

06-14-2019 LETTING ITEM 006

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

* 24 + 1 = 25 TOTAL SHEETS

F.A.P. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
337	20RS-10	LAKE	24	1
CONTRACT NO. 62G30				

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENTS LOCATED IN THE VILLAGES OF KILDEER AND LONG GROVE

TRAFFIC DATA

2017 ADT
IL-22 = 20,400

POSTED SPEED LIMIT

IL-22 = 45 - 50 MPH

PROPOSED
HIGHWAY PLANS

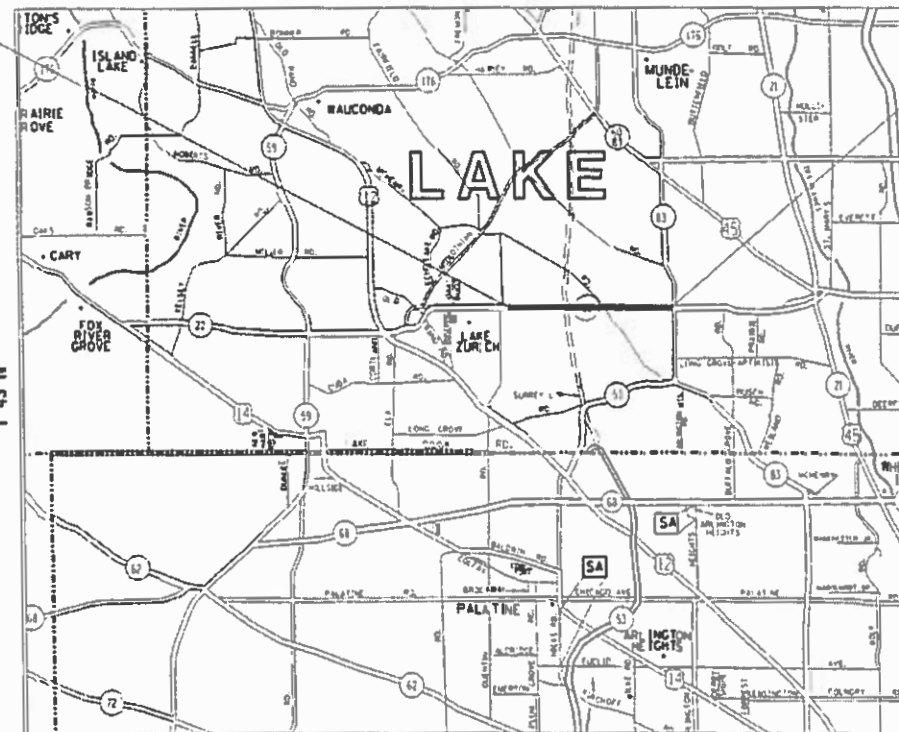
F.A.P. ROUTE 337: IL-22
N. QUENTIN ROAD TO IL-83
SECTION: 20RS-10
PROJECT: NHPP-B5AU(458)
RESURFACING (3P)
LAKE COUNTY

C-91-170-18

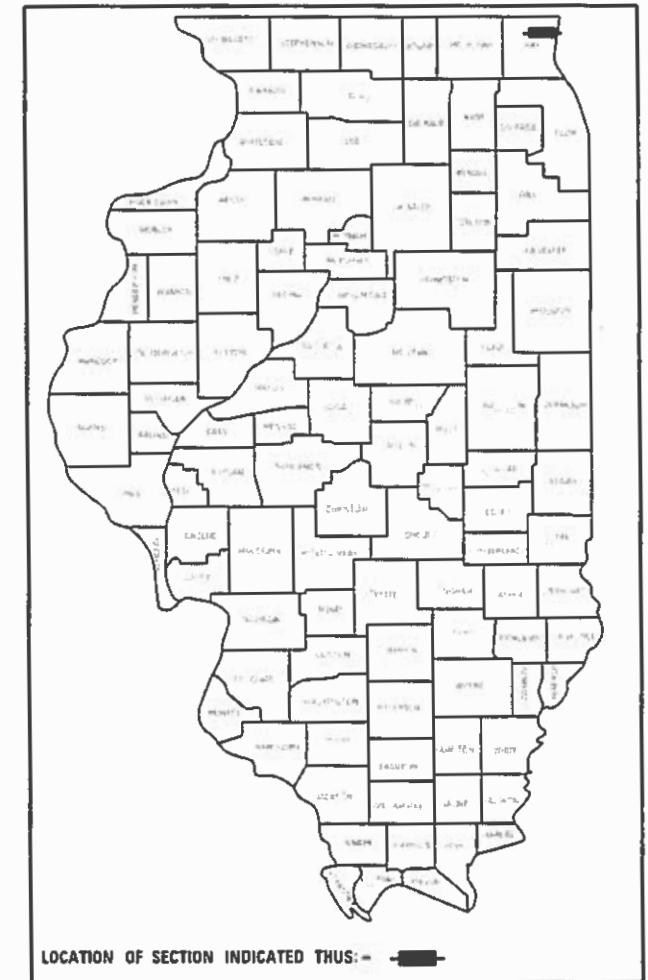
R 10 E TO 11 E

PROJECT BEGINS
EB: STA 21+31
WB: STA 22+22

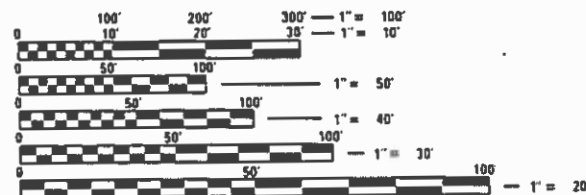
PROJECT ENDS
STA 185+37



ELA AND VERNON TOWNSHIP



LOCATION OF SECTION INDICATED THIS: [Symbol]



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811

PROJECT ENGINEER: ALAIN MIDY (847) 221-3056
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

GROSS LENGTH = 16,406 FT. = 3.107 MILE
NET LENGTH = 16,406 FT. = 3.107 MILE

CONTRACT NO. 62G30

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED March 18 2019
Anthony G. Wangley / CRB
REGIONAL ENGINEER

May 10 2019
Paul E. [Signature]
ENGINEER OF DESIGN AND ENVIRONMENT

May 10 2019
Paul E. [Signature]
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE				
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	40	40				
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	45	45				
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	5	5				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	100	100				
25200110	SODDING, SALT TOLERANT	SO YD	100	100				
25200200	SUPPLEMENTAL WATERING	UNIT	1	1				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	47733	47733				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	107	107				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4, 75, N50	TON	2980	2980				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	295	295				
40603565	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70	TON	6955	6955				
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	70930	70930				
44201749	CLASS D PATCHES, TYPE I, 9 INCH	SO YD	200	200				
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SO YD	480	480				
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SO YD	440	440				
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SO YD	1250	1250				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE				
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	160	160				
60250200	CATCH BASINS TO BE ADJUSTED	EACH	12	12				
60255410	CATCH BASINS TO BE CLEANED	EACH	14	14				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6				
67100100	MOBILIZATION	L SUM	1	1				
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1				
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1				
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1				
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1				
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1				
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	53110	53110				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	4430	4430				
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	622.2	622.2				

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

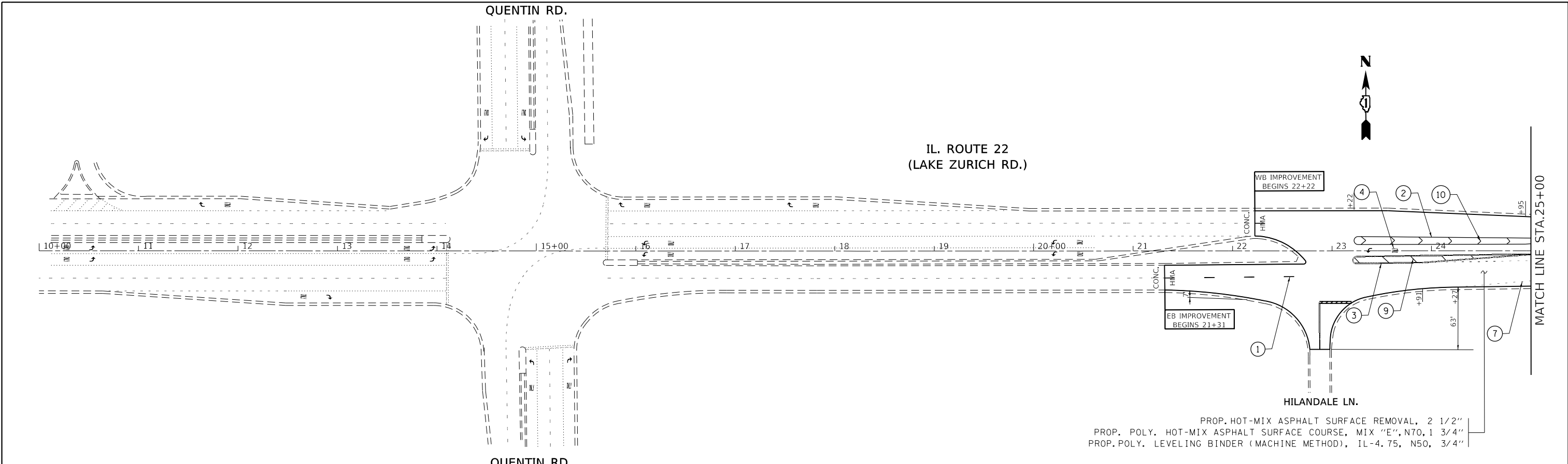
**IL-22 FROM QUENTIN ROAD TO IL-83
SUMMARY OF QUANTITIES**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
337	20RS-10	LAKE	24	3
CONTRACT NO.			62G30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

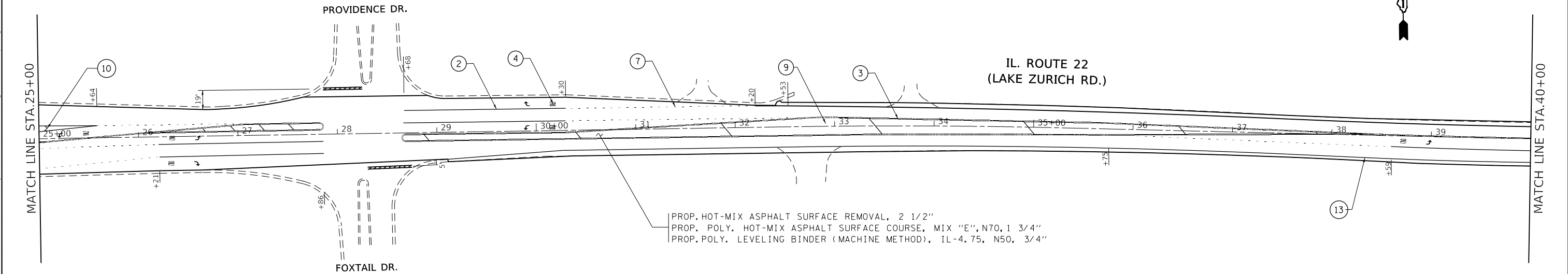
* SPECIALTY ITEM △ NON PARTICIPATING ITEM

REV. - MS

SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	0005 80% FED 20% STATE						CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	0005 80% FED 20% STATE					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	63520	63520															
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	3515	3515						X0326898	CENTER LINE - RUMBLE STRIP - 16"	FOOT	3160	3160					
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	665	665						X2020110	GRADING AND SHAPING SHOULDERS	UNIT	134	134					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1190	1190						△ X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	250	250					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	440	440						△ X5537900	STORM SEWERS TO BE CLEANED 15"	FOOT	250	250					
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	13280	13280						X6030205	FRAMES AND GRATES TO BE ADJUSTED (SPECIAL)	EACH	3	3					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	622.2	622.2						X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	30040	30040					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	57200	57200						* X7800815	HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 4 INCH	FOOT	6320	6320					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3515	3515						Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	450	450					
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	665	665						Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	27650	27650					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1190	1190						Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	102	102					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	440	440						* Z0064800	SELECTIVE CLEARING	UNIT	11	11					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	725	725															
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	700	700															
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1207	1207															



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|--|---|---|
| <ul style="list-style-type: none"> ① PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, WHITE (TYP.) ② PROP. THERMOPLASTIC PVMT MARKING 6", TURN LANE, WHITE (TYP.) ③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW @ 11" C-C (TYP.) ④ PROP. THERMOPLASTIC PVMT, MARKING LETTERS AND SYMBOLS, WHITE (TYP.) ⑤ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.) | <ul style="list-style-type: none"> ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.) ⑦ PROP. THERMOPLASTIC PVMT MARKING 6", 2' DASH 6' SKIP, WHITE (TYP.) ⑧ PROP. THERMOPLASTIC PVMT MARKING 8", EDGE LINE, WHITE (TYP.) ⑨ PROP. THERMOPLASTIC PVMT, MARKING 12" MEDIAN @ 45° 150' C-C, YELLOW (TYP.) ⑩ PROP. THERMOPLASTIC PVMT, MARKING 12" GORE MARKING, 30' C-C, WHITE (TYP.) | <ul style="list-style-type: none"> ⑪ PROP. THERMOPLASTIC PVMT MARKING 4", MEDIAN LANE, YELLOW (TYP.) ⑫ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, YELLOW (TYP.) ⑬ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.) ⑭ PROP. THERMOPLASTIC PVMT, MARKING 12" SHOULDER DIAGONALS @ 45° 150' C-C, WHITE (TYP.) |
|--|---|---|



PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 PROP. POLY. HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

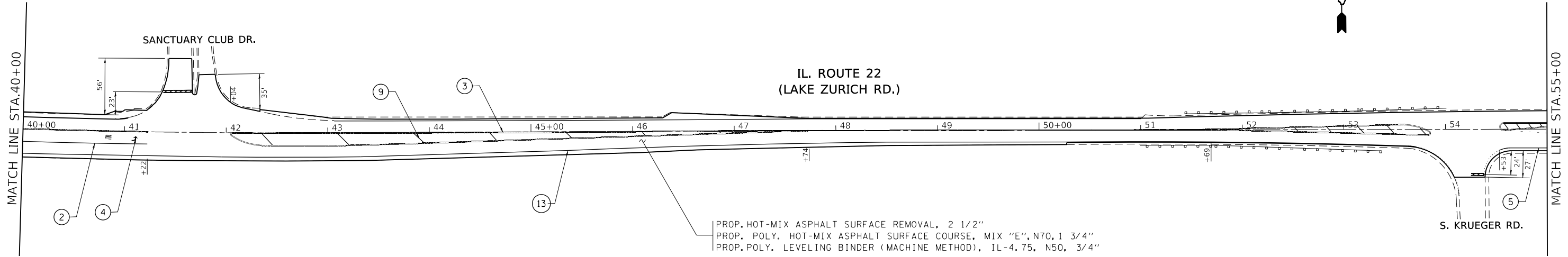
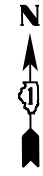
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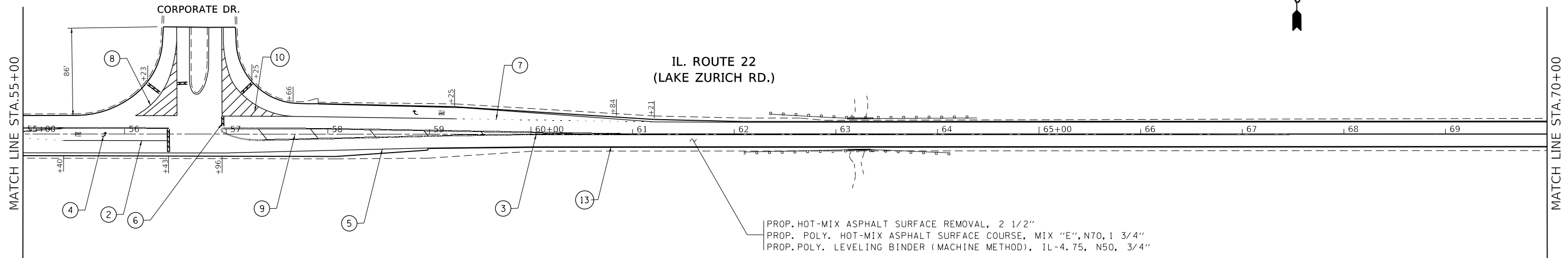
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED ROADWAY PLANS			
IL. ROUTE 22 (N. QUENTIN RD. TO IL. ROUTE 83)			
SCALE: 1"= 50'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
337	20RS-10	LAKE	24	6
CONTRACT NO. 62G30				
ILLINOIS FED. AID PROJECT				



- | | | |
|--|--|---|
| ① PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, WHITE (TYP.) | ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.) | ⑪ PROP. THERMOPLASTIC PVMT MARKING 4", MEDIAN LANE, YELLOW (TYP.) |
| ② PROP. THERMOPLASTIC PVMT MARKING 6", TURN LANE, WHITE (TYP.) | ⑦ PROP. THERMOPLASTIC PVMT MARKING 6", 2' DASH 6' SKIP, WHITE (TYP.) | ⑫ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, YELLOW (TYP.) |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW @ 11" C-C (TYP.) | ⑧ PROP. THERMOPLASTIC PVMT MARKING 8", EDGE LINE, WHITE (TYP.) | ⑬ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LINE, WHITE (TYP.) |
| ④ PROP. THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS, WHITE (TYP.) | ⑨ PROP. THERMOPLASTIC PVMT. MARKING 12" MEDIAN @ 45° 150' C-C, YELLOW (TYP.) | ⑭ PROP. THERMOPLASTIC PVMT. MARKING 12" SHOULDER DIAGONALS @ 45° 150' C-C, WHITE (TYP.) |
| ⑤ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.) | ⑩ PROP. THERMOPLASTIC PVMT. MARKING 12" GORE MARKING, 30' C-C, WHITE (TYP.) | |



PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 PROP. POLY. HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED ROADWAY PLANS	
IL. ROUTE 22 (N. QUENTIN RD. TO IL. ROUTE 83)	
SCALE: 1"= 50'	SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
337	20RS-10	LAKE	24	7
CONTRACT NO. 62G30				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 70+00

MATCH LINE STA. 85+00

IL. ROUTE 22
(LAKE ZURICH RD.)

70+00 71 72 73 74 75+00 76 77 78 79 80+00 81 82 83 84

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
PROP. POLY. HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"
PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

- | | | |
|---|---|---|
| ① PROP. THERMOPLASTIC PVMT MARKING 4",
10' DASH 30" SKIP, WHITE (TYP.) | ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24"
STOPBAR, WHITE (TYP.) | ⑪ PROP. THERMOPLASTIC PVMT MARKING 4",
MEDIAN LANE, YELLOW (TYP.) |
| ② PROP. THERMOPLASTIC PVMT MARKING 6",
TURN LANE, WHITE (TYP.) | ⑦ PROP. THERMOPLASTIC PVMT MARKING 6",
2' DASH 6" SKIP, WHITE (TYP.) | ⑫ PROP. THERMOPLASTIC PVMT MARKING 4",
10' DASH 30" SKIP, YELLOW (TYP.) |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4",
DOUBLE YELLOW @ 11" C-C (TYP.) | ⑧ PROP. THERMOPLASTIC PVMT MARKING 8",
EDGE LINE, WHITE (TYP.) | ⑬ PROP. THERMOPLASTIC PVMT MARKING 4",
SHOULDER LINE, WHITE (TYP.) |
| ④ PROP. THERMOPLASTIC PVMT. MARKING
LETTERS AND SYMBOLS, WHITE (TYP.) | ⑨ PROP. THERMOPLASTIC PVMT. MARKING 12"
MEDIAN @ 45° 150' C-C, YELLOW (TYP.) | ⑭ PROP. THERMOPLASTIC PVMT. MARKING 12"
SHOULDER DIAGONALS @ 45° 150' C-C,
WHITE (TYP.) |
| ⑤ PROP. THERMOPLASTIC PVMT MARKING 4",
SHOULDER LANE, WHITE (TYP.) | ⑩ PROP. THERMOPLASTIC PVMT. MARKING 12"
GORE MARKING, 30' C-C, WHITE (TYP.) | |



MATCH LINE STA. 85+00

MATCH LINE STA. 100+00

IL. ROUTE 22
(LAKE ZURICH RD.)

85+00 86 87 88 89 90+00 91 92 93 94 95+00 96 97 98 99

SALEM LAKE DR.

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
PROP. POLY. HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"
PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

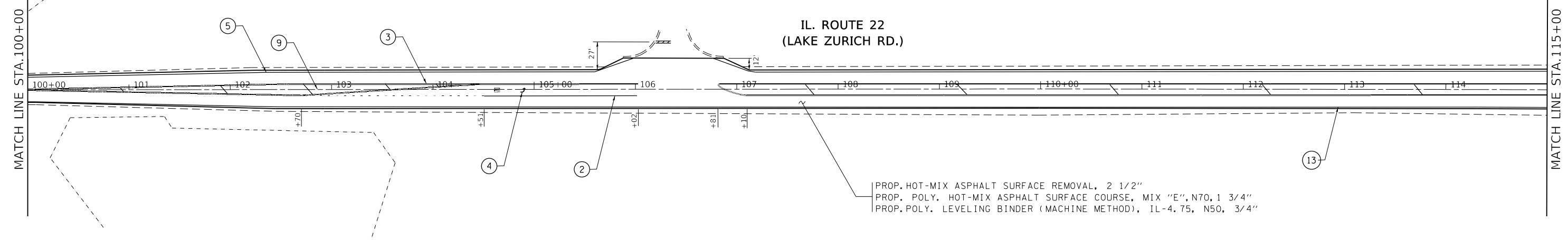
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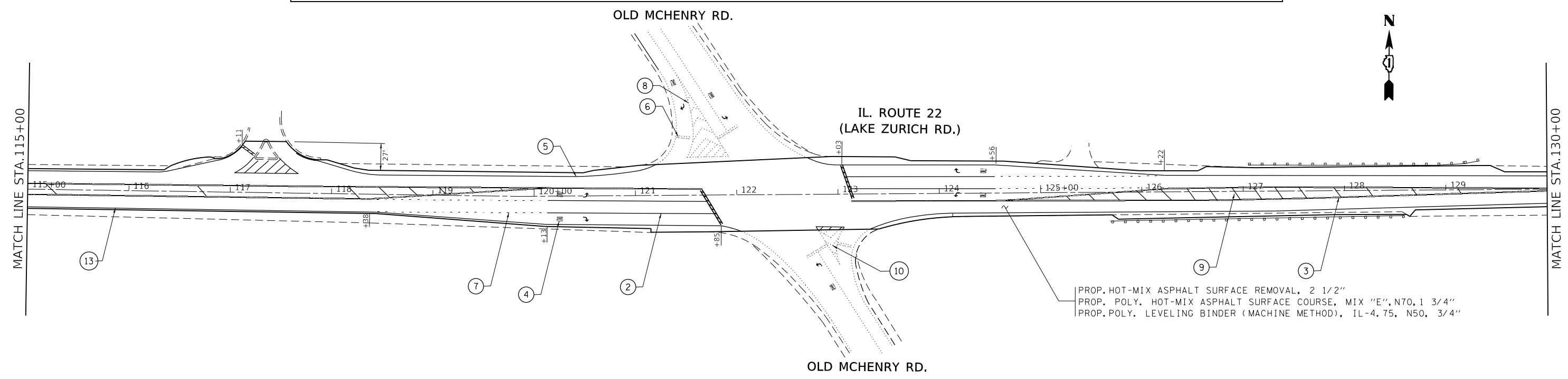
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PLANS
IL. ROUTE 22 (N. QUENTIN RD. TO IL. ROUTE 83)
SCALE: 1"= 50'
SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
337	20RS-10	LAKE	24	8
CONTRACT NO. 62G30				
ILLINOIS FED. AID PROJECT				



- | | | |
|---|--|--|
| <p>① PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, WHITE (TYP.)</p> <p>② PROP. THERMOPLASTIC PVMT MARKING 6", TURN LANE, WHITE (TYP.)</p> <p>③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW @ 11" C-C (TYP.)</p> <p>④ PROP. THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS, WHITE (TYP.)</p> <p>⑤ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.)</p> | <p>⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.)</p> <p>⑦ PROP. THERMOPLASTIC PVMT MARKING 6", 2' DASH 6' SKIP, WHITE (TYP.)</p> <p>⑧ PROP. THERMOPLASTIC PVMT MARKING 8", EDGE LINE, WHITE (TYP.)</p> <p>⑨ PROP. THERMOPLASTIC PVMT. MARKING 12" MEDIAN @ 45° 150' C-C, YELLOW (TYP.)</p> <p>⑩ PROP. THERMOPLASTIC PVMT. MARKING 12" GORE MARKING, 30' C-C, WHITE (TYP.)</p> | <p>⑪ PROP. THERMOPLASTIC PVMT MARKING 4", MEDIAN LANE, YELLOW (TYP.)</p> <p>⑫ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, YELLOW (TYP.)</p> <p>⑬ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LINE, WHITE (TYP.)</p> <p>⑭ PROP. THERMOPLASTIC PVMT. MARKING 12" SHOULDER DIAGONALS @ 45° 150' C-C, WHITE (TYP.)</p> |
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PLOT DATE = 3/20/2019	DATE -	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLANS
IL. ROUTE 22 (N. QUENTIN RD. TO IL. ROUTE 83)**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
337	20RS-10	LAKE	24	9
CONTRACT NO. 62G30				
ILLINOIS FED. AID PROJECT				

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MATCH LINE STA. 130+00

MATCH LINE STA. 145+00

MATCH LINE STA. 145+00

MATCH LINE STA. 160+00



MIRANDA LN.

IL. ROUTE 22
(LAKE ZURICH RD.)

N. KRUEGER RD.

IL. ROUTE 22
(LAKE ZURICH RD.)

OAK CREEK LN.

OAK LEAF LN.

OAK LEAF LN.

3

12

11

13

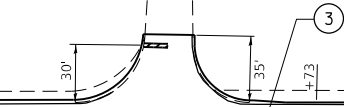
PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 PROP. POLY. HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 PROP. POLY. HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

- | | | |
|---|---|--|
| 1 PROP. THERMOPLASTIC PVMT MARKING 4",
10' DASH 30' SKIP, WHITE (TYP.) | 6 PROP. THERMOPLASTIC PVMT MARKING LINE, 24"
STOPBAR, WHITE (TYP.) | 11 PROP. THERMOPLASTIC PVMT MARKING 4",
MEDIAN LANE, YELLOW (TYP.) |
| 2 PROP. THERMOPLASTIC PVMT MARKING 6",
TURN LANE, WHITE (TYP.) | 7 PROP. THERMOPLASTIC PVMT MARKING 6",
2' DASH 6' SKIP, WHITE (TYP.) | 12 PROP. THERMOPLASTIC PVMT MARKING 4",
10' DASH 30' SKIP, YELLOW (TYP.) |
| 3 PROP. THERMOPLASTIC PVMT MARKING 4",
DOUBLE YELLOW @ 11" C-C (TYP.) | 8 PROP. THERMOPLASTIC PVMT MARKING 8",
EDGE LINE, WHITE (TYP.) | 13 PROP. THERMOPLASTIC PVMT MARKING 4",
SHOULDER LINE, WHITE (TYP.) |
| 4 PROP. THERMOPLASTIC PVMT. MARKING
LETTERS AND SYMBOLS, WHITE (TYP.) | 9 PROP. THERMOPLASTIC PVMT. MARKING 12"
MEDIAN @ 45' 150' C-C, YELLOW (TYP.) | 14 PROP. THERMOPLASTIC PVMT. MARKING 12"
SHOULDER DIAGONALS @ 45' 150' C-C,
WHITE (TYP.) |
| 5 PROP. THERMOPLASTIC PVMT MARKING 4",
SHOULDER LANE, WHITE (TYP.) | 10 PROP. THERMOPLASTIC PVMT. MARKING 12"
GORE MARKING, 30' C-C, WHITE (TYP.) | |

BLACKHAWK LN.

WILLOWBROOK RD.

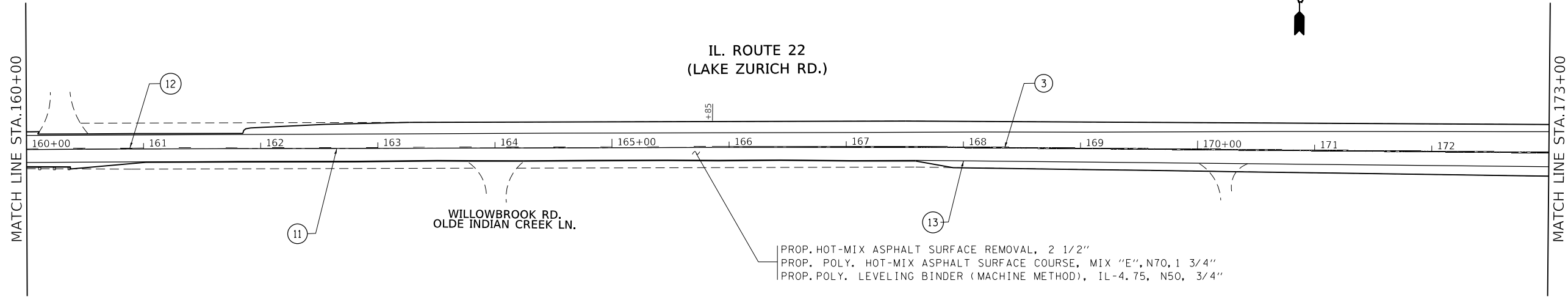


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PLOT DATE = 3/20/2019	CHECKED -	REVISED -
	DATE -	REVISED -

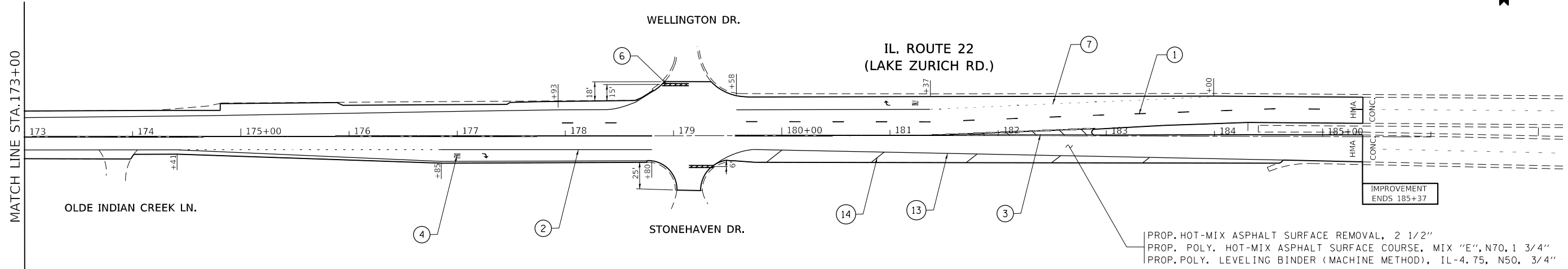
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PLANS
 IL. ROUTE 22 (N. QUENTIN RD. TO IL. ROUTE 83)
 SCALE: 1"= 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
337	20RS-10	LAKE	24	10
CONTRACT NO. 62G30				
ILLINOIS FED. AID PROJECT				



- | | | |
|--|--|---|
| ① PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, WHITE (TYP.) | ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.) | ⑪ PROP. THERMOPLASTIC PVMT MARKING 4", MEDIAN LANE, YELLOW (TYP.) |
| ② PROP. THERMOPLASTIC PVMT MARKING 6", TURN LANE, WHITE (TYP.) | ⑦ PROP. THERMOPLASTIC PVMT MARKING 6", 2' DASH 6' SKIP, WHITE (TYP.) | ⑫ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, YELLOW (TYP.) |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW @ 11" C-C (TYP.) | ⑧ PROP. THERMOPLASTIC PVMT MARKING 8", EDGE LINE, WHITE (TYP.) | ⑬ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LINE, WHITE (TYP.) |
| ④ PROP. THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS, WHITE (TYP.) | ⑨ PROP. THERMOPLASTIC PVMT. MARKING 12" MEDIAN @ 45° 150' C-C, YELLOW (TYP.) | ⑭ PROP. THERMOPLASTIC PVMT. MARKING 12" SHOULDER DIAGONALS @ 45° 150' C-C, WHITE (TYP.) |
| ⑤ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.) | ⑩ PROP. THERMOPLASTIC PVMT. MARKING 12" GORE MARKING, 30' C-C, WHITE (TYP.) | |



- PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 PROP. POLY. HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

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	DATE -	REVISED -

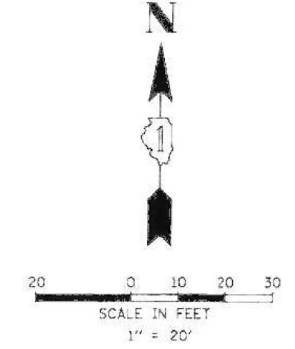
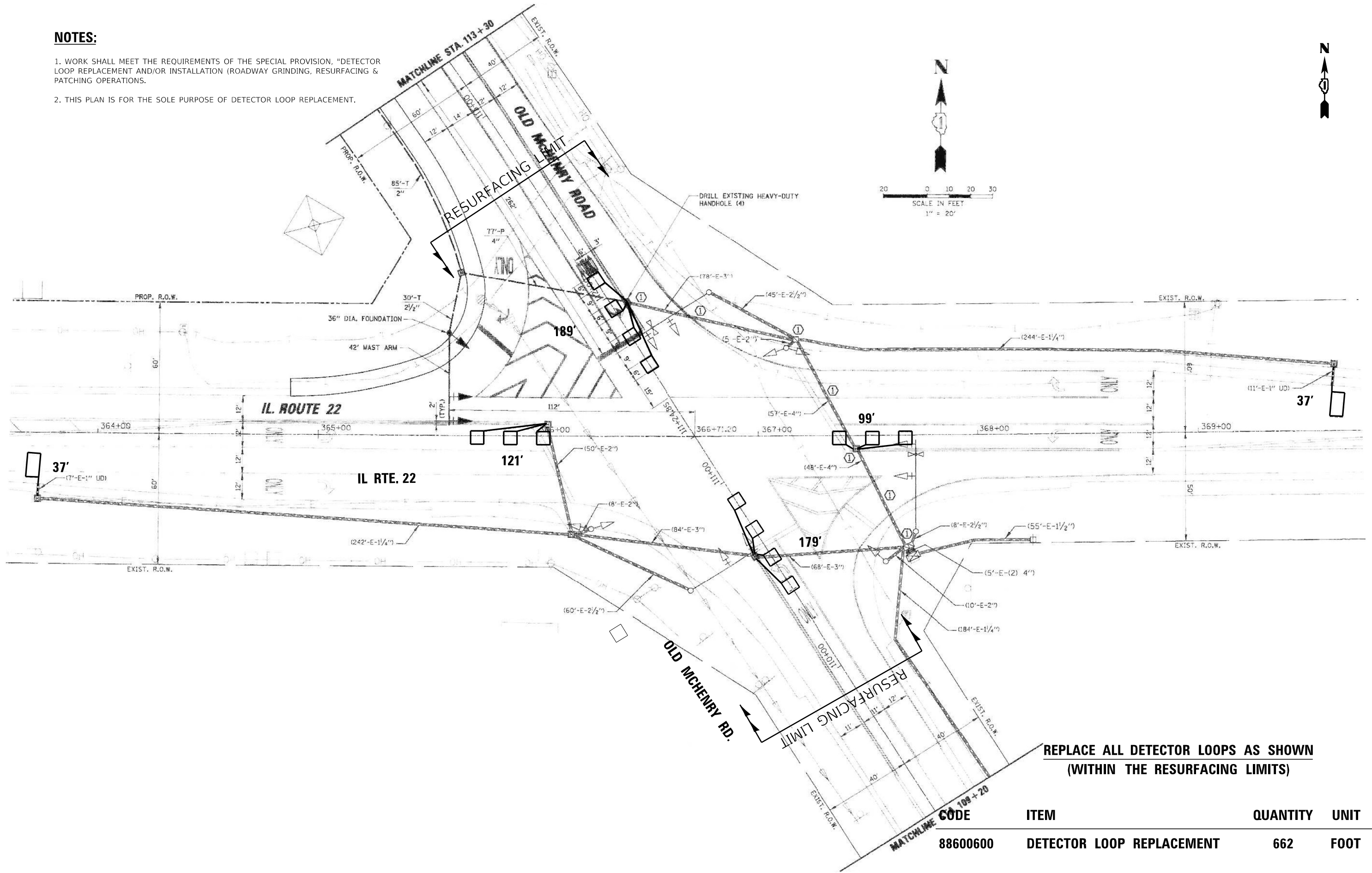
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED ROADWAY PLANS	
IL. ROUTE 22 (N. QUENTIN RD. TO IL. ROUTE 83)	
SCALE: 1" = 50'	SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
337	20RS-10	LAKE	24	11
CONTRACT NO. 62G30				
ILLINOIS FED. AID PROJECT				

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	662	FOOT

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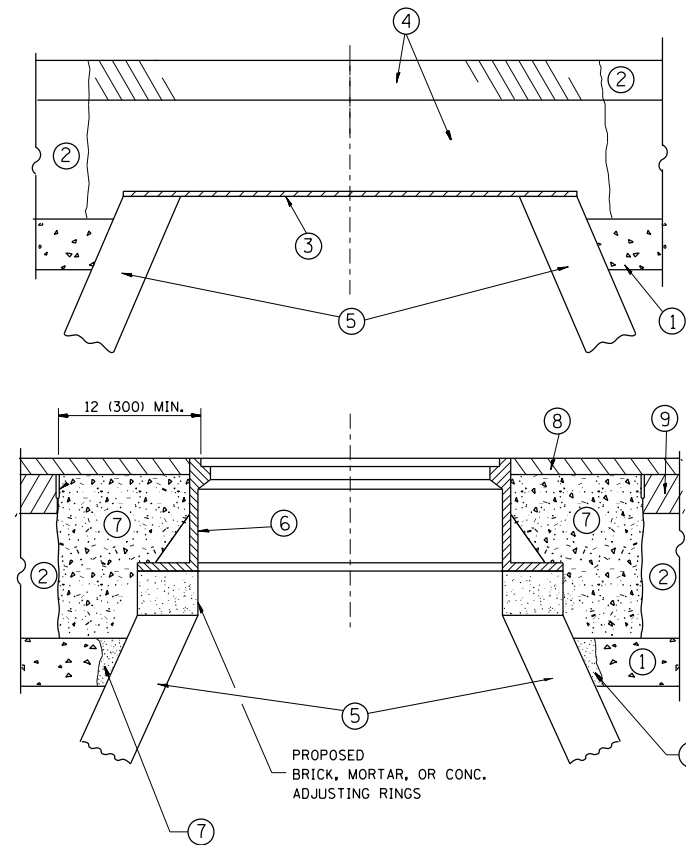
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PLOT DATE = 1/23/2019	CHECKED - LP	REVISD -
	DATE - 1/23/2019	REVISD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
ROUTE 22 AT OLD MCHENRY RD.**

SCALE: 1"=20' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
337	20RS-10	LAKE	24	13
CONTRACT NO. 62G30				
ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ⑥ FRAME AND LID (SEE NOTES)
- ② EXISTING PAVEMENT
- ⑦ CLASS PP-1* CONCRETE
- ③ 36 (900) DIAMETER METAL PLATE
- ⑧ PROPOSED HMA SURFACE COURSE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑨ PROPOSED HMA BINDER COURSE
- ⑤ EXISTING STRUCTURE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

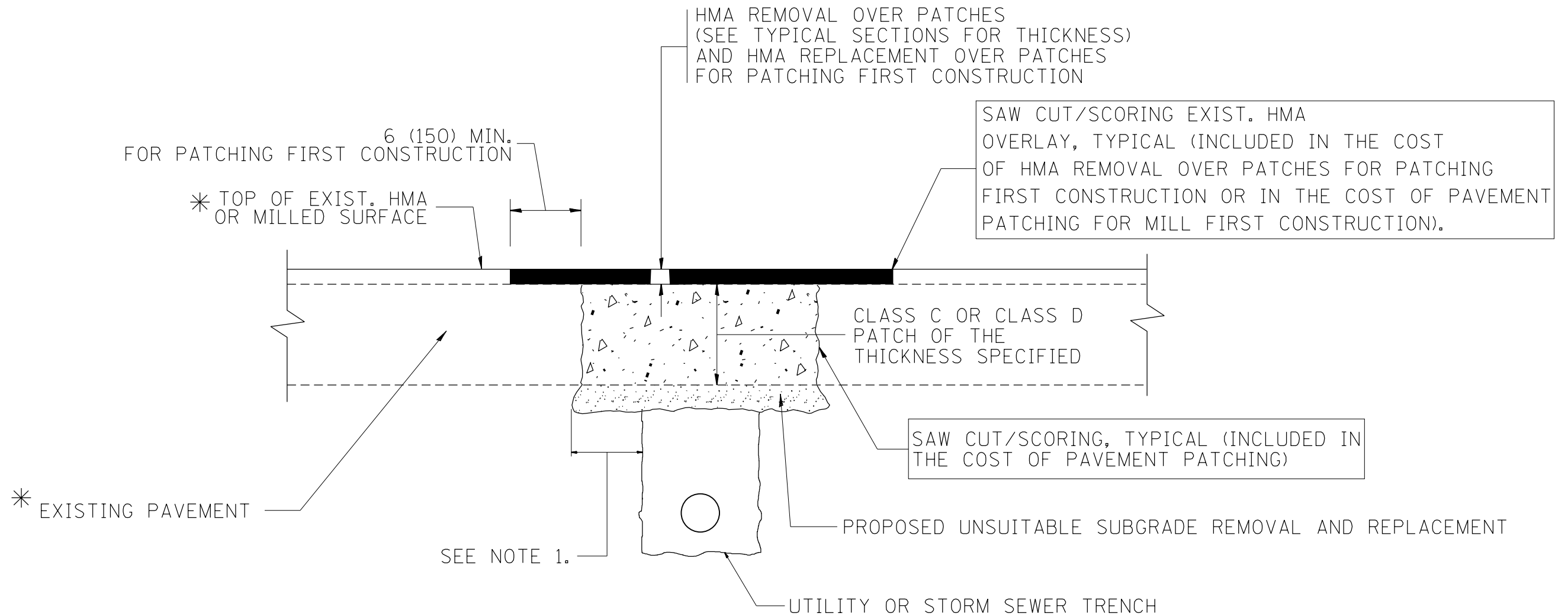
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = hassan	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
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	PLOT DATE = 3/20/2019	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
337	20RS-10	LAKE	24	14
BD600-03 (BD-8)			CONTRACT NO. 62G30	
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

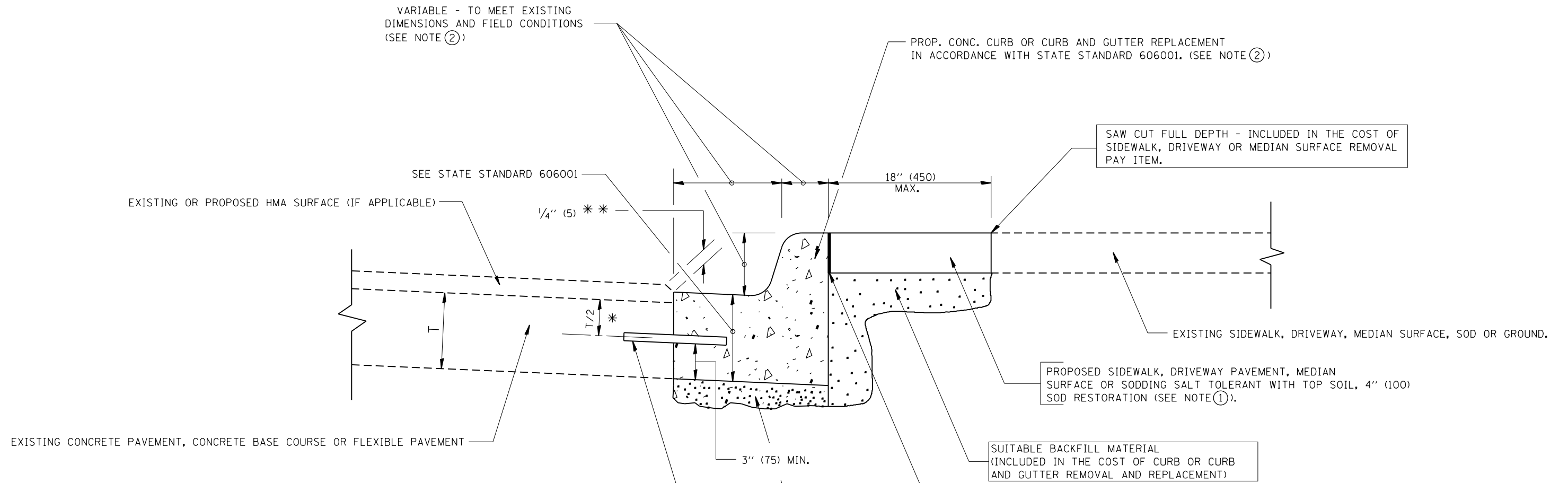
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = hassan	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 3/20/2019	DATE - 10-25-94	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 62G30				
			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

3" (75) MIN.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

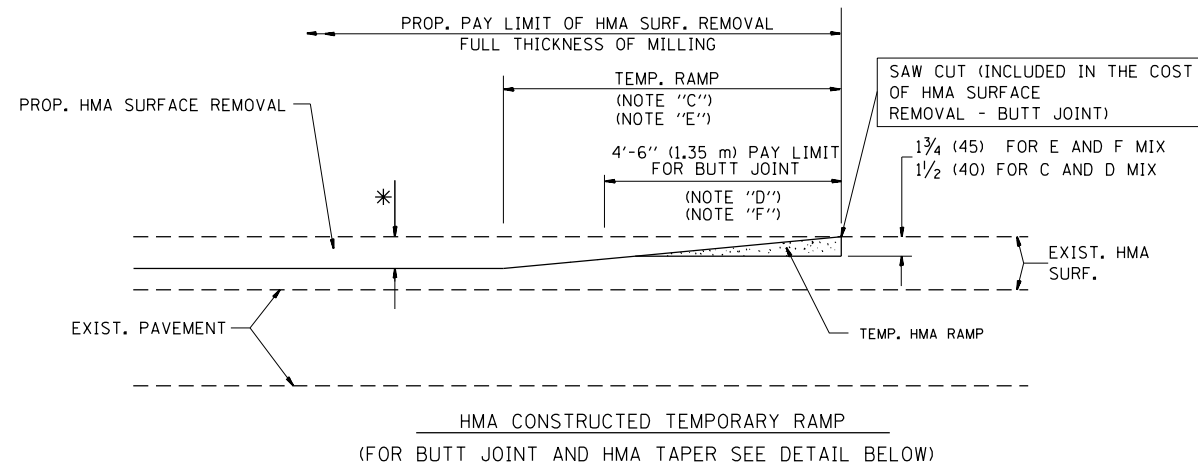
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = hassan	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0127318\BID\Drawings\Design\DistStd.dgn		REVISION	REVISED - A. ABBAS 03-21-97			337	20RS-10	LAKE	24	16
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 01-22-01				BD600-06 (BD-24)		CONTRACT NO. 62G30		
PLOT DATE = 3/20/2019	DATE - 03-11-94	REVISED - R. BORO 12-15-09	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

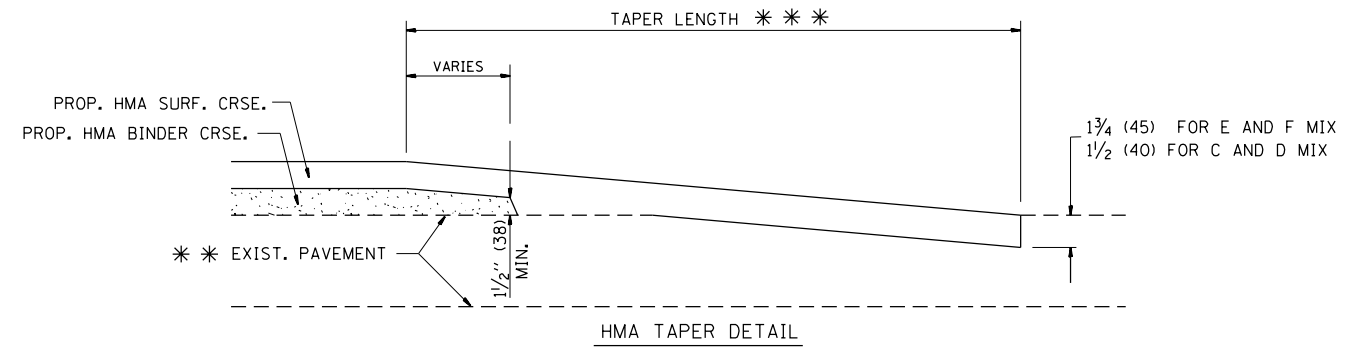
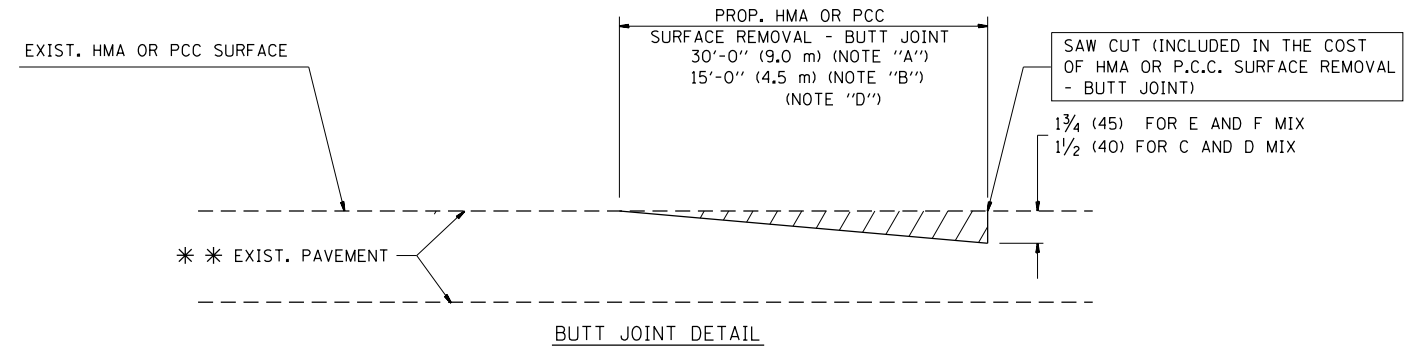


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

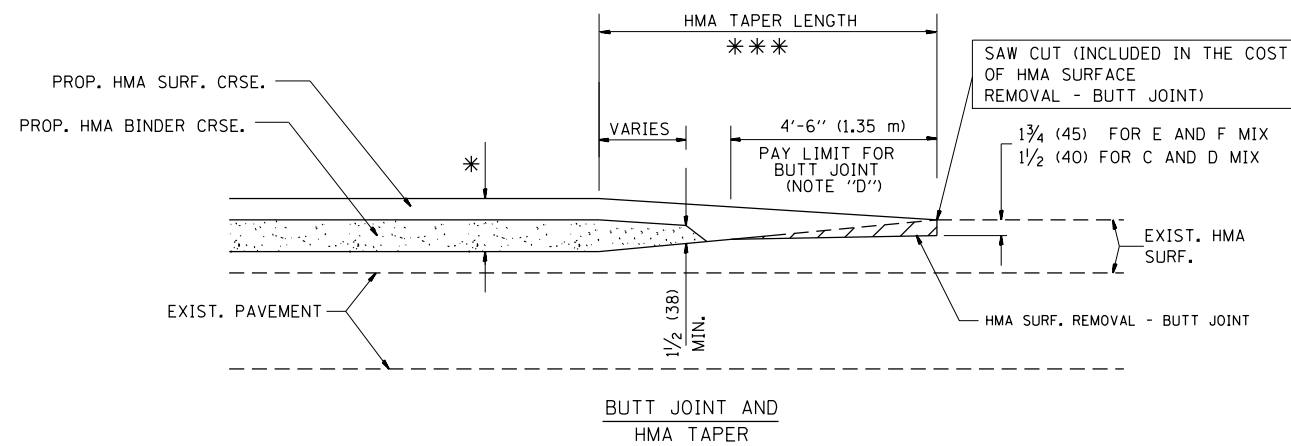
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



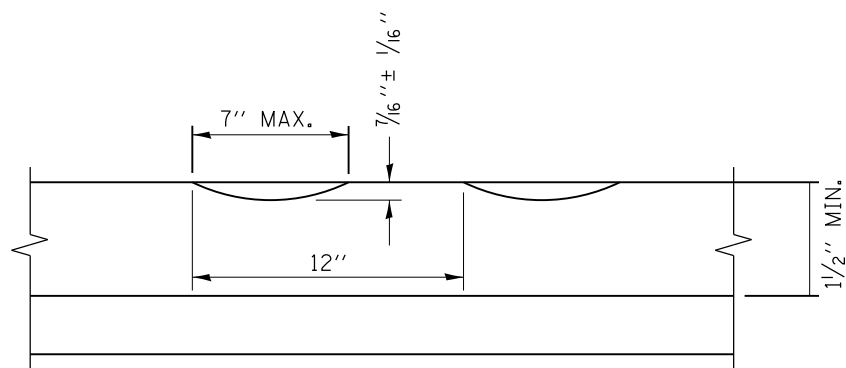
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME =	USER NAME = hassan	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0127318\DRAWING\Design\DistStd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 3/20/2019	DATE - 06-13-90	REVISED - R. BORO 01-01-07

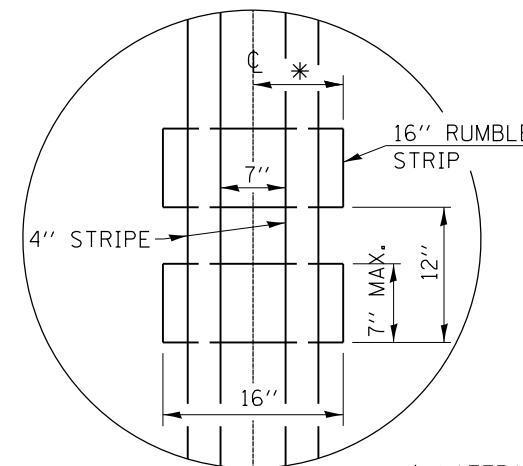
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
337	20RS-10	LAKE	24	17
BD400-05 BD32		CONTRACT NO.	62G30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SECTION A-A



* LATERAL DEVIATION SHALL NOT EXCEED ONE INCH IN 100 FEET.

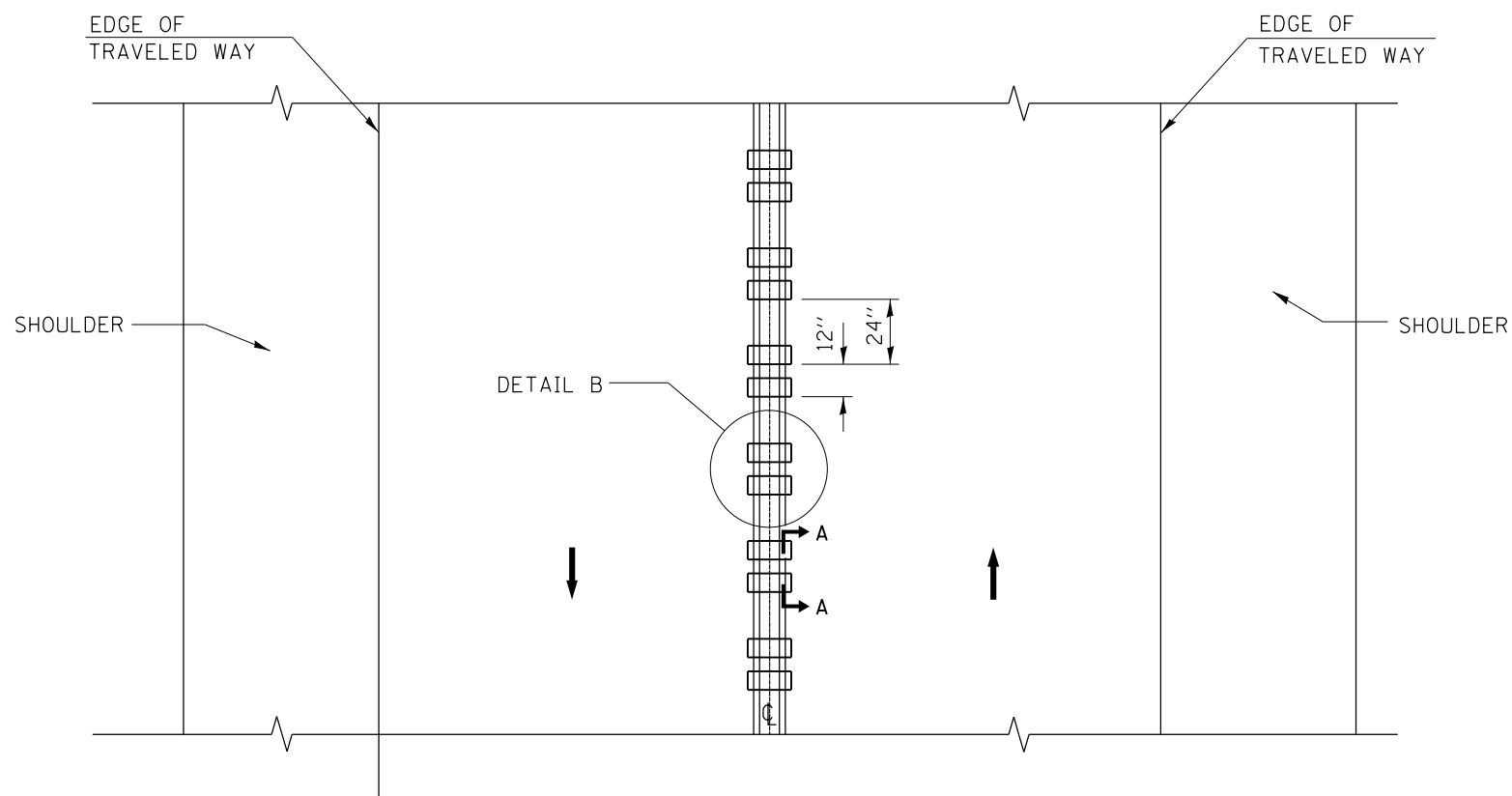
DETAIL B

GENERAL NOTES

- CENTERLINE RUMBLE STRIPS SHALL BE CONSTRUCTED ACCORDING TO SECTION 642 ALONG THE CENTERLINE OF PAVEMENT.
- SEE STANDARD 780001 FOR OTHER STRIPING LAYOUTS.
- RUMBLE STRIPS SHALL NOT BE PLACED ON BRIDGES.
- ALL RUMBLE STRIPS SHALL BE MILLED.
- CENTERLINE RUMBLE STRIPS SHALL BE CONTINUOUS THROUGH CONNECTIONS OF SIDEROADS WITH NO LEFT TURN LANES.
- DISCONTINUE CENTERLINE RUMBLE STRIPS THROUGH THE LIMITS OF ALL LEFT TURN LANES, INCLUDING ANY LANE TAPER SECTIONS.
- AFTER RUMBLE STRIPS ARE INSTALLED, THE PAVEMENT SURFACE SHALL BE SWEEP CLEAN PRIOR TO THE PLACEMENT OF THE NEW PAVEMENT MARKINGS.
- WHERE USED, ADJUST SPACING OF RAISED REFLECTIVE PAVEMENT MARKERS TO FALL IN WIDER GAP BETWEEN RUMBLE STRIPS.

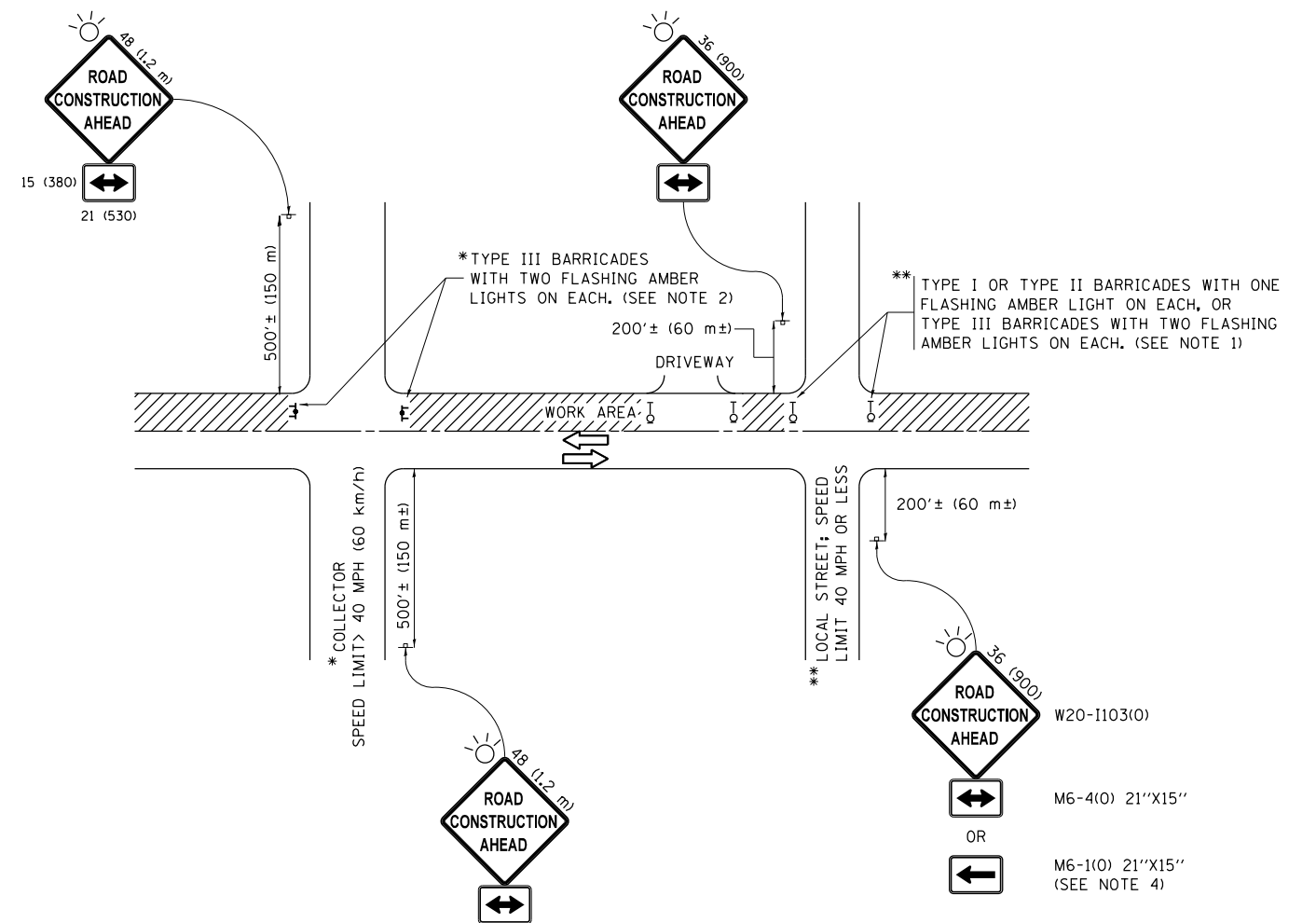
BASIS OF PAYMENT

- THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR CENTERLINE-RUMBLE STRIP OF THE WIDTH SPECIFIED.
- HOT-SPRAY THERMOPLASTIC PAVEMENT MARKING WILL BE USED OVER THE RUMBLE STRIPS, AND WILL BE PAID FOR SEPARATELY.



TWO-WAY ROAD

FILE NAME =	USER NAME = midyja	DESIGNED - R. BORO	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RUMBLE STRIPES FOR CENTERLINE, NON-FREEWAY			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0127318\DRAWING\Design\DistStd.dgn		CHECKED -	REVISED -					337	20RS-10	LAKE	24	17A
PLOT SCALE = 100.0000' / 1in.		DATE - 08-06-2012	REVISED -		BD 55			CONTRACT NO. 62G30				
PLOT DATE = 3/20/2019					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

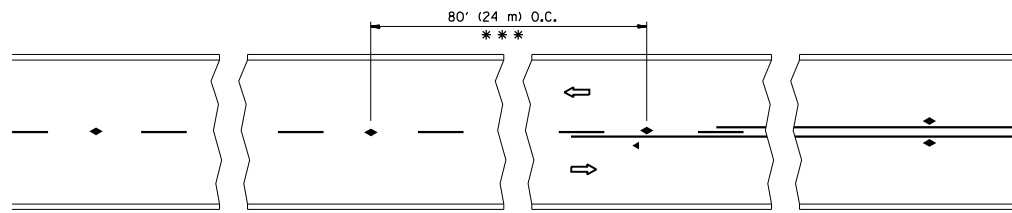
FILE NAME =	USER NAME = hassan	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
pw\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\012731\B\DRAWING\Design\DistStd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 3/20/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

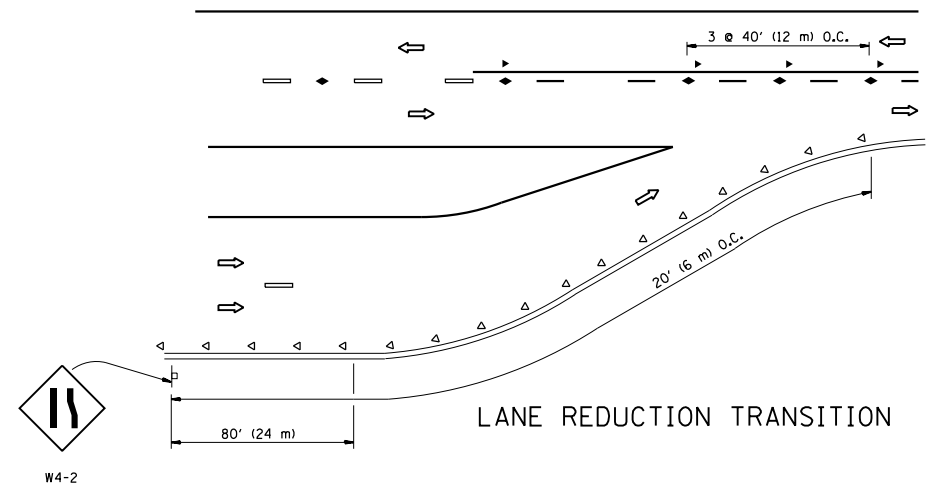
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
337	20RS-10	LAKE	24	18
TC-10			CONTRACT NO. 62G30	
ILLINOIS FED. AID PROJECT				

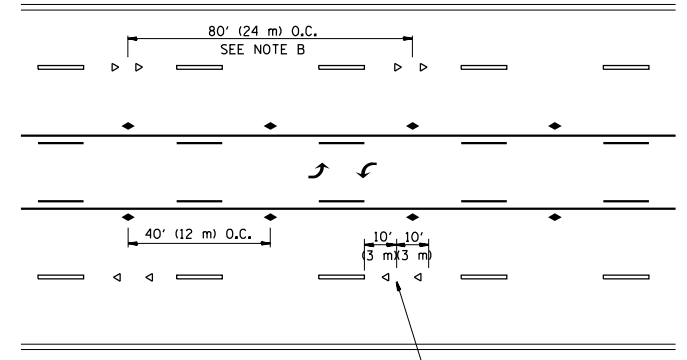


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

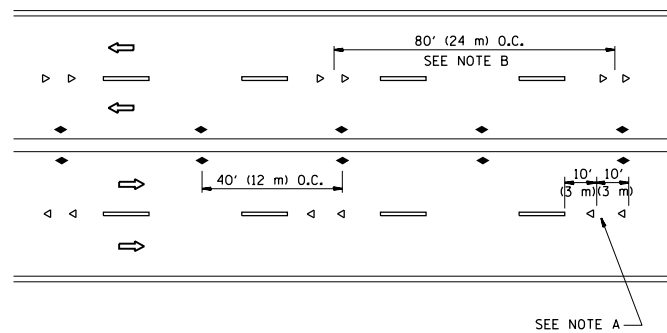
TWO-LANE/TWO-WAY



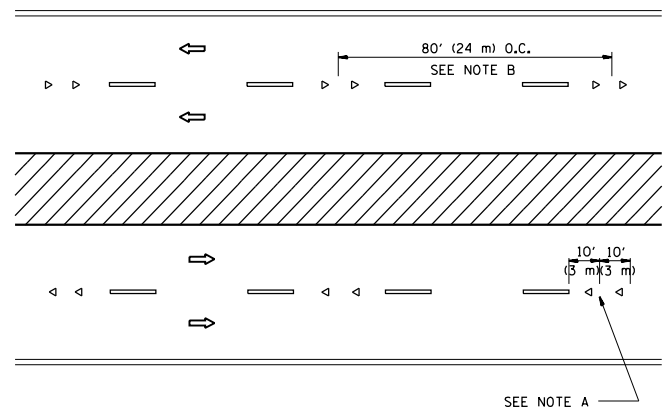
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

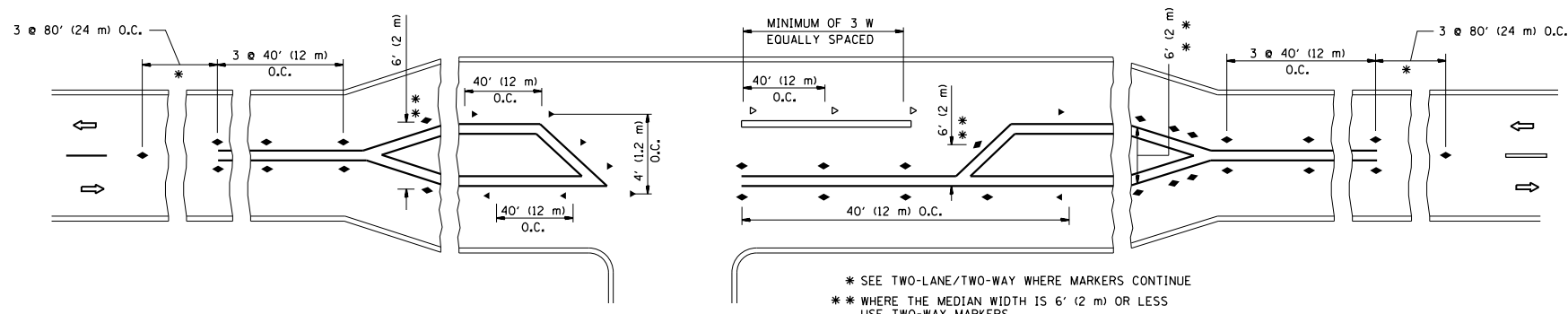
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

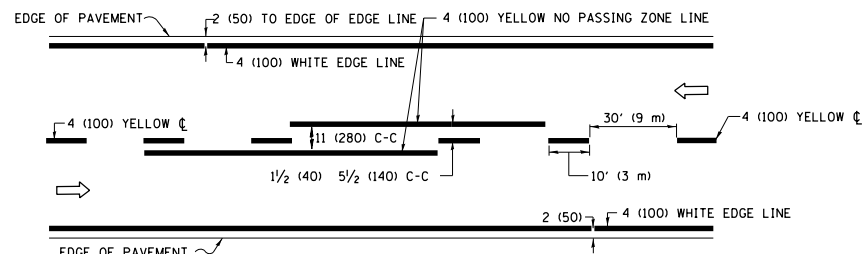


LEFT TURN

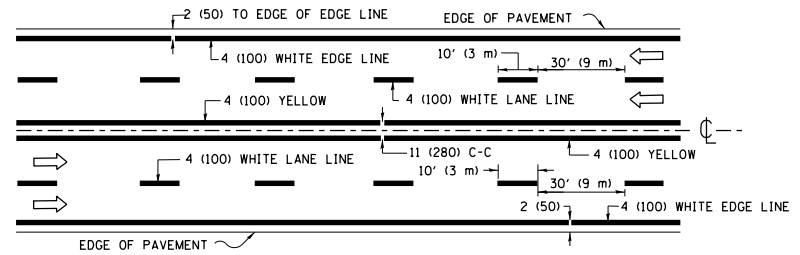
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

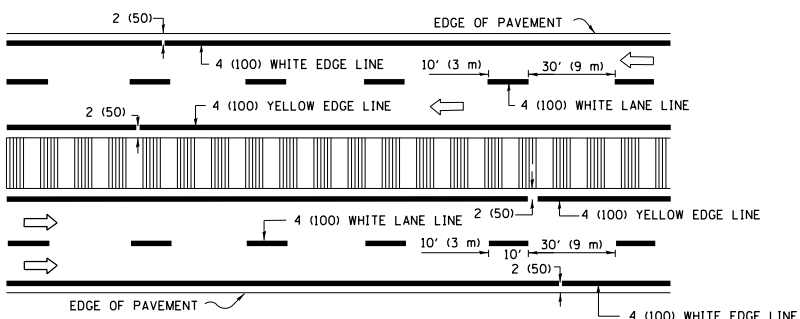
FILE NAME =	USER NAME = hassann	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\212731\B\DRAWING\Design\DistStd.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 03-12-99					337	20RS-10	LAKE	24	19
PLOT DATE = 3/20/2019	DATE -	REVISED - T. RAMMACHER 01-06-00	REVISED - C. JUCIUS 09-09-09		TC-11			CONTRACT NO. 62G30				
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

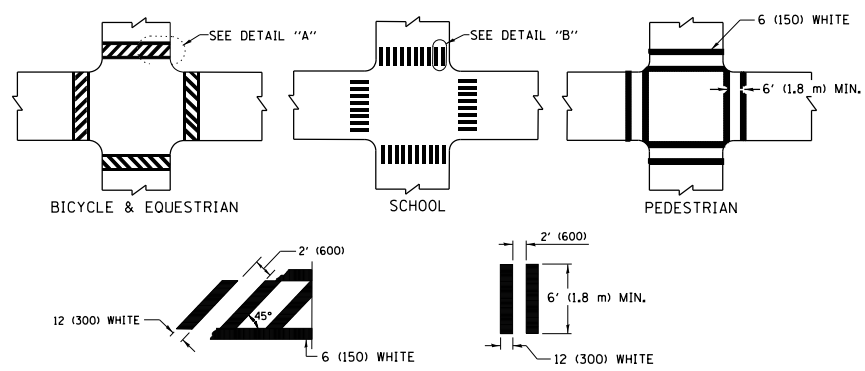


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

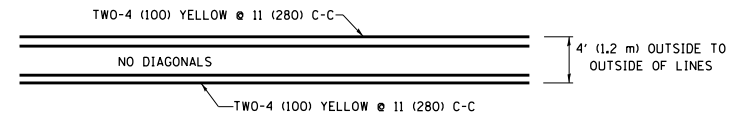


DETAIL "A"

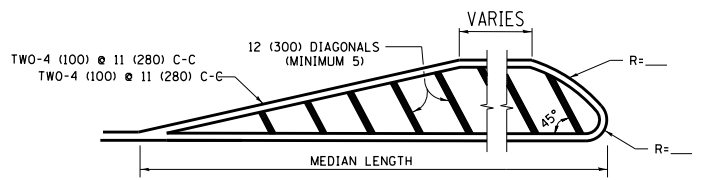
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

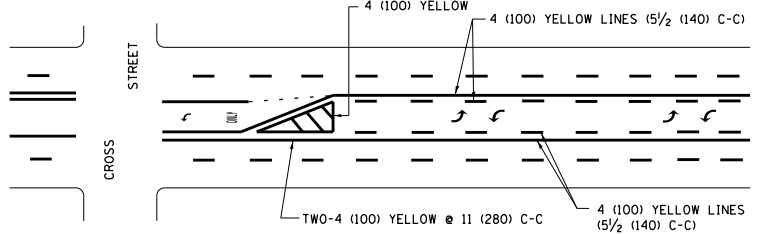


4' (1.2 m) WIDE MEDIANS ONLY

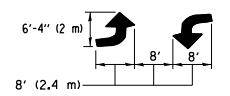


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

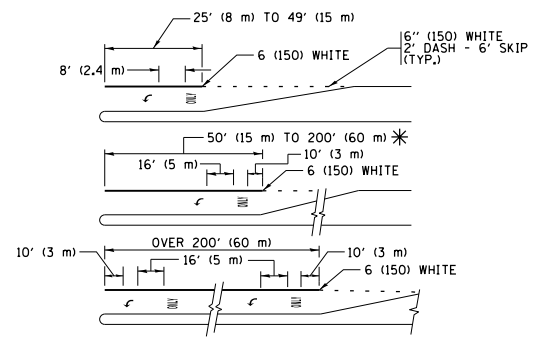


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

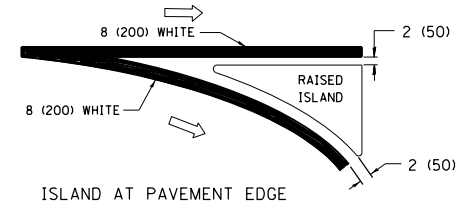
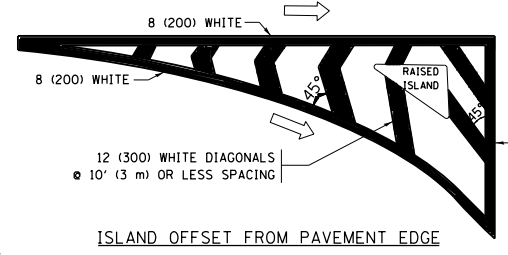


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 * AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

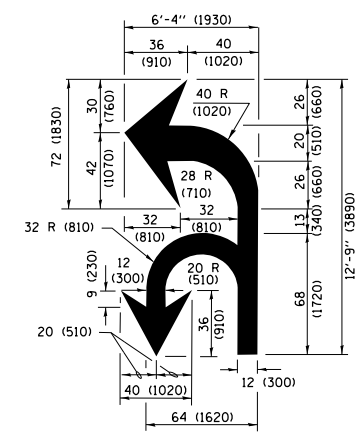
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

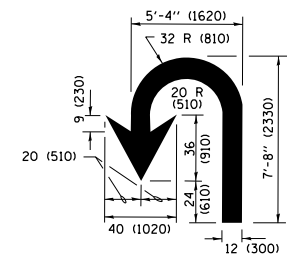
TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = hassann	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\planroom.dot\illinois.gov\PWIDOT\Documents\1007 Offices\District 1\Projects\012731\DRAWING\Design\DistStd.dgn		CHECKED -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 3/20/2019		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
337	20RS-10	LAKE	24	20
TC-13		CONTRACT NO.	62G30	
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

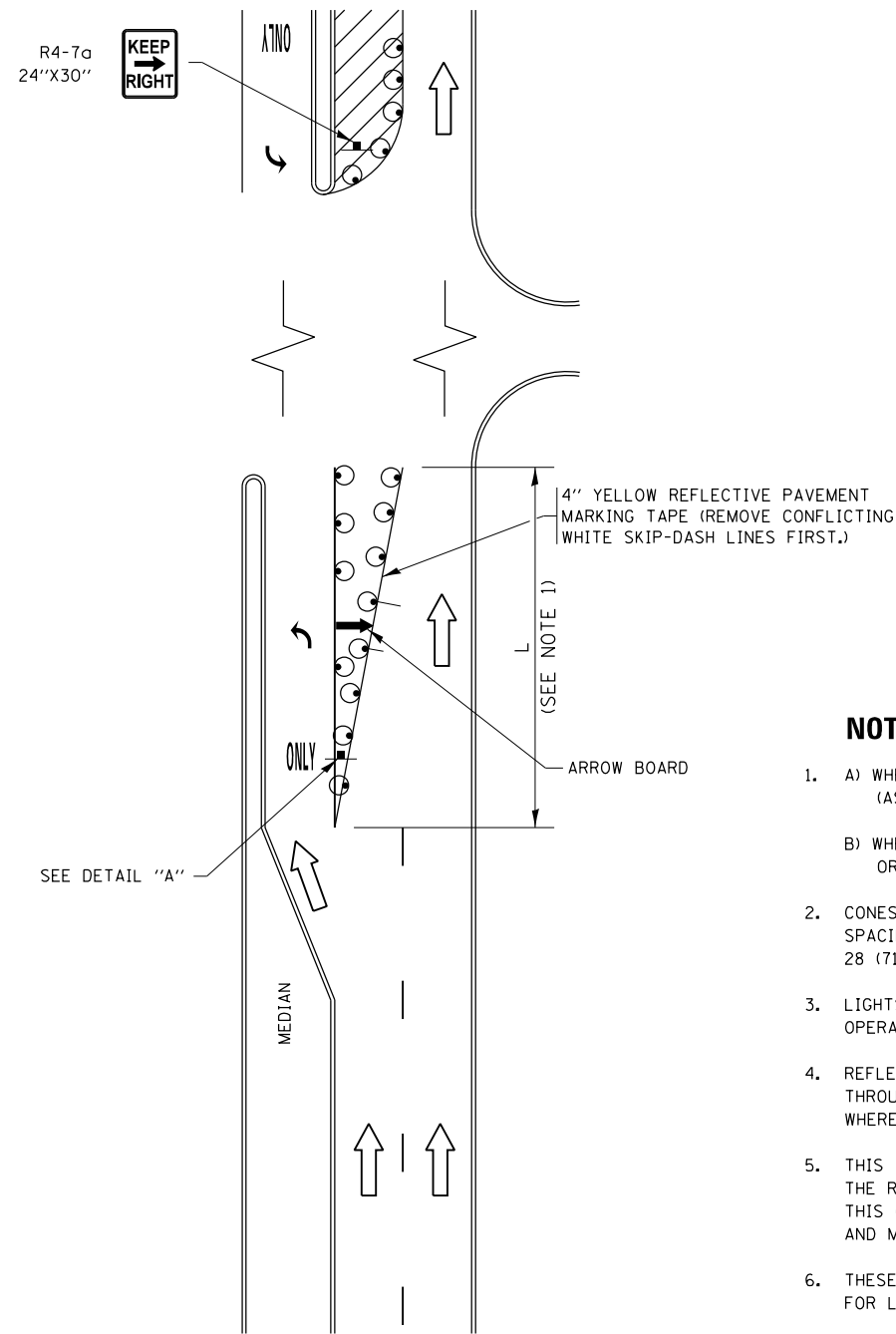


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

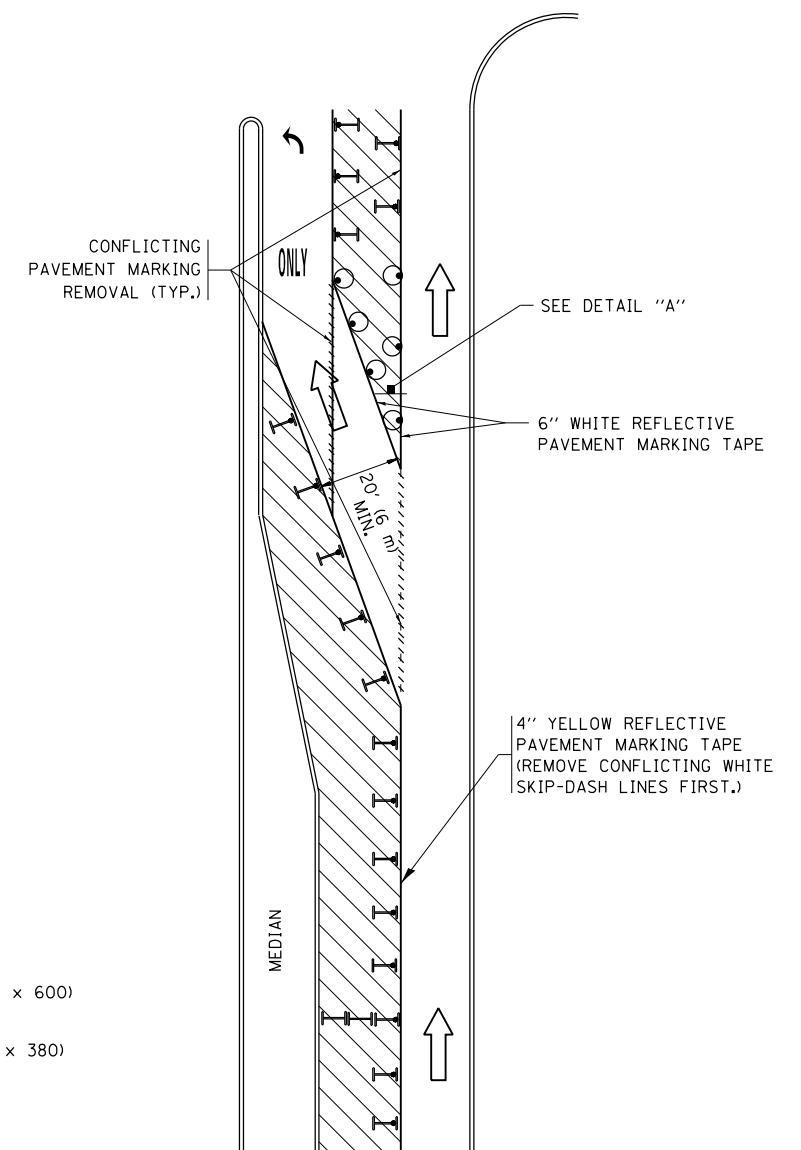
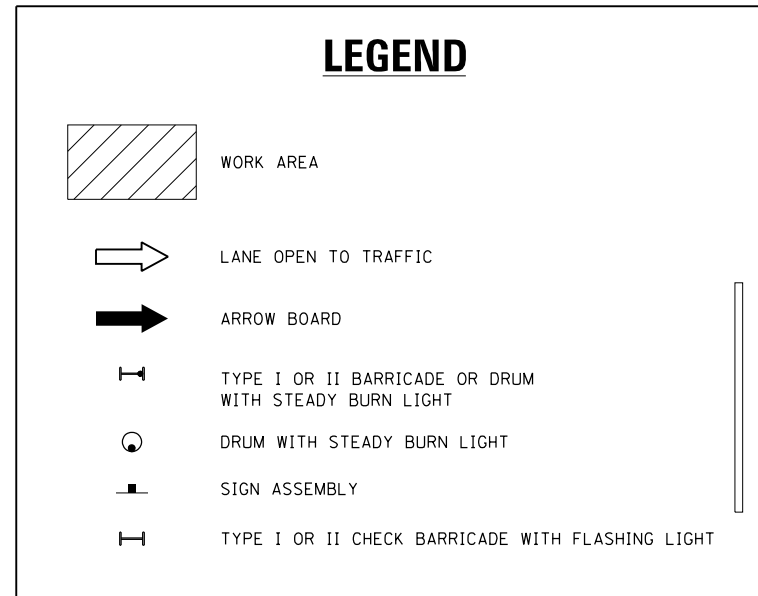
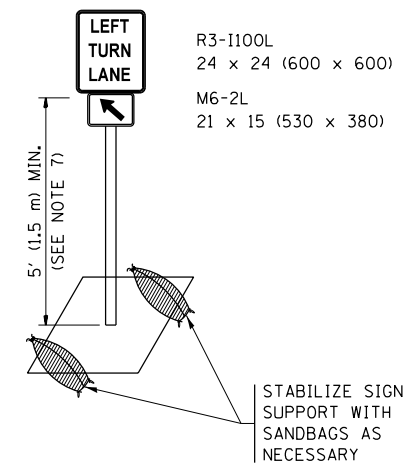


FIGURE 2



NOTES:

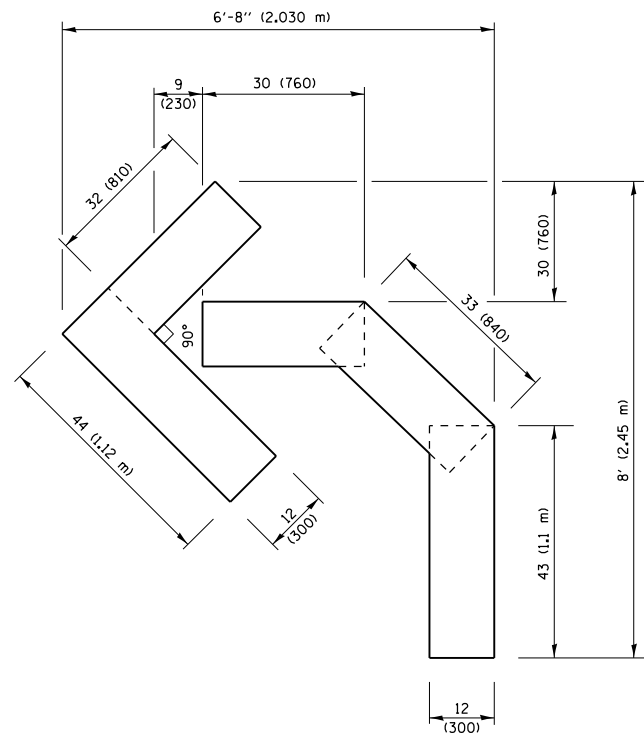
1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

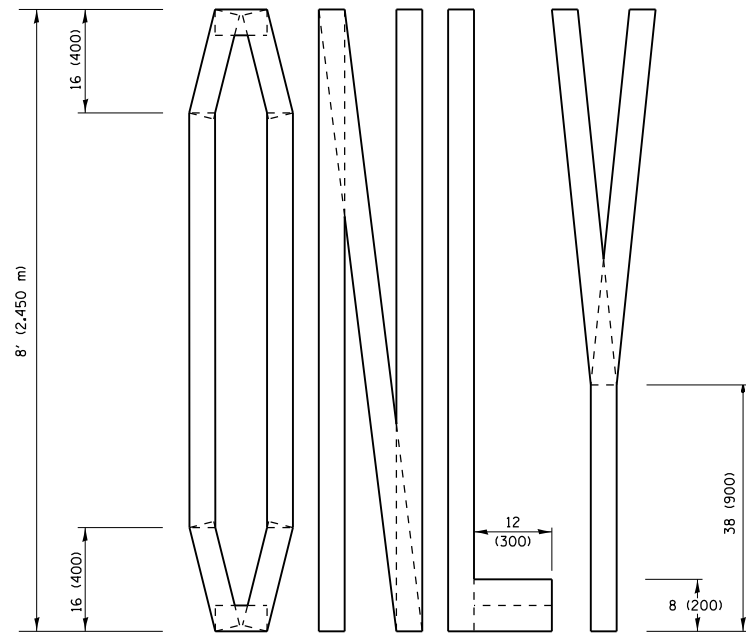
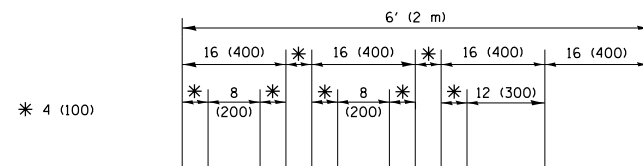
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = hassan	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\planroom.dot.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\0127318\REVISED Design\A1\HOUSEH 11-07-95	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	REVISED - A. SCHUETZE 07-01-13			337	20RS-10	LAKE	24	21
Default	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			TC-14		CONTRACT NO. 62G30		
	PLOT DATE = 3/20/2019	REVISED - T. RAMMACHER 01-06-00	REVISED -			SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.



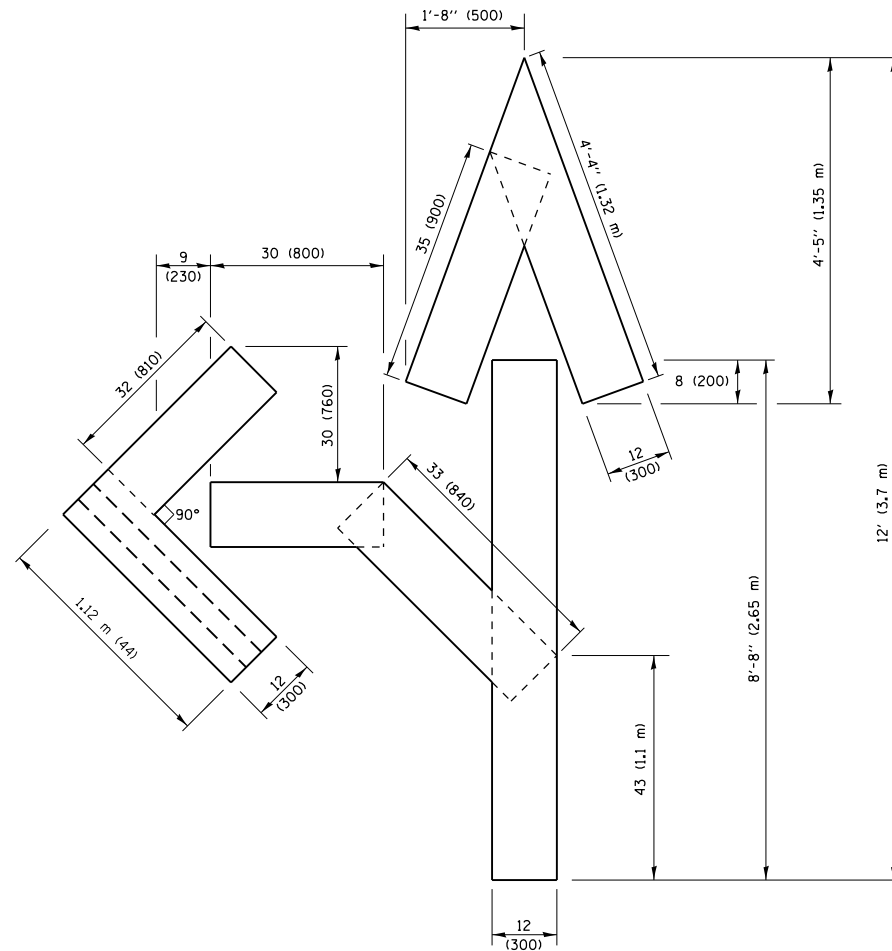
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

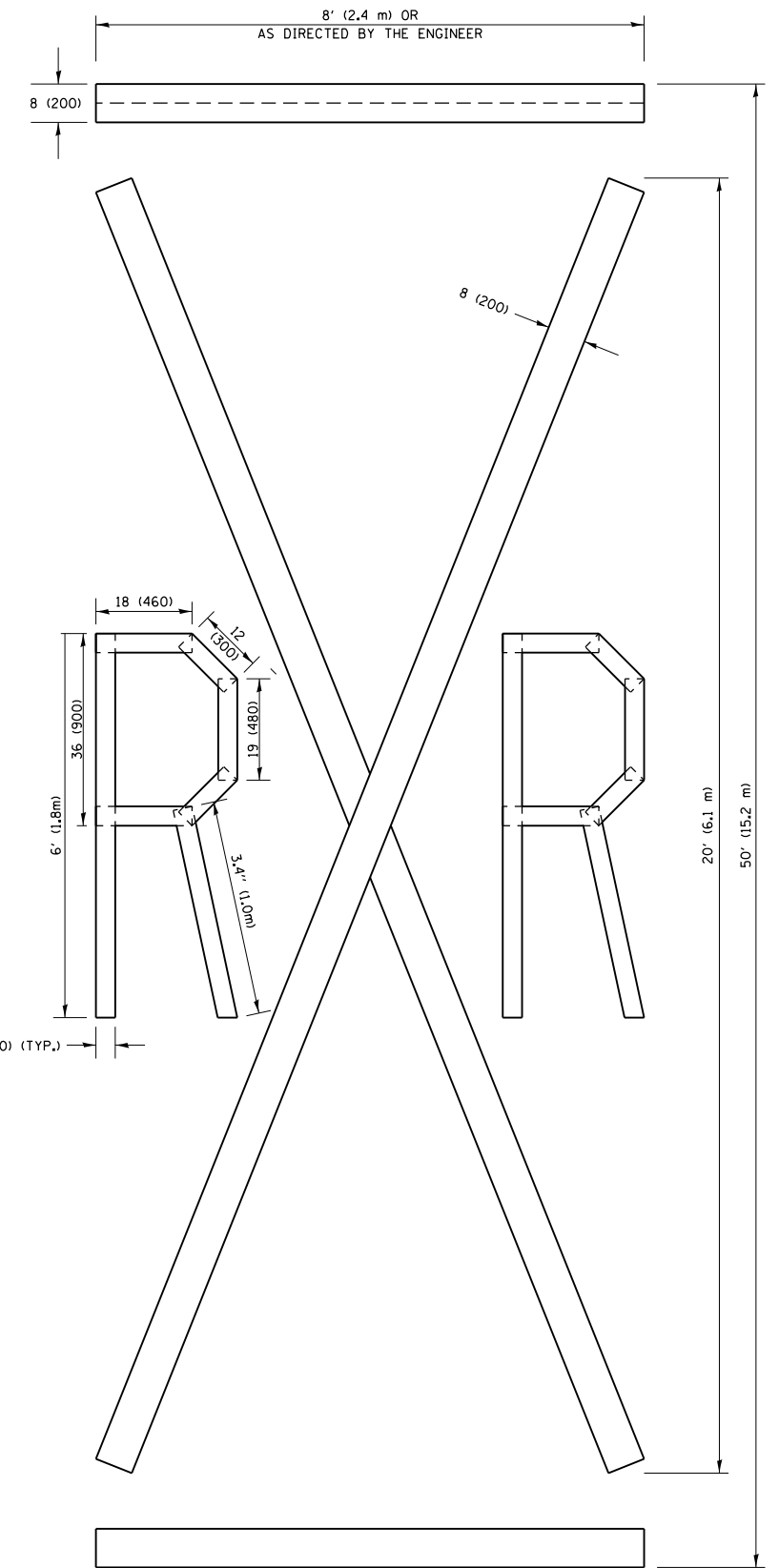


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

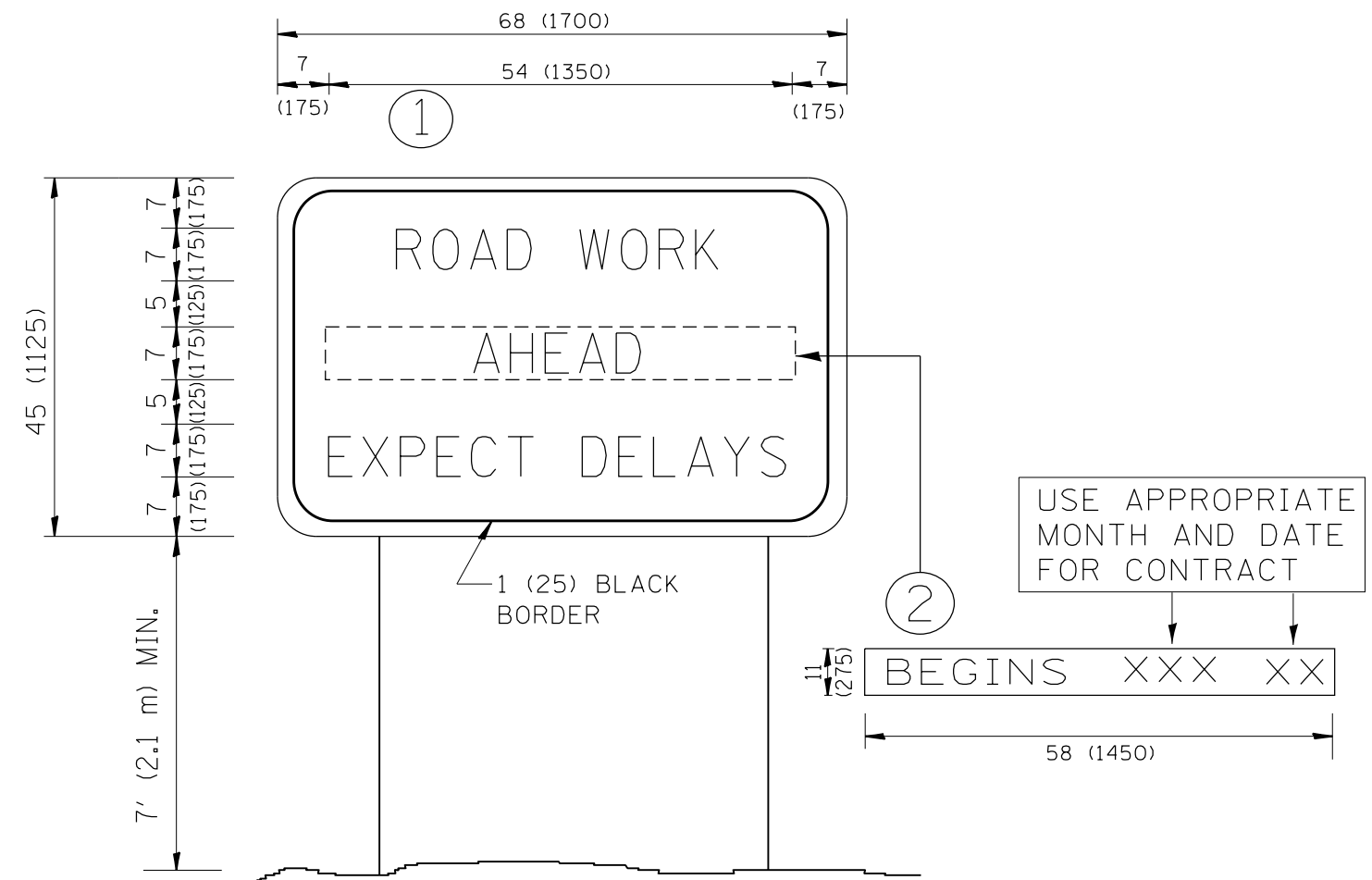
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		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
337	20RS-10	LAKE	24	22
TC-16		CONTRACT NO. 62G30		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = hassann	DESIGNED -	REVISED - R. MIRS 09-15-97
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0127318\DRAWING\Design\DistStd.dgn			REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 3/20/2019	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

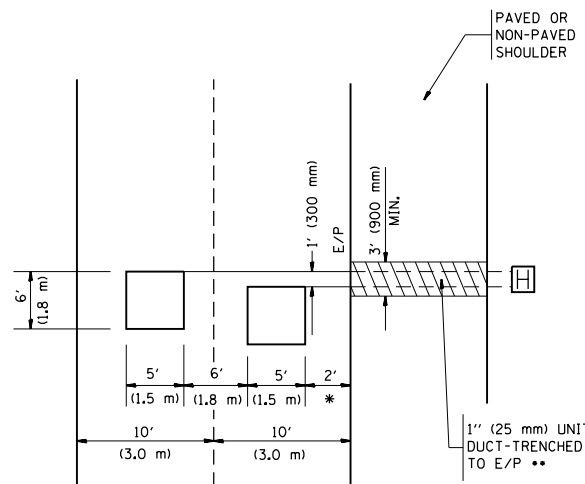
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
337	20RS-10	LAKE	24	23
TC-22		CONTRACT NO. 62G30		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

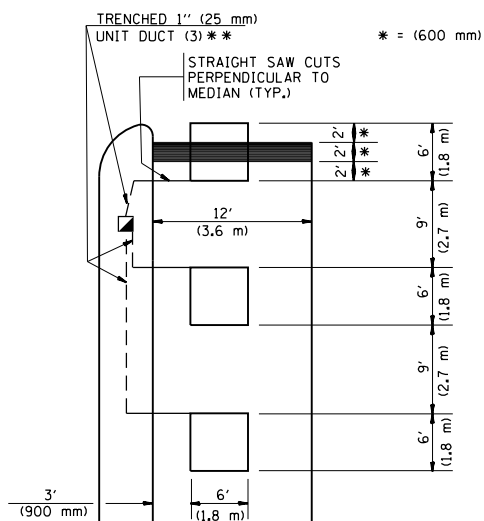


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

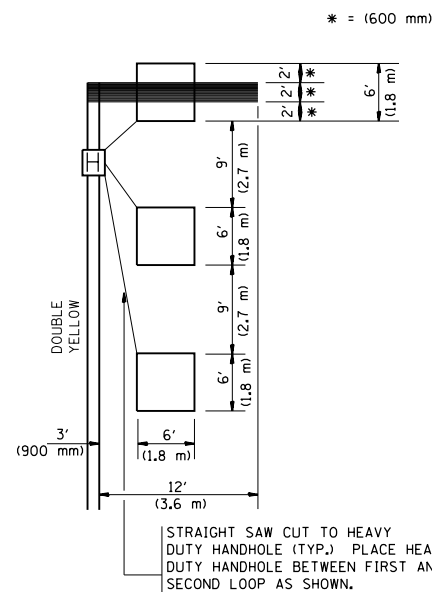
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

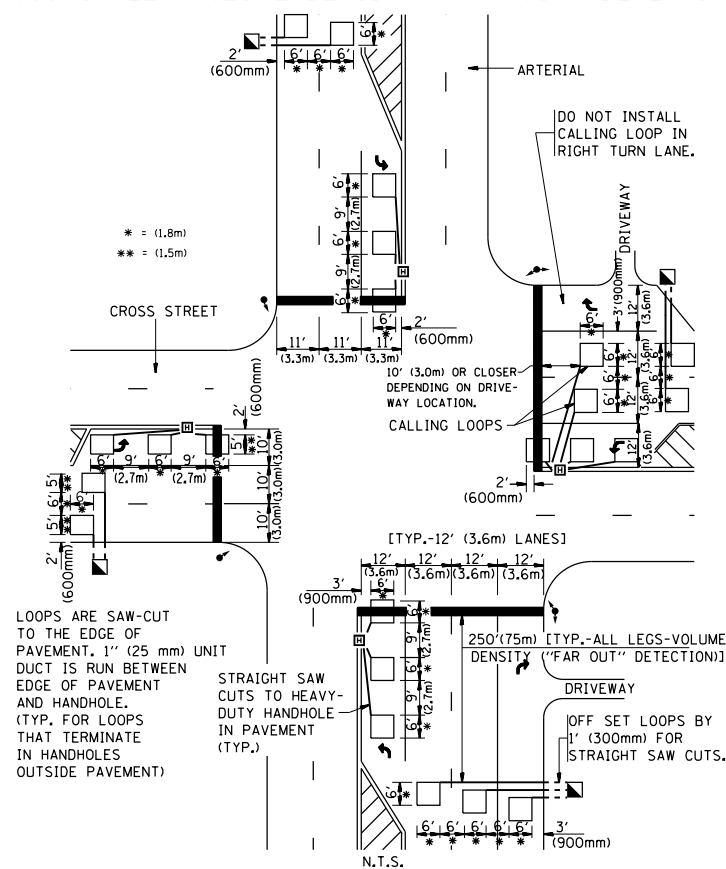
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



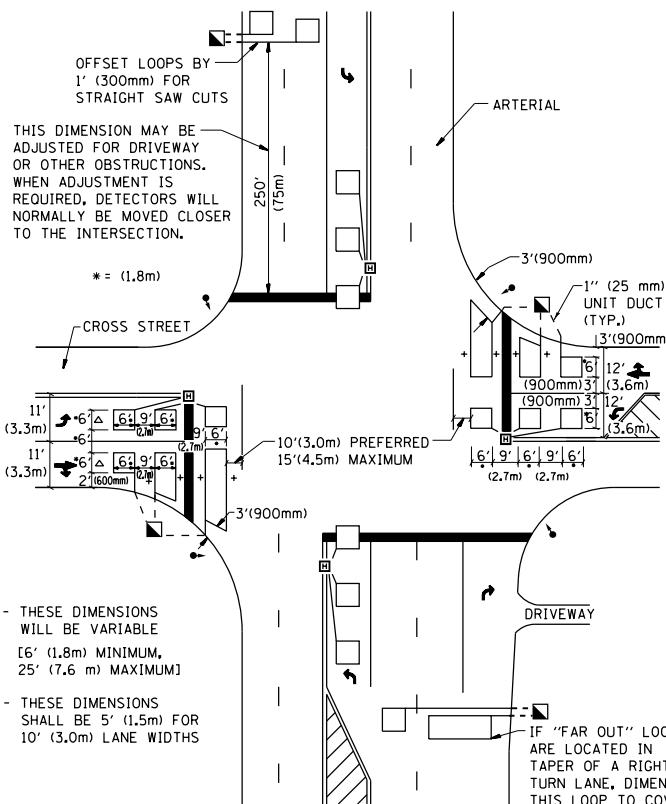
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = hassann	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\012731\BID\Drawn\Design\DistStd.dgn		CHECKED - R.K.F.	REVISED -			337	20RS-10	LAKE	24	24
PLOT SCALE = 100.0000' / 1"		DATE -	REVISED -			TS-07		CONTRACT NO. 62G30		
PLOT DATE = 3/20/2019						SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				