

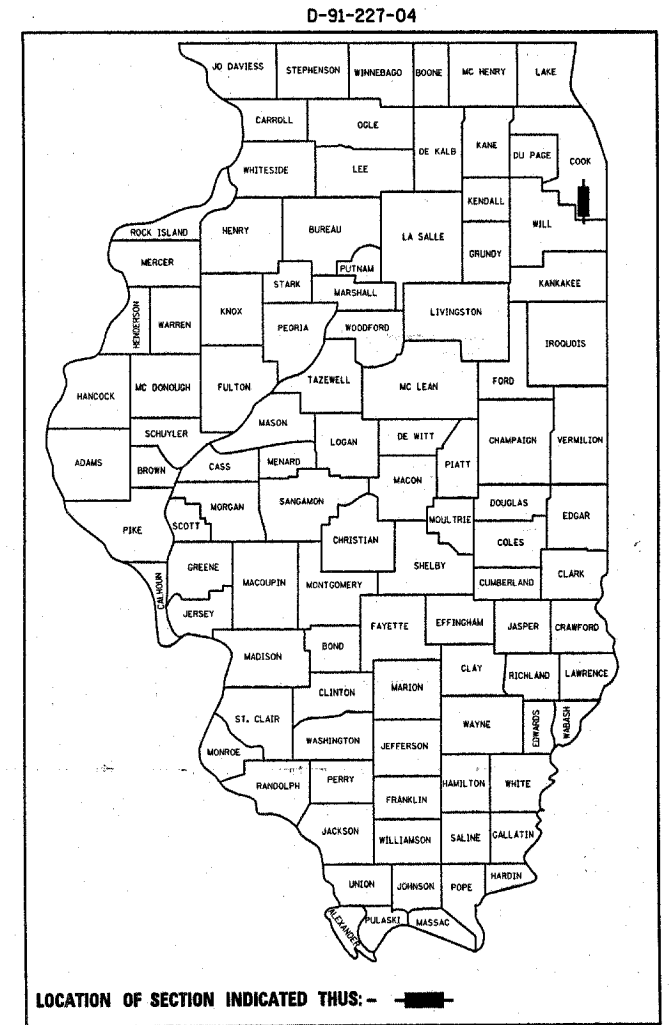
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2860	2004-047 RS	COOK	34	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 62765		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**
F.A.U. ROUTE 2860 (RIEGEL ROAD)
RIDGE ROAD TO JOE ORR ROAD
SECTION: 2004-047 RS
RESURFACING (MAINTENANCE)
PROJECT: ACM-2860(002)
COOK COUNTY
C-91-227-04

FOR INDEX OF SHEETS, SEE SHEET NO. 2

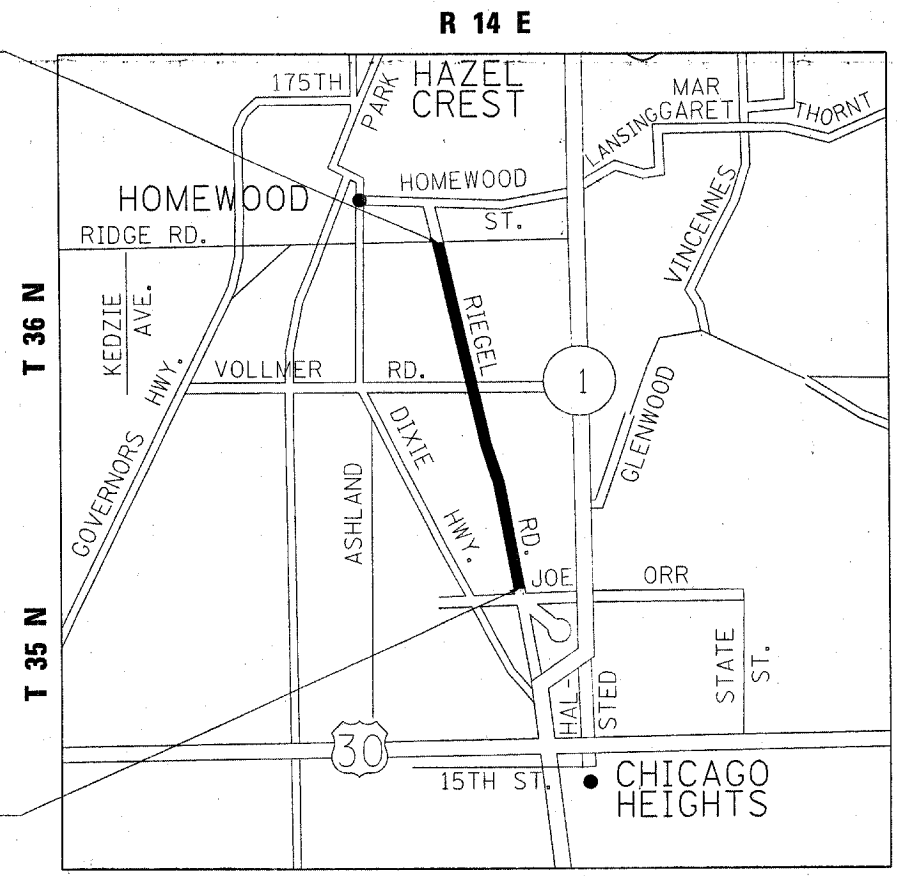
PROJECT IS LOCATED IN
THE VILLAGES OF CHICAGO
HEIGHTS AND HOMEWOOD



PROJECT ENDS
258 + 63

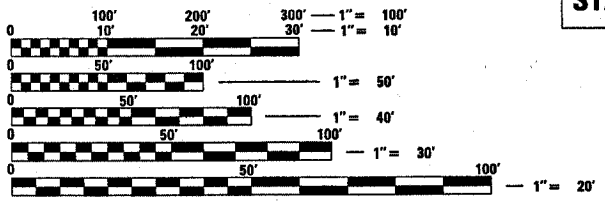
OMISSIONS:
STA. 133 + 70 TO STA. 134 + 79
STA. 170 + 20 TO STA. 178 + 70

PROJECT BEGINS
105 + 08



TRAFFIC DATA

RIEGEL ROAD
2006 ADT = 10,000
POSTED SPEED LIMIT = VARIES 30-40 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: SUNG H. BYUN
PROJECT MANAGER: KEN ENG

GROSS LENGTH OF PROJECT = 15,355 LIN. FT. = 2.91 MI.
NET LENGTH OF PROJECT = 14,401 LIN. FT. = 2.73 MI.

CONTRACT NO. 62765

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED MARCH 31, 20 08

Diana M. O'Keefe DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
May 9, 20 08
Eric E. Harms INTERIM ENGINEER OF DESIGN AND ENVIRONMENT
May 9, 20 08
Christine M. Reed DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

DISTRICT ONE DESIGN PLAN PREPARATION ENGINEER:
KEN ENG (847) 705-4247

INDEX OF SHEETS:

STATE STANDARDS:

GENERAL NOTES:

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STANDARDS, AND GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5-7	TYPICAL SECTIONS
8-13	RIEGLE ROAD PROPOSED ROADWAY/PAVEMENT MARKING PLANS
14-19	DETECTOR LOOP REPLACEMENT PLANS
20	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
21	PAVEMENT PATCHING FOR BITUMINOUS SURFACE PAVEMENT
22	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
23	BUTT JOINT AND BITUMINOUS TAPER DETAILS
24	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
25	RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)
26	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
27	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
28	PAVEMENT MARKINGS, LETTERS AND SYMBOLS FOR TRAFFIC STAGING
29	ARTERIAL ROAD INFORMATION SIGN
30-33	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAIL
34	DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING

000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
424001-05	CURB RAMPS FOR SIDEWALKS
701301-02	LANE CLOSURE, 2L 2W, SHORT TIME OPERATIONS
701306-01	LANE CLOSURE, 2L 2W SLOW MOVING DAY ONLY OPERATIONS, FOR SPEEDS GREATER THAN OR EQUAL TO 45 MPH
701336-04	LANE CLOSURE, 2L, 2W WORK AREAS IN SERIES FOR SPEEDS GREATER THAN OR EQUAL TO 45 MPH
701501-04	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701601-05	URBAN LANE CLOSURE, MULTILANE 1W OR 2W WITH NON-TRAVERSIBLE MEDIAN
701606-05	LANE CLOSURE, MULTILANE, 2-W, WITH MOUNTABLE MEDIAN
701701-05	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-03	LANE CLOSURE, MULTILANE, 1-W OR 2-W, CROSSWALK OR SIDEWALK CLOSURE
701901	TRAFFIC CONTROL DEVICES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).

3 METER (10 FEET) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS & GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE VILLAGES OF CHICAGO HEIGHTS AND HOMEWOOD.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 40 MM (1 1/2 INCHES WHERE THE SPEED LIMIT IS 80 KM/H (45 MPH) OR LESS AND 25 MM (1 INCH) WHERE THE SPEED LIMIT IS GREATER THAN 80 KM/H (45 MPH)). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 75 MM (3 INCHES) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

THE ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705- 4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TRAFFIC CONTROL DEVICES

PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL. TWO WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS, CONTACT MS. PATRICE HARRIS, AREA TRAFFIC FIELD TECHNICIAN AT (708) 597-9800

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE CATCH BASIN/INLET LIDS HAVE A SPECIAL ENVIRONMENTAL MONUMENT ATTACHED STATING NO DUMPING ALLOWED. WHEN A FRAME AND LID IS REPLACED, THESE MONUMENTS SHALL BE ATTACHED. THE COST OF THE MONUMENT SHALL BE INCLUDED IN THE NEW FRAME AND LIDS

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AC TYPE	AIR VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 MM)	PG 64-22	4% @ 70 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	SBS/SBR PG 76-28/-22	4% @ 50 GYR
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES, (HMA BINDER IL-19.0 MM)	PG 64-22*	4% @ 70 GYR
ALL CLASS D PATCHES, (HMA BINDER IL-19 MM)	PG 64-22*	4% @ 70 GYR

NOTE: THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAU 2860/ RIEGLE ROAD RIDGE RD. TO JOE ORR RD. INDEX OF SHEETS, LIST OF STATE STANDARDS AND GENERAL NOTES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
*FILE#		DRAWN -	REVISED -			2860	2004-047 RS	COOK	34	2	
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	PLOT DATE = #DATE#	DATE -	REVISED -			SCALE:	SHEET NO. OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

SUMMARY OF QUANTITIES			URBAN		CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES 80% FED. 20% STATE	1000-2A					
20201006	DEPRESSED CURB RAMP ACCESSIBLE TO THE DISABLED, TYPE B	SQ FT	100	100					
20201006	GRADING AND SHAPING SHOULDERS	UNIT	26	26					
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	21	21					
40600300	AGGREGATE (PRIME COAT)	TON	103	103					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	5	5					
40600895	CONSTRUCTING TEST STRIP	EACH	1	1					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	102.5	102.5					
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	566	566					
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	5058	5058					
42001300	PROTECTIVE COAT	SQ YD	17	17					
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	51586	51586					
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	140	140					
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	110	110					
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SQ YD	2527	2527					
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	798	798					
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	829	829					
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	906	906					
48101200	AGGREGATE SHOULDERS, TYPE B	TON	1050	1050					
60261320	INLETS TO BE ADJUSTED WITH NEW TYPE 11V FRAME AND GRATE	EACH	10	10					
60266600	VALVE BOXES TO BE ADJUSTED	EACH	5	5					
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	30	30					
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	110	110					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6					
67100100	MOBILIZATION	L SUM	1	1					
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1					

SUMMARY OF QUANTITIES			URBAN		CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES 80% FED. 20% STATE	1000-2A					
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1					
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1					
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1					
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	2081	2081					
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	291	291					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	58548	58548					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1362.5	1362.5					
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	162	162					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	489	489					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	209	209					
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	600	600					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	291	291					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	58548	58548					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1362.5	1362.5					
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	162	162					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	489	489					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	209	209					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1000	1000					
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	800	800					

*Specialty Items

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
FAU 2860 (RIEGLE ROAD)
FAU 2843 (DIXIE HWY)

4/3/2008 10:10:30 AM

SUMMARY OF QUANTITIES			URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES 80% FED. 20% STATE	1000-2A				
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1700	1700				
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4				
X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	2200	2200				
XX005089	BITUMINOUS DRIVEWAY PAVEMENT 6"	SQ YD	140	140				
NP Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	10	10				
42400300	PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH	SQ FT	100	100				
42400800	DETECTABLE WARNING	SQ FT	12	12				
41000600	SIDEWALK REMOVAL	SQ FT	100	100				
⊙ Z0076600	TRAINERS	HOUR	1000	1000				

SUMMARY OF QUANTITIES			TOTAL	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	QUANTITIES	1000-2A				

NP= Non-participating
 * - Specialty Items
 ⊙ - Y080

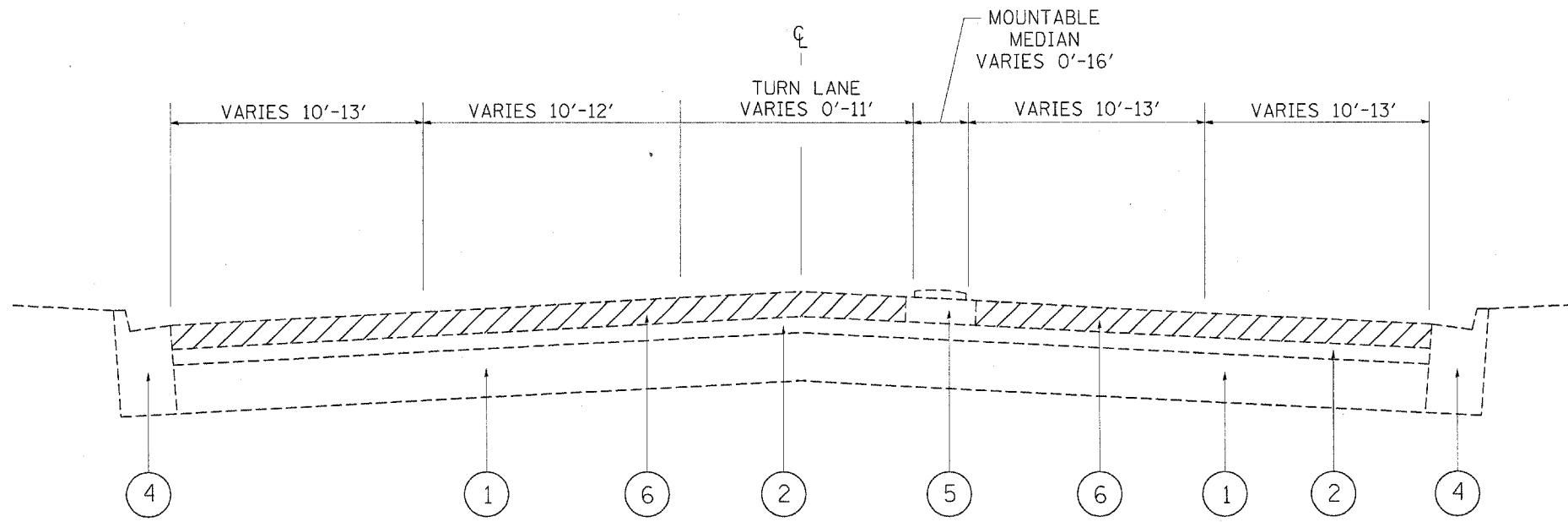
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SUMMARY OF QUANTITIES
 FAU 2860 (RIEGLE ROAD)
 FAU 2843 (DIXIE HWY)

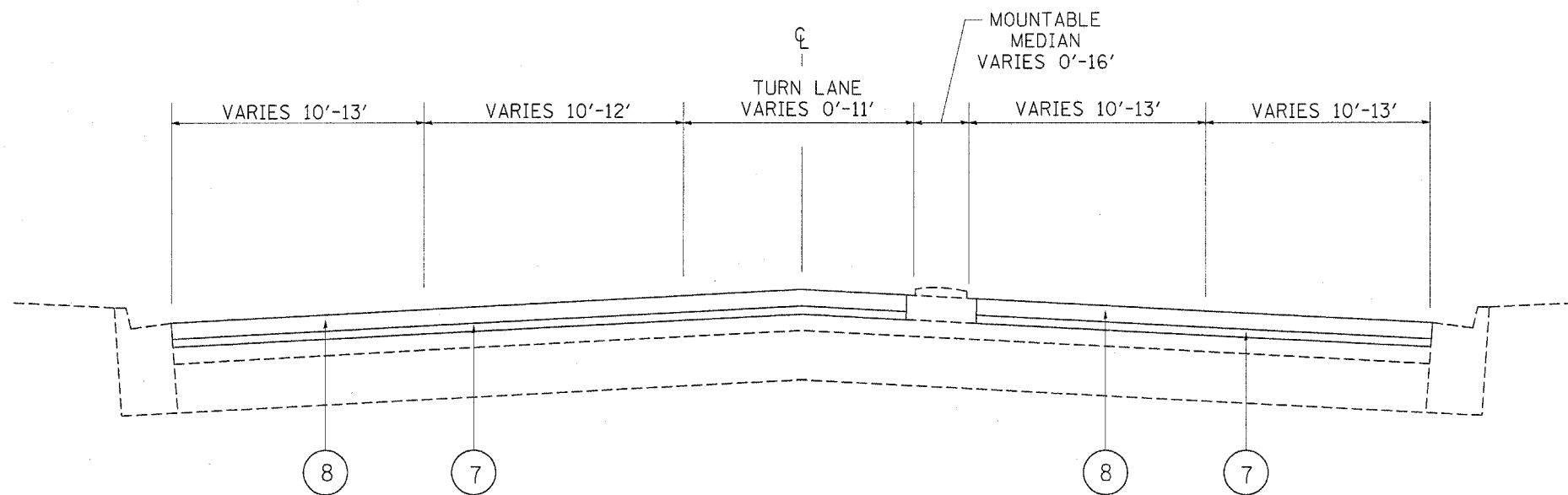
Rev.

PLOT DATE: 4/2/2008

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EXISTING TYPICAL SECTION
 RIEGLE ROAD
 STA. 105+08 TO STA. 111+05

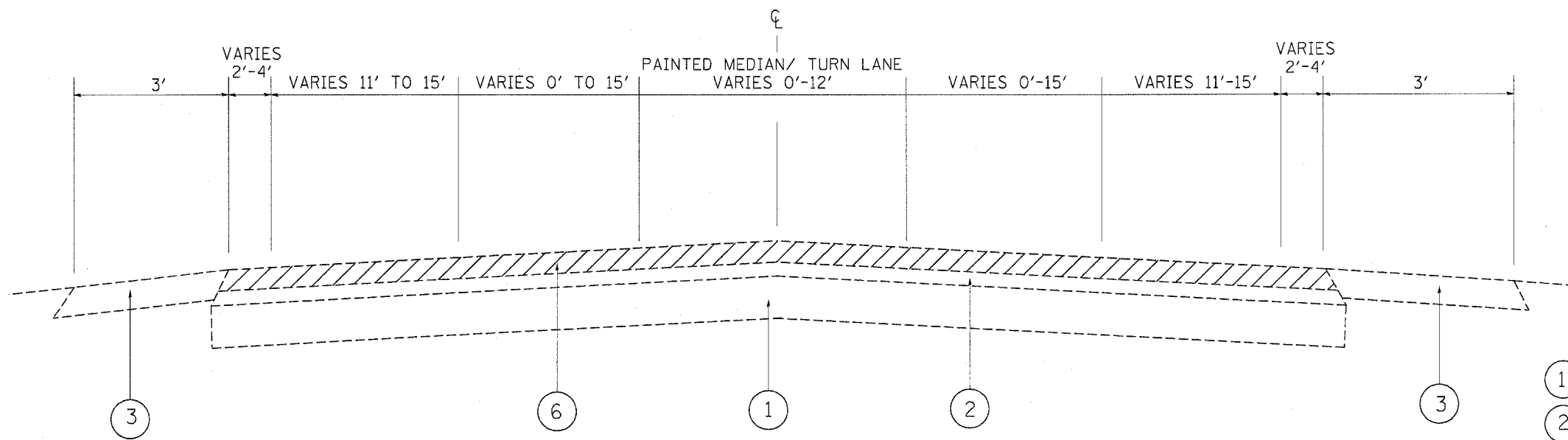


PROPOSED TYPICAL SECTION
 RIEGLE ROAD
 STA. 105+08 TO STA. 111+05

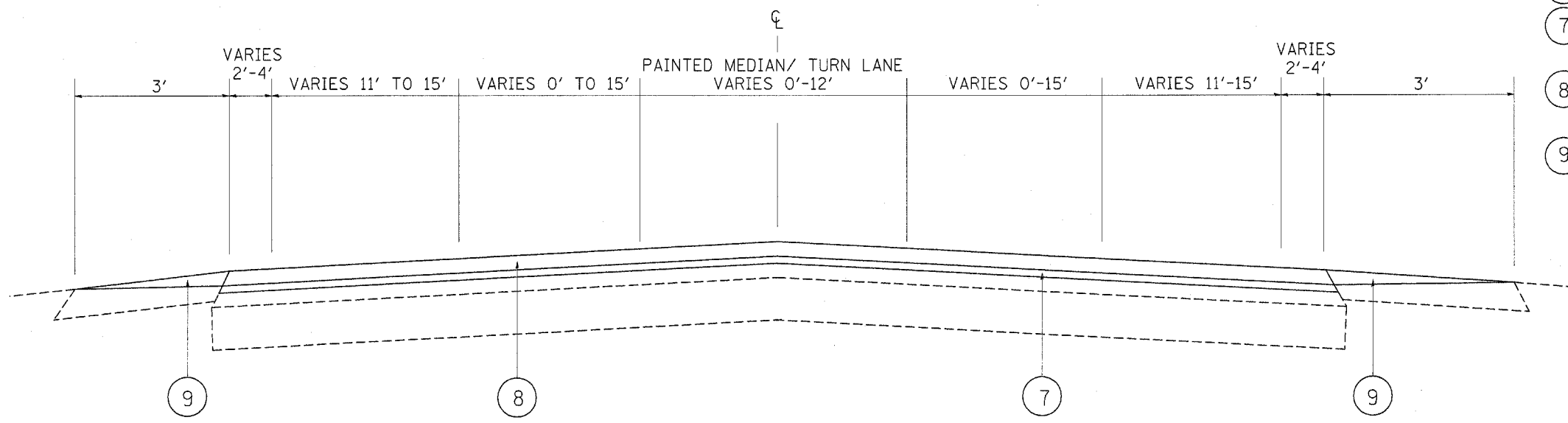
LEGEND

- ① EXISTING PCC PAVEMENT, ±9"
- ② EXISTING HMA SURFACE COURSE, ±5"
- ③ EXISTING AGGREGATE SHOULDER
- ④ EXISTING CURB AND GUTTER, TYPE B 6.12
- ⑤ EXISTING MOUNTABLE MEDIAN
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑦ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑧ PROPOSED HOT-MIX ASPHALT CONCRETE SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑨ AGGREGATE SHOULDERS, TYPE B

FILE NAME =	USER NAME = *USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAU 2860/ RIEGLE ROAD RIDGE RD. TO JOE ORR RD. TYPICAL SECTION	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FILEL	DRAWN -	REVISED -	2860			2004-047 RS	COOK	34	5	
PLOT SCALE = *SCALE*	CHECKED -	REVISED -	CONTRACT NO. 62765							
PLOT DATE = *DATE*	DATE -	REVISED -	SCALE:			SHEET NO. 1 OF 3 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



EXISTING TYPICAL SECTION
 RIEGLE ROAD
 STA. 111+05 TO STA. 247+14
 OMISSION STA. 170+20 TO STA. 178+70

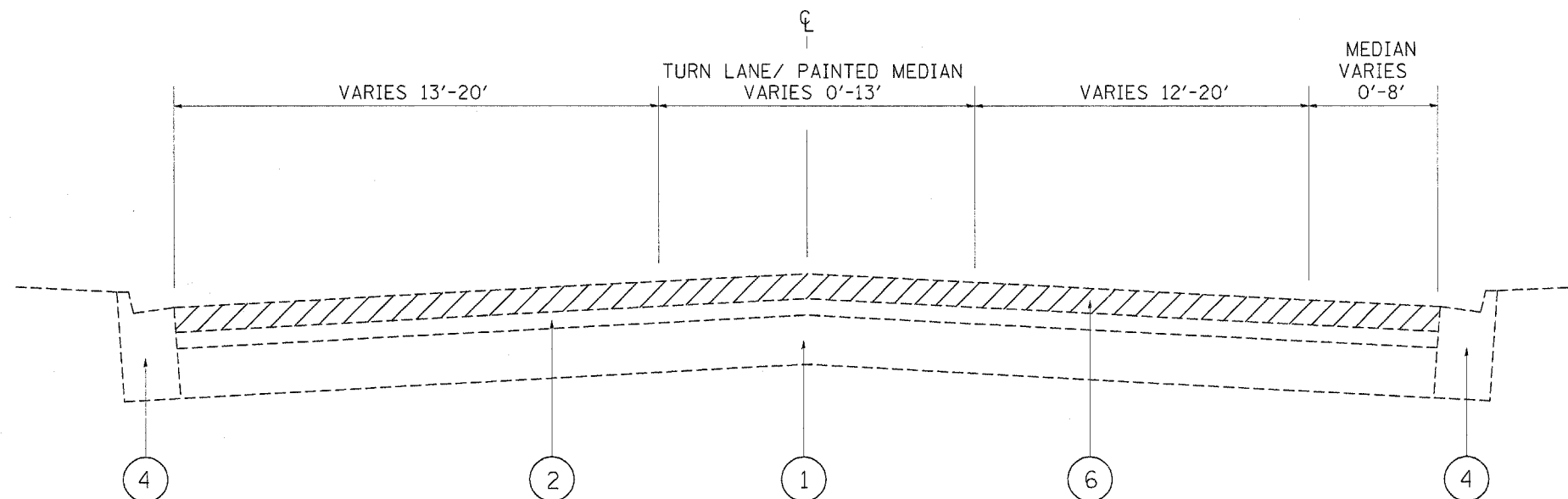


PROPOSED TYPICAL SECTION
 RIEGLE ROAD
 STA. 111+05 TO STA. 247+14
 OMISSION STA. 170+20 TO STA. 178+70

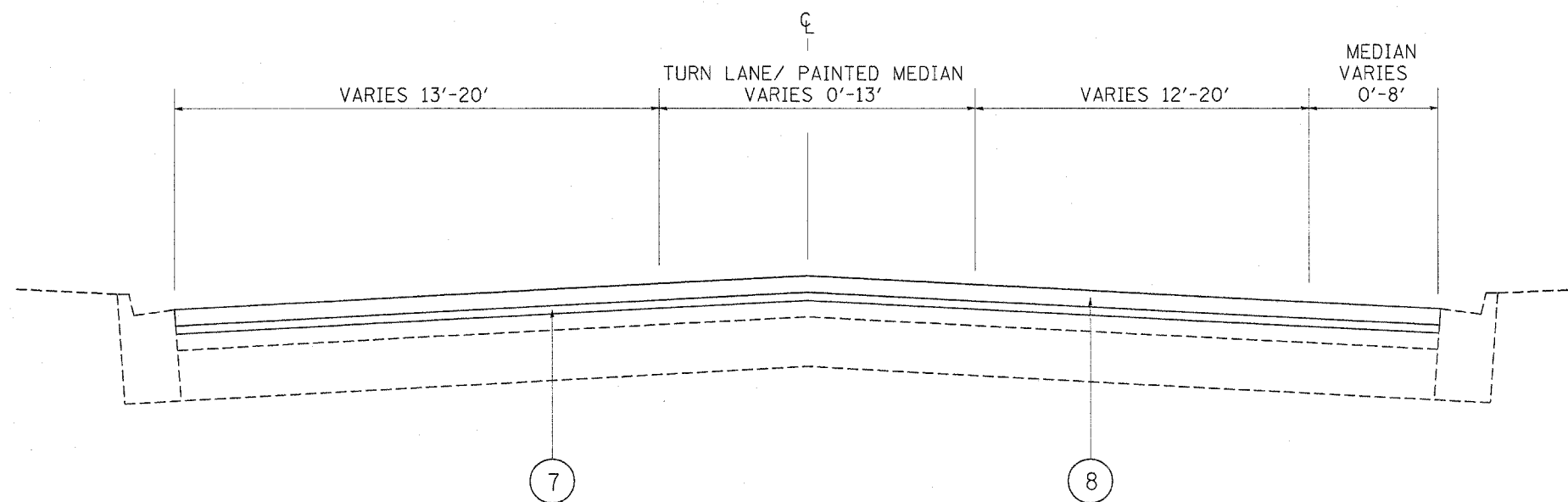
LEGEND

- ① EXISTING PCC PAVEMENT, ±9"
- ② EXISTING HMA SURFACE COURSE, ±5"
- ③ EXISTING AGGREGATE SHOULDER
- ④ EXISTING CURB AND GUTTER, TYPE B 6.12
- ⑤ EXISTING MOUNTABLE MEDIAN
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑦ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑧ PROPOSED HOT-MIX ASPHALT CONCRETE SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑨ AGGREGATE SHOULDERS, TYPE B

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAU 2860/ RIEGLE ROAD RIDGE RD. TO JOE ORR RD. TYPICAL SECTION		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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		CHECKED -	REVISED -				TO STA.					
		DATE -	REVISED -					CONTRACT NO. 62765				
							FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



EXISTING TYPICAL SECTION
 RIEGLE ROAD
 STA. 247+14 TO STA. 258+63

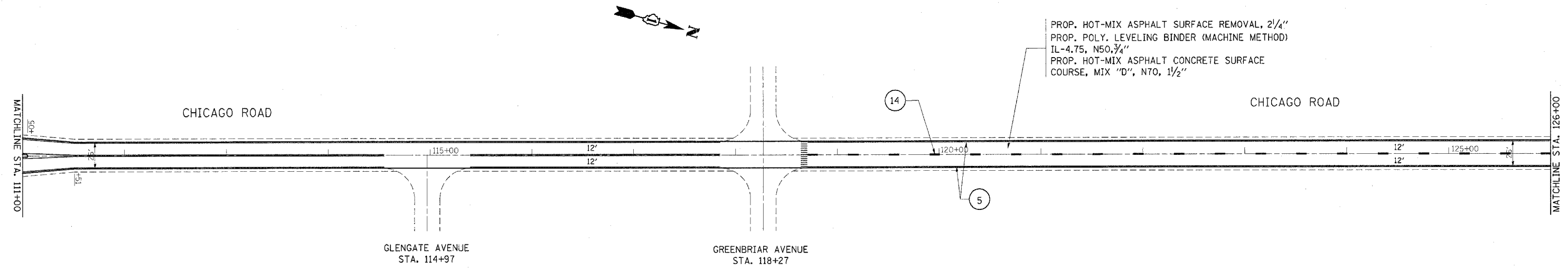
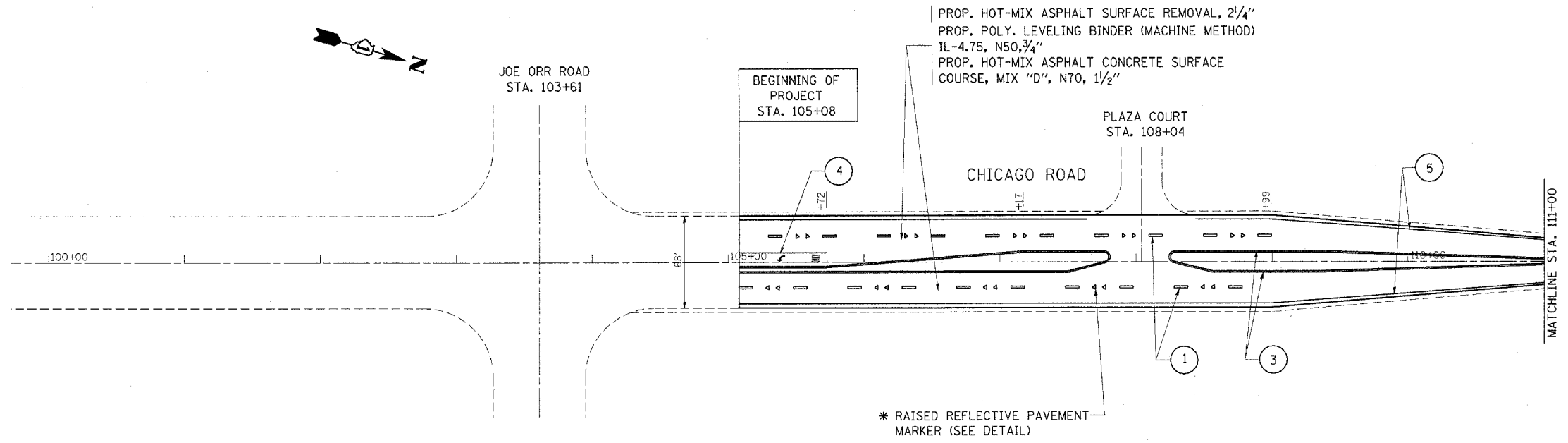


PROPOSED TYPICAL SECTION
 RIEGLE ROAD
 STA. 247+14 TO STA. 258+63

LEGEND

- ① EXISTING PCC PAVEMENT, ±9"
- ② EXISTING HMA SURFACE COURSE, ±5"
- ③ EXISTING AGGREGATE SHOULDER
- ④ EXISTING CURB AND GUTTER, TYPE B 6.12
- ⑤ EXISTING MOUNTABLE MEDIAN
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑦ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑧ PROPOSED HOT-MIX ASPHALT CONCRETE SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑨ AGGREGATE SHOULDERS, TYPE B

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAU 2860/ RIEGLE ROAD RIDGE RD. TO JOE ORR RD. TYPICAL SECTION		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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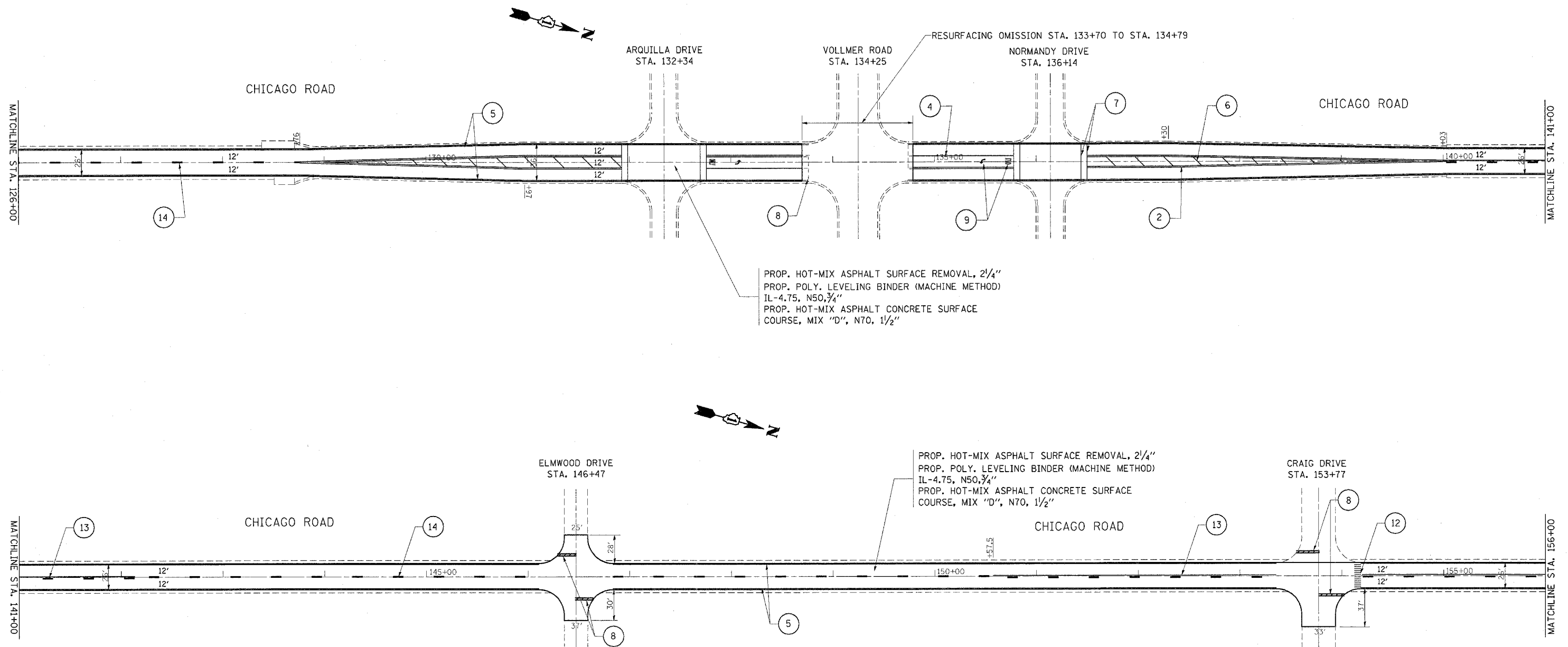


* REFER TO RAISED REFLECTIVE PAVEMENT MARKER (SNOW PLOW RESISTANT DETAIL FOR LOCATION AND SPACING OF LANE LINE, CENTERLINE AND CHANNELIZATION PAVEMENT MARKERS.

LEGEND

- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING- LINE 4", SKIP DASH @ 30' SKIP AND 10' DASH, WHITE (TYP.)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", DOUBLE LINE @ 11" C-C, YELLOW (TYP.)
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", MEDIAN LINE, YELLOW (TYP.)
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6" TURN LANE LINE, WHITE (TYP.)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4" EDGE LINE, WHITE (TYP.)
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12" DIAGONAL @ 45 DEG. (5 MIN.), YELLOW (TYP.)
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING- LINE 6", CROSS WALK, WHITE (TYP.)
- 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP BAR, WHITE (TYP.)
- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.)
- 10 PROPOSED THERMOPLASTIC PAVEMENT MARKING- LINE 6", DOTTED LINE @ 2' DASH AND 6' SKIP, WHITE (TYP.)
- 11 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12", DIAGONAL @ 45 DEG. (5 MIN.), WHITE (TYP.)
- 12 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", SCHOOL CROSSWALK, WHITE (TYP.)
- 13 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", NO PASSING ZONE LINE, YELLOW (TYP.)
- 14 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", SKIP DASH @ 30' SKIP AND 10' DASH, YELLOW (TYP.)

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAU 2860/ RIEGLE ROAD RIDGE RD. TO JOE ORR RD. PROPOSED ROADWAY/PAVEMENT MARKING PLAN	F.A.U. RTE. 2860	SECTION 2004-047 RS	COUNTY COOK	TOTAL SHEETS 34	SHEET NO. 8	
#FILE#		DRAWN -	REVISED -		SCALE:	SHEET NO. 1 OF 6 SHEETS	STA.	TO STA.	CONTRACT NO. 62765		
		CHECKED -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						
		DATE -	REVISED -								



PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD)
 IL-4.75, N50, 3/4"
 PROP. HOT-MIX ASPHALT CONCRETE SURFACE COURSE, MIX "D", N70, 1 1/2"

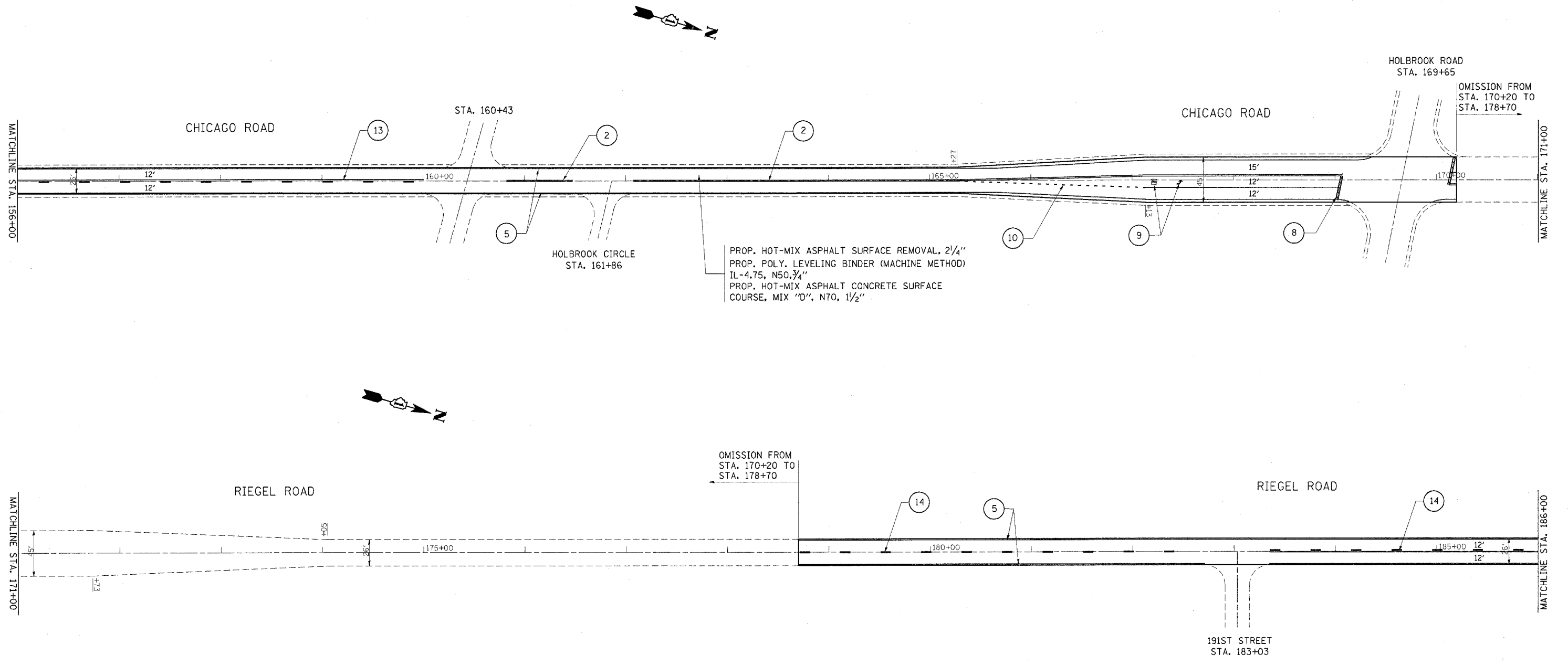
PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD)
 IL-4.75, N50, 3/4"
 PROP. HOT-MIX ASPHALT CONCRETE SURFACE COURSE, MIX "D", N70, 1 1/2"

* REFER TO RAISED REFLECTIVE PAVEMENT MARKER (SNOW PLOW RESISTANT DETAIL FOR LOCATION AND SPACING OF LANE LINE, CENTERLINE AND CHANNELIZATION PAVEMENT MARKERS.

LEGEND

- ① PROPOSED THERMOPLASTIC PAVEMENT MARKING- LINE 4", SKIP DASH @ 30' SKIP AND 10' DASH, WHITE (TYP.)
- ② PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", DOUBLE LINE @ 11" C-C, YELLOW (TYP.)
- ③ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", MEDIAN LINE, YELLOW (TYP.)
- ④ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6" TURN LANE LINE, WHITE (TYP.)
- ⑤ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4" EDGE LINE, WHITE (TYP.)
- ⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12" DIAGONAL @ 45 DEG. (5 MIN.), YELLOW (TYP.)
- ⑦ PROPOSED THERMOPLASTIC PAVEMENT MARKING- LINE 6", CROSS WALK, WHITE (TYP.)
- ⑧ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP BAR, WHITE (TYP.)
- ⑨ PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.)
- ⑩ PROPOSED THERMOPLASTIC PAVEMENT MARKING- LINE 6", DOTTED LINE @ 2' DASH AND 6' SKIP, WHITE (TYP.)
- ⑪ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12", DIAGONAL @ 45 DEG. (5 MIN.), WHITE (TYP.)
- ⑫ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", SCHOOL CROSSWALK, WHITE (TYP.)
- ⑬ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", NO PASSING ZONE LINE, YELLOW (TYP.)
- ⑭ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", SKIP DASH @ 30' SKIP AND 10' DASH, YELLOW (TYP.)

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAU 2860/ RIEGLE ROAD RIDGE RD. TO JOE ORR RD. PROPOSED ROADWAY/PAVEMENT MARKING PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
#FILE#		DRAWN -	REVISED -		SCALE:	SHEET NO. 2 OF 6 SHEETS	2860	2004-047 RS	COOK	34	9
		CHECKED -	REVISED -			STA. TO STA.					
		DATE -	REVISED -								CONTRACT NO. 62765
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD)
 IL-4.75, N50, 3/4"
 PROP. HOT-MIX ASPHALT CONCRETE SURFACE
 COURSE, MIX "D", N70, 1 1/2"

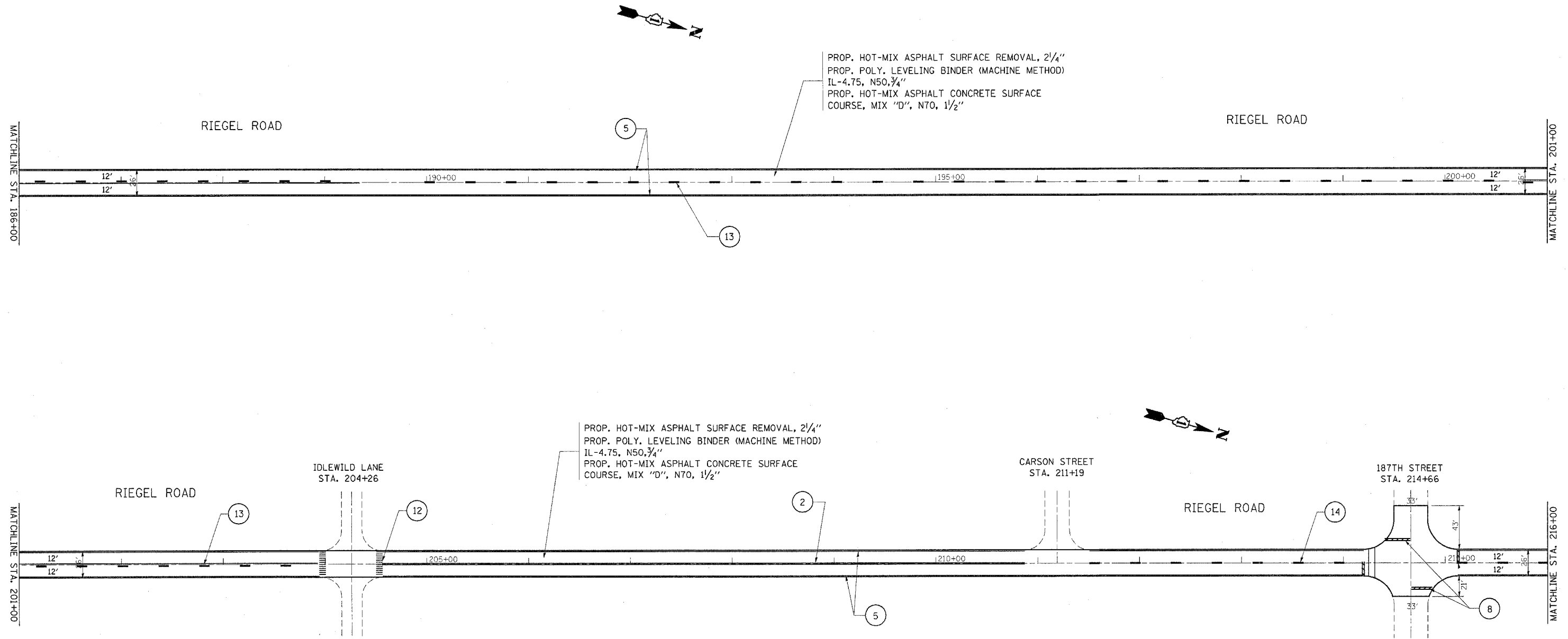
OMISSION FROM
 STA. 170+20 TO
 STA. 178+70

LEGEND

- | | |
|---|--|
| 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING- LINE 4", SKIP DASH @ 30' SKIP AND 10' DASH, WHITE (TYP.) | 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP BAR, WHITE (TYP.) |
| 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", DOUBLE LINE @ 11" C-C, YELLOW (TYP.) | 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.) |
| 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", MEDIAN LINE, YELLOW (TYP.) | 10 PROPOSED THERMOPLASTIC PAVEMENT MARKING- LINE 6", DOTTED LINE @ 2' DASH AND 6' SKIP, WHITE (TYP.) |
| 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6" TURN LANE LINE, WHITE (TYP.) | 11 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12", DIAGONAL @ 45 DEG. (5 MIN.), WHITE (TYP.) |
| 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4" EDGE LINE, WHITE (TYP.) | 12 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", SCHOOL CROSSWALK, WHITE (TYP.) |
| 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12" DIAGONAL @ 45 DEG. (5 MIN.), YELLOW (TYP.) | 13 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", NO PASSING ZONE LINE, YELLOW (TYP.) |
| 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING- LINE 6", CROSS WALK, WHITE (TYP.) | 14 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", SKIP DASH @ 30' SKIP AND 10' DASH, YELLOW (TYP.) |

* REFER TO RAISED REFLECTIVE PAVEMENT MARKER (SNOW PLOW RESISTANT DETAIL FOR LOCATION AND SPACING OF LANE LINE, CENTERLINE AND CHANNELIZATION PAVEMENT MARKERS.

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAU 2860/ RIEGLE ROAD RIDGE RD. TO JOE ORR RD. PROPOSED ROADWAY/PAVEMENT MARKING PLAN		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
#FILE#		DRAWN -	REVISED -		SCALE:	SHEET NO. 3 OF 6 SHEETS	STA.	TO STA.	2860	2004-047 RS	COOK	34	10
		CHECKED -	REVISED -						CONTRACT NO. 62765				
		DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								



PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD)
 IL-4.75, N50, 3/4"
 PROP. HOT-MIX ASPHALT CONCRETE SURFACE
 COURSE, MIX "D", N70, 1 1/2"

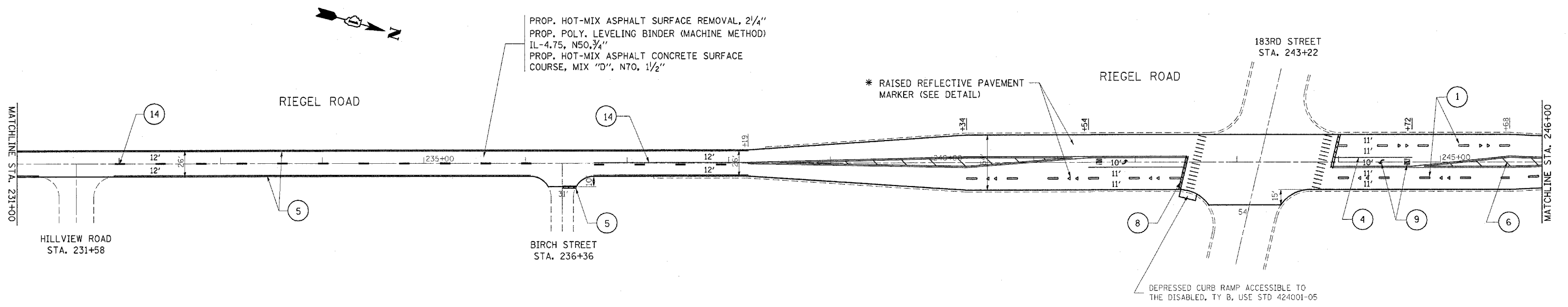
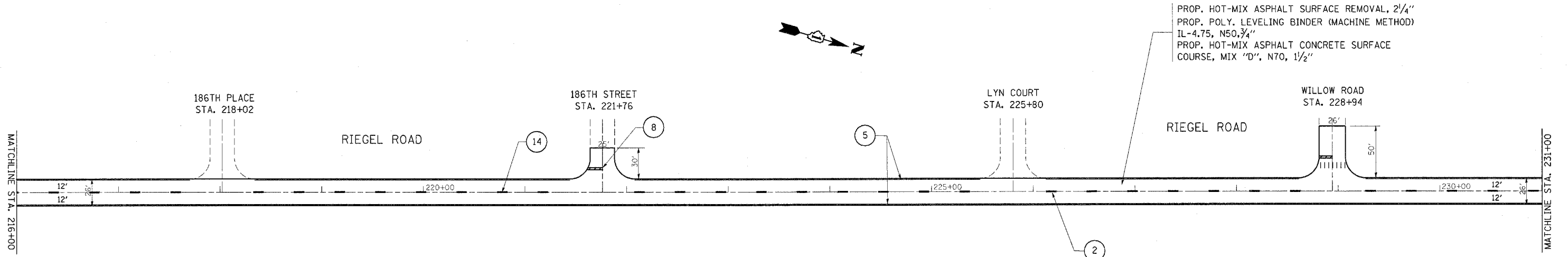
PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD)
 IL-4.75, N50, 3/4"
 PROP. HOT-MIX ASPHALT CONCRETE SURFACE
 COURSE, MIX "D", N70, 1 1/2"

LEGEND

- | | | | |
|---|---|----|---|
| 1 | PROPOSED THERMOPLASTIC PAVEMENT MARKING- LINE 4", SKIP DASH @ 30' SKIP AND 10' DASH, WHITE (TYP.) | 8 | PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP BAR, WHITE (TYP.) |
| 2 | PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", DOUBLE LINE @ 11" C-C, YELLOW (TYP.) | 9 | PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.) |
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| 4 | PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", TURN LANE LINE, WHITE (TYP.) | 11 | PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12", DIAGONAL @ 45 DEG. (5 MIN.), WHITE (TYP.) |
| 5 | PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4" EDGE LINE, WHITE (TYP.) | 12 | PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", SCHOOL CROSSWALK, WHITE (TYP.) |
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| 7 | PROPOSED THERMOPLASTIC PAVEMENT MARKING- LINE 6", CROSS WALK, WHITE (TYP.) | 14 | PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", SKIP DASH @ 30' SKIP AND 10' DASH, YELLOW (TYP.) |

* REFER TO RAISED REFLECTIVE PAVEMENT MARKER (SNOW PLOW RESISTANT DETAIL FOR LOCATION AND SPACING OF LANE LINE, CENTERLINE AND CHANNELIZATION PAVEMENT MARKERS.

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAU 2860/ RIEGLE ROAD RIDGE RD. TO JOE ORR RD. PROPOSED ROADWAY/PAVEMENT MARKING PLAN		F.A.U. RTE. 2860	SECTION 2004-047 RS	COUNTY COOK	TOTAL SHEETS 34	SHEET NO. 11	
#FILE#		DRAWN -	REVISED -		SCALE:	SHEET NO. 4 OF 6 SHEETS	STA.	TO STA.	CONTRACT NO. 62765			
	PLOT SCALE = #SCALE#	CHECKED -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
	PLOT DATE = #DATE#	DATE -	REVISED -									



PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD)
 IL-4.75, N50, 3/4"
 PROP. HOT-MIX ASPHALT CONCRETE SURFACE
 COURSE, MIX "D", N70, 1 1/2"

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD)
 IL-4.75, N50, 3/4"
 PROP. HOT-MIX ASPHALT CONCRETE SURFACE
 COURSE, MIX "D", N70, 1 1/2"

* RAISED REFLECTIVE PAVEMENT
 MARKER (SEE DETAIL)

DEPRESSED CURB RAMP ACCESSIBLE TO
 THE DISABLED, TY B, USE STD 424001-05

LEGEND

- ① PROPOSED THERMOPLASTIC PAVEMENT MARKING- LINE 4", SKIP DASH @ 30' SKIP AND 10' DASH, WHITE (TYP.)
- ② PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", DOUBLE LINE @ 11" C-C, YELLOW (TYP.)
- ③ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", MEDIAN LINE, YELLOW (TYP.)
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- ⑨ PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.)
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* REFER TO RAISED REFLECTIVE PAVEMENT MARKER (SNOW PLOW RESISTANT DETAIL FOR LOCATION AND SPACING OF LANE LINE, CENTERLINE AND CHANNELIZATION PAVEMENT MARKERS.

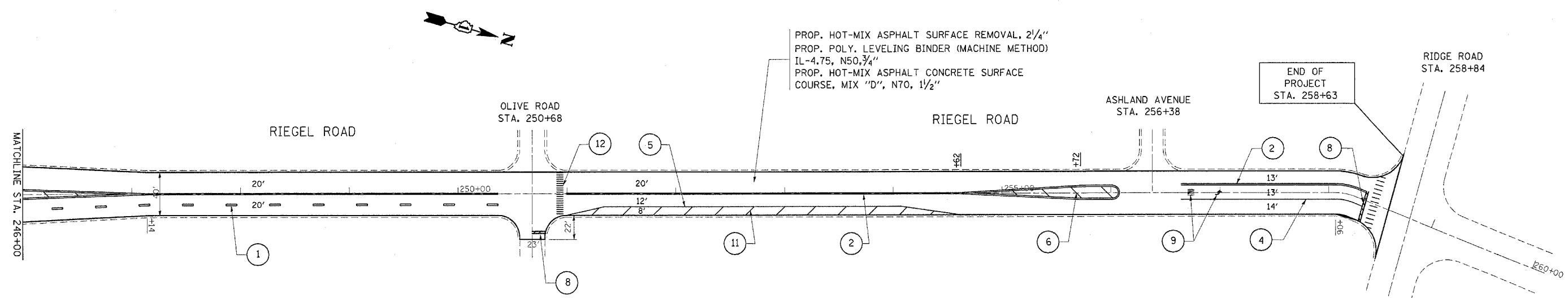
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

FAU 2860/ RIEGLE ROAD
 RIDGE RD. TO JOE ORR RD.
 PROPOSED ROADWAY/PAVEMENT MARKING PLAN

F.A.U. RTE. 2860	SECTION 2004-047 RS	COUNTY COOK	TOTAL SHEETS 34	SHEET NO. 12
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62765	

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -
#FILE#		DRAWN -	REVISED -
	PLOT SCALE = #SCALE#	CHECKED -	REVISED -
	PLOT DATE = #DATE#	DATE -	REVISED -

SCALE: SHEET NO. 5 OF 6 SHEETS STA. TO STA.



* REFER TO RAISED REFLECTIVE PAVEMENT MARKER (SNOW PLOW RESISTANT DETAIL FOR LOCATION AND SPACING OF LANE LINE, CENTERLINE AND CHANNELIZATION PAVEMENT MARKERS.

LEGEND

- ① PROPOSED THERMOPLASTIC PAVEMENT MARKING- LINE 4", SKIP DASH @ 30' SKIP AND 10' DASH, WHITE (TYP.)
- ② PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", DOUBLE LINE @ 11" C-C, YELLOW (TYP.)
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- ⑫ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", SCHOOL CROSSWALK, WHITE (TYP.)
- ⑬ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", NO PASSING ZONE LINE, YELLOW (TYP.)
- ⑭ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", SKIP DASH @ 30' SKIP AND 10' DASH, YELLOW (TYP.)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAU 2860/ RIEGLE ROAD
 RIDGE RD. TO JOE ORR RD.
 PROPOSED ROADWAY/PAVEMENT MARKING PLAN

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2860	2004-047 RS	COOK	34	13
CONTRACT NO. 62765				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -
#FILE#		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

SCALE: SHEET NO. 6 OF 6 SHEETS STA. TO STA.

RESURFACING - TRAFFIC SIGNAL SCHEDULE OF QUANTITIES

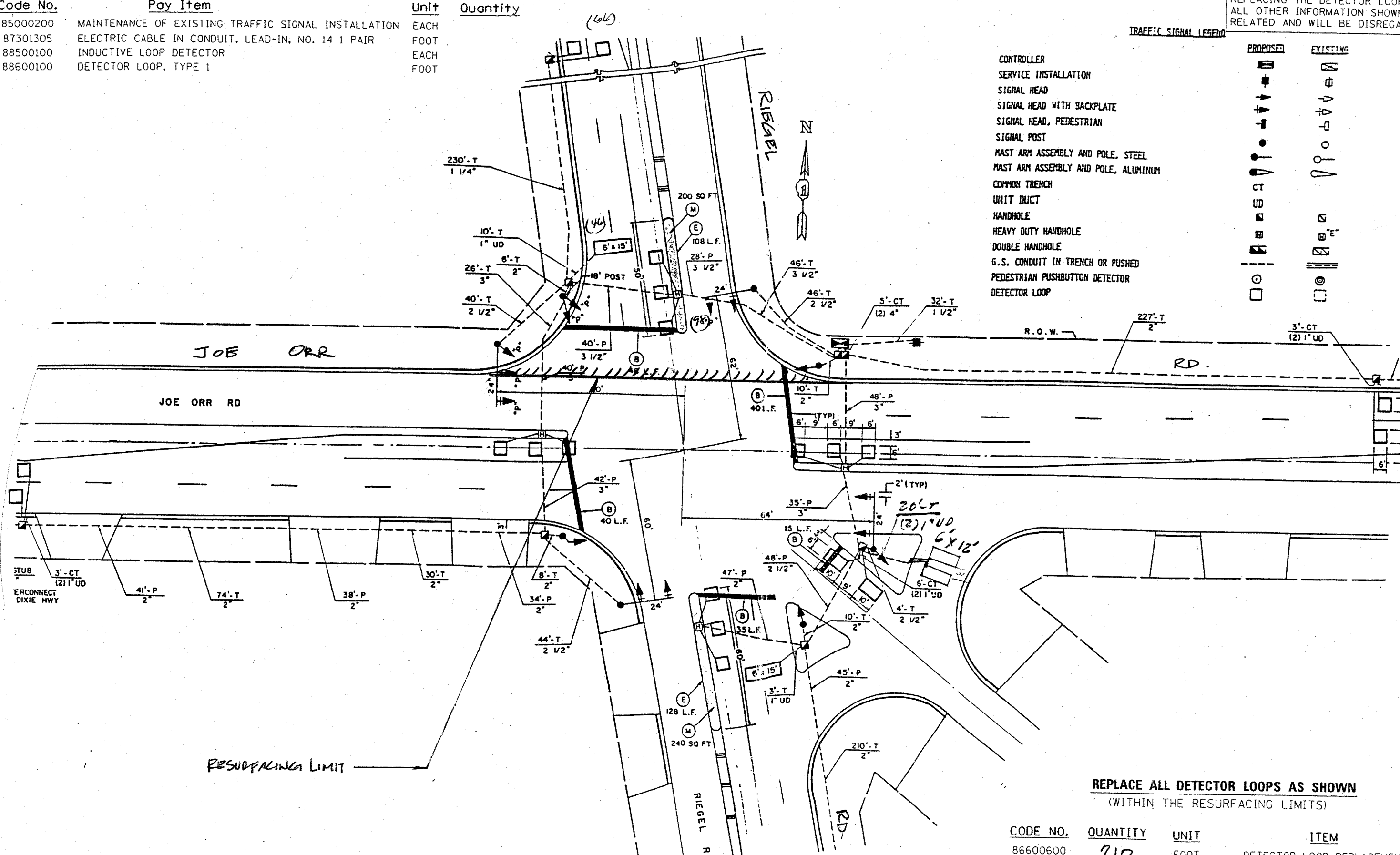
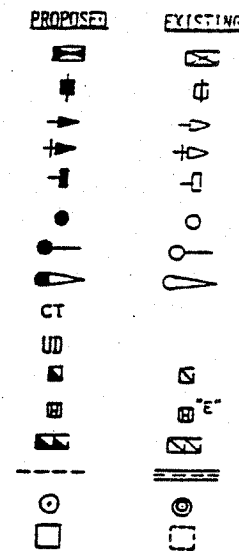
The "Pay Items" below are paid separately as per the attached "Specification" for MAGNETIC DETECTOR REMOVAL AND DETECTOR LOOP INSTALLATION.

Code No.	Pay Item	Unit	Quantity
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	
87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	
88500100	INDUCTIVE LOOP DETECTOR	EACH	
88600100	DETECTOR LOOP, TYPE 1	FOOT	

NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

TRAFFIC SIGNAL LEGEND

- CONTROLLER
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD WITH BACKPLATE
- SIGNAL HEAD, PEDESTRIAN
- SIGNAL POST
- MAST ARM ASSEMBLY AND POLE, STEEL
- MAST ARM ASSEMBLY AND POLE, ALUMINUM
- COMMON TRENCH
- UNIT DUCT
- HANDHOLE
- HEAVY DUTY HANDHOLE
- DOUBLE HANDHOLE
- G.S. CONDUIT IN TRENCH OR PUSHED
- PEDESTRIAN PUSHBUTTON DETECTOR
- DETECTOR LOOP



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	210	FOOT	DETECTOR LOOP REPLACEMENT

FILE NAME:
c:\projects\traffic\1070027\usj2.20.45.dwg

USER NAME = kanthapixoybc
DESIGNED -
DRAWN -
CHECKED -
DATE -

REVISOR -
REVISOR -
REVISOR -
REVISOR -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT
RIEGEL ROAD @ JOE ORR RD.

SCALE: SHEET NO. OF SHEETS STA. TO STA.

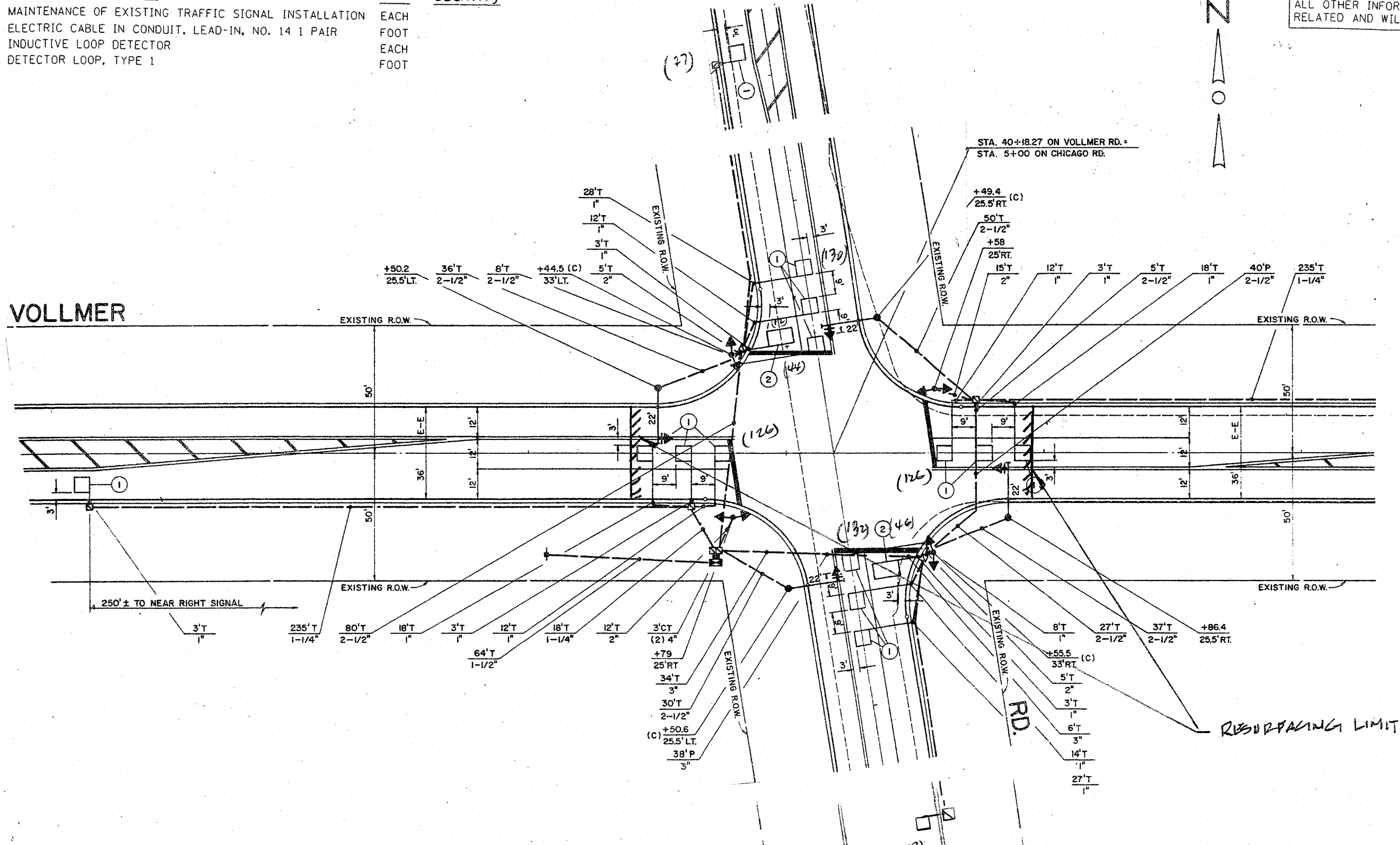
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		COOK	34	14
CONTRACT NO. 62765				

RESURFACING - TRAFFIC SIGNAL SCHEDULE OF QUANTITIES

The "Pay Items" below are paid separately as per the attached "Specification" for MAGNETIC DETECTOR REMOVAL AND DETECTOR LOOP INSTALLATION.

Code No.	Pay Item	Unit	Quantity
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	
87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	
88500100	INDUCTIVE LOOP DETECTOR	EACH	
88600100	DETECTOR LOOP, TYPE 1	FOOT	

NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	631	FOOT	DETECTOR LOOP REPLACEMENT

FILE NAME =	USER NAME = konthaphixaybc	DESIGNED -	REVISED -
c:\projects\traffic\870027\us12.20.45.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT
RIEDEL RD @ VOLLMER RD

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			34	15
			CONTRACT NO. 62765	

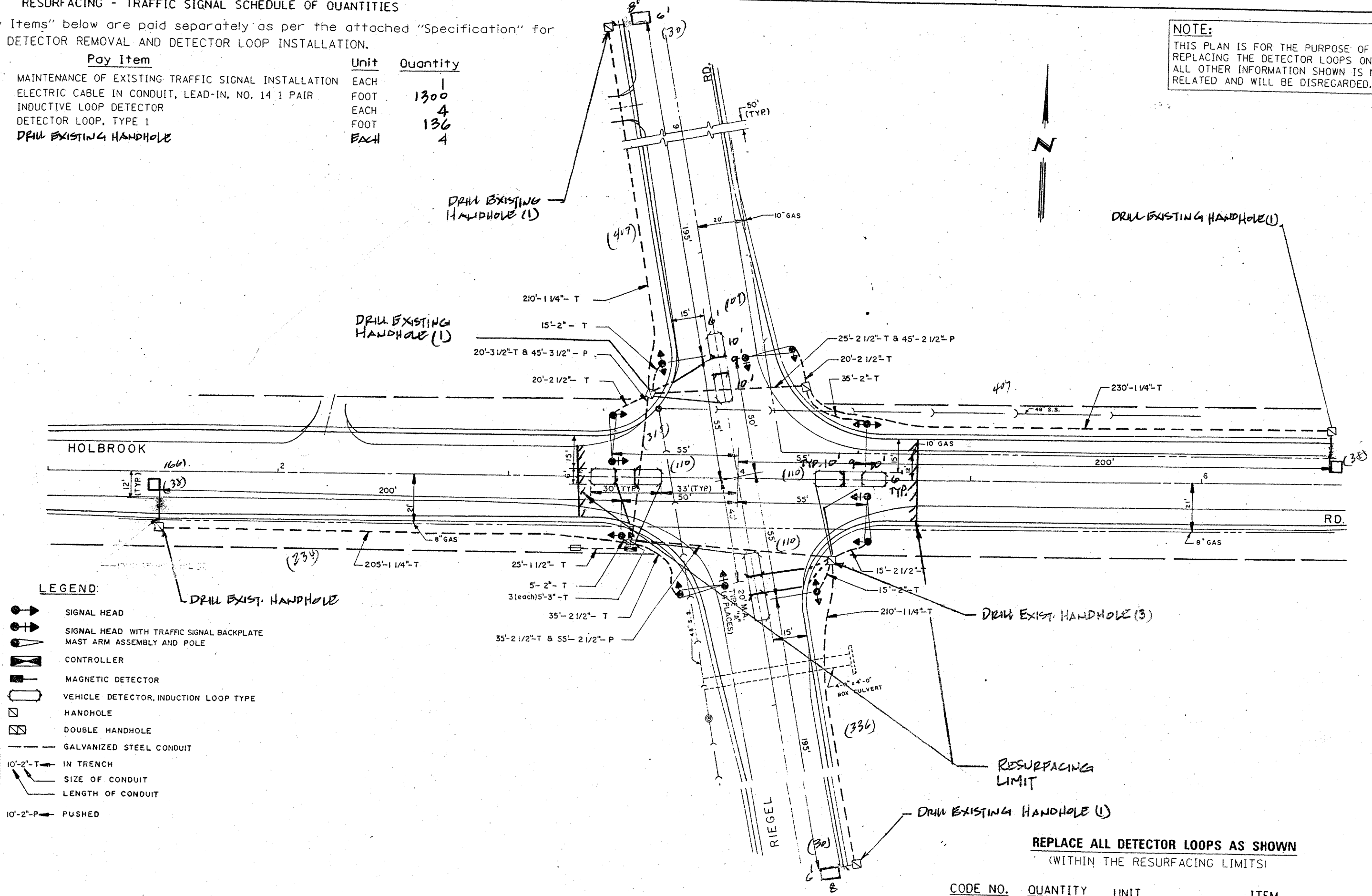
SCALE: SHEET NO. OF SHEETS STA. TO STA.

RESURFACING - TRAFFIC SIGNAL SCHEDULE OF QUANTITIES

The "Pay Items" below are paid separately as per the attached "Specification" for MAGNETIC DETECTOR REMOVAL AND DETECTOR LOOP INSTALLATION.

Code No.	Pay Item	Unit	Quantity
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1300
88500100	INDUCTIVE LOOP DETECTOR	EACH	4
88600100	DETECTOR LOOP, TYPE 1	FOOT	136
	DRILL EXISTING HANDHOLE	EACH	4

NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.



LEGEND:

- SIGNAL HEAD
- SIGNAL HEAD WITH TRAFFIC SIGNAL BACKPLATE
MAST ARM ASSEMBLY AND POLE
- CONTROLLER
- MAGNETIC DETECTOR
- VEHICLE DETECTOR, INDUCTION LOOP TYPE
- HANDHOLE
- DOUBLE HANDHOLE
- GALVANIZED STEEL CONDUIT
- 10'-2"-T IN TRENCH
- 10'-2"-P PUSHED

REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

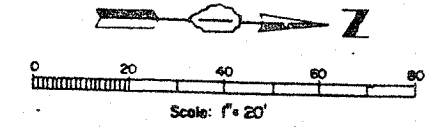
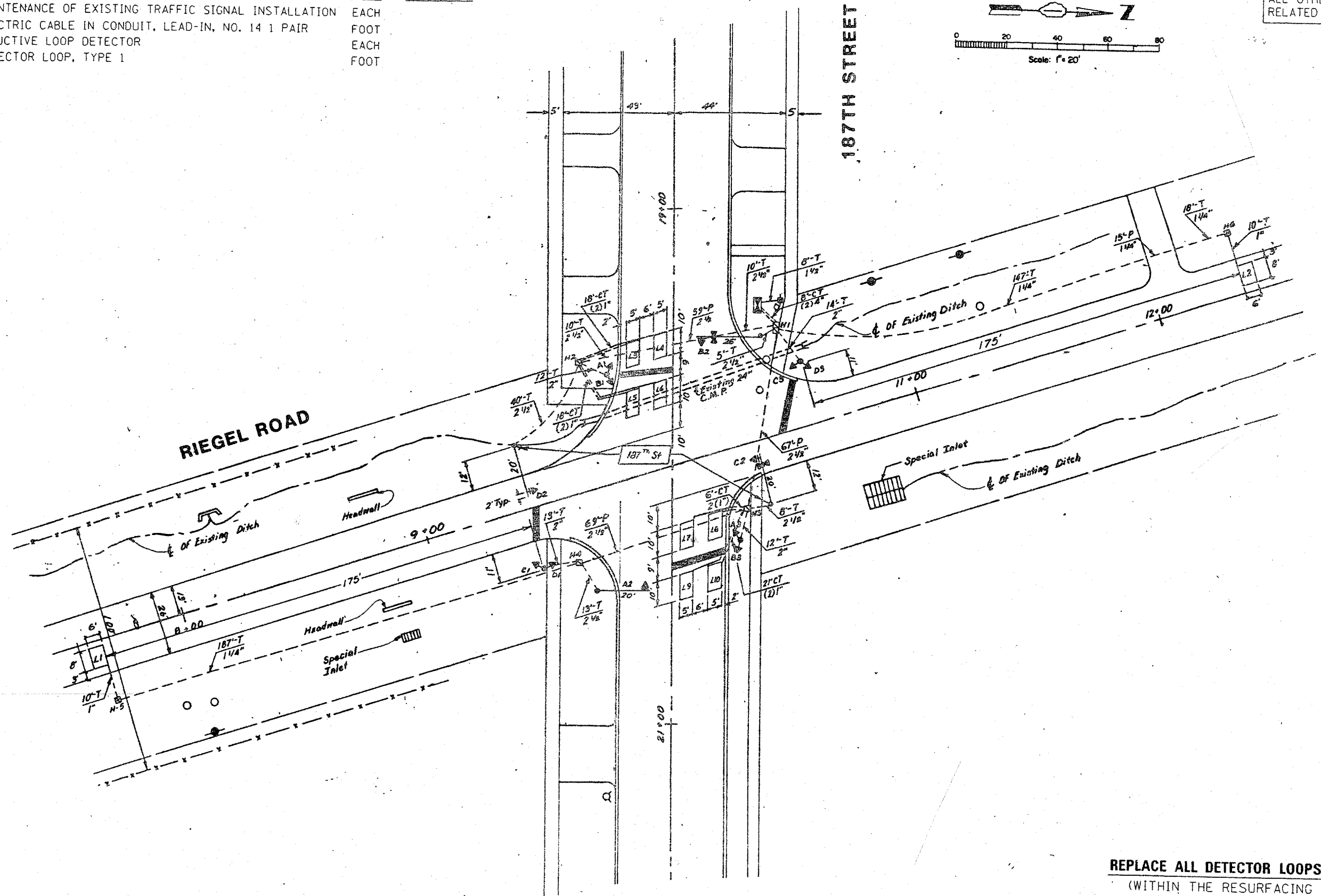
CODE NO.	QUANTITY	UNIT	ITEM
86600600	440	FOOT	DETECTOR LOOP REPLACEMENT

RESURFACING - TRAFFIC SIGNAL SCHEDULE OF QUANTITIES

The "Pay Items" below are paid separately as per the attached "Specification" for MAGNETIC DETECTOR REMOVAL AND DETECTOR LOOP INSTALLATION.

Code No.	Pay Item	Unit	Quantity
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	
87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	
88500100	INDUCTIVE LOOP DETECTOR	EACH	
88600100	DETECTOR LOOP, TYPE 1	FOOT	

NOTE:
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REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600		FOOT	DETECTOR LOOP REPLACEMENT

FILE NAME =	USER NAME = konthaphixybc	DESIGNED -	REVISED -
c:\projects\traffic\187th\187th2_20_45.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT
RIEGEL RD @ 187TH ST

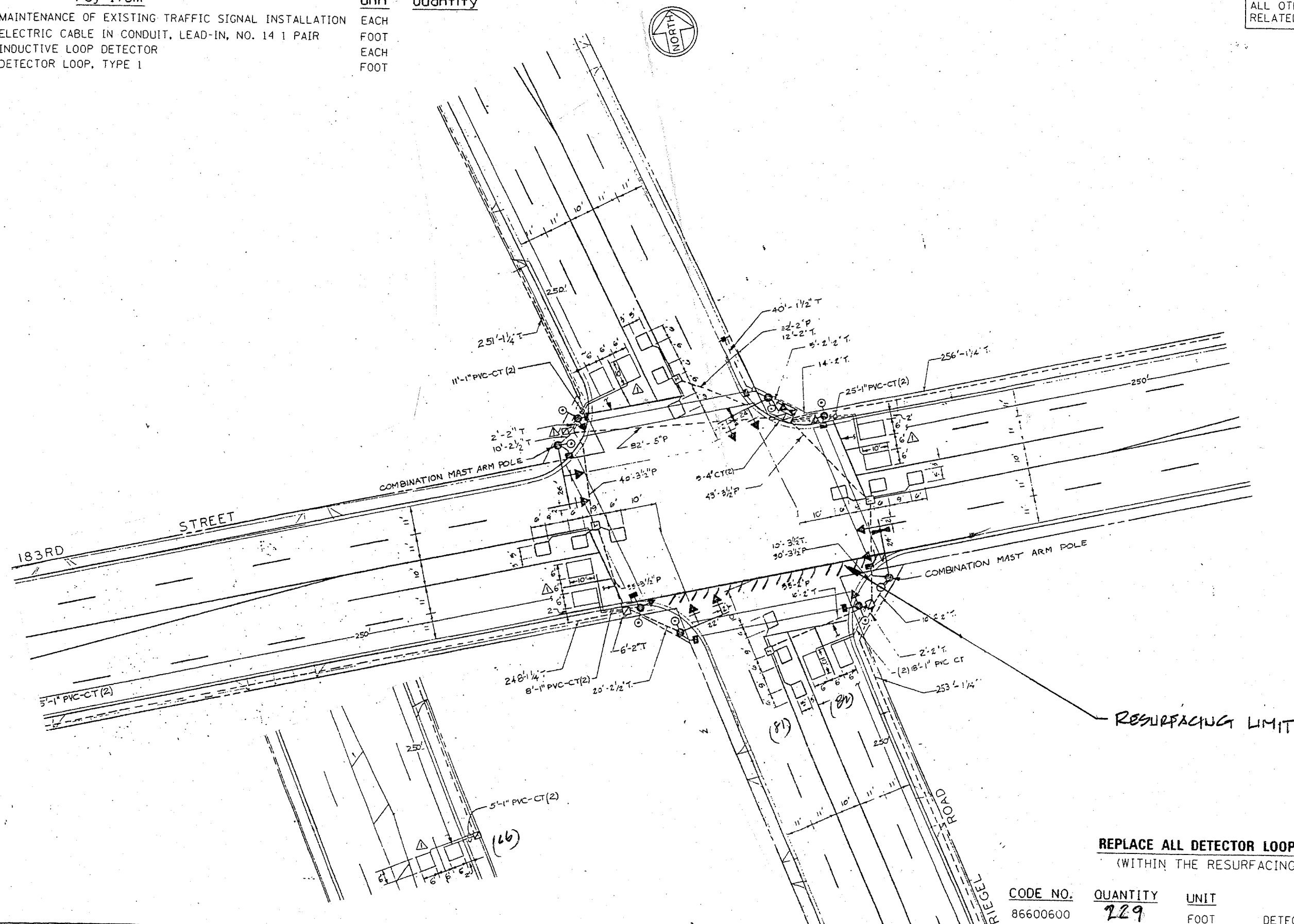
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			24	17
			CONTRACT NO. 62765	

RESURFACING - TRAFFIC SIGNAL SCHEDULE OF QUANTITIES

The "Pay Items" below are paid separately as per the attached "Specification" for MAGNETIC DETECTOR REMOVAL AND DETECTOR LOOP INSTALLATION.

Code No.	Pay Item	Unit	Quantity
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	
87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	
88500100	INDUCTIVE LOOP DETECTOR	EACH	
88600100	DETECTOR LOOP, TYPE 1	FOOT	

NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	229	FOOT	DETECTOR LOOP REPLACEMENT

FILE NAME =	USER NAME = konthaphixoybc	DESIGNED -	REVISED -
c:\projects\tralfic\1878027\us12.28.45.d		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT
RIEGEL ROAD @ 183 RD ST.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		COOK	34	18
CONTRACT NO. 62765				

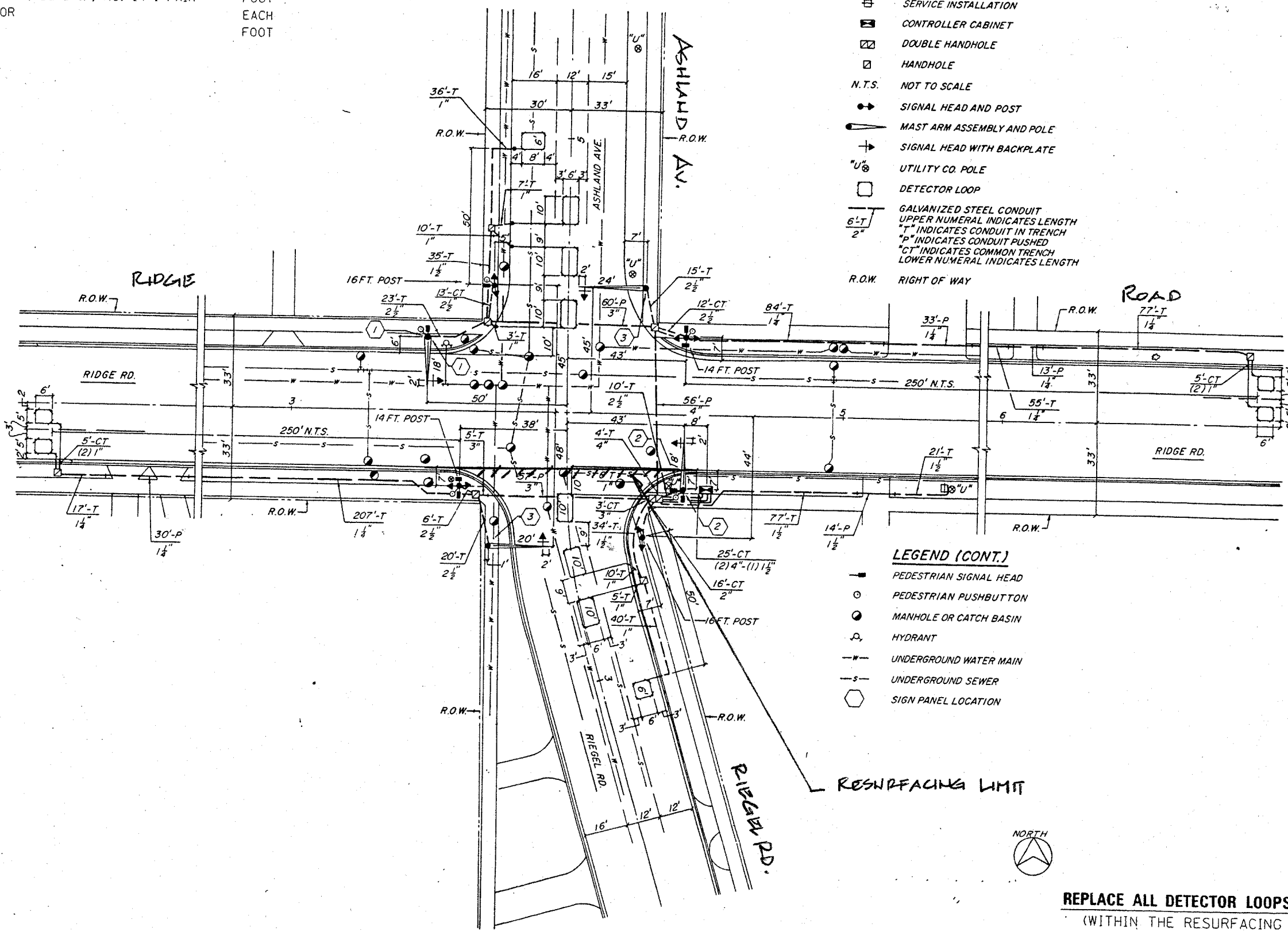
SCALE: SHEET NO. OF SHEETS | STA. TO STA.

RESURFACING - TRAFFIC SIGNAL SCHEDULE OF QUANTITIES

The "Pay Items" below are paid separately as per the attached "Specification" for MAGNETIC DETECTOR REMOVAL AND DETECTOR LOOP INSTALLATION.

Code No.	Pay Item	Unit	Quantity
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	
87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	
88500100	INDUCTIVE LOOP DETECTOR	EACH	
88600100	DETECTOR LOOP, TYPE 1	FOOT	

NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.



- LEGEND**
- ⊕ SERVICE INSTALLATION
 - ⊞ CONTROLLER CABINET
 - ⊞ DOUBLE HANDHOLE
 - ⊞ HANDHOLE
 - N.T.S. NOT TO SCALE
 - ⊕ SIGNAL HEAD AND POST
 - ⊕ MAST ARM ASSEMBLY AND POLE
 - ⊕ SIGNAL HEAD WITH BACKPLATE
 - ⊕ UTILITY CO. POLE
 - ⊕ DETECTOR LOOP
 - 6-T 2" GALVANIZED STEEL CONDUIT UPPER NUMERAL INDICATES LENGTH "T" INDICATES CONDUIT IN TRENCH "P" INDICATES CONDUIT PUSHED "CT" INDICATES COMMON TRENCH LOWER NUMERAL INDICATES LENGTH
 - R.O.W. RIGHT OF WAY

- LEGEND (CONT.)**
- ⊕ PEDESTRIAN SIGNAL HEAD
 - PEDESTRIAN PUSHBUTTON
 - MANHOLE OR CATCH BASIN
 - HYDRANT
 - W- UNDERGROUND WATER MAIN
 - S- UNDERGROUND SEWER
 - ⬡ SIGN PANEL LOCATION



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	182	FOOT	DETECTOR LOOP REPLACEMENT

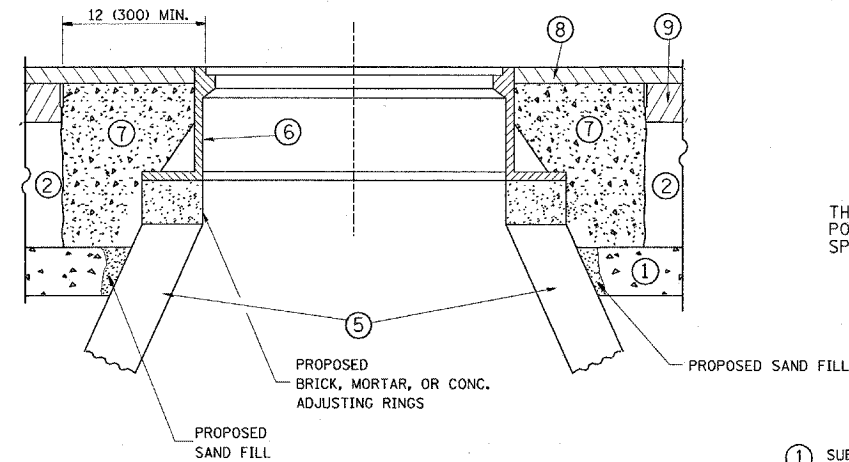
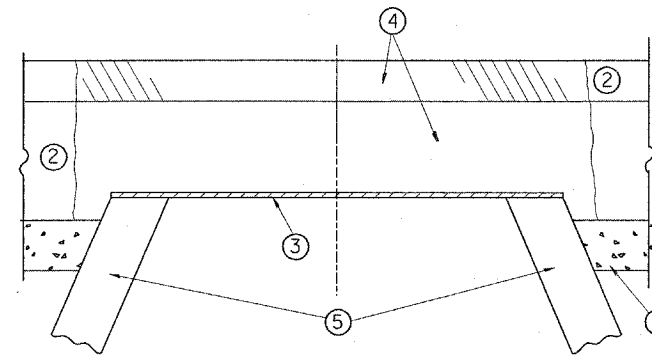
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PLOT DATE: 2/29/2008	DATE: -	REVISED: -	REVISED: -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT
RIEGEL ROAD @ RIDGE ROAD

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		COOK	34	19
CONTRACT NO. 62765				

SCALE: SHEET NO. OF SHEETS STA. TO STA.



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

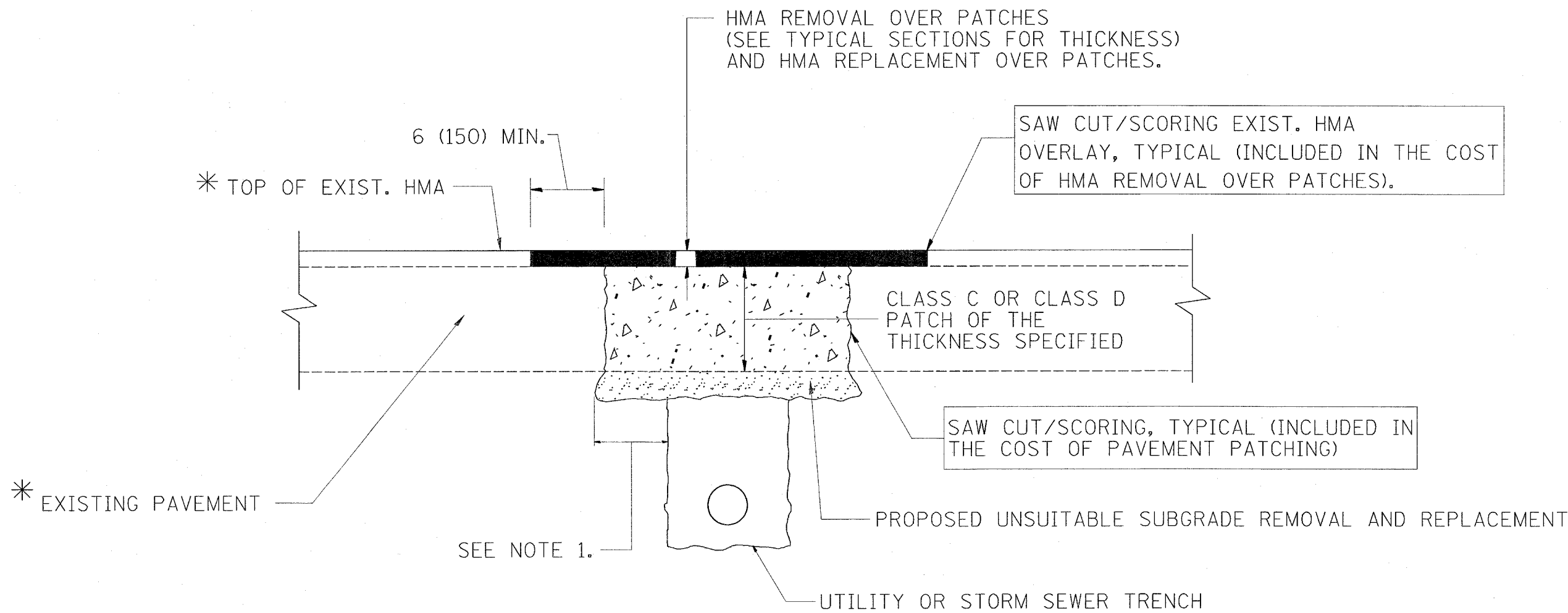
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		CHECKED -	REVISED - R. WIEDEMAN 05-14-04
		DATE - 10-25-94	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			34	20
BD600-03 (BD-8)			CONTRACT NO. 62765	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - A. ABBAS 04-27-98		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	34	21
		CHECKED -	REVISED - R. BORO 01-01-07		BD400-04 (BD-22)			CONTRACT NO. 62765				
		DATE - 10-25-94	REVISED - R. BORO 09-04-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SALT TOLERANT SOD AND TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

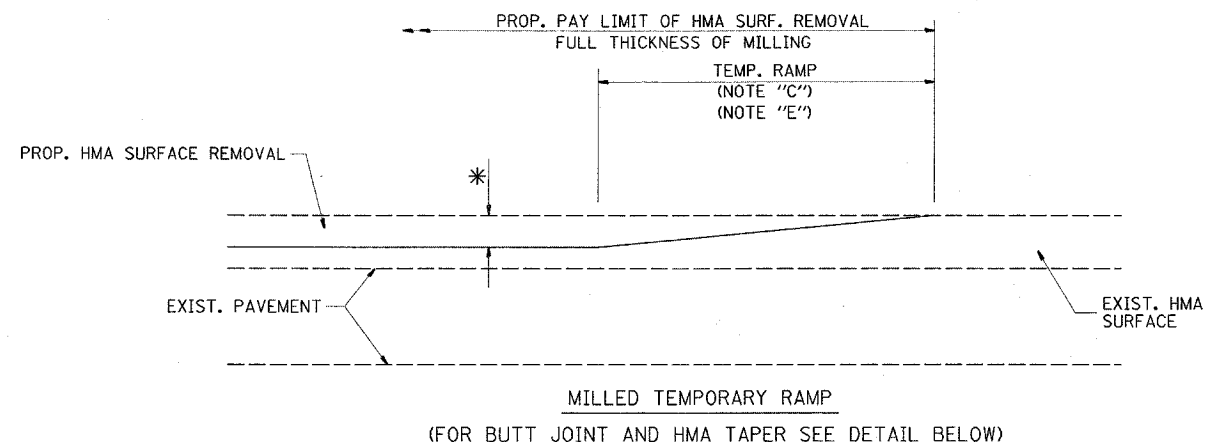
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

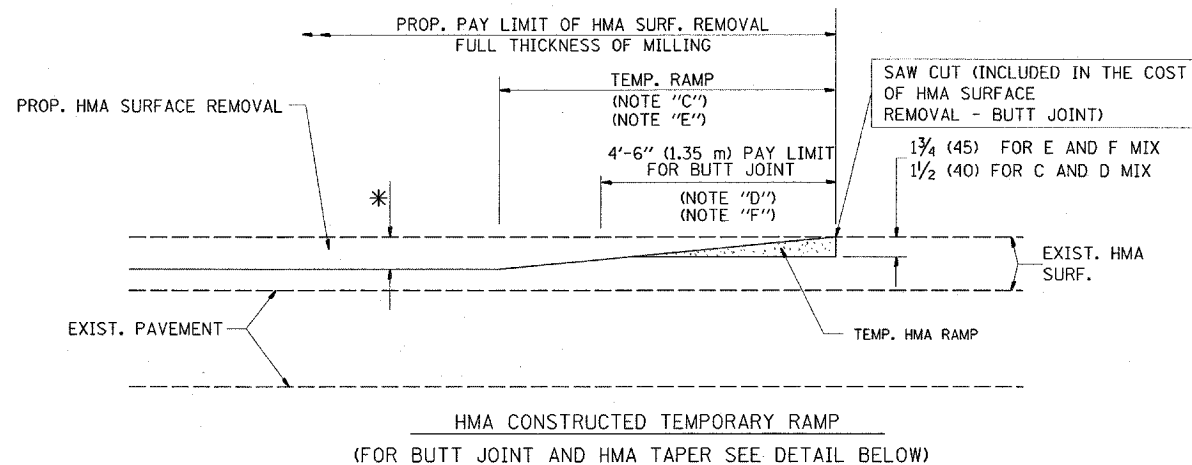
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

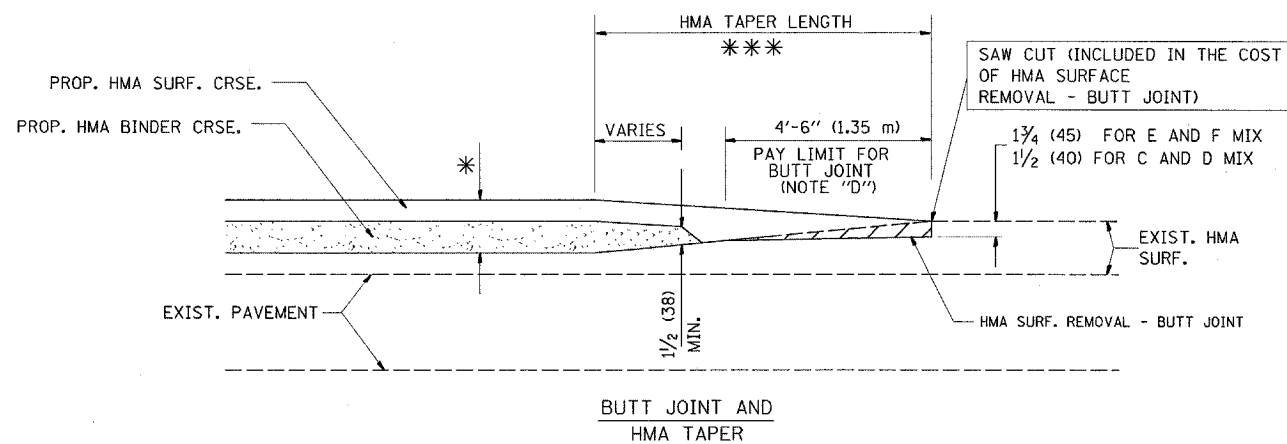
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		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	34	22
		PLOT SCALE = 50,000' / IN.	CHECKED -		REVISED - M. GOMEZ 01-22-01				BD600-06 (BD-24)		CONTRACT NO. 62765	
		PLOT DATE = 3/13/2008	DATE - 03-11-94		REVISED - R. BORO 01-01-07				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



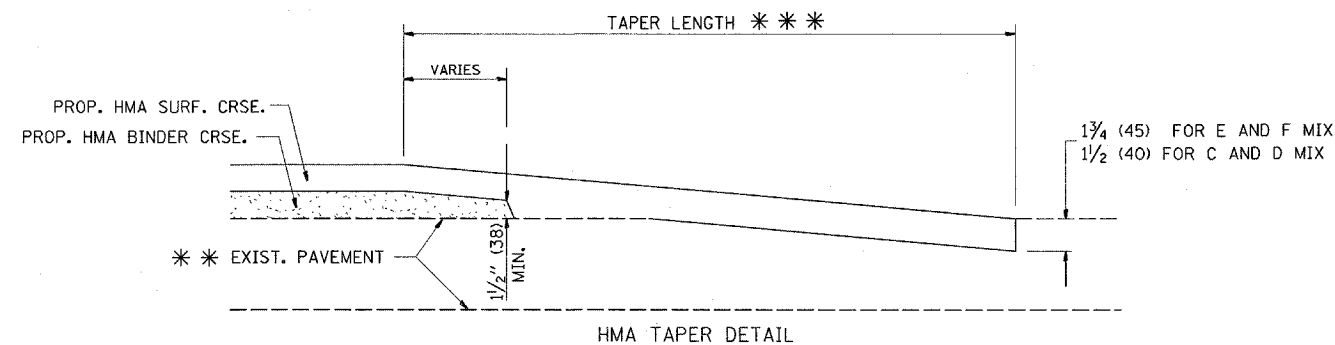
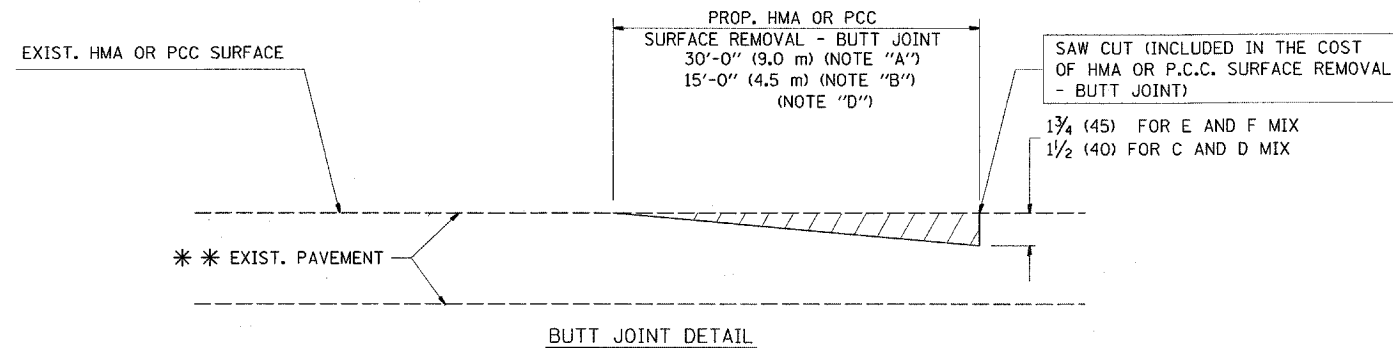
OPTION 1



**OPTION 2
TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

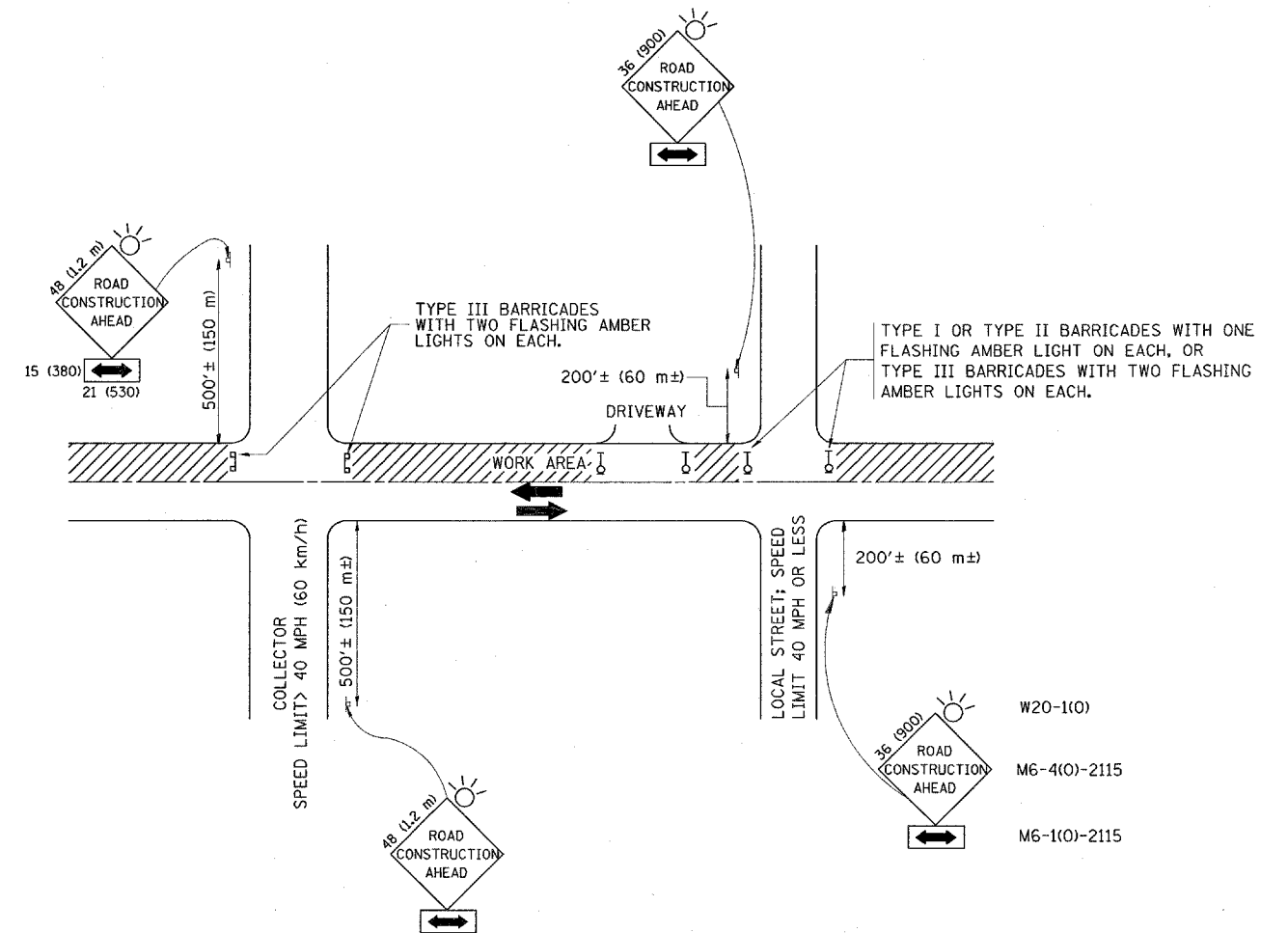
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	PLOT DATE = 3/13/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. - RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	BD400-05 BD32		34	23
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62765	



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

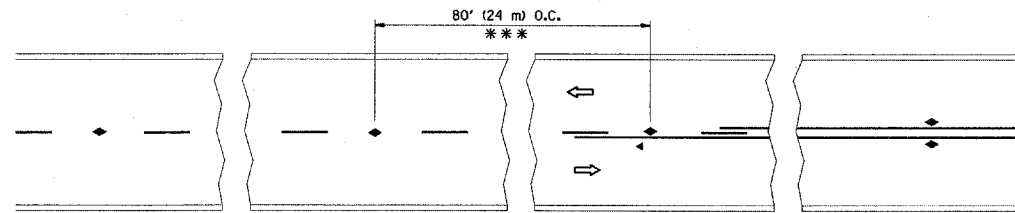
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	PLOT DATE = 3/13/2008	CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

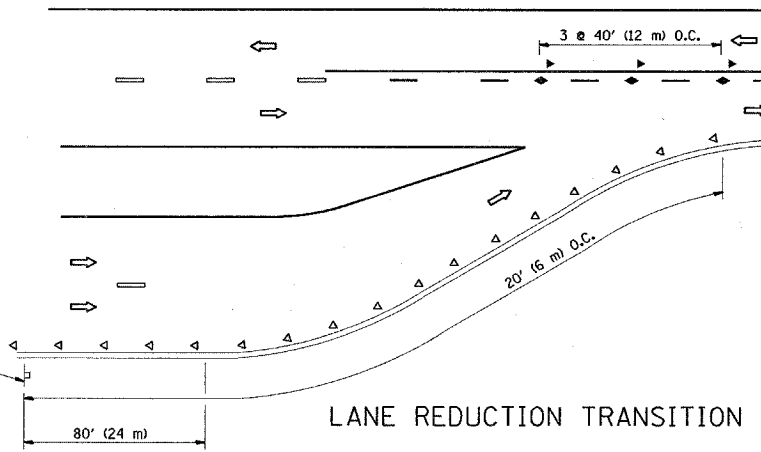
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

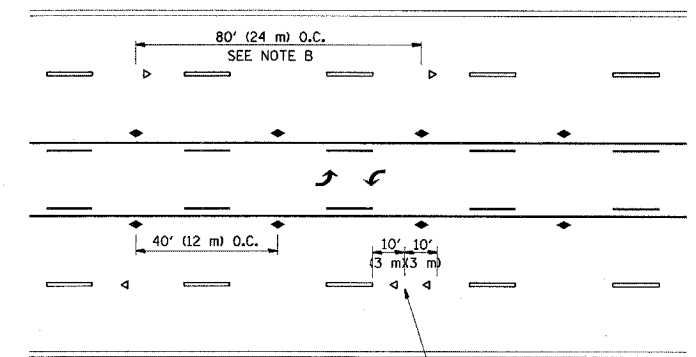
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	TC-10		34	24
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62765	



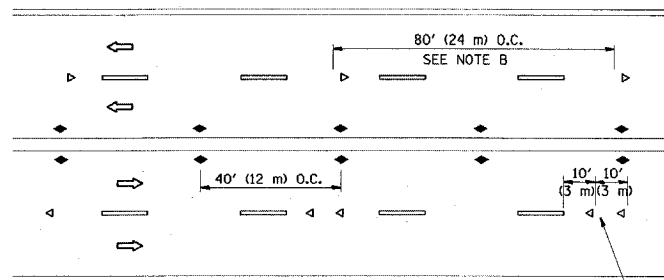
TWO-LANE/TWO-WAY



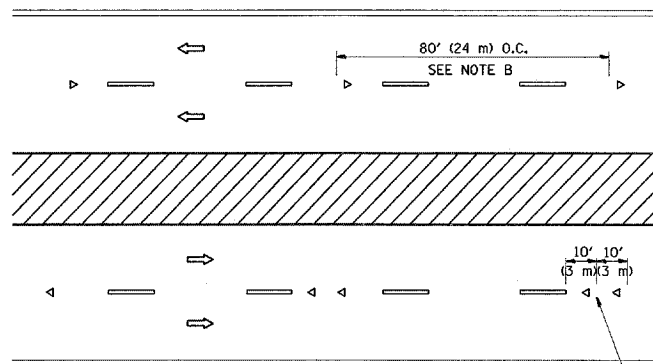
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

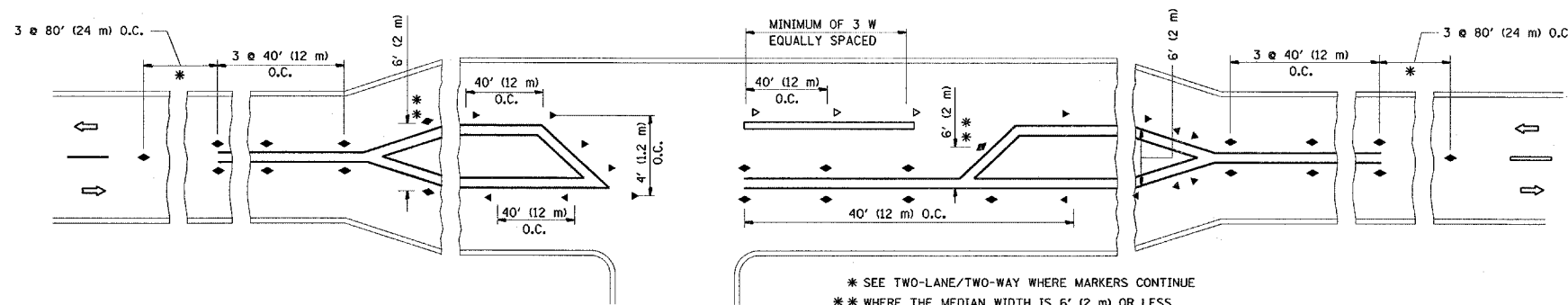
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

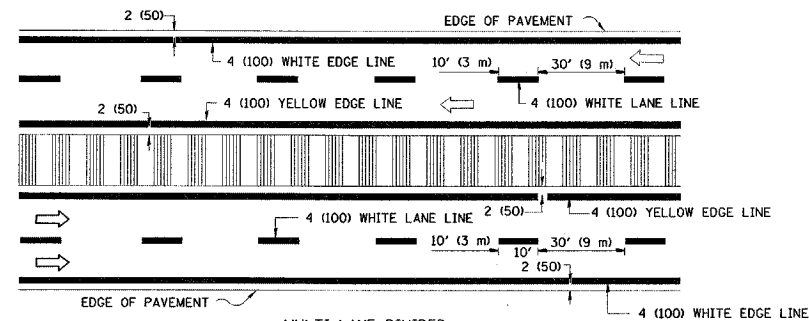
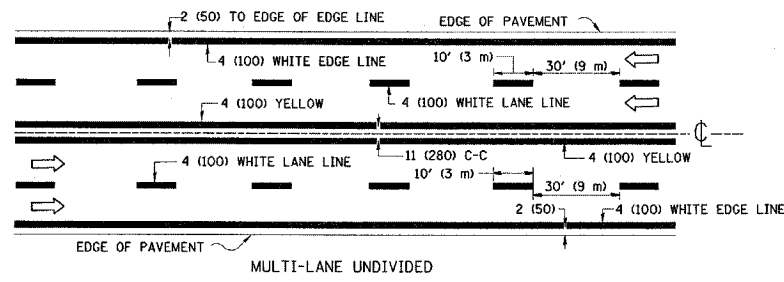
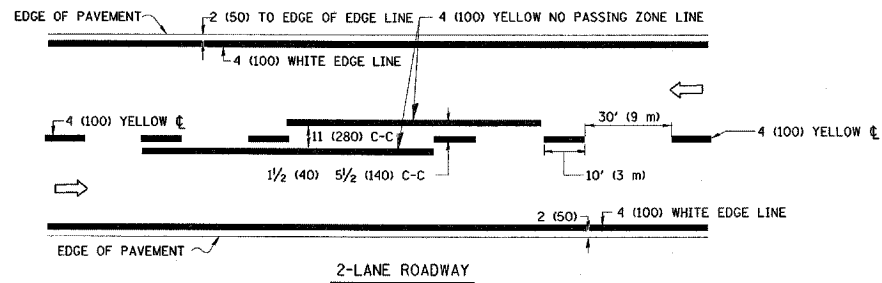
All dimensions are in Inches (millimeters) unless otherwise shown.

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	PLOT DATE = 3/13/2008	CHECKED -	REVISED - T. RAMMACHER 01-06-00
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

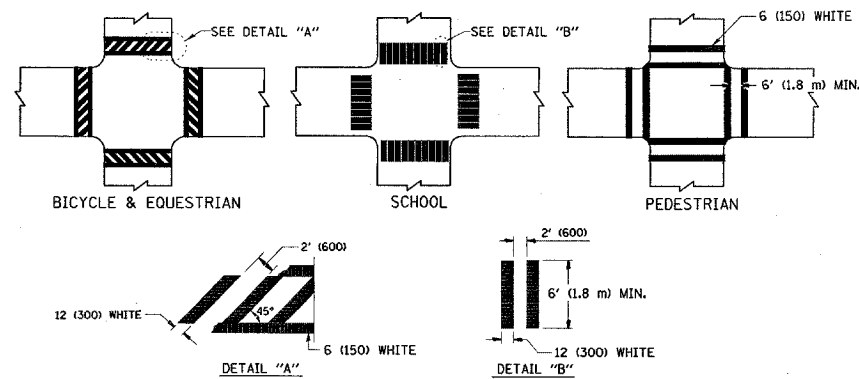
TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-11		34	25
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62765	

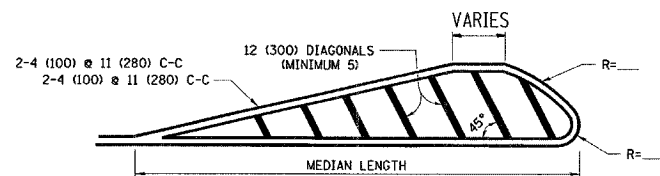
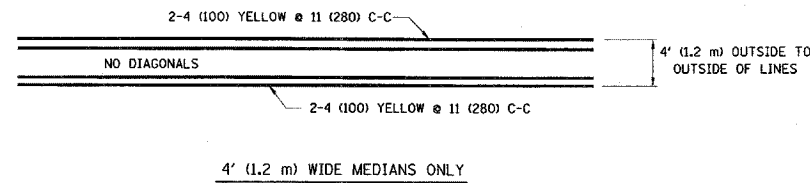


TYPICAL LANE AND EDGE LINE MARKING

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

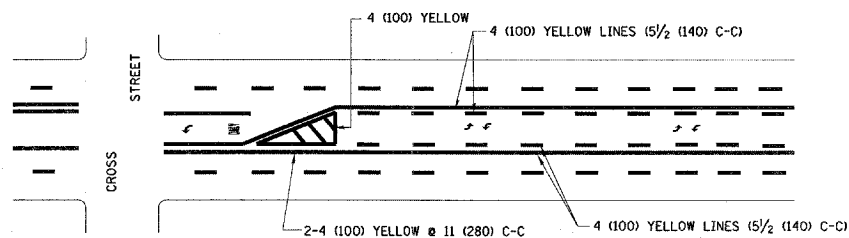


TYPICAL CROSSWALK MARKING



FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

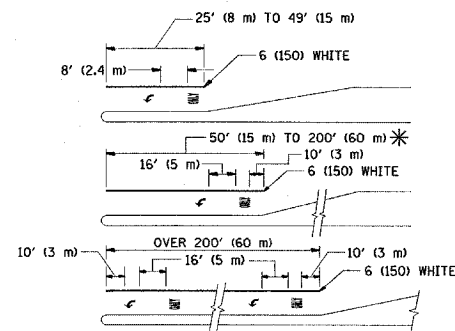
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

MEDIAN WITH TWO-WAY LEFT TURN LANE

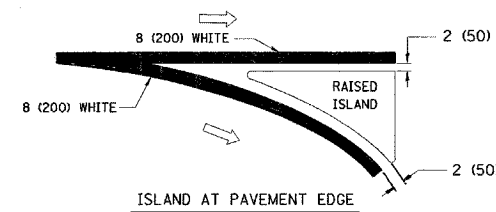
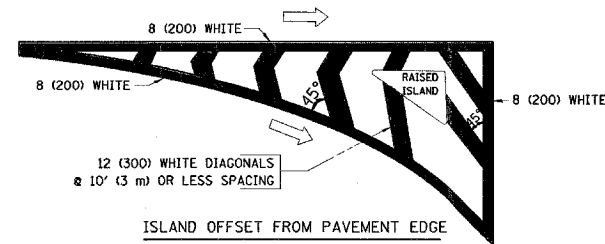
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

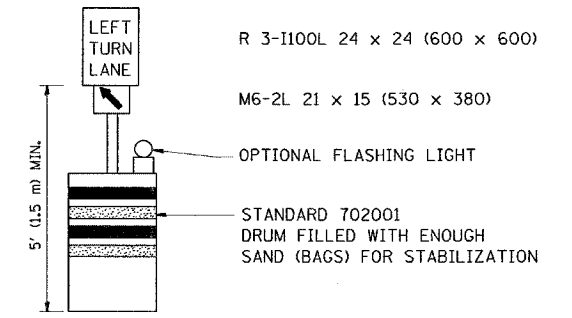
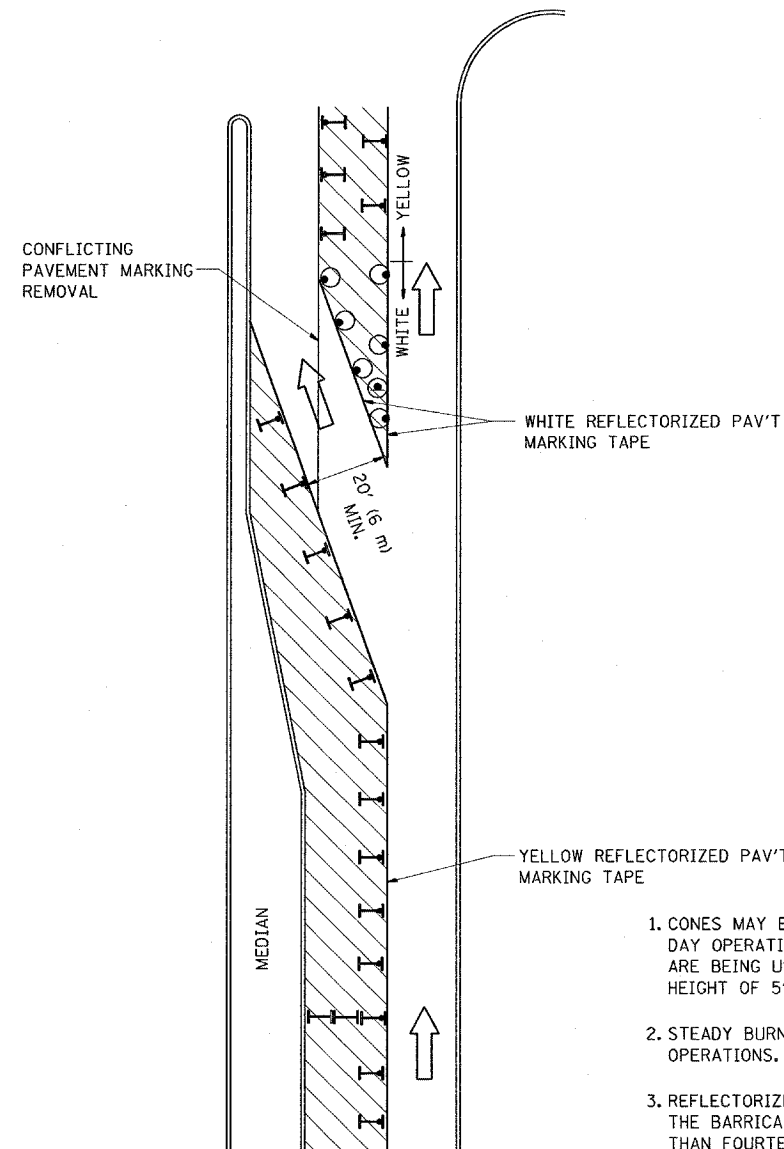
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	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED -A. HOUSEH 10-17-96
	PLOT DATE = 3/13/2008	DATE - 03-19-90	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			34	26
TC-13		CONTRACT NO. 62765		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

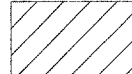





SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

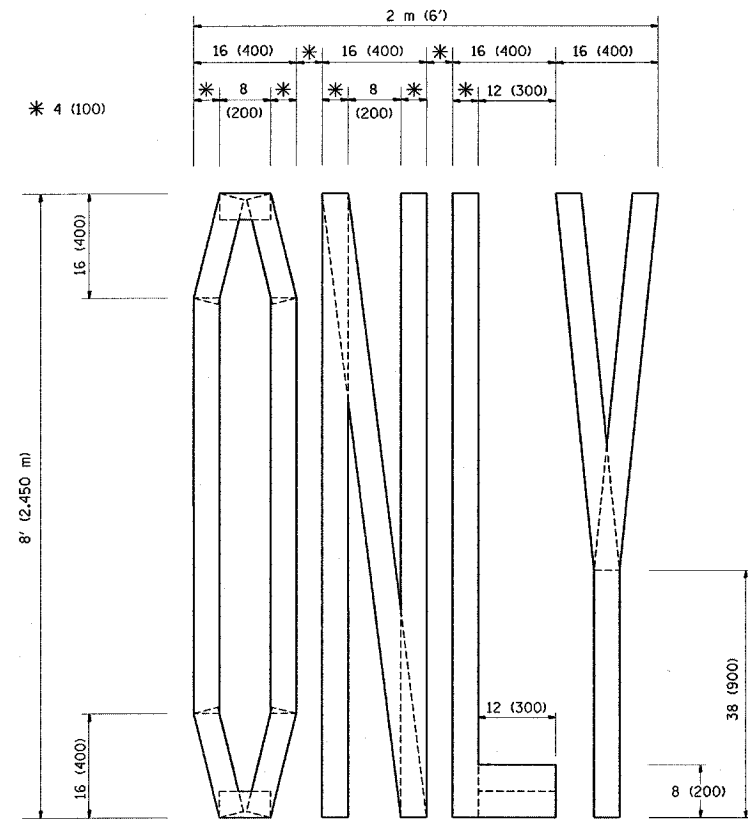
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

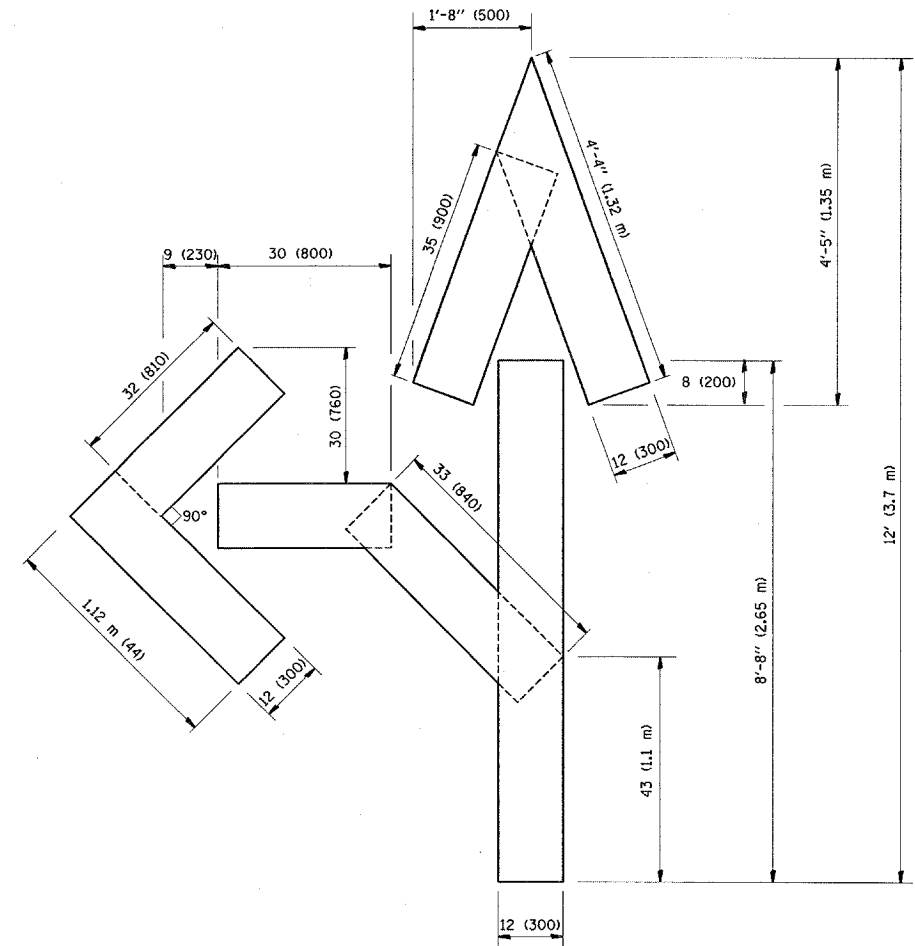
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

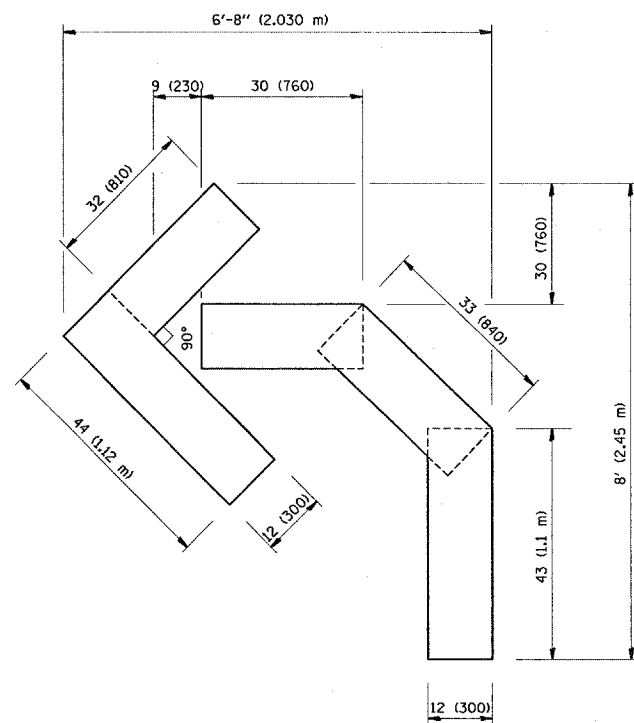
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-14		34	27
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62765	



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

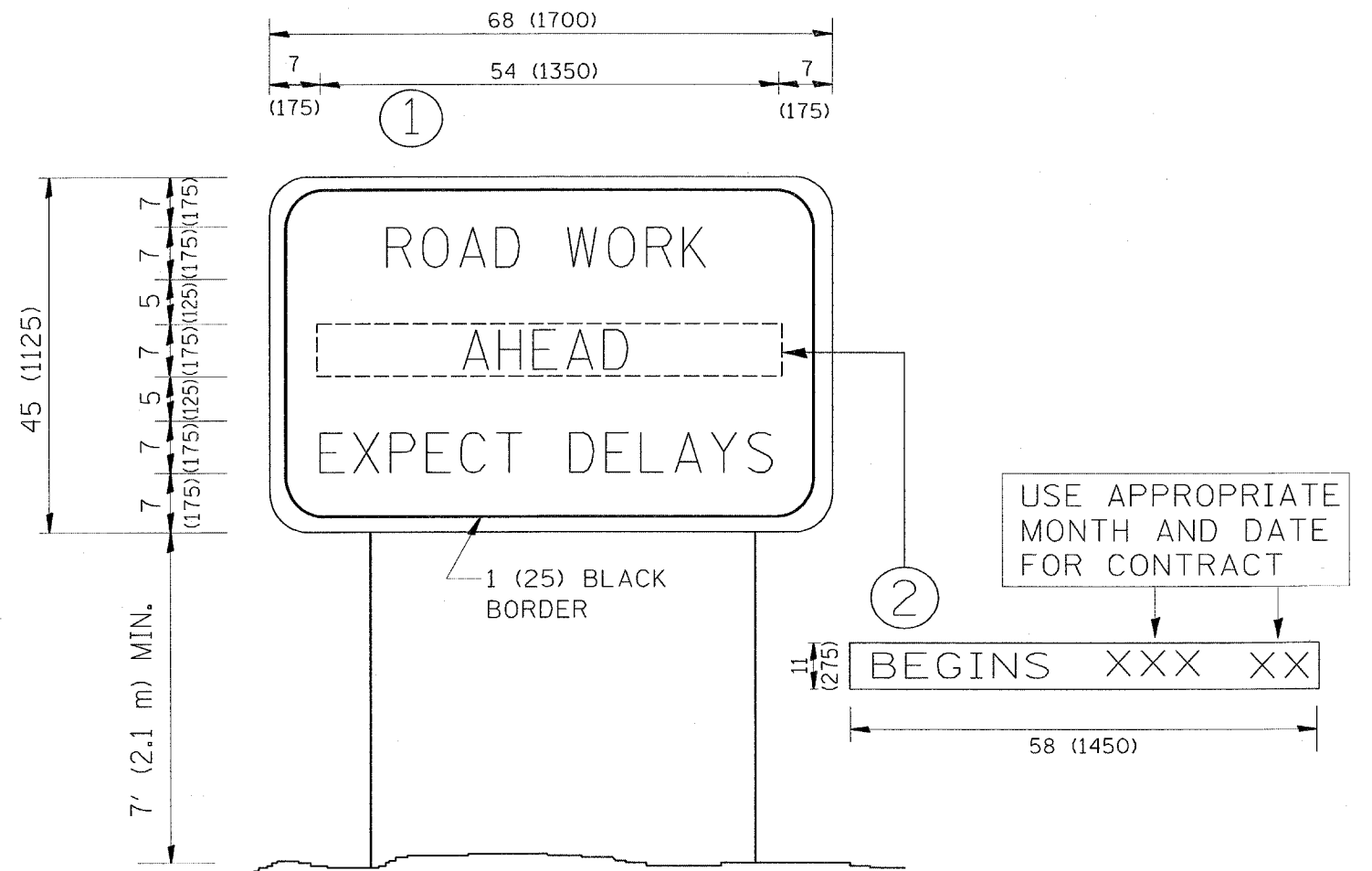
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			34	28
TC-16		CONTRACT NO. 62765		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

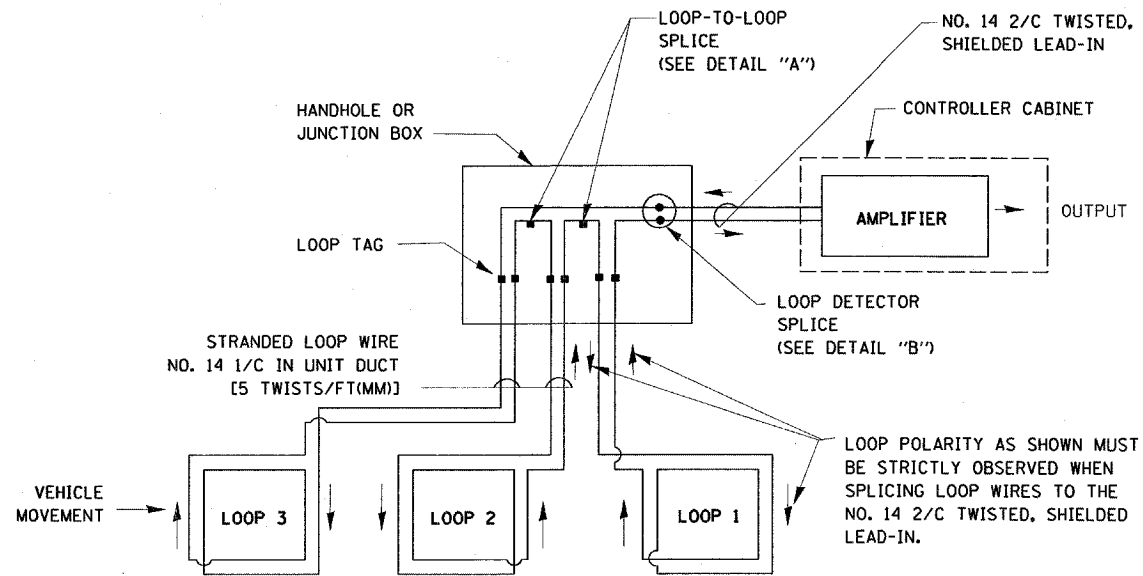
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\d\st\d\22x34\tc22.dgn	USER NAME = byunah	DESIGNED - DRAWN -	REVISED - R. MIRS 09-15-97 REVISED - R. MIRS 12-11-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE = 50,000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	SCALE: NONE		SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	TC-22	CONTRACT NO. 62765	34	29
PLOT DATE = 3/13/2008	DATE -	REVISED - C. JUCIUS 01-31-07	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT										

LOOP DETECTOR NOTES

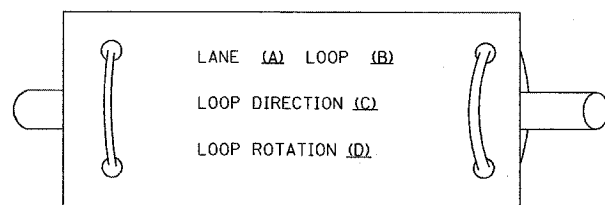
- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.



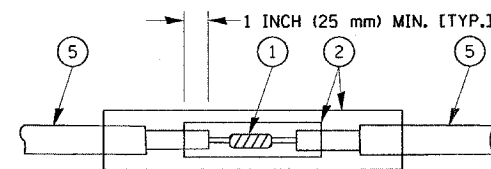
DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

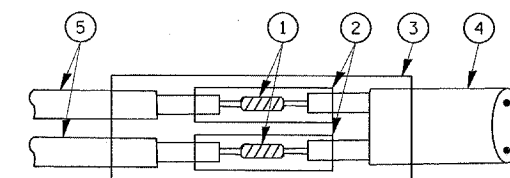
LOOP LEAD-IN CABLE TAG



- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

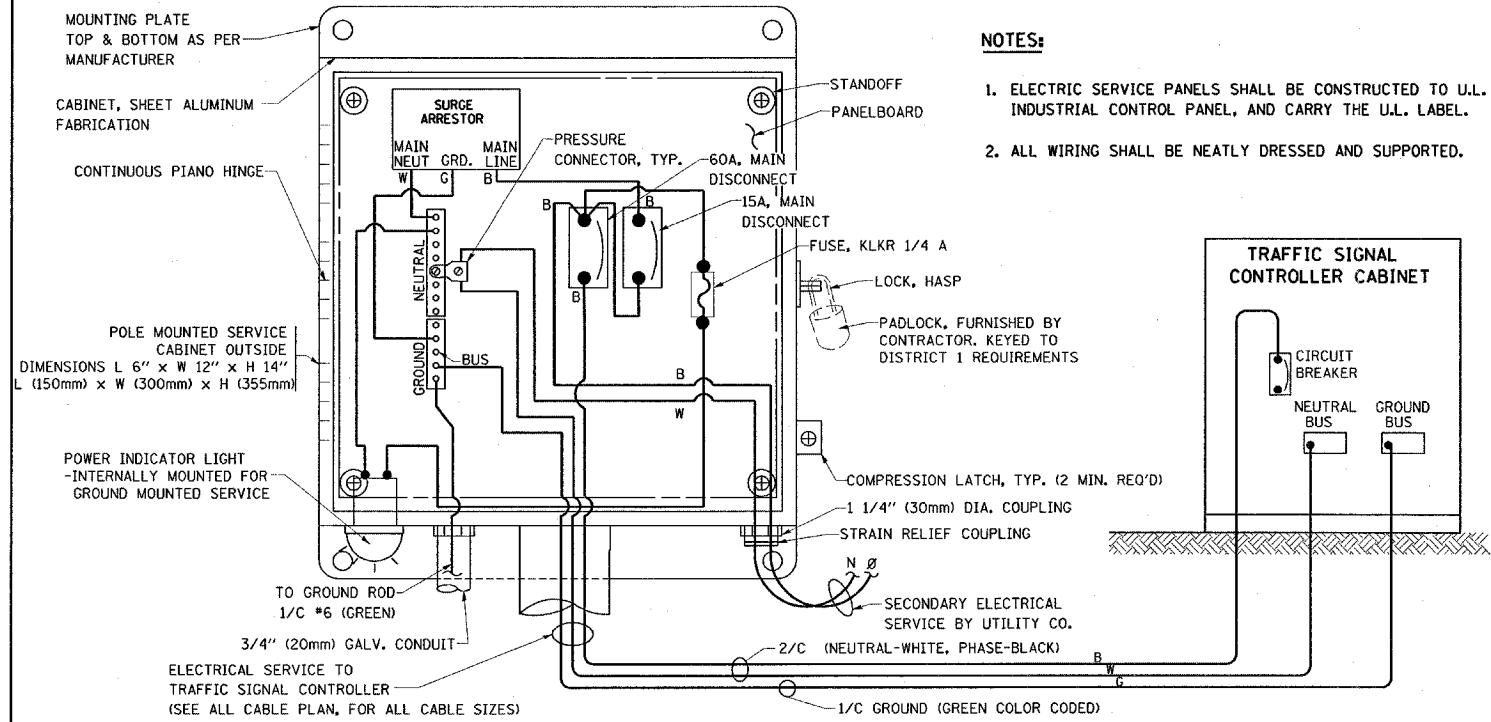
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	PLOT DATE = 3/13/2008	CHECKED - D.A.Z.	REVISED -
		DATE - 05-30-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

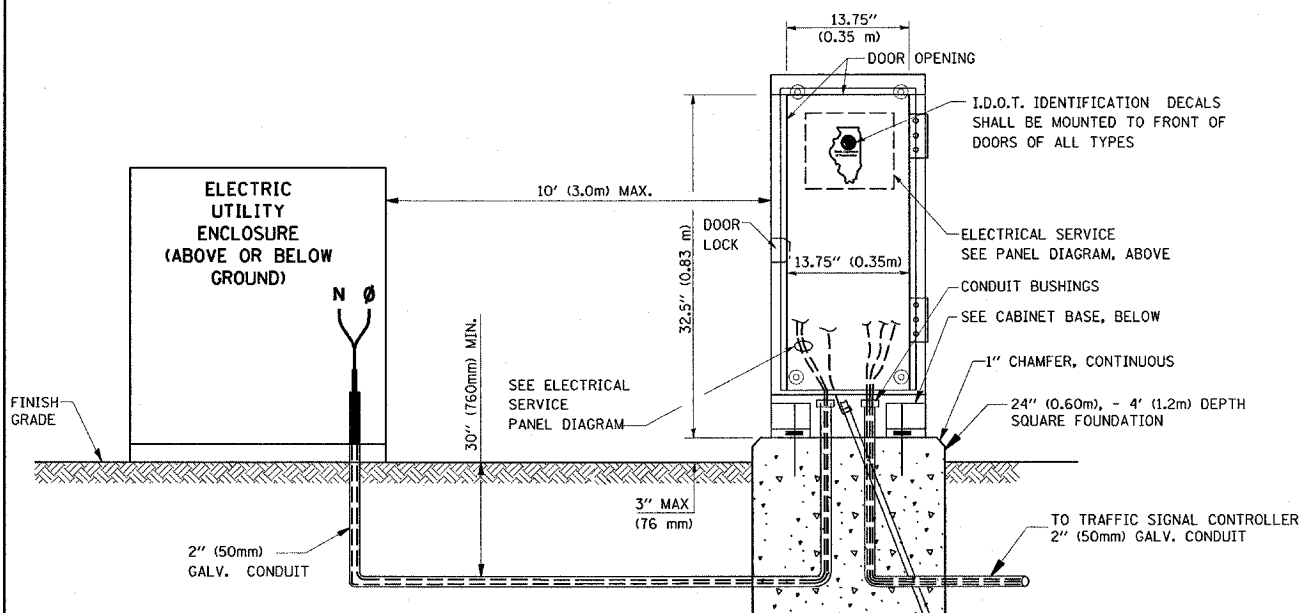
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 1 OF 4 SHEETS STA. TO STA.

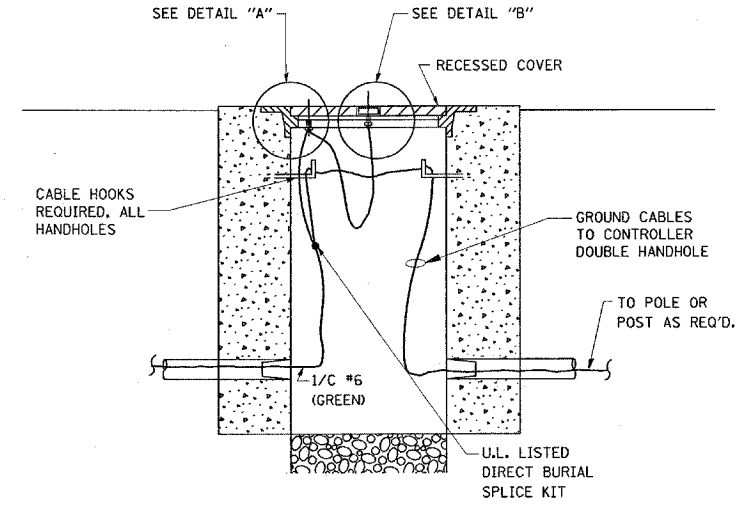
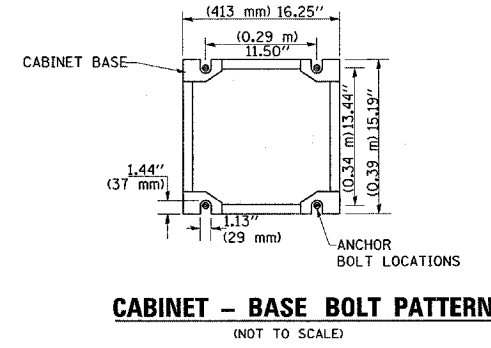
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			34	30
TS-05			CONTRACT NO. 62765	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				



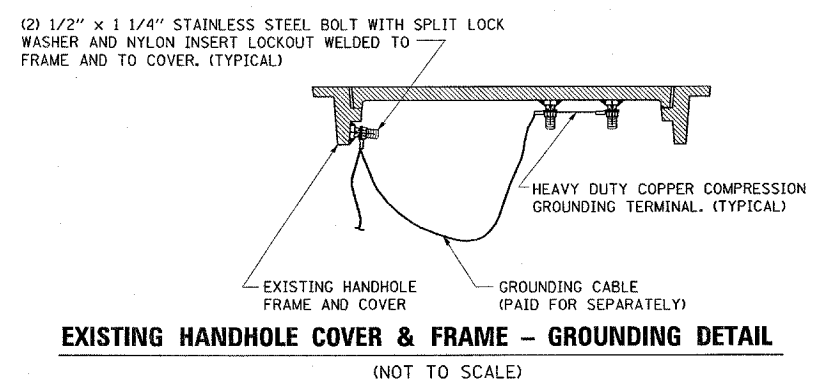
ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



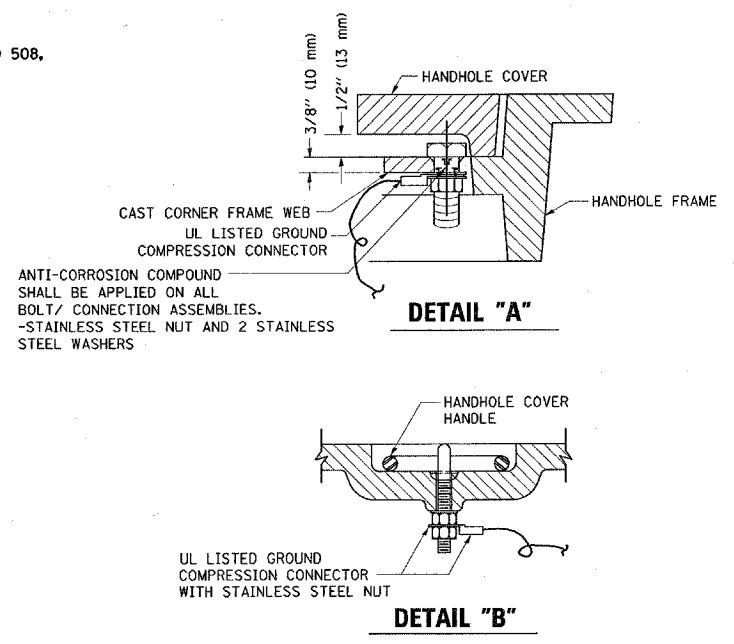
SERVICE INSTALLATION GROUND MOUNT
 (NOT TO SCALE)



HANDHOLE COVER & FRAME - GROUNDING DETAIL
 (NOT TO SCALE)



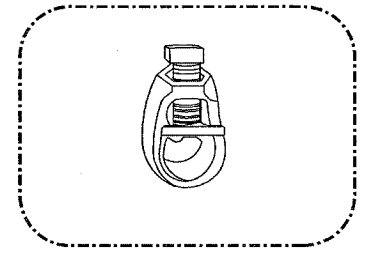
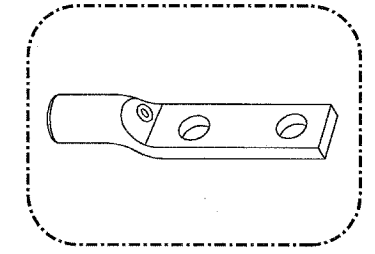
EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL
 (NOT TO SCALE)



NOTES:

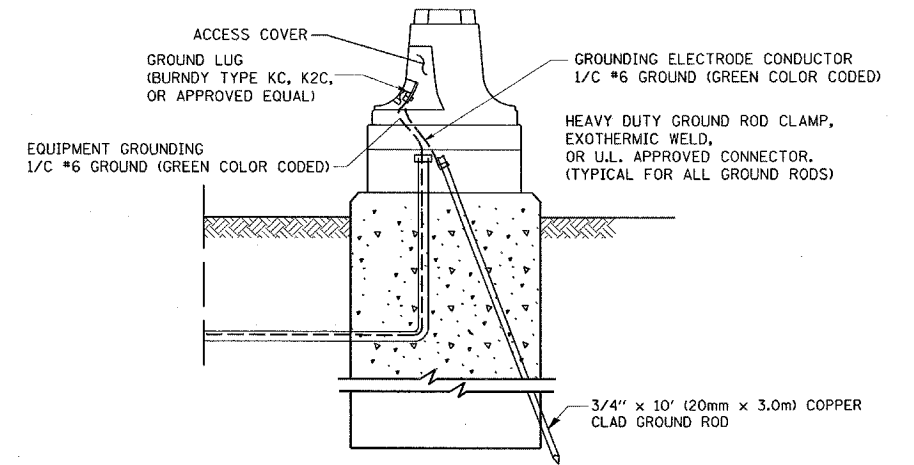
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



MAST ARM POLE / POST-GROUNDING DETAIL
 (NOT TO SCALE)

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W:\dststd\22x34\ts05.dgn	
PLOT SCALE = 50.0000' / IN.	
PLOT DATE = 3/13/2008	

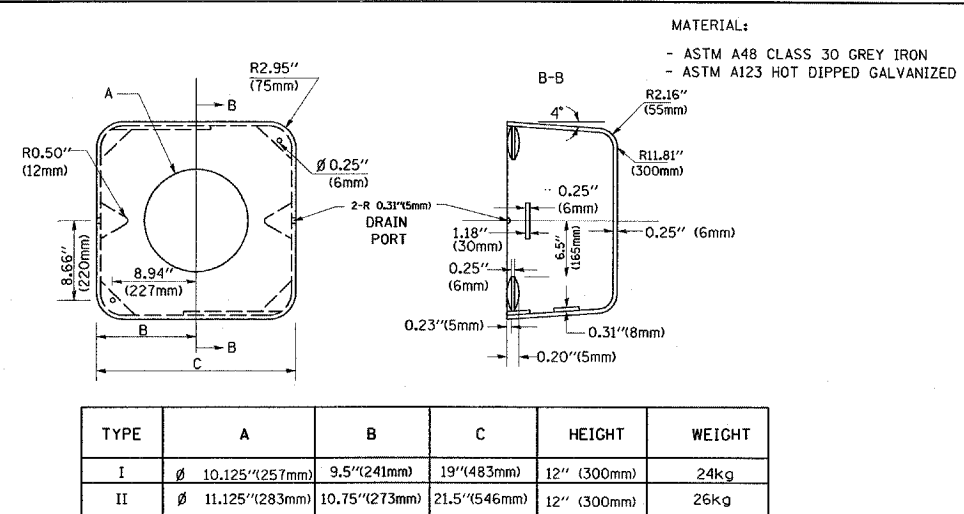
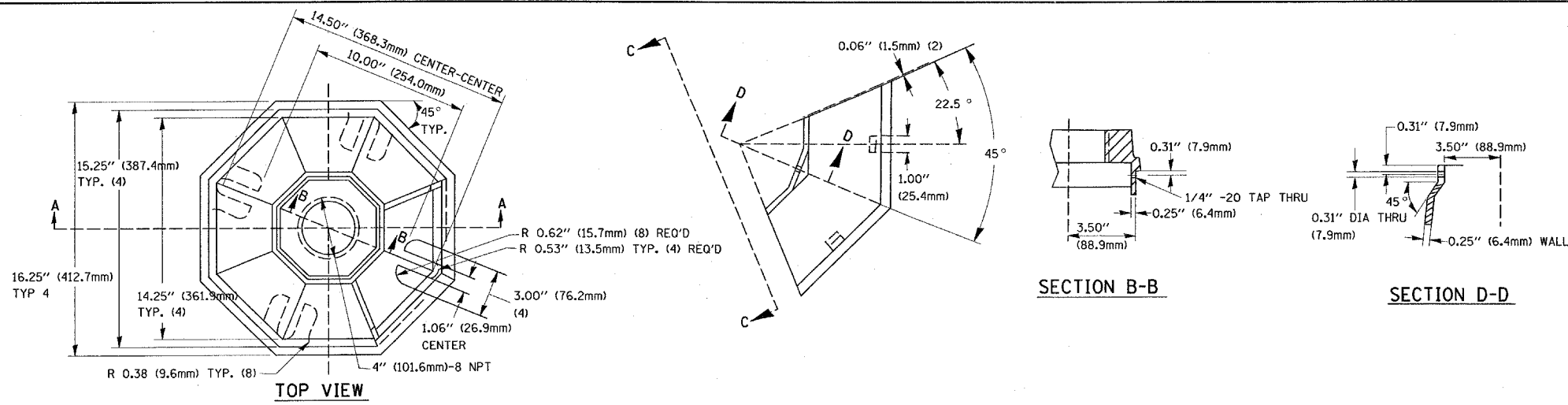
DESIGNED - D.A.D.	REVISED - 03-15-01
DRAWN - R.W.P.	REVISED - BUR. TRAFFIC 01-01-02
CHECKED - D.A.Z.	REVISED -
DATE - 05-30-00	REVISED -

DESIGNED - D.A.D.	REVISED - 03-15-01
DRAWN - R.W.P.	REVISED - BUR. TRAFFIC 01-01-02
CHECKED - D.A.Z.	REVISED -
DATE - 05-30-00	REVISED -

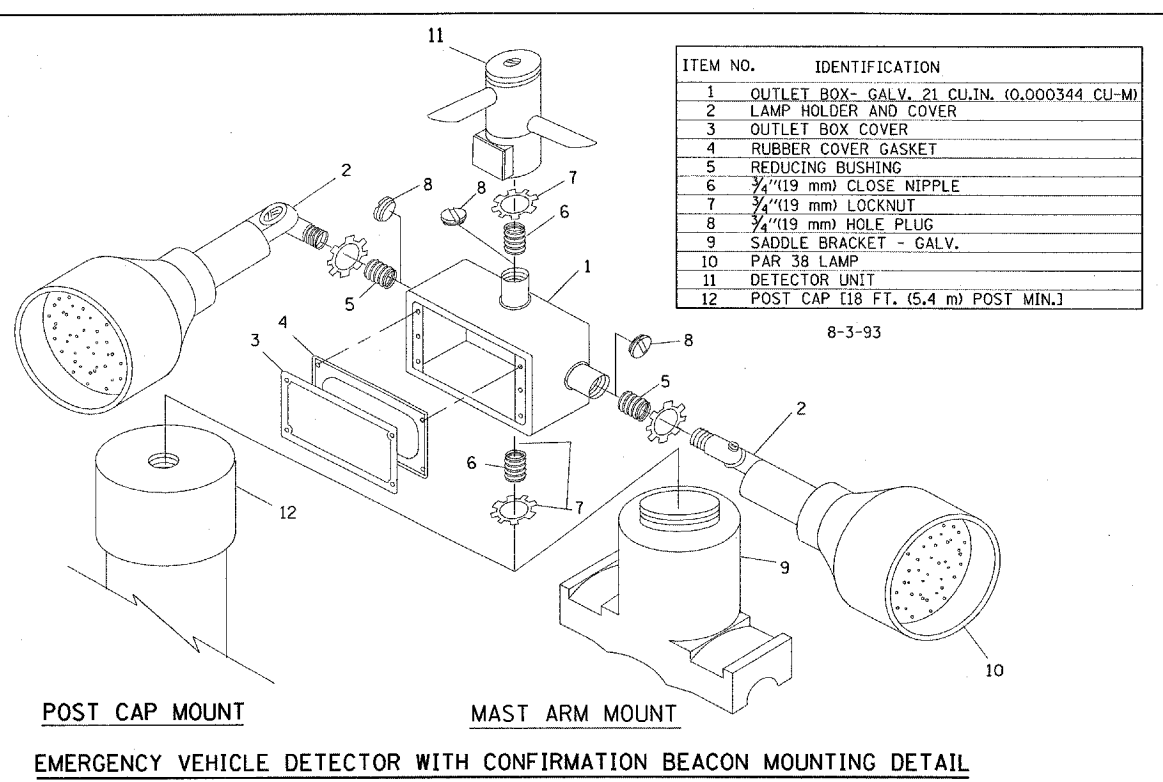
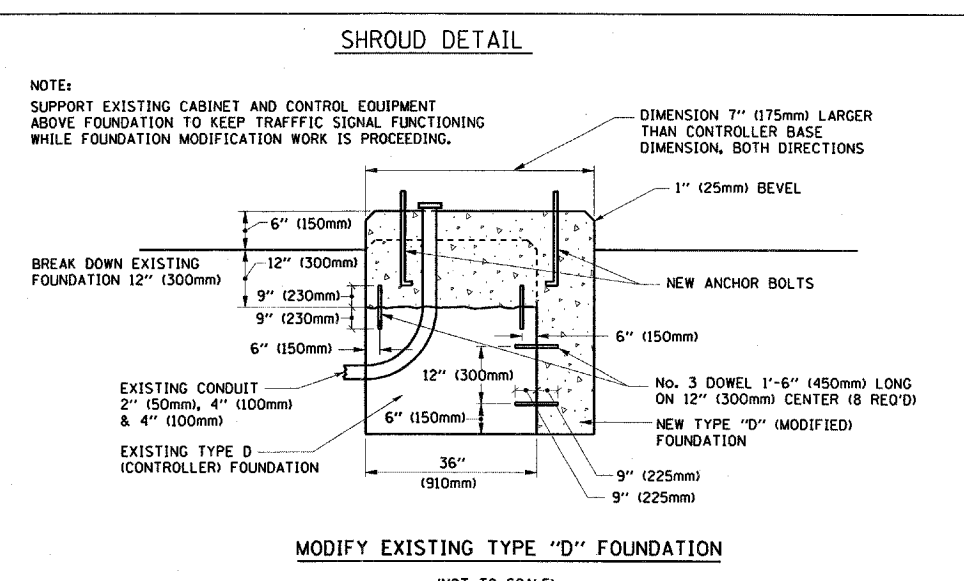
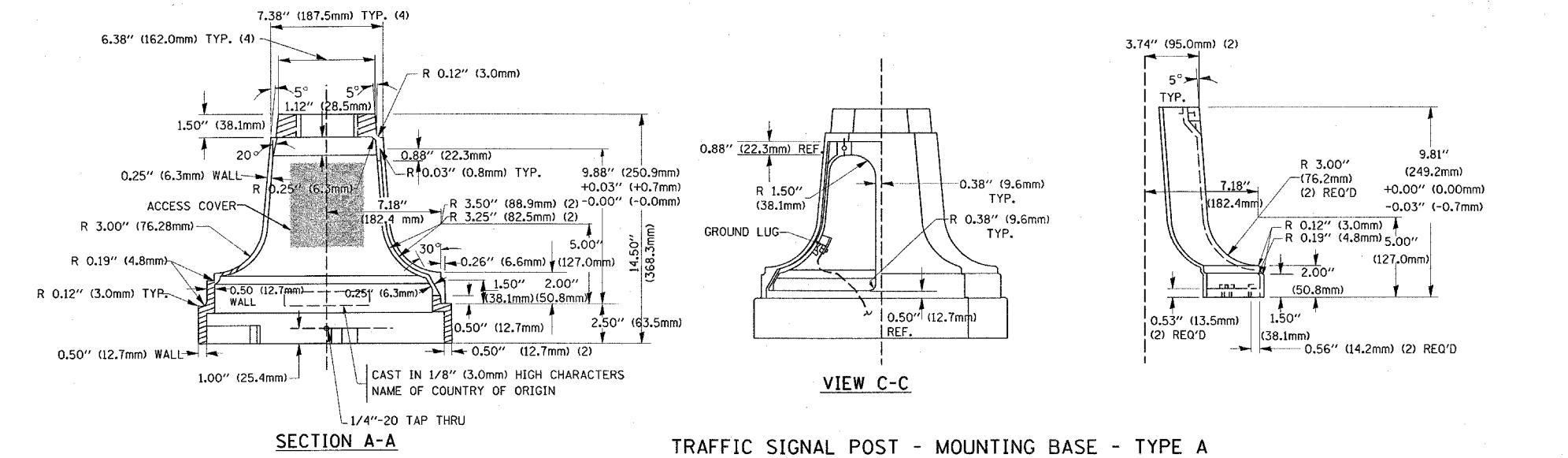
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE	
STANDARD TRAFFIC SIGNAL DESIGN DETAILS	
SCALE: NONE	SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			34	32
TS-05		CONTRACT NO. 62765		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



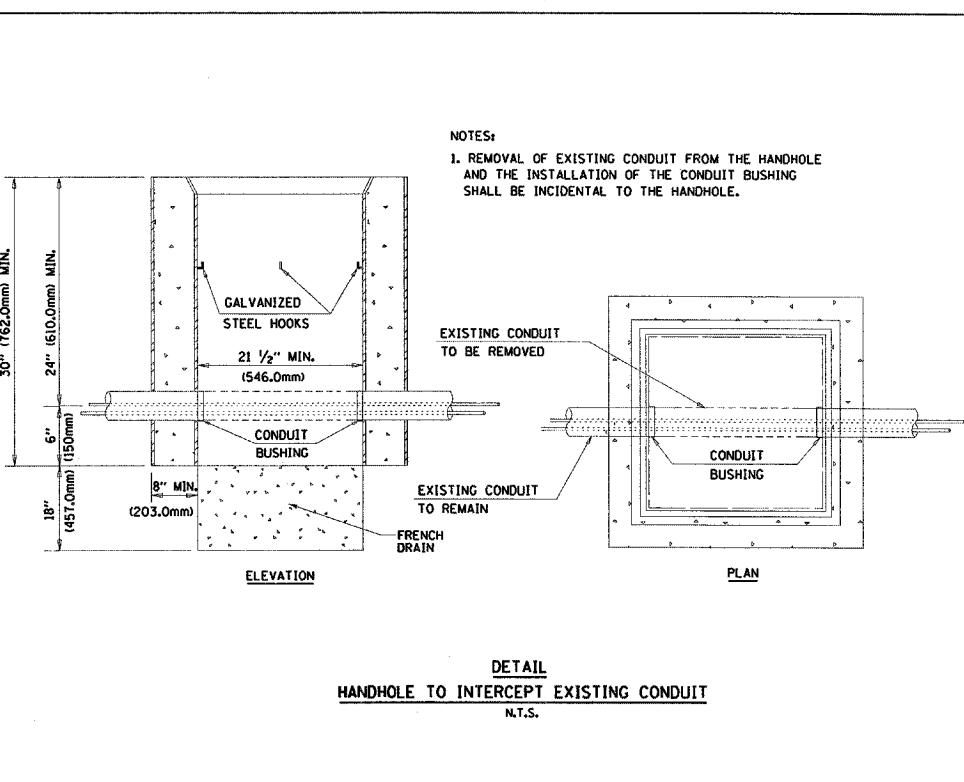
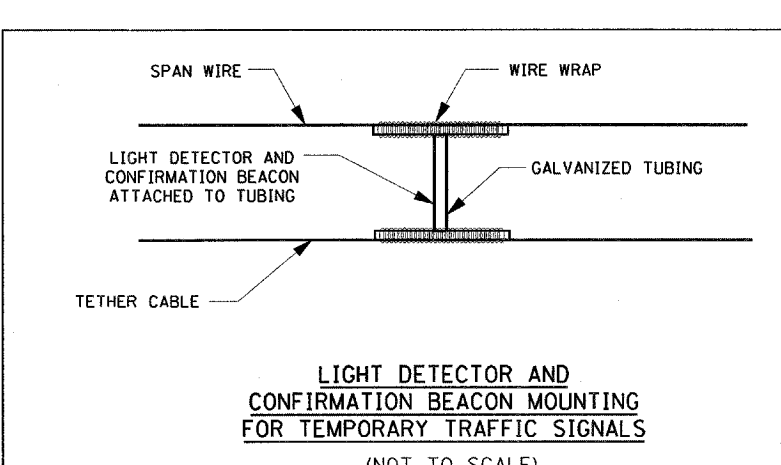
TYPE	A	B	C	HEIGHT	WEIGHT
I	Ø 10.125\"(257mm)	9.5\"(241mm)	19\"(483mm)	12\"(300mm)	24kg
II	Ø 11.125\"(283mm)	10.75\"(273mm)	21.5\"(546mm)	12\"(300mm)	26kg



ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\"(19 mm) CLOSE NIPPLE
7	3/4\"(19 mm) LOCKNUT
8	3/4\"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	PAR 38 LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

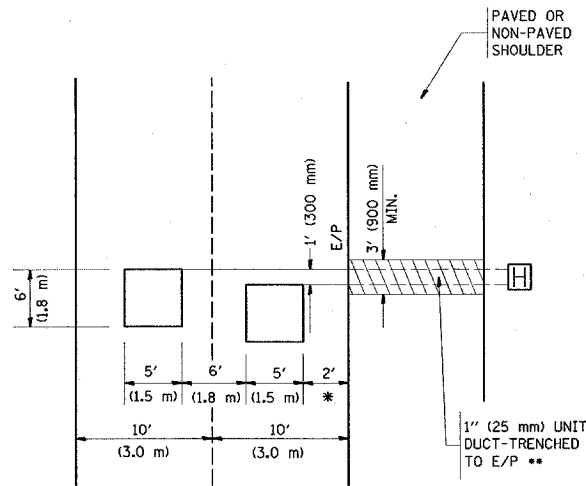
NOTES:

- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

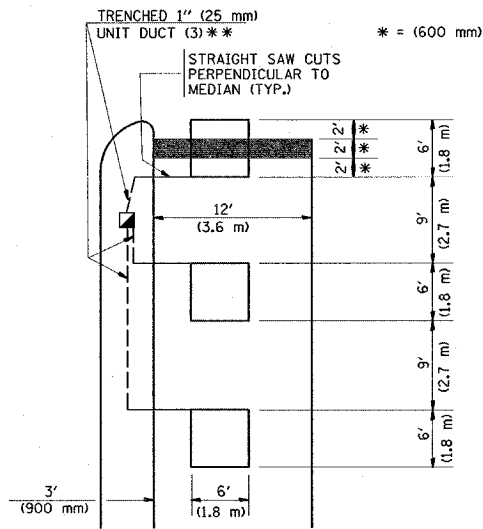


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

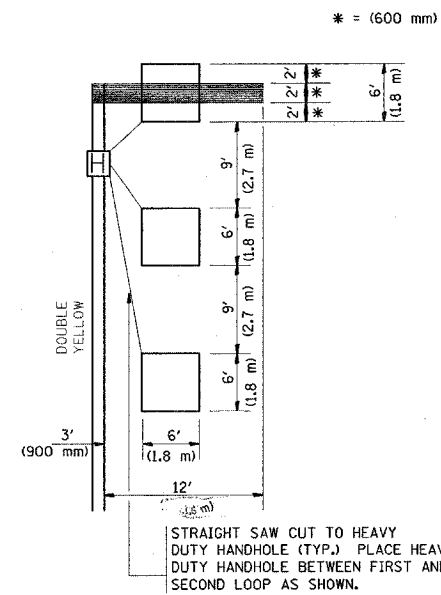
HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

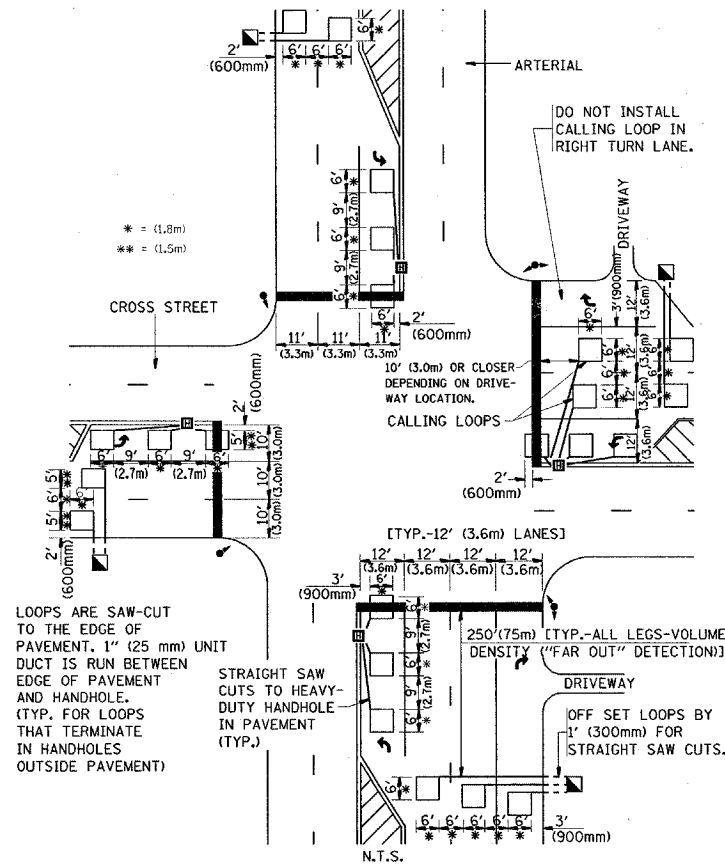
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



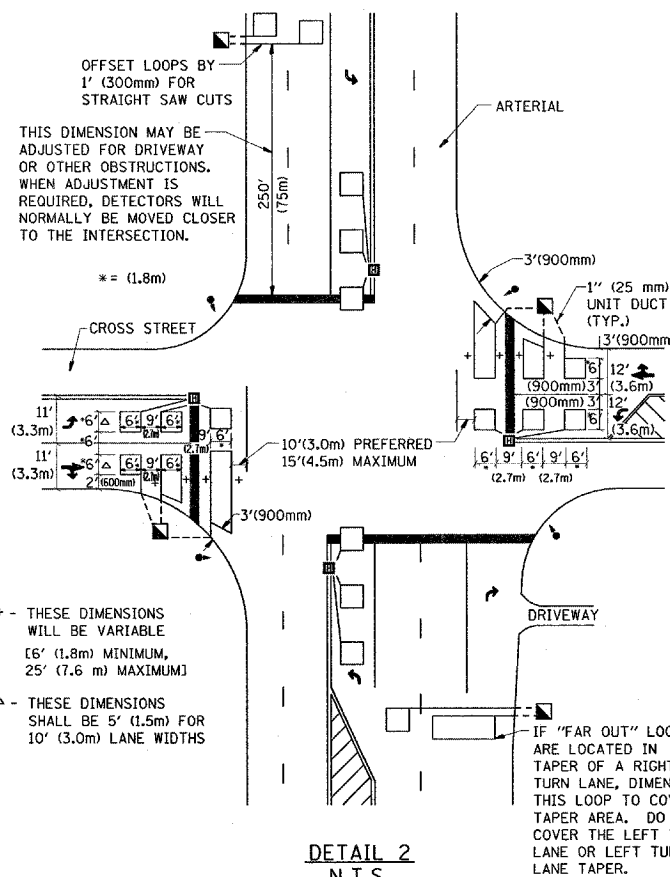
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\distrtd\22x34\ts07.dgn	USER NAME = bjunsh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE = 50,0000 1 / IN.	CHECKED - R.K.F.	DRAWN -	REVISED -		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	TS-07	34	34
PLOT DATE = 3/13/2008	DATE -	CHECKED -	REVISED -								FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT
		DATE -	REVISED -								CONTRACT NO.	62765	