

06-15-12 LETTING ITEM 205

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR LIST OF HIGHWAY STANDARDS, SEE SHEET 2

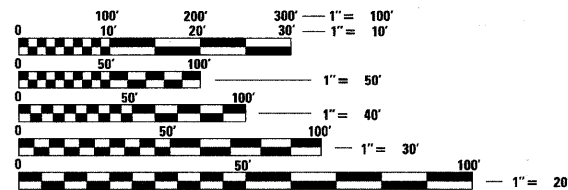
TRAFFIC DATA

ADT:
ILLINOIS ROUTE 59 34,100 VPD (2008) 34,950 VPD (2012)
SHOE FACTORY RD. 7,700 VPD (2008) 8,200 VPD (2012)

SPEED	POSTED SPEED	DESIGN SPEED
ILLINOIS ROUTE 59	45 MPH (EXISTING/ PROPOSED)	45 MPH
SHOE FACTORY RD.	45 MPH (EXISTING/ PROPOSED)	45 MPH

DESIGN DESIGNATION

FAP 338 (ILLINOIS ROUTE 59) - PRINCIPAL ARTERIAL
FAU 1304 (SHOE FACTORY RD.) - MINOR ARTERIAL

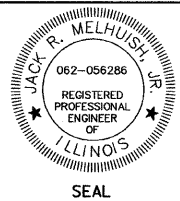


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION CALL 811

PROFESSIONAL ENGINEER'S SIGN & SEAL EXCLUDING SHEET(S):

J.R. Melhuish 4/5/12
JACK R. MELHUISH, P.E.
EXPIRES: 11-30-13



PROFESSIONAL ENGINEER'S SIGN & SEAL FOR LIGHTING SHEET(S):

P. Keranen 4/5/12
PHILIP KERANEN, P.E.
EXPIRES: 11-30-13



**SHOE FACTORY ROAD AT SUTTON ROAD (IL 59)
COUNTY HIGHWAY A62
SECTION 11-A6204-01-CH**

PROJECT ENGINEER: J. ATTANASEO
PROJECT MANAGER: J. MELHUISH

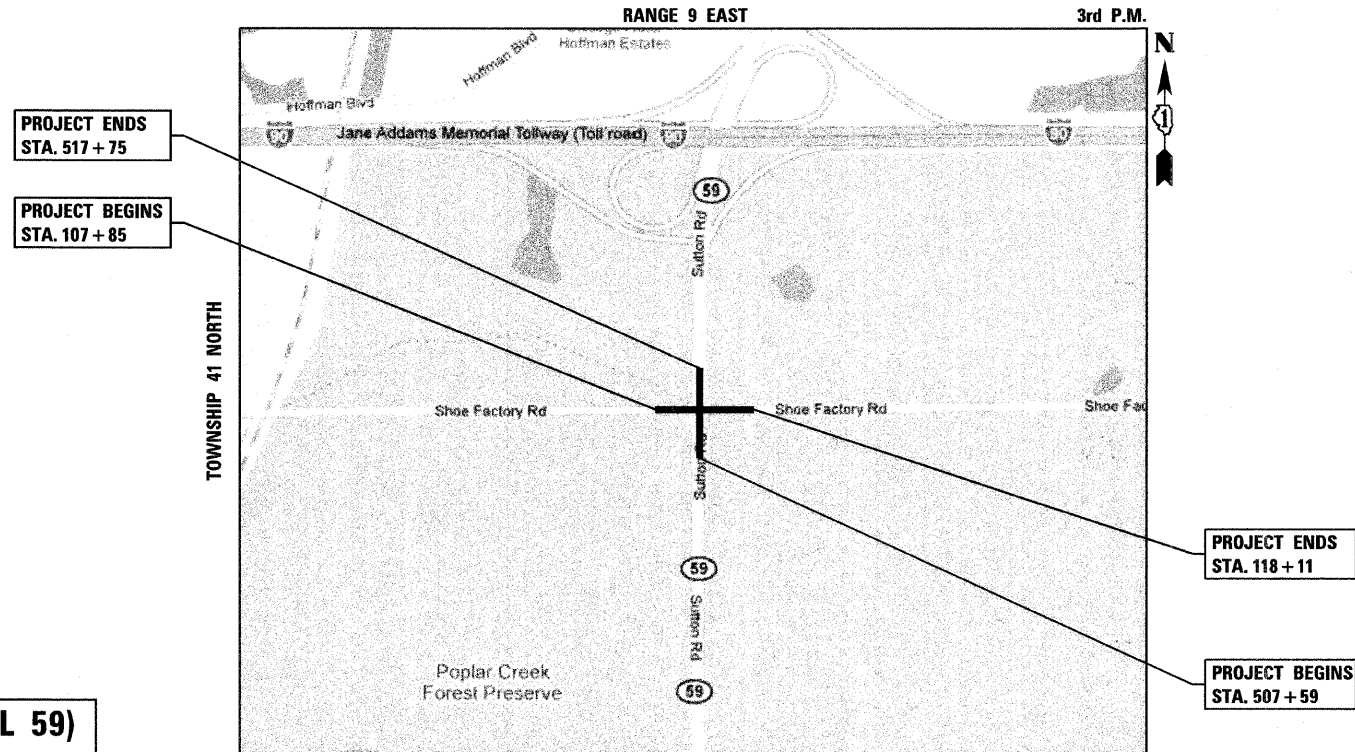
CONTRACT NO. 63711

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL-AID HIGHWAY

FAP 338 (ILLINOIS ROUTE 59)
FAU 1304 (SHOE FACTORY ROAD)
INTERSECTION IMPROVEMENTS
SECTION: 10-00084-00-CH
PROJECT: CMM-9003 (596)
COOK COUNTY
C-91-417-10

PROJECT LOCATION MAP
HANOVER TOWNSHIP
N.T.S.
RANGE 9 EAST



PROJECT ENDS
STA. 517 + 75

PROJECT BEGINS
STA. 107 + 85

PROJECT ENDS
STA. 118 + 11

PROJECT BEGINS
STA. 507 + 59

PROJECT LENGTH
NET LENGTH OF IMPROVEMENT (ILLINOIS ROUTE 59) = 1,013 FT (0.192 MI)
NET LENGTH OF PROJECT (SHOE FACTORY RD.) = 1,026 FT (0.194 MI)
GROSS LENGTH OF PROJECT = 2,039 FT (0.386 MI)

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO.	63711	



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 4/5 2012
Gary Salawitch
VILLAGE ENGINEER, HOFFMAN ESTATES

PASSED APRIL 6 2012
C. Holt C. HOLT
DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
BASED ON LIMITED REVIEW APRIL 6 2012
Diane M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

420 NORTH FRONT STREET, SUITE 100 | McHENRY, ILLINOIS 60050
Phone: 815.385.1778 | Toll Free: 800.728.7805 | Fax: 815.385.1781 | HRGreen.com
ILLINOIS PROFESSIONAL DESIGN FIRM #184-001322

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OF THE STATE OF ILLINOIS**

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. 847-705-4406 SCHAUMBURG, IL

COMPANY NAME: PROJECT CONTACT: CLIENT: DATE PLOTTED: FILE NAME: PLOT DRIVER: PEN TABLE:

GENERAL NOTES

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2012. ALL WORK TO BE COMPLETED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
- PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, HE MUST IMMEDIATELY REPORT THEM TO THE ENGINEER BEFORE DOING ANY WORK, OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1 AND THE VILLAGE OF HOFFMAN ESTATES AT (847) 490-6800 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- ALL ELEVATIONS SHOWN ON THE PLANS ARE ON THE USGS DATUM.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT, UNLESS OTHERWISE NOTED. CURB AND GUTTER ELEVATIONS SHOWN ALONG RETURNS AND AT POINTS OF CURVATURE, ETC. ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.

10. SAW CUTTING OF PAVEMENTS SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE ITEM REMOVED.

- DRIVEWAYS ARE TO BE CONSTRUCTED TO THE R.O.W. OR UNLESS OTHERWISE NOTED.
- REMOVAL OF EXISTING COMBINATION CURB AND GUTTER, REGARDLESS OF CURB AND GUTTER TYPE, SHALL BE PAID FOR AS "COMBINATION CURB AND GUTTER REMOVAL".
- THE ELEVATIONS SHOWN ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT, UNLESS OTHERWISE NOTED.
- ALL SIGNS TO BE REMOVED SHALL REMAIN THE PROPERTY OF IDOT AND SHALL BE RETURNED TO IDOT UPON REMOVAL. SIGNS TEMPORARILY RELOCATED OR COVERED SHALL NOT BECOME THE PROPERTY OF IDOT UNTIL PERMANENTLY REMOVED.
- THE RIGHT TURN LANE WIDTHS THAT ARE 5 FEET OR LESS SHALL BE POURED MONOLITHICALLY WITH THE PROPOSED COMBINATION CONCRETE CURB AND GUTTER.
- A TYPE A CONCRETE PAVEMENT FINISH SHOULD BE USED.

STORM SEWERS, WATER MAINS, AND UTILITIES
 1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.

2. THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.

4. ALL UTILITY COMPANIES SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
 5. UNLESS OTHERWISE NOTED OFFSETS FOR DRAINAGE STRUCTURES LOCATED IN CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT AND OFFSETS FOR DRAINAGE STRUCTURES NOT LOCATED IN THE CURB AND GUTTER ARE TO THE CENTER OF THE STRUCTURE.

6. STORM SEWER LENGTHS ARE MEASURED FROM INSIDE EDGE OF STRUCTURE TO THE INSIDE EDGE OF STRUCTURE, THEN ADDING 0.5' TO EACH END OF PIPE. SLOPE OF PIPE IS CALCULATED BY DIVIDING THE DIFFERENCE IN ELEVATION BY THE PIPE LENGTH.

7. THE PAY ITEM FOR STORM SEWER REMOVAL SHALL INCLUDE ANY TRENCH BACKFILL REQUIRED TO FILL AREA OF SEWER REMOVAL WHERE THERE IS NO REPLACEMENT. IF PROPOSED STORM SEWER IS REPLACED IN THE SAME TRENCH, THEN TRENCH BACKFILL SHALL BE PAID FOR SEPARATELY.

8. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN, IN AN OPERATING CONDITION, TEMPORARY OUTLETS AND CONNECTIONS FOR ALL DRAINS, SEWERS, AND CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES WHICH HAVE THE CAPACITY TO RECEIVE AND DISCHARGE THE STORM WATER FLOW RATES NORMALLY ACCEPTED AND RELEASED BY THE EXISTING DRAINAGE FACILITIES. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.

GENERAL NOTES (CONT.)

- STORM SEWERS, WATER MAINS, AND UTILITIES (CONT.)**
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
 - ALL STORM SEWER PIPES SHALL BE CLEANED PRIOR TO ACCEPTANCE.
 - ALL FRAMES, GRATES, LIDS, AND BOXES SCHEDULED TO BE REMOVED FROM EXISTING STRUCTURES SHALL REMAIN THE PROPERTY AND BE DELIVERED TO THE COOK COUNTY HIGHWAY DEPARTMENT OR IDOT.
 - ANY ITEMS DAMAGED DURING REMOVAL OPERATIONS SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE. THE COST OF SALVAGING EXISTING FRAMES, GRATES, LIDS, OR BOXES AND/OR STOCKPILING THEM ON THE JOB SITE FOR DELIVERY TO THE IDOT OR CCHD, AS APPLICABLE SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
 - ALL FRAMES WITH CLOSED LIDS TO BE FURNISHED AS PART OF THE CONTRACT FOR CONSTRUCTION, ADJUSTMENT OR RECONSTRUCTION OF ANY MANHOLE, CATCH BASIN, INLET, VALVE VAULT OR METER VAULT SHALL HAVE CAST INTO THE LID ONE OF THE FOLLOWING WORDS: ALL LIDS TO BE USED ON STORM SEWER STRUCTURES SHALL BEAR THE WORD "STORM." ALL LIDS TO BE USED ON SANITARY SYSTEM STRUCTURES SHALL BEAR THE WORD "SANITARY." ANY ADDITIONAL COST FOR THIS REQUIREMENT WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE FRAMES AND CLOSED LIDS PROVIDED.
 - FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW, ADJUSTED OR RECONSTRUCTED STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE STRUCTURE, ADJUSTMENT OR RECONSTRUCTION COST.
 - ALL STORM SEWER SHALL BE REINFORCED CONCRETE PIPE WITH FLEXIBLE GASKET MEETING THE REQUIREMENTS OF ASTM C361. THIS ITEM WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE PRICE OF STORM SEWER.
 - WHERE NEW WORK MEETS EXISTING FEATURES TO REMAIN, FIELD CHECK ALL DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH CONSTRUCTION, NOTIFY ENGINEER IMMEDIATELY OF ANY DISCREPANCIES.
 - THE INDISCRIMINATE USE OF FIRE HYDRANTS OR EXISTING STREAMS, CREEKS, WETLANDS OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE ENGINEER PRIOR TO USE OF THE WATER.
 - FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.

BACKFILL

- FOR STORM SEWER CONSTRUCTED UNDER THE ROADWAY, BACKFILLING METHODS TWO AND THREE AUTHORIZED UNDER THE PROVISIONS OF ARTICLE 550.07 OF THE STANDARD SPECIFICATIONS WILL NOT BE ALLOWED.
- PROVIDE TRENCH BACKFILL FOR ALL UTILITY LINES WITHIN 2' OF PAVED AREAS. ALL TRENCH BACKFILL QUANTITIES FOR STORM SEWER HAVE BEEN COMPUTED AND SHALL BE PAID FOR IN ACCORDANCE WITH THE STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS BUREAU OF CONSTRUCTION TRENCH BACKFILL TABLE, BASED ON PIPE SIZE AND INVERT DEPTH FROM SUBGRADE.
- COST OF ANY TRENCH BACKFILL REQUIRED IN EXCESS OF THE QUANTITY ESTABLISHED IN THE DRAINAGE SCHEDULES, INCLUDING BEDDING MATERIAL, SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE BID.

SIGNING AND STRIPING

- SEE IDOT DISTRICT ONE DETAILS AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.
- SIGNS SHALL NOT BE MOVED OR COVERED UNTIL PROGRESS OF WORK NECESSITATES IT.
- THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL SUCH SIGNS THAT INTERFERE WITH HIS CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING AND MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
- ALL SIGNS SHALL BE INSTALLED IN PERMANENT LOCATIONS AS THE ROADWAY IS COMPLETED. THIS WORK SHALL BE PAID FOR USING THE APPROPRIATE PAY ITEMS.
- LONGER POSTS MAY BE REQUIRED AT SOME TEMPORARY OR PERMANENT SIGN LOCATIONS TO MAINTAIN PROPER SIGN ELEVATIONS. THIS WORK SHALL BE PAID FOR IN ACCORDANCE WITH SECTION 728 OF THE STANDARD SPECIFICATIONS.
- THE RESIDENT ENGINEER SHALL CONTACT WALT CZARNY, AREA TRAFFIC FIELD ENGINEER AT 847-715-8419 AT LEAST TWO (2) WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS.

GENERAL NOTES (CONT.)

- EROSION CONTROL PLANS**
- SEE EROSION CONTROL PLANS FOR GENERAL NOTES CONCERNING EROSION CONTROL.
- TRAFFIC CONTROL**
- SEE TRAFFIC CONTROL PLANS FOR GENERAL NOTES CONCERNING TRAFFIC CONTROL AND PROTECTION.
 - THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.
- EARTHWORK**
- PRIOR TO ANY EMBANKMENT PLACEMENT ALL VEGETATION AND UNSTABLE MATERIAL SHOULD BE REMOVED TO DEPTH ENCOUNTERED AND REPLACED WITH SUITABLE EMBANKMENT MATERIAL.
 - SUBGRADE SHALL BE PREPARED IN ACCORDANCE WITH ARTICLE 301.03 OF THE STANDARD SPECIFICATIONS BEFORE REMOVAL OF ANY UNSTABLE MATERIAL.

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STATE STANDARDS

STANDARD NO.	LIST OF DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERS
280001-06	TEMPORARY EROSION CONTROL SYSTEMS
420001-07	PAVEMENT JOINTS
420111-03	PCC PAVEMENT ROUNDOUTS
424001-06	PERPENDICULAR CURB RAMPS FOR SIDEWALK
442201-03	CLASS C AND D PATCHES
482006-03	HMA SHOULDER ADJACENT TO RIGID PAVEMENT
542116-02	REINFORCED CONCRETE END SECTIONS FOR MULTIPLE (2 & 3) PIPE CULVERTS 15" (375 mm) THRU 36" (900 mm) DIA. AT RIGHT ANGLES WITH ROADWAY
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
602001-02	CATCH BASIN TYPE A
602011-02	CATCH BASIN TYPE C
602301-03	INLET, TYPE A
602401-03	MANHOLE, TYPE A
602701-02	MANHOLE STEPS
604001-03	FRAME AND LIDS, TYPE 1
604036-02	GRATE, TYPE 8
604051-03	FRAME AND GRATE, TYPE 11
604091-02	FRAME AND GRATE, TYPE 24
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
630001-10	STEEL PLATE BEAM GUARDRAIL
630201-06	PCC/HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
701101-02	OFF-RD OPERATIONS, MULTILANE 15' (4.5M) TO 24" (600MM) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701502-04	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-03	SIGN PANEL ERECTION DETAILS
728001-01	TELESCOPING STEEL SIGN SUPPORT
729001-01	APPLICATIONS OF TYPES A AND B METAL POSTS (FOR SIGNS & MARKERS)
731001-01	BASE FOR TELESCOPING STEEL SIGN SUPPORT
814001-02	HANDHOLES
814006-02	DOUBLE HANDHOLES
836001-01	LIGHT POLE FOUNDATION
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
876001-01	PEDESTRIAN PUSH BUTTON POST
878001-08	CONCRETE FOUNDATION DETAILS
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: Illinois Professional Design Firm #194-001922
 CLIENT: HRGreen
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 Illinois Professional Design Firm
 #194-001922

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PLOT DATE = 4/5/2012	DATE - 4/5/12	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES, INDEX, AND LIST OF STATE STANDARDS
 IL 59 & SHOE FACTORY RD**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	2
CONTRACT NO.			63711	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES

PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL	ROADWAY 80% FEDERAL 20% LOCAL 0004	SIGNALS 80% FEDERAL 20% LOCAL 0021	LIGHTING 80% FEDERAL 20% LOCAL 0021
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	4,162	4,162		
20400800	FURNISHED EXCAVATION	CU YD	169	169		
20800150	TRENCH BACKFILL	CU YD	71	71		
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	4,051	4,051		
25000210	SEEDING, CLASS 2A	ACRE	0.84	0.84		
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	51	51		
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	51	51		
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	51	51		
25100630	EROSION CONTROL BLANKET	SQ YD	4,051	4,051		
28000305	TEMPORARY DITCH CHECKS	FOOT	100	100		
28000400	PERIMETER EROSION BARRIER	FOOT	1,200	1,200		
28000500	INLET AND PIPE PROTECTION	EACH	2	2		
28000510	INLET FILTERS	EACH	14	14		
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	3,040	3,040		
31101600	SUBBASE GRANULAR MATERIAL, TYPE B 8"	SQ YD	75	75		
31200100	STABILIZED SUBBASE 4"	SQ YD	3,040	3,040		
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	7	7		
42000511	PORTLAND CEMENT CONCRETE PAVEMENT 10-1/2" (JOINTED)	SQ YD	2,400	2,400		
42001300	PROTECTIVE COAT	SQ YD	2,901	2,901		
42400800	DETECTABLE WARNINGS	SQ FT	65	65		
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	325	325		
44000100	PAVEMENT REMOVAL	SQ YD	910	910		
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	75	75		
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1,540	1,540		
44004250	PAVED SHOULDER REMOVAL	SQ YD	854	854		
44200986	CLASS B PATCHES, TYPE III, 11"	SQ YD	65	65		
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	158	158		
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	1	1		
54215424	CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 24"	EACH	2	2		
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	416	416		
550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	24	24		
55100700	STORM SEWER REMOVAL 15"	FOOT	10	10		
55100900	STORM SEWER REMOVAL 18"	FOOT	95	95		
60107600	PIPE UNDERDRAINS 4"	FOOT	1,364	1,364		
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	1		
60200405	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 4 FRAME AND GRATE	EACH	2	2		
60207605	CATCH BASINS, TYPE C, TYPE 8 GRATE	EACH	1	1		
60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	3	3		
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1		
60222240	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	1	1		
60237470	INLETS, TYPE A, TYPE 24 FRAME AND GRATE	EACH	5	5		
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	205	205		
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	1,325	1,325		
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	1,102	1,102		
60619600	CONCRETE MEDIAN, TYPE SB-6.12	SQ FT	584	584		
* 63301210	REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL, TYPE A	FOOT	25	25		

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen
 CLIENT: HRGreen
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 PLOT DRIVER: pct.plt
 PLOT TABLE: Standard-Trans.tbl


 HRGreen.com
 Illinois Professional Design Firm
 # 194-01322

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PLOT SCALE = N.T.S.	DRAWN -	REVISED -
PLOT DATE = 4/11/2012	CHECKED - TEH	REVISED -
	DATE - 4/11/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES	
IL 59 & SHOE FACTORY RD	
SCALE: N.T.S.	SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A.P. RTE. 338	SECTION 10-00084-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 3
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63711	

SUMMARY OF QUANTITIES

PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL	ROADWAY 80% FEDERAL 20% LOCAL 0004	SIGNALS 80% FEDERAL 20% LOCAL 0021	LIGHTING 80% FEDERAL 20% LOCAL 0021
67100100	MOBILIZATION	L SUM	1	1		
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1		
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1		
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1		
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1		
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DAY	60	60		
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	12	12		
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	295	295		
70300220	TEMPORARY PAVEMENT MARKING-LINE 4"	FOOT	6,505	6,505		
70300240	TEMPORARY PAVEMENT MARKING-LINE 6"	FOOT	1,185	1,185		
70300280	TEMPORARY PAVEMENT MARKING-LINE 24"	FOOT	75	75		
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	1,000	1,000		
72000100	SIGN PANEL-TYPE 1	SQ FT	70	55	15	
72000200	SIGN PANEL-TYPE 2	SQ FT	21		21	
72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	200	200		
* 78008200	POLYUREA PAVEMENT MARKING TYPE I - LETTERS AND SYMBOLS	SQ FT	255	255		
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	1,395	1,395		
* 78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	2,970	2,970		
* 78008250	POLYUREA PAVEMENT MARKING TYPE I - LINE 12"	FOOT	650	650		
* 78008270	POLYUREA PAVEMENT MARKING TYPE I - LINE 24"	FOOT	75	75		
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	97	97		
78300100	PAVEMENT MARKING REMOVAL	SQ FT	790	790		
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	66	66		
* 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	986		986	
* 81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	98		28	70
* 81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	47		47	
* 81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	508		508	
* 81400100	HANDHOLE	EACH	4		4	
* 81400300	DOUBLE HANDHOLE	EACH	1		1	
* 81603081	UNIT DUCT, 600V, 3-1C NO.2, 1/C NO.4 GROUND, (XLP-TYPE USE), 1 1/2" DIA. POLYETHYLENE	FOOT	3,084			3,084
* 83600355	LIGHT POLE FOUNDATION, METAL, 15" BOLT CIRCLE, 8" X 6"	EACH	7			7
* 83800205	BREAKAWAY DEVICE, TRANSFORMER BASE, 15 INCH BOLT CIRCLE	EACH	7			7
* 84200500	REMOVAL OF LIGHTING UNIT, SALVAGE	EACH	9		2	7
* 84200804	REMOVAL OF POLE FOUNDATION	EACH	7			7
* 84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	7			7
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2		2	
* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	707.5		707.5	
* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1,197		1,197	
* 87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	614.5		614.5	
* 87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	3,100		3,100	
* 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1,802		1,802	
* 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	1,247		1,247	
* 87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	2		2	
* 87700340	STEEL MAST ARM ASSEMBLY AND POLE, 58 FT.	EACH	1		1	
* 87700408	STEEL MAST ARM ASSEMBLY AND POLE, 64 FT.	EACH	1		1	
* 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	8		8	

COMPANY NAME: HRGreen
 PROJECT CONTACT: HRGreen
 CLIENT: HRGreen
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DRAWN -	REVISOR -	
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PLLOT DATE = 4/11/2012	DATE - 4/11/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: N.T.S.			SHEET NO. 2 OF 3 SHEETS		STA. TO STA.
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SUMMARY OF QUANTITIES
IL 59 & SHOE FACTORY RD

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	4
CONTRACT NO.				63711
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES

PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL	ROADWAY 80% FEDERAL 20% LOCAL 0004	SIGNALS 80% FEDERAL 20% LOCAL 0021	LIGHTING 80% FEDERAL 20% LOCAL 0021
* 87800420	CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER	FOOT	42		42	
* 87900200	DRILL EXISTING HANDHOLE	EACH	10		10	
* 88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	2		2	
* 88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	8		8	
* 88030220	SIGNAL HEAD, LED, 2-FACE, 5-SECTION, BRACKET MOUNTED	EACH	4		4	
* 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4		4	
* 88200110	TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	14		14	
* 88500100	INDUCTIVE LOOP DETECTOR	EACH	2		2	
* 88600100	DETECTOR LOOP, TYPE 1	FOOT	1,049		1,049	
* 88700200	LIGHT DETECTOR	EACH	2		2	
* 88700300	LIGHT DETECTOR AMPLIFIER	EACH	1		1	
* 88800100	PEDESTRIAN PUSH-BUTTON	EACH	4		4	
* 89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1		1	
* 89502200	MODIFY EXISTING CONTROLLER	EACH	1		1	
* 89502210	MODIFY EXISTING CONTROLLER CABINET	EACH	1		1	
* 89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	7,241		6,941	300
* 89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	5,885		5,885	
* 89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1		1	
* 89502380	REMOVE EXISTING HANDHOLE	EACH	6		5	1
* 89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	4		4	
X0322936	REMOVE EXISTING FLARED END SECTION	EACH	2	2		
* X0324085	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	336		336	
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	1	1		
X4402020	CONCRETE MEDIAN SURFACE REMOVAL	SQ FT	3,310	3,310		
* X8410102	TEMPORARY LIGHTING SYSTEM	L SUM	1			1
* X8620200	UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1		1	
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1		
Z0017400	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	EACH	2	2		
Z0018700	DRAINAGE STRUCTURE TO BE REMOVED	EACH	8	8		
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	105	105		
* Z0033020	LUMINAIRE SAFETY CABLE ASSEMBLY	EACH	7			7
* Z0033028	MAINTENANCE OF LIGHTING SYSTEM	CAL MO	3			3
* Z0033046	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2	EACH	1		1	
Z0042002	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	1,474	1,474		
Z0066600	STABILIZED DRIVEWAYS 8"	SQ YD	75	75		
Z0070200	SURVEY MONUMENTS	EACH	1	1		
* Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1		1	
X Z0076600	TRAINEES	HOUR	500	500		

* SPECIALTY ITEM

X CONSTRUCTION TYPE CODE 0042

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: jmelhus
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION
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 PLOT DRIVER: perfPlot
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PLOT SCALE = N.T.S.	DRAWN -	REVISED -
PLOT DATE = 4/11/2012	CHECKED - TEH	REVISED -
	DATE - 4/11/12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
IL 59 & SHOE FACTORY RD**

SCALE: N.T.S. SHEET NO. 3 OF 3 SHEETS STA. TO STA.

F.A.P. RTE. 338	SECTION 10-00084-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 5
CONTRACT NO. 63711			ILLINOIS FED. AID PROJECT	

ILLINOIS ROUTE 59			
STATION	REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL	FURNISHED EXCAVATION	POROUS GRANULAR EMBANKMENT, SUBGRADE
507+66.17	0.0	0.0	0.0
508+00.00	18.0	0.0	5.3
508+50.00	54.3	0.3	19.7
508+90.14	63.7	0.8	22.9
509+00.00	18.6	0.5	6.7
509+50.00	107.2	6.0	39.0
510+00.00	126.7	5.2	44.0
510+14.46	39.2	0.3	13.0
510+50.00	94.6	0.3	31.3
511+00.00	125.4	0.6	42.6
511+40.00	136.3	0.3	48.5
512+30.00	0.0	0.0	0.0
512+50.00	52.9	0.2	11.1
513+00.00	137.6	2.8	48.8
513+50.00	121.7	3.9	43.2
513+93.11	111.0	1.9	38.5
514+00.00	18.0	0.2	6.1
514+50.00	140.3	5.4	47.3
515+00.00	186.0	9.4	63.3
515+15.48	69.1	2.9	23.8
515+50.00	155.4	5.9	53.1
516+00.00	217.2	6.8	76.7
516+40.50	158.9	2.3	57.1
516+50.00	33.4	0.1	11.9
517+00.00	145.5	0.3	53.1
517+50.00	100.6	0.1	37.0
TOTALS	2,431.5	56.4	844.1
	(CU YD)	(CU YD)	(CU YD)

SHOE FACTORY ROAD			
STATION	REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL	FURNISHED EXCAVATION	POROUS GRANULAR EMBANKMENT, SUBGRADE
107+89.36	0.0	0.0	0.0
108+00.00	1.4	0.1	0.0
108+50.00	16.9	0.3	4.4
109+00.00	38.2	2.8	13.4
109+50.00	60.0	6.9	22.6
110+00.00	78.5	7.9	30.3
110+50.00	87.5	5.2	33.3
111+00.00	87.7	4.5	33.3
111+50.00	85.6	6.2	33.3
111+90.00	175.5	3.8	66.8
112+00.00	83.4	1.1	32.1
113+10.00	0.0	0.0	0.0
113+40.00	125.8	0.8	45.4
113+50.00	28.2	0.1	8.9
113+56.83	16.7	1.1	5.8
114+00.00	92.4	13.1	36.2
114+50.00	109.0	14.2	41.9
115+00.00	111.1	11.7	41.9
115+50.00	117.8	8.1	42.2
116+00.00	120.6	7.8	42.4
116+50.00	107.4	9.3	38.4
117+00.00	93.9	5.4	29.5
117+32.09	45.2	1.4	13.8
117+50.00	18.3	0.6	6.1
118+00.00	29.2	0.1	8.0
TOTALS	1,730.4	112.3	630.2
	(CU YD)	(CU YD)	(CU YD)

NOTE:
ESTIMATED VALUE OF POROUS GRANULAR EMBANKMENT, SUBGRADE. ACTUAL QUANTITY TO BE DETERMINED IN THE FIELD.

HR GREEN, INC.
COMPANY NAME
PROJECT CONTACT: J. MELHUISH
CLIENT: VILLAGE OF HOFFMAN ESTATES
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PLOT DATE: 4/5/2012
PLOT SCALE: N.T.S.



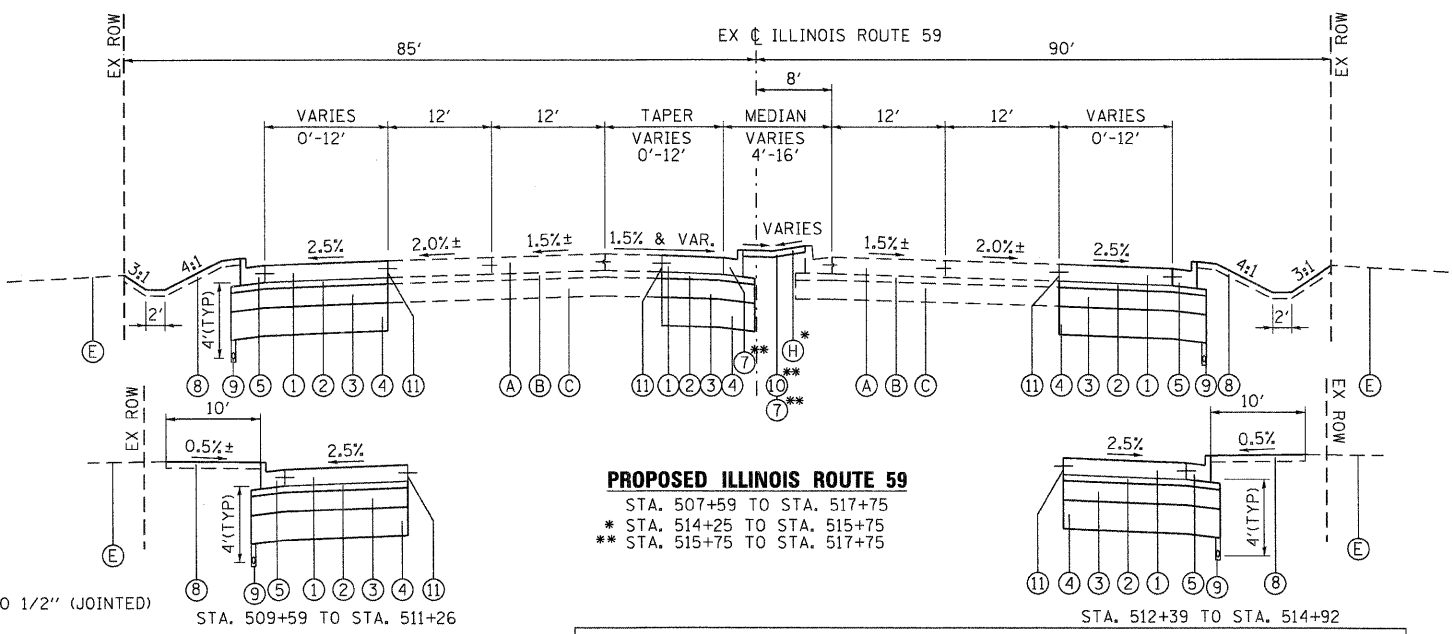
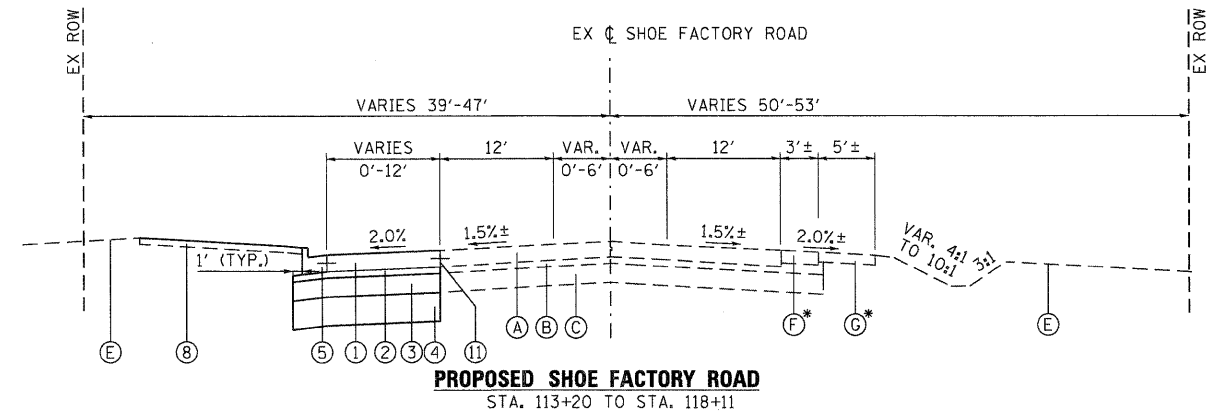
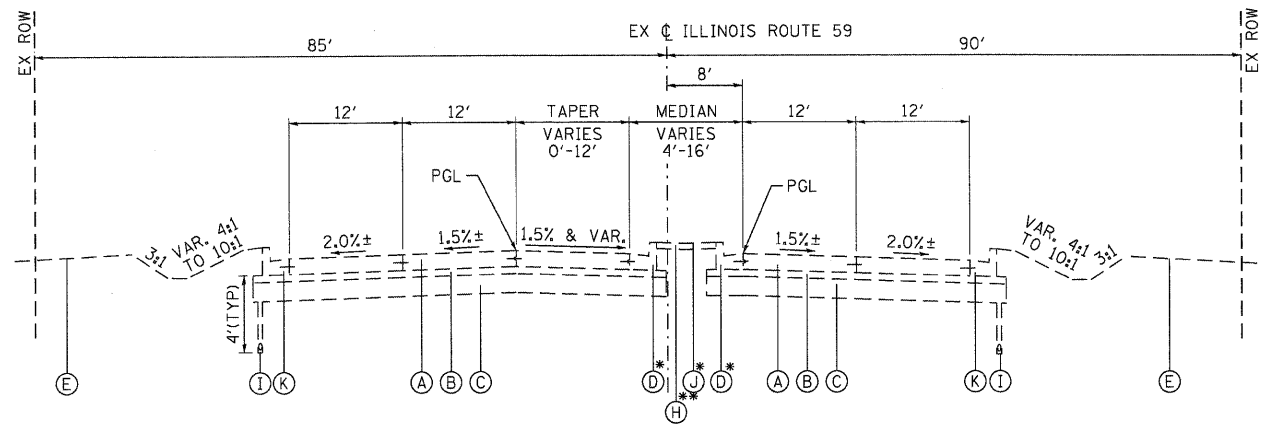
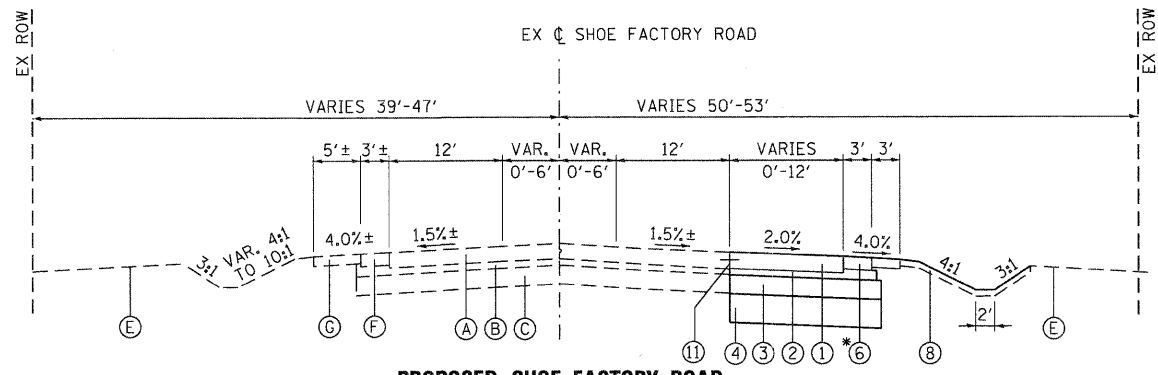
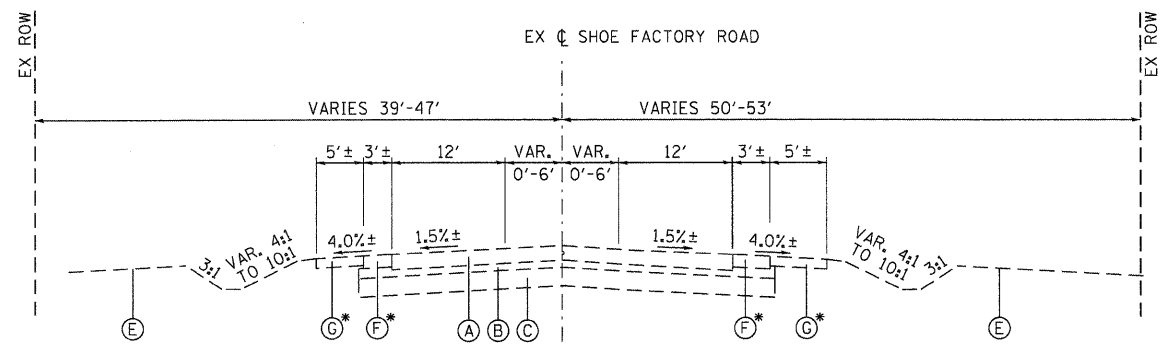
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PLOT DATE = 4/5/2012	DATE - 4/5/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
IL 59 & SHOE FACTORY RD

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	6
CONTRACT NO.			63711	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



NOTE:
 POROUS GRANULAR EMBANKMENT, SUBGRADE (PGES) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR USE OF PGES SHOULD BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY A QUALIFIED SOILS INSPECTOR. AFTER EXCAVATION TO ROUGH SUBGRADE ELEVATION, THE ENGINEER OR SOILS INSPECTOR SHOULD OBSERVE THE BEHAVIOR OF THE ENTIRE EXPOSED SUBGRADE UNDER THE TRAFFIC OF HEAVY RUBBER-TIRED CONSTRUCTION EQUIPMENT SUCH AS MOTOR GRADERS OR FULLY LOADED DUMP TRUCKS. ALL POTENTIALLY UNSUITABLE SOIL SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE STANDARD SPECIFICATIONS AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL (SSM). ANY PGES OR FABRIC NOT NEEDED AT THE TIME OF CONSTRUCTION SHOULD BE DELETED FROM THE CONTRACT WITH NO ADDITIONAL COMPENSATION DUE THE CONTRACTOR.

EXISTING TYPICAL SECTION LEGEND

- (A) PCC PAVEMENT, 10 1/2"
- (B) STABILIZED SUB-BASE, 4"
- (C) AGGREGATE SUBGRADE, 12"
- (D) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (E) EXISTING GROUND
- (F) HOT-MIX ASPHALT SHOULDER, 8"
- (G) AGGREGATE SHOULDER, 6"
- (H) CONCRETE MEDIAN, TYPE SM-6.12
- (I) PIPE UNDERDRAIN, 4" (FABRIC LINED TRENCH)
- (J) CONCRETE MEDIAN SURFACE, 4 INCH
- (K) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24

PROPOSED TYPICAL SECTION LEGEND

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)
- (2) STABILIZED SUBBASE 4"
- (3) AGGREGATE SUBGRADE IMPROVEMENT 12"
- (4) POROUS GRANULAR EMBANKMENT, SUBGRADE; 18" (TYP) (SEE NOTE)
- (5) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (DOWELLED)
- (6) HOT-MIX ASPHALT SHOULDERS, 8"
- (7) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (8) TOPSOIL FURNISH AND PLACE, 4", SEEDING, CLASS 2A
- (9) PIPE UNDERDRAINS 4"
- (10) CONCRETE MEDIAN SURFACE, 4 INCH
- (11) SAW CUTTING, (FULL DEPTH)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
SHOULDER RECONSTRUCTION	
HMA SHOULDER (HMA BINDER IL-19mm); 8" (2 LIFTS)	4% @ 50 GYR.
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2"	4% @ 50 GYR.
SHARED-USE PATH	
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2"	4% @ 50 GYR.
DRIVEWAYS	
HMA SURFACE COURSE, MIX "C", N50; (IL 9.5mm); 2"	4% @ 50 GYR.
HMA BASE COURSE (HMA BINDER IL-19 mm); PE-6", CE-8"	4% @ 50 GYR.

* THE UNIT WEIGHT TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN
 * THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE SPECIAL PROVISIONS.

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: jmelhui
 CLIENT: HRGreen
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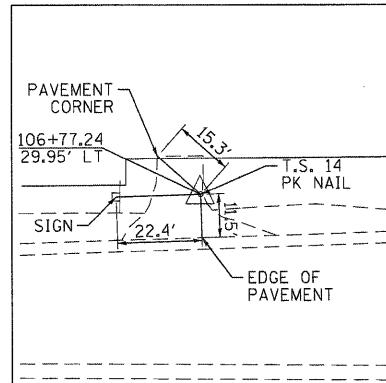


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	DRAWN: -	REVISED: -
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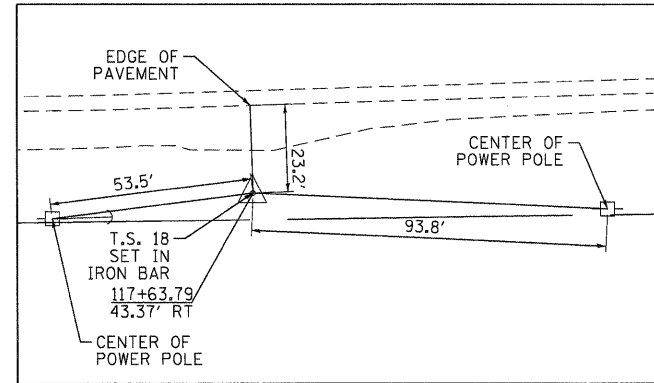
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS IL 59 & SHOE FACTORY RD	
SCALE: N.T.S.	SHEET NO. 1 OF 1 SHEETS
STA. TO STA.	

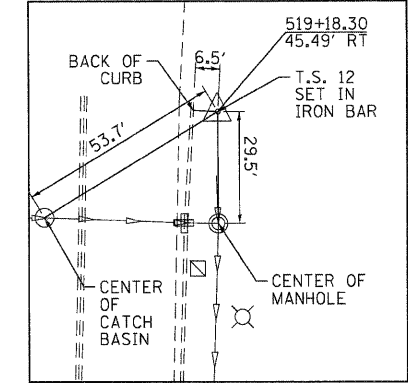
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CONTRACT NO. 63711			ILLINOIS FED. AID PROJECT	



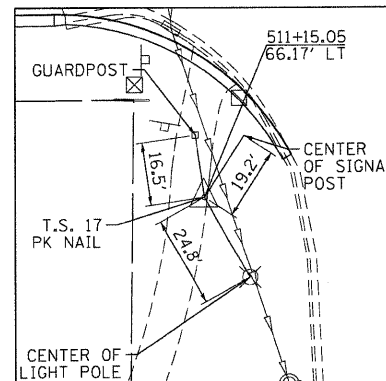
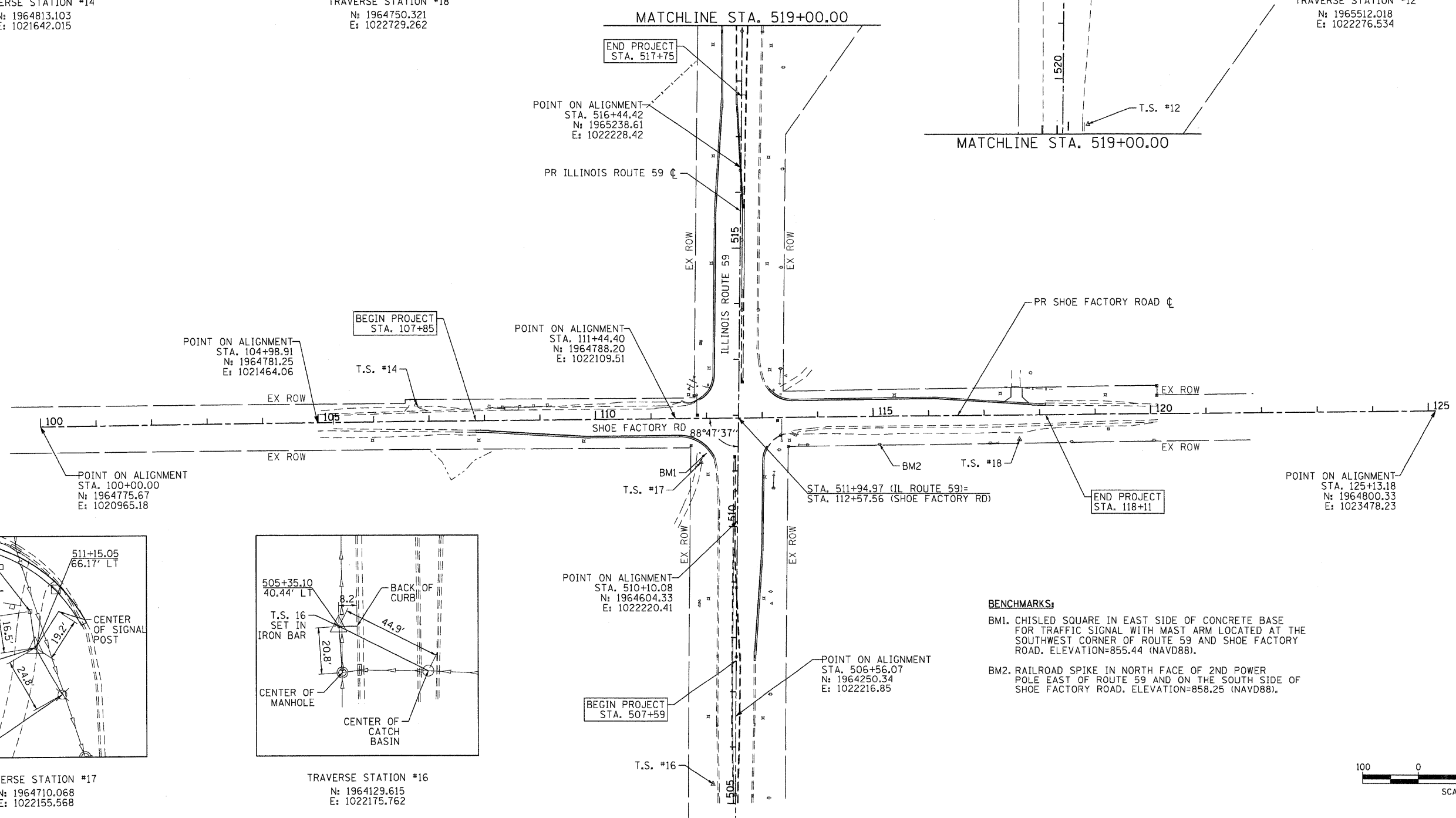
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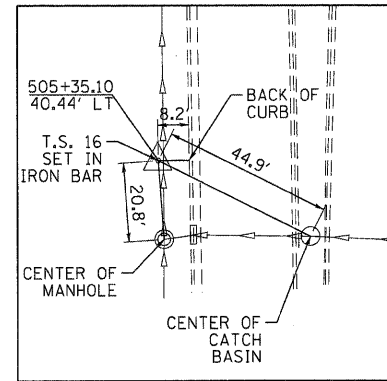
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 E: 1022276.534

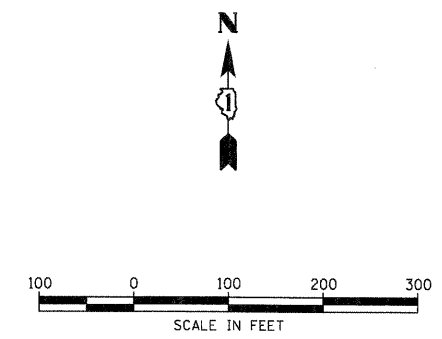


TRAVERSE STATION #17
 N: 1964710.068
 E: 1022155.568



TRAVERSE STATION #16
 N: 1964129.615
 E: 1022175.762

BENCHMARKS:
 BM1. CHISLED SQUARE IN EAST SIDE OF CONCRETE BASE FOR TRAFFIC SIGNAL WITH MAST ARM LOCATED AT THE SOUTHWEST CORNER OF ROUTE 59 AND SHOE FACTORY ROAD. ELEVATION=855.44 (NAVD88).
 BM2. RAILROAD SPIKE IN NORTH FACE OF 2ND POWER POLE EAST OF ROUTE 59 AND ON THE SOUTH SIDE OF SHOE FACTORY ROAD. ELEVATION=858.25 (NAVD88).



COMPANY NAME: HRGreen
 PROJECT CONTACT: jmelhus
 CLIENT: HRGreen
 DATE PLOTTED: 4/5/2012 5:03:15 PM
 FILE NAME: 10-00084-00-CH
 PLOT SCALE: 1"=100'
 PEN TABLE: Standard-Trans.tbl



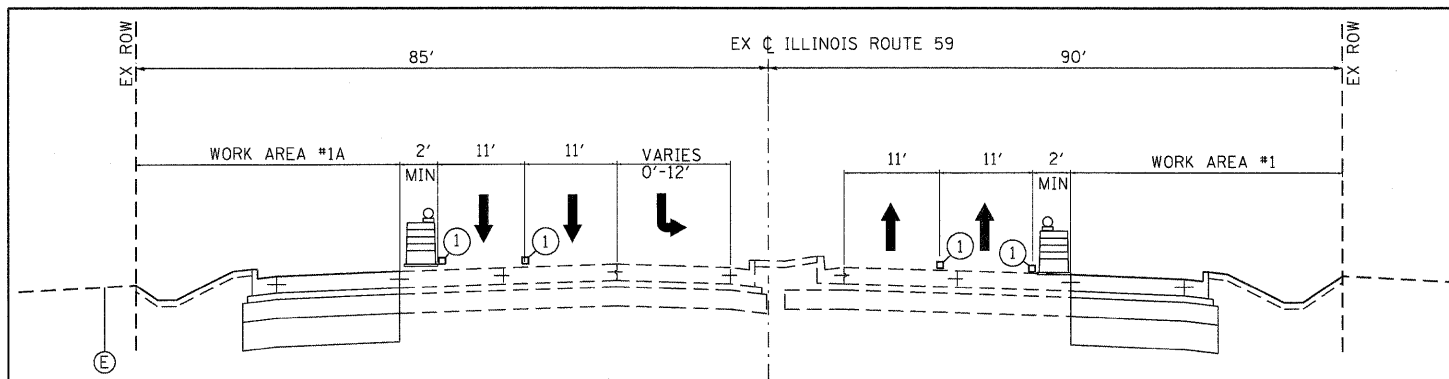
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	DATE - 4/5/12	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

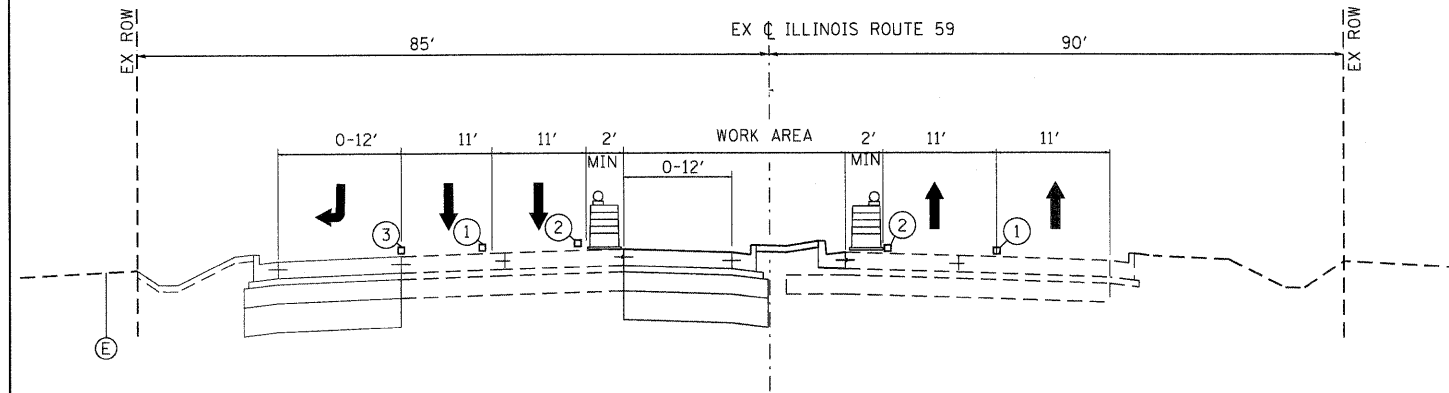
**ALIGNMENT, TIE AND BENCHMARK PLAN
 IL 59 & SHOE FACTORY RD**

SCALE: 1"=100' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

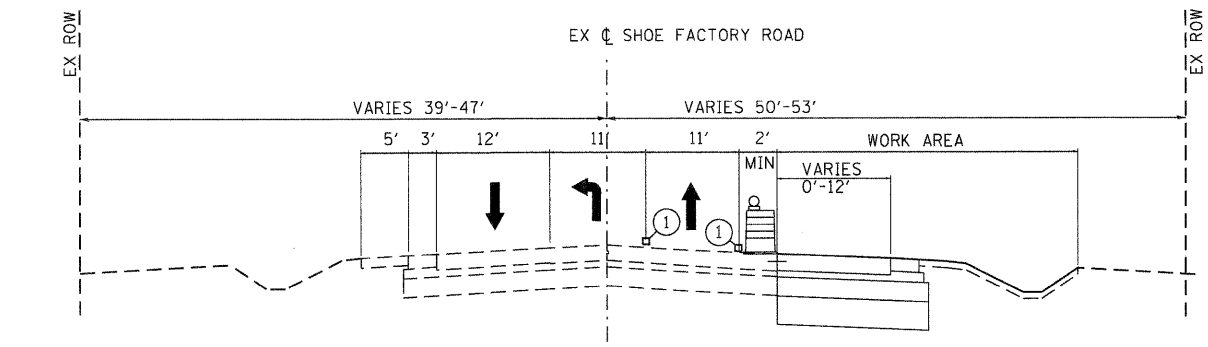
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	8
CONTRACT NO. 63711				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



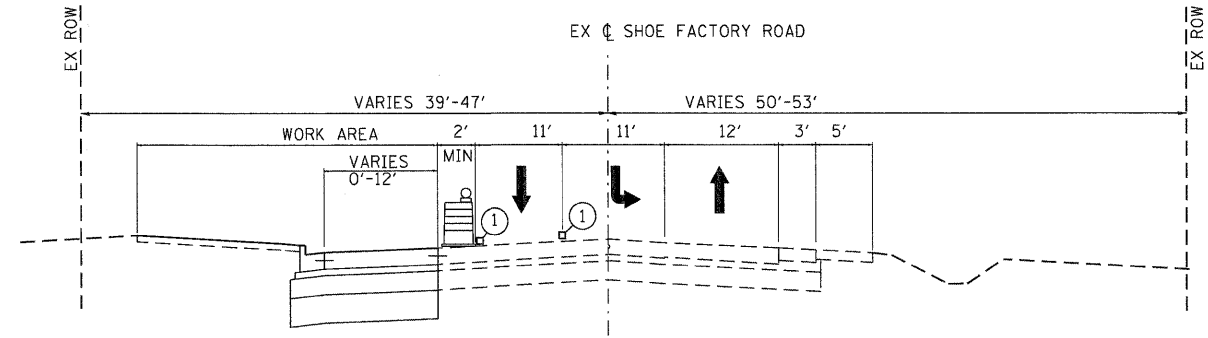
ILLINOIS ROUTE 59
STAGE 1 AND 1A: CONSTRUCT OUTSIDE LANES
 STA. 507+49 TO STA. 511+46
 STA. 512+51 TO STA. 517+72



ILLINOIS ROUTE 59
STAGE 2: CONSTRUCT MEDIAN
 STA. 514+25 TO STA. 517+75



SHOE FACTORY ROAD
STAGE 1
 STA. 107+85 TO STA. 112+12



SHOE FACTORY ROAD
STAGE 2
 STA. 113+20 TO STA. 118+11

- LEGEND**
- DIRECTION OF TRAFFIC
 - PROPOSED DRUM BARRICADE WITH BI-DIRECTIONAL STEADY BURNING LIGHT 50 FT C-C
 - ① TEMPORARY PAVEMENT MARKING LINE 4" (WHITE)
 - ② TEMPORARY PAVEMENT MARKING LINE 4" (YELLOW)
 - ③ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE)

SEQUENCE OF CONSTRUCTION

- PRE-STAGE**
- CONSTRUCTION**
- PLACE TRAFFIC CONTROL ADVANCE SIGNING.
 - REMOVE EXISTING PAVEMENT MARKINGS IN CONFLICT WITH STAGE I MAINTENANCE OF TRAFFIC STRIPING. COVER/REMOVE CONFLICTING EXISTING SIGNS LOCATED ALONG THE MAINLINE. RELOCATE EXISTING SIGNS AS NEEDED.
- MAINTENANCE OF TRAFFIC**
- MAINTAIN THE EXISTING TRAFFIC CONFIGURATION.
- STAGE 1**
- CONSTRUCTION**
- PLACE STAGE 1 TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENT MARKINGS.
 - INSTALL DRAINAGE STRUCTURES AND STORM SEWER AT LOCATIONS SHOWN IN THE PLANS.
 - CONSTRUCT THE PROPOSED PCC PAVEMENT PAVEMENT. CONSTRUCT THE CURB AND GUTTER.
- MAINTENANCE OF TRAFFIC**
- MAINTAIN TWO LANES OF TRAFFIC.
 - TRAFFIC WILL BE SHIFTED FOR STAGE 1 CONSTRUCTION AND WILL UTILIZE THE EXISTING PAVEMENT.
- STAGE 2**
- CONSTRUCTION**
- INSTALL STAGE 2 TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENT MARKINGS.
 - INSTALL DRAINAGE STRUCTURES AND STORM SEWER AT LOCATIONS SHOWN IN THE PLANS.
 - CONSTRUCT THE PROPOSED PCC PAVEMENT PAVEMENT. CONSTRUCT THE CURB AND GUTTER.
- MAINTENANCE OF TRAFFIC**
- MAINTAIN TWO LANES OF TRAFFIC.
 - TRAFFIC WILL BE SHIFTED FOR STAGE 2 CONSTRUCTION AND WILL UTILIZE EXISTING PAVEMENT.
- STAGE 3**
- CONSTRUCTION**
- COMPLETE REMAINING RESTORATION.
 - INSTALL PERMANENT PAVEMENT MARKINGS.
- MAINTENANCE OF TRAFFIC**
- MAINTAIN MINIMUM OF TWO LANES OF TRAFFIC.

GENERAL NOTES

THE TRAFFIC CONTROL DETAILS AND NOTES SHALL SERVE AS A GUIDE FOR SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT. HOWEVER, THE CONTRACTOR MAY MODIFY THE TRAFFIC CONTROL PLANS TO MEET CONSTRUCTION NEEDS BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE TRAFFIC CONTROL PLAN SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

THE ENGINEER SHALL BE INFORMED 72 HOURS IN ADVANCE OF ANY CHANGE TO THE STAGING PLANS.

THE RESIDENT ENGINEER SHALL CONTACT THE IDOT TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

CONTRACTOR SHALL MAINTAIN A MINIMUM OF ONE THROUGH LANE IN EACH DIRECTION THROUGHOUT THE PROJECT AREA AT ALL TIMES.

TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY, OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION WITHIN TWO HOURS FROM THE TIME OF NOTIFICATION.

ALL OF THE TRAFFIC CONTROL DEVICES SHALL BE IN PLACE BEFORE CONSTRUCTION IS STARTED. ALL TRAFFIC CONTROL DEVICES SHALL BE IN PLACE AT THE BEGINNING OF EACH STAGE, AND SHALL BE MAINTAINED FOR THE DURATION OF THAT STAGE UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN ACCESS TO ENTRANCES AS INDICATED ON THE PLANS WITHIN THE PROJECT LIMITS. THIS WORK IS TO BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR TEMPORARY ACCESS (COMMERCIAL ENTRANCE).

SIGNS

TRAFFIC CONTROL DEPICTED ON THE HIGHWAY STANDARDS IS THE MINIMUM REQUIREMENT. OTHER WORK OR SIGNING MAY BE REQUIRED BY THE ENGINEER.

ALL EXISTING SIGNS THAT CONFLICT WITH THE TRAFFIC CONTROL PLAN SHALL BE COVERED OR REMOVED IN ACCORDANCE WITH ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL PROVIDE, INSTALL, MAINTAIN AND REMOVE ALL SIGNS AND SIGN SUPPORTS REQUIRED FOR TRAFFIC CONTROL AND PROTECTION. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT LUMP SUM PRICES PER THE TRAFFIC CONTROL AND PROTECTION STANDARDS.

TEMPORARY SIGNING SHALL CONFORM TO THE APPLICABLE STANDARDS INCLUDED IN THE SPECIAL PROVISIONS AND CONTRACT PLANS OR AS DIRECTED BY THE ENGINEER. ALL TEMPORARY INFORMATION SIGNS SHALL BE PAID FOR SEPARATELY AT THE UNIT PER SQUARE FOOT FOR "TEMPORARY INFORMATION SIGNING".

THE CONTRACTOR SHALL PLACE A CHANGEABLE MESSAGE SIGN AT THE BEGINNING OF THE PROJECT ON BOTH LEGS OF IL ROUTE 59 AND SHOE FACTORY ROAD. SIGNS SHALL INFORM MOTORISTS OF UPCOMING CONSTRUCTION ACTIVITIES. THE MESSAGE SIGNS WITH THE APPROPRIATE INFORMATION SHALL BE IN PLACE TWO WEEKS BEFORE START OF CONSTRUCTION ACTIVITY. THIS WORK IS TO BE PAID FOR AT THE CONTRACT UNIT PRICE PER CALENDAR MONTH FOR "CHANGEABLE MESSAGE SIGN".

BARRICADES

TYPE II BARRICADES EQUIPPED WITH MONO-DIRECTIONAL STEADY BURN LIGHTS SHALL BE PLACED ALONG THE ROADWAY AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. TYPE II BARRICADES WITH DIRECTIONAL ARROWS SHALL BE PLACED AT LANE SHIFTS (TAPER SECTIONS) AS INDICATED ON THE PLANS OR DIRECTED BY THE ENGINEER. ALL TYPE II BARRICADES SHALL BE NON-METALLIC.

BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED - ONE (1) WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL.

EXISTING TRAFFIC CONTROL DEVICES WITHIN THE LIMITS OF CONSTRUCTION ARE TO BE PROTECTED FROM DAMAGE BY THE CONTRACTOR. ANY DAMAGED SIGNS CAUSED BY HIS WORK SHALL BE REPLACED BY THE CONTRACTOR.

CHANNELIZING DEVICES ARE TO BE PLACED AT SAME LEVEL AS TRAVELING LANE OR SHOULDER PROFILE.

PAVEMENT MARKING

ALL TEMPORARY PAVEMENT MARKINGS PROPOSED WITHIN THE WORK AREA SHALL BE COMPLETED PRIOR TO THE CONSTRUCTION PHASE CHANGE.

THE REMOVAL OF ALL PAVEMENT MARKING ON PCC PAVEMENT MUST BE DONE USING WATER BLASTING WITH VACUUM RECOVERY, AND IT WILL BE PAID AS "PAVEMENT MARKING REMOVAL".

ALL PAVEMENT MARKINGS CONFLICTING WITH REVISED TRAFFIC PATTERNS SHALL BE REMOVED. TYPE III MARKING TAPE SHALL BE USED ON THE FINAL WEARING SURFACE OR ON THE EXISTING PAVEMENT TO REMAIN WHEN THE TEMPORARY PAVEMENT MARKING WILL CONFLICT WITH THE PERMANENT PAVEMENT MARKING SUCH AS ON TAPERS OR LANE SHIFTS.

OTHER

ANY SAWCUTTING OF THE EXISTING PAVEMENT FOR STAGE CONSTRUCTION SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE YARD FOR "PAVEMENT REMOVAL".

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 PROJECT CONTACT: JRM
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HRGreen.com
 Illinois Professional Design Firm
 #194-091322

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PLOT DATE = 4/5/2012	CHECKED - TEH	REVISED -
	DATE - 4/5/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED MAINTENANCE OF TRAFFIC

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

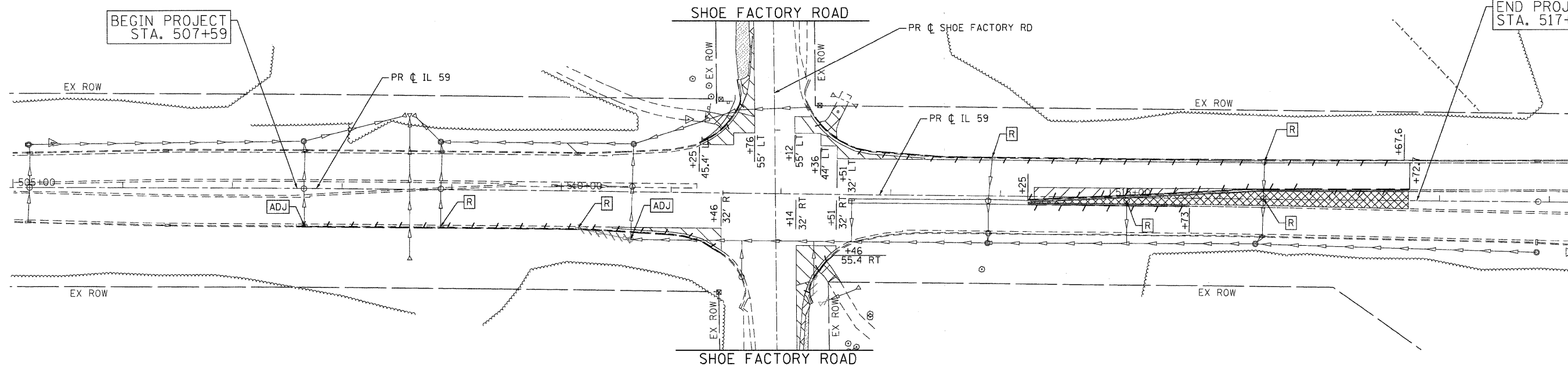
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	9
CONTRACT NO.				63711

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



BEGIN PROJECT
STA. 507+59

END PROJECT
STA. 517+79

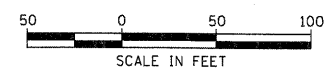
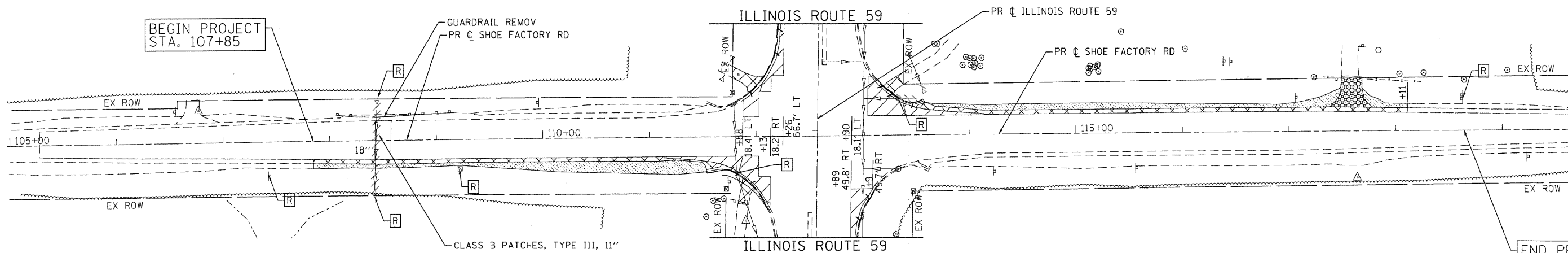


LEGEND:

- PAVEMENT REMOVAL
- PAVED SHOULDER REMOVAL
- MEDIAN SURFACE REMOVAL
- AGGREGATE SHOULDER REMOVAL (PAID AS REMOVAL OF UNSUITABLE)
- COMBINATION CURB & GUTTER REMOVAL
- DRIVEWAY PAVEMENT REMOVAL
- STORM SEWER REMOVAL
- REMOVE ITEM
- ADJUST ITEM

BEGIN PROJECT
STA. 107+85

END PROJECT
STA. 118+64



COMPANY NAME: HRGreen
 PROJECT CONTACT: jme1hus
 CLIENT: HRGreen
 DATE PLOTTED: 4/5/2012 5:02:25 PM
 FILE NAME: 10-00084-00-CH
 PEN NUMBER: 0177-rem.dgn
 PEN TABLE: standard-trans.tbl



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

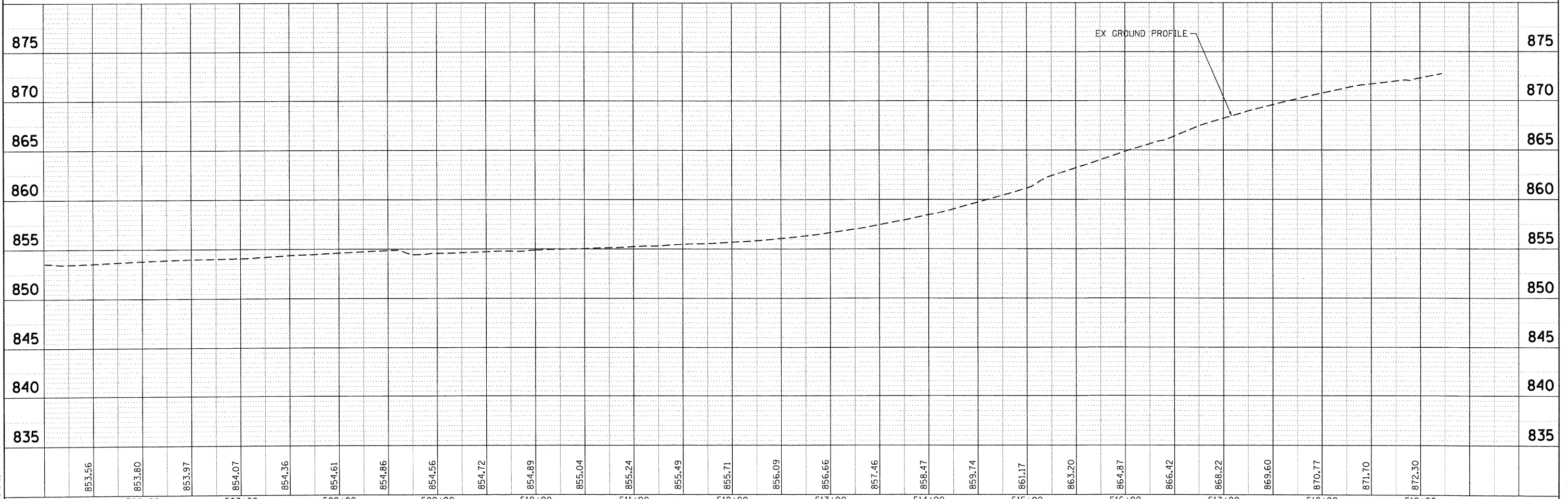
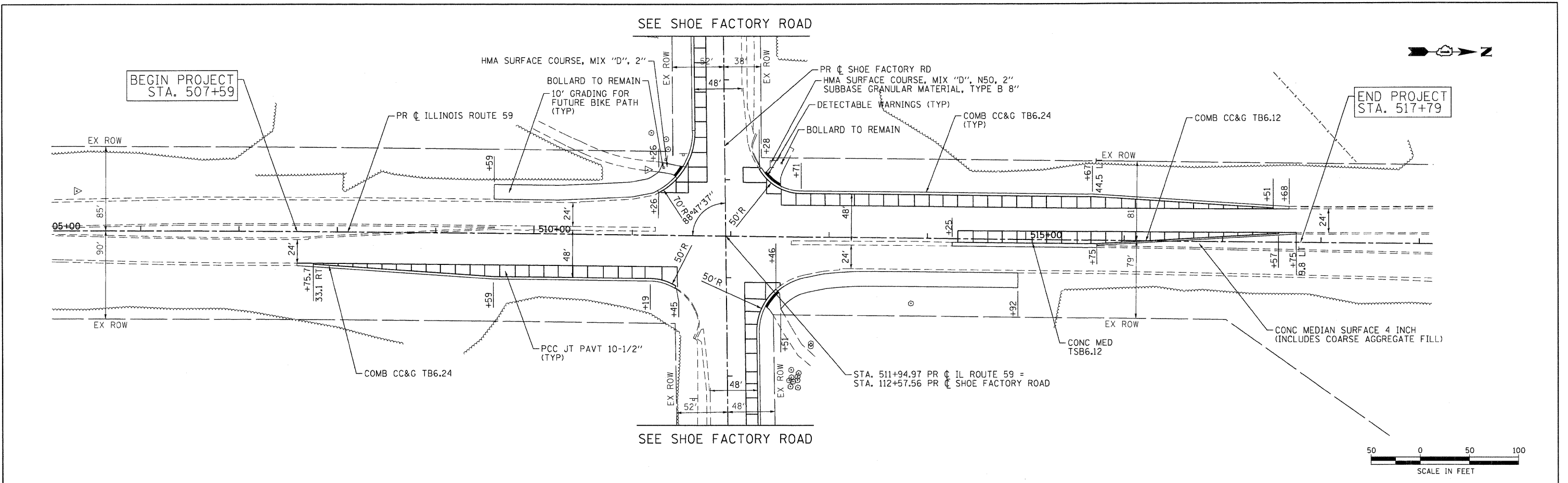
REMOVAL PLAN IL 59 & SHOE FACTORY RD		
SCALE: 1"=50'	SHEET NO. 1 OF 1 SHEETS	STA. BEG TO STA. END

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	10
CONTRACT NO. 63711			ILLINOIS FED. AID PROJECT	

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	ALIGNED		
	CHECKED		
	PLOTTED		
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PROFILE	REVIEWED	BY	DATE
	GRADES CHECKED		
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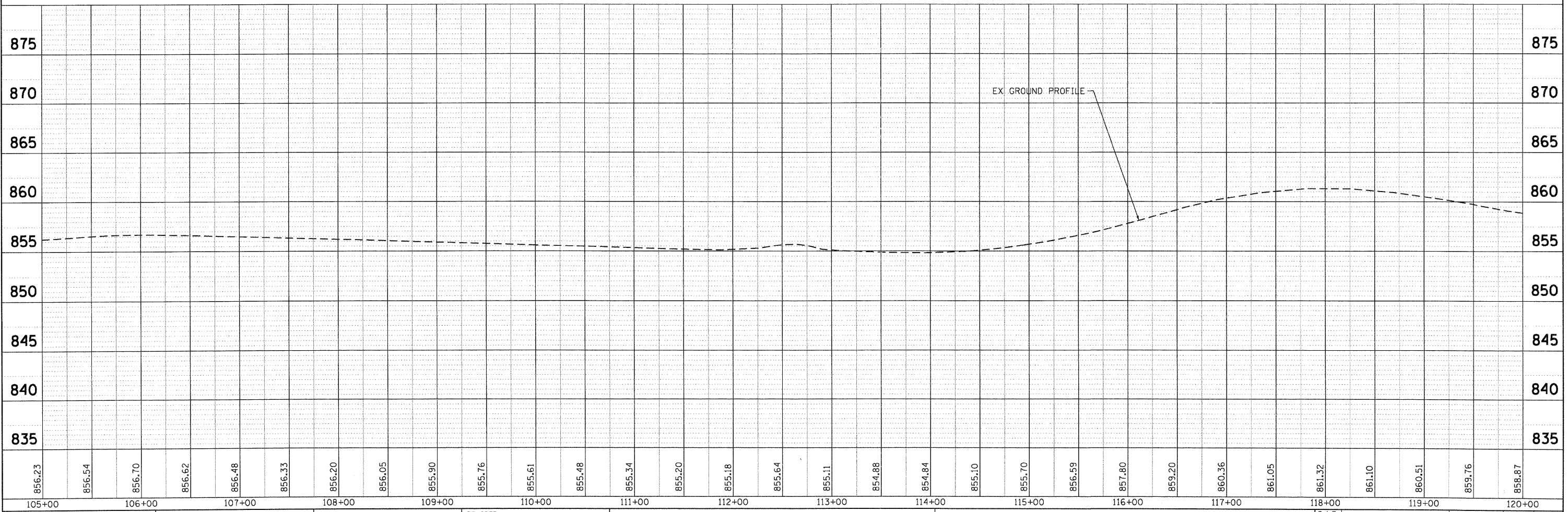
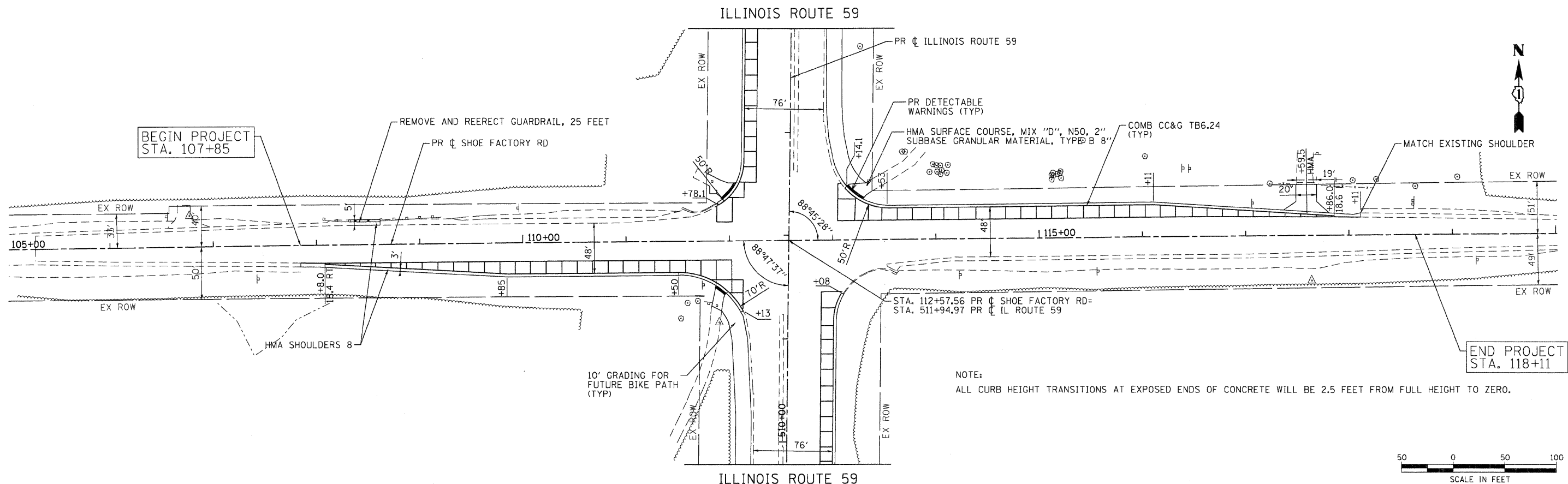


HRGreen.com Illinois Professional Design Firm # 184-001322	USER NAME = jmelhus DESIGNED - DRAWN - CHECKED - DATE = 4/5/12	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN AND PROFILE ILLINOIS ROUTE 59 SCALE: 1"=50' SHEET NO. 1 OF 2 SHEETS STA. 507+59 TO STA. 517+79	F.A.P. RTE. 338 SECTION 10-00084-00-CH COUNTY COOK TOTAL SHEETS 63 SHEET NO. 11 CONTRACT NO. 63711 ILLINOIS FED. AID PROJECT
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PLAN	REVIEWED	DATE
	PLOTTED	
	ALIGNMENT CHECKED	
	CONSTRUCTION CHECKED	
	CADD FILE NAME	
	NO.	

PROFILE	REVIEWED	DATE
	PLOTTED	
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 PROJECT CONTACT: HRGreen.com
 CLIENT: Illinois Professional Design Firm
 DATE PLOTTED: 4/5/2012 5:03:34 PM
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105+00	106+00	107+00	108+00	109+00	110+00	111+00	112+00	113+00	114+00	115+00	116+00	117+00	118+00	119+00	120+00																

HRGreen
 Illinois Professional Design Firm
 # 184-001322

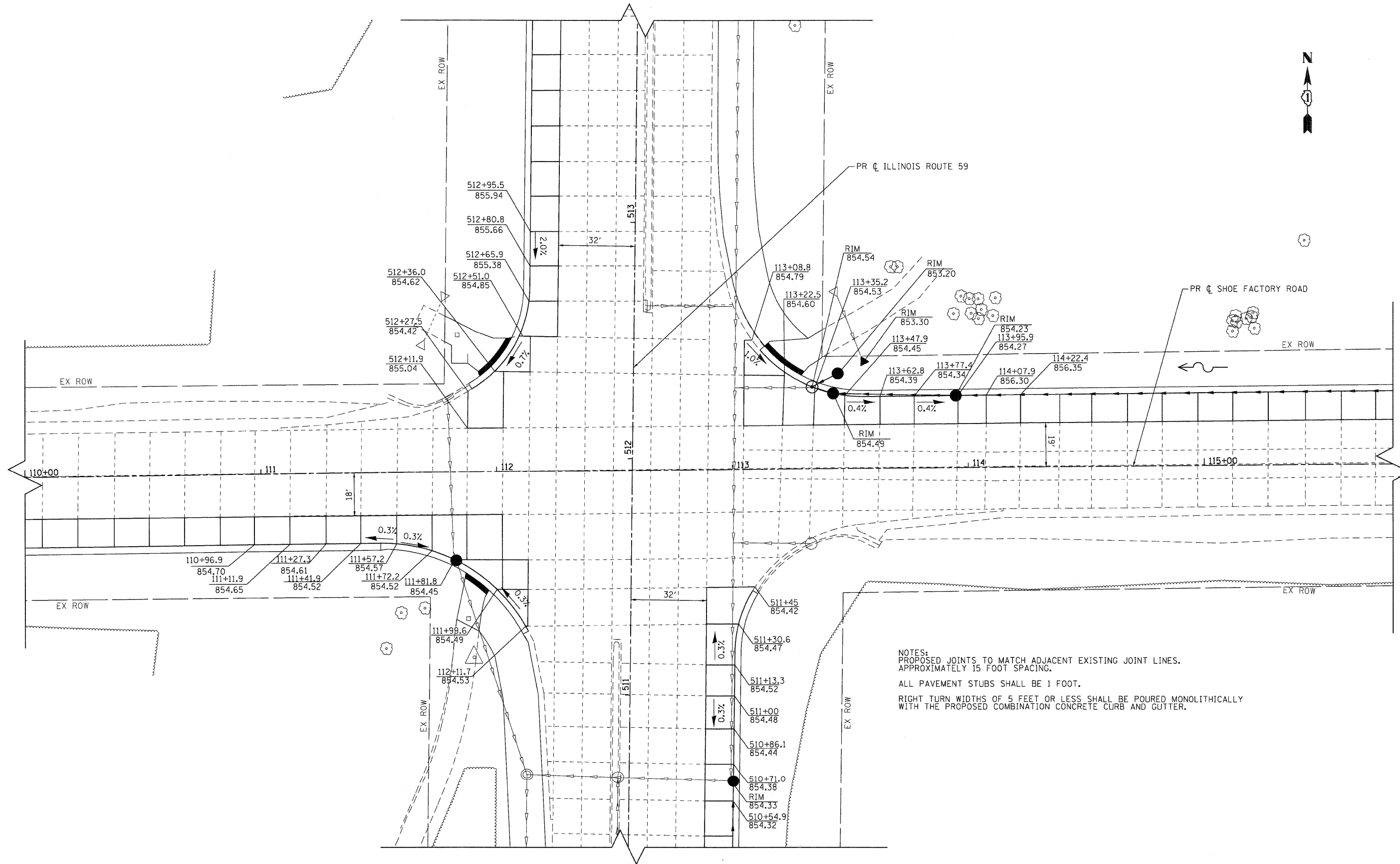
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PLOT DATE = 4/5/2012	DATE - 4/5/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN AND PROFILE
SHOE FACTORY ROAD

SCALE: 1"=50' SHEET NO. 2 OF 2 SHEETS STA. 107+85 TO STA. 118+64

F.A.P. RTE. 338	SECTION 10-00084-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 12
CONTRACT NO. 63711				
ILLINOIS FED. AID PROJECT				



NOTES:
 PROPOSED JOINTS TO MATCH ADJACENT EXISTING JOINT LINES.
 APPROXIMATELY 15 FOOT SPACING.
 ALL PAVEMENT STUBS SHALL BE 1 FOOT.
 RIGHT TURN WIDTHS OF 5 FEET OR LESS SHALL BE POURED MONOLITHICALLY
 WITH THE PROPOSED COMBINATION CONCRETE CURB AND GUTTER.

COMPANY NAME: HRGreen
 PROJECT CONTACT: jmelhus
 CLIENT: HRGreen
 DATE PLOTTED: 4/5/2012 5:03:38 PM
 PLOT DRIVER: pcplm
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PLOT DATE = 4/5/2012	CHECKED -	REVISED -
	DATE - 4/5/12	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**INTERSECTION GRADING PLAN
 IL 59 & SHOE FACTORY RD**

SCALE: 1"=20' SHEET NO. 1 OF 1 SHEETS STA. 110+00 TO STA. 115+80

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	13
CONTRACT NO. 63711				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

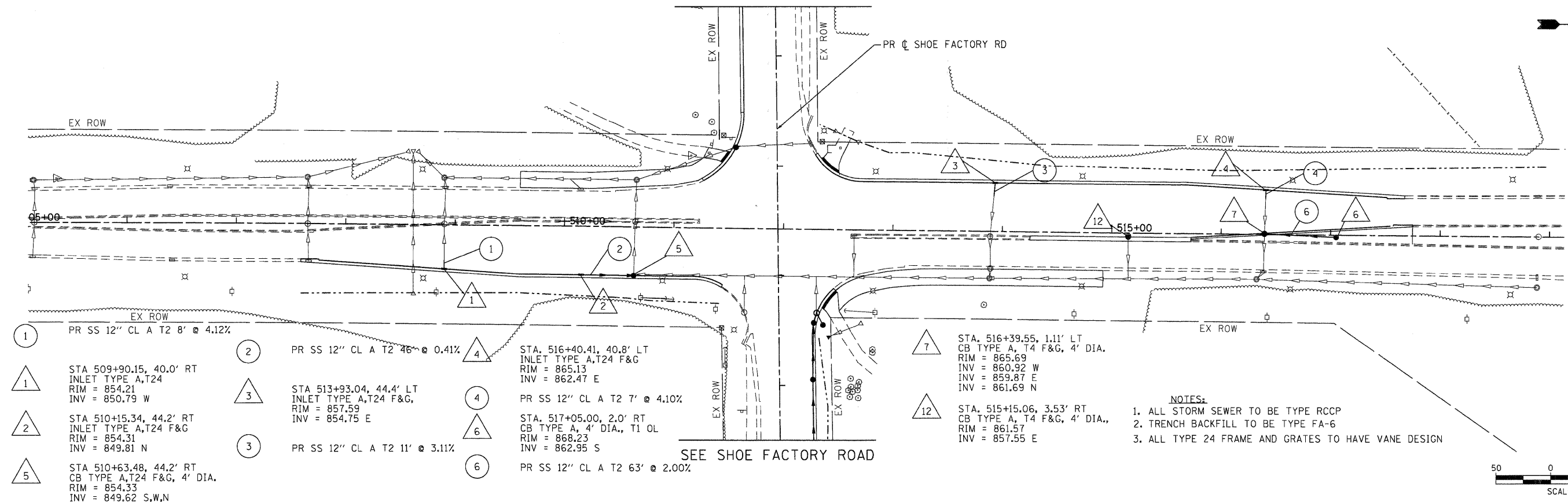
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BY	
REVISIONS	
ALIGNED	CHECKED
PLOTTED	CHECKED
NOTE BOOK	
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FILE NAME	

DATE	
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REVISIONS	
GRADES	CHECKED
PLOTTED	CHECKED
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FILE NAME	

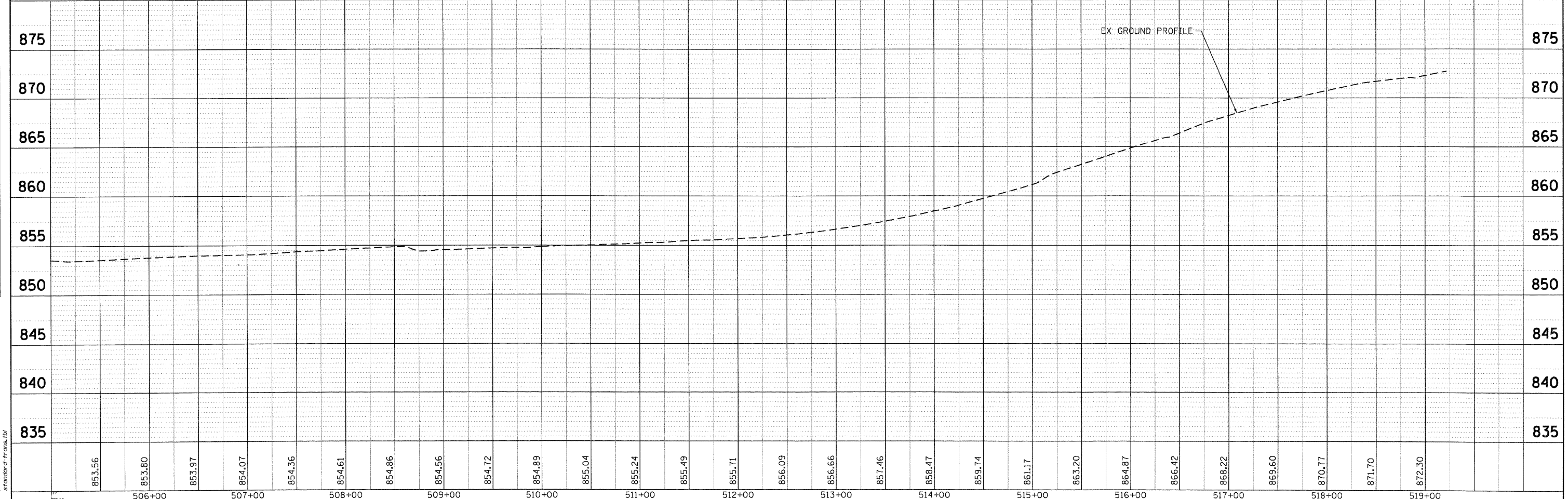
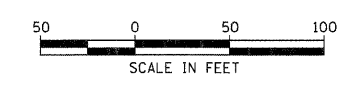
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 PROJECT CONTACT: HRGreen
 CLIENT: HRGreen
 DATE PLOTTED: 4/5/2012 5:04:47 PM
 FILE NAME: 011-59p-01.dgn
 PLOT DRIVER: pdfplot
 PEN TABLE: standard.ctb

SEE SHOE FACTORY ROAD



- 1 PR SS 12" CL A T2 8' @ 4.12%
 - 2 PR SS 12" CL A T2 46' @ 0.41%
 - 3 PR SS 12" CL A T2 11' @ 3.11%
 - 4 PR SS 12" CL A T2 7' @ 4.10%
 - 5 PR SS 12" CL A T2 63' @ 2.00%
 - 6
 - 7 STA. 516+39.55, 1.11' LT
CB TYPE A, T4 F&G, 4' DIA.
RIM = 865.69
INV = 860.92 W
INV = 859.87 E
INV = 861.69 N
 - 12 STA. 515+15.06, 3.53' RT
CB TYPE A, T4 F&G, 4' DIA.,
RIM = 861.57
INV = 857.55 E
- STA 509+90.15, 40.0' RT
INLET TYPE A, T24
RIM = 854.21
INV = 850.79 W
 STA 510+15.34, 44.2' RT
INLET TYPE A, T24 F&G
RIM = 854.31
INV = 849.81 N
 STA 510+63.48, 44.2' RT
CB TYPE A, T24 F&G, 4' DIA.
RIM = 854.33
INV = 849.62 S,W,N
 STA 513+93.04, 44.4' LT
INLET TYPE A, T24 F&G,
RIM = 857.59
INV = 854.75 E
 STA. 516+40.41, 40.8' LT
INLET TYPE A, T24 F&G
RIM = 865.13
INV = 862.47 E
 STA. 517+05.00, 2.0' RT
CB TYPE A, 4' DIA., T1 OL
RIM = 868.23
INV = 862.95 S

- NOTES:**
- ALL STORM SEWER TO BE TYPE RCCP
 - TRENCH BACKFILL TO BE TYPE FA-6
 - ALL TYPE 24 FRAME AND GRATES TO HAVE VANE DESIGN



853.56	853.80	853.97	854.07	854.36	854.61	854.86	854.56	854.72	854.89	855.04	855.24	855.49	855.71	856.09	856.66	857.46	858.47	859.74	861.17	863.20	864.87	866.42	868.22	869.60	870.77	871.70	872.30	875
506+00	507+00	508+00	509+00	510+00	511+00	512+00	513+00	514+00	515+00	516+00	517+00	518+00	519+00															

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED DRAINAGE PLAN
ILLINOIS ROUTE 59

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	14
CONTRACT NO. 63711				

SCALE: 1"=50' SHEET NO. 1 OF 2 SHEETS STA. 507+59 TO STA. 517+79

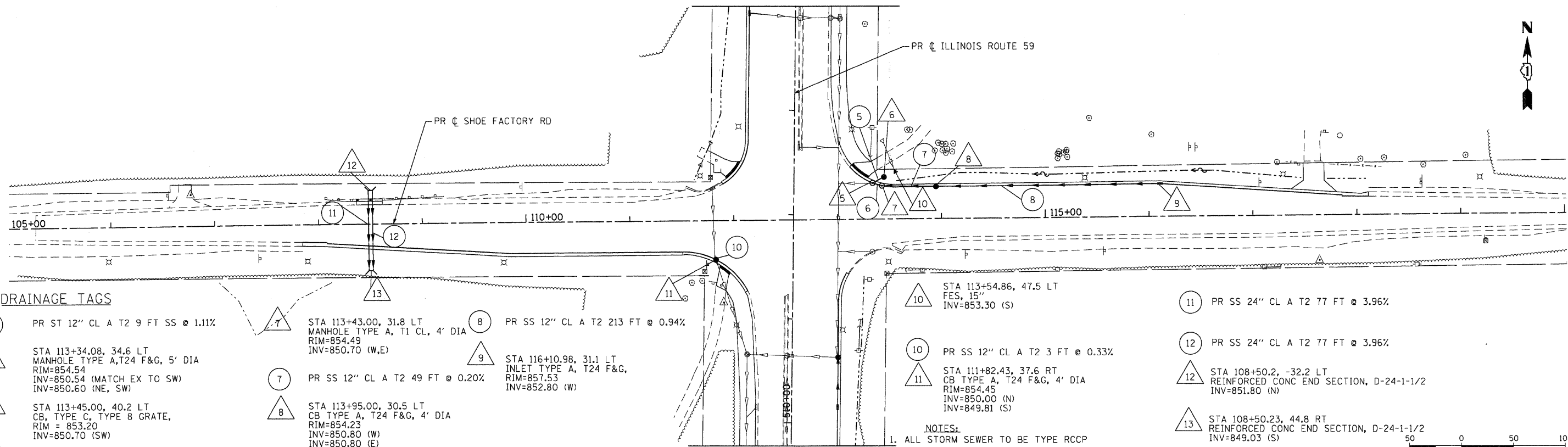
ILLINOIS FED. AID PROJECT

REVISIONS	DATE
BY	
PLAN	
NOTE BOOK	
ALIGNED CHECKED	
PLOTTED	
DATE	
NO.	

REVISIONS	DATE
BY	
PROFILE	
NOTE BOOK	
GRADES CHECKED	
PLOTTED	
DATE	
NO.	

COMPANY NAME: HR GREEN, INC.
 PROJECT CONTACT: J. McHUGH
 CLIENT: VILLAGE OF HOFFMAN ESTATES
 DATE PLOTTED: 4/6/2012 9:46:18 AM
 FILE NAME: 0177-pp-02.dgn
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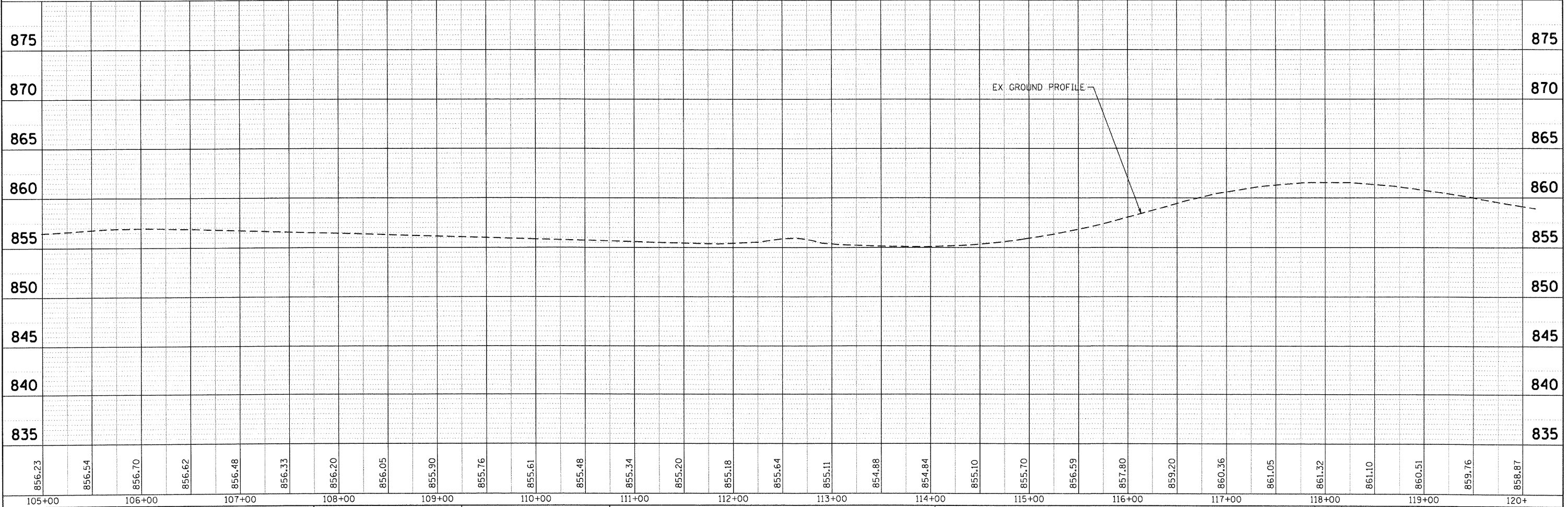
ILLINOIS ROUTE 59



DRAINAGE TAGS

- 5 PR ST 12" CL A T2 9 FT SS @ 1.11%
- 5 STA 113+34.08, 34.6 LT
MANHOLE TYPE A, T24 F&G, 5' DIA
RIM=854.54
INV=850.54 (MATCH EX TO SW)
- 6 STA 113+45.00, 40.2 LT
CB, TYPE C, TYPE 8 GRATE,
RIM = 853.20
INV=850.70 (SW)
- 6 PR SS 12" CL A T2 7 FT @ 1.43%
- 7 STA 113+43.00, 31.8 LT
MANHOLE TYPE A, T1 CL, 4' DIA
RIM=854.49
INV=850.70 (W,E)
- 7 PR SS 12" CL A T2 49 FT @ 0.20%
- 8 STA 113+95.00, 30.5 LT
CB TYPE A, T24 F&G, 4' DIA
RIM=854.23
INV=850.80 (W)
INV=850.80 (E)
- 8 PR SS 12" CL A T2 213 FT @ 0.94%
- 9 STA 116+10.98, 31.1 LT
INLET TYPE A, T24 F&G,
RIM=857.53
INV=852.80 (W)
- 10 STA 113+54.86, 47.5 LT
FES, 15"
INV=853.30 (S)
- 10 PR SS 12" CL A T2 3 FT @ 0.33%
- 11 STA 111+82.43, 37.6 RT
CB TYPE A, T24 F&G, 4' DIA
RIM=854.45
INV=850.00 (N)
INV=849.81 (S)
- 11 PR SS 24" CL A T2 77 FT @ 3.96%
- 12 PR SS 24" CL A T2 77 FT @ 3.96%
- 12 STA 108+50.2, -32.2 LT
REINFORCED CONC END SECTION, D-24-1-1/2
INV=851.80 (N)
- 13 STA 108+50.23, 44.8 RT
REINFORCED CONC END SECTION, D-24-1-1/2
INV=849.03 (S)

- NOTES:
- ALL STORM SEWER TO BE TYPE RCCP
 - TRENCH BACKFILL TO BE TYPE FA-6
 - ALL TYPE 24 FRAME AND GRATES TO BE VANE DESIGN



856.23	856.54	856.70	856.62	856.48	856.33	856.20	856.05	855.90	855.76	855.61	855.48	855.34	855.20	855.18	855.64	855.11	854.88	854.84	855.10	855.70	856.59	857.80	859.20	860.36	861.05	861.32	861.10	860.51	859.76	858.87
105+00		106+00		107+00		108+00		109+00		110+00		111+00		112+00		113+00		114+00		115+00		116+00		117+00		118+00		119+00		120+

HRGreen.com
 Illinois Professional Design Firm
 #184-001522

USER NAME	SPernal	DESIGNED	-	REVISED	-
PLOT SCALE	1"=50'	DRAWN	-	REVISED	-
PLOT DATE	4/6/2012	CHECKED	-	REVISED	-
		DATE	4/6/12	REVISED	-

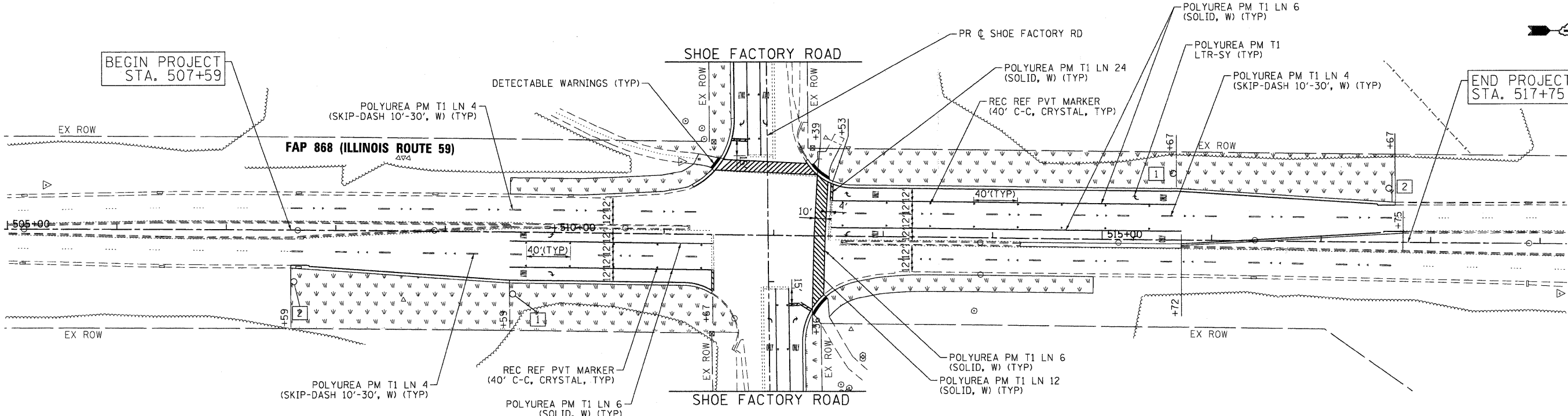
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION


PROPOSED DRAINAGE PLAN
SHOE FACTORY ROAD
 SCALE: 1"=50' SHEET NO. 2 OF 2 SHEETS STA. 107+85 TO STA. 118+64

F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	15
CONTRACT NO.			63711	
ILLINOIS FED. AID PROJECT				

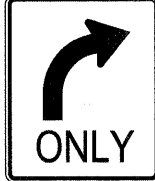
BEGIN PROJECT STA. 507+59



END PROJECT STA. 517+75



LEGEND:
 SEEDING, CLASS 2A

RIGHT LANE ONLY

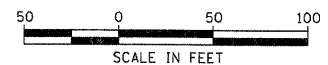
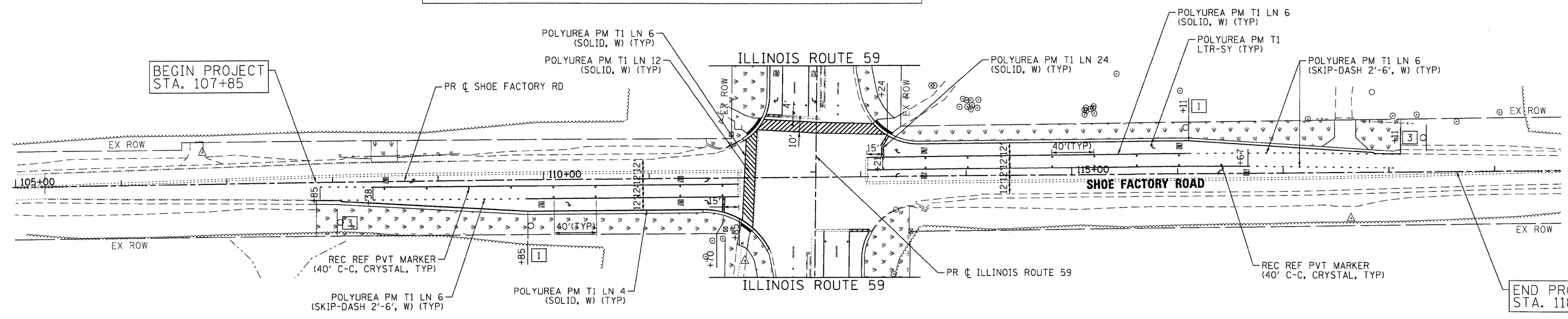


Shoe Factory Rd  **Illinois Route 59** 

2 CUSTOM 60" X 18"
3 CUSTOM 60" X 18"
1 R3-5fP (30" x 12")
 R3-5R (30" x 36")

BEGIN PROJECT STA. 107+85

END PROJECT STA. 118+11



COMPANY NAME: HRGreen
 PROJECT CONTACT: jme2hus
 CLIENT: HRGreen
 DATE PLOTTED: 4/5/2012 5:05:51 PM
 FILE NAME: 0177.plt
 PLOT TO: 0
 PLOT TABLE: Standard.ctb



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PLOT DATE = 4/5/2012	CHECKED -	REVISED -
	DATE = 4/5/12	REVISED -

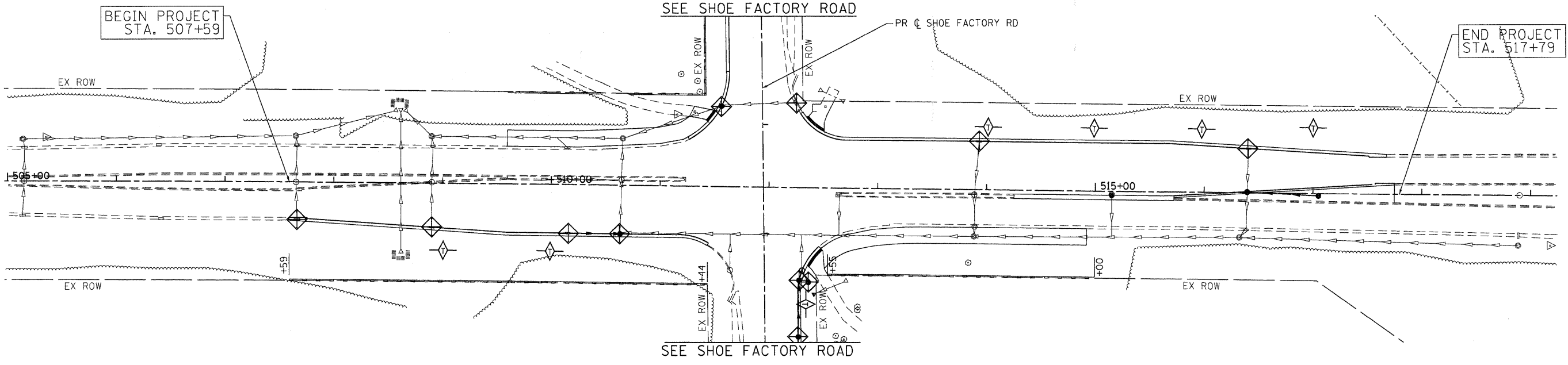
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING, SIGNING, AND RESTORATION PLAN
 IL 59 & SHOE FACTORY RD

SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. BEG TO STA. END

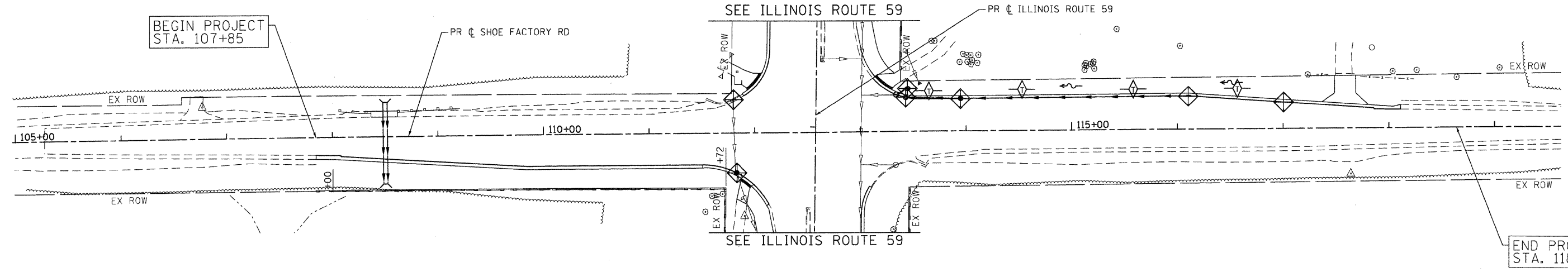
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	16
CONTRACT NO.			63711	

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



LEGEND:

	PERIMETER EROSION BARRIER (FT)
	INLET FILTERS (EA)
	TEMPORARY DITCH CHECK (FT)
	INLET AND PIPE PROTECTION (EA)

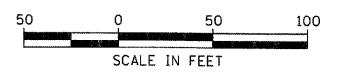


EROSION CONTROL GENERAL NOTES (CONT.)

ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (30) DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.

EROSION CONTROL GENERAL NOTES (CONT.)

THE CONTRACTOR SHALL PROVIDE ADEQUATE RECEPTACLES FOR THE DEPOSITION OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE CONSTRUCTION PROCESS. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL DEBRIS UPON OR INTO THE CONSTRUCTION SITE, CHANNEL, WATERS OF THE UNITED STATES OR ISOLATED WATERS. THE CONTRACTOR SHALL MAINTAIN THE CONSTRUCTION SITE FREE OF CONSTRUCTION MATERIAL DEBRIS.



COMPANY NAME: HRGreen
 PROJECT CONTACT: jmelhus
 CLIENT: HRGreen.com
 DATE PLOTTED: 4/5/2012 5:02:56 PM
 FILE NAME: 0177-eros.dgn
 PLOT DATE: 4/5/2012
 PEN TABLE: 0177-eros.tbl



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PLOT SCALE = 1"=50'	DRAWN -	REVISED -
PLOT DATE = 4/5/2012	CHECKED -	REVISED -
	DATE - 4/5/12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EROSION CONTROL PLAN
IL 59 & SHOE FACTORY RD**

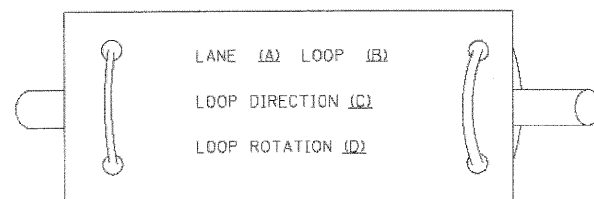
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F.A.P. RTE. 338	SECTION 10-00084-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 17
CONTRACT NO. 63711			ILLINOIS FED. AID PROJECT	

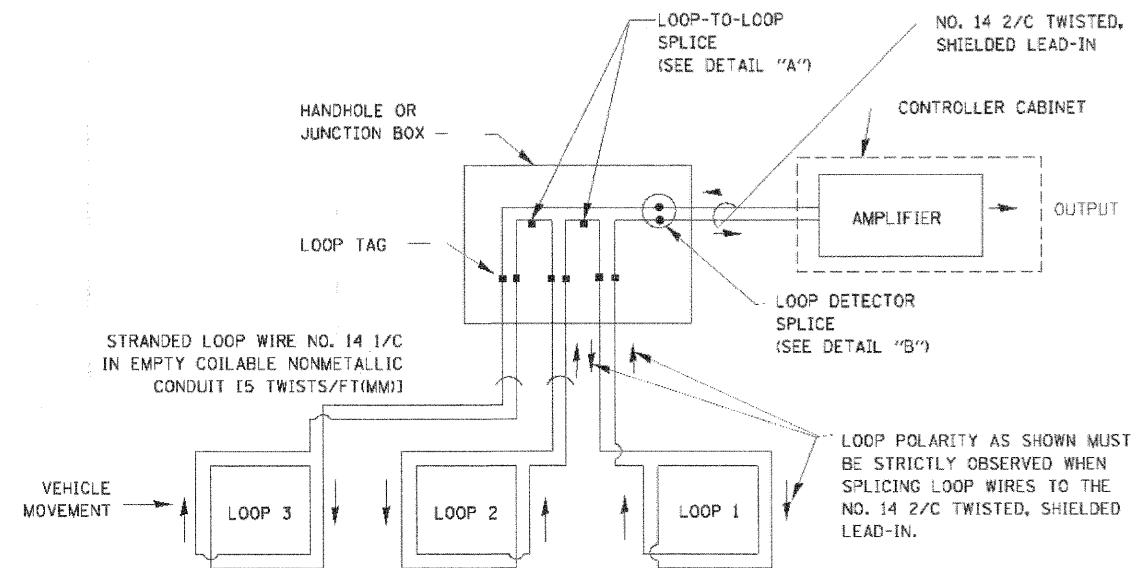
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SFAIANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

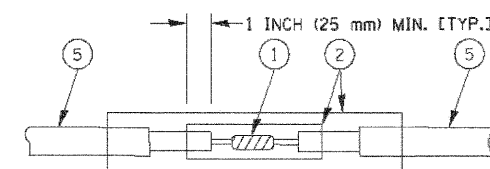


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

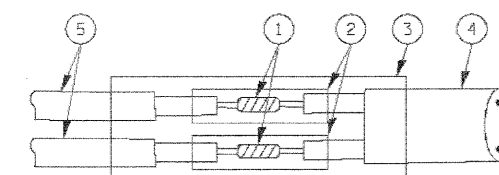


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

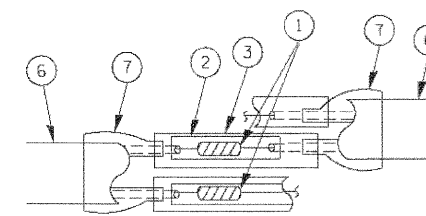


DETAIL "A"
LOOP-TO-LOOP SPLICE

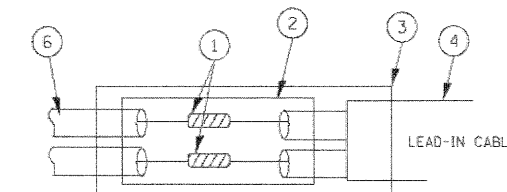


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE 1 LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



PRE-FORMED LOOP

DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: Illinois Professional Design Firm #184-001322
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USER NAME = jme1hut
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 CHECKED - DAD
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

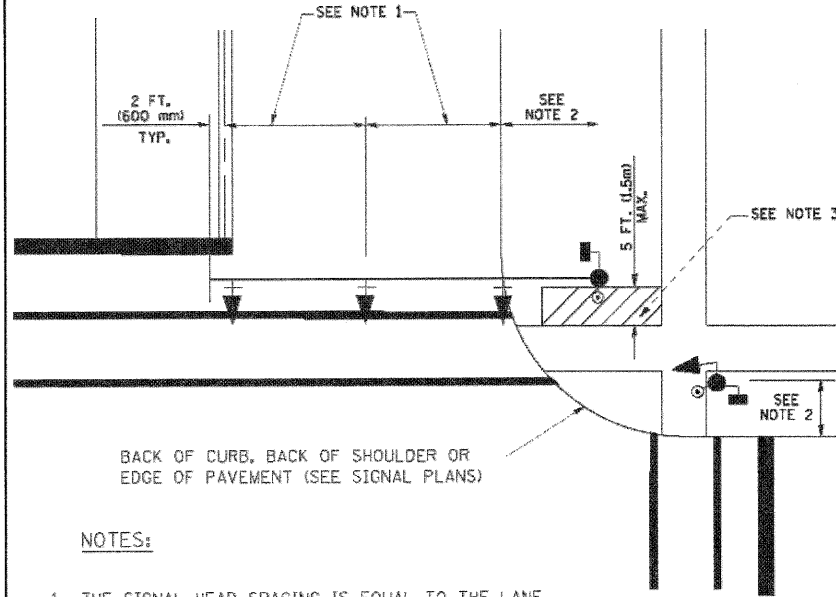
DISTRICT ONE
 STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: SHEET NO. 1 OF 7 SHEETS STA. TO STA.

F.A.P. RTE. 338	SECTION 10-00084-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 18
CONTRACT NO. 63711			ILLINOIS FED. AID PROJECT	

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

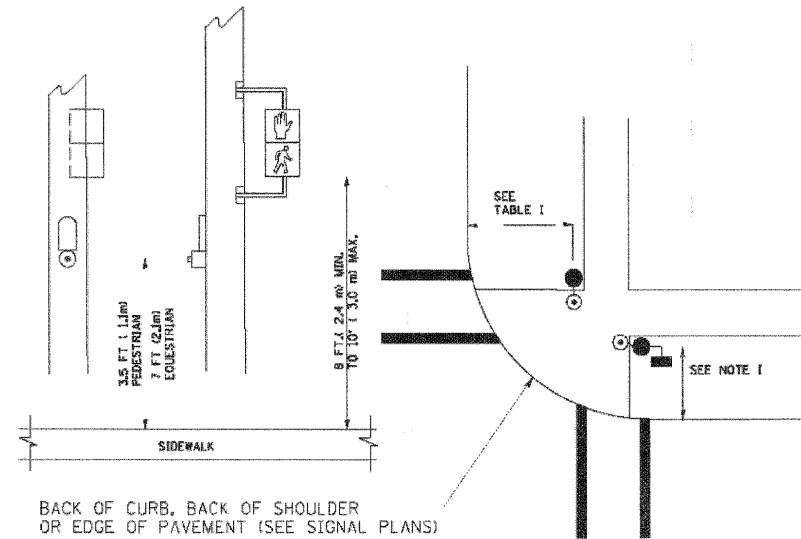
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

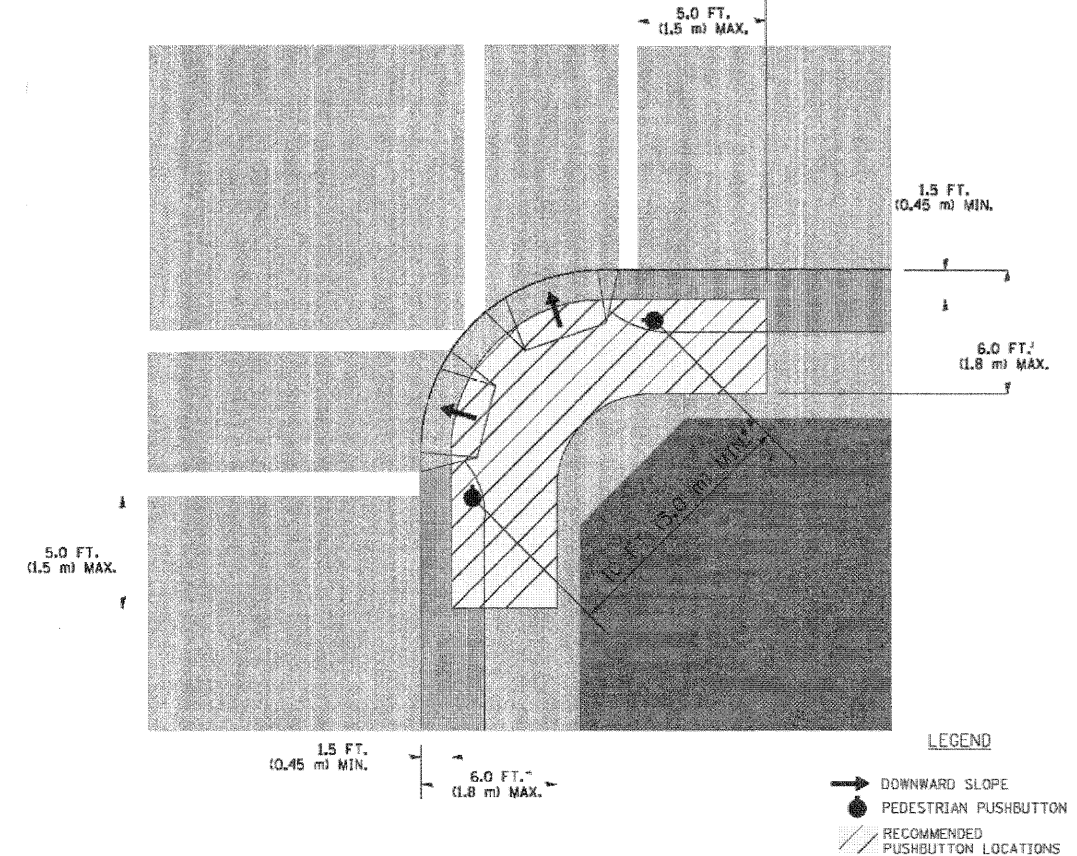
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen
 CLIENT: HRGreen
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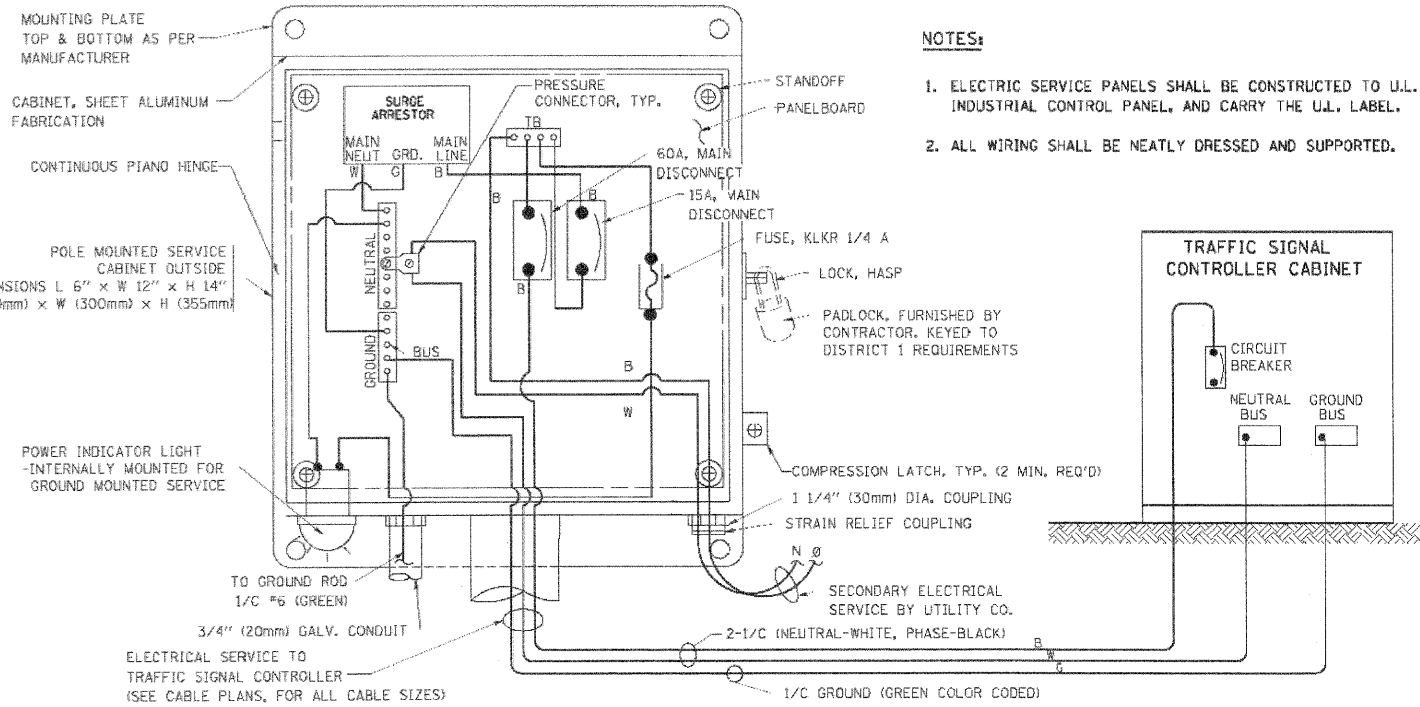
HRGreen.com
 Illinois Professional Design Firm
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

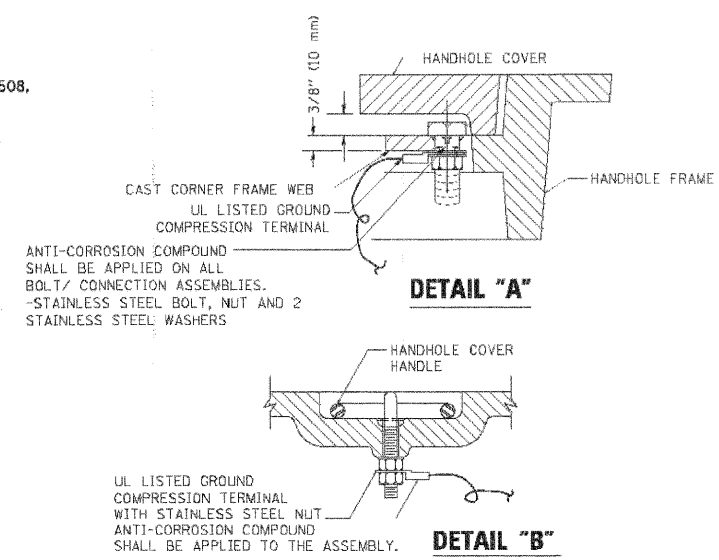
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: SHEET NO. 2 OF 7 SHEETS STA. TO STA.

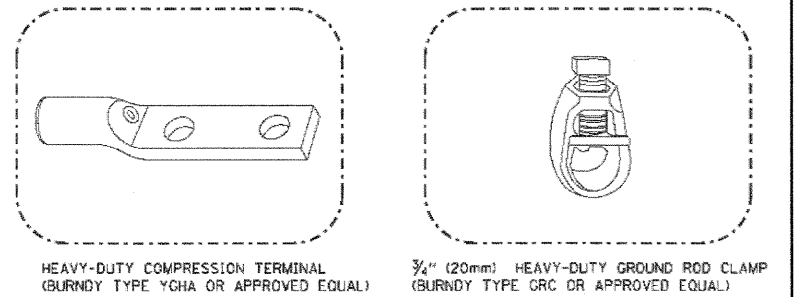
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	19
CONTRACT NO.			63711	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



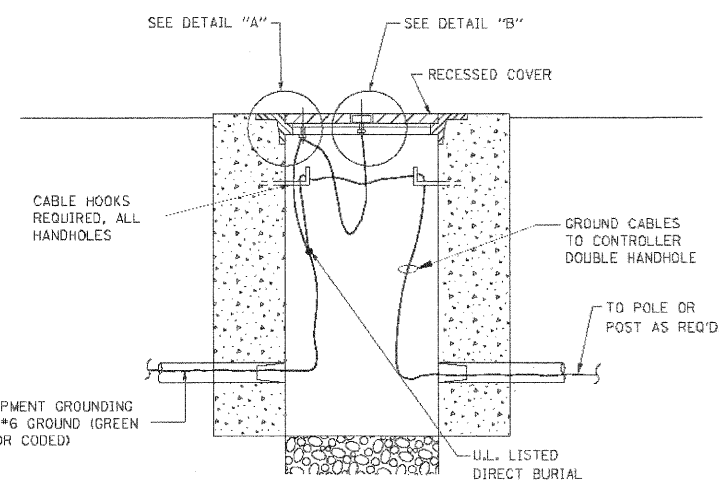
ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



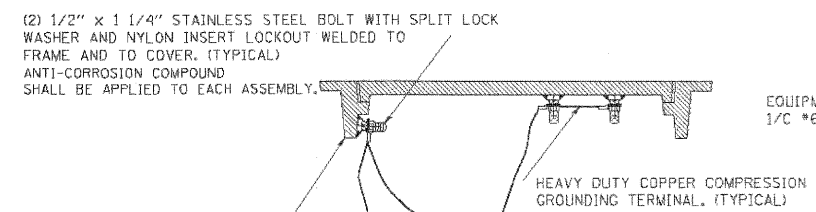
- NOTES:**
- GROUNDING SYSTEM**
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD, ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (647) 705-4139.
 2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
 3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
 4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



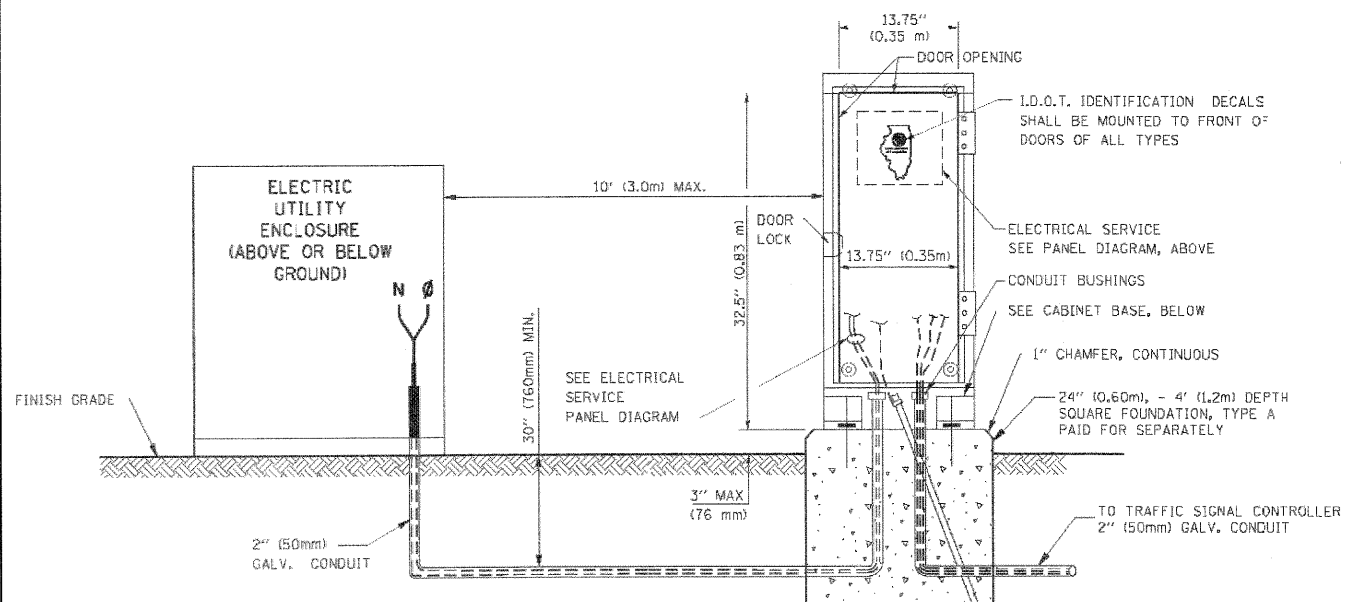
- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



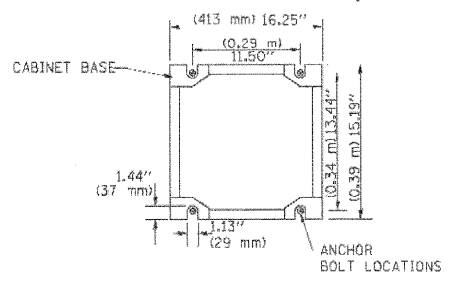
HANDHOLE COVER & FRAME - GROUNDING DETAIL
 (NOT TO SCALE)



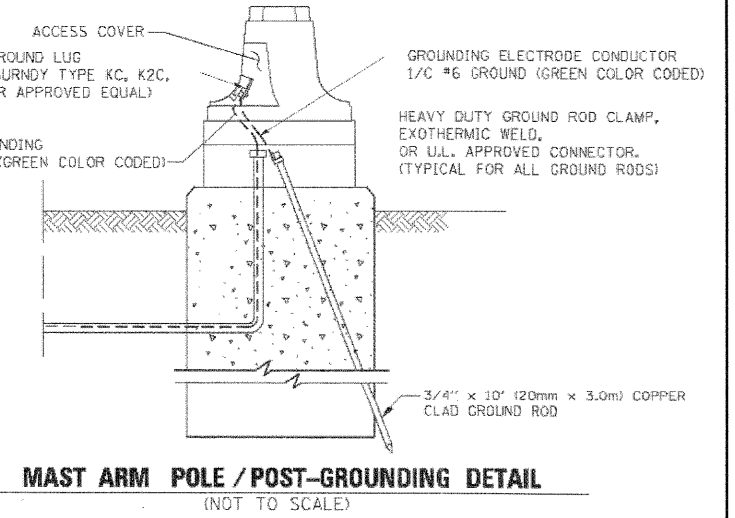
EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL
 (NOT TO SCALE)



SERVICE INSTALLATION GROUND MOUNT
 (NOT TO SCALE)



CABINET - BASE BOLT PATTERN
 (NOT TO SCALE)



MAST ARM POLE / POST-GROUNDING DETAIL
 (NOT TO SCALE)

COMPANY NAME: HRGreen
 PROJECT CONTACT: HRGreen.com
 CLIENT: Illinois Professional Design Firm
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 PEN TABLE: standard.ctb

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DRAWN	- BCK
CHECKED	- DAD
DATE	- 10-28-09
PLOT SCALE	50.0000 / 1/8"
PLOT DATE	11/4/2009

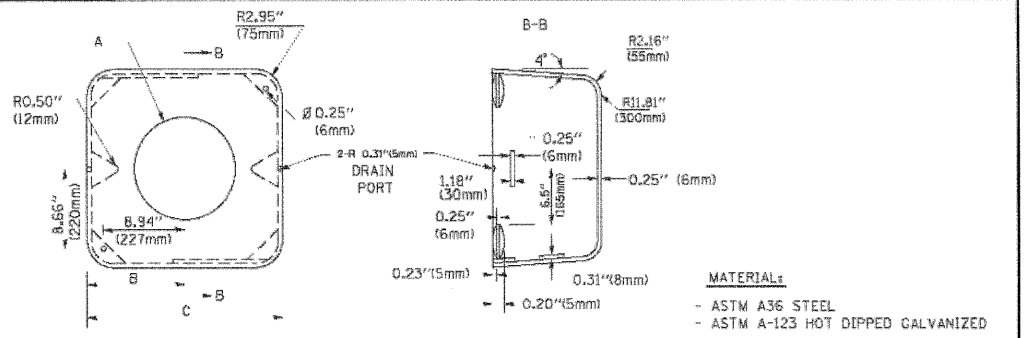
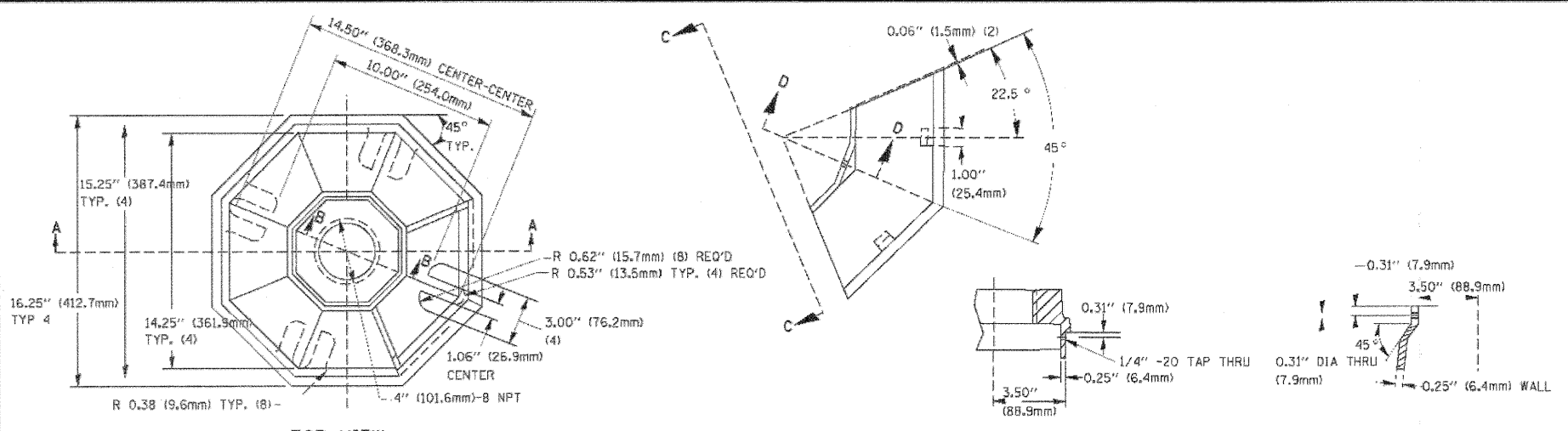
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REVISÉ	-

DESIGNED	- DAD
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REVISÉ	-
REVISÉ	-
REVISÉ	-

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE	
STANDARD TRAFFIC SIGNAL DESIGN DETAILS	
SCALE:	SHEET NO. 3 OF 7 SHEETS STA. TO STA.

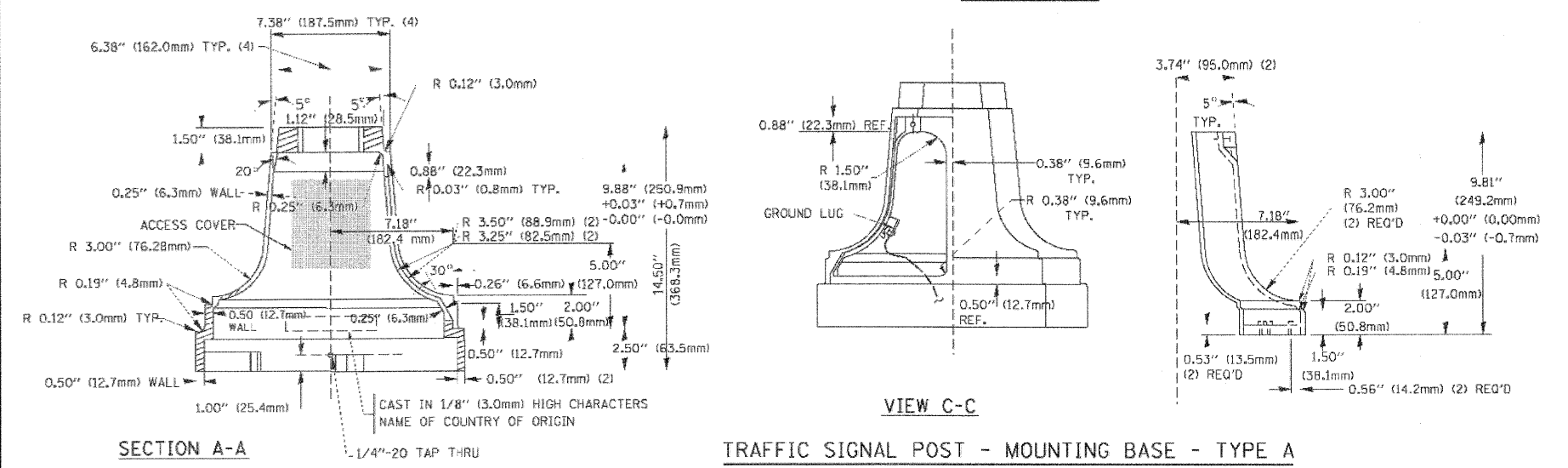
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-0084-00-CH	COOK	63	20
CONTRACT NO.			63711	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



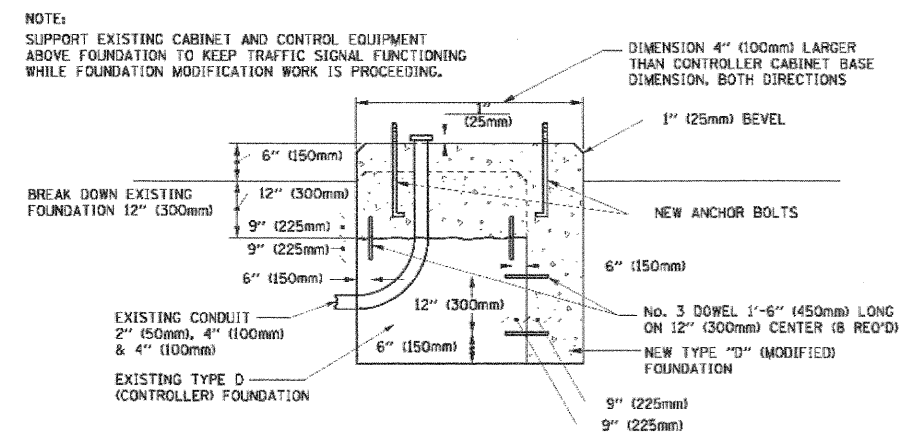
A	B	C	HEIGHT	WEIGHT
VARIABLES	9.5\" (241mm)	19\" (483mm)	7\" (178mm) - 12\" (300mm)	53 lbs (24kg)
VARIABLES	10.75\" (273mm)	21.5\" (546mm)	7\" (178mm) - 12\" (300mm)	68 lbs (31 kg)
VARIABLES	13.0\" (330mm)	26\" (660mm)	7\" (178mm) - 12\" (300mm)	81 lbs (37 kg)
VARIABLES	18.5\" (470mm)	37\" (940mm)	7\" (178mm) - 12\" (300mm)	126 lbs (57 kg)

SHROUD

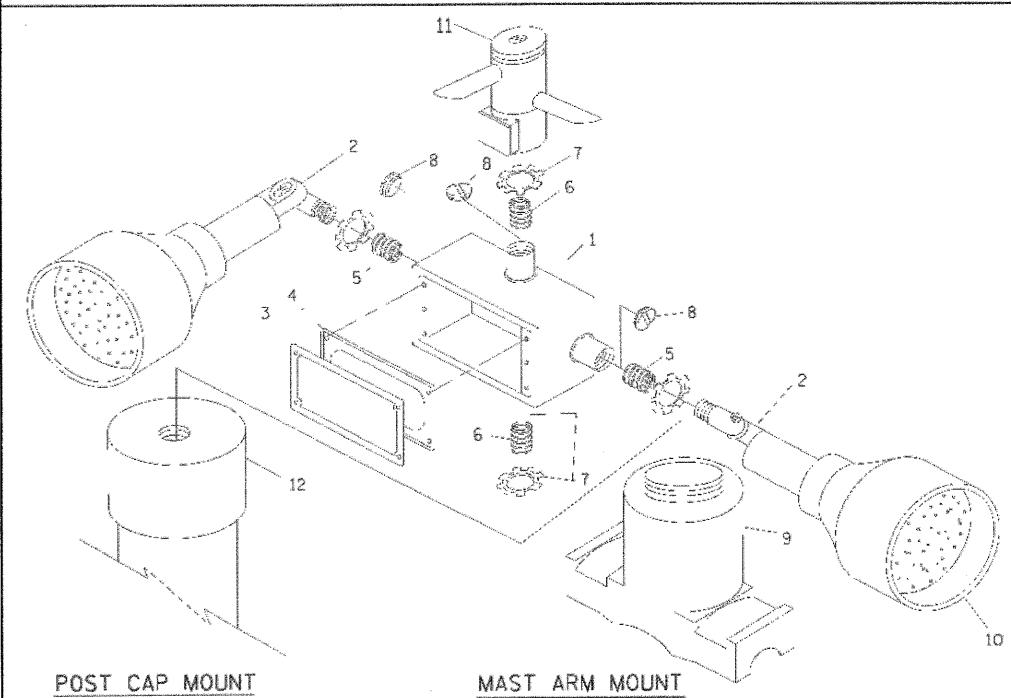
- NOTES:
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
 - THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
 - THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A

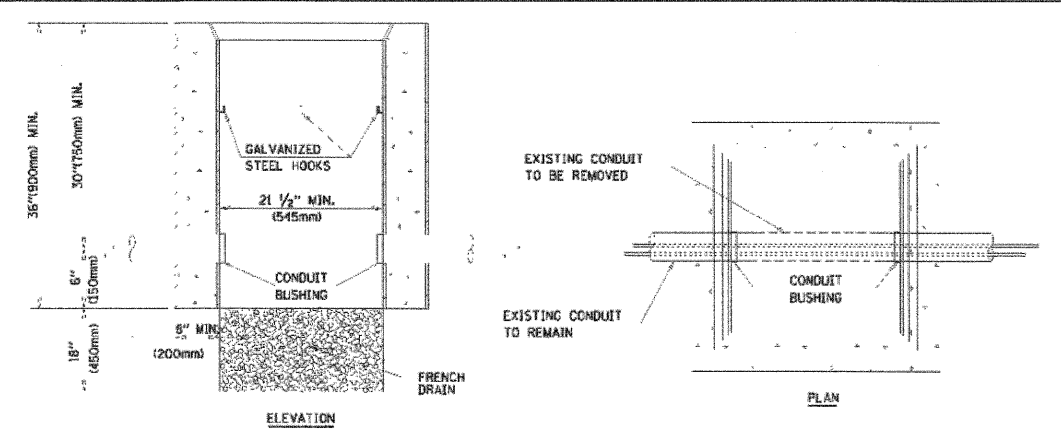


MODIFY EXISTING TYPE "D" FOUNDATION



ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\" (19 mm) CLOSE NIPPLE
7	3/4\" (19 mm) LOCKNUT
8	3/4\" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

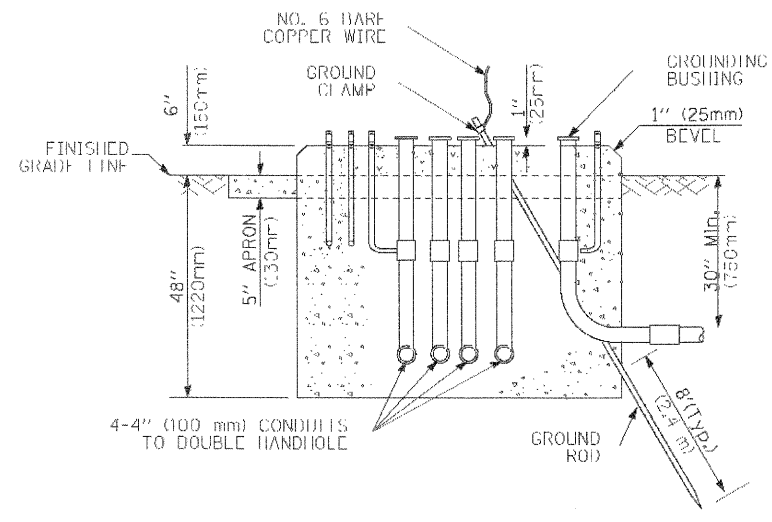
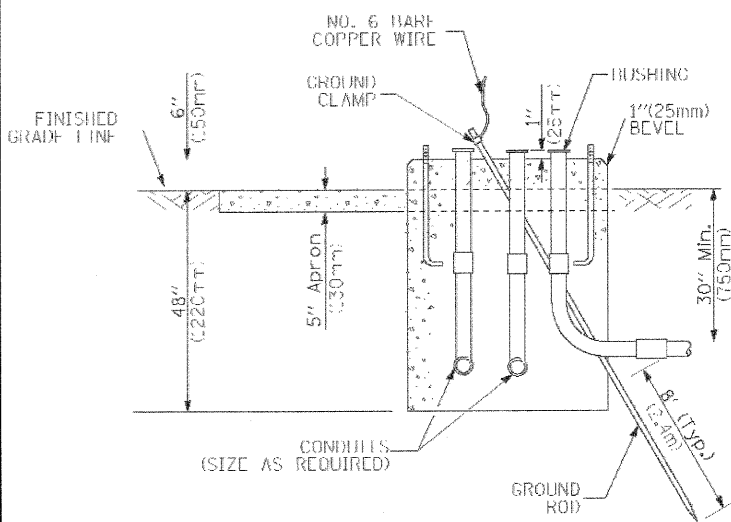
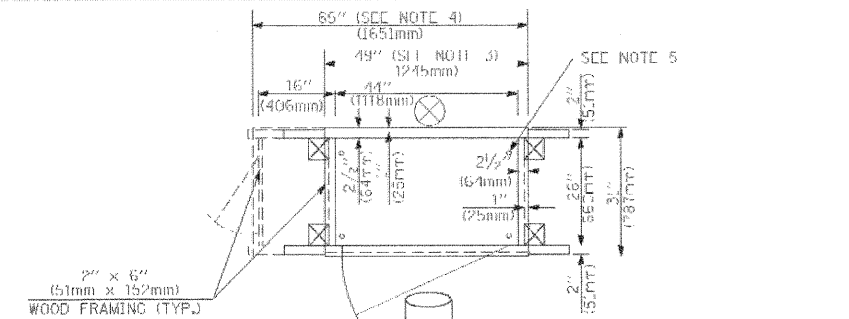
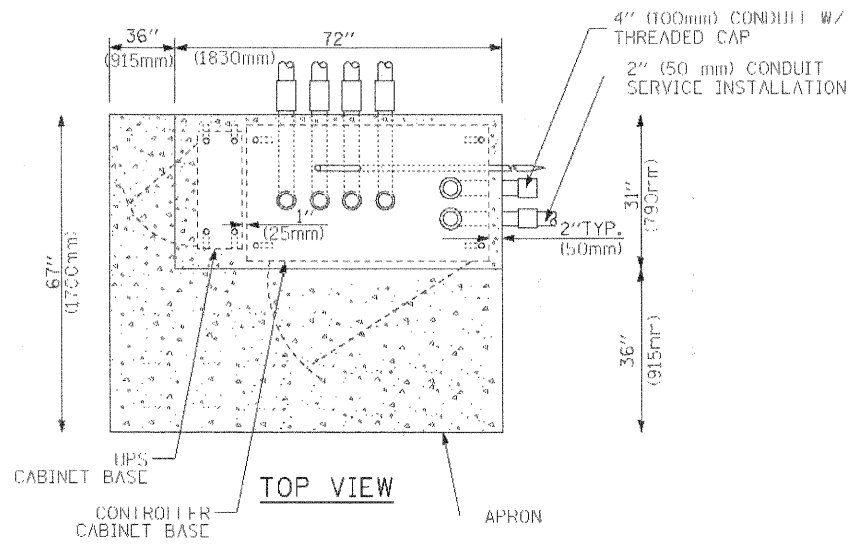
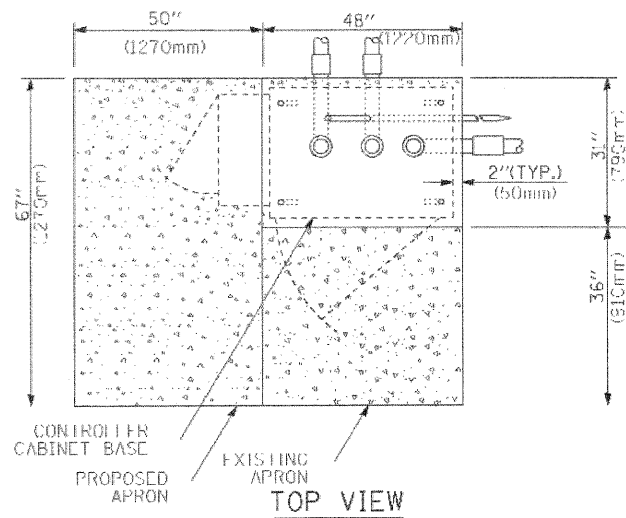
- NOTES:
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
 - ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
 - WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



- NOTES:
- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
 - REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

COMPANY NAME: HRGreen
 PROJECT CONTACT: HRGreen.com
 CLIENT: Illinois Professional Design Firm
 DATE PLOTTED: 4/5/2012 5:16:50 PM
 FILE NAME: 0177-sig04.dgn
 PLOT DRIVER: acadplot
 PEN TABLE: standard.ctb



**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**

**TYPE C
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**

NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm), ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm), ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0±	6.0±
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

MAST ARM LENGTH	FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: USER NAME = jmelhui
 CLIENT: DATE PLOTTED: 4/5/2012 5:20:07 PM
 FILE NAME: 0177-5102E105.dgn
 PLOT DRIVER: pdfplot
 PEN TABLE: standard-props.tbl



DESIGNED - DAG
 DRAWN - BCK
 CHECKED - DAD
 DATE - 10-28-09

REVISD -
 REVISD -
 REVISD -
 REVISD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: SHEET NO. 5 OF 7 SHEETS STA. TO STA.

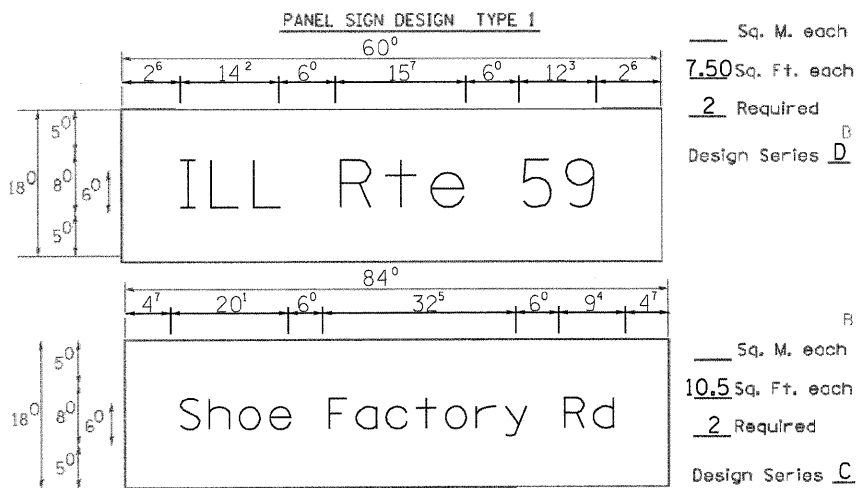
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	22
CONTRACT NO.			63711	

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED																		
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE																					
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE																					
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA																					
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED																					
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F																					
UNINTERRUPTIBLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F																					
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F																					
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F																					
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)																					
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE																					
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED																					
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM				STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED																					
SIGNAL POST				REMOVE ITEM				ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED																					
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM				STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED																					
GUY WIRE				ABANDON ITEM				SIGNAL POST AND FOUNDATION TO BE REMOVED																					
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				INTERSECTION & SAMPLING (SYSTEM) DETECTOR																					
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				SAMPLING (SYSTEM) DETECTOR																					
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				EXISTING INTERSECTION LOOP DETECTOR																					
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR																					
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				EXISTING PREFORMED INTERSECTION LOOP DETECTOR																					
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR																					
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR																					
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER				PREFORMED SAMPLING (SYSTEM) DETECTOR																					
ILLUMINATED SIGN "NO LEFT TURN"				RADIO INTERCONNECT				<h2 style="margin: 0;">RAILROAD SYMBOLS</h2> <table style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th></th> <th style="text-align: center;">EXISTING</th> <th style="text-align: center;">PROPOSED</th> </tr> </thead> <tbody> <tr> <td>RAILROAD CONTROL CABINET</td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> </tr> <tr> <td>RAILROAD CANTILEVER MAST ARM</td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> </tr> <tr> <td>FLASHING SIGNAL</td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> </tr> <tr> <td>CROSSING GATE</td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> </tr> <tr> <td>CROSSBUCK</td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> </tr> </tbody> </table>					EXISTING	PROPOSED	RAILROAD CONTROL CABINET			RAILROAD CANTILEVER MAST ARM			FLASHING SIGNAL			CROSSING GATE			CROSSBUCK		
	EXISTING	PROPOSED																											
RAILROAD CONTROL CABINET																													
RAILROAD CANTILEVER MAST ARM																													
FLASHING SIGNAL																													
CROSSING GATE																													
CROSSBUCK																													
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO REPEATER																									
DETECTOR LOOP, TYPE I				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED																									
PREFORMED DETECTOR LOOP				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)																									
MICROWAVE VEHICLE SENSOR																													
VIDEO DETECTION CAMERA																													
VIDEO DETECTION ZONE																													
PAN, TILT, ZOOM CAMERA																													
WIRELESS DETECTOR SENSOR																													
WIRELESS ACCESS POINT																													

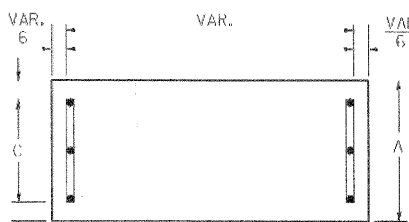
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0177-signto106.dgn



Sq. M. each
7.50 Sq. Ft. each
2 Required
Design Series **D**

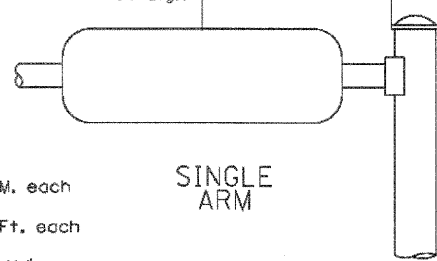
Sq. M. each
10.5 Sq. Ft. each
2 Required
Design Series **C**

SUPPORTING CHANNELS



A	B	C
18"	2"	14"

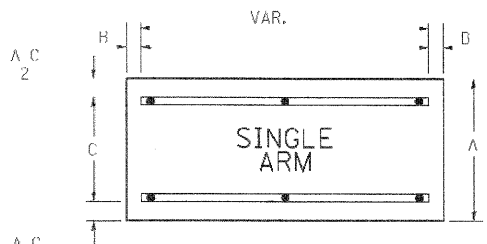
Mid-Point of Sign
VAR. TO 8 FT.(MAX.)



SINGLE ARM

Sq. M. each
Sq. Ft. each
Required
Design Series

SUPPORTING CHANNELS

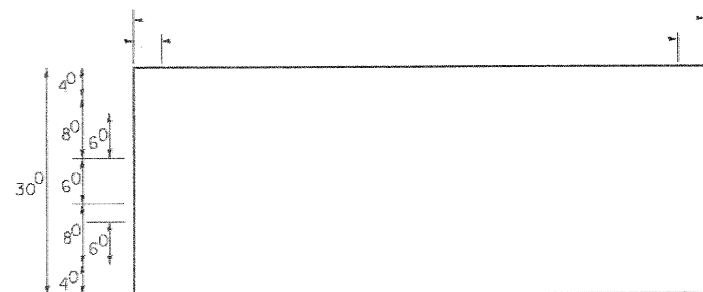
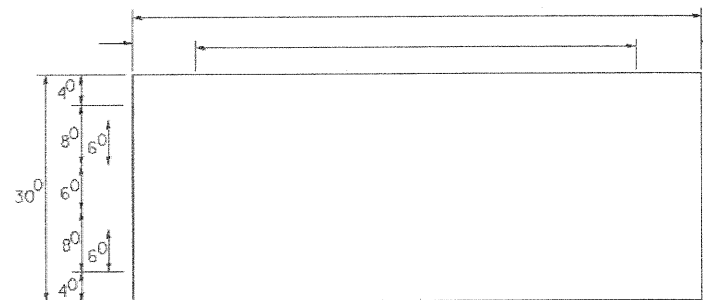


A	B	C
18"	2"	12"
30"	2"	22"

Sq. M. each
Sq. Ft. each
Required
Design Series

NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS

PANEL SIGN DESIGN TYPE 2



Sq. M. each
Sq. Ft. each
Required
Design Series

GENERAL NOTES

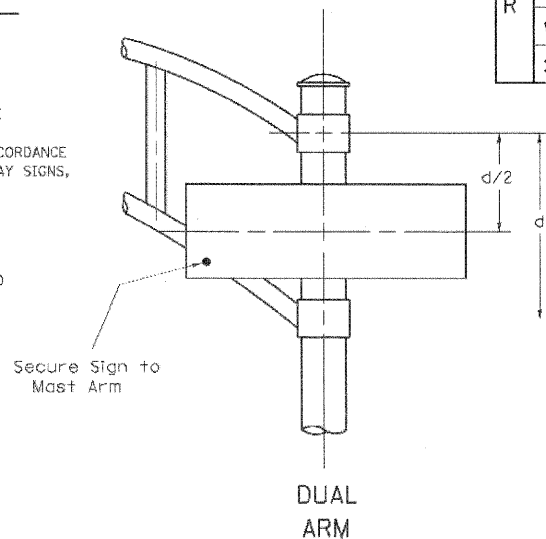
- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 87T001, 87T002, 87T006, 87T011 AND 87T012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
- THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 8'-0".
- ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:

* J.O. HERBERT CO. MIDLOTHIAN, VA. * WESTERN REMAC INC. WOODRIDGE, IL.

PARTS LISTING:
SIGN CHANNEL PART #HPN653 (MED. CHANNEL)
SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3
BRACKETS PART #HPN034 (UNIVERSAL)
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM shall be used. See Note #5.



DUAL ARM

Upper Case To Lower Case
Spacing Chart 8-6 Inch Series "C & D"

SERIES	SECOND LETTER															
	acde		bhikl		f w		j		s t		v y		x		z	
	g	o	q	m	n	p	r	u								
A W X	12	14	14	15	12	14	06	10	11	14	06	10	11	12	12	14
B	14	15	20	21	14	15	11	12	14	15	12	14	12	14	16	17
C E G	14	15	20	21	12	14	06	10	12	14	12	14	14	15	14	15
D O Q R	14	15	20	21	14	15	06	10	12	14	12	14	14	15	14	15
F	05	06	14	15	06	10	05	06	06	10	06	10	06	10	11	12
H I M N	20	21	22	24	20	21	14	15	16	17	16	17	20	21	20	21
J U	20	21	20	21	16	17	14	15	16	17	16	17	16	17	20	21
K L	11	12	16	17	11	12	05	06	11	12	11	12	11	12	12	14
P	12	14	14	15	12	14	05	06	11	12	11	12	12	14	12	14
S	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14
T	11	12	16	17	06	10	06	10	11	12	11	12	11	12	12	14
V	06	10	14	15	11	12	06	10	12	14	12	14	12	14	12	14
Y	05	06	14	15	06	10	05	06	05	07	05	06	06	10	11	12
Z	16	17	22	24	16	17	12	14	16	17	16	17	16	17	20	21

Lower Case To Lower Case
Spacing Chart 6 Inch Series "C & D"

SERIES	SECOND LETTER															
	acde		bhikl		f w		j		s t		v y		x		z	
	g	o	q	m	n	p	r	u								
ad h g i j	16	17	22	24	16	17	12	14	14	15	14	15	16	17	16	17
l m n q u																
b f k o p s	12	14	16	17	11	12	05	06	11	12	11	12	12	14	12	14
c e	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14
r	06	10	12	14	06	10	03	03	05	06	05	06	06	10	06	10
t z	12	14	16	17	12	14	06	10	11	12	11	12	12	14	12	14
v y	11	12	14	15	11	12	05	06	06	10	06	10	11	12	11	12
w	11	12	14	15	11	12	05	06	11	12	11	12	11	12	12	14
x	12	14	16	17	11	12	05	06	11	12	11	12	11	12	12	14

Number To Number
Spacing Chart 8 Inch Series "C & D"

SERIES	SECOND NUMBER																			
	0		1		2		3		4		5		6		7		8		9	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
0 9	16	17	16	17	14	15	12	14	14	15	14	15	16	17	12	14	16	17	16	17
1	20	21	20	21	20	21	16	17	14	15	20	21	20	21	14	15	20	21	20	21
2 3 4	14	15	14	15	14	15	12	14	12	14	14	15	14	15	11	12	16	17	14	15
5	14	15	14	15	14	15	11	12	11	12	14	15	14	15	11	12	14	15	14	15
6	16	17	14	15	14	15	12	15	12	14	14	15	14	15	11	12	14	15	14	15
7	12	14	12	14	14	15	12	15	05	06	12	14	14	15	11	12	14	15	12	14
8	16	17	16	17	14	15	12	15	12	14	14	15	16	17	12	14	16	17	14	15

EXAMPLE, 2³ DENOTES 3/8

UPPER AND LOWER CASE LETTER WIDTHS

LETTERS	6 INCH UPPER CASE LETTERS				LETTERS	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES			SERIES	
	C	D	C	D		C	D
A	36	50	50	65	a	35	42
B	32	40	43	53	b	35	42
C	32	40	43	53	c	35	41
D	32	40	43	53	d	35	42
E	30	35	40	47	e	35	42
F	30	35	40	47	f	23	26
G	32	40	43	53	g	35	42
H	32	40	43	53	h	35	42
I	07	07	11	12	i	11	11
J	30	36	40	50	j	20	22
K	32	41	43	54	k	35	42
L	30	35	40	47	l	11	11
M	37	45	51	61	m	60	70
N	32	40	43	53	n	35	42
O	34	42	45	55	o	36	43
P	32	40	43	53	p	35	42
Q	34	42	45	55	q	35	42
R	32	40	43	53	r	26	32
S	32	40	43	53	s	36	42
T	30	35	40	47	t	27	32
U	32	40	43	53	u	35	42
V	35	44	47	60	v	42	47
W	44	52	60	70	w	55	64
X	34	40	45	53	x	44	51
Y	36	50	50	66	y	46	53
Z	32	40	43	53	z	36	43

NUMBER	6 INCH SERIES		8 INCH SERIES	
	C	D	C	D
1	12	14	15	20
2	32	40	43	53
3	32	40	43	53
4	35	43	47	57
5	32	40	43	53
6	32	40	43	53
7	32	40	43	53
8	32	40	43	53
9	32	40	43	53
0	34	42	45	55

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HRGreen.com
Illinois Professional Design Firm
#184-01122

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DESIGNED - DAG/BCK.
DRAWN - BCK.
CHECKED - DAG/DAD
DATE - 03/15/09

REVISED - DAG-10/28/09
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1
MAST ARM MOUNTED STREET NAME SIGNS

SCALE: SHEET NO. 7 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	24
CONTRACT NO.				63711

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY TDD DISTRICT 1, INSTALLED IN A NEMA 1752 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 1/2" (20mm) DIAMETER HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL. AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL. TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
8. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
9. DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
10. WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

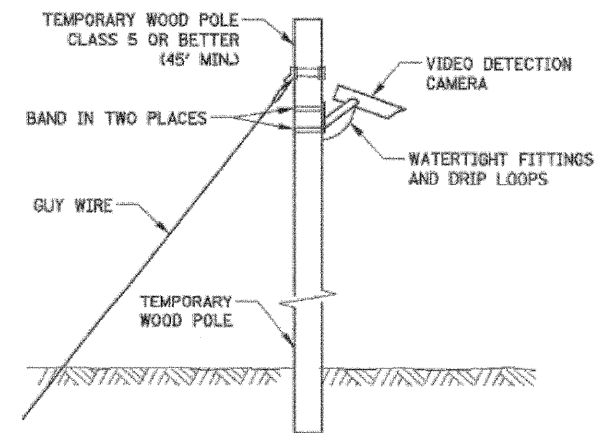
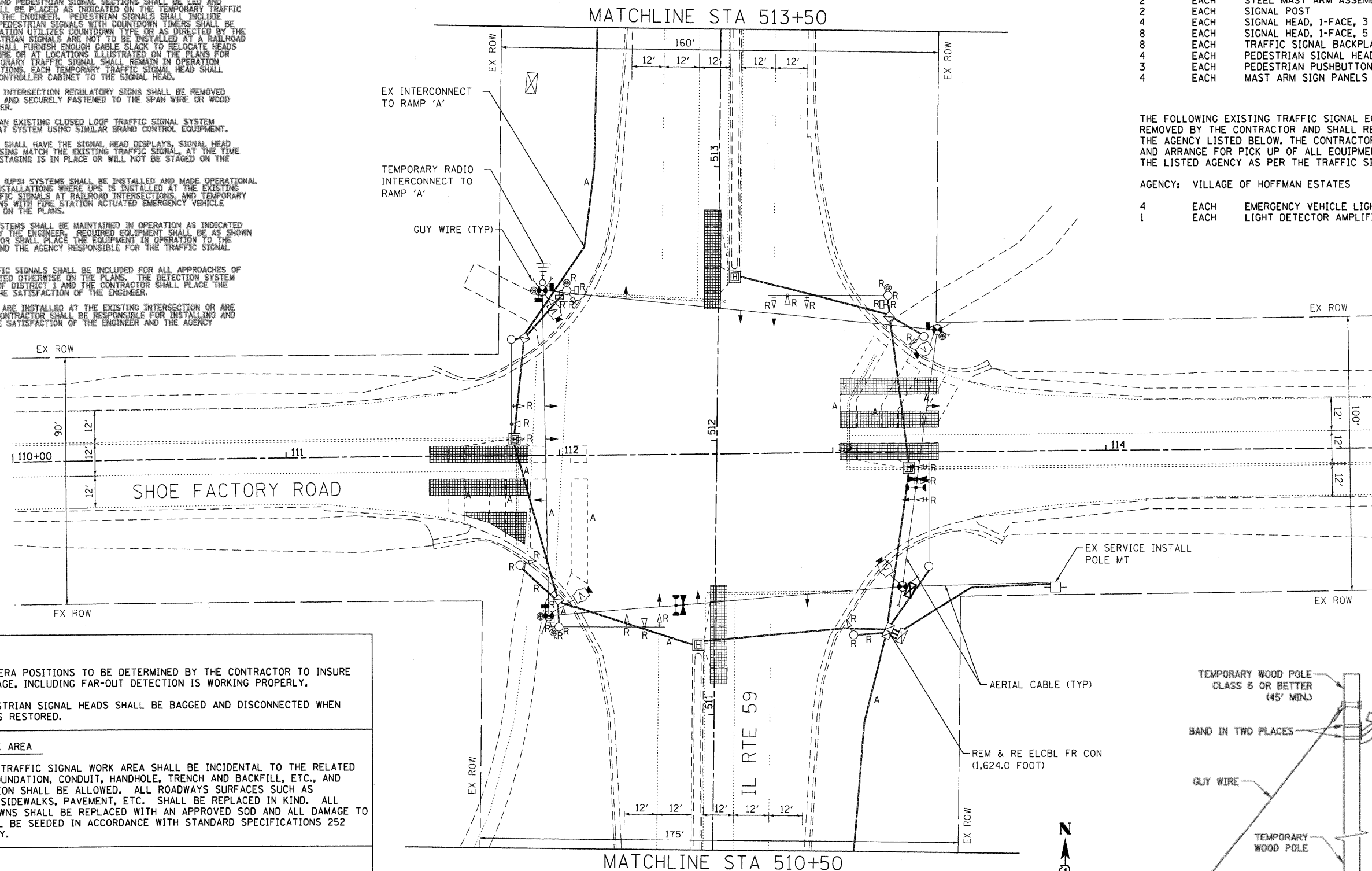
THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

2	EACH	STEEL MAST ARM ASSEMBLY AND POLE
2	EACH	SIGNAL POST
4	EACH	SIGNAL HEAD, 1-FACE, 3 SECTION
8	EACH	SIGNAL HEAD, 1-FACE, 5 SECTION
8	EACH	TRAFFIC SIGNAL BACKPLATE
4	EACH	PEDESTRIAN SIGNAL HEAD, 1-FACE
3	EACH	PEDESTRIAN PUSHBUTTON
4	EACH	MAST ARM SIGN PANELS

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

AGENCY: VILLAGE OF HOFFMAN ESTATES

4	EACH	EMERGENCY VEHICLE LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER



CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR TO INSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.

ALL TEMPORARY PEDESTRIAN SIGNAL HEADS SHALL BE BAGGED AND DISCONNECTED WHEN EXISTING PAVEMENT IS RESTORED.

RESTORATION OF WORK AREA

RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAYS SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL DAMAGE TO UNMOWED LAWNS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTE

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

NOTE

THE TEMPORARY RADIO INTERCONNECT SHALL NOT BE REMOVED UNTIL FIBER INTERCONNECT IS INSTALLED AND OPERATIONAL.

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 PROJECT CONTACT: Illinois Professional Design Firm
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HRGreen.com
 Illinois Professional Design Firm
 #184-001322

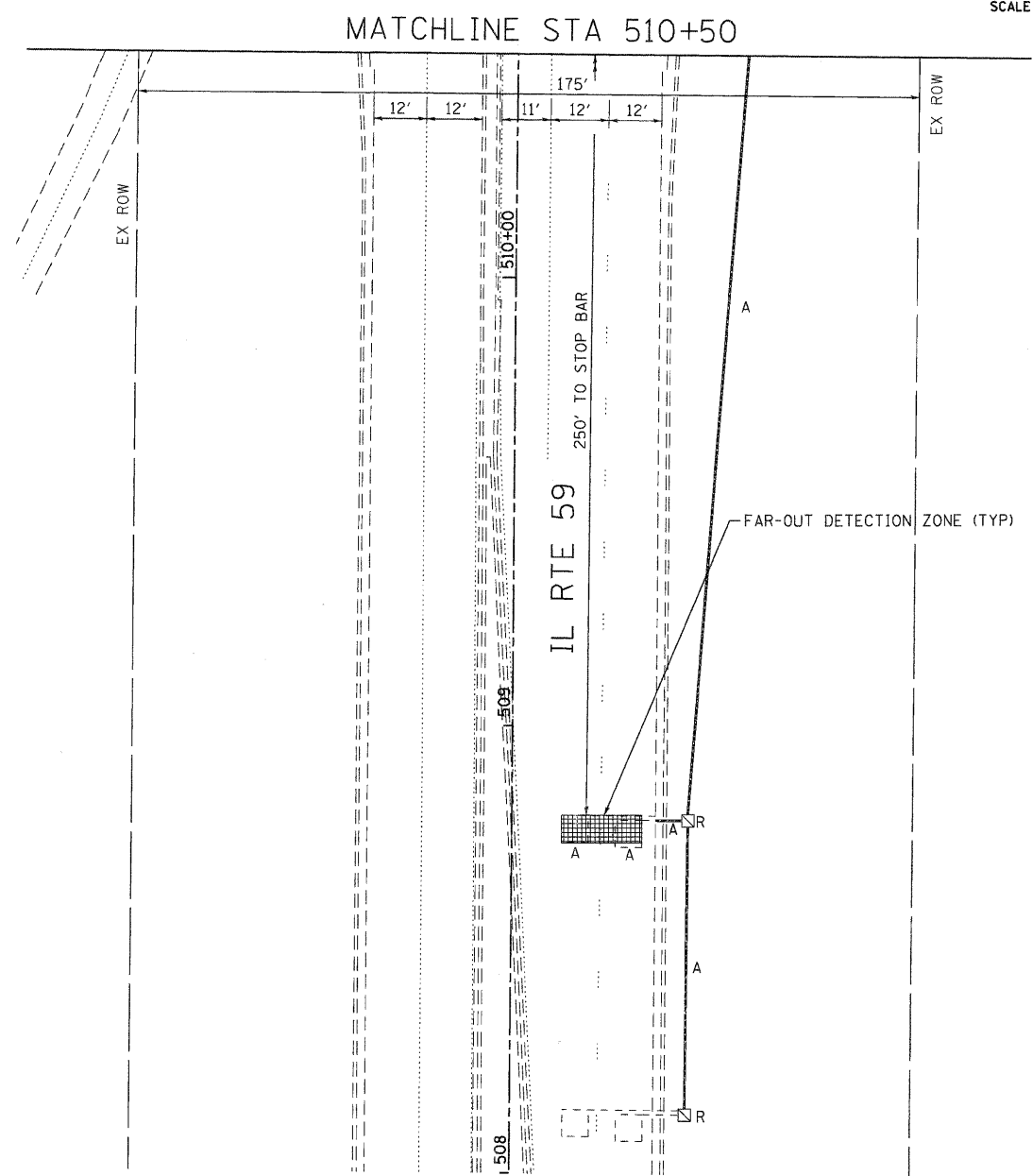
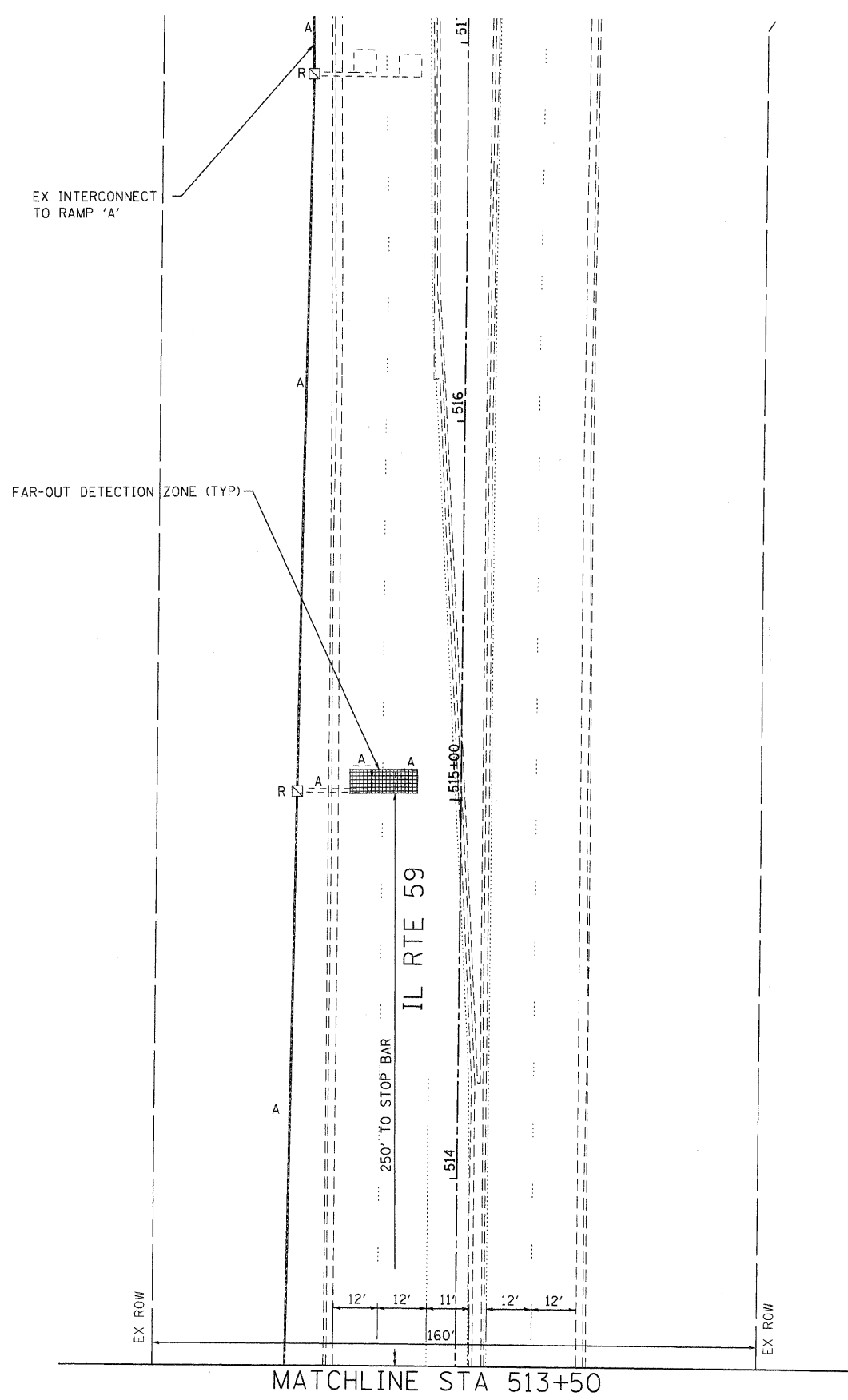
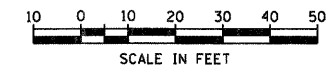
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	DATE - 4/5/12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN
IL ROUTE 59 AT SHOE FACTORY ROAD**

SCALE: 1"=20' SHEET NO. 1 OF 2 SHEETS STA. NA TO STA. NA

F.A.P. RTE. 338	SECTION 10-00084-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 25
CONTRACT NO. 63711			ILLINOIS FED. AID PROJECT	



RESTORATION OF WORK AREA

RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAYS SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL DAMAGE TO UNMOWED LAWNS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTE

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR TO INSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.

ALL TEMPORARY PEDESTRIAN SIGNAL HEADS SHALL BE BAGGED AND DISCONNECTED WHEN EXISTING PAVEMENT IS RESTORED.

NOTE

THE TEMPORARY RADIO INTERCONNECT SHALL NOT BE REMOVED UNTIL FIBER INTERCONNECT IS INSTALLED AND OPERATIONAL.

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 PROJECT CONTACT: jme1hut
 CLIENT: HRGreen.com
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PLOT DATE = 4/5/2012	CHECKED - DJ	REVISED -
	DATE - 4/5/12	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN
 IL ROUTE 59 AT SHOE FACTORY ROAD**

SCALE: 1"=20' SHEET NO. 2 OF 2 SHEETS STA. NA TO STA. NA

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	26
CONTRACT NO.				63711
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

CONSTRUCTION NOTES

ALL TEMPORARY PEDESTRIAN SIGNAL HEADS SHALL BE BAGGED AND DISCONNECTED WHEN EXISTING PAVEMENT IS REMOVED AND REMAINED BAGGED UNTIL PAVEMENT IS RESTORED.

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR TO INSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.

RESTORATION OF WORK AREA

RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAYS SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL DAMAGE TO UNMOWED LAWNS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTE

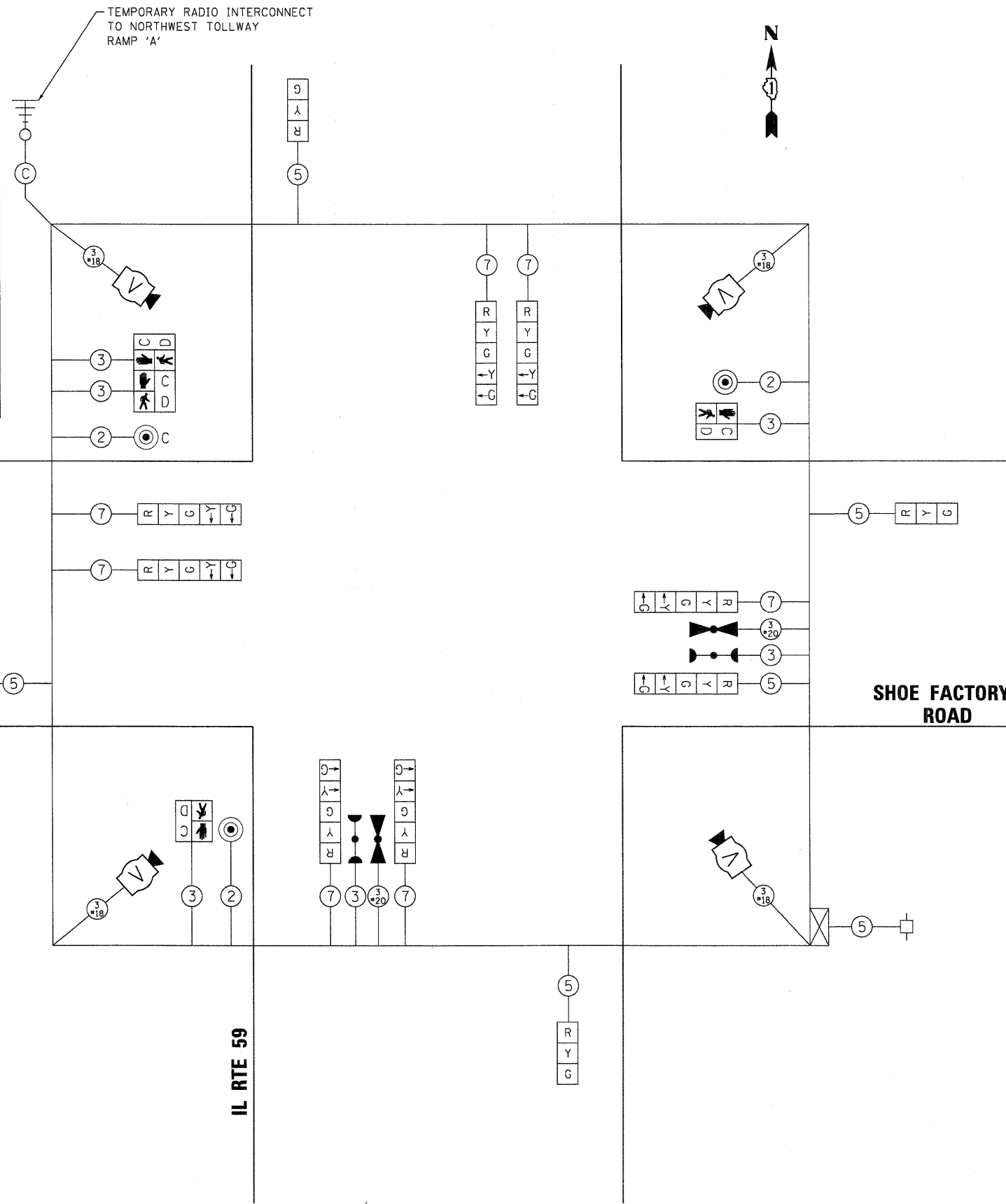
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

NOTE

THE TEMPORARY RADIO INTERCONNECT SHALL NOT BE REMOVED UNTIL FIBER INTERCONNECT IS RE-INSTALLED AND OPERATIONAL.

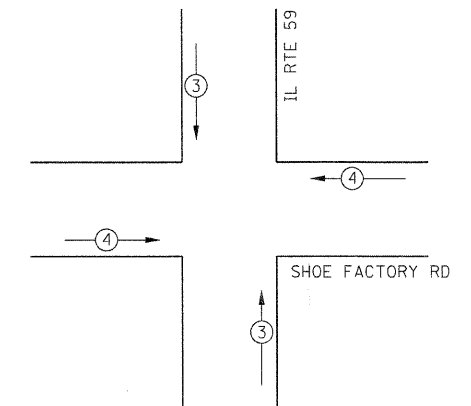
NOTE

PUSHBUTTON "C" SHALL PLACE A CALL IN PHASES 6 AND 8.



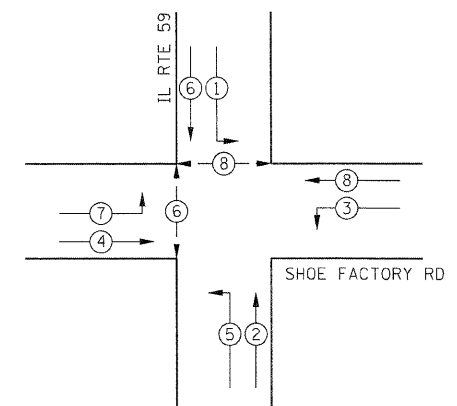
TEMPORARY CABLE PLAN

TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE

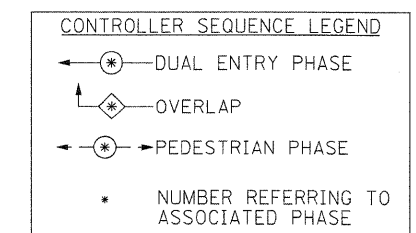


TEMPORARY EMERGENCY VEHICLE PREEMPTORS		
TEMPORARY EMERGENCY VEHICLE PREEMPTORS	3	4
MOVEMENT	↑	←

TEMPORARY CONTROLLER SEQUENCE



TEMPORARY PHASE DESIGNATION DIAGRAM



I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE		% OPERATION	
		INCAND.	LED		
SIGNAL (RED)	12		17	0.50	102.00
(YELLOW)	12		25	0.25	75.00
(GREEN)	12		15	0.25	45.00
ARROW (YELLOW)	8		12	0.10	9.60
ARROW (GREEN)	8		12	0.10	9.60
PEDESTRIAN SIGNAL	4		25	1.00	100.00
CONTROLLER	1		100	1.00	100.00
VIDEO SYSTEM	1		15	1.00	15.00
TOTAL =					456.20

ENERGY COSTS TO: ILLINOIS DEPARTMENT OF TRANSPORTATION
201 W. CENTER COURT
SCHAUMBURG, IL 60196

ENERGY SUPPLY CONTACT: KATHY NYSTROM
PHONE: (847) 608-2331
COMPANY: COMMONWEALTH EDISON

TEMPORARY CABLE PLAN AND TEMPORARY PHASE DESIGNATION DIAGRAM
IL ROUTE 59 AT SHOE FACTORY ROAD
SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. NA TO STA. NA

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	27
CONTRACT NO.			63711	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

USER NAME = jme3hut	DESIGNED - JJS	REVISED -
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PLOT DATE = 4/5/2012	CHECKED - DJ	REVISED -
	DATE - 4/5/12	REVISED -

HRGreen.com
Illinois Professional Design Firm
#184-001322

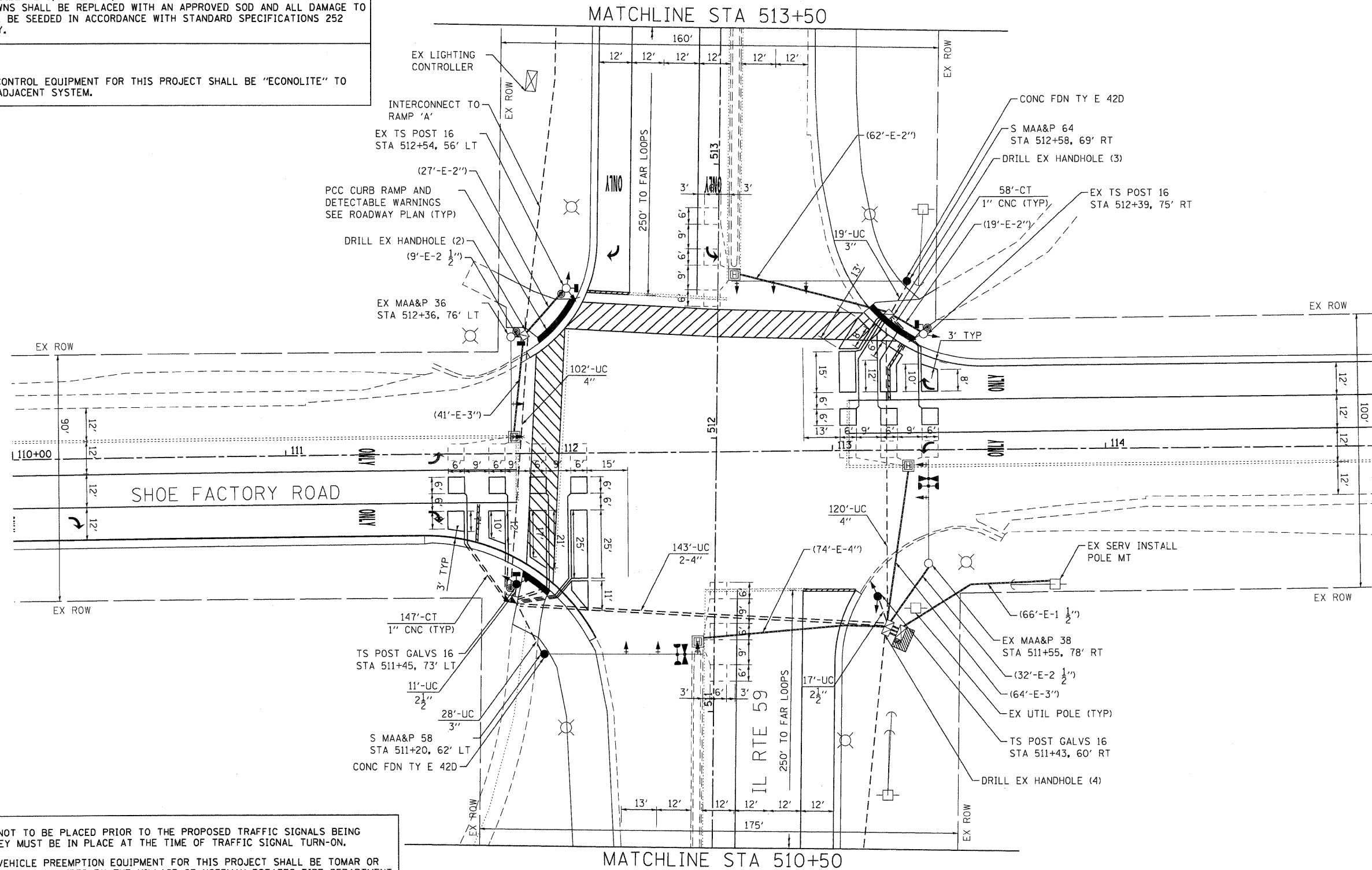
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RESTORATION OF WORK AREA

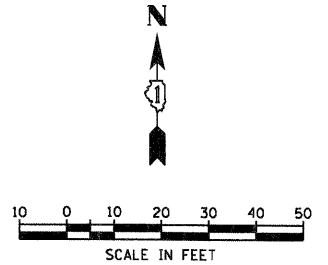
RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAYS SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL DAMAGE TO UNMOWED LAWNS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTE

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.



1. STOP BARS ARE NOT TO BE PLACED PRIOR TO THE PROPOSED TRAFFIC SIGNALS BEING OPERATIONAL. THEY MUST BE IN PLACE AT THE TIME OF TRAFFIC SIGNAL TURN-ON.
2. THE EMERGENCY VEHICLE PREEMPTION EQUIPMENT FOR THIS PROJECT SHALL BE TOMAR OR AN APPROVED EQUAL AS REQUIRED BY THE VILLAGE OF HOFFMAN ESTATES FIRE DEPARTMENT.
3. NEW LED CONFIRMATION BEACONS SHALL BE INSTALLED IN ACCORDANCE WITH THE SPECIFICATIONS FOR EVP AND UNINTERRUPTIBLE POWER SUPPLY SPECIAL. THIS WORK WILL BE INCLUDED IN THE COST FOR UNINTERRUPTIBLE POWER SUPPLY SPECIAL.
4. THE CONCRETE APRON AND EARTH EXCAVATION REQUIRED WILL BE INCLUDED IN THE COST FOR UNINTERRUPTIBLE POWER SUPPLY SPECIAL.



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 PROJECT CONTACT: jmh
 CLIENT: HRGreen.com
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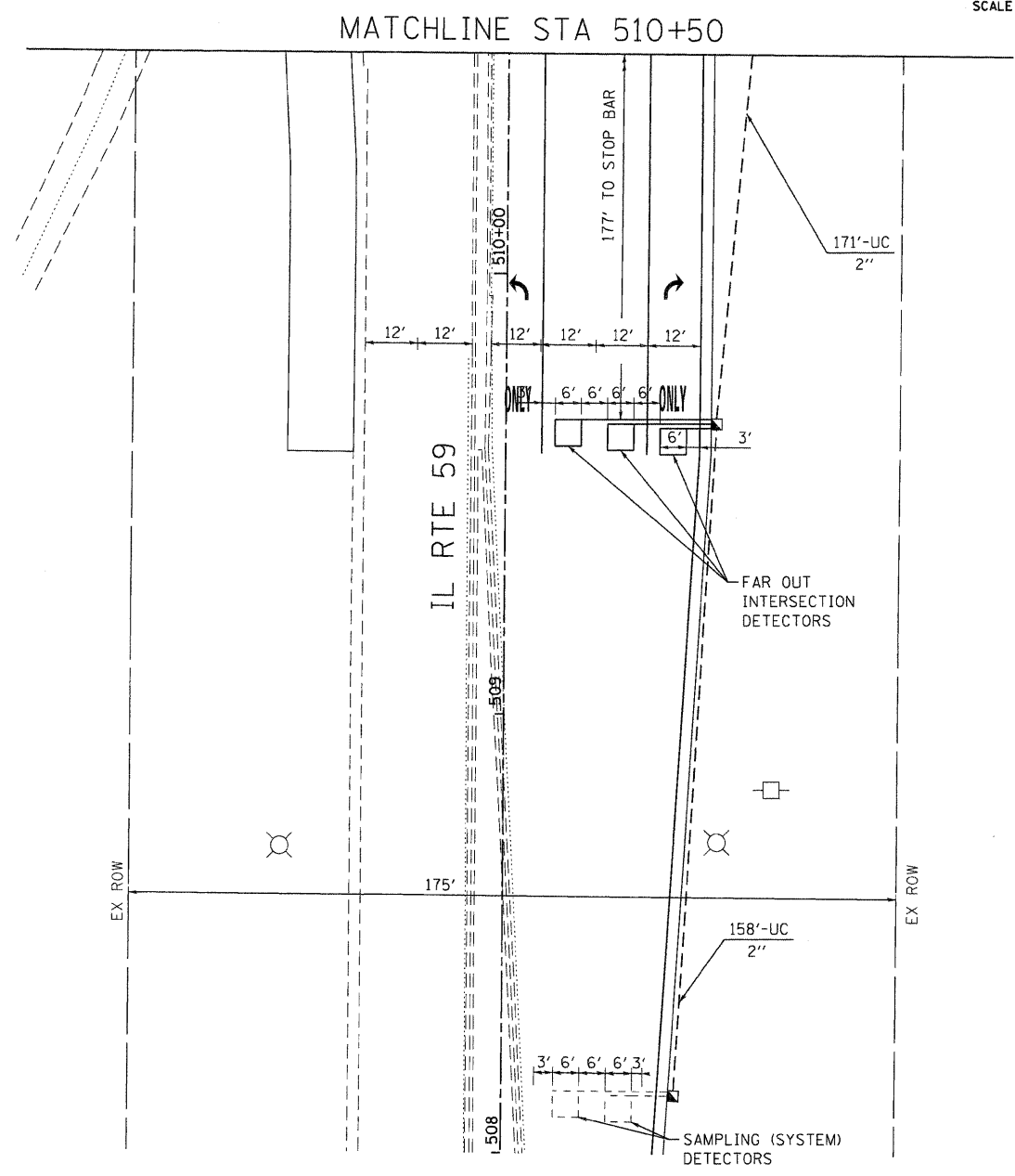
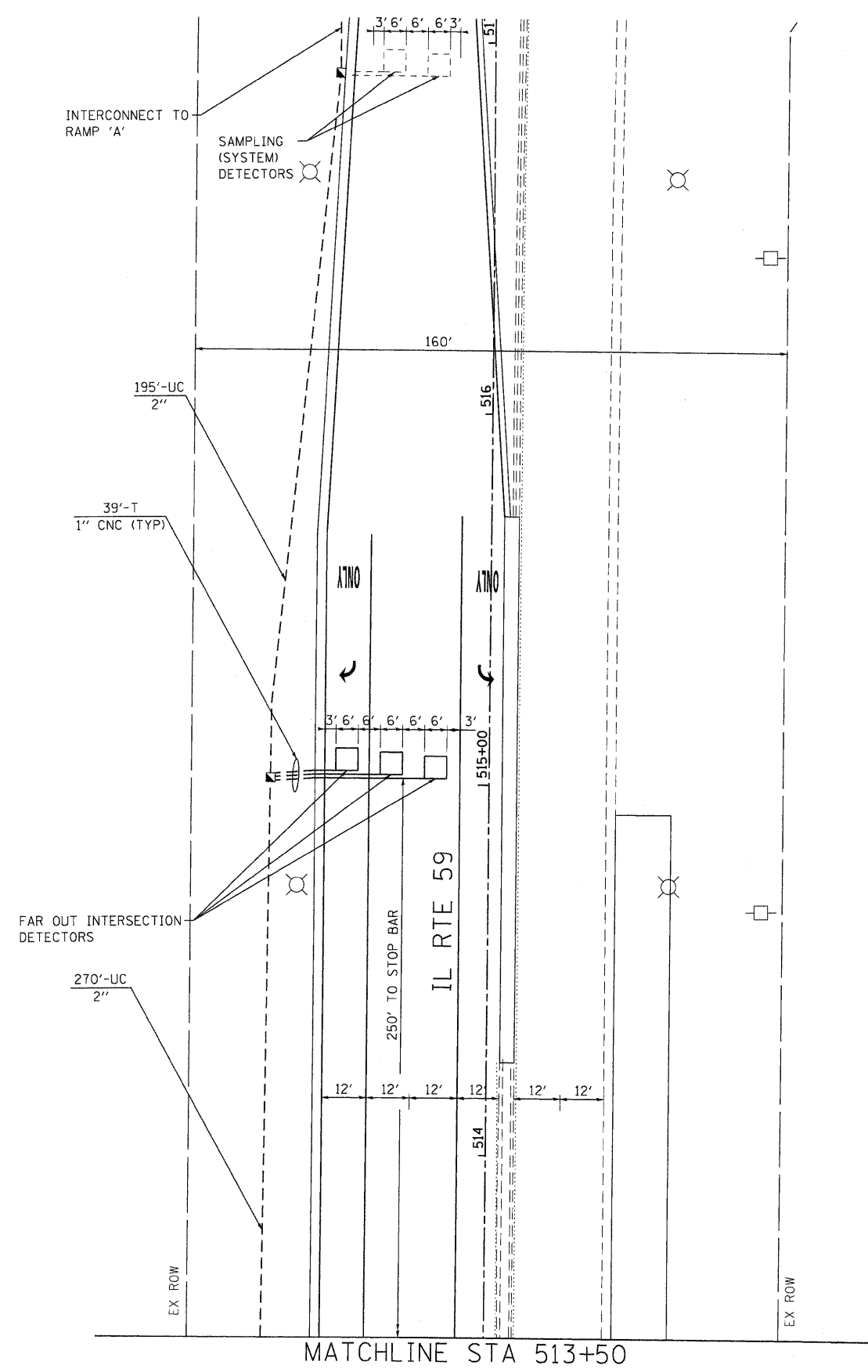
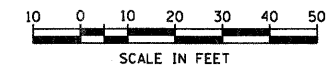
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	DATE - 4/5/12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL INSTALLATION PLAN
IL ROUTE 59 AT SHOE FACTORY ROAD**

SCALE: 1"=20' SHEET NO. 1 OF 2 SHEETS STA. NA TO STA. NA

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	28
CONTRACT NO. 63711				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



RESTORATION OF WORK AREA

RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAYS SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL DAMAGE TO UNMOWED LAWNS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTE

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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 PROJECT CONTACT: HRGreen.com
 CLIENT: HRGreen.com
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PLOT DATE = 4/5/2012	CHECKED - DJ	REVISED -
	DATE - 4/5/12	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL INSTALLATION PLAN
 IL ROUTE 59 AT SHOE FACTORY ROAD**

SCALE: 1"=20' SHEET NO. 2 OF 2 SHEETS STA. NA TO STA. NA

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	29
CONTRACT NO.			63711	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

SCHEDULE OF QUANTITIES

SIGN PANEL - TYPE 1	SQ. FT	15.0
SIGN PANEL - TYPE 2	SQ. FT	21.0
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	441.0
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	28.0
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	47.0
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	508.0
HANDHOLE	EACH	2
DOUBLE HANDHOLE	EACH	1
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	707.5
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1,197.0
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	614.5
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	3,100.0
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1,802.0
TRAFFIC SIGNAL POST, GALVANIZED STEEL, 16 FT.	EACH	2
STEEL MAST ARM ASSEMBLY AND POLE, 58 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 64 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	8.0
CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER	FOOT	42.0
DRILL EXISTING HANDHOLE	EACH	9
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	2
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	8
SIGNAL HEAD, LED, 2-FACE, 5-SECTION, BRACKET MOUNTED	EACH	4
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	14
INDUCTIVE LOOP DETECTOR	EACH	2
DETECTOR LOOP, TYPE 1	FOOT	1,049.0
LIGHT DETECTOR	EACH	2
LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	4
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
MODIFY EXISTING CONTROLLER	EACH	1
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	6,940.5
REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	1,624.0
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	3
REMOVE EXISTING CONCRETE FOUNDATION	EACH	4
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1
UNINTERRUPTIBLE POWER SUPPLY, SPECIAL	EACH	1
EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	336.0
ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	1,247.0

*NOTE:
100% OF THE COST SHALL BE PAID FOR
BY THE VILLAGE OF HOFFMAN ESTATES.

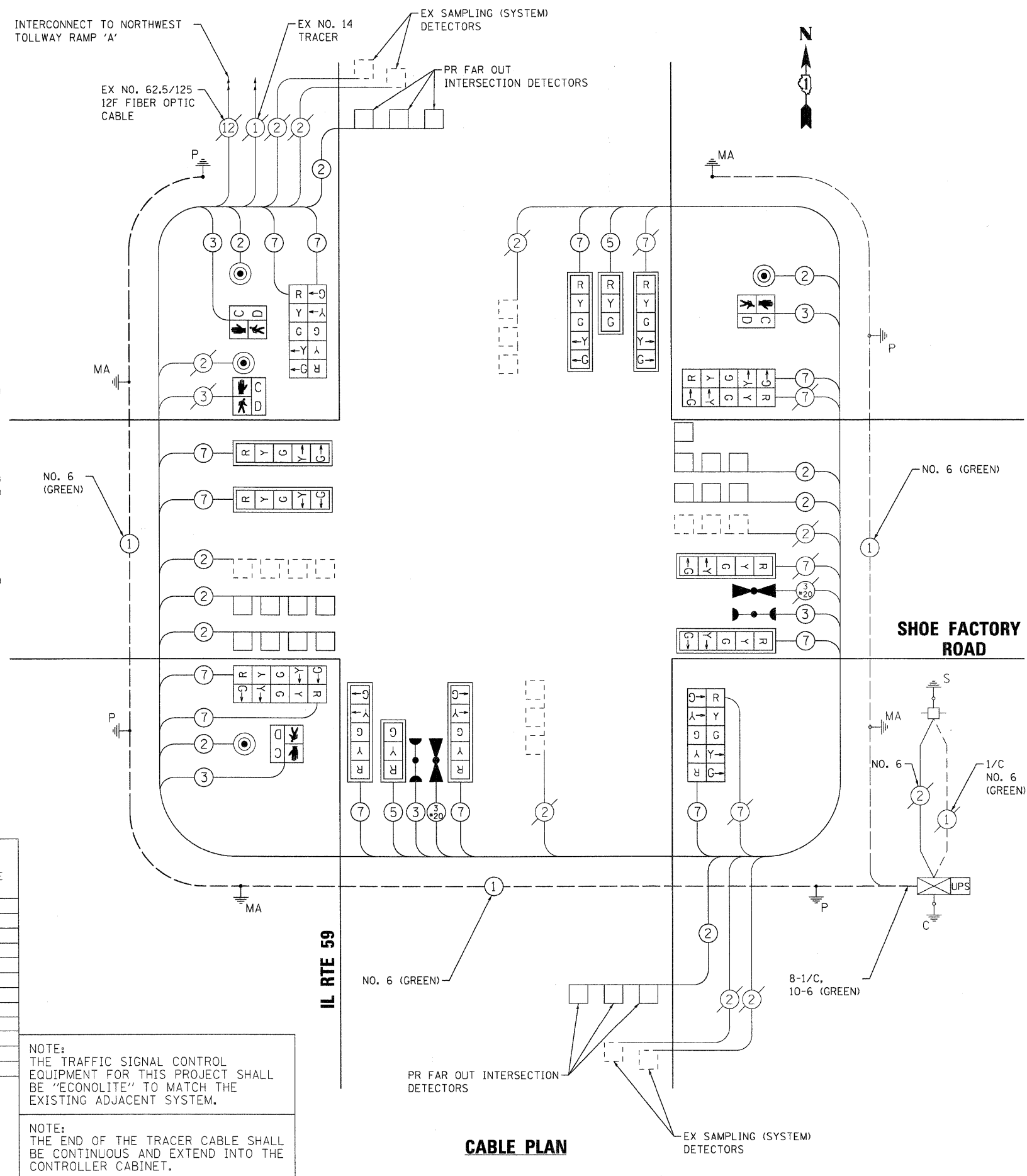
I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE		% OPERATION	
		INCAND.	LED		
SIGNAL (RED)	18	17		0.50	153.00
(YELLOW)	18		25	0.25	112.50
(GREEN)	18	15		0.25	67.50
ARROW (YELLOW)	16		12	0.10	19.20
ARROW (GREEN)	16		12	0.10	19.20
PEDESTRIAN SIGNAL	4		25	1.00	100.00
CONTROLLER	1		100	1.00	100.00
UPS	1		25	1.00	25.00
TOTAL =					596.40

ENERGY COSTS TO: ILLINOIS DEPARTMENT OF TRANSPORTATION
201 W. CENTER COURT
SCHAUMBURG, IL 60196

ENERGY SUPPLY CONTACT: KATHY NYSTROM
PHONE: (847) 608-2331
COMPANY: COMMONWEALTH EDISON

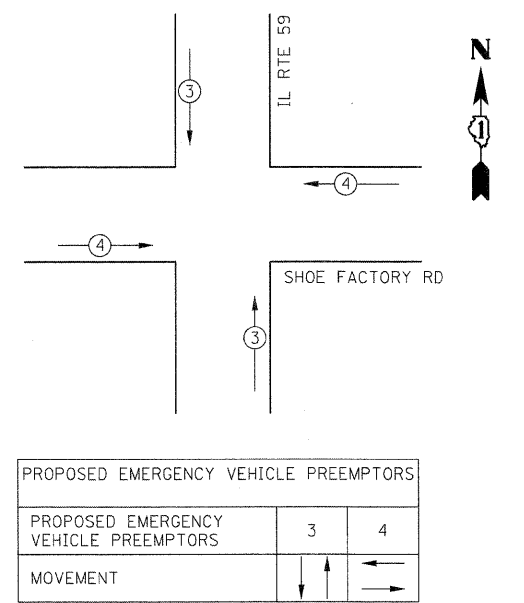
NOTE:
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

NOTE:
THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.

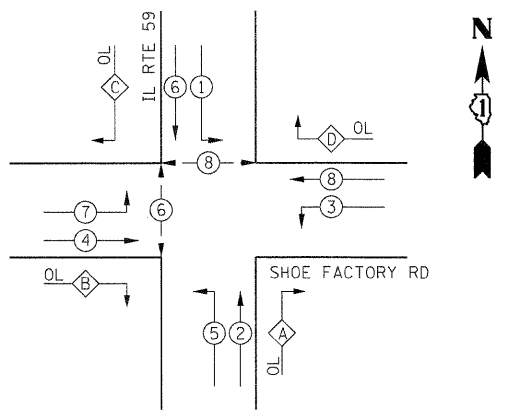


CABLE PLAN

EMERGENCY VEHICLE PREEMPTION SEQUENCE

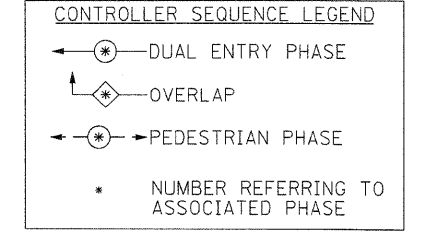


CONTROLLER SEQUENCE



PHASE DESIGNATION DIAGRAM

OVERLAP PHASE	PERMISSIVE PHASE	PROTECTED PHASE
A =	2 +	3
B =	4 +	5
C =	6 +	7
D =	8 +	1



COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen
 DATE PLOTTED: 4/5/2012 5:53:17 PM
 FILE NAME: 0177-31gcb001.dgn
 PLOT DRIVER: pldrvr
 PLOT TABLE: standard-trans.tbl

HRGreen.com
Illinois Professional Design Firm
#184-001922

USER NAME = jme1hui	DESIGNED - JJS	REVISED -
PLOT SCALE = N.T.S.	DRAWN -	REVISED -
PLOT DATE = 4/5/2012	CHECKED - DJ	REVISED -
	DATE - 4/5/12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CABLE PLAN AND PHASE DESIGNATION DIAGRAM
IL ROUTE 59 AT SHOE FACTORY ROAD**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. NA TO STA. NA

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	30
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63711	

NOTES:

1. THE EXISTING FIBER OPTIC AND TRACER CABLE FROM SHOE FACTORY ROAD TO I-90 TOLL RAMP A SHALL BE PULLED BACK TO THE FAR HANDHOLE ON THE NORTH LEG AND STORED IN IT FOR REUSE ON THE PROPOSED INTERCONNECT. THIS WORK WILL BE PAID FOR AS REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT.

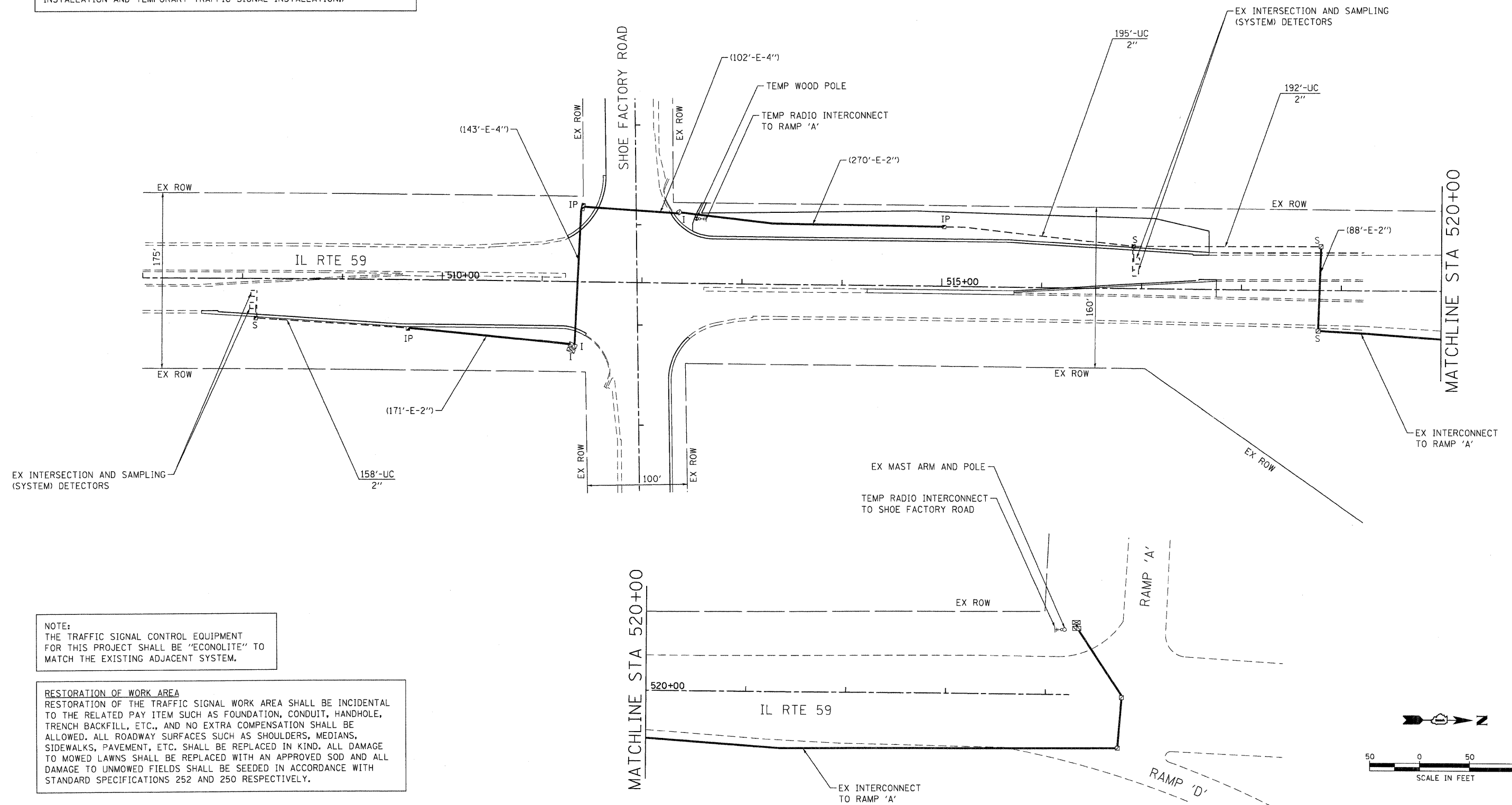
2. A TEMPORARY RADIO INTERCONNECT SHALL BE INSTALLED TO MAINTAIN THE EXISTING INTERCONNECT DURING CONSTRUCTION. EQUIPMENT SHALL BE INSTALLED AT I-90 TOLL RAMP A AND SHOE FACTORY ROAD FOR THIS TEMPORARY INTERCONNECT TO MAINTAIN A WORKING ORDER. (SEE SPECIAL PROVISIONS FOR MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION AND TEMPORARY TRAFFIC SIGNAL INSTALLATION.)

NOTE:

THE FIBER OPTIC CABLE AND TRACER CABLE SHALL BE STORED AND PROTECTED DURING CONSTRUCTION. THE CONTRACTOR WILL BE RESPONSIBLE FOR ANY DAMAGE AND WILL BE REQUIRED TO REPLACE ANY DAMAGED CABLE AT NO ADDITIONAL COST TO THE CONTRACT.

NOTE:

THE FIBER OPTIC CABLE SHALL BE TESTED ACCORDING TO ARTICLE 801.13(d) AFTER BEING REINSTALLED.



NOTE:
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA
RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

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 PROJECT CONTACT: HRGreen.com
 CLIENT: Illinois Professional Design Firm
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 PLOT DATE: 4/5/2012
 PLOT SCALE: 1"=50'
 PLOT DATE: 4/5/2012



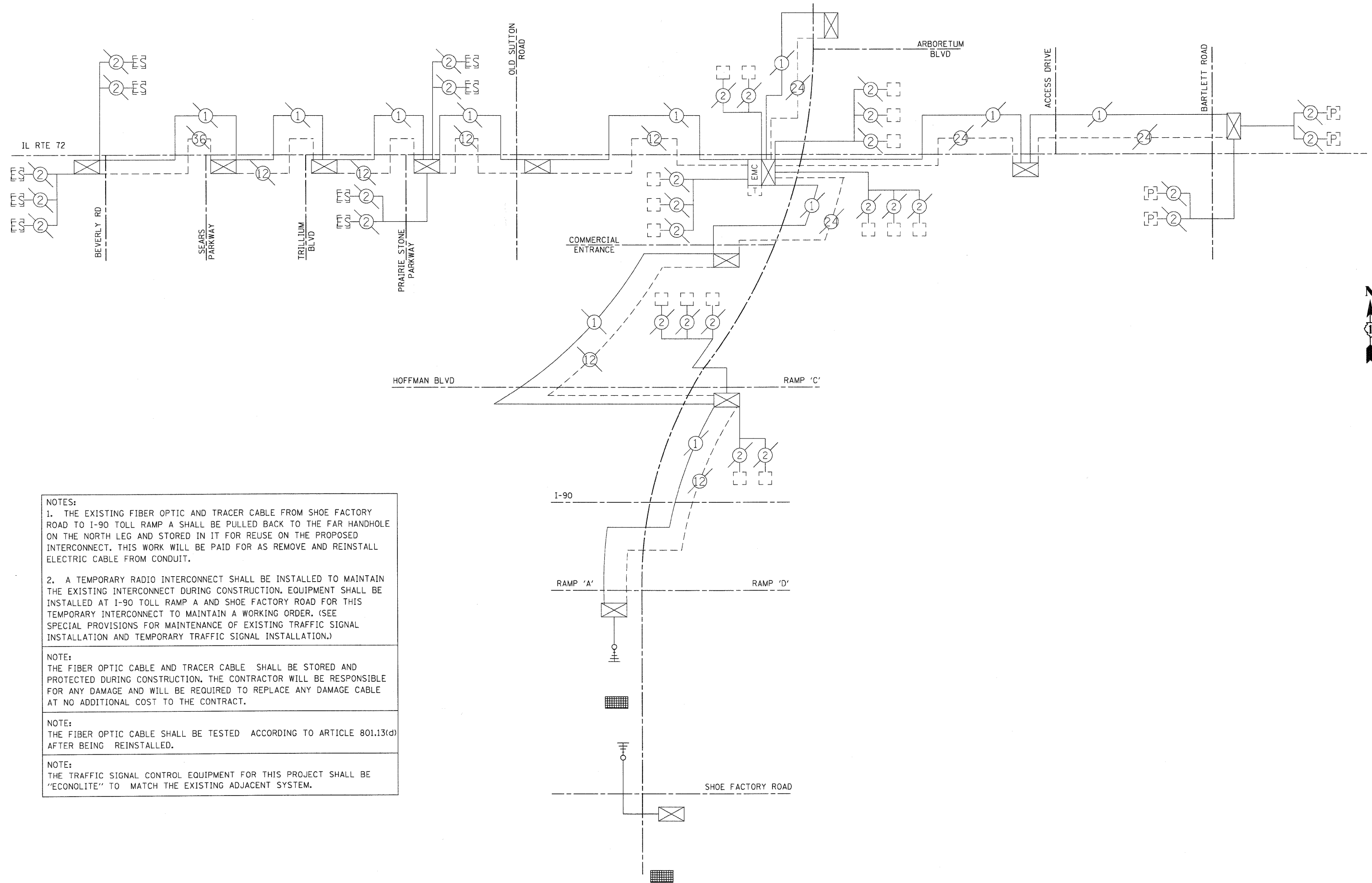
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	DRAWN -	REVISED -
	CHECKED - DJ	REVISED -
	DATE - 4/5/12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY INTERCONNECT PLAN
IL ROUTE 59 AT SHOE FACTORY ROAD**

SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. NA TO STA. NA

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	31
CONTRACT NO. 63711			ILLINOIS FED. AID PROJECT	



NOTES:

1. THE EXISTING FIBER OPTIC AND TRACER CABLE FROM SHOE FACTORY ROAD TO I-90 TOLL RAMP A SHALL BE PULLED BACK TO THE FAR HANDHOLE ON THE NORTH LEG AND STORED IN IT FOR REUSE ON THE PROPOSED INTERCONNECT. THIS WORK WILL BE PAID FOR AS REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT.

2. A TEMPORARY RADIO INTERCONNECT SHALL BE INSTALLED TO MAINTAIN THE EXISTING INTERCONNECT DURING CONSTRUCTION. EQUIPMENT SHALL BE INSTALLED AT I-90 TOLL RAMP A AND SHOE FACTORY ROAD FOR THIS TEMPORARY INTERCONNECT TO MAINTAIN A WORKING ORDER. (SEE SPECIAL PROVISIONS FOR MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION AND TEMPORARY TRAFFIC SIGNAL INSTALLATION.)

NOTE:
THE FIBER OPTIC CABLE AND TRACER CABLE SHALL BE STORED AND PROTECTED DURING CONSTRUCTION. THE CONTRACTOR WILL BE RESPONSIBLE FOR ANY DAMAGE AND WILL BE REQUIRED TO REPLACE ANY DAMAGE CABLE AT NO ADDITIONAL COST TO THE CONTRACT.

NOTE:
THE FIBER OPTIC CABLE SHALL BE TESTED ACCORDING TO ARTICLE 801.13(d) AFTER BEING REINSTALLED.

NOTE:
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

COMPANY NAME: HRGreen
 PROJECT CONTACT: jmelhut
 CLIENT: HRGreen.com
 DATE PLOTTED: 4/5/2012 5:03:26 PM
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 PLOT DRIVER: plotdrv
 PEN TABLE: Standard-trans.tbl


HRGreen.com
 Illinois Professional Design Firm
 #184-01322

USER NAME = jmelhut	DESIGNED - JJS	REVISED -
PLOT SCALE = N.T.S.	DRAWN -	REVISED -
PLOT DATE = 4/5/2012	CHECKED - DJ	REVISED -
	DATE - 4/5/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY INTERCONNECT SCHEMATIC
IL RTE 59 FROM SHOE FACTORY RD TO IL RTE 72

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. NA TO STA. NA

F.A.P. RTE. 338	SECTION 10-00084-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 32
CONTRACT NO. 63711			ILLINOIS FED. AID PROJECT	

NOTES:

1. THE EXISTING FIBER OPTIC AND TRACER CABLE FROM SHOE FACTORY ROAD TO I-90 TOLL RAMP A SHALL BE PULLED BACK TO THE FAR HANDHOLE ON THE NORTH LEG AND STORED IN IT FOR REUSE ON THE PROPOSED INTERCONNECT. THIS WORK WILL BE PAID FOR AS REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT.

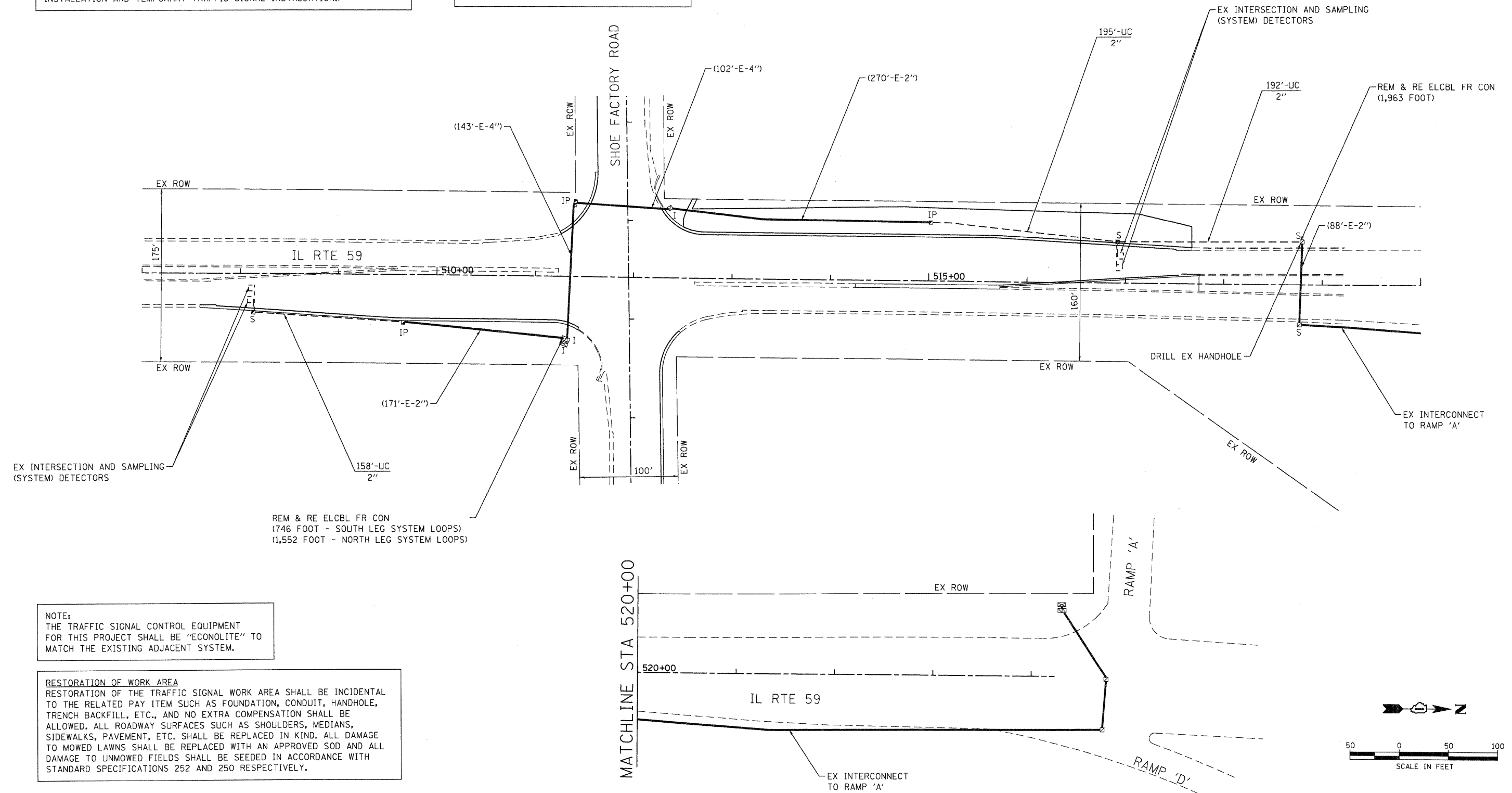
2. A TEMPORARY RADIO INTERCONNECT SHALL BE INSTALLED TO MAINTAIN THE EXISTING INTERCONNECT DURING CONSTRUCTION. EQUIPMENT SHALL BE INSTALLED AT I-90 TOLL RAMP A AND SHOE FACTORY ROAD FOR THIS TEMPORARY INTERCONNECT TO MAINTAIN A WORKING ORDER. (SEE SPECIAL PROVISIONS FOR MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION AND TEMPORARY TRAFFIC SIGNAL INSTALLATION.)

NOTE:

THE FIBER OPTIC CABLE AND TRACER CABLE SHALL BE STORED AND PROTECTED DURING CONSTRUCTION. THE CONTRACTOR WILL BE RESPONSIBLE FOR ANY DAMAGE AND WILL BE REQUIRED TO REPLACE ANY DAMAGED CABLE AT NO ADDITIONAL COST TO THE CONTRACT.

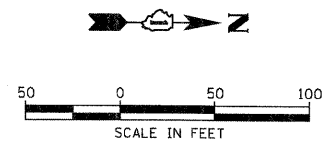
NOTE:

THE FIBER OPTIC CABLE SHALL BE TESTED ACCORDING TO ARTICLE 801.13(d) AFTER BEING REINSTALLED.



NOTE:
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA
RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



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 PROJECT CONTACT: jmelhus
 CLIENT: HRGreen
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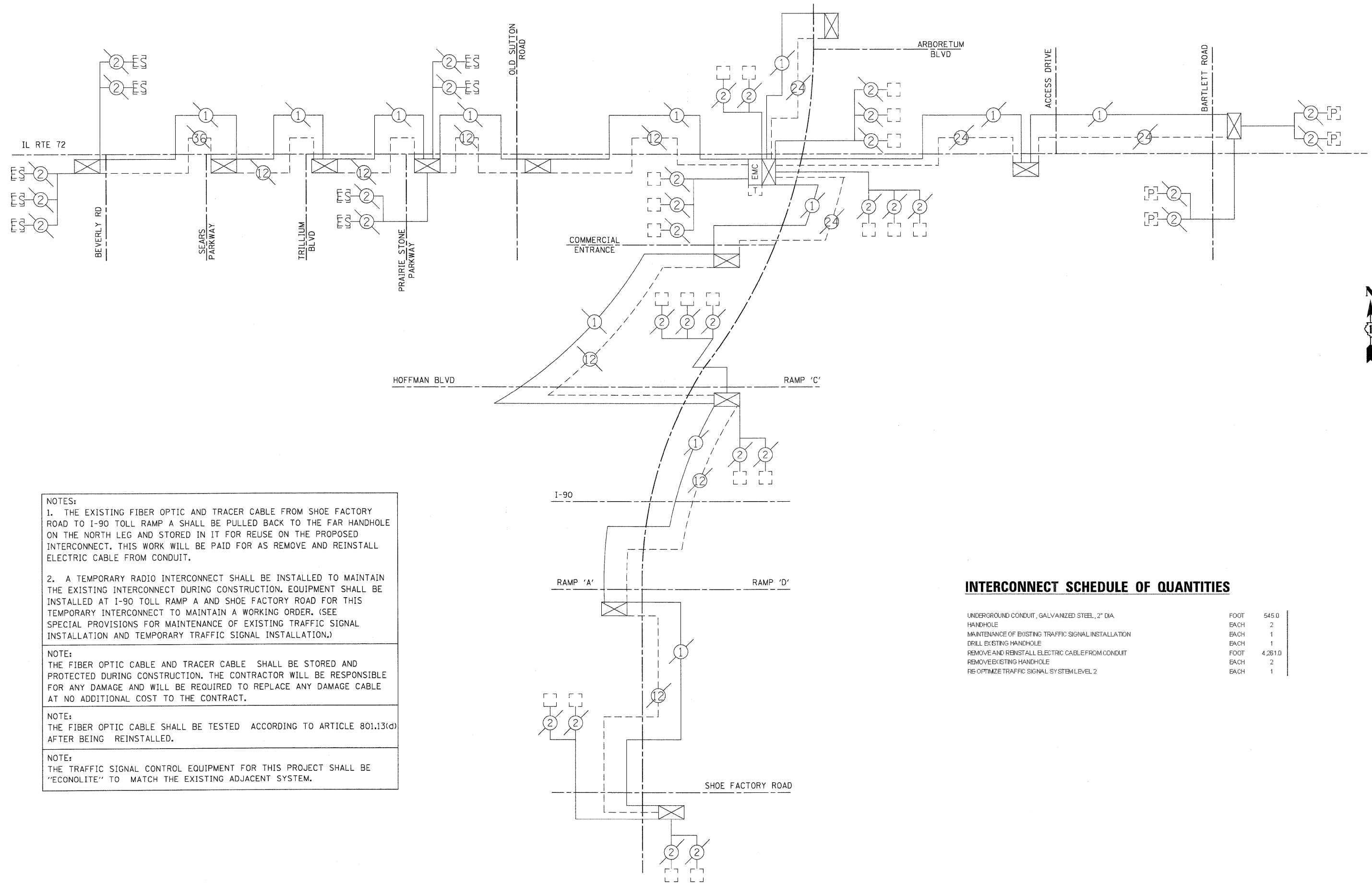
HRGreen.com Illinois Professional Design Firm #184-001322	USER NAME = jmelhus	DESIGNED - JJS	REVISED -
	PLOT SCALE = 1"=50'	CHECKED - DJ	REVISED -
	PLOT DATE = 4/5/2012	DATE - 4/5/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INTERCONNECT PLAN
IL ROUTE 59 AT SHOE FACTORY ROAD

SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. NA TO STA. NA

F.A.P. RTE. 338	SECTION 10-00084-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 33
CONTRACT NO. 63711			ILLINOIS FED. AID PROJECT	



NOTES:

1. THE EXISTING FIBER OPTIC AND TRACER CABLE FROM SHOE FACTORY ROAD TO I-90 TOLL RAMP A SHALL BE PULLED BACK TO THE FAR HANDHOLE ON THE NORTH LEG AND STORED IN IT FOR REUSE ON THE PROPOSED INTERCONNECT. THIS WORK WILL BE PAID FOR AS REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT.

2. A TEMPORARY RADIO INTERCONNECT SHALL BE INSTALLED TO MAINTAIN THE EXISTING INTERCONNECT DURING CONSTRUCTION. EQUIPMENT SHALL BE INSTALLED AT I-90 TOLL RAMP A AND SHOE FACTORY ROAD FOR THIS TEMPORARY INTERCONNECT TO MAINTAIN A WORKING ORDER. (SEE SPECIAL PROVISIONS FOR MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION AND TEMPORARY TRAFFIC SIGNAL INSTALLATION.)

NOTE:
THE FIBER OPTIC CABLE AND TRACER CABLE SHALL BE STORED AND PROTECTED DURING CONSTRUCTION. THE CONTRACTOR WILL BE RESPONSIBLE FOR ANY DAMAGE AND WILL BE REQUIRED TO REPLACE ANY DAMAGE CABLE AT NO ADDITIONAL COST TO THE CONTRACT.

NOTE:
THE FIBER OPTIC CABLE SHALL BE TESTED ACCORDING TO ARTICLE 801.13(d) AFTER BEING REINSTALLED.

NOTE:
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

INTERCONNECT SCHEDULE OF QUANTITIES

UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	545.0
HANDHOLE	EACH	2
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
DRILL EXISTING HANDHOLE	EACH	1
REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	4,261.0
REMOVE EXISTING HANDHOLE	EACH	2
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2	EACH	1

COMPANY NAME: HRGreen
 PROJECT CONTACT: jmelhus
 CLIENT: HRGreen.com
 DATE PLOTTED: 4/5/2012 5:53:34 PM
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 PLOT USER: perdue
 PLOT TABLE: standard-trans.tbl



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PLOT SCALE = N.T.S.	CHECKED - DJ	REVISED -
PLOT DATE = 4/5/2012	DATE - 4/5/12	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**INTERCONNECT SCHEMATIC
 IL RTE 59 FROM SHOE FACTORY RD TO IL RTE 72**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. NA TO STA. NA

F.A.P. RTE. 338	SECTION 10-00084-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 34
CONTRACT NO. 63711			ILLINOIS FED. AID PROJECT	







GENERAL ELECTRICAL PLAN NOTES

1. THE CONTRACTOR SHALL SUBMIT FOR THE RESIDENT ENGINEER'S REVIEW WITHIN 30 DAYS AFTER CONTRACT EXECUTION, EIGHT COPIES OF APPROVED MANUFACTURER'S PRODUCT DATA AND DETAILED SHOP DRAWINGS TO THE RESIDENT ENGINEER.
2. THE QUANTITIES OF RACEWAY WHERE INDICATED IN THESE PLANS ARE APPROXIMATIONS ONLY. THE CONTRACTOR SHALL FIELD VERIFY ALL LENGTHS AND SHALL INSTALL RACEWAYS IN COMPLETE COMPLIANCE WITH THE SPECIFIED REQUIREMENTS.
3. THE CONTRACTOR SHALL NOTIFY J.U.L.I.E. TO LOCATE AND MARK/STAKE ALL UNDERGROUND UTILITIES.
4. TRENCHES FOR LIGHTING CIRCUITS SHALL HAVE A MINIMUM DEPTH OF 30".
5. THE CONTRACTOR SHALL COORDINATE THE FOUNDATION HEIGHTS WITH THE FINISHED GRADE ELEVATION SO FOUNDATION DOES NOT PROTRUDE BEYOND SPECIFIED REQUIREMENTS.
6. AT THE COMPLETION OF THE PROJECT THE LIGHTING SYSTEMS SHALL BE OWNED AND MAINTAINED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.
7. LIGHTING SYSTEM INSTALLATION SHALL CONFORM TO THE LATEST IDOT STANDARDS, NEC AND LOCAL CODES.
8. ALL ELECTRICAL EQUIPMENT AND PRODUCTS SHALL BE U/L LISTED AND LABELED.
9. THE CONTRACTOR SHALL SUBMIT FOUR (4) SETS OF FULL SIZED COMPLETE AND ACCURATE "RECORD DRAWINGS" TO THE ENGINEER FOR REVIEW AND COMMENT, AS SPECIFIED. THE "RECORD DRAWINGS" SHALL BE UPDATED ON A REGULAR BASIS AND DEPICT ALL ROADWAY LIGHTING MATERIAL INSTALLATIONS WITH ANY CHANGES INDICATED IN RED. "REPRODUCIBLE RECORD DRAWINGS" SHALL BE SUBMITTED AT LEAST 7 DAYS BEFORE SCHEDULING A FINAL INSPECTION.
10. PROVIDE 9-INCH HIGH TRANSFORMER BASE FOR EACH RELOCATED POLE.
11. EXISTING TAPERED ALUMINUM POLE IS 47-1/2 FOOT HIGH WITH 15-FOOT MAST ARM.
12. THE CONTRACTOR SHALL NOTIFY THE IDOT ELECTRICAL MAINTENANCE SECTION AT (708)-524-2145 TO LOCATE UNDERGROUND ELECTRIC CABLE AND TRANSFER THE MAINTENANCE TO ELECTRICAL CONTRACTOR.


SUMMARY OF QUANTITIES - ROADWAY LIGHTING

PAY ITEM	DESCRIPTION	UNIT	QUANTITY
81002810	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA	FOOT	70
81603081	UNIT DUCT, 600V, 3-1C NO.2, 1/C NO.4 GROUND, (XLP-TYPE USE), 1 1/2" DIA. POLYETHYLENE	FOOT	3,084
83600355	LIGHT POLE FOUNDATION METAL, 15" BOLT CIRCLE, 8" X 6"	EACH	7
83800105	BREAKAWAY DEVICE TRANSFORMER BASE 11.5" BOLT CIRCLE	EACH	7
84200500	REMOVAL OF LIGHTING UNIT, SALVAGE	EACH	7
84200804	REMOVAL OF POLE FOUNDATION	EACH	7
84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	7
89502300	REMOVE ELECTRICAL CABLE IN CONDUIT	FOOT	300
89502380	REMOVE EXISTING HANDHOLE	EACH	1
Z0033028	MAINTENANCE OF LIGHTING SYSTEM	CAL MO	3
X8410102	TEMPORARY LIGHTING SYSTEM	LSUM	1

LEGEND

-  RELOCATED LIGHTING UNIT (PROPOSED LOCATION)
-  EXISTING LIGHTING UNIT
-  UNIT DUCT 3 1/C #2 WITH 1/C #4 GROUND IN 1 1/2" POLYETHYLENE
-  EXISTING UNIT DUCT
-  EXISTING LIGHTING CONTROLLER
-  GROUND ROD

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 CLIENT: HRGreen.com
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 FILE NAME: 0177-lighting.dgn
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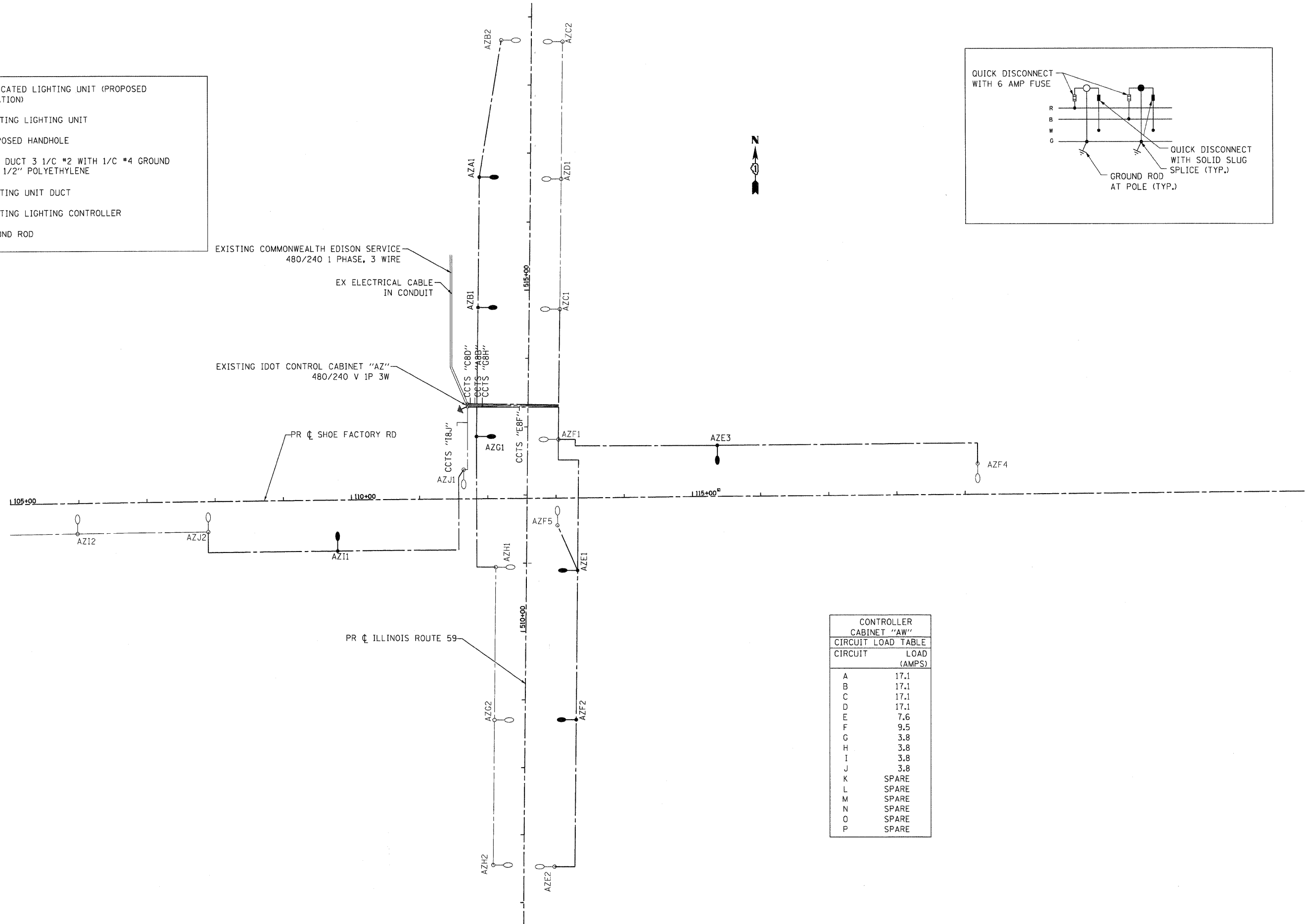
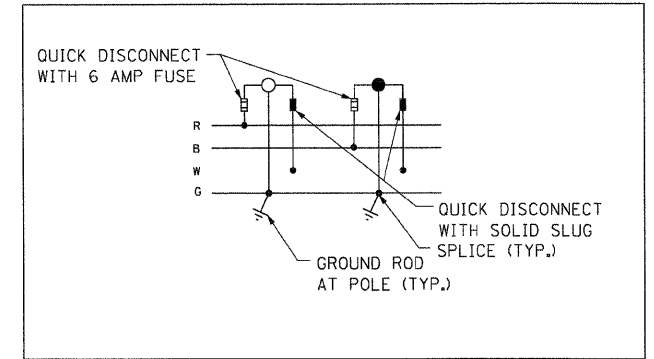
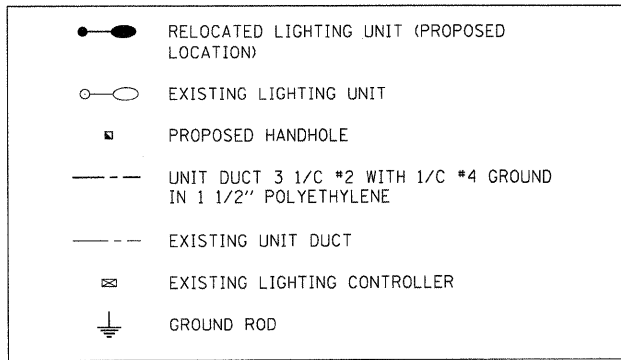
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	PLOT DATE = 4/5/2012	CHECKED - JRM	REVISED -
		DATE - 4/5/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LIGHTING PLANS
GENERAL NOTES, SCHEDULE OF QUANTITIES AND LEGEND

SCALE: N.T.S. SHEET NO. 1 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	35
CONTRACT NO.			63711	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



CONTROLLER CABINET "AW"	
CIRCUIT LOAD TABLE	
CIRCUIT	LOAD (AMPS)
A	17.1
B	17.1
C	17.1
D	17.1
E	7.6
F	9.5
G	3.8
H	3.8
I	3.8
J	3.8
K	SPARE
L	SPARE
M	SPARE
N	SPARE
O	SPARE
P	SPARE

COMPANY NAME: HRGreen
 PROJECT CONTACT: HRGreen.com
 CLIENT: Illinois Professional Design Firm
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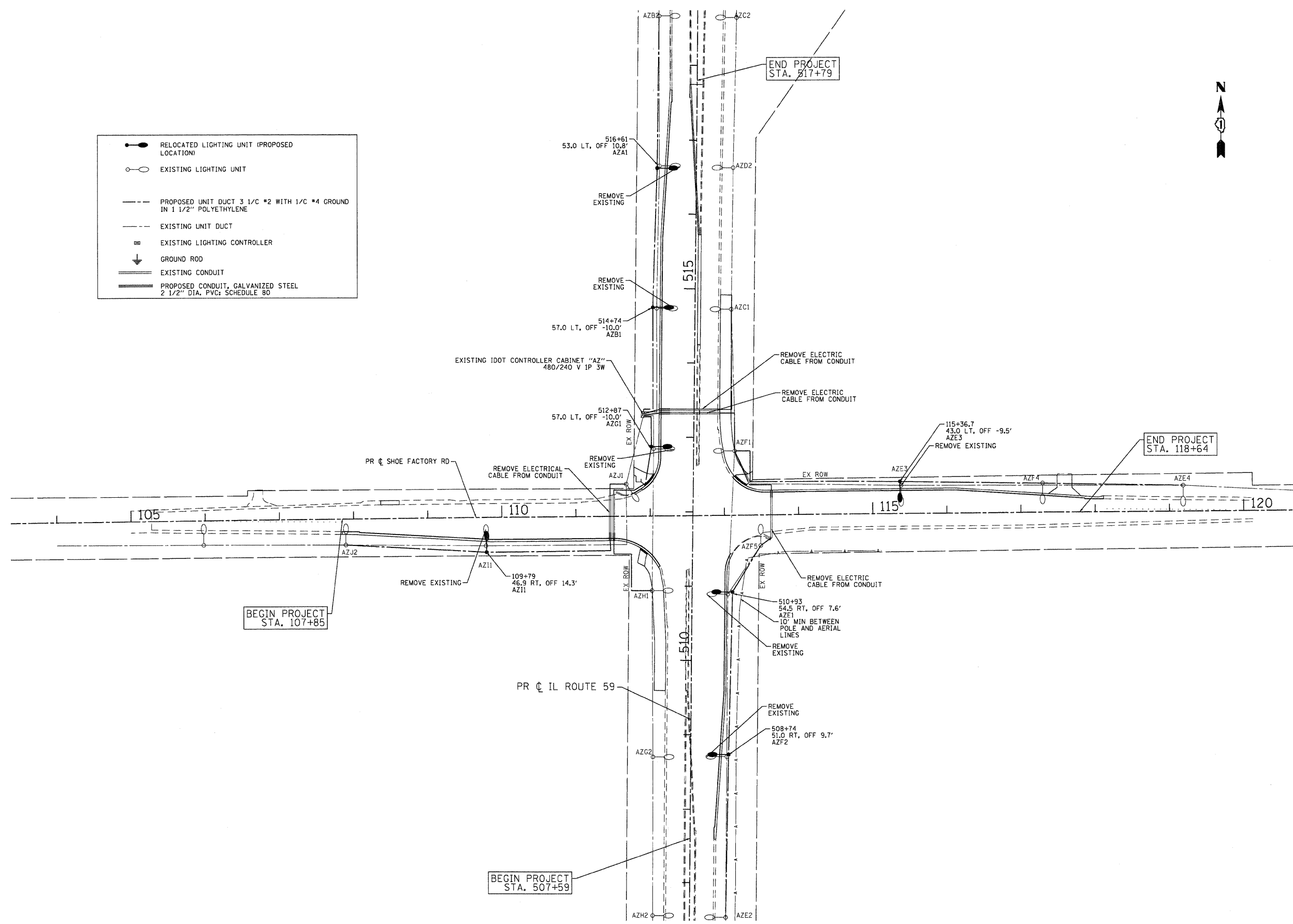
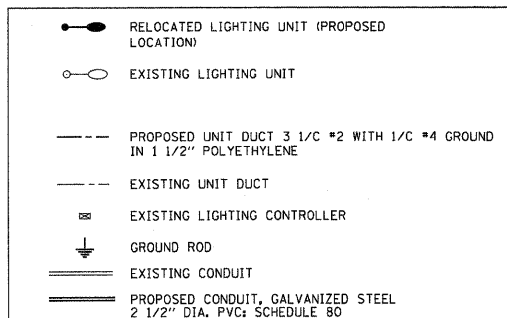


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	DRAWN - JPA	REVISED -
PLOT SCALE = N.T.S.	CHECKED - JRM	REVISED -
PLOT DATE = 4/5/2012	DATE - 4/5/12	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

LIGHTING PLANS
 IL ROUTE 59 AT SHOE FACTORY ROAD
 SCALE: N.T.S. SHEET NO. 2 OF 7 SHEETS STA. TO STA.

F.A.P. RTE. 338	SECTION 10-00084-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 36
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63711	



COMPANY NAME: HRGreen
 PROJECT CONTACT: HRGreen.com
 CLIENT: Illinois Professional Design Firm
 DATE PLOTTED: 4/5/2012 5:03:48 PM
 FILE NAME: 0177-lighting.dgn
 PLOT DATE: 4/5/2012
 PEN TABLE: Standard Tables



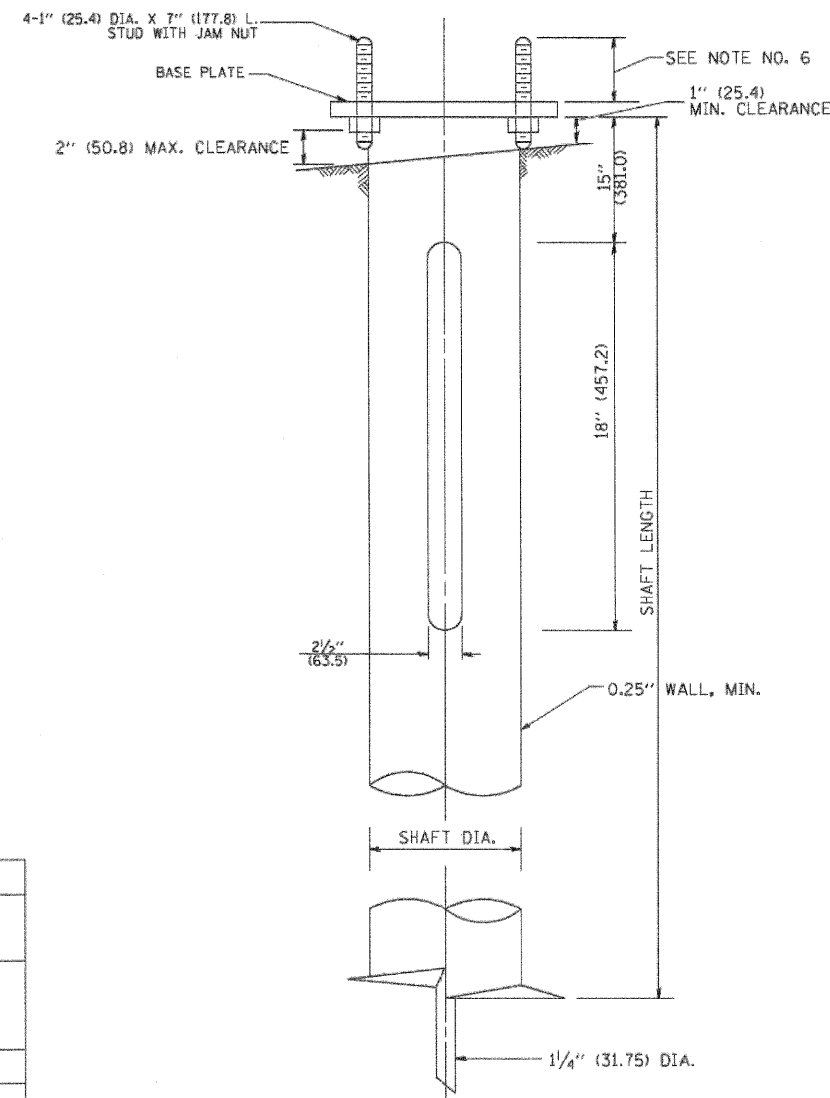
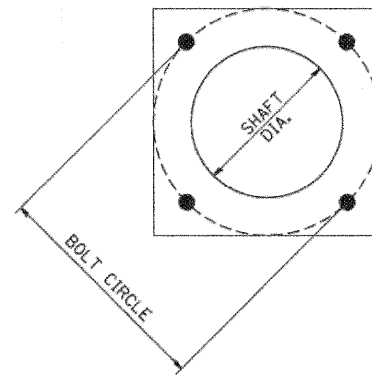
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	DRAWN - JPA	REVISED -
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PLOT DATE = 4/5/2012	DATE - 4/5/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LIGHTING PLANS
IL ROUTE 59 AT SHOE FACTORY ROAD

SCALE: N.T.S. SHEET NO. 3 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	37
CONTRACT NO.			63711	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



HELIX FOUNDATION SIZE

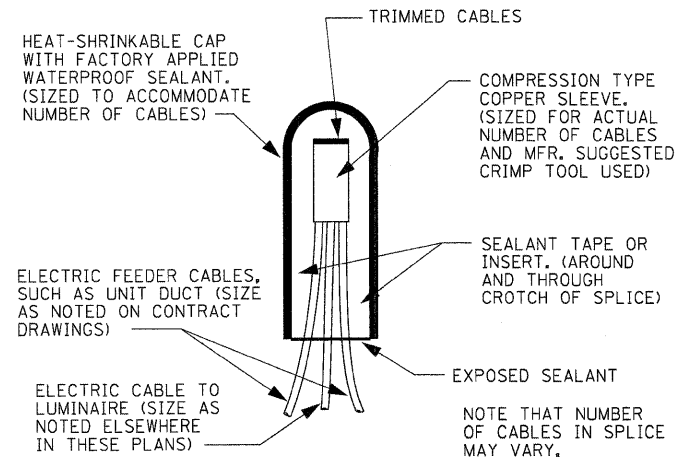
POLE MOUNTING HEIGHT	BOLT CIRCLE	SHAFT DIAMETER	SHAFT LENGTH	BASEPLATE
30 FT.	11 1/2"	8 5/8"	6 FT.	12"x12"x1"
31 FT.-35 FT.	11 1/2"	8 5/8"	6 FT.	12"x12"x1"
36 FT.-40 FT.	15"	8 5/8"	6 FT.	15"x15"x1 1/4"
41 FT.-45 FT.	15"	8 5/8"	6 FT.	15"x15"x1 1/4"
46 FT.-50 FT.	15"	10"	8 FT.	15"x15"x1 1/4"

METAL HELIX FOUNDATION MATERIALS

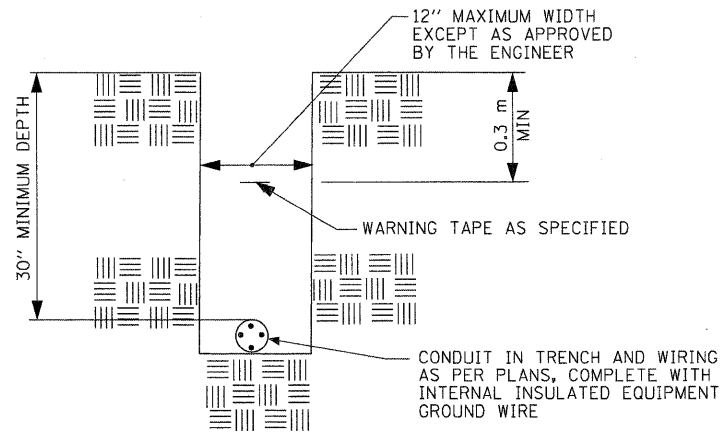
ITEM	MATERIAL REQUIREMENT
BASEPLATE	AASHTO M 270M, GRADE 36 (M270M, GRADE 250)
SHAFT	ASTM A 252, GRADE 2 (PHOSPHOROUS 0.04% MAXIMUM, SULFUR 0.05% MAXIMUM)
HELIX SCREW	AASHTO M 183 (ASTM A 635)
PILOT POINT	AASHTO M 270 (ASTM A 575)
ANCHOR RODS/STUDS	AASHTO M 314 (ASTM F 1554)
HEXAGON NUTS	AASHTO M 291M (ASTM A 563) GRADE DH, OR AASHTO M 292 (ASTM A 194) GRADE 2H
WASHERS	AASHTO M 293 (ASTM F 436)

NOTES:

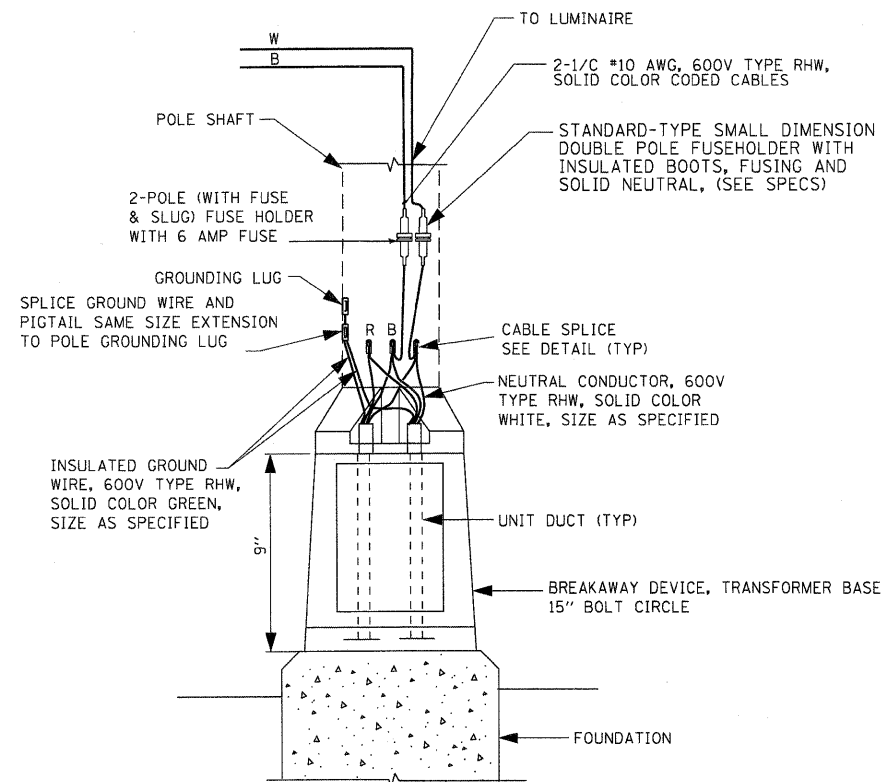
1. ALL DIMENSION IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
2. ALL MATERIAL SHALL BE GALVINIZED ACCORDING TO AASHTO M111, UNLESS OTHERWISE SPECIFIED.
3. ALL WELDS SHALL BE CONTINUOUS AND NOT LESS THAN 1/4" (6.35 mm) FILLET WELDS. THE WELDED FOUNDATION SHALL BE CAPABLE OF WITHSTANDING 10,000 FT/LBS (13558.18 n.m) OF INSTALLATION TORQUE APPLIED ABOUT THE AXIS OF THE FOUNDATION.
4. THE HELIX FOUNDATION SHAFT SHALL BE INSTALLED VERTICAL AND THE BASE PLATE SHALL BE IN LEVEL. THE BREAKAWAY COUPLINGS AND HARDWARE SHALL NOT BE USED TO ALIGN THE POLE INSTALLATION.
5. THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE INSTALLATION OF THE LIGHT POLE.
6. THE CONTRACTOR SHALL COORDINATE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF THE BASE PLATE WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS.
7. ANY VOIDS WITHIN THE METAL FOUNDATION SHALL BE FILLED WITH FINE AGGREGATE.
8. METAL FOUNDATIONS SHALL BE INSTALLED IN UNDISTURBED SOIL. PREDRILLING A PILOT HOLE AND/OR BACKFILLING AROUND THE FOUNDATION IS NOT ALLOWED.
9. THE METAL FOUNDATION SHALL NOT BE INSTALLED TO A TORQUE WHICH EXCEEDS THE MANUFACTURER'S MAXIMUM TORQUE RATING NOR SHALL IT BE INSTALLED TO AN INSTALLATION TORQUE VALUE OF LESS THAN 3,500 FT LB (4,750 KNM). METAL FOUNDATIONS THAT ARE NOT INSTALLED TO FULL INSTALLATION DEPTH OR DO NOT ACHIEVE THE MINIMUM INSTALLATION TORQUE SHALL BE REMOVED AND REPLACED WITH A CONCRETE FOUNDATION AT NO ADDITIONAL COST.
10. THE BASEPLATE SHALL BE PERPENDICULAR TO THE SHAFT AXIS ($\pm 1^\circ$) AND THE HOLE CENTERLINE SHALL BE CONCENTRIC (± 0.188) TO THE SHAFT AXIS.
11. THE PILOT POINT AND SHAFT AXIS SHALL BE CONCENTRIC (± 0.125) AND IN LINE ($\pm 2^\circ$).
12. THE BASEPLATE SHALL BE STAMPED WITH THE MANUFACTURERS NAME AND DATE OF MANUFACTURE.



SPLICING DETAIL
N.T.S.



TYPICAL CONDUIT IN TRENCH DETAIL
N.T.S.



POLE BASE WIRING DIAGRAM
SINGLE MAST ARM
N.T.S.

COMPANY NAME: HRGreen
PROJECT CONTACT: HRGreen.com
CLIENT: Illinois Professional Design Firm
DATE PLOTTED: 4/5/2012 5:45:51 PM
FILE NAME: 017-lighting05.dgn
PLOT ORIGIN: parash
PEN TABLE: standard.trens.tbl



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PLOT SCALE = N.T.S.	DRAWN - JPA	REVISED -
PLOT DATE = 4/5/2012	CHECKED - JRM	REVISED -
	DATE - 4/5/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LIGHTING DETAILS			
IL ROUTE 59 AT SHOE FACTORY ROAD			
SCALE: N.T.S.	SHEET NO. 5 OF 7 SHEETS	STA. TO STA.	

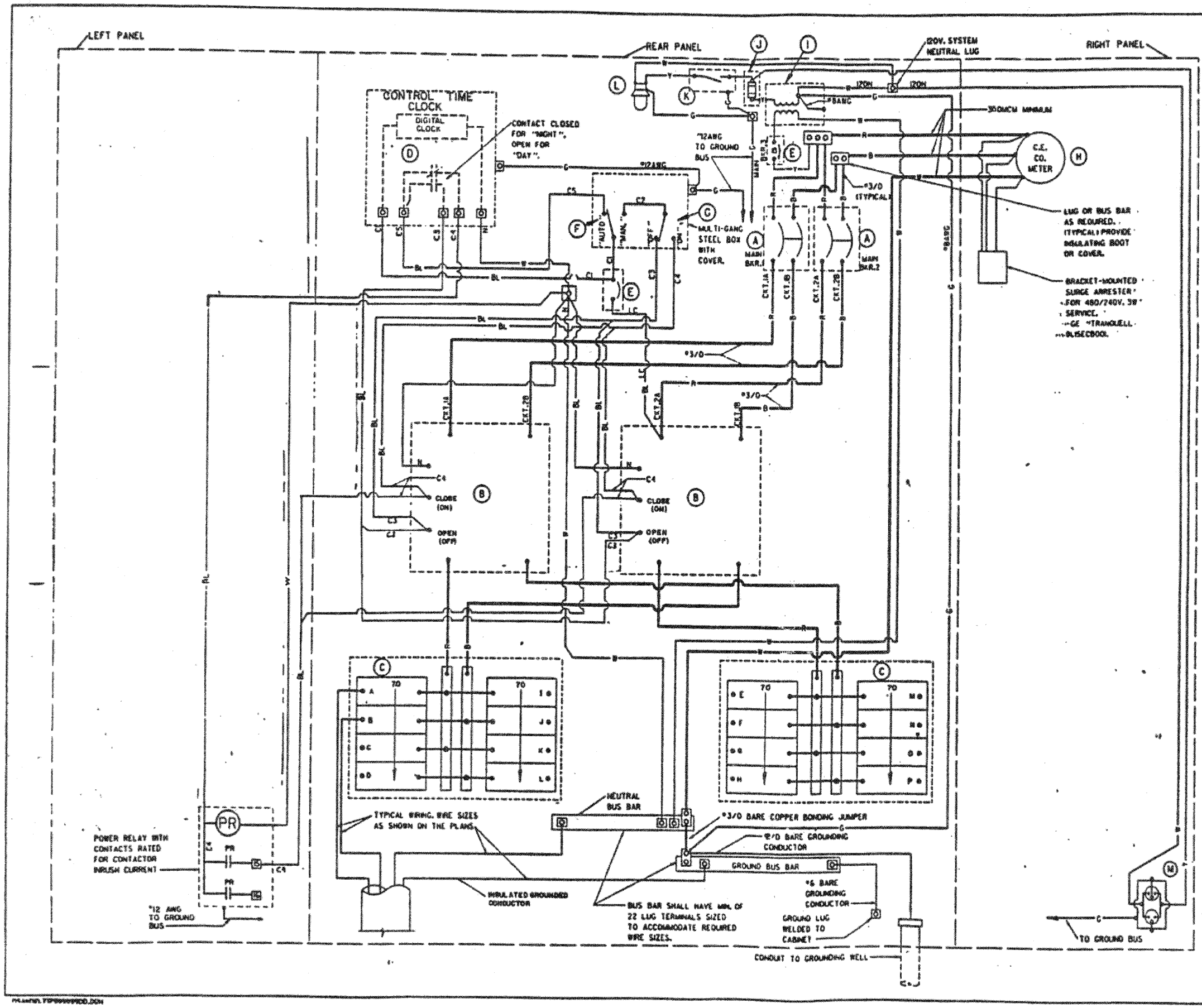
F.A.P. RTE. 338	SECTION 10-00084-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 39
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 63711	

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	40

DEVICE SCHEDULE		
ITEM	QUANT.	DESCRIPTION
A	2	CIRCUIT BREAKER, MOLDED CASE, THERMAL MAGNETIC, 2-POLE, 800V, A.C., 225A FRAME, 25A, NON-INTERCHANGEABLE TRIP, BOLT-ON TYPE, INTERRUPTING CAPACITY OF NOT LESS THAN 22,000 RMS SYMMETRICAL AMPERES AT 480V.
B	2	LIGHTING CONTACTOR (REMOTE CONTROL SWITCH MECHANICALLY HELD, ASCD 870, MOUNTED ON SUB PANEL, 200A, 2P, 600V, WITH 240V. COIL.
C	2	PANELBOARD (INTERIOR) 480/240V, SINGLE PHASE WITH 200A COPPER MAINS AND EIGHT #70A BOLT-ON BRANCH BREAKERS EACH RATED 277V, WITH INTERRUPTING CAPACITY OF NOT LESS THAN 14,000 RMS SYMMETRICAL AMPERES AT 277V.
D	1	DIGITAL ASTRONOMIC TIME CLOCK, 240V, 60 HZ AC, SPDT, 2 CHANNEL, WITH BATTERY BACKUP, NEMA 1 ENCLOSURE
E	2	CIRCUIT BREAKER, MOLDED CASE, THERMAL MAGNETIC, 1-POLE, 277V, BOLT-ON TYPE WITH AN INTERRUPTING RATING OF NOT LESS THAN 14,000 RMS SYMMETRICAL AMPERES AT 277V.
F	1	CONTROL SWITCH, MOMENTARY CONTACT, SPDT, 6A, 240V.
G	1	CONTROL SWITCH, TOGGLE TYPE, SPDT, 6A, 240V, PREMIUM SPEC. GRADE.
H	1	SOCKET FOR ELECTRIC UTILITY CO. METER.
I	1	STEP DOWN TRANSFORMER 240V-120V, 1KVA
J	2	FUSE HOLDER 15A, 250V, SA FUSE
K	1	MICRO SWITCH MOUNT WITH ACTUATOR TO SWITCH WHEN DOOR OPENED
L	1	60 WATT LIGHT FIXTURE, VAPORTIGHT, WITH GLOBE AND GUARD AND MOUNTING BOX.
M	1	RECEPTACLE, 120V, 6A, PREMIUM SPEC. GRADE, NEMA REFERENCE S-5R IN WEATHER-PROOF BOX WITH FLAP-TYPE COVER.

- NOTES:**
- ALL CONTROL CABINET ITEMS SHALL HAVE SUITABLE IDENTIFICATION, OPEN CIRCUIT BREAKERS, CONTACTORS AND OTHER OPEN DEVICES SHALL HAVE PERMANENT SELF-STOPPING TAGS. DEVICES IN ENCLOSURES SHALL HAVE ENGRAVED 2-COLOR LAMINATED PLASTIC NAMEPLATES ATTACHED TO ENCLOSURES WITH SCREWS. NAMEPLATES SHALL BE ENGRAVED TO CORRESPOND TO DESIGNATIONS ON THE DRAWING. INTERNAL CABINET WIRING SHALL BE IDENTIFIED AS INDICATED OR AS DIRECTED BY THE ENGINEER BY MEANS OF SELF-STOPPING TAGS APPLIED AT EACH CONNECTED END. IDENTIFICATION SHALL BE MADE BY THE CABINET MANUFACTURER.
 - ALL WIRING WITHIN THE CABINET SHALL BE COLOR CODED AS INDICATED.
 R = RED
 B = BLACK
 BL = BLUE
 Y = YELLOW
 W = WHITE
 G = GREEN
 - PROVIDE SEALING GROMMETS FOR ALL OPEN WIRING EXTENDING FROM DEVICES IN BOXES OR CABINETS WITHIN THE CONTROL CABINET.
 - ALL 120 VOLT SYSTEM AND ALL CONTROL WIRING SHALL BE #12AWG STRANDED UNLESS OTHERWISE INDICATED.
 - ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

Carl R. Bratkov
 10/27/90
 Elect. Drawg
 REVISION 11/22/90



REVISIONS	
NAME	DATE
G. BRATKIV	6-30-85
G. BRATKIV	10-31-88

ILLINOIS DEPARTMENT OF TRANSPORTATION
 IL RTE. 59 AND IL RTE. 72
 CONTROL CABINET, CONSOLE TYPE
 PANEL LAYOUT
 SCALE: NONE
 DATE 6/20/85
 DRAWN BY J6
 CHECKED BY

4/5/2012 5:46:55 PM
 017-lighting.dwg
 G:\projects\10-00084-00-CH
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USER NAME = jme1hu
 DESIGNED - PK
 DRAWN - JPA
 CHECKED - JRM
 DATE - 4/5/12

REVISIONS
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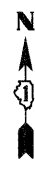
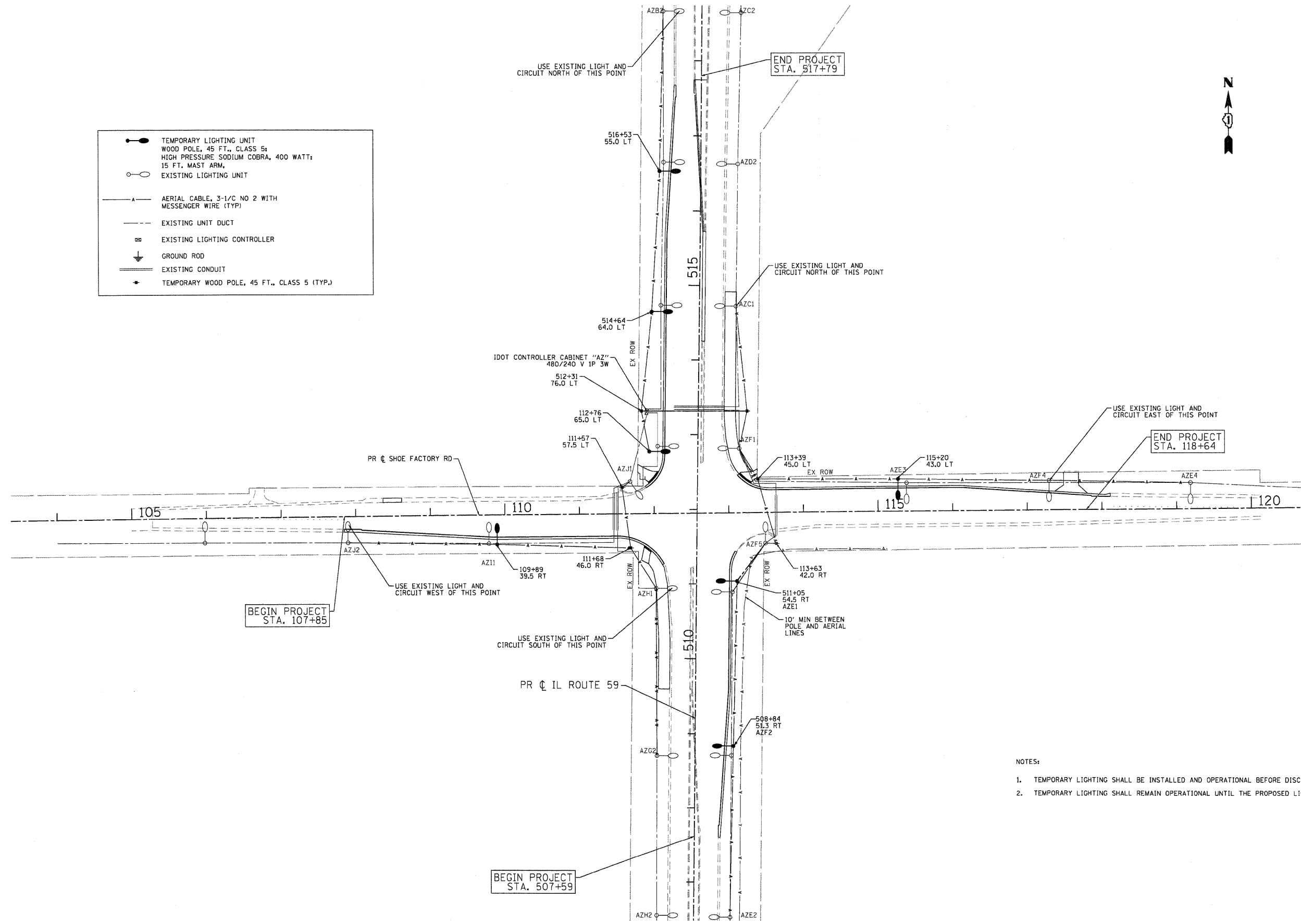
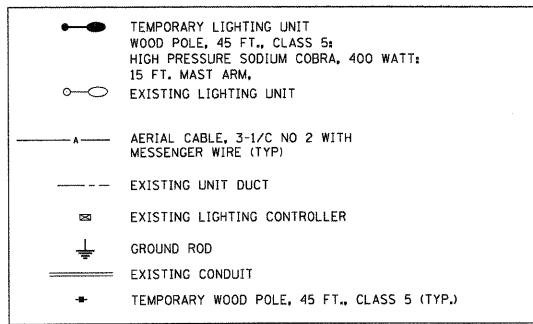
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CONTROL CABINET, CONSOLE TYPE
 IL ROUTE 59 AT SHOE FACTORY ROAD

SCALE: N.T.S. SHEET NO. 6 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	40
CONTRACT NO.			63711	

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



NOTES:

- TEMPORARY LIGHTING SHALL BE INSTALLED AND OPERATIONAL BEFORE DISCONNECTING AND REMOVING EXISTING LIGHTING UNITS.
- TEMPORARY LIGHTING SHALL REMAIN OPERATIONAL UNTIL THE PROPOSED LIGHTING SYSTEM IS INSTALLED AND OPERATIONAL.

COMPANY NAME: HRGreen
 PROJECT CONTACT: jme@hrgreen.com
 CLIENT: HRGreen
 DATE PLOTTED: 4/5/2012 5:05:38 PM
 FILE NAME: 1017-lighting06.dgn
 PLOT SHEETS: 63
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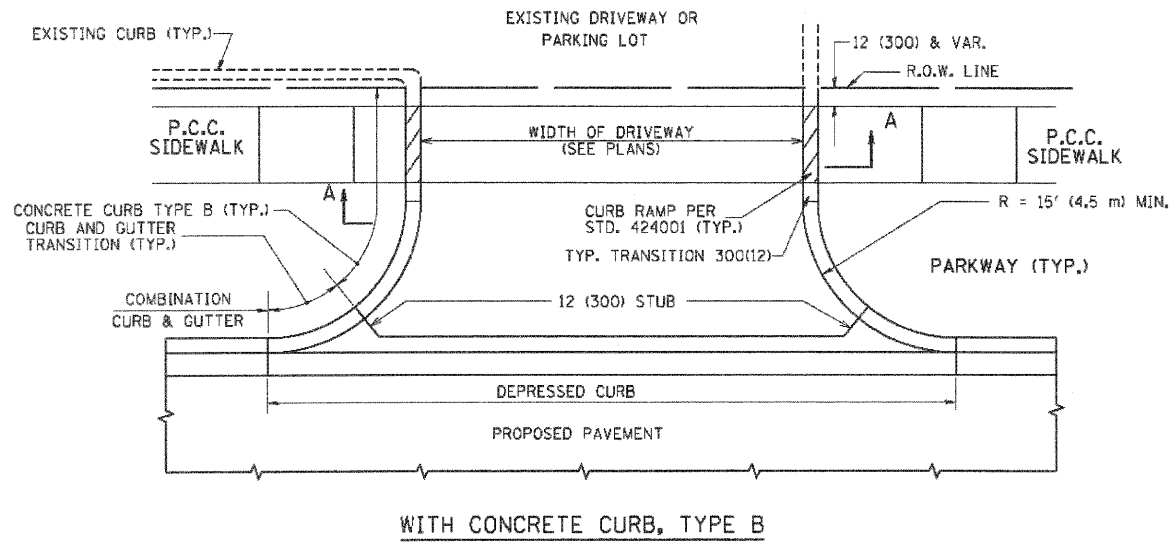


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PLOT DATE = 4/5/2012	DATE - 4/5/12	REVISED -

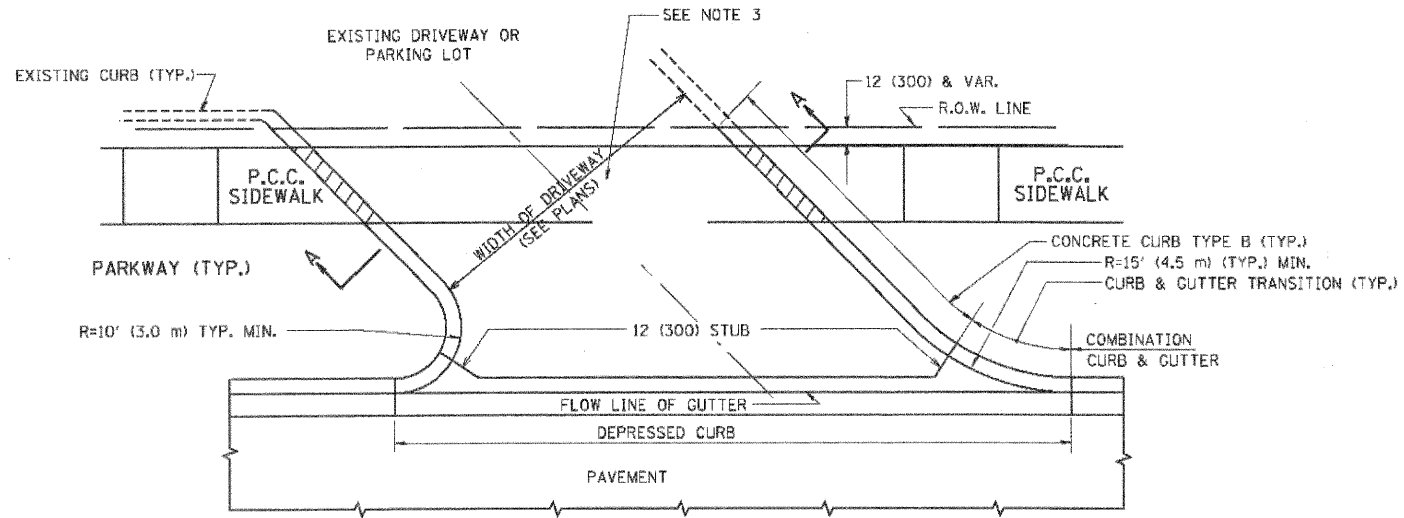
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

TEMPORARY LIGHTING PLANS	
IL ROUTE 59 AT SHOE FACTORY ROAD	
SCALE:	TO STA.
SHEET NO. 7 OF 7 SHEETS	STA. TO STA.

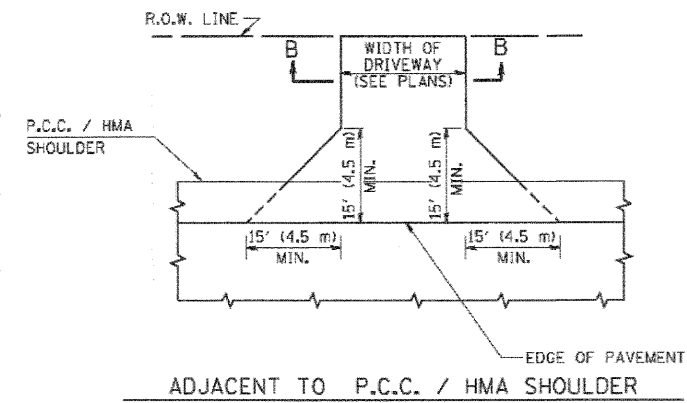
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 63711			ILLINOIS FED. AID PROJECT	



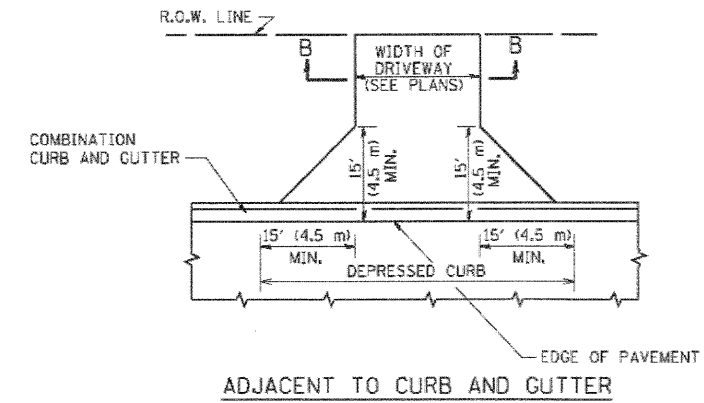
WITH CONCRETE CURB, TYPE B



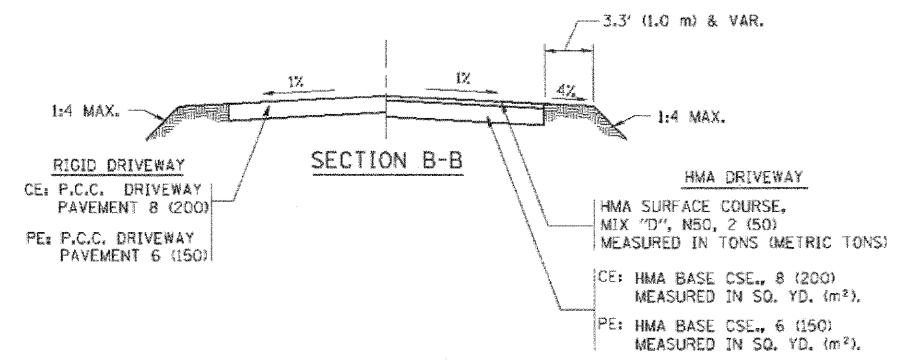
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



SECTION B-B

RURAL FIELD ENTRANCE (FE)

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

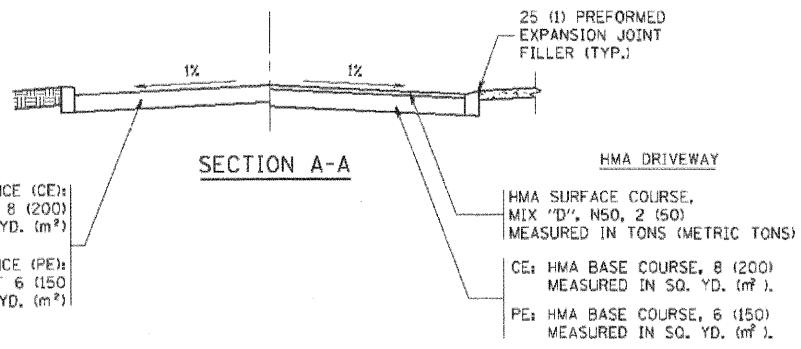
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



SECTION A-A

RIGID DRIVEWAY
 COMMERCIAL ENTRANCE (CE):
 P.C.C. DRIVEWAY PAVEMENT 8 (200)
 MEASURED IN SQ. YD. (m²)
 NON-COMMERCIAL ENTRANCE (PE):
 P.C.C. DRIVEWAY PAVEMENT 6 (150)
 MEASURED IN SQ. YD. (m²)

HMA DRIVEWAY
 HMA SURFACE COURSE,
 MIX "D", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 CE: HMA BASE COURSE, 8 (200)
 MEASURED IN SQ. YD. (m²).
 PE: HMA BASE COURSE, 6 (150)
 MEASURED IN SQ. YD. (m²).

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 Standard Trans. B1

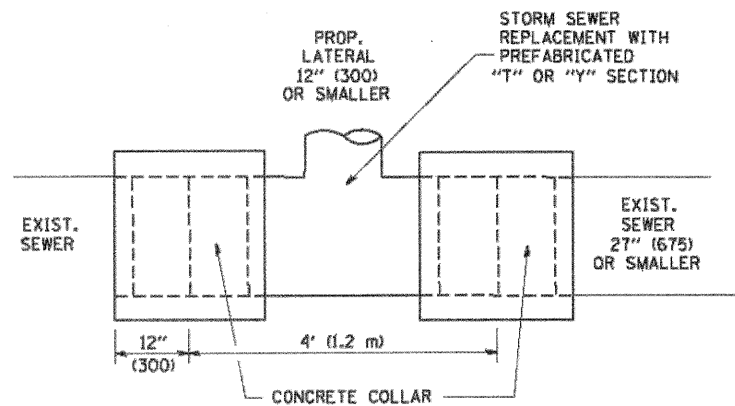


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	DRAWN: -	REVISED: R. BORO 01-01-07
PLOT SCALE: 50,000.00 / 1" = 100'	CHECKED: -	REVISED: R. BORO 06-11-08
PLOT DATE: 4/5/2012	DATE: 4/5/12	REVISED: R. BORO 09-06-11

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

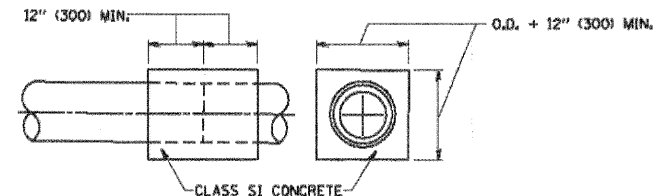
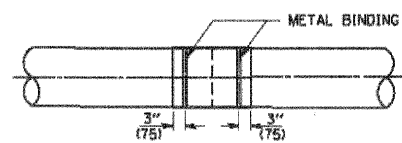
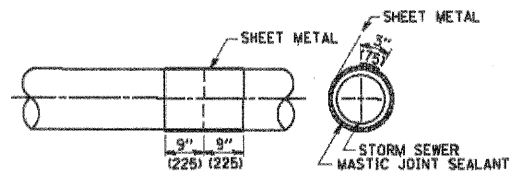
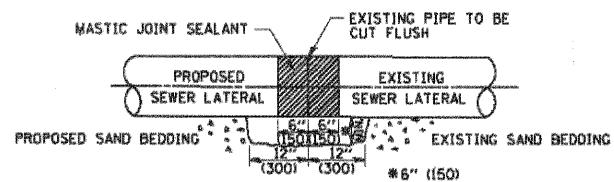
DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER ≥ 15' (4.5 m)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	42
BD0156-07 (BD-01)			CONTRACT NO. 63711	
RD, ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

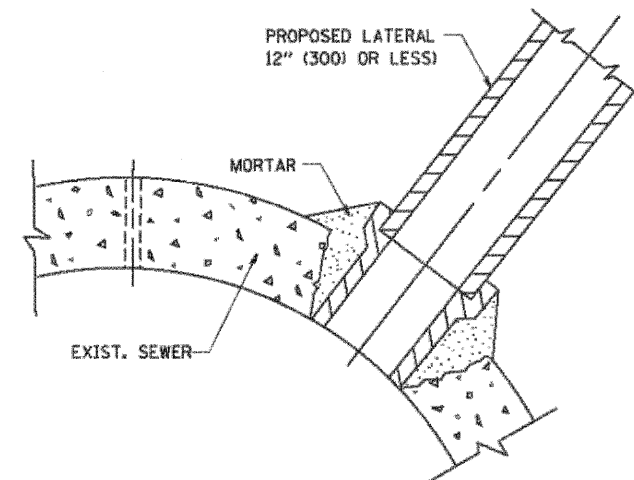


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

COMPANY NAME: HRGreen
 PROJECT CONTACT: HRGreen.com
 CLIENT: Illinois Professional Design Firm
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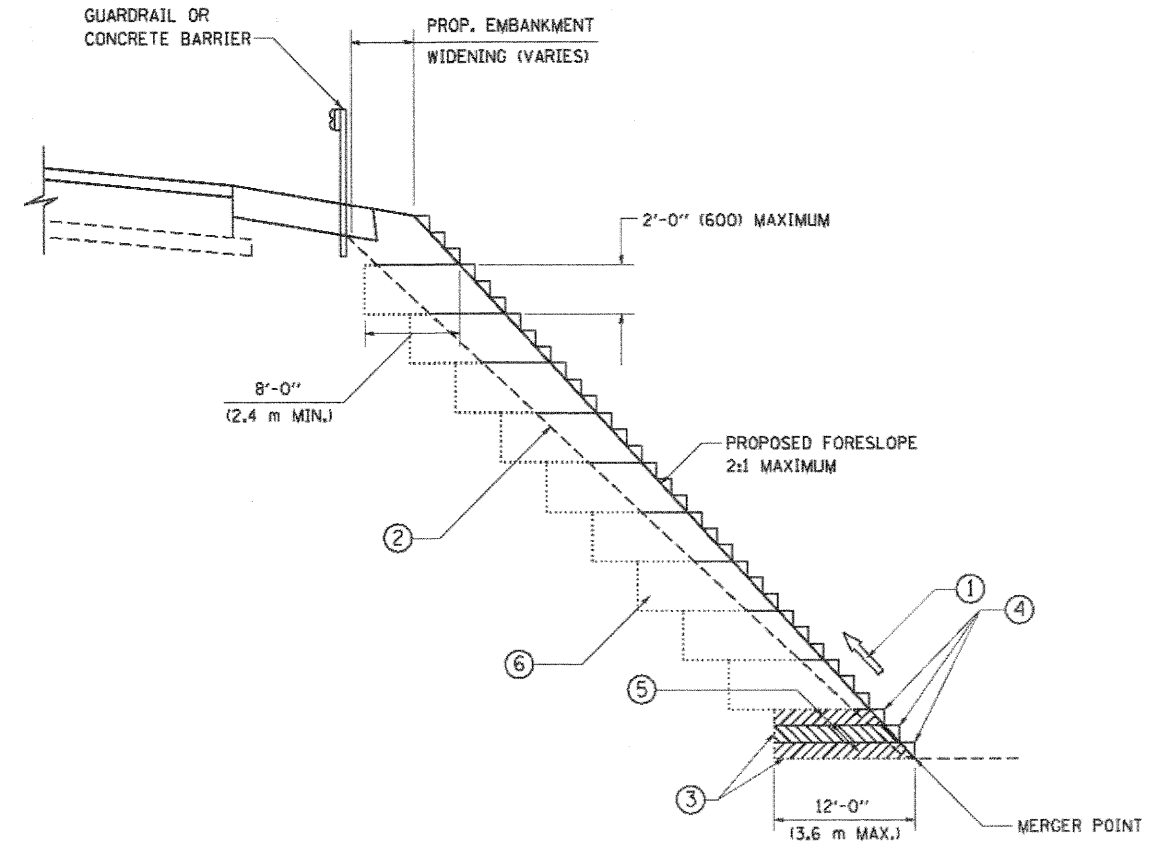


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	DRAWN -	REVISED - R. SHAH 09-09-94
PLOT SCALE = 1/8" = 1'-0"	CHECKED -	REVISED - R. SHAH 10-25-94
PLOT DATE = 5/6/2012	DATE = 6/7/2010	REVISED - R. SHAH 06-12-96

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	43
BD500-01 (BD-7)			CONTRACT NO. 63711	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TYPICAL BENCHING DETAIL
FOR EMBANKMENT

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

COMPANY NAME: HRGreen
PROJECT CONTACT: HRGreen.com
CLIENT: 4/5/2012 5:16:14 PM
DATE PLOTTED: 01/17/2017 07:56pm
FILE NAME: 10-00084-00-CH
PLOT USER: srb
PLOT DATE: 4/5/2012 5:16:14 PM



USER NAME = gpe1@hr.com	DESIGNED =	REVISED =
PLOT SCALE = 50.0000 / IN.	DRAWN = CADD	REVISED =
PLOT DATE = 4/5/2012	CHECKED = S.E.B.	REVISED =
	DATE = 06-16-04	REVISED =

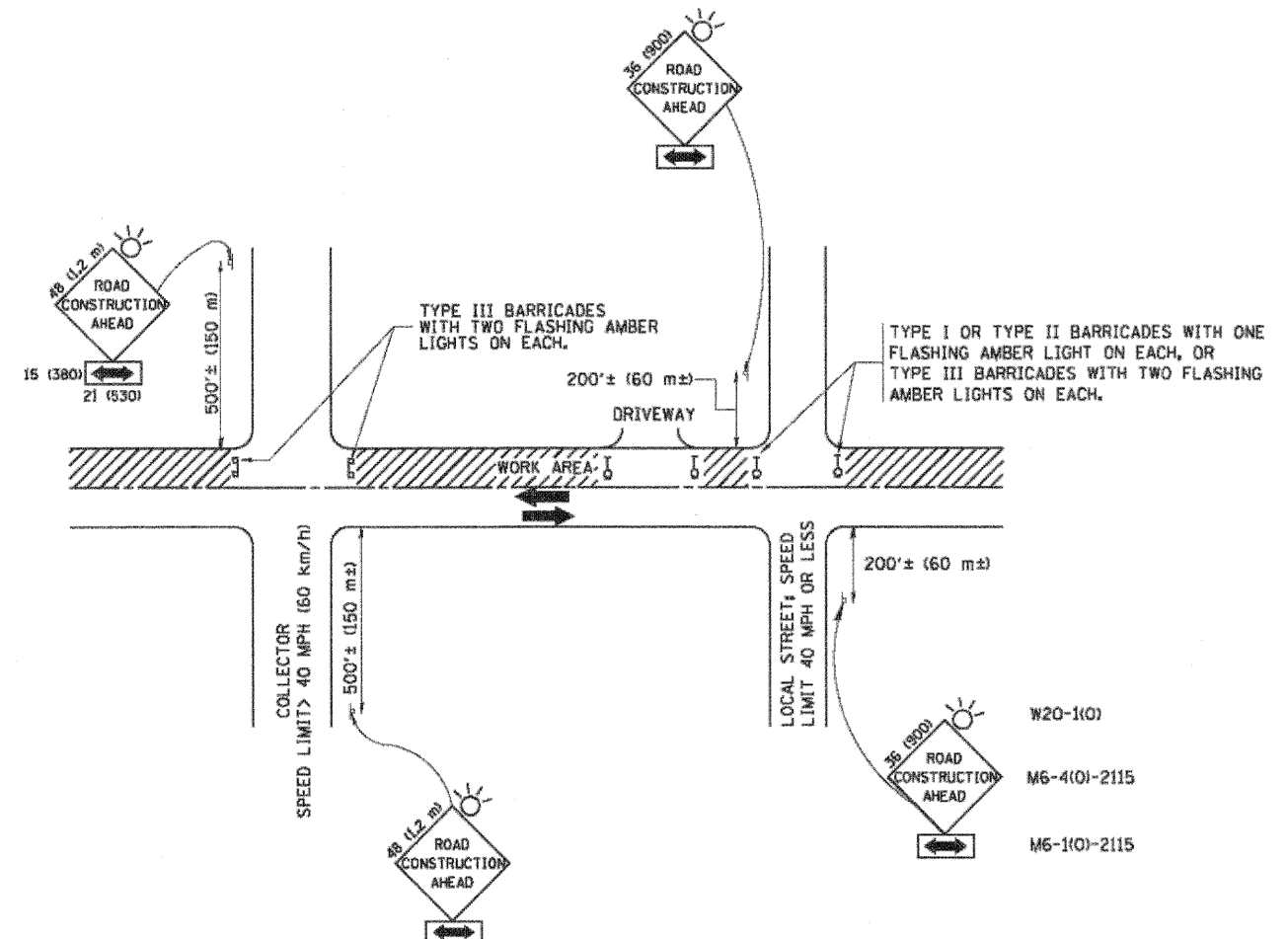
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BENCHING DETAIL
FOR EMBANKMENT WIDENING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	44
	BD-51			CONTRACT NO. 63711

RFD. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

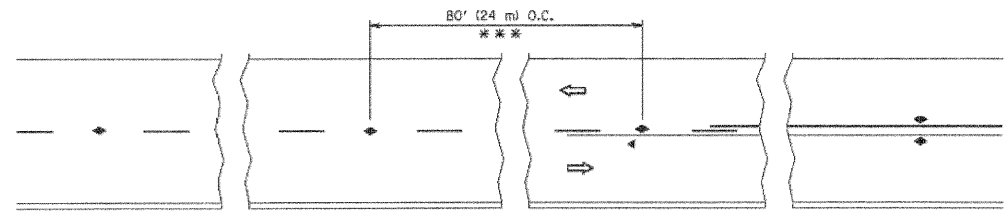
COMPANY NAME: HRGreen.com
 PROJECT CONTACT: gael@hsnet
 CLIENT: 4/5/2012 5:06:30 PM
 DATE PLOTTED: 07/17/2012 10:06:09
 FILE NAME: 10-00084-00-CH
 PLOT USER: gael@hsnet
 PEN TABLE: Standard.ctb

	USER NAME = gael@hsnet	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
	PLOT SCALE = 1/8" = 1'-0"	DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT DATE = 4/5/2012	CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 07/12/12	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

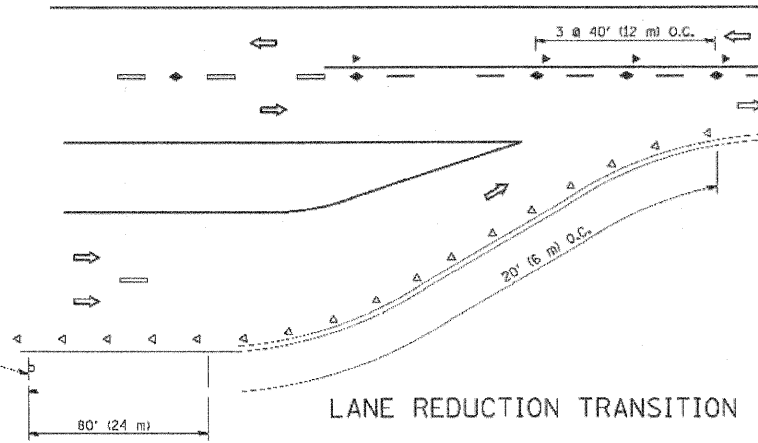
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE. 338	SECTION 10-00084-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 45
TC-10			CONTRACT NO. 63711	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

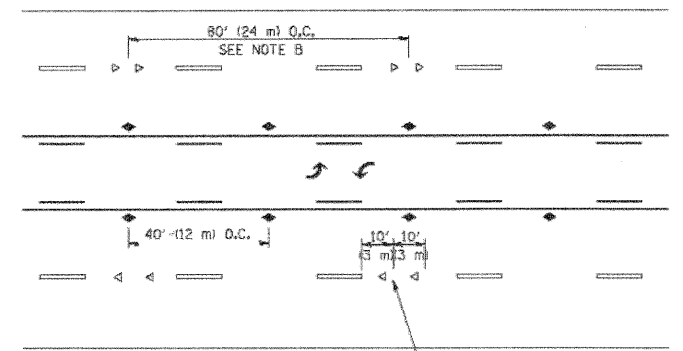


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

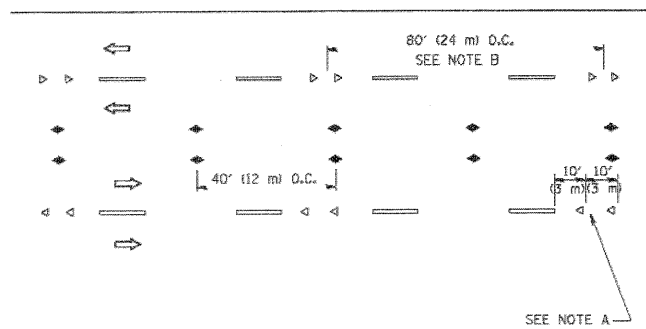
TWO-LANE/TWO-WAY



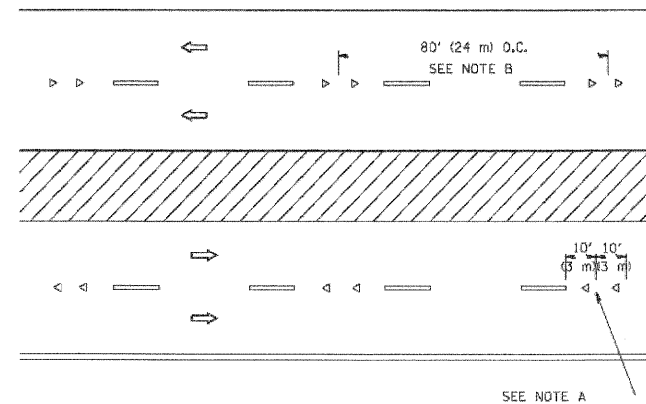
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

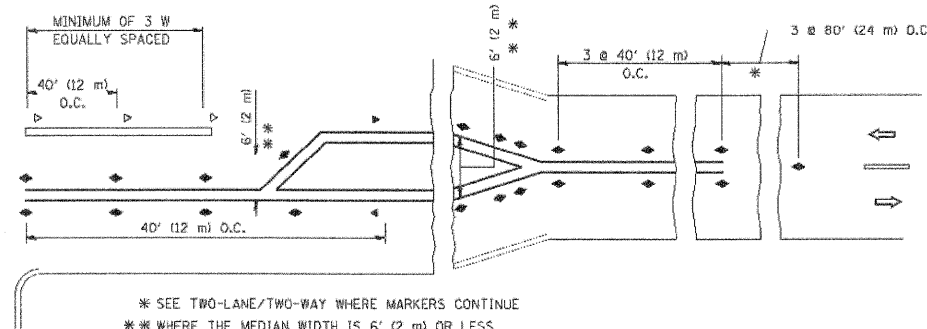
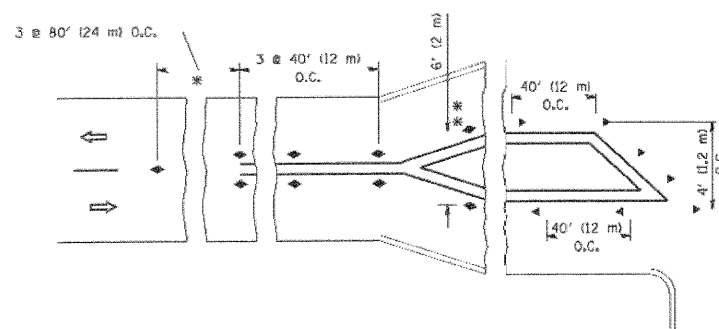
- YELLOW STRIPE
- WHITE STRIPE
- ▲ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

COMPANY NAME: HRGreen
 PROJECT CONTACT: HRGreen
 CLIENT: HRGreen
 DATE PLOTTED: 4/5/2012 5:06:46 PM
 FILE NAME: 017-081-05.dgn
 PLOT DATE: 4/5/2012

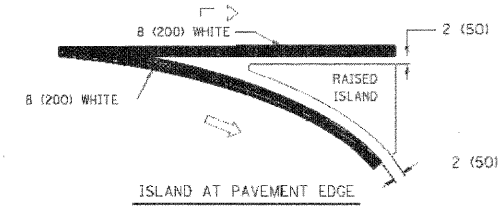
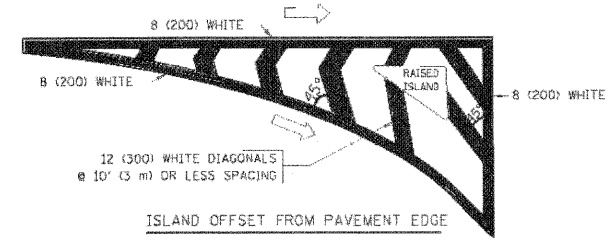
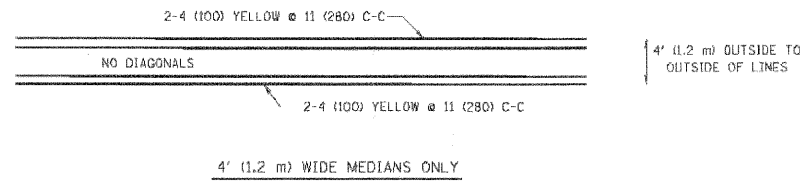
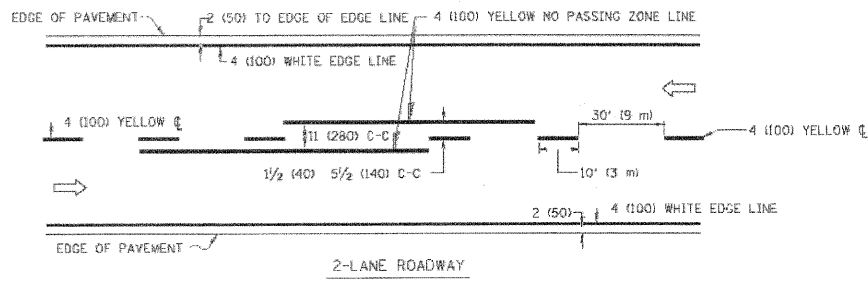


USER NAME =	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
	DRAWN -	REVISED - T. RAMMACHER 03-12-99
PLOT SCALE = 1/8" = 1'-0"	CHECKED -	REVISED - T. RAMMACHER 01-06-00
PLOT DATE = 4/5/2012	DATE - 4/5/12	REVISED - C. JUCLUS 09-09-09

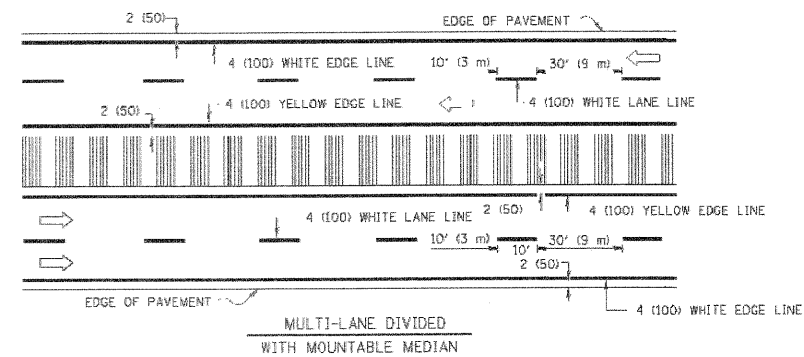
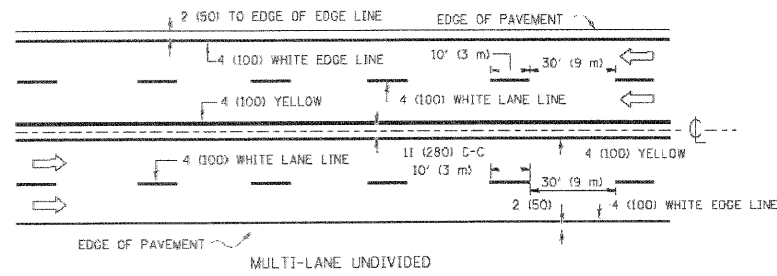
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS	
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	46
TC-11			CONTRACT NO. 63711	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

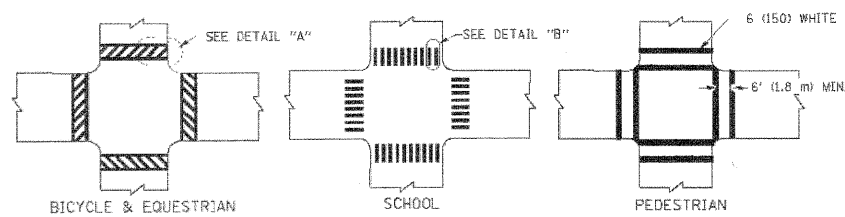


TYPICAL ISLAND MARKING

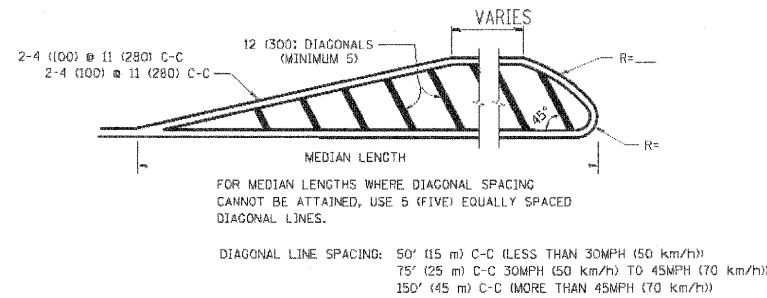


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

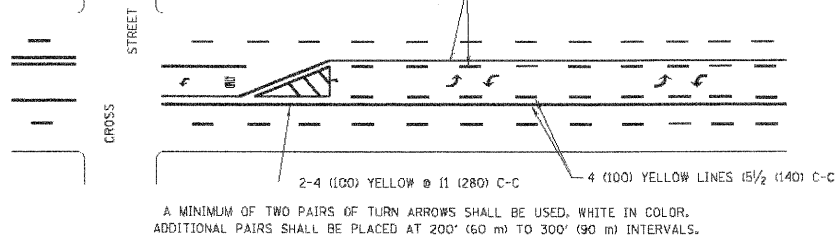
TYPICAL LANE AND EDGE LINE MARKING



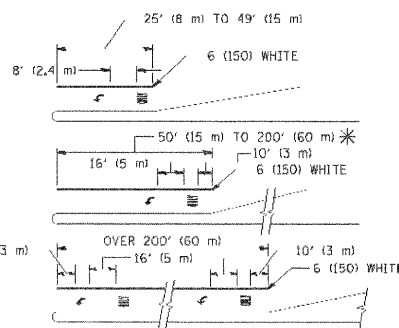
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
* AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 6 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE, FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL))	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

COMPANY NAME: HRGreen
PROJECT CONTACT: HRGreen
CLIENT: HRGreen
DATE PLOTTED: 4/5/2012 5:17:02 PM
FILE NAME: 017-cen-10.dgn
PLOT USER: jma
PEN TABLE: Standard-Trans.tbl



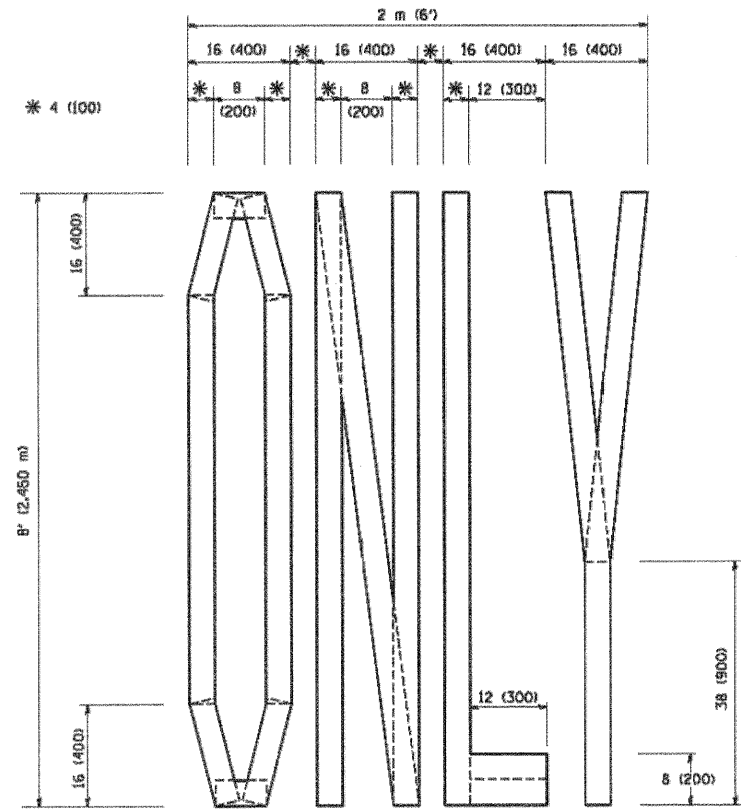
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DESIGNED - EVERS
DRAWN -
CHECKED -
DATE - 4/5/2012

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REVISED - T. RAMMACHER 10-27-94
DRAWN -
REVISED - C. JUCIUS 09-09-09
CHECKED -
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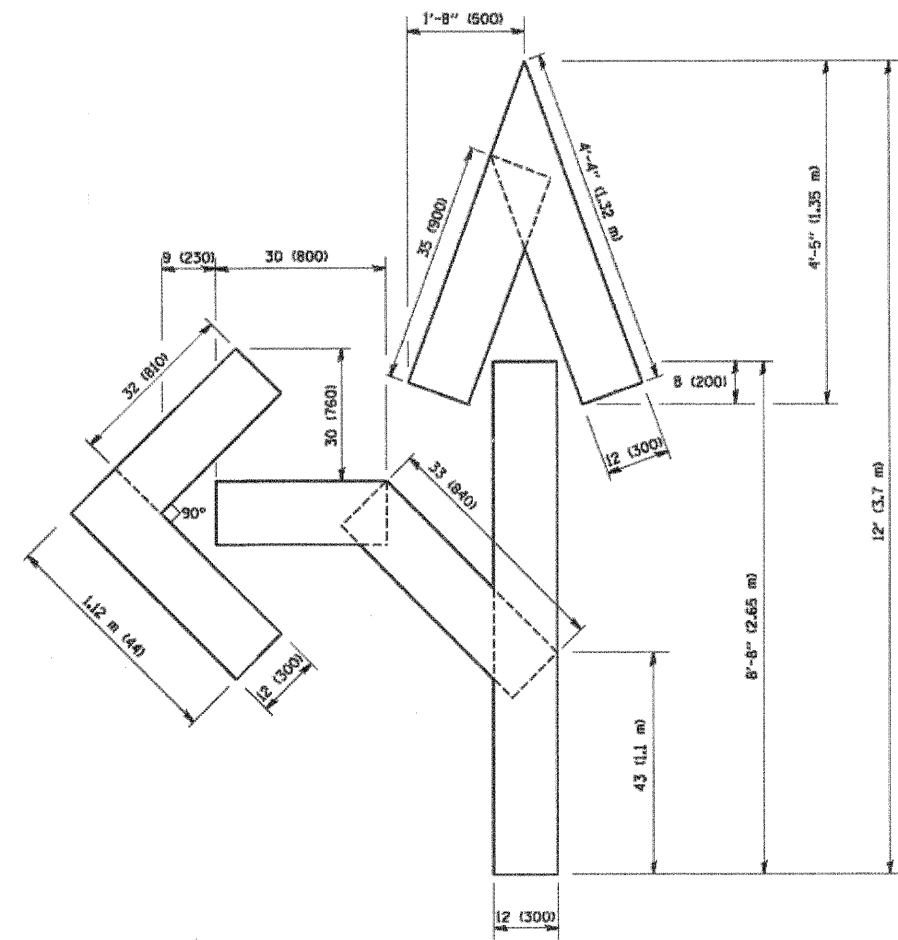
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

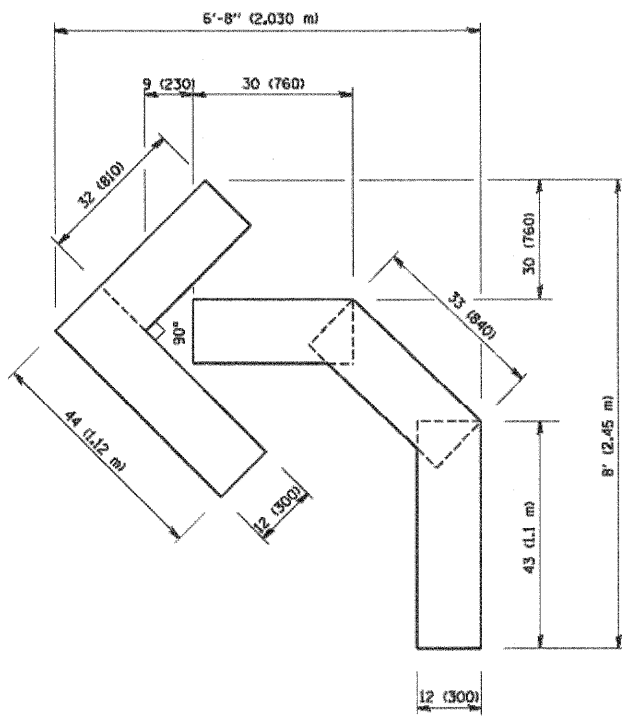
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	47
TC-13		CONTRACT NO. 63711		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: Illinois Professional Design Firm
 CLIENT: #184-001322
 DATE PLOTTED: 4/5/2012 5:17:38 PM
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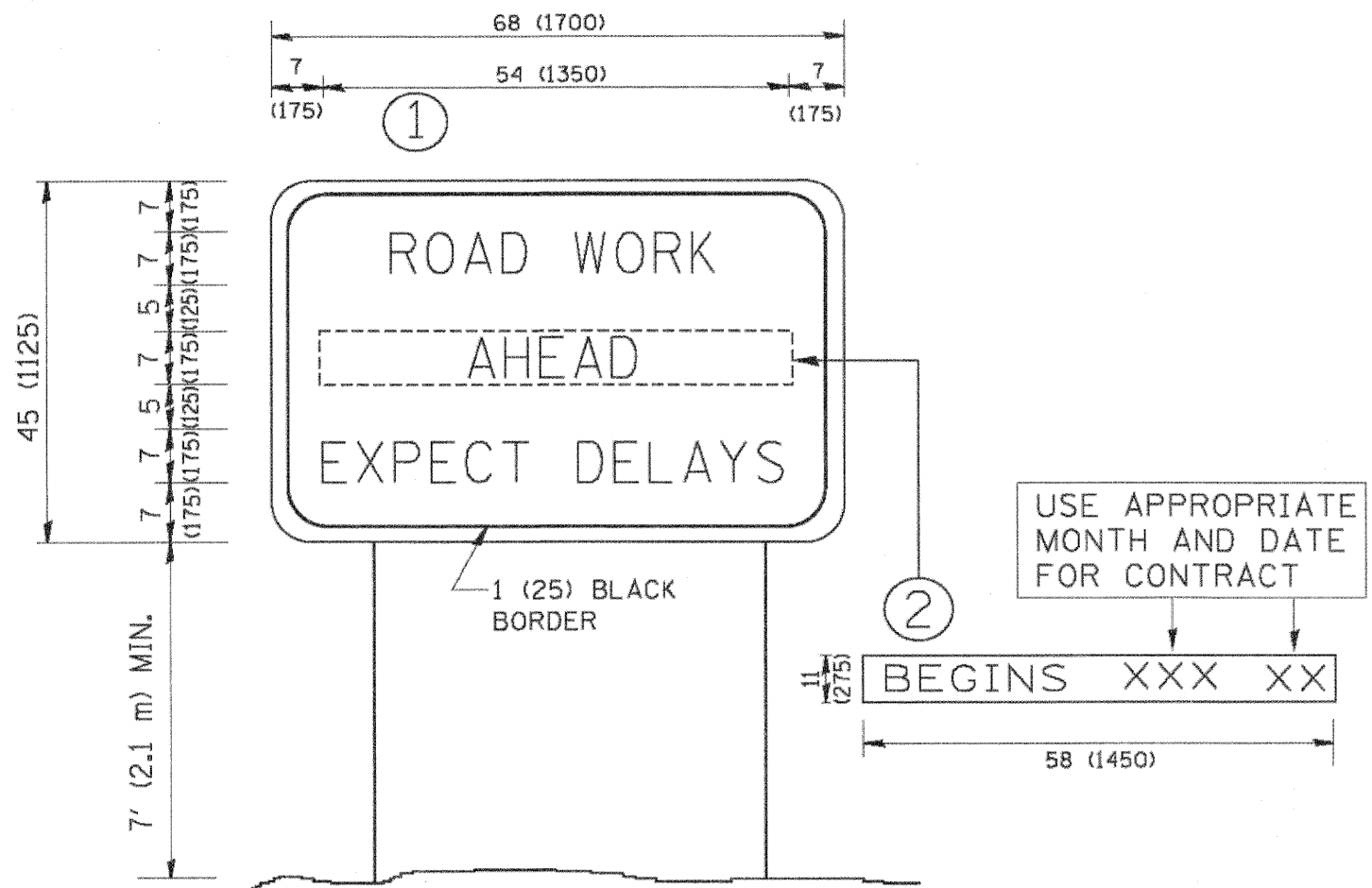


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	DRAWN -	REVISED -T. RAMMACHER 11-04-97
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PLOT DATE = 5/5/2012	DATE = 09-28-1994	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			
SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	48
TC-16			CONTRACT NO. 63711	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

COMPANY NAME: HRGreen
 PROJECT CONTACT: HRGreen.com
 CLIENT: Illinois Professional Design Firm
 DATE PLOTTED: 4/5/2012 5:17:54 PM
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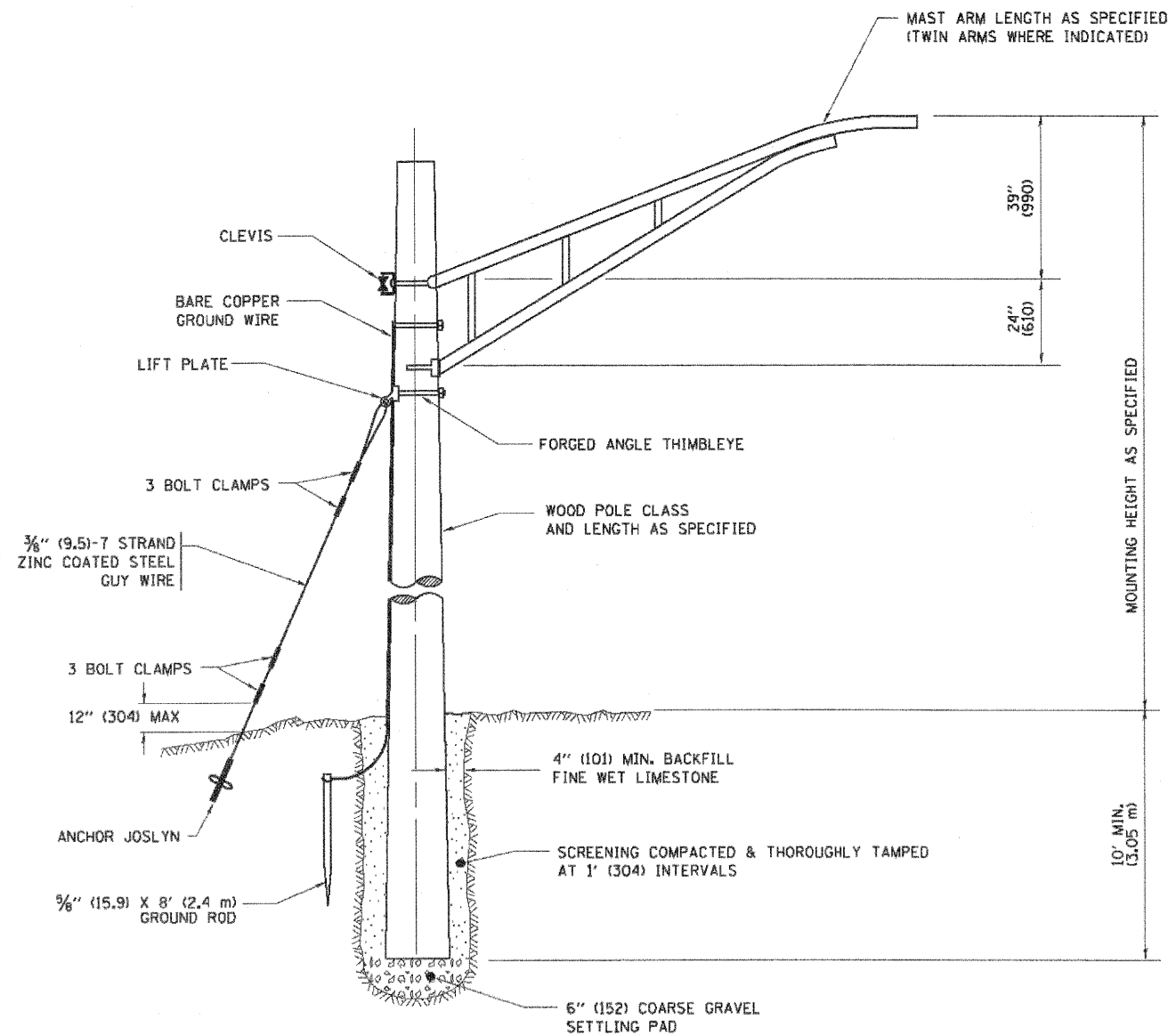

 HRGreen.com
 Illinois Professional Design Firm
 #184-001322

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	DRAWN -	REVISED - R. MIRS 12-11-97
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PLOT DATE = 1/16/2008	DATE - 4/5/12	REVISED - C. JUCIUS 01-31-07

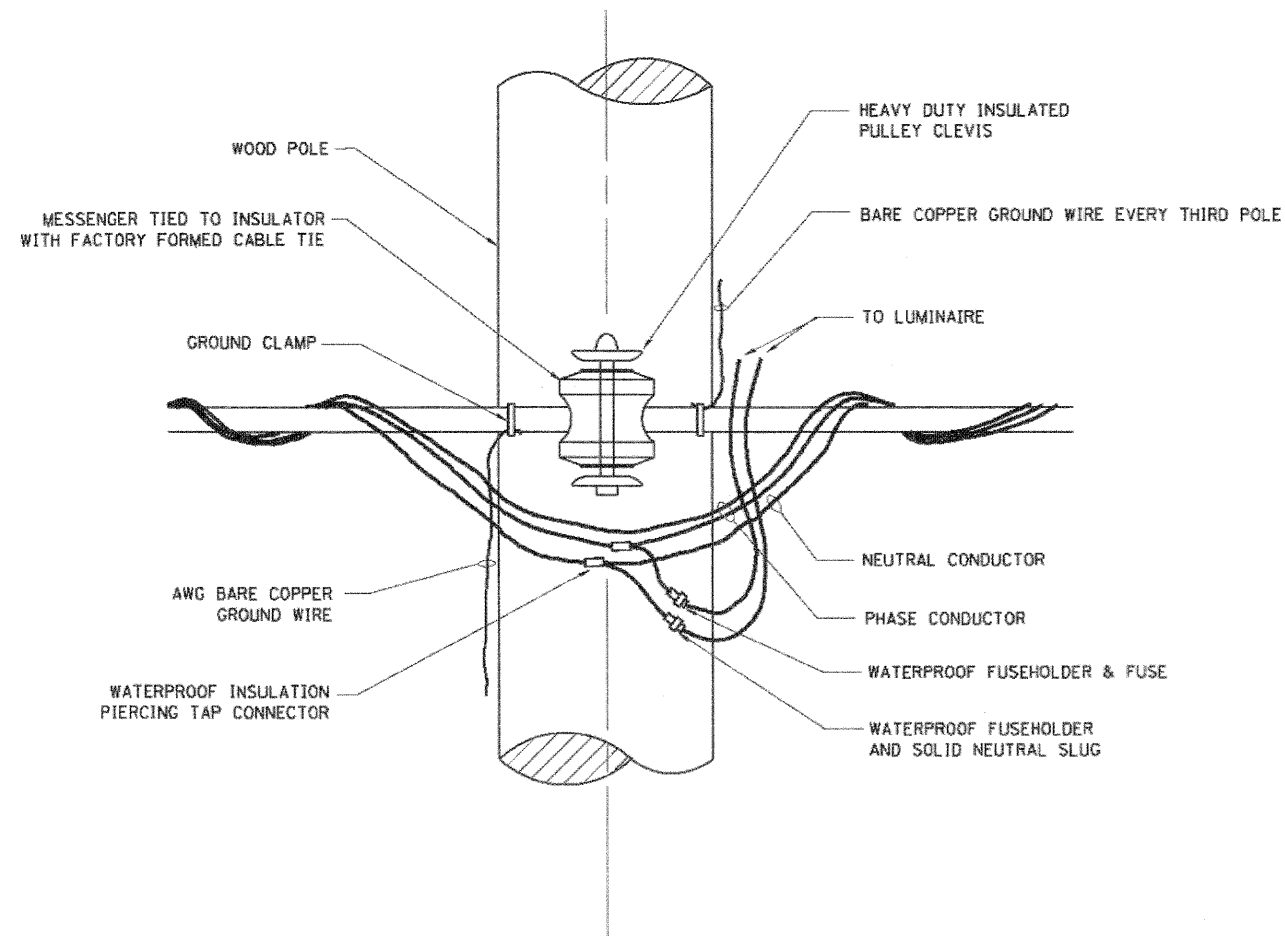
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ARTERIAL ROAD INFORMATION SIGN	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-0084-00-CH	COOK	63	49
TC-22			CONTRACT NO. 63711	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TEMPORARY LIGHT POLE DETAIL



TEMPORARY LIGHT POLE ATTACHMENT DETAIL

NOTES:

1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: greg@hrgreen.com
 CLIENT: HRGreen.com
 DATE PLOTTED: 4/5/2012 5:17:50 PM
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 PLOT DATE: 4/5/2012



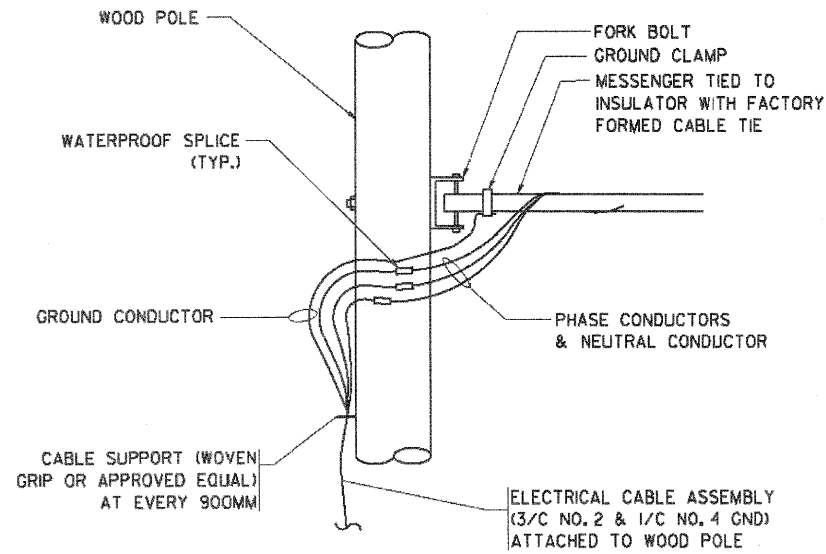
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

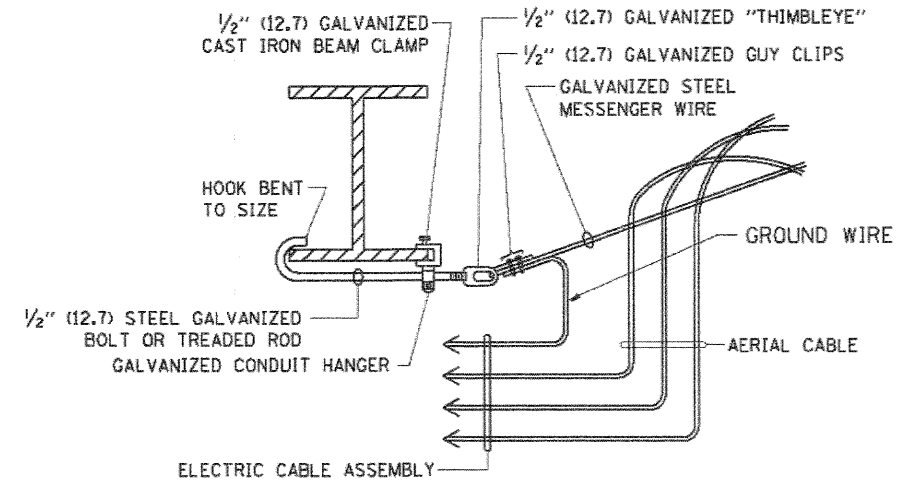
TEMPORARY LIGHT POLE DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	50
BE-800		CONTRACT NO. 63711		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



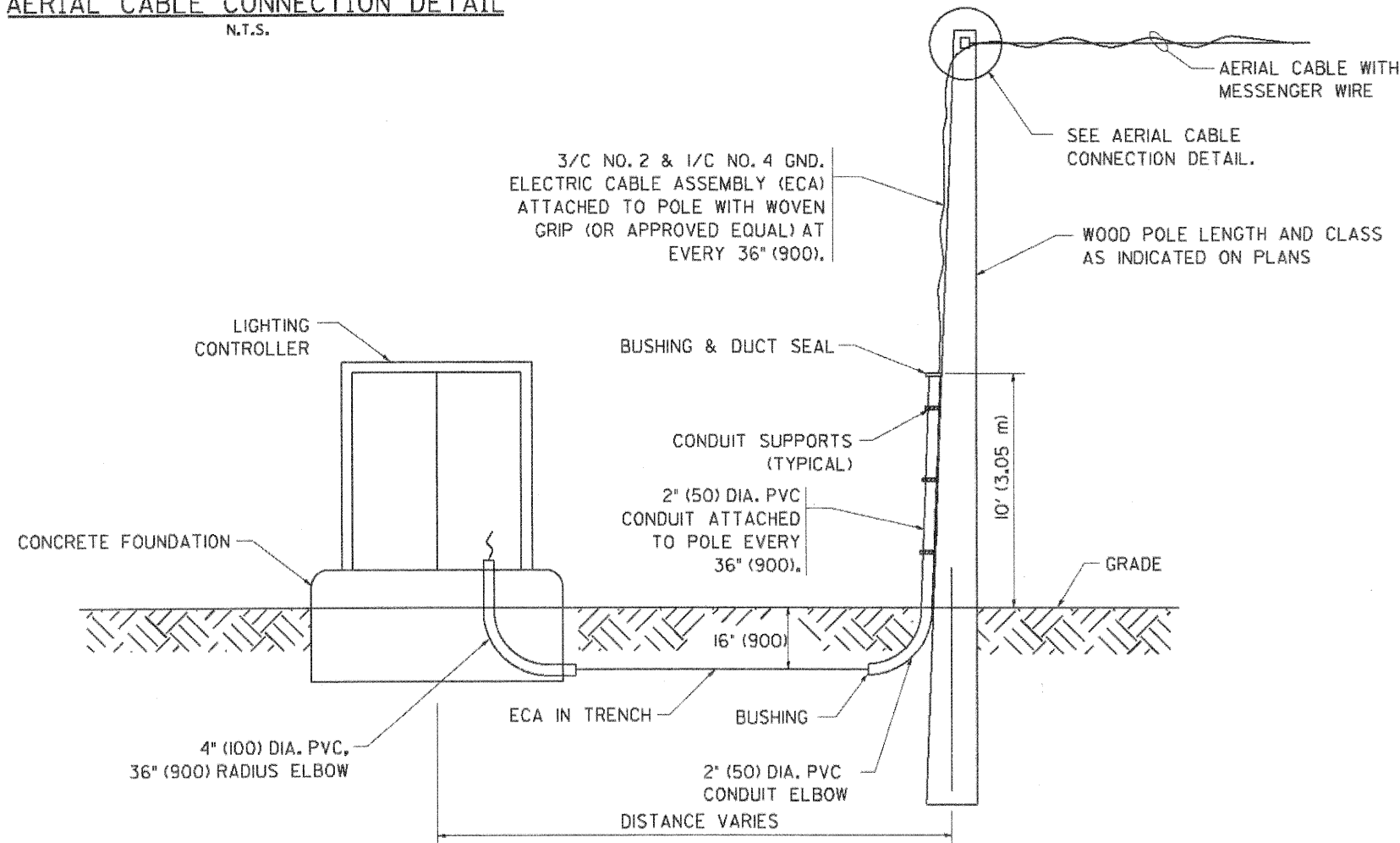
AERIAL CABLE CONNECTION DETAIL
N.T.S.



AERIAL CABLE ATTACHED TO STRUCTURE
NOT TO SCALE

NOTES:

1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED.
2. SEE PROPOSED LIGHTING PLAN FOR CONDUIT, CABLE AND ROUTING.
3. THE CONTRACTOR SHALL PROVIDE INTERMEDIATE SUPPORTS TO MAINTAIN MINIMUM CLEARANCES. REFER TO AERIAL AERIAL CABLE ATTACHED TO STRUCTURE DETAIL.
4. COST OF SPLICES AND MOUNTING HARDWARE SHALL BE INCLUDED IN THE UNIT PRICE FOR AERIAL CABLE.



WOOD POLE TO LIGHTING CONTROLLER WIRING CONNECTION DETAIL
N.T.S.

COMPANY NAME: HRGreen
 PROJECT CONTACT: HRGreen.com
 CLIENT: Illinois Professional Design Firm
 DATE PLOTTED: 4/5/2012 5:08:06 PM
 FILE NAME: 0177-081-16.dgn
 PLOT NUMBER: 63711
 PEN TABLE: Standard Trans.BY



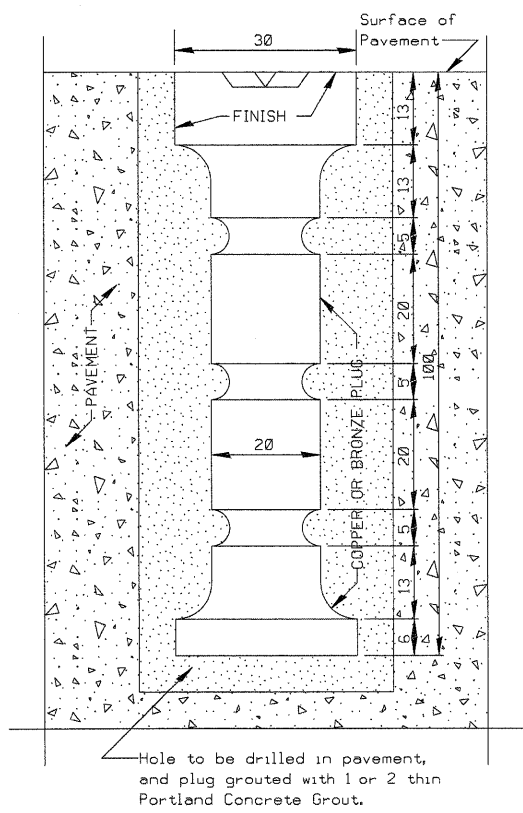
USER NAME = gdelsholt	DESIGNED =	REVISED = 08-08-03
PLOT SCALE = 50,000 / IN.	DRAWN =	REVISED =
PLOT DATE = 4/5/2012	CHECKED =	REVISED =
	DATE = 4/5/12	REVISED =

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

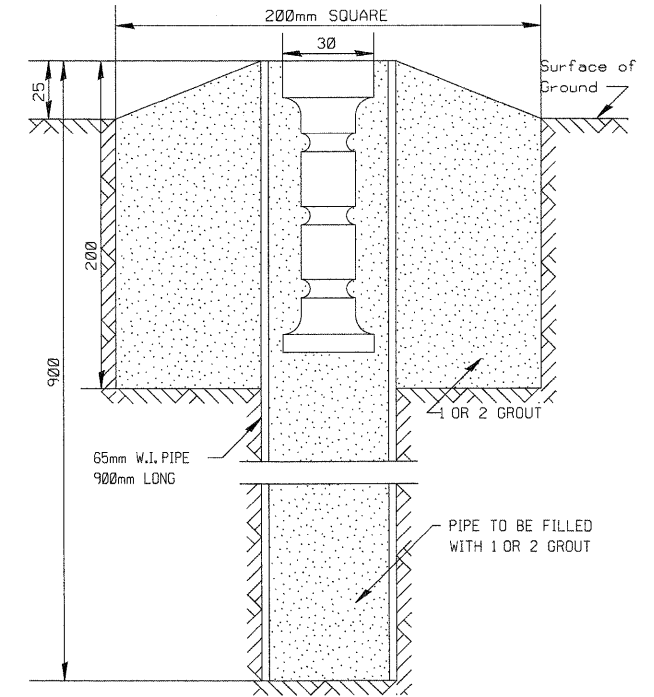
TEMPORARY AERIAL CABLE INSTALLATION

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

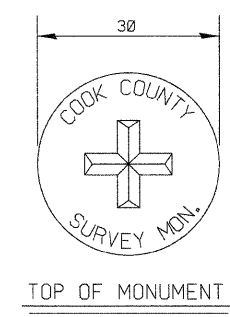
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	51
BE-801		CONTRACT NO. 63711		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



DETAIL FOR PLACING MONUMENT IN PAVEMENT



DETAIL FOR PLACING MONUMENT WHEN LOCATED OUTSIDE OF PAVEMENT



TOP OF MONUMENT

ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE SHOWN.

NOT TO SCALE

COUNTY OF COOK DEPARTMENT OF HIGHWAYS	
DETAIL OF SURVEY MONUMENT	
COMPUTED	K.L.P.
DRAWN	G.D.S.
CHECKS	K.L.P.

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 CLIENT: HRGreen.com
 DATE PLOTTED: 4/5/2012 5:08:22 PM
 FILE NAME: 017-084-17.dwg
 PLOT DATE: 4/5/2012
 PLOT SCALE: 1:1
 PLOT SIZE: 300x300
 PLOT TABLE: 1



USER NAME = jmelhus	DESIGNED -	REVISED -
PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE = 4/5/2012	CHECKED -	REVISED -
	DATE - 4/5/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

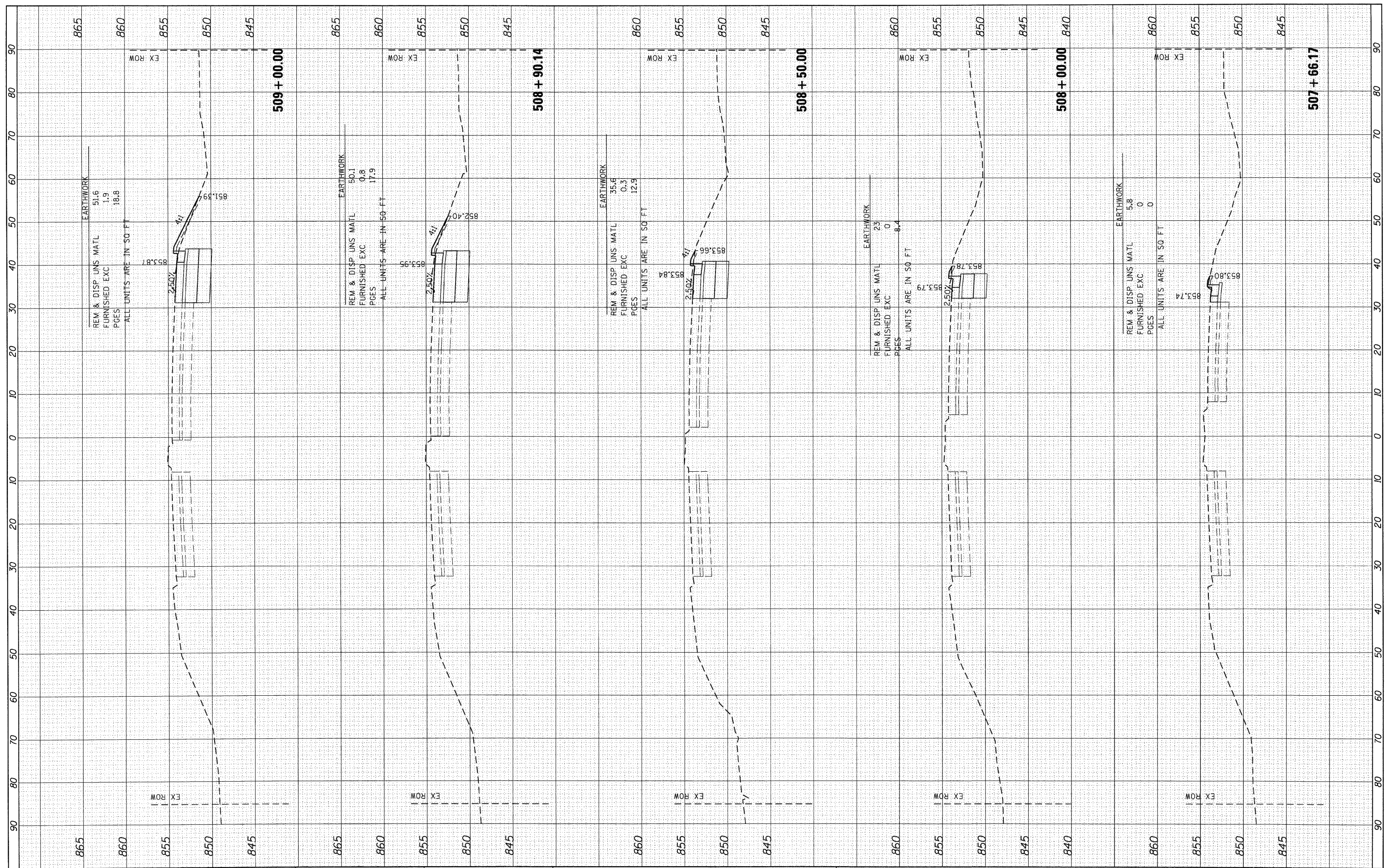
COOK COUNTY HIGHWAY DEPARTMENT
SURVEY MONUMENTS

SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	52
CONTRACT NO. 63711			ILLINOIS FED. AID PROJECT	

FINAL SURVEY NO.	DESIGNED	BY	DATE
NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	DESIGNED	BY	DATE
NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		



FILE NAME = 0:\86110177\CAD\SHEETS\0177-xa-1159.dgn
 USER NAME = jmeihus
 PLOT SCALE =
 PLOT DATE = 4/5/2012

DESIGNED - JPA	REVISED -
DRAWN -	REVISED -
CHECKED - JRM	REVISED -
DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

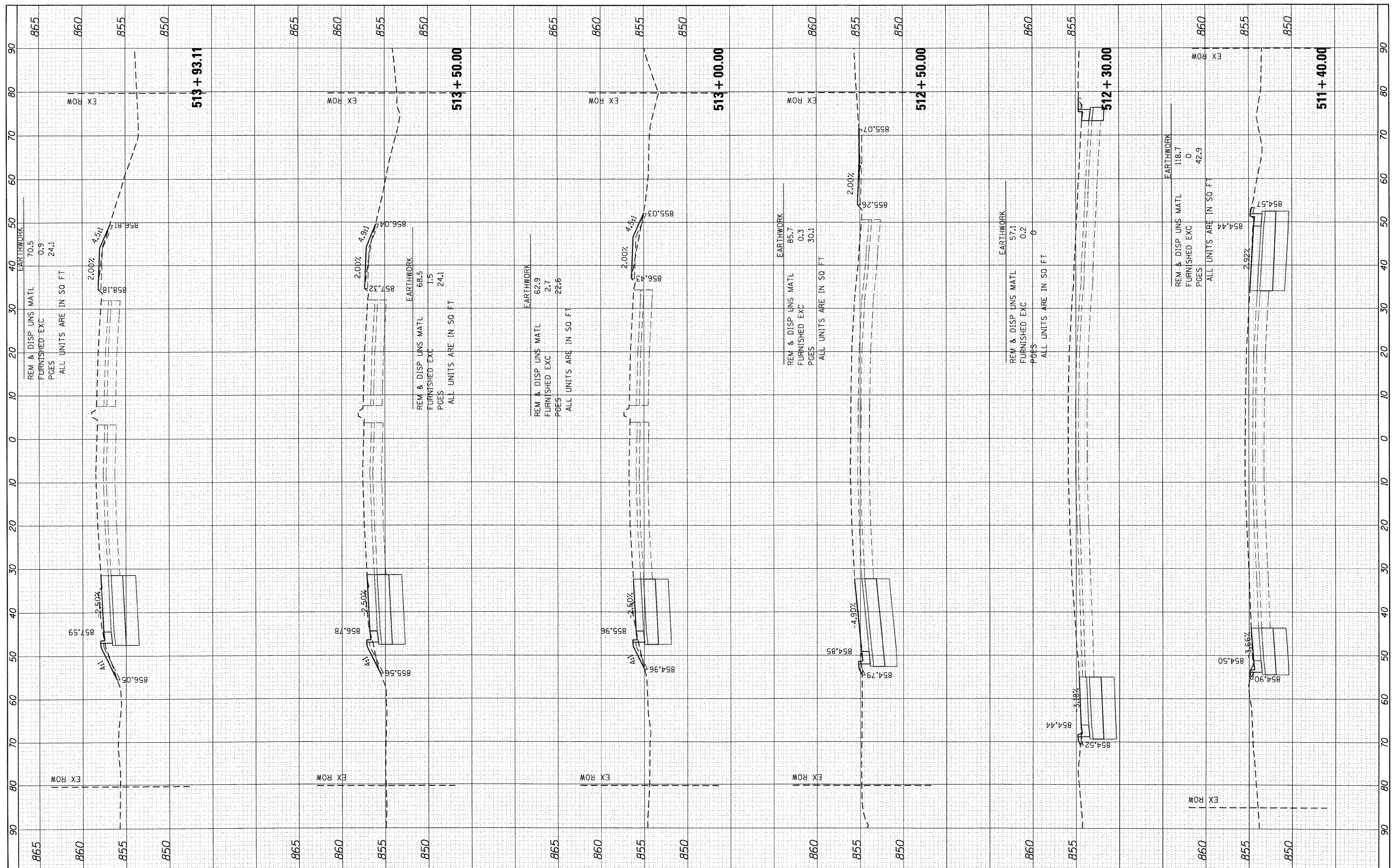
**IL 59
 CROSS SECTIONS**

SCALE: SHEET NO. 1 OF 6 SHEETS STA. 507+66.17 TO STA. 509+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	53
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		



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USER NAME = jmeihus
 DESIGNED - JPA
 DRAWN -
 CHECKED - JRM
 PLOT SCALE =
 PLOT DATE = 4/5/2012

REVISOR -
 REVISION -
 REVISION -
 REVISION -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

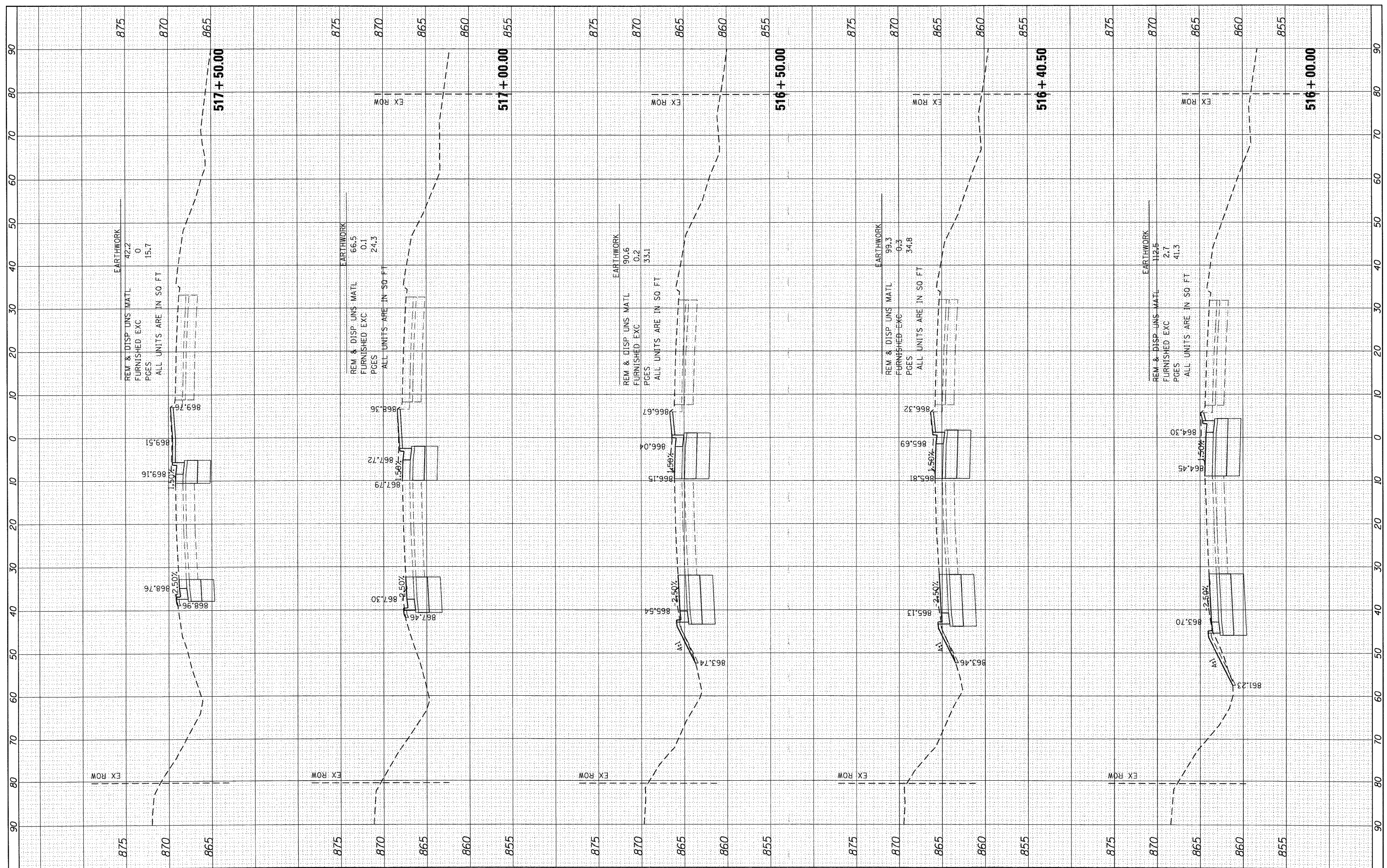
SCALE: SHEET NO. 3 OF 6 SHEETS STA. 511+40.00 TO STA. 513+93.11

**IL 59
 CROSS SECTIONS**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	55
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	BY	DATE
SURVEY PLOTTED		
NOTE BOOK		
AREAS CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEY PLOTTED		
NOTE BOOK		
AREAS CHECKED		
NO.		



FILE NAME =
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USER NAME = jmelhus
 PLOT SCALE =
 PLOT DATE = 4/5/2012

DESIGNED - JPA
 DRAWN -
 CHECKED - JRM
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

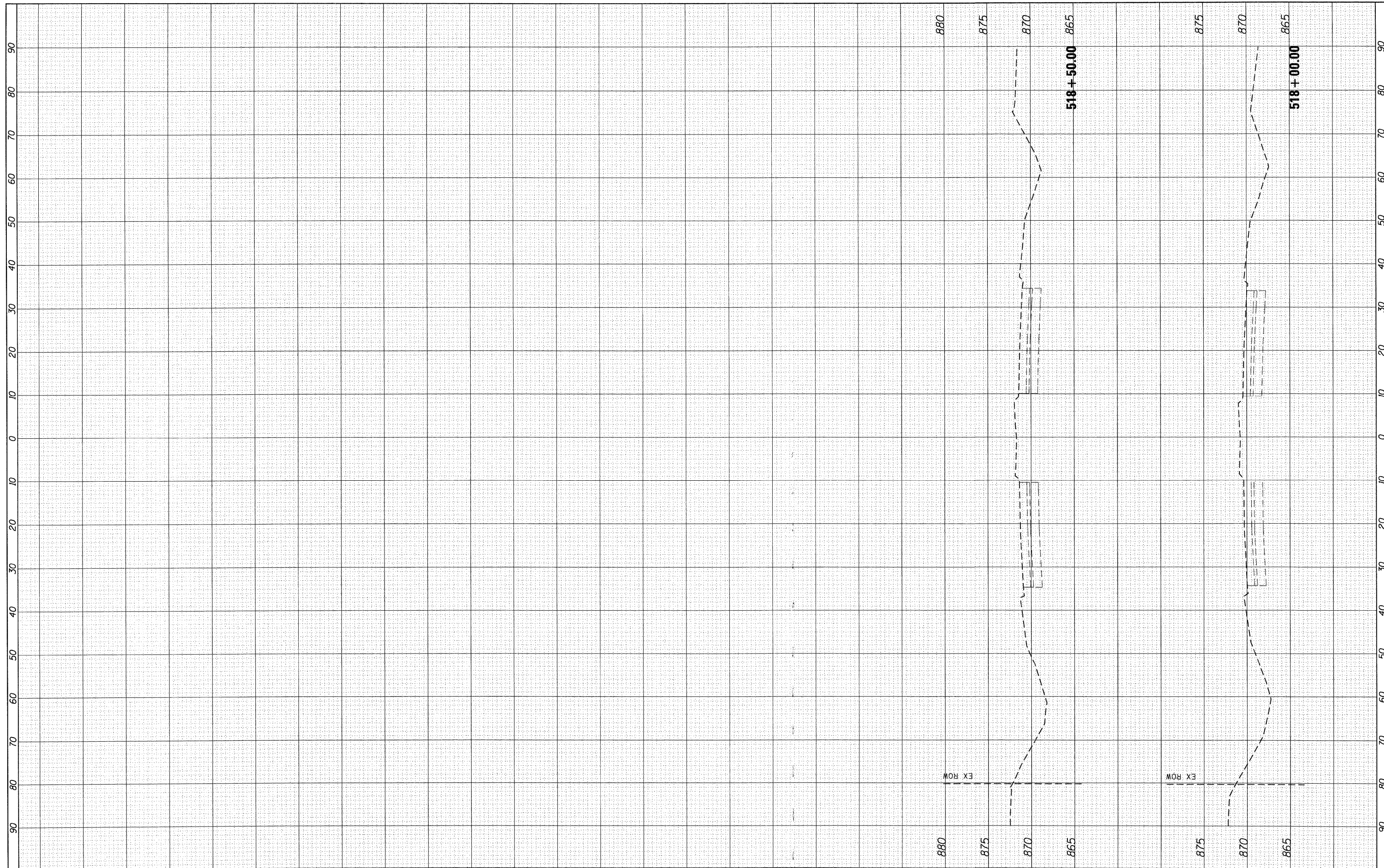
SCALE: SHEET NO. 5 OF 6 SHEETS STA. 516+00.00 TO STA. 517+50.00

**IL 59
 CROSS SECTIONS**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	57
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	CHECKED		



FILE NAME = D:\86118177\CAD\SHEETS\0177-ss-159.dgn

USER NAME = jmelhus
 PLOT SCALE =
 PLOT DATE = 4/5/2012

DESIGNED - JPA
 DRAWN -
 CHECKED - JRM
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

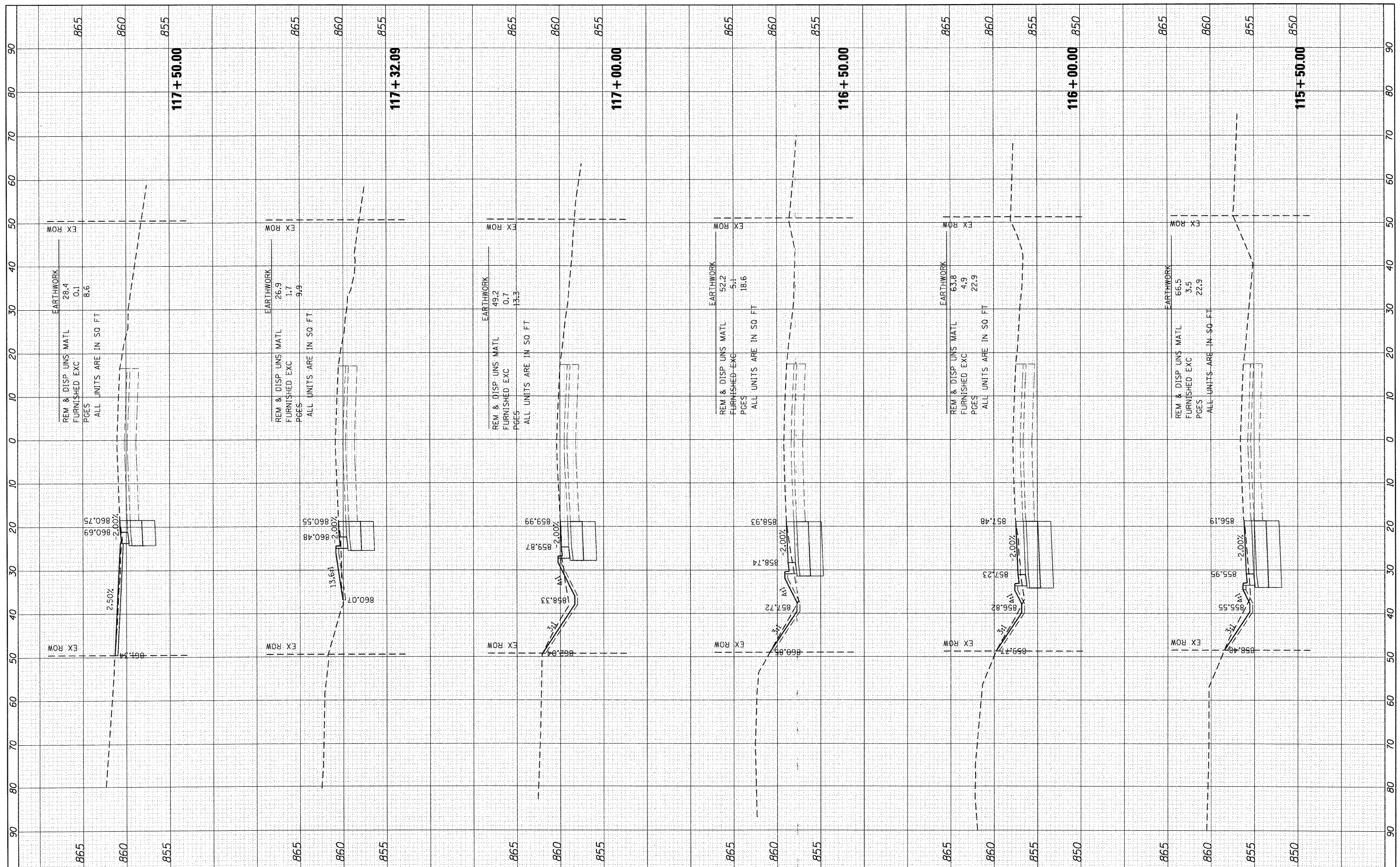
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

IL 59
CROSS SECTIONS
 SCALE: SHEET NO. 6 OF 6 SHEETS STA. 518+00.00 TO STA. 518+50.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	58
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	SUPERVED	DATE
NOTE BOOK	PLOTTED	BY
AREAS CHECKED	TEMPLATE	
NO.	AREAS CHECKED	

ORIGINAL SURVEY	SUPERVED	DATE
NOTE BOOK	PLOTTED	BY
AREAS CHECKED	TEMPLATE	
NO.	AREAS CHECKED	



FILE NAME = \\hrgmhas\data\8611017\CAD\SHEETS\0177-x-s-shp.dgn
 USER NAME = SPernal
 PLOT SCALE =
 PLOT DATE = 4/5/2012

DESIGNED - JPA
 DRAWN -
 CHECKED - JRM
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SHOE FACTORY ROAD
 CROSS SECTIONS**

SCALE: SHEET NO. 4 OF 5 SHEETS STA. 115+50.00 TO STA. 117+50.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	10-00084-00-CH	COOK	63	62
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

