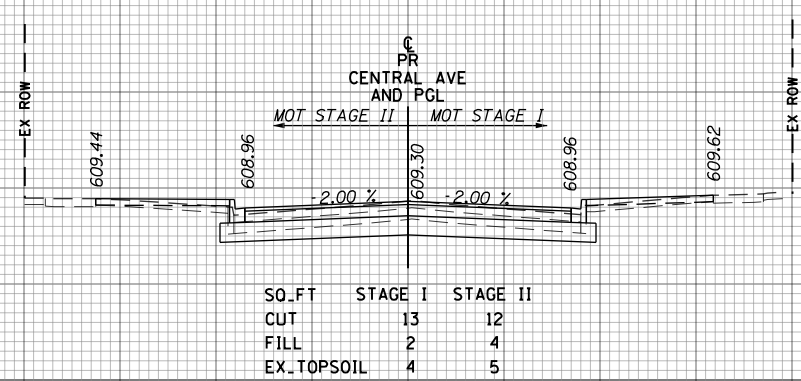
134+50.00

615 610 605 600



134+40.14

620 615 610 605 600



134+00.00

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

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FILE NAME =

USER NAME = Mibeering
DESIGNED - MLB
DRAWN - MLB
CHECKED - DWB
DATE 02/13/2020

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

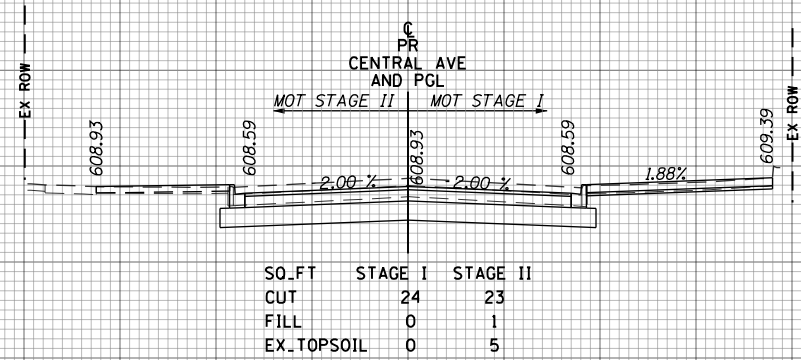
**CENTRAL AVENUE AND WILMETTE AVENUE
CROSS SECTIONS
CENTRAL AVENUE**

SCALE: 10H:5V SHEET NO.285 OF 53 SHEETS STA. 134+00.00 TO STA. 134+50.00

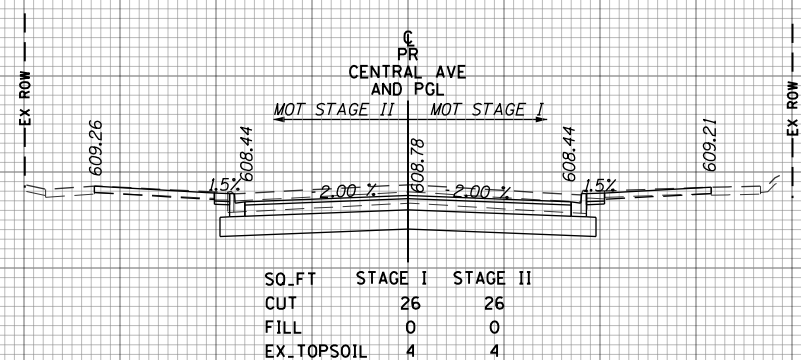
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	285
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

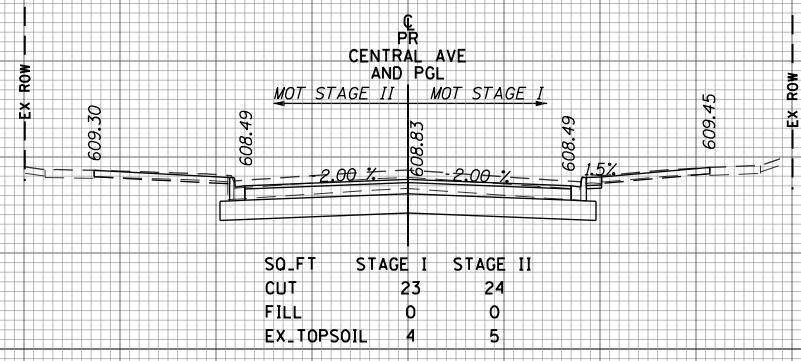
620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600



136+00.00



135+50.00



135+00.00

DATE	BY	SURVEYED	PLOTTED	TEMPLATE	AREAS CHECKED
FINAL SURVEY NO.		NOTE BOOK			

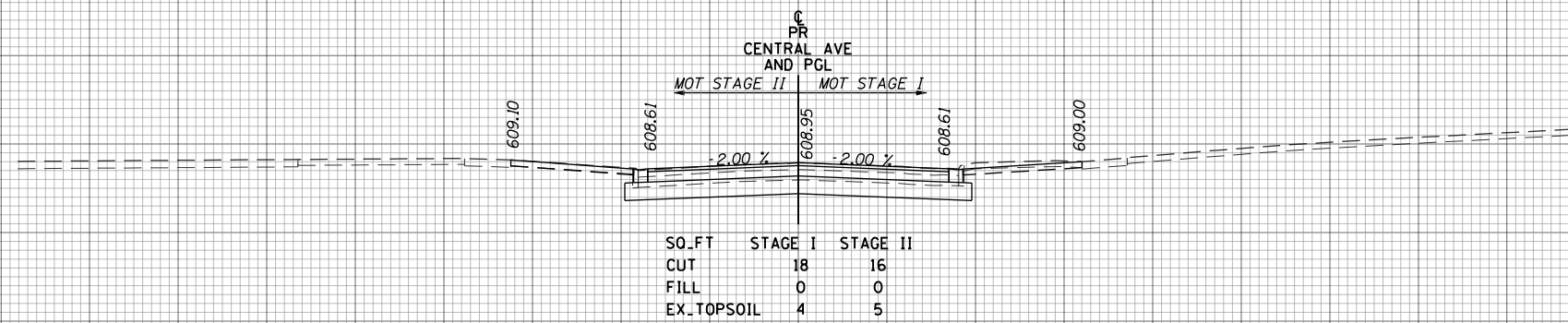
DATE	BY	SURVEYED	PLOTTED	TEMPLATE	AREAS CHECKED
ORIGINAL SURVEY NO.		NOTE BOOK			

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

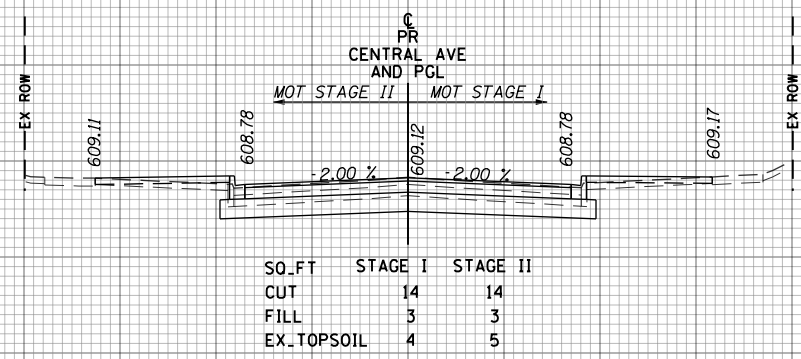
FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISIED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 20.0000' / in.	DRAWN - MLB	REVISIED -		1296	16-00199-00-RS	COOK	318	286			
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISIED -		SCALE: 10H:5V SHEET NO.286 OF 53 SHEETS STA. 135+00.00 TO STA. 136+00.00			CONTRACT NO. 61G42		ILLINOIS FED. AID PROJECT		
		DATE 02/13/2020	REVISIED -									

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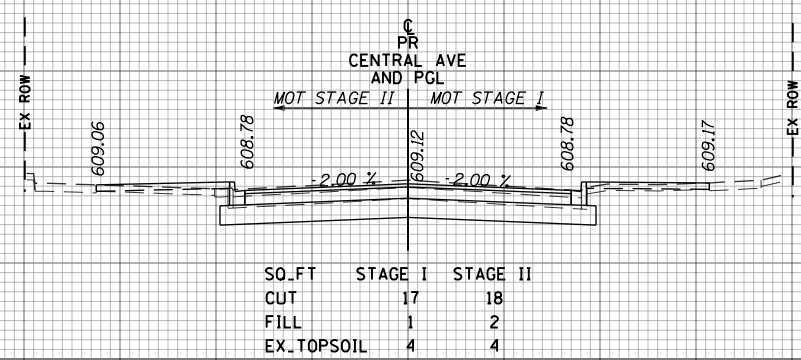
620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600 620 615 610 605 600



137+50.00



137+00.00



136+50.00

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FINAL SURVEY	
NOTE BOOK	
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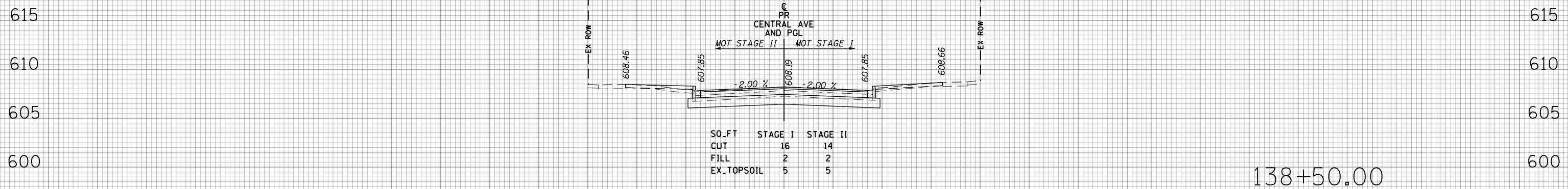
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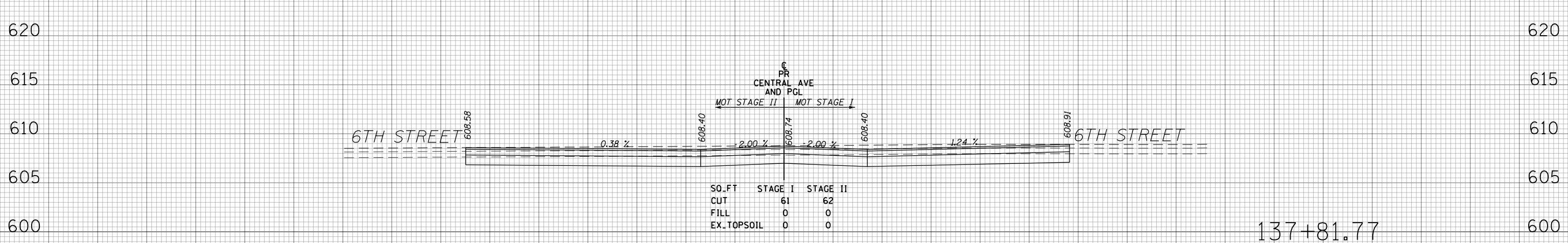
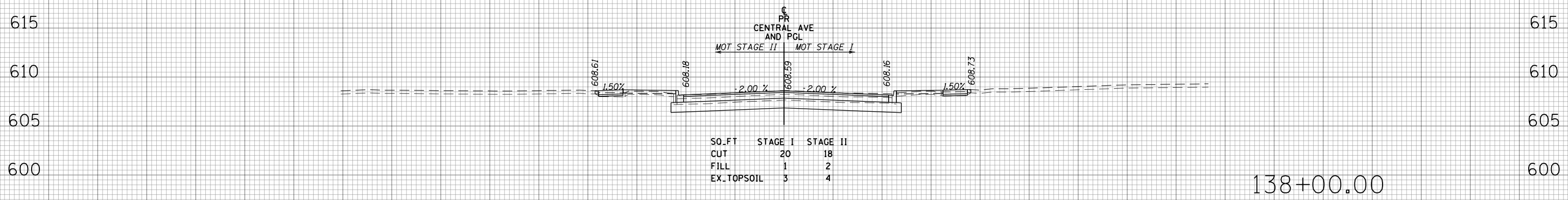
FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE	F.A.U. RTE. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 287	
	PLOT SCALE = 20.0000' / in.	CHECKED - DWB	REVISED -		SCALE: 10H:5V	SHEET NO.287 OF 53 SHEETS	STA. 136+50.00 TO STA. 137+50.00		CONTRACT NO. 61G42		
	PLOT DATE = 2/14/2020	DATE 02/13/2020	REVISED -		ILLINOIS FED. AID PROJECT						

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

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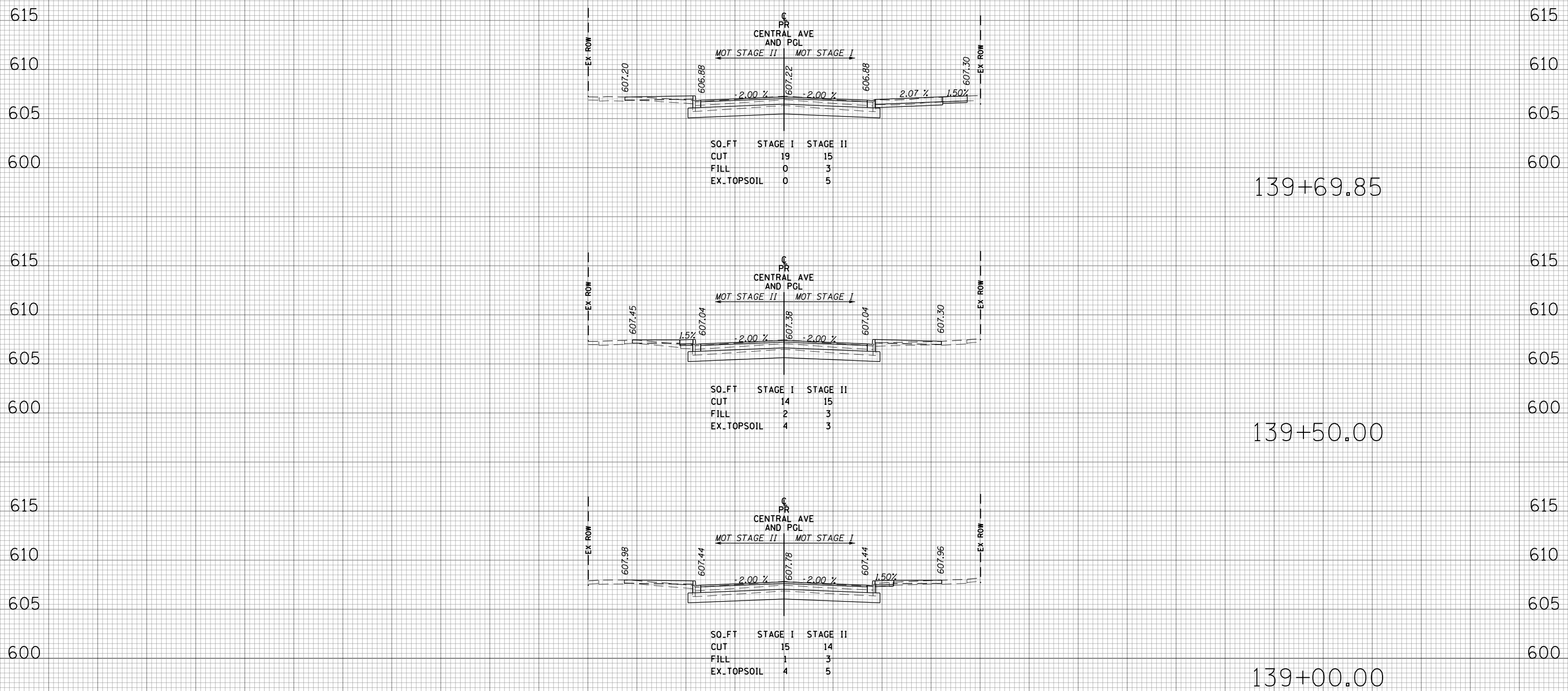
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FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISIED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - MLB	REVISIED -		1296	16-00199-00-RS	COOK	318	288			
		CHECKED - DWB	REVISIED -		CONTRACT NO. 61G42							
		DATE 02/13/2020	REVISIED -		ILLINOIS FED. AID PROJECT							
		PLOT SCALE = 20.0000' / in.		SCALE: 10H:5V	SHEET NO.288 OF 53 SHEETS	STA. 137+81.77 TO STA. 138+50.00						

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

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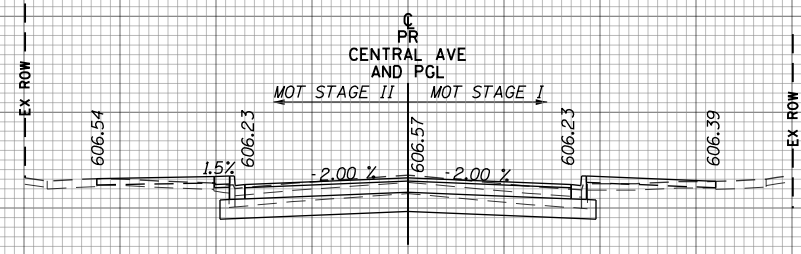


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FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 289
	PLOT SCALE = 20.0000' / in.	CHECKED - DWB	REVISED -		SCALE: 10H:5V	SHEET NO. 289 OF 53 SHEETS	STA. 139+00.00 TO STA. 139+69.85	CONTRACT NO. 61G42				
	PLOT DATE = 2/14/2020	DATE 02/13/2020	REVISED -		ILLINOIS FED. AID PROJECT							

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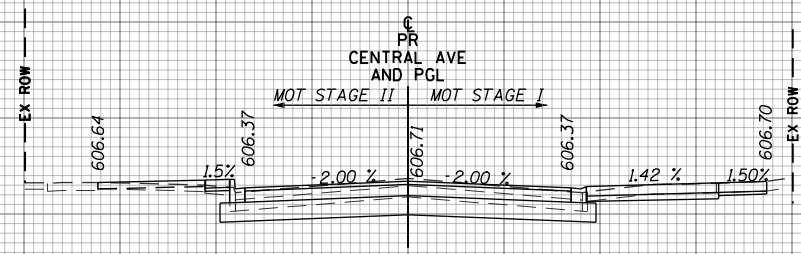
615 610 605 600 595



SO.FT	STAGE I	STAGE II
CUT	14	16
FILL	3	2
EX.TOPSOIL	4	4

140+50.00

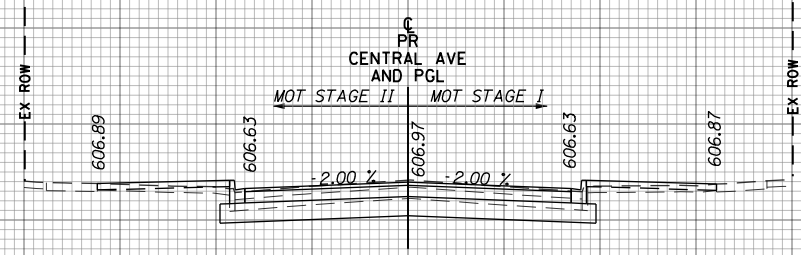
615 610 605 600 595



SO.FT	STAGE I	STAGE II
CUT	19	15
FILL	0	2
EX.TOPSOIL	0	4

140+32.11

615 610 605 600 595



SO.FT	STAGE I	STAGE II
CUT	15	15
FILL	3	3
EX.TOPSOIL	4	5

140+00.00

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

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FILE NAME =

USER NAME = Mibeering
 DESIGNED - MLB
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 CHECKED - DWB
 DATE 02/13/2020

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

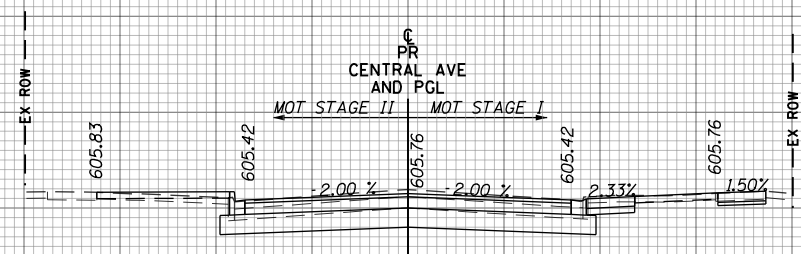
CENTRAL AVENUE AND WILMETTE AVENUE
 CROSS SECTIONS
 CENTRAL AVENUE

SCALE: 10H:5V SHEET NO290 OF 53 SHEETS STA. 140+00.00 TO STA. 140+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	290
CONTRACT NO. 61G42				ILLINOIS FED. AID PROJECT

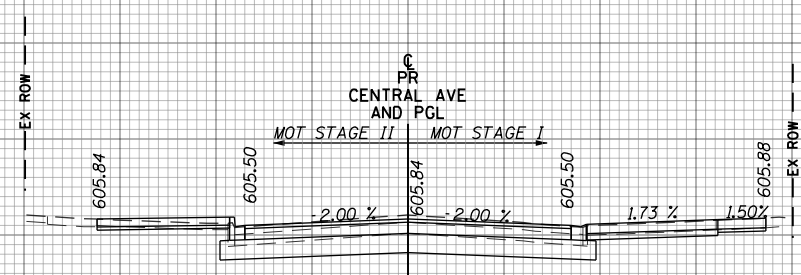
160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

615 610 605 600 595 615 610 605 600 595 615 610 605 600 595 615 610 605 600 595



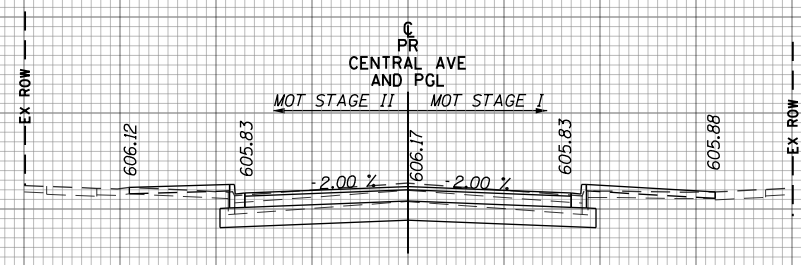
SO.FT	STAGE I	STAGE II
CUT	22	17
FILL	0	2
EX.TOPSOIL	4	5

141+50.00



SO.FT	STAGE I	STAGE II
CUT	21	18
FILL	0	0
EX.TOPSOIL	0	5

141+40.06



SO.FT	STAGE I	STAGE II
CUT	16	16
FILL	2	2
EX.TOPSOIL	4	4

141+00.00

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

DATE	BY	SURVEYED	PLOTTED	TEMPLATE	AREAS	CHECKED
FINAL SURVEY NO.		NOTE BOOK				

DATE	BY	SURVEYED	PLOTTED	TEMPLATE	AREAS	CHECKED
ORIGINAL SURVEY NO.		NOTE BOOK				

FILE NAME =

USER NAME = Mibeering
 DESIGNED - MLB
 DRAWN - MLB
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 DATE 02/13/2020

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

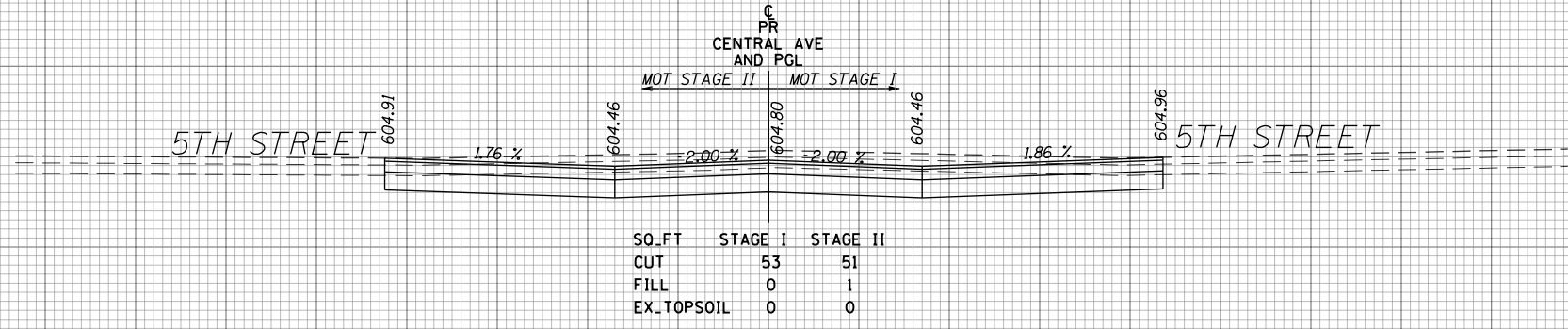
CENTRAL AVENUE AND WILMETTE AVENUE
 CROSS SECTIONS
 CENTRAL AVENUE

SCALE: 10H:5V SHEET NO.291 OF 53 SHEETS STA. 141+00.00 TO STA. 141+50.00

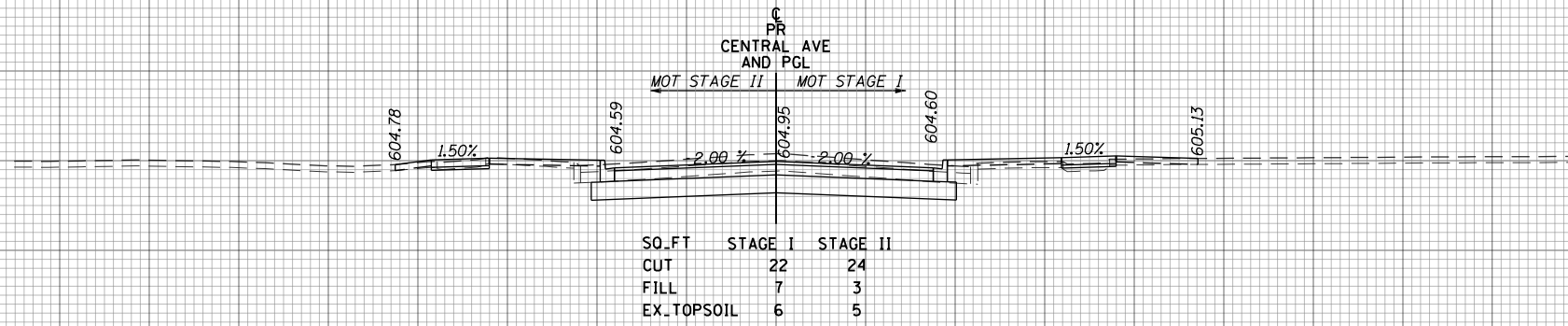
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	291
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

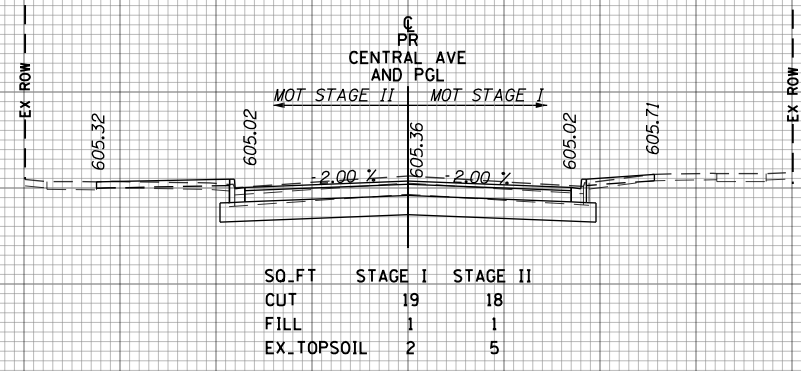
615 610 605 600 595 615 610 605 600 595 615 610 605 600 595 615 610 605 600 595



142+68.50



142+50.00



142+00.00

DATE	
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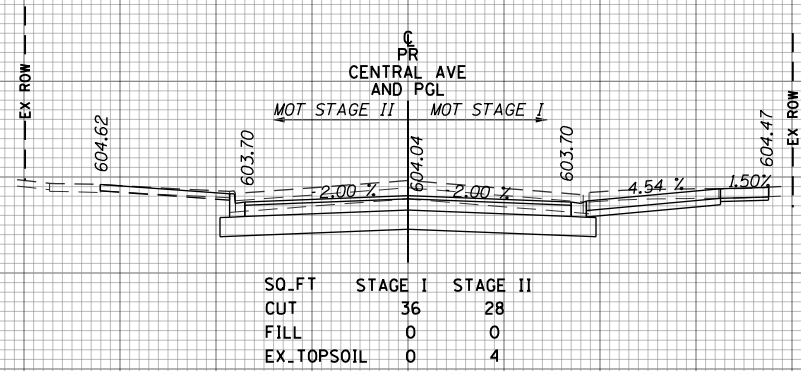
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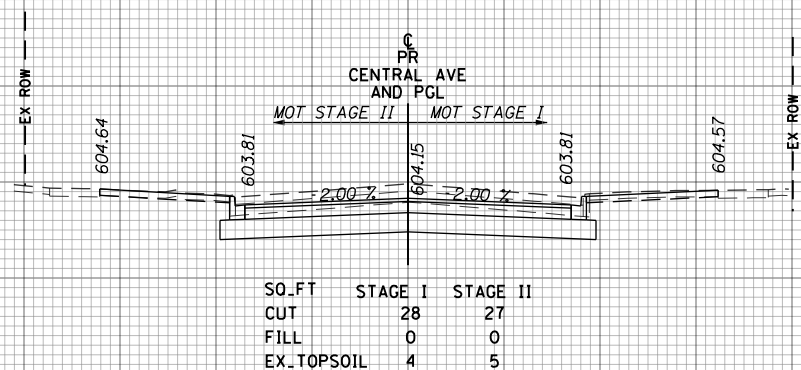
FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 292
	PLOT SCALE = 20.0000' / in.	CHECKED - DWB	REVISED -		SCALE: 10H:5V	SHEET NO. 292 OF 53 SHEETS	STA. 142+00.00 TO STA. 142+68.50	CONTRACT NO. 61G42		ILLINOIS FED. AID PROJECT		
	PLOT DATE = 2/14/2020	DATE 02/13/2020	REVISED -									

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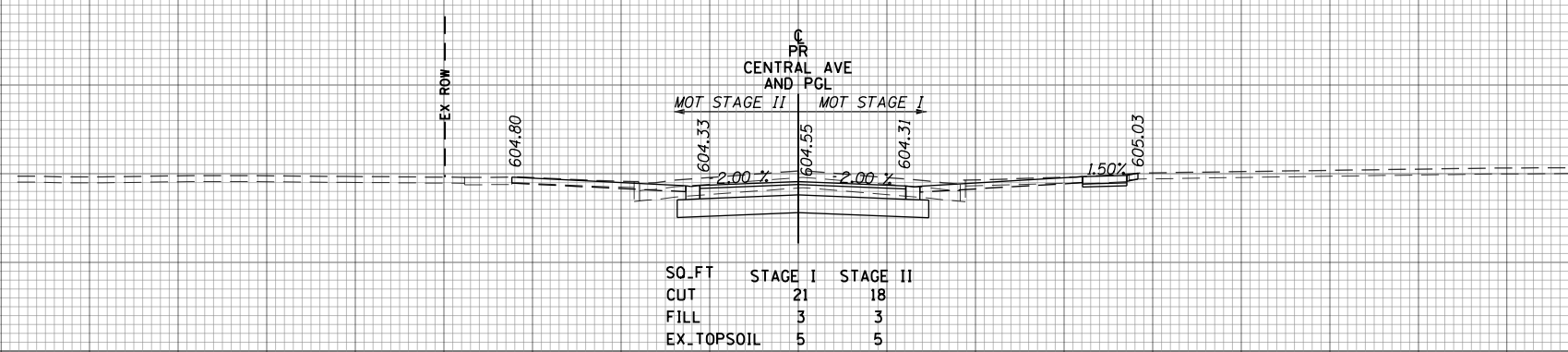
615 610 605 600 595 615 610 605 600 595 615 610 605 600 595 615 610 605 600 595



143+68.03



143+50.00



143+00.00

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BY	
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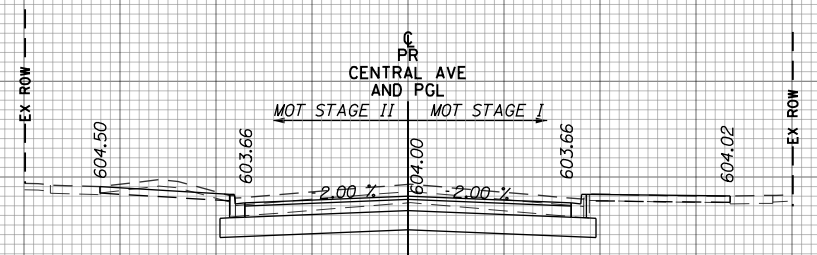
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ORIGINAL SURVEY	
NOTE BOOK	
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160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - MLB	REVISED -		1296	16-00199-00-RS	COOK	318	293			
		CHECKED - DWB	REVISED -		CONTRACT NO. 61G42							
		DATE 02/13/2020	REVISED -		ILLINOIS FED. AID PROJECT							
		PLOT SCALE = 20.0000' / in.		SCALE: 10H:5V	SHEET NO.293 OF 53 SHEETS	STA. 143+00.00 TO STA. 143+68.03						

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

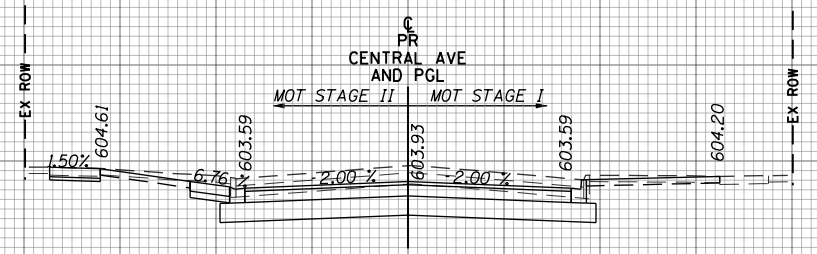
615 610 605 600 595



SO.FT	STAGE I	STAGE II
CUT	24	28
FILL	0	0
EX.TOPSOIL	5	4

144+50.00

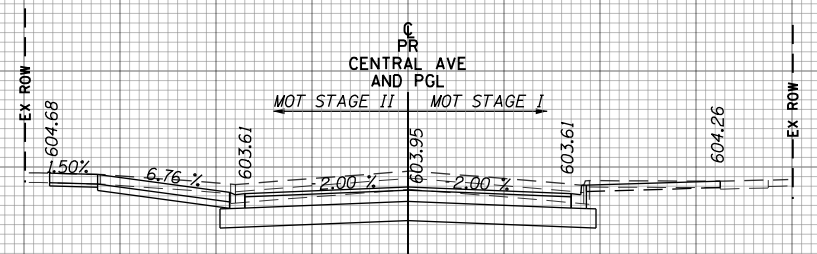
615 610 605 600 595



SO.FT	STAGE I	STAGE II
CUT	30	35
FILL	0	0
EX.TOPSOIL	5	5

144+00.00

615 610 605 600 595



SO.FT	STAGE I	STAGE II
CUT	29	36
FILL	0	0
EX.TOPSOIL	5	0

143+91.29

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

DATE	BY
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TEMPLATE	AREAS CHECKED
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NOTE BOOK	AREAS CHECKED
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FILE NAME =

USER NAME = Mibeering
 DESIGNED - MLB
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

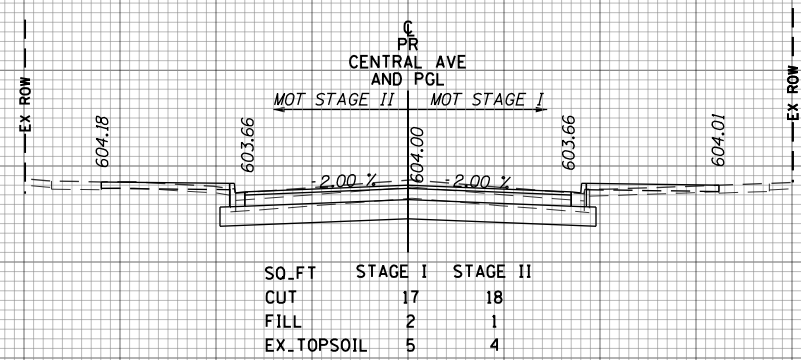
CENTRAL AVENUE AND WILMETTE AVENUE
 CROSS SECTIONS
 CENTRAL AVENUE

SCALE: 10H:5V SHEET NO.294 OF 53 SHEETS STA. 143+91.29 TO STA. 144+50.00

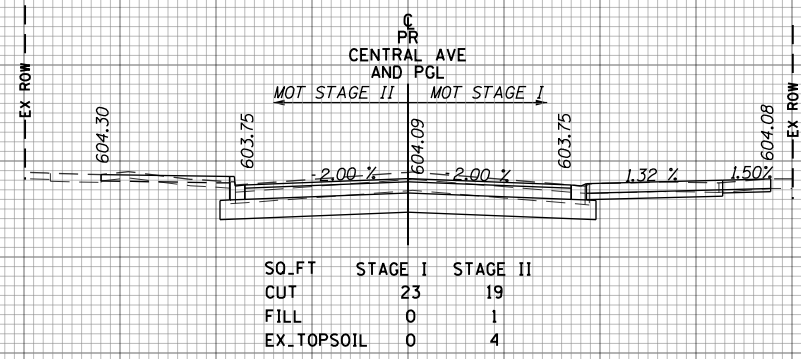
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1296	16-00199-00-RS	COOK	318	294
CONTRACT NO. 61G42			ILLINOIS FED. AID PROJECT	

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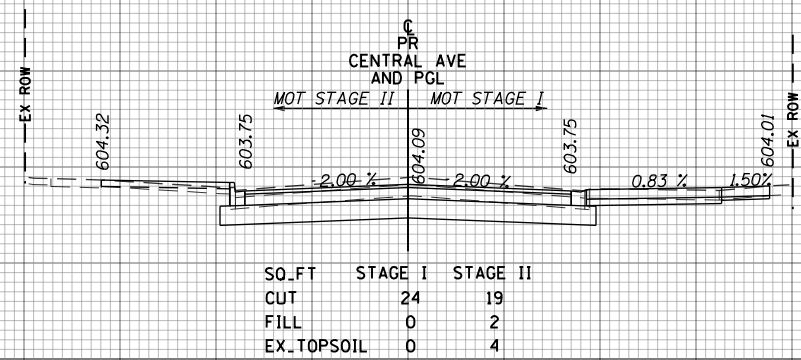
615 610 605 600 595 615 610 605 600 595 615 610 605 600 595 615 610 605 600 595



145+50.00



145+10.63



145+00.00

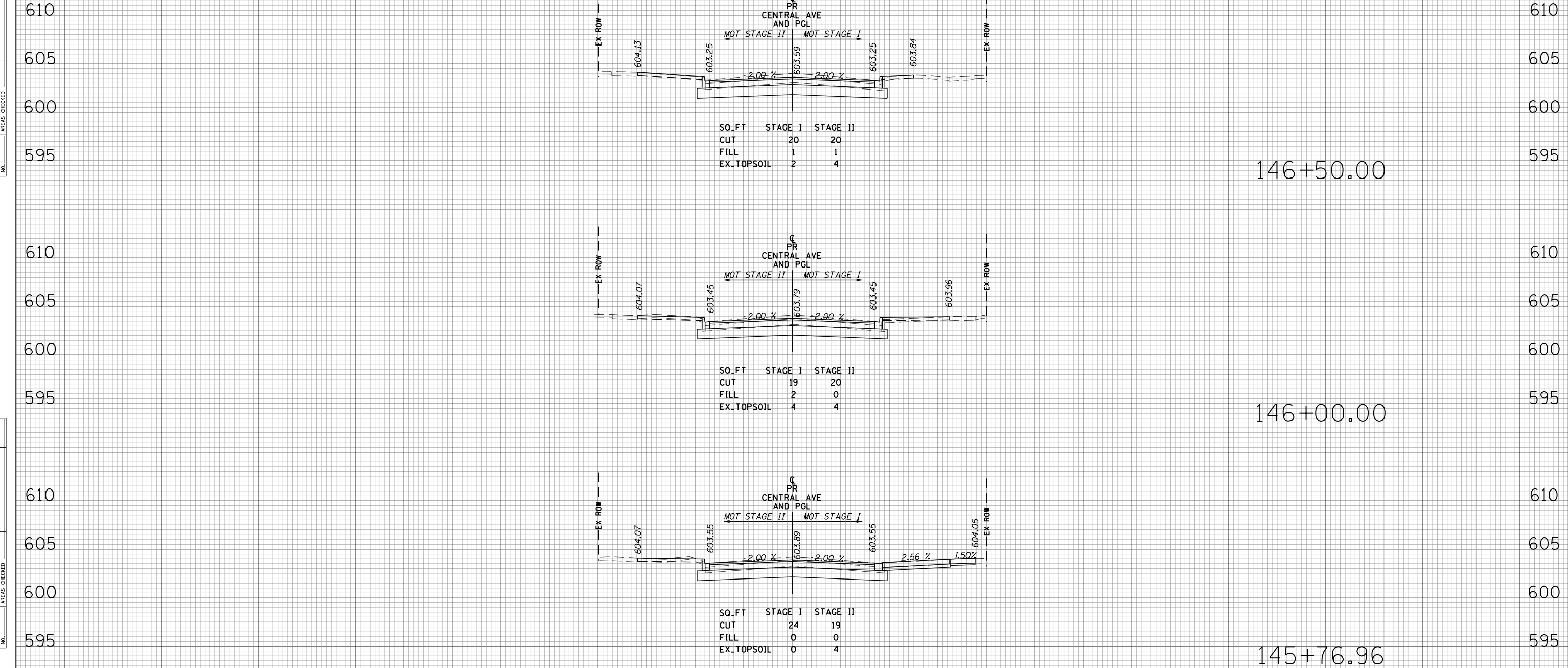
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TEMPLATE	AREAS CHECKED
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160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE. 1296	SECTION 16-00199-00-RS	COUNTY COOK	TOTAL SHEETS 318	SHEET NO. 295
	PLOT SCALE = 20.0000' / in.	CHECKED - DWB	REVISED -		SCALE: 10H:5V	SHEET NO.295 OF 53 SHEETS	STA. 145+00.00 TO STA. 145+50.00	CONTRACT NO. 61G42		ILLINOIS FED. AID PROJECT		
	PLOT DATE = 2/14/2020	DATE 02/13/2020	REVISED -									

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160



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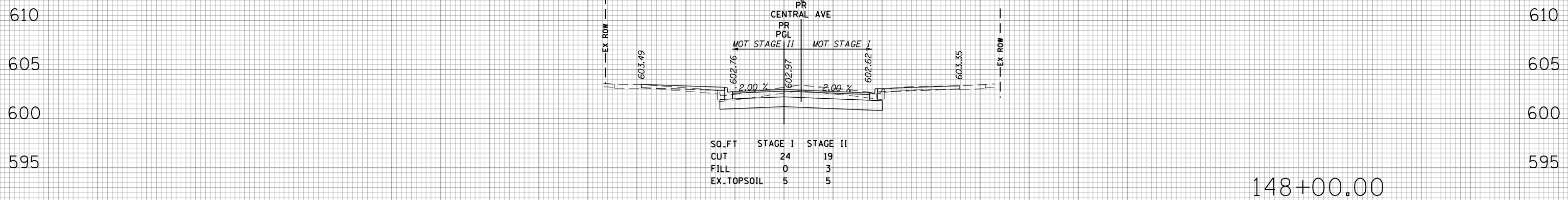
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AREAS	
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NOTE BOOK NO.	

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - MLB	REVISED -		1296	16-00199-00-RS	COOK	318	296			
		CHECKED - DWB	REVISED -		CONTRACT NO. 61G42							
		DATE 02/13/2020	REVISED -		ILLINOIS FED. AID PROJECT							
		PLOT SCALE = 20.0000' / in.			SCALE: 10H:5V	SHEET NO296 OF 53 SHEETS	STA. 145+76.96	TO STA. 146+50.00				

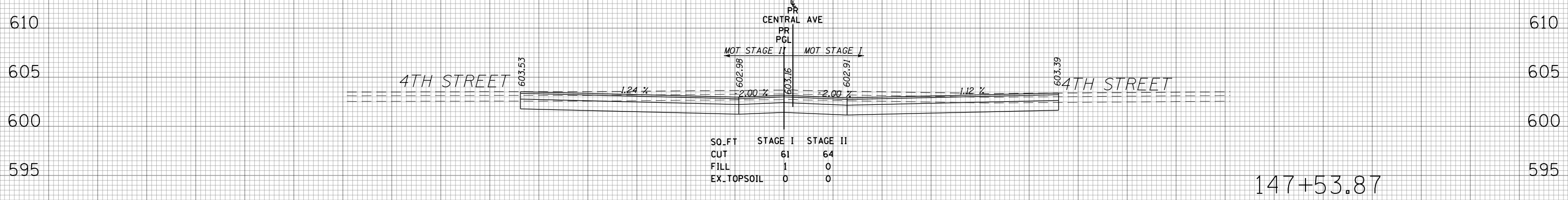
160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

DATE	
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SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

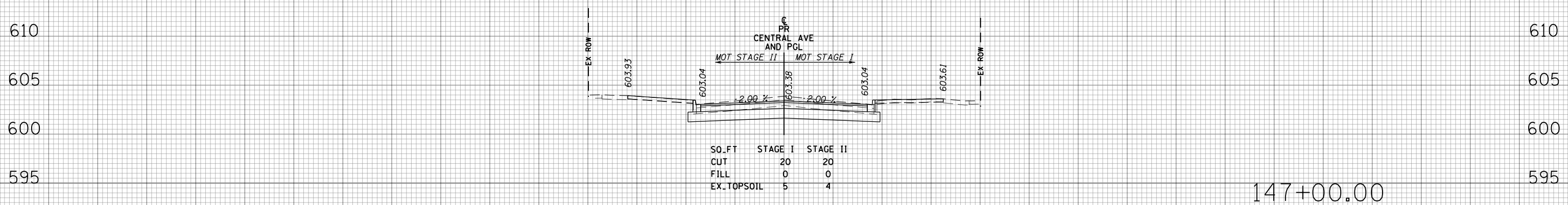


148+00.00

DATE	
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NOTE BOOK	
NO.	



147+53.87



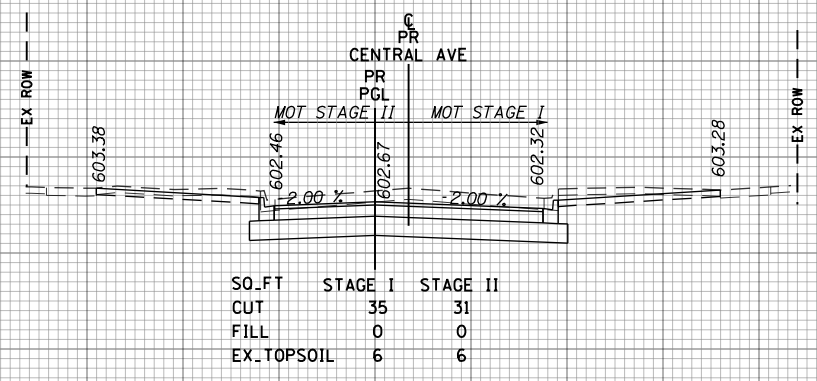
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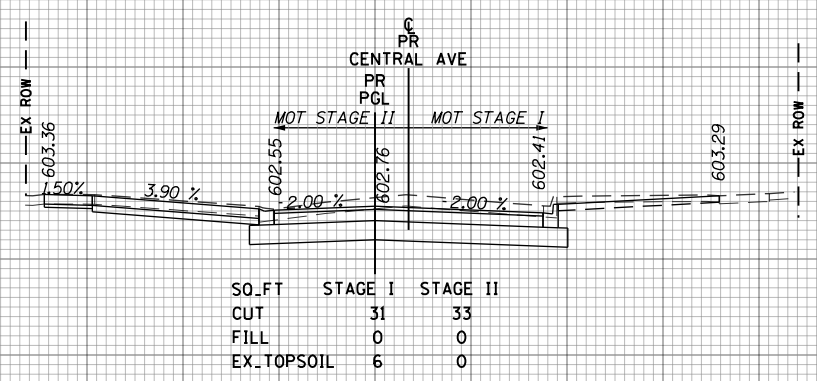
FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - MLB	REVISED -		1296	16-00199-00-RS	COOK	318	297			
		CHECKED - DWB	REVISED -		CONTRACT NO. 61G42							
		DATE 02/13/2020	REVISED -		ILLINOIS FED. AID PROJECT							
		PLOT SCALE = 20.0000' / in.		SCALE: 10H:5V	SHEET NO.297 OF 53 SHEETS	STA. 147+00.00 TO STA. 148+00.00						

DATE	
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FINAL SURVEY NO.	

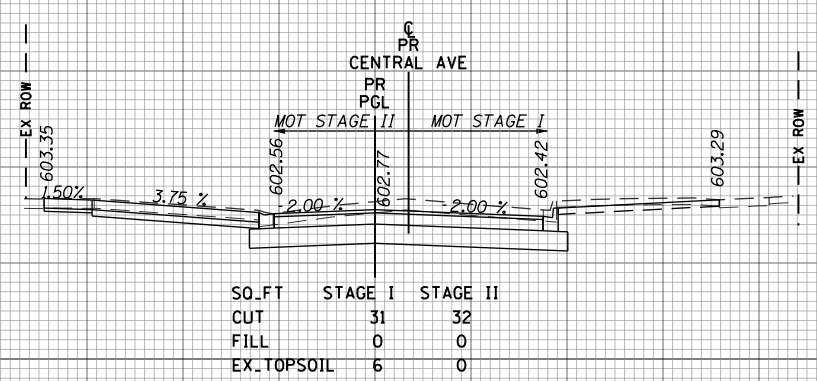
DATE	
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ORIGINAL SURVEY NO.	



149+00.00



148+54.19

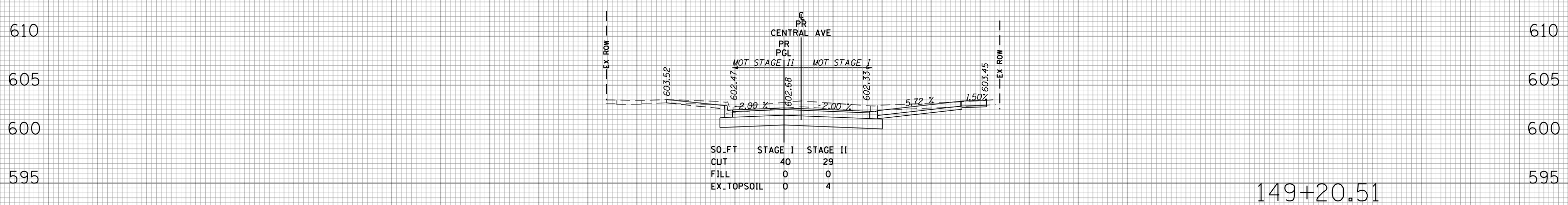
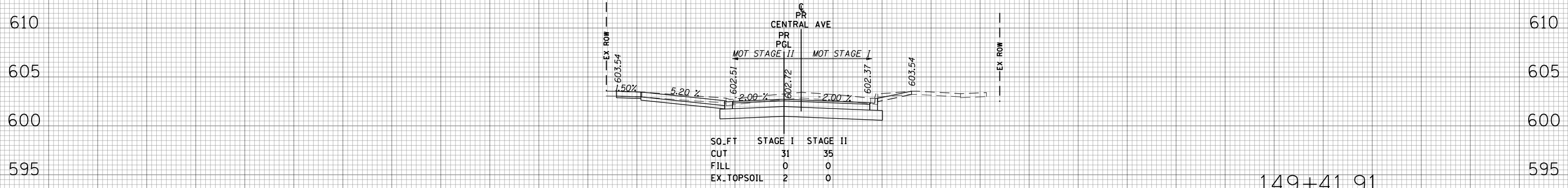
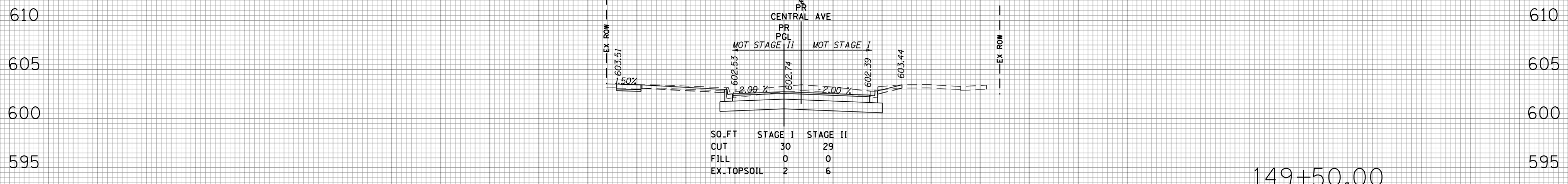


148+50.00

FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 20.0000' / in.	DRAWN - MLB	REVISED -		1296	16-00199-00-RS	COOK	318	298				
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -		SCALE: 10H:5V SHEET NO.298 OF 53 SHEETS STA. 148+50.00 TO STA. 149+00.00				CONTRACT NO. 61G42		ILLINOIS FED. AID PROJECT		

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FINAL SURVEY NO.	

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AREAS	
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ORIGINAL SURVEY NO.	

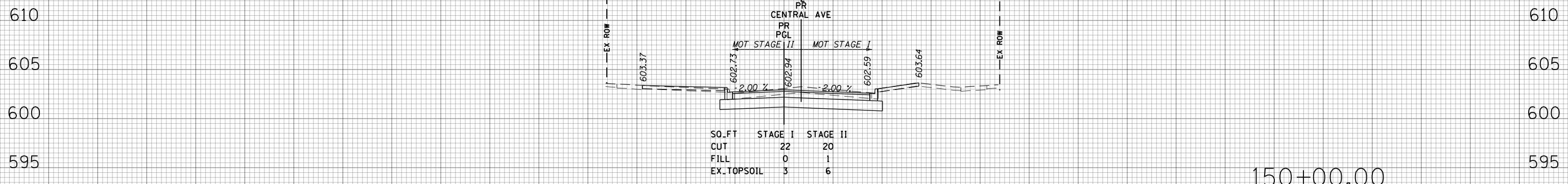


FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	<p align="center">STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</p> <p align="center">CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE</p>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 20.0000' / in.	DRAWN - MLB	REVISED -		1296	16-00199-00-RS	COOK	318	299
	PLOT DATE = 2/14/2020	CHECKED - DWB	REVISED -		CONTRACT NO. 61G42				
		DATE 02/13/2020	REVISED -		ILLINOIS FED. AID PROJECT				

SCALE: 10H:5V SHEET NO299 OF 53 SHEETS STA. 149+20.51 TO STA. 149+50.00

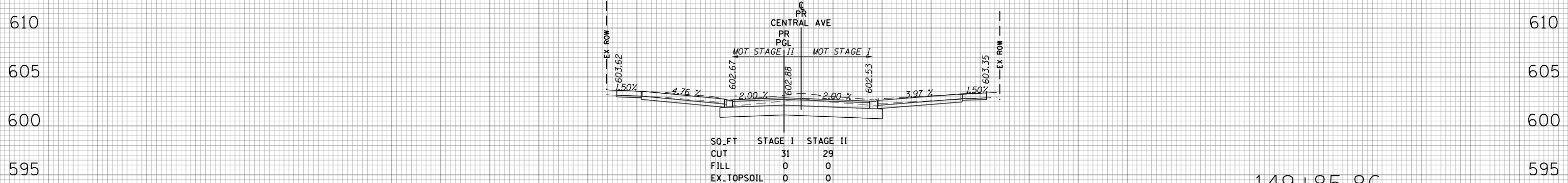
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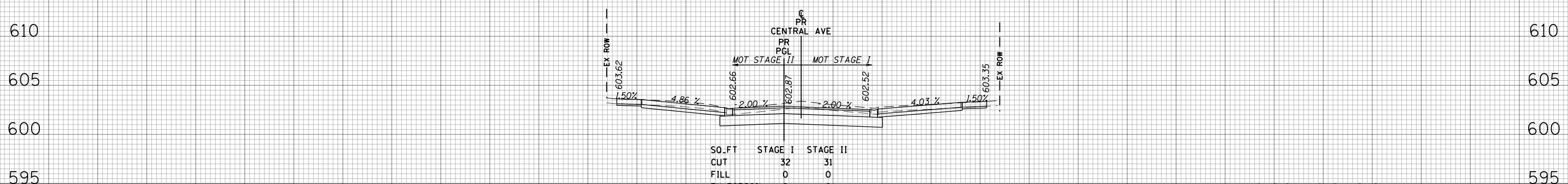


150+00.00

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PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



149+85.86



149+82.66

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

FILE NAME =	USER NAME = Mibeering	DESIGNED - MLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CENTRAL AVENUE AND WILMETTE AVENUE CROSS SECTIONS CENTRAL AVENUE			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - MLB	REVISED -		1296	16-00199-00-RS	COOK	318	300			
		CHECKED - DWB	REVISED -		CONTRACT NO. 61G42							
		DATE 02/13/2020	REVISED -		SCALE: 10H:5V	SHEET NO.300 OF 53 SHEETS	STA. 149+82.66 TO STA. 150+00.00	ILLINOIS FED. AID PROJECT				