

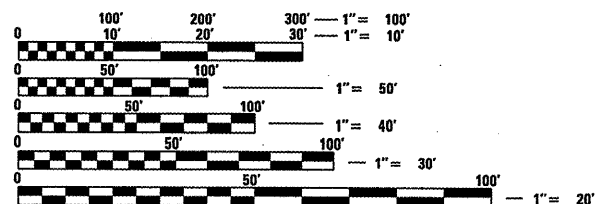
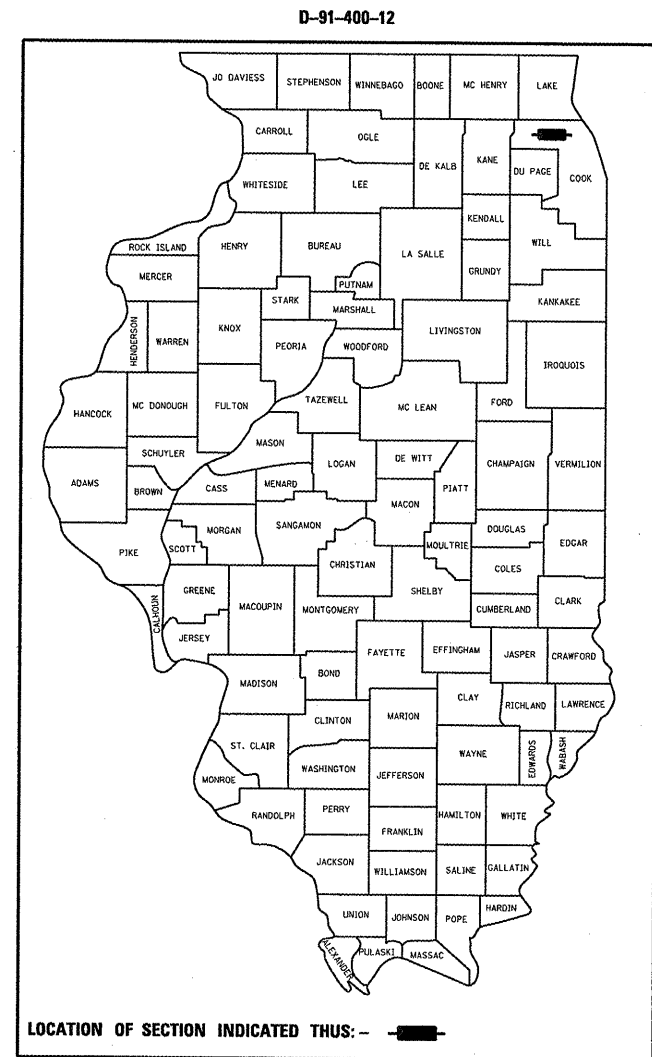
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PROPOSED  
HIGHWAY PLANS**

VARIOUS ROUTES  
SECTION: 2012-016 RS  
VARIOUS LOCATIONS IN NORTHERN COOK COUNTY  
INTERMITTENT RESURFACING  
COOK COUNTY  
C-91-400-12

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2012-016 RS	COOK	33	1
		ILLINOIS	CONTRACT NO. 60T62	

FOR INDEX OF SHEETS, SEE SHEET NO. 2



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: DANIEL WILGREEN (847) 705-4240  
PROJECT MANAGER: KEN ENG (847) 705-4247

CONTRACT NO. 60T62

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED APRIL 4 20 12  
Diane O'Keefe/ASCO  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 11 20 12  
John D. Baumgaller, P.E.  
ENGINEER OF DESIGN AND ENVIRONMENT

May 11 20 12  
William R. Frenzel  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

SHEET NO.	DESCRIPTION	STANDARD NO.	DESCRIPTION
1	TITLE SHEET	000001-06	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	701011-02	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
3	SUMMARY OF QUANTITIES	701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
4	GENERAL LOCATION MAP	701306-03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS - DAY ONLY
5	ROUTE INFORMATION	701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
6	SUMMARY OF PATCHING SCHEDULE	701336-06	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES
7-24	PATCHING SCHEDULE	701421-04	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS ≥ 45 MPH TO 55 MPH
25	BUTT JOINT AND HMA TAPER DETAILS (BD-32)	701426-04	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS
26	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)	701427	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
27	TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)	701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
28	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)	701502-04	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
29	TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)	701601-07	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
30	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)	701602-05	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
31	ARTERIAL ROAD INFORMATION SIGN (TC-22)	701606-08	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
32	STANDARD TRAFFIC SIGNAL DESIGN DETAILS (TS-05, SHEET 1 OF 6)	701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
33	DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING (TS-07)	701901-02	TRAFFIC CONTROL DEVICES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE (OR TOLLWAY) PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (OR ISTHA)

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT MS. PATRICE HARRIS, AREA TRAFFIC FIELD ENGINEER AT (708) 597-9800 MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

THE EXISTING ROADWAY TYPICAL SECTION IS ASSUMED TO HAVE A 3 INCH HOT-MIX ASPHALT OVERLAY ON TOP OF A TEN INCH CONCRETE BASE.

ALL PAVEMENT PATCHES SHOWN IN THE PLANS ARE TWO (2) INCH MILL AND RESURFACE ONLY. THE MINIMUM WIDTH FOR MILLING AND PATCHING SHALL BE THREE (3) FEET.

NO PATCHING OR RESURFACING IS TO BE DONE WITHIN FIFTY (50) FEET OF ANY RAILROAD CROSSING.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING

THE COST OF TRAFFIC CONTROL AND PROTECTION FOR THE PROJECT SHALL BE INCLUDED IN THE COST OF THE ASSOCIATED ROAD WORK.

THE COST OF ANY PARTIAL OR FULL DEPTH PATCHING REQUIRED AFTER THE REMOVAL OF THE EXISTING 2 INCH HOT-MIX ASPHALT SURFACE SHALL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

ANY DETECTOR LOOPS DAMAGED BY MILLING SHALL BE REPLACED IN KIND. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO QUANTIFY LOOP REPLACEMENTS NEEDED AND PROVIDE THE RESIDENT ENGINEER THIS INFORMATION PRIOR TO GRINDING OR REMOVAL.

ALL LOOP DETECTOR LOCATIONS SHALL BE CURB MARKED BY THE CONTRACTOR PRIOR TO MILLING FOR THE PURPOSE OF REESTABLISHING DETECTOR LOOP LAYOUT AFTER THE RESURFACING IS COMPLETED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).

OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS. ANY MILLED PAVEMENT IS TO BE RESURFACED BY THE END OF EACH DAY AND OPEN TO TRAFFIC.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS (%) @ N <sub>DES.</sub>
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5MM), 2"	4% @ 70 GYR

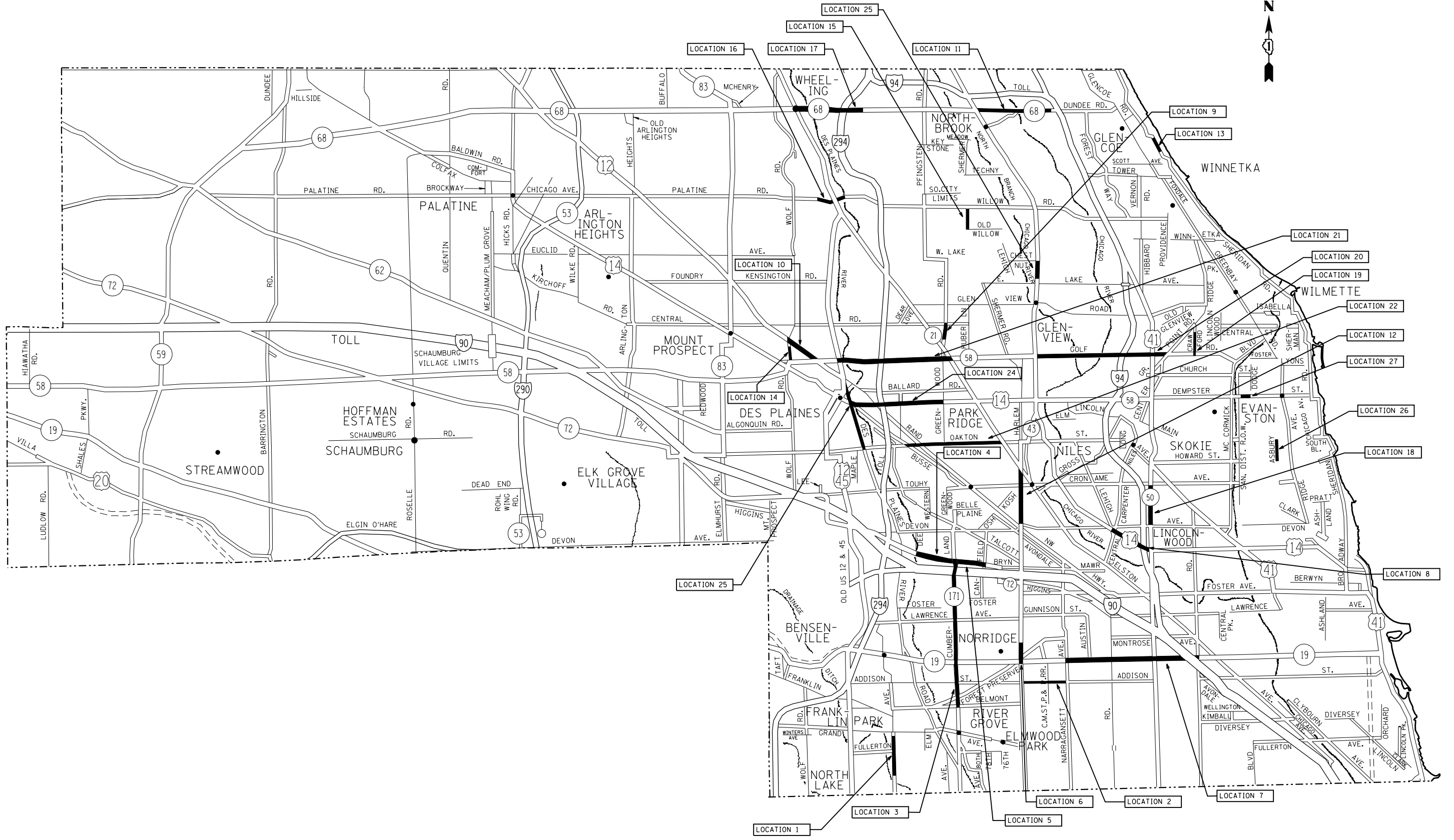
THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

FILE NAME *	USER NAME = pencepl	DESIGNED -	REVISED - PLP 04/30/2012	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw\work\p\dot\pencepl\d0303692\014012-Design.dgn	DRAWN -	REVISED -	VAR.			2012-016 RS	COOK	33	2	
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 60T62							
PLOT DATE = 4/30/2012	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
SCALE:						SHEET	OF	SHEETS	STA.	TO STA.

Rev.

SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE 0005						CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE 0005					
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	16	16						* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	720	720					
40600300	AGGREGATE (PRIME COAT)	TON	77	77						* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	100	100					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	58	58						* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	685	685					
40600895	CONSTRUCTING TEST STRIP	EACH	1	1						* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	235	235					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	1150	1150						* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	987	987					
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	4295	4295						78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	987	987					
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SO YD	38340	38340						* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	980	980					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6						Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	1388	1388					
67100100	MOBILIZATION	L SUM	1	1															
70300100	SHORT TERM PAVEMENT MARKING	FOOT	1290	1290															
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	430	430															
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	300	300															
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	13465	13465						* SPECIALTY ITEM									



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		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>GENERAL LOCATION MAP</b>			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2012-016 R5	COOK	33	4
CONTRACT NO. 60T62			ILLINOIS FED. AID PROJECT	

	SUMMARY - NORTHERN COOK COUNTY ROUTES	MUNICIPALITIES	SPEED LIMIT	ADT (YEAR)
LOC. 1	25TH AVE. (JOHANNA AVE. TO MCLEAN AVE.)	FRANKLIN PARK, LEYDEN TWP.	35 MPH	17,700 (2010)
LOC. 2	ADDISON ST. (HARLEM AVE. TO NATOMA AVE.)	CHICAGO	30 MPH	17,900 (2010)
LOC. 3	IL 171 (BELMONT AVE. TO HIGGINS RD.)	CHICAGO, NORRIDGE, RIVER GROVE	35 MPH	34,800 (2011)
LOC. 4	IL 72 (DEE RD. TO COMBERLAND AVE.)	PARK RIDGE, CHICAGO	35 MPH	23,500 (2009)
LOC. 5	IL 72 (CUMBERLAND AVE. TO CANFIELD AVE.)	PARK RIDGE, CHICAGO	35 MPH	20,900 (2011)
LOC. 6	IL 43 (IRVING PARK RD. TO CULLOM AVE.)	NORRIDGE, CHICAGO	30 MPH	26,800 (2011)
LOC. 7	IL 19 (NARRAGANSETT AVE. TO PULASKI RD.)	CHICAGO	30 MPH	28,600 (2011)
LOC. 8	CALDWELL AVE. (PETERSON AVE. TO CENTRAL AVE.)	CHICAGO	30-35 MPH	25,400 (2011)
LOC. 9	GREENWOOD RD. (CENTRAL RD. TO RONALD RD.)	GLENVIEW, MAINE TWP.	35 MPH	12,700 (2010)
LOC. 10	RAND RD. (GOLF RD. TO WOLF RD.)	DES PLAINES	40 MPH	20,600 (2011)
LOC. 11	DUNDEE RD. (WAUKEGAN RD. TO SKOKIE BLVD.)	NORTHBROOK	35-40 MPH	34,600 (2011)
LOC. 12	HARLEM AVE. (MILWAUKEE AVE. TO DEVON AVE.)	NILES, CHICAGO	35 MPH	35,900 (2009)
LOC. 13	SHERIDAN RD. (HARBOR ST. TO WOODLAWN AVE.)	GLENCOE	30 MPH	4,100 (2010)
LOC. 14	WOLF RD. (GOLF RD. TO PRINCETON ST.)	DES PLAINES	30 MPH	17,100 (2010)
LOC. 15	SHERMER RD. (WILLOW RD. TO OLD WILLOW RD.)	NORTHBROOK, GLENVIEW	35 MPH	5,200 (2010)
LOC. 16	PALATINE RD. RAMPS AT MILWAUKEE AVE.	PROSPECT HEIGHTS	55 MPH	45,700 (2010)
LOC. 17	DUNDEE RD. (SANDERS RD. TO MILWAUKEE AVE.)	NORTHBROOK, WHEELING	45-50 MPH	31,300 (2009)
LOC. 18	SKOKIE BLVD. (TOUHY AVE. TO DEVON AVE.)	LINCOLNWOOD, SKOKIE, CHICAGO	40 MPH	17,900 (2011)
LOC. 19	CRAWFORD AVE. (GOLF RD. TO HARRISON ST.)	SKOKIE, EVANSTON	35 MPH	17,100 (2010)
LOC. 20	GOLF RD. (WAUKEGAN RD. TO GROSS POINT RD.)	GLENVIEW, MORTON GROVE, GOLF, SKOKIE	35-45 MPH	36,100 (2010)
LOC. 21	GOLF RD. (MILWAUKEE AVE. TO EAST RIVER RD.)	NILES, DES PLAINES, MAINE TWP.	35-45 MPH	37,800 (2011)
LOC. 22	OAKTON ST. (GREENWOOD RD. TO BUSSE RD.)	PARK RIDGE	35 MPH	25,600 (2010)
LOC. 23	RIVER RD. (OAKTON ST. TO MINER ST.)	DES PLAINES	35 MPH	21,200 (2010)
LOC. 24	DEMPSTER ST. (GREENWOOD RD. TO RIVER RD.)	DES PLAINES, NILES	25-35 MPH	41,500 (2009)
LOC. 25	WAUKEGAN RD. (CHESTNUT AVE. TO WOODLAWN AVE.)	GLENVIEW	35 MPH	26,500 (2011)
LOC. 26	ASBURY AVE. (OAKTON ST. TO HOWARD ST.)	EVANSTON, CHICAGO	30 MPH	12,300 (2010)
LOC. 27	DEMPSTER ST. (FOWLER AVE. TO DODGE AVE.)	EVANSTON	30 MPH	18,500 (2010)

	SUMMARY -NORTHERN COOK COUNTY ROUTES	HMA 2" MILL & RESURFACE (SY)
LOC. 1	25TH AVE. (JOHANNA AVE. TO MCLEAN AVE.)	2059
LOC. 2	ADDISON ST. (HARLEM AVE. TO NATOMA AVE.)	4501
LOC. 3	IL 171 (BELMONT AVE. TO HIGGINS RD.)	5772
LOC. 4	IL 72 (DEE RD. TO COMBERLAND AVE.)	565
LOC. 5	IL 72 (CUMBERLAND AVE. TO CANFIELD AVE.)	752
LOC. 6	IL 43 (IRVING PARK RD. TO CULLOM AVE.)	873
LOC. 7	IL 19 (NARRAGANSETT AVE. TO PULASKI RD.)	2165
LOC. 8	CALDWELL AVE. (PETERSON AVE. TO CENTRAL AVE.)	1133
LOC. 9	GREENWOOD RD. (CENTRAL RD. TO RONALD RD.)	300
LOC. 10	RAND RD. (GOLF RD. TO WOLF RD.)	1567
LOC. 11	DUNDEE RD. (WAUKEGAN RD. TO SKOKIE BLVD.)	2262
LOC. 12	HARLEM AVE. (MILWAUKEE AVE. TO DEVON AVE.)	1306
LOC. 13	SHERIDAN RD. (HARBOR ST. TO WOODLAWN AVE.)	711
LOC. 14	WOLF RD. (GOLF RD. TO PRINCETON ST.)	1667
LOC. 15	SHERMER RD. (WILLOW RD. TO OLD WILLOW RD.)	346
LOC. 16	PALATINE RD. RAMPS AT MILWAUKEE AVE.	567
LOC. 17	DUNDEE RD. (SANDERS RD. TO MILWAUKEE AVE.)	937
LOC. 18	SKOKIE BLVD. (TOUHY AVE. TO DEVON AVE.)	570
LOC. 19	CRAWFORD AVE. (GOLF RD. TO HARRISON ST.)	673
LOC. 20	GOLF RD. (WAUKEGAN RD. TO GROSS POINT RD.)	1067
LOC. 21	GOLF RD. (MILWAUKEE AVE. TO EAST RIVER RD.)	689
LOC. 22	OAKTON ST. (GREENWOOD RD. TO BUSSE RD.)	1417
LOC. 23	RIVER RD. (OAKTON ST. TO MINER ST.)	1949
LOC. 24	DEMPSTER ST. (GREENWOOD RD. TO RIVER RD.)	2117
LOC. 25	WAUKEGAN RD. (CHESTNUT AVE. TO WOODLAWN AVE.)	1137
LOC. 26	ASBURY AVE. (OAKTON ST. TO HOWARD ST.)	800
LOC. 27	DEMPSTER ST. (FOWLER AVE. TO DODGE AVE.)	438
	<b>NORTH COOK COUNTY TOTAL =</b>	<b>38340</b>
		<b>SY</b>

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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 4/10/2012	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF PATCHING SCHEDULE			
SHEET	OF	SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2012-016 R5	COOK	33	6
CONTRACT NO. 60T62				
ILLINOIS FED. AID PROJECT				







ROUTE: Addison St. (Harlem Ave. to Natoma Ave.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Harlem Ave.		EB	2	7	35	245	27
		EB	3	8	17	136	15
		EB	EP	22	5	110	12
		EB	EP	7	150	1050	117
		EB	EP	22	5	110	12
		EB	EP	22	5	110	12
		EB	EP	7	25	175	19
		EB	1	8	20	160	18
		EB	EP	11	165	1815	202
		EB	EP	7	95	665	74
		EB	EP	20	6	120	13
		EB	EP	20	5	100	11
		EB	EP	20	5	100	11
		EB	EP	20	5	100	11
		EB	EP	20	5	100	11
		EB	EP	20	5	100	11
		EB	EP	7	65	455	51
		EB	EP	7	125	875	97
	Natoma Ave.	EB	EP	7	125	875	97

ROUTE: Addison St. (Harlem Ave. to Natoma Ave.) (Continued)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Harlem Ave.		WB	EP	7	200	1400	156
		WB	EP	14	20	280	31
		WB	EP	5	200	1000	111
		WB	EP	5	65	325	36
		WB	EP	5	65	325	36
		WB	EP	10	110	1100	122
		WB	EP	5	150	750	83
		WB	EP	5	100	500	56
		WB	EP	5	100	500	56
		WB	EP	5	100	500	56
		WB	EP	5	70	350	39
		WB	1,2.	20	6	120	13
		WB	1,2.	20	5	100	11
		WB	EP	3	150	450	50
		WB	1	10	5	50	6
		WB	1,2.	20	100	2000	222
		WB	1,2.	20	5	100	11
		WB	EP	5	20	100	11
		WB	EP	6	40	240	27
		WB	EP	3	90	270	30
		WB	1,2.	20	6	120	13
		WB	2	6	20	120	13
		WB	EP	20	5	100	11
		WB	1, 2	10	25	250	28
	Natoma Ave.	WB	1, 2.	4	200	800	89
Harlem Ave.		EB	2	20	15	300	33
		EB	2	5	50	250	28
		EB	2	20	15	300	33
		EB	2	20	115	2300	256
		EB	2	10	50	500	56
		EB	EP	10	140	1400	156
		EB	1	10	80	800	89
		EB	1,2.	10	40	400	44
		EB	1, 2.	15	90	1350	150
		EB	1, 2.	4	95	380	42
		EB	1, 2.	3	35	105	12
		EB	1, 2.	15	40	600	67
		EB	2,3.	20	25	500	56
		EB	1, 2.	20	200	4000	444
		EB	EP	20	20	400	44
		EB	1, 2.	22	60	1320	147
		EB	1, 2.	250	5	1250	139
		EB	1, 2.	250	5	1250	139
		EB	EP	250	5	1250	139
		EB	1,2.	4	200	800	89
		EB	EP	10	90	900	100
		EB	EP	4	100	400	44
	Natoma Ave.	EB	EP	4	100	400	44
		<b>TOTALS:</b>				<b>4305</b>	<b>4501</b>
						<b>FT</b>	<b>SY</b>

ROUTE: IL 171 (Belmont Ave. to Higgins Rd.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Higgins		SB	1,2	30	10	300	33
		SB	2	4	70	280	31
		SB	2	4	80	320	36
		SB	1	7	25	175	19
		SB	1,2,3	40	10	400	44
		SB	2	12	12	144	16
		SB	1,2	10	30	300	33
		SB	1,2,3	12	60	720	80
		SB	L	12	12	144	16
		SB	L,1	30	10	300	33
		SB	L,L,1	36	10	360	40
		SB	L,L,1,2,3	70	12	840	93
I-90 Ramp	I-90 Ramp	SB	L,L	12	12	144	16
		SB	1,2,3	36	10	360	40
		SB	3	12	100	1200	133
		SB	2	4	150	600	67
		SB	1,2	24	10	240	27
		SB	3	4	120	480	53
		SB	3,R	28	10	280	31
		SB	3,R	28	12	336	37
		SB	3	12	80	960	107
		SB	L	4	220	880	98
		SB	R	12	12	144	16
		SB	R	12	12	144	16
Gregory	Gregory	SB	R	12	12	144	16
		SB	1,2,3	10	36	360	40
		SB	1,2,3	36	10	360	40
		SB	1,2,3	10	36	360	40
		SB	1,2	10	24	240	27
Catherine	Catherine	SB	1,2	10	24	240	27
		SB	1,2	10	24	240	27
		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	1,2	4	200	800	89
		SB	1	10	12	120	13
		SB	2	4	300	1200	133
		SB	1	12	12	144	16
		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	L,1,2	36	10	360	40
Foster	Foster	SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	1,2	10	24	240	27
		SB	1,2	4	220	880	98
		SB	2	12	12	144	16
		SB	2	10	12	120	13
		SB	2	12	12	144	16
		SB	1	12	12	144	16
		SB	1	12	12	144	16
		SB	2	12	12	144	16
		SB	2	4	60	240	27
		SB	2	12	12	144	16
Lawrence	Lawrence	SB	1,2	10	24	240	27
		SB	1,2	12	24	288	32
		SB	1,2	10	24	240	27
		SB	1,2	18	24	432	48
		SB	2	12	12	144	16
		SB	2	12	40	480	53
		SB	1,2	12	20	240	27
	Montrose	SB	L,1	4	100	400	44

ROUTE: IL 171 (Belmont Ave. to Higgins Rd.) (Continued)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Montrose		SB	1,2	10	24	240	27
		SB	2	10	12	120	13
		SB	2	10	12	120	13
		SB	2	10	12	120	13
		SB	2	10	12	120	13
		SB	2	10	12	120	13
		SB	1,2	4	310	1240	138
		SB	2	12	12	144	16
		SB	2	10	12	120	13
		SB	2	12	12	144	16
		SB	2	12	12	144	16
	Irving Park	SB	1,2	4	80	320	36
Irving Park	Irving Park	SB	1,2,3	10	36	360	40
		SB	1	10	12	120	13
		SB	2,3	10	24	240	27
		SB	1	10	24	240	27
		SB	L,1	4	220	880	98
		SB	L,1	4	200	800	89
		SB	1	24	10	240	27
		SB	1,2	24	10	240	27
		SB	2	12	12	144	16
	Addison	SB	1	12	10	120	13
Addison	Addison	SB	2	12	12	144	16
		SB	2	10	12	120	13
		SB	2	10	12	120	13
		SB	1,2	10	24	240	27
		SB	1	12	12	144	16
	Belmont	SB	1	12	10	120	13
Belmont	Belmont	NB	1	12	10	120	13
		NB	1	12	10	120	13
		NB	1,2	24	10	240	27
		NB	1,2	24	10	240	27
		NB	2	12	12	144	16
		NB	2	12	12	144	16
		NB	1	12	40	480	53
		NB	1,2	12	24	288	32
		NB	1,2	12	24	288	32
	Addison	NB	1,2	24	12	288	32
Addison	Addison	NB	1,2	24	10	240	27
		NB	1,2	24	10	240	27
		NB	1	12	50	600	67
		NB	1,2	24	12	288	32
		NB	1,2	24	12	288	32
		NB	2	4	140	560	62
		NB	2	7	30	210	23
		NB	1	7	12	84	9
		NB	1,2	12	24	288	32
		NB	2	4	150	600	67
		NB	1	10	12	120	13
		NB	1	7	12	84	9
		NB	1,2	12	10	120	13
		NB	L	12	12	144	16
		NB	L,1,2,R	48	12	576	64
	Irving Park	NB	1,2	24	12	288	32
Irving Park	Irving Park	NB	1,2	24	10	240	27
		NB	1	4	80	320	36
		NB	1	4	120	480	53
		NB	1,2	24	12	288	32
		NB	1,2	24	10	240	27
		NB	1	12	7	84	9
		NB	1	12	7	84	9
		NB	1,2	4	250	1000	111
	Montrose	NB	L,1	4	180	720	80

Continued on next sheet

FILE NAME =	USER NAME = chrzesclr	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PATCHING SCHEDULE IL 171</b>				F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
ct:\pw\work\p\id\chrzesclr\0303692\010012-Design.dgn		DRAWN -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	VAR.	2012-016 R5	COOK	33	10
		CHECKED -	REVISED -		CONTRACT NO. 60T62											
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT											



ROUTE: IL 72 / Higgins Rd. (Cumberland Ave. to Canfield Ave.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Canfield		WB	2	7	12	84	9
		WB	2	7	12	84	9
		WB	2	7	12	84	9
		WB	2	7	12	84	9
		WB	1	7	12	84	9
		WB	1	4	150	600	67
		WB	2	7	10	70	8
		WB	1	7	12	84	9
	Cumberland	WB	1 & 2	7	12	84	9
Cumberland		EB	2	7	12	84	9
		EB	2	7	12	84	9
		EB	2	4	110	440	49
		EB	2	12	12	144	16
		EB	2	4	80	320	36
		EB	2	4	40	160	18
		EB	2	4	30	120	13
		EB	1 & 2	24	10	240	27
		EB	2	7	12	84	9
		EB	2	12	110	1320	147
		EB	1	7	12	84	9
		EB	1	7	12	84	9
		EB	1	7	12	84	9
		EB	1 & 2	20	70	1400	156
		EB	2	4	80	320	36
		EB	2	4	110	440	49
	Canfield	EB	2	7	15	105	12
		<b>TOTALS:</b>				<b>983</b>	<b>752</b>
						<b>FT</b>	<b>SY</b>

ROUTE: IL 43 / Harlem Ave. (Irving Park Rd. to Cullom Ave.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Irving Park Road		NB	2	4	30	120	13
		NB	1 & 2	22	70	1540	171
		NB	1	11	40	440	49
	Forest Preserve Drive	NB	2	7	100	700	78
Forest Preserve Drive		NB	1	12	6	72	8
		NB	1 & LT	15	12	180	20
		NB	1 & LT	22	12	264	29
		NB	1 & LT	24	12	288	32
		NB	1 & 2	24	10	240	27
		NB	2	4	20	80	9
		NB	1 & 2	24	10	240	27
		NB	1 & 2	24	6	144	16
		NB	2	4	40	160	18
		NB	1 & LT	21	10	210	23
		NB	2	4	50	200	22
		NB	1 & 2	12	6	72	8
		NB	2	4	40	160	18
		NB	1	11	60	660	73
	Cullom	NB	1 & 2	22	7	154	17
Cullom		SB	1	7	100	700	78
		SB	2	4	30	120	13
		SB	1 & 2	4	6	24	3
	Forest Preserve Drive	SB	2	4	50	200	22
Forest Preserve Drive		SB	1 & 2	7	30	210	23
		SB	2	4	10	40	4
		SB	1 & 2	4	50	200	22
		SB	2	12	20	240	27
	Irving Park Road	SB	2	4	50	200	22
		<b>TOTALS:</b>				<b>887</b>	<b>873</b>
						<b>FT</b>	<b>SY</b>

ROUTE: IL 19 / Irving Park Rd. (Narragansett Ave. to Pulaski Rd.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Narragansette		EB	2	4	50	200	22
		EB	2	4	40	160	18
		EB	2	7	30	210	23
		EB	2	7	15	105	12
		EB	2	4	40	160	18
		EB	2	7	12	84	9
	Mobile	EB	1	7	12	84	9
Mobile		EB	2	7	12	84	9
		EB	2	4	20	80	9
	Melvina	EB	1	7	12	84	9
Melvina		EB	LT,1,2	30	20	600	67
		EB	2	4	12	48	5
		EB	1 & 2	15	45	675	75
		EB	2	4	12	48	5
		EB	2	4	12	48	5
		EB	2	7	12	84	9
		EB	2	7	40	280	31
		EB	2	7	12	84	9
		EB	1 & 2	16	12	192	21
	Meade	EB	1 & 2	25	10	250	28
Meade		EB	2	4	10	40	4
	Austin	EB	1 & 2	7	7	49	5
Austin		EB	1 & 2	25	10	250	28
		EB	2	7	7	49	5
		EB	2	7	15	105	12
		EB	1 & 2	7	7	49	5
		EB	1 & 2	7	7	49	5
	Menard	EB	2	20	15	300	33
Menard		EB	1	7	12	84	9
		EB	2	7	7	49	5
		EB	2	4	80	320	36
		EB	2	4	80	320	36
	Major	EB	2	7	7	49	5
Major		EB	2	12	35	420	47
		EB	2	7	12	84	9
	Central	EB	2	7	12	84	9
Central		EB	2	10	10	100	11
		EB	2	7	12	84	9
	Long	EB	2	10	30	300	33
Long		EB	2	12	12	144	16
		EB	2	7	30	210	23
		EB	2	4	50	200	22
	Laramie	EB	2	7	7	49	5
Laramie		EB	2	7	50	350	39
		EB	2	12	8	96	11
		EB	1 & 2	25	10	250	28
	Lavergne	EB	2	12	15	180	20
Lavergne		EB	2	12	12	144	16
Milwaukee	Kilbourn	EB	1 & 2	24	12	288	32
Kilbourn		EB	2	12	40	480	53
		EB	2	12	30	360	40
	Kostner	EB	2	4	80	320	36
Kostner		EB	1 & 2	20	15	300	33
	Tripp	EB	2	12	50	600	67

ROUTE: IL 19 / Irving Park Rd. (Narragansett Ave. to Pulaski Rd.) (Continued)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Tripp		EB	2	12	12	144	16
		EB	2	12	12	144	16
		EB	RT	4	50	200	22
		EB	2	7	7	49	5
	Pulaski	EB	2	12	25	300	33
Pulaski		WB	2	12	12	144	16
		WB	1 & 2	7	7	49	5
		WB	1 & 2	7	12	84	9
		WB	2	12	80	960	107
		WB	2	7	20	140	16
		WB	2	12	40	480	53
		WB	RT	7	7	49	5
		WB	2	10	20	200	22
		WB	1 & 2	10	30	300	33
		WB	2	10	20	200	22
	Keeler	WB	2	7	25	175	19
Keeler	Kildare	WB	1 & 2	24	10	240	27
Kildare		WB	2	12	80	960	107
		WB	2	4	100	400	44
	Milwaukee	WB	2	12	12	144	16
Milwaukee	Lavergne	WB	2	12	12	144	16
Lavergne		WB	2	12	20	240	27
	Laramie	WB	2	12	25	300	33
Laramie		WB	1 & 2	20	15	300	33
	Long	WB	2	12	12	144	16
Long		WB	2	7	10	70	8
	Central	WB	2	12	12	144	16
Central	Manard	WB	2	7	7	49	5
Manard		WB	2	12	12	144	16
		WB	2	7	7	49	5
	Austin	WB	1 & 2	7	35	245	27
Austin		WB	2	12	12	144	16
		WB	2	12	12	144	16
		WB	2	7	7	49	5
		WB	2	12	30	360	40
	Melvina	WB	1	15	20	300	33
Melvina		WB	1	4	150	600	67
	Narragansette	WB	2	12	25	300	33
		<b>TOTALS:</b>				<b>2242</b>	<b>2165</b>
						<b>FT</b>	<b>SY</b>

ROUTE: Caldwell Ave. (Peterson Ave. to Central Ave.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Peterson Ave.		WB	2	2	100	200	22
		WB	2	2	300	600	67
		WB	2	2	100	200	22
		WB	2	2	200	400	44
		WB	2	2	100	200	22
		WB	2	2	200	400	44
		WB	2	2	100	200	22
		WB	2	2	200	400	44
		WB	2	2	200	400	44
		WB	2	2	500	1000	111
		EB	2	2	300	600	67
		EB	2	2	200	400	44
		EB	2	2	100	200	22
		EB	2	2	200	400	44
		EB	1,2	2	100	200	22
		EB	2	2	600	1200	133
		EB	2	2	200	400	44
		EB	2	2	400	800	89
		EB	2	2	300	600	67
		EB	2	2	300	600	67
		EB	2	2	200	400	44
	Central Ave.	EB	2	2	200	400	44
<b>TOTALS:</b>						<b>5100</b>	<b>1133</b>
						<b>FT</b>	<b>SY</b>

ROUTE: Rand Rd. (Golf Rd. to Wolf Rd.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Golf Rd.		NB	1,2	3	600	1800	200
		NB	1	3	600	1800	200
		NB	2	3	400	1200	133
		NB	1	3	300	900	100
		NB	2	3	200	600	67
		NB	2	3	200	600	67
	Wolf Rd.	NB	1,2	3	300	900	100
Wolf Rd.		SB	1,2	3	300	900	100
		SB	1,2	3	300	900	100
		SB	1,2	3	400	1200	133
		SB	2	3	400	1200	133
		SB	1,2	3	400	1200	133
	Golf Rd.	SB	1,2	3	300	900	100
<b>TOTALS:</b>						<b>4700</b>	<b>1567</b>
						<b>FT</b>	<b>SY</b>

ROUTE: Greenwood Rd. (Central Rd. to Ronald Rd.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Central Rd.		NB	1	3	300	900	100
		NB	1	3	100	300	33
		NB	1	3	200	600	67
	Ronald Rd.	SB	1	3	300	900	100
<b>TOTALS:</b>						<b>900</b>	<b>300</b>
						<b>FT</b>	<b>SY</b>



ROUTE: Dundee Rd. (Waukegan Rd. to Skokie Blvd.) (Continued)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Bittersweet Dr.		WB	1,2,med	30	3	90	10
		WB	1,2,med	30	3	90	10
		WB	1,2,med	30	3	90	10
		WB	1,2,med	30	3	90	10
		WB	1,2	24	3	72	8
Lee Rd.	Lee Rd.	WB	1,2	24	3	72	8
		WB	1,2,med	30	3	90	10
		WB	1,2,med	30	3	90	10
		WB	1	3	100	300	33
		WB	1,2	24	3	72	8
Shermer Rd.	Shermer Rd.	WB	1,2	24	3	72	8
		WB	1,2,med	36	3	108	12
		WB	1	3	100	300	33
		WB	1,2,med	30	3	90	10
		WB	1,2,med	30	3	90	10
		WB	1,2,med	24	3	72	8
		WB	1,2	24	3	72	8
		WB	1,2,med	30	3	90	10
		WB	1,2,med	30	3	90	10
		WB	1,2	24	3	72	8
RR	RR	WB	1,2,med	30	3	90	10
		WB	1,2,med	30	3	90	10
		WB	1,2	24	3	72	8
		WB	1,2	24	3	72	8
		WB	1,2	24	3	72	8
		WB	1,2	24	3	72	8
		WB	1	3	350	1050	117
		WB	1,2,med	30	3	90	10
		WB	1	3	100	300	33
		WB	1,2	24	3	72	8
Skokie Blvd.	Skokie Blvd.	WB	1	3	200	600	67
		WB	1	3	250	750	83
		WB	1,2	24	3	72	8
		WB	1,2	24	12	288	32
		<b>TOTALS:</b>				<b>3225</b>	<b>2262</b>
						<b>FT</b>	<b>SY</b>

ROUTE: Harlem Ave. (Milwaukee Ave. to Devon Ave.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Devon		NB	1 & 2	24	20	480	53
		NB	2	4	40	160	18
		NB	1 & 2	4	50	200	22
		NB	2	12	12	144	16
		NB	2	4	140	560	62
Pratt	Pratt	NB	2	4	80	320	36
		NB	2	4	60	240	27
		NB	2	4	120	480	53
		NB	2	4	140	560	62
		NB	1	7	12	84	9
Touhy	Touhy	NB	2	4	50	200	22
		NB	1 & 2	7	24	168	19
		NB	2	7	7	49	5
		NB	2	4	150	600	67
		NB	2	7	12	84	9
		NB	2	4	80	320	36
		NB	2	7	12	84	9
		NB	2	7	30	210	23
		NB	2	4	60	240	27
		NB	2	4	20	80	9
Milwaukee	Milwaukee	NB	2	4	40	160	18
		NB	2	4	50	200	22
		NB	1 & 2	7	24	168	19
		SB	1 & 2	7	24	168	19
		SB	1	7	12	84	9
Touhy	Touhy	SB	2	12	12	144	16
		SB	2	4	400	1600	178
		SB	2	4	50	200	22
		SB	2	4	50	200	22
		SB	1, 2, LT	7	35	245	27
Pratt	Pratt	SB	1 & 2	24	35	840	93
		SB	1 & 2	12	12	144	16
		SB	1 & 2	12	12	144	16
		SB	2	4	40	160	18
		SB	2	4	60	240	27
Devon	Devon	SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	1	4	80	320	36
		SB	2	4	120	480	53
		SB	1 & 2	10	10	100	11
		<b>TOTALS:</b>				<b>2313</b>	<b>1306</b>
						<b>FT</b>	<b>SY</b>



ROUTE: Sheridan Rd. (Harbor St. to Woodlawn Ave.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Harbor St.		NB	1	3	400	1200	133
		NB	1	3	100	300	33
		NB	1	3	200	600	67
		NB	1	3	100	300	33
	Woodlawn Ave.	NB	1	3	200	600	67
Woodlawn Ave.		SB	1	3	100	300	33
		SB	1	3	100	300	33
		SB	1	3	200	600	67
		SB	1	3	200	600	67
	Harbor St.	SB	1	10	100	1000	111
<b>TOTALS:</b>					<b>1900</b>		<b>711</b>
					<b>FT</b>		<b>SY</b>

ROUTE: Shermer Rd. (Willow Rd. to Old Willow Rd.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Willow		SB	1,2	16	3	48	5
		SB	1	3	50	150	17
	RR Viaduct	SB	1	12	3	36	4
RR viaduct		SB	1	3	75	225	25
		SB	1	12	3	36	4
	Holste Rd.	SB	1	3	50	150	17
Holste Rd.		SB	1	3	200	600	67
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	3	100	300	33
		SB	1	3	75	225	25
	Old Willow Rd.	SB	1	12	3	36	4
Willow Rd.	RR Viaduct	NB	1	12	3	36	4
RR viaduct		NB	1	12	3	36	4
		NB	1	12	3	36	4
		NB	1	12	3	36	4
	Holste Rd.	NB	1	12	3	36	4
Holste Rd.		NB	1	3	75	225	25
		NB	1	12	3	36	4
		NB	1	12	3	36	4
		NB	1	3	125	375	42
		NB	1	12	3	36	4
		NB	1	3	65	195	22
	Old Willow Rd.	NB	1	12	3	36	4
<b>TOTALS:</b>					<b>869</b>		<b>346</b>
					<b>FT</b>		<b>SY</b>

ROUTE: Wolf Rd. (Golf Rd. to Princeton St.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Golf Rd.		NB	1	3	100	300	33
		NB	2	3	200	600	67
		NB	1	3	100	300	33
		NB	2	3	300	900	100
		NB	1,2	3	200	600	67
		NB	2	3	500	1500	167
		NB	1,2	3	300	900	100
		NB	2	3	300	900	100
		NB	1,2	3	100	300	33
	Princeton St.	NB	2	3	500	1500	167
Princeton St.		NB	1,2	3	200	600	67
		SB	1,2	3	300	900	100
		SB	2	3	500	1500	167
		SB	1,2	3	300	900	100
		SB	2	3	400	1200	133
	Wolf Rd.	SB	1,2	3	200	600	67
<b>TOTALS:</b>					<b>5000</b>		<b>1667</b>
					<b>FT</b>		<b>SY</b>

ROUTE: Palatine Rd. Ramps at Milwaukee Ave.							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Palatine exit ramp EB		EB	2	3	100	300	33
		EB	2	3	100	300	33
		EB	2	3	100	300	33
		EB	2	3	100	300	33
		EB	2	3	200	600	67
	SB Milwaukee Ave	EB	2	3	200	600	67
NB Milwaukee on ramp	EB Palatine	EB	1	3	600	1800	200
WB Palatine exit ramp	NB Milwaukee	WB	1	3	300	900	100
<b>TOTALS:</b>					<b>1700</b>		<b>567</b>
					<b>FT</b>		<b>SY</b>



ROUTE: Skokie Blvd. (Toughy Ave. to Devon Ave.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB) (NB/SB)	NO. (1, 2, 3)	PATCH WIDTH	PATCH LENGTH	AREA (SQ FT)	AREA (SQ YD)
Devon		NB	2	4	80	320	36
		NB	1 & 2	7	24	168	19
		NB	2	12	12	144	16
	Pratt	NB	1	4	150	600	67
Pratt		NB	1	4	80	320	36
		NB	1 & 2	7	40	280	31
	Estes	NB	2	7	18	126	14
Estes		NB	1 & 2	7	24	168	19
	Touhy	NB	1 & 2	7	30	210	23
Touhy		SB	1 & 2	7	50	350	39
		SB	1 & 2	7	30	210	23
		SB	2	4	130	520	58
		SB	1 & 2	7	30	210	23
	Pratt	SB	1 & 2	7	50	350	39
Pratt		SB	2	12	12	144	16
		SB	2	7	12	84	9
		SB	2	12	12	144	16
		SB	2	4	120	480	53
	Devon	SB	1 & 2	10	30	300	33
		<b>TOTALS:</b>				<b>934</b>	<b>570</b>
						<b>FT</b>	<b>SY</b>

ROUTE: Crawford Ave. (Golf Rd. to Harrison St.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB) (NB/SB)	NO. (1, 2, 3)	PATCH WIDTH	PATCH LENGTH	AREA (SQ FT)	AREA (SQ YD)
Harrison St.		SB	1,2	2	100	200	22
		SB	2	2	30	60	7
		SB	1,2	2	400	800	89
		SB	2	2	400	800	89
		SB	1,2	2	200	400	44
		SB	1,2	2	200	400	44
		SB	2	2	200	400	44
		SB	1,2	2	200	400	44
		NB	1,2	2	200	400	44
		NB	1,2	2	100	200	22
		NB	2	2	100	200	22
		NB	1,2	2	300	600	67
		NB	1,2	2	200	400	44
		NB	1,2	2	100	200	22
	Golf Rd.	NB	1,2	2	300	600	67
		<b>TOTALS:</b>				<b>3030</b>	<b>673</b>
						<b>FT</b>	<b>SY</b>

ROUTE: Golf Rd. (Waukegan Rd. to Gross Point Rd.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Waukegan		EB	1,2	2	100	200	22
		EB	2	2	200	400	44
		EB	1,2	2	100	200	22
		EB	1,2	2	100	200	22
		EB	1,2	2	200	400	44
		EB	1,2	2	300	600	67
		EB	1,2	2	100	200	22
		EB	1,2	2	100	200	22
		EB	1,2	2	200	400	44
		EB	1,2	2	100	200	22
		EB	1,2	2	100	200	22
		EB	1,2	2	300	600	67
	Harms	EB	1,2	2	100	200	22
Harms		EB	2	2	100	200	22
	Skokie	EB	2	2	200	400	44
Skokie	Gross Point Rd	EB	2	2	200	400	44
Gross Point Rd		WB	2	2	100	200	22
		WB	1	2	100	200	22
	Skokie	WB	2	2	100	200	22
Skokie		WB	1	2	100	200	22
		WB	1	2	100	200	22
		WB	1,2	2	100	200	22
		WB	2	2	100	200	22
		WB	1,2	2	200	400	44
	Harms	WB	1	2	100	200	22
Harms		WB	1	2	100	200	22
		WB	1,2	2	200	400	44
		WB	2	2	200	400	44
		WB	1	2	100	200	22
		WB	1,2	2	100	200	22
		WB	1	2	100	200	22
		WB	1,2	2	200	400	44
		WB	1,2	2	100	200	22
		WB	1,2	2	100	200	22
	Waukegan	WB	1,2	2	100	200	22
		<b>TOTALS:</b>				<b>4800</b>	<b>1067</b>
						<b>FT</b>	<b>SY</b>

ROUTE: Golf Rd. (Milwaukee Ave. to East River Rd.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Milwaukee Ave.		WB	1,2,3	2	200	400	44
		WB	1,2,3	2	200	400	44
		WB	1,2,3	2	200	400	44
		WB	1,2,3	2	300	600	67
	Dee Rd.	WB	1,2,3	2	100	200	22
Dee Rd.	River Rd.	WB	2	2	500	1000	111
River Rd.	Dee Rd.	EB	1	2	500	1000	111
Dee Rd.	Milwaukee Ave.	EB	1,2	2	100	200	22
		EB	1	2	100	200	22
		EB	1,2	2	100	200	22
		EB	1,2,3	2	400	800	89
Milwaukee Ave.	Golf Rd. Intersection	EB	1,2,3	2	400	800	89
		<b>TOTALS:</b>				<b>3100</b>	<b>689</b>
						<b>FT</b>	<b>SY</b>

ROUTE: Oakton St. (Greenwood Rd. to Busse Rd.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Greenwood Road		WB	1	12	12	144	16
		WB	2	4	80	320	36
		WB	1	4	120	480	53
		WB	1 & 2	7	24	168	19
		WB	1 & 2	12	12	144	16
		WB	1 & 2	12	12	144	16
		WB	2	12	12	144	16
		WB	2	7	12	84	9
		WB	1	7	12	84	9
		WB	1	4	150	600	67
		WB	1	7	12	84	9
		WB	1 & 2	7	24	168	19
	Northwest Highway	WB	1	4	150	600	67
Northwest Highway		WB	1 & 2	7	24	168	19
		WB	1 & 2	7	24	168	19
		WB	2	4	50	200	22
		WB	1	12	12	144	16
	Dee Road	WB	1	12	12	144	16
Dee Road		WB	2	12	12	144	16
	Busse Road	WB	1	12	12	144	16
Busse Road	Dee Road	EB	1	12	12	144	16
Dee Road		EB	1 & 2	7	24	168	19
		EB	2	12	12	144	16
		EB	1	4	120	480	53
		EB	2	4	250	1000	111
		EB	2	7	12	84	9
	Northwest Highway	EB	2	4	160	640	71
Northwest Highway		EB	1 & 2	7	12	84	9
		EB	2	12	20	240	27
		EB	1 & 2	7	24	168	19
		EB	1 & 2	7	24	168	19
		EB	2	4	120	480	53
		EB	2	4	150	600	67
		EB	2	4	200	800	89
		EB	1 & 2	10	24	240	27
		EB	2	4	120	480	53
		EB	2	4	180	720	80
		EB	2	4	150	600	67
		EB	1 & 2	10	24	240	27
		EB	1 & 2	10	24	240	27
		EB	2	4	160	640	71
	Greenwood Road	EB	2	10	12	120	13
		<b>TOTALS:</b>				<b>2612</b>	<b>1417</b>
						<b>FT</b>	<b>SY</b>

ROUTE: River Rd. (Oakton St. to Miner St.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Oakton Street		NB	1 & 2	4	150	600	67
		NB	2	10	12	120	13
		NB	1	7	12	84	9
		NB	1 & 2	7	24	168	19
		NB	1 & 2	7	24	168	19
		NB	1 & 2	4	200	800	89
		NB	1	4	150	600	67
		NB	1 & 2	10	24	240	27
		NB	2	7	20	140	16
	Algonquin Road	NB	2	4	80	320	36
Algonquin Road		NB	2	12	12	144	16
		NB	1	12	12	144	16
		NB	1 & 2	12	12	144	16
		NB	1 & 2	4	160	640	71
		NB	2	12	50	600	67
		NB	2	4	180	720	80
		NB	1	12	12	144	16
		NB	1	10	12	120	13
	Miner Street	NB	2	12	12	144	16

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ROUTE: River Rd. (Oakton St. to Miner St.) (Continued)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Miner Street		SB	2	12	12	144	16
		SB	1	12	20	240	27
		SB	1	12	20	240	27
		SB	1 & 2	12	24	288	32
		SB	2	10	10	100	11
		SB	1	4	180	720	80
		SB	1 & 2	12	24	288	32
		SB	2	12	20	240	27
		SB	1	12	20	240	27
		SB	2	4	80	320	36
		SB	1	12	50	600	67
		SB	2	12	20	240	27
		SB	1	12	12	144	16
		SB	2	12	12	144	16
		SB	2	4	140	560	62
		SB	1 & 2	10	24	240	27
		SB	2	7	12	84	9
		SB	1	10	12	120	13
Walnut Avenue	Walnut Avenue	SB	1 & 2	10	24	240	27
		SB	2	4	100	400	44
		SB	2	4	100	400	44
		SB	1 & 2	12	24	288	32
		SB	2	4	80	320	36
Algonquin Road	Algonquin Road	SB	1	10	12	120	13
		SB	1 & 2	4	300	1200	133
		SB	1	7	12	84	9
		SB	2	7	12	84	9
		SB	1	12	12	144	16
		SB	2	7	12	84	9
		SB	2	7	12	84	9
		SB	2	7	12	84	9
		SB	1 & 2	10	24	240	27
		SB	2	4	180	720	80
		SB	1	10	12	120	13
		SB	2	10	12	120	13
		SB	1 & 2	10	24	240	27
		SB	1 & 2	10	24	240	27
		SB	2	4	80	320	36
		SB	1 & 2	7	24	168	19
		SB	1 & 2	7	24	168	19
		SB	2	4	50	200	22
		SB	2	12	12	144	16
		SB	1	7	12	84	9
		SB	2	7	12	84	9
		SB	1	7	12	84	9
		SB	2	7	12	84	9
	Oakton Street	SB	2	7	12	84	9
		<b>TOTALS:</b>				<b>3076</b>	<b>1949</b>
						<b>FT</b>	<b>SY</b>

ROUTE: Dempster St. (Greenwood Rd. to River Rd.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
River Road		EB	1 & 2	10	12	120	13
		EB	2	7	12	84	9
		EB	1	12	30	360	40
		EB	2	4	350	1400	156
		EB	1	12	12	144	16
		EB	1 & 2	10	24	240	27
		EB	2	4	250	1000	111
		EB	2	12	12	144	16
		EB	2	4	130	520	58
		EB	2	12	30	360	40
		EB	1 & 2	12	24	288	32
		EB	1	12	12	144	16
		EB	2	12	12	144	16
		EB	1 & 2	10	24	240	27
		EB	2	12	12	144	16
		EB	1 & 2	10	50	500	56
	Northwest Highway	EB	2	12	12	144	16
Northwest Highway		EB	2	7	12	84	9
		EB	1	12	12	144	16
		EB	1 & 2	4	180	720	80
		EB	2	12	12	144	16
		EB	1 & 2	12	12	144	16
		EB	2	4	80	320	36
		EB	1 & LT	4	80	320	36
	Potter Road	EB	1 & 2	4	120	480	53
Potter Road		EB	2	7	12	84	9
		EB	2	7	12	84	9
	Dee Road	EB	2	7	12	84	9
Dee Road		EB	1 & 2	10	24	240	27
	Luther Lane	EB	1 & 2	10	36	360	40
Luther Lane		EB	1	12	20	240	27
		EB	2	12	12	144	16
		EB	1	4	80	320	36
	Western Avenue	EB	1	12	12	144	16
Western Avenue		EB	1 & 2	4	60	240	27
		EB	2	10	12	120	13
		EB	2	4	250	1000	111
		EB	2	12	12	144	16
	Greenwood Road	EB	2	4	150	600	67

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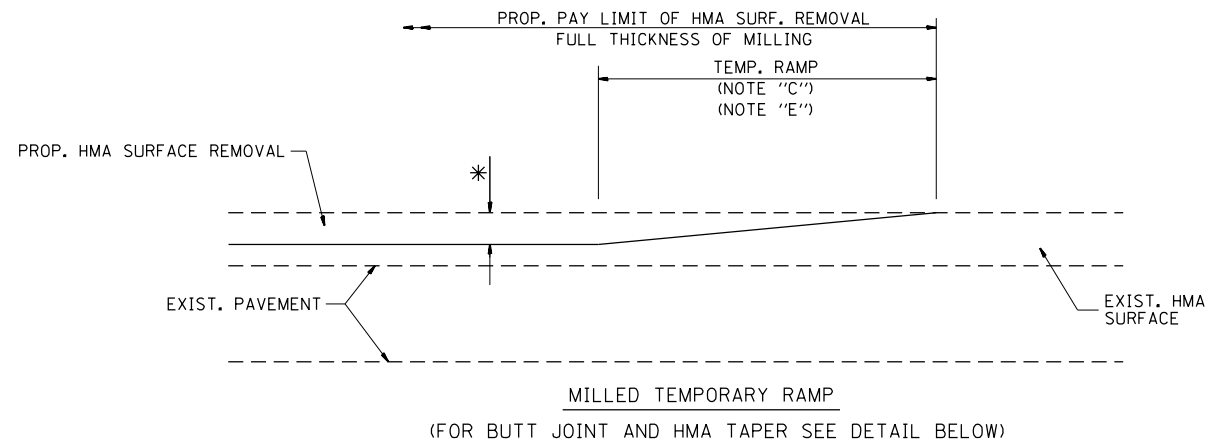
ROUTE: Dempster St. (Greenwood Rd. to River Rd.) (Continued)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Greenwood Road		WB	2	12	12	144	16
		WB	1	7	12	84	9
	Western Avenue	WB	2	7	12	84	9
Western Avenue	Luther Lane	WB	2	12	25	300	33
Luther Lane		WB	2	10	12	120	13
		WB	1	10	12	120	13
		WB	1	10	12	120	13
	Potter Road	WB	2	10	12	120	13
Potter Road		WB	1 & 2	4	80	320	36
		WB	2	10	24	240	27
		WB	2	7	20	140	16
		WB	1 & 2	12	24	288	32
		WB	1 & 2	4	350	1400	156
		WB	1 & 2	10	24	240	27
		WB	3	10	12	120	13
	Rand Road	WB	2	10	12	120	13
Rand Road		WB	1	10	12	120	13
		WB	1 & 2	10	12	120	13
		WB	1 & 2	10	24	240	27
		WB	1 & 2	10	24	240	27
		WB	1 & 2	10	24	240	27
		WB	2	10	24	240	27
		WB	2	4	50	200	22
		WB	2	12	12	144	16
		WB	2	4	80	320	36
		WB	2	12	60	720	80
		WB	2	7	12	84	9
	River Road	WB	1 & 2	12	24	288	32
		<b>TOTALS:</b>				<b>3233</b>	<b>2117</b>
						<b>FT</b>	<b>SY</b>

ROUTE: Waukegan Rd. (Chestnut Ave. to Woodlawn Ave.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Woodlawn		NB	2	4	120	480	53
		NB	2	7	12	84	9
		NB	2	4	80	320	36
		NB	2	4	60	240	27
		NB	2	4	220	880	98
		NB	1 & 2	7	24	168	19
	Chestnut	NB	1 & 2	7	24	168	19
Chestnut		NB	1 & 2	7	12	84	9
		NB	2	4	120	480	53
		NB	2	4	200	800	89
		NB	1	4	50	200	22
		NB	1	4	80	320	36
	Ammer	NB	2	4	250	1000	111
Ammer		SB	2	4	100	400	44
		SB	12	7	24	168	19
		SB	1	4	50	200	22
		SB	1	7	12	84	9
		SB	2	7	12	84	9
		SB	1	4	100	400	44
		SB	1 & 2	7	12	84	9
		SB	2	4	80	320	36
		SB	1	4	150	600	67
		SB	2	7	12	84	9
		SB	2	7	12	84	9
	Chestnut	SB	2	7	12	84	9
Chestnut		SB	2	7	12	84	9
		SB	1 & 2	7	24	168	19
		SB	1	4	200	800	89
		SB	2	7	12	84	9
		SB	1	4	50	200	22
		SB	2	4	250	1000	111
	Woodlawn	SB	1 & 2	7	24	168	19
		<b>TOTALS:</b>				<b>2388</b>	<b>1137</b>
						<b>FT</b>	<b>SY</b>

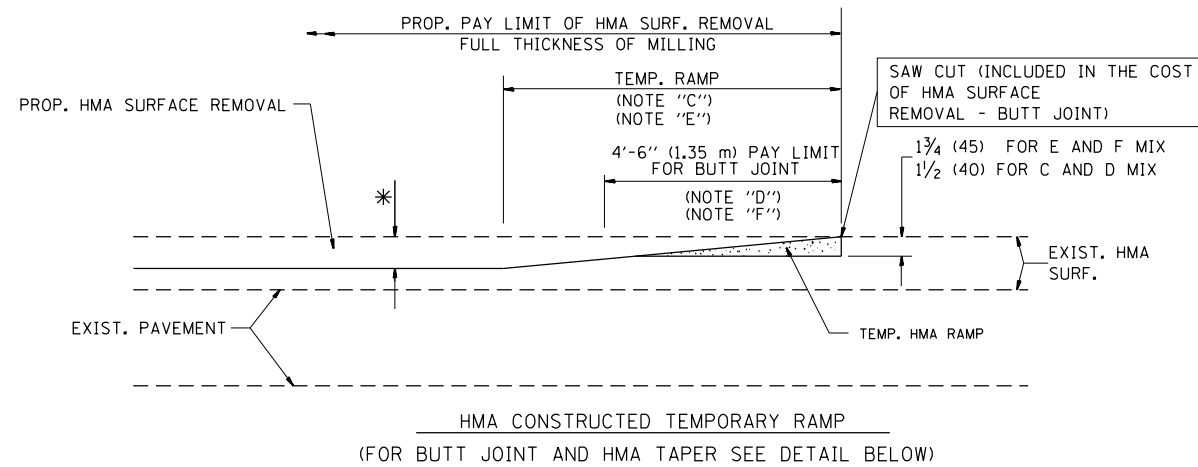
ROUTE: Asbury Ave. (Oakton St. to Howard St.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Oakton		SB	1,2	2	300	600	67
		SB	1,2	2	50	100	11
		SB	1,2	2	700	1400	156
		SB	1,2	2	150	300	33
		SB	2	2	200	400	44
		SB	1,2	2	300	600	67
		NB	1,2	2	50	100	11
		NB	2	2	50	100	11
		NB	2	2	200	400	44
		NB	2	2	150	300	33
		NB	2	2	200	400	44
		NB	2	2	200	400	44
		NB	2	2	200	400	44
		NB	1,2	2	1000	2000	222
	Howard	NB	1,2	2	50	100	11
<b>TOTALS:</b>					<b>3600</b>		<b>800</b>
					<b>FT</b>		<b>SY</b>

ROUTE: Dempster St. (Fowler Ave. to Dodge Ave.)							
CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Fowler		EB	2	2	100	200	22
		WB	2	2	100	200	22
		WB	2	4	10	40	4
		WB	2	2	200	400	44
		WB	2	2	200	400	44
		WB	2	2	150	300	33
		WB	2	2	150	300	33
		WB	2	2	50	100	11
		EB	2	2	100	200	22
		EB	2	2	200	400	44
		EB	2	2	200	400	44
		EB	2	2	200	400	44
		EB	1	10	10	100	11
	Dodge	EB	2	10	50	500	56
<b>TOTALS:</b>					<b>1720</b>		<b>438</b>
					<b>FT</b>		<b>SY</b>



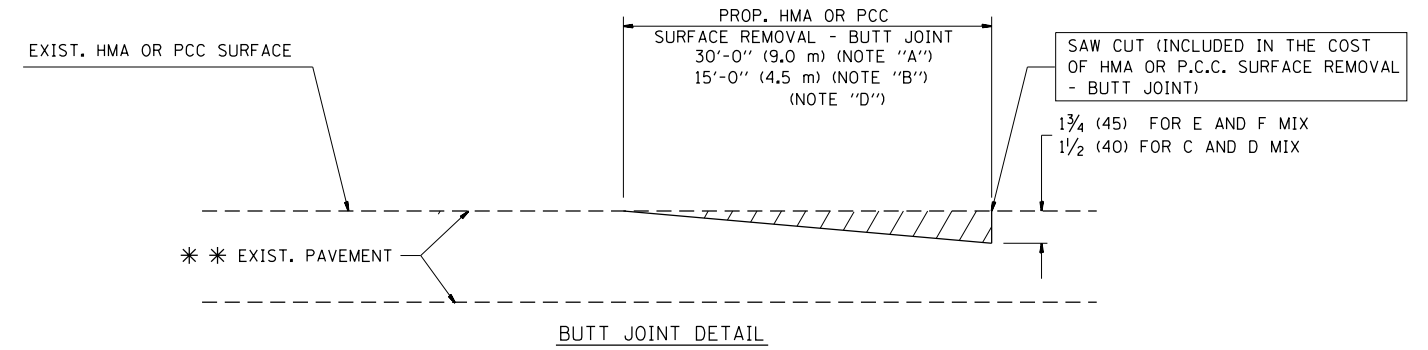


**OPTION 1**

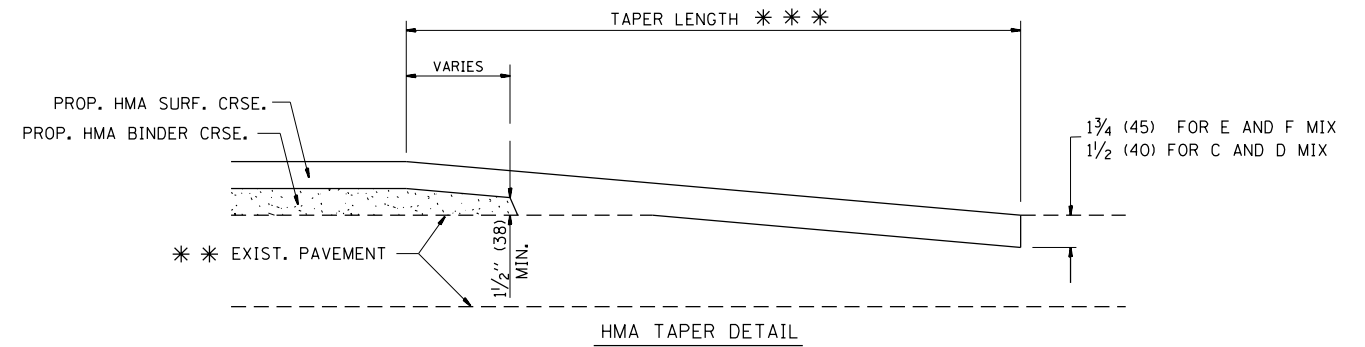


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\* \* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

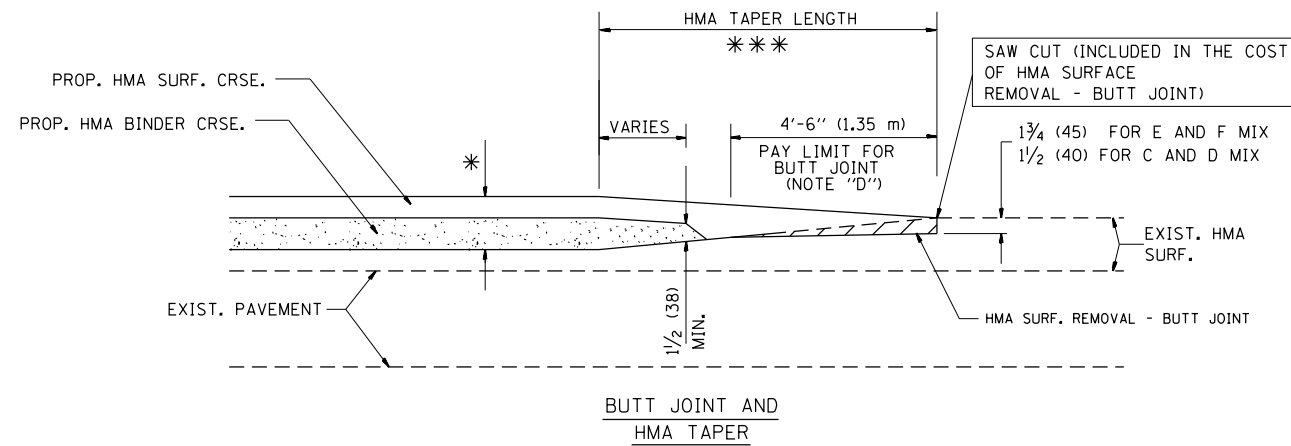
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



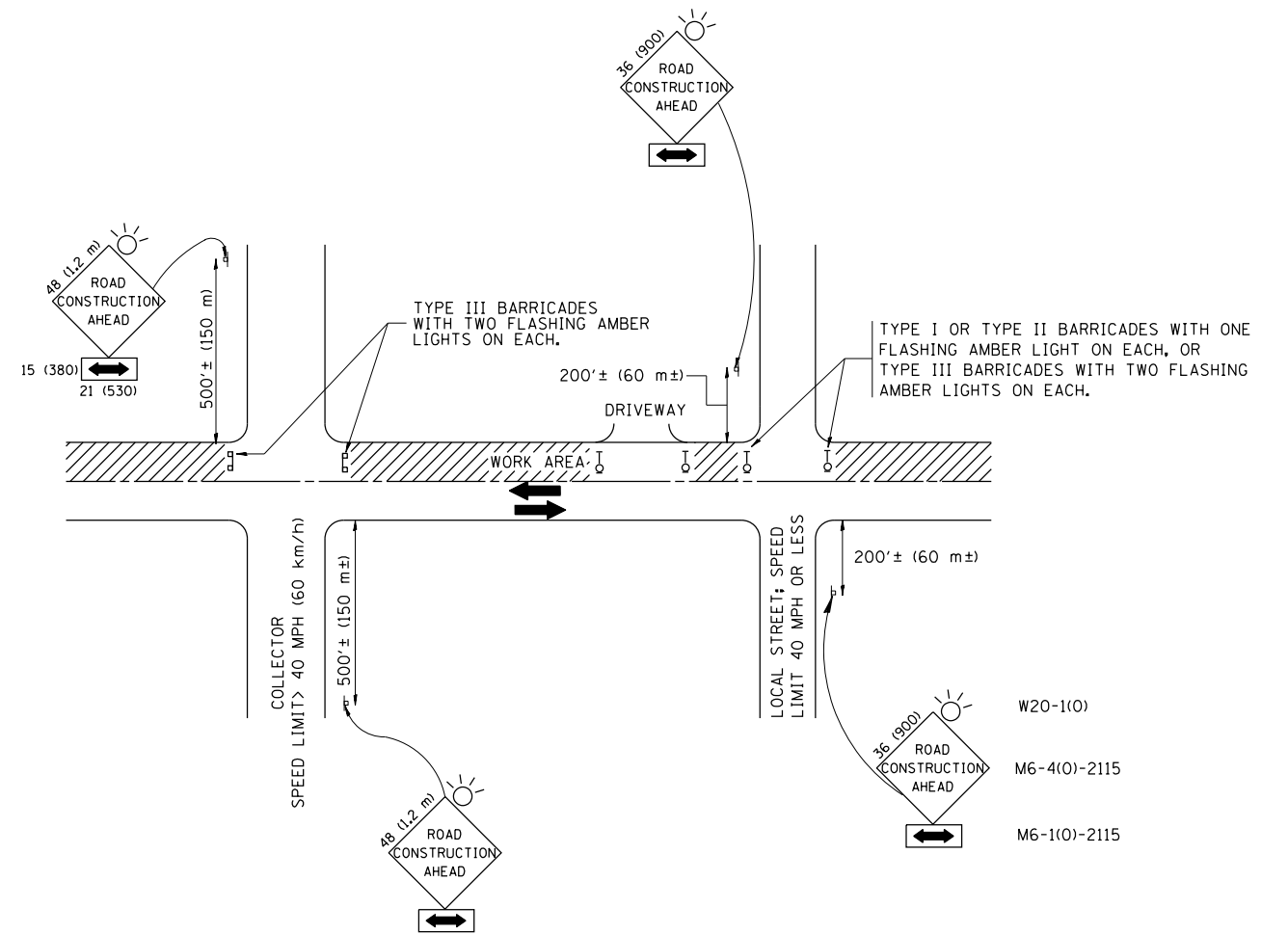
**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

FILE NAME =	USER NAME = chrzesclr	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
et:\pw\work\p\idot\chrzesclr\d0303692\01stgStd.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 4/10/2012	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2012-016 R5	COOK	33	25
<b>BD400-05 BD32</b>		<b>CONTRACT NO. 60T62</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



## TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

### NOTES:

#### A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

#### B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

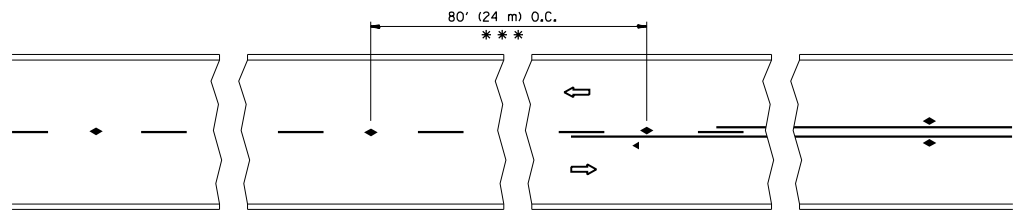
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 4/10/2012	DATE - 06-89	REVISED - T. RAMMACH 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

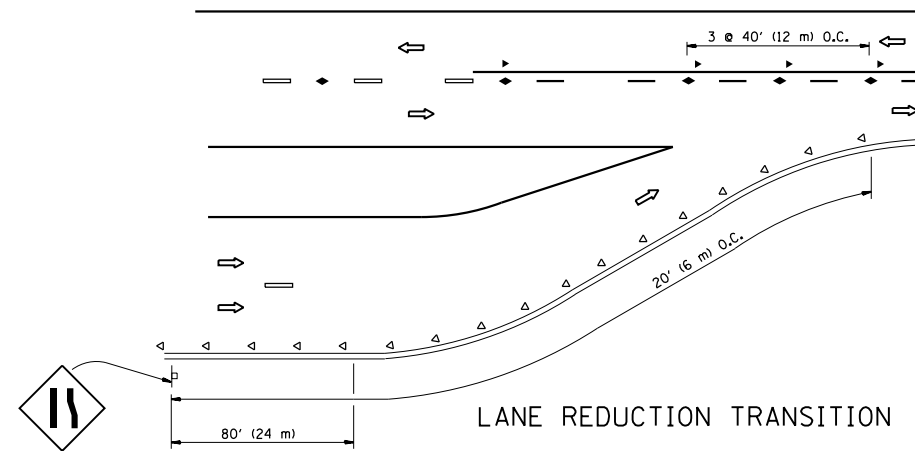
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 60T62	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

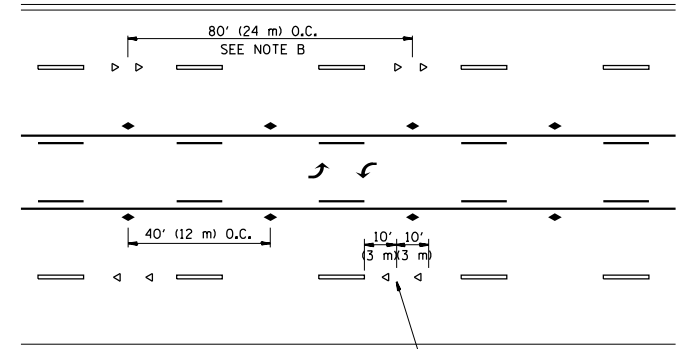


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

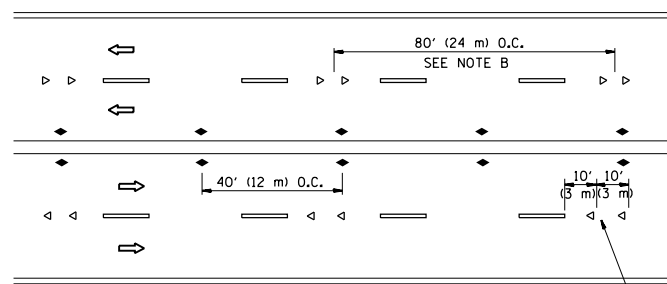
TWO-LANE/TWO-WAY



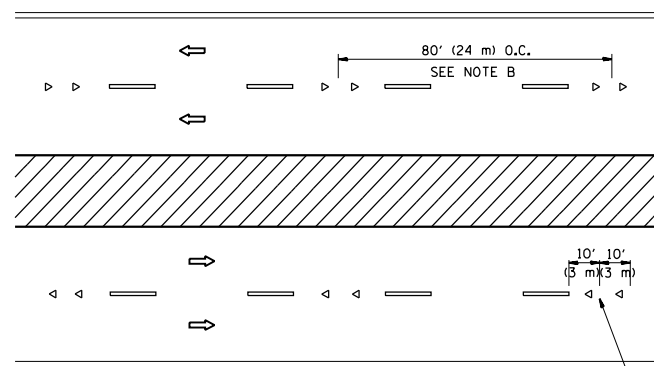
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

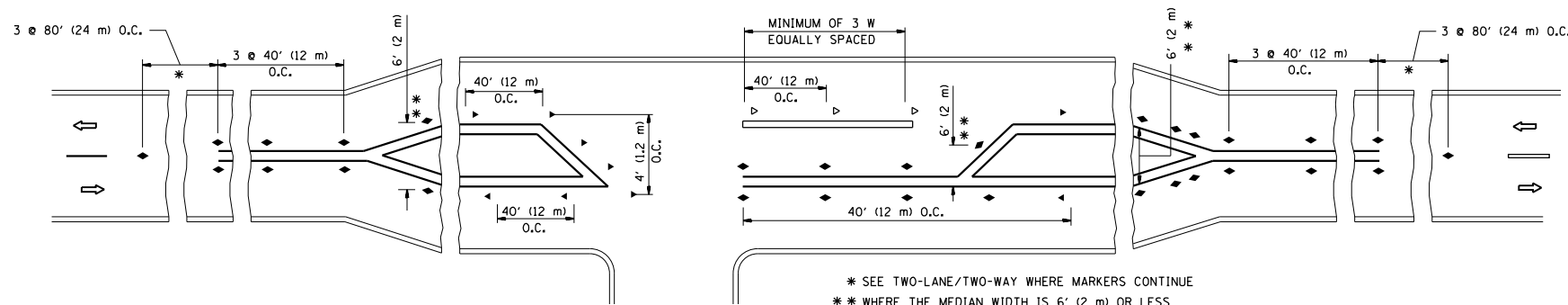
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

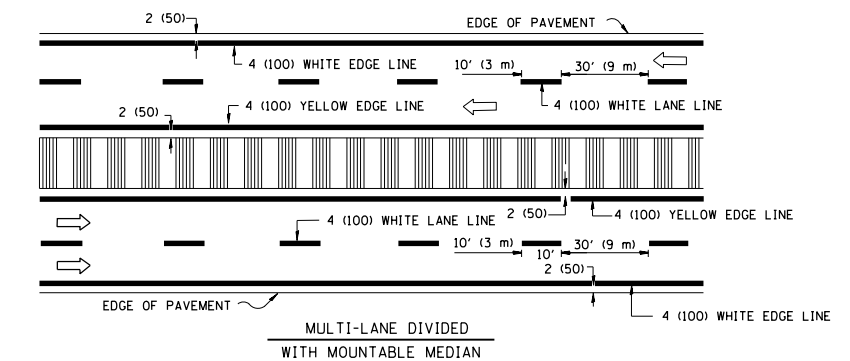
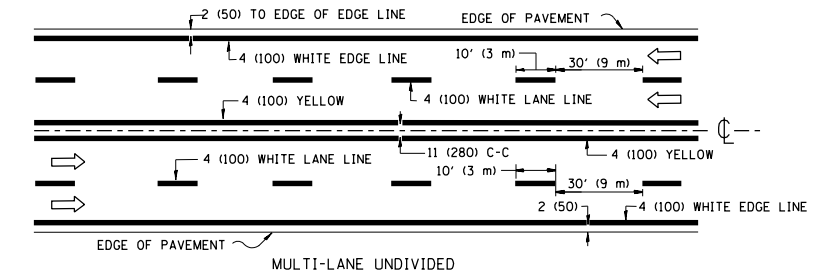
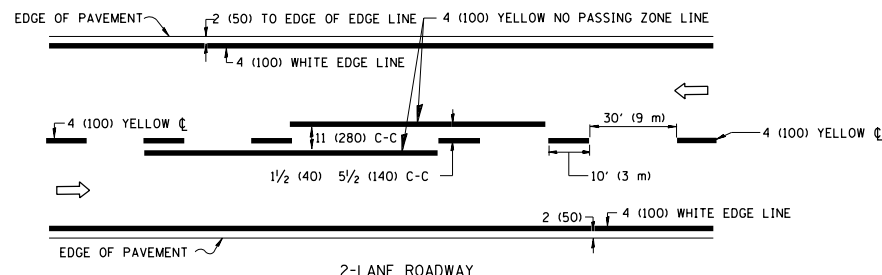
All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 4/10/2012	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

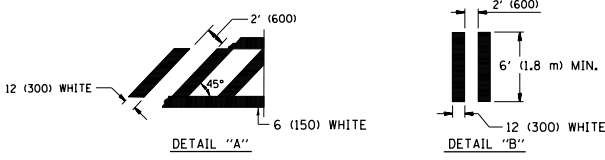
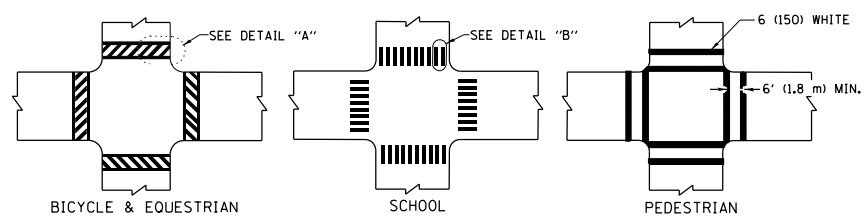
TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2012-016 R5	COOK	33	27
TC-11			CONTRACT NO. 60T62	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

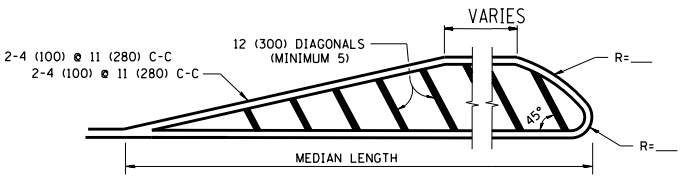
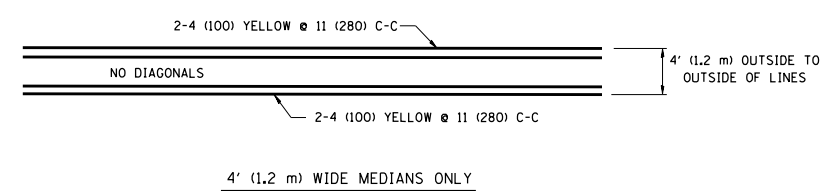


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

**TYPICAL LANE AND EDGE LINE MARKING**



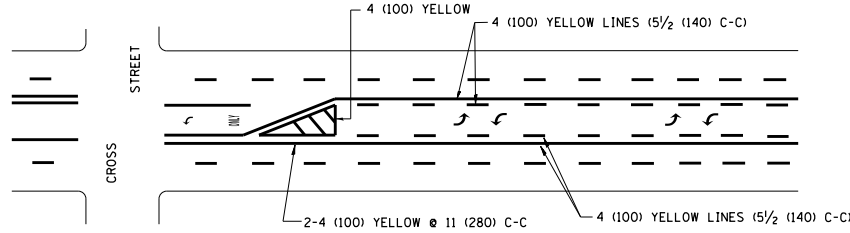
**TYPICAL CROSSWALK MARKING**



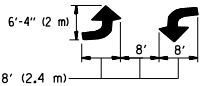
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

**MEDIANS OVER 4' (1.2 m) WIDE**

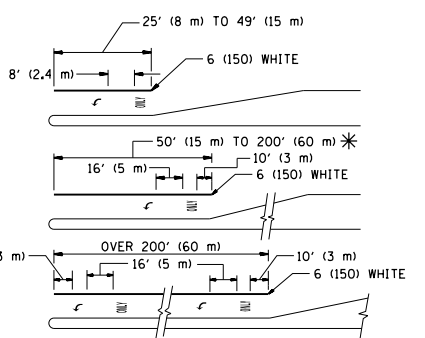


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

**TYPICAL PAINTED MEDIAN MARKING**

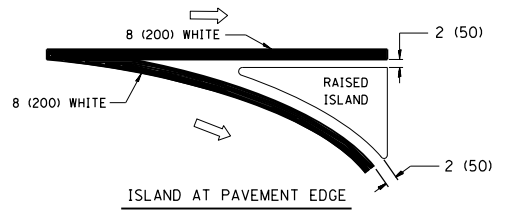
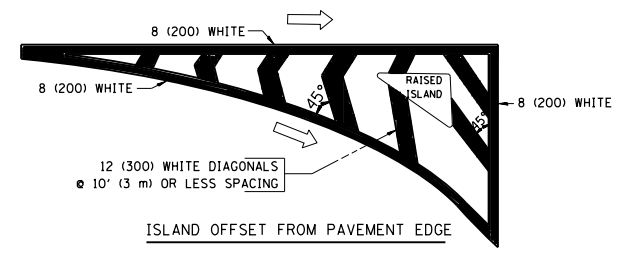


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**



**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R": 3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X": 54.0 SQ. FT. (5.0 m <sup>2</sup> ) EACH
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

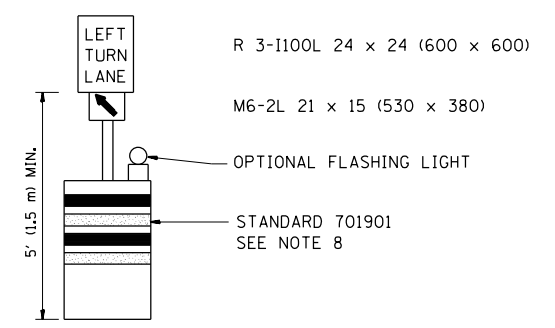
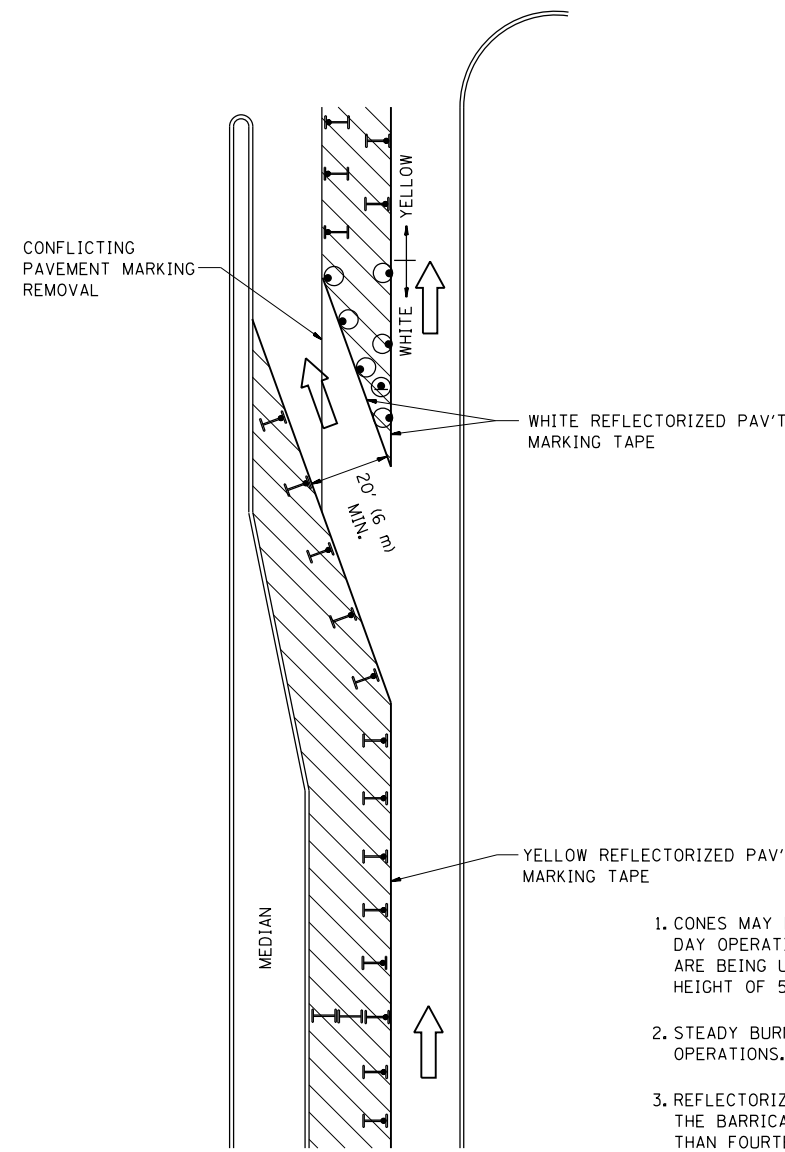
All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT DATE = 4/10/2012	DATE - 03-19-90	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2012-016 R5	COOK	33	28
TC-13		CONTRACT NO. 60T62		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

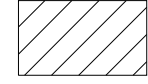







**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

**LEGEND**

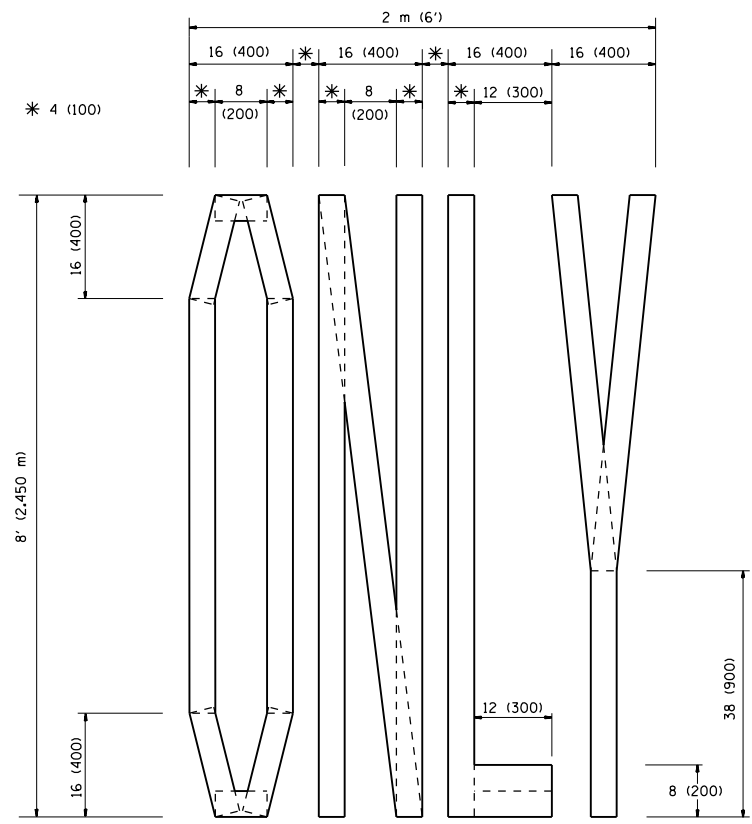
-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = chrzesclr	REVISED -T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
et:\pw\work\pwidot\chrzesclr\d0303692\01std.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 4/10/2012	REVISED -T. RAMMACHER 01-06-00	REVISED -

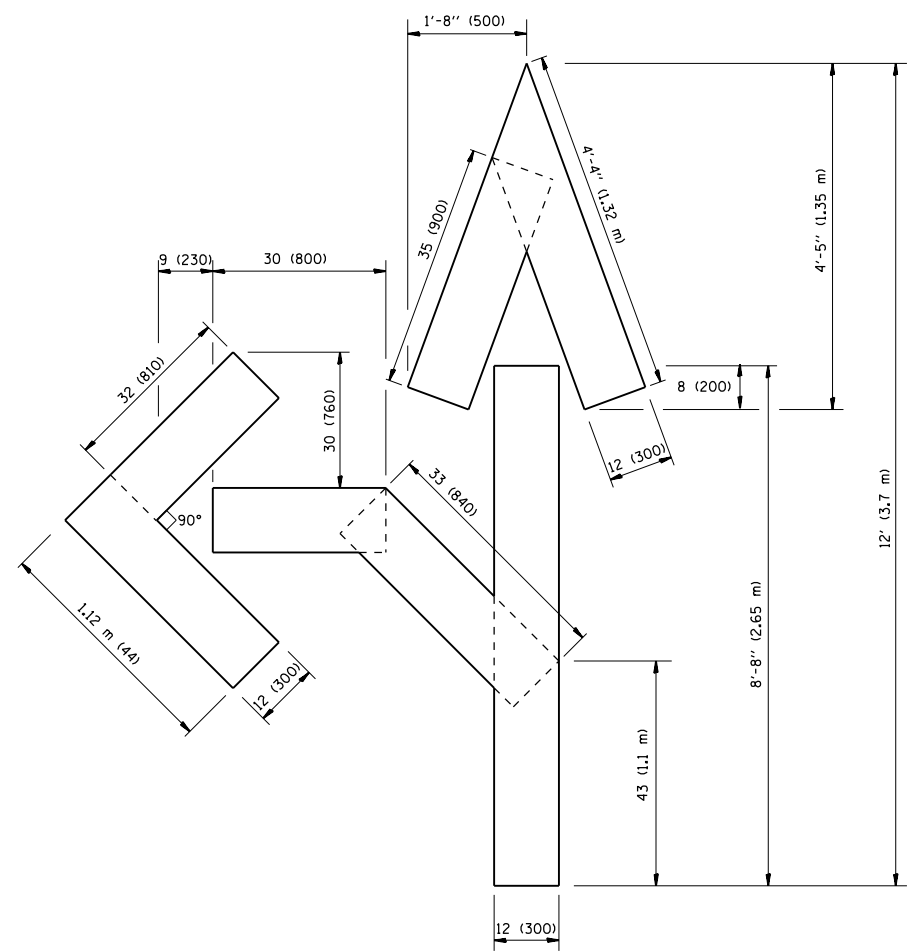
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

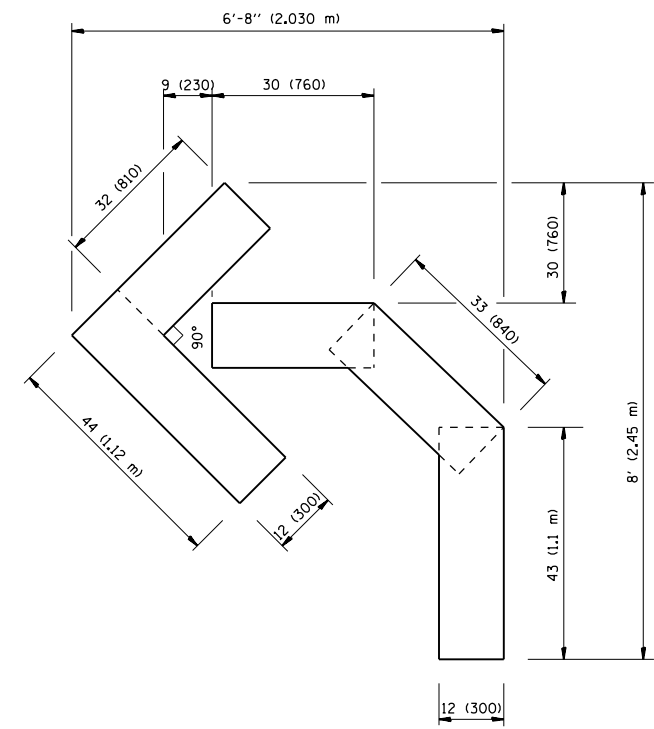
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2012-016 R5	COOK	33	29
TC-14		CONTRACT NO. 60T62		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

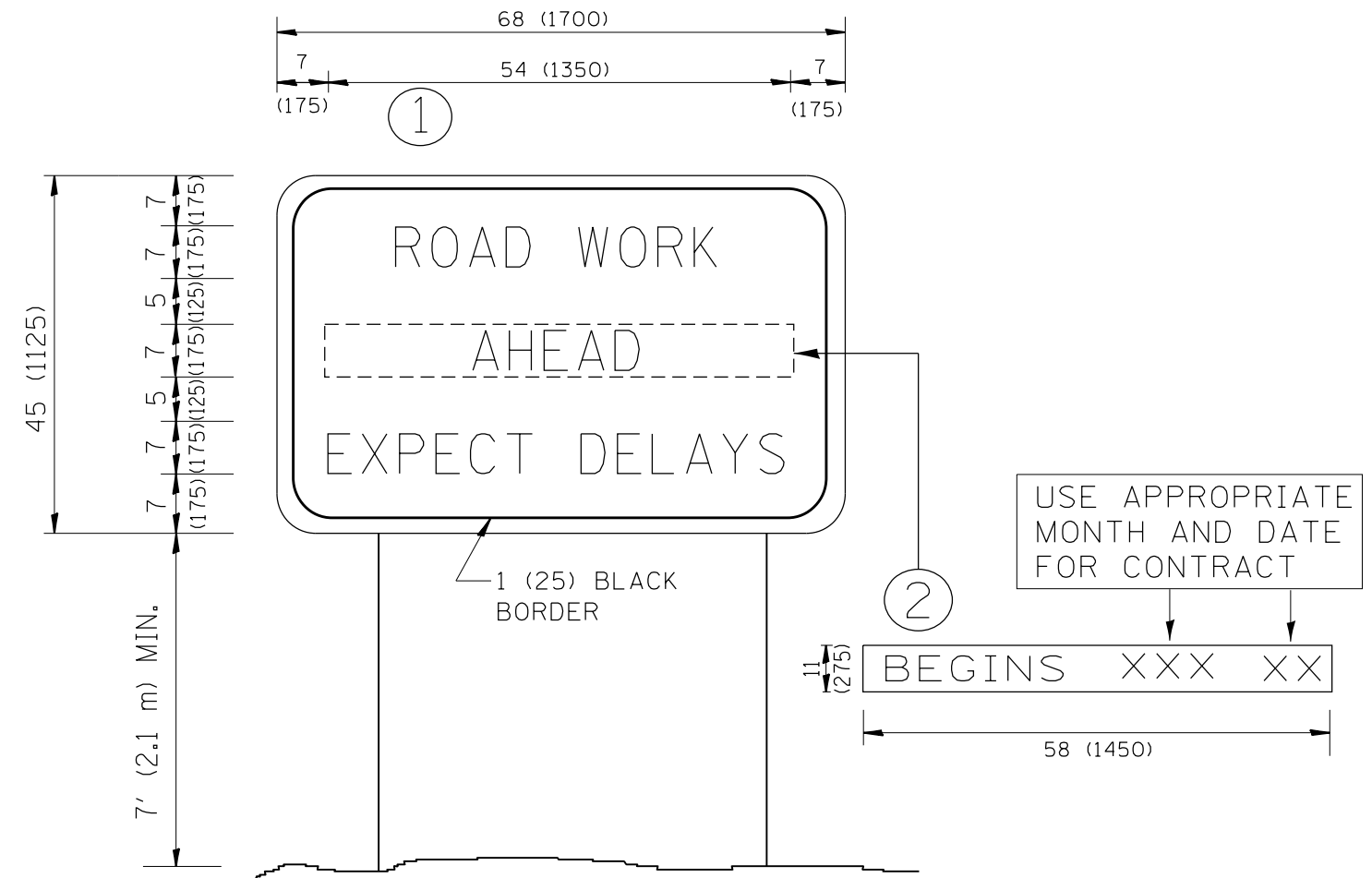
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = chrzesclr	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
et:\pw\work\p\dot\chrzesclr\d0303692\01stgStd.dgn		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 4/10/2012	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2012-016 R5	COOK	33	30
TC-16			CONTRACT NO. 60T62	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = chrzesclr	DESIGNED -	REVISED - R. MIRS 09-15-97
et:\pwork\pwork\chrzesclr\d0303692\01std.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 4/10/2012	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

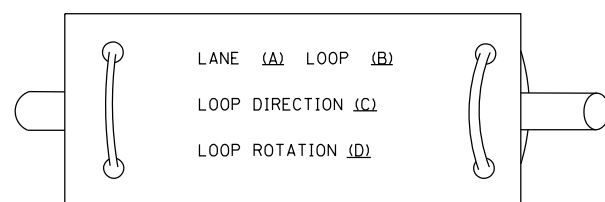
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2012-016 R5	COOK	33	31
TC-22		CONTRACT NO. 60T62		
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				

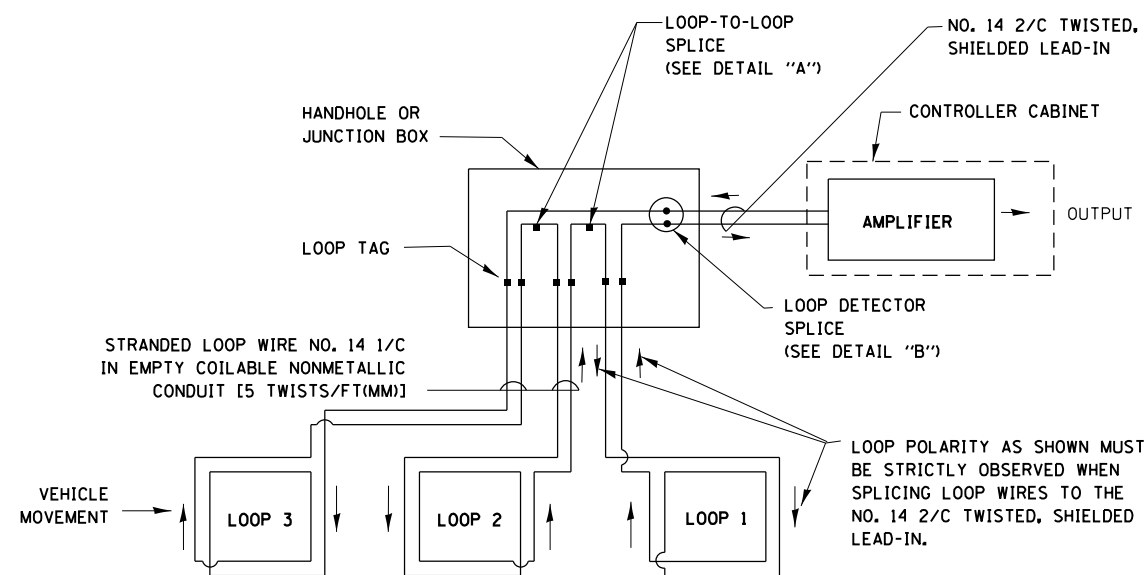
## LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

### LOOP LEAD-IN CABLE TAG

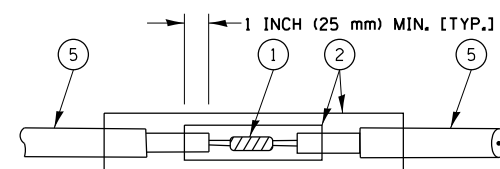


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

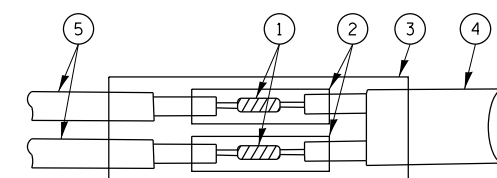


### DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

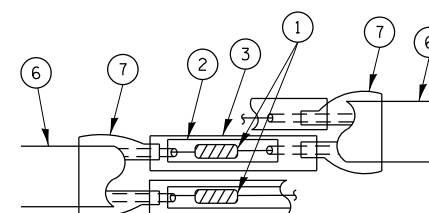


DETAIL "A"  
LOOP-TO-LOOP SPLICE

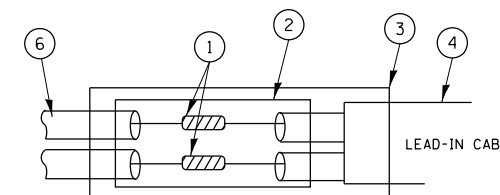


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

### TYPE I LOOP



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

### LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = chrzesclr	DESIGNED - DAD	REVISED -
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	PLOT SCALE = 100.0000' / in.	CHECKED - DAD	REVISED -
	PLOT DATE = 4/10/2012	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

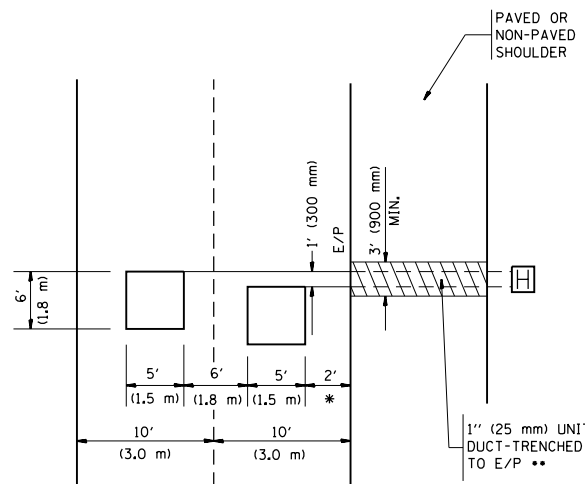
SCALE: NONE SHEET NO. 1 OF 6 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2012-016 R5	COOK	33	32
TS-05			CONTRACT NO. 60T62	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



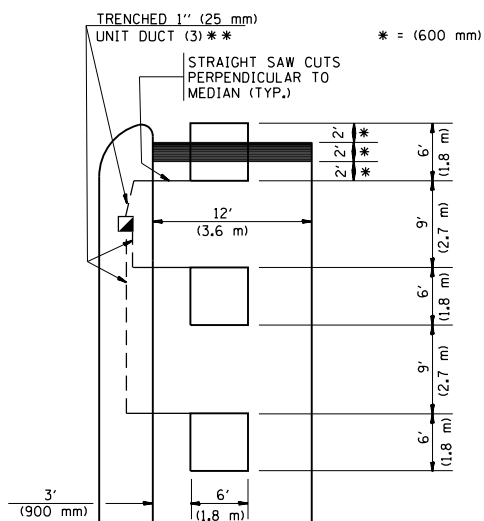
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

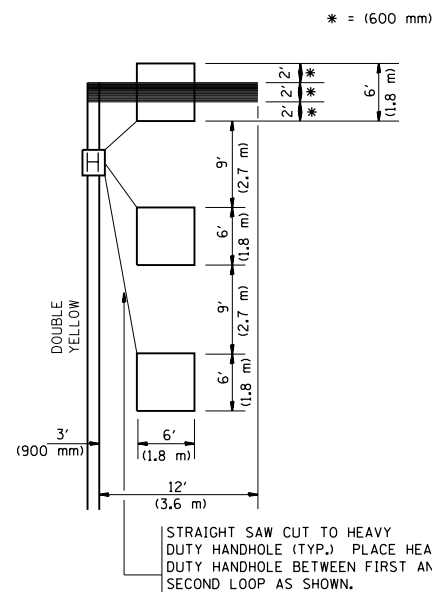


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

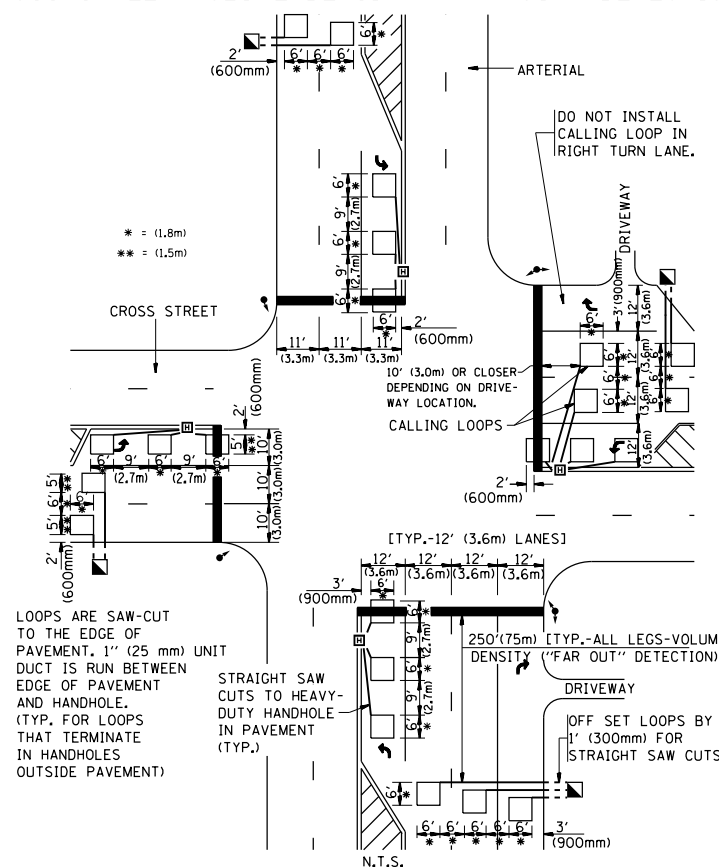
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

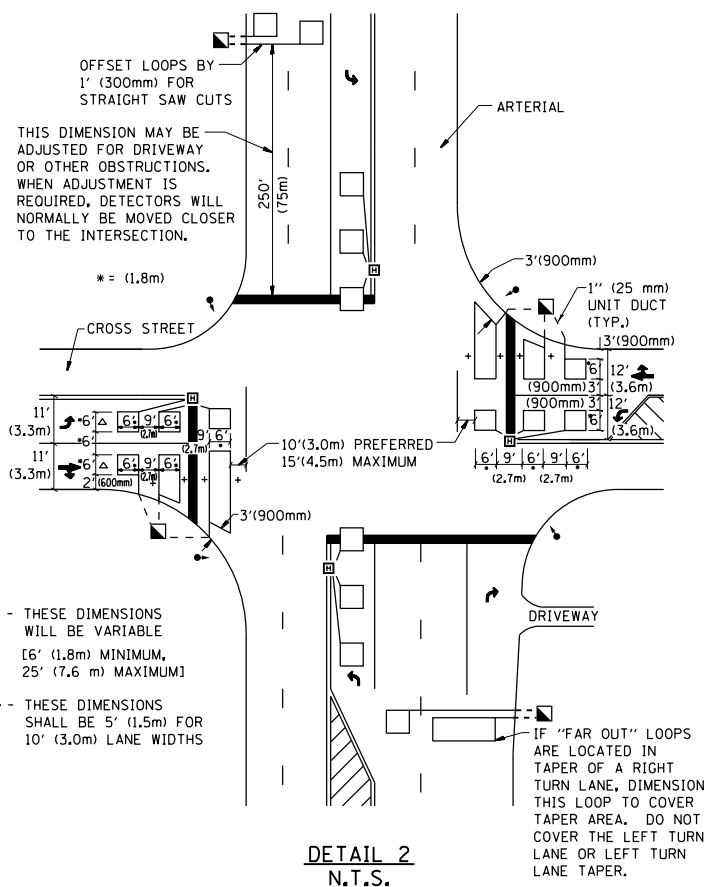
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1  
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2  
N.T.S.

FILE NAME =	USER NAME = chrzesl	DESIGNED -	REVISED -
et:\pw\work\p1dot\chrzesl\d0303692\Dist1Std.dgn		DRAWN -	REVISED -
		CHECKED - R.K.F.	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION  
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2012-016 R5	COOK	33	33
TS-07		CONTRACT NO. 60T62		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				