

LEGEND

SECTION CORNER 16 15
QUARTER SECTION CORNER SECTION 113 & 114 R-5
CONTRACT NO. 62417

SECTION LINE
QUARTER SECTION LINE
PLATTED LOT LINE
PROPERTY (DEED) LINE
APL
APPARENT PROPERTY LINE
CENTER LINE
EXISTING RIGHT OF WAY LINE
PROPOSED RIGHT OF WAY LINE
PROPOSED EASEMENT
MEASURED DIMENSION
COMPUTED DIMENSION
RECORD DATA
EXISTING BUILDING

Bearings are referenced to the Illinois Coordinate System NAD83 (1997) East Zone at Found Geodetic Survey Control Monuments "Will County GPS 316", P.I.D. AE2551 and "Will County GPS 333", P.I.D. AE2553.

IRON PIPE OR ROD FOUND
CUT CROSS FOUND OR SET
THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
RIGHT OF WAY STAKING PROPOSED TO BE SET.

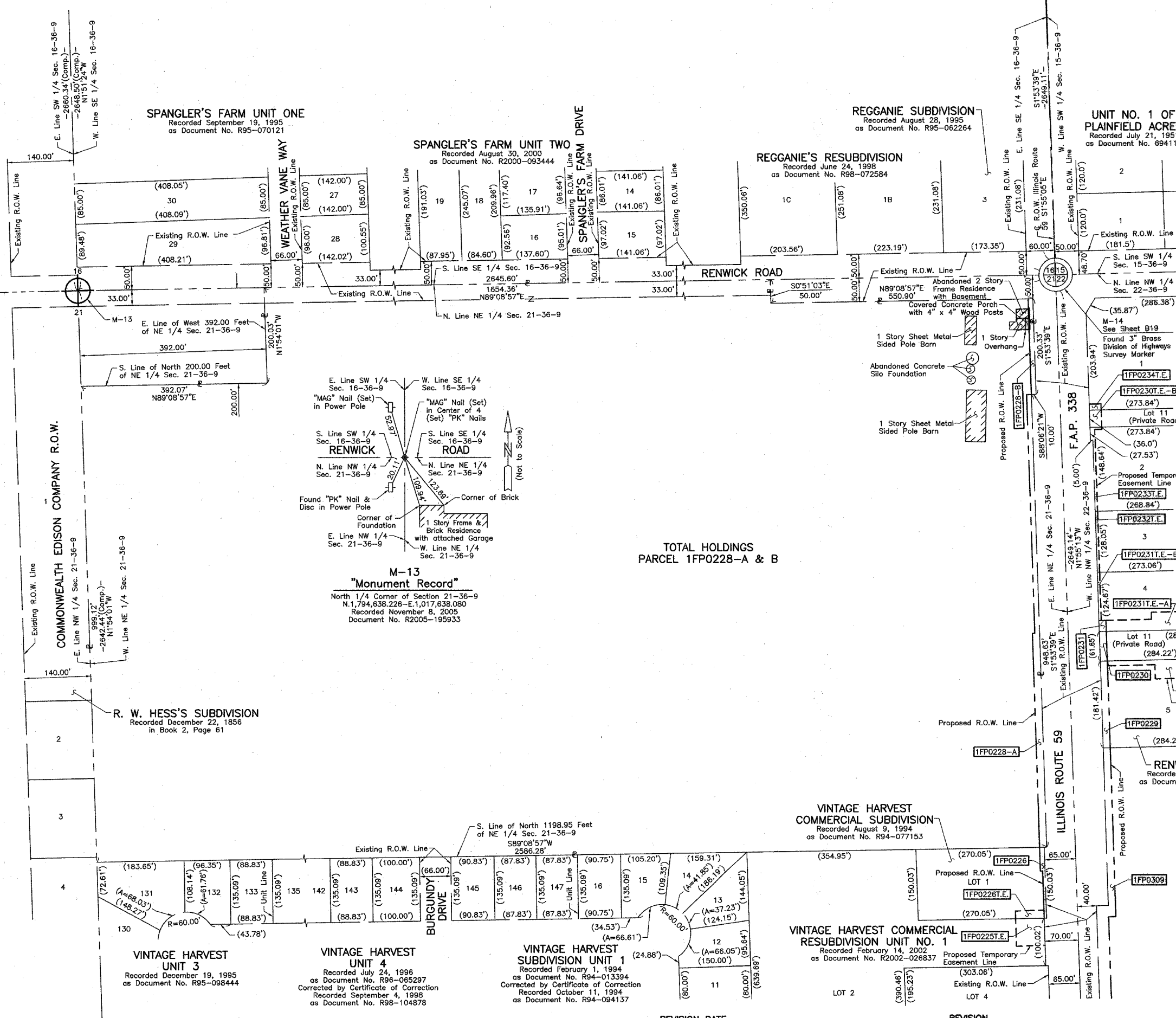
STATE OF ILLINOIS }
COUNTY OF LAKE }SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 16, TOWNSHIP 36N., RANGE 9E. AND SECTION 21, TOWNSHIP 36N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 9th DAY OF November 2005 A.D.

Christian H. Jorgensen PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRATION DATE: NOVEMBER 30, 2006
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
Coordinates are based on the Published Metric Coordinate Values at Found Geodetic Survey Control Monument "Will County GPS 333", P.I.D. AE2553, of N.542,285.095-E.311,129.617.
NOTE: SURFACE COORDINATES ARE SHOWN.

SCALE: 1"=100'



**M-13
"Monument Record"**
North 1/4 Corner of Section 21-36-9
N.1,794,638.226-E.1,017,638.080
Recorded November 8, 2005
Document No. R2005-195933

TOTAL HOLDINGS
PARCEL 1FP0228-A & B



SCANNED

RECEIVED
JUN 2 9 2006
PLATS & LEGALS

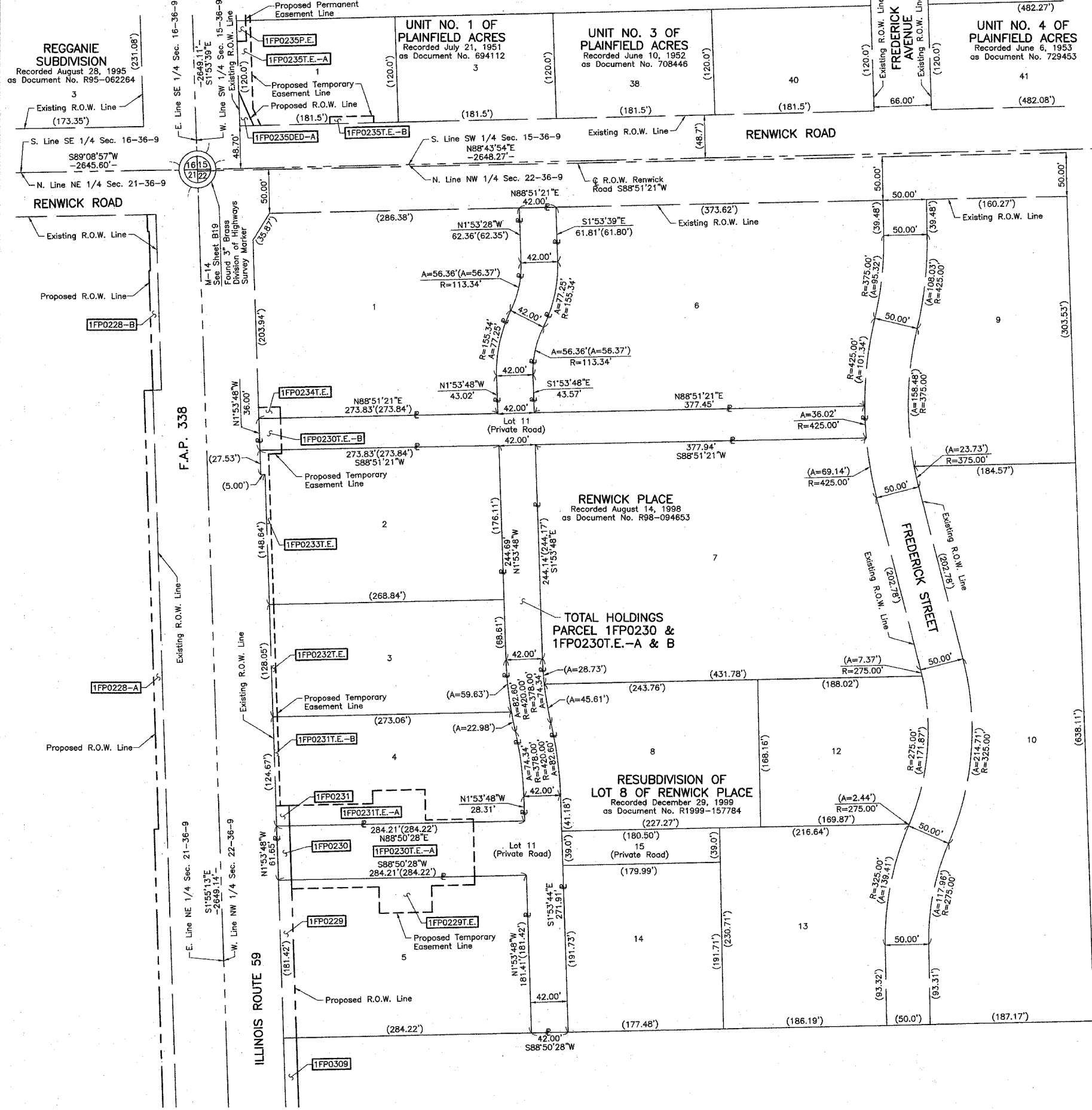
JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60046
(847) 356-3371

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
F.A.P. 338 (ILLINOIS ROUTE 59)

SECTION WILL COUNTY
PROJECT JOB NO. R-91-067-01
STATION NONE TO STATION
SCALE: 1"=100' SHEET 851 OF 858

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

BY	DATE
MADE	
CHECKED	
INVEST	
NO	



LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION LINE
- QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APL APPARENT PROPERTY LINE
- CENTER LINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORD DATA
- EXISTING BUILDING

Bearings are referenced to the Illinois Coordinate System NAD83 (1997) East Zone at Found Geodetic Survey Control Monuments "Will County GPS 316", P.I.D. AE2551 and "Will County GPS 333", P.I.D. AE2553.

- IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
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- ⊙ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }
COUNTY OF LAKE }SS

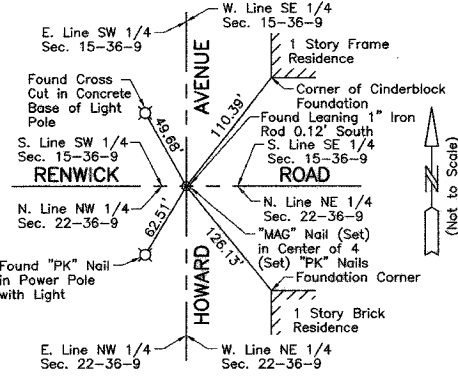
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DATED AT LAKE VILLA, ILLINOIS THIS 9th DAY OF November 2005 A.D.

Christian H. Jorgensen PRESIDENT
2797 PROFESSIONAL LAND SURVEYOR
STATE OF ILLINOIS
LAKE VILLA, ILLINOIS

2797 LICENSE EXPIRATION DATE: NOVEMBER 30, 2006
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
Coordinates are based on the Published Metric Coordinate Values at Found Geodetic Survey Control Monument "Will County GPS 333", P.I.D. AE2553, of N.542,285.095-E.311,129.617.
NOTE: SURFACE COORDINATES ARE SHOWN.

ROW PLAT	DATE	BY
MADE		
RECHECKED		
NOTED		
NO		



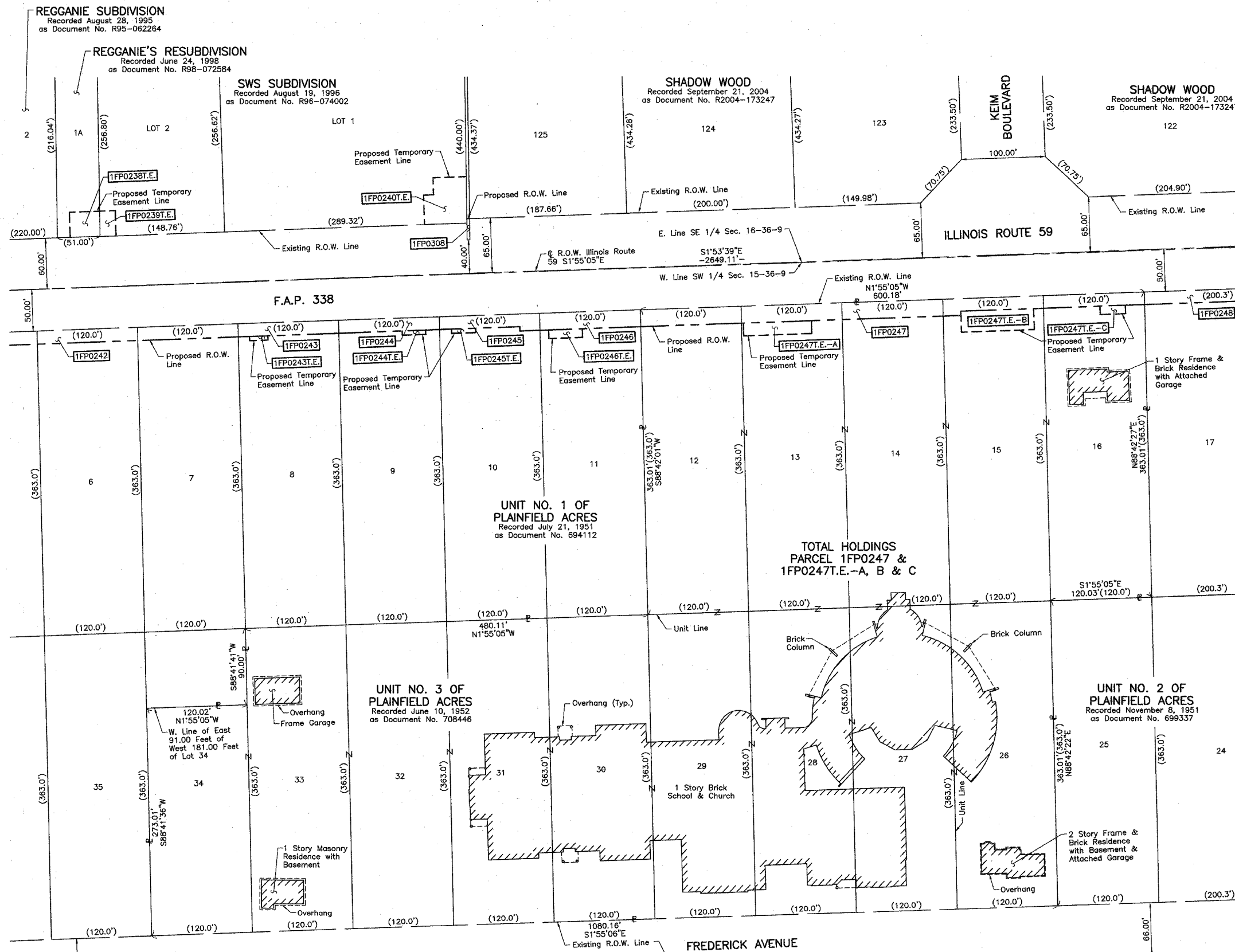
M-15 "Monument Record"
South 1/4 Corner of Section 15-36-9
N.1,794,736.123-E.1,022,931.002
Recorded November 8, 2005
Document No. R2005-195931

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
F.A.P. 338 (ILLINOIS ROUTE 59)

SECTION WILL COUNTY
PROJECT JOB NO. R-91-067-01
STATION NONE TO STATION
SCALE: 1"=60' SHEET 852 OF 858

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

PART OF THE SW 1/4 OF SEC. 15 AND PART OF THE SE 1/4 OF SEC. 16, TWP. 36 N., R. 9 E. OF THE 3RD. P.M., IN WILL COUNTY, ILLINOIS.



LEGEND

SECTION CORNER: 910 1815

QUARTER SECTION CORNER: 16 15

SECTION (113 & 114) R-5: 525 303

CONTRACT NO. 62417

SECTION LINE
 QUARTER SECTION LINE
 QUARTER, QUARTER SECTION LINE
 PLATTED LOT LINE
 PROPERTY (DEED) LINE

APL APPARENT PROPERTY LINE
 CENTER LINE
 EXISTING RIGHT OF WAY LINE
 PROPOSED RIGHT OF WAY LINE
 PROPOSED EASEMENT
 MEASURED DIMENSION
 COMPUTED DIMENSION
 RECORD DATA

EXISTING BUILDING

Bearings are referenced to the Illinois Coordinate System NAD83 (1997) East Zone at Found Geodetic Survey Control Monuments "Will County GPS 316", P.I.D. AE2551 and "Will County GPS 333", P.I.D. AE2553.

○ IRON PIPE OR ROD FOUND ⊗ "MAG" NAIL SET
 + CUT CROSS FOUND OR SET ● 5/8" REBAR SET

● T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
 T2
 T3

● BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
 BT2
 BT3

■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

⊕ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)

□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }
 COUNTY OF LAKE }SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 15, TOWNSHIP 36N., RANGE 9E. AND SECTION 16, TOWNSHIP 36N., RANGE 9E. OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 9th DAY OF November 2006 A.D.

REGGANIE SUBDIVISION
 Recorded August 28, 1995
 as Document No. R95-062264

REGGANIE'S RESUBDIVISION
 Recorded June 24, 1998
 as Document No. R98-072584

SWS SUBDIVISION
 Recorded August 19, 1996
 as Document No. R96-074002

SHADOW WOOD
 Recorded September 21, 2004
 as Document No. R2004-173247

SHADOW WOOD
 Recorded September 21, 2004
 as Document No. R2004-173247

ILLINOIS ROUTE 59

F.A.P. 338

UNIT NO. 1 OF PLAINFIELD ACRES
 Recorded July 21, 1951
 as Document No. 694112

TOTAL HOLDINGS PARCEL 1FP0247 & 1FP0247T.E.-A, B & C

UNIT NO. 2 OF PLAINFIELD ACRES
 Recorded November 8, 1951
 as Document No. 699337

UNIT NO. 3 OF PLAINFIELD ACRES
 Recorded June 10, 1952
 as Document No. 708446

CHRISTIAN H. JORGENSEN
 2797 PROFESSIONAL LAND SURVEYOR
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2006
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

LAKE VILLA, ILLINOIS

Christian H. Jorgensen - PRESIDENT

JORGENSEN & ASSOCIATES, INC.
 120 PARK AVENUE
 LAKE VILLA, ILLINOIS 60046
 (847) 356-3371

SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

RECEIVED
 DEC 29 2006
 PLATS & LEGALS

REVISION DATE REVISION MADE BY

ROUTE F.A.P. 338 (ILLINOIS ROUTE 59) SECTION COUNTY WILL JOB NO. R-91-067-01 RECORDING: RECORDED ON AS DOCUMENT NO.

**PLAT OF HIGHWAYS
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 F.A.P. 338 (ILLINOIS ROUTE 59)**

SECTION WILL COUNTY
 PROJECT JOB NO. R-91-067-01
 STATION NONE TO STATION
 SCALE: 1"=60' SHEET 853 OF 858

**BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAMBURG, ILLINOIS 60196**

PART OF THE WEST 1/2 OF SEC. 15 AND PART OF THE EAST 1/2 OF SEC. 16, TWP. 36 N., R. 9 E. OF THE 3RD. P.M., IN WILL COUNTY, ILLINOIS.

TOTAL SHEETS 525 SHEET NO. 304

LEGEND

SECTION CORNER: 9 10 16 15

QUARTER SECTION CORNER: 16 15

SECTION (113 & 114) R-5 CONTRACT NO. 62417

SECTION LINE: ---

QUARTER SECTION LINE: - - -

QUARTER SECTION LINE: - - -

PLATTED LOT LINE: - - -

PROPERTY (DEED) LINE: ———

APL: ———

APPARENT PROPERTY LINE: ———

CENTER LINE: ———

EXISTING RIGHT OF WAY LINE: ———

PROPOSED RIGHT OF WAY LINE: ———

PROPOSED EASEMENT: ———

MEASURED DIMENSION: ———

COMPUTED DIMENSION: ———

RECORD DATA: ———

EXISTING BUILDING: [Hatched Box]

120.32' (129.32' Comp.)

SCALE: 1"=60'

Bearings are referenced to the Illinois Coordinate System NAD83 (1997) East Zone at Found Geodetic Survey Control Monuments "Will County GPS 316", P.I.D. AE2551 and "Will County GPS 333", P.I.D. AE2553.

○ IRON PIPE OR ROD FOUND ⊗ "MAG" NAIL SET

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● BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

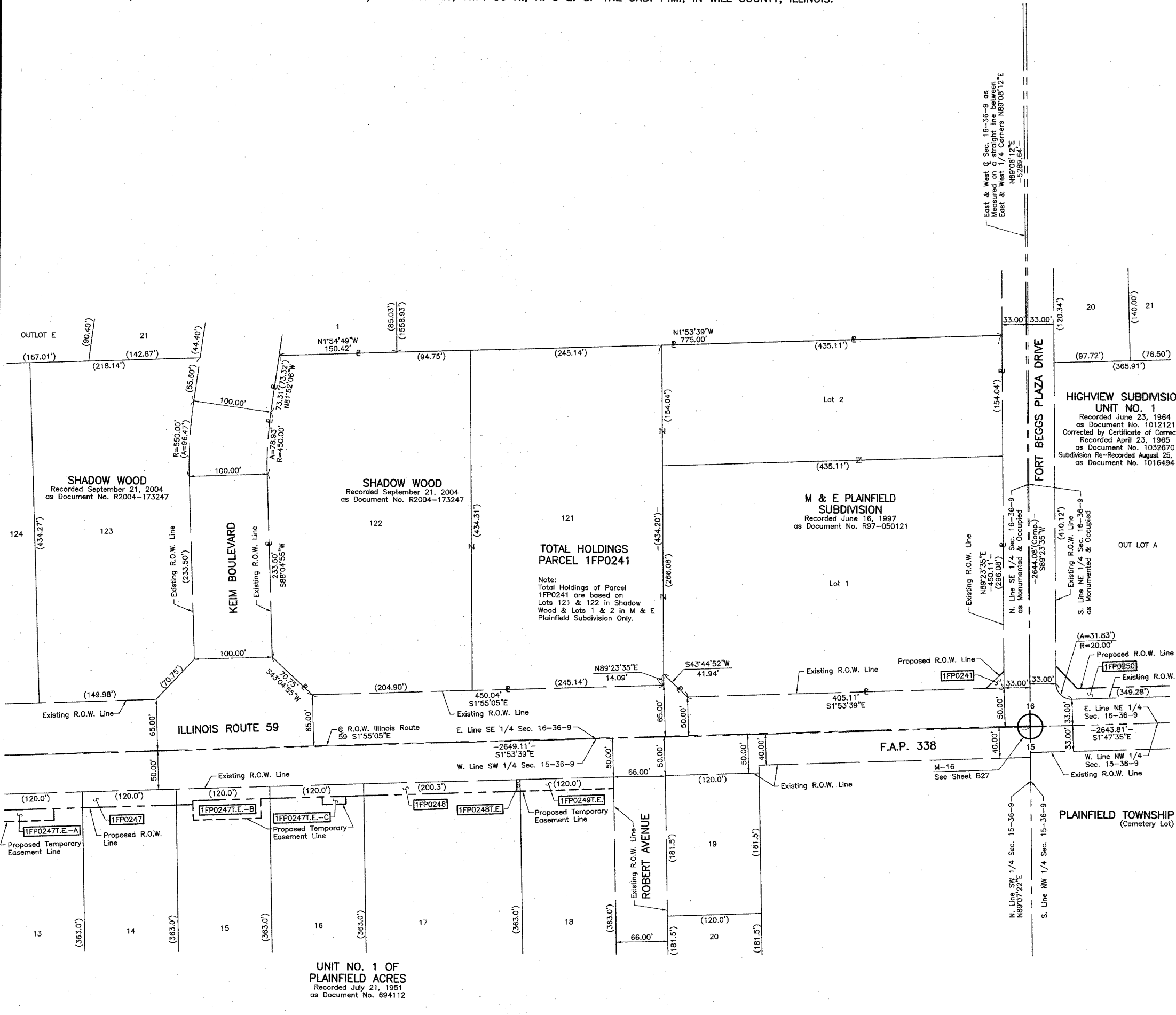
■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

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⊙ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)

□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

ROW PLAT	MADE	CHECKED	LINKED	DATE



HIGHVIEW SUBDIVISION UNIT NO. 1
 Recorded June 23, 1964
 as Document No. 1012121
 Corrected by Certificate of Correction
 Recorded April 23, 1965
 as Document No. 1032670
 Subdivision Re-Recorded August 25, 1964
 as Document No. 1016494



THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 15, TOWNSHIP 36N., RANGE 9E. AND SECTION 16, TOWNSHIP 36N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.
 DATED AT LAKE VILLA, ILLINOIS THIS 9th DAY OF November 2006 A.D.
 Christian H. Jorgensen, PRESIDENT
 ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2006
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

JORGENSEN & ASSOCIATES, INC.
 120 PARK AVENUE
 LAKE VILLA, ILLINOIS 60046
 (847) 356-3371

PLAT OF HIGHWAYS
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 F.A.P. 338 (ILLINOIS ROUTE 59)

SECTION WILL COUNTY
 PROJECT JOB NO. R-91-067-01
 STATION NONE TO STATION
 SCALE: 1"=60' SHEET B54 OF B58

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAUMBURG, ILLINOIS 60196

UNIT NO. 1 OF PLAINFIELD ACRES
 Recorded July 21, 1951
 as Document No. 694112

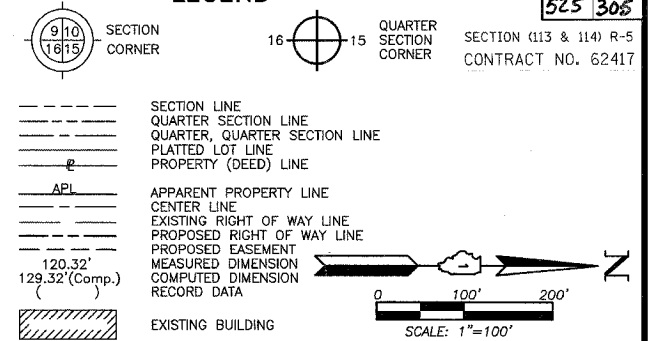
REVISION DATE May 18, 2006

REVISION Total Holdings Parcel 1FP0241

MADE BY

PART OF THE SE 1/4 OF SEC. 9, PART OF THE SW 1/4 OF SEC. 10, PART OF THE NW 1/4 OF SEC. 15 AND PART OF THE NE 1/4 OF SEC. 16, TWP. 35 N., R. 9 E. OF THE 3RD. P.M., IN WILL COUNTY, ILLINOIS.

LEGEND



TOTAL SHEETS 525 SHEET NO. 306

SECTION CORNER 9 10 15 15 QUARTER SECTION CORNER SECTION (113 & 114) R-5 CONTRACT NO. 62417

Proposed \odot Pavement Illinois Route 59 Curve #9

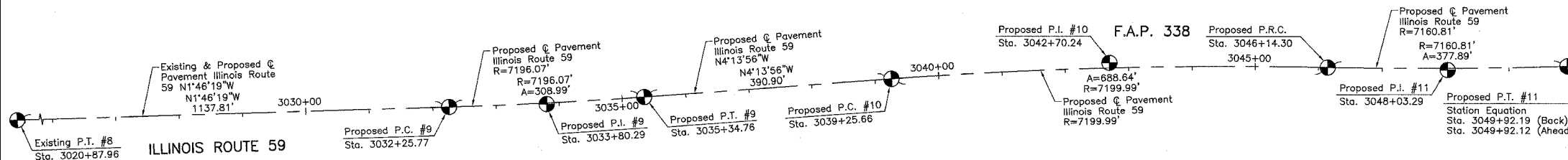
P.I. = Sta. 3033+80.29
 Δ = 227.57'
 R = 7196.07'
 T = 154.52'
 L = 308.99'
 E = 1.66'
 P.C. = Sta. 3032+25.77
 P.T. = Sta. 3035+34.76

Proposed \odot Pavement Illinois Route 59 Curve #10

P.I. = Sta. 3042+70.24
 Δ = 528.48'
 R = 7199.99'
 T = 344.58'
 L = 688.64'
 E = 8.24'
 P.C. = Sta. 3039+25.66
 P.R.C. = Sta. 3046+14.30

Proposed \odot Pavement Illinois Route 59 Curve #11

P.I. = Sta. 3048+03.29
 Δ = 301.25'
 R = 7160.81'
 T = 188.99'
 L = 377.89'
 E = 2.49'
 P.R.C. = Sta. 3046+14.30
 P.T. = Sta. 3049+92.19 (Back)
 P.T. = Sta. 3049+92.12 (Ahead)



SEE SHEET B56

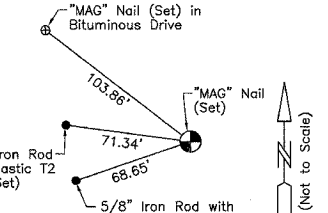
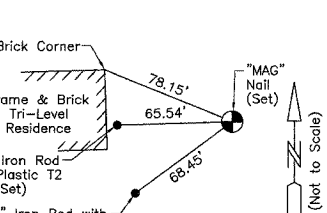
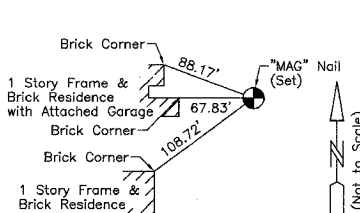
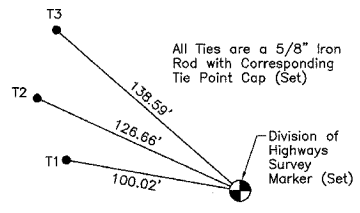
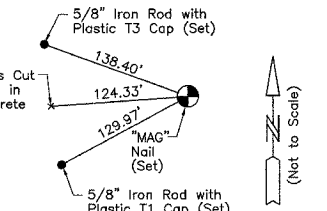
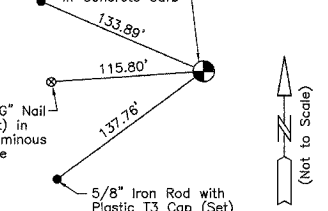
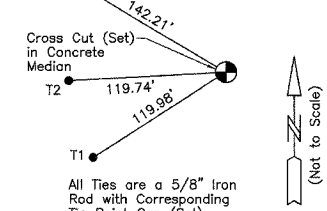
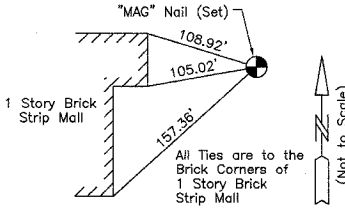
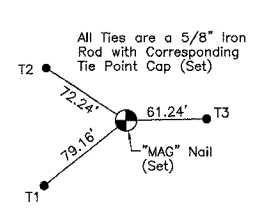
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- IRON PIPE OR ROD FOUND
 - ⊕ "MAG" NAIL SET
 - + CUT CROSS FOUND OR SET
 - 5/8" REBAR SET
 - T1
 - T2
 - T3
 - BT1
 - BT2
 - BT3
 - STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
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 - ⊙ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
 - RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }
 COUNTY OF LAKE } SS

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DATED AT LAKE VILLA, ILLINOIS THIS ____ DAY OF ____ 20__ A.D.

 PRESIDENT
 ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
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 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
 Coordinates are based on the Published Metric Coordinate Values at Found Geodetic Survey Control Monument "Will County GPS 333", P.I.D. AE2553, of N.542,285.095-E.311,129.617.
 NOTE: SURFACE COORDINATES ARE SHOWN.



DATE	
BY	
MADE	
CHECKED	
LINKED	
ROW	
PLAT	
NOTEBOOK	
NO	

JORGENSEN & ASSOCIATES, INC.
 120 PARK AVENUE
 LAKE VILLA, ILLINOIS 60046
 (847) 356-3371

SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 F.A.P. 338 (ILLINOIS ROUTE 59)

SECTION WILL COUNTY
 PROJECT JOB NO. R-91-067-01
 STATION 3020+87.96 TO STATION 3050+00
 SCALE: 1"=100' SHEET B55 OF B58

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAUMBURG, ILLINOIS 60196

REVISION DATE March 10, 2006 REVISION Station Equation at Prop. P.T. #11 MADE BY

PART OF THE EAST 1/2 OF SEC. 9, PART OF THE WEST 1/2 OF SEC. 10, PART OF THE WEST 1/2 OF SEC. 3, PART OF THE EAST 1/2 OF SEC. 4, TWP. 35 N., R. 9 E. AND PART OF THE SE 1/4 OF SEC. 33 AND PART OF THE SW 1/4 OF SEC. 34, TWP. 36 N., R. 9 E. OF THE 3RD. P.M., IN WILL COUNTY, ILLINOIS.

LEGEND

SECTION CORNER 16 15
QUARTER SECTION CORNER SECTION (113 & 114) R-5
CONTRACT NO. 62417

SECTION LINE
QUARTER SECTION LINE
QUARTER, QUARTER SECTION LINE
PLATTED LOT LINE
PROPERTY (DEED) LINE
APL
APPARENT PROPERTY LINE
CENTER LINE
EXISTING RIGHT OF WAY LINE
PROPOSED RIGHT OF WAY LINE
PROPOSED EASEMENT
MEASURED DIMENSION
COMPUTED DIMENSION
RECORD DATA

120.32'
129.32'(Comp.)

EXISTING BUILDING

SCALE: 1"=100'

Bearings are referenced to the Illinois Coordinate System NAD83 (1997) East Zone at Found Geodetic Survey Control Monuments "Will County GPS 316", P.I.D. AE2551 and "Will County GPS 333", P.I.D. AE2553.

○ IRON PIPE OR ROD FOUND ⊕ "MAG" NAIL SET
+ CUT CROSS FOUND OR SET ● 5/8" REBAR SET

● T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
T2
T3

● BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
BT2
BT3

■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

⊙ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)

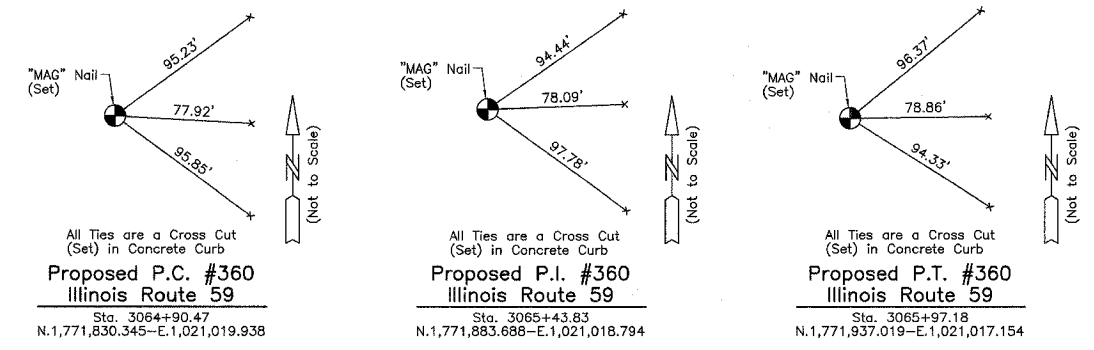
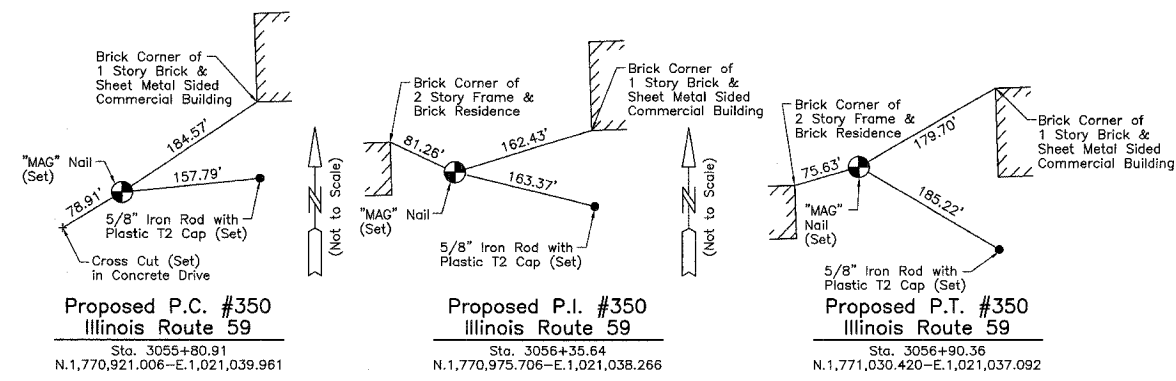
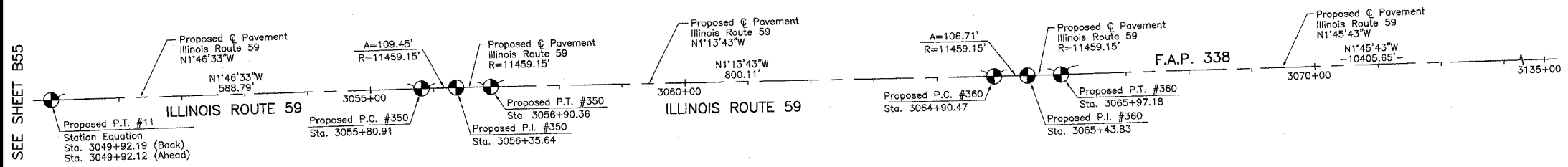
□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

Proposed C Pavement Illinois Route 59 Curve #350

P.I. = Sta. 3056+35.64
Δ = 0°32'50"
R = 11459.15'
T = 54.73'
L = 109.45'
E = 0.13'
P.C. = Sta. 3055+80.91
P.T. = Sta. 3056+90.36

Proposed C Pavement Illinois Route 59 Curve #360

P.I. = Sta. 3065+43.83
Δ = 0°32'00"
R = 11459.15'
T = 53.36'
L = 106.71'
E = 0.12'
P.C. = Sta. 3064+90.47
P.T. = Sta. 3065+97.18



SEE SHEET B57

STATE OF ILLINOIS }
COUNTY OF LAKE }SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 9, TOWNSHIP 35N., RANGE 9E. AND SECTION 34, TOWNSHIP 36N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS _____ DAY OF _____ 20__ A.D.

PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRATION DATE: NOVEMBER 30, 2006
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY. Coordinates are based on the Published Metric Coordinate Values at Found Geodetic Survey Control Monument "Will County GPS 333", P.I.D. AE2553, of N.542,285.095-E.311,129.617.
NOTE: SURFACE COORDINATES ARE SHOWN.

JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60046
(847) 356-3371

60046 SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

PLAT OF HIGHWAYS STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.P. 338 (ILLINOIS ROUTE 59)

SECTION WILL COUNTY
PROJECT JOB NO. R-91-067-01
STATION 3049+92.12 TO STATION 3135+00
SCALE: 1"=100' SHEET B58 OF B58

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

DATE	
BY	
MADE	
CHECKED	
INKEID	
ROW PLAT	
NOTEBOOK NO.	

PART OF THE SE 1/4 OF SEC. 21, PART OF THE SW 1/4 OF SEC. 22, PART OF THE WEST 1/2 OF SEC. 27, PART OF THE EAST 1/2 OF SEC. 28, PART OF THE EAST 1/2 OF SEC. 33 AND PART OF THE WEST 1/2 OF SEC. 34, TWP. 36 N., R. 9 E. OF THE 3RD. P.M., IN WILL COUNTY, ILLINOIS.

TOTAL SHEETS 525 307

LEGEND

SECTION CORNER 16 15 QUARTER SECTION CORNER SECTION (113 & 114) R-5 CONTRACT NO. 62417

SECTION LINE
 QUARTER SECTION LINE
 QUARTER, QUARTER SECTION LINE
 PLATTED LOT LINE
 PROPERTY (DEED) LINE

APL APPARENT PROPERTY LINE
 CENTER LINE
 EXISTING RIGHT OF WAY LINE
 PROPOSED RIGHT OF WAY LINE
 PROPOSED EASEMENT
 MEASURED DIMENSION
 COMPUTED DIMENSION
 RECORD DATA

120.32'
 129.32'(Comp.)

EXISTING BUILDING

0 100' 200'
 SCALE: 1"=100'

Bearings are referenced to the Illinois Coordinate System NAD83 (1997) East Zone at Found Geodetic Survey Control Monuments "Will County GPS 316", P.I.D. AE2551 and "Will County GPS 333", P.I.D. AE2553.

○ IRON PIPE OR ROD FOUND ⊕ "MAG" NAIL SET
 + CUT CROSS FOUND OR SET ● 5/8" REBAR SET

● T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
 ● T2
 ● T3

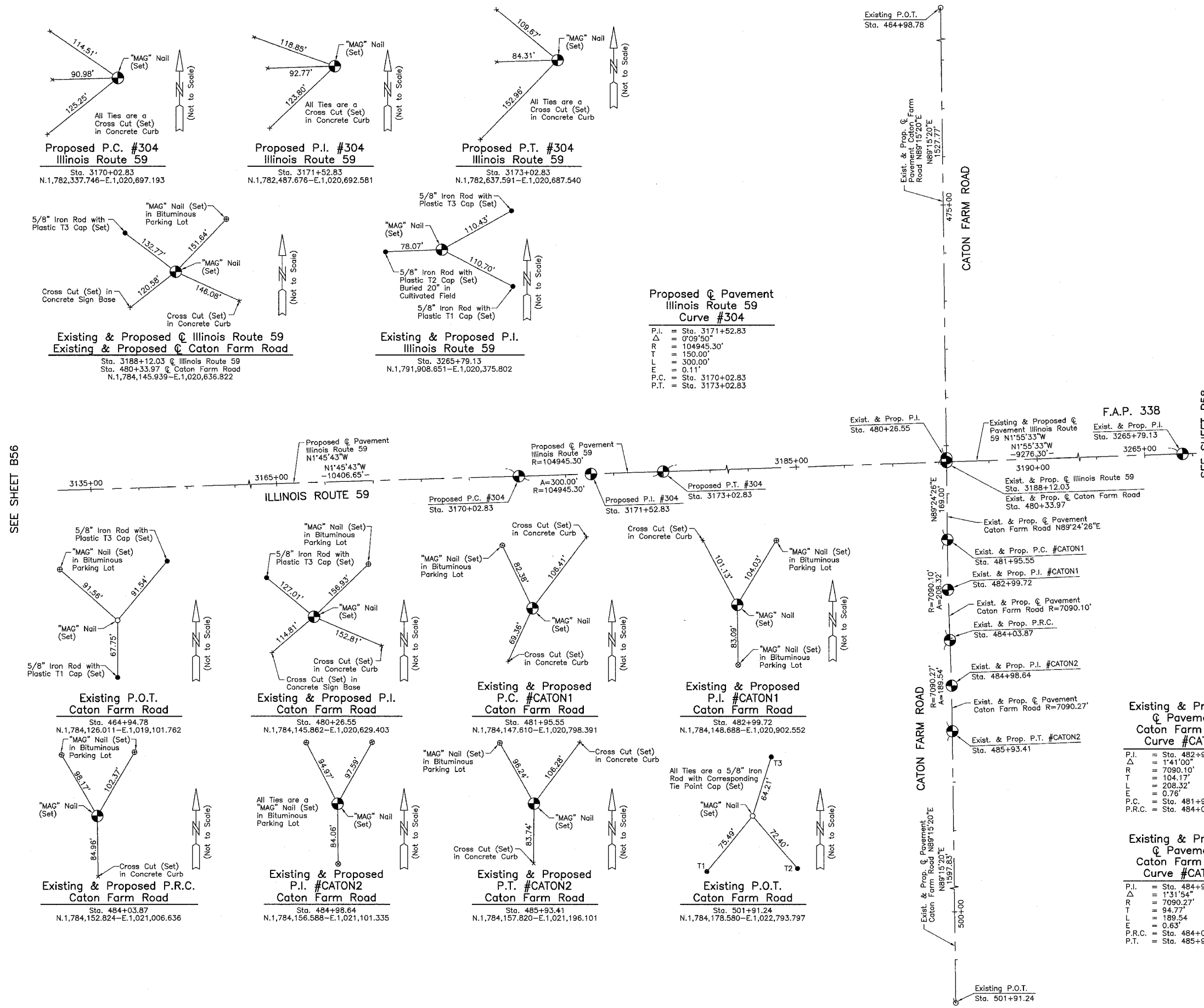
● BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
 ● BT2
 ● BT3

■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

● PERMANENT SURVEY MARKER, I.D.O.T. STD 2135 (TO BE SET BY OTHERS)

□ RIGHT OF WAY STAKING PROPOSED TO BE SET.



STATE OF ILLINOIS }
 COUNTY OF LAKE } SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 21, TOWNSHIP 36N., RANGE 9E. AND SECTION 34, TOWNSHIP 36N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS _____ DAY OF _____ 20____ A.D.

 PRESIDENT

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2008
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
 Coordinates are based on the Published Metric Coordinate Values at Found Geodetic Survey Control Monument "Will County GPS 333", P.I.D. AE2553, of N.542,285.095-E.311,129.617.
 NOTE: SURFACE COORDINATES ARE SHOWN.

DATE	
BY	
MADE	
CHECKED	
INKED	
NOTED	
NO	

REVISION DATE February 1, 2006 Proposed Q Illinois Route 59
 October 20, 2005 Proposed Q Illinois Route 59

MADE BY

PLAT OF HIGHWAYS
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 F.A.P. 338 (ILLINOIS ROUTE 59)

SECTION WILL COUNTY
 PROJECT JOB NO. R-91-067-01
 STATION 464+98.78 TO STATION 501+91.24
 STATION 3135+00 TO STATION 3265+79.13
 SCALE: 1"=100' SHEET B57 OF B58

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAUMBURG, ILLINOIS 60196

PART OF THE SE 1/4 OF SEC. 9, PART OF THE SW 1/4 OF SEC. 10, PART OF THE WEST 1/2 OF SEC. 15, PART OF THE EAST 1/2 OF SEC. 16, PART OF THE EAST 1/2 OF SEC. 21 AND PART OF THE WEST 1/2 OF SEC. 22, TWP. 36 N., R. 9 E. OF THE 3RD. P.M., IN WILL COUNTY, ILLINOIS.

TOTAL SHEETS 525
SHEET NO. 308
CONTRACT NO. 62417

LEGEND

SECTION CORNER: 9 10 16 15

QUARTER SECTION CORNER: 16 15

SECTION (113 & 114) R-5

SECTION LINE
QUARTER SECTION LINE
PLATTED LOT LINE
PROPERTY (DEED) LINE

APL
APPARENT PROPERTY LINE
CENTER LINE
EXISTING RIGHT OF WAY LINE
PROPOSED RIGHT OF WAY LINE
PROPOSED EASEMENT
MEASURED DIMENSION
COMPUTED DIMENSION
RECORD DATA

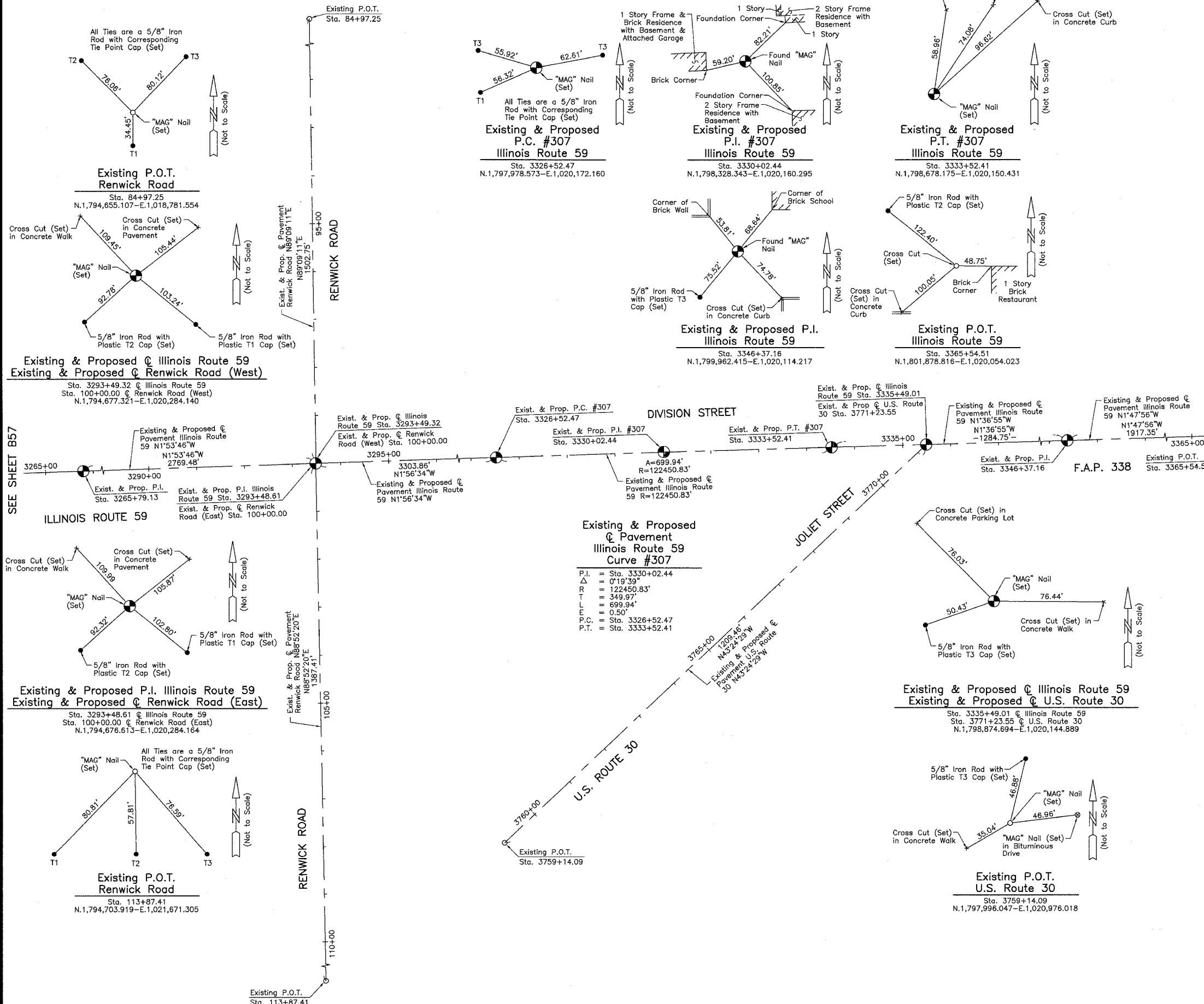
120.32'
129.32'(Comp.)

EXISTING BUILDING

Scale: 1"=100'

Bearings are referenced to the Illinois Coordinate System NAD83 (1997) East Zone at Found Geodetic Survey Control Monuments "Will County GPS 316", P.I.D. AE2551 and "Will County GPS 333", P.I.D. AE2553.

- IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- T2
- T3
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT2
- BT3
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊙ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.



STATE OF ILLINOIS }
COUNTY OF LAKE }
THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HERON BETWEEN SECTION 9, TOWNSHIP 36N., RANGE 9E. AND SECTION 22, TOWNSHIP 36N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.
DATED AT LAKE VILLA, ILLINOIS THIS ____ DAY OF ____ 20__ A.D.

PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRATION DATE: NOVEMBER 30, 2006
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
Coordinates are based on the Published Metric Coordinate Values at Found Geodetic Survey Control Monument "Will County GPS 333", P.I.D. AE2553, of N.542,285.095-E.311,129.617.
NOTE: SURFACE COORDINATES ARE SHOWN.

DATE	BY	MADE	CHECKED	INKED	NOTEBOOK NO.

JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60046
(847) 356-3371

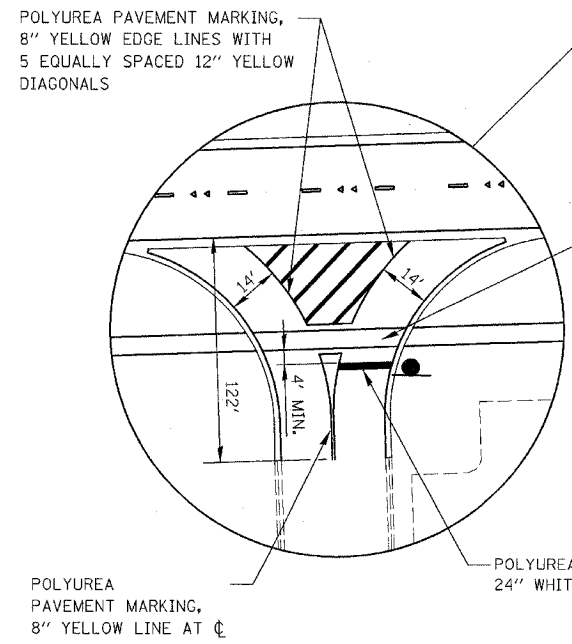
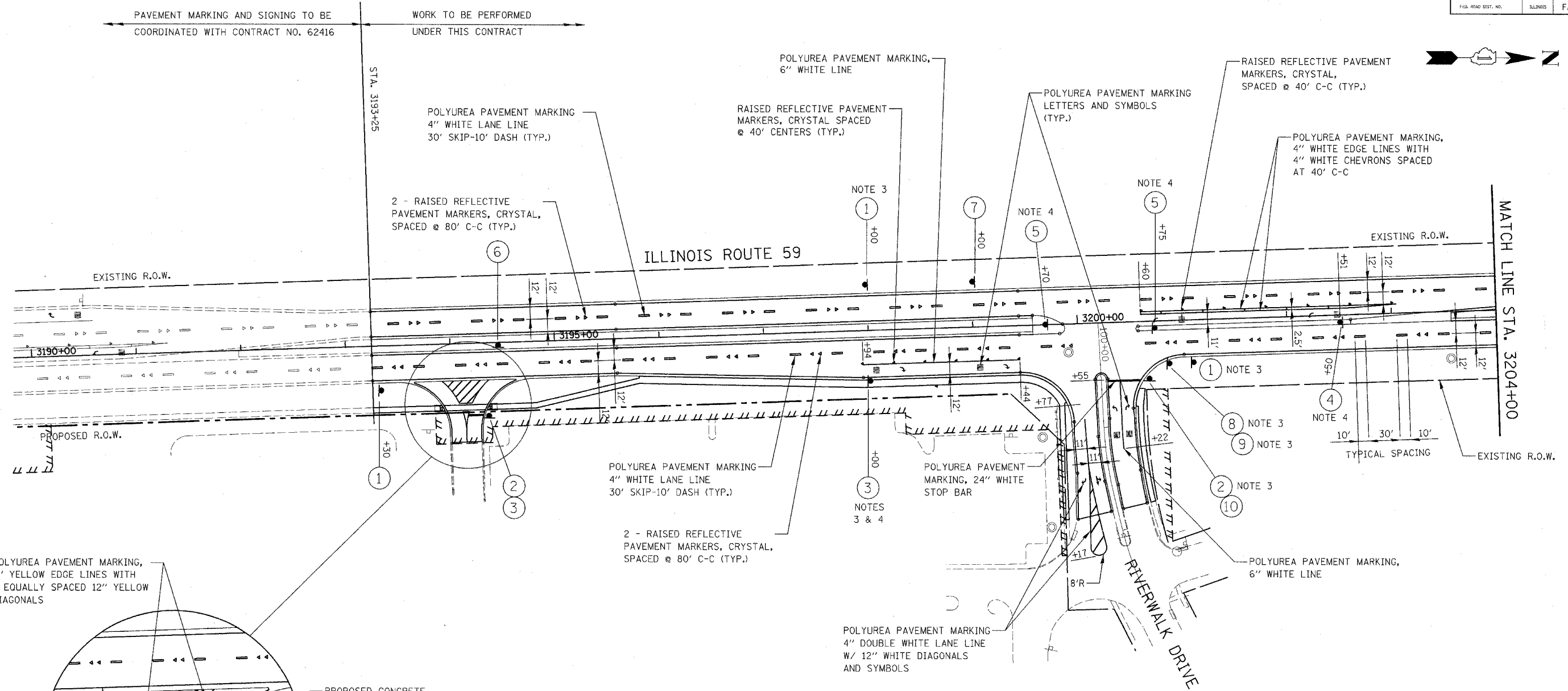
SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
F.A.P. 338 (ILLINOIS ROUTE 59)

SECTION WILL COUNTY
PROJECT JOB NO. R-91-067-01
STATION 84+97.25 TO STATION 113+87.41
STATION 3265+00 TO STATION 3365+54.51
STATION 3759+14.09 TO STATION 3771+23.55
SCALE: 1"=100' SHEET 858 OF 858

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

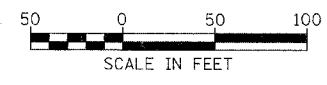
FAP RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	309
STA. 3190+00		TO STA. 3204+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



- | | | | | | | | | |
|---|--|------------|---|--|------------|---|--|----------------|
| ① | | R2-1-3036 | ④ | | R3-5L-3036 | ⑦ | | M3-3-2412 |
| ② | | R1-1-30 | ⑤ | | R4-7-2430 | ⑧ | | D3
30" X 8" |
| ③ | | R3-5R-3036 | ⑥ | | R6-2R-1824 | ⑨ | | D3
30" X 8" |
| | | | | | | ⑩ | | R6-3A-2418 |

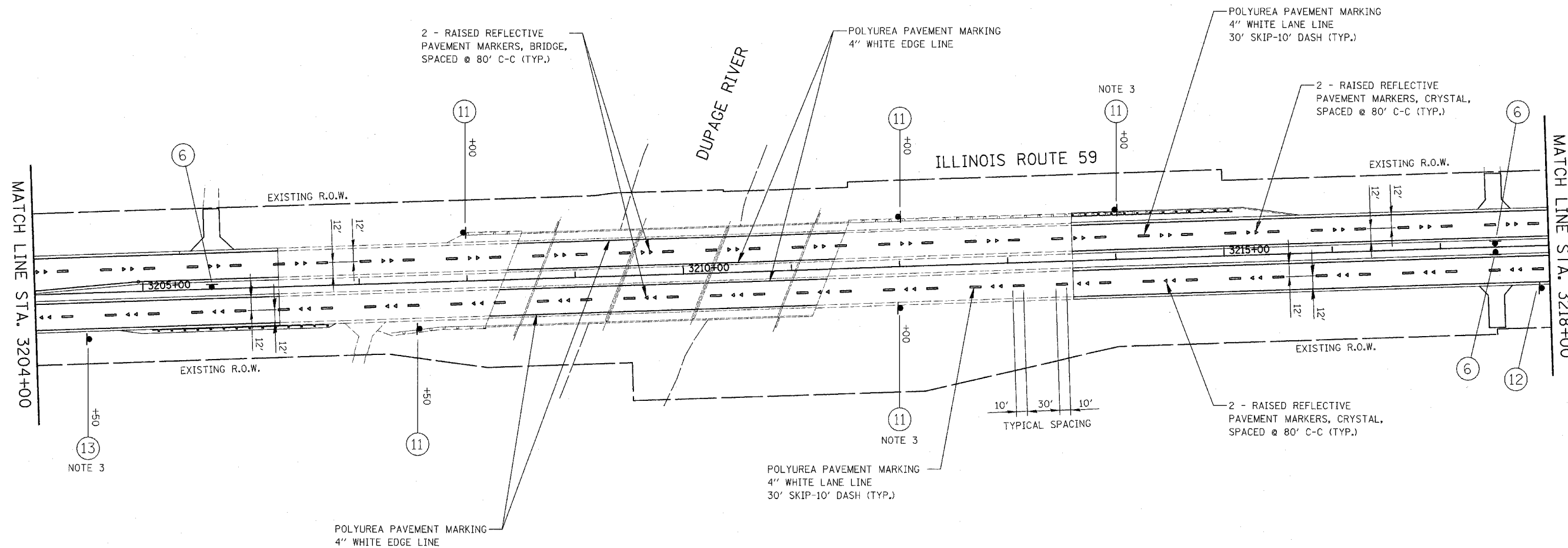
- NOTES:**
- PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH IDOT DISTRICT 1 DETAILS TC-11 AND TC-13.
 - SIGN LOCATIONS MAY VARY BASED ON FIELD CONDITIONS.
 - EXISTING SIGN TO BE REMOVED AND RE-ERECTED.
 - SIGN SHALL BE MOUNTED ON A TELESCOPING STEEL SIGN SUPPORT IN ACCORDANCE WITH HIGHWAY STANDARDS 720001 AND 728001.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG
**PAVEMENT MARKING
AND SIGNING PLANS**



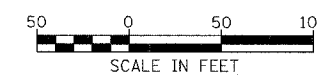
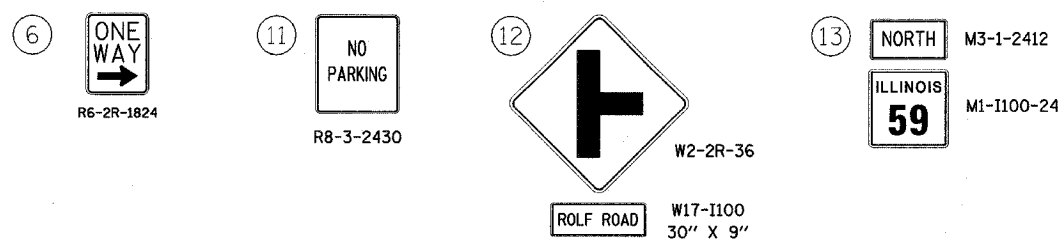
CONTRACT NO. 62417

FED. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338 (113 & 114) R-5	WILL		525	310
STA. 3204+00		TO STA. 3218+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



NOTES:

1. PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH IDOT DISTRICT 1 DETAILS TC-11 AND TC-13.
2. SIGN LOCATIONS MAY VARY BASED ON FIELD CONDITIONS.
3. EXISTING SIGN TO BE REMOVED AND RE-ERECTED.
4. SIGN SHALL BE MOUNTED ON A TELESCOPING STEEL SIGN SUPPORT IN ACCORDANCE WITH HIGHWAY STANDARDS 720001 AND 728001.



Rev. 05-01-08 JCM

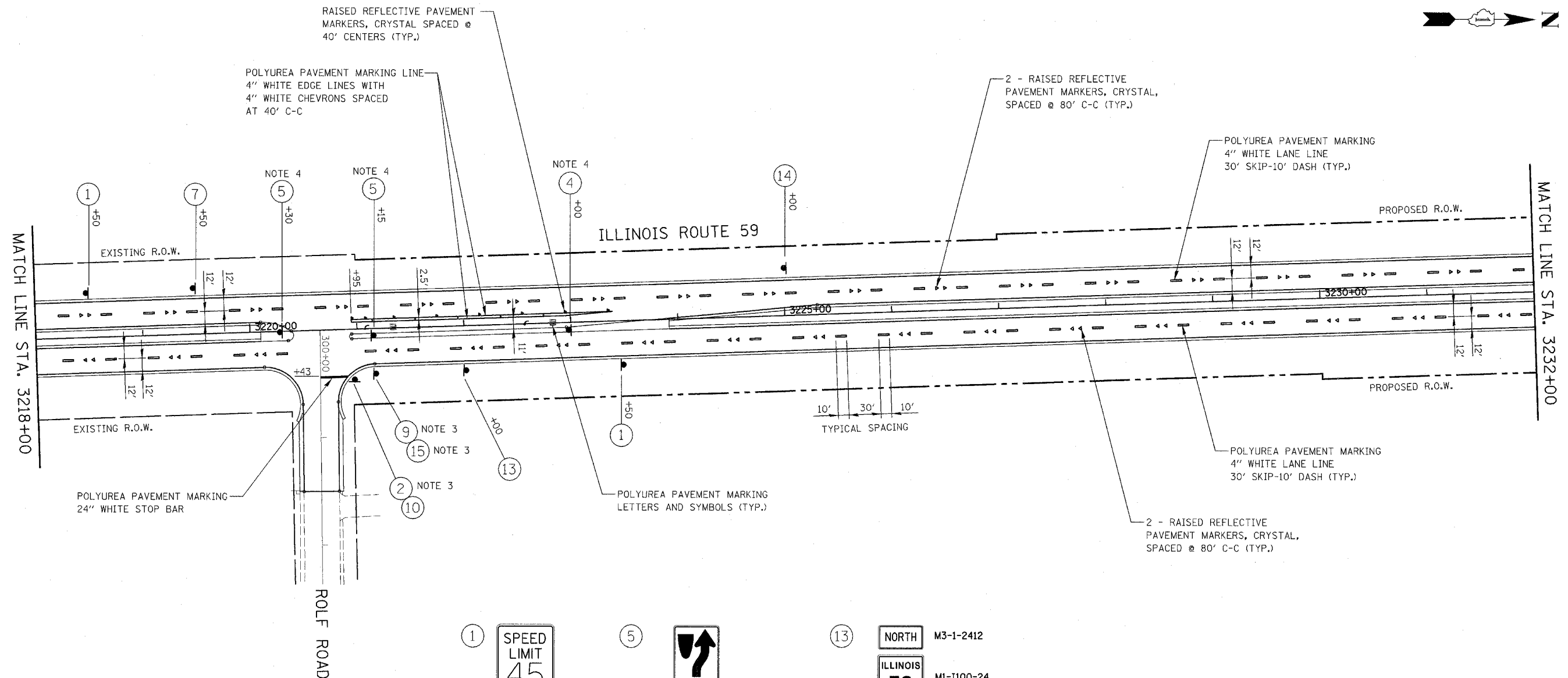
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG

**PAVEMENT MARKING
AND SIGNING PLANS**

SCALE AS SHOWN
DATE MARCH 18, 2008

DRAWN BY REW
CHECKED BY JCM/HME

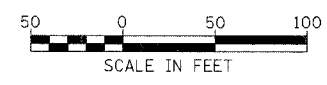
FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	311
STA. 3218+00		TO STA. 3232+00		
FED. ROAD DIST. NO.		ILLINOIS FAP 338 (IL RTE. 59)		



①	 R2-1-3036	⑤	 R4-7-2430	⑬	 M3-1-2412 M1-I100-24
②	 R1-1-3030	⑦	 M3-3-2412 M1-I100-24	⑭	 W2-2L-36
④	 R3-5L-3036	⑨	 D3 24" X 6"	⑮	 W17-I100 30" X 9"
		⑩	 R6-3A-2418		

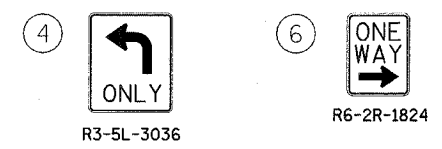
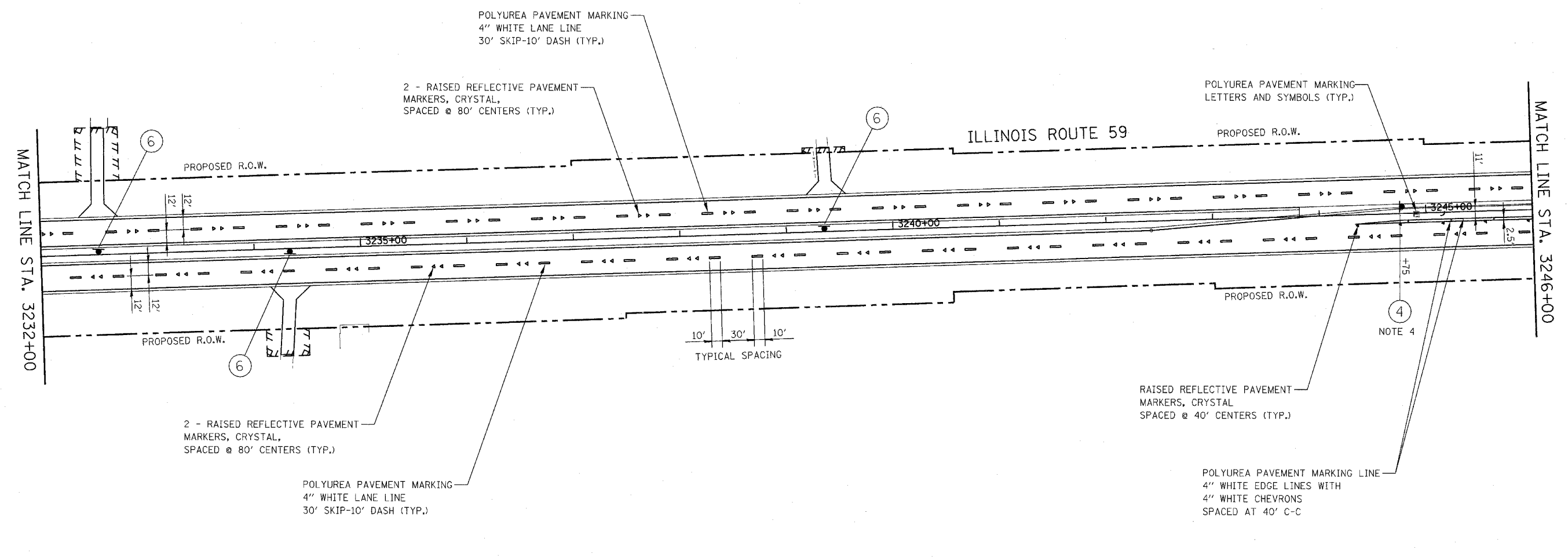
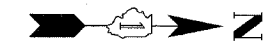
- NOTES:**
- PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH IDOT DISTRICT 1 DETAILS TC-11 AND TC-13.
 - SIGN LOCATIONS MAY VARY BASED ON FIELD CONDITIONS.
 - EXISTING SIGN TO BE REMOVED AND RE-ERECTED.
 - SIGN SHALL BE MOUNTED ON A TELESCOPING STEEL SIGN SUPPORT IN ACCORDANCE WITH HIGHWAY STANDARDS 720001 AND 728001.

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 PAVEMENT MARKING
 AND SIGNING PLANS



CONTRACT NO. 62417

FAP SITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	312
STA. 3232+00		TO STA. 3246+00		
FED. ROAD DIST. NO.		ILLINOIS FAP 338 (IL RTE. 59)		



NOTES:

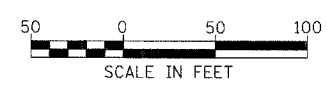
1. PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH IDOT DISTRICT 1 DETAILS TC-11 AND TC-13.
2. SIGN LOCATIONS MAY VARY BASED ON FIELD CONDITIONS.
3. EXISTING SIGN TO BE REMOVED AND RE-ERECTED.
4. SIGN SHALL BE MOUNTED ON A TELESCOPING STEEL SIGN SUPPORT IN ACCORDANCE WITH HIGHWAY STANDARDS 720001 AND 728001.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG

**PAVEMENT MARKING
AND SIGNING PLANS**

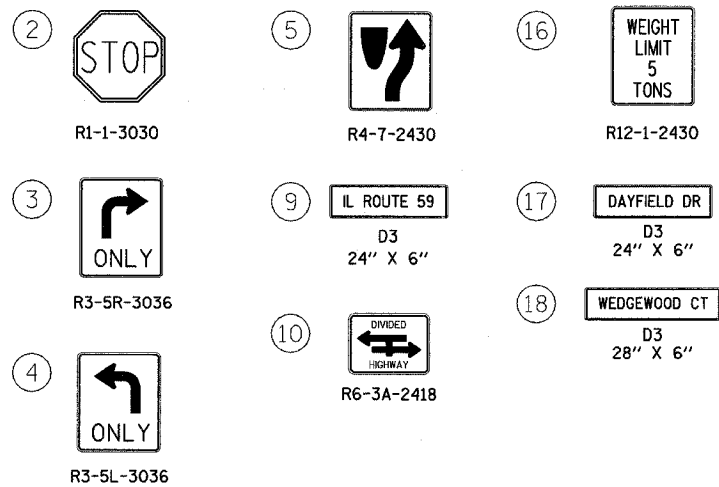
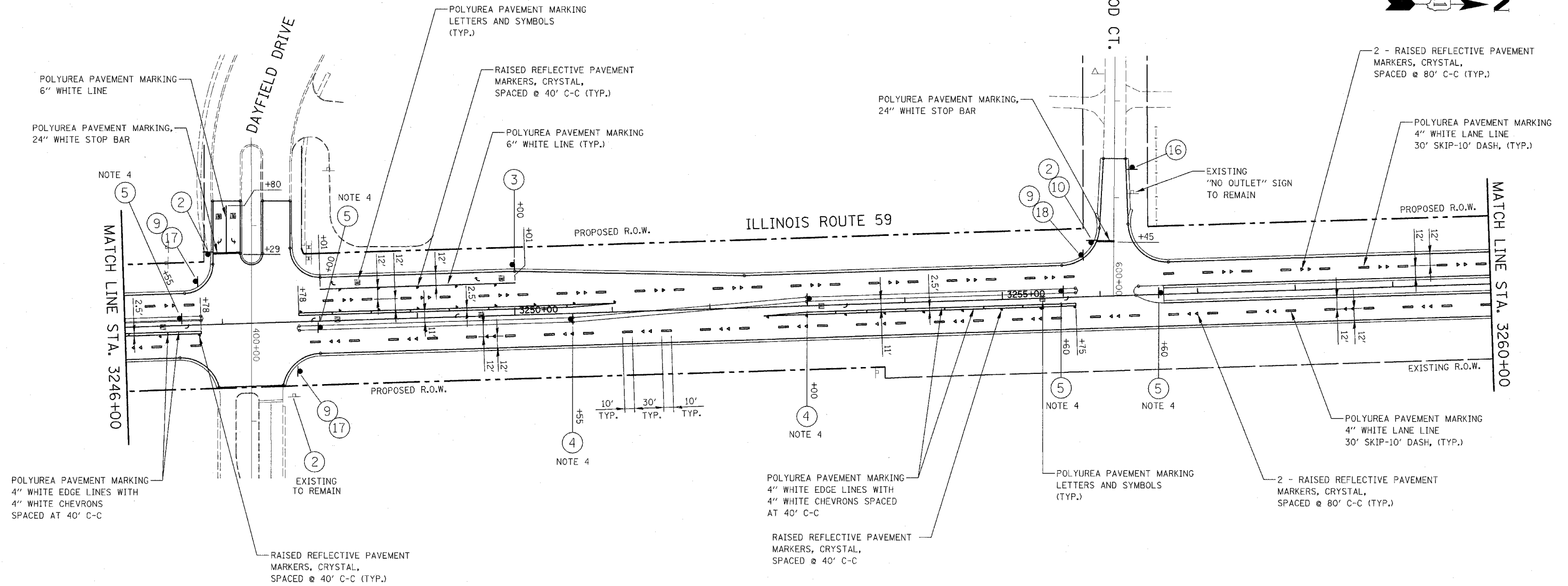
SCALE AS SHOWN
DATE MARCH 18, 2008

DRAWN BY REW
CHECKED BY JCM/HME



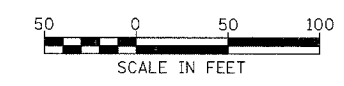
Rev. 05-01-08 JCM

FAP RFL.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	313
STA. 3246+00		TO STA. 3260+00		
FED. ROAD DIST. NO.		ILLINOIS FAP 338 (IL RTE. 59)		

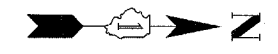


- NOTES:**
- PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH IDOT DISTRICT 1 DETAILS TC-11 AND TC-13.
 - SIGN LOCATIONS MAY VARY BASED ON FIELD CONDITIONS.
 - EXISTING SIGN TO BE REMOVED AND RE-ERECTED.
 - SIGN SHALL BE MOUNTED ON A TELESCOPING STEEL SIGN SUPPORT IN ACCORDANCE WITH HIGHWAY STANDARDS 720001 AND 728001.

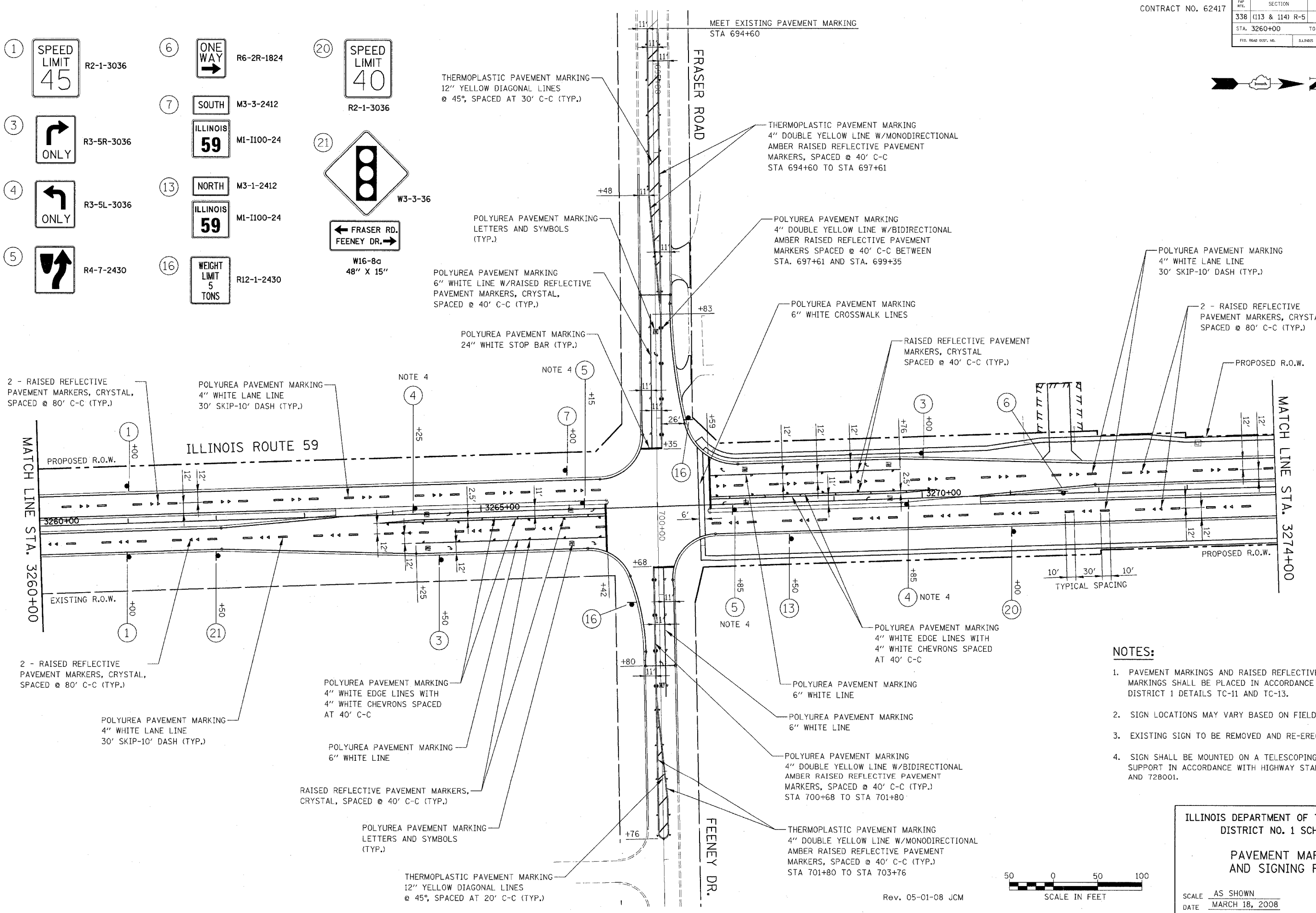
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG
**PAVEMENT MARKING
AND SIGNING PLANS**



FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338 (113 & 114) R-5	WILL	526	314	
STA. 3260+00		TO STA. 3274+00		
FID. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		

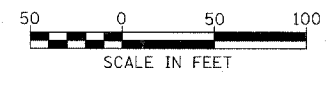


- ① SPEED LIMIT 45 R2-1-3036
- ② ONE WAY R6-2R-1824
- ③ SOUTH M3-3-2412
- ④ ILLINOIS 59 MI-I100-24
- ⑤ NORTH M3-1-2412
- ⑥ ILLINOIS 59 MI-I100-24
- ⑦ WEIGHT LIMIT 5 TONS R12-1-2430
- ⑧ SPEED LIMIT 40 R2-1-3036
- ⑨ W16-8a 48" X 15" W3-3-36
- ⑩ POLYUREA PAVEMENT MARKING LETTERS AND SYMBOLS (TYP.)
- ⑪ POLYUREA PAVEMENT MARKING 6" WHITE LINE W/RAISED REFLECTIVE PAVEMENT MARKERS, CRYSTAL, SPACED @ 40' C-C (TYP.)
- ⑫ POLYUREA PAVEMENT MARKING 24" WHITE STOP BAR (TYP.)
- ⑬ POLYUREA PAVEMENT MARKING 4" WHITE LANE LINE 30' SKIP-10' DASH (TYP.)
- ⑭ POLYUREA PAVEMENT MARKING 4" WHITE EDGE LINES WITH 4" WHITE CHEVRONS SPACED AT 40' C-C
- ⑮ POLYUREA PAVEMENT MARKING 6" WHITE LINE
- ⑯ RAISED REFLECTIVE PAVEMENT MARKERS, CRYSTAL, SPACED @ 40' C-C (TYP.)
- ⑰ POLYUREA PAVEMENT MARKING LETTERS AND SYMBOLS (TYP.)
- ⑱ THERMOPLASTIC PAVEMENT MARKING 12" YELLOW DIAGONAL LINES @ 45°, SPACED AT 20' C-C (TYP.)
- ⑳ THERMOPLASTIC PAVEMENT MARKING 12" YELLOW DIAGONAL LINES @ 45°, SPACED AT 30' C-C (TYP.)
- ㉑ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE W/MONODIRECTIONAL AMBER RAISED REFLECTIVE PAVEMENT MARKERS, SPACED @ 40' C-C (TYP.)
- ㉒ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE W/BIDIRECTIONAL AMBER RAISED REFLECTIVE PAVEMENT MARKERS, SPACED @ 40' C-C BETWEEN STA. 697+61 AND STA. 699+35
- ㉓ POLYUREA PAVEMENT MARKING 4" WHITE LANE LINE 30' SKIP-10' DASH (TYP.)
- ㉔ 2 - RAISED REFLECTIVE PAVEMENT MARKERS, CRYSTAL, SPACED @ 80' C-C (TYP.)



- NOTES:**
- PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH IDOT DISTRICT 1 DETAILS TC-11 AND TC-13.
 - SIGN LOCATIONS MAY VARY BASED ON FIELD CONDITIONS.
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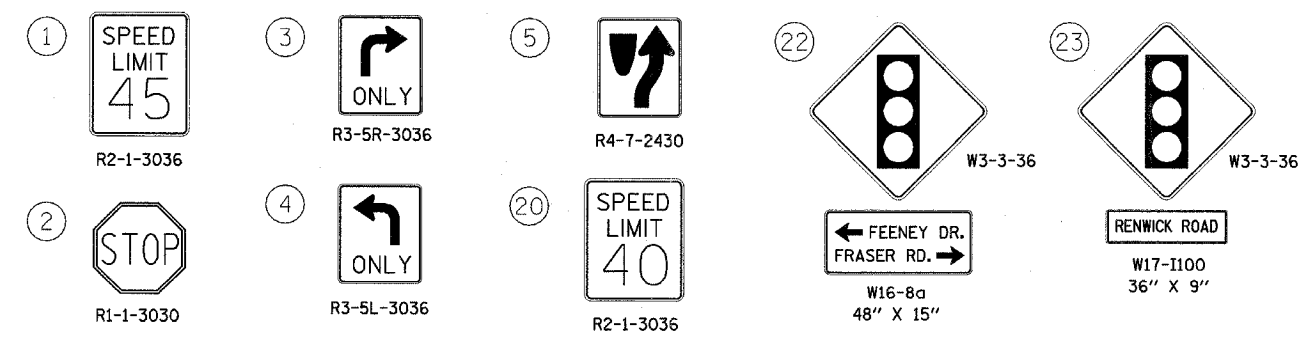
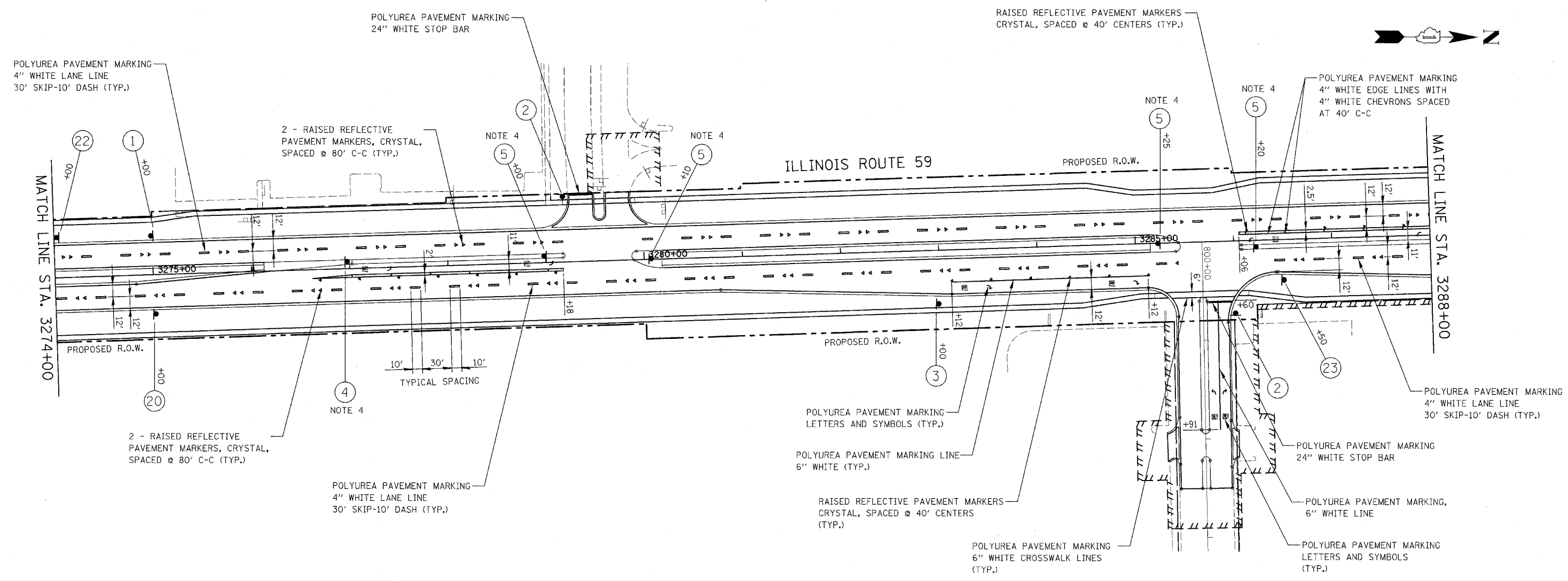
ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
**PAVEMENT MARKING
 AND SIGNING PLANS**



Rev. 05-01-08 JCM

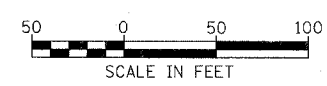
SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY REW
 CHECKED BY JCM/HME

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	315
STA. 3274+00		TO STA. 3288+00		
FED. ROAD DIST. NO.		ILLINOIS FAP 338 (IL RTE. 59)		



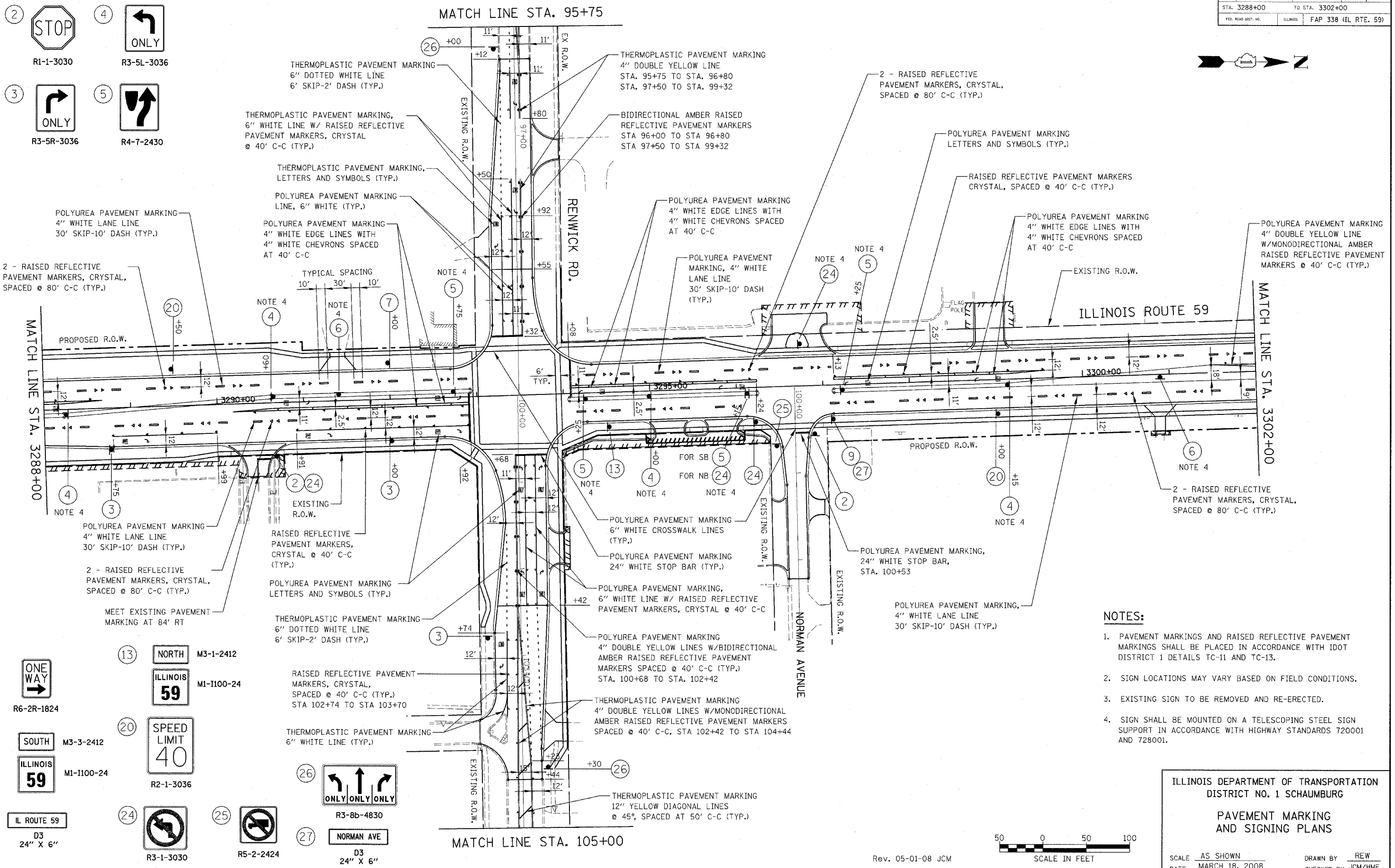
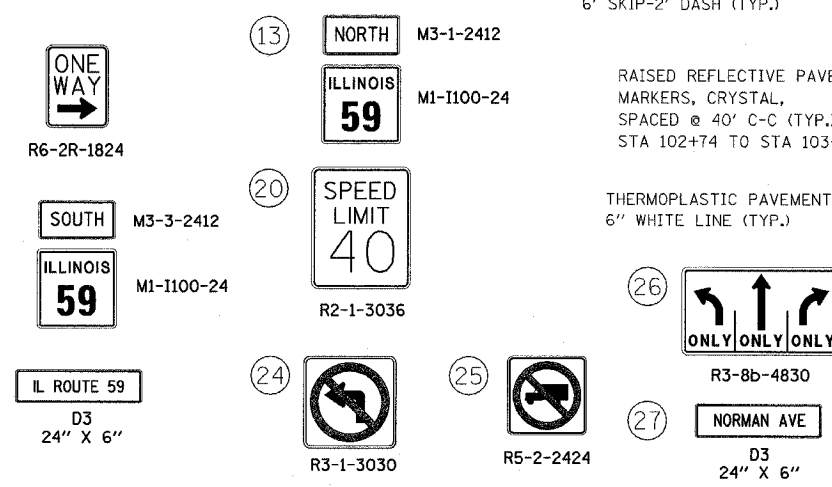
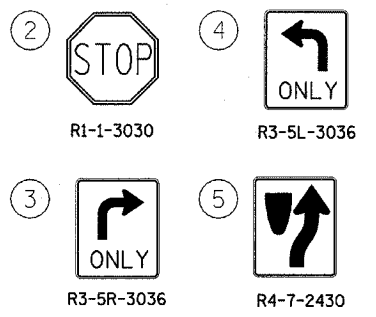
- NOTES:**
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ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG
**PAVEMENT MARKING
AND SIGNING PLANS**



Rev. 05-01-08 JCM

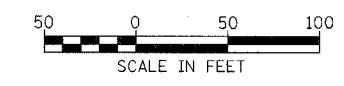
SCALE AS SHOWN
DATE MARCH 18, 2008
DRAWN BY REW
CHECKED BY JCM/HME



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ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG

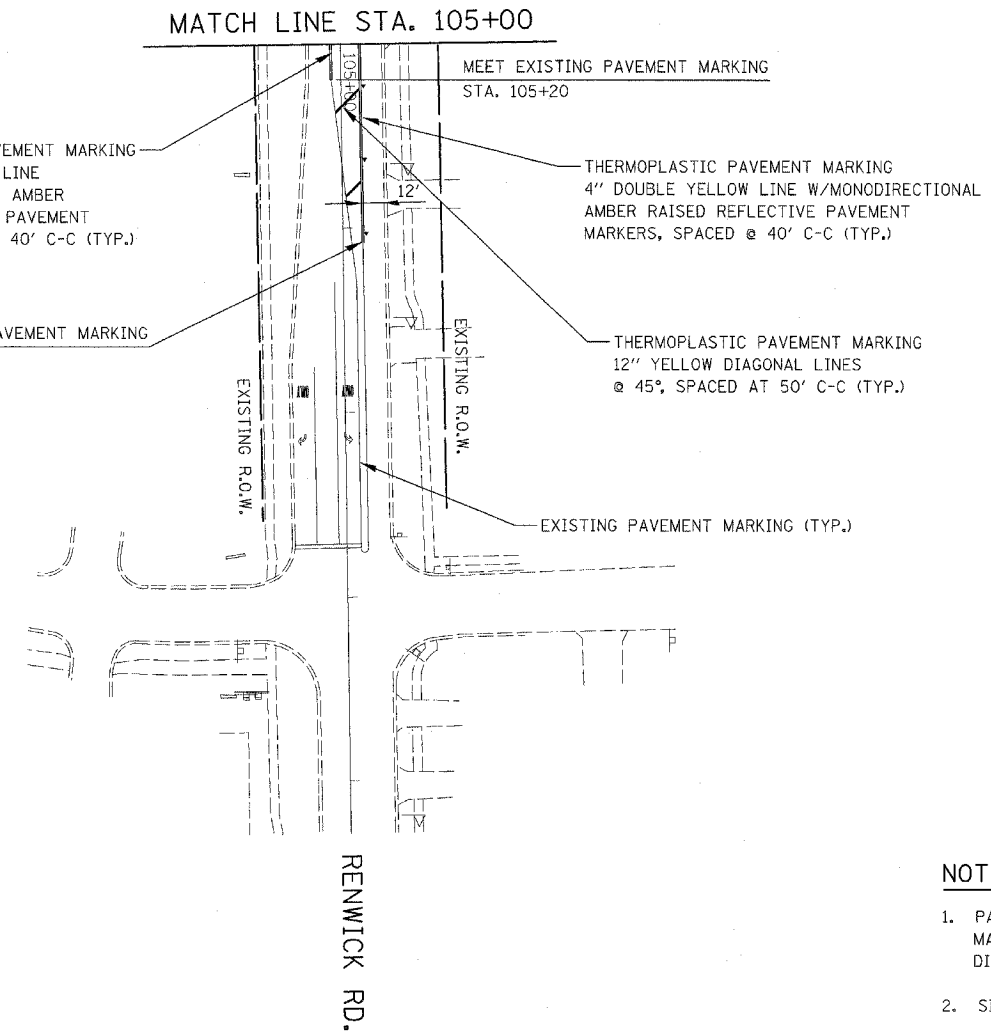
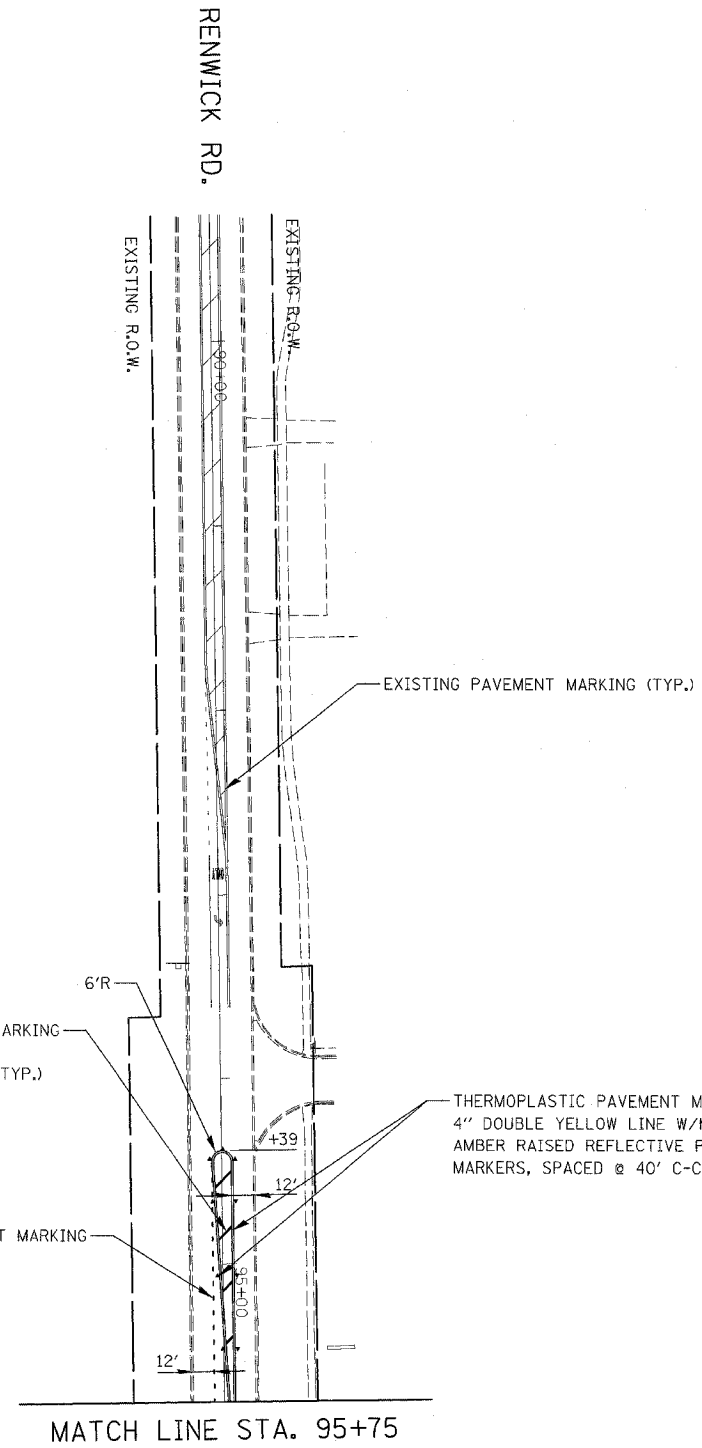
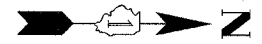
PAVEMENT MARKING
AND SIGNING PLANS



Rev. 05-01-08 JCM

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FAP SITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	317
STA. 3288+00		TO STA. 3302+00		
FED. ROAD DIST. NO.		ILLINOIS FAP 338 (IL RTE. 59)		



NOTES:

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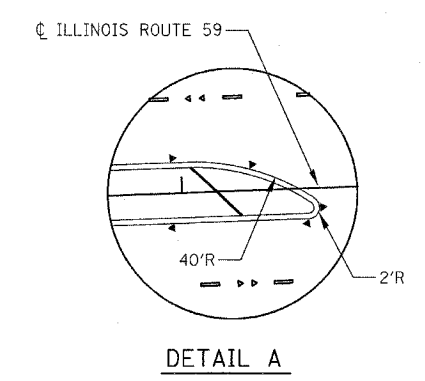
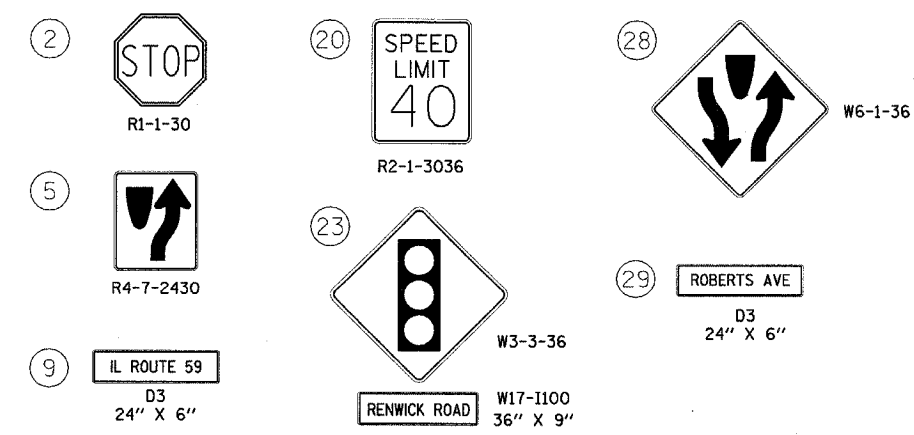
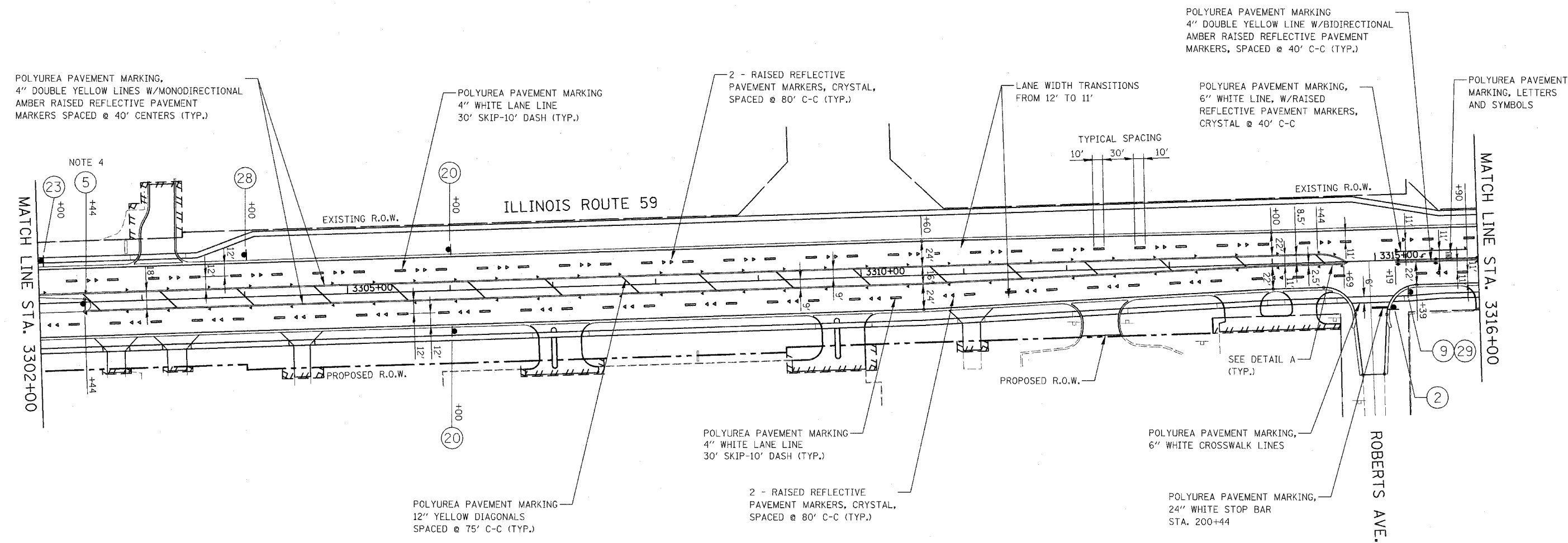
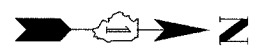


ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG

**PAVEMENT MARKING
AND SIGNING PLANS**

SCALE AS SHOWN
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CHECKED BY JCM/HME

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	318
STA. 3302+00		TO STA. 3316+00		
FED. ROAD DIST. NO.		ILLINOIS	FAP 338 (IL RTE. 59)	



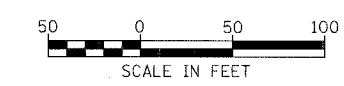
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ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG

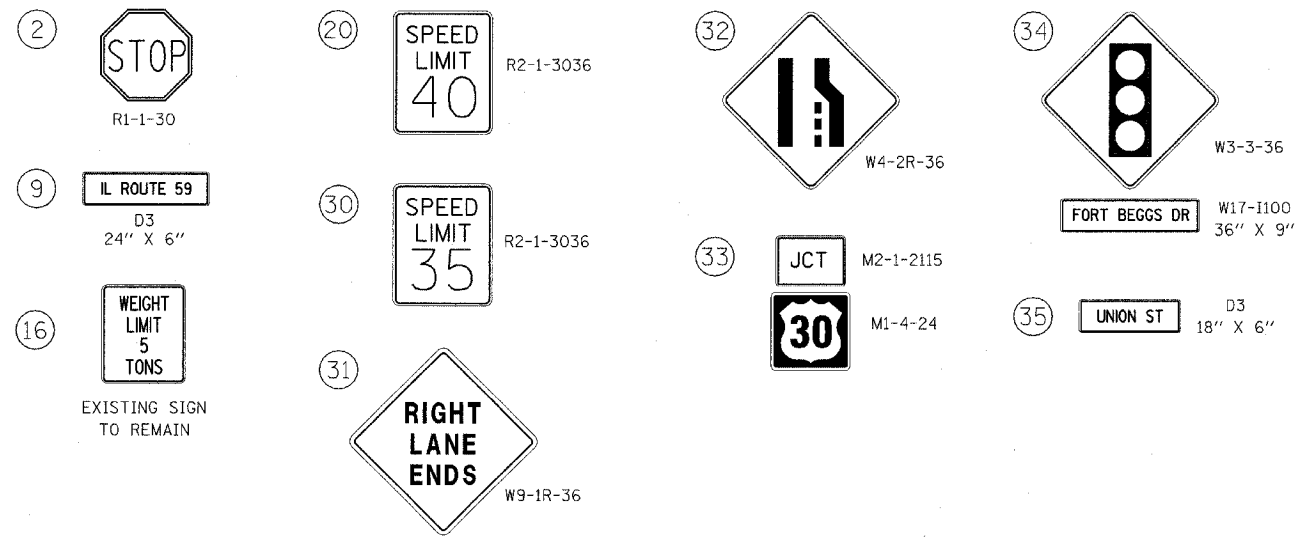
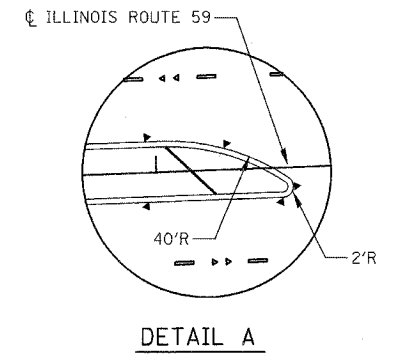
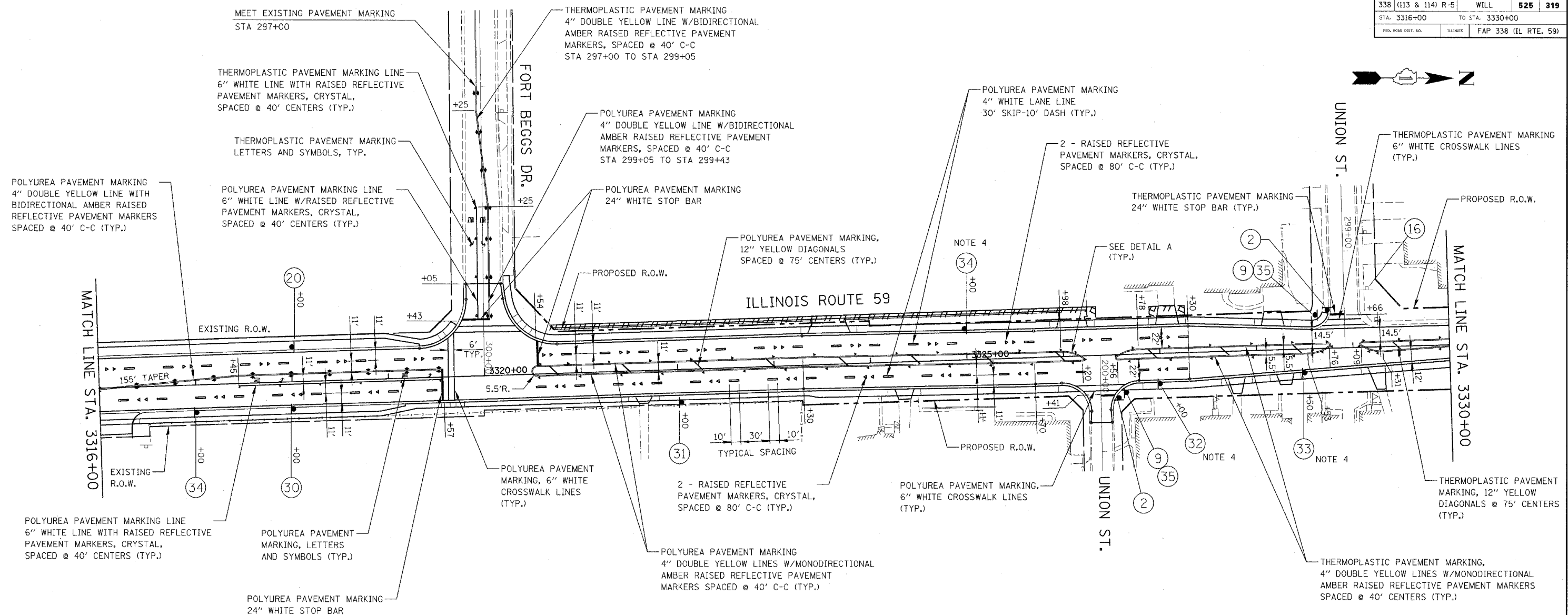
**PAVEMENT MARKING
AND SIGNING PLANS**

SCALE AS SHOWN
DATE MARCH 18, 2008

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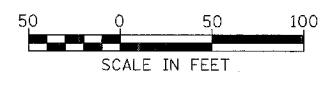


FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	319
STA. 3316+00		TO STA. 3330+00		
FID. ROAD DIST. NO.		ILLINOIS	FAP 338 (IL RTE. 59)	



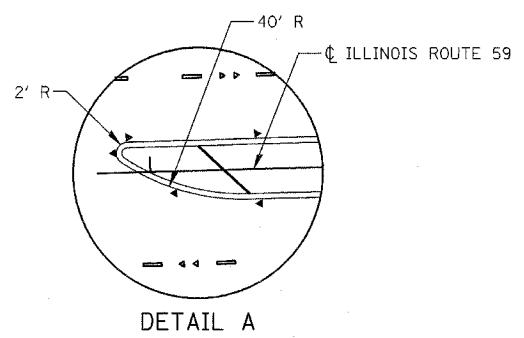
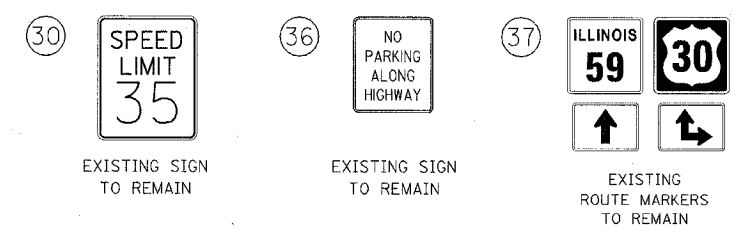
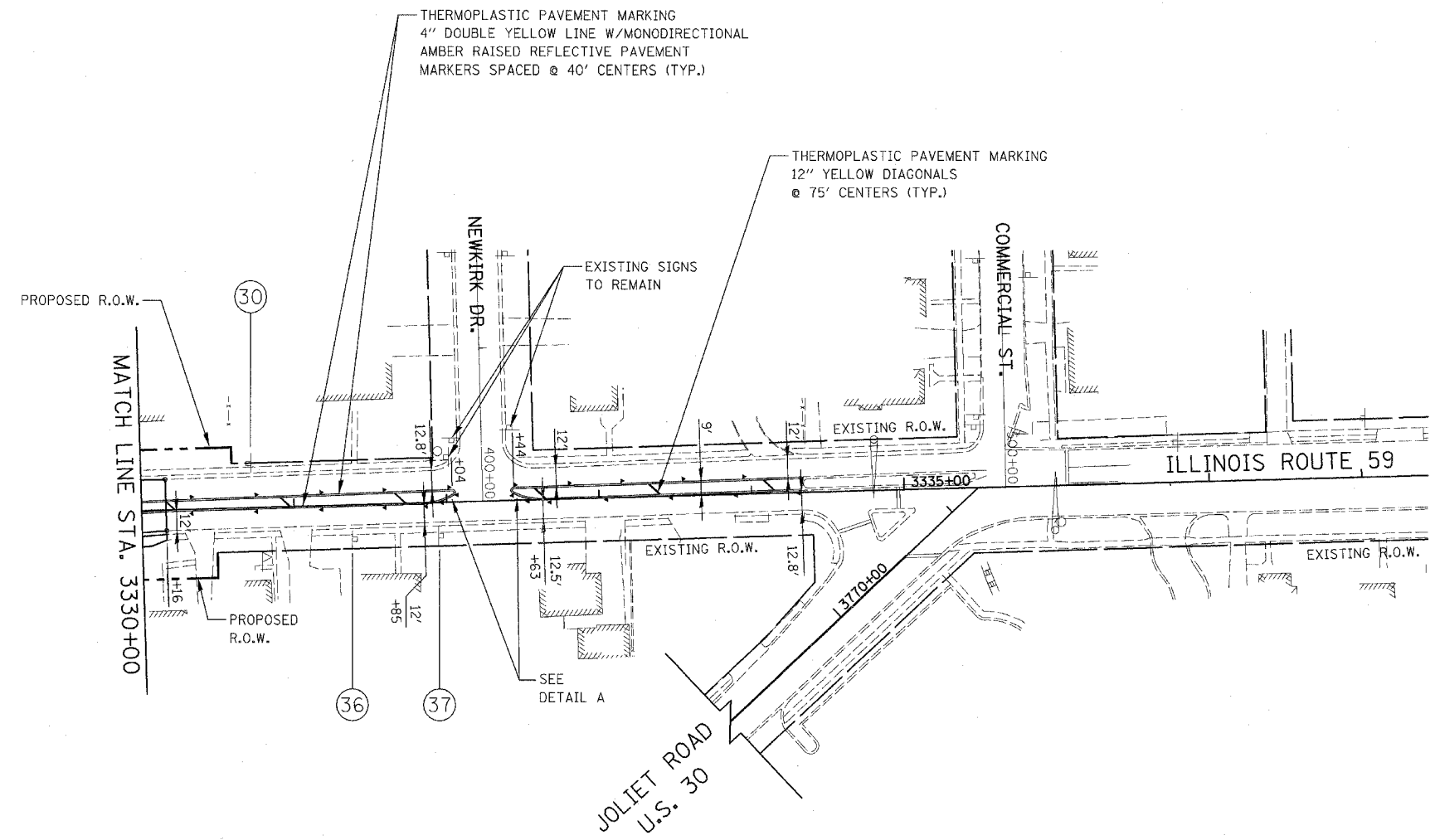
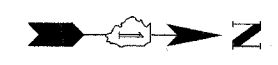
- NOTES:**
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ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG
**PAVEMENT MARKING
AND SIGNING PLANS**



SCALE AS SHOWN
DATE MARCH 18, 2008
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FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338 (113 & 114) R-5	WILL		525	320
STA. 3330+00		TO STA. 3335+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



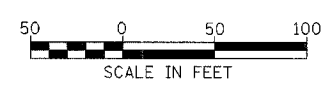
NOTES:

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ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG






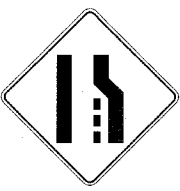







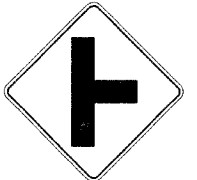






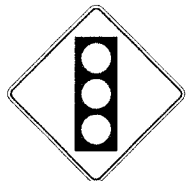
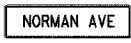




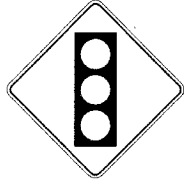

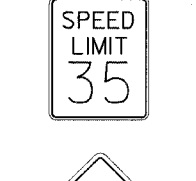
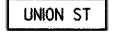


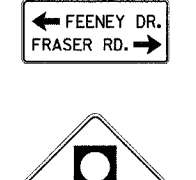





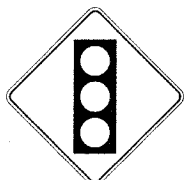





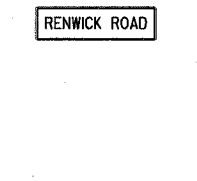
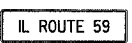


**PAVEMENT MARKING
AND SIGNING PLANS**

SCALE AS SHOWN DRAWN BY REW
DATE MARCH 18, 2008 CHECKED BY JCM/HME



FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338 (113 & 114) R-5	WILL		525	320A
STA. 3190+00		TO STA. 3204+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		

SIGN LEGEND

- | | | | | | | | | | | | | | | | | | |
|---|---|-------------------------|---|---|-------------------------|---|---|----------------------|---|---|----------------|---|---|------------|---|---|----------------------------------|
| ① |  | R2-1-3036 | ⑩ |  | R6-3A-2418 | ⑮ |  | D3
24" X 6" | ⑳ |  | R2-1-3036 | ㉔ |  | R3-1-3030 | ㉓ |  | W4-2R-36 |
| ② |  | R1-1-30 | ⑪ |  | R8-3-2430 | ⑯ |  | D3
28" X 6" | ㉑ |  | | ㉕ |  | R5-2-2424 | ㉔ |  | M2-1-2115 |
| ③ |  | R3-5R-3036 | ⑫ |  | W2-2R-36 | ㉒ |  | | ㉒ |  | R2-1-3036 | ㉖ |  | R3-8b-4830 | ㉔ |  | M1-4-24 |
| ④ |  | R3-5L-3036 | ⑬ |  | W17-1100
30" X 9" | ㉓ |  | W3-3-36 | ㉓ |  | D3
24" X 6" | ㉗ |  | W6-1-36 | ㉔ |  | W3-3-36 |
| ⑤ |  | R4-7-2430 | ⑭ |  | M3-1-2412
M1-1100-24 | ㉔ |  | W3-3-36 | ㉔ |  | D3
24" X 6" | ㉘ |  | W6-1-36 | ㉔ |  | D3
18" X 6" |
| ⑥ |  | R6-2R-1824 | ⑮ |  | W2-2L-36 | ㉕ |  | W3-3-36 | ㉕ |  | R2-1-3036 | ㉙ |  | | ㉔ |  | EXISTING SIGN TO REMAIN |
| ⑦ |  | M3-3-2412
M1-1100-24 | ⑯ |  | W17-1100
30" X 9" | ㉖ |  | W3-3-36 | ㉖ |  | W9-1R-36 | ㉚ |  | | ㉔ |  | EXISTING ROUTE MARKERS TO REMAIN |
| ⑧ |  | D3
30" X 8" | ⑰ |  | D3
18" X 6" | ㉗ |  | W3-3-36 | ㉗ | | | | | | | | |
| ⑨ |  | D3
30" X 8" | ⑱ |  | R12-1-2430 | ㉘ |  | W17-1100
36" X 9" | | | | | | | | | |

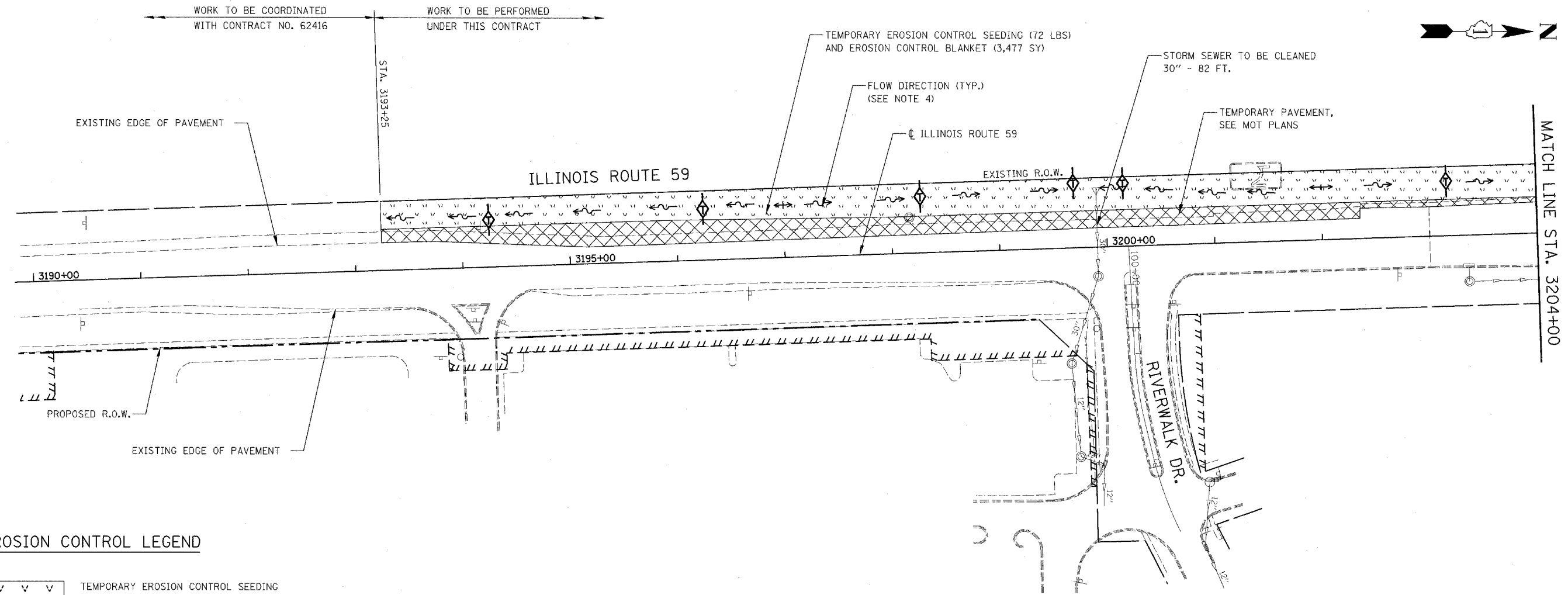
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG

PAVEMENT MARKING
AND SIGNING PLANS

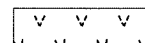

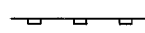







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FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	321
STA. 3190+00		TO STA. 3204+00		
FED. ROAD DIST. NO.	BLANDIS	FAP 338 (IL RTE. 59)		



EROSION CONTROL LEGEND

-  TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET
-  TEMPORARY PAVEMENT
-  SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)
-  DRAINAGE STRUCTURE TO BE CLEANED
-  INLET FILTER
-  TEMPORARY DITCH CHECK
-  FLOW DIRECTION (SEE NOTE 4)
-  PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
-  PROPOSED STORM SEWER (SEE NOTE 5)
-  TEMPORARY PIPE CULVERT

GENERAL EROSION CONTROL NOTES:

TREE TRUNK PROTECTION, TREE ROOT PRUNING, AND TREE PRUNING WILL BE REQUIRED FOR THIS PROJECT. A REPRESENTATIVE FROM IDOT'S ROADSIDE DEVELOPMENT UNIT WILL DETERMINE LOCATIONS ON SITE AT THE START OF CONSTRUCTION. PAY ITEMS HAVE BEEN INCLUDED IN THE SUMMARY OF QUANTITIES.

THE CONTRACTOR WILL BE REQUIRED TO SUBMIT CERTIFICATION STATEMENT (NPDES ATTACHMENT 6). FULL COMPLIANCE WITH ALL TERMS OF THE NPDES PERMIT MUST BE STRICTLY ADHERED TO.

DRAINAGE STRUCTURE INLET FILTERS ARE TO BE PROVIDED AND MAINTAINED FOR ALL EXISTING AND PROPOSED DRAINAGE STRUCTURE WITHIN THE PROJECT LIMITS AND ALL STRUCTURES NOTED FOR INLET AND PIPE PROTECTION AS DIRECTED BY THE ENGINEER. PAY ITEMS, DETAILS AND SPECIAL PROVISIONS HAVE BEEN INCLUDED IN THE SUMMARY OF QUANTITIES, SPECIAL PROVISIONS, AND PLAN DETAILS FOR THIS WORK.

NOTES

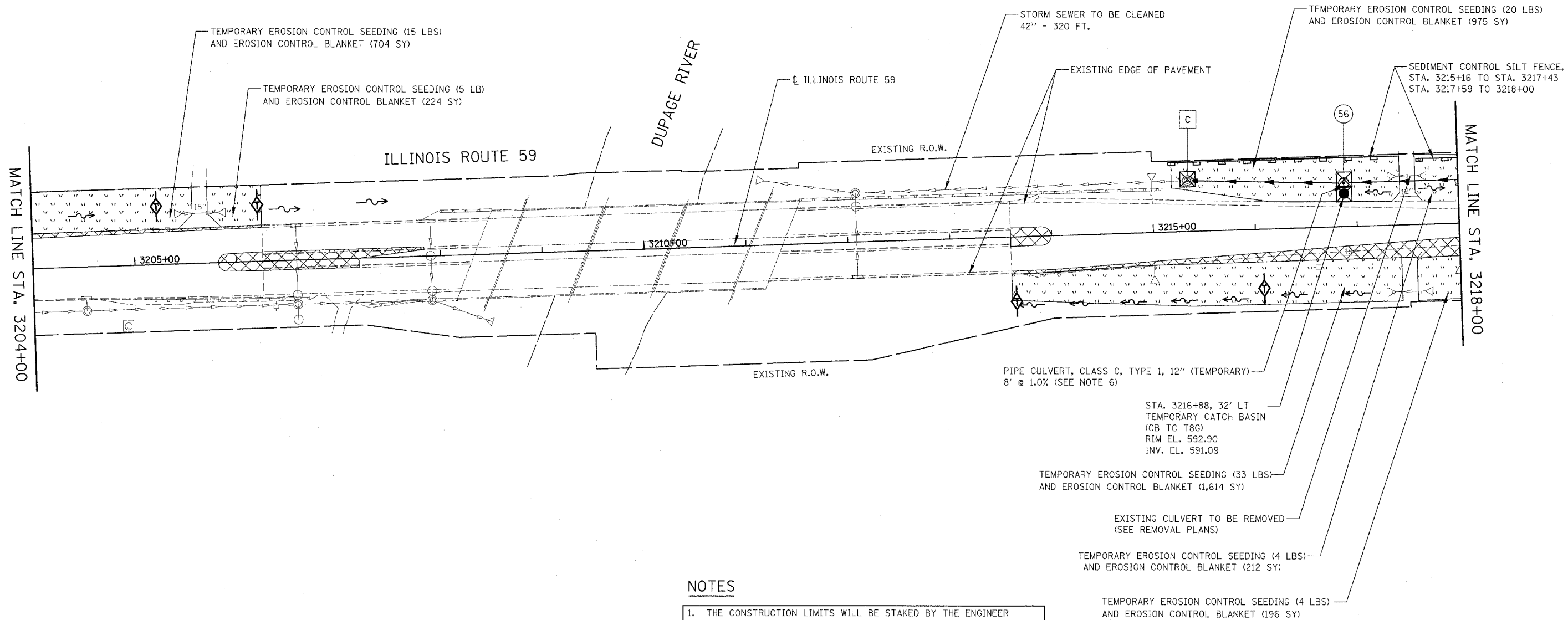
1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE PRE-STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 PRE-STAGE 1, 1A AND 1B
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY BSB
 CHECKED BY HME

REP. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	322
STA.	3204+00	TO STA.	3218+00	
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.

2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.

3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.

4. SEE PRE-STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.

5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.

6. TEMPORARY PIPE CULVERT SHALL BE TEMPORARILY CONNECTED TO PROPOSED MANHOLE. COST OF TEMPORARY CONNECTION SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF "PIPE CULVERT, CLASS C, TYPE 1, 12". (TEMPORARY)".

EROSION CONTROL LEGEND

TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET

TEMPORARY PAVEMENT

SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)

INLET FILTER

DRAINAGE STRUCTURE TO BE CLEANED

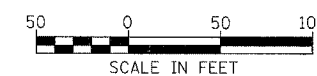
TEMPORARY DITCH CHECK

FLOW DIRECTION (SEE NOTE 4)

PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)

PROPOSED STORM SEWER (SEE NOTE 5)

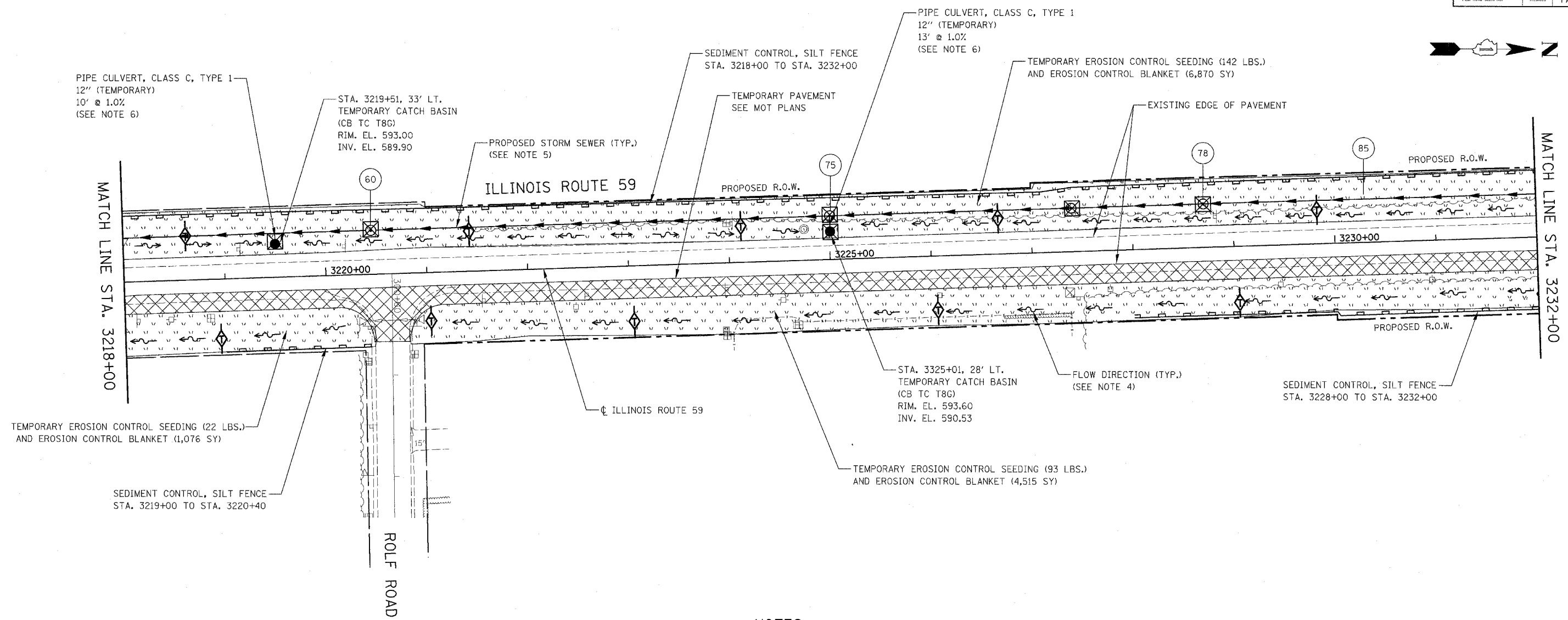
TEMPORARY PIPE CULVERT



ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG
EROSION CONTROL PLAN
PRE-STAGE 1, 1A AND 1B
ILLINOIS ROUTE 59

SCALE AS SHOWN
DATE MARCH 18, 2008
DRAWN BY BSB
CHECKED BY HME

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	323
STA. 3218+00		TO STA. 3232+00		
FED. ROAD DIST. NO.		ILLINOIS	FAP 338 (IL RTE. 59)	

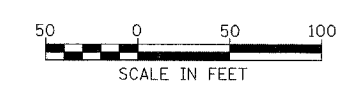


NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE PRE-STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. TEMPORARY PIPE CULVERT SHALL BE TEMPORARILY CONNECTED TO PROPOSED MANHOLE OR SEWER. COST OF TEMPORARY CONNECTION SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF "PIPE CULVERT, CLASS C, TYPE 1, 12". (TEMPORARY)".

EROSION CONTROL LEGEND

- | | | | |
|--|---|--|--|
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET | | DRAINAGE STRUCTURE TO BE CLEANED |
| | TEMPORARY PAVEMENT | | TEMPORARY DITCH CHECK |
| | SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2) | | FLOW DIRECTION (SEE NOTE 4) |
| | INLET FILTER | | PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5) |
| | | | PROPOSED STORM SEWER (SEE NOTE 5) |
| | | | TEMPORARY PIPE CULVERT |

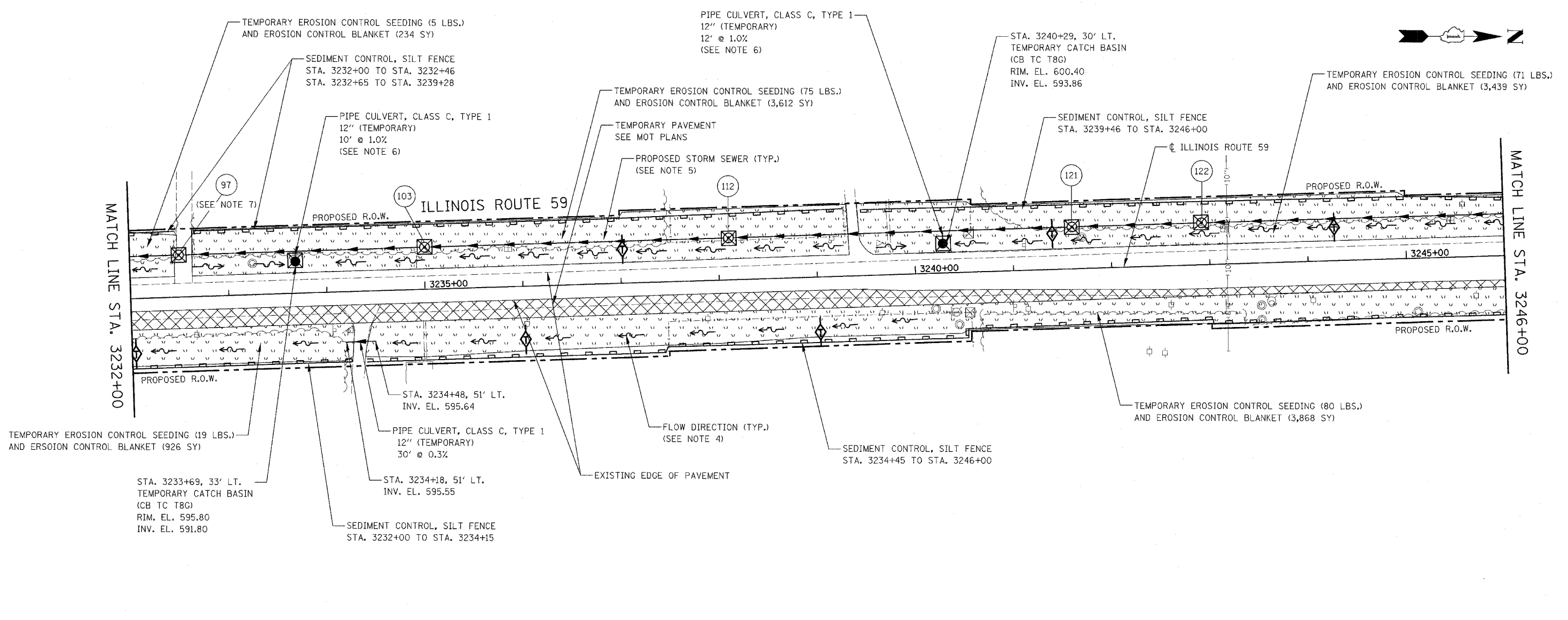


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 PRE-STAGE 1, 1A AND 1B
 ILLINOIS ROUTE 59

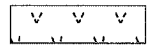

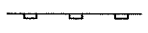







SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY BSB
 CHECKED BY HME

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	324
STA. 3232+00		TO STA. 3246+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		

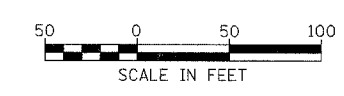


EROSION CONTROL LEGEND

-  TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET
-  TEMPORARY PAVEMENT
-  SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)
-  INLET FILTER
-  DRAINAGE STRUCTURE TO BE CLEANED
-  TEMPORARY DITCH CHECK
-  FLOW DIRECTION (SEE NOTE 4)
-  PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
-  PROPOSED STORM SEWER (SEE NOTE 5)
-  TEMPORARY PIPE CULVERT

NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE PRE-STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. TEMPORARY PIPE CULVERT SHALL BE TEMPORARILY CONNECTED TO PROPOSED MANHOLE OR SEWER. COST OF TEMPORARY CONNECTION SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF "PIPE CULVERT, CLASS C, TYPE 1, 12". (TEMPORARY)".
7. PROPOSED STRUCTURE SHALL BE BURIED AND TEMPORARILY COVERED WITH STEEL PLATE OF 0.25 INCH MINIMUM THICKNESS OR OF THICKNESS SPECIFIED AND APPROVED BY THE ENGINEER. PAYMENT FOR THE INSTALLATION AND REMOVAL OF THE STEEL PLATE INCLUDING ANY REQUIRED EXCAVATION SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.

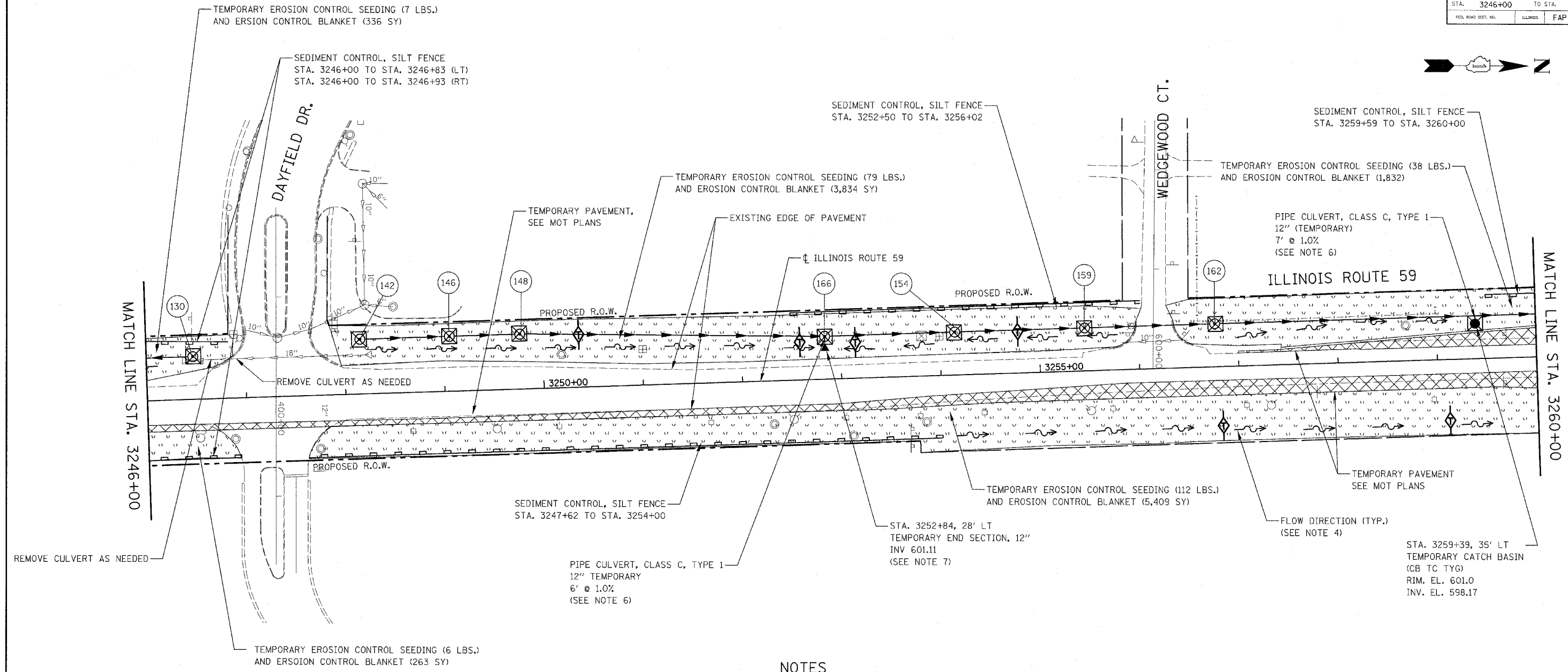


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 PRE-STAGE 1, 1A AND 1B
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY BSB
 CHECKED BY HME

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	325
STA. 3246+00		TO STA. 3260+00		
FED. ROAD DIST. NO.		ILLINOIS	FAP 338 (IL RTE. 59)	



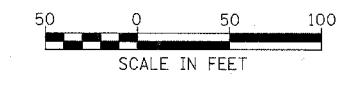
EROSION CONTROL LEGEND

- | | | | |
|--|---|--|--|
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET | | TEMPORARY DITCH CHECK |
| | TEMPORARY PAVEMENT | | FLOW DIRECTION (SEE NOTE 4) |
| | SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2) | | PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5) |
| | INLET FILTER | | PROPOSED STORM SEWER (SEE NOTE 5) |
| | DRAINAGE STRUCTURE TO BE CLEANED | | TEMPORARY PIPE CULVERT |

NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
6. TEMPORARY PIPE CULVERT SHALL BE TEMPORARILY CONNECTED TO PROPOSED MANHOLE OR SEWER. COST OF TEMPORARY CONNECTION SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF "PIPE CULVERT, CLASS C, TYPE 1, 12\" (TEMPORARY)".

2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE PRE-STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
7. TYPE OF TEMPORARY END SECTION REQUIRED AS END TREATMENT WILL BE SUBJECT FOR APPROVAL BY THE ENGINEER. PAYMENT FOR THIS TEMPORARY END SECTION (12") SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF "PIPE CULVERT, CLASS C, TYPE 1, 12\" (TEMPORARY)".

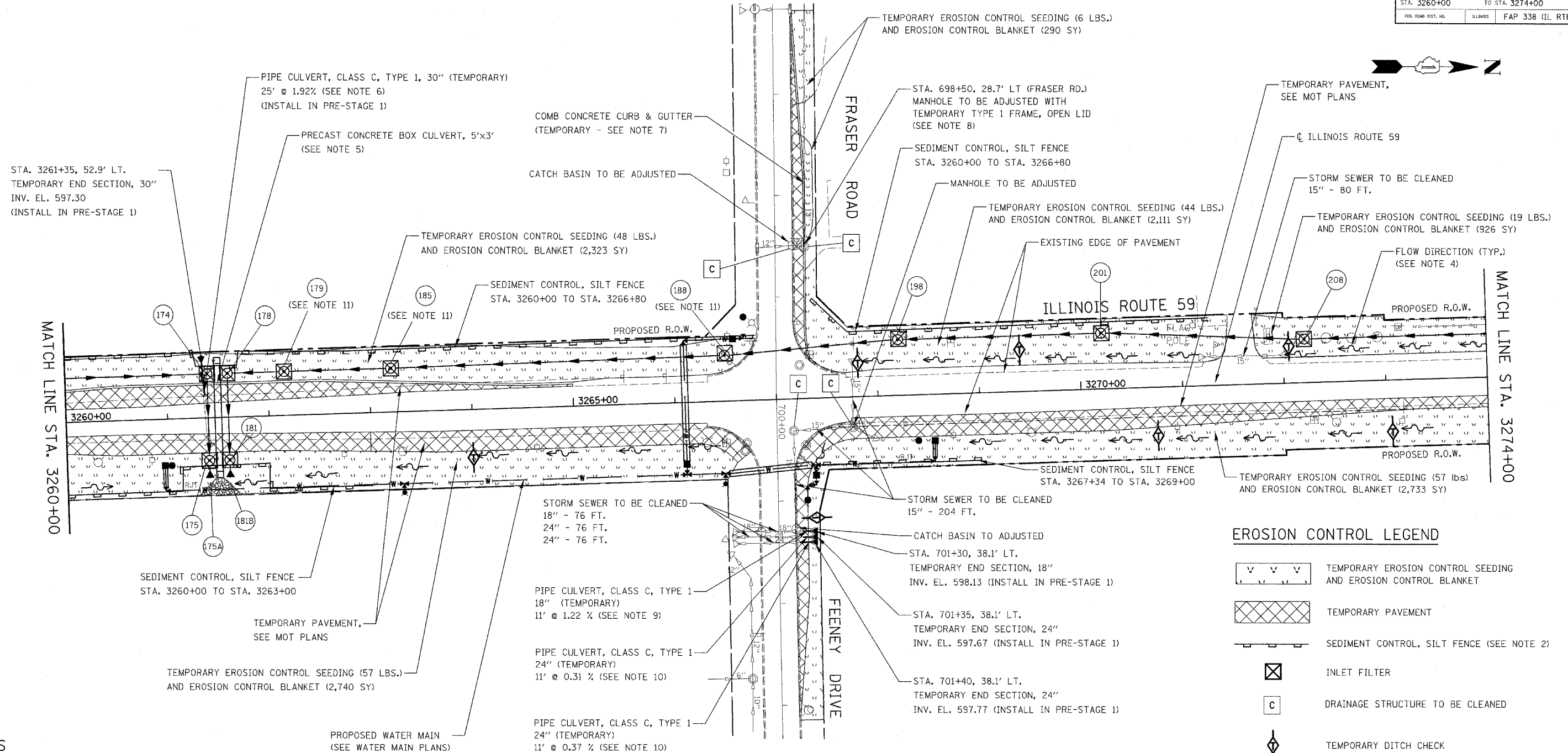


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 PRE-STAGE 1, 1A AND 1B
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY BSB
 CHECKED BY HME

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	326
STA. 3260+00		TO STA. 3274+00		
FED. ROAD DIST. NO.		ILLINOIS FAP 338 (IL RTE. 59)		



EROSION CONTROL LEGEND

- TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET
- TEMPORARY PAVEMENT
- SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)
- INLET FILTER
- DRAINAGE STRUCTURE TO BE CLEANED
- TEMPORARY DITCH CHECK
- FLOW DIRECTION (SEE NOTE 4)
- PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
- PROPOSED STORM SEWER (SEE NOTE 5)
- TEMPORARY PIPE CULVERT

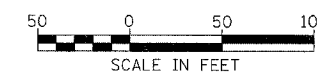
NOTES

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2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE PRE-STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.

6. TEMPORARY PIPE CULVERT SHALL BE TEMPORARILY CONNECTED TO EXISTING CULVERT AS DIRECTED BY THE ENGINEER. COST OF TEMPORARY CONNECTION SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF "PIPE CULVERT, CLASS C, TYPE 1, 30" (TEMPORARY)".
7. SEE MOT PLANS FOR CONCRETE CURB INFORMATION.
8. EXISTING MANHOLE RIM ELEVATION SHALL BE ADJUSTED TO TEMPORARY PAVEMENT SURFACE ELEVATION. PAYMENT OF TEMPORARY TYPE 1 FRAME, OPEN LID IS CONSIDERED INCLUDED IN THE COST OF MANHOLE ADJUSTMENT, "MANHOLE TO BE ADJUSTED".
9. TEMPORARY PIPE CULVERT SHALL BE TEMPORARILY CONNECTED TO EXISTING CULVERT AS DIRECTED BY THE ENGINEER. COST OF TEMPORARY CONNECTION SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF "PIPE CULVERT, CLASS C, TYPE 1, 18" (TEMPORARY)".

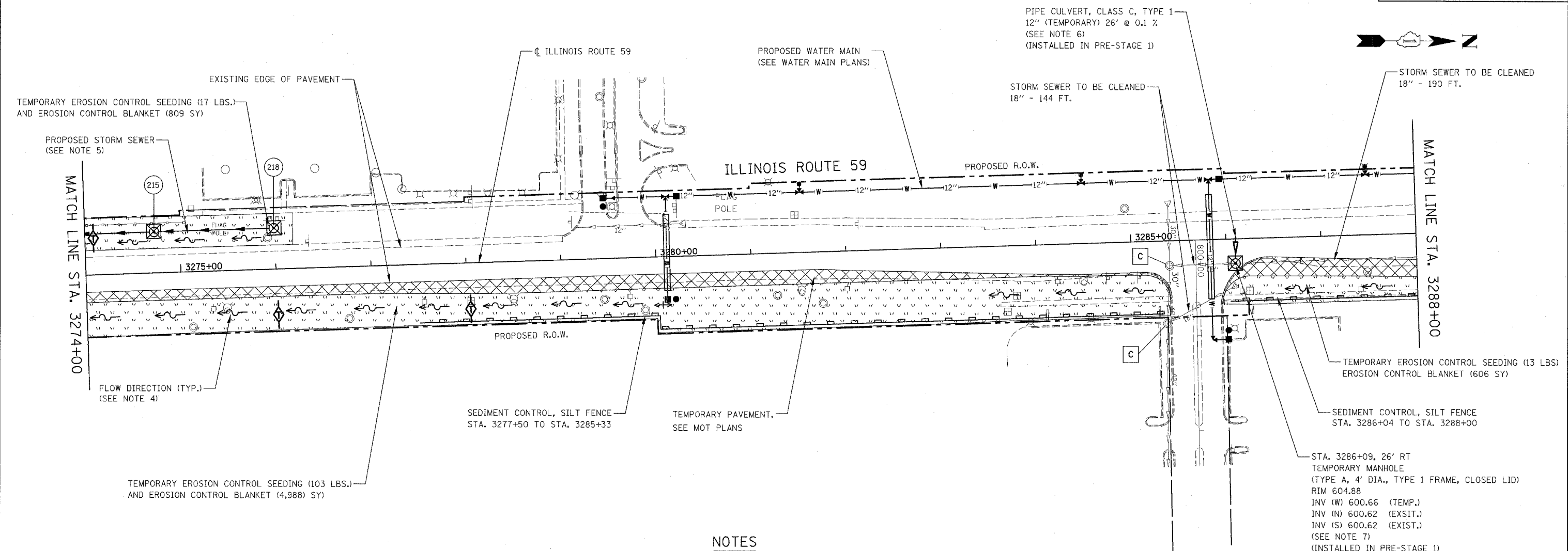
10. TEMPORARY PIPE CULVERT SHALL BE TEMPORARILY CONNECTED TO EXISTING CULVERT AS DIRECTED BY THE ENGINEER. COST OF TEMPORARY CONNECTION SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF "PIPE CULVERT, CLASS C, TYPE 1, 24" (TEMPORARY)".

11. TO CONSTRUCT TEMPORARY PAVEMENT IN STAGES 1 AND 1A, PROPOSED STRUCTURE SHALL BE BURIED AND TEMPORARILY COVERED WITH STEEL PLATE OF 0.25 INCH MINIMUM THICKNESS OR OF THICKNESS SPECIFIED AND APPROVED BY THE ENGINEER. PAYMENT FOR THE INSTALLATION AND REMOVAL OF THE STEEL PLATE INCLUDING ANY REQUIRED EXCAVATION SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 PRE-STAGE 1, 1A AND 1B
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY BSB
 CHECKED BY HME

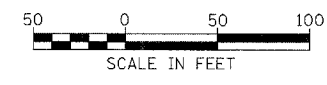


NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE PRE-STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. TEMPORARY PIPE WILL BE USED DURING FUTURE STAGE 1 STORM WATER FLOW. WEST END OF PIPE SHALL BE INSTALLED TO A MINIMUM DISTANCE OF 2' WEST OF IL 59 CL. PROVIDE TEMPORARY BULKHEAD AT UPSTREAM END OF PIPE. PAYMENT FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF "PIPE CULVERT, CLASS C, TYPE 1, 12" (TEMPORARY)".
7. INSTALL TEMPORARY MANHOLE OVER CL OF EXISTING STORM SEWER.

EROSION CONTROL LEGEND

- | | | | |
|--|---|--|--|
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET | | TEMPORARY DITCH CHECK |
| | TEMPORARY PAVEMENT | | FLOW DIRECTION (SEE NOTE 4) |
| | SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2) | | PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5) |
| | INLET FILTER | | PROPOSED STORM SEWER (SEE NOTE 5) |
| | DRAINAGE STRUCTURE TO BE CLEANED | | TEMPORARY PIPE CULVERT |

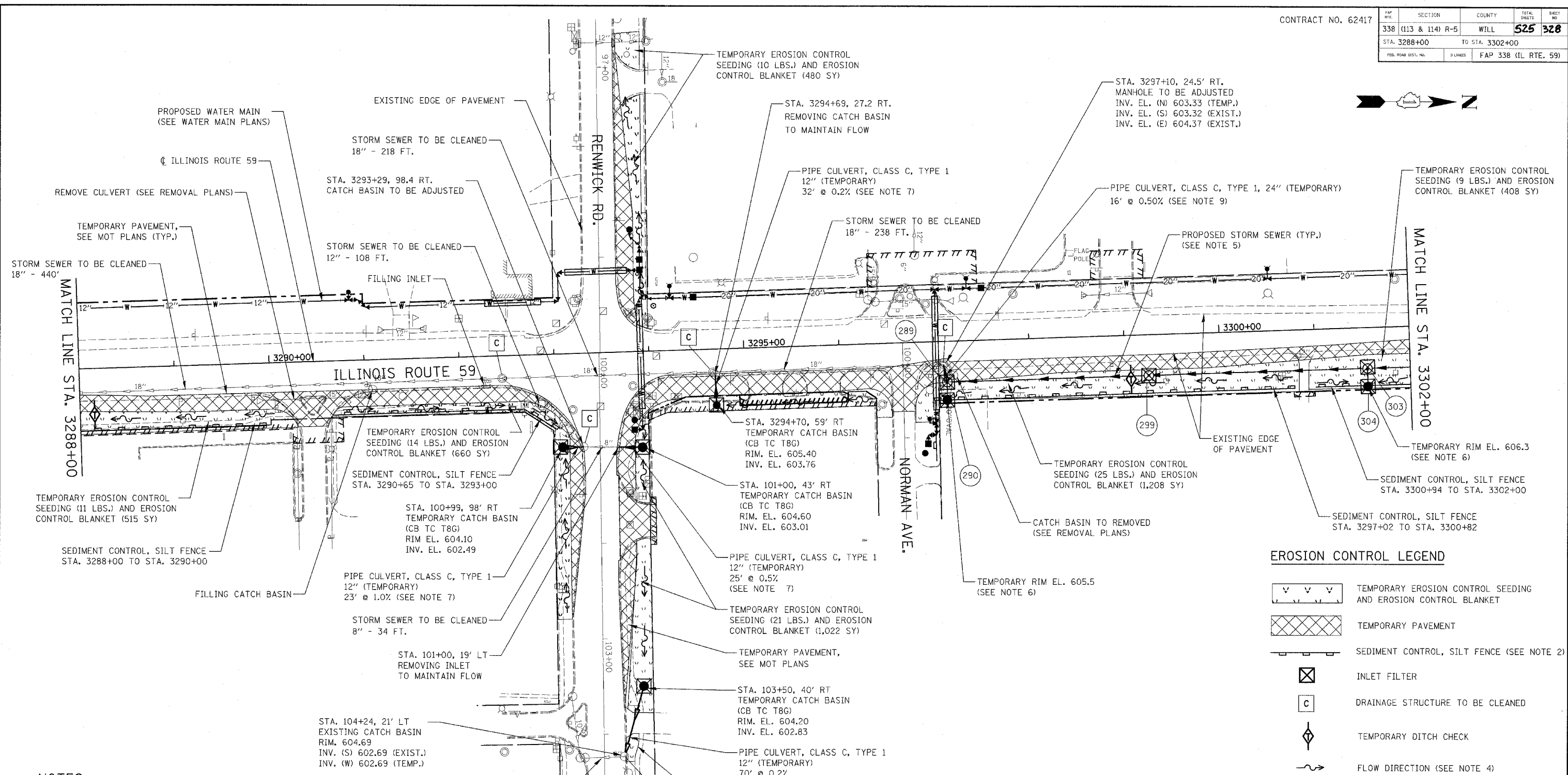


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 PRE-STAGE 1, 1A AND 1B
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY BSB
 CHECKED BY HME

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	328
STA. 3288+00		TO STA. 3302+00		
FED. ROAD DIST. NO.		FAP 338 (IL RTE. 59)		



EROSION CONTROL LEGEND

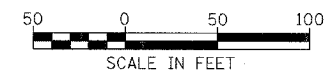
- TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET
- TEMPORARY PAVEMENT
- SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)
- INLET FILTER
- DRAINAGE STRUCTURE TO BE CLEANED
- TEMPORARY DITCH CHECK
- FLOW DIRECTION (SEE NOTE 4)
- PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
- PROPOSED STORM SEWER (SEE NOTE 5)
- TEMPORARY PIPE CULVERT

NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE PRE-STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.

5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. ANY REQUIRED ADJUSTMENT AND/OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPARATELY. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.
7. TEMPORARY PIPE CULVERT SHALL BE TEMPORARILY CONNECTED TO EXISTING STORM SEWER. COST OF TEMPORARY CONNECTION SHALL BE CONSIDERED INCLUDED IN UNIT PRICE OF "PIPE CULVERT, CLASS C, TYPE 1, 12\", (TEMPORARY)".

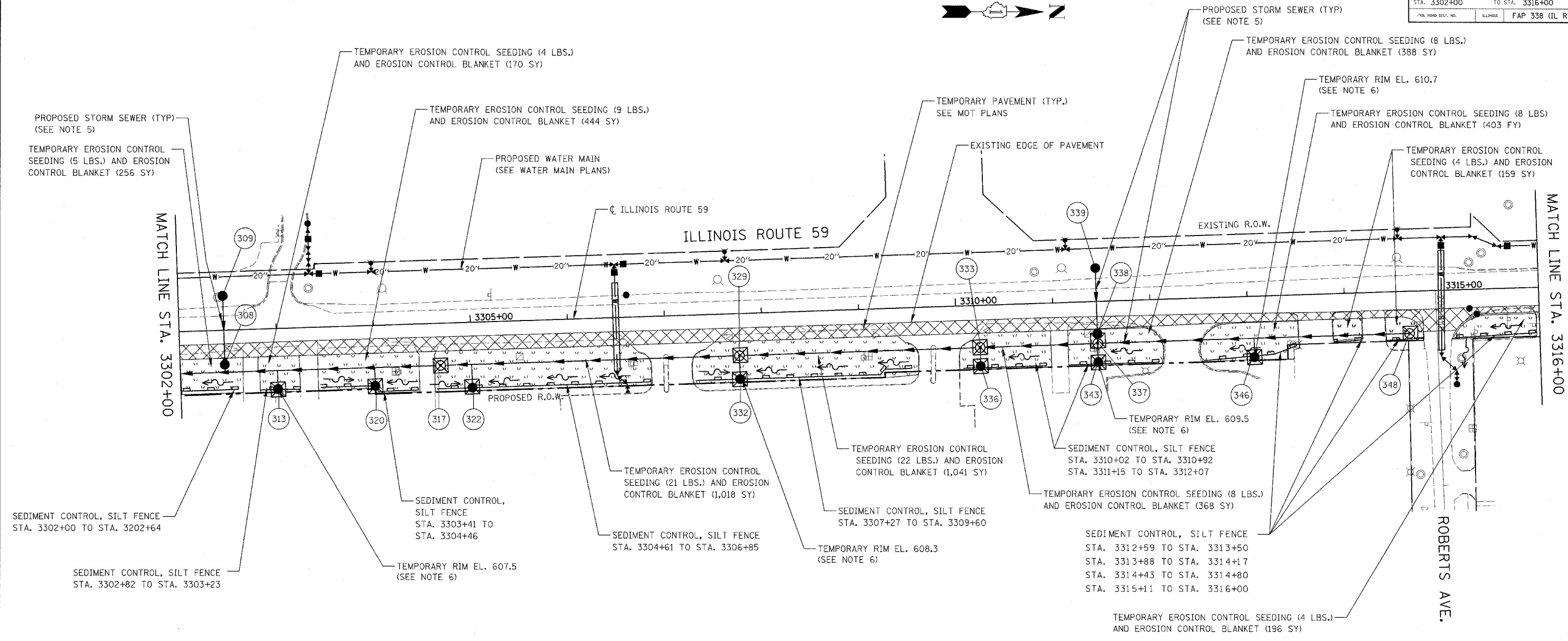
8. TEMPORARY PIPE CULVERT SHALL BE TEMPORARILY CONNECTED TO EXISTING MANHOLE. COST OF TEMPORARY CONNECTION SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF "PIPE CULVERT, CLASS C, TYPE 1, 24\", (TEMPORARY)".
9. TEMPORARY PIPE CULVERT SHALL BE TEMPORARILY CONNECTED TO PROPOSED MANHOLE AND TO EXISTING MANHOLE. COST OF TEMPORARY CONNECTION SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF "PIPE CULVERT, CLASS C, TYPE 1, 24\", (TEMPORARY)".



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 PRE-STAGE 1, 1A AND 1B
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY BSB
 CHECKED BY HME

FAP	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	329
STA. 3302+00		TO STA. 3316+00		
FAP ROAD DIST. NO.		ILLINOIS	FAP 338 (IL RTE. 59)	

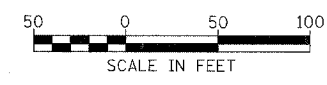


NOTES

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2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE PRE-STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. ANY REQUIRED ADJUSTMENT AND/ OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPARATELY. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.

EROSION CONTROL LEGEND

- | | | | |
|--|---|--|--|
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET | | DRAINAGE STRUCTURE TO BE CLEANED |
| | TEMPORARY PAVEMENT | | TEMPORARY DITCH CHECK |
| | SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2) | | FLOW DIRECTION (SEE NOTE 4) |
| | INLET FILTER | | PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5) |
| | | | PROPOSED STORM SEWER (SEE NOTE 5) |
| | | | TEMPORARY PIPE CULVERT |

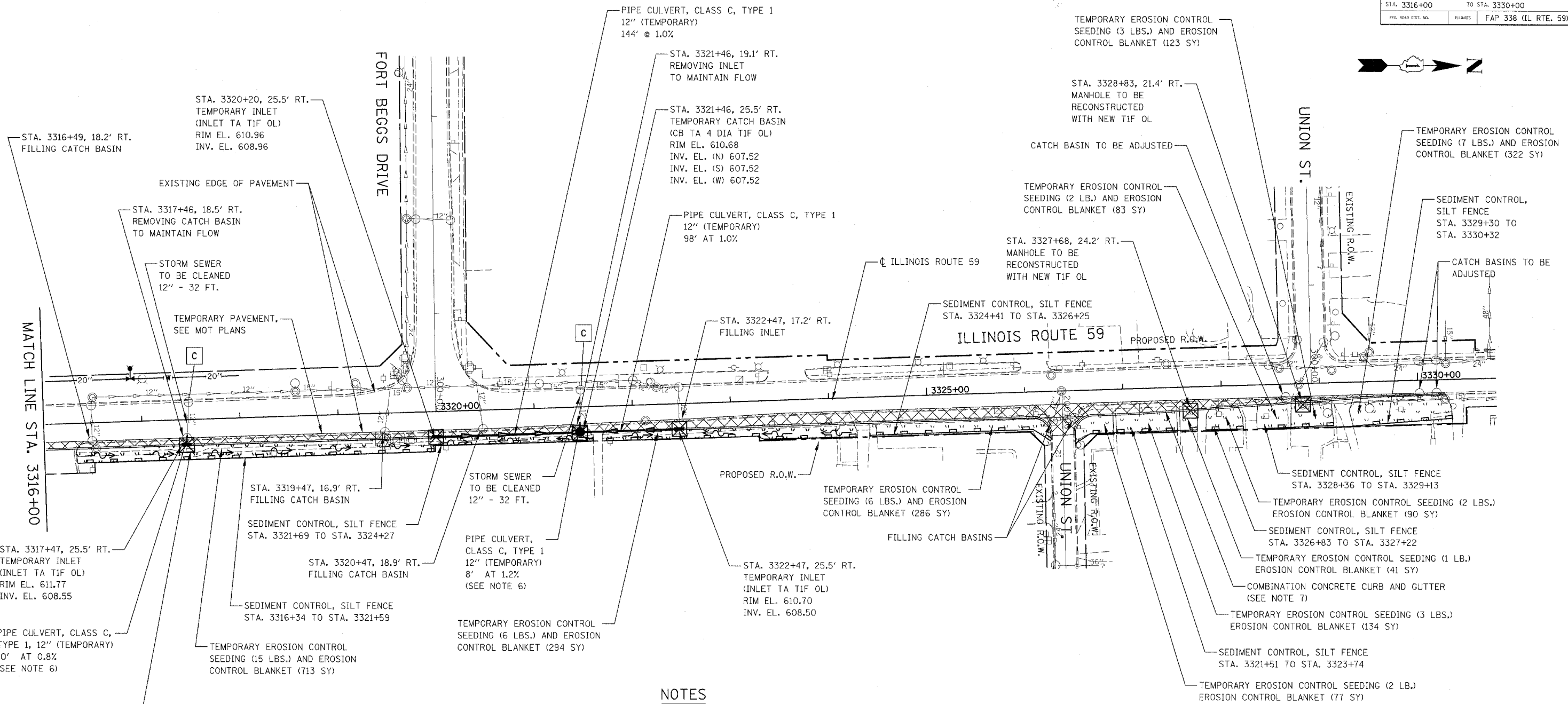


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 PRE-STAGE 1, 1A AND 1B
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY BSB
 CHECKED BY HME

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	330
SIAL: 3316+00		TO STA. 3330+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



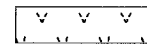

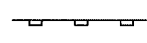







NOTES

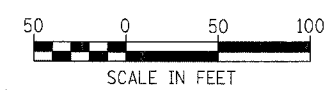
1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES. NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.

6. TEMPORARY PIPE CULVERT SHALL BE TEMPORARILY CONNECTED TO EXISTING STORM SEWER. PAYMENT FOR TEMPORARY CONNECTION SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF "PIPE CULVERT, CLASS C, TYPE 1, 12\" (TEMPORARY)".

7. SEE MOT PLANS FOR CONCRETE CURB INFORMATION.

EROSION CONTROL LEGEND

-  TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET
-  TEMPORARY PAVEMENT
-  SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)
-  INLET FILTER
-  DRAINAGE STRUCTURE TO BE CLEANED
-  TEMPORARY DITCH CHECK
-  FLOW DIRECTION (SEE NOTE 4)
-  PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
-  PROPOSED STORM SEWER (SEE NOTE 5)
-  TEMPORARY PIPE CULVERT

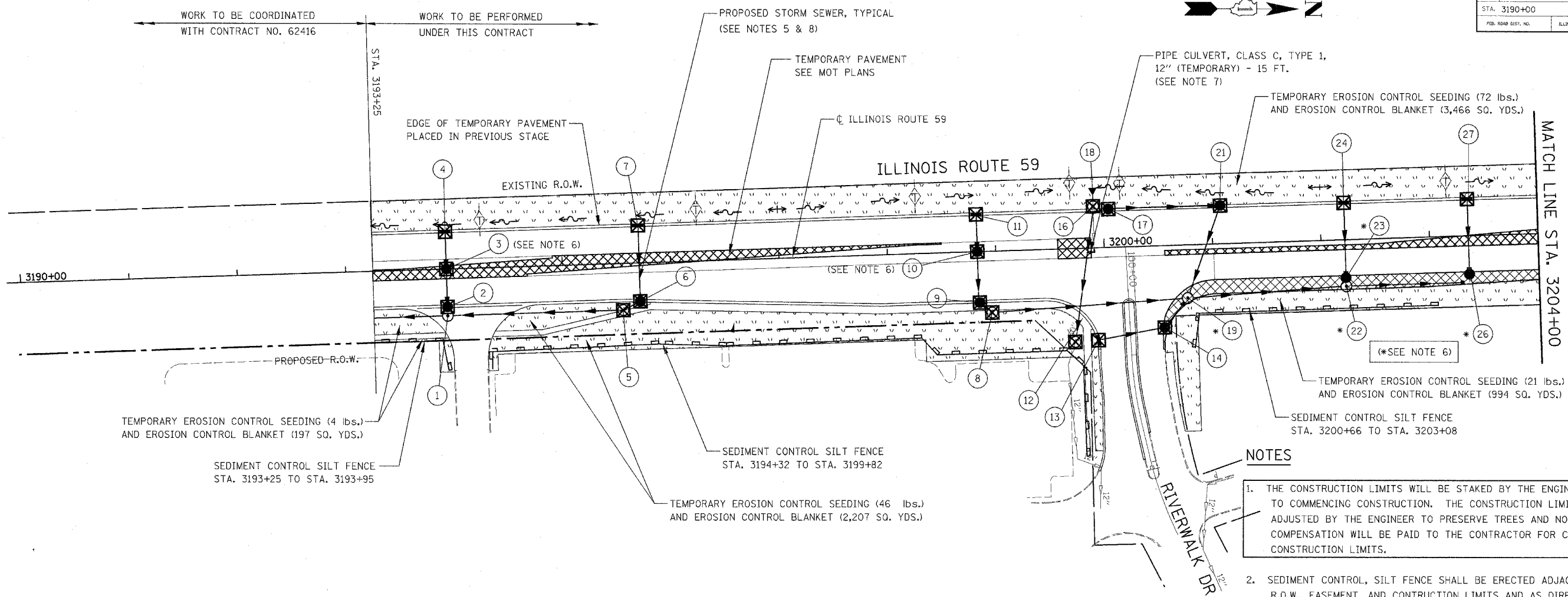


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 PRE-STAGE 1, 1A AND 1B
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY BSB
 CHECKED BY HME

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	331
STA. 3190+00		TO STA. 3204+00		
FED. ROAD DIST. NO.		ILLINOIS	FAP 338 (IL RTE. 59)	



NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. PROPOSED STRUCTURE SHALL BE BURIED AND TEMPORARILY COVERED WITH STEEL PLATE OF 0.25 INCH MINIMUM THICKNESS OR OF THICKNESS SPECIFIED AND APPROVED BY THE ENGINEER. PAYMENT FOR THE INSTALLATION AND REMOVAL OF THE STEEL PLATE INCLUDING ANY REQUIRED EXCAVATION SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.
7. TEMPORARY PIPE CULVERT SHALL BE TEMPORARILY CONNECTED TO THE EXISTING 30" STORM SEWER AND TO THE INSTALLED PROPOSED 24" STORM SEWER. PAYMENT FOR THESE TEMPORARY CONNECTIONS SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF "PIPE CULVERT, CLASS C, TYPE 1, 12" (TEMPORARY)".
8. PROPOSED LATERAL STORM SEWERS SHALL BE INSTALLED IN STAGE 1 AND STAGE 1A, RESPECTIVELY. SEE MOT STAGES 1 AND 1A FOR WORKZONE LIMIT INFORMATION.

EROSION CONTROL LEGEND

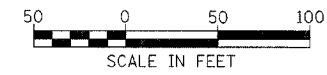
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	TEMPORARY PAVEMENT		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	DRAINAGE STRUCTURE TO BE CLEANED		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT
	INLET FILTER		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE
	INLET FILTER INSTALLED IN PREVIOUS STAGE		

GENERAL EROSION CONTROL NOTES:

TREE TRUNK PROTECTION, TREE ROOT PRUNING, AND TREE PRUNING WILL BE REQUIRED FOR THIS PROJECT. A REPRESENTATIVE FROM IDOT'S ROADSIDE DEVELOPMENT UNIT WILL DETERMINE LOCATIONS ON SITE AT THE START OF CONSTRUCTION. PAY ITEMS HAVE BEEN INCLUDED IN THE SUMMARY OF QUANTITIES.

THE CONTRACTOR WILL BE REQUIRED TO SUBMIT CERTIFICATION STATEMENT (NPDES ATTACHMENT 6). FULL COMPLIANCE WITH ALL TERMS OF THE NPDES PERMIT MUST BE STRICTLY ADHERED TO.

DRAINAGE STRUCTURE INLET FILTERS ARE TO BE PROVIDED AND MAINTAINED FOR ALL EXISTING AND PROPOSED DRAINAGE STRUCTURE WITHIN THE PROJECT LIMITS AND ALL STRUCTURES NOTED FOR INLET AND PIPE PROTECTION AS DIRECTED BY THE ENGINEER. PAY ITEMS, DETAILS AND SPECIAL PROVISIONS HAVE BEEN INCLUDED IN THE SUMMARY OF QUANTITIES, SPECIAL PROVISIONS, AND PLAN DETAILS FOR THIS WORK.



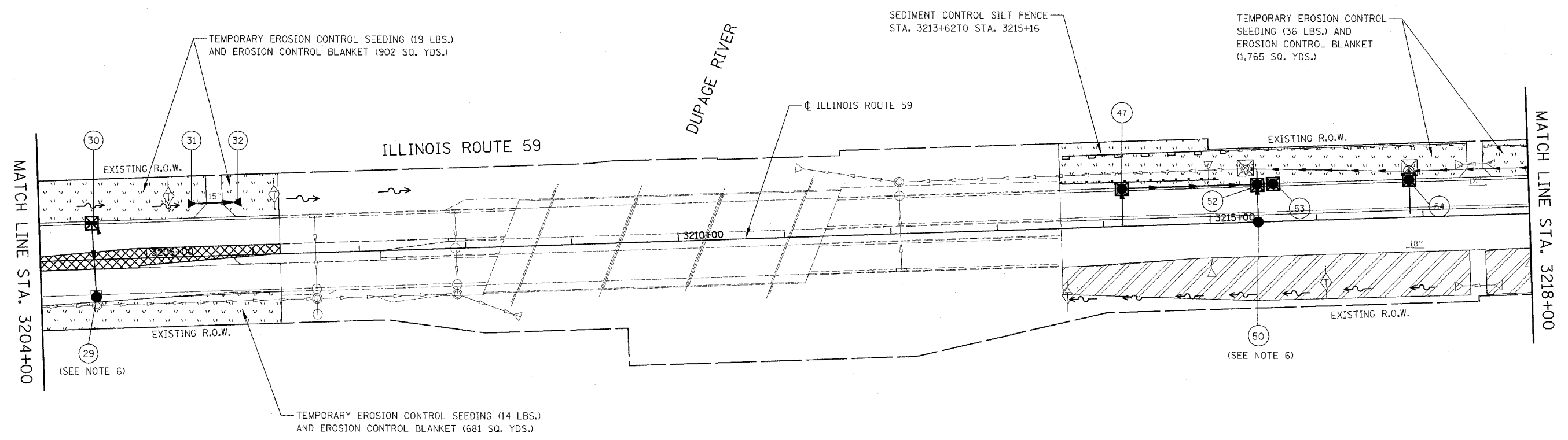
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG

EROSION CONTROL PLAN
STAGE 1, 1A, 1B AND 1C
ILLINOIS ROUTE 59

SCALE AS SHOWN
DATE MARCH 18, 2008

DRAWN BY BSB
CHECKED BY HME

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338 (113 & 114) R-5	WILL		525	392
STA. 3204+00		TO STA. 3218+00		
FED. ROAD DIST. NO.	BLINDS	FAP 338 (IL RTE. 59)		

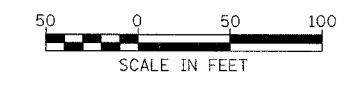


EROSION CONTROL LEGEND

	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	TEMPORARY PAVEMENT		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	DRAINAGE STRUCTURE TO BE CLEANED		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT
	INLET FILTER		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE
	INLET FILTER INSTALLED IN PREVIOUS STAGE		

NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. PROPOSED STRUCTURE SHALL BE BURIED AND TEMPORARILY COVERED WITH STEEL PLATE OF 0.25 INCH MINIMUM THICKNESS OR OF THICKNESS SPECIFIED AND APPROVED BY THE ENGINEER. PAYMENT FOR THE INSTALLATION AND REMOVAL OF THE STEEL PLATE INCLUDING ANY REQUIRED EXCAVATION SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.

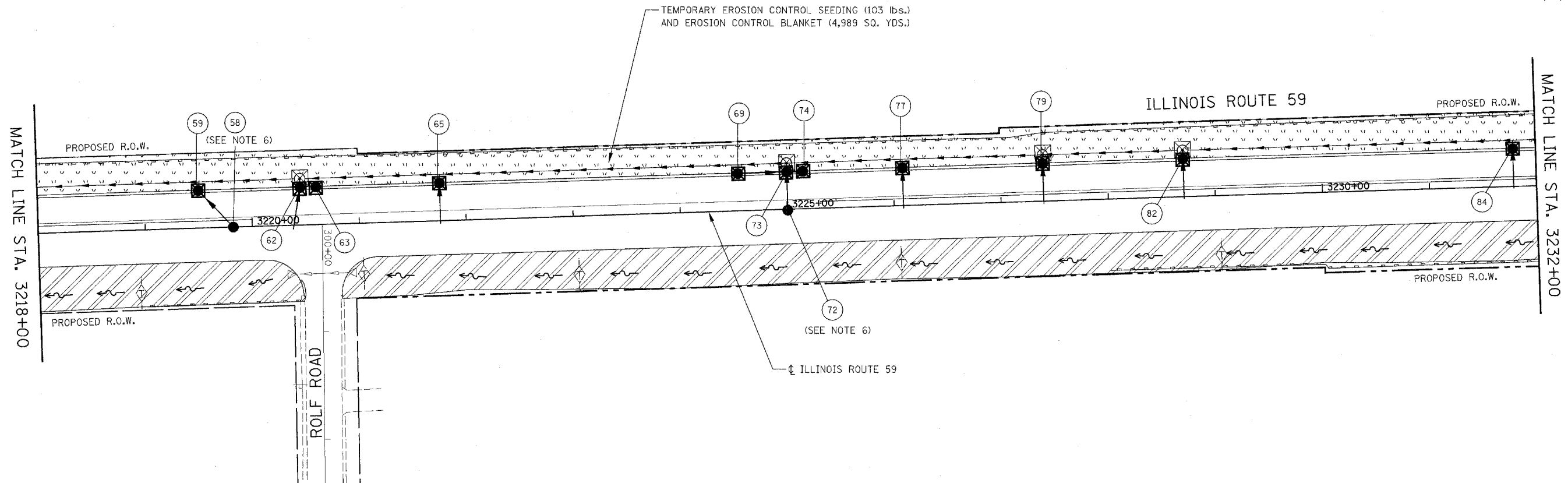


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGE 1, 1A, 1B AND 1C
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY BSB
 CHECKED BY HME

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	333
STA. 3218+00		TO STA. 3232+00		
FED. ROAD DIST. NO.		ILLINOIS FAP 338 (IL RTE. 59)		



EROSION CONTROL LEGEND

	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	TEMPORARY PAVEMENT		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	DRAINAGE STRUCTURE TO BE CLEANED		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT
	INLET FILTER		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE
	INLET FILTER INSTALLED IN PREVIOUS STAGE		

NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.

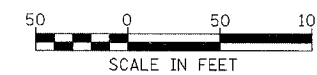
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.

3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.

4. SEE STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.

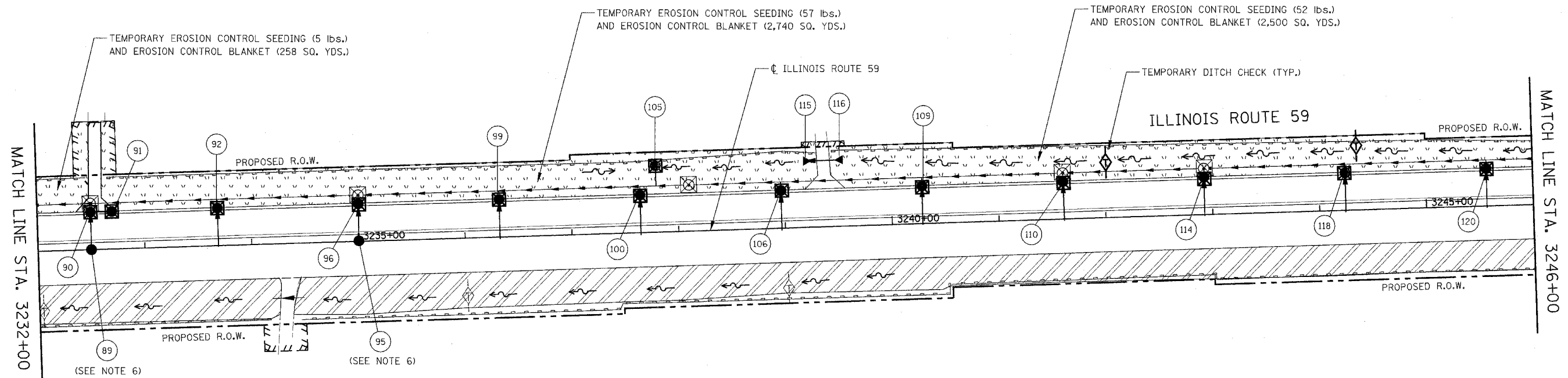
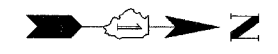
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.

6. PROPOSED STRUCTURE SHALL BE BURIED AND TEMPORARILY COVERED WITH STEEL PLATE OF 0.25 INCH MINIMUM THICKNESS OR OF THICKNESS SPECIFIED AND APPROVED BY THE ENGINEER. PAYMENT FOR THE INSTALLATION AND REMOVAL OF THE STEEL PLATE INCLUDING ANY REQUIRED EXCAVATION SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGE 1, 1A, 1B AND 1C
 ILLINOIS ROUTE 59
 SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY BSB
 CHECKED BY HME

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	334
STA. 3232+00		TO STA. 3246+00		
FED. ROAD DIST. NO.		ILLINOIS	FAP 338 (IL RTE. 59)	



	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	TEMPORARY PAVEMENT		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	DRAINAGE STRUCTURE TO BE CLEANED		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT
	INLET FILTER		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE
	INLET FILTER INSTALLED IN PREVIOUS STAGE		

NOTES

- THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
- SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
- SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
- SEE STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.
- SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
- PROPOSED STRUCTURE SHALL BE BURIED AND TEMPORARILY COVERED WITH STEEL PLATE OF 0.25 INCH MINIMUM THICKNESS OR OF THICKNESS SPECIFIED AND APPROVED BY THE ENGINEER. PAYMENT FOR THE INSTALLATION AND REMOVAL OF THE STEEL PLATE INCLUDING ANY REQUIRED EXCAVATION SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.

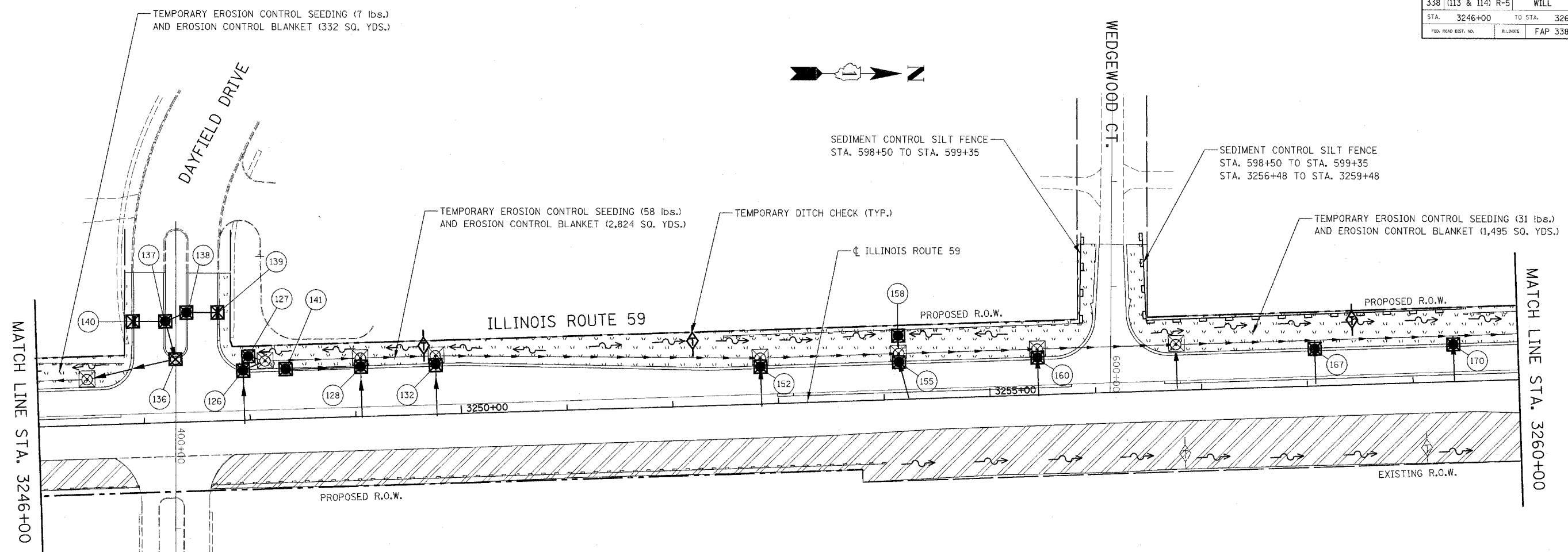


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGE 1, 1A, 1B AND 1C
 ILLINOIS ROUTE 59

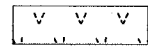

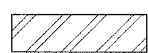

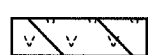

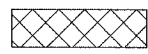






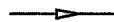



SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY BSB
 CHECKED BY HME

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	335
STA.	3246+00	TO STA.	3260+00	
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		

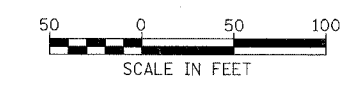


EROSION CONTROL LEGEND

- | | | | |
|---|---|---|--|
|  | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET |  | TEMPORARY DITCH CHECK |
|  | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE |  | TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE |
|  | PERMANENT LANDSCAPING (SEE NOTE 3) |  | FLOW DIRECTION (SEE NOTE 4) |
|  | TEMPORARY PAVEMENT |  | PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5) |
|  | DRAINAGE STRUCTURE TO BE CLEANED |  | PROPOSED STORM SEWER (SEE NOTE 5) |
|  | SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2) |  | PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE |
|  | SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE |  | TEMPORARY PIPE CULVERT |
|  | INLET FILTER |  | TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE |
|  | INLET FILTER INSTALLED IN PREVIOUS STAGE | | |

NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.

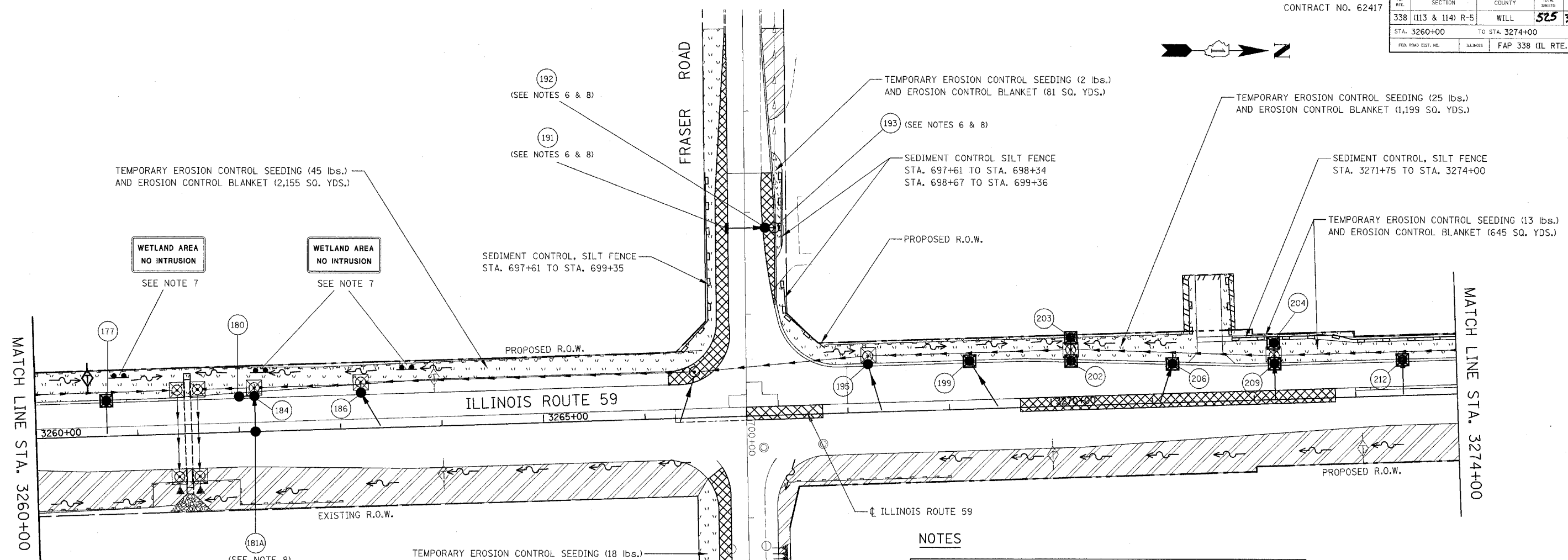


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
EROSION CONTROL PLAN
 STAGE 1, 1A, 1B AND 1C
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

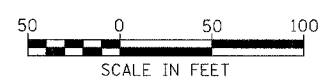
DRAWN BY BSB
 CHECKED BY HME

MAP SHEET NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	336
STA. 3260+00		TO STA. 3274+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



EROSION CONTROL LEGEND

- | | | | |
|--|---|--|--|
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET | | TEMPORARY DITCH CHECK |
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE | | TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE |
| | PERMANENT LANDSCAPING (SEE NOTE 3) | | FLOW DIRECTION (SEE NOTE 4) |
| | TEMPORARY PAVEMENT | | PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5) |
| | DRAINAGE STRUCTURE TO BE CLEANED | | PROPOSED STORM SEWER (SEE NOTE 5) |
| | SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2) | | PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE |
| | SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE | | TEMPORARY PIPE CULVERT |
| | INLET FILTER | | TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE |
| | INLET FILTER INSTALLED IN PREVIOUS STAGE | | |

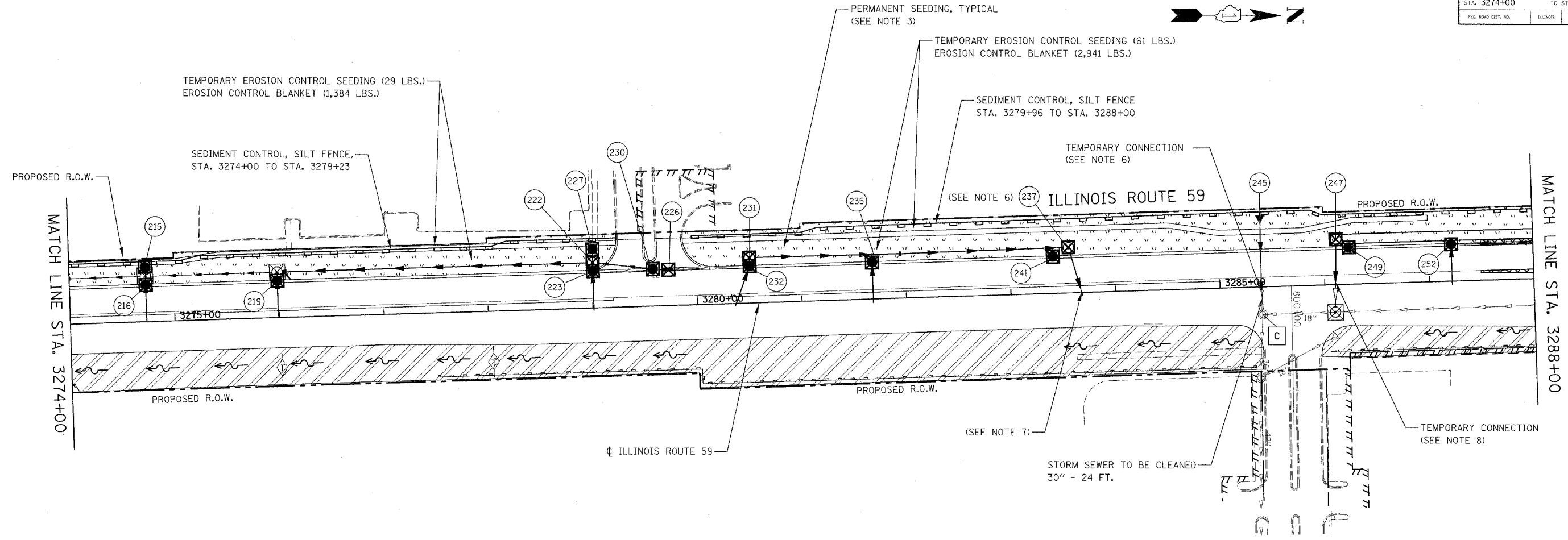


NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. ANY REQUIRED ADJUSTMENT AND/OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPERATELY. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.
7. CONTRACTOR SHALL ERECT SIGNS AS DIRECTED BY THE ENGINEER. COST OF ERECTING SIGNS SHALL BE CONSIDERED INCLUDED IN THE COST OF SEDIMENT CONTROL SILT FENCE. THE SIGNS WILL BE SUPPLIED BY IDOT. THE CONTRACTOR SHALL CONTACT RICK WANNER AT (847) 705-4172 WHEN SIGNS ARE NEEDED. THE SIGN SHALL REMAIN IN PLACE WHILE WORK IS BEING DONE IN THIS AREA, AND SHALL BE TAKEN DOWN ONLY WHEN DIRECTED BY THE ENGINEER.
8. PROPOSED STRUCTURE SHALL BE BURIED AND TEMPORARILY COVERED WITH STEEL PLATE OF 0.25 INCH MINIMUM THICKNESS OR OF THICKNESS SPECIFIED AND APPROVED BY THE ENGINEER. PAYMENT FOR THE INSTALLATION AND REMOVAL OF THE STEEL PLATE INCLUDING ANY REQUIRED EXCAVATION SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
EROSION CONTROL PLAN
 STAGE 1, 1A, 1B AND 1C
 ILLINOIS ROUTE 59
 SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY BSB
 CHECKED BY HME

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	337
STA. 3274+00		TO STA. 3288+00		
FED. ROAD DIST. NO.		ILLINOIS	FAP 338 (IL RTE. 59)	

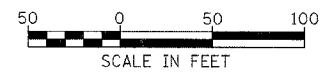


EROSION CONTROL LEGEND

	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	TEMPORARY PAVEMENT		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	DRAINAGE STRUCTURE TO BE CLEANED		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT
	INLET FILTER		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE
	INLET FILTER INSTALLED IN PREVIOUS STAGE		

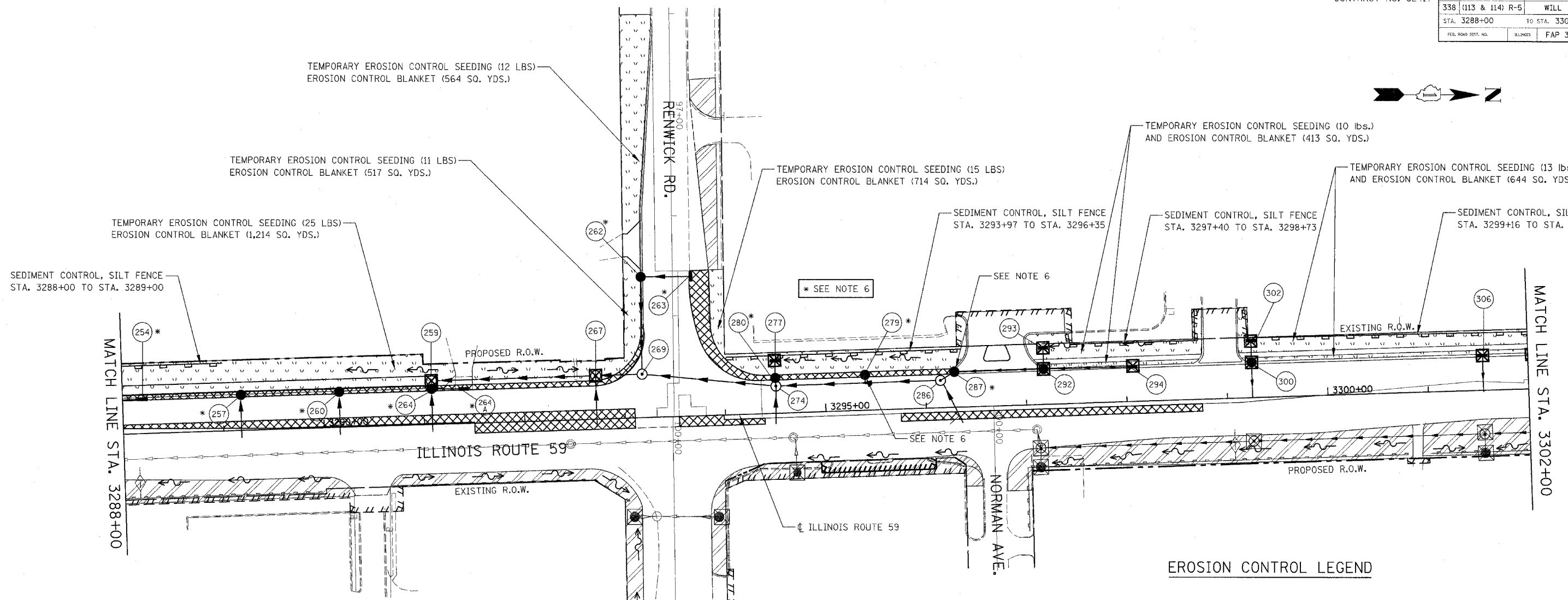
NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. PROPOSED 30" STORM SEWER SHALL BE TEMPORARILY CONNECTED TO THE EXISTING 30" PIPE CULVERT. COST OF TEMPORARY CONNECTION SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF "STORM SEWERS, CLASS A, TY 1, 30 INCH".
7. TEMPORARY CLOSING / PLUGGING OF PROPOSED PIPE INSTALLED SHALL BE APPROVED BY THE ENGINEER.
8. PROPOSED 36" STORM SEWER SHALL BE TEMPORARILY CONNECTED TO THE TEMPORARY 12" PIPE CULVERT. COST OF TEMPORARY CONNECTION SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF "STORM SEWERS, CLASS A, TY 1, 36 INCH".



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGE 1, 1A, 1B AND 1C
 ILLINOIS ROUTE 59
 SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY BSB
 CHECKED BY HME

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	338
STA. 3288+00		TO STA. 3302+00		
FED. ROAD DIST. NO.	BLANKETS	FAP 338 (IL RTE. 59)		



NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.

2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.

3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.

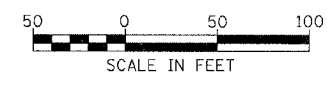
4. SEE STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.

5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.

6. PROPOSED STRUCTURE SHALL BE BURIED AND TEMPORARILY COVERED WITH STEEL PLATE OF 0.25 INCH MINIMUM THICKNESS OR OF THICKNESS SPECIFIED AND APPROVED BY THE ENGINEER. PAYMENT FOR THE INSTALLATION AND REMOVAL OF THE STEEL PLATE INCLUDING ANY REQUIRED EXCAVATION SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.

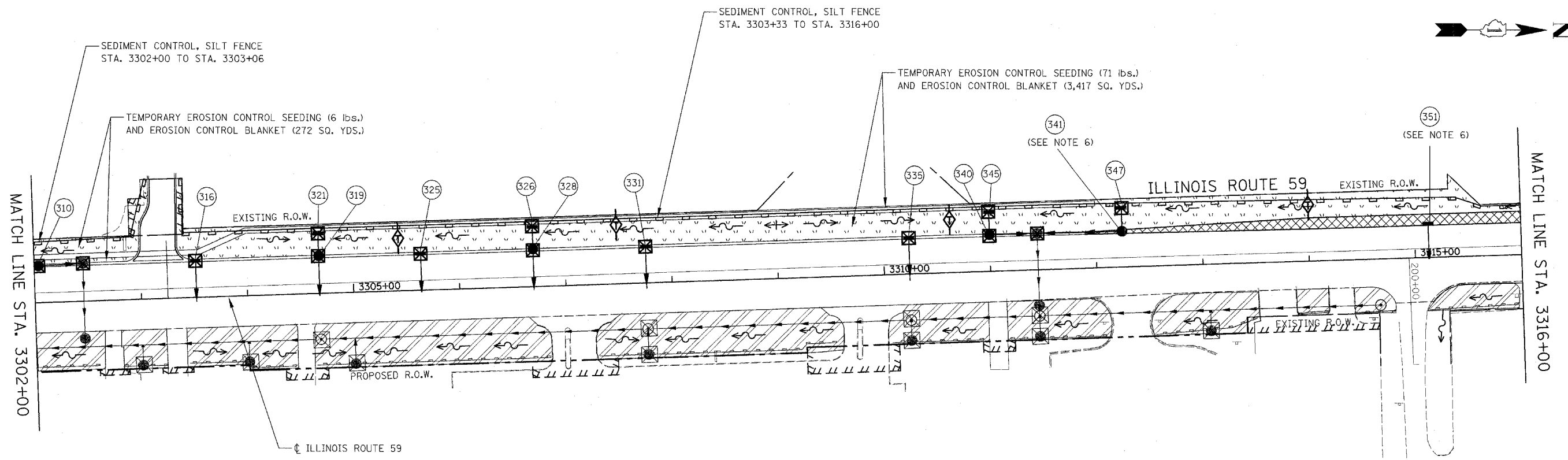
EROSION CONTROL LEGEND

	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	TEMPORARY PAVEMENT		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	DRAINAGE STRUCTURE TO BE CLEANED		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT
	INLET FILTER		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE
	INLET FILTER INSTALLED IN PREVIOUS STAGE		



ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG
EROSION CONTROL PLAN
STAGE 1, 1A, 1B AND 1C
ILLINOIS ROUTE 59
SCALE AS SHOWN
DATE MARCH 18, 2008
DRAWN BY BSB
CHECKED BY HME

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	339
STA. 3302+00		TO STA. 3316+00		
FED. ROAD DIST. NO.		ILLINOIS FAP 338 (IL RTE. 59)		



EROSION CONTROL LEGEND

	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	TEMPORARY PAVEMENT		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	DRAINAGE STRUCTURE TO BE CLEANED		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT
	INLET FILTER		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE
	INLET FILTER INSTALLED IN PREVIOUS STAGE		

NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.

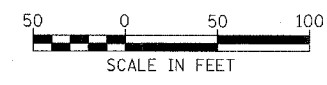
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.

3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.

4. SEE STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.

5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.

6. PROPOSED STRUCTURE SHALL BE BURIED AND TEMPORARILY COVERED WITH STEEL PLATE OF 0.25 INCH MINIMUM THICKNESS OR OF THICKNESS SPECIFIED AND APPROVED BY THE ENGINEER. PAYMENT FOR THE INSTALLATION AND REMOVAL OF THE STEEL PLATE INCLUDING ANY REQUIRED EXCAVATION SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.

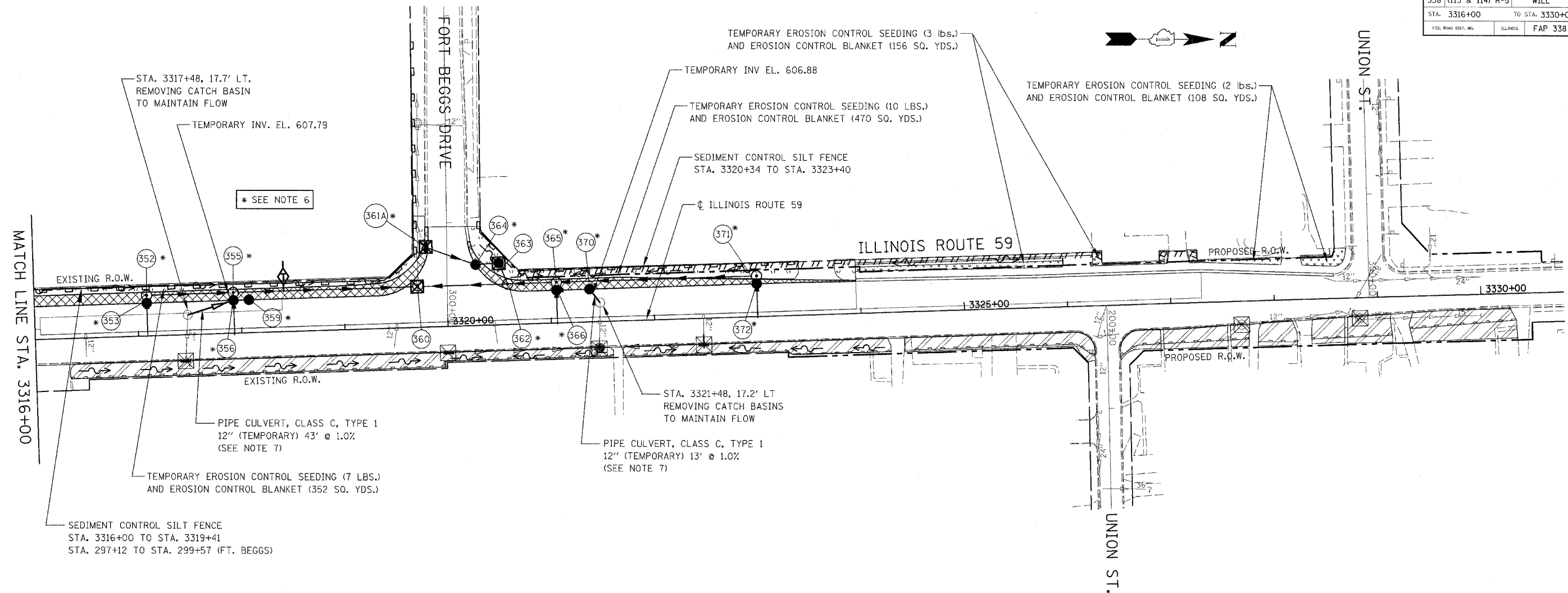


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGE 1, 1A, 1B AND 1C
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY BSB
 CHECKED BY HME

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	340
STA. 3316+00		TO STA. 3330+00		
FAP 338 (IL RTE. 59)				

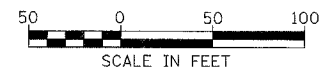


EROSION CONTROL LEGEND

	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	TEMPORARY PAVEMENT		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	DRAINAGE STRUCTURE TO BE CLEANED		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT
	INLET FILTER		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE
	INLET FILTER INSTALLED IN PREVIOUS STAGE		

NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 1 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. PROPOSED STRUCTURE SHALL BE BURIED AND TEMPORARILY COVERED WITH STEEL PLATE OF 0.25 INCH MINIMUM THICKNESS OR OF THICKNESS SPECIFIED AND APPROVED BY THE ENGINEER. PAYMENT FOR THE INSTALLATION AND REMOVAL OF THE STEEL PLATE INCLUDING ANY REQUIRED EXCAVATION SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.
7. TEMPORARY PIPE CULVERT SHALL BE TEMPORARILY CONNECTED TO THE EXISTING STORM SEWER AND TO THE PROPOSED MANHOLE. PAYMENT FOR TEMPORARY CONNECTIONS SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF "PIPE CULVERT, CLASS C, TYPE 1, 12" (TEMPORARY)".

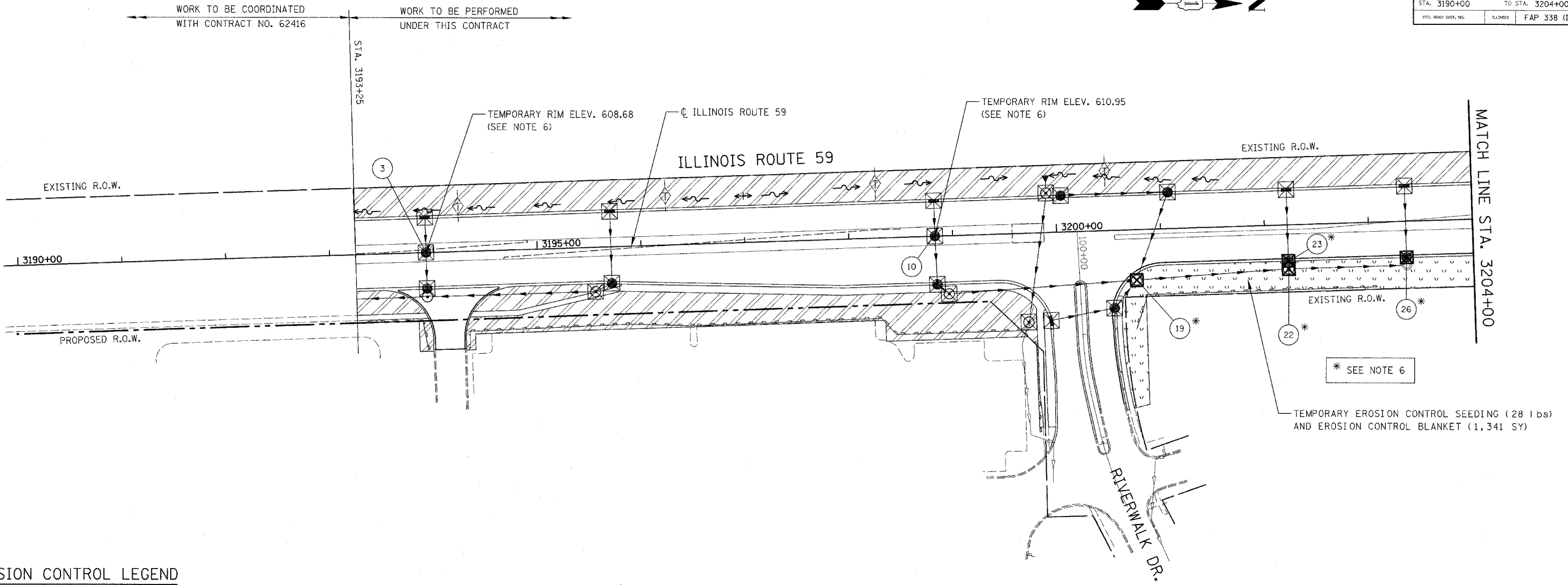


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
EROSION CONTROL PLAN
 STAGE 1, 1A, 1B AND 1C
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY BSB
 CHECKED BY HME

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	341
STA. 3190+00		TO STA. 3204+00		
FPO. ROAD DIST. NO.		ILLINOIS FAP 338 (IL RTE. 59)		



* SEE NOTE 6

TEMPORARY EROSION CONTROL SEEDING (28 lbs) AND EROSION CONTROL BLANKET (1,341 SY)

EROSION CONTROL LEGEND

- | | | | |
|--|---|--|--|
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET | | TEMPORARY DITCH CHECK |
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE | | TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE |
| | PERMANENT LANDSCAPING (SEE NOTE 3) | | FLOW DIRECTION (SEE NOTE 4) |
| | DRAINAGE STRUCTURE TO BE CLEANED | | PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5) |
| | SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2) | | PROPOSED STORM SEWER (SEE NOTE 5) |
| | SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE | | PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE |
| | INLET FILTER | | TEMPORARY PIPE CULVERT |
| | INLET FILTER INSTALLED IN PREVIOUS STAGE | | TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE |

GENERAL EROSION CONTROL NOTES:

TREE TRUNK PROTECTION, TREE ROOT PRUNING, AND TREE PRUNING WILL BE REQUIRED FOR THIS PROJECT. A REPRESENTATIVE FROM IDOT'S ROADSIDE DEVELOPMENT UNIT WILL DETERMINE LOCATIONS ON SITE AT THE START OF CONSTRUCTION. PAY ITEMS HAVE BEEN INCLUDED IN THE SUMMARY OF QUANTITIES.

THE CONTRACTOR WILL BE REQUIRED TO SUBMIT CERTIFICATION STATEMENT (NPDES ATTACHMENT 6). FULL COMPLIANCE WITH ALL TERMS OF THE NPDES PERMIT MUST BE STRICTLY ADHERED TO.

DRAINAGE STRUCTURE INLET FILTERS ARE TO BE PROVIDED AND MAINTAINED FOR ALL EXISTING AND PROPOSED DRAINAGE STRUCTURE WITHIN THE PROJECT LIMITS AND ALL STRUCTURES NOTED FOR INLET AND PIPE PROTECTION AS DIRECTED BY THE ENGINEER. PAY ITEMS, DETAILS AND SPECIAL PROVISIONS HAVE BEEN INCLUDED IN THE SUMMARY OF QUANTITIES, SPECIAL PROVISIONS, AND PLAN DETAILS FOR THIS WORK.

NOTES

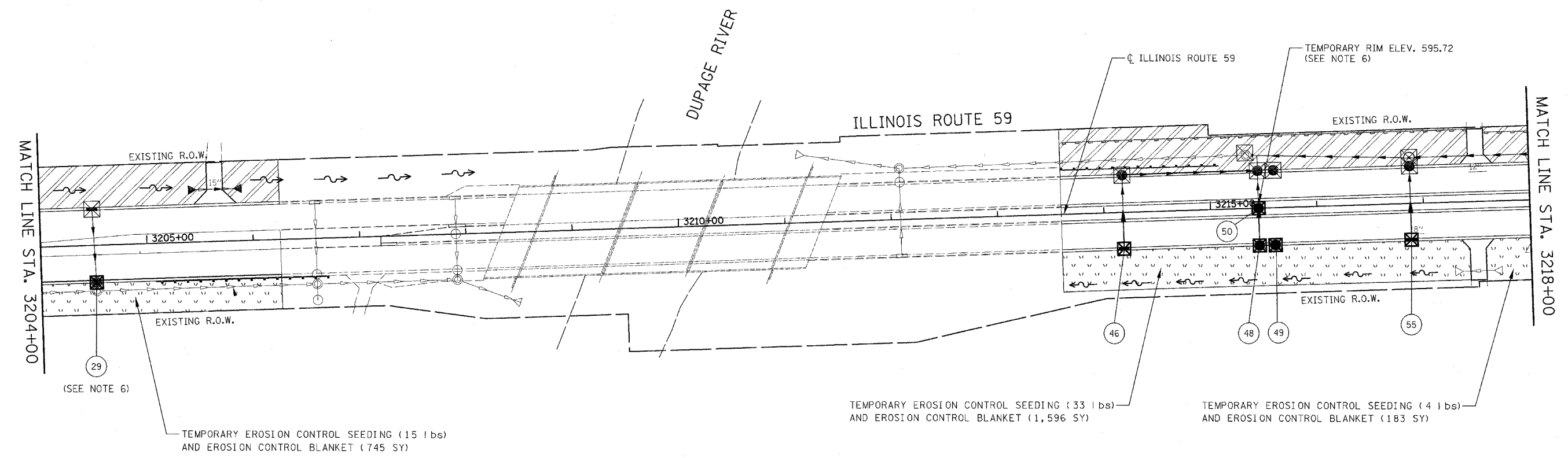
1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 2 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. ANY REQUIRED ADJUSTMENT AND/OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPARATELY. THE COST FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 2, 2A, 2B, 2C & 2D
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY REW
 CHECKED BY JCM

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338 (113 & 114) R-5	WILL	525	342	
STA. 3204+00		TO STA. 3218+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		

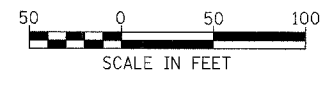


EROSION CONTROL LEGEND

- | | | | |
|--|---|--|--|
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET | | TEMPORARY DITCH CHECK |
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE | | TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE |
| | PERMANENT LANDSCAPING (SEE NOTE 3) | | FLOW DIRECTION (SEE NOTE 4) |
| | DRAINAGE STRUCTURE TO BE CLEANED | | PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5) |
| | SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2) | | PROPOSED STORM SEWER (SEE NOTE 5) |
| | SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE | | PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE |
| | INLET FILTER | | TEMPORARY PIPE CULVERT |
| | INLET FILTER INSTALLED IN PREVIOUS STAGE | | TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE |

NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
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3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 2 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. ANY REQUIRED ADJUSTMENT AND/OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPERATELY. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.

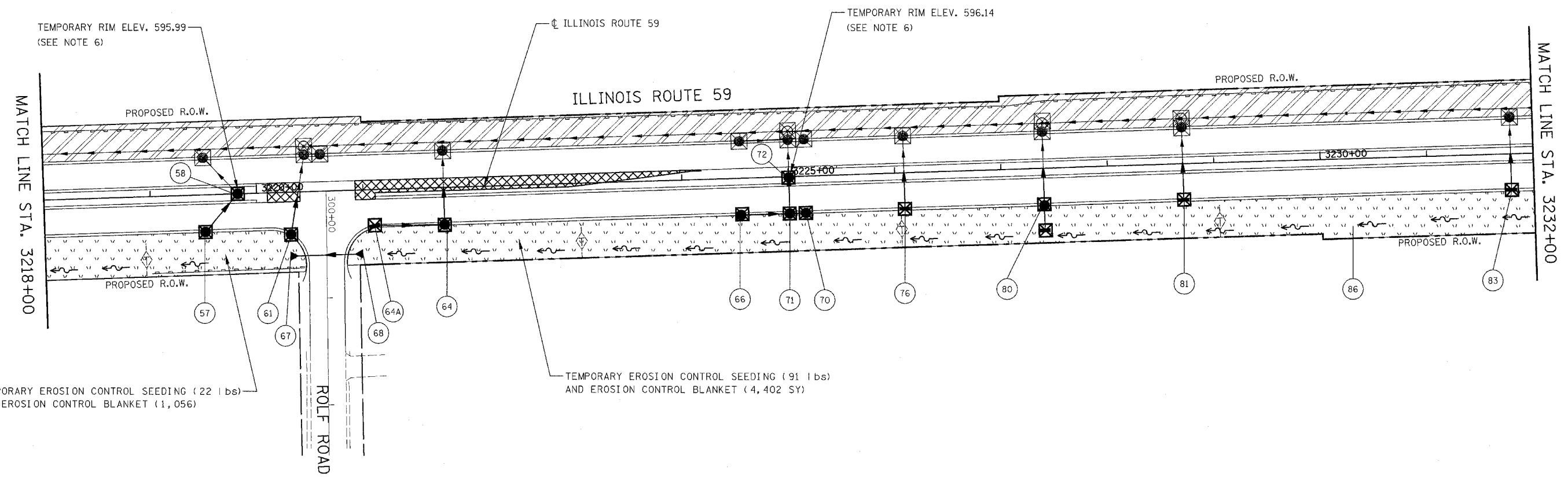


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
EROSION CONTROL PLAN
 STAGES 2, 2A, 2B, 2C & 2D
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY REW
 CHECKED BY JCM

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	343
STA.	3218+00	TO STA.	3232+00	
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		

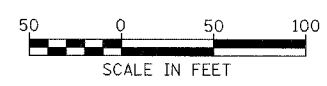


EROSION CONTROL LEGEND

- TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET
- TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE
- PERMANENT LANDSCAPING (SEE NOTE 3)
- DRAINAGE STRUCTURE TO BE CLEANED
- SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)
- SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE
- INLET FILTER
- INLET FILTER INSTALLED IN PREVIOUS STAGE
- TEMPORARY DITCH CHECK
- TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
- FLOW DIRECTION (SEE NOTE 4)
- PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
- PROPOSED STORM SEWER (SEE NOTE 5)
- PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
- TEMPORARY PIPE CULVERT
- TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE

NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 2 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. ANY REQUIRED ADJUSTMENT AND/OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPERATELY. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.

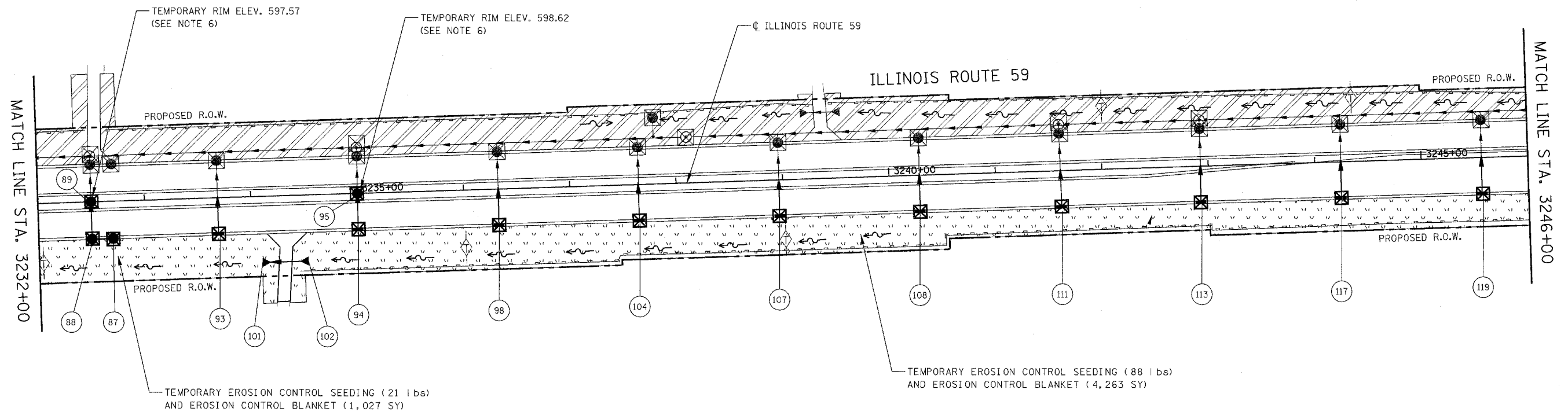


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
EROSION CONTROL PLAN
 STAGES 2, 2A, 2B, 2C & 2D
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY REW
 CHECKED BY JCM

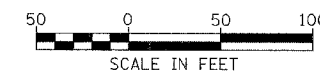
FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338 (113 & 114) R-5	WILL		525	344
STA. 3232+00		TO STA. 3246+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	PERMANENT SEEDING INSTALLED IN PREVIOUS STAGE		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	DRAINAGE STRUCTURE TO BE CLEANED		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT
	INLET FILTER		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE
	INLET FILTER INSTALLED IN PREVIOUS STAGE		

NOTES

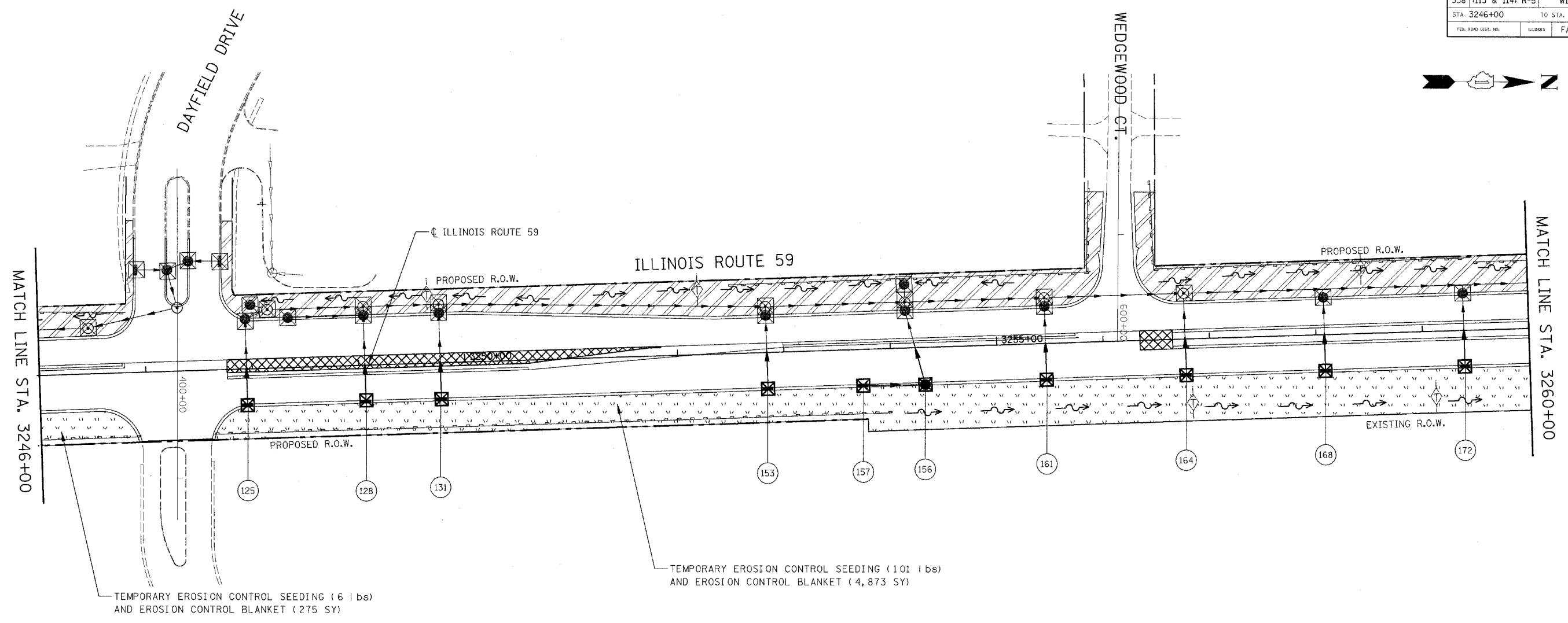
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3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 2 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. ANY REQUIRED ADJUSTMENT AND/OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPERATELY. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 2, 2A, 2B, 2C & 2D
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY REW
 CHECKED BY JCM

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	345
STA. 3246+00		TO STA. 3260+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		

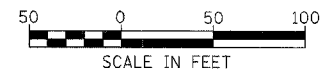


EROSION CONTROL LEGEND

- | | | | |
|--|---|--|--|
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET | | TEMPORARY DITCH CHECK |
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE | | TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE |
| | PERMANENT LANDSCAPING (SEE NOTE 3) | | FLOW DIRECTION (SEE NOTE 4) |
| | DRAINAGE STRUCTURE TO BE CLEANED | | PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5) |
| | SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2) | | PROPOSED STORM SEWER (SEE NOTE 5) |
| | SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE | | PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE |
| | INLET FILTER | | TEMPORARY PIPE CULVERT |
| | INLET FILTER INSTALLED IN PREVIOUS STAGE | | TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE |

NOTES

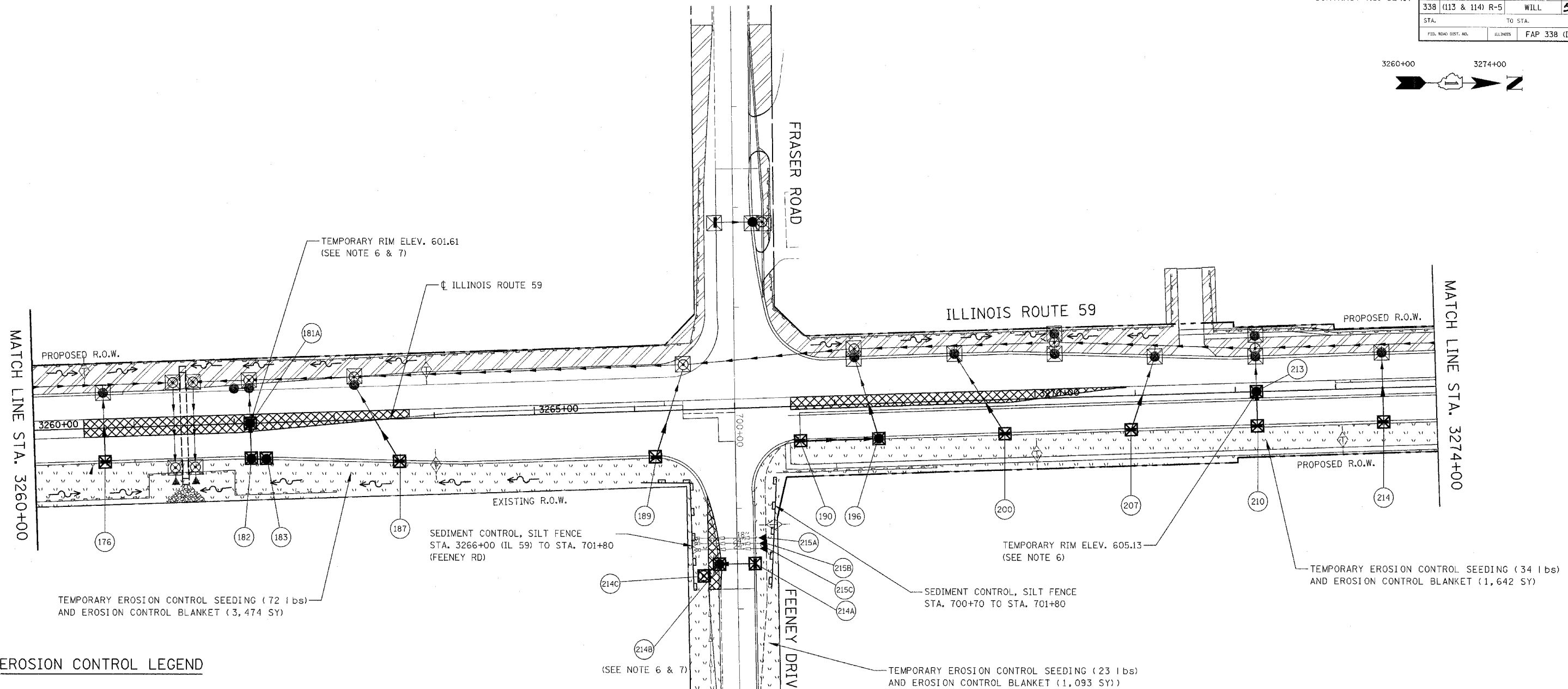
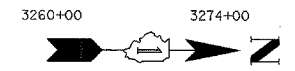
1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 2 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 2, 2A, 2B, 2C & 2D
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY REW
 CHECKED BY JCM

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	346
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



EROSION CONTROL LEGEND

	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	DRAINAGE STRUCTURE TO BE CLEANED		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	INLET FILTER		TEMPORARY PIPE CULVERT
	INLET FILTER INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE

NOTES

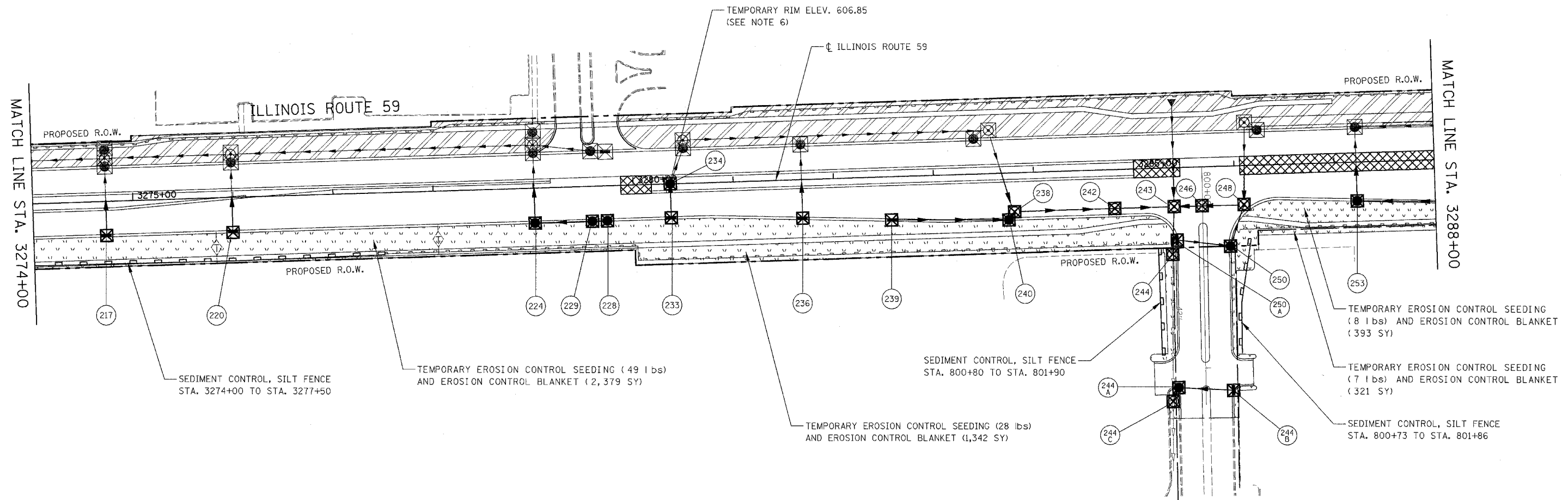
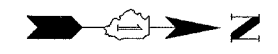
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2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 2 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. ANY REQUIRED ADJUSTMENT AND/OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPERATELY. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.
7. PROPOSED STRUCTURE SHALL BE BURIED AND TEMPORARILY COVERED WITH STEEL PLATE OF 0.25 INCH MINIMUM THICKNESS OR OF THICKNESS SPECIFIED AND APPROVED BY THE ENGINEER. PAYMENT FOR THE INSTALLATION AND REMOVAL OF THE STEEL PLATE INCLUDING ANY REQUIRED EXCAVATION SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 2, 2A, 2B, 2C & 2D
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY REW
 CHECKED BY JCM

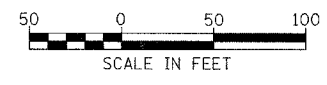
FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338 (113 & 114) R-5	WILL	525	347	
STA. 3274+00		TO STA. 3288+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	DRAINAGE STRUCTURE TO BE CLEANED		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	INLET FILTER		TEMPORARY PIPE CULVERT
	INLET FILTER INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE

NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 2 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. ANY REQUIRED ADJUSTMENT AND/OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPARATELY. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.

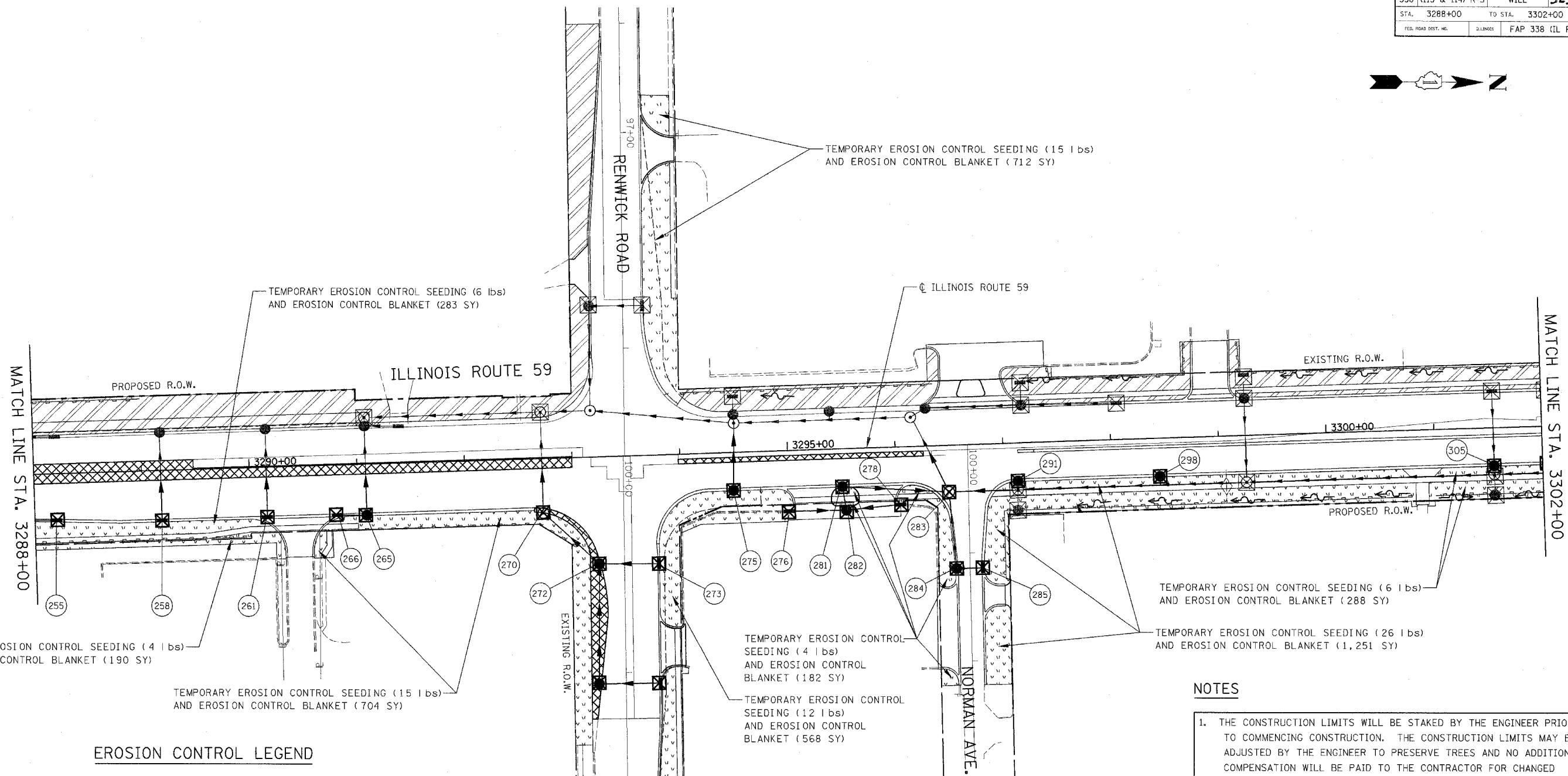


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
EROSION CONTROL PLAN
 STAGES 2, 2A, 2B, 2C & 2D
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY REW
 CHECKED BY JCM

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	348
STA.	3288+00	TO STA.	3302+00	
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



EROSION CONTROL LEGEND

	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	DRAINAGE STRUCTURE TO BE CLEANED		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	INLET FILTER		TEMPORARY PIPE CULVERT
	INLET FILTER INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE

NOTES

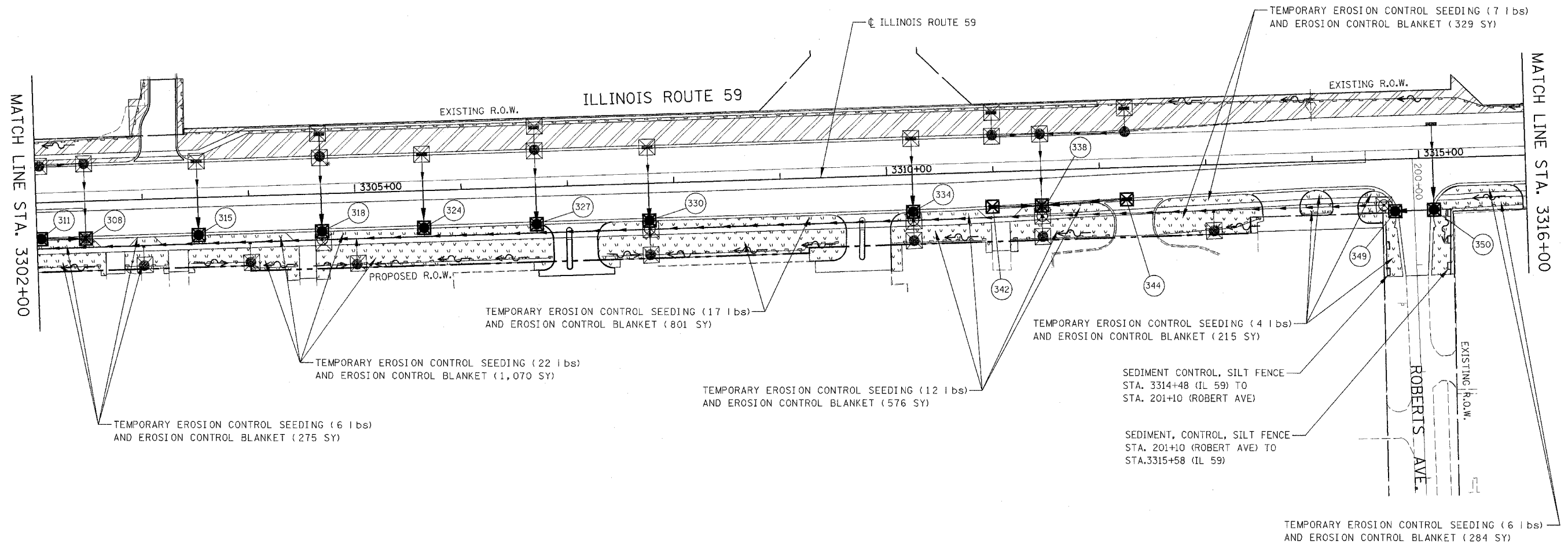
1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 2 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 2, 2A, 2B, 2C & 2D
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY REW
 CHECKED BY JCM

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338 (113 & 114) R-5		WILL	525	349
STA. 3302+00		TO STA. 3316+00		
FED. ROAD DIST. NO.	PLANS	FAP 338 (IL RTE. 59)		

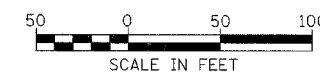


EROSION CONTROL LEGEND

	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	DRAINAGE STRUCTURE TO BE CLEANED		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	INLET FILTER		TEMPORARY PIPE CULVERT
	INLET FILTER INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE

NOTES

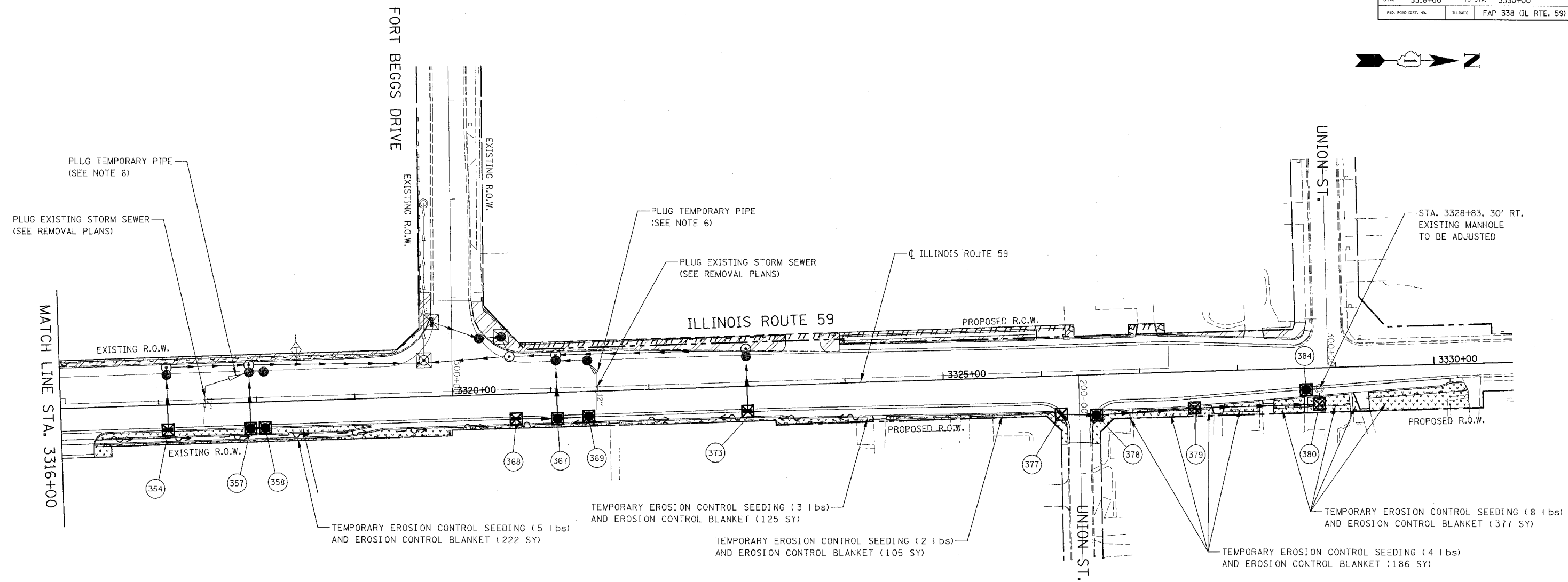
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3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 2 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION..



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 2, 2A, 2B, 2C & 2D
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY REW
 CHECKED BY JCM

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	350
STA. 3316+00		TO STA. 3330+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		

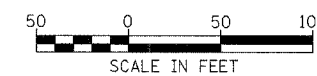


EROSION CONTROL LEGEND

	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
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	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	INLET FILTER		TEMPORARY PIPE CULVERT
	INLET FILTER INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE

NOTES

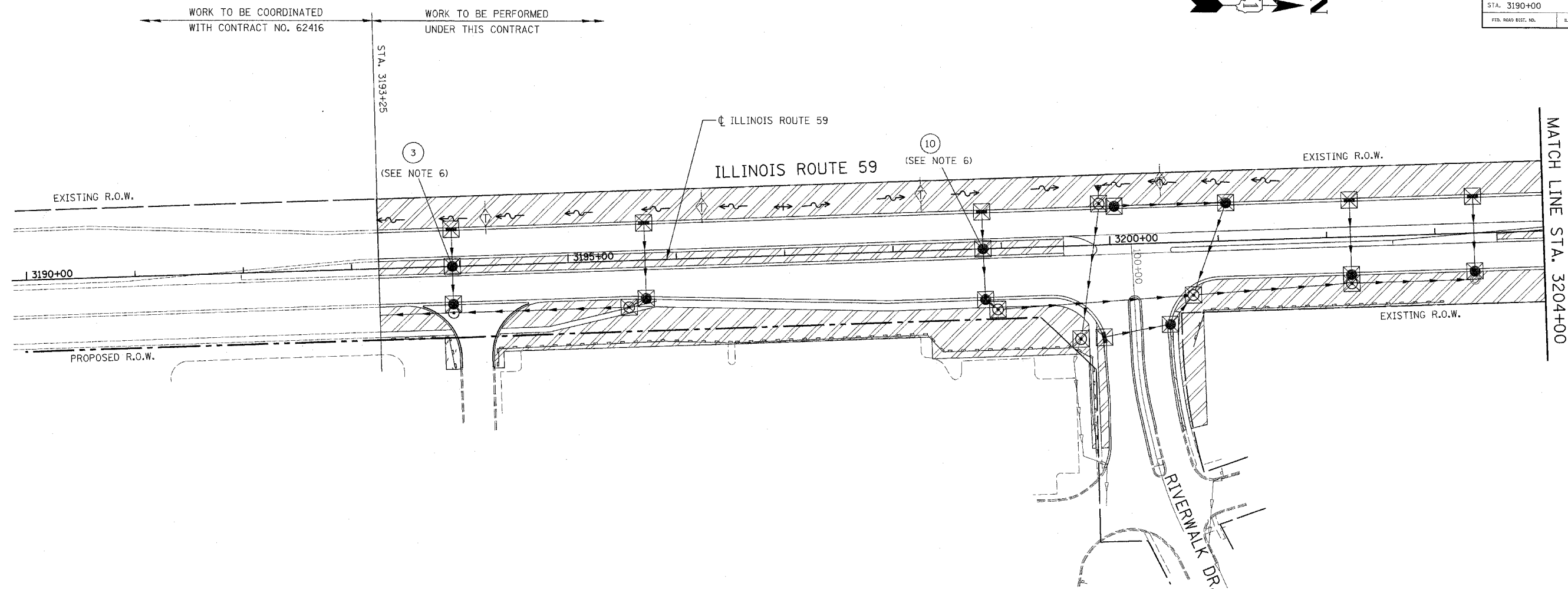
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3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 2 CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. THE COST OF PLUGGING TEMPORARY PIPE SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR PIPE CULVERT (TEMPORARY) OF THE CLASS, TYPE AND SIZE SPECIFIED.



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 2, 2A, 2B, 2C & 2D
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY REW
 CHECKED BY JCM

FAP #/E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	351
STA. 3190+00		TO STA. 3204+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



EROSION CONTROL LEGEND

	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	DRAINAGE STRUCTURE TO BE CLEANED		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	INLET FILTER		TEMPORARY PIPE CULVERT
	INLET FILTER INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE

GENERAL EROSION CONTROL NOTES:

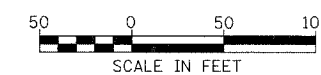
TREE TRUNK PROTECTION, TREE ROOT PRUNING, AND TREE PRUNING WILL BE REQUIRED FOR THIS PROJECT. A REPRESENTATIVE FROM IDOT'S ROADSIDE DEVELOPMENT UNIT WILL DETERMINE LOCATIONS ON SITE AT THE START OF CONSTRUCTION. PAY ITEMS HAVE BEEN INCLUDED IN THE SUMMARY OF QUANTITIES.

THE CONTRACTOR WILL BE REQUIRED TO SUBMIT CERTIFICATION STATEMENT (NPDES ATTACHMENT 6). FULL COMPLIANCE WITH ALL TERMS OF THE NPDES PERMIT MUST BE STRICTLY ADHERED TO.

DRAINAGE STRUCTURE INLET FILTERS ARE TO BE PROVIDED AND MAINTAINED FOR ALL EXISTING AND PROPOSED DRAINAGE STRUCTURE WITHIN THE PROJECT LIMITS AND ALL STRUCTURES NOTED FOR INLET AND PIPE PROTECTION AS DIRECTED BY THE ENGINEER. PAY ITEMS, DETAILS AND SPECIAL PROVISIONS HAVE BEEN INCLUDED IN THE SUMMARY OF QUANTITIES, SPECIAL PROVISIONS, AND PLAN DETAILS FOR THIS WORK.

NOTES

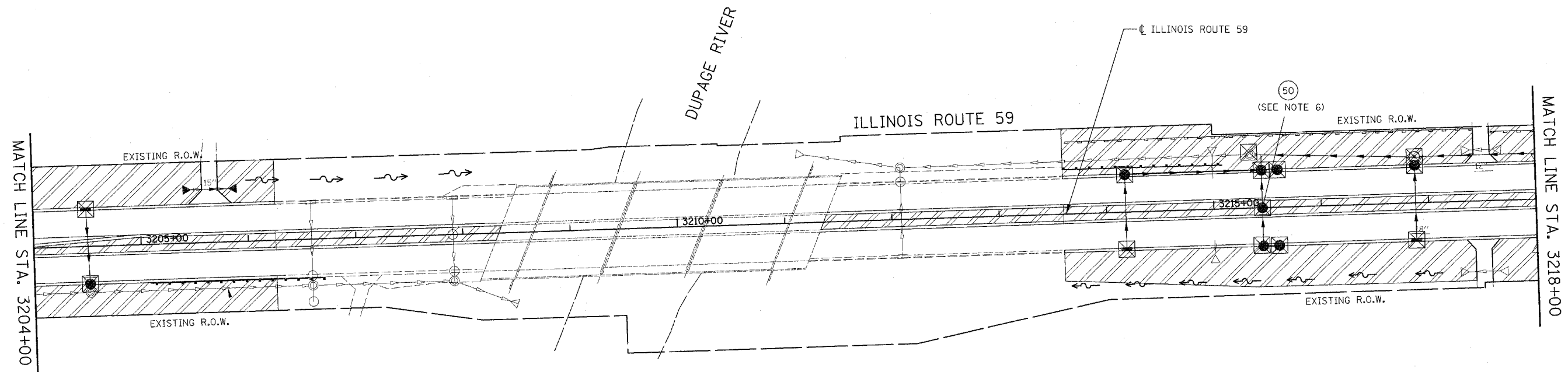
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2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 3 OF THE FINAL CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. ANY REQUIRED ADJUSTMENT AND/OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPARATELY. THE COST FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 3, 3A, AND 3B
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY REW
 CHECKED BY JCM

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338 (113 & 114) R-5	WILL	525	352	
STA. 3204+00		TO STA. 3218+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



EROSION CONTROL LEGEND

	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	DRAINAGE STRUCTURE TO BE CLEANED		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	INLET FILTER		TEMPORARY PIPE CULVERT
	INLET FILTER INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE

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2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.

3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.

4. SEE STAGE 3 OF THE FINAL CROSS SECTIONS FOR GRADING INFORMATION.

5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.

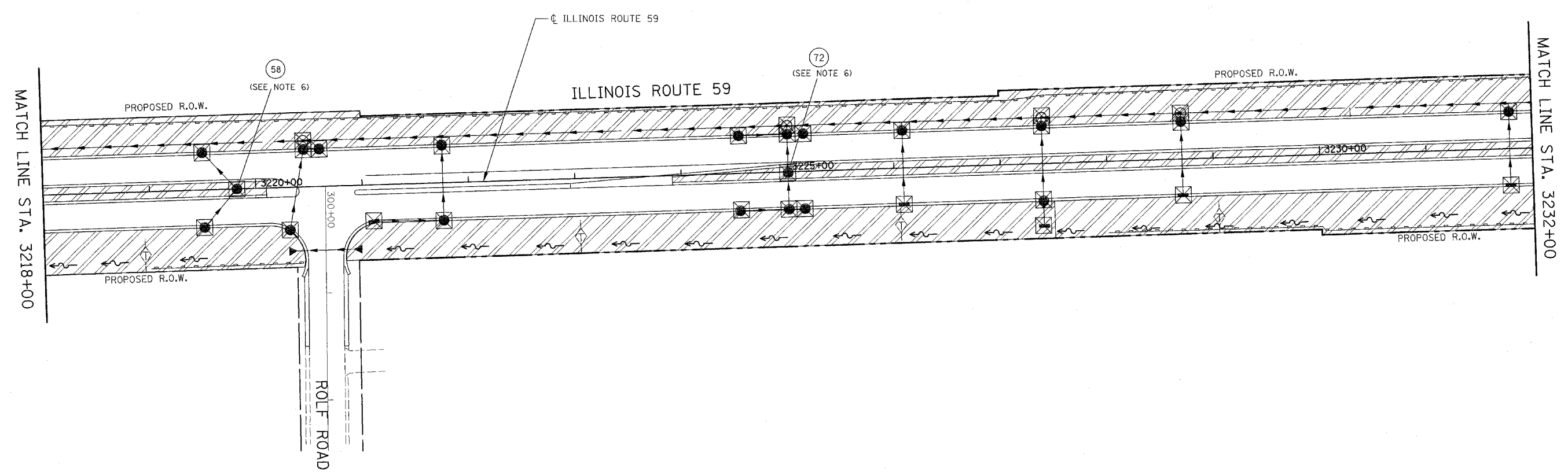
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 3, 3A AND 3B
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY REW
 CHECKED BY JCM

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	353
STA. 3218+00		TO STA. 3232+00		
ILLINOIS		FAP 338 (IL RTE. 59)		

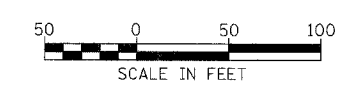


EROSION CONTROL LEGEND

- | | | | |
|--|---|--|--|
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET | | TEMPORARY DITCH CHECK |
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE | | TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE |
| | PERMANENT LANDSCAPING (SEE NOTE 3) | | FLOW DIRECTION (SEE NOTE 4) |
| | DRAINAGE STRUCTURE TO BE CLEANED | | PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5) |
| | SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2) | | PROPOSED STORM SEWER (SEE NOTE 5) |
| | SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE | | PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE |
| | INLET FILTER | | TEMPORARY PIPE CULVERT |
| | INLET FILTER INSTALLED IN PREVIOUS STAGE | | TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE |

NOTES

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- SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
- ANY REQUIRED ADJUSTMENT AND/OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPARATELY. THE COST FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.



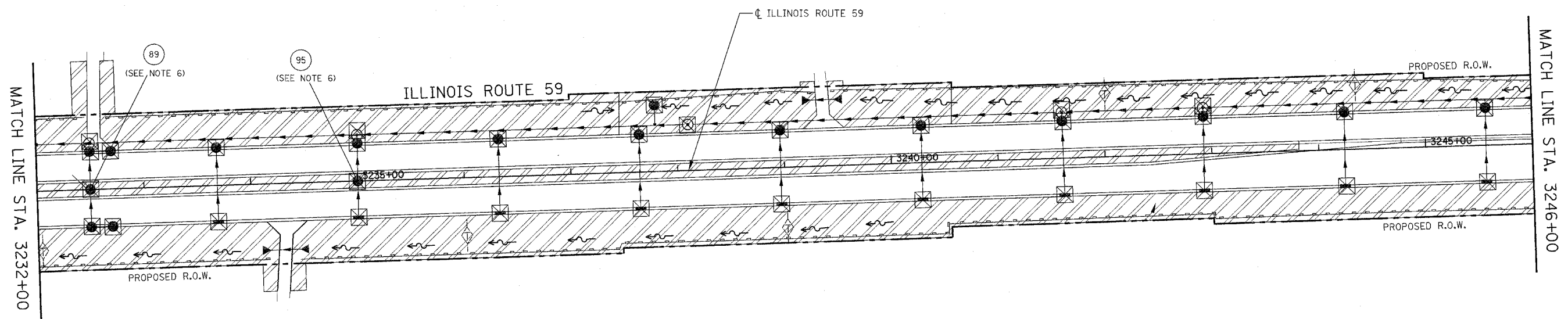
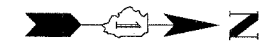
ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 3, 3A AND 3B
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY REW
 CHECKED BY JCM

CONTRACT NO. 62417

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	354
STA. 3232+00		TO STA. 3246+00		
FED. ROAD DIST. NO.		ILLINOIS	FAP 338 (IL RTE. 59)	

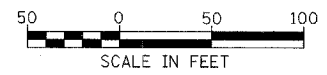


EROSION CONTROL LEGEND

- | | | | |
|--|---|--|--|
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET | | TEMPORARY DITCH CHECK |
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE | | TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE |
| | PERMANENT LANDSCAPING (SEE NOTE 3) | | FLOW DIRECTION (SEE NOTE 4) |
| | PERMANENT SEEDING INSTALLED IN PREVIOUS STAGE | | PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5) |
| | DRAINAGE STRUCTURE TO BE CLEANED | | PROPOSED STORM SEWER (SEE NOTE 5) |
| | SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2) | | PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE |
| | SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE | | TEMPORARY PIPE CULVERT |
| | INLET FILTER | | TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE |
| | INLET FILTER INSTALLED IN PREVIOUS STAGE | | |

NOTES

- THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
- SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
- SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
- SEE STAGE 3 OF THE FINAL CROSS SECTIONS FOR GRADING INFORMATION.
- SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
- ANY REQUIRED ADJUSTMENT AND/OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPARATELY. THE COST FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.

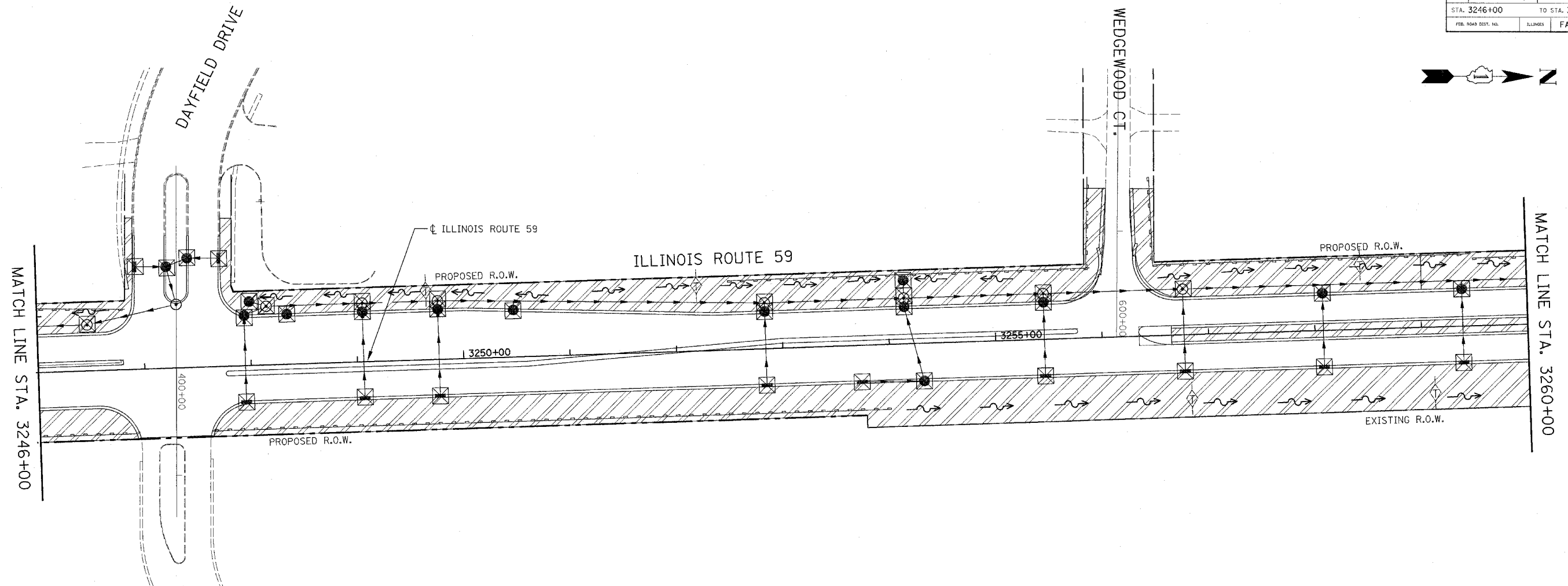
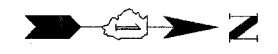


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 3, 3A AND 3B
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY REW
 CHECKED BY JCM

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	355
STA. 3246+00		TO STA. 3260+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		

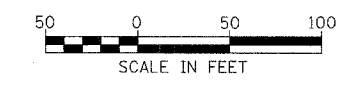


EROSION CONTROL LEGEND

- | | | | |
|--|---|--|--|
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET | | TEMPORARY DITCH CHECK |
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE | | TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE |
| | PERMANENT LANDSCAPING (SEE NOTE 3) | | FLOW DIRECTION (SEE NOTE 4) |
| | DRAINAGE STRUCTURE TO BE CLEANED | | PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5) |
| | SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2) | | PROPOSED STORM SEWER (SEE NOTE 5) |
| | SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE | | PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE |
| | INLET FILTER | | TEMPORARY PIPE CULVERT |
| | INLET FILTER INSTALLED IN PREVIOUS STAGE | | TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE |

NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 3 OF THE FINAL CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.



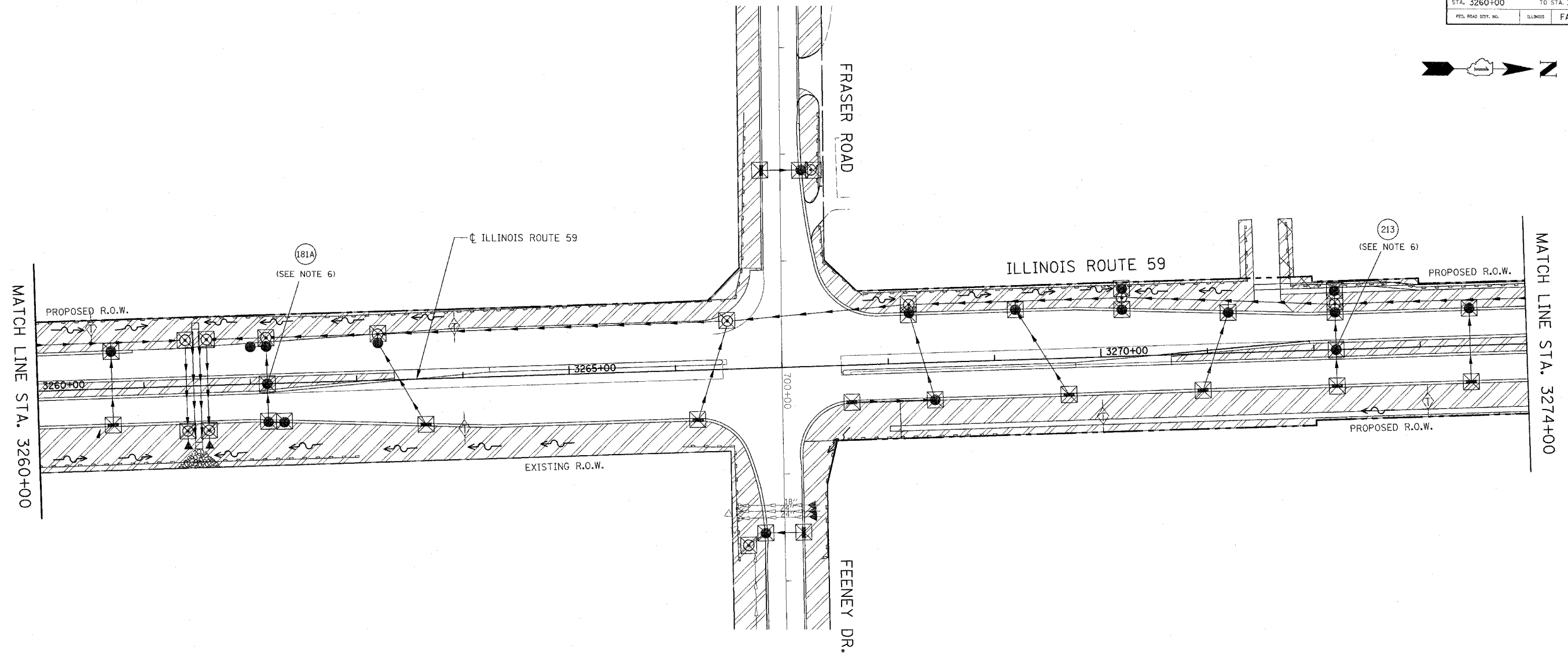
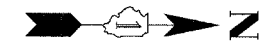
ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 3, 3A AND 3B
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY REW
 CHECKED BY JCM

CONTRACT NO. 62417

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	356
STA. 3260+00		TO STA. 3274+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		

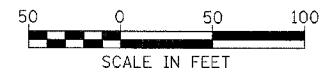


EROSION CONTROL LEGEND

- | | | | |
|--|---|--|--|
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET | | TEMPORARY DITCH CHECK |
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE | | TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE |
| | PERMANENT LANDSCAPING (SEE NOTE 3) | | FLOW DIRECTION (SEE NOTE 4) |
| | DRAINAGE STRUCTURE TO BE CLEANED | | PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5) |
| | SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2) | | PROPOSED STORM SEWER (SEE NOTE 5) |
| | SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE | | PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE |
| | INLET FILTER | | TEMPORARY PIPE CULVERT |
| | INLET FILTER INSTALLED IN PREVIOUS STAGE | | TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE |

NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 3 OF THE FINAL CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
6. ANY REQUIRED ADJUSTMENT AND/OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPARATELY. THE COST FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.

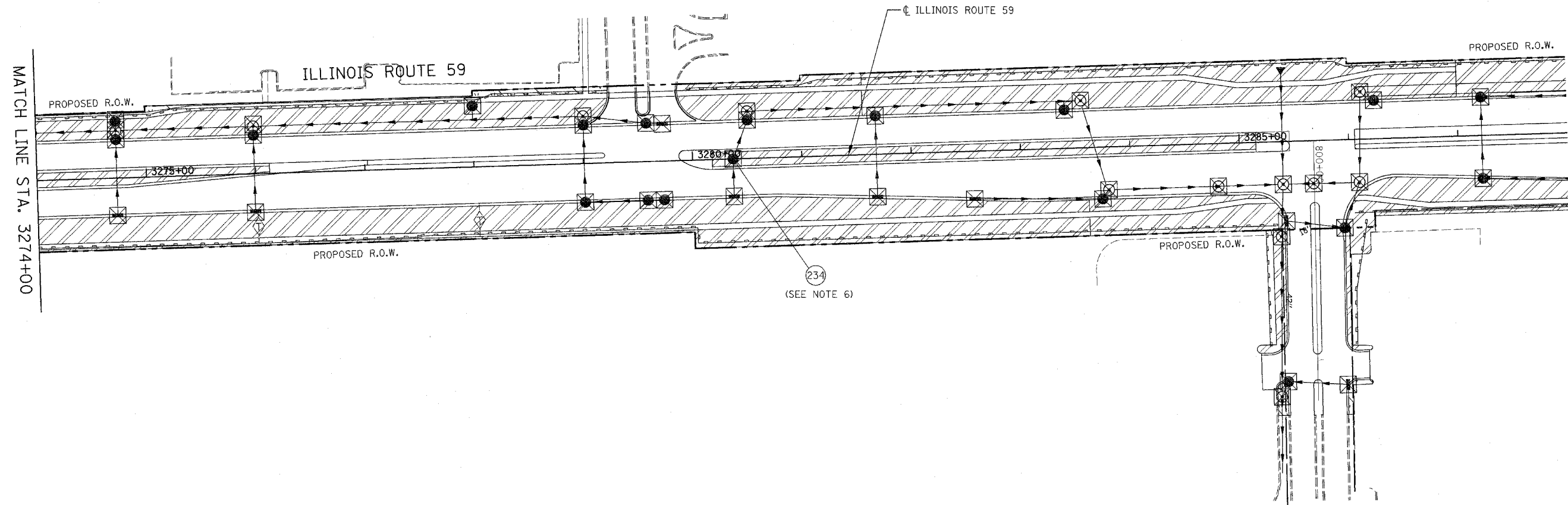
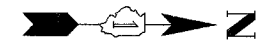


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 3, 3A AND 3B
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY REW
 CHECKED BY JCM

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	357
STA. 3274+00		TO STA. 3288+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



EROSION CONTROL LEGEND

	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	DRAINAGE STRUCTURE TO BE CLEANED		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	INLET FILTER		TEMPORARY PIPE CULVERT
	INLET FILTER INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE

NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.

2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.

3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.

4. SEE STAGE 3 OF THE FINAL CROSS SECTIONS FOR GRADING INFORMATION.

5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.

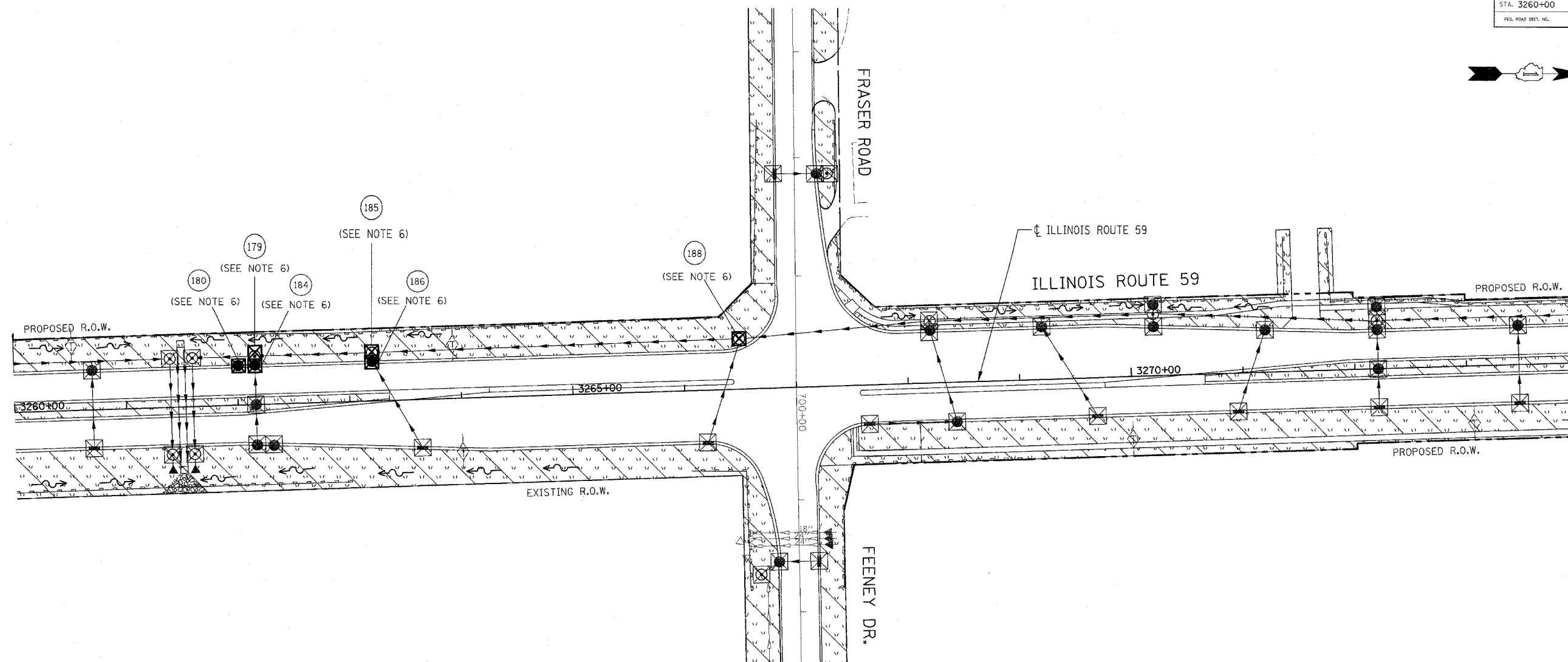
6. ANY REQUIRED ADJUSTMENT AND/OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPARATELY. THE COST FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 3, 3A AND 3B
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY REW
 CHECKED BY JCM

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	358
STA. 3260+00		TO STA. 3274+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



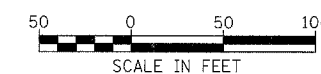
EROSION CONTROL LEGEND

- | | | | |
|--|---|--|--|
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET | | TEMPORARY DITCH CHECK |
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE | | TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE |
| | PERMANENT LANDSCAPING (SEE NOTE 3) | | FLOW DIRECTION (SEE NOTE 4) |
| | PERMANENT LANDSCAPING INSTALLED IN PREVIOUS STAGE | | PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5) |
| | DRAINAGE STRUCTURE TO BE CLEANED | | PROPOSED STORM SEWER (SEE NOTE 5) |
| | SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2) | | PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE |
| | SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE | | TEMPORARY PIPE CULVERT |
| | INLET FILTER | | TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE |
| | INLET FILTER INSTALLED IN PREVIOUS STAGE | | |

NOTES

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3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS.
4. SEE STAGE 4 OF THE FINAL CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.

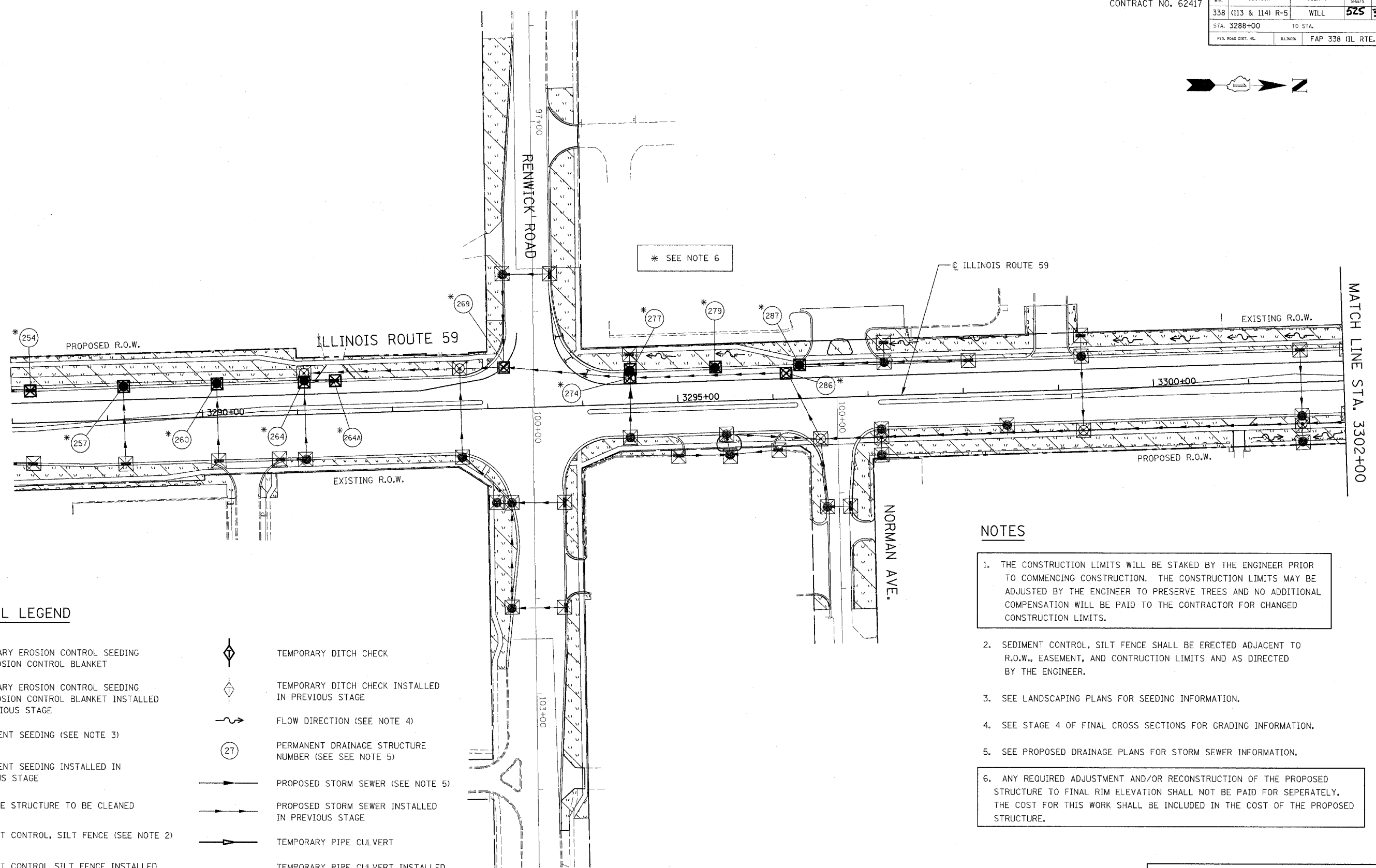
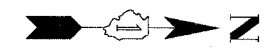
6. ANY REQUIRED ADJUSTMENT AND/OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPERATELY. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 4 & 4A
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008
 DRAWN BY REW
 CHECKED BY JCM

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	359
STATIONING		TO STA.		
STATION 3288+00		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		

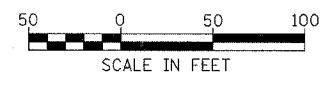


EROSION CONTROL LEGEND

- TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET
- TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE
- PERMANENT SEEDING (SEE NOTE 3)
- PERMANENT SEEDING INSTALLED IN PREVIOUS STAGE
- DRAINAGE STRUCTURE TO BE CLEANED
- SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)
- SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE
- INLET FILTER
- INLET FILTER INSTALLED IN PREVIOUS STAGE
- TEMPORARY DITCH CHECK
- TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
- FLOW DIRECTION (SEE NOTE 4)
- PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
- PROPOSED STORM SEWER (SEE NOTE 5)
- PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
- TEMPORARY PIPE CULVERT
- TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE

NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT, AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.
3. SEE LANDSCAPING PLANS FOR SEEDING INFORMATION.
4. SEE STAGE 4 OF FINAL CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
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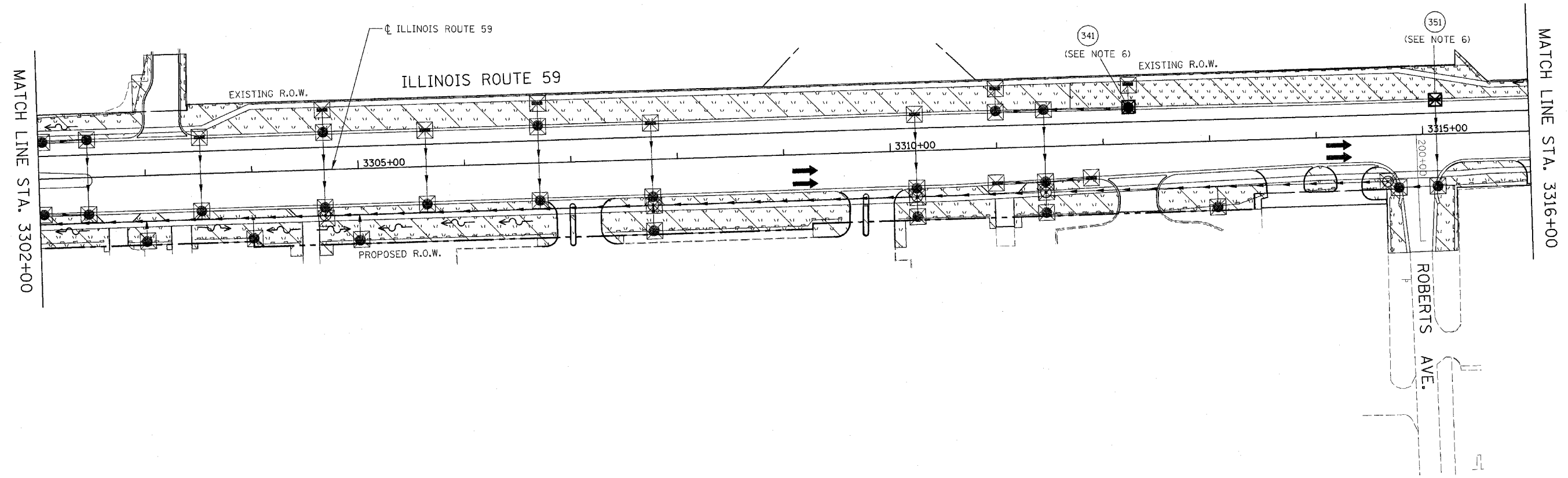
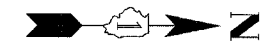
ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 4 & 4A
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY BSB
 CHECKED BY HME

MATCH LINE STA. 3302+00

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	360
STA. 3302+00	TO STA. 3316+00			
FEB. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



EROSION CONTROL LEGEND

	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET		TEMPORARY DITCH CHECK
	TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE		TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE
	PERMANENT LANDSCAPING (SEE NOTE 3)		FLOW DIRECTION (SEE NOTE 4)
	PERMANENT LANDSCAPING INSTALLED IN PREVIOUS STAGE		PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5)
	DRAINAGE STRUCTURE TO BE CLEANED		PROPOSED STORM SEWER (SEE NOTE 5)
	SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2)		PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE
	SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE		TEMPORARY PIPE CULVERT
	INLET FILTER		TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE
	INLET FILTER INSTALLED IN PREVIOUS STAGE		

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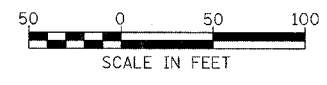
2. SEDIMENT CONTROL, SILT FENCE SHALL BE ERECTED ADJACENT TO R.O.W., EASEMENT AND CONSTRUCTION LIMITS AND AS DIRECTED BY THE ENGINEER.

3. SEE LANDSCAPING PLANS FOR SEEDING INFORMATION.

4. SEE STAGE 4 OF FINAL CROSS SECTIONS FOR GRADING INFORMATION.

5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.

6. ANY REQUIRED ADJUSTMENT AND/OR RECONSTRUCTION OF THE PROPOSED STRUCTURE TO FINAL RIM ELEVATION SHALL NOT BE PAID FOR SEPERATELY. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.

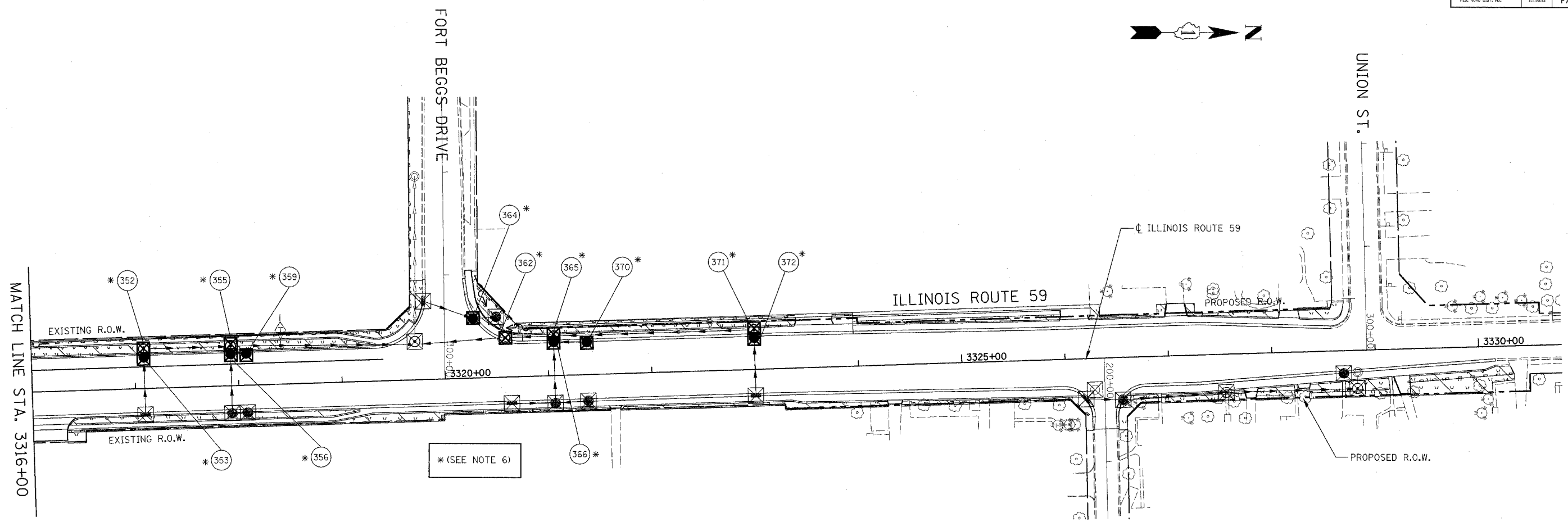


ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 4, & 4A
 ILLINOIS ROUTE 59

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY BSB
 CHECKED BY JCM

FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338 (113 & 114) R-5	WILL	525	361	
STA. 3316+00		TO STA. 3330+00		
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		

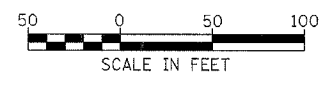


EROSION CONTROL LEGEND

- | | | | |
|--|---|--|--|
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET | | TEMPORARY DITCH CHECK |
| | TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET INSTALLED IN PREVIOUS STAGE | | TEMPORARY DITCH CHECK INSTALLED IN PREVIOUS STAGE |
| | PERMANENT LANDSCAPING (SEE NOTE 3) | | FLOW DIRECTION (SEE NOTE 4) |
| | PERMANENT LANDSCAPING INSTALLED IN PREVIOUS STAGE | | PERMANENT DRAINAGE STRUCTURE NUMBER (SEE NOTE 5) |
| | DRAINAGE STRUCTURE TO BE CLEANED | | PROPOSED STORM SEWER (SEE NOTE 5) |
| | SEDIMENT CONTROL, SILT FENCE (SEE NOTE 2) | | PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE |
| | SEDIMENT CONTROL SILT FENCE INSTALLED IN PREVIOUS STAGE | | TEMPORARY PIPE CULVERT |
| | INLET FILTER | | TEMPORARY PIPE CULVERT INSTALLED IN PREVIOUS STAGE |
| | INLET FILTER INSTALLED IN PREVIOUS STAGE | | |

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4. SEE STAGE 4 OF THE FINAL CROSS SECTIONS FOR GRADING INFORMATION.
5. SEE PROPOSED DRAINAGE PLANS FOR STORM SEWER INFORMATION.
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG
 EROSION CONTROL PLAN
 STAGES 4, & 4A
 ILLINOIS ROUTE 59

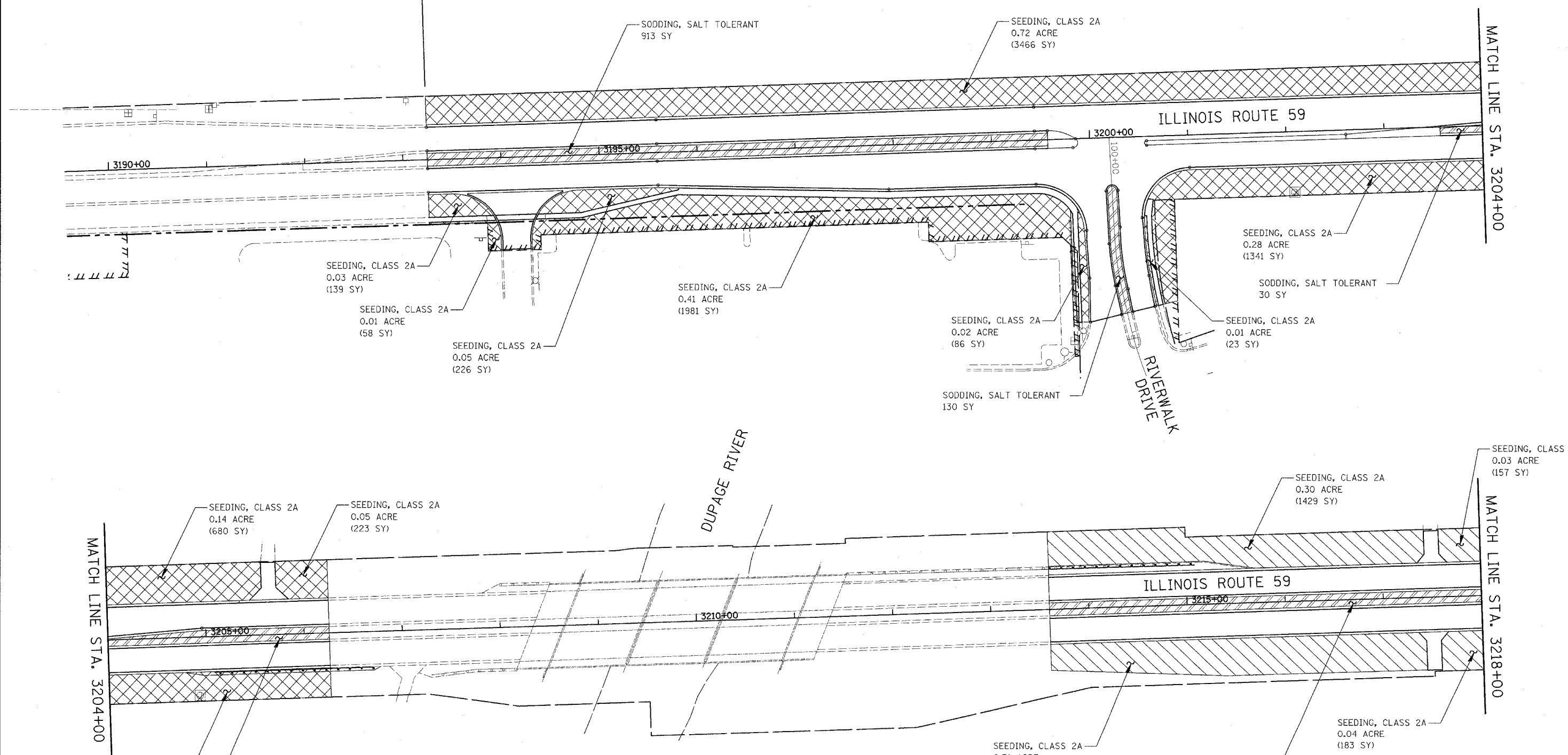
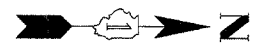
SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY BSB
 CHECKED BY HME

CONTRACT NO. 62417

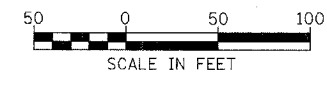
FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	362
STA. 3190+00		TO STA. 3218+00		
FED. ROAD DIST. NO.		ILLINOIS	FAP 338 (IL RTE. 59)	

← WORK TO BE COORDINATED WITH CONTRACT NO. 62416
 STA. 3193+25
 WORK TO BE PERFORMED UNDER THIS CONTRACT →



LEGEND

- SEEDING, CLASS 2A AND TOPSOIL FURNISH AND PLACE, 8" EROSION CONTROL BLANKET
- SEEDING, CLASS 2A AND TOPSOIL FURNISH AND PLACE, 4" EROSION CONTROL BLANKET
- SODDING, SALT TOLERANT AND TOPSOIL FURNISH AND PLACE, 8"
- SODDING, SALT TOLERANT AND TOPSOIL FURNISH AND PLACE, 8"



ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO. 1 SCHAUMBURG

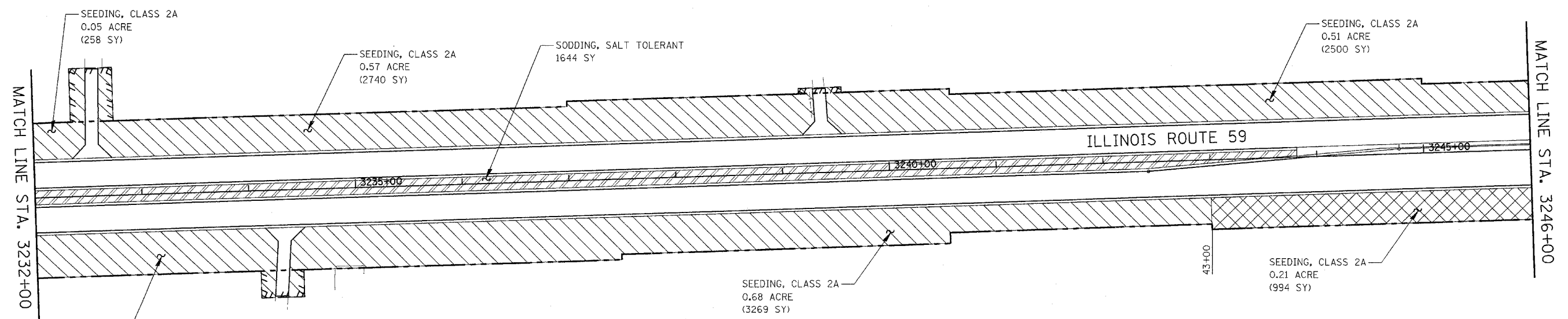
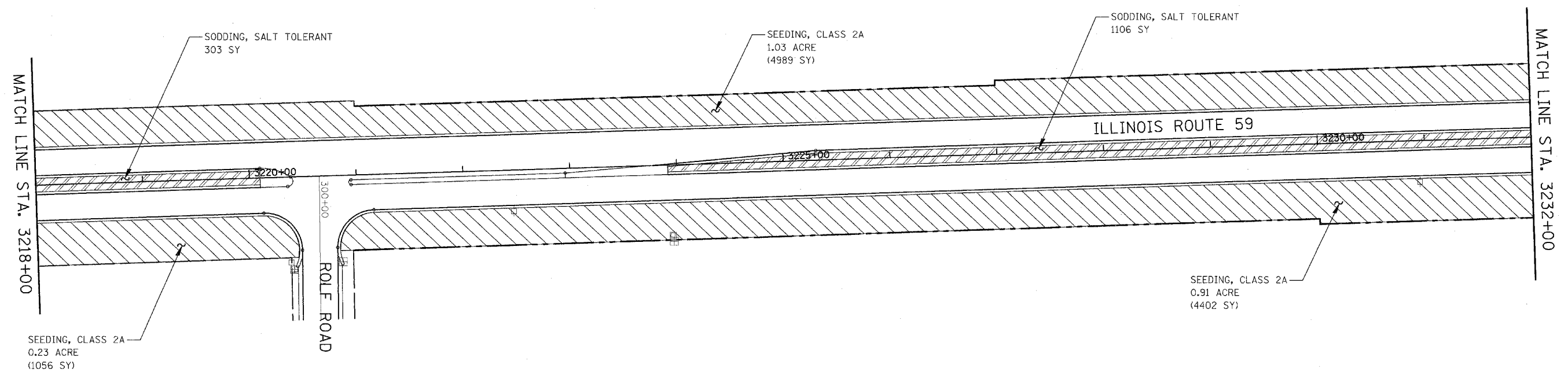
LANDSCAPE PLANS

SCALE AS SHOWN
 DATE MARCH 18, 2008

DRAWN BY JRS
 CHECKED BY JCM/HME

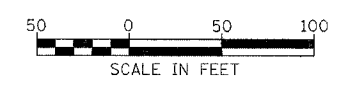
CONTRACT NO. 62417

PROJECT NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338 (113 & 114) R-5	WILL		525	363
STA. 3218+00	TO STA. 3246+00			
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



LEGEND

- SODDING, SALT TOLERANT AND TOPSOIL FURNISH AND PLACE, 12"
- SEEDING, CLASS 2A AND TOPSOIL FURNISH AND PLACE, 8" EROSION CONTROL BLANKET
- SODDING, SALT TOLERANT AND TOPSOIL FURNISH AND PLACE, 8"
- SEEDING, CLASS 2A AND TOPSOIL FURNISH AND PLACE, 4" EROSION CONTROL BLANKET



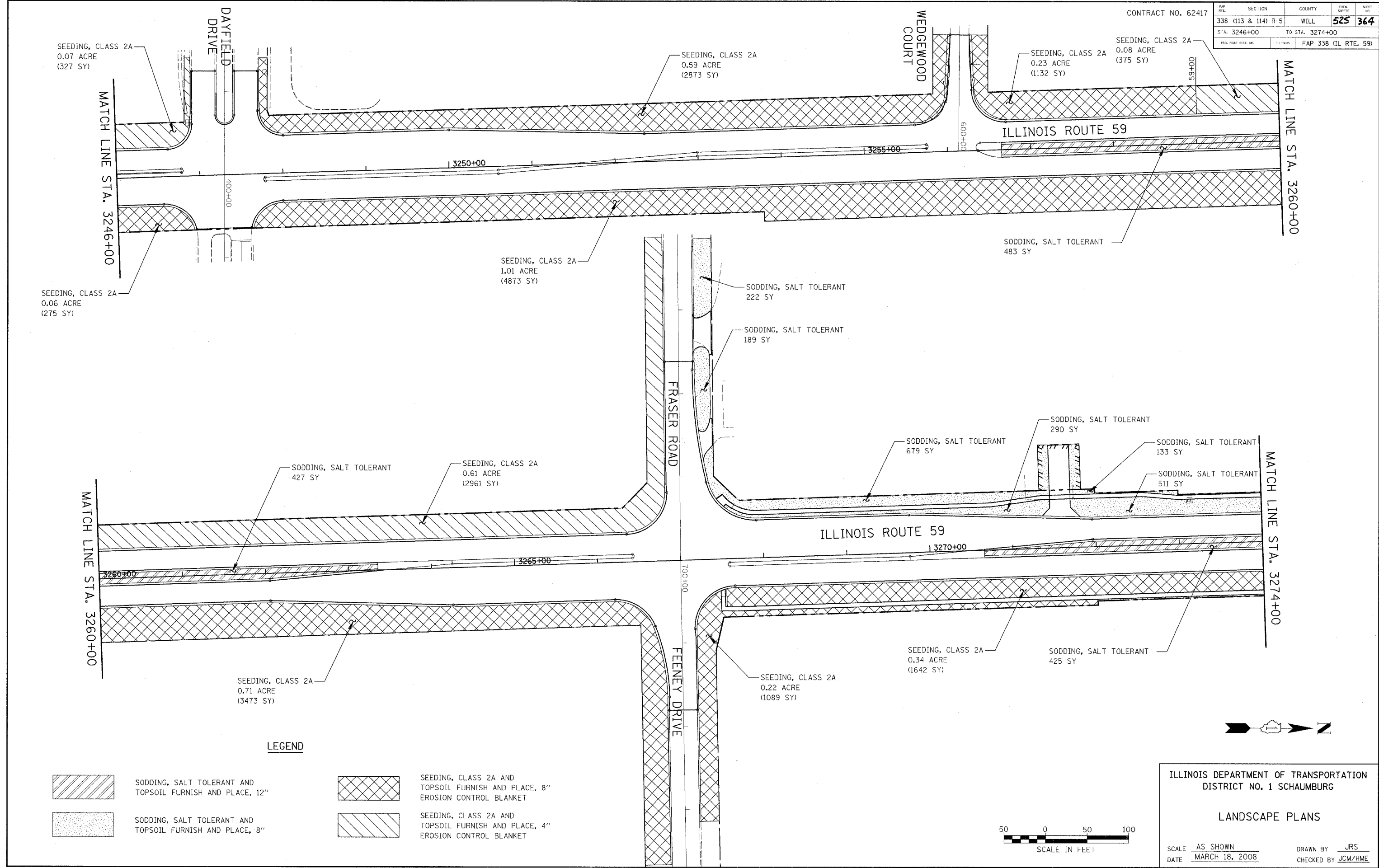
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG

LANDSCAPE PLANS

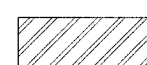


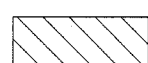
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DATE MARCH 18, 2008

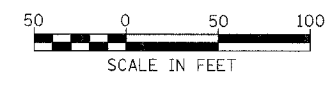
DRAWN BY JRS
CHECKED BY JCM/HME

CONTRACT NO. 62417		FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338 (113 & 114) R-5		338	WILL	525	364	
STA. 3246+00		TO STA. 3274+00				
FED. ROAD DIST. NO.		ILLINOIS		FAP 338 (IL RTE. 59)		



LEGEND

- | | | | |
|---|---|---|---|
|  | SODDING, SALT TOLERANT AND TOPSOIL FURNISH AND PLACE, 12" |  | SEEDING, CLASS 2A AND TOPSOIL FURNISH AND PLACE, 8" EROSION CONTROL BLANKET |
|  | SODDING, SALT TOLERANT AND TOPSOIL FURNISH AND PLACE, 8" |  | SEEDING, CLASS 2A AND TOPSOIL FURNISH AND PLACE, 4" EROSION CONTROL BLANKET |



ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG

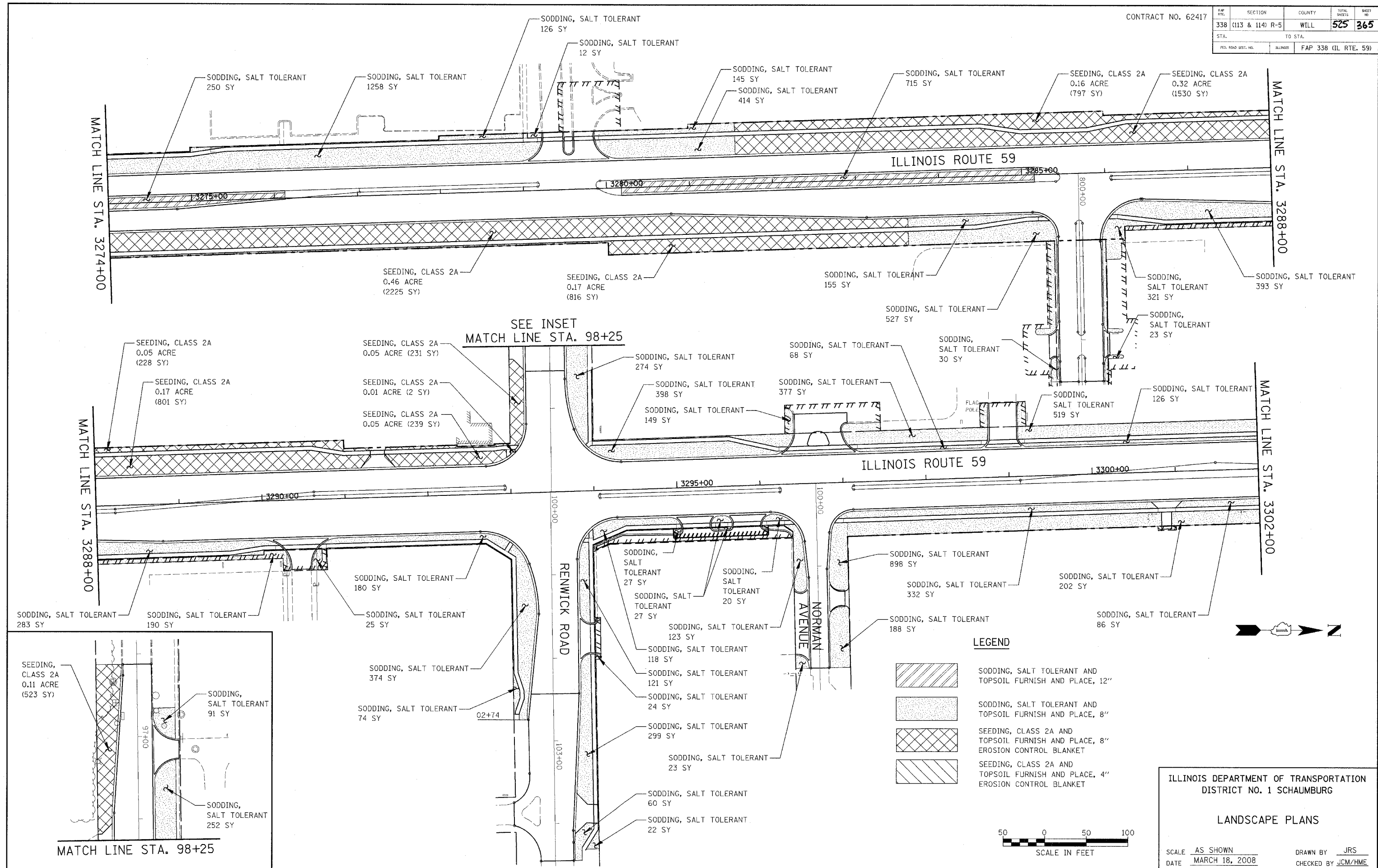
LANDSCAPE PLANS

SCALE AS SHOWN
DATE MARCH 18, 2008

DRAWN BY JRS
CHECKED BY JCM/HME

CONTRACT NO. 62417

FAP SITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	365
STA.	TO STA.			
FED. ROAD DIST. NO.	ALIGNMENT	FAP 338 (IL RTE. 59)		



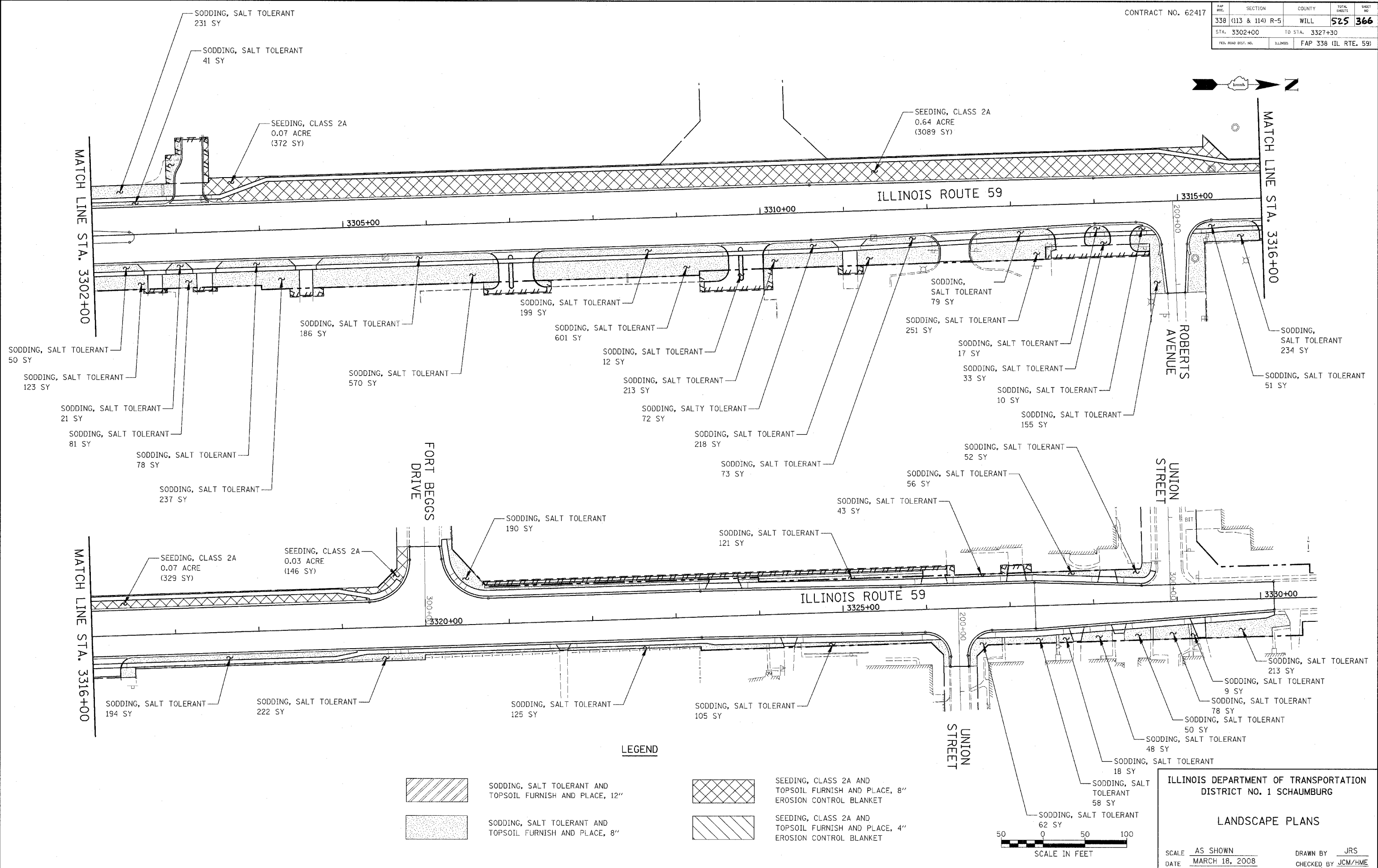
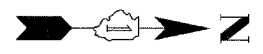
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG

LANDSCAPE PLANS


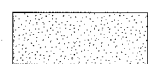
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DATE MARCH 18, 2008
DRAWN BY JRS
CHECKED BY JCM/HME


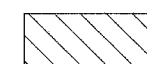
CONTRACT NO. 62417

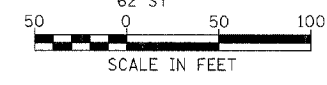
FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	366
STA. 3302+00	TO STA. 3327+30			
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



LEGEND

-  SODDING, SALT TOLERANT AND TOPSOIL FURNISH AND PLACE, 12"
-  SODDING, SALT TOLERANT AND TOPSOIL FURNISH AND PLACE, 8"

-  SEEDING, CLASS 2A AND TOPSOIL FURNISH AND PLACE, 8" EROSION CONTROL BLANKET
-  SEEDING, CLASS 2A AND TOPSOIL FURNISH AND PLACE, 4" EROSION CONTROL BLANKET



ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG

LANDSCAPE PLANS

SCALE AS SHOWN
DATE MARCH 18, 2008

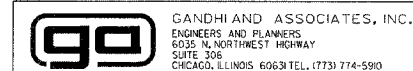
DRAWN BY JRS
CHECKED BY JCM/HME

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	113 & 114R-5	WILL	525	367
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT:		
CONTRACT # 62417				

SUMMARY OF QUANTITIES

DESIGNATION	UNIT	TOTAL QUANTITIES	ILL. RTE. 59 @ FRASER RD./FEENEY DR.	ILL. RTE. 59 @ RENWICK RD.	ILL. RTE. 59 @ FORT BEGGS DR.	TEMPORARY INTERCONNECT	PROPOSED INTERCONNECT
SIGN PANEL - TYPE 1	SQ FT	102	45	31.5	25.5	-	-
CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	6652	508	488	605	-	5051
CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	322	32	82	208	-	-
CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	38	38	-	-	-	-
CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	60	10	34	16	-	-
CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	799	110	49	78	-	562
CONDUIT PUSHED, 3" DIA., GALVANIZED STEEL	FOOT	146	146	-	-	-	-
CONDUIT PUSHED, 3 1/2" DIA., GALVANIZED STEEL	FOOT	68	68	-	-	-	-
CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	608	104	324	180	-	-
HANDHOLE	EACH	22	4	6	4	-	8
HEAVY-DUTY HANDHOLE	EACH	9	4	4	1	-	-
DOUBLE HANDHOLE	EACH	4	2	1	1	-	-
TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	7205	583	813	758	-	5051
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1	-	-	-	-	1
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	3	1	1	1	-	-
TRANSCIEVER - FIBER OPTIC	EACH	3	1	1	1	-	-
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1676.5	969	382	325.5	-	-
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	3104.5	1660	770	674.5	-	-
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2653	809	856	988	-	-
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	3942.5	1636	1671	635.5	-	-
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	7008	2524	2865	1619	-	-
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	296.5	26	42	228.5	-	-
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	6	-	3	3	-	-
TRAFFIC SIGNAL POST, GALVANIZED STEEL 18 FT.	EACH	1	-	-	1	-	-
STEEL MAST ARM ASSEMBLY AND POLE, 22 FT.	EACH	1	-	-	1	-	-
STEEL MAST ARM ASSEMBLY AND POLE, 30 FT.	EACH	4	2	2	-	-	-
STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.	EACH	1	-	-	1	-	-
STEEL MAST ARM ASSEMBLY AND POLE, 38 FT.	EACH	1	-	1	-	-	-
STEEL MAST ARM ASSEMBLY AND POLE, 52 FT.	EACH	1	-	1	-	-	-
STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 28 FT. AND 50 FT.	EACH	1	1	-	-	-	-
STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 38 FT. AND 44 FT	EACH	1	1	-	-	-	-
CONCRETE FOUNDATION, TYPE A	FOOT	28	-	12	16	-	-
CONCRETE FOUNDATION, TYPE C	FOOT	12	4	4	4	-	-
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	45	30	-	15	-	-
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	105	30	60	15	-	-
DRILL EXISTING HANDHOLE	EACH	3	-	-	-	-	3
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	11	4	4	3	-	-
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2	-	-	2	-	-
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	8	2	4	2	-	-
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	11	6	4	1	-	-
SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	1	-	-	1	-	-
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED	EACH	6	2	2	2	-	-
PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED	EACH	2	-	1	1	-	-
TRAFFIC SIGNAL BACKPLATE	EACH	12	-	8	4	-	-
TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	10	10	-	-	-	-
INDUCTIVE LOOP DETECTOR	EACH	29	10	12	7	-	-
* LIGHT DETECTOR	EACH	6	2	2	2	-	-
* LIGHT DETECTOR AMPLIFIER	EACH	3	1	1	1	-	-
PEDESTRIAN PUSH-BUTTON	EACH	8	2	3	3	-	-
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	2	-	1	1	-	-
MODIFY EXISTING CONTROLLER	EACH	1	-	-	-	-	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	2	-	1	1	-	-
REMOVE EXISTING HANDHOLE	EACH	19	-	10	5	4	-
REMOVE EXISTING CONCRETE FOUNDATION	EACH	12	-	9	3	-	-
ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1C	FOOT	7550	-	-	-	-	7550
PERFORMED DETECTOR LOOP	FOOT	2583	887	1239	457	-	-
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	2	-	1	1	-	-
SERVICE INSTALLATION - POLE MOUNTED	EACH	3	1	1	1	-	-
UNINTERRUPTIBLE POWER SUPPLY	EACH	3	1	1	1	-	-
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F	FOOT	7628	-	-	-	-	7628
* ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	2037.5	493	799	745.5	-	-
ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED	FOOT	873	290	313	270	-	-
TEMPORARY TRAFFIC SIGNAL INTERCONNECTION SYSTEM	L SUM	1	-	-	-	1	-
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM - LEVEL II	EACH	1	-	-	-	-	1

* 100% COST TO VILLAGE OF PLAINFIELD



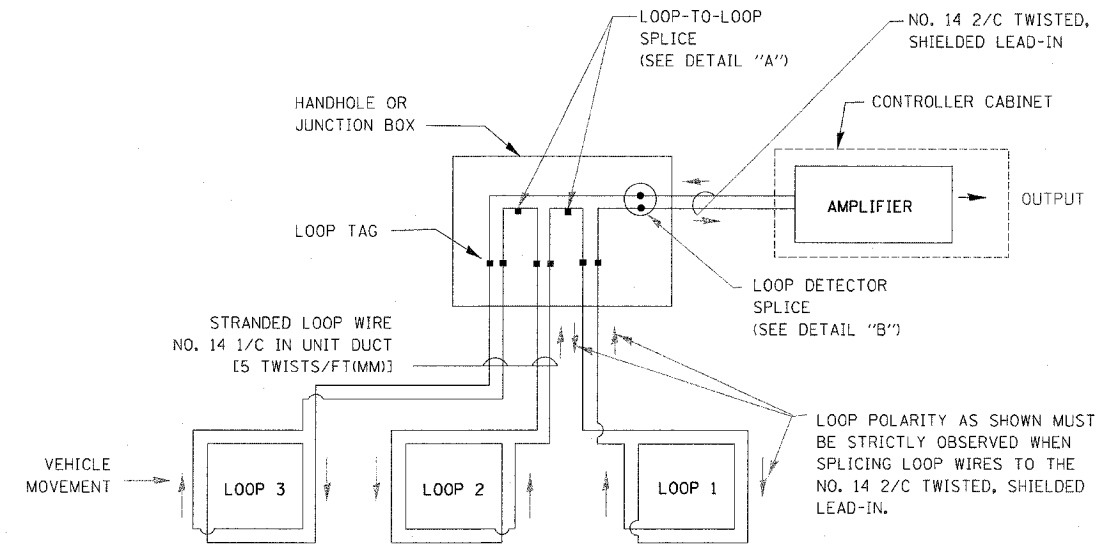
GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 N. WORTHWEST HIGHWAY
SUITE 306
CHICAGO, ILLINOIS 60631 TEL. (773) 774-5910

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
TRAFFIC SIGNAL INSTALLATION
ILLINOIS ROUTE 59
FROM FRASER RD./FEENEY DR. TO
US ROUTE 30
SCALE: N.T.S.
DATE: 03/18/2008
DRAWN BY: BB,KP
DESIGNED BY: PKG/RRM
CHECKED BY: PKG/RRM

LOOP DETECTOR NOTES

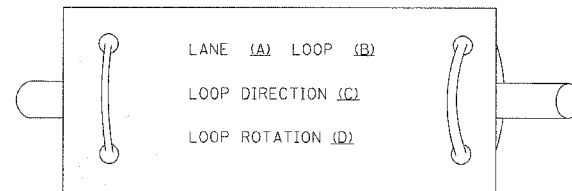
- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PERFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PERFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.



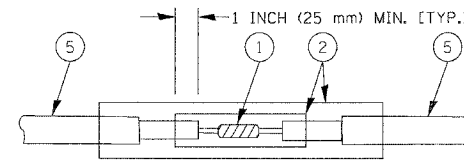
DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

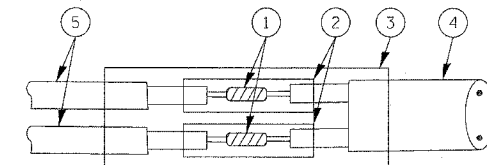
LOOP LEAD-IN CABLE TAG



- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

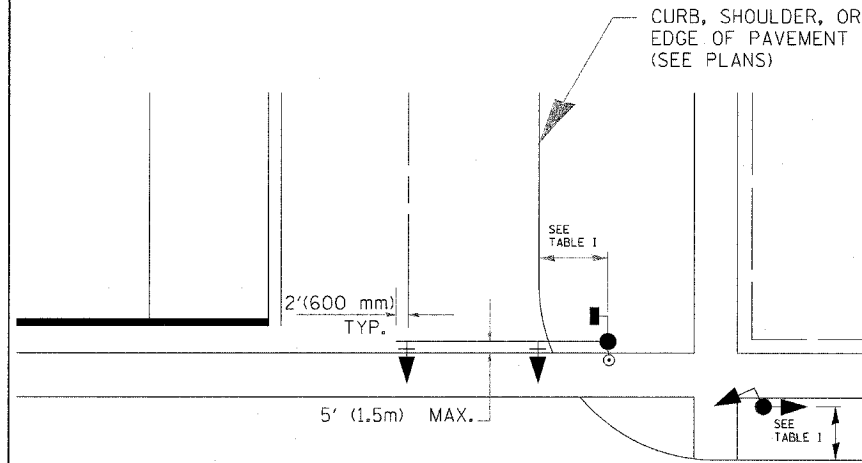
SCALE: VERT. NONE
HORIZ. NONE
DATE 1-01-02

DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 1 OF 4

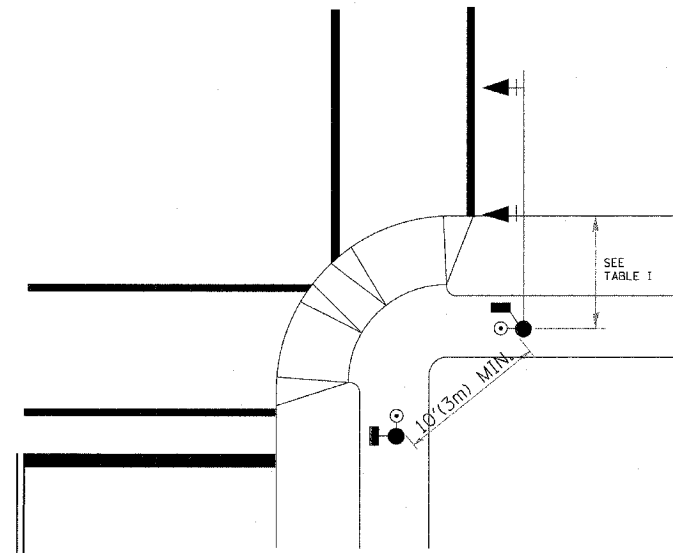
CONTRACT # 62417

TRAFFIC SIGNAL MAST ARM AND POST

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



PEDESTRIAN SIGNAL PUSHBUTTON



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

NOTES:

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.

AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.

PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:
 - A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.
 - B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.
 - C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
 - D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).
 - E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

PEDESTRIAN SIGNAL POST

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

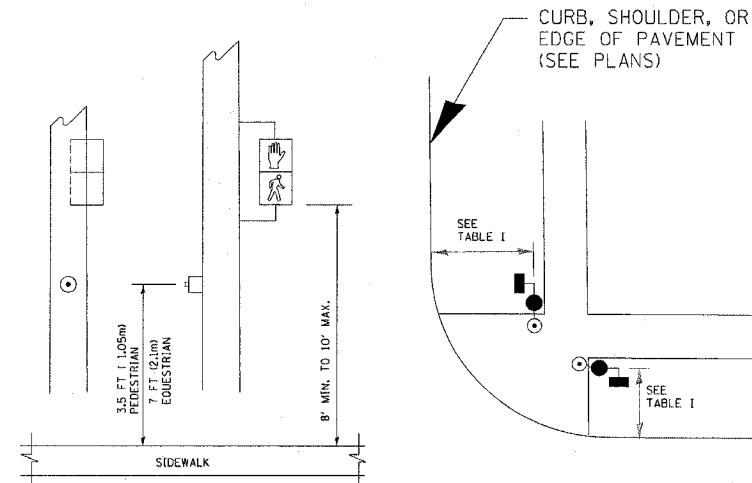


TABLE I

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB)	SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN PUSHBUTTON	SEE NOTE 1	SEE NOTE 1

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

SCALE: VERT. NONE
HORIZ. NONE
DATE 1-01-02

DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 2 OF 4

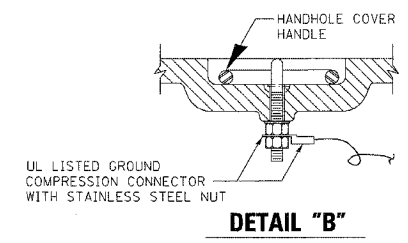
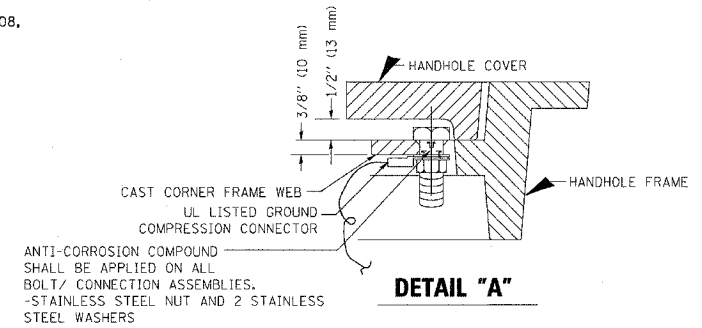
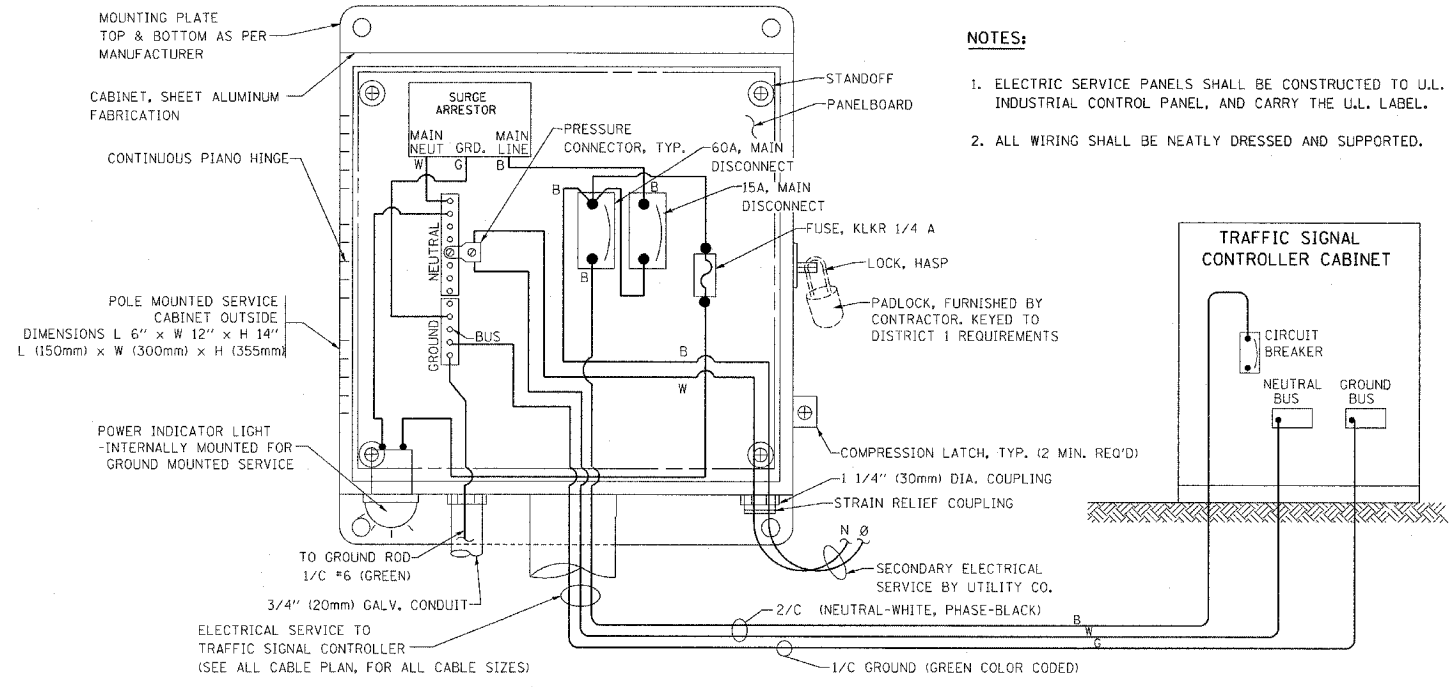
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STA. _____ TO STA. _____		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

CONTRACT # 62417

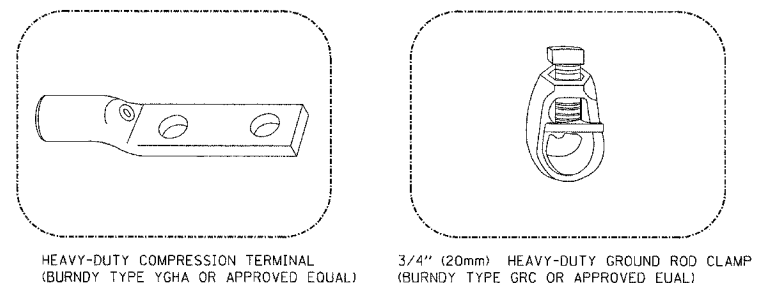
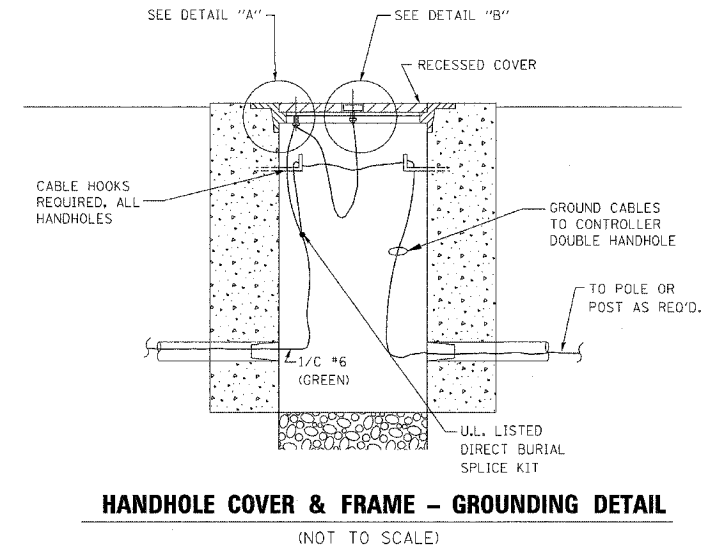
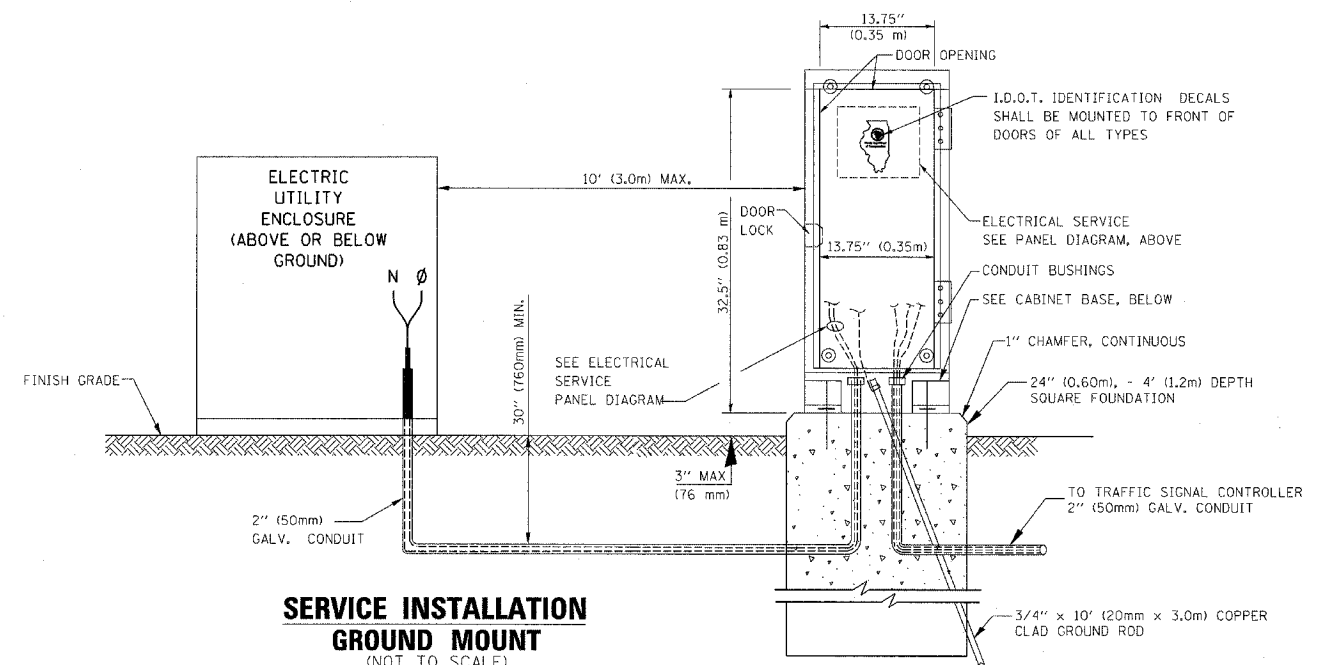
NOTES:

GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

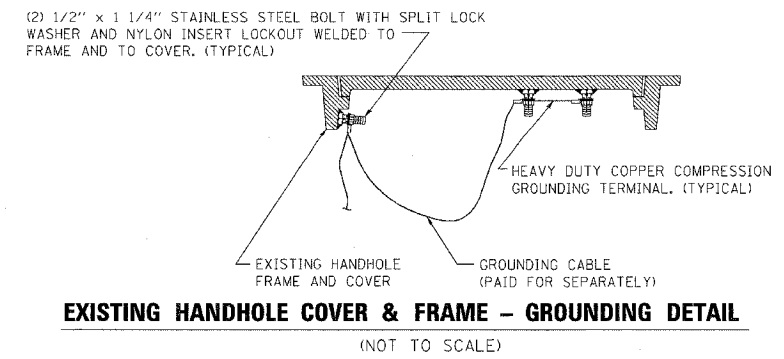
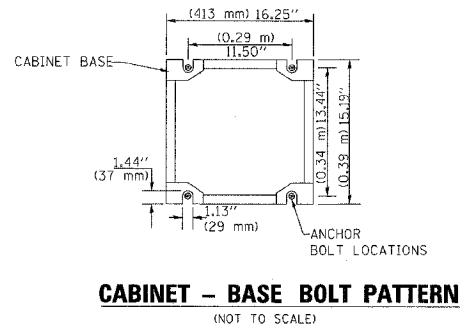
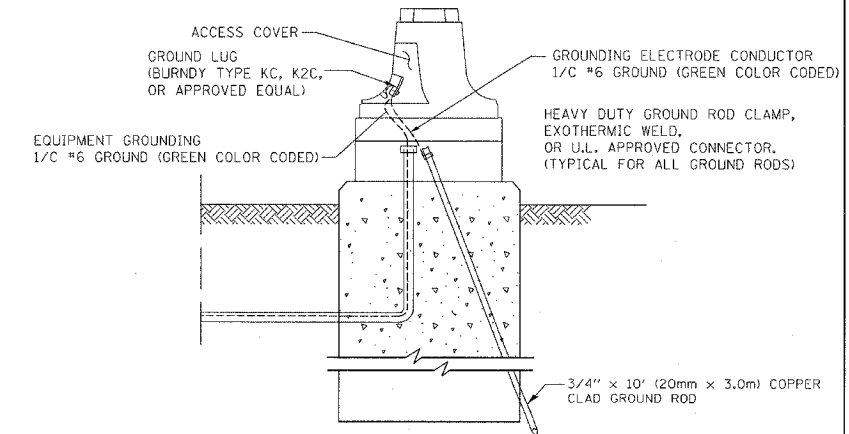


ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES. 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES. 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



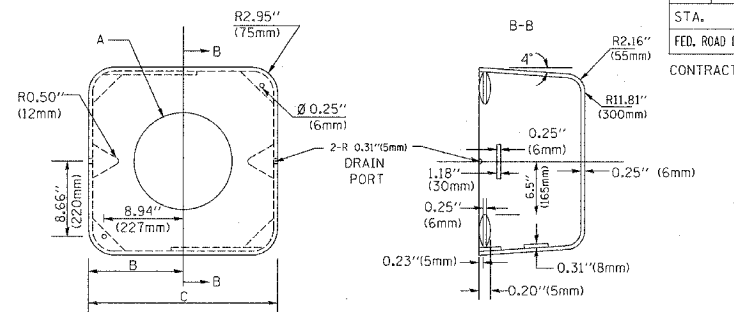
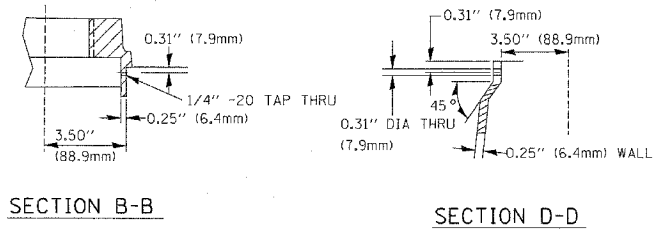
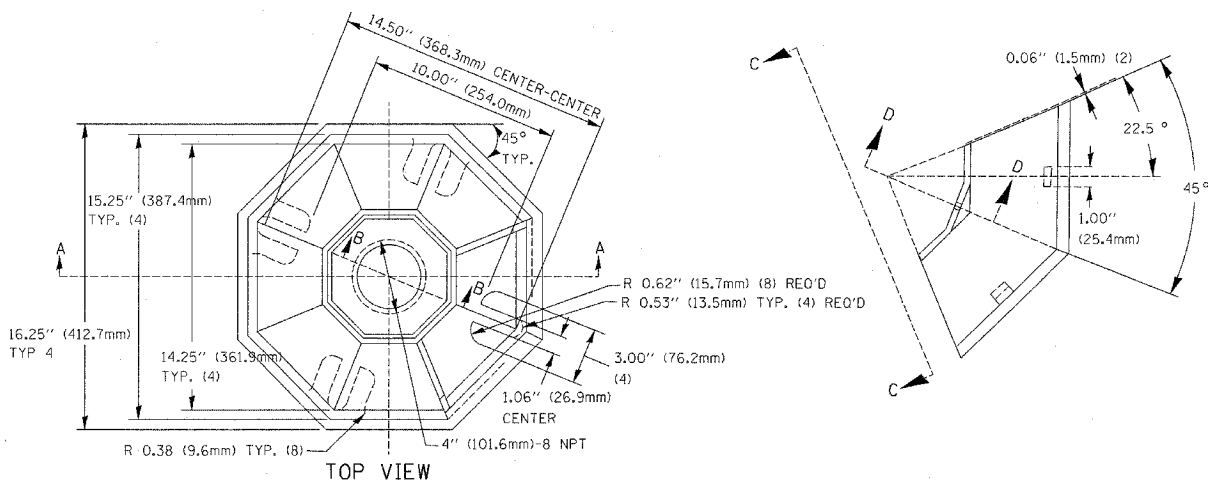
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

SCALE: VERT. NONE
 HORIZ. 1-01-02
 DRAWN BY: RWP
 DESIGNED BY: DAD
 CHECKED BY: DAZ
 SHEET 3 OF 4

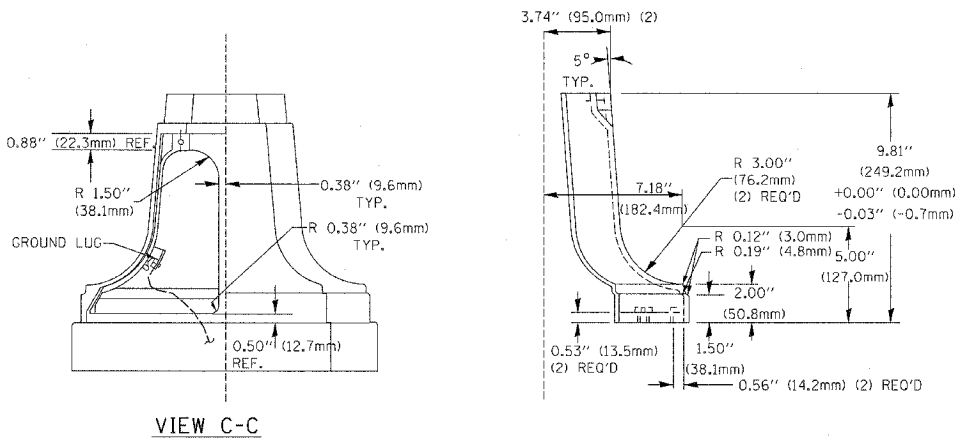
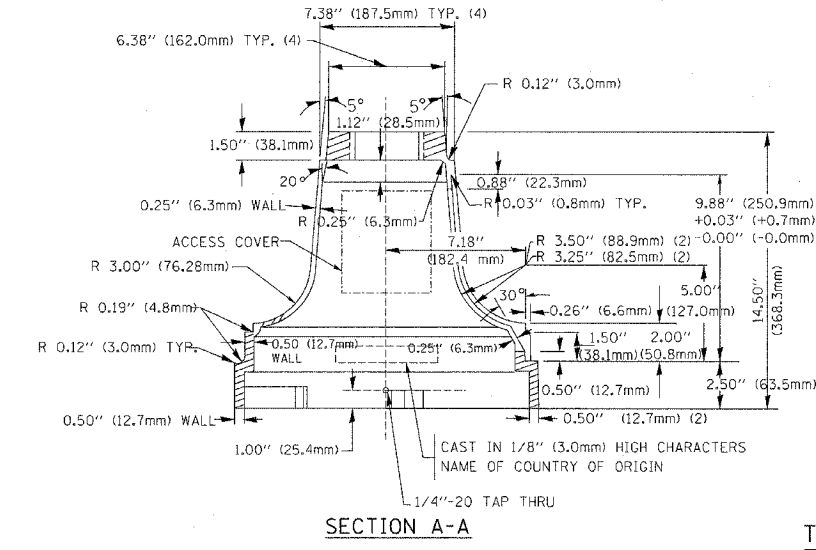
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 DATE-TIME: 11/13/08 11:50:00
 DGN-SPEC:

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	1113 & 1114R-5	WILL	525	371
STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	
CONTRACT # 62417				

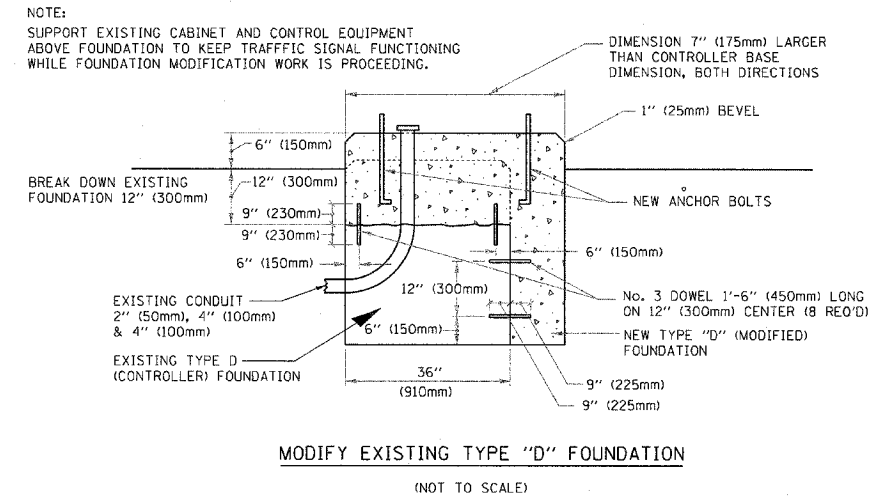


TYPE	A	B	C	HEIGHT	WEIGHT
I	Ø 10.125\"(257mm)	9.5\"(241mm)	19\"(483mm)	12\"(300mm)	24kg
II	Ø 11.125\"(283mm)	10.75\"(273mm)	21.5\"(546mm)	12\"(300mm)	26kg

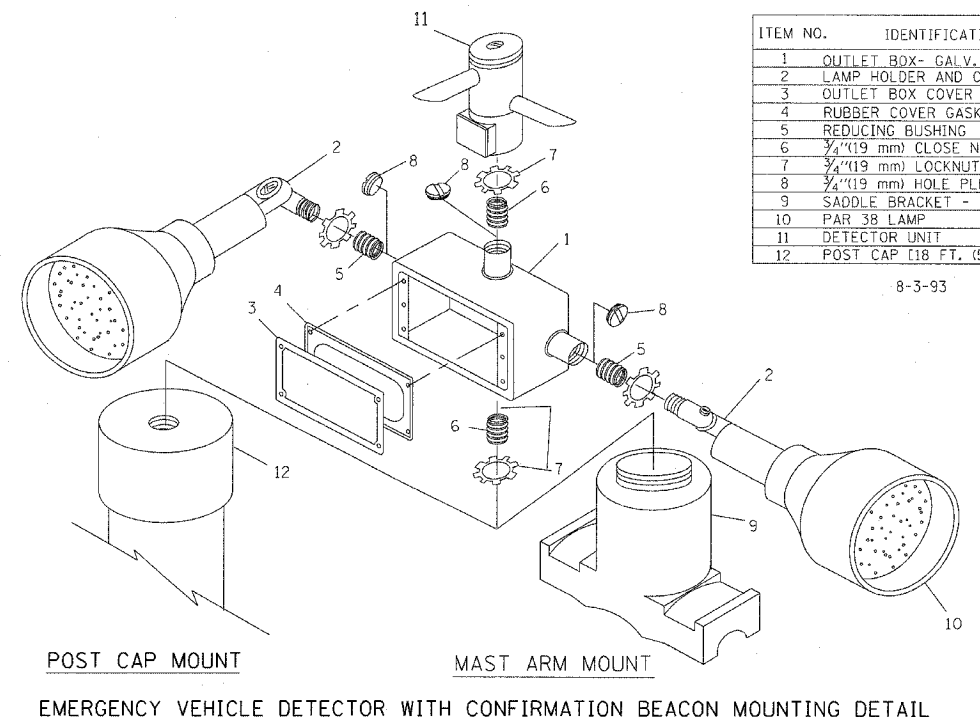
MATERIAL:
 - ASTM A48 CLASS 30 GREY IRON
 - ASTM A123 HOT DIPPED GALVANIZED



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A

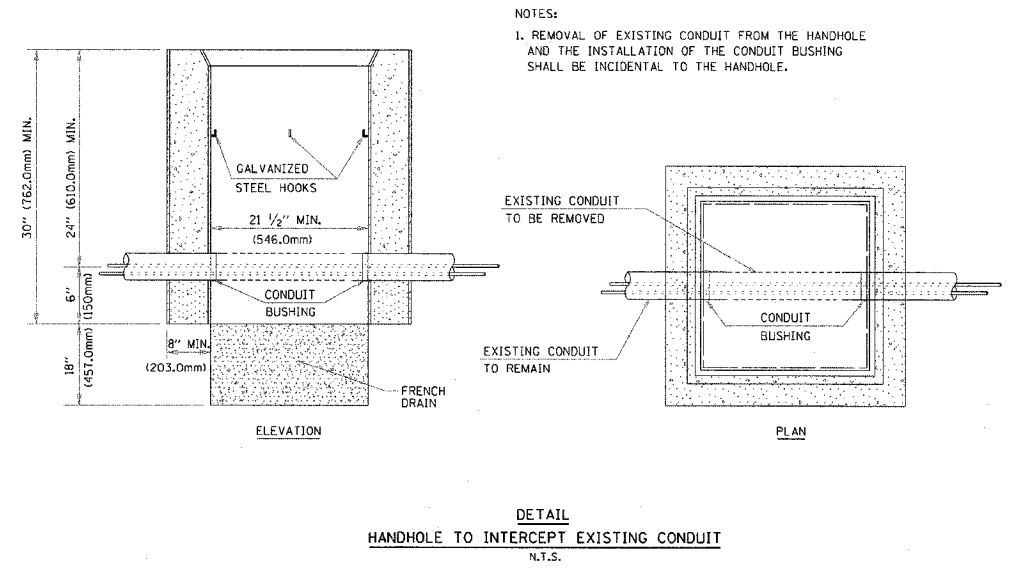
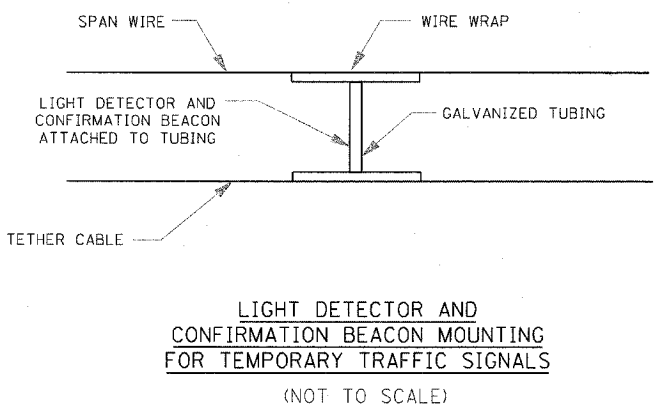


NOTE:
 SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\"(19 mm) CLOSE NIPPLE
7	3/4\"(19 mm) LOCKNUT
8	3/4\"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	PAR 38 LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

- NOTES:
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
 - ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
 ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
 ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
 - WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT 1
 STANDARD TRAFFIC SIGNAL DESIGN DETAILS
 SCALE: VERT. NONE
 HORIZ. 1-01-02
 DRAWN BY: RWP
 DESIGNED BY: DAD
 CHECKED BY: DAZ
 SHEET 4 OF 4

J:\PROJECTS\Projects 2003\1159.Catona\2008_rev\ok_for_t_beggs\05-std4.dgn
 DATE-TIME: 05-01-02 10:00:00
 DGN-SPEC:

NOTES FOR TEMPORARY TRAFFIC SIGNALS

- ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12". HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL. AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACTOR'S BID PRICE.

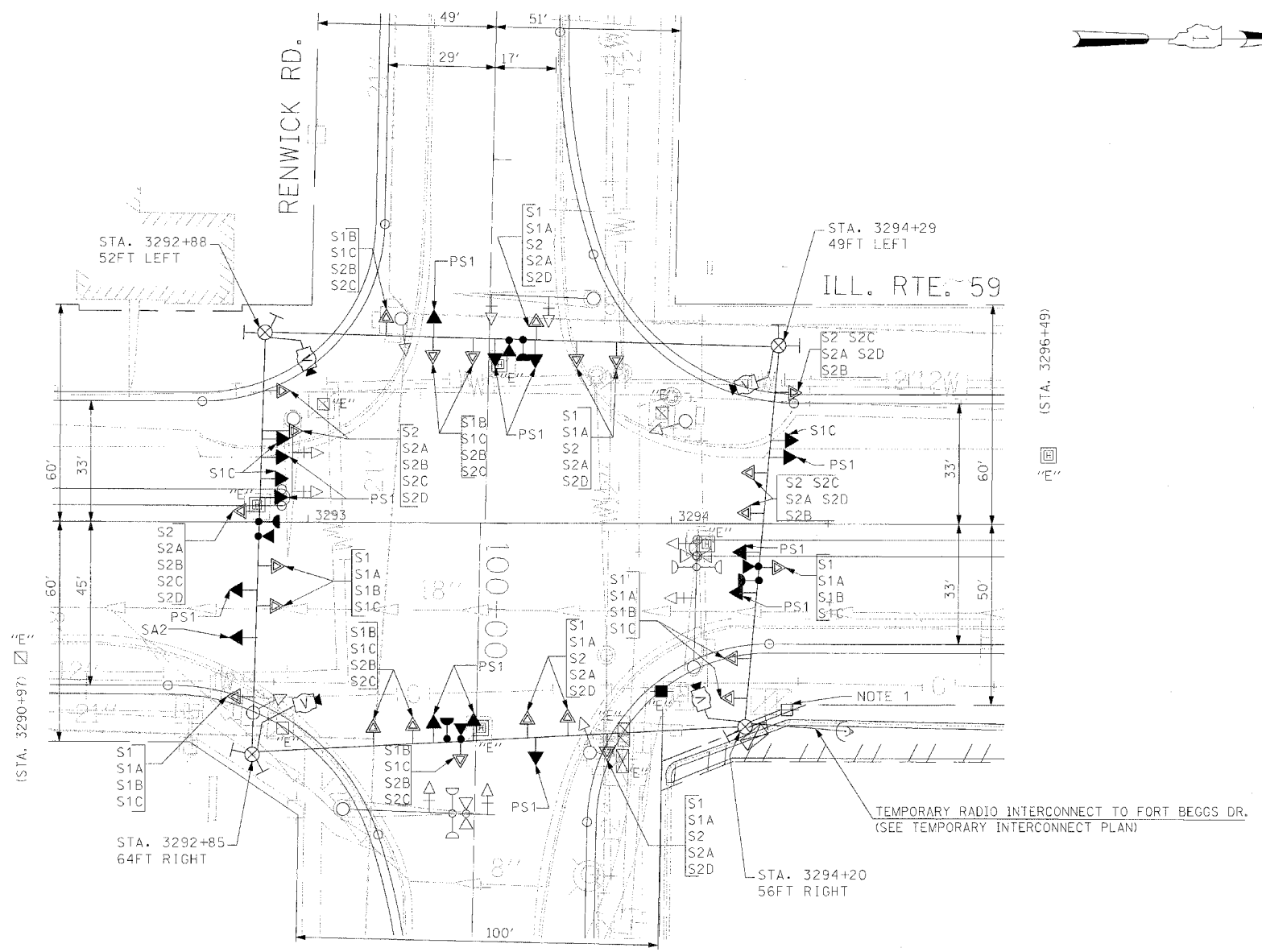
- | | | |
|---|------|---|
| 1 | EACH | CONTROLLER AND CABINET COMPLETE |
| 2 | EACH | SIGNAL HEAD, 1-FACE 3-SECTION, BRACKET MOUNTED |
| 6 | EACH | SIGNAL HEAD, 1-FACE 3-SECTION, MAST ARM MOUNTED |
| 2 | EACH | SIGNAL HEAD, 1-FACE 5-SECTION, BRACKET MOUNTED |
| 2 | EACH | SIGNAL HEAD, 1-FACE 5-SECTION, MAST ARM MOUNTED |
| 8 | EACH | TRAFFIC SIGNAL BACK PLATE |
| 4 | EACH | TRAFFIC SIGNAL POST |
| 4 | EACH | STEEL MAST ARM ASSEMBLY AND POLE (22 FOOT, 30 FOOT, 36 FOOT, 42 FOOT) |
| 1 | EACH | SERVICE INSTALLATION |

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

AGENCY: VILLAGE OF PLAINFIELD

CONTACT INFORMATION:
MR. RANDY JESSEN
PLAINFIELD PUBLIC WORKS
14400 COIL PLUS DRIVE
PLAINFIELD, ILLINOIS 60544
PHONE: (815) 436-3577

- | | | |
|---|------|------------------------------------|
| 2 | EACH | LIGHT DETECTOR |
| 1 | EACH | LIGHT DETECTOR AMPLIFIER MODEL 452 |



TEMPORARY TRAFFIC SIGNAL PLAN
STAGES: PRE-STAGE 1, STAGE 1, STAGE 1A, STAGE 1B, STAGE 1C, STAGE 2, STAGE 2A, STAGE 2B, STAGE 2C, AND STAGE 2D

- NOTE 1: THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION WITH COMED FOR THE PROPOSED SERVICE LOCATION, IF NECESSARY THE TEMPORARY TRAFFIC SIGNAL CONTROLLER CAN BE LOCATED AT A MORE CONVENIENT LOCATION WITH THE APPROVAL OF THE ENGINEER, THE COST OF WHICH SHALL BE INCIDENTAL TO THE PAY ITEM "TEMPORARY TRAFFIC SIGNAL INSTALLATION".
- NOTE 2: ALL THE VIDEO DETECTION ZONES SHALL BE REDEFINED FOR EACH CONSTRUCTION STAGE. THIS WORK IS INCIDENTAL TO THE PAY ITEM "TEMPORARY TRAFFIC SIGNAL INSTALLATION".
- NOTE 3: THE ORIGINAL SIGNAL HEAD PLACEMENT FOR ALL APPROACHES OF THE INTERSECTION IS FOR EXISTING PAVEMENT GEOMETRICS (PS1). ADDITIONAL CONSTRUCTION STAGES, WHERE ORIGINAL SIGNAL HEAD PLACEMENT IS UTILIZED, ARE SHOWN WITH S1 THROUGH S2D FOR CONSTRUCTION STAGE 1 THROUGH CONSTRUCTION STAGE 2D.
- NOTE 4: THE SECONDARY SIGNAL HEAD PLACEMENT IS FOR CONSTRUCTION STAGES AS MARKED. NEXT TO THE SIGNAL HEAD, FOR APPLICABLE CONSTRUCTION STAGES FOR INDIVIDUAL APPROACH OF THE INTERSECTION.

TEMPORARY TRAFFIC SIGNAL LEGEND

- ← TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- ↖ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- ⊗ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- ⊠ TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- ⊕ TEMPORARY SERVICE INSTALLATION
- ⊔ TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- ⊙ PEDESTRIAN PUSHBUTTON DETECTOR
- ⊕ EMERGENCY VEHICLE LIGHT DETECTOR
- ⊕ CONFIRMATION BEACON
- CT COMMON TRENCH
- UD UNIT DUCT
- G.S. CONDUIT IN GROUND
- ⊕ HANDHOLE
- ⊕ HEAVY DUTY HANDHOLE
- ⊕ WIRELESS INTERCONNECT (ANTENNA)
- ⊕ VIDEO CAMERA ASSEMBLY

EXISTING EQUIPMENT TO BE REMOVED LEGEND

- ← EXISTING SIGNAL TO BE REMOVED
- ⊕ "E" EXISTING SERVICE INSTALLATION TO BE REMOVED
- ⊙ EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED
- ⊕ EXISTING MAST ARM POLE AND FOUNDATION TO BE REMOVED
- ⊕ "E" EXISTING CONTROLLER AND FOUNDATION TO BE REMOVED
- ⊕ "E" EXISTING HANDHOLE TO BE REMOVED
- ⊕ "E" EXISTING DOUBLE HANDHOLE TO BE REMOVED
- ⊕ PEDESTRIAN SIGNAL TO BE REMOVED
- ⊙ EXISTING PEDESTRIAN PUSH-BUTTON TO BE REMOVED
- ⊕ EXISTING EMERGENCY VEHICLE LIGHT DETECTOR TO BE REMOVED
- ⊕ EXISTING CONFIRMATION BEACON TO BE REMOVED
- ⊕ "E" EXISTING HEAVY DUTY HANDHOLE TO BE REMOVED
- ⊕ EXISTING STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

ILLINOIS DEPARTMENT OF TRANSPORTATION

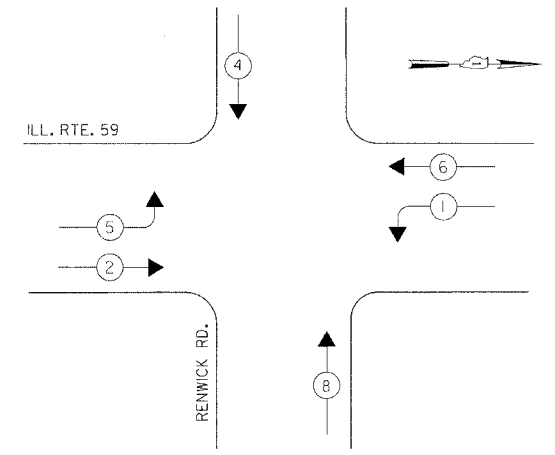
TEMPORARY TRAFFIC SIGNAL
INSTALLATION AND REMOVAL PLAN
ILLINOIS ROUTE 59 AT RENWICK ROAD
(SHEET 1 OF 3)

REVISIONS	
NAME	DATE
PKG, MAA	05-01-08

SCALE: 1"=20'
DATE: 03/18/2008
DRAWN BY: BB, KP
DESIGNED BY: PKG/RRM
CHECKED BY: PKG/RRM

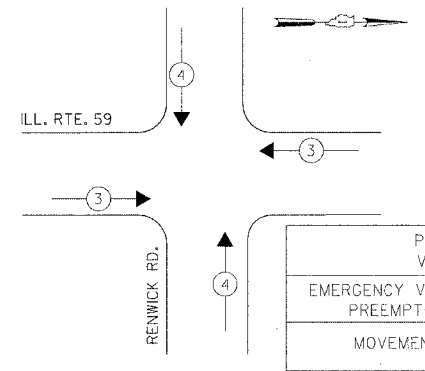
GO GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 306
CHICAGO, ILLINOIS 60631 TEL: (773) 774-5900

CONTROLLER SEQUENCE



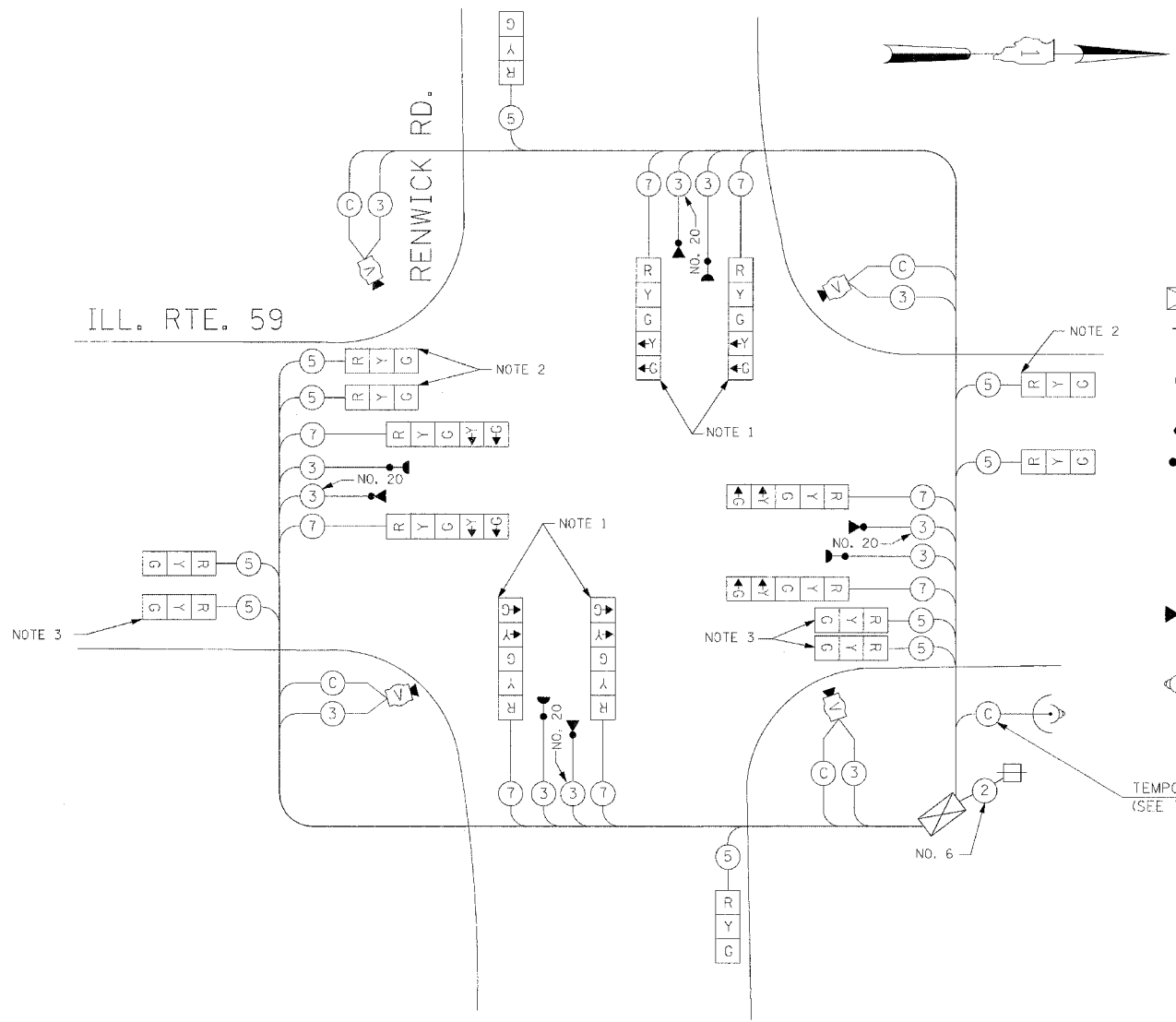
TEMPORARY PHASE DESIGNATION DIAGRAM
 STAGES: PRE-STAGE 1, STAGE 1, STAGE 1A, STAGE 1B, STAGE 1C, STAGE 2, STAGE 2A, STAGE 2B, STAGE 2C, AND STAGE 2D.

EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	↔	↑↓

STAGES: PRE-STAGE 1, STAGE 1, STAGE 1A, STAGE 1B, STAGE 1C, STAGE 2, AND STAGE 2A



TEMPORARY CABLE PLAN

STAGES: PRE-STAGE 1, STAGE 1, STAGE 1A, STAGE 1B, STAGE 1C, STAGE 2, STAGE 2A, STAGE 2B, STAGE 2C, AND STAGE 2D.

TEMPORARY CABLE DIAGRAM LEGEND

- TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12" (300mm)
- TEMPORARY CONTROLLER CABINET
- TEMPORARY SERVICE INSTALLATION
- INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED.
- EMERGENCY VEHICLE LIGHT DETECTOR
- CONFIRMATION BEACON
- VEHICLE DETECTOR, INDUCTION LOOP
- PEDESTRIAN PUSHBUTTON DETECTOR
- 12" (300mm) PEDESTRIAN SIGNAL SECTION
- MICROWAVE VEHICLE SENSOR
- VIDEO CAMERA ASSEMBLY
- WIRELESS INTERCONNECT (ANTENNA)
- CO-AXIAL VIDEO CABLE

- NOTE 1: DURING CONSTRUCTION STAGES PRE-STAGE 1, STAGE 1, STAGE 1A, STAGE 1B, STAGE 1C, STAGE 2, STAGE 2A, STAGE 2B, STAGE 2C, AND STAGE 2D THE GREEN AND YELLOW LEFT ARROW INDICATION SECTIONS FOR EASTBOUND AND WESTBOUND DIRECTION OF TRAFFIC SHALL BE BAGGED AND DISCONNECTED AT THE CONTROLLER AND APPLICABLE PHASES SHALL BE DISABLED IN THE CONTROLLER.
- NOTE 2: THESE 3-SECTION CIRCULAR GREEN, YELLOW, AND RED INDICATION SIGNALS SHALL ONLY BE USED DURING CONSTRUCTION STAGE 1C FOR SOUTHBOUND RIGHT TURN LANE. THESE 3 SECTION SIGNAL HEADS SHALL BE DISCONNECTED AT THE CONTROLLER AND BAGGED DURING OTHER CONSTRUCTION STAGES OR SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.
- NOTE 3: THESE 3-SECTION CIRCULAR GREEN, YELLOW, AND RED INDICATION SIGNALS SHALL ONLY BE USED DURING CONSTRUCTION STAGE 2A FOR NORTHBOUND RIGHT TURN LANE. THESE 3 SECTION SIGNAL HEADS SHALL BE DISCONNECTED AT THE CONTROLLER AND BAGGED DURING OTHER CONSTRUCTION STAGES OR SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.
- NOTE 4: ALL THE TEMPORARY TRAFFIC SIGNAL HEADS SHALL BE LED TYPE

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO LAMPS	INCAND.	LED	OPERATION	
SIGNAL (RED)	16	135	17	0.50	153
(YELLOW)	18	135	25	0.25	112.5
(GREEN)	18	135	15	0.25	67.5
ARROW	12	135	12	0.10	14.4
PED. SIGNAL		90	25	1.00	
CONTROLLER		100	100	1.00	100.0
ILLUM. SIGN				0.05	
FLASHER				0.50	
ENERGY COSTS TO:				TOTAL =	447.4

ILLINOIS DEPARTMENT OF TRANSPORTATION
 201 WEST CENTER COURT
 SCHAMBURG, ILLINOIS 60196-1096

ENERGY SUPPLY CONTACT: MS. BETTY BRULC
 PHONE: (815) 724-5052
 COMPANY: COMED

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

GA GANDHI AND ASSOCIATES, INC.
 ENGINEERS AND PLANNERS
 6035 N. NORTHWEST HIGHWAY
 SUITE 300
 CHICAGO, ILLINOIS 60631 TEL. (773) 774-5900

REVISIONS	
NAME	DATE
PKG, MAA	05-01-08

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TEMPORARY CABLE PLAN,
 PHASE DESIGNATION DIAGRAM, AND
 EMERGENCY VEHICLE PREEMPTION SEQUENCE
 ILLINOIS ROUTE 59 AT RENWICK ROAD
 (SHEET 2 OF 3)

SCALE: N.T.S.
 DATE: 03/18/2008

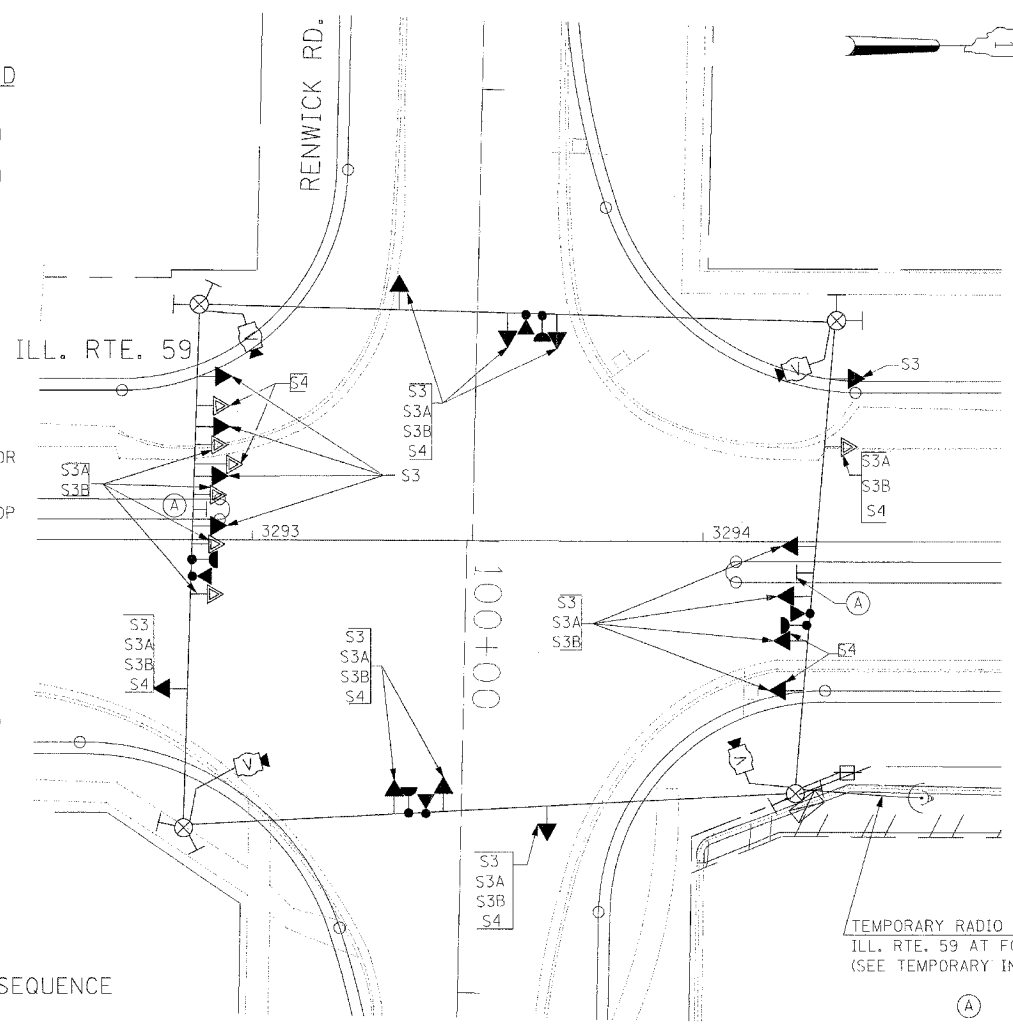
DRAWN BY: BB, KP
 DESIGNED BY: PKG/RRM
 CHECKED BY: PKG/RRM

TEMPORARY TRAFFIC SIGNAL LEGEND

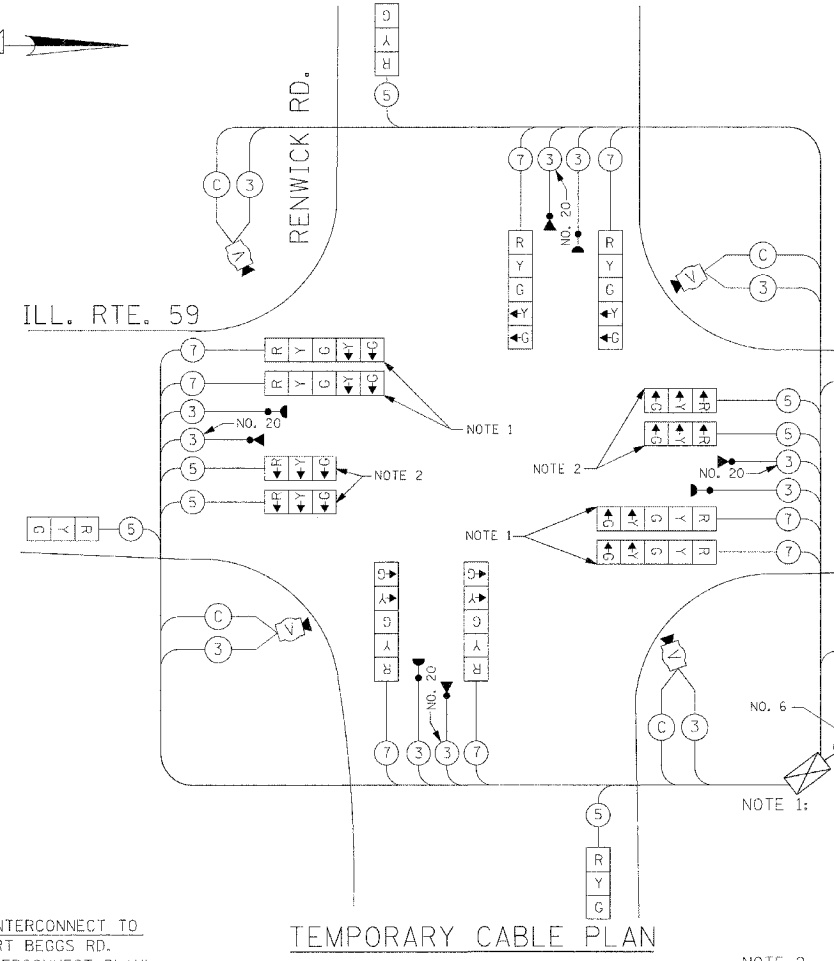
- ← TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- ↔ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- ⊗ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- ⊠ TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- ⊕ TEMPORARY SERVICE INSTALLATION
- ⊡ TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- ⊡ MICROWAVE VEHICLE SENSOR
- ⊡ PEDESTRIAN PUSHBUTTON DETECTOR
- ⊡ EMERGENCY VEHICLE LIGHT DETECTOR
- ⊡ CONFIRMATION BEACON
- ⊡ VEHICLE DETECTOR, INDUCTION LOOP
- CT COMMON TRENCH
- UD UNIT DUCT
- G.S. CONDUIT IN GROUND
- ⊡ HANDHOLE
- ⊡ HEAVY DUTY HANDHOLE
- ⊡ VIDEO CAMERA ASSEMBLY
- ⊡ WIRELESS INTERCONNECT (ANTENNA)

TEMPORARY CABLE DIAGRAM LEGEND

- ⊠ TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12" (300mm)
- ⊠ TEMPORARY CONTROLLER CABINET
- ⊠ TEMPORARY SERVICE INSTALLATION
- 5 INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED.
- ⊡ EMERGENCY VEHICLE LIGHT DETECTOR
- ⊡ CONFIRMATION BEACON
- ⊡ VEHICLE DETECTOR, INDUCTION LOOP
- ⊡ PEDESTRIAN PUSHBUTTON DETECTOR
- ⊡ 12" (300mm) PEDESTRIAN SIGNAL SECTION
- ⊡ VIDEO CAMERA ASSEMBLY
- ⊡ WIRELESS INTERCONNECT (ANTENNA)
- ⊡ CO-AXIAL VIDEO CABLE
- ⊡ TEMPORARY RADIO INTERCONNECT TO FORT BEGGS RD. (SEE TEMPORARY INTERCONNECT PLAN)

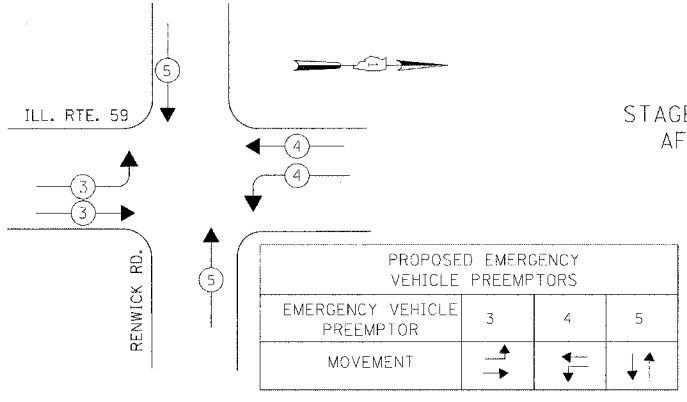


TEMPORARY TRAFFIC SIGNAL PLAN
(SCALE: 1"=20')
STAGES: STAGE 3, STAGE 3A, STAGE 3B, AND AFTER PROPOSED ROADWAY GEOMETRICS ARE BUILT AND OPEN TO TRAFFIC



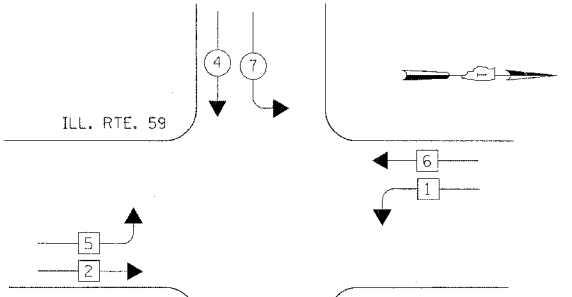
TEMPORARY CABLE PLAN
(NOT TO SCALE)
STAGES: STAGE 3, STAGE 3A, STAGE 3B, AND AFTER PROPOSED ROADWAY GEOMETRICS ARE BUILT AND OPEN TO TRAFFIC

EMERGENCY VEHICLE PREEMPTION SEQUENCE



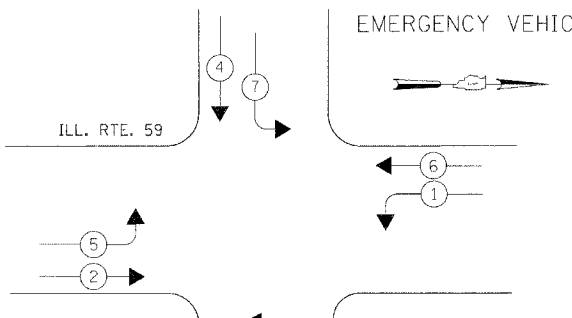
STAGES: STAGE 3, STAGE 3A, AND 3B.

CONTROLLER SEQUENCE



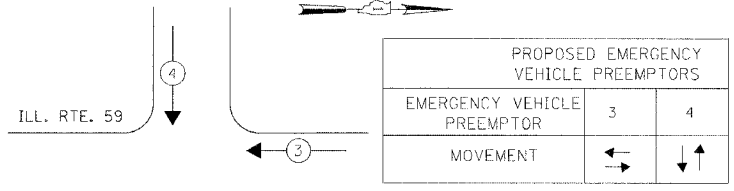
TEMPORARY PHASE DESIGNATION DIAGRAM
STAGES: STAGE 3, STAGE 3A, AND 3B.

CONTROLLER SEQUENCE



TEMPORARY PHASE DESIGNATION DIAGRAM
STAGE 4, AND AFTER PROPOSED ROADWAY GEOMETRICS ARE BUILT AND OPEN TO TRAFFIC

EMERGENCY VEHICLE PREEMPTION SEQUENCE



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO LAMPS	WATTAGE INCAND.	WATTAGE LED	% OPERATION	
SIGNAL (RED)	16	135	17	0.50	136.0
(YELLOW)	16	135	25	0.25	100.0
(GREEN)	16	135	15	0.25	60.0
ARROW	8	135	12	0.10	9.6
PED. SIGNAL		90	25	1.00	
CONTROLLER	1	100	100	1.00	100.0
ILLUM. SIGN				0.05	
FLASHER				0.50	
ENERGY COSTS TO:				TOTAL =	405.6

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196-1096
ENERGY SUPPLY CONTACT: MS. BETTY BRULC
PHONE: (815) 724-5052
COMPANY: COMED

- LEGEND**
- ⊡ DUAL ENTRY PHASE
 - ⊡ SINGLE ENTRY PHASE
 - ⊡ PEDESTRIAN PHASE
 - * NUMBER REFERS TO ASSOCIATED PHASE

REVISIONS	
NAME	DATE
PKG, MAA	05-01-08

ILLINOIS DEPARTMENT OF TRANSPORTATION
TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN,
TEMPORARY CABLE PLAN,
PHASE DESIGNATION DIAGRAM, AND
EMERGENCY VEHICLE PREEMPTION SEQUENCE
ILLINOIS ROUTE 59 AT RENWICK ROAD
(SHEET 3 OF 3)

SCALE: AS NOTED
DATE: 03/18/2008

DRAWN BY: BB/KP
DESIGNED BY: PKG/RRM
CHECKED BY: PKG/RRM

GO GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 306
CHICAGO, ILLINOIS 60631 TEL: (773) 774-5910

NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12". HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS. SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON. IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

1	EACH	CONTROLLER AND CABINET, COMPLETE
4	EACH	WOOD POLES
1	L SUM	CABLE, MESSENGER WIRE, TETHER WIRE AND DOWN GUYS
6	EACH	SIGNAL HEAD, 1-FACE 3-SECTION, SPAN WIRE MOUNTED
3	EACH	SIGNAL HEAD, 1-FACE 5-SECTION, SPAN WIRE MOUNTED
1	EACH	SIGNAL HEAD, 1-FACE 5-SECTION, BRACKET MOUNTED
3	EACH	TRAFFIC SIGNAL POST
2	EACH	PEDESTRIAN SIGNAL HEAD, 1-FACE, BRACKET MOUNTED
1	EACH	PEDESTRIAN SIGNAL HEAD, 2-FACE, BRACKET MOUNTED
3	EACH	PEDESTRIAN PUSH-BUTTON DETECTORS
1	EACH	SERVICE INSTALLATION

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

AGENCY: VILLAGE OF PLAINFIELD

CONTACT INFORMATION:
MR. RANDY JESSEN
PLAINFIELD PUBLIC WORKS
14400 COIL PLUS DRIVE
PLAINFIELD, ILLINOIS 60544
PHONE: (815) 436-3577

2	EACH	LIGHT DETECTOR AND CONFIRMATION BEACON
1	EACH	LIGHT DETECTOR AMPLIFIER

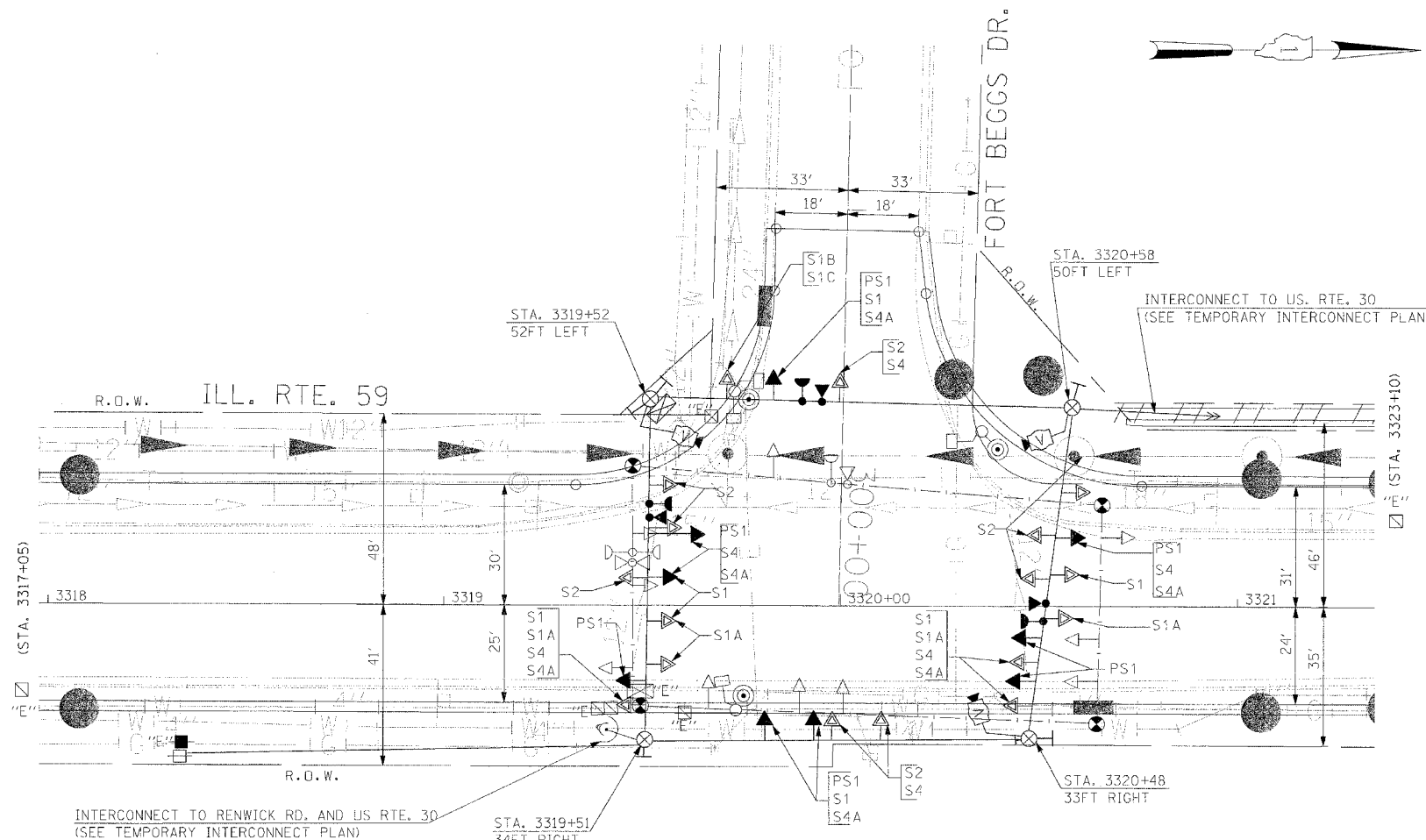
NOTE 1: ALL THE VIDEO DETECTION ZONES SHALL BE REDEFINED FOR EACH CONSTRUCTION STAGE. THIS WORK IS INCIDENTAL TO THE PAY ITEM "TEMPORARY TRAFFIC SIGNAL INSTALLATION".

NOTE 2: THE ORIGINAL SIGNAL HEAD PLACEMENT FOR ALL APPROACHES OF THE INTERSECTION IS FOR EXISTING PAVEMENT GEOMETRICS (PS1). ADDITIONAL CONSTRUCTION STAGES, WHERE ORIGINAL SIGNAL HEAD PLACEMENT IS UTILIZED, ARE SHOWN WITH S1 THROUGH S2A FOR CONSTRUCTION STAGE 1 THROUGH CONSTRUCTION STAGE 2A.

NOTE 3: THE SECONDARY SIGNAL HEAD PLACEMENT IS FOR CONSTRUCTION STAGES AS MARKED, NEXT TO THE SIGNAL HEAD, FOR APPLICABLE CONSTRUCTION STAGES FOR INDIVIDUAL APPROACH OF THE INTERSECTION.

NOTE 4: DURING WINTER SHUTDOWN THE SIGNAL HEAD PLACEMENTS SHALL BE SIMILAR TO STAGE 3 OR AS DIRECTED BY THE ENGINEER.

NOTE 5: WHEN FORT BEGGS DRIVE IS CLOSED TO VEHICLES TRAFFIC DURING STAGE 1A, THE CIRCULAR GREEN INDICATION ON THE SIGNAL HEADS FOR THE NORTHBOUND AND SOUTHBOUND DIRECTION OF TRAFFIC SHALL BE PROGRAMMED TO STAY ON THROUGHOUT THE DURATION OF STAGE 1A. THE SIGNAL H



TEMPORARY TRAFFIC SIGNAL PLAN
STAGES: PRE-STAGE 1, STAGE 1, STAGE 2, STAGE 4, AND STAGE 4A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114)R-5	WILL	525	375
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT:		

CONTRACT # 62417

EXISTING EQUIPMENT TO BE REMOVED LEGEND

- ◀ EXISTING SIGNAL TO BE REMOVED
- "E" EXISTING SERVICE INSTALLATION TO BE REMOVED
- EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED
- EXISTING MAST ARM POLE AND FOUNDATION TO BE REMOVED
- ⊠ "E" EXISTING CONTROLLER AND FOUNDATION TO BE REMOVED
- ⊠ "E" EXISTING HANDHOLE TO BE REMOVED
- ⊠ "E" EXISTING DOUBLE HANDHOLE TO BE REMOVED
- PEDESTRIAN SIGNAL TO BE REMOVED
- ⊙ EXISTING PEDESTRIAN PUSH-BUTTON TO BE REMOVED
- ⊙ EXISTING EMERGENCY VEHICLE LIGHT DETECTOR TO BE REMOVED
- ⊙ CONFIRMATION BEACON TO BE REMOVED
- ⊠ "E" EXISTING HEAVY DUTY HANDHOLE TO BE REMOVED
- EXISTING STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED
- ⊙ EXISTING WOOD POLE (CLASS 5 OR BETTER) 45 FOOT MINIMUM TO BE REMOVED
- EXISTING CABLE, MESSENGER WIRE AND TETHER WIRE TO BE REMOVED

TEMPORARY TRAFFIC SIGNAL LEGEND

- ◀ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- ◀ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- ⊙ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- ⊠ TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- ⊠ TEMPORARY SERVICE INSTALLATION
- ⊠ TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- ⊙ MICROWAVE VEHICLE SENSOR
- ⊙ PEDESTRIAN PUSHBUTTON DETECTOR
- ⊙ EMERGENCY VEHICLE LIGHT DETECTOR
- ⊙ CONFIRMATION BEACON
- VEHICLE DETECTOR, INDUCTION LOOP
- CT COMMON TRENCH
- UD UNIT DUCT
- G.S. CONDUIT IN GROUND
- ⊠ HANDHOLE
- ⊠ HEAVY DUTY HANDHOLE
- ⊙ WIRELESS INTERCONNECT (ANTENNA)
- ⊙ VIDEO CAMERA ASSEMBLY

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

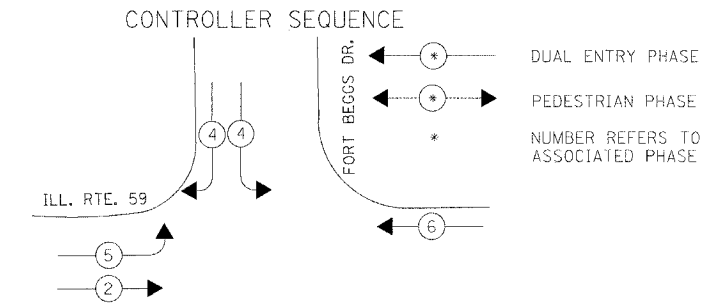
REVISIONS	
NAME	DATE
PKG, MAA	05-01-08

ILLINOIS DEPARTMENT OF TRANSPORTATION
TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVAL PLAN
ILLINOIS ROUTE 59 AT FORT BEGGS DRIVE
(SHEET 1 OF 3)

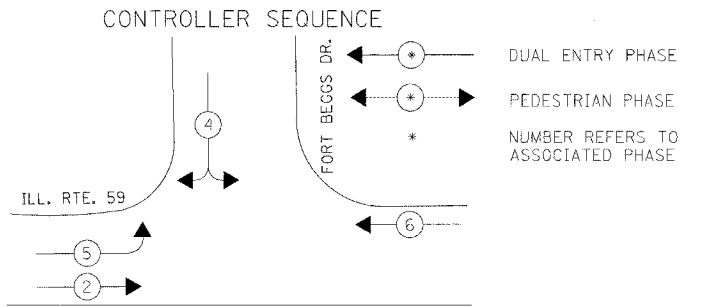
SCALE: 1"=20'
DATE: 03/18/2008

DRAWN BY: BB, KP
DESIGNED BY: PKG/RRM
CHECKED BY: PKG/RRM

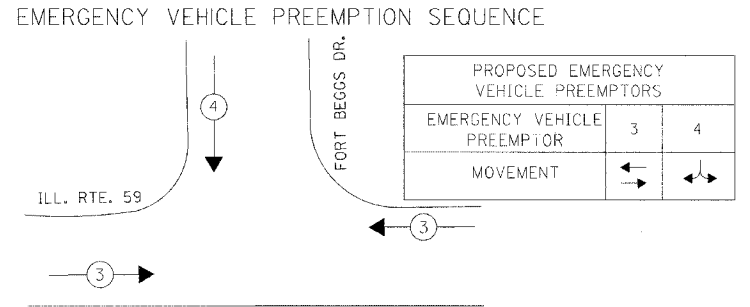
ga GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 306
CHICAGO, ILLINOIS 60631 TEL: (773) 774-5900



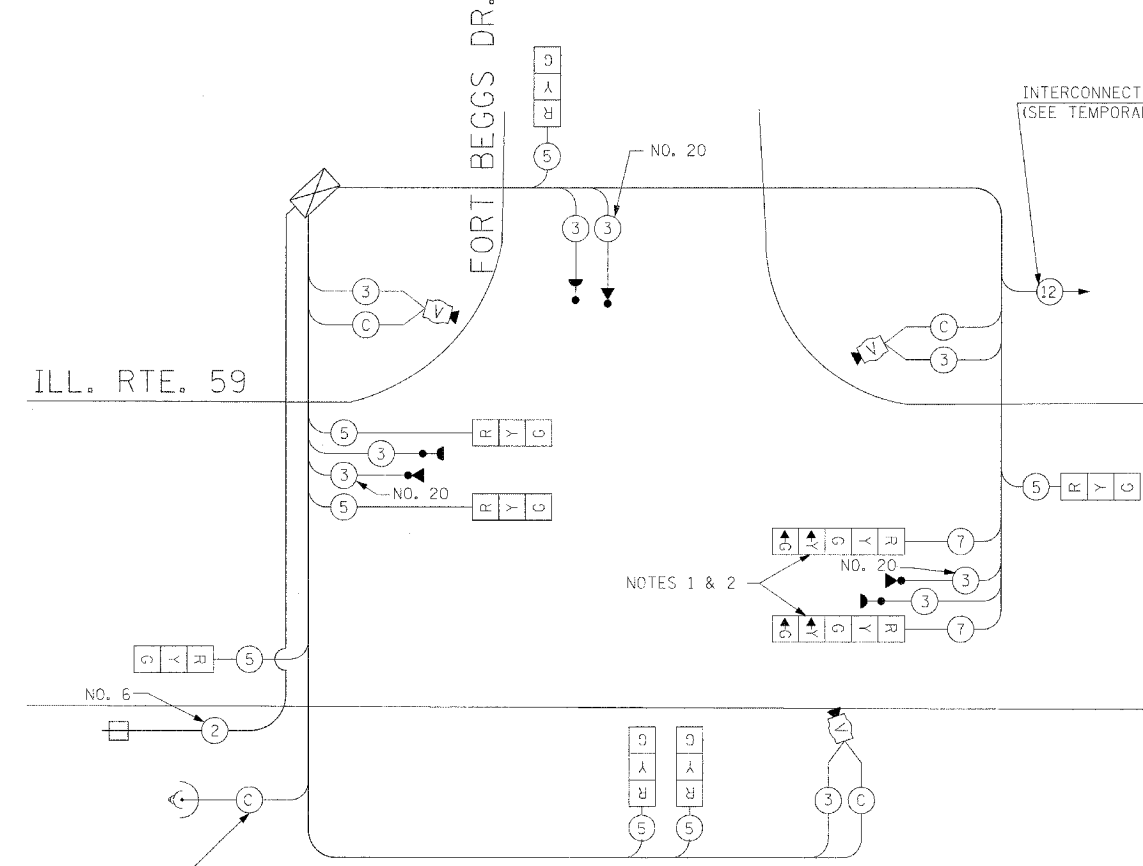
TEMPORARY PHASE DESIGNATION DIAGRAM
STAGES: PRE-STAGE 1 AND STAGE 1



TEMPORARY PHASE DESIGNATION DIAGRAM
STAGES: STAGE 2, STAGE 4, AND STAGE 4A.



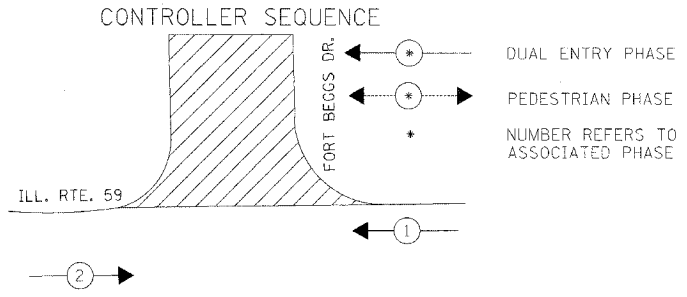
STAGES: PRE-STAGE 1, STAGE 1,
STAGE 2, STAGE 4, AND STAGE 4A.



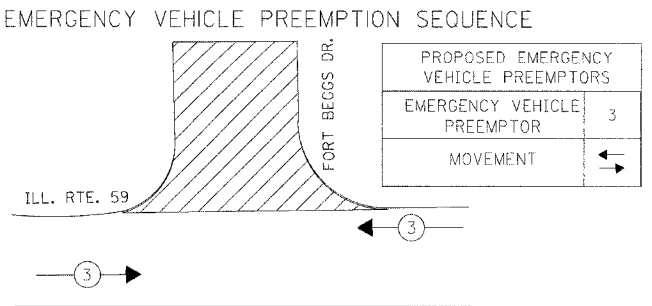
TEMPORARY CABLE PLAN
STAGES: PRE-STAGE 1, STAGE 1, STAGE 2,
STAGE 4, AND STAGE 4A.

- TEMPORARY CABLE DIAGRAM LEGEND
- TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12" (300mm)
 - TEMPORARY CONTROLLER CABINET
 - TEMPORARY SERVICE INSTALLATION
 - INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED.
 - EMERGENCY VEHICLE LIGHT DETECTOR CONFIRMATION BEACON
 - VEHICLE DETECTOR, INDUCTION LOOP
 - PEDESTRIAN PUSHBUTTON DETECTOR
 - 12" (300mm) PEDESTRIAN SIGNAL SECTION
 - VIDEO CAMERA ASSEMBLY
 - WIRELESS INTERCONNECT (ANTENNA)
 - CO-AXIAL VIDEO CABLE

- TEMPORARY CABLE PLAN NOTE:
- NOTE 1: DURING CONSTRUCTION STAGES STAGE 1A THE YELLOW AND GREEN LEFT ARROW INDICATION SECTIONS FOR NORTHBOUND DIRECTION OF TRAFFIC SHALL BE BAGGED AND DISCONNECTED AT THE CONTROLLER AND APPLICABLE PHASES SHALL BE DISABLED IN THE CONTROLLER.
- NOTE 2: WHEN FORT BEGGS ROAD IS CLOSED TO VEHICULAR TRAFFIC DURING STAGE 1A, THE CIRCULAR GREEN INDICATION ON THE SIGNAL HEADS FOR THE NORTHBOUND AND SOUTHBOUND DIRECTION OF TRAFFIC SHALL BE PROGRAMMED TO STAY ON THROUGHOUT THE DURATION OF STAGE 1A. THE SIGNAL HEADS FOR THE EASTBOUND DIRECTION OF TRAFFIC AND THE EVP EQUIPMENT SHALL BE DISCONNECTED AND BAGGED DURING THIS CONSTRUCTION STAGE AS DIRECTED BY THE ENGINEER.



TEMPORARY PHASE DESIGNATION DIAGRAM
STAGES: STAGE 1A



STAGES: STAGE 1A

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO LAMPS	WATTAGE	%OPERATION		
		INCAND.	LED		
SIGNAL (RED)	9	135	17	0.50	76.5
(YELLOW)	9	135	25	0.25	56.3
(GREEN)	9	135	15	0.25	33.8
ARROW	4	135	12	0.10	4.8
PED. SIGNAL		90	25	1.00	
CONTROLLER	1	100	100	1.00	100.0
ILLUM. SIGN				0.05	
FLASHER				0.50	
ENERGY COSTS TO:				TOTAL =	271.3

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196-1096

ENERGY SUPPLY CONTACT: MS. BETTY BRULC
PHONE: (815) 724-5052
COMPANY: COMED

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

REVISIONS	
NAME	DATE
PKG, MAA	05-01-08

ILLINOIS DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN,
PHASE DESIGNATION DIAGRAM, AND
EMERGENCY VEHICLE PREEMPTION SEQUENCE
ILLINOIS ROUTE 59 AT FORT BEGGS DRIVE
(SHEET 2 OF 3)

SCALE: N.T.S.
DATE: 03/18/2008

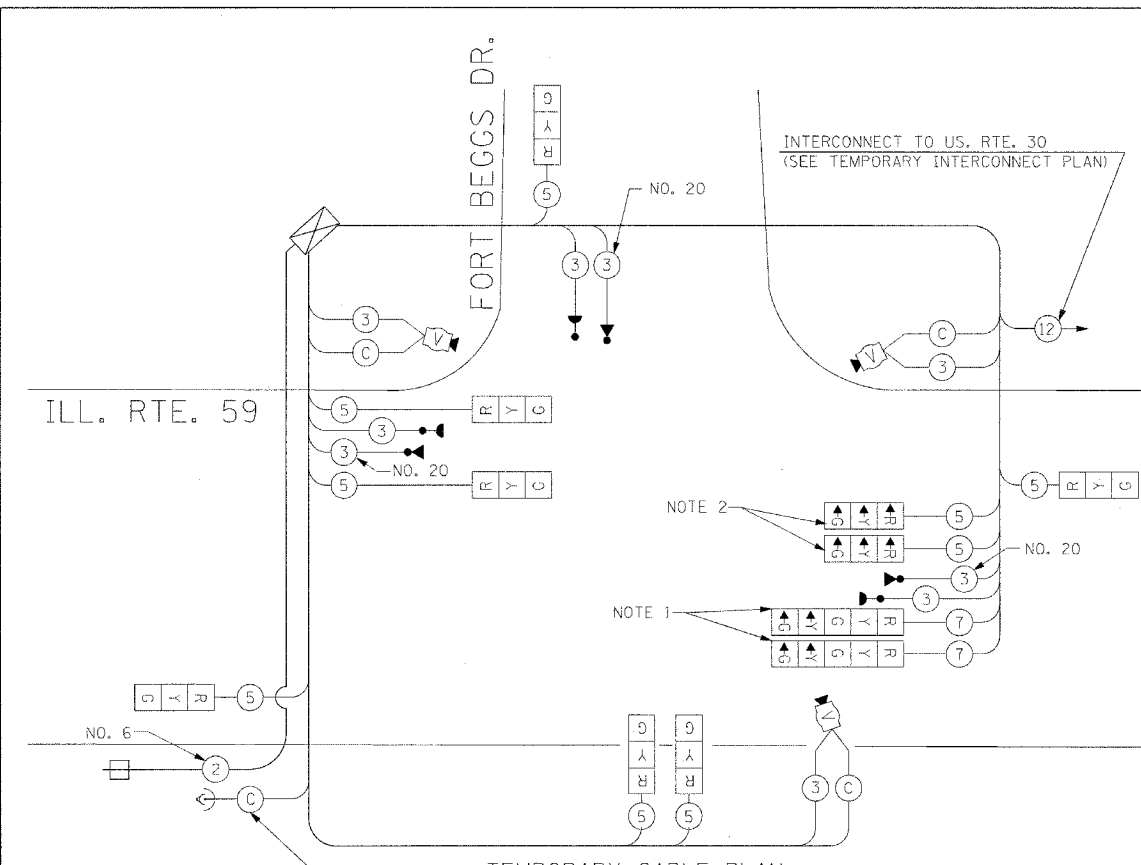
DRAWN BY: BB/KP
DESIGNED BY: PKG/RRM
CHECKED BY: PKG/RRM

GO GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 306
CHICAGO, ILLINOIS 60631 TEL.: (773) 774-5910

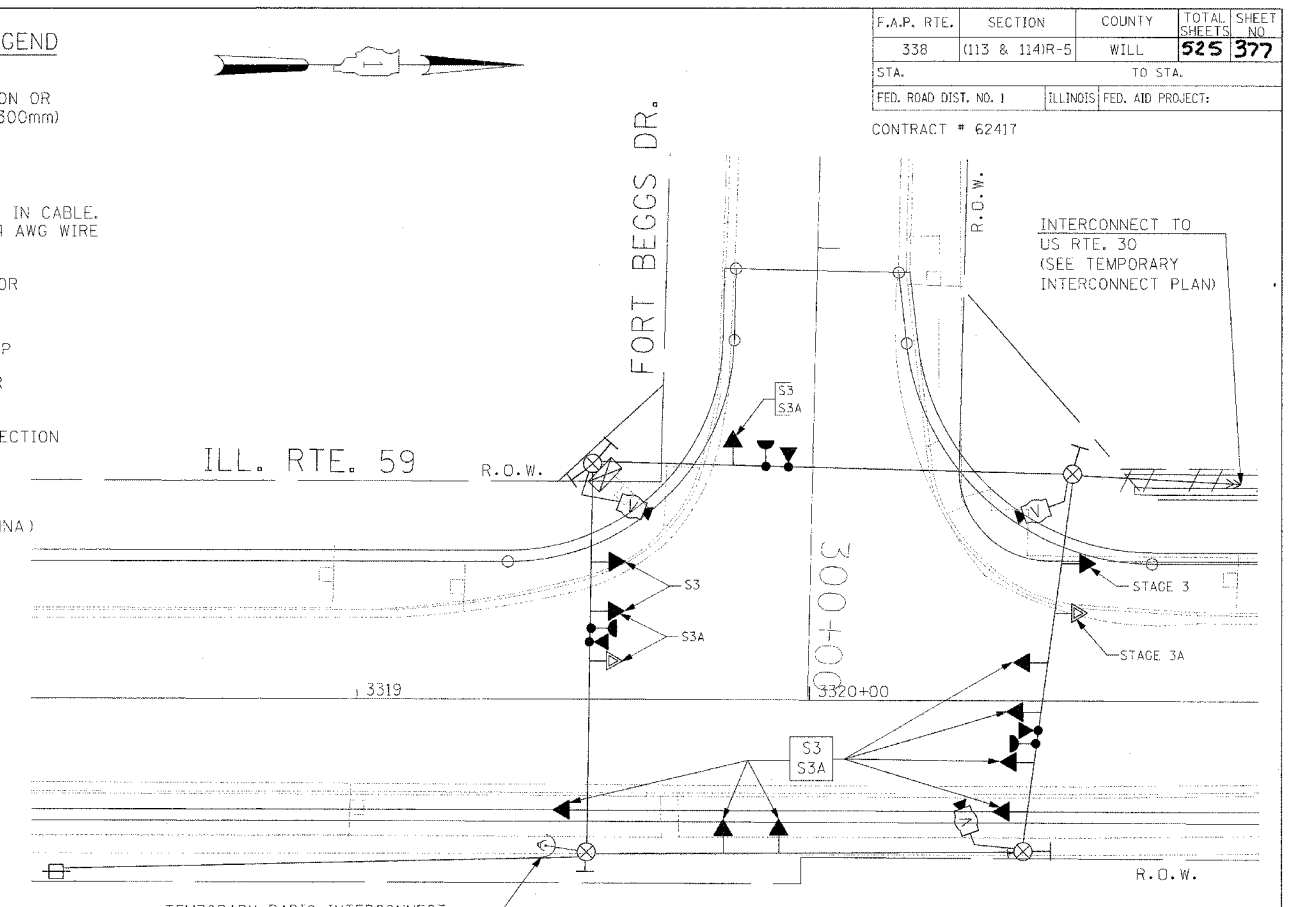
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114)R-5	WILL	525	377
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT:		
CONTRACT # 62417				

TEMPORARY CABLE DIAGRAM LEGEND

- [R] TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12" (300mm)
- [X] TEMPORARY CONTROLLER CABINET
- [+] TEMPORARY SERVICE INSTALLATION
- (5) INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED.
- [V] EMERGENCY VEHICLE LIGHT DETECTOR
- [B] CONFIRMATION BEACON
- [□] VEHICLE DETECTOR, INDUCTION LOOP
- [○] PEDESTRIAN PUSHBUTTON DETECTOR
- [12"] 12" (300mm) PEDESTRIAN SIGNAL SECTION
- [V] VIDEO CAMERA ASSEMBLY
- [<->] WIRELESS INTERCONNECT (ANTENNA)
- [C] CO-AXIAL VIDEO CABLE



TEMPORARY CABLE PLAN (NOT TO SCALE)
STAGES: STAGE 3 AND AFTER
PROPOSED ROADWAY GEOMETRICS
ARE BUILT AND OPEN TO TRAFFIC

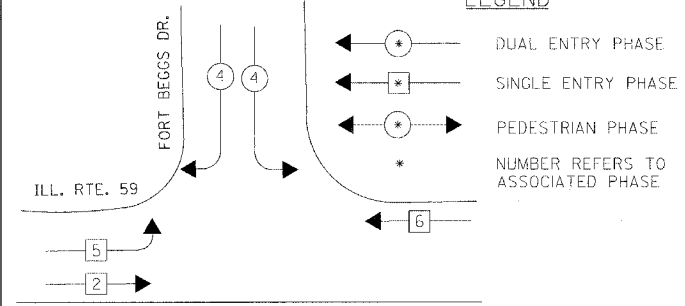


TEMPORARY TRAFFIC SIGNAL PLAN (SCALE 1"=20')
STAGES: STAGE 3 AND AFTER
PROPOSED ROADWAY GEOMETRICS
ARE BUILT AND OPEN TO TRAFFIC

TEMPORARY RADIO INTERCONNECT TO RENWICK RD. AND US RTE. 30 (SEE TEMPORARY INTERCONNECT PLAN)

TEMPORARY RADIO INTERCONNECT TO RENWICK RD. AND US RTE. 30 (SEE TEMPORARY INTERCONNECT PLAN)

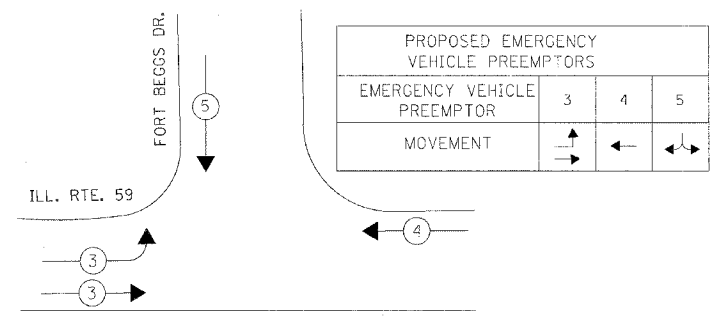
CONTROLLER SEQUENCE



- LEGEND
- [*] DUAL ENTRY PHASE
 - [*] SINGLE ENTRY PHASE
 - [*] PEDESTRIAN PHASE
 - * NUMBER REFERS TO ASSOCIATED PHASE

TEMPORARY PHASE DESIGNATION DIAGRAM STAGES: STAGE 3

EMERGENCY VEHICLE PREEMPTION SEQUENCE



STAGES: STAGE 3

TEMPORARY CABLE PLAN NOTES:

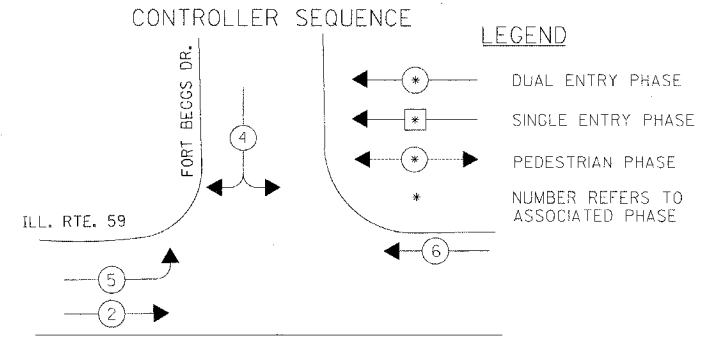
- NOTE 1: DURING CONSTRUCTION STAGE STAGE 3 THESE GREEN AND YELLOW LEFT ARROW INDICATION SECTIONS SHALL BE BAGGED AND DISCONNECTED AT THE CONTROLLER. THEY SHALL BE UNBAGGED AND CONNECTED AFTER CONSTRUCTION STAGE 3 UNTIL PROPOSED TRAFFIC SIGNALS ARE MADE OPERATIONAL.
- NOTE 2: THESE SIGNAL HEADS SHALL BE BAGGED AND DISCONNECTED AT THE CONTROLLER AFTER CONSTRUCTION STAGE 3, WHEN ALL THE PROPOSED ROADWAY GEOMETRICS ARE BUILT AND OPEN TO TRAFFIC. THESE SIGNAL HEADS CAN BE REMOVED AFTER STAGE 3 WITH APPROVAL OF THE ENGINEER.
- NOTE 3: DURING WINTER SHUTDOWN THE SIGNAL HEAD PLACEMENTS SHALL BE SIMILAR TO STAGE 3 OR AS DIRECTED BY THE ENGINEER.

TEMPORARY TRAFFIC SIGNAL LEGEND

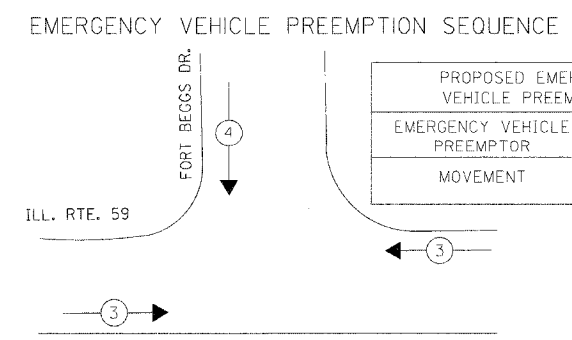
- [<-] TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- [<-] TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- [X] TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- [X] TEMPORARY CONTROLLER CABINET
- [+] TEMPORARY SERVICE INSTALLATION
- [+] TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- [V] MICROWAVE VEHICLE SENSOR
- [○] PEDESTRIAN PUSHBUTTON DETECTOR
- [V] EMERGENCY VEHICLE LIGHT DETECTOR
- [B] CONFIRMATION BEACON
- [□] VEHICLE DETECTOR, INDUCTION LOOP
- CT COMMON TRENCH
- UD UNIT DUCT
- [] G.S. CONDUIT IN GROUND
- [] HANDHOLE
- [] HEAVY DUTY HANDHOLE
- [<->] WIRELESS INTERCONNECT (ANTENNA)
- [V] VIDEO CAMERA ASSEMBLY

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO LAMPS	WATTAGE	%OPERATION		
SIGNAL (RED)	11	135	17	0.50	93.5
(YELLOW)	11	135	25	0.25	68.8
(GREEN)	11	135	15	0.25	41.3
ARROW		90	25	1.00	
PED. SIGNAL		100	100	1.00	100.0
CONTROLLER				0.05	
ILLUM. SIGN					
FLASHER				0.50	
ENERGY COSTS TO:				TOTAL =	303.5

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAMBURG, ILLINOIS 60196-1096
ENERGY SUPPLY CONTACT: MS. BETTY BRULC
PHONE: (815) 724-5052
COMPANY: COMED

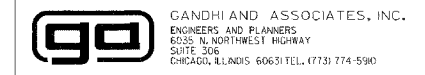


TEMPORARY PHASE DESIGNATION DIAGRAM AFTER PROPOSED ROADWAY GEOMETRICS ARE BUILT AND OPEN TO TRAFFIC



AFTER PROPOSED ROADWAY GEOMETRICS ARE BUILT AND OPEN TO TRAFFIC

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.



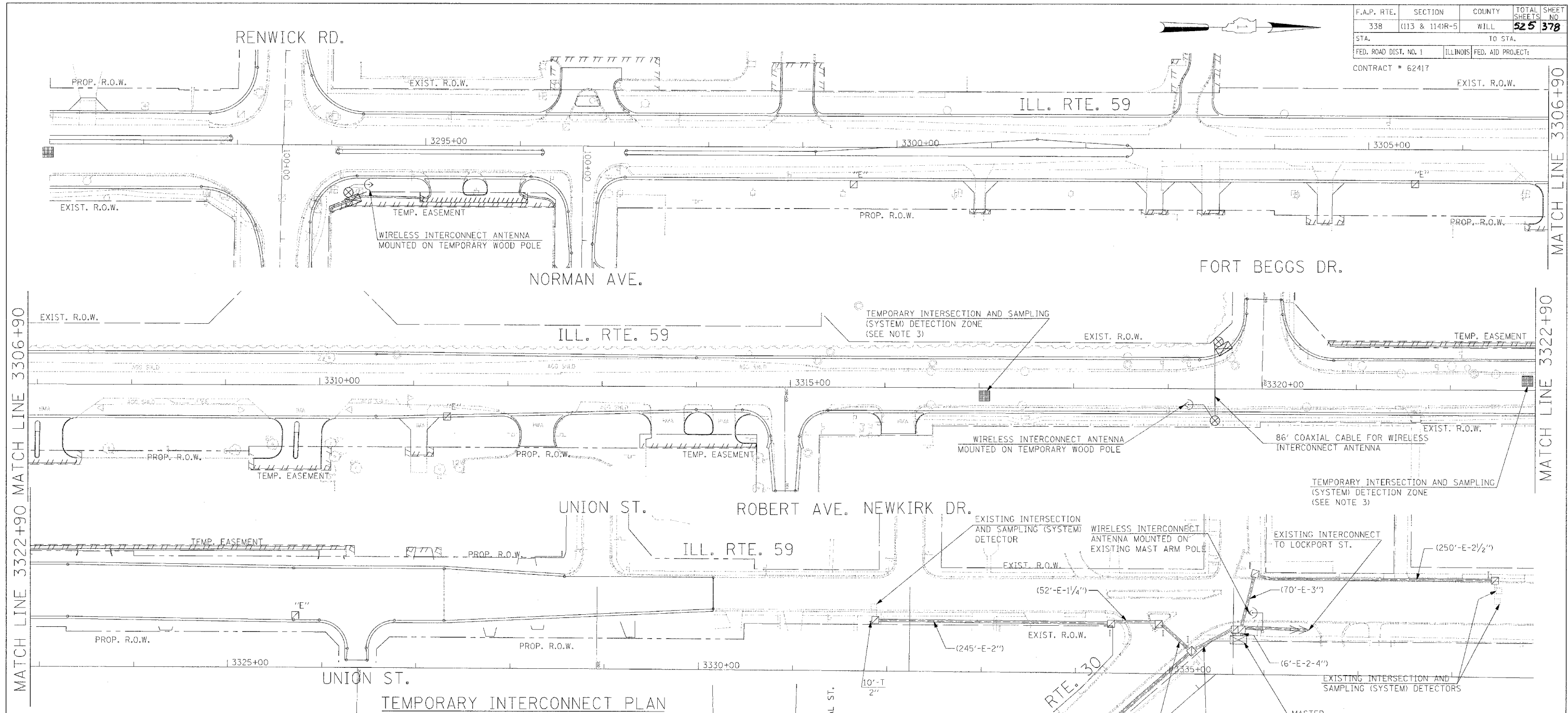
REVISIONS	
NAME	DATE
PKG, MAA	05-01-08

ILLINOIS DEPARTMENT OF TRANSPORTATION
TEMPORARY TRAFFIC SIGNAL INSTALLATION
TEMPORARY CABLE PLAN,
PHASE DESIGNATION DIAGRAM, AND
EMERGENCY VEHICLE PREEMPTION SEQUENCE
ILLINOIS ROUTE 59 AT FORT BEGGS DRIVE
(SHEET 3 OF 3)

SCALE: AS NOTED
DATE: 03/18/2008

DRAWN BY: BB/KP
DESIGNED BY: PKG/RRM
CHECKED BY: PKG/RRM

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114)R-5	WILL	525	378
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT:		
CONTRACT # 62417				

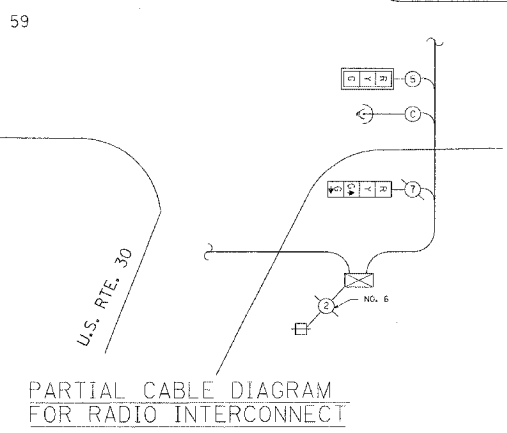


TEMPORARY INTERCONNECT PLAN LEGEND

	PROPOSED	EXISTING
CONTROLLER		
HANDHOLE		
DOUBLE HANDHOLE		
HEAVY DUTY HANDHOLE		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
UNIT DUCT		
SYSTEM		
INTERSECTION		
TELEPHONE CONNECTION		
WOOD POLE		
AERIAL FIBER OPTIC CABLE		
VIDEO DETECTION ZONE		
WIRELESS INTERCONNECT (ANTENNA)		

CABLE PLAN LEGEND

EXISTING	PROPOSED



- NOTES:**
- ALL THE VIDEO DETECTION ZONES SHALL BE REDEFINED FOR EACH CONSTRUCTION STAGE. THIS WORK IS INCIDENTAL TO THE PAY ITEM "TEMPORARY TRAFFIC SIGNAL INSTALLATION".

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

REVISIONS	
NAME	DATE
PKG, MAA	05-01-08

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TEMPORARY INTERCONNECT PLAN
 ILLINOIS ROUTE 59 FROM
 RENWICK ROAD
 TO US ROUTE 30

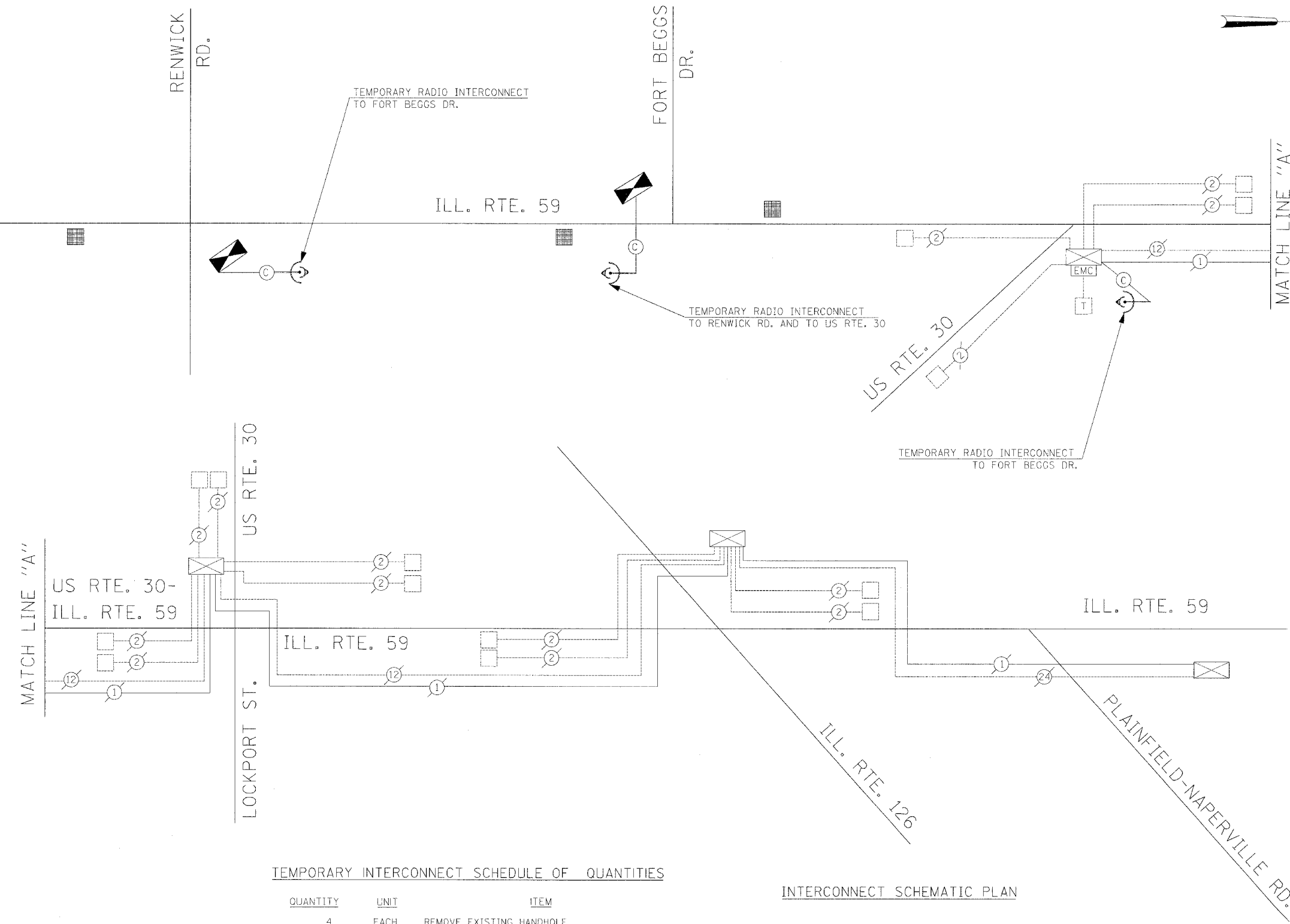
SCALE: 1"=50'
 DATE: 03/18/2008

DRAWN BY: BB,KP
 DESIGNED BY: PKG/RRM
 CHECKED BY: PKG/RRM

GA GANDHI AND ASSOCIATES, INC.
 ENGINEERS AND PLANNERS
 6035 N. NORTHWEST HIGHWAY
 SUITE 306
 CHICAGO, ILLINOIS 60631 TEL. (773) 774-590

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114)R-5	WILL	525	379
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT:			

































CONTRACT # 62417



MATCH LINE "A"

MATCH LINE "A"

INTERCONNECT SCHEMATIC LEGEND

-  EXISTING INTERSECTION CONTROLLER
-  TEMPORARY INTERSECTION CONTROLLER
-  EXISTING MASTER CONTROLLER
-  PROPOSED MASTER CONTROLLER
-  MASTER MASTER CONTROLLER
-  EXISTING INTERSECTION & SAMPLING (SYSTEM) DETECTORS
-  PROPOSED INTERSECTION & SAMPLING (SYSTEM) DETECTORS
-  EXISTING INTERSECTION LOOP DETECTORS AND PROPOSED SAMPLING (SYSTEM) DETECTORS
-  EXISTING SAMPLING (SYSTEM) DETECTORS
-  PROPOSED SAMPLING (SYSTEM) DETECTORS
-  EXISTING SAMPLING (SYSTEM) DETECTORS, PROPOSED INTERSECTION & SAMPLING (SYSTEM) DETECTORS.
-  EXISTING SAMPLING (SYSTEM) DETECTORS, PROPOSED SAMPLING (SYSTEM) DETECTORS.
-  EXISTING PREFORMED INTERSECTION & SAMPLING (SYSTEM) DETECTORS
-  PROPOSED PREFORMED INTERSECTION & SAMPLING (SYSTEM) DETECTORS
-  EXISTING SAMPLING (SYSTEM) PREFORMED DETECTORS.
-  PROPOSED SAMPLING (SYSTEM) PREFORMED DETECTORS.
-  EXISTING FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 MM12F & SM12F
-  PROPOSED FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 MM12F & SM12F
-  EXISTING INTERCONNECT CABLE - NO. 62.5/125 12F. FIBER OPTIC CABLE
-  PROPOSED INTERCONNECT CABLE - NO. 62.5/125 12F FIBER OPTIC CABLE
-  EXISTING INTERCONNECT CABLE - NO. 18 3 PAIR TWISTED, SHIELDED
-  PROPOSED INTERCONNECT CABLE - NO. 18 3 PAIR TWISTED, SHIELDED
-  EXISTING LOOP DETECTOR CABLE - 2/C TWISTED, SHIELDED
-  PROPOSED LOOP DETECTOR CABLE - 2/C TWISTED, SHIELDED
-  EXISTING ELECTRIC CABLE 1/C (AS SPECIFIED)
-  PROPOSED ELECTRIC CABLE, 1/C (AS SPECIFIED)
-  PROPOSED ELECTRIC CABLE, 1/C (AS SPECIFIED)
-  EXISTING TELEPHONE CONNECTION
-  PROPOSED TELEPHONE CONNECTION
-  WIRELESS INTERCONNECT (ANTENNA)
-  PROPOSED TEMPORARY INTERCONNECT CABLE - NO. 62.5/125 12F FIBER OPTIC CABLE
-  TEMPORARY INTERSECTION & SAMPLING (SYSTEM) DETECTION ZONE

TEMPORARY INTERCONNECT SCHEDULE OF QUANTITIES

QUANTITY	UNIT	ITEM
4	EACH	REMOVE EXISTING HANDHOLE
1	L SUM	TEMPORARY TRAFFIC SIGNAL INTERCONNECTION SYSTEM

INTERCONNECT SCHEMATIC PLAN

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

REVISIONS	
NAME	DATE
PKG, MAA	05-01-08

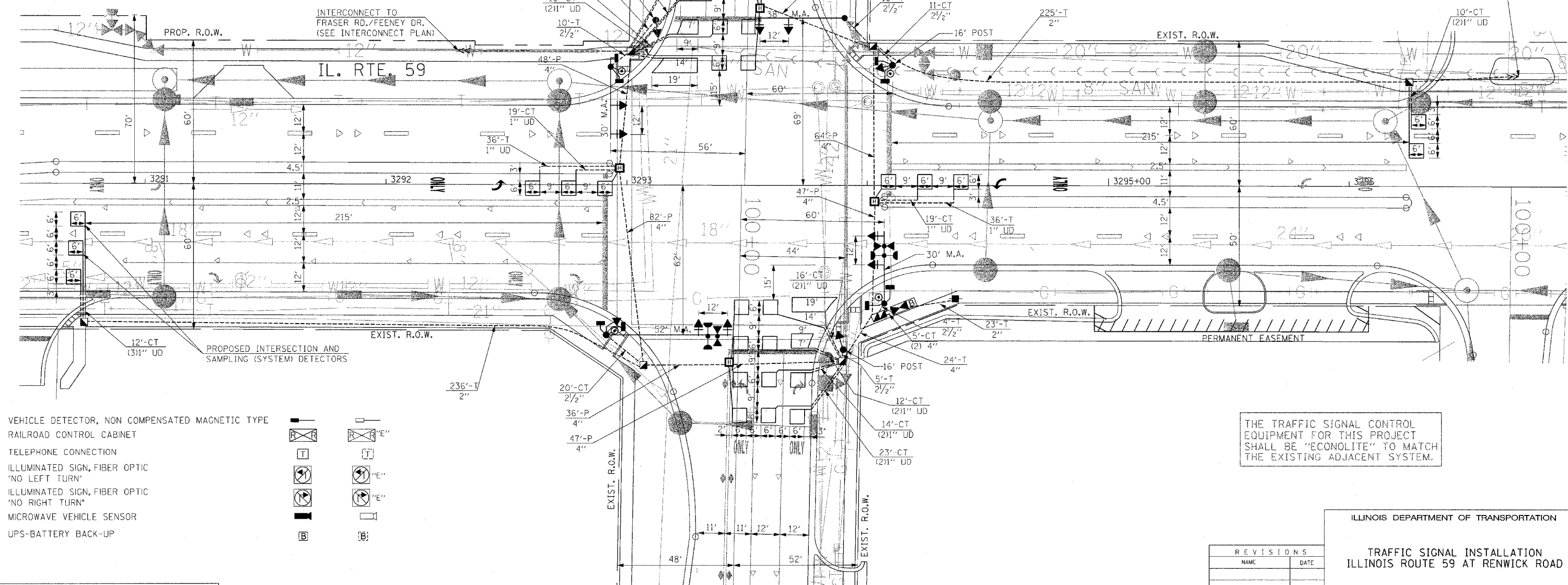
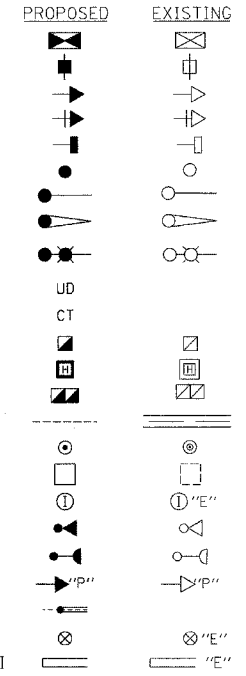
GO GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6030 N. NORTHWEST HIGHWAY
SUITE 306
CHICAGO, ILLINOIS 60631 TEL. (773) 774-5910

ILLINOIS DEPARTMENT OF TRANSPORTATION
TEMPORARY INTERCONNECT SCHEMATIC AND SCHEDULE OF QUANTITIES
ILLINOIS ROUTE 59
FROM RENWICK ROAD TO
PLAINFIELD-NAPERVILLE ROAD

SCALE: N.T.S. DRAWN BY: BB/KP
DATE: 03/18/2008 DESIGNED BY: PKG/RRM
CHECKED BY: PKG/RRM

TRAFFIC SIGNAL LEGEND

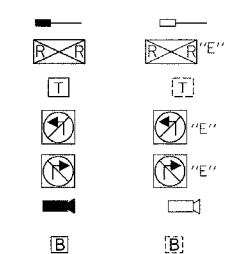
- CONTROLLER
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD WITH BACKPLATE
- SIGNAL HEAD, PEDESTRIAN
- SIGNAL POST
- MAST ARM ASSEMBLY AND POLE, STEEL
- MAST ARM ASSEMBLY AND POLE, ALUMINUM
- COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE
- UNIT DUCT
- COMMON TRENCH
- HANDHOLE
- HEAVY DUTY HANDHOLE
- DOUBLE HANDHOLE
- GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED
- PEDESTRIAN PUSHBUTTON DETECTOR
- DETECTOR LOOP
- CAST IRON JUNCTION BOX
- EMERGENCY VEHICLE SYSTEM DETECTOR
- CONFIRMATION BEACON
- SIGNAL HEAD, OPTICALLY PROGRAMMED
- CONDUIT SPLICE
- WOOD POLE
- RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II



TRAFFIC SIGNAL PLAN

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114)R-5	WILL	525	380
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT:			
CONTRACT # 62417				

- VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE
- RAILROAD CONTROL CABINET
- TELEPHONE CONNECTION
- ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
- ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"
- MICROWAVE VEHICLE SENSOR
- UPS-BATTERY BACK-UP



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL INSTALLATION
ILLINOIS ROUTE 59 AT RENWICK ROAD

SCALE: 1"=20'
DATE: 03/18/2008

DRAWN BY: BB,KP
DESIGNED BY: PKG/RRM
CHECKED BY: PKG/RRM

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114)R-5	WILL	525	381
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT:			

CONTRACT # 62417

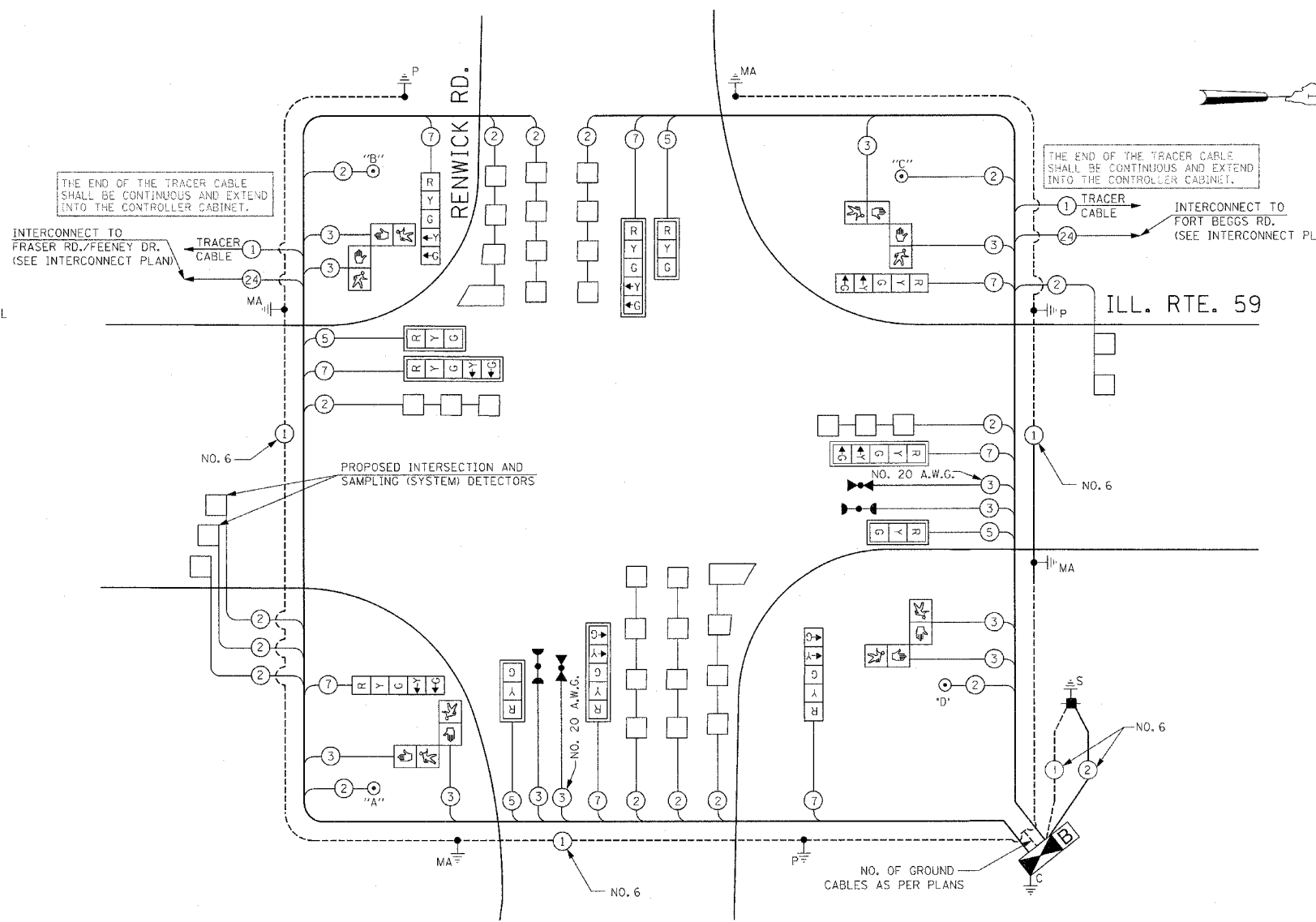
CABLE PLAN LEGEND

EXISTING	PROPOSED	DESCRIPTION
		8' (200mm) TRAFFIC SIGNAL SECTION
		12' (300mm) TRAFFIC SIGNAL SECTION
		12' (300mm) PEDESTRIAN SIGNAL SECTION
		CONTROLLER CABINET
		SERVICE INSTALLATION
		TELEPHONE INSTALLATION
		VEHICLE DETECTOR, INDUCTION LOOP
		MAGNETIC DETECTOR
		EMERGENCY VEHICLE LIGHT DETECTOR
		CONFIRMATION BEACON
		PUSH-BUTTON DETECTOR
		DENOTES NUMBER OF CONDUCTORS, ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED.
		GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)
		FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 2-MM12F & SM12F
		SIGNAL FACE WITH BACKPLATE. *P* INDICATES PROGRAMMED HEAD.
		RAILROAD CONTROL CABINET
		ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
		ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"
		GROUND ROD AT HANDHOLE, DOUBLE HANDHOLE, OR CONTROLLER
		GROUND ROD AT POST OR MAST ARM POLE
		GROUND ROD AT ELECTRIC SERVICE INSTALLATION
		LOCAL AND MASTER CONTROLLER
		MICROWAVE VEHICLE SENSOR
		UPS-BATTERY BACK-UP

SCHEDULE OF QUANTITIES

QUANTITY	UNIT	ITEM
31.5	SQ FT	SIGN PANEL - TYPE 1
488	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
82	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL
34	FOOT	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL
49	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL
324	FOOT	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL
6	EACH	HANDHOLE
4	EACH	HEAVY-DUTY HANDHOLE
1	EACH	DOUBLE HANDHOLE
813	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
1	EACH	TRANSCEIVER - FIBER OPTIC
382	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
770	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
856	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
1671	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
2865	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
42	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C
3	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.
2	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 30 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 38 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 52 FT.
12	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
60	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
4	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED
4	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED
4	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED
1	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED
8	EACH	TRAFFIC SIGNAL BACKPLATE
12	EACH	INDUCTIVE LOOP DETECTOR
* 2	EACH	LIGHT DETECTOR
* 1	EACH	LIGHT DETECTOR AMPLIFIER
3	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
10	EACH	REMOVE EXISTING HANDHOLE
9	EACH	REMOVE EXISTING CONCRETE FOUNDATION
1239	FOOT	PREFORMED DETECTOR LOOP
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING
1	EACH	SERVICE INSTALLATION - POLE MOUNTED
1	EACH	UNINTERRUPTIBLE POWER SUPPLY
799	FOOT	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C
* 313	FOOT	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED

* 100% COST TO THE VILLAGE OF PLAINFIELD

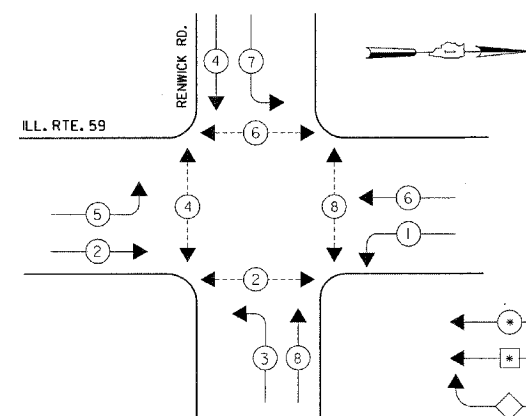


PUSH-BUTTON NOTE:
 PUSH-BUTTON "A" SHALL PLACE A CALL IN PHASES 2 AND 4
 PUSH-BUTTON "B" SHALL PLACE A CALL IN PHASES 4 AND 6
 PUSH-BUTTON "C" SHALL PLACE A CALL IN PHASES 6 AND 8
 PUSH-BUTTON "D" SHALL PLACE A CALL IN PHASES 2 AND 8

CABLE PLAN

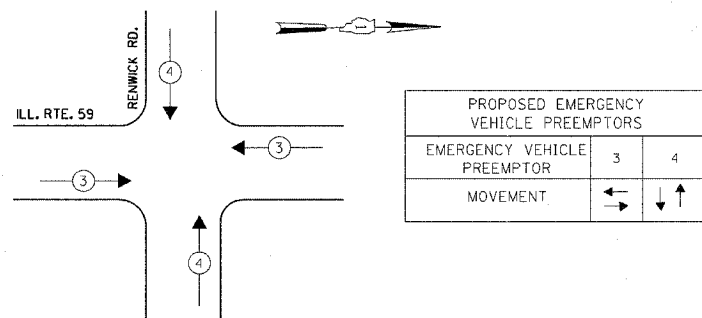
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

CONTROLLER SEQUENCE



PHASE DESIGNATION DIAGRAM

EMERGENCY VEHICLE PREEMPTION SEQUENCE



I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO LAMPS	WATTAGE	%OPERATION		
SIGNAL (RED)	12	135	17	0.50	102.0
(YELLOW)	12	135	25	0.25	75.0
(GREEN)	12	135	15	0.25	45.0
ARROW	16	135	12	0.10	19.2
PED. SIGNAL	4	90	25	1.00	100.0
CONTROLLER	1	100	100	1.00	100.0
ILLUM. SIGN				0.05	
FLASHER				0.50	
TOTAL =					441.2

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	VERTICAL	FT. (m)
TYPE A-POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS	3.5 (1.0)
D-CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	20'+L-2'
E-MAST ARM POLE		SIGNAL POST	2 (0.6)	(6m+L-0.6m)±	
24" (600mm)	10 (3.0)	CONTROLLER CAB.	1 (0.5)	BRACKET MOUNTED	13 (4.0)
30" (750mm)	15 (4.6)	FIBER OPTIC	13 (4.0)	PED. PUSHBUTTON	4 (1.2)
		ELECTRIC SERVICE	1 (0.5)	ELECTRIC SERVICE	13.5 (4.1)
		GROUND CABLE	1 (0.5)	SERVICE TO GROUND	13.5 (4.1)
				POST MOUNTED	6 (1.8)

GO GANDHI AND ASSOCIATES, INC.
 ENGINEERS AND PLANNERS
 6035 N. NORTHWEST HIGHWAY
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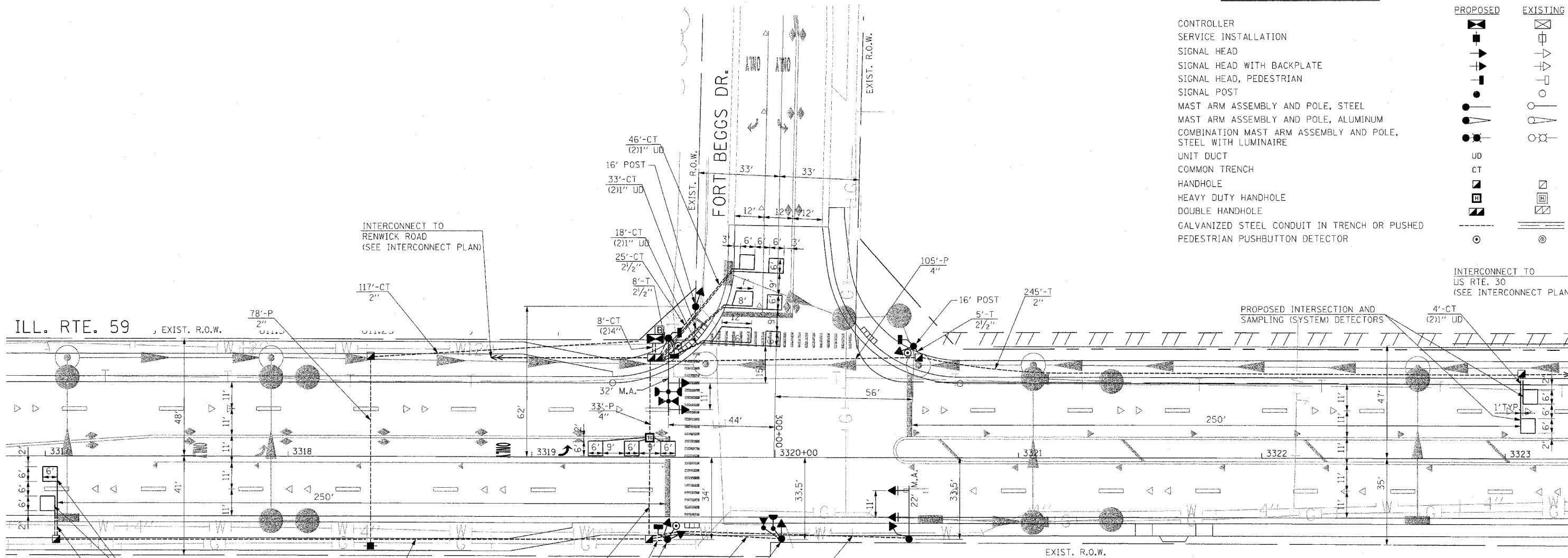
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**CABLE PLAN
 PHASE DESIGNATION DIAGRAM
 EMERGENCY VEHICLE PREEMPTION SEQUENCE
 AND SCHEDULE OF QUANTITIES
 ILLINOIS ROUTE 59 AT RENWICK ROAD**
 SCALE: N.T.S. DATE: 03/18/2008
 DRAWN BY: BB, KP
 DESIGNED BY: PKG/RRM
 CHECKED BY: PKG/RRM



TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
CONTROLLER		
SERVICE INSTALLATION		
SIGNAL HEAD		
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD, PEDESTRIAN		
SIGNAL POST		
MAST ARM ASSEMBLY AND POLE, STEEL		
MAST ARM ASSEMBLY AND POLE, ALUMINUM		
COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE		
UNIT DUCT		
COMMON TRENCH		
HANDHOLE		
HEAVY DUTY HANDHOLE		
DOUBLE HANDHOLE		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
PEDESTRIAN PUSHBUTTON DETECTOR		



TRAFFIC SIGNAL PLAN

DETECTOR LOOP		
CAST IRON JUNCTION BOX		
EMERGENCY VEHICLE SYSTEM DETECTOR		
CONFIRMATION BEACON		
SIGNAL HEAD, OPTICALLY PROGRAMMED		
CONDUIT SPLICE		
WOOD POLE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RAILROAD CONTROL CABINET		
TELEPHONE CONNECTION		
ILLUMINATED SIGN, FIBER OPTIC 'NO LEFT TURN'		
ILLUMINATED SIGN, FIBER OPTIC 'NO RIGHT TURN'		
MICROWAVE VEHICLE SENSOR		
UPS-BATTERY BACK-UP		

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL INSTALLATION
ILLINOIS ROUTE 59 AT
FORT BEGGS DRIVE

SCALE: 1"=20'
DATE: 03/18/2008

DRAWN BY: BB, KP
DESIGNED BY: PKG/RRM
CHECKED BY: PKG/RRM

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114)R-5	WILL	525	303
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT:		
CONTRACT # 62417				

SCHEDULE OF QUANTITIES

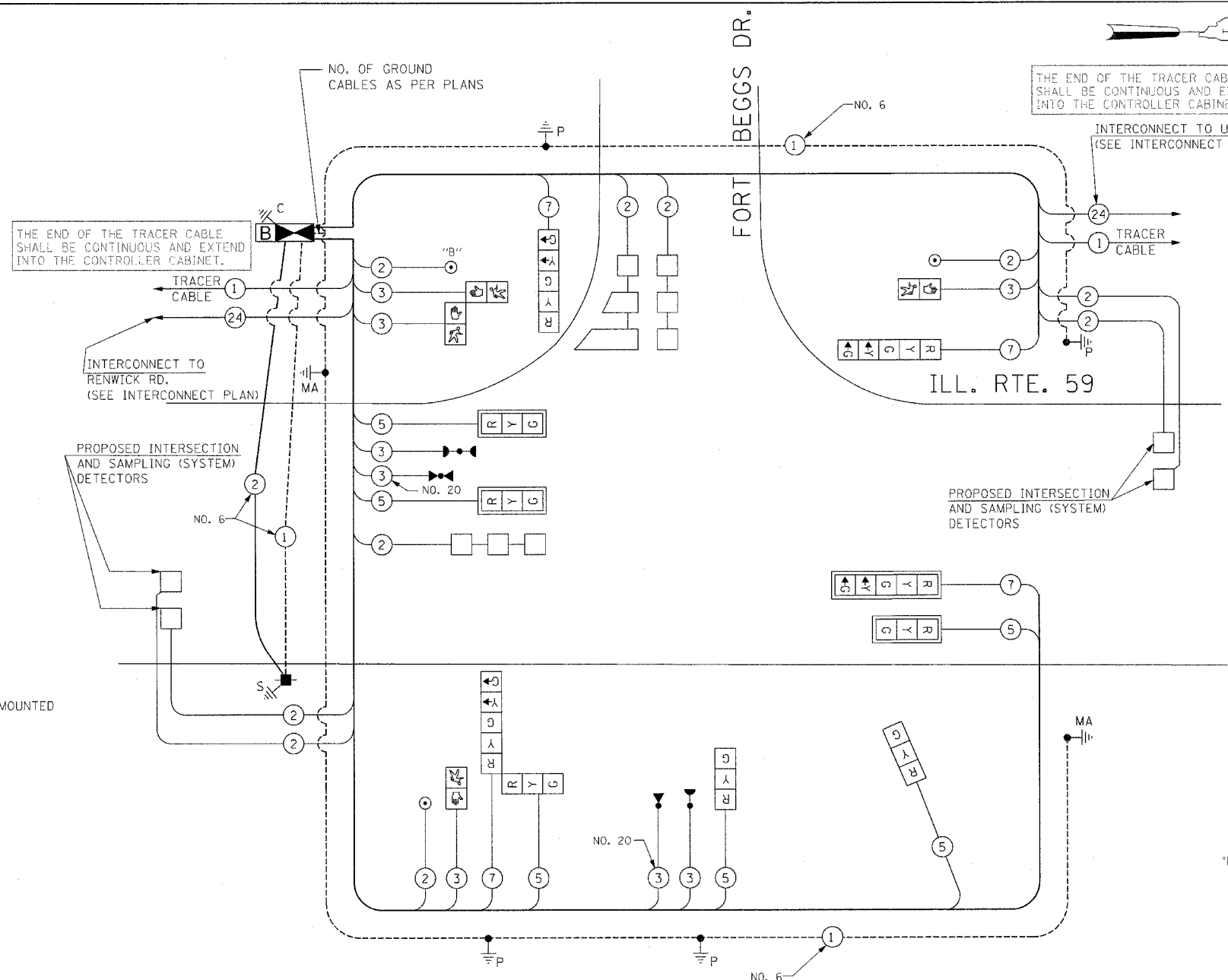
QUANTITY	UNIT	ITEM
25.5	SO FT	SIGN PANEL - TYPE 1
605	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
208	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL
16	FOOT	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL
78	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL
180	FOOT	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL
4	EACH	HANDHOLE
1	EACH	HEAVY-DUTY HANDHOLE
1	EACH	DOUBLE HANDHOLE
724	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
1	EACH	TRANSCEIVER - FIBER OPTIC
326	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
675	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
988	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
636	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
1619	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
229	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C
3	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 18 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 22 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.
16	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
15	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
15	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
3	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED
2	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
2	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED
1	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
1	EACH	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED
1	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED
4	EACH	TRAFFIC SIGNAL BACKPLATE
7	EACH	INDUCTIVE LOOP DETECTOR
* 2	EACH	LIGHT DETECTOR
* 1	EACH	LIGHT DETECTOR AMPLIFIER
3	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
5	EACH	REMOVE EXISTING HANDHOLE
3	EACH	REMOVE EXISTING CONCRETE FOUNDATION
457	FOOT	PREFORMED DETECTOR LOOP
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING
1	EACH	SERVICE INSTALLATION - POLE MOUNTED
1	EACH	UNINTERRUPTIBLE POWER SUPPLY
746	FOOT	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C
* 270	FOOT	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED

* 100% COST TO VILLAGE OF PLAINFIELD

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO LAMPS	WATTAGE	OPERATION		
SIGNAL (RED)	10	135	17	0.50	85.0
(YELLOW)	10	135	25	0.25	62.5
(GREEN)	10	135	15	0.25	37.5
ARROW	8	135	12	0.10	9.6
PED. SIGNAL	4	90	25	1.00	100.0
CONTROLLER ILLUM. SIGN	1	100	100	1.00	100.0
				0.05	
FLASHER				0.50	
ENERGY COSTS TO:				TOTAL =	394.6
ILLINOIS DEPARTMENT OF TRANSPORTATION 201 WEST CENTER COURT SCHAUMBURG, ILLINOIS 60196-1096					
ENERGY SUPPLY CONTACT: MS. BETTY BRULC PHONE: (815) 724-5052 COMPANY: COMED					

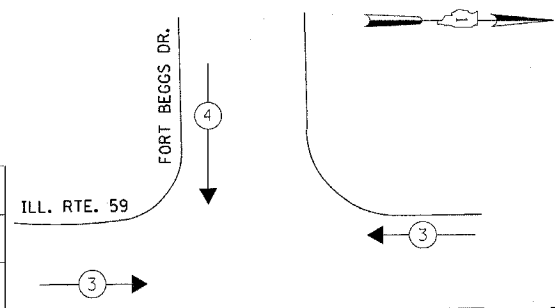
PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	←→	↔

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	VERTICAL	FT. (m)
TYPE A-POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS	3.5 (1.0)
D-CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	20'-L-25"
E-MAST ARM POLE		SIGNAL POST	2 (1.0)		(6m+L-0.6m)
24" (6.00mm)	10 (3.0)	CONTROLLER CAB.	1 (0.5)	BRACKET MOUNTED	13 (4.0)
30" (7.50mm)	15 (4.6)	FIBER OPTIC	13 (4.0)	PED. PUSHBUTTON	4 (1.2)
		ELECTRIC SERVICE	1 (0.5)	ELECTRIC SERVICE	13.5 (4.1)
		GROUND CABLE	1 (0.5)	SERVICE TO GROUND	13.5 (4.1)
				POST MOUNTED	6 (1.8)

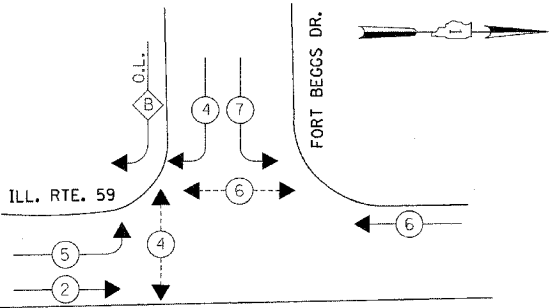


CABLE PLAN

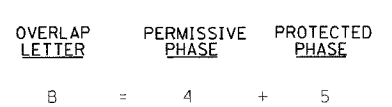
EMERGENCY VEHICLE PREEMPTION SEQUENCE



CONTROLLER SEQUENCE



PHASE DESIGNATION DIAGRAM



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

CABLE PLAN LEGEND

EXISTING	PROPOSED
[Symbol]	[Symbol] 8" (200mm) TRAFFIC SIGNAL SECTION
[Symbol]	[Symbol] 12" (300mm) TRAFFIC SIGNAL SECTION
[Symbol]	[Symbol] 12" (300mm) PEDESTRIAN SIGNAL SECTION
[Symbol]	[Symbol] 12" (300mm) PEDESTRIAN SIGNAL SECTION
[Symbol]	[Symbol] CONTROLLER CABINET
[Symbol]	[Symbol] SERVICE INSTALLATION
[Symbol]	[Symbol] TELEPHONE INSTALLATION
[Symbol]	[Symbol] VEHICLE DETECTOR, INDUCTION LOOP
[Symbol]	[Symbol] MAGNETIC DETECTOR
[Symbol]	[Symbol] EMERGENCY VEHICLE LIGHT DETECTOR
[Symbol]	[Symbol] CONFIRMATION BEACON
[Symbol]	[Symbol] PUSH-BUTTON DETECTOR
[Symbol]	[Symbol] DENOTES NUMBER OF CONDUCTORS. ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED.
[Symbol]	[Symbol] ① GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)
[Symbol]	[Symbol] ②④ FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 2-MM12F & SM12F
[Symbol]	[Symbol] SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD.
[Symbol]	[Symbol] RAILROAD CONTROL CABINET
[Symbol]	[Symbol] ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
[Symbol]	[Symbol] ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"
[Symbol]	[Symbol] GROUND ROD AT HANDHOLE, DOUBLE HANDHOLE, OR CONTROLLER
[Symbol]	[Symbol] GROUND ROD AT POST OR MAST ARM POLE
[Symbol]	[Symbol] GROUND ROD AT ELECTRIC SERVICE INSTALLATION
[Symbol]	[Symbol] LOCAL AND MASTER CONTROLLER
[Symbol]	[Symbol] MICROWAVE VEHICLE SENSOR
[Symbol]	[Symbol] UPS-BATTERY BACK-UP

PUSH-BUTTON NOTE:
PUSH-BUTTON "B" SHALL PLACE A CALL IN PHASES 4 AND 6

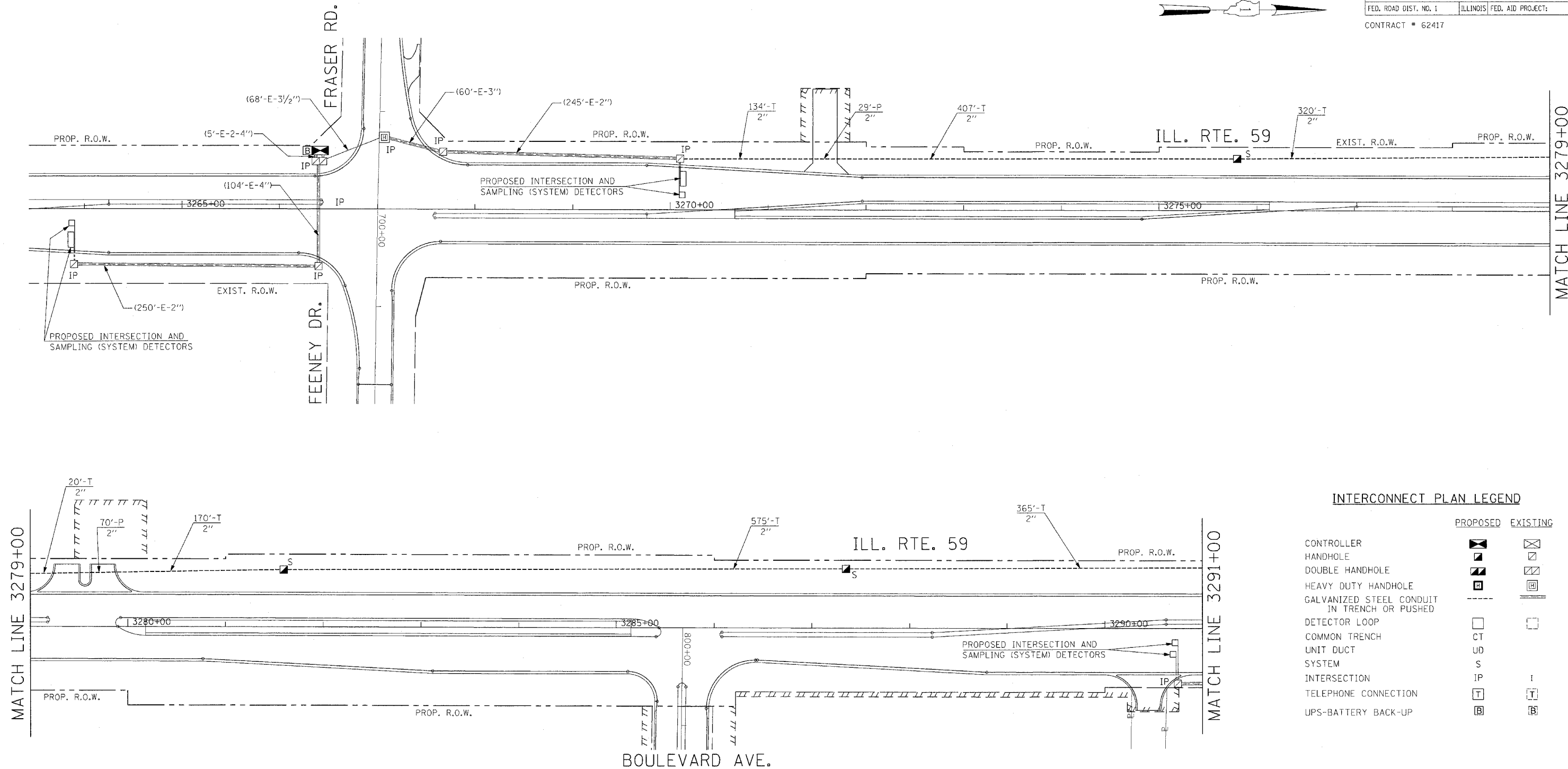
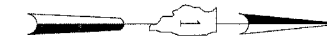
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
CABLE PLAN, PHASE DESIGNATION DIAGRAM,
EMERGENCY VEHICLE PREEMPTION SEQUENCE
AND SCHEDULE OF QUANTITIES
ILLINOIS ROUTE 59 AT FORT BEGGS DRIVE

SCALE: N.T.S. DATE: 03/18/2008

DRAWN BY: BB, KP
DESIGNED BY: PKG/RRM
CHECKED BY: PKG/RRM

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114)R-5	WILL.	525	384
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT:			
CONTRACT = 62417				



INTERCONNECT PLAN LEGEND

	PROPOSED	EXISTING
CONTROLLER		
HANDHOLE		
DOUBLE HANDHOLE		
HEAVY DUTY HANDHOLE		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
COMMON TRENCH	CT	
UNIT DUCT	UD	
SYSTEM	S	
INTERSECTION	IP	I
TELEPHONE CONNECTION		
UPS-BATTERY BACK-UP		

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

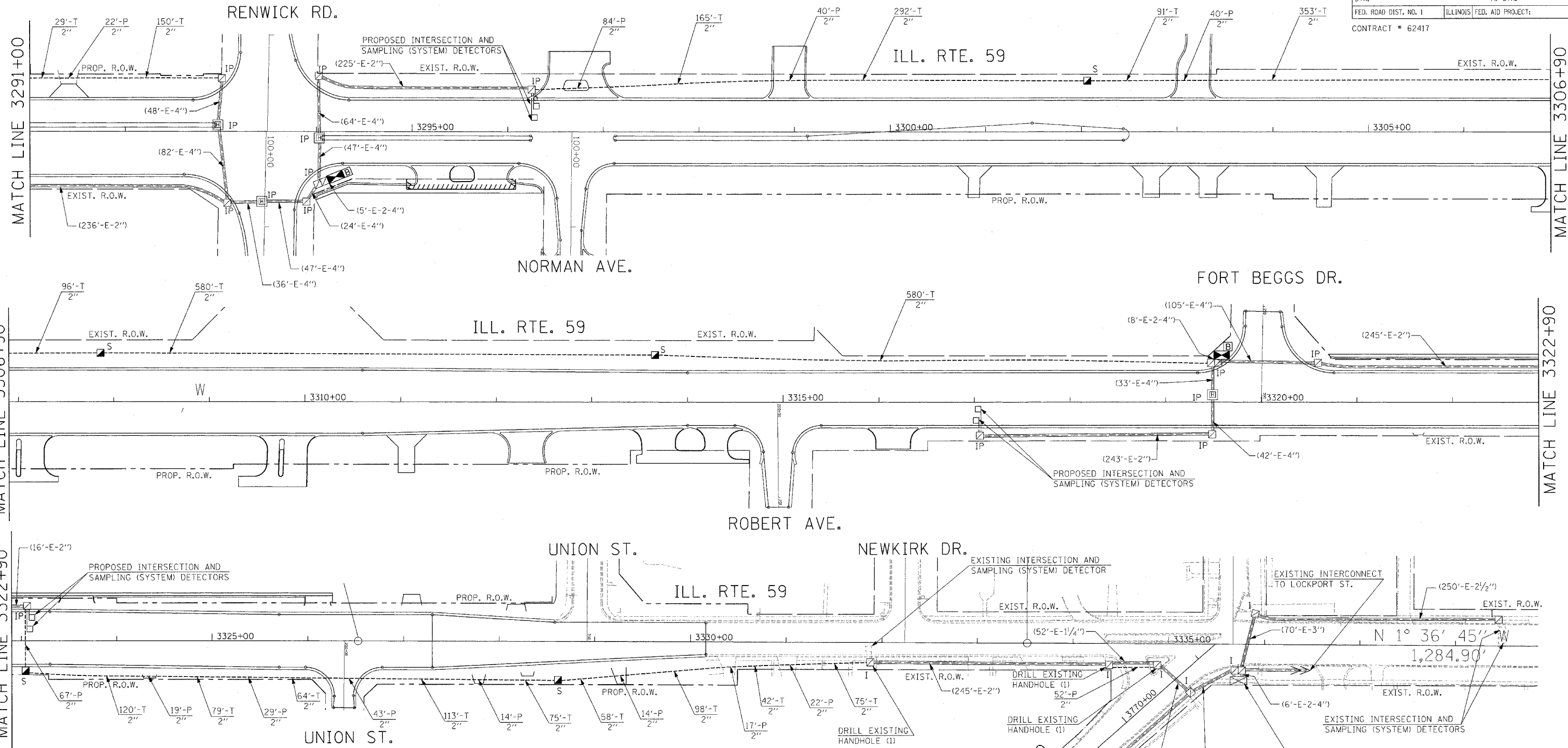
ILLINOIS DEPARTMENT OF TRANSPORTATION
 PROPOSED INTERCONNECT PLAN
 ILLINOIS ROUTE 59 FROM
 FRASER ROAD/FEENEY DRIVE
 TO US ROUTE 30
 (SHEET 1 OF 2)

REVISIONS	
NAME	DATE

ga GANDHI AND ASSOCIATES, INC.
 ENGINEERS AND PLANNERS
 6035 N. NORTHWEST HIGHWAY
 SUITE 306
 CHICAGO, ILLINOIS 60631 TEL. (773) 774-5900

SCALE: 1"=50'
 DATE: 03/18/2008
 DRAWN BY: BB/KP
 DESIGNED BY: PKG/RRM
 CHECKED BY: PKG/RRM

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114)R-5	WILL	525	385
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT:		
CONTRACT # 62417				



INTERCONNECT PLAN LEGEND

	PROPOSED	EXISTING
CONTROLLER		
HANDHOLE		
DOUBLE HANDHOLE		
HEAVY DUTY HANDHOLE		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
UNIT DUCT	UD	
SYSTEM	S	
INTERSECTION	IP	I
TELEPHONE CONNECTION	T	T
UPS-BATTERY BACK-UP	B	B

INTERCONNECT PLAN

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

ILLINOIS DEPARTMENT OF TRANSPORTATION
 PROPOSED INTERCONNECT PLAN
 ILLINOIS ROUTE 59 FROM
 FRASER ROAD/FEENEY DRIVE
 TO US ROUTE 30
 (SHEET 2 OF 2)

REVISIONS

NAME	DATE





























SCALE: 1"=50'
 DATE: 03/18/2008
 DRAWN BY: BB/KP
 DESIGNED BY: PKG/RRM
 CHECKED BY: PKG/RRM

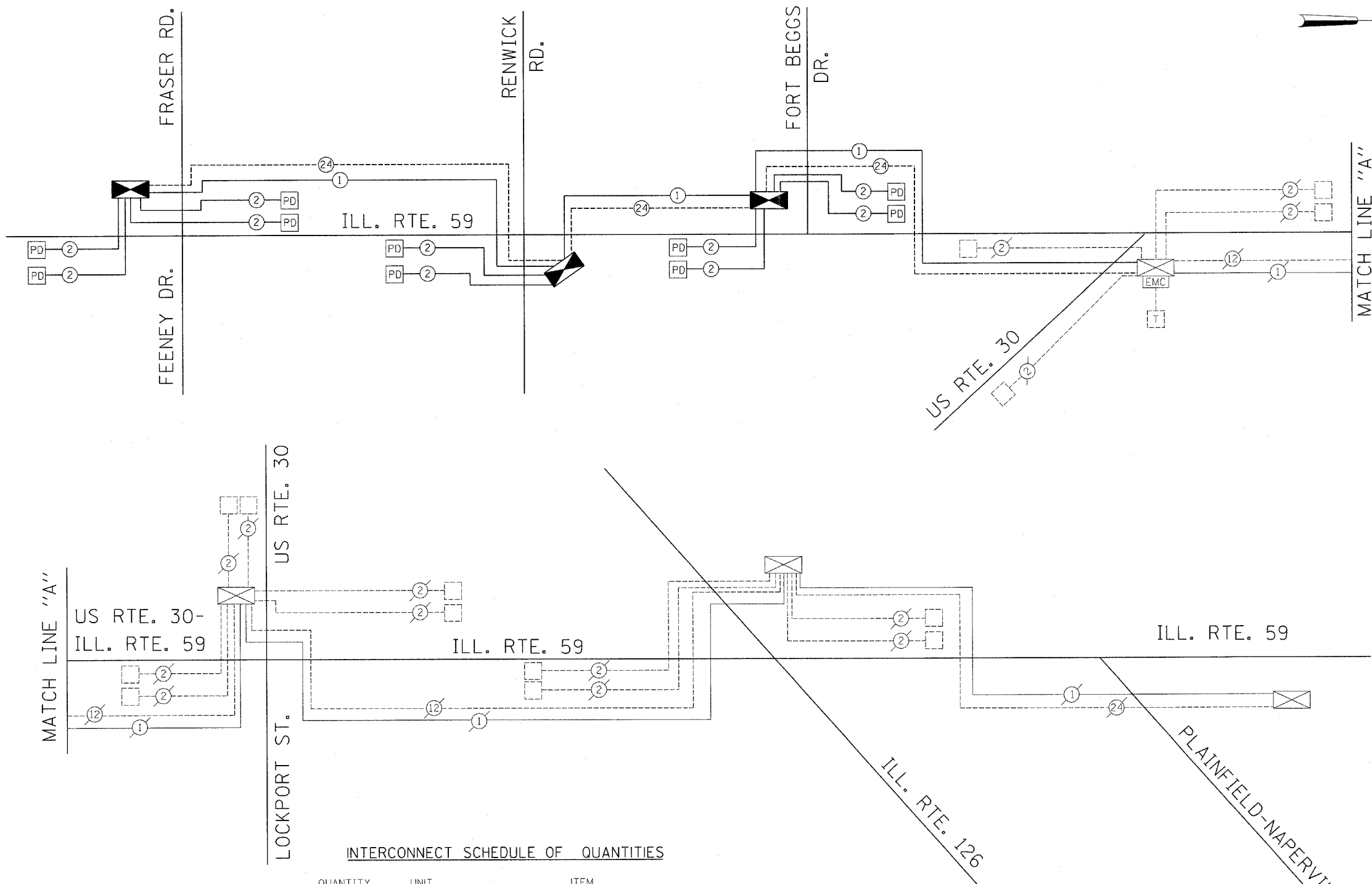
99 GANDHI AND ASSOCIATES, INC.
 ENGINEERS AND PLANNERS
 6035 N. NORTHWEST HIGHWAY
 SUITE 306
 CHICAGO, ILLINOIS 60631 TEL. (773) 774-5900

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114)R-5	WILL	525	386
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT:		

CONTRACT # 62417

INTERCONNECT SCHEMATIC LEGEND

-  EXISTING INTERSECTION CONTROLLER
-  PROPOSED INTERSECTION CONTROLLER
-  EXISTING MASTER CONTROLLER
-  PROPOSED MASTER CONTROLLER
-  MASTER MASTER CONTROLLER
-  EXISTING INTERSECTION & SAMPLING (SYSTEM) DETECTORS
-  PROPOSED INTERSECTION & SAMPLING (SYSTEM) DETECTORS
-  EXISTING INTERSECTION LOOP DETECTORS AND PROPOSED SAMPLING (SYSTEM) DETECTORS
-  EXISTING SAMPLING (SYSTEM) DETECTORS
-  PROPOSED SAMPLING (SYSTEM) DETECTORS
-  EXISTING SAMPLING (SYSTEM) DETECTORS, PROPOSED INTERSECTION & SAMPLING (SYSTEM) DETECTORS.
-  EXISTING SAMPLING (SYSTEM) DETECTORS, PROPOSED SAMPLING (SYSTEM) DETECTORS.
-  EXISTING PREFORMED INTERSECTION & SAMPLING (SYSTEM) DETECTORS
-  PROPOSED PREFORMED INTERSECTION & SAMPLING (SYSTEM) DETECTORS
-  EXISTING SAMPLING (SYSTEM) PREFORMED DETECTORS.
-  PROPOSED SAMPLING (SYSTEM) PREFORMED DETECTORS.
-  EXISTING FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 MM12F & SM12F
-  PROPOSED FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 MM12F & SM12F
-  EXISTING INTERCONNECT CABLE - NO. 62.5/125 12F. FIBER OPTIC CABLE
-  PROPOSED INTERCONNECT CABLE - NO. 62.5/125 12F. FIBER OPTIC CABLE
-  EXISTING INTERCONNECT CABLE - NO. 18 3 PAIR TWISTED, SHIELDED
-  PROPOSED INTERCONNECT CABLE - NO. 18 3 PAIR TWISTED, SHIELDED
-  EXISTING LOOP DETECTOR CABLE - 2/C TWISTED, SHIELDED
-  PROPOSED LOOP DETECTOR CABLE - 2/C TWISTED, SHIELDED
-  EXISTING ELECTRIC CABLE 1/C (AS SPECIFIED)
-  PROPOSED ELECTRIC CABLE, 1/C (AS SPECIFIED)
-  EXISTING TELEPHONE CONNECTION
-  PROPOSED TELEPHONE CONNECTION



INTERCONNECT SCHEDULE OF QUANTITIES

QUANTITY	UNIT	ITEM
5051	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
562	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL
8	EACH	HANDHOLE
5051	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
3	EACH	DRILL EXISTING HANDHOLE
1	EACH	MODIFY EXISTING CONTROLLER
7550	FOOT	ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1C
7628	FOOT	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F
1	EACH	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM - LEVEL II

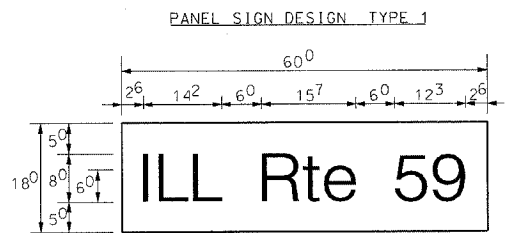
INTERCONNECT SCHEMATIC PLAN

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

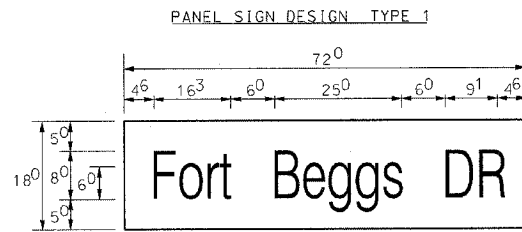
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
INTERCONNECT SCHEMATIC AND SCHEDULE OF QUANTITIES
ILLINOIS ROUTE 59
FROM FRASER RD./FEENEY DR. TO PLAINFIELD-NAPERVILLE RD.
 SCALE: N.T.S.
 DATE: 03/18/2008
 DESIGNED BY: PKG/RRM
 CHECKED BY: PKG/RRM
 DRAWN BY: BB/KP

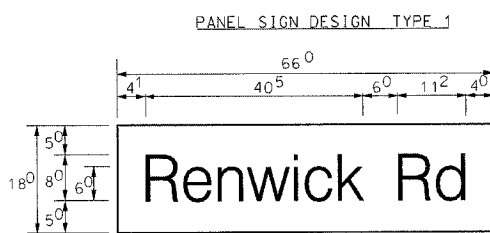
GO GANDHI AND ASSOCIATES, INC.
 ENGINEERS AND PLANNERS
 6035 N. NORTHWEST HIGHWAY
 SUITE 308
 CHICAGO, ILLINOIS 60631 TEL: (773) 774-5900



7.5 Sq. Ft. each
3 Required
Design Series D



9.0 Sq. Ft. each
2 Required
Design Series C



8.25 Sq. Ft. each
2 Required
Design Series D

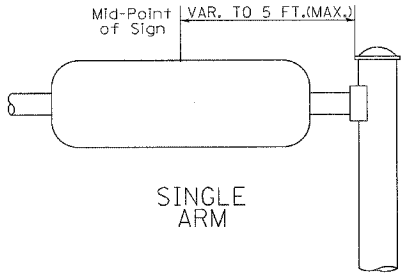
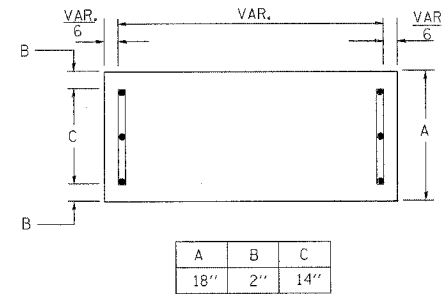
GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 834001, 834006 AND 834011, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 6'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
- THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 6'-0".
- ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:
 * A.K.T. CORPORATION
 SCHAUMBURG, IL
 * TUCKER COMPANY, INC.
 WAUWATOSA, WI
 * AMERICAN FABRICATION CO.
 CHICAGO HEIGHTS, IL
 * WESTERN TRAFFIC CONTROL, INC.
 CICERO, IL

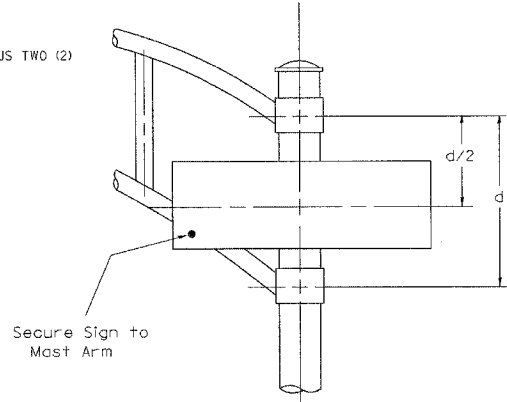
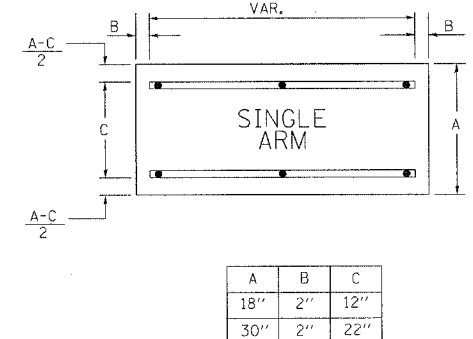
PARTS LISTING:
SIGN CHANNEL PART #HPN053 (MED. CHANNEL)
SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3
BRACKETS SELF TAPPING WITH NEOPRENE WASHER
PART #HPN034 (UNIVERSAL)
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

SUPPORTING CHANNELS



SUPPORTING CHANNELS



DUAL ARM

SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM Shall be used. See Note #5.

Upper Case To Lower Case
Spacing Chart 8-6 Inch Series "C & D"

FIRST LETTER	SECOND LETTER															
	a c d e		b h i k l		f w		j		s t		v y		x		z	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
A W X	12	14	14	15	12	14	06	10	11	14	06	10	11	12	12	14
B	14	15	20	21	14	15	11	12	14	15	12	14	12	14	16	17
C E G	14	15	20	21	12	14	06	10	12	14	12	14	14	15	14	15
D O Q R	14	15	20	21	14	15	06	10	12	14	12	14	14	15	14	15
F	05	06	14	15	06	10	05	06	06	10	06	10	06	10	11	12
H I M N	20	21	22	24	20	21	14	15	16	17	16	17	20	21	20	21
J U	20	21	20	21	16	17	14	15	16	17	16	17	16	17	20	21
K L	11	12	16	17	11	12	05	06	11	12	11	12	11	12	12	14
P	12	14	14	15	12	14	05	06	11	12	11	12	12	14	12	14
S	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14
T	11	12	16	17	06	10	06	10	11	12	11	12	11	12	12	14
V	06	10	14	15	11	12	06	10	12	14	12	14	12	14	12	14
Y	05	06	14	15	06	10	05	06	05	07	05	06	06	10	11	12
Z	16	17	22	24	16	17	12	14	16	17	16	17	16	17	20	21

Lower Case To Lower Case
Spacing Chart 6 Inch Series "C & D"

FIRST LETTER	SECOND LETTER															
	a c d e		b h i k l		f w		j		s t		v y		x		z	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
a d h g i j	16	17	22	24	16	17	12	14	14	15	14	15	16	17	16	17
l m n q u																
b f k o p s	12	14	16	17	11	12	05	06	11	12	11	12	12	14	12	14
c e	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14
r	06	10	12	14	06	10	03	03	05	06	05	06	06	10	06	10
t z	12	14	16	17	12	14	06	10	11	12	11	12	12	14	12	14
v y	11	12	14	15	11	12	05	06	06	10	06	10	11	12	11	12
w	11	12	14	15	11	12	05	06	11	12	11	12	11	12	12	14
x	12	14	16	17	11	12	05	06	11	12	11	12	11	12	12	14

Number To Number
Spacing Chart 8 Inch Series "C & D"

FIRST NUMBER	SECOND NUMBER																			
	0		1		2		3		4		5		6		7		8		9	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
0 9	16	17	16	17	14	15	12	14	14	15	14	15	16	17	12	14	16	17	16	17
1	20	21	20	21	20	21	16	17	14	15	20	21	20	21	14	15	20	21	20	21
2 3 4	14	15	14	15	14	15	12	14	14	15	14	15	11	12	16	17	14	15		
5	14	15	14	15	14	15	11	12	11	12	14	15	14	15	11	12	14	15	14	15
6	16	17	14	15	14	15	12	15	12	14	14	15	14	15	11	12	14	15	14	15
7	12	14	12	14	14	15	12	15	05	06	12	14	14	15	11	12	14	15	12	14
8	16	17	16	17	14	15	12	15	12	14	14	15	16	17	12	14	16	17	14	15

UPPER AND LOWER CASE LETTER WIDTHS

LETTERS	6 INCH UPPER CASE LETTERS				8 INCH UPPER CASE LETTERS				6 INCH LOWER CASE LETTERS			
	SERIES		SERIES		SERIES		SERIES		SERIES		SERIES	
	C	D	C	D	C	D	C	D	C	D	C	D
A	3 ⁶	5 ⁰	5 ⁰	6 ⁵	a	3 ⁵	4 ²					
B	3 ²	4 ⁰	4 ³	5 ³	b	3 ⁵	4 ²					
C	3 ²	4 ⁰	4 ³	5 ³	c	3 ⁵	4 ¹					
D	3 ²	4 ⁰	4 ³	5 ³	d	3 ⁵	4 ²					
E	3 ⁰	3 ⁵	4 ⁰	4 ⁷	e	3 ⁵	4 ²					
F	3 ⁰	3 ⁵	4 ⁰	4 ⁷	f	2 ³	2 ⁶					
G	3 ²	4 ⁰	4 ³	5 ³	g	3 ⁵	4 ²					
H	3 ²	4 ⁰	4 ³	5 ³	h	3 ⁵	4 ²					
I	0 ⁷	0 ⁷	1 ¹	1 ²	i	1 ¹	1 ¹					
J	3 ⁰	3 ⁶	4 ⁰	5 ⁰	j	2 ⁰	2 ²					
K	3 ²	4 ¹	4 ³	5 ⁴	k	3 ⁵	4 ²					
L	3 ⁰	3 ⁵	4 ⁰	4 ⁷	l	1 ¹	1 ¹					
M	3 ⁷	4 ⁵	5 ¹	6 ¹	m	6 ⁰	7 ⁰					
N	3 ²	4 ⁰	4 ³	5 ³	n	3 ⁵	4 ²					
O	3 ⁴	4 ²	4 ⁵	5 ⁵	o	3 ⁶	4 ³					
P	3 ²	4 ⁰	4 ³	5 ³	p	3 ⁵	4 ²					
Q	3 ⁴	4 ²	4 ⁵	5 ⁵	q	3 ⁵	4 ²					
R	3 ²	4 ⁰	4 ³	5 ³	r	2 ⁶	3 ²					
S	3 ²	4 ⁰	4 ³	5 ³	s	3 ⁶	4 ²					
T	3 ⁰	3 ⁵	4 ⁰	4 ⁷	t	2 ⁷	3 ²					
U	3 ²	4 ⁰	4 ³	5 ³	u	3 ⁵	4 ²					
V	3 ⁵	4 ⁴	4 ⁷	6 ⁰	v	4 ²	4 ⁷					
W	4 ⁴	5 ²	6 ⁰	7 ⁰	w	5 ⁵	6 ⁴					
X	3 ⁴	4 ⁰	4 ⁵	5 ³	x	4 ⁴	5 ¹					
Y	3 ⁶	5 ⁰	5 ⁰	6 ⁶	y	4 ⁶	5 ³					
Z	3 ²	4 ⁰	4 ³	5 ³	z	3 ⁶	4 ³					

NUMBER

NUMBER	6 INCH SERIES		8 INCH SERIES	
	C	D	C	D
1	1 ²	1 ⁴	1 ⁵	2 ⁰
2	3 ²	4 ⁰	4 ³	5 ³
3	3 ²	4 ⁰	4 ³	5 ³
4	3 ⁵	4 ³	4 ⁷	5 ⁷
5	3 ²	4 ⁰	4 ³	5 ³
6	3 ²	4 ⁰	4 ³	5 ³
7	3 ²	4 ⁰	4 ³	5 ³
8	3 ²	4 ⁰	4 ³	5 ³
9	3 ²	4 ⁰	4 ³	5 ³
0	3 ⁴	4 ²	4 ⁵	5 ⁵

REVISIONS

NAME	DATE
D.A.Z./D.A.G.	11/90
	6/98
CADD	10/00

Illinois Department of Transportation
DISTRICT 1

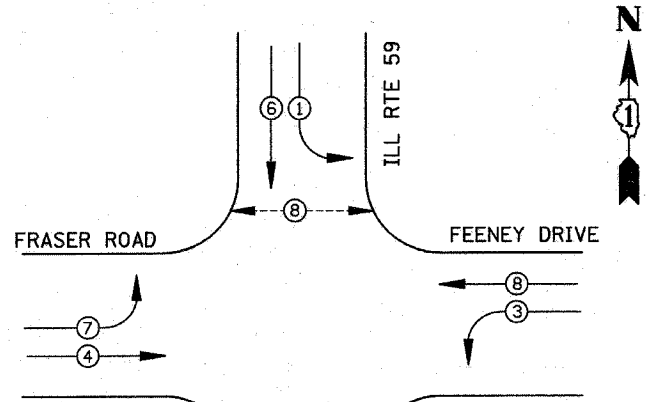
MAST ARM MOUNTED STREET NAME SIGNS

SCALE: NONE
DATE: Apr 13, 2003
DRAWN BY: RDB
DESIGNED BY: JHE
CHECKED BY: DAD

PREPARED BY:
CEMCON, Ltd.
 Consulting Engineers, Land Surveyors & Planners
 111/1016
 Ph: 630-862-2100 Fax: 630-862-2199
 E-Mail: cedd@cemcon.com WebSite: www.cemcon.com

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	113 & 114R	WILL	525	308
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO.		62417		

CONTROLLER SEQUENCE



EXISTING	PROPOSED	EXISTING	PROPOSED	DESCRIPTION
				8" (200mm) TRAFFIC SIGNAL SECTION
				12" (300mm) TRAFFIC SIGNAL SECTION
				12" (300mm) PEDESTRIAN SIGNAL SECTION
				12" (300mm) PEDESTRIAN SIGNAL SECTION
				CONTROLLER CABINET
				SERVICE INSTALLATION
				TELEPHONE CONNECTION
				MAGNETIC DETECTOR
				EMERGENCY VEHICLE LIGHT DETECTOR
				CONFIRMATION BEACON
				PUSHBUTTON DETECTOR
				VEHICLE DETECTOR, INDUCTION LOOP
				DENOTES NUMBER OF CONDUCTORS, ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED.
				MICROWAVE VEHICLE SENSOR
				UNINTERRUPTIBLE POWER SUPPLY
				PERFORMED DETECTOR LOOP

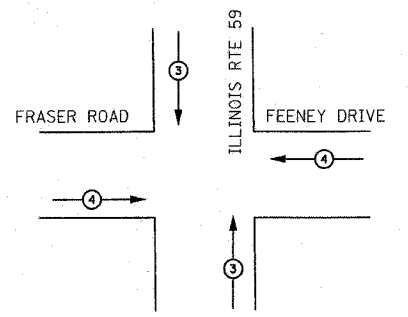
LEGEND

	DUAL ENTRY PHASE
	SINGLE ENTRY PHASE
	PEDESTRIAN PHASE
	NUMBER REFERS TO ASSOCIATED PHASE
	OVERLAP

PHASE DESIGNATION DIAGRAM



CONTROLLER SEQUENCE



EMERGENCY VEHICLE PREEMPTION SEQUENCE

PROPOSED EMERGENCY VEHICLE PREEMPTOR		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	↑	→

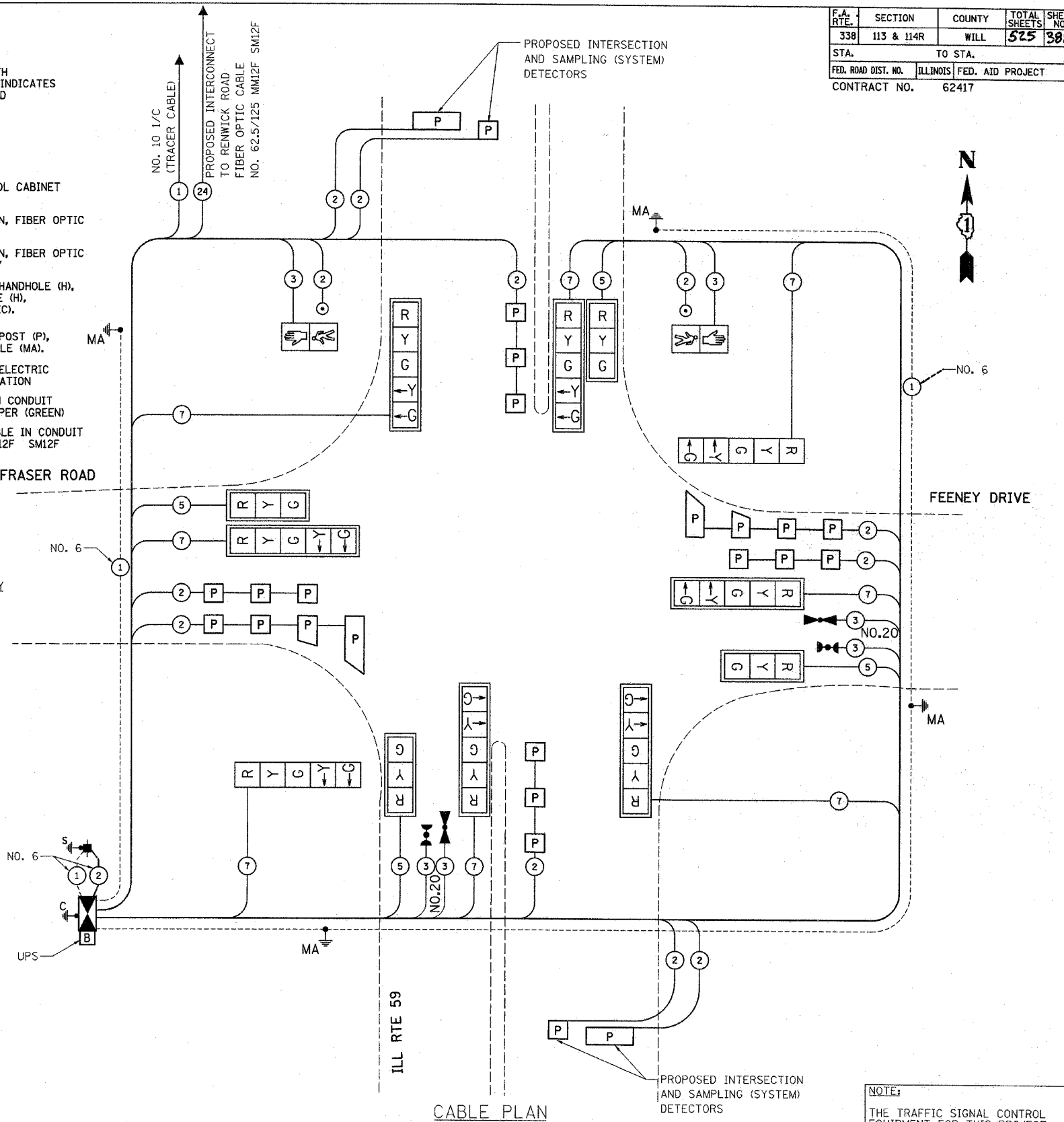
SCHEDULE OF QUANTITIES

ITEM	UNIT	QUANTITY
SIGN PANEL, TYPE 1	SQ FT	45
CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	508
CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	32
CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	38
CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	10
CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	110
CONDUIT PUSHED, 3" DIA., GALVANIZED STEEL	FOOT	146
CONDUIT PUSHED, 3 1/2" DIA., GALVANIZED STEEL	FOOT	68
CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	104
HANDHOLE	EACH	4
HEAVY-DUTY HANDHOLE	EACH	4
DOUBLE HANDHOLE	EACH	2
TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	583
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL TRANSCIEVER - FIBER OPTIC	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	969
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1660
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	809
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1636
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	2524
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	26
STEEL MAST ARM ASSEMBLY AND POLE, 30 FT.	EACH	2
STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 28 FT. & 50 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 38 FT. & 44 FT.	EACH	1
CONCRETE FOUNDATION, TYPE C	FOOT	4
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	30
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	30
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	6
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED	EACH	2
TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	10
INDUCTIVE LOOP DETECTOR	EACH	10
LIGHT DETECTOR	EACH	2
LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	2
PERFORMED DETECTOR LOOP	FOOT	887
SERVICE INSTALLATION, POLE MOUNTED	EACH	1
ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6, 1C	FOOT	493
ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED & SHIELDED	FOOT	290
UNINTERRUPTIBLE POWER SUPPLY	EACH	1

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	%OPERATION	
SIGNAL (RED)	12	135	17	0.50	102
(YELLOW)	12	135	25	0.25	75
(GREEN)	12	135	15	0.25	45
ARROW	16	135	12	0.10	19.2
PED. SIGNAL	2	90	25	1.00	50
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN	-	84	-	0.05	-

ENERGY COSTS TO: TOTAL = 391.2
 ILLINOIS DEPARTMENT OF TRANSPORTATION
 201 W. CENTER COURT
 SCHAUMBURG, IL 60196-1096
 ENERGY SUPPLY CONTACT: JUDY MILLER
 PHONE: (815)724-5717
 COMPANY: Commonwealth Edison

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	VERTICAL	FT. (m)
TYPE A - POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS	3.0 (1.0)
D - CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	29.0 (8.0)
E - M. ARM POLE	0	SIGNAL POST	0 (0.0)	BRACKET MOUNTED	13 (4.0)
24" (600mm)	10 (3.0)	CONTROLLER CAB.	0 (0.0)	PED. PUSH-BUTTON	6 (2.0)
30" (750mm)	15 (4.6)	FIBER OPTIC	13 (4.0)	ELECTRIC SERVICE	13.5 (4.1)
		ELECTRIC SERVICE	1 (0.5)	SERVICE TO GROUND	13.5 (4.1)
		GROUND CABLE	1 (0.5)	POST MOUNTED	13 (4.0)



CABLE PLAN



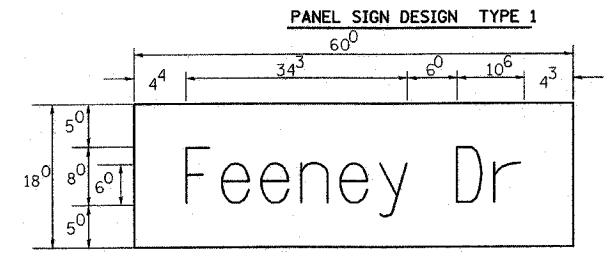
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
CABLE PLAN, PHASE DESIGNATION DIAGRAM, SCHEDULE OF QUANTITIES
 ILL RTE 59 AT FRASER RD. AND FEENEY DR.
 SCALE: VERT. NONE
 HORIZ. NONE
 DATE 3/10/2008
 DRAWN BY JLA
 DESIGNED BY KK/BPT
 CHECKED BY JTL/BPT

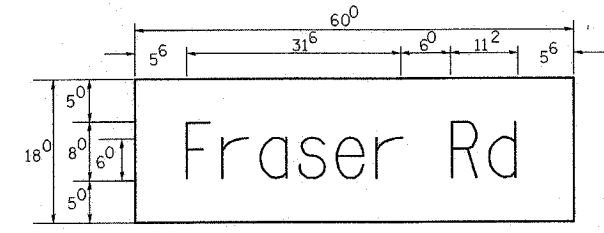
NOTE:
 THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

PLOT DATE = 3/10/2008
 FILE NAME = J:\Microstation\3520842\SIGNALS\SRV\CABLE_RTE59FRASER.dgn
 PLOT SCALE = 1"=20'
 USER NAME = JLA

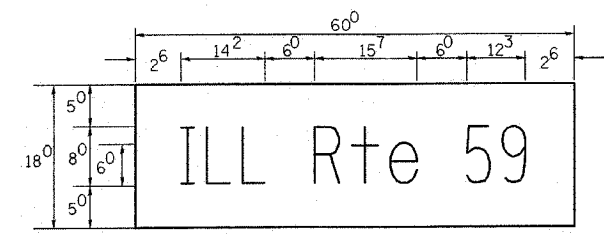
PREPARED BY:
CEMCON, Ltd.
 Consulting Engineers, Land Surveyors & Planners
 1111 East 11th Street, Suite 100
 Aurora, IL 60504-9676
 Ph: 630-862-2100 Fax: 630-862-2199
 E-Mail: cec@cemcon.com Website: www.cemcon.com



___ Sq. M. each
 7.5 Sq. Ft. each
 2 Required
 Design Series D



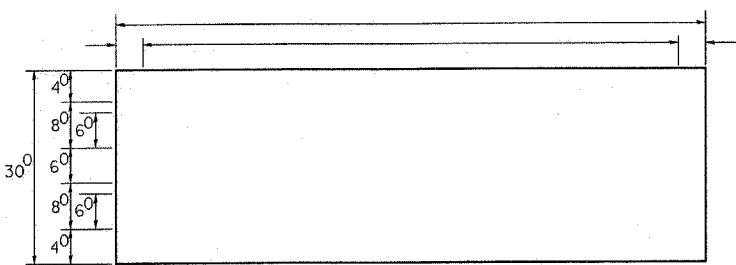
___ Sq. M. each
 7.5 Sq. Ft. each
 2 Required
 Design Series D



___ Sq. M. each
 7.5 Sq. Ft. each
 2 Required
 Design Series D

NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS

PANEL SIGN DESIGN TYPE 2



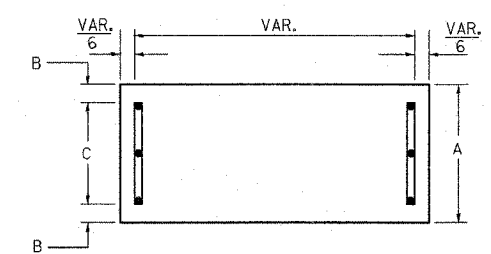
___ Sq. M. each
 ___ Sq. Ft. each
 ___ Required
 Design Series ___

GENERAL NOTES

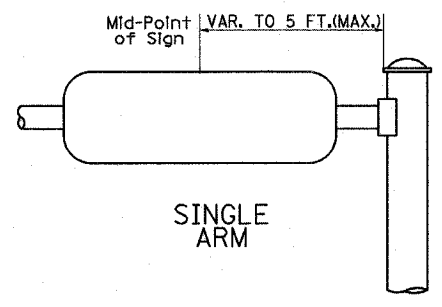
- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 834001, 834006 AND 834011, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 6'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
- THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 6'-0".
- ALL BORDERS SHALL BE 3/8" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:
 - * A.K.T. CORPORATION SCHAUMBURG, IL
 - * TUCKER COMPANY, INC. WAUWATOSA, WI
 - * AMERICAN FABRICATION CO. CHICAGO HEIGHTS, IL
 - * WESTERN TRAFFIC CONTROL INC. CICERO, IL

PARTS LISTING:
 SIGN CHANNEL PART #HPN053 (MED. CHANNEL)
 SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3
 SELF TAPPING WITH NEOPRENE WASHER
 BRACKETS PART #HPN034 (UNIVERSAL)
 CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING
 OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

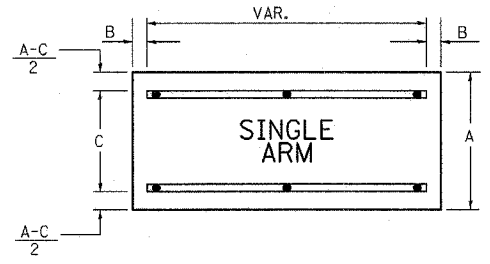
SUPPORTING CHANNELS



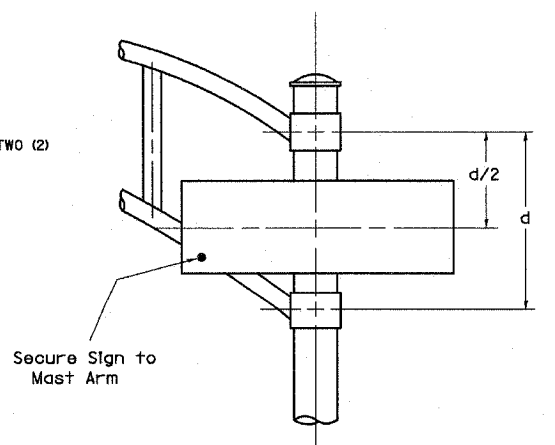
A	B	C
18"	2"	14"



SUPPORTING CHANNELS



A	B	C
18"	2"	12"
30"	2"	22"



DUAL ARM
 SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM
 Shall be used. See Note #5.

Upper Case To Lower Case
 Spacing Chart 8-6 Inch Series "C & D"

EXAMPLE, 2 DENOTES 3/8"

FIRST LETTER	SECOND LETTER															
	a c c d e		b h k l		f w		j		s t		v y		x		z	
	g o q	m n p r u														
SERIES	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
A W X	12	14	14	15	12	14	06	10	11	14	06	10	11	12	12	14
B	14	15	20	21	14	15	11	12	14	15	12	14	12	14	16	17
C E G	14	15	20	21	12	14	06	10	12	14	12	14	14	15	14	15
D O Q R	14	15	20	21	14	15	06	10	12	14	12	14	14	15	14	15
F	05	06	14	15	06	10	05	06	06	10	06	10	06	10	11	12
H I M N	20	21	22	24	20	21	14	15	16	17	16	17	20	21	20	21
J U	20	21	20	21	16	17	14	15	16	17	16	17	16	17	20	21
K L	11	12	16	17	11	12	05	06	11	12	11	12	11	12	12	14
P	12	14	14	15	12	14	05	06	11	12	11	12	12	14	12	14
S	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14
T	11	12	16	17	06	10	06	10	11	12	11	12	11	12	12	14
V	06	10	14	15	11	12	06	10	12	14	12	14	12	14	12	14
Y	05	06	14	15	06	10	05	06	05	07	05	06	06	10	11	12
Z	16	17	22	24	16	17	12	14	16	17	16	17	16	17	20	21

Lower Case To Lower Case
 Spacing Chart 6 Inch Series "C & D"

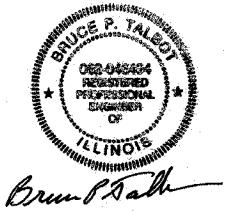
FIRST LETTER	SECOND LETTER															
	a c c d e		b h k l		f w		j		s t		v y		x		z	
	g o q	m n p r u														
SERIES	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
a d h g l j	16	17	22	24	16	17	12	14	14	15	14	15	16	17	16	17
l m n q u																
b f k o p s	12	14	16	17	11	12	05	06	11	12	11	12	12	14	12	14
c e	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14
r	06	10	12	14	06	10	03	03	05	06	05	06	06	10	06	10
t z	12	14	16	17	12	14	06	10	11	12	11	12	12	14	12	14
v y	11	12	14	15	11	12	05	06	06	10	06	10	11	12	11	12
w	11	12	14	15	11	12	05	06	11	12	11	12	11	12	12	14
x	12	14	16	17	11	12	05	06	11	12	11	12	11	12	12	14

Number To Number
 Spacing Chart 8 Inch Series "C & D"

FIRST NUMBER	SECOND NUMBER																				
	0		1		2		3		4		5		6		7		8		9		
SERIES	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	
0 9	16	17	16	17	14	15	12	14	14	15	14	15	16	17	12	14	16	17	16	17	
1	20	21	20	21	20	21	16	17	14	15	20	21	20	21	14	15	20	21	20	21	
2 3 4	14	15	14	15	14	15	12	14	12	14	14	15	14	15	11	12	16	17	14	15	
5	14	15	14	15	14	15	11	12	11	12	14	15	14	15	11	12	14	15	14	15	
6	16	17	14	15	14	15	12	14	14	15	14	15	11	12	14	15	14	15	14	15	
7	12	14	12	14	14	15	12	14	15	05	06	12	14	14	15	11	12	14	15	12	14
8	16	17	16	17	14	15	12	14	14	15	16	17	12	14	16	17	14	15	14	15	

LETTERS	6 INCH UPPER CASE LETTERS				8 INCH UPPER CASE LETTERS				6 INCH LOWER CASE LETTERS			
	SERIES		SERIES		SERIES		SERIES		SERIES		SERIES	
	C	D	C	D	C	D	C	D	C	D	C	D
A	36	50	50	65	a	35	42					
B	32	40	43	53	b	35	42					
C	32	40	43	53	c	35	41					
D	32	40	43	53	d	35	42					
E	30	35	40	47	e	35	42					
F	30	35	40	47	f	23	26					
G	32	40	43	53	g	35	42					
H	32	40	43	53	h	35	42					
I	07	07	11	12	i	11	11					
J	30	36	40	50	j	20	22					
K	32	41	43	54	k	35	42					
L	30	35	40	47	l	11	11					
M	37	45	51	61	m	60	70					
N	32	40	43	53	n	35	42					
O	34	42	45	55	o	36	43					
P	32	40	43	53	p	35	42					
Q	34	42	45	55	q	35	42					
R	32	40	43	53	r	26	32					
S	32	40	43	53	s	36	42					
T	30	35	40	47	t	27	32					
U	32	40	43	53	u	35	42					
V	35	44	47	60	v	42	47					
W	44	52	60	70	w	55	64					
X	34	40	45	53	x	44	51					
Y	36	50	50	66	y	46	53					
Z	32	40	43	53	z	36	43					

NUMBER	6 INCH SERIES		8 INCH SERIES	
	C	D	C	D
1	12	14	15	20
2	32	40	43	53
3	32	40	43	53
4	35	43	47	57
5	32	40	43	53
6	32	40	43	53
7	32	40	43	53
8	32	40	43	53
9	32	40	43	53
0	34	42	45	55



REVISIONS
 NAME DATE
 ILLINOIS DEPARTMENT OF TRANSPORTATION
 STANDARD TRAFFIC SIGNAL DESIGN DETAILS
 SCALE: VERT. NOT TO SCALE
 HORIZ. DATE 3/10/2008
 DRAWN BY JLA
 DESIGNED BY KK/BPT
 CHECKED BY JTL/BPT
 SHEET OF

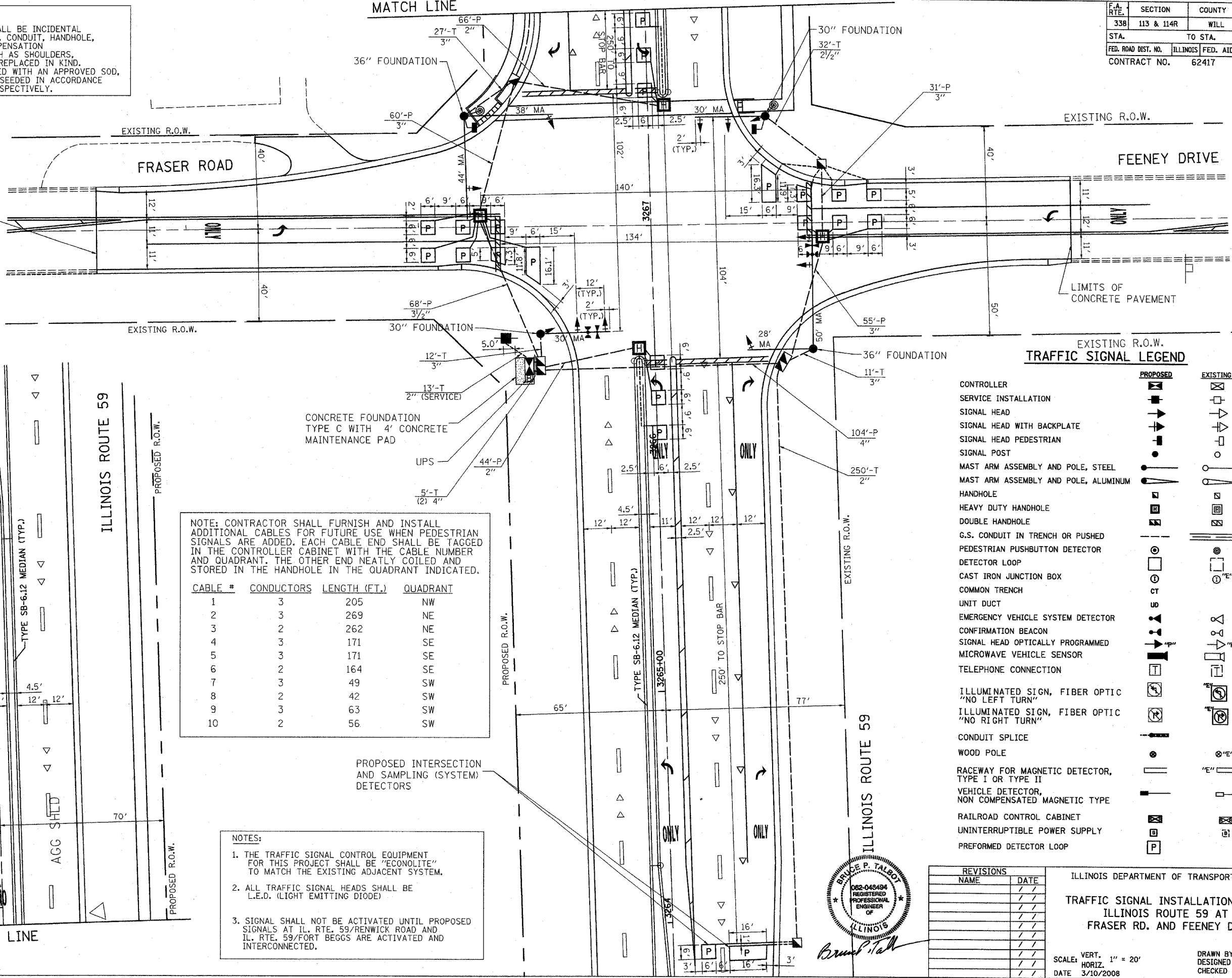
DATE: 3/10/2008
 FILE NAME: 1111-001-1382842-SIGNALS-VISIBILITY-STREET_NAME.dgn
 PLOT SCALE: 1/8"=1'-0"
 USER NAME: JLA

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	113 & 114R	WILL	525	390
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO.		62417		

RESTORATION OF WORK AREA
 RESTORATION OF TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

FOR UNDERGROUND UTILITY LOCATIONS, CALL J.U.L.I.E. TOLL FREE TEL. 1-800-892-0123

PREPARED BY:
CEMCON, Ltd.
 Consulting Engineers, Land Surveyors & Planners
 2280 Wolfe Oak Circle, Suite 100
 Naperville, IL 60563
 Ph: 630.682.2100 Fax: 630.682.2199
 E-Mail: info@cemcon.com Website: www.cemcon.com



PROPOSED INTERCONNECT TO RENWICK ROAD ECONOLITE SYSTEM NO. 140 (FIBER)

PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTORS

TRAFFIC SIGNAL LEGEND

- | | | | |
|--|--|----------|--|
| CONTROLLER | | EXISTING | |
| SERVICE INSTALLATION | | EXISTING | |
| SIGNAL HEAD | | EXISTING | |
| SIGNAL HEAD WITH BACKPLATE | | EXISTING | |
| SIGNAL HEAD PEDESTRIAN | | EXISTING | |
| SIGNAL POST | | EXISTING | |
| MAST ARM ASSEMBLY AND POLE, STEEL | | EXISTING | |
| MAST ARM ASSEMBLY AND POLE, ALUMINUM | | EXISTING | |
| HANDHOLE | | EXISTING | |
| HEAVY DUTY HANDHOLE | | EXISTING | |
| DOUBLE HANDHOLE | | EXISTING | |
| G.S. CONDUIT IN TRENCH OR PUSHED | | EXISTING | |
| PEDESTRIAN PUSHBUTTON DETECTOR | | EXISTING | |
| DETECTOR LOOP | | EXISTING | |
| CAST IRON JUNCTION BOX | | EXISTING | |
| COMMON TRENCH | | EXISTING | |
| UNIT DUCT | | EXISTING | |
| EMERGENCY VEHICLE SYSTEM DETECTOR | | EXISTING | |
| CONFIRMATION BEACON | | EXISTING | |
| SIGNAL HEAD OPTICALLY PROGRAMMED | | EXISTING | |
| MICROWAVE VEHICLE SENSOR | | EXISTING | |
| TELEPHONE CONNECTION | | EXISTING | |
| ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN" | | EXISTING | |
| ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN" | | EXISTING | |
| CONDUIT SPLICE | | EXISTING | |
| WOOD POLE | | EXISTING | |
| RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II | | EXISTING | |
| VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE | | EXISTING | |
| RAILROAD CONTROL CABINET | | EXISTING | |
| UNINTERRUPTIBLE POWER SUPPLY | | EXISTING | |
| PERFORMED DETECTOR LOOP | | EXISTING | |

NOTE: CONTRACTOR SHALL FURNISH AND INSTALL ADDITIONAL CABLES FOR FUTURE USE WHEN PEDESTRIAN SIGNALS ARE ADDED. EACH CABLE END SHALL BE TAGGED IN THE CONTROLLER CABINET WITH THE CABLE NUMBER AND QUADRANT. THE OTHER END NEATLY COILED AND STORED IN THE HANDHOLE IN THE QUADRANT INDICATED.

CABLE #	CONDUCTORS	LENGTH (FT.)	QUADRANT
1	3	205	NW
2	3	269	NE
3	2	262	NE
4	3	171	SE
5	3	171	SE
6	2	164	SE
7	3	49	SW
8	2	42	SW
9	3	63	SW
10	2	56	SW

- NOTES:**
1. THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.
 2. ALL TRAFFIC SIGNAL HEADS SHALL BE L.E.D. (LIGHT EMITTING DIODE)
 3. SIGNAL SHALL NOT BE ACTIVATED UNTIL PROPOSED SIGNALS AT IL. RTE. 59/RENWICK ROAD AND IL. RTE. 59/FORT BEGGS ARE ACTIVATED AND INTERCONNECTED.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC SIGNAL INSTALLATION PLAN
 ILLINOIS ROUTE 59 AT
 FRASER RD. AND FEENEY DR.

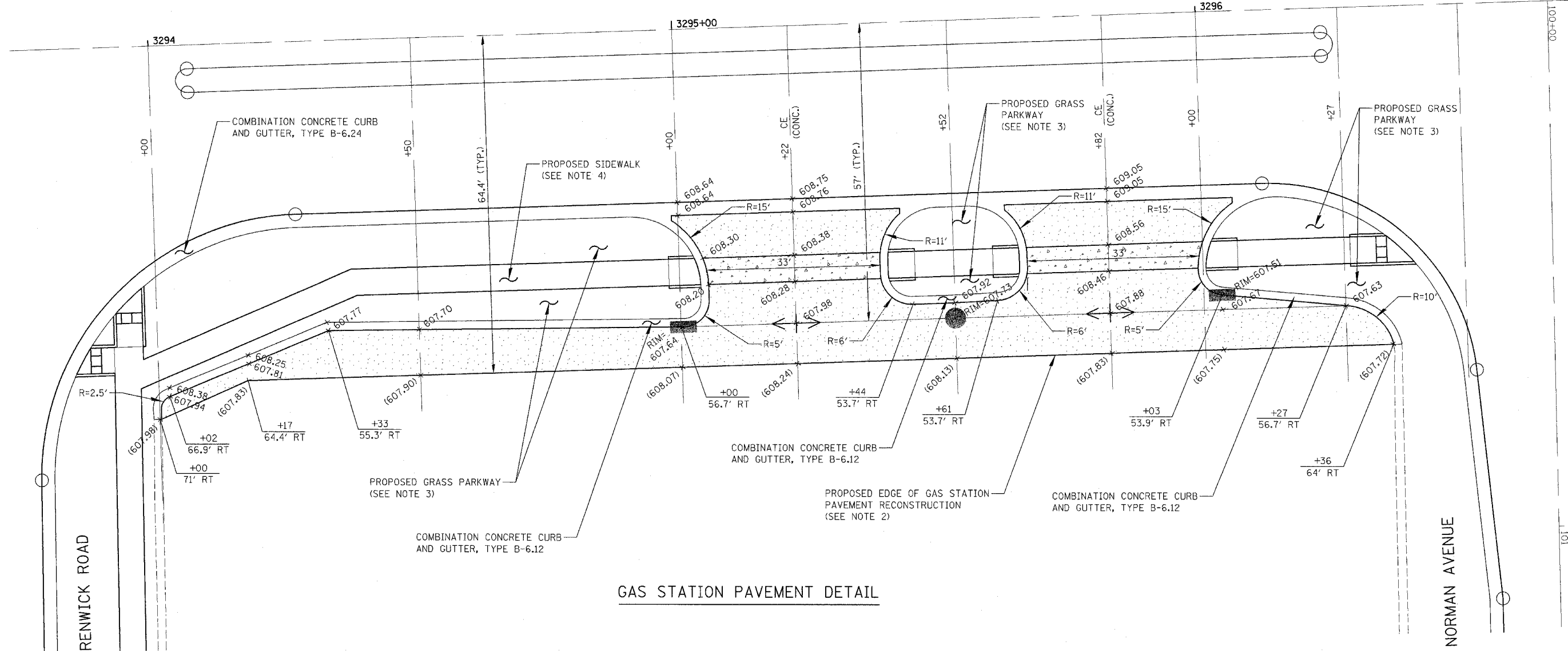
SCALE: VERT. 1" = 20'
 HORIZ. 1" = 20'
 DATE 3/10/2008

DRAWN BY JLA
 DESIGNED BY KK/BPT
 CHECKED BY JTL/BPT

PLOT DATE = 2/18/2008
 FILE NAME = J:\M\302842\SIGNALS\SH1\ITE 59 @ FRASER.dgn
 PLOT SCALE = 1"=20'
 USER NAME = JLA

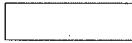
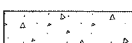
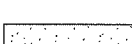


FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	391
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FAP 338 (IL RTE. 59)		

ILLINOIS 59



GAS STATION PAVEMENT DETAIL

LEGEND

-  PCC SIDEWALK, 5"
-  PCC SIDEWALK, 8"
-  PCC DRIVEWAY PAVEMENT, 8"
-  PROPOSED CATCH BASIN (SEE NOTE 5)
-  PROPOSED INLET (SEE NOTE 5)

NOTES:

1. (607.98) EXISTING ELEVATIONS
607.94 PROPOSED ELEVATIONS
2. GAS STATION ENTRANCE AND PAVEMENT RECONSTRUCTION SHALL CONSIST OF 8" THICK PORTLAND CEMENT CONCRETE
3. SEE LANDSCAPING PLANS FOR LANDSCAPING REQUIREMENTS
4. SEE SIDEWALK PLANS FOR PROPOSED SIDEWALK DETAILS.
5. SEE DRAINAGE PLANS FOR DRAINAGE STRUCTURE INFORMATION.



ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG
IL59/RENWICK RD. GAS STATION
PAVEMENT RECONSTRUCTION
DETAIL

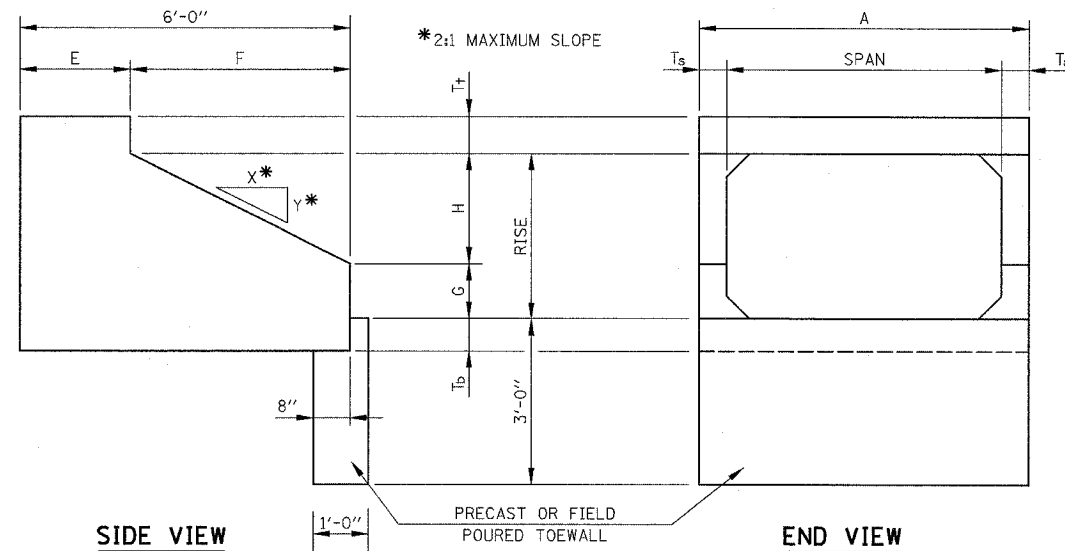
SCALE AS SHOWN
DATE MARCH 18, 2008

DRAWN BY RTA
CHECKED BY PJO

FAP RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	392
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		

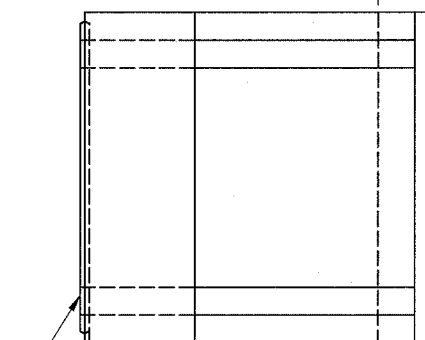
GENERAL NOTES

- PRECAST CONCRETE BOX CULVERT AND PRECAST CONCRETE BOX CULVERT END SECTIONS**
1. THIS WORK SHALL CONSIST OF FURNISHING AND INSTALLING PRECAST BOX CULVERTS AND BOX CULVERT END SECTIONS AS SHOWN ON THE PLANS.
 2. CONSTRUCTION OF PRECAST BOX CULVERTS AND BOX CULVERT END SECTIONS SHALL CONFORM TO ALL APPLICABLE REQUIREMENTS OF SECTION 540 OF THE STANDARD SPECIFICATIONS AND AS SPECIFIED HEREIN.
 3. THE PRECAST CONCRETE BOX CULVERT SECTIONS SHALL CONFORM TO THE REQUIREMENTS OF ASTM C1433 EXCEPT THAT THE AGGREGATE SHALL CONFORM TO THE REQUIREMENTS OF ARTICLES 1003.02 AND 1004.02 OF THE STANDARD SPECIFICATIONS.
 4. JOINTS SHALL BE SEALED ACCORDING TO ARTICLE 540.06 OF THE STANDARD SPECIFICATIONS. THE MASTIC JOINT SEALER SHALL BE SPREAD A MINIMUM OF 1 FOOT WIDE ON EACH SIDE OF THE TOP AND SIDE JOINTS BEFORE PLACING THE EXTERNAL SEALING BANDS OR NONWOVEN GEOTECHNICAL FABRIC. THE COST WILL BE INCLUDED IN THE COST OF THE PRECAST CONCRETE BOX CULVERT.
 5. SHOP PLANS FOR PRECAST CONCRETE BOX CULVERT SECTIONS WHICH SATISFY THE STANDARD SHAPES, REINFORCEMENT, AND DETAILING OF ASTM C1433 ARE NOT REQUIRED TO BE REVIEWED AND APPROVED BY THE ENGINEER.
 6. THE COST OF THE PRECAST CONCRETE BOX CULVERT, OF THE SIZE SPECIFIED, INCLUDES THE COST OF EXCAVATION FOR THE POROUS GRANULAR BACKFILL, EXCEPT EXCAVATION OF ROCK AND/OR UNSTABLE OR UNSUITABLE MATERIAL BELOW BEDDING GRADE.
 7. THE COST OF THE PRECAST CONCRETE BOX CULVERT END SECTIONS INCLUDES THE COST OF THE EXCAVATION, TOEWALL AND COLLARS.



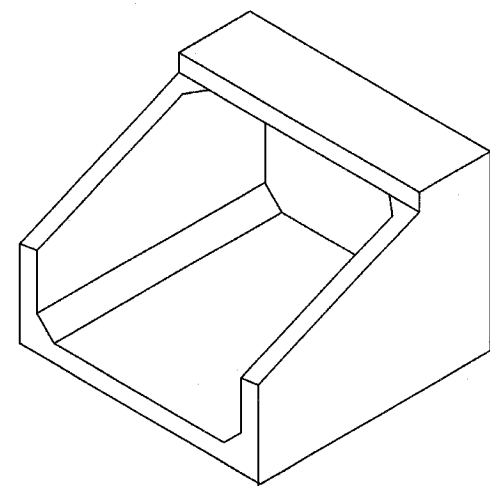
SIDE VIEW

END VIEW

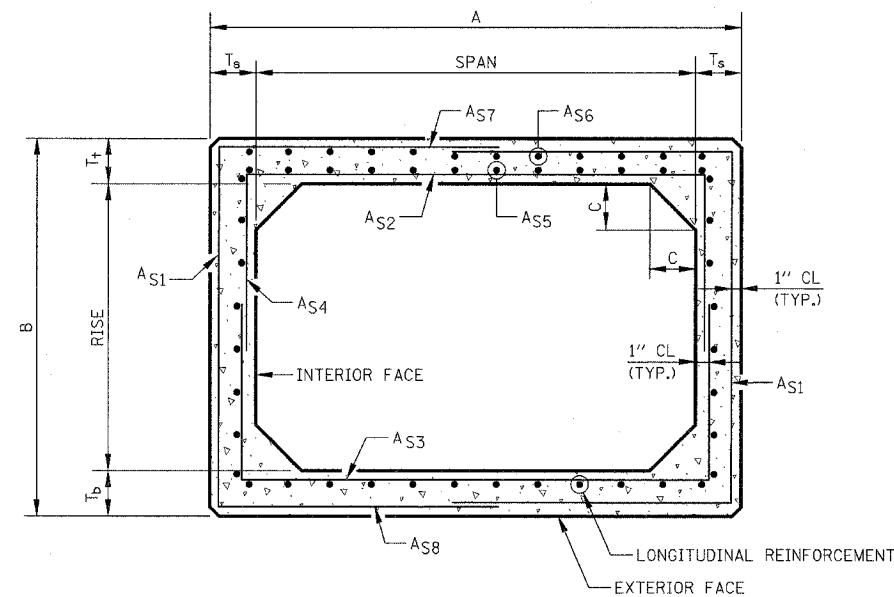


PLAN

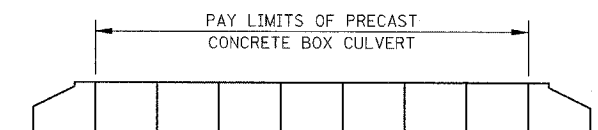
END CONNECTION TO FIT PRECAST BOX CULVERT BELL OR SPIGOT MAY BE OMITTED WHEN COLLARING TO AN EXISTING BOX OR HEADWALL



ISOMETRIC VIEW



CROSS SECTION



DIMENSIONS (FOR ASTM C1433)*

SPAN X RISE (FEET)	T ₊ (INCHES)	T _b (INCHES)	T _s (INCHES)	A (FT.-IN.)	B (FT.-IN.)	C (INCHES)	E (FT.-IN.)	F (FT.-IN.)	G (FT.-IN.)	H (FT.-IN.)	SLOPE (X:Y)
5 X 3	8	7	6	6-0	4-3	6	2-0	4-0	1-0	2-0	2:1

*ALL DIMENSIONS SHOULD BE VERIFIED WITH SUPPLIER

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG

5' X 3' PRECAST CONCRETE
BOX CULVERT AND END SECTIONS

SCALE NONE
DATE MARCH 18, 2008

DRAWN BY TB
CHECKED BY WPM

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(113 & 114) R-5	WILL	525	993
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		

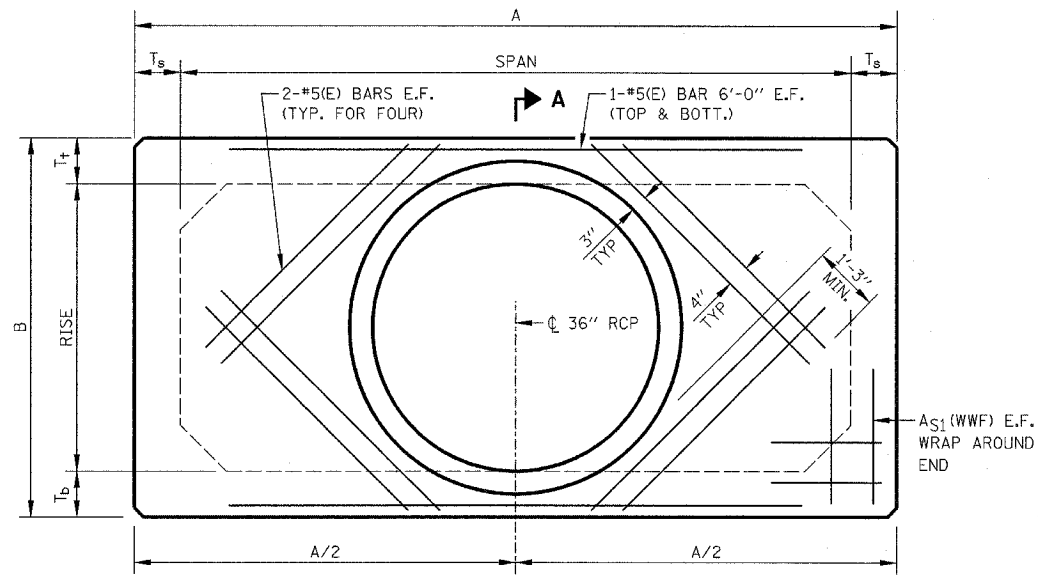
GENERAL NOTES

PRECAST CONCRETE BOX CULVERT

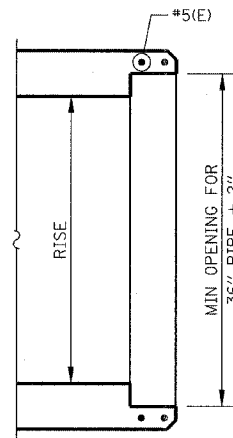
1. THIS WORK SHALL CONSIST OF FURNISHING AND INSTALLING PRECAST BOX CULVERTS AS SHOWN ON THE PLANS.
2. CONSTRUCTION OF PRECAST BOX CULVERTS SHALL CONFORM TO ALL APPLICABLE REQUIREMENTS OF SECTION 540 OF THE STANDARD SPECIFICATIONS AND AS SPECIFIED HEREIN.
3. THE PRECAST CONCRETE BOX CULVERT SECTIONS SHALL CONFORM TO THE REQUIREMENTS OF ASTM C1433 EXCEPT THAT THE AGGREGATE SHALL CONFORM TO THE REQUIREMENTS OF ARTICLES 1003.02 AND 1004.02 OF THE STANDARD SPECIFICATIONS.
4. JOINTS SHALL BE SEALED ACCORDING TO ARTICLE 540.06 OF THE STANDARD SPECIFICATIONS. THE MASTIC JOINT SEALER SHALL BE SPREAD A MINIMUM OF 1 FOOT WIDE ON EACH SIDE OF THE TOP AND SIDE JOINTS BEFORE PLACING THE EXTERNAL SEALING BANDS OR NONWOVEN GEOTECHNICAL FABRIC. THE COST WILL BE INCLUDED IN THE COST OF THE PRECAST CONCRETE BOX CULVERT.
5. THE COST OF THE PRECAST CONCRETE BOX CULVERT, OF THE SIZE SPECIFIED, INCLUDES THE COST OF EXCAVATION FOR THE POROUS GRANULAR BACKFILL, EXCEPT EXCAVATION OF ROCK AND/OR UNSTABLE OR UNSUITABLE MATERIAL BELOW BEDDING GRADE.
6. THE COST OF THE PRECAST CONCRETE BOX CULVERT, OF THE SIZE SPECIFIED, INCLUDES THE COST OF THE ADDITIONAL REINFORCEMENT AROUND STORM SEWER PIPE.

NOTE
OD = OUTSIDE PIPE DIAMETER

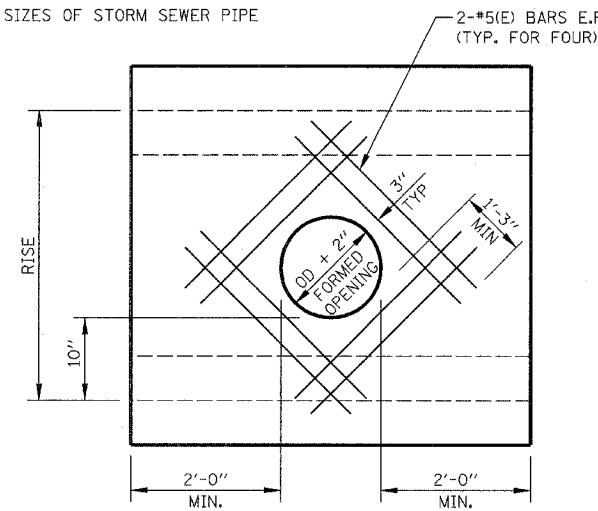
SEE DRAINAGE SHEETS FOR LOCATIONS AND SIZES OF STORM SEWER PIPE



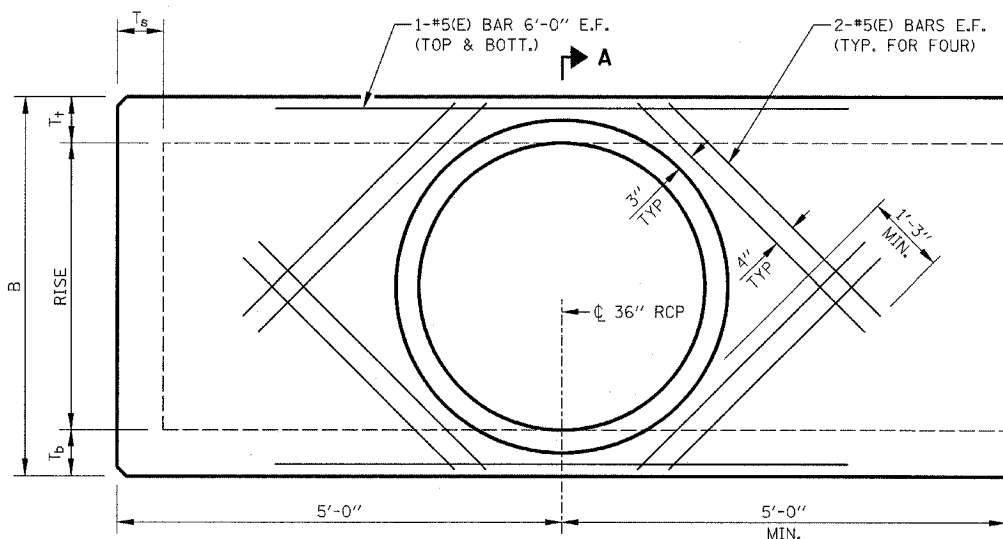
**UPSTREAM CLOSED SECTION
END VIEW**



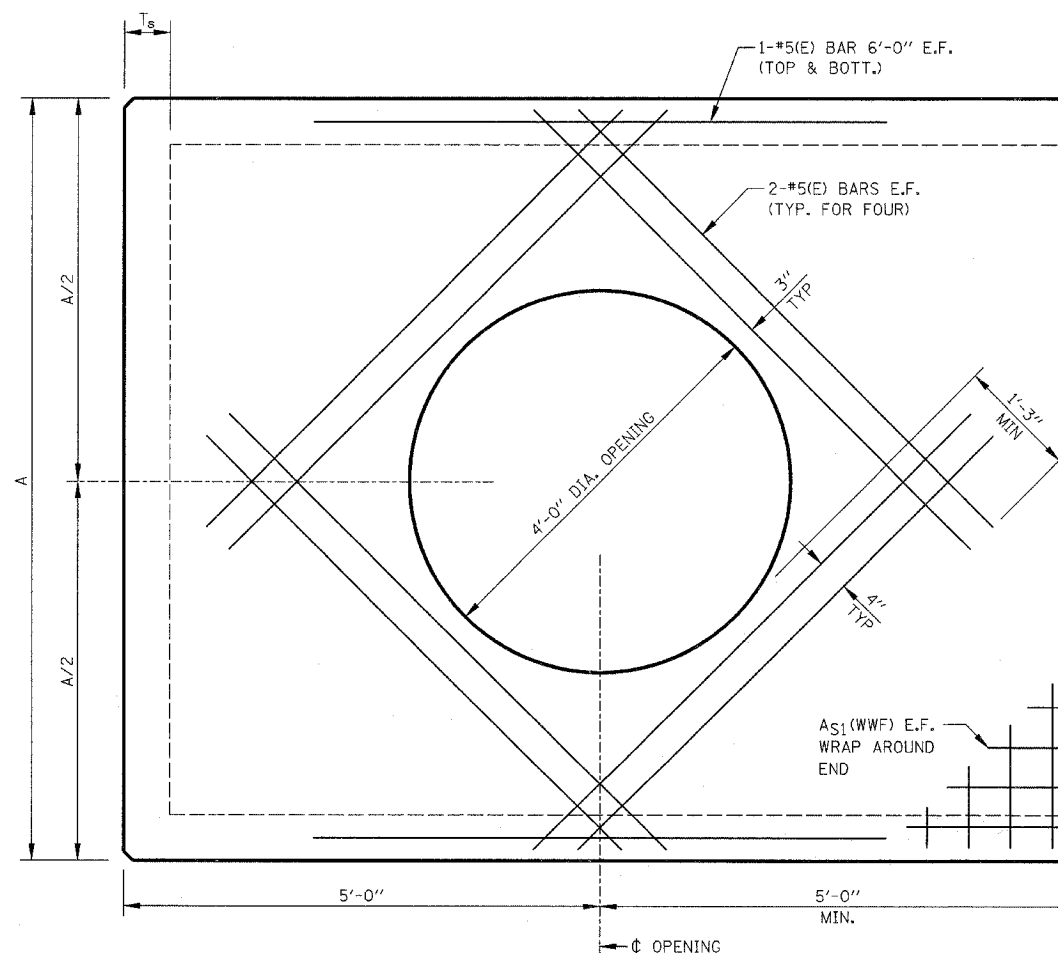
SECTION A-A



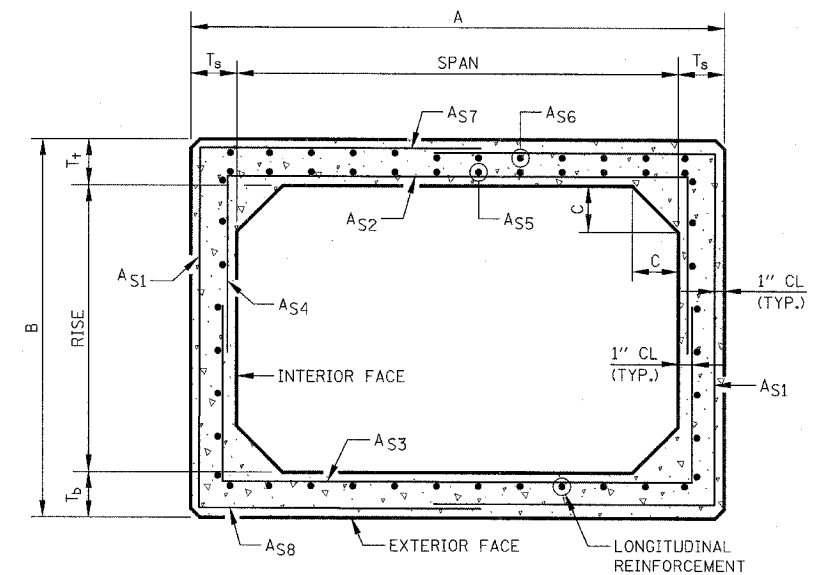
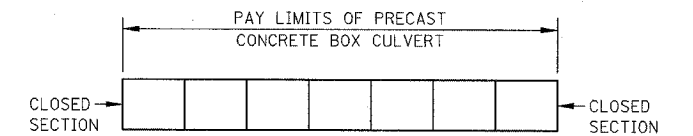
12" RCP STORM SEWER OPENINGS



**DOWNSTREAM CLOSED SECTION
SIDE VIEW**



**CLOSED SECTION
TOP VIEW
(2 THUS)**



CROSS SECTION

DIMENSIONS (FOR ASTM C1433)*

SPAN X RISE (FEET)	T ₊ (INCHES)	T _b (INCHES)	T _s (INCHES)	A (FT.-IN.)	B (FT.-IN.)	C (INCHES)
8 X 3	8	8	8	9-4	4-4	8

*ALL DIMENSIONS SHOULD BE VERIFIED WITH SUPPLIER

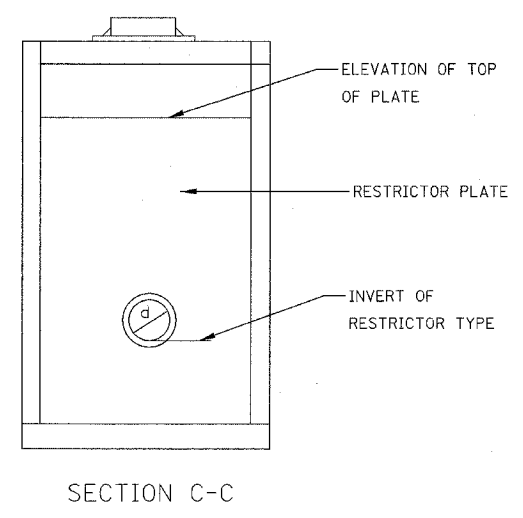
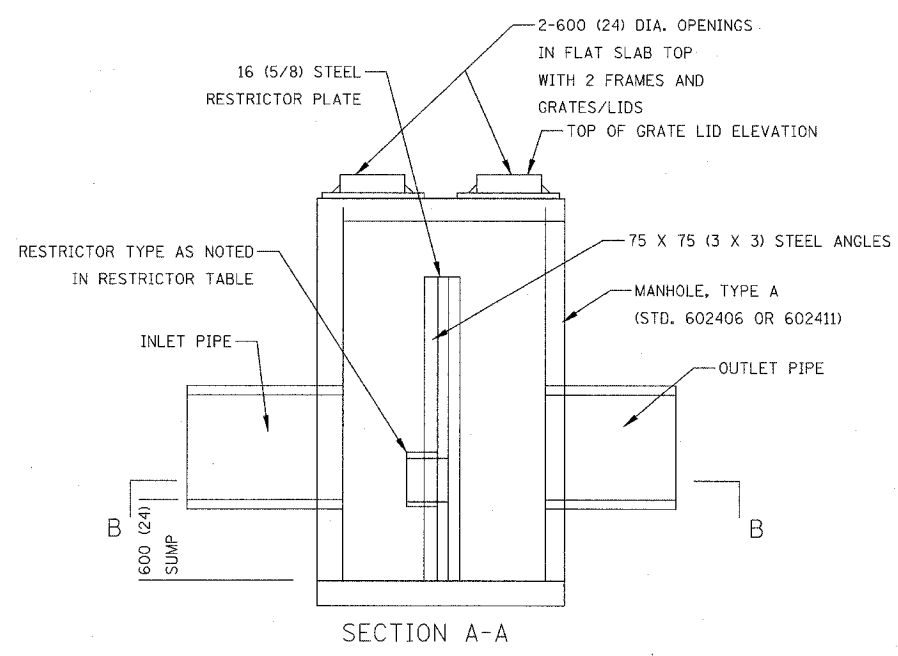
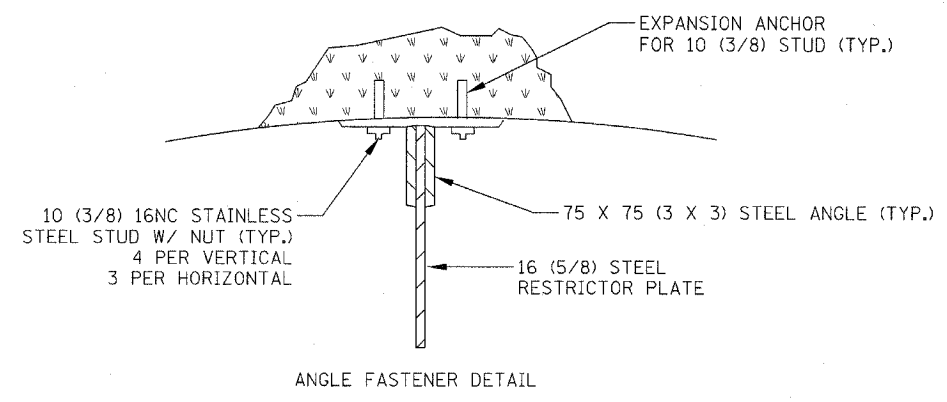
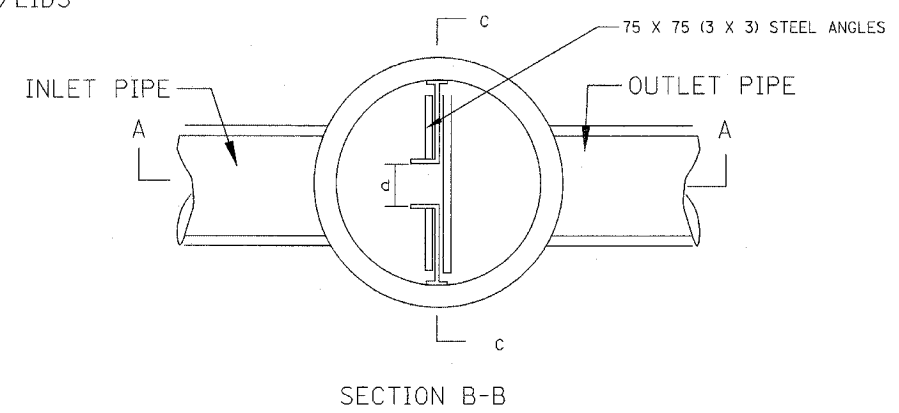
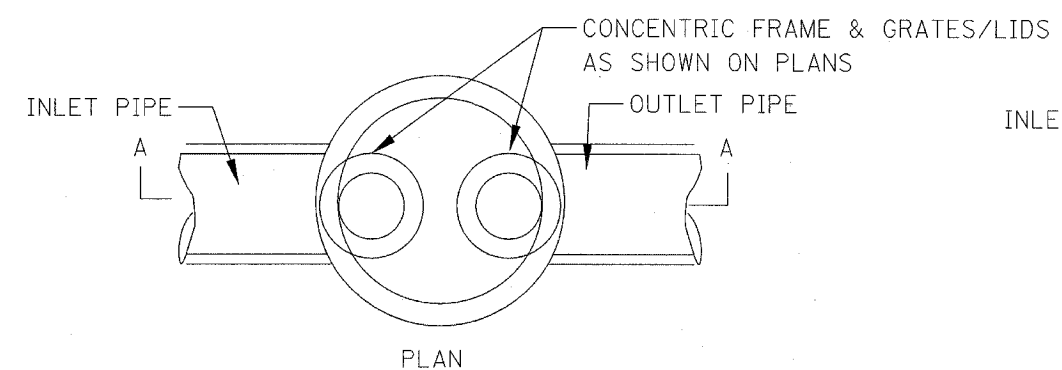
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG

**8' X 3' PRECAST CONCRETE
BOX CULVERT**

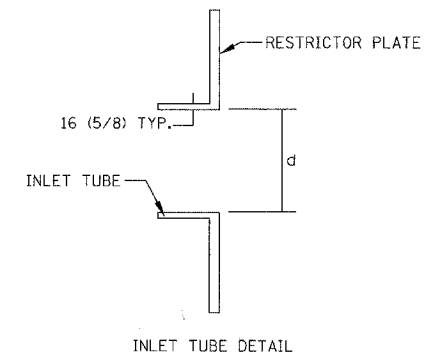
SCALE NONE
DATE MARCH 18, 2008

DRAWN BY TB
CHECKED BY WPM

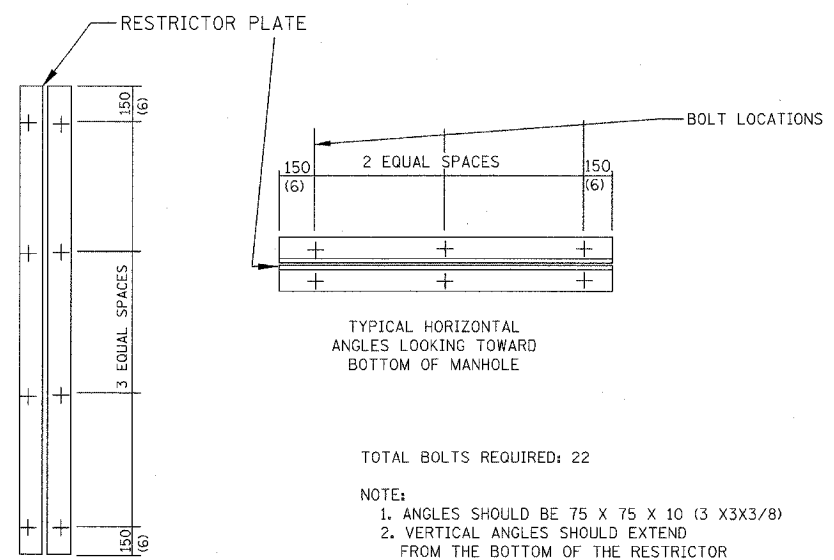
FAP DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338 (113 & 114) R-5	WILL	525	394	
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FAP 338 (IL RTE. 59)		



- NOTES:
1. ALL STEEL ANGLES AND PLATES TO BE GALVANIZED AFTER FABRICATION
 2. ALL RESTRICTOR PLATES, ANGLES AND HARDWARE TO BE INCLUDED IN THE COST OF THE MANHOLE.
 3. BASIS OF PAYMENT: "MANHOLES, TYPE A, TYPE I FRAME, CLOSED LID, RESTRICTOR PLATE" EACH, OF THE DIAMETER SPECIFIED



STR #	STATION	MANHOLE DIAMETER	FRAME AND GRATE	RESTRICTOR TYPE	INSIDE RESTRICTOR TYPE DIAMETER (d)	INVERT OF RESTRICTOR TYPE	ELEVATION OF TOP OF PLATE OVERFLOW	RIM ELEVATION
175	3261+40	6'	Type I-CL	2	8"	597.11	599.75	602.10
181	3261+60	6'	Type I-CL	2	7"	597.11	599.75	602.10
242	3284+80	6'	Type I-CL	2	6"	600.83	604.50	609.60
246	3285+67	6'	Type I-CL	2	14"	600.61	604.50	609.54
360	3319+72	7'	Type I-CL	2	14"	603.90	607.50	611.14



- TOTAL BOLTS REQUIRED: 22
- NOTE:
1. ANGLES SHOULD BE 75 X 75 X 10 (3 X3X3/8)
 2. VERTICAL ANGLES SHOULD EXTEND FROM THE BOTTOM OF THE RESTRICTOR PLATE TO THE TOP
 3. HORIZONTAL ANGLES SHOULD EXTEND FROM VERTICAL ANGLE TO VERTICAL ANGLE

1	2	3	4	5	6
RE-ENTRANT TUBE	SHARP EDGES	SQUARE EDGED	RE-ENTRANT TUBE	SQUARE EDGED	ROUNDED
LENGTH 1/2 TO 1 DIA.		STREAM CLEARS SIDES	LENGTH 2-1/2 DIA.	LENGTH 2-1/2 DIA.	
C=.52	C=.61	C=.61	C=.73	C=.82	C=.98

VALUES OF "C" FOR CIRCULAR AND SQUARE ORIFICES

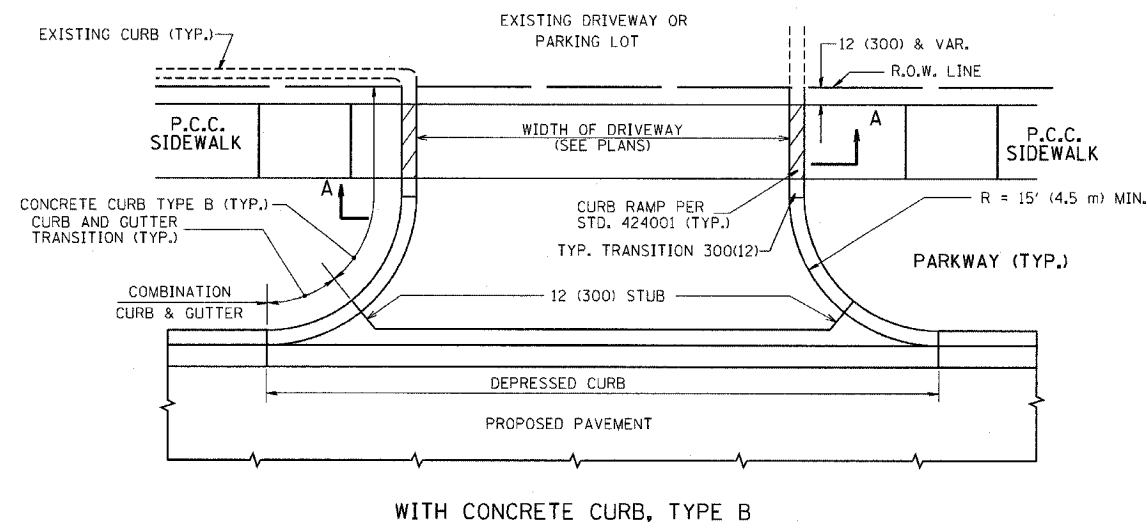
ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO. 1 SCHAUMBURG

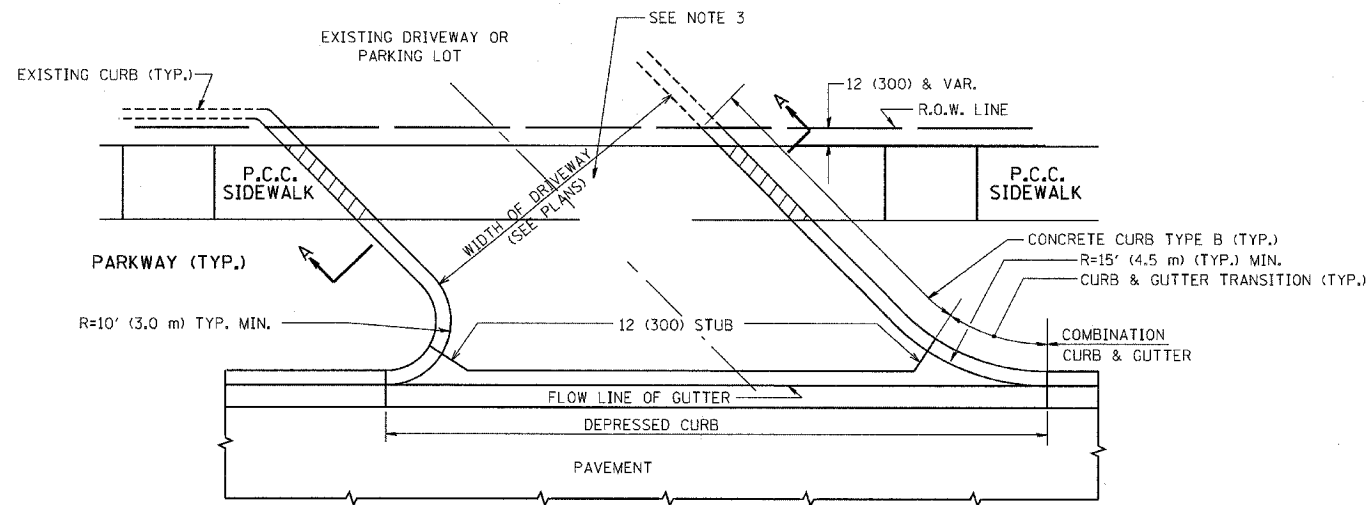
MANHOLE WITH RESTRICTOR PLATE DETAIL

SCALE NONE
DATE MARCH 18, 2008
DRAWN BY JWM
CHECKED BY PJD

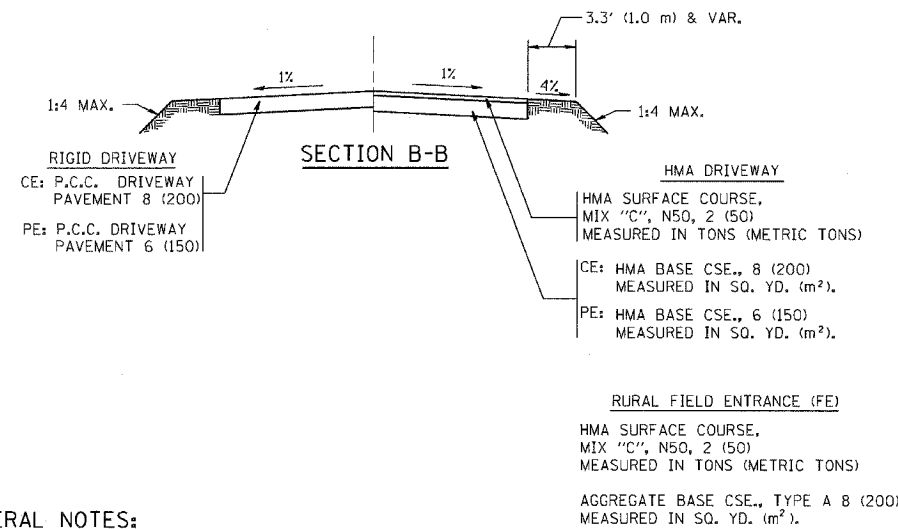
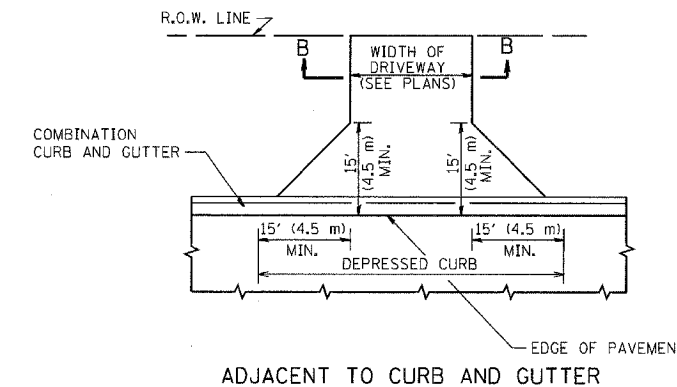
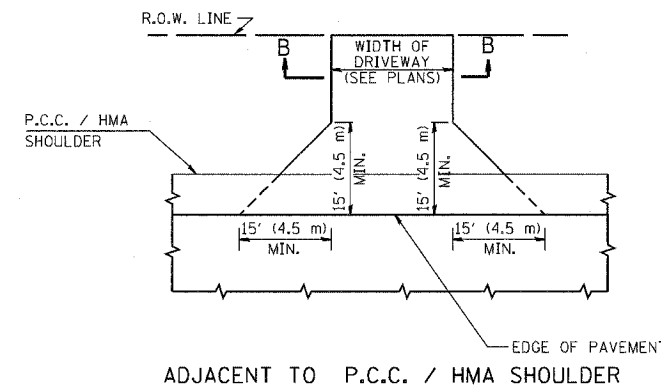
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			525	395
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

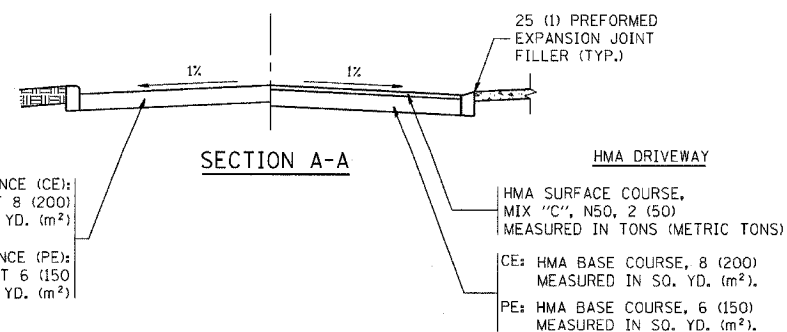
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED

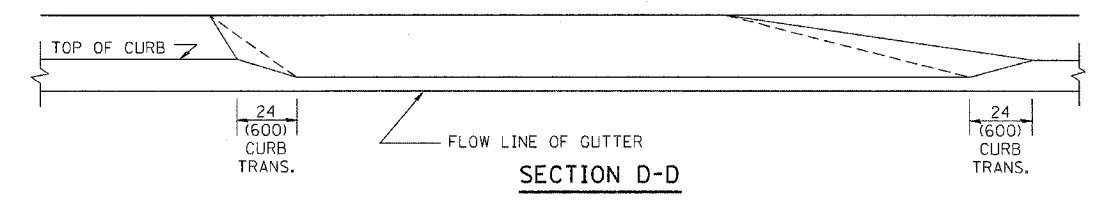
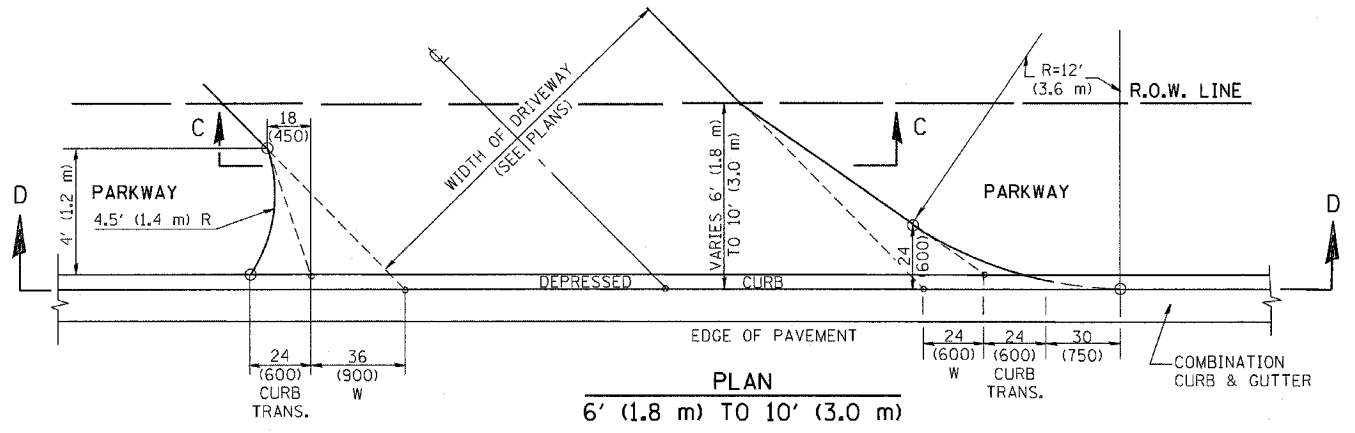
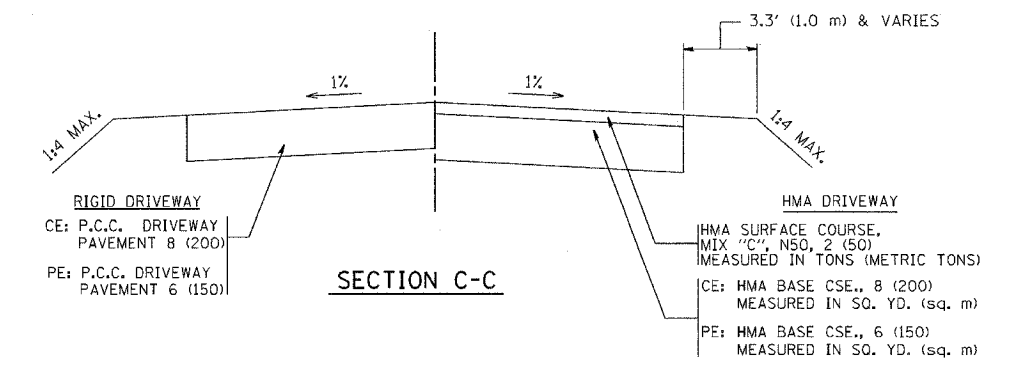
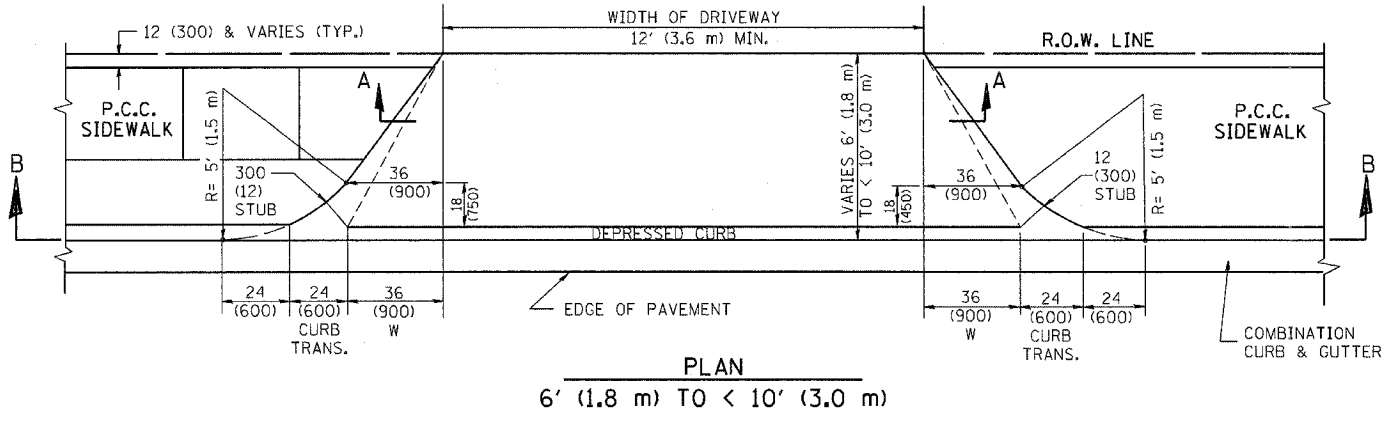
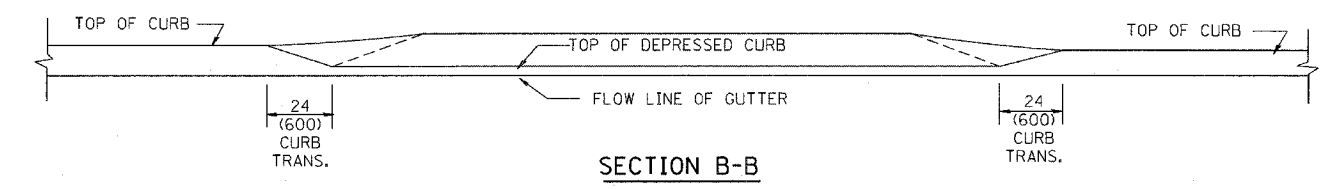
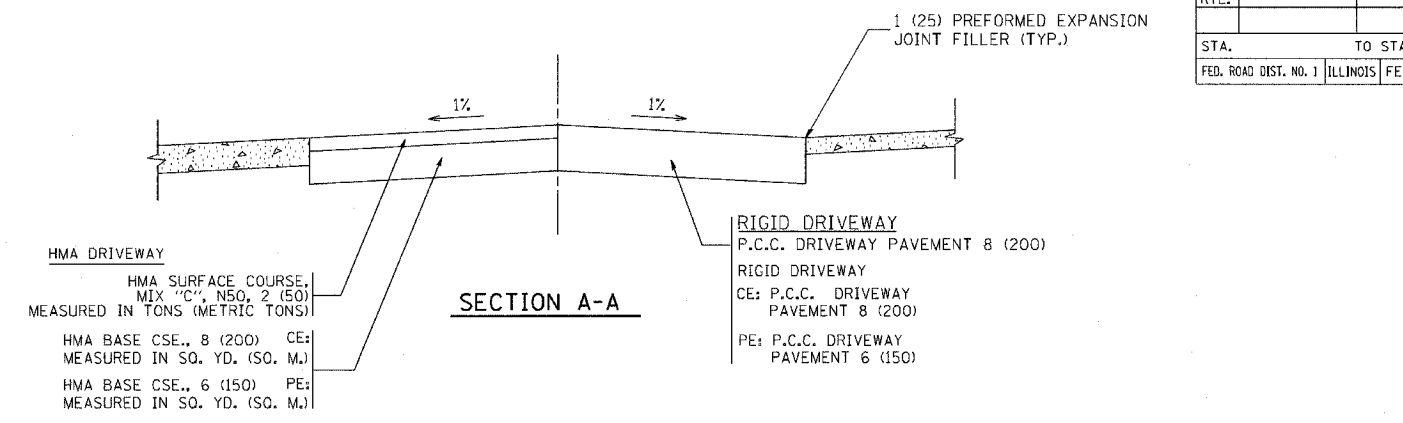
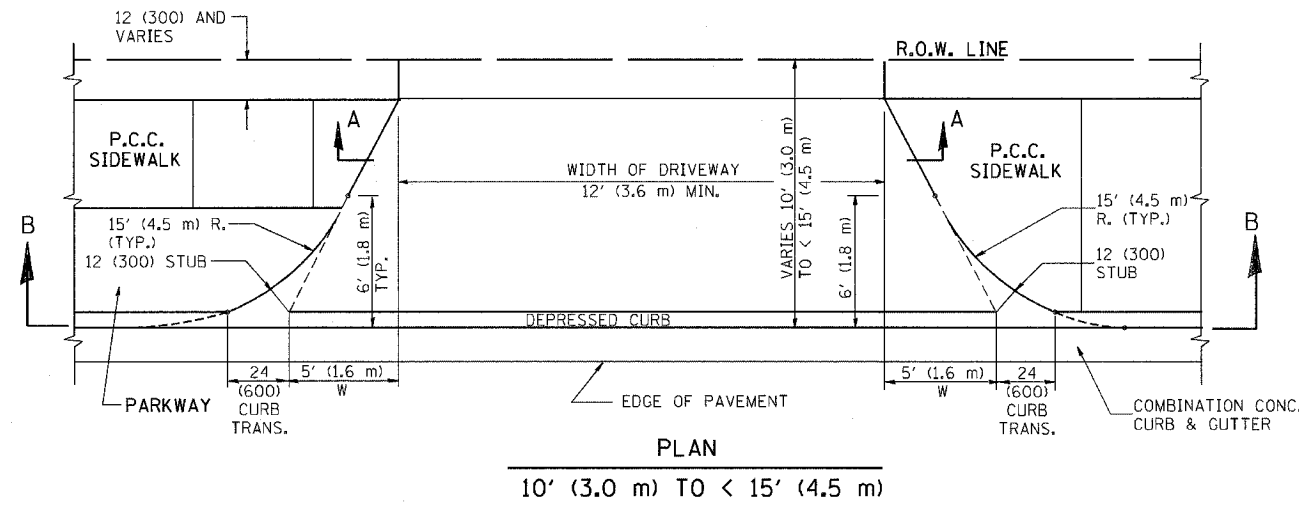
ILLINOIS DEPARTMENT OF TRANSPORTATION
DRIVEWAY DETAILS
 DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)

REVISIONS	
NAME	DATE
R. SHAH	11-04-95
J. POLLASTRINI	08-12-96
J. POLLASTRINI	12-14-96
A. ABBAS	03-21-97
T. HOLTZ	04-08-97
M. GOMEZ	04-06-01
P. LaFLEUR	04-15-03
R. BORO	01-01-07

SCALE: VERT. NONE
 HORIZ.

DRAWN BY
 CHECKED BY

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			525	396
STA.	TO STA.			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

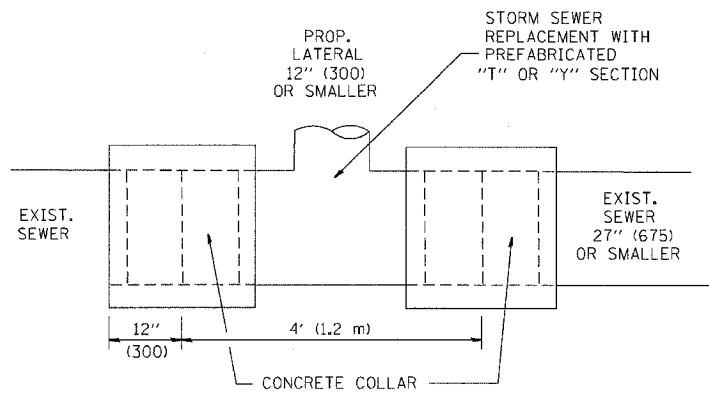
ILLINOIS DEPARTMENT OF TRANSPORTATION
DRIVEWAY DETAILS
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)

REVISIONS	
NAME	DATE
R. SHAH	11/06/95
J. POLLASTRINI	08/12/96
J. POLLASTRINI	12/14/96
A. ABBAS	03/21/97
T. HOLTZ	04/08/97
M. GOMEZ	04/06/01
P. LAFLEUR	04/15/03
R. BORO	01/01/07

SCALE: VERT. NONE
HORIZ.

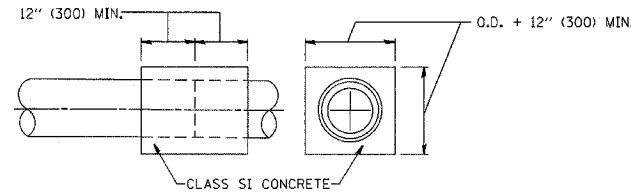
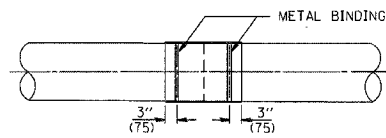
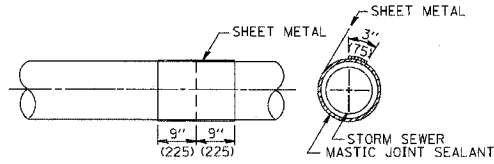
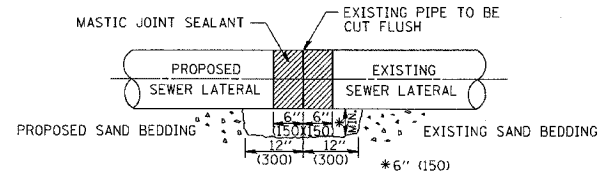
DRAWN BY
CHECKED BY

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			525	397
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

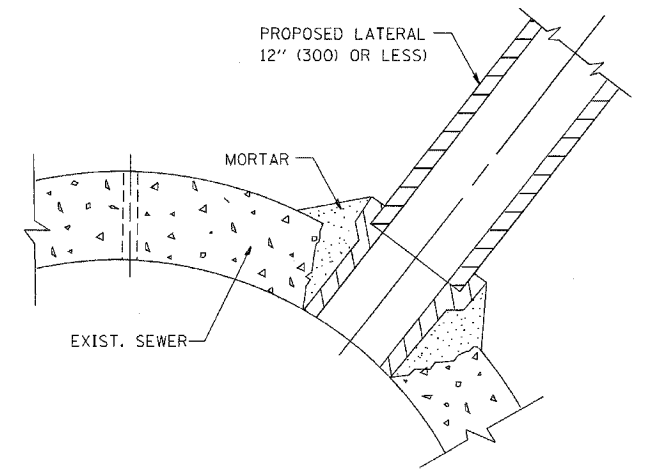


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6' (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
M. DE YONG	07/25/90
M. DE YONG	02/05/92
M. DE YONG	05/08/93
R. SHAH	09/09/94
R. SHAH	10/25/94
R. SHAH	06/12/96

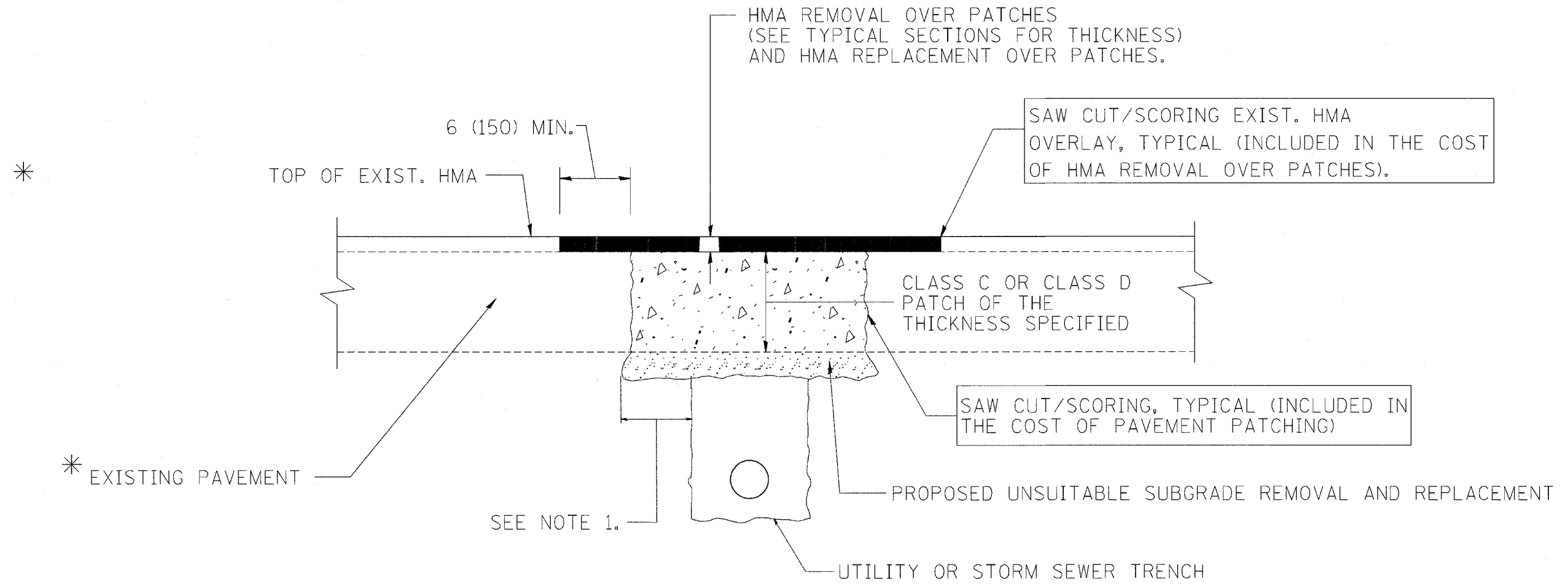
ILLINOIS DEPARTMENT OF TRANSPORTATION

DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER

SCALE: VERT. NONE
HORIZ.

DRAWN BY
CHECKED BY

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			525	398
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

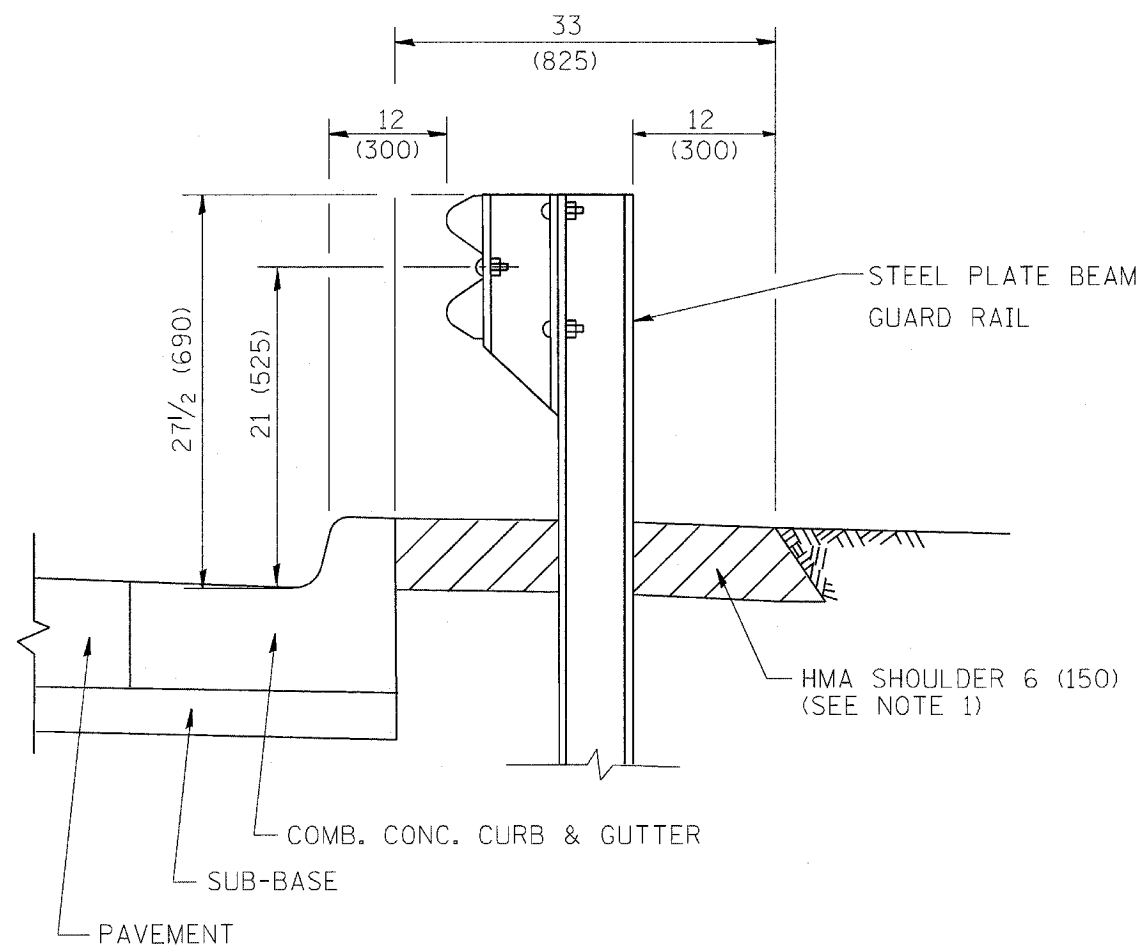
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/14/95
R. SHAH	03/23/95
R. SHAH	04/24/95
A. HOUSEH	03/15/96
A. ABBAS	03/21/97
A. ABBAS	01/20/98
ART ABBAS	04/27/98
R. BORO	01/01/07
R. BORO	09/04/07

ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT

SCALE: VERT. NONE
 HORIZ.

DRAWN BY
 CHECKED BY

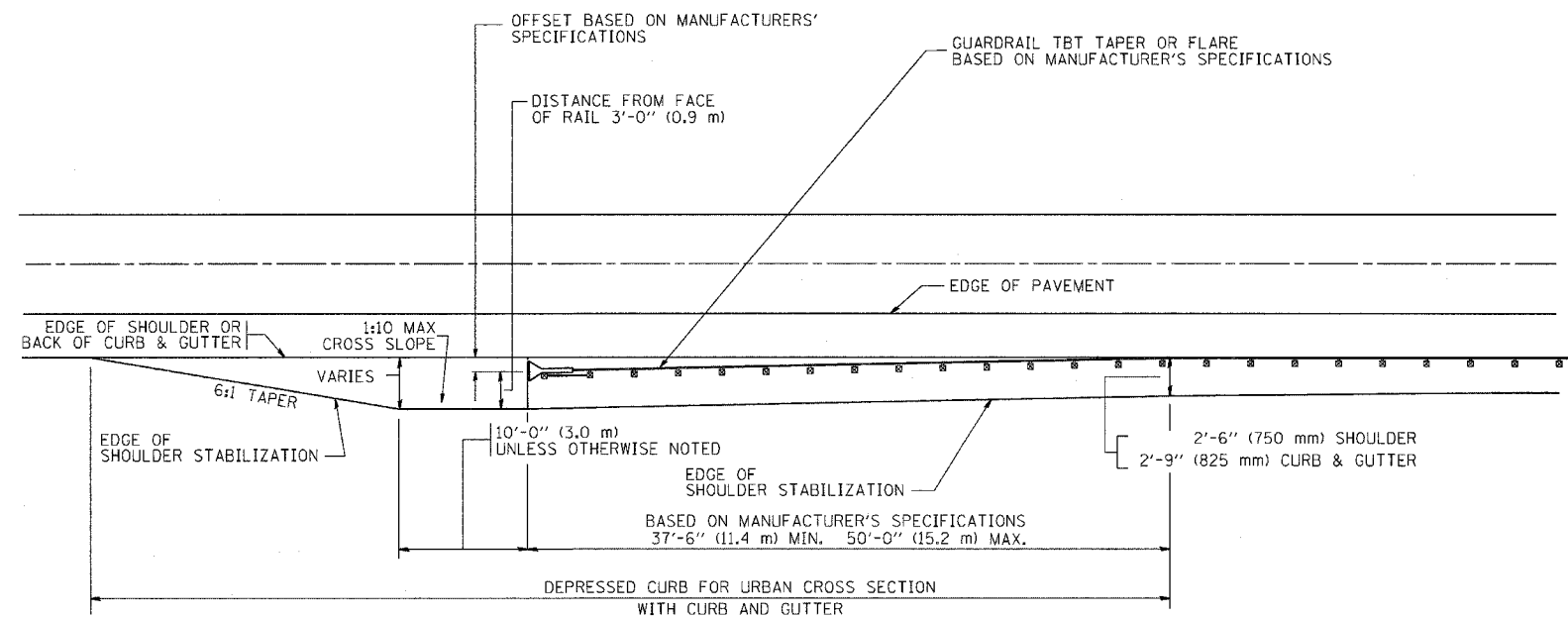


- NOTES: 1. THE HMA SHOULDER SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL
2. GUARD RAIL MAY BE PLACED AT THE BACK OF CURB WHEN DIRECTED BY THE ENGINEER.

BASIS OF PAYMENT: HMA SHOULDER 6 (150) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDER 6" (150 mm)".

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

**DETAILS FOR STEEL PLATE BEAM
GUARD RAIL ADJACENT TO CURB AND GUTTER
[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]**



STABILIZATION AT TBT TY. 1 SPL.

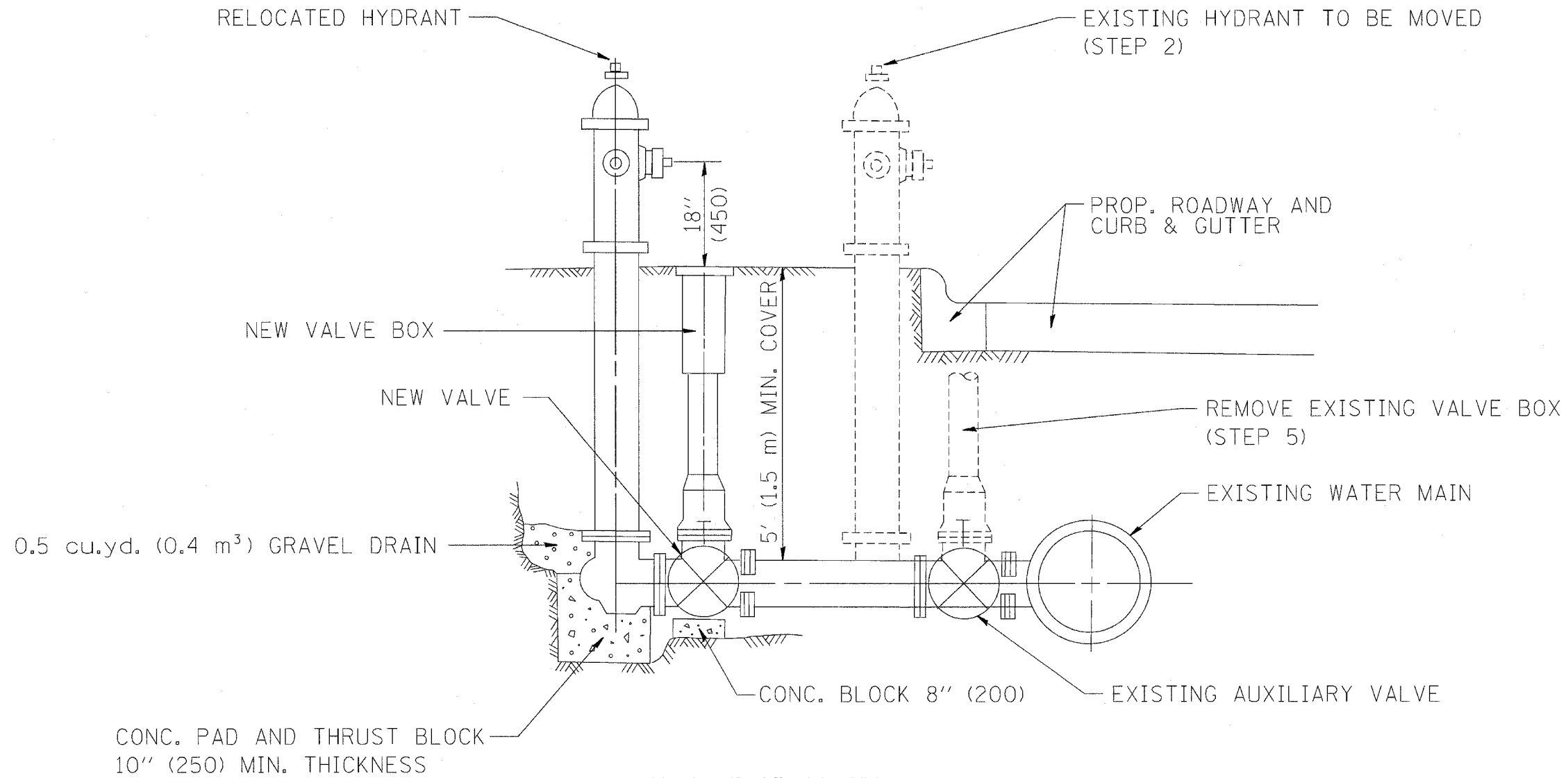
TBT = TRAFFIC BARRIER TERMINAL
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
M. DE YONG	09-22-90
M. DE YONG	07-14-92
R. SHAH	09/09/94
R. SHAH	10/25/94
R. SHAH	02/23/95
A. ABBAS	03/21/97
E. GOMEZ	08/28/00
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION
**DETAILS FOR
STEEL PLATE BEAM GUARD RAIL
ADJACENT TO CURB AND GUTTER
STABILIZATION AT TBT TY 1 SPL.**

SCALE: VERT. NONE
HORIZ. NONE
DRAWN BY JIS
CHECKED BY

CONTRACT NO.				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			525	400
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



SEQUENCE OF CONSTRUCTION:

1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. SHAH	09/09/94
R. SHAH	10/25/94

ILLINOIS DEPARTMENT OF TRANSPORTATION

FIRE HYDRANT TO BE MOVED

SCALE: VERT. NONE
HORIZ.

DRAWN BY
CHECKED BY