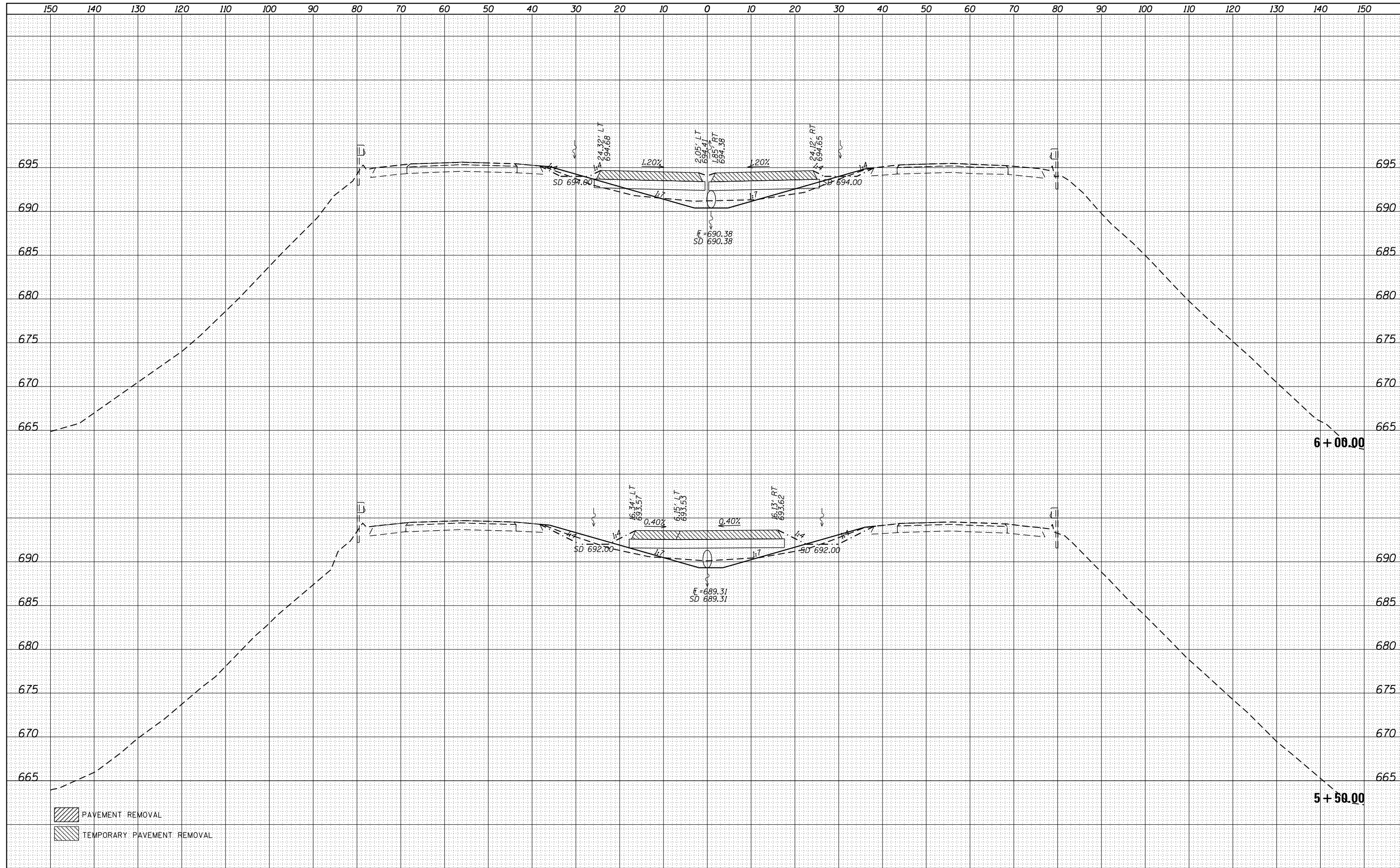


DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



PAVEMENT REMOVAL  
 TEMPORARY PAVEMENT REMOVAL

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USER NAME = kcorider  
 PLOT SCALE = 20.0000' / in.  
 PLOT DATE = 12/11/2019 - 4:44:54 PM

DESIGNED -	REVISIED -
DRAWN -	REVISIED -
CHECKED -	REVISIED -
DATE -	REVISIED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

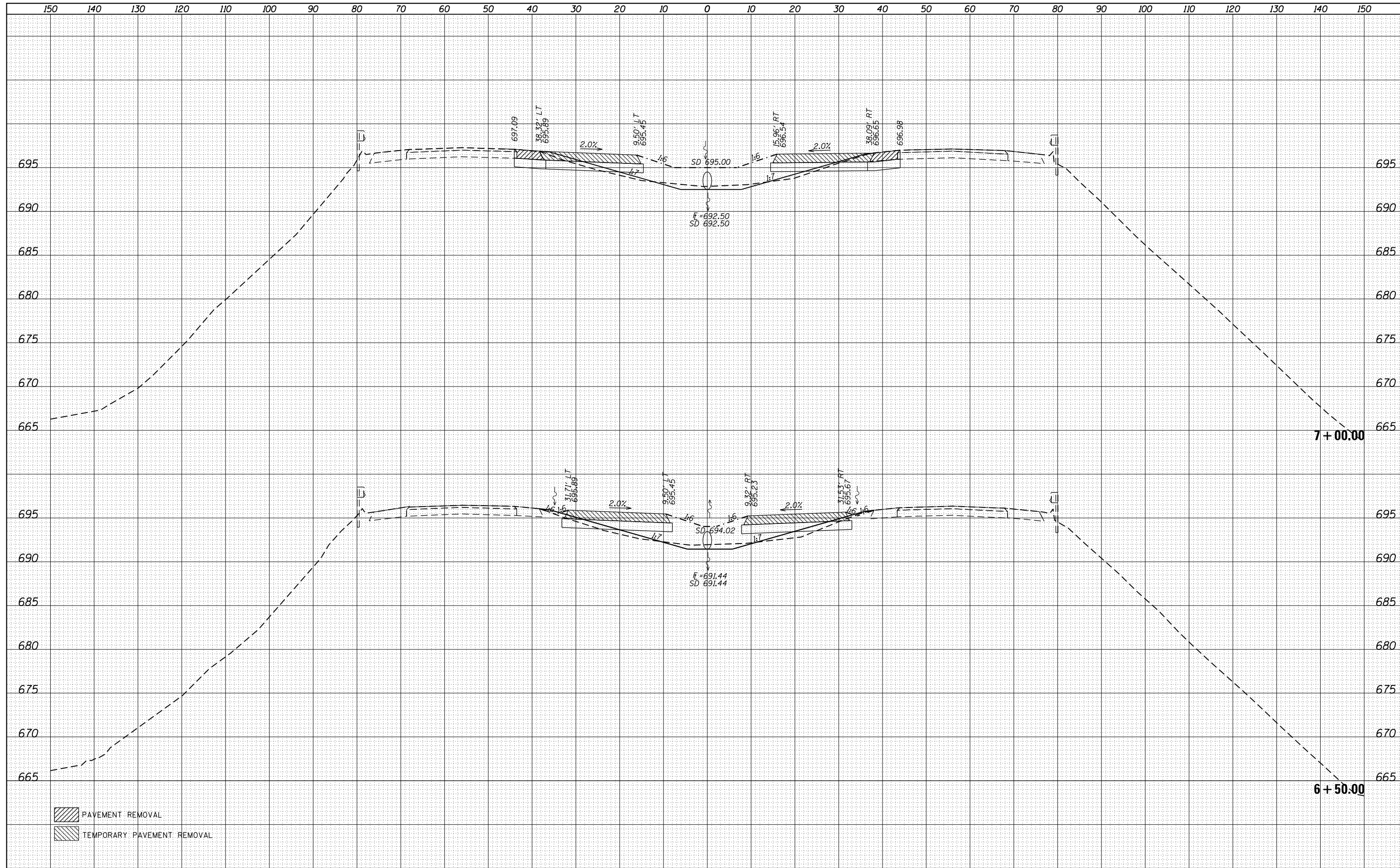
CROSS OVERS 1 & 2  
CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. 5+50.00 TO STA. 6+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	168-1.3 RS-3.68-2 RS5)BR	MONTGOMERY	307	101
CONTRACT NO. 72031			ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED



PAVEMENT REMOVAL  
 TEMPORARY PAVEMENT REMOVAL

FILE NAME = ...\\D672031-sht-xsc-crossover.dgn  
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PLOT DATE = 12/11/2019 - 4:44:54 PM	CHECKED -	REVISÉ -
	DATE -	REVISÉ -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

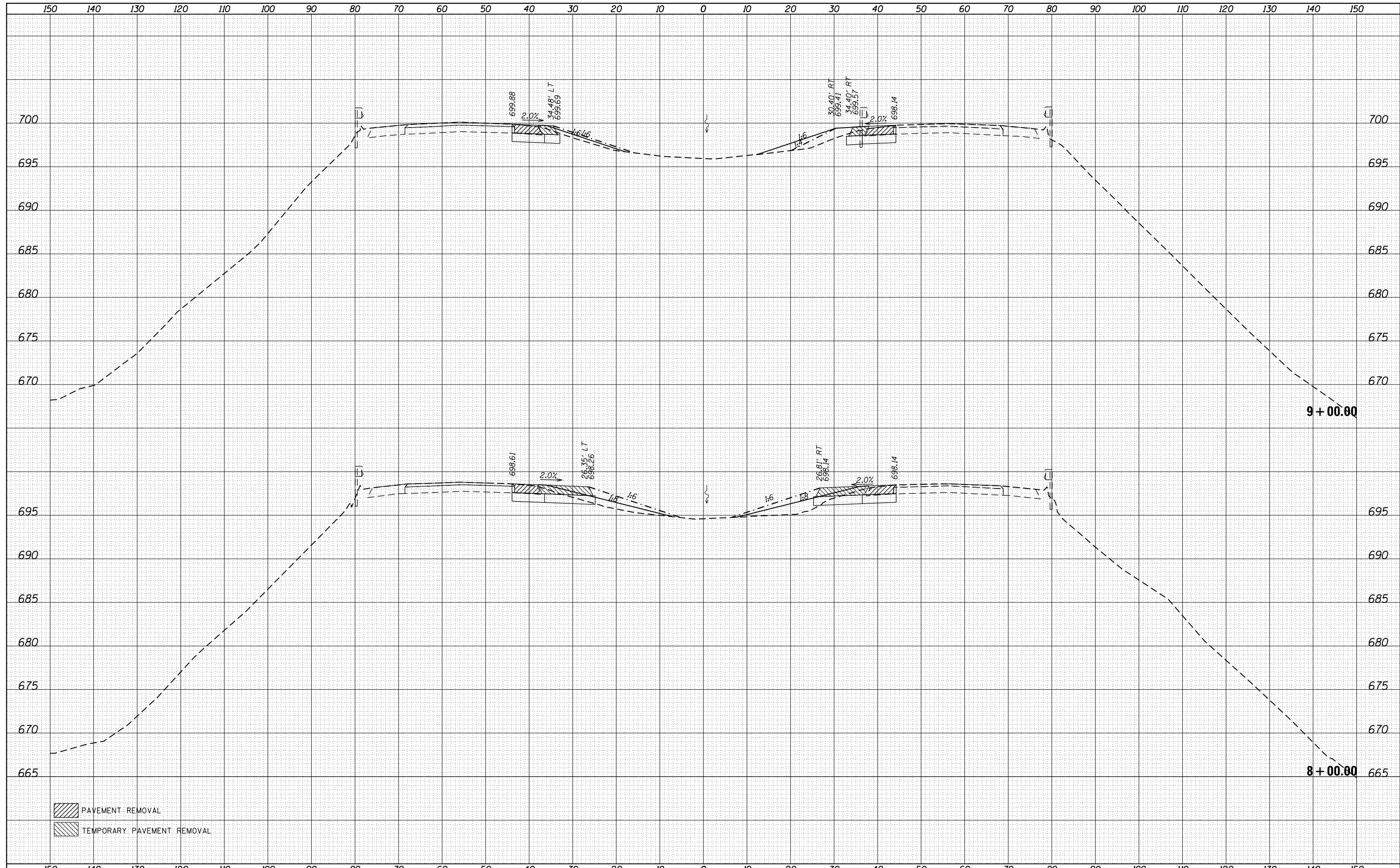
**CROSS OVERS 1 & 2**  
**CROSS SECTIONS**

SCALE: SHEET OF SHEETS STA. 6+50.00 TO STA. 7+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	168-1.3 RS-3.68-2 RS5)BR	MONTGOMERY	307	102
CONTRACT NO. 72031			ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED



PAVEMENT REMOVAL  
 TEMPORARY PAVEMENT REMOVAL

FILE NAME = ...\\D672031-sht-xsc-crossover.dgn  
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	DATE -	REVISÉ -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

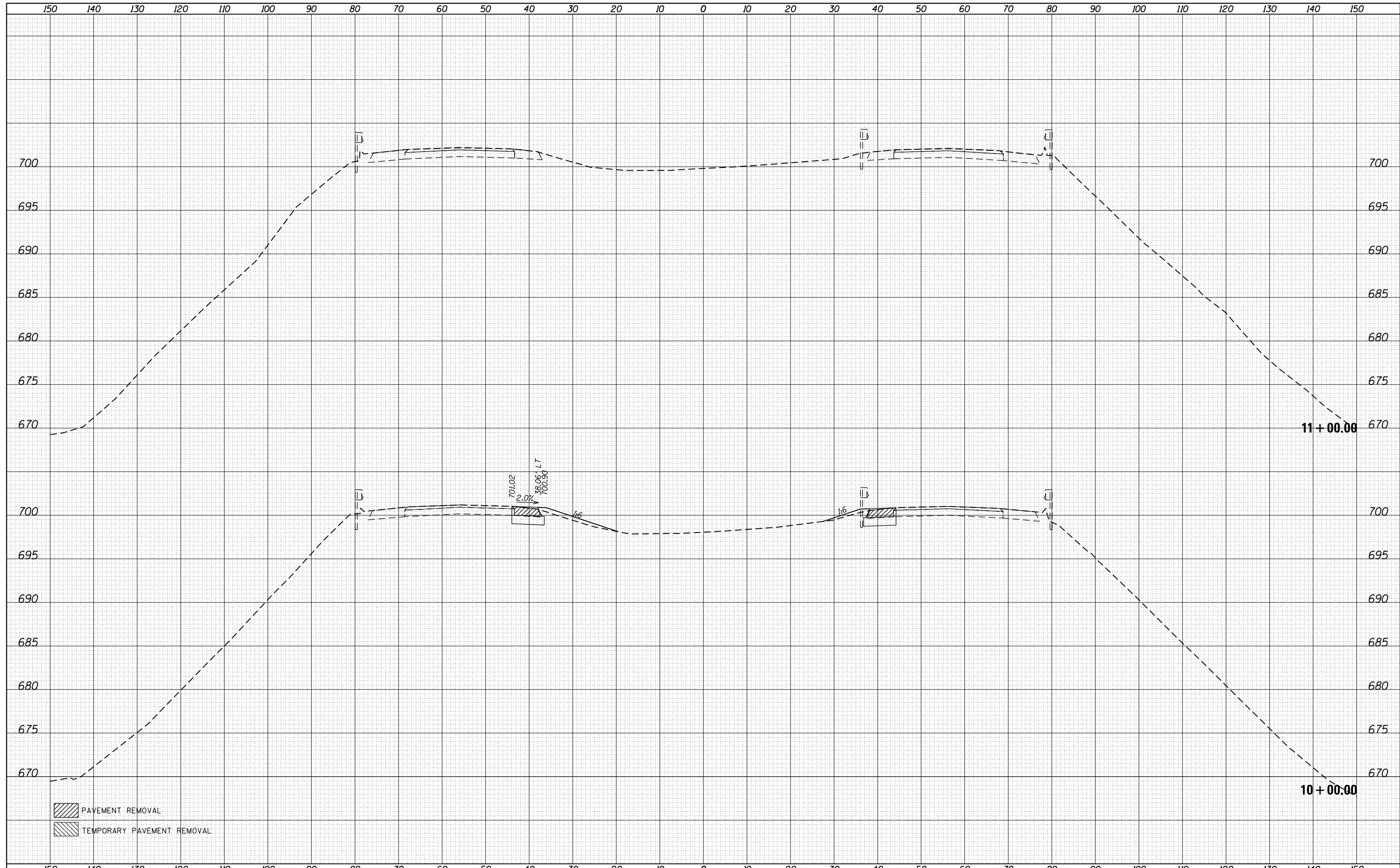
**CROSS OVERS 1 & 2  
CROSS SECTIONS**



SCALE: SHEET OF SHEETS STA. 8+00.00 TO STA. 9+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	168-1.3 RS-3.68-2 RS5)BR	MONTGOMERY	307	103
CONTRACT NO. 72031			ILLINOIS FED. AID PROJECT	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



 PAVEMENT REMOVAL  
 TEMPORARY PAVEMENT REMOVAL

FILE NAME = ...D672031-sht-xsc-crossover.dgn  
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	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

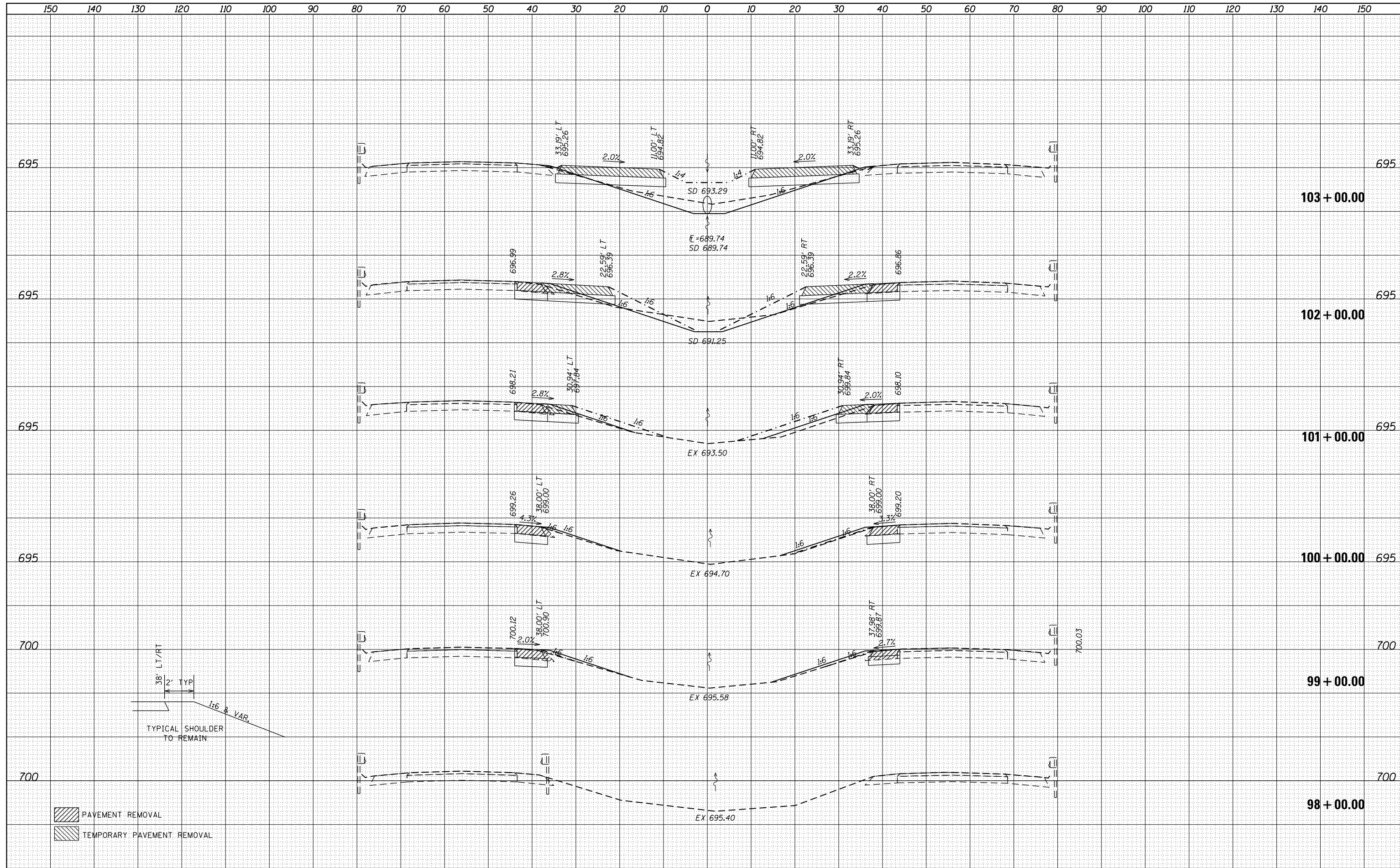
**CROSS OVERS 1 & 2**  
**CROSS SECTIONS**

SCALE: SHEET OF SHEETS STA. 10+00.00 TO STA. 11+00.00

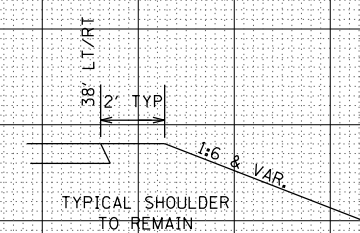
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55	168-1.3 RS-3.68-2 RS5)BR	MONTGOMERY	307	104
CONTRACT NO. 72031			ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
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PLOTTED	
TEMPLATE	
NOTE BOOK	
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PAVEMENT REMOVAL  
 TEMPORARY PAVEMENT REMOVAL



FILE NAME = ...D672031-sht-xsc-crossover.dgn

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

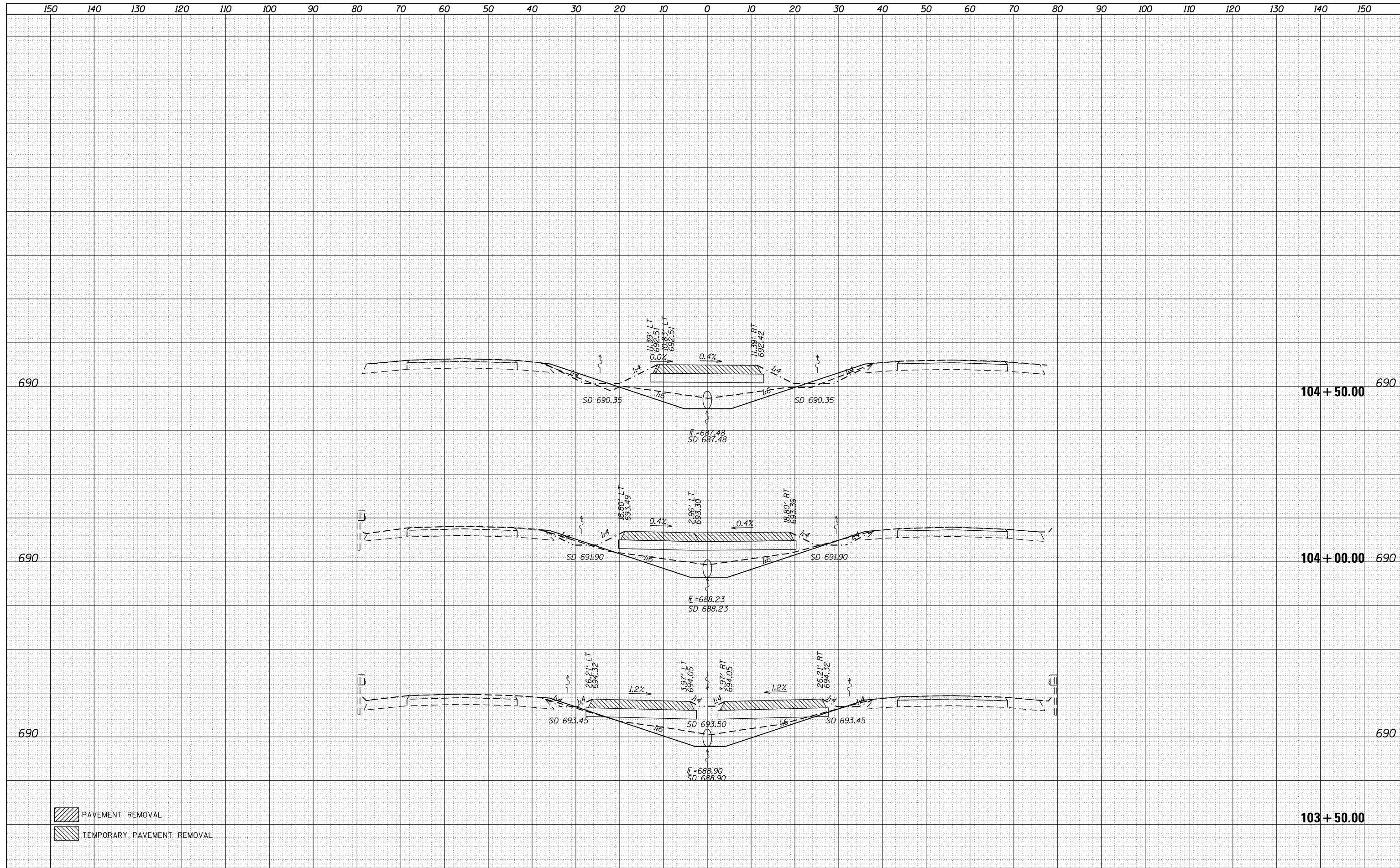
**CROSS OVERS 3 & 4  
CROSS SECTIONS**

SCALE: SHEET OF SHEETS STA. 98+00.00 TO STA. 103+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	168-1.3 RS-3.68-2 RSSIBR	MONTGOMERY	307	105
CONTRACT NO. 72031				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

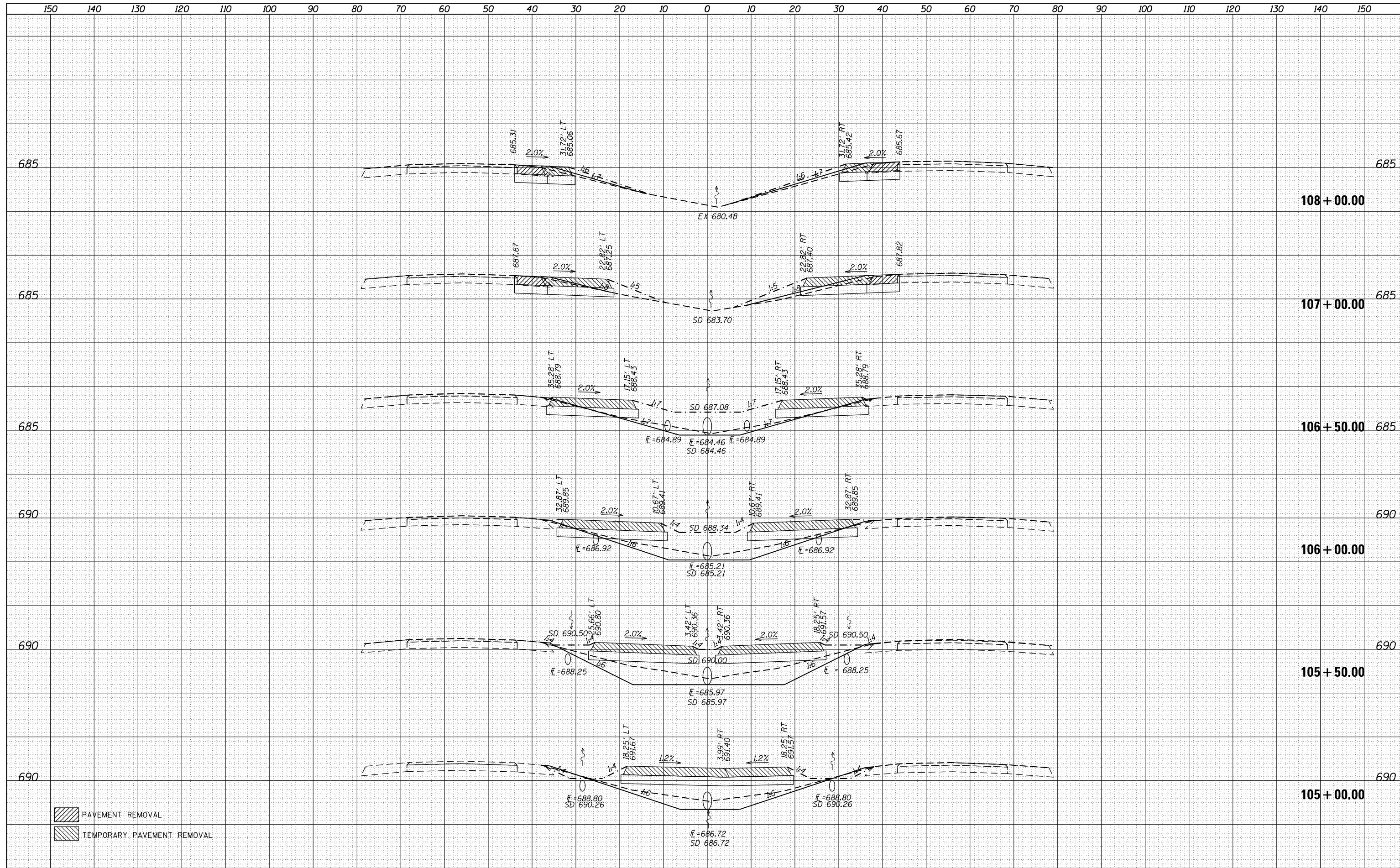
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BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
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ORIGINAL SURVEY	
NOTE BOOK	
NO.	



PAVEMENT REMOVAL  
 TEMPORARY PAVEMENT REMOVAL

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
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NOTE BOOK	
AREAS CHECKED	
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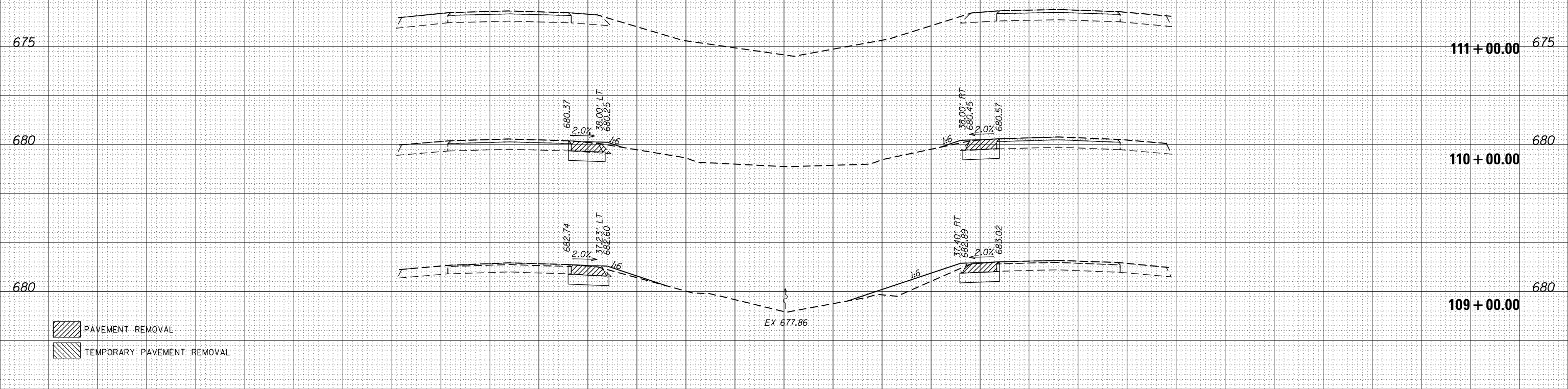


PAVEMENT REMOVAL  
 TEMPORARY PAVEMENT REMOVAL

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED



PAVEMENT REMOVAL  
 TEMPORARY PAVEMENT REMOVAL

FILE NAME = ...\\D672031-sht-xsc-crossover.dgn  
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USER NAME = kcorider	DESIGNED -	REVISIED -
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	DATE -	REVISIED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

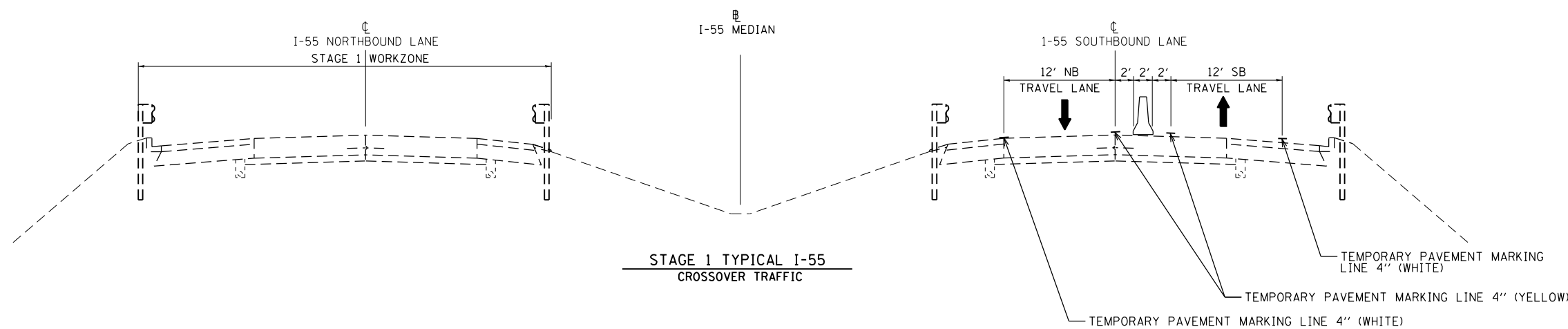
CROSS OVERS 3 & 4  
 CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. 109+00.00 TO STA. 111+00.00

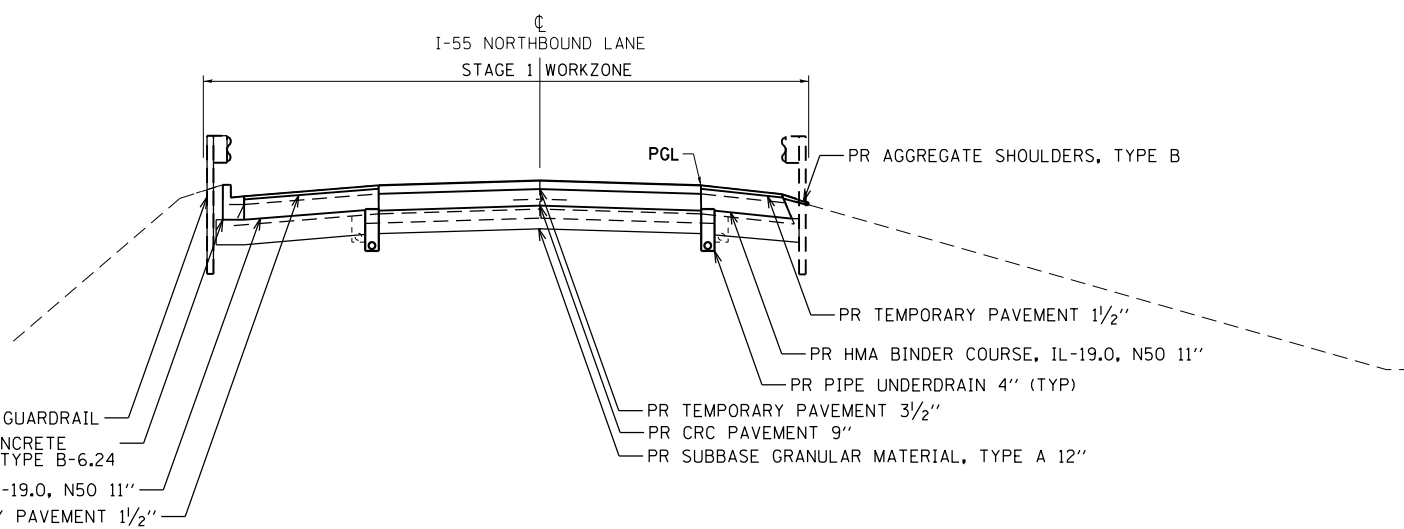
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	168-1,3 RS-3,68-2 RS5)BR	MONTGOMERY	307	108
			CONTRACT NO. 72031	

ILLINOIS FED. AID PROJECT





STAGE 1 TYPICAL I-55  
CROSSOVER TRAFFIC



STAGE 1 TYPICAL I-55  
FULL DEPTH PAVEMENT

STA. 12+75.00 TO STA. 17+60.00  
STA. 92+00.00 TO STA. 96+30.00  
BRIDGE (SN 068-0049) OMISSION STA 93+02.01 TO 95+28.84

**STAGE 1 TRAFFIC**  
I-55 PRIOR TO CROSSOVER - MAINTAIN ONE LANE OF TRAFFIC IN BOTH NB AND SB DIRECTIONS.  
I-55 CROSSOVER - MAINTAIN ONE LANE OF TRAFFIC IN BOTH NB AND SB DIRECTIONS. CROSS NB TRAFFIC OVER ONTO I-55 SB.  
WEIGH STATION - MAINTAIN ACCESS (BOTH ENTERING AND EXITING) AT ALL TIMES.

**ROADWAY CONSTRUCTION**  
I-55 PRIOR TO CROSSOVER - CONSTRUCT NB OUTSIDE LANE AND SHOULDER AND SB INSIDE LANE AND SHOULDER.  
I-55 CROSSOVER SEGMENT - CONSTRUCT NB I-55 PAVEMENT AND SHOULDERS.  
SEE PROPOSED TYPICAL SECTION SHEETS FOR DETAILED INFORMATION.

**STAGE GENERAL NOTES**  
SEE STRUCTURE PLANS FOR BRIDGE STAGING.  
SEE CROSS ROAD STAGING PLANS AND DETOURS.

PRIMARY IDOT STANDARDS THIS STAGE  
(SEE SPECIAL PROVISIONS FOR ADDITIONAL APPLICABLE STANDARDS)  
701101, 701106, 701101, 701106, 701400, 701401, 701406, 701411, 701416, 701426, 701428, 701451, 701456, 701901

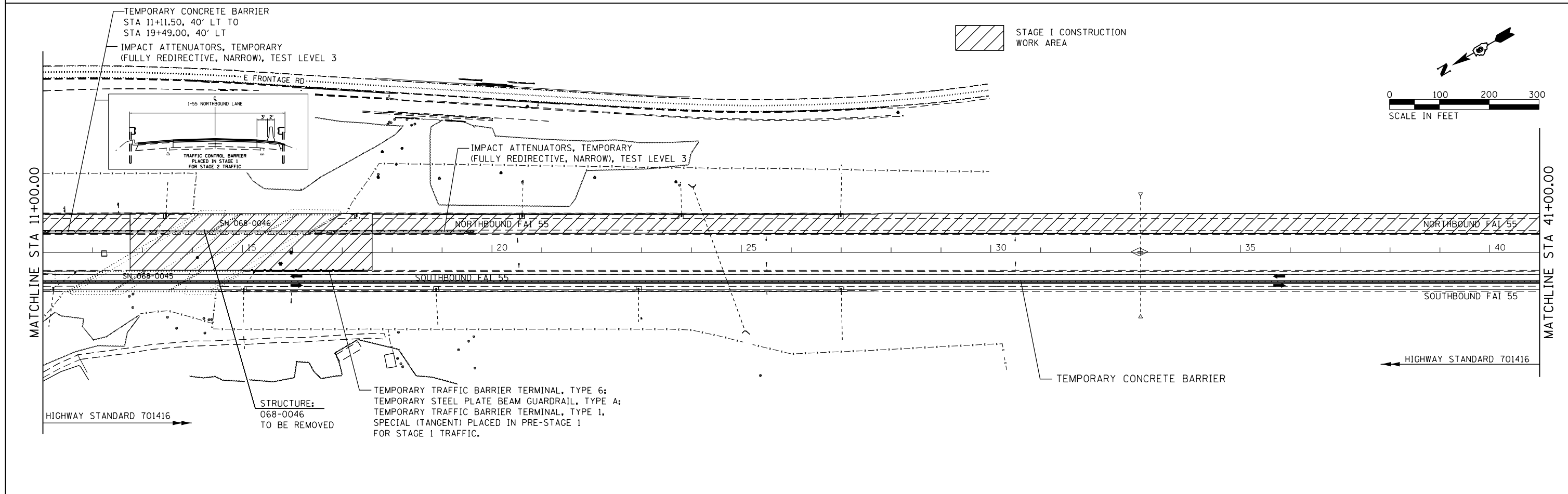
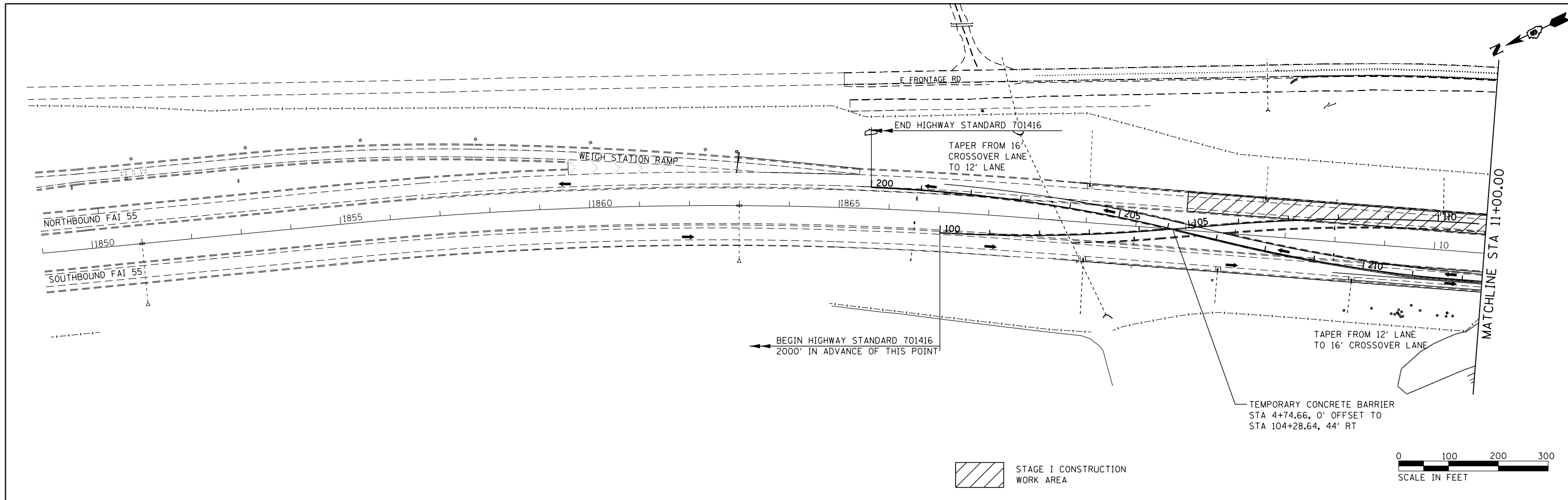
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	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

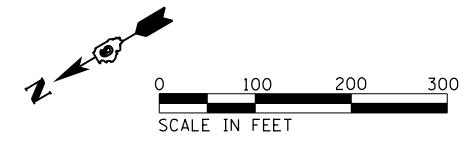
**MAINTENANCE OF TRAFFIC  
I-55 STAGE 1**

SCALE: N.T.S.    SHEET NO.    OF    SHEETS    STA.    TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	109
CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	



FILE NAME = D672031-sh1-MOT-155-Stage1.dgn	USER NAME = korider	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MAINTENANCE OF TRAFFIC I-55 STAGE 1</b>			F.A.I. RTE. 55	SECTION 468-1,3 RS-3,68-2 RS-5)BR	COUNTY MONTGOMERY	TOTAL SHEETS 307	SHEET NO. 110	
	PLOT SCALE = 200.0000' / in.	DRAWN -	REVISED -		SCALE: 1" = 100'	SHEET NO.	OF SHEETS	STA. 1849+00.00 TO STA. 41+00.00	CONTRACT NO. 72D31				
	PLOT DATE = 12/11/2019 - 4:45:15 PM	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT								
		DATE -	REVISED -										

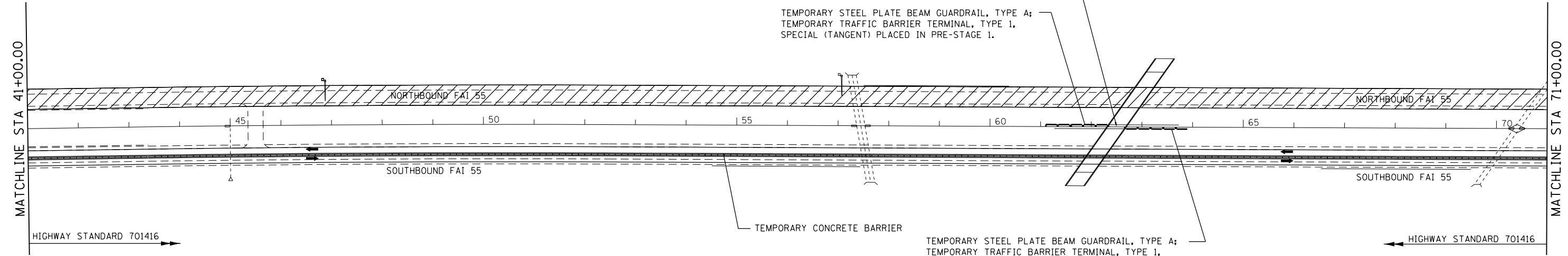


SN 068-0047  
 STA. 62+50 (FAI-55) =  
 STA. 96+82.22 (TR 48) E 2ND RD.

TEMPORARY STEEL PLATE BEAM GUARDRAIL, TYPE A;  
 TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 1,  
 SPECIAL (TANGENT) PLACED IN PRE-STAGE 1.

TEMPORARY STEEL PLATE BEAM GUARDRAIL, TYPE A;  
 TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 1,  
 SPECIAL (TANGENT) PLACED IN PRE-STAGE 1  
 FOR STAGE 1 TRAFFIC.

STAGE I CONSTRUCTION  
 WORK AREA

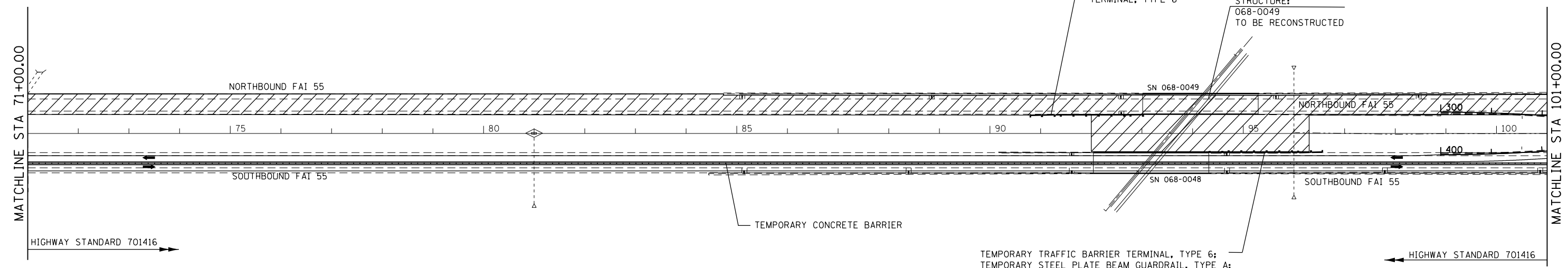
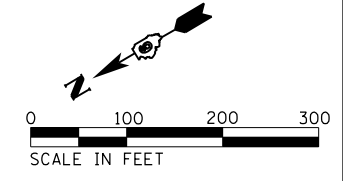


STAGE I CONSTRUCTION  
 WORK AREA

TEMPORARY STEEL PLATE BEAM GUARDRAIL, TYPE A  
 TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 1, SPECIAL (TANGENT)  
 TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 6

STRUCTURE:  
 068-0049  
 TO BE RECONSTRUCTED

TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 6;  
 TEMPORARY STEEL PLATE BEAM GUARDRAIL, TYPE A;  
 TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 1,  
 SPECIAL (TANGENT) PLACED IN PRE-STAGE 1  
 FOR STAGE 1 TRAFFIC.

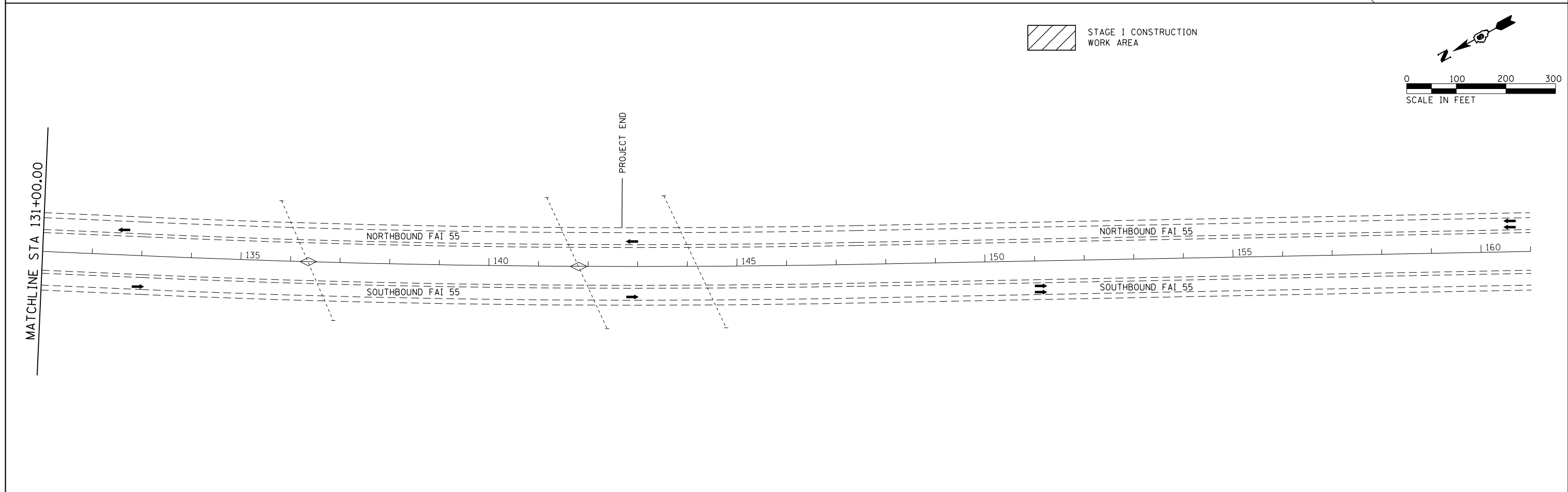
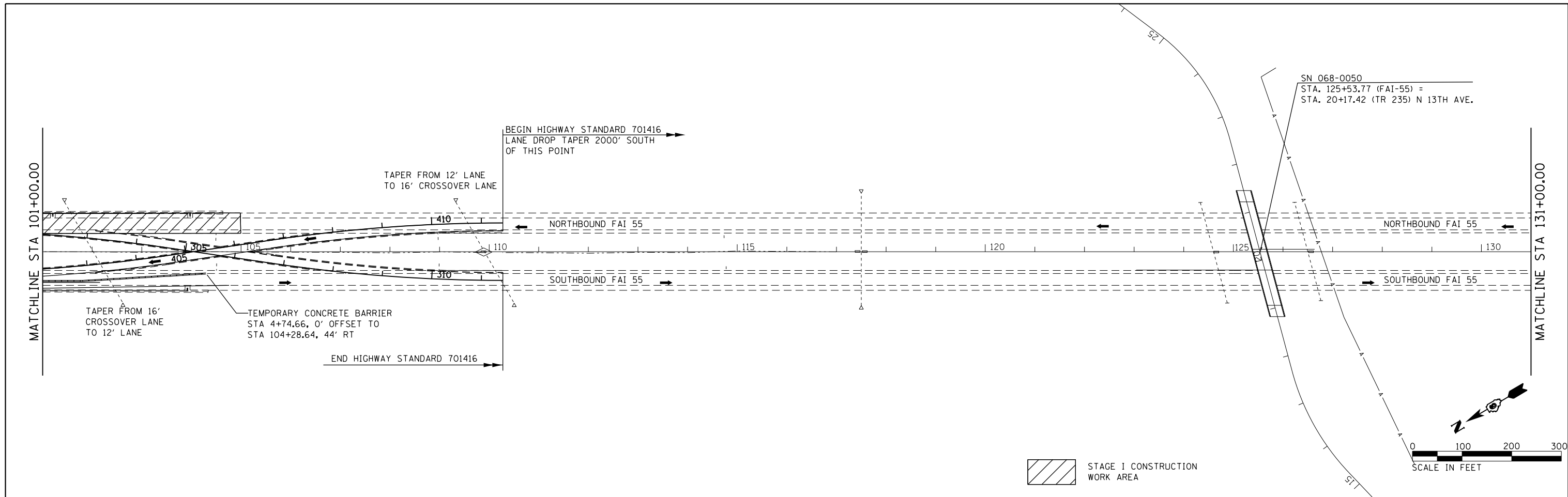


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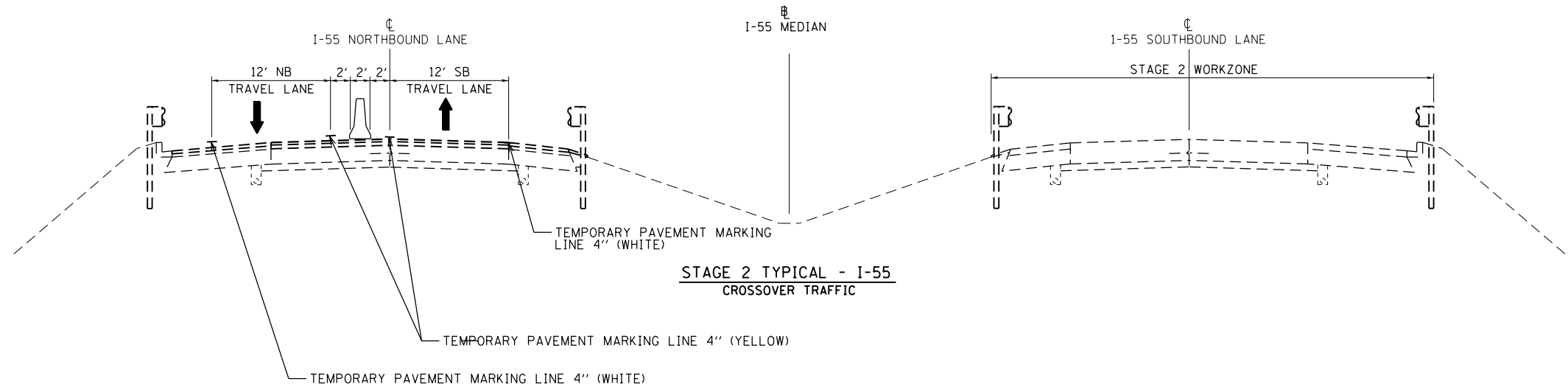
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>MAINTENANCE OF TRAFFIC I-55 STAGE 1</b>			
SCALE: 1" = 100'	SHEET NO.	OF SHEETS	STA. 41+00.00 TO STA. 101+00.00

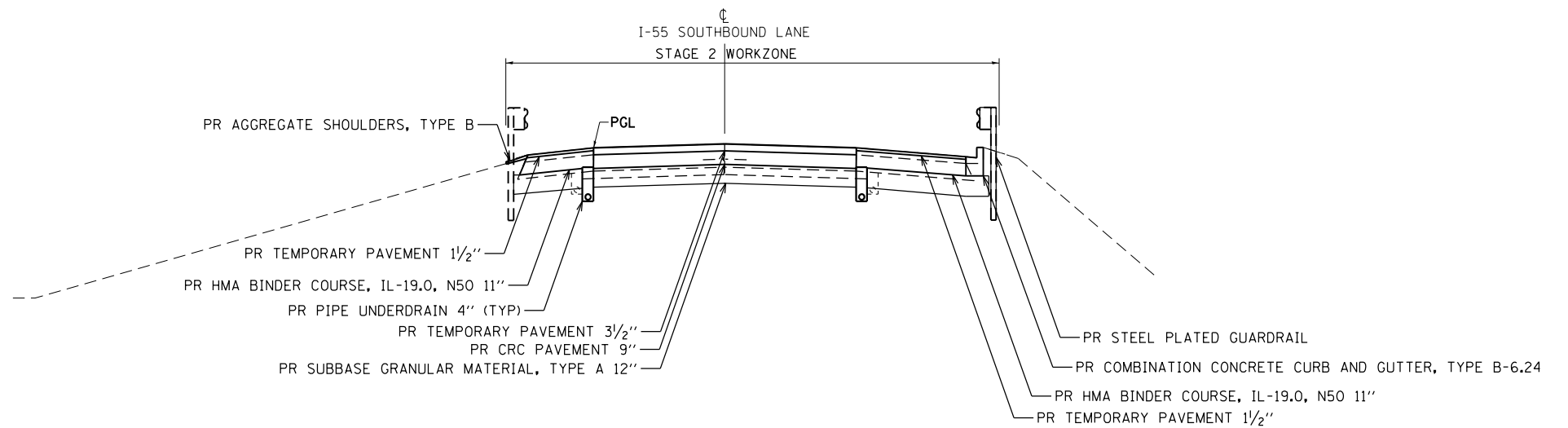
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	



FILE NAME = D672031-sht-MOT-155-Stage1.dgn	USER NAME = korider	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MAINTENANCE OF TRAFFIC I-55 STAGE 1</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = 12/11/2019 - 4:45:15 PM	DATE -	REVISED -	REVISED -		SCALE: 1" = 100'	SHEET NO.	OF SHEETS	STA. 101+00.00 TO STA. 161+00.00	CONTRACT NO. 72031			
								ILLINOIS FED. AID PROJECT				



STAGE 2 TYPICAL - I-55  
CROSSOVER TRAFFIC



STAGE 2 TYPICAL I-55  
FULL DEPTH PAVEMENT  
STA. 10+80.00 TO STA. 15+75.00  
STA. 91+05.00 TO STA. 95+30.00  
BRIDGE (SN 068-0048) OMISSION STA. 92+04.58 TO 94+31.41

**STAGE 2**

**TRAFFIC**

I-55 PRIOR TO CROSSOVER - MAINTAIN ONE LANE OF TRAFFIC IN BOTH NB AND SB DIRECTIONS.

I-55 CROSSOVER - MAINTAIN ONE LANE OF TRAFFIC IN BOTH NB AND SB DIRECTIONS. CROSS SB TRAFFIC OVER ONTO I-55 NB.

WEIGH STATION - MAINTAIN ACCESS (BOTH ENTERING AND EXITING) AT ALL TIMES.

**ROADWAY CONSTRUCTION**

I-55 PRIOR TO CROSSOVER - CONSTRUCT NB INSIDE LANE AND SHOULDER AND SB OUTSIDE LANE AND SHOULDER.

I-55 CROSSOVER SEGMENT - CONSTRUCT SB I-55 PAVEMENT AND SHOULDERS.

SEE PROPOSED TYPICAL SECTION SHEETS FOR DETAILED INFORMATION.

**STAGE GENERAL NOTES**

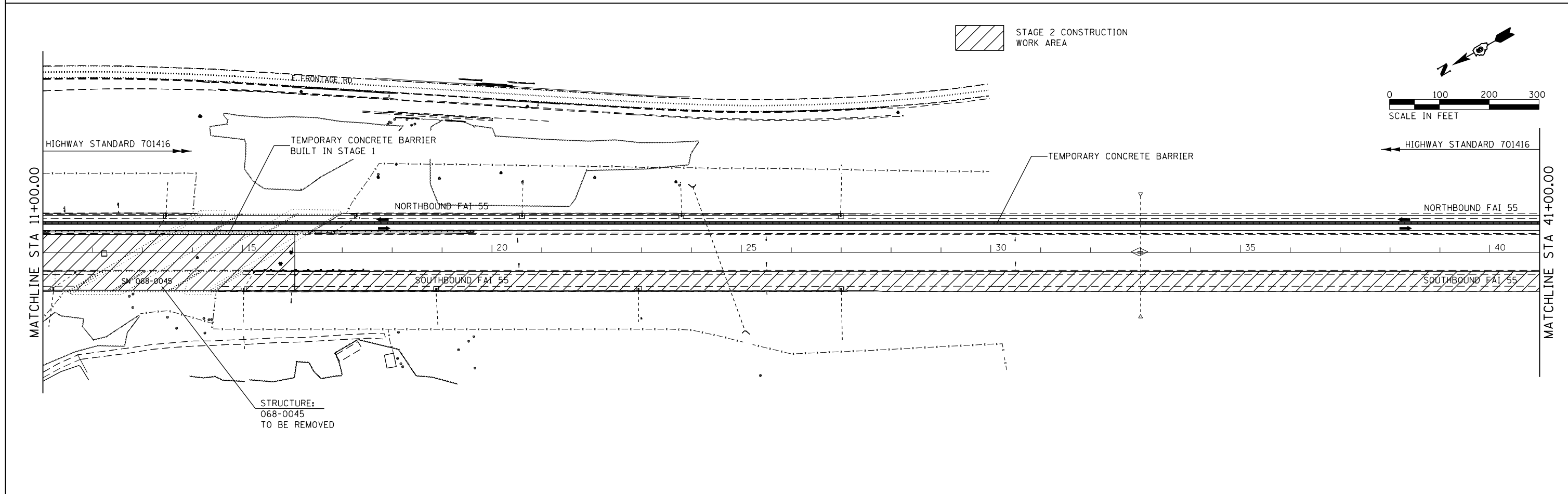
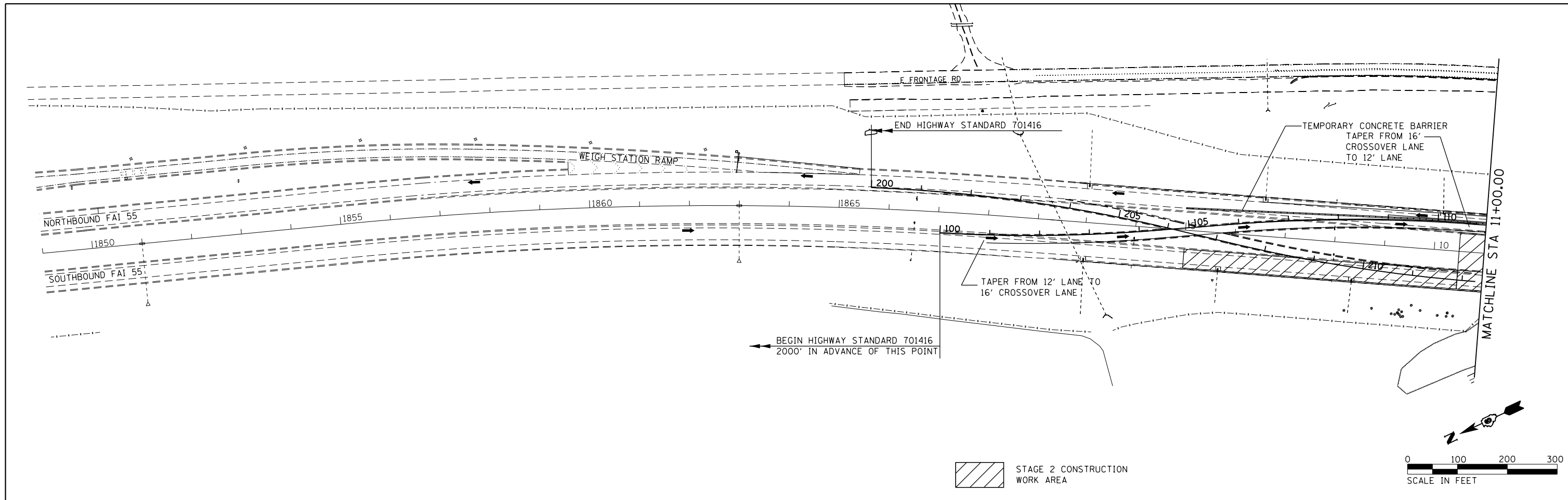
SEE STRUCTURE PLANS FOR BRIDGE STAGING.

SEE CROSS ROAD STAGING PLANS AND DETOURS.

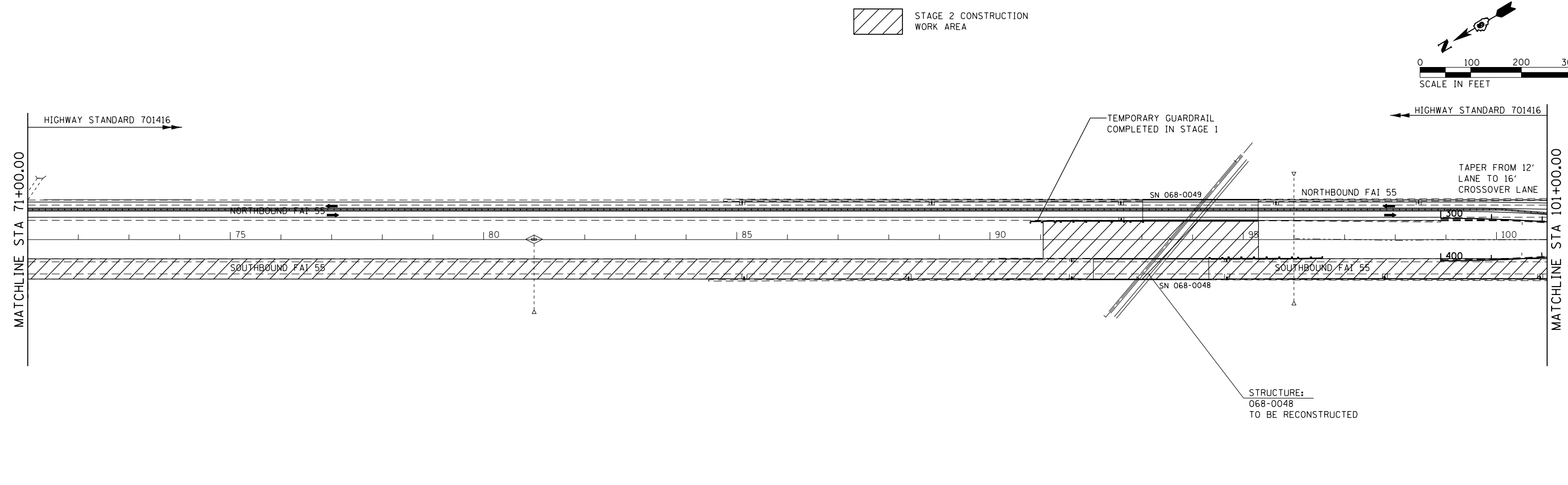
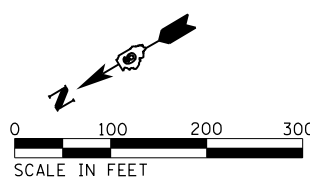
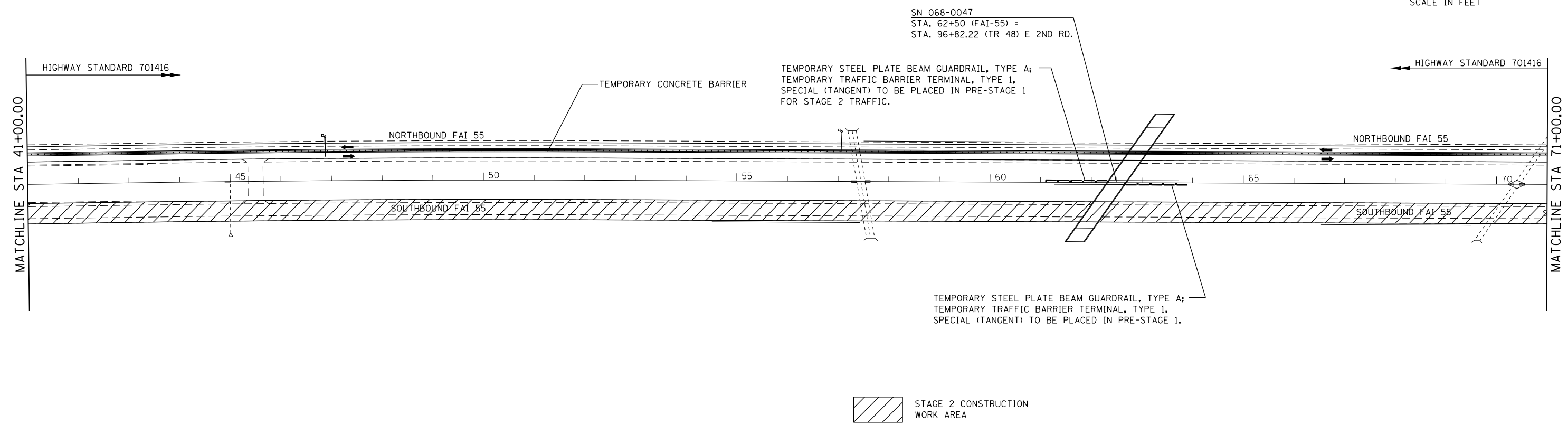
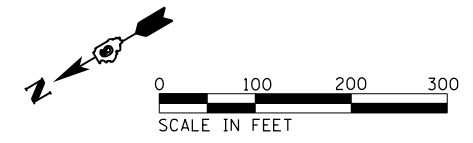
PRIMARY IDOT STANDARDS THIS STAGE  
(SEE SPECIAL PROVISIONS FOR ADDITIONAL APPLICABLE STANDARDS)

701101, 701106, 701101, 701106, 701400, 701401, 701406, 701411, 701416, 701426, 701428, 701451, 701456, 701901

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PLOT DATE = 4/7/2020 - 4:54:53 PM	DATE -	REVISED -	REVISED -	SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 72D31			
											ILLINOIS FED. AID PROJECT	



FILE NAME = D672031-sh1-MOT-155-Stage2.dgn	USER NAME = korider	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MAINTENANCE OF TRAFFIC I-55 STAGE 2</b>			F.A.I. RTE. 55	SECTION (68-1,3 RS-3,68-2 RS-5)BR	COUNTY MONTGOMERY	TOTAL SHEETS 307	SHEET NO. 114
	PLOT SCALE = 200.0000' / in.	DRAWN -	REVISED -		SCALE: 1" = 100'	SHEET NO.	OF	SHEETS	STA. 1849+00.00 TO STA. 41+00.00	CONTRACT NO. 72D31		
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		DATE -	REVISED -									



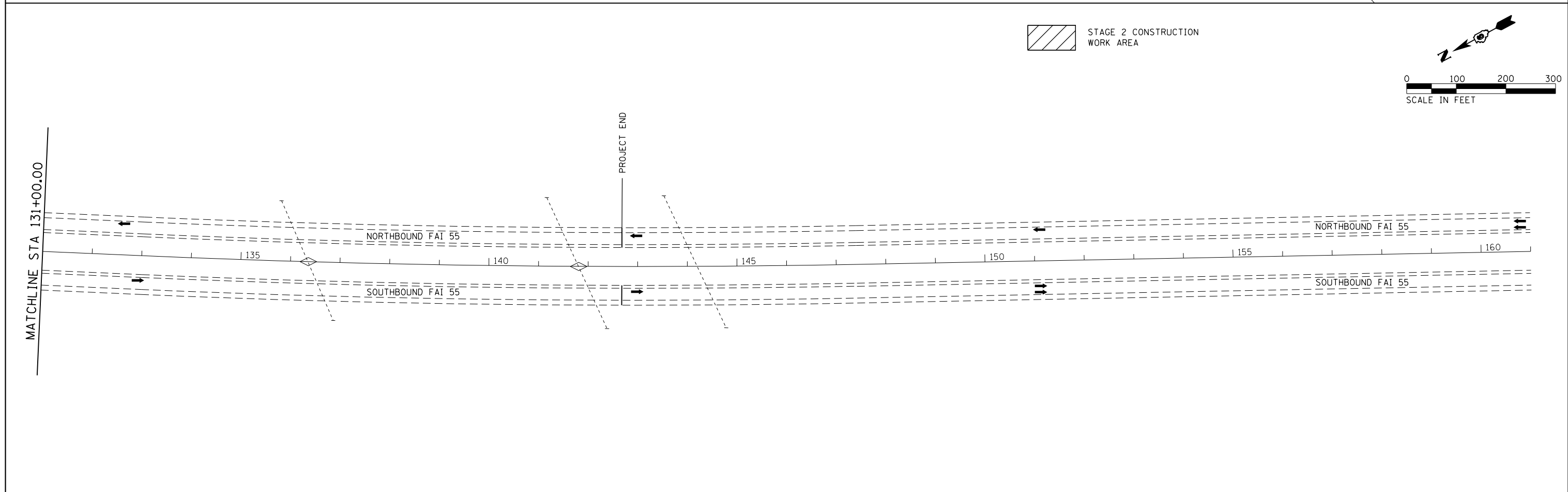
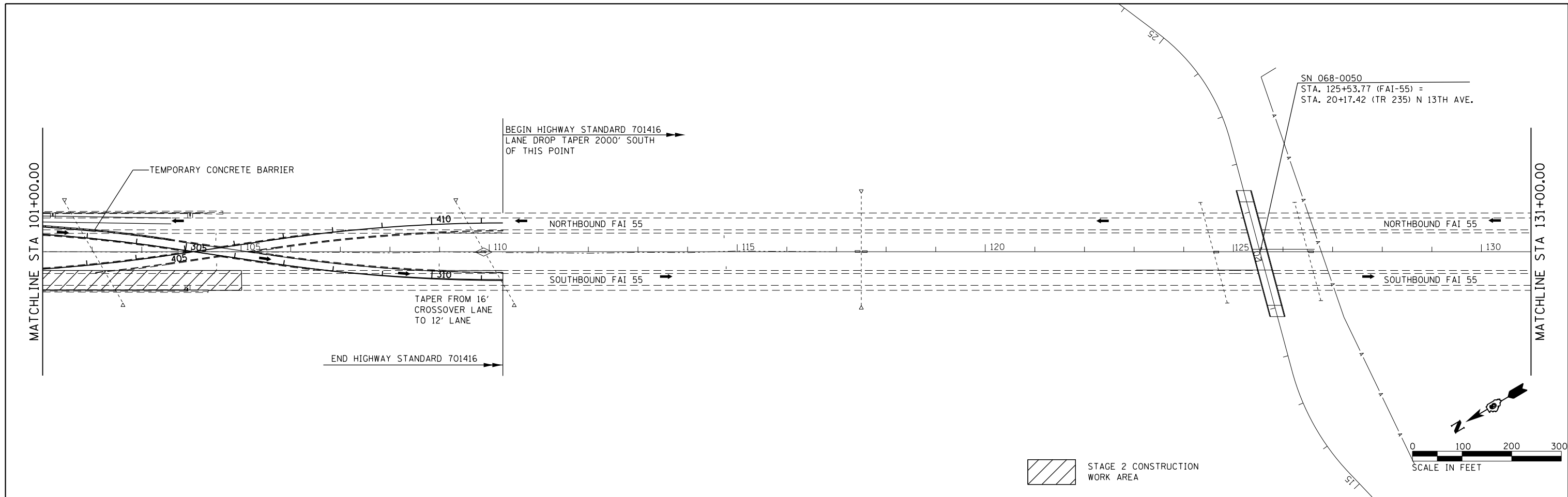
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	PLOT DATE = 12/11/2019 - 4:45:34 PM	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC  
 I-55 STAGE 2**

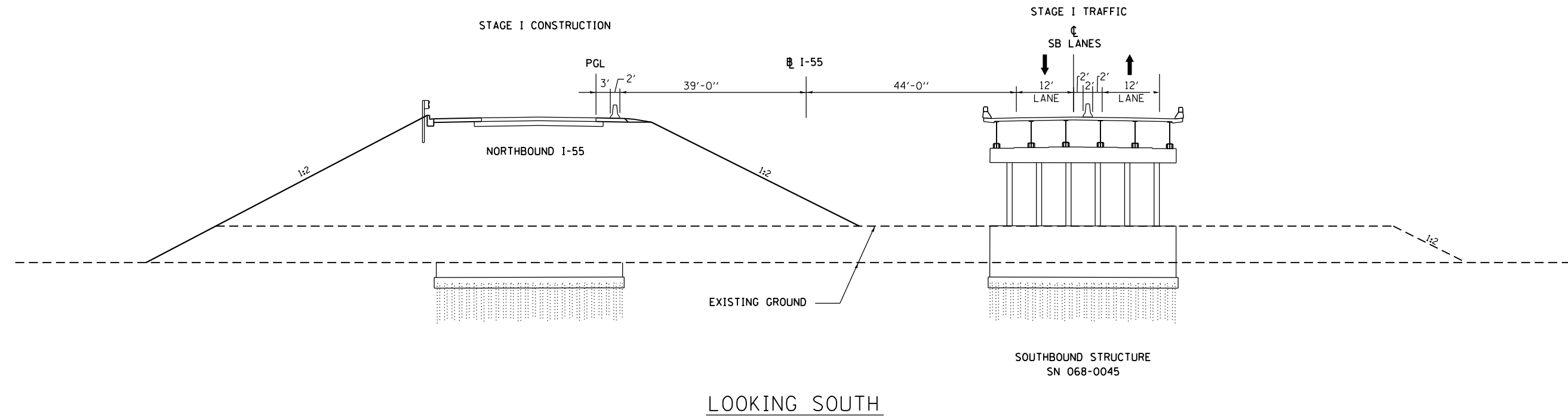
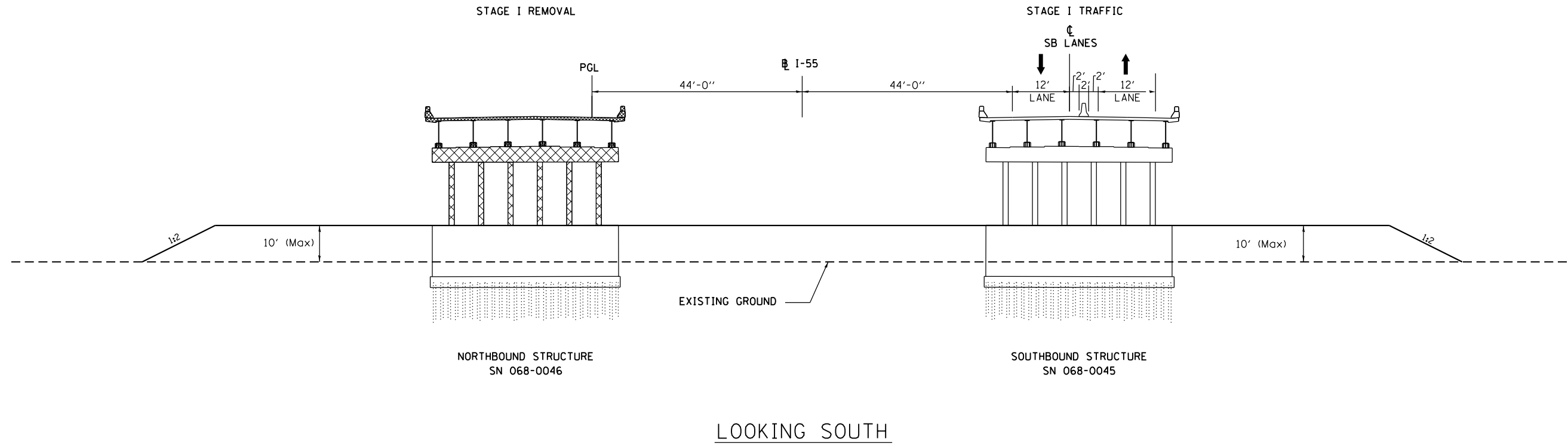
SCALE: 1" = 100'    SHEET NO.    OF    SHEETS    STA. 41+00.00    TO    STA. 101+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	115
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72D31	



FILE NAME = D672031-sht-MOT-155-Stage2.dgn	USER NAME = kcorider	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MAINTENANCE OF TRAFFIC I-55 STAGE 2</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -					55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	116
PLOT DATE = 12/11/2019 - 4:45:34 PM	DATE -	REVISED -	REVISED -		SCALE: 1" = 100'	SHEET NO.	OF SHEETS	STA. 101+00.00 TO STA. 161+00.00	CONTRACT NO. 72031			
								ILLINOIS FED. AID PROJECT				





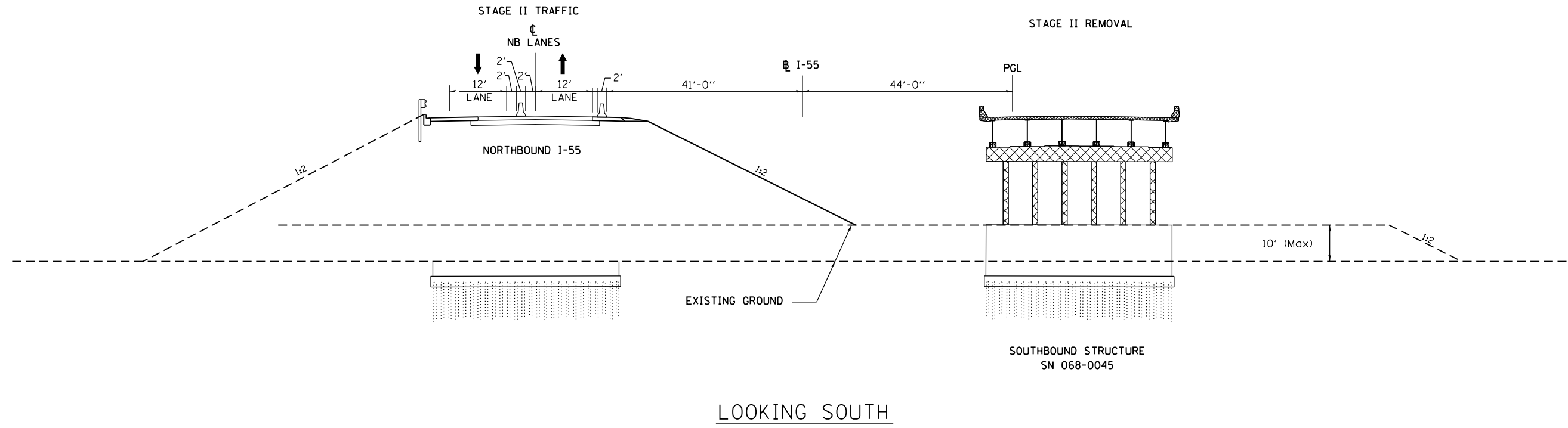
FILE NAME =	USER NAME = korider	DESIGNED -	REVISED -
D672031-sht-MOT-Geotechnical Staging.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 24.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 4/10/2020	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

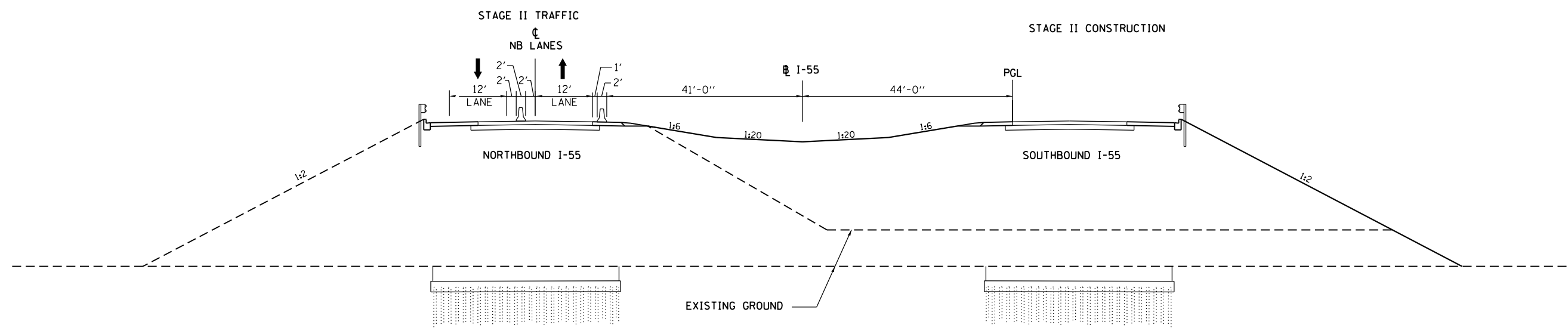
**STAGING STRUCTURE REMOVAL - GRADING PLAN**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	117
CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	



LOOKING SOUTH



LOOKING SOUTH

FILE NAME = D672031-sht-MOT-Geotechnical Staging.dgn	USER NAME = kcorider	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STAGING STRUCTURE REMOVAL - GRADING PLAN</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	PLOT SCALE = 24.0000' / in.	DRAWN -	REVISED -					55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	118
	PLOT DATE = 4/10/2020	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 72D31				
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

PRE-STAGE A

TRAFFIC

IL 108 RAMPS - KEEP RAMP TRAFFIC OPEN DURING PATCHING OPERATIONS.  
SHALL FOLLOW HIGHWAY STANDARD 701456.

ROADWAY CONSTRUCTION

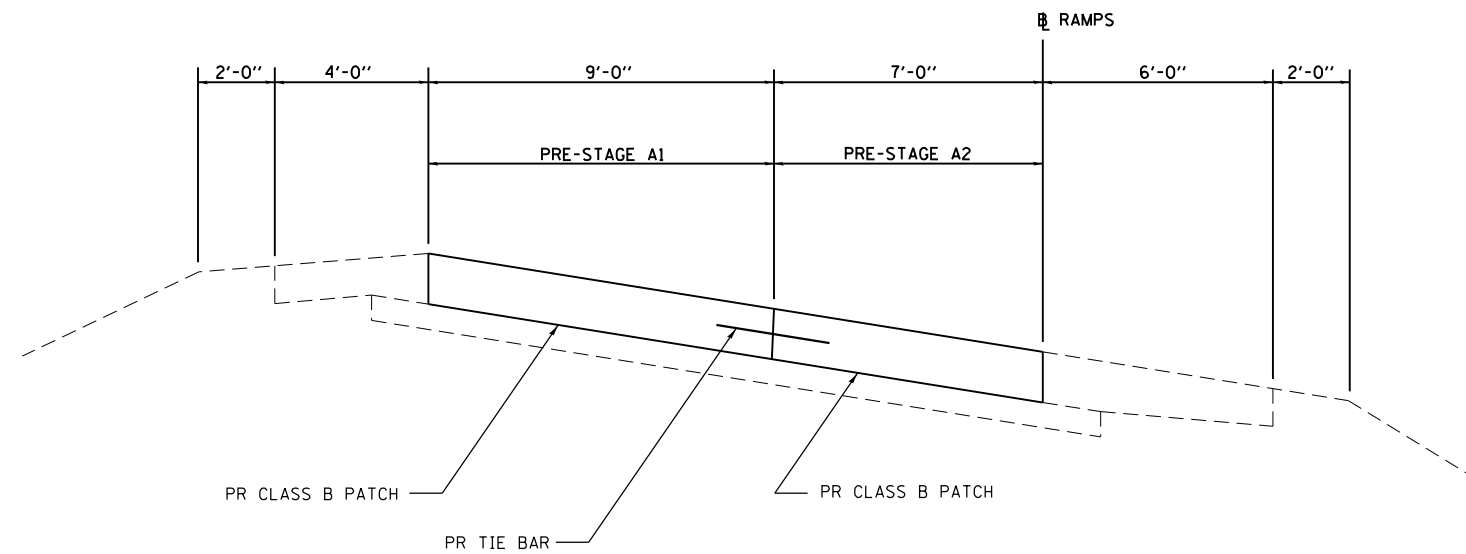
IL 108 RAMPS - CONSTRUCT CLASS B PATCHES PER PAVEMENT PATCHING SCHEDULE.

STAGE GENERAL NOTES

SEE CLASS B PATCHING RAMP DETAIL SHEET.

PRIMARY IDOT STANDARDS THIS STAGE

(SEE SPECIAL PROVISIONS FOR ADDITIONAL APPLICABLE STANDARDS)  
701456, 701901



PRE-STAGE A TYPICAL #1 - IL 108 RAMPS PATCHING

FILE NAME = D672031-sht-MOT-PreStage-SN0680043.dgn	USER NAME = kcorider	DESIGNED -	REVISED -
		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -
Default	PLOT DATE = 12/11/2019 - 4:46:09 PM		

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

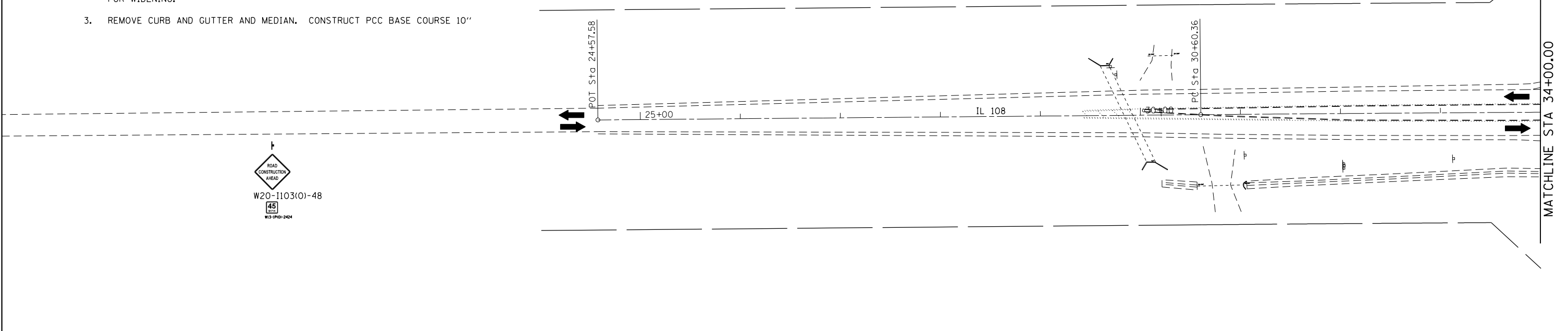
**MAINTENANCE OF TRAFFIC PLAN - PRESTAGE A  
S.N. 068-0043 IL 108**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	119
CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	

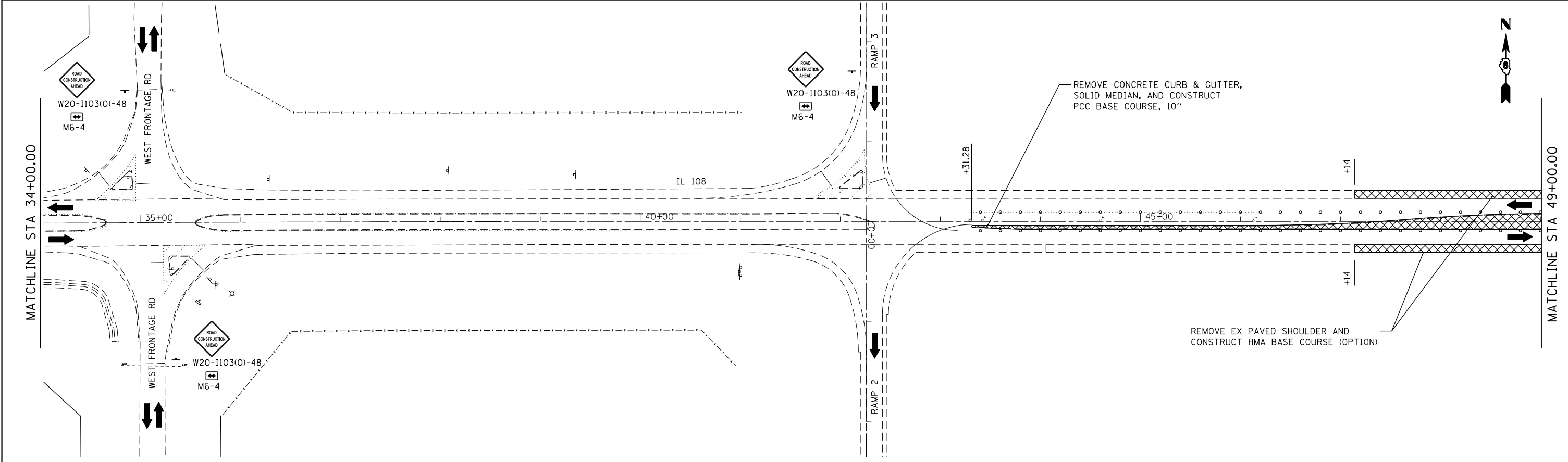
**STAGE 1 NOTES**

1. ERECT TRAFFIC CONTROL AND PROTECTION (INCLUDING SIGNS) ACCORDING TO STANDARD 701326 & 701701 AND AS SHOWN ON THESE PLANS.
2. REMOVED PAVED SHOULDER AND CONSTRUCT HMA BASE COURSE 10" FOR WIDENING.
3. REMOVE CURB AND GUTTER AND MEDIAN. CONSTRUCT PCC BASE COURSE 10"



**LEGEND**

	WORK AREA		REMOVAL ITEMS		DIRECTION OF TRAFFIC		DRUM WITH MONO DIRECTIONAL LIGHT		TRAFFIC SIGN		TYPE III BARRICADE WITH FLASHING LIGHTS		0 50 100 150 SCALE IN FEET
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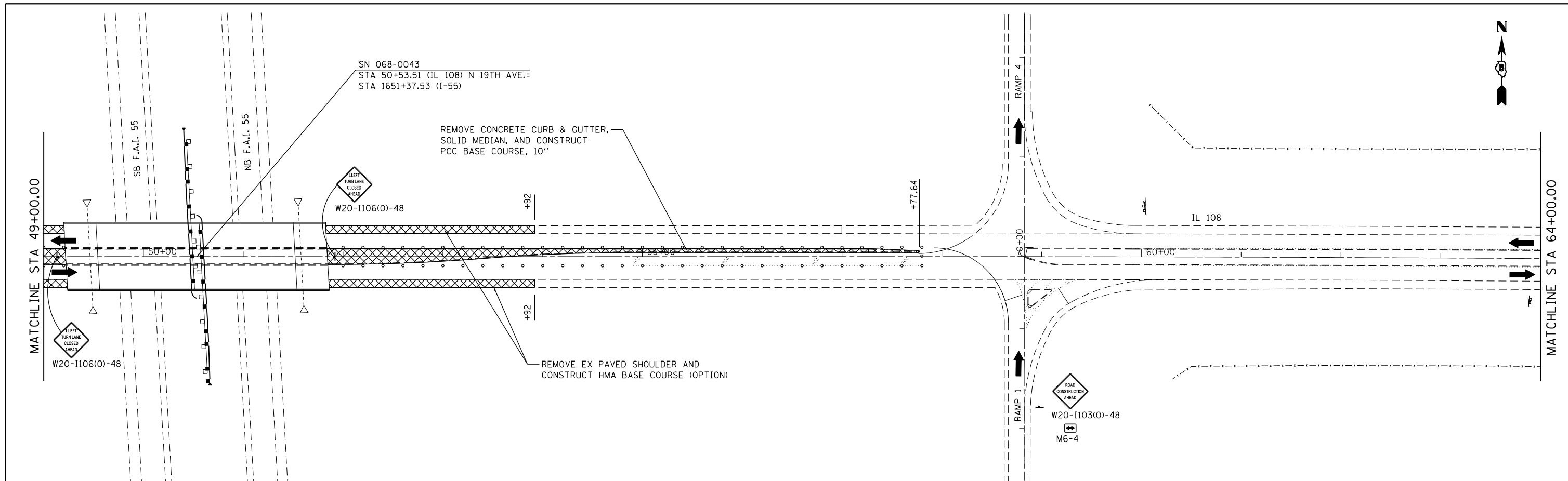
FILE NAME = D672031-sht-MOT-stage1-SN0680043.dgn	USER NAME = kcorider	DESIGNED -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
	PLOT DATE = 12/11/2019 - 4:46:33 PM	CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC PLAN - STAGE 1  
S.N. 068-0043 IL 108**

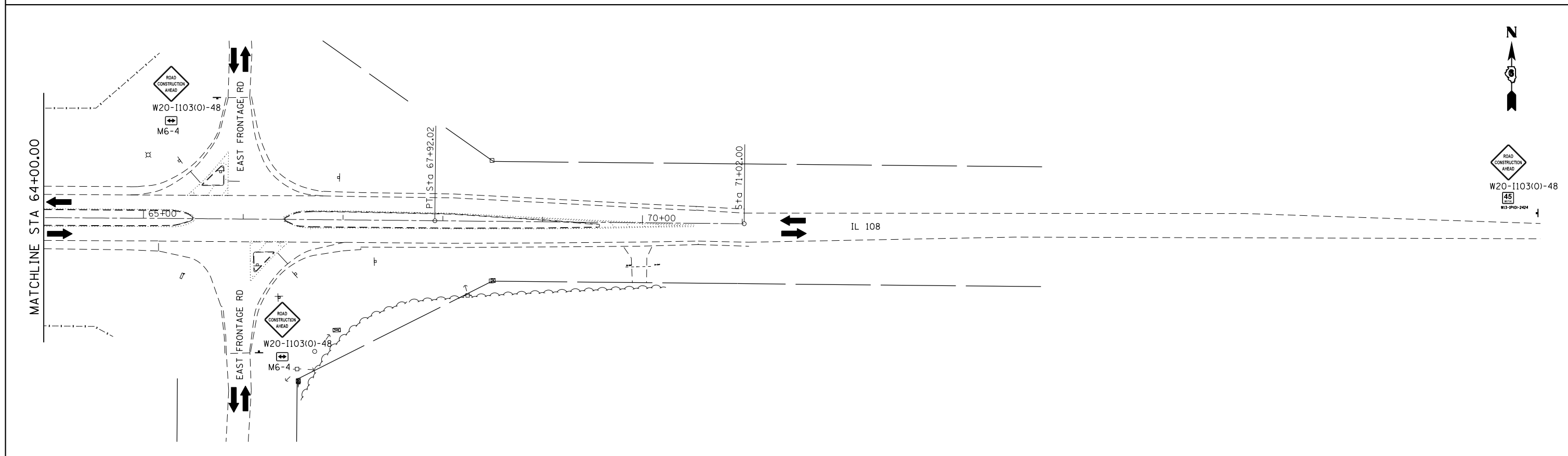
SCALE: 1" = 50' SHEET OF SHEETS STA. 24+57.58 TO STA. 49+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	120
CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	



**LEGEND**

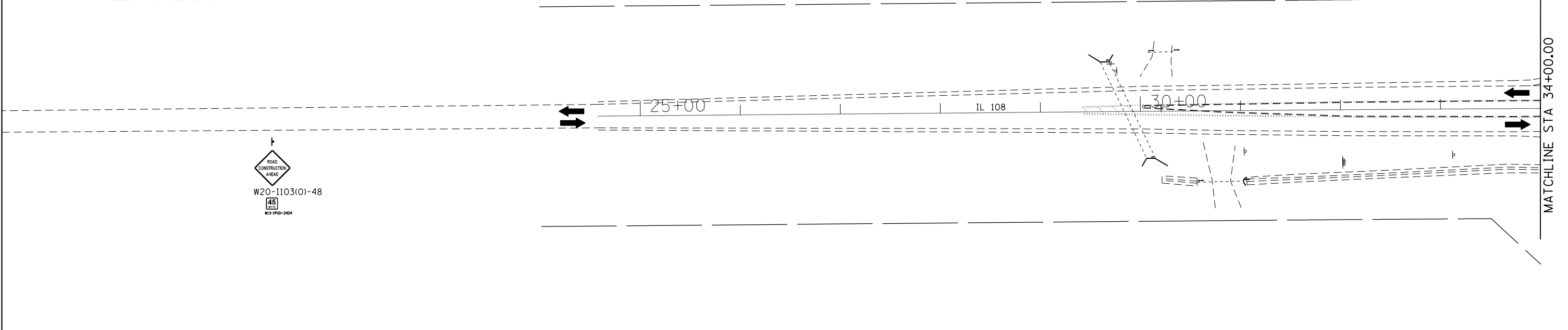
	WORK AREA		REMOVAL ITEMS		DIRECTION OF TRAFFIC		DRUM WITH MONO DIRECTIONAL LIGHT		TRAFFIC SIGN		TYPE III BARRICADE WITH FLASHING LIGHTS		0 50 100 150 SCALE IN FEET
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FILE NAME = D672031-sht-MOT-stage1-SN0680043.dgn	USER NAME = kcorider	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MAINTENANCE OF TRAFFIC PLAN - STAGE 1 S.N. 068-0043 IL 108</b>				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
Default	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -		SCALE: 1" = 50'	SHEET	OF	SHEETS	STA. 49+00.00	TO STA. 71+00.00	55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	121
	PLOT DATE = 12/11/2019 - 4:46:34 PM	CHECKED -	REVISED -								CONTRACT NO. 72D31				
		DATE -	REVISED -						ILLINOIS FED. AID PROJECT						

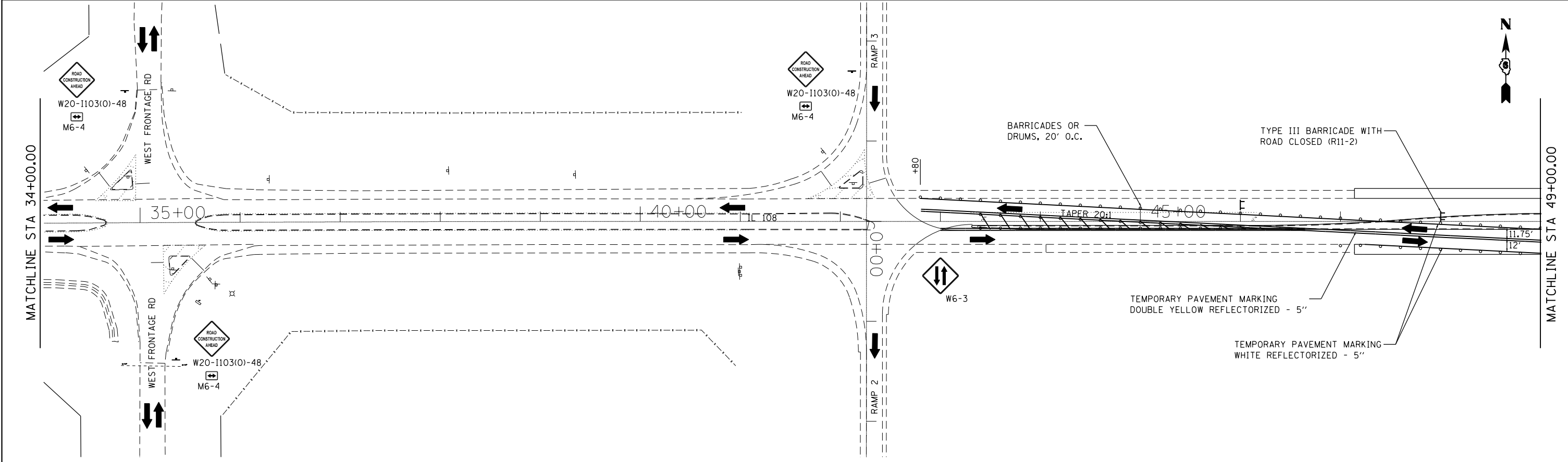
STAGE 2 NOTES

1. ERECT SIGNS, TEMPORARY BARRIERS, AND TEMPORARY PAVEMENT MARKINGS ACCORDING TO THESE PLANS AND TC&P STANDARD 701331.
2. REMOVE AND CONSTRUCT THE STAGE I PORTION OF THE EXISTING STRUCTURE. (SEE STRUCTURE PLANS)



**LEGEND**

 WORK AREA	 REMOVAL ITEMS	 DIRECTION OF TRAFFIC	 DRUM WITH MONO DIRECTIONAL LIGHT	 TRAFFIC SIGN	 TYPE III BARRICADE WITH FLASHING LIGHTS	 0 50 100 150 SCALE IN FEET
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FILE NAME = D672031-sht-MOT-stage2-SN0680043.dgn	USER NAME = kcorider	DESIGNED -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
	PLOT DATE = 12/11/2019 - 4:46:51 PM	CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC PLAN - STAGE 2  
S.N. 068-0043 IL 108**

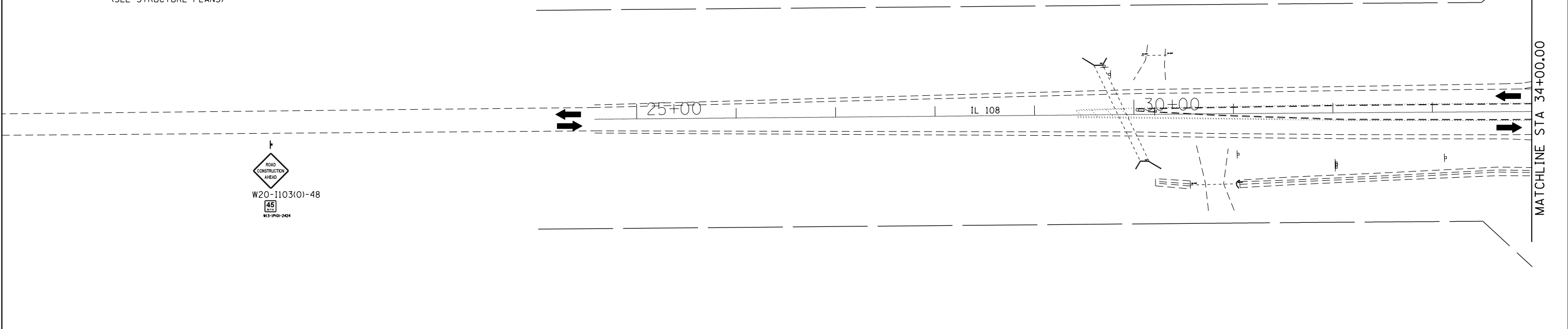
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	122
CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	

SCALE: 1" = 50' SHEET OF SHEETS STA. 24+57.58 TO STA. 49+00.00



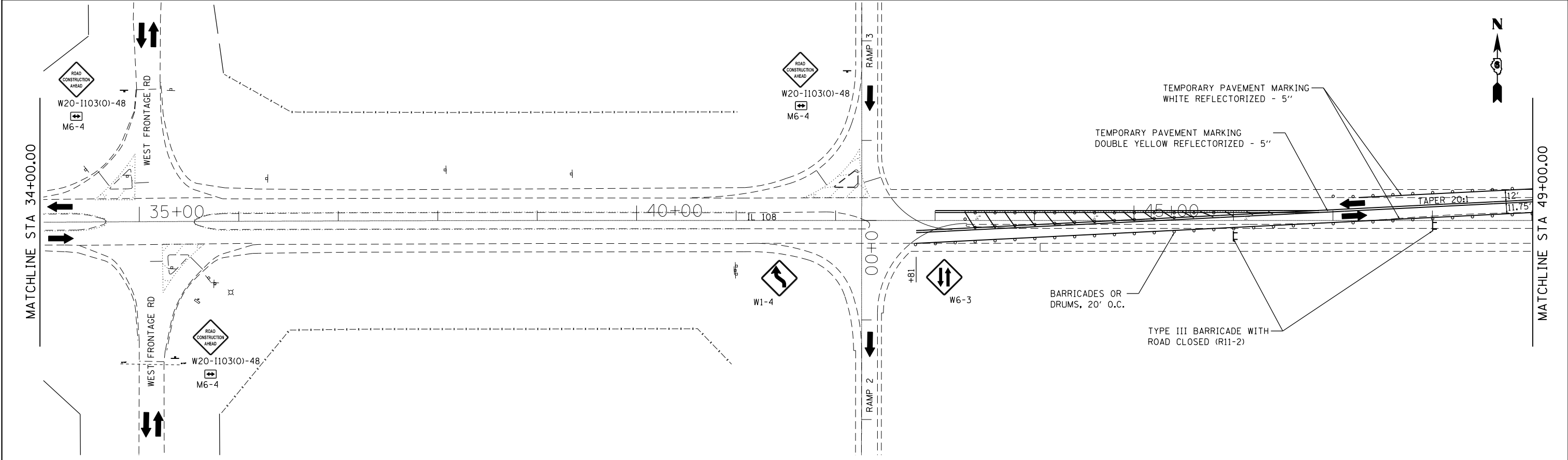
**STAGE 3 NOTES**

1. ERECT TRAFFIC CONTROL AND PROTECTION ACCORDING TO STANDARD 701331 AND AS SHOWN ON THESE PLANS.
2. REMOVE AND CONSTRUCT THE STAGE II PORTION OF THE EXISTING STRUCTURE. (SEE STRUCTURE PLANS)



**LEGEND**

 WORK AREA	 REMOVAL ITEMS	 DIRECTION OF TRAFFIC	 DRUM WITH MONO DIRECTIONAL LIGHT	 TRAFFIC SIGN	 TYPE III BARRICADE WITH FLASHING LIGHTS	 0 50 100 150 SCALE IN FEET
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FILE NAME = D672031-sht-MOT-stage3-SN0680043.dgn	USER NAME = korider	DESIGNED -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
	PLOT DATE = 12/11/2019 - 4:47:10 PM	CHECKED -	REVISED -
		DATE -	REVISED -

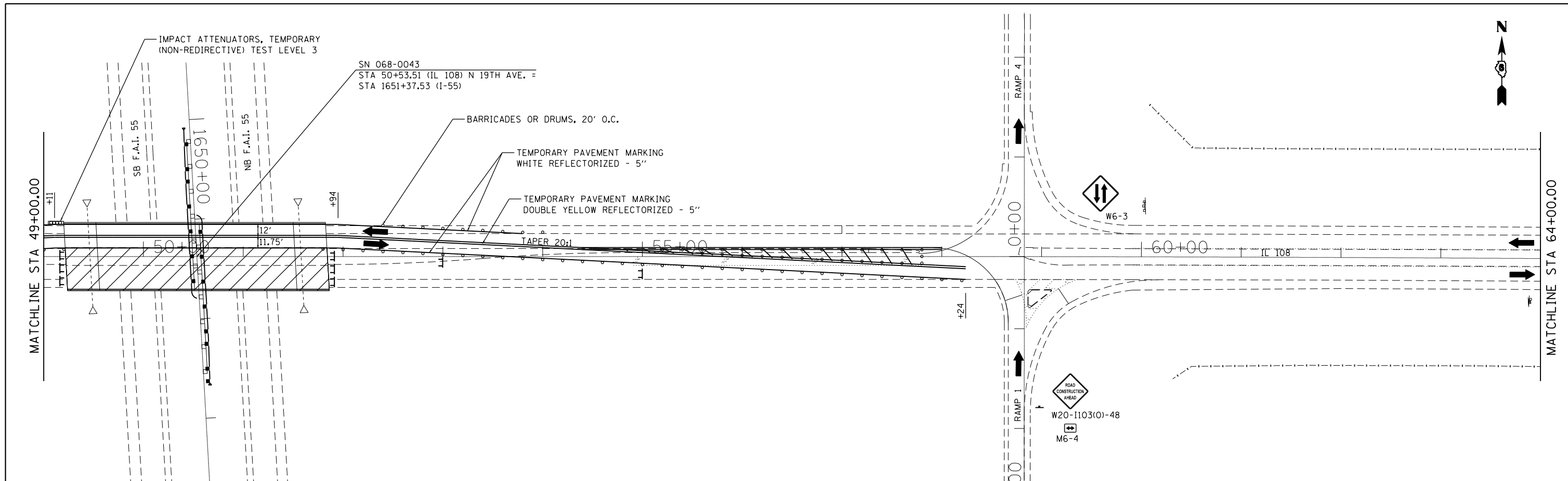
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC PLAN - STAGE 3  
S.N. 068-0043 IL 108**



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	124
CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	

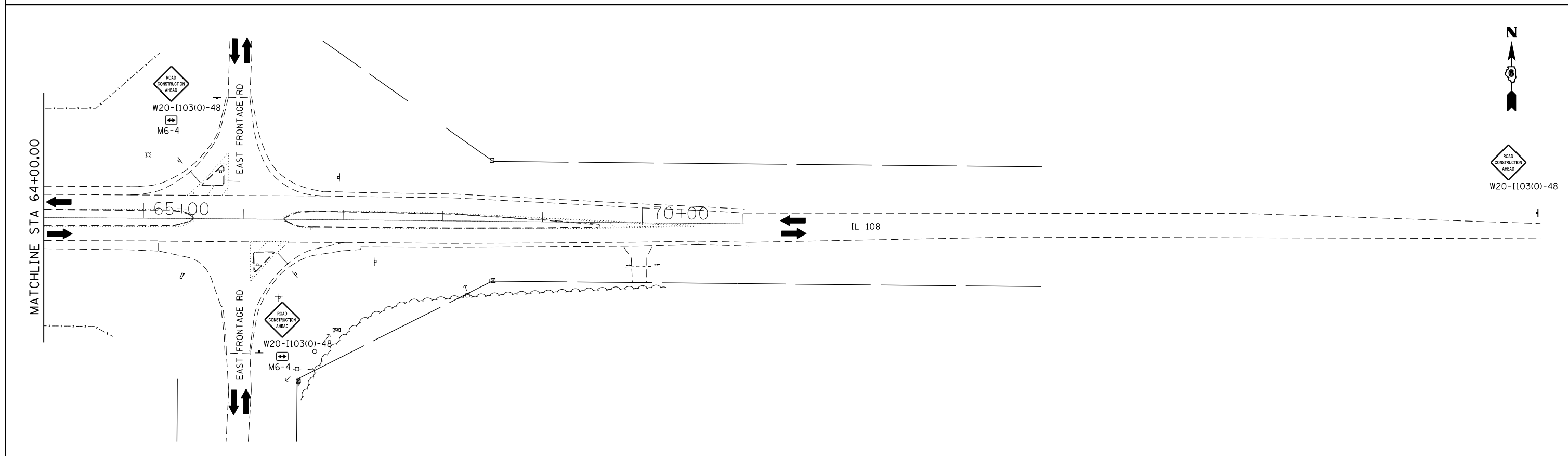
SCALE: 1" = 50' SHEET OF SHEETS STA. 24+57.58 TO STA. 49+00.00





**LEGEND**

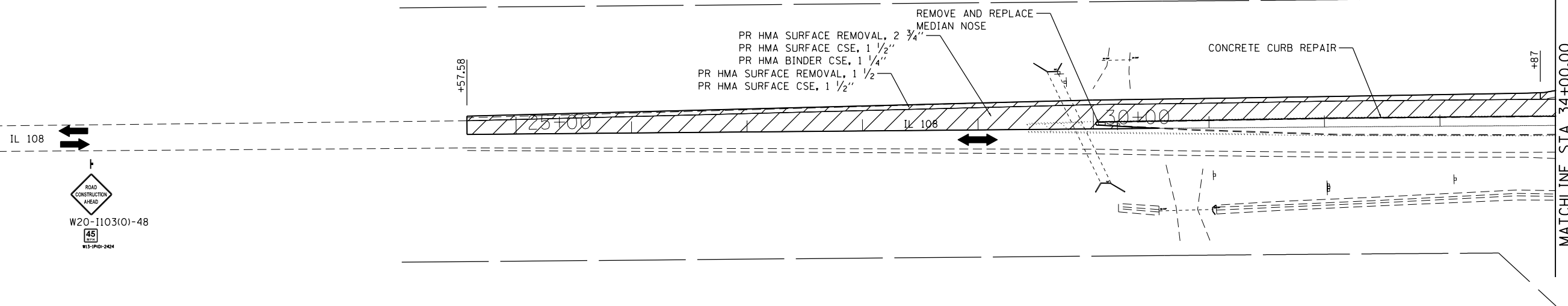
	WORK AREA		REMOVAL ITEMS		DIRECTION OF TRAFFIC		DRUM WITH MONO DIRECTIONAL LIGHT		TRAFFIC SIGN		TYPE III BARRICADE WITH FLASHING LIGHTS		0 50 100 150 SCALE IN FEET
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FILE NAME = D672031-sht-MOT-stage3-SN0680043.dgn	USER NAME = kcorider	DESIGNED - DRAWN -	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MAINTENANCE OF TRAFFIC PLAN - STAGE 3 S.N. 068-0043 IL 108</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE: 1" = 50'	SHEET	OF	SHEETS	STA. 49+00.00	TO STA. 71+00.00	55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	125
	PLOT DATE = 12/11/2019 - 4:47:10 PM	DATE -	REVISED -								CONTRACT NO. 72D31 ILLINOIS FED. AID PROJECT				

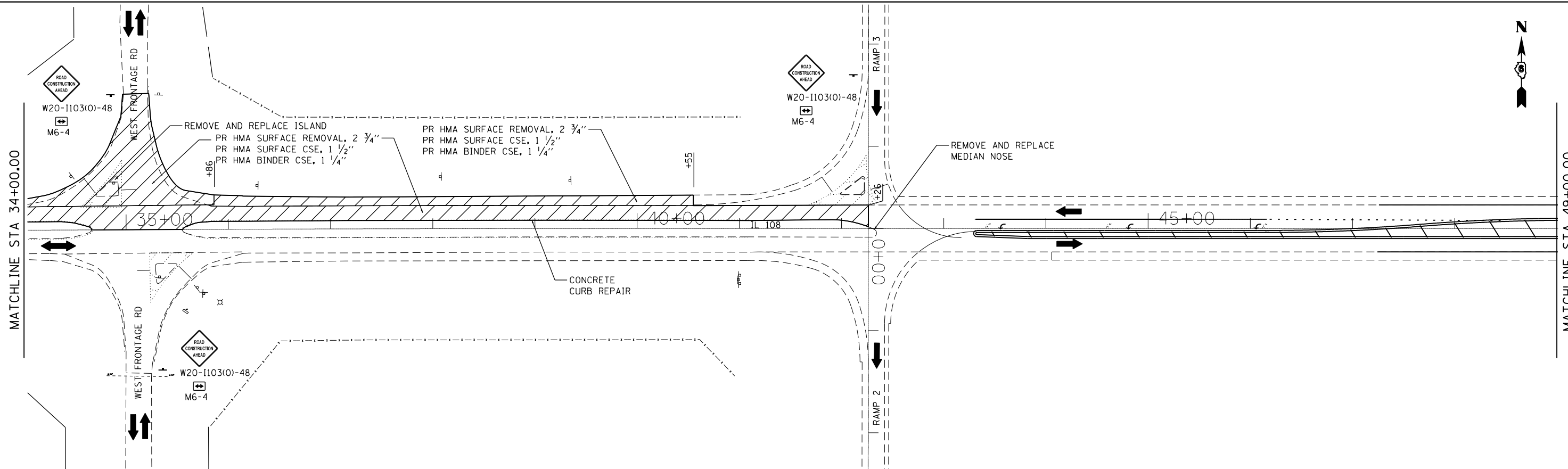
STAGE 4A NOTES

- ERECT TRAFFIC CONTROL AND PROTECTION ACCORDING TO STANDARD 701701 AND AS SHOWN IN THESE PLANS.
- CONSTRUCT IL 108 AS SHOWN IN THESE PLANS.



LEGEND

- WORK AREA
  - REMOVAL ITEMS
  - DIRECTION OF TRAFFIC
  - DRUM WITH MONO DIRECTIONAL LIGHT
  - TRAFFIC SIGN
  - TYPE III BARRICADE WITH FLASHING LIGHTS
- 0 50 100 150  
SCALE IN FEET



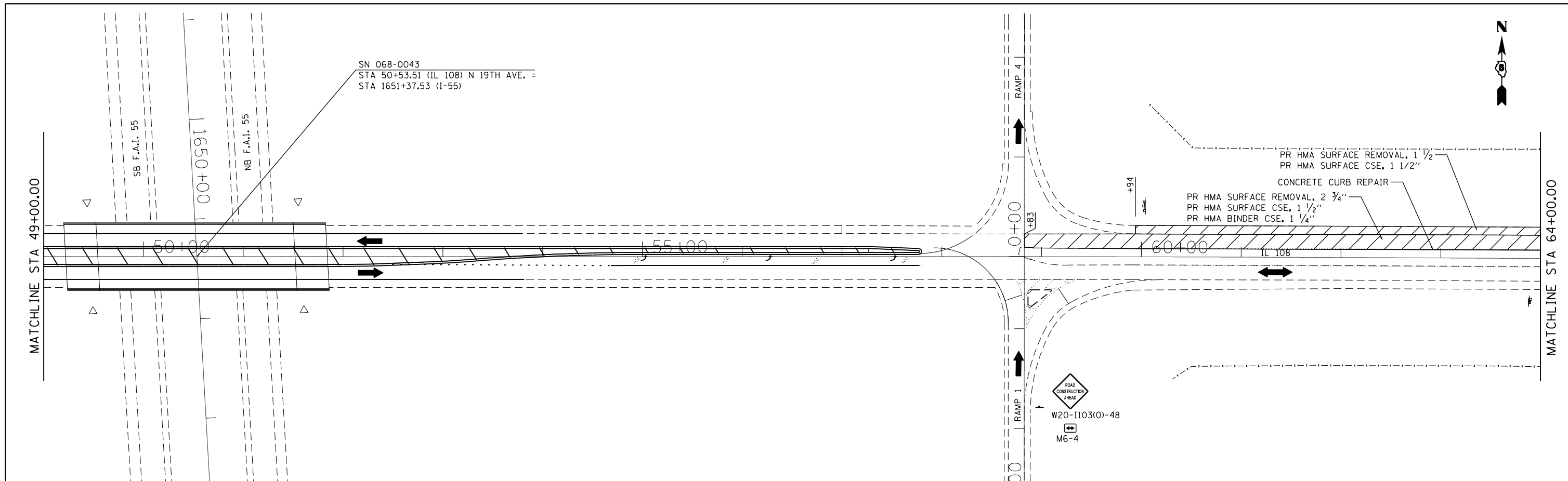
FILE NAME = D672031-sht-MOT-stage4A-SN0680043.dgn	USER NAME = kcorider	DESIGNED -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
	PLOT DATE = 12/11/2019 - 4:47:28 PM	CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

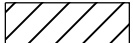

**MAINTENANCE OF TRAFFIC PLAN - STAGE 4A  
S.N. 068-0043 IL 108**

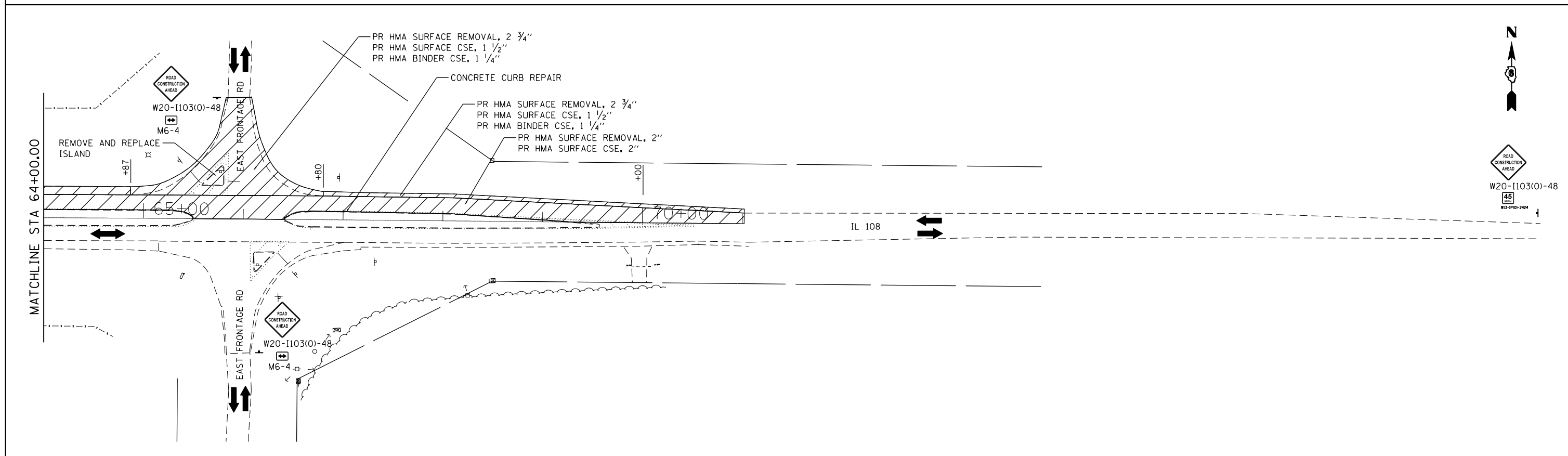
SCALE: 1" = 50'    SHEET    OF    SHEETS    STA. 24+57.58    TO    STA. 49+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	126
CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	



**LEGEND**

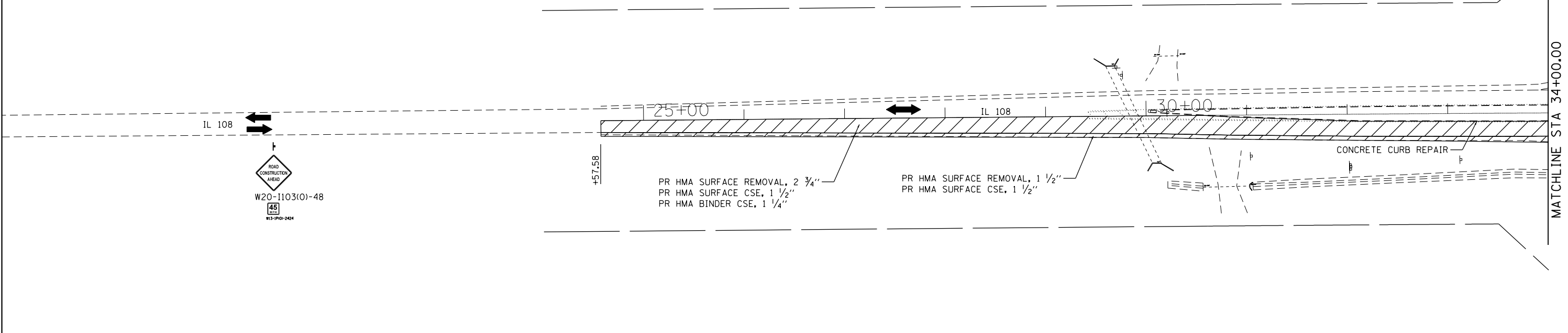
	WORK AREA		REMOVAL ITEMS		DIRECTION OF TRAFFIC		DRUM WITH MONO DIRECTIONAL LIGHT		TRAFFIC SIGN		TYPE III BARRICADE WITH FLASHING LIGHTS		0 50 100 150 SCALE IN FEET
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FILE NAME = D672031-sht-MOT-stage4A-SN0680043.dgn	USER NAME = kcorider	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MAINTENANCE OF TRAFFIC PLAN - STAGE 4A IL 108</b>			F.A.I. RTE. = 55	SECTION = (68-1,3 RS-3,68-2 RS-5)BR	COUNTY = MONTGOMERY	TOTAL SHEETS = 307	SHEET NO. = 127
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE: 1" = 50'	SHEET OF SHEETS	STA. 49+00.00 TO STA. 71+00.00	CONTRACT NO. 72D31				
	PLOT DATE = 12/11/2019 - 4:47:28 PM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

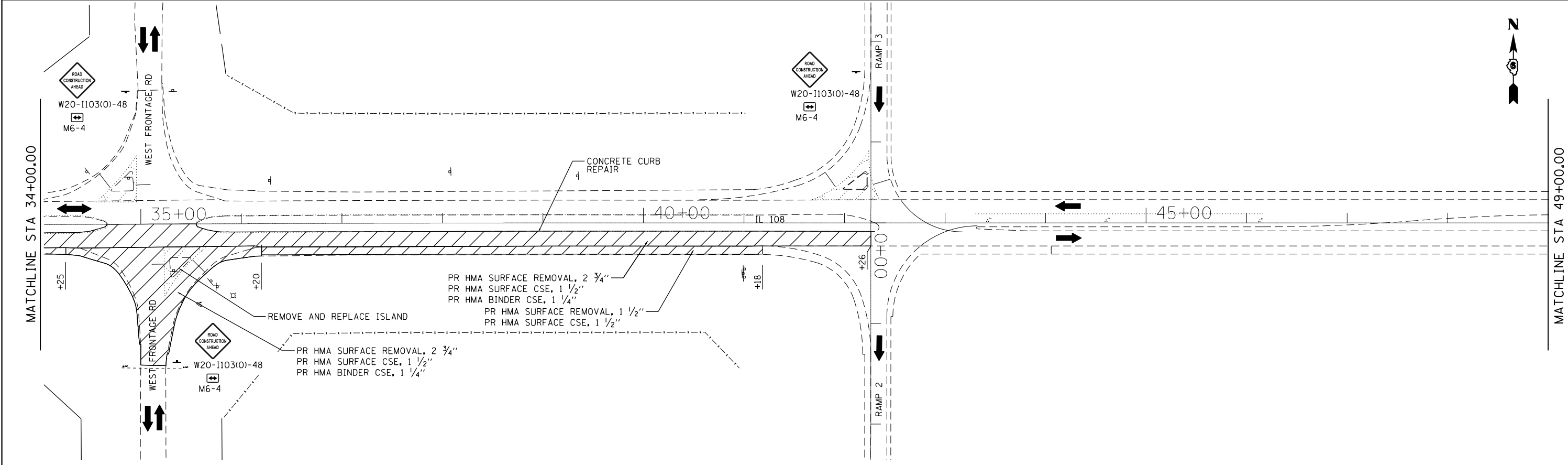
STAGE 4B NOTES

1. ERECT TRAFFIC CONTROL AND PROTECTION ACCORDING TO STANDARD 701701 AND AS SHOWN IN THESE PLANS.
2. CONSTRUCT IL 108 AS SHOWN IN THESE PLANS.

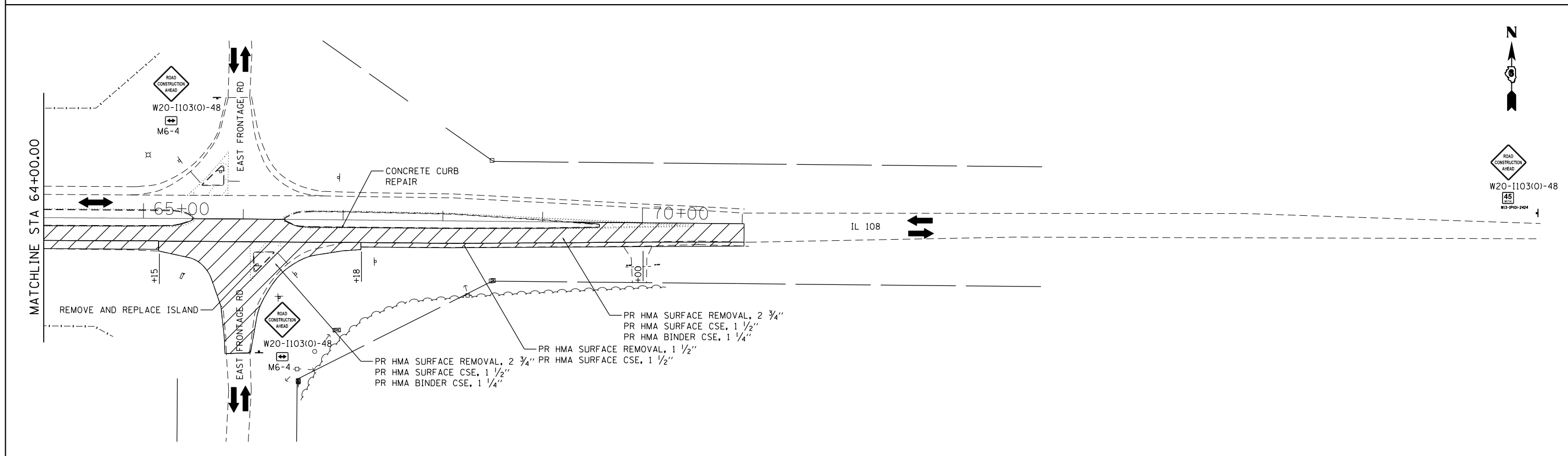
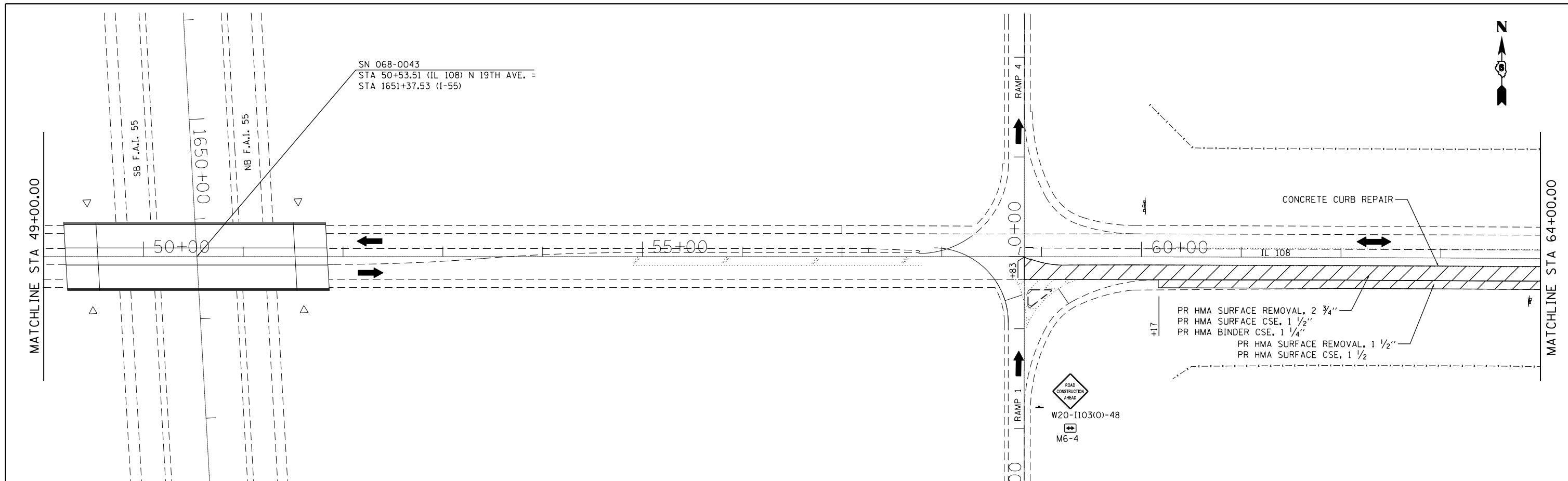


**LEGEND**

	WORK AREA		REMOVAL ITEMS		DIRECTION OF TRAFFIC		DRUM WITH MONO DIRECTIONAL LIGHT		TRAFFIC SIGN		TYPE III BARRICADE WITH FLASHING LIGHTS		0 50 100 150 SCALE IN FEET
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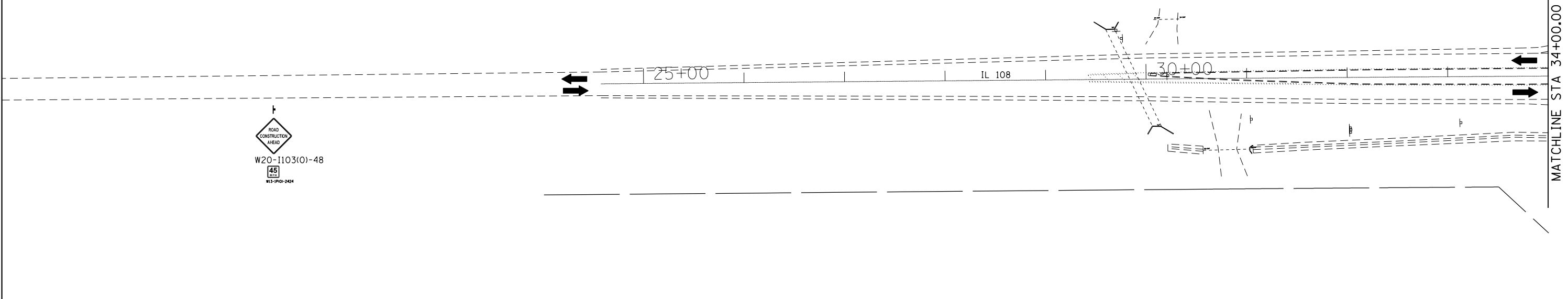
FILE NAME = D672031-sht-MOT-stage4B-SN0680043.dgn	USER NAME = kcorider	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MAINTENANCE OF TRAFFIC PLAN - STAGE 4B S.N. 068-0043 IL 108</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -					55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	128
Default	PLOT DATE = 12/11/2019 - 4:47:46 PM	DATE -	REVISED -	SCALE: 1" = 50'			SHEET OF SHEETS	STA. 24+57.58 TO STA. 49+00.00	ILLINOIS FED. AID PROJECT CONTRACT NO. 72D31			



FILE NAME = D672031-sht-MOT-stage4B-SN0680043.dgn	USER NAME = kcorider	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MAINTENANCE OF TRAFFIC PLAN - STAGE 4B</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -		<b>S.N. 068-0043 IL 108</b>			55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	129
	PLOT DATE = 12/11/2019 - 4:47:46 PM	CHECKED -	REVISED -		SCALE: 1" = 50'	SHEET	OF	SHEETS	STA. 49+00.00	TO STA. 71+00.00	CONTRACT NO. 72D31	
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

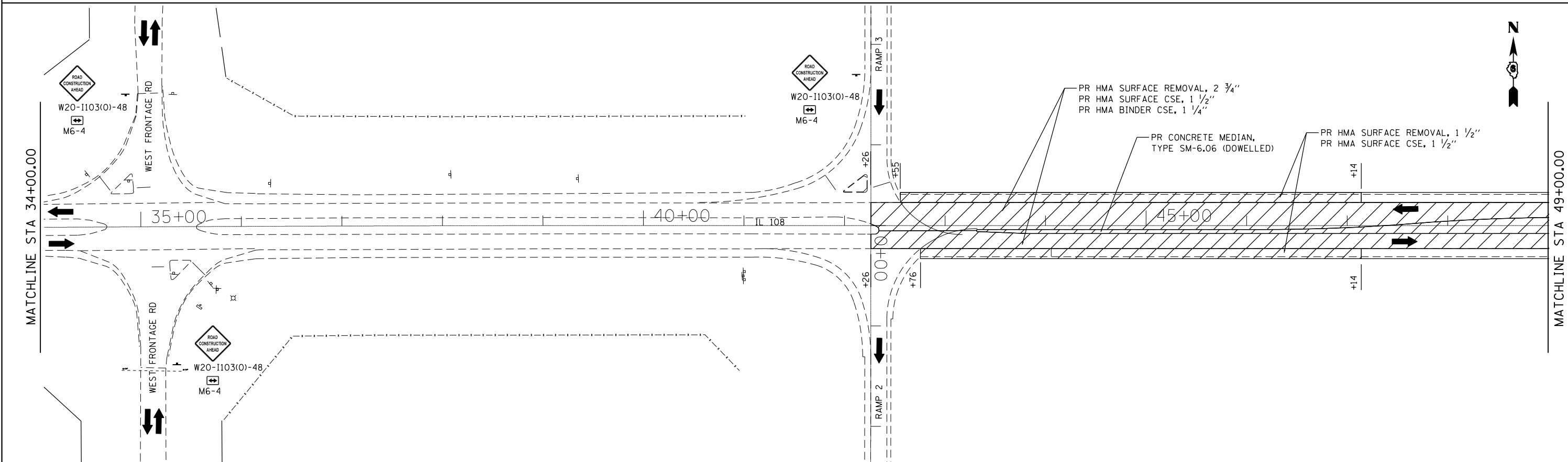
STAGE 5 NOTES

1. ERECT TRAFFIC CONTROL AND PROTECTION ACCORDING TO STANDARD 701701 AND AS SHOWN IN THESE PLANS.
2. CONSTRUCT MEDIAN AS SHOWN ON THE PLAN SHEET AND DETAIL SHEET.
3. ALL SIGNAGE, SPACING, TRAFFIC CONTROL EQUIPMENT AND TEMPORARY MARKINGS SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF HIGHWAY STANDARDS 701326 AND 701701, AND SHALL BE INCLUDED IN THE CONTRACT UNIT LS PRICE FOR EACH OF THE TRAFFIC CONTROL AND PROTECTION STANDARDS.



LEGEND

 WORK AREA	 REMOVAL ITEMS	 DIRECTION OF TRAFFIC	 DRUM WITH MONO DIRECTIONAL LIGHT	 TRAFFIC SIGN	 TYPE III BARRICADE WITH FLASHING LIGHTS	 SCALE IN FEET
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FILE NAME = D672031-sht-MOT-stage5-SN0680043.dgn	USER NAME = kcorider	DESIGNED -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
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		DATE -	REVISED -

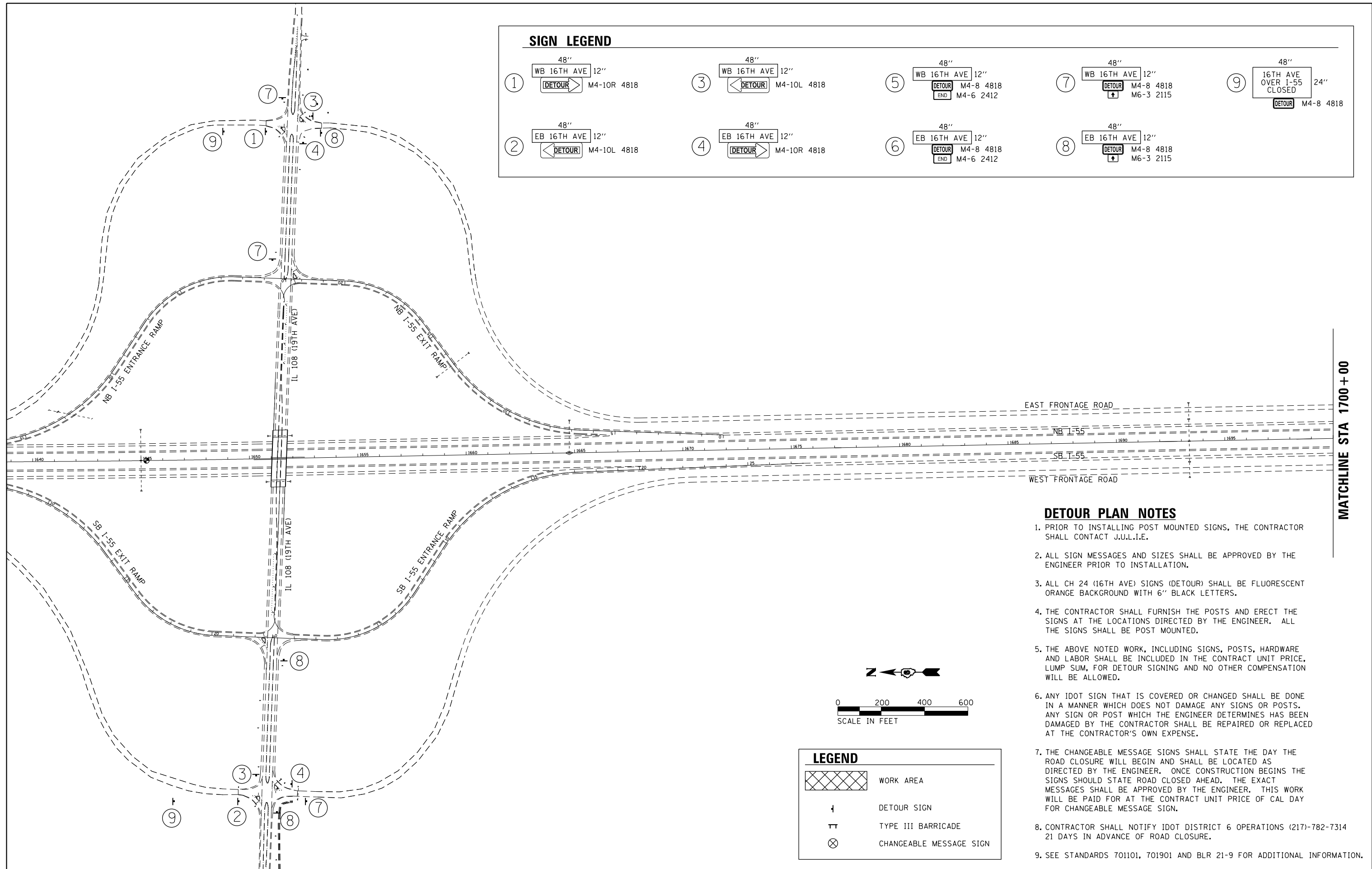
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC PLAN - STAGE 5  
S.N. 068-0043 IL 108

SCALE: 1" = 50' SHEET OF SHEETS STA. 24+57.58 TO STA. 49+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	130
CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	



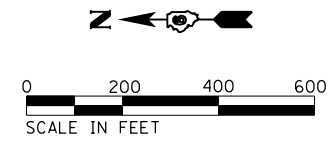


### SIGN LEGEND

①	<div style="border: 1px solid black; padding: 2px; width: fit-content; margin: 0 auto;">             48"              WB 16TH AVE 12"              DETOUR → M4-10R 4818           </div>	③	<div style="border: 1px solid black; padding: 2px; width: fit-content; margin: 0 auto;">             48"              WB 16TH AVE 12"              ← DETOUR M4-10L 4818           </div>	⑤	<div style="border: 1px solid black; padding: 2px; width: fit-content; margin: 0 auto;">             48"              WB 16TH AVE 12"              DETOUR → M4-8 4818              END M4-6 2412           </div>	⑦	<div style="border: 1px solid black; padding: 2px; width: fit-content; margin: 0 auto;">             48"              WB 16TH AVE 12"              DETOUR ↓ M4-8 4818              M6-3 2115           </div>	⑨	<div style="border: 1px solid black; padding: 2px; width: fit-content; margin: 0 auto;">             48"              16TH AVE              OVER I-55              CLOSED              DETOUR M4-8 4818           </div>
②	<div style="border: 1px solid black; padding: 2px; width: fit-content; margin: 0 auto;">             48"              EB 16TH AVE 12"              ← DETOUR M4-10L 4818           </div>	④	<div style="border: 1px solid black; padding: 2px; width: fit-content; margin: 0 auto;">             48"              EB 16TH AVE 12"              DETOUR → M4-10R 4818           </div>	⑥	<div style="border: 1px solid black; padding: 2px; width: fit-content; margin: 0 auto;">             48"              EB 16TH AVE 12"              DETOUR → M4-8 4818              END M4-6 2412           </div>	⑧	<div style="border: 1px solid black; padding: 2px; width: fit-content; margin: 0 auto;">             48"              EB 16TH AVE 12"              DETOUR ↓ M4-8 4818              M6-3 2115           </div>		

### DETOUR PLAN NOTES

1. PRIOR TO INSTALLING POST MOUNTED SIGNS, THE CONTRACTOR SHALL CONTACT J.U.L.I.E.
2. ALL SIGN MESSAGES AND SIZES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
3. ALL CH 24 (16TH AVE) SIGNS (DETOUR) SHALL BE FLUORESCENT ORANGE BACKGROUND WITH 6" BLACK LETTERS.
4. THE CONTRACTOR SHALL FURNISH THE POSTS AND ERECT THE SIGNS AT THE LOCATIONS DIRECTED BY THE ENGINEER. ALL THE SIGNS SHALL BE POST MOUNTED.
5. THE ABOVE NOTED WORK, INCLUDING SIGNS, POSTS, HARDWARE AND LABOR SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE, LUMP SUM, FOR DETOUR SIGNING AND NO OTHER COMPENSATION WILL BE ALLOWED.
6. ANY IDOT SIGN THAT IS COVERED OR CHANGED SHALL BE DONE IN A MANNER WHICH DOES NOT DAMAGE ANY SIGNS OR POSTS. ANY SIGN OR POST WHICH THE ENGINEER DETERMINES HAS BEEN DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S OWN EXPENSE.
7. THE CHANGEABLE MESSAGE SIGNS SHALL STATE THE DAY THE ROAD CLOSURE WILL BEGIN AND SHALL BE LOCATED AS DIRECTED BY THE ENGINEER. ONCE CONSTRUCTION BEGINS THE SIGNS SHOULD STATE ROAD CLOSED AHEAD. THE EXACT MESSAGES SHALL BE APPROVED BY THE ENGINEER. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE OF CAL DAY FOR CHANGEABLE MESSAGE SIGN.
8. CONTRACTOR SHALL NOTIFY IDOT DISTRICT 6 OPERATIONS (217)-782-7314 21 DAYS IN ADVANCE OF ROAD CLOSURE.
9. SEE STANDARDS 701101, 701901 AND BLR 21-9 FOR ADDITIONAL INFORMATION.



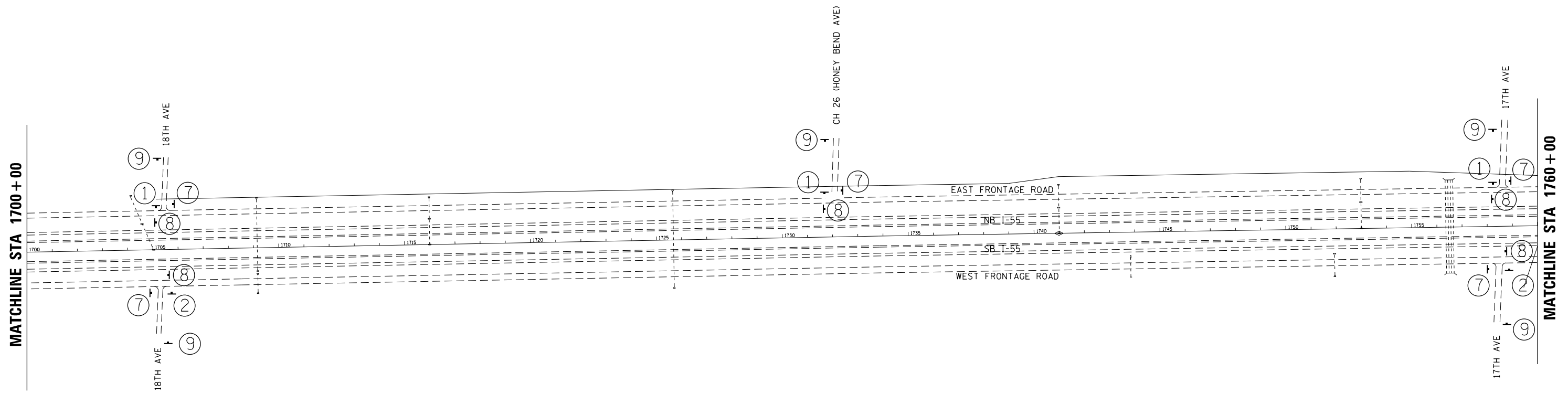
### LEGEND

	WORK AREA
↓	DETOUR SIGN
⊥	TYPE III BARRICADE
⊗	CHANGEABLE MESSAGE SIGN

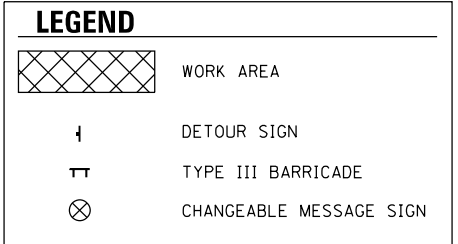
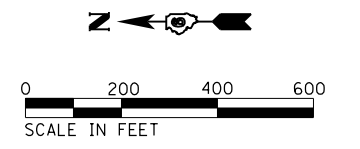
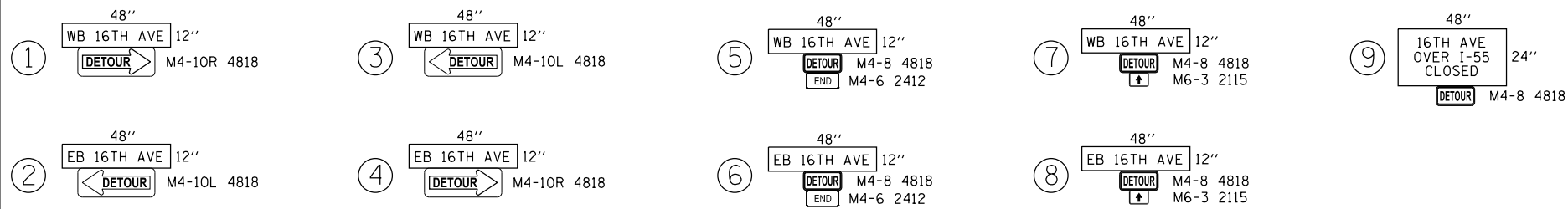
MATCHLINE STA 1700 + 00

FILE NAME = D672031-sht-MOTplan-SN0680044.dgn	USER NAME = karider	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MAINTENANCE OF TRAFFIC – DETOUR PLAN S.N. 068-0044 CH 24 (16TH AVE) OVER I-55</b>	F.A.I. R.T.E. = 55	SECTION = 68-1,3 RS-3,68-2 RS-5)BR	COUNTY = MONTGOMERY	TOTAL SHEETS = 307	SHEET NO. = 132		
Default	PLOT SCALE = 400.0000' / in.	CHECKED -	REVISED -			SCALE: 1" = 200'	SHEET OF SHEETS	STA. 1640+00.00	TO STA. 1700+00.00	ILLINOIS FED. AID PROJECT		
	PLOT DATE = 12/11/2019 - 4:48:22 PM	DATE -	REVISED -			CONTRACT NO. 72D31						
9. SEE STANDARDS 701101, 701901 AND BLR 21-9 FOR ADDITIONAL INFORMATION.												





**SIGN LEGEND**



FILE NAME = D672031-sht-MOTplan-SN0680044.dgn	USER NAME = karider	DESIGNED -	REVISED -
Default	PLOT SCALE = 400.0000' / in.	DRAWN -	REVISED -
	PLOT DATE = 12/11/2019 - 4:48:23 PM	CHECKED -	REVISED -
		DATE -	REVISED -

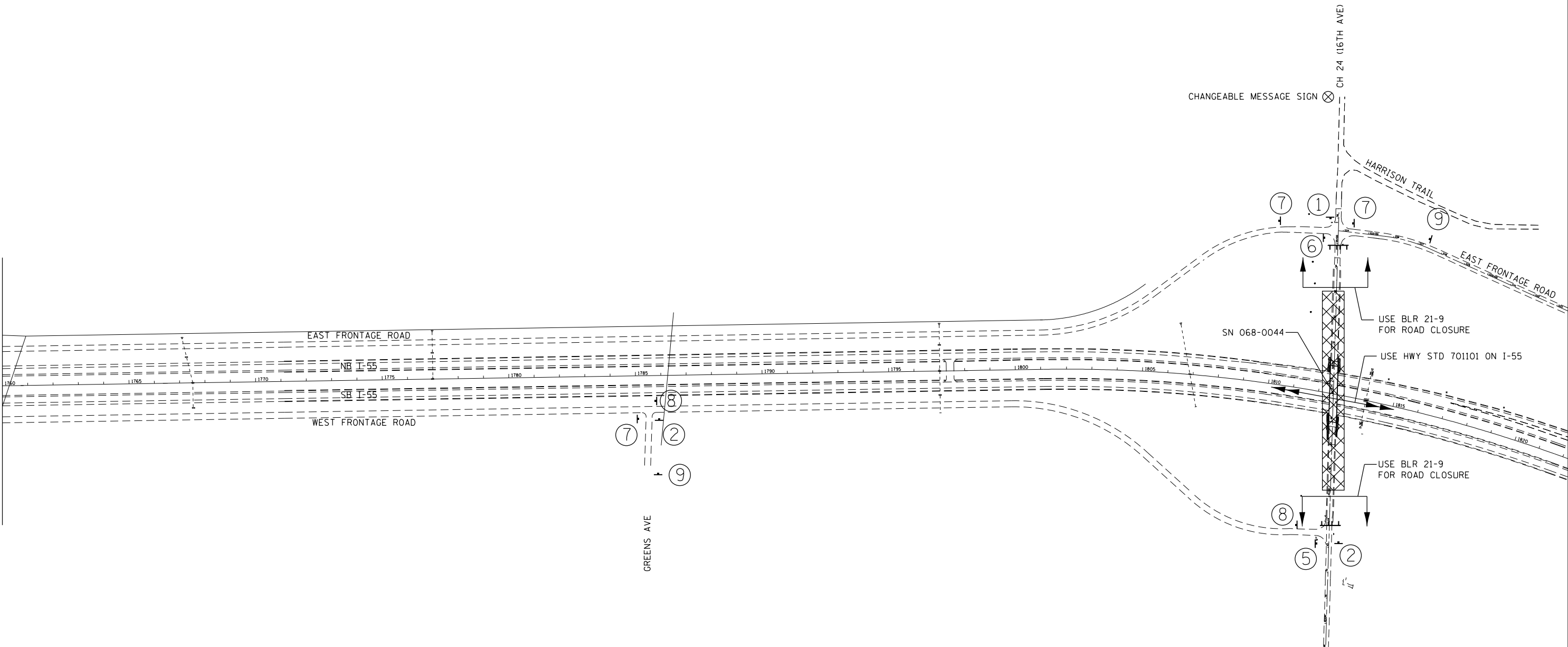
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - DETOUR PLAN  
S.N. 068-0044 CH 24 (16TH AVE) OVER I-55**

SCALE: 1" = 200' SHEET OF SHEETS STA. 1700+00.00 TO STA. 1760+00.00

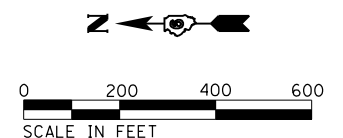
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	133
CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	

MATCHLINE STA 1760 +00



SIGN LEGEND					
①	48" WB 16TH AVE 12" DETOUR M4-10R 4818	③	48" WB 16TH AVE 12" DETOUR M4-10L 4818	⑤	48" WB 16TH AVE 12" DETOUR M4-8 4818 END M4-6 2412
②	48" EB 16TH AVE 12" DETOUR M4-10L 4818	④	48" EB 16TH AVE 12" DETOUR M4-10R 4818	⑥	48" EB 16TH AVE 12" DETOUR M4-8 4818 END M4-6 2412
⑦	48" WB 16TH AVE 12" DETOUR M4-8 4818 M6-3 2115	⑧	48" EB 16TH AVE 12" DETOUR M4-8 4818 M6-3 2115	⑨	48" 16TH AVE OVER I-55 CLOSED 24" DETOUR M4-8 4818

LEGEND	
	WORK AREA
	DETOUR SIGN
	TYPE III BARRICADE
	CHANGEABLE MESSAGE SIGN



### STAGE 1

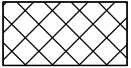
**TRAFFIC**  
 TR 48 - SHIFT TRAFFIC NORTH AND PROVIDE ONE 12' TRAVEL LANE, UTILIZING ALTERNATING ONE-WAY OPERATIONS. SHALL FOLLOW HIGHWAY STANDARD 701201.


FAI 55 - MAINTAIN TRAFFIC IN CURRENT LANE CONFIGURATIONS. SHALL FOLLOW HIGHWAY STANDARD 701101.

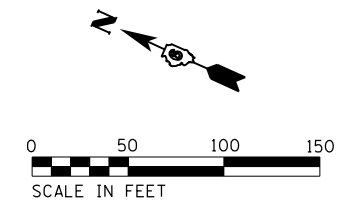
**CONSTRUCTION**  
 SEE STRUCTURE PLANS.

PRIMARY IDOT STANDARDS USED THIS STAGE  
 (SEE SPECIAL PROVISIONS FOR ADDITIONAL)  
 701101, 701201, 701901

**LEGEND**

 WORK AREA

 DIRECTION OF TRAFFIC



### STAGE 1

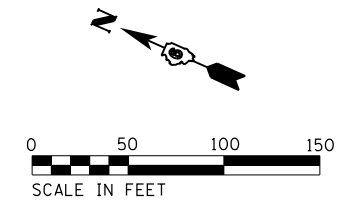
### STAGE 2

**TRAFFIC**  
 TR 48 - SHIFT TRAFFIC SOUTH AND PROVIDE ONE 12' TRAVEL LANE, UTILIZING ALTERNATING ONE-WAY OPERATIONS. SHALL FOLLOW HIGHWAY STANDARD 701201.

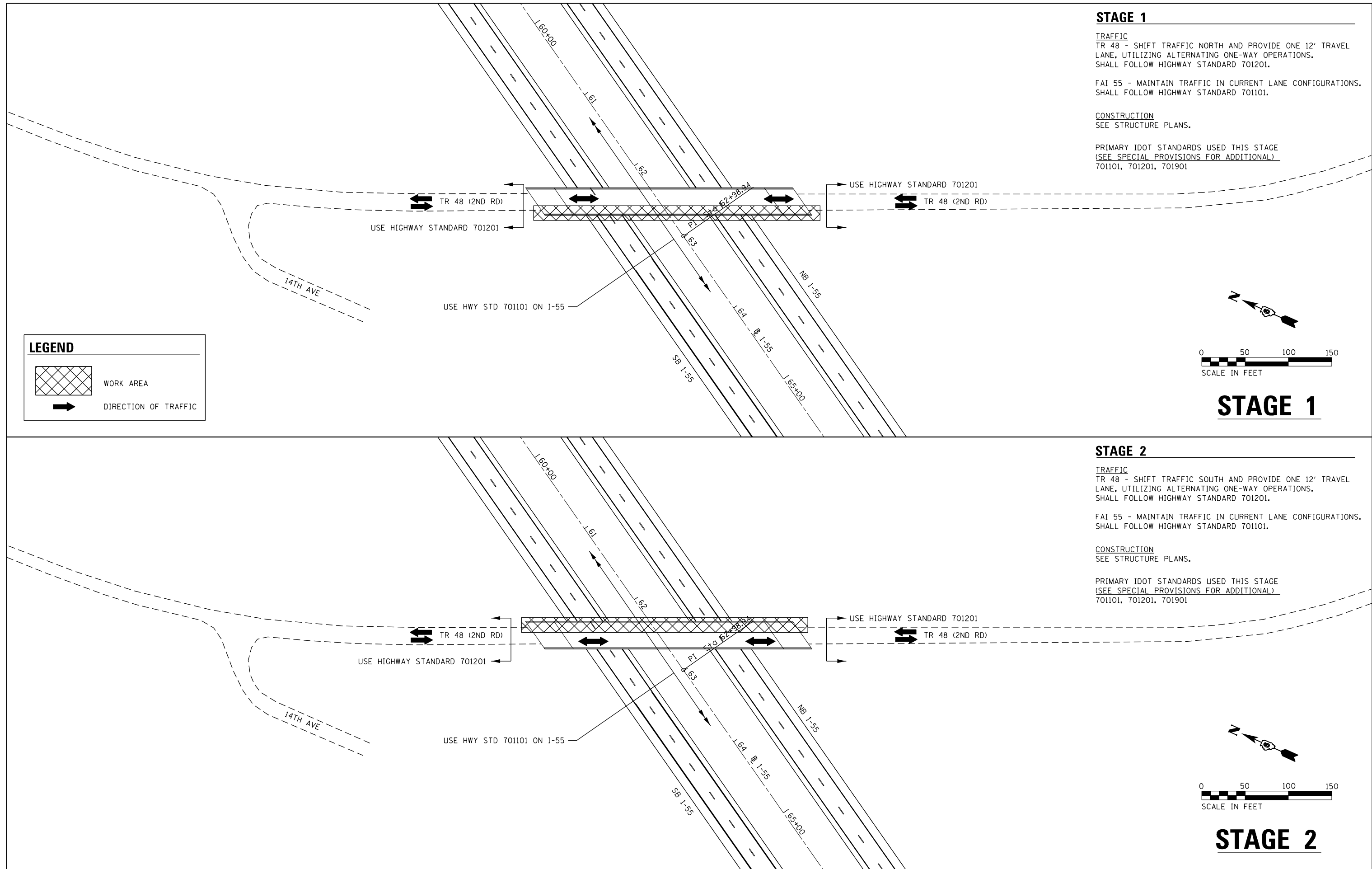
FAI 55 - MAINTAIN TRAFFIC IN CURRENT LANE CONFIGURATIONS. SHALL FOLLOW HIGHWAY STANDARD 701101.

**CONSTRUCTION**  
 SEE STRUCTURE PLANS.

PRIMARY IDOT STANDARDS USED THIS STAGE  
 (SEE SPECIAL PROVISIONS FOR ADDITIONAL)  
 701101, 701201, 701901



### STAGE 2



FILE NAME = D672031-sht-MOTplan-SN0680047.dgn	USER NAME = karider	DESIGNED - DRAWN -	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MAINTENANCE OF TRAFFIC S.N. 068-0047 TR 48 (2ND RD) OVER I-55</b>				F.A.I. RTE. 55	SECTION (68-1,3 RS-3,68-2 RS-5)BR	COUNTY MONTGOMERY	TOTAL SHEETS 307	SHEET NO. 135
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE: 1" = 50'	SHEET	OF	SHEETS	STA.	TO	STA.	CONTRACT NO. 72031	
	PLOT DATE = 12/11/2019 - 4:48:41 PM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

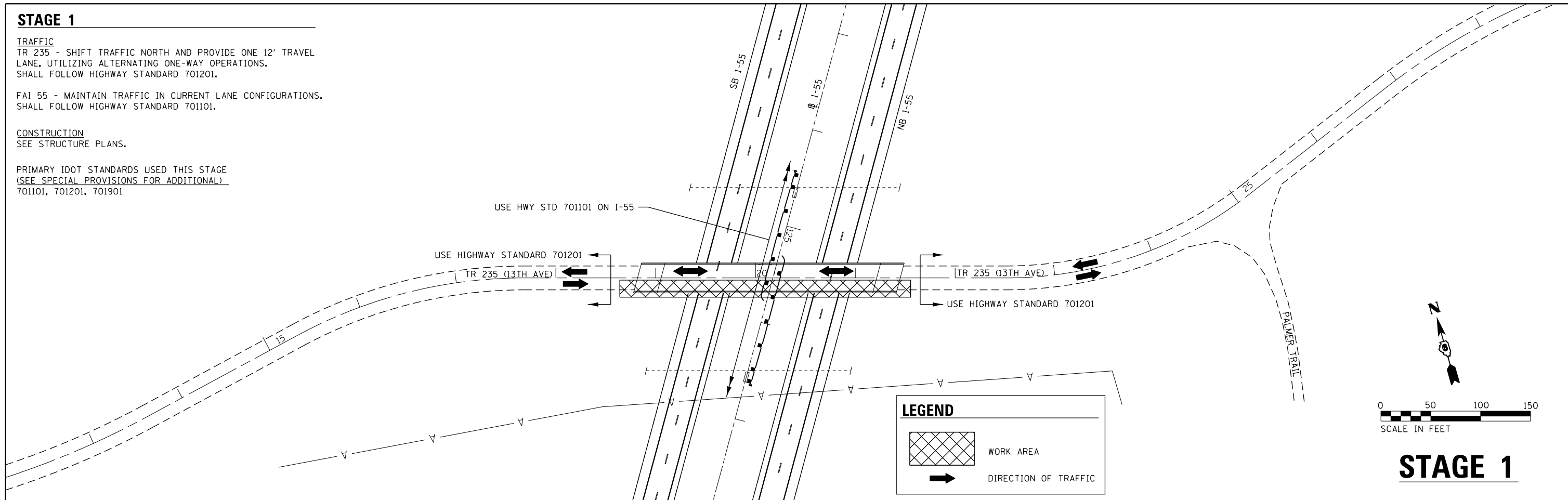
### STAGE 1

**TRAFFIC**  
 TR 235 - SHIFT TRAFFIC NORTH AND PROVIDE ONE 12' TRAVEL LANE, UTILIZING ALTERNATING ONE-WAY OPERATIONS. SHALL FOLLOW HIGHWAY STANDARD 701201.

FAI 55 - MAINTAIN TRAFFIC IN CURRENT LANE CONFIGURATIONS. SHALL FOLLOW HIGHWAY STANDARD 701101.

**CONSTRUCTION**  
 SEE STRUCTURE PLANS.

PRIMARY IDOT STANDARDS USED THIS STAGE  
 (SEE SPECIAL PROVISIONS FOR ADDITIONAL)  
 701101, 701201, 701901



### STAGE 1

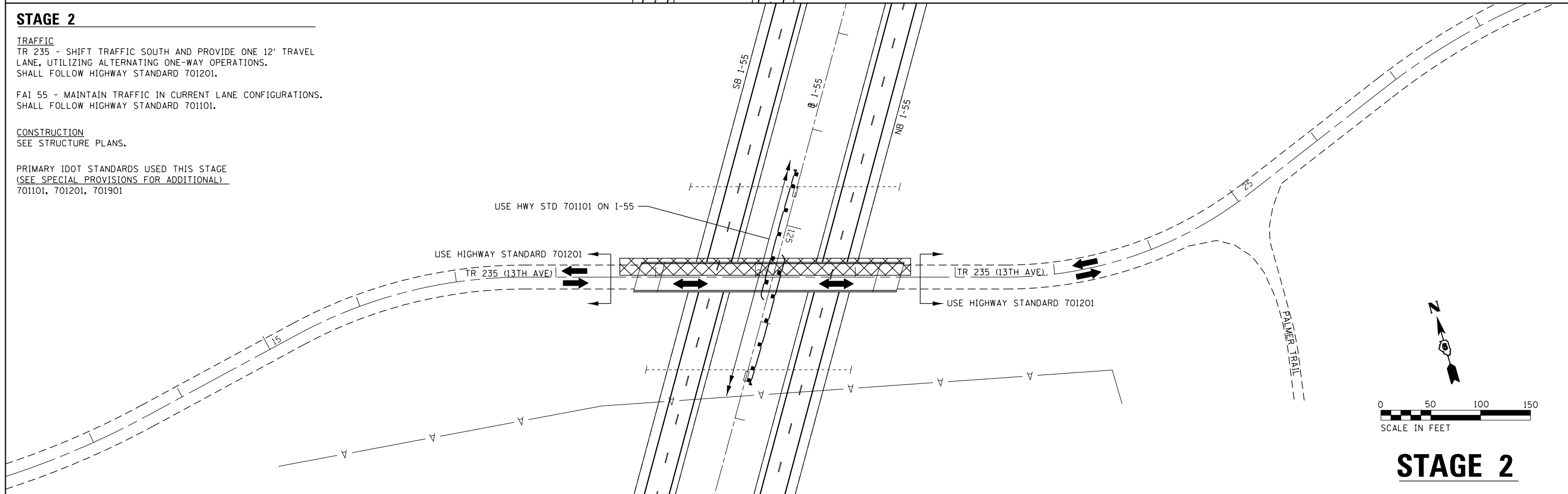
### STAGE 2

**TRAFFIC**  
 TR 235 - SHIFT TRAFFIC SOUTH AND PROVIDE ONE 12' TRAVEL LANE, UTILIZING ALTERNATING ONE-WAY OPERATIONS. SHALL FOLLOW HIGHWAY STANDARD 701201.

FAI 55 - MAINTAIN TRAFFIC IN CURRENT LANE CONFIGURATIONS. SHALL FOLLOW HIGHWAY STANDARD 701101.

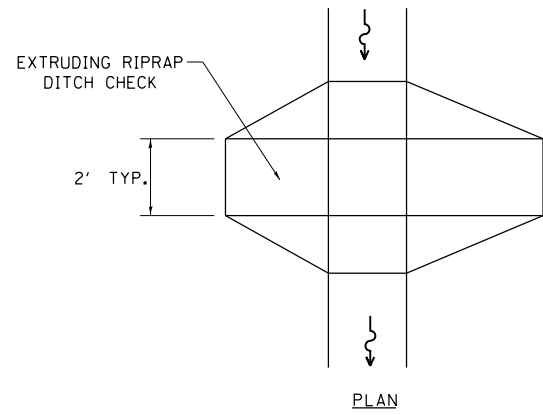
**CONSTRUCTION**  
 SEE STRUCTURE PLANS.

PRIMARY IDOT STANDARDS USED THIS STAGE  
 (SEE SPECIAL PROVISIONS FOR ADDITIONAL)  
 701101, 701201, 701901

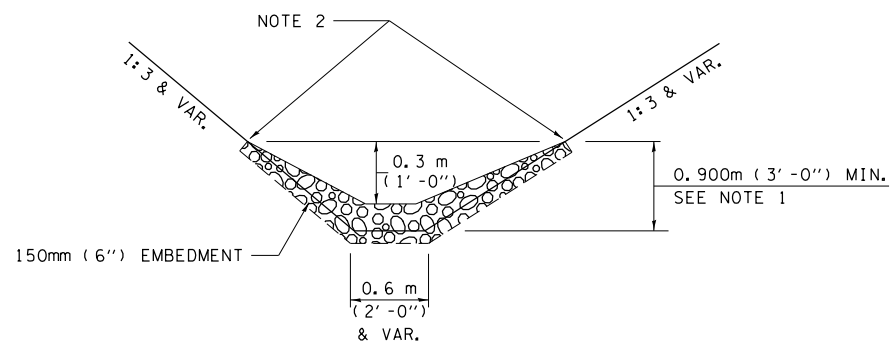


### STAGE 2

FILE NAME = D672031-sht-MOTplan-SN0680050.dgn	USER NAME = kcorider	DESIGNED - DRAWN -	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MAINTENANCE OF TRAFFIC S.N. 068-0050 TR 235 (13TH AVE) OVER I-55</b>			F.A.I. RTE. 55	SECTION 68-1,3 RS-3,68-2 RS-5)BR	COUNTY MONTGOMERY	TOTAL SHEETS 307	SHEET NO. 136
Default	PLOT SCALE = 100.0000' / in.	CHECKED - DATE -	REVISED - REVISED -		SCALE: 1" = 50'	SHEET	OF	SHEETS	STA. 13+00.00	TO STA. 28+00.00	CONTRACT NO. 72D31	
	PLOT DATE = 12/11/2019 - 4:48:58 PM				ILLINOIS FED. AID PROJECT							



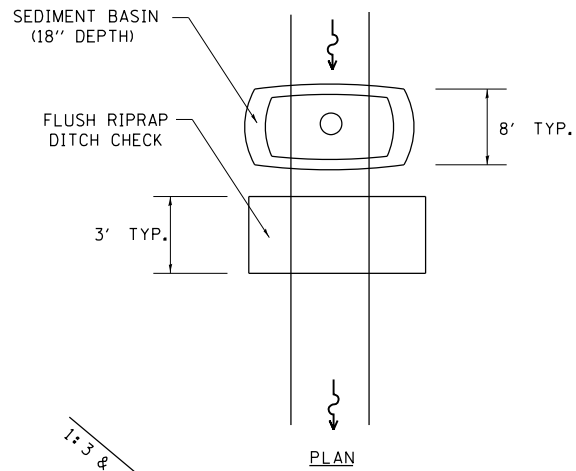
PLAN



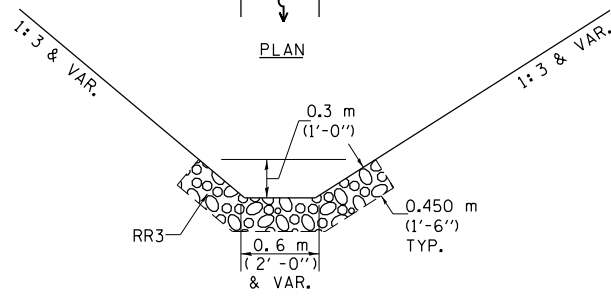
ELEVATION

OPTION 1

(EXTRUDING DITCH CHECK)  
RECOMMENDED FOR AREAS  
W/ RIPRAP DITCH LINING



PLAN



ELEVATION

OPTION 2

(FLUSH DITCH CHECK)  
RECOMMENDED FOR AREAS  
W/O RIPRAP DITCH LINING

**AGGREGATE DITCH CHECK**

OPTIONS 1 & 2 OR  
AS DIRECTED BY THE ENGINEER

NOTE 1: RIPRAP SHALL EXTEND FAR ENOUGH UP THE SLOPES TO ALLOW 0.3m (1') OVERTOPPING TO AVOID ERODING AROUND THE EDGES OF THE RIPRAP.

NOTE 2: ENDS SHALL BE TIED INTO SLOPES.

**LEGEND FOR EROSION CONTROL PLAN**

ITEM	SYMBOL
AGGREGATE DITCH CHECKS	
INLET PIPE PROTECTION	
PERIMETER EROSION BARRIER	
SEDIMENT BASINS	
EARTH EXCAVATION FOR EROSION CONTROL AGGREGATE (EROSION CONTROL)	
PRESERVE EXISTING TREES, WOODLANDS, AND UNDERSTORY (OUTSIDE CONSTRUCTION LIMITS)	
ITEM PLACED AT BEGINNING OF CONSTRUCTION (Requirement)	*  *
ITEM PLACED AS DIRECTED BY ENGINEER (When required by situation)	
DIRECTION OF OVERLAND FLOW	

**GENERAL NOTES:**

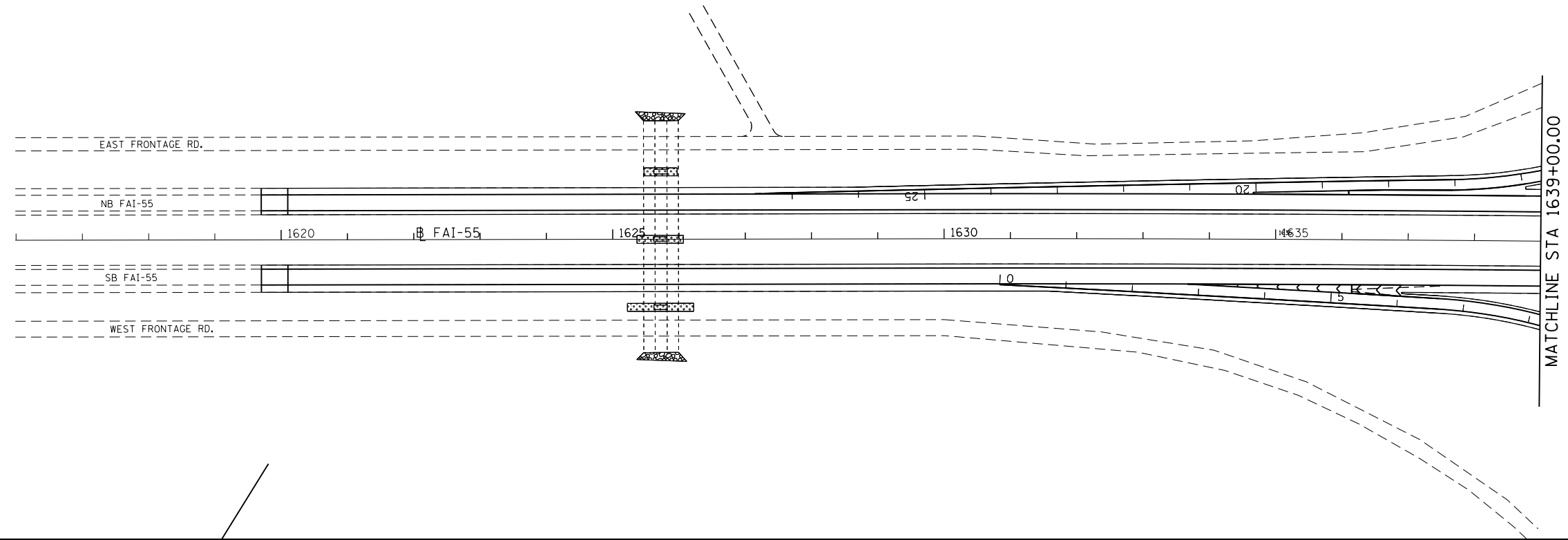
All items shall be constructed as shown on this sheet, on Standard 280001, and as directed by the Engineer.

FILE NAME = D672E49-sht-swpplen.dgn	USER NAME = karider	DESIGNED - DRAWN - CADD	REVISED - REVISED - OCT 2010 (JCN)
Default	PLOT SCALE = 40.000' / in.	CHECKED - DATE - APRIL 5, 1999	REVISED - REVISED - MAY 2012 (JPM)

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

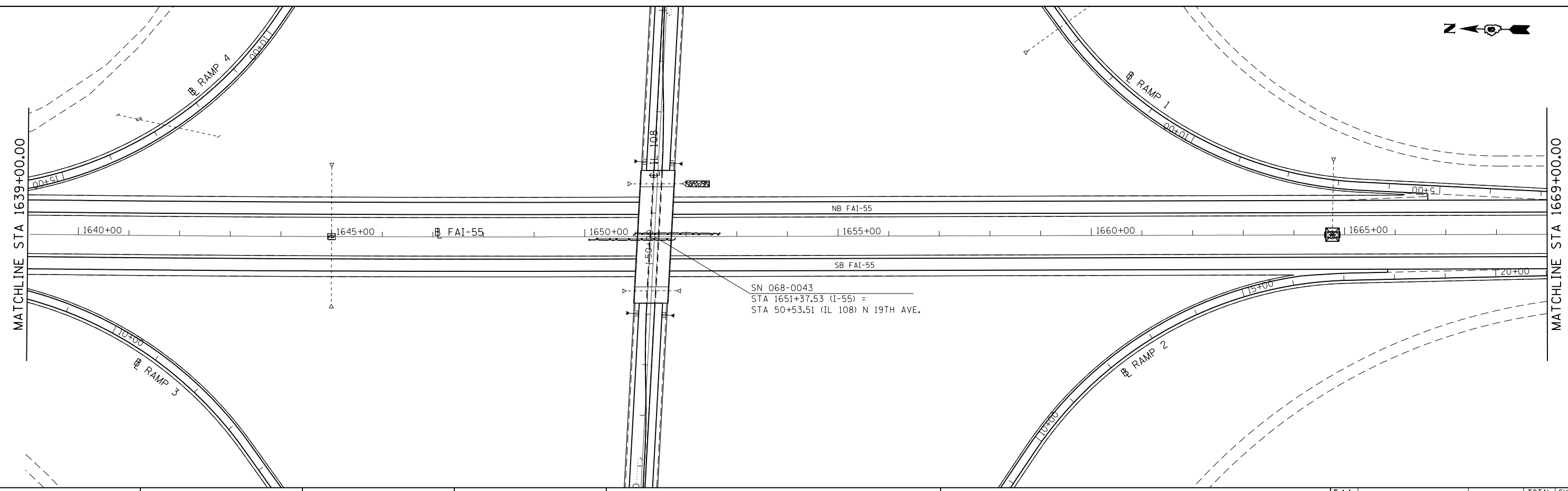
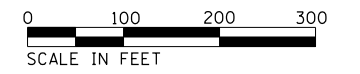
<b>I-55 EROSION CONTROL PLAN</b>			
SCALE: N.T.S.	SHEET	OF	SHEETS
STA.	TO STA.		

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	137
				CONTRACT NO. 72D31
ILLINOIS FED. AID PROJECT				



**LEGEND**

- PERIMETER EROSION BARRIER
- INLET PROTECTION
- AGGREGATE DITCH CHECK
- SEEDING CLASS 2 & MULCH METHOD 2
- TEMPORARY EROSION CONTROL SEEDING & MULCH METHOD 2
- HEAVY DUTY EROSION CONTROL BLANKET & SEEDING CLASS 2
- RIPRAP



FILE NAME = D672031-sht-eros-155.dgn  
 Default

USER NAME = karider  
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 PLOT DATE = 12/10/2019 - 7:17:26 PM

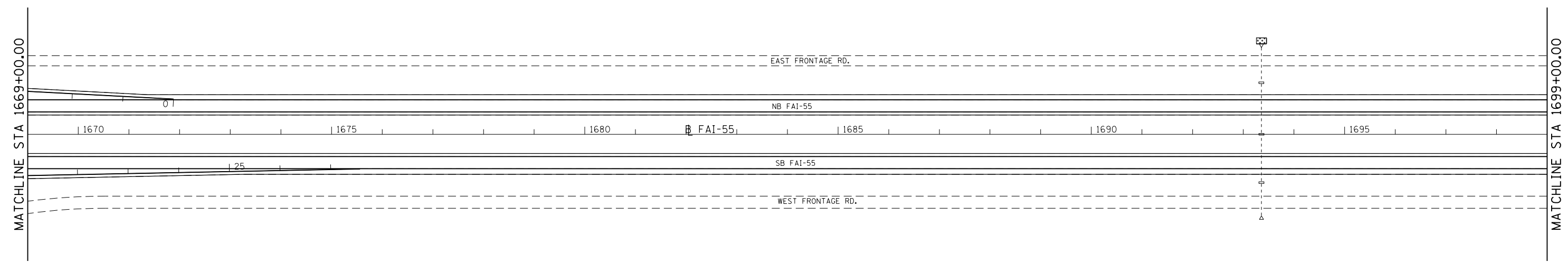
DESIGNED -  
 DRAWN -  
 CHECKED -  
 DATE -

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

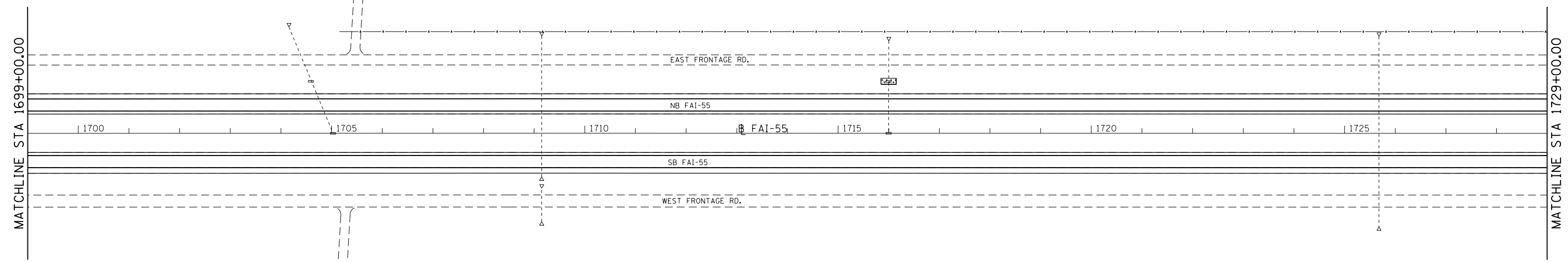
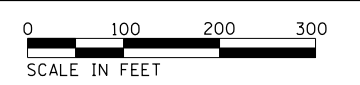
**I-55  
 EROSION CONTROL PLAN**  
 SCALE: 1" = 100'  
 SHEET OF SHEETS STA. 1616+00.00 TO STA. 1669+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	138
<b>CONTRACT NO. 72D31</b>				
ILLINOIS FED. AID PROJECT				



LEGEND

- PERIMETER EROSION BARRIER
- AGGREGATE DITCH CHECK
- TEMPORARY EROSION CONTROL SEEDING & MULCH METHOD 2
- RIPRAP
- INLET PROTECTION
- SEEDING CLASS 2 & MULCH METHOD 2
- HEAVY DUTY EROSION CONTROL BLANKET & SEEDING CLASS 2



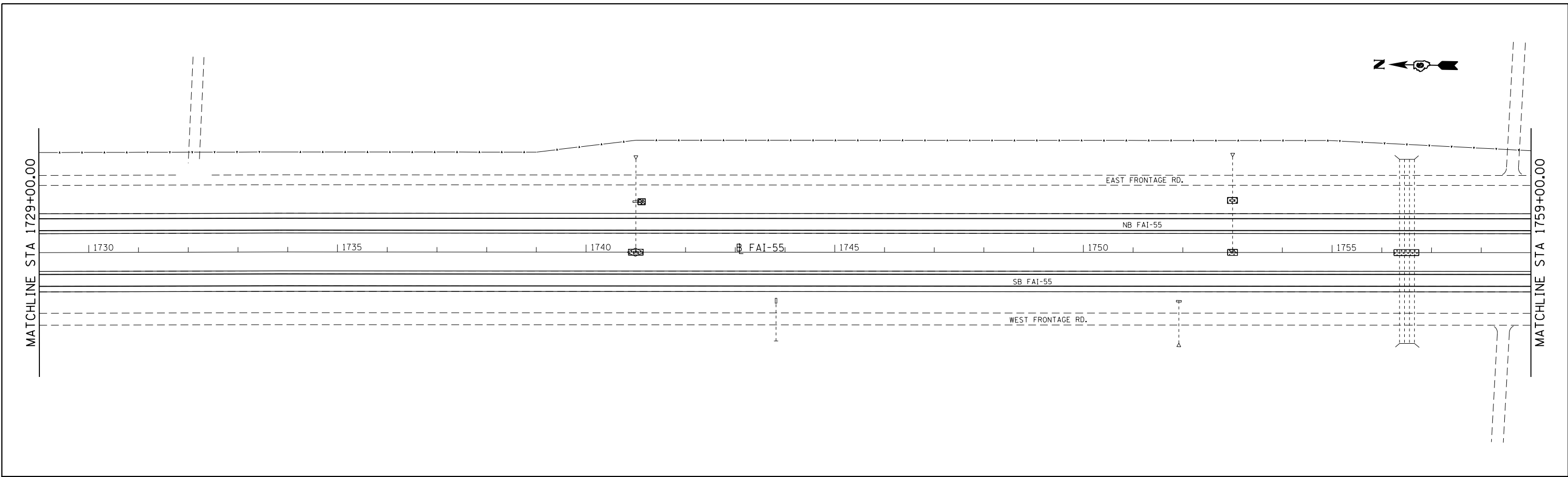
FILE NAME = D672031-sht-eros-155.dgn	USER NAME = kcorider	DESIGNED -	REVISED -
Default	PLOT SCALE = 200.0000' / in.	DRAWN -	REVISED -
	PLOT DATE = 12/10/2019 - 7:17:27 PM	CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**I-55  
EROSION CONTROL PLAN**

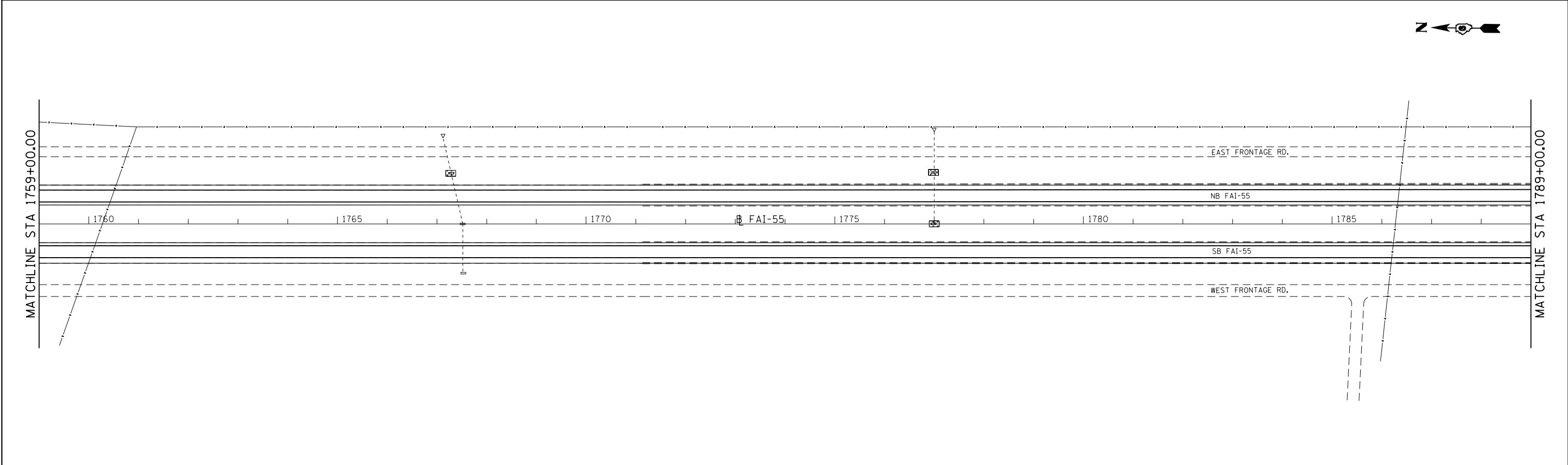
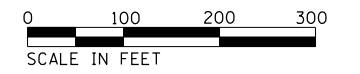
SCALE: 1" = 100'    SHEET    OF    SHEETS    STA. 1669+00.00    TO    STA. 1729+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	139
<b>CONTRACT NO. 72D31</b>				
ILLINOIS FED. AID PROJECT				



LEGEND

- PERIMETER EROSION BARRIER
- AGGREGATE DITCH CHECK
- TEMPORARY EROSION CONTROL SEEDING & MULCH METHOD 2
- RIPRAP
- INLET PROTECTION
- SEEDING CLASS 2 & MULCH METHOD 2
- HEAVY DUTY EROSION CONTROL BLANKET & SEEDING CLASS 2



FILE NAME = D672031-sht-eros-155.dgn	USER NAME = kcorider	DESIGNED -	REVISED -
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		DATE -	REVISED -

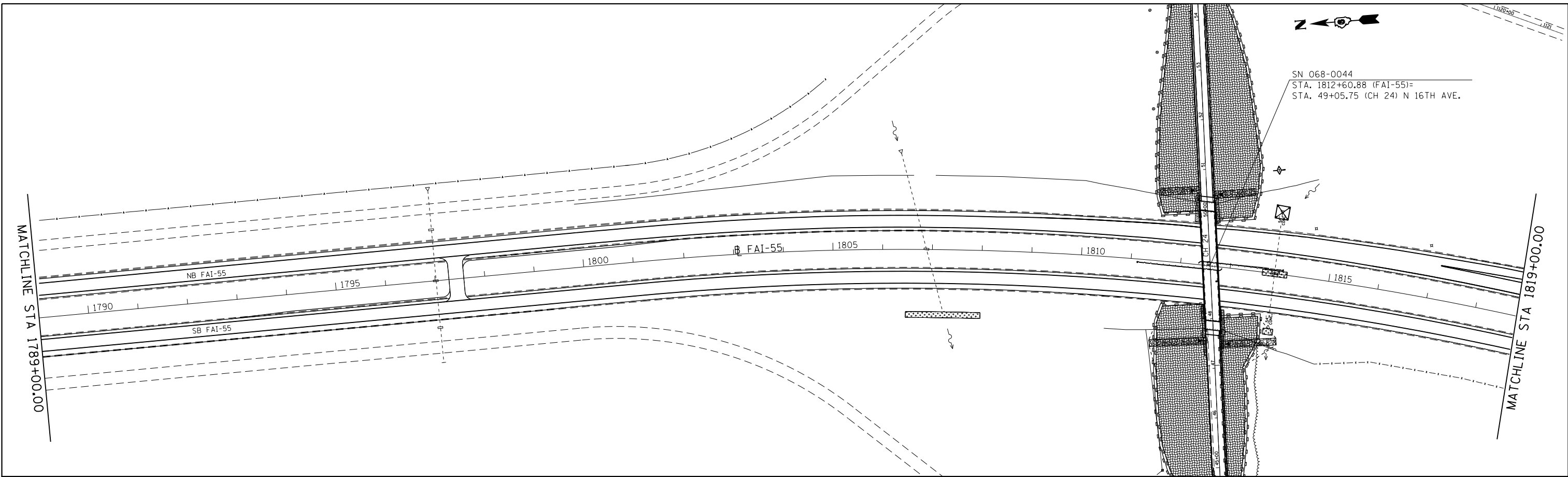
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**I-55  
EROSION CONTROL PLAN**

SCALE: 1" = 100'    SHEET    OF    SHEETS    STA. 1729+00.00    TO    STA. 1789+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	140
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				



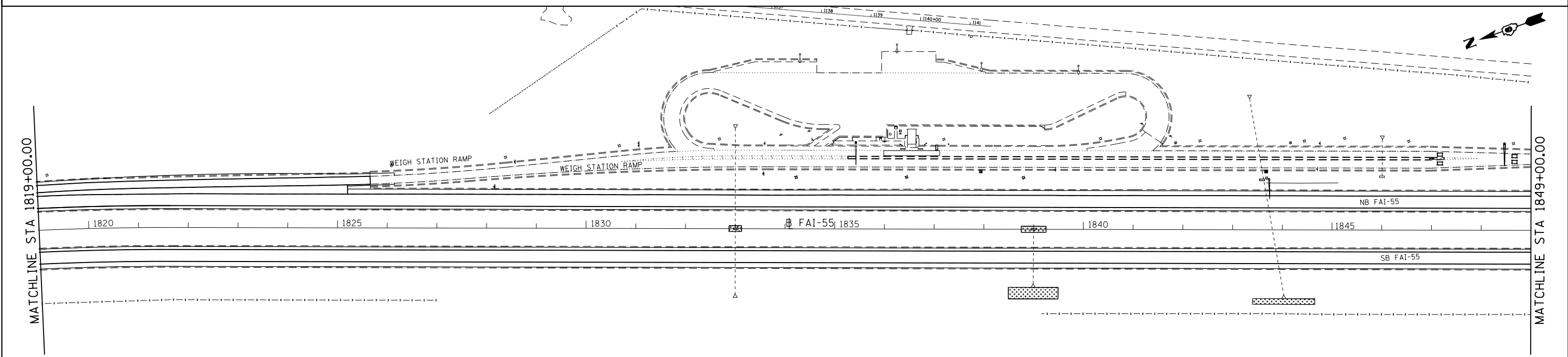


SN 068-0044  
 STA. 1812+60.88 (FAI-55)=  
 STA. 49+05.75 (CH 24) N 16TH AVE.

**LEGEND**

PERIMETER EROSION BARRIER	AGGREGATE DITCH CHECK	TEMPORARY EROSION CONTROL SEEDING & MULCH METHOD 2	RIPRAP
INLET PROTECTION	SEEDING CLASS 2 & MULCH METHOD 2	HEAVY DUTY EROSION CONTROL BLANKET & SEEDING CLASS 2	

0 100 200 300  
SCALE IN FEET



FILE NAME =  
 D672031-sht-eros-155.dgn  
 Default

USER NAME = korider  
 PLOT SCALE = 200.0000' / in.  
 PLOT DATE = 12/10/2019 - 7:17:28 PM

DESIGNED -  
 DRAWN -  
 CHECKED -  
 DATE -

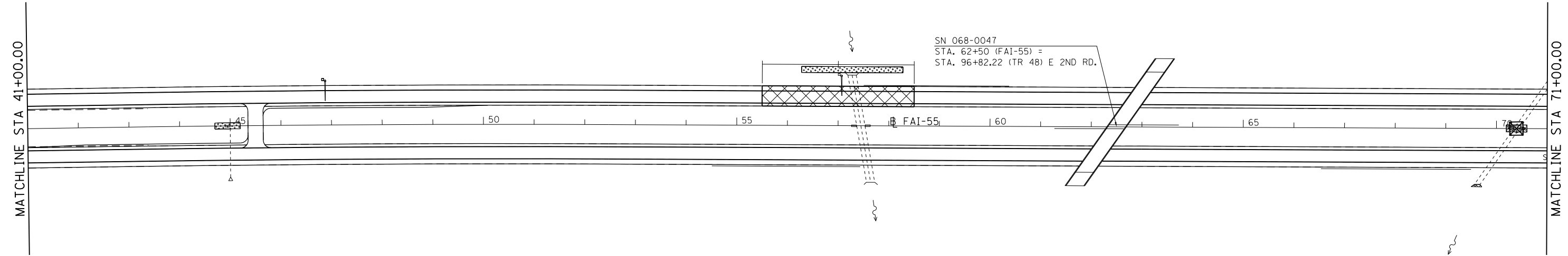
REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**I-55  
 EROSION CONTROL PLAN**  
 SCALE: 1" = 100' SHEET OF SHEETS STA. 1789+00.00 TO STA. 1849+00.00

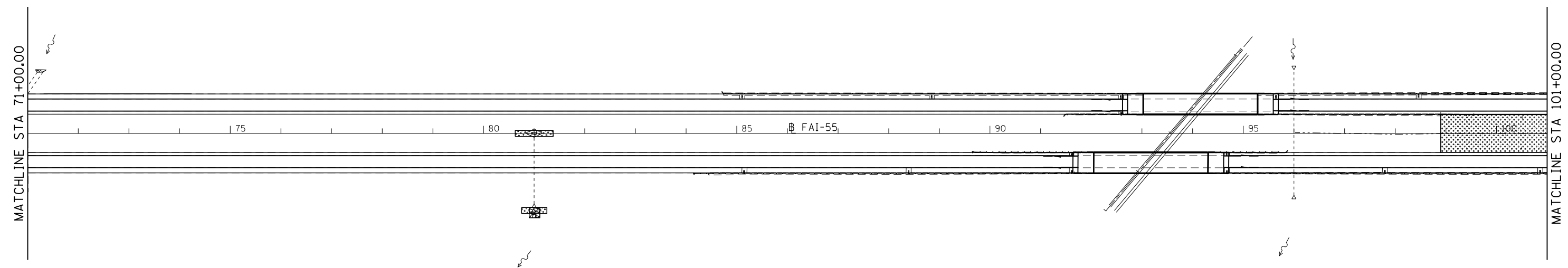
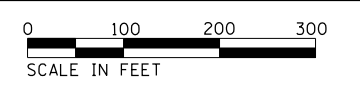
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3,68-2 RS-5)BR	MONTGOMERY	307	141
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				





LEGEND

- PERIMETER EROSION BARRIER
- INLET PROTECTION
- AGGREGATE DITCH CHECK
- SEEDING CLASS 2 & MULCH METHOD 2
- TEMPORARY EROSION CONTROL SEEDING & MULCH METHOD 2
- HEAVY DUTY EROSION CONTROL BLANKET & SEEDING CLASS 2
- RIPRAP



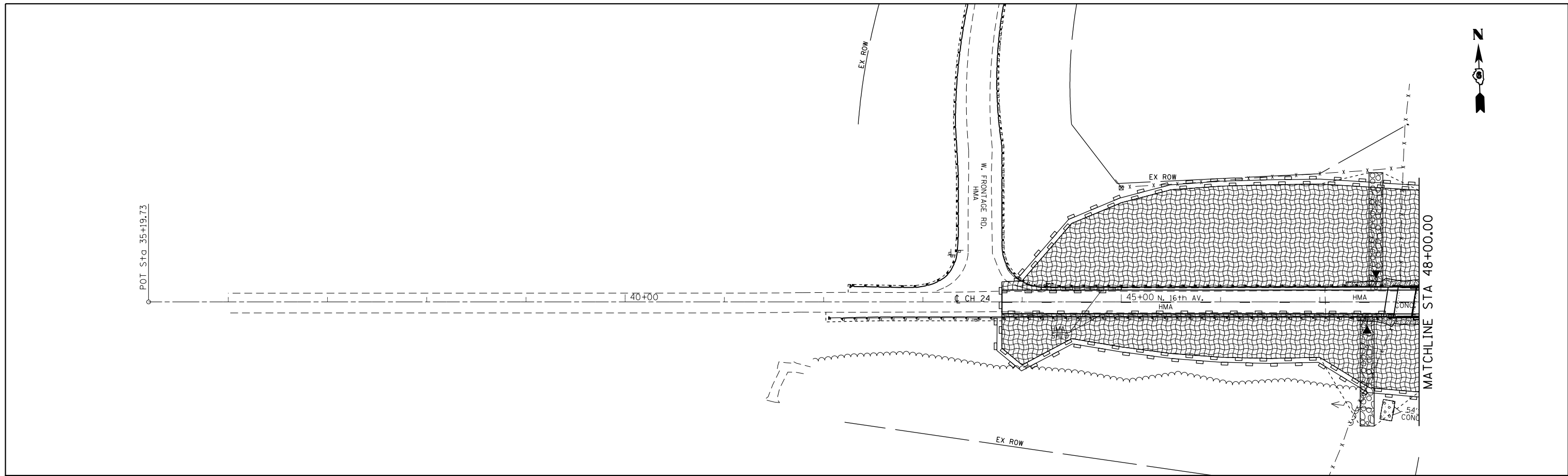
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		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>I-55 EROSION CONTROL PLAN</b>			
SCALE: 1" = 100'	SHEET	OF	SHEETS
		STA. 41+00.00	TO STA. 101+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				

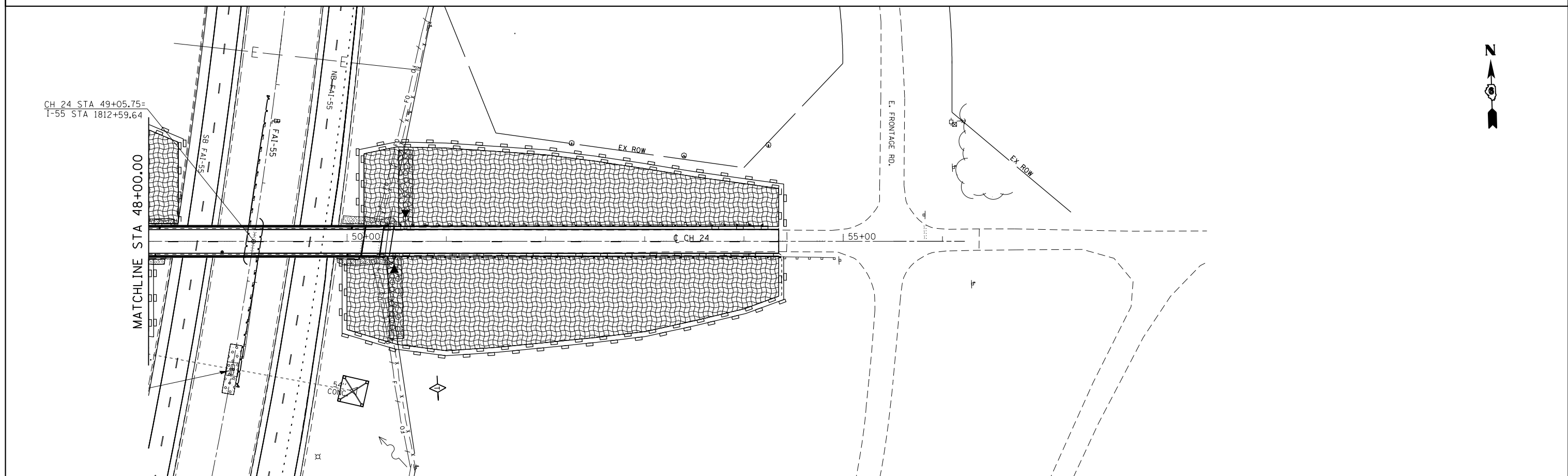




**LEGEND**

PERIMETER EROSION BARRIER	AGGREGATE DITCH CHECK	TEMPORARY EROSION CONTROL SEEDING & MULCH METHOD 2	RIPRAP
INLET PROTECTION	SEEDING CLASS 2 & MULCH METHOD 2	HEAVY DUTY EROSION CONTROL BLANKET & SEEDING CLASS 2	

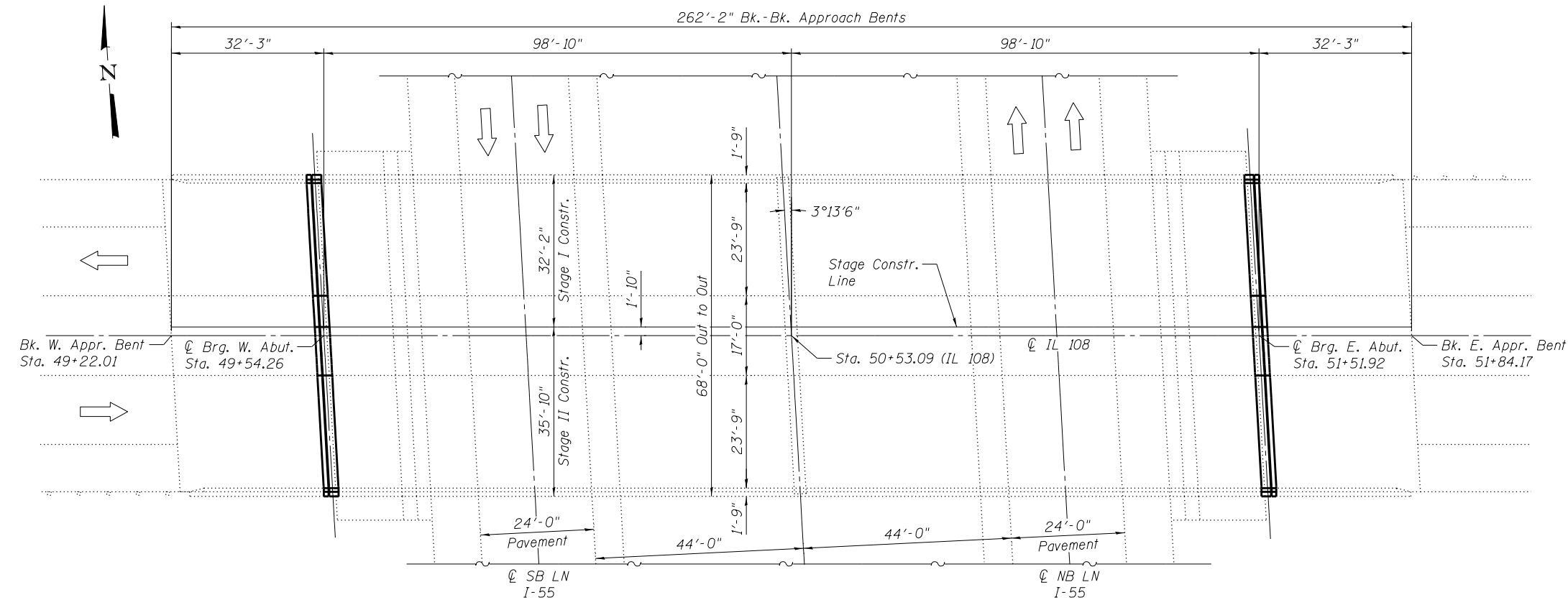
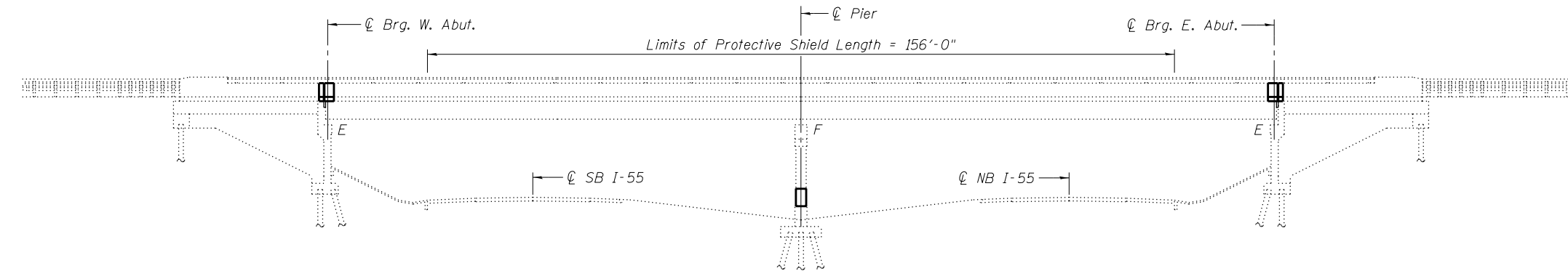
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SCALE IN FEET



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Existing Structure: S.N. 068-0043, originally built in 1973, is a two-span continuous steel girder bridge with vaulted abutments and a multicolumn pier. A 2 1/4" min. microsilica conc. overlay was placed on the deck in 2003. Structure is to be repaired as detailed in these plans. One lane of traffic will be maintained in each direction using staged construction.

No Salvage



**TOTAL BILL OF MATERIAL**

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	14.3	-	14.3
Protective Shield	Sq. Yd.	1179	-	1179
Concrete Structures	Cu. Yd.	-	10.6	10.6
Concrete Superstructure	Cu. Yd.	14.3	-	14.3
Bridge Deck Grooving	Sq. Yd.	1263	-	1263
Protective Coat	Sq. Yd.	1395	-	1395
Reinforcement Bars, Epoxy Coated	Pound	3150	1040	4190
Bar Splicers	Each	30	-	30
Preformed Joint Strip Seal	Foot	135	-	135
Concrete Bridge Deck Scarification 2 1/4"	Sq. Yd.	1345	-	1345
Bridge Deck Microsilica Concrete Overlay 2 1/4"	Sq. Yd.	1345	-	1345
Structural Repair of Concrete (Depth Equal to or Less than 5")	Sq. Ft.	-	461	461
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	23	-	23

**GENERAL NOTES**

No field welding is permitted except as specified in the contract documents.

Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Existing Reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.

The protective coat shall be applied to the front and inside faces of the new parapets and to the top face of the new concrete deck sections.



*Michael D. Cima* 12/10/2019  
 Michael D. Cima, Illinois S.E. 081-005984 Date  
 Expires 11/30/2020

**DESIGN STRESSES**

**FIELD UNITS (EXIST. CONSTR.)**

f'c = 3,500 psi  
 fy = 40,000 psi (Reinforcement)  
 fy = 36,000 psi

**FIELD UNITS (NEW CONSTR.)**

f'c = 4,000 psi (Superstructure)  
 f'c = 3,500 psi (Substructure)  
 fy = 60,000 psi (Reinforcement)  
 fy = 36,000 psi (M270 Grade 36)

**INDEX OF SHEETS**

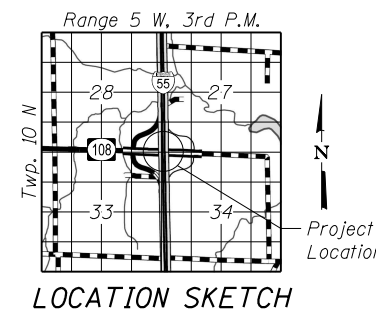
1. General Plan and Elevation
2. Stage Construction Details
3. Deck Overlay Plan
- 4-5. Expansion Joint Details
6. Preformed Joint Strip Seal
7. Abutment Repair Plan
8. Pier Crashwall Extension
9. Bar Splicer Details

**DESIGN SPECIFICATIONS**

2002 AASHTO Standard Specifications for Highway Bridges (Proposed)

**LOADING HS20-44**

Existing and Proposed



**GENERAL PLAN & ELEVATION**

**ILLINOIS ROUTE 108 OVER**

**F.A.I. RTE. 55**

**SECT. (68-1,3 RS-3, 68-2 RS-5) BR**

**MONTGOMERY COUNTY**

**STATION 50+53.09**

**STRUCTURE NO. 068-0043**



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 PLOT DATE = 12/10/2019

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 CHECKED - MDC  
 DRAWN - CFS  
 CHECKED - MDC

REVISED  
 REVISED  
 REVISED  
 REVISED

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN AND ELEVATION  
 STRUCTURE NO. 068-0043**

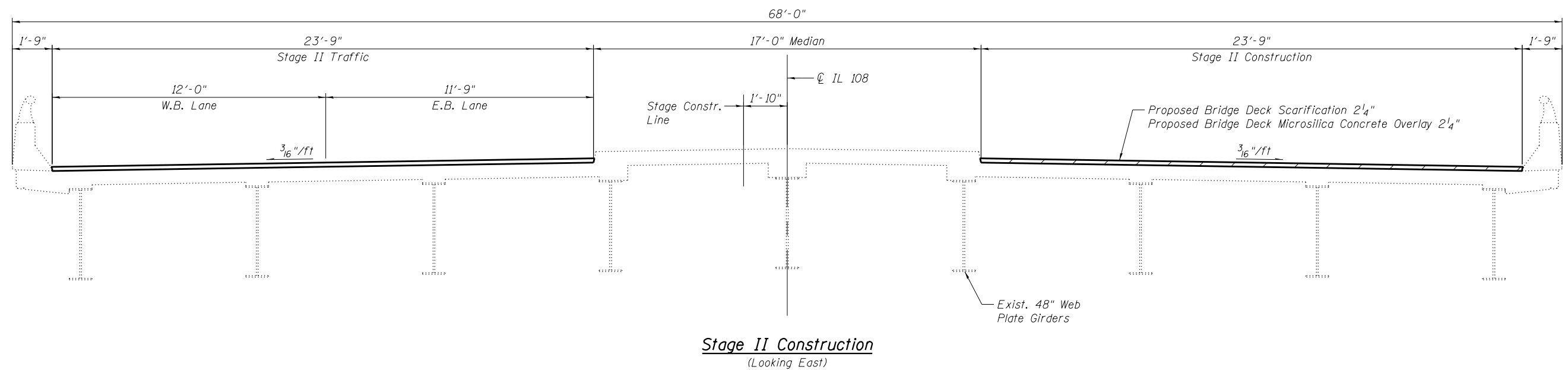
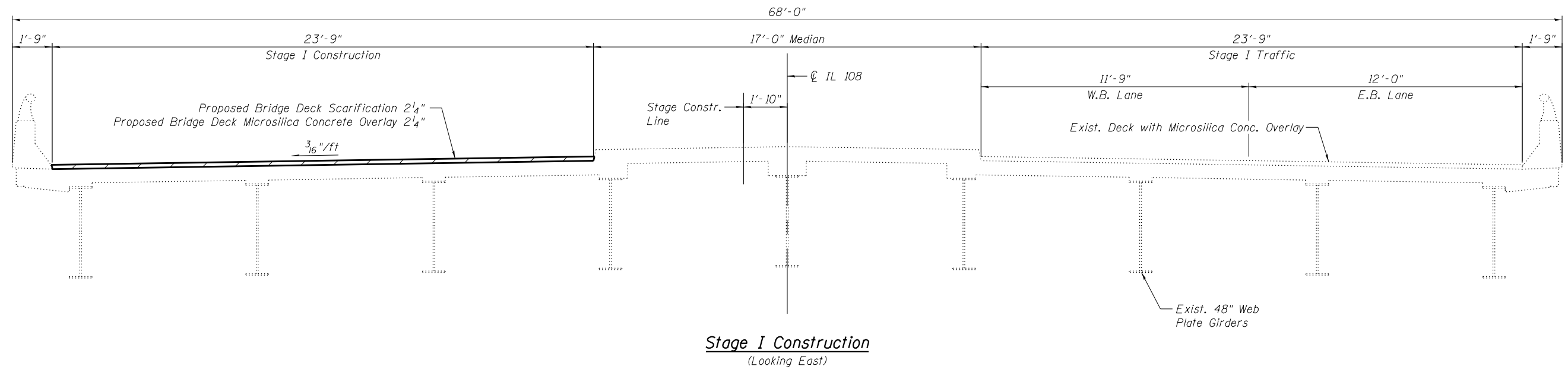
SHEET NO. 1 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			CONTRACT NO. 72031	

ILLINOIS FED. AID PROJECT

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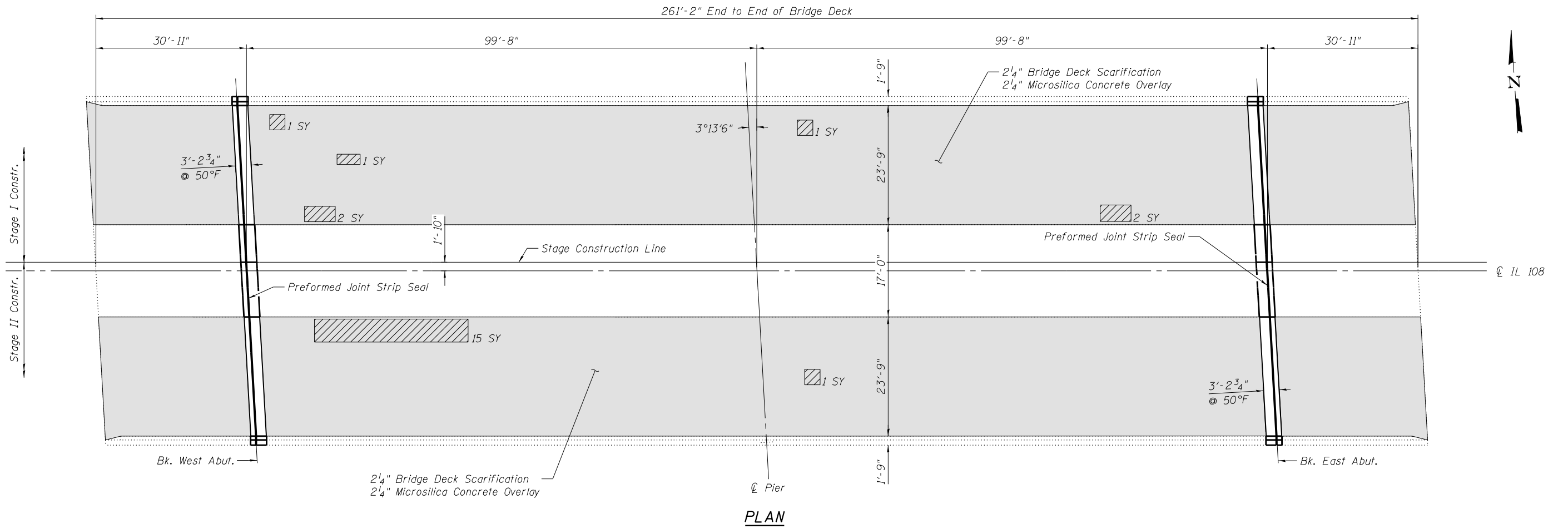
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE CONSTRUCTION DETAILS  
STRUCTURE NO. 068-0043**

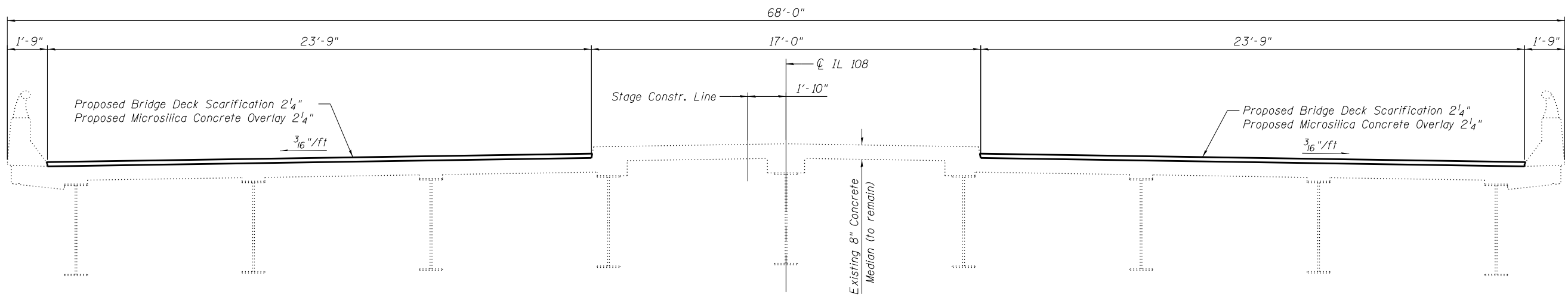
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55	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	147
CONTRACT NO. 72D31				

ILLINOIS FED. AID PROJECT



**PLAN**



**CROSS SECTION**  
(Looking East)

**LEGEND**

Deck Slab Repair (Full Depth, Type II)

**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Bridge Deck Microsilica Concrete Overlay 2 1/4"	Sq. Yd.	1345
Concrete Bridge Deck Scarification 2 1/4"	Sq. Yd.	1345
Bridge Deck Grooving	Sq. Yd.	1263
Protective Coat	Sq. Yd.	1395
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	23

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 CHECKED - MDC  
 DRAWN - CFS  
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 REVISED  
 REVISED  
 REVISED  
 REVISED

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

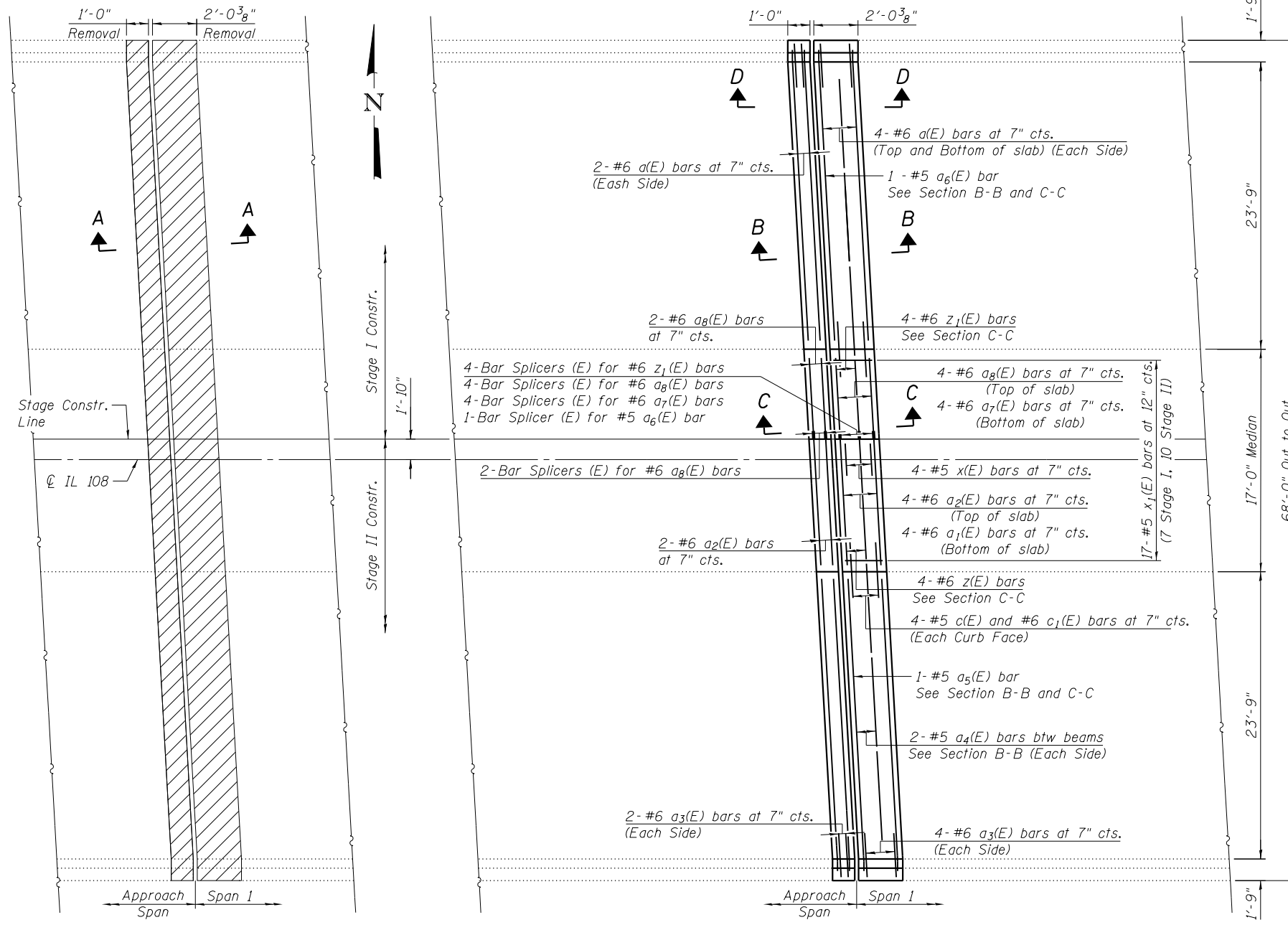
**DECK OVERLAY PLAN**  
**STRUCTURE NO. 068-0043**

SHEET NO. 3 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	

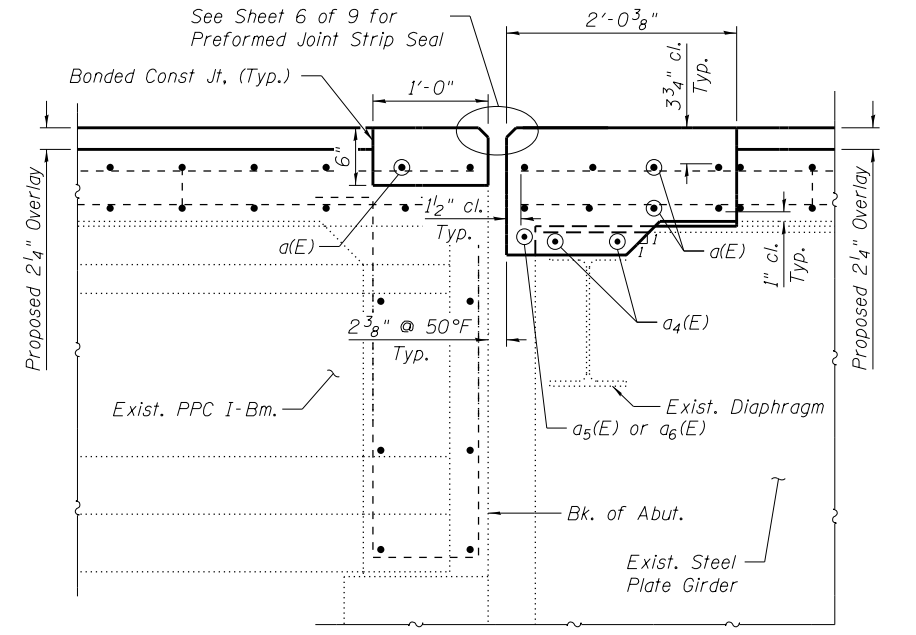


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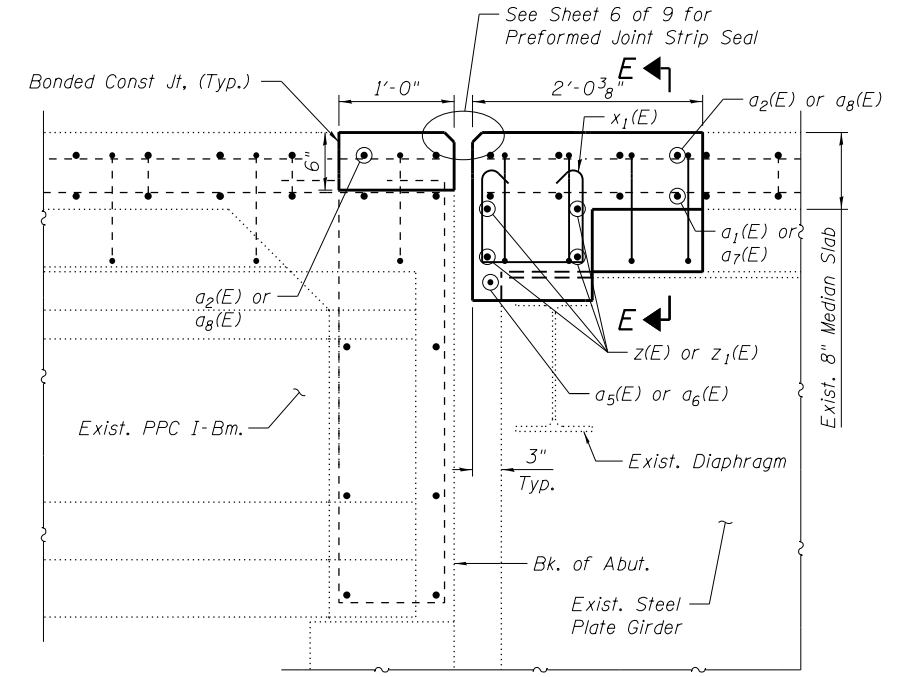


**CONCRETE REMOVAL PLAN**  
 (West Abut. Shown, East Abut. Similar)

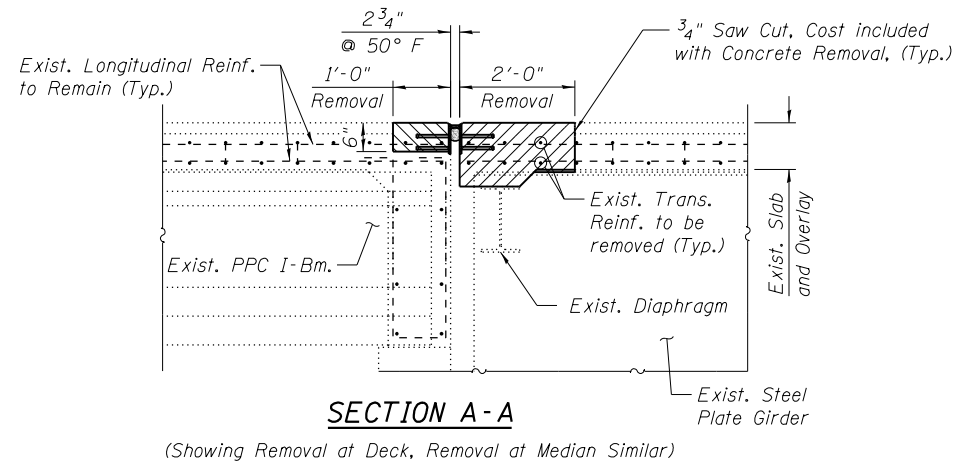
**CONCRETE REPLACEMENT PLAN**  
 (West Abut. Shown, East Abut. Similar)



**SECTION B-B**  
 (Showing Proposed Section at Deck)



**SECTION C-C**  
 (Showing Proposed Section at Median)



**SECTION A-A**

- Notes:
- Existing longitudinal reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.
  - Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.
  - See Sheet 5 of 9 for Bar Details, Bill of Material, Parapet Reinforcement, Section D-D, and Section E-E.



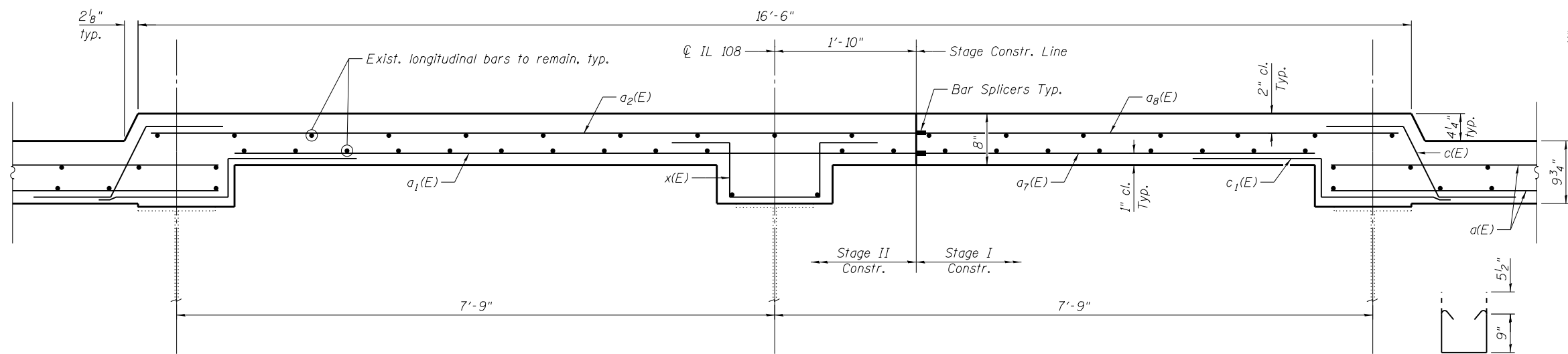
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

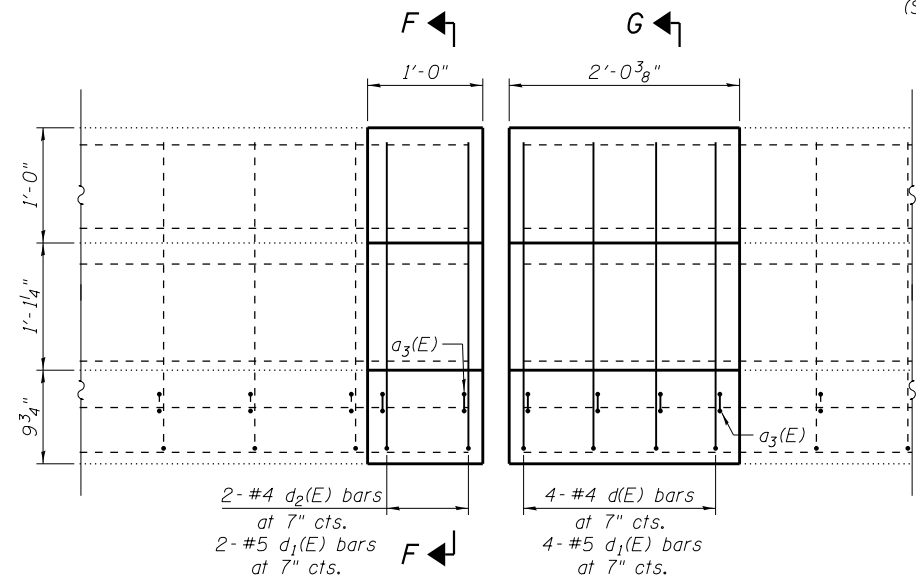
EXPANSION JOINT DETAILS  
 STRUCTURE NO. 068-0043  
 SHEET NO. 4 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1.3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	149
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				

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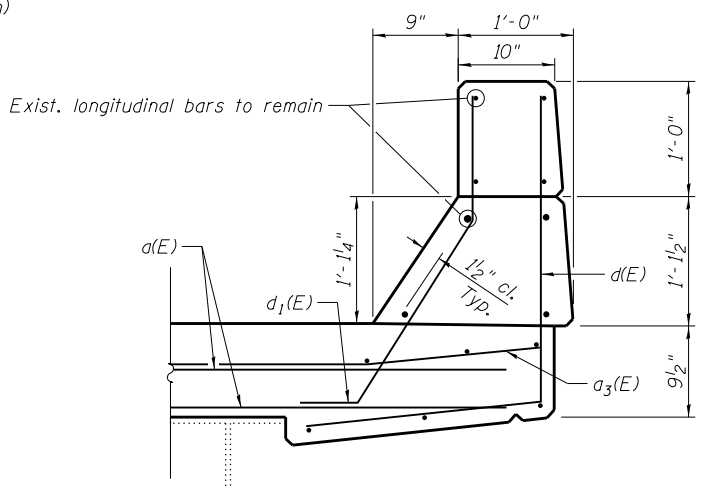
**Section E-E**  
 (Showing Median Section)  
 (Looking West)



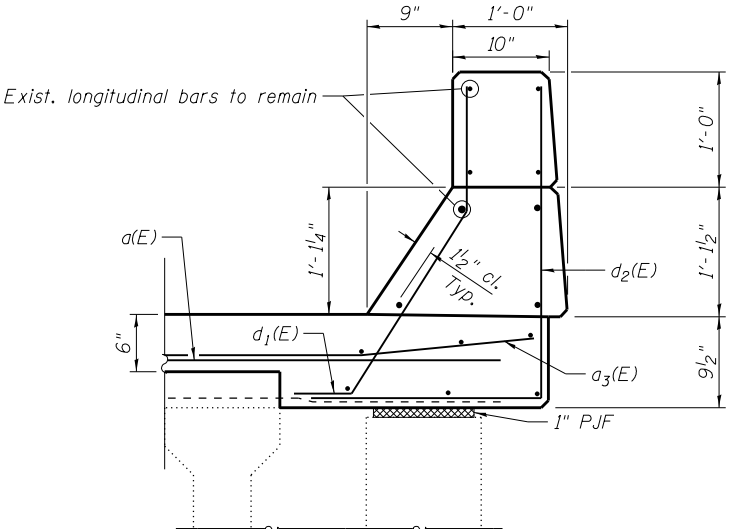
**Section D-D**  
 (Showing Inside Parapet Face)

2- #4 d<sub>2</sub>(E) bars at 7" cts.  
 2- #5 d<sub>1</sub>(E) bars at 7" cts.

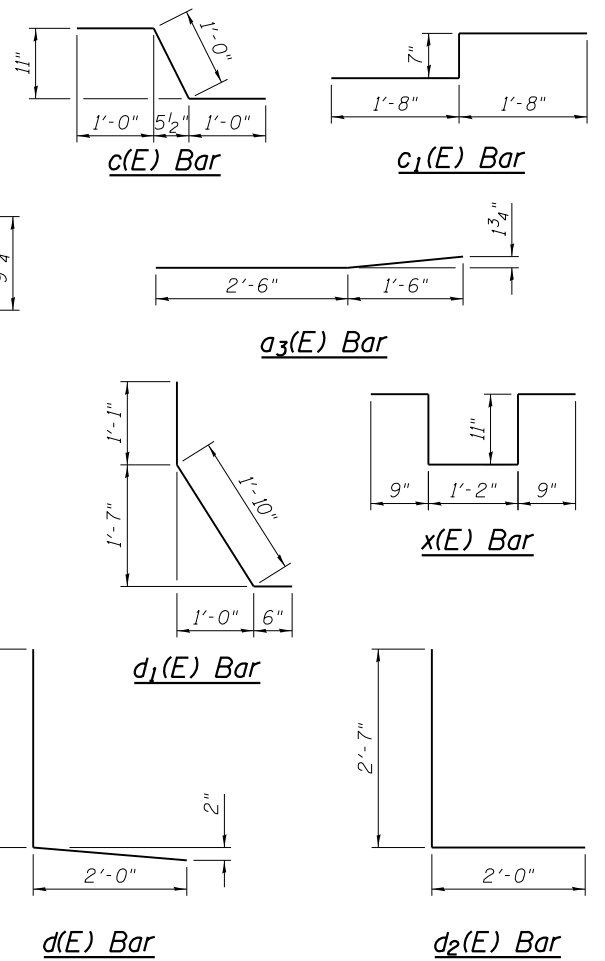
4- #4 d(E) bars at 7" cts.  
 4- #5 d<sub>1</sub>(E) bars at 7" cts.



**Section G-G**  
 (Showing proposed section at parapet)



**Section F-F**  
 (Showing proposed section at parapet)



**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	40	#6	26'-3"	—
a <sub>1</sub> (E)	8	#6	8'-10"	—
a <sub>2</sub> (E)	12	#6	9'-10"	—
a <sub>3</sub> (E)	24	#6	4'-0"	—
a <sub>4</sub> (E)	24	#5	7'-5"	—
a <sub>5</sub> (E)	2	#5	35'-4"	—
a <sub>6</sub> (E)	2	#5	31'-8"	—
a <sub>7</sub> (E)	8	#6	5'-2"	—
a <sub>8</sub> (E)	12	#6	6'-1"	—
c(E)	16	#5	3'-0"	⌋
c <sub>1</sub> (E)	16	#6	3'-11"	⌋
d(E)	16	#4	4'-7"	L
d <sub>1</sub> (E)	24	#5	3'-5"	⌋
d <sub>2</sub> (E)	8	#4	4'-7"	L
x(E)	8	#5	4'-6"	⌋
x <sub>1</sub> (E)	34	#5	3'-2"	⌋
z(E)	8	#6	9'-11"	—
z <sub>1</sub> (E)	8	#6	6'-3"	—
Item		Unit	Total	
Concrete Removal		Cu. Yd.	14.3	
Reinforcement Bars, Epoxy Coated		Pound	3150	
Concrete Superstructure		Cu. Yd.	14.3	
Bar Splicers		Each	30	

- Notes:
- See sheet 4 of 9 for the location of Section D-D and Section E-E.
  - Parapet metal rail shall be removed and reused. Cost included with Concrete Removal.



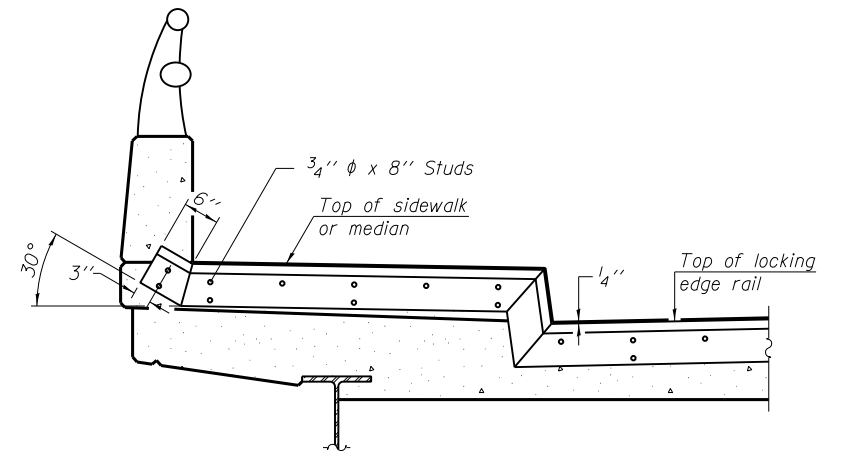
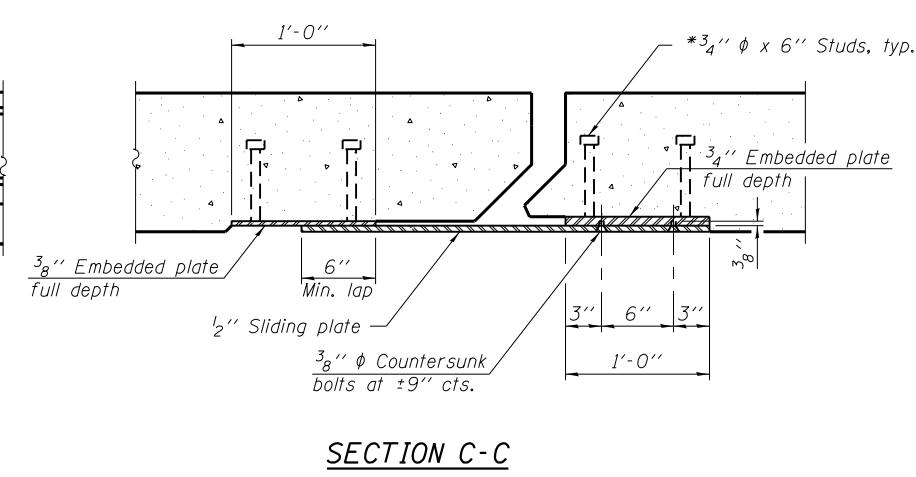
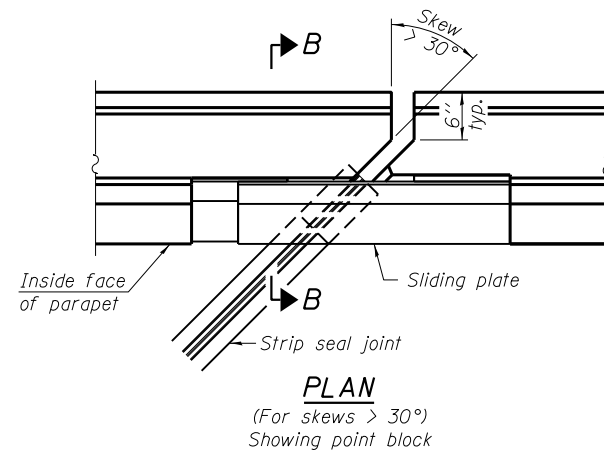
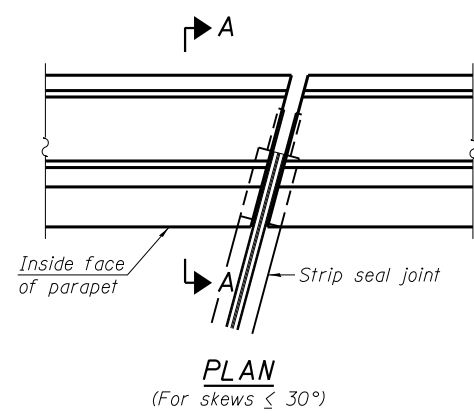
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

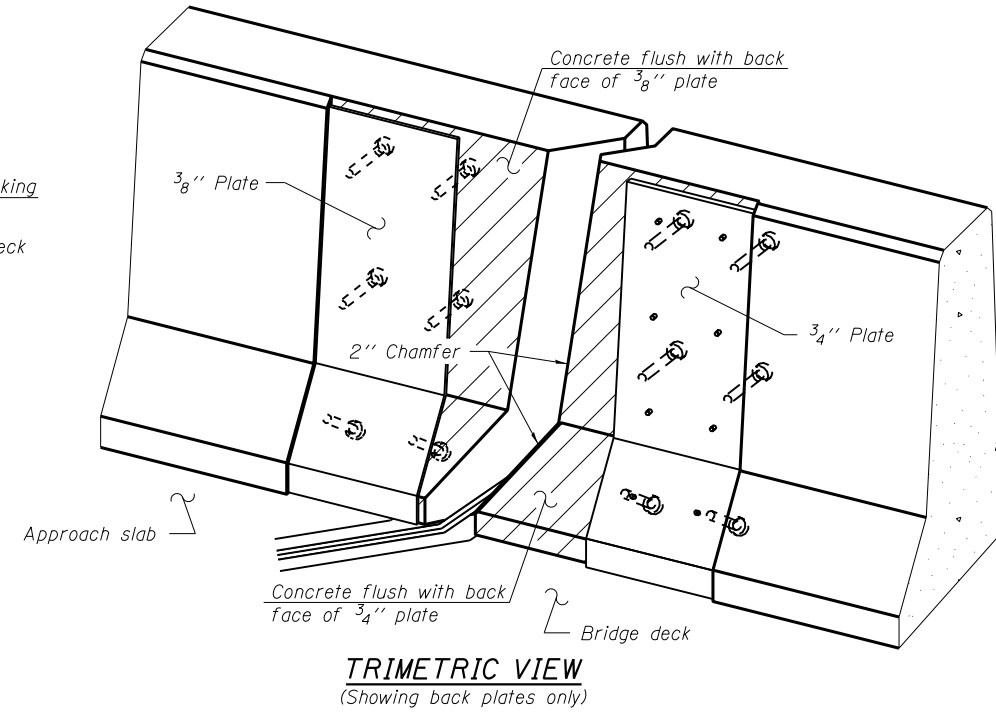
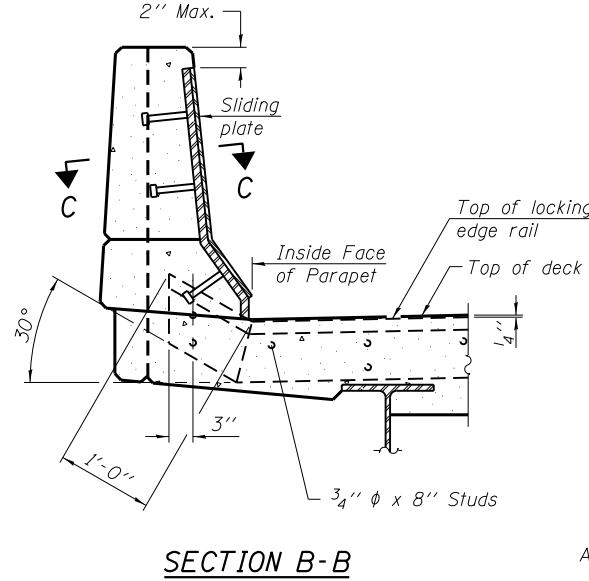
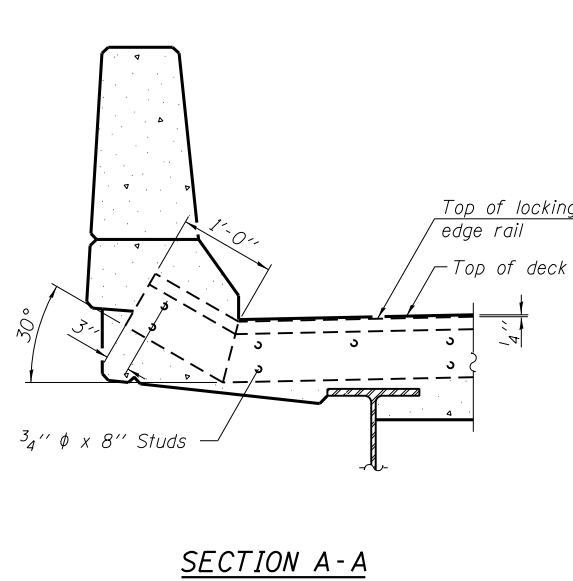
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**STRUCTURE NO. 068-0043**  
 SHEET NO. 5 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	

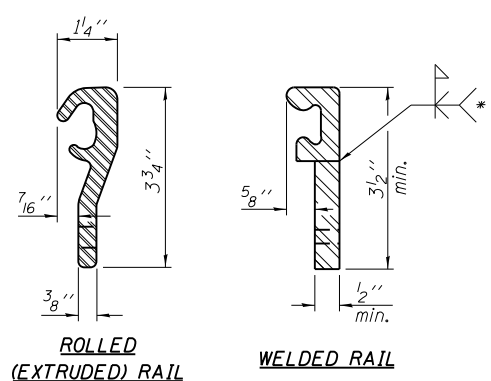
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**TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN**  
 Shorter plates with a single row of studs at 12 inch cts. may be necessary on medians which are shallower than 9 inch. See manufacturer's recommendation.

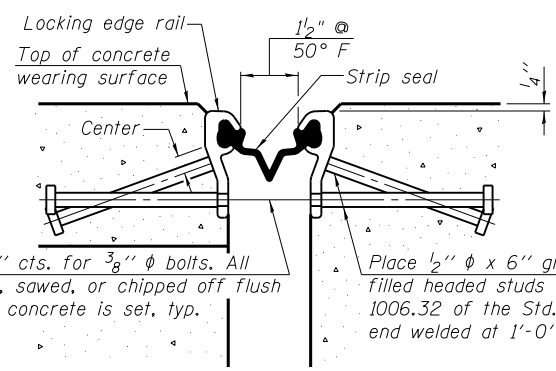


- Notes:
- The strip seal shall be made continuous and shall have a minimum thickness of 1/4 inch. The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
  - The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.
  - The manufacturer's recommended installation methods shall be followed.
  - The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.
  - All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
  - Maximum space between rail segments shall be 13/16 inch, sealed with a suitable sealant. Joints in rails within 10 feet of curbs shall be welded.
  - Parapet plates and anchorage studs for skews > 30 degrees included in the cost of Preformed Joint Strip Seal.



**LOCKING EDGE RAIL SPLICE**

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.



7/16 inch diameter holes at 4'-0 inch cts. for 3/8 inch diameter bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after concrete is set, typ.

Place 1/2 inch diameter x 6 inch granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded at 1'-0 inch alt. cts.

**SECTION THRU STRIP SEAL JOINT**

**BILL OF MATERIAL**

Item	Unit	Total
Preformed Joint Strip Seal	Foot	135



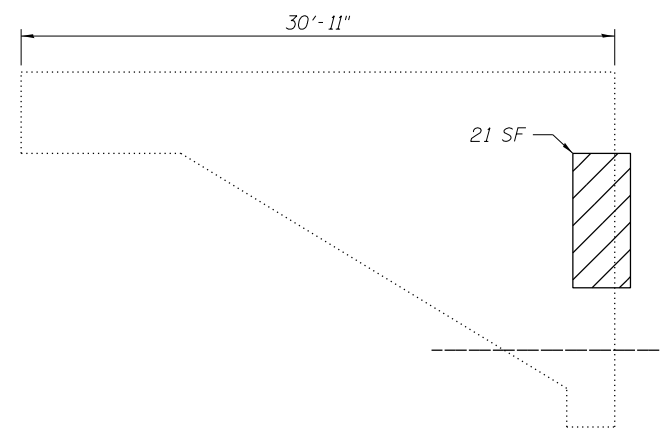
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

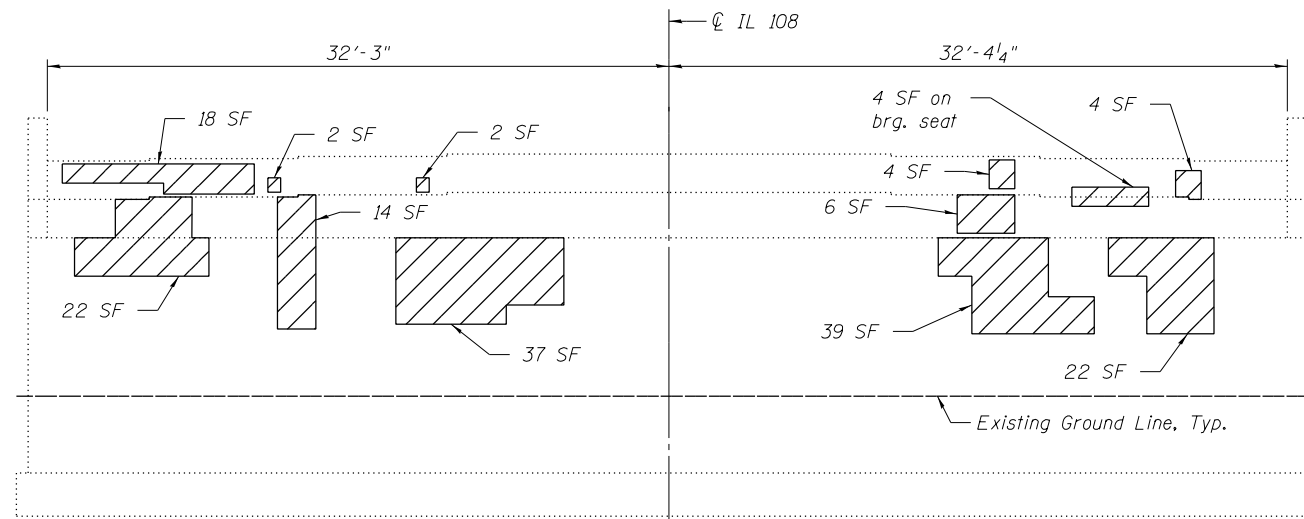
PREFORMED JOINT STRIP SEAL  
 STRUCTURE NO. 068-0043

SHEET NO. 6 OF 9 SHEETS

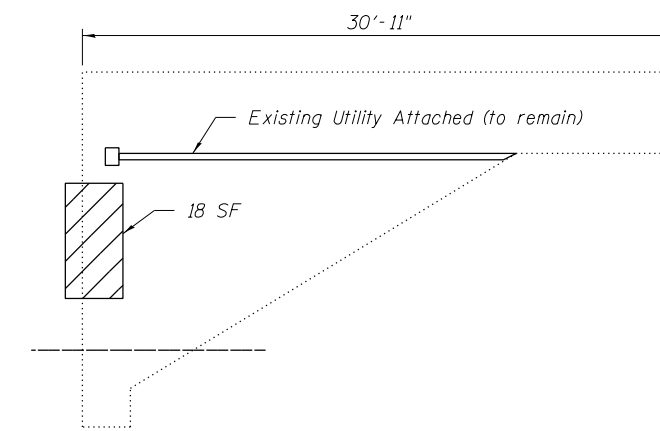
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			CONTRACT NO. 72D31	
ILLINOIS FED. AID PROJECT				



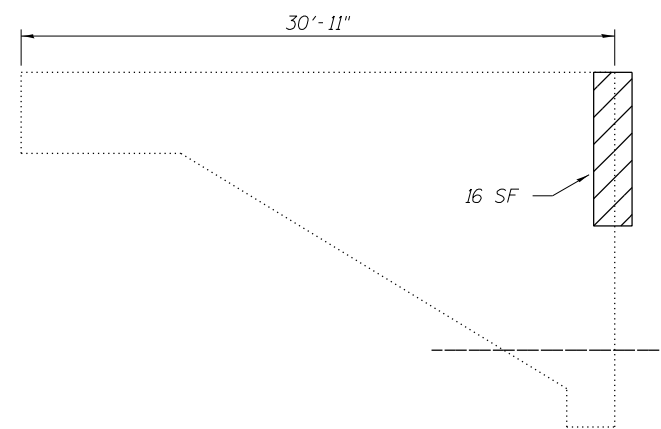
**ELEVATION**  
(South Wingwall, Looking North)



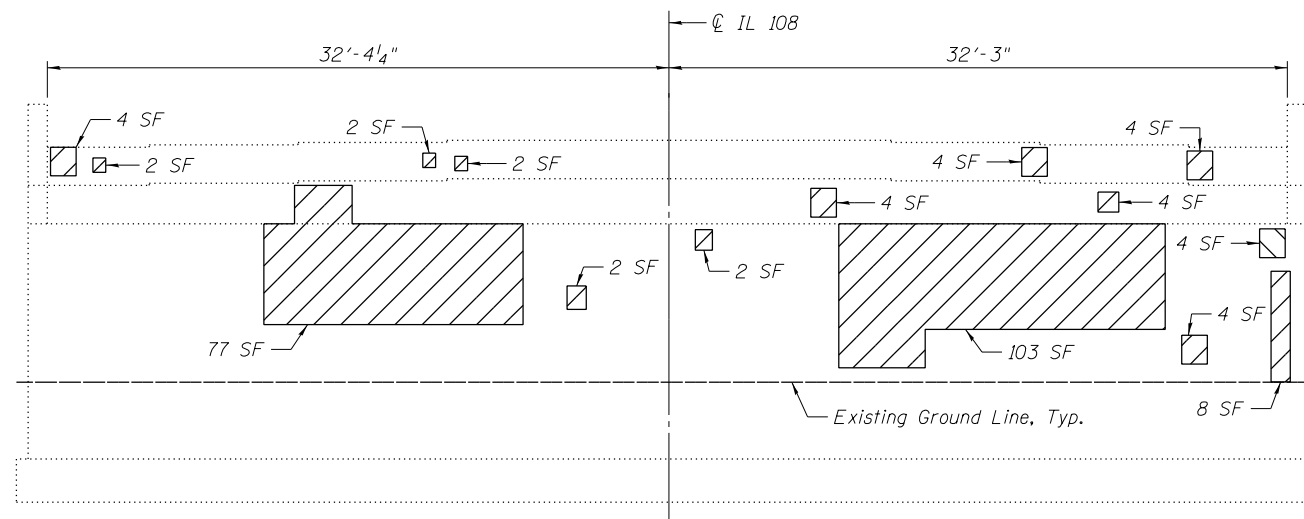
**ELEVATION**  
(West Abut. Looking West)



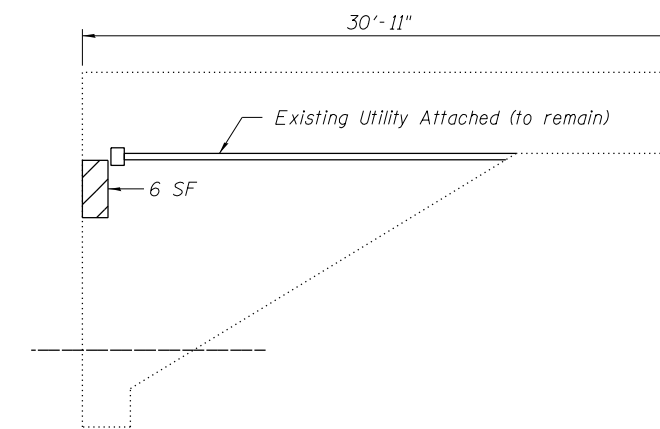
**ELEVATION**  
(North Wingwall, Looking South)



**ELEVATION**  
(North Wingwall, Looking South)



**ELEVATION**  
(East Abut. Looking East)



**ELEVATION**  
(South Wingwall, Looking North)

**LEGEND**

Structural Repair of Concrete  
(Depth Equal to or Less than 5")

**BILL OF MATERIAL**

ITEM	UNIT	Total
Structural Repair of Concrete (Depth Equal to or Less than 5")	Sq. Ft.	461

FILE NAME = \\Quigg\Projects\Projects\2013\JOBS\13-60\BFW\PTB\16\ITEM\_30\06-0043\CADD\CADD Sheets\0680043-72D31-007-Abut.Repair.dgn  
MODEL = Default  
PLOT DRIVER = I001\_PDF.plt



USER NAME = cstokes	DESIGNED - CFS	REVISED
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PLOT SCALE = 0:2.0000 '1' / in.	DRAWN - CFS	REVISED
PLOT DATE = 12/10/2019	CHECKED - MDC	REVISED

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

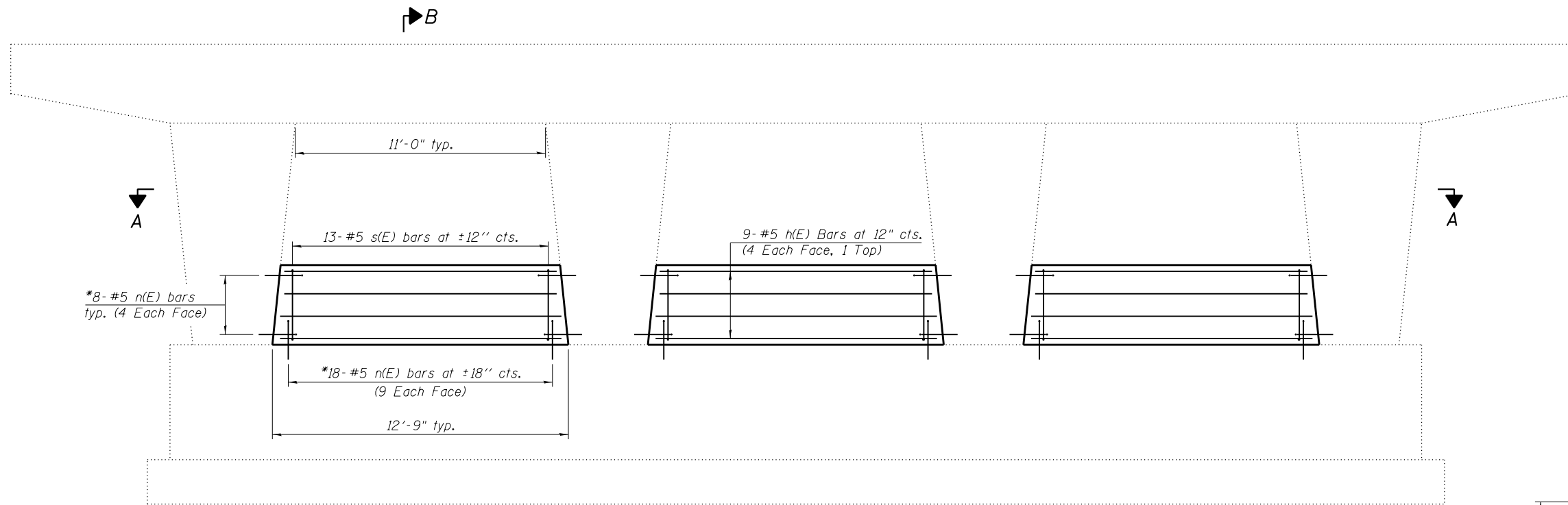
**ABUTMENT REPAIR PLAN  
STRUCTURE NO. 068-0043**

SHEET NO. 7 OF 9 SHEETS

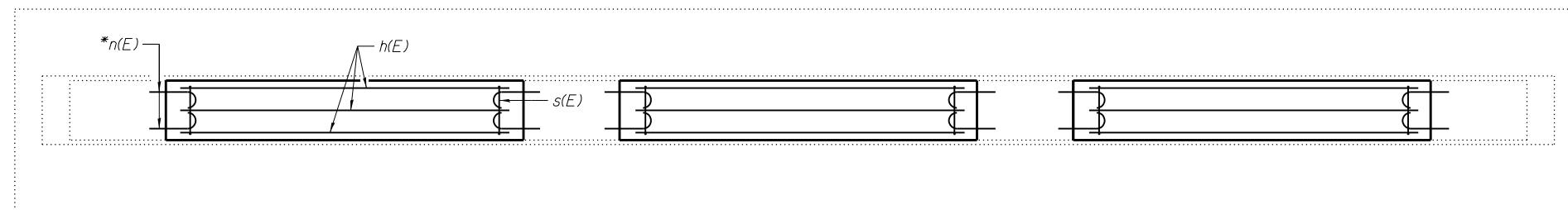
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1.3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	152
CONTRACT NO. 72D31				

ILLINOIS FED. AID PROJECT

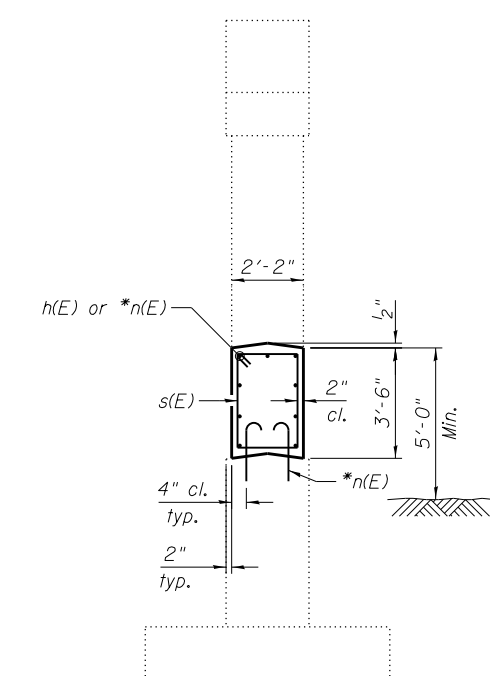
Note:  
 1. The cost of drilling and epoxy grouting threaded rods shall be included with Reinforcement Bars, Epoxy Coated.



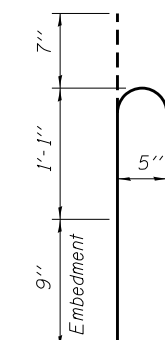
**ELEVATION**  
 (Looking East)



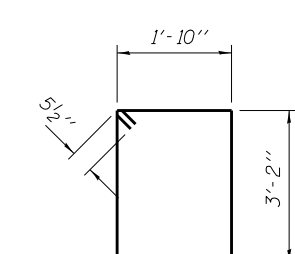
**SECTION A-A**



**SECTION B-B**



**BAR n(E)**



**BAR s(E)**

\* Drill and epoxy grout n(E) bars in 9" min. holes according to Article 584 of the Standard Specifications.

**BILL OF MATERIAL**

BAR	NO.	SIZE	LENGTH	SHAPE
n(E)	27	#5	12'-0"	—
n(E)	102	#5	2'-5"	⌋
s(E)	39	#5	10'-11"	□
ITEM		UNIT	TOTAL	
Concrete Structures		Cu. Yd.	10.6	
Reinforcement Bars, Epoxy Coated		Pound	1040	

FILE NAME = \\Gaugg\Projects\Projects\2013\JOBS\13-60 BFW PTB 169 ITEM 30 DB V-V PH 1-11\WD 4\SN 868-0043\CADD\CADD Sheets\0680043-72D31-008-Pier-CWEExtension.dgn  
 MODEL = Default  
 PLOT DRIVER = 100T\_PDF-plotter



USER NAME = cstokes	DESIGNED - CFS	REVISED
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PLOT SCALE = 0:2.0000 '1' / in.	DRAWN - CFS	REVISED
PLOT DATE = 12/10/2019	CHECKED - MDC	REVISED

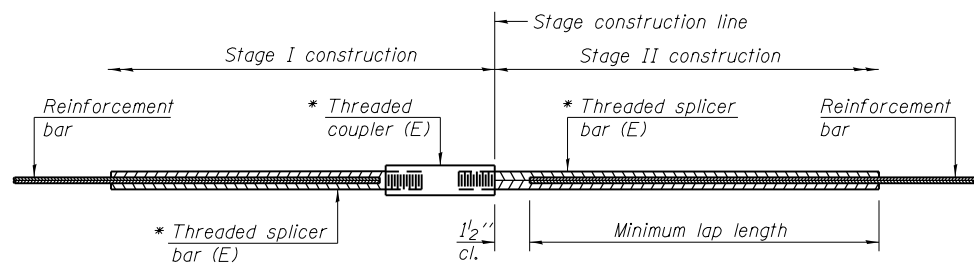
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PIER CRASHWALL EXTENSION**  
**STRUCTURE NO. 068-0043**

SHEET NO. 8 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	153
CONTRACT NO. 72D31				

ILLINOIS FED. AID PROJECT

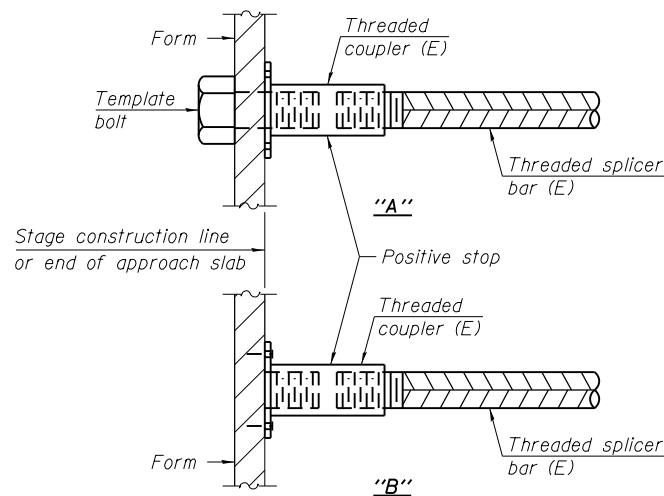


**STANDARD BAR SPLICER ASSEMBLY**

Threaded splicer bar length = min. lap length + 1/2" + thread length

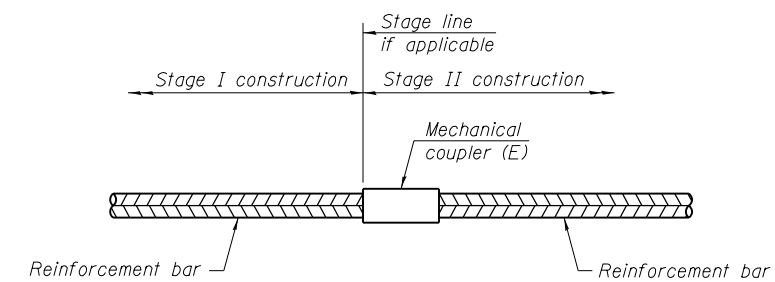
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Median	#6	20	3'-7"
Diaphragm	#6	8	3'-7"
Diaphragm	#5	2	3'-0"



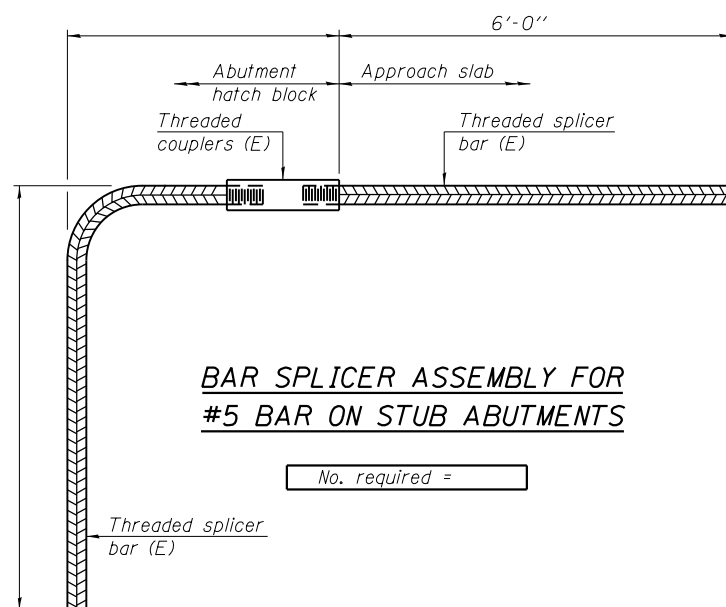
**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

No. required =

Notes:

- Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
- All reinforcement shall be lapped and tied to the splicer bars.
- Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
- See approved list of bar splicer assemblies and mechanical splicers for alternatives.

FILE NAME : \\Quigg\Projects\Projects\2013\JOBS\13-60 BFW PTB 16\ITEM 30 DB V-V PH 1-11\WD 4\SN 068-0043\CADD\CADD Sheets\0680043-72D31-009-Bar Splicer Details.dgn  
 MODEL : Default  
 PLOT DRIVER : IODT\_PDF.plt

BSD-1

6-8-15



USER NAME : cstokes  
 0680043-72D31-009-Bar Splicer Details.dgn  
 PLOT SCALE : 0:2.0000 '1' / in.  
 PLOT DATE : 12/10/2019

DESIGNED - CFS  
 CHECKED - MDC  
 DRAWN - CFS  
 CHECKED - MDC  
 REVISED  
 REVISED  
 REVISED  
 REVISED

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER AND MECHANICAL SPLICER DETAILS  
 STRUCTURE NO. 068-0043

SHEET NO. 9 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1.3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	154
CONTRACT NO. 72D31				

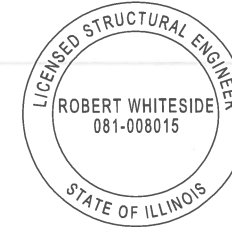
ILLINOIS FED. AID PROJECT

Bench Mark: B.M. #84 Chiseled square on center opening of pier crashwall at Sta. 1812+80.8, Elev. 658.35

Existing Structure: S.N. 068-0044 built in 1972 as FAI Route 55, section 68-2HB at station 1812+60.08 (FAI-55).  
Structure consists of 2 span composite steel plate girder supported on vaulted abutments and a 2 column pier.  
It is 259'-0" bk.-bk. approach bents and 32'-0" out-out deck. Bridge to be rehabilitated as shown using road closure with detoured traffic.

No Salvage

**APPROVED**  
FOR STRUCTURAL ADEQUACY ONLY  
*Robert Whiteside*  
ENGINEER OF BRIDGES AND STRUCTURES

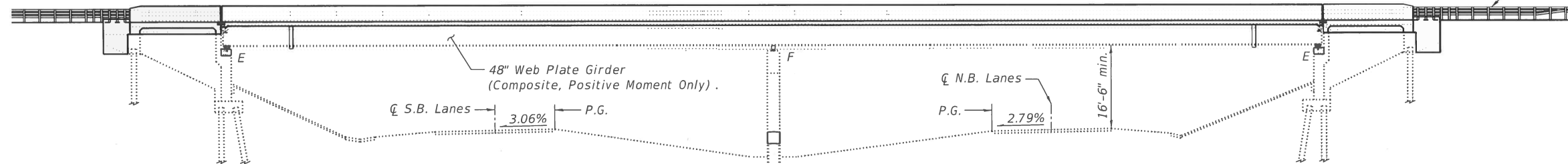


*Robert Whiteside* 12/10/2019  
Robert Whiteside, Illinois S.E. 081-008015 Date  
Expires 11/30/2020

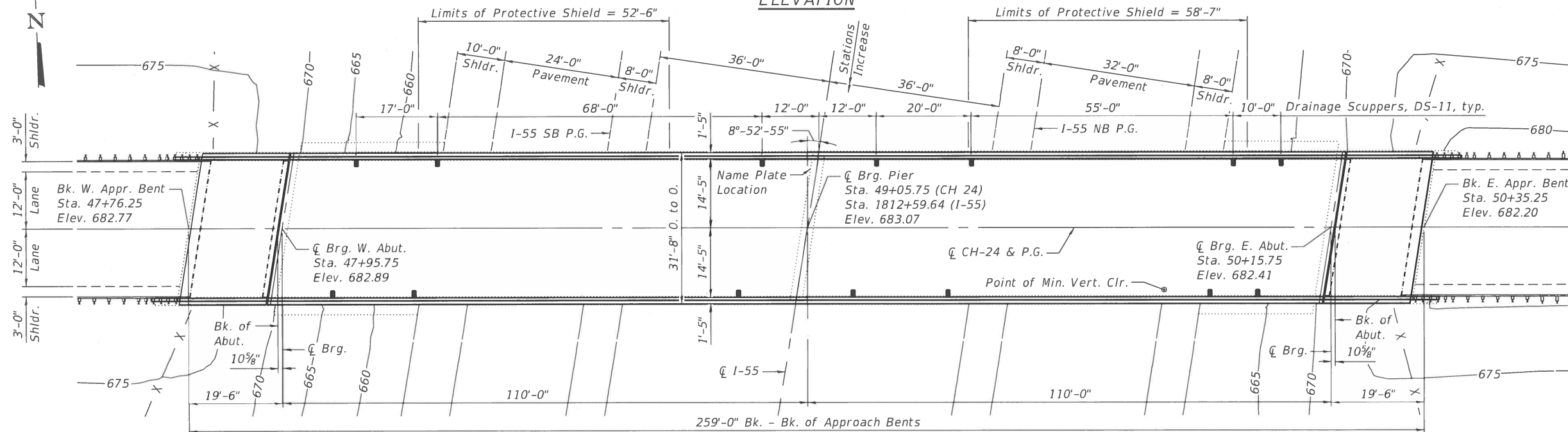
**SCOPE OF WORK**

1. Remove and replace existing concrete deck
2. Raise superstructure to achieve 16'-6" min. vertical clearance over I-55.
3. Replace existing bearings with new fixed and elastomeric bearings.
4. Raise pier crashwall height to 5'-0" min.
5. Modify the substructure to account for the grade raise.
6. Repair the abutments.

Traffic Barrier Terminal Type 6 (Std. 631031), typ.



**ELEVATION**



**PLAN**

**DESIGN STRESSES**

**FIELD UNITS (New Construction)**

$f'_c = 3,500$  psi  
 $f'_c = 4,000$  psi (superstructure concrete)  
 $f_y = 60,000$  psi (reinforcement)

**FIELD UNITS (Exist. Construction)**

$f'_c = 3,500$  psi  
 $f_y = 40,000$  psi (reinforcement)  
 $f_y = 36,000$  psi (structural steel)

**LOADING HS20-44 (New Constr.)**

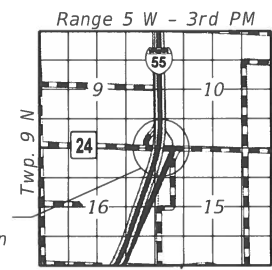
Allow 25#/sq. ft. for future wearing surface.

**DESIGN SPECIFICATIONS (New Constr.)**

2002 AASHTO Standard Specifications

**SEISMIC DATA**

Seismic Performance Category (SPC) = A  
Horizontal Bedrock Acceleration Coefficient (A) = 0.065g  
Site Coefficient (S) = 1.0



**LOCATION SKETCH**

**GENERAL PLAN & ELEVATION**  
**CH 24 OVER I-55**  
**F.A.S. RTE. 1768**  
**SECT. (68-1,3 RS-3, 68-2 RS-5) BR**  
**MONTGOMERY COUNTY**  
**STATION 49+05.75 (CH-24)**  
**STRUCTURE NO. 068-0044**

MODEL: Default  
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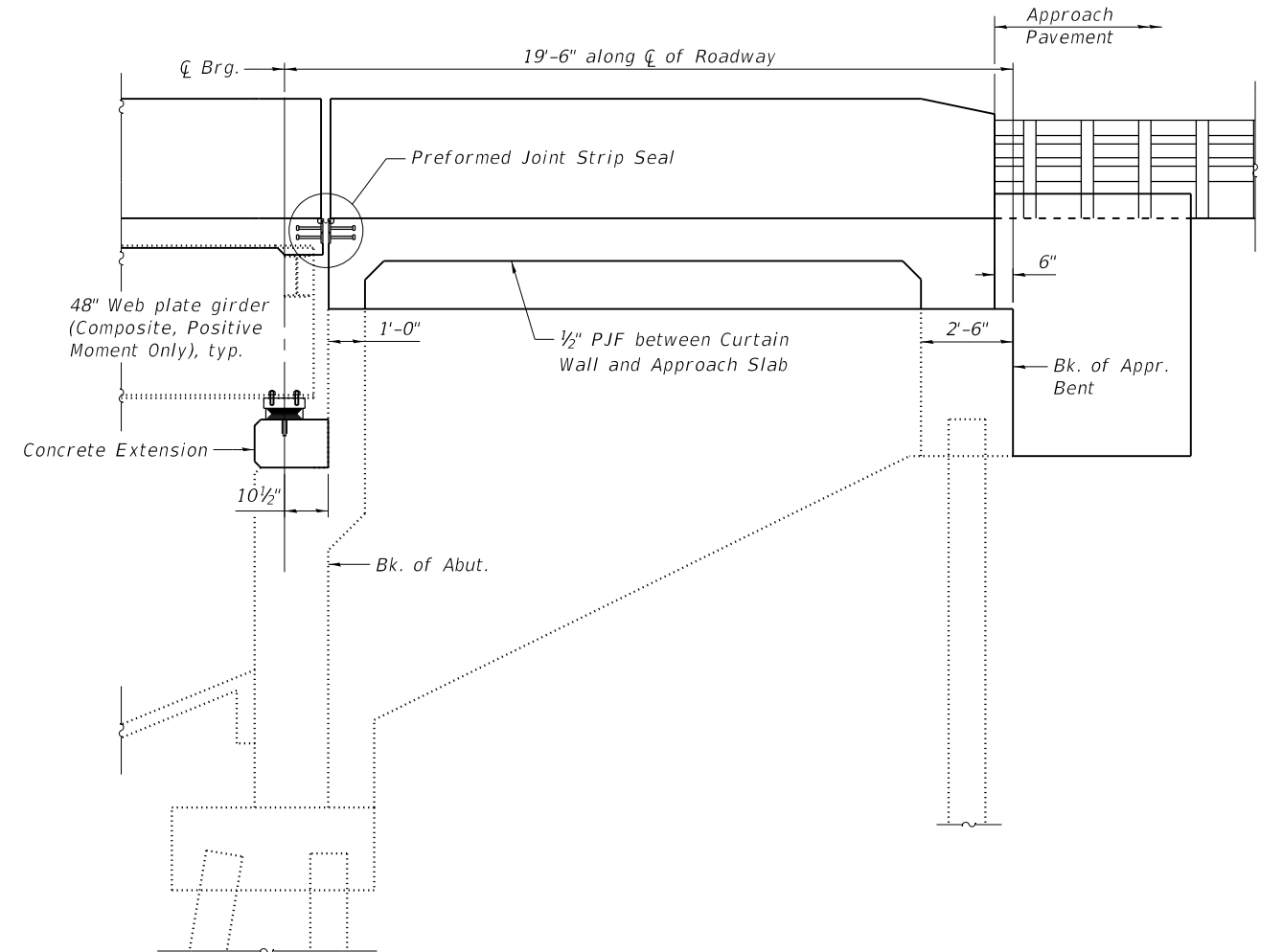
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN AND ELEVATION**  
**STRUCTURE NO. 068-0044**  
SHEET 1 OF 21 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1768	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	155
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				

**GENERAL NOTES**

Fasteners shall be ASTM A325 Type 1, mechanically galvanized bolts. Bolts 3/4" Ø, holes 7/8" Ø, unless otherwise noted.  
 No field welding is permitted except as specified in the contract documents.  
 Reinforcement bars designated (E) shall be epoxy coated.  
 Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.  
 As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer. Any cracks that can not be removed by grinding 1/4" in. deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.  
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.  
 Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of 1/8 inch (0.01 ft.). Adjustment shall be made either by grinding the surface or by shimming the bearings.  
 The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.  
 The Inorganic Zinc Rich Primer / Acrylic / Acrylic Paint System shall be used for shop and field painting of new structural steel except where otherwise noted. The color of the final finish coat for all interior steel surfaces shall be gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Interstate Green, Munsell No. 7.5G 4/8.  
 Concrete Sealer shall be applied to the backwall and bearing seats of the abutments.



**SECTION THRU VAULTED ABUTMENT**

(Horizontal dimensions are shown at right angles, unless otherwise noted)

**BILL OF MATERIAL**

Item	Unit	Super	Sub	Total
Concrete Removal	Cu. Yd.	-	7.4	7.4
Removal of Existing Concrete Deck	Each	1	-	1
Protective Shield	Sq. Yd.	441	-	441
Structure Excavation	Cu. Yd.	-	24	24
Concrete Structures	Cu. Yd.	-	14.4	14.4
Concrete Superstructure	Cu. Yd.	294.1	-	294.1
Bridge Deck Grooving	Sq. Yd.	767	-	767
Protective Coat	Sq. Yd.	1,062	-	1,062
Furnishing and Erecting Structural Steel	Pound	2,550	-	2,550
Stud Shear Connectors	Each	90	-	90
Reinforcement Bars, Epoxy Coated	Pound	77,640	2,840	80,480
Name Plates	Each	1	-	1
Preformed Joint Strip Seal	Foot	62.5	-	62.5
Elastomeric Bearing Assembly, Type I	Each	10	-	10
Anchor Bolts, 5/8"	Each	40	-	40
Anchor Bolts, 1 1/4"	Each	10	-	10
Sand Backfill	Cu. Yd.	-	41	41
Concrete Sealer	Sq. Ft.	-	609	609
Structural Repair of Concrete (≤ 5 inches)	Sq. Ft.	-	54	54
Drainage Scuppers, DS-11	Each	14	-	14
Jacking Existing Superstructure	L. Sum	1	-	1

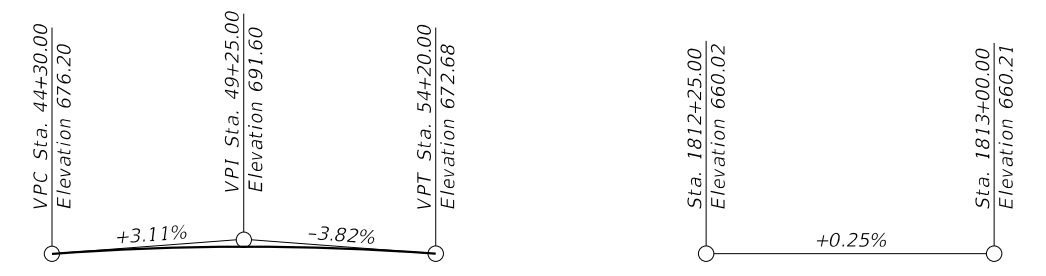
**INDEX OF SHEETS**

- 1 General Plan and Elevation
- 2 General Details
- 3 - 4 Top of Slab Elevations
- 5 Top of West Approach Slab Elevations
- 6 Top of East Approach Slab Elevations
- 7 Superstructure
- 8 Superstructure Details
- 9 Diaphragm Details
- 10 Vaulted Abutment Approach Span
- 11 Concrete Parapet Slipforming Option
- 12 Drainage Scupper, DS-11
- 13 Preformed Joint Strip Seal
- 14 Framing Plan
- 15 Structural Steel Details
- 16 Bearing Details
- 17 West Abutment
- 18 East Abutment
- 19 Abutment Repairs
- 20 Abutment Details
- 21 Pier Crashwall Extension

STATION 49+05.75  
 REBUILT BY  
 STATE OF ILLINOIS  
 F.A.S. RT. 1768 SEC. (68-1,3)RS-3,(68-2)RS-5  
 LOADING HS20-44  
 STRUCTURE NO. 068-0044

**NAME PLATE**  
 See Std. 515001

Place the Proposed Name Plate next to the Existing Name Plate. Cost included with Name Plates.



**PROFILE GRADE CH-24**

(along centerline roadway)

**PROFILE GRADE I-55**

(along NB & SB P.G.)

MODEL: Default  
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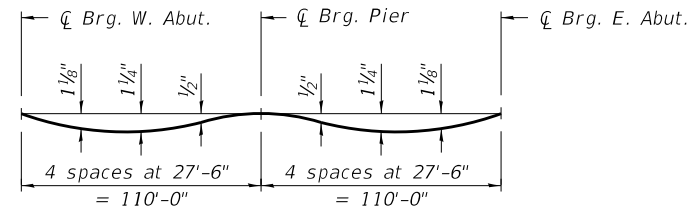
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**GENERAL DETAILS**  
**STRUCTURE NO. 068-0044**

SHEET 2 OF 21 SHEETS

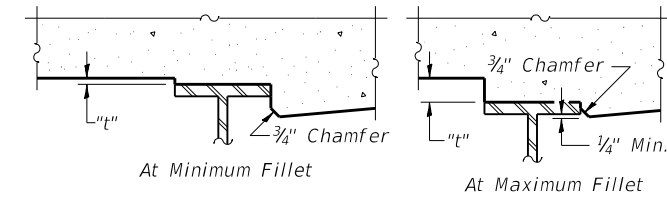
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1768	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	156
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				





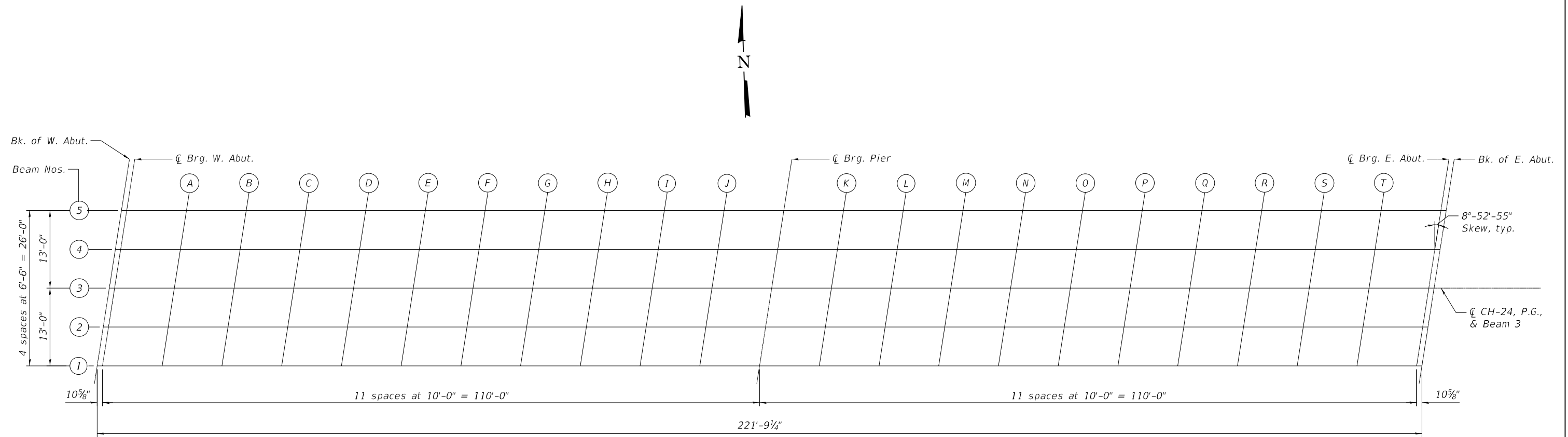
**DEAD LOAD DEFLECTION DIAGRAM**  
(Includes weight of concrete only.)

Note:  
The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on Sheet 4 of 21.



To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on Sheet 4 of 21, minus slab thickness, equals the fillet heights "t" above top flange of beams.

**FILLET HEIGHTS**



**PLAN**

MODEL: Default  
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PLOT DATE = 12/10/2019	CHECKED - KWB	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS  
STRUCTURE NO. 068-0044

SHEET 3 OF 21 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1768	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	157
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				

BEAM 5

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of W. Abut.	47+96.90	-13.00	682.70	682.70
☉ Brg. W. Abut.	47+97.78	-13.00	682.70	682.70
A	48+07.78	-13.00	682.75	682.79
B	48+17.78	-13.00	682.80	682.87
C	48+27.78	-13.00	682.83	682.93
D	48+37.78	-13.00	682.86	682.97
E	48+47.78	-13.00	682.88	682.99
F	48+57.78	-13.00	682.90	683.00
G	48+67.78	-13.00	682.91	682.98
H	48+77.78	-13.00	682.91	682.96
I	48+87.78	-13.00	682.90	682.93
J	48+97.78	-13.00	682.89	682.90
☉ Brg. Pier	49+07.78	-13.00	682.87	682.87
K	49+17.78	-13.00	682.84	682.85
L	49+27.78	-13.00	682.81	682.83
M	49+37.78	-13.00	682.77	682.82
N	49+47.78	-13.00	682.72	682.80
O	49+57.78	-13.00	682.66	682.76
P	49+67.78	-13.00	682.60	682.71
Q	49+77.78	-13.00	682.53	682.65
R	49+87.78	-13.00	682.46	682.56
S	49+97.78	-13.00	682.37	682.45
T	50+07.78	-13.00	682.28	682.33
☉ Brg. E. Abut.	50+17.78	-13.00	682.19	682.19
Bk. of E. Abut.	50+18.67	-13.00	682.18	682.18

BEAM 4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of W. Abut.	47+95.88	-6.50	682.80	682.80
☉ Brg. W. Abut.	47+96.77	-6.50	682.80	682.80
A	48+06.77	-6.50	682.85	682.89
B	48+16.77	-6.50	682.90	682.97
C	48+26.77	-6.50	682.93	683.03
D	48+36.77	-6.50	682.96	683.07
E	48+46.77	-6.50	682.98	683.10
F	48+56.77	-6.50	683.00	683.10
G	48+66.77	-6.50	683.01	683.09
H	48+76.77	-6.50	683.01	683.06
I	48+86.77	-6.50	683.01	683.03
J	48+96.77	-6.50	682.99	683.00
☉ Brg. Pier	49+06.77	-6.50	682.97	682.97
K	49+16.77	-6.50	682.95	682.95
L	49+26.77	-6.50	682.91	682.94
M	49+36.77	-6.50	682.87	682.92
N	49+46.77	-6.50	682.83	682.90
O	49+56.77	-6.50	682.77	682.87
P	49+66.77	-6.50	682.71	682.82
Q	49+76.77	-6.50	682.64	682.76
R	49+86.77	-6.50	682.57	682.67
S	49+96.77	-6.50	682.49	682.56
T	50+06.77	-6.50	682.40	682.44
☉ Brg. E. Abut.	50+16.77	-6.50	682.30	682.30
Bk. of E. Abut.	50+17.65	-6.50	682.29	682.29

☉ CH-24, P.G., & BEAM 3

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of W. Abut.	47+94.86	0.00	682.89	682.89
☉ Brg. W. Abut.	47+95.75	0.00	682.89	682.89
A	48+05.75	0.00	682.94	682.98
B	48+15.75	0.00	682.99	683.06
C	48+25.75	0.00	683.03	683.13
D	48+35.75	0.00	683.06	683.17
E	48+45.75	0.00	683.08	683.19
F	48+55.75	0.00	683.10	683.19
G	48+65.75	0.00	683.11	683.18
H	48+75.75	0.00	683.11	683.16
I	48+85.75	0.00	683.10	683.13
J	48+95.75	0.00	683.09	683.10
☉ Brg. Pier	49+05.75	0.00	683.07	683.07
K	49+15.75	0.00	683.05	683.05
L	49+25.75	0.00	683.02	683.04
M	49+35.75	0.00	682.98	683.03
N	49+45.75	0.00	682.93	683.01
O	49+55.75	0.00	682.88	682.97
P	49+65.75	0.00	682.82	682.93
Q	49+75.75	0.00	682.75	682.86
R	49+85.75	0.00	682.67	682.77
S	49+95.75	0.00	682.59	682.67
T	50+05.75	0.00	682.50	682.54
☉ Brg. E. Abut.	50+15.75	0.00	682.41	682.41
Bk. of E. Abut.	50+16.64	0.00	682.40	682.40

BEAM 2

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of W. Abut.	47+93.85	6.50	682.78	682.78
☉ Brg. W. Abut.	47+94.73	6.50	682.79	682.79
A	48+04.73	6.50	682.84	682.88
B	48+14.73	6.50	682.89	682.96
C	48+24.73	6.50	682.93	683.02
D	48+34.73	6.50	682.96	683.07
E	48+44.73	6.50	682.98	683.09
F	48+54.73	6.50	683.00	683.10
G	48+64.73	6.50	683.01	683.08
H	48+74.73	6.50	683.01	683.06
I	48+84.73	6.50	683.01	683.03
J	48+94.73	6.50	683.00	683.00
☉ Brg. Pier	49+04.73	6.50	682.98	682.98
K	49+14.73	6.50	682.95	682.96
L	49+24.73	6.50	682.92	682.95
M	49+34.73	6.50	682.88	682.93
N	49+44.73	6.50	682.84	682.91
O	49+54.73	6.50	682.78	682.88
P	49+64.73	6.50	682.72	682.84
Q	49+74.73	6.50	682.66	682.77
R	49+84.73	6.50	682.58	682.68
S	49+94.73	6.50	682.50	682.58
T	50+04.73	6.50	682.42	682.46
☉ Brg. E. Abut.	50+14.73	6.50	682.32	682.32
Bk. of E. Abut.	50+15.62	6.50	682.31	682.31

BEAM 1

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of W. Abut.	47+92.83	13.00	682.68	682.68
☉ Brg. W. Abut.	47+93.72	13.00	682.68	682.68
A	48+03.72	13.00	682.73	682.77
B	48+13.72	13.00	682.78	682.86
C	48+23.72	13.00	682.82	682.92
D	48+33.72	13.00	682.85	682.96
E	48+43.72	13.00	682.88	682.99
F	48+53.72	13.00	682.89	682.99
G	48+63.72	13.00	682.90	682.98
H	48+73.72	13.00	682.91	682.96
I	48+83.72	13.00	682.91	682.93
J	48+93.72	13.00	682.90	682.90
☉ Brg. Pier	49+03.72	13.00	682.88	682.88
K	49+13.72	13.00	682.85	682.86
L	49+23.72	13.00	682.82	682.85
M	49+33.72	13.00	682.79	682.83
N	49+43.72	13.00	682.74	682.82
O	49+53.72	13.00	682.69	682.79
P	49+63.72	13.00	682.63	682.74
Q	49+73.72	13.00	682.56	682.67
R	49+83.72	13.00	682.49	682.59
S	49+93.72	13.00	682.41	682.48
T	50+03.72	13.00	682.32	682.36
☉ Brg. E. Abut.	50+13.72	13.00	682.23	682.23
Bk. of E. Abut.	50+14.60	13.00	682.22	682.22

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS  
STRUCTURE NO. 068-0044

SHEET 4 OF 21 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1768	(68-1.3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	158
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				

NORTH EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Appr. Slab	47+79.01	-14.42	682.56
A1	47+89.01	-14.42	682.63
E. End of W. Appr. Slab	47+97.12	-14.42	682.67

NORTH EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Appr. Slab	47+78.63	-12.00	682.61
A1	47+88.63	-12.00	682.67
E. End of W. Appr. Slab	47+96.74	-12.00	682.72

CL CH-24 & P.G.

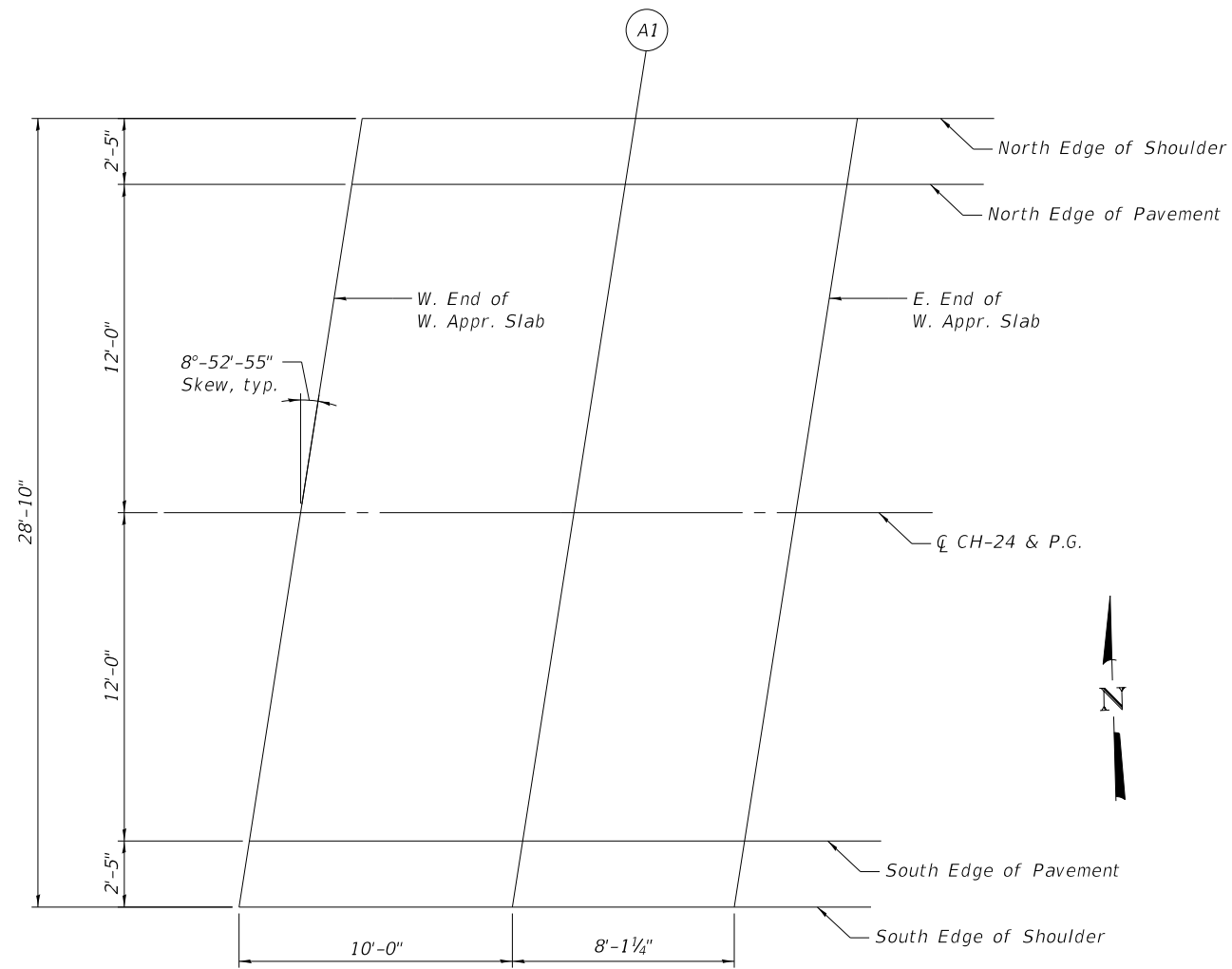
Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Appr. Slab	47+76.76	0.00	682.78
A1	47+86.76	0.00	682.84
E. End of W. Appr. Slab	47+94.86	0.00	682.89

SOUTH EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Appr. Slab	47+74.88	12.00	682.58
A1	47+84.88	12.00	682.65
E. End of W. Appr. Slab	47+92.99	12.00	682.70

SOUTH EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Appr. Slab	47+74.50	14.42	682.53
A1	47+84.50	14.42	682.60
E. End of W. Appr. Slab	47+92.61	14.42	682.65



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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF WEST APPROACH SLAB ELEVATIONS  
STRUCTURE NO. 068-0044

SHEET 5 OF 21 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1768	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	159
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				

NORTH EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
W. End of E. Appr. Slab	50+18.89	-14.42	682.15
A2	50+28.89	-14.42	682.04
E. End of E. Appr. Slab	50+37.00	-14.42	681.95

NORTH EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
W. End of E. Appr. Slab	50+18.51	-12.00	682.20
A2	50+28.51	-12.00	682.10
E. End of E. Appr. Slab	50+36.62	-12.00	682.01

CL CH-24 & P.G.

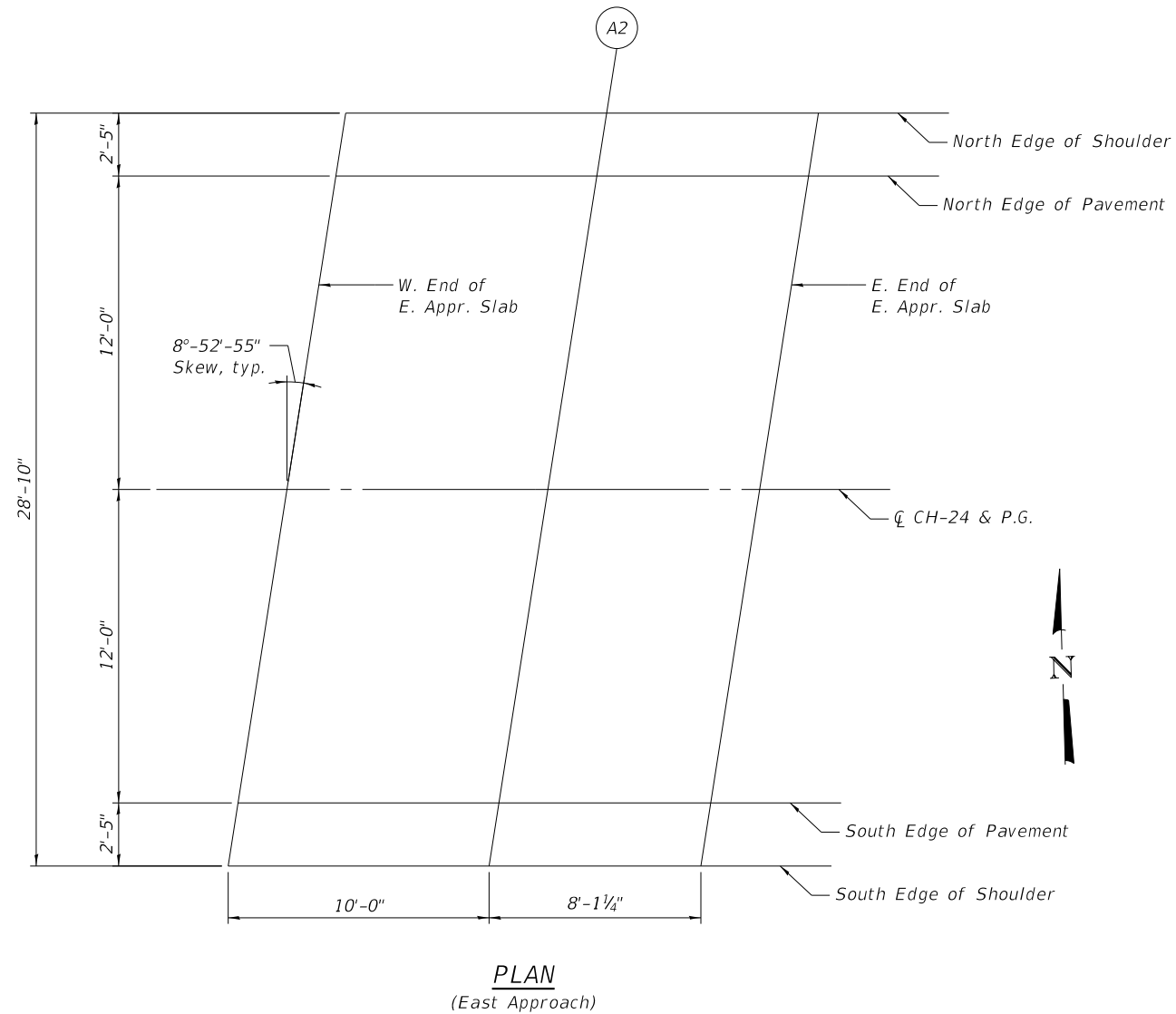
Location	Station	Offset	Theoretical Grade Elevations
W. End of E. Appr. Slab	50+16.64	0.00	682.40
A2	50+26.64	0.00	682.30
E. End of E. Appr. Slab	50+34.74	0.00	682.21

SOUTH EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
W. End of E. Appr. Slab	50+14.76	12.00	682.24
A2	50+24.76	12.00	682.14
E. End of E. Appr. Slab	50+32.87	12.00	682.05

SOUTH EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
W. End of E. Appr. Slab	50+14.38	14.42	682.19
A2	50+24.38	14.42	682.09
E. End of E. Appr. Slab	50+32.49	14.42	682.00



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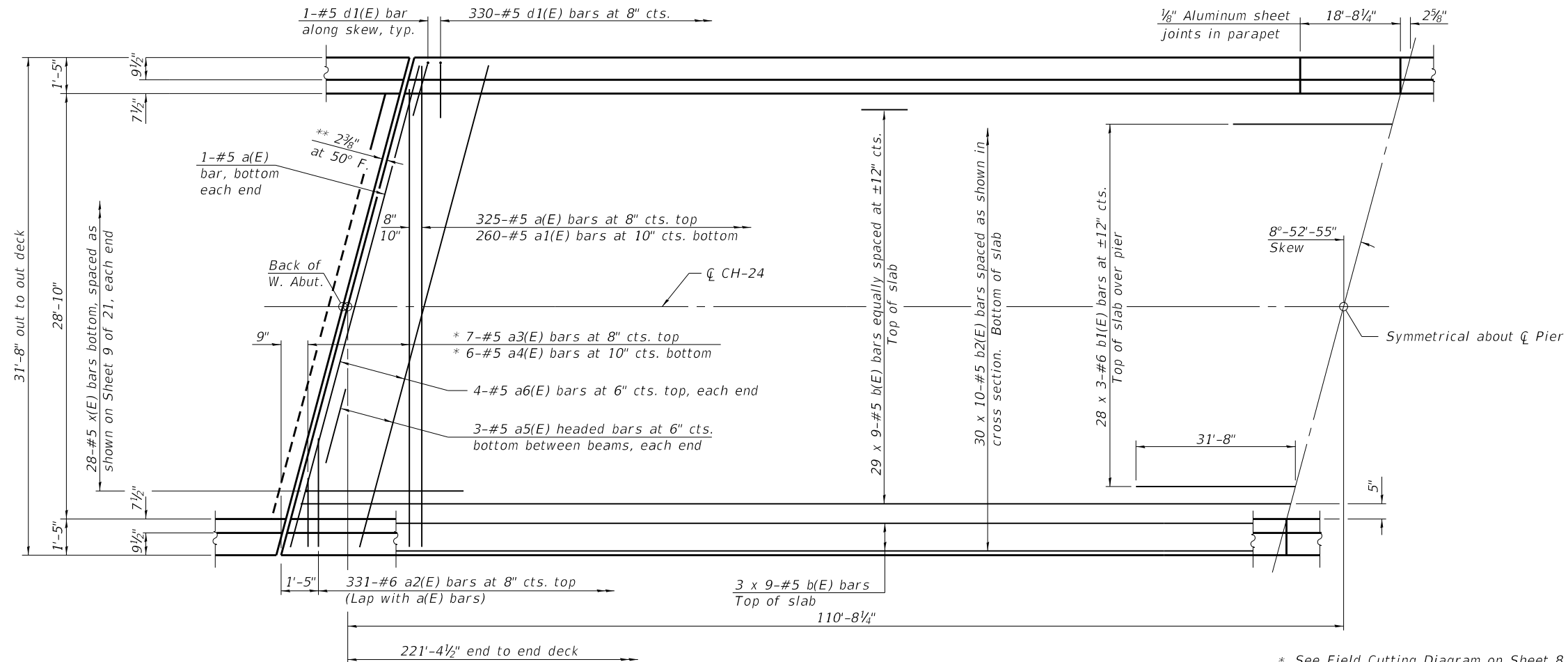
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TOP OF EAST APPROACH SLAB ELEVATIONS  
STRUCTURE NO. 068-0044**

SHEET 6 OF 21 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1768	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	160
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				

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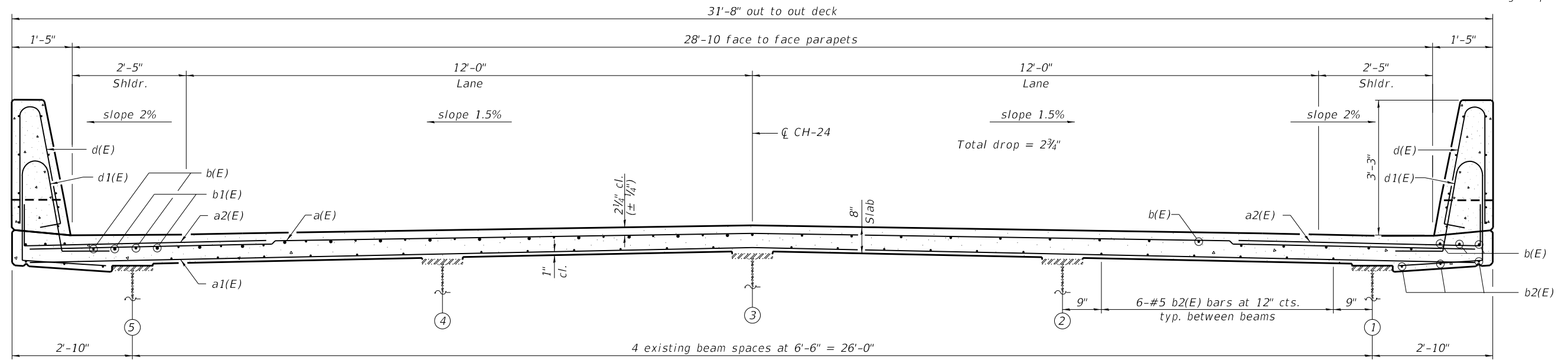
**MINIMUM BAR LAP**

#5 bar = 3'-6"  
 #6 bar = 4'-5"

**PARTIAL PLAN**

\* See Field Cutting Diagram on Sheet 8 of 21.  
 \*\* Dimension showing concrete opening. For joint opening see Sheet 13 of 21.

Notes:  
 See Sheet 8 of 21 for superstructure details and Bill of Material.  
 Bars indicated thus 20 x 3-#5 etc. indicates 20 lines of bars with 3 lengths per line.



**NEAR PIER**

**NEAR MIDSPAN**

**CROSS SECTION**  
 (Looking East)



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0680044-72D31-007-Superstructure.dgn	CHECKED - CFS	REVISED -
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

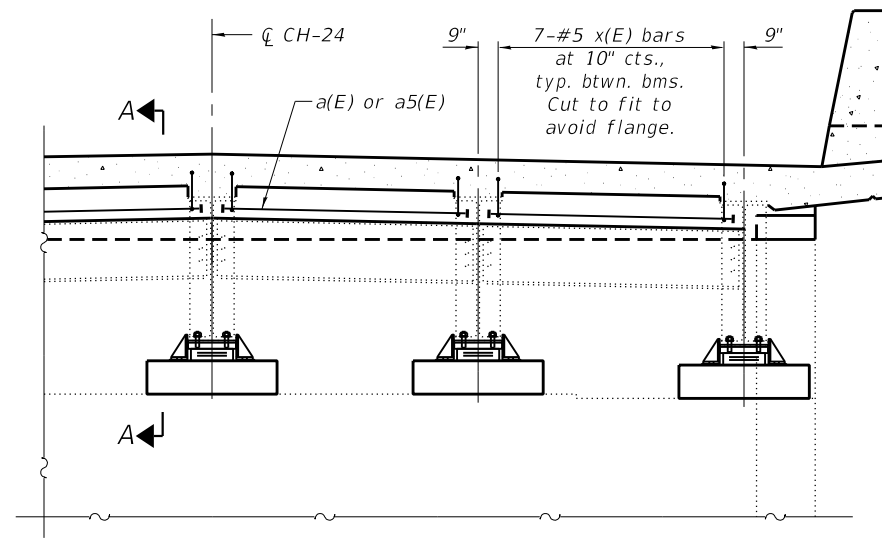
SUPERSTRUCTURE  
 STRUCTURE NO. 068-0044

SHEET 7 OF 21 SHEETS

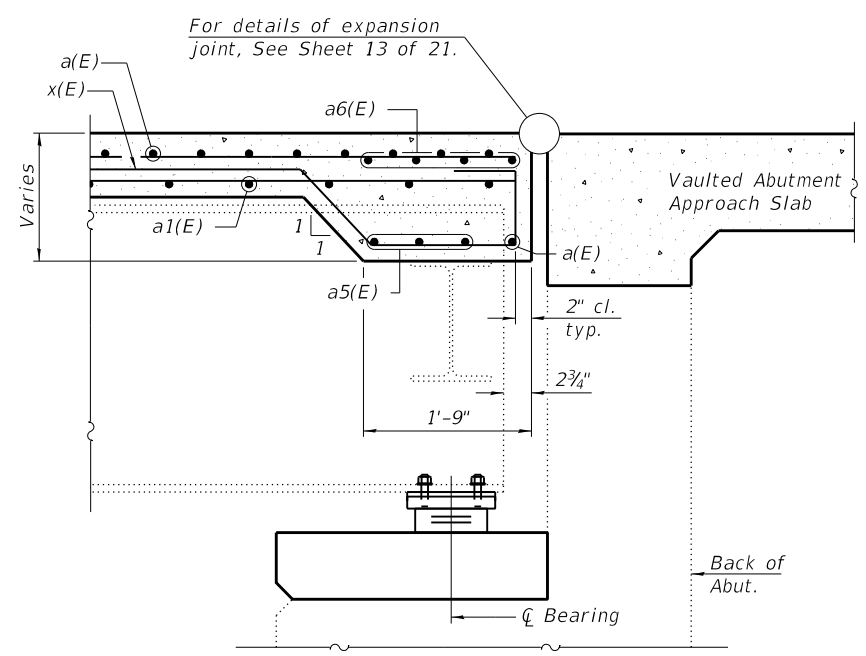
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1768	(68-1.3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	161
CONTRACT NO. 72D31				

ILLINOIS FED. AID PROJECT





DIAPHRAGM AT ABUTMENT



SECTION A-A  
(at Rt. L's)

Notes:  
See Sheet 8 of 21 for superstructure details and Bill of Material.  
The x(E) bars shall be placed parallel to the beams. Spacing for these bars shall be at right angles to the beams.

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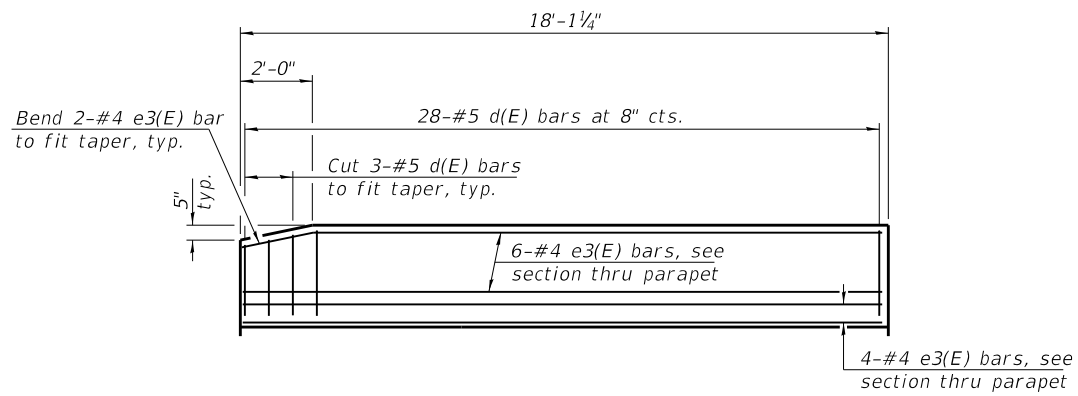
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

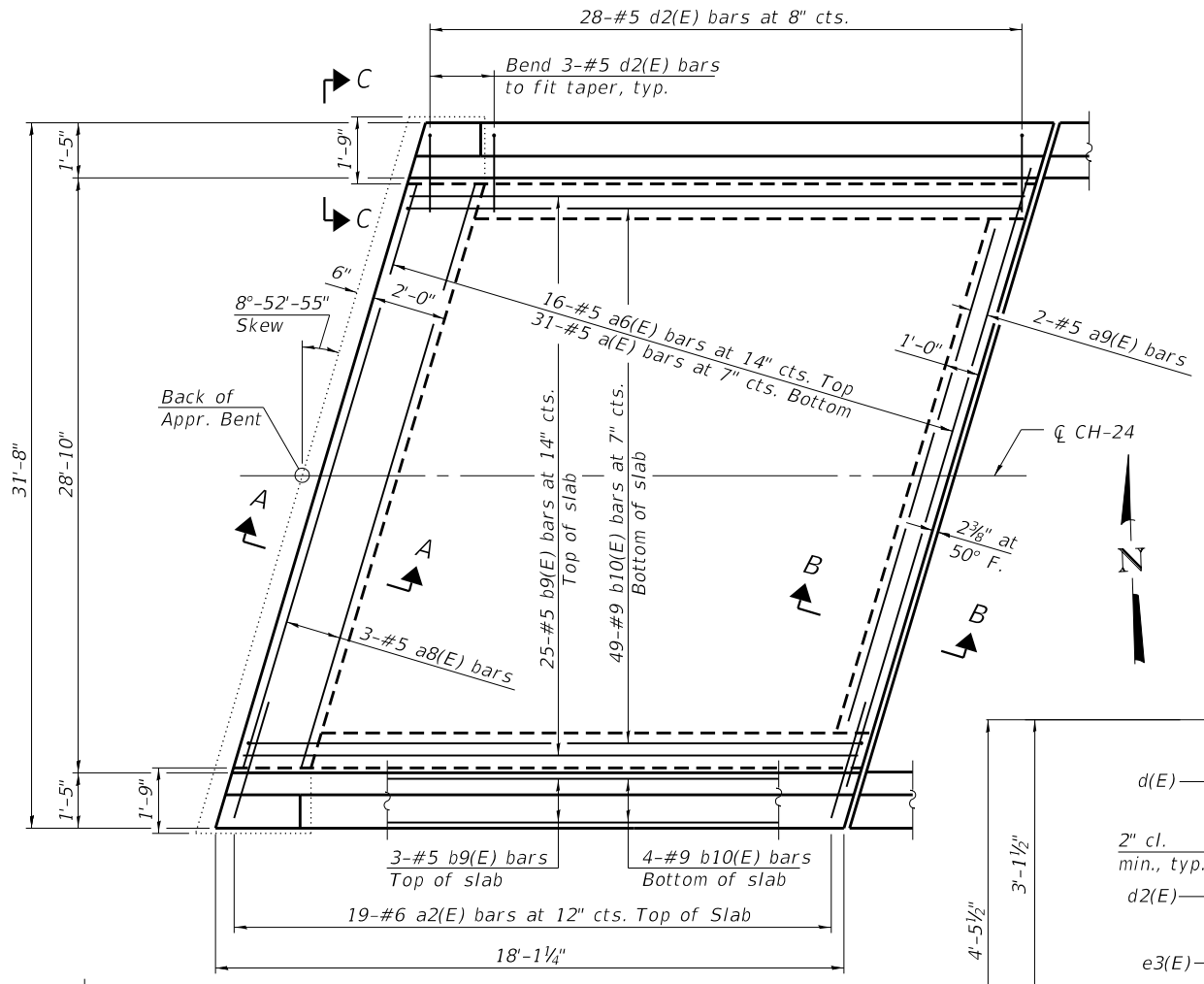
DIAPHRAGM DETAILS  
STRUCTURE NO. 068-0044

SHEET 9 OF 21 SHEETS

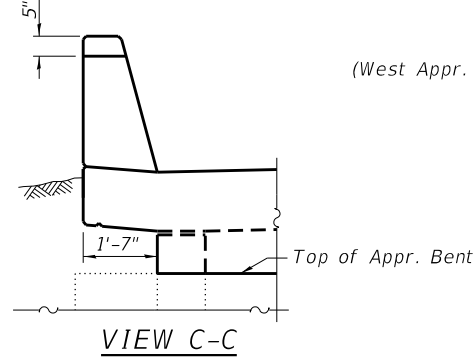
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CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				



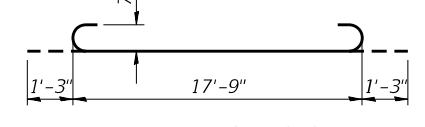
**INSIDE ELEVATION OF PARAPET**



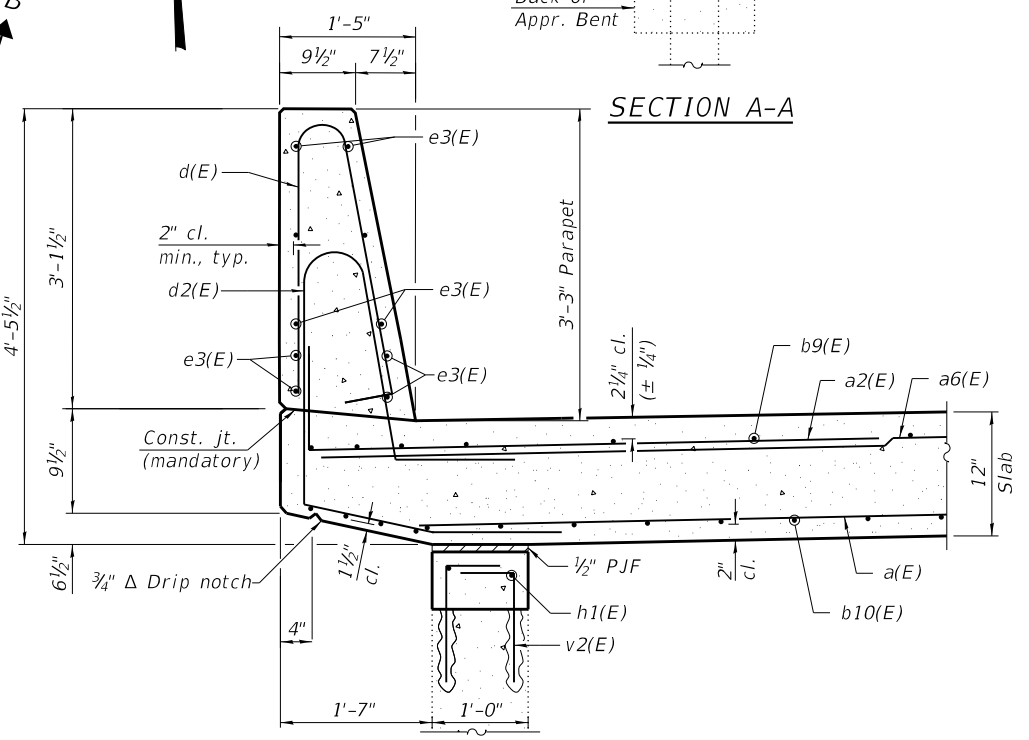
**PLAN**  
(West Appr. Shown, East Appr. Similar)



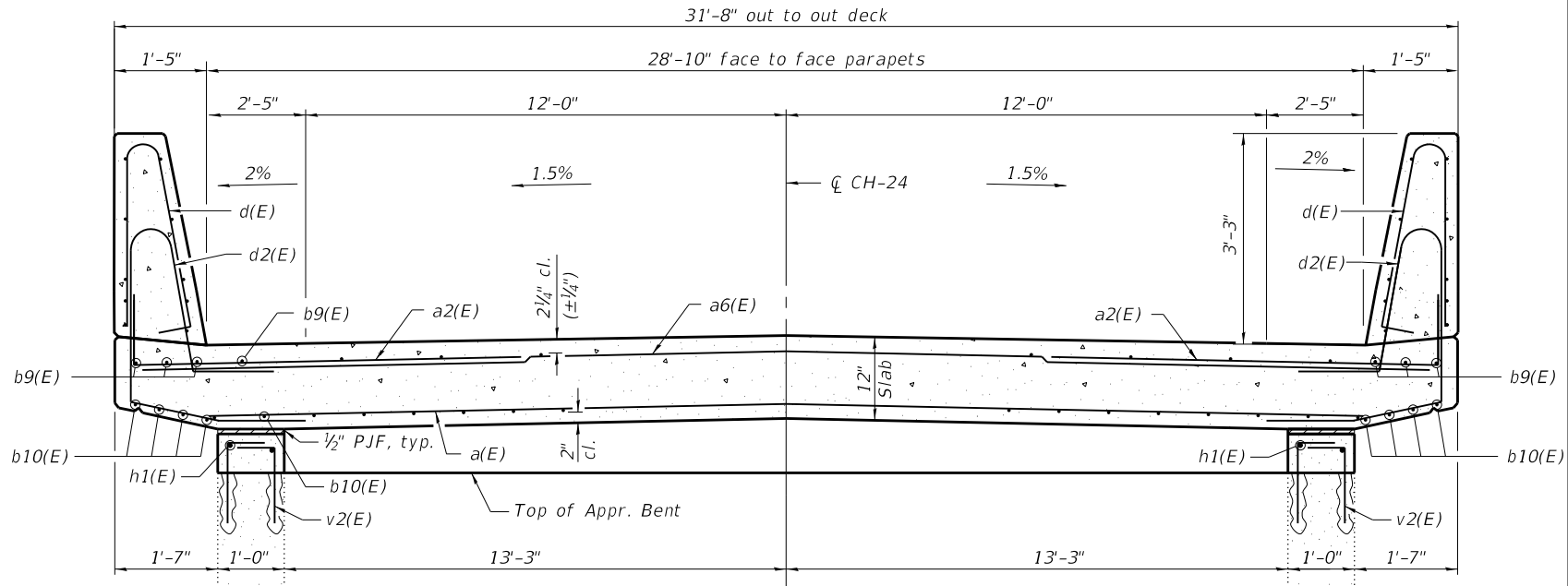
**VIEW C-C**



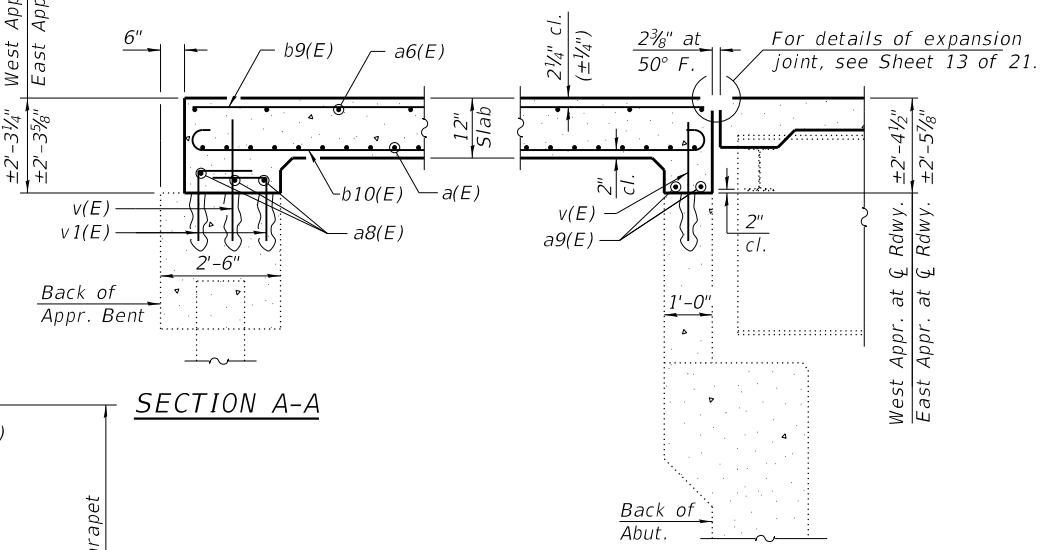
**BAR b10(E)**



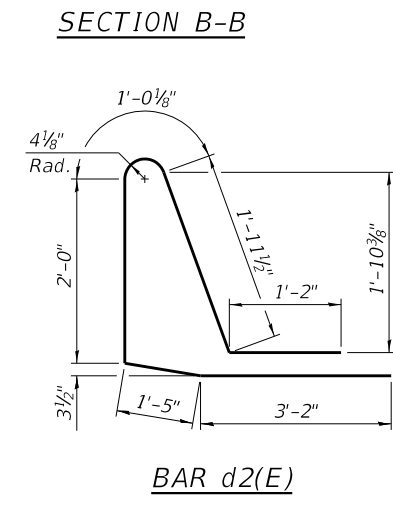
**SECTION THRU PARAPET**



**CROSS SECTION**



**SECTION A-A**



**BAR d2(E)**

Notes:  
For details of bars a2(E) and d(E) see Sheet 8 of 21.  
For location and quantity of v and h bars see abutment sheets.

**TWO APPROACH SLABS**  
**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	62	#5	31'-0"	—
a2(E)	76	#6	8'-4"	—
a6(E)	32	#5	31'-4"	—
a8(E)	6	#5	28'-6"	—
a9(E)	4	#5	26'-5"	—
b9(E)	62	#5	17'-9"	—
b10(E)	114	#9	20'-3"	—
d(E)	112	#5	6'-5"	—
d2(E)	112	#5	10'-9"	—
e3(E)	40	#4	17'-9"	—
Reinforcement Bars, Epoxy Coated			Pound	15,770
Concrete Superstructure			Cu. Yd.	55.6

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

VAULTED ABUTMENT APPROACH SPAN  
STRUCTURE NO. 068-0044

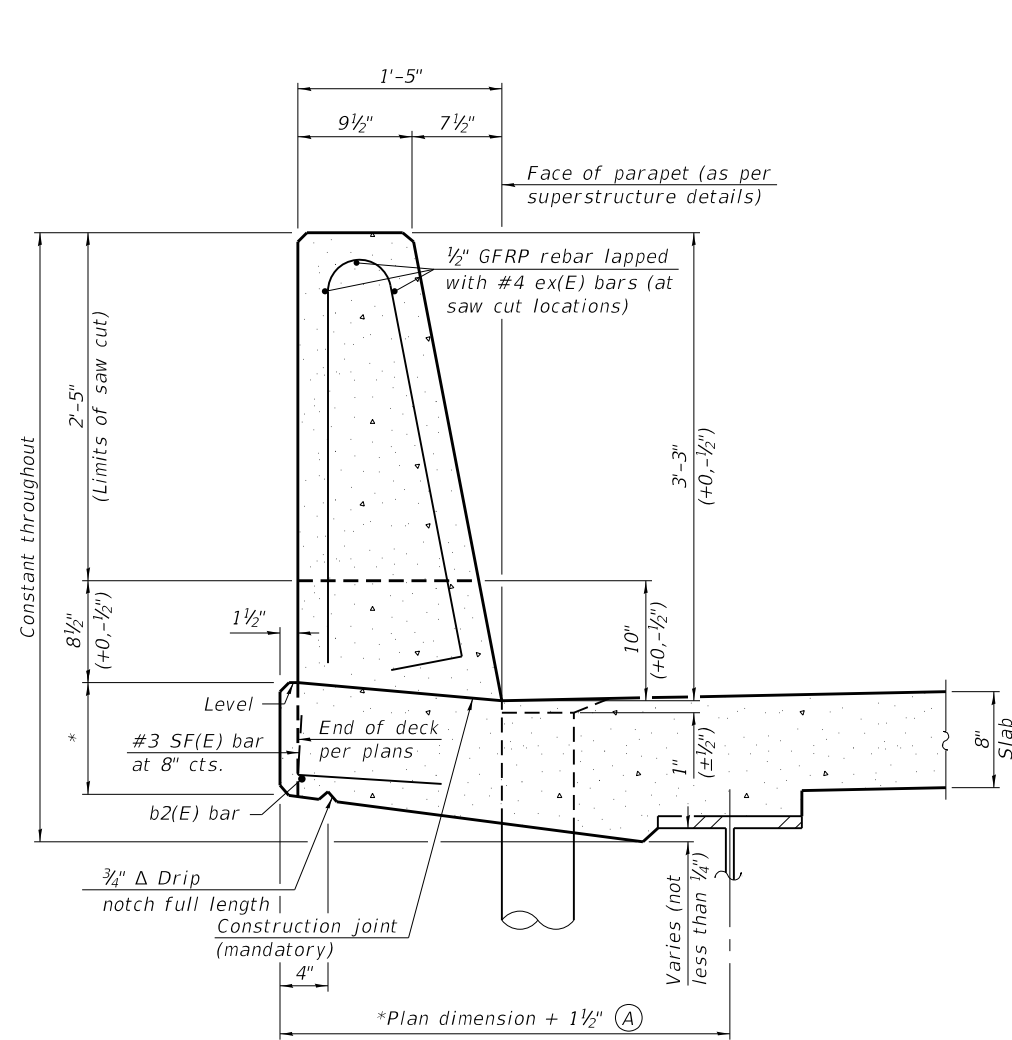
SHEET 10 OF 21 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				



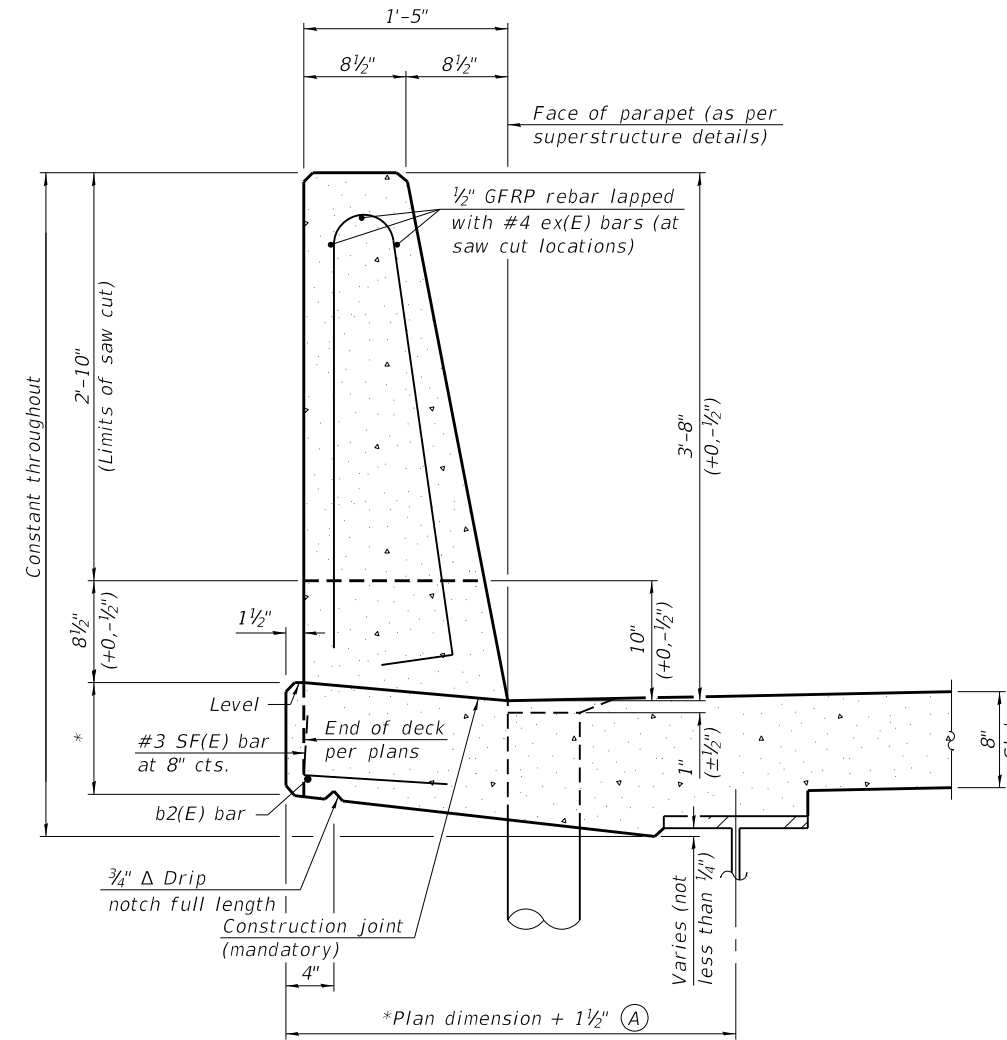
**GENERAL NOTES**

All dimensions shall remain the same as shown on superstructure details, except dimension A which is to be revised as shown. Additional concrete needed to revise dimension A = 0.00348 cu. yds./ft. for 39" and 44" parapets.  
 Place full depth aluminum sheets as shown on superstructure details.  
 Replace all cork joint filler locations with a full thickness saw cut.  
 Steel superstructure shown. Other superstructure types similar.



**39" CONSTANT-SLOPE  
PARAPET SECTION**

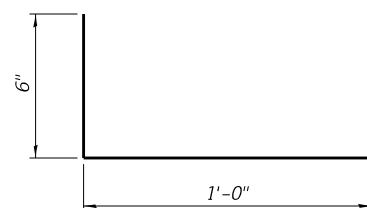
(Showing dimensions, d(E), and 1/2" Ø GFRP rebar)



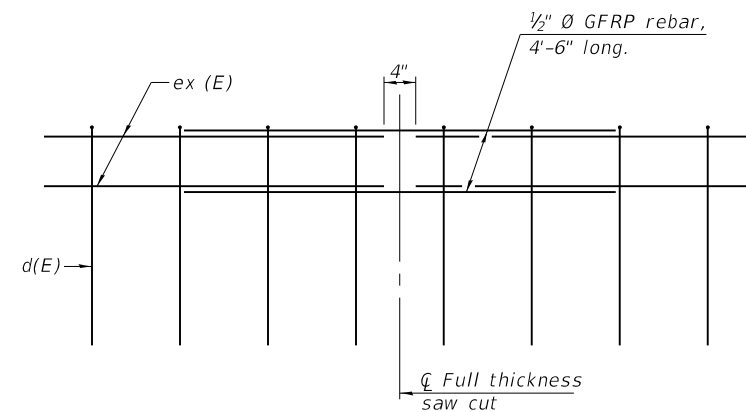
**44" CONSTANT-SLOPE  
PARAPET SECTION**

(Showing dimensions, d(E), and 1/2" Ø GFRP rebar)

\*See Superstructure Details.



**#3 (E) BAR**



**GFRP REBAR STIFFENING DETAIL**

(Place as shown in parapet section at each parapet joint location.)

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SFP 39-44

1-14-2019



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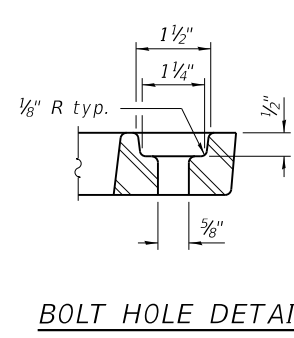
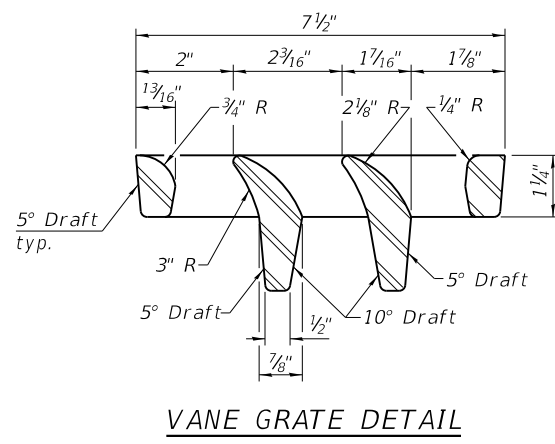
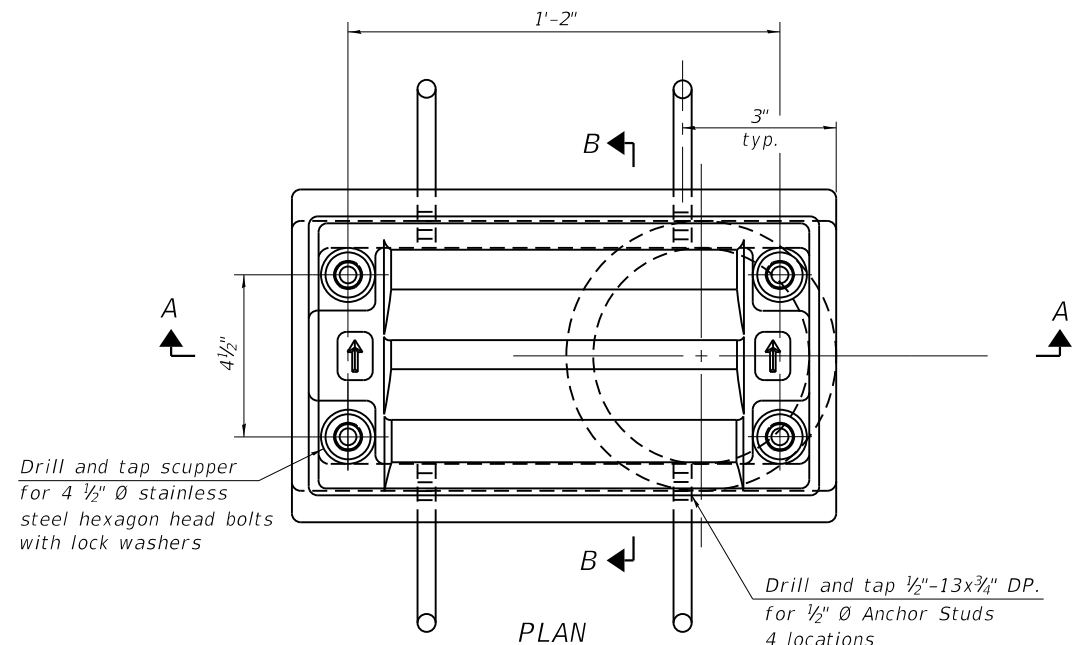
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CONCRETE PARAPET SLIPFORMING OPTION  
STRUCTURE NO. 068-0044**

SHEET 11 OF 21 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1768	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	165
			CONTRACT NO. 72D31	

ILLINOIS FED. AID PROJECT



Notes:

All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 35B.

Bolts, anchor studs, washers and nuts shall conform to the requirements of ASTM A 307 and shall be galvanized according to AASHTO M 232.

Downspouts located on the exterior side of a painted steel fascia beam shall be painted with the finish coat specified for the exterior side of the fascia beam.

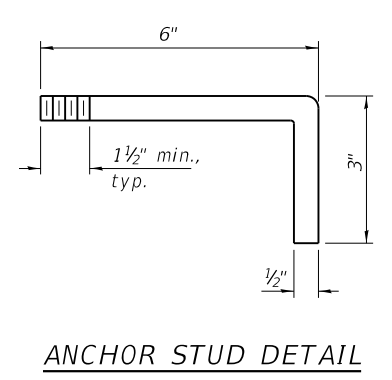
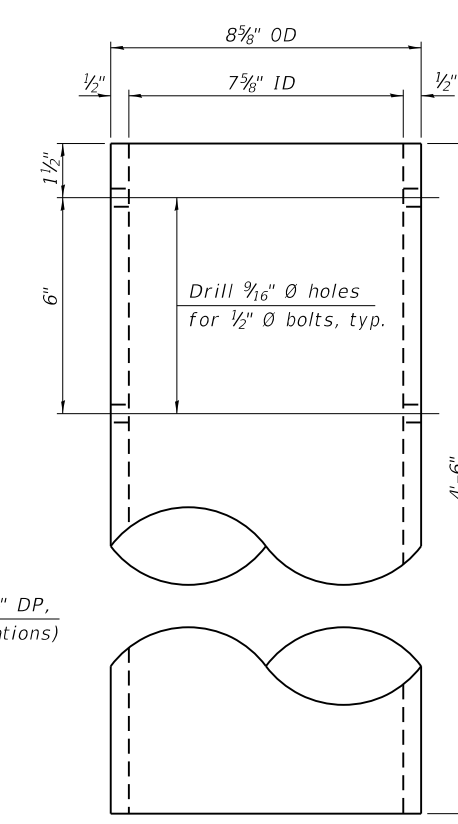
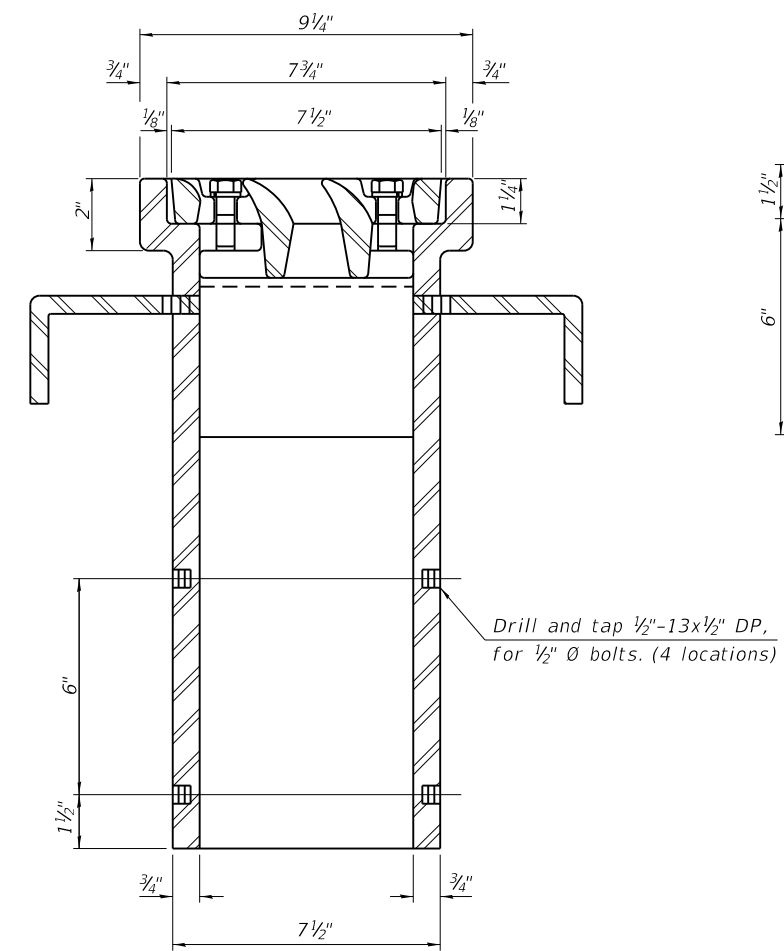
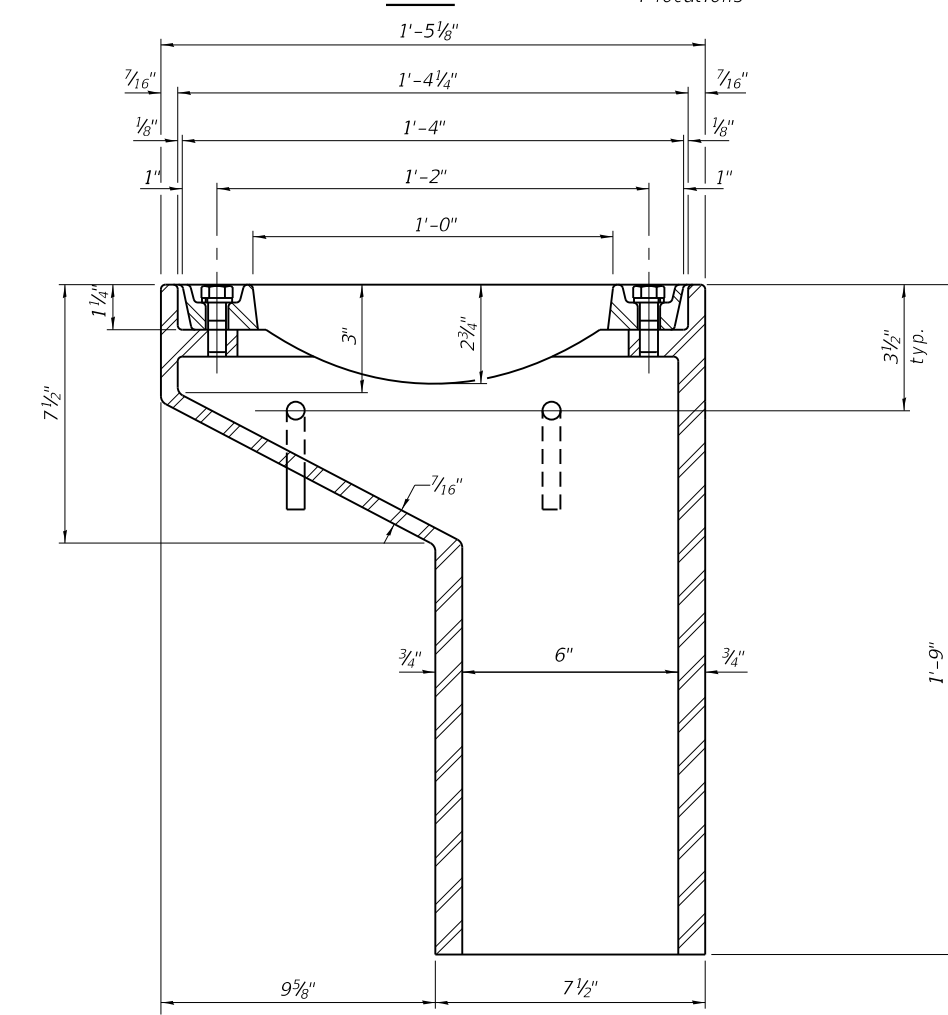
As an alternate, bolts, anchor studs, washers and nuts may be stainless steel according to Article 1006.29(d) of the Standard Specifications.

Structural steel weldments of equal sections and of the same configuration may be substituted for the cast iron scupper frame. Fillet or full penetration welds shall be used for the weldments. Details shall be submitted to the Engineer for approval. Structural steel weldments shall not be substituted for the cast iron scupper grate. Structural steel frames and downspouts shall be galvanized according to AASHTO M111.

The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the scupper.

Cost of the Grate, Frame, Downspout, Anchor Studs, Bolts, Washers and Nuts including complete installation of the scupper shall be paid for at the contract unit price each for Drainage Scupper, DS-11.

Alternate fiberglass downspout conforming to ASTM D 2996 with a short-time rupture strength hoop tensile stress of 30,000 psi min. may be used in lieu of the cast iron or steel equivalent.



See Sheet 8 of 21 for scupper location relative to parapet.

**BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Drainage Scupper, DS-11	Each	14

DS-11 2-17-2017



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PLOT DATE = 12/10/2019	CHECKED - KWB	REVISED -

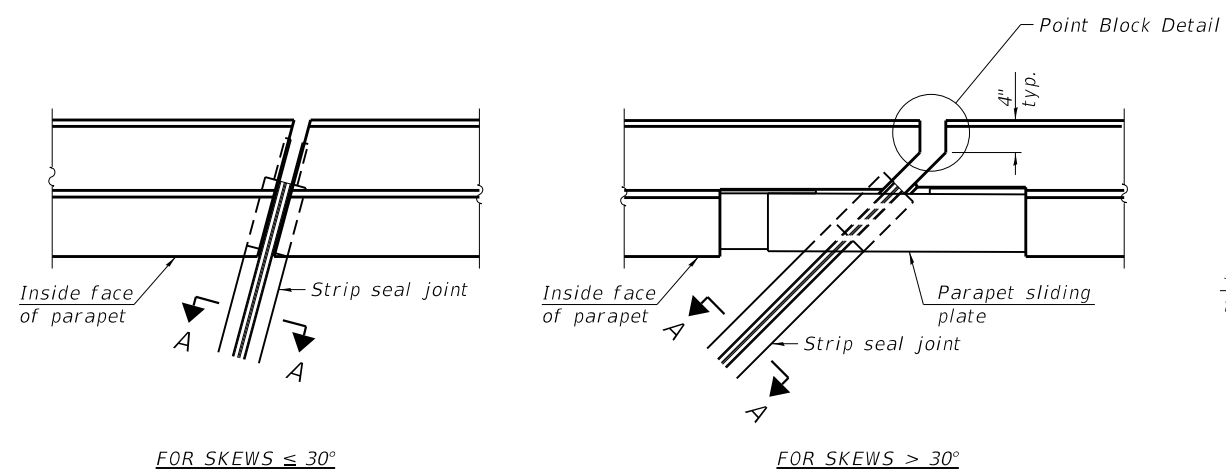
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**DRAINAGE SCUPPER, DS-11**  
**STRUCTURE NO. 068-0044**

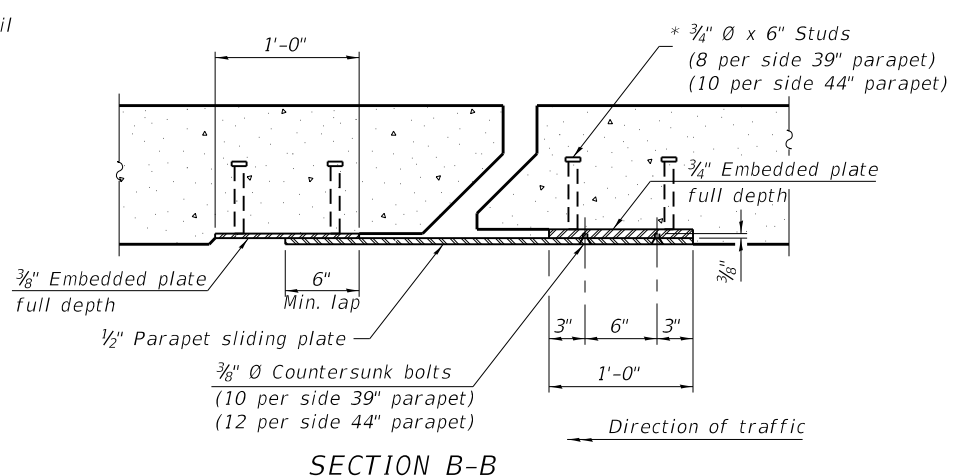
SHEET 12 OF 21 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1768	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	166
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72D31	

MODEL: Default  
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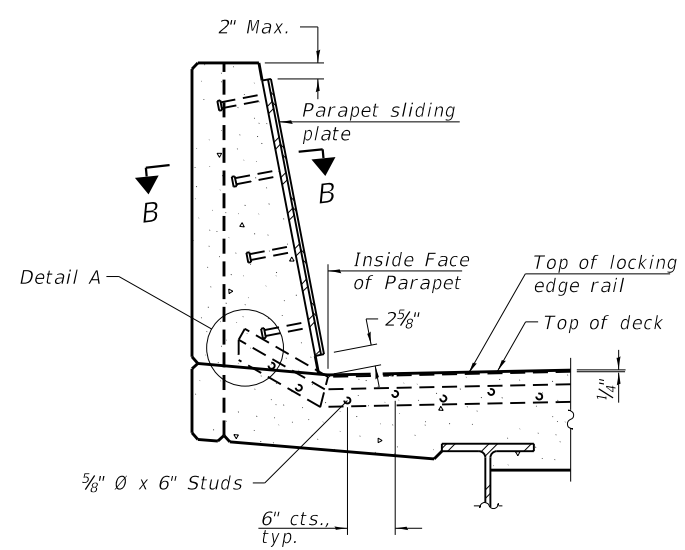


FOR SKEWS  $\leq 30^\circ$   
**PLAN AT PARAPET**

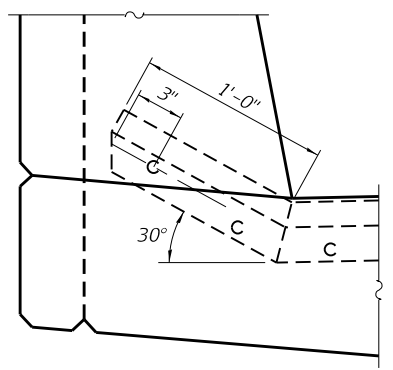


**SECTION B-B**

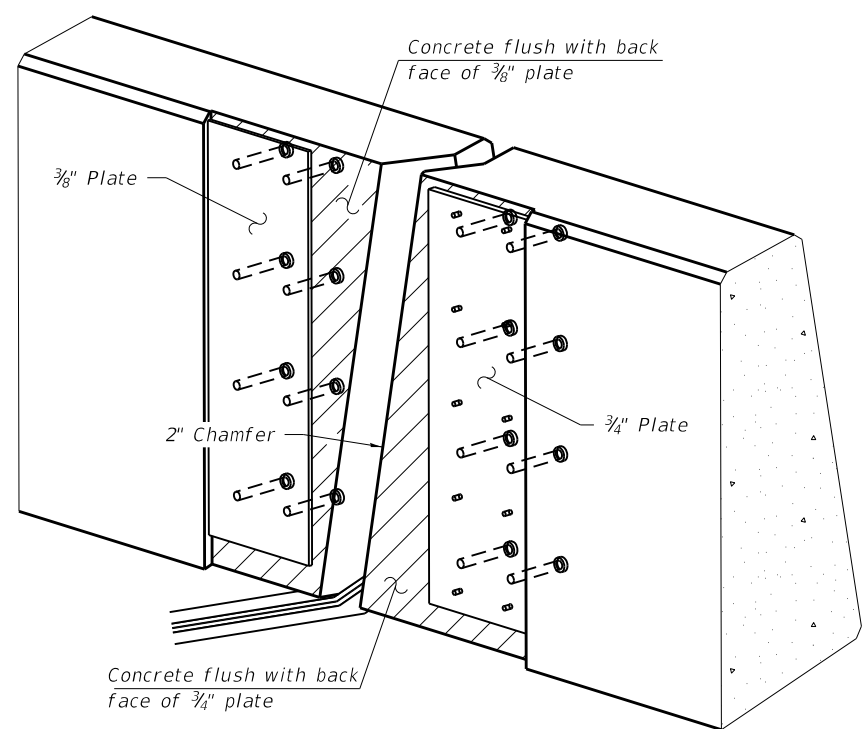
**Notes:**  
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.  
 The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.  
 The manufacturer's recommended installation methods shall be followed.  
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.  
 The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.  
 The top surface of sidewalk sliding plates shall have a raised pattern according to ASTM A786.  
 Cost of parapet sliding plates, sidewalk sliding plates, embedded plates, anchorage studs, and expansion anchors included with Preformed Joint Strip Seal.  
 39" constant slope barrier shown, 44" constant slope barrier similar as noted.  
 The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.



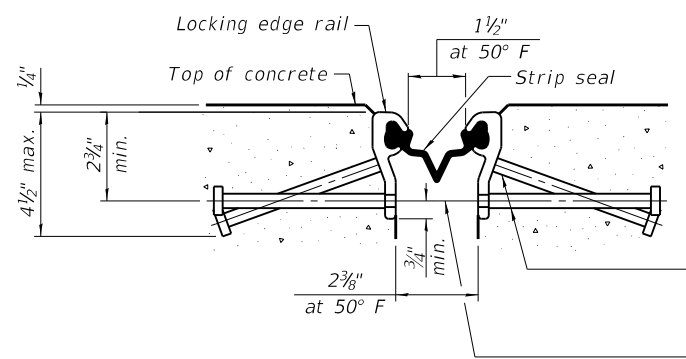
**SECTION AT PARAPET**  
 (Skews > 30° shown. Skews  $\leq 30^\circ$  similar except as shown in plan view.)



**DETAIL A**



**TRIMETRIC VIEW**  
 (Showing embedded plates only)



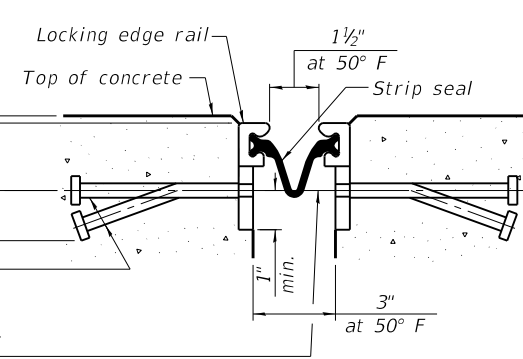
**SHOWING ROLLED RAIL JOINT**

\* 3/8"  $\varnothing$  x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

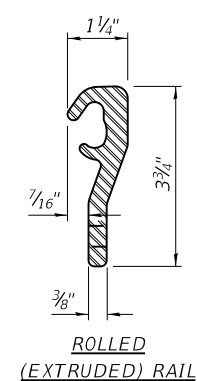
3/8"  $\varnothing$  threaded rods in 7/16"  $\varnothing$  holes at  $\pm 4$ -0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

**SECTION A-A**

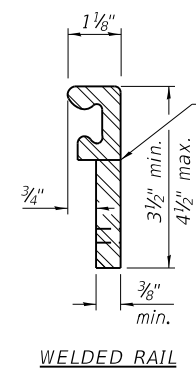
\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



**SHOWING WELDED RAIL JOINT**



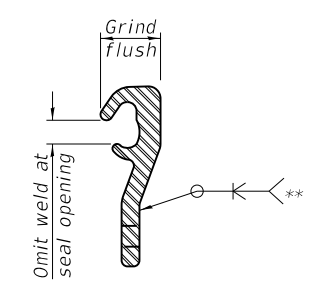
**ROLLED (EXTRUDED) RAIL**



**WELDED RAIL**

**LOCKING EDGE RAILS**

\*\* Back gouge not required if complete joint penetration is verified by mock-up.



**LOCKING EDGE RAIL SPLICE**

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

**BILL OF MATERIAL**

Item	Unit	Total
Preformed Joint Strip Seal	Foot	62.5



USER NAME = cstokes  
 0680044-72D31-013-Pref Jt Strip Seal.dgn  
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 PLOT DATE = 12/10/2019

DESIGNED - RPW  
 CHECKED - CFS  
 DRAWN - RPW  
 CHECKED - KWB

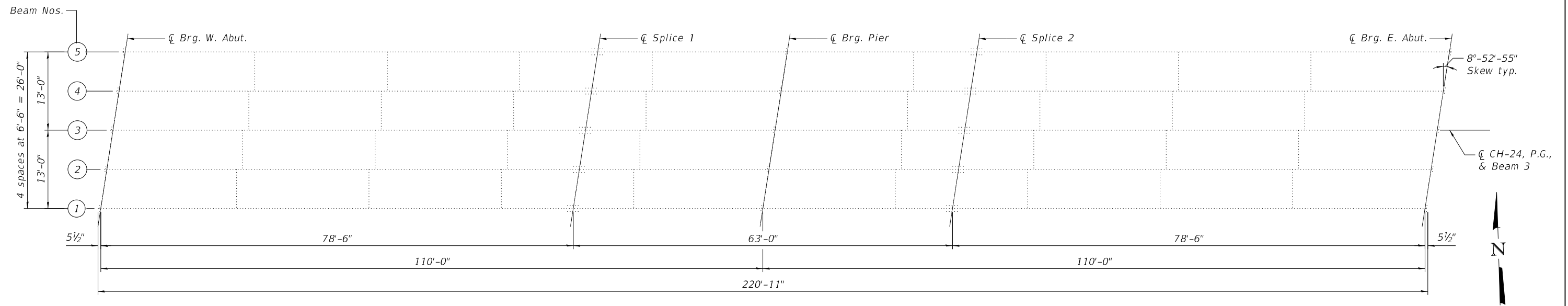
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

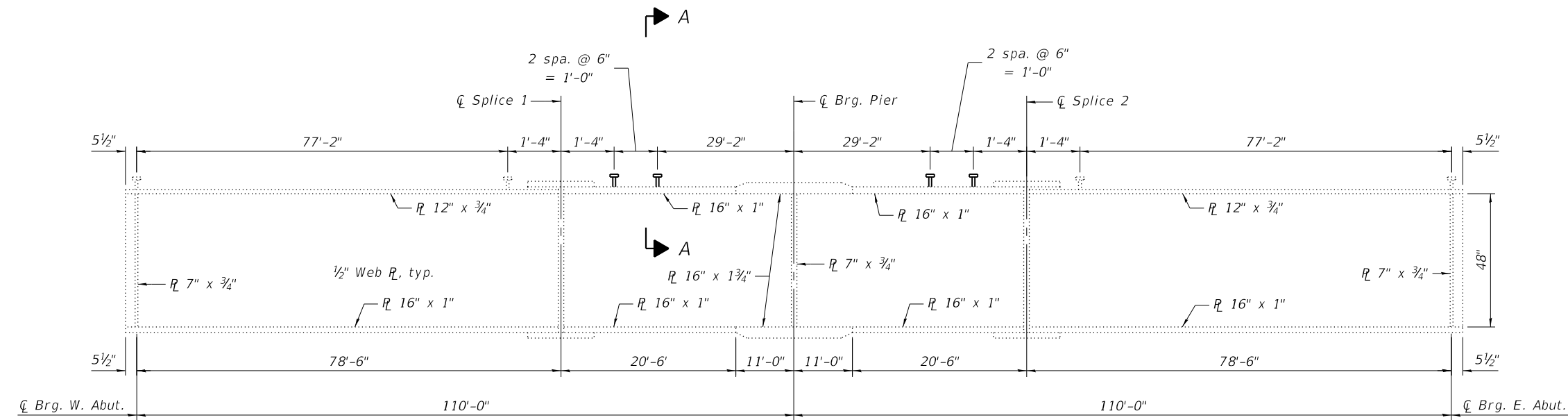
PREFORMED JOINT STRIP SEAL  
 STRUCTURE NO. 068-0044

SHEET 13 OF 21 SHEETS

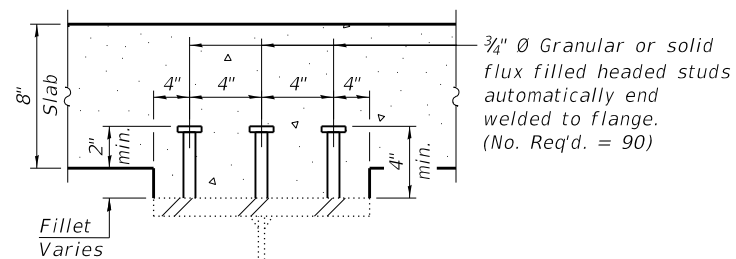
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1768	(68-1.3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	167
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				



EXISTING FRAMING PLAN



EXISTING BEAM ELEVATION



SECTION A-A

MODEL: Default  
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PLOT DATE = 12/10/2019	CHECKED - KWB	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

FRAMING PLAN  
 STRUCTURE NO. 068-0044

SHEET 14 OF 21 SHEETS

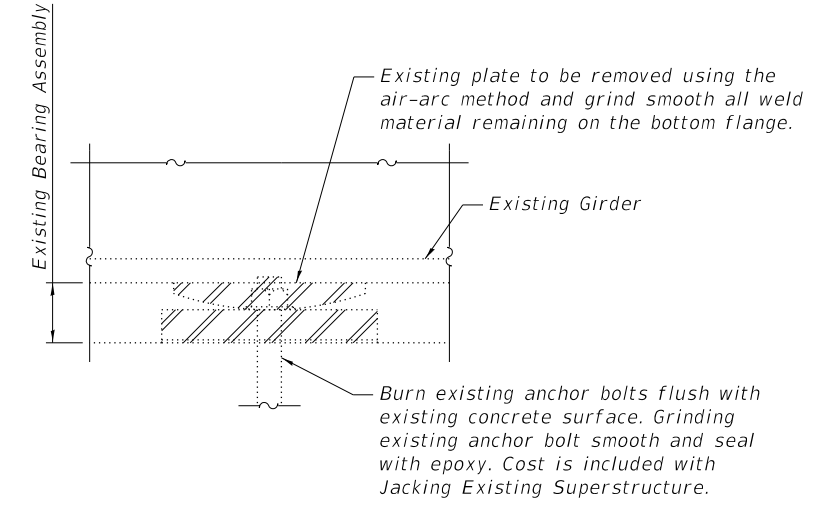
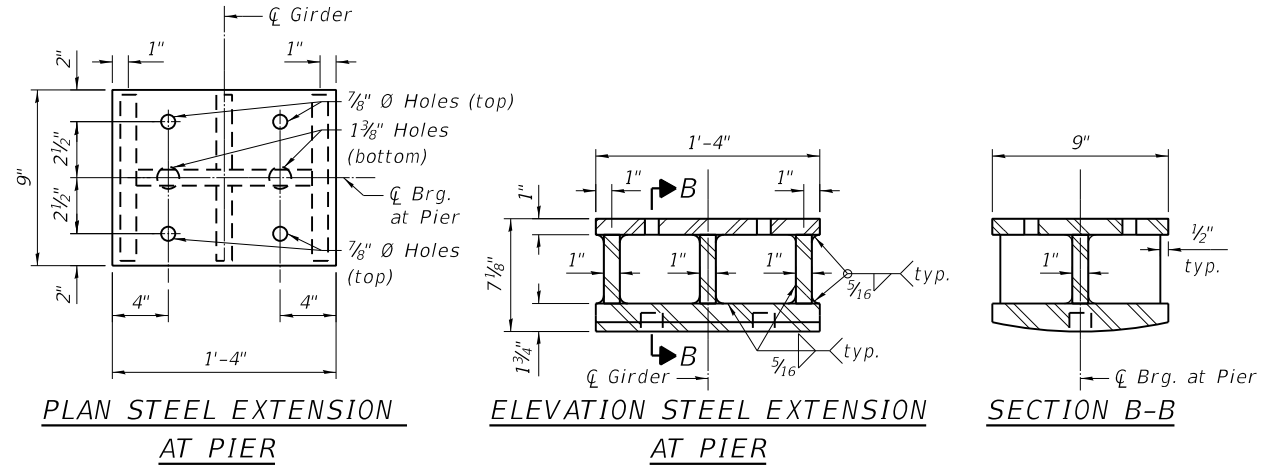
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1768	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	168
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				

INTERIOR GIRDER MOMENT TABLE		
	0.4 Sp. 1 or 0.6 Sp. 2	Pier
$I_s$	(in <sup>4</sup> ) 18,953	39,273
$I_c(n)$	(in <sup>4</sup> ) 52,686	39,273
$I_c(3n)$	(in <sup>4</sup> ) 37,897	39,273
$S_s$	(in <sup>3</sup> ) 882	1,525
$S_c(n)$	(in <sup>3</sup> ) 1,257	1,525
$S_c(3n)$	(in <sup>3</sup> ) 1,148	1,525
$Z$	(in <sup>3</sup> ) -	1,681
$\rho$	(k/ft.) 0.91	1.02
$M\rho$	(k) 665	1,506
$s\rho$	(k/ft.) 0.35	0.35
$M_s\rho$	(k) 311	507
$M_L$	(k) 859	612
$MIM$	(k) 183	130
$\frac{2}{3}[M_L + I]$	(k) 1,737	1,237
$M_a$	(k) 3,527	4,225
$M_u$	(k) 4,990	5,043
$f_s\rho$ non-comp	(ksi) 9.0	11.9
$f_s\rho$ (comp)	(ksi) 3.3	4.0
$f_s \frac{2}{3}[M_L + M_I]$	(ksi) 16.6	9.8
$f_s$ (Overload)	(ksi) 28.9	25.7
$f_s$ (Total)	(ksi) -	-
$VR$	(k) 56.2	-

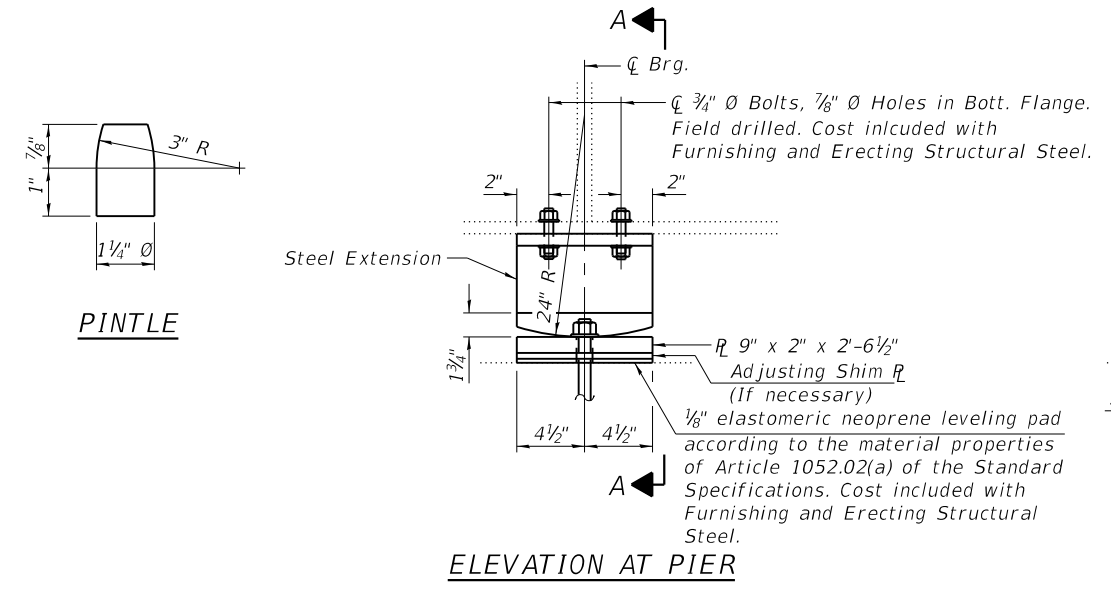
INTERIOR GIRDER REACTION TABLE		
	Abut.	Pier
$R\rho$	(k) 49.9	173.4
$R_L$	(k) 42.8	64.9
$R_I$	(k) 9.1	9.4
$R_{Total}$	(k) 101.8	247.7

\* Compact section  
 \*\* Braced non-compact and partially braced section

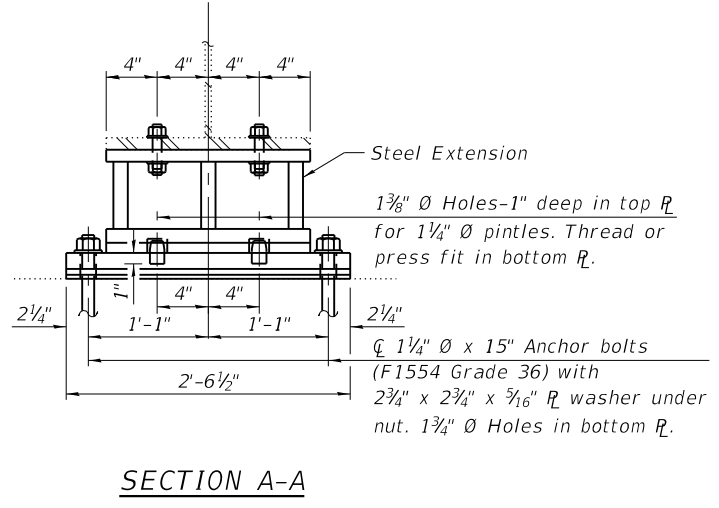
$I_s, S_s$ : Non-composite moment of inertia and section modulus of the steel section used for computing  $f_s$ (Total and Overload) due to non-composite dead loads (in.<sup>4</sup> and in.<sup>3</sup>).  
 $I_c(n), S_c(n)$ : Composite moment of inertia and section modulus of the steel and deck based upon the modular ratio, "n", used for computing  $f_s$ (Total and Overload) due to short-term composite live loads (in.<sup>4</sup> and in.<sup>3</sup>).  
 $I_c(3n), S_c(3n)$ : Composite moment of inertia and section modulus of the steel and deck based upon 3 times the modular ratio, "3n", used for computing  $f_s$ (Total and Overload) due to long-term composite (superimposed) dead loads (in.<sup>4</sup> and in.<sup>3</sup>).  
 $Z$ : Plastic Section Modulus of the steel section in non-composite areas (in.<sup>3</sup>).  
 $\rho$ : Un-factored non-composite dead load (kips/ft.).  
 $M\rho$ : Un-factored moment due to non-composite dead load (kip-ft.).  
 $s\rho$ : Un-factored long-term composite (superimposed) dead load (kips/ft.).  
 $M_s\rho$ : Un-factored moment due to long-term composite (superimposed) dead load (kip-ft.).  
 $M_L$ : Un-factored live load moment (kip-ft.).  
 $M_I$ : Un-factored moment due to impact (kip-ft.).  
 $M_a$ : Factored design moment (kip-ft.).  
 $1.3 [M\rho + M_s\rho + \frac{2}{3}(M_L + M_I)]$   
 $M_u$ : Compact composite moment capacity according to AASHTO LFD 10.50.1.1 or compact non-composite moment capacity according to AASHTO LFD 10.48.1 (kip-ft.).  
 $f_s$  (Overload): Sum of stresses as computed from the moments below (ksi).  
 $M\rho + M_s\rho + \frac{2}{3}(M_L + M_I)$   
 $f_s$  (Total): Sum of stresses as computed from the moments below on non-compact section (ksi).  
 $1.3 [M\rho + M_s\rho + \frac{2}{3}(M_L + M_I)]$   
 $VR$ : Maximum  $\frac{1}{2}$  in. impact shear range within the composite portion of the span for stud shear connector design (kips).



EXISTING BEARING REMOVAL DETAIL (PIER)  
 (Cost of bearing removal is included with Jacking Existing Superstructure)



FIXED BEARING



SECTION A-A

Notes:  
 Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.  
 Anchor bolts at all supports shall be installed as each existing bearing assembly is replaced unless an equivalent temporary means of lateral restraint is used.  
 Plates and fasteners required for steel extensions and other steel members required for the fixed bearing assembly shall be paid for with Furnishing and Erecting Structural Steel.  
 Prior to ordering any material, the Contractor shall verify steel extension height needed to accomplish minimum vertical clearance.  
 Two 1/8 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details. Cost included with Furnishing and Erecting Structural Steel.

BILL OF MATERIAL

Item	Unit	Total
Anchor Bolts, 1 1/4"	Each	10

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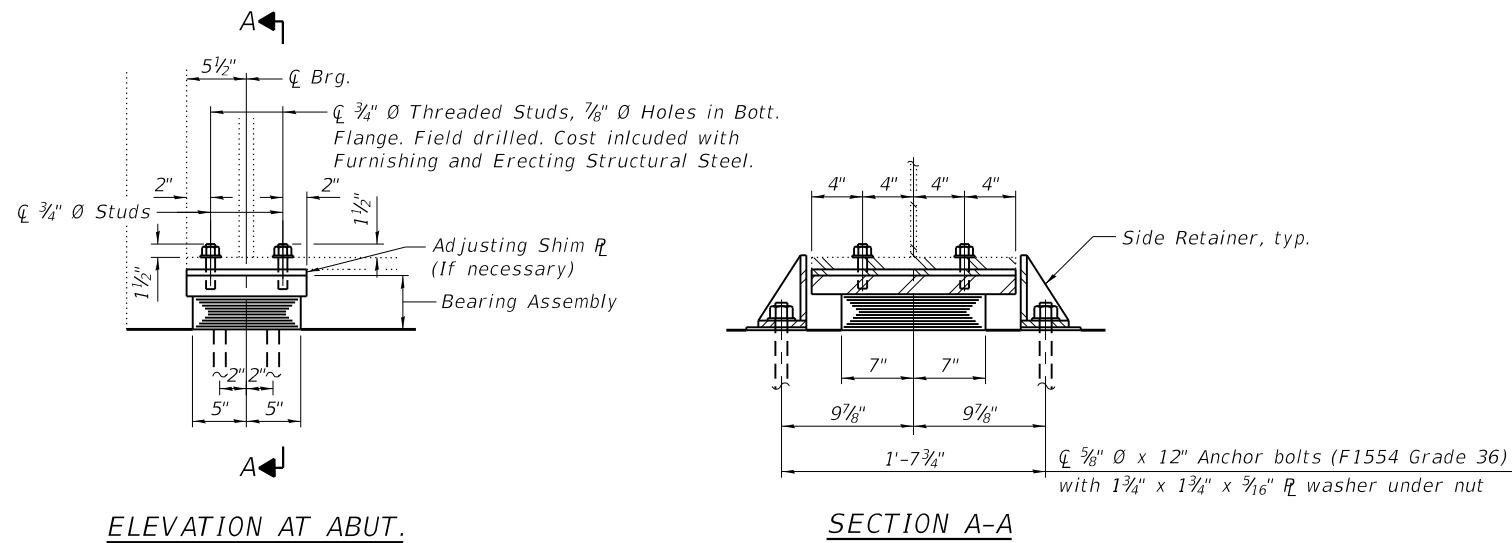
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

STRUCTURAL STEEL DETAILS  
 STRUCTURE NO. 068-0044

SHEET 15 OF 21 SHEETS

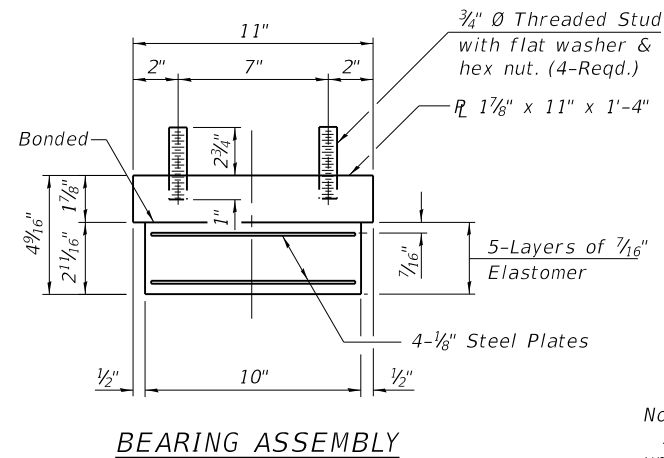
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1768	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	169
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				



ELEVATION AT ABUT.

SECTION A-A

**TYPE I ELASTOMERIC EXP. BRG.**

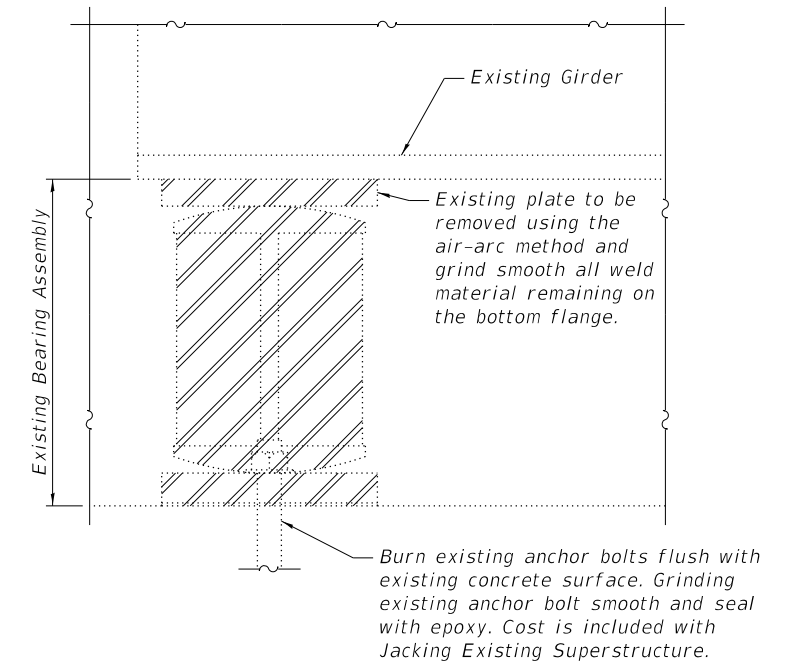


BEARING ASSEMBLY

Note:  
Shim plates shall not be placed under Bearing Assembly.

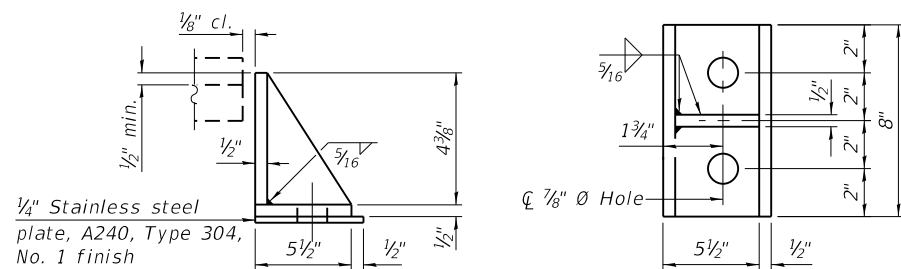
**SHIM PLATE THICKNESS TABLE**

Location	Beam 1	Beam 2	Beam 3	Beam 4	Beam 5
W. Abut.	1/4"	1/4"	3/4"	3/8"	3/8"
Pier	1/4"	5/8"	1 1/8"	5/8"	1/8"
E. Abut.	5/8"	1/2"	7/8"	1/4"	1/8"



**EXISTING BEARING REMOVAL DETAIL (BOTH ABUTMENTS)**

(Cost of bearing removal is included with Jacking Existing Superstructure)



**SIDE RETAINER**

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

**BILL OF MATERIAL**

Item	Unit	Total
Elastomeric Bearing Assembly Type I	Each	10
Anchor Bolts, 5/8"	Each	40

MODEL: Default  
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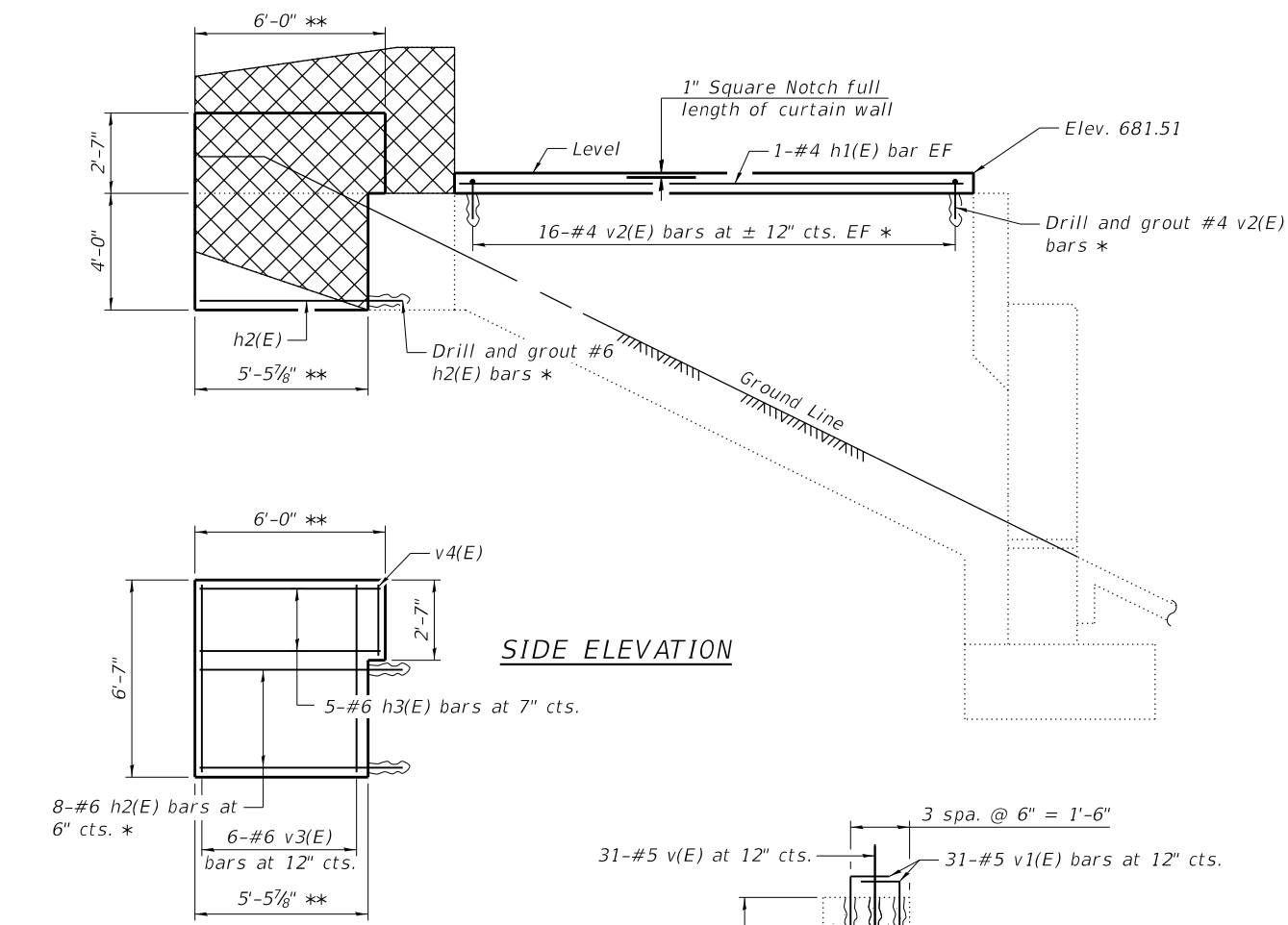
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BEARING DETAILS  
STRUCTURE NO. 068-0044

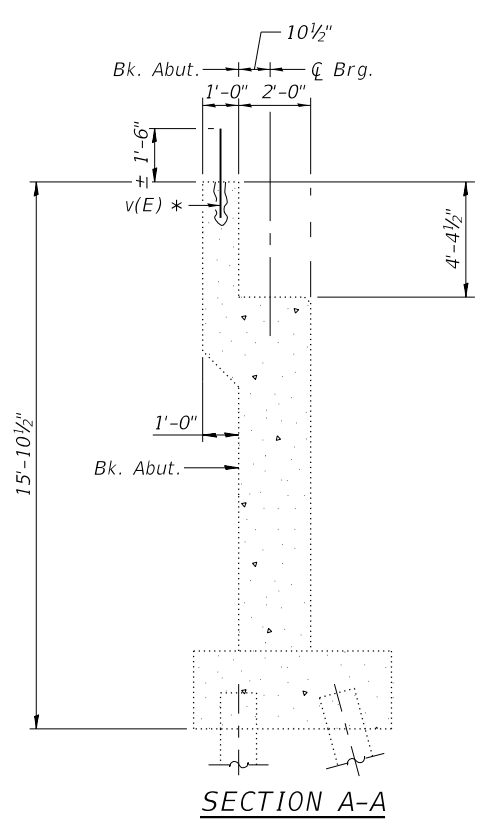
SHEET 16 OF 21 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1768	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	170
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				

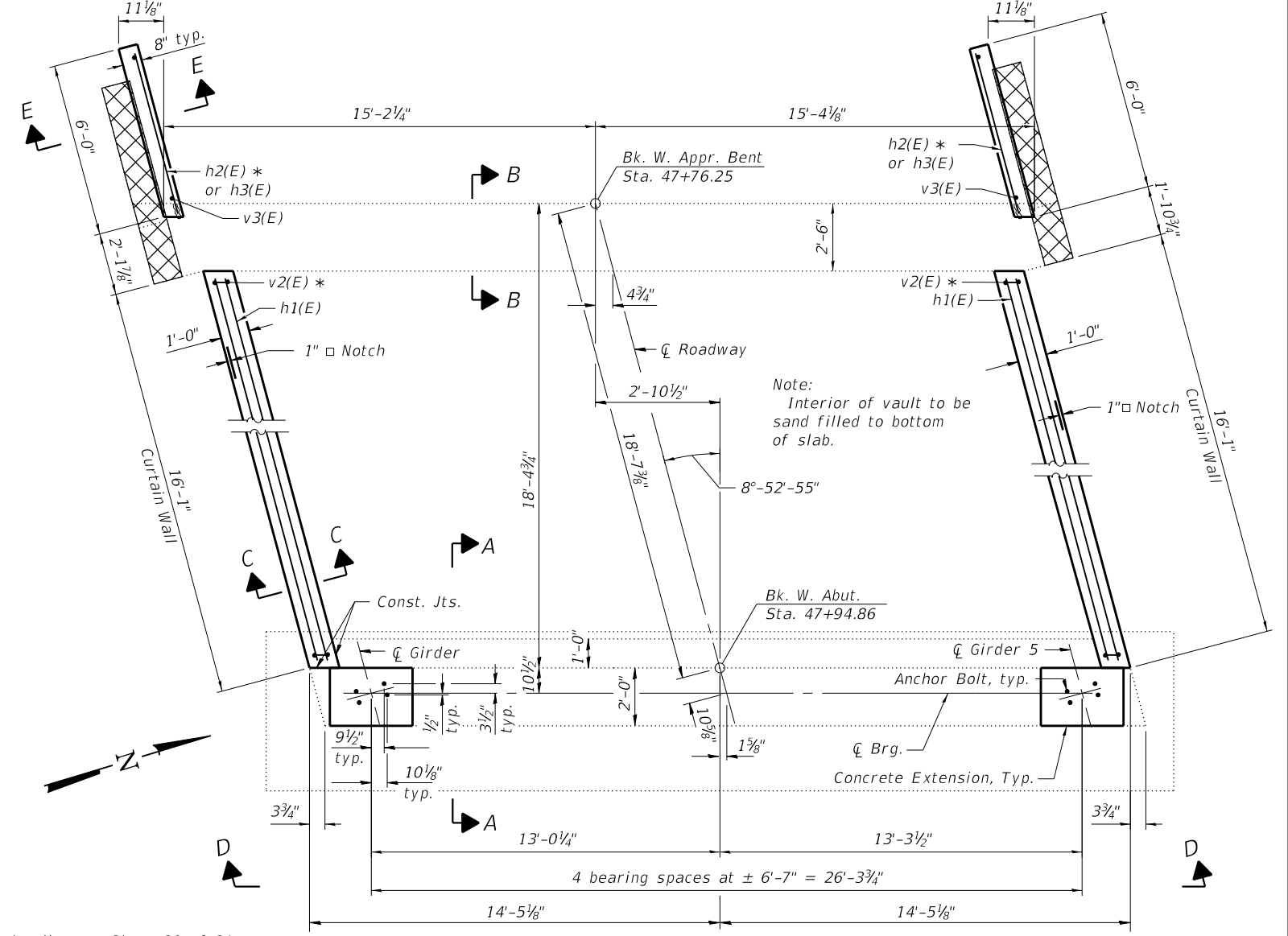
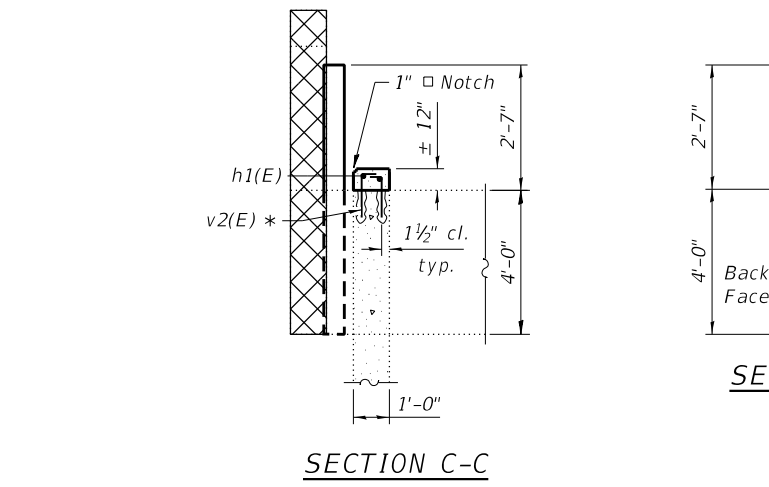
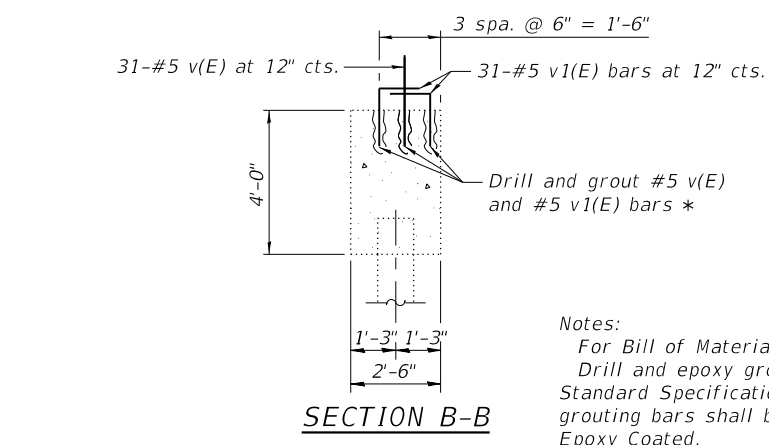
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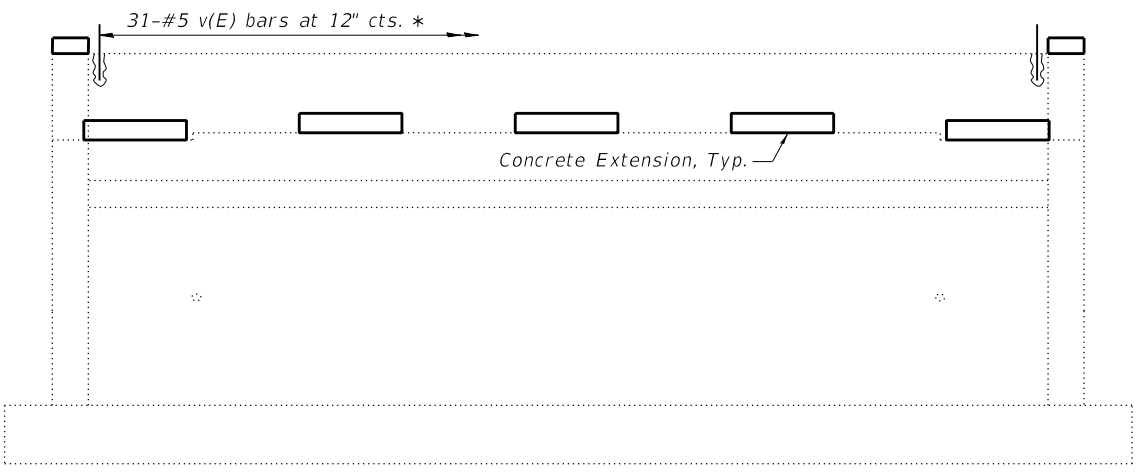
**WINGWALL REINFORCEMENT**



**SIDE ELEVATION**



**PLAN**



**VIEW D-D**

Notes:  
 For Bill of Material and bar details, see Sheet 20 of 21.  
 Drill and epoxy grout bars according to Article 584 of the Standard Specifications. The cost of drilling and epoxy grouting bars shall be included with Reinforcement Bars, Epoxy Coated.  
 Existing reinforcement shall be cut flush with existing concrete surface. Cost included with Concrete Removal.  
 For Concrete Extension details see Sheet 20 of 21.

\* Drill and epoxy grout bars. 9" min. embedment. Space to miss existing bars.  
 \*\* Measured along back face.

**LEGEND**

Existing Concrete Removal



USER NAME = cstokes	DESIGNED - RPW	REVISED -
0680044-72D31-017-West Abutment.dgn	CHECKED - CFS	REVISED -
PLOT SCALE = 0:2.0000 "/> <td>DRAWN - RPW</td> <td>REVISED -</td>	DRAWN - RPW	REVISED -
PLOT DATE = 12/10/2019	CHECKED - KWB	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**WEST ABUTMENT  
 STRUCTURE NO. 068-0044**

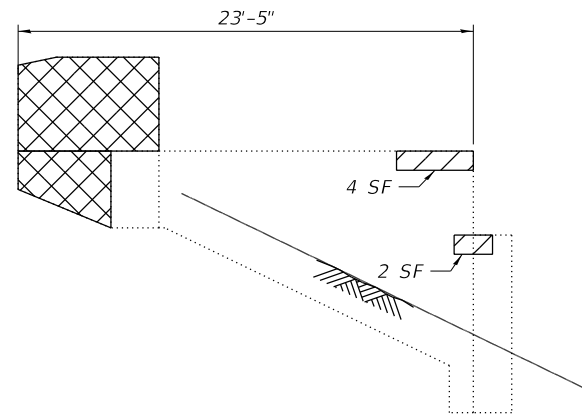
SHEET 17 OF 21 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				

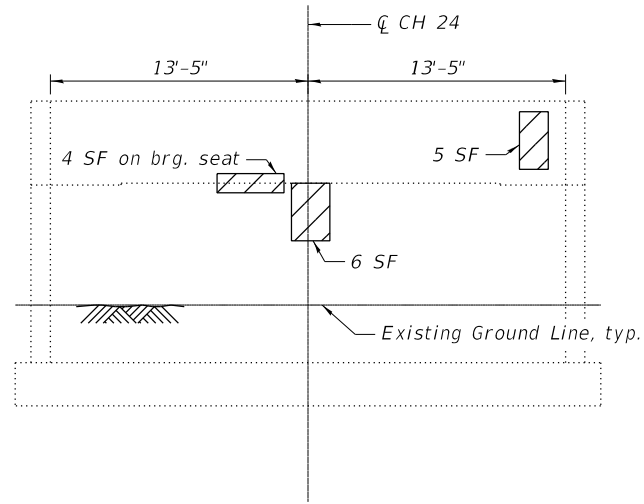




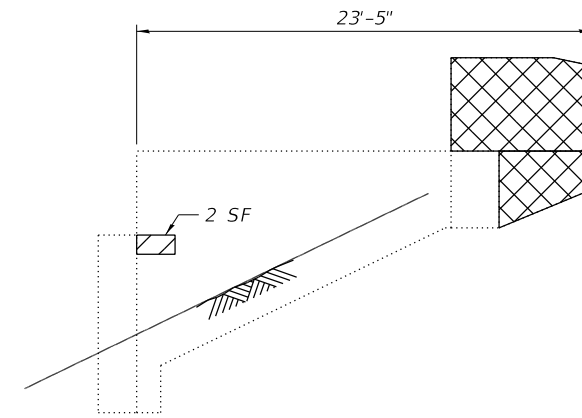
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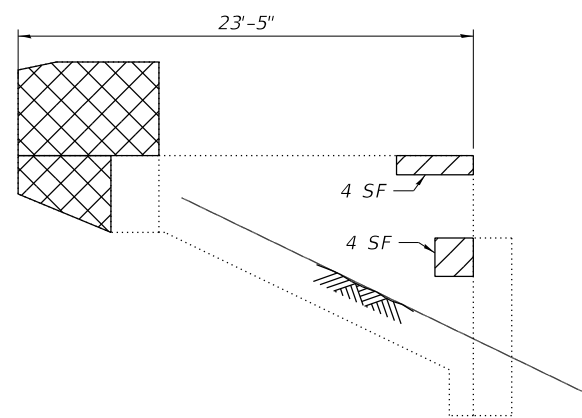
**ELEVATION**  
 (South Wingwall, Looking North)



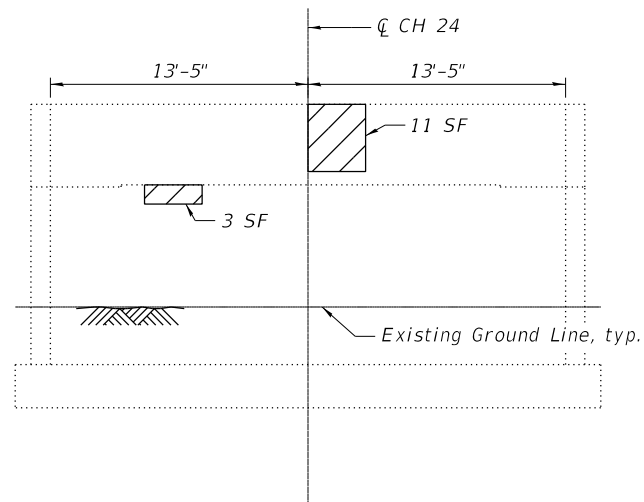
**ELEVATION**  
 (West Abut. Looking West)



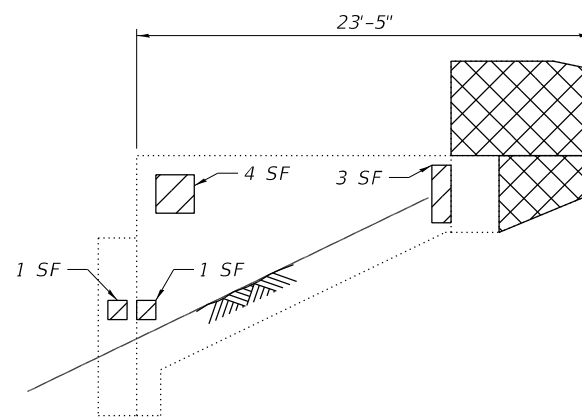
**ELEVATION**  
 (North Wingwall, Looking South)



**ELEVATION**  
 (North Wingwall, Looking South)



**ELEVATION**  
 (East Abut. Looking East)



**ELEVATION**  
 (South Wingwall, Looking North)

**LEGEND**

- Structural Repair of Concrete  
 (Depth Equal to or Less than 5")
- Existing Concrete Removal

Note:  
 Existing reinforcement shall be cut  
 flush with existing concrete surface.  
 Cost included with Concrete Removal.

**BILL OF MATERIAL**

Item	Unit	Total
Structural Repair of Concrete (Depth Equal to or Less than 5")	Sq. Ft.	54



QUIGG ENGINEERING INC

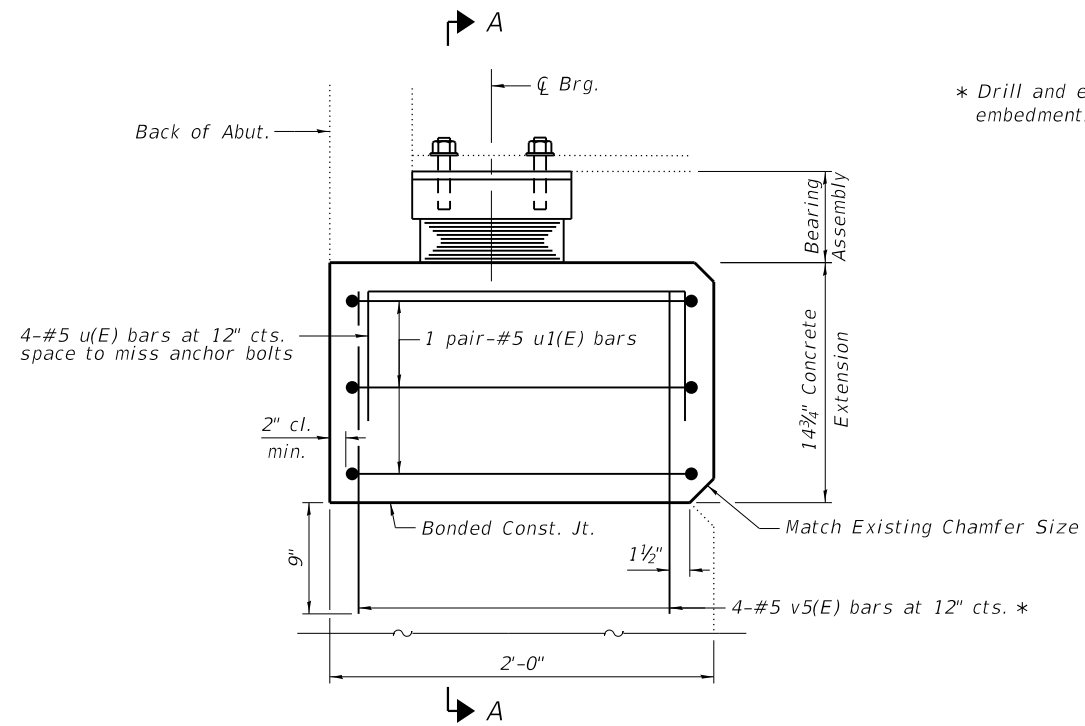
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PLOT DATE =	12/10/2019	CHECKED -	KWB	REVISED -	

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

ABUTMENT REPAIRS  
 STRUCTURE NO. 068-0044

SHEET 19 OF 21 SHEETS

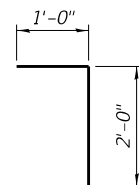
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1768	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	173
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				



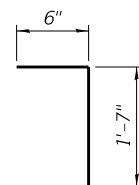
\* Drill and epoxy grout bars. 9" min. embedment. Space to miss existing bars.

Note:  
 Prior to ordering any material, the contractor shall verify in the field all bearing height and shim thickness dimensions.  
 For location of concrete extensions, see Sheets 17 and 18 of 21.  
 Drill and epoxy grout bars according to Article 584 of the Standard Specification. The cost of drilling and epoxy grouting bars shall be included with Reinforcement Bars, Epoxy Coated.

**CONCRETE EXTENSION ELEVATION**  
 (Dimensions at right angles to abutment)



**BAR v1(E)**



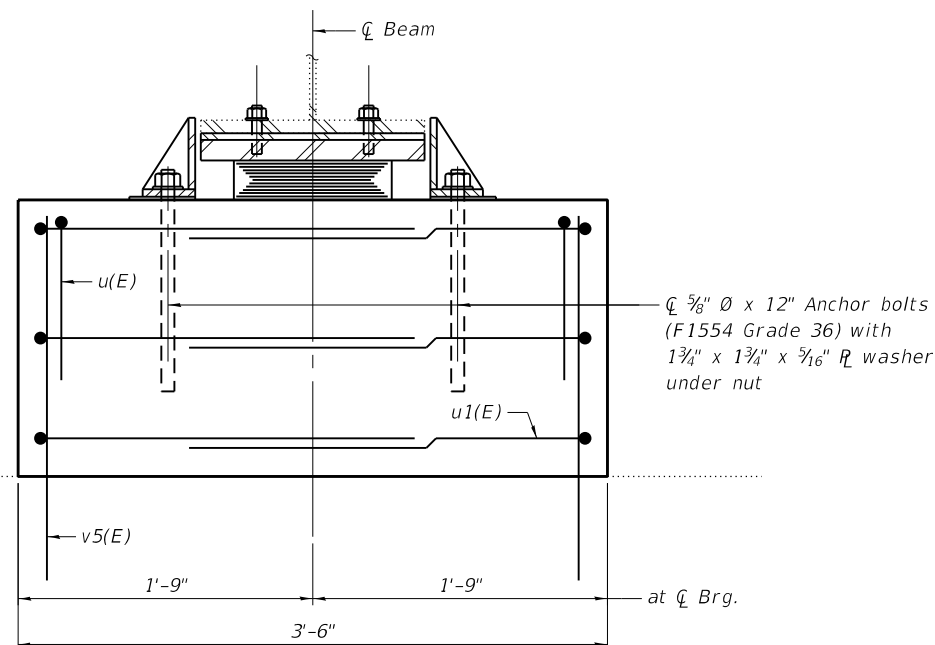
**BAR v2(E)**

**WEST ABUTMENT  
 BILL OF MATERIAL**

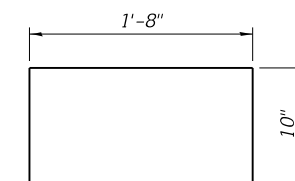
Bar	No.	Size	Length	Shape
h1(E)	4	#4	15'-9"	—
h2(E)	16	#6	6'-2"	—
h3(E)	10	#6	5'-7"	—
v(E)	31	#5	2'-3"	—
v1(E)	62	#5	3'-0"	└
v2(E)	64	#4	2'-1"	└
v3(E)	12	#6	6'-3"	—
v4(E)	2	#6	2'-4"	—
v5(E)	40	#5	1'-10"	—
u(E)	20	#5	3'-4"	┐
u1(E)	30	#5	7'-4"	┐
Concrete Structures			Cu. Yd.	4.7
Reinforcement Bars, Epoxy Coated			Pound	1,200
Sand Backfill			Cu. Yd.	20
Concrete Sealer			Sq. Ft.	304
Concrete Removal			Cu. Yd.	3.7
Structure Excavation			Cu. Yd.	12

**EAST ABUTMENT  
 BILL OF MATERIAL**

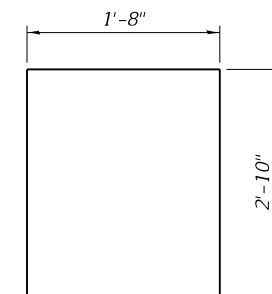
Bar	No.	Size	Length	Shape
h1(E)	4	#4	15'-9"	—
h2(E)	16	#6	6'-2"	—
h3(E)	10	#6	5'-7"	—
v(E)	31	#5	2'-3"	—
v1(E)	62	#5	3'-0"	└
v2(E)	64	#4	2'-1"	└
v3(E)	12	#6	6'-3"	—
v4(E)	2	#6	2'-4"	—
v5(E)	40	#5	1'-10"	—
u(E)	20	#5	3'-4"	┐
u1(E)	30	#5	7'-4"	┐
Concrete Structures			Cu. Yd.	4.7
Reinforcement Bars, Epoxy Coated			Pound	1,200
Sand Backfill			Cu. Yd.	21
Concrete Sealer			Sq. Ft.	305
Concrete Removal			Cu. Yd.	3.7
Structure Excavation			Cu. Yd.	12



**SECTION A-A**



**BAR u(E)**



**BAR u1(E)**

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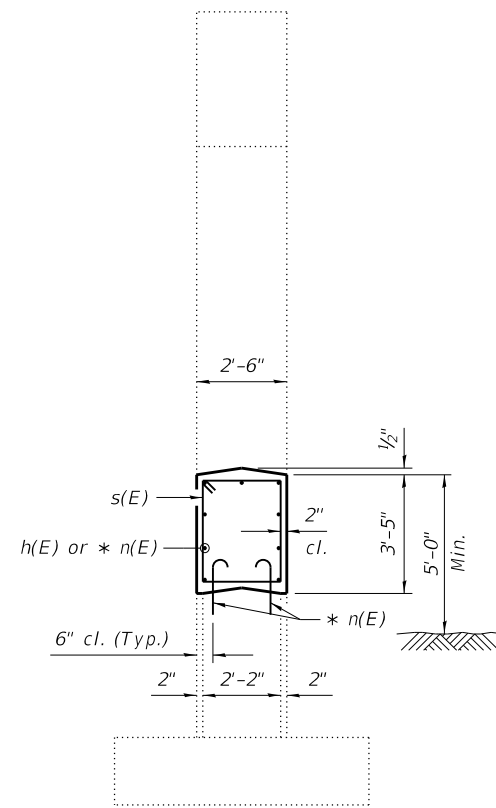
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ABUTMENT DETAILS  
 STRUCTURE NO. 068-0044**

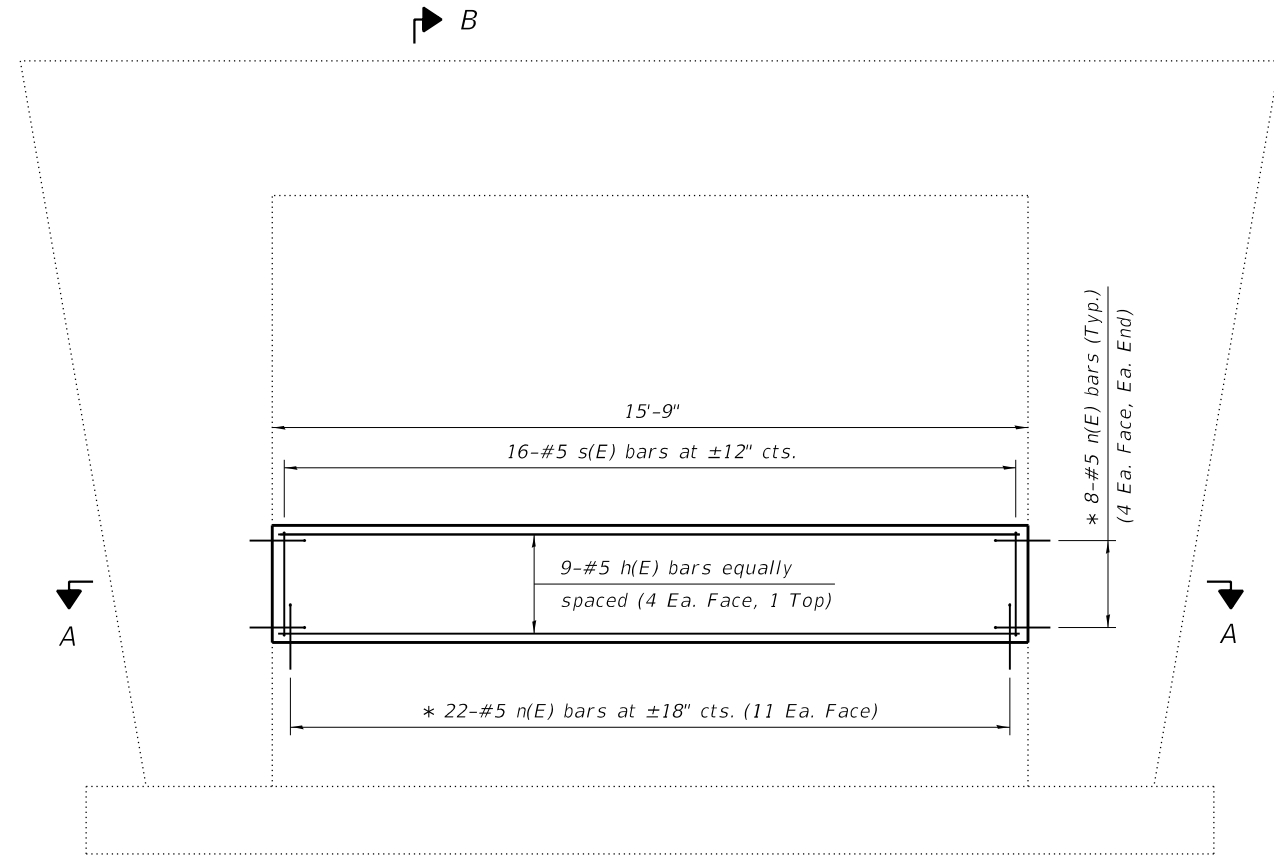
SHEET 20 OF 21 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1768	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	174
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				

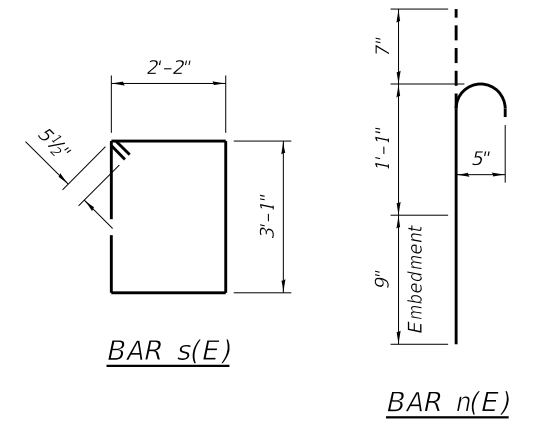
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**SECTION B-B**



**ELEVATION**

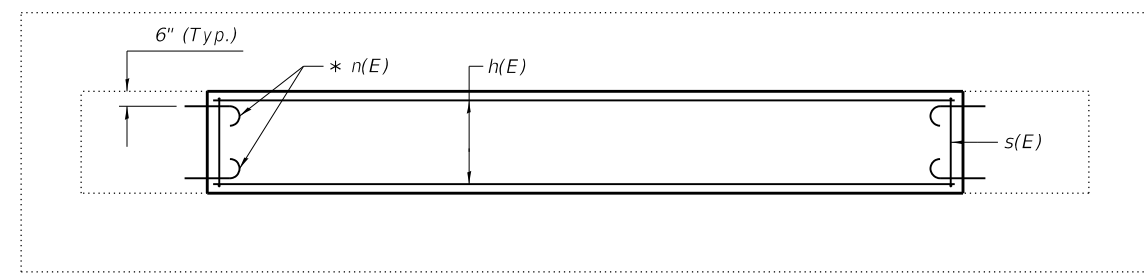


**BAR s(E)**

**BAR n(E)**

\* Drill and epoxy grout bars. 9" min. embedment. Space to miss existing bars.

Note:  
 Drill and epoxy grout bars according to Article 584 of the Standard Specifications. The cost of drilling and epoxy grouting bars shall be included with Reinforcement Bars, Epoxy Coated.



**SECTION A-A**

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
h(E)	9	#5	15'-6"	—
n(E)	38	#5	2'-5"	U
s(E)	16	#5	11'-5"	□
Concrete Structures			Cu. Yd.	5.0
Reinforcement Bars, Epoxy Coated			Pound	440



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0680044-72D31-021-Pier.dgn	CHECKED - CFS	REVISED -
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PLOT DATE = 12/10/2019	CHECKED - KWB	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PIER CRASHWALL EXTENSION  
 STRUCTURE NO. 068-0044**

SHEET 21 OF 21 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1768	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	175
CONTRACT NO. 72D31				

ILLINOIS FED. AID PROJECT

EXISTING STRUCTURE - S.N. 068-0047, originally built in 1971, is a two-span, continuous steel, multi-girder structure with vaulted abutments and a 3-columned pier.

Structure is to be repaired as detailed in these plans using stage construction. One lane of traffic to be maintained at all times.

No Salvage

### TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Structures	Cu. Yd.		5.0	5.0
Reinforcement Bars, Epoxy Coated	Pound		500	500
Silicone Joint Sealer, 2.75"	Feet	74		74

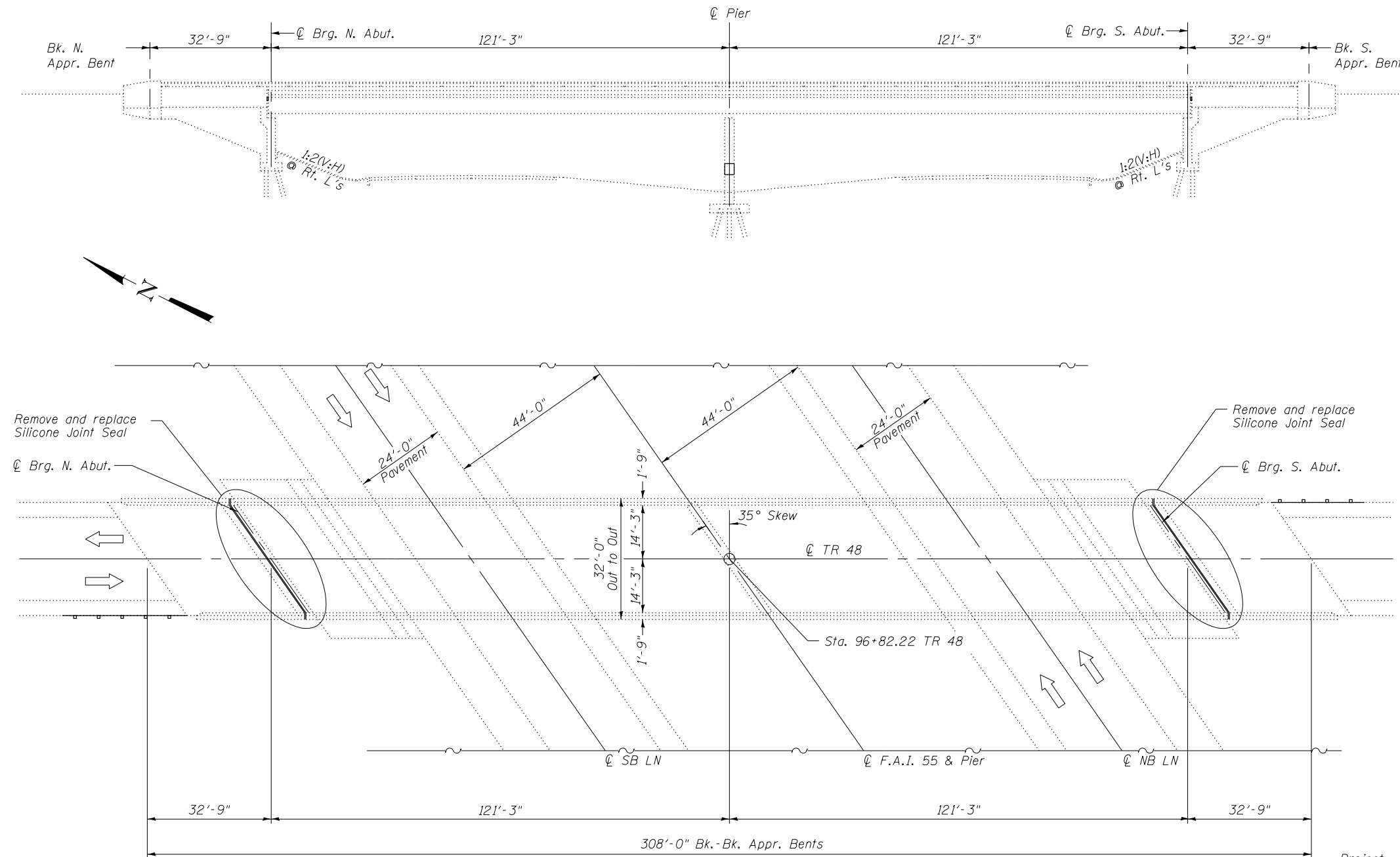
### GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and detail affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished that the unit price bid for the work.

The Contractor shall provide, erect and maintain all the necessary barricades, cones, drums and lights for the warning and protection of traffic along I-55, as required by IDOT Standard 701101, for any activities that require workers, equipment or materials to be within fifteen feet of the existing edge of pavement. All other activities on I-55 that are beyond these limits shall be completed in accordance with IDOT Standard 701106

The Contractor shall provide traffic control along TR 48 in accordance with IDOT Standard 701316, when temporary lane closures are required to remove and replace the existing preformed joint seals. Flaggers will be utilized instead of installing temporary signals and no nighttime lane closures will be allowed. Flaggers will be used in accordance with IDOT Standard 701201 during temporary daytime closures.

No field welding is permitted except as specified in the contract documents. Reinforcement bars designated (E) shall be epoxy coated.



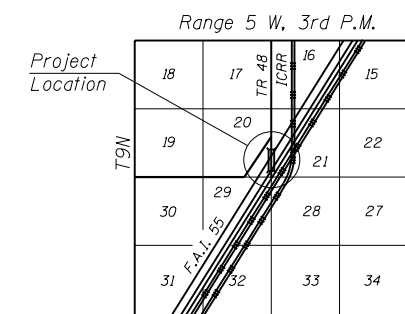
*Michael D. Cima* 12/10/2019  
 Michael D. Cima, Illinois S.E. 081-005984 Date  
 Expires 11/30/2020

**DESIGN SPECIFICATIONS**  
 2002 AASHTO Standard Specifications  
 for Highway Bridges

**DESIGN STRESSES**  
**FIELD UNITS (NEW CONSTR.)**  
 f'c = 3,500 psi  
 fy = 60,000 psi (Reinforcement)

**SCOPE OF WORK**  
 1. Replacement of expansion joint seals at each abutment  
 2. Raise pier crash wall

**INDEX OF SHEETS**  
 1. General Plan and Elevation  
 2. Stage Construction Details  
 3. Joint Sealer Details  
 4. Pier Crashwall Extensions



**GENERAL PLAN & ELEVATION**  
**TR 48 OVER F.A.I. RTE. 55**  
**SECT. (68-1,3 RS-3, 68-2 RS-5) BR**  
**MONTGOMERY COUNTY**  
**STATION 96+82.22**  
**STRUCTURE NO. 068-0047**

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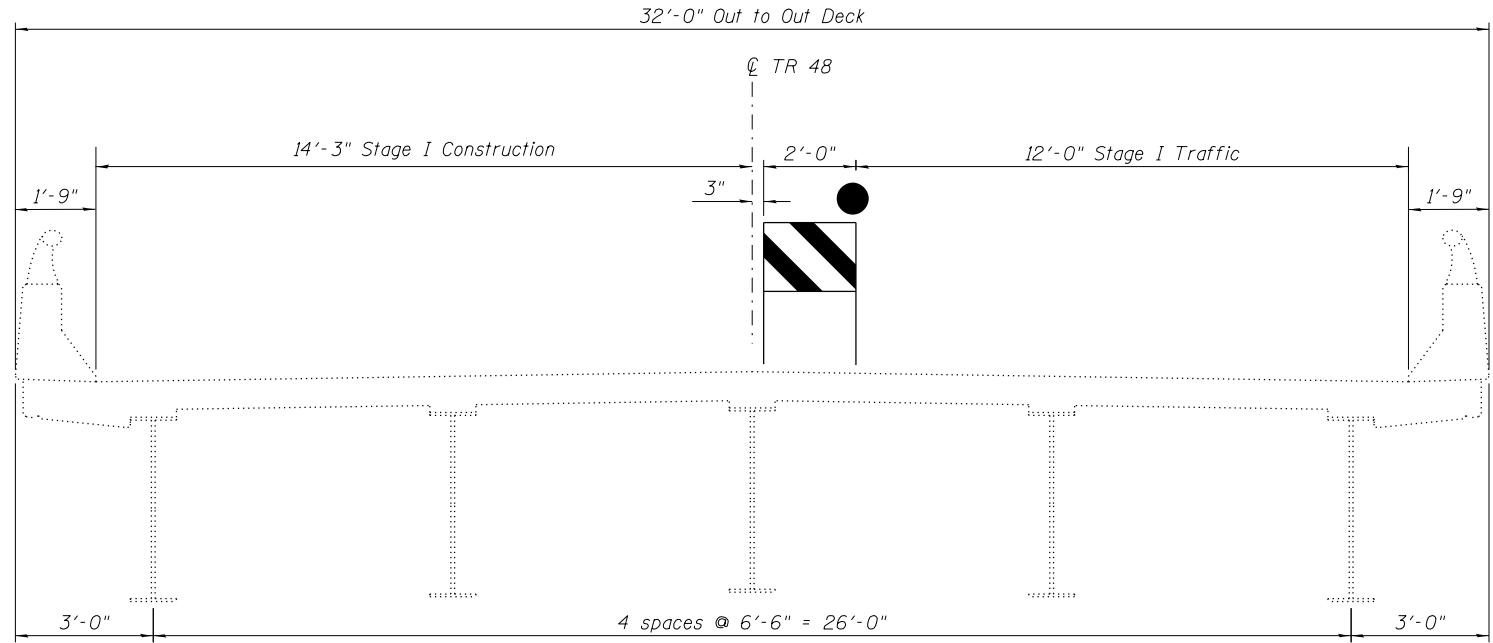
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION  
 STRUCTURE NO. 068-0047

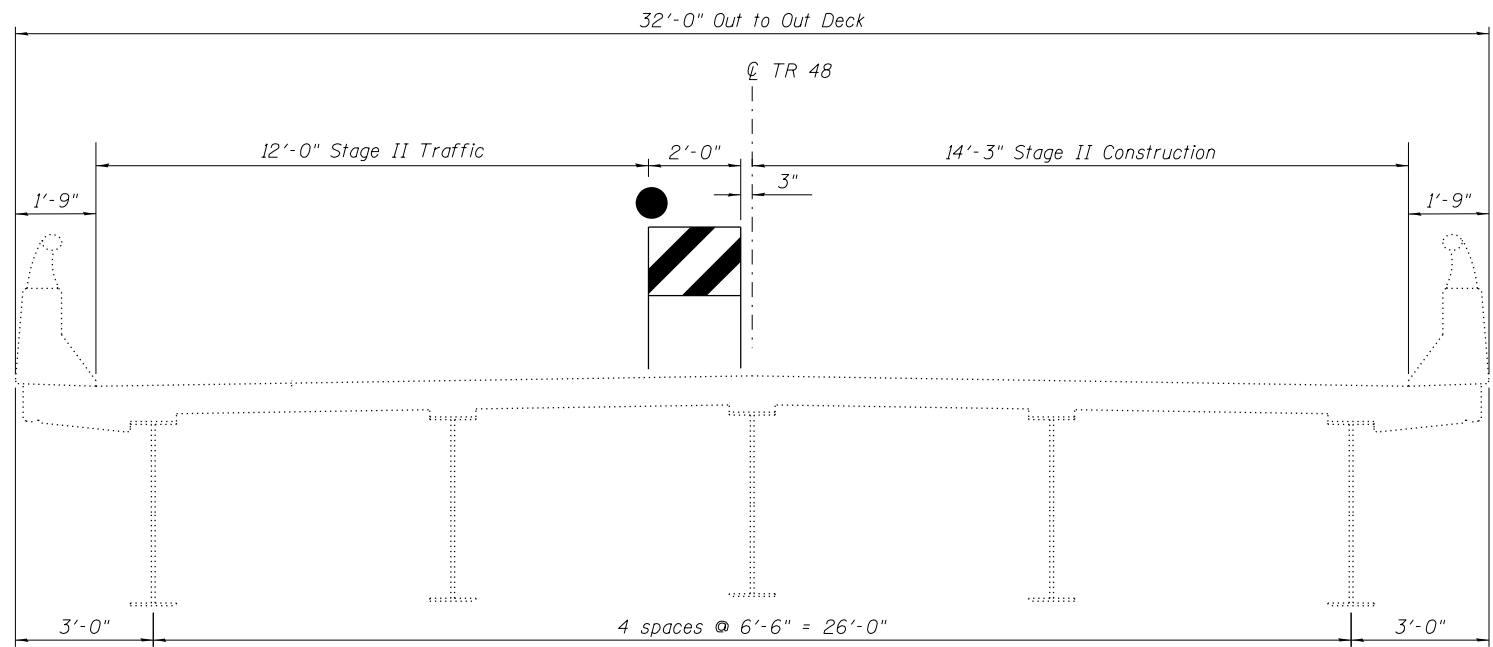
SHEET NO. 1 OF 4 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			CONTRACT NO. 72031	
ILLINOIS FED. AID PROJECT				

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 MODEL : Default  
 PLOT DRIVER : IODT\_PDF.plt



Stage I Construction  
(Looking South)



Stage II Construction  
(Looking South)



USER NAME : cstokes	DESIGNED - SDM	REVISED
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PLOT SCALE : 0:2.0000 '1' / in.	DRAWN - SDM	REVISED
PLOT DATE : 12/10/2019	CHECKED - MDC	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

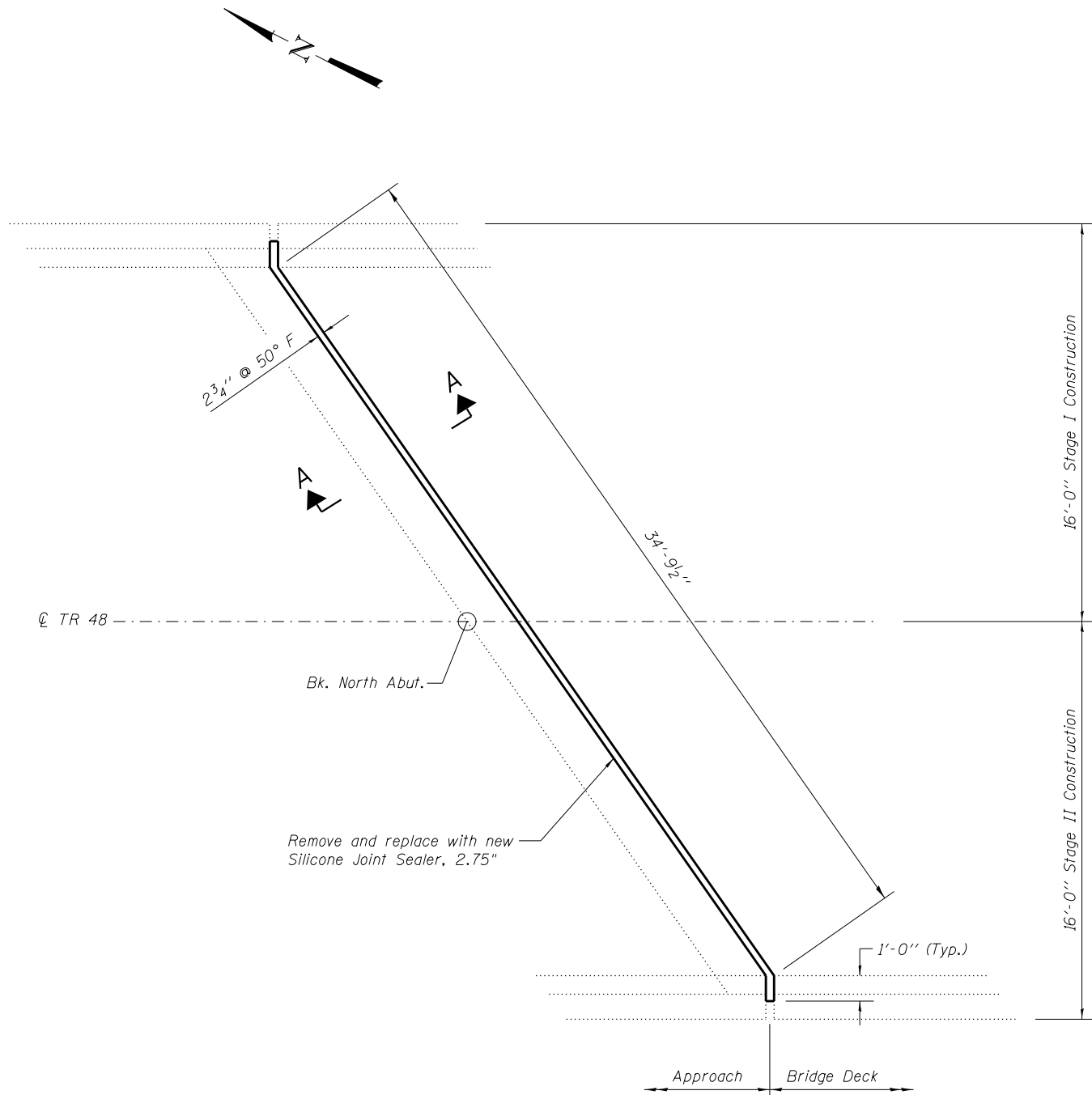
STAGE CONSTRUCTION DETAILS  
STRUCTURE NO. 068-0047

SHEET NO. 2 OF 4 SHEETS

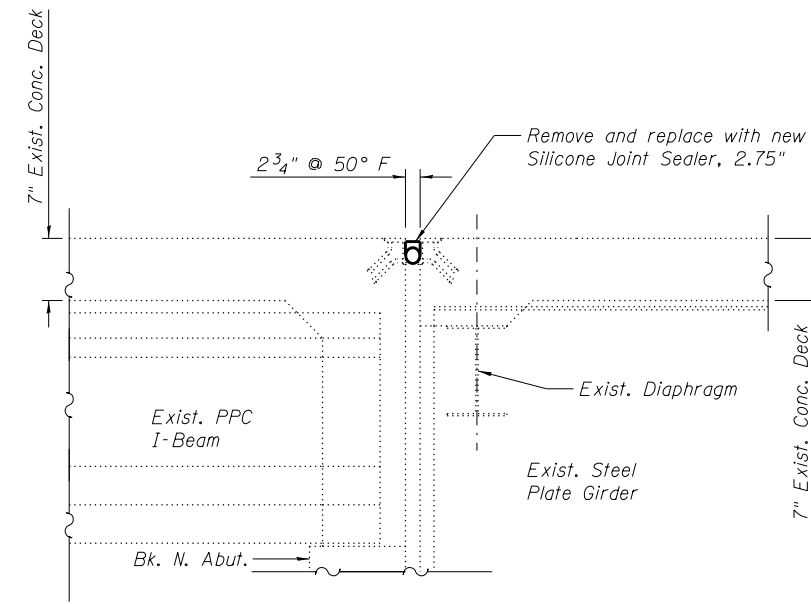
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	177
CONTRACT NO. 72D31				

ILLINOIS FED. AID PROJECT

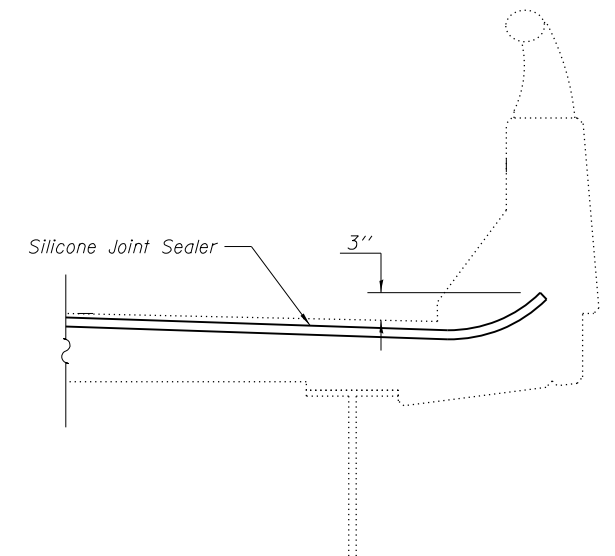
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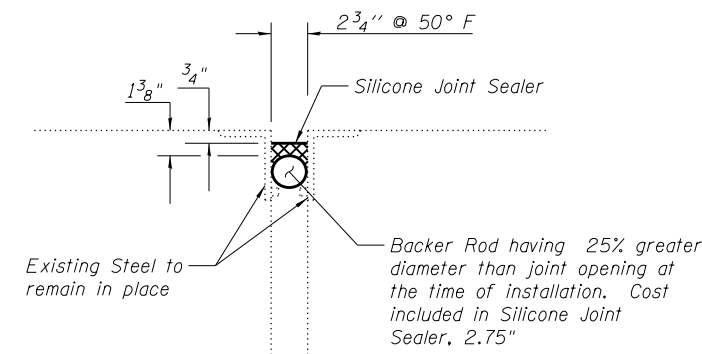
**PLAN**  
 (North Abut. Shown, South Abut. Similar)



**SECTION A-A**



**TYPICAL END OF SEAL TREATMENT**



**SILICONE JOINT SEALER DETAIL**  
 (Proposed)

**BILL OF MATERIAL**

Item	Unit	Total
Silicone Joint Sealer, 2.75"	Foot	74



USER NAME = cstokes	DESIGNED - SDM	REVISED
0680047-72D31-003-ExpJDetails.dgn	CHECKED - RPW	REVISED
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PLOT DATE = 12/10/2019	CHECKED - MDC	REVISED

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

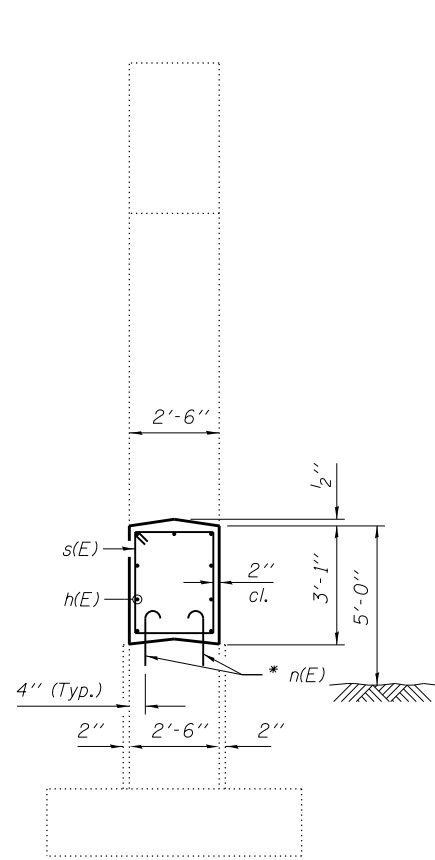
**JOINT SEALER DETAILS  
 STRUCTURE NO. 068-0047**

SHEET NO. 3 OF 4 SHEETS

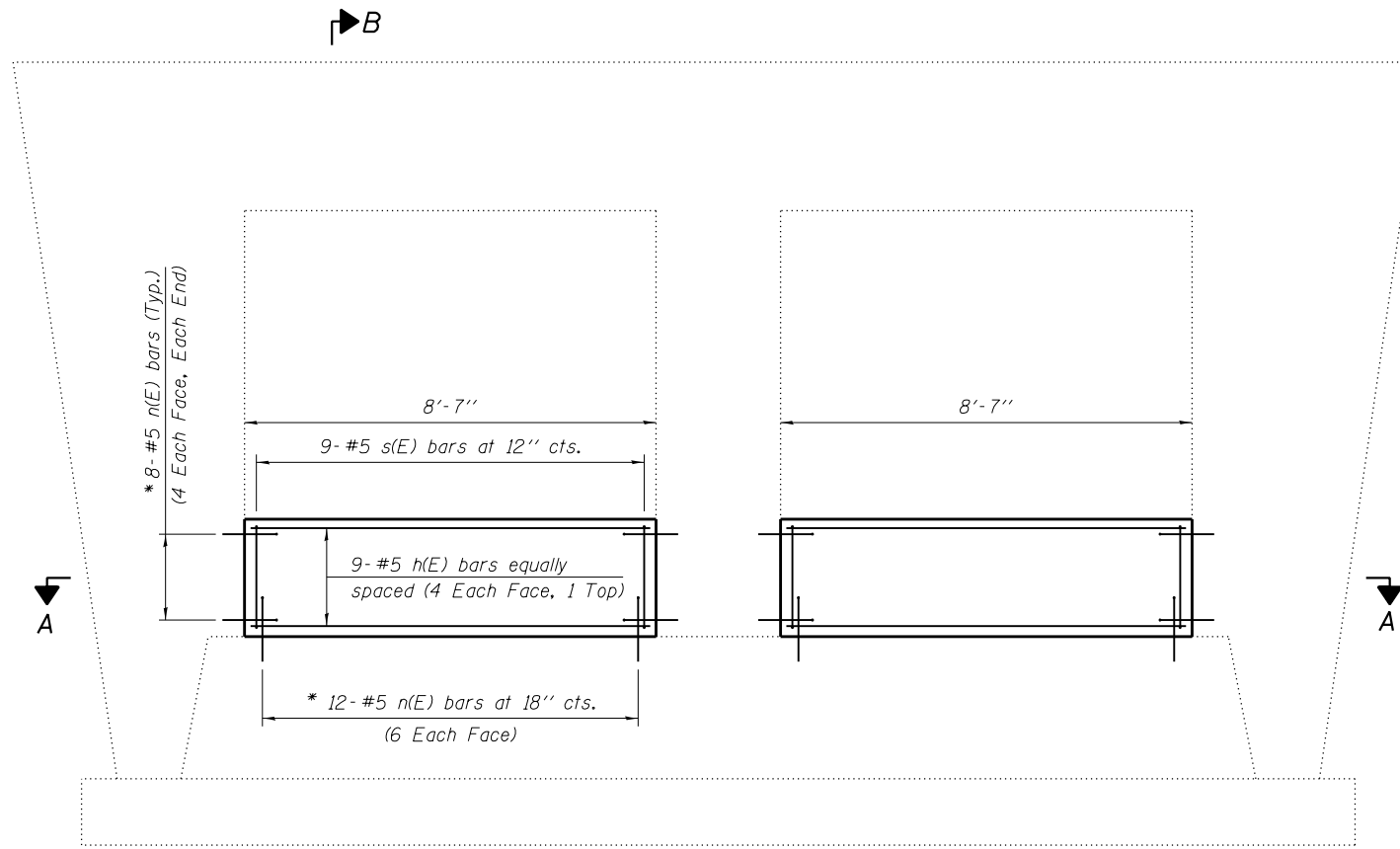
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1.3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	178
CONTRACT NO. 72D31				

ILLINOIS FED. AID PROJECT

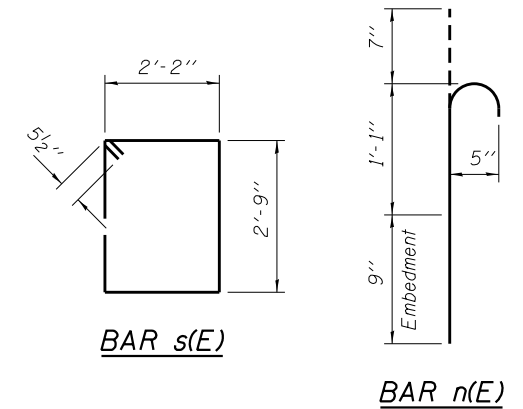
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SECTION B-B



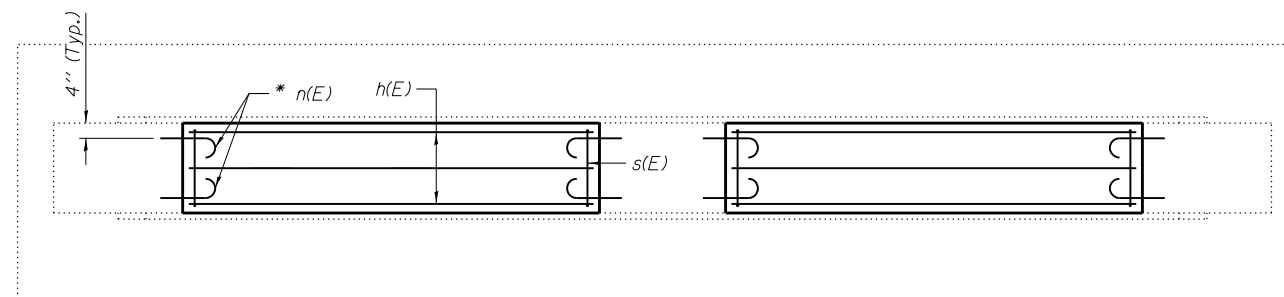
ELEVATION



BAR s(E)

BAR n(E)

\* Epoxy grout n(E) bars in 9" min. holes according to Article 584 of the Standard Specifications. The cost of epoxy grouting threaded rods shall be included with Reinforcement Bars, Epoxy Coated.



SECTION A-A

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	18	#5	8'-3"	—
n(E)	56	#5	2'-5"	⌋
s(E)	18	#5	10'-9"	□
Item		Unit	Total	
Concrete Structures		Cu. Yd.	5.0	
Reinforcement Bars, Epoxy Coated		Pound	500	



USER NAME = cstokes	DESIGNED - SDM	REVISED
0680047-72D31-004-Pier-CWExtension.dgn	CHECKED - RPW	REVISED
PLOT SCALE = 0:2.0000 '1' / in.	DRAWN - SDM	REVISED
PLOT DATE = 12/10/2019	CHECKED - MDC	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PIER CRASHWALL EXTENSION  
STRUCTURE NO. 068-0047

SHEET NO. 4 OF 4 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(68-1,3 RS-3, 68-2 RS-5) BR	MONTGOMERY	307	179
CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	

Benchmark: B.M. #91 Chiseled "□" on NW approach wall to S.B. bridge, Sta. 92+03.90, 78.6' RT, Elev. = 704.52  
 B.M. #92 Chiseled "□" on SW approach wall to N.B. bridge, Sta. 95+27.70, 37.5' LT, Elev. = 705.30

Existing Structures: S.N. 068-0048 (S.B.) and S.N. 068-0049 (N.B.) were originally built in 1973 as F.A.I. 55, Section 68-1VB-1. The back-to-back abutment length is 227'-10" and the out-to-out deck width is 42'-0". Each structure consists of a three span, welded plate girder superstructure supported by concrete stub abutments founded on concrete piles and concrete hammerhead piers founded on concrete pile supported footings. In 2002, the expansion joints were replaced and a new overlay was provided. Concrete deck to be removed and replaced.

Traffic to be maintained using temporary median crossover.

No Salvage.

Traffic Barrier Terminal Type 6 (Std. 631031). At all approach ends and northwest corner of (SN 068-0049).

Note: Up to 1/4 inch may be ground off the bridge deck and the bridge approach slabs.

DESIGN SPECIFICATIONS (New Const.)  
 2002 AASHTO Std. Spec. for Highway Bridges, 17th Edition

DESIGN STRESSES

FIELD UNITS (New Construction)  
 f'c = 4,000 psi (Superstructure)  
 f'c = 3,500 psi (Substructure)  
 fy = 60,000 psi (Reinforcement)  
 fy = 50,000 psi (M 270 Grade 50)

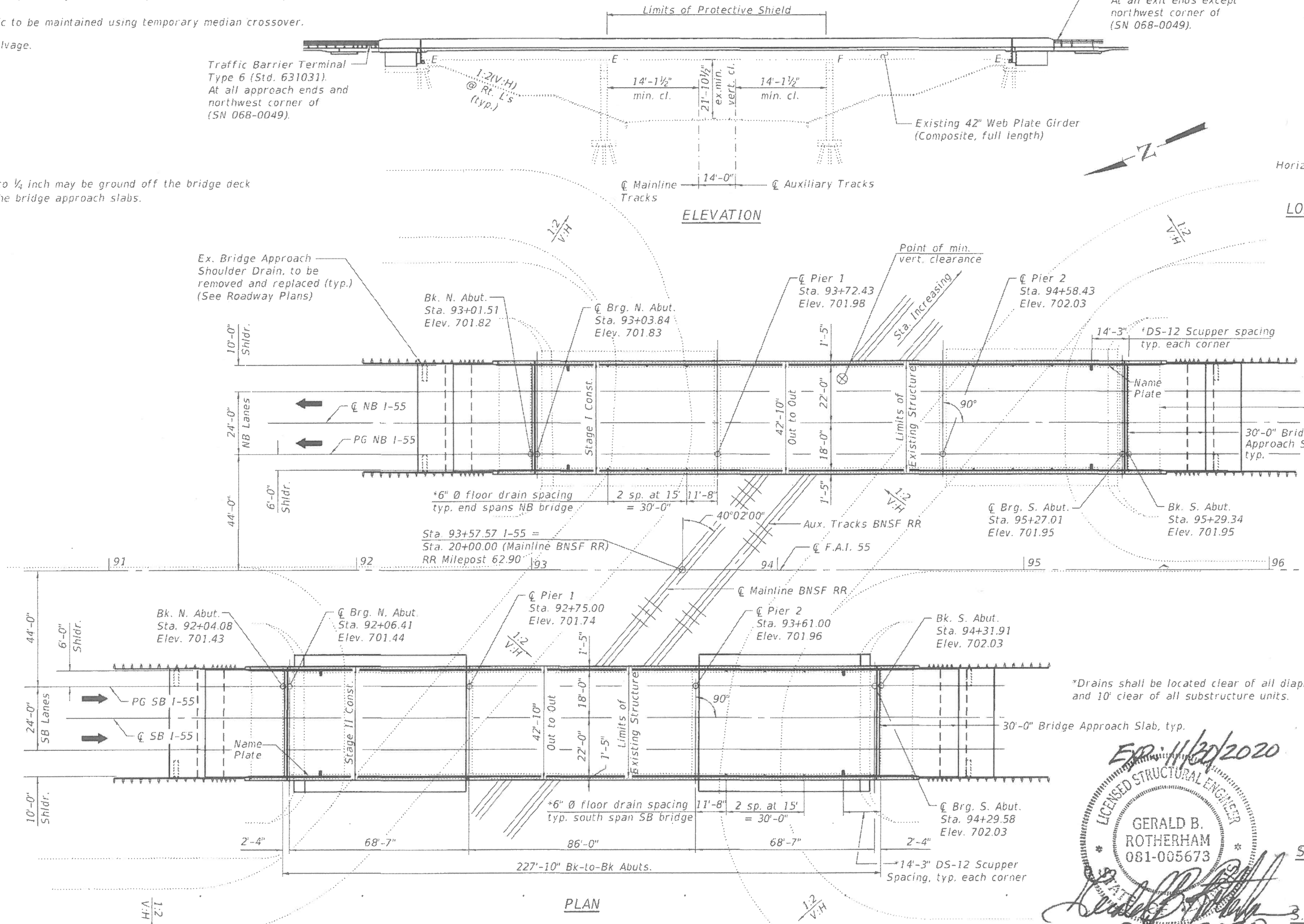
FIELD UNITS (Exist. Construction)  
 f'c = 3,000 psi (deck slab)  
 f'c = 3,500 psi (parapets, sub)  
 fy = 40,000 psi (Reinforcement)  
 fy = 36,000 psi (Structural Steel)

SEISMIC DATA

Seismic Performance Category (SPC) = A  
 Horizontal Bedrock Acceleration Coefficient (A) = 0.068g  
 Site Coefficient (S) = 1.2

LOADING HS20-44 & ALT (New Const.)

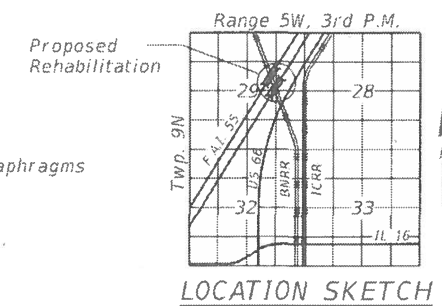
Allow 25 psf for future wearing surface



See Roadway Plans for the Removal of the Ex. Bridge Approach Pavement (typ.)

APPROVED  
 FOR STRUCTURAL ADEQUACY ONLY

*Gerald B. Rotherham*  
 ENGINEER OF BRIDGES AND STRUCTURES



\*Drains shall be located clear of all diaphragms and 10' clear of all substructure units.

EXP: 11/30/2020  
 LICENSED STRUCTURAL ENGINEER  
 GERALD B. ROTHERHAM  
 081-005673  
*Gerald B. Rotherham*  
 01/09/2020

GENERAL PLAN & ELEVATION  
 F.A.I. RTE. 55 OVER B.N.S.F. R.R.  
 SECTION (68-1,3 RS-3, 68-2 RS-5)BR  
 MONTGOMERY COUNTY  
 STATION 93+57.57  
 S.N. 068-0048 (S.B.) & 068-0049 (N.B.)

FILE NAME = 001-0P&E	USER NAME =	DESIGNED - CMV	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL PLAN AND ELEVATION STRUCTURE NO. 068-0048(SB) & 068-0049(NB)	F.A.I. RTE. 55	SECTION 68-1,3 RS-3, 68-2 RS-5)BR	COUNTY MONTGOMERY	TOTAL SHEETS 307	SHEET NO. 180
BACON   FARMER   WORKMAN ENGINEERS & TESTERS, INC.	PLOT SCALE =	CHECKED - CBR	REVISED -			CONTRACT NO. T2D31				
110 NORTH 1000 WEST STREET MONTGOMERY, ILLINOIS 61801 PHONE: 314-635-1000	PLOT DATE = 1/6/2020	DRAWN - BJV	REVISED -			ILLINOIS FED. AID PROJECT				
		CHECKED - CMV	REVISED -							



**SCOPE OF WORK**

1. Remove and replace existing concrete deck.
2. Make new deck composite by installing shear studs.
3. Replace existing expansion bearings at abutments with elastomeric bearings.
4. Remove and replace abutment backwalls.
5. Remove and replace expansion joints with strip seal expansion joint.
6. Repair broken and undermined portions of concrete slopewall.
7. Remove and reconstruct existing wingwalls to clear the underside of the proposed approach slabs.
8. Repair steel beams and diaphragms as necessary.
9. Clean and paint existing structural steel under separate "Paint Only" contract.

**GENERAL NOTES**

Fasteners shall be high strength bolts. Bolts  $\frac{3}{4}$  in.  $\emptyset$ , holes  $1\frac{1}{16}$  in.  $\emptyset$ , unless otherwise noted.

No field welding is permitted except as specified in the contract documents.

Reinforcement bars designated (E) shall be epoxy coated.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer.

Any cracks that cannot be removed by grinding  $\frac{1}{4}$  inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

If the Contractor elects to use cantilever forming brackets on the exterior beams, the brackets shall be placed at the same locations as required for the hardwood blocks in Article 503.06(b) of the Standard Specifications. If additional cantilever forming brackets are required, hardwood blocking shall be wedged between the exterior and first interior beam at each of these additional bracket locations.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Concrete Sealer shall be applied to the front faces of the backwall.

Existing structural steel shall only be cleaned and painted as required by the Special Provision for "Cleaning and Painting Contact Surface Areas of Existing Steel Structures."

All new Structural Steel and bearing assemblies shall be hot-dip galvanized. See Special Provision for "Hot Dip Galvanizing for Structural Steel."

Cleaning and field painting of structural steel shall be done under a separate painting contract.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

Cost of removal and re-installation of all members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included with Furnishing and Erecting Structural Steel.

Existing reinforcement bars extending into the removal areas shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

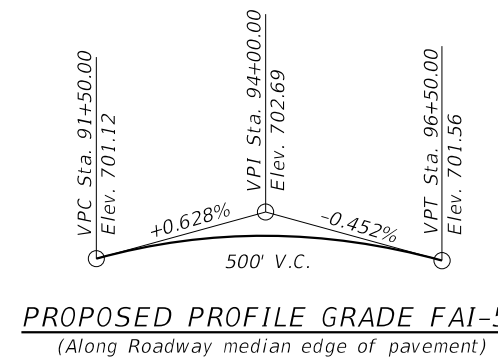
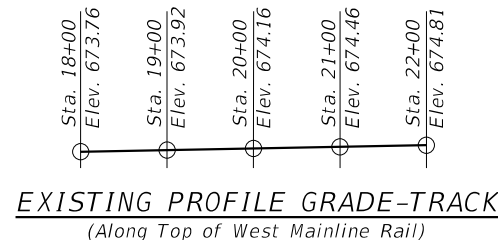
STATION 93+57.57  
RE-BUILT 20\_\_ BY  
STATE OF ILLINOIS  
F.A.I. RT. 55 SEC. (68-1,3 RS-3, 68-2 RS-5)BR  
LOADING HS 20-44 & ALT.  
STR. NO. 068-0048

**NAME PLATE**  
See Std. 515001

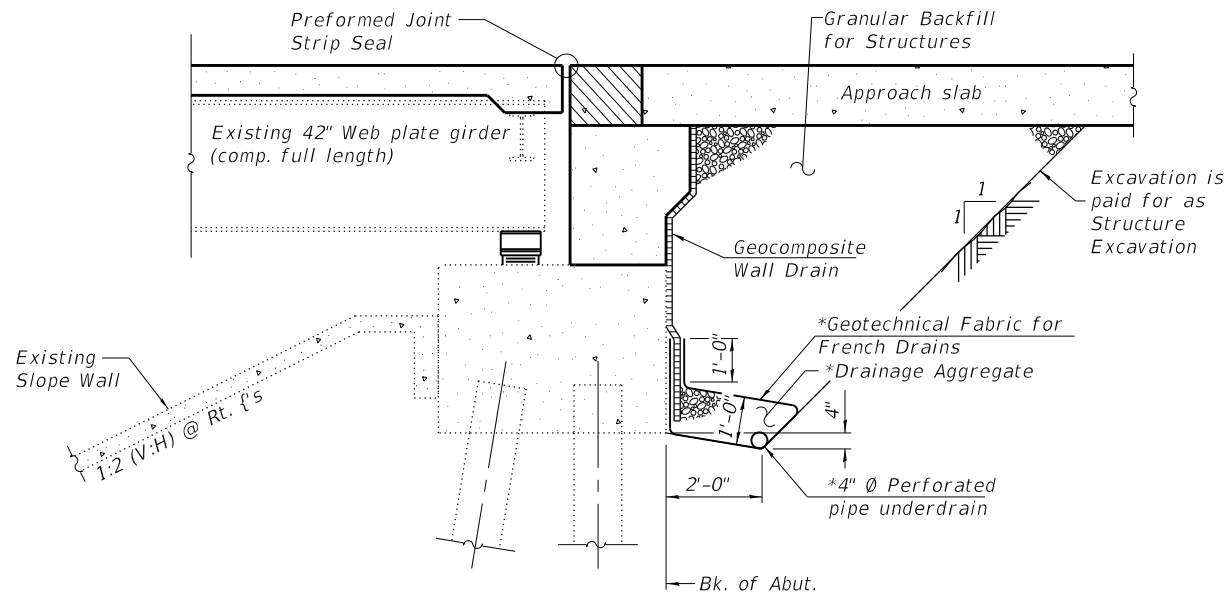
STATION 93+57.57  
RE-BUILT 20\_\_ BY  
STATE OF ILLINOIS  
F.A.I. RT. 55 SEC. (68-1,3 RS-3, 68-2 RS-5)BR  
LOADING HS 20-44 & ALT.  
STR. NO. 068-0049

**NAME PLATE**  
See Std. 515001

Existing Name Plates shall be cleaned and relocated next to new Name Plates. Cost included with Name Plates.



The profile grade shows the final elevations after grinding.



**SECTION THRU ABUTMENT**

(Horiz. dim. @ Rt. f's)

\*Included in the cost of Pipe Underdrains for Structures (See Special Provisions)

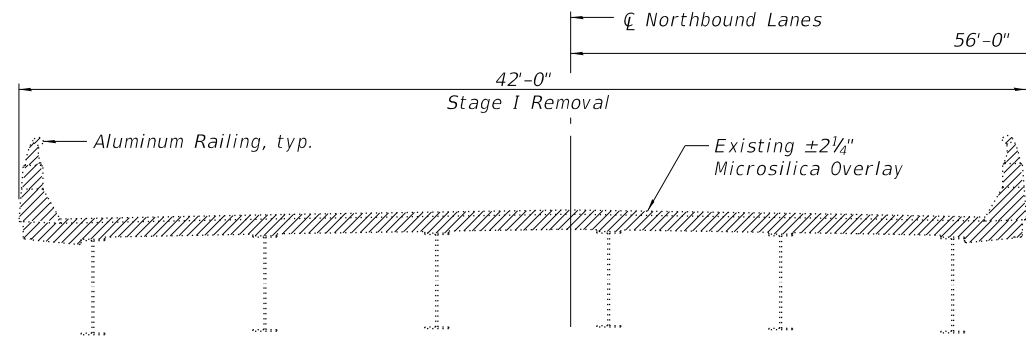
Note:  
All drainage system components shall extend from inside face to inside face of existing wingwalls except outlet pipes shall extend until intersecting with the side or front slopes. The outlet pipes shall be connected to the 4" perforated pipe drain and placed under the existing wingwalls. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).

**TOTAL BILL OF MATERIAL (BOTH STRUCTURES)**

ITEM	UNIT	SUPER	SUB	TOTAL
Removal of Existing Concrete Deck	Each	2		2
Concrete Removal	Cu. Yd.		84.4	84.4
Slope Wall Repair	Sq. Yd.		265	265
Protective Shield	Sq. Yd.	780		780
Structure Excavation	Cu. Yd.		384.0	384.0
Floor Drains	Each	24		24
Concrete Structures	Cu. Yd.		116.0	116.0
Concrete Superstructure	Cu. Yd.	724.8		724.8
Protective Coat	Sq. Yd.	3064		3064
Concrete Superstructure (Approach Slab)	Cu. Yd.	237.6		237.6
Furnishing and Erecting Structural Steel	Pound	7020		7020
Stud Shear Connectors	Each	9240		9240
Reinforcement Bars, Epoxy Coated	Pound	256950	7110	264060
Controlled Low Strength Material	Cu. Yd.		176.7	176.7
Name Plates	Each	2		2
Preformed Joint Strip Seal	Foot	168		168
Elastomeric Bearing Assembly, Type 1	Each	24		24
Anchor Bolts, $\frac{3}{4}$ "	Each	48		48
Geocomposite Wall Drain	Sq. Yd.		146	146
Concrete Sealer	Sq. Ft.		761	761
Structural Steel Removal	Pound	2120		2120
Structural Steel Repair	Pound	1460		1460
Bridge Deck Grooving (Longitudinal)	Sq. Yd.	1528		1528
Granular Backfill for Structures	Cu. Yd.		299.8	299.8
Drainage Scuppers, DS-12	Each	8		8
Diamond Grinding (Bridge Section)	Sq. Yd.	2532		2532
Jack and Remove Existing Bearings	Each	24		24
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq. Ft.		34.3	34.3
Pipe Underdrains for Structures 4"	Foot		248	248

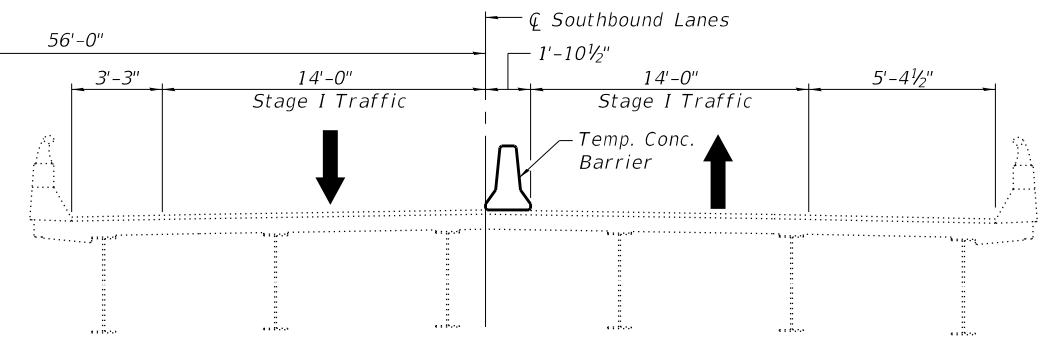
**INDEX OF SHEETS**

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- 2 General Data
- 3 Stage Construction Details
- 4-8 Top of Slab Elevations
- 9-12 Top of Approach Slab Elevations
- 13 Superstructure (NB)
- 14-15 Superstructure Details (NB)
- 16 Superstructure (SB)
- 17-18 Superstructure Details (SB)
- 19 Preformed Joint Strip Seal
- 20 Drainage Scupper DS-12
- 21-22 Bridge Approach Slab Details (NB)
- 23-24 Bridge Approach Slab Details (SB)
- 25 Concrete Parapet Slipforming Option
- 26 Structural Steel Framing (NB)
- 27 Structural Steel Framing (SB)
- 28-29 Structural Steel Repairs (NB)
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- 31-32 Bridge Bearing Replacement Details
- 33 Abutment Removal Details
- 34 North Abutment (NB)
- 35 South Abutment (NB)
- 36 North Abutment (SB)
- 37 South Abutment (SB)
- 38 Slope Wall Repairs
- 39 Concrete Repairs (NB)
- 40 Concrete Repairs (SB)

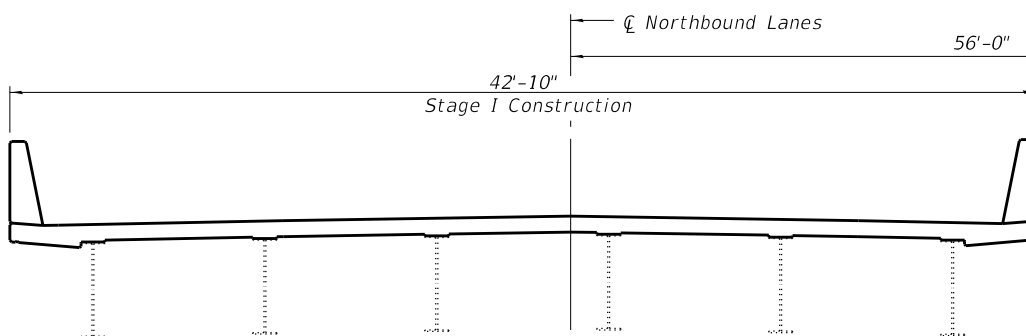


▨ - Indicates Limits of Removal of Existing Concrete Deck

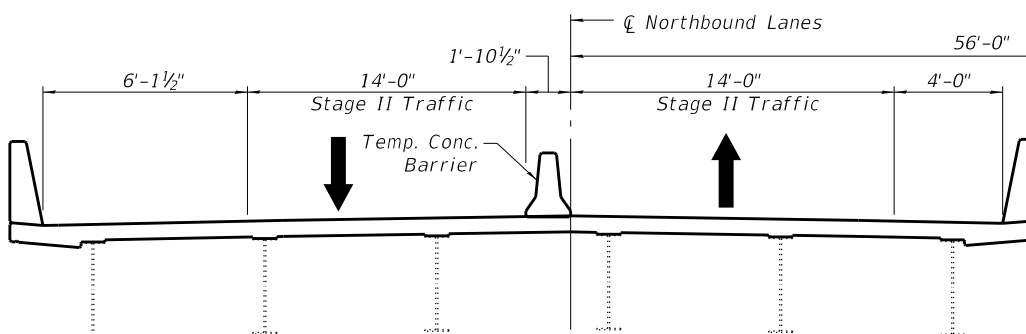
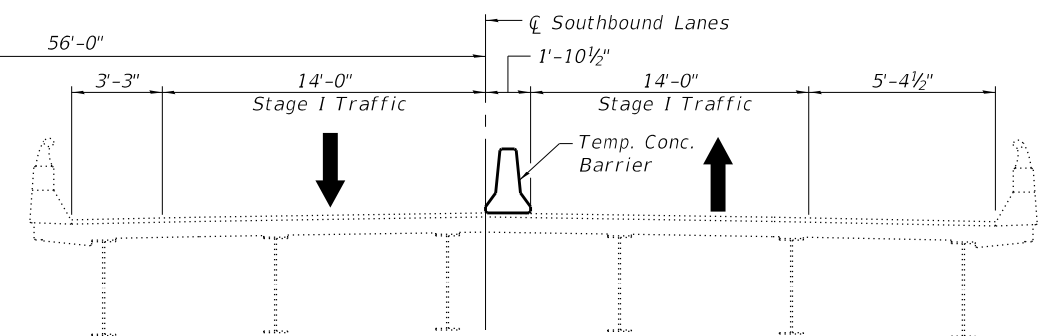
**STAGE I REMOVAL & STAGE I TRAFFIC**  
(Looking South)



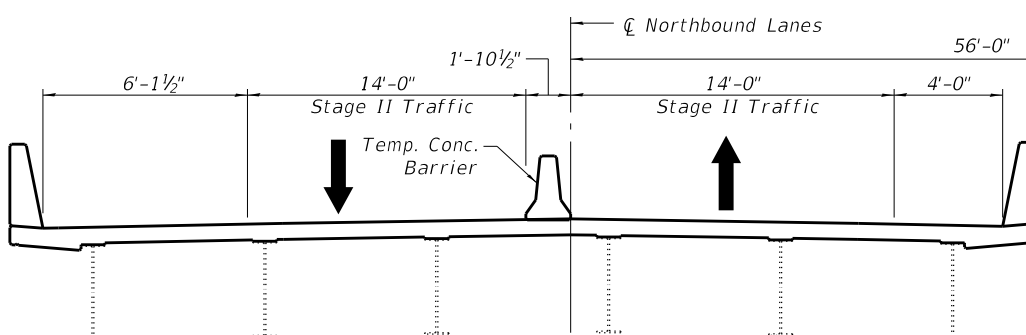
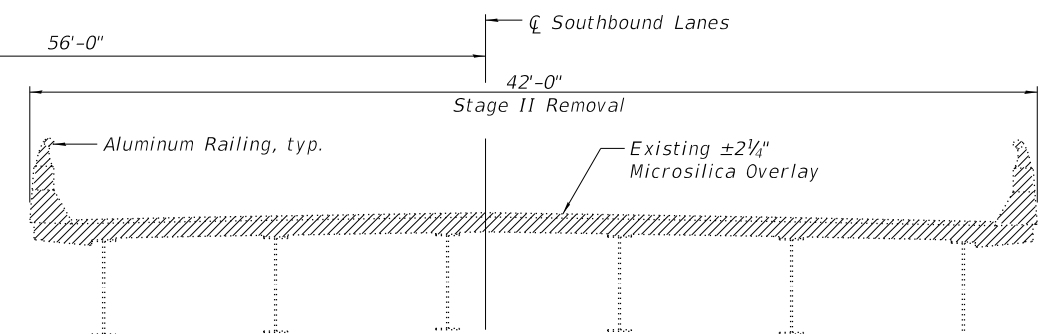
Notes:  
Cost of removing existing Aluminum Railing and Microsilica Overlay is included in the Cost of Removal of Existing Concrete Deck.  
See Roadway Plans for Quantity of Temporary Concrete Barrier.



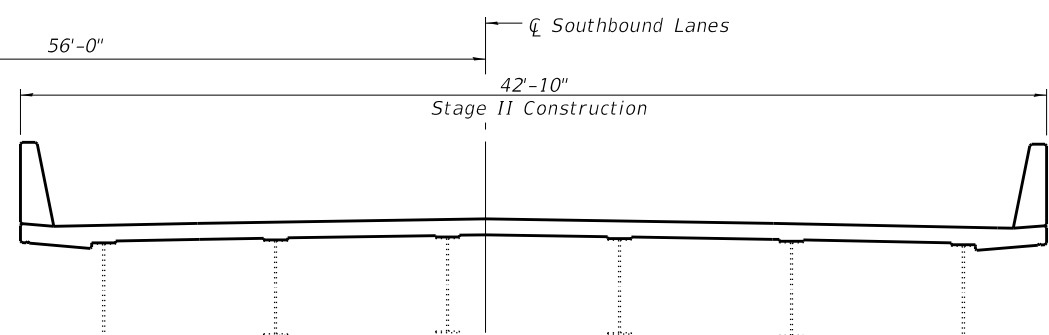
**STAGE I CONSTRUCTION & STAGE I TRAFFIC**  
(Looking South)



**STAGE II REMOVAL & STAGE II TRAFFIC**  
(Looking South)



**STAGE II CONSTRUCTION & STAGE II TRAFFIC**  
(Looking South)



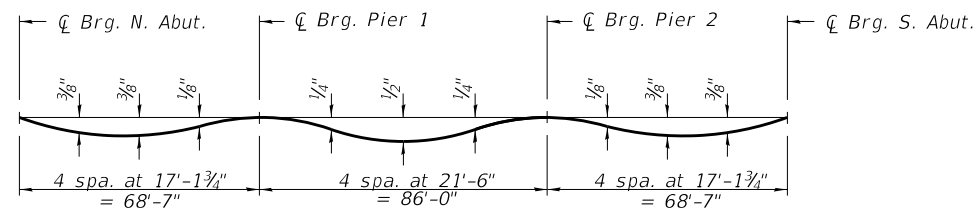
FILE NAME = 003-Stage Const Details	USER NAME =	DESIGNED - CMV	REVISED -
BACON   FARMER   WORKMAN ENGINEERING & TESTING, INC.		CHECKED - GBR	REVISED -
403 NORTH COURT STREET NAPERVILLE, IL 60563 PHONE - 630.987.9100	PLOT SCALE =	DRAWN - BJV	REVISED -
	PLOT DATE = 1/6/2020	CHECKED - CMV	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE CONSTRUCTION DETAILS  
STRUCTURE NO. 068-0048 (SB) & 068-0049 (NB)**

SHEET NO. 3 OF 40 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	68-1,3 RS-3, 68-2 RS-5)BR	MONTGOMERY	307	182
CONTRACT NO. 72D31				
ILLINOIS FED. AID PROJECT				

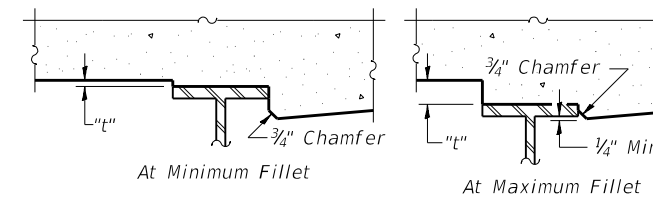


**DEAD LOAD DEFLECTION DIAGRAM**

(Includes weight of concrete only.)

Note:

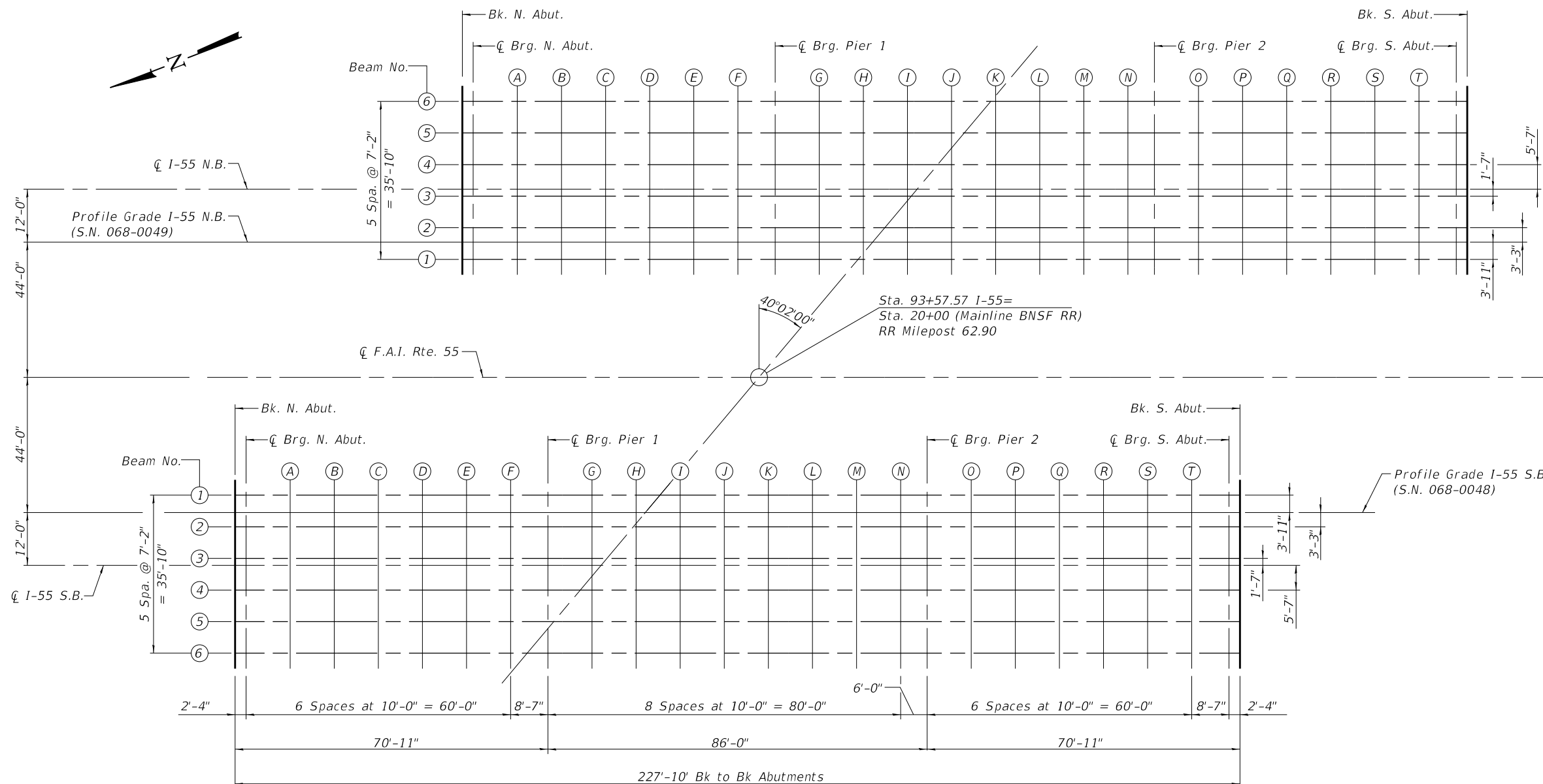
The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections and grinding as shown on sheets 5 thru 8 of 40 sheets.



To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding" shown on sheets 5 thru 8 of 40 sheets, minus the initial slab thickness prior to grinding, equals the fillet heights "t" above top flange of beams.

The slab is to be ground after curing to achieve smoothness, but the slab is not to be ground to elevations below the "Theoretical Grade Elevations" shown on sheets 5 thru 8 of 40 sheets. For grinding the deck, see Special Provisions.

**FILLET HEIGHTS**



**PLAN**

FILE NAME = 004-Top of Slab Elevations <b>BFW</b> BACON   FARMER   WORKMAN ENGINEERING & TESTING, INC. <small>403 NORTH COURT STREET          NAPERVILLE, ILLINOIS 60563          PHONE - 630.987.8100</small>	USER NAME =	DESIGNED - CMV	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>TOP OF SLAB ELEVATIONS</b> <b>STRUCTURE NO. 068-0048 (S.B.) &amp; 068-0049 (N.B.)</b>	F.A.I. RTE. = 55	SECTION = 68-1,3 RS-3, 68-2 RS-5)BR	COUNTY = MONTGOMERY	TOTAL SHEETS = 307	SHEET NO. = 183
	PLOT SCALE =	DRAWN - BJV	REVISED -			CONTRACT NO. = 72D31	ILLINOIS FED. AID PROJECT			
	PLOT DATE = 1/6/2020	CHECKED - GBR	REVISED -	SHEET NO. 4 OF 40 SHEETS						

BEAM 1

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding. Rows include BK. N. ABUT., BRG. N. ABUT. (A-F), BRG. PIER 1 (G-N), BRG. PIER 2 (O-T), BRG. S. ABUT., and BK. S. ABUT.

PROFILE GRADE I-55 N.B. (S.N. 068-0049)

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding. Rows include BK. N. ABUT., BRG. N. ABUT. (A-F), BRG. PIER 1 (G-N), BRG. PIER 2 (O-T), BRG. S. ABUT., and BK. S. ABUT.

BEAM 2

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding. Rows include BK. N. ABUT., BRG. N. ABUT. (A-F), BRG. PIER 1 (G-N), BRG. PIER 2 (O-T), BRG. S. ABUT., and BK. S. ABUT.

BEAM 3

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding. Rows include BK. N. ABUT., BRG. N. ABUT. (A-F), BRG. PIER 1 (G-N), BRG. PIER 2 (O-T), BRG. S. ABUT., and BK. S. ABUT.

CL I-55 N.B.

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding. Rows include BK. N. ABUT., BRG. N. ABUT. (A-F), BRG. PIER 1 (G-N), BRG. PIER 2 (O-T), BRG. S. ABUT., and BK. S. ABUT.

Note: Offsets are from Profile Grade I-55 N.B. (S.N. 068-0049). Looking Upstation: Left is "-"; Right is "+"

**BEAM 4**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
BK. N. ABUT.	93+01.51	-17.58	701.92	701.94
☒ BRG. N. ABUT.	93+03.84	-17.58	701.93	701.95
A	93+13.84	-17.58	701.96	701.99
B	93+23.84	-17.58	701.98	702.03
C	93+33.84	-17.58	702.01	702.06
D	93+43.84	-17.58	702.03	702.08
E	93+53.84	-17.58	702.05	702.08
F	93+63.84	-17.58	702.07	702.09
☒ BRG. PIER 1	93+72.43	-17.58	702.08	702.10
G	93+82.43	-17.58	702.09	702.12
H	93+92.43	-17.58	702.10	702.14
I	94+02.43	-17.58	702.11	702.17
J	94+12.43	-17.58	702.12	702.18
K	94+22.43	-17.58	702.13	702.18
L	94+32.43	-17.58	702.13	702.18
M	94+42.43	-17.58	702.13	702.16
N	94+52.43	-17.58	702.13	702.15
☒ BRG. PIER 2	94+58.43	-17.58	702.13	702.15
O	94+68.43	-17.58	702.12	702.15
P	94+78.43	-17.58	702.11	702.15
Q	94+88.43	-17.58	702.10	702.15
R	94+98.43	-17.58	702.09	702.15
S	95+08.43	-17.58	702.08	702.13
T	95+18.43	-17.58	702.06	702.10
☒ BRG. S. ABUT.	95+27.01	-17.58	702.05	702.07
BK. S. ABUT.	95+29.34	-17.58	702.04	702.07

**BEAM 5**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
BK. N. ABUT.	93+01.51	-24.75	701.81	701.83
☒ BRG. N. ABUT.	93+03.84	-24.75	701.82	701.84
A	93+13.84	-24.75	701.84	701.88
B	93+23.84	-24.75	701.87	701.92
C	93+33.84	-24.75	701.89	701.95
D	93+43.84	-24.75	701.92	701.97
E	93+53.84	-24.75	701.94	701.97
F	93+63.84	-24.75	701.95	701.98
☒ BRG. PIER 1	93+72.43	-24.75	701.97	701.99
G	93+82.43	-24.75	701.98	702.01
H	93+92.43	-24.75	701.99	702.03
I	94+02.43	-24.75	702.00	702.06
J	94+12.43	-24.75	702.01	702.07
K	94+22.43	-24.75	702.01	702.07
L	94+32.43	-24.75	702.02	702.07
M	94+42.43	-24.75	702.02	702.05
N	94+52.43	-24.75	702.02	702.04
☒ BRG. PIER 2	94+58.43	-24.75	702.01	702.04
O	94+68.43	-24.75	702.01	702.04
P	94+78.43	-24.75	702.00	702.04
Q	94+88.43	-24.75	701.99	702.04
R	94+98.43	-24.75	701.98	702.04
S	95+08.43	-24.75	701.97	702.02
T	95+18.43	-24.75	701.95	701.99
☒ BRG. S. ABUT.	95+27.01	-24.75	701.94	701.96
BK. S. ABUT.	95+29.34	-24.75	701.93	701.95

**BEAM 6**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
BK. N. ABUT.	93+01.51	-31.92	701.67	701.69
☒ BRG. N. ABUT.	93+03.84	-31.92	701.67	701.69
A	93+13.84	-31.92	701.70	701.74
B	93+23.84	-31.92	701.73	701.78
C	93+33.84	-31.92	701.75	701.81
D	93+43.84	-31.92	701.77	701.82
E	93+53.84	-31.92	701.79	701.83
F	93+63.84	-31.92	701.81	701.84
☒ BRG. PIER 1	93+72.43	-31.92	701.82	701.85
G	93+82.43	-31.92	701.84	701.86
H	93+92.43	-31.92	701.85	701.89
I	94+02.43	-31.92	701.86	701.91
J	94+12.43	-31.92	701.87	701.93
K	94+22.43	-31.92	701.87	701.93
L	94+32.43	-31.92	701.87	701.92
M	94+42.43	-31.92	701.87	701.91
N	94+52.43	-31.92	701.87	701.90
☒ BRG. PIER 2	94+58.43	-31.92	701.87	701.89
O	94+68.43	-31.92	701.87	701.89
P	94+78.43	-31.92	701.86	701.90
Q	94+88.43	-31.92	701.85	701.90
R	94+98.43	-31.92	701.84	701.89
S	95+08.43	-31.92	701.83	701.88
T	95+18.43	-31.92	701.81	701.85
☒ BRG. S. ABUT.	95+27.01	-31.92	701.79	701.82
BK. S. ABUT.	95+29.34	-31.92	701.79	701.81

Note:  
 Offsets are from Profile Grade I-55 N.B. (S.N. 068-0049).  
 Looking Upstation: Left is "-"; Right is "+"

BEAM 1

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
BK. N. ABUT.	92+04.08	-3.92	701.35	701.37
☉ BRG. N. ABUT.	92+06.41	-3.92	701.36	701.38
A	92+16.41	-3.92	701.41	701.45
B	92+26.41	-3.92	701.46	701.51
C	92+36.41	-3.92	701.50	701.56
D	92+46.41	-3.92	701.55	701.60
E	92+56.41	-3.92	701.59	701.62
F	92+66.41	-3.92	701.63	701.65
☉ BRG. PIER 1	92+75.00	-3.92	701.66	701.68
G	92+85.00	-3.92	701.69	701.72
H	92+95.00	-3.92	701.73	701.77
I	93+05.00	-3.92	701.76	701.81
J	93+15.00	-3.92	701.78	701.84
K	93+25.00	-3.92	701.81	701.87
L	93+35.00	-3.92	701.83	701.88
M	93+45.00	-3.92	701.86	701.89
N	93+55.00	-3.92	701.88	701.90
☉ BRG. PIER 2	93+61.00	-3.92	701.89	701.91
O	93+71.00	-3.92	701.90	701.93
P	93+81.00	-3.92	701.92	701.96
Q	93+91.00	-3.92	701.93	701.98
R	94+01.00	-3.92	701.94	701.99
S	94+11.00	-3.92	701.95	702.00
T	94+21.00	-3.92	701.95	701.99
☉ BRG. S. ABUT.	94+29.58	-3.92	701.95	701.97
BK. S. ABUT.	94+31.91	-3.92	701.95	701.97

PROFILE GRADE I-55 S.B. (S.N. 068-0048)

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
BK. N. ABUT.	92+04.08	0.00	701.43	701.45
☉ BRG. N. ABUT.	92+06.41	0.00	701.44	701.46
A	92+16.41	0.00	701.49	701.53
B	92+26.41	0.00	701.54	701.59
C	92+36.41	0.00	701.58	701.64
D	92+46.41	0.00	701.63	701.67
E	92+56.41	0.00	701.67	701.70
F	92+66.41	0.00	701.70	701.73
☉ BRG. PIER 1	92+75.00	0.00	701.74	701.76
G	92+85.00	0.00	701.77	701.80
H	92+95.00	0.00	701.80	701.84
I	93+05.00	0.00	701.83	701.89
J	93+15.00	0.00	701.86	701.92
K	93+25.00	0.00	701.89	701.95
L	93+35.00	0.00	701.91	701.96
M	93+45.00	0.00	701.93	701.97
N	93+55.00	0.00	701.95	701.98
☉ BRG. PIER 2	93+61.00	0.00	701.96	701.99
O	93+71.00	0.00	701.98	702.01
P	93+81.00	0.00	701.99	702.03
Q	93+91.00	0.00	702.01	702.06
R	94+01.00	0.00	702.02	702.07
S	94+11.00	0.00	702.02	702.07
T	94+21.00	0.00	702.03	702.07
☉ BRG. S. ABUT.	94+29.58	0.00	702.03	702.05
BK. S. ABUT.	94+31.91	0.00	702.03	702.05

BEAM 2

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
BK. N. ABUT.	92+04.08	3.25	701.48	701.50
☉ BRG. N. ABUT.	92+06.41	3.25	701.49	701.51
A	92+16.41	3.25	701.54	701.58
B	92+26.41	3.25	701.59	701.64
C	92+36.41	3.25	701.63	701.69
D	92+46.41	3.25	701.67	701.72
E	92+56.41	3.25	701.71	701.75
F	92+66.41	3.25	701.75	701.78
☉ BRG. PIER 1	92+75.00	3.25	701.79	701.81
G	92+85.00	3.25	701.82	701.85
H	92+95.00	3.25	701.85	701.89
I	93+05.00	3.25	701.88	701.94
J	93+15.00	3.25	701.91	701.97
K	93+25.00	3.25	701.94	702.00
L	93+35.00	3.25	701.96	702.01
M	93+45.00	3.25	701.98	702.02
N	93+55.00	3.25	702.00	702.03
☉ BRG. PIER 2	93+61.00	3.25	702.01	702.03
O	93+71.00	3.25	702.03	702.06
P	93+81.00	3.25	702.04	702.08
Q	93+91.00	3.25	702.05	702.11
R	94+01.00	3.25	702.06	702.12
S	94+11.00	3.25	702.07	702.12
T	94+21.00	3.25	702.08	702.11
☉ BRG. S. ABUT.	94+29.58	3.25	702.08	702.10
BK. S. ABUT.	94+31.91	3.25	702.08	702.10

BEAM 3

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
BK. N. ABUT.	92+04.08	10.42	701.58	701.61
☉ BRG. N. ABUT.	92+06.41	10.42	701.60	701.62
A	92+16.41	10.42	701.65	701.69
B	92+26.41	10.42	701.69	701.74
C	92+36.41	10.42	701.74	701.79
D	92+46.41	10.42	701.78	701.83
E	92+56.41	10.42	701.82	701.86
F	92+66.41	10.42	701.86	701.89
☉ BRG. PIER 1	92+75.00	10.42	701.89	701.91
G	92+85.00	10.42	701.93	701.95
H	92+95.00	10.42	701.96	702.00
I	93+05.00	10.42	701.99	702.04
J	93+15.00	10.42	702.02	702.08
K	93+25.00	10.42	702.04	702.10
L	93+35.00	10.42	702.07	702.12
M	93+45.00	10.42	702.09	702.12
N	93+55.00	10.42	702.11	702.13
☉ BRG. PIER 2	93+61.00	10.42	702.12	702.14
O	93+71.00	10.42	702.14	702.16
P	93+81.00	10.42	702.15	702.19
Q	93+91.00	10.42	702.16	702.21
R	94+01.00	10.42	702.17	702.23
S	94+11.00	10.42	702.18	702.23
T	94+21.00	10.42	702.18	702.22
☉ BRG. S. ABUT.	94+29.58	10.42	702.19	702.21
BK. S. ABUT.	94+31.91	10.42	702.19	702.21

☉ I-55 S.B.

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
BK. N. ABUT.	92+04.08	12.00	701.61	701.63
☉ BRG. N. ABUT.	92+06.41	12.00	701.62	701.64
A	92+16.41	12.00	701.67	701.71
B	92+26.41	12.00	701.72	701.77
C	92+36.41	12.00	701.76	701.82
D	92+46.41	12.00	701.81	701.85
E	92+56.41	12.00	701.85	701.88
F	92+66.41	12.00	701.89	701.91
☉ BRG. PIER 1	92+75.00	12.00	701.92	701.94
G	92+85.00	12.00	701.95	701.98
H	92+95.00	12.00	701.98	702.02
I	93+05.00	12.00	702.01	702.07
J	93+15.00	12.00	702.04	702.10
K	93+25.00	12.00	702.07	702.13
L	93+35.00	12.00	702.09	702.14
M	93+45.00	12.00	702.11	702.15
N	93+55.00	12.00	702.13	702.16
☉ BRG. PIER 2	93+61.00	12.00	702.14	702.17
O	93+71.00	12.00	702.16	702.19
P	93+81.00	12.00	702.17	702.21
Q	93+91.00	12.00	702.19	702.24
R	94+01.00	12.00	702.20	702.25
S	94+11.00	12.00	702.20	702.25
T	94+21.00	12.00	702.21	702.25
☉ BRG. S. ABUT.	94+29.58	12.00	702.21	702.23
BK. S. ABUT.	94+31.91	12.00	702.21	702.23

Note:  
Offsets are from Profile Grade I-55 S.B. (S.N. 068-0048).  
Looking Upstation: Left is "-"; Right is "+"

FILE NAME = \$FILES\$

USER NAME =

DESIGNED - CMV

REVISIED -



BACON | FARMER | WORKMAN  
ENGINEERING & TESTING, INC.

CHECKED - GBR

REVISIED -

PLOT SCALE =

DRAWN - BJV

REVISIED -

PLOT DATE = \$DATE\$

CHECKED - GBR

REVISIED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS  
STRUCTURE NO. 068-0048 (S.B.)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	68-1,3 RS-3, 68-2 RS-5)BR	MONTGOMERY	307	186
CONTRACT NO. 72D31				

SHEET NO. 7 OF 40 SHEETS

ILLINOIS FED. AID PROJECT

**BEAM 4**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
BK. N. ABUT.	92+04.08	17.58	701.52	701.55
☒ BRG. N. ABUT.	92+06.41	17.58	701.54	701.56
A	92+16.41	17.58	701.59	701.63
B	92+26.41	17.58	701.63	701.68
C	92+36.41	17.58	701.68	701.73
D	92+46.41	17.58	701.72	701.77
E	92+56.41	17.58	701.76	701.80
F	92+66.41	17.58	701.80	701.83
☒ BRG. PIER 1	92+75.00	17.58	701.83	701.85
G	92+85.00	17.58	701.87	701.89
H	92+95.00	17.58	701.90	701.94
I	93+05.00	17.58	701.93	701.98
J	93+15.00	17.58	701.96	702.02
K	93+25.00	17.58	701.98	702.04
L	93+35.00	17.58	702.01	702.06
M	93+45.00	17.58	702.03	702.06
N	93+55.00	17.58	702.05	702.07
☒ BRG. PIER 2	93+61.00	17.58	702.06	702.08
O	93+71.00	17.58	702.08	702.10
P	93+81.00	17.58	702.09	702.13
Q	93+91.00	17.58	702.10	702.15
R	94+01.00	17.58	702.11	702.17
S	94+11.00	17.58	702.12	702.17
T	94+21.00	17.58	702.12	702.16
☒ BRG. S. ABUT.	94+29.58	17.58	702.13	702.15
BK. S. ABUT.	94+31.91	17.58	702.13	702.15

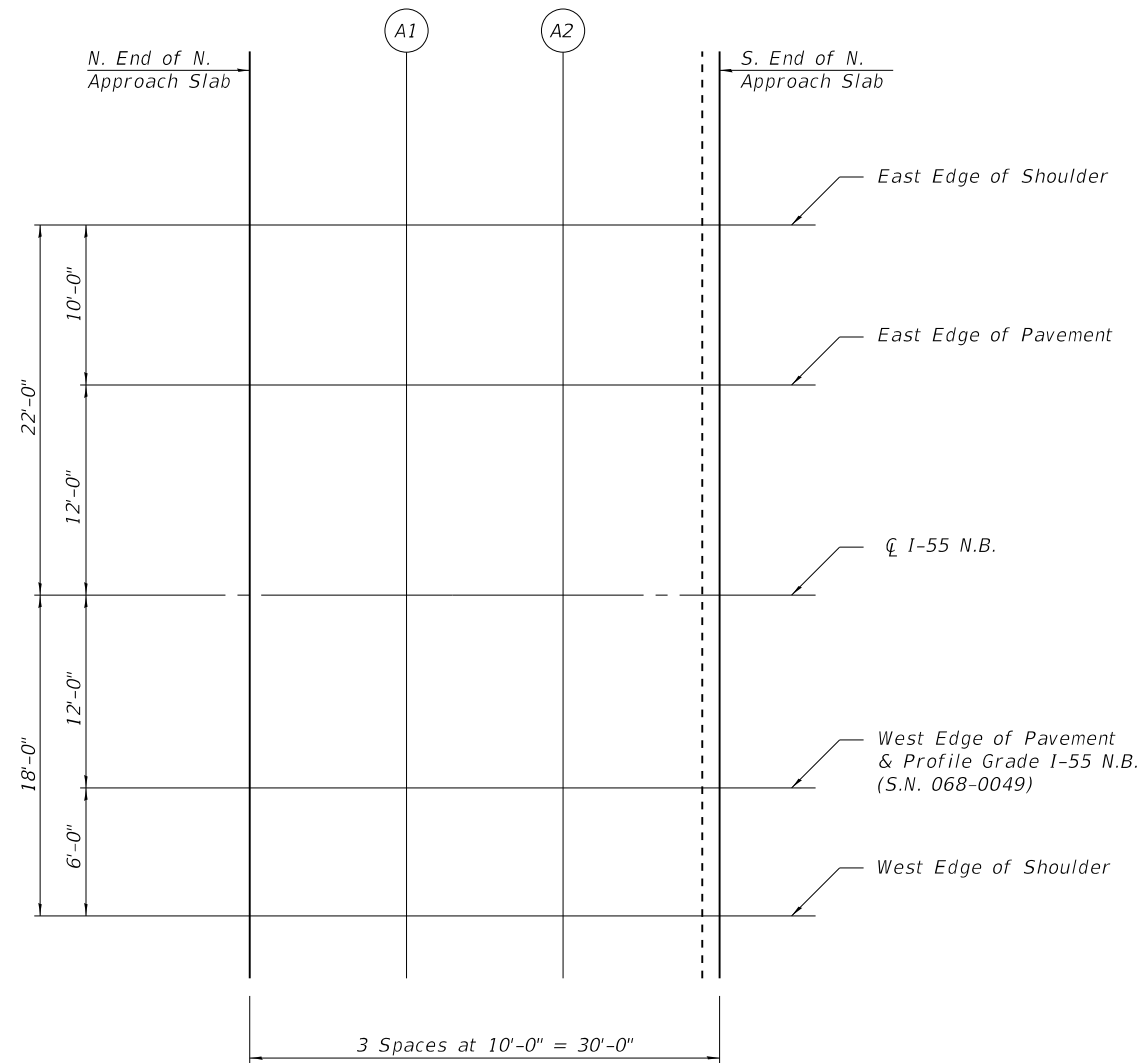
**BEAM 5**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
BK. N. ABUT.	92+04.08	24.75	701.41	701.43
☒ BRG. N. ABUT.	92+06.41	24.75	701.42	701.45
A	92+16.41	24.75	701.47	701.51
B	92+26.41	24.75	701.52	701.57
C	92+36.41	24.75	701.57	701.62
D	92+46.41	24.75	701.61	701.66
E	92+56.41	24.75	701.65	701.69
F	92+66.41	24.75	701.69	701.72
☒ BRG. PIER 1	92+75.00	24.75	701.72	701.74
G	92+85.00	24.75	701.76	701.78
H	92+95.00	24.75	701.79	701.83
I	93+05.00	24.75	701.82	701.87
J	93+15.00	24.75	701.85	701.91
K	93+25.00	24.75	701.87	701.93
L	93+35.00	24.75	701.90	701.95
M	93+45.00	24.75	701.92	701.95
N	93+55.00	24.75	701.94	701.96
☒ BRG. PIER 2	93+61.00	24.75	701.95	701.97
O	93+71.00	24.75	701.97	701.99
P	93+81.00	24.75	701.98	702.02
Q	93+91.00	24.75	701.99	702.04
R	94+01.00	24.75	702.00	702.06
S	94+11.00	24.75	702.01	702.06
T	94+21.00	24.75	702.01	702.05
☒ BRG. S. ABUT.	94+29.58	24.75	702.02	702.04
BK. S. ABUT.	94+31.91	24.75	702.02	702.04

**BEAM 6**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
BK. N. ABUT.	92+04.08	31.92	701.27	701.29
☒ BRG. N. ABUT.	92+06.41	31.92	701.28	701.30
A	92+16.41	31.92	701.33	701.37
B	92+26.41	31.92	701.38	701.43
C	92+36.41	31.92	701.42	701.48
D	92+46.41	31.92	701.47	701.52
E	92+56.41	31.92	701.51	701.54
F	92+66.41	31.92	701.55	701.57
☒ BRG. PIER 1	92+75.00	31.92	701.58	701.60
G	92+85.00	31.92	701.61	701.64
H	92+95.00	31.92	701.65	701.69
I	93+05.00	31.92	701.68	701.73
J	93+15.00	31.92	701.70	701.76
K	93+25.00	31.92	701.73	701.79
L	93+35.00	31.92	701.75	701.80
M	93+45.00	31.92	701.78	701.81
N	93+55.00	31.92	701.80	701.82
☒ BRG. PIER 2	93+61.00	31.92	701.81	701.83
O	93+71.00	31.92	701.82	701.85
P	93+81.00	31.92	701.84	701.88
Q	93+91.00	31.92	701.85	701.90
R	94+01.00	31.92	701.86	701.91
S	94+11.00	31.92	701.87	701.92
T	94+21.00	31.92	701.87	701.91
☒ BRG. S. ABUT.	94+29.58	31.92	701.87	701.89
BK. S. ABUT.	94+31.91	31.92	701.87	701.89

Note:  
 Offsets are from Profile Grade I-55 S.B. (S.N. 068-0048).  
 Looking Upstation: Left is "-"; Right is "+"



**PLAN**

Note:  
 Offsets are from West Edge of Pavement  
 & Profile Grade I-55 N.B. (S.N. 068-0049).  
 Looking Upstation: Left is "-"; Right is "+"

**EAST EDGE OF SHOULDER**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of North Appr. Slab	92+72.01	-34.00	701.53	701.55
A1	92+82.01	-34.00	701.56	701.58
A2	92+92.01	-34.00	701.59	701.61
S. End of North Appr. Slab	93+02.01	-34.00	701.63	701.65

**EAST EDGE OF PAVEMENT**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of North Appr. Slab	92+72.01	-24.00	701.73	701.75
A1	92+82.01	-24.00	701.76	701.78
A2	92+92.01	-24.00	701.79	701.81
S. End of North Appr. Slab	93+02.01	-24.00	701.83	701.85

**CL I-55 N.B.**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of North Appr. Slab	92+72.01	-12.00	701.91	701.93
A1	92+82.01	-12.00	701.94	701.96
A2	92+92.01	-12.00	701.97	701.99
S. End of North Appr. Slab	93+02.01	-12.00	702.01	702.03

**WEST EDGE OF PAVEMENT & PROFILE GRADE I-55 N.B.**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of North Appr. Slab	92+72.01	0.00	701.73	701.75
A1	92+82.01	0.00	701.76	701.78
A2	92+92.01	0.00	701.79	701.81
S. End of North Appr. Slab	93+02.01	0.00	701.83	701.85

**WEST EDGE OF SHOULDER**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of North Appr. Slab	92+72.01	6.00	701.61	701.63
A1	92+82.01	6.00	701.64	701.66
A2	92+92.01	6.00	701.67	701.69
S. End of North Appr. Slab	93+02.01	6.00	701.71	701.73

FILE NAME = 009-N.Appr Slab Elev SN 068-0049 (N.B.)	USER NAME =	DESIGNED - CMV	REVISED -
BACON   FARMER   WORKMAN ENGINEERING & TESTING, INC.		CHECKED - GBR	REVISED -
403 NORTH COURT STREET NAPERVILLE, IL 60563 PHONE - 630.987.9100	PLOT SCALE =	DRAWN - BJV	REVISED -
	PLOT DATE = 1/6/2020	CHECKED - GBR	REVISED -

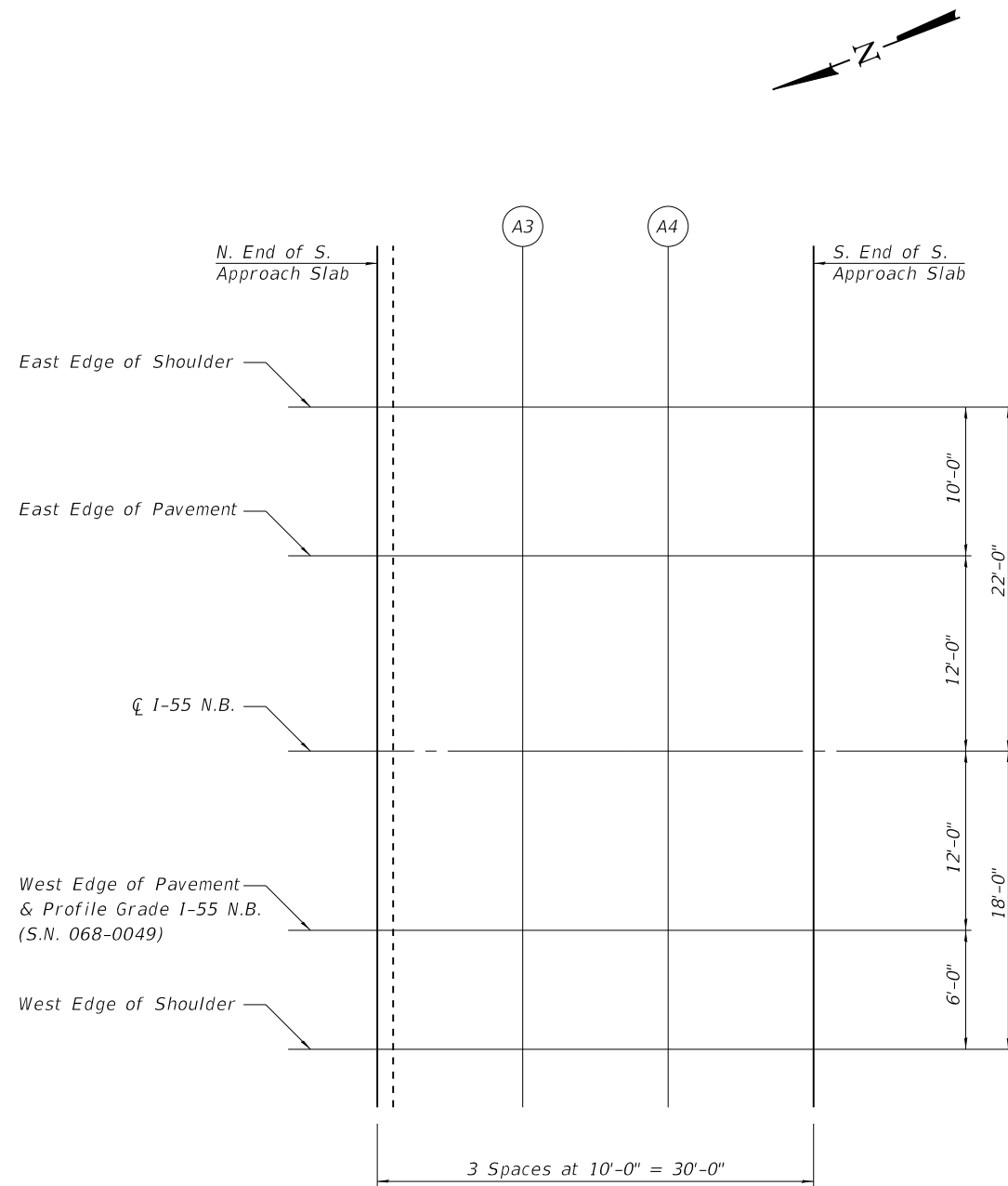
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TOP OF NORTH APPROACH SLAB ELEVATIONS  
 STRUCTURE NO. 068-0049 (N.B.)**

SHEET NO. 9 OF 40 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	68-1,3 RS-3, 68-2 RS-5)BR	MONTGOMERY	307	188
CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	





PLAN

Note:  
 Offsets are from West Edge of Pavement  
 & Profile Grade I-55 N.B. (S.N. 068-0049).  
 Looking Upstation: Left is "-"; Right is "+"

EAST EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of South Appr. Slab	95+28.84	-34.00	701.75	701.77
A3	95+38.84	-34.00	701.73	701.75
A4	95+48.84	-34.00	701.71	701.73
S. End of South Appr. Slab	95+58.84	-34.00	701.68	701.70

EAST EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of South Appr. Slab	95+28.84	-24.00	701.95	701.97
A3	95+38.84	-24.00	701.93	701.95
A4	95+48.84	-24.00	701.91	701.93
S. End of South Appr. Slab	95+58.84	-24.00	701.88	701.90

CL I-55 N.B.

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of South Appr. Slab	95+28.84	-12.00	702.13	702.15
A3	95+38.84	-12.00	702.11	702.13
A4	95+48.84	-12.00	702.09	702.11
S. End of South Appr. Slab	95+58.84	-12.00	702.06	702.08

WEST EDGE OF PAVEMENT & PROFILE GRADE I-55 N.B.

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of South Appr. Slab	95+28.84	0.00	701.95	701.97
A3	95+38.84	0.00	701.93	701.95
A4	95+48.84	0.00	701.91	701.93
S. End of South Appr. Slab	95+58.84	0.00	701.88	701.90

WEST EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of South Appr. Slab	95+28.84	6.00	701.83	701.85
A3	95+38.84	6.00	701.81	701.83
A4	95+48.84	6.00	701.79	701.81
S. End of South Appr. Slab	95+58.84	6.00	701.76	701.78

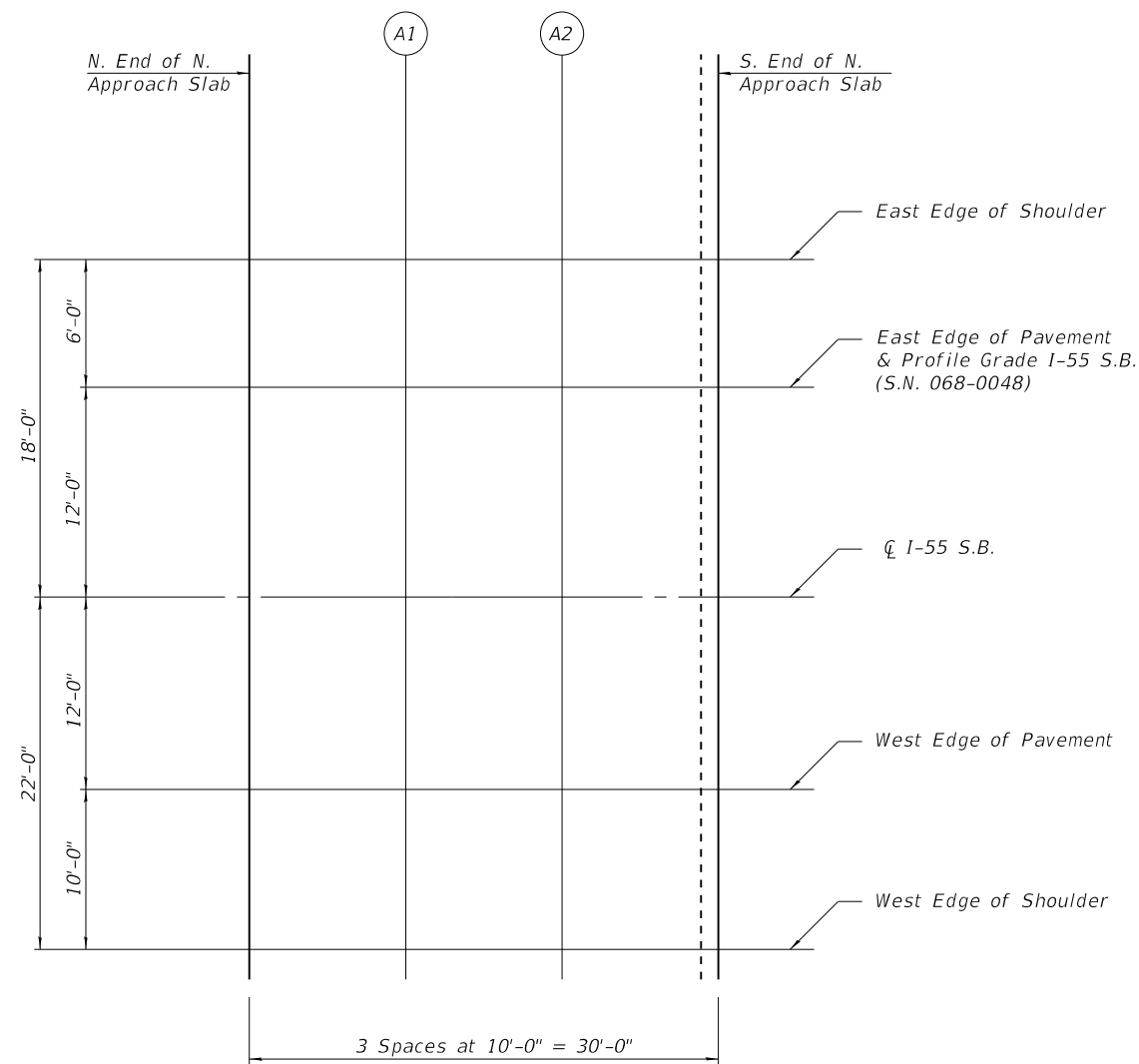
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BACON   FARMER   WORKMAN ENGINEERING & TESTING, INC.		CHECKED - GBR	REVISED -
PLOT SCALE =		DRAWN - BJV	REVISED -
PLOT DATE = 1/6/2020		CHECKED - GBR	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TOP OF SOUTH APPROACH SLAB ELEVATIONS  
 STRUCTURE NO. 068-0049 (N.B.)

SHEET NO. 10 OF 40 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	68-1,3 RS-3, 68-2 RS-5)BR	MONTGOMERY	307	189
CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	



**PLAN**

Note:  
Offsets are from East Edge of Pavement  
& Profile Grade I-55 S.B. (S.N. 068-0048).  
Looking Upstation: Left is "-"; Right is "+"

**EAST EDGE OF SHOULDER**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of North Appr. Slab	91+74.58	-6.00	701.15	701.17
A1	91+84.58	-6.00	701.20	701.22
A2	91+94.58	-6.00	701.26	701.28
S. End of North Appr. Slab	92+04.58	-6.00	701.31	701.33

**EAST EDGE OF PAVEMENT & PROFILE GRADE I-55 S.B.**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of North Appr. Slab	91+74.58	0.00	701.27	701.29
A1	91+84.58	0.00	701.32	701.34
A2	91+94.58	0.00	701.38	701.40
S. End of North Appr. Slab	92+04.58	0.00	701.43	701.45

**Centerline I-55 S.B.**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of North Appr. Slab	91+74.58	12.00	701.45	701.47
A1	91+84.58	12.00	701.50	701.52
A2	91+94.58	12.00	701.56	701.58
S. End of North Appr. Slab	92+04.58	12.00	701.61	701.63

**WEST EDGE OF PAVEMENT**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of North Appr. Slab	91+74.58	24.00	701.27	701.29
A1	91+84.58	24.00	701.32	701.34
A2	91+94.58	24.00	701.38	701.40
S. End of North Appr. Slab	92+04.58	24.00	701.43	701.45

**WEST EDGE OF SHOULDER**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of North Appr. Slab	91+74.58	34.00	701.07	701.09
A1	91+84.58	34.00	701.12	701.14
A2	91+94.58	34.00	701.18	701.20
S. End of North Appr. Slab	92+04.58	34.00	701.23	701.25

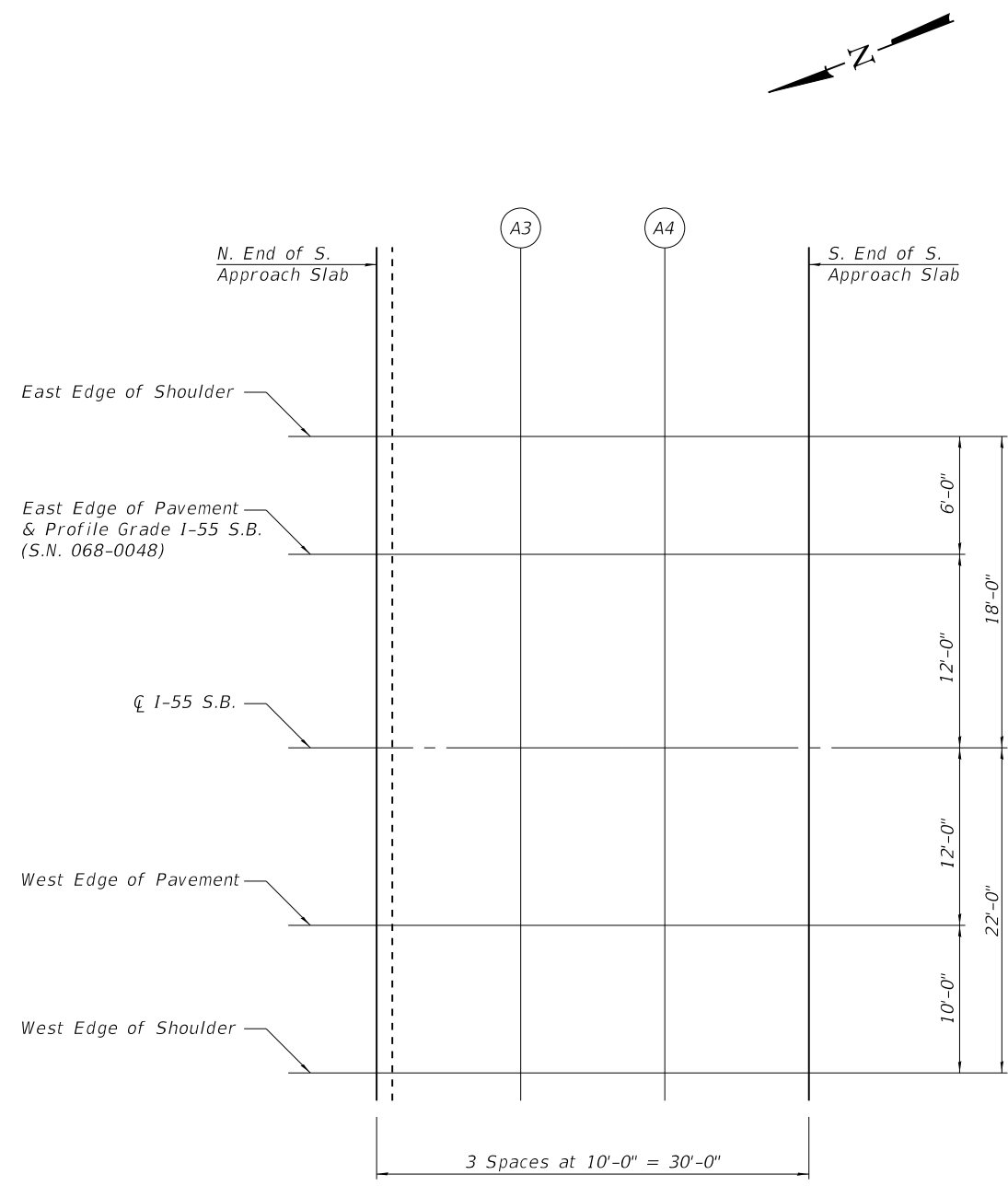
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BACON   FARMER   WORKMAN ENGINEERING & TESTING, INC.		CHECKED - GBR	REVISED -
PLOT SCALE =		DRAWN - BJV	REVISED -
PLOT DATE = 1/6/2020		CHECKED - GBR	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TOP OF NORTH APPROACH SLAB ELEVATIONS  
STRUCTURE NO. 068-0048 (S.B)**

SHEET NO. 11 OF 40 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	68-1,3 RS-3, 68-2 RS-5)BR	MONTGOMERY	307	190
CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	



**PLAN**

Note:  
 Offsets are from East Edge of Pavement  
 & Profile Grade I-55 S.B. (S.N. 068-0048).  
 Looking Upstation: Left is "-"; Right is "+"

**EAST EDGE OF SHOULDER**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of South Appr. Slab	94+31.41	-6.00	701.91	701.93
A3	94+41.41	-6.00	701.91	701.93
A4	94+51.41	-6.00	701.91	701.93
S. End of South Appr. Slab	94+61.41	-6.00	701.91	701.93

**EAST EDGE OF PAVEMENT & PROFILE GRADE I-55 S.B.**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of South Appr. Slab	94+31.41	0.00	702.03	702.05
A3	94+41.41	0.00	702.03	702.05
A4	94+51.41	0.00	702.03	702.05
S. End of South Appr. Slab	94+61.41	0.00	702.03	702.05

**CL I-55 S.B.**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of South Appr. Slab	94+31.41	12.00	702.21	702.23
A3	94+41.41	12.00	702.21	702.23
A4	94+51.41	12.00	702.21	702.23
S. End of South Appr. Slab	94+61.41	12.00	702.21	702.23

**WEST EDGE OF PAVEMENT**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of South Appr. Slab	94+31.41	24.00	702.03	702.05
A3	94+41.41	24.00	702.03	702.05
A4	94+51.41	24.00	702.03	702.05
S. End of South Appr. Slab	94+61.41	24.00	702.03	702.05

**WEST EDGE OF SHOULDER**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Grinding
N. End of South Appr. Slab	94+31.41	34.00	701.83	701.85
A3	94+41.41	34.00	701.83	701.85
A4	94+51.41	34.00	701.83	701.85
S. End of South Appr. Slab	94+61.41	34.00	701.83	701.85

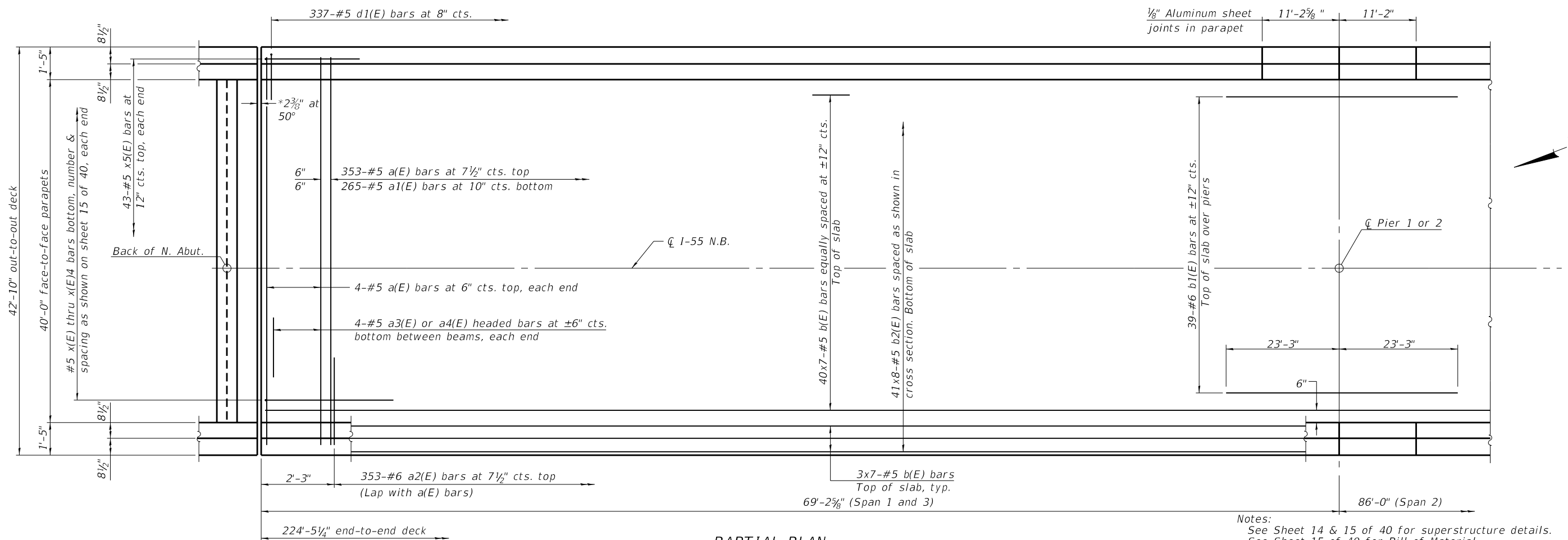
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BACON   FARMER   WORKMAN ENGINEERING & TESTING, INC.		CHECKED - GBR	REVISED -
PLOT SCALE =		DRAWN - BJV	REVISED -
PLOT DATE = 1/6/2020		CHECKED - GBR	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TOP OF SOUTH APPROACH SLAB ELEVATIONS  
 STRUCTURE NO. 068-0048 (S.B.)**

SHEET NO. 12 OF 40 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	68-1,3 RS-3, 68-2 RS-5)BR	MONTGOMERY	307	191
CONTRACT NO. 72D31			ILLINOIS FED. AID PROJECT	



**PARTIAL PLAN**

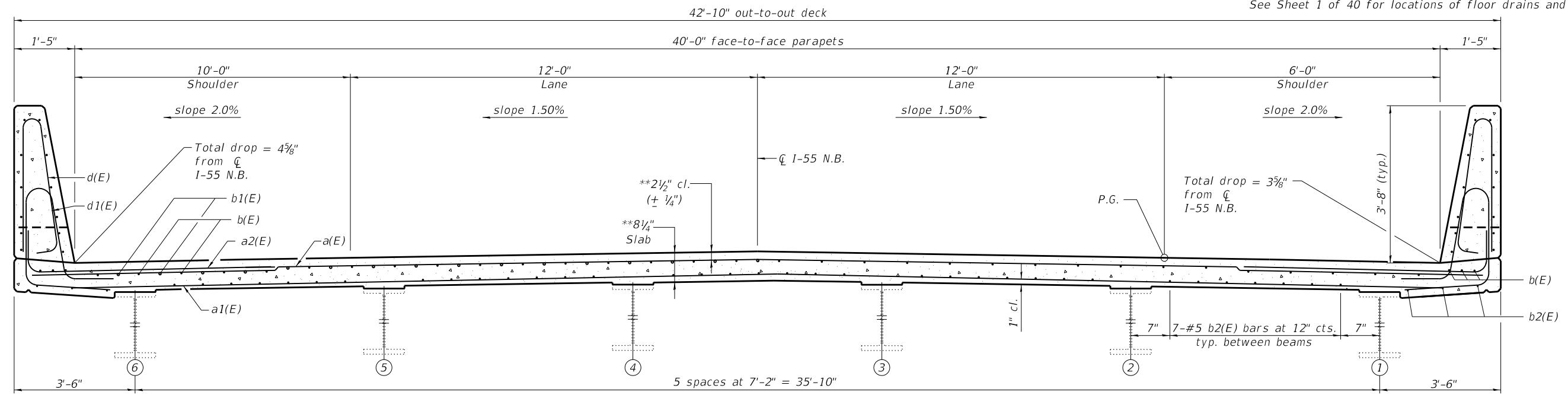
(Symmetric about the center of Span 2)

**MINIMUM BAR LAP**

#5 bar = 3'-6"

\* Dimension showing concrete opening. For joint opening see sheet 19 of 40.

Notes:  
 See Sheet 14 & 15 of 40 for superstructure details.  
 See Sheet 15 of 40 for Bill of Material.  
 Bars indicated thus 40x7-#5 etc. indicates 40 lines of bars with 7 lengths per line.  
 Dimensions are based on a Rolled Rail Strip Seal Joint. If the Contractor elects to use the Welded Rail Strip Seal Joint, the deck dimensions may require adjustments to satisfy the details on Sheet 19 of 40.  
 See Sheet 1 of 40 for locations of floor drains and DS-12 Scuppers.



**CROSS SECTION**

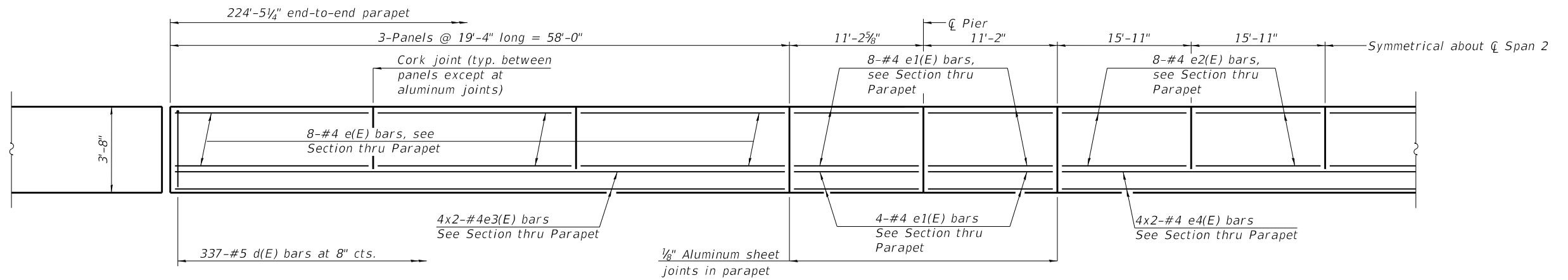
(Looking South)

**NEAR PIER**

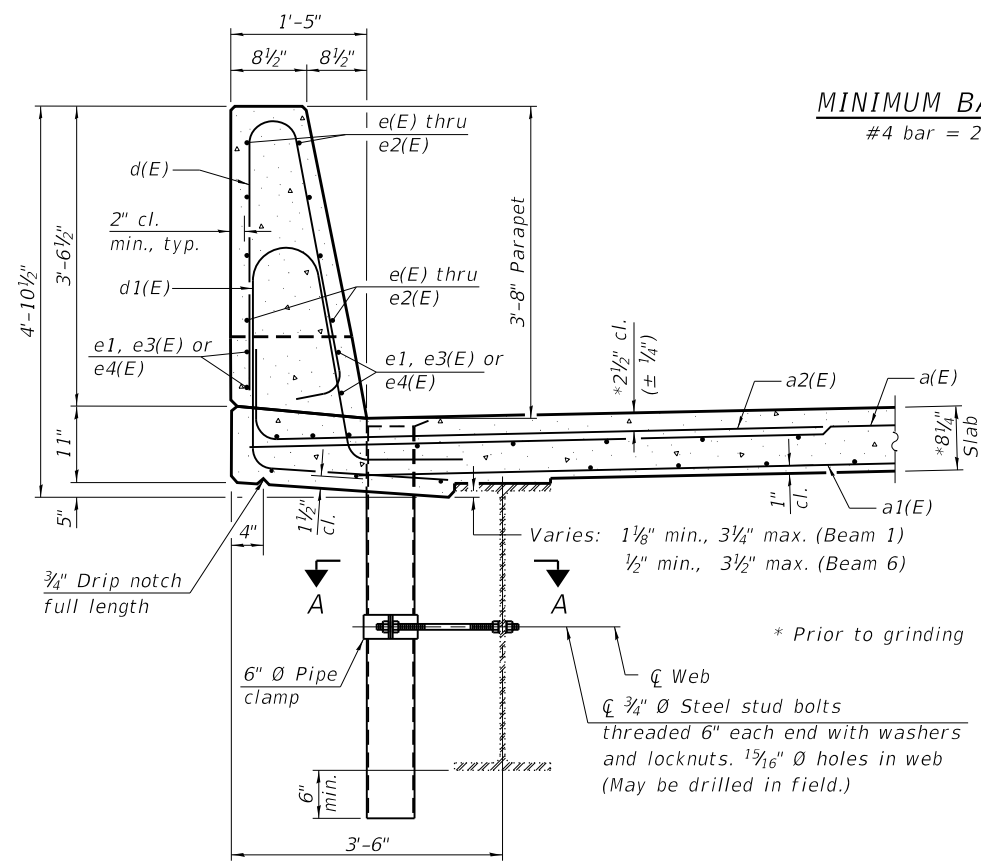
**NEAR MIDSPAN**

\*\* Prior to grinding

FILE NAME = 013-Superstructure NB <b>BFW</b> BACON   FARMER   WORKMAN ENGINEERING & TESTING, INC. <small>403 NORTH COURT STREET          WAPLETALE, ILLINOIS 60494          PHONE - 630.987.9100</small>	USER NAME =	DESIGNED - CMV	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>SUPERSTRUCTURE</b> <b>STRUCTURE NO. 068-0049 (NB)</b> SHEET NO. 13 OF 40 SHEETS	F.A.I. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	DRAWN - BJV	REVISED -			55	68-1,3 RS-3, 68-2 RS-5)BR	MONTGOMERY	307	192
	PLOT DATE = 1/6/2020	CHECKED - CMV	REVISED -			CONTRACT NO. 72031			ILLINOIS FED. AID PROJECT	

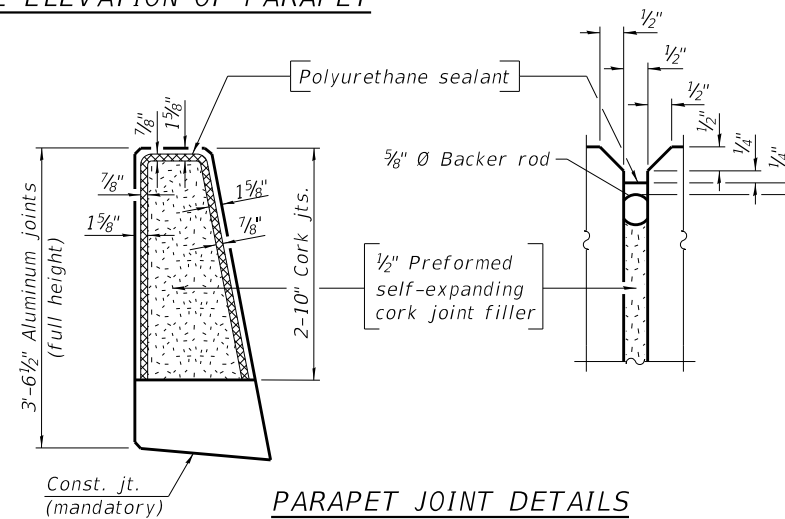


INSIDE ELEVATION OF PARAPET

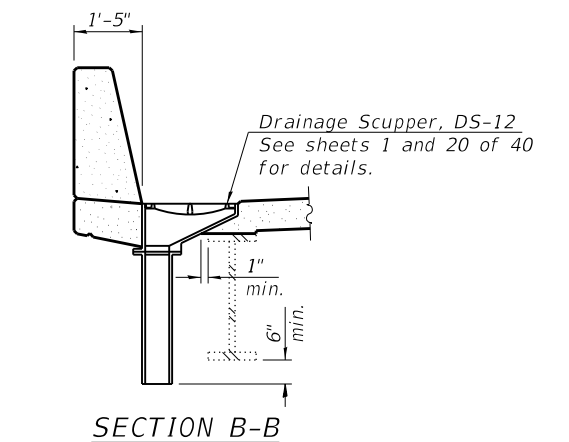


SECTION THRU PARAPET

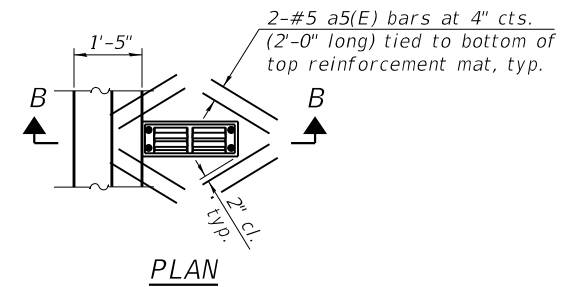
MINIMUM BAR LAP  
#4 bar = 2'-5"



PARAPET JOINT DETAILS



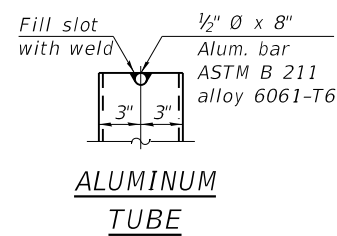
SECTION B-B



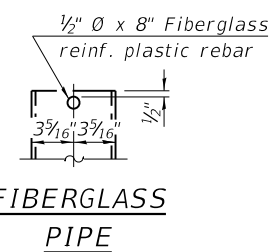
PLAN

Note:  
Cut longitudinal reinforcement to clear drainage scuppers.

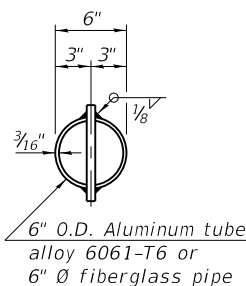
Notes:  
Fiberglass pipe shall conform to ASTM D2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.  
Floor drains need not be painted.  
The top portion of aluminum floor drains shall be coated to minimize reaction with wet concrete.  
The clamping device shall be galvanized according to AASHTO M 232. Cost of clamping device included with Floor Drains.  
The 1/8" aluminum sheet shall be ASTM B 209 alloy 3003-H14 and coated to minimize reaction with wet concrete. Cost included with Concrete Superstructure.  
The polyurethane sealant shall be according to Article 1050.04 of the Std. Spec. and the color shall be gray.  
Bars indicated thus 4x2-#4 etc. indicates 4 lines of bars with 2 lengths per line.  
Dimensions are based on a Rolled Rail Strip Seal Joint. If the Contractor elects to use the Welded Rail Strip Seal Joint, the deck dimensions may require adjustments to satisfy the details on Sheet 19 of 40.  
See Sheet 15 of 40 for additional superstructure details and Bill of Material.  
Drains shall be located clear of all cross frames.



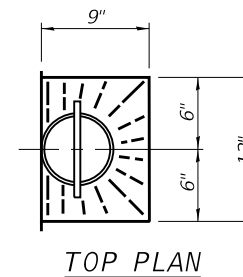
ALUMINUM TUBE



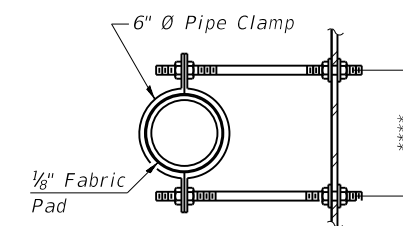
FIBERGLASS PIPE



TOP PLAN  
(Showing aluminum tube)



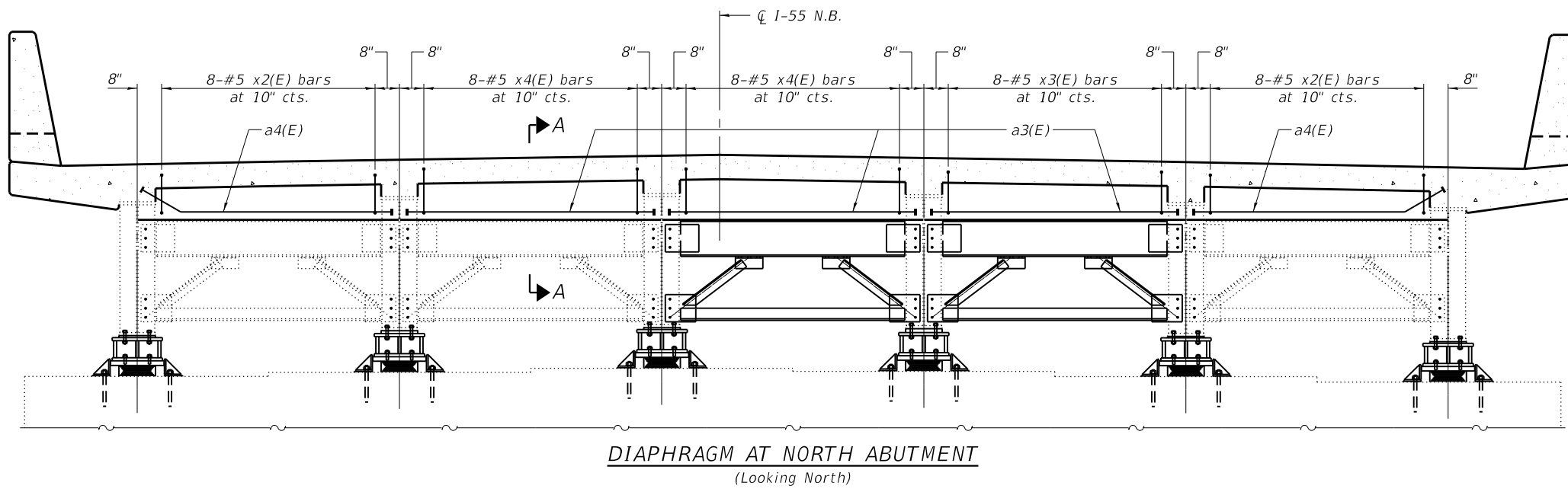
TOP PLAN



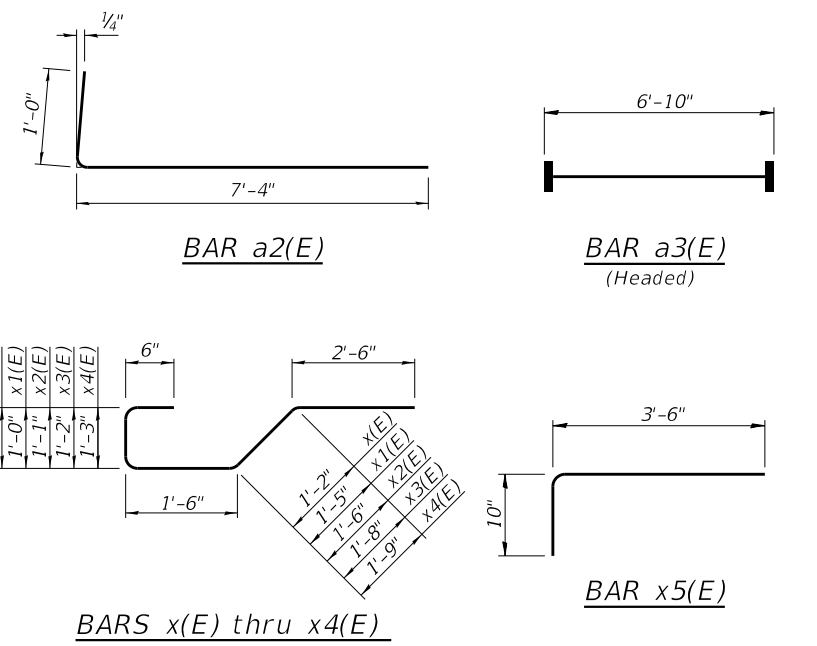
SECTION A-A

\*\*\*Dimension as required by Pipe Clamp

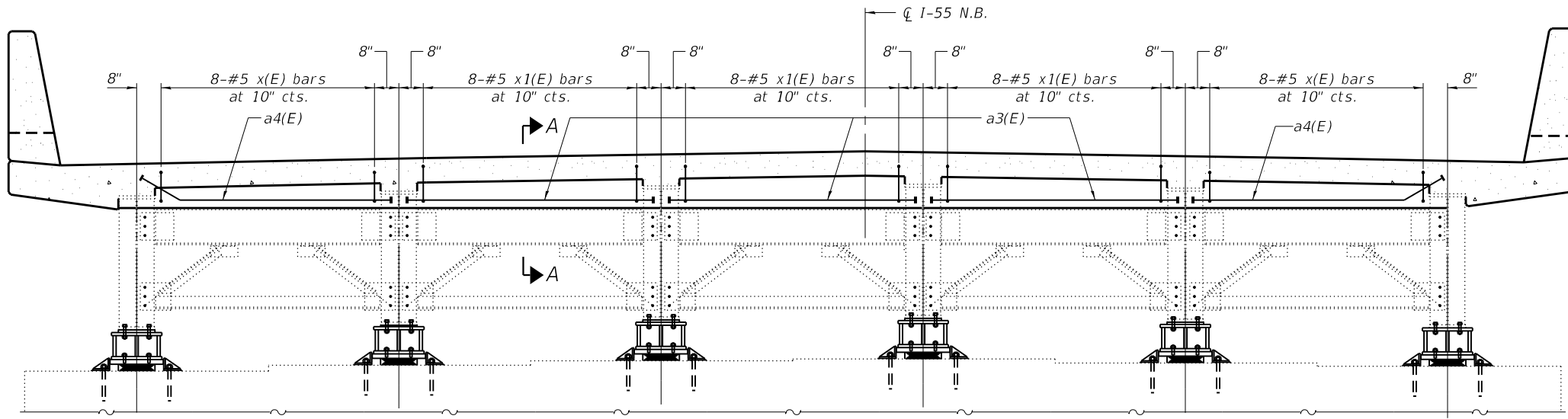
FILE NAME = 014-Superstructure Details NB	USER NAME =	DESIGNED - CMV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUPERSTRUCTURE DETAILS STRUCTURE NO. 068-0049 (NB)</b>	F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
BACON   FARMER   WORKMAN ENGINEERING & TESTING, INC.	PLOT SCALE =	CHECKED - GBR	REVISED -			55	68-1,3 RS-3, 68-2 RS-5)BR	MONTGOMERY	307	193	
403 NORTH COURT STREET NAPERVILLE, ILLINOIS 60563 PHONE - 630.957.9100	PLOT DATE = 1/6/2020	DRAWN - BJV	REVISED -			CONTRACT NO. 72D31					
		CHECKED - GBR	REVISED -			ILLINOIS FED. AID PROJECT					



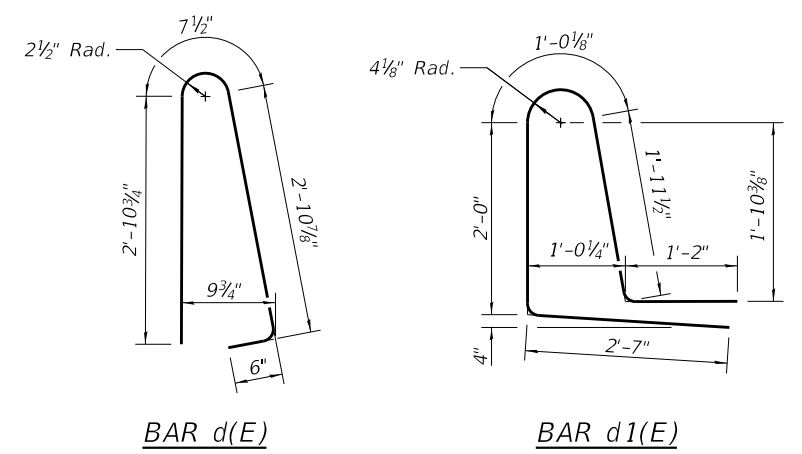
**DIAPHRAGM AT NORTH ABUTMENT**  
(Looking North)



**BARS x(E) thru x4(E)**



**DIAPHRAGM AT SOUTH ABUTMENT**  
(Looking South)

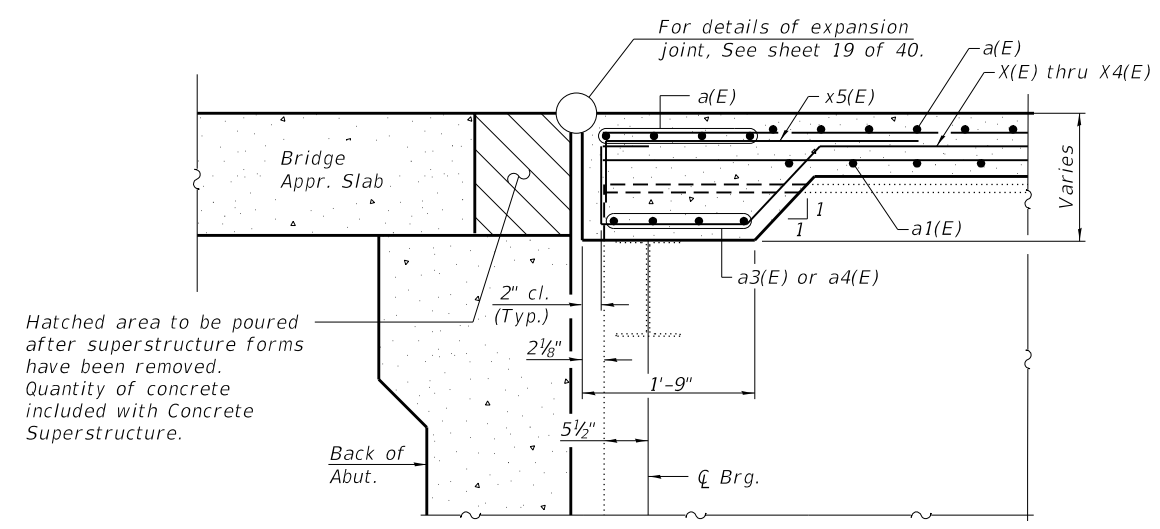


**BAR d(E)**

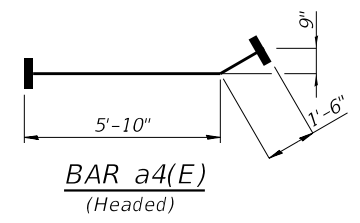
**BAR d1(E)**

**SUPERSTRUCTURE: I-55 N.B.**  
**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	361	#5	42'-6"	—
a1(E)	265	#5	42'-0"	—
a2(E)	706	#6	8'-4"	┌
a3(E)	24	#5	6'-10"	┌
a4(E)	16	#5	7'-4"	┌
a5(E)	64	#5	2'-0"	—
b(E)	322	#5	35'-1"	—
b1(E)	78	#6	46'-6"	—
b2(E)	328	#5	31'-1"	—
d(E)	674	#5	7'-0"	┌
d1(E)	674	#5	8'-9"	┌
e(E)	96	#4	19'-0"	—
e1(E)	96	#4	10'-10"	—
e2(E)	64	#4	15'-7"	—
e3(E)	32	#4	30'-0"	—
e4(E)	16	#4	33'-0"	—
x(E)	16	#5	6'-6"	┌
x1(E)	24	#5	6'-11"	┌
x2(E)	16	#5	7'-1"	┌
x3(E)	8	#5	7'-4"	┌
x4(E)	16	#5	7'-6"	┌
x5(E)	86	#5	4'-4"	┌
Reinforcement Bars, Epoxy Coated		Lbs.	80360	
Concrete Superstructure		Cu. Yds.	352.4	

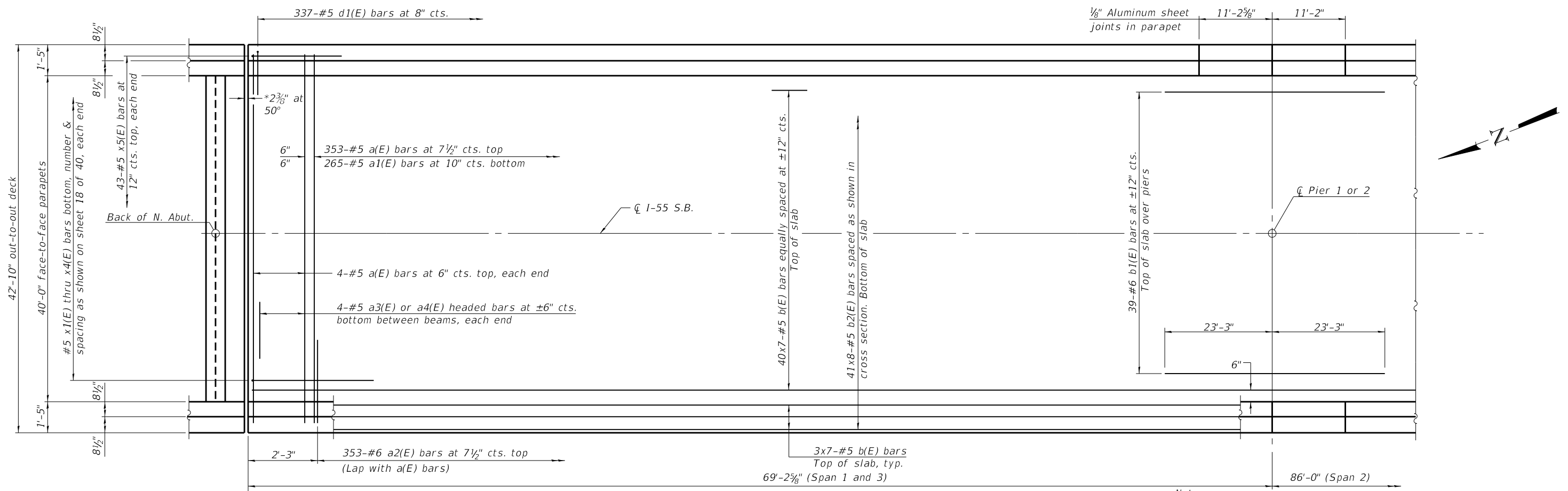


**SECTION A-A**



**BAR a4(E)**  
(Headed)

**Notes:**  
Tilt and/or Bend x(E) thru x4(E) bars in field to fit.  
Headed bars shall conform to ASTM A970 with threaded attachment;  
Class HA; and reinforcement bars conforming to ASTM A706. Cost included with Reinforcement Bars, Epoxy Coated.  
See sheet 14 of 40 for additional superstructure details.



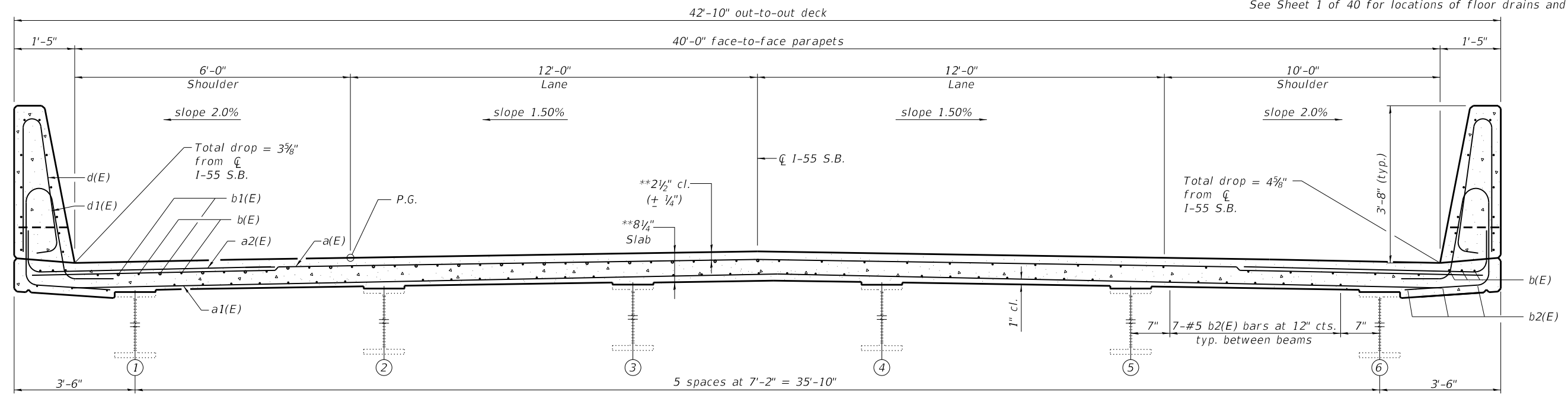
**MINIMUM BAR LAP**

#5 bar = 3'-6"

\* Dimension showing concrete opening. For joint opening see sheet 19 of 40.

**PARTIAL PLAN**  
(Symmetric about the center of Span 2)

Notes:  
See Sheet 17 & 18 of 40 for superstructure details.  
See Sheet 18 of 40 for Bill of Material.  
Bars indicated thus 40x7-#5 etc. indicates 40 lines of bars with 7 lengths per line.  
Dimensions are based on a Rolled Rail Strip Seal Joint. If the Contractor elects to use the Welded Rail Strip Seal Joint, the deck dimensions may require adjustments to satisfy the details on Sheet 19 of 40.  
See Sheet 1 of 40 for locations of floor drains and DS-12 Scuppers.

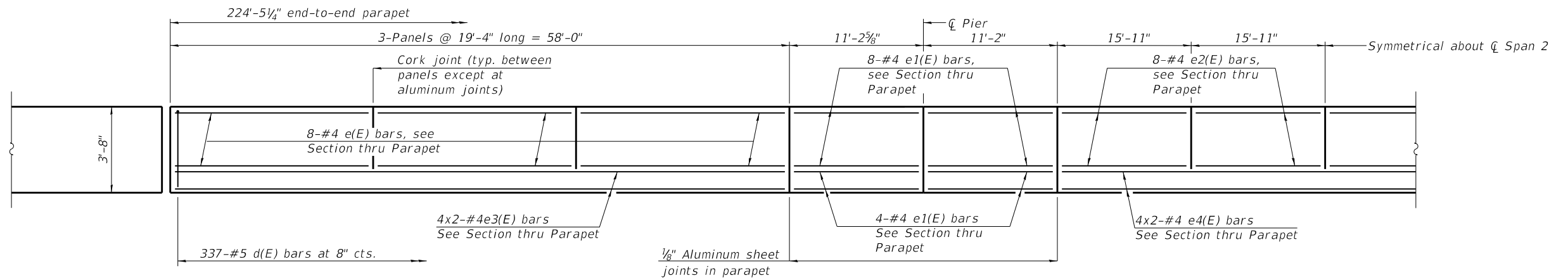


**NEAR PIER**

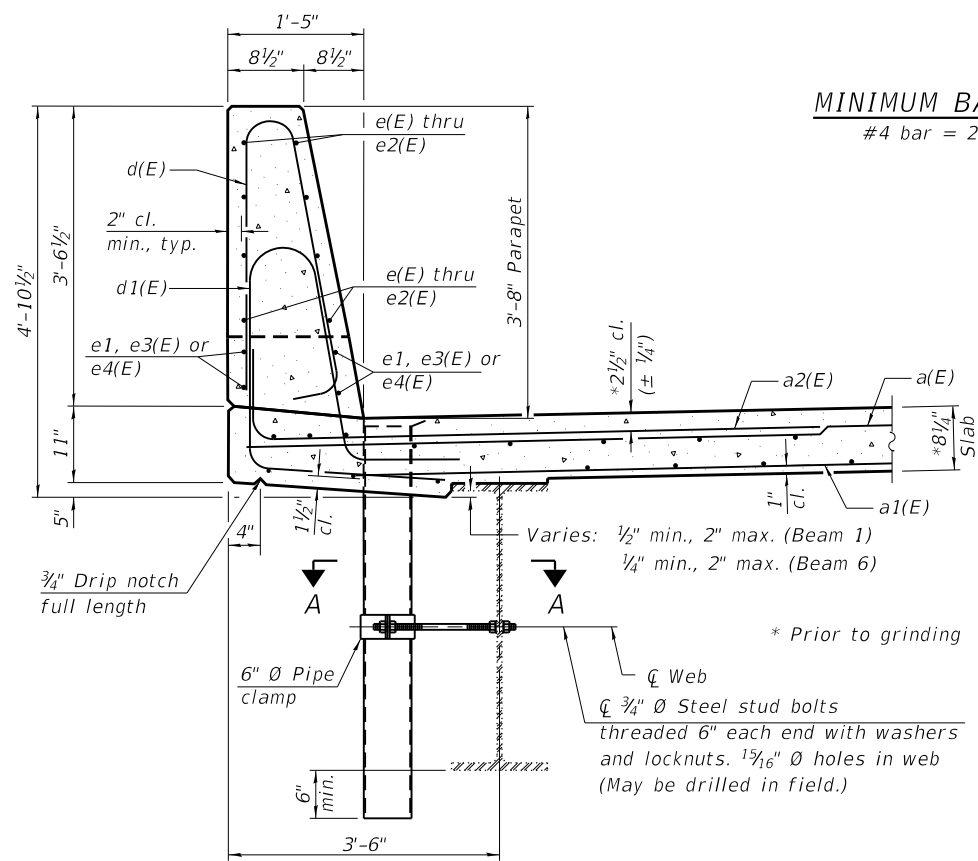
**CROSS SECTION**  
(Looking South)

**NEAR MIDSPAN**

FILE NAME = 016-Superstructure SB <b>BFW</b> BACON   FARMER   WORKMAN ENGINEERING & TESTING, INC. <small>403 NORTH COURT STREET          WAPLETALE, IL 60494          PHONE - 630.987.0100</small>	USER NAME =	DESIGNED - CMV	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>SUPERSTRUCTURE</b> <b>STRUCTURE NO. 068-0048 (SB)</b> SHEET NO. 16 OF 40 SHEETS	F.A.I. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	DRAWN - BJV	REVISED -			55	68-1,3 RS-3, 68-2 RS-5)BR	MONTGOMERY	307	195
	PLOT DATE = 1/6/2020	CHECKED - CMV	REVISED -			CONTRACT NO. 72D31		ILLINOIS FED. AID PROJECT		

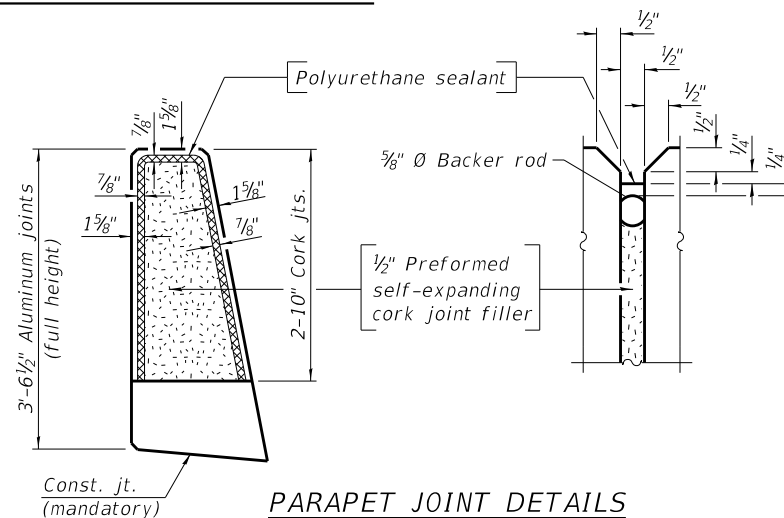


**INSIDE ELEVATION OF PARAPET**

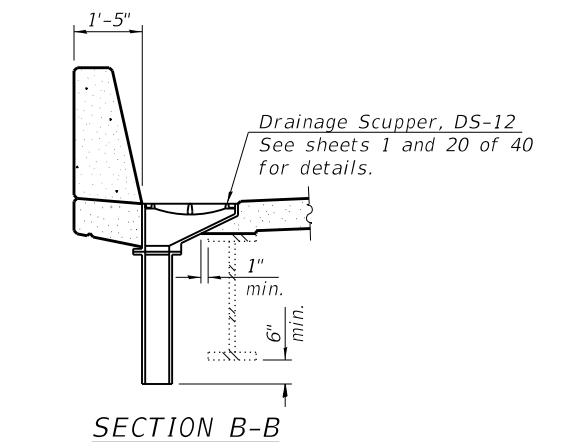


**SECTION THRU PARAPET**

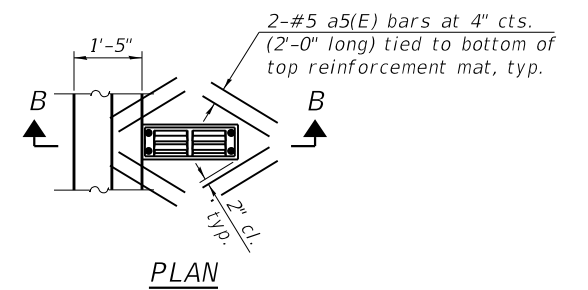
**MINIMUM BAR LAP**  
#4 bar = 2'-5"



**PARAPET JOINT DETAILS**



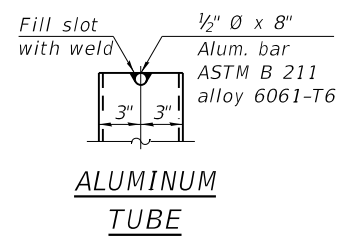
**SECTION B-B**



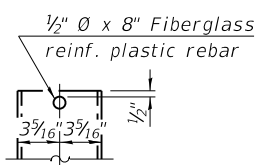
**PLAN**

Note:  
Cut longitudinal reinforcement to clear drainage scuppers.

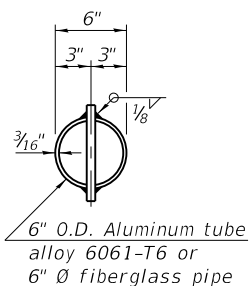
Notes:  
Fiberglass pipe shall conform to ASTM D2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.  
Floor drains need not be painted.  
The top portion of aluminum floor drains shall be coated to minimize reaction with wet concrete.  
The clamping device shall be galvanized according to AASHTO M 232. Cost of clamping device included with Floor Drains.  
The 1/8" aluminum sheet shall be ASTM B 209 alloy 3003-H14 and coated to minimize reaction with wet concrete. Cost included with Concrete Superstructure.  
The polyurethane sealant shall be according to Article 1050.04 of the Std. Spec. and the color shall be gray.  
Bars indicated thus 4x2-#4 etc. indicates 4 lines of bars with 2 lengths per line.  
Dimensions are based on a Rolled Rail Strip Seal Joint. If the Contractor elects to use the Welded Rail Strip Seal Joint, the deck dimensions may require adjustments to satisfy the details on Sheet 19 of 40.  
See Sheet 18 of 40 for additional superstructure details and Bill of Material.  
Drains shall be located clear of all cross frames.



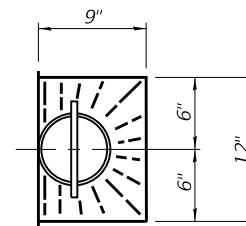
**ALUMINUM TUBE**



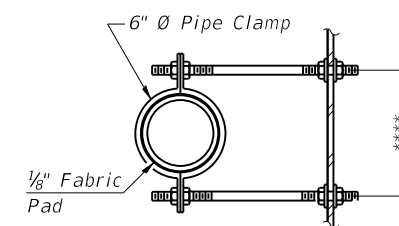
**FIBERGLASS PIPE**



**TOP PLAN**  
(Showing aluminum tube)



**TOP PLAN**

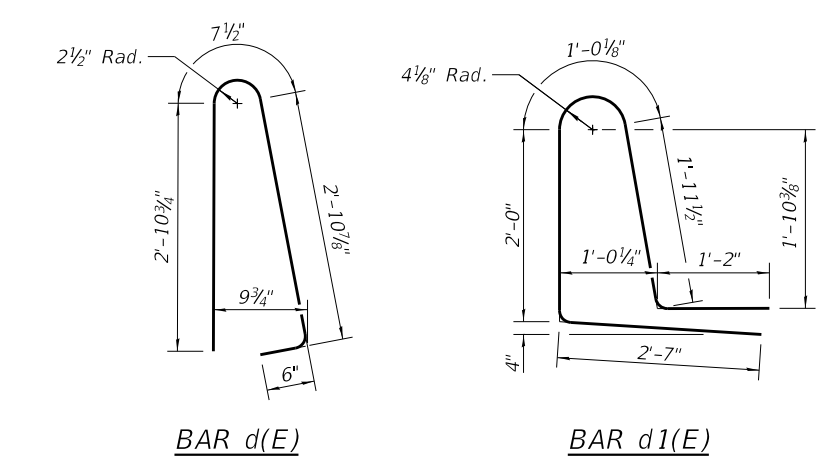
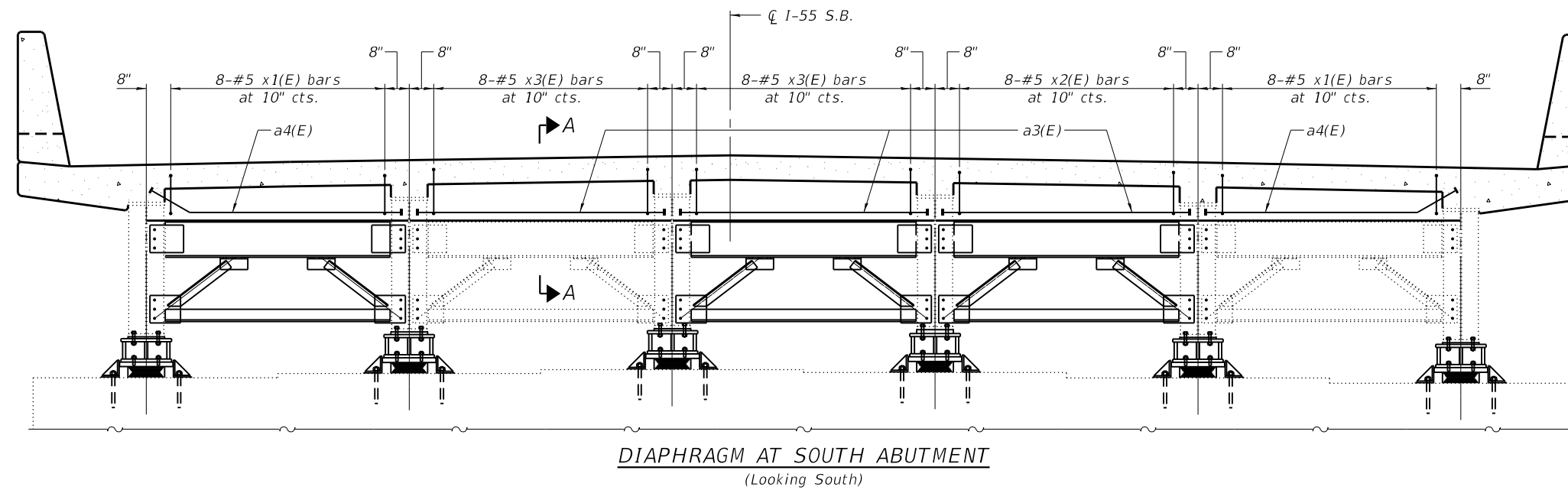
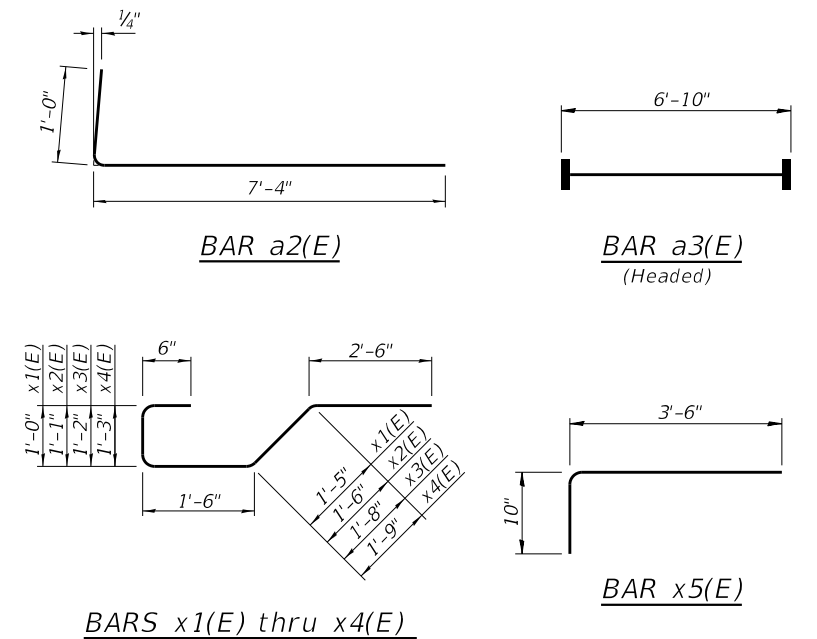
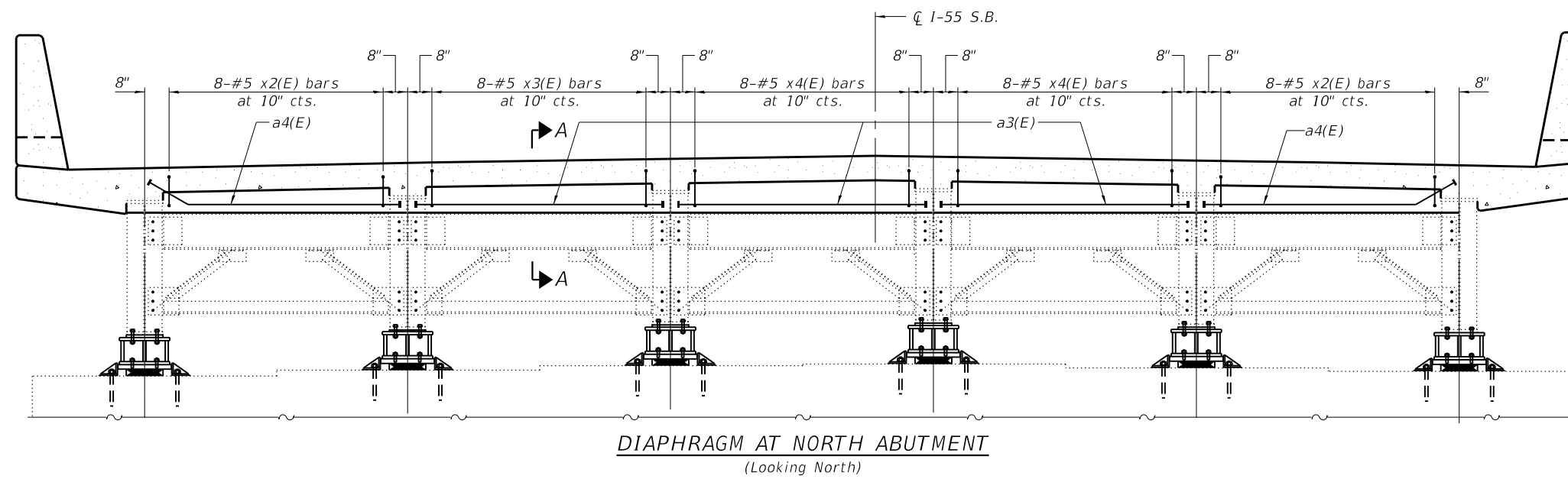


**SECTION A-A**

\*\*\*Dimension as required by Pipe Clamp

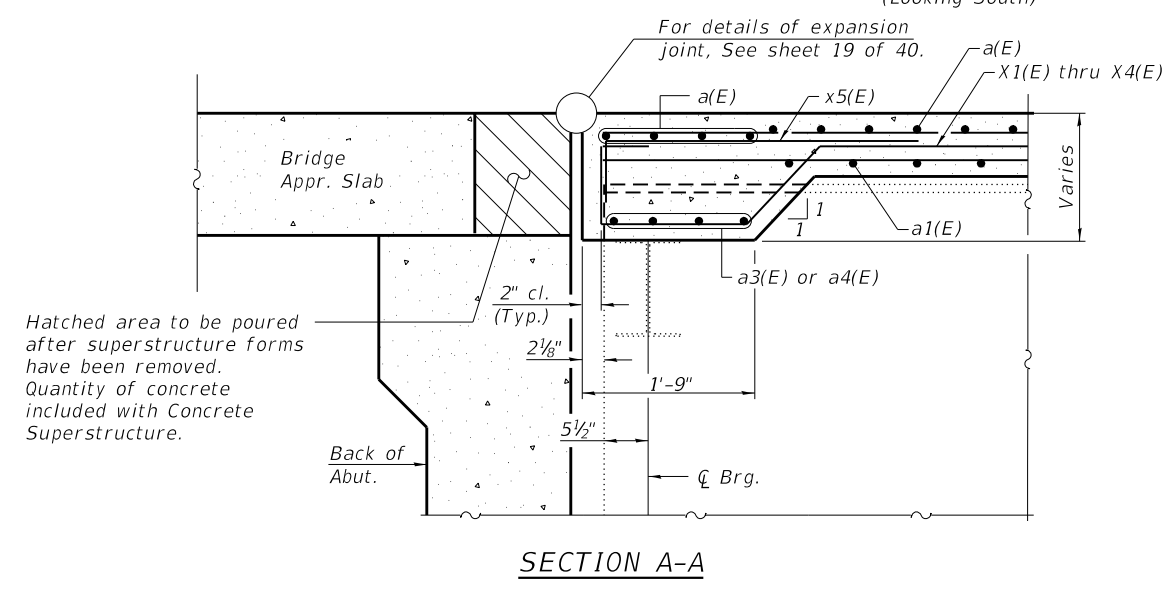
FILE NAME = 017-Superstructure Details SB <b>BFW</b> BACON   FARMER   WORKMAN ENGINEERING & TESTING, INC. 803 NORTH COURT STREET NAPERVILLE, IL 60563 PHONE = 630.987.9100	USER NAME =	DESIGNED - CMV	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>SUPERSTRUCTURE DETAILS</b> <b>STRUCTURE NO. 068-0048 (SB)</b>	F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	CHECKED - GBR	REVISED -			55	68-1,3 RS-3, 68-2 RS-5)BR	MONTGOMERY	307	196
PLOT DATE = 1/6/2020	DRAWN - BJV	CHECKED - CMV	REVISED -	SHEET NO. 17 OF 40 SHEETS		CONTRACT NO. 72D31		ILLINOIS FED. AID PROJECT		



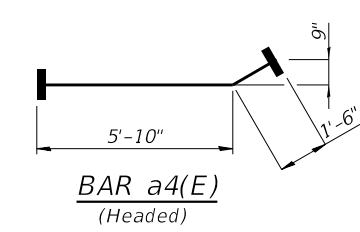


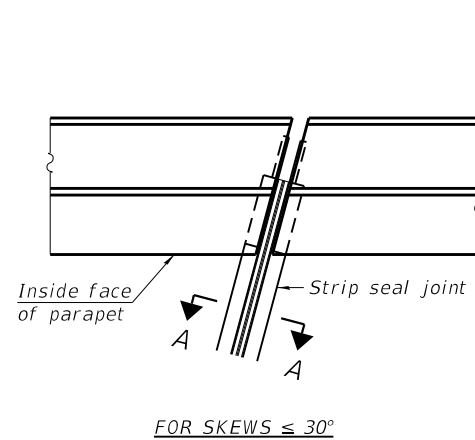
**SUPERSTRUCTURE: I-55 S.B.**  
**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	361	#5	42'-6"	—
a1(E)	265	#5	42'-0"	—
a2(E)	706	#6	8'-4"	┌
a3(E)	24	#5	6'-10"	—
a4(E)	16	#5	7'-4"	└
a5(E)	64	#5	2'-0"	—
b(E)	322	#5	35'-1"	—
b1(E)	78	#6	46'-6"	—
b2(E)	328	#5	31'-1"	—
d(E)	674	#5	7'-0"	┌
d1(E)	674	#5	8'-9"	└
e(E)	96	#4	19'-0"	—
e1(E)	96	#4	10'-10"	—
e2(E)	64	#4	15'-7"	—
e3(E)	32	#4	30'-0"	—
e4(E)	16	#4	33'-0"	—
x1(E)	16	#5	6'-11"	└
x2(E)	24	#5	7'-1"	└
x3(E)	24	#5	7'-4"	└
x4(E)	16	#5	7'-6"	└
x5(E)	86	#5	4'-4"	└
Reinforcement Bars, Epoxy Coated			Lbs.	80370
Concrete Superstructure			Cu. Yds.	354.4



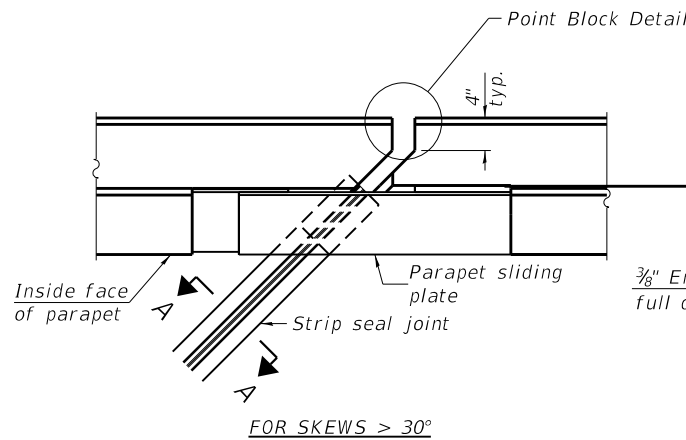
**Notes:**  
Tilt and/or Bend x1(E) thru x4(E) bars in field to fit.  
Headed bars shall conform to ASTM A970 with threaded attachment;  
Class HA; and reinforcement bars conforming to ASTM A706. Cost included with Reinforcement Bars, Epoxy Coated.  
See sheet 17 of 40 for additional superstructure details.



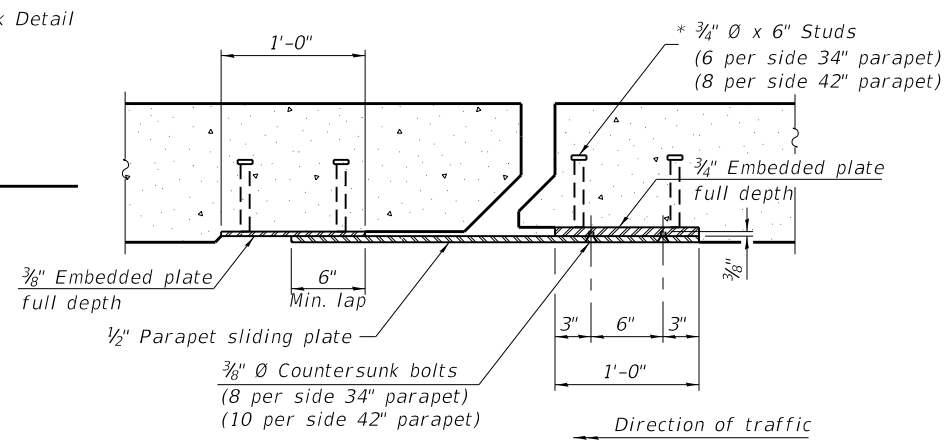


FOR SKEWS  $\leq 30^\circ$

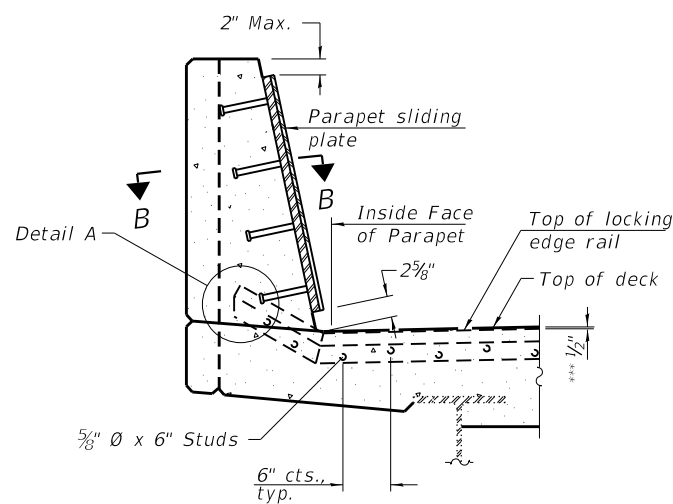
PLAN AT PARAPET



FOR SKEWS  $> 30^\circ$



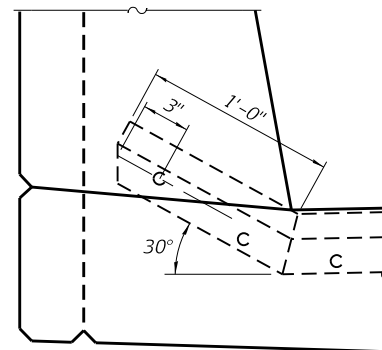
SECTION B-B



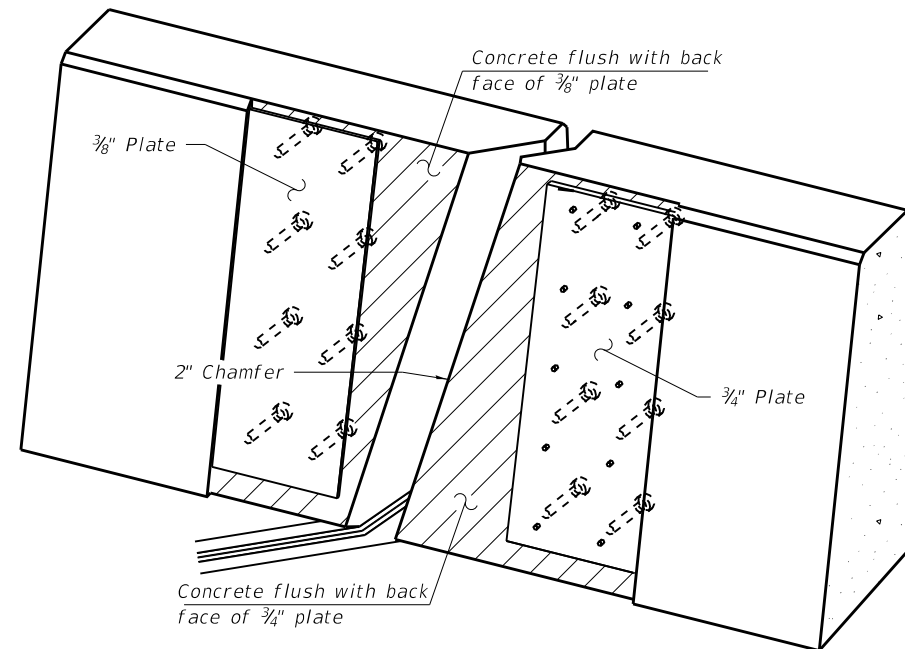
ELEVATION AT PARAPET

(Skews  $> 30^\circ$  shown. Skews  $\leq 30^\circ$  similar except as shown in plan view.)

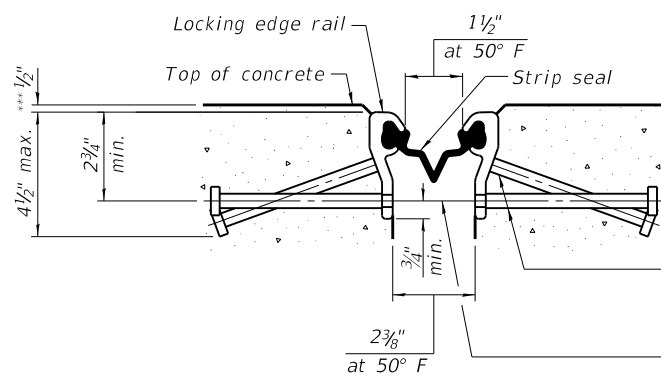
\*\*\*Prior to grinding



DETAIL A



TRIMETRIC VIEW  
(Showing embedded plates only)



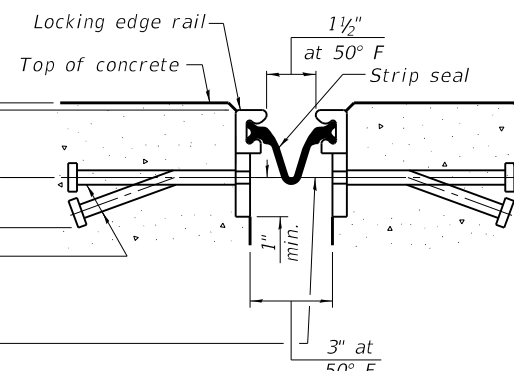
SHOWING ROLLED RAIL JOINT

\*  $\frac{5}{8}$ "  $\phi$  x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

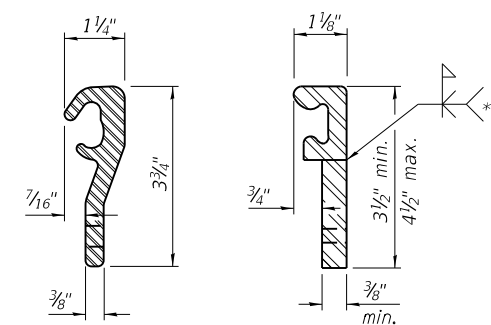
$\frac{3}{8}$ "  $\phi$  threaded rods in  $\frac{7}{16}$ "  $\phi$  holes at  $\pm 4$ "-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

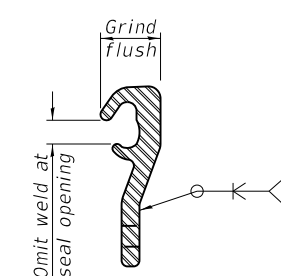


SHOWING WELDED RAIL JOINT



LOCKING EDGE RAILS

\*\* Back gouge not required if complete joint penetration is verified by mock-up.

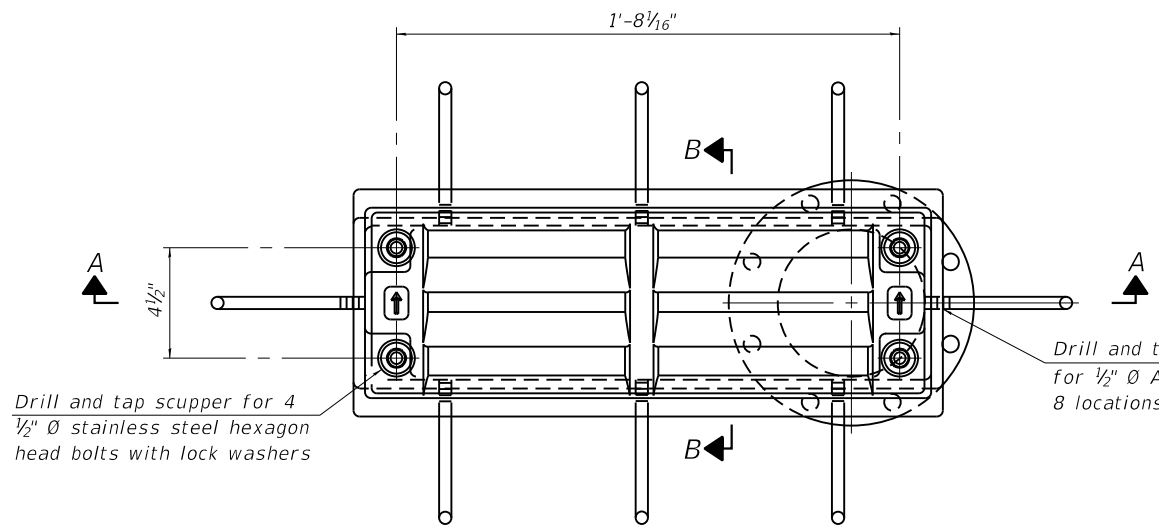


LOCKING EDGE RAIL SPLICE

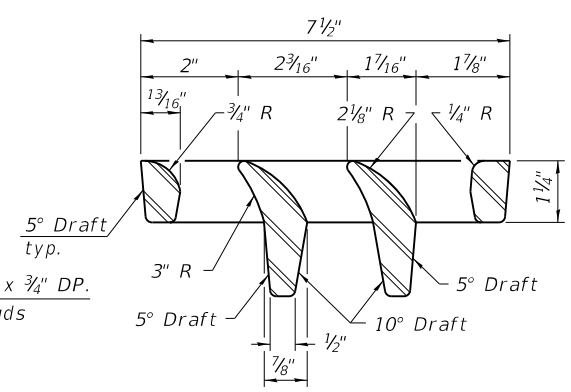
The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL (BOTH STRUCTURES)

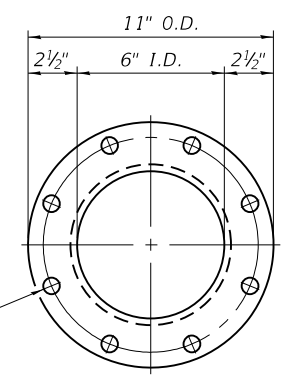
Item	Unit	Total
Preformed Joint Strip Seal	Foot	168



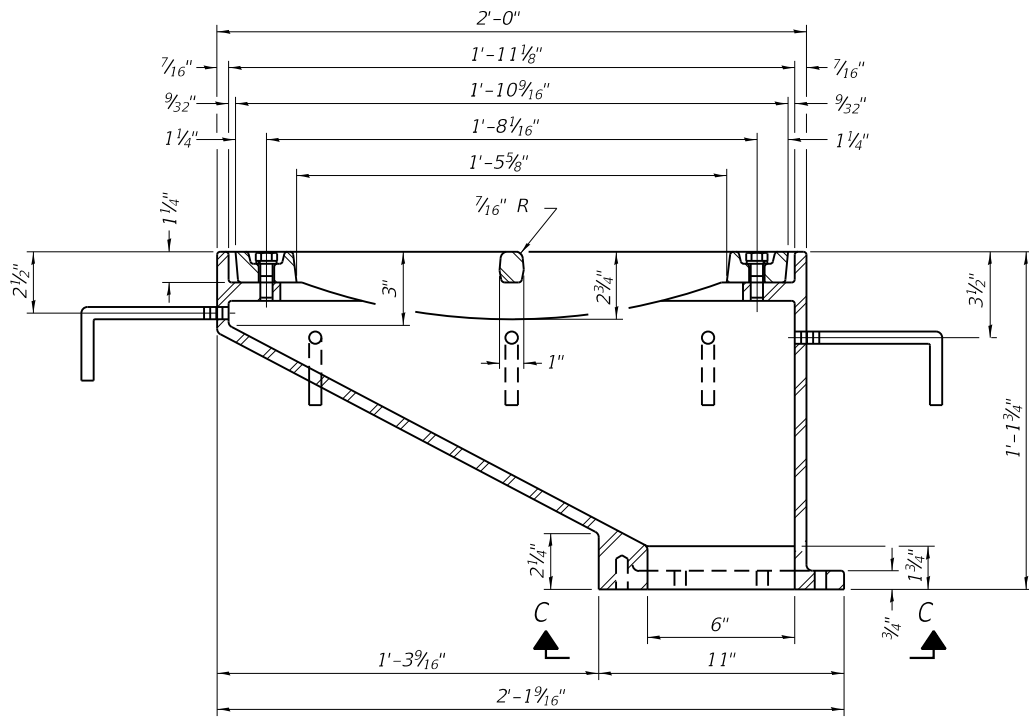
PLAN



VANE GRATE DETAIL

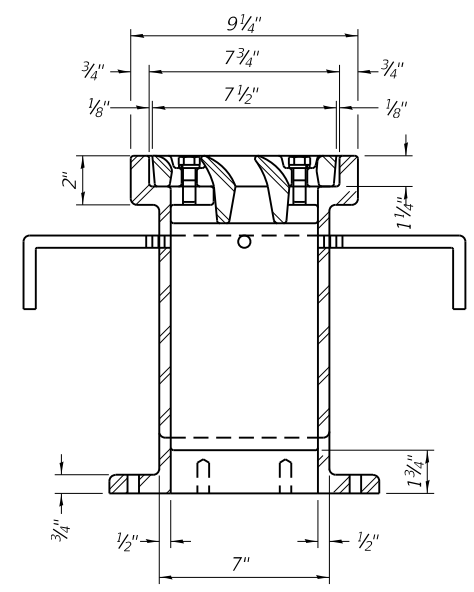


VIEW C-C

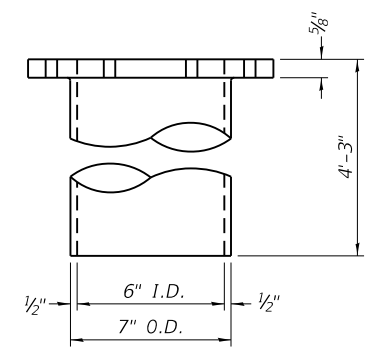


SECTION A-A

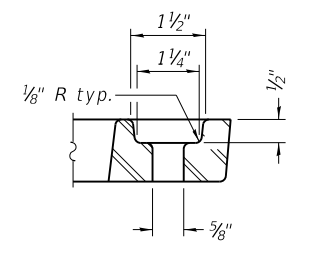
See sheet 14 & 17 of 40 for scupper location relative to parapet.



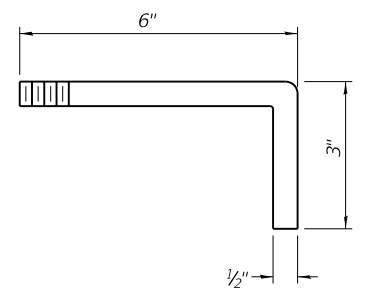
SECTION B-B



DOWNSPOUT



BOLT HOLE DETAIL



ANCHOR STUD DETAIL

Drill and tap 8 holes for 1/2"-13 bolts on a 9 1/2" Ø bolt circle. (2 blind holes are 1 1/4" deep, 6 thru holes)

Notes:  
 All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 35B.  
 Bolts, anchor studs, washers and nuts shall conform to the requirements of ASTM A 307 and shall be galvanized according to AASHTO M 232.  
 Downspouts need not be painted.  
 As an alternate, bolts, anchor studs, washers and nuts may be stainless steel according to Article 1006.29(d) of the Standard Specifications.  
 Structural steel weldments of equal sections and of the same configuration may be substituted for the cast iron scupper frame. Fillet or full penetration welds shall be used for the weldments. Details shall be submitted to the Engineer for approval. Structural steel weldments shall not be substituted for the cast iron scupper grate. Structural steel frames and downspouts shall be galvanized according to AASHTO M111.  
 The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the scupper.  
 Cost of the Grate, Frame, Downspout, Anchor Studs, Bolts, Washers and Nuts including complete installation of the scupper shall be paid for at the contract unit price each for Drainage Scupper, DS-12.  
 Alternate fiberglass downspout conforming to ASTM D 2996 with a short-time rupture strength hoop tensile stress of 30,000 psi min. may be used in lieu of the cast iron or steel equivalent.

BILL OF MATERIAL (BOTH STRUCTURES)

ITEM	UNIT	QUANTITY
Drainage Scupper, DS-12	Each	8

DS-12

2-17-2017

FILE NAME = 020-Drainage Scupper  
 BACON | FARMER | WORKMAN  
 ENGINEERING & TESTING, INC.  
 403 NORTH COURT STREET  
 NAPERVILLE, IL 60563  
 PHONE - 630.957.9100

USER NAME =	DESIGNED - CMV	REVISED -
PLOT SCALE =	CHECKED - GBR	REVISED -
PLOT DATE = 1/6/2020	DRAWN - BJV	REVISED -
	CHECKED - GBR	REVISED -

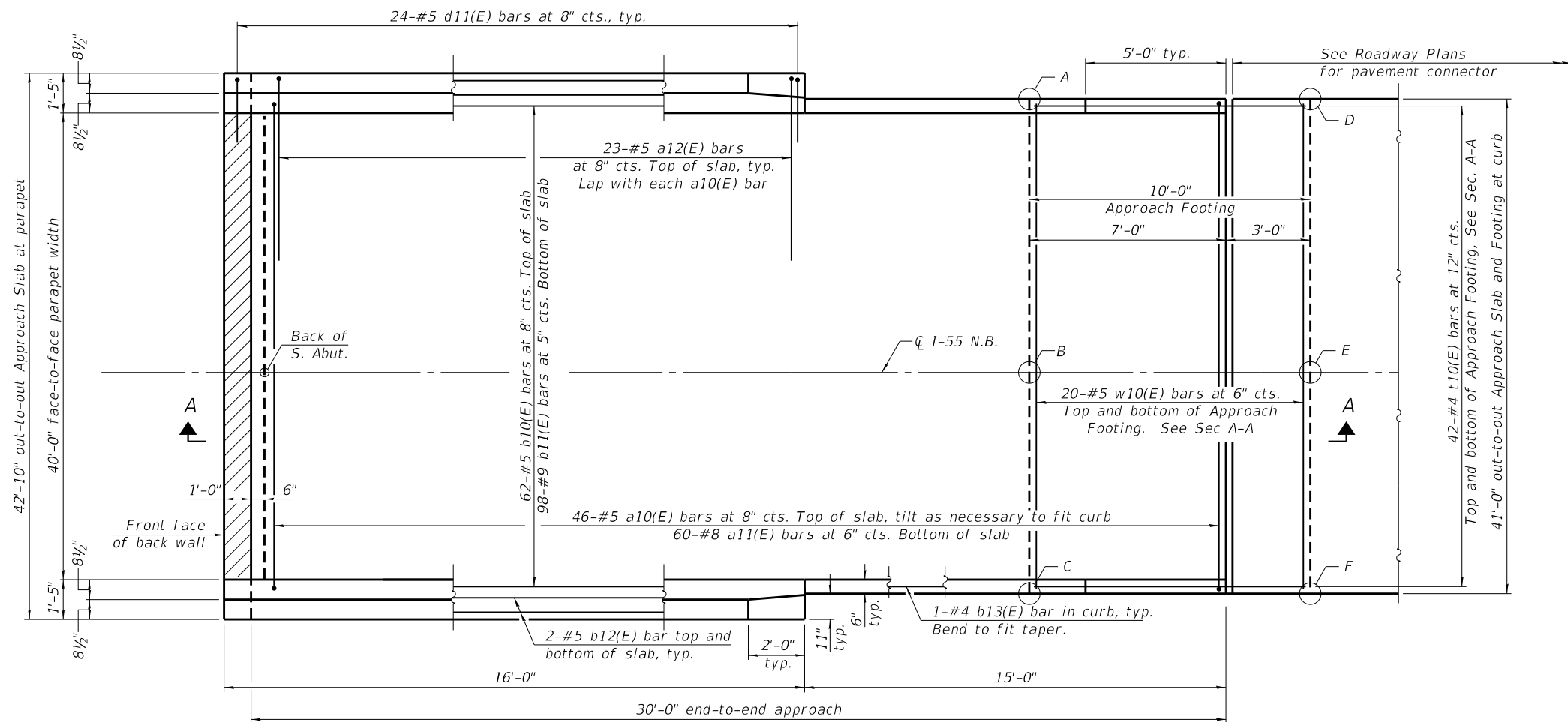
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

DRAINAGE SCUPPER, DS-12  
 STRUCTURE NO. 068-0048 (SB) & 068-0049 (NB)

SHEET NO. 20 OF 40 SHEETS

F.A.I. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	68-1.3 RS-3, 68-2 RS-5JBR	MONTGOMERY	307	199
CONTRACT NO. 72D31				

ILLINOIS FED. AID PROJECT

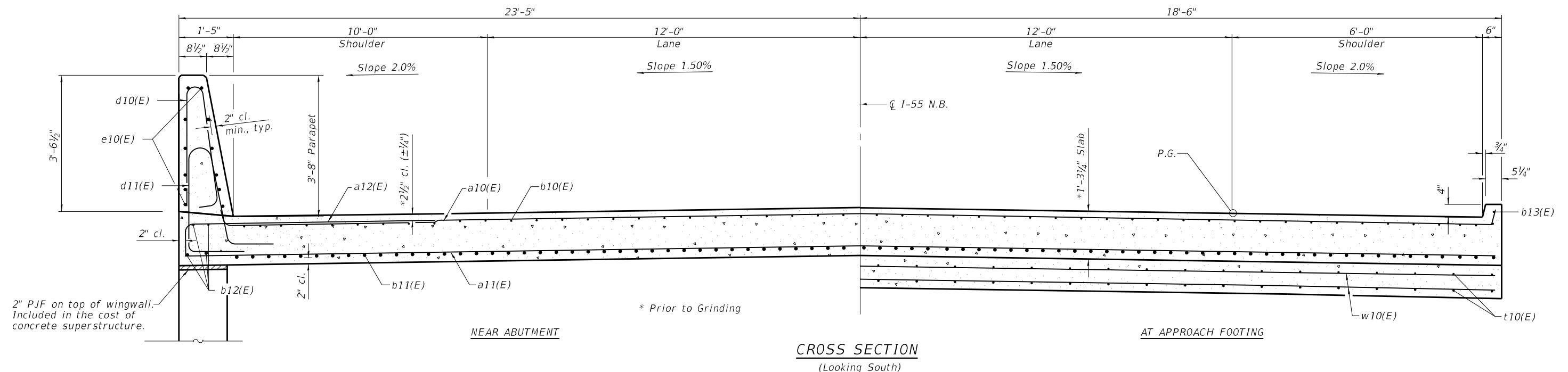
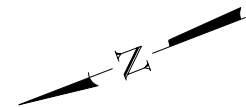


**TOP AND BOTTOM ELEVATIONS  
FOR APPROACH FOOTING  
SN 068-0049 (I-55 N.B.)**

Point	North Approach		South Approach	
	Top	Bottom	Top	Bottom
A	700.29	699.46	700.44	699.61
B	700.68	699.85	700.83	700.00
C	700.37	699.54	700.52	699.69
D	700.25	699.42	700.41	699.58
E	700.64	699.81	700.80	699.97
F	700.33	699.50	700.49	699.66

**PLAN**

(South Approach (SN 068-0049) shown,  
North Approach (SN 068-0049) similar  
by Mirror Image)



**CROSS SECTION**  
(Looking South)

(Sheet 1 of 2)

FILE NAME = 021-App Slab Details NB	USER NAME =	DESIGNED - CMV	REVISED -
BACON   FARMER   WORKMAN ENGINEERING & TESTING, INC.		CHECKED - GBR	REVISED -
	PLOT SCALE =	DRAWN - BJV	REVISED -
	PLOT DATE = 1/6/2020	CHECKED - CMV	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BRIDGE APPROACH SLAB DETAILS  
STRUCTURE NO. 068-0049 (NB)**

SHEET NO. 21 OF 40 SHEETS

F.A.I. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	68-1.3 RS-3, 68-2 RS-5)BR	MONTGOMERY	307	200
CONTRACT NO. 72D31				

ILLINOIS FED. AID PROJECT