

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PROPOSED  
HIGHWAY PLANS**

F.A.P. RTE. 372 (IL RTE. 171)  
AT FOREST AVE./RIDGEWOOD RD.  
SECTION 0303S-TS  
PROJECT: *ACHSIP-0372 (011)*  
TRAFFIC SIGNAL MODERNIZATION  
AND CHANNELIZATION  
COOK COUNTY  
C-91-436-13

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0303S-TS	COOK	87	1
ILLINOIS			CONTRACT NO. 60X08	

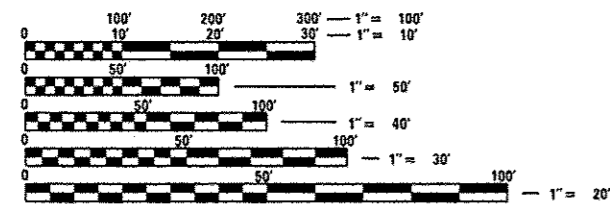
FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN  
THE VILLAGE OF RIVERSIDE

TRAFFIC DATA:

IL RTE. 171  
2013 ADT = 42,700  
SPEED LIMIT = 35 MPH

FOREST AVE./RIDGEWOOD RD.  
2010 ADT = 6,300  
SPEED LIMIT = 25 MPH

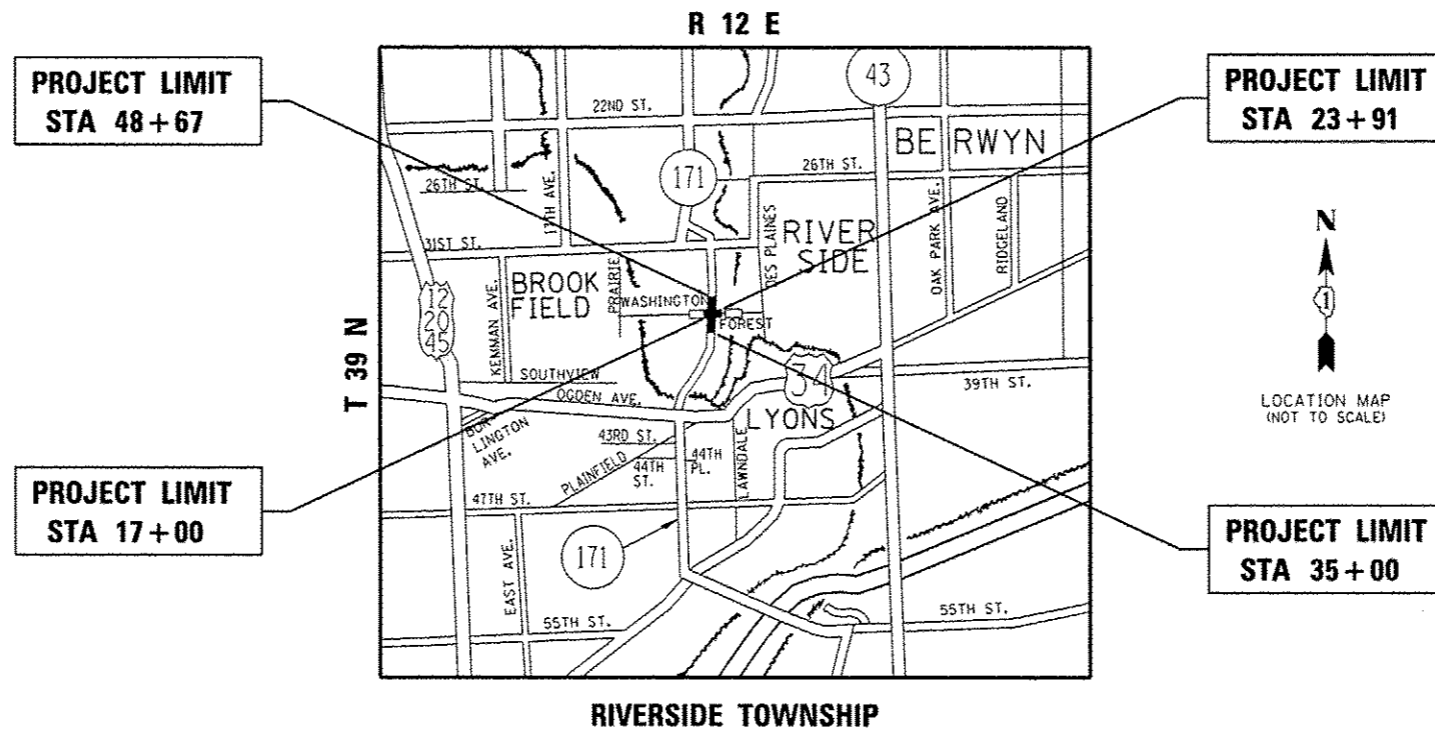


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: DAN WILGREEN (847) 705-4240  
PROJECT MANAGER: KEN ENG (847) 705-4247

CONTRACT NO. 60X08



GROSS AND NET LENGTH = 2058 FT. = 0.390 MILE



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED *March 21* 20 *14*

*John Forman Jr.*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

*May 9* 20 *14*  
*John D. Baranzelli, PE, Inc.*  
ENGINEER OF DESIGN AND ENVIRONMENT

*May 9* 20 *14*  
*Orker Osman, PE, Inc.*  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

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**STATE STANDARDS**

STANDARD NO.	DESCRIPTION
000001-06	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-07	PERPENDICULAR CURP RAMPS FOR SIDEWALKS
424011-01	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424021-02	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
601001-04	SUB-SURFACE DRAINS
601101-01	CONCRETE HEADWALL FOR PIPE DRAIN
602001-02	CATCH BASIN TYPE A
602011-02	CATCH BASIN TYPE C
602401-03	MANHOLE TYPE A
602601-03	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
604001-03	FRAME AND LIDS, TYPE 1
604061-02	FRAME AND GRATE TYPE 12
604086-02	FRAME AND GRATE TYPE 23
606001-05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
701101-04	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24' (600 mm) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701427-02	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS <= 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701602-07	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-03	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
728001-01	TELESCOPING STEEL SIGN SUPPORT
731001-01	BASE FOR TELESCOPING STEEL SIGN SUPPORT
780001-04	TYPICAL PAVEMENT MARKINGS
814001-02	HANDHOLES
814006-02	DOUBLE HANDHOLES
857001-01	STANDARD SIGNAL DESIGNATION DIAGRAMS AND PHASE SEQUENCES
862001-01	UNINTERRUPTABLE POWER SUPPLY (UPS)
877011-05	STEEL COMB. MAST ARM ASSEMBLY AND POLE 16' THOUGH 55'
878001-09	CONCRETE FOUNDATION DETAILS

- TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- TOP OF FRAME ("RIM") ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF EACH STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATIONS OF THE AREAS IN WHICH THEY ARE LOCATED, AS PART OF THE STRUCTURE COST. TOP OF FRAME ("RIM") ELEVATIONS SHOWN ON THE PLANS FOR STRUCTURES LOCATED IN THE CURB LINE ARE GIVEN AT THE EDGE OF PAVEMENT.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE RESIDENT ENGINEER SHALL CONTACT JOE ECKERT, AREA TRAFFIC FIELD TECHNICIAN, AT (224) 217-8632 A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)".
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARMS LENGTHS.
- AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH AGGREGATE SUBGRADE IMPROVEMENT (CU YD) WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 (01/01/2012) AND THE IDOT SUBGRADE STABILITY MANUAL (05/01/2005). IF UNSTABLE AND/OR UNSUITABLE SOILS IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
- ALL PIPE UNDERDRAINS SHALL BE PLACED AT A DEPTH OF 30" BELOW THE TOP OF THE PROPOSED PAVEMENT OR AS DEEP AS POSSIBLE AND IN ACCORDANCE WITH CHECK SHEET #19 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.

**GENERAL NOTES**

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF RIVERSIDE.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

**COMMITMENTS**

- THE MONUMENTS AND ATTACHED HANGING SIGNS IN THE NORTHEAST AND SOUTHEAST CORNERS OF IL ROUTE 171 AND FOREST AVE. WILL NOT BE IMPACTED.

FILE NAME :	USER NAME : teriafm	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 171 AT FOREST AVE./RIDGWOOD RD.</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
as:\pwork\pwork\teriafm\0315740\01144	2-Design.dgn	DRAWN -	REVISED -		<b>INDEX OF SHEETS, STATE STANDARDS, GEN. NOTES, AND COMMITMENTS</b>				372	03035-TS	COOK	87	2
Default	PLOT SCALE = 100.000' / in.	CHECKED -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	<b>CONTRACT NO. 60X08</b>	
	PLOT DATE = 3/31/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								



SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE																		
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	90% FED 10% STATE	90% FED 5% STATE 5% RIVERSIDE	100% RIVERSIDE	90% FED 90% RIVERSIDE	80% STATE 20% RIVERSIDE	90% FED 10% STATE	100% RIVERSIDE											
				ROADWAY 0005	TR. SIGNALS 0021	E.V.P. 0021	COMBINATION LIGHTING 0021	SHARED-USE PATH AND SIDEWALK 0028	INTERCONNECT 0021	UTILITIES 0043											
35101800	AGGREGATE BASE COURSE, TYPE B 6"	SQ YD	842	335				507													
35600670	HOT-MIX ASPHALT BASE COURSE WIDENING, 5"	SQ YD	105	105																	
35600714	HOT-MIX ASPHALT BASE COURSE WIDENING, 9 1/2"	SQ YD	172	172																	
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	17	17																	
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4, 75, N50	TON	474	474																	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	84	84																	
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	8	8																	
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	95	38				57													
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	1127	1127																	
42001300	PROTECTIVE COAT	SQ YD	721	654				67													
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	3223	2621				602													
42400800	DETECTABLE WARNINGS	SQ FT	228	193				35													
44000100	PAVEMENT REMOVAL	SQ YD	357	357																	
* SPECIALTY ITEMS																					

FILE NAME =	USER NAME = Torigm	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL. ROUTE 171 AT FOREST AVE./RIDGWOOD RD. SUMMARY OF QUANTITIES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\p\work\p100\101\m10315749.P10412.Dwg		DRAWN -	REVISED -		372	03035-T5	COOK	87	4				
PLOT SCALE = 1/8" = 1'		CHECKED -	REVISED -		SCALE:				SHEET NO.	OF	SHEETS	STA.	TO STA.
PLOT DATE = 4/3/2014		DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				CONTRACT NO. 60X08				



SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE																
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	90% FED 10% STATE	90% FED 5% STATE 5% RIVERSIDE	100% RIVERSIDE	90% FED 80% RIVERSIDE	80% STATE 20% RIVERSIDE	90% FED 10% STATE	100% RIVERSIDE									
				ROADWAY 0005	TR. SIGNALS 0021	E.V.P. 0021	COMBINATION LIGHTING 0021	SHARED-USE PATH AND SIDEWALK 0028	INTERCONNECT 0021	UTILITIES 0043									
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	38	38															
550A0400	STORM SEWERS, CLASS A, TYPE 2 21"	FOOT	26	26															
55100300	STORM SEWER REMOVAL 8"	FOOT	52	52															
55100500	STORM SEWER REMOVAL 12"	FOOT	56	56															
60107600	PIPE UNDERDRAINS 4"	FOOT	350	350															
60201330	CATCH BASINS, TYPE A, 4' -DIAMETER, TYPE 23 FRAME AND GRATE	EACH	1	1															
60208105	CATCH BASINS, TYPE C, TYPE 12 FRAME AND GRATE	EACH	1	1															
60208230	CATCH BASINS, TYPE C, TYPE 23 FRAME AND GRATE	EACH	4	4															
60218400	MANHOLES, TYPE A, 4' -DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1															
60221100	MANHOLES, TYPE A, 5' -DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1															
60250500	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1	1															
60251730	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 23 FRAME AND GRATE	EACH	1	1															

\* SPECIALTY ITEMS

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FILE NAME :	USER NAME : Tardifm	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD. SUMMARY OF QUANTITIES</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
C:\pwworking\rdofm\03151481\1514812-Design.dwg	DRAWN -	REVISED -	372			03035-TS	COOK	87	6	
PLOT SCALE : 100,000 / 1"	CHECKED -	REVISED -	CONTRACT NO. 60X08							
PLOT DATE : 3/31/2014	DATE -	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
SCALE:		SHEET NO. OF SHEETS		STA. TO STA.						

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE																		
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	90% FED 10% STATE	90% FED 5% STATE 5% RIVERSIDE	100% RIVERSIDE	90% FED 10% RIVERSIDE	80% STATE 20% RIVERSIDE	90% FED 10% STATE	100% RIVERSIDE											
				ROADWAY 0005	TR. SIGNALS 0021	E.V.P. 0021	COMBINATION LIGHTING 0021	SHARED-USE PATH AND SIDEWALK 0028	INTERCONNECT 0021	UTILITIES 0043											
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1					1													
60260400	INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1	1																	
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	5	5																	
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	12	10				2													
60500050	REMOVING CATCH BASINS	EACH	1	1																	
60500060	REMOVING INLETS	EACH	4	4																	
60600605	CONCRETE CURB, TYPE B	FOOT	108	108																	
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	205	205																	
60604400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18	FOOT	1060	1060																	
63200310	GUARDRAIL REMOVAL	FOOT	131	131																	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6																	
67100100	MOBILIZATION	L SUM	1	1																	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	6957	6957																	
											* SPECIALTY ITEMS										

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\* SPECIALTY ITEMS

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE																		
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	90% FED 10% STATE	90% FED 5% STATE 5% RIVERSIDE	100% RIVERSIDE	90% FED 10% RIVERSIDE	80% STATE 20% RIVERSIDE	90% FED 10% STATE	100% RIVERSIDE											
				ROADWAY 0005	TR. SIGNALS 0021	E.V.P. 0021	COMBINATION LIGHTING 0021	SHARED-USE PATH AND SIDEWALK 0028	INTERCONNECT 0021	UTILITIES 0043											
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	292	292																	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	6320	6320																	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1025	1025																	
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	635	635																	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	137	137																	
70300510	PAVEMENT MARKING TAPE, TYPE III - LETTERS AND SYMBOLS	SQ FT	146	146																	
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	6272	6272																	
70300540	PAVEMENT MARKING TAPE, TYPE III 6"	FOOT	612	612																	
70300560	PAVEMENT MARKING TAPE, TYPE III 12"	FOOT	151	151																	
70300570	PAVEMENT MARKING TAPE, TYPE III 24"	FOOT	108	108																	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	773	773																	
* 72000100	SIGN PANEL - TYPE 1	SQ FT	166.3	166.3																	
* 72000200	SIGN PANEL - TYPE 2	SQ FT	60	60																	
* 72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	5	5																	
* 72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	2	2																	
											* SPECIALTY ITEMS										

FILE NAME =	USER NAME = Taylor	DESIGNED -	REVISED -
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		CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD.			
SUMMARY OF QUANTITIES			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0303S-TS	COOK	87	8
CONTRACT NO. 60X08				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE																			
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	90% FED 10% STATE	90% FED 5% STATE 5% RIVERSIDE	100% RIVERSIDE	90% FED 90% RIVERSIDE	80% STATE 20% RIVERSIDE	90% FED 10% STATE	100% RIVERSIDE												
				ROADWAY	TR. SIGNALS 0021	E.V.P. 0021	COMBINATION LIGHTING 0021	SHARED-USE PATH AND SIDEWALK 0028	INTERCONNECT 0021	UTILITIES 0043												
				URBAN	0005																	
* 72400310	REMOVE SIGN PANEL - TYPE 1	SQ FT	102.2	102.2																		
* 72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	EACH	1	1																		
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	93.5	93.5																		
* 73100100	BASE FOR TELESCOPING STEEL SIGN SUPPORT	EACH	6	6																		
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	292	292																		
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	6320	6320																		
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1025	1025																		
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	635	635																		
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	137	137																		
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	151	151																		
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	8	8																		
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	2	2																		
	78300100 PAVEMENT MARKING REMOVAL	SQ FT	2307	2307																		
* 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	141	141																		
* 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	815		740				75		* SPECIALTY ITEMS											

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE																			
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	90% FED 10% STATE	90% FED 5% STATE 5% RIVERSIDE	100% RIVERSIDE	90% FED 10% RIVERSIDE	80% STATE 20% RIVERSIDE	90% FED 10% STATE	100% RIVERSIDE												
				ROADWAY 0005	TR. SIGNALS 0021	E.V.P. 0021	COMBINATION LIGHTING 0021	SHARED-USE PATH AND SIDEWALK 002B	INTERCONNECT 0021	UTILITIES 0043												
* 81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	45		45																	
* 81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	70		70																	
* 81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	600		600																	
* 81400100	HANDHOLE	EACH	3		3																	
* 81400200	HEAVY-DUTY HANDHOLE	EACH	4		4																	
* 81400300	DOUBLE HANDHOLE	EACH	4		4																	
* 81702110	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 10	FOOT	1600				1600															
* 81702120	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 8	FOOT	275				275															
* 81702130	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6	FOOT	150				150															
* 81800240	AERIAL CABLE, 2-1/C NO. 8 WITH MESSENGER WIRE	FOOT	1425		1425																	
* 82103310	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, PHOTO-CELL CONTROL, 310 WATT	EACH	8		4		4															
* 84200500	REMOVAL OF LIGHTING UNIT, SALVAGE	EACH	1	1																		
													* SPECIALTY ITEMS									

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE																					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	90% FED 10% STATE	90% FED 5% STATE 5% RIVERSIDE	100% RIVERSIDE	90% FED 30% RIVERSIDE	80% STATE 20% RIVERSIDE	90% FED 10% STATE	100% RIVERSIDE														
				ROADWAY URBAN	TR. SIGNALS 0021	E.V.P. 0021	COMBINATION LIGHTING 0021	SHARED-USE PATH AND SIDEWALK 0028	INTERCONNECT 0021	UTILITIES 0043														
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2		1				1															
* 86400100	TRANSCEIVER - FIBER OPTIC	EACH	1		1																			
* 87300925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	2950						2950															
* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1250		1250																			
* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1600		1600																			
* 87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1775		1775																			
* 87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1475		1475																			
* 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	2525		2525																			
* 87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	125		125																			
* 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	1100		1100																			
* 87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	4		4																			
* 87702880	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 30 FT.	EACH	1		1																			
* SPECIALTY ITEMS																								

SUMMARY OF QUANTITIES

CONSTRUCTION TYPE CODE

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE																	
				90% FED 10% STATE ROADWAY	90% FED 5% STATE 5% RIVERSIDE TR. SIGNALS	100% RIVERSIDE E.V.P.	90% FED 80% RIVERSIDE COMBINATION LIGHTING	80% STATE 20% RIVERSIDE SHARED-USE PATH AND SIDEWALK	90% FED 10% STATE INTERCONNECT	100% RIVERSIDE UTILITIES											
				0005	0021	0021	0021	002R	0021	0043											
* 87702900	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 34 FT.	EACH	1		1																
* 87702910	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 36 FT.	EACH	1		1																
* 87702920	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 38 FT.	EACH	1		1																
* 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	16		16																
* 87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4		4																
* 87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	44		44																
* 87900200	DRILL EXISTING HANDHOLE	EACH	1							1											
* 88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	6		6																
* 88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4		4																
* 88030240	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	4		4																
* 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8		8																

\* SPECIALTY ITEMS

FILE NAME =	USER NAME = Torigin	DESIGNED -	REVISED -
0:\p\2011\p1150\1011\10115157\10115142-Design.dwg		DRAWN -	REVISED -
PLOT SCALE = 100,000 1" = 100'		CHECKED -	REVISED -
PLOT DATE = 3/31/2014		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD.  
SUMMARY OF QUANTITIES**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	12
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60X08	

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE																		
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	90% FED 10% STATE	90% FED 5% STATE 5% RIVERSIDE	100% RIVERSIDE	90% FED 10% RIVERSIDE	80% STATE 20% RIVERSIDE	90% FED 10% STATE	100% RIVERSIDE											
				ROADWAY 0005	TR. SIGNALS 0021	E.V.P. 0021	COMBINATION LIGHTING 0021	SHARED-USE PATH AND SIDEWALK 0028	INTERCONNECT 0021	UTILITIES 0043											
* 88200210	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	10		10																
* 88500100	INDUCTIVE LOOP DETECTOR	EACH	11		11																
* 88600100	DETECTOR LOOP, TYPE I	FOOT	910		910																
* 88800100	PEDESTRIAN PUSH-BUTTON	EACH	8		8																
* 89501400	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT	EACH	2			2															
* 89501410	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT	EACH	1			1															
* 89502210	MODIFY EXISTING CONTROLLER CABINET	EACH	1		1																
* 89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	5000						5000												
* 89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1		1																
* 89502380	REMOVE EXISTING HANDHOLE	EACH	8		8																
* 89502382	REMOVE EXISTING DOUBLE HANDHOLE	EACH	1		1																
* 89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	9		9																
X0322917	PROPOSED STORM SEWER CONNECTION TO EXISTING MANHOLE	EACH	3	3																	
											* SPECIALTY ITEMS										

13

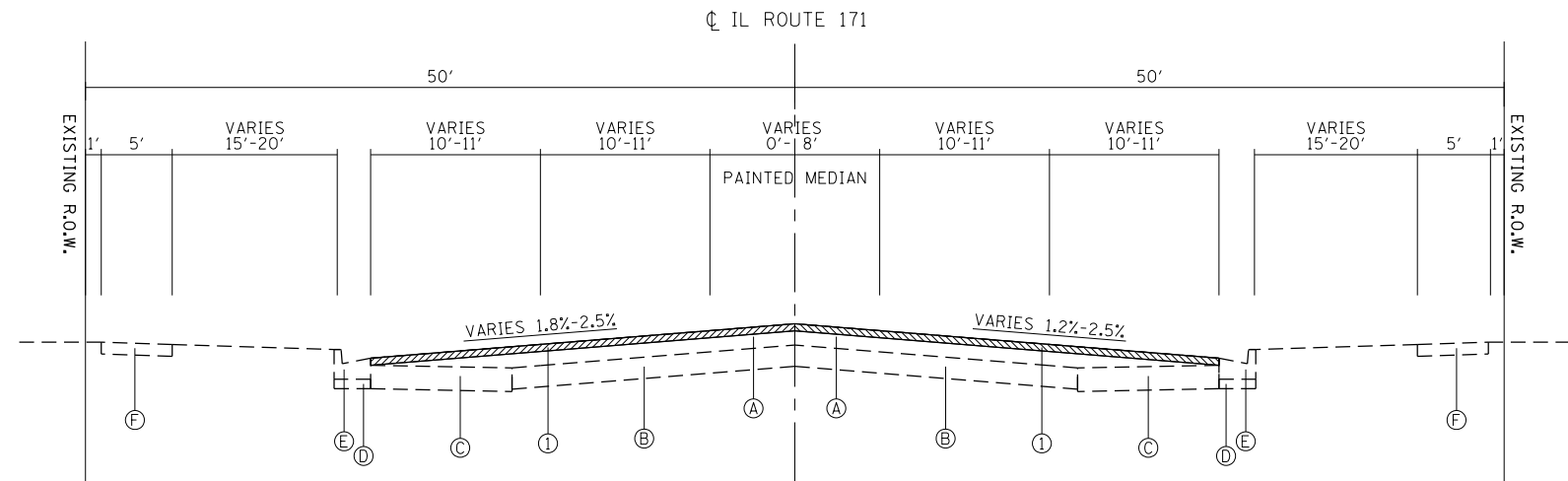
SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE																	
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	90% FED 10% STATE	90% FED 5% STATE 5% RIVERSIDE	100% RIVERSIDE	90% FED 10% RIVERSIDE	80% STATE 20% RIVERSIDE	90% FED 10% STATE	100% RIVERSIDE										
				ROADWAY 0005	TR. SIGNALS 0021	E.V.P. 0021	COMBINATION LIGHTING 0021	SHARED-USE PATH AND SIDEWALK 0028	INTERCONNECT 0021	UTILITIES 0043										
X0322918	PROPOSED MANHOLE/CATCH BASIN CONNECTION OVER EXISTING STORM SEWER	EACH	4	4																
* X0324085	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	320			320														
X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	1	1																
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	1	1																
X4060110	BITUMINOUS MATERIALS (PRIME COAT)	POUND	7834	7834																
① X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	450	450																
X6020095	MANHOLES, TYPE A, 4' DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE	EACH	1	1																
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	17	14						3										
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1																
* X8570226	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1		1															
* X8620200	UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1		1															
* X8710024	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	2950						2950											

\* SPECIALTY ITEMS

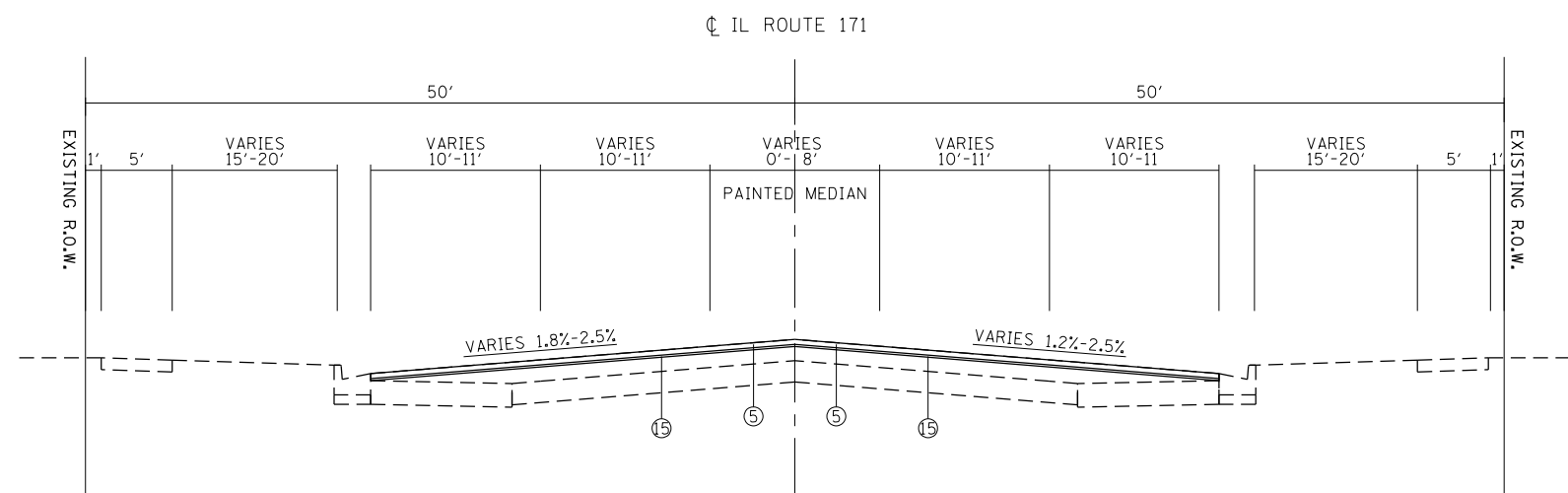
©100% STATE (NP)

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE																			
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	90% FED 10% STATE	90% FED 5% STATE 5% RIVERSIDE	100% RIVERSIDE	90% FED 90% RIVERSIDE	80% STATE 20% RIVERSIDE	90% FED 10% STATE	100% RIVERSIDE												
				ROADWAY	TR. SIGNALS	E.V.P.	COMBINATION LIGHTING	SHARED-USE PATH AND SIDEWALK	INTERCONNECT	UTILITIES												
			URBAN	0005	0021	0021	0021	0028	0021	0043												
Z0004530	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 8"	SQ YD	27	27																		
Z0004538	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 10"	SQ YD	76	76																		
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	30	30																		
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1																		
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	16	16																		
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	116	116																		
* Z0033020	LUMINAIRE SAFETY CABLE ASSEMBLY	EACH	4				4															
* Z0033040	ELECTRIC SERVICE DISCONNECT, LIGHTING AND TRAFFIC SIGNAL	EACH	1		1																	
* Z0033046	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2	EACH	1						1													
Z0038123	PORTLAND CEMENT CONCRETE SURFACE REMOVAL 2 1/2"	SQ YD	26	26																		
Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1		1																	
											* SPECIALTY ITEMS											

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EXISTING TYPICAL SECTION  
STA. 35+00 TO 37+40



PROPOSED TYPICAL SECTION  
STA. 35+00 TO 37+40

- EXISTING LEGEND:
- (A) HMA SURFACE & BINDER COURSE, ± 10"
  - (B) PCC PAVEMENT, ± 9"
  - (C) HMA BASE COURSE, ± 10"
  - (D) SUBBASE GRANULAR MATERIAL, 4"
  - (E) COMB. CONC. CURB & GUTTER, TYPE B-6.24
  - (F) PCC SIDEWALK

- PROPOSED LEGEND:
- (1) HMA SURFACE REMOVAL, 2 1/2"
  - (2) PAVEMENT REMOVAL
  - (3) COMB. CONC. CURB & GUTTER REMOVAL
  - (4) SIDEWALK REMOVAL
  - (5) POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1 3/4"
  - (6) HMA BASE COURSE WIDENING, 9 1/2"
  - (7) AGGREGATE SUBGRADE IMPROVEMENT, 12"
  - (8) COMB. CONC. CURB & GUTTER, TYPE B-6.18
  - (9) SUBBASE GRANULAR MATERIAL, TYPE B, 6"
  - (10) HMA SURFACE COURSE, MIX "D", N50, (IL 9.5 mm), 2"
  - (11) AGGREGATE BASE COURSE, TYPE B, 6"
  - (12) PCC SIDEWALK, 5"
  - (13) SODDING & TOPSOIL 4"
  - (14) PIPE UNDERDRAIN, 4"
  - (15) POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"

NOTE: THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

	MIXTURE TYPE	AIR VOIDS @ N <sub>DES</sub>	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING	POLYMERIZED HMA SURFACE COURSE, MIX "F", N90 (IL-9.5 mm)	4% @ 90 GYR	QCP
	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% @ 50 GYR	QC/OA
PAVEMENT WIDENING	POLYMERIZED HMA SURFACE COURSE, MIX "F", N90 (IL-9.5 mm)	4% @ 90 GYR	QCP
	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% @ 50 GYR	QC/OA
	HMA BASE COURSE WIDENING, 5" & 9 1/2" (HMA BINDER IL-19.0)	4% @ 70 GYR	QC/OA
HMA DRIVEWAY	HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2"	4% @ 50 GYR	QC/OA
	HMA BASE COURSE (HMA BINDER IL-19 mm); PE - 6", CE - 8"	4% @ 50 GYR	QC/OA
SHARED-USE PATH	HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2"	4% @ 50 GYR	QC/OA
PATCHING	CLASS D PATCHES, (HMA BINDER IL-19 mm)	4% @ 70 GYR	QC/OA
	HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR	QC/OA

QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (QCP)

NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA MIXTURES IS 112 LBS/SQ YD/IN.

NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS. QUALITY MANAGEMENT PROGRAM (QCP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

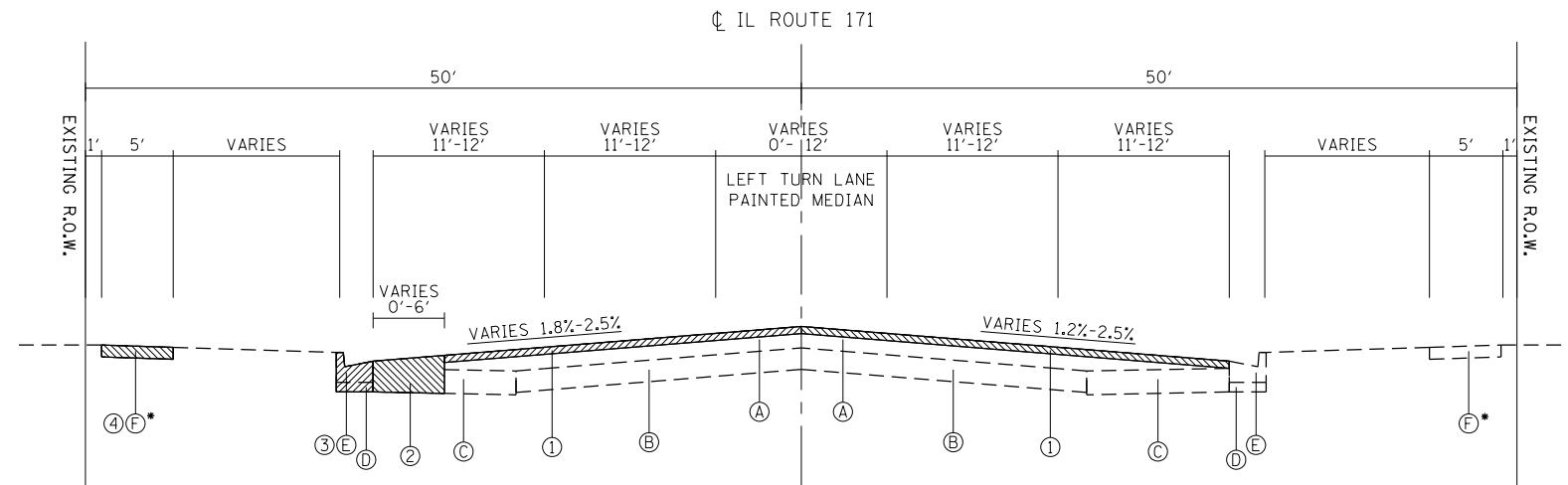


- EXISTING LEGEND:
- Ⓐ HMA SURFACE & BINDER COURSE, ± 10"
  - Ⓑ PCC PAVEMENT, ± 9"
  - Ⓒ HMA BASE COURSE, ± 10"
  - Ⓓ SUBBASE GRANULAR MATERIAL, 4"
  - Ⓔ COMB. CONC. CURB & GUTTER, TYPE B-6.24
  - Ⓕ PCC SIDEWALK

- PROPOSED LEGEND:
- ① HMA SURFACE REMOVAL, 2½"
  - ② PAVEMENT REMOVAL
  - ③ COMB. CONC. CURB & GUTTER REMOVAL
  - ④ SIDEWALK REMOVAL
  - ⑤ POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1¾"
  - ⑥ HMA BASE COURSE WIDENING, 9½"
  - ⑦ AGGREGATE SUBGRADE IMPROVEMENT, 12"
  - ⑧ COMB. CONC. CURB & GUTTER, TYPE B-6.18
  - ⑨ SUBBASE GRANULAR MATERIAL, TYPE B, 6"
  - ⑩ HMA SURFACE COURSE, MIX "D", N50, (IL 9.5 mm), 2"
  - ⑪ AGGREGATE BASE COURSE, TYPE B, 6"
  - ⑫ PCC SIDEWALK, 5"
  - ⑬ SODDING & TOPSOIL 4"
  - ⑭ PIPE UNDERDRAIN, 4"
  - ⑮ POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾"

NOTE: THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.

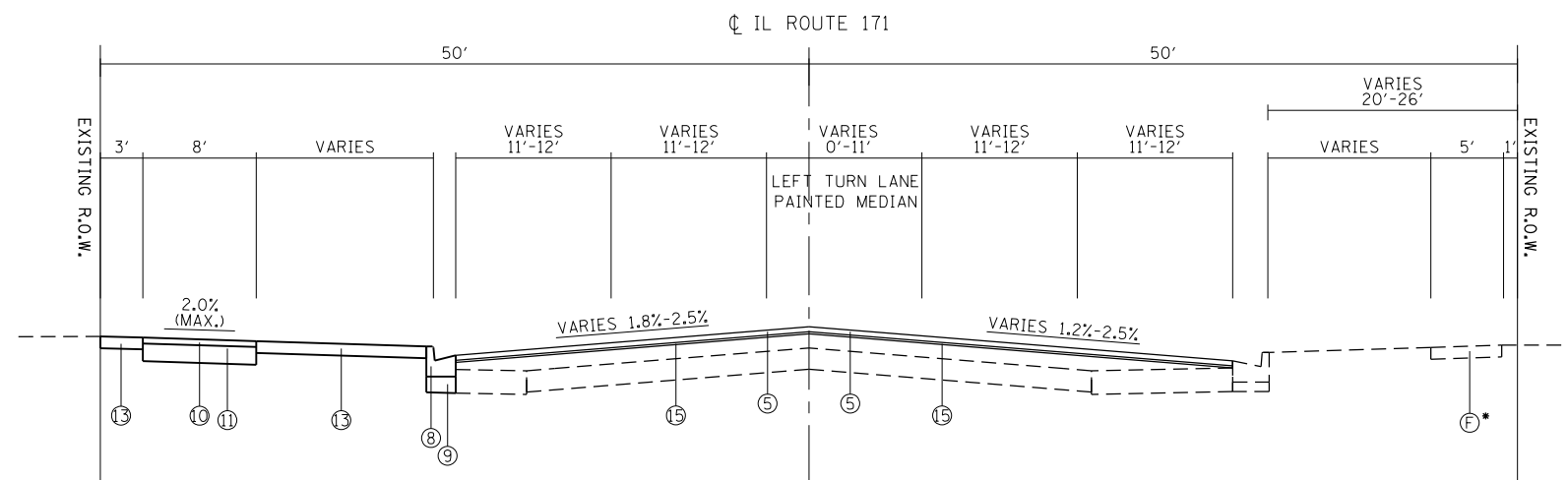
\* EXISTING PCC SIDEWALK FROM STA. 37+40 TO 41+30 ONLY



EXISTING TYPICAL SECTION

STA. 37+40 TO 42+00

STA. 45+47 TO 48+67



PROPOSED TYPICAL SECTION

STA. 37+40 TO 42+00

STA. 45+47 TO 48+67

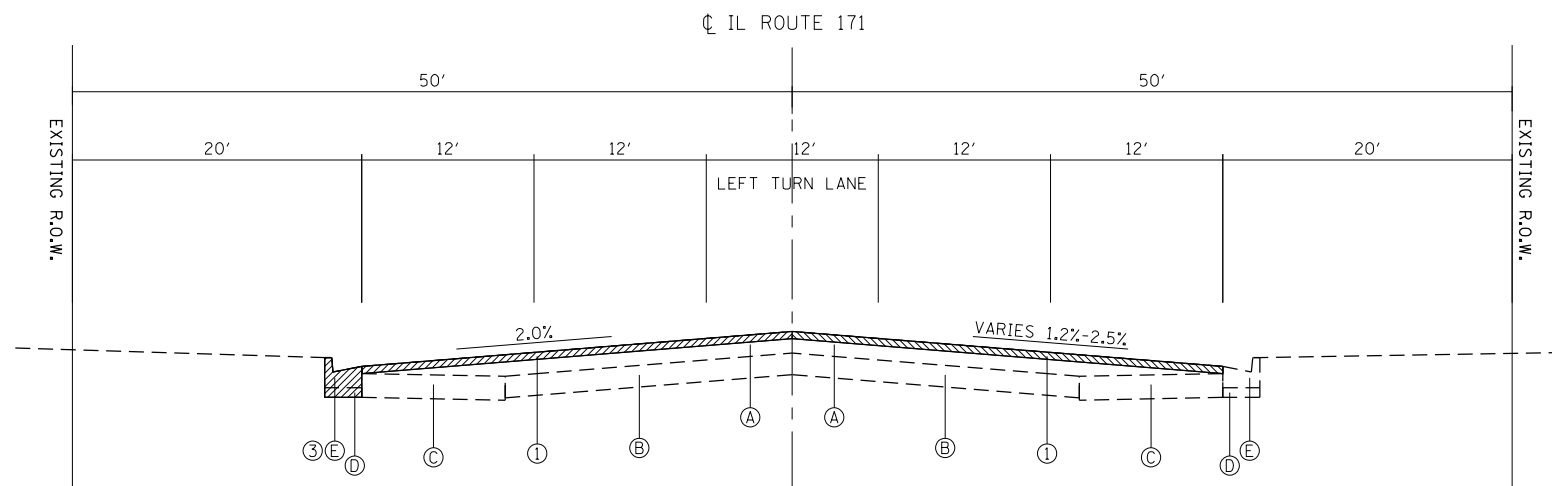
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	PLOT DATE = 3/31/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL ROUTE 171 AT FOREST AVE./RIDGWOOD RD.  
TYPICAL SECTIONS - IL ROUTE 171

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	17
CONTRACT NO. 60X08			ILLINOIS FED. AID PROJECT	



EXISTING TYPICAL SECTION  
STA. 42+00 TO 45+47

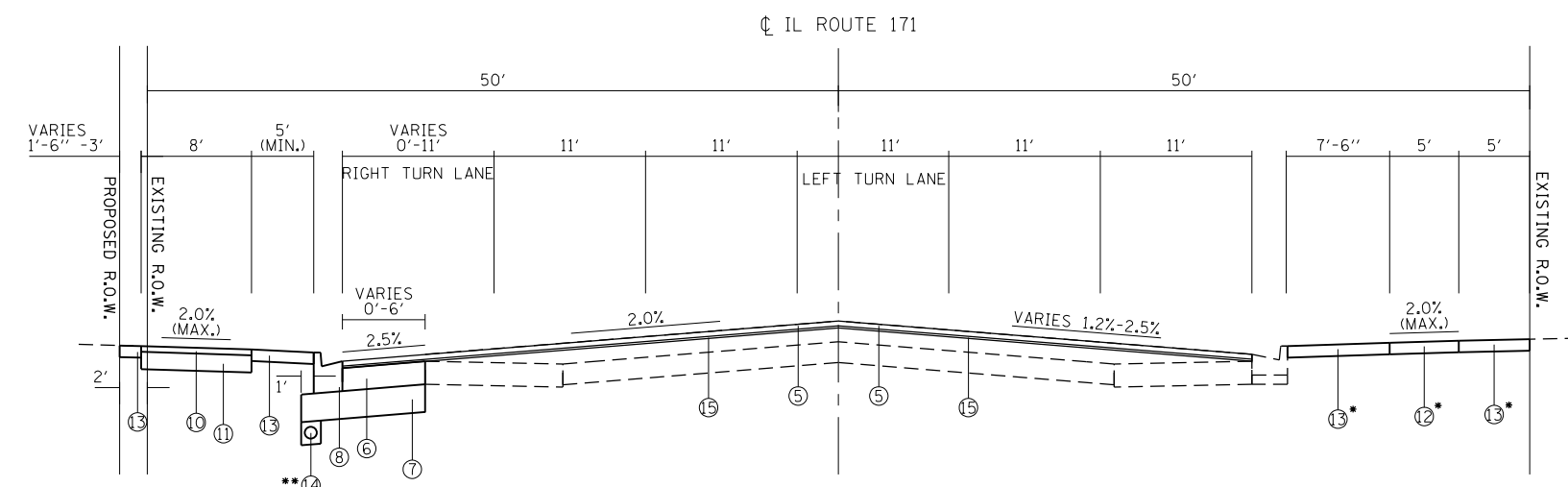
- EXISTING LEGEND:
- (A) HMA SURFACE & BINDER COURSE, ± 10"
  - (B) PCC PAVEMENT, ± 9"
  - (C) HMA BASE COURSE, ± 10"
  - (D) SUBBASE GRANULAR MATERIAL, 4"
  - (E) COMB. CONC. CURB & GUTTER, TYPE B-6.24
  - (F) PCC SIDEWALK

- PROPOSED LEGEND:
- (1) HMA SURFACE REMOVAL, 2 1/2"
  - (2) PAVEMENT REMOVAL
  - (3) COMB. CONC. CURB & GUTTER REMOVAL
  - (4) SIDEWALK REMOVAL
  - (5) POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1 3/4"
  - (6) HMA BASE COURSE WIDENING, 9 1/2"
  - (7) AGGREGATE SUBGRADE IMPROVEMENT, 12"
  - (8) COMB. CONC. CURB & GUTTER, TYPE B, 12"
  - (9) SUBBASE GRANULAR MATERIAL, TYPE B, 6"
  - (10) HMA SURFACE COURSE, MIX "D", N50, (IL 9.5 mm), 2"
  - (11) AGGREGATE BASE COURSE, TYPE B, 6"
  - (12) PCC SIDEWALK, 5"
  - (13) SODDING & TOPSOIL 4"
  - (14) PIPE UNDERDRAIN, 4"
  - (15) POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"

NOTE: THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.

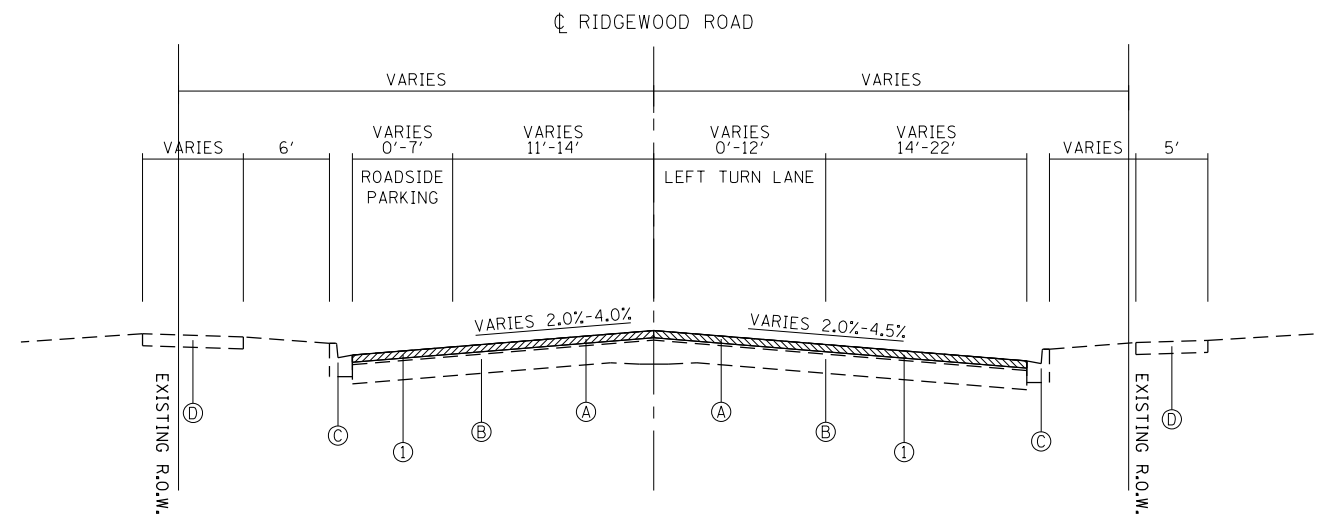
\* PCC SIDEWALK, 5" FROM STA. 42+37 TO 43+10 ONLY.  
SEE ROADWAY PLAN.

\*\* LONGITUDINAL PIPE UNDERDRAIN FROM STA. 42+50 TO STA. 44+50.



PROPOSED TYPICAL SECTION  
STA. 42+00 TO 45+47

FILE NAME =	USER NAME = tariqfm	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL. ROUTE 171 AT FOREST AVE./RIDGWOOD RD. TYPICAL SECTIONS - IL. ROUTE 171</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	2-sh-t-typical.dgn	DRAWN -	REVISED -			372	03035-TS	COOK	87	18	
	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60X08					
	PLOT DATE = 3/31/2014	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO
ILLINOIS FED. AID PROJECT											



EXISTING TYPICAL SECTION  
STA. 17+00 TO 19+25

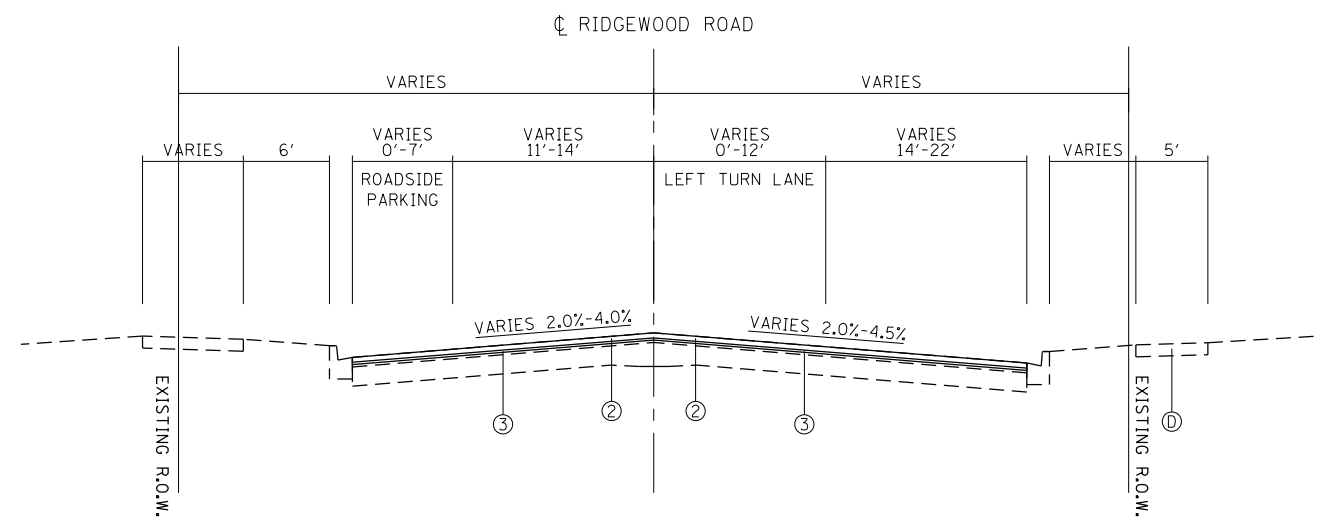
EXISTING LEGEND:

- Ⓐ HMA SURFACE & BINDER COURSE, ± 2½"
- Ⓑ PCC PAVEMENT, ± 8"
- Ⓒ COMB. CONC. CURB & GUTTER, TYPE B-6.12
- Ⓓ PCC SIDEWALK

PROPOSED LEGEND:

- ① HMA SURFACE REMOVAL, 2½"
- ② POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1¾"
- ③ POLYMERIZED LEVELING BINDER (MM), 1L-4.75, N50, ¾"

NOTE: THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.



PROPOSED TYPICAL SECTION  
STA. 17+00 TO 19+25

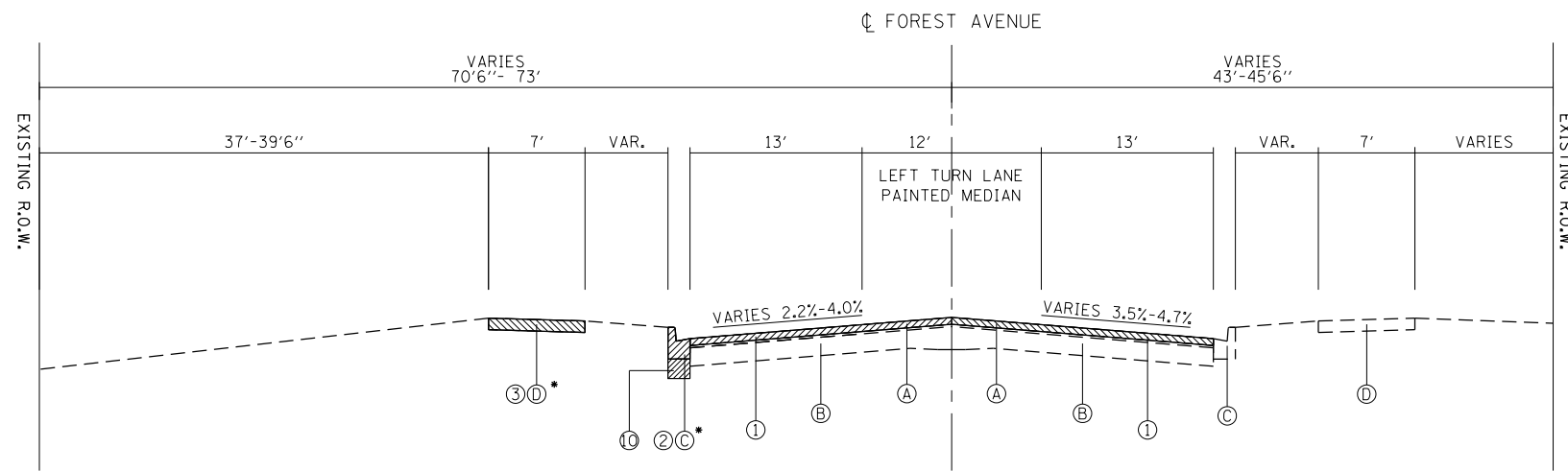
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	PLOT DATE = 3/31/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD.  
TYPICAL SECTIONS - RIDGEWOOD RD.

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	19
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60X08	



EXISTING TYPICAL SECTION  
STA. 20+54 TO 23+91

EXISTING LEGEND:

- Ⓐ HMA SURFACE & BINDER COURSE, ± 2½"
- Ⓑ PCC PAVEMENT, ± 8"
- Ⓒ COMB. CONC. CURB & GUTTER, TYPE B-6.12
- Ⓓ PCC SIDEWALK

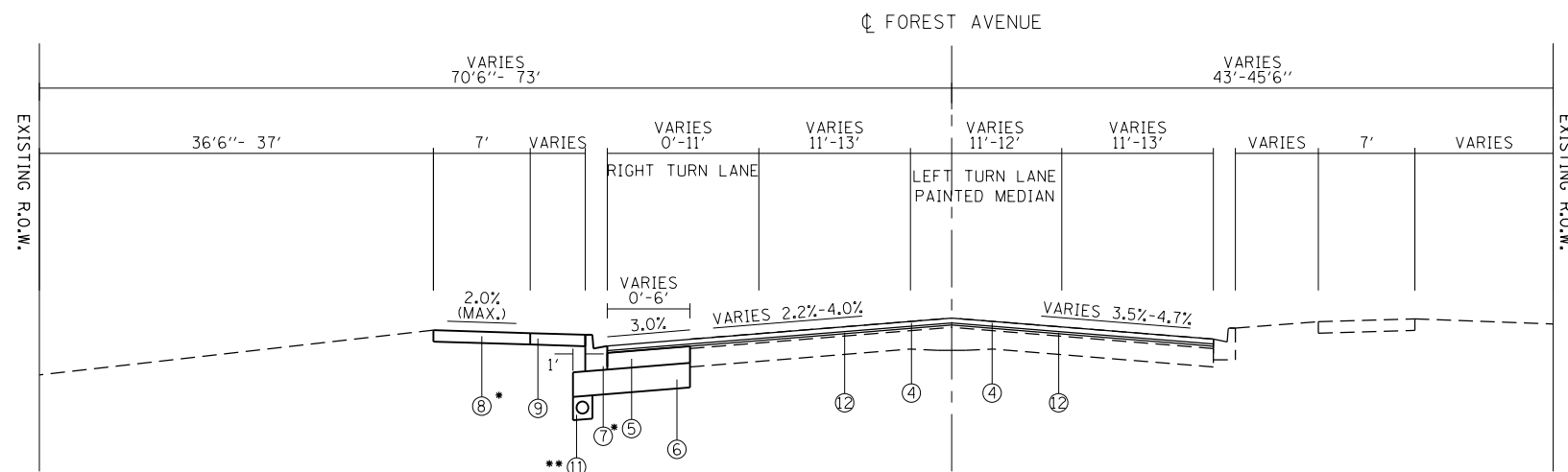
PROPOSED LEGEND:

- ① HMA SURFACE REMOVAL, 2½"
- ② COMB. CONC. CURB & GUTTER REMOVAL
- ③ SIDEWALK REMOVAL
- ④ POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1¾"
- ⑤ HMA BASE COURSE WIDENING, 5"
- ⑥ AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ⑦ COMB. CONC. CURB & GUTTER, TYPE B-6.12
- ⑧ PCC SIDEWALK, 5"
- ⑨ SODDING & TOPSOIL, 4"
- ⑩ 6" UNDERCUT (STA. 21+00 TO STA. 22+00, WIDTH OF WIDENING)
- ⑪ PIPE UNDERDRAIN, 4"
- ⑫ POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾"

NOTE: THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.

\* PCC SIDEWALK REMOVAL AND PROPOSED PCC SIDEWALK, 5" FROM STA. 20+54 TO 22+06 ONLY.  
CURB & GUTTER REMOVAL AND PROPOSED COMB. CURB & GUTTER, TYPE B6.12 FROM 20+54 TO 22+22 ONLY.

\*\* LONGITUDINAL PIPE UNDERDRAIN FROM STA. 20+50 TO STA. 22+00.



PROPOSED TYPICAL SECTION  
STA. 20+54 TO 23+91

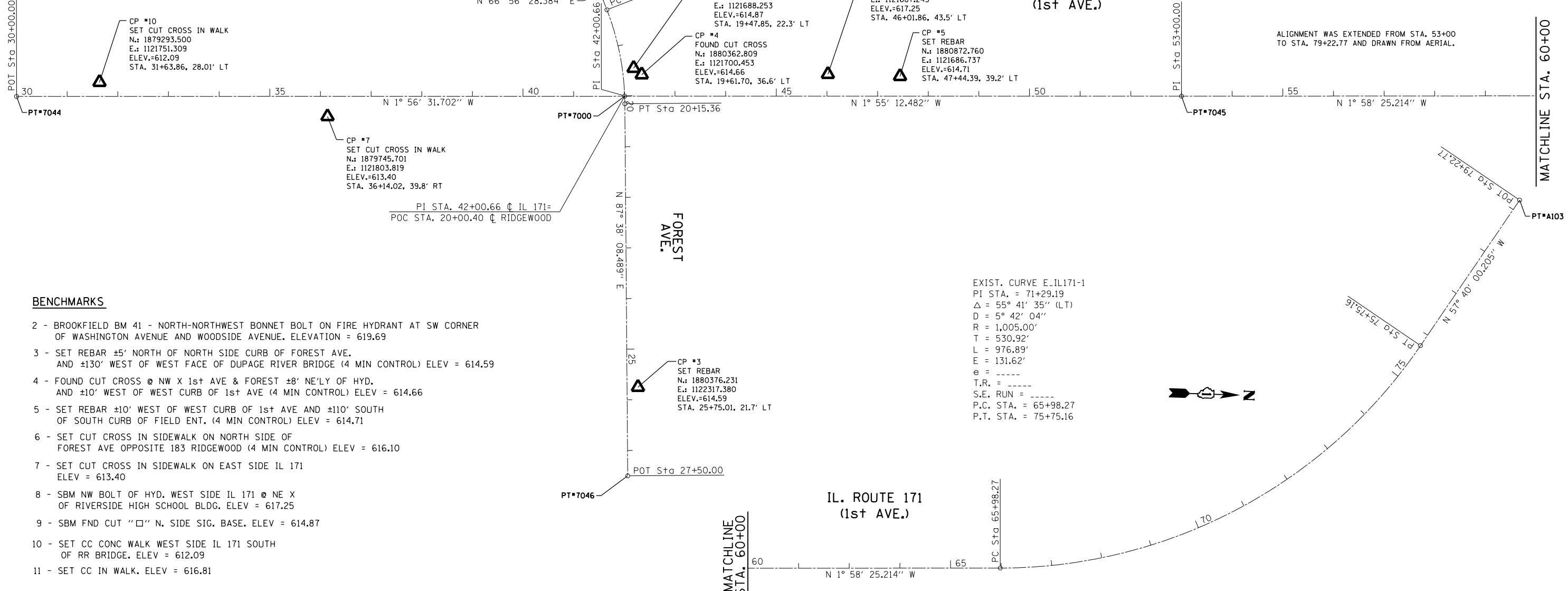
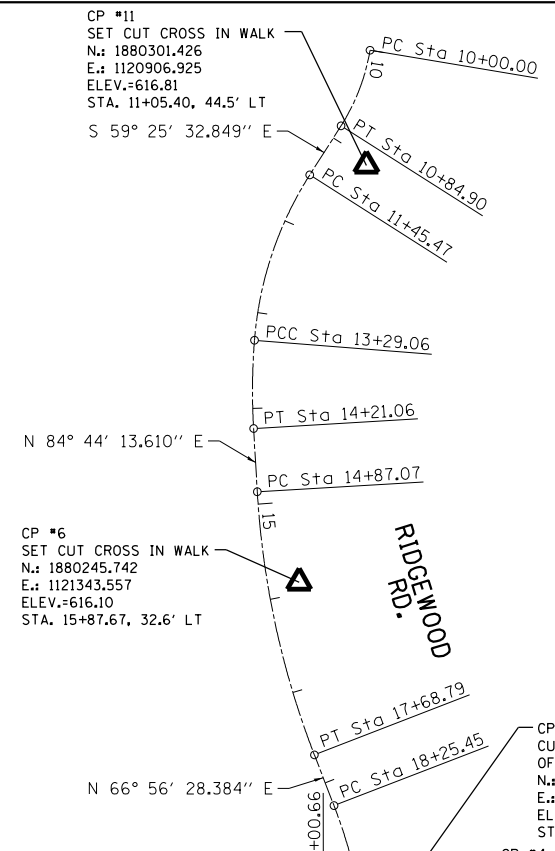
FILE NAME =	USER NAME = Tariqfm	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD. TYPICAL SECTIONS - FOREST AVE.</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	2-sht-typical.dgn	DRAWN -	REVISED -			372	03035-TS	COOK	87	20	
	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60X08					
	PLOT DATE = 3/31/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	

EARTHWORK						
1	2	3	4	5	6	7
IL. ROUTE 171 AT FOREST AVE. / RIDGEWOOD RD.	EARTH EXCAVATION (CU YD)	EMBANKMENT (CU YD)	ADJUSTMENT FOR SHRINKAGE (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)	UNSUITABLE MATERIAL (CU YD)	TOPSOIL EXCAVATION AND PLACEMENT (CU YD)
IL. ROUTE 171 (SOUTH LEG) - STA. 35+00 TO STA. 42+00	17	211	15	-196	96	69
IL. ROUTE 171 (NORTH LEG) - STA. 42+00 TO STA. 48+67	88	284	75	-209	302	102
FOREST AVE. - STA. 20+29 TO STA. 23+91	58	14	49	35	63	15
TOTAL	163	509	139	-370	461	186
<p>COLUMN 1: LOCATION FROM PLANS            COLUMN 2: CUT QUANTITIES FROM CROSS SECTIONS, WHICH DOES NOT INCLUDE UNSUITABLE MATERIAL            COLUMN 3: QUANTITIES FROM CROSS SECTIONS (FILL)            COLUMN 4: EARTH EXCAVATION THAT IS TO BE USED AS FILL MATERIAL IN THE EMBANKMENT, SHRINKAGE FACTOR IS 15%</p> <p>COLUMN 5: COLUMN 4 - COLUMN 3            POSITIVE QUANTITY = EXTRA EXCAVATION            NEGATIVE QUANTITY = FURNISHED EXCAVATION NEEDED            COLUMN 6: CUT MATERIAL THAT IS DETERMINED TO BE EITHER UNSTABLE OR UNSUITABLE FOR USE IN EMBANKMENT            COLUMN 7: TOPSOIL EXCAVATION AND PLACEMENT = AREA OF SOD AND TOPSOIL</p>						

NOTES:

- TOPSOIL SHALL BE EXCAVATED TO A DEPTH OF 12" THROUGHOUT THE PROJECT LIMITS.
- EXCAVATED TOPSOIL REQUIRED AT LOCATIONS OF NEW SOD AS SHOWN ON THE LANDSCAPING PLAN SHALL BE PLACED AT A DEPTH OF 4" AND PAID FOR AS TOPSOIL EXCAVATION AND PLACEMENT.
- EXCAVATED TOPSOIL NOT REQUIRED ON THE PROJECT SHALL BE CONSIDERED UNSUITABLE MATERIAL AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL.

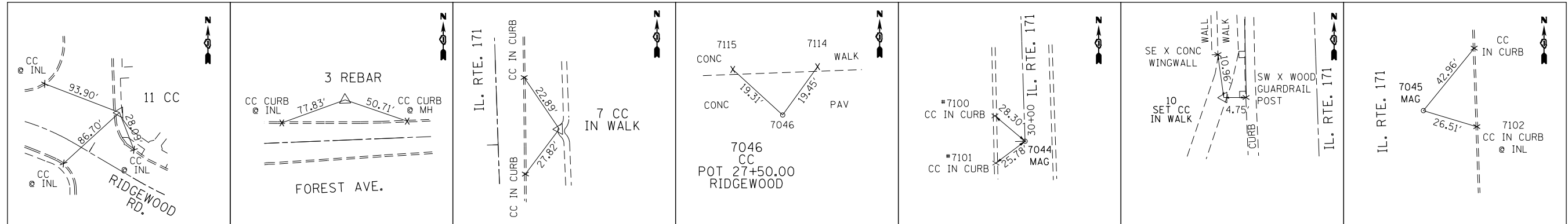
IL. 171	PT* 7044	N 1879130.6816	E 1121784.8522	STA. 30+00.00
	PT* 7000	N 1880330.6563	E 1121744.1614	STA. 42+00.664
	PT* 7045	N 1881429.3757	E 1121707.3266	STA. 53+00.001
	PC CURVE 1	N 1882726.8770	E 1121662.6138	STA. 65+98.273
	PI CURVE 1	N 1883257.4819	E 1121644.3288	STA. 71+29.192
	PT CURVE 1	N 1883541.4407	E 1121195.7274	STA. 75+75.160
	PT* A103	N 1883727.3582	E 1120902.0128	STA. 79+22.771
	PC CURVE 1	N 1880301.0863	E 1120786.9535	STA. 10+00
	PI CURVE 1	N 1880295.4083	E 1120829.6085	STA. 10+43.031
	PT CURVE 1	N 1880273.5203	E 1120866.6571	STA. 10+84.904
FOREST AVE./ RIDGEWOOD RD.	PC CURVE 2	N 1880242.7141	E 1120918.8011	STA. 11+45.469
	PI CURVE 2	N 1880195.0699	E 1120999.4458	STA. 12+39.136
	PT CURVE 2	N 1880190.8708	E 1121093.0188	STA. 13+29.058
	PC CURVE 3	N 1880190.8708	E 1121093.0188	STA. 13+29.058
	PI CURVE 3	N 1880188.8053	E 1121139.0454	STA. 13+75.131
	PT CURVE 3	N 1880193.0314	E 1121184.9241	STA. 14+21.061
	PC CURVE 4	N 1880199.0863	E 1121250.6567	STA. 14+87.072
	PI CURVE 4	N 1880212.1113	E 1121392.0583	STA. 16+29.072
	PT CURVE 4	N 1880267.7293	E 1121522.7131	STA. 17+68.785
	PC CURVE 5	N 1880289.9251	E 1121574.8545	STA. 18+25.454
	PI CURVE 5	N 1880327.5260	E 1121663.1844	STA. 19+21.454
	PT CURVE 5	N 1880331.4863	E 1121759.1028	STA. 20+15.362
	PT* 7046	N 1880361.7924	E 1122493.1126	STA. 27+49.998



**BENCHMARKS**

- BROOKFIELD BM 41 - NORTH-NORTHWEST BONNET BOLT ON FIRE HYDRANT AT SW CORNER OF WASHINGTON AVENUE AND WOODSIDE AVENUE. ELEVATION = 619.69
- SET REBAR ±5' NORTH OF NORTH SIDE CURB OF FOREST AVE. AND ±130' WEST OF WEST FACE OF DUPAGE RIVER BRIDGE (4 MIN CONTROL) ELEV = 614.59
- FOUND CUT CROSS @ NW X 1st AVE & FOREST ±8' NE'LY OF HYD. AND ±10' WEST OF WEST CURB OF 1st AVE (4 MIN CONTROL) ELEV = 614.66
- SET REBAR ±10' WEST OF WEST CURB OF 1st AVE AND ±110' SOUTH OF SOUTH CURB OF FIELD ENT. (4 MIN CONTROL) ELEV = 614.71
- SET CUT CROSS IN SIDEWALK ON NORTH SIDE OF FOREST AVE OPPOSITE 183 RIDGEWOOD (4 MIN CONTROL) ELEV = 616.10
- SET CUT CROSS IN SIDEWALK ON EAST SIDE IL 171 ELEV = 613.40
- SBM NW BOLT OF HYD. WEST SIDE IL 171 @ NE X OF RIVERSIDE HIGH SCHOOL BLDG. ELEV = 617.25
- SBM FND CUT "□" N. SIDE SIG. BASE. ELEV = 614.87
- SET CC CONC WALK WEST SIDE IL 171 SOUTH OF RR BRIDGE. ELEV = 612.09
- SET CC IN WALK. ELEV = 616.81

FILE NAME =	USER NAME = tariqfm	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD. ALIGNMENT, TIES AND BENCHMARKS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	2-sht-ATB.dgn	DRAWN - D.B.	REVISED -			372	03035-TS	COOK	87	22	
	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -			<b>CONTRACT NO. 60X08</b>					
	PLOT DATE = 4/2/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



**POINT #11**

SET CC IN WALK  
 N 1880301.4260  
 E 1120906.9250

**POINT #3**

REBAR  
 N 1880376.2310  
 E 1122317.3800

**POINT #4**

CC IN WALK  
 N 1880362.8090  
 E 1121700.4530

**POINT #7046**

POT 27+50.00  
 RIDGEWOOD  
 N 1880361.7924  
 E 1122493.1126

**POINT #7044**

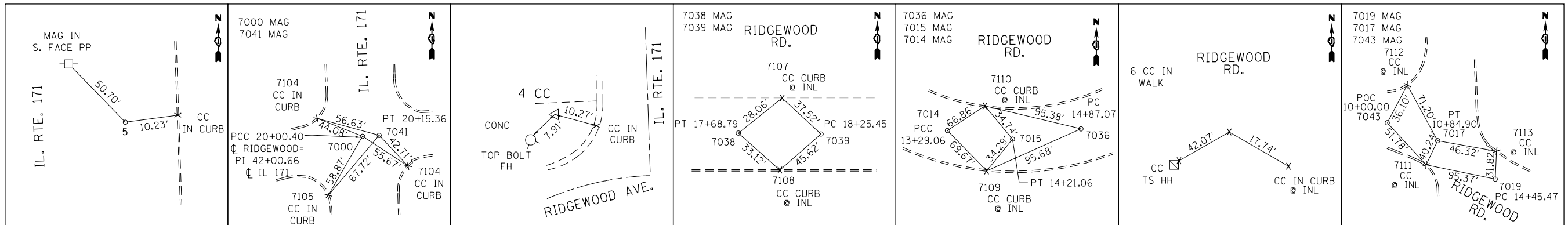
SET MAG IN  
 CL POT 30+00 CL IL 171  
 N 1879130.6816  
 E 1121784.8522

**POINT #10**

SET CC IN WALK  
 N 1879293.5000  
 E 1121751.3090

**POINT #7045**

SET MAG IN  
 CL POT 53+00 CL IL 171  
 N 1881429.3757  
 E 1121707.3266



**POINT #5**

REBAR  
 N 1880872.7600  
 E 1121686.7370

**POINT #7000**

SET MAG IN  
 POC 20+00.40 CL RIDGEWOOD=  
 PI 42+00.66 CL IL 171  
 N 1880330.6563  
 E 1121744.1614

**POINT #7041**

SET MAG IN  
 PT 20+15.36 CL FOREST/  
 RIDGEWOOD=  
 N 1880331.4863  
 E 1121759.1028

**CONTROL POINT #4**

CC IN WALK  
 N 1880362.8090  
 E 1121700.4530

**POINT #7038**

SET MAG IN  
 PT 17+68.785  
 RIDGEWOOD  
 N 1880267.7293  
 E 1121522.7131

**POINT #7039**

SET MAG IN  
 PC 18+25.454  
 RIDGEWOOD  
 N 1880289.9252  
 E 1121574.8546

**POINT #7014**

SET MAG IN  
 PCC 13+29.06 CL RIDGEWOOD  
 N 1880190.8708  
 E 1121093.0188

**POINT #7015**

SET MAG IN  
 PT 14+21.06 CL RIDGEWOOD  
 N 1880193.0314  
 E 1121184.9243

**POINT #7036**

SET MAG IN  
 PC 14+87.07 CL RIDGEWOOD  
 N 1880199.0863  
 E 1121250.6569

**POINT #6**

SET CC IN WALK  
 N 1880245.7420  
 E 1121343.5570

**POINT #7019**

SET MAG IN  
 PC 11+45.47 CL RIDGEWOOD  
 N 1880242.7140  
 E 1121093.0188

**POINT #7017**

SET MAG IN  
 PT 10+84.90 CL RIDGEWOOD  
 N 1880273.5204  
 E 1120866.6570

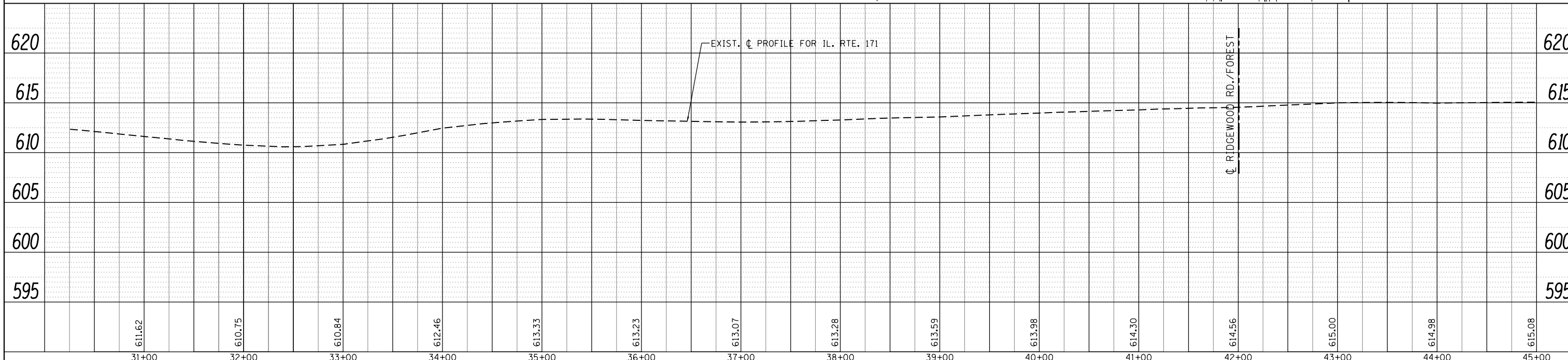
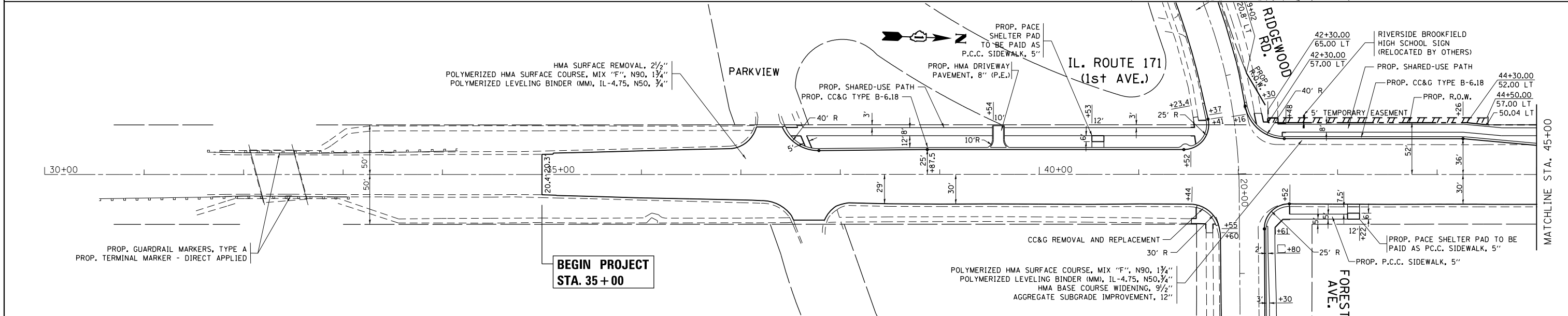
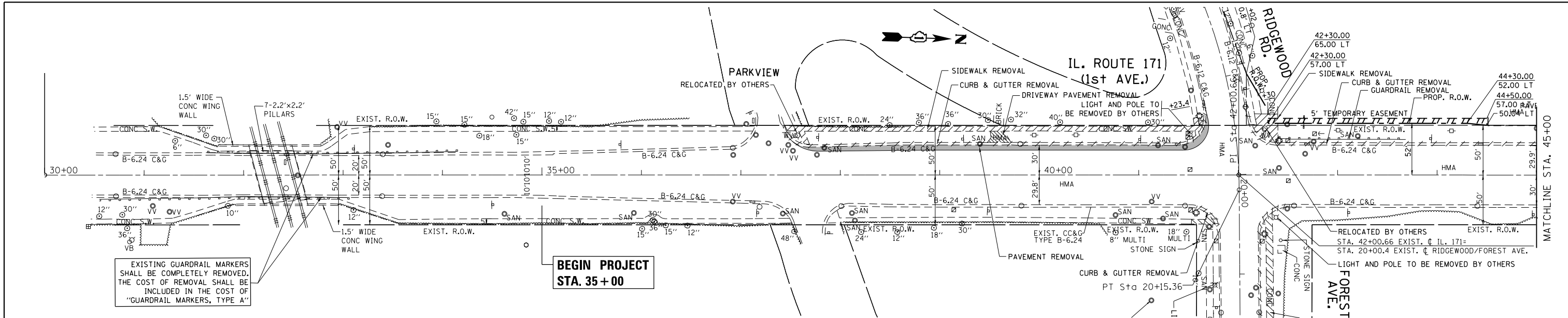
**POINT #7043**

SET MAG IN  
 POC 10+00 CL RIDGEWOOD  
 N 1880301.0863  
 E 1120786.9535

FILE NAME =	USER NAME = Tariqfm	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD. TIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 4/2/2014	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	ALIGNED	
	CHECKED	
	FILED	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES	
	CHECKED	
	STRUCTURE	
	NOTATIONS	
	CHKD	
	NO.	

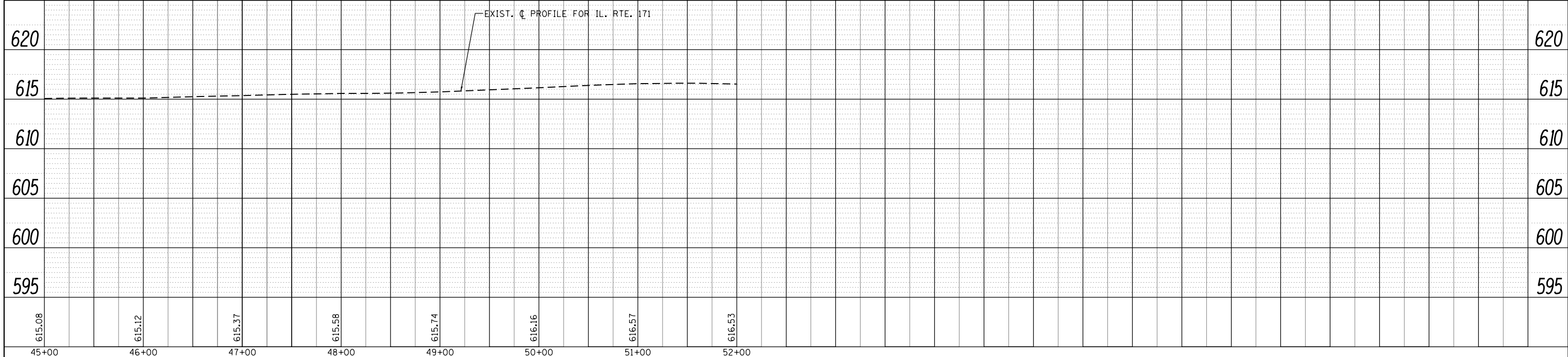
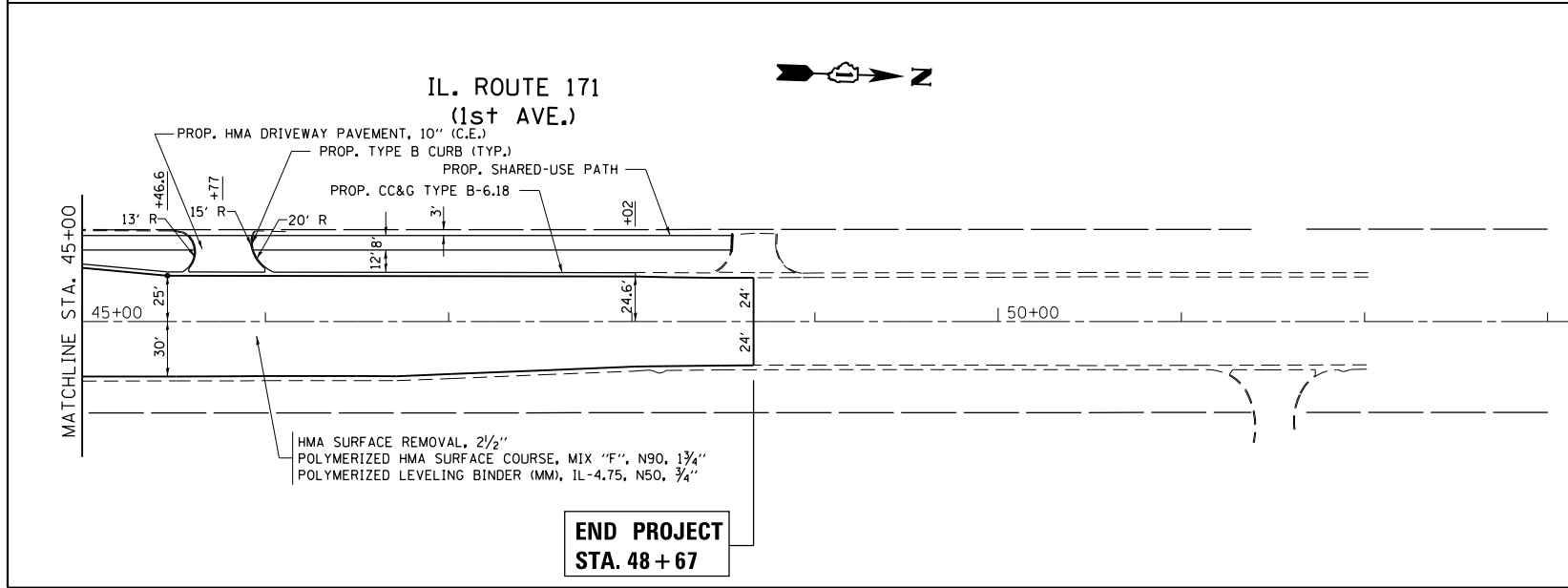
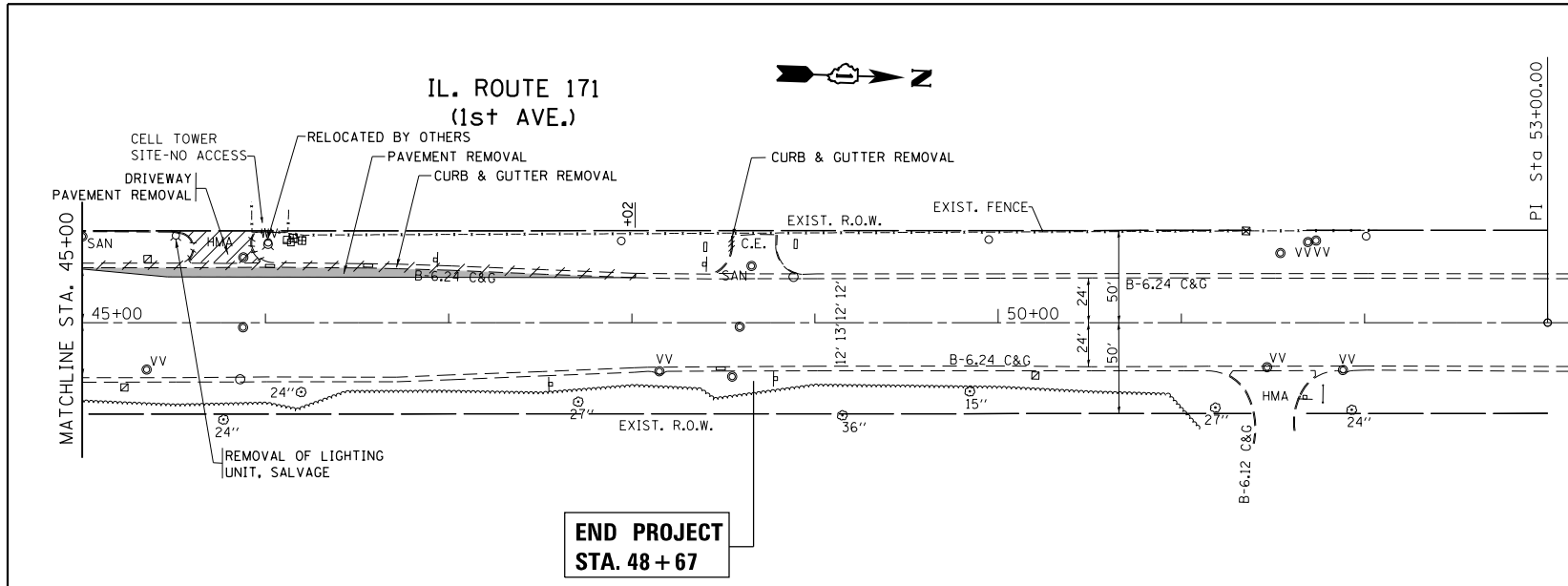


FILE NAME =	USER NAME =	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD.</b> <b>ROADWAY PLAN AND PROFILE</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 4/2/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT				



PLAN	SURVEYED	DATE
	PLOTTED	
	ALIGNED	
	CHECKED	
	FILE NAME	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE	
	NOTATIONS CHECKED	
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	PLOT DATE = 4/2/2014		

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

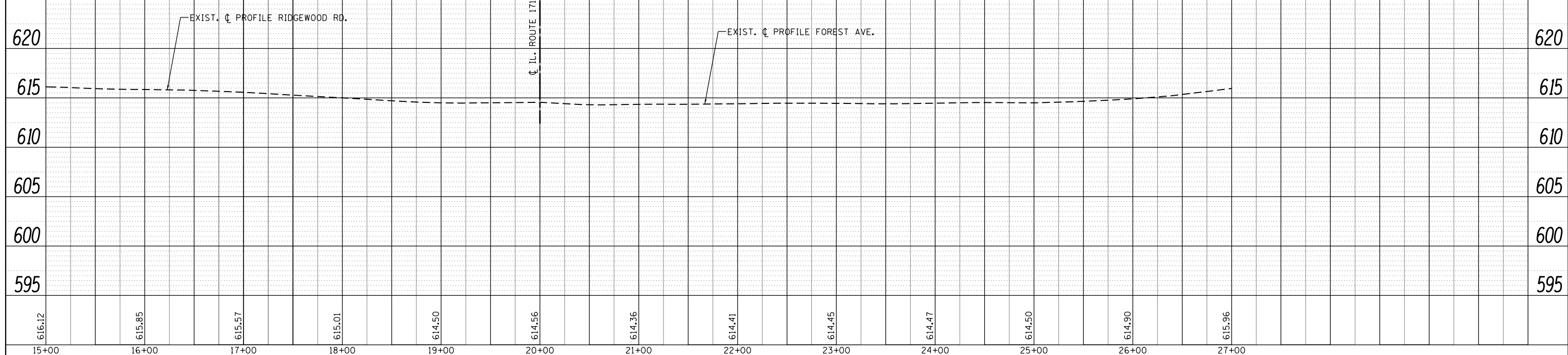
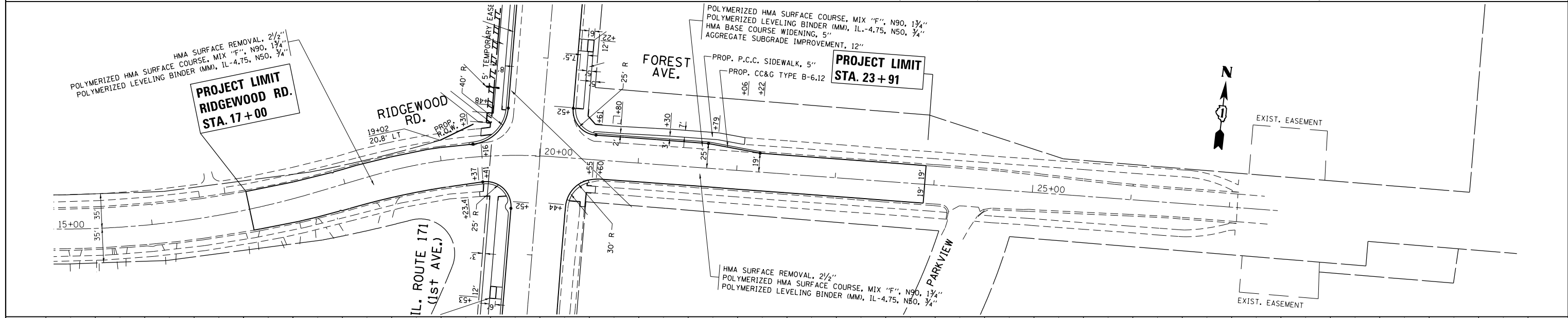
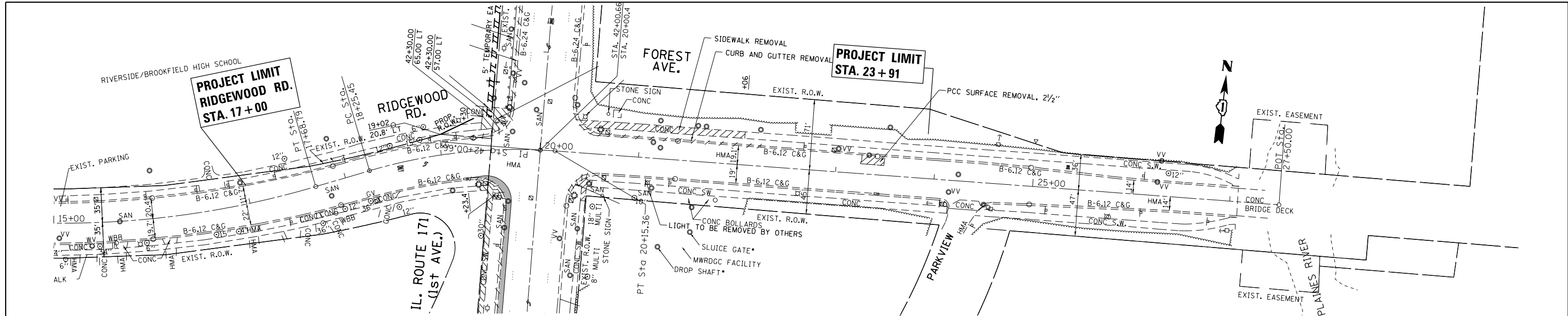
IL. ROUTE 171 AT FOREST AVE./RIDGWOOD RD.  
ROADWAY PLAN AND PROFILE

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

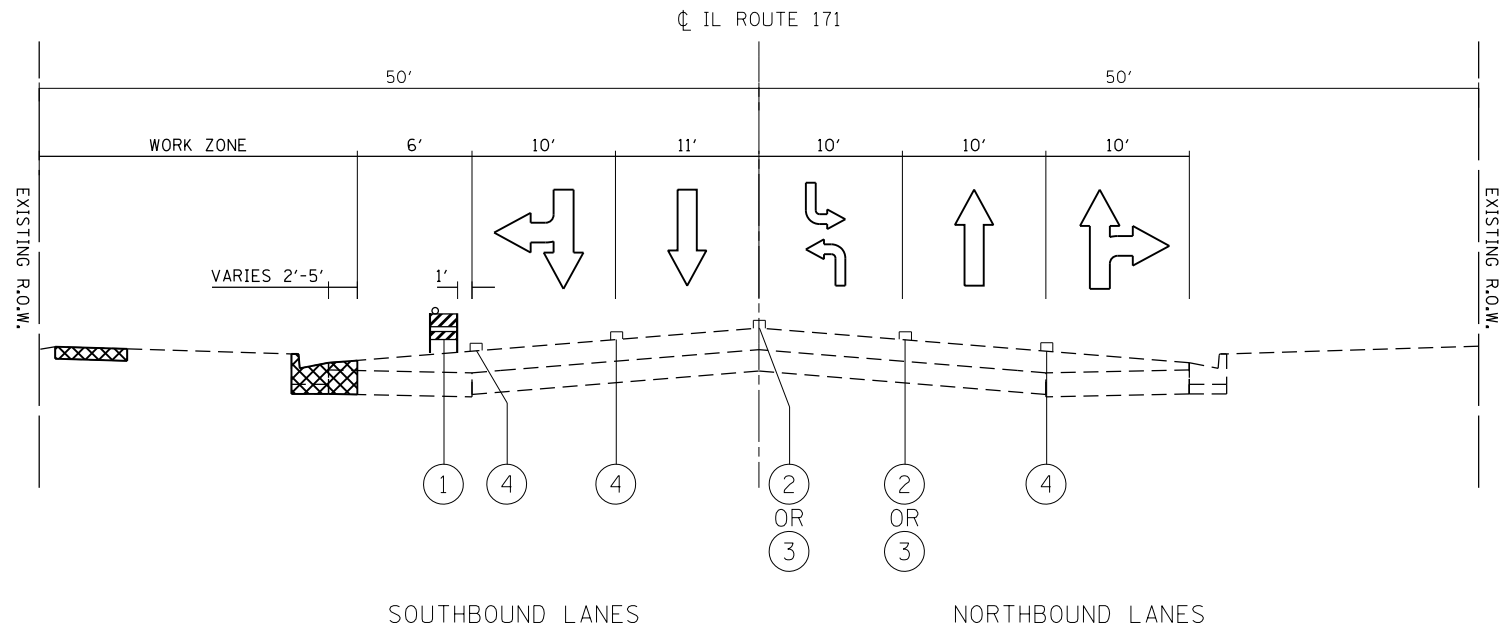
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372	0303S-TS	COOK	87	25
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
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	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTE BOOK	
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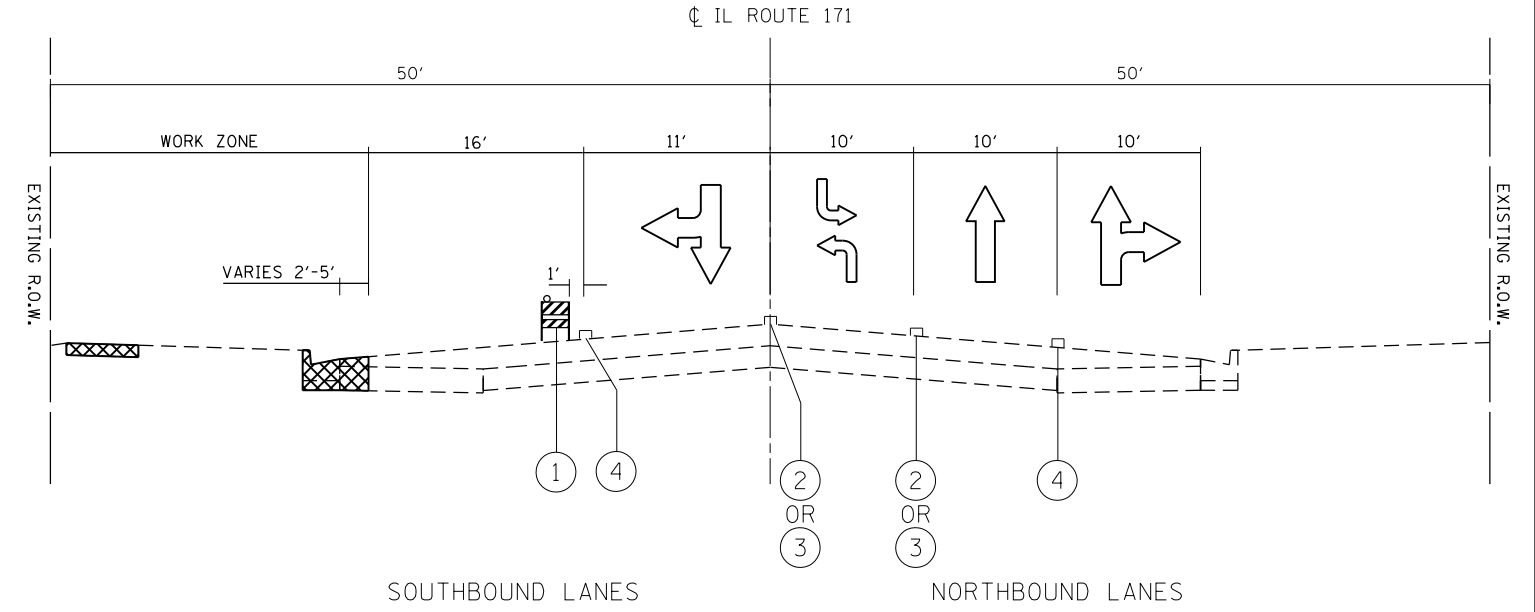
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	PLOT DATE = 4/2/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT				



STAGE 1A - PAVEMENT, CC&G, SIDEWALK REMOVAL & DRAINAGE DETENTION SYSTEM INSTALLATION

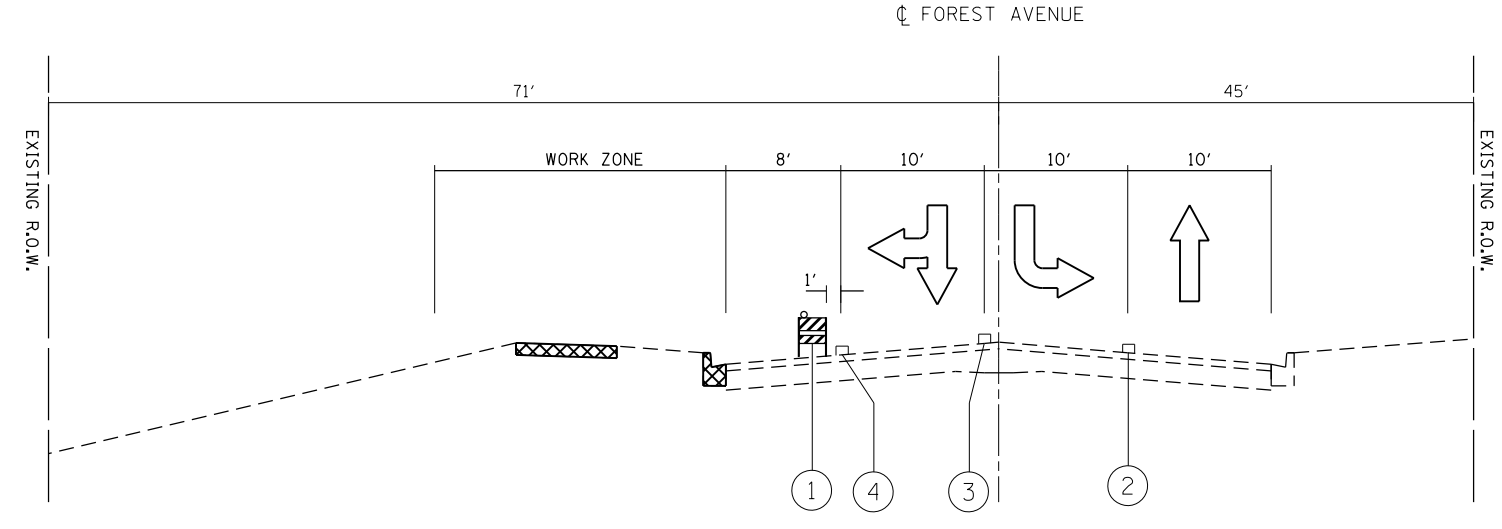
STA. 37+40 TO STA. 48+67  
PEAK HOURS

- PRE-STAGE:
- PAVEMENT MARKING REMOVAL, PLACEMENT OF TEMPORARY PAVEMENT MARKING AND UTILITY RELOCATION
  - EXISTING TRAFFIC SIGNAL MAST ARM AND POST LOCATED IN THE NORTHWEST QUADRANT OF THE INTERSECTION WHERE THE SOUTHBOUND RIGHT TURN LANE IS PROPOSED WILL NEED TO BE REMOVED
- STAGE 1A:
- PAVEMENT, COMBINATION CURB AND GUTTER, AND SIDEWALK REMOVAL ALONG THE WEST SIDE OF IL ROUTE 171 AND NORTH SIDE OF FOREST AVENUE
  - INSTALLATION OF THE PROPOSED DRAINAGE DETENTION SYSTEM AND ASSOCIATED PAVEMENT PATCHING (CLASS D PATCHES, 17") UTILIZING A DAILY OFF-PEAK LANE CLOSURE
  - MAINTAIN LANES IN THE EXISTING CONFIGURATION WITH WIDTHS REDUCED TO 10' FOR TURNING LANES AND 10' TO 11' FOR THRU LANES UTILIZING HIGHWAY STANDARD 701701 "SHOULDER OPERATIONS" DURING PEAK HOURS.
  - SOUTHBOUND IL ROUTE 171 WILL BE REDUCED TO ONE 11' THRU LANE AND ONE 10' LEFT TURN LANE UTILIZING HIGHWAY STANDARD 701701 "CORNER ISLAND OPERATIONS" DURING OFF-PEAK HOURS
  - DROP-OFF CONDITIONS BETWEEN TRAVELED WAY AND WORK ZONE DICTATE THAT CHANNELIZING DEVICES BE USED AT 50' INTERVALS AND 20' INTERVALS AT RADII
  - UTILIZE HIGHWAY STANDARD 701602 FOR LANE SHIFTS



STAGE 1A - PAVEMENT, CC&G, SIDEWALK REMOVAL & DRAINAGE DETENTION SYSTEM INSTALLATION

STA. 37+40 TO STA. 48+67  
OFF-PEAK HOURS



STAGE 1A - PAVEMENT, CC&G & SIDEWALK REMOVAL

STA. 20+50 TO STA. 23+91

**LEGEND**

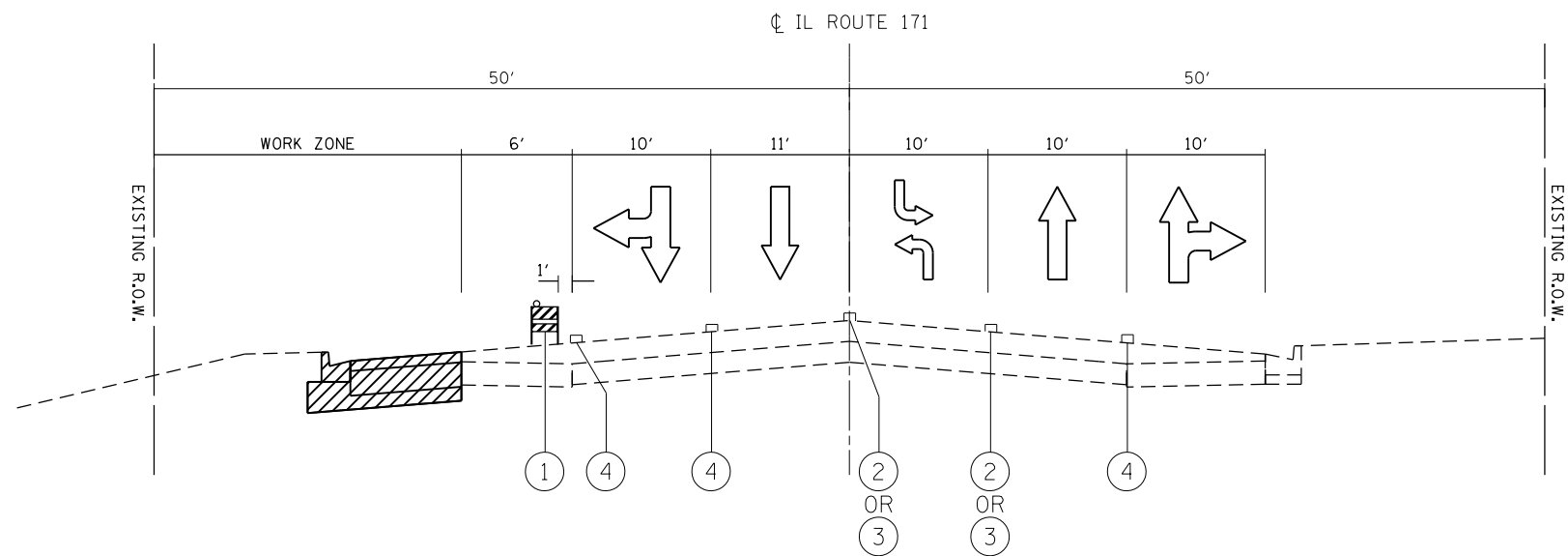
- ① TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE
- ② PAVEMENT MARKING TAPE, TYPE III, 4" (YELLOW)
- ③ PAVEMENT MARKING TAPE, TYPE III, 6" (WHITE)
- ④ PAVEMENT MARKING TAPE, TYPE III, 4" (WHITE)
- ← DIRECTION OF TRAFFIC
- ▣ REMOVAL ITEMS
- ▨ CONSTRUCTION ITEMS

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>IL ROUTE 171 AT FOREST AVE/RIDGEWOOD RD.</b>			
<b>SUGGESTED STAGING TYPICAL SECTIONS - STAGE 1A</b>			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	27
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				



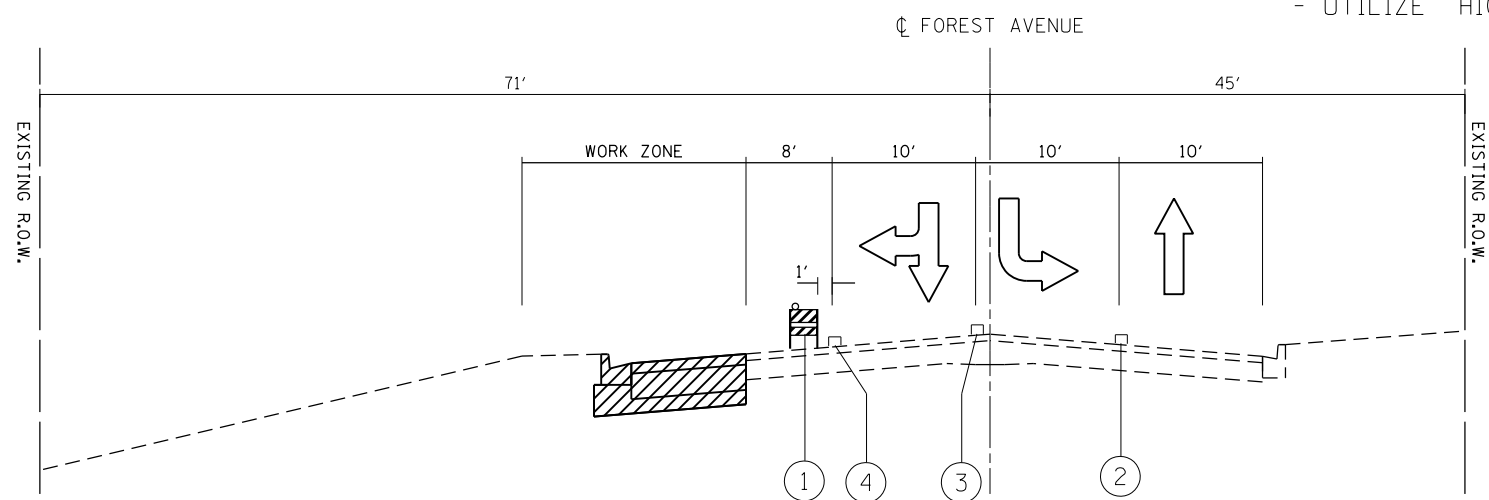
SOUTHBOUND LANES NORTHBOUND LANES

STAGE 1B - PAVEMENT WIDENING & CC&G INSTALLATION

STA. 37+40 TO STA. 48+67

STAGE 1B:

- PAVEMENT WIDENING AND COMBINATION CURB AND GUTTER INSTALLATION
- ALL HOT-MIX ASPHALT SHALL BE INSTALLED TO THE BINDER ONLY; THE FINAL SURFACE COURSE AND LEVELING BINDER IS TO BE INSTALLED IN POST-STAGE
- MAINTAIN LANES IN THE EXISTING CONFIGURATION WITH WIDTHS REDUCED TO 10' TO 11' FOR THRU LANES AND 10' FOR TURNING LANES UTILIZING HIGHWAY STANDARD 701701 "SHOULDER OPERATIONS"
- DROP-OFF CONDITIONS BETWEEN TRAVELED WAY AND WORK ZONE DICTATE THAT CHANNELIZING DEVICES BE USED AT 50' INTERVALS AND 20' INTERVALS AT RADII AND LANE CLOSURE TAPERS
- UTILIZE HIGHWAY STANDARD 701602 FOR LANE SHIFTS



WESTBOUND LANES EASTBOUND LANES

STAGE 1B - PAVEMENT WIDENING & CC&G INSTALLATION

STA. 20+50 TO STA. 23+91

**LEGEND**

- ① TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE
- ② PAVEMENT MARKING TAPE, TYPE III, 4" (YELLOW)
- ③ PAVEMENT MARKING TAPE, TYPE III, 6" (WHITE)
- ④ PAVEMENT MARKING TAPE, TYPE III, 4" (WHITE)
- ← DIRECTION OF TRAFFIC
- ▨ REMOVAL ITEMS
- ▩ CONSTRUCTION ITEMS

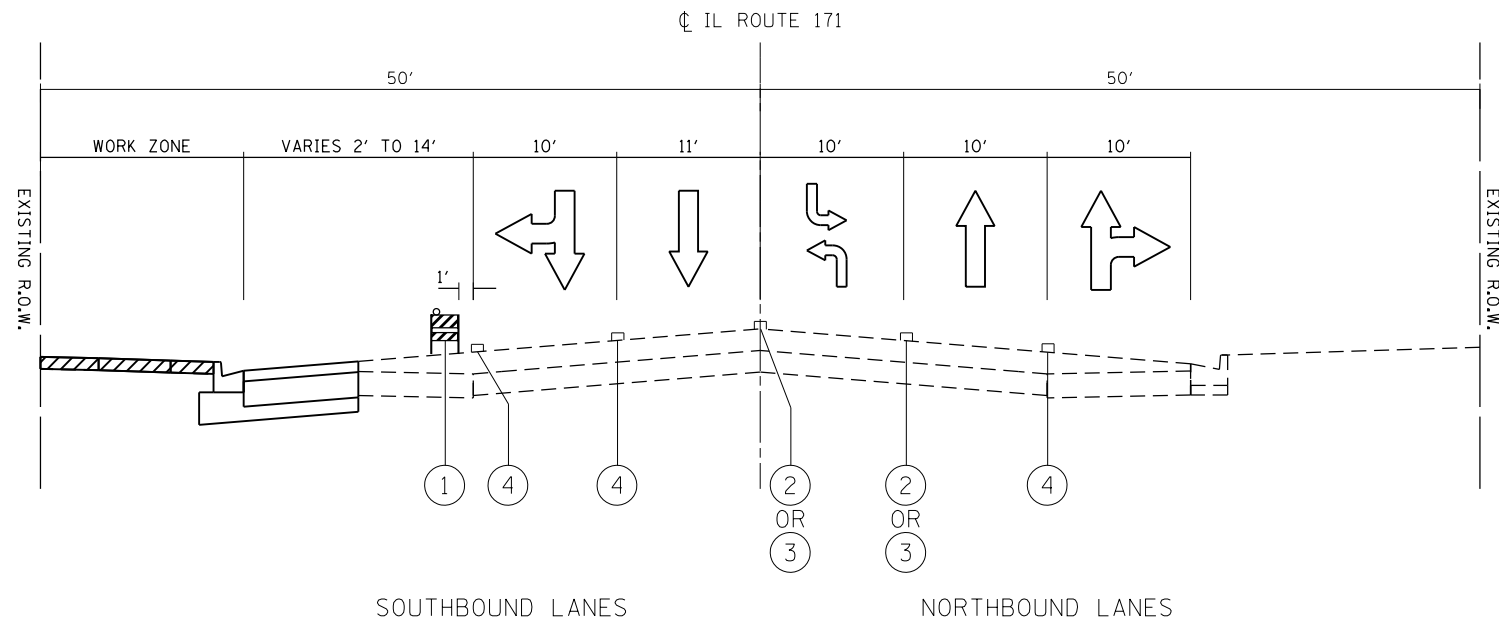
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 171 AT FOREST AVE/RIDGEWOOD RD.  
SUGGESTED STAGING TYPICAL SECTIONS - STAGE 1B**

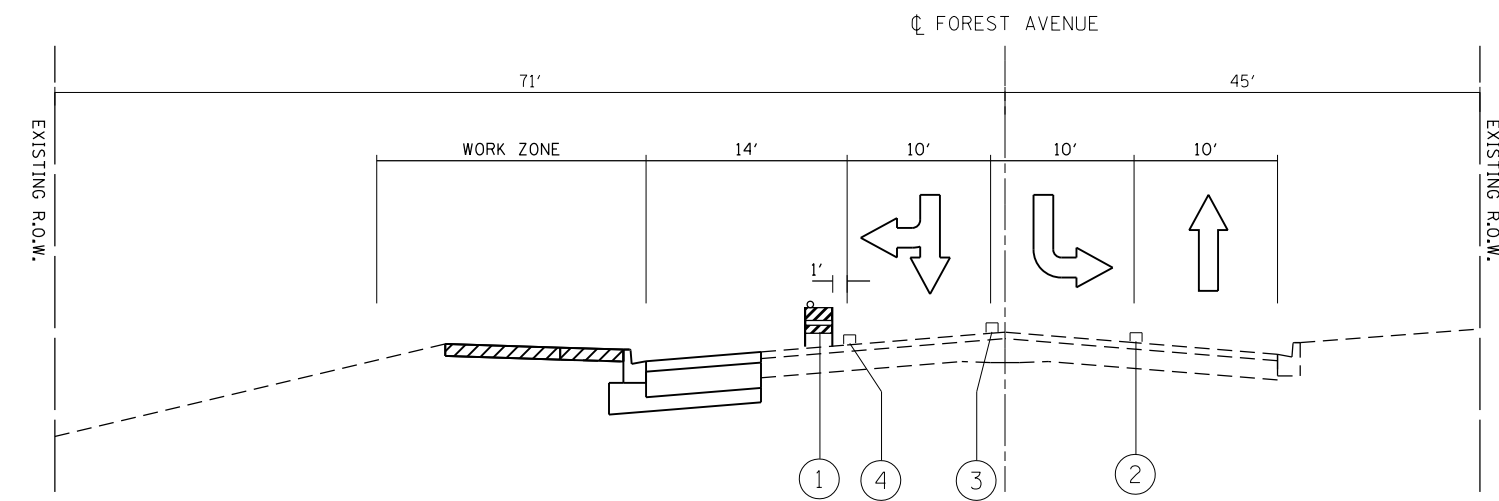
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	28
CONTRACT NO. 60X08			ILLINOIS FED. AID PROJECT	



SOUTHBOUND LANES NORTHBOUND LANES

STAGE 1C - SIDEWALK INSTALLATION & GRADING  
STA. 37+40 TO STA. 48+67



WESTBOUND LANES EASTBOUND LANES

STAGE 1C - SIDEWALK INSTALLATION & GRADING  
STA. 20+50 TO STA. 23+91

STAGE 1C:

- INSTALLATION OF SIDEWALK/SHARED-USE PATHS AND TRAFFIC SIGNALS
- MAINTAIN LANES IN THE EXISTING CONFIGURATION WITH WIDTHS REDUCED TO 10' TO 11' FOR THRU LANES AND 10' FOR TURNING LANES UTILIZING HIGHWAY STANDARD 701701 "SHOULDER OPERATIONS"
- DROP-OFF CONDITIONS BETWEEN TRAVELED WAY AND WORK ZONE DICTATE THAT CHANNELIZING DEVICES BE USED AT 50' INTERVALS AND 20' INTERVALS AT RADII AND LANE CLOSURE TAPERS
- UTILIZE HIGHWAY STANDARD 701602 FOR LANE SHIFTS

POST-STAGE:

- RESURFACING OF ALL LEGS WITHIN THE PROJECT LIMITS
- PAVEMENT MARKING PLACEMENT UTILIZING THE PROPOSED TRAFFIC PATTERN
- HIGHWAY STANDARD 701801 WILL BE UTILIZED TO CLOSE THE SHARED-USE PATH ON THE NORTH SIDE OF FOREST AVENUE, BETWEEN IL ROUTE 171 AND GROVELAND AVENUE TO THE EAST, AND THE SIDEWALK ON THE WEST SIDE OF THE IL ROUTE 171, BETWEEN PARKVIEW AVENUE AND FOREST AVENUE, DURING CONSTRUCTION. WESTBOUND PEDESTRIAN AND BICYCLE TRAFFIC ALONG FOREST AVENUE WILL BE DETOURED AT GROVELAND AVENUE TO THE EXISTING SHARED-USE PATH ON THE SOUTH SIDE OF FOREST AVENUE. NORTHBOUND PEDESTRIAN AND BICYCLE TRAFFIC ALONG IL ROUTE 171 WILL BE DETOURED AT PARKVIEW AVENUE TO THE EXISTING SIDEWALK ON THE EAST SIDE OF IL ROUTE 171.

LEGEND

- ① TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE
- ② PAVEMENT MARKING TAPE, TYPE III, 4" (YELLOW)
- ③ PAVEMENT MARKING TAPE, TYPE III, 6" (WHITE)
- ④ PAVEMENT MARKING TAPE, TYPE III, 4" (WHITE)
- ← DIRECTION OF TRAFFIC
- ▨ REMOVAL ITEMS
- ▨ CONSTRUCTION ITEMS





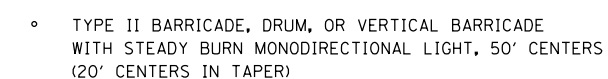
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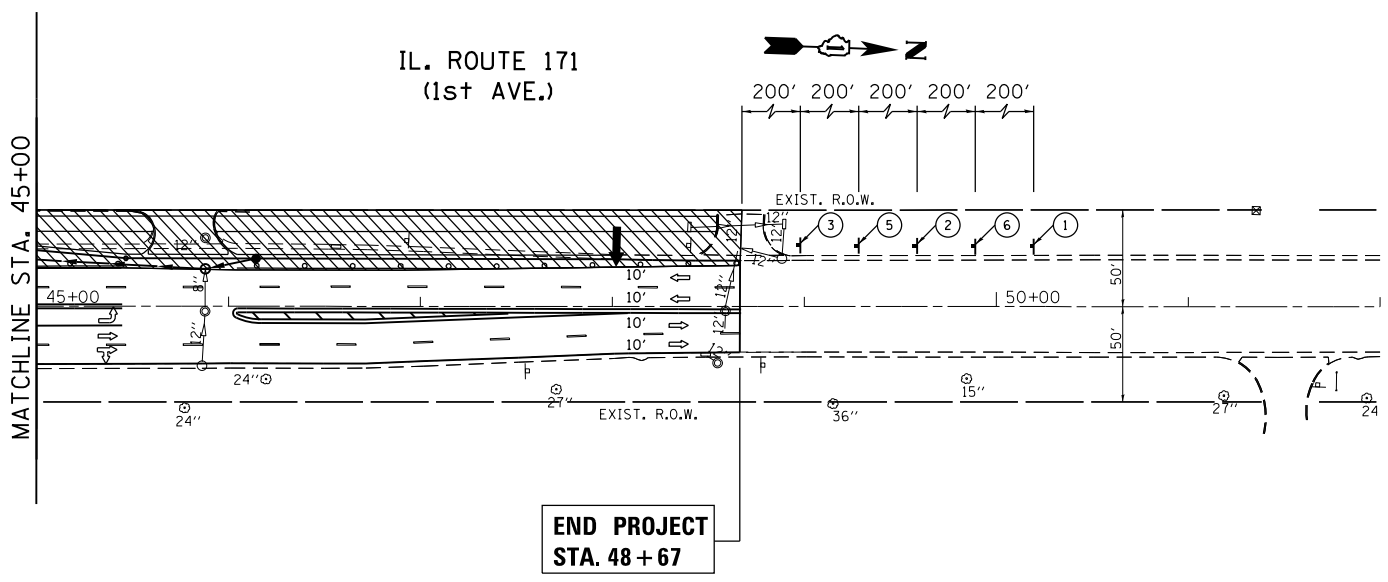
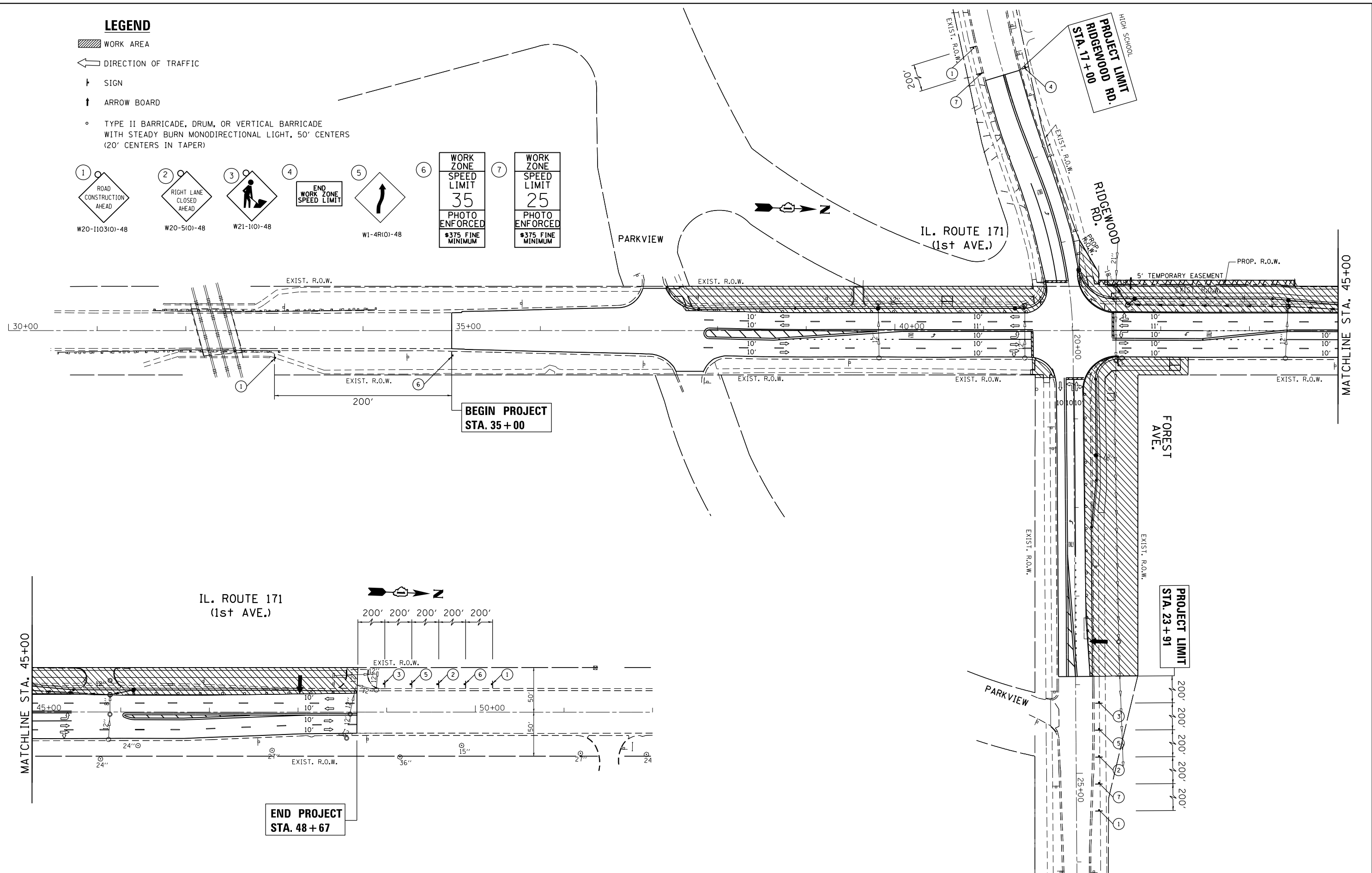
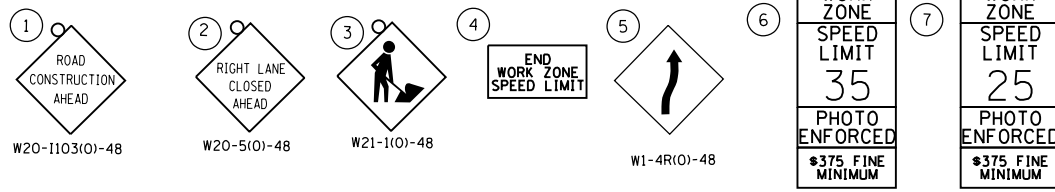
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL ROUTE 171 AT FOREST AVE/RIDGEWOOD RD.			
SUGGESTED STAGING TYPICAL SECTIONS - STAGE 1C			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	29
				CONTRACT NO. 60X08
ILLINOIS FED. AID PROJECT				

**LEGEND**

-  WORK AREA
-  DIRECTION OF TRAFFIC
-  SIGN
-  ARROW BOARD
-  TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT, 50' CENTERS (20' CENTERS IN TAPER)



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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD.  
SUGGESTED STAGES 1A, 1B, AND 1C - PEAK HOURS**

SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	30
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				

**LEGEND**

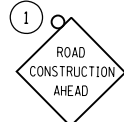
▨ WORK AREA

⇨ DIRECTION OF TRAFFIC

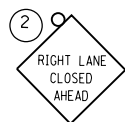
↑ SIGN

↑ ARROW BOARD

○ TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT, 50' CENTERS (20' CENTERS IN TAPER)



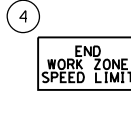
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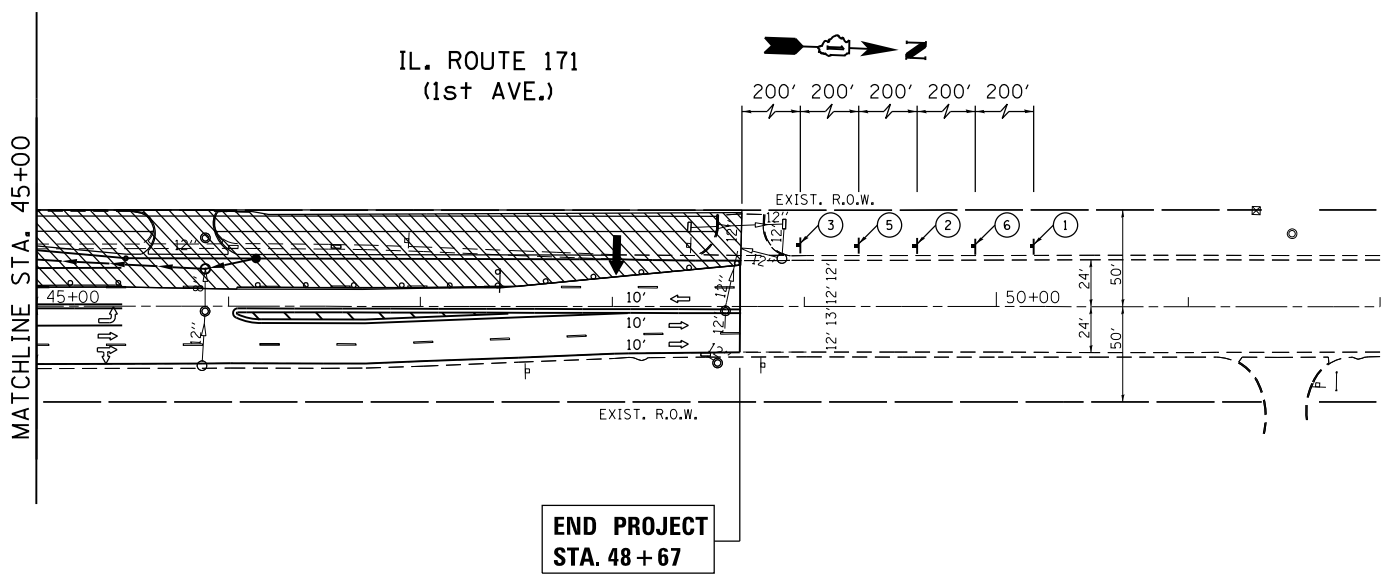
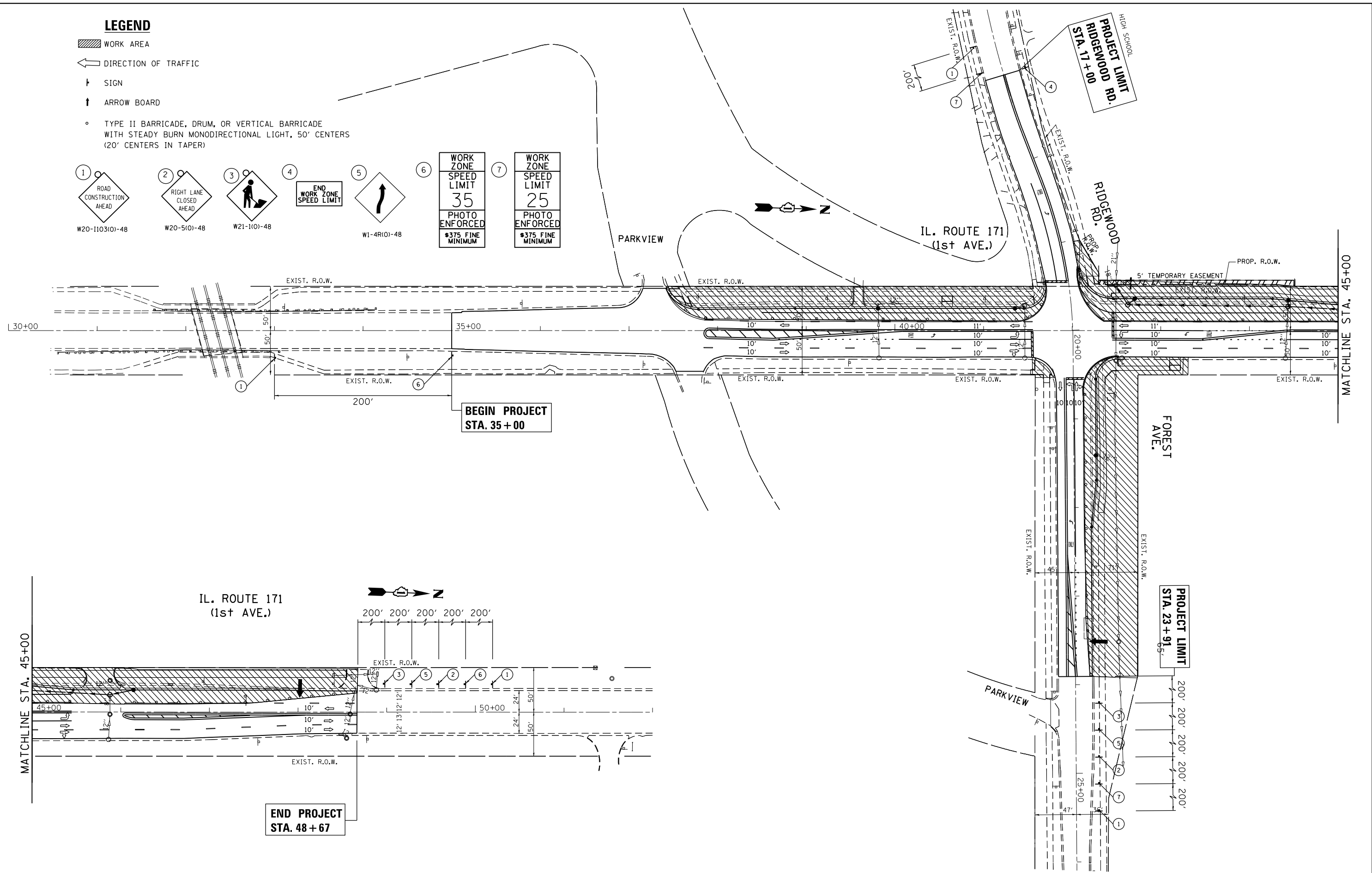
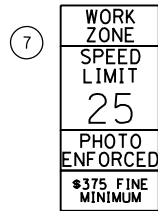
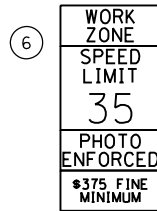
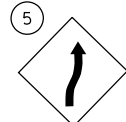
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W21-1(O)-48



W1-4R(O)-48



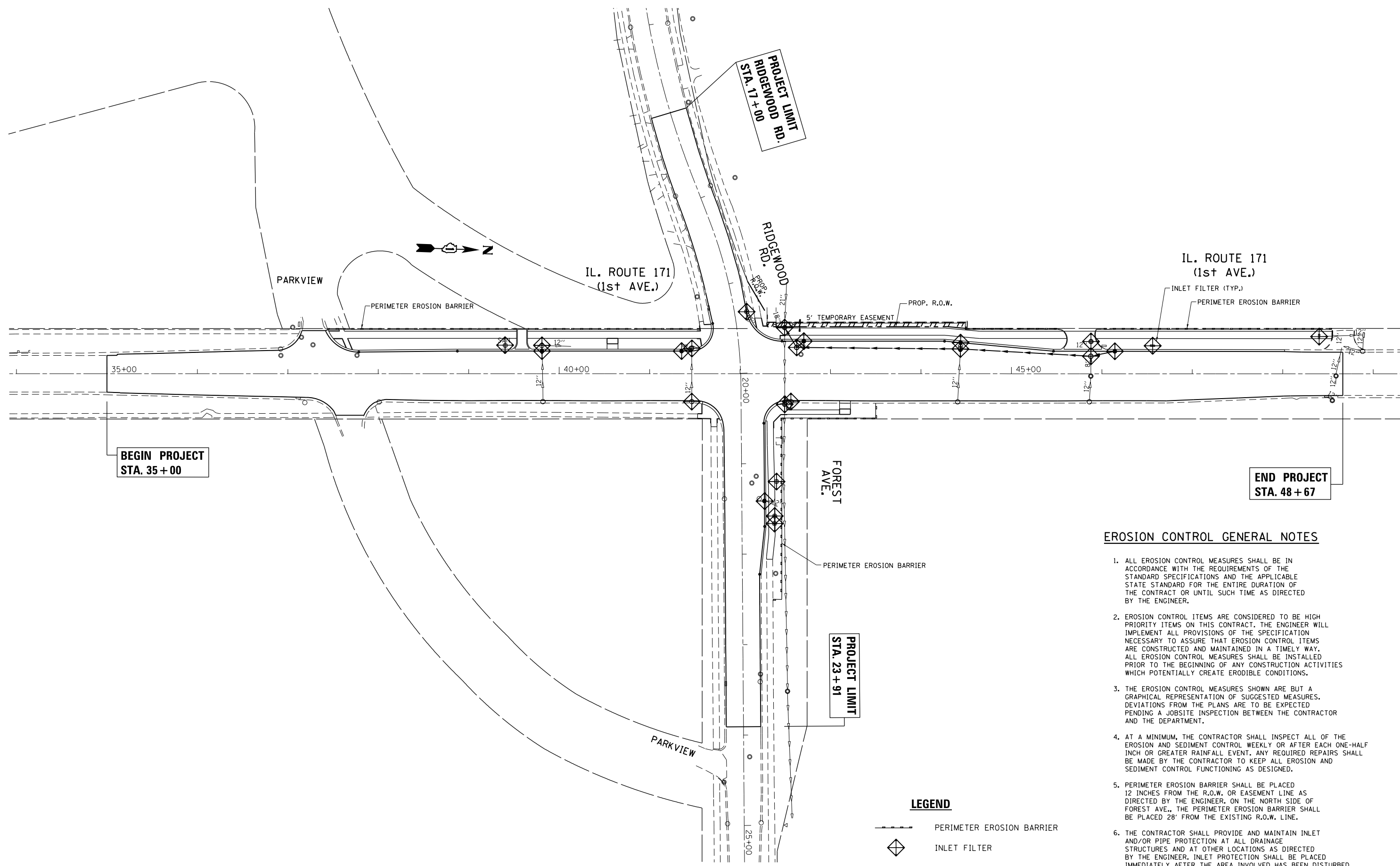
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD.  
SUGGESTED STAGES 1A, 1B, AND 1C - OFF-PEAK HOURS**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	31
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.



**BEGIN PROJECT  
STA. 35 + 00**

**END PROJECT  
STA. 48 + 67**

**PROJECT LIMIT  
RIDGEWOOD RD.  
STA. 17 + 00**

**PROJECT LIMIT  
STA. 23 + 91**

**EROSION CONTROL GENERAL NOTES**

1. ALL EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE STATE STANDARD FOR THE ENTIRE DURATION OF THE CONTRACT OR UNTIL SUCH TIME AS DIRECTED BY THE ENGINEER.
2. EROSION CONTROL ITEMS ARE CONSIDERED TO BE HIGH PRIORITY ITEMS ON THIS CONTRACT. THE ENGINEER WILL IMPLEMENT ALL PROVISIONS OF THE SPECIFICATION NECESSARY TO ASSURE THAT EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY WAY. ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE BEGINNING OF ANY CONSTRUCTION ACTIVITIES WHICH POTENTIALLY CREATE ERODIBLE CONDITIONS.
3. THE EROSION CONTROL MEASURES SHOWN ARE BUT A GRAPHICAL REPRESENTATION OF SUGGESTED MEASURES. DEVIATIONS FROM THE PLANS ARE TO BE EXPECTED PENDING A JOBSITE INSPECTION BETWEEN THE CONTRACTOR AND THE DEPARTMENT.
4. AT A MINIMUM, THE CONTRACTOR SHALL INSPECT ALL OF THE EROSION AND SEDIMENT CONTROL WEEKLY OR AFTER EACH ONE-HALF INCH OR GREATER RAINFALL EVENT. ANY REQUIRED REPAIRS SHALL BE MADE BY THE CONTRACTOR TO KEEP ALL EROSION AND SEDIMENT CONTROL FUNCTIONING AS DESIGNED.
5. PERIMETER EROSION BARRIER SHALL BE PLACED 12 INCHES FROM THE R.O.W. OR EASEMENT LINE AS DIRECTED BY THE ENGINEER. ON THE NORTH SIDE OF FOREST AVE., THE PERIMETER EROSION BARRIER SHALL BE PLACED 28' FROM THE EXISTING R.O.W. LINE.
6. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN INLET AND/OR PIPE PROTECTION AT ALL DRAINAGE STRUCTURES AND AT OTHER LOCATIONS AS DIRECTED BY THE ENGINEER. INLET PROTECTION SHALL BE PLACED IMMEDIATELY AFTER THE AREA INVOLVED HAS BEEN DISTURBED.

**LEGEND**

- PERIMETER EROSION BARRIER
- INLET FILTER

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD.  
EROSION AND SEDIMENT CONTROL PLAN**

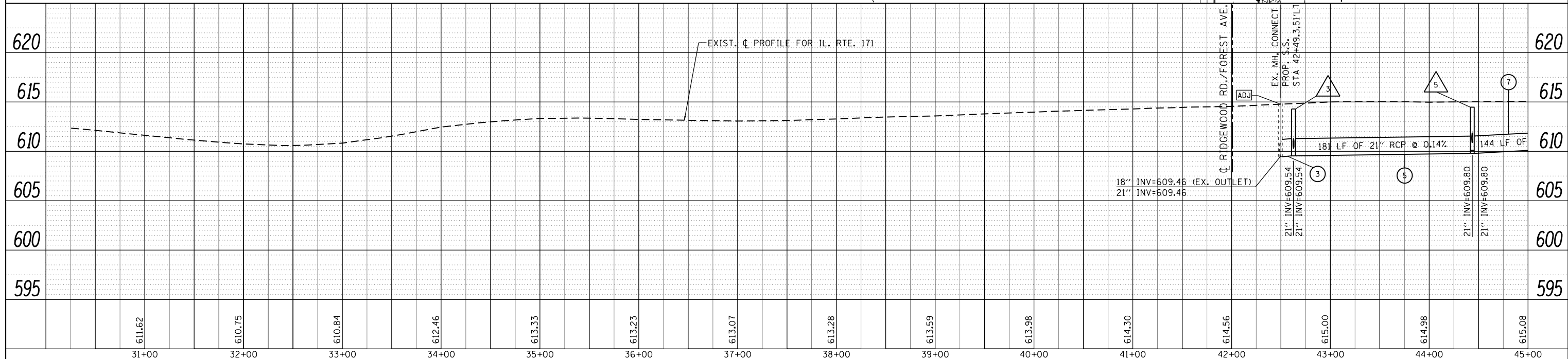
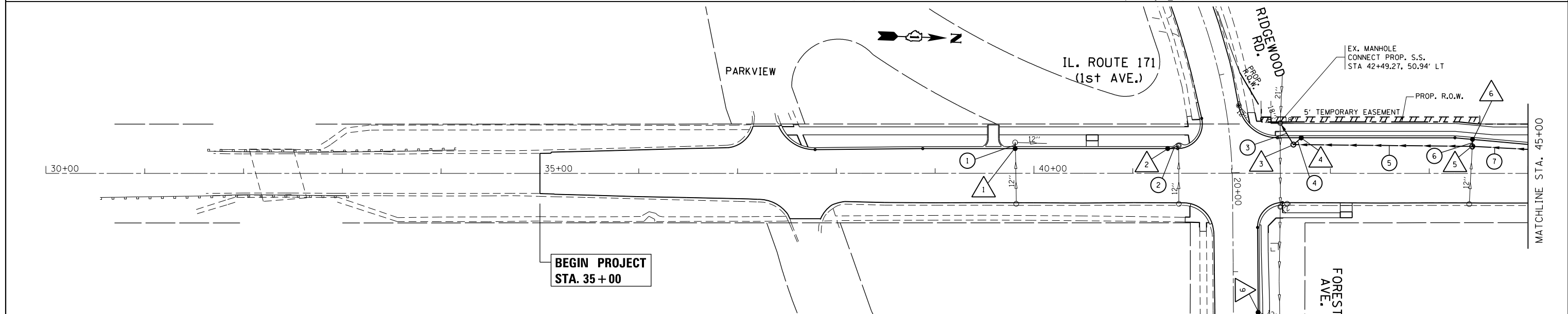
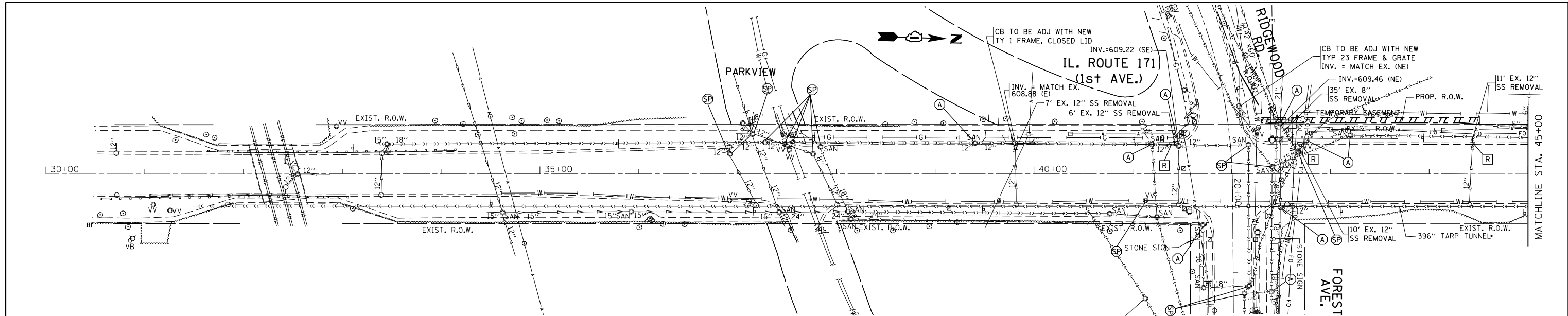
SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	32
<b>CONTRACT NO. 60X08</b>				
ILLINOIS FED. AID PROJECT				



PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	ALIGNMENT CHECKED	
	STRUCTURE CHECKED	
	NOTE BOOK NO.	
	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	ALIGNMENT CHECKED	
	STRUCTURE CHECKED	
	NOTE BOOK NO.	
	FILE NAME	

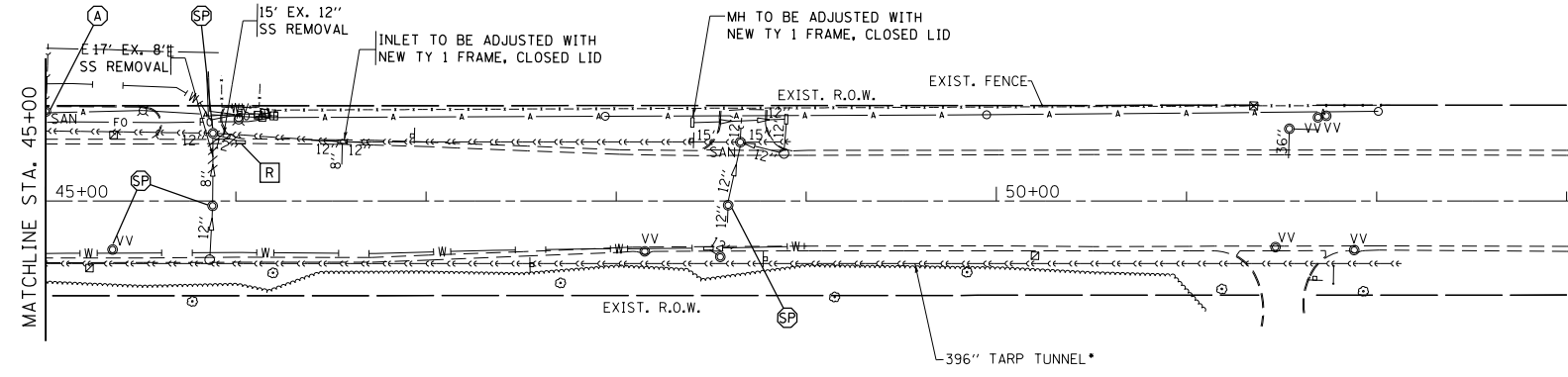


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c:\pwork\pwidot\toriqf\m\0315747\P114412	sh-t-drain.dgn	DRAWN -	REVISED -		372	03035-TS	COOK	87	33				
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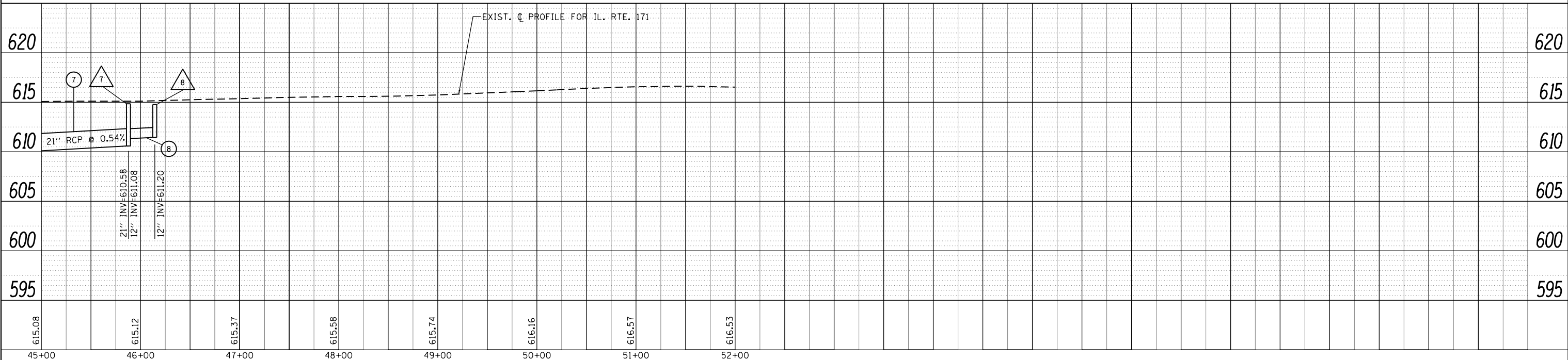
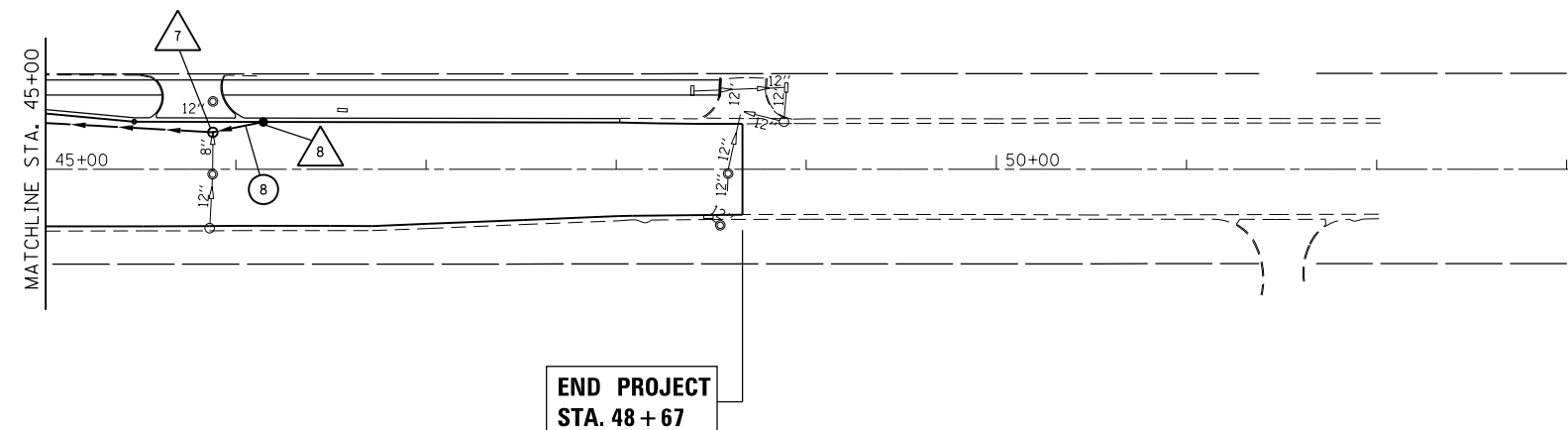
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	PLOTTED	
	ALIGNED	
	CHECKED	
	FILE NAME	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NO.	

IL. ROUTE 171  
(1st AVE.)



IL. ROUTE 171  
(1st AVE.)



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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

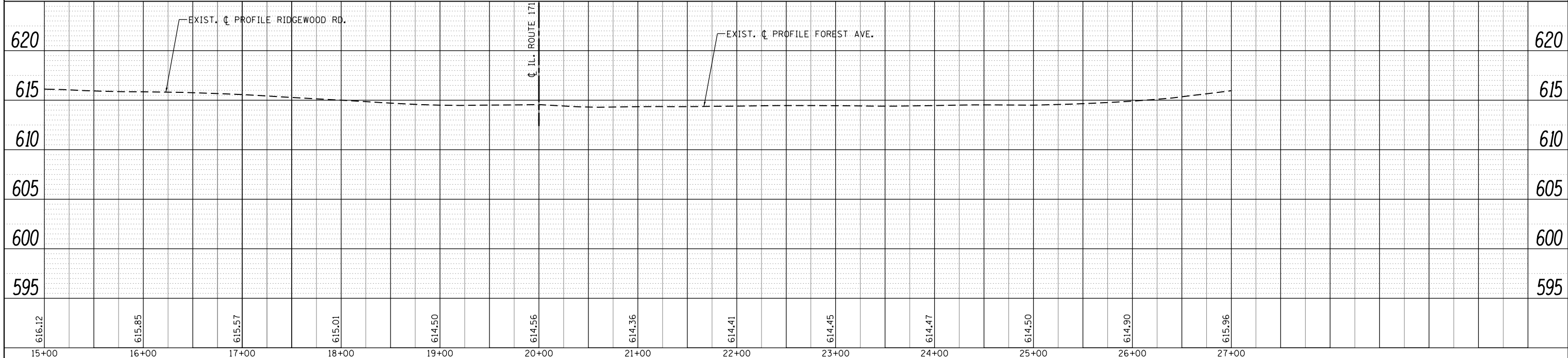
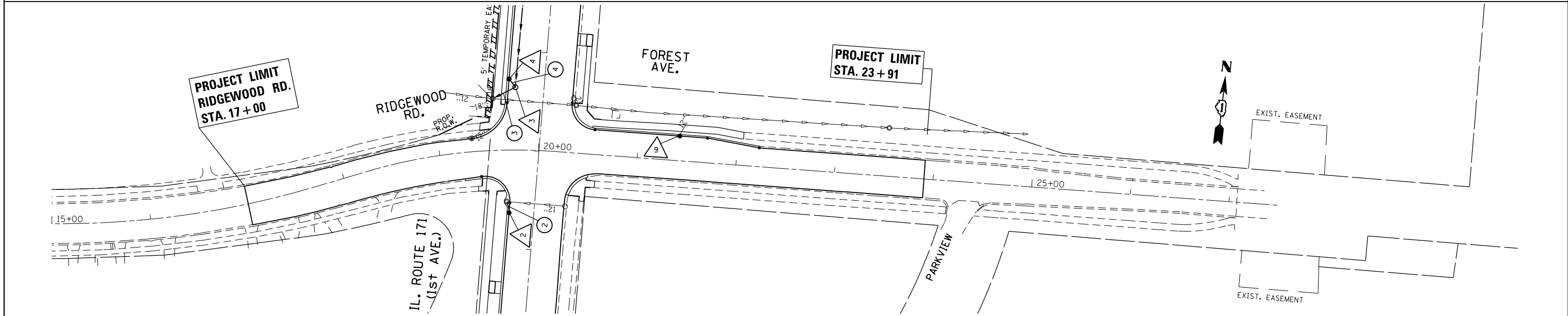
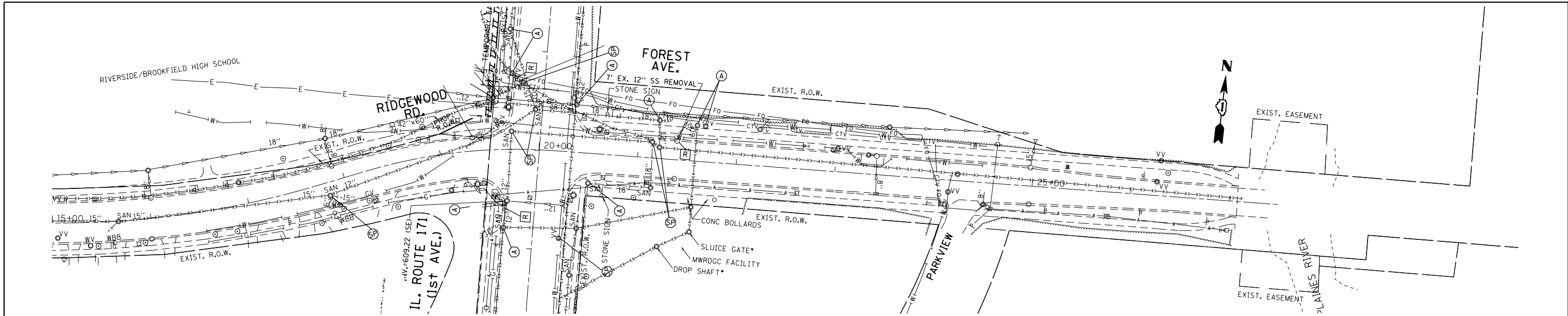
IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD.  
DRAINAGE PLAN AND PROFILE

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372	0303S-TS	COOK	87	34
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.


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	CHECKED	
	FILED	
	NO.	


PROFILE	SURVEYED	DATE
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	GRADES	
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	STRUCTURE	
	NOTATIONS	
	CHKD	
	NO.	





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
DRAINAGE STRUCTURES:


FLAT TOP  CB, TY-A, 4' DIA  
W/ TY-23 F&G  
STA 39+81.2 (IL 171), 25.8' LT  
RIM 613.35  
INV MATCH EX 608.94 (E)  
INV 608.94 (W)


 CB, TY-C  
W/ TY-23 F&G  
STA 41+35.4 (IL 171), 25.8' LT  
RIM 613.56  
INV 609.50 (NW)


FLAT TOP  MH, TY-A, 4' DIA  
W/ 9" RESTRICTOR  
W/ TY-1 FR & CLID  
STA 42+62.6 (IL 171), 29.0' LT  
RIM 614.30  
INV 609.54 (N)  
INV 609.54 (SW)  
INV 609.54 (NW)  
INV 609.54 (RESTRICTOR)  
OVERFLOW ELEVATION 613.11  
• NOTE: NOTE #3 IN DISTRICT DETAIL BD-12 SHALL BE DISREGARDED.


 CB, TY-C  
W/ TY-23 F&G  
STA 42+70.6 (IL 171), 36.9' LT  
RIM 614.15  
INV 609.75 (SE)

FLAT TOP  MH, TY-A, 5' DIA  
W/ TY-1 FR & CLID  
STA 44+43.6 (IL 171), 27.4' LT  
RIM 614.48  
INV 609.80 (N)  
INV 609.80 (S)  
INV MATCH EX 609.84 (E)  
INV 610.30 (W)


 CB, TY-C  
W/ TY-23 F&G  
STA 44+43.6 (IL 171), 35.5' LT  
RIM 614.32  
INV 610.45 (E)


FLAT TOP  MH, TY-A, 4' DIA  
W/ TY-1 FR & CLID  
STA 45+87.9 (IL 171), 19.4' LT  
RIM 614.85  
INV 611.08 (NW)  
INV 610.58 (S)  
INV MATCH EX (E)  
• NOTE: MANHOLE SHALL BE CONSTRUCTED TO A DEPTH OF 10'  
(TOP OF MASONRY TO TOP OF BOTTOM SLAB)


 CB, TY-C  
W/ TY-23 F&G  
STA 46+14.6 (IL 171), 25.7' LT  
RIM 614.78  
INV 611.20 (SE)


 CB, TY-C  
W/ TY-12 F&G  
STA 21+41.2 (FOREST AVE), 25.9' LT  
RIM 613.53  
INV MATCH EX (NE)


STORM SEWERS:


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LENGTH = 7'  
SLOPE = 0.9%  
TBF = 2.4 CU. YD.


 12" DIA SS, CL A, TY 2  
LENGTH = 12'  
SLOPE = 2.3%  
TBF = 3.1 CU. YD.


 21" DIA SS, CL A, TY 2  
LENGTH = 26'  
SLOPE = 0.3%  
TBF = 10.8 CU. YD.

 12" DIA SS, CL A, TY 2  
LENGTH = 11'  
SLOPE = 1.9%  
TBF = 3.3 CU. YD.

 21" DIA SS, CL A, TY 1  
LENGTH = 181'  
SLOPE = 0.1%  
TBF = 47.5 CU. YD.

 12" DIA SS, CL A, TY 2  
LENGTH = 8'  
SLOPE = 1.9%  
TBF = 2.1 CU. YD.

 21" DIA SS, CL A, TY 1  
LENGTH = 144'  
SLOPE = 0.5%  
TBF = 31.7 CU. YD.

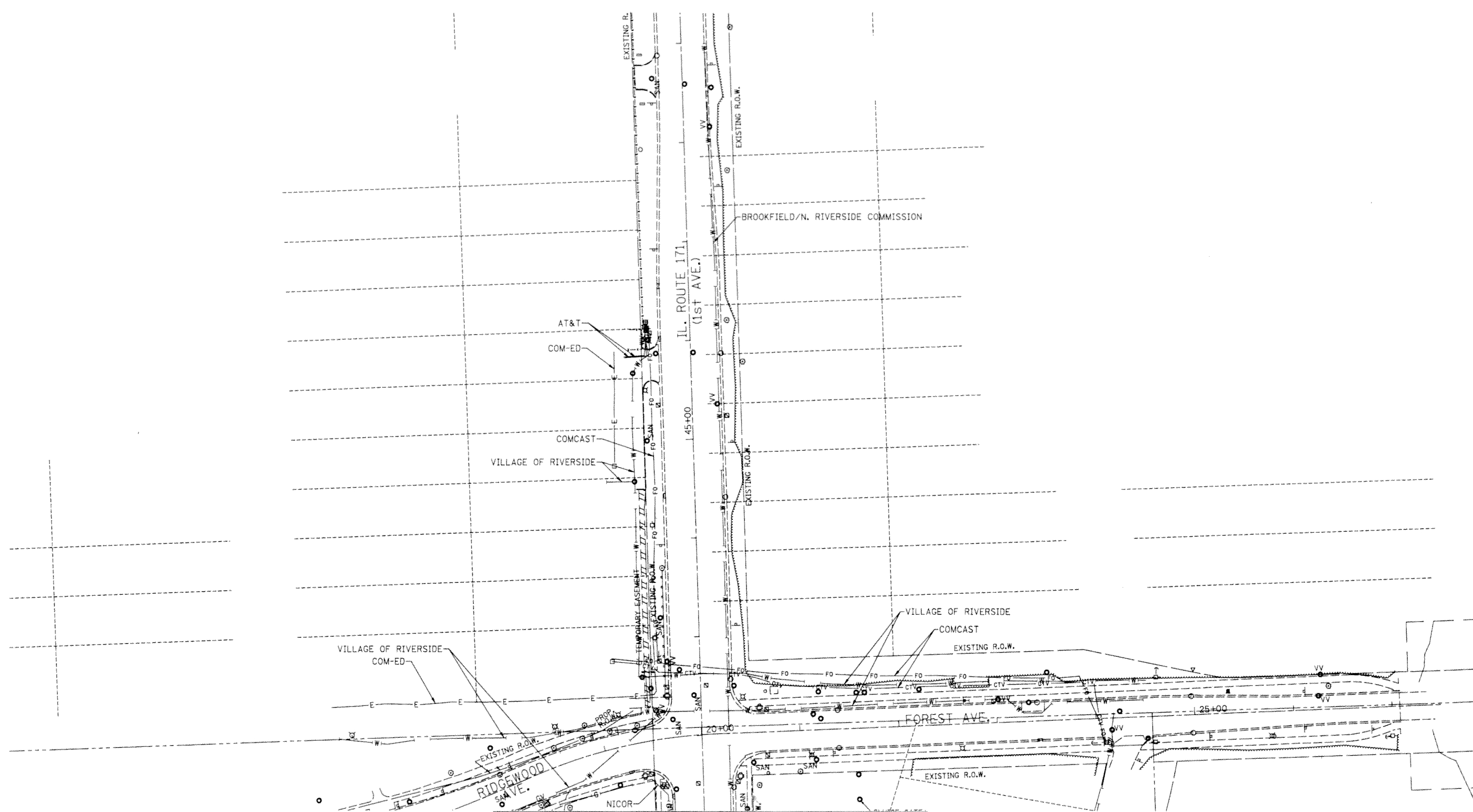
 12" DIA SS, CL A, TY 1  
LENGTH = 27'  
SLOPE = 0.4%  
TBF = 5.0 CU. YD.

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	PLOT DATE = 3/31/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD. PROPOSED DRAINAGE STRUCTURES AND STORM SEWERS			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	36
				CONTRACT NO. 60X08
ILLINOIS FED. AID PROJECT				



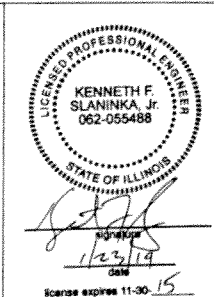
- A — A — AERIAL
- - - - - UNKNOWN
- CTV — CTV — CABLE TV
- T — T — TELEPHONE
- G — G — GAS
- E — E — ELECTRIC
- W — W — WATER
- FO — FO — FIBER OPTIC
- ⊙ TBE TEST HOLE

**UTILITY OWNERS**

AT&T - FIBER OPTIC  
 AT&T - TELEPHONE  
 BROOKFIELD/N. RIVERSIDE COMMISSION - WATER  
 COMCAST - CABLE TV  
 COMCAST - FIBER OPTIC  
 COM-ED - ELECTRIC  
 NICOR - GAS  
 VILLAGE OF RIVERSIDE - WATER

Utilities shown on these plans as depicted in the legend have been investigated by Cardno TBE in accordance with SUE Industry Standards. All other information shown has been provided to Cardno TBE by others. Cardno TBE's SUE field investigation was performed 1/08/14 through 1/17/14. Changes to utilities after 1/17/14 may have been made and therefore may result in variances from this plan. Consideration should be given to updating this plan if deemed advisable prior to final design and construction.

ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



TBE Job No. IL09510558  
 SUE Plan Pages: 1 of 2

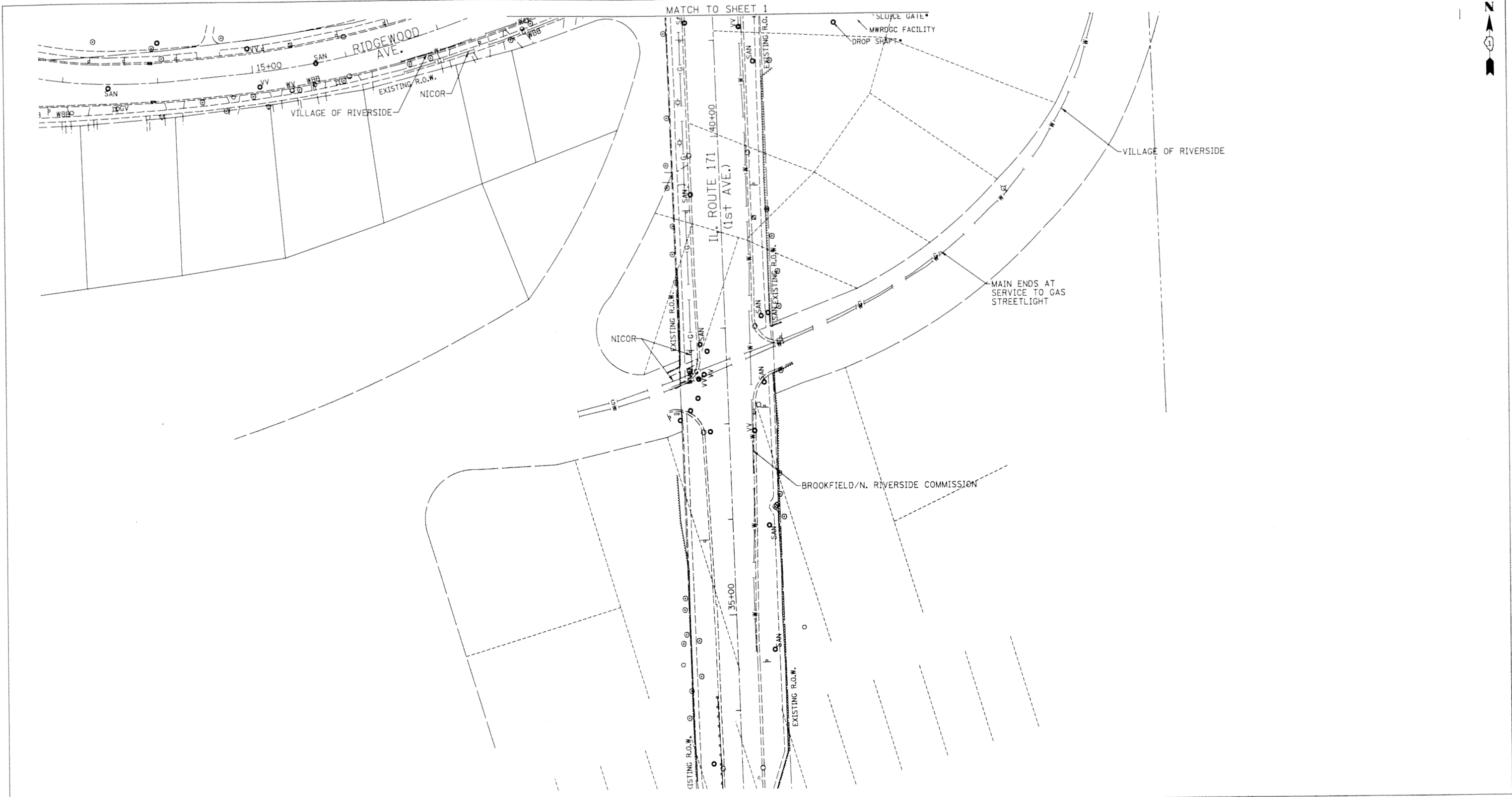
Utility Quality Level "A": Visually Verified Test Hole  
 Utility Quality Level "B": Designating/non Visually Verified Test Hole  
 Utility Quality Level "C": Research with Survey  
 Utility Quality Level "D": Records Research

DESIGNED LP	REVISED
DRAWN SRK	REVISED
CHECKED KFS	REVISED
DATE 1/20/14	REVISED

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**IL 171 at Forest Avenue/Ridgewood Road  
 Riverside, Illinois**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	37
Contract No. 60X08				
FED. ROAD DIST. NO. ILLINOIS IDOT Project No.				

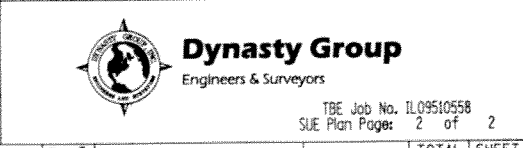
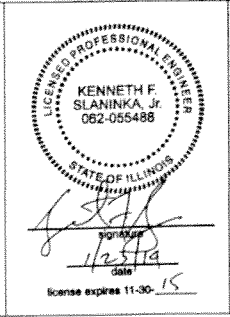


— A —	A	AERIAL
- - - - -		UNKNOWN
— CTV —	CTV	CABLE TV
— T —	T	TELEPHONE
— G —		GAS
— E —	E	ELECTRIC
— W —	W	WATER
— FO —	FO	FIBER OPTIC
⊕		TBE TEST HOLE

UTILITY OWNERS	
AT&T - FIBER OPTIC	
AT&T - TELEPHONE	
BROOKFIELD/N. RIVERSIDE COMMISSION - WATER	
COMCAST - CABLE TV	
COMCAST - FIBER OPTIC	
COM-ED - ELECTRIC	
NICOR - GAS	
VILLAGE OF RIVERSIDE - WATER	

Utilities shown on these plans as depicted in the legend have been investigated by Cardno TBE in accordance with SUE Industry Standards. All other information shown has been provided to Cardno TBE by others. Cardno TBE's SUE field investigation was performed 1/08/14 through 1/17/14. Changes to utilities after 1/17/14 may have been made and therefore may result in variances from this plan. Consideration should be given to updating this plan if deemed advisable prior to final design and construction.

ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



Utility Quality Level "A": Visually Verified Test Hole  
 Utility Quality Level "B": Designating/non Visually Verified Test Hole  
 Utility Quality Level "C": Research with Survey  
 Utility Quality Level "D": Records Research

DESIGNED	LP	REVISED	
DRAWN	SRK	REVISED	
CHECKED	KFS	REVISED	
DATE	1/20/14	REVISED	

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

IL 171 at Forest Avenue/Ridgewood Road  
 Riverside, Illinois

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	38
Contract No. 60X08				
FED. ROAD DIST. NO. ILLINOIS IDOT Project No.				

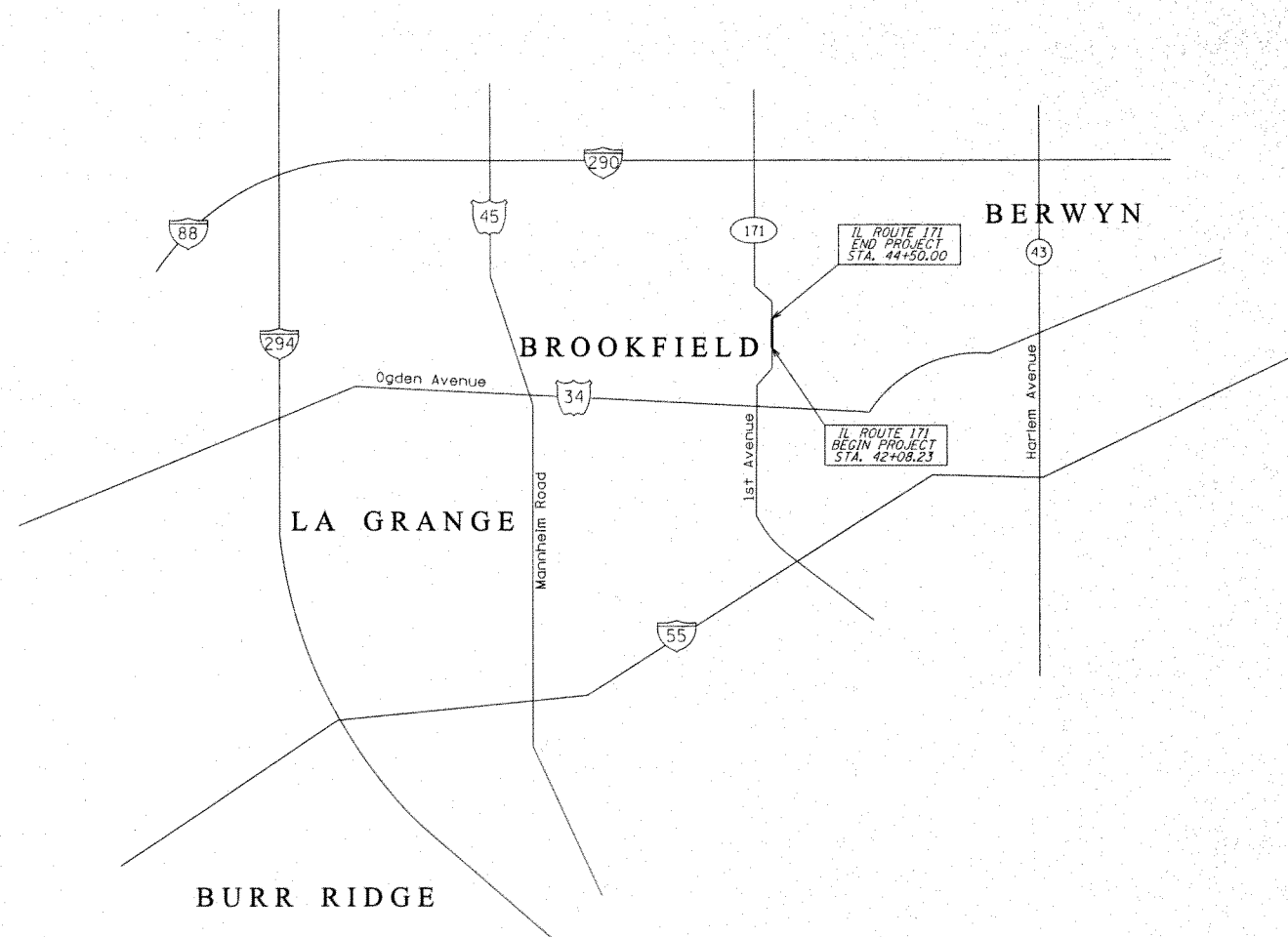
TBE Job No. IL09510558  
 SUE Plan Pages: 2 of 2

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
**PLAT OF HIGHWAYS**

**IL ROUTE 171  
SECTION  
COOK COUNTY  
LIMITS Forest Ave./Ridgewood Ave.  
JOB NO.: R-90-024-13**

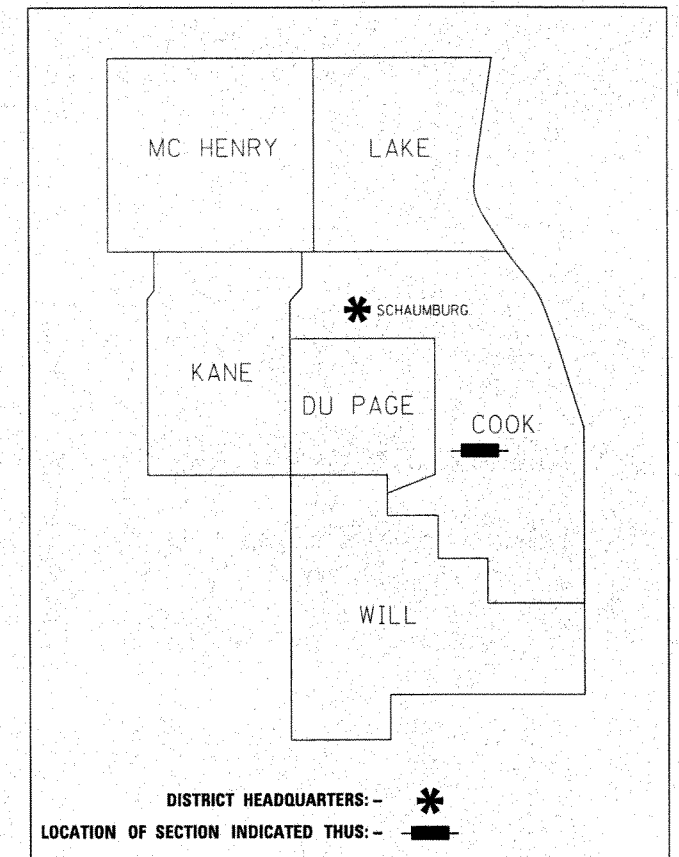
PARCEL NUMBER	OWNER	SHEET NUMBER	PROPERTY ACQUIRED BY
0K90001 0K90001E	RIVERSIDE BROOKFIELD HIGH SCHOOL DISTRICT NO. 208 OF COOK COUNTY, ILLINOIS WHICH ACQUIRED TITLE AS THE TRUSTEES OF SCHOOLS OF TOWNSHIP 39 NORTH, RANGE 12 EAST OF THE 3RD PRINCIPAL MERIDIAN, COOK COUNTY, ILLINOIS IN TRUST FOR THE RIVERSIDE BROOKFIELD HIGH SCHOOL, BEING SCHOOL DISTRICT NO. 208	3	

PROJECT COORDINATES				
Illinois State Plane, East Zone, NAD 83 (2007)				
US ROUTE 171 CENTERLINE				
Pt. Number	Station	Offset	Northing	Easting
503	42+08.24	102.04	LT. 1880334.806	1121641.928
504	42+30.00	64.94	LT. 1880357.801	1121678.279
505	42+30.00	56.94	LT. 1880358.068	1121686.274
506	42+30.00	51.94	LT. 1880358.235	1121691.271
507	44+30.00	52.03	LT. 1880558.120	1121684.479
508	44+50.00	50.04	LT. 1880578.175	1121685.799
509	44+50.00	57.04	LT. 1880577.941	1121678.803
2138	42+08.23	49.93	LT. 1880336.548	1121694.010
7000	42+00.66	0.00	LT. 1880330.656	1121744.161
RIDGEWOOD AVE. CENTERLINE				
Pt. Number	Station	Offset	Northing	Easting
503	19+02.00	20.81	LT. 1880334.806	1121641.928
504	19+40.45	35.31	LT. 1880357.801	1121678.279
505	19+47.88	34.17	LT. 1880358.068	1121686.274
506	19+52.54	33.51	LT. 1880358.235	1121691.271
507	19+70.08	232.33	LT. 1880558.120	1121684.479
508	19+72.69	525.06	LT. 1880578.175	1121685.799
509	19+67.98	252.72	LT. 1880577.941	1121678.803
2009	10+76.70	35.97	LT. 1880309.212	1120876.603
2138	19+51.79	11.67	LT. 1880336.548	1121694.010
7000	20+00.40	0.00	LT. 1880330.656	1121744.161
7014	13+29.06	0.00	LT. 1880190.871	1121093.019
7015	14+21.06	0.00	LT. 1880193.031	1121184.924
7017	10+84.90	0.00	LT. 1880273.520	1120866.657
7019	11+45.47	0.00	LT. 1880242.714	1120918.801
7036	14+87.07	0.00	LT. 1880199.086	1121250.657
7039	18+25.45	0.00	LT. 1880289.925	1121574.855



**LOCATION MAP**

GROSS LENGTH = 241.77 FT. = 0.046 MILE



**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

<b>RT &amp;A</b>	<b>Ruettiger, Tonelli &amp; Associates, Inc.</b> <small>Surveyors • Engineers • Planners • Landscape Architects • E.I.T.S. Consultants</small> <small>129 CAUSTIA DRIVE • BIRKENWOOD, ILLINOIS 60004</small> <small>PH: (815) 744-6600 FAX: (815) 744-0101</small> <small>website: www.ruettiger-tonelli.com</small>
R.T. & A. Dwg. No.: 2013-0897	

**IDOT USE ONLY**

RECEIVED  
GF JAN 28 2014  
PLATS & LEGALS

SHEET  
NO.  
39

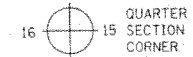
PART OF NE.1/4 OF SECTION 35, TWP. 39 N., R. 12 E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.

EXIST. CURVE C2	EXIST. CURVE C3	EXIST. CURVE C4	EXIST. CURVE C5
PI STA. = 12+39.14	PI STA. = 13+75.13	PI STA. = 16+29.07	PI STA. = 19+21.45
$\Delta = 28^\circ 00' 17''$ (LT)	$\Delta = 7^\circ 49' 56''$ (LT)	$\Delta = 17^\circ 47' 45''$ (LT)	$\Delta = 20^\circ 41' 40''$ (RT)
D = 15° 15' 14"	D = 8° 30' 48"	D = 6° 19' 01"	D = 10° 53' 50"
R = 375.61'	R = 673.02'	R = 907.01'	R = 525.79'
T = 93.67'	T = 46.07'	T = 142.00'	T = 96.00'
L = 183.59'	L = 92.00'	L = 281.71'	L = 189.91'
E = 11.50'	E = 1.58'	E = 11.05'	E = 8.69'
e = -----	e = -----	e = -----	e = -----
T.R. = -----	T.R. = -----	T.R. = -----	T.R. = -----
S.E. RUN = -----	S.E. RUN = -----	S.E. RUN = -----	S.E. RUN = -----
P.C. STA. = 11+45.47	P.C. STA. = 13+29.06	P.C. STA. = 14+87.07	P.C. STA. = 18+25.45
P.T. STA. = 13+29.06	P.T. STA. = 14+21.06	P.T. STA. = 17+68.79	P.T. STA. = 20+15.36

PARCEL NUMBER	TOTAL HOLDING ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT ACRES	AREA SQUARE FEET	PARCEL INDEX NUMBER
OK90001	7.299	0.026	0.000	7.273			15-35-200-003
OK90001TE					0.026	1120	

- NOTES:
- ALL DIMENSIONS ARE MEASURED UNLESS OTHERWISE SPECIFIED.
  - BEARINGS AND DISTANCES SHOWN HEREON REFERENCE THE ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NORTH AMERICAN DATUM OF 1983 (2007 ADJUSTMENT) "GRID".
  - ALL MEASURED AND CALCULATED DISTANCES ARE "GRID" NOT "GROUND". TO OBTAIN GROUND DISTANCES, DIVIDE GRID DISTANCES SHOWN BY THE COMBINATION FACTOR OF 0.9999672065.
  - THE PLAT OF RIVERSIDE FOURTH DIVISION IS VAGUE IN ITS DIMENSIONING OF RIGHT OF WAY LINES. IN THAT NO RADII ARE LABELED FOR APPARENT CURVED LINES. SURVEYOR HAS DELINEATED RIGHT OF WAY LINES BASED ON FOUND MONUMENTATION AND/OR BEST EVIDENCE.
  - SEE SHEET 1 FOR THE COORDINATE TABLE.

LEGEND



- SECTION / QUARTER SECTION LINE
- PLATTED LOT LINES
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- EXISTING CENTERLINE
- PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- EXISTING EASEMENT
- PROPOSED EASEMENT
- AC - EXISTING ACCESS CONTROL LINE
- AC - PROPOSED ACCESS CONTROL LINE
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORDED DIMENSION
- EXISTING BUILDING

- o IRON PIPE OR ROD FOUND
- + CUT CROSS FOUND OR SET
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- T2 THESE STAKES IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.
- BT1 BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT2
- BT3
- STAKING OF PROPOSED RIGHT OF WAY, SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET

STATE OF ILLINOIS )  
COUNTY OF WILL )

THIS IS TO CERTIFY THAT I, KENNETH J. PESAVENTO, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, (WE, RUETTIGER, TONELLI & ASSOCIATES, INC., A PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 3425) HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 35, TOWNSHIP 39 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

FIELD WORK COMPLETED ON 10-30-2013  
DATED AT SHOREWOOD, ILLINOIS THIS 18th DAY OF March 2014 A.D.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3425  
LICENSE EXPIRATION DATE: 11-30-2014

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

**RT & A** Ruettiger, Tonelli & Associates, Inc.  
Surveyors • Engineers • Planners • Landscape Architects • GIS Consultants  
129 CAPSTA DRIVE - SHOREWOOD, ILLINOIS 60404  
PH (815) 744-6505 FAX (815) 744-9101  
www.ruettiger-tonelli.com  
R.T. & A. Dwg. No.: 2013-0897

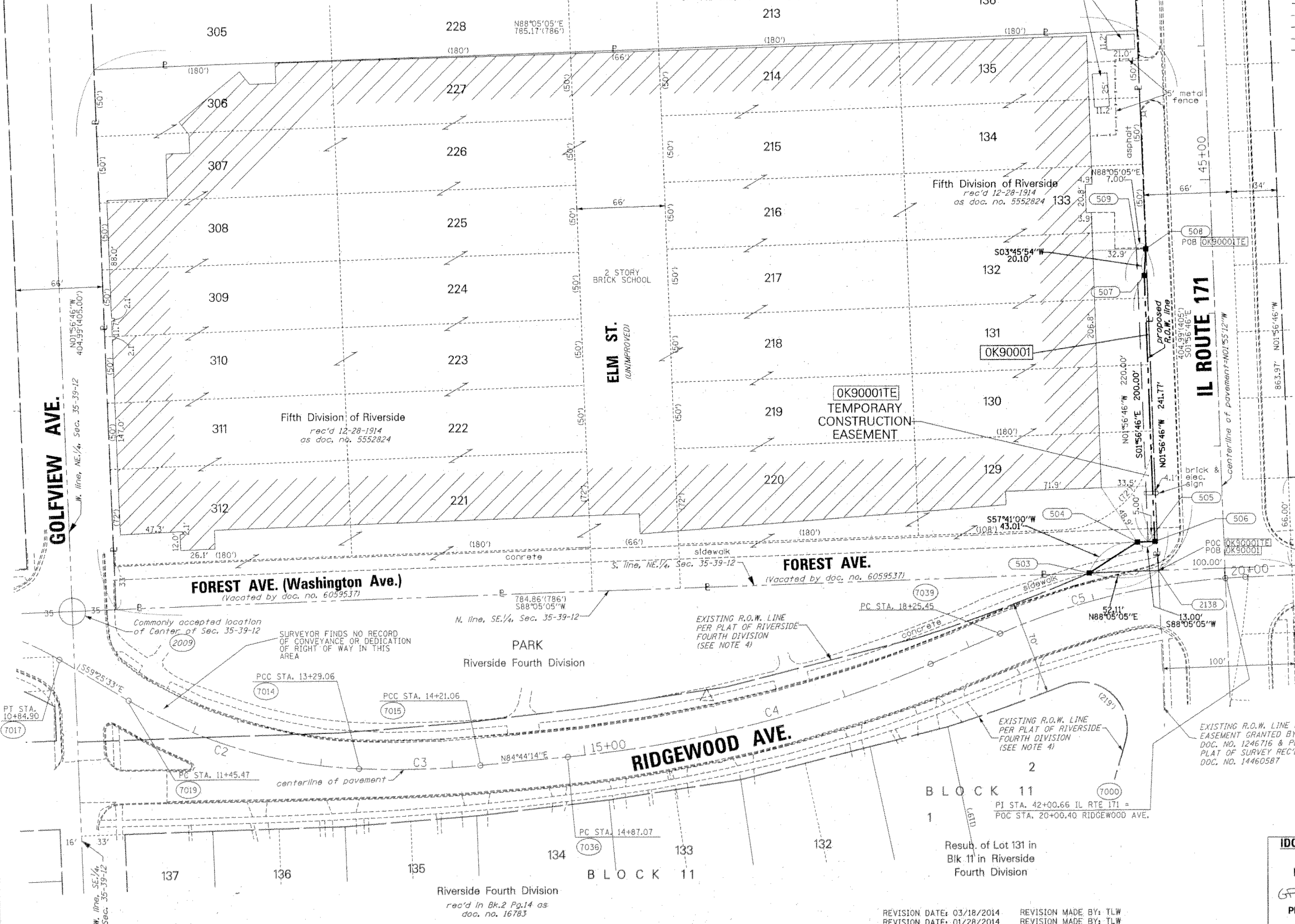
**PLAT OF HIGHWAYS**  
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**IL ROUTE 171**

LIMITS: @ Forest/Ridgewood COUNTY: COOK  
SECTION: JOB NO.: R90-024-13  
STA. 42+00.66 TO STA. 46+50  
SCALE: 1"=40' SHEET 2 OF 2 SHEETS

**IDOT USE ONLY**  
**RECEIVED**  
MAR 18 2014  
**PLATS & LEGALS**

**BUREAU OF LAND ACQUISITION**  
**201 WEST CENTER COURT**  
**SCHAUMBURG, ILLINOIS 60196**

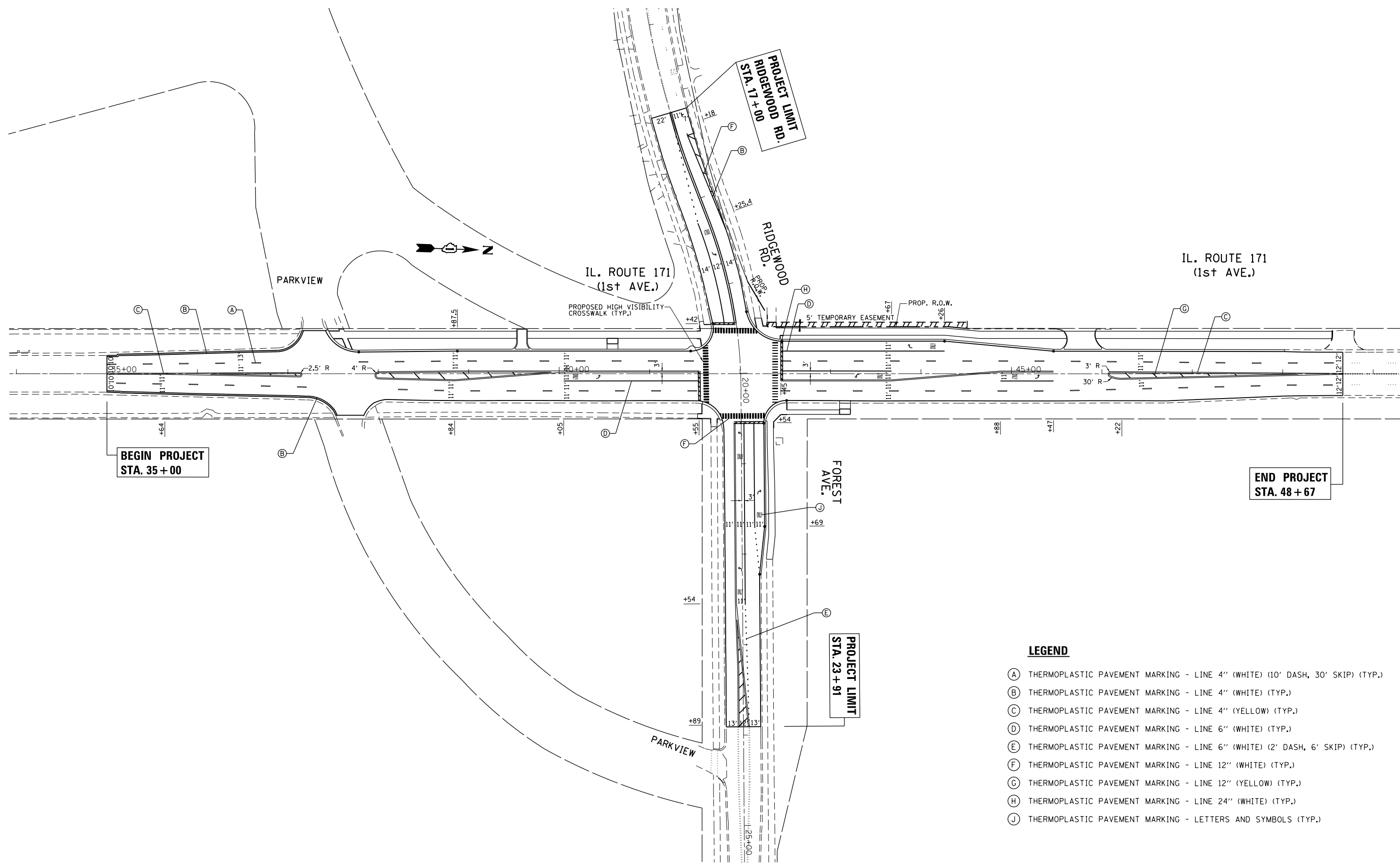
**SHEET NO. 40**



REVISION DATE: 03/18/2014 REVISION MADE BY: TLW  
REVISION DATE: 01/28/2014 REVISION MADE BY: TLW

Result of Lot 131 in Blk. 11 in Riverside Fourth Division





**BEGIN PROJECT  
STA. 35 + 00**

**END PROJECT  
STA. 48 + 67**

**PROJECT LIMIT  
RIDGEWOOD RD.  
STA. 17 + 00**

**PROJECT LIMIT  
STA. 23 + 91**

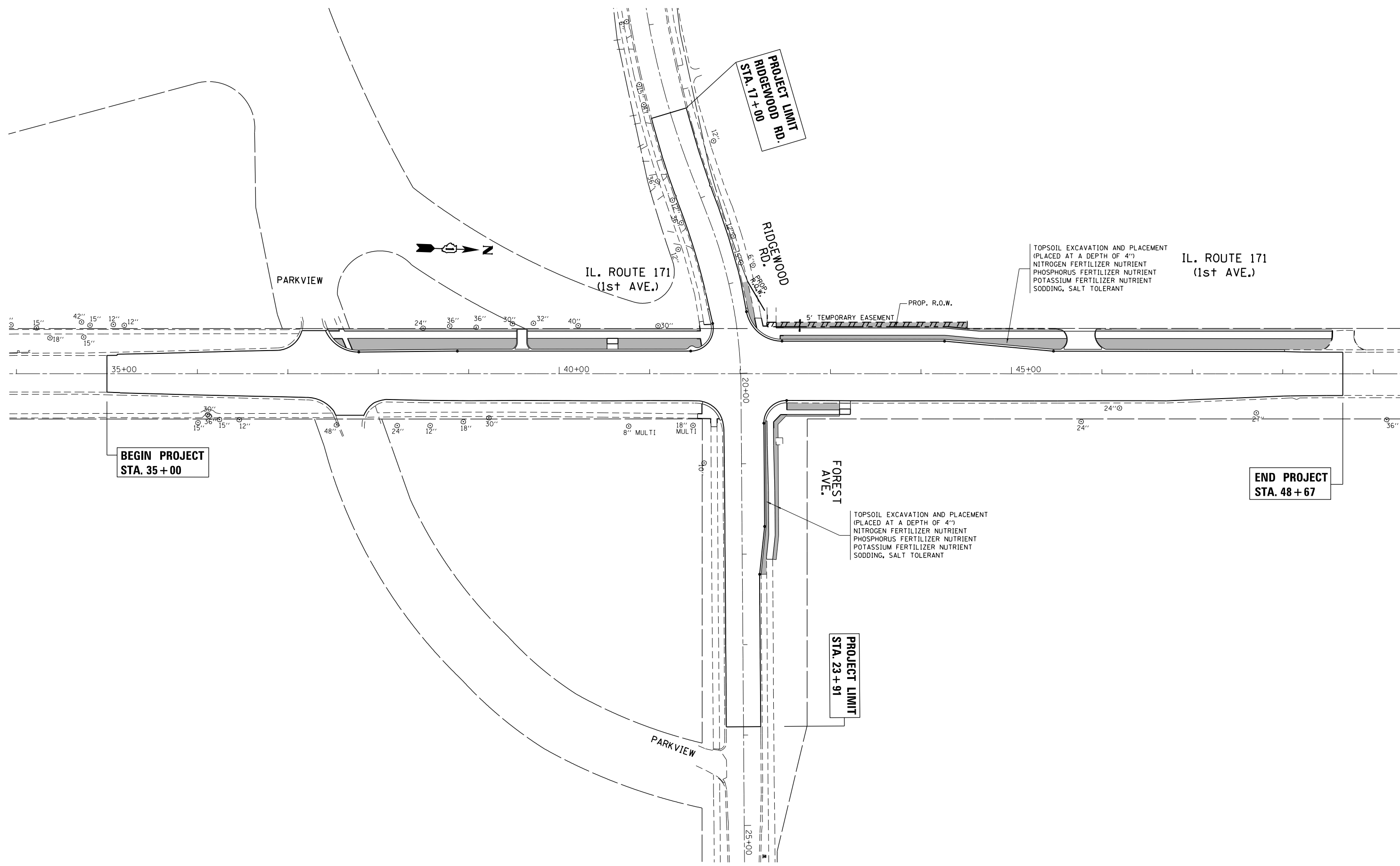
**LEGEND**

- (A) THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE) (10' DASH, 30' SKIP) (TYP.)
- (B) THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE) (TYP.)
- (C) THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW) (TYP.)
- (D) THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE) (TYP.)
- (E) THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE) (2' DASH, 6' SKIP) (TYP.)
- (F) THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE) (TYP.)
- (G) THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW) (TYP.)
- (H) THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE) (TYP.)
- (J) THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (TYP.)

NOTE: RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE INSTALLED PER DISTRICT STANDARD TC-11

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	PLOT DATE = 4/2/2014	DATE -	REVISED -			SCALE: 1"=50'	SHEET	OF	SHEETS	STA.	TO

ILLINOIS FED. AID PROJECT



FILE NAME =	USER NAME = Tariqfm	DESIGNED -	REVISED -
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 4/2/2014	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD.  
LANDSCAPING PLAN**

SCALE: 1"=50'      SHEET      OF      SHEETS      STA.      TO      STA.

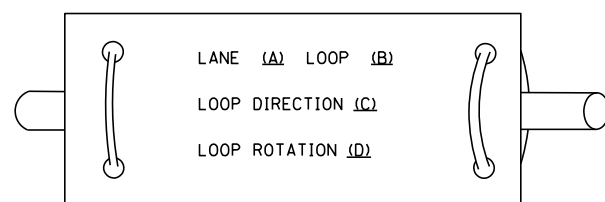
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372	03035-TS	COOK	87	42
CONTRACT NO. 60X08			ILLINOIS FED. AID PROJECT	



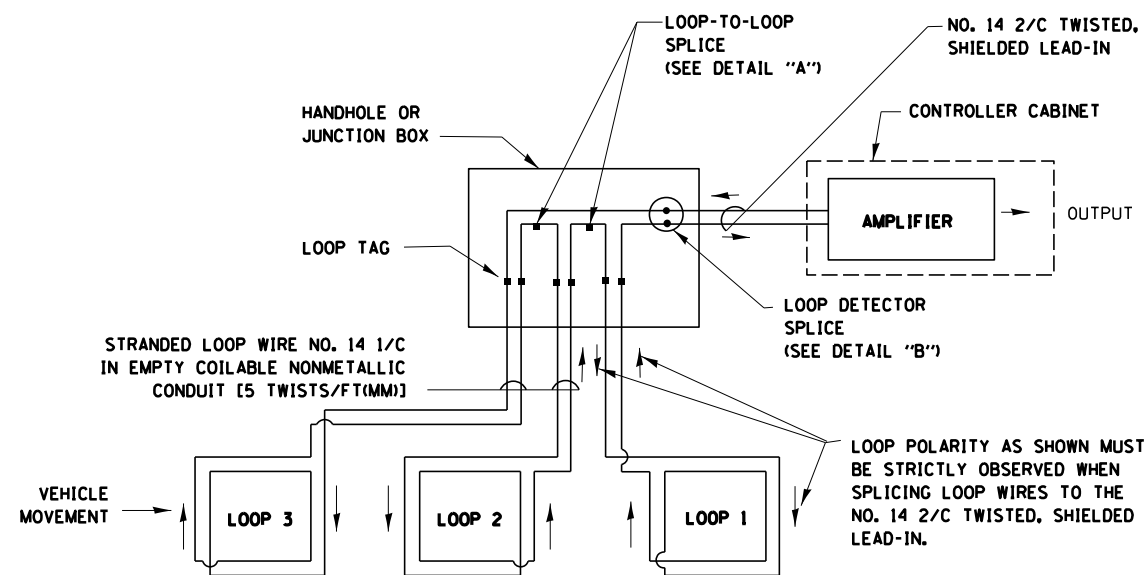
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

**LOOP LEAD-IN CABLE TAG**

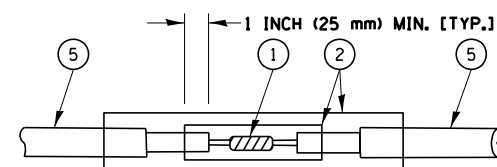


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

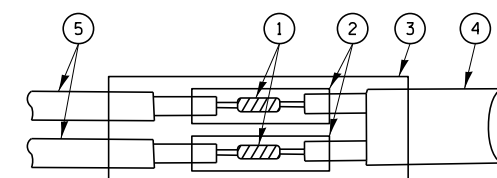


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

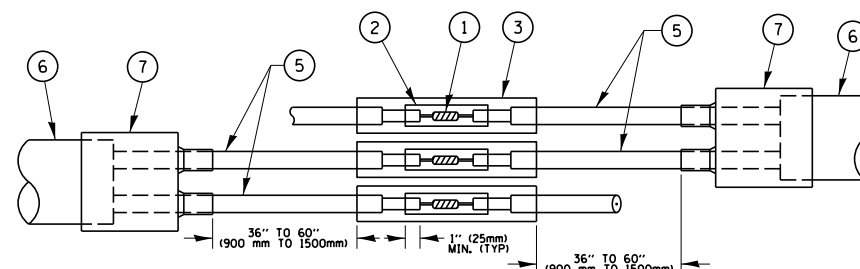


**DETAIL "A"  
LOOP-TO-LOOP SPLICE**

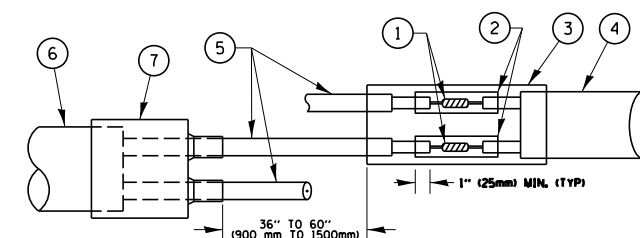


**DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE**

**TYPE I LOOP**



**DETAIL "A"  
LOOP-TO-LOOP SPLICE**



**DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE**

**LOOP DETECTOR SPLICE**

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH, THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

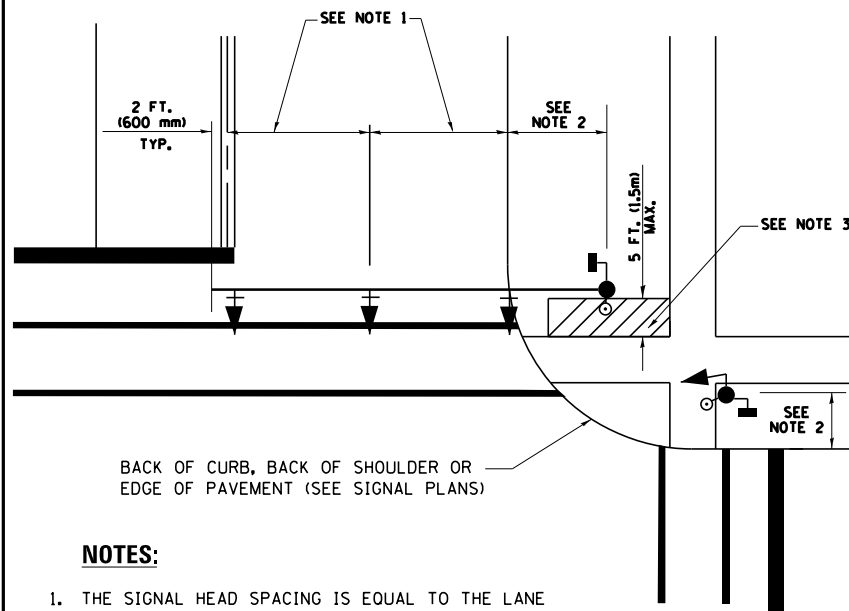
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PLOT SCALE = 100.0000' / 1in.		CHECKED - DAD	REVISED -
PLOT DATE = 3/28/2014		DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
STANDARD TRAFFIC SIGNAL DESIGN DETAILS			
SCALE: NONE	SHEET NO. 2 OF 7 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	44
TS-05		CONTRACT NO. 60X08		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

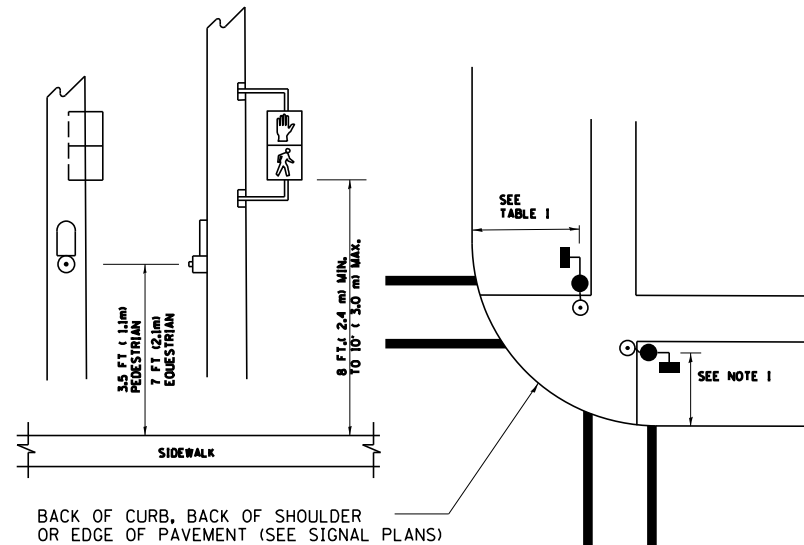
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST  
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR  
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN  
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.**



**NOTES:**

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

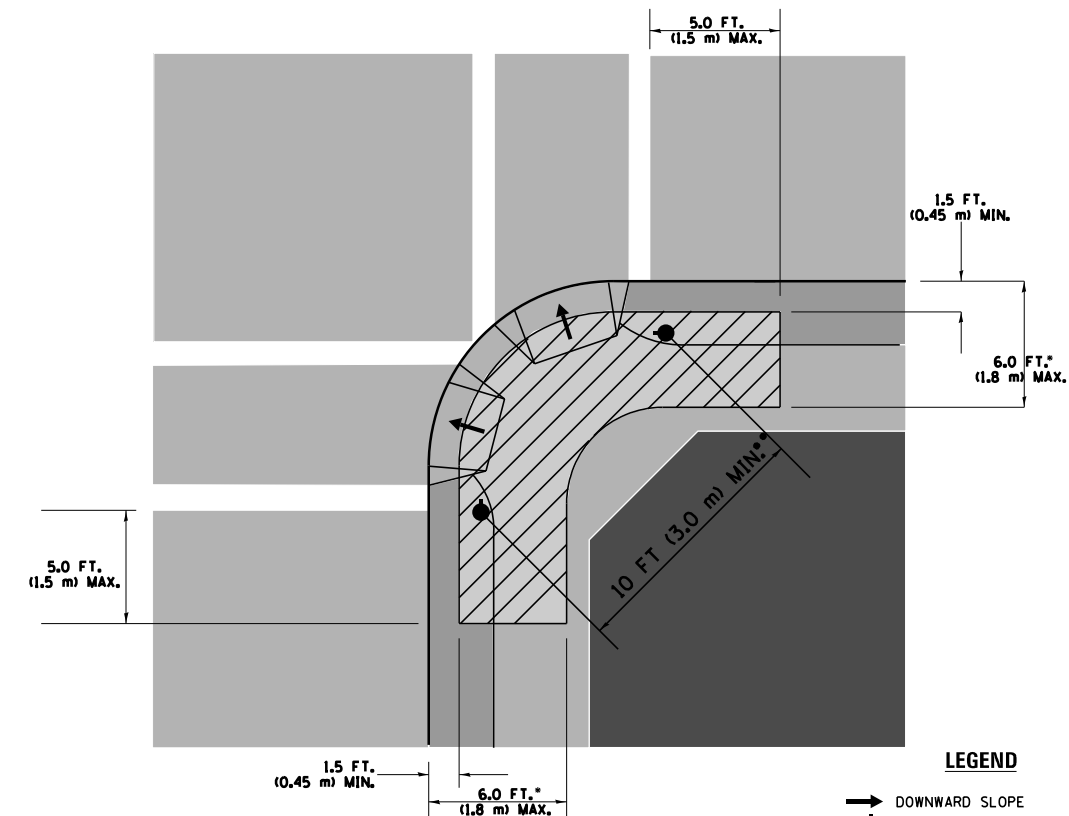
**PEDESTRIAN SIGNAL POST  
AND  
PEDESTRIAN PUSH BUTTON POST**



**NOTES:**

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

**RECOMMENDED PUSHBUTTON LOCATIONS**



**LEGEND**

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

**NOTES:**

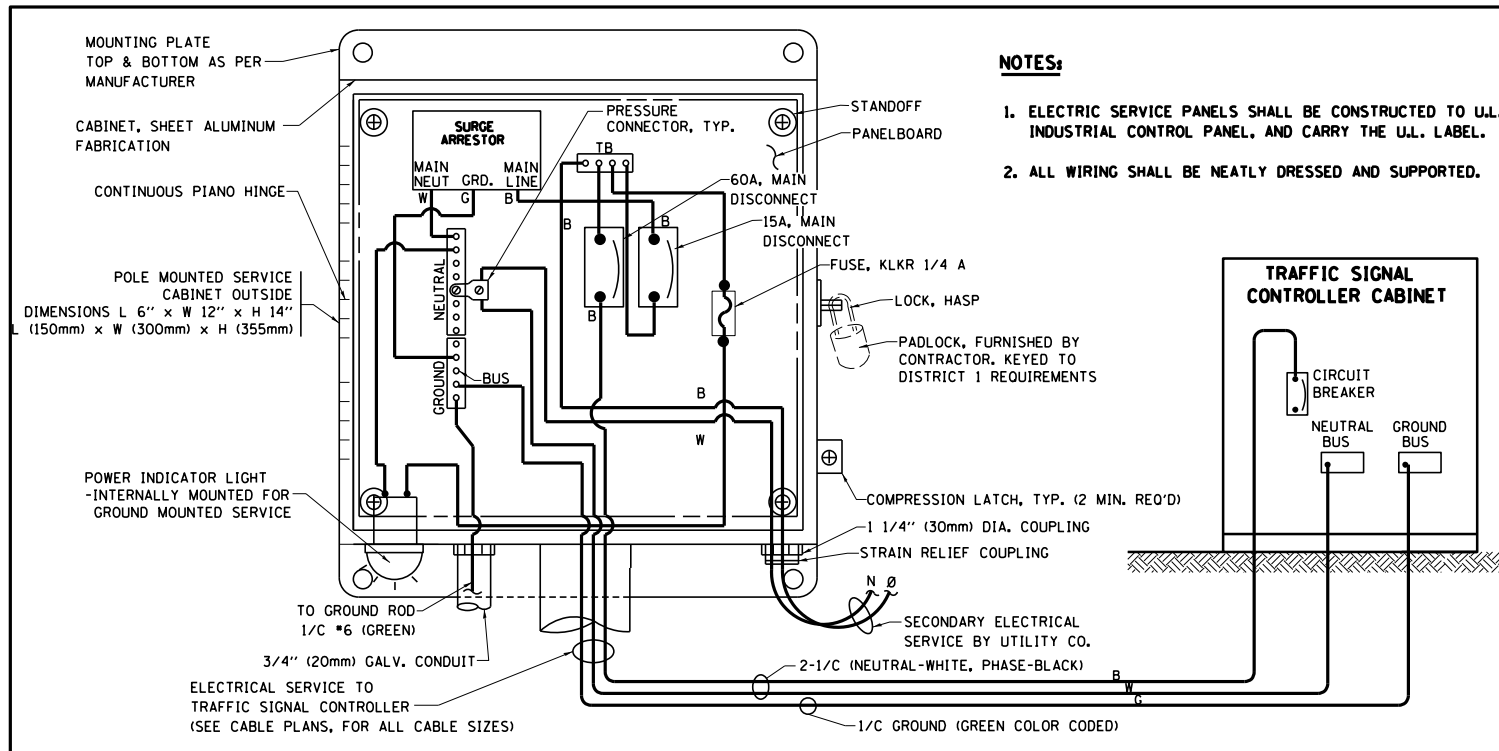
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

**TRAFFIC SIGNAL EQUIPMENT OFFSET**

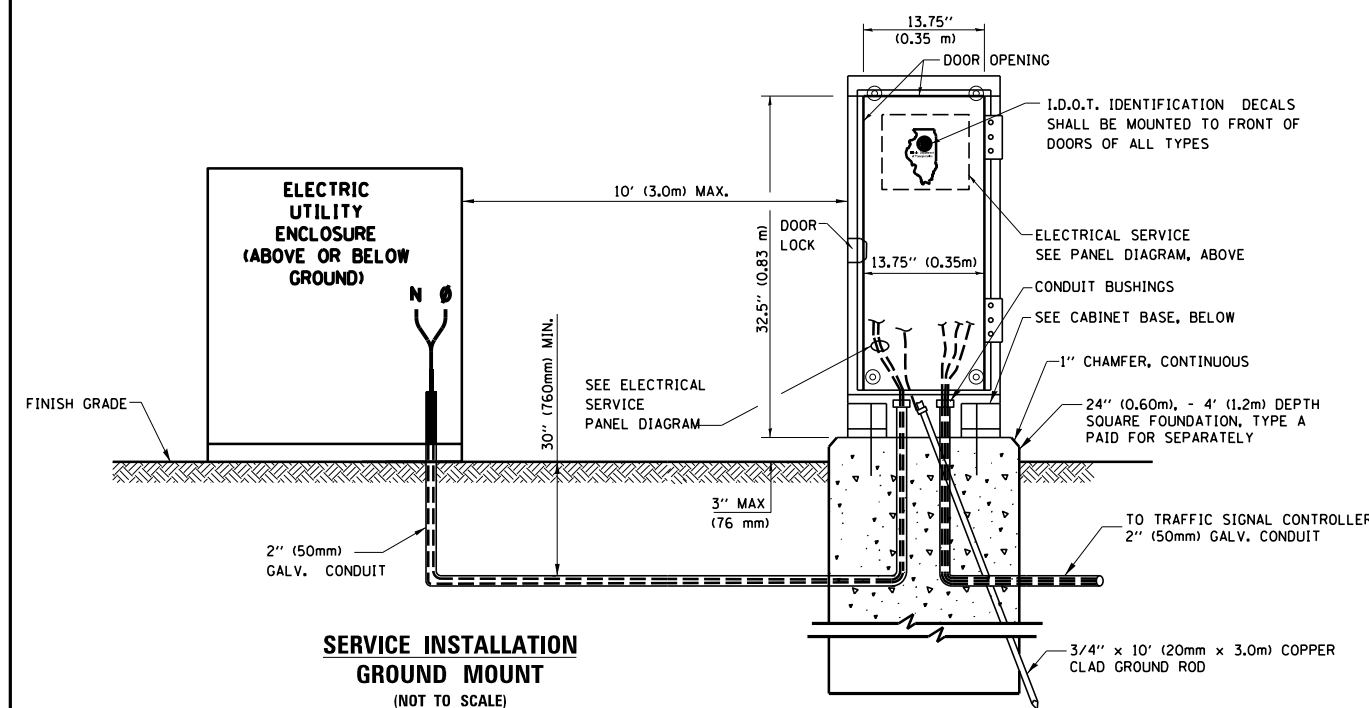
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

**NOTES:**

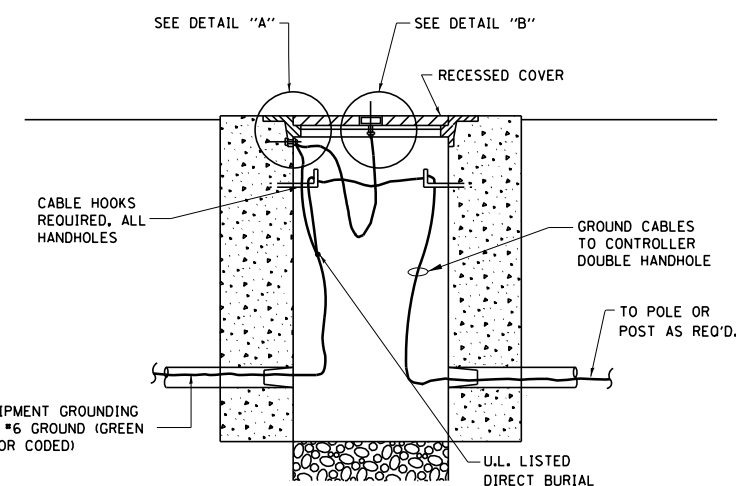
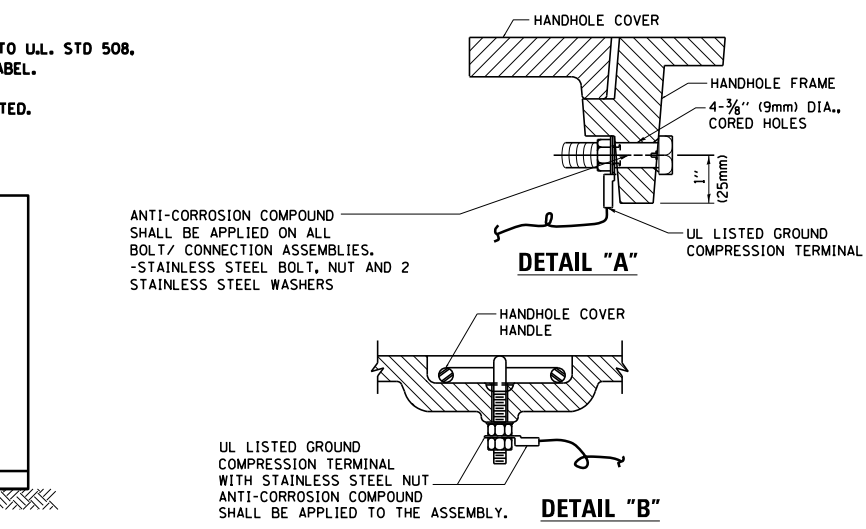
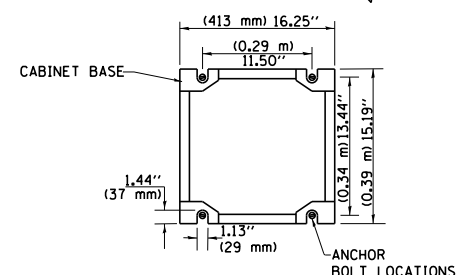
1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.



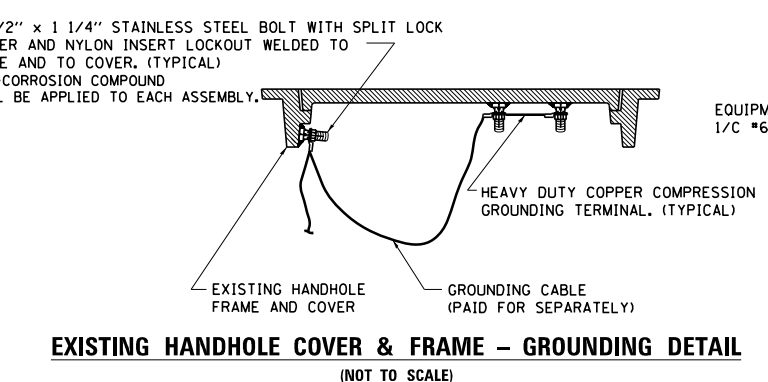
**ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)  
SERVICE INSTALLATION POLE MOUNT (SHOWN)  
(NOT TO SCALE)**



**CABINET - BASE BOLT PATTERN  
(NOT TO SCALE)**

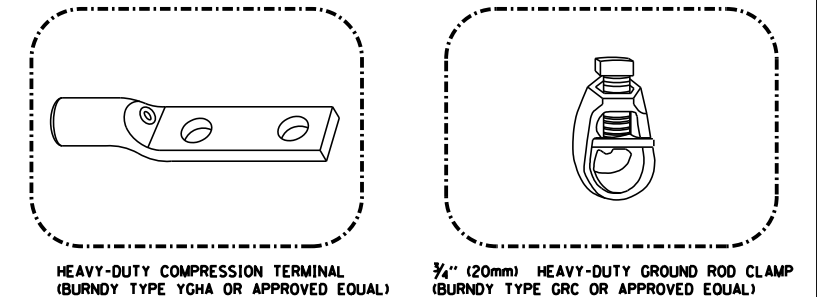


**HANDHOLE COVER & FRAME - GROUNDING DETAIL  
(NOT TO SCALE)**

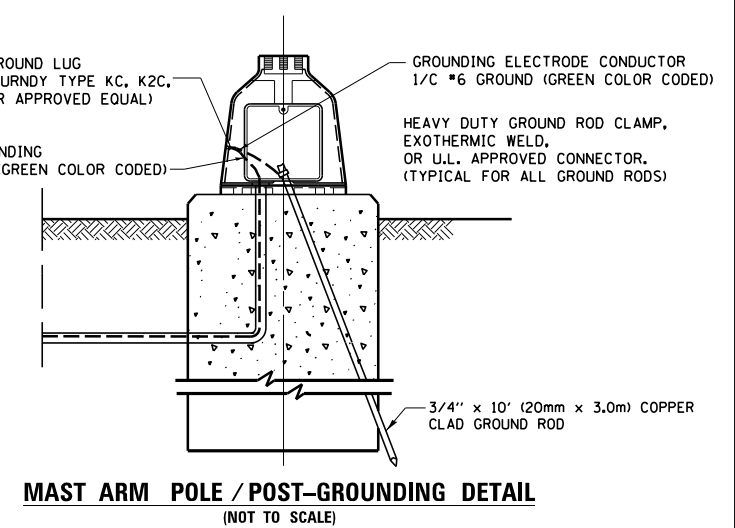


**EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL  
(NOT TO SCALE)**

- NOTES:**
- GROUNDING SYSTEM**
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
  2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
  3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
  4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

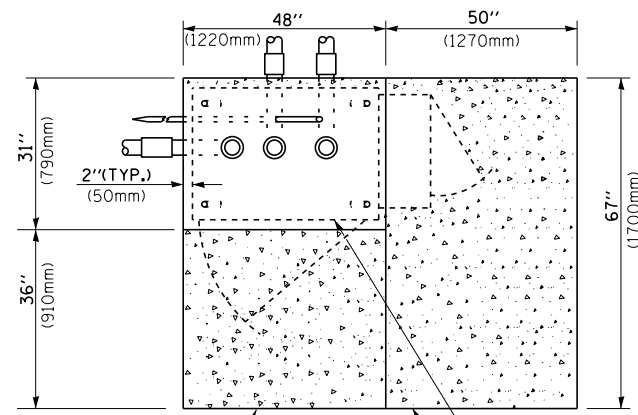


- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
  - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

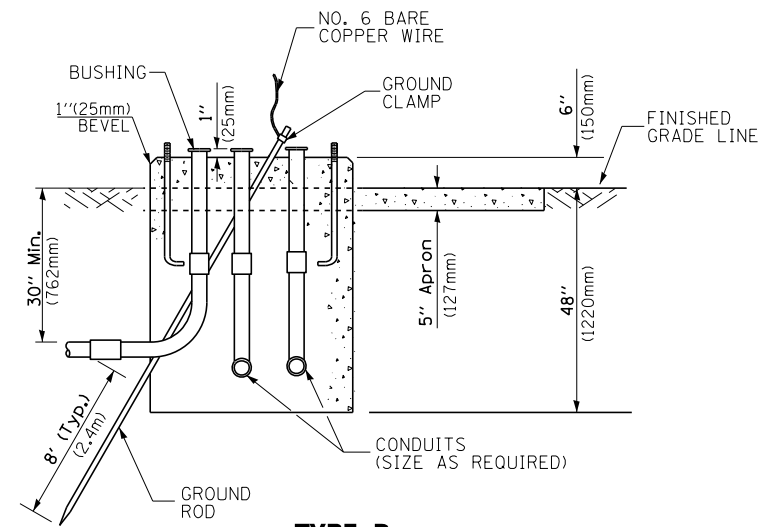


**MAST ARM POLE / POST-GROUNDING DETAIL  
(NOT TO SCALE)**

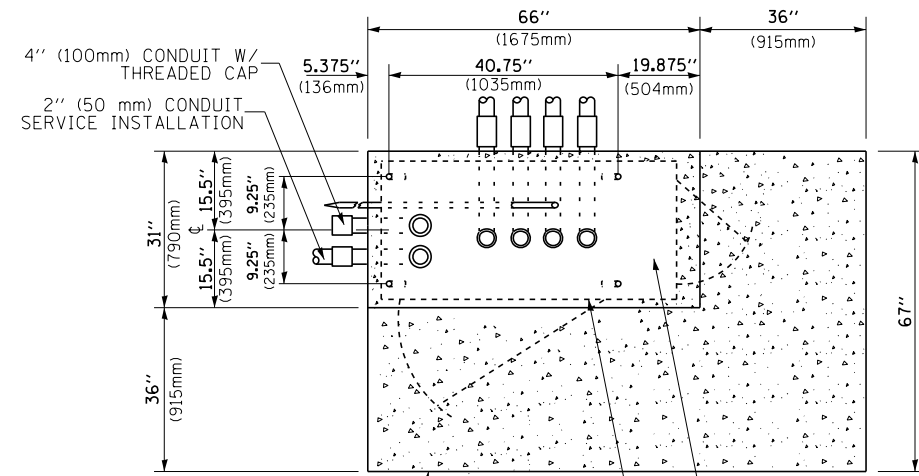
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ei:\pw_work\p\idot\plascencia\140370834\114412-TS.dgn	PLOT SCALE = 100.0000' / 1"	DRAWN - BCK	REVISED -		<b>STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>		<b>TS-05</b>		<b>CONTRACT NO. 60X08</b>		<b>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</b>	
	PLOT DATE = 3/28/2014	CHECKED - DAD	REVISED -		SCALE: NONE	SHEET NO. 4 OF 7 SHEETS	STA. TO STA.					
		DATE - 10-28-09	REVISED -									



**TOP VIEW**



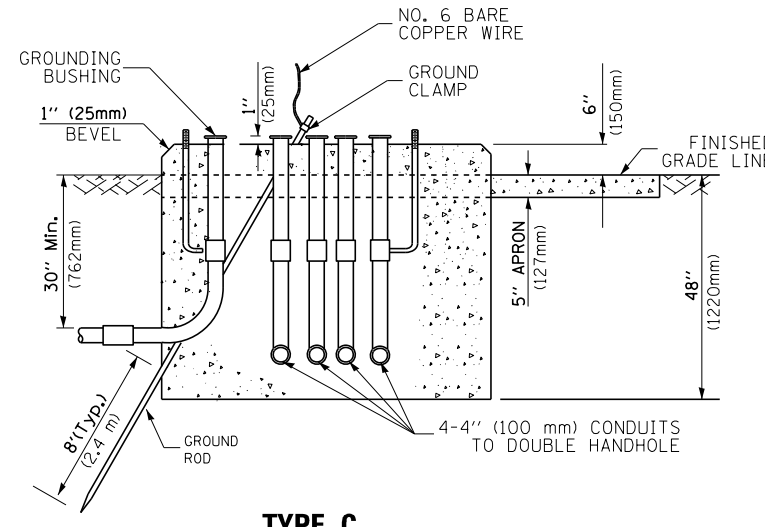
**TYPE D  
FOR GROUND MOUNTED  
CONTROLLER CABINET  
AND UPS BATTERY CABINET**



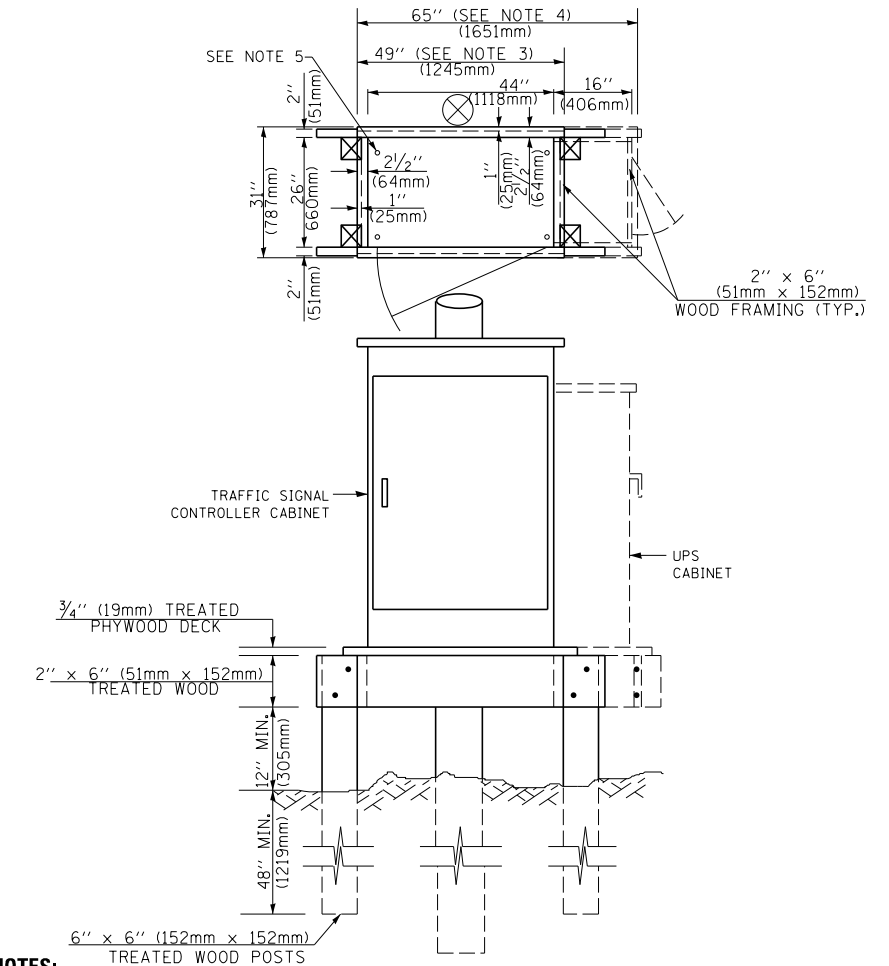
**TOP VIEW**

**NOTE:**

TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE C  
FOR GROUND MOUNTED  
SUPER P (TYPE IV) AND SUPER R (TYPE V)  
CONTROLLER CABINETS**



**NOTES:**

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER  
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

**CABLE SLACK**

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE ( MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

**VERTICAL CABLE LENGTH**

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

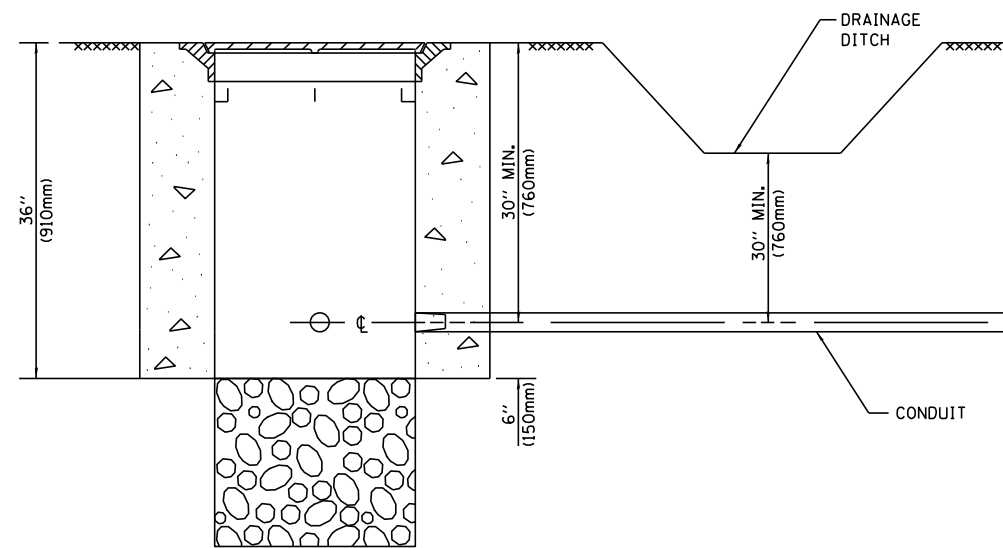
**DEPTH OF FOUNDATION**

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	24" (600mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and up to 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and less than 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m) and up to 85' (25.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

**NOTES:**

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average unconfined compressive strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001..

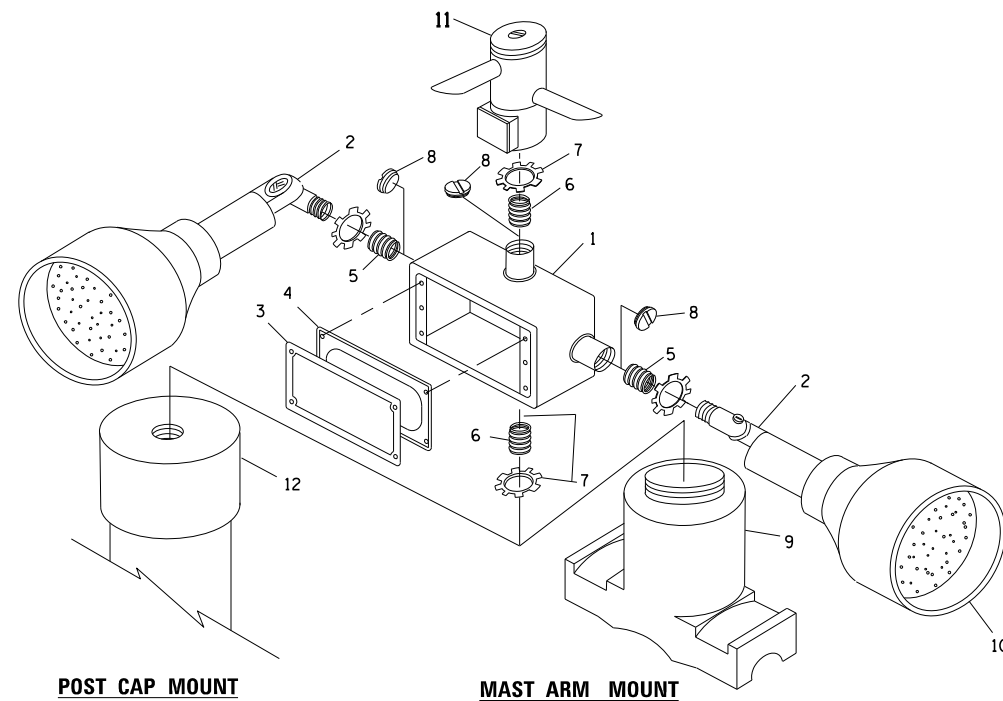
**DEPTH OF MAST ARM FOUNDATIONS, TYPE E**



**NOTES:**

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

**HANDHOLE WITH MINIMUM CONDUIT DEPTH**  
(NOT TO SCALE)

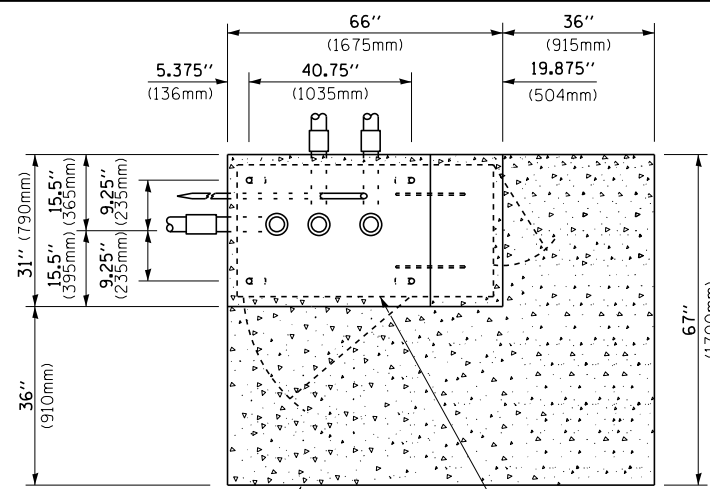


**POST CAP MOUNT**                      **MAST ARM MOUNT**  
**EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL**

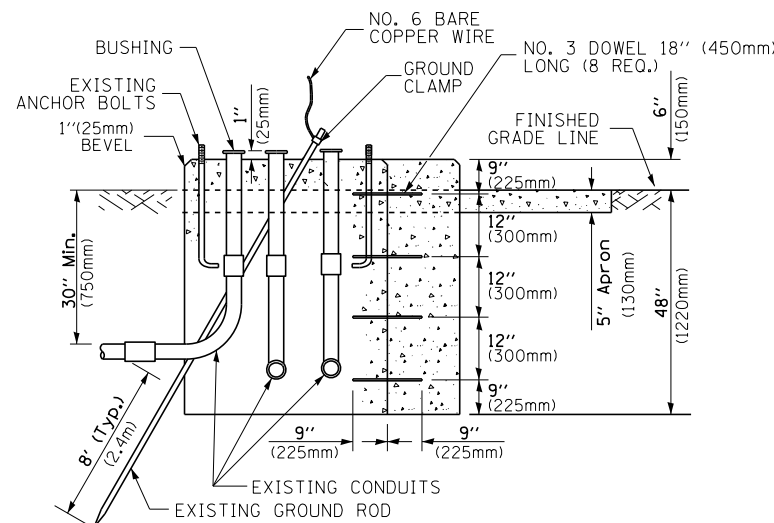
ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

**NOTES:**

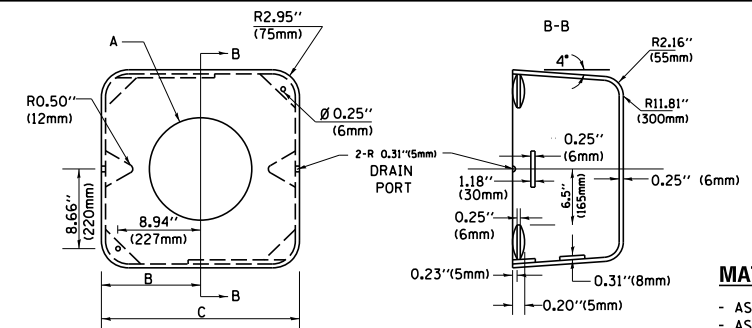
1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT  
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT  
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



**TOP VIEW**  
(NOT TO SCALE)



**MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION**  
(NOT TO SCALE)



**MATERIAL:**  
- ASTM A36 STEEL  
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIABLES	9.5" (241mm)	19" (483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIABLES	10.75" (273mm)	21.5" (546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIABLES	13.0" (330mm)	26" (660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIABLES	18.5" (470mm)	37" (940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

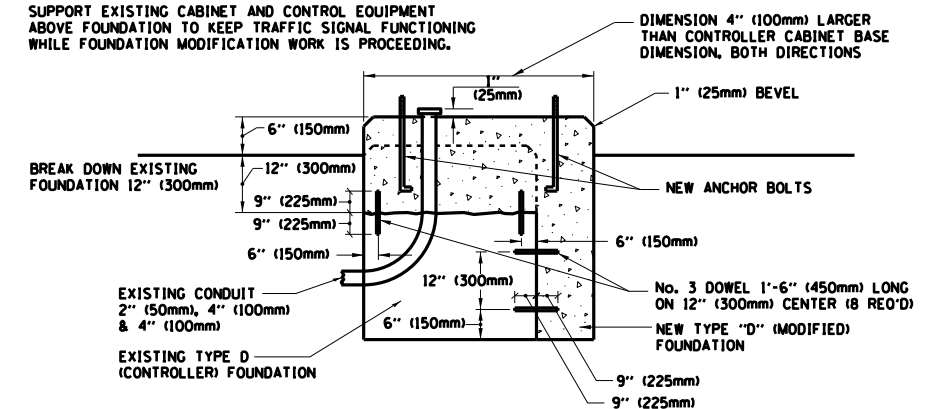
**SHROUD**

**NOTES:**

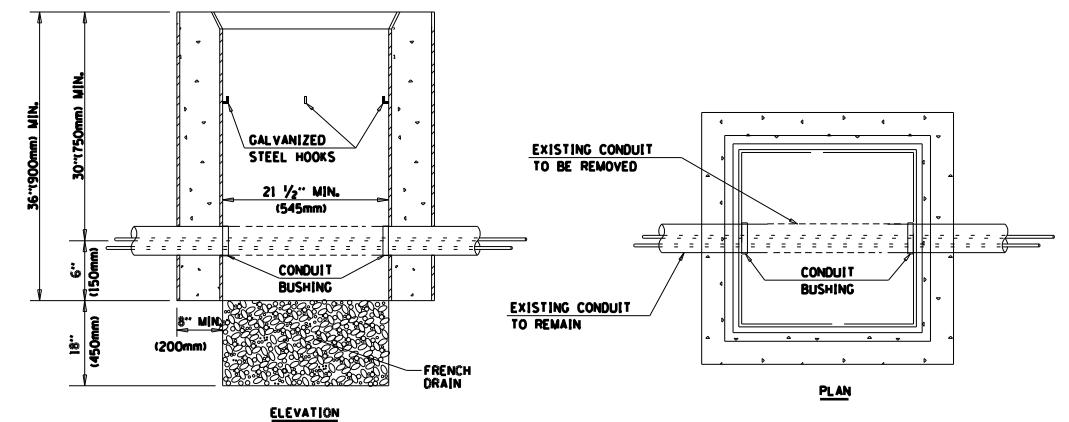
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

**NOTE:**

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



**MODIFY EXISTING TYPE "D" FOUNDATION**



**NOTES:**

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

**HANDHOLE TO INTERCEPT EXISTING CONDUIT**

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	PLOT DATE = 3/28/2014	DATE - 10-28-09	REVISED -

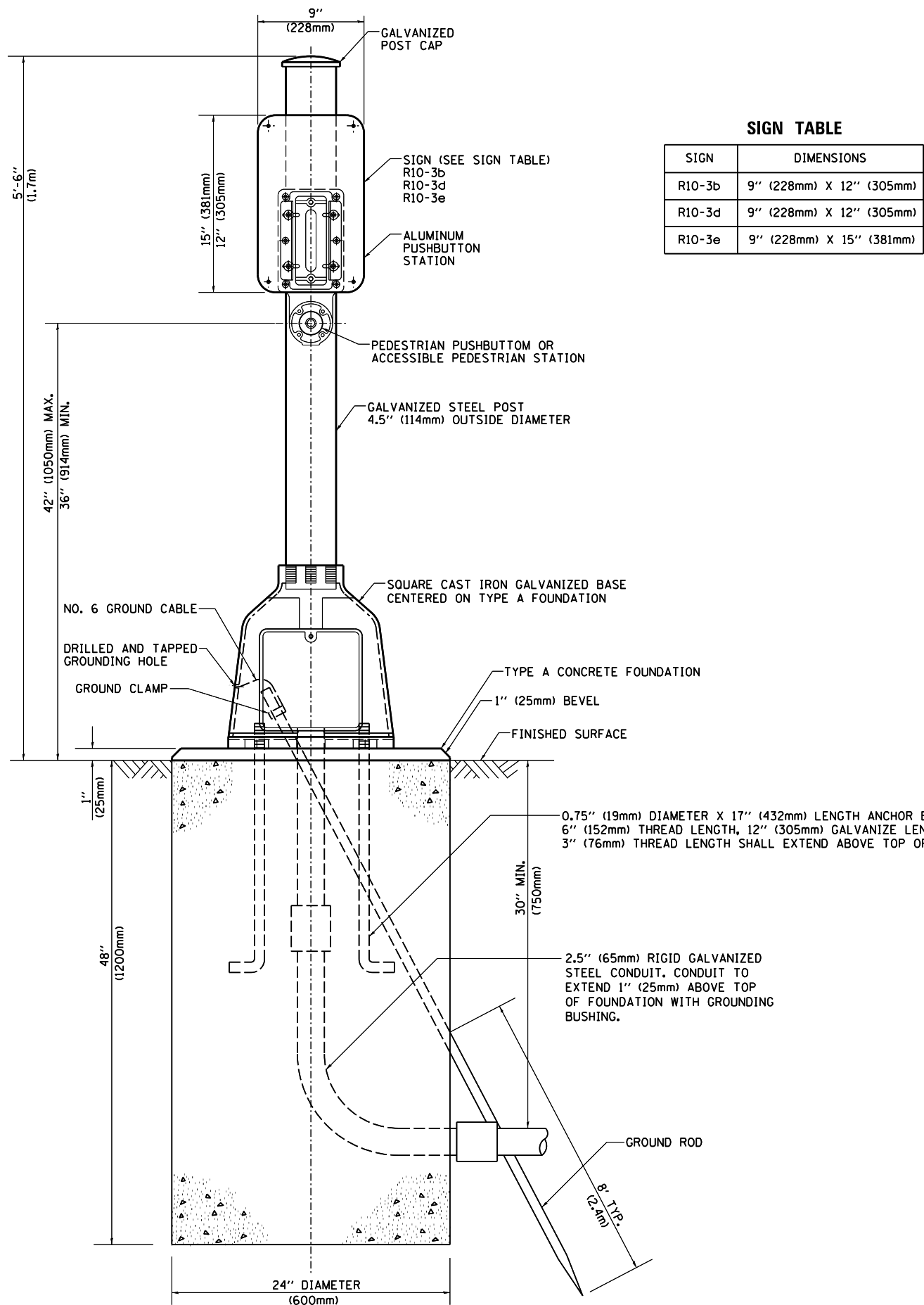
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE**  
**STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE      SHEET NO. 6 OF 7 SHEETS      STA.      TO STA.

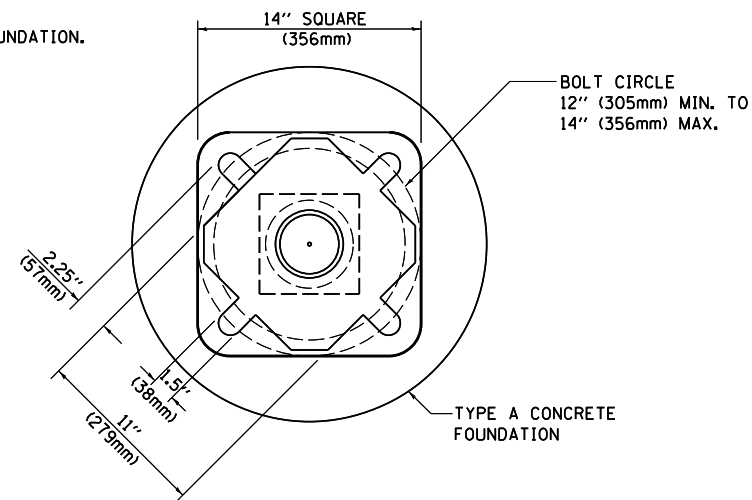
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	48
TS-05		CONTRACT NO. 60X08		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				





**SIGN TABLE**

SIGN	DIMENSIONS
R10-3b	9" (228mm) X 12" (305mm)
R10-3d	9" (228mm) X 12" (305mm)
R10-3e	9" (228mm) X 15" (381mm)



**BOLT PATTERN**

**PEDESTRIAN PUSH BUTTON POST, TYPE A**

FILE NAME =	USER NAME = plascencia	DESIGNED - DAG	REVISED - DAG 1-1-14
ca:\pwork\pwork\plascencia\14412-TS.dgn	14412-TS.dgn	DRAWN - GND	REVISED -
PLOT SCALE = 100.0000' / 1"		CHECKED - DAD	REVISED -
PLOT DATE = 3/28/2014		DATE - 10/1/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>			
SCALE: NONE	SHEET NO. 7 OF 7 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	49
TS-05			CONTRACT NO. 60X08	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

THE EXISTING SPAN WIRE TRAFFIC SIGNAL INSTALLATION SHALL REMAIN IN PLACE AND BE USED AS THE TEMPORARY TRAFFIC SIGNAL INSTALLATION FOR THIS CONTRACT.

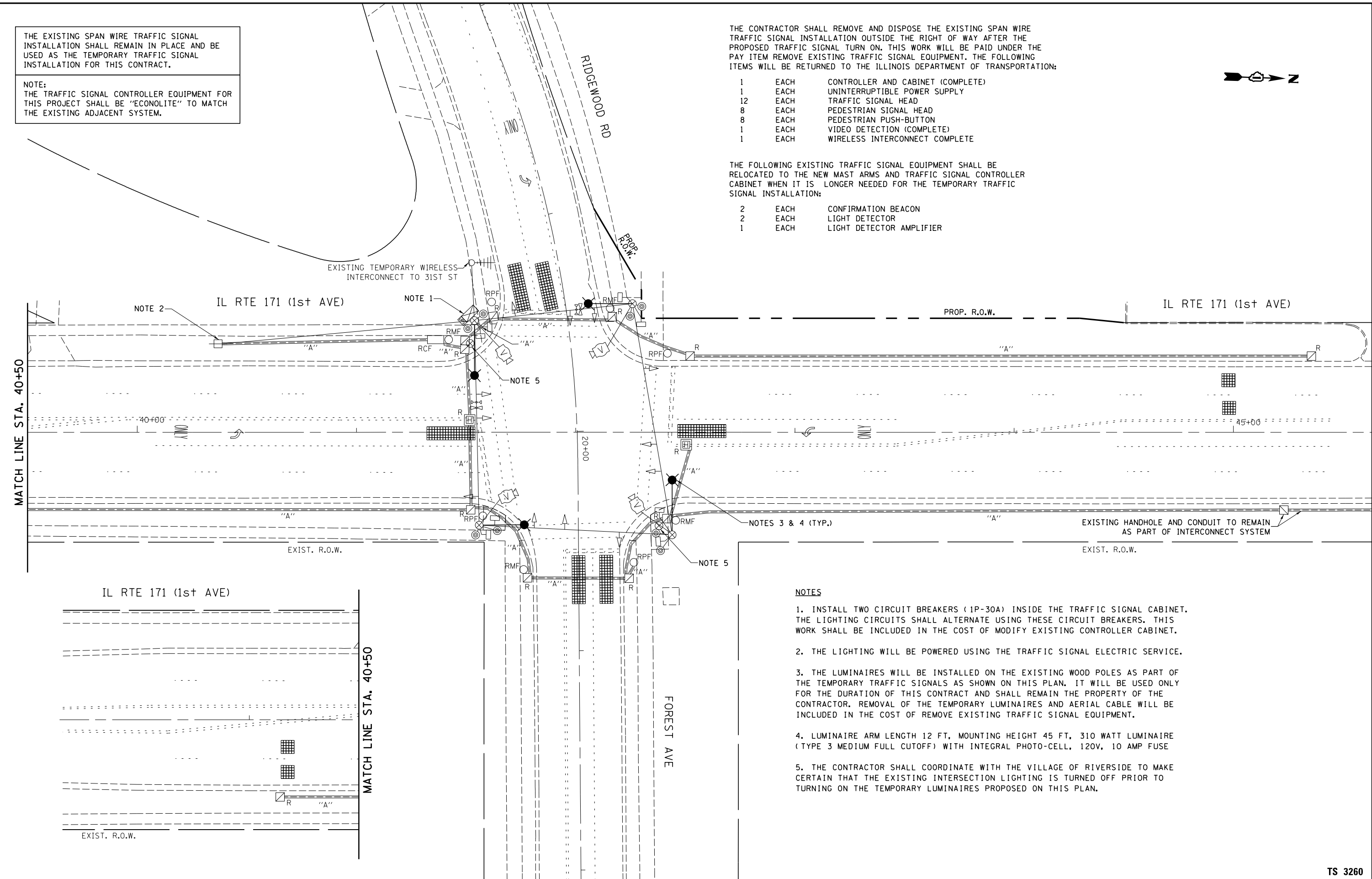
NOTE:  
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

THE CONTRACTOR SHALL REMOVE AND DISPOSE THE EXISTING SPAN WIRE TRAFFIC SIGNAL INSTALLATION OUTSIDE THE RIGHT OF WAY AFTER THE PROPOSED TRAFFIC SIGNAL TURN ON. THIS WORK WILL BE PAID UNDER THE PAY ITEM REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT. THE FOLLOWING ITEMS WILL BE RETURNED TO THE ILLINOIS DEPARTMENT OF TRANSPORTATION:

- 1 EACH CONTROLLER AND CABINET (COMPLETE)
- 1 EACH UNINTERRUPTIBLE POWER SUPPLY
- 12 EACH TRAFFIC SIGNAL HEAD
- 8 EACH PEDESTRIAN SIGNAL HEAD
- 8 EACH PEDESTRIAN PUSH-BUTTON
- 1 EACH VIDEO DETECTION (COMPLETE)
- 1 EACH WIRELESS INTERCONNECT COMPLETE

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE RELOCATED TO THE NEW MAST ARMS AND TRAFFIC SIGNAL CONTROLLER CABINET WHEN IT IS LONGER NEEDED FOR THE TEMPORARY TRAFFIC SIGNAL INSTALLATION:

- 2 EACH CONFIRMATION BEACON
- 2 EACH LIGHT DETECTOR
- 1 EACH LIGHT DETECTOR AMPLIFIER



**NOTES**

1. INSTALL TWO CIRCUIT BREAKERS (1P-30A) INSIDE THE TRAFFIC SIGNAL CABINET. THE LIGHTING CIRCUITS SHALL ALTERNATE USING THESE CIRCUIT BREAKERS. THIS WORK SHALL BE INCLUDED IN THE COST OF MODIFY EXISTING CONTROLLER CABINET.
2. THE LIGHTING WILL BE POWERED USING THE TRAFFIC SIGNAL ELECTRIC SERVICE.
3. THE LUMINAIRES WILL BE INSTALLED ON THE EXISTING WOOD POLES AS PART OF THE TEMPORARY TRAFFIC SIGNALS AS SHOWN ON THIS PLAN. IT WILL BE USED ONLY FOR THE DURATION OF THIS CONTRACT AND SHALL REMAIN THE PROPERTY OF THE CONTRACTOR. REMOVAL OF THE TEMPORARY LUMINAIRES AND AERIAL CABLE WILL BE INCLUDED IN THE COST OF REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT.
4. LUMINAIRE ARM LENGTH 12 FT, MOUNTING HEIGHT 45 FT, 310 WATT LUMINAIRE (TYPE 3 MEDIUM FULL CUTOFF) WITH INTEGRAL PHOTO-CELL, 120V, 10 AMP FUSE
5. THE CONTRACTOR SHALL COORDINATE WITH THE VILLAGE OF RIVERSIDE TO MAKE CERTAIN THAT THE EXISTING INTERSECTION LIGHTING IS TURNED OFF PRIOR TO TURNING ON THE TEMPORARY LUMINAIRES PROPOSED ON THIS PLAN.

FILE NAME =	USER NAME = plascencia	DESIGNED - IP	REVISED -
et:\pw\work\p\idot\plascencia\14412-TS.dgn		DRAWN - IP	REVISED -
Default	PLOT SCALE = 40.0000' / in.	CHECKED - LP	REVISED -
	PLOT DATE = 3/28/2014	DATE - 3/15/14	REVISED -

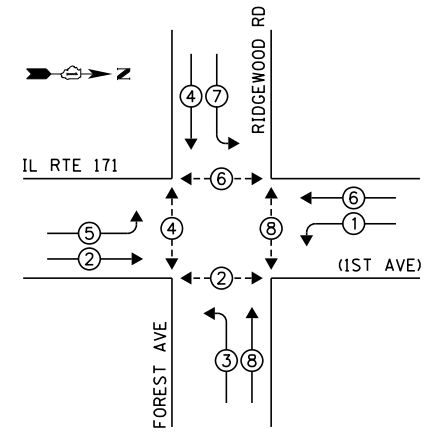
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL AND  
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN  
IL RTE 171 (1ST AVE) AND FOREST AVE / RIDGEWOOD RD**

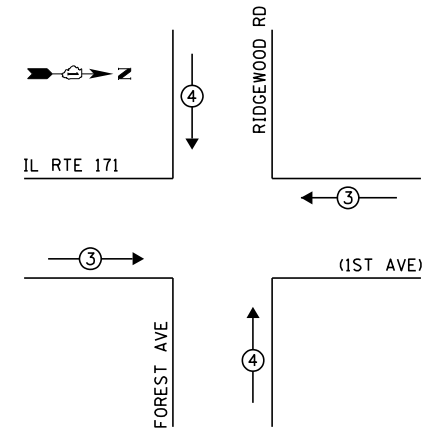
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	50
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60X08	

**TS 3260**



**PHASE DESIGNATION DIAGRAM**



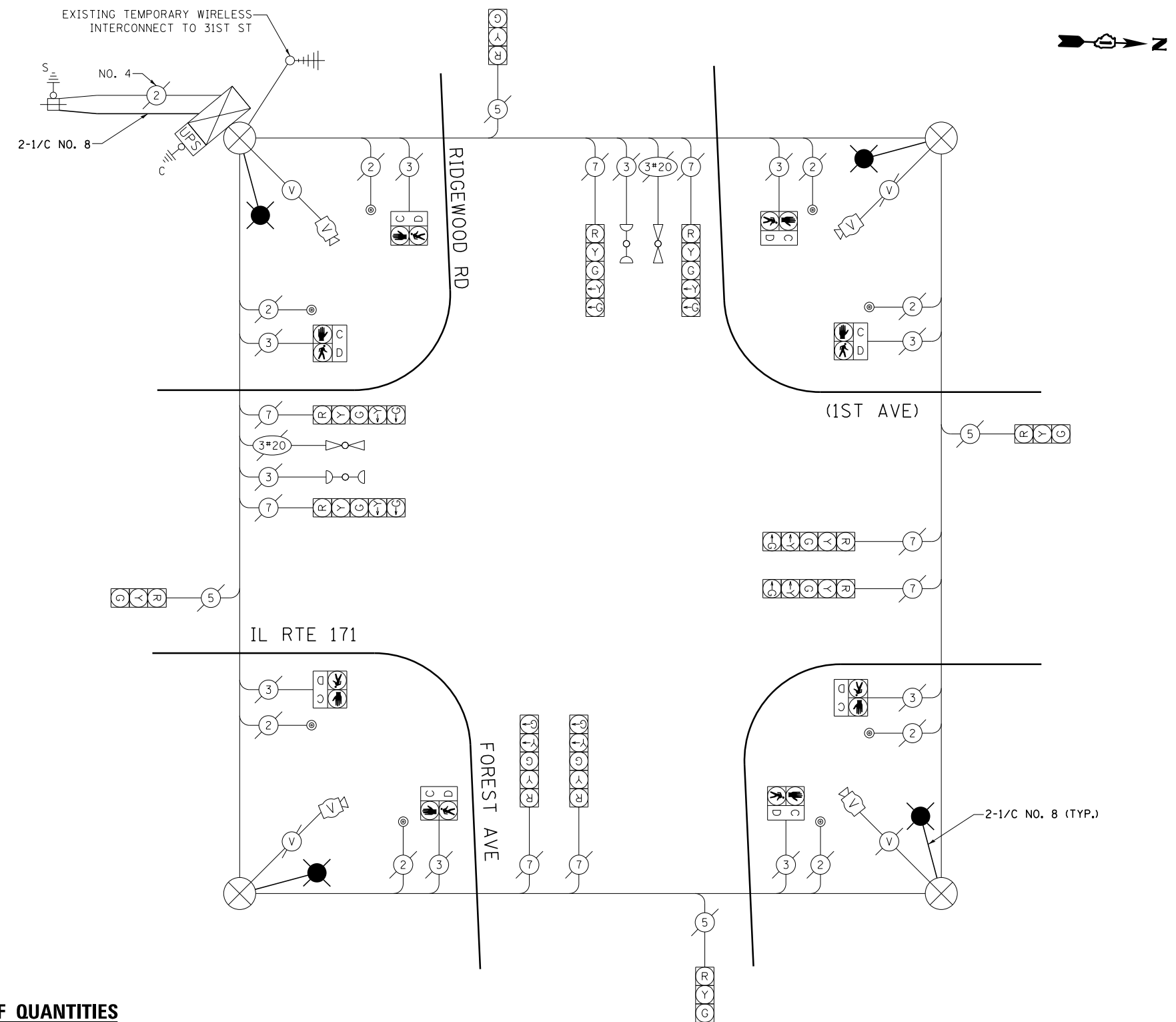
**EMERGENCY VEHICLE PREEMPTION SEQUENCE**

EXISTING EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	—	

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. OF LAMPS	WATTAGE		% OPERATION	
SIGNAL (RED)	12	INCAND.	17	0.50	102.0
(YELLOW)	12		25	0.25	75.0
(GREEN)	12		15	0.25	45.0
ARROW	16		12	0.10	19.2
PED. SIGNAL	8		25	1.00	200.0
CONTROLLER	1		100	1.00	100.0
ILLUM. SIGN	-		25	0.05	-
VIDEO SYSTEM	1		150	1.00	150.0
LUMINAIRE	4	310		0.50	620.0
FLASHER				0.50	
ENERGY COSTS TO:				TOTAL =	1311.2

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAY/DISTRICT 1  
 201 WEST CENTER COURT/SCHAUMBURG, ILLINOIS 60196-1096  
 ENERGY SUPPLY: CONTACT: JOE STACHO  
 PHONE: (630) 424-5704  
 COMPANY: COMMONWEALTH EDISON

- LEGEND:**
- ← \* → SINGLE ENTRY PHASE
  - ← ⊕ → DUAL ENTRY PHASE
  - ← ⊕ → PEDESTRIAN PHASE



**EX. SPAN WIRE CABLE PLAN**  
(NOT TO SCALE)

**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY.
AERIAL CABLE, 2-1/C NO. 8 WITH MESSENGER WIRE	FOOT	1425
LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, PHOTO-CELL CONTROL, 310 WATT	EACH	4
MODIFY EXISTING CONTROLLER CABINET	EACH	1

NOTE:  
 THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

FILE NAME =	USER NAME = plascencia	DESIGNED - IP	REVISED -
et:\pw_work\p\idot\plascencia\14412-TS.dgn		DRAWN - IP	REVISED -
Default	PLOT SCALE = 40.0000' / in.	CHECKED - LP	REVISED -
	PLOT DATE = 3/28/2014	DATE - 3/15/14	REVISED -

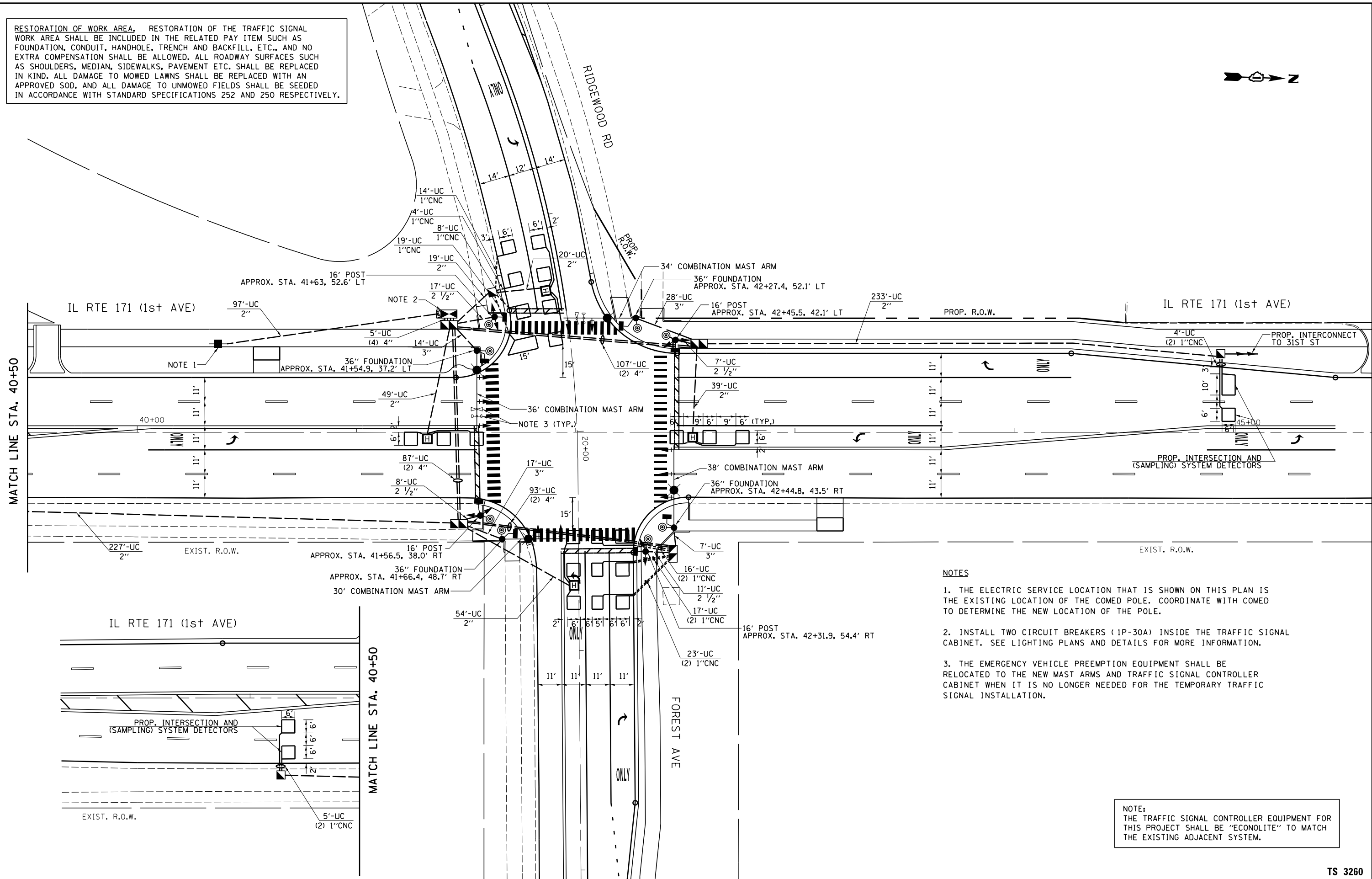
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN, TEMPORARY PHASE DESIGNATION DIAGRAM,  
 AND TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE  
 IL RTE 171 (1ST AVE) AND FOREST AVE / RIDGEWOOD RD  
 SCALE: NTS SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	51
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				

TS 3260

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



**NOTES**

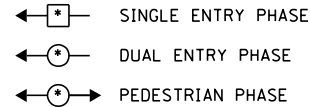
1. THE ELECTRIC SERVICE LOCATION THAT IS SHOWN ON THIS PLAN IS THE EXISTING LOCATION OF THE COMED POLE. COORDINATE WITH COMED TO DETERMINE THE NEW LOCATION OF THE POLE.
2. INSTALL TWO CIRCUIT BREAKERS (1P-30A) INSIDE THE TRAFFIC SIGNAL CABINET. SEE LIGHTING PLANS AND DETAILS FOR MORE INFORMATION.
3. THE EMERGENCY VEHICLE PREEMPTION EQUIPMENT SHALL BE RELOCATED TO THE NEW MAST ARMS AND TRAFFIC SIGNAL CONTROLLER CABINET WHEN IT IS NO LONGER NEEDED FOR THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.

NOTE:  
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

FILE NAME = c:\pwwork\p\dot\plascencia\14412-TS.dgn	USER NAME = plascencia	DESIGNED - IP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC SIGNAL MODERNIZATION PLAN IL RTE 171 (1ST AVE) AND FOREST AVE / RIDGEWOOD RD</b>	F.A.P. RTE. 372	SECTION 03035-TS	COUNTY COOK	TOTAL SHEETS 87	SHEET NO. 52	<b>CONTRACT NO. 60X08</b>
	PLOT SCALE = 40.0000' / in.	CHECKED - LP	REVISED -			SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT		
Default	PLOT DATE = 3/28/2014	DATE - 3/15/14	REVISED -								

TS 3260

**LEGEND:**

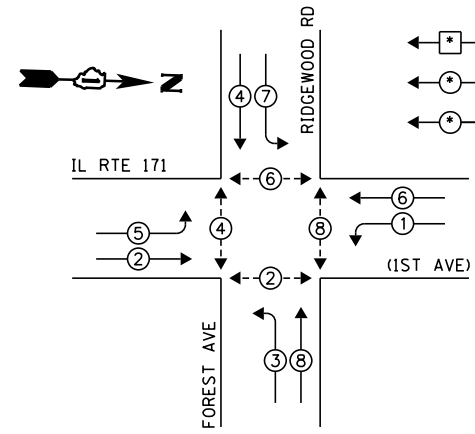


**SCHEDULE OF QUANTITIES**

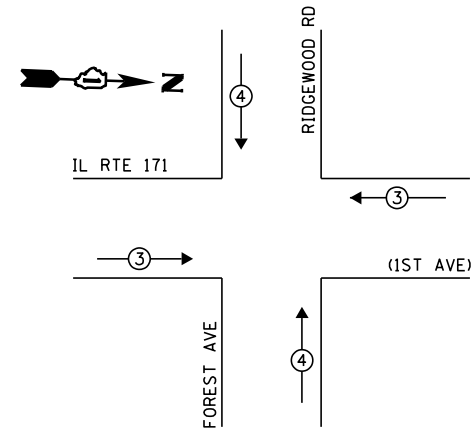
ITEM DESCRIPTION	UNIT	TOTAL QTY.
SIGN PANEL - TYPE 2	SO FT	60
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	740
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	45
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	70
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	600
HANDHOLE	EACH	3
HEAVY-DUTY HANDHOLE	EACH	4
DOUBLE HANDHOLE	EACH	4
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1,250
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1,600
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1,775
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1,475
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	2,525
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	125
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	1100
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	4
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 30 FT.	EACH	1
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 34 FT.	EACH	1
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 36 FT.	EACH	1
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 38 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	16
CONCRETE FOUNDATION, TYPE C	FOOT	4
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	44
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	6
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4
SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	4
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8
TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	10
INDUCTIVE LOOP DETECTOR	EACH	11
DETECTOR LOOP, TYPE I	FOOT	910
PEDESTRIAN PUSH-BUTTON	EACH	8
* RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT	EACH	2
* RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	8
REMOVE EXISTING DOUBLE HANDHOLE	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	9
* EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	320
** FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1
UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1
ELECTRIC SERVICE DISCONNECT, LIGHTING AND TRAFFIC SIGNAL	EACH	1
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1

\* 100% COST TO THE VILLAGE OF RIVERSIDE  
 \*\* SUPER P CABINET

NOTE:  
 THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.



**PHASE DESIGNATION DIAGRAM**

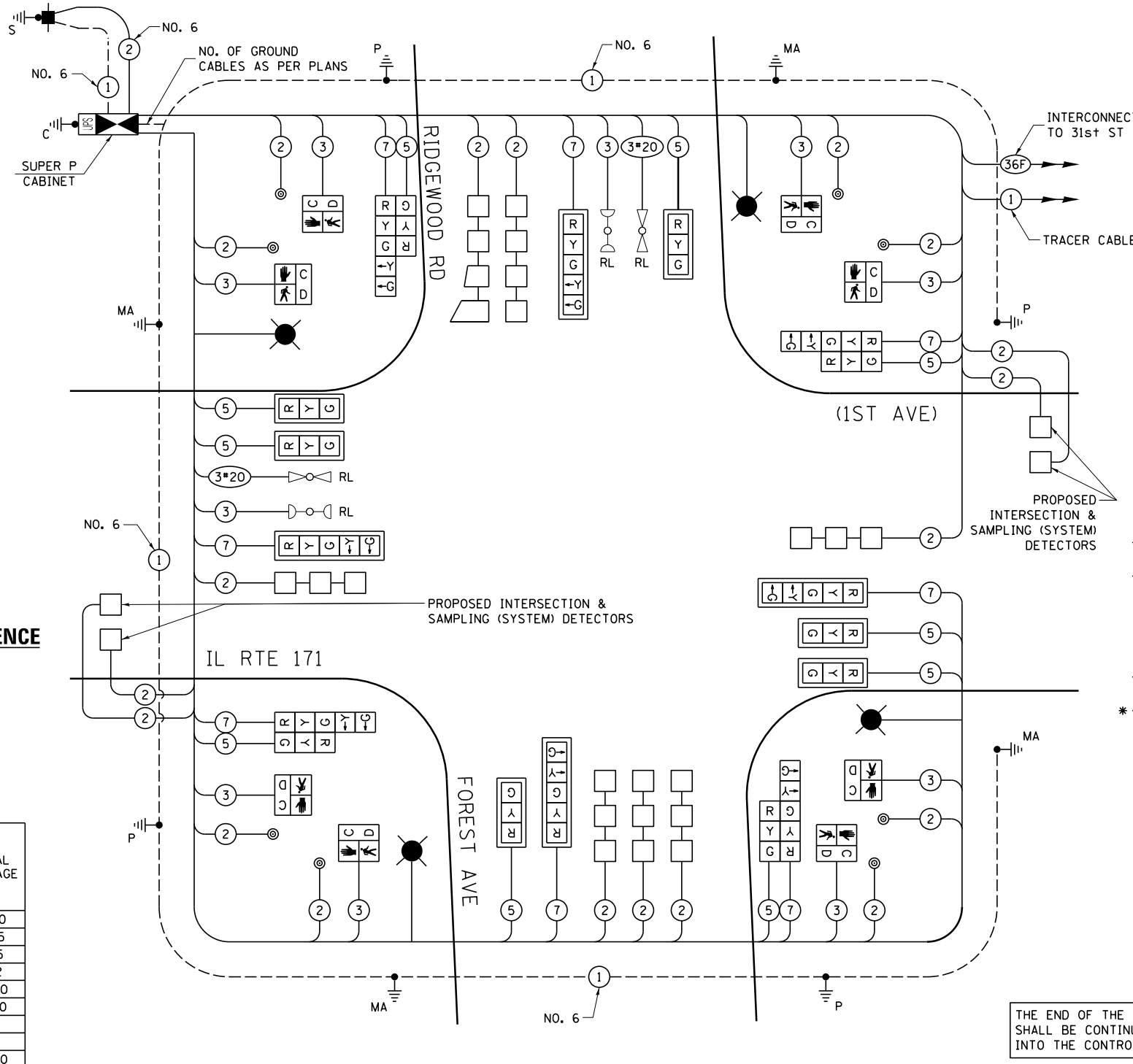


**EMERGENCY VEHICLE PREEMPTION SEQUENCE**

PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	—	

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. OF LAMPS	WATTAGE		% OPERATION	
SIGNAL (RED)	18	INCAND.	17	0.50	153.0
(YELLOW)	18		25	0.25	112.5
(GREEN)	18		15	0.25	67.5
ARROW	16		12	0.10	19.2
PED. SIGNAL	8		25	1.00	200.0
CONTROLLER	1		100	1.00	100.0
ILLUM. SIGN	-		25	0.05	-
VIDEO SYSTEM	-		150	1.00	-
LUMINAIRE	4		310	0.50	620.0
FLASHER				0.50	
ENERGY COSTS TO:	TOTAL =				1272.2

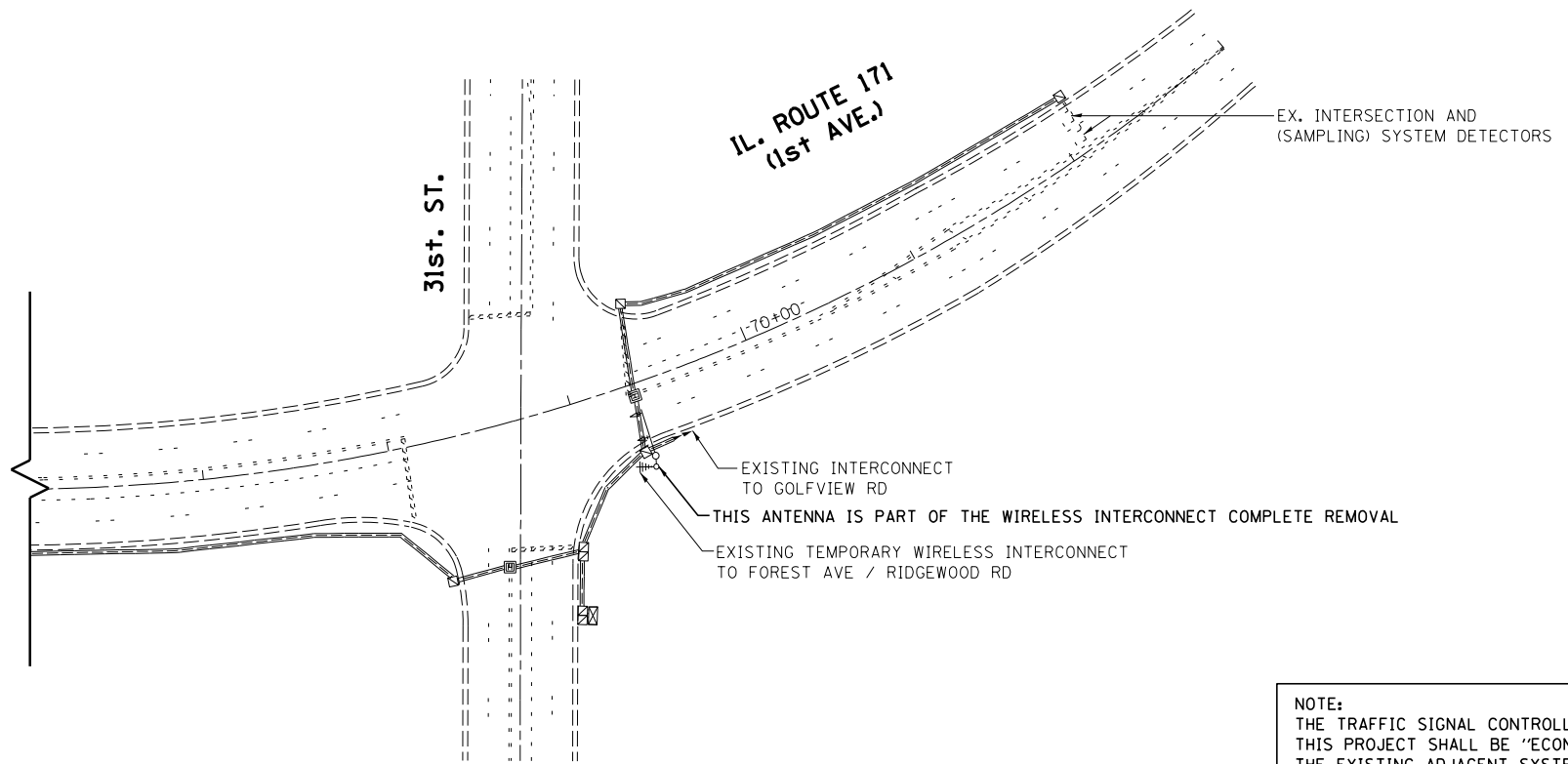
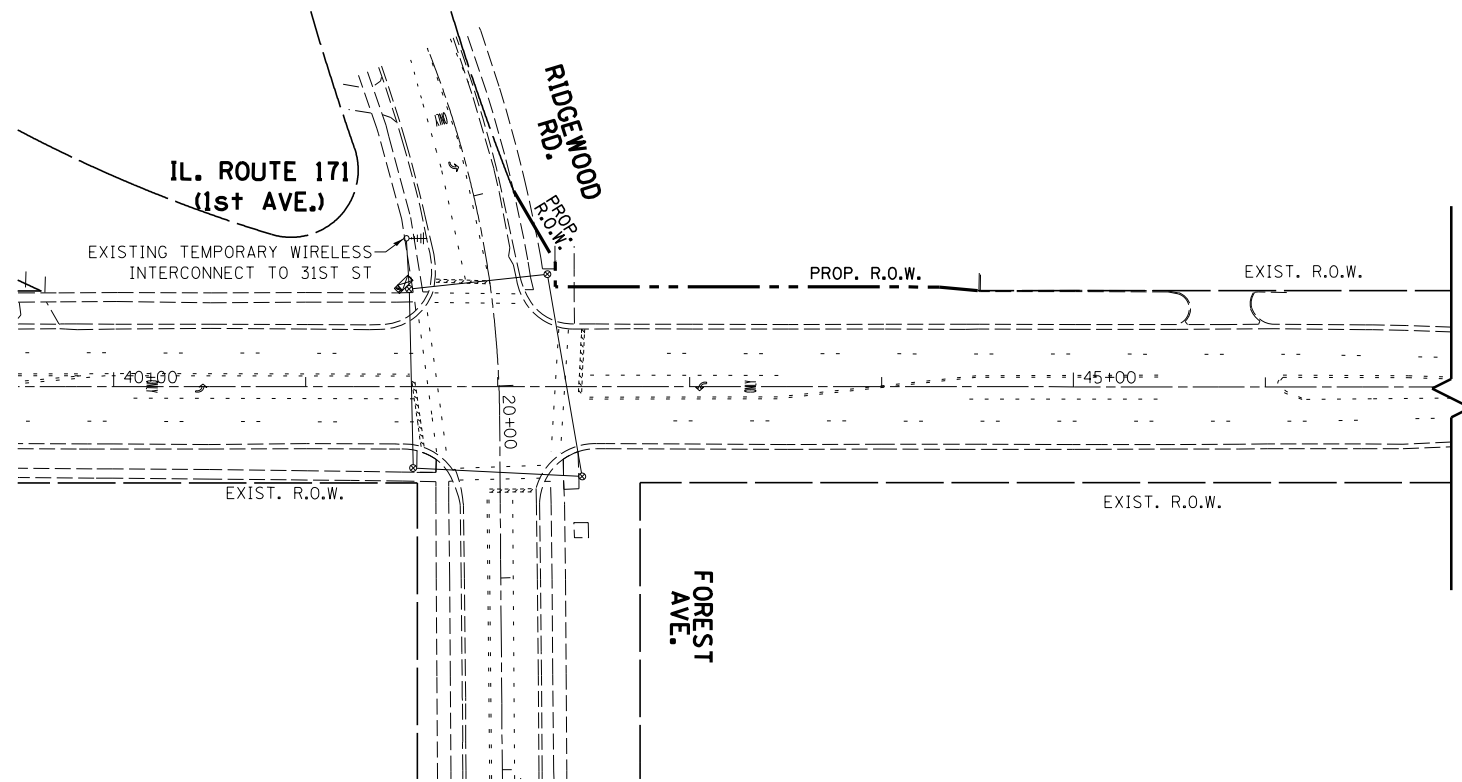
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAY/DISTRICT 1  
 201 WEST CENTER COURT/SCHAUMBURG, ILLINOIS 60196-1096  
 ENERGY SUPPLY: CONTACT: JOE STACHO  
 PHONE: (630) 424-5704  
 COMPANY: COMMONWEALTH EDISON



**CABLE PLAN**  
 (NOT TO SCALE)

THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



NOTE:  
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

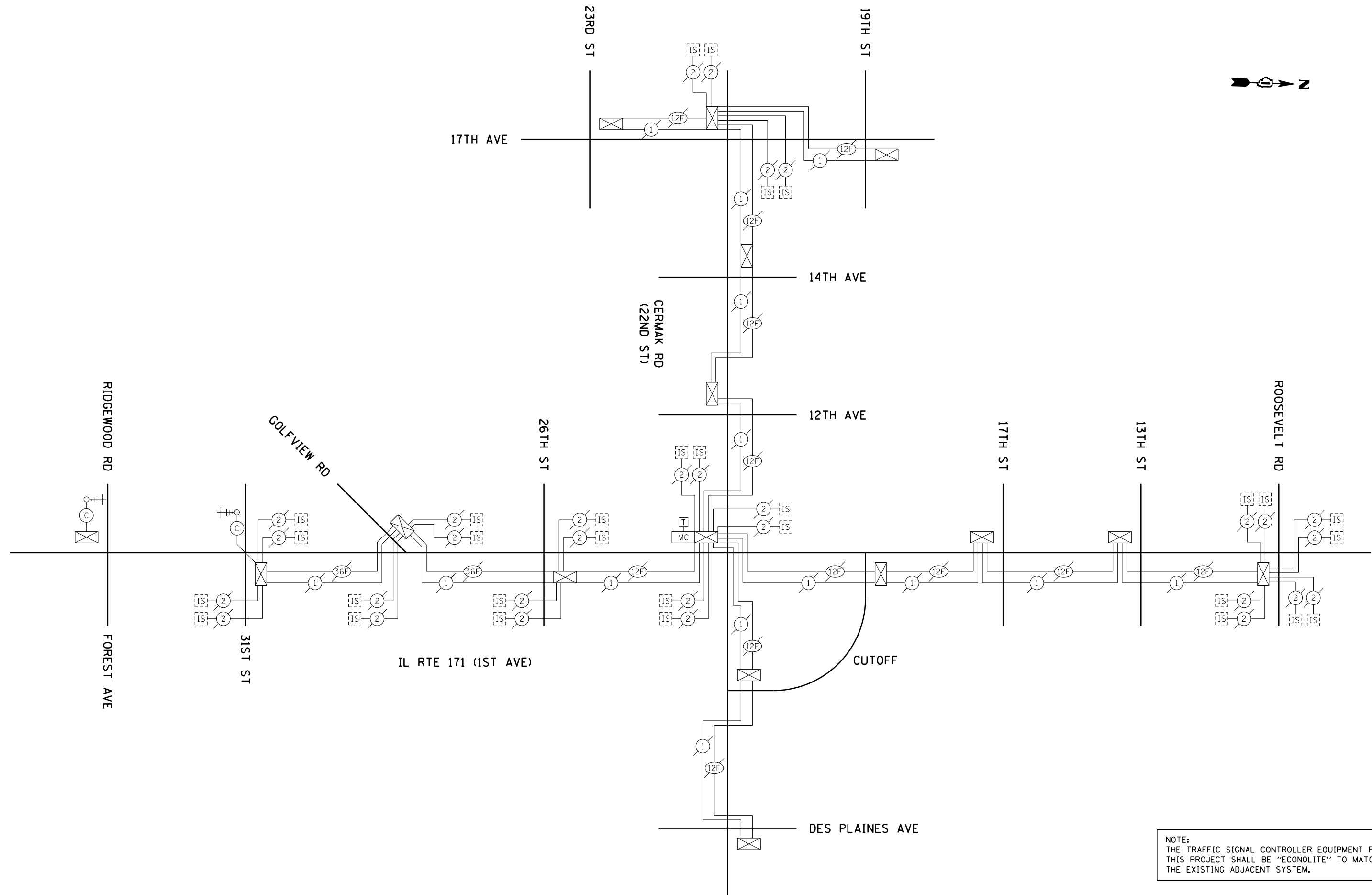
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Default	PLOT SCALE = 100.0000' / in.	CHECKED - LP	REVISED -
	PLOT DATE = 3/28/2014	DATE - 3/15/14	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXISTING INTERCONNECT PLAN  
IL RTE 171 (FOREST AVE / RIDGEWOOD RD TO 31ST ST)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	54
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				



NOTE:  
 THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR  
 THIS PROJECT SHALL BE "ECONOLITE" TO MATCH  
 THE EXISTING ADJACENT SYSTEM.

FILE NAME =	USER NAME = plascencia	DESIGNED - IP	REVISED -
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Default	PLOT SCALE = 100.0000' / in.	CHECKED - LP	REVISED -
	PLOT DATE = 3/28/2014	DATE - 3/15/14	REVISED -

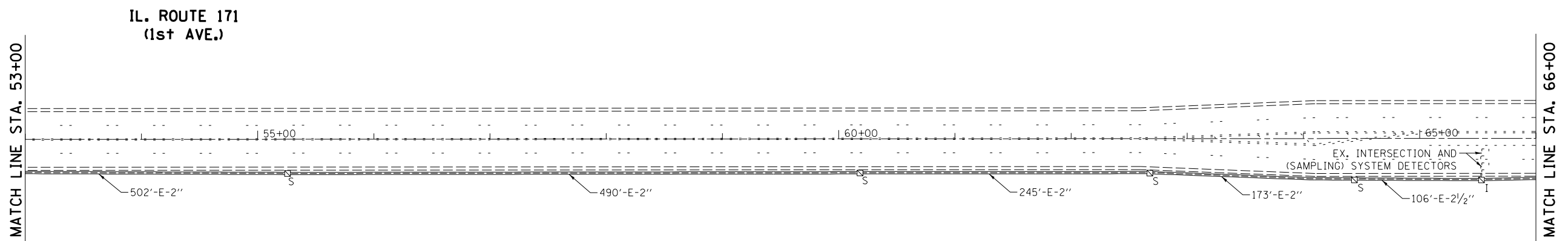
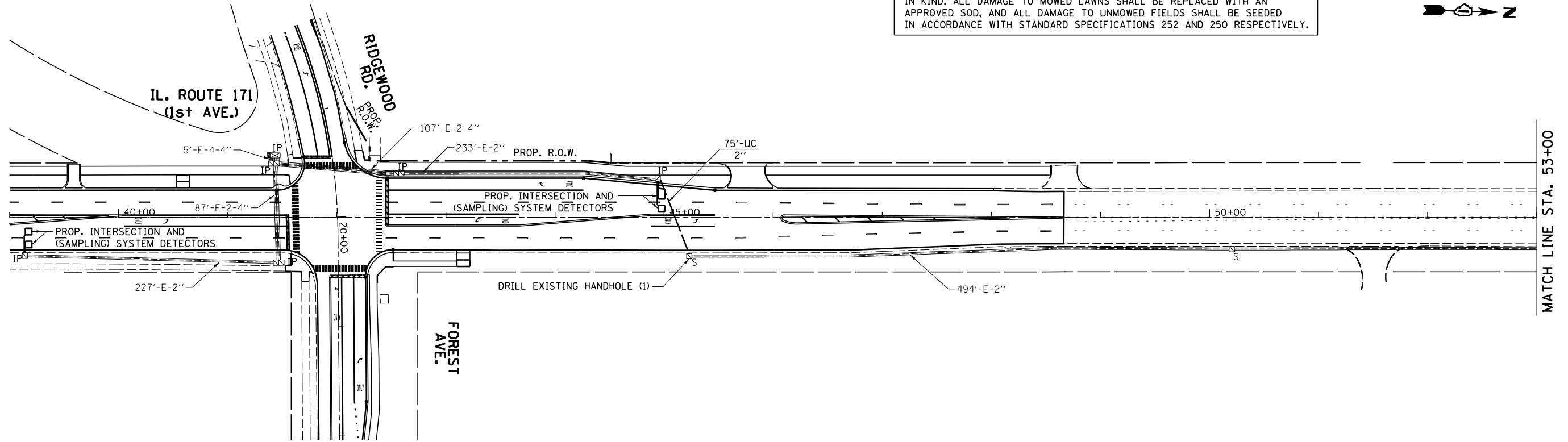
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**EXISTING INTERCONNECT SCHEMATIC  
 IL RTE 171 (FOREST AVE / RIDGEWOOD RD TO ROOSEVELT RD)**

SCALE: NTS      SHEET      OF      SHEETS      STA.      TO      STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	55
ILLINOIS FED. AID PROJECT			<b>CONTRACT NO. 60X08</b>	

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



NOTE:  
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

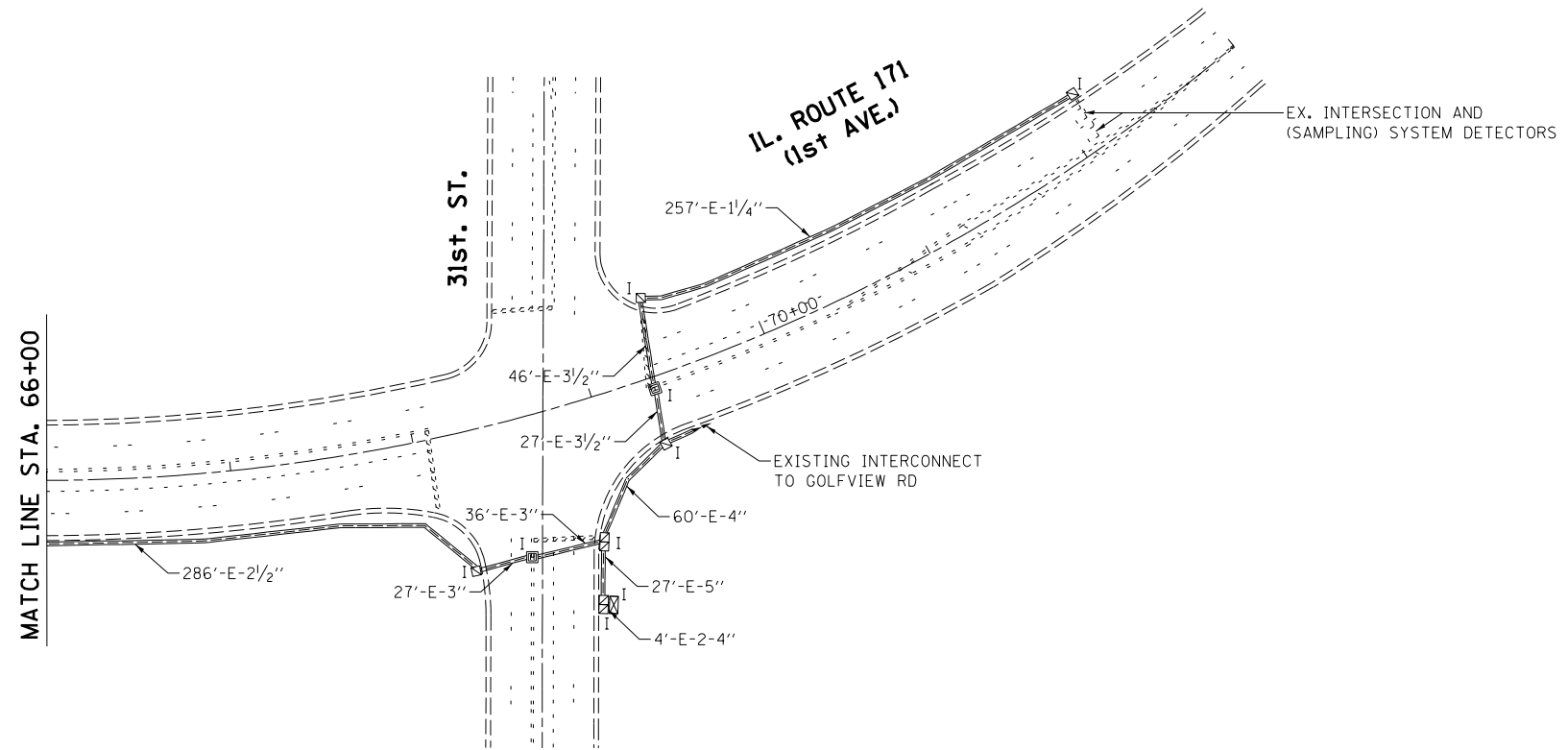
PROPOSED INTERCONNECT PLAN (1 OF 2)  
IL RTE 171 (FOREST AVE / RIDGEWOOD RD TO 31ST ST)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	56
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				



RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



NOTE:  
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

FILE NAME =	USER NAME = plascencia	DESIGNED - IP	REVISED -
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Default	PLOT SCALE = 100.0000' / in.	CHECKED - LP	REVISED -
	PLOT DATE = 3/28/2014	DATE - 3/15/14	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

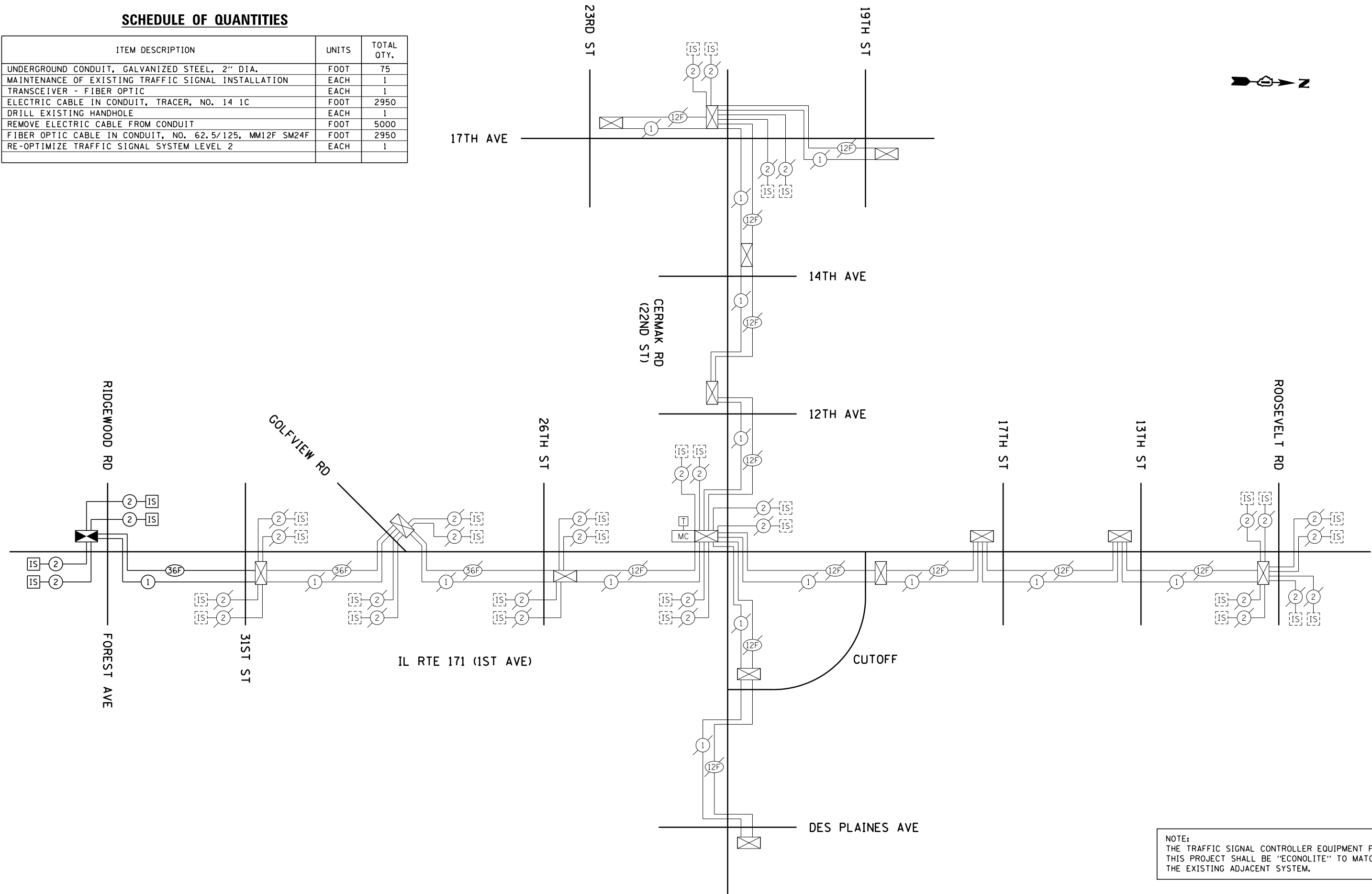
PROPOSED INTERCONNECT PLAN (2 OF 2)  
IL RTE 171 (FOREST AVE / RIDGEWOOD RD TO 31ST ST)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	57
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				

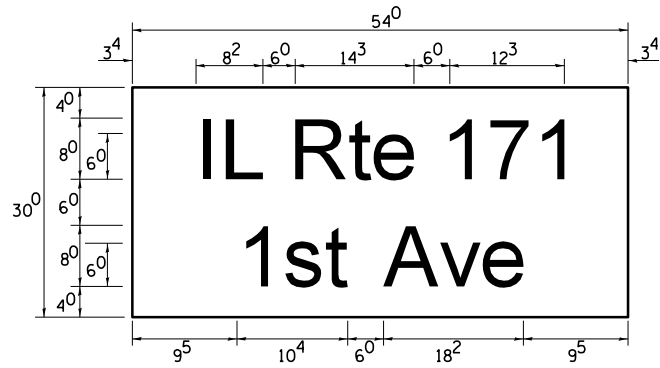
**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY.
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	75
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
TRANSCEIVER - FIBER OPTIC	EACH	1
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	2950
DRILL EXISTING HANDHOLE	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	5000
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	2950
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2	EACH	1



NOTE:  
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

PANEL SIGN DESIGN TYPE 2



11.25 Sq. Ft. each  
2 Required  
Design Series D



18.75 Sq. Ft. EA  
1  
Design Series D



18.75 Sq. Ft. EA  
1  
Design Series D

NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS

GENERAL NOTES

- 1. WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- 2. ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
- 3. THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 8'-0".
- 4. ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
- 5. SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:

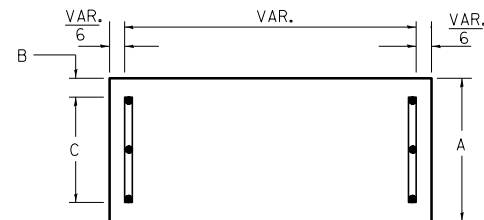
- \* J.O. HERBERT CO. MIDLOTHIAN, VA.
- \* WESTERN REMAC INC. WOODRIDGE, IL.

PARTS LISTING:  
SIGN CHANNEL PART \*HPN053 (MED. CHANNEL)  
SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3  
SELF TAPPING WITH NEOPRENE WASHER

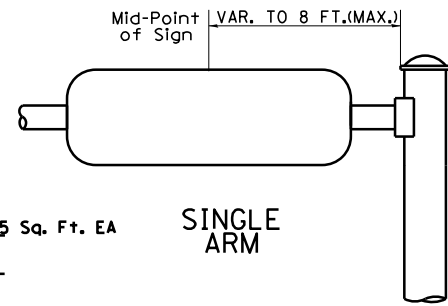
BRACKETS PART \*HPN034 (UNIVERSAL)  
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BRACKET OF THE ABOVE PRODUCT.

SUPPORTING CHANNELS

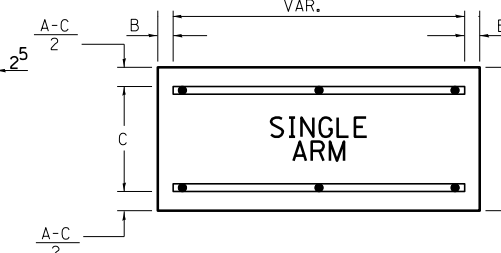


A	B	C
18"	2"	14"



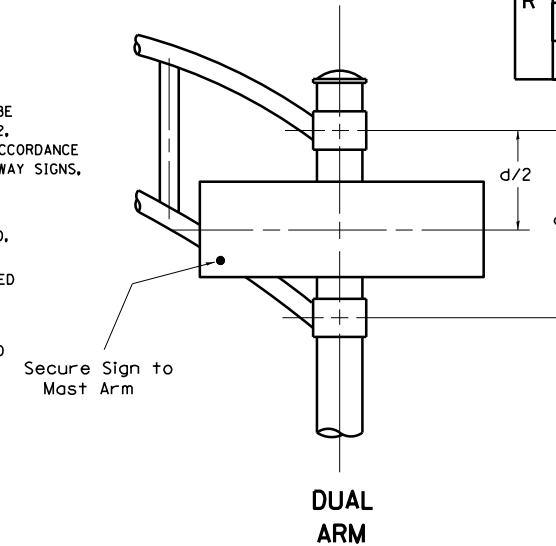
18.75 Sq. Ft. EA  
1  
Design Series D

SUPPORTING CHANNELS



A	B	C
18"	2"	12"
30"	2"	22"

18.75 Sq. Ft. EA  
1  
Design Series D



SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM shall be used. See Note #5.

Upper Case To Lower Case  
Spacing Chart 8-6 Inch Series "C & D"

SERIES	SECOND LETTER																	
	a c d e		b h i k l				f w		j		s t		v y		x		z	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
A W X	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
B	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>
C E G	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
D O O R	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
F	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>4</sup>	1 <sup>5</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>
H I M N	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>2</sup>	2 <sup>4</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>
J U	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>
K L	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
P	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
S	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
T	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
V	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
Y	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>4</sup>	1 <sup>5</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>5</sup>	0 <sup>7</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>
Z	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>2</sup>	2 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>

Lower Case To Lower Case  
Spacing Chart 6 Inch Series "C & D"

SERIES	SECOND LETTER																		
	a c d e		b h i k l				f w		j		s t		v y		x		z		
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	
ad h g i j	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>2</sup>	2 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	
l m n q u																			
b f k o p s	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	
c e	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	
r	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>3</sup>	0 <sup>3</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	
t z	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	
v y	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	
w	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	
x	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	

Number To Number  
Spacing Chart 8 Inch Series "C & D"

SERIES	SECOND NUMBER																			
	0		1		2		3		4		5		6		7		8		9	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
0 9	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>
1	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>
2 3 4	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>
5	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
6	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
7	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>5</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>
8	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>

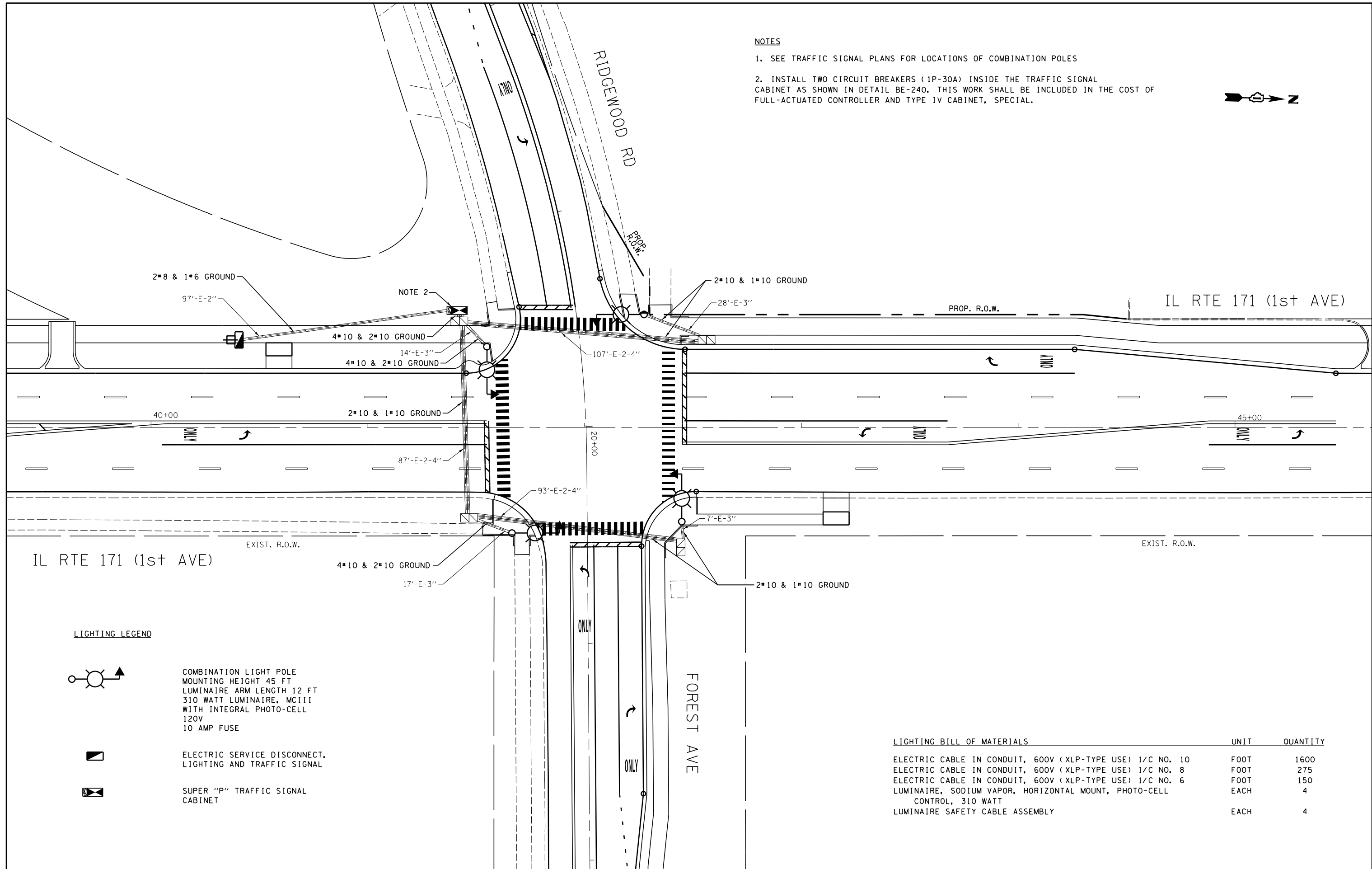
EXAMPLE, 2<sup>3</sup> DENOTES 3/8

UPPER AND LOWER CASE  
LETTER WIDTHS

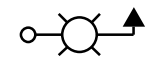
LETTERS	6 INCH UPPER CASE LETTERS				8 INCH UPPER CASE LETTERS				LETTERS	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES		SERIES		SERIES			C	D
	C	D	C	D	C	D	C	D			
A	3 <sup>6</sup>	5 <sup>0</sup>	5 <sup>0</sup>	6 <sup>5</sup>	a	3 <sup>5</sup>	4 <sup>2</sup>				
B	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	b	3 <sup>5</sup>	4 <sup>2</sup>				
C	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	c	3 <sup>5</sup>	4 <sup>1</sup>				
D	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	d	3 <sup>5</sup>	4 <sup>2</sup>				
E	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	e	3 <sup>5</sup>	4 <sup>2</sup>				
F	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	f	2 <sup>3</sup>	2 <sup>6</sup>				
G	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	g	3 <sup>5</sup>	4 <sup>2</sup>				
H	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	h	3 <sup>5</sup>	4 <sup>2</sup>				
I	0 <sup>7</sup>	0 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	i	1 <sup>1</sup>	1 <sup>1</sup>				
J	3 <sup>0</sup>	3 <sup>6</sup>	4 <sup>0</sup>	5 <sup>0</sup>	j	2 <sup>0</sup>	2 <sup>2</sup>				
K	3 <sup>2</sup>	4 <sup>1</sup>	4 <sup>3</sup>	5 <sup>4</sup>	k	3 <sup>5</sup>	4 <sup>2</sup>				
L	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	l	1 <sup>1</sup>	1 <sup>1</sup>				
M	3 <sup>7</sup>	4 <sup>5</sup>	5 <sup>1</sup>	6 <sup>1</sup>	m	6 <sup>0</sup>	7 <sup>0</sup>				
N	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	n	3 <sup>5</sup>	4 <sup>2</sup>				
O	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>	o	3 <sup>6</sup>	4 <sup>3</sup>				
P	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	p	3 <sup>5</sup>	4 <sup>2</sup>				
Q	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>	q	3 <sup>5</sup>	4 <sup>2</sup>				
R	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	r	2 <sup>6</sup>	3 <sup>2</sup>				
S	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	s	3 <sup>6</sup>	4 <sup>2</sup>				
T	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	t	2 <sup>7</sup>	3 <sup>2</sup>				
U	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	u	3 <sup>5</sup>	4 <sup>2</sup>				
V	3 <sup>5</sup>	4 <sup>4</sup>	4 <sup>7</sup>	6 <sup>0</sup>	v	4 <sup>2</sup>					

**NOTES**

1. SEE TRAFFIC SIGNAL PLANS FOR LOCATIONS OF COMBINATION POLES
2. INSTALL TWO CIRCUIT BREAKERS (1P-30A) INSIDE THE TRAFFIC SIGNAL CABINET AS SHOWN IN DETAIL BE-240. THIS WORK SHALL BE INCLUDED IN THE COST OF FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL.



**LIGHTING LEGEND**



COMBINATION LIGHT POLE  
MOUNTING HEIGHT 45 FT  
LUMINAIRE ARM LENGTH 12 FT  
310 WATT LUMINAIRE, MCIII  
WITH INTEGRAL PHOTO-CELL  
120V  
10 AMP FUSE



ELECTRIC SERVICE DISCONNECT,  
LIGHTING AND TRAFFIC SIGNAL



SUPER "P" TRAFFIC SIGNAL  
CABINET

**LIGHTING BILL OF MATERIALS**

	UNIT	QUANTITY
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 10	FOOT	1600
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 8	FOOT	275
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6	FOOT	150
LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, PHOTO-CELL CONTROL, 310 WATT	EACH	4
LUMINAIRE SAFETY CABLE ASSEMBLY	EACH	4

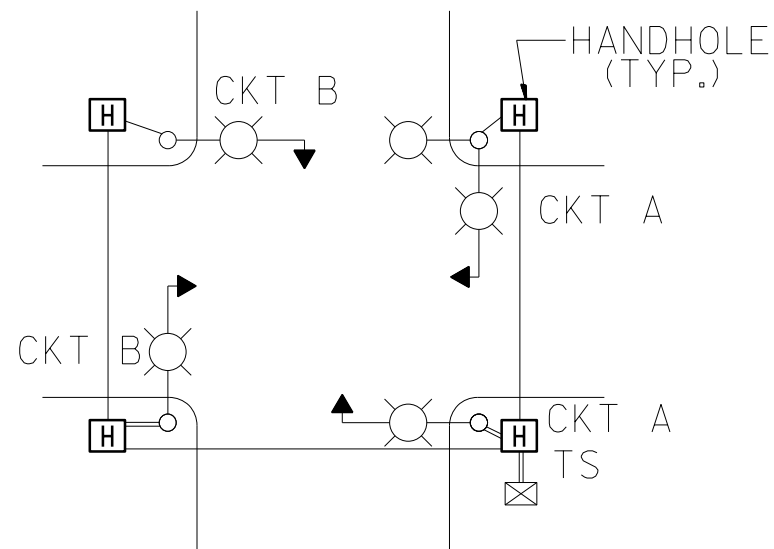
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

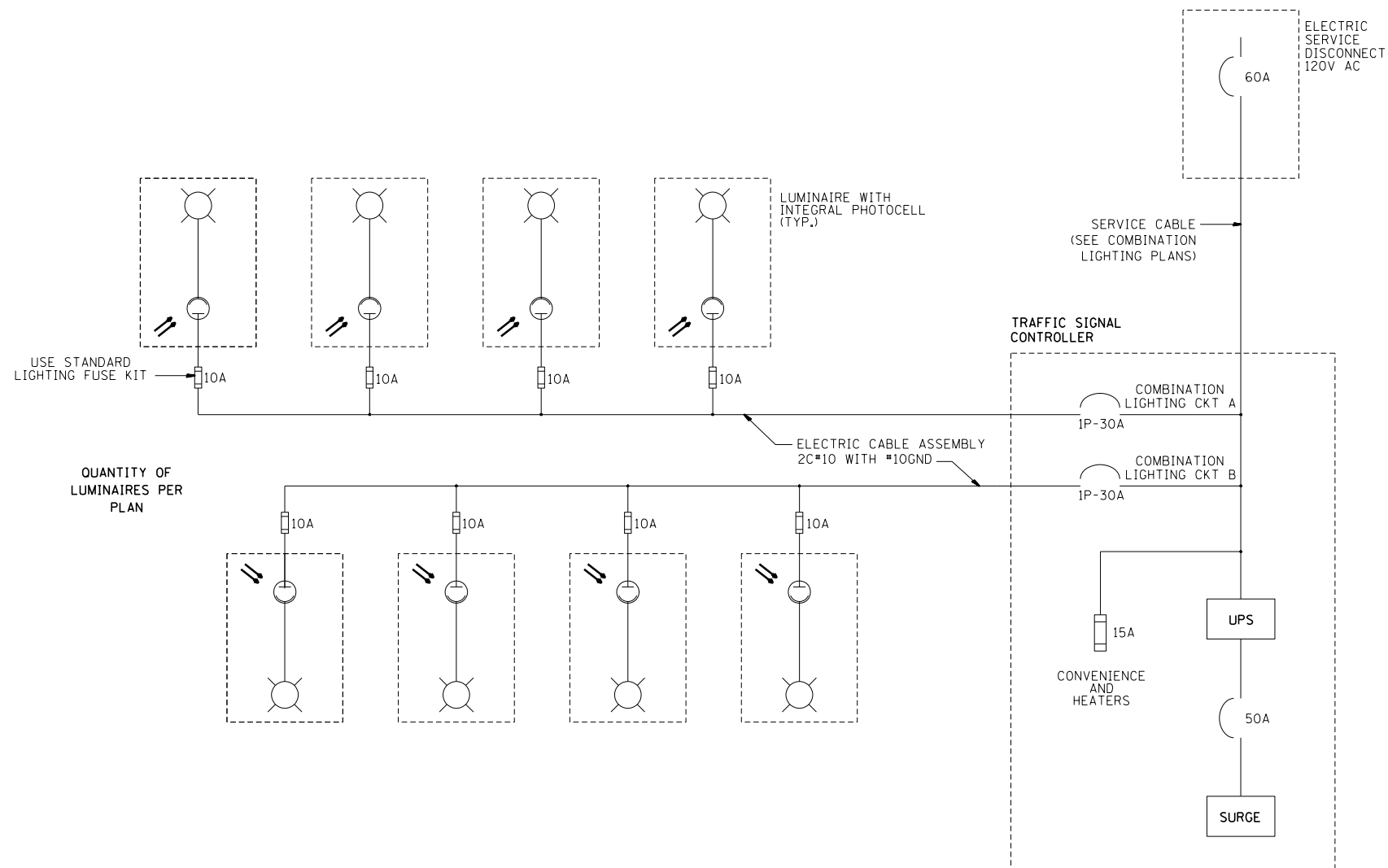
**COMBINATION LIGHTING PLAN  
IL RTE 171 (1ST AVE) AND FOREST AVE / RIDGEWOOD RD**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	60
<b>CONTRACT NO. 60X08</b>				
ILLINOIS FED. AID PROJECT				

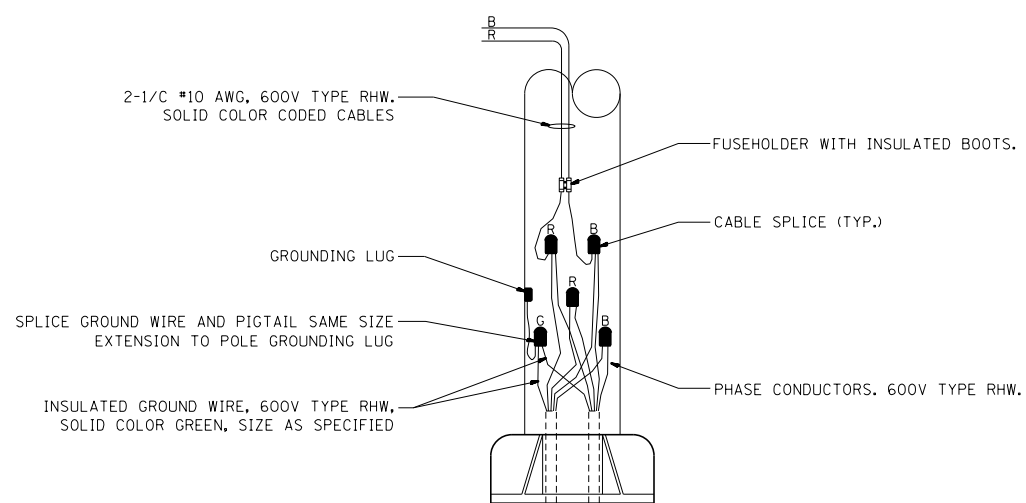
SCALE: SHEET OF SHEETS STA. TO STA.



TYPICAL LIGHTING LAYOUT  
(NOT TO SCALE)



QUANTITY OF LUMINAIRES PER PLAN

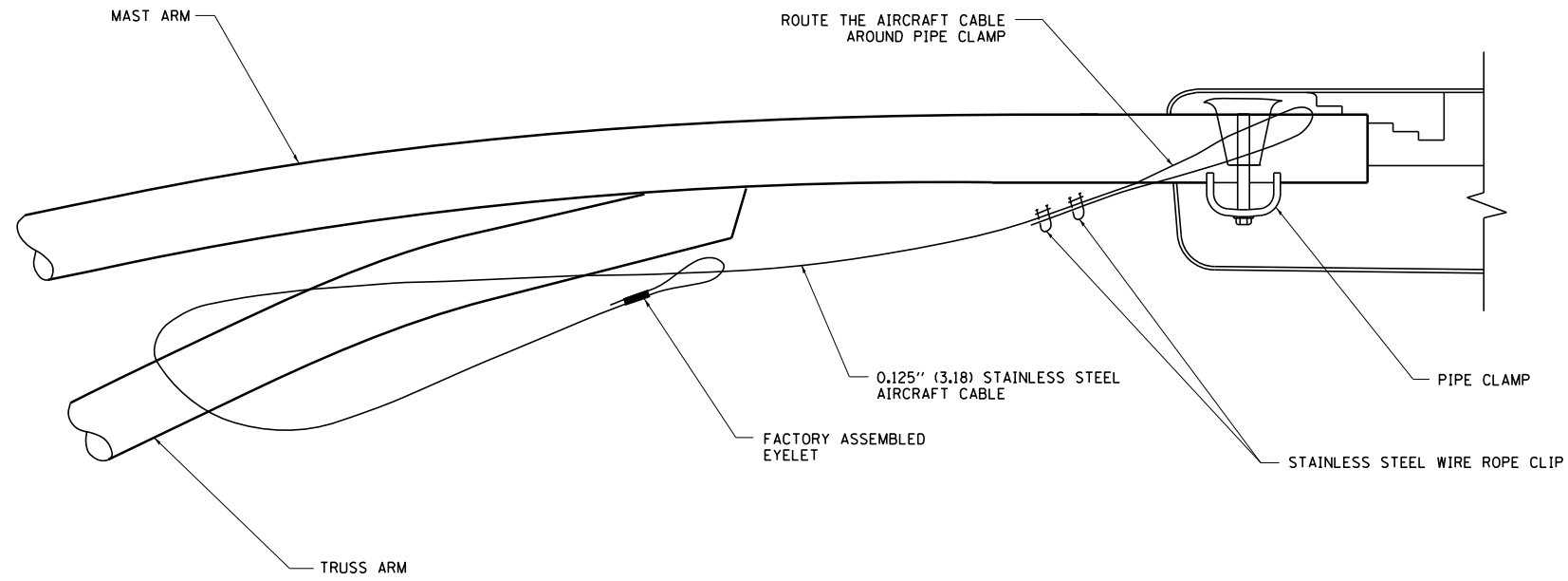


COMBINATION POLE WIRING DETAIL  
(NOT TO SCALE)

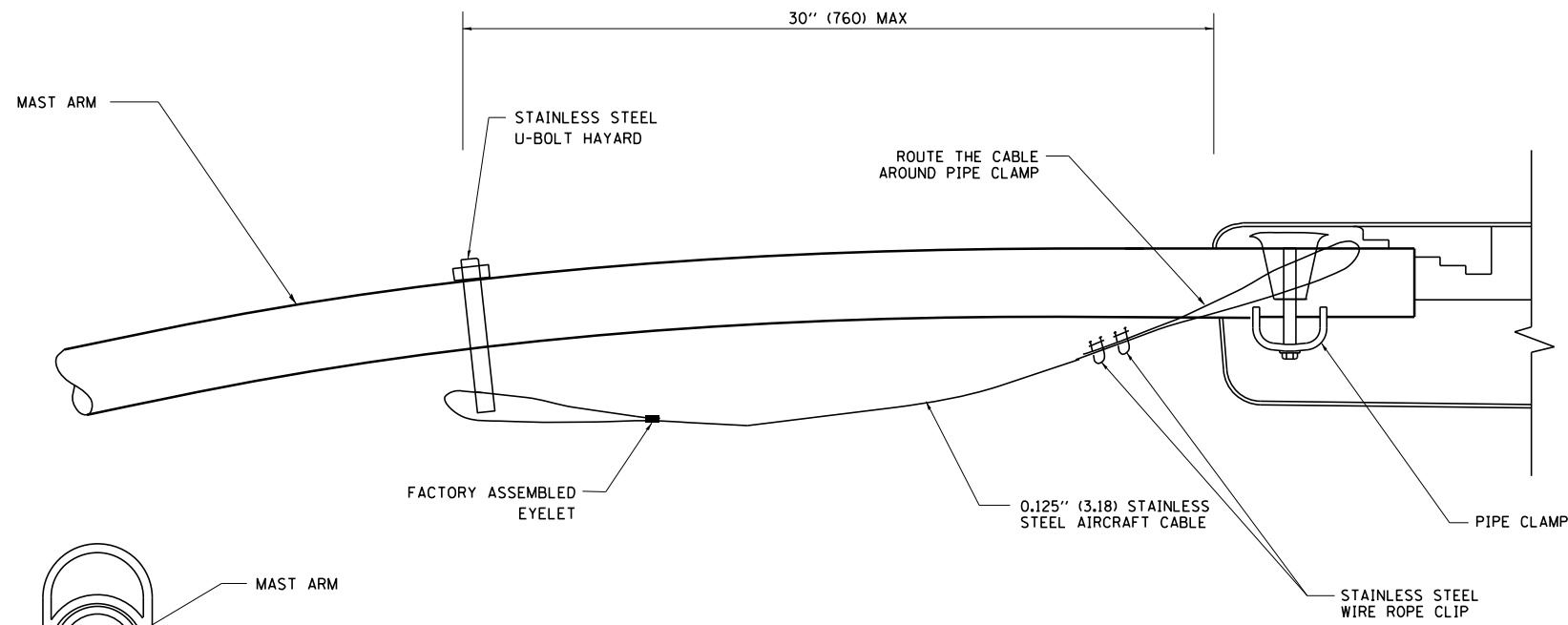
NOTES:

- 1.- 4 LUMINAIRES PER CIRCUIT, MAXIMUM.
- 2.- EACH LUMINAIRE IS PHOTOCCELL CONTROLLED.
- 3.- MULTI-CONDUCTOR CABLE ASSEMBLY FOR LIGHTING CIRCUITS.
- 4.- ROUTE LIGHTING CIRCUITS IN TRAFFIC SIGNAL CONDUIT SYSTEM.
- 5.- ALL CONTROLLERS TO HAVE TWO LIGHTING BRANCH BREAKERS.
- 6.- ALL WIRING SHALL BE NEATLY DRESSED, IDENTIFIED BY TAGS, AND SUPPORTED.
- 7.- ALL SPLICES AND CONNECTIONS FOR ROADWAY LIGHTING SHALL BE AT POLE BASE ONLY. UNDERGROUND SPLICING OF LIGHTING CONDUCTORS IS NOT PERMITTED.

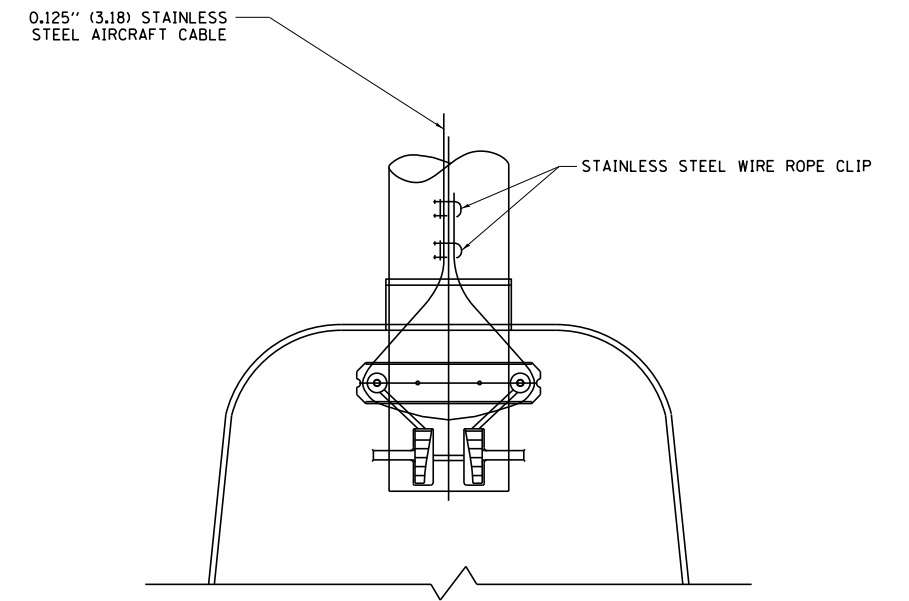
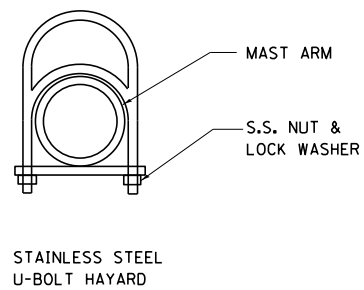
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Default	14412-TS.dgn	DRAWN -	REVISED -						372	03035-TS	COOK	87	61
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	PLOT DATE = 3/28/2014	DATE - 03/14/2014	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT	



**SIDE VIEW (TRUSS ARM)**  
N.T.S.



**SIDE VIEW (SINGLE MEMBER OR DAVIT ARM)**  
N.T.S.

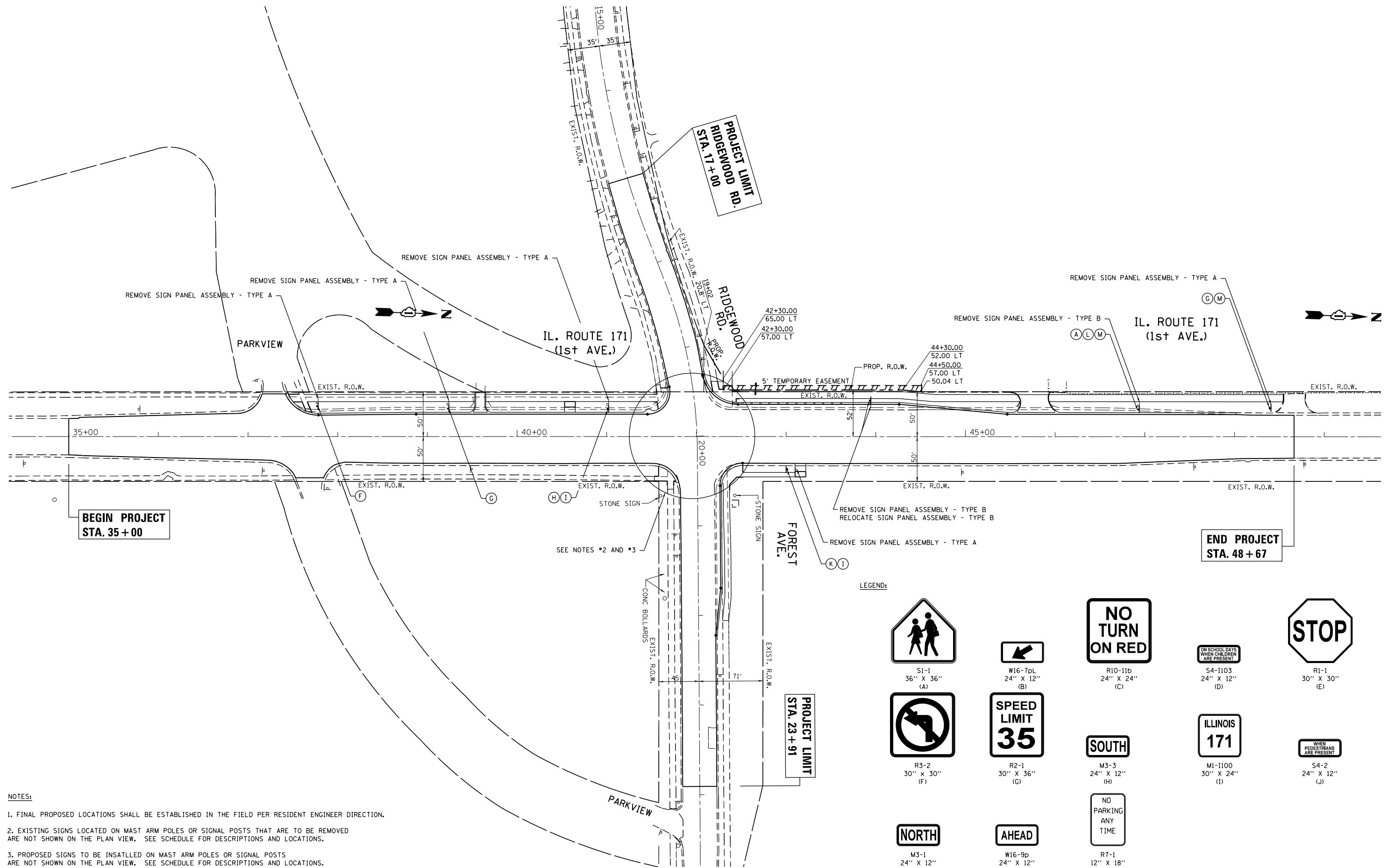


**BOTTOM VIEW**  
N.T.S.

**NOTES:**

1. ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.
2. CONTRACTOR SHALL ADJUST THE WIRE CLIP TO ELIMINATE ANY SLACK FROM THE WIRE ROPE.
3. THE 0.125" (3.18) STAINLESS STEEL AIRCRAFT CABLE SHALL REMAIN VISIBLE FROM THE GROUND LEVEL.
4. THE BREAKING STRENGTH OF THE CABLE SHALL BE 1700 LBS. MIN.

FILE NAME =	USER NAME = plascencia	DESIGNED -	REVISED - 08-08-03	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>LUMINAIRE SAFETY CABLE ASSEMBLY</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 3/28/2014	DATE -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT			



NOTES:

1. FINAL PROPOSED LOCATIONS SHALL BE ESTABLISHED IN THE FIELD PER RESIDENT ENGINEER DIRECTION.
2. EXISTING SIGNS LOCATED ON MAST ARM POLES OR SIGNAL POSTS THAT ARE TO BE REMOVED ARE NOT SHOWN ON THE PLAN VIEW. SEE SCHEDULE FOR DESCRIPTIONS AND LOCATIONS.
3. PROPOSED SIGNS TO BE INSATLLED ON MAST ARM POLES OR SIGNAL POSTS ARE NOT SHOWN ON THE PLAN VIEW. SEE SCHEDULE FOR DESCRIPTIONS AND LOCATIONS.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD.  
SIGNING PLAN**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	63
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.

RIDGEWOOD ROAD / FOREST AVENUE SIGNS TO BE REMOVED:							
STATION	OFFSET	FACING TRAFFIC	PANEL DESCRIPTION	REMOVE SIGN PANEL		REMOVE SIGN PANEL ASSEMBLY	
				TYPE 1 (SQ FT)	TYPE 2 (SQ FT)	TYPE A (EACH)	TYPE B (EACH)
19+35.4	33.8' RT	EB	NO TURN ON RED	4			
19+35.4	33.8' RT	EB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2			
19+35.4	33.8' RT	EB	STOP (FOLDED)	6.25			
19+48.7	21.0' LT	WB	NO TURN ON RED	4			
19+48.7	21.0' LT	WB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2			
20+59.9	25.2' LT	WB	SCHOOL	9			
20+59.9	25.2' LT	WB	LEFT DIAGONAL ARROW	2.2			
20+59.9	25.2' LT	WB	NO TURN ON RED	4			
20+59.9	25.2' LT	WB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2			
20+59.9	25.2' LT	WB	STOP (FOLDED)	6.25			
20+60.6	25.4' RT	EB	NO TURN ON RED	4			
20+60.6	25.4' RT	EB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2			

IL 171 SIGNS TO BE RELOCATED:					
STATION	OFFSET	FACING TRAFFIC	PANEL DESCRIPTION	RELOCATE SIGN PANEL ASSEMBLY	
				TYPE A (EACH)	TYPE B (EACH)
43+94.5 *	40.3' LT	SB	BROOKFIELD ZOO, OPEN EVERY DAY		1
		SB	SERVICE & SOUTH GATE		

\* EXISTING SIGN POST TO BE REUSED WHEN SIGN PANEL ASSEMBLY IS RELOCATED.

IL 171 SIGNS TO BE REMOVED:							
STATION	OFFSET	FACING TRAFFIC	PANEL DESCRIPTION	REMOVE SIGN PANEL		REMOVE SIGN PANEL ASSEMBLY	
				TYPE 1 (SQ FT)	TYPE 2 (SQ FT)	TYPE A (EACH)	TYPE B (EACH)
37+78.1	37.9' LT	SB	LEFT TURN PROHIBITION			1	
39+23.9	36.3' LT	SB	SPEED LIMIT (35)			1	
41+2.0	35.8' LT	SB	CARDINAL DIRECTION (SOUTH)			1	
		SB	ILLINOIS ROUTE 171				
41+51.6	40.5' LT	SB	NO TURN ON RED	4			
41+51.6	40.5' LT	SB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2			
41+57.7	37.9' RT	NB	SCHOOL CROSSING	9			
41+57.7	37.9' RT	NB	NO TURN ON RED	4			
41+57.7	37.9' RT	NB	WHEN PEDESTRIANS ARE PRESENT	2			
41+57.7	37.9' RT	NB	STOP (FOLDED)	6.25			
42+40.9	34.6' LT	SB	SCHOOL CROSSING	9			
42+40.9	34.6' LT	SB	NO TURN ON RED	4			
42+40.9	34.6' LT	SB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2			
42+40.9	34.6' LT	SB	STOP (FOLDED)	6.25			
42+47.3	38.7' RT	NB	NO TURN ON RED	4			
42+47.3	38.7' RT	NB	WHEN PEDESTRIANS ARE PRESENT	2			
43+10.1	37.9' RT	NB	CARDINAL DIRECTION (NORTH)			1	
		NB	ILLINOIS ROUTE 171				
43+94.5 *	36.7' LT	SB	BROOKFIELD ZOO, OPEN EVERY DAY				1
		SB	SERVICE & SOUTH GATE				
46+93.9	34.2' LT	SB	SCHOOL				1
		SB	NO PARKING				
48+40.7	32.0' LT	SB	SPEED LIMIT (35)			1	
		SB	NO PARKING				

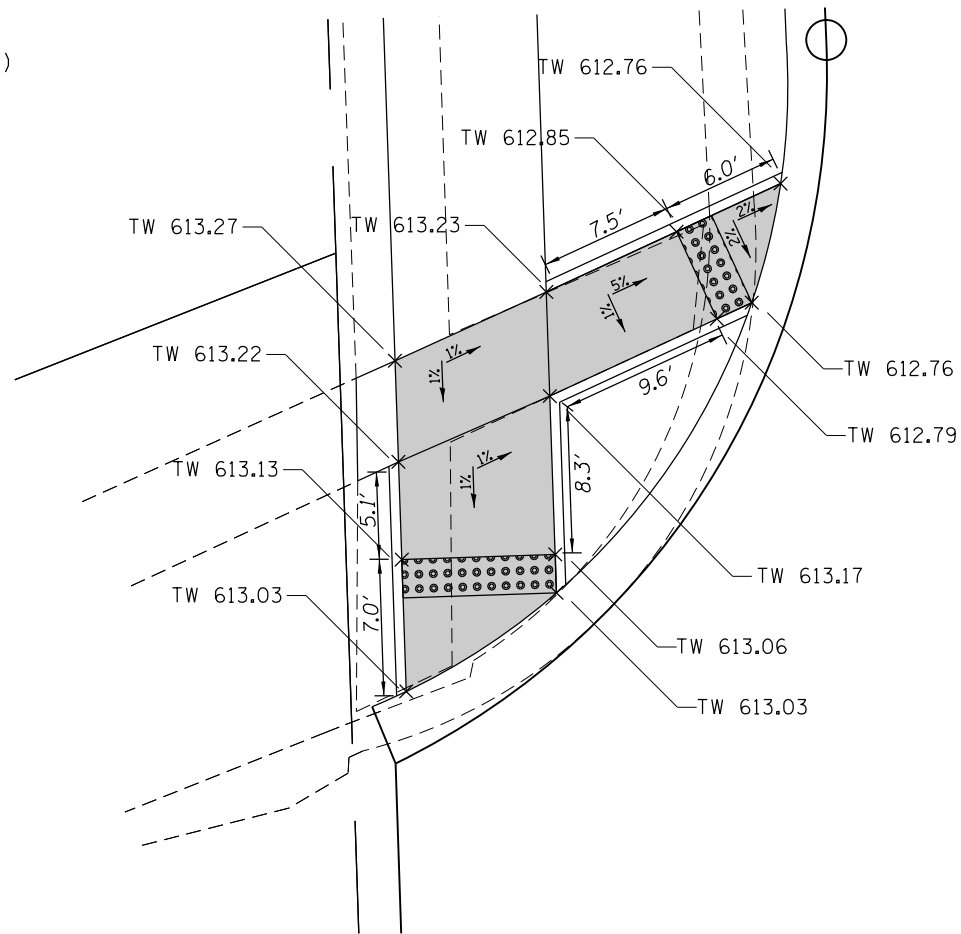
\* EXISTING SIGN POST TO BE REUSED WHEN SIGN PANEL ASSEMBLY IS RELOCATED.



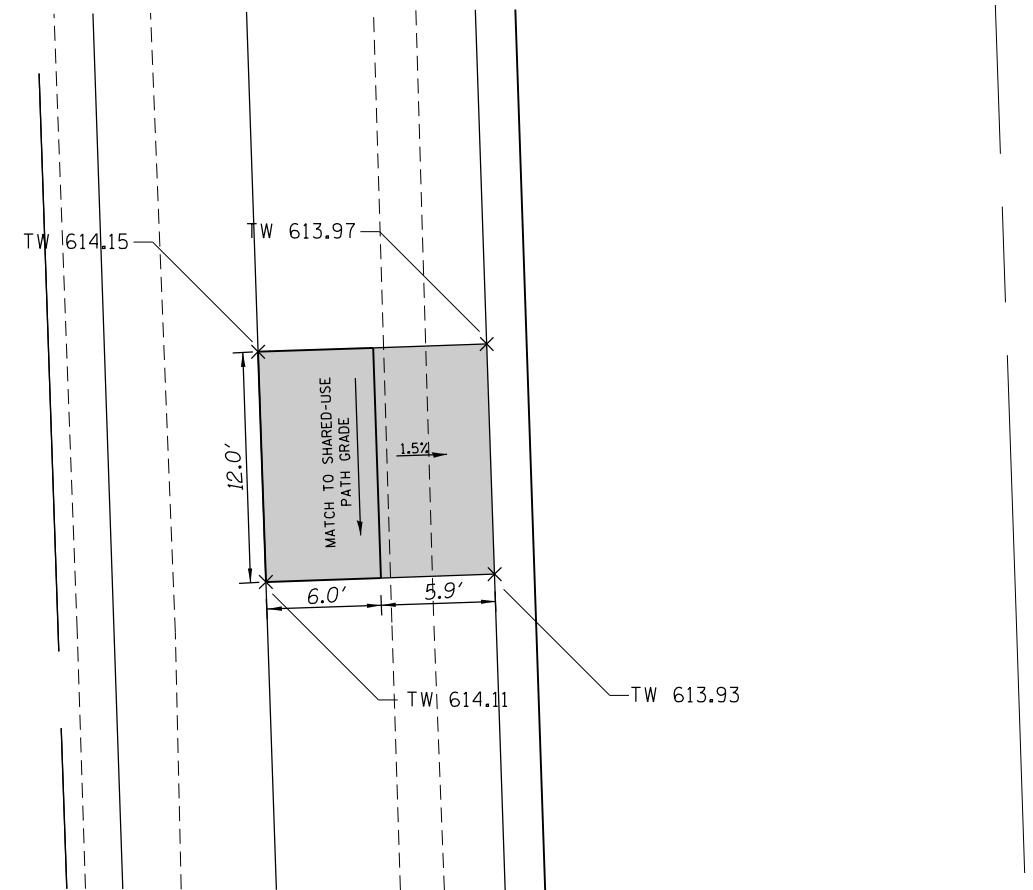
RIDGEWOOD ROAD / FOREST AVENUE PROPOSED SIGNS:										
SIGN SYMBOL	SIGN CODE	STATION	OFFSET	FACING TRAFFIC	PANEL DESCRIPTION	SIGN PANEL		TELESCOPING STEEL SIGN SUPPORT (FT)	BASE FOR TELESCOPING STEEL SIGN SUPPORT (EA)	NOTES
						TYPE 1 (SQ. FT)	TYPE 2 (SQ. FT)			
A	S1-1	19+42.8	32.8' RT	EB	SCHOOL	9				PLACED ON SIGNAL POST
B	W16-7pL	19+42.8	32.8' RT	EB	LEFT DIAGONAL ARROW	2				PLACED ON SIGNAL POST
C	R10-11b	19+42.8	32.8' RT	EB	NO TURN ON RED	4				PLACED ON SIGNAL POST
D	S4-I103	19+42.8	32.8' RT	EB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				PLACED ON SIGNAL POST
E	R1-1	19+42.8	32.8' RT	EB	STOP	6.25				THIS IS A TRI-FOLD SIGN, PLACED ON SIGNAL POST
C	R10-11b	19+52.1	31.0' LT	WB	NO TURN ON RED	4				PLACED ON MAST ARM POLE
D	S4-I103	19+52.1	31.0' LT	WB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				PLACED ON MAST ARM POLE
C	R10-11b	20+48.9	34.8' RT	EB	NO TURN ON RED	4				PLACED ON MAST ARM POLE
D	S4-I103	20+48.9	34.8' RT	EB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				PLACED ON MAST ARM POLE
A	S1-1	20+55.0	30.6" LT	WB	SCHOOL	9				PLACED ON SIGNAL POST
B	W16-7pL	20+55.0	30.6" LT	WB	LEFT DIAGONAL ARROW	2				PLACED ON SIGNAL POST
C	R10-11b	20+55.0	30.6" LT	WB	NO TURN ON RED	4				PLACED ON SIGNAL POST
D	S4-I103	20+55.0	30.6" LT	WB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				PLACED ON SIGNAL POST
E	R1-1	20+55.0	30.6" LT	WB	STOP	6.25				THIS IS A TRI-FOLD SIGN, PLACED ON SIGNAL POST

IL 171 PROPOSED SIGNS:										
SIGN SYMBOL	SIGN CODE	STATION	OFFSET	FACING TRAFFIC	PANEL DESCRIPTION	SIGN PANEL		TELESCOPING STEEL SIGN SUPPORT (FT)	BASE FOR TELESCOPING STEEL SIGN SUPPORT (EA)	NOTES
						TYPE 1 (SQ. FT)	TYPE 2 (SQ. FT)			
F	R3-2	37+78.1	33.7' LT	SB	LEFT TURN PROHIBITION	6.25		14.5	1	
G	R2-1	39+23.9	31.1' LT	SB	SPEED LIMIT (35)	7.5		15	1	
H	M3-3	41+2.0	30.7' LT	SB	CARDINAL DIRECTION (SOUTH)	2		15	1	
I	M1-I100				ILLINOIS ROUTE 171	5				
C	R10-11b	41+54.9	37.2' LT	SB	NO TURN ON RED	4				PLACED ON MAST ARM POLE
D	S4-I103	41+54.9	37.2' LT	SB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				PLACED ON MAST ARM POLE
A	S1-1	41+56.5	38.0' RT	NB	SCHOOL	9				PLACED ON SIGNAL POST
B	W16-7pL	41+56.5	38.0' RT	NB	LEFT DIAGONAL ARROW	2				PLACED ON SIGNAL POST
C	R10-11b	41+56.5	38.0' RT	NB	NO TURN ON RED	4				PLACED ON SIGNAL POST
J	S4-2	41+56.5	38.0' RT	NB	WHEN PEDESTRIANS ARE PRESENT	2				PLACED ON SIGNAL POST
E	R1-1	41+56.5	38.0' RT	NB	STOP	6.25				THIS IS A TRI-FOLD SIGN, PLACED ON SIGNAL POST
C	R10-11b	42+44.8	43.5' RT	NB	NO TURN ON RED	4				PLACED ON MAST ARM POLE
J	S4-2	42+44.8	43.5' RT	NB	WHEN PEDESTRIANS ARE PRESENT	2				PLACED ON MAST ARM POLE
A	S1-1	42+46.2	42.1' LT	SB	SCHOOL	9				PLACED ON SIGNAL POST
B	W16-7pL	42+46.2	42.1' LT	SB	LEFT DIAGONAL ARROW	2				PLACED ON SIGNAL POST
C	R10-11b	42+46.2	42.1' LT	SB	NO TURN ON RED	4				PLACED ON SIGNAL POST
D	S4-I103	42+46.2	42.1' LT	SB	ON SCHOOL DAYS WHEN CHILDREN ARE PRESENT	2				PLACED ON SIGNAL POST
E	R1-1	42+46.2	42.1' LT	SB	STOP	6.25				THIS IS A TRI-FOLD SIGN, PLACED ON SIGNAL POST
K	M3-1	43+00.0	36.3' RT	NB	CARDINAL DIRECTION (NORTH)	2		15	1	
I	M1-I100				ILLINOIS ROUTE 171	5				
A	S1-1	46+93.9	30.4' LT	SB	SCHOOL	9		17.5	1	
L	W16-9p				AHEAD	2				
M	R7-1				NO PARKING ANYTIME	1.5				
G	R2-1	48+40.7	32.0' LT	SB	SPEED LIMIT (35)	7.5		16.5	1	
M	R7-1				NO PARKING ANYTIME	1.5				

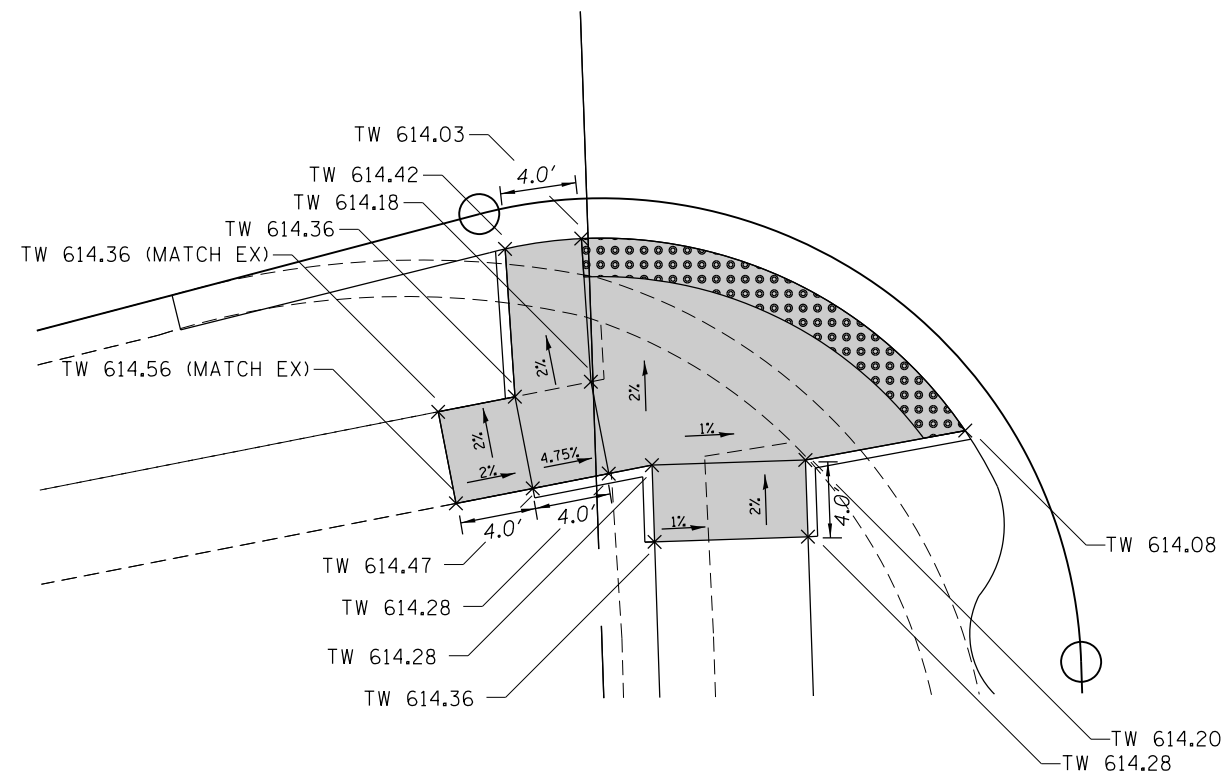
PARKVIEW AVENUE  
(NORTHWEST CORNER)



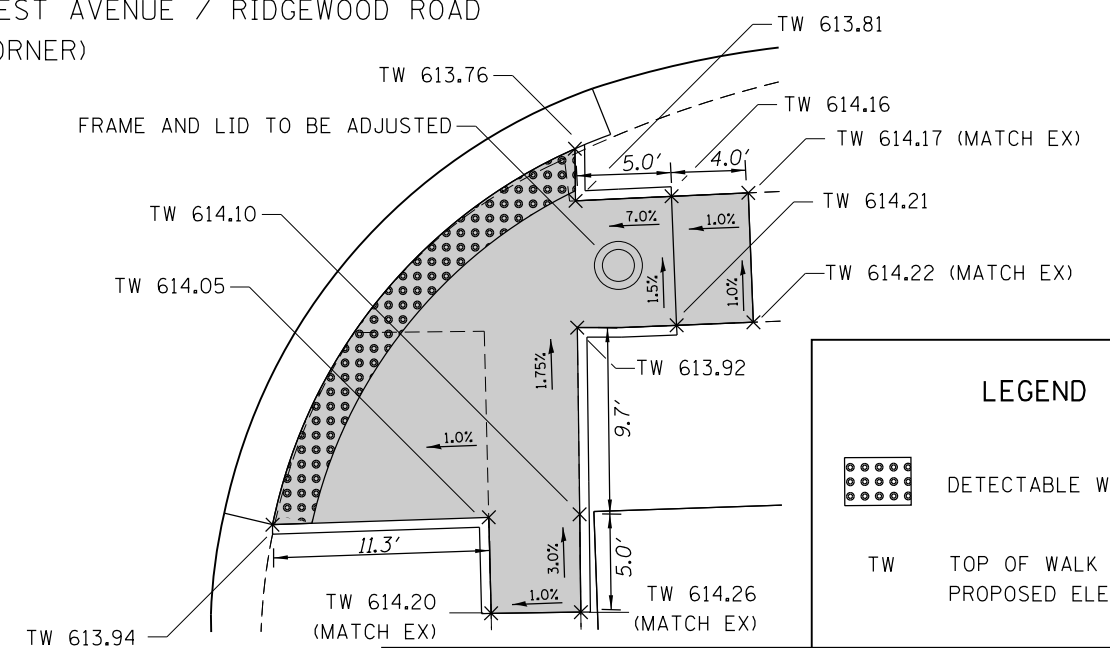
BUS PAD  
STA 40+59, 33' LT



IL 171 AT FOREST AVENUE / RIDGEWOOD ROAD  
(SOUTHWEST CORNER)



IL 171 AT FOREST AVENUE / RIDGEWOOD ROAD  
(SOUTHEAST CORNER)



**LEGEND**

	DETECTABLE WARNINGS
TW	TOP OF WALK PROPOSED ELEVATION

- GENERAL NOTES:**
1. PROPOSED ELEVATIONS ARE APPROXIMATE.
  2. REF: BM ELEV 619.69, LOCATED ON THE NORTH-NORTHWEST BONNET BOLT ON THE FIRE HYDRANT AT THE SW CORNER OF WASHINGTON AVENUE AND WOODSIDE AVENUE.

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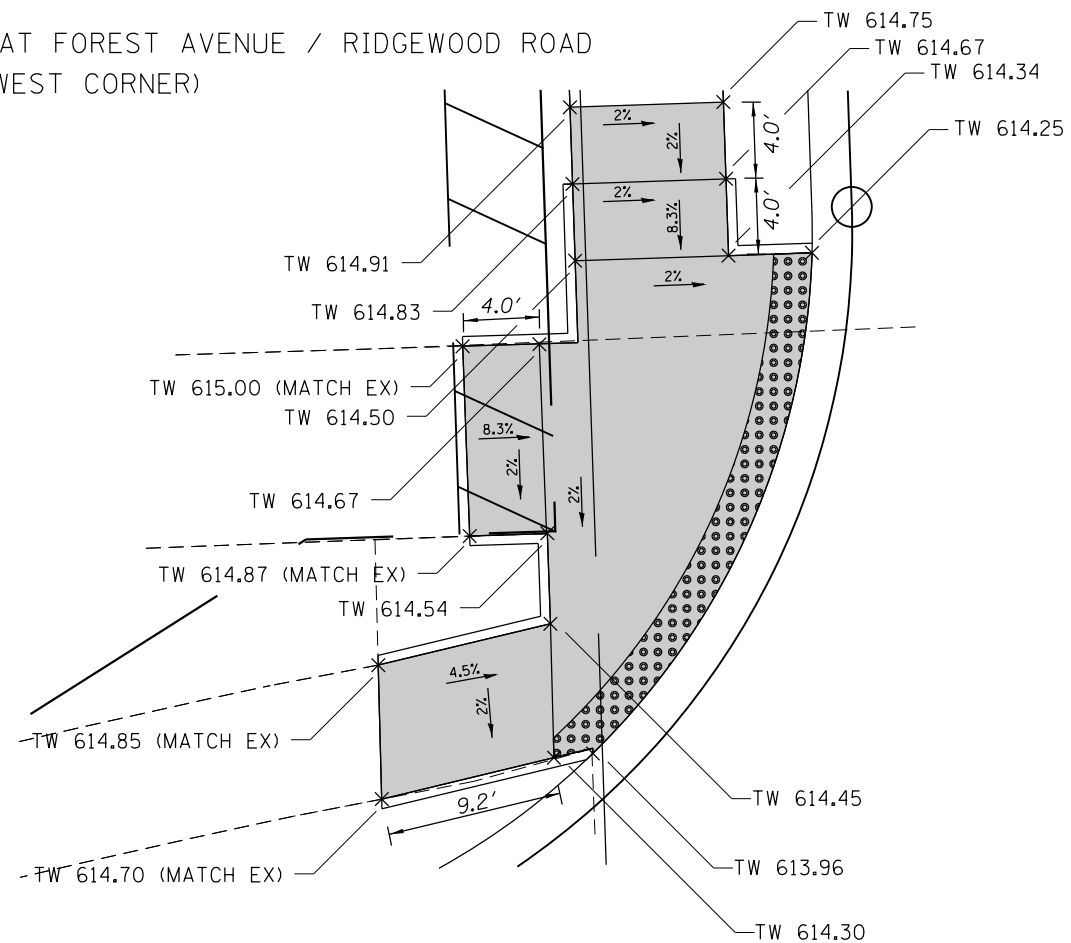
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD.  
SIDEWALK DETAIL**

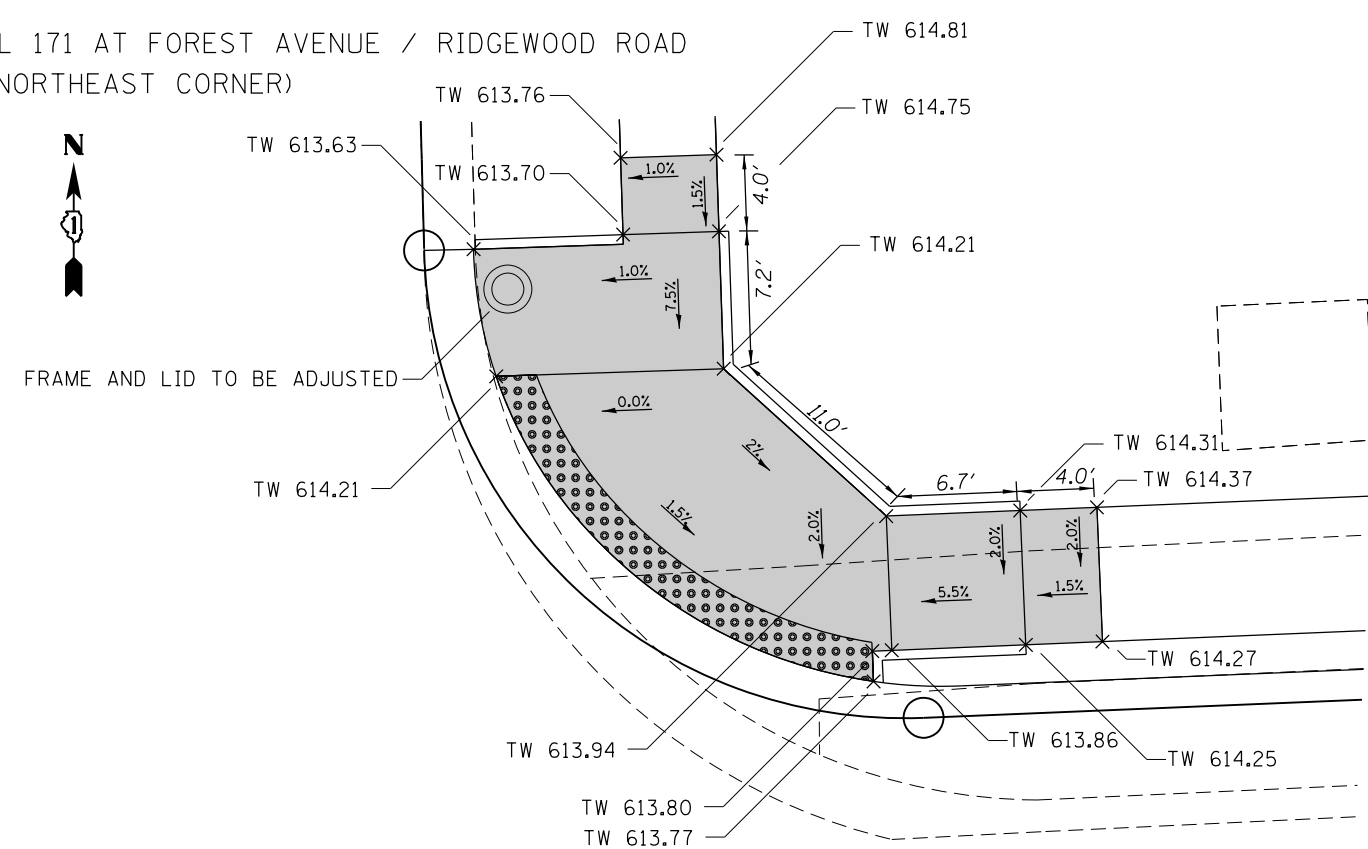
SCALE: 1:5 SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	66
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				

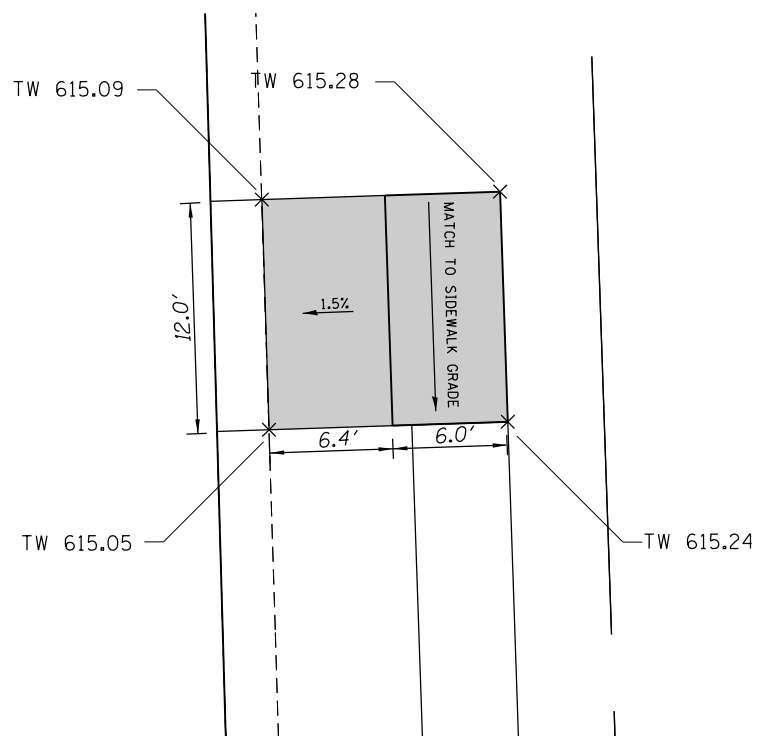
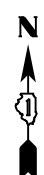
IL 171 AT FOREST AVENUE / RIDGEWOOD ROAD  
(NORTHWEST CORNER)



IL 171 AT FOREST AVENUE / RIDGEWOOD ROAD  
(NORTHEAST CORNER)



BUS PAD  
STA 43+16, 39' RT



LEGEND



DETECTABLE WARNINGS

TW TOP OF WALK  
PROPOSED ELEVATION

GENERAL NOTES:

1. PROPOSED ELEVATIONS ARE APPROXIMATE.
2. REF: BM ELEV 619.69, LOCATED ON THE NORTH-NORTHWEST BONNET BOLT ON THE FIRE HYDRANT AT THE SW CORNER OF WASHINGTON AVENUE AND WOODSIDE AVENUE.

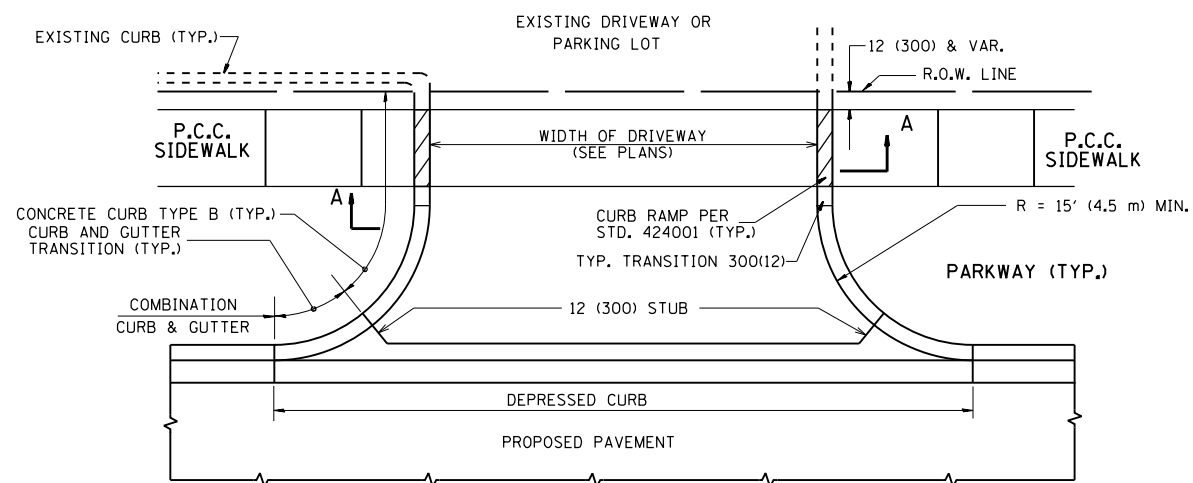
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Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/31/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

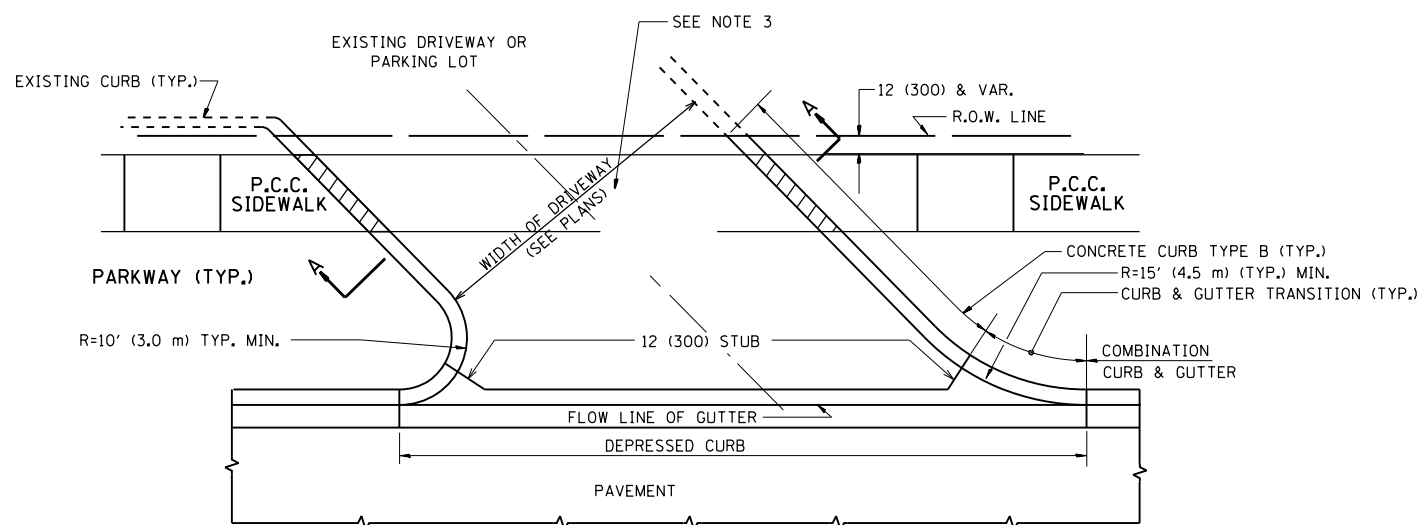
IL. ROUTE 171 AT FOREST AVE./RIDGEWOOD RD.  
SIDEWALK DETAIL

SCALE: 1:5 SHEET OF SHEETS STA. TO STA.

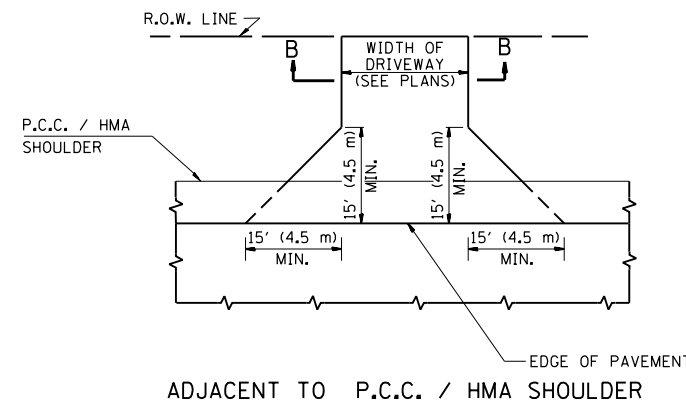
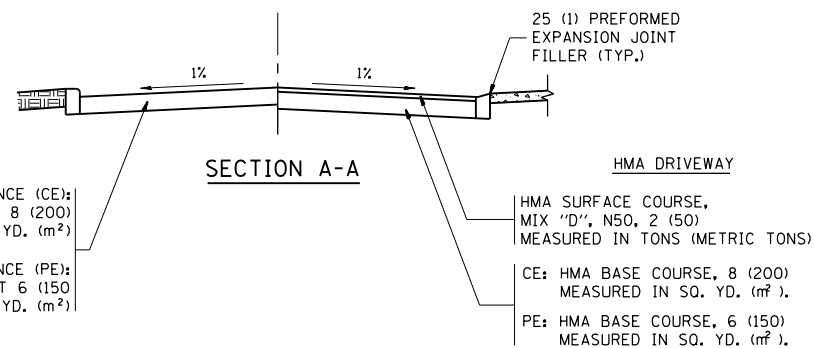
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	67
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				



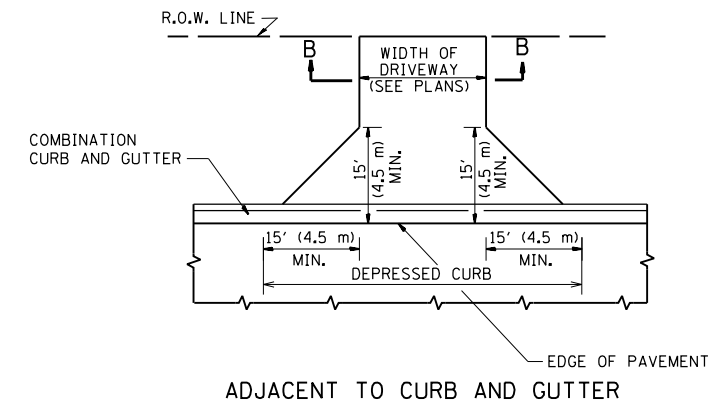
WITH CONCRETE CURB, TYPE B



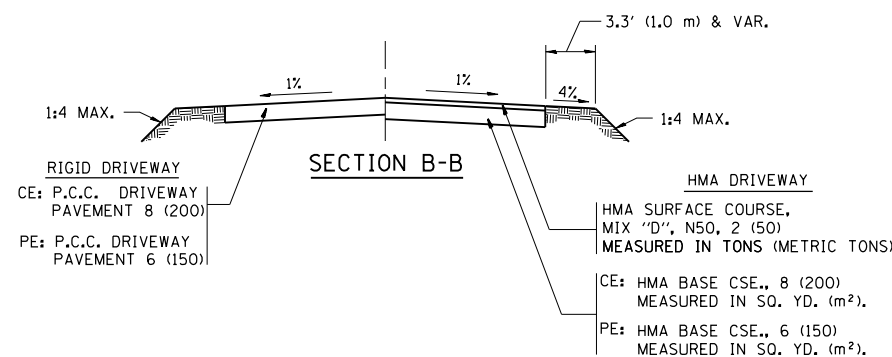
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX 'D', N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m²).

**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

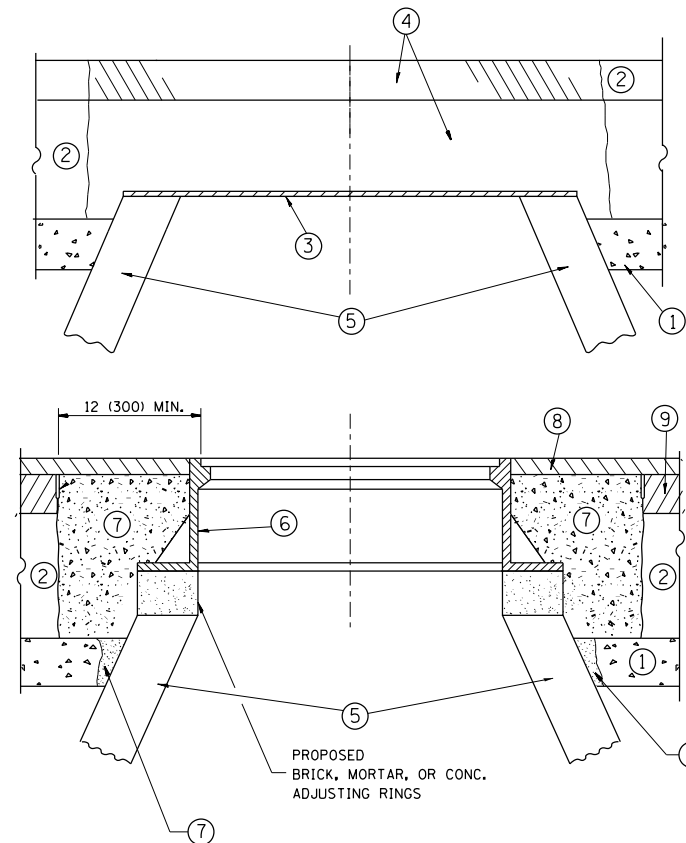
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	PLOT DATE = 3/31/2014	DATE - 11-04-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.  
AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	68
BD0156-07 (BD-01)			CONTRACT NO. 60X08	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

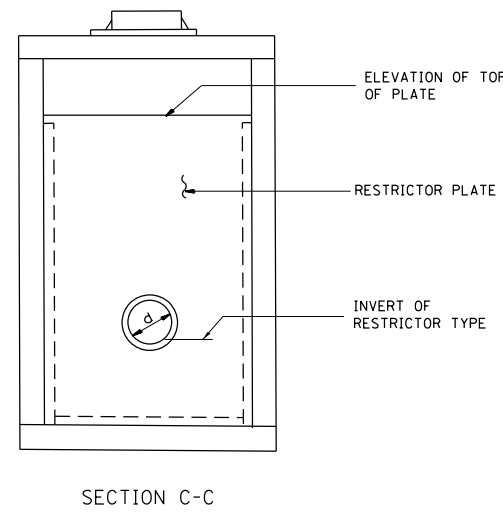
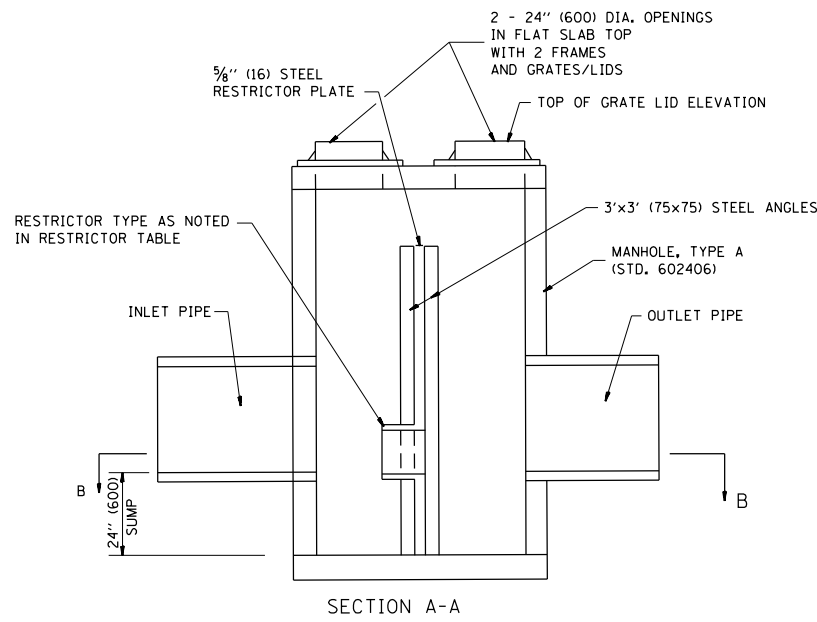
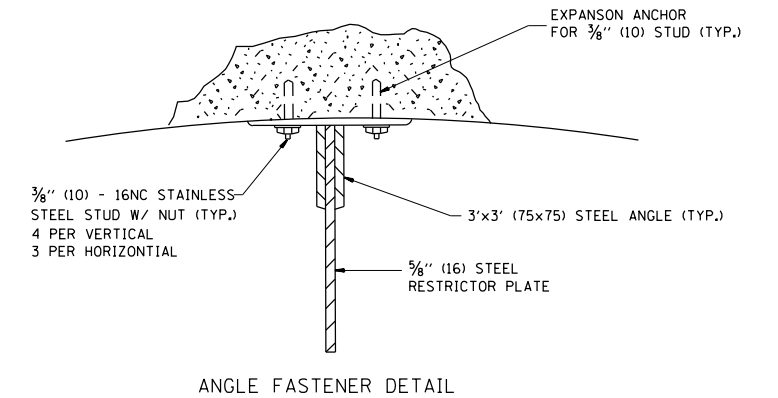
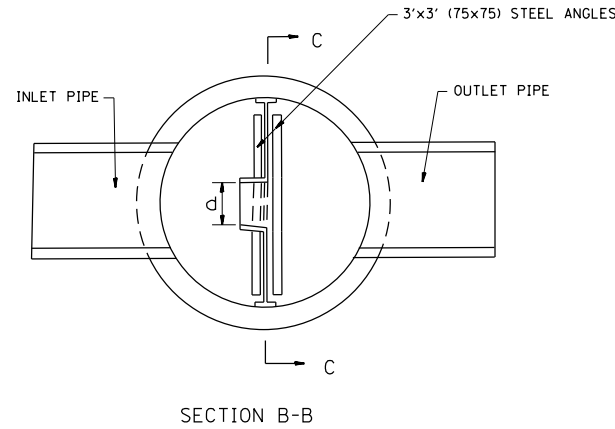
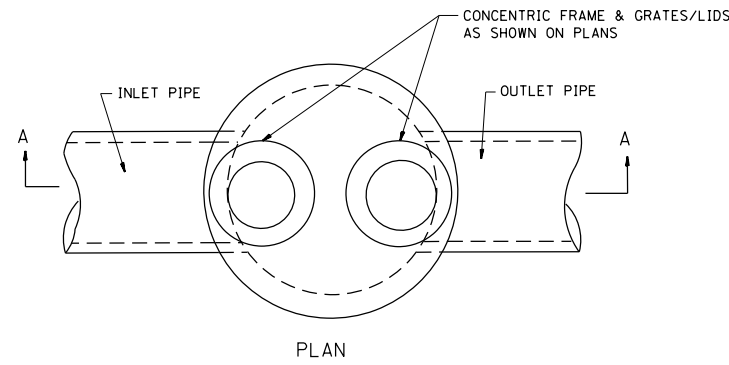
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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	PLOT DATE = 3/31/2014	DATE - 10-25-94	REVISED - R. BORO 12-06-11

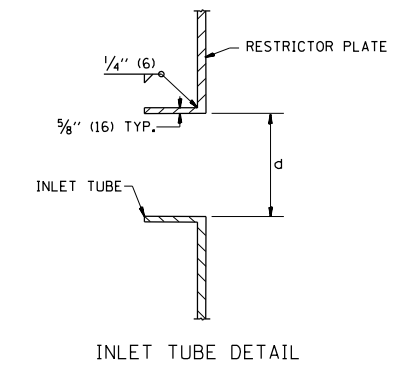
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

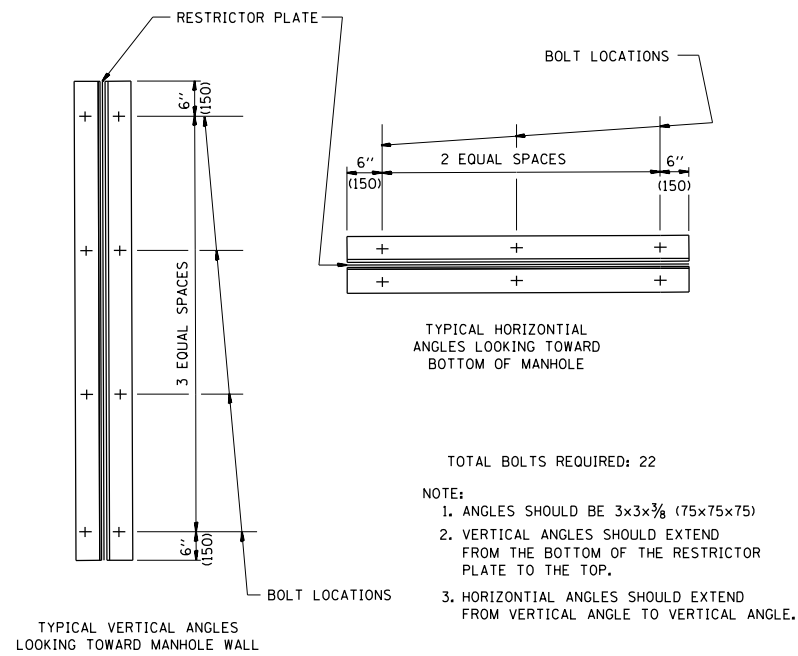
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0303S-TS	COOK	87	69
<b>BD600-03 (BD-8)</b>		<b>CONTRACT NO. 60X08</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- NOTES:
1. ALL STEEL ANGLES AND PLATES TO BE GALVANIZED AFTER FABRICATION.
  2. ALL RESTRICTOR PLATES, ANGLES AND HARDWARE TO BE INCLUDED IN THE COST OF THE MANHOLE.
  3. BASIS OF PAYMENT: "MANHOLES, TYPE A, 6 FT. (1.8 m)-DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE" EACH



STATION	MANHOLE DIAMETER	FRAME AND GRATE	RESTRICTOR TYPE	INSIDE RESTRICTOR TYPE DIAMETER in. (mm) (d)	INVERT OF RESTRICTOR TYPE	ELEVATION OF TOP OF PLATE OVERFLOW
42+62.6	4'	T1 FR & CLID	3	9	609.54	613.11



RESTRICTOR TYPE					
1	2	3	4	5	6
RE-ENTRANT TUBE	SHARP EDGED	SQUARE EDGED	RE-ENTRANT TUBE	SQUARE EDGED	ROUNDED
LENGTH: 1/2 TO 1 DIA.		STREAM CLEARS SIDES	LENGTH: 2-1/2 DIA.	LENGTH: 2-1/2 DIA.	
C=.52	C=.61	C=.61	C=.73	C=.82	C=.98

VALUES OF "C" FOR CIRCULAR AND SQUARE ORIFICES

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

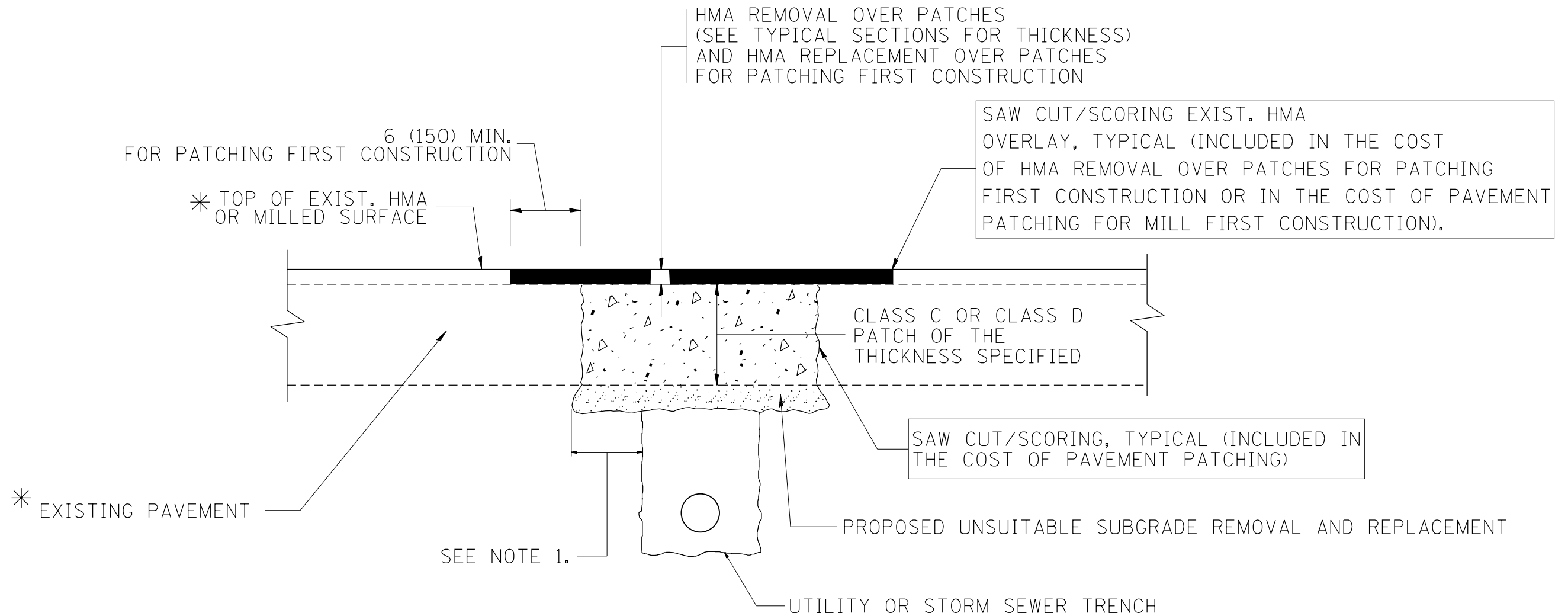
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		CHECKED -	REVISED - M. GOMEZ 01-08-01
		DATE - 09-09-94	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

MANHOLE WITH  
RESTRICTOR PLATE

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0303S-TS	COOK	87	70
BD600-04 (BD-12)		CONTRACT NO. 60X08		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

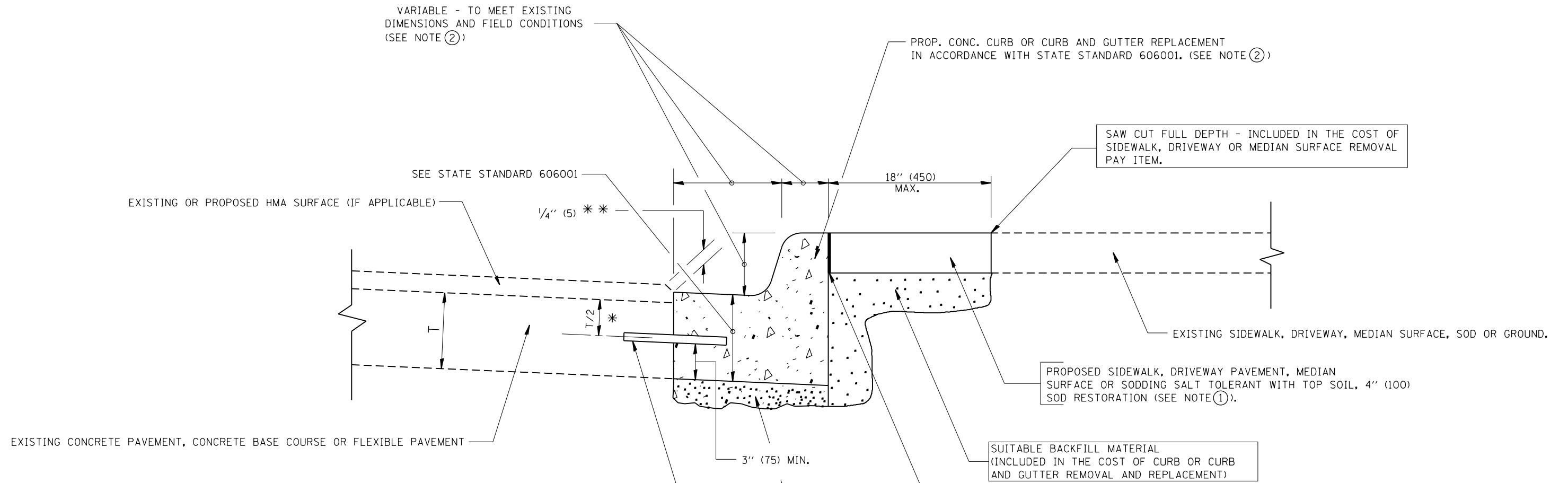
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	PLOT DATE = 3/31/2014	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR  
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0303S-TS	COOK	87	71
BD400-04 (BD-22)			CONTRACT NO. 60X08	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
  - \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
  - ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
  - ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
  - ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
  - ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

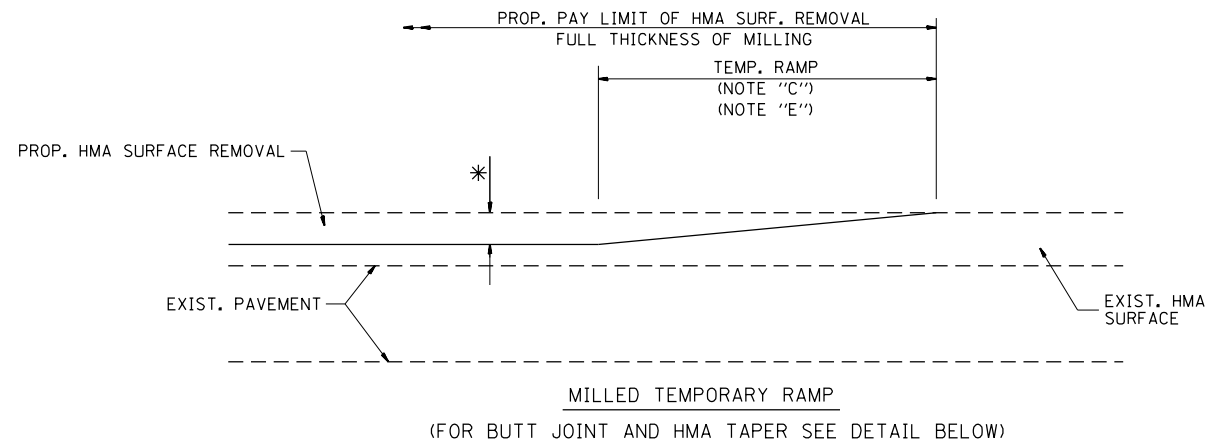
**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

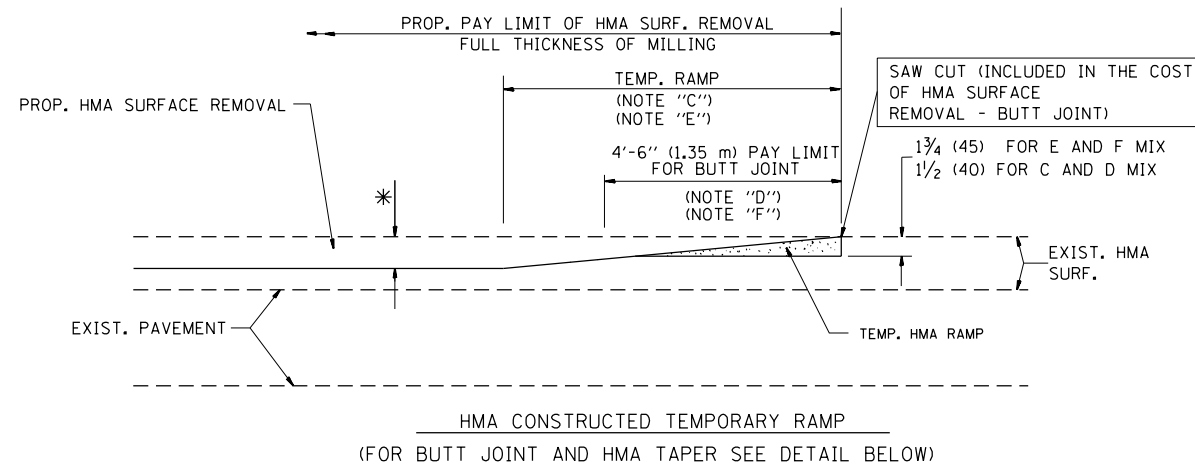
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - A. ABBAS 03-21-97			372	0303S-TS	COOK	87	72	
		CHECKED -	REVISED - M. GOMEZ 01-22-01			<b>BD600-06 (BD-24)</b>		<b>CONTRACT NO. 60X08</b>			
		DATE - 03-11-94	REVISED - R. BORO 12-15-09			SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.	



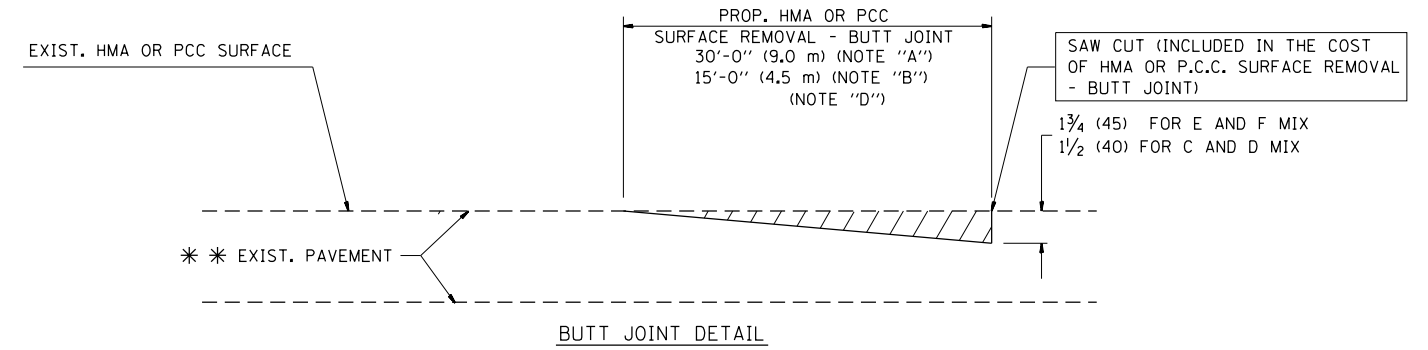


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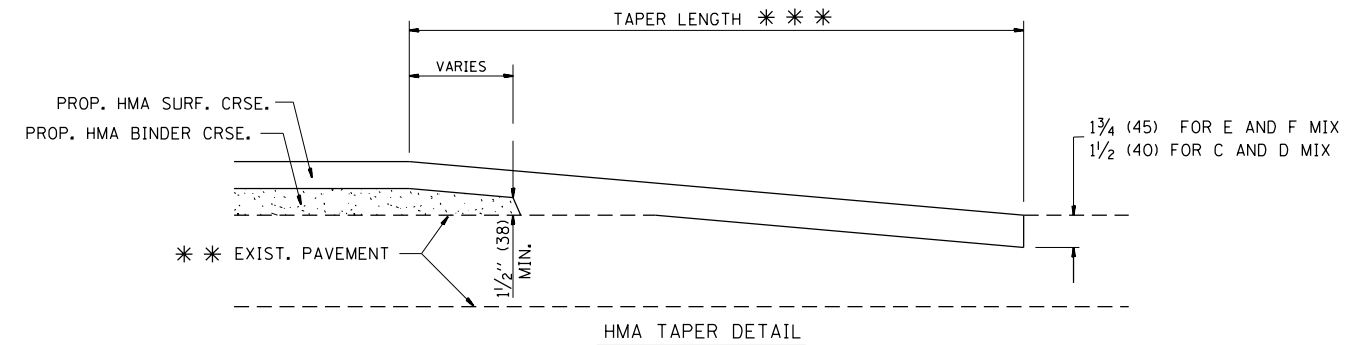


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\* \* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

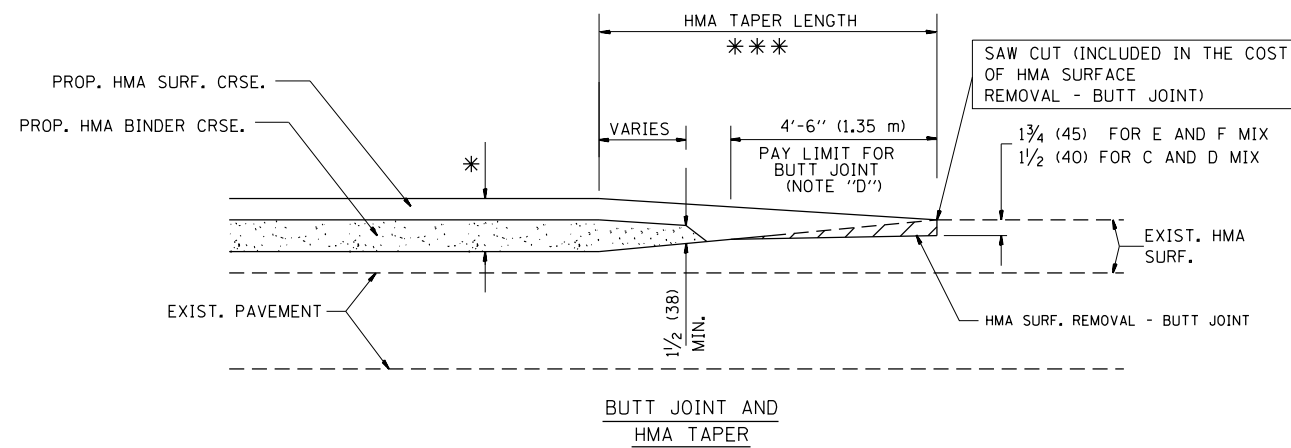
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



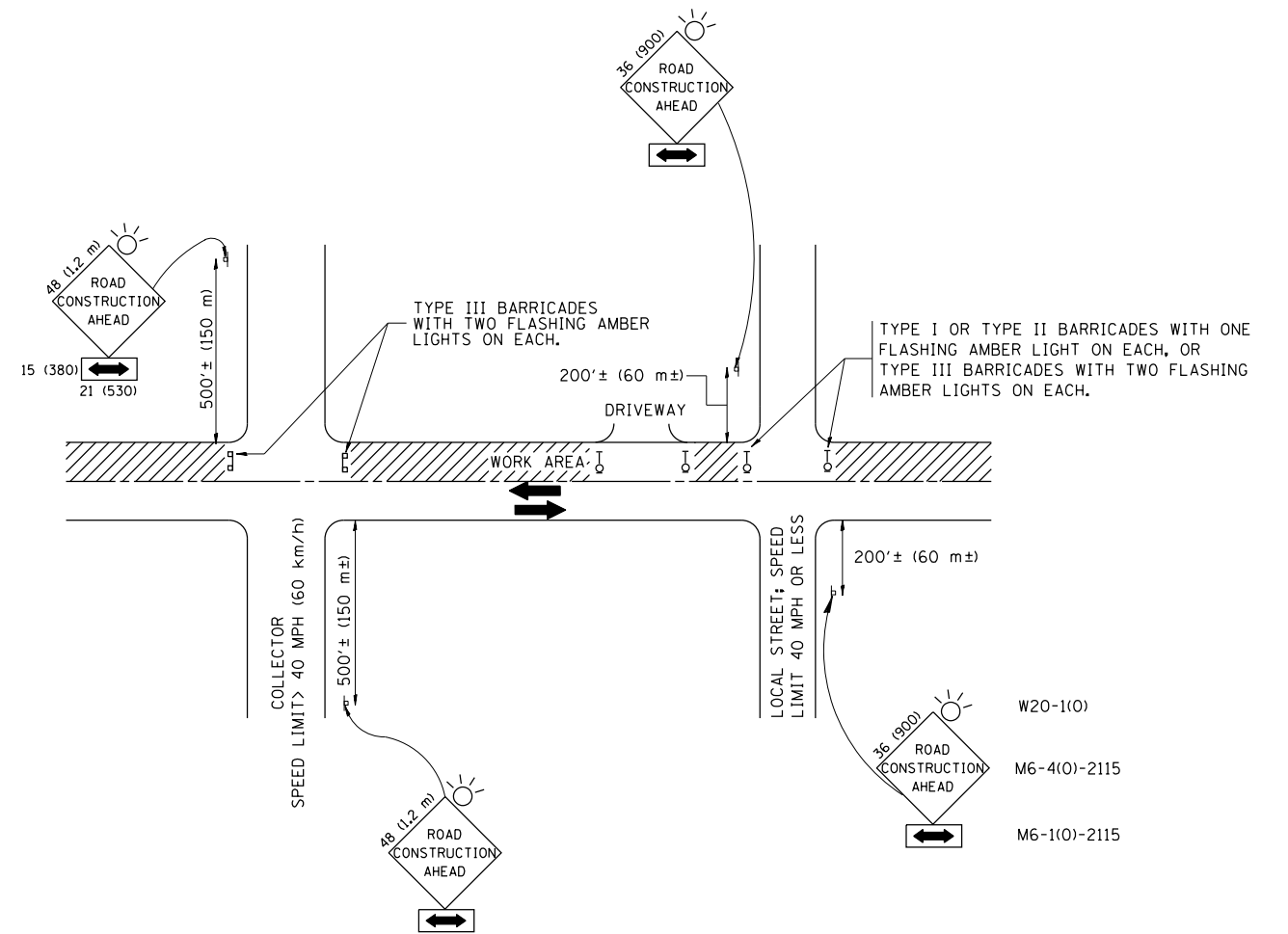
**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

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	PLOT DATE = 3/31/2014	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0303S-TS	COOK	87	73
<b>BD400-05 BD32</b>		<b>CONTRACT NO. 60X08</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



## TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

### NOTES:

#### A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

#### B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

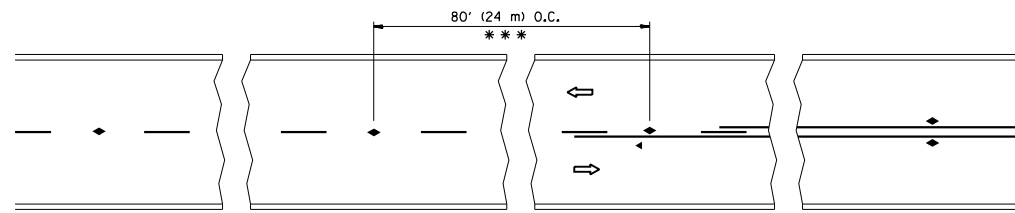
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	PLOT DATE = 3/31/2014	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

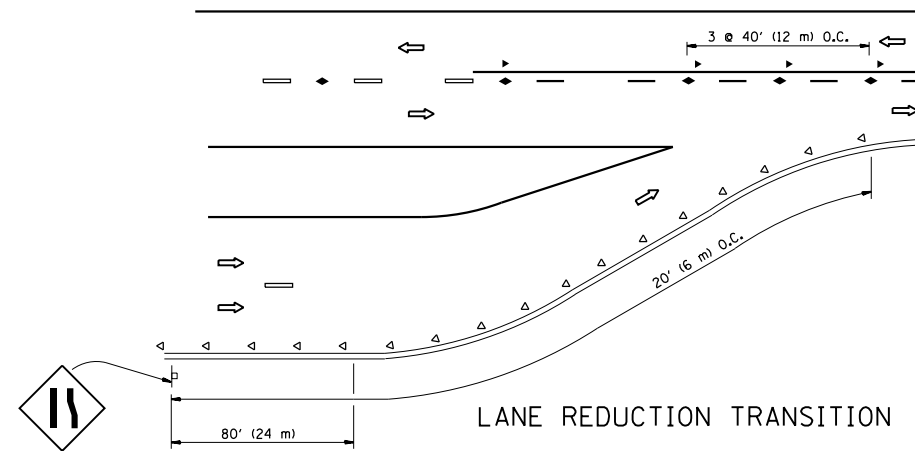
SCALE: NONE    SHEET NO. 1 OF 1 SHEETS    STA.    TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0303S-TS	COOK	87	74
<b>TC-10</b>			<b>CONTRACT NO. 60X08</b>	
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				

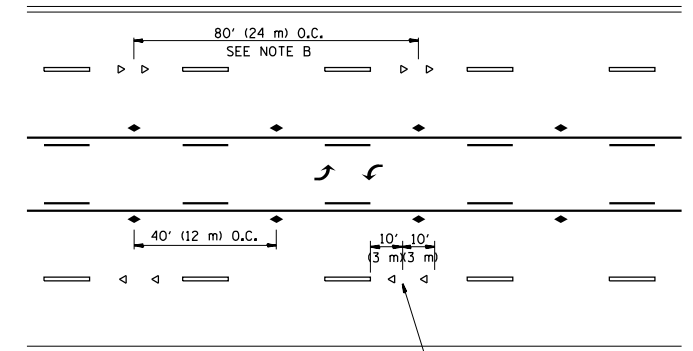


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

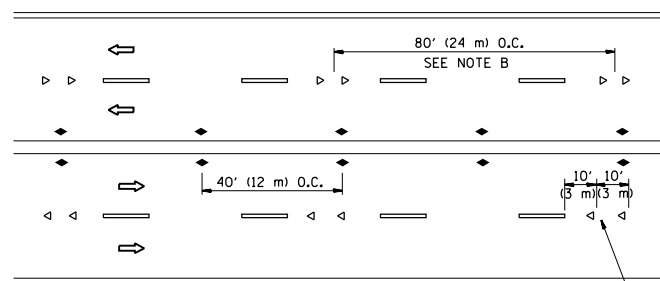
TWO-LANE/TWO-WAY



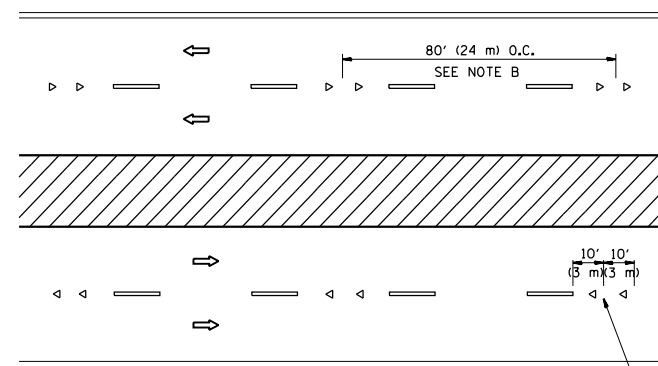
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

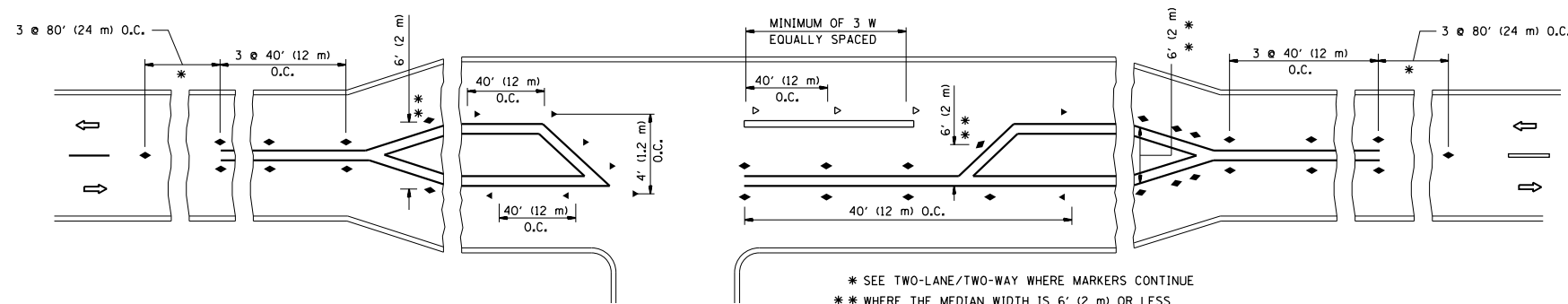
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

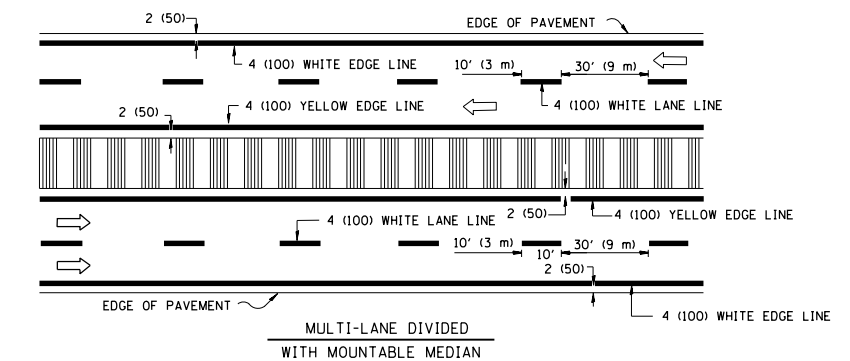
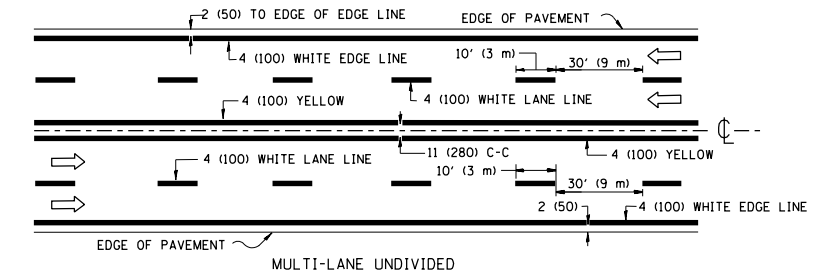
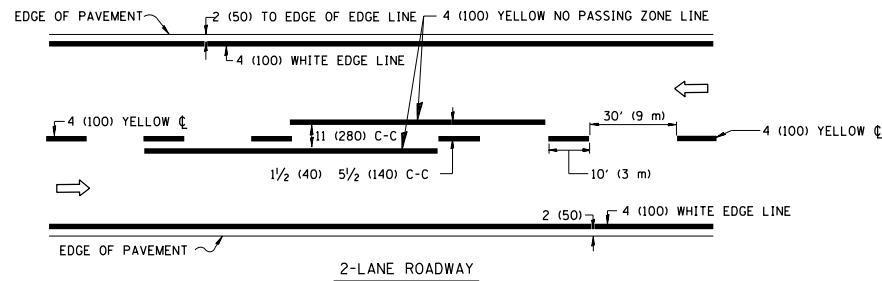
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = Tariqf	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
et:\pw\work\p\id\dot\tariqf\m\0315748\Dist\Std.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 100.0002' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 3/31/2014	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

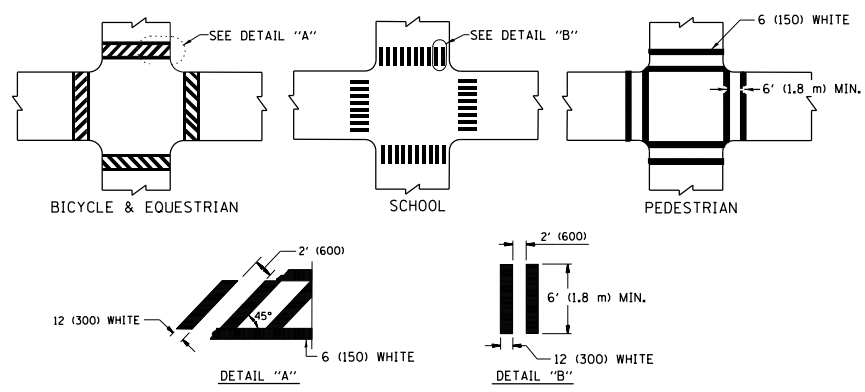
TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0303S-TS	COOK	87	75
TC-11			CONTRACT NO. 60X08	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

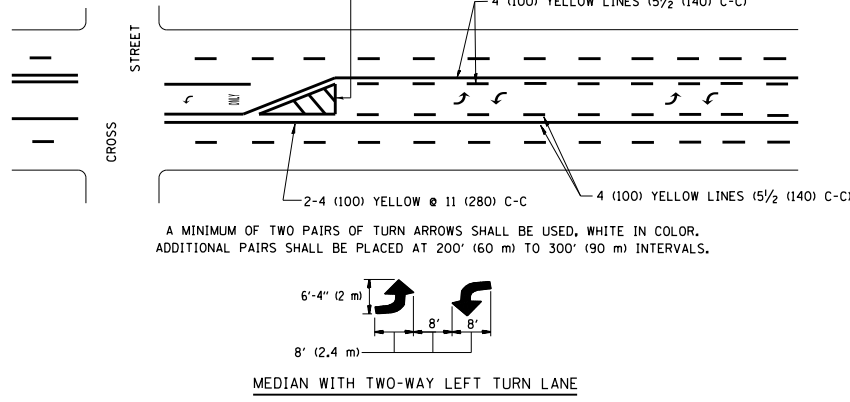
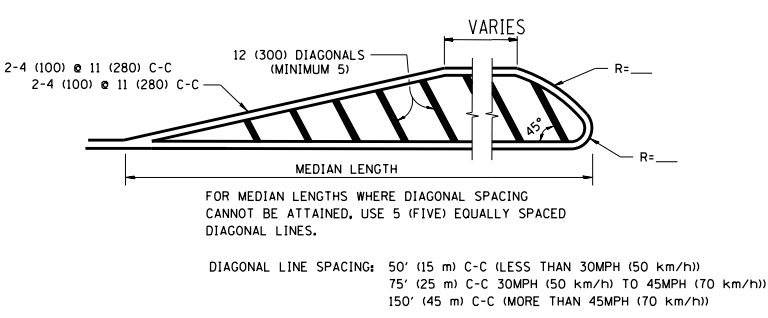
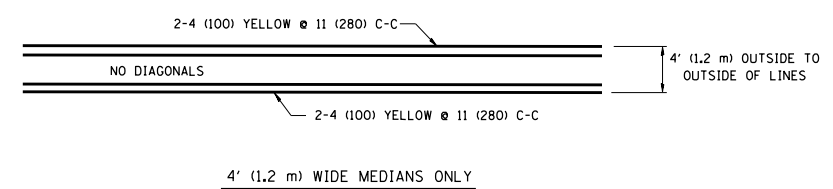


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

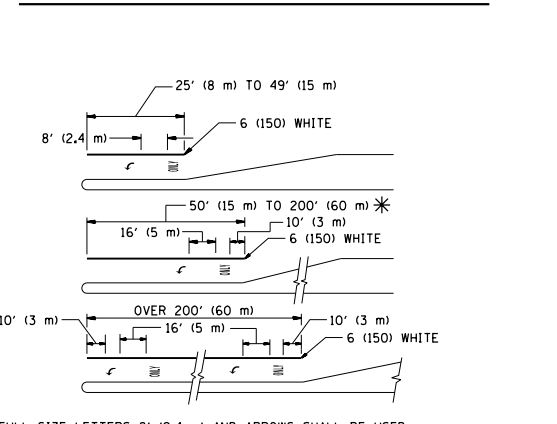
**TYPICAL LANE AND EDGE LINE MARKING**



**TYPICAL CROSSWALK MARKING**



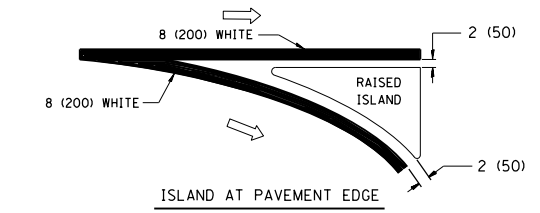
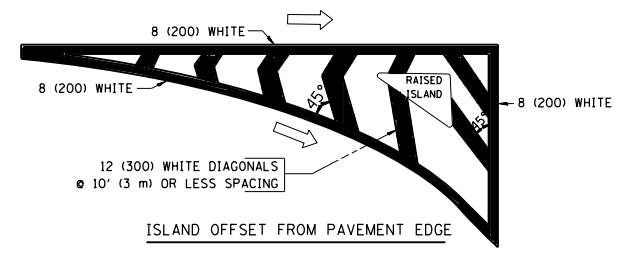
**TYPICAL PAINTED MEDIAN MARKING**



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

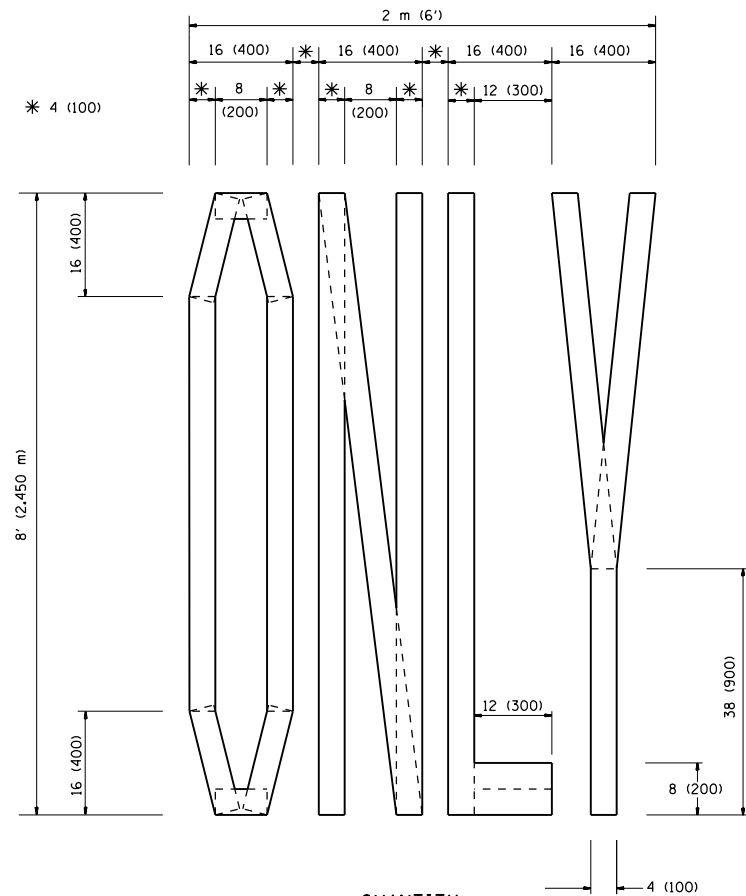


**TYPICAL ISLAND MARKING**

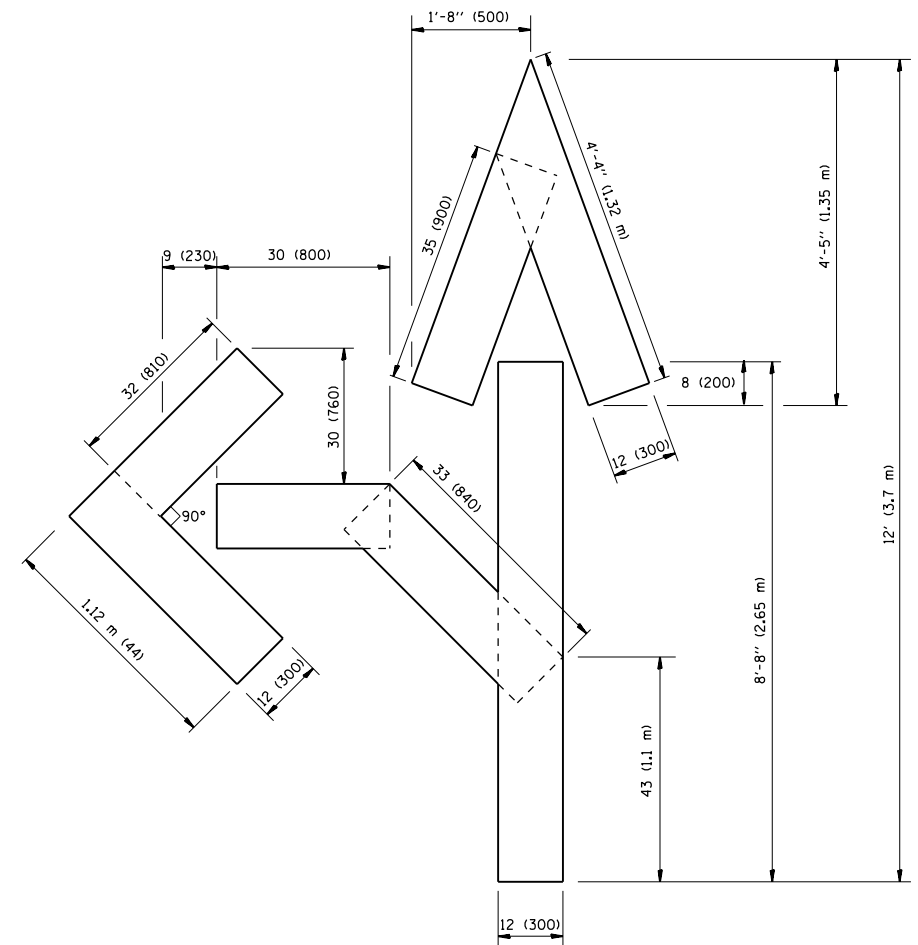
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R": 3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X": 54.0 SQ. FT. (5.0 m <sup>2</sup> ) EACH
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

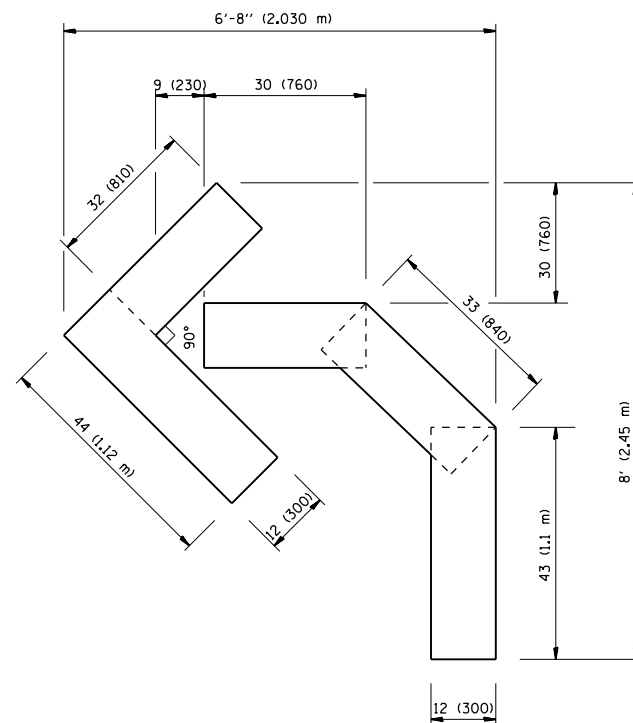
All dimensions are in inches (millimeters) unless otherwise shown.



QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

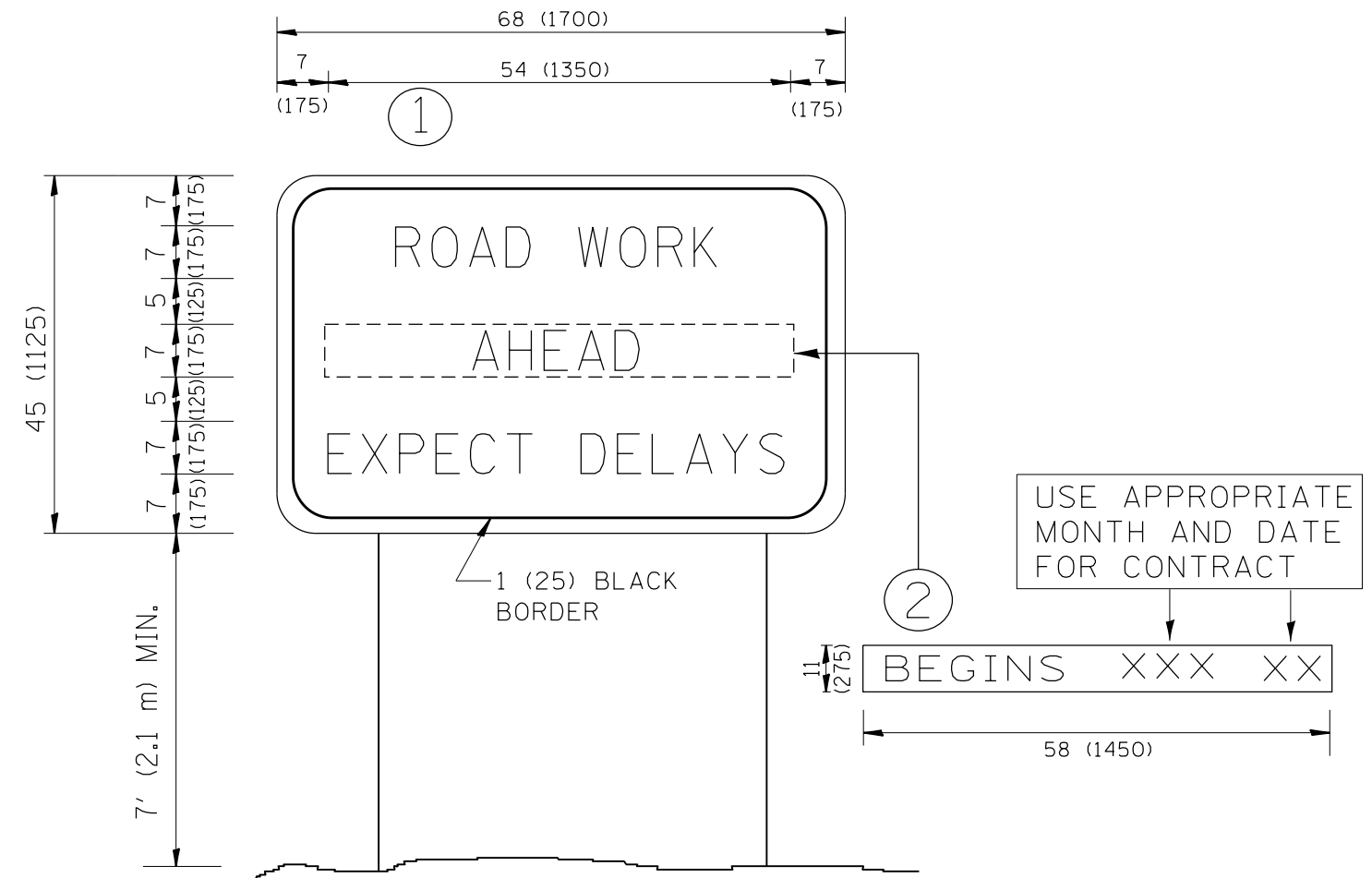
FILE NAME =	USER NAME = Tariqf	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
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		CHECKED -	REVISED -T. RAMMACHER 03-02-98
		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0303S-TS	COOK	87	77
TC-16		CONTRACT NO. 60X08		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

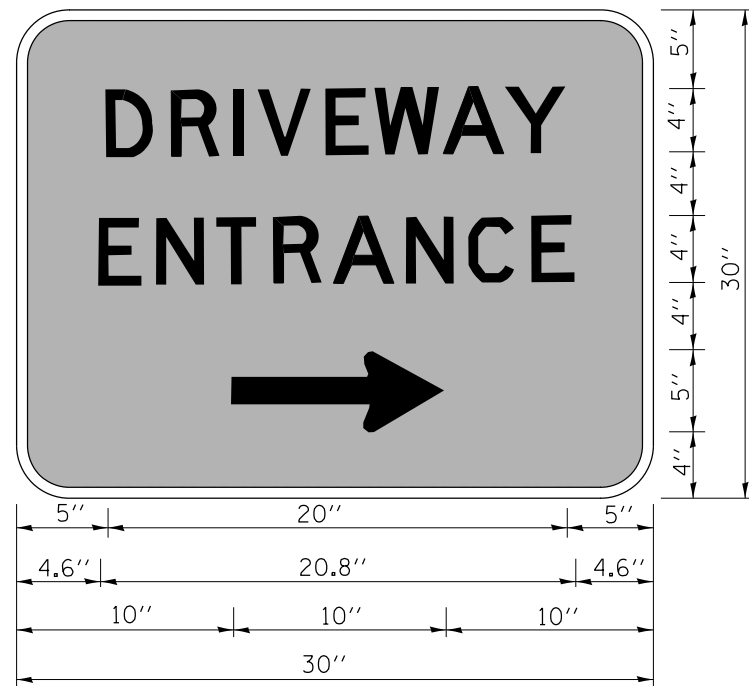


**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = TariqM	DESIGNED -	REVISED - R. MIRS 09-15-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\tariqm\d0315748\DistS	DRAWN -	REVISED - R. MIRS 12-11-97	372			0303S-TS	COOK	87	78	
PLOT SCALE = 100.0002' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	<b>TC-22</b>			<b>CONTRACT NO. 60X08</b>				
PLOT DATE = 3/31/2014	DATE -	REVISED - C. JUCIUS 01-31-07	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT							
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE  
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)  
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY  
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE  
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = TariqM	DESIGNED -	REVISED - C. JUCIUS 02-15-07
ct:\pw\work\p1dot\tariqM\d0315748\DistStd.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

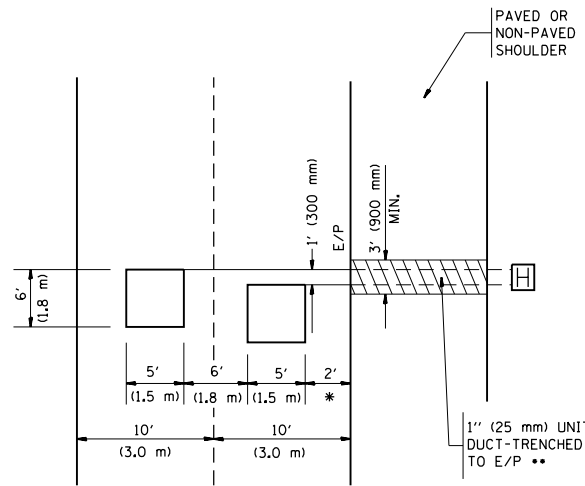
**DRIVEWAY ENTRANCE SIGNING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	0303S-TS	COOK	87	79
<b>TC-26</b>			<b>CONTRACT NO. 60X08</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



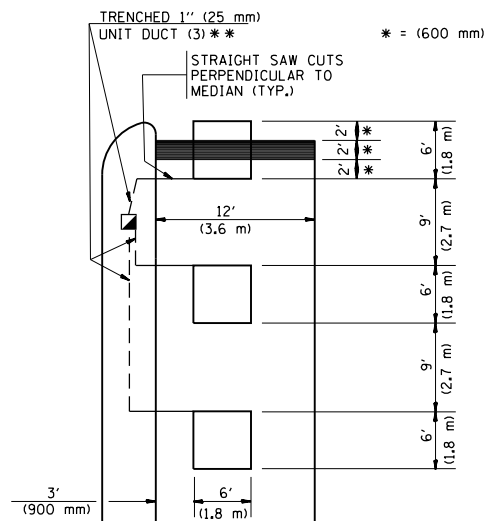
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



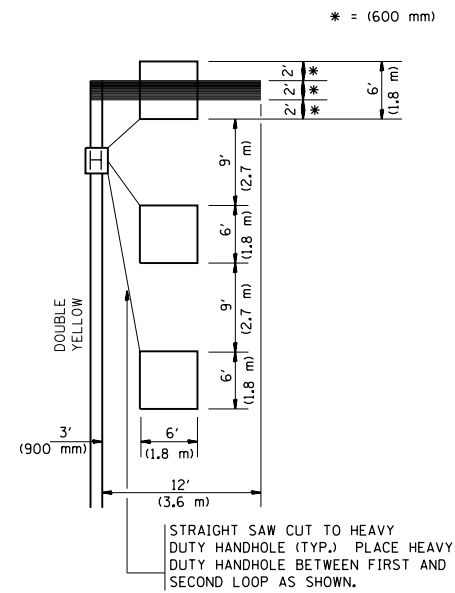
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



\* = (600 mm)

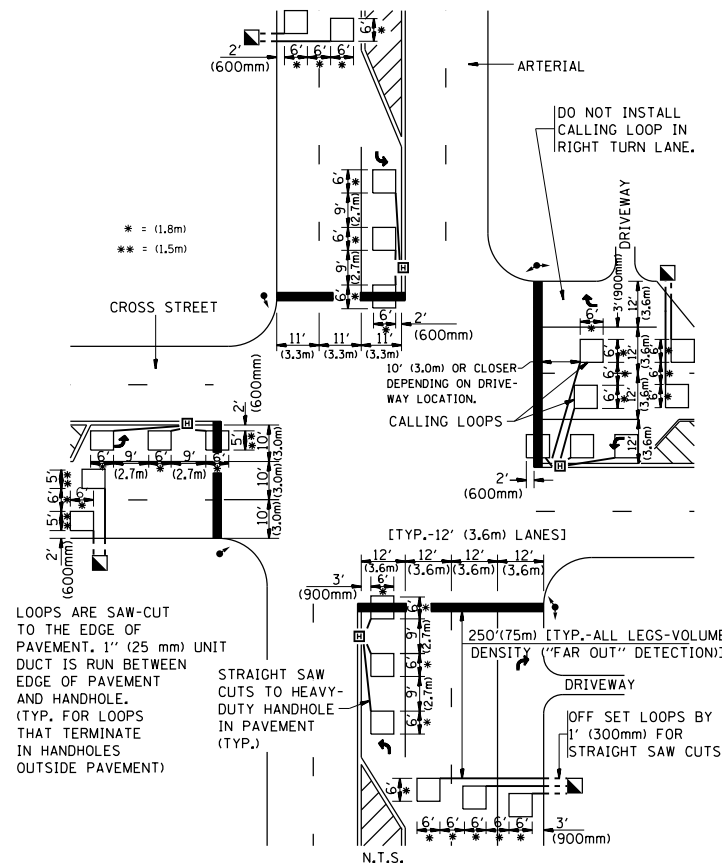
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



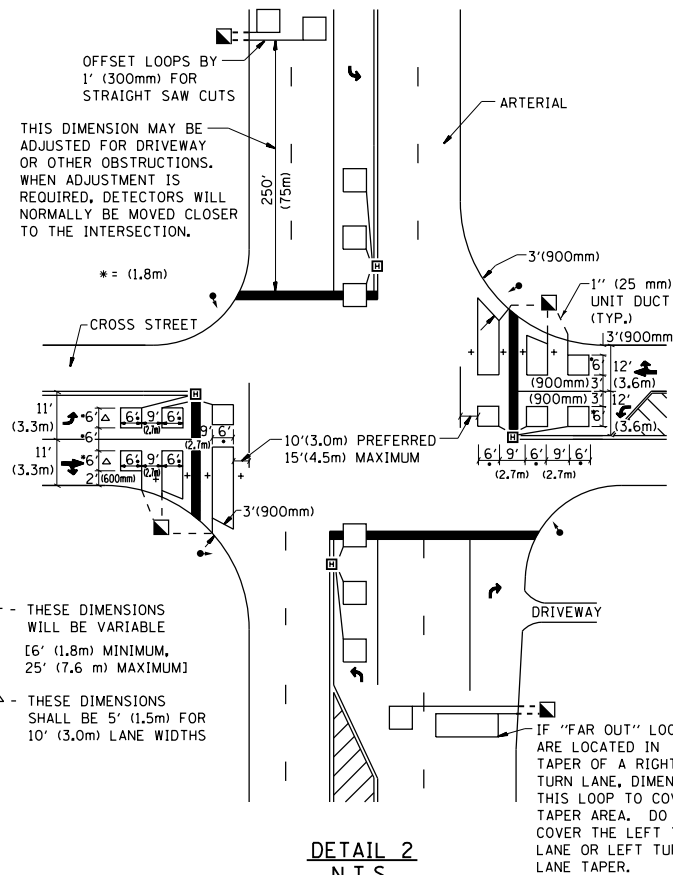
\* = (1.8m)  
\*\* = (1.5m)

LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

△ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

**DETAIL 2**  
N.T.S.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

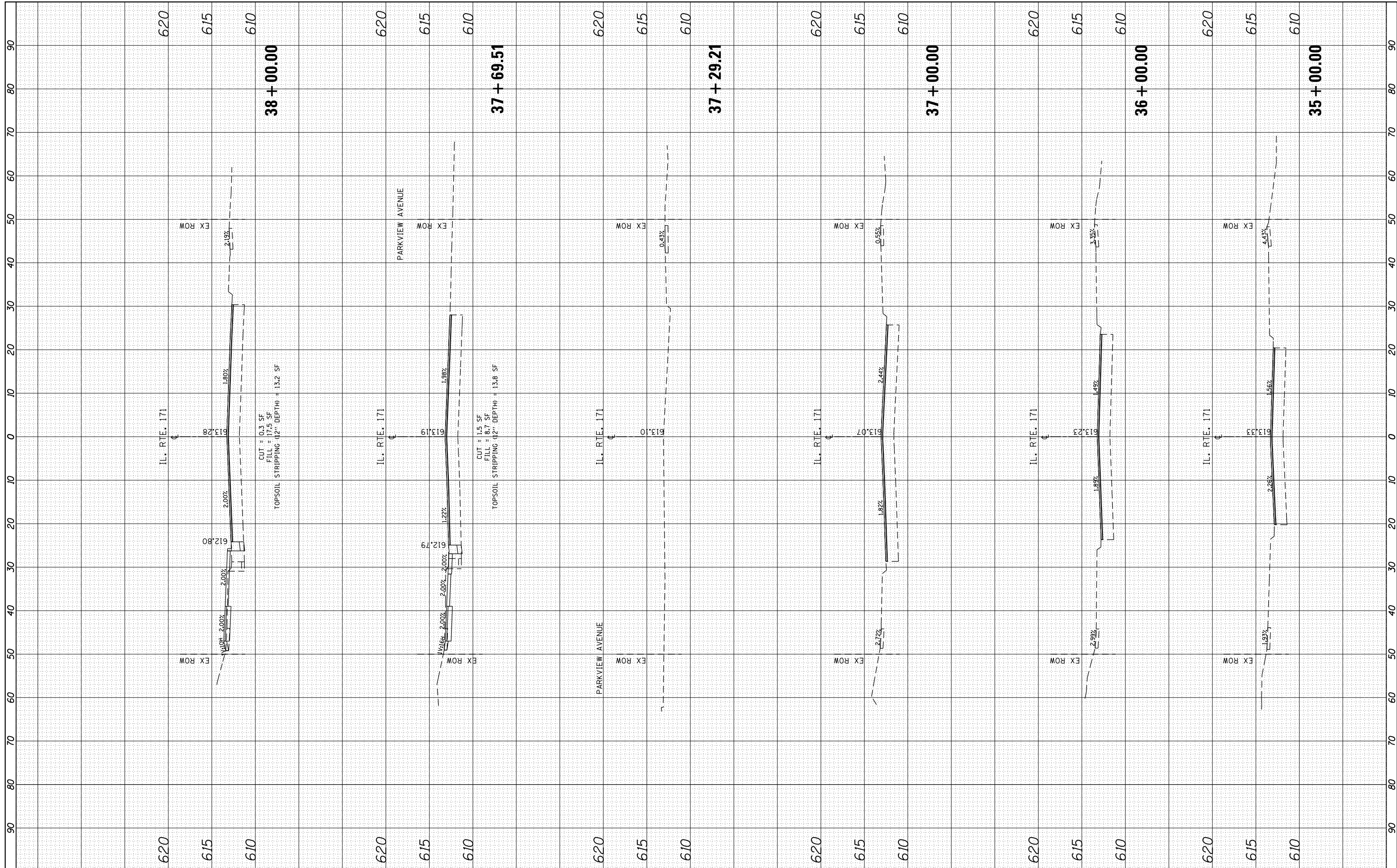
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = Tariqfm	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
et:\pw\work\p\dot\Tariqfm\d0315748\Dist1.dgn		DRAWN -	REVISED -			372	03035-TS	COOK	87	80	
		CHECKED - R.K.F.	REVISED -			<b>TS-07</b>		<b>CONTRACT NO. 60X08</b>			
		DATE -	REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



BY	DATE

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		



FILE NAME =  
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USER NAME = Teriqfm  
 PLOT SCALE = 20.0000' / in.  
 PLOT DATE = 3/31/2014

DESIGNED -  
 DRAWN -  
 CHECKED -  
 DATE -

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

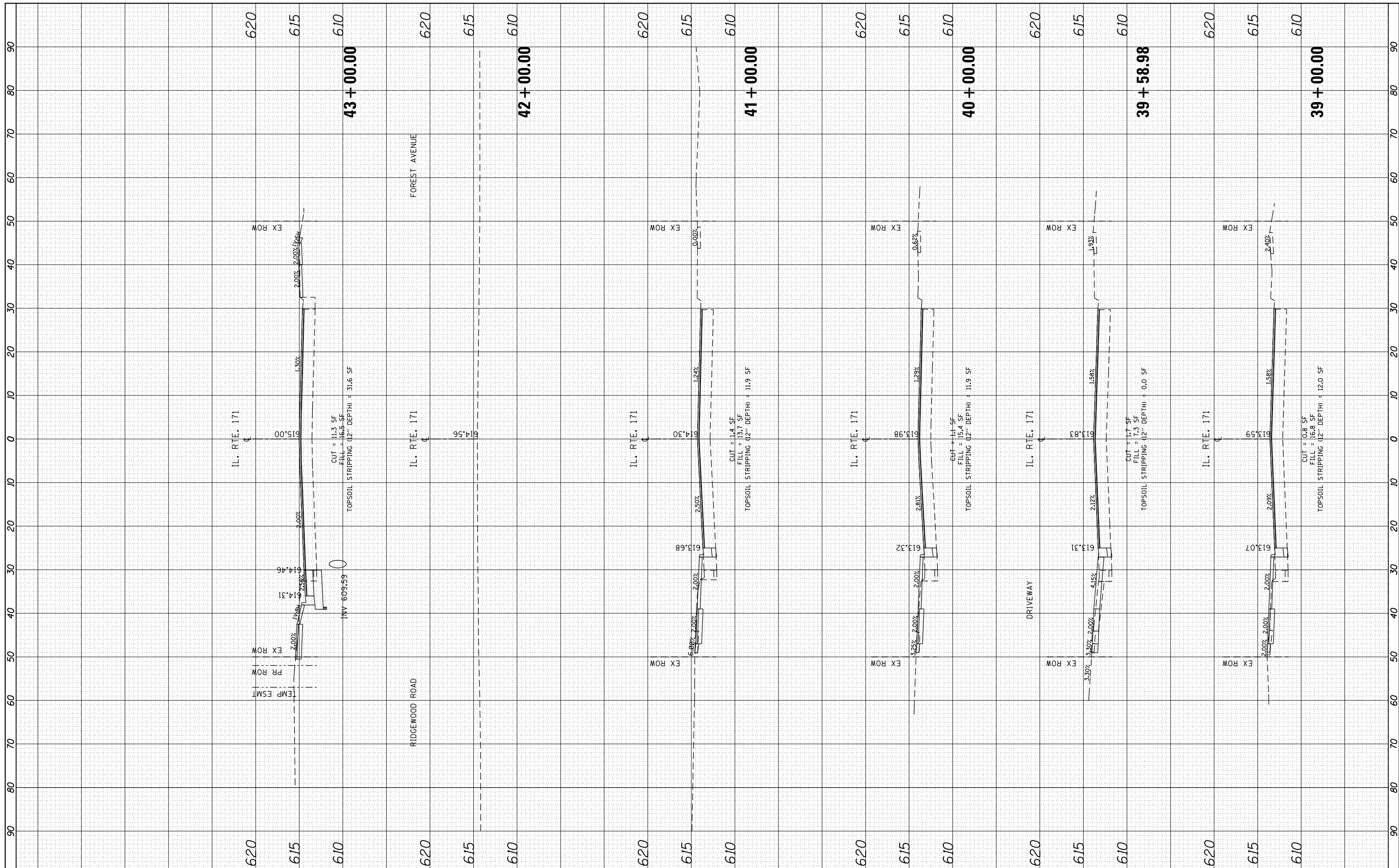
**IL ROUTE 171 AT FOREST AVE/RIDGEWOOD RD.  
 CROSS SECTIONS - IL. ROUTE 171**

SCALE: 1"=10' (H) SHEET 1 OF 4 SHEETS STA. 35+00.00 TO STA. 38+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	81
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				

BY	DATE

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		
	AREAS CHECKED		



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Default	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/31/2014	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

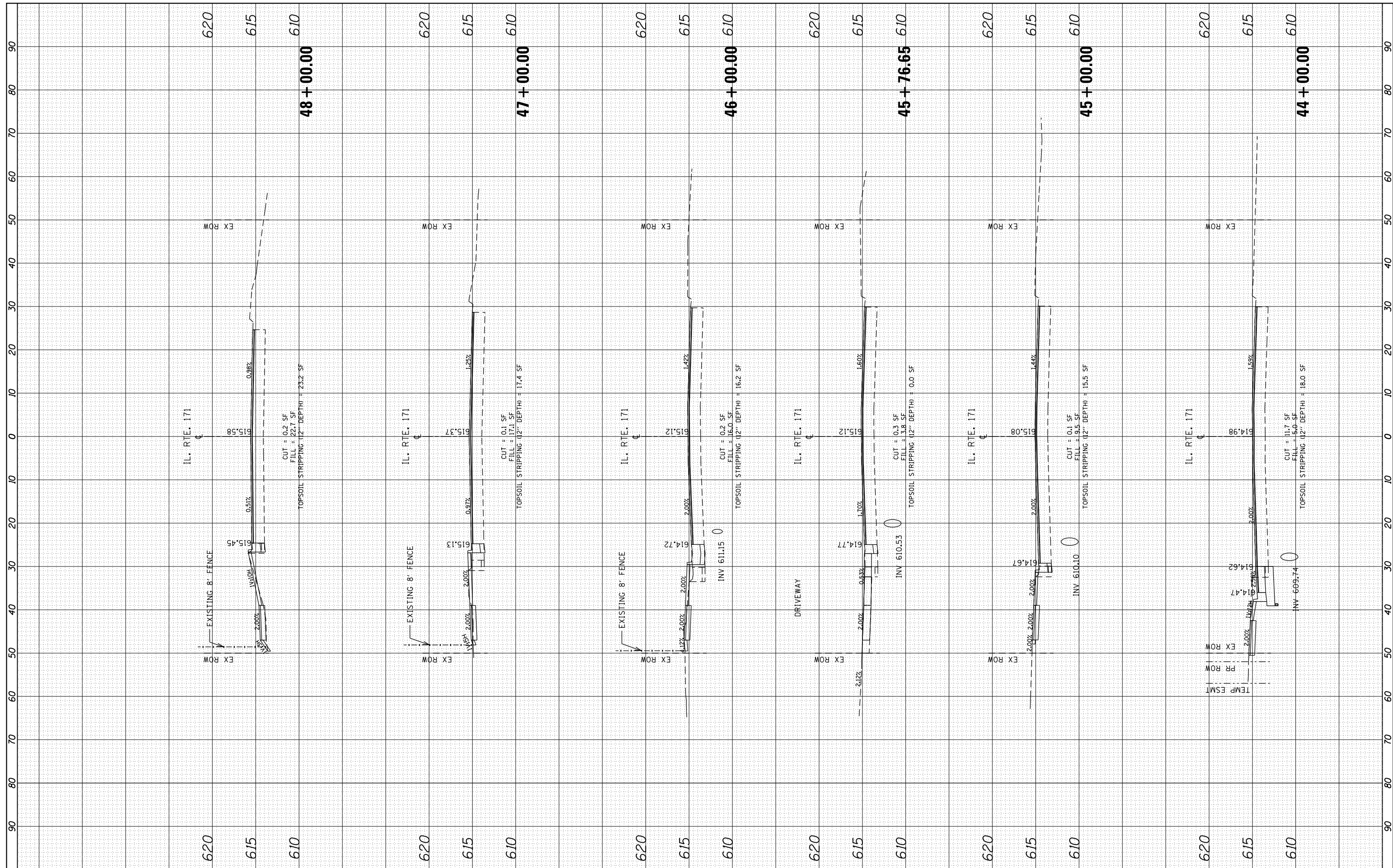
**IL ROUTE 171 AT FOREST AVE/RIDGEWOOD RD.  
CROSS SECTIONS - IL. ROUTE 171**

SCALE: 1"=10' (H)    SHEET 2 OF 4 SHEETS    STA. 39+00.00 TO STA. 43+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	82
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				

FINIAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		
	AREAS CHECKED		



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 PLOT DATE = 3/31/2014

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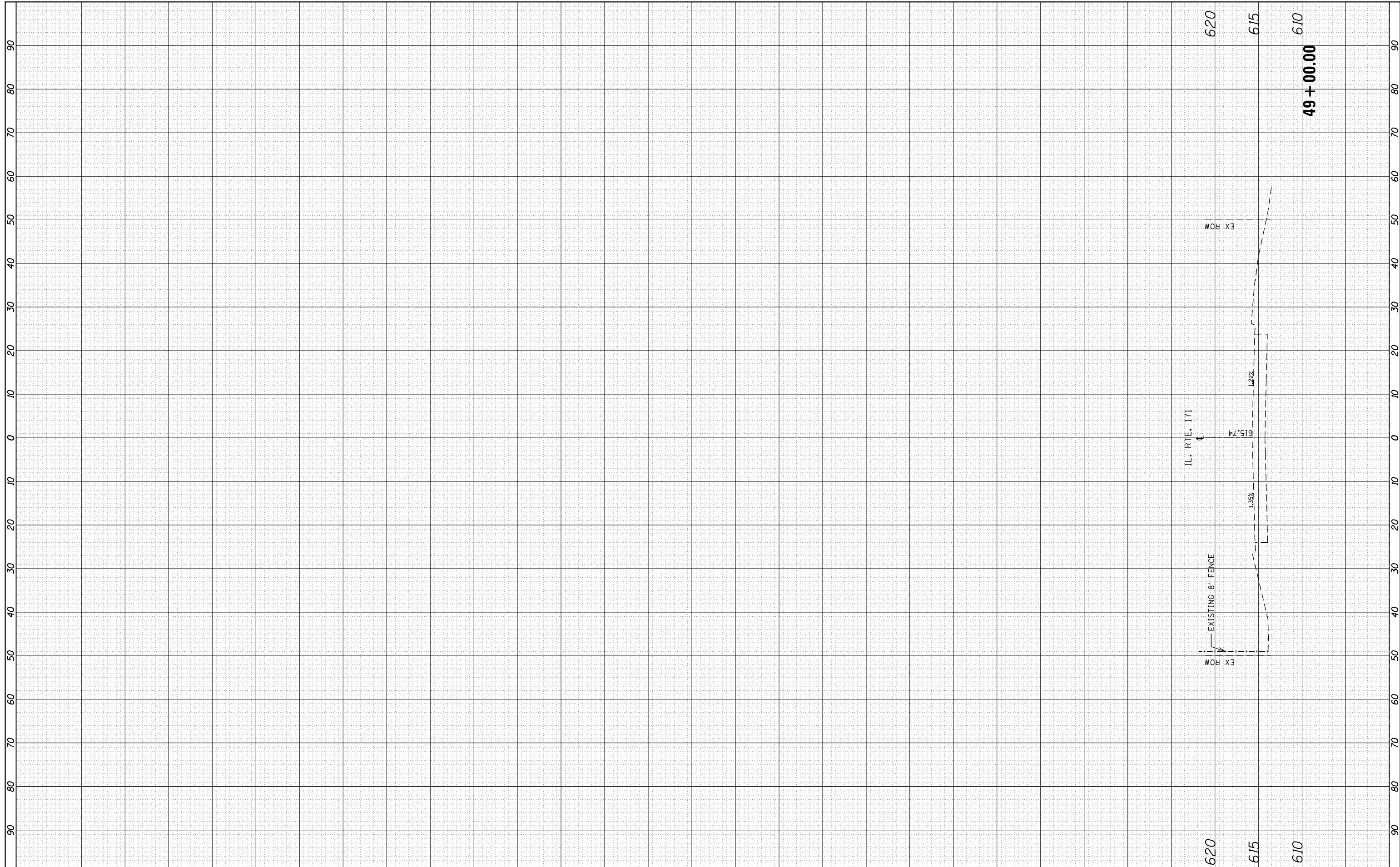
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 171 AT FOREST AVE/RIDGEWOOD RD.  
 CROSS SECTIONS - IL. ROUTE 171**  
 SCALE: 1"=10' (H) SHEET 3 OF 4 SHEETS STA. 44+00.00 TO STA. 48+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	83
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				

BY	DATE
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED

BY	DATE
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED



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 PLOT DATE = 3/31/2014

DESIGNED -  
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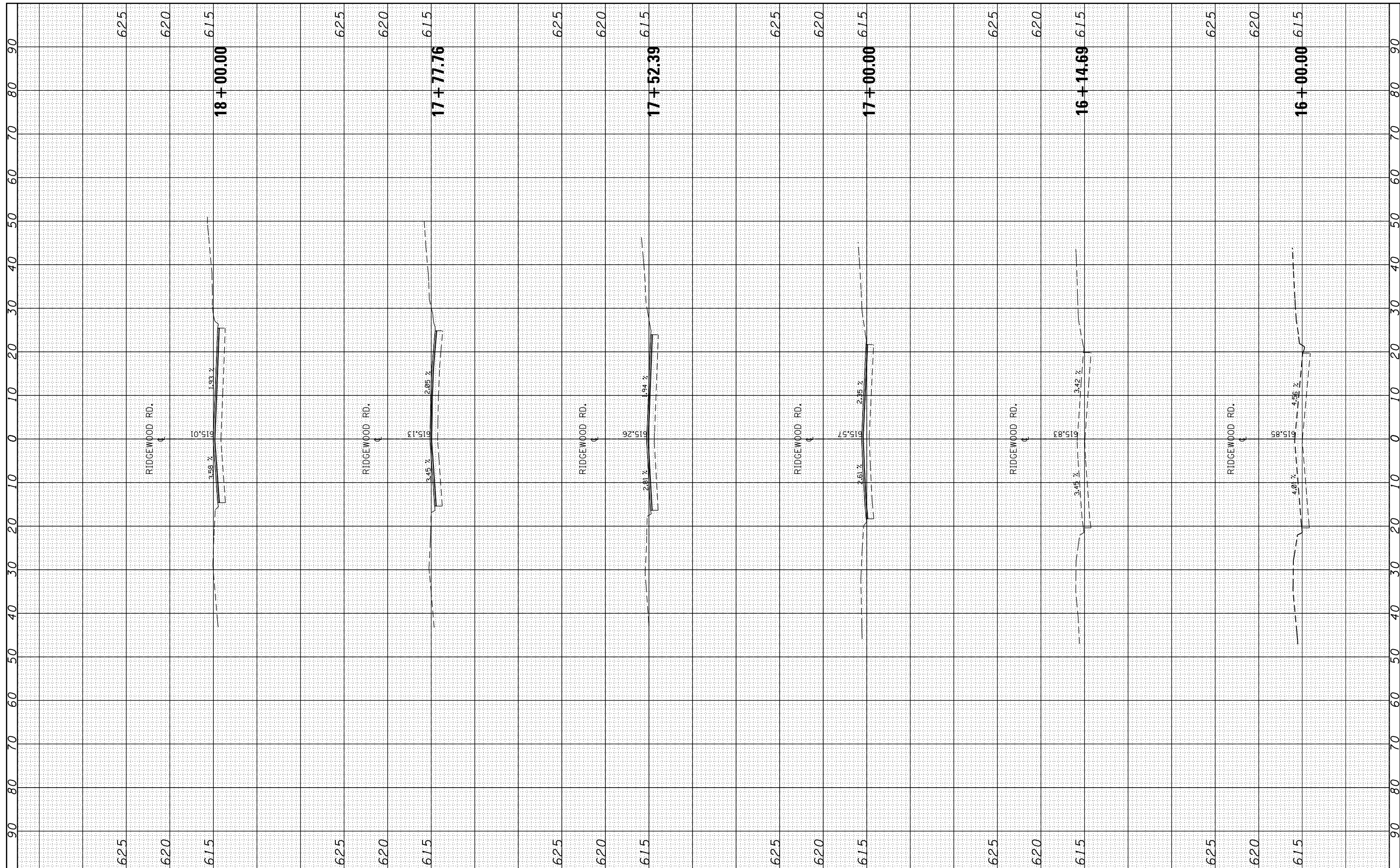
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 171 AT FOREST AVE/RIDGEWOOD RD.  
 CROSS SECTIONS - IL. ROUTE 171**  
 SCALE: 1"=10' (H) SHEET 4 OF 4 SHEETS STA. 49+00.00 TO STA. 49+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	84
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		



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 PLOT DATE = 3/31/2014

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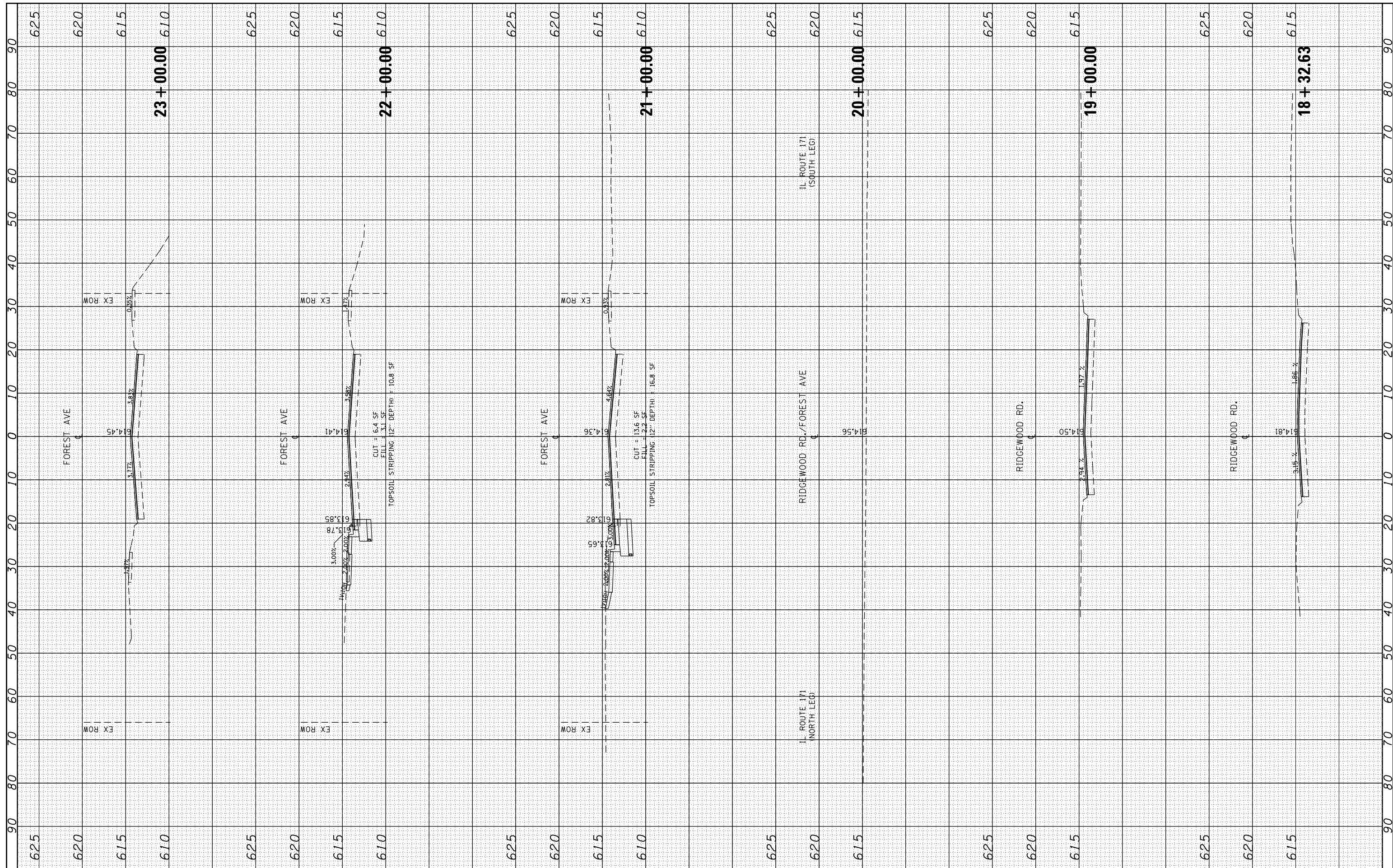
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 171 AT FOREST AVE/RIDGEWOOD RD.  
 CROSS SECTIONS - RIDGEWOOD RD.**  
 SCALE: 1"=10' (H) SHEET 1 OF 2 SHEETS STA. 16+00.00 TO STA. 18+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	85
CONTRACT NO. 60X08				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE



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 PLOT SCALE = 20.0000' / in.  
 CHECKED -  
 DATE -

REVISED -  
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 171 AT FOREST AVE/RIDGEWOOD RD.  
 CROSS SECTIONS - FOREST AVE/RIDGEWOOD RD.**

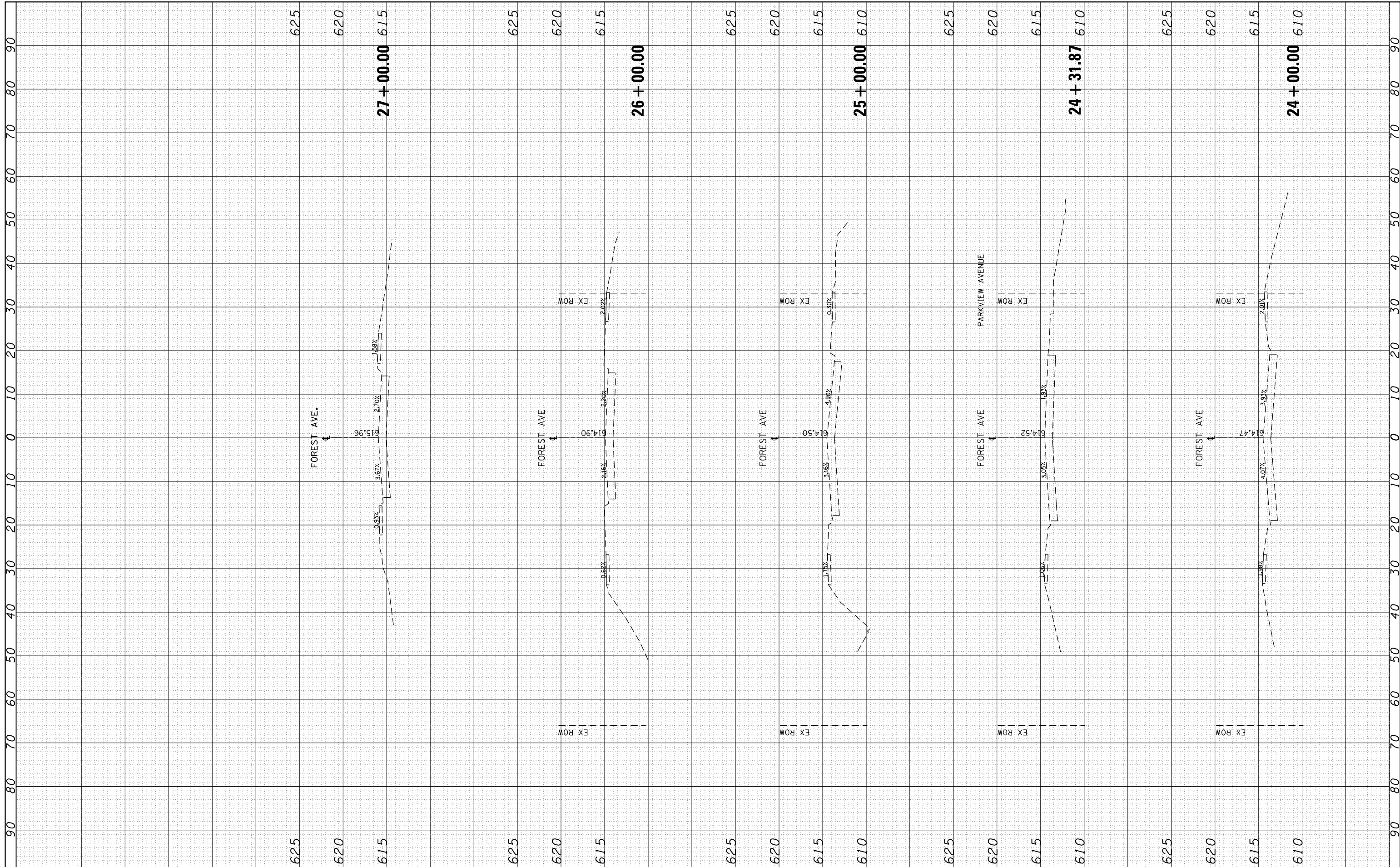
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	03035-TS	COOK	87	86
CONTRACT NO. 60X08				

SCALE: 1"=10' (H) SHEET 2 OF 2 SHEETS STA. 18+32.63 TO STA. 23+00.00

ILLINOIS FED. AID PROJECT

FINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE



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 PLOT DATE = 3/31/2014

DESIGNED -  
 DRAWN -  
 CHECKED -  
 DATE -

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 171 AT FOREST AVE/RIDGEWOOD RD.  
 CROSS SECTIONS - FOREST AVE.**

SCALE: SHEET OF SHEETS STA. 24+00.00 TO STA. 27+00.00

F.A.P. RTE. 372	SECTION 03035-TS	COUNTY COOK	TOTAL SHEETS 87	SHEET NO. 87
CONTRACT NO. 60X08			ILLINOIS FED. AID PROJECT	