

CONSTRUCTION PLANS FOR

SPARTA COMMUNITY AIRPORT

HUNTER FIELD

SPARTA, ILLINOIS

ILLINOIS PROJECT SAR - 4224

S.B.G. PROJECT 3-17-0094-B11

EXTEND PARALLEL TAXIWAY TO RUNWAY 18 THRESHOLD

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SUMMARY OF QUANTITIES			
ITEM NO.	Description	UNIT	QUANTITY
AR108158	1/C #8 5 KV UG CABLE IN UD	LF	3,975.0
AR110102	DUCT MARKER - IN PAVEMENT	EA	2.0
AR110504	4-WAY CONCRETE ENCASED DUCT	LF	45.0
AR125415	MILT-BASE MOUNTED	EA	33.0
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EA	2.0
AR125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EA	2.0
AR125447	TAXI GUIDANCE SIGN, 7 CHARACTER	EA	2.0
AR125565	SPLICE CAN	EA	1.0
AR125610	REILS	PAIR	1.0
AR125901	REMOVE STAKE MOUNTED LIGHT	EA	3.0
AR125904	REMOVE TAXI GUIDANCE SIGN	EA	4.0
AR125907	REMOVE REILS	PAIR	1.0
AR150510	ENGINEER'S FIELD OFFICE	LS	1.0
AR150520	MOBILIZATION	LS	1.0
AR151450	CLEARING AND GRUBBING	AC	0.3
AR152410	UNCLASSIFIED EXCAVATION	CY	7,000.0
AR156510	SILT FENCE	LF	650.0
AR156514	STRAW WATTLE	LF	270.0
AR156520	INLET PROTECTION	EA	1.0
AR156530	TEMPORARY SEEDING	AC	3.5
AR801246	TIED CONCRETE BLOCK MAT	SF	1,600.0
AR208540	OVERSIZED AGGREGATE	TON	1,800.0
AR209600	GEOTEXTILE FABRIC	SY	3,250.0
AR209606	CRUSHED AGG. BASE COURSE - 6"	SY	3,150.0
AR401610	BITUMINOUS SURFACE COURSE	TON	325.0
AR401655	BUTT JOINT CONSTRUCTION	SY	234.0
AR401921	REMOVE PAVEMENT	SY	785.0
AR403610	BITUMINOUS BASE COURSE	TON	455.0
AR602510	BITUMINOUS PRIME COAT	GAL	1,600.0
AR603510	BITUMINOUS TACK COAT	GAL	850.0
AR620520	PAVEMENT MARKING-WATERBORNE	SF	3,000.0
AR620525	PAVEMENT MARKING-BLACK BORDER	SF	1,960.0
AR620900	PAVEMENT MARKING REMOVAL	SF	1,210.0
AR625511	SEAL COAT	SY	2,250.0
AR701530	30" RCP, CLASS IV	LF	192.0
AR701900	REMOVE PIPE	LF	140.0
AR751410	INLET	EA	1.0
AR752430	PRECAST REINFORCED CONC. FES 30"	EA	1.0
AR752900	REMOVE END SECTION	EA	1.0
AR901510	SEEDING	AC	3.5
AR908510	MULCHING	AC	3.5
AR908520	EXCELSIOR BLANKET	SY	500.0
AR908525	KNITTED STRAW MAT	SY	2,050.0

MARCH 1, 2013

811 Know what's below. Call before you dig. **J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS** www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

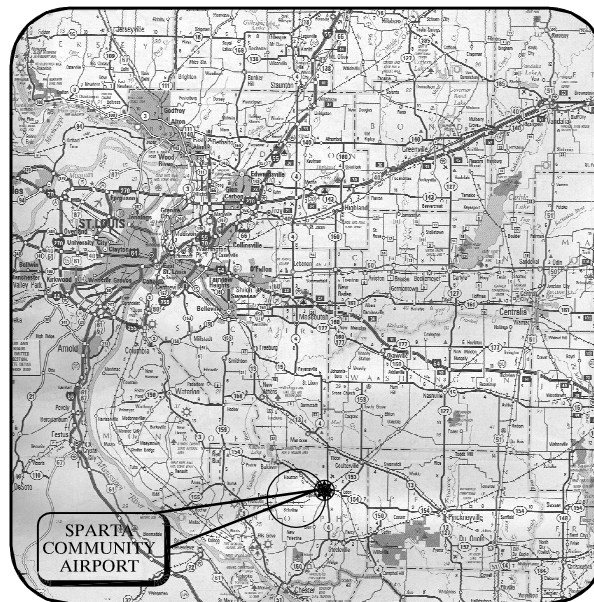
CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

DESIGN INFORMATION
 GEOMETRIC CRITERIA
 AIRPLANE DESIGN GROUP II
 TAXIWAY DESIGN GROUP II
 PAVEMENT DESIGN CRITERIA
 AIRCRAFT SINGLE WHEEL GEAR
 DEPARTURE WEIGHT = 12,500 LBS.
 100 ANNUAL DEPARTURES

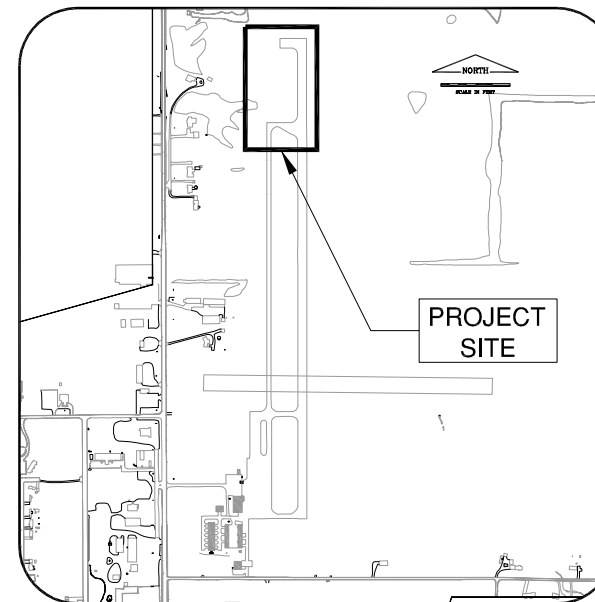
MAXIMUM ALLOWABLE EQUIPMENT HEIGHT = 25'

RADIO CONTROL FREQUENCY 123.075

CALL J.U.L.I.E. BEFORE EXCAVATING
 1-800-892-0123
 TOWNSHIP: T4S
 RANGE: R5W
 SECTION: 30
 COUNTY: RANDOLPH
 CIVIL TOWNSHIP: COULTERVILLE



LOCATION MAP



SITE PLAN

Exp. 11/30/2013

CHRISTOPHER B. GRATH
 062-056232
 LICENSED PROFESSIONAL ENGINEER OF ILLINOIS
 3/18/2013

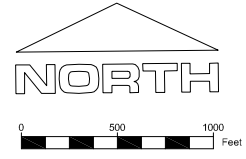
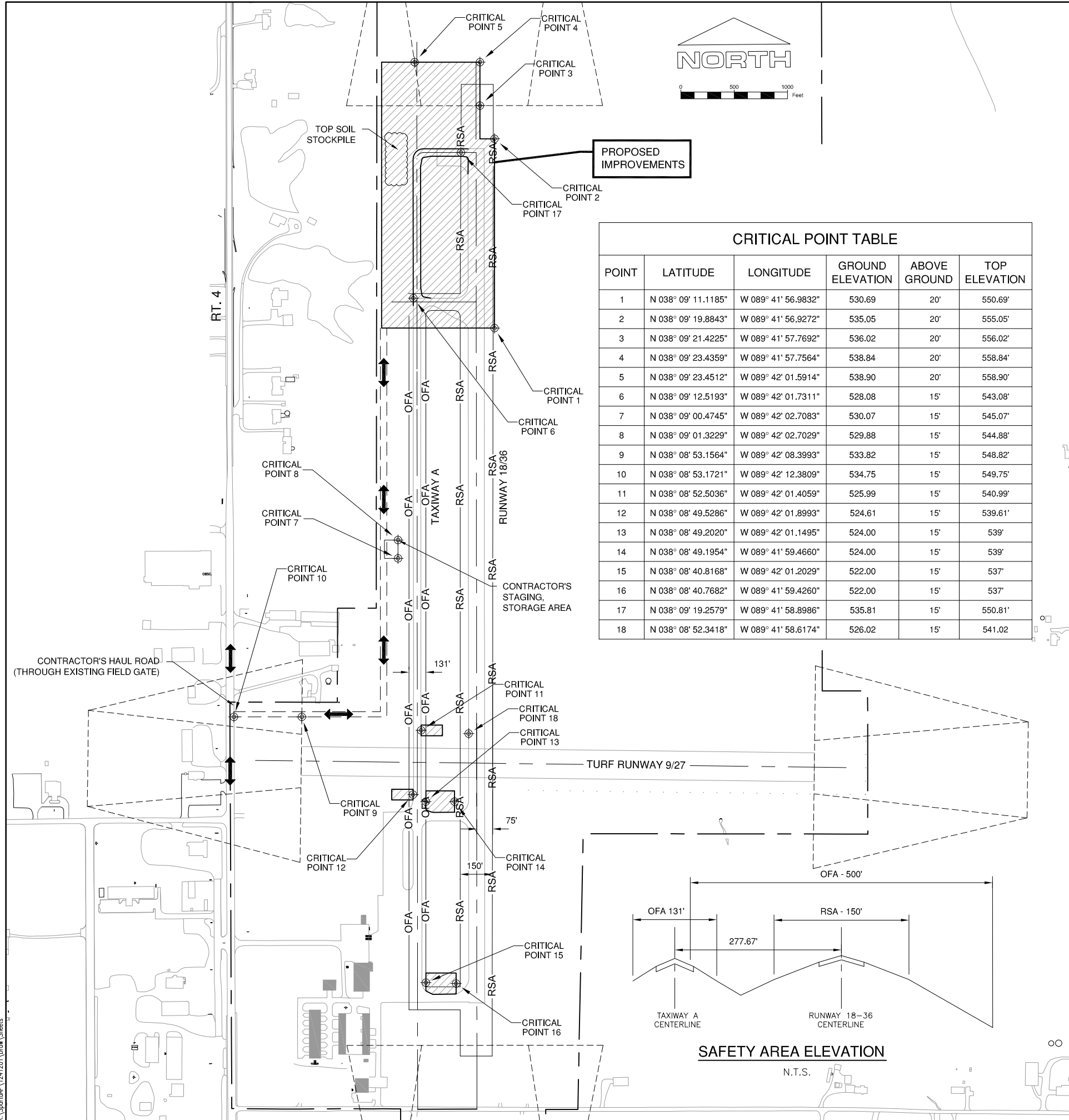
**SPARTA COMMUNITY AIRPORT
 SPARTA, ILLINOIS**

APPROVED *[Signature]*
 DATE 3/13/13 2013

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 CONSULTING ENGINEERS
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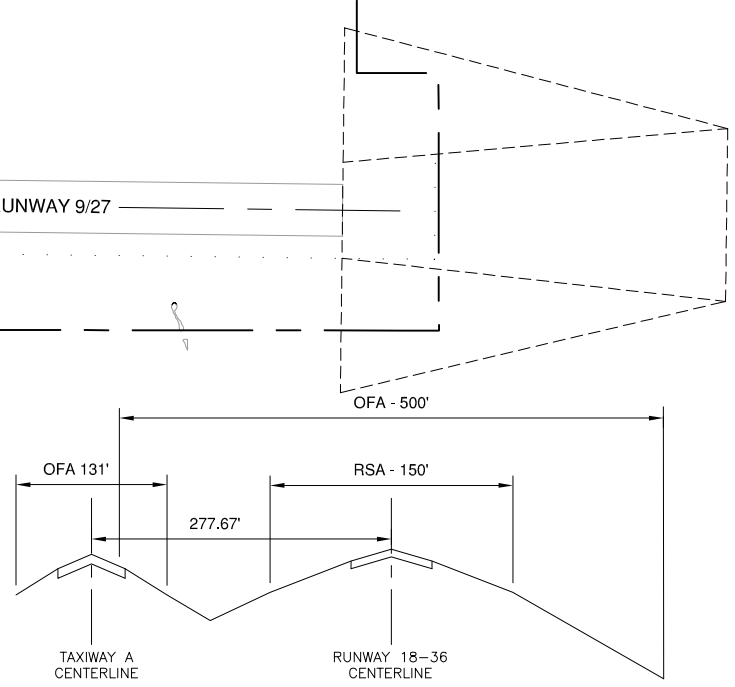
SUBMITTED BY *[Signature]*
 DATE: 3/18/2013

CMT JOB NUMBER 12412-01-00



CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
1	N 038° 09' 11.1185"	W 089° 41' 56.9832"	530.69	20'	550.69'
2	N 038° 09' 19.8843"	W 089° 41' 56.9272"	535.05	20'	555.05'
3	N 038° 09' 21.4225"	W 089° 41' 57.7692"	536.02	20'	556.02'
4	N 038° 09' 23.4359"	W 089° 41' 57.7564"	538.84	20'	558.84'
5	N 038° 09' 23.4512"	W 089° 42' 01.5914"	538.90	20'	558.90'
6	N 038° 09' 12.5193"	W 089° 42' 01.7311"	528.08	15'	543.08'
7	N 038° 09' 00.4745"	W 089° 42' 02.7083"	530.07	15'	545.07'
8	N 038° 09' 01.3229"	W 089° 42' 02.7029"	529.88	15'	544.88'
9	N 038° 08' 53.1564"	W 089° 42' 08.3993"	533.82	15'	548.82'
10	N 038° 08' 53.1721"	W 089° 42' 12.3809"	534.75	15'	549.75'
11	N 038° 08' 52.5036"	W 089° 42' 01.4059"	525.99	15'	540.99'
12	N 038° 08' 49.5286"	W 089° 42' 01.8993"	524.61	15'	539.61'
13	N 038° 08' 49.2020"	W 089° 42' 01.1495"	524.00	15'	539'
14	N 038° 08' 49.1954"	W 089° 41' 59.4660"	524.00	15'	539'
15	N 038° 08' 40.8168"	W 089° 42' 01.2029"	522.00	15'	537'
16	N 038° 08' 40.7682"	W 089° 41' 59.4260"	522.00	15'	537'
17	N 038° 09' 19.2579"	W 089° 41' 58.8986"	535.81	15'	550.81'
18	N 038° 08' 52.3418"	W 089° 41' 58.6174"	526.02	15'	541.02'



SAFETY AREA ELEVATION
N.T.S.

GENERAL NOTES

- MEN, VEHICLES AND EQUIPMENT WILL NOT BE ALLOWED WITHIN THE FOLLOWING AREAS, EXCEPT WHEN UNDER THE CONTROL OF A FLAGMAN IN RADIO CONTACT WITH AIR AND GROUND TRAFFIC AT ALL TIMES:
 - 75' FROM THE CENTERLINE OF 9/27 WHEN ACTIVE.
 - 75' FROM THE CENTERLINE OF 18/36 WHEN ACTIVE.
 - 66' FROM THE CENTER OF AN ACTIVE TAXIWAY.

THE CONTRACTOR SHALL PROVIDE THE FLAGMAN AND RADIO EQUIPMENT AT NO EXTRA COST TO THE CONTRACT.
- CONSTRUCTION TRAFFIC SHALL BE RESTRICTED TO DESIGNATED HAUL ROADS CONSTRUCTED BY THE CONTRACTOR. THE CONTRACTOR SHALL NOT ACCESS EXISTING AIRFIELD PAVEMENTS TO GAIN ACCESS TO THE CONSTRUCTION SITE.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT MANAGER.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN.
- BROKEN CONCRETE, BITUMINOUS MATERIALS, AND MISCELLANEOUS WASTE SHALL BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY.
- ALL PAVEMENTS, DRIVES, OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE AIRPORT.
- THE CONTRACTOR SHALL CONTINUOUSLY CLEAN ALL CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC.
- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER, THE AIRPORT AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - THE CONTRACTOR'S ACCESS TO THE WORK SHALL BE AS SHOWN IN THE CONSTRUCTION ACTIVITY PLAN.
 - THE CONTRACTOR SHALL ACCESS THE SITE OFF OF RT. 4 THE CONTRACTOR SHALL CONTACT LOCAL AUTHORITIES WHEN ESTABLISHING THIS CONSTRUCTION ENTRANCE AND ENSURE THAT ALL SIGNAGE IS IN PLACE PER THEIR DIRECTION AND ALL PERMITS (IF NEEDED) ARE IN PLACE. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T..
 - DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - THE CONTRACTOR WILL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRSPACE SURFACES, OR RUNWAY AND TAXIWAY OBJECT FREE OR SAFETY AREAS.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.

LEGEND

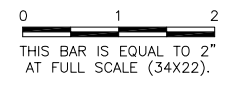
- TSA — TAXIWAY SAFETY AREA
- RSA — RUNWAY SAFETY AREA
- OFA — OBJECT FREE AREA
- - - - - PROPERTY LINE
- ↔ CONTRACTOR'S ACCESS
- ▨ WORK AREA

FILE: SAR4224-GC001.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 4/26/2013 2:14 PM
 SAR-V-ALAE
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SP023

REVISIONS

NUMBER	BY	DATE



**SPARTA COMMUNITY AIRPORT
 HUNTER FIELD
 SPARTA, ILLINOIS
 TAXIWAY EXTENSION
 SITE PLAN**

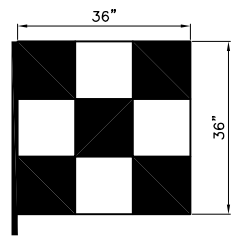
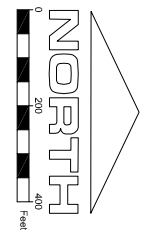
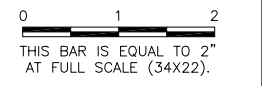
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SHEET 02 OF 28 SHEETS	

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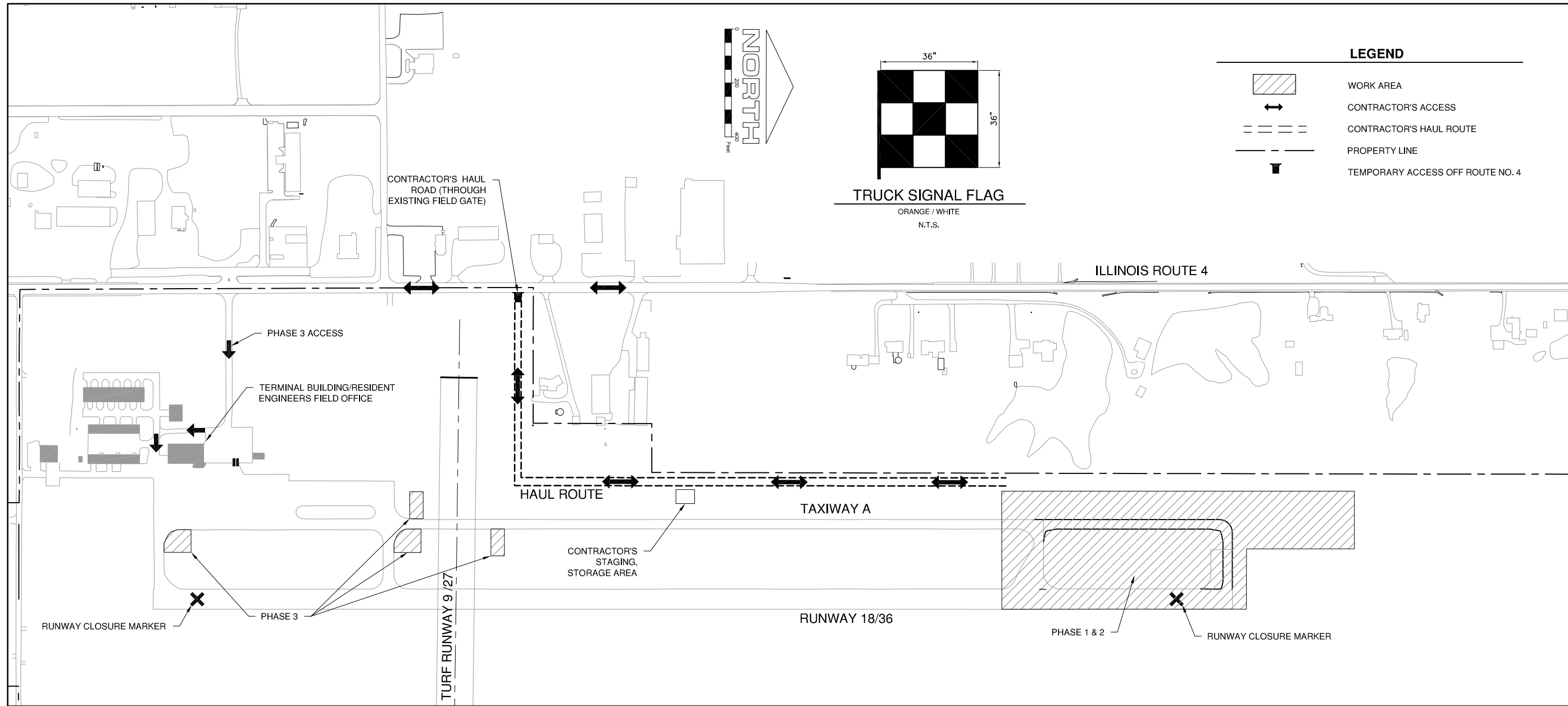
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TRUCK SIGNAL FLAG
 ORANGE / WHITE
 N.T.S.

LEGEND

	WORK AREA
	CONTRACTOR'S ACCESS
	CONTRACTOR'S HAUL ROUTE
	PROPERTY LINE
	TEMPORARY ACCESS OFF ROUTE NO. 4

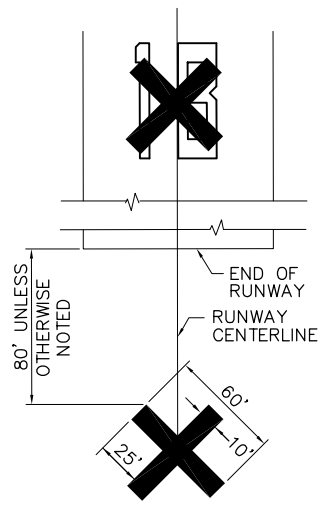


**SPARTA COMMUNITY AIRPORT
 HUNTER FIELD
 SPARTA, ILLINOIS**

**TAXIWAY EXTENSION
 CONSTRUCTION ACTIVITY PLAN INDEX**

NOTES

- MARKERS SHALL BE SOLID YELLOW.
- MARKERS SHALL BE PAINTED BURLAP, PLYWOOD, PLASTIC, OR OTHER APPROVED MATERIAL.
- CONTRACTOR SHALL MAINTAIN MARKERS.
- COST OF FURNISHING, INSTALLING, MAINTAINING, RELOCATING, AND REMOVING MARKERS SHALL BE INCIDENTAL TO THE CONTRACT.
- MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS. HOWEVER, WHEN WORK IS NECESSARY FOR THAT AREA, THE MARKERS SHALL BE RELOCATED OFF THE PAVEMENT AS SHOWN ON THE DETAIL.



CLOSED RUNWAY MARKER DETAIL

N.T.S.

PHASING INDEX NOTES:

- CONTRACTOR SHALL ACCESS PHASE 1 & 2 FROM ILLINOIS ROUTE 4. AIRPORT PAVEMENT SHALL NOT BE FOR THIS PHASE.
- CONTRACTOR WILL BE ALLOWED TO ACCESS PHASE 3 THROUGH EXISTING AIRPORT PAVEMENTS AS SHOWN. LIGHT DUTY TRUCKS & RUBBER TIRE EQUIPMENT ONLY WILL BE ALLOWED TO USE THIS ROUTE. CONTRACTOR SHALL HAVE SUFFICIENT MEANS TO CLEAN AIRFIELD PAVEMENT ON SITE DURING THIS PHASE.
- SEE INDIVIDUAL PHASING SHEETS FOR ADDITIONAL PHASING NOTES.

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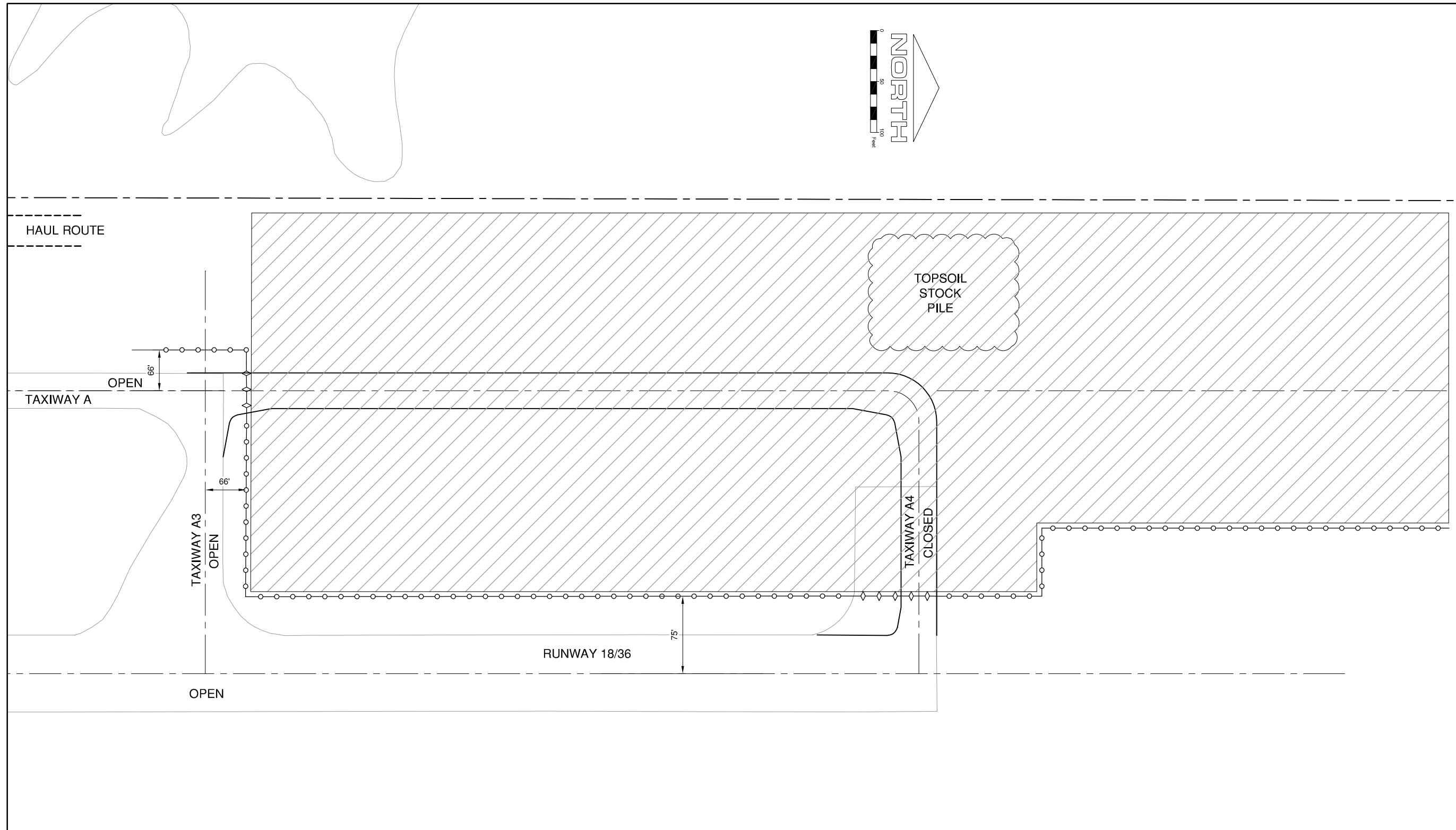
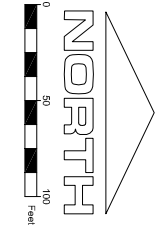
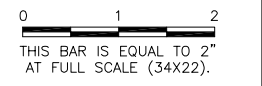


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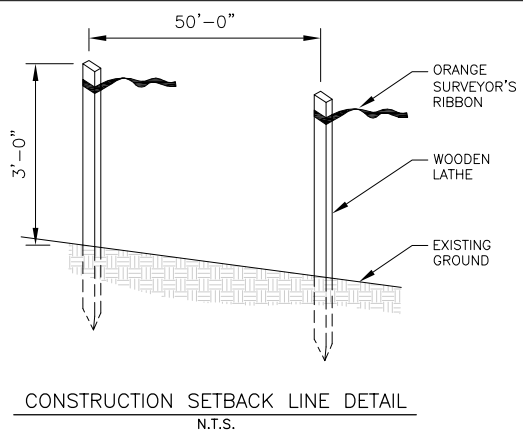
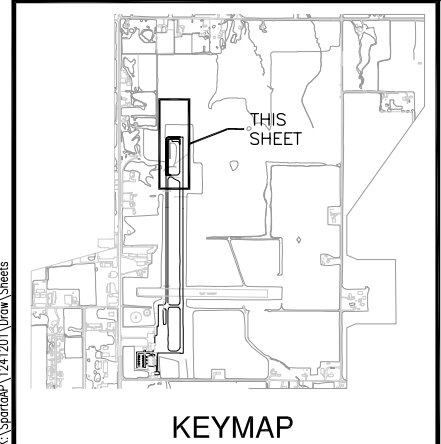
**SPARTA COMMUNITY AIRPORT
 HUNTER FIELD
 SPARTA, ILLINOIS**

**TAXIWAY EXTENSION
 CONSTRUCTION ACTIVITY PLAN 1**

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SHEET	04 OF 28 SHEETS



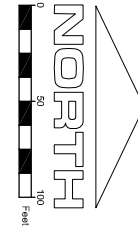
PHASE 1 NOTES:

1. CONTRACTOR SHALL ACCESS SITE FROM HAUL ROUTE SHOWN .
2. CONTRACTOR SHALL PLACE BARRICADES & SET BACK LINE PRIOR TO BEGINNING WORK IN THIS PHASE.
3. WORK IN THIS PHASE SHALL CONSIST OF EMBANKMENT EXCAVATION, PAVEMENT CONSTRUCTION, DRAINAGE IMPROVEMENTS, TURFING & ELECTRICAL IMPROVEMENTS.

LEGEND

	WORK AREA
	CONTRACTOR'S ACCESS
	CONTRACTOR'S HAUL ROUTE
	PROPERTY LINE
	CONSTRUCTION SET BACK STAKES
	BARRICADES

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PHASE 2 NOTES:

1. THIS WORK SHALL CONSIST OF THE WORK LOCATED WITHIN 75' OF THE RUNWAY CENTERLINE INCLUDING BUT NOT LIMITED TO THE CONSTRUCTION OF PAVEMENT REMOVAL, CONSTRUCTING EMBANKMENT, CABLING AND LIGHTING IMPROVEMENTS.
2. THE CONTRACTOR SHALL BE ALLOWED TO CLOSE THE RUNWAY DURING WORKING HOURS TO COMPLETE THE WORK. THE RUNWAY SHALL BE CLEANED AND REOPENED AT THE END OF EACH WORKING DAY. THE SEQUENCE OF CLOSING AND OPENING THE RUNWAY SHALL BE GOVERNED BY THE REQUIREMENTS SET FORTH IN FAA ADVISORY CIRCULAR 5370-2 CURRENT VERSION; IN GENERAL THE SEQUENCE WILL BE:
 - 2.1 COORDINATE CLOSURE WITH THE AIRPORT MANAGER
 - 2.2 NOTIFY AIR TRAFFIC OF RUNWAY CLOSURE
 - 2.3 DISABLE VISUAL AIDS
 - 2.4 PLACE CLOSURE MARKERS
 - 2.5 KEEP EQUIPMENT CLEAR OF THE RUNWAY PROPER AND MONITOR UNICOM
 - 2.6 COMPLETE WORK
 - 2.7 CLEAN PAVEMENTS
 - 2.8 COORDINATE OPENING WITH AIRPORT MANAGER
 - 2.9 REMOVE RUNWAY CLOSURE MARKERS
 - 2.10 NOTIFY AIR TRAFFIC
3. THE CONTRACTOR SHALL BE GIVEN 12 CONSECUTIVE CALENDAR DAYS, STARTING ON A MONDAY SELECTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT MANAGER TO COMPLETE THE WORK IN PHASE 2.
4. LIQUIDATED DAMAGES OF \$500 PER DAY WILL BE CHARGED FOR ANY DAYS BEYOND THE SPECIFIED AMOUNT THAT THE RUNWAY MUST BE CLOSED.
5. THE CONTRACTOR MAY CONTINUE TO WORK IN PHASE 1 AREA DURING PHASE 2.
6. CONTRACTOR SHALL PLACE ADDITIONAL BARRICADES ON THE NORTH SIDE OF THE TURF RUNWAY AND AT THE SOUTH CONNECTING TAXIWAYS. LOCATIONS NOT SHOWN ON THIS SHEET.

FILE: SAR4224-GC005.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 4/26/2013 2:15 PM

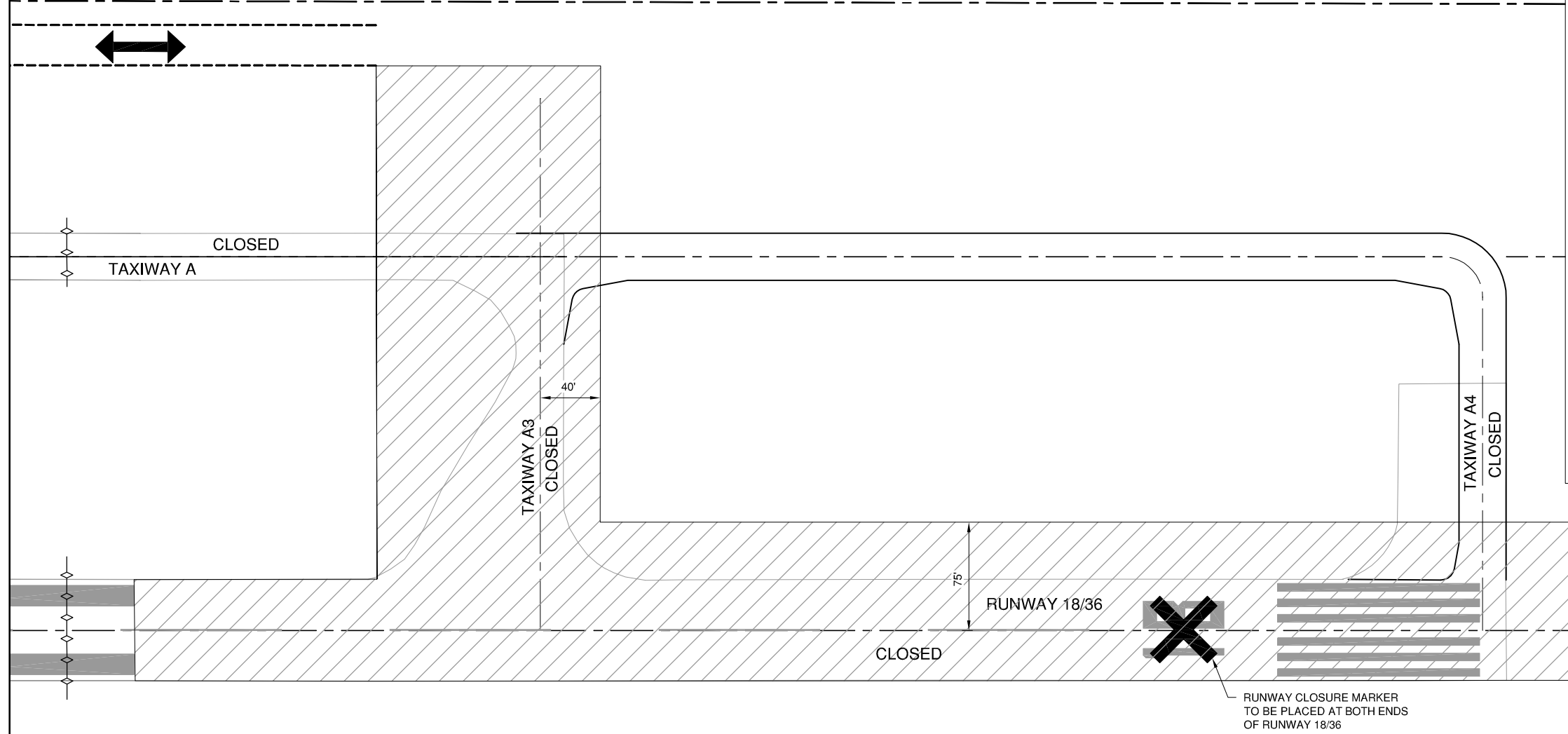
SAR-V-ALAE
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SP023

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



RUNWAY CLOSURE MARKER TO BE PLACED AT BOTH ENDS OF RUNWAY 18/36

SPARTA COMMUNITY AIRPORT
 HUNTER FIELD
 SPARTA, ILLINOIS

TAXIWAY EXTENSION
 CONSTRUCTION ACTIVITY PLAN 2

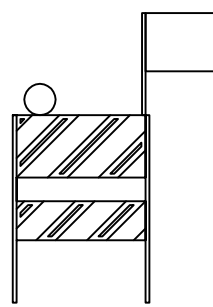
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JOB No:	1241201
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SBG NO.	3-17-0094-B11
SHEET	05 OF 28 SHEETS

STEADY BURN BARRICADE NOTES:

1. STEADY BURN TO BE BATTERY OPERATED OMNI DIRECTIONAL LENS TO BE RED.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 15' INTERVALS.
5. ONE 20" x 20" ORANGE FLAG TO BE INSTALLED ON EACH BARRICADE AND SECURELY FASTENED.



FLASHER BARRICADE DETAIL

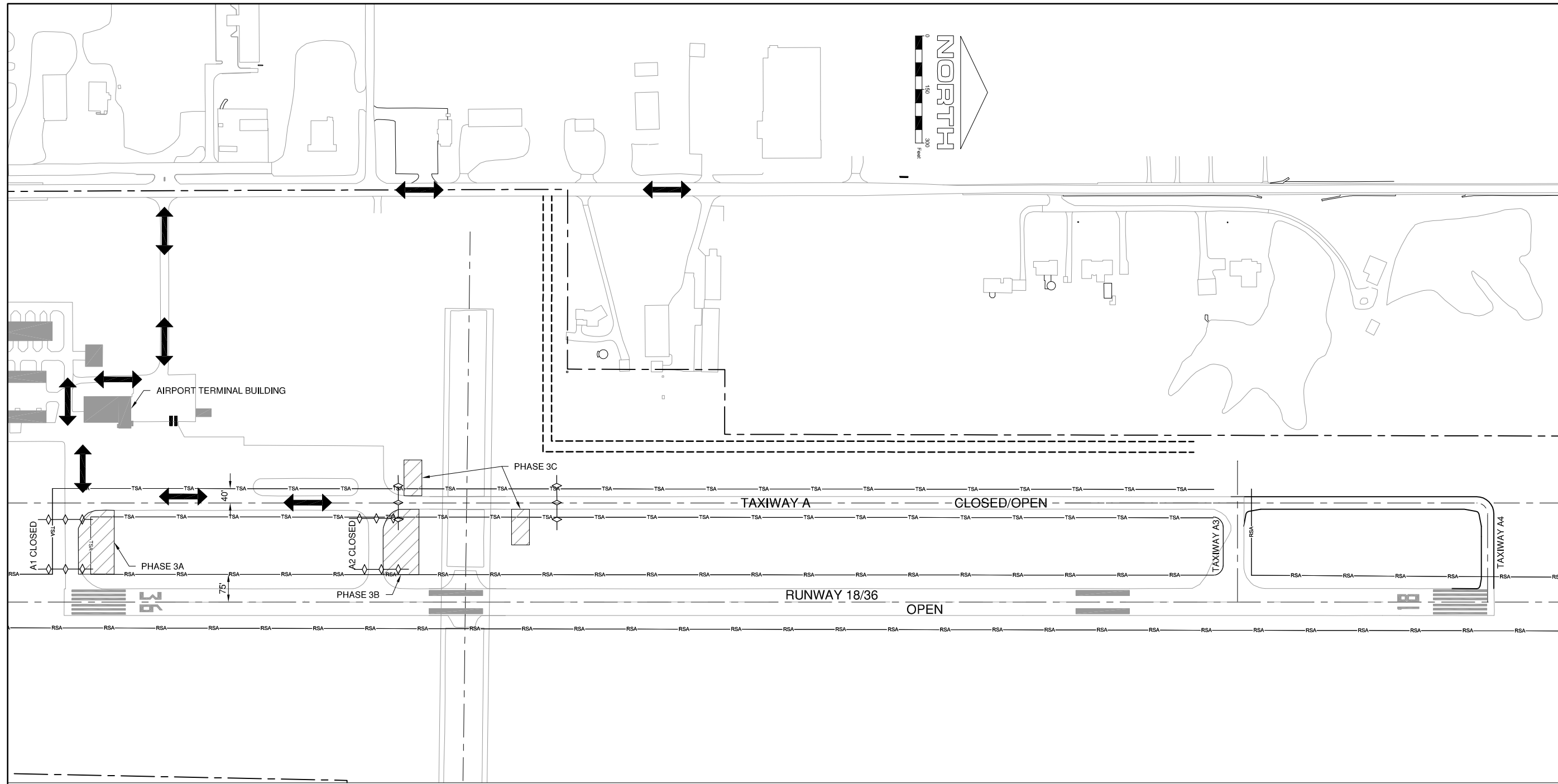
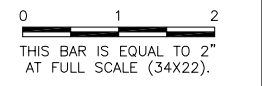
N.T.S.

LEGEND

- WORK AREA
- CONTRACTOR'S ACCESS
- CONTRACTOR'S HAUL ROUTE
- PROPERTY LINE
- BARRICADES

SP023

REVISIONS		
NUMBER	BY	DATE



**SPARTA COMMUNITY AIRPORT
 HUNTER FIELD
 SPARTA, ILLINOIS**

**TAXIWAY EXTENSION
 CONSTRUCTION ACTIVITY PLAN 3**

PHASE 3 NOTES:

1. PHASE 3 SHALL CONSIST OF ELECTRICAL IMPROVEMENTS, INCLUDING REMOVING EXISTING SIGNS & REPLACING THEM WITH NEW SIGNS & ASSOCIATED CABLING.
2. WORK WITHIN THESE PHASES WILL BE OUTSIDE THE RUNWAY SAFETY AREA BUT WITHIN THE TAXIWAY SAFETY AREA.
3. WORK WITHIN THE SUB PHASED 3A, 3B, 3C SHALL NOT BE CONSTRUCTED CONCURRENTLY. THE AIRPORT SHALL REQUIRE OPEN ACCESS TO RUNWAY 18/36 AT ALL TIMES.
4. CONTRACTOR SHALL ACCESS THE SITE THRU THE PAVEMENTS NEAR THE AIRPORT BUILDINGS.
5. CONTRACTOR SHALL HAVE SUFFICIENT MEANS ON SITE TO CLEAN THE PAVEMENTS DURING THIS PHASE.

LEGEND

	WORK AREA
	CONTRACTOR'S ACCESS
	CONTRACTOR'S HAUL ROUTE
	PROPERTY LINE
	CONSTRUCTION SET BACK STAKES
	BARRICADES

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IL. PROJ. NO. SAR-4224 SBG NO. 3-17-0094-B11	
SHEET 06 OF 28 SHEETS	

GENERAL

- 1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP)...

1. COORDINATION

- 1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA)...

2. PHASING

- 1. TOTAL CONTRACT TIME SHALL BE 54 CALENDAR DAYS.
- 2. WORK ON THIS PROJECT LOCATED WITHIN 75' OF THE RUNWAY 18-36 CENTERLINE WILL SEVERELY LIMIT OPERATIONS AT THE AIRPORT...

PHASE 1 NOTES

- 1. THIS WORK SHALL CONSIST OF ALL WORK ON THE PROJECT NOT WITHIN 75' OF EITHER SIDE OF THE RUNWAY 18-36 CENTERLINE...

PHASE 2 NOTES

- 1. THIS WORK SHALL CONSIST OF THE WORK LOCATED WITHIN 75' OF THE RUNWAY CENTERLINE INCLUDING BUT NOT LIMITED TO THE CONSTRUCTION OF PAVEMENT REMOVAL...

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- 1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

- 1. THE CONTRACTOR SHALL REMAIN CLEAR OF THE RUNWAY 18 END REILs AFTER INSTALLATION, PAPI, WINDCONE AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

- 1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
- 2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE ENTRANCE SHOWN.

6. WILDLIFE MANAGEMENT

- 1. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- 1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- 1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- 1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- 2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS...

AIRPORT MANAGER
SCOTT MARQUARDT - AIRPORT OPERATOR (618) 443-2002
ENGINEER
CHRIS GROTH P.E. - PROJECT ENGINEER (217) 787-8050
CMT - RESIDENT ENGINEER (217) 787-8050

10. INSPECTION REQUIREMENTS

- 1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.

11. UNDERGROUND UTILITIES

- 1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS...

12. PENALTIES

- 1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

- 1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- 1. PROJECT WILL REQUIRE THE CLOSURE OF RUNWAY 18-36 FOR A LIMITED DURATION DURING PHASE 2. THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2F.

15. MARKING AND SIGNS FOR ACCESS ROUTES

- 1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THIS SHEET AND THE CONSTRUCTION ACTIVITY PLAN SHEET.

16. HAZARD MARKING AND LIGHTING

- 1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.

17. PROTECTION

- 1. ALL WORK REQUIRED INSIDE OF THE RUNWAY 18/36 SAFETY AREAS, WHICH EXTENDS 250' FROM THE RUNWAY CENTERLINE, WILL REQUIRE THE RUNWAY TO BE CLOSED...

18. OTHER LIMITATIONS ON CONSTRUCTION

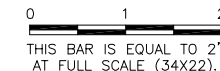
- 1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.

FILE: SAR4224-GC003.dwg
UPDATE BY: Chris Groth
PLOT DATE: 4/26/2013 2:16 PM

SP023

REVISIONS

Table with 3 columns: NUMBER, BY, DATE



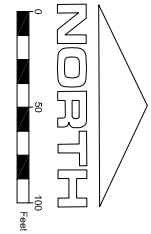
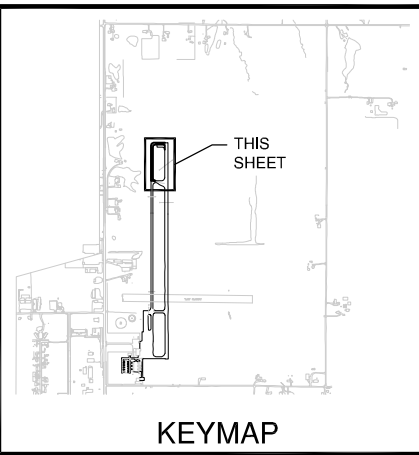
SPARTA COMMUNITY AIRPORT
HUNTER FIELD
SPARTA, ILLINOIS

TAXIWAY EXTENSION
CONSTRUCTION SAFETY PHASING
PLAN NOTES



Table with 2 columns: Field Name, Value. Fields include DESIGN BY, DRAWN BY, CHECKED BY, APPROVED BY, DATE, JOB No.

IL PROJ. NO. SAR-4224
SBG NO. 3-17-0094-B11



LEGEND

- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- ◻ EXISTING BASE MOUNTED TAXIWAY LIGHT
- ▭ EXISTING TAXIWAY GUIDANCE SIGN
- EXISTING PAVING MARKING
- /// PAVEMENT MARKER REMOVAL
- - - EXISTING TAXIWAY CIRCUIT
- - - EXISTING RUNWAY CIRCUIT
- CP — EXISTING CULVERT
- R REMOVALS
- ▽▽ EXISTING REIL
- ◻ EXISTING RUNWAY THRESHOLD LIGHT
- ◻ EXISTING RUNWAY LIGHT
- ~ BRUSH LINE
- ⊕ SPLICE CAN
- P EXISTING PAPI POWER
- ▭ EXISTING DUCT BANK

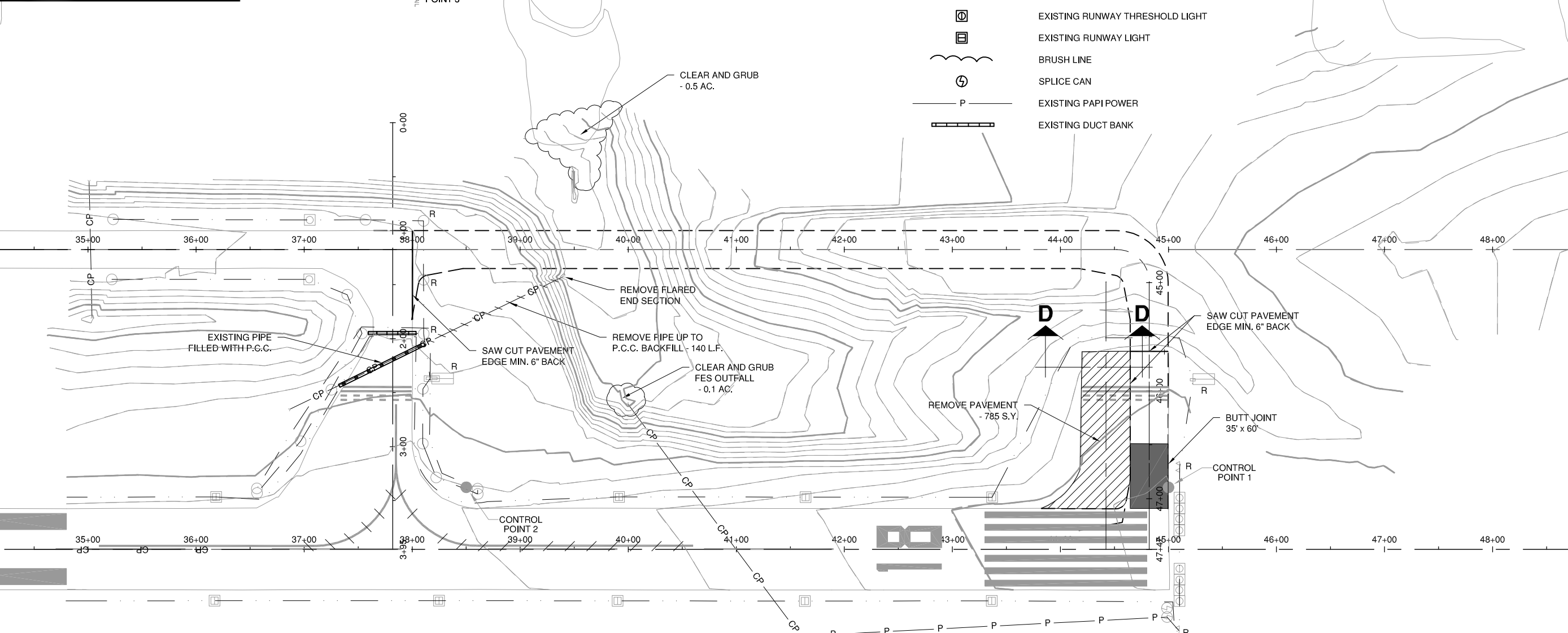
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 PLOT DATE: 4/26/2013 2:17 PM

SAR-V-ALAE
 SAR-C-SPAL
 SAR-V-AL
 KEYMAP

SP023

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



CONTROL POINT TABLE

POINT	NORTHING	EASTING	ELEVATION
1	542389.931	2430869.603	536.11
2	541739.956	2430870.097	531.10
3	541696.657	2430409.086	518.58
4 NOT SHOWN	541107.332	2430870.047	530.18
5 NOT SHOWN	540439.951	2430870.980	530.22

SPARTA COMMUNITY AIRPORT
 HUNTER FIELD
 SPARTA, ILLINOIS
 TAXIWAY EXTENSION
 EXISTING CONDITIONS AND REMOVALS

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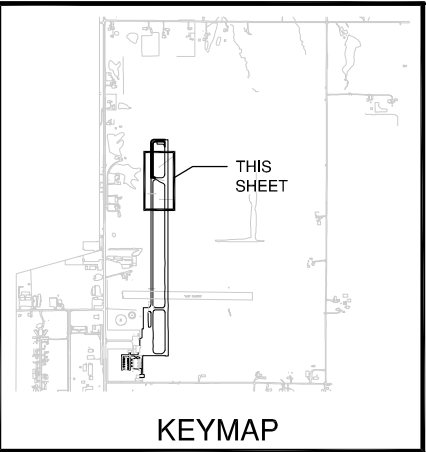
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 CHECKED BY: CET
 APPROVED BY: CET
 DATE: MARCH 1, 2013
 JOB No: 1241201
 IL. PROJ. NO. SAR-4224
 SBG NO. 3-17-0094-B11
 SHEET 08 OF 28 SHEETS

SAR-V-ALAE
 SAR-C-SPAL
 SAR-V-AL
 KEYMAP

SP023

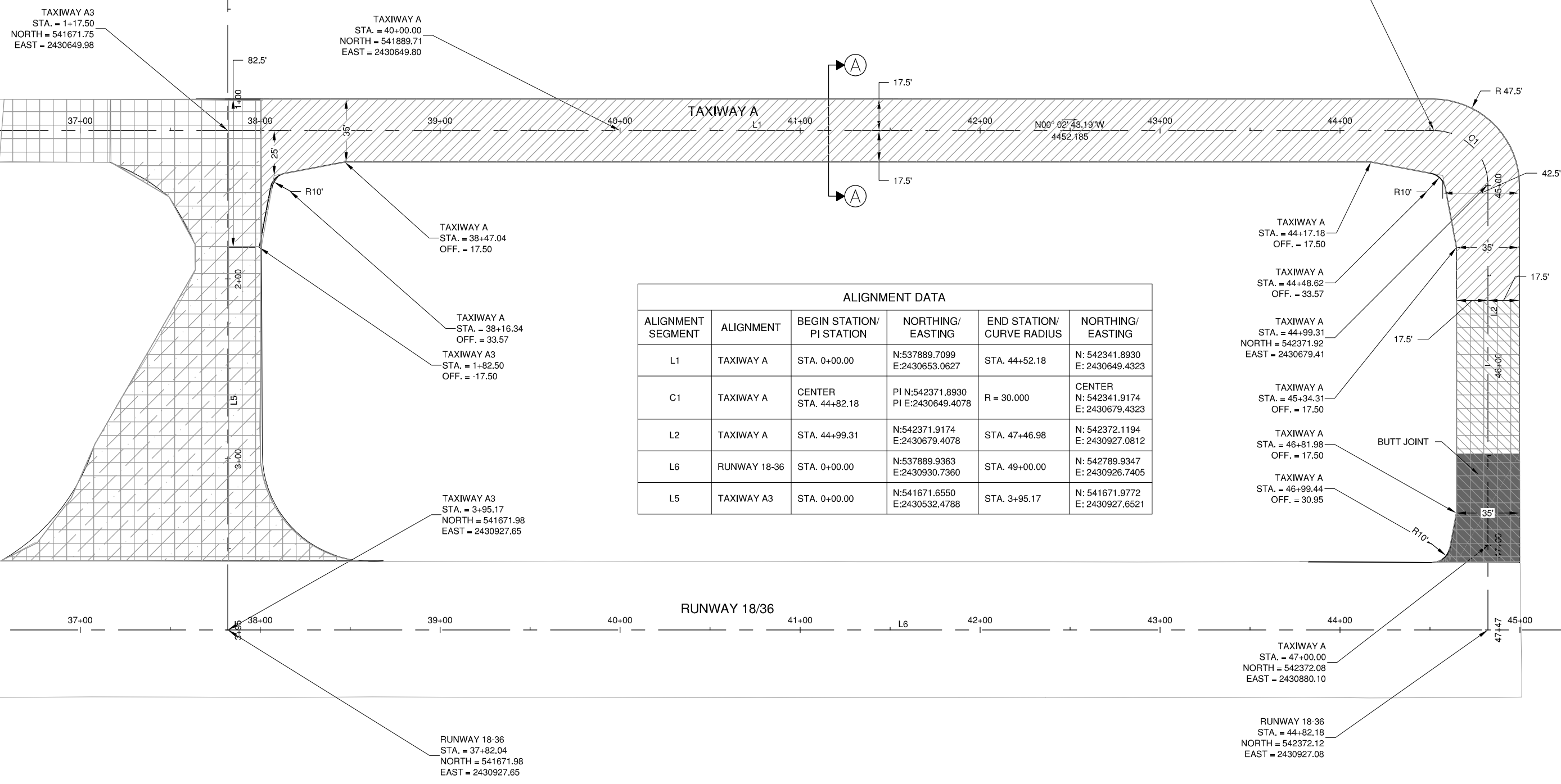
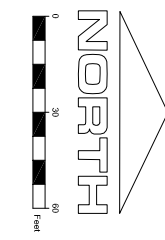
REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



LEGEND

- NEW 1 1/2" BITUMINOUS SURFACE (401)
 NEW 2 1/2" BITUMINOUS BASE (408)
 NEW 11" CRUSHED AGGREGATE BASE (209)
- NEW BITUMINOUS SEAL COAT
- EXISTING 4" BITUMINOUS
 EXISTING 8" AGGREGATE BASE
- NEW 1 1/2" BITUMINOUS SURFACE (401)
- BUTT JOINT - NEW 1 1/2" BITUMINOUS SURFACE (401)



ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L1	TAXIWAY A	STA. 0+00.00	N:537889.7099 E:2430653.0627	STA. 44+52.18	N: 542341.8930 E: 2430649.4323
C1	TAXIWAY A	CENTER STA. 44+82.18	PI N:542371.8930 PI E:2430649.4078	R = 30.000	CENTER N: 542341.9174 E: 2430679.4323
L2	TAXIWAY A	STA. 44+99.31	N:542371.9174 E:2430679.4078	STA. 47+46.98	N: 542372.1194 E: 2430927.0812
L6	RUNWAY 18-36	STA. 0+00.00	N:537889.9363 E:2430930.7360	STA. 49+00.00	N: 542789.9347 E: 2430926.7405
L5	TAXIWAY A3	STA. 0+00.00	N:541671.6550 E:2430532.4788	STA. 3+95.17	N: 541671.9772 E: 2430927.6521

**SPARTA COMMUNITY AIRPORT
 HUNTER FIELD
 SPARTA, ILLINOIS**

**TAXIWAY EXTENSION
 PROPOSED IMPROVEMENTS**

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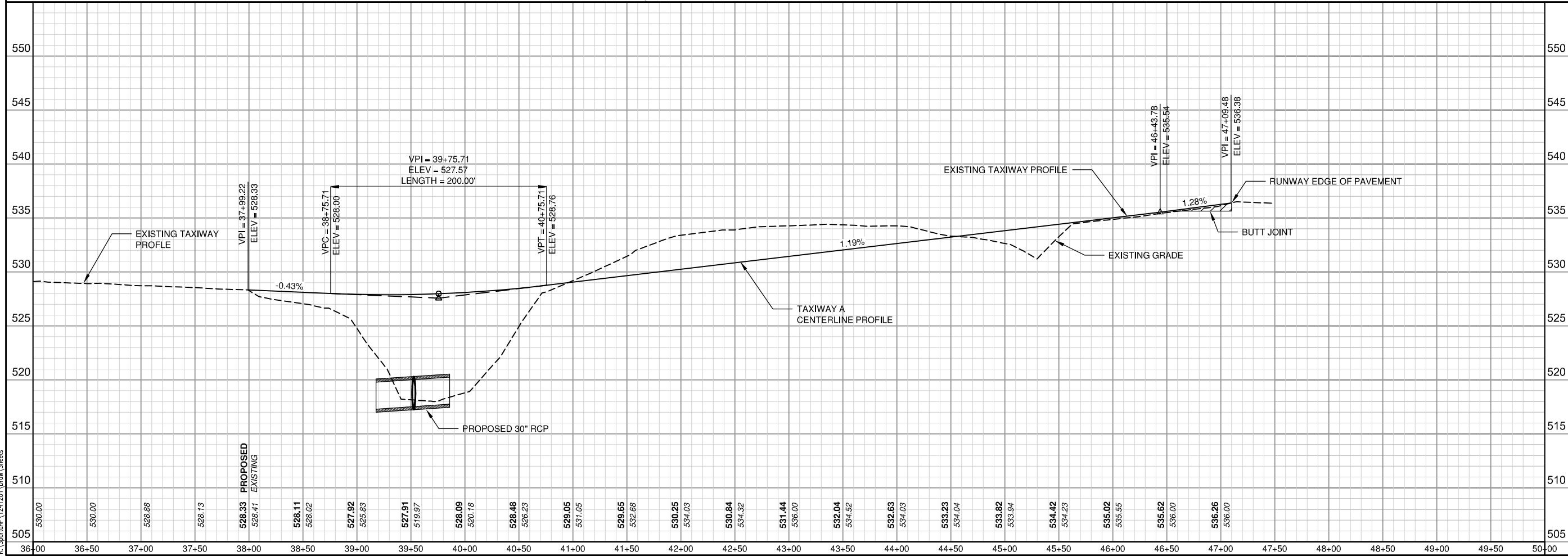
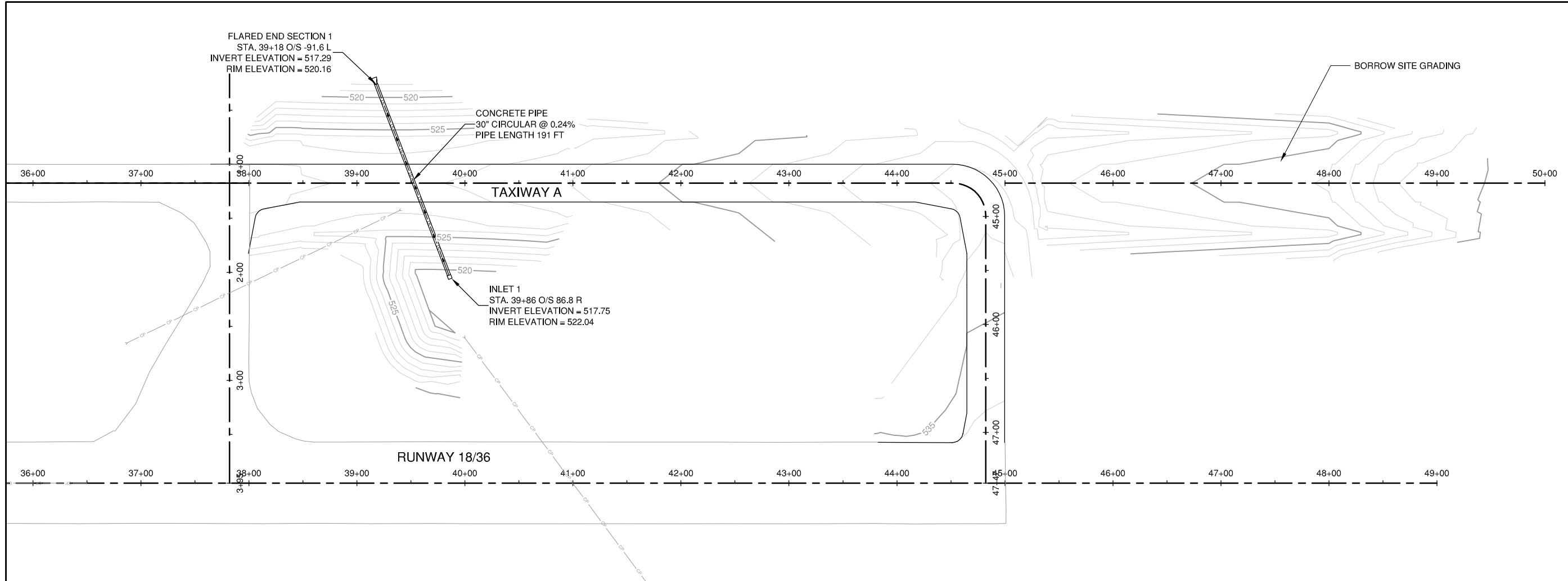
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SHEET 09 OF 28 SHEETS	

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SP023

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



**SPARTA COMMUNITY AIRPORT
 HUNTER FIELD
 SPARTA, ILLINOIS**

**TAXIWAY EXTENSION
 TAXIWAY A PLAN & PROFILE SHEET
 STA. 35+00 TO STA. 50+00**

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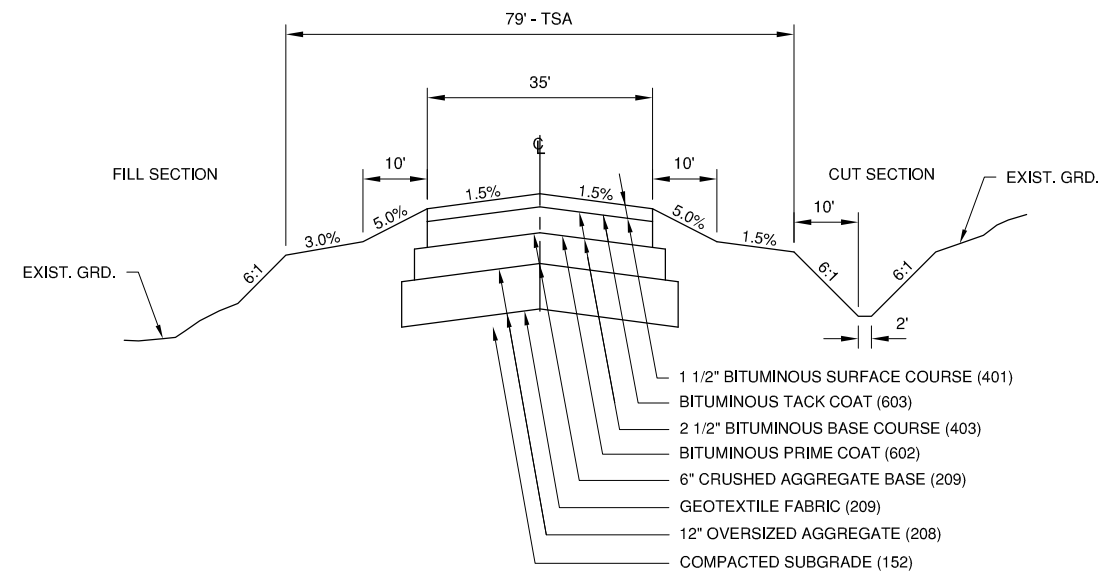
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SHEET	10 OF 28 SHEETS

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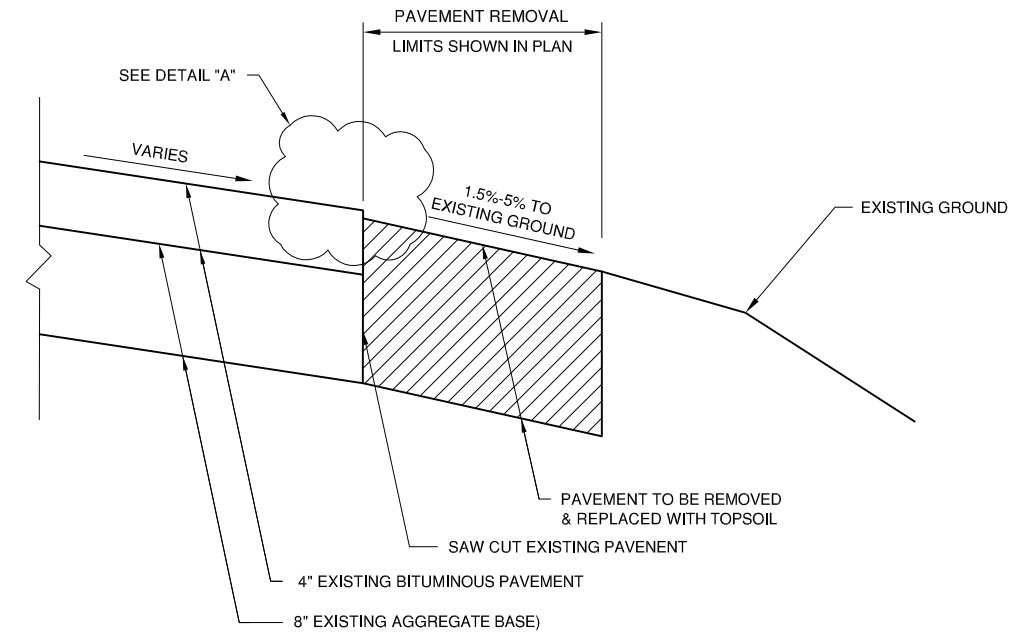
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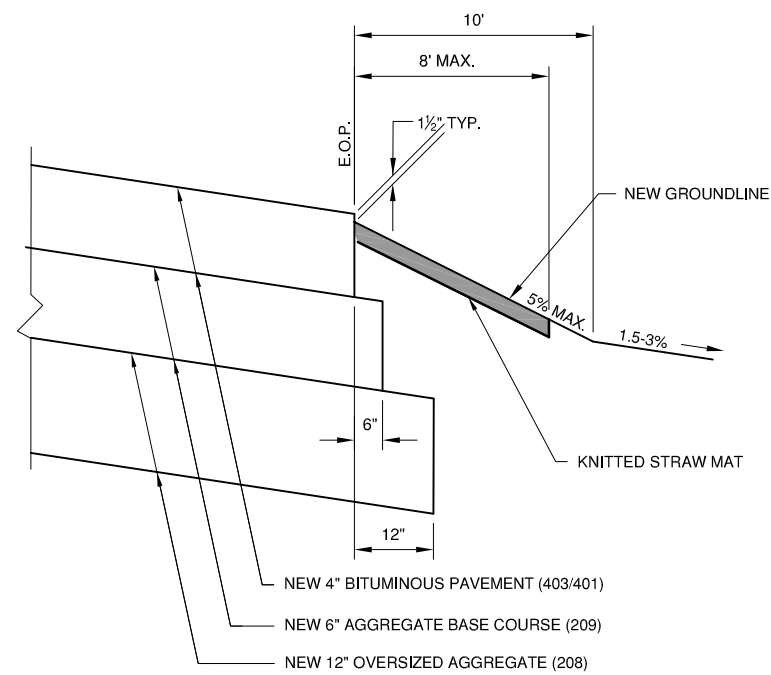
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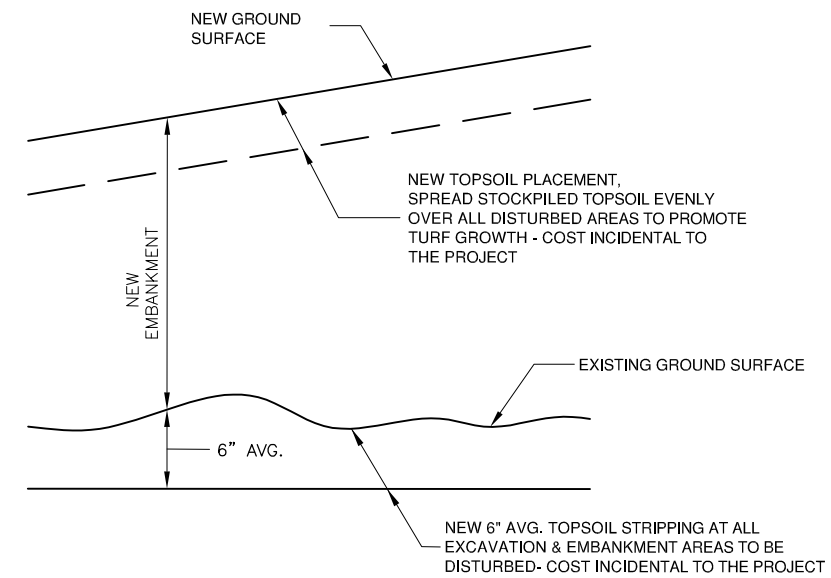
SECTION A-A
 N.T.S.



REMOVAL SECTION D-D
 N.T.S.



NEW SHOULDER DETAIL "A"
 N.T.S.



TOPSOIL STRIPPING DETAIL
 N.T.S.

**SPARTA COMMUNITY AIRPORT
 HUNTER FIELD
 SPARTA, ILLINOIS**

**TAXIWAY EXTENSION
 TYPICAL SECTIONS**

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SBG NO.	3-17-0094-B11
SHEET	11 OF 28 SHEETS

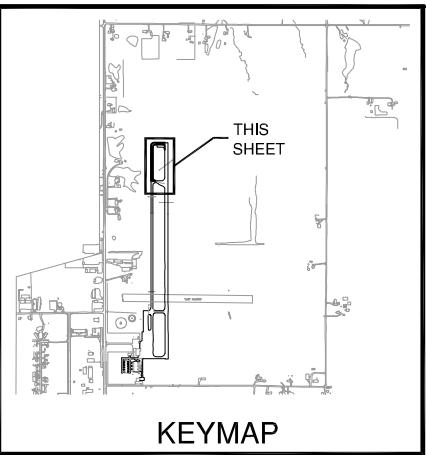
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 SAR-C-SPAL
 SAR-V-AL
 SAR-E-AL
 KEYMAP

SP023

REVISIONS

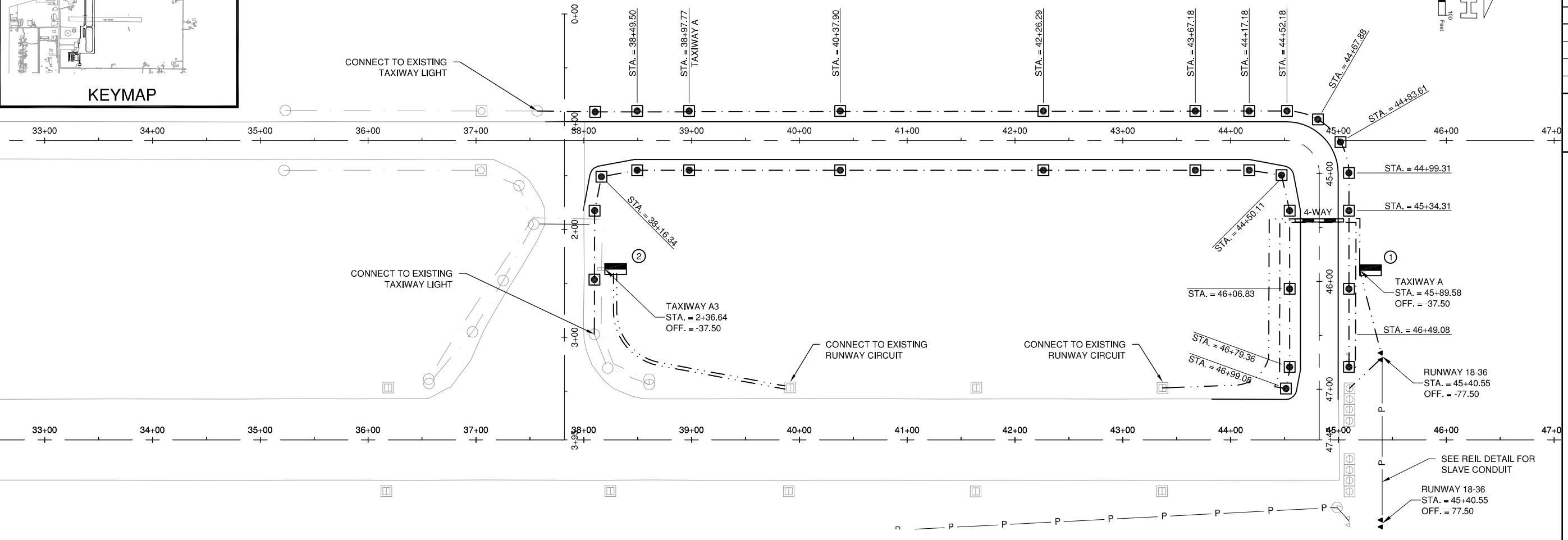
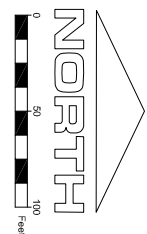
NUMBER	BY	DATE

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 AT FULL SCALE (34X22).



LEGEND

- NEW STAKE MOUNTED TAXIWAY LIGHT
- ◻ NEW BASE MOUNTED TAXIWAY LIGHT
- ▲ NEW REIL LOCATION
- ▬ NEW TAXIWAY GUIDANCE SIGN
- - - NEW TAXIWAY CIRCUIT
- — — EXISTING TAXIWAY CIRCUIT
- · - · - NEW RUNWAY CIRCUIT
- - - EXISTING RUNWAY CIRCUIT
- P — EXISTING PAPI POWER CABLE
- ▬ NEW DUCT BANK



SIGNAGE SCHEDULE

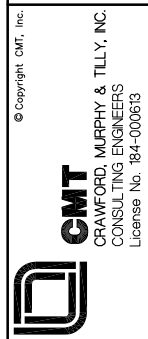
SIGN #	SIDE	NEW SIGN LEGEND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
1	W E	A4 18 A4	18 -	- -	A4 A4	4	RUNWAY	AR125444
2	W E	A3 18-36 A3	18-36 -	- -	A3 A3	7	RUNWAY	AR125447
3	N S	A 9-27 A	9-27 -	- -	A A	5	TAXIWAY A	AR125445
4	N S	A A 9-27	- 9-27	- -	A A	5	TAXIWAY A	AR125445
5	W E	A2 18-36 A2	18-36 -	- -	A2 A2	7	-	AR125447
6	W E	A1 36 A1	36 -	- -	A1 A1	4	-	AR125444

NOTES

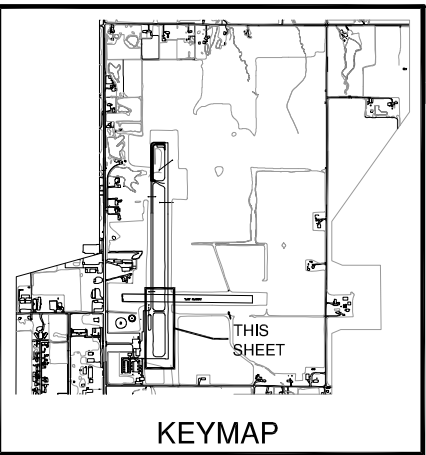
- SIGN SCHEDULE IS SUBJECT TO FAA APPROVAL OF THE SIGNAGE PLAN.
- CHANGES TO NEW LEGENDS MAY OCCUR SUBJECT TO NOTE 1 ABOVE.
- CONTRACTOR SHALL SUBMIT SHOP DRAWINGS WITH NEW LEGENDS FOR APPROVAL PRIOR TO STARTING MANUFACTURE.

SPARTA COMMUNITY AIRPORT
 HUNTER FIELD
 SPARTA, ILLINOIS

TAXIWAY EXTENSION
 ELECTRICAL LAYOUT AND LIGHTING PLAN 1

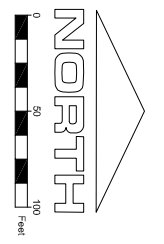


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SHEET 12 OF 28 SHEETS	



LEGEND

- NEW STAKE MOUNTED TAXIWAY LIGHT
- ◻ NEW BASE MOUNTED TAXIWAY LIGHT
- ▲ NEW REIL LOCATION
- ▬ NEW TAXIWAY GUIDANCE SIGN
- NEW TAXIWAY CIRCUIT
- EXISTING TAXIWAY CIRCUIT
- NEW RUNWAY CIRCUIT
- EXISTING RUNWAY CIRCUIT
- P — EXISTING PAPI POWER CABLE
- ▬ NEW DUCT BANK



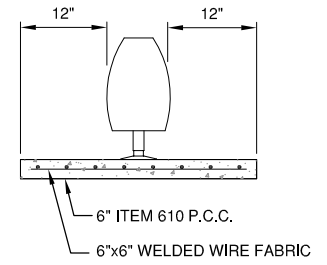
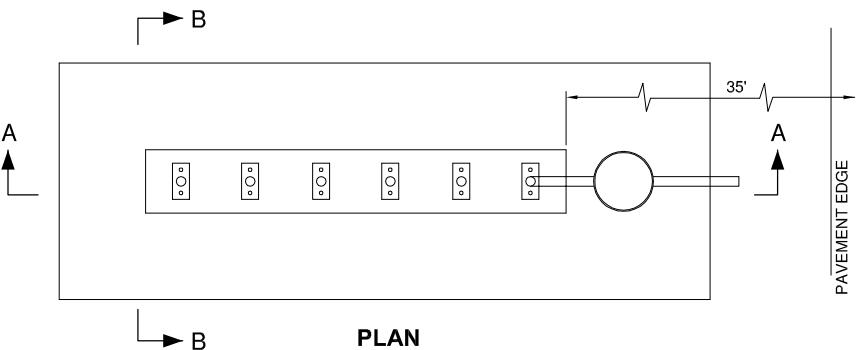
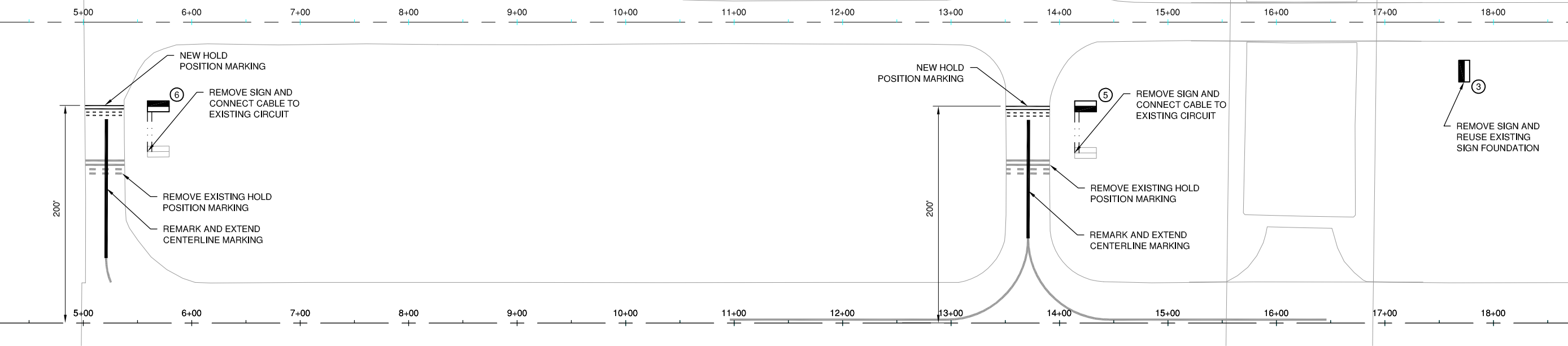
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 PLOT DATE: 4/26/2013 2:21 PM

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 SAR-C-SPAL
 SAR-V-AL
 SAR-E-AL
 KEYMAP
 SAR-C-MP

SP023

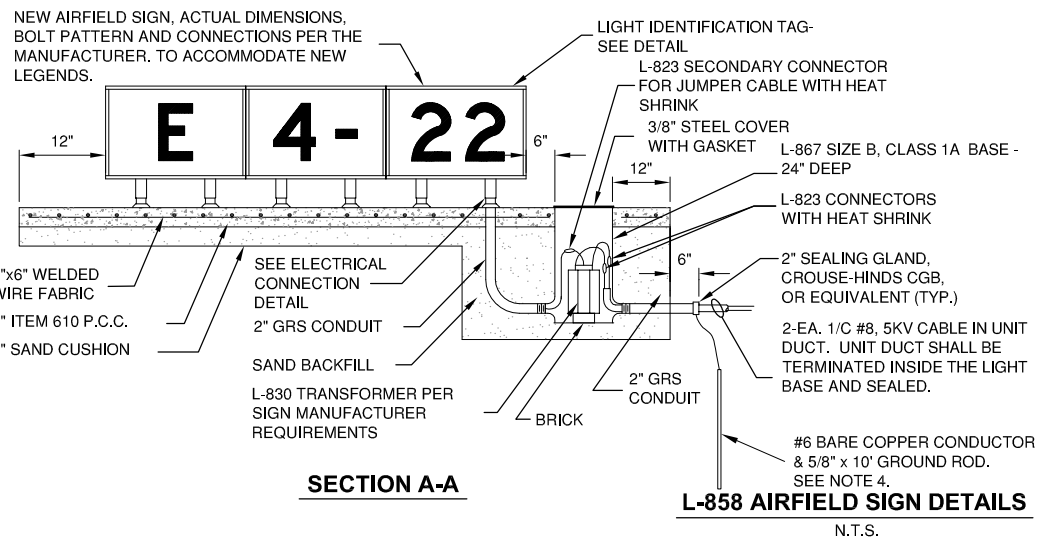
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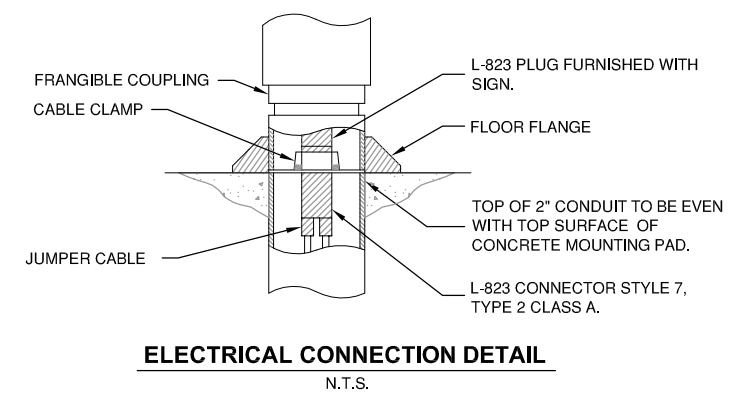


AIRFIELD SIGN NOTES

1. TRANSFORMER WATTAGE SHALL BE AS REQUIRED BY SIGN MANUFACTURER.
2. SIGN LEGEND SHALL BE AS SHOWN IN THE PLANS.
3. SIGNS SHALL BE SIZE 1, STYLE 2 OR 3, CLASS 2.
4. SEE "NEW BASE MOUNTED EDGE LIGHT" DETAIL FOR GROUNDING DETAILS.



SECTION B-B



ELECTRICAL CONNECTION DETAIL

N.T.S.

SPARTA COMMUNITY AIRPORT
 HUNTER FIELD
 SPARTA, ILLINOIS

TAXIWAY EXTENSION
 ELECTRICAL LAYOUT AND LIGHTING PLAN 2

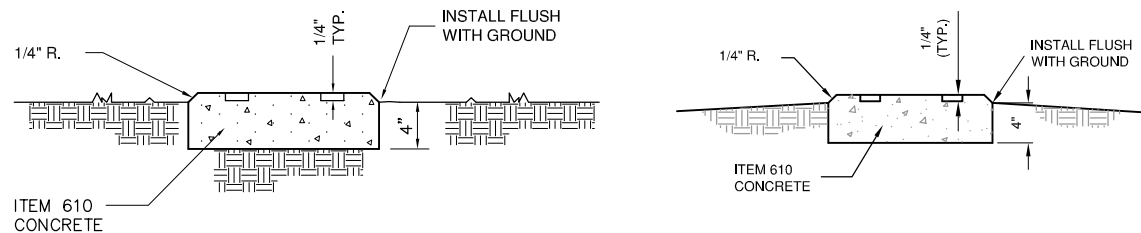
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IL PROJ. NO.	SAR-4224
SBG NO.	3-17-0094-B11
SHEET	13 OF 28 SHEETS

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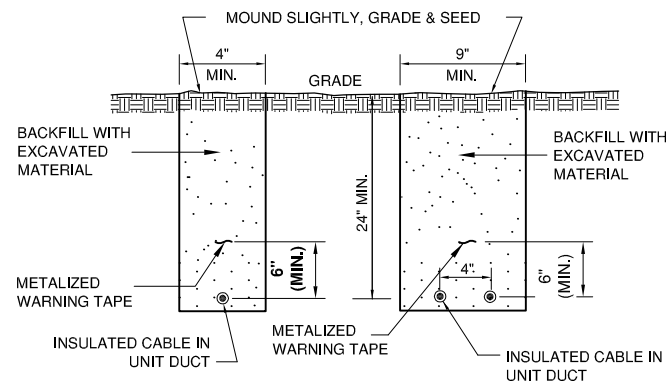
TURF CABLE / SPLICE MARKER NOTES

- CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE HOMERUN.
- ITEM 610 CONCRETE SHALL BE USED.
- ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
- THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED ITEMS.
- 0.049 CU. YD. CONCRETE PER MARKER.
- A MARKER CONFORMING TO THIS DETAIL MARKED "SPLICE" SHALL BE INSTALLED AT ALL SPLICE LOCATIONS NOT IN LIGHT CANS OR MANHOLES.



TURF CABLE / SPLICE MARKER DETAILS

N.T.S.

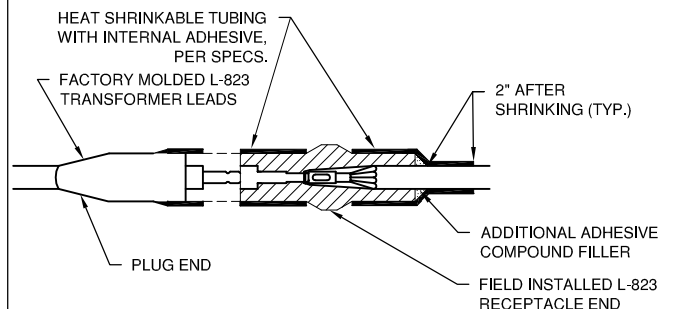
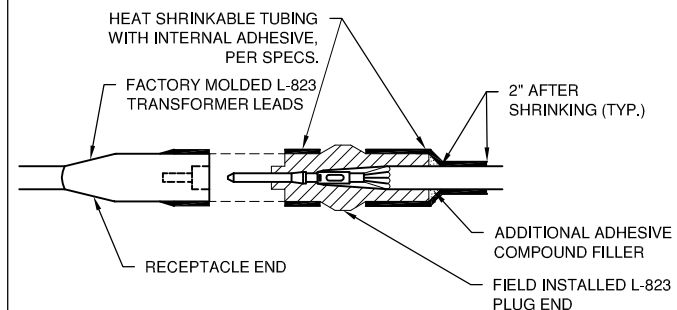


TRENCH DETAIL

N.T.S.

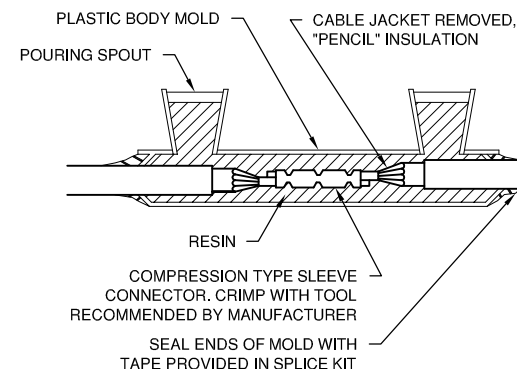
NOTES:

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO ITEM 108.



FOR SPLICES AT ISOLATION TRANSFORMERS.

TYPE C AND D



FOR IN-LINE CONNECTIONS OF EXISTING CABLES CUT DURING CONSTRUCTIONS.

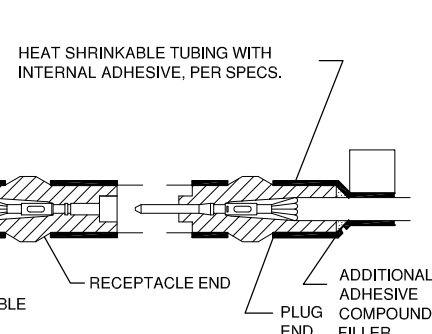
TYPE A

CABLE SPLICES

N.T.S.

NOTES

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.
- CONNECTION OF CONDUCTORS MUST BE MADE BY USING CRIMP CONNECTORS AND A CRIMPING TOOL APPROVED BY THE CONNECTOR/LUG MANUFACTURER. THE TOOL MUST PRODUCE A COMPLETE CRIMP BEFORE IT CAN BE REMOVED. THE CRIMPING TOOL USED MUST BE LISTED BY THE L-823 KIT MANUFACTURER. MAKE THE NUMBER AND TYPE OF CRIMPS PER THE KIT MANUFACTURER'S INSTRUCTIONS.



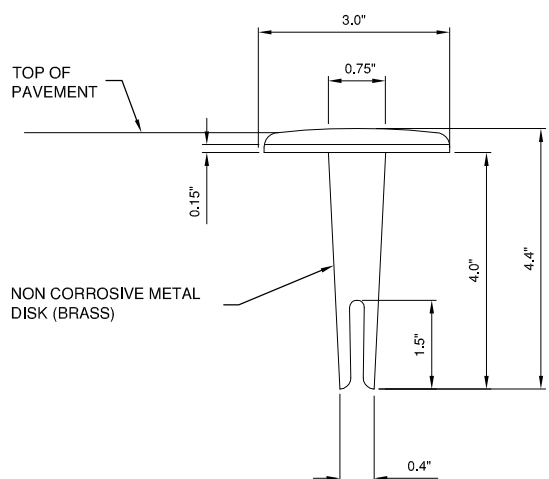
NOT TO BE USED IN THIS PROJECT UNLESS OTHERWISE DIRECTED BY ENGINEER

TYPE B

**ELEC.
4-4"**

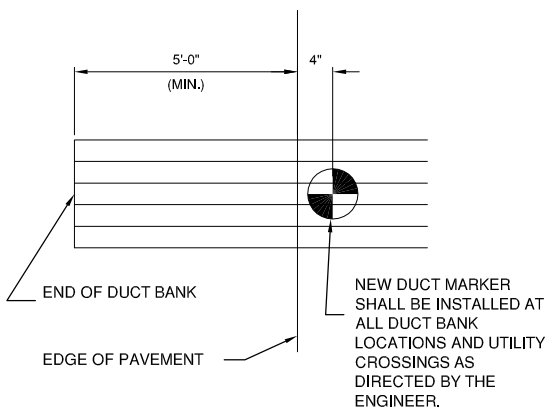
TYPE, NUMBER OF DUCTS AND DUCT SIZE STAMPED ON MARKER

PLAN



SECTION

NEW DUCT MARKERS SHALL BE INSTALLED AT BOTH EDGES OF PAVEMENT AT ALL NEW DIRECTIONAL BORE, SPLIT DUCT, AND CONCRETE ENCASED DUCT LOCATIONS. AR110105-DUCT MARKER-IN PAVEMENT



DUCT MARKERS SHALL BE RECESSED AND GROUTED INTO THE PAVEMENTS.

MARKER PLACEMENT

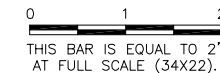
DUCT MARKER DETAILS

N.T.S.

SP023

REVISIONS

NUMBER	BY	DATE



SPARTA COMMUNITY AIRPORT
HUNTER FIELD
SPARTA, ILLINOIS

TAXIWAY EXTENSION

ELECTRICAL DETAILS 1

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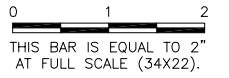


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JOB No:	1241201

IL PROJ. NO. SAR-4224
SBG NO. 3-17-0094-B11

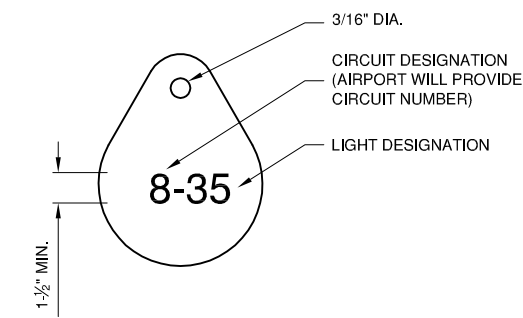
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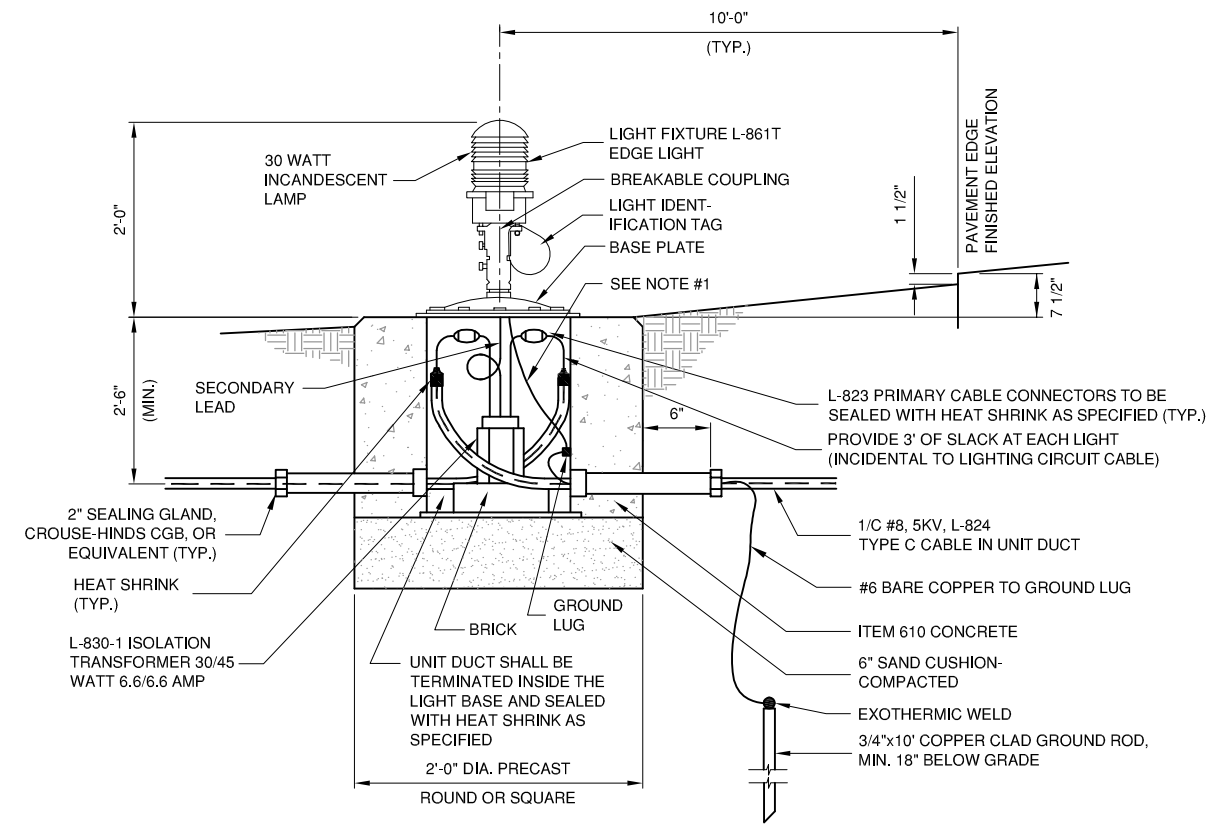
NOTES

1. INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH SET SCREW.
2. LEGENDS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. CONTRACTOR TO COORDINATE LEGEND WITH AIRPORT.
3. THE CONTRACTOR SHALL NUMBER THE EXISTING/ PROPOSED LIGHTS AND SIGNS IN EACH CIRCUIT STARTING AT THE HOMERUN CONTINUING AROUND THE ENTIRE CIRCUIT BACK TO THE HOMERUN.
4. AIRFIELD SIGNS SHALL BE TAGGED & NUMBERED.



LIGHT IDENTIFICATION DETAIL

N.T.S.

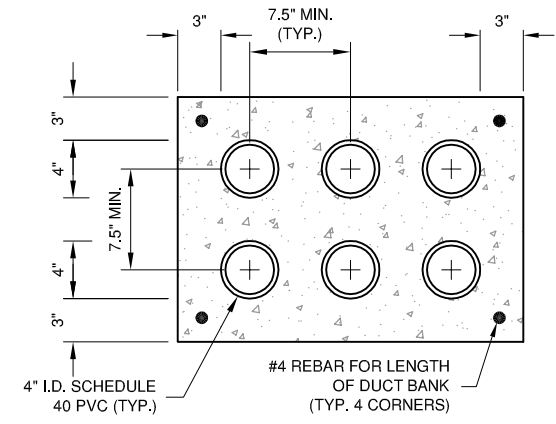


BASE MOUNTED EDGE LIGHT NOTES

1. THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS WITH GREEN XHHW INSULATION. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE.

BASE MOUNTED EDGE LIGHTS

N.T.S.



DUCT BANK NOTES

1. DIMENSIONS SHOWN ARE MINIMUM.
2. TOP OF CONCRETE ENCASEMENT TO BE NOT LESS THAN 18" BELOW FINISHED SUBGRADE.
3. DUCT CONCRETE SHALL BE ITEM 610 STRUCTURAL P.C.C. CONCRETE.
4. ALL DUCT SHALL BE 4" INSIDE DIAMETER.
5. LOCATIONS SHOWN ARE APPROXIMATE. DUCT BANKS SHALL BE INSTALLED AT LOCATIONS DESIGNATED BY THE ENGINEER.
6. CONTRACTOR SHALL INSTALL DUCT BANKS AT A DEPTH WHICH WILL NOT CONFLICT WITH ELEVATION SENSITIVE UTILITIES.
7. A PULL WIRE SHALL BE PROVIDED IN EACH UNUSED CONDUIT. UNUSED DUCT SHALL BE SEALED WITH PVC PLUGS/CAPS TO THE SATISFACTION OF THE ENGINEER.
8. DUCT BANKS SHALL EXTEND A MINIMUM OF 5' BEYOND THE NEW EDGE OF PAVEMENT.

DUCT BANK DETAIL

N.T.S.

**SPARTA COMMUNITY AIRPORT
 HUNTER FIELD
 SPARTA, ILLINOIS**

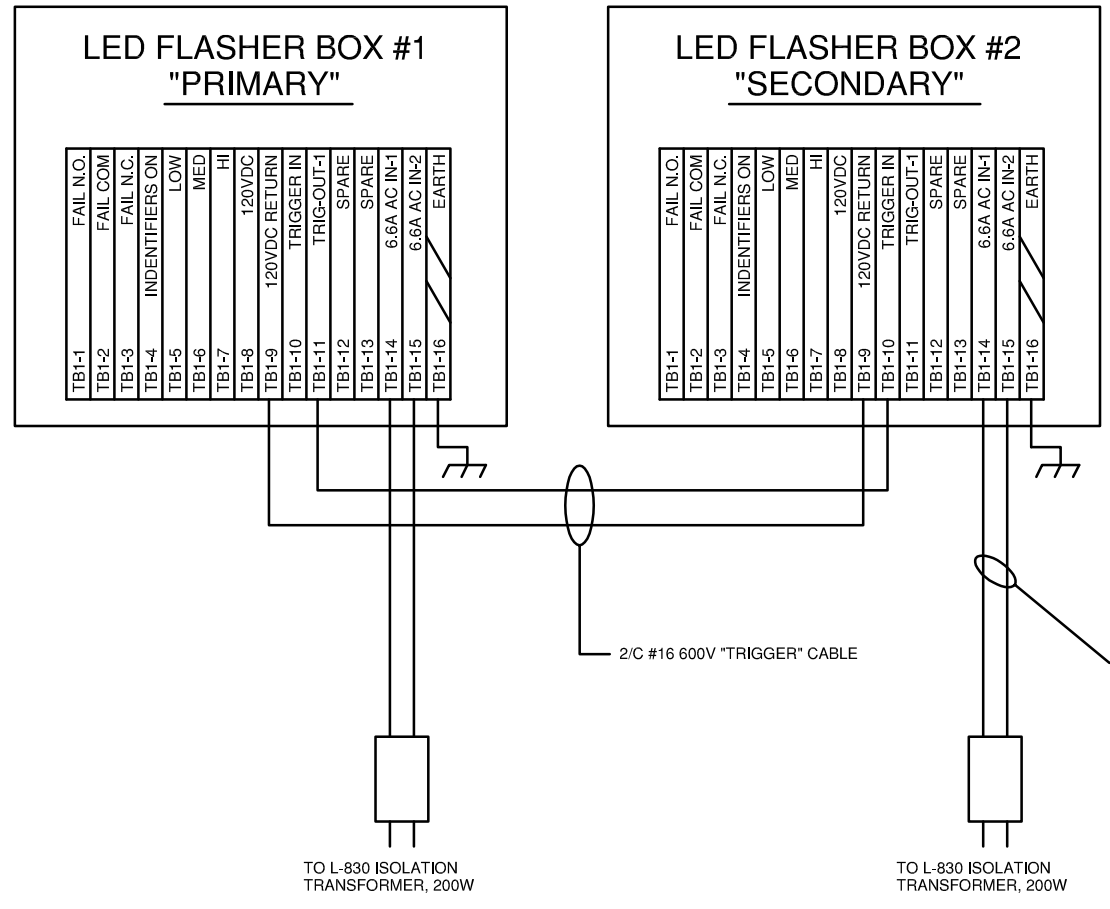
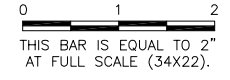
**TAXIWAY EXTENSION
 ELECTRICAL DETAILS 2**

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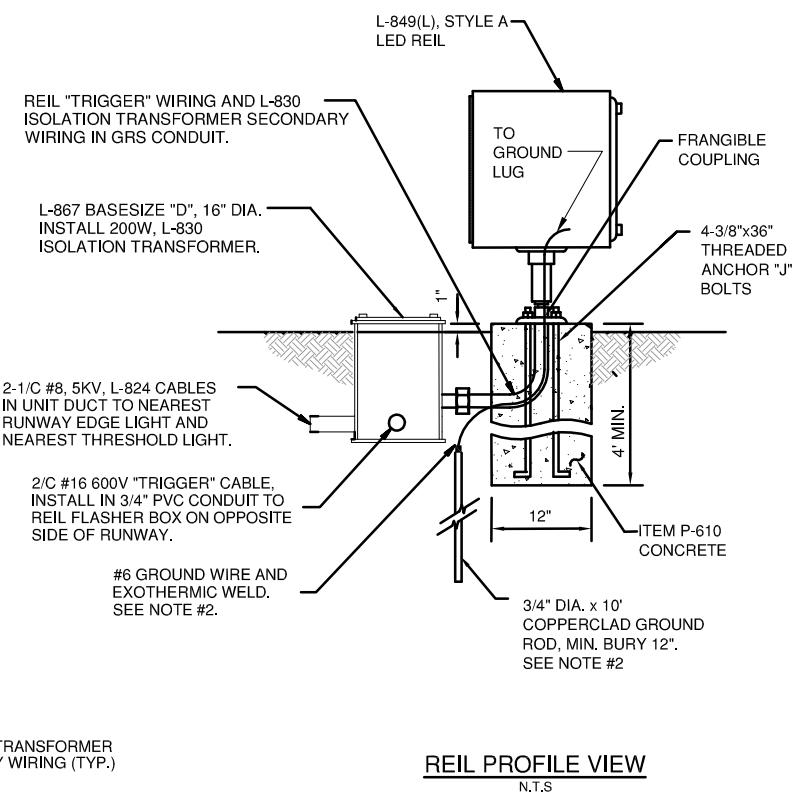
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JOB No:	1241201
IL PROJ. NO.	SAR-4224
SBG NO.	3-17-0094-B11
SHEET	15 OF 28 SHEETS

SP023

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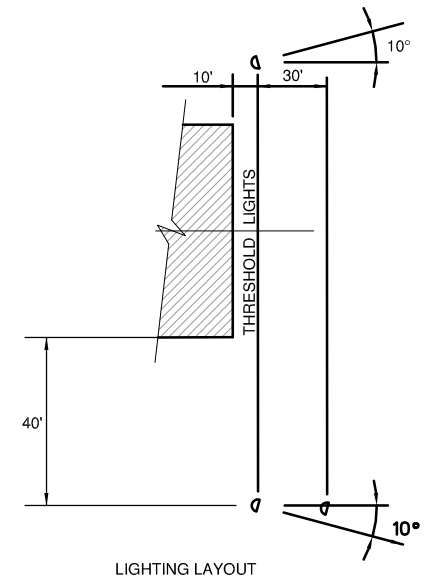


REIL EXTERNAL WIRING DIAGRAM
N.T.S.



REIL PROFILE VIEW
N.T.S.

- NOTES:**
- LOCATE LIGHTS 40' AHEAD OF RUNWAY THRESHOLD
 - LIGHTS ARE ELEVATED 3" ABOVE HORIZONTAL.



REIL PLAN VIEW
N.T.S.

REIL WIRING NOTES

- REIL WIRING SHOWN IS BASED ON ONE MANUFACTURER. REQUIRED REIL WIRING FROM ANOTHER REIL MANUFACTURER MAY VARY FROM WIRING SHOWN. CONTRACTOR SHALL OBTAIN REIL WIRING DIAGRAM FROM MANUFACTURER OF REIL TO BE INSTALLED AND VERIFY REIL WIRING PRIOR TO INSTALLATION OF WIRING. INCLUDE WIRING DIAGRAM WITH REIL SUBMITTAL.
- INSTALL A GROUND RING AROUND EACH REIL LIGHT BOX INSTALLATION, USING #6 BARE COPPER GROUND WIRE ATTACHED TO A MINIMUM OF TWO GROUND RODS, INCLUDING GROUND ROD SHOWN.

REIL DETAILS
N.T.S.

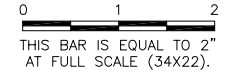
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SPARTA, ILLINOIS
TAXIWAY EXTENSION
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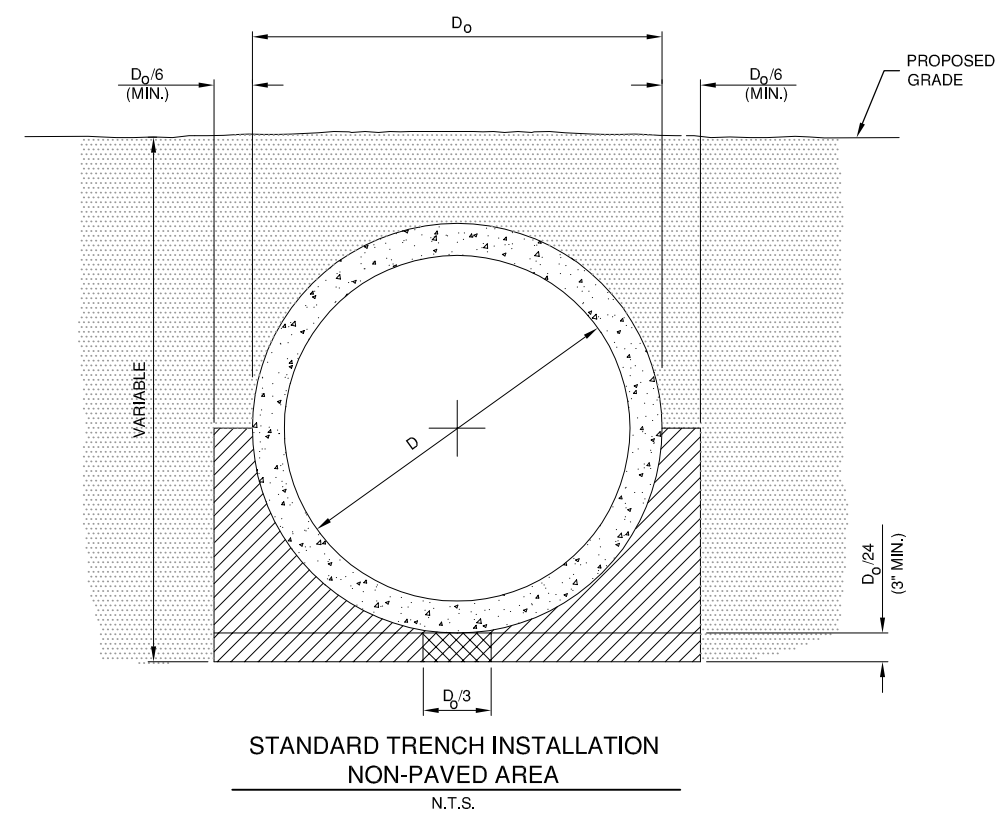


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 SPARTA, ILLINOIS**

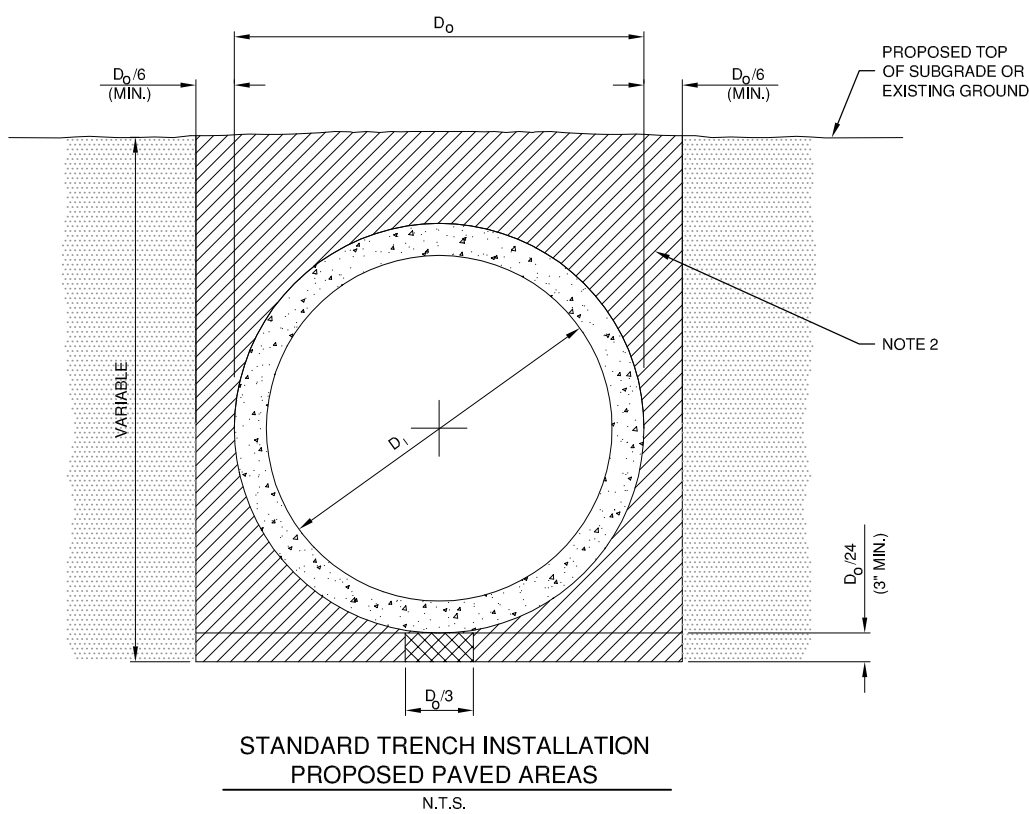
**TAXIWAY EXTENSION
 DRAINAGE DETAILS**

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**STANDARD TRENCH INSTALLATION
 NON-PAVED AREA**
 N.T.S.



**STANDARD TRENCH INSTALLATION
 PROPOSED PAVED AREAS**
 N.T.S.

LEGEND

- DRAINAGE CONDUIT MATERIAL-CONCRETE
- MIDDLE BEDDING LOOSELY PLACED UNCOMPACTED BEDDING
- HAUNCH AND OUTER BEDDING COMPACTION- 95% STANDARD PROCTOR
- LOWER SIDE AND OVERFILL COMPACTION- SAME AS EMBANKMENT REQUIREMENTS

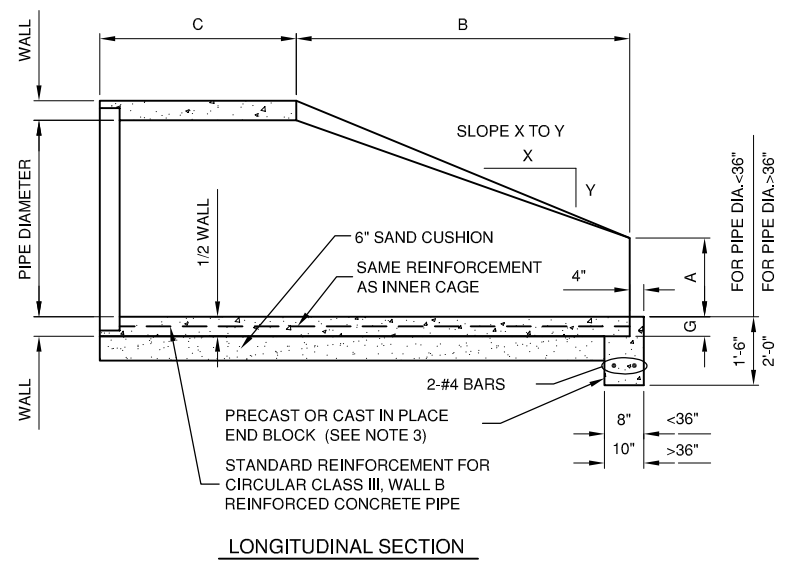
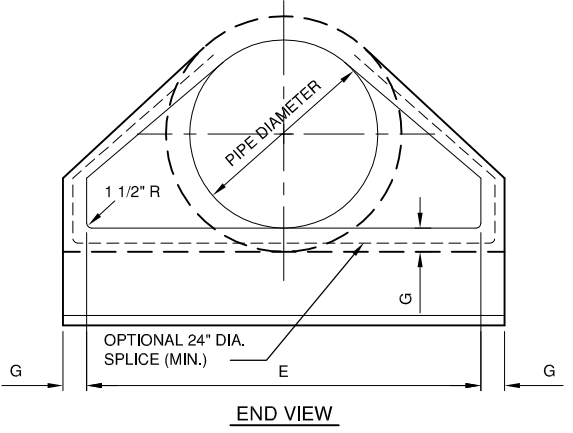
D_o PIPE OUTSIDE DIAMETER
 D_i PIPE INSIDE DIAMETER

- NOTES**
- BEDDING SHOWN IS IN ACCORDANCE WITH "STANDARD EMBANKMENT INSTALLATIONS", STANDARD INSTALLATION & BEDDING FACTORS FOR THE INDIRECT DESIGN METHOD (DESIGN DATA 40), AMERICAN CONCRETE PIPE ASSOCIATION.
 - BACKFILL TO EXTEND 3' BEYOND EDGES OF PROPOSED PAVEMENT.

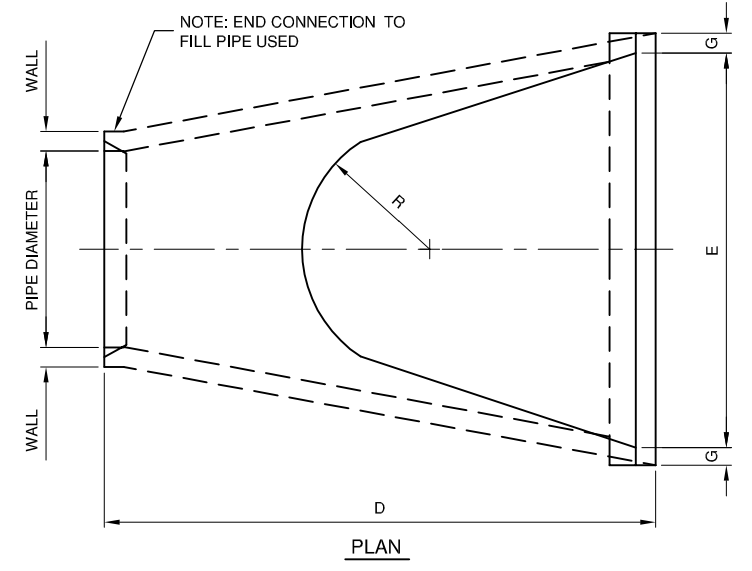
DIMENSIONS - TABLE 1

PIPE DIA.	APPROX WT. (lbs.)	WALL	A	B	C	D	E	G	R	SLOPE
12"	530	2"	4"	2'-0"	4'-0 7/8"	6'-0 7/8"	2'-0"	2"	9"	3:1
15"	740	2 1/4"	6"	2'-3"	3'-10"	6'-1"	2'-6"	2 1/4"	11"	3:1
18"	990	2 1/2"	9"	2'-3"	3'-10"	6'-1"	3'-0"	2 1/2"	12"	3:1
21"	1280	2 3/4"	9"	2'-11"	3'-2"	6'-1"	3'-6"	2 3/4"	13"	3:1
24"	1520	3"	9 1/2"	3'-7 1/2"	2'-6"	6'-1 1/2"	4'-0"	3"	14"	3:1
27"	1930	3 1/4"	10 1/2"	4'-0"	2'-1 1/2"	6'-1 1/2"	4'-6"	3 1/4"	14 1/2"	3:1
30"	2190	3 1/2"	1'-0"	4'-6"	1'-7 3/4"	6'-1 3/4"	5'-0"	3 1/2"	15"	3:1
33"	3200	3 3/4"	1'-1 1/2"	4'-10 1/2"	3'-3 1/4"	8'-1 3/4"	5'-6"	3 3/4"	17 1/2"	3:1
36"	4100	4"	1'-3"	5'-3"	2'-10 3/4"	8'-1 3/4"	6'-0"	4"	20"	3:1
42"	5380	4 1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	4 1/2"	22"	3:1
48"	6550	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	5"	22"	3:1
54"	8240	5 1/2"	2'-3"	5'-5"	2'-11"	8'-4"	7'-6"	5 1/2"	24"	2.4:1
60"	8730	6"	2'-11"	5'-0"	3'-3"	8'-3"	8'-0"	5"	*	2:1
66"	10710	6 1/2"	2'-6"	6'-0"	2'-3"	8'-3"	8'-6"	5 1/2"	*	2:1
72"	12520	7"	3'-0"	6'-6"	1'-9"	8'-3"	9'-0"	6"	*	1.86:1
78"	14770	7 1/2"	3'-0"	7'-6"	1'-9"	9'-3"	9'-6"	6 1/2"	*	1.82:1
84"	18160	8"	3'-0"	7'-6 1/2"	1'-9"	9'-3 1/2"	10'-0"	6 1/2"	*	1.5:1

* RADIUS AS FURNISHED BY MANUFACTURER.



PRECAST REINFORCED CONCRETE FLARED END SECTION
 (I.D.O.T. STD. NO. 542301)
 N.T.S.



- NOTES**
- PRECAST CONCRETE FLARED END SECTIONS SHALL CONFORM TO THE APPLICABLE REQUIREMENTS OF AASHTO M-170 CLASS III, WALL B REINFORCED CONCRETE PIPE.
 - PRECAST CONCRETE FLARED END SECTION FOR PIPE DIAMETER REQUIRED SHALL BE AS INDICATED ON DETAIL PLAN FOR EACH INDIVIDUAL INSTALLATION.
 - THE END BLOCK SHALL BE PLACED PRIOR TO THE INSTALLATION OF THE FLARED END SECTION. THE END BLOCK SHALL BE BACKFILLED IN ACCORDANCE WITH ITEM 701.

SP023

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NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
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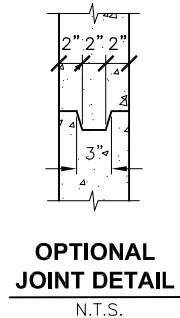
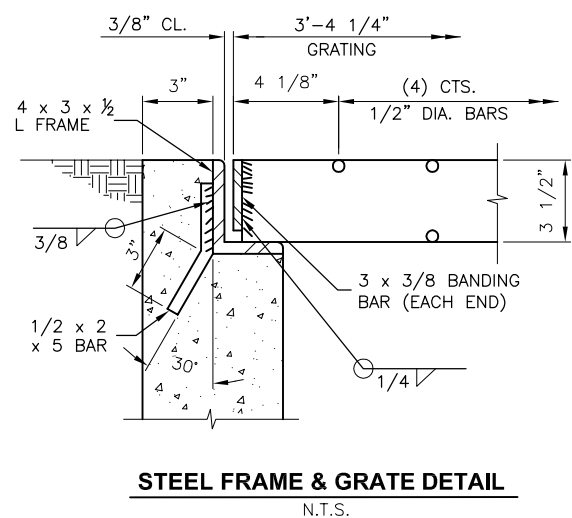
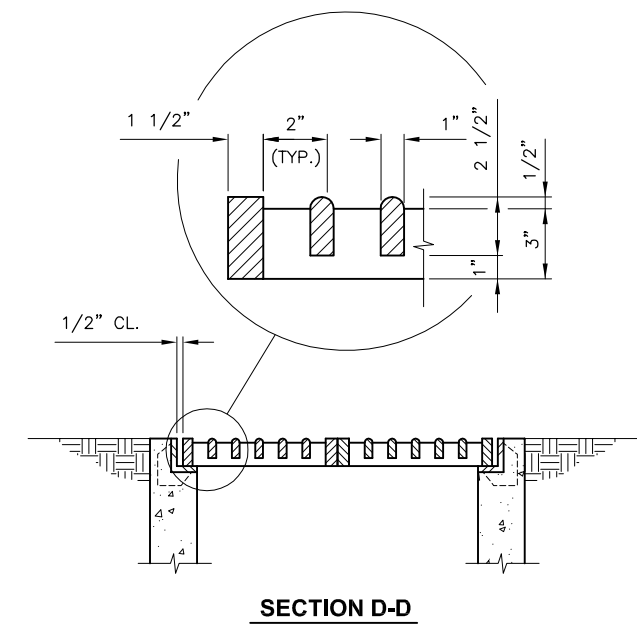
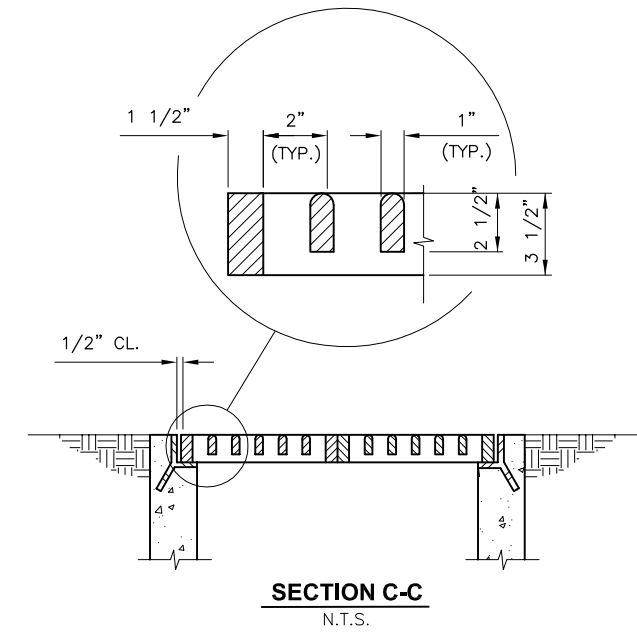
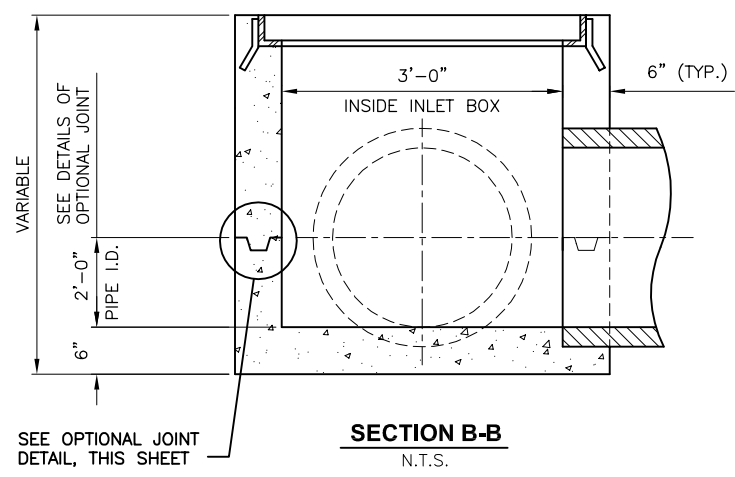
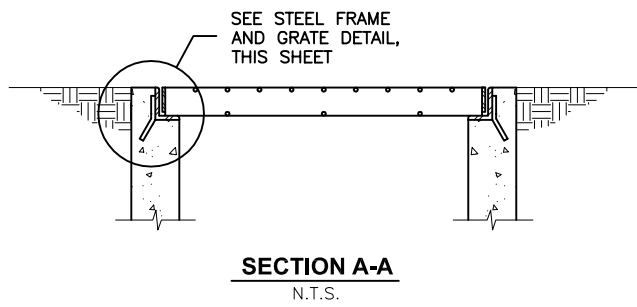
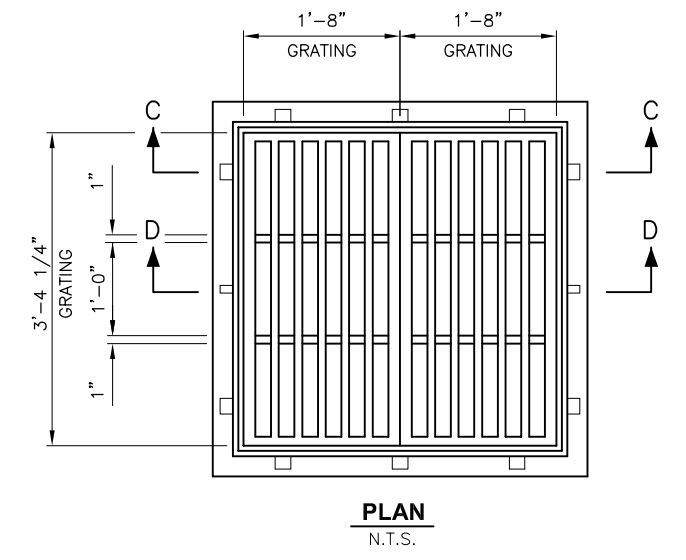
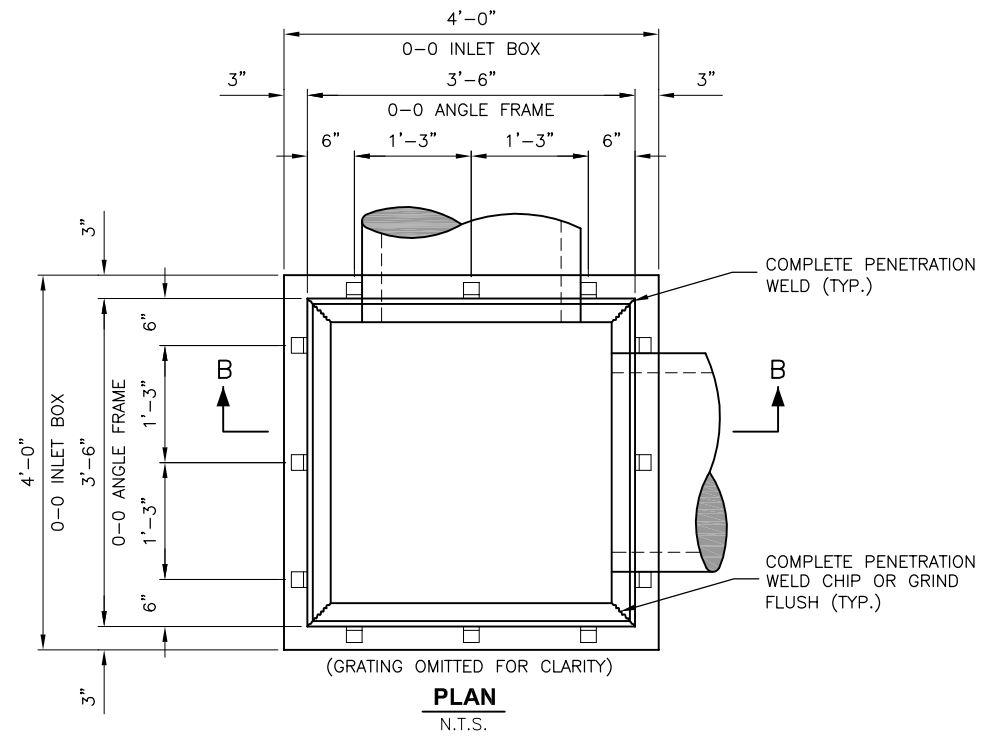
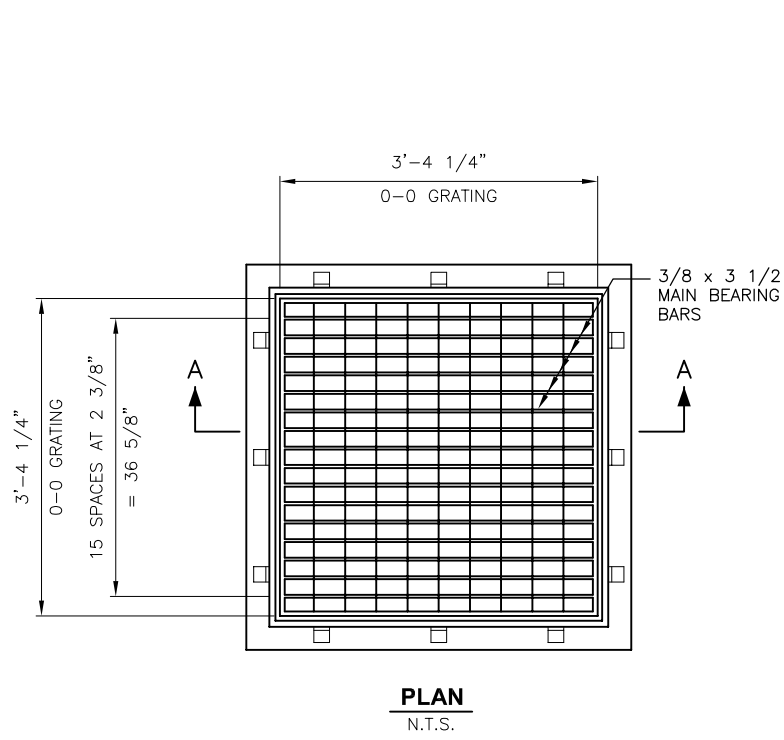
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TAXIWAY EXTENSION
 INLET DETAILS

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INLET NOTE
 INLET IS AN IDOT STD. 542546,
 FLUSH INLET BOX FOR MEDIAN,
 FRAME & GRATES ARE NEENAH
 R-3807 OR APPROVED EQUAL.

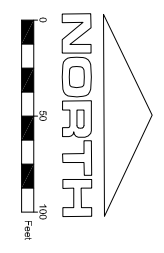
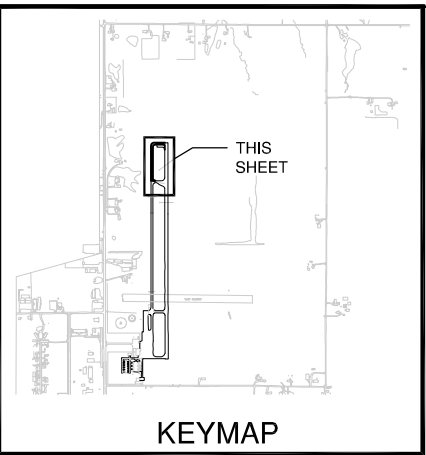
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SAR-V-ALAE
 SAR-C-SPAL
 SAR-V-AL
 SAR-C-MP
 KEYMAP

SP023

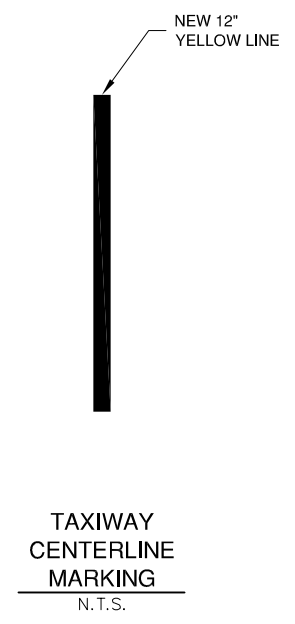
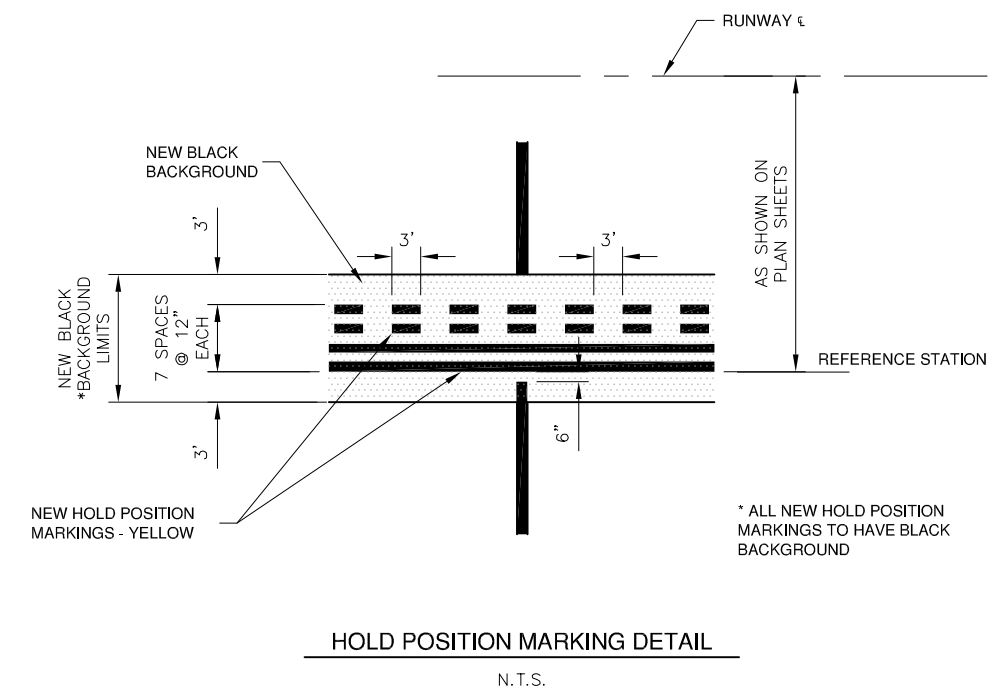
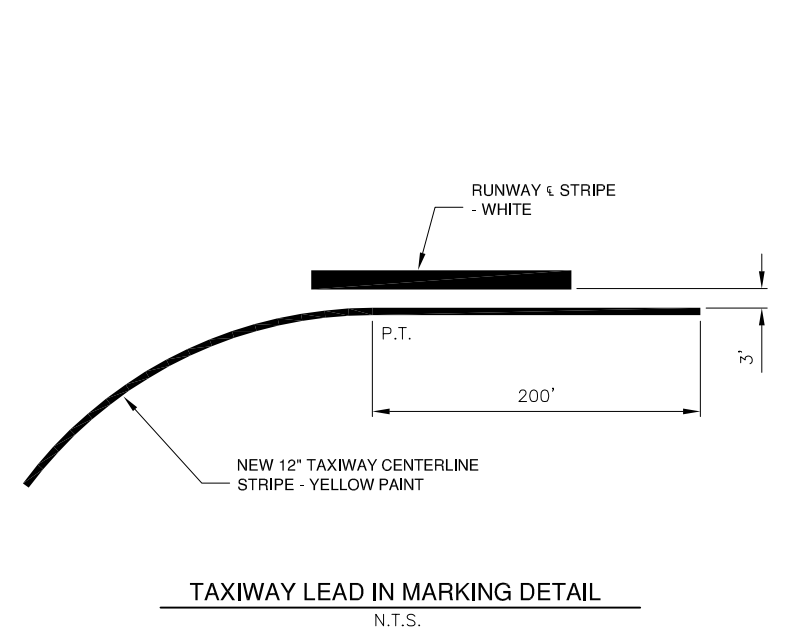
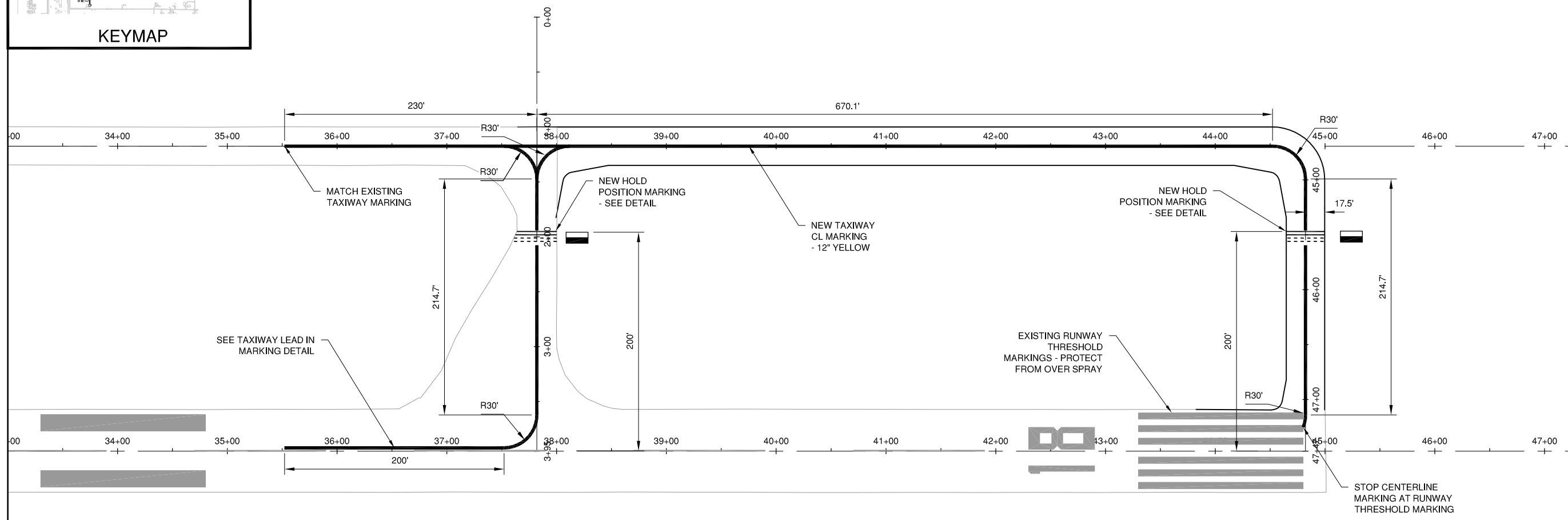
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0 1 2
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LEGEND

	EXISTING MARKING
	NEW MARKING
	PAVEMENT MARKING REMOVAL



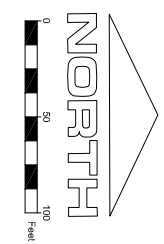
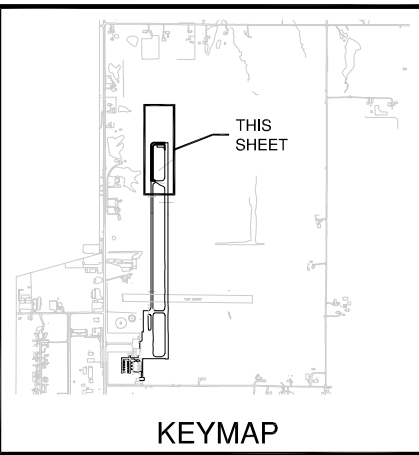
- NOTES:**
1. YELLOW MARKINGS TO BE PAID FOR UNDER ITEM AR620520 PAVEMENT MARKING - WATER BORNE.
 2. BLACK MARKINGS TO BE PAID FOR UNDER ITEM AR620525 - PAVEMENT MARKING BLACK BORDER.

**SPARTA COMMUNITY AIRPORT
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**TAXIWAY EXTENSION
 PAVEMENT MARKING PLAN**

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SHEET 19 OF 28 SHEETS	

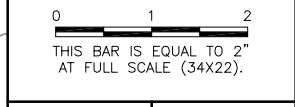
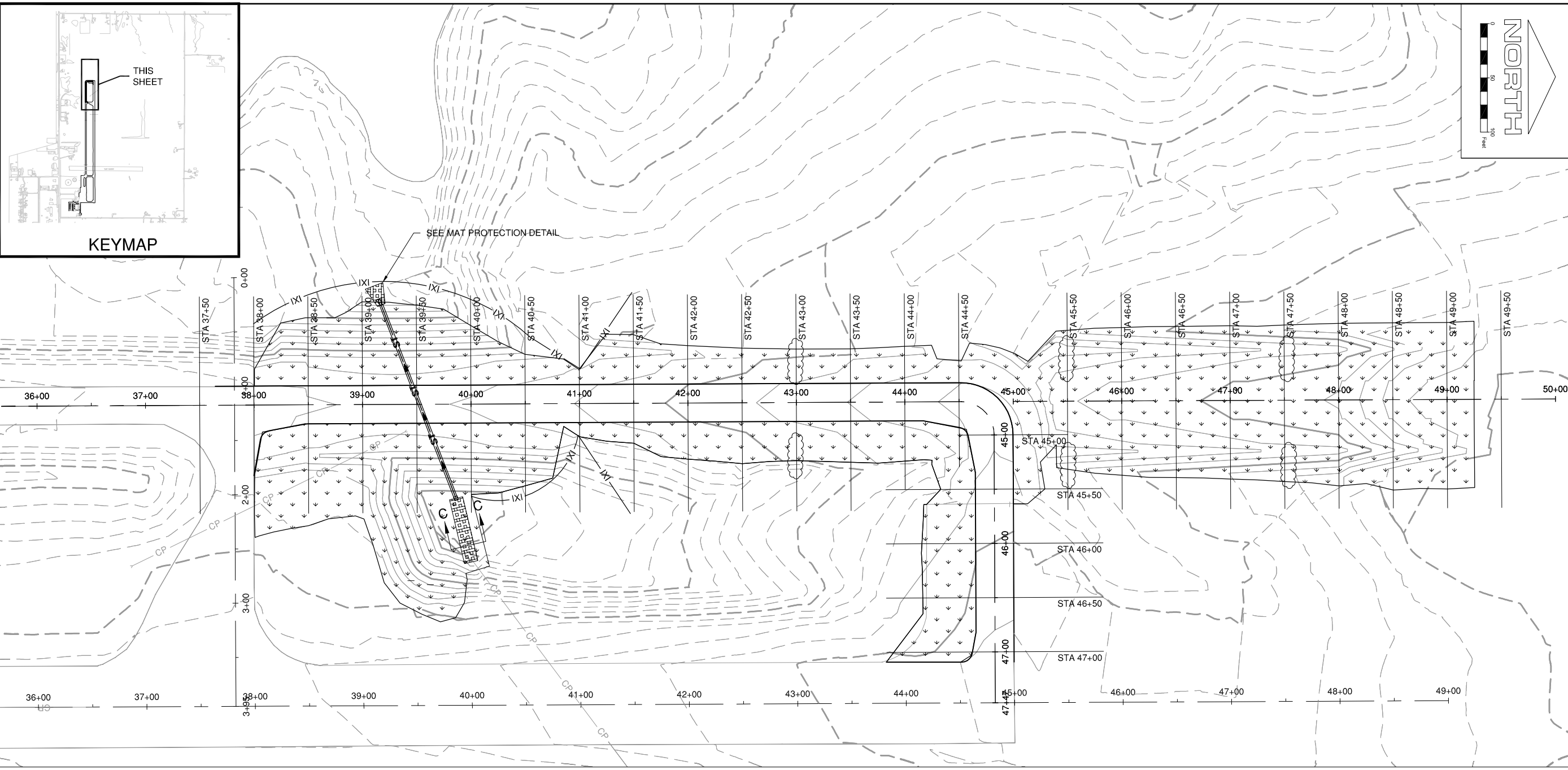


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 UPDATE BY: Chris Groth
 PLOT DATE: 4/26/2013 2:23 PM

SAR-V-LAER
 SAR-C-SPAL
 SAR-V-AL
 KEYMAP
 SAR-CP701

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**SPARTA COMMUNITY AIRPORT
 HUNTER FIELD
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**TAXIWAY EXTENSION
 EROSION CONTROL AND INDEX TO
 CROSS SECTIONS**

- NOTES**
- 1000 SY OF EXCELSIOR BLANKET (AR908520) HAS BEEN ADDED FOR USE AT LOCATIONS DETERMINED BY THE RESIDENT ENGINEER.

LEGEND

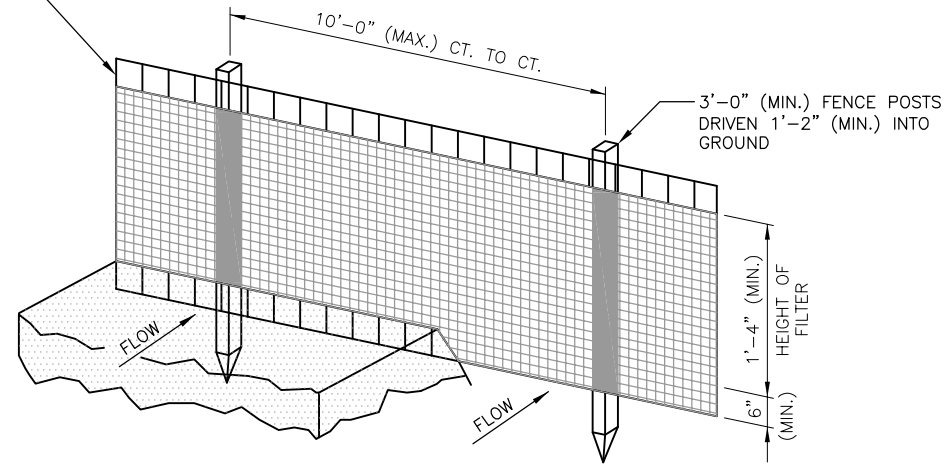
— IXI —	SILT FENCE
	TIED CONCRETE BLOCK MAT
	STRAW WATTLE
	SEEDING AND MULCH

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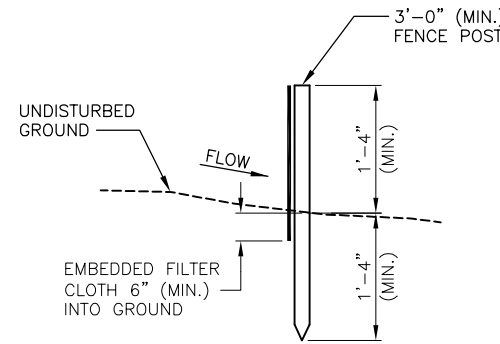
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WOVEN WIRE FENCE (MIN. 14-1/2" GA. MIN., MAX. 6" MESH SPACING)
 POLYPROPYLENE REINFORCING MESH MAY BE USED IF INCORPORATED IN A PREFABRICATED UNIT.



PERSPECTIVE VIEW



SECTION

EROSION CONTROL FABRIC FENCE DETAILS

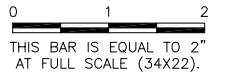
N.T.S.

NOTES

1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
 2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
 3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
 4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.
- POSTS: STEEL EITHER T OR U TYPE OR 2" HARDWOOD.
 FENCE: WOVEN WIRE, 14-1/2" GA. 6" MAX. MESH OPENING
 POLYPROPYLENE MESH, IF INCORPORATED IN PREFAB UNIT.

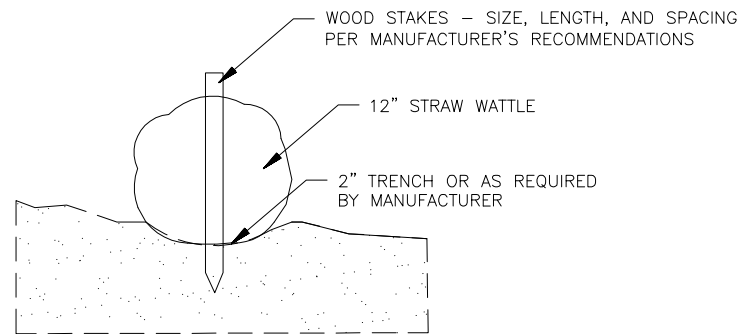
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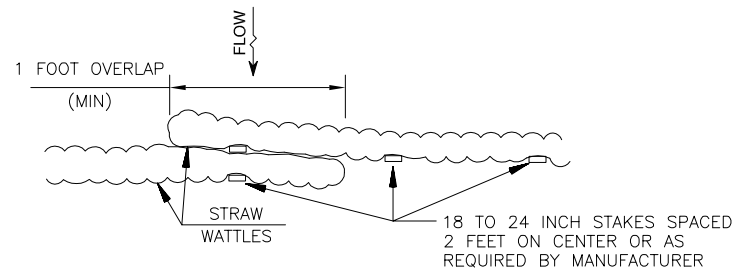
TAXIWAY EXTENSION
 EROSION CONTROL DETAILS 1



SECTION

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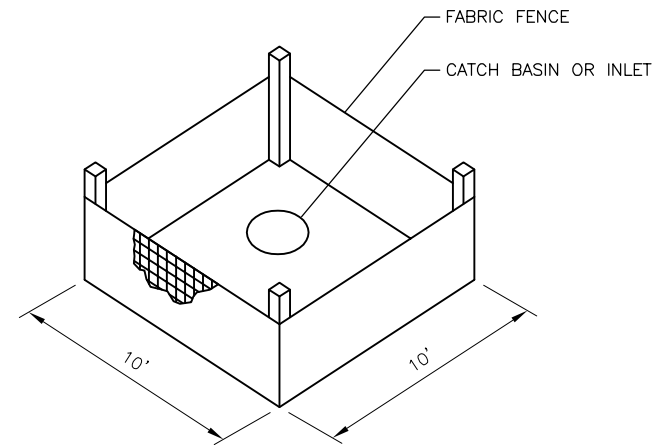
1. NORTH AMERICAN GREEN
2. WESTERN EXCELSIOR
3. APPROVED EQUAL



PLAN

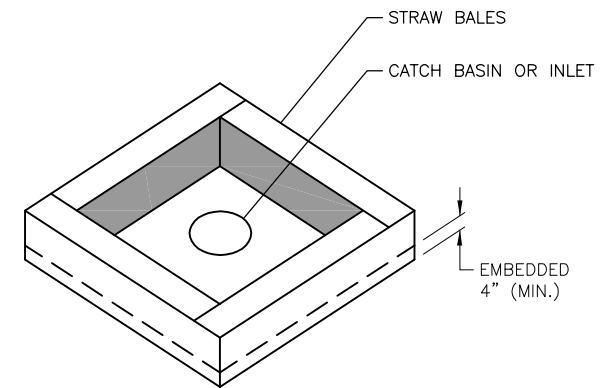
STRAW WATTLES

N.T.S.



INLET PROTECTION WITH FABRIC

N.T.S.



INLET PROTECTION WITH STRAW BALES

N.T.S.

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

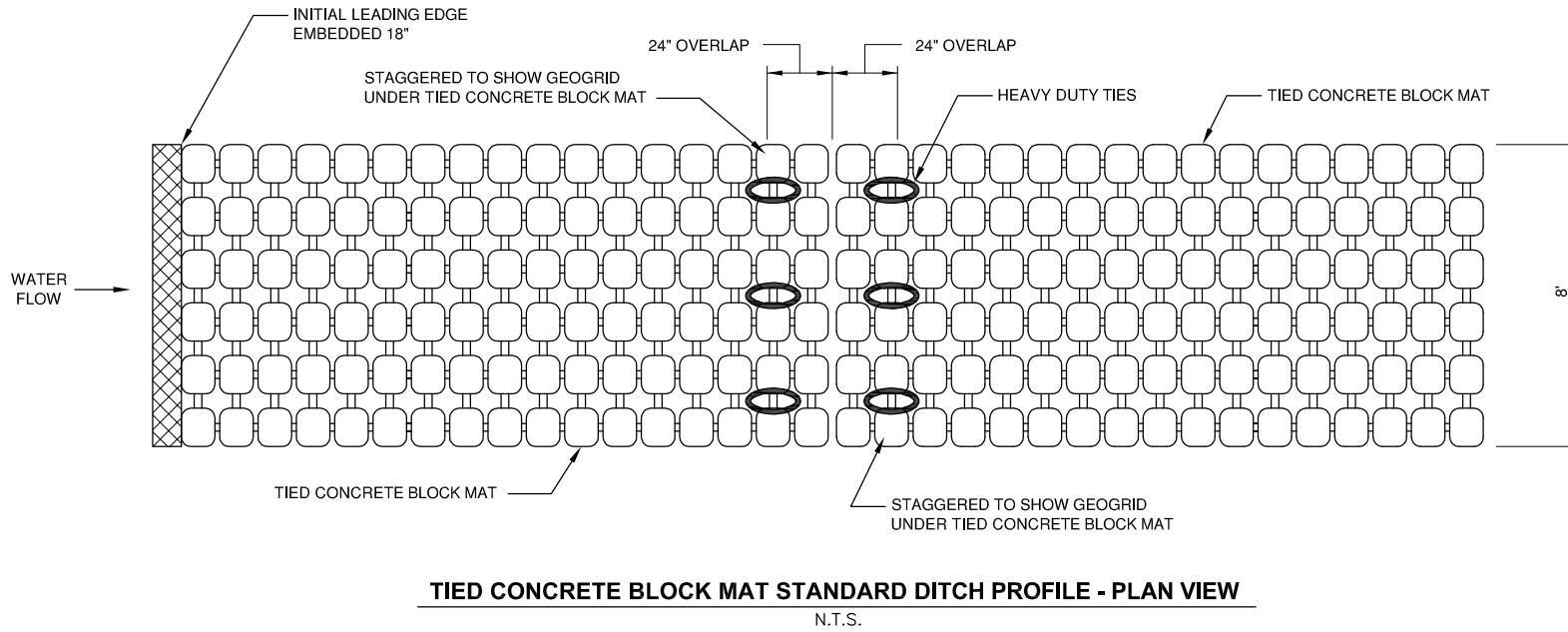
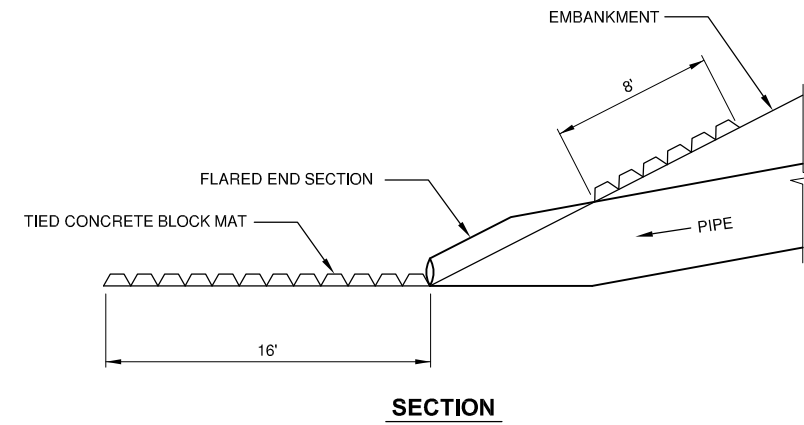
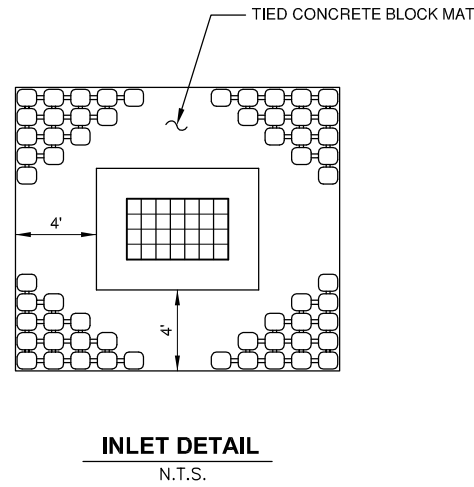
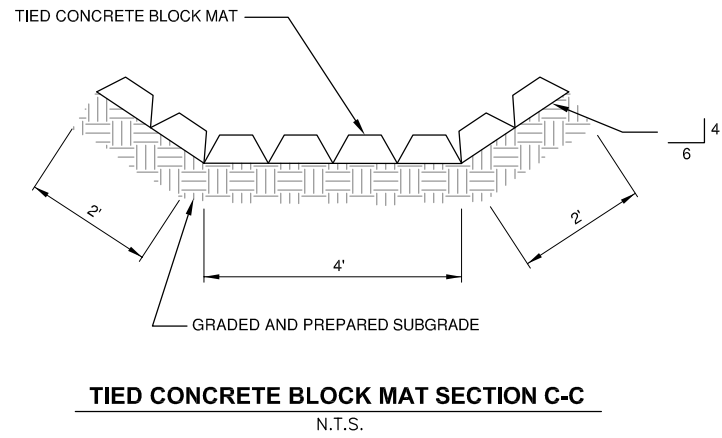
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TAXIWAY EXTENSION
 EROSION CONTROL DETAILS 2

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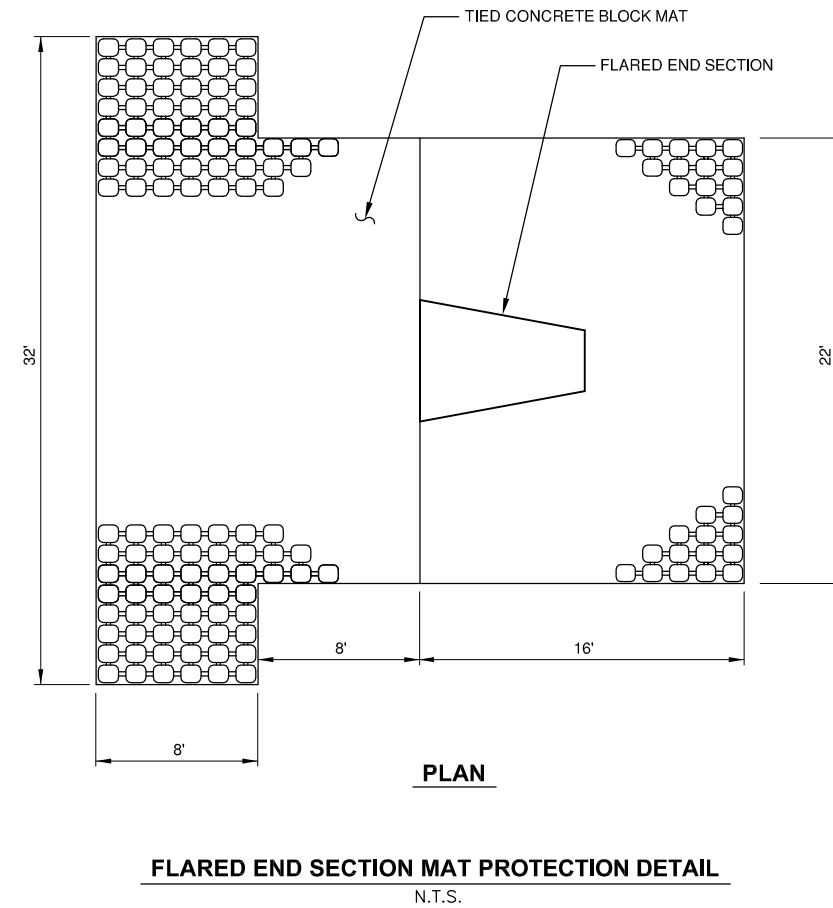
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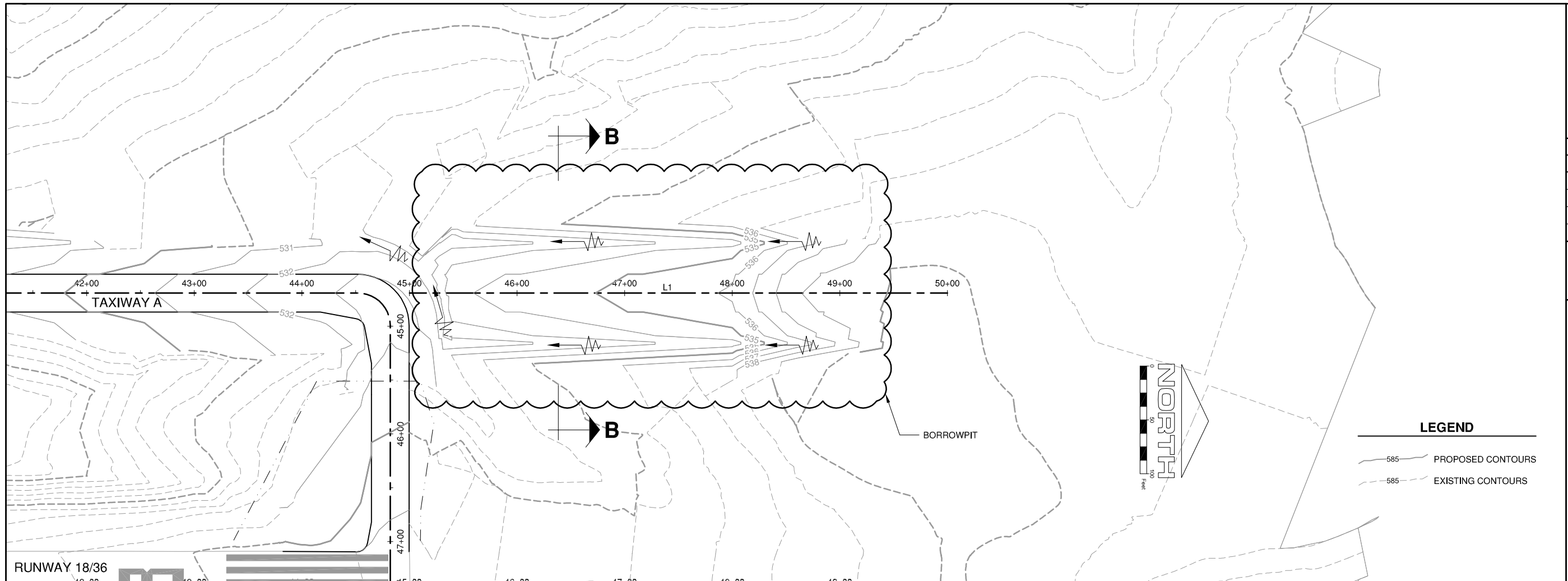
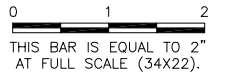
NOTES

1. GRADE A RADIAL CHANNEL SO THAT WATER WILL FLOW DOWN THE CENTER OF THE CHANNEL AND BE CONTAINED TO THE CHANNEL. ALL SUBGRADE SURFACES PREPARED FOR PLACEMENT OF MATS SHALL BE SMOOTH AND FREE OF ALL ROCKS, STICKS, ROOTS, OTHER PROTRUSIONS, OR DEBRIS OF ANY KIND. THE PREPARED SURFACE SHALL PROVIDE A FIRM UNYIELDING FOUNDATION FOR THE MATS WITH NO SHARP OR ABRUPT BREAKS IN THE GRADE.
2. APPLY SEED DIRECTLY TO THE PREPARED SOIL PRIOR TO TIED CONCRETE BLOCK MAT INSTALLATION. USE SEED PER ITEM 901.
3. INSTALL TIED CONCRETE BLOCK MAT ROLLS. SECTIONS OF MAT WITH SEAMS PERPENDICULAR TO THE WATER FLOW ARE CONNECTED VIA A GEOGRID SEAM. LONGITUDINAL JOINTS DO NOT REQUIRE CONNECTING. THE SEAMS CONSIST OF A 4' SECTION OF HIGH STRENGTH GEOGRID THE WIDTH OF THE CHANNEL. 24" OF THE SEAM GOES UNDERNEATH THE UPSTREAM SECTION OF MAT, 24" LAYS UNDER THE DOWNSTREAM SECTION OF MAT. THE SEAM IS THEN TIED TO THE UPSTREAM OF MAT AND DOWNSTREAM SECTION OF MAT USING HEAVY DUTY TIES IN 1' INCREMENTS ACROSS THE SEAM. TIES CONSIST OF 1/4" ZIP TIES.
4. THE INITIAL LEADING EDGE OF TIED CONCRETE BLOCK MAT IS EMBEDDED 18" VERTICALLY INTO THE GROUND TO SERVE AS AN ANCHOR TRENCH.



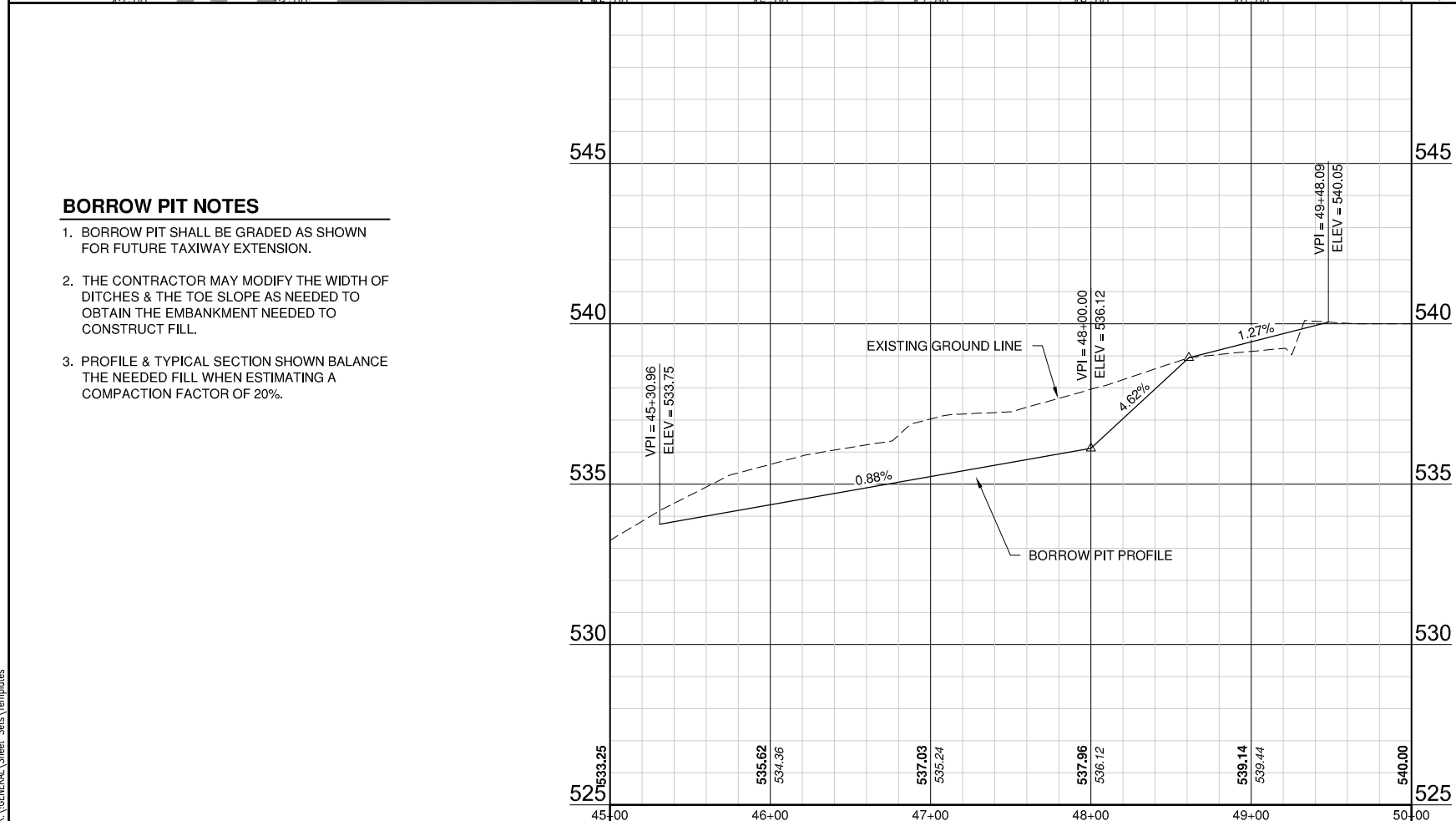
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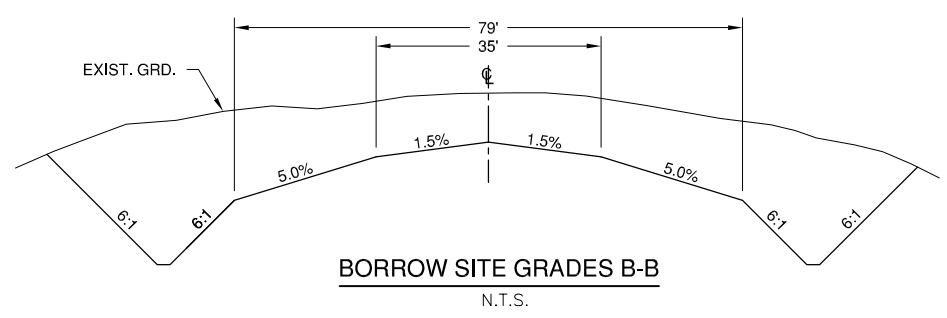


LEGEND

- 585- PROPOSED CONTOURS
- 585- EXISTING CONTOURS



ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L1	BORROW PIT	STA. 45+00.00	N:542389.7084 E:2430649.3933	STA. 50+00.00	N: 542889.7082 E: 2430648.9856



BORROW SITE GRADES B-B
 N.T.S.

BORROW PIT NOTES

1. BORROW PIT SHALL BE GRADED AS SHOWN FOR FUTURE TAXIWAY EXTENSION.
2. THE CONTRACTOR MAY MODIFY THE WIDTH OF DITCHES & THE TOE SLOPE AS NEEDED TO OBTAIN THE EMBANKMENT NEEDED TO CONSTRUCT FILL.
3. PROFILE & TYPICAL SECTION SHOWN BALANCE THE NEEDED FILL WHEN ESTIMATING A COMPACTION FACTOR OF 20%.

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 HUNTER FIELD
 SPARTA, ILLINOIS**
TAXIWAY EXTENSION
BORROW SITE PLAN AND PROFILE

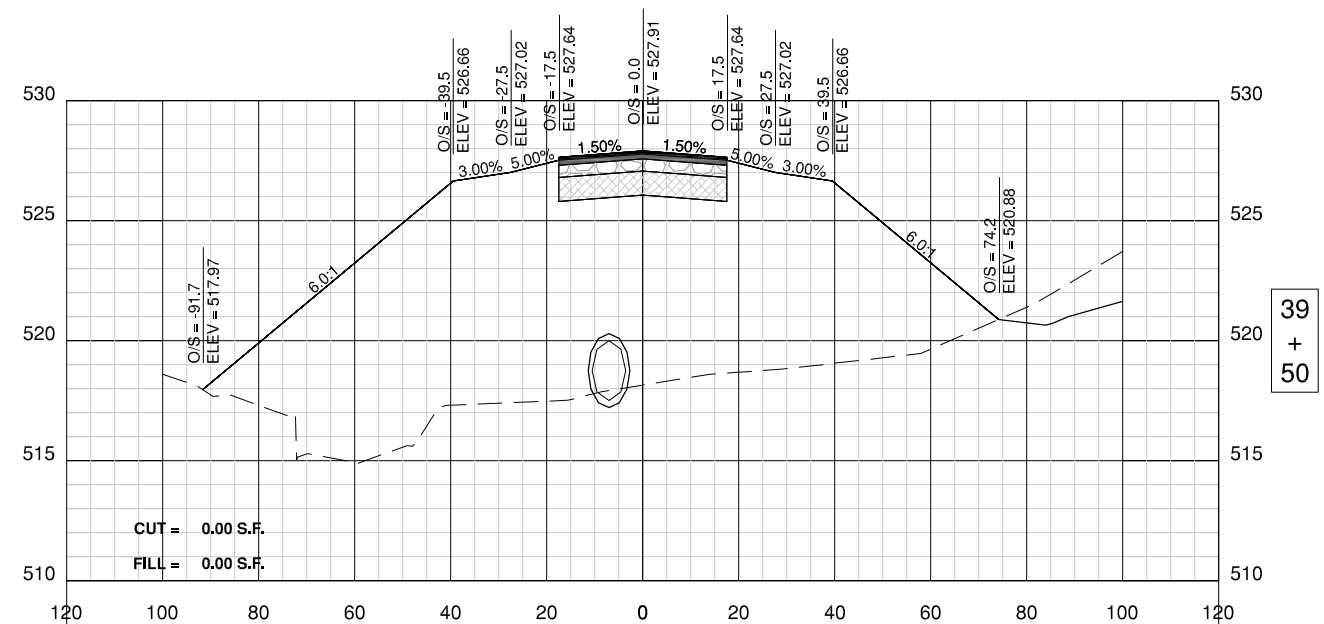
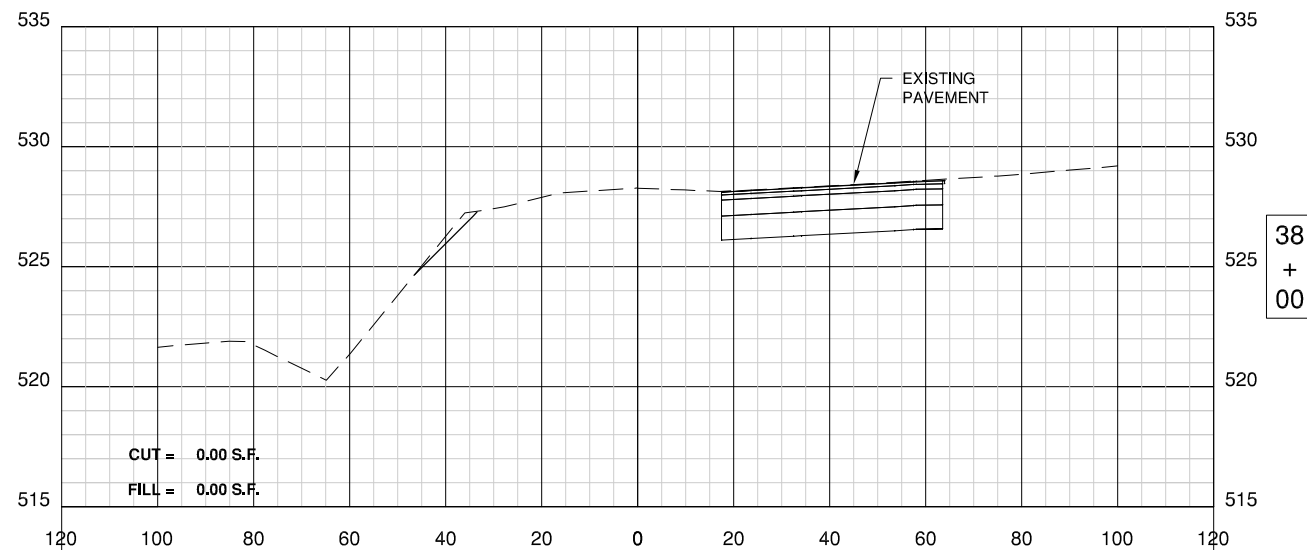
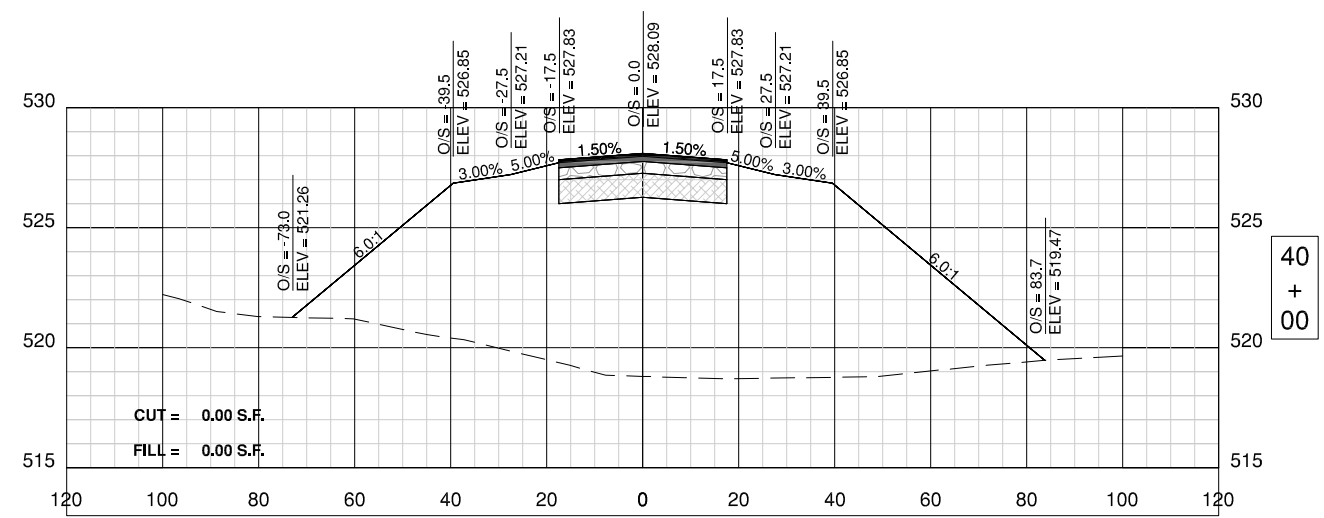
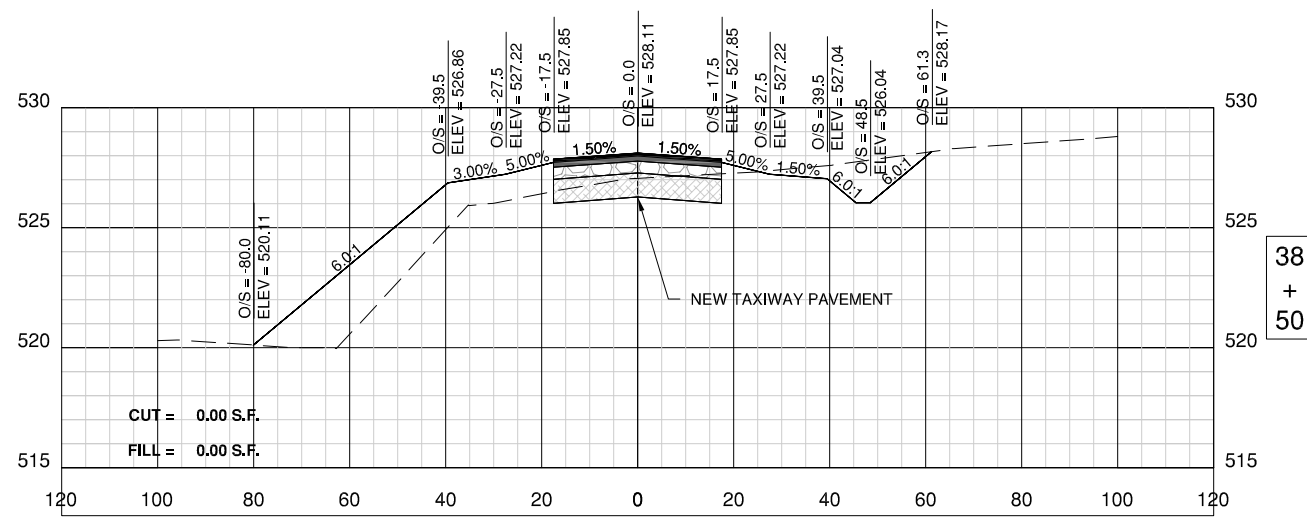
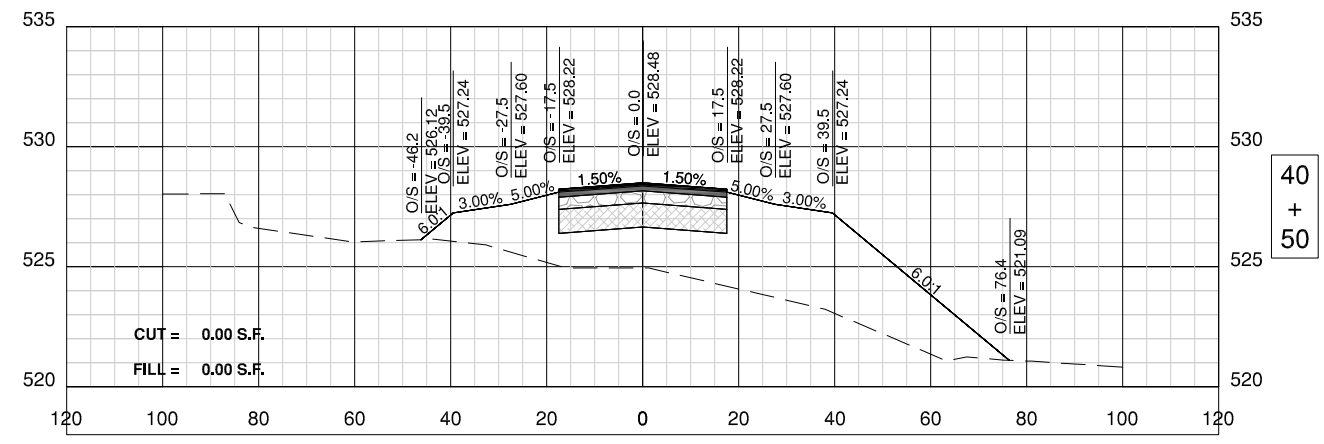
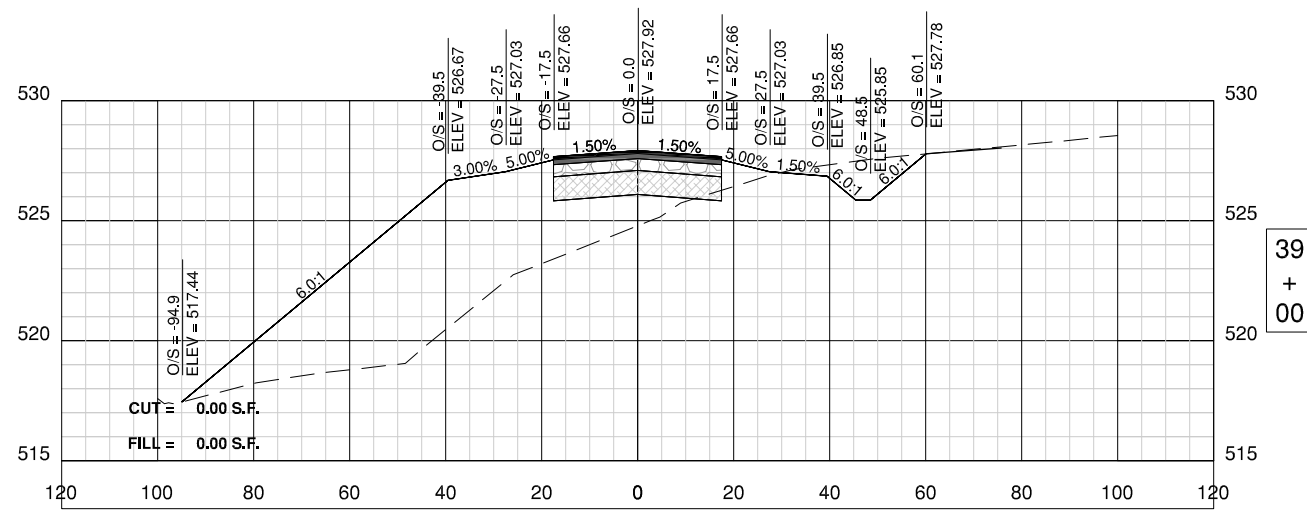
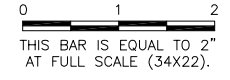
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TAXIWAY EXTENSION

TAXIWAY A CROSS SECTIONS SHEET

1

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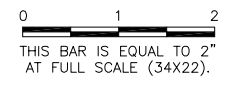
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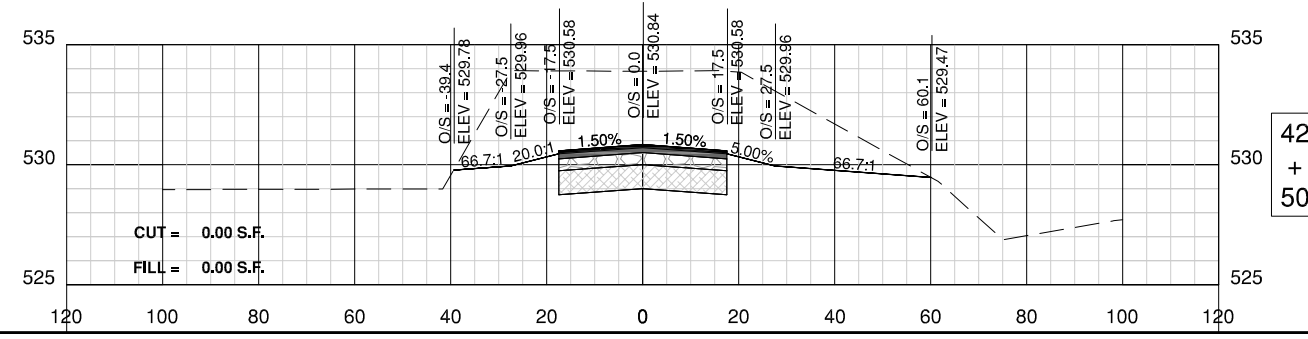
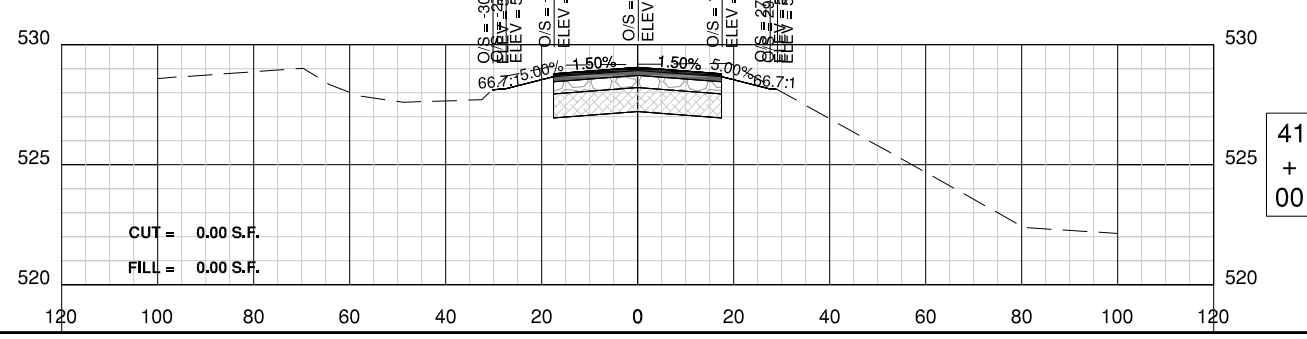
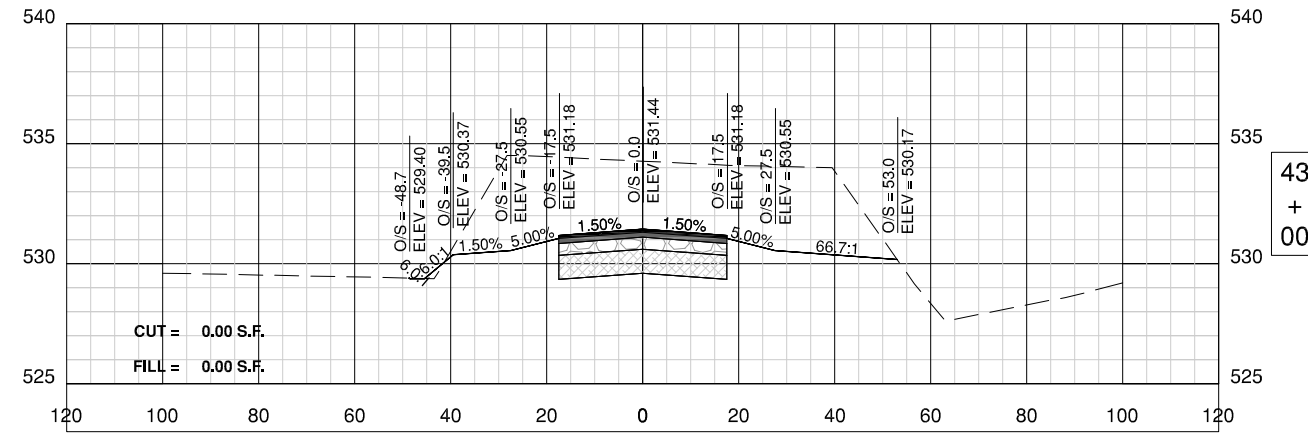
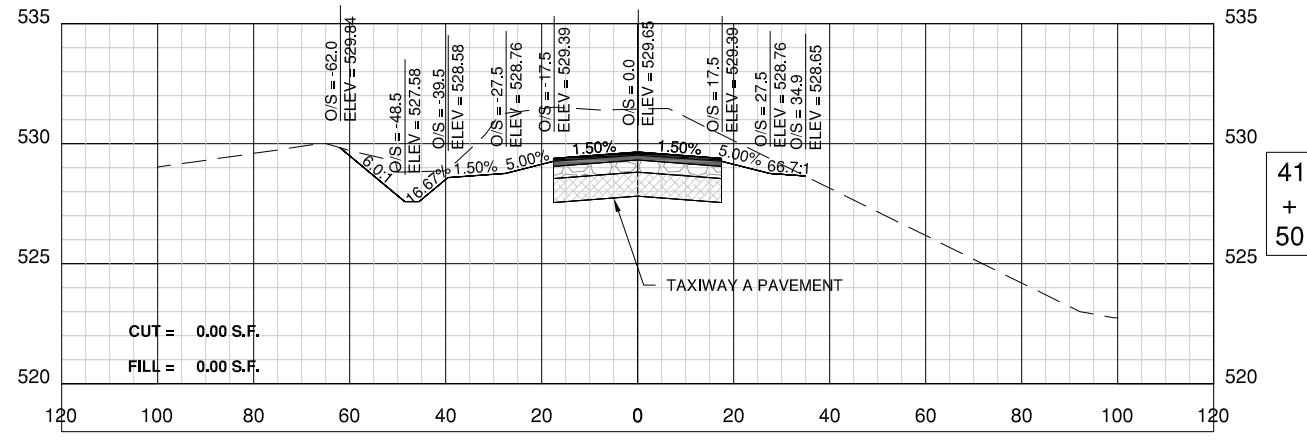
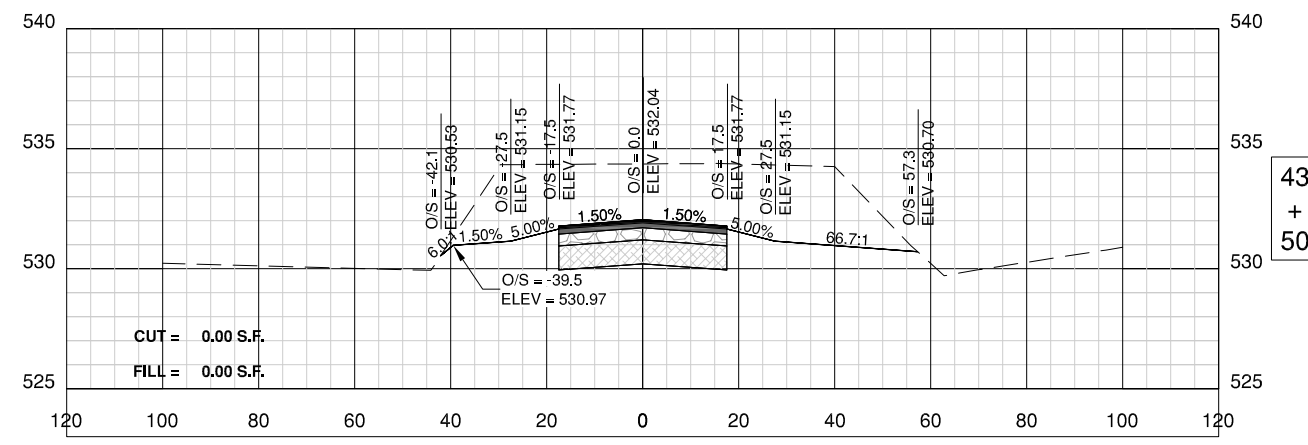
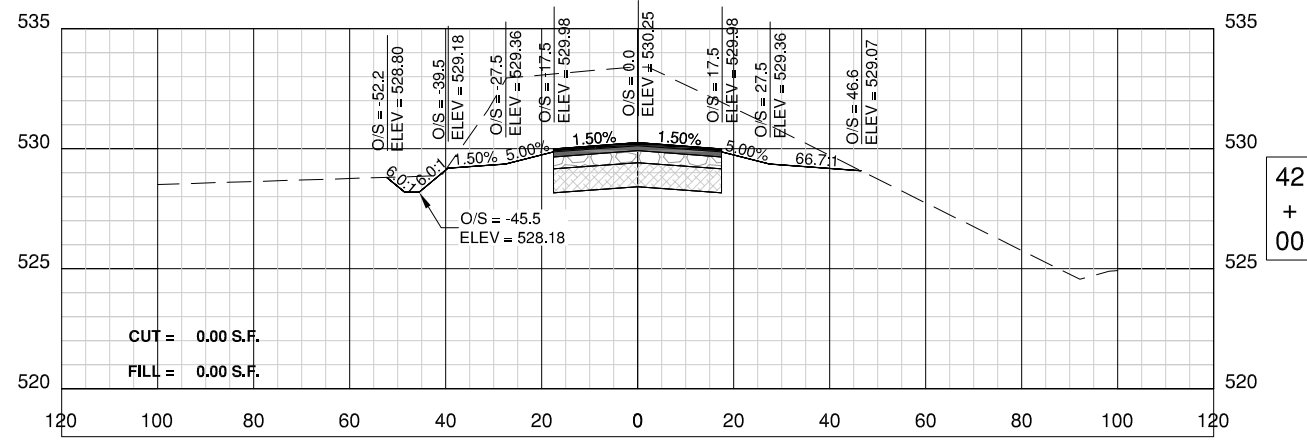


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**TAXIWAY EXTENSION
 2**

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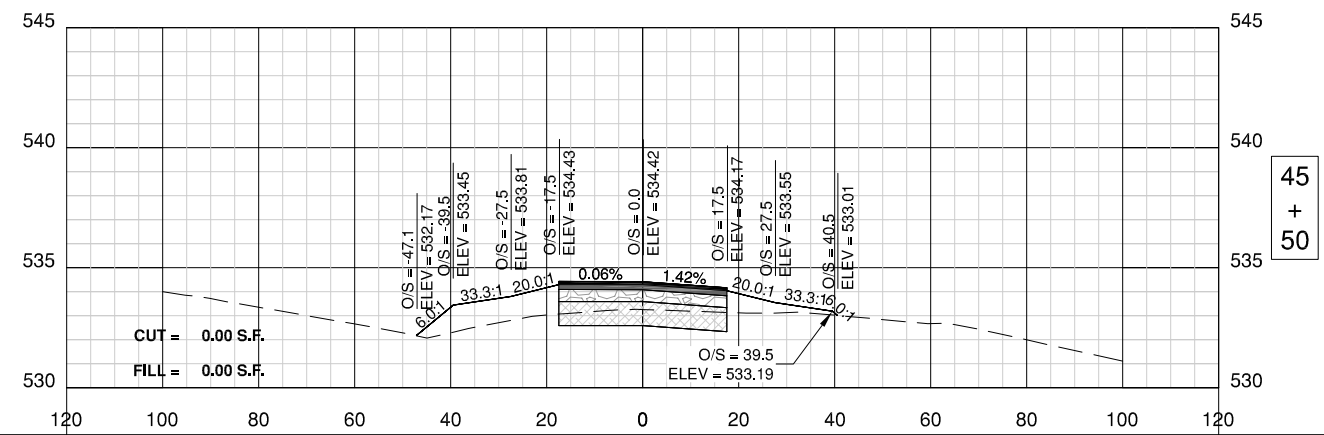
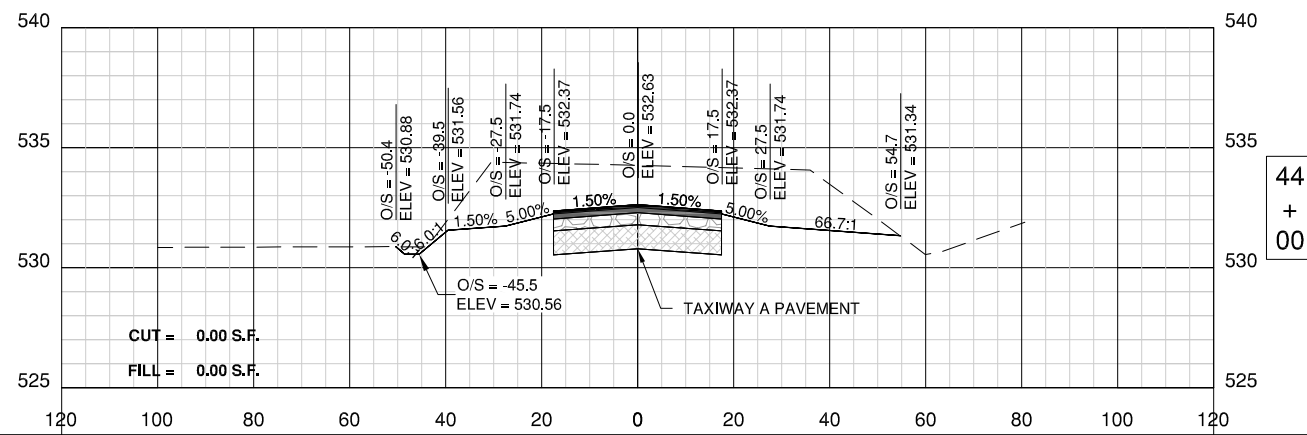
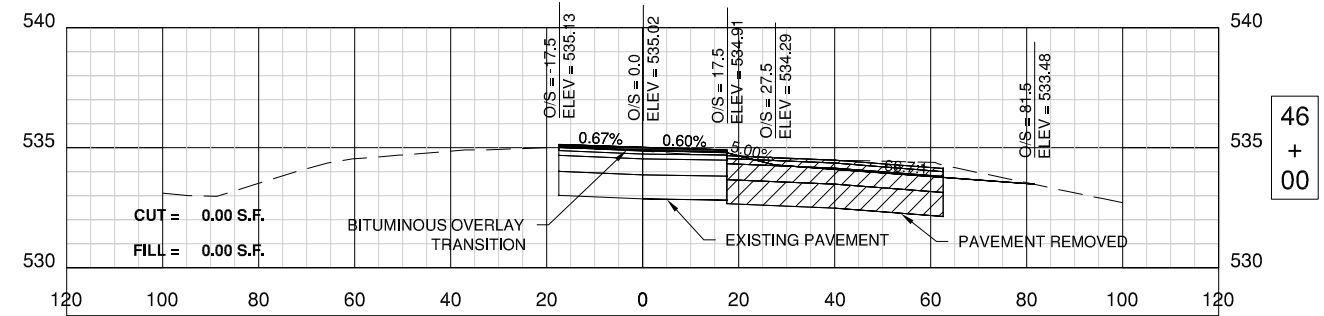
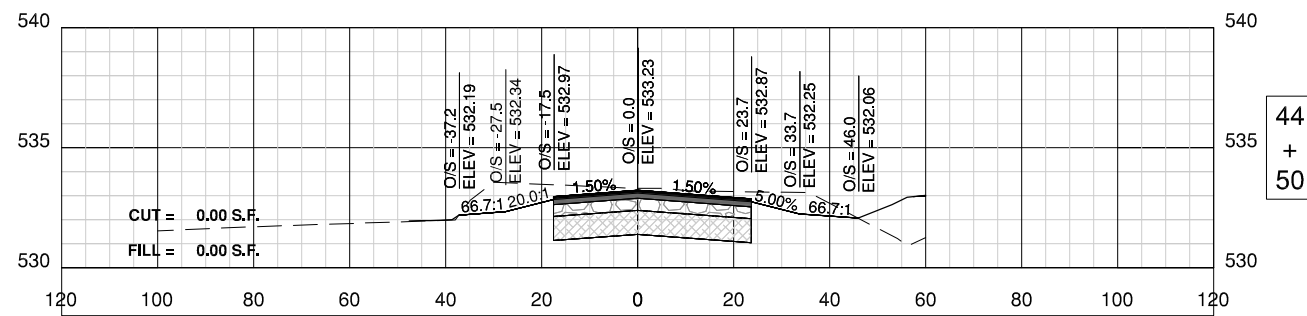
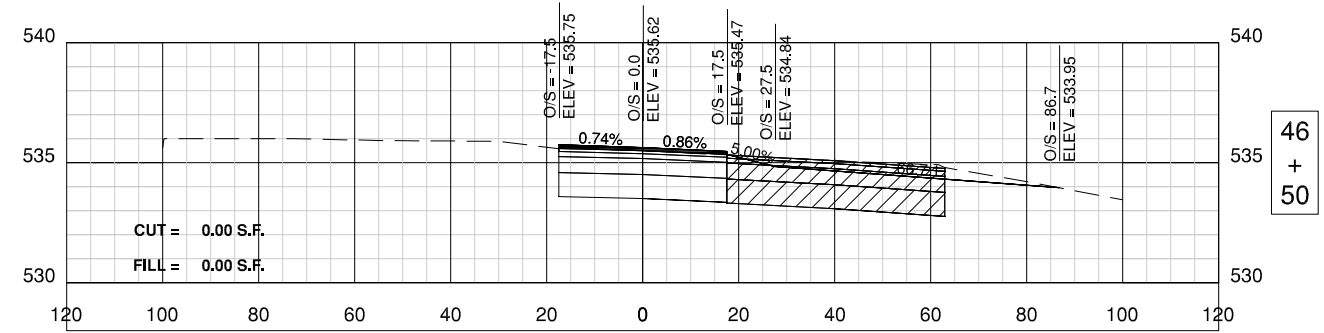
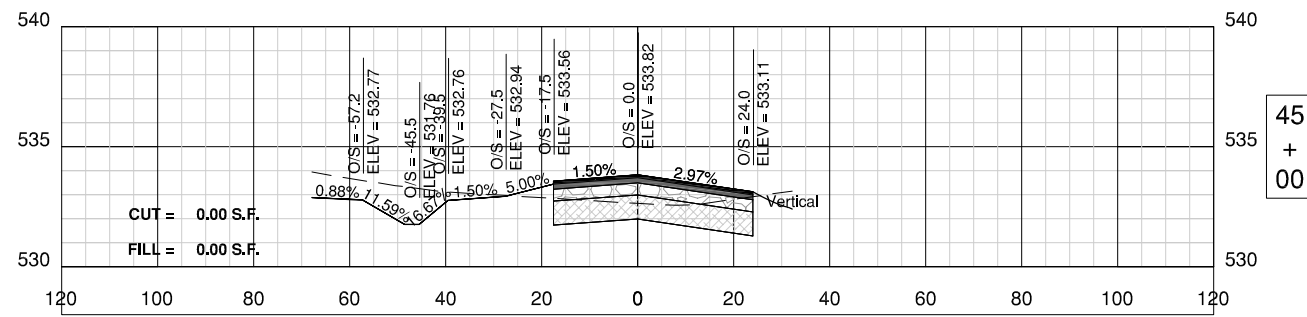
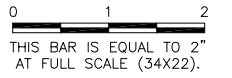
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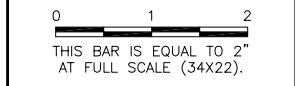
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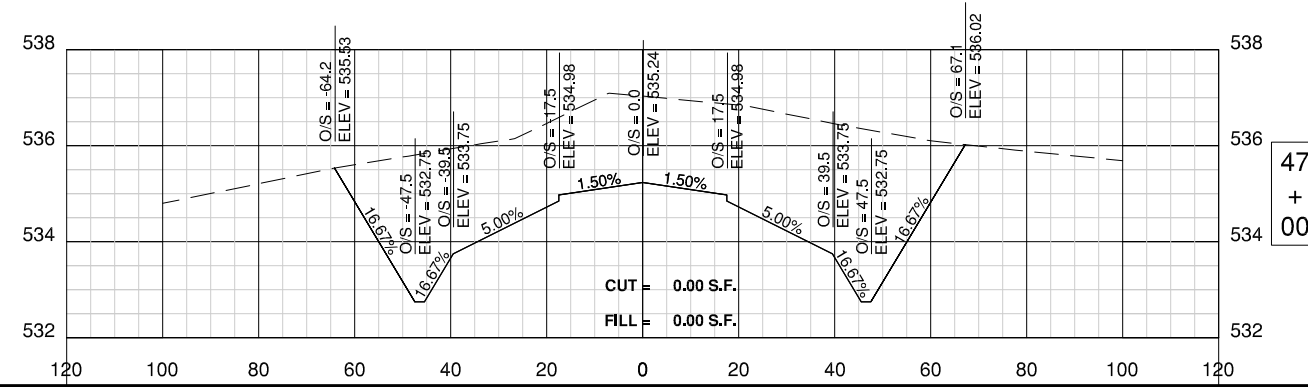
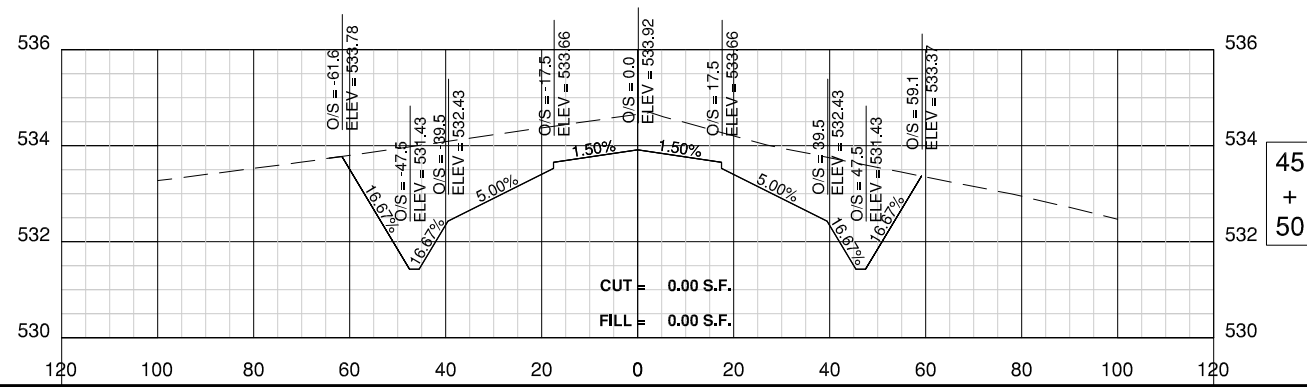
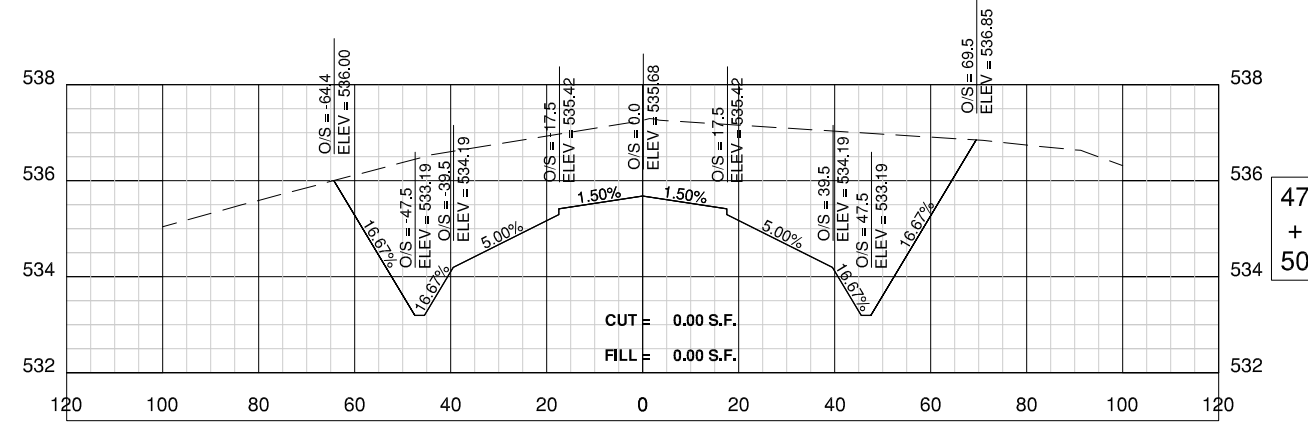
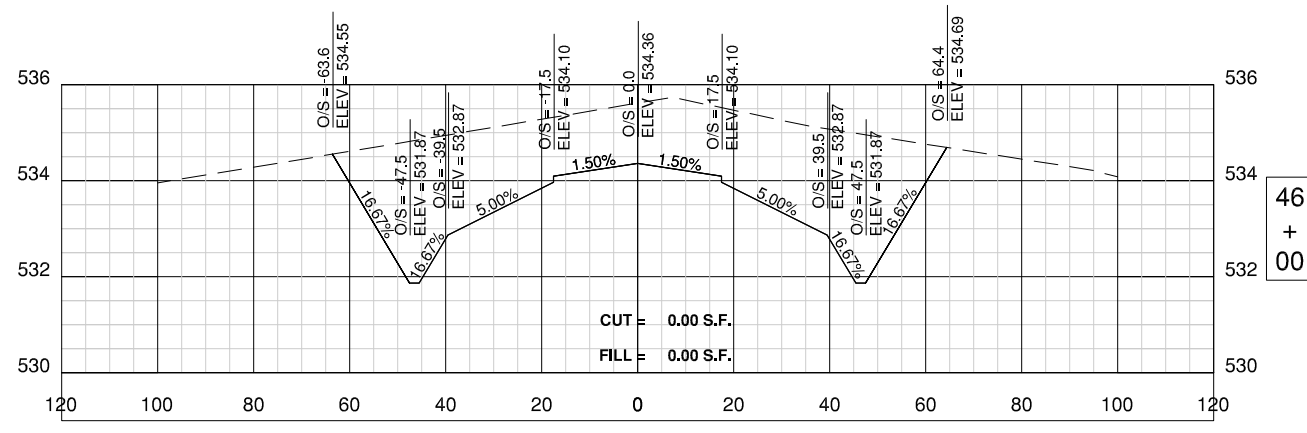
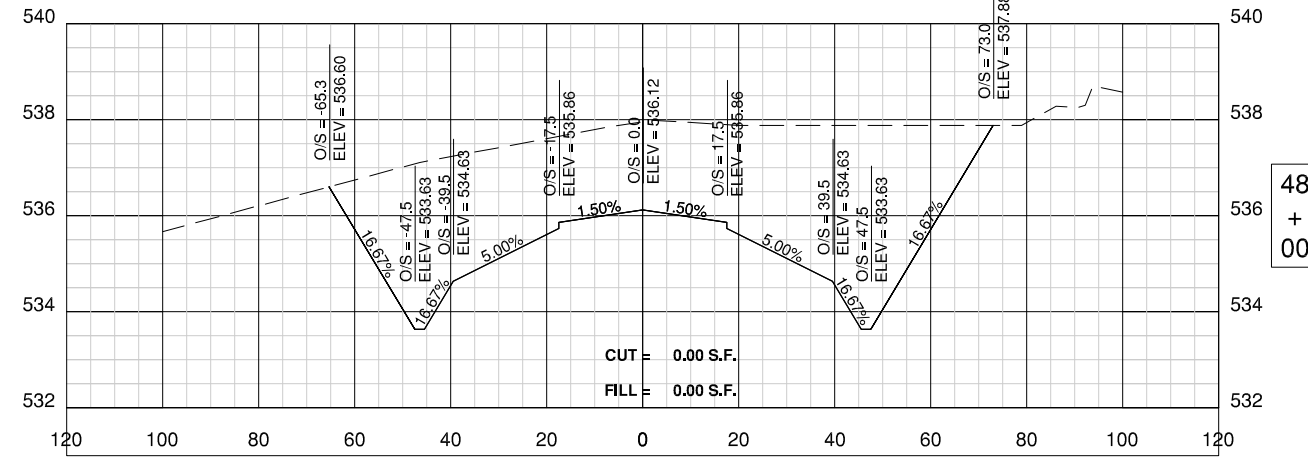
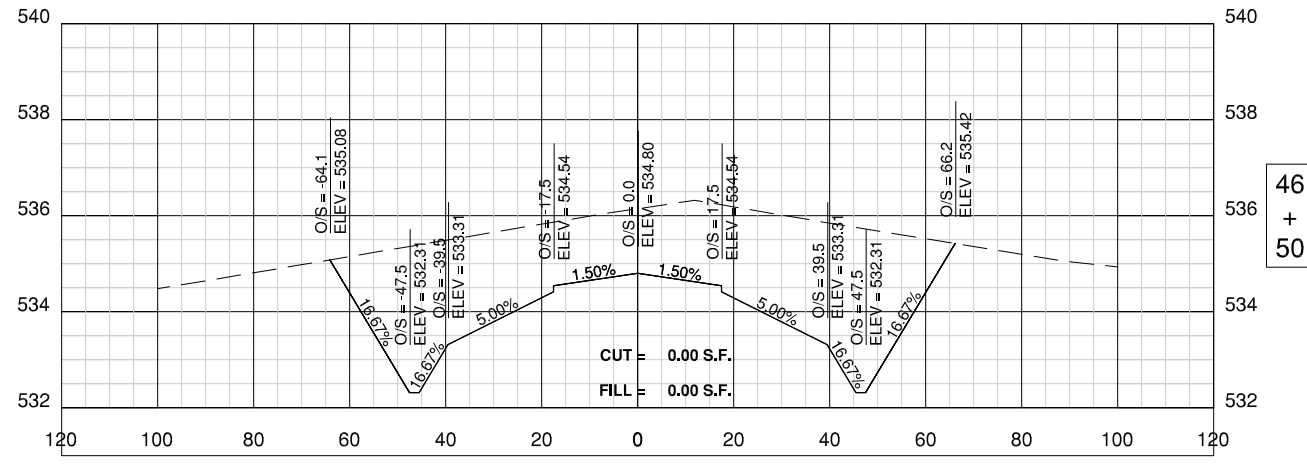
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**TAXIWAY EXTENSION
 BORROW PIT CROSS SECTIONS
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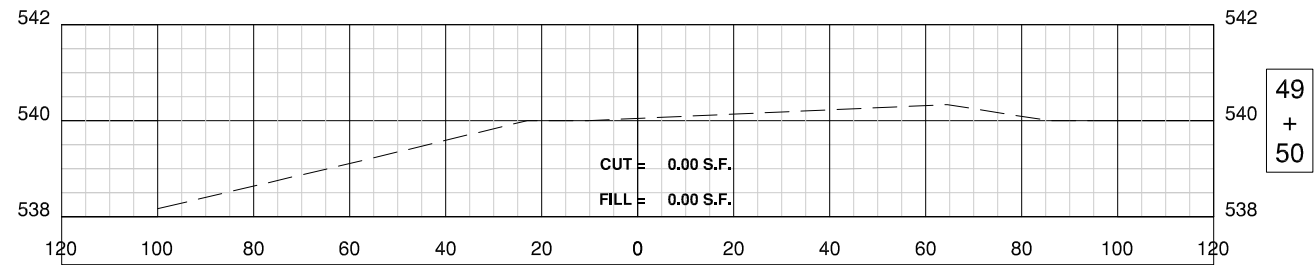
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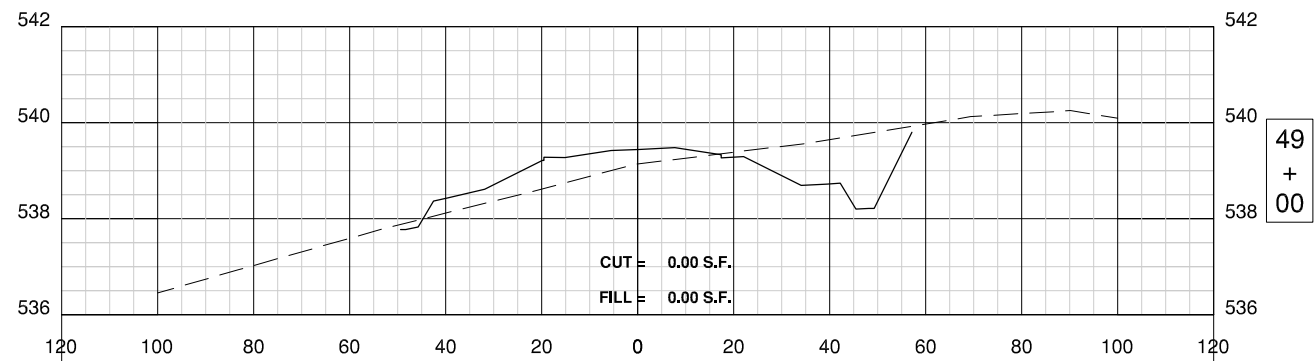


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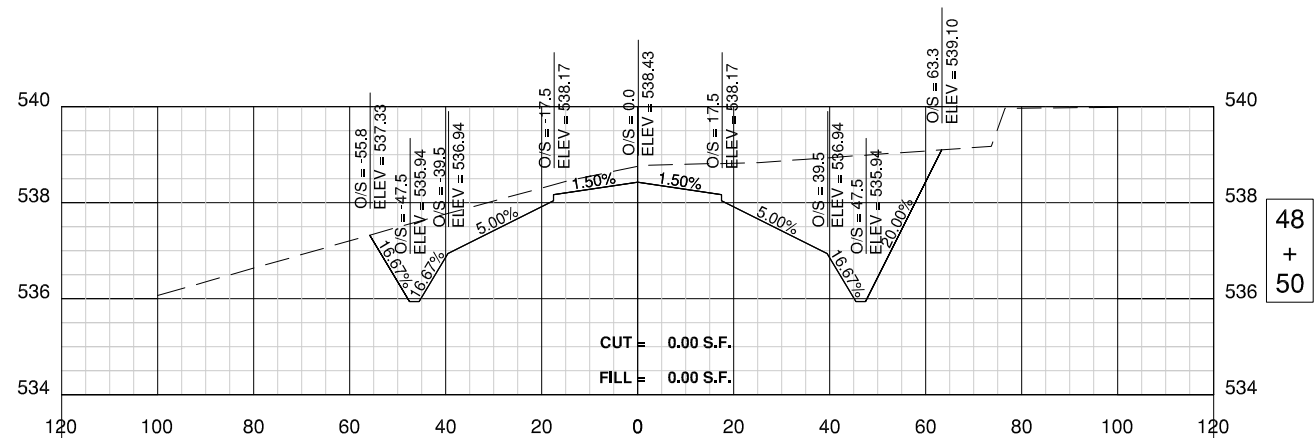
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