

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
FAU ROUTE 2615 (MAIN STREET)
FAU 1504 (55TH STREET) TO FAU 1487 (MAPLE AVENUE)
RESURFACING
SECTION: 12-00106-00-RS
PROJECT: M-4003 (130)
JOB: C-91-138-13
VILLAGE OF DOWNERS GROVE
DU PAGE COUNTY**

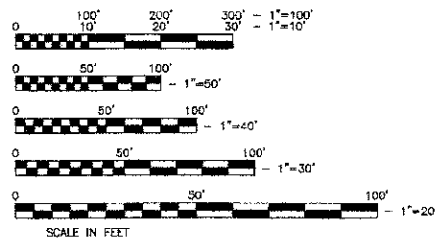
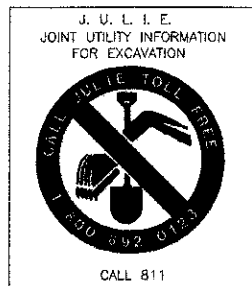
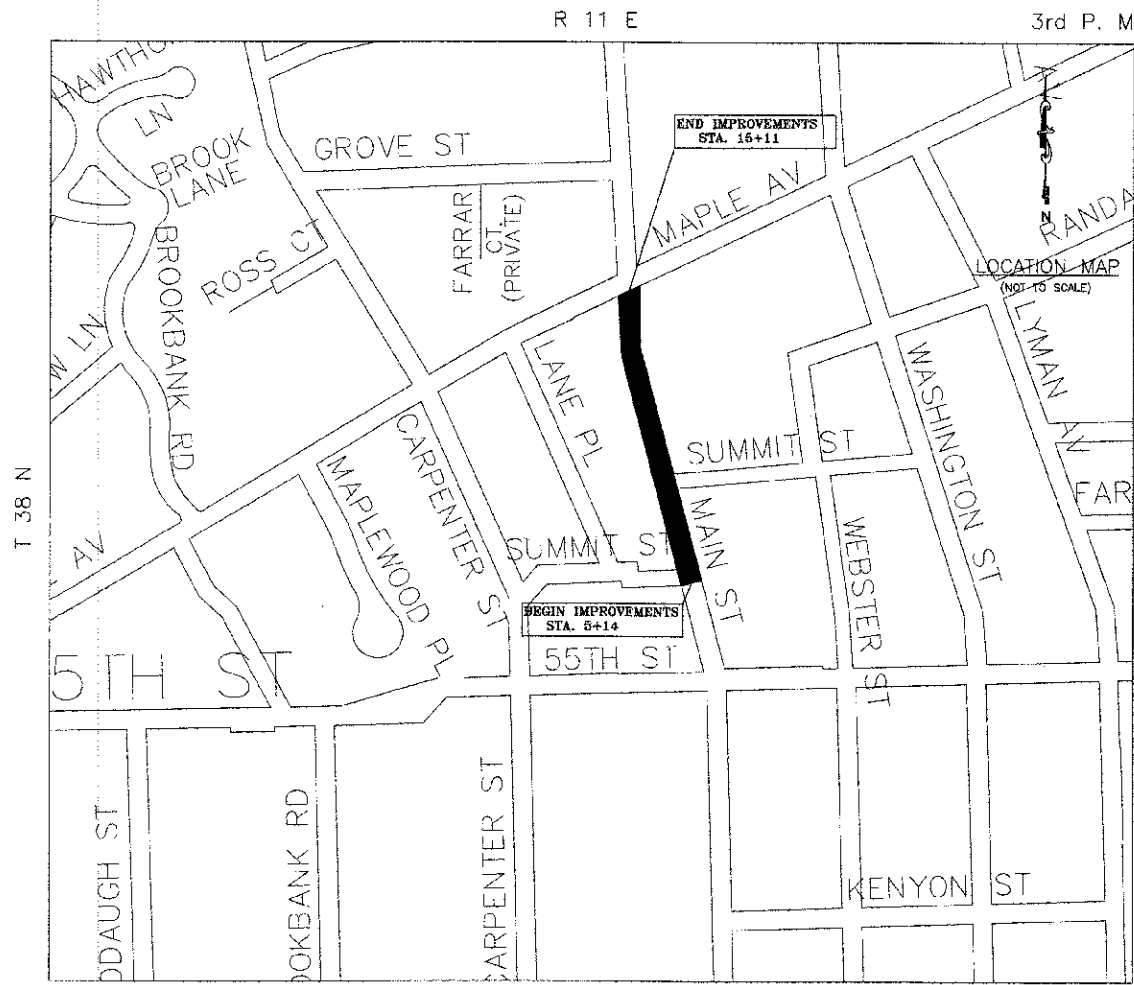
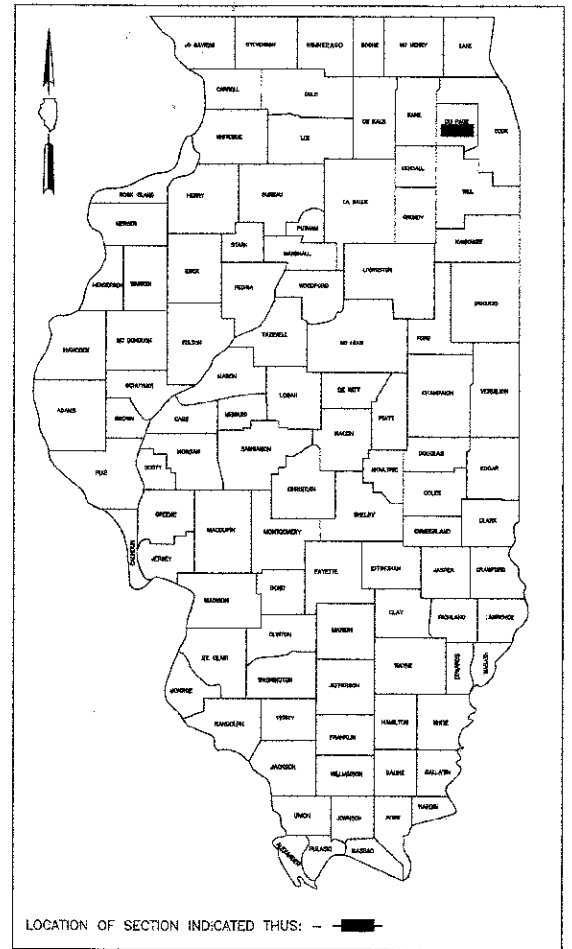
| F.A.D. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------------|----------|------------------|-----------|
| 2615 | 12-00106-00-RS | DU PAGE | 14 | 1 |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 63822 | | | | |

INDEX OF SHEETS

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| 2 | GENERAL NOTES & IDOT STANDARDS |
| 3 | SUMMARY OF QUANTITIES |
| 4 | TYPICAL SECTIONS |
| 5 | PLAN SHEET (STA 1+91 TO STA 7+00) |
| 6 | PLAN SHEET (STA 7+00 TO STA 13+00) |
| 7 | PLAN SHEET (STA 13+00 TO STA 18+41) |
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| 9 | BD-32 BUTT JOINT AND HMA TAPER DETAILS |
| 10 | TC-13 IDOT DISTRICT ONE TYPICAL PAVEMENT MARKINGS DETAIL |
| 11 | BD-08 FRAMES AND LIDS ADJUSTMENT WITH MILLING |
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| 14 | TC-16 PAVEMENT MARKING LETTERS & SYMBOLS FOR TRAFFIC STAGING |

PROJECT LOCATED IN THE
VILLAGE OF DOWNERS GROVE

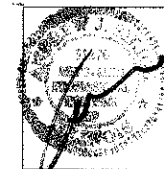
MAIN STREET DESIGN DESIGNATION
MINOR ARTERIAL
DESIGN SPEED: 35 MPH
ADT: 16,000



PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P. E. 847-705-4406 SCHAUMBURG, IL

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZE PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES CAN BE USED

Contract No. 63822



Andrew J. Sikich, P.E.
#062-051270
My License Expires 11/30/13

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED *Nancy Nelson* 2013
DIRECTOR OF PUBLIC WORKS, VILLAGE OF DOWNERS GROVE

PASSED *April 11* 2013
C. Holt *C. Hoer*
DIVISION 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW *April 12* 2013
John Fortson
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

1. ALL REFERENCES TO THE 'VILLAGE' IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE VILLAGE OF DOWNERS GROVE.

2. ALL REFERENCES TO THE 'STANDARD SPECIFICATIONS' IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE 'STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION' ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) ON JANUARY 1, 2012.

3. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE VILLAGE, DEPARTMENT, AND THE ENGINEERS DO NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING THE CONSTRUCTION OPERATION SO AS NOT TO DAMAGE THEM. IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 107.20 OF THE 'STANDARD SPECIFICATIONS' THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS.

4. THOSE EXISTING TRAFFIC SIGNS WHICH ARE SO DESIGNATED BY THE ENGINEER SHALL BE REMOVED, STORED AND SUBSEQUENTLY RELOCATED BY THE CONTRACTOR AT NO ADDITIONAL COST. ANY SIGNS WHICH ARE DAMAGED BY THE CONTRACTOR AS DETERMINED BY THE ENGINEER SHALL BE REPLACED IN KIND BY THE CONTRACTOR AND TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST.

5. ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE PLANS AND IDOT STANDARDS FOR TRAFFIC CONTROL AND PROTECTION.

6. SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.

7. DEBRIS REMOVAL - MATERIALS RESULTING FROM THE VARIOUS CONSTRUCTION OPERATIONS SHALL BE REMOVED AT THE END OF EACH WORK DAY TO AN APPROVED SITE. IN THE JUDGEMENT OF THE ENGINEER, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE ENGINEER WILL REMOVE MATERIALS AND THE CONTRACTOR SHALL BE BILLED ACCORDINGLY.

8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THE WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

9. WHENEVER, DURING CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

10. THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS WITHOUT PRIOR AUTHORIZATION FROM THE VILLAGE WATER DEPARTMENT.

11. QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL VERIFY FINAL PATCH LOCATIONS IN THE FIELD PRIOR TO REMOVAL.

12. HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

13. THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NORMAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASE ON WHICH THEY ARE PLACED. PLAN THICKNESS SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

14. MAILBOXES WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED, TEMPORARILY RELOCATED, AND REPLACED UPON COMPLETION OF THE PROPOSED IMPROVEMENTS IN ACCORDANCE WITH ARTICLE 107.20 AND AS DIRECTED BY THE ENGINEER.

15. THE CONTRACTOR SHALL NOT PLACE SOD UNTIL THE TEMPERATURE IS 80 DEGREES OR LESS AND THE FORECAST FOR THE NEXT 7 DAYS SHOWS TEMPERATURES OF 80 DEGREES OR LESS. IF ALL OTHER PAY ITEMS ARE COMPLETED THE CONTRACTOR WILL NOT BE CHARGED WORKING DAYS FOR DELAYS IN PARKWAY RESTORATION DUE TO TEMPERATURE.

16. SPECIAL ATTENTION IS CALLED TO ARTICLE 107.30 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DEFACEMENT OF ANY CONCRETE POURS BEFORE THEY HAVE SET UP. CONCRETE SIDEWALK, DRIVEWAY, CURB, AND CURB AND GUTTER THAT HAVE BEEN DEFACED, IN THE OPINION OF THE ENGINEER, SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR AT HIS EXPENSE. THIS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

17. FOR WORK OUTSIDE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.

IDOT STANDARDS

| | |
|-----------|--|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS |
| 280001-07 | TEMPORARY EROSION CONTROL SYSTEMS |
| 424001-07 | PERPENDICULAR CURB RAMPS FOR SIDEWALKS |
| 442201-03 | CLASS C & D PATCHES |
| 606001-05 | CONCRETE CURB TYPE B & COMBINATION CONCRETE CURB AND GUTTER |
| 701427-01 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH |
| 701606-08 | URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN |
| 701701-08 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 701801-05 | SIDEWALK, CORNER OR CROSSWALK CLOSURE |
| 701901-02 | TRAFFIC CONTROL DEVICES |
| 780001-03 | TYPICAL PAVEMENT MARKINGS |

BOXED ITEMS INDICATE WORK NOT PAID FOR SEPARATELY BUT INCLUDED IN ANOTHER PAY ITEM OR INCLUDED IN THE CONTRACT.

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| | PLOT SCALE - | CHECKED - AJS | REVISED |
| | PLOT DATE - 03/25/13 | DATE - 03/25/13 | REVISED |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAIN STREET IMPROVEMENTS
GENERAL NOTES

| | | | | |
|-----------------------|----------------|------------------|--------------|-----------|
| F.A.D. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2615 | 12-00106-00-RS | DU PAGE | 14 | 2 |
| CONTRACT NO. 63822 | | | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT | | |

NOT TO SCALE SHEET NO. 2 OF 14 SHEETS STA. TO STA.

SUMMARY OF QUANTITIES

| MAIN STREET | | | | |
|-------------|--|--------|----------------|-----------------------------|
| CODE NO. | PAY ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION TYPE CODE 0005 |
| 20101200 | TREE ROOT PRUNING | EACH | 1 | 1 |
| 21101615 | TOPSOIL FURNISH AND PLACE, 4" | SQ YD | 90 | 90 |
| 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 4 | 4 |
| 25000500 | PHOSPHOROUS FERTILIZER NUTRIENT | POUND | 4 | 4 |
| 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 4 | 4 |
| 25200110 | SODDING, SALT TOLERANT | SQ YD | 90 | 90 |
| 25200200 | SUPPLEMENTAL WATERING | UNIT | 5 | 5 |
| 28000510 | INLET FILTERS | EACH | 4 | 4 |
| 40600100 | BITUMINOUS MATERIALS (PRIME COAT) | GALLON | 513 | 513 |
| 40600300 | AGGREGATE (PRIME COAT) | TON | 13 | 13 |
| 40600400 | MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS | TON | 10 | 10 |
| 40600635 | LEVELING BINDER (MACHINE METHOD), N70 | TON | 216 | 216 |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 90 | 90 |
| 40603340 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | TON | 435 | 435 |
| 42001300 | PROTECTIVE COAT | SQ YD | 160 | 160 |
| 42300400 | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH | SQ YD | 50 | 50 |
| 44000157 | HOT-MIX ASPHALT SURFACE REMOVAL, 2" | SQ YD | 5,130 | 5,130 |
| 44000200 | DRIVEWAY PAVEMENT REMOVAL | SQ YD | 65 | 65 |
| 44000500 | COMBINATION CURB AND GUTTER REMOVAL | FOOT | 161 | 161 |

| MAIN STREET | | | | |
|-------------|--|-------|----------------|-----------------------------|
| CODE NO. | PAY ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION TYPE CODE 0005 |
| 44201798 | CLASS D PATCHES, TYPE I, 13 INCH | SQ YD | 100 | 100 |
| 44201803 | CLASS D PATCHES, TYPE II, 13 INCH | SQ YD | 150 | 100 |
| 44201807 | CLASS D PATCHES, TYPE III, 13 INCH | SQ YD | 150 | 150 |
| 44201809 | CLASS D PATCHES, TYPE IV, 13 INCH | SQ YD | 200 | 200 |
| 50406100 | FRAMES AND LIDS, TYPE 1, CLOSED LID | EACH | 3 | 3 |
| 60603800 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 | FOOT | 161 | 161 |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 |
| 70102625 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701606 | L SUM | 1 | 1 |
| 70102635 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701701 | L SUM | 1 | 1 |
| 70102640 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701801 | L SUM | 1 | 1 |
| 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 720 | 720 |
| 70300210 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS | SQ FT | 45 | 45 |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 295 | 295 |
| * 78000100 | THERMOPLASTIC PAVEMENT MARKING-LETTERS AND SYMBOLS | SQ FT | 45 | 45 |
| * 78000200 | THERMOPLASTIC PAVEMENT MARKING-LINE 4" | FOOT | 2,700 | 2,700 |
| * 78000650 | THERMOPLASTIC PAVEMENT MARKING-LINE 24" | FOOT | 25 | 25 |
| Z0004510 | HOT-MIX ASP-ALT DRIVEWAY PAVEMENT, 3" | SQ YD | 15 | 15 |
| X2800510 | INLET FILTER CLEANING | EACH | 4 | 4 |
| X6030310 | FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) | EACH | 20 | 20 |

* DENOTES SPECIALTY ITEM

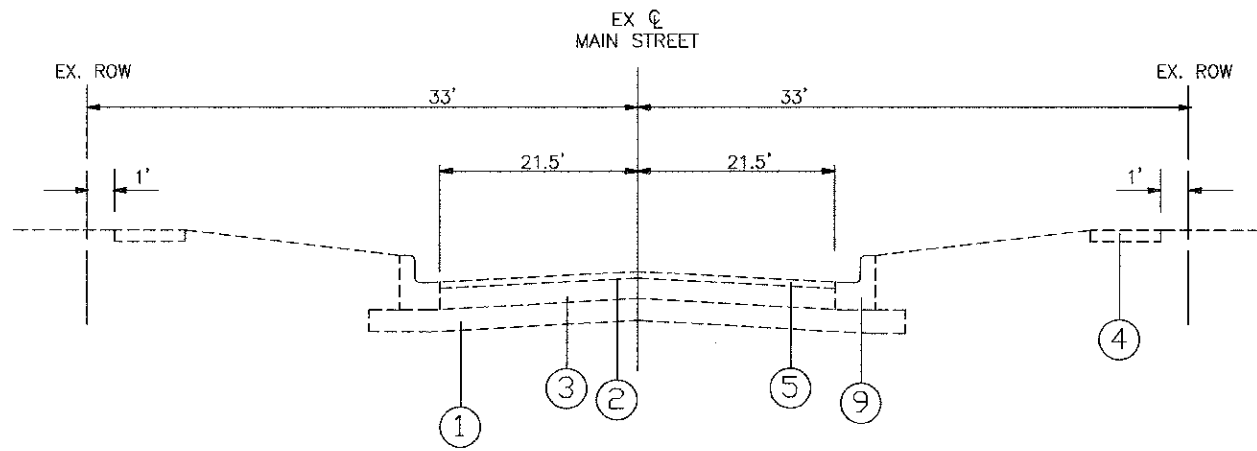
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

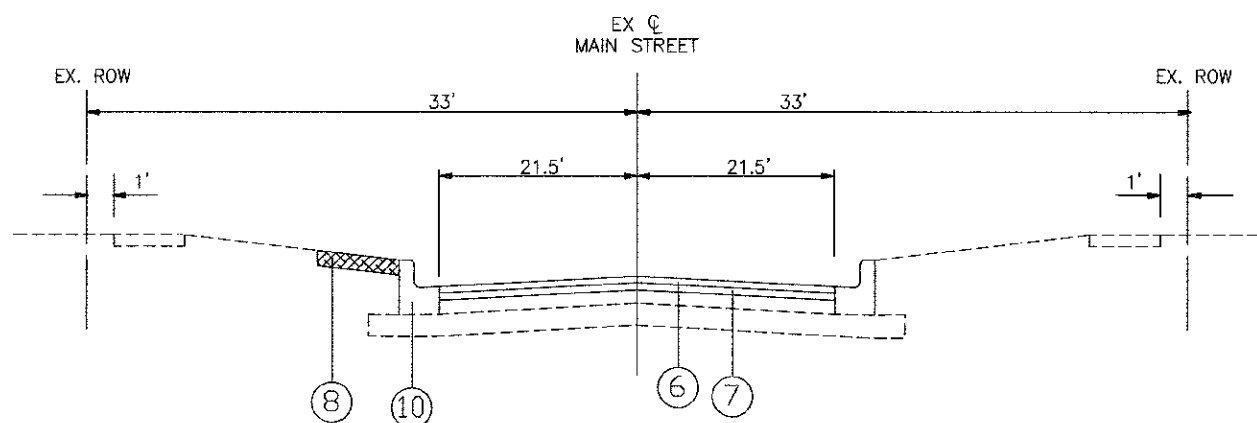
MAIN STREET IMPROVEMENTS
SUMMARY OF QUANTITIES

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| F. A. W. REC. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2818 | 12-00106-00-RS | DU PAGE | 14 | 3 |
| CONTRACT NO. 63822 | | | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |

NOT TO SCALE SHEET NO. 3 OF 14 SHEETS STA. TO STA.



EXISTING TYPICAL SECTION
STA. 5+14 TO STA. 15+11, MAIN STREET



PROPOSED TYPICAL SECTION
STA. 5+14 TO STA. 15+11, MAIN STREET

LEGEND

- ① EXISTING SUBGRADE
- ② EXISTING BITUMINOUS PAVEMENT, 2" - 4", VARIES
- ③ EXISTING CONCRETE, BRICK BASE, 5 1/2" - 11", VARIES
- ④ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- ⑤ PROPOSED HMA SURFACE REMOVAL, 2"
- ⑥ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑦ PROPOSED LEVELING BINDER (MACHINE METHOD), N70, 3/4"
- ⑧ PROPOSED SODDING, SALT TOLERANT & TOP SOIL FURNISH AND PLACE, 4"
- ⑨ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B6.12 REMOVAL WHERE SHOWN ON PLANS
- ⑩ PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B6.12 REPLACEMENT WHERE SHOWN ON PLANS

CONTRACTOR SHALL MILL FIRST BEFORE PATCHING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

| MIXTURE TYPE | AIR VOIDS @ Ndes |
|--|------------------|
| HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm), 1 1/2" | 4% @ 70 GYR |
| LEVELING BINDER (MACHINE METHOD), N70, 3/4" | 4.0% @ 70 GYR |
| DRIVEWAY: HOT MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 3" | 4% @ 50 GYR |
| CLASS D PATCHES, (HMA BINDER) IL-19mm, N70, 6" (IN 2 LIFTS) | 4% @ 70 GYR |

-THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.

-THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 - 22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 - 22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

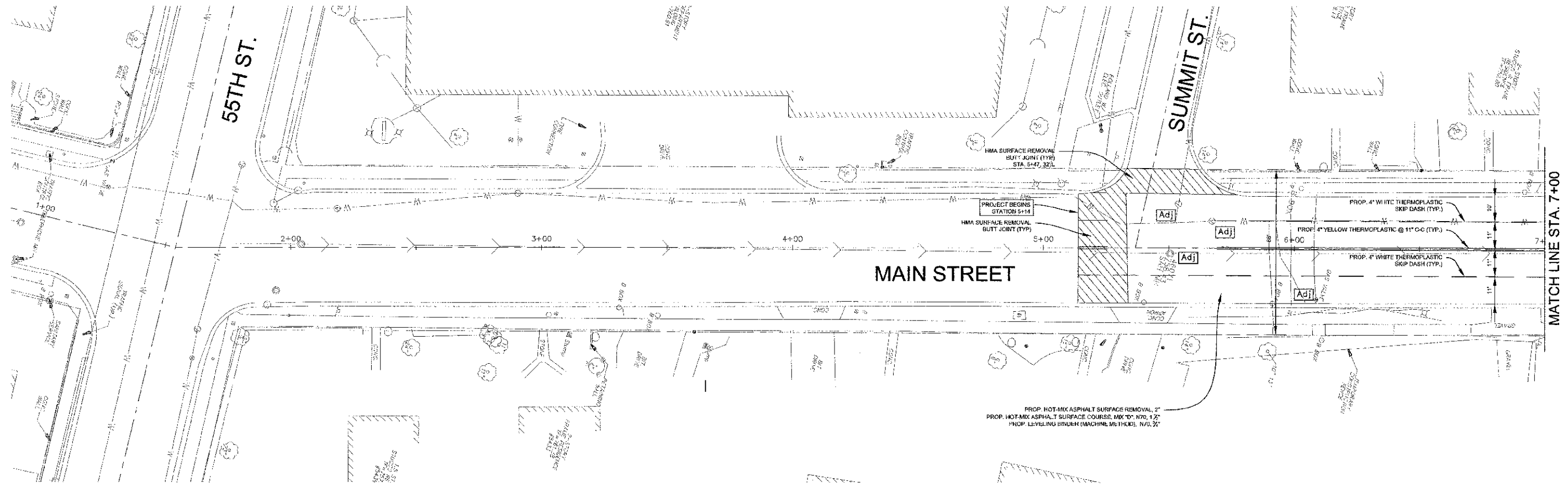
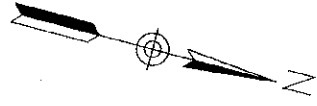
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAIN STREET IMPROVEMENTS
TYPICAL SECTIONS

NOT TO SCALE SHEET NO. 4 OF 14 SHEETS STA. TO STA.

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|-----------------------|----------------|------------------|--------------|-----------|
| F.A.D. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2615 | 12-00106-00-RS | DJ PAGE | 14 | 4 |
| CONTRACT NO. 63822 | | | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT | | |



NOTES

1. CLASS D PATCHES, 13" AS DIRECTED BY THE ENGINEER.
2. ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE ADJUSTMENT ITEM.
3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.

[Adj] DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
[IF] INLET FILTERS

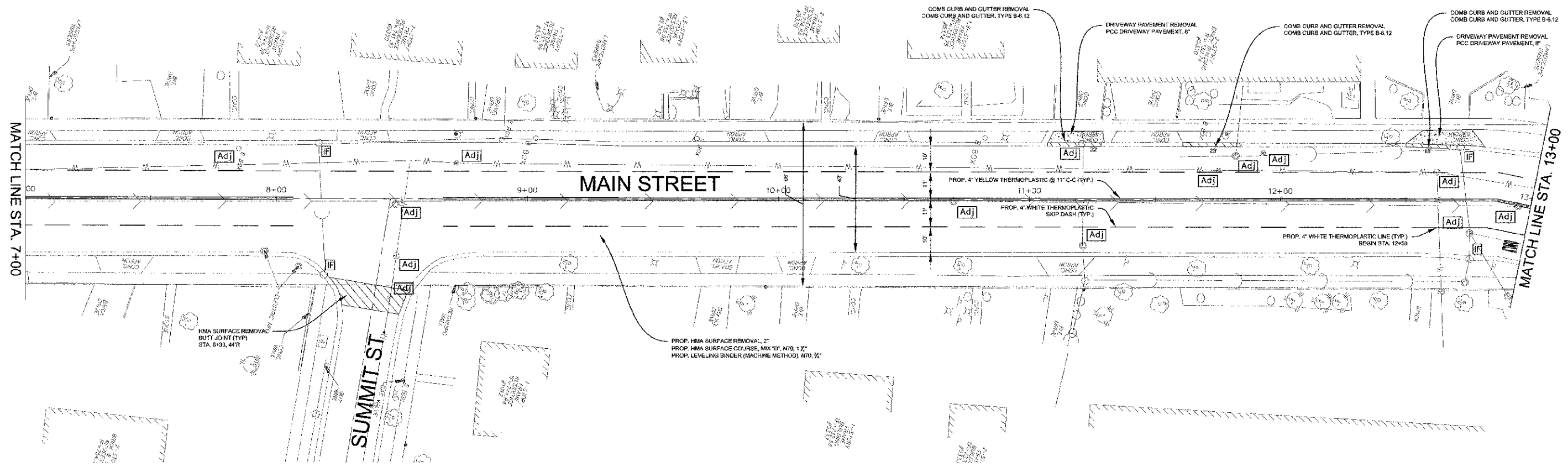
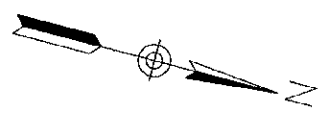
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

MAIN STREET IMPROVEMENTS
 IMPROVEMENT PLAN

NOT TO SCALE SHEET NO. 5 OF 14 SHEETS STA. 2+00 TO STA. 7+00

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| F.A.E. REC. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2615 | 12-00106-00-RS | DU PAGE | 14 | 5 |
| CONTRACT NO. 63822 | | | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT | | |



NOTES

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2. ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE ADJUSTMENT ITEM.
3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.

Adj DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
If INLET FILTERS

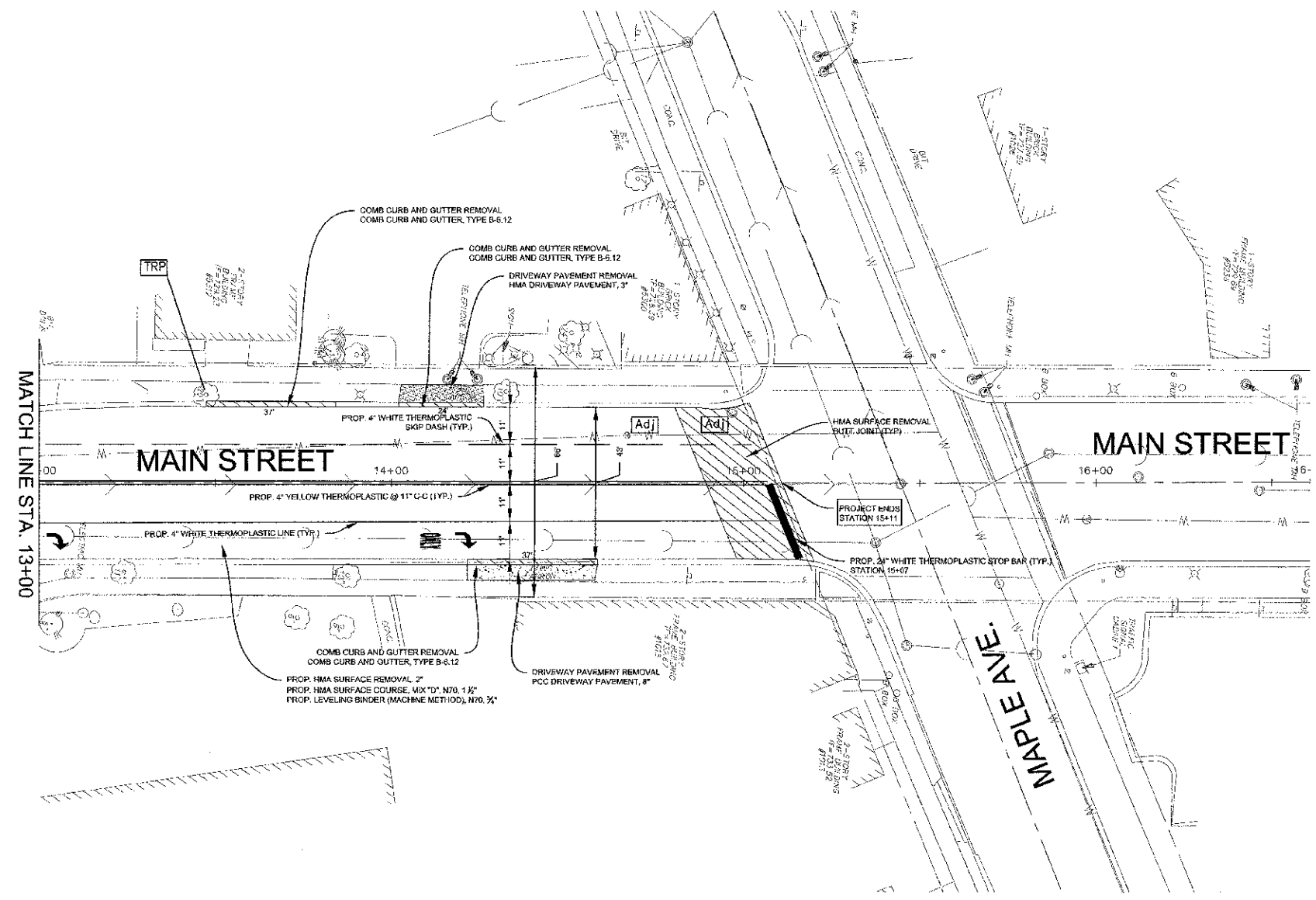
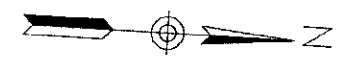
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| | PLOT DATE - 03/25/13 | DATE - 03/25/13 | REVISED |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

MAIN STREET IMPROVEMENTS
 IMPROVEMENT PLAN

NOT TO SCALE SHEET NO. 6 OF 14 SHEETS STA. 7+00 TO STA. 13+00

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| F. A. L. SHE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2815(12-00108-00-RS | DU PAGE | ILLINOIS | 14 | 6 |
| CONTRACT NO. 63822 | | | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT | | |



NOTES

1. CLASS D PATCHES, 13" AS DIRECTED BY THE ENGINEER.
2. ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE ADJUSTMENT ITEM.
3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.

- TRP TREE ROOT PRUNE
- Adj DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
- IF INLET FILTERS

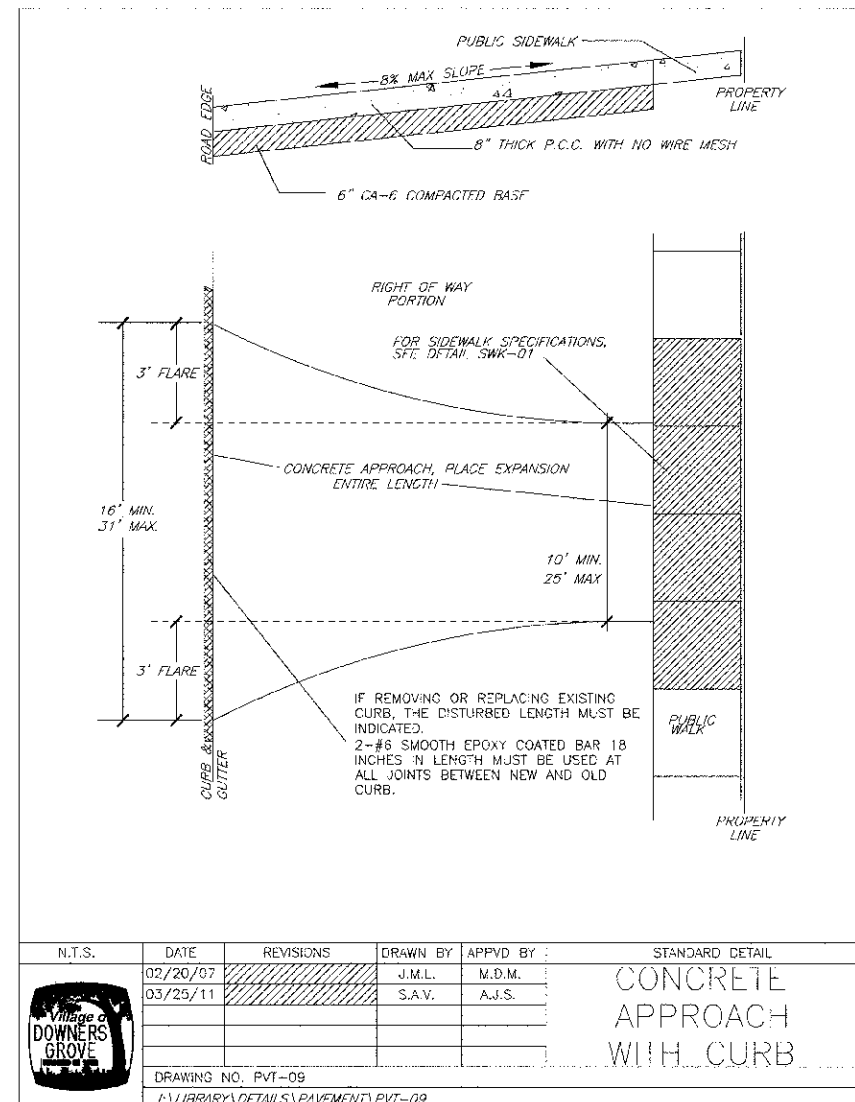
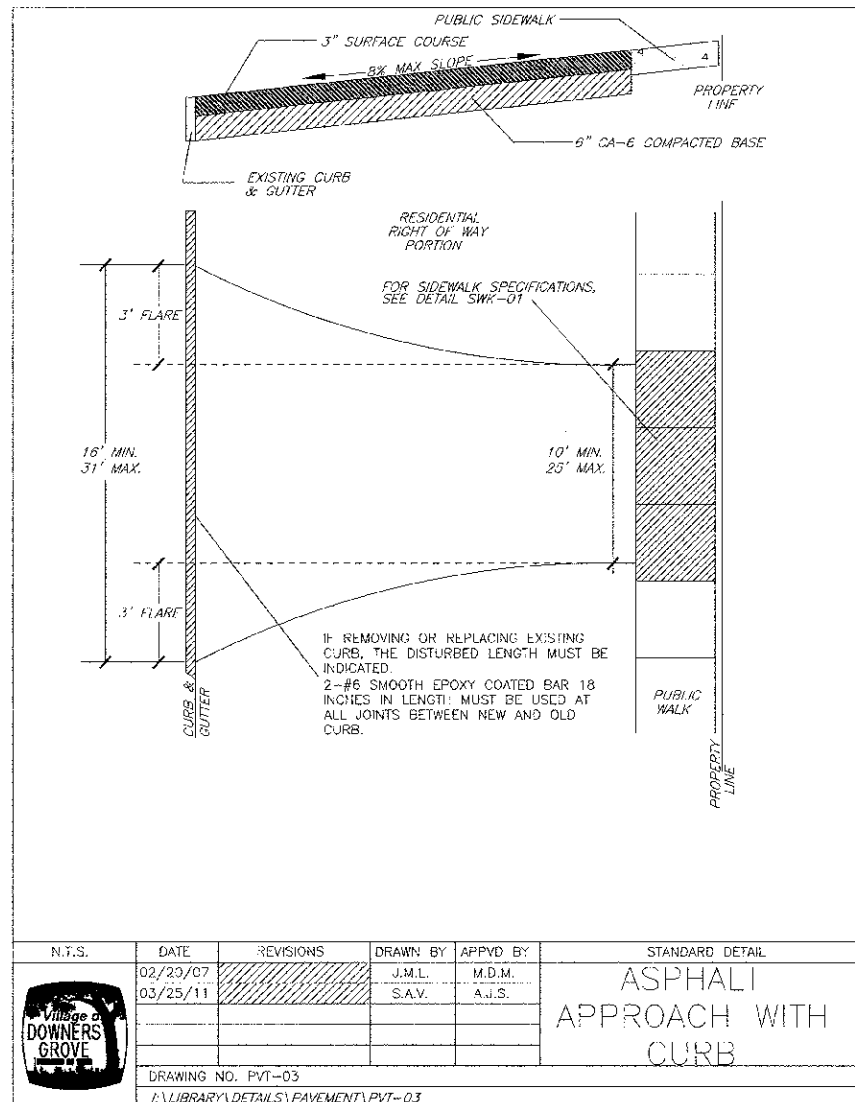
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| | PLOT DATE - 03/25/13 | DATE - 03/25/13 | REVISED |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAIN STREET IMPROVEMENTS
IMPROVEMENT PLAN

NOT TO SCALE SHEET NO. 7 OF 14 SHEETS STA. 13+00 TO STA. 16+61

| F.A.N. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------------|----------|------------------|-----------|
| 2615 | 12-00106-00-RS | DU PAGE | 14 | 7 |
| CONTRACT NO. 63822 | | | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |



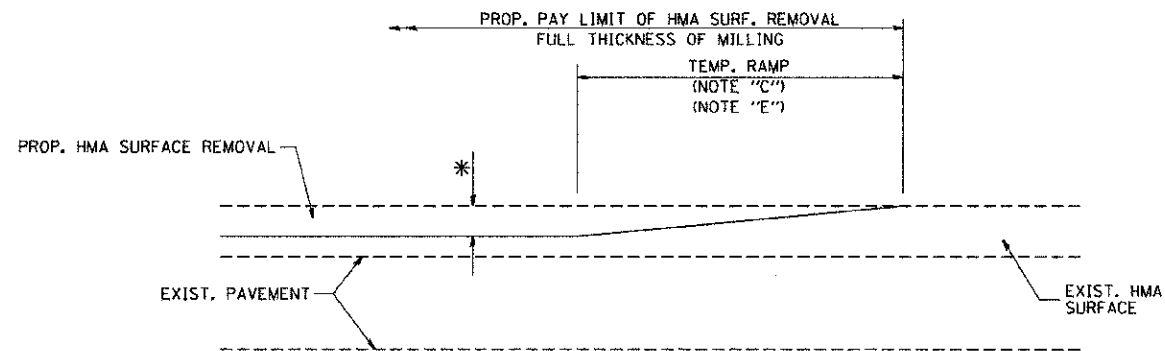
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| FILE NAME = | | DRAWN -- NRH | REVISED |
| | PLOT SCALE -- | CHECKED -- AJS | REVISED |
| | PLOT DATE -- 03/25/13 | DATE -- 03/25/13 | REVISED |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAIN STREET IMPROVEMENTS
VILLAGE DETAILS

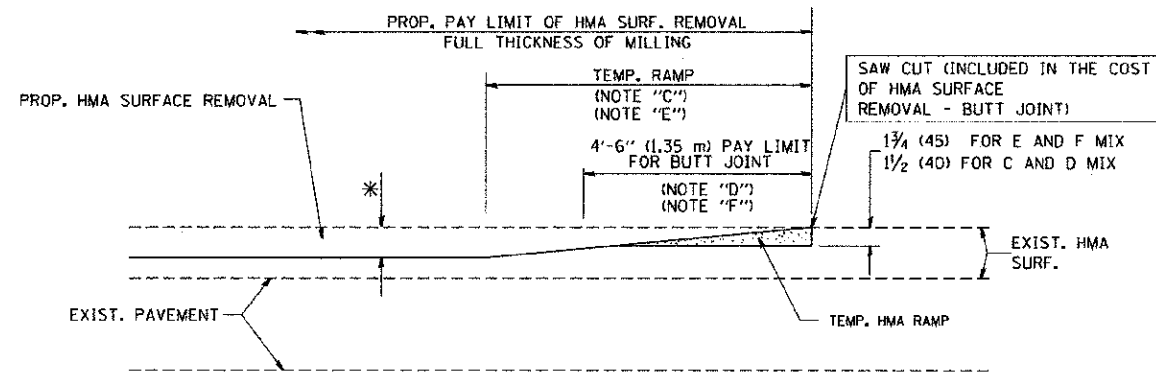
NOT TO SCALE SHEET NO. 8 OF 14 SHEETS STA. TO STA.

| | | | | |
|-----------------------|----------|------------------|--------------|-----------|
| F. A. N. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 261512-00106-00-RS | DU PAGE | 14 | 8 | |
| CONTRACT NO. 63822 | | | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT | | |



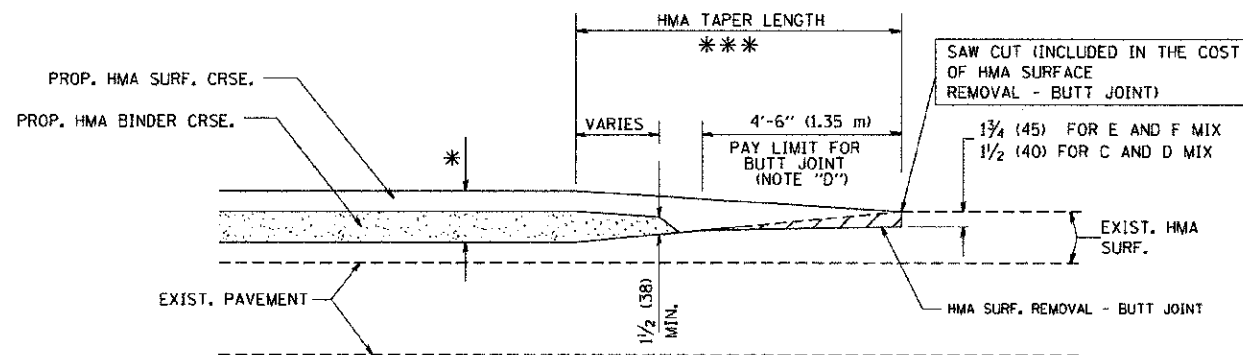
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

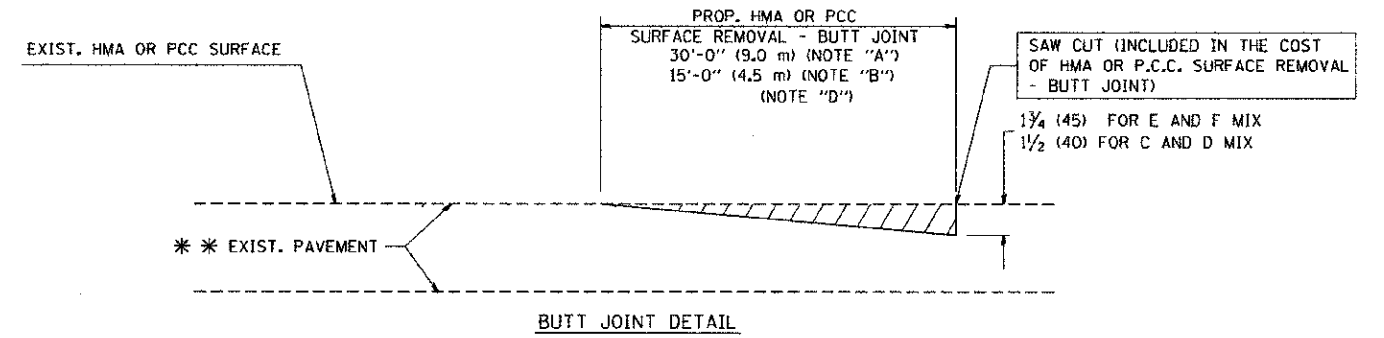


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

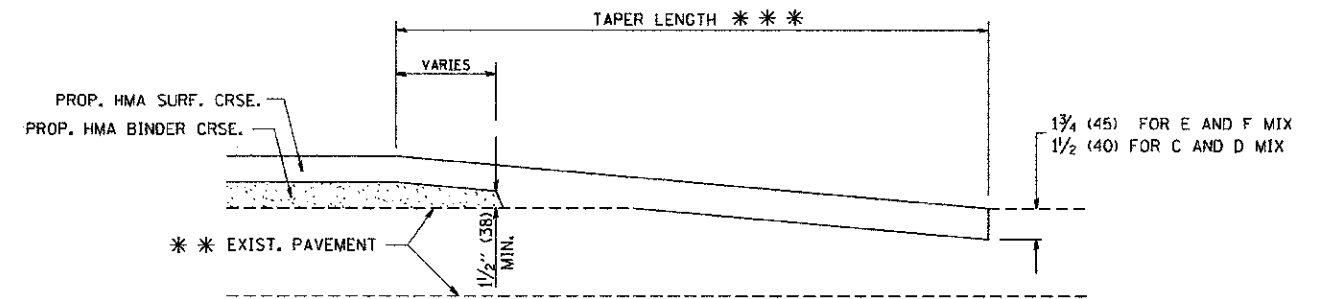
OPTION 2
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

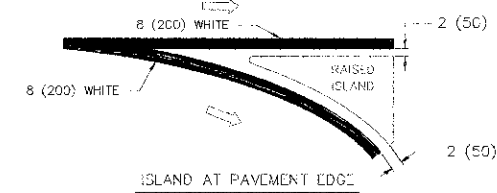
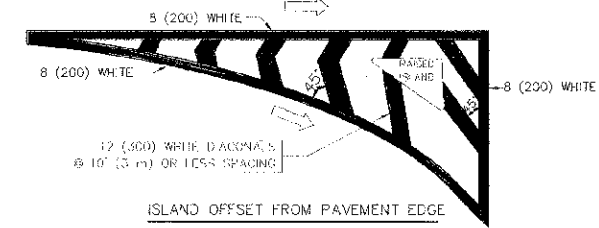
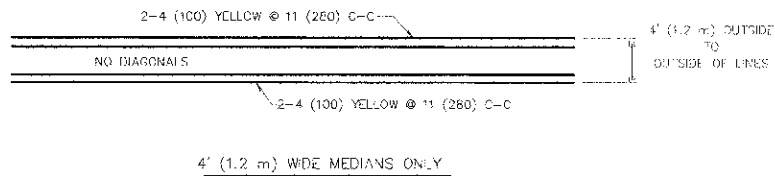
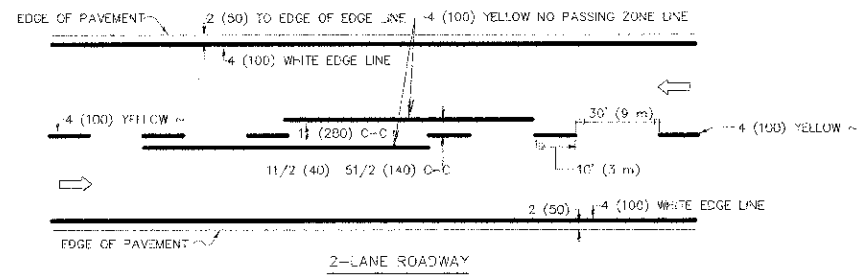
- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

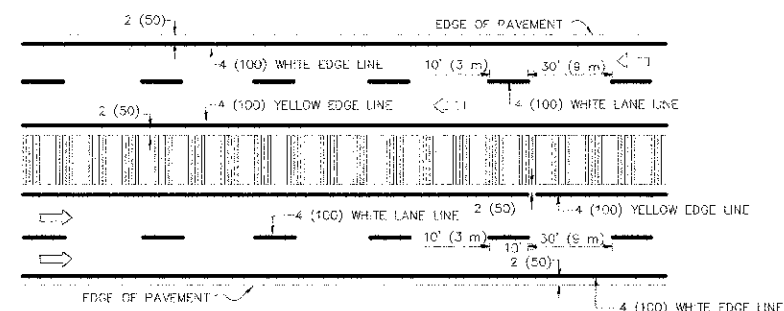
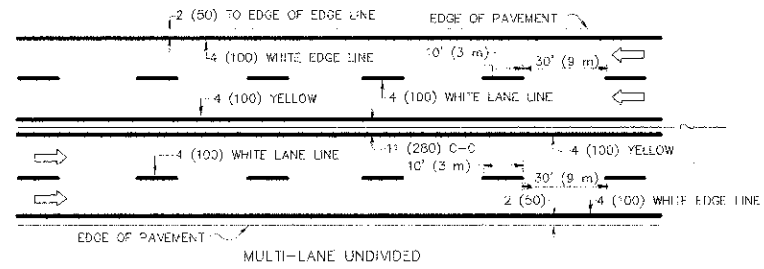
THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | |
|---|--------------------------------|-----------------------|-----------------------------|---|---|-------------------------|-------------------|---------------------------|----------------------|--------------------|--------------------|
| FILE NAME = W:\jststd\22x34\bd32.dgn | USER NAME = goghanoht | DESIGNED - M. DE YONG | REVISED - R. SHAH 10-25-94 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | BUTT JOINT AND HMA TAPER DETAILS | | F.A. RTE. 2615 | SECTION 12-00106-00-RS | COUNTY DJ PAGE | TOTAL SHEETS 14 | SHEET NO. 9 |
| | PLOT SCALE = 50.0000' / 1" IN. | DRAWN - | REVISED - A. ABBAS 03-21-97 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | BD400-05 BD32 | | CONTRACT NO. 63822 |
| | PLOT DATE = 1/4/2008 | CHECKED - | REVISED - M. GOMEZ 04-06-01 | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | |
| | | DATE - 06-13-90 | REVISED - R. BORO 01-01-07 | | | | | | | | |

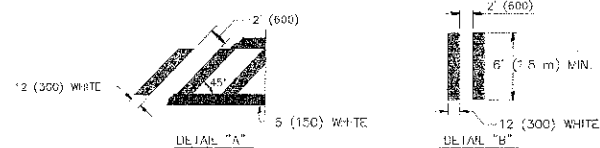
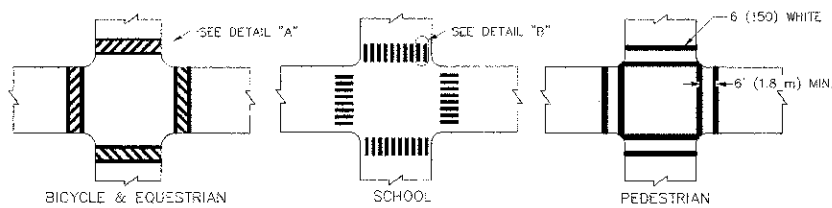


TYPICAL ISLAND MARKING

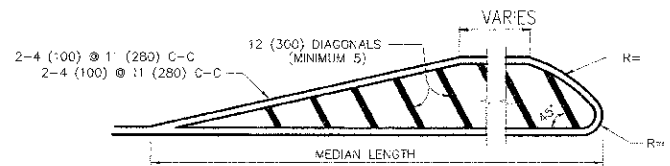


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



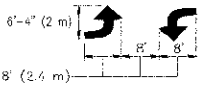
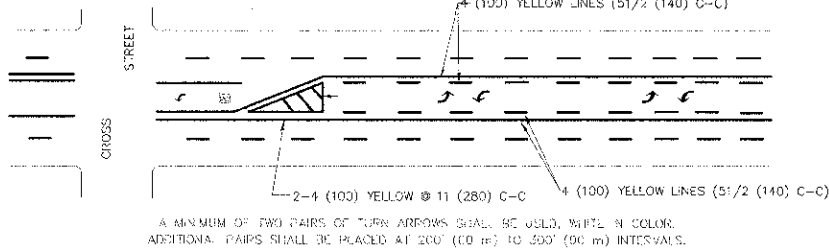
TYPICAL CROSSWALK MARKING



FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

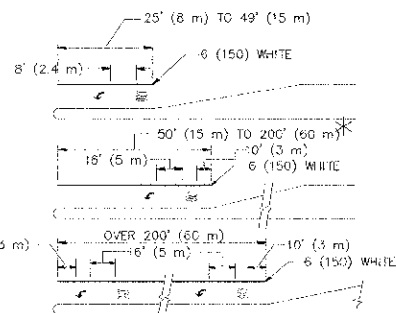
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (23 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (OVER 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

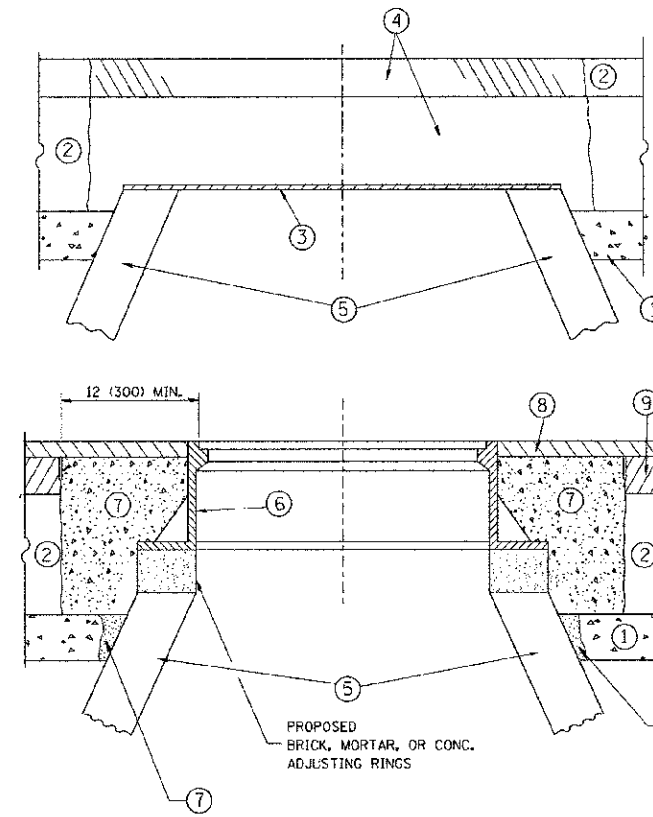
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|--|--|----------------------------|---|---|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINE: FOR ONE DIRECTION FOR BOTH DIRECTIONS | 4 (100) 2 @ 4 (100) | SOLID SOLID | YELLOW YELLOW | 5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C ON 1 SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (25) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| BOULEVARD LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW LEFT WHITE RIGHT | SHOULDER MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 6 (50) LINE (ALL SET LETTERS & SYMBOLS 8' (2.4 m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW | SKIP-DASH AND 5/7 IN PAIRS | YELLOW WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE (OR SKIP-DASH 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (HIC & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45' 12 (300) @ 90' | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAIL |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK. PROTECT OTHERWISE. PLACE AT NEAREST STOPPING POINT PARALLEL TO CROSSROAD CENTERLINE, WHEN POSSIBLE. |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45' | SOLID | YELLOW TWO WAY TRAFFIC WHITE ONE WAY TRAFFIC | 11 (280) C-C FOR T.I.L. DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING |
| SOLE MARKING AND CHANNELIZING LINES | 11 (200) WITH 12 (300) DIAGONALS @ 45' | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) EACH 12 (300) @ 45' | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF 18' x 3.6 SQ. FT. (0.33 x 2) EACH LINE |
| SHOULDER DIAGONALS | 12 (300) @ 45' | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (23 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

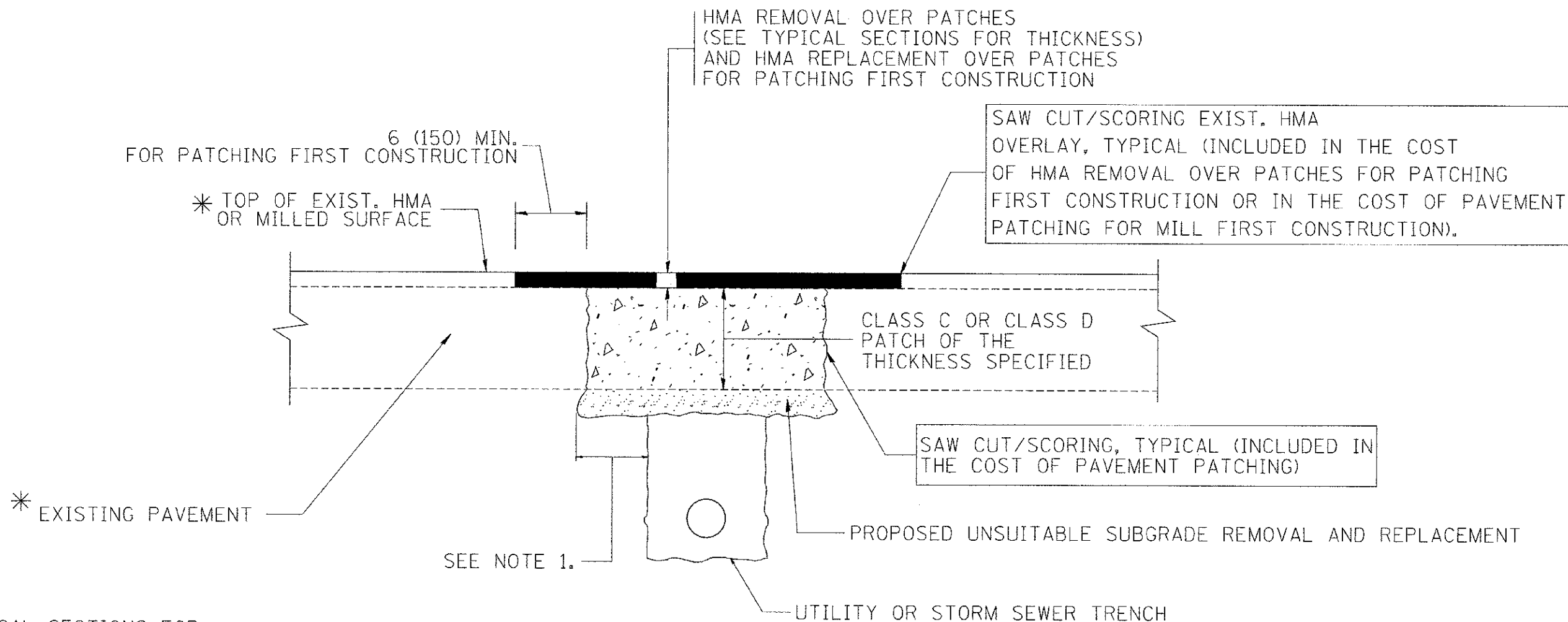
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

| | | | |
|---|-------------|------------|----------------------|
| FILE NAME : | USER NAME : | DESIGNED : | REVISED : |
| at:\pwwork\work\dot\beuerd\108\28315\bd09.dgn | beuerd | R. SHAH | R. WIEDEMAN 05-14-04 |
| | | DRAWN : | REVISED : |
| | | | R. BORO 01-01-07 |
| | | CHECKED : | REVISED : |
| | | | R. BORO 03-09-11 |
| | | DATE : | REVISED : |
| | | 10-25-94 | R. BORO 12-06-11 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | | | | | | |
|--|-------------------------|---|----------------|--------------------|--------------|-----------|
| DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | 2615 | 12-00106-00-RS | DU PAGE | 14 | 11 |
| | | BD600-03 (BD-8) | | CONTRACT NO. 63622 | | |
| | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

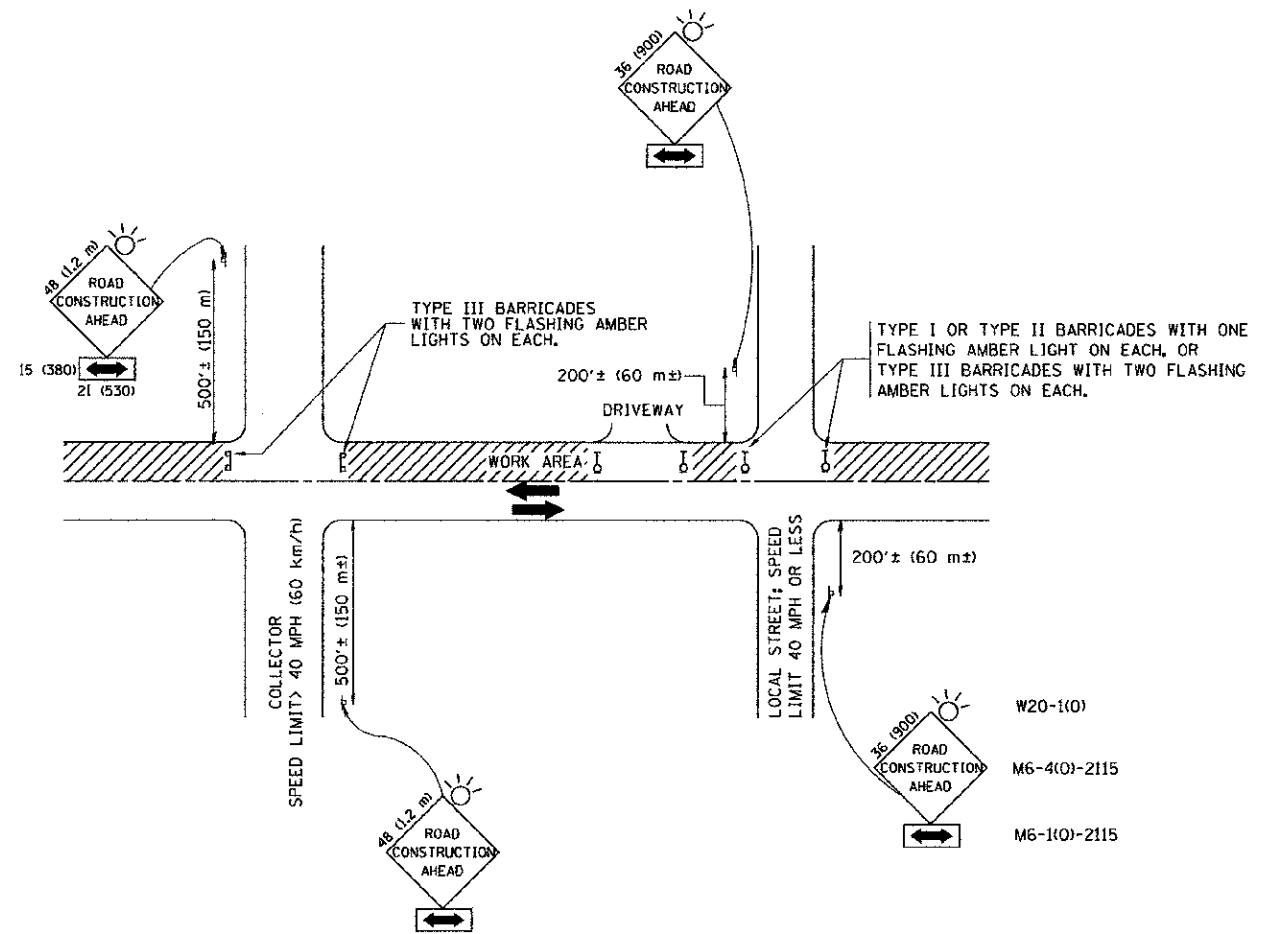
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | |
|--|---------------------|----------------------------|-----------------------------|---|--|-------------------------|--------------------|---|--------|-----------------|--------------|
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| | | DRAWN - | REVISED - R. BORO 01-01-07 | | 2615 | 12-00106-00-RS | DU PAGE | 14 | 12 | | |
| | | PLOT SCALE = 50,000' / IN. | REVISED - R. BORO 09-04-07 | | BD400-04 (BD-22) | | CONTRACT NO. 63822 | | | | |
| | | PLOT DATE = 10/27/2008 | REVISED - K. ENG 10-27-08 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY: USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

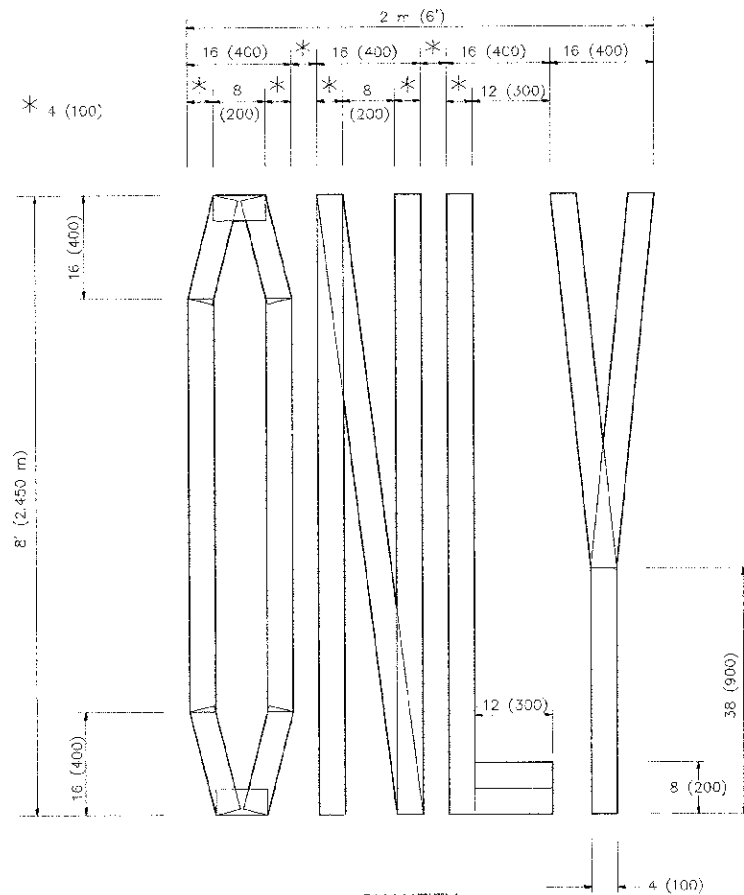
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| | | DRAWN - | REVISED - A. HOUSEH 03-06-96 |
| | PLOT SCALE = 50.000' / IN. | CHECKED - | REVISED - A. HOUSEH 10-15-96 |
| | PLOT DATE = 1/4/2008 | DATE - 06-99 | REVISED - T. RAMMACHER 01-06-00 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

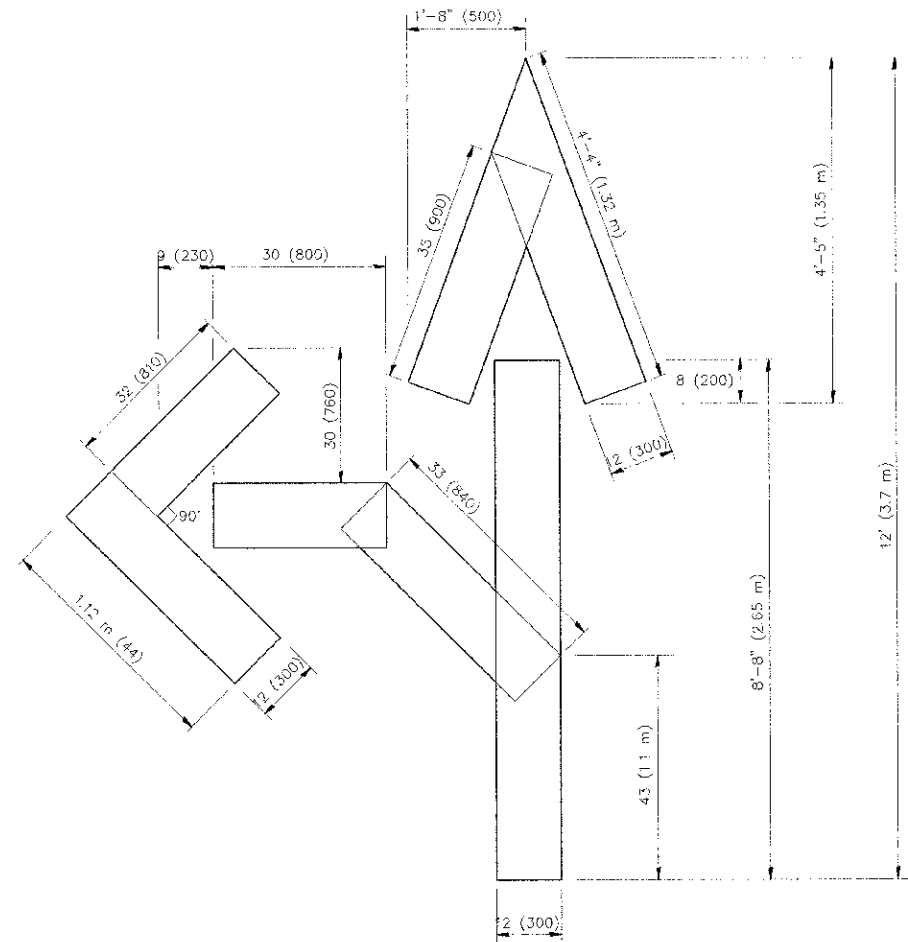
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

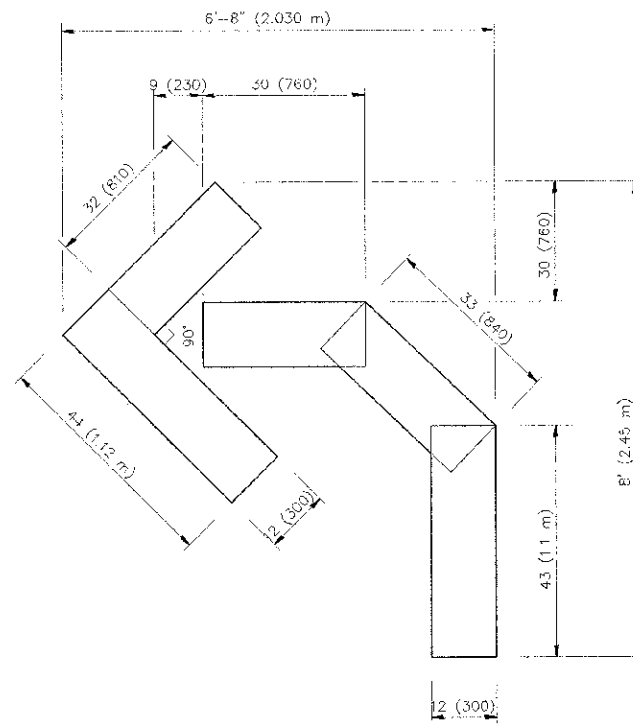
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|---|----------------|---------|--------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2615 | 12-00106-00-RS | DU PAGE | 14 | 13 |
| TC-10 | | | CONTRACT NO. 63822 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

| | | | | | | | | | | |
|-----------|-----------|----------|---------|---|---|-------------|-------------------------|--------------------|--------------|-----------------------|
| FILE NAME | USER NAME | DESIGNED | REVISED | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | |
| SHEETS | | DRAWN | REVISED | | | 2615 | 12-00106-00-RS | DU PAGE | 14 | 14 |
| | | CHECKED | REVISED | | | TC-16 | | CONTRACT NO. 63822 | | |
| | | DATE | REVISED | | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. 1 |
| | | DATE | REVISED | | | | | | | |