

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR LIST OF APPLICABLE HIGHWAY STANDARDS
SEE SHEET 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

BRYN MAWR AVENUE PEDESTRIAN BRIDGE INTERSECTION IMPROVEMENT PROJECT

SECTION 19-00036-00-BR

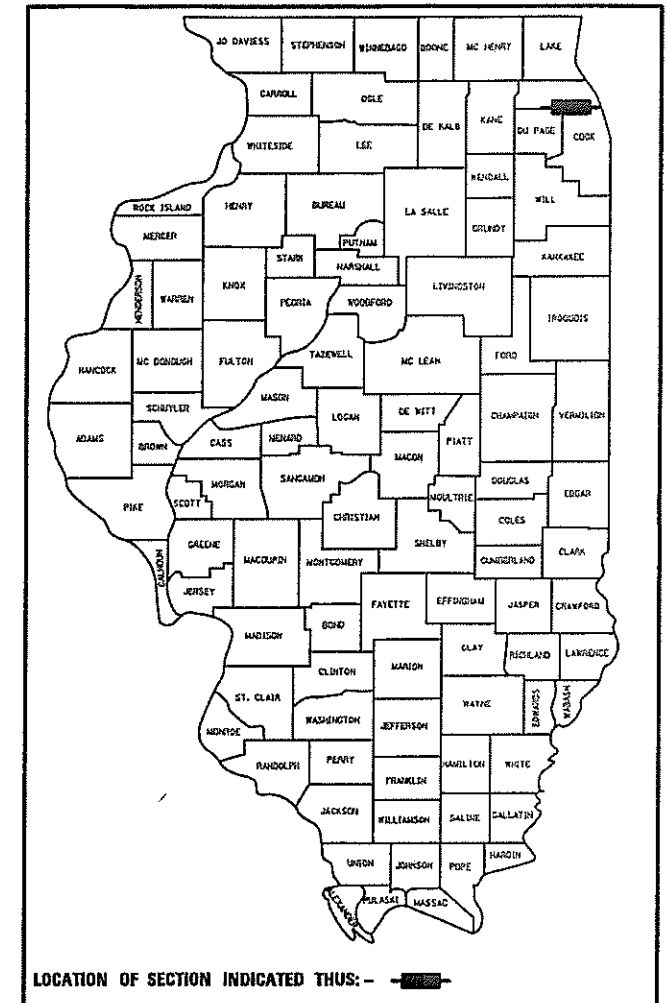
PROJECT NO: RI5Y(318)

VILLAGE OF ROSEMONT

COOK COUNTY

JOB NO. C-91-146-20

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	19-00036-00-BR	COOK	36	1
		ILLINOIS	CONTRACT NO. 61G51	



BRYN MAWR AVENUE

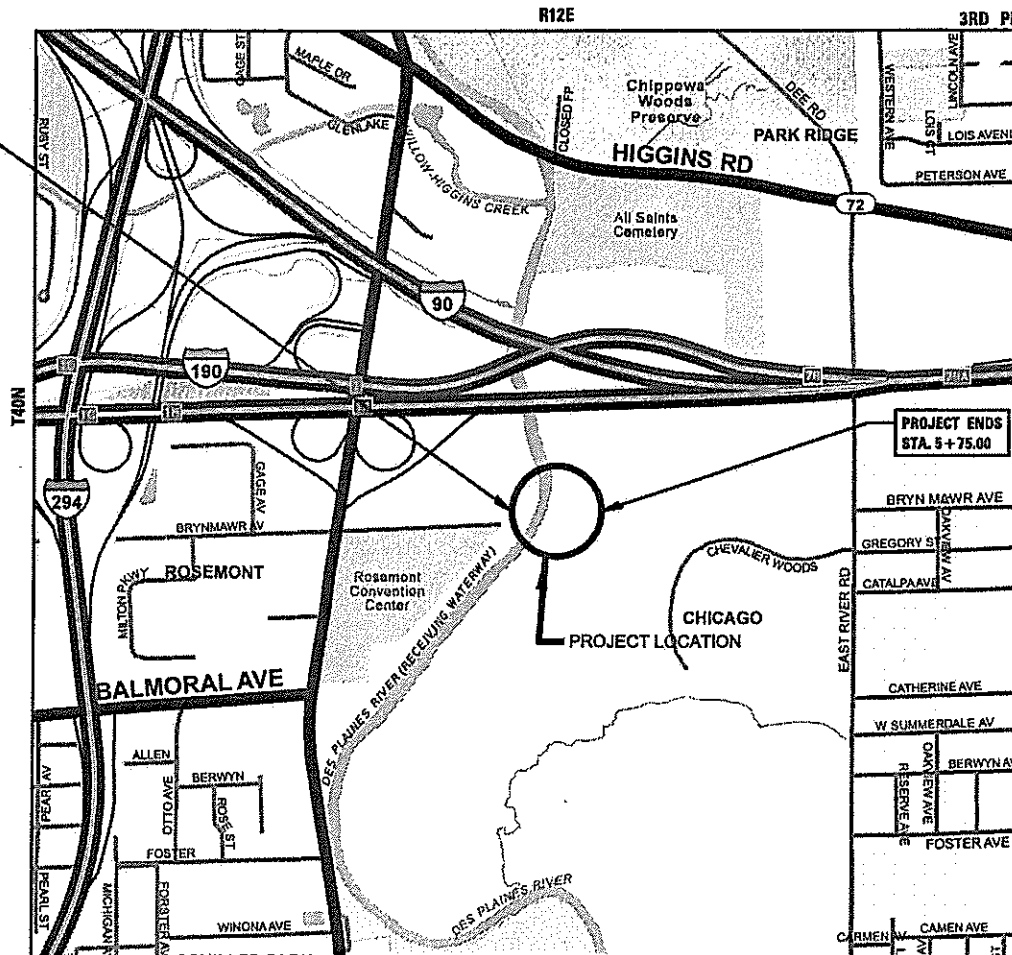
DESIGN DESIGNATION
LOCAL ROAD

POSTED SPEED
25 M.P.H.

TRAFFIC DATA
ADT (YEAR) = <2,500 (2019)

PROJECT BEGINS
STA. 0+82.00

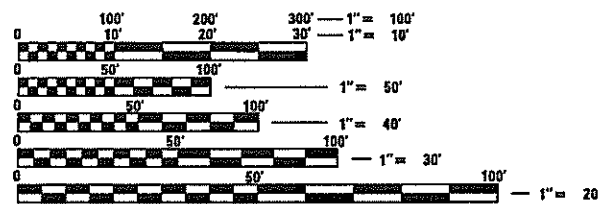
PROJECT ENDS
STA. 5+75.00



LEYDEN TOWNSHIP

LOCATION MAP
(NOT TO SCALE)

GROSS LENGTH = 483 FT. = 0.09 MILE
NET LENGTH = 483 FT. = 0.09 MILE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CHRISTOPHER B. BURKE ENGINEERING, LTD.
9575 W. Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 623-0500

PROFESSIONAL DESIGN FIRM NO. 184-001175
EXPIRATION DATE: 04/30/21

CONTRACT NO. 61G51

FEDERAL AID ENGINEER: CARMEN E. RAMOS, P.E. SCHAUMBURG, ILLINOIS

AGENCY RESPONSIBLE FOR LETTING

APPROVED February 26, 2020
[Signature]
VILLAGE MAYOR, VILLAGE OF ROSEMONT

PASSED 3-11-2020
[Signature]
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASED FOR BID
BASED ON LIMITED
REVIEW March 13, 2020
[Signature]
REGIONAL ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

NICHOLAS J. MOREL
REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS
[Signature] 2/16/20
ENGINEER DATE

MASJID MOBASSERI
ILLINOIS REGISTRATION No. 081-005058
STRUCTURAL ENGINEER
EXPIRATION DATE: 11/30/2020
[Signature] 2/16/20

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GENERAL NOTES

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2020; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (MUTCD); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" PUBLISHED JULY 2014; THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", (SSTCI), THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT STANDARD.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 701 OF THE STANDARD SPECIFICATIONS.

UTILITIES

THE CONTRACTOR SHALL COOPERATE WITH THE VILLAGE IN ANY UNDERGROUND UTILITY CONSTRUCTION WHICH THE VILLAGE MAY WANT TO PLACE DURING THE CONTRACTOR'S OPERATIONS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE VILLAGE.

COORDINATION OF ALL UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, CABLE AND GAS FACILITIES AND THE VILLAGE OF BENSENVILLE PUBLIC WORKS DEPT. AT 350-3435 FOR FIELD LOCATIONS OF BURIED WATER, SANITARY AND STORM FACILITIES (48-HOUR ADVANCE NOTIFICATION IS REQUIRED).

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE VILLAGE, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED, AND SHALL BE AS INDICATED ON THE PLANS. ELEVATIONS SHOWN AT POINT OF CURVE, ETC. ARE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.

ALL OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS FOR STRUCTURES, BACK OF CURBS, ETC., ARE FROM THE CENTERLINE OF CONSTRUCTION.

STORM WATER STRUCTURE OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS ARE TO THE EDGE OF PAVEMENT FOR ALL STRUCTURES FALLING IN THE CURB LINE AND TO THE CENTER OF THE STRUCTURE FOR ALL OTHER STRUCTURES.

TOP OF CURB ELEVATIONS: BEFORE SETTING THE TOP OF CURB ELEVATIONS, THE ENGINEER SHALL CHECK THE EXISTING ELEVATIONS AT THE ADJACENT PROPERTY LINE AND, IF NECESSARY, REQUIRE THE CONTRACTOR TO VARY THE CURB EXPOSURE AND/OR GUTTER ELEVATIONS IN ORDER TO MORE CLOSELY FOLLOW THE PROPERTY LINE GRADES.

PROPOSED DRAINAGE STRUCTURES: DRAINAGE STRUCTURE RIM ELEVATIONS ADJACENT TO THE EDGE OF PAVEMENT ARE THE EDGE OF PAVEMENT ELEVATION.

WATER, STORM SEWER AND SANITARY SEWER

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS.

WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. HE SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE THE SAME. HE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWERS ARE BUILT AND IN SERVICE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT.

FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES OF ALL NEW, ADJUSTED OR RECONSTRUCTED STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED.

ANY EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR.

DRAINAGE STRUCTURE OFFSETS AS SHOWN ON THE PLANS ARE GIVEN TO THE FOLLOWING POINTS:

- (A) FOR STRUCTURES FALLING IN THE CURB LINE - TO THE EDGE OF PAVEMENT.
 - (B) FOR ALL OTHER STRUCTURES - TO THE CENTER OF THE STRUCTURE.
- RIM ELEVATIONS SHOWN ON THE PLANS FOR DRAINAGE STRUCTURES IN THE CURB LINE ARE EDGE OF PAVEMENT ELEVATIONS.

THE ENDS OF EXISTING DRAINAGE LINES AND HOLES IN EXISTING MANHOLES WHICH ARE NOT TO BE INCORPORATED INTO THE PROPOSED IMPROVEMENTS DESIGNATED BY THE ENGINEER SHALL BE SEALED WITH A PORTLAND CEMENT MORTAR TO THE SATISFACTION OF THE ENGINEER.

THE CONTRACTOR SHALL CONFIRM ALL EXISTING STORM SEWER PIPE SIZES AND INVERTS PRIOR TO ORDERING STRUCTURES.

THE CONTRACTOR SHALL USE RUBBER "O-RING" JOINTS FOR ALL STORM PIPE. MASTIC MATERIAL WILL NOT BE ALLOWED.

BACKFILL

STORM SEWER SHALL BE BACKFILLED IN ACCORDANCE WITH ARTICLE 550.07.

ALL TRENCH BACKFILL QUANTITIES FOR STORM AND SANITARY SEWER HAVE BEEN COMPUTED AND SHALL BE PAID FOR IN ACCORDANCE WITH THE STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS BUREAU OF CONSTRUCTION TRENCH BACKFILL TABLE, BASED ON INVERT DEPTH FROM EXISTING PAVEMENT.

MISCELLANEOUS

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION.

ALL EXISTING AGGREGATE DRIVEWAYS SHALL BE REPLACED AS BITUMINOUS DRIVEWAYS.

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

AT ALL BUTT JOINT LOCATIONS, THE EXISTING SURFACE SHALL BE CUT TO A MINIMUM DEPTH OF ONE AND ONE HALF (1-1/2) INCHES.

TYPE "A" SIDEWALK RAMPS FOR THE HANDICAPPED SHALL BE INSTALLED AT ALL INTERSECTING STREETS AND DRIVEWAYS AS DIRECTED BY THE ENGINEER (SEE SPECIFICATIONS FOR CONSTRUCTION DETAILS).

DEPRESSED CURB: RAMPS OR DEPRESSED CURBS ACCESSIBLE TO THE HANDICAPPED SHALL BE PROVIDED AT ALL CROSSWALKS AND BICYCLE PATHS. NON-ADA DEPRESSED CURB SHALL BE PROVIDED AT ALL ALLEYS OR DRIVEWAYS.

PROTECTIVE COAT SHALL BE APPLIED TO ALL GUTTER FLAGS, FACE AND TOP OF CURB AND GUTTER, P.C.C. SIDEWALK AND P.C.C. DRIVEWAY PAVEMENT AS DIRECTED BY THE ENGINEER.

PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT OR SURFACE COURSE UNLESS OTHERWISE INDICATED.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS".

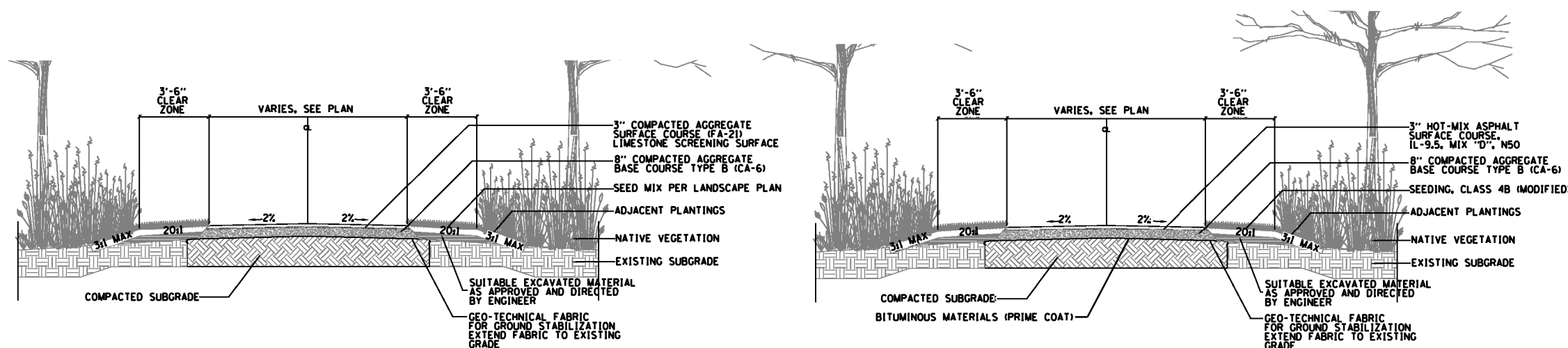
EXPOSED SUBGRADE MUST BE COVERED WITHIN 24 HOURS OF EXCAVATION. UNSTABLE SUBGRADE AREAS, AS DETERMINED BY THE ENGINEER, RESULTING FROM THE CONTRACTOR'S FAILURE TO COVER THE SUBGRADE SHALL BE EXCAVATED AND BACKFILLED WITH POROUS GRANULAR EMBANKMENT, SUBGRADE.

AT THE COMPLETION OF THE PROPOSED SIGNING ALL EXISTING STREET TRAFFIC SIGNS SHALL BE REMOVED BY THE CONTRACTOR AND DELIVERED TO THE VILLAGE OF ROSEMONT DEPARTMENT OF PUBLIC WORKS FACILITY AT 5300 PEARL STREET.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
ITEM	VOIDS
HMA PATH HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 3"	4% @ 50 GYR.
CLASS D PATCHES, 8 INCH (HMA SURFACE COURSE, IL-9.5, MIX "D", N50, 2") (HMA BINDER COURSE, IL-19.0, N50, 6")	4% @ 50 GYR.

NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE IS 112 LBS/SY/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
3. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.



X LIMESTONE TRAIL
SECTION SCALE: 1"=1' UNIT

X HMA PATH
SECTION SCALE: 1"=1' UNIT

STA. 4+96 - STA. 5+79
N.T.S.

STA. 0+96 - STA. 1+20
STA. 4+70 - STA. 4+96
N.T.S.

FILE NAME = N:\ROSEMONT\14019\Civil\02_GEN.14019.SH	USER NAME = jlepeglia	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BRYN MAWR BRIDGE AND RIVERWALK GENERAL NOTES AND TYPICAL SECTIONS	F.A. -	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	PLOT SCALE = 2"	CHECKED -	REVISED -			-	19-00036-00-BR	COOK	36	3	
	PLOT DATE = 3/19/2020	DATE -	REVISED -			CONTRACT NO. 61G51					
						ILLINOIS FED. AID PROJECT					

A. REFERENCED SPECIFICATIONS

- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE APPLICABLE SECTIONS OF THE FOLLOWING, EXCEPT AS MODIFIED HEREIN OR ON THE PLANS:
 * STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (LATEST EDITION), BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT SS) FOR ALL IMPROVEMENTS EXCEPT SANITARY SEWER AND WATER MAIN CONSTRUCTION;
 * STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS, LATEST EDITION (SSWS) FOR SANITARY SEWER AND WATER MAIN CONSTRUCTION;
 * VILLAGE OF ROSEMONT MUNICIPAL CODE;
 * THE METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO (MWRD) WATERSHED MANAGEMENT ORDINANCE AND TECHNICAL GUIDANCE MANUAL;
 * IN CASE OF CONFLICT BETWEEN THE APPLICABLE ORDINANCES NOTED, THE MORE STRINGENT SHALL TAKE PRECEDENCE AND SHALL CONTROL ALL CONSTRUCTION.

B. NOTIFICATIONS

- THE MWRD LOCAL SEWER SYSTEMS SECTION FIELD OFFICE MUST BE NOTIFIED AT LEAST TWO (2) WORKING DAYS PRIOR TO THE COMMENCEMENT OF ANY WORK (CALL 708-588-4055).
- THE VILLAGE OF ROSEMONT ENGINEERING DEPARTMENT AND PUBLIC MUST BE NOTIFIED AT LEAST 24 HOURS PRIOR TO THE START OF CONSTRUCTION AND PRIOR TO EACH PHASE OF WORK. CONTRACTOR SHALL DETERMINE ITEMS REQUIRING INSPECTION PRIOR TO START OF CONSTRUCTION OR EACH WORK PHASE.
- THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES PRIOR TO BEGINNING CONSTRUCTION FOR THE EXACT LOCATIONS OF UTILITIES AND FOR THEIR PROTECTION DURING CONSTRUCTION. IF EXISTING UTILITIES ARE ENCOUNTERED THAT CONFLICT IN LOCATION WITH NEW CONSTRUCTION, IMMEDIATELY NOTIFY THE ENGINEER SO THAT THE CONFLICT CAN BE RESOLVED. CALL J.U.L.I.E. AT 1-800-892-0123.

C. GENERAL NOTES

- ALL ELEVATIONS SHOWN ON PLANS REFERENCE THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).
- MWRD, THE MUNICIPALITY AND THE OWNER OR OWNER'S REPRESENTATIVE SHALL HAVE THE AUTHORITY TO INSPECT, APPROVE, AND REJECT THE CONSTRUCTION IMPROVEMENTS.
- THE CONTRACTOR(S) SHALL INDEMNIFY THE OWNER, ENGINEER, MUNICIPALITY, MWRD, AND THEIR AGENTS, ETC., FROM ALL LIABILITY INVOLVED WITH THE CONSTRUCTION, INSTALLATION, OR TESTING OF THIS WORK ON THE PROJECT.
- THE PROPOSED IMPROVEMENTS MUST BE CONSTRUCTED IN ACCORDANCE WITH THE ENGINEERING PLANS AS APPROVED BY MWRD AND THE MUNICIPALITY UNLESS CHANGES ARE APPROVED BY MWRD, THE MUNICIPALITY, OR AUTHORIZED AGENT. THE CONSTRUCTION DETAILS, AS PRESENTED ON THE PLANS, MUST BE FOLLOWED. PROPER CONSTRUCTION TECHNIQUES MUST BE FOLLOWED ON THE IMPROVEMENTS INDICATED ON THE PLANS.
- THE LOCATION OF VARIOUS UNDERGROUND UTILITIES WHICH ARE SHOWN ON THE PLANS ARE FOR INFORMATION ONLY AND REPRESENT THE BEST KNOWLEDGE OF THE ENGINEER. VERIFY LOCATIONS AND ELEVATIONS PRIOR TO BEGINNING THE CONSTRUCTION OPERATIONS.
- ANY EXISTING PAVEMENT, SIDEWALK, DRIVEWAY, ETC., DAMAGED DURING CONSTRUCTION OPERATIONS AND NOT CALLED FOR TO BE REMOVED SHALL BE REPLACED AT THE EXPENSE OF THE CONTRACTOR.
- BACKFILL MATERIAL AND COMPACTION TESTING SHALL BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE MUNICIPALITY, MWRD, AND OWNER.
- THE UNDERGROUND CONTRACTOR SHALL MAKE ALL NECESSARY ARRANGEMENTS TO NOTIFY ALL INSPECTION AGENCIES.
- ALL NEW AND EXISTING UTILITY STRUCTURES ON SITE AND IN AREAS DISTURBED DURING CONSTRUCTION SHALL BE ADJUSTED TO FINISH GRADE PRIOR TO FINAL INSPECTION.
- RECORD DRAWINGS SHALL BE KEPT BY THE CONTRACTOR AND SUBMITTED TO THE ENGINEER AS SOON AS UNDERGROUND IMPROVEMENTS ARE COMPLETED. FINAL PAYMENTS TO THE CONTRACTOR SHALL BE HELD UNTIL THEY ARE RECEIVED. ANY CHANGES IN LENGTH, LOCATION OR ALIGNMENT SHALL BE SHOWN IN RED. ALL WYES OR BENDS SHALL BE LOCATED FROM THE DOWNSTREAM MANHOLE. ALL VALVES, B-BOXES, TEES OR BENDS SHALL BE TIED TO A FIRE HYDRANT.

D. SANITARY SEWER

- THE CONTRACTOR SHALL TAKE MEASURES TO PREVENT ANY POLLUTED WATER, SUCH AS GROUND AND SURFACE WATER, FROM ENTERING THE EXISTING SANITARY SEWERS.
- A WATER-TIGHT PLUG SHALL BE INSTALLED IN THE DOWNSTREAM SEWER PIPE AT THE POINT OF SEWER CONNECTION PRIOR TO COMMENCING ANY SEWER CONSTRUCTION. THE PLUG SHALL REMAIN IN PLACE UNTIL REMOVAL IS AUTHORIZED BY THE MUNICIPALITY AND/OR MWRD AFTER THE SEWERS HAVE BEEN TESTED AND ACCEPTED.
- DISCHARGING ANY UNPOLLUTED WATER INTO THE SANITARY SEWER SYSTEM FOR THE PURPOSE OF SEWER FLUSHING OF LINES FOR THE DEFLECTION TEST SHALL BE PROHIBITED WITHOUT PRIOR APPROVAL FROM THE MUNICIPALITY OR MWRD.
- ALL SANITARY SEWER CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS (LATEST EDITION).
- ALL FLOOR DRAINS SHALL DISCHARGE TO THE SANITARY SEWER SYSTEM.
- ALL DOWNSPOUTS AND FOOTING DRAINS SHALL DISCHARGE TO THE STORM SEWER SYSTEM.
- ALL SANITARY SEWER PIPE MATERIALS AND JOINTS (AND STORM SEWER PIPE MATERIALS AND JOINTS IN A COMBINED SEWER AREA) SHALL CONFORM TO THE FOLLOWING:

PIPE MATERIAL	PIPE SPECIFICATIONS	JOINT SPECIFICATIONS
VITRIFIED CLAY PIPE	ASTM C-700	ASTM C-425
REINFORCED CONCRETE SEWER PIPE	ASTM C-76	ASTM C-443
CAST IRON SOIL PIPE	ASTM A-74	ASTM C-564
DUCTILE IRON PIPE	ANSI A21.51	ANSI A21.11
POLYVINYL CHLORIDE (PVC) PIPE 6-INCH TO 15-INCH DIAMETER SDR 26 18-INCH TO 27-INCH DIAMETER F/DY=46	ASTM D-3034 ASTM F-679	ASTM D-3212 ASTM D-3212
HIGH DENSITY POLYETHYLENE (HDPE)	ASTM D-3350 ASTM D-3035	ASTM D-3261,F-2620 (HEAT FUSION) ASTM D-3212,F-477 (GASKETED)
WATER MAIN QUALITY PVC 4-INCH TO 36-INCH 4-INCH TO 12-INCH 14-INCH TO 48-INCH	ASTM D-2241 AWWA C900 AWWA C905	ASTM D-3139 ASTM D-3139 ASTM D-3139

THE FOLLOWING MATERIALS ARE ALLOWED ON A QUALIFIED BASIS SUBJECT TO DISTRICT REVIEW AND APPROVAL PRIOR TO PERMIT ISSUANCE. A SPECIAL CONDITION WILL BE ADDED TO THE PERMIT WHEN THE PIPE MATERIAL BELOW IS USED FOR SEWER CONSTRUCTION OR A CONNECTION IS MADE.

PIPE MATERIAL	PIPE SPECIFICATIONS	JOINT SPECIFICATIONS
POLYPROPYLENE (PP) PIPE		
12-INCH TO 24-INCH DOUBLE WALL	ASTM F-2736	D-3212, F-477
30-INCH TO 60-INCH TRIPLE WALL	ASTM F-2764	D3212, F-477

- ALL SANITARY SEWER CONSTRUCTION (AND STORM SEWER CONSTRUCTION IN COMBINED SEWER AREAS), REQUIRES STONE BEDDING WITH STONE ¼ " TO 1" IN SIZE, WITH MINIMUM BEDDING THICKNESS EQUAL TO ¼ THE OUTSIDE DIAMETER OF THE SEWER PIPE, BUT NOT LESS THAN FOUR (4) INCHES NOR MORE THAN EIGHT (8) INCHES. MATERIAL SHALL BE CA-7, CA-11 OR CA-13 AND SHALL BE EXTENDED AT LEAST 12" ABOVE THE TOP OF THE PIPE WHEN USING PVC.
- NON-SHEAR FLEXIBLE-TYPE COUPLINGS SHALL BE USED IN THE CONNECTION OF SEWER PIPES OF DISSIMILAR PIPE MATERIALS.
- ALL MANHOLES SHALL BE PROVIDED WITH BOLTED, WATERTIGHT COVERS. SANITARY LIDS SHALL BE CONSTRUCTED WITH A CONCEALED PICKHOLE AND WATERTIGHT GASKET WITH THE WORD "SANITARY" CAST INTO THE LID.
- WHEN CONNECTING TO AN EXISTING SEWER MAIN BY MEANS OTHER THAN AN EXISTING WYE, TEE, OR AN EXISTING MANHOLE, ONE OF THE FOLLOWING METHODS SHALL BE USED:
 a) A CIRCULAR SAW-CUT OF SEWER MAIN BY PROPER TOOLS AND PROPER INSTALLATION OF HUBWYE SADDLE OR HUB-TEE SADDLE.
 b) REMOVE AN ENTIRE SECTION OF PIPE (BREAKING ONLY THE TOP OF ONE BELL) AND REPLACE WITH A WYE OR TEE BRANCH SECTION.
 c) WITH PIPE CUTTER, NEATLY AND ACCURATELY CUT OUT DESIRED LENGTH OF PIPE FOR INSERTION OF PROPER FITTING, USING COUPLINGS TO HOLD IT FIRMLY IN PLACE.
- WHENEVER A SANITARY/COMBINED SEWER CROSSES UNDER A WATERMAIN, THE MINIMUM VERTICAL DISTANCE FROM THE TOP OF THE SEWER TO THE BOTTOM OF THE WATERMAIN SHALL BE 18 INCHES. FURTHERMORE, A MINIMUM HORIZONTAL DISTANCE OF 10 FEET BETWEEN SANITARY/COMBINED SEWERS AND WATERMANS SHALL BE MAINTAINED UNLESS: THE SEWER IS LAID IN A SEPARATE TRENCH, KEEPING A MINIMUM 18" VERTICAL SEPARATION; OR THE SEWER IS LAID IN THE SAME TRENCH WITH THE WATERMAIN LOCATED AT THE OPPOSITE SIDE ON A BENCH OF UNDISTURBED EARTH, KEEPING A MINIMUM 18" VERTICAL SEPARATION. IF EITHER THE VERTICAL OR HORIZONTAL DISTANCES DESCRIBED CANNOT BE MAINTAINED, OR THE SEWER CROSSES ABOVE THE WATER MAIN, THE SEWER SHALL BE CONSTRUCTED TO WATER MAIN STANDARDS OR IT SHALL BE ENCASED WITH A WATER MAIN QUALITY CARRIER PIPE WITH THE ENDS SEALED.
- ALL EXISTING SEPTIC SYSTEMS SHALL BE ABANDONED. ABANDONED TANKS SHALL BE FILLED WITH GRANULAR MATERIAL OR REMOVED.
- ALL SANITARY MANHOLES, (AND STORM MANHOLES IN COMBINED SEWER AREAS), SHALL HAVE A MINIMUM INSIDE DIAMETER OF 48 INCHES, AND SHALL BE CAST IN PLACE OR PRE-CAST REINFORCED CONCRETE.
- ALL SANITARY MANHOLES, (AND STORM MANHOLES IN COMBINED SEWER AREAS), SHALL HAVE PRECAST "RUBBER BOOTS" THAT CONFORM TO ASTM C-923 FOR ALL PIPE CONNECTIONS. PRECAST SECTIONS SHALL CONSIST OF MODIFIED GROOVE TONGUE AND RUBBER GASKET TYPE JOINTS.
- ALL ABANDONED SANITARY SEWERS SHALL BE PLUGGED AT BOTH ENDS WITH AT LEAST 2 FEET LONG NON-SHRINK CONCRETE OR MORTAR PLUG.
- EXCEPT FOR FOUNDATION/FOOTING DRAINS PROVIDED TO PROTECT BUILDINGS, OR PERFORATED PIPES ASSOCIATED WITH VOLUME CONTROL FACILITIES, DRAIN TILES/FIELD TILES/UNDERDRAINS/PERFORATED PIPES ARE NOT ALLOWED TO BE CONNECTED TO OR TRIBUTARY TO COMBINED SEWERS, SANITARY SEWERS, OR STORM SEWERS TRIBUTARY TO COMBINED SEWERS IN COMBINED SEWER AREAS. CONSTRUCTION OF NEW FACILITIES OF THIS TYPE IS PROHIBITED; AND ALL EXISTING DRAIN TILES AND PERFORATED PIPES ENCOUNTERED WITHIN THE PROJECT AREA SHALL BE PLUGGED OR REMOVED, AND SHALL NOT BE CONNECTED TO COMBINED SEWERS, SANITARY SEWERS, OR STORM SEWERS TRIBUTARY TO COMBINED SEWERS.
- A BACKFLOW PREVENTER IS REQUIRED FOR ALL DETENTION BASINS TRIBUTARY TO COMBINED SEWERS. REQUIRED BACKFLOW PREVENTERS SHALL BE INSPECTED AND EXERCISED ANNUALLY BY THE PROPERTY OWNER TO ENSURE PROPER OPERATION, AND ANY NECESSARY MAINTENANCES SHALL BE PERFORMED TO ENSURE FUNCTIONALITY. IN THE EVENT OF A SEWER SURCHARGE INTO AN OPEN DETENTION BASIN TRIBUTARY TO COMBINED SEWERS, THE PERMITTEE SHALL ENSURE THAT CLEAN UP AND WASH OUT OF SEWAGE TAKES PLACE WITHIN 48 HOURS OF THE STORM EVENT.

E. EROSION AND SEDIMENT CONTROL

- THE CONTRACTOR SHALL INSTALL THE EROSION AND SEDIMENT CONTROL DEVICES AS SHOWN ON THE APPROVED EROSION AND SEDIMENT CONTROL PLAN.
- EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE FUNCTIONAL PRIOR TO HYDROLOGIC DISTURBANCE OF THE SITE.
- ALL DESIGN CRITERIA, SPECIFICATIONS, AND INSTALLATION OF EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL.
- A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
- INSPECTIONS AND DOCUMENTATION SHALL BE PERFORMED, AT A MINIMUM:
 a) UPON COMPLETION OF INITIAL EROSION AND SEDIMENT CONTROL MEASURES, PRIOR TO ANY SOIL DISTURBANCE.
 b) ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A STORM EVENT WITH GREATER THAN 0.5 INCH OF RAINFALL OR LIQUID EQUIVALENT PRECIPITATION.
- SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. IF STRIPPING, CLEARING, GRADING, OR LANDSCAPING ARE TO BE DONE IN PHASES, THE CO-PERMITTEE SHALL PLAN FOR APPROPRIATE SOIL EROSION AND SEDIMENT CONTROL MEASURES.
- A STABILIZED MAT OF CRUSHED STONE MEETING THE STANDARDS OF THE ILLINOIS URBAN MANUAL SHALL BE INSTALLED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE. SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
- CONCRETE WASHOUT FACILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL AND SHALL BE INSTALLED PRIOR TO ANY ON SITE CONSTRUCTION ACTIVITIES INVOLVING CONCRETE.
- MORTAR WASHOUT FACILITIES SHALL BE CONSTRUCTED IN ADDITION TO CONCRETE WASHOUT FACILITIES FOR ANY BRICK AND MORTAR BUILDING ENVELOPE CONSTRUCTION ACTIVITIES.
- TEMPORARY DIVERSIONS SHALL BE CONSTRUCTED AS NECESSARY TO DIRECT ALL RUNOFF FROM HYDROLOGICALLY DISTURBED AREAS TO AN APPROPRIATE SEDIMENT TRAP OR BASIN. VOLUME CONTROL FACILITIES SHALL NOT BE USED AS TEMPORARY SEDIMENT BASINS.
- DISTURBED AREAS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) DAYS.
- ALL FLOOD PROTECTION AREAS AND VOLUME CONTROL FACILITIES SHALL, AT A MINIMUM, BE PROTECTED WITH A DOUBLE-ROW OF SILT FENCE (OR EQUIVALENT).
- VOLUME CONTROL FACILITIES SHALL NOT BE CONSTRUCTED UNTIL ALL OF THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED.
- SOIL STOCKPILES SHALL, AT A MINIMUM, BE PROTECTED WITH PERIMETER SEDIMENT CONTROLS. SOIL STOCKPILES SHALL NOT BE PLACED IN FLOOD PROTECTION AREAS OR THEIR BUFFERS.
- EARTHEN EMBANKMENT SIDE SLOPES SHALL BE STABILIZED WITH APPROPRIATE EROSION CONTROL BLANKET.
- STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY APPROPRIATE SEDIMENT CONTROL MEASURES.
- THE CONTRACTOR SHALL EITHER REMOVE OR REPLACE ANY EXISTING DRAIN TILES AND INCORPORATE THEM INTO THE DRAINAGE PLAN FOR THE DEVELOPMENT. DRAIN TILES CANNOT BE TRIBUTARY TO A SANITARY OR COMBINED SEWER. DRAIN TILES ALLOWED IN COMBINED SEWER AREA FOR GREEN INFRASTRUCTURE PRACTICES.
- IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION. DEWATERING SYSTEMS SHOULD BE INSPECTED DAILY DURING OPERATIONAL PERIODS. THE SITE INSPECTOR MUST BE PRESENT AT THE COMMENCEMENT OF DEWATERING ACTIVITIES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRENCH DEWATERING AND EXCAVATION FOR THE INSTALLATION OF SANITARY SEWERS, STORM SEWERS, WATERMANS AS WELL AS THEIR SERVICES AND OTHER APPURTENANCES. ANY TRENCH DEWATERING, WHICH CONTAINS SEDIMENT SHALL PASS THROUGH A SEDIMENT SETTLING POND OR EQUALLY EFFECTIVE SEDIMENT CONTROL DEVICE. ALTERNATIVES MAY INCLUDE DEWATERING INTO A SUMP PIT, FILTER BAG OR EXISTING VEGETATED UPSLOPE AREA. SEDIMENT LADEN WATERS SHALL NOT BE DISCHARGE TO WATERWAYS, FLOOD PROTECTION AREAS OR THE COMBINED SEWER SYSTEM.
- ALL PERMANENT EROSION CONTROL PRACTICES SHALL BE INITIATED WITHIN SEVEN (7) DAYS FOLLOWING THE COMPLETION OF SOIL DISTURBING ACTIVITIES.
- ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED AND REPAIRED AS NEEDED ON A YEAR-ROUND BASIS DURING CONSTRUCTION AND ANY PERIODS OF CONSTRUCTION SHUTDOWN UNTIL PERMANENT STABILIZATION IS ACHIEVED.
- ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (30) DAYS AFTER PERMANENT SITE STABILIZATION.
- THE EROSION AND SEDIMENT CONTROL MEASURES SHOWN ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER, SITE INSPECTOR, OR MWRD.

SUMMARY OF QUANTITIES

CODE NO.	ITEM	UNIT	PROJECT TOTAL	CONSTRUCTION CODE
			QUANTITY	0028 QUANTITY
51603000	DRILLED SHAFT IN SOIL	CU YD	10.5	10.5
58600101	GRANULAR BACKFILL FOR STRUCTURES	CU YD	70	70
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	25	25
60250200	CATCH BASINS TO BE ADJUSTED	EACH	1	1
60255500	MANHOLES TO BE ADJUSTED	EACH	1	1
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	150	150
Δ *66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	100	100
Δ *66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1
Δ *66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1
Δ *66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1
Δ *66901006	REGULATED SUBSTANCES MONITORING	CAL DA	5	5
67100100	MOBILIZATION	LSUM	1	1
Δ 72000100	SIGN PANEL - TYPE 1	SQ FT	5	5
Δ A2005042	TREE, GYMNOCLADUS DIOICUS ESPRESSO-JFS (ESPRESSO KENTUCKY COFFEETREE), 3" CALIPER, BALLED AND BURLAPPED	EACH	3	3
Δ A2002884	TREE, CELTIS OCCIDENTALIS CHICAGOLAND, (CHICAGOLAND HACKBERRY), 3" CALIPER, BALLED AND BURLAPPED	EACH	3	3
Δ A2007624	TREE, TAXODIUM DISTICHUM (COMMON BALD CYPRESS), 3" CALIPER, BALLED AND BURLAPPED	EACH	3	3
*Z0013797	STABILIZED CONSTRUCTION ENTRANCE	SQ YD	250	250

CODE NO.	ITEM	UNIT	PROJECT TOTAL	CONSTRUCTION CODE
			QUANTITY	0028 QUANTITY
*Z0013798	CONSTRUCTION LAYOUT	LSUM	1	1
*Z0019600	DUST CONTROL WATERING	UNIT	4	4
*Z0004002	BOLLARDS	EACH	2	2
***Z0046304	PIPE UNDERDRAINS FOR STRUCTURES, 4"	FOOT	55	55
*X0321322	DROP GATE	EACH	1	1
***X0322508	PEDESTRIAN TRUSS SUPERSTRUCTURE	SQ FT	3,640	3,640
*X0322924	RETAINING WALL REMOVAL	SQ FT	80	80
*X0326243	SEDIMENT CONTROL, SILT CURTAIN	LSUM	1	1
*X2070304	POROUS GRANULAR EMBANKMENT, SPECIAL	CU YD	70	70
Δ *X2501800	SEEDING, CLASS 4 (MODIFIED)	ACRE	0.25	0.25
Δ *X2502019	SEEDING, CLASS 4B (SPECIAL)	ACRE	0.25	0.25
*X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1
*XX006658	FLOCCULATION LOGS	EACH	2	2
*XX006659	FLOCCULATION POWDER	POUND	100	100
*XX008287	BOARDWALK STRUCTURE	SQ FT	650	650
*XX009318	LIMESTONE SCREENING SURFACE	TON	500	500

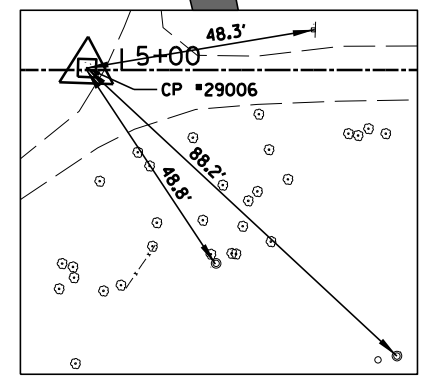
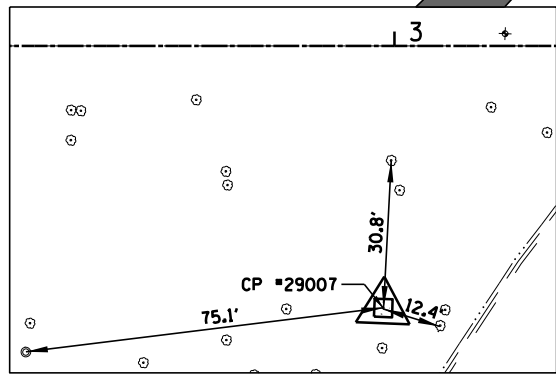
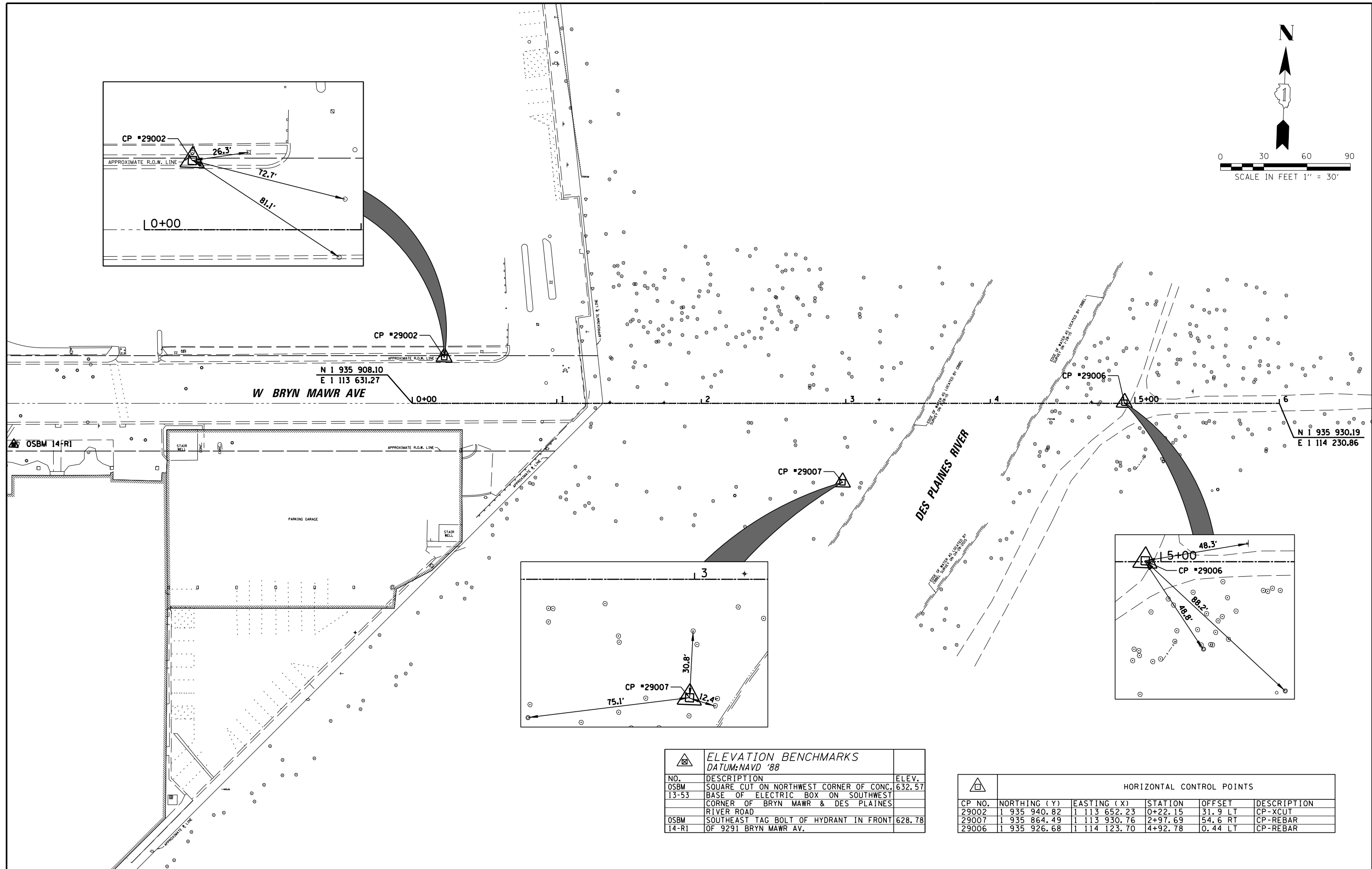
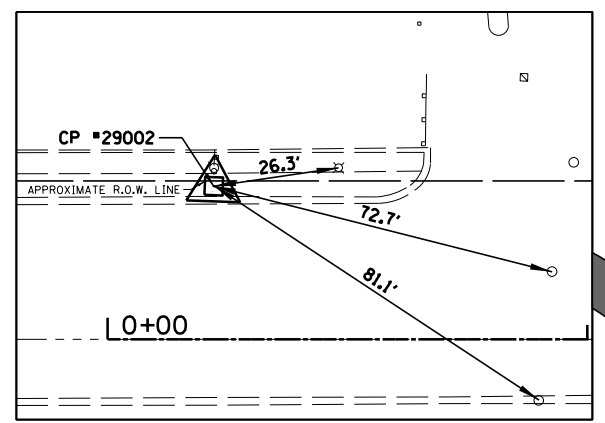
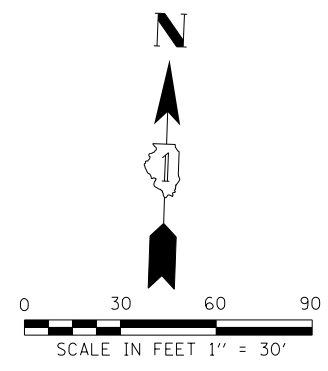
*INDICATES SPECIAL PROVISION

Δ SPECIALTY ITEMS

**INDICATES SUPPLEMENTAL SPECIFICATION

***INDICATES GUIDE BRIDGE SPECIAL PROVISION

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		DRAWN -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	COOK	36	6
		CHECKED -	REVISED -		CONTRACT NO. 61051								
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT								



ELEVATION BENCHMARKS DATUM: NAVD '88		
NO.	DESCRIPTION	ELEV.
OSBM 13-53	SQUARE CUT ON NORTHWEST CORNER OF CONC. BASE OF ELECTRIC BOX ON SOUTHWEST CORNER OF BRYN MAWR & DES PLAINES RIVER ROAD	632.57
OSBM 14-R1	SOUTHEAST TAG BOLT OF HYDRANT IN FRONT OF 9291 BRYN MAWR AV.	628.78

HORIZONTAL CONTROL POINTS					
CP NO.	NORTHING (Y)	EASTING (X)	STATION	OFFSET	DESCRIPTION
29002	1 935 940.82	1 113 652.23	0+22.15	31.9 LT	CP-XCUT
29007	1 935 864.49	1 113 930.76	2+97.69	54.6 RT	CP-REBAR
29006	1 935 926.68	1 114 123.70	4+92.78	0.44 LT	CP-REBAR

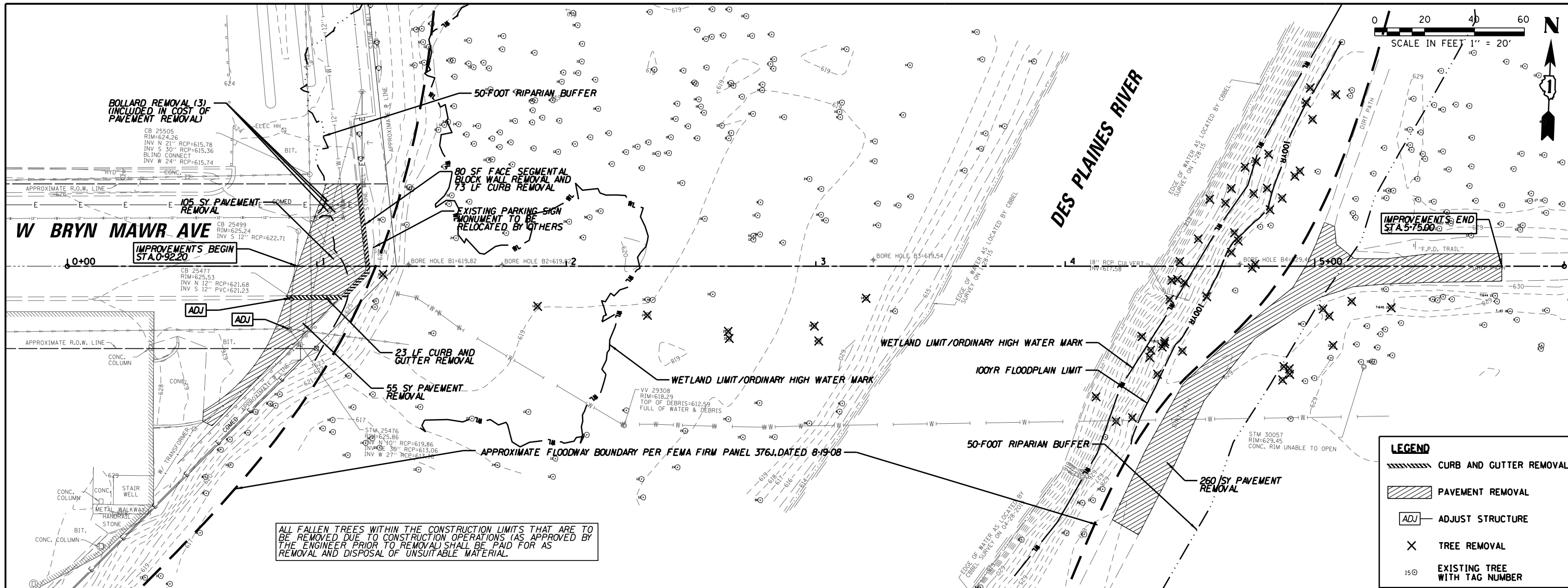
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Default	PLOT SCALE = 68'	DRAWN -	REVISED -
	PLOT DATE = 3/19/2020	CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BRYN MAWR BRIDGE AND RIVERWALK
ALIGNMENT, TIES, AND BENCHMARKS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. - RTE. -	SECTION 19-00036-00-BR	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 7
CONTRACT NO. 61G51				
ILLINOIS FED. AID PROJECT				



ALL FALLEN TREES WITHIN THE CONSTRUCTION LIMITS THAT ARE TO BE REMOVED DUE TO CONSTRUCTION OPERATIONS (AS APPROVED BY THE ENGINEER PRIOR TO REMOVAL) SHALL BE PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL.

Removed	Tag #	Species	Size (in)	Condition	Form	Comments	Removed	Tag #	Species	Size (in)	Condition	Form	Comments	Removed	Tag #	Species	Size (in)	Condition	Form	Comments	Removed	Tag #	Species	Size (in)	Condition	Form	Comments
	37	Silver maple	36	3	3			81	American elm	3	3	3		YES	125	Buckthorn	3	3	3		YES	169	Buckthorn	4	3	3	
	38	Silver maple	12	3	4	Lean		82	American elm	3	3	3		YES	126	Buckthorn	3	3	3		YES	170	Green ash	12	3	4	Lean
	39	American elm	8	3	3			83	Cottonwood	18	2	2		YES	127	Buckthorn	3	3	3		YES	171	Buckthorn	4	3	3	
	40	Green ash	16	3	3			84	American elm	3	3	3		YES	128	Box elder	3	3	3		YES	172	Ironwood	6	2	2	
	41	Silver maple	10	3	3			85	American elm	6	3	3		YES	129	Green ash	10	3	3		YES	173	Black cherry	7	3	3	
	42	American elm	8	3	4			86	Cottonwood	10	2	2		YES	130	Green ash	5	3	4	Lean	YES	174	Green ash	10	3	3	
	43	American elm	3	3	3			87	Cottonwood	20	2	2		YES	131	Box elder	6	3	3		YES	175	American elm	10	3	3	
	44	Cottonwood	18	2	2			88	American elm	4	3	3		YES	132	Buckthorn	3	3	4	Multiple stems	YES	176	American elm	18	2	2	
	45	Silver maple	12	2	3			89	American elm	8	3	3		YES	133	Buckthorn	3	3	4	Multiple stems	YES	177	American elm	8	3	3	
	46	Cottonwood	18	2	2			90	American elm	6	3	3		YES	134	Buckthorn	4	3	4	Multiple stems	YES	178	Ironwood	8	2	2	
	47	Silver maple	6	3	4			91	American elm	6	3	3		YES	135	Black cherry	4	3	4	Deadwood	YES	179	Basswood	4	3	3	
	48	American elm	3	3	3			92	American elm	6	3	3		YES	136	Red oak	12	2	2		YES	180	Black cherry	9	3	4	Lean
	49	American elm	12	3	3			93	American elm	10	3	3		YES	137	Buckthorn	3	3	4		YES	181	White oak	7	2	2	
	50	American elm	8	3	3			94	American elm	3	3	3		YES	138	Green ash	8	3	3		YES	182	Green ash	15	3	4	Deadwood
	51	Silver maple	10	3	3			95	Cottonwood	24	2	2		YES	139	Buckthorn	5	3	4	Multiple stems	YES	183	Basswood	12	2	2	
	52	Cottonwood	18	2	2			96	American elm	8	3	3		YES	140	Buckthorn	3	3	4		YES	184	Green ash	5	3	3	
	53	Cottonwood	16	2	3			97	Cottonwood	24	2	2		YES	141	Buckthorn	3	3	4	Multiple stems	YES	185	Green ash	8	3	3	
	54	American elm	4	3	3			98	American elm	8	3	3		YES	142	Hackberry	3	2	3		YES	186	Buckthorn	3	3	4	
	55	American elm	8	3	3			99	Cottonwood	24	3	2		YES	143	American elm	5	3	4	Lean	YES	187	Buckthorn	4	3	4	
	56	American elm	5	3	3			100	American elm	12	2	2		YES	144	Dead	5	5	5	Dead	YES	188	Black cherry	15	3	3	
	57	American elm	6	3	3			101	American elm	6	3	3		YES	145	Buckthorn	5	3	4	Multiple stems	YES	189	Buckthorn	4	3	3	
	58	Cottonwood	18	2	2			102	American elm	6	3	3		YES	146	Hackberry	5	2	3		YES	190	Red oak	12	2	2	
	59	Cottonwood	18	2	2			103	Green ash	12	3	4	Deadwood	YES	147	Basswood	6	3	4	Lean	YES	191	Red oak	12	2	2	
	60	American elm	12	3	3			104	American elm	8	3	3		YES	148	Basswood	12	3	3		YES	192	Buckthorn	3	3	3	
	61	Cottonwood	20	2	2			105	Buckthorn	6	3	3		YES	149	Hackberry	4	2	2		YES	193	Buckthorn	3	3	3	
	62	American elm	3	3	3			106	American elm	6	3	3		YES	150	Buckthorn	3	3	4	Multiple stems	YES	194	Buckthorn	5	3	4	
	63	American elm	3	3	4			107	Green ash	24	3	4	Multiple stems	YES	151	Buckthorn	3	3	4	Multiple stems	YES	195	Green ash	18	3	4	Lean
	64	American elm	10	3	4	Lean		108	Green ash	10	3	3		YES	152	Buckthorn	3	3	4	Multiple stems	YES	196	Black cherry	12	3	4	Lean
	65	American elm	6	3	3			109	Box elder	12	3	3		YES	153	Buckthorn	3	3	3		YES	197	American elm	10	3	3	
	66	American elm	4	3	3			110	Box elder	18	3	4	Lean	YES	154	Red oak	18	2	2		YES	198	American elm	12	3	3	
	67	American elm	6	3	3			111	Green ash	36	3	4	Multiple stems	YES	155	Basswood	12	2	3		YES	199	Black cherry	11	3	3	
	68	Cottonwood	18	2	2			112	Blue spruce	3	2	2		YES	156	Red oak	19	2	2		YES	200	Black cherry	12	3	3	
	69	American elm	4	3	3			113	Blue spruce	3	2	2		YES	157	Green ash	4	3	3		YES	201	Black cherry	12	3	3	
	70	American elm	3	3	3			114	American elm	6	3	4	Multiple stems	YES	158	American elm	5	2	2		YES	202	Buckthorn	3	3	4	Multiple stems
	71	American elm	3	3	3			115	Green ash	4	3	3		YES	159	Buckthorn	4	3	4	Multiple stems	YES	203	American elm	6	3	3	
	72	American elm	6	2	3			116	Green ash	3	3	3		YES	160	Buckthorn	3	3	4		YES	204	Red oak	12	2	2	
	73	Green ash	15	4	4	Deadwood		117	Buckthorn	3	3	4	Multiple stems	YES	161	Buckthorn	3	3	4		YES	205	Black cherry	3	3	4	
	74	Green ash	36	3	4	Multiple stems		118	Buckthorn	3	3	4		YES	162	Buckthorn	3	3	4		YES	206	Black walnut	15	2	2	
	75	Buckthorn	8	3	4			119	Silver maple	5	2	2		YES	163	Buckthorn	3	3	4		YES	207	American elm	5	3	3	
	76	Box elder	26	4	4	Deadwood		120	Buckthorn	4	3	4	Multiple stems	YES	164	Buckthorn	4	3	4	Multiple stems	YES	208	Black cherry	11	3	3	
	77	American elm	3	3	3			121	Buckthorn	3	3	4	Multiple stems	YES	165	Buckthorn	3	3	4	Multiple stems	YES	209	Black cherry	8	3	3	
	78	American elm	12	3	3			122	Dead	11	5	5	Dead	YES	166	Buckthorn	2	3	4	Multiple stems	YES	210	American elm	8	3	3	
	79	American elm	3	3	3			123	Buckthorn	4	3	4	Multiple stems	YES	167	Black cherry	12	3	4	Lean	YES	211	Black cherry	13	3	3	
	80	American elm	3	3	3			124	Box elder	12	4	4	Deadwood	YES	168	Bur oak	36	2	2		YES	212	Buckthorn	5	3	4	Lean



STAGING AREA (CONTRACTOR TO BARRICADE OFF CENTER 18' OF BRYN MAWR AVE. 11' LANES MUST BE MAINTAINED EAST BOUND AND WEST BOUND AND MAINTAIN ALL ACCESS)

BRIDGE CRANE ACCESS FROM BRYN MAWR ONLY

TREE TRIMMING / TREE REMOVAL TO BE APPROVED BY FPCC

EXISTING TRAIL SYSTEM

TEMPORARY FENCE TO SURROUND CONSTRUCTION ENTRANCE

TRAIL CLOSED
R11-2 (MODIFIED)
24x30

PROPOSED ACCESS BEGINS AT E. RIVER ROAD

STAGING AREA TO BE FENCED OFF FROM PUBLIC

EXISTING TRAIL SYSTEM

PROPOSED 10' ACCESS

PATH TO BE RESTORED TO EXISTING CONDITIONS AFTER CONSTRUCTION PER LIMESTONE TRAIL TYPICAL SECTION (REPAIR PAID FOR AS LIMESTONE SCREENING SURFACE AND AGGREGATE BASE COURSE TYPE B 8")

STABILIZED CONSTRUCTION ENTRANCE WITH TREE PROTECTION MATTING ALONG THE EXISTING WOOD CHIP PATH

ANY DAMAGE TO THE EXISTING PARKING LOT PAVEMENT OR CURB AND GUTTER SHALL BE REPAIRED TO THE SATISFACTION OF THE FPCC

TEMPORARY RAMP/STABILIZED CONSTRUCTION ENTRANCE

PROPOSED 10' ACCESS

EXISTING TRAIL SYSTEM

DETOUR
M4-9A-L
30x24

DETOUR
M4-9A-R
30x24

TRAIL CLOSED
R11-2 (MODIFIED)
24x30

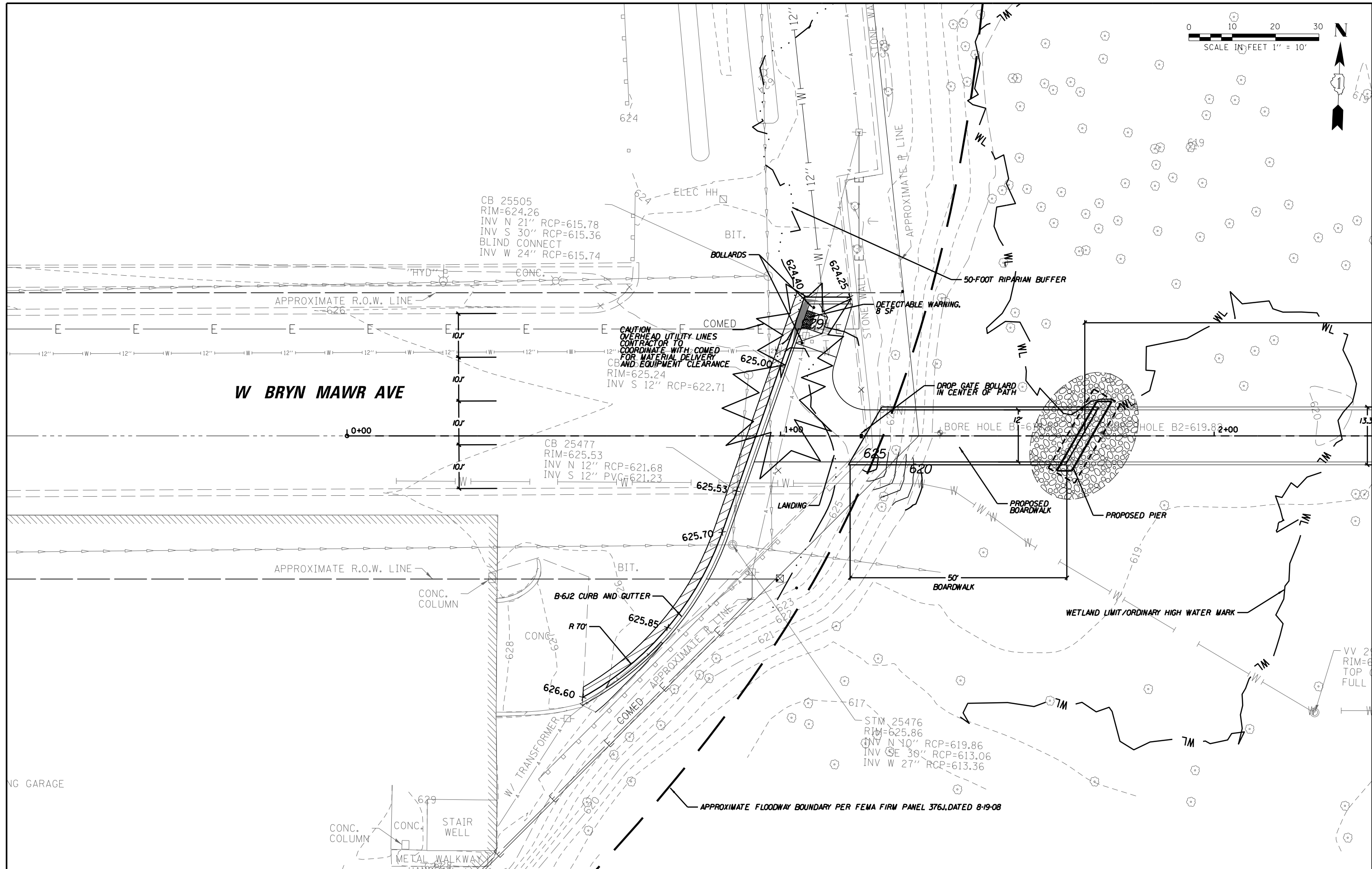
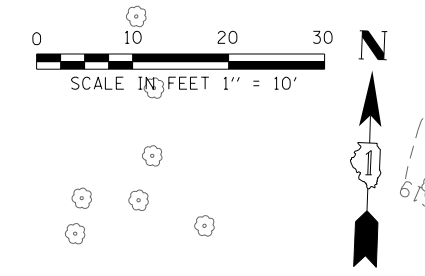
END DETOUR
M4-8A
24x18

DETOUR
M4-9A-R
30x24

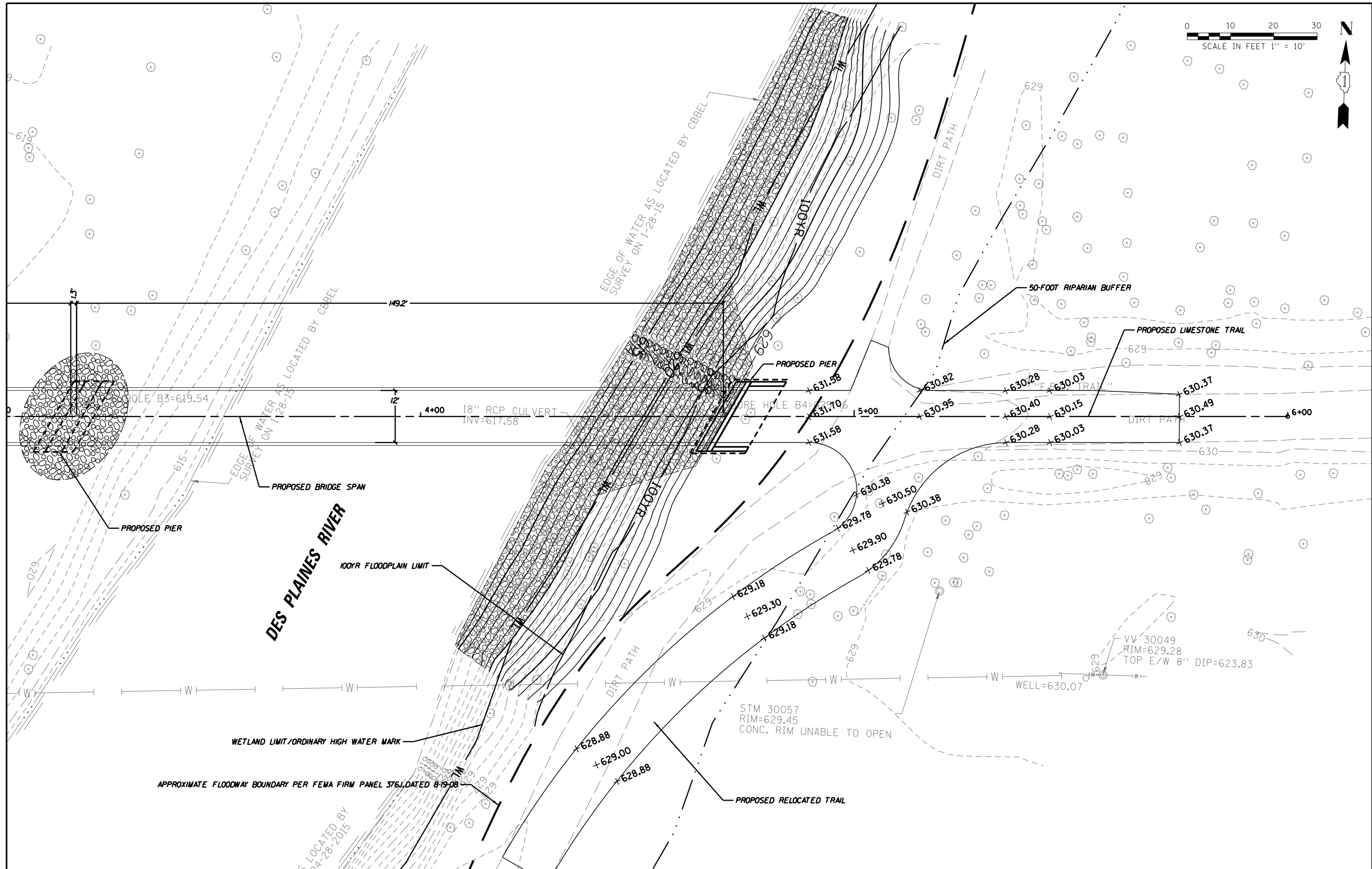
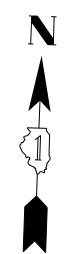
DO NOT ENTER
R5-1
30x30

- NOTES:
- EXISTING PATH TO BE REPAIRED AS DETERMINED IN THE FIELD BY THE ENGINEER.
 - ALL DETOUR SIGNAGE SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION, (SPECIAL).

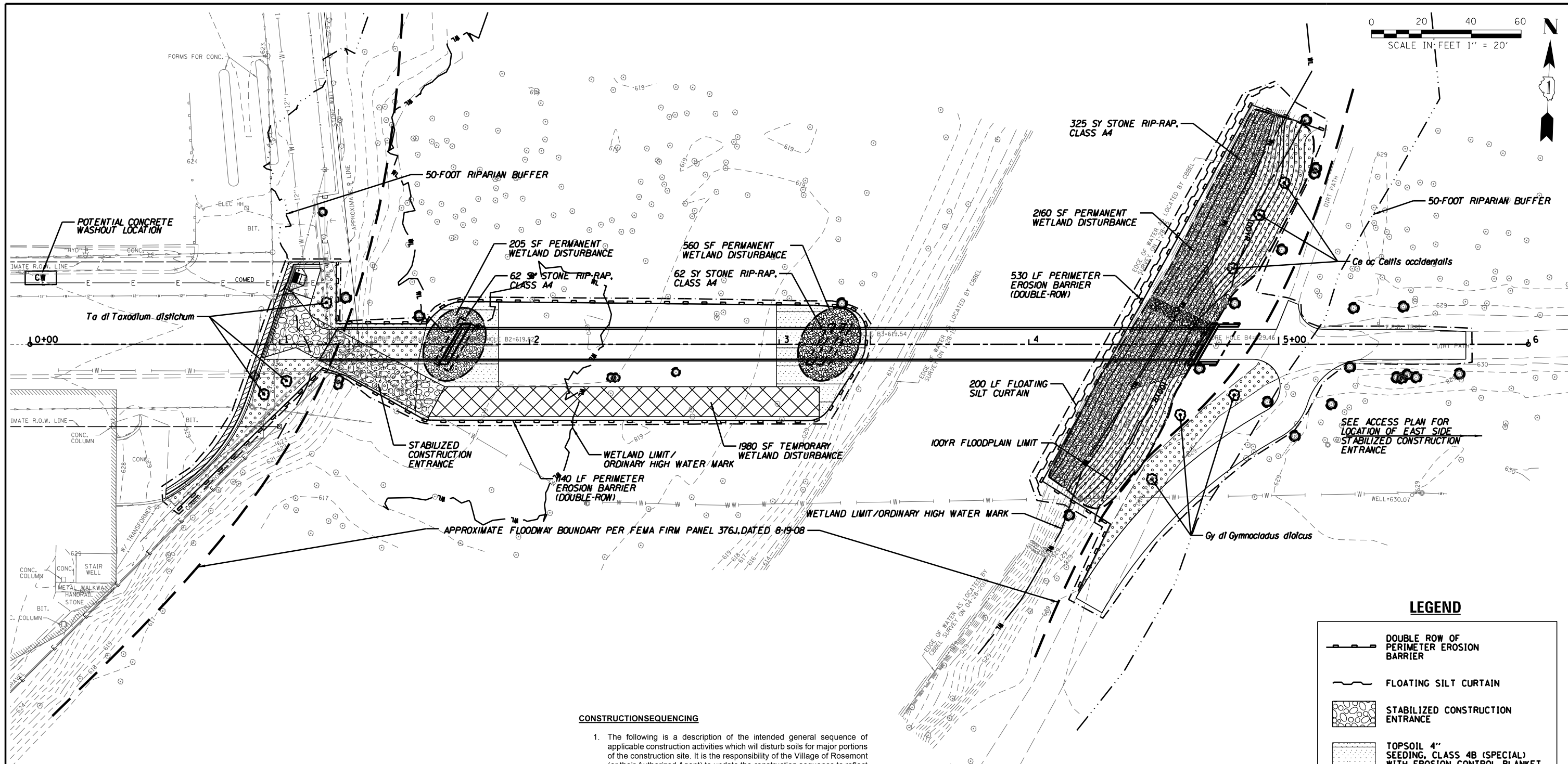
FILE NAME =	USER NAME = jlepola	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BRYN MAWR BRIDGE AND RIVERWALK PROPOSED ACCESS PLAN AND PEDESTRIAN DETOUR ROUTE	F.A. -	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\ROSEMONT\14019\Civil\04_ACCESS.14019.SHT		DRAWN -	REVISED -			-	19-00036-00-BR	COOK	36	10	
Default		CHECKED -	REVISED -			CONTRACT NO. 61G51					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
	PLOT DATE = 3/19/2020			SCALE:	SHEET OF SHEETS	STA. TO STA.					



FILE NAME =	USER NAME = jlepeglia	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BRYN MAWR BRIDGE AND RIVERWALK PROPOSED GRADING PLAN - WEST OF DES PLAINES RIVER			F.A. -	SECTION	COUNTY	TOTAL	SHEET
N:\ROSEMONT\14019\Civil\01_GRA_14019.dwg		DRAWN -	REVISED -					RTÉ.	19-00036-00-BR	COOK	36	11
Default	PLOT SCALE = 28'	CHECKED -	REVISED -		CONTRACT NO. 61G51			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 3/19/2020	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	



FILE NAME = N:\ROSEMONT\14019\Civil\02.GRA\14019.plt	USER NAME = jlepaglia	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BRYN MAWR BRIDGE AND RIVERWALK PROPOSED GRADING PLAN - EAST OF DES PLAINES RIVER	F.A. -	SECTION	COUNTY	TOTAL	SHEET	
Default	PLOT SCALE = 28'	DRAWN -	REVISED -			-	19-00036-00-BR	COOK	36	12	
	PLOT DATE = 3/19/2020	CHECKED -	REVISED -			CONTRACT NO. 61G51					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.



CONSTRUCTION SEQUENCING

1. The following is a description of the intended general sequence of applicable construction activities which will disturb soils for major portions of the construction site. It is the responsibility of the Village of Rosemont (or their Authorized Agent) to update the construction sequence to reflect the current stage or phasing of construction activities.
 - i. Install temporary erosion and sediment control measures;
 - a. Perimeter silt fence and silt curtain installation;
 - b. Installation of stabilized construction entrance(s);
 - c. Installation of wood matting;
 - d. Installation of inlet protection in existing storm sewer structure(s), if present;
 - e. Designation of stabilized contractor equipment storage yard(s).
 - ii. Tree and vegetation clearing/cutting.
 - iii. Minor grading, excavation, and soil stockpiling on matting or outside of Flood Protection Areas.
 - iv. Temporary stabilization of areas that will remain idle for 14 days within 1 day of disturbance cessation (including soil stockpiles).
 - v. Construct concrete washout containment facilities.
 - vi. Bridge pier and boardwalk pier construction.
 - vii. Bridge and boardwalk construction.
 - viii. Paving.
 - ix. Install permanent or temporary soil stabilization, landscaping, and vegetative enhancements.
 - x. Remove all temporary soil erosion and sediment control measures after the site is stabilized with vegetation.

NOTES:

1. COLLECTED SEDIMENT SHALL BE DISPOSED OF AT AN OFF-SITE DUMP FACILITY.
2. EROSION CONTROL BLANKET SHALL BE TENSAR NORTH AMERICAN GREEN BIONET S75BN (OR APPROVED EQUAL) ON ALL SLOPES FLATTER THAN 4:1 AND TENSAR NORTH AMERICAN GREEN BIONET S150BN (OR APPROVED EQUAL) ON ALL SLOPES GREATER THAN 4:1.
3. EROSION CONTROL BLANKET SHALL BE WILDLIFE FRIENDLY.

LEGEND

	DOUBLE ROW OF PERIMETER EROSION BARRIER
	FLOATING SILT CURTAIN
	STABILIZED CONSTRUCTION ENTRANCE
	TOPSOIL 4" SEEDING, CLASS 4B (SPECIAL) WITH EROSION CONTROL BLANKET
	TOPSOIL 4" SEEDING, CLASS 4 (MODIFIED) WITH EROSION CONTROL BLANKET
	WOOD MATTING FOR ACCESS ROUTE TOPSOIL 4" SEEDING, CLASS 4B (SPECIAL) WITH EROSION CONTROL BLANKET
	CONCRETE WASHOUT
	INLET FILTER
	TREE PROTECTION (PAID AS TEMPORARY FENCE)
	PROPOSED TREE

FILE NAME = N:\ROSEMONT\14019\Civil\01.ERO.14019.SHT	USER NAME = jlepeglia	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BRYN MAWR BRIDGE AND RIVERWALK EROSION CONTROL AND LANDSCAPE PLAN	F.A. RT.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	PLOT SCALE = 48"	DRAWN -	REVISED -			-	19-00036-00-BR	COOK	36	13	
	PLOT DATE = 3/19/2020	CHECKED -	REVISED -			CONTRACT NO. 61G51					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

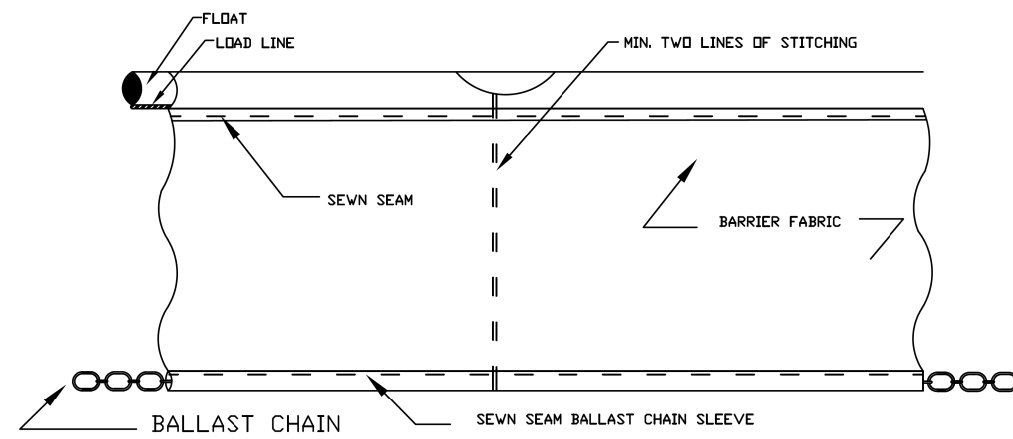
SEEDING, CLASS 4B (SPECIAL)

Scientific Name	Common Name	Lb/Acre
Bromus latiglumis	ear leaved brome	0.400
Cinna arundianacea	common wood reed	0.500
Diarrhena americana	beak grass	0.031
Elymus riparius	riverbank rye	1.000
Elymus villosus	silky wild rye	0.768
Elymus virginicus	Virginia wild rye	2.000
Festuca obtusa	nodding fescue	0.150
Glyceria striata	fowl manna grass	0.500
Impatiens capensis	orange jewelweed	0.250
Actinomeris alternifolia	wingstem	0.620
Aquilegia canadensis	wild columbine	0.620
Aster lateriflorus	side flowering aster	0.620
Aster shortii	Short's aster	0.150
Boehmeria cylindrica	false nettle	0.125
Campanula americana	tall bellflower	0.060
Carex blanda	common wood sedge	0.062
Eupatorium rugosum	white snake root	0.150
Leersia virginica	white grass	0.250
Penstemon digitalis	foxglove beard tongue	0.150
Phlox divaricata	blue phlox	0.150
Pilea pumila	clearweed	0.150
Polygonatum canaliculatum	smooth Solomon's seal	0.150
Pycnanthemum virginianum	mountain mint	0.150
Ratibida pinnata	yellow coneflower	0.125
Rudbeckia lacinata	wild goldenglow	0.310
Solidago flexicaulis	broadleaved goldenrod	0.031
Solidago gigantea	old field goldenrod	0.031
Solidago ulmifolia	elm leaved goldenrod	0.125
Veronicastrum virginicum	Culver's root	0.310
Zizia aurea	golden alexanders	0.062
	Total Weight of Seeds (lbs)	10.000
Lolium multiflorum	annual rye	4.000

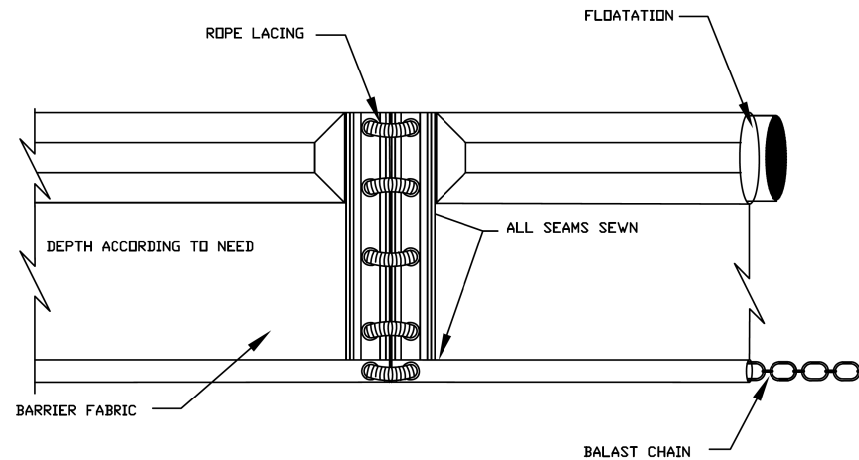
SEEDING, CLASS 4 (MODIFIED)

Scientific Name	Common Name	Lb/Acre
Andropogon gerardii	Big blue stem	4.000
Andropogon scoparius	Little blue stem	5.000
Bouteloua curtipendula	Side-oats gramma	5.000
Elymus canadensis	Wild rye	1.000
Panicum virgatum	Switch grass	1.000
Sorghastrum nutans	Indian grass	2.000
Sporobolus heterolepis	Prairie dropseed	0.500
	Cover Crop:	
	Annual ryegrass	25.000
	Oats, Spring	25.000
	Perennial ryegrass	15.000

FLOATING SILT CURTAIN - PANEL CONNECTORS



SEWN SEAM



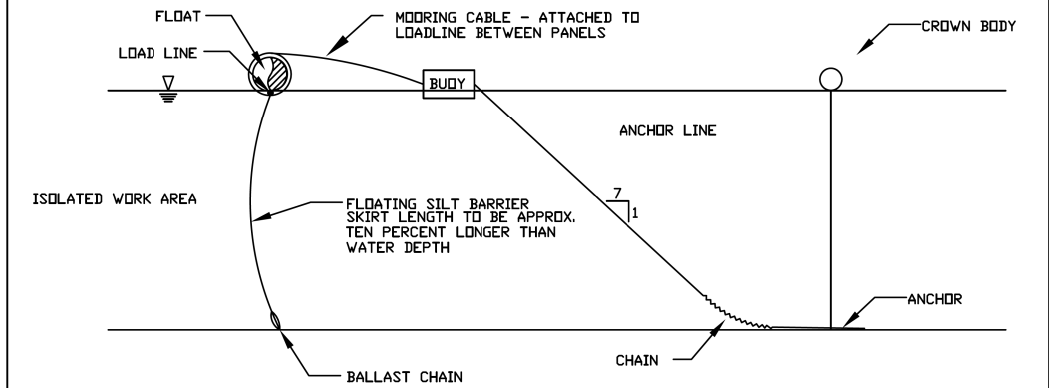
GROMMETED HOLES WITH ROPE LACING

REFERENCE
Project _____
Designed _____ Date _____
Checked _____ Date _____
Approved _____ Date _____

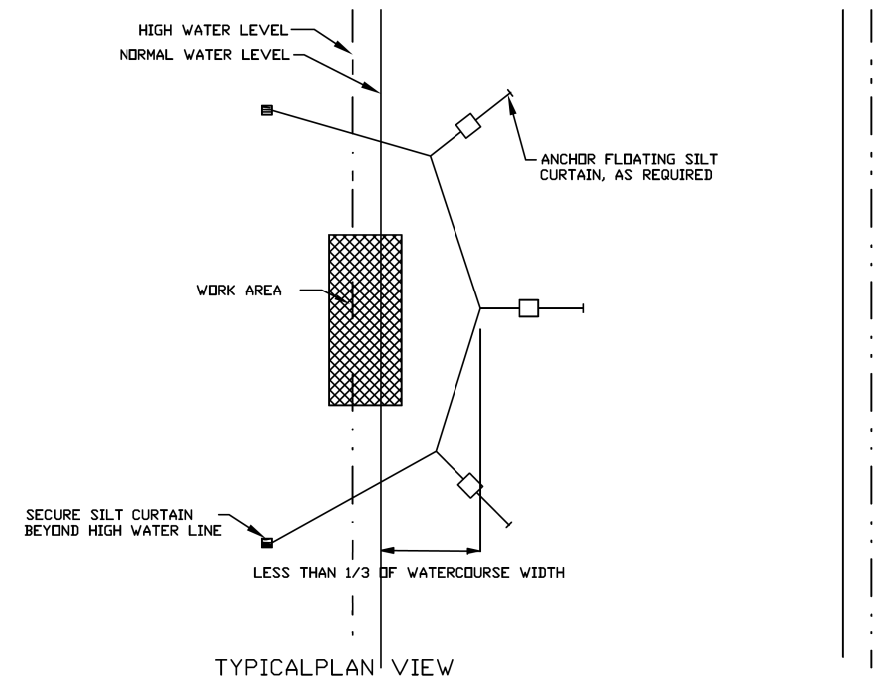


STANDARD DWG. NO.
IUM-617B
SHEET 1 OF 1
DATE 1-6-2012

FLOATING SILT CURTAIN - TYPICAL LAYOUT



TYPICAL COMPONENTS / ANCHORAGE SYSTEM



TYPICAL PLAN VIEW

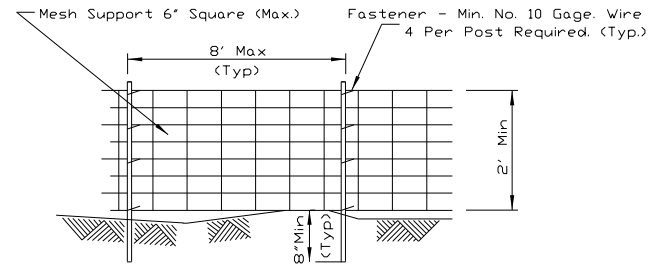
Maximum flow for waterbody shall be less than 5fps.
Isolated work area shall not exceed more than 1/3 stream width.
Silt curtain shall be placed parallel to stream flow.

REFERENCE
Project _____
Designed _____ Date _____
Checked _____ Date _____
Approved _____ Date _____

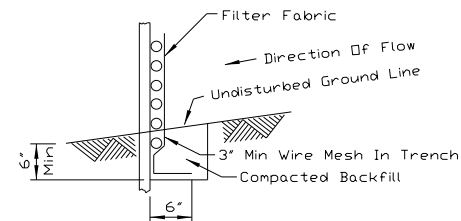


STANDARD DWG. NO.
IUM-617A
SHEET 1 OF 1
DATE 1-6-2012

SILT FENCE WITH WIRE SUPPORT PLAN



ELEVATION



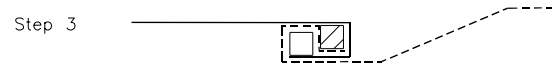
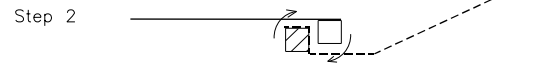
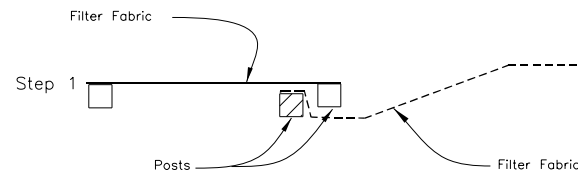
FABRIC ANCHOR DETAIL

NOTES:

1. Wires of mesh support shall be min. gage no. 12.
2. Temporary sediment fence shall be installed prior to any grading work in the area to be protected. They shall be maintained throughout the construction period and removed in conjunction with the final grading and site stabilization.
3. Filter fabric shall meet the requirements of material specification 592 Geotextile Table 1 or 2, Class I with equivalent opening size of at least 30 for nonwoven and 50 for woven.
4. Fence posts shall be either standard steel post or wood post with a minimum cross-sectional area of 3.0 sq. in.

REFERENCE Project	DESIGNED _____ Date _____	CHECKED _____ Date _____	APPROVED _____ Date _____	STANDARD DWG. NO. IL-620W	SHEET 1 OF 2	DATE 3-3-95
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SILT FENCE

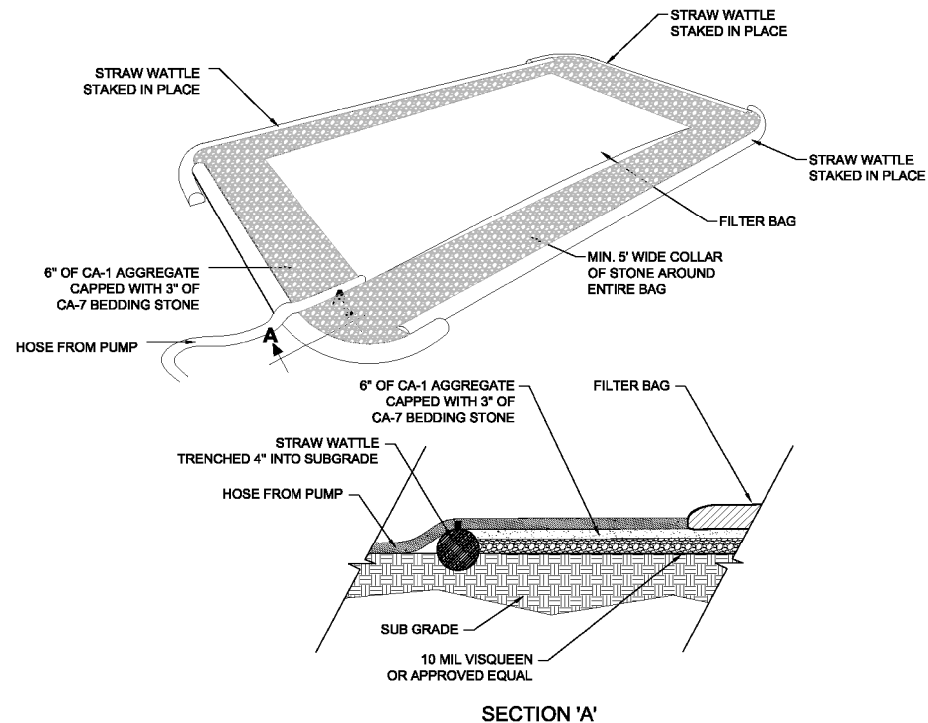


ATTACHING TWO SILT FENCES

NOTES:

1. Place the end post of the second fence inside the end post of the first fence.
2. Rotate both posts at least 180 degrees in a clockwise direction to create a tight seal with the fabric material.
3. Drive both posts a minimum of 18 inches into the ground and bury the flap.

REFERENCE Project	DESIGNED _____ Date _____	CHECKED _____ Date _____	APPROVED _____ Date _____	STANDARD DWG. NO. IL-620(W)	SHEET 2 OF 2	DATE 1-29-99
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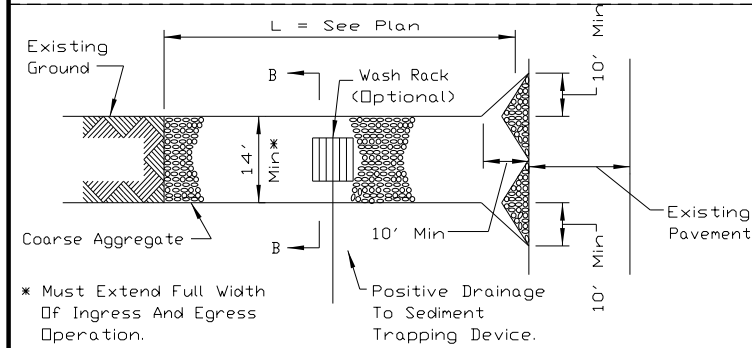


SECTION 'A'

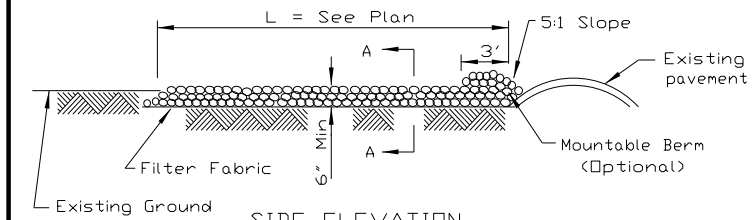
- NOTES:
 (1) ACTUAL SIZE AND LAYOUT DETERMINED IN THE FIELD
 (2) PUMP INTAKE HEAD SHOULD BE FLOATED AT SURFACE OR PLACED IN A STABILIZED SUMP PIT

DEWATERING FILTER PAD

STABILIZED CONSTRUCTION ENTRANCE PLAN



PLAN VIEW



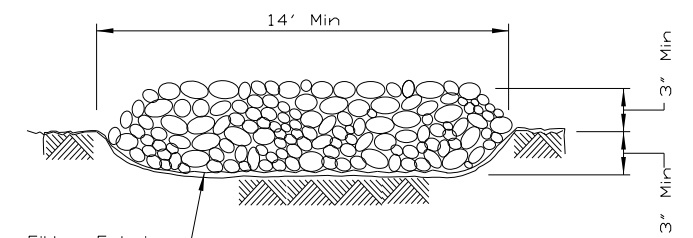
SIDE ELEVATION

NOTES:

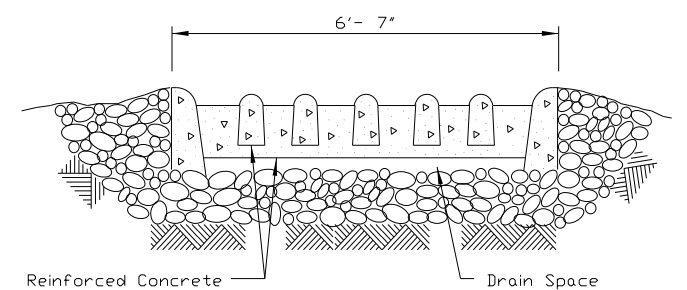
1. Filter fabric shall meet the requirements of material specification 592 GEOTEXTILE, Table 1 or 2, Class I, II or IV and shall be placed over the cleared area prior to the placing of rock.
2. Rock or reclaimed concrete shall meet one of the following IDOT coarse aggregate gradation, CA-1, CA-2, CA-3 or CA-4 and be placed according to construction specification 25 ROCKFILL using placement Method 1 and Class III compaction.
3. Any drainage facilities required because of washing shall be constructed according to manufacturers specifications.
4. If wash racks are used they shall be installed according to the manufacturer's specifications.

REFERENCE Project	DESIGNED _____ Date _____	CHECKED _____ Date _____	APPROVED _____ Date _____	STANDARD DWG. NO. IL-630	SHEET 1 OF 2	DATE 8-18-94
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STABILIZED CONSTRUCTION ENTRANCE PLAN



SECTION A-A



SECTION B-B

REFERENCE Project	DESIGNED _____ Date _____	CHECKED _____ Date _____	APPROVED _____ Date _____	STANDARD DWG. NO. IL-630	SHEET 2 OF 2	DATE 8-18-94
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Default	PLOT SCALE = 48"	CHECKED -	REVISED -
	PLOT DATE = 3/19/2020	DATE -	REVISED -

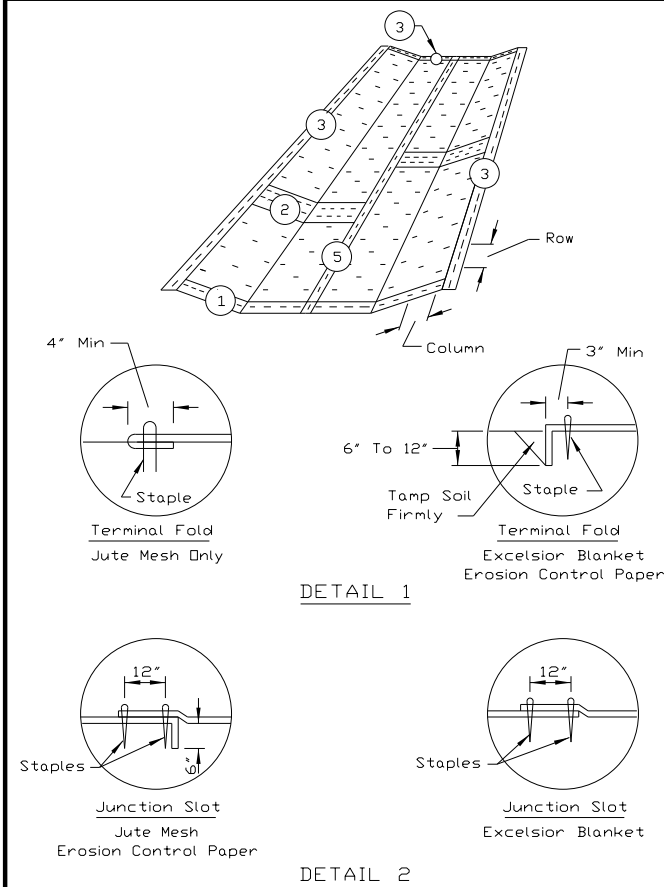
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BRYN MAWR BRIDGE AND RIVERWALK
EROSION CONTROL DETAILS

SCALE: SHEET OF SHEETS STA. TO STA.

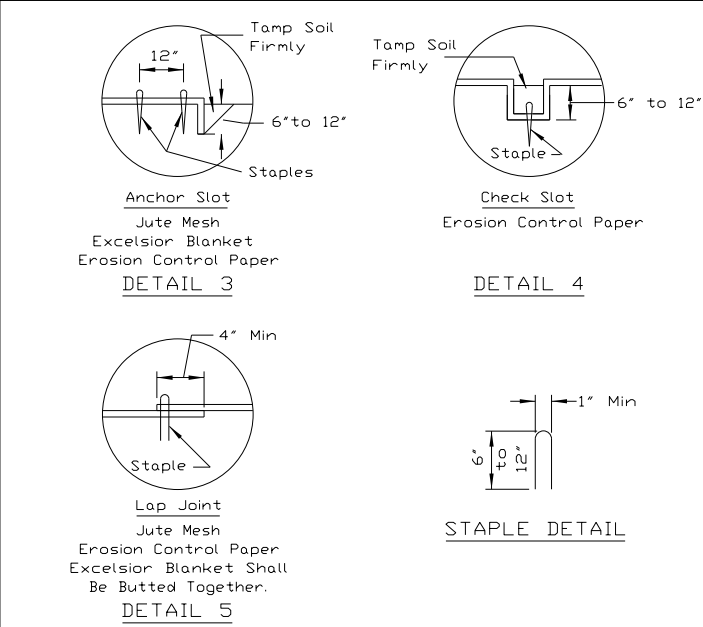
F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	19-00036-00-BR	COOK	36	15
CONTRACT NO. 61G51				
ILLINOIS FED. AID PROJECT				

EROSION BLANKET PLAN



REFERENCE		STANDARD DWG. NO.
Project		IL-530
Designed		SHEET 1 OF 2
Checked		DATE 5-24-94

EROSION BLANKET PLAN

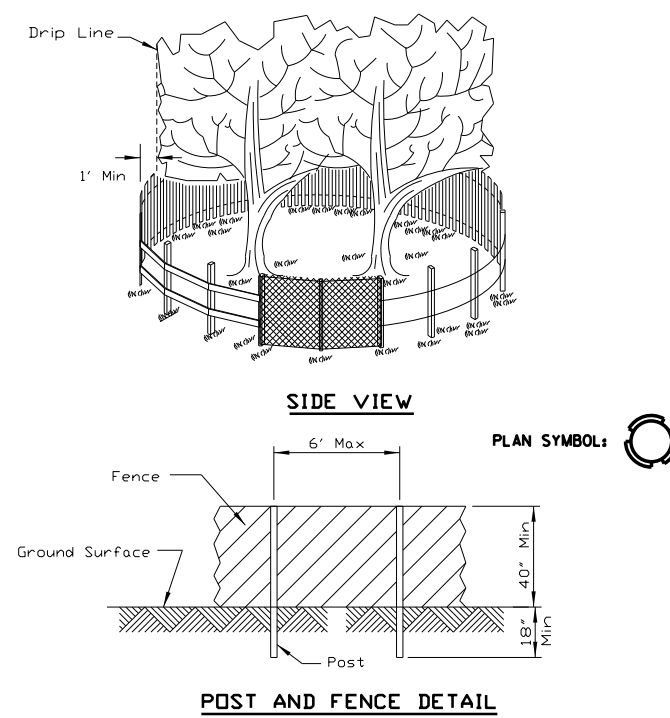


NOTES:

- On erosion control paper, check slots, in ditch channel shall be spaced so that one occurs within each 50' on slopes of more than 4% and less than 6%. On slopes of 6% or more, they shall be spaced so that one occurs within each 25'.
- Staples are to be placed alternately, in columns approximately 2' apart and in rows approximately 3' apart. Approximately 175 staples are required per 4'x 225' roll of material and 125 staples are required per 4'x 150' roll of material.
- Erosion control material shall be placed loosely over ground surface. Do not stretch.
- All terminal ends and transverse laps shall be stapled at approximately 12" intervals.

REFERENCE		STANDARD DWG. NO.
Project		IL-530
Designed		SHEET 2 OF 2
Checked		DATE 3-1-95

TREE PROTECTION - FENCING

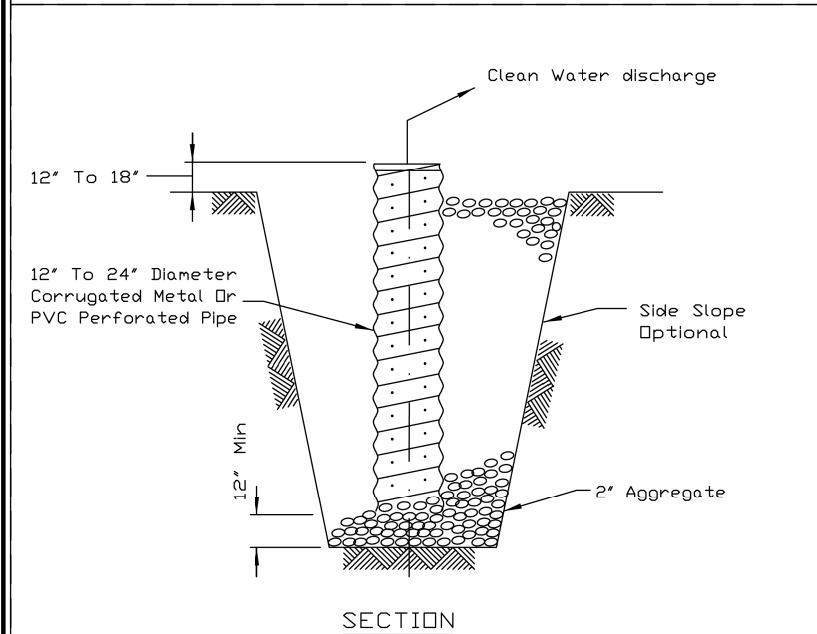


NOTES:

- The fence shall be located a minimum of 1 foot outside the drip line of the tree to be saved and in no case closer than 5 feet to the trunk of any tree.
- Fence posts shall be either standard steel posts or wood posts with a minimum cross sectional area of 3.0 sq. in.
- The fence may be either 40" high snow fence, 40" plastic web fencing or any other material as approved by the engineer/inspector.

REFERENCE		STANDARD DWG. NO.
Project		IL-690
Designed		SHEET 1 OF 1
Checked		DATE 4-7-94

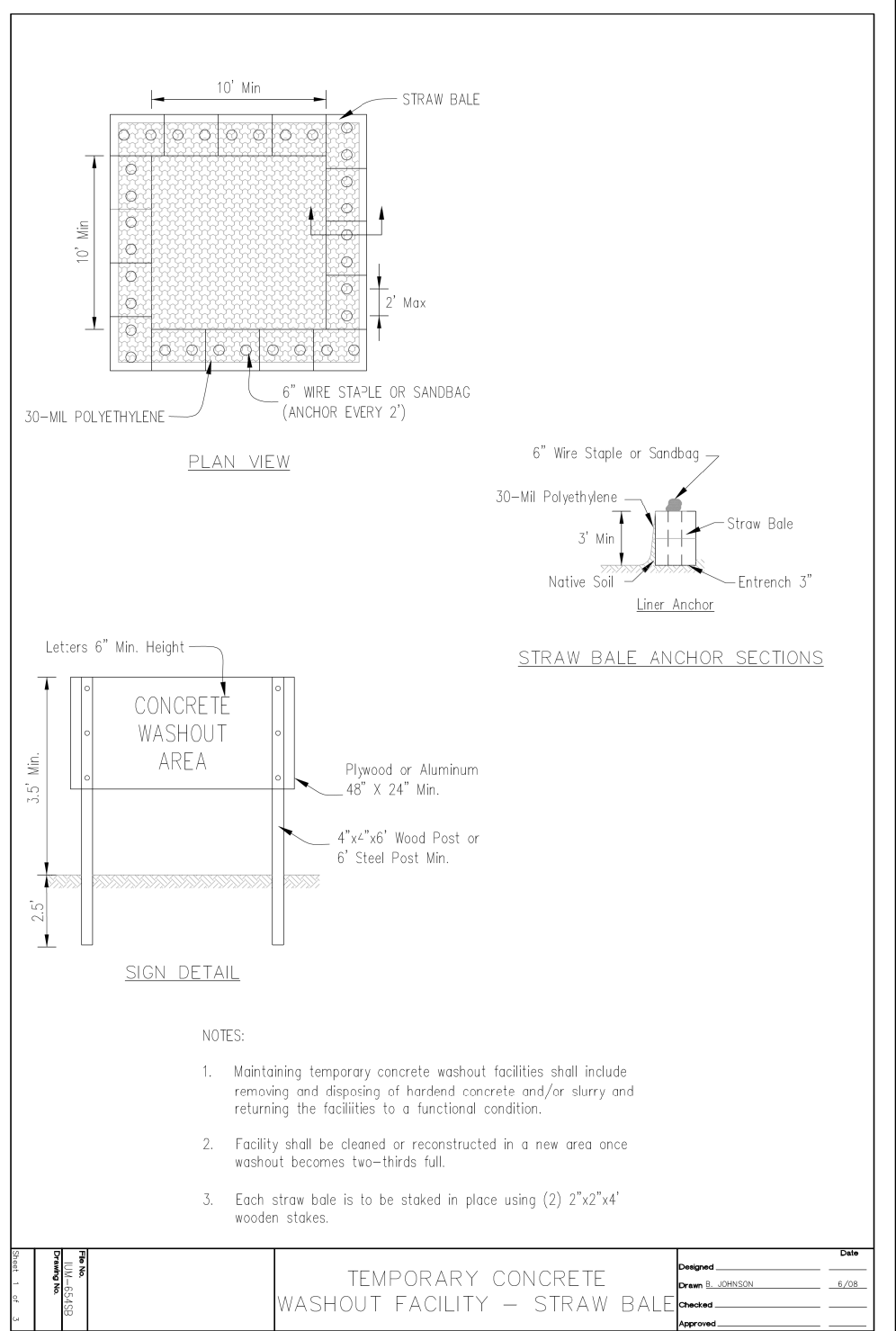
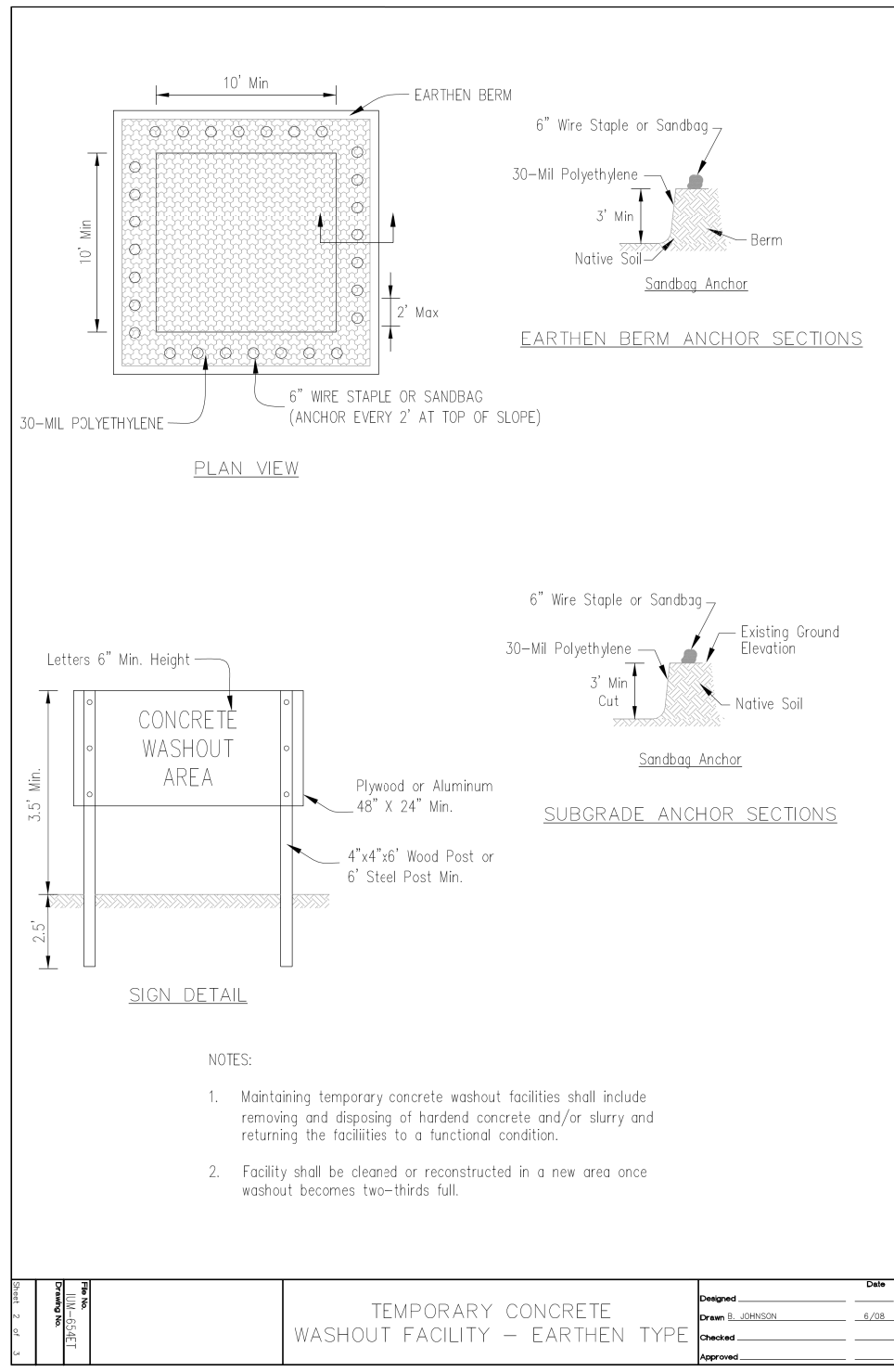
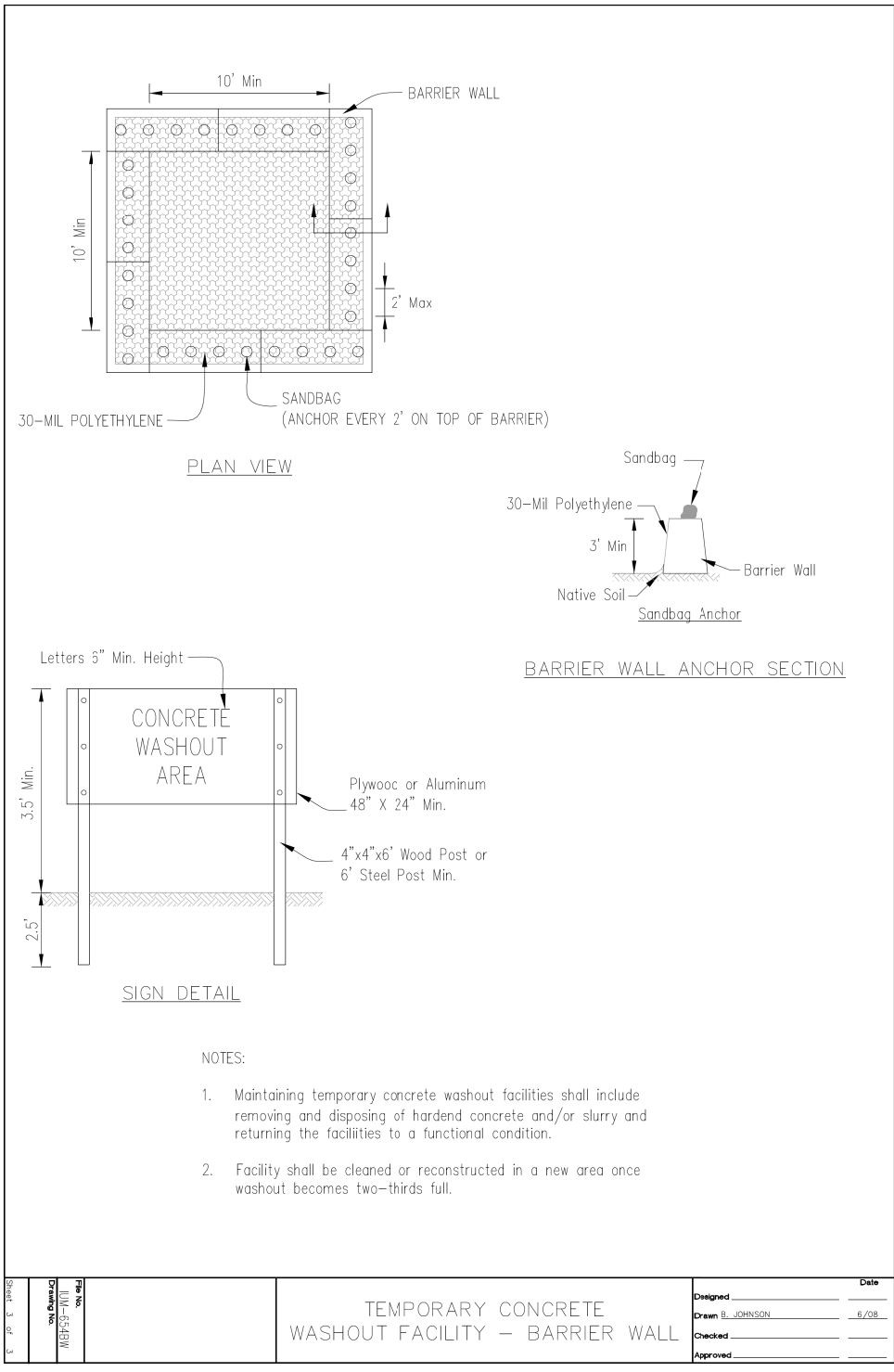
SUMP PIT PLAN



NOTES:

- Pit dimensions are optional.
- The standpipe will be constructed by perforating a 12"-24" diameter corrugated metal or PVC pipe.
- A base of 2" aggregate will be placed in the pit to a minimum depth of 12". After installing the standpipe, the pit surrounding the standpipe will then be backfilled with 2" aggregate.
- The standpipe will extend 12" to 18" above the lip of the pit.
- If discharge will be pumped directly to a storm drainage system, the standpipe will be wrapped with filter fabric before installation.
- If desired, 1/4"-1/2" hardware cloth may be placed around the standpipe prior to attaching the filter fabric. This will increase the rate of water seepage into the pipe.

REFERENCE		STANDARD DWG. NO.
Project		IL-650
Designed		SHEET 1 OF 1
Checked		DATE 8-11-94

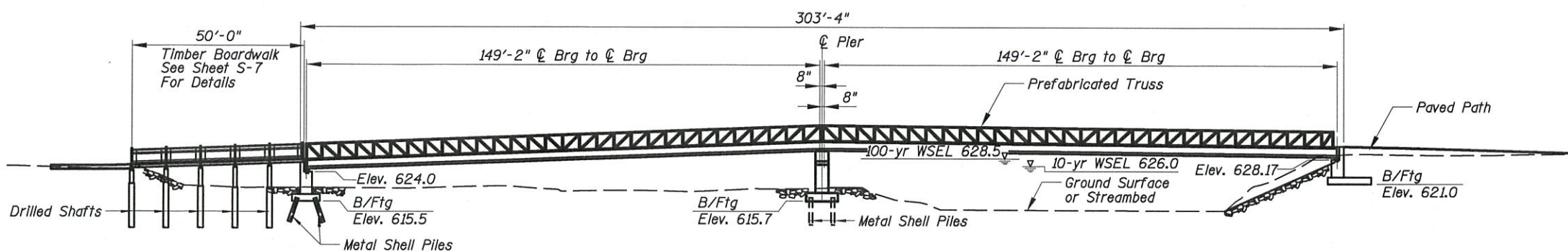


Bench Mark: OSBM-13-53 Square cut on northwest corner of conc. base of electric box on southwest corner of Bryn Mawr & Des Plaines River Road. Elevation=632.57

Existing Structure: None

INDEX OF SHEETS

- S1 General Plan And Elevation
- S2 Structural General Notes
- S3 Superstructure Details
- S4 Boardwalk Details
- S5 East Abutment-Plan and Sections
- S6 West Abutment-Plan and Sections
- S7 Pier Detail
- S8 Metal Shell Pile Details
- S9 Boring Logs
- S10 Boring Logs
- S11 Boring Logs



BRIDGE REACTION TABLE

ITEM	P (LBS) BRG.	H (LBS) ABUTMENT	L (LBS)
DEAD LOAD	47,875	—	—
UNI. LIVE LOAD	40,500	—	—
VEHICLE LOAD	10,000	—	—
UPLIFT WIND	-15,375	—	—
20 PSF	-5,125	—	—
WIND	$\pm 12,585$	25,815	—
THERMAL	—	—	7,185

All Footings Have Been Designed Based On The Bridge Reactions Shown
 "P"- Vertical Load Each Base Plate (4 Per Bridge)
 "H"- Horizontal Load Per Pier (2 Per Bridge)
 "L"- Longitudinal Load Each Base Plate (4 Per Bridge)

DESIGN SPECIFICATIONS

2018 AASHTO Bridge Design Specifications, 8th Edition and 2009 LRFD Guide Specifications for the Design of Pedestrian Bridge.

LOADING H6

Uniform Live Load = 90 PSF

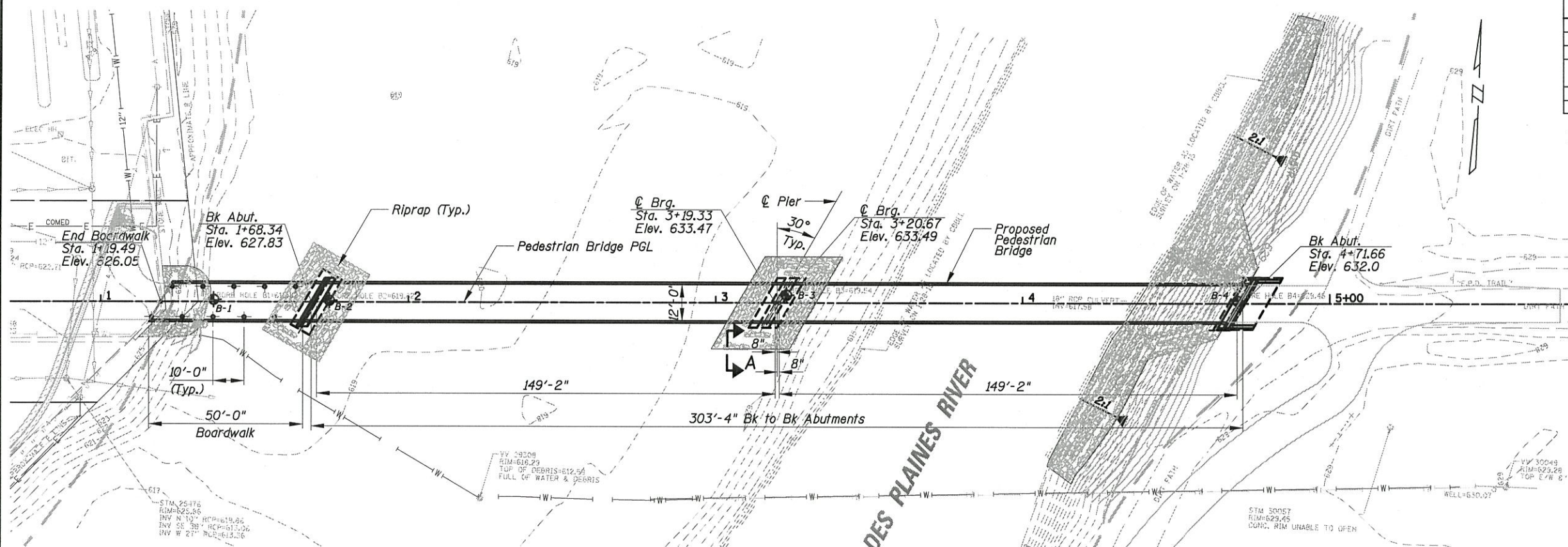
DESIGN STRESSES

FIELD UNITS

f'_c = 4,000 psi (Concrete Structures)
 f_y = 60,000 psi (Reinforcement)

SEISMIC DATA

Seismic Performance Zone (SPZ) = I
 Design Spectral Acceleration at 1.0 sec. (S_{D1}) = 0.084g
 Design Spectral Acceleration at 0.2 sec. (S_{D5}) = 0.145g
 Soil Site Class = D



NOTE: All Dimensions are Measured Along the Baseline.

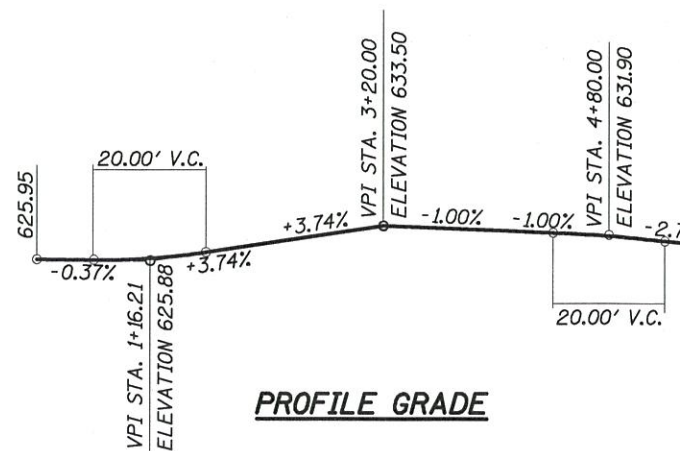
WATERWAY INFORMATION

Drainage Area = 419 sq. mi. Low Grade Elev. 625.88 @ Sta. 1+16.21

Flood	Freq. Yr.	Q C.F.S.	Opening	Sq. Ft.	Nat. H.W.E.	Head - Ft.	Headwater El.
	10	4990	NA	2867	626.3	NA	626.3
Design	50	6100	NA	3537	627.9	NA	627.9
Base	100	6508	NA	3838	628.5	NA	628.5
Overtopping	<10						
Max. Calc.	500	7283	NA	4510	629.7	NA	629.7

DESIGN SCOUR ELEVATION TABLE

Event / Limit	Design Scour Elevations (ft.) NAVD 88						
	Pier 7	Pier 6	Pier 5	Pier 4	Pier 3	Pier 2	Pier 1 E. Abut.
Q100	NA	620.96	618.23	617.43	617.28	615.08	614.78
Q200	NA	620.38	617.64	616.85	616.70	614.48	614.18
Design	NA	620.96	618.23	617.43	617.28	614.48	614.18
Check	NA	620.37	617.64	616.85	616.70	614.48	614.18
Item 113	NA	8	8	8	8	5	5



I Certify That To The Best Of My Knowledge, Information And Belief, This Bridge Design Is Structurally Adequate For The Design Loading Shown On The Plans. The Design Is An Economical One For The Style Of Structure And Complies With Requirements Of The Current "AASHTO Standard Specification For Highway And Bridges".



MAJID MOBASSERI
 ILLINOIS REGISTRATION No. 081-005058
 STRUCTURAL ENGINEER
 EXPIRATION DATE: 11/30/20



**GENERAL PLAN & ELEVATION
 BRYN MAWR PEDESTRIAN BRIDGE
 OVER DES PLAINES RIVER**

I GENERAL NOTES

- All work shall be done in accordance to the Illinois Department of Transportation (IDOT) Standard Specification For Road and Bridge Construction, adopted April 1, 2016, and the "Supplemental Specifications and Recurring Special Provisions", adopted January 1, 2020, unless noted otherwise. Construction Plans and Subsequent Details are all to be considered as part of the Contract. Incidental Items or Accessories necessary to complete this work may not be specifically noted but are considered a part of this Contract.
- No Construction Plans shall be used for Construction unless specifically Marked For Construction. Prior to commencement of construction, the Contractor shall verify all dimensions and conditions affecting the work with the actual conditions. If there are discrepancies between the job site and what is shown on the construction plans, the contractor must immediately report to Engineer before doing any work, otherwise the Contractor shall assume full responsibility. In the event of disagreement between the plans and existing conditions and or details, the Contractor shall secure written instruction from the Engineer prior to proceeding with any part of the work affected by omissions or discrepancies. In failing to secure such instruction, the Contractor will be considered to have proceeded at his own risk and expense. In the event of any doubt or questions arising with respect to the true meaning of the Construction Plans or Specifications, the decision of the Engineer shall be final and conclusive.
- Contractor shall verify all topographic information and grade elevations adjacent to bridge prior to proceeding, inform Engineer of any variation.

II CAST-IN-PLACE CONCRETE

- All cast-in-place concrete work and reinforcing steel work shall be in accordance with Sections 503 and 508 respectively of the IDOT Standard Specifications For Road And Bridge Construction, adopted April 1, 2016, and Supplemental Specifications and Recurring Special Provisions and as noted below.
- Cover from the face of concrete to face of reinforcement bars shall be 3" for surfaces cast against earth and 2" for all other surfaces unless otherwise shown.
- All reinforcement bars shall be epoxy coated.
- Reinforcement Bars shall conform to the requirements of ASTM A760 Grade 60. Field bending or cutting shall not be permitted.
- Reinforcing bar bending dimensions are out to out.
- Concrete in drilled shafts shall be class DS concrete and shall have a minimum compressive strength of 4,000 psi @ 28 days. All other C.I.P. concrete shall be class SI concrete and shall have a minimum compressive strength of 3,500 psi @ 28 days.
- All bearing surfaces must be true and level.
- All exposed concrete edges shall be beveled 3/4".
- Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.
- The embankment configuration shown shall be the minimum that must be placed and compacted prior to construction of the abutments.
- Concrete Seat Sealer shall be applied to the seat area of all pier and abutments, top, front and backwall and seat area.

III PREFABRICATED PEDESTRIAN BRIDGE

The Prefabricated Pedestrian Bridge shall be designed, fabricated, delivered and erected according to the Special Provisions of "Pedestrian Truss Superstructure" and design plans.

- Style: Pratt Truss or Approved Equal.
- Span: 150'-0" end to end of each bridge span.
- Loading: Per AASHTO Guide Specification for Design of Pedestrian Bridges.
Dead Load : Actual weight of the structure
Live Load : 90 PSF or H6 (12,000 Lb) vertical load. Vertical impact is not required.
Wind Load : 35 PSF on the full vertical projected area of the bridge, as if enclosed.

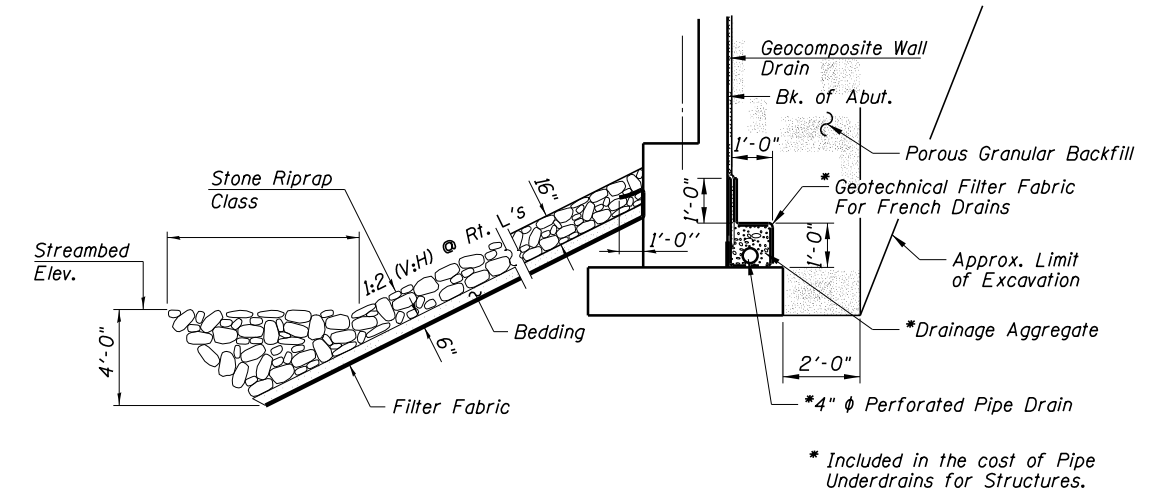
- Finishes: All steel shall be unpainted weathering steel, except structural steel and exposed surfaces of bearings within a distance of 15 ft. each way from the deck joints shall be painted as specified in Section 506 of the Standard Specifications.
- The total depth of deck, from top of deck to the bottom of bottom chord shall be less than 3'-3".
- Quality: The bridge manufacturer shall maintain proper records assuring that all steel, bolts, and materials used are in accordance with material specified. The bridge shall be identified and marked with a permanent nameplate showing the manufacturer's name, location, date of manufacture, and load carrying capacity. Structural material shall be traceable to each bridge. All welders shall be qualified in accordance with AWS D1.1-2020 structural welding code. All workmanship shall be in compliance with AASHTO and AISC standard practice. Full penetration weld details used in shop splices shall be submitted to the Engineer to determine testing required (if any).
- Delivery: Bridges shall be delivered by truck to a location nearest the site accessible by roads.
- Field welding of construction accessories will not be permitted to beams or girders.

IV CONSTRUCTION

- Do not scale dimensions for construction. Scale, if shown, applies only to full size drawings.
- No construction joints, except those shown on the plans, will be allowed unless directed by the Engineer.
- Any information concerning type or location of underground and other utilities is not guaranteed to be accurate or all inclusive. The Contractor is responsible for making his own determinations as to the type and location of the utilities as may be necessary to avoid damage thereto. Contractor shall call J.U.L.I.E. and the Engineer prior to excavation.
- Shop working or layout drawings pertaining to the construction of the work, as may be required, shall be submitted to the Engineer for approval prior to the start of construction.
- Shop drawing shall be signed and sealed by a Structural Engineer licensed in State of Illinois.
- Upon completion, the contractor shall collect and remove all construction debris and excess material from the site. Damaged trees, shrubs, and other landscape features resulting from construction activities shall be replaced or repaired.
- Contractor must coordinate with Bridge Manufacturer to ensure proper placement of cast-in-place anchors. If the contractor elects to use post-installed anchors in lieu of cast-in-place anchors, he must coordinate the plate dimensions, bolt spacing and bolt quantity with the Bridge Manufacturer prior to construction.

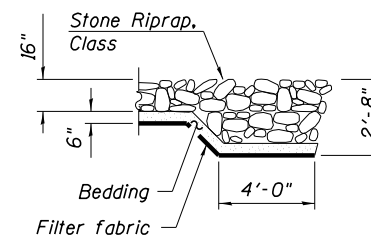
V FOUNDATION NOTES

- Soil Borings and Geotechnical Report prepared by Testing Service Corporation, File No.L-83,271, dated September 29, 2016 for the Proposed Pedestrian Bridge, Bryn Mawr Ave Path over Des Plaines River, have been included in these plans, and project documents.
- The Factored Bearing Resistance for spread footing is 9000 psf. The Factored Resistance Available for Metal Shell piles are listed in the Geotechnical Report prepared by TSC.
- The Contractor is responsible for design, installation and removal of all excavation support system.
- The excavation and work area shall be properly drained at all times during construction, all wet, loose, frozen or other unsuitable material shall be removed prior to placement of concrete or compacted backfill.
- Temporary casing will be required to prevent sloughing or squeezing of the upper fill material into the caisson excavation, as well as (and more importantly) to seal against ground water inflow from the granular and/or intermediate layers often found in the upper 15 feet. The temporary casing may be withdrawn during concrete placement, as long as the concrete within the casing is maintained at an adequate level (or head) to balance squeezing and/or hydrostatic pressures in surrounding soils.



STONE RIPRAP DETAIL

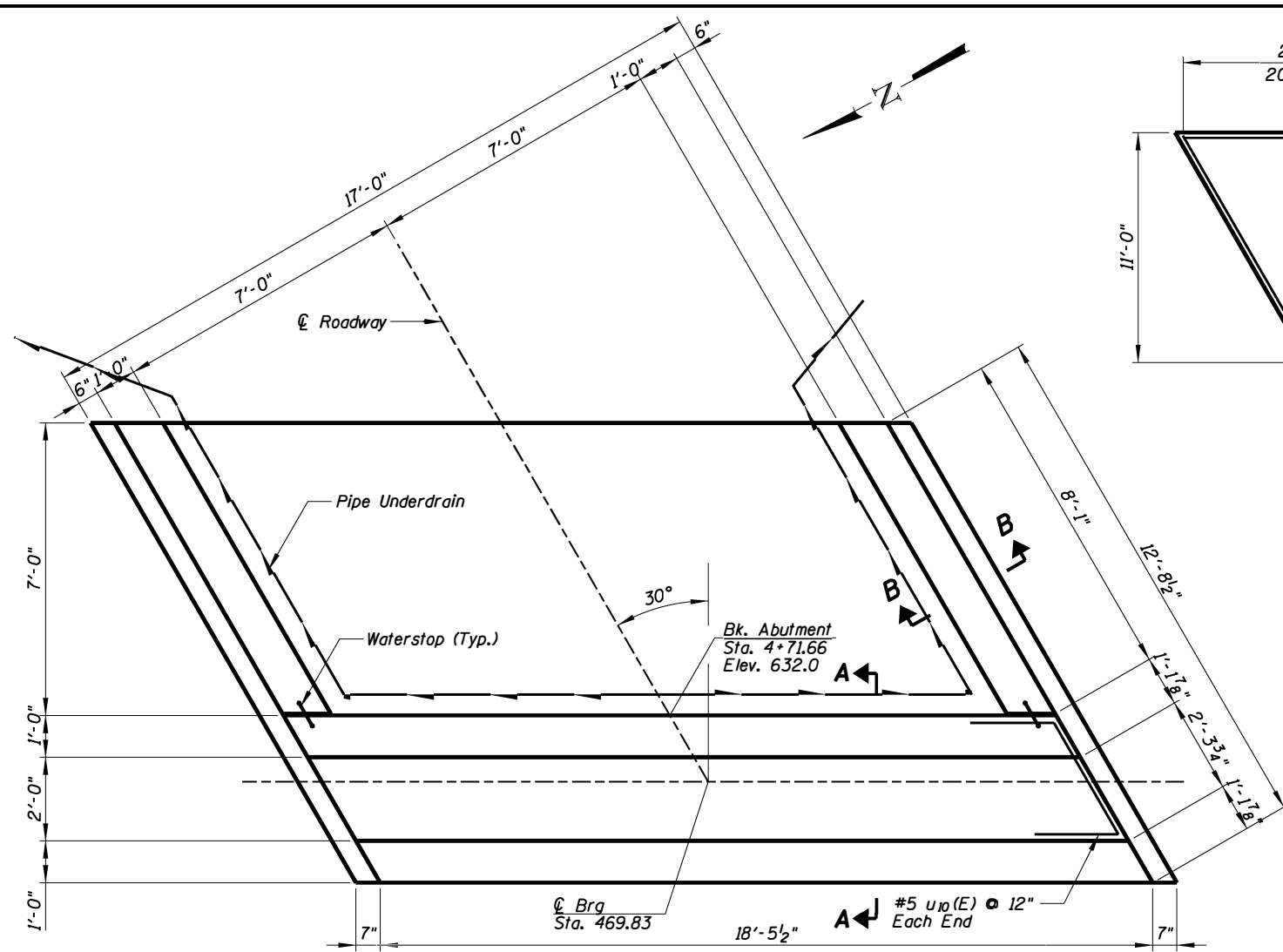
DRAINAGE DETAIL AT ABUTMENT



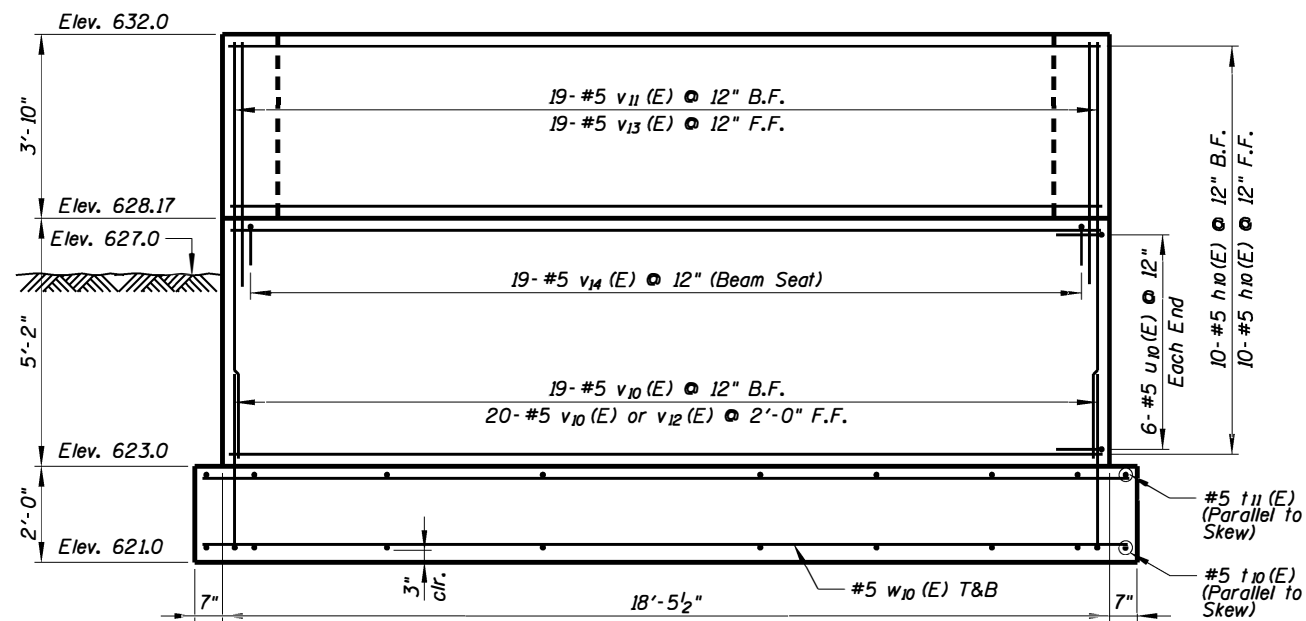
SECTION A-A

STRUCTURE BILL OF MATERIALS

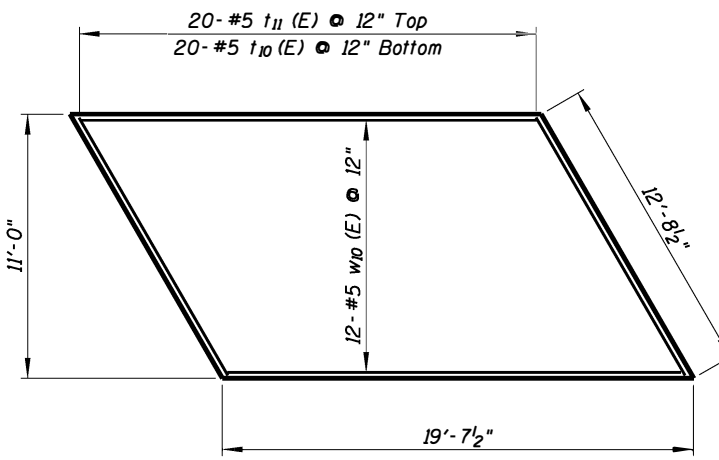
UNIT	ITEM	UNIT	TOTAL
50200100	Structure Excavation	Cu. Yd.	218.5
50300225	Concrete Structures	Cu. Yd.	93.2
50800205	Reinforcement Bars, Epoxy Coated	Lbs.	10,140
51200957	Furnishing Metal Shell Piles, 12" x 0.250"	L. Ft.	280
51202305	Driving Piles	L. Ft.	280
51203200	Test Pile Metal Shells	Each	2
51603000	Drilled Shaft in Soil	Cu. Yd.	10.5
58600101	Granular Backfill for Structures	Cu. Yd.	70
59100100	Geocomposite Wall Drain	Sq. Yd.	25
X0322508	Pedestrian Truss Superstructure	Sq. Ft.	3640
XX008287	Boardwalk Structure	Sq. Ft.	650
Z0046304	Pipe Underdrains for Structures, 4"	L. Ft.	55



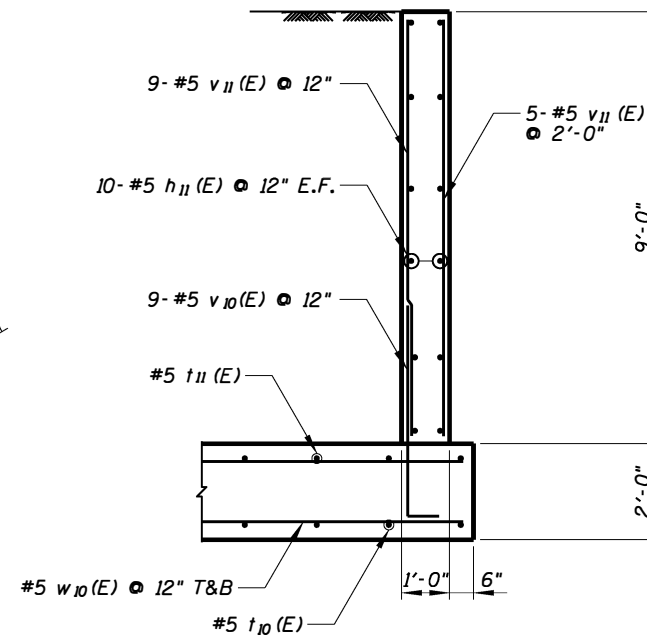
PLAN - EAST ABUTMENT



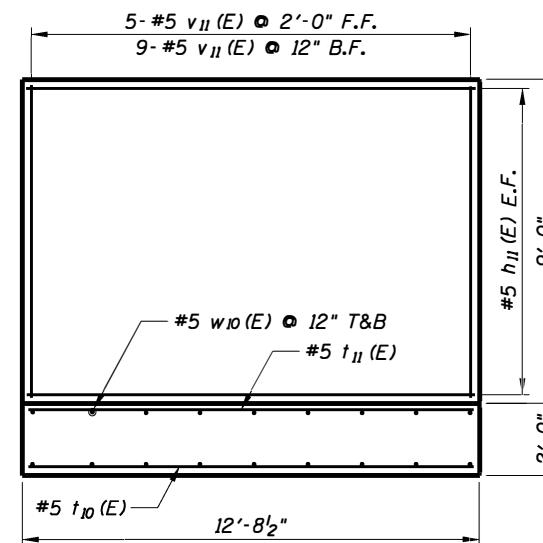
ELEVATION - EAST ABUTMENT



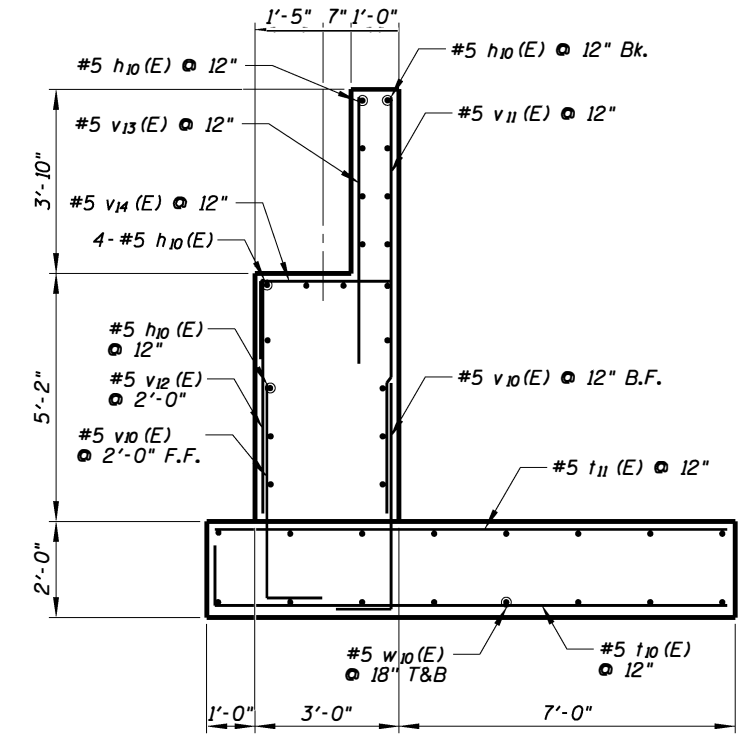
FOUNDATION PLAN



SECTION B-B



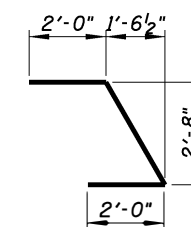
WINGWALL ELEVATION



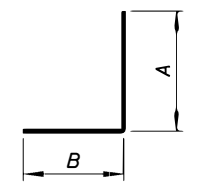
SECTION A-A

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h10 (E)	22	#5	18'-2"	—
h11 (E)	40	#5	7'-9"	—
t10 (E)	20	#5	13'-10"	┘
t11 (E)	20	#5	12'-4"	—
u10 (E)	12	#5	7'-1"	┘
v10 (E)	47	#5	5'-9"	┘
v11 (E)	47	#5	8'-10"	—
v12 (E)	10	#5	5'-0"	—
v13 (E)	19	#5	6'-7"	—
v14 (E)	19	#5	4'-1"	┘
w10 (E)	24	#5	19'-2"	—
Structure Excavation			Cu. Yd.	139.3
Concrete Structures			Cu. Yd.	34.6
Reinforcement Bars, Epoxy Coated			Pound	2,830



BAR u10 (E)



BARS t10 (E), v10 (E), v14 (E)

A & B DIMENSIONS

Bar	A	B
t10 (E)	12'-4"	1'-6"
v10 (E)	4'-9"	1'-0"
v14 (E)	3'-1"	1'-0"

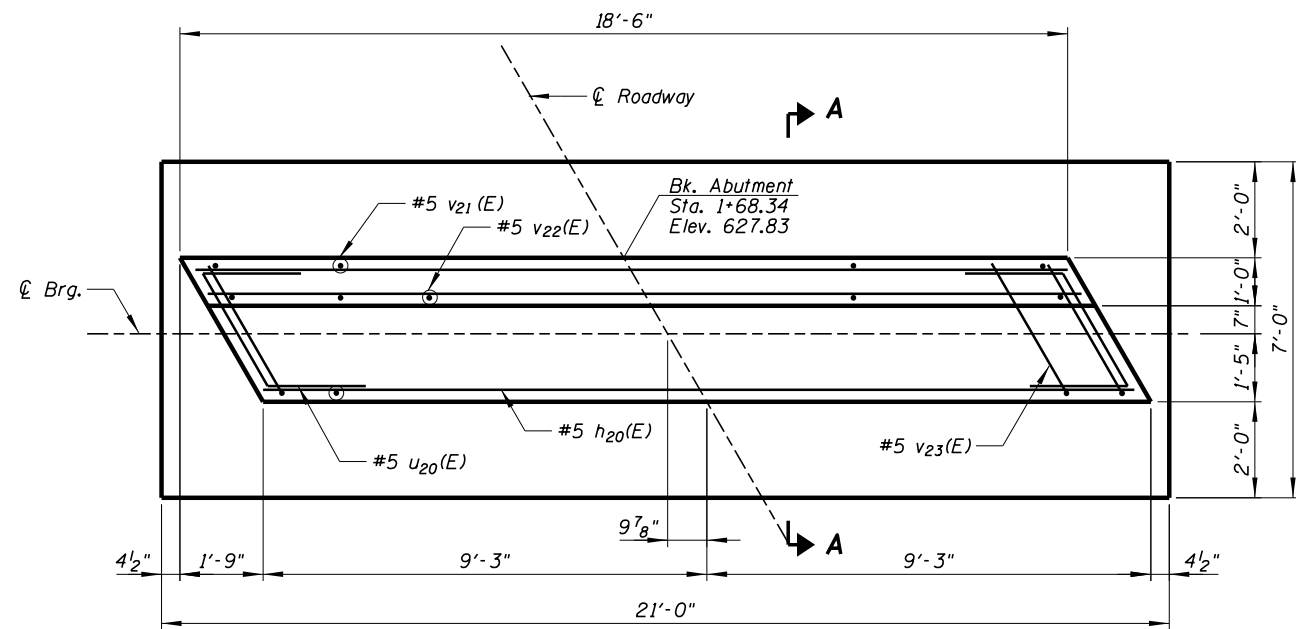
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

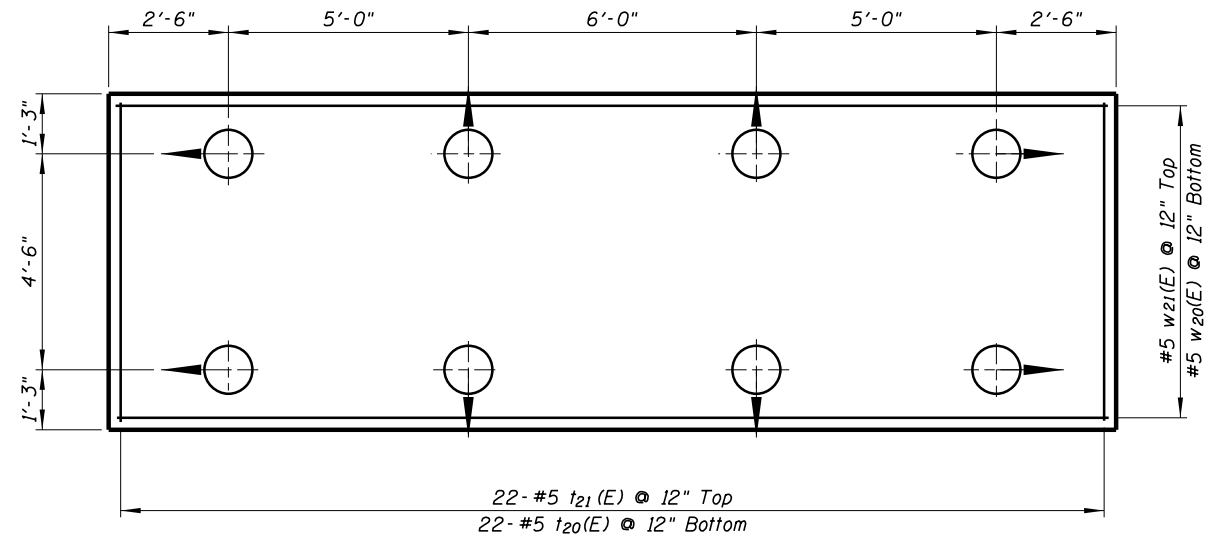
BRYN MAWR BRIDGE AND RIVERWALK
EAST ABUTMENT - PLAN AND SECTIONS

SCALE: SHEET 5 OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 61G51				
ILLINOIS FED. AID PROJECT				



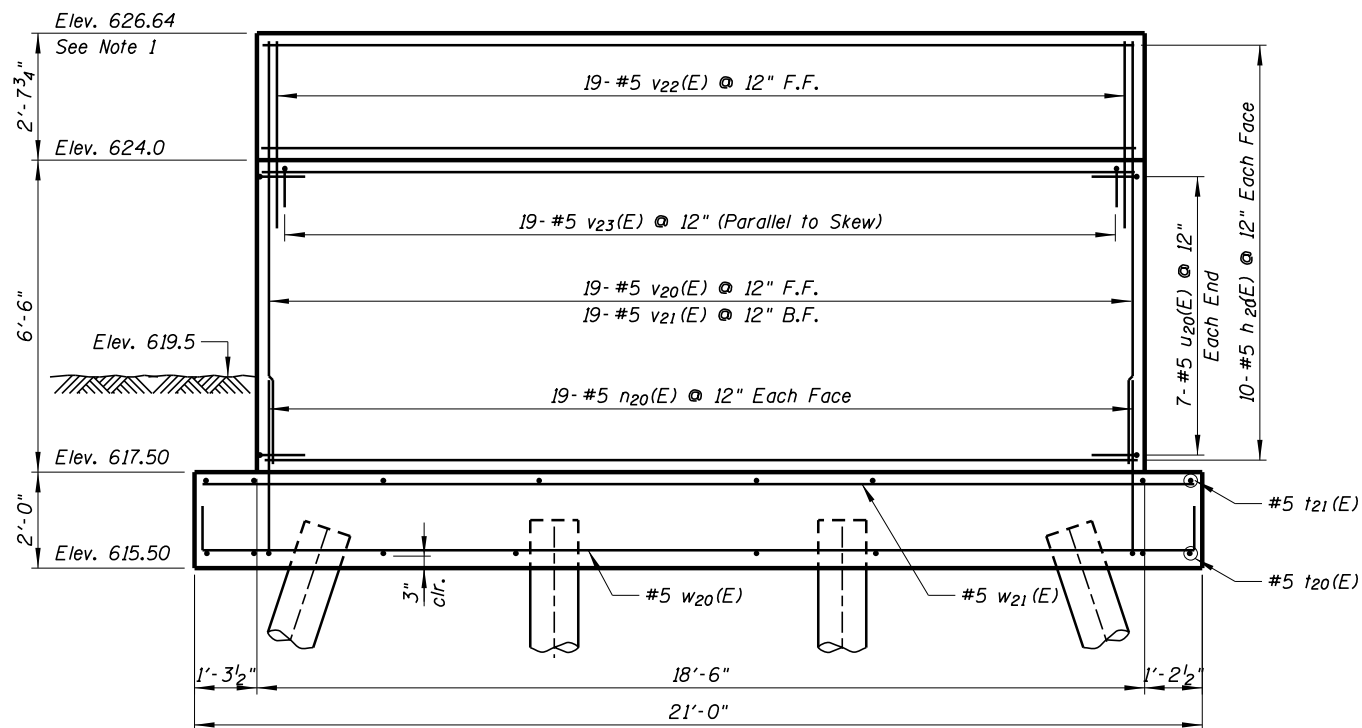
PLAN - WEST ABUTMENT



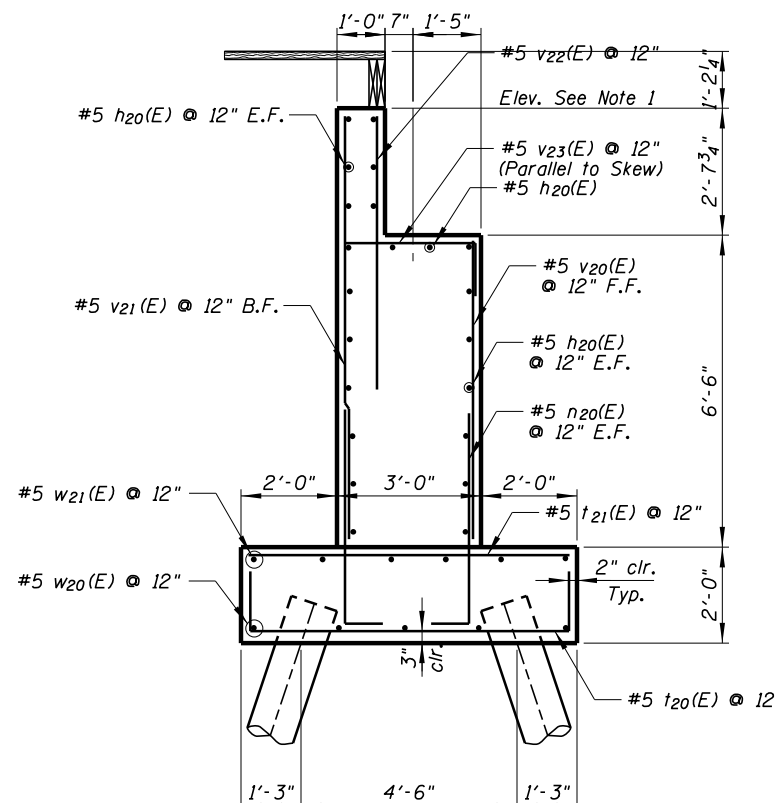
FOUNDATION PLAN - WEST ABUTMENT

PILE DATA

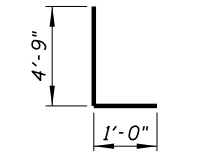
Metal Shell - 12" dia. x 1/4" Wall
 Nominal Required Bearing 227 kip
 Factored Resistance Available 125 kip
 Estimated Pile Length 18 ft
 Number of Production Piles 7
 Number of Test Piles 1
 Batter Piles 4"/12" (Typ.)



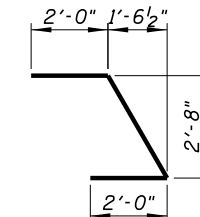
ELEVATION - WEST ABUTMENT



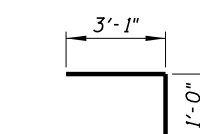
SECTION A-A



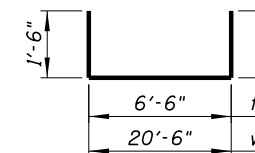
BAR n20(E)



BAR u20(E)



BAR v23(E)



BAR t20(E), w20(E)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h20(E)	22	#5	18'-2"	—
n20(E)	38	#5	5'-9"	└
t20(E)	22	#5	9'-6"	└
t21(E)	22	#5	6'-6"	—
u20(E)	14	#5	7'-1"	└
v20(E)	19	#5	6'-4"	—
v21(E)	19	#5	9'-0"	—
v22(E)	19	#5	5'-7"	—
v23(E)	19	#5	4'-1"	└
w20(E)	8	#5	23'-6"	└
w21(E)	8	#5	20'-6"	—
Structure Excavation		Cu. Yd.	40.8	
Concrete Structures		Cu. Yd.	26.2	
Reinforcement Bars, Epoxy Coated		Pound	2,000	
Furnishing Metal Shell Piles, 12"x0.250"		Foot	126	
Driving Piles		Foot	126	
Test Pile, Metal Shells		Each	1	

NOTE:
 1. The Contractor Shall Coordinate the Top of Back Wall Elevation with Timber Boardwalk Construction.

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Default	PLOT SCALE = 2'	DRAWN -	REVISED -
	PLOT DATE = 3/19/2020	CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BRYN MAWR BRIDGE AND RIVERWALK
 WET ABUTMENT - PLAN AND SECTIONS

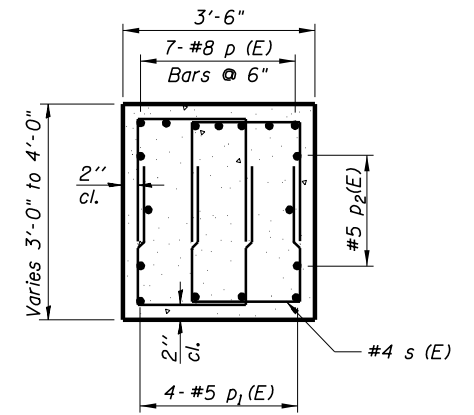
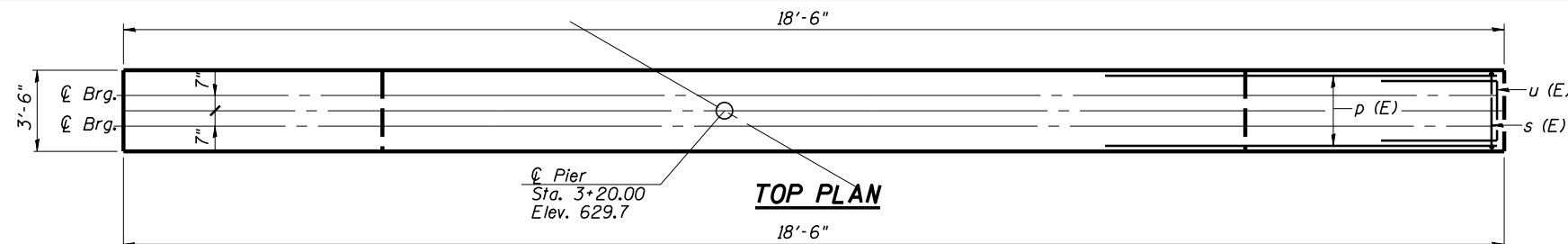
SCALE: SHEET 6 OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 61G51				
ILLINOIS FED. AID PROJECT				

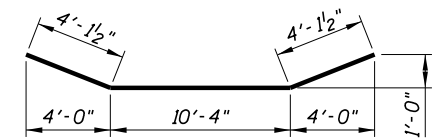
Notes:
Space reinforcement in cap to miss anchor bolts.
For details of piles, see sheet - of -.

PILE DATA

Metal Shell - 12" dia. x 1/4" Wall
Nominal Required Bearing 227 kip
Factored Resistance Available 125 kip
Estimated Pile Length 22 ft
Number of Production Piles 7
Number of Test Piles 1
Batter Piles 4"/12"



SECTION A-A

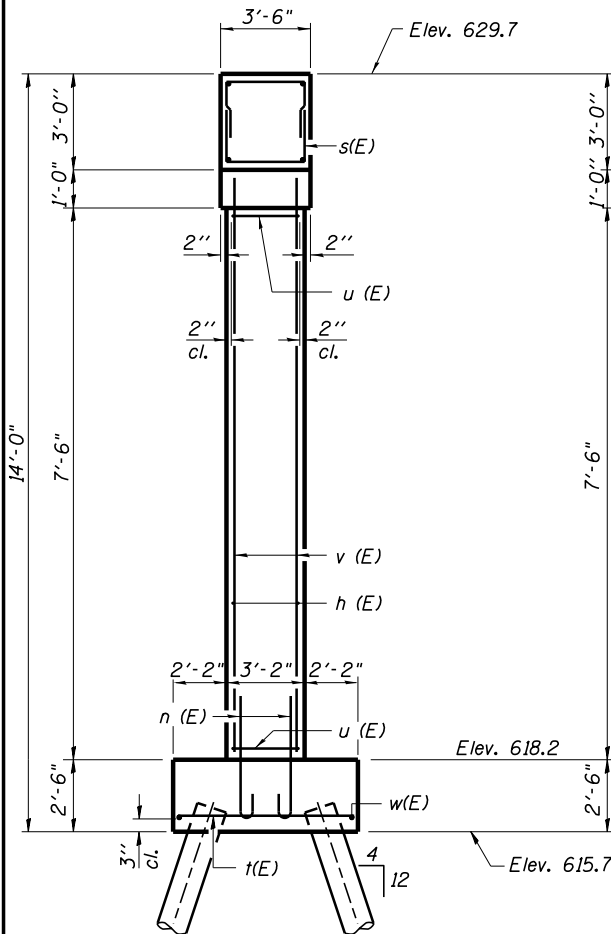


BAR p1 (E)

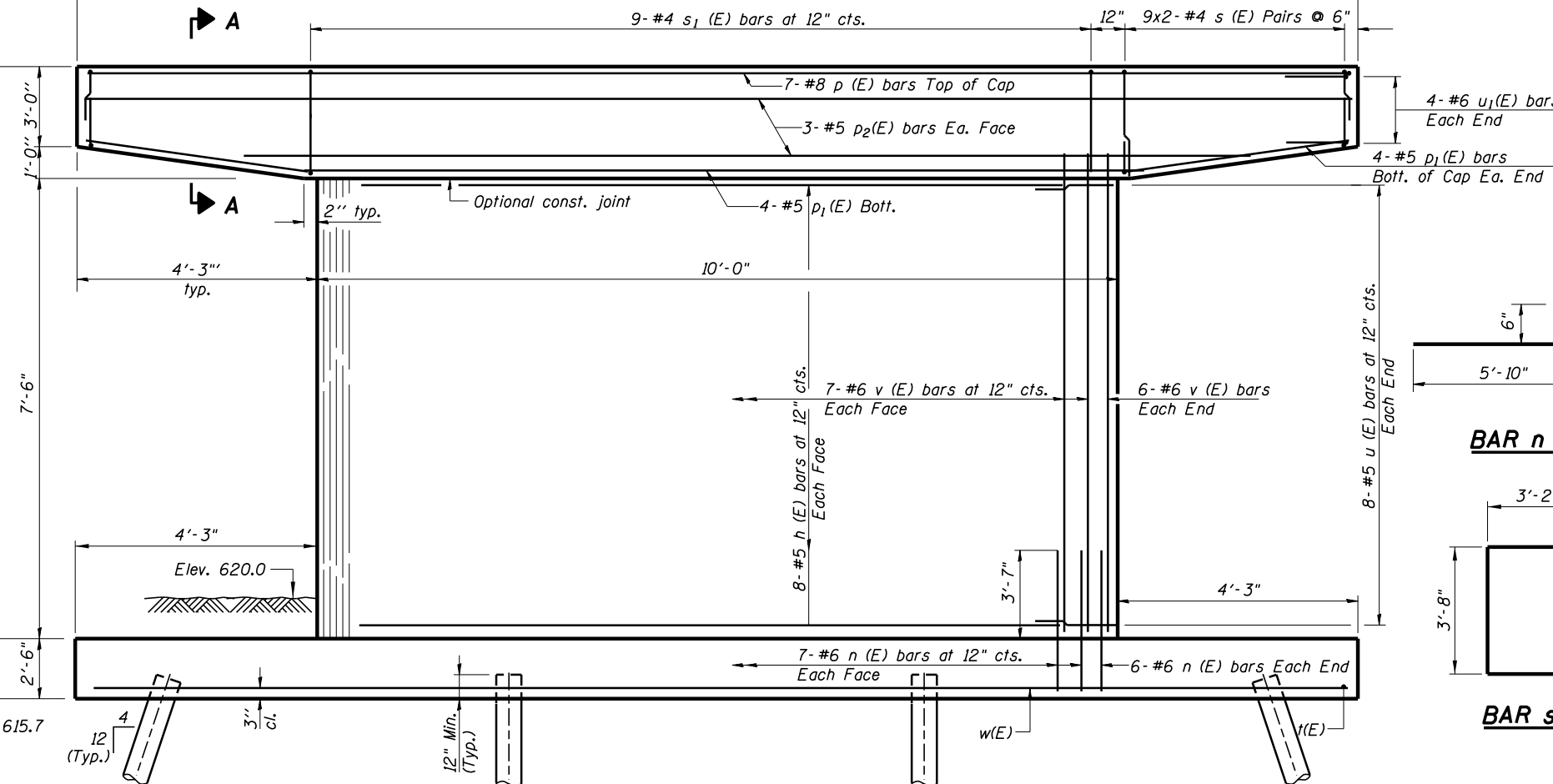
PIER

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h (E)	16	#5	7'-6"	—
n (E)	26	#6	6'-6"	U
p (E)	7	#8	23'-0"	□
p1 (E)	4	#5	18'-3"	U
p2 (E)	6	#5	18'-4"	—
s (E)	72	#4	7'-0"	□
s1 (E)	9	#4	14'-5"	□
t (E)	19	#6	11'-0"	U
u (E)	16	#5	9'-7"	U
u1 (E)	8	#6	9'-4"	U
v (E)	26	#6	10'-3"	—
w (E)	10	#6	22'-0"	U
Structure Excavation		Cu. Yd.	38.4	
Concrete Structures		Cu. Yd.	30.1	
Reinforcement Bars, Epoxy Coated		Pound	2,740	
Furnishing Metal Shell Piles, 12"x0.250"		Foot	154	
Driving Piles		Foot	154	
Test Pile, Metal Shells		Each	1	

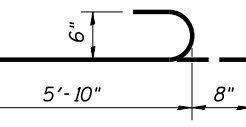


END VIEW

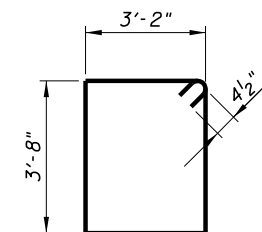


ELEVATION

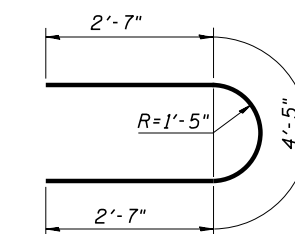
(Looking)



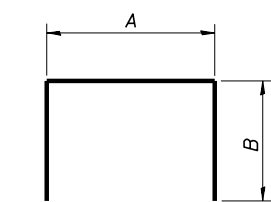
BAR n (E)



BAR s1 (E)



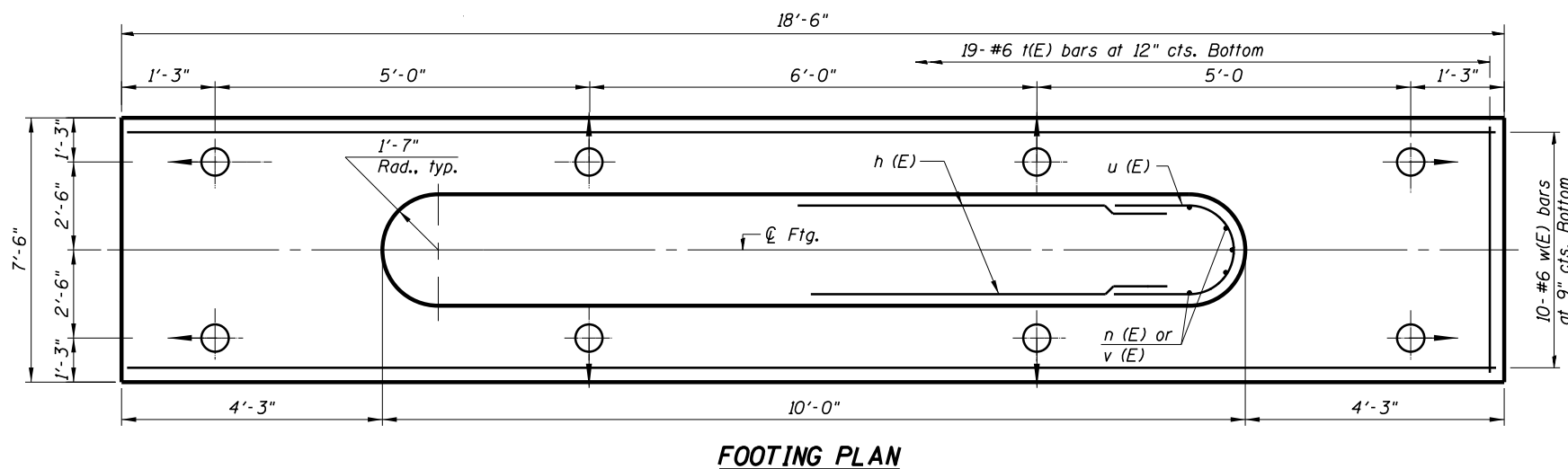
BAR u (E)



BARS

A & B DIMENSIONS

Bar	A	B
p (E)	18'-0"	2'-6"
s (E)	2'-0"	2'-6"
u1 (E)	3'-2"	3'-1"
w (E)	18'-0"	2'-0"
t (E)	7'-0"	2'-0"



FOOTING PLAN

FILE NAME =
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USER NAME = jlepajla
DRAWN -
CHECKED -
DATE -

DESIGNED -
REVISOR -
REVISOR -
REVISOR -
REVISOR -

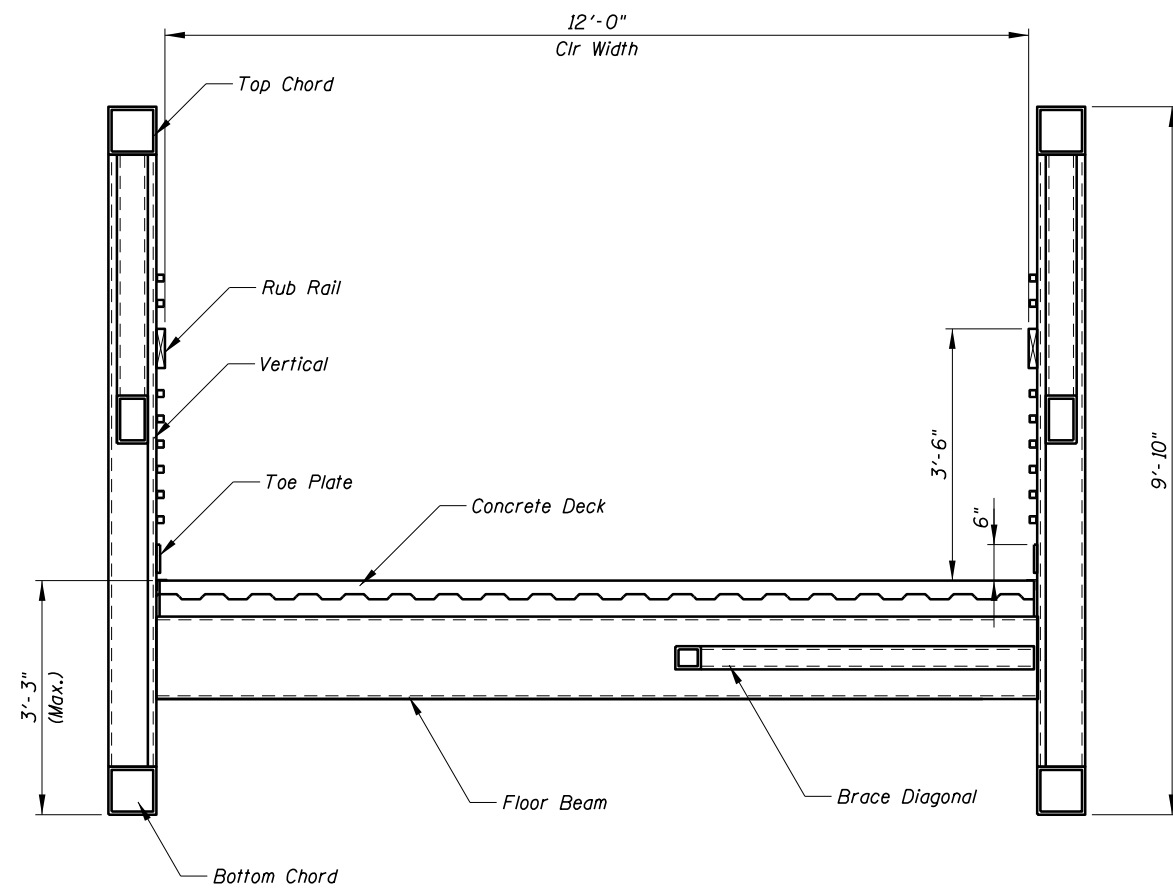
REVISIONS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

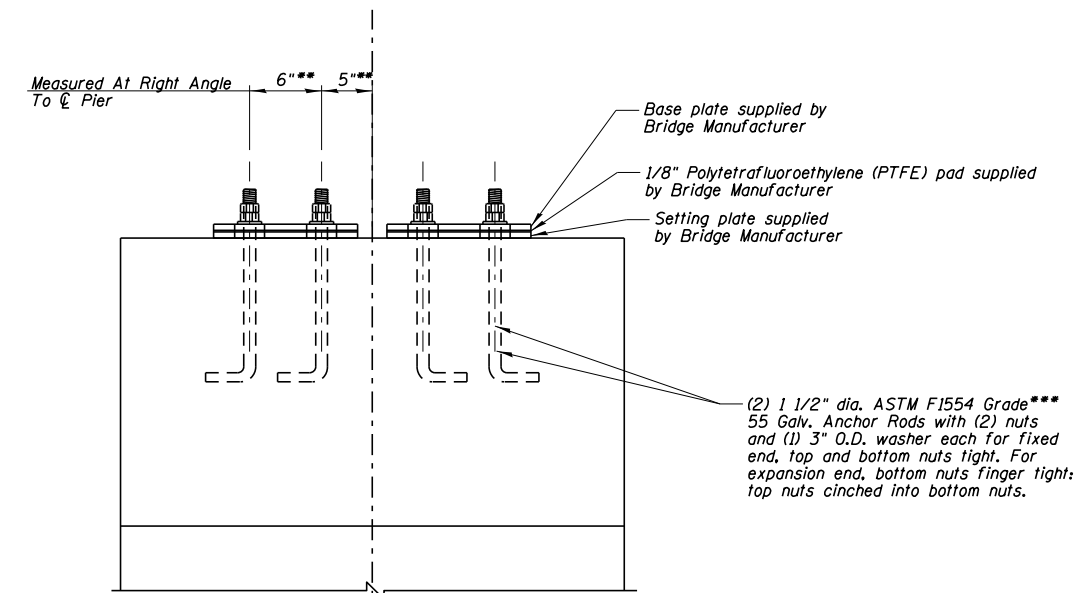
BRYN MAWR BRIDGE AND RIVERWALK
PIER DETAIL

SCALE: SHEET OF SHEETS STA. TO STA.

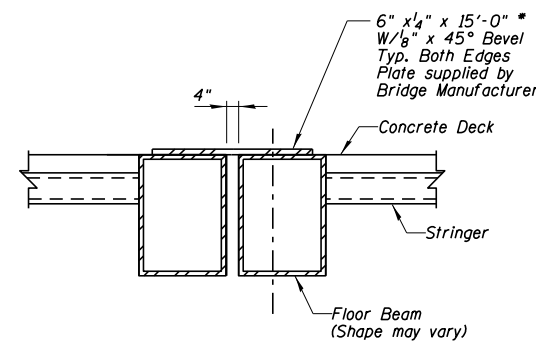
F.A. RTÉ. SECTION COUNTY TOTAL SHEETS SHEET NO.
- 19-00036-00-BR COOK 36 22
CONTRACT NO. 61G51
ILLINOIS FED. AID PROJECT



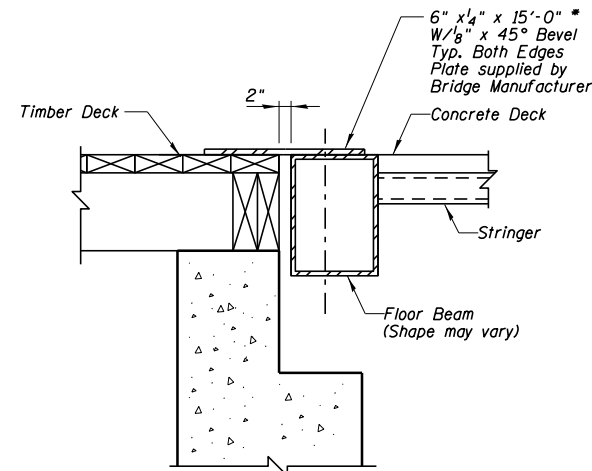
SECTION THRU FABRICATED BRIDGE STRUCTURE



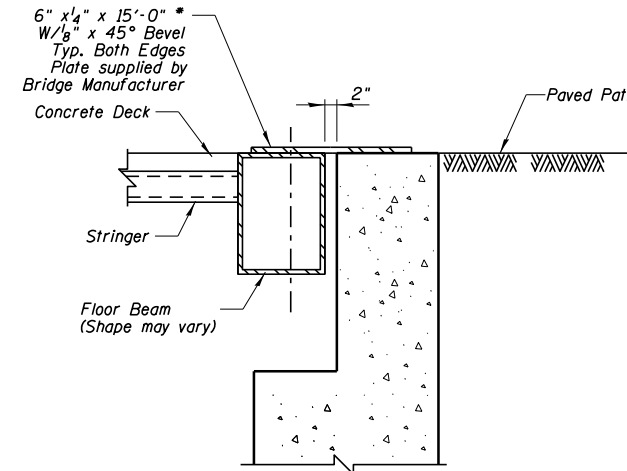
BEARING DETAIL AT PIER



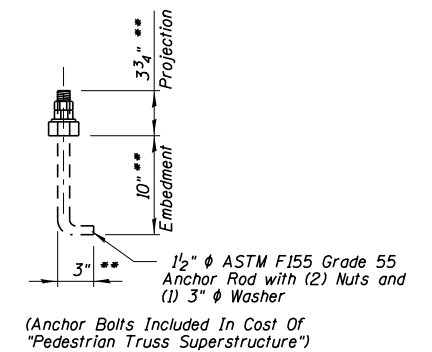
JOINT SEAL AT PIERS



JOINT SEAL AT WEST ABUTMENT



JOINT SEAL AT EAST ABUTMENT

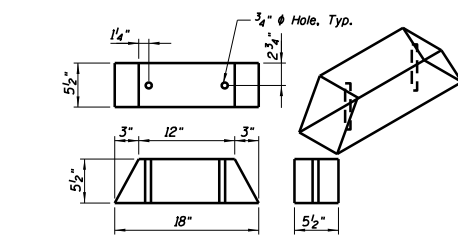


ANCHOR BOLT DETAIL

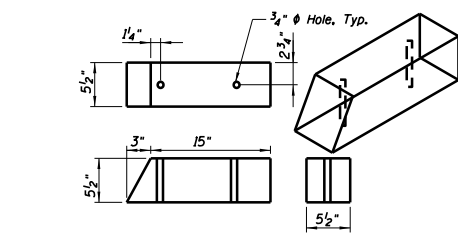
NOTES:

- * Contractor shall coordinate plate dimensions with Bridge Manufacturer prior to construction.
- ** The Contractor shall coordinate the location and layout of the anchor bolts with the Bridge Manufacturer and the Boardwalk Manufacturer
- *** Contractor has the option of substituting anchor bolts with 4-1" ϕ HILTI HAS-R316 Bolts embedded 6" into HIT HY 200 Injection adhesive. Bolts shall not be placed less than 5" from the edge of the structure or less than 6" apart. Contractor shall coordinate plate dimensions, bolt spacing and bolt quantity with Bridge Manufacturer prior to construction.

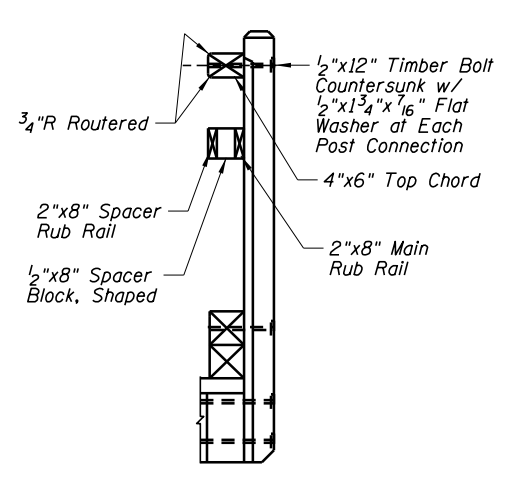
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Default	PLOT SCALE = 2"	DRAWN -	REVISED -					-	19-00036-00-BR	COOK	36	23			
	PLOT DATE = 3/19/2020	CHECKED -	REVISED -					CONTRACT NO. 61G51							
		DATE -	REVISED -					SCALE:	SHEET 3 OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			



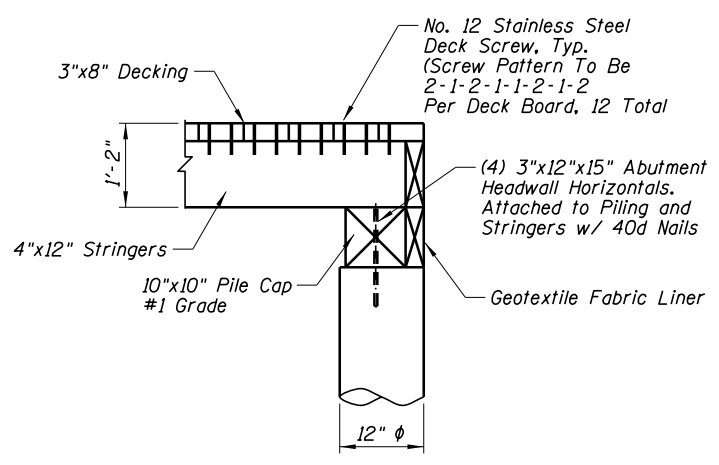
6"x6"x18" CURB BLOCK DETAIL



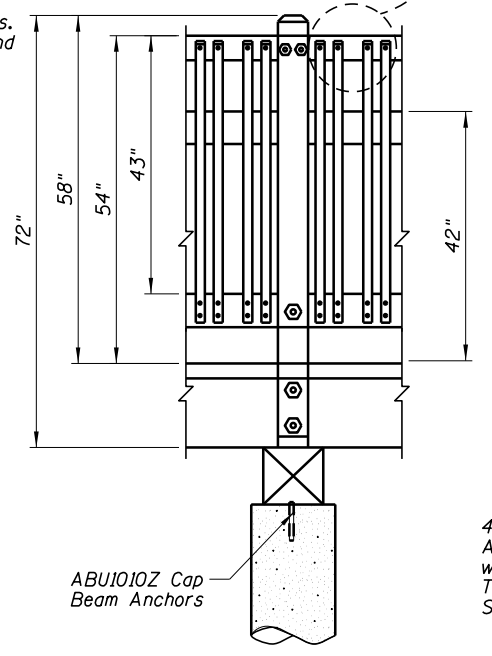
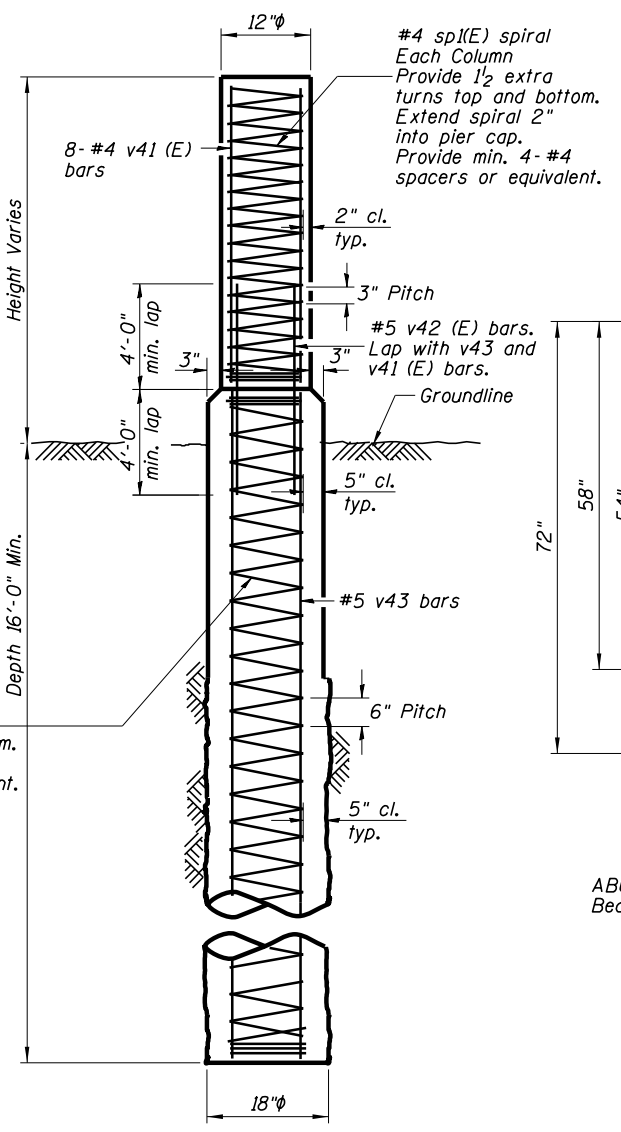
6"x6"x18" END BLOCK DETAIL



MODIFICATION DETAIL

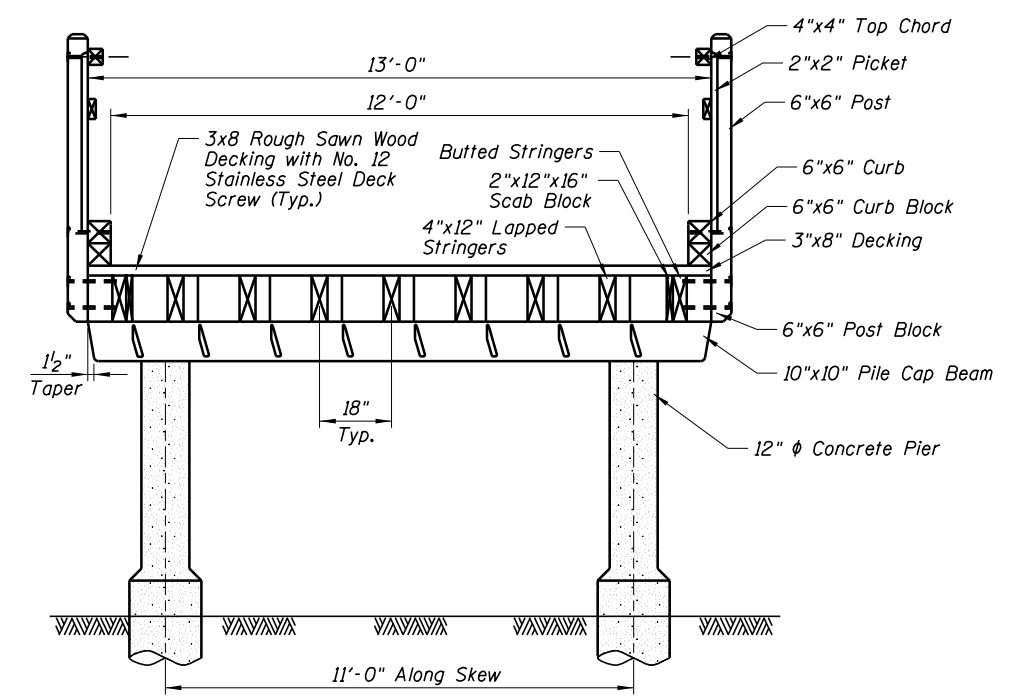
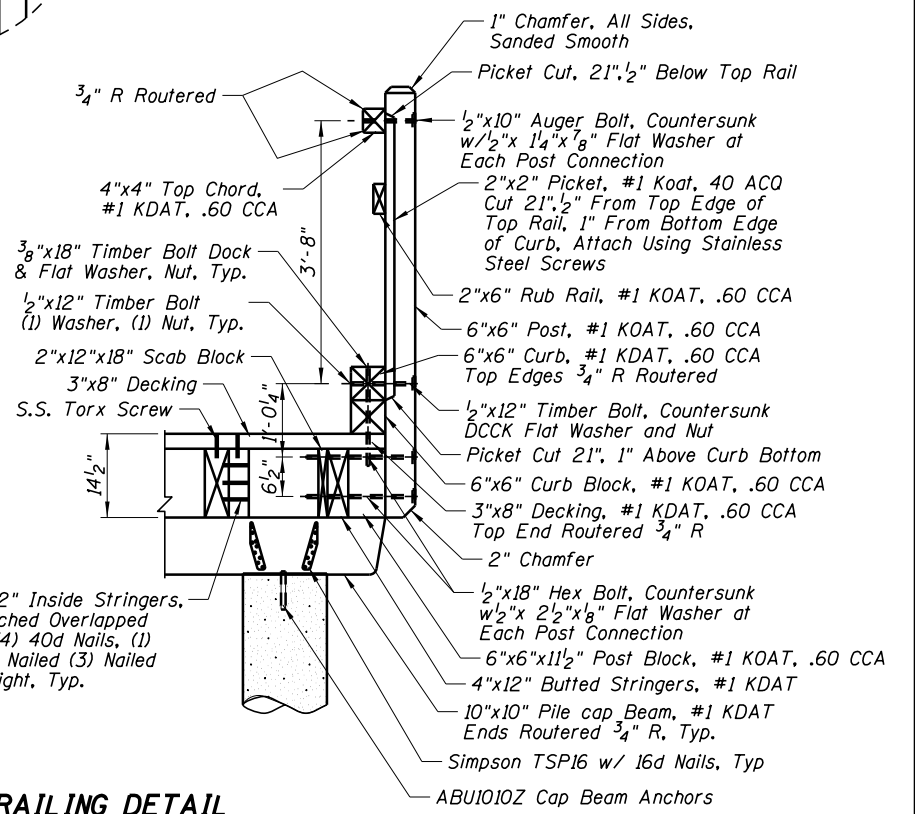


SECTION



RAILING DETAIL

NOTE:
Timber Board Walk shall be supported on Drilled Shaft as shown on plans. All the rest of Timber Board Walk details, members, and connections shown on these plans are schematic and for Information Only. The Contractor shall submit design plans and calculations prepared by a Structural Engineer licensed in the State of Illinois for review.



PILE SUPPORT SECTION

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
v41(E)	80	#4	8'-0"	—
v42(E)	80	#5	8'-0"	—
v43	80	#5	16'-0"	—
sp1(E)	10	#4	1'-6"	⋈
sp2(E)	10	#4	1'-6"	⋈
Concrete Structures			Cu. Yd.	2.3
Reinforcement Bars, Epoxy Coated			Pound	2,570
Drilled Shaft in Soil			Cu. Yd.	10.5

PROJECT **Pedestrian Bridge, Bryn Mawr Ave Path Over Des Plaines River, Rosemont, IL**

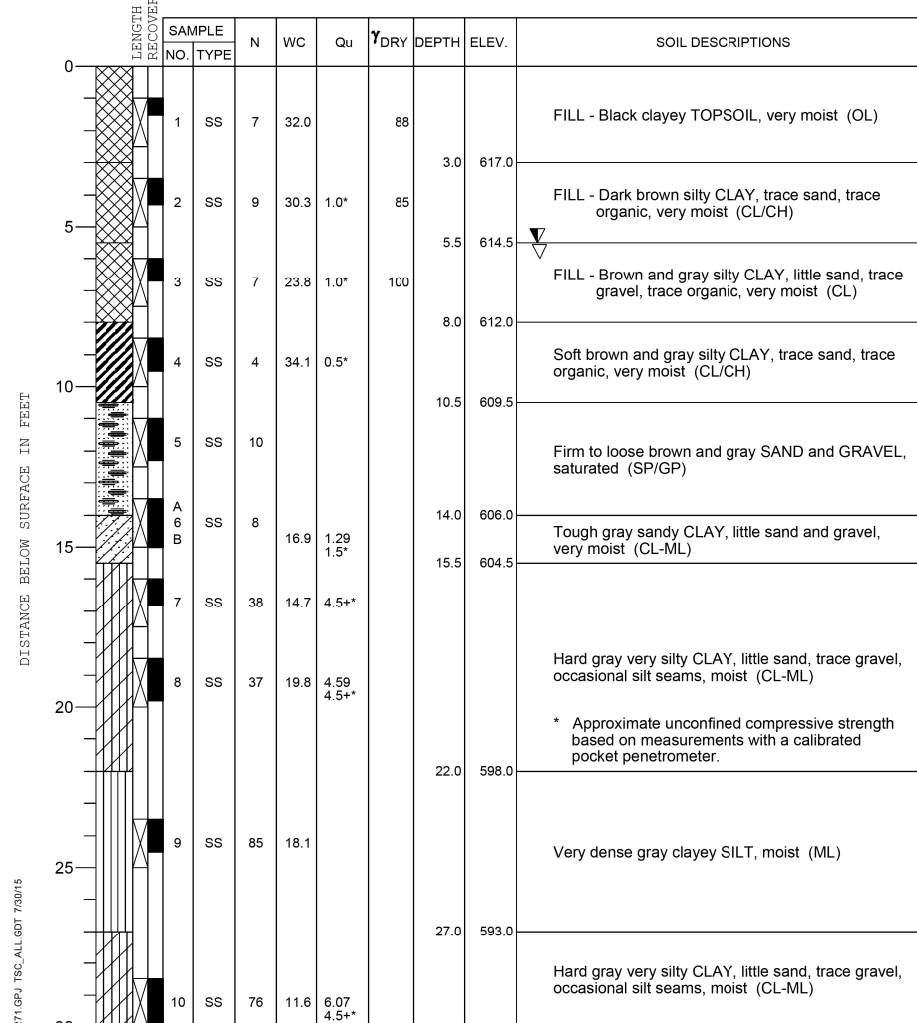


CLIENT **Christopher B. Burke Engineering, Ltd., 9575 West Higgins Road, Rosemont, IL**

BORING **1** DATE STARTED **6-26-15** DATE COMPLETED **6-26-15** JOB **L-83,271**

ELEVATIONS WATER LEVEL OBSERVATIONS
 GROUND SURFACE **620.0** WHILE DRILLING **5.5'**
 END OF BORING **590.0** AT END OF BORING **6.0'**
 24 HOURS

Boardwalk



DRILL RIG NO. **314**

Division lines between deposits represent approximate boundaries between soil types; in-situ, the transition may be gradual.

End of Boring at 30.0'

PROJECT **Pedestrian Bridge, Bryn Mawr Ave Path Over Des Plaines River, Rosemont, IL**

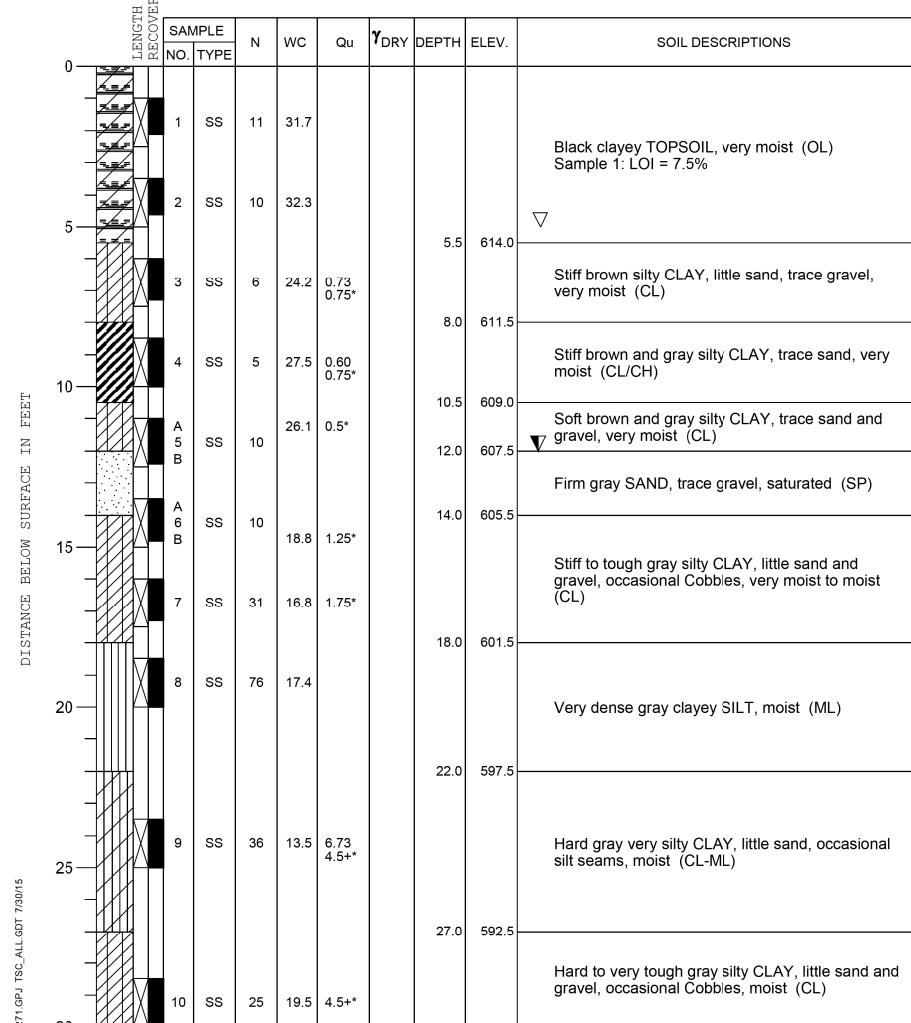


CLIENT **Christopher B. Burke Engineering, Ltd., 9575 West Higgins Road, Rosemont, IL**

BORING **2** DATE STARTED **6-26-15** DATE COMPLETED **6-26-15** JOB **L-83,271**

ELEVATIONS WATER LEVEL OBSERVATIONS
 GROUND SURFACE **619.5** WHILE DRILLING **12.0'**
 END OF BORING **569.5** AT END OF BORING **5.0'**
 24 HOURS

West Pier



DRILL RIG NO. **314**

Division lines between deposits represent approximate boundaries between soil types; in-situ, the transition may be gradual.

Page 1 of 2

PROJECT **Pedestrian Bridge, Bryn Mawr Ave Path Over Des Plaines River, Rosemont, IL**

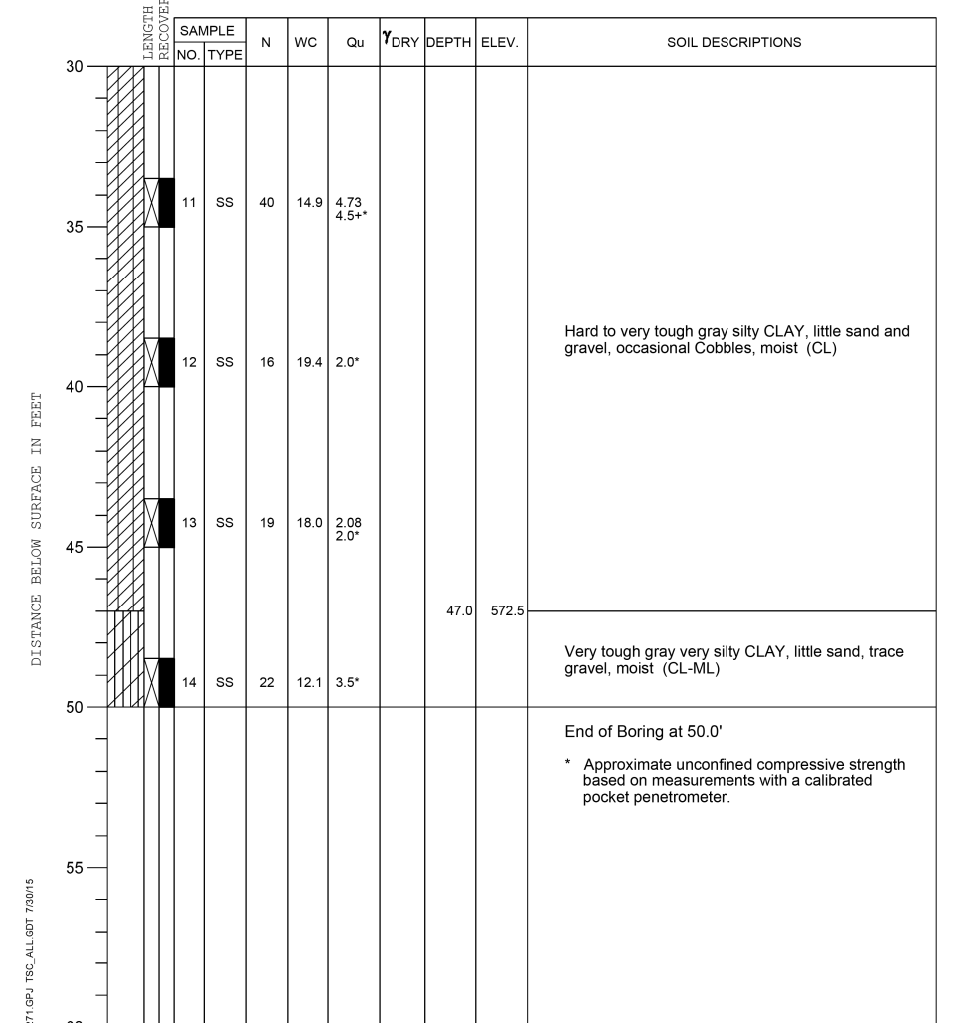


CLIENT **Christopher B. Burke Engineering, Ltd., 9575 West Higgins Road, Rosemont, IL**

BORING **2** DATE STARTED **6-26-15** DATE COMPLETED **6-26-15** JOB **L-83,271**

ELEVATIONS WATER LEVEL OBSERVATIONS
 GROUND SURFACE **619.5** WHILE DRILLING **12.0'**
 END OF BORING **569.5** AT END OF BORING **5.0'**
 24 HOURS

West Pier

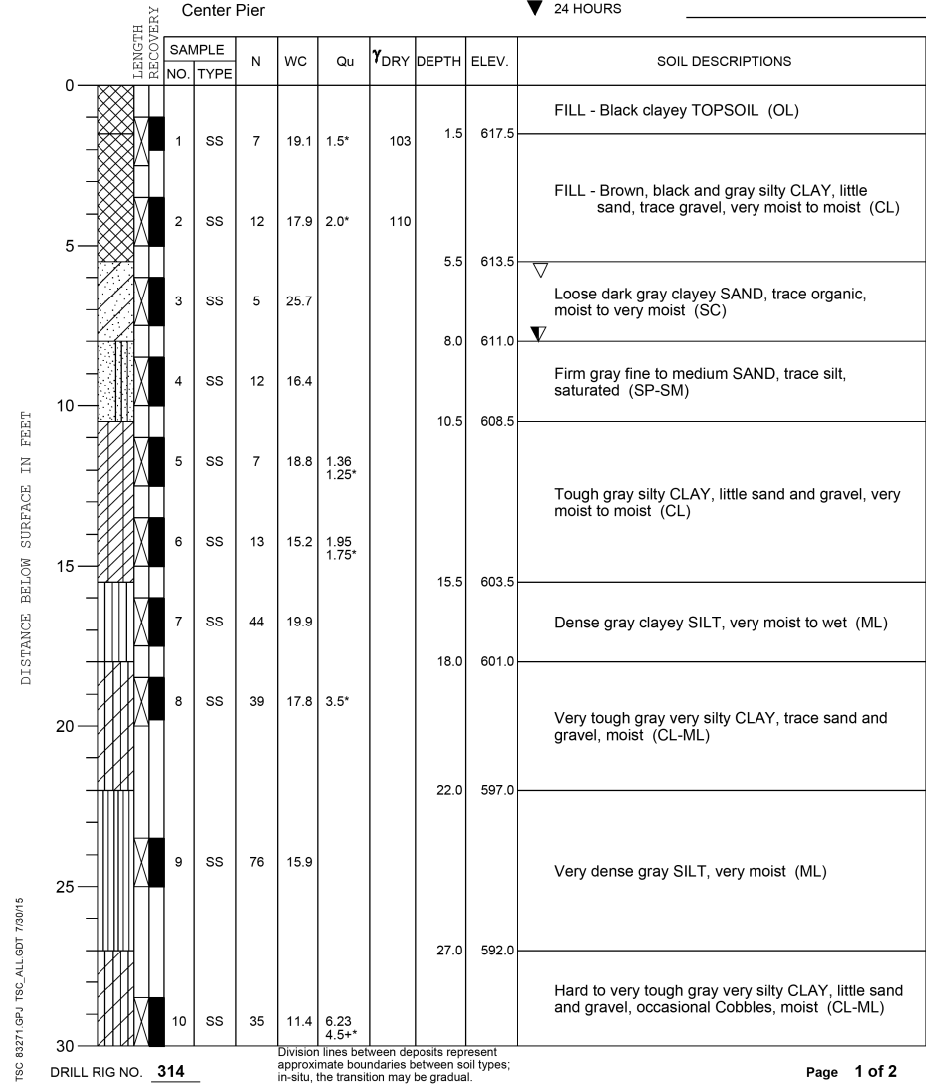


DRILL RIG NO. **314**

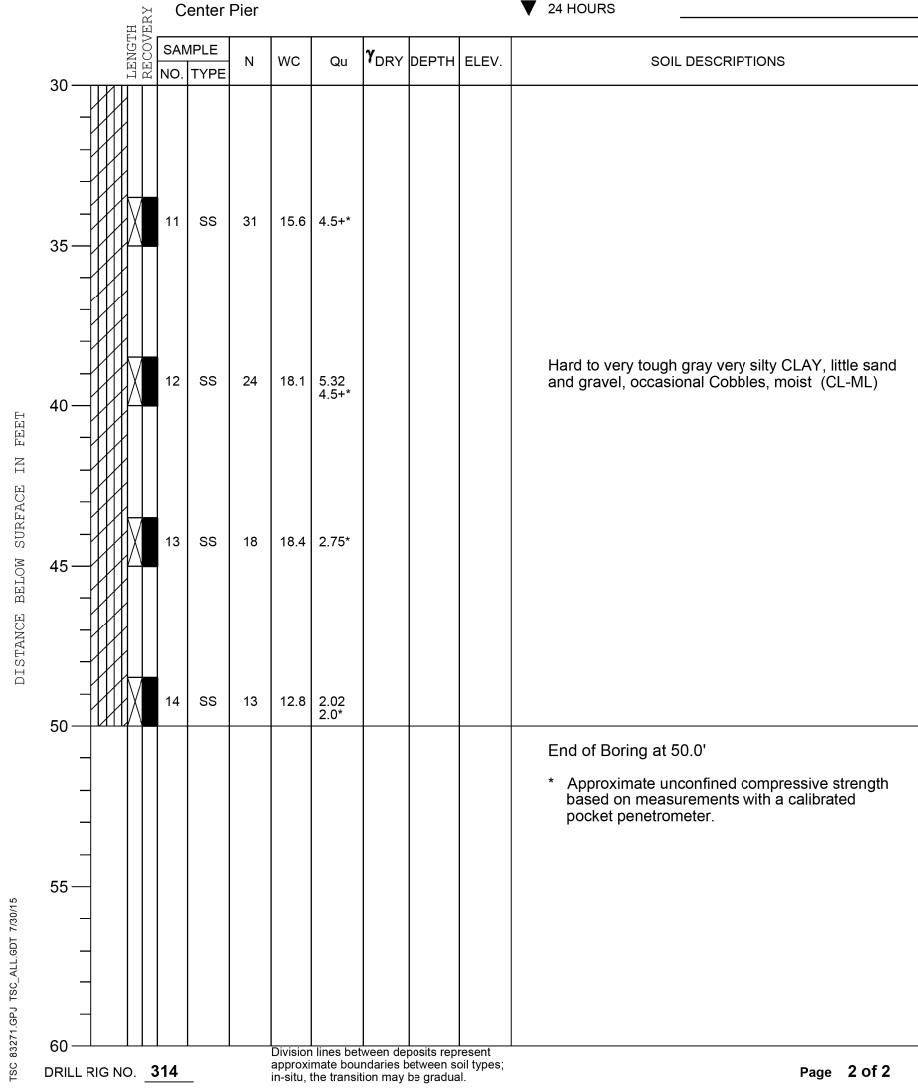
Division lines between deposits represent approximate boundaries between soil types; in-situ, the transition may be gradual.

Page 2 of 2

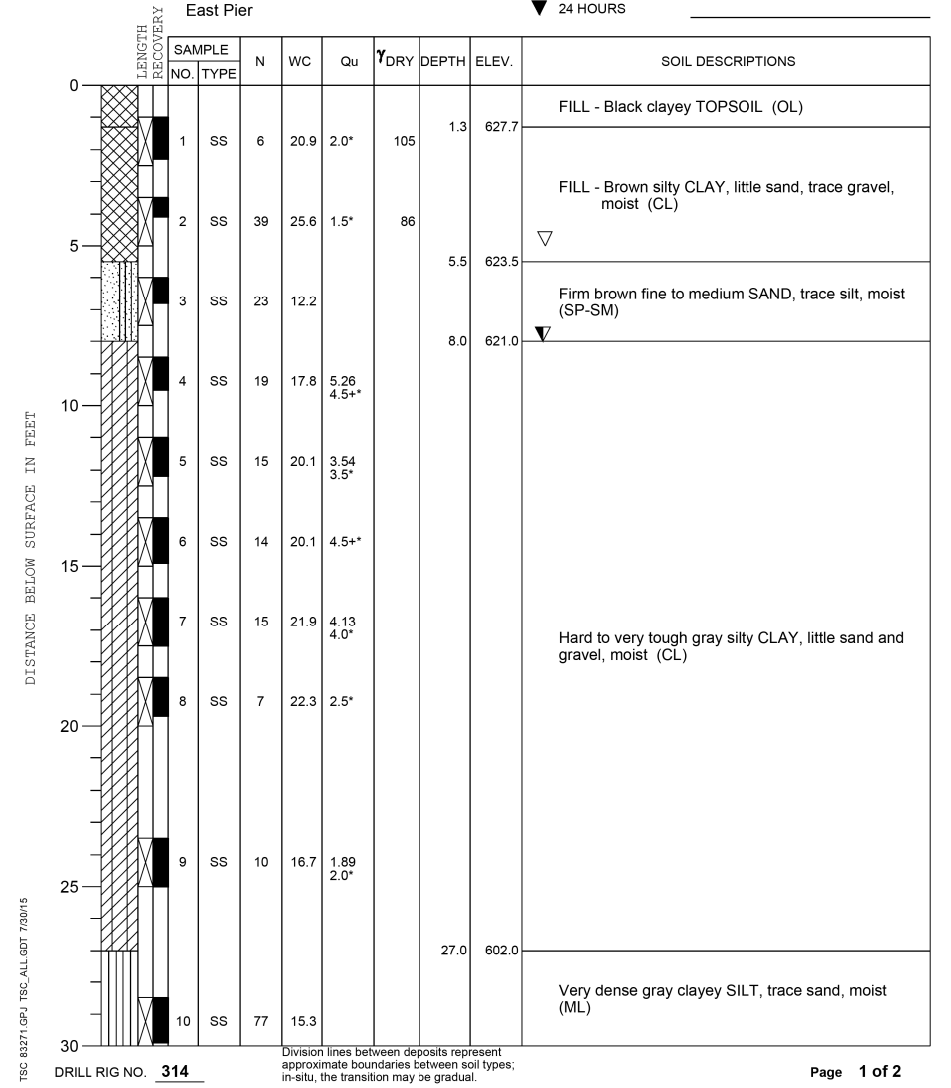
PROJECT **Pedestrian Bridge, Bryn Mawr Ave Path Over Des Plaines River, Rosemont, IL** **TSC**
 CLIENT **Christopher B. Burke Engineering, Ltd., 9575 West Higgins Road, Rosemont, IL**
 BORING **3** DATE STARTED **7-1-15** DATE COMPLETED **7-1-15** JOB **L-83,271**
 ELEVATIONS WATER LEVEL OBSERVATIONS
 GROUND SURFACE **619.0** WHILE DRILLING **8.0'**
 END OF BORING **569.0** AT END OF BORING **6.0'**
 24 HOURS

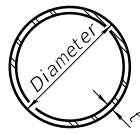


PROJECT **Pedestrian Bridge, Bryn Mawr Ave Path Over Des Plaines River, Rosemont, IL** **TSC**
 CLIENT **Christopher B. Burke Engineering, Ltd., 9575 West Higgins Road, Rosemont, IL**
 BORING **3** DATE STARTED **7-1-15** DATE COMPLETED **7-1-15** JOB **L-83,271**
 ELEVATIONS WATER LEVEL OBSERVATIONS
 GROUND SURFACE **619.0** WHILE DRILLING **8.0'**
 END OF BORING **569.0** AT END OF BORING **6.0'**
 24 HOURS



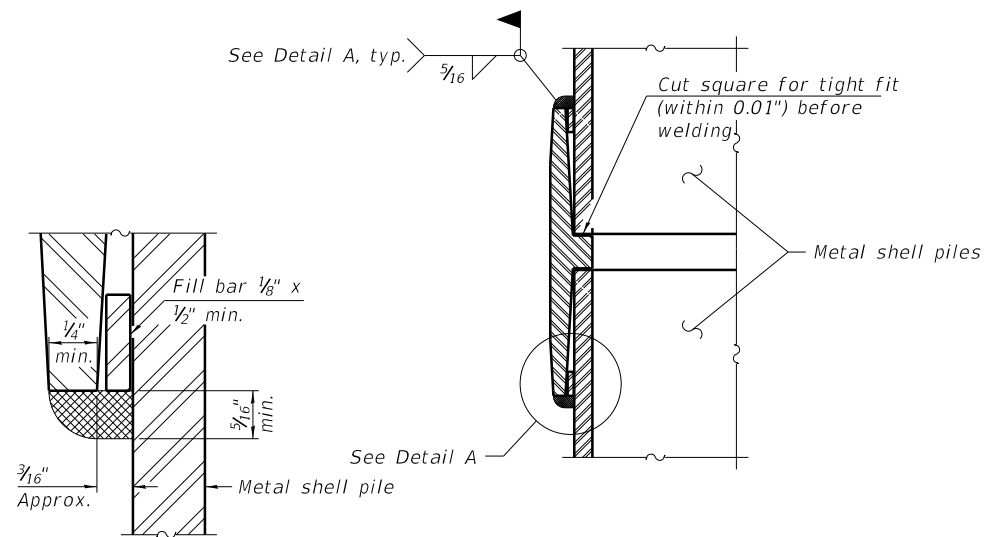
PROJECT **Pedestrian Bridge, Bryn Mawr Ave Path Over Des Plaines River, Rosemont, IL** **TSC**
 CLIENT **Christopher B. Burke Engineering, Ltd., 9575 West Higgins Road, Rosemont, IL**
 BORING **4** DATE STARTED **7-1-15** DATE COMPLETED **7-1-15** JOB **L-83,271**
 ELEVATIONS WATER LEVEL OBSERVATIONS
 GROUND SURFACE **629.0** WHILE DRILLING **8.0'**
 END OF BORING **579.0** AT END OF BORING **5.0'**
 24 HOURS





METAL SHELL PILE TABLE

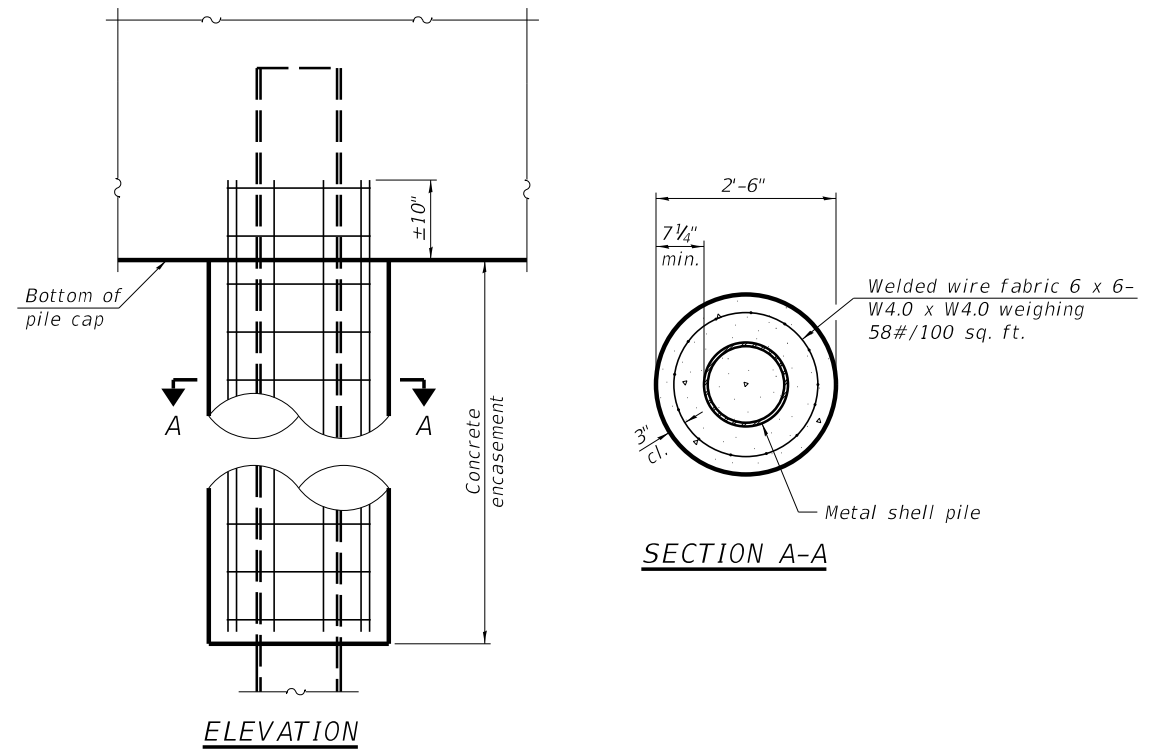
Designation and outside diameter	Wall thickness t	Weight per foot (Lbs./ft.)	Inside volume (yd. ³ /ft.)
PP12	0.250"	31.37	0.0267
PP14	0.250"	36.71	0.0368
PP14	0.312"	45.61	0.0361
PP16	0.312"	52.32	0.0478
PP16	0.375"	62.64	0.0470



DETAIL A

WELDED COMMERCIAL SPLICE

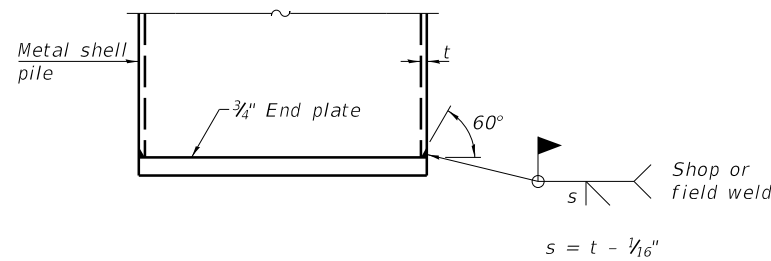
Notes:
 The 1/8" x 1/2" min. fill bar may be constructed of 2 bars with a 1/8" max. gap between them.
 Pile segments shall be driven to solid contact with splicer before welding.



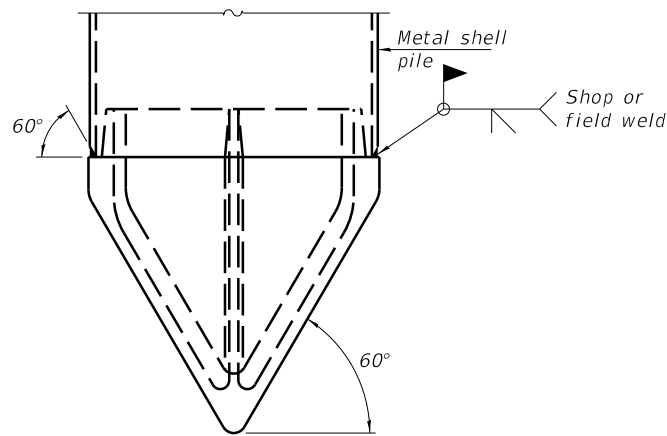
ELEVATION

SECTION A-A

INDIVIDUAL PILE CONCRETE ENCASEMENT AT PIERS

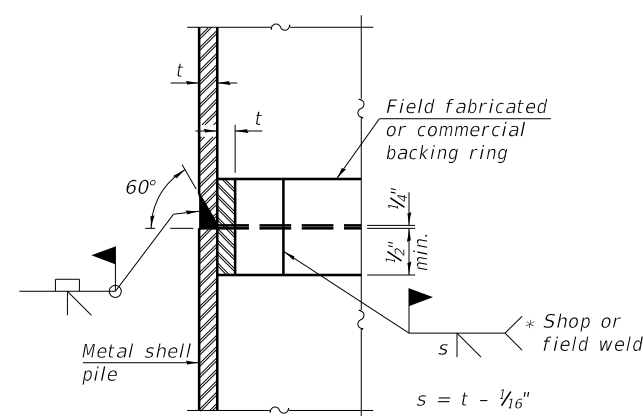


END PLATE ATTACHMENT



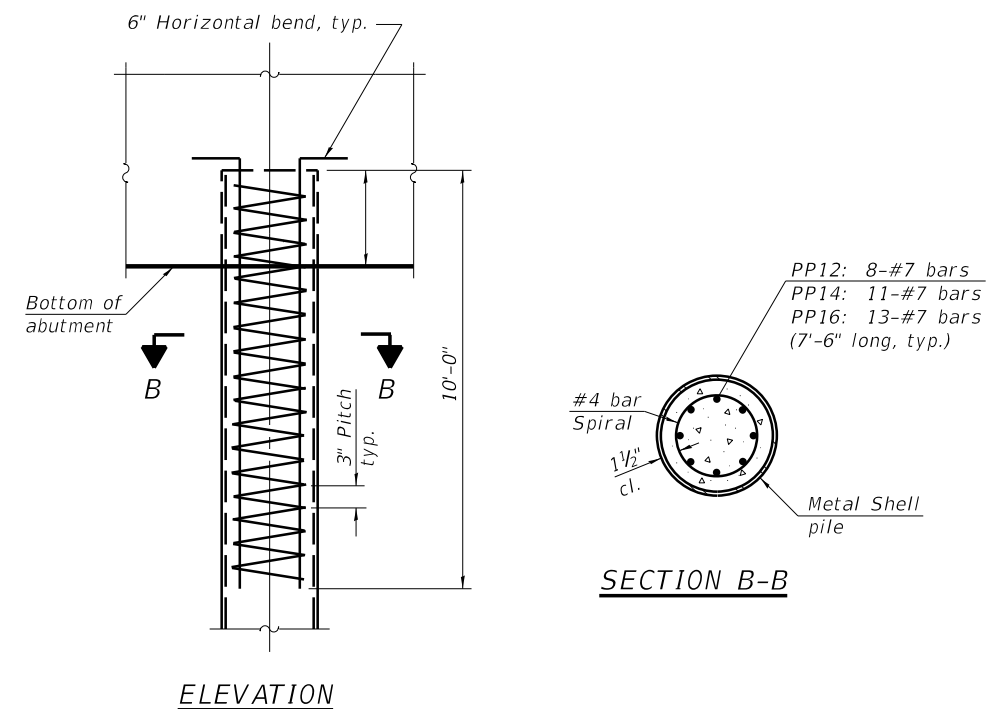
PILE SHOE ATTACHMENT

(When called for on the plans, the Contractor shall furnish metal shell pile shoes consisting of a single piece conical pile point as shown. The pile shoes shall be cast in one piece steel according to either ASTM A 148 Grade 90-60 or AASHTO M 103 Grade 65-35 and shall provide full bearing over the full circumference of the metal shell pile. The pile shoe shall have tapered leads to assure proper alignment and fitting and shall be secured to the pile with a circumferential weld).



COMPLETE PENETRATION WELD SPLICE

* Field fabricated backing ring may be made from pile shell by removing segment to allow reducing circumference and vertically rejoin with partial joint penetration weld.



ELEVATION

SECTION B-B

REINFORCEMENT AT ABUTMENTS

Note:
 The metal shell piles shall be according to Article 1006.05 of the Standard Specifications.

F-MS 2-17-2017

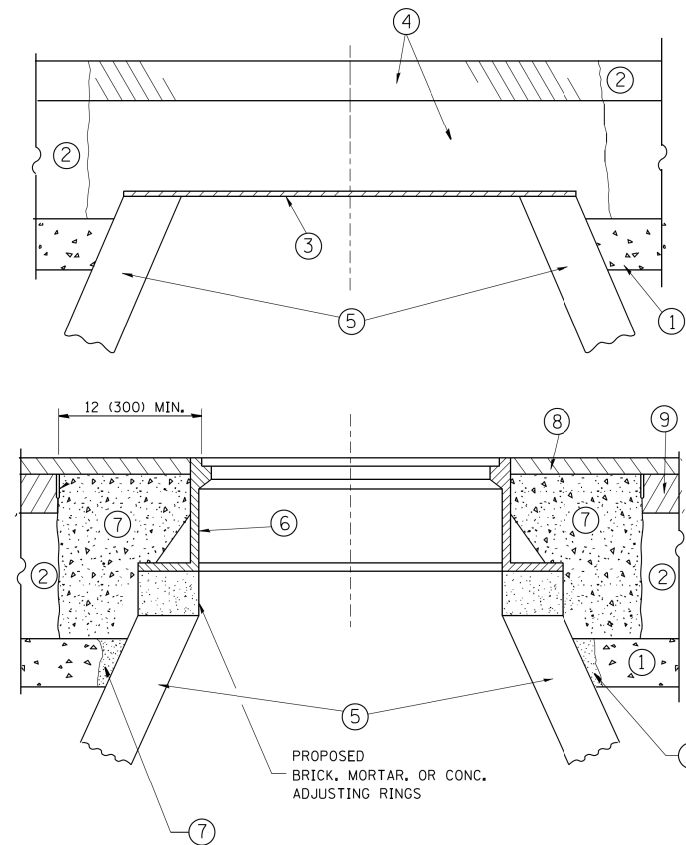
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	PLOT DATE = 3/19/2020	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**BRYN MAWR BRIDGE AND RIVERWALK
 METAL SHELL PILE DETAILS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	19-00036-00-BR	COOK	36	28
CONTRACT NO. 61G51				
ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

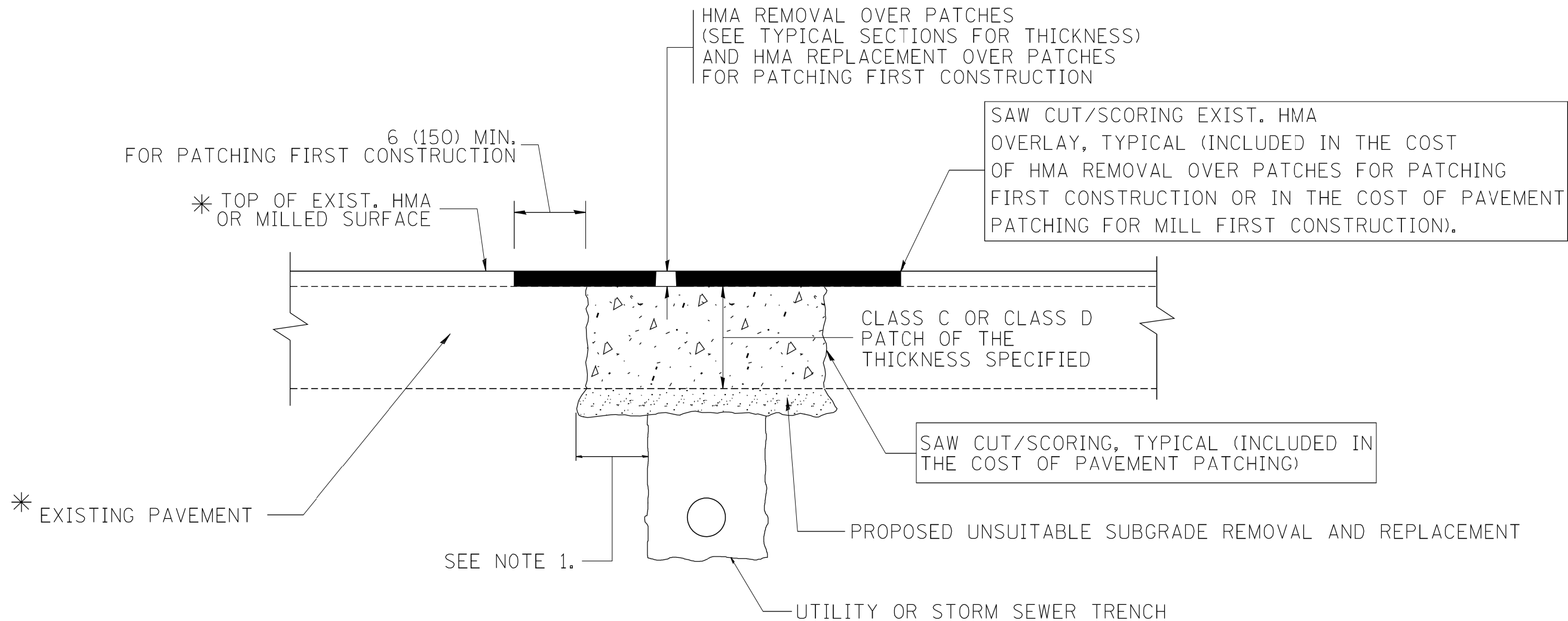
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	19-00036-00-BR	COOK	36	29
BD600-03 (BD-8)		CONTRACT NO.	61651	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - R. BORO 01-01-07
		CHECKED -	REVISED - R. BORO 09-04-07
		DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	19-00036-00-BR	COOK	36	30
BD400-04 (BD-22)			CONTRACT NO. 61651	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) * *

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

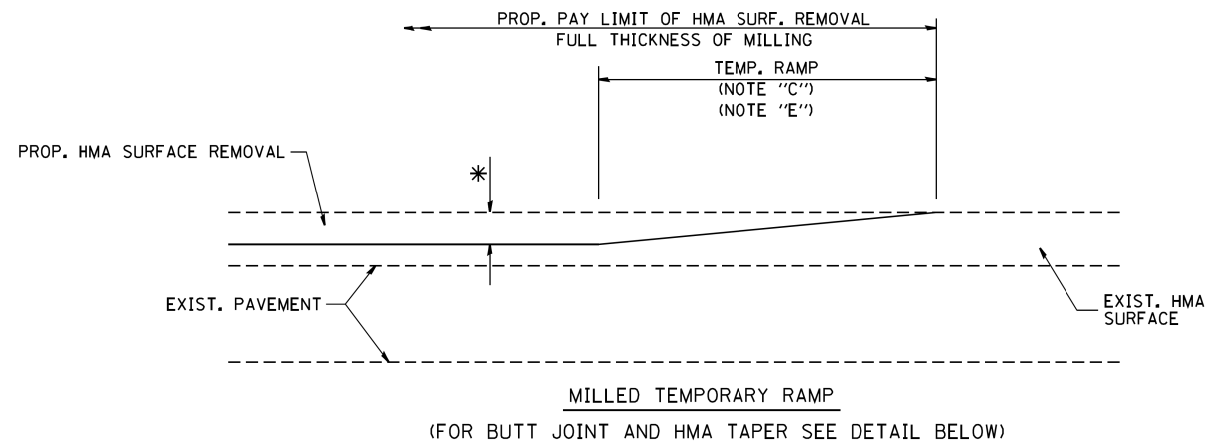
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

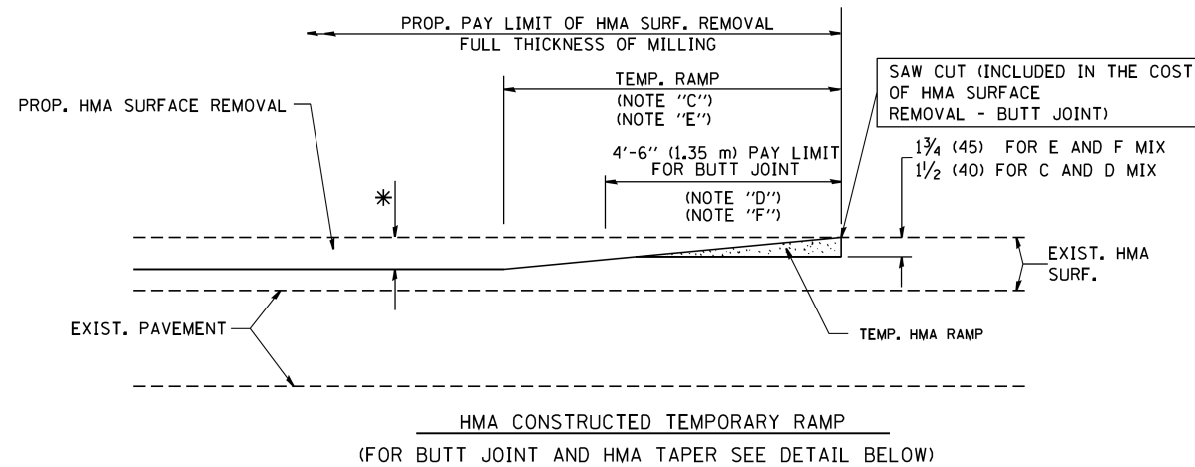
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A. -	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01						BD600-06 (BD-24)				CONTRACT NO. 61651
	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.		TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

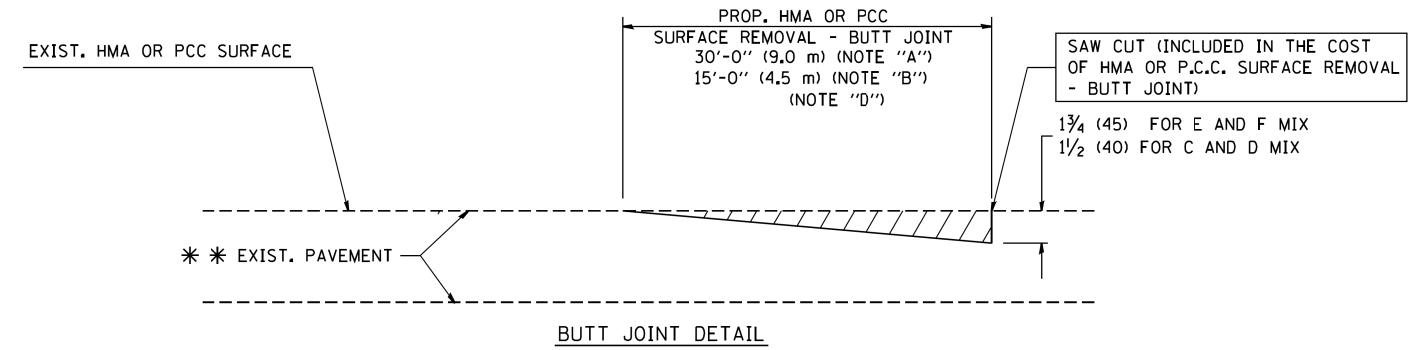


OPTION 1

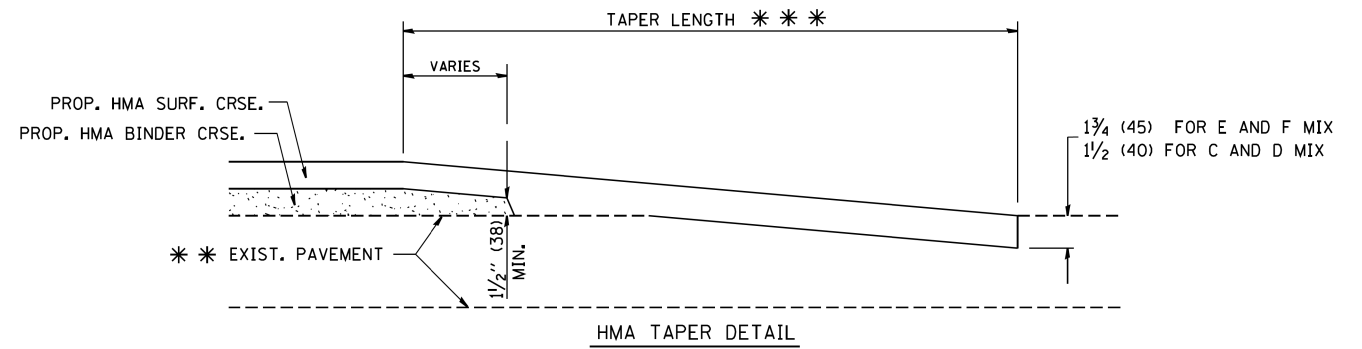


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

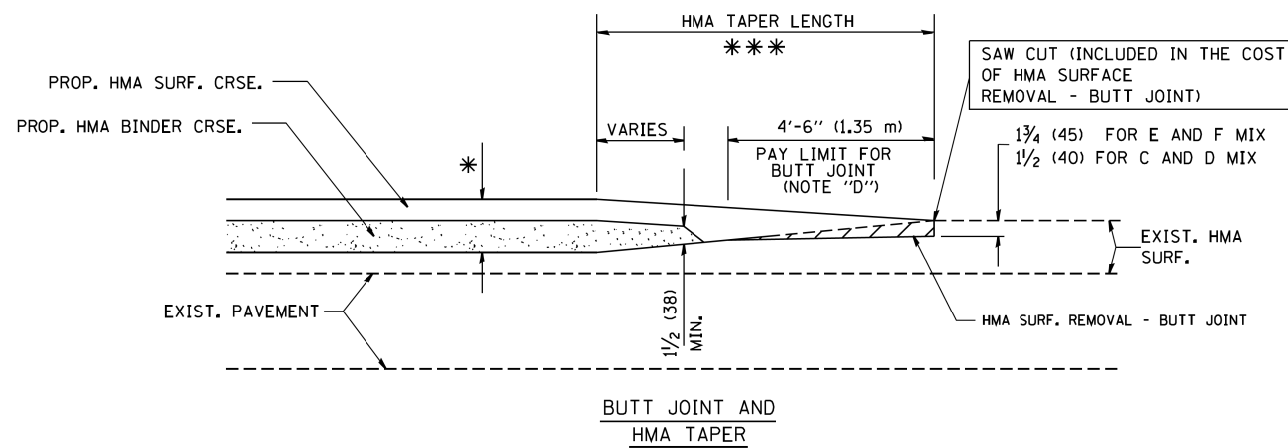
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

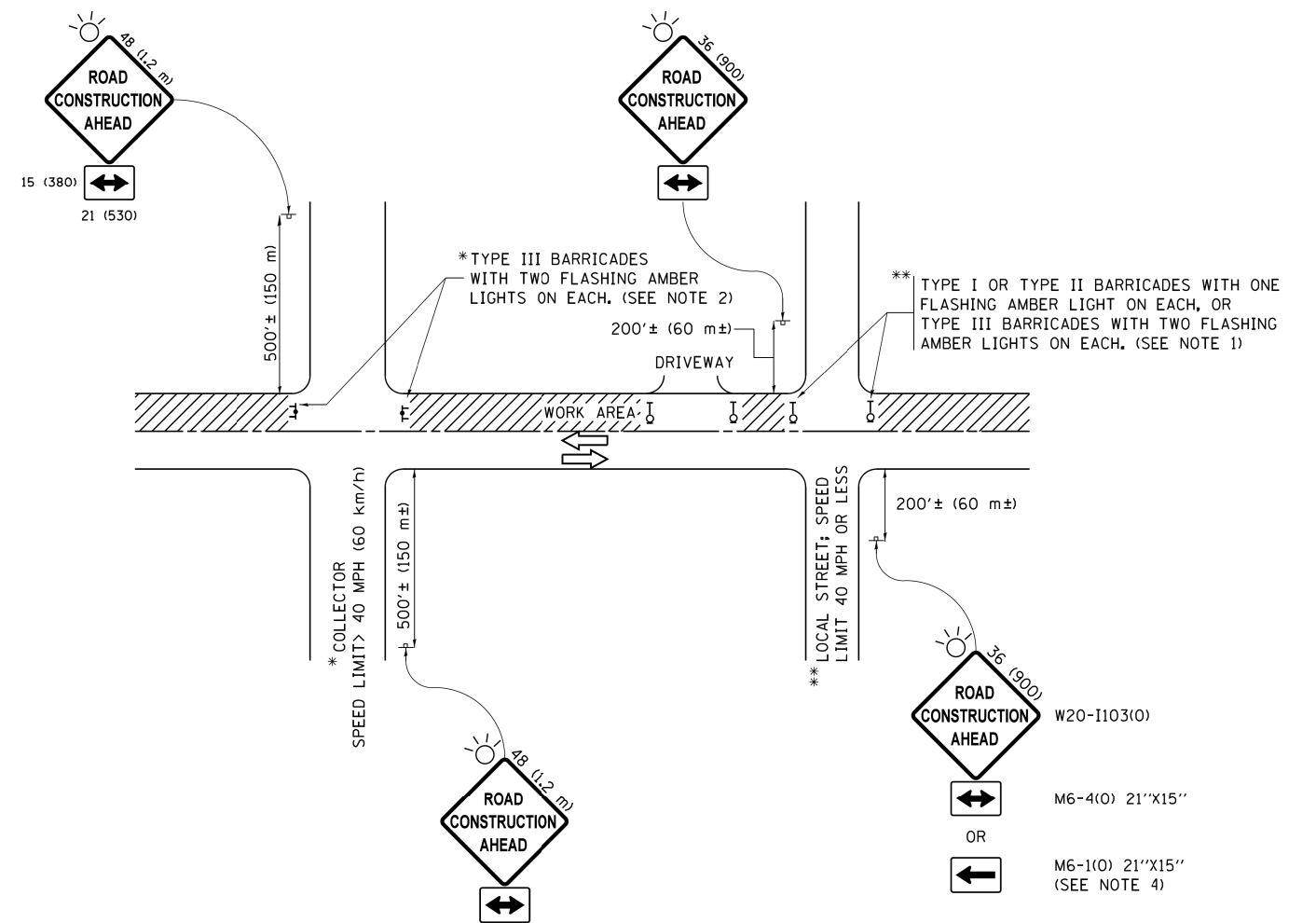
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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50,0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	19-00036-00-BR	COOK	36	32
BD400-05 BD32		CONTRACT NO. 61651		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

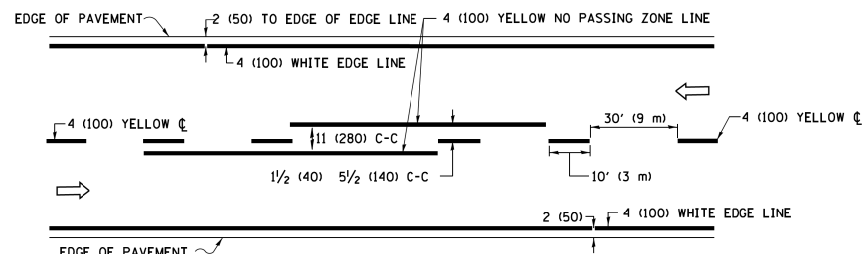
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

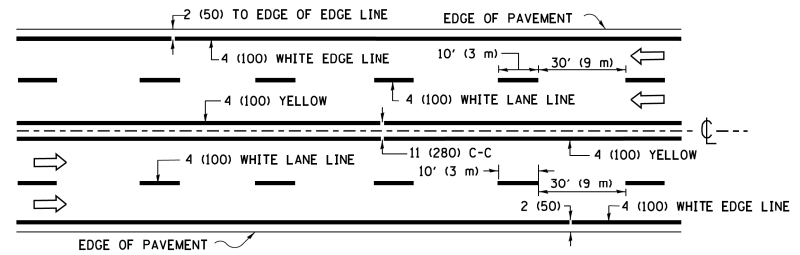
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

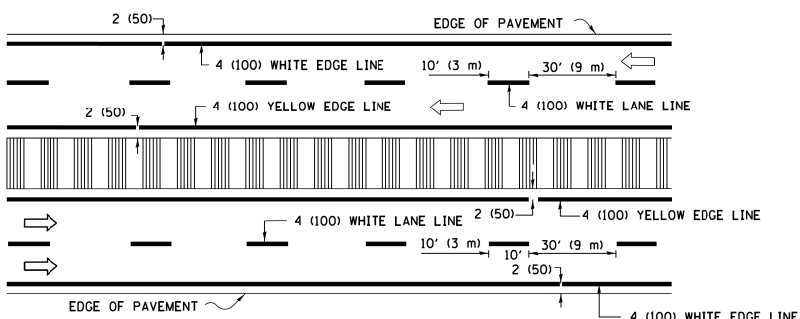
F.A.-RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	19-00036-00-BR	COOK	36	33
TC-10			CONTRACT NO. 61651	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

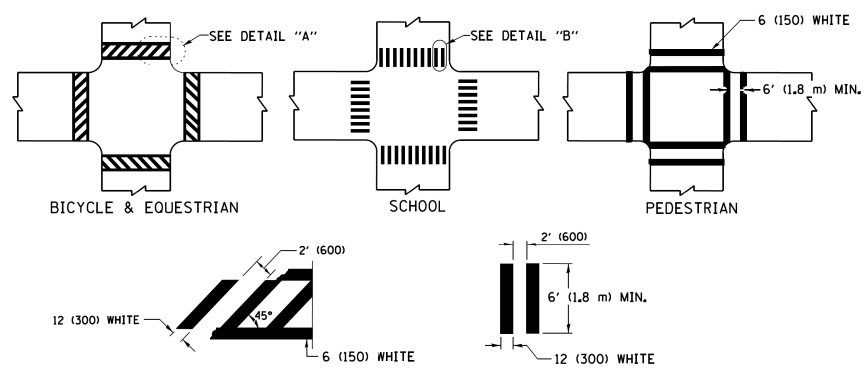


MULTI-LANE UNDIVIDED



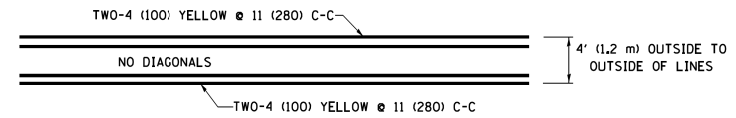
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

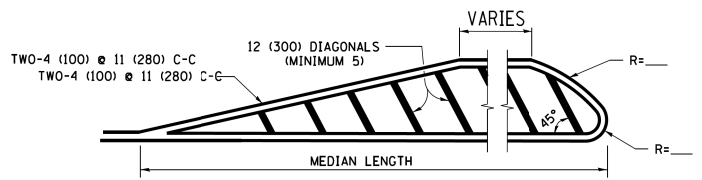


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

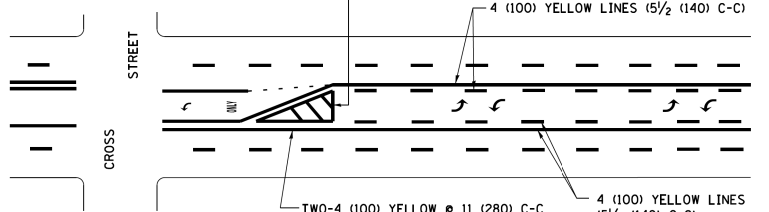


4' (1.2 m) WIDE MEDIANS ONLY



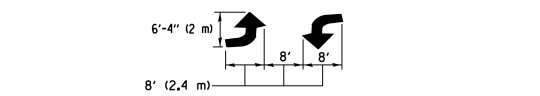
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

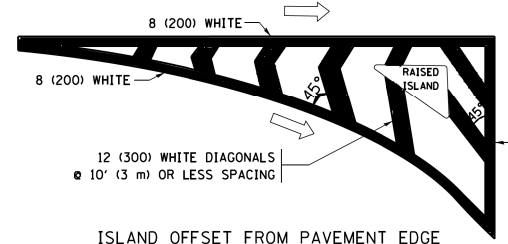
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



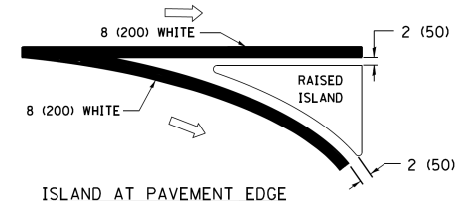
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

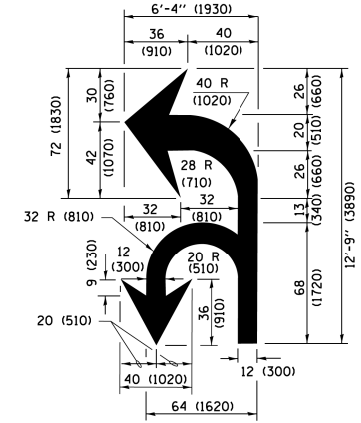


ISLAND OFFSET FROM PAVEMENT EDGE

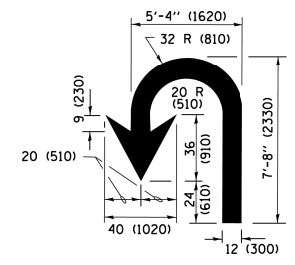


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

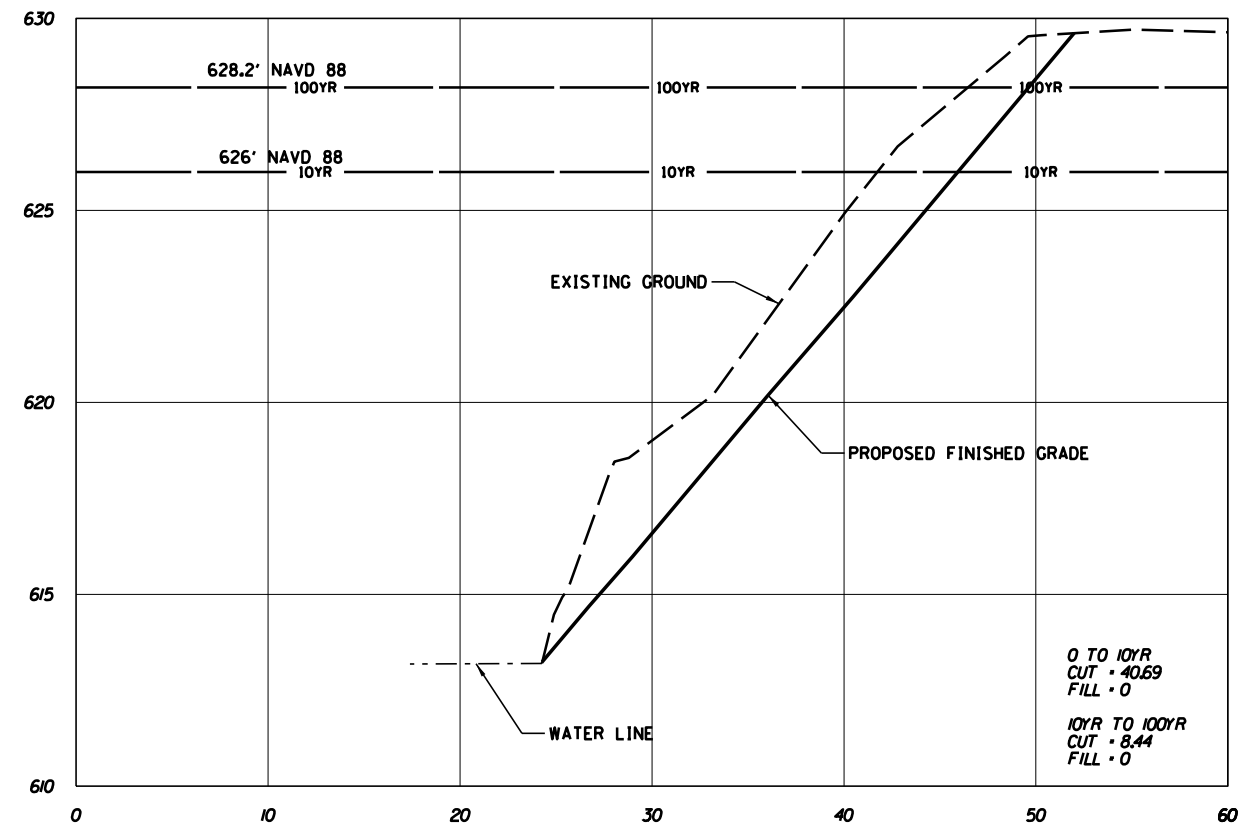
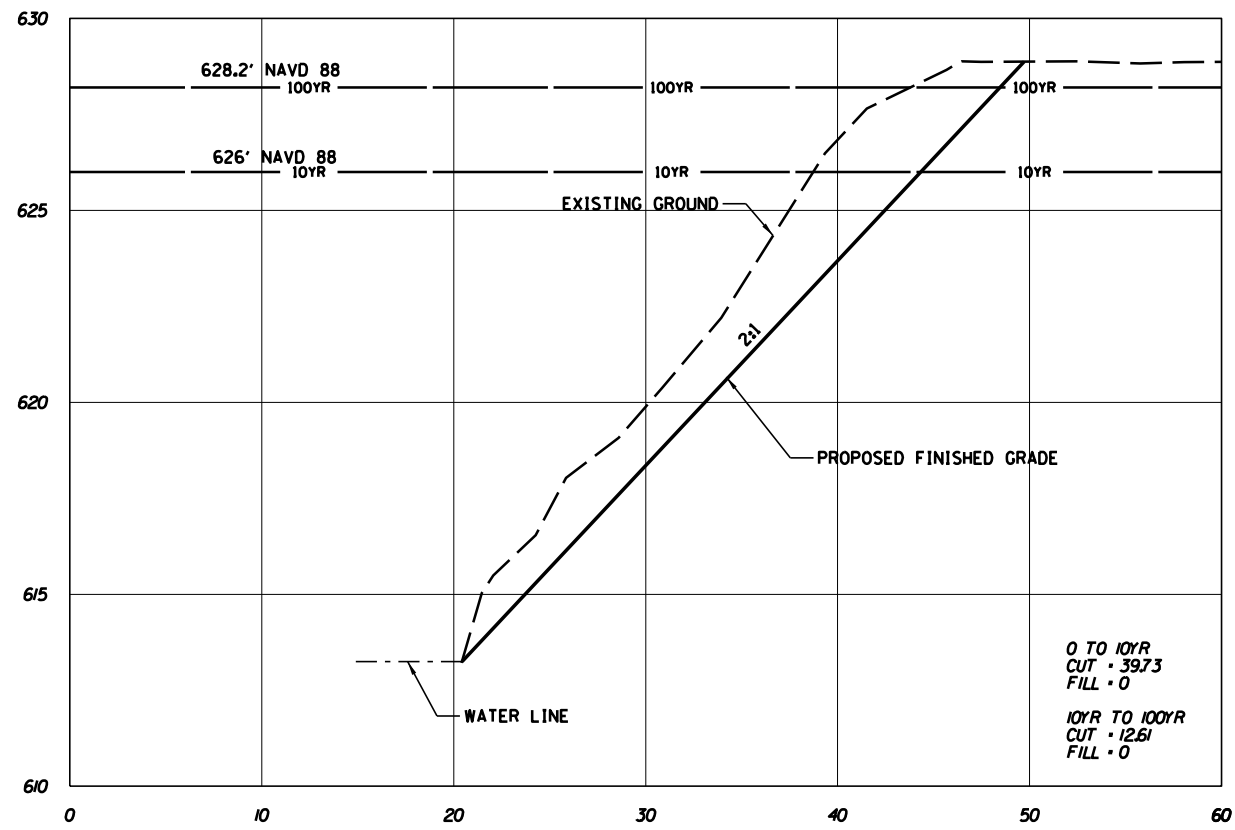
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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.-RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	19-00036-00-BR	COOK	36	34
	TC-13	CONTRACT NO.	61651	
ILLINOIS FED. AID PROJECT				



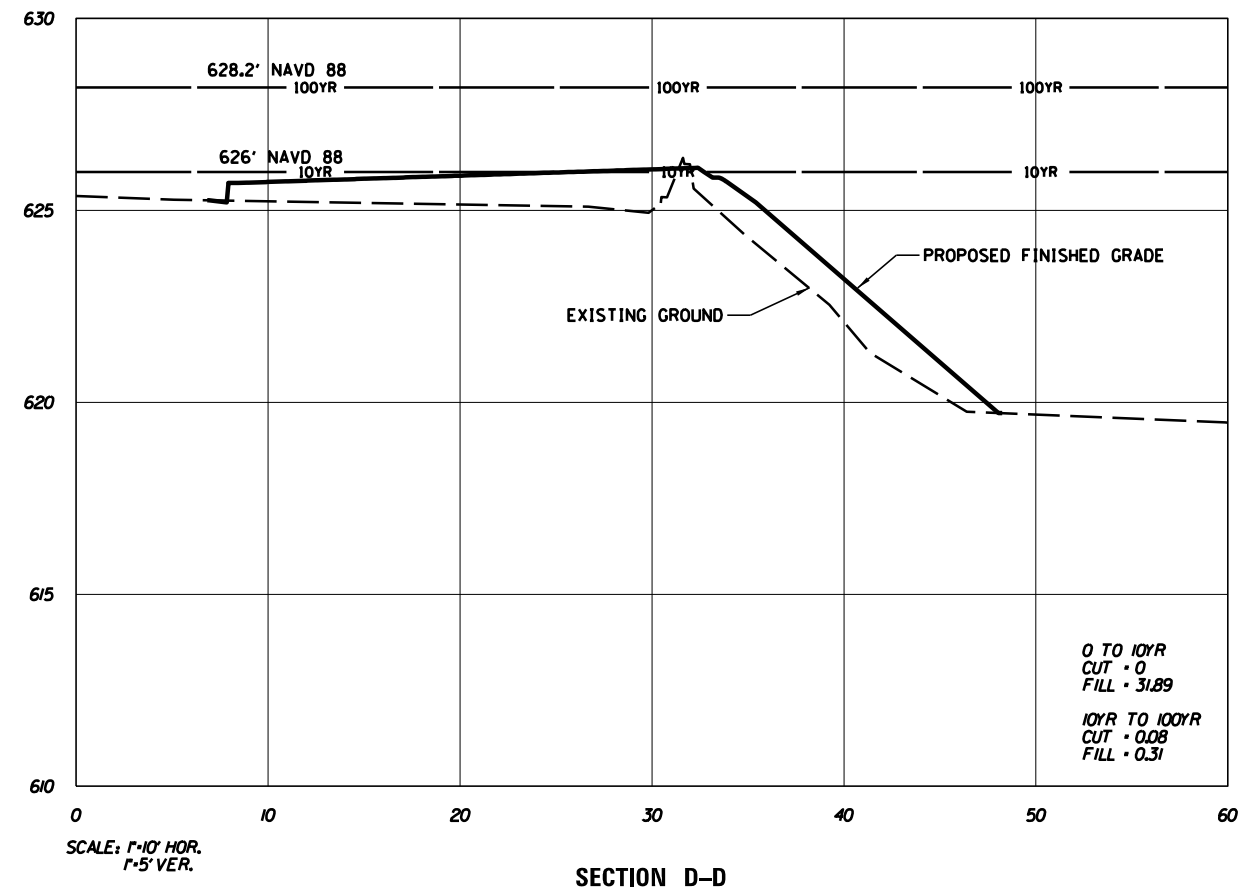
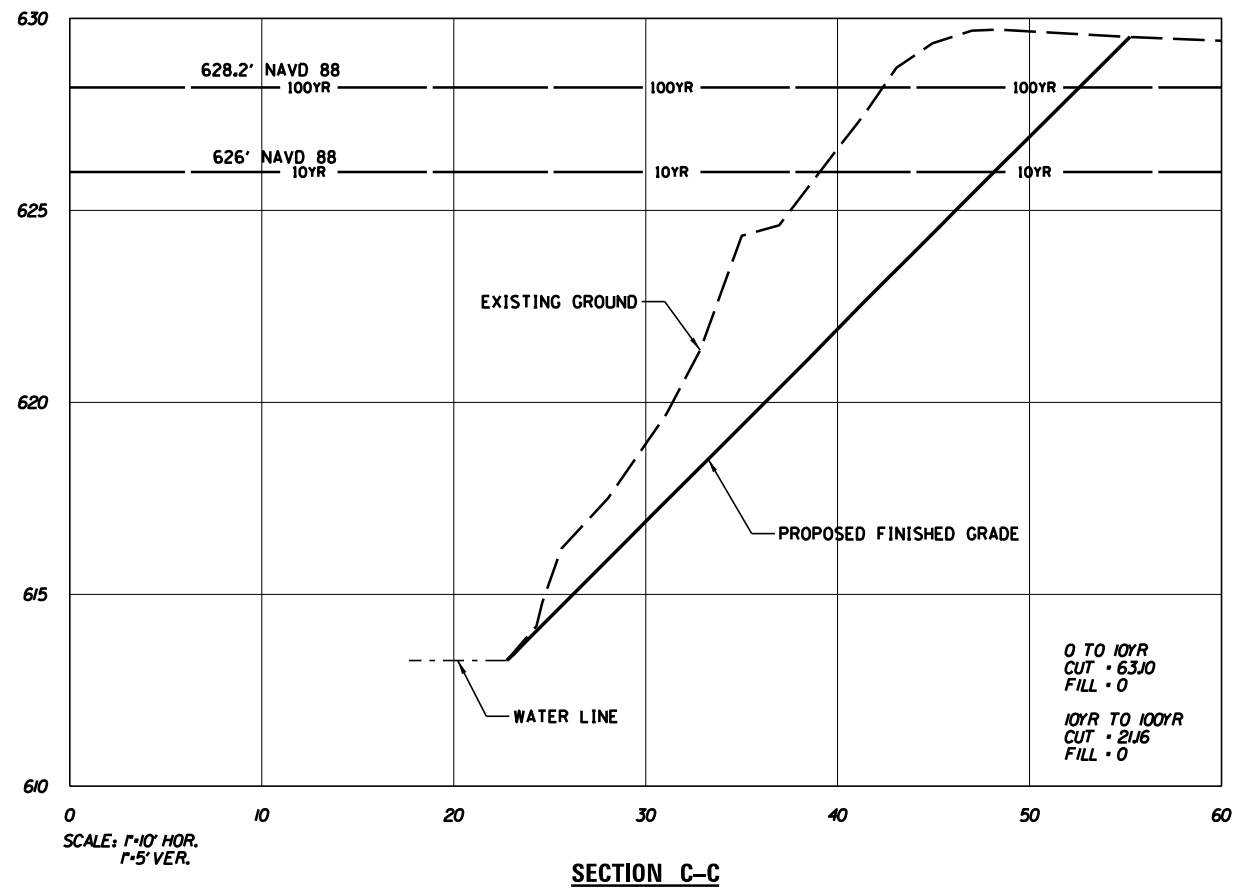
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BRYN MAWR BRIDGE AND RIVERWALK
CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	19-00036-00-BR	COOK	36	35
CONTRACT NO. 61G51				
ILLINOIS FED. AID PROJECT				



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BRYN MAWR BRIDGE AND RIVERWALK
CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	19-00036-00-BR	COOK	36	36
CONTRACT NO. 61G51			ILLINOIS FED. AID PROJECT	